FOR INDEX OF SHEETS, SEE SHEET NO. 2

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

PROJECT LOCATED IN THE VILLAGE OF LOMBARD

FOR INDEX OF HIGHWAY STANDARDS, SEE SHEET NO. 2

ST. CHARLES ROAD (FAU 1397) IL ROUTE 53 (COLUMBINE AVENUE) TO GRACE STREET / RESURFACING

TRAFFIC DATA ST.CHARLES ROAD

POSTED AND DESIGN SPEED **WESTBOUND**

IL ROUTE 53 TO WEST ROAD = 35 MPH CRESCENT BOULEVARD TO GARFIELD STREET = 25 MPH GARFIELD STREET TO MARTHA STREET = 30 MPH **EASTBOUND**

IL 53 TO CRESCENT BOULEVARD = 35 MPH CRESCENT BOULEVARD TO GARFIELD STREET = 25 MPH GARFIELD STREET TO MARTHA STREET = 30 MPH 2008 ADT = 16,000 VPD

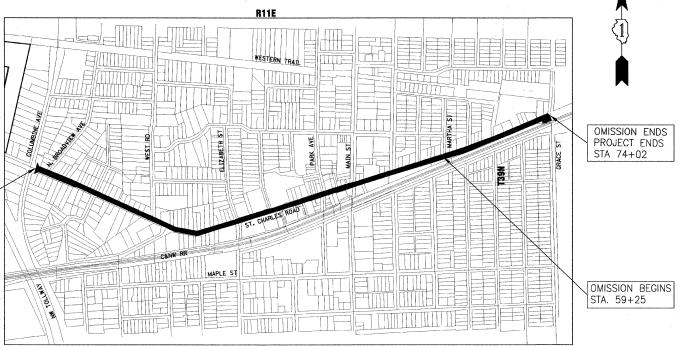
DESIGN DESIGNATION COLLECTOR

BAXTER & WOODMAN, INC. STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM LICENSE NO. 184-001121 EXPIRES 4/30/2011

PROJECT BEGINS STA. 1+00

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED SIZE PLANS, THE ABOVE SCALES SHALL BE USED.

PROJECT NO.: M-ARA-9003 (421) **SECTION NO.: 09-00153-00-RS** JOB NO.: C-91-810-09 **VILLAGE OF LOMBARD. ILLINOIS DUPAGE COUNTY**



R11E 3rd PRIME MERIDIAN YORK TOWNSHIP LOCATION MAP **NOT TO SCALE**

GROSS LENGTH OF IMPROVEMENT = 7303 LIN.FT. OR 1.383 MILES NET LENGTH OF IMPROVEMENT = 5825 LIN. FT. OR 1.103 MILES



Consulting Engineers Crystal Lake, Illinois

815.459.1260

Grayslake, Illinois 847.223.5088

Itasca, Illinois

Plainfield, Illinois

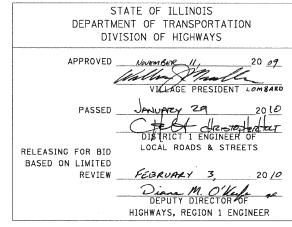
815.609.7425

11-30-2011" "LICENSE EXPIRES

1397 09-00153-00-RS DuPAGE 36 ILLINOIS PROJECT C-91-81(-09

CONTRACT NO. 63351





PRINTED BY THE AUTHORITY **OF THE STATE OF ILLINOIS**

CONTRACT NO. 63351

Mokena, Illinois

Madison, Wisconsin

608.241.5481

708.478.2090

Chicago, Illinois 312.578.0500

62-056222

PROFFSSIONAL

(TRAFFIC SIGNAL PLANS)

PROJECT NO: 090317.40 DATE: 06-26-09

J.U.L.I.E. DESIGN STAGE REQUEST DIG. No. A1552018

CONTACT JULIE AT 811 OR 800-892-0123 WITH THE FOLLOWING:

COUNTY = DuPAGE CITY-TWISHP = LOMBARD-YORK TOWNSHIP SEC. & 1/4 SEC. NO. = 07-08 48 HOURS(2 working days)BEFORE YOU DIG

DeKalb, Illinois 815.787.3111

Burlington, Wisconsin

262.763.7834

630.773.1870

62~050844

PROFESSIONAL

INDEX OF SHEETS

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2	INDEX OF SHEETS, HIGHWAY STANDARDS, SUMMARY OF QUANTITIES
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HIGHWAY STANDARDS

VILLAGE OF LOMBARD STANDARD DRAWINGS

000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS	DRAWING NO.	TITLE
280001-05	TEMPORARY EROSION CONTROL SYSTEMS	PAVEMENT 2	SIDEWALK CROSS SECTION
424001-05	CURB RAMPS FOR SIDEWALKS	PAVEMENT 4	SIDEWALK CONSTRUCTION
442201-03	CLASS C AND D PATCHES	PAVEMENT 5	TYPICAL PAVEMENT CROSS-SECTION
602301-02	INLET - TYPE A	PAVEMENT 6	RESIDENTIAL DRIVEWAY APRON
602401-02	MANHOLE, TYPE A	PAVEMENT 7	COMMERCIAL DRIVEWAY APRON
602701-02	MANHOLE STEPS	PAVEMENT 8	CURB AND GUTTER
604001-03	FRAMES AND LIDS, TYPE 1	PAVEMENT 9	STORM SEWER INLET CURB AND GUTTER
604006-04	FRAME AND GRATE, TYPE 3	STORM 2	INLET TYPE A
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER	STORM 7	RIM ADJUSTMENTS PAVED AREA
		STORM 8	RIM ADJUSTMENTS IN CURB LINE
701501-05	URBAN LANE CLOSURE, 2L, 2W UNDIVIDED	STORM 12	GRATE BOX INLET
701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION	SANITARY 1	SANITARY MANHOLE
701801-04	LANE CLOSURE, MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE		
701901-01	TRAFFIC CONTROL DEVICES		
886001-01	DETECTOR LOOP INSTALLATIONS		
886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS		

SUMMARY OF QUANTITIES

PAY ITEM NO.	PAY ITEM DESCRIPTION	UNIT	TOTAL QUANT 1000
20101100	TREE TRUNK PROTECTION	EACH	75
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	18
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	45
25200200	SUPPLEMENTAL WATERING	UNIT	2
25200700	SODDING, SPECIAL	SQ YD	45
28000510	INLET FILTERS	EACH	55
31101100	SUB-BASE GRANULAR MATERIAL, TYPE B	CU YD	18
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	30
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	3,168
40600300	AGGREGATE (PRIME COAT)	TON	64
40600400	MIXTURE FOR CRACKS, JOINTS AND FLANGEWAYS	TON	5
40600826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL 4.75 N50	TON	1,049
40600895	CONSTRUCTING TEST STRIP	EACH	1
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	209
40603340	HMA SURFACE COURSE, MIX "D", N70	TON	3,154
42001300	PROTECTIVE COAT	SQ YD	1,699
42400430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL	SQ FT	8,259
42400800	DETECTABLE WARNINGS	SQ FT	422
44000155	HMA SURFACE REMOVAL, 1 1/2"	SQ YD	11,706
44000162	HMA SURFACE REMOVAL, 3 1/4"	SQ YD	19,977
44000600	SIDEWALK REMOVAL	SQ FT	8,259
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1,507
44201789	CLASS D PATCHES, TYPE II, 12 INCH	SQ YD	208
44201794	CLASS D PATCHES, TYPE III, 12 INCH	SQ YD	795
44201796	CLASS D PATCHES, TYPE IV, 12 INCH	SQ YD	2,106
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	7,000
56500600	DOMESTIC WATER SERVICE BOXES TO BE ADJUSTED	EACH	9
60250200 60250300	CATCH BASINS TO BE ADJUSTED CATCH BASINS TO BE ADJUSTED (SPECIAL)	EACH	<u>6</u> 4
	MANHOLES TO BE ADJUSTED (SPECIAL)	EACH	
60255500 60255600	MANHOLES TO BE ADJUSTED (SPECIAL)	EACH EACH	9 7
60260100	INLETS TO BE ADJUSTED		12
60260200	INLETS TO BE ADJUSTED (SPECIAL)	EACH EACH	25
60262700	INLETS TO BE RECONSTRUCTED	EACH	23
60265800	VALVE VAULTS TO BE ADJUSTED (SPECIAL)	EACH	23
60266610	VALVE BOXES TO BE ADJUSTED (SPECIAL)	EACH	<u></u>
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	42
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	2
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	50
60500040	REMOVING MANHOLES	EACH	1
67100100	MOBILIZATION	L SUM	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
78003100	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LETTERS AND SYMBOLS	SQ FT	250
78003110	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 4"	FOOT	14,043
78003130	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 6"	FOOT	3,013
78003150	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 12"	F00T	643
78003180	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 24"	F00T	454
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	4
88600600	DETECTOR LOOP REPLACEMENT	FOOT	75
89502200	MODIFY EXISTING CONTROLLER	EACH	4
89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	2,599
X0321556	SANITARY MANHOLES TO BE ADJUSTED	EACH	15
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	51
XX003435	PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD	160
XX003668	PRECONSTRUCTION VIDEO TAPING	L SUM	1
XX005723	VIDEO DETECTION SYSTEM, COMPLETE INTERSECTION	EACH	44
XX005972	STAMPED ASPHALT CROSSWALK	SQ YD	234
XX007246	ELECTRIC CABLE IN CONDUIT NO. 18 3C	FOOT	2,327
XX007509	REMOVE AND REPLACE SANITARY MANHOLES, 4' DIAMETER, 0-8 FEET DEEP	EACH	10
XX007510	REMOVE AND REPLACE SANITARY MANHOLES, 4' DIAMETER, 8-12 FEET DEEP	EACH	9
XX008264	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (SPECIAL)	FOOT	1,285

^{*} DENOTE SPECIALTY ITEM

DESIGNED - TMS	REVISED - PER VILLAGE REVIEW 09-09-09
DRAWN - KAR	REVISED - PER VILLAGE REVIEW 09-16-09
CHECKED -	REVISED - PER IDOT REVIEW 09-17-09
DAIL - 06-26-09	REVISED - PER IDOT FINAL COMMENTS 01-18-10

INDEX OF SHEETS. I	HIGHWAY STANDARDS, SUMM	ARY OF QUANTITIES	F.A. P. RTE.	SECT	ION	COUNTY	TOTAL SHEETS	SHEET NO.
		•	1397	09-0015	3-00-RS	DuPAGE	36	2
	*					CONTRACT	NO. 63	3351
.E:	PROJECT NO: 090317 STA.	TO STA.	FED. ROA	ND DIST, NO. 1	ILLINOIS FED. AID	PROJECT M-ARA-	9003(421)	

^{**} INDICATES ITEM COVERED BY SPECIAL PROVISIONS

- ALL REFERENCES TO STANDARD SPECIFICATIONS IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, JANUARY 1, 2007 AND THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2010.
- 2. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" MAY 1996 FIFTH EDITION, THE "DETAILS" IN THE PLANS AND THE "SPECIAL PROVISIONS, IDOT STANDARD DRAWINGS. AND VILLAGE OF LOMBARD STANDARD DRAWINGS" INCLUDED IN THE CONTRACT DOCUMENTS.
- 3. IN THE GENERAL NOTES, ALL REFERENCES TO ENGINEER SHALL BE INTERPRETED AS THE RESIDENT ENGINEER.

STAKING

1. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE VILLAGE, ITS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

TREE REMOVAL. CLEARING AND HEDGE REMOVAL

1. ALL TREES ARE DESIGNATED TO BE SAVED UNLESS OTHERWISE NOTED ON THE PLANS, AND SHALL BE PROTECTED IN ACCORDANCE WITH THE PROVISIONS OF ARTICLE 201.05 OF THE STANDARD SPECIFICATIONS.

UTILITIES

- 1. PRIOR TO THE START OF THE CONSTRUCTION OPERATIONS, THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES. THE LOCATION OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE VILLAGE OF LOMBARD DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATION OF SUCH UTILITIES AND EXERCISE CARE DURING CONSTRUCTION OPERATIONS SO AS NOT TO DAMAGE THEM IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITIES SO THAT THEIR FACILITIES MAY BE LOCATED AND ADJUSTED OR MOVED.
- 2. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS, WATER, SEWER, AND CABLE TELEVISION FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED.)
- THE VILLAGE OF LOMBARD WILL LOCATE EXISTING SANITARY AND WATER SERVICES TO THE INDIVIDUAL RESIDENCES WITHIN THE PROJECT'S LIMITS. THESE SERVICES WILL BE LOCATED ONE TIME ONLY. FROM THEN ON THE CONTRACTOR SHALL DOCUMENT AND BE RESPONSIBLE FOR MAINTAINING KNOWLEDGE OF THE LOCATIONS OF THESE SERVICES. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING AS—BUILT KNOWLEDGE OF THE LOCATION (INCLUDING ELEVATIONS) OF THE NEWLY INSTALLED SERVICES AND MAINS. THE VILLAGE WILL NOT BE RESPONSIBLE FOR LOCATING NEW MAINS OR SERVICES FOR THE CONTRACTOR. THIS IS IN EFFECT UNTIL FINAL PAYMENT. FOR THE DURATION OF THE CONTRACT, THE CONTRACTOR SHALL MAKE THE AS—BUILT UNDERGROUND UTILITY INFORMATION AVAILABLE TO THE ENGINEER WHENEVER REQUESTED. IN THE EVENT OF A "JULIE" CALL WITHIN THE PROJECT LIMITS, THE ENGINEER WILL NOTIFY THE CONTRACTOR TO MARK ANY UTILITIES STILL UNDER THE CONTRACTORS RESPONSIBILITY.
- 4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL ABOVE AND BELOW GROUND UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ALL UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR THE VILLAGE OF LOMBARD. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.
- 5. THE CONTRACTOR SHALL COOPERATE WITH THE VILLAGE OF LOMBARD IN ALL UNDERGROUND UTILITY CONSTRUCTION WHICH THE VILLAGE MAY WANT TO PLACE DURING THE CONTRACTOR'S OPERATIONS.
- 6. ALL MANHOLES, CATCH BASINS, AND VALVE VAULTS SHALL BE MORTARED WITH HYDRAULIC CEMENT AS SHOWN ON THE DETAILS ON THE INSIDE AND OUTSIDE AT ALL STRUCTURE JOINTS BETWEEN BARREL, CONE, AND FLAT TOP SECTIONS. ADJUSTING RINGS SHALL BE MORTARED ON THE OUTSIDE AROUND JOINTS TO PREVENT INFILTRATION PROVIDING THEY ARE OUTSIDE OF PAVED AREAS. ONLY NON-PREFORMED MASTIC SHALL BE USED BETWEEN THE FRAME, RINGS, AND TOP OF CONE OR FLAT TOP. (PREFORMED MASTIC -- EZ STICK. RUB-R-NECK. ETC IS NOT ALLOWED ABOVE THE CONE OR FLAT TOP)
- 7. GRADATION OF TRENCH BACKFILL MATERIAL SHALL BE CA-6, AND SHALL BE PLACED IN UNIFORM LAYERS NOT EXCEEDING 12 INCHES (LOOSE MEASURE) AND COMPACTED WITH MECHANICAL EQUIPMENT TO 95% OF STANDARD PROCTOR DENSITY. PIPE BEDDING SHALL BE A MINIMUM OF 4 INCHES THICK.
- 8. ALL FRAMES WITH SELF SEALING CLOSED LIDS TO BE FURNISHED AS PART OF THIS CONTRACT FOR CONSTRUCTION, ADJUSTMENT OR RECONSTRUCTION OF MANHOLES, CATCH BASIN, INLET, VALVE VAULT, OR METER VAULT SHALL HAVE CAST INTO THE LID ONE OF THE FOLLOWING WORDS: ALL LIDS TO BE USED ON STORM SEWER STRUCTURES SHALL BEAR THE WORD "STORM". ALL LIDS TO BE USED ON SANITARY SEWER STRUCTURES SHALL BEAR THE WORD "SANITARY". ALL LIDS TO BE USED ON WATER SYSTEM STRUCTURES SHALL BEAR THE WORD "WATER". ALL CURB BOXES SHALL SAY "DUMP NO WASTE!" OR "DRAINS TO RIVERS" OR SIMILAR. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE FRAME AND GRATE OR FRAME AND CLOSED LID PROVIDED.
- 9. WHENEVER DURING CONSTRUCTION OPERATIONS LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT.
- 10. ALL AUXILIARY VALVES, FRAMES, GRATES, LIDS AND WATER SERVICE BOXES WHICH ARE TO BE ABANDONED OR ADJUSTED WITH A NEW OR DIFFERENT FRAME AND LID SHALL BECOME THE PROPERTY OF THE CONTRACTOR. ALL HYDRANTS TO BE REMOVED SHALL BE REMOVED IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, THE CONTRACTOR SHALL CONTACT THE VILLAGE OF LOMBARD TO DETERMINE IF THE VILLAGE WISHES TO SALVAGE THE HYDRANTS, FRAMES, OR LIDS. THE CONTRACTOR WILL BE RESPONSIBLE TO EITHER RETURN THE HYDRANTS. FRAMES. OR LIDS TO PUBLIC WORKS OR DISPOSE OF THEM.
- 11. ALL EXISTING AND PROPOSED SEWER DAMAGED BY THE CONSTRUCTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST.
- 2. THE CONTRACTOR SHALL RECEIVE NO ADDITIONAL COMPENSATION FOR CONSTRUCTION STAGING NECESSARY TO ACCOMMODATE UTILITY RELOCATION OR ADJUSTMENT AND/OR FOR DELAYS CAUSED BY UTILITY RELOCATION OR ADJUSTMENT.

GENERAL NOTES

13. ALL PROPOSED UTILITY STRUCTURES SHALL INCLUDE AN EXTERNAL CHIMNEY SEAL UNLESS THEY FALL IN THE PAVEMENT AND ARE
ADJUSTED TO FINAL GRADE WITH CLASS SI CONCRETE OR HOT—MIX ASPHALT SURFACE OR BINDER FULLY COVERING THE RINGS AND
CASTING. THE COST OF THE CHIMNEY SEAL SHALL BE CONSIDERED INCLUDED IN THE COST OF THE UTILITY STRUCTURE BEING INSTALLED.

PAVING AND CURB & GUTTER

- THE CONTRACTOR SHALL SAW CUT PAVEMENT, CURB & GUTTER, AND SIDEWALK AS INDICATED ON THE PLANS TO SEPARATE THE EXISTING MATERIAL TO BE REMOVED BY MEANS OF AN APPROVED CONCRETE SAW TO A DEPTH AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM BEING REMOVED.
- 2. BASE COURSE SHALL NOT BE PLACED ADJACENT TO CURB AND GUTTER UNTIL THE CURB AND GUTTER HAS BEEN PROPERLY CURED AND BACKFILLED TO THE SATISFACTION OF THE ENGINEER.
- 3. HOT-MIX ASPHALT SURFACE COURSE SHALL NOT BE PLACED UNTIL ALL EARTH EXCAVATION, TOP SOIL, SOD PLACEMENT, AND HOT-MIX ASPHALT BINDER COURSE HAVE BEEN COMPLETED TO THE SATISFACTION OF THE ENGINEER.
- 4. LONGITUDINAL PAVING JOINTS SHALL BE PLACED TO PROVIDE FOR A MINIMUM 6" OFFSET FROM ANY PROPOSED LANE LINE MARKINGS.

STORM & SANITARY SEWER

- THE COST OF MAKING SEWER CONNECTIONS TO EXISTING OR PROPOSED SEWER OR DRAINAGE STRUCTURES SHALL BE INCLUDED IN THE
 COST OF THE SEWER OR STRUCTURE BEING CONSTRUCTED.
- 2. WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS OR CATCH BASINS. THE CONTRACTOR SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS AND DISCHARGE THE SAME. HE SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT, IF NECESSARY, AND A TEMPORARY OUTLET. HE SHALL BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM TEMPORARY CONNECTIONS UNTIL SUCH TIME AS THE PERMANENT CONNECTIONS WITH SEWERS ARE BUILT AND IN SERVICE. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT.
- ALL ABANDONED PIPE AND STRUCTURE INVERTS SHALL BE PLUGGED WITH BRICK AND MORTAR TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST OF THE STORM OR SANITARY SEWER ITEMS BEING REMOVED.
- 4. ALL TYPE 3 FRAME AND GRATES FOR CATCH BASINS AND INLETS SHALL BE NEENAH R-3279-1 OR EAST JORDAN IRON WORKS 7220 OR APPROVED EQUAL.

SIGNING. STRIPING & LANDSCAPING

- 1. THOSE SIGNS WHICH ARE SO DESIGNATED BY THE ENGINEER SHALL BE REMOVED, STORED AND SUBSEQUENTLY RELOCATED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL SIGNS WHICH ARE DAMAGED DURING CONSTRUCTION OPERATIONS BEYOND REPAIR SHALL BE REPLACED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST TO THE CONTRACT.
- 2. WHEN DIRECTED BY THE ENGINEER, SUPPLEMENTAL WATERING SHALL BE APPLIED TO ALL SODDED AREAS PRIOR TO FINAL ACCEPTANCE AT A RATE SPECIFIED BY THE ENGINEER AND IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.
- 3. THE CONTRACTOR SHALL ADHERE TO LIMITS OF RESTORATION SHOWN. AREAS OUTSIDE THESE LIMITS THAT ARE DAMAGED OR DISTURBED BY THE CONTRACTOR, SHALL BE RESTORED BY THE CONTRACTOR AT HIS EXPENSE, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

MISCELLANEOUS

- 1. ACCESS: THE CONTRACTOR SHALL PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING CONSTRUCTION, EXCEPT FOR PERIODS OF SHORT DURATION (4 HOURS OR LESS). THE COST TO PROVIDE AND MAINTAIN ACCESS SHALL BE PAID FOR AND INCLUDED IN THE ITEM "AGGREGATE FOR TEMPORARY ACCESS."
- ALL DRIVEWAY APRONS SHALL BE REPLACED WITH MATERIAL OF THE SAME KIND AS THE EXISTING APRON, EXCEPT FOR EXISTING AGGREGATE DRIVEWAY APRONS WHICH SHALL BE REPLACED WITH HOT—MIX ASPHALT DRIVEWAY APRONS.
- 3. DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
- 4. ALL WASTE MATERIAL SHALL BE LEGALLY DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY AT THE CONTRACTOR'S EXPENSE.
- 5. THE CONTRACTOR SHALL ADHERE TO IDOT STANDARD DRAWING NO. 701801 WHEN CLOSING SIDEWALKS.
- ESTIMATED LOCATIONS OF SIDEWALK REMOVAL AND REPLACEMENT HAVE BEEN SHOWN ON THE PLANS. THE ENGINEER WILL DETERMINE THE EXACT LIMITS IN THE FIELD DURING CONSTRUCTION.
- IF, IN THE ENGINEER'S OPINION, THE WORK SHOWN ON THE PLANS IS NOT REQUIRED, THE ITEM WILL BE DEDUCTED FROM THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- BARRICADES: THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SAND BAGS ON EACH TYPE I OR II BARRICADE USED. (ONE (1) WEIGHTED BAG ACROSS EACH BOTTOM RAIL.)
- 9. THE FOLLOWING RATES OF APPLICATION HAVE BEEN ASSUMED IN CALCULATING PLAN QUANTITIES:

BITUMINOUS MATERIALS (PRIME COAT)

0.1 GAL/SQ YD

HOT-MIX ASPHALT SURFACE COURSE POLYMERIZED LEVELING BINDER (MACHINE METHOD) 112 LBS/SQ YD/INCH 105 LBS/SQ YD/INCH

10. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 13" (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H) OR LESS AND 1" (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3" (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 3:1 (H:V).

DESIGNED - TMS	REVISED - PER VILLAGE REVIEW 09-09-09
DRAWN - KAR	REVISED' - PER VILLAGE REVIEW 09-16-09
CHECKED -	REVISED - PER IDOT REVIEW 09-17-09
DATE - 06-26-09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOT	ES	-	F.A. P. RTE.		SEC	TION		COUNTY	TOTAL SHEETS	SHEE NO.
			1397		09-0015	53-00-R	3	DuPAGE	36	3
,								CONTRACT	NO. 6	3351
 PROJECT NO. 000317	ΔT2	ATS OT	cco o	NO DICT	N/75 4	I I I IN IOIC	EED ALE	DDO JECT W ADA	0007/404\	

SCHEDULE OF QUANTITIES

					PAY ITEM		UCTURES TO BE ADJUSTED	
GIS ID	STATION	OFFSET	STRUCTURE TYPE	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	FRAMES AND LIDS, TYPE 1, OPEN LID	FRAMES AND LIDS, TYPE 1, CLOSED LID	ADJUSTMENT TYPE	NOTES
V512B003		39' LT 23' RT	VALVE VAULT SAN MANHOLE			X	VALVE VAULTS TO BE ADJUSTED (SPECIAL) SANITARY MANHOLES TO BE ADJUSTED	FIX LEAKAGE IN JOINTS
M512B003 M512B004	2+43	22' LT 31' RT	ST MANHOLE				MANHOLES TO BE ADJUSTED CATCH BASINS TO BE ADJUSTED (SPECIAL)	REPLACE ADJ. RINGS, REPAIR BENCH GROUT FRAME AND RINGS
M512B005 M512B002	2+47	17' RT 24' LT	ST MANHOLE	X		X	CATOTI DASING TO BE ADDUSTED (SI ECIAE)	GROOT FRAME AND RINGS
M512B002 SN512001		22' LT		X		×	SANITARY MANHOLES TO BE ADJUSTED	FIX CORBEL
M512B001	2+89	32' LT	CATCH BASIN				MANHOLES TO BE ADJUSTED (SPECIAL) CATCH BASINS TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS
M607A031 M607A030	4+19	19' RT 18' RT		X		X	CATCH BASINS TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS
M607A027 N607237	5+60 5+62	24' RT	INLET SAN MANHOLE			X	INLETS TO BE ADJUSTED (SPECIAL) SANITARY MANHOLES TO BE ADJUSTED	GROUT FRAME AND RINGS
M607A028 M607A029	5+64	25' LT 4' RT	INLET	x		X	INLETS TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS
SN607813		22' LT	SAN MANHOLE			X	SANITARY MANHOLES TO BE ADJUSTED MANHOLES TO BE ADJUSTED (SPECIAL)	FIX CORBEL AND BENCH
M607A026 M607A025		19' RT 25' LT	ST MANHOLE INLET			X	MANHOLES TO BE ADJUSTED INLETS TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS
M607A024 M607A024	6+91	24' RT	INLET				INLETS TO BE ADJUSTED	
M607A023	7+91	23' RT 3' RT	ST MANHOLE	X			INLETS TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS
M607A014 SN607238	8+85	25' RT 25' RT	SAN MANHOLE			X	INLETS TO BE ADJUSTED (SPECIAL) SANITARY MANHOLES TO BE ADJUSTED	GROUT FRAME AND RINGS
M607A016 V607A014	8+97	5' RT 15' LT	VALVE VAULT	X		X	VALVE VAULTS TO BE ADJUSTED (SPECIAL)	FIX LEAKAGE IN JOINTS
M607A017 M607A019	9+06 9+10	16' RT 20' LT	ST MANHOLE ST MANHOLE	X		X	MANHOLES TO BE ADJUSTED MANHOLES TO BE ADJUSTED VALVE BOXES TO BE ADJUSTED (SPECIAL)	
V607A013 SN607811	9+20 9+25	20' LT 46' RT 18' LT	VALVE BOX SAN MANHOLE				SANITARY MANHOLES TO BE ADJUSTED	
N607239 M607A018	9+34	21' RT 17' RT	SAN MANHOLE	X		X	SANITARY MANHOLES TO BE ADJUSTED	
M607A012	9+43	27' RT	ST MANHOLE	X				
146074013	9+76	18' LT 5' RT	ST MANHOLE INLET	X			INLETS TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS
M607A010 M607A011 V607A015	9+79	24' LT	INLET VALVE VAULT	X		X	INLETS TO BE ADJUSTED	OTTOGET THE PART WITTER
N607224	12+14		SAN MANHOLE	1		x	SANITARY MANHOLES TO BE ADJUSTED MANHOLES TO BE ADJUSTED (SPECIAL)	FIX LEAKAGE IN JOINTS
M607A007 M607A011	12+67	22' RT	INLET ST MANHOLE			X	INLETS TO BE ADJUSTED MANHOLES TO BE ADJUSTED	FIX LEARAGE IN JOINTS
M607A006	14+41	24' RT	INLET			^	INLETS TO BE ADJUSTED SANITARY MANHOLES TO BE ADJUSTED MANHOLES TO BE ADJUSTED	FIV. LEAVAGE IV. JOINTO
N607810	14+56	1 1	SAN MANHOLE			X	MANHOLES TO BE ADJUSTED (SPECIAL)	FIX LEAKAGE IN JOINTS
M607817 M607A004		24' RT	INLET			X	SANITARY MANHOLES TO BE ADJUSTED INLETS TO BE ADJUSTED	
M607A003 M607A009	16+16	8' RT	ST MANHOLE ST MANHOLE				INLETS TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS
M607A009 M607A002 SN607250	16+65 16+85	26' RT 26' RT	ST MANHOLE SAN MANHOLE			X	MANHOLES TO BE ADJUSTED SANITARY MANHOLES TO BE REMOVED AND REPLACED	
N607807 V607A019	17+48	10' LT	SAN MANHOLE			X	SANITARY MANHOLES TO BE REMOVED AND REPLACED VALVE VAULTS TO BE ADJUSTED (SPECIAL)	FIX LEAKAGE IN BARREL
W607B111 M607B049			VALVE VAULT INLET			X	VALVE VAULTS TO BE ADJUSTED (SPECIAL) INLETS TO BE ADJUSTED (SPECIAL)	FIX LEAKAGE IN JOINTS GROUT FRAME AND RINGS
V607B029 V607B081	18+10	18' LT	VALVE BOX	X		X	VALVE BOXES TO BE ADJUSTED (SPECIAL)	CHOOL THAME AND THOS
M607B047	18+62	23' LT	INLET				INLETS TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS
M607B046 M607B048	18+82	11' RT	ST MANHOLE	X		, , , , , , , , , , , , , , , , , , ,	INLETS TO BE RECONSTRUCTED	
V607B080 M607B045	20+46	19' RT	ST MANHOLE	X		X	VALVE VAULTS TO BE ADJUSTED (SPECIAL)	FIX LEAKAGE IN JOINTS
M607B045 N607822 M607B044 M607B042	20+70	9' RT	SAN MANHOLE ST MANHOLE	X		X	SANITARY MANHOLES TO BE REMOVED AND REPLACED	
M607B042 M607B043 M607B039	20+85	22' LT 24' RT	INLET INLET				INLETS TO BE ADJUSTED INLETS TO BE ADJUSTED	
M607B039 M607B041	22+33 22+39	24' RT 22' LT	ST MANHOLE INLET	X	X		INLETS TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS
M607B038 N607082	23+76 23+78	18' RT 6' RT	INLET SAN MANHOLE			Х	INLETS TO BE ADJUSTED SANITARY MANHOLES TO BE REMOVED AND REPLACED	
M607082 M607B040 M607B037	23+81 25+48	18' RT	ST MANHOLE INLET	X		X	INLETS TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS
N607285 M607B035	25+60 26+65	2' RT	SAN MANHOLE ST MANHOLE			X	SANITARY MANHOLES TO BE REMOVED AND REPLACED MANHOLES TO BE ADJUSTED	
M607B034	26+69	124'l RT	I INLET				INLETS TO BE ADJUSTED INLETS TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS
M607B033 SN607286 V607B108	27+60 28+63	2' RT 8' IT	SAN MANHOLE VALVE VAULT			X	SANITARY MANHOLES TO BE REMOVED AND REPLACED VALVE VAULTS TO BE ADJUSTED (SPECIAL)	FIX LEAKAGE IN JOINTS
M607B032	29+09	4' RT	ST MANHOLE				MANHOLES TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS
M607B029 M607B031	29+53	5' RT	ST MANHOLE	X			INLETS TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS
M607B028 V607B007 SN607287	29+55 29+57	20 LT 10 LT	ST MANHOLE VALVE VAULT	X		X		
SN60/84/	29+66	24 LI	SAN MANHULE				SANITARY MANHOLES TO BE REMOVED AND REPLACED SANITARY MANHOLES TO BE ADJUSTED	
V607B008 M607B025	29+73 30+99	5' LT 8' RT	VALVE VAULT ST MANHOLE	X		X	VALVE VAULTS TO BE ADJUSTED (SPECIAL)	FIX LEAKAGE IN JOINTS
N607848	31+34	O' CL	SAN MANHOLE	L		Х	SANITARY MANHOLES TO BE REMOVED AND REPLACED	

						STR	UCTURES TO BE ADJUSTED	
					PAY ITEM			
GIS ID	STATION	OFFSET	STRUCTURE TYPE	ADJUSTED	TYPE 1,	FRAMES AND LIDS, TYPE 1, CLOSED LID	ADJUSTMENT TYPE	NOTES
WV607B110 STM607B022	31+74 31+77	6' LT	VALVE VAULT INLET	(SPECIAL)	OFEN LID	X X	VALVE VAULTS TO BE ADJUSTED (SPECIAL) INLETS TO BE ADJUSTED	FIX LEAKAGE IN JOINTS
STM607B024	31+78	7' RT	ST MANHOLE	X				DEDIACE AND DINCE DEDAID DADDE
STM607B023 SN607849	33+08	0, CF	INLET SAN MANHOLE			X	INLETS TO BE ADJUSTED (SPECIAL) SANITARY MANHOLES TO BE REMOVED AND REPLACED	REPLACE ADJ. RINGS, REPAIR BARREL
WV607B109 STM607B020	33+85	4' RT	VALVE VAULT ST MANHOLE			X	VALVE VAULTS TO BE ADJUSTED (SPECIAL) MANHOLES TO BE ADJUSTED	GROUT FRAME AND RINGS
STM607B018	33+90	22' RT	INLET				INLETS TO BE ADJUSTED INLETS TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS
STM607B019 WV607B060	34+20 34+52	18' RT	ST MANHOLE VALVE VAULT	X		X		
WV60/B061	34+5/	20' LI	VALVE VAULI	X				
STM607B015 SN607834	35+58 35+68	23' LT 0' CL	INLET SAN MANHOLE			Х	INLETS TO BE ADJUSTED (SPECIAL) SANITARY MANHOLES TO BE REMOVED AND REPLACED	GROUT FRAME AND RINGS, REPAIR BARREL
STM607B015 STM607B015 SN607834 WV607B059 SN607832 WV607B058 SN607386	35+71 36+76	13' LT 0' CL	VALVE VAULT SAN MANHOLE	Х			SANITARY MANHOLES TO BE ADJUSTED	
WV607B058	36+85 36+91	25' LT	VALVE VAULT VALVE VAULT	X				
SN607380 WV607B056	37+40 37+50	18' RT	SAN MANHOLE VALVE VAULT				SANITARY MANHOLES TO BE ADJUSTED VALVE VAULTS TO BE ADJUSTED (SPECIAL)	FIX LEAKAGE IN JOINTS
WV607B057	37+57	R' IT	VALVE VAULT	Х			SANITARY MANHOLES TO BE REMOVED AND REPLACED	
SN607837 WV607B055 WV607B054	37+88 37+95	11' LT	VALVE VAULT VALVE VAULT	X			G. W. W. W. G. C.	
WV607B053 STM607B014	38+44	12' LT	VALVE VAULT	X			VALVE VAULTS TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS
STM607B009	39+75	23' LT	INLET				INLETS TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS, RESEAL JOINT AROUND EAST PIPE
SN607838 STM607B006	39+95 40+20	19' LT	SAN MANHOLE ST MANHOLE	Х		X	SANITARY MANHOLES TO BE REMOVED AND REPLACED	
WV607B051 WV607B052	40+35 40+42	10' LT	VALVE VAULT VALVE VAULT	Х			VALVE VAULTS TO BE ADJUSTED (SPECIAL)	FIX LEAKAGE IN JOINTS
WV607B038 STM607B005			VALVE VAULT ST MANHOLE	X		Х	VALVE VAULTS TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS, FIX LEAKAGE IN JOINTS
SN607818 WV607B037	41+90 41+99 42+32	0' CL	SAN MANHOLE VALVE VAULT				SANITARY MANHOLES TO BE REMOVED AND REPLACED VALVE VAULTS TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS
	43+58	24' LT	VALVE BOX				VALVE BOXES TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS GROUT FRAME AND RINGS, FIX LEAKAGE IN
WV607B072 STM607B003		13' LT	VALVE VAULT CATCH BASIN				VALVE VAULTS TO BE ADJUSTED (SPECIAL) CATCH BASINS TO BE ADJUSTED (SPECIAL)	JOINTS GROUT FRAME AND RINGS
STM607B004 SN607319	43+87	29' RT	INLET SAN MANHOLE			~	INLETS TO BE ADJUSTED	GROOT FRANKE AND KINGS
STM607B001	45+25	32' RT	CATCH BASIN			^	SANITARY MANHOLES TO BE REMOVED AND REPLACED CATCH BASINS TO BE ADJUSTED	GROUT FRAME AND RINGS, FIX LEAKAGE IN
WV607B071	45+29		VALVE VAULT				VALVE VAULTS TO BE ADJUSTED (SPECIAL)	JOINTS REPLACE ADJ. RINGS, REPAIR BARREL AND
STM607B002 STM607B013		l l	INLET CATCH BASIN	X			INLETS TO BE ADJUSTED (SPECIAL)	BENCH
WV607B073	45+65	22' LT	ST MANHOLE VALVE VAULT	X	X		VALVE VAULTS TO BE ADJUSTED (SPECIAL)	FIX LEAKAGE IN JOINTS
WV607B070	45+82	29' LT	VALVE VAULT				VALVE VAULTS TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS, FIX LEAKAGE IN JOINTS
SN607359 STM608A069	45+92 46+09	13' LT 20' LT	SAN MANHOLE ST MANHOLE	X		Х	SANITARY MANHOLES TO BE REMOVED AND REPLACED	CONTO
	46+22	14' T	I VALVE VALLET				REMOVING MANHOLES CATCH BASINS TO BE ADJUSTED	
STM608A067 STM608A068 SN608405	46+37 46+40	21' LT 1' RT	CATCH BASIN SAN MANHOLE	X		Х	SANITARY MANHOLES TO BE REMOVED AND REPLACED	
STM608A066	46+50	18' LT 15' LT	INLET VALVE BOX				INLETS TO BE ADJUSTED (SPECIAL) VALVE BOXES TO BE ADJUSTED (SPECIAL)	REPLACE ADJ. RINGS, REPAIR BENCH
WV608A090	47+25	15' LT 15' LT	VALVE VAULT VALVE BOX	X		-	VALVE BOXES TO BE ADJUSTED (SPECIAL)	
STM608A065 SN608824	47+50	19' LT	INLET SAN MANHOLE			Х	INLETS TO BE ADJUSTED (SPECIAL) SANITARY MANHOLES TO BE REMOVED AND REPLACED	REPAIR BARREL
WV608A088 STM608A064	49+03	17' LT	VALVE VAULT INLET				VALVE VAULTS TO BE ADJUSTED (SPECIAL) INLETS TO BE ADJUSTED (SPECIAL)	FIX LEAKAGE IN JOINTS
STM608A060	51+25	22' LT	INLET				INLETS TO BE ADJUSTED (SPECIAL)	REPLACE ADJ. RINGS, REPAIR BARREL REPLACE ADJ. RINGS, REPAIR BARREL
STM608A061 SN608827	51+44	1' LT	INLET SAN MANHOLE			X	INLETS TO BE RECONSTRUCTED SANITARY MANHOLES TO BE ADJUSTED	ODOLIT FRANK AND DINES FOR FOR
WV608A086	51+51	l	VALVE VAULT				VALVE VAULTS TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS, FIX LEAKAGE IN JOINTS
STM608A059 WV608A087	52+92		INLET VALVE VAULT				INLETS TO BE ADJUSTED (SPECIAL) VALVE VAULTS TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS GROUT FRAME AND RINGS, FIX LEAKAGE IN
SN608825	54+11	n' CI	SAN MANHOLE			X	SANITARY MANHOLES TO BE REMOVED AND REPLACED CATCH BASINS TO BE ADJUSTED	JOINTS
STM608A057 STM608A058			,				CATCH BASINS TO BE ADJUSTED	GROUT FRAME AND RINGS, FIX LEAKAGE IN
WV608A049 SN608335	54+80 55+11	18' LT 22' LT	VALVE VAULT SAN MANHOLE			Х	VALVE VAULTS TO BE ADJUSTED (SPECIAL) SANITARY MANHOLES TO BE ADJUSTED	JOINTS FIX LEAKAGE IN JOINTS
STM608A056			INLET				MANHOLES TO BE ADJUSTED (SPECIAL) INLETS TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS
STM608A055 SN608826	56+60	24' RT	CATCH BASIN	-			CATCH BASINS TO BE ADJUSTED SANITARY MANHOLES TO BE REMOVED AND REPLACED	GINOUT FRAME AIND KINGS
STM608A054 STM608A053	56+75	23' RT	ST MANHOLE			^	MANHOLES TO BE REMOVED AND REPLACED MANHOLES TO BE ADJUSTED MANHOLES TO BE ADJUSTED (SPECIAL)	DEDI ACE AD L DINCE DEDVID SENSU
STM608A051	59+02	25' RT	ST MANHOLE CATCH BASIN				CATCH BASINS TO BE ADJÚSTED	REPLACE ADJ. RINGS, REPAIR BENCH
WV608A048 STM608A050			VALVE VAULT INLET			X	VALVE VAULTS TO BE ADJUSTED (SPECIAL) INLETS TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS, FIX LEAKAGE IN JOINTS CROUT FRAME AND RINGS
J. INIOOOMU30	J3∓U/	IZI Li	I INLEI			,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,	INLETS TO BE ADJUSTED (SPECIAL)	GROUT FRAME AND RINGS

I:\LOMBD\090317-St Char LAPP\CADD-SURVEY\DRAWINGS\DWGS\0900 Polited: 2-08-10 & 8t-44cm By: 560kar Capyright = 0.2009, By Baxter & Woodmon, Inc. State of Illinois - Professional Design Firm License No. - 184-001121 - Expires 4-30-11

WOODMAN

 DRAWN
 KAR
 REVISED - PER VILLAGE REVIEW 09-16

 CHECKED REVISED - PER IDOT REVIEW 09-17

 DATE
 - 06-26-09
 REVISED - PER IDOT FINAL COMMENTS 01-18

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

 F.A.P.
 SECTION
 COUNTY
 TOTAL NEETS NO.
 SHEETS NO.

 1397
 09-00153-00-RS
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 CONTRACT NO. 63351

 PROJECT NO: 090317
 STA.
 10 SIA.
 FED. ROAD DIST. NO. 1 | ILLINDIS | FED. AID PROJECT M-ARK-9003(421)

1+81	SIDEWALK REI	MOVAL & PORTLA	ND CEMENT	CONCRETE SIDE	WALK 5 INCH	H, SPECIAL
2+21						AREA (SQ FT)
2+65		1+93				
2+93						
## # # # # # # # # # # # # # # # # # #		7 . 76				
## 4+58						
4+96 5+15 RT 19 VARIES 45 5+53 5+70 RT 17 5 85 6+74 6+98 RT 24 5 120 7+04 7+27 LT 23 7 161 7+87 8+03 LT 16 7 114 8+15 8+20 LT 5 7 27 8+75 8+82 LT 7 8 55 8+88 9+05 RT 17 5 85 9+45 9+71 RT VARIES VARIES 48 9+45 9+71 RT VARIES VARIES 48 9+45 9+71 RT AVARIES VARIES 48 9+45 9+71 RT 47 VARIES 350 11+66 11+40 LT 35 7 245 11+66 11+39 RT 13 7 91						
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8+75 8+82 LT 7 8 55 9+08 LT VARIES VARIES 48 9+45 9+71 RT VARIES VARIES 77 9+54 10+01 LT 47 VARIES 77 9+54 10+01 LT 47 VARIES 77 9+54 10+01 LT 47 VARIES 77 9+55 11+46 11+59 RT 13 7 160 11+46 11+59 RT 13 7 160 12+90 13+04 LT 14 7 98 12+94 12+83 RT 19 7 133 14+35 14+43 RT 8 7 66 14+455 14+43 RT 8 7 66 15+75 LT 15 7 105 15+78 15+91 RT 13 7 115 15 7 105 15+78 15+191 RT 13						
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111+05 11+40 LT 35 7 245 11+46 11+59 RT 13 7 91 12+55 12+78 LT 23 7 160 12+90 13+04 LT 14 7 98 12+64 12+83 RT 19 7 133 14+35 14+43 RT 8 7 66 14+61 14+89 RT 28 7 196 15+60 15+75 LT 15 7 105 15+78 15+91 RT 13 7 91 15+95 16+22 LT 27 7 189 16+04 16+23 RT 19 7 133 16+83 16+88 RT 5 7 35 17+10 17+19 LT 9 7 63 17+89 18+12 RT 23 7 161 1						
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14+61	12+64			19		
15+60						66
15+78						
15+95						
16+04		15+91	<u> </u>			
16+83						
16+97		16±23				
17+10						
17+89	17+10	17+19				63
18+12 18+17 LT 5 5 25 18+67 19+23 RT 56 VARIES 367 18+99 19+60 LT 61 6 413 19+98 20+09 LT 11 6 70 20+15 20+63 LT 48 6 313 20+40 20+50 RT 10 6 60 20+63 21+02 RT 39 6 294 20+63 21+02 RT 39 6 294 20+76 20+86 LT 10 6 62 21+05 21+16 LT 11 6 70 21+29 21+45 RT 16 6 105 21+29 21+45 RT 16 6 105 21+29 21+45 RT 11 6 67 22+37 22+47 RT 10 5 50						
18+99 19+60 LT 61 6 413 19+98 20+09 LT 11 6 70 20+15 20+63 LT 48 6 313 20+40 20+50 RT 10 6 60 20+63 21+02 RT 39 6 294 20+76 20+86 LT 10 6 62 21+05 21+16 LT 11 6 70 21+29 21+45 RT 16 6 105 21+29 21+45 RT 16 6 105 21+60 21+71 LT 11 6 67 22+37 22+47 RT 10 5 50 22+37 22+46 LT 11 6 67 23+60 23+72 LT 12 6 73 23+61 23+82 RT 21 6 16 23				5	5	25
19+98					VARIES	
20+15						
20+40						70
20+63						
20+76						
21+05 21+16 LT 11 6 70 21+29 21+45 RT 16 6 105 21+60 21+71 LT 11 6 67 22+37 22+47 RT 10 5 50 22+54 22+66 LT 11 6 87 22+75 22+86 LT 11 6 67 23+60 23+72 LT 12 6 73 23+61 23+82 RT 21 6 126 23+91 24+05 LT 14 6 91 24+17 24+27 LT 10 6 77 24+46 24+69 RT 23 6 161 24+94 25+10 LT 16 6 98 25+38 25+73 RT 35 6 210 25+45 25+54 LT 9 6 56 26+1						
21+29 21+45 RT 16 6 105 21+60 21+71 LT 11 6 67 22+37 22+47 RT 10 5 50 22+54 22+66 LT 12 6 87 22+75 22+86 LT 11 6 67 23+60 23+72 LT 12 6 73 23+61 23+82 RT 21 6 126 23+91 24+05 LT 14 6 91 24+17 24+27 LT 10 6 77 24+46 24+69 RT 23 6 161 24+94 25+10 LT 16 6 98 25+38 25+73 RT 35 6 210 25+45 25+54 LT 9 6 56 26+13 26+30 LT 20 6 120 26+						
21+60 21+71 LT 11 6 67 22+37 22+47 RT 10 5 50 22+54 22+66 LT 112 6 87 22+75 22+86 LT 11 6 67 23+60 23+72 LT 12 6 73 23+61 23+82 RT 21 6 126 23+91 24+05 LT 14 6 91 24+17 24+27 LT 10 6 77 24+46 24+69 RT 23 6 161 24+94 25+10 LT 16 6 98 25+38 25+73 RT 35 6 210 25+45 25+54 LT 9 6 56 26+13 26+30 LT 20 6 120 26+44 26+74 RT 30 6 179 26						
22+37 22+47 RT 10 5 50 22+54 22+66 LT 12 6 87 22+75 22+86 LT 11 6 67 23+60 23+72 LT 12 6 73 23+61 23+82 RT 21 6 126 23+91 24+05 LT 14 6 91 24+17 24+27 LT 10 6 77 24+46 24+69 RT 23 6 161 24+94 25+10 LT 16 6 98 25+38 25+73 RT 35 6 210 25+45 25+54 LT 9 6 56 26+13 26+30 LT 20 6 120 26+44 26+74 RT 30 6 179 26+52 26+75 LT 23 7 161 27						
22+54 22+66 LT 12 6 87 22+75 22+86 LT 11 6 67 23+60 23+72 LT 12 6 73 23+61 23+82 RT 21 6 126 23+91 24+05 LT 14 6 91 24+17 24+27 LT 10 6 77 24+46 24+69 RT 23 6 161 24+94 25+10 LT 16 6 98 25+38 25+73 RT 35 6 210 25+45 25+54 LT 9 6 56 26+13 26+30 LT 20 6 120 26+44 26+74 RT 30 6 179 26+52 26+75 LT 23 7 161 27+18 27+23 RT 5 6 30 26+						
23+60 23+72 LT 12 6 73 23+61 23+82 RT 21 6 126 23+91 24+05 LT 14 6 91 24+17 24+27 LT 10 6 77 24+46 24+69 RT 23 6 161 24+94 25+10 LT 16 6 98 25+38 25+73 RT 35 6 210 25+45 25+54 LT 9 6 56 26+13 26+30 LT 20 6 120 26+44 26+74 RT 30 6 179 26+52 26+75 LT 23 7 161 27+18 27+23 RT 5 6 30 26+97 27+66 LT 9 6 59 27+52 27+68 LT 16 6 98 28+0		22+66	LT	12	6	87
23+61 23+82 RT 21 6 126 23+91 24+05 LT 14 6 91 24+17 24+27 LT 10 6 77 24+46 24+69 RT 23 6 161 24+94 25+10 LT 16 6 98 25+38 25+73 RT 35 6 210 25+45 25+54 LT 9 6 56 26+13 26+30 LT 20 6 120 26+44 26+74 RT 30 6 179 26+52 26+75 LT 23 7 161 27+18 27+23 RT 5 6 30 26+97 27+06 LT 9 6 59 27+52 27+68 LT 16 6 98 28+02 28+12 LT 10 6 61 28+8						
23+91 24+05 LT 14 6 91 24+17 24+27 LT 10 6 77 24+46 24+69 RT 23 6 161 24+94 25+10 LT 16 6 98 25+38 25+73 RT 35 6 210 25+45 25+54 LT 9 6 56 26+13 26+30 LT 20 6 120 26+44 26+74 RT 30 6 179 26+52 26+75 LT 23 7 161 27+18 27+23 RT 5 6 30 26+97 27+06 LT 9 6 56 26+97 27+68 LT 16 6 98 28+02 28+12 LT 10 6 61 28+23 28+33 RT 10 6 60 28+67						
24+17 24+27 LT 10 6 77 24+46 24+69 RT 23 6 161 24+94 25+10 LT 16 6 98 25+38 25+73 RT 35 6 210 25+45 25+54 LT 9 6 56 26+13 26+30 LT 20 6 120 26+44 26+74 RT 30 6 179 26+52 26+75 LT 23 7 161 27+18 27+23 RT 5 6 30 26+97 27+06 LT 9 6 59 27+52 27+68 LT 16 6 98 28+02 28+12 LT 10 6 61 28+23 28+33 RT 10 6 60 28+67 28+77 RT 10 6 60 28+60					6	
24+46 24+69 RT 23 6 161 24+94 25+10 LT 16 6 98 25+38 25+73 RT 35 6 210 25+45 25+54 LT 9 6 56 26+13 26+30 LT 20 6 120 26+44 26+74 RT 30 6 179 26+52 26+75 LT 23 7 161 27+18 27+23 RT 5 6 30 26+97 27+06 LT 9 6 59 27+52 27+68 LT 16 6 98 28+02 28+12 LT 10 6 61 28+23 28+33 RT 10 6 60 28+67 28+77 RT 10 6 60 28+60 28+72 LT 12 6 77 28+84						
24+94 25+10 LT 16 6 98 25+38 25+73 RT 35 6 210 25+45 25+54 LT 9 6 56 26+13 26+30 LT 20 6 120 26+44 26+74 RT 30 6 179 26+52 26+75 LT 23 7 161 27+18 27+23 RT 5 6 30 26+97 27+06 LT 9 6 59 27+52 27+68 LT 16 6 98 28+02 28+12 LT 10 6 61 28+23 28+33 RT 10 6 60 28+67 28+77 RT 10 6 60 28+60 28+72 LT 12 6 77 28+84 29+00 LT 16 6 98 29+30<						
25+38 25+73 RT 35 6 210 25+45 25+54 LT 9 6 56 26+13 26+30 LT 20 6 120 26+44 26+74 RT 30 6 179 26+52 26+75 LT 23 7 161 27+18 27+23 RT 5 6 30 26+97 27+06 LT 9 6 59 28+97 27+68 LT 16 6 98 28+02 28+12 LT 10 6 61 28+23 28+33 RT 10 6 60 28+67 28+77 RT 10 6 60 28+60 28+72 LT 12 6 77 28+84 29+00 LT 16 6 98 29+10 29+20 LT 10 6 98 29+30<						
25+45 25+54 LT 9 6 56 26+13 26+30 LT 20 6 120 26+44 26+74 RT 30 6 179 26+52 26+75 LT 23 7 161 27+18 27+23 RT 5 6 30 26+97 27+06 LT 9 6 59 27+52 27+68 LT 16 6 98 28+02 28+12 LT 10 6 61 28+23 28+33 RT 10 6 60 28+67 28+77 RT 10 6 60 28+60 28+72 LT 12 6 77 28+84 29+00 LT 16 6 98 29+10 29+20 LT 10 6 65 29+30 29+44 LT 14 VARIES 52 29					6	
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26+44 26+74 RT 30 6 179 26+52 26+75 LT 23 7 161 27+18 27+23 RT 5 6 30 26+97 27+06 LT 9 6 59 27+52 27+68 LT 16 6 98 28+02 28+12 LT 10 6 61 28+23 28+33 RT 10 6 60 28+67 28+77 RT 10 6 60 28+60 28+72 LT 12 6 77 28+84 29+00 LT 16 6 98 29+10 29+20 LT 10 6 65 29+30 29+44 LT 14 VARIES 52 29+25 29+50 RT 25 VARIES 224		26+30				
27+18 27+23 RT 5 6 30 26+97 27+06 LT 9 6 59 27+52 27+68 LT 16 6 98 28+02 28+12 LT 10 6 61 28+23 28+33 RT 10 6 60 28+67 28+77 RT 10 6 60 28+60 28+72 LT 12 6 77 28+84 29+00 LT 16 6 98 29+10 29+20 LT 10 6 65 29+30 29+44 LT 14 VARIES 52 29+25 29+50 RT 25 VARIES 224						
26+97 27+06 LT 9 6 59 27+52 27+68 LT 16 6 98 28+02 28+12 LT 10 6 61 28+23 28+33 RT 10 6 60 28+67 28+77 RT 10 6 60 28+60 28+72 LT 12 6 77 28+84 29+00 LT 16 6 98 29+10 29+20 LT 10 6 65 29+30 29+44 LT 14 VARIES 52 29+25 29+50 RT 25 VARIES 224					7	161
27+52 27+68 LT 16 6 98 28+02 28+12 LT 10 6 61 28+23 28+33 RT 10 6 60 28+67 28+77 RT 10 6 60 28+60 28+72 LT 12 6 77 28+84 29+00 LT 16 6 98 29+10 29+20 LT 10 6 65 29+30 29+44 LT 14 VARIES 52 29+25 29+50 RT 25 VARIES 224		27+23	RT		6	
28+02 28+12 LT 10 6 61 28+23 28+33 RT 10 6 60 28+67 28+77 RT 10 6 60 28+60 28+72 LT 12 6 77 28+84 29+00 LT 16 6 98 29+10 29+20 LT 10 6 65 29+30 29+44 LT 14 VARIES 52 29+25 29+50 RT 25 VARIES 224						
28+23 28+33 RT 10 6 60 28+67 28+77 RT 10 6 60 28+60 28+72 LT 12 6 77 28+84 29+00 LT 16 6 98 29+10 29+20 LT 10 6 65 29+30 29+44 LT 14 VARIES 52 29+25 29+50 RT 25 VARIES 224						
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28+60 28+72 LT 12 6 77 28+84 29+00 LT 16 6 98 29+10 29+20 LT 10 6 65 29+30 29+44 LT 14 VARIES 52 29+25 29+50 RT 25 VARIES 224						
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29+10 29+20 LT 10 6 65 29+30 29+44 LT 14 VARIES 52 29+25 29+50 RT 25 VARIES 224						
29+30 29+44 LT 14 VARIES 52 29+25 29+50 RT 25 VARIES 224						
29+25 29+50 RT 25 VARIES 224						
00 - 10 - 10 - 10 - 10 - 10 - 10 - 10 -	51+50	51+60	RT	10	5	50
52+82 53+04 RT 22 5 110	52+82	53+04	RT	22	5	110
54+21 54+37 RT 16 5 80						
55+49 55+66 RT 17 5 85	55+49	55+66	RT	17		
TOTAL 8259					TOTAL	8259

	REMOVAL A		LACEM
STATION	STATION	0/S	LENG
1+81 2+93 3+18 4+20	1 1 1 1 1	LT	13
2+93	3+40 3+30 4+33	LI	47
<u>3+18</u>	3+30	<u>RT</u>	12
7120	4+33 4+44	RT	
4+32 4+58			12
4+96	5+15	ŘŤ	1 10
5+53		RT	1 17
6174	i kilaa i	RT	25
7+05	7+29 8+03	<u> </u>	13
7+05 7+90 8+75	8+02	<u></u>	1 1
8188	8+82 9+05	RT	17
9145	Q±71	ŔŤ	26
9+54	10101	LT	47
10+46	10+57	RŢ	1 1.
11+05	11+40	<u>LT</u>	J 35
12+40	11+60 12+80 12+83 13+05	IT IT	14
12+64	12+83	함	39 19
12+90	13+05	17	1 15
14+29	13+05 14+43	ŘŤ	14
11+05 11+46 12+41 12+64 12+90 14+29 14+61 15+60	I 14±89 I	RT	14 28 15
	15+75 15+91	LŢ	15
15+78 15+91	15+91	<u>KI</u>	13
16+04	16+24	RT.	33 19
	15+91 16+24 16+23 17+20 18+12 19+36 19+60 20+09 20+63 21+04 20+86	- -	1 18
17+10 17+89 18+67 18+99 19+98 20+20 20+61 20+76 21+06	18+12	ŘŤ	
18+67	19+36	RT	69
18+99	19+60 20+09 20+63 21+04	<u> </u>	6
19+98	20+09	<u> </u>	4.
20+20	21+04	- k t	4.
20+76	20+86 21+20 21+45	- 	1 10
21+06 21+26	20+86 21+20 21+45	ĽΤ	14
21+26	21+45	RT	19
	21+67	<u> </u>	11
22+25	22+08	RT	
21+06 21+26 21+56 21+95 22+25 23+60 23+61	21+67 22+08 22+41 23+72 23+82 24+08 24+30 24+69 25+09	IT	12
23+60 23+61	23+82	ŘŤ	2
23+96	24+08	ĹŤ	12
23+61 23+96 24+19 24+46 24+95 25+40	24+30	LT	1 1
24+46	24+69	<u> </u>	1 2
24+95 25+40 26+13	25+09	DT	14
2611	26+30	- KI	1 17
/D+44	26+74	ŘŤ	30
26+52 27+74 28+24 28+32 28+62 28+85	26+73	LŤ	7
27+74	27+89	LT	15
28+24	28+33	<u>RŢ</u>	9
28+32	28+50	<u> </u>	15
28185	28+00	╌┾	14
28+32 28+62 28+85 29+10 29+25 43+32 47+40 47+92	29+20	—† †	10
29+25	29+20 29+50 43+42 47+55 48+02	RT	
43+32	43+42	RT RT	1 10
47+40	47+55	RT	15 10
47+92 48+13	48+02	RI DT	1 10
48+13 48+13	40+27	TT	14
	24+30 24+69 25+09 25+70 26+74 26+73 27+89 28+33 28+50 28+73 28+99 29+20 29+50 43+42 47+55 48+27 48+27 48+80 49+59 50+42		16
48+43 49+19	49+59	- [† -	1 40
50+27	50+42	ΞŤ	1 15
50+66	1 31714	LŤ	48
54+21	51+14 54+37 55+67	RT	16
54+21 55+49 55+84	55+6/	<u> </u>	1 18
50+66 54+21 55+49 55+84 56+53 58+94	54+37 55+67 55+95 56+95 59+08	RT RT RT RT	42
58+94	59+08	RT	14
	, ,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,	TOTAL	150

COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (SPECIAL)

42+09	42	+18				16	
42+28	42	+18 +39 +84	Ř			11	
42+33 42+62	42-	+84 +95	R			5/	
42+62	43	+90	R			33 25	
43+29	4.3	+54	R			25	
47 LEA	4.3	+54 +61	Τ̈́		_	11	
43+67	43	+61 +81	Ŕ		_	28	
44+32	44-	+46	Ľ			14	
144+54		+46	R	T		12	
44+62	44-	+87 +26	R	T		25	
45+13	45-	+26	L			13	
45+28	45	+36	R			13 18	
45+40	45	+36 +38 +58 +25 +57		<u> </u>	-	18	
46+03	40	+23	R			29	
46+30 46+33	76.	+86	1			60	
46+75		+86	R			11	
46+97		+27		+		30	4.0
47+01	47.	+20	R			19	
47+42		+58	Ϊ́			16	
50+12	I 50-	+27	R			15	
51+13	51-	+30	L.	Ť		20	
51+92 52+23	51	+09	Ľ			17	
52+23 52+52	52-	+38				15	
52+52	52-	+67 +04				15	
52+82	53.	<u>+04