

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
362	0608 RS-5	COOK	25	1
FED. ROAD DIST. NO.		ILLINOIS	CONTRACT NO. 60J98	

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**DIVISION OF HIGHWAYS**

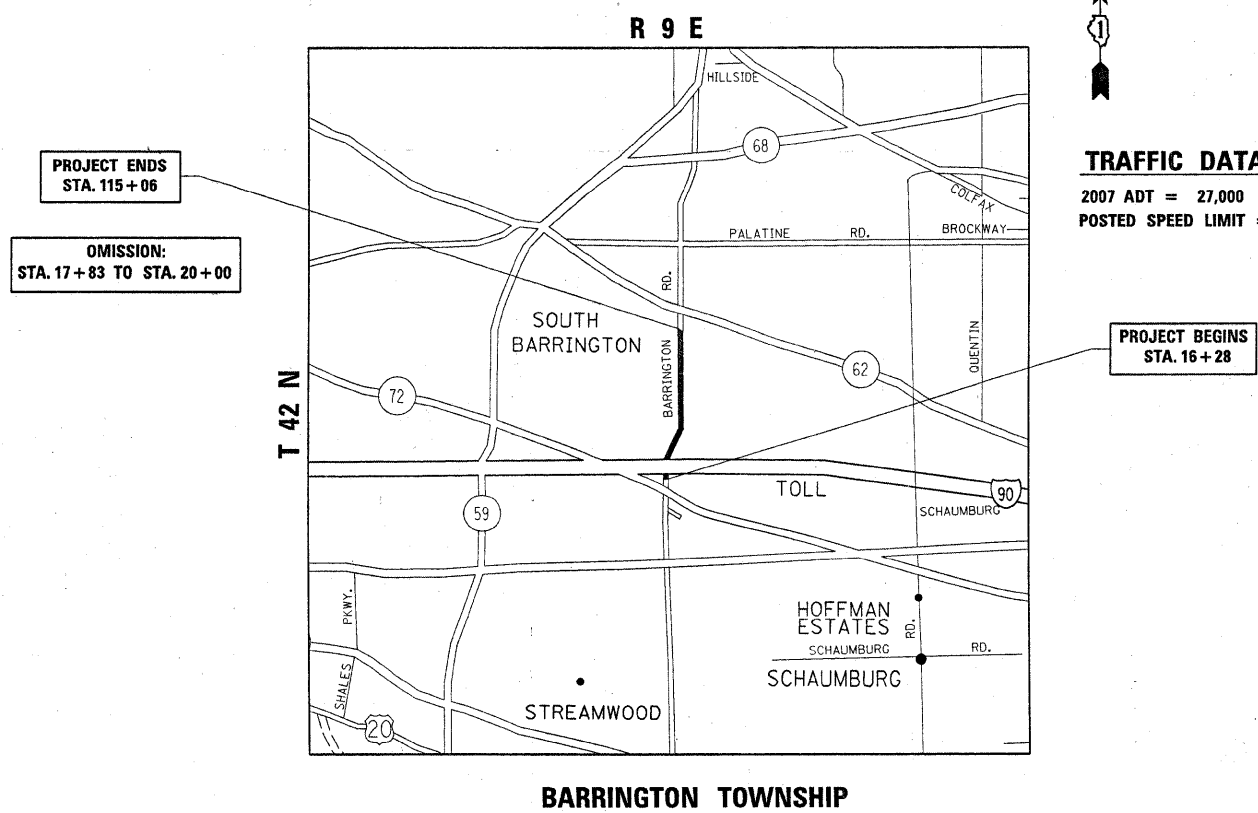
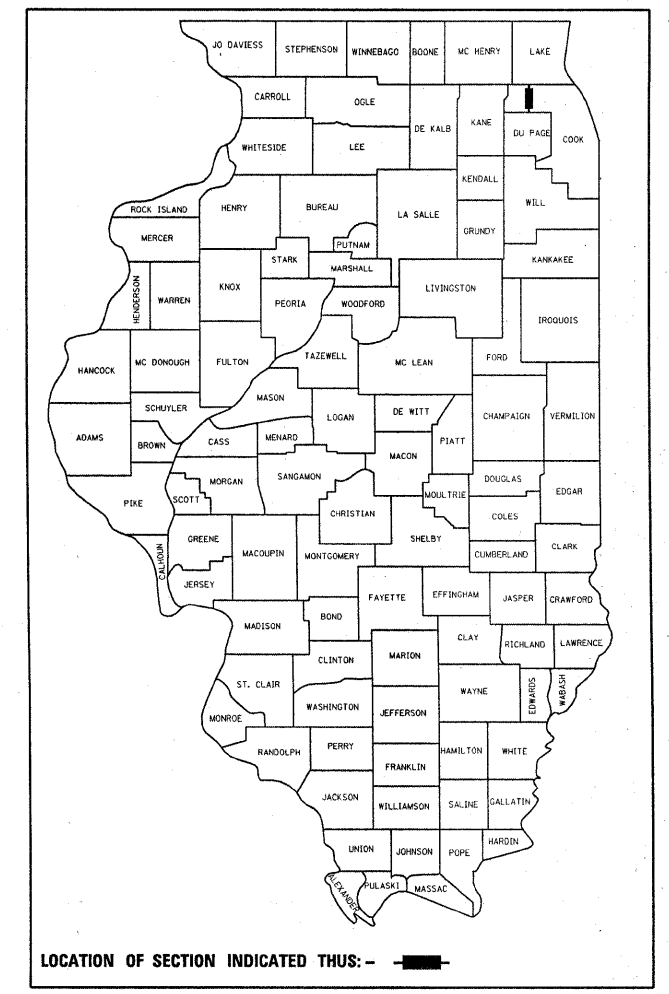
**PROPOSED**  
**HIGHWAY PLANS**

**FAP 362 (BARRINGTON ROAD)**  
**SO IL 62 TO I-90**  
**SECTION: 0608 RS-5**  
**RESURFACING (3P)**  
**PROJECT: NHF-0362(001)**  
**COOK COUNTY**  
**C-91-320-10**

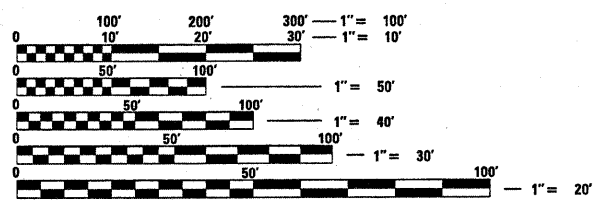
FOR INDEX OF SHEETS, SEE SHEET NO. 2

**THE PROJECT IS LOCATED IN THE VILLAGES OF SOUTH BARRINGTON & HOFFMAN ESTATES**

D -91-320-10



**TRAFFIC DATA**  
2007 ADT = 27,000  
POSTED SPEED LIMIT = 45-50 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENDS  
STA. 115+06

OMISSION:  
STA. 17+83 TO STA. 20+00

PROJECT BEGINS  
STA. 16+28

PROJECT ENGINEER DAN WILGREEN (847) 705-4240  
PROJECT MANAGER KEN ENG (847) 705-4247

GROSS LENGTH OF PROJECT = 9,878 LINEAL FEET = 1.87 MILE  
NET LENGTH OF PROJECT = 9,661 LINEAL FEET = 1.83 MILE

CONTRACT NO. 60J98

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED FEBRUARY 3, 2010

Diana M. O'Keefe  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

March 19 2010  
Scott E. Stitt P.E./m  
ACTING ENGINEER OF DESIGN AND ENVIRONMENT

March 19 2010  
Christine M. Reedy/m  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS**

INDEX OF SHEETS

<u>SHEET NO.</u>	<u>DESCRIPTION</u>
1	COVER SHEET
2	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
3	SUMMARY OF QUANTITIES
4-5	TYPICAL SECTIONS
6-9	ROADWAY AND PAVEMENT MARKING PLANS
10-12	DETECTOR LOOP REPLACEMENT PLANS
13	DETAILS FOR FRAMES AND LIDS ADJUSTMENTS WITH MILLING
14	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
15	CURB AND CURB AND GUTTER REMOVAL AND REPLACEMENT
16	BUTT JOINT AND HMA TAPER DETAILS
17	HMA TAPER AT EDGE OF P.C.C. PAVEMENT
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20	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
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22	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
23	TRAFFIC CONTROL FOR SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES
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25	DISTRICT ONE DETECTOR LOOP INSTALLATION DETAIL FOR ROADWAY RESURFACING

STATE STANDARDS

000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-03	CLASS C AND D PATCHES
604091-02	FRAME AND GRATES, TYPE 24
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
642001-01	SHOULDER RUMBLE STRIPS
701201-03	LANE CLOSURE, 2L, 2W, DAY ONLY
701301-03	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-02	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATION DAY ONLY FOR SPEEDS $\geq$ 45 MPH
701311-03	LANE CLOSURE, 2L 2W, MOVING OPERATIO, DAY ONLY
701501-05	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701601-04	URBAN LANE CLOSURE, MULTILANE 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701606-04	URBAN LANE CLOSURE, MULTILANE 2W WITH MOUNTABLE MEDIAN
701701-04	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-01	TRAFFIC CONTROL DEVICES

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOURS NOTIFICATION REQUIRED).

10 FEET (3 METER) TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURB AND GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGES OF SOUTH BARRINGTON AND HOFFMAN ESTATES.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

THE RESIDENT ENGINEER SHALL CONTACT MR. WALTER CZARNY, AREA TRAFFIC FIELD ENGINEER AT (847) 715-8419 A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKING.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF TEMPORARY TRAFFIC CONTROL DEVICES.

THE RESIDENT ENGINEER SHALL DETERMINE THE LOCATIONS OF CLASS "D" PATCHES AND CURB AND GUTTER REMOVAL AND REPLACEMENT.

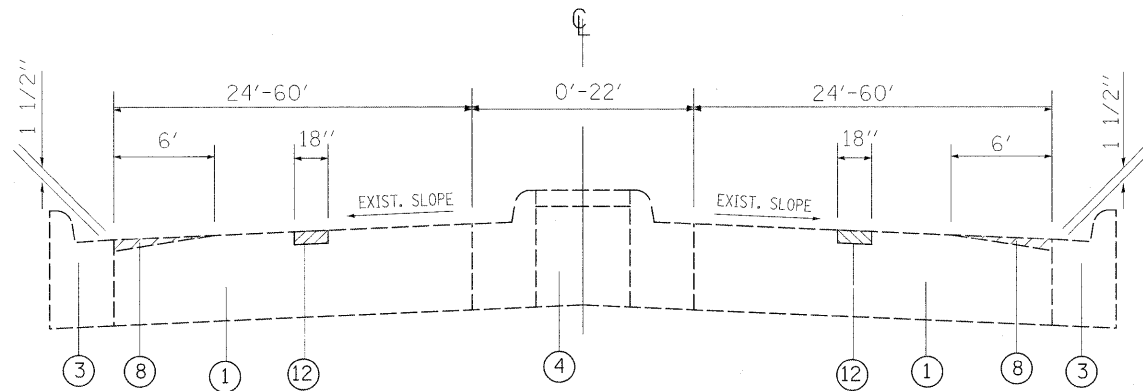
FILE NAME =	USER NAME = shronisb	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES BARRINGTON ROAD, SO IL 62 TO I-90</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
or:\pwork\pwork\shronisb\0182833\0132010-shr-plandgn	DRAWN -	REVISED -	362			0608 RS-5	COOK	25	2	
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -	CONTRACT NO. 60J98							
PLOT DATE = 2/4/2010	DATE -	REVISED -	SCALE: 1"=50'			SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

SUMMARY OF QUANTITIES			80% FED. 20% STATE TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					80% FED. 20% STATE TOTAL QUANTITIES	CONSTRUCTION TYPE CODE						
CODE NO	ITEM	UNIT		URBAN 1000-2A						CODE NO	ITEM	UNIT	URBAN 1000-2A			
20201006	GRADING AND SHAPING SHOULDERS	UNIT	76	76					70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1			
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	50	50					70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1			
25200110	SODDING, SALT TOLERANT	SQ YD	50	50					70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1			
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	37	37					70106800	CHANGEABLE MESSAGE SIGN	CAL MO	2	2			
40600300	AGGREGATE (PRIME COAT)	TON	188	188					70300100	SHORT-TERM PAVEMENT MARKING	FOOT	9000	9000			
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	40	40					70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	1711	1711			
40600826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	2770	2770					70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	37500	37500			
40600895	CONSTRUCTING TEST STRIP	EACH	2	2					70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	8600	8600			
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	172	172					70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	1300	1300			
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	1210	1210					70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1375	1375			
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	52	52					70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	680	680			
40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	7230	7230					70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1000	1000			
42001300	PROTECTIVE COAT	SQ YD	100	100					* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	1711	1711			
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	29300	29300					* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	37500	37500			
44000198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	150	150					* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	8600	8600			
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	300	300					* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	1300	1300			
44002216	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 4"	SQ YD	222	222					* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1375	1375			
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	1464	1464					* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	680	680			
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	17	17					* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	400	400			
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	34	34					78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	395	395			
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	305	305					* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	3209	3209			
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	10	10					X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	51.4	51.4			
60404950	FRAMES AND GRATES, TYPE 24	EACH	1	1					X4400100	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)	SQ YD	6405	6405			
64200105	SHOULDER RUMBLE STRIP	FOOT	2000	2000					X4421000	PARTIAL DEPTH PATCHING	TON	280	280			
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6					X4422030	PARTIAL DEPTH REMOVAL 3"	SQ YD	1602	1602			
67100100	MOBILIZATION	L SUM	1	1					Z0018400	DRAINAGE STRUCTURES TO BE ADJUSTED	EACH	35	35			
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1					Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	40	40			
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1					Z0018600	DRAINAGE STRUCTURES TO BE RECONSTRUCTED	EACH	1	1			
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1												

\* SPECIALTY ITEMS  
NON-PARTICIPATING  
© Y080

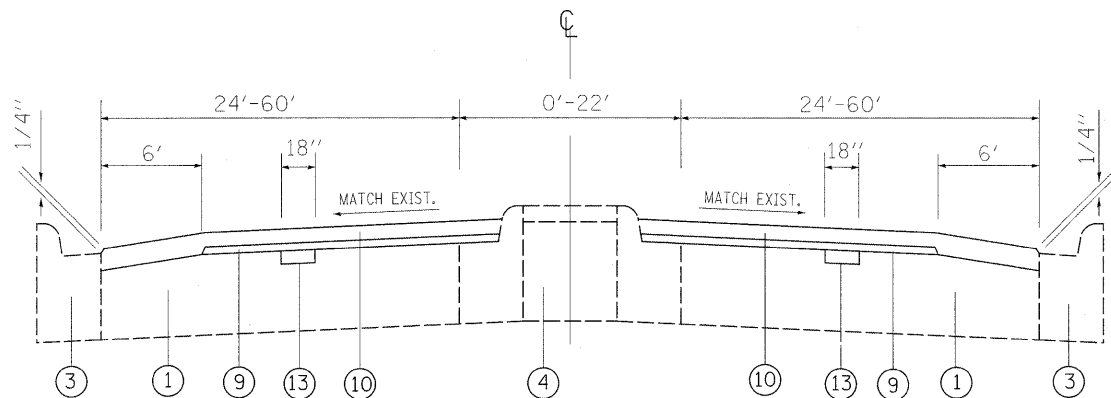
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FILE NAME =	USER NAME = strantab	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>				<b>SUMMARY OF QUANTITIES BARRINGTON ROAD, SO IL 62 TO I-90</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
es:\pw_wor\p\w\dot\strantab\0102835\0132010-str-plot.rgn		DRAWN -	REVISED -									362	0608 RS-5	COOK	25	3			
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED -									SCALE: SHEET NO. OF SHEETS STA. TO STA.				FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT			
	PLOT DATE = 2/4/2010	DATE -	REVISED -													CONTRACT NO. 60J98			



EXISTING TYPICAL SECTION

BARRINGTON ROAD  
STA. 20+66 TO STA. 68+69



PROPOSED TYPICAL SECTION

BARRINGTON ROAD  
STA. 20+66 TO STA. 68+69

LEGEND

- ① EXISTING P.C.C. PAVEMENT, 9" (+/-)
- ② EXISTING HMA SURFACE COURSE, 4" (+/-)
- ③ EXISTING CONCRETE CURB & GUTTER
- ④ EXISTING MEDIAN
- ⑤ EXISTING HMA SHOULDER
- ⑥ EXISTING AGGREGATE SHOULDER
- ⑦ PROPOSED HMA SURFACE REMOVAL, 2 1/2"
- ⑧ PROPOSED PCC SURFACE REMOVAL (VARIABLE DEPTH), 6' WIDE
- ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4 "
- ⑩ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, 1 3/4 "
- ⑪ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B AND GRADING AND SHAPING SHOULDERS
- ⑫ PROPOSED PARTIAL DEPTH REMOVAL, 3"
- ⑬ PROPOSED PARTIAL DEPTH PATCHING, 3"
- ⑭ PROPOSED SHOULDER RUMBLE STRIP

NOTE: CONTRACTOR SHALL PATCH FIRST BEFORE MILLING.

NOTE: SEE HMA TAPER AT EDGE OF PCC PAVEMENT DETAIL BD-33

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

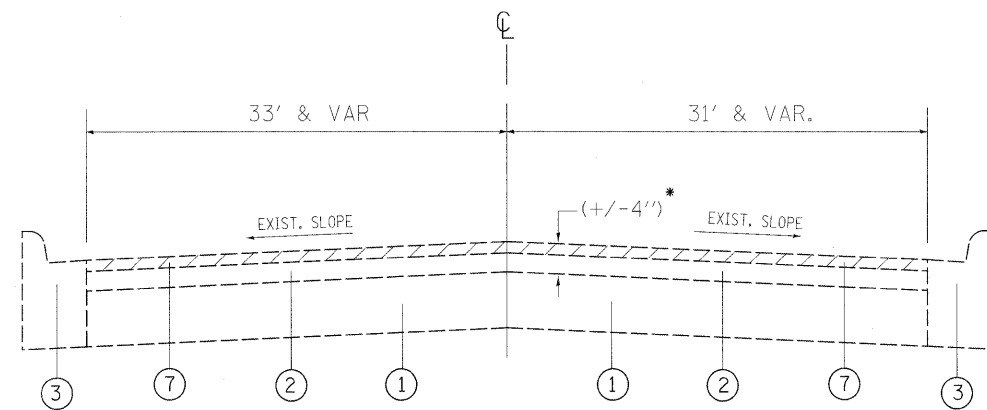
MIXTURE USES	DESIGN AIR VOIDS
POLYMERIZED HMA SURFACE COURSE, MIX "F", N90 (IL-9.5 mm)	4% AT 90 GYR.
POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50	4% AT 50 GYR.
CLASS D PATCHES, (HMA BINDER IL-19.0 mm)	4% AT 70 GYR.
PARTIAL DEPTH PATCHING (HMA BINDER IL-19.0, N70), 3"	4% AT 70 GYR.
HMA REPLACEMENT OVER PATCHES (HMA BINDER, IL-19.0 mm)	4% AT 70 GYR.

NOTES:

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURES QUANTITIES IS 112 LBS/SQ YD/IN.

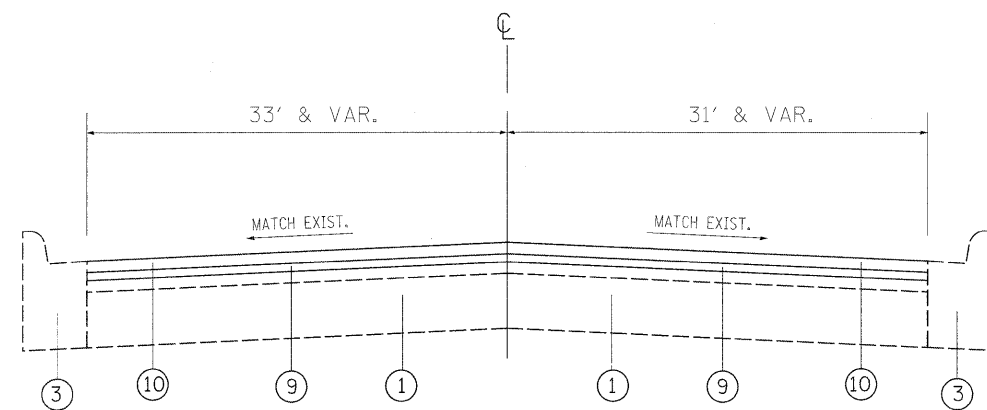
THE "AC TYPE FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

FILE NAME =	USER NAME = shronsb	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EXISTING AND PROPOSED TYPICAL SECTIONS BARRINGTON ROAD, SO IL 62 TO I-90</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT DATE = 2/4/2010	CHECKED -	REVISED -			CONTRACT NO. 60J98				
		DATE -	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
					SCALE:	SHEET NO. OF SHEETS		STA. TO STA.		



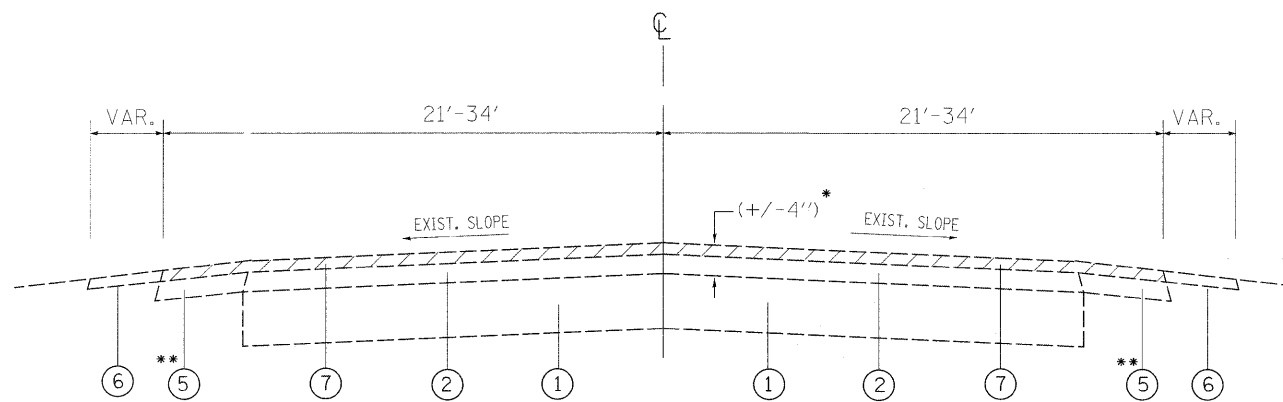
EXISTING TYPICAL SECTION

BARRINGTON ROAD  
 STA. 68+69 TO STA. 78+00  
 STA. 16+28 TO STA. 17+83  
 STA. 20+00 TO STA. 20+66



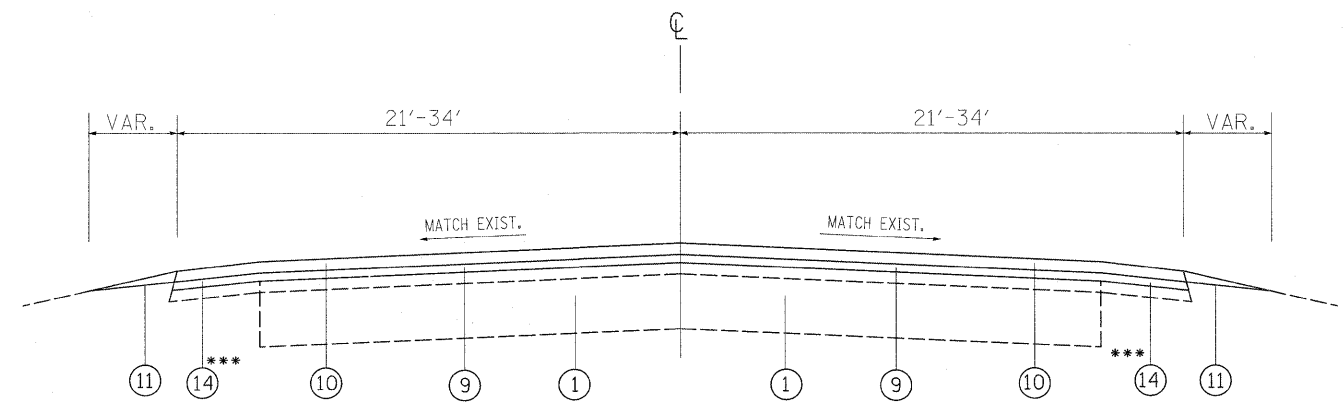
PROPOSED TYPICAL SECTION

BARRINGTON ROAD  
 STA. 68+69 TO STA. 78+00  
 STA. 16+28 TO STA. 17+83  
 STA. 20+00 TO STA. 20+66



EXISTING TYPICAL SECTION

BARRINGTON ROAD  
 STA. 78+00 TO STA. 115+06  
 \*\* EXISTING SHOULDER RUMBLE STRIPS STA. 78+60 TO 87+00



PROPOSED TYPICAL SECTION

BARRINGTON ROAD  
 STA. 78+00 TO STA. 115+06  
 \*\*\* PROP. SHOULDER RUMBLE STRIPS STA. 78+60 TO 87+00

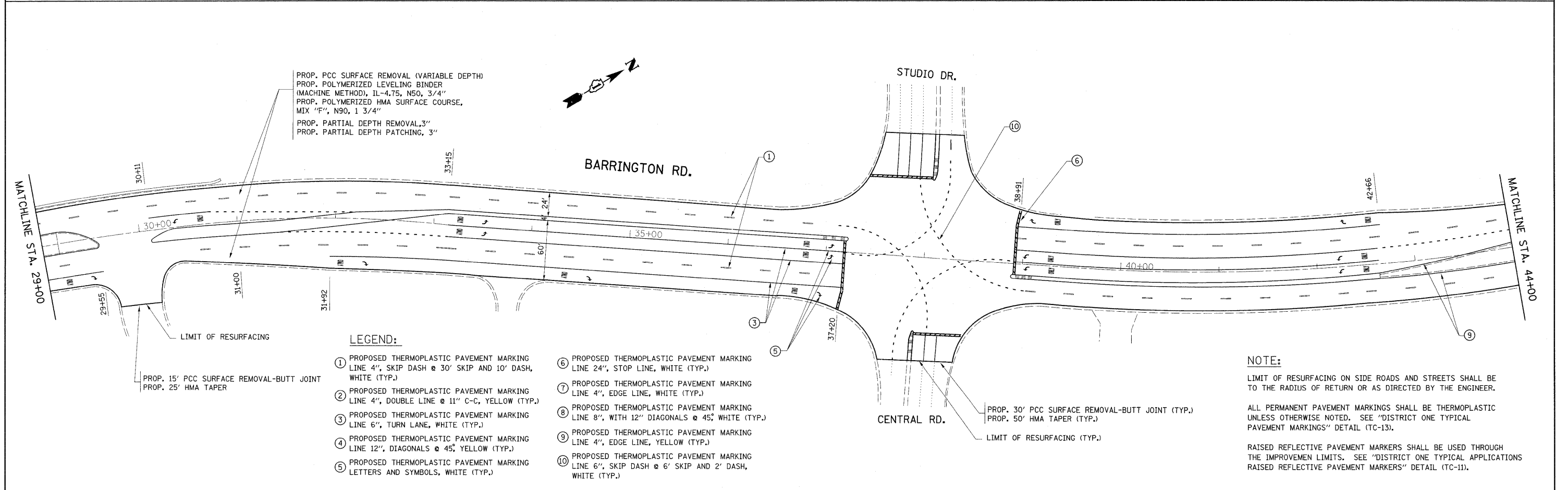
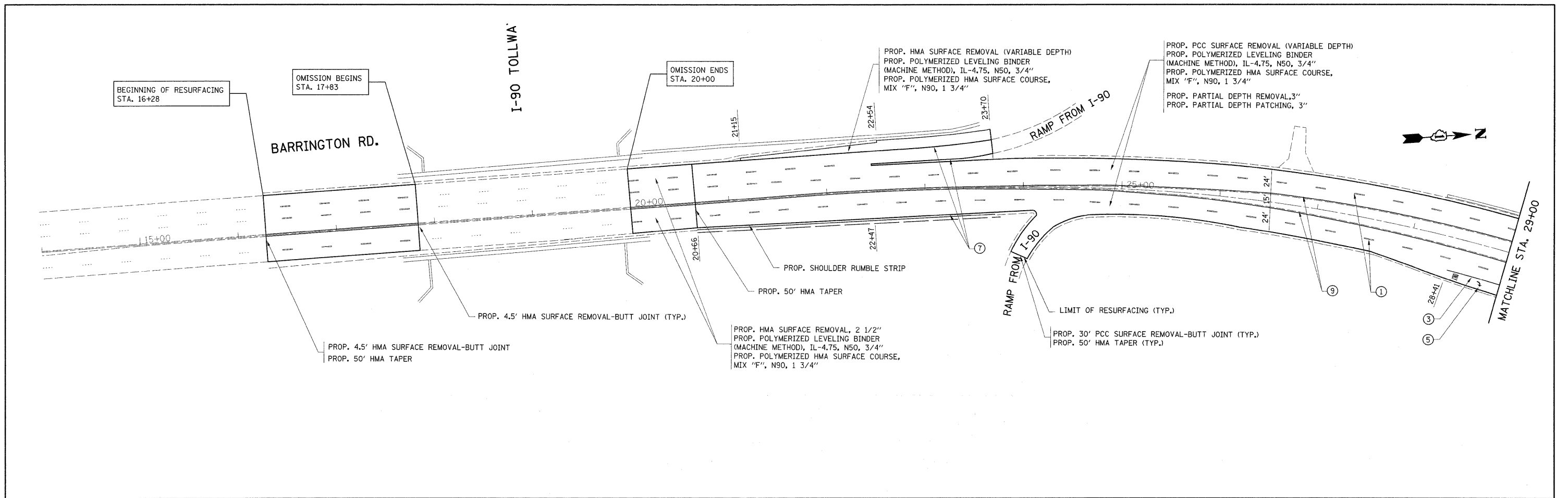
LEGEND

- ① EXISTING P.C.C. PAVEMENT, 9" (+/-)
- ② EXISTING HMA SURFACE COURSE, 4" (+/-)
- ③ EXISTING CONCRETE CURB & GUTTER
- ④ EXISTING MEDIAN
- ⑤ EXISTING HMA SHOULDER
- ⑥ EXISTING AGGREGATE SHOULDER
- ⑦ PROPOSED HMA SURFACE REMOVAL, 2 1/2"
- ⑧ PROPOSED PCC SURFACE REMOVAL (VARIABLE DEPTH), 6' WIDE
- ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4 "
- ⑩ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, 1 3/4 "
- ⑪ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B AND GRADING AND SHAPING SHOULDERS
- ⑫ PROPOSED PARTIAL DEPTH REMOVAL, 3"
- ⑬ PROPOSED PARTIAL DEPTH PATCHING, 3"
- ⑭ PROPOSED SHOULDER RUMBLE STRIP

NOTE: CONTRACTOR SHALL PATCH FIRST BEFORE MILLING.

NOTE: SEE HMA TAPER AT EDGE OF PCC PAVEMENT DETAIL BD-33

FILE NAME =	USER NAME = shiransb	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EXISTING AND PROPOSED TYPICAL SECTIONS BARRINGTON ROAD, SO IL 62 TO I-90</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pwork\p\dot\shiransb\182833\013018-shr-p\an.dgn	PLOT SCALE = 50,0000 ' / IN.	DRAWN -	REVISED -					362	0608 RS-5	COOK	25	5
	PLOT DATE = 2/4/2010	CHECKED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		CONTRACT NO. 60J98		
		DATE -	REVISED -									



**LEGEND:**

- ① PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", SKIP DASH @ 30' SKIP AND 10' DASH, WHITE (TYP.)
- ② PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", DOUBLE LINE @ 11" C-C, YELLOW (TYP.)
- ③ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 6", TURN LANE, WHITE (TYP.)
- ④ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 12", DIAGONALS @ 45°, YELLOW (TYP.)
- ⑤ PROPOSED THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE (TYP.)
- ⑥ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 24", STOP LINE, WHITE (TYP.)
- ⑦ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", EDGE LINE, WHITE (TYP.)
- ⑧ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 8", WITH 12" DIAGONALS @ 45°, WHITE (TYP.)
- ⑨ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", EDGE LINE, YELLOW (TYP.)
- ⑩ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 6", SKIP DASH @ 6' SKIP AND 2' DASH, WHITE (TYP.)

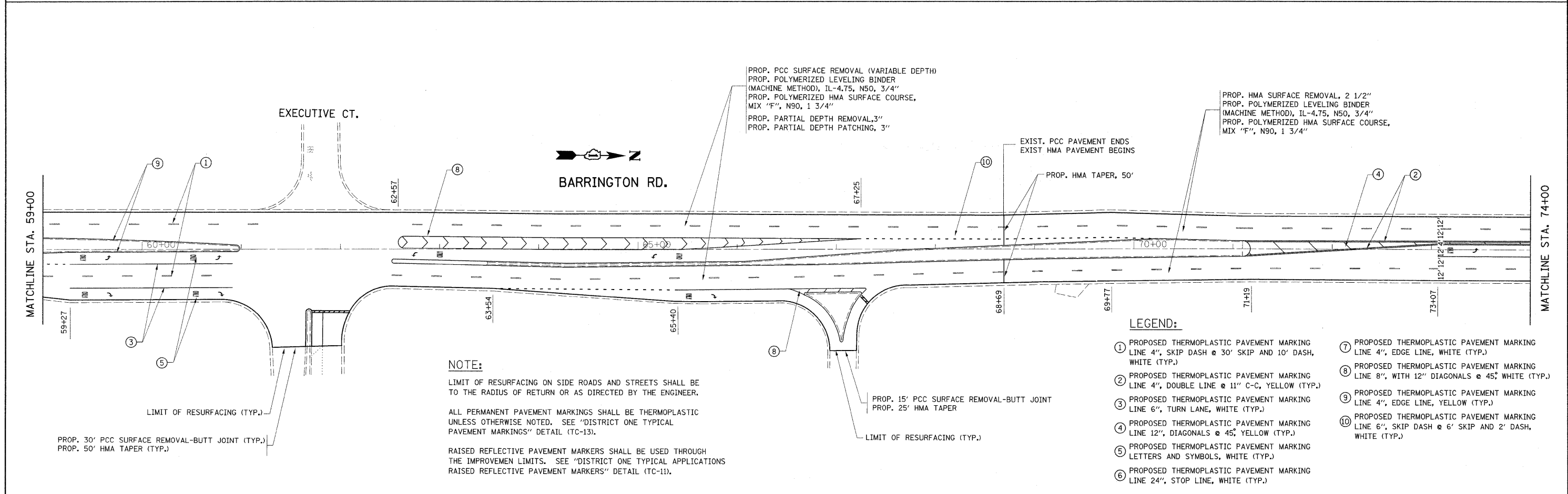
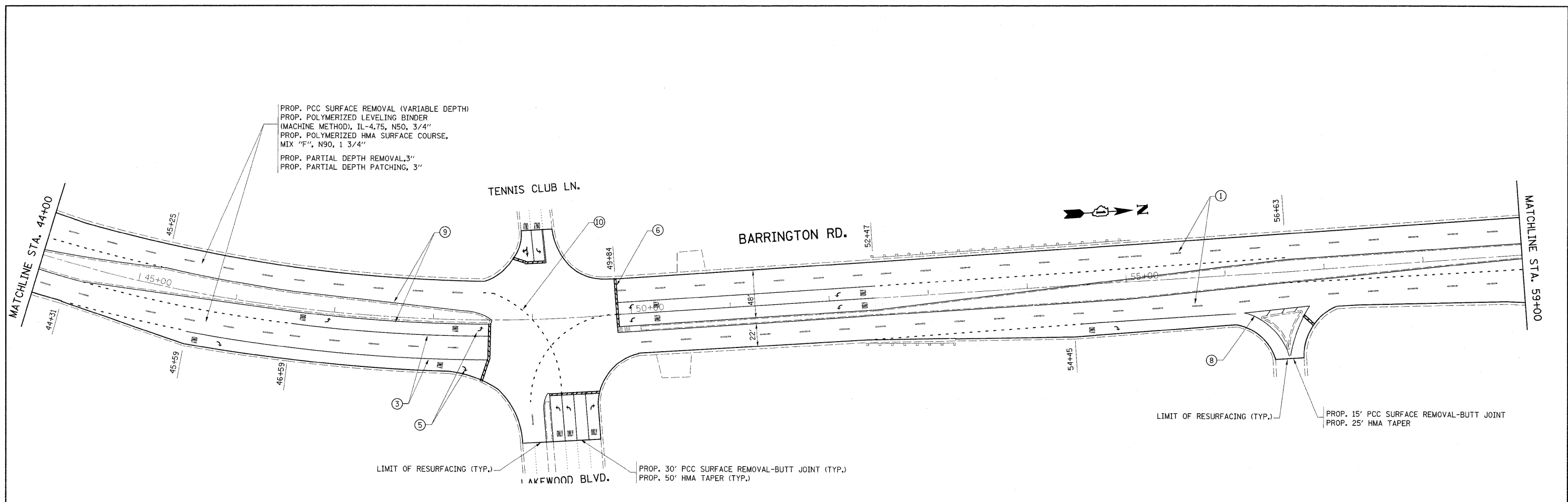
**NOTE:**

LIMIT OF RESURFACING ON SIDE ROADS AND STREETS SHALL BE TO THE RADIUS OF RETURN OR AS DIRECTED BY THE ENGINEER.

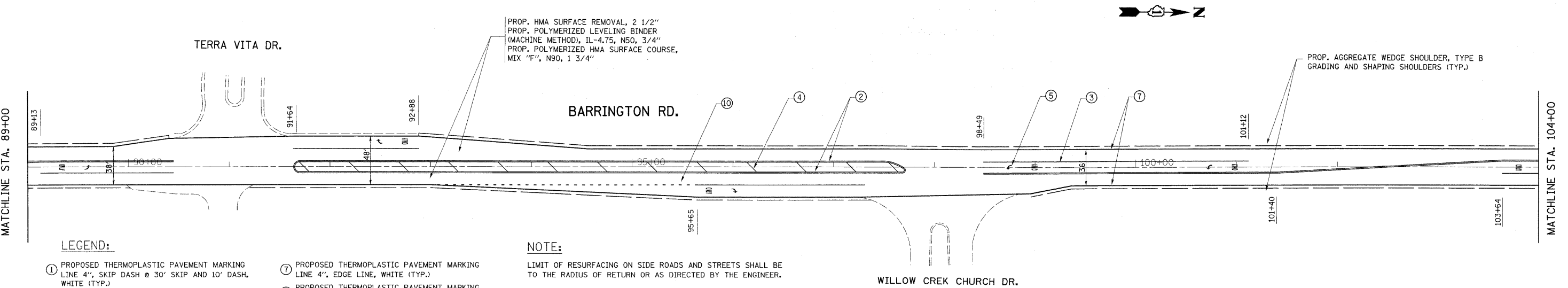
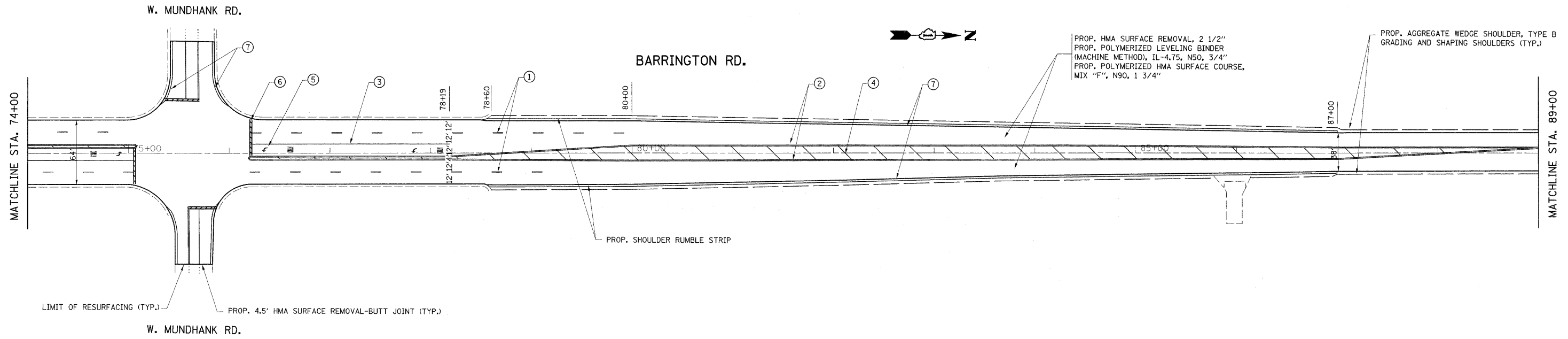
ALL PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED. SEE "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL (TC-13).

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE USED THROUGH THE IMPROVEMENT LIMITS. SEE "DISTRICT ONE TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS" DETAIL (TC-11).

FILE NAME =	USER NAME = shiranisb	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY AND PAVEMENT MARKING PLANS BARRINGTON ROAD, SO IL 62 TO I-90</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca\pwwork\pwwork\shiranisb\0182833\0134	218-shr-plan.dgn	DRAWN -	REVISED -					362	0608 RS-5	COOK	25	6
PLOT SCALE = 58.0000' / IN.	CHECKED -	REVISED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.			CONTRACT NO. 60J98				
PLOT DATE = 2/4/2010	DATE -	REVISED -	REVISED -					ILLINOIS FED. AID PROJECT				



FILE NAME =	USER NAME = shuranisb	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY AND PAVEMENT MARKING PLANS BARRINGTON ROAD, SO IL 62 TO I-90</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pwork\pwork\shuranisb\0182833\0132018-shr-plan.dgn	DRAWN -	REVISED -	362			0608 RS-5	COOK	25	7	
PLOT SCALE = 58.0000' / IN.	CHECKED -	REVISED -	CONTRACT NO. 60J98							
PLOT DATE = 2/4/2018	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							



**LEGEND:**

- ① PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", SKIP DASH @ 30' SKIP AND 10' DASH, WHITE (TYP.)
- ② PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", DOUBLE LINE @ 11" C-C, YELLOW (TYP.)
- ③ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 6", TURN LANE, WHITE (TYP.)
- ④ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 12", DIAGONALS @ 45°, YELLOW (TYP.)
- ⑤ PROPOSED THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE (TYP.)
- ⑥ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 24", STOP LINE, WHITE (TYP.)
- ⑦ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", EDGE LINE, WHITE (TYP.)
- ⑧ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 8", WITH 12" DIAGONALS @ 45°, WHITE (TYP.)
- ⑨ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", EDGE LINE, YELLOW (TYP.)
- ⑩ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 6", SKIP DASH @ 6' SKIP AND 2' DASH, WHITE (TYP.)

**NOTE:**

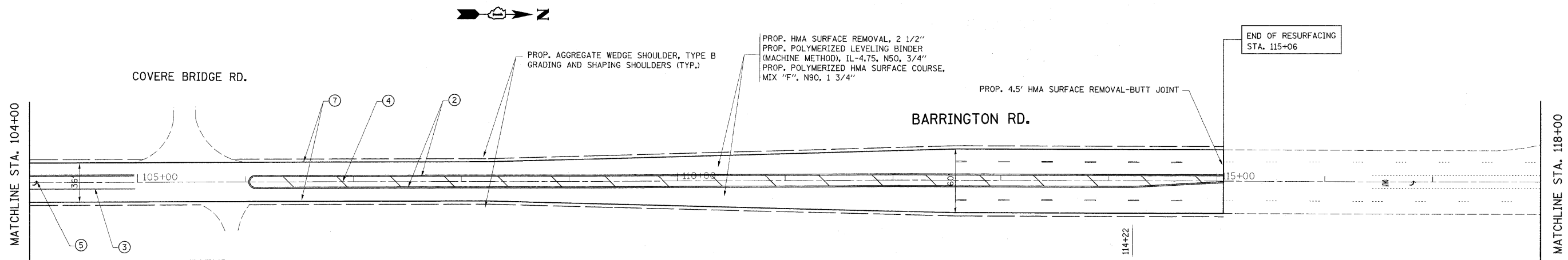
LIMIT OF RESURFACING ON SIDE ROADS AND STREETS SHALL BE TO THE RADIUS OF RETURN OR AS DIRECTED BY THE ENGINEER.

ALL PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED. SEE "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL (TC-13).

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE USED THROUGH THE IMPROVEMENT LIMITS. SEE "DISTRICT ONE TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS" DETAIL (TC-11).

FILE NAME =	USER NAME = shurenab	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY AND PAVEMENT MARKING PLANS BARRINGTON ROAD, SO IL 62 TO I-90</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pw\work\p\dot\shurenab\08182833\013	018-shr-plan.dgn	DRAWN -	REVISED -			362	0608 RS-5	COOK	25	8
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 60J98				
	PLOT DATE = 2/4/2018	DATE -	REVISED -			ILLINOIS FED. AID PROJECT				
					SCALE:	SHEET NO. OF SHEETS STA. TO STA.				





**LEGEND:**

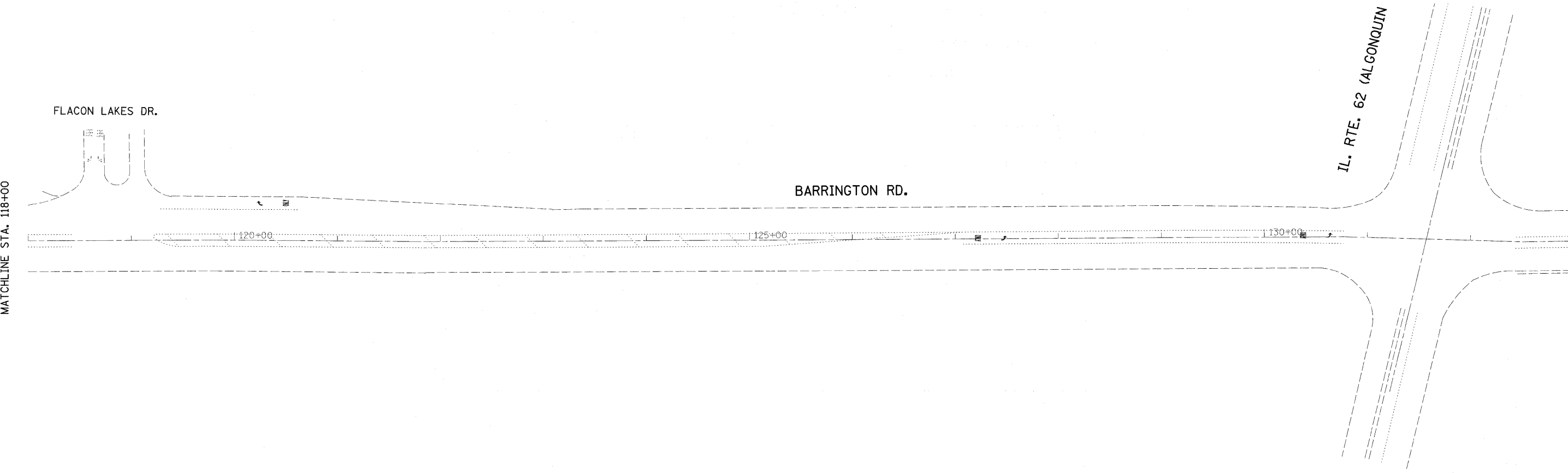
- |   |  |
|---|--|
| <ul style="list-style-type: none"> <li>① PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", SKIP DASH @ 30' SKIP AND 10' DASH, WHITE (TYP.)</li> <li>② PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", DOUBLE LINE @ 11" C-C, YELLOW (TYP.)</li> <li>③ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 6", TURN LANE, WHITE (TYP.)</li> <li>④ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 12", DIAGONALS @ 45°, YELLOW (TYP.)</li> <li>⑤ PROPOSED THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE (TYP.)</li> <li>⑥ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 24", STOP LINE, WHITE (TYP.)</li> </ul> | <ul style="list-style-type: none"> <li>⑦ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", EDGE LINE, WHITE (TYP.)</li> <li>⑧ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 8", WITH 12" DIAGONALS @ 45°, WHITE (TYP.)</li> <li>⑨ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", EDGE LINE, YELLOW (TYP.)</li> <li>⑩ PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 6", SKIP DASH @ 6' SKIP AND 2' DASH, WHITE (TYP.)</li> </ul> |
|---|--|

**NOTE:**

LIMIT OF RESURFACING ON SIDE ROADS AND STREETS SHALL BE TO THE RADIUS OF RETURN OR AS DIRECTED BY THE ENGINEER.

ALL PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED. SEE "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL (TC-13).

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE USED THROUGH THE IMPROVEMENT LIMITS. SEE "DISTRICT ONE TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS" DETAIL (TC-11).

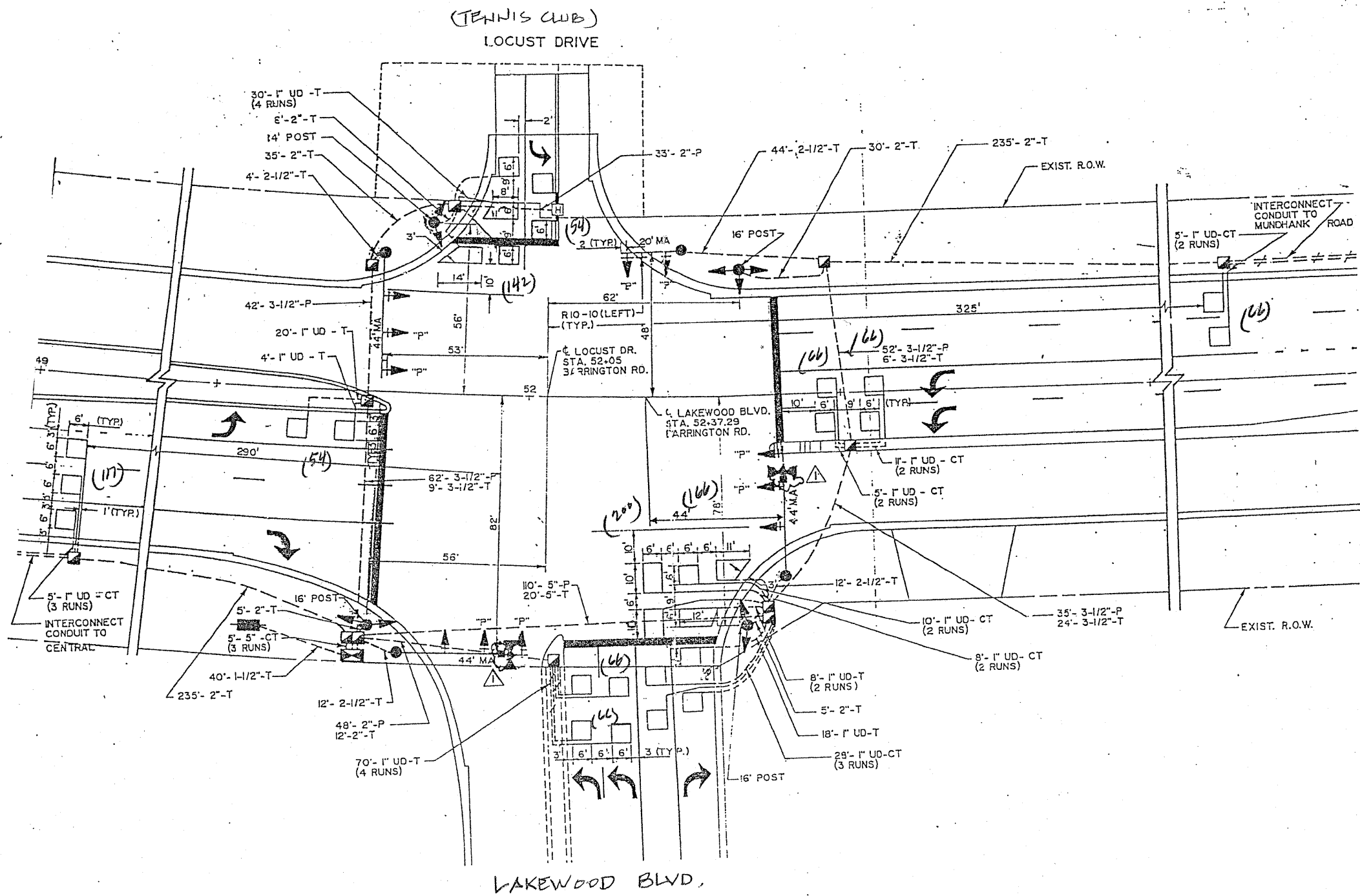


FILE NAME =	USER NAME = shiranisb	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY AND PAVEMENT MARKING PLANS BARRINGTON ROAD, SO IL 62 TO I-90</b>	F.A.B. RTE. 362	SECTION 0608 RS-5	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 9	
ca:\pwwork\pwwork\shiranisb\182833\0132018-shrt-plan.dgn	PLOT SCALE = 50,0000 ' / IN.	CHECKED -	REVISED -			SCALE:	SHEET NO. OF SHEETS STA. TO STA.	CONTRACT NO. 60J98			
PLOT DATE = 2/4/2018	DATE -	REVISED -	REVISED -			ILLINOIS FED. AID PROJECT					



TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP	□	□
SIGNAL HEAD	—	⇨
G.S. CONDUIT IN TRENCH OR PUSHED	---	—
SIGNAL HEAD WITH BACKPLATE		⇨
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		— "E"
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		⇨



THIS PLAN IS FOR THE SOLE  
PURPOSE OF DETECTOR LOOP  
REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	931	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = kentphixaybc	DESIGNED - BCK	REVISED -
c:\p\work\KPH1001\KANTHAPHIXAYBC\d01126	c:\work\legand_v7.dgn	DRAWN - BCK	REVISED -
PLOT SCALE = 31.9360' / IN.	CHECKED - DAD	REVISED -	REVISED -
PLOT DATE = 4/3/2009	DATE	REVISED -	REVISED -

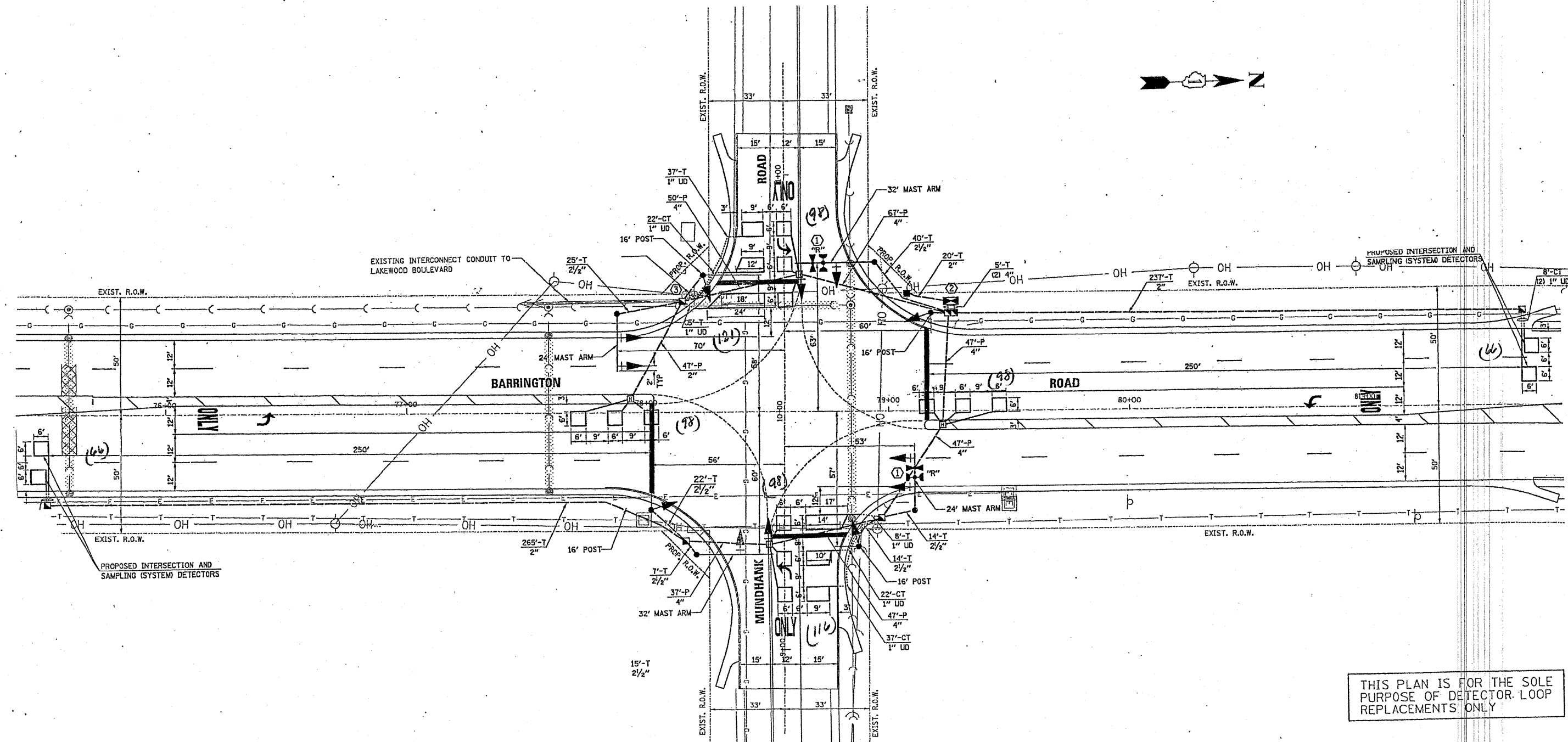
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE - DETECTOR LOOP REPLACEMENT  
BARRINGTON ROAD @ LAKEWOOD BLVD.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
362	0608 RS-5	LOOK	25	11
SCALE: NONE		SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO. (ILLINOIS) FED. AID PROJECT
				60798

**TRAFFIC SIGNAL LEGEND**

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	761	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =  
c:\pwwork\PHIDOT\KANTHAPHIXAYBC\01126

USER NAME = kenthophixaybc  
4\tr\offic.legend.v7.dgn  
PLOT SCALE = 3/8" = 1' IN.

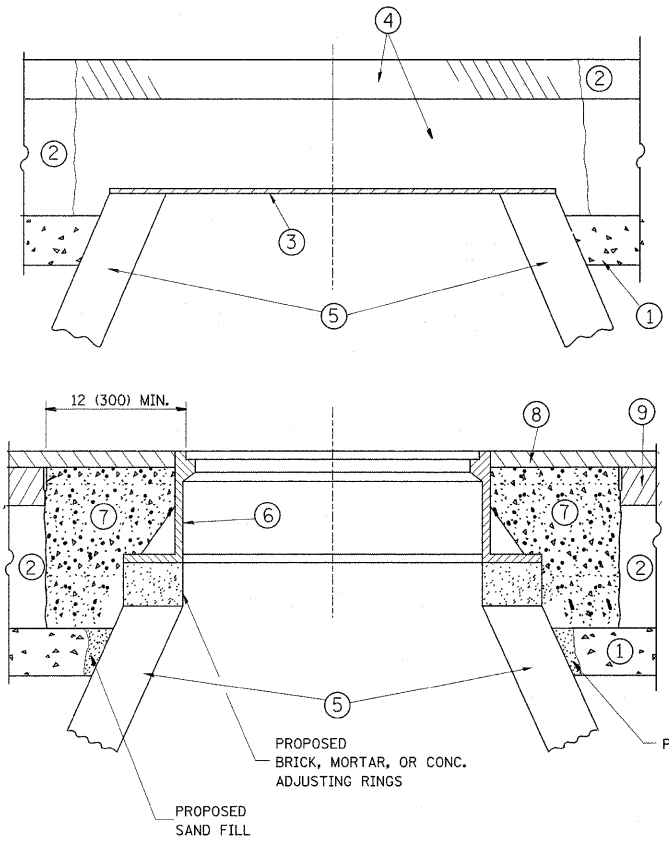
DESIGNED - BCK	REVISED -
DRAWN - BCK	REVISED -
CHECKED - DAD	REVISED -
DATE	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE - DETECTOR LOOP REPLACEMENT  
BARRINGTON ROAD @ MUNDHANK RD.

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE. 362	SECTION 0608 RS-5	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 12
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		CONTRACT NO. 00798



**CONSTRUCTION PROCEDURES**

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
  - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
  - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
  - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.
- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
  - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
  - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**LOCATION OF STRUCTURES:**

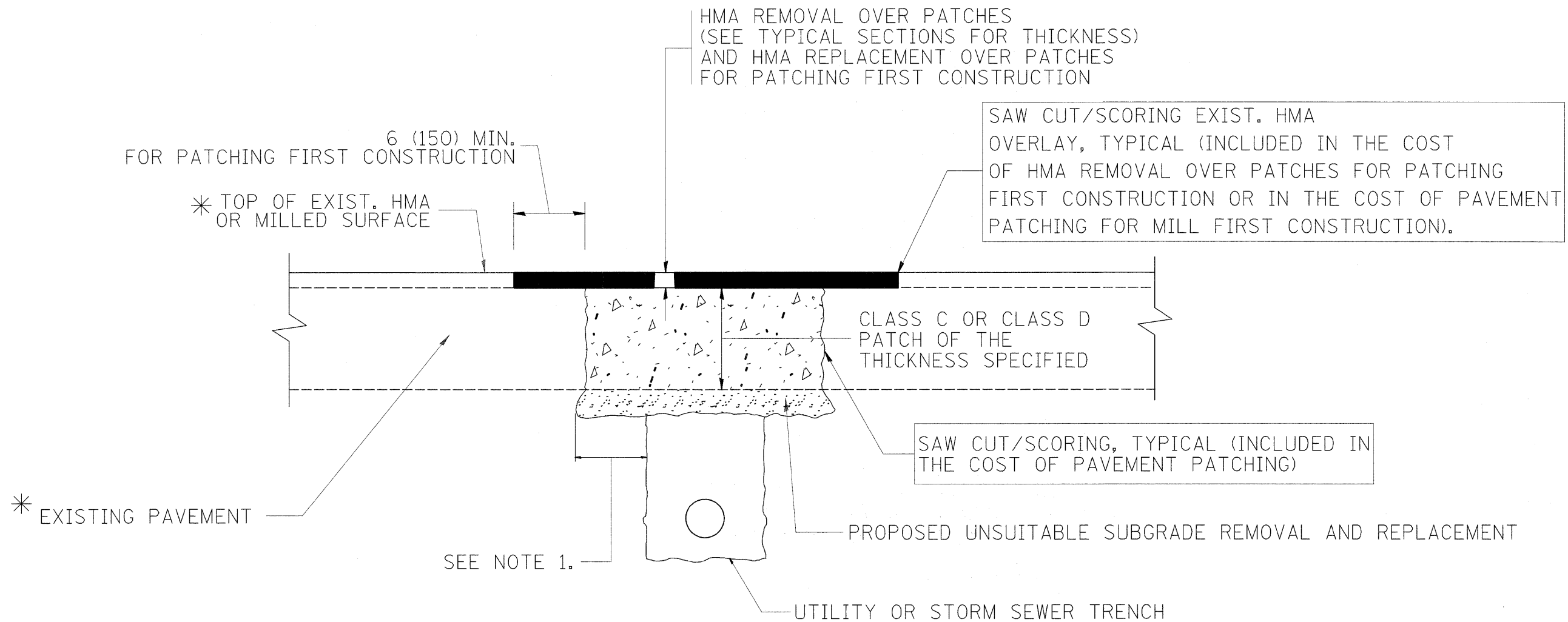
THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:** THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL" NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = shiransb	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>			F.A.P. RTE. 362	SECTION 0608 RS-5	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 13
et\pwork\PMIDOT\SHIRANISB\0177349\01st.dgn	PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	<b>BD600-03 (BD-8)</b>		CONTRACT NO. 60J98	
	PLOT DATE = 2/4/2010	CHECKED -	REVISED - R. WIEDEMAN 05-14-04									
		DATE - 10-25-94	REVISED - R. BORO 01-01-07									



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = shiransab	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			F.A.P. RTE. 362	SECTION 0608 RS-5	COUNTY COOK	TOTAL SHEETS 25	SHEET NO. 14
ca\pwork\PIWIDOT\SHIRANISB\02177349\Di\Std.dgn		DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	<b>BD400-04 (BD-22)</b>		CONTRACT NO. 60J98		
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - R. BORO 09-04-07		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
	PLOT DATE = 2/4/2010	DATE - 10-25-94	REVISED - K. ENG 10-27-08									

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001  
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

1/4" (5) \*\*

18" (450) MAX.

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

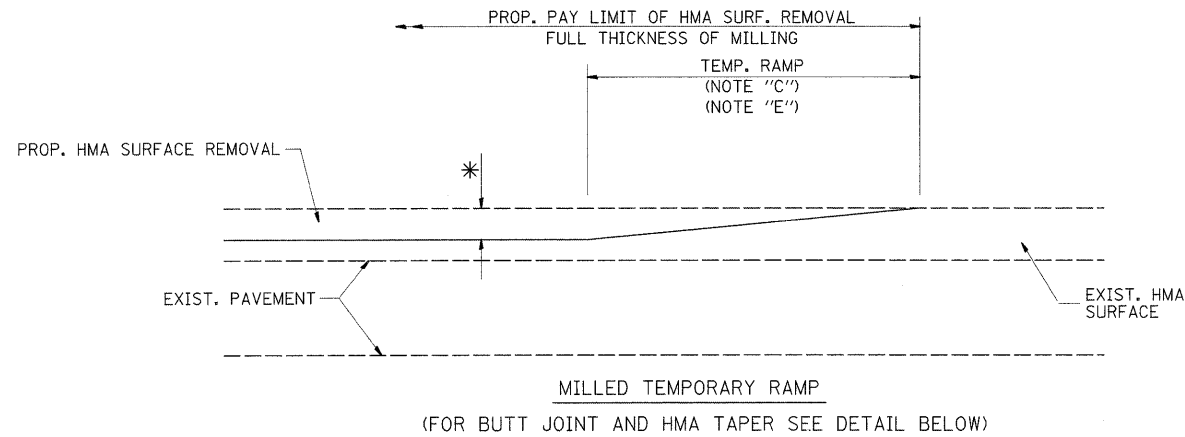
\*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY. SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

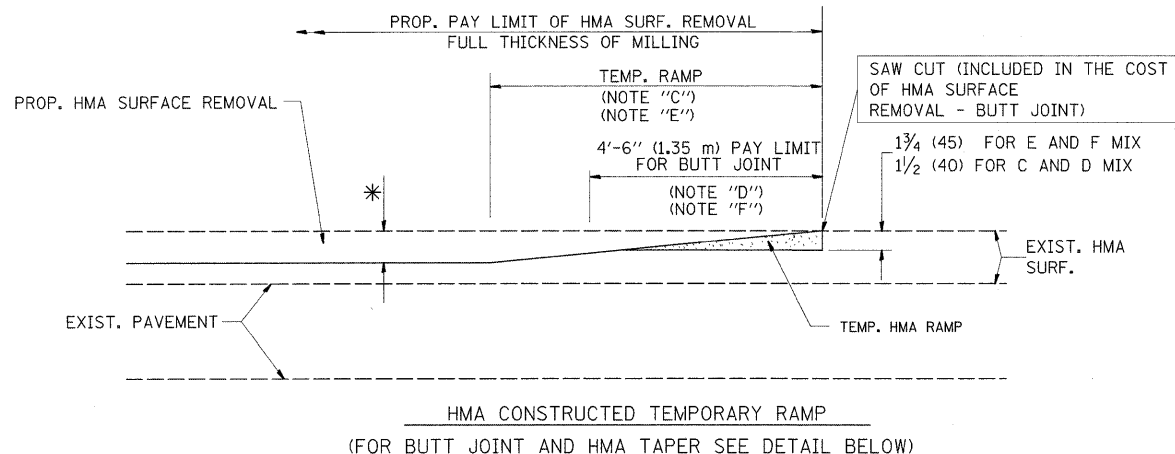
# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = shurenab	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et\pwork\PWIDOT\SHIRANISB\0177349\01std.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97		362	0608 RS-5	COOK	25	15			
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED - M. GOMEZ 01-22-01		<b>BD600-06 (BD-24)</b>			CONTRACT NO. 60J98				
PLOT DATE = 2/4/2010		DATE - 03-11-94	REVISED - R. BORO 12-15-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

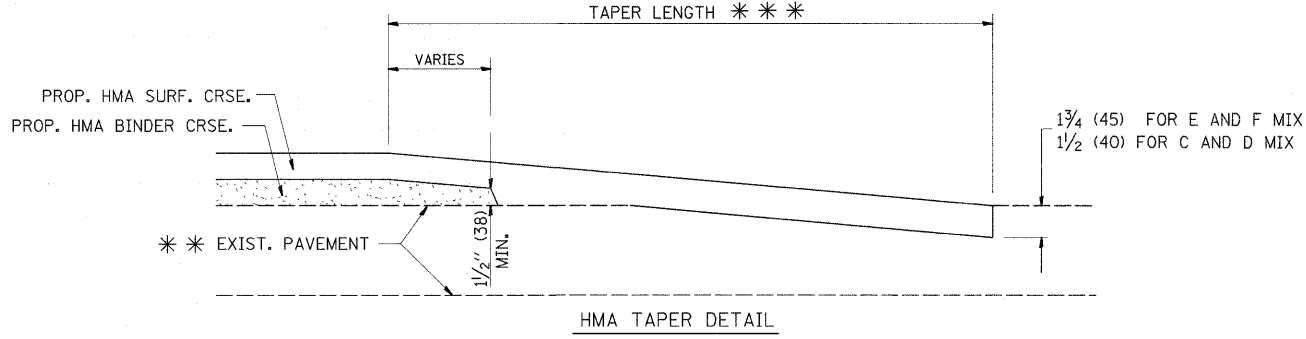
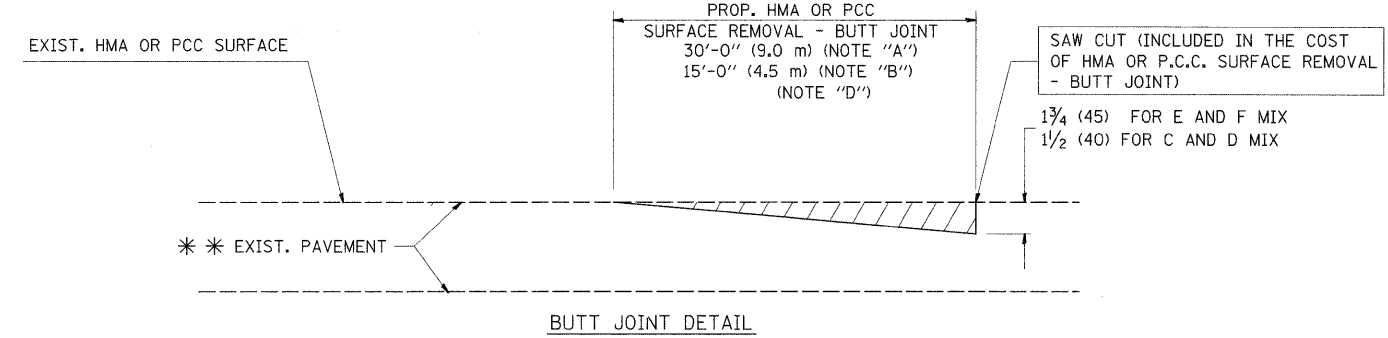


**OPTION 1**



**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

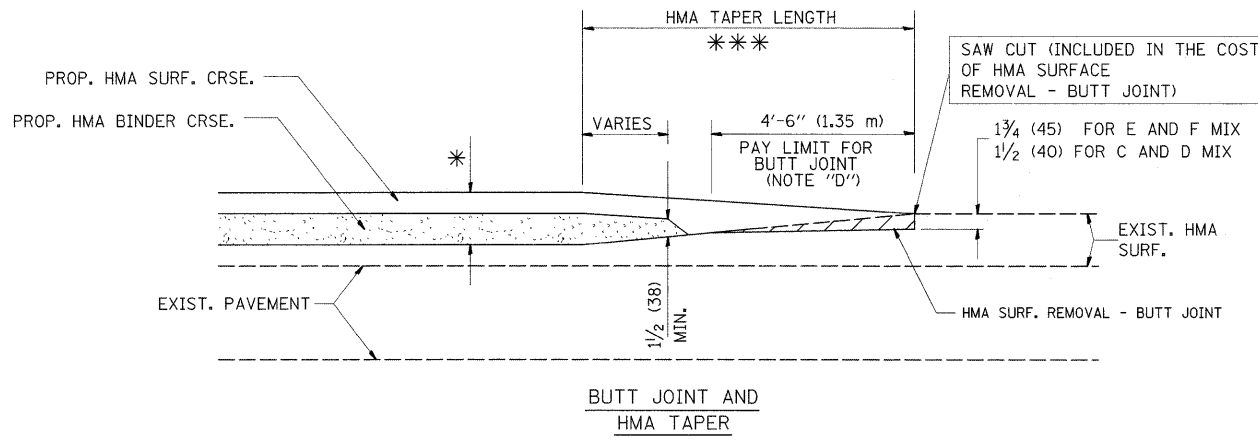
**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

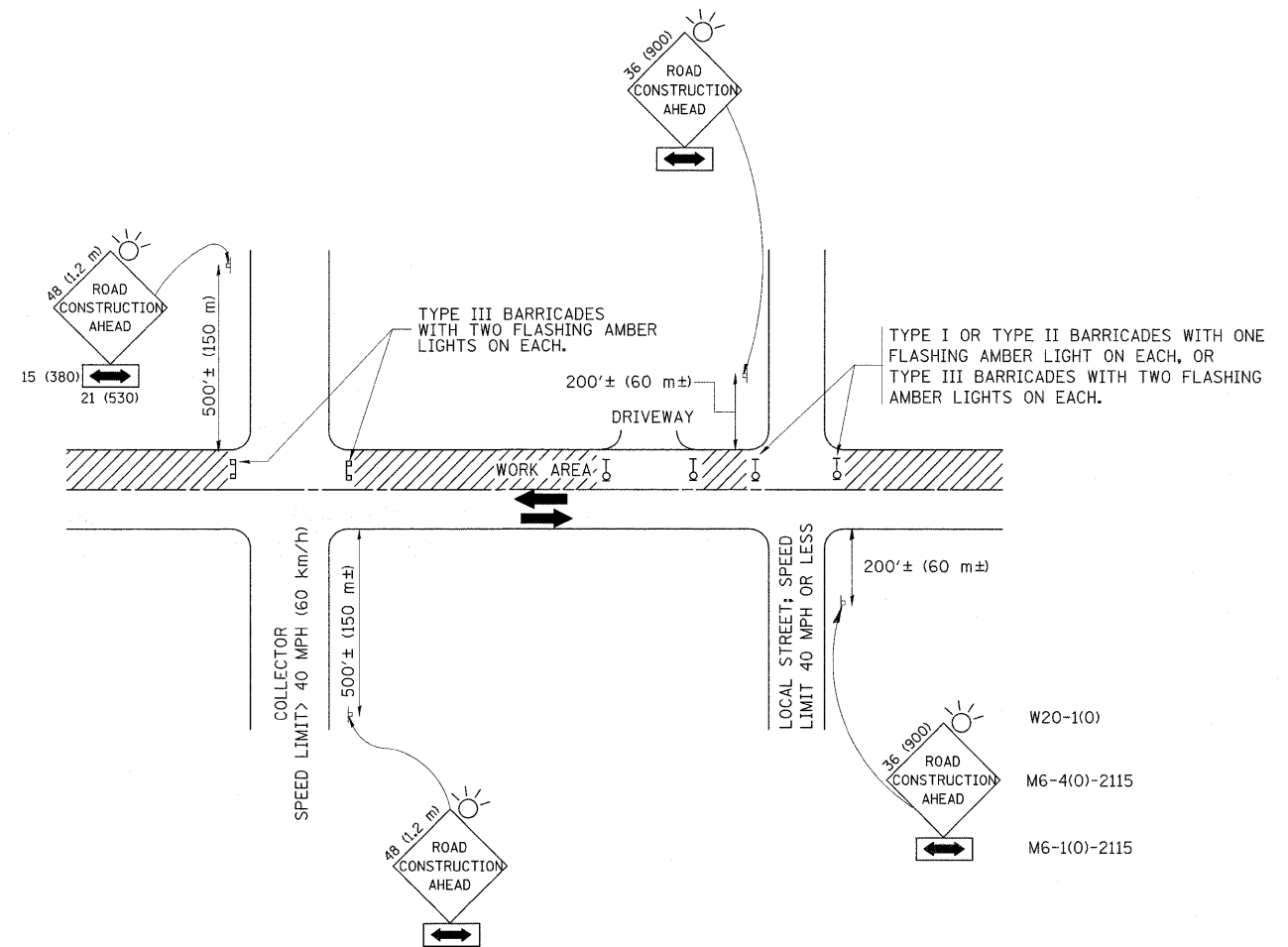


**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

FILE NAME =	USER NAME = shiransb	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BUTT JOINT AND HMA TAPER DETAILS</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pw\work\PIWIDOT\SHIRANISB\0177349\01st.dgn	std.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97		362	0608 RS-5	COOK	25	16			
PLOT SCALE = 50.0000 ' / IN.		CHECKED -	REVISED - M. GOMEZ 04-06-01		<b>BD400-05 BD32</b>			<b>CONTRACT NO. 60J98</b>				
PLOT DATE = 2/4/2010		DATE - 06-13-90	REVISED - R. BORO 01-01-07		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.					







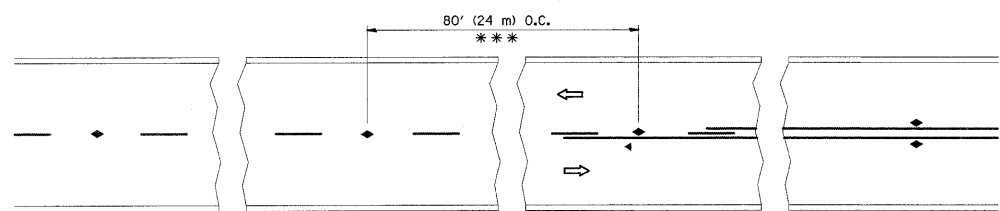
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
  - USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
  - C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
  - D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.
- 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

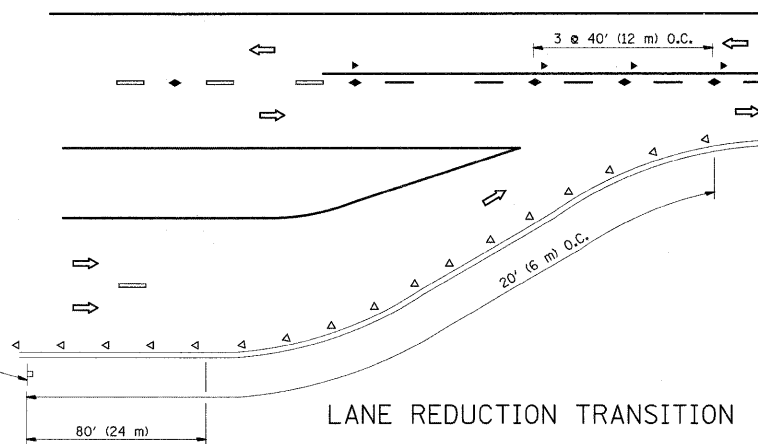
All dimensions are in millimeters (Inches) unless otherwise shown.

FILE NAME =	USER NAME = shiransab	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ea\pwork\p\WIDOT\SHIRANISB\d0177349\01.kStd.dgn		DRAWN -	REVISED - A. HOUSEH 03-06-96		362	0608 RS-5	COOK	25	18			
PLOT SCALE = 50,0000 ' / IN.		CHECKED -	REVISED - A. HOUSEH 10-15-96		<b>TC-10</b>			<b>CONTRACT NO. 60J98</b>				
PLOT DATE = 2/4/2010		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

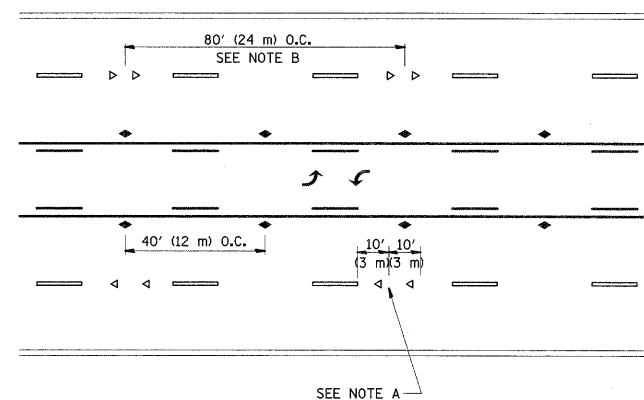


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

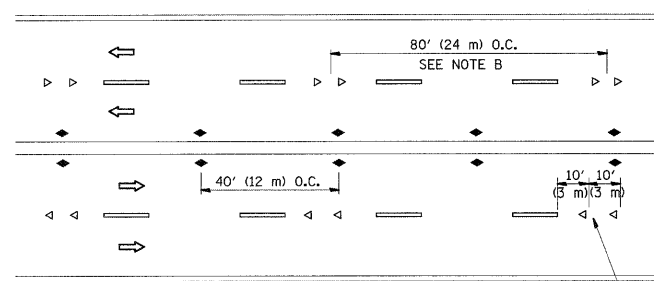
TWO-LANE/TWO-WAY



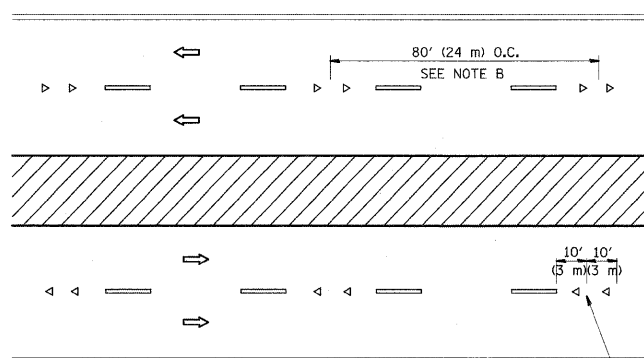
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

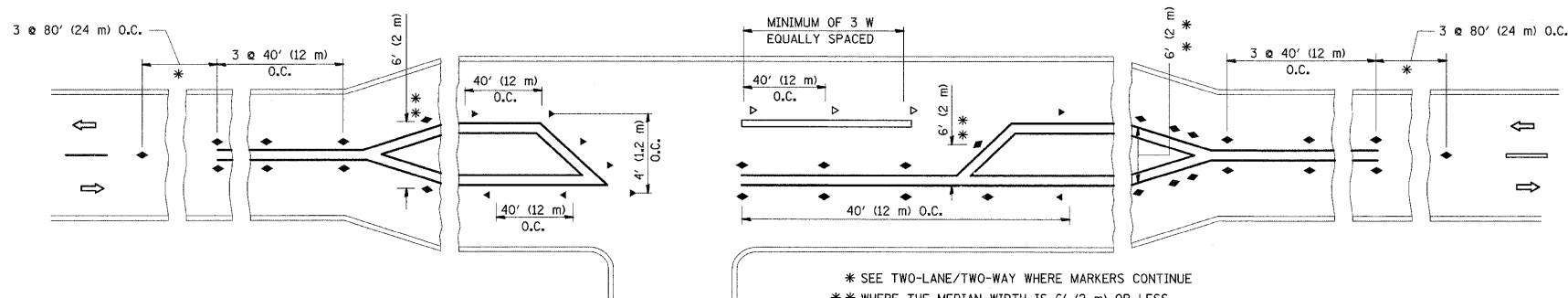
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

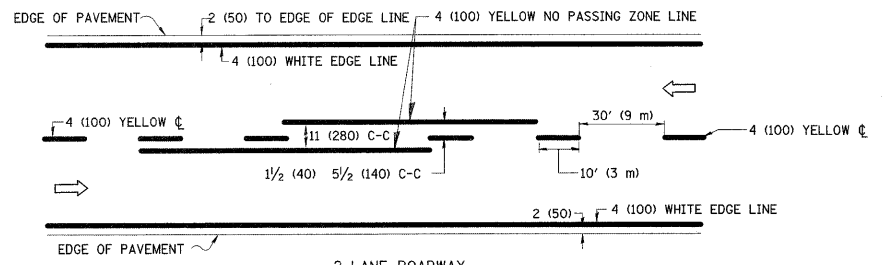
1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



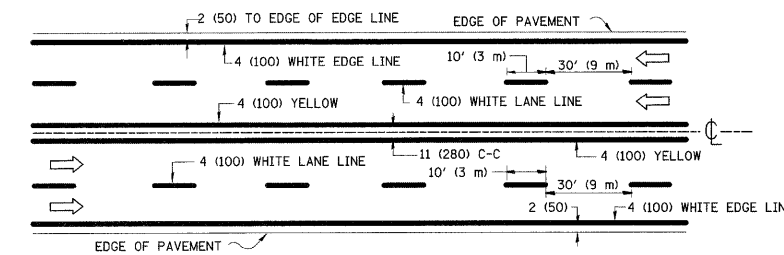
LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

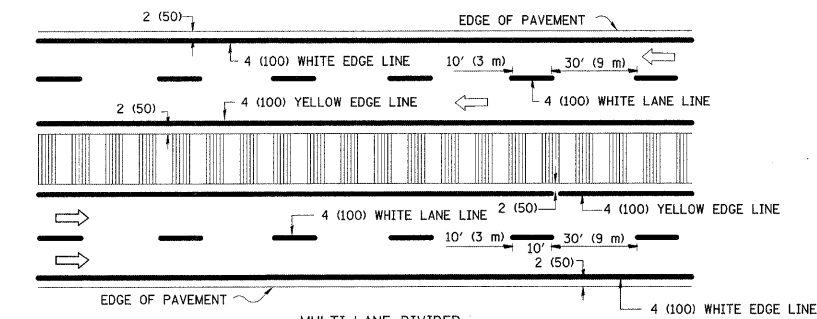
FILE NAME =	USER NAME = shiransb	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL APPLICATIONS</b>		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pwwork\pwwid\T\SHIRANISB\0177349\01	Std.dgn	DRAWN -	REVISED - T. RAMMACHER 03-12-99		362	0608 RS-5	COOK	25	19		
	PLOT SCALE = 50,0000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00		<b>RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)</b>		<b>TC-11</b>		CONTRACT NO. 60J98		
	PLOT DATE = 2/4/2010	DATE -	REVISED - C. JUCIUS 09-09-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



2-LANE ROADWAY



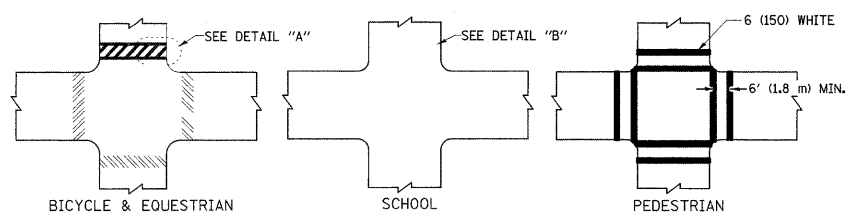
MULTI-LANE UNDIVIDED



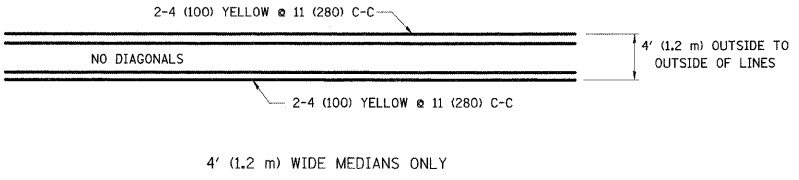
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

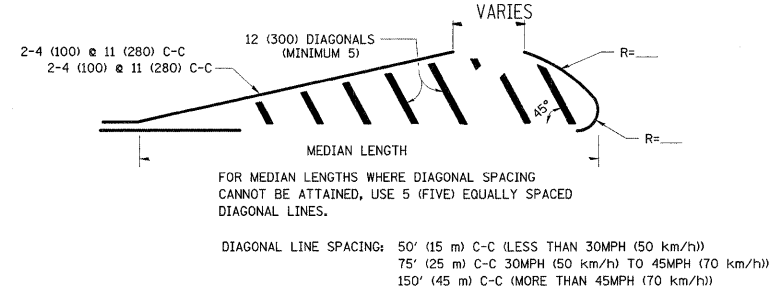
TYPICAL LANE AND EDGE LINE MARKING



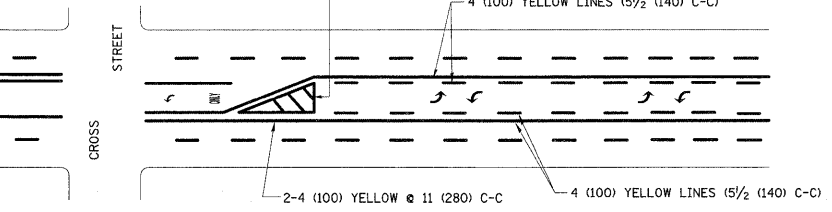
TYPICAL CROSSWALK MARKING



4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE

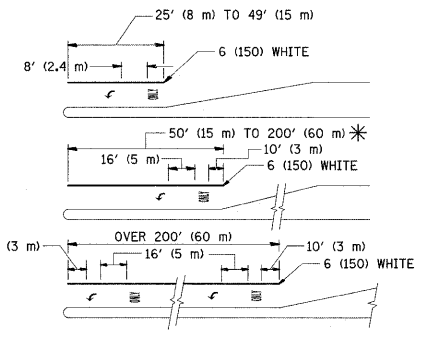


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

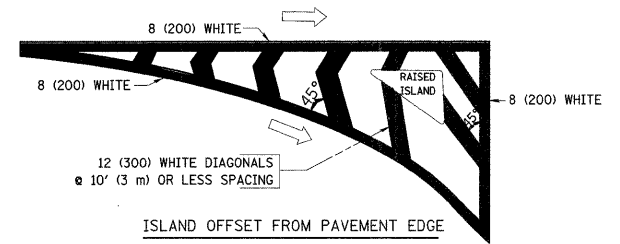


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

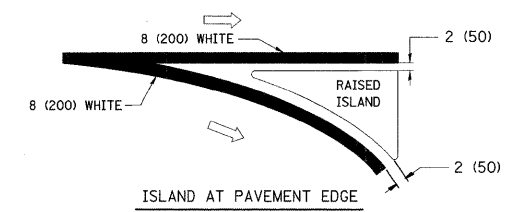
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



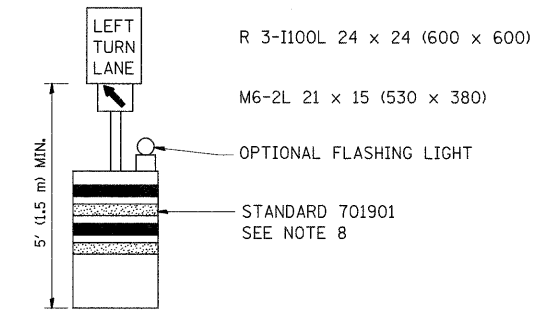
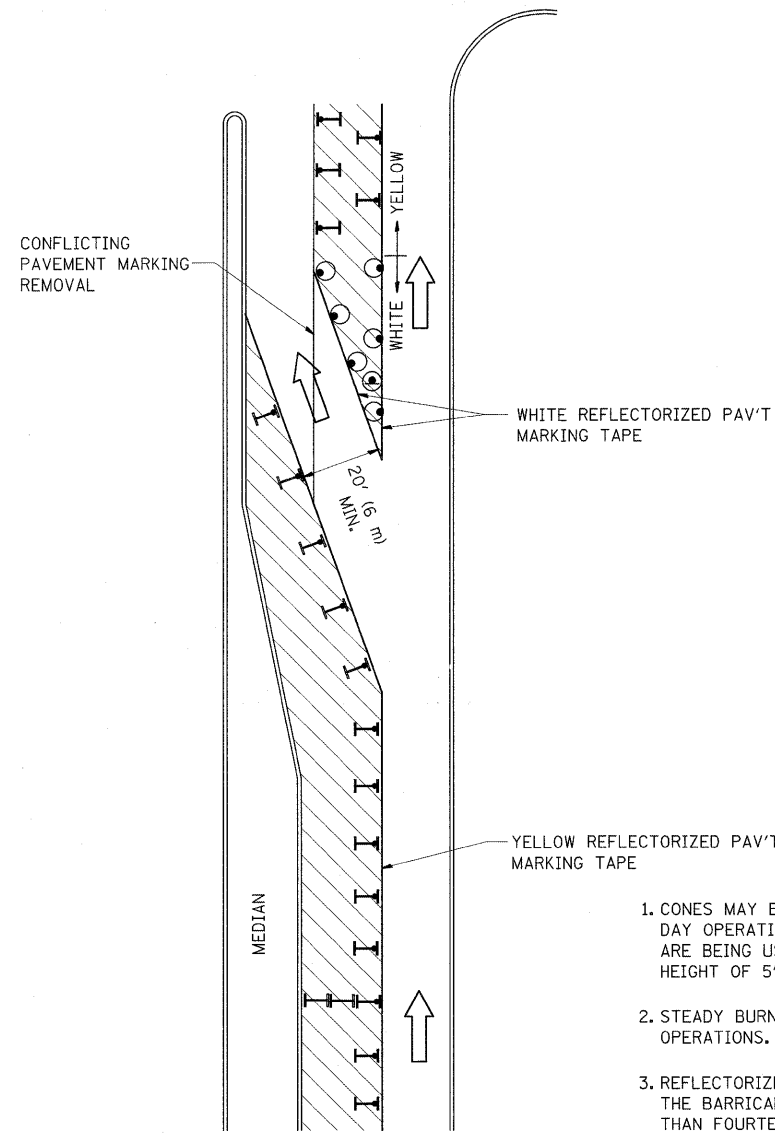
ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

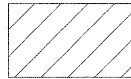







**GENERAL NOTES**

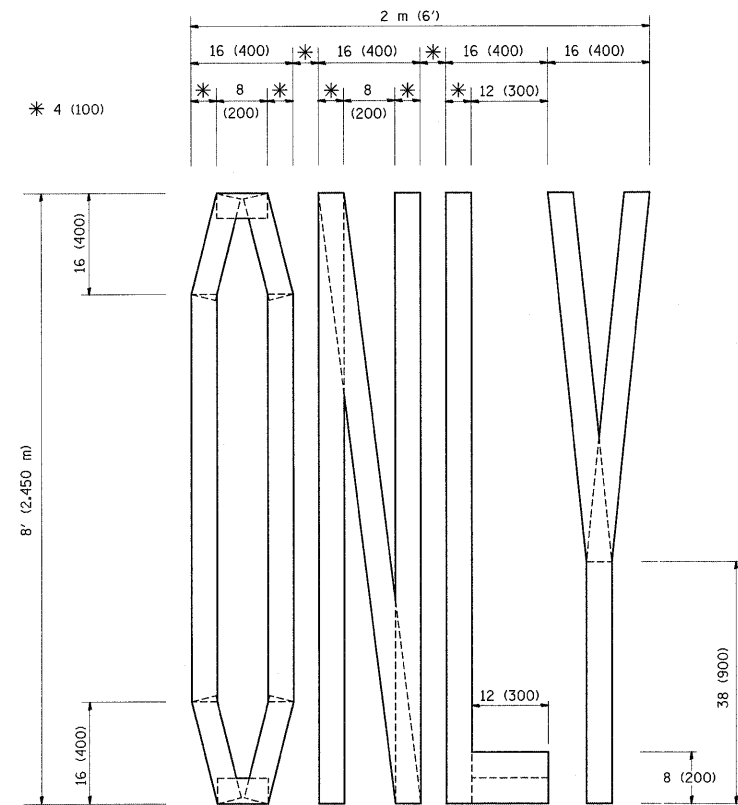
1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHR 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

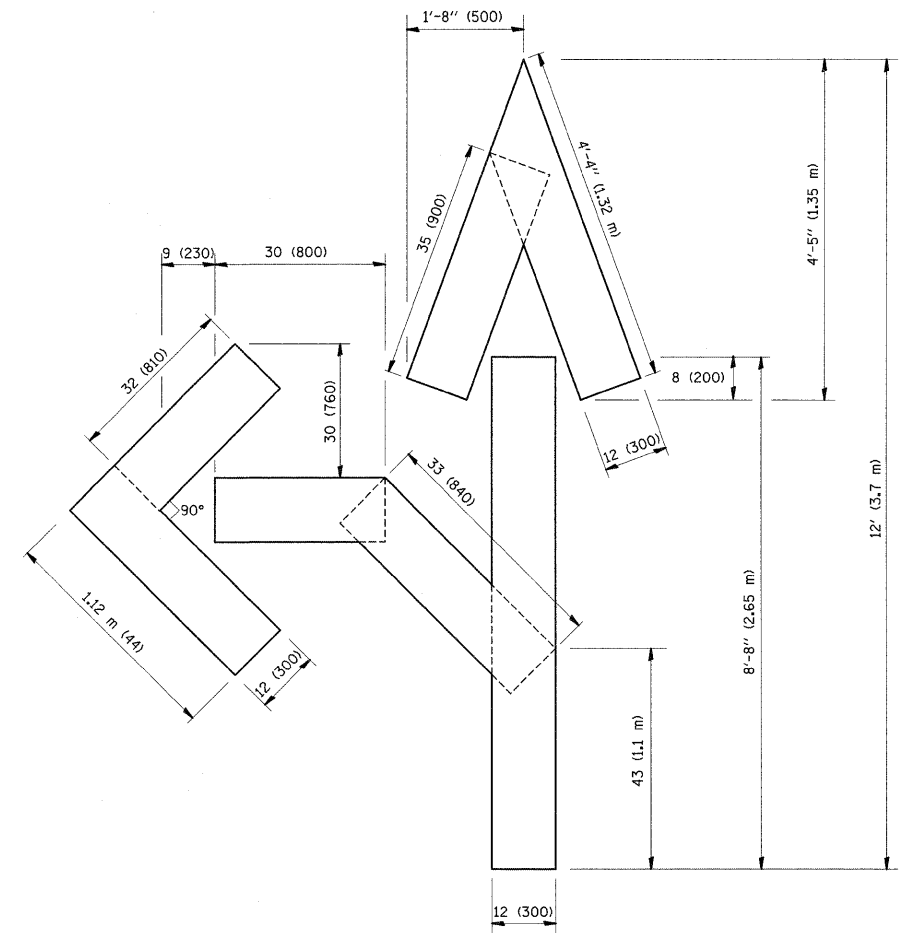
**LEGEND**

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

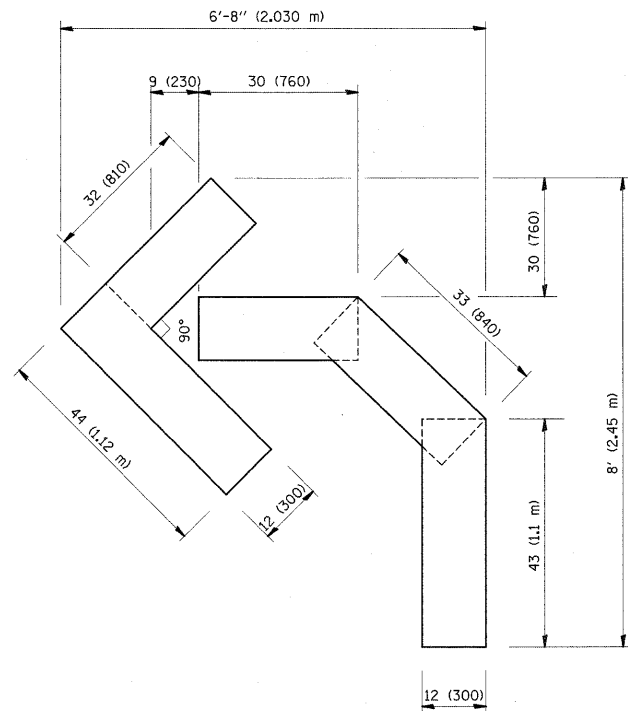
FILE NAME =	USER NAME = shiranisb	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
st\pw_work\PWIDOT\SHIRANISB\0177349\DistStd.dgn	PLOT SCALE = 50.0000' / IN.	REVISED - A. HOUSEH 11-07-95	REVISED -			362	0608 RS-5	COOK	25	21
PLOT DATE = 2/4/2012	REVISED - A. HOUSEH 10-12-96	REVISED -	REVISED -			<b>TC-14</b>		CONTRACT NO. 60J98		
	REVISED - T. RAMMACHER 01-06-00	REVISED -	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS		STA.	TO STA.	



QUANTITY  
 4 (100) LINE = 64.1 ft. (19.7 m)  
 21.1 sq. ft. (1.97 sq. m)



QUANTITY  
 4 (100) LINE = 82.5 ft. (25.3 m)  
 27.5 sq. ft. (2.53 sq. m)



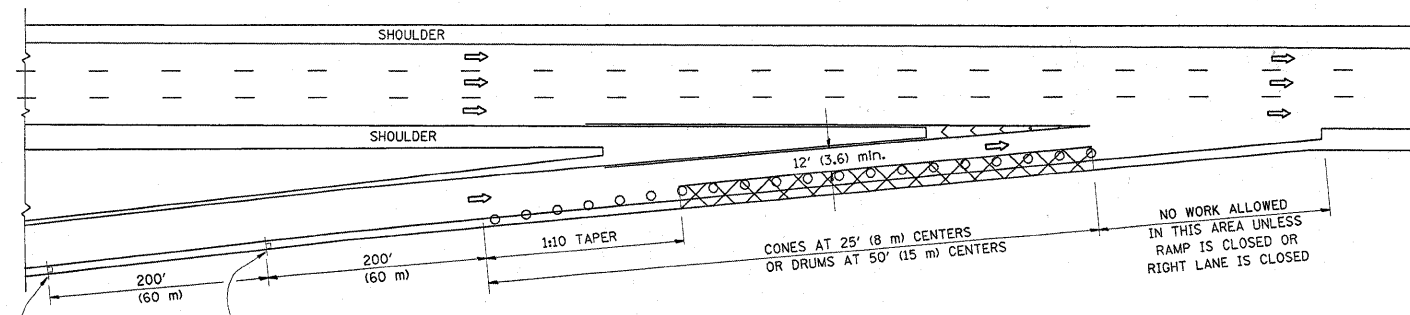
QUANTITY  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in Inches (millimeters) unless otherwise shown.

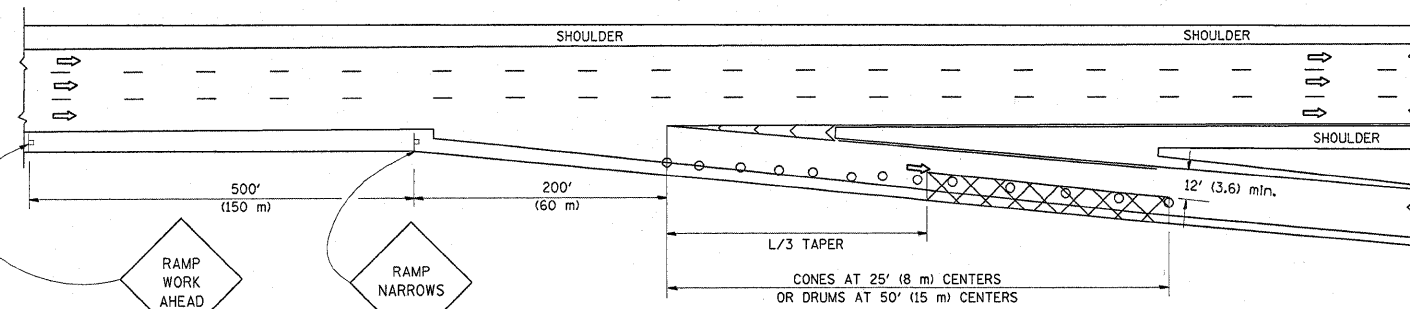
FILE NAME =	USER NAME = shiransab	DESIGNED -	REVISED -T. RAMMACHER 06-05-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
est\pw_work\PWIDOT\SHIRANISB\80177349\Di+Std.dgn	PLOT SCALE = 50.0000 ' / IN.	DRAWN -	REVISED -T. RAMMACHER 11-04-97		362	0608 RS-5	COOK	25	22			
PLOT DATE = 2/4/2018	DATE = 09-18-94	CHECKED -	REVISED -T. RAMMACHER 03-02-98		<b>TC-16</b>			<b>CONTRACT NO. 60J98</b>				
		DATE = 09-18-94	REVISED -E. GOMEZ 08-28-00		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

PARTIAL RAMP CLOSURE DETAILS

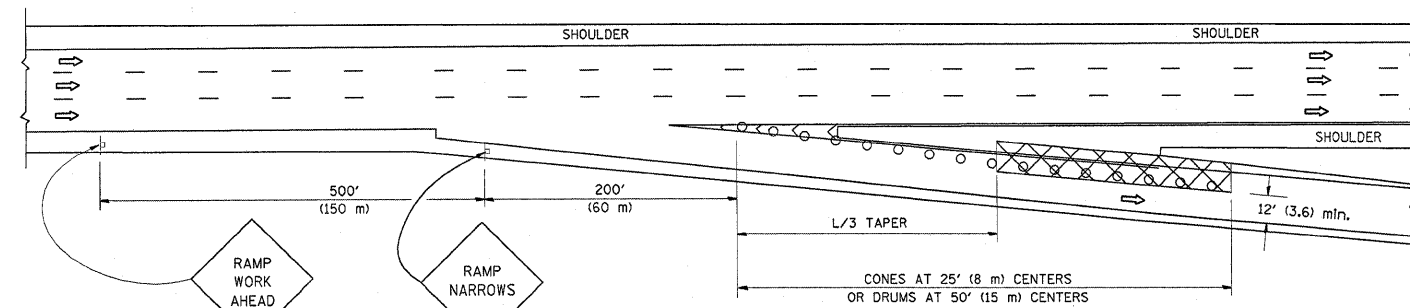
SHOULDER CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

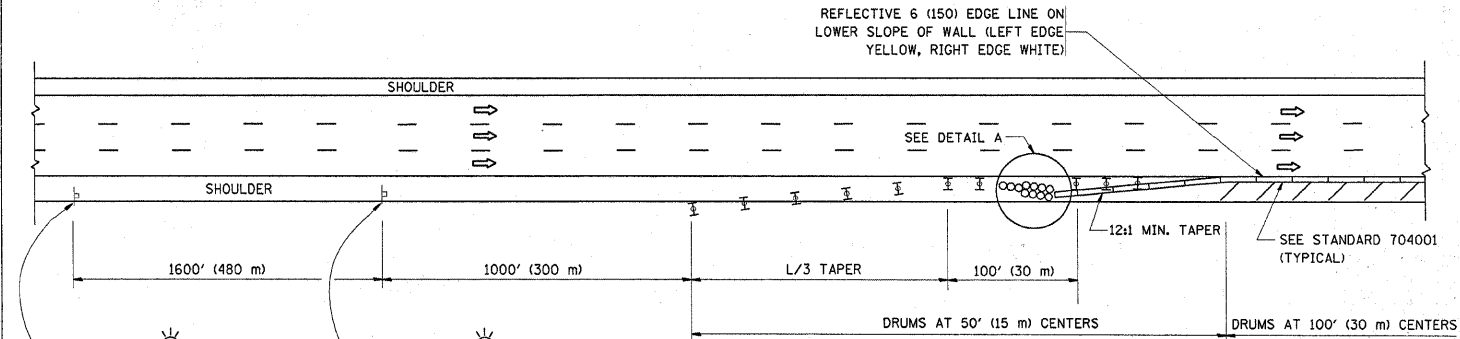
SYMBOLS

- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE

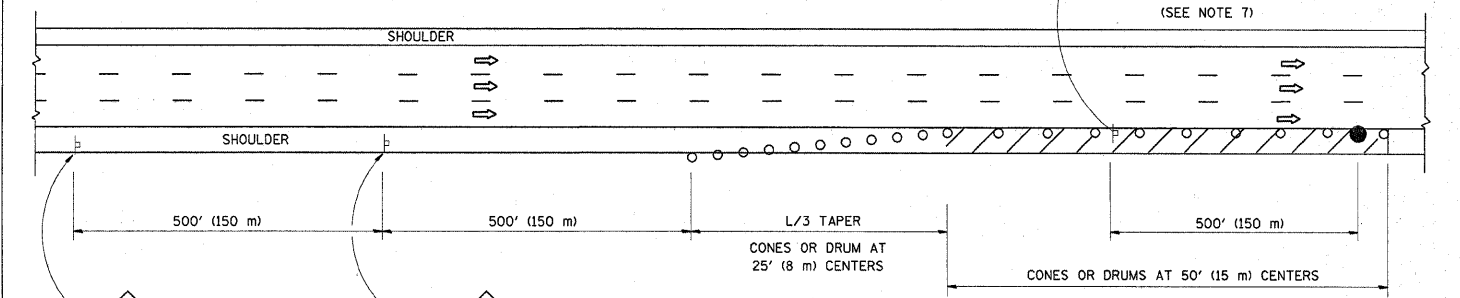
GENERAL NOTES

1. THE "L" DISTANCE EQUALS:
 

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC    ENGLISH
	$L=0.65(W)(S)$ $L=(W)(S)$
	W = WIDTH OF OFFSET IN FEET (METERS)
	S = NORMAL POSTED SPEED MPH (KM/H)
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.
5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
  - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
  - b. THE WORK AVTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.

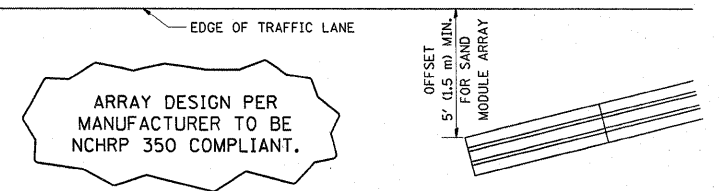


PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

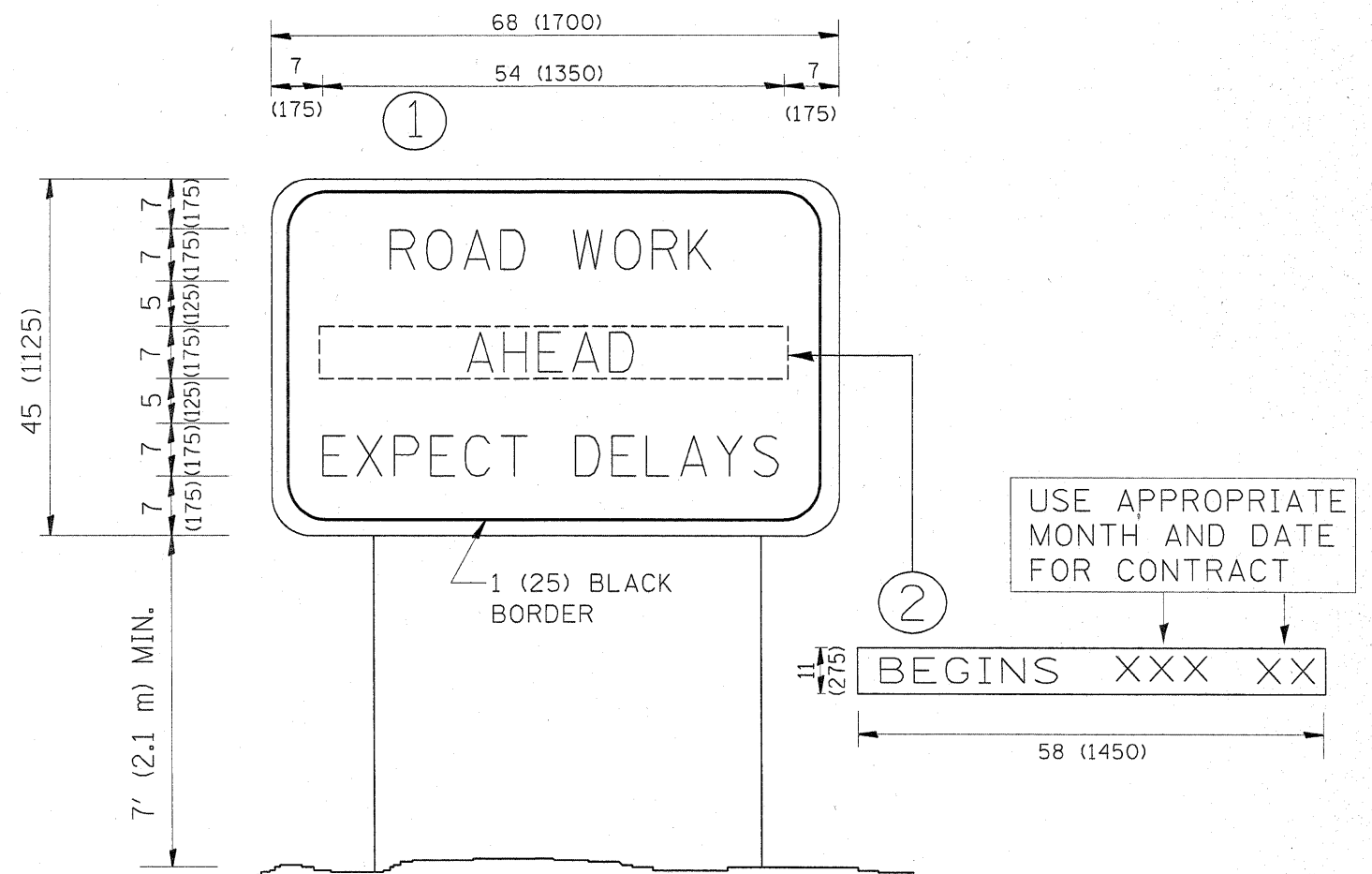
THIS DETAIL IS USED WHERE:  
 1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCROACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



DETAIL "A"  
 IMPACT ATTENUATOR, TEMPORARY  
 (SEE NOTE 5)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = shuraniab	DESIGNED -	REVISED - 04-03	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ct:\pw_work\PW1001\SHIRANISB\08177349\01.dwg	std.dgn	DRAWN - D.W.S.	REVISED - J.A.F. 12-06			362	0608 RS-5	COOK	25	23	
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED - S.P.B. 01-07			<b>TC-17</b>		CONTRACT NO. 60J98			
PLOT DATE = 2/3/2010		DATE - 11-96	REVISED - S.P.B. 12-09			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

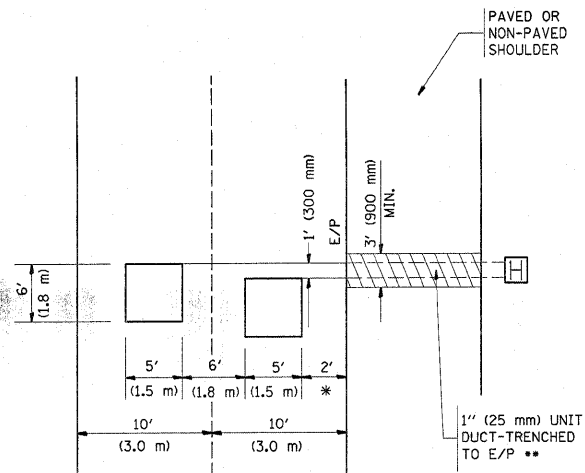
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = shiraniab	DESIGNED -	REVISED - R. MIRS 09-15-97	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARTERIAL ROAD INFORMATION SIGN</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ci:\pw_work\FW\DOT\SHIRANIAB\0177349\01-std.dgn	DRAWN -	REVISED - R. MIRS 12-11-97	362			0608 RS-5	COOK	25	24		
PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99	<b>TC-22</b>			CONTRACT NO. 60J98					
PLOT DATE = 2/3/2010	DATE -	REVISED - C. JUCIUS 01-31-07	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT								
						SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		



**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

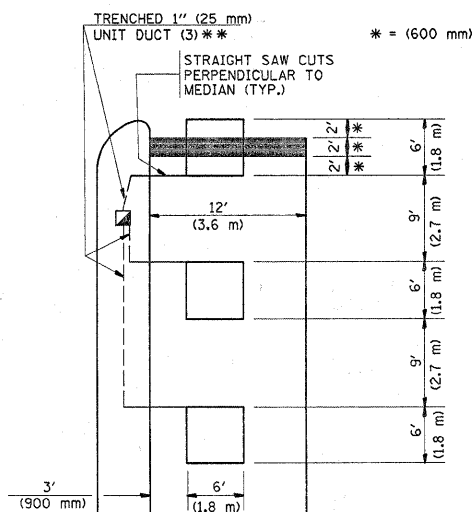


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)**

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

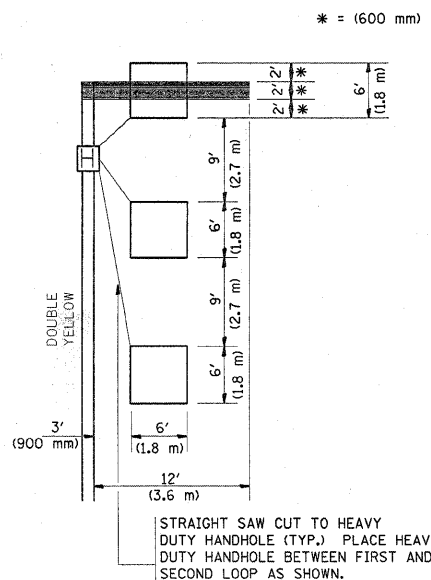


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

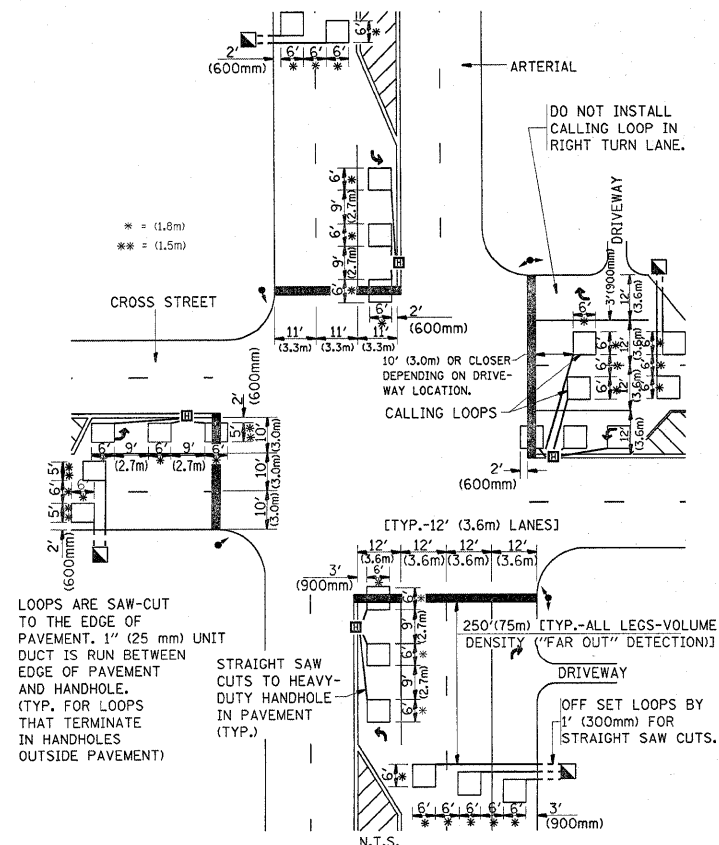
**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)**



\* = (600 mm)

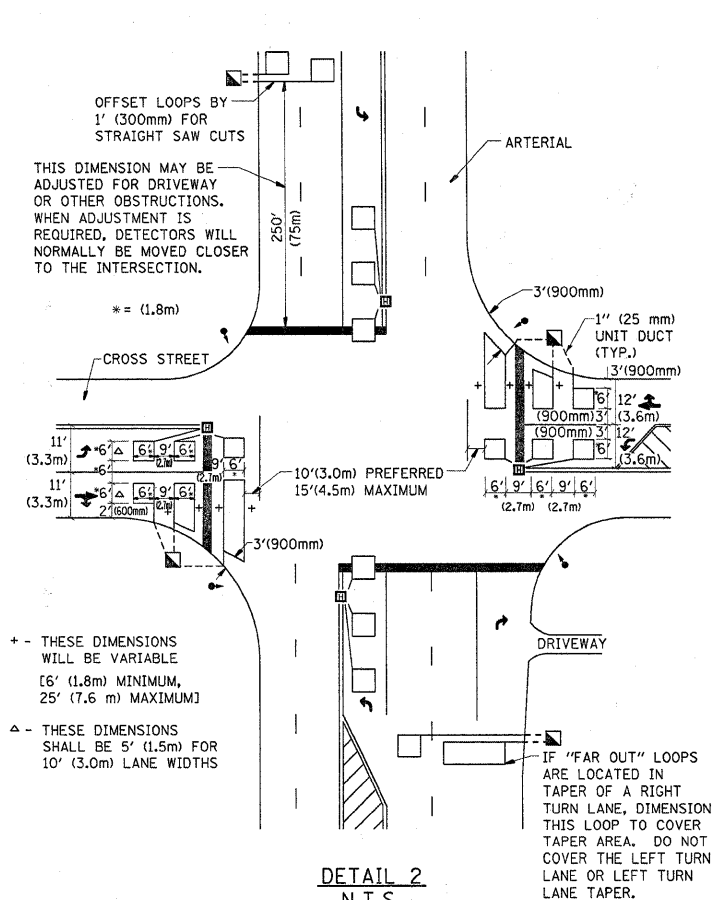
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1  
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2  
N.T.S.**

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = shiranisb	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pwork\PW\IDOT\SHIRANISB\0177349\Dist	Std.dgn	DRAWN -	REVISED -			362	0608 RS-5	COOK	25	25
PLOT SCALE = 50.0000 / IN.		CHECKED - R.K.F.	REVISED -			<b>TS-07</b>		<b>CONTRACT NO. 60J98</b>		
PLOT DATE = 2/3/2010		DATE -	REVISED -			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	
						FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT				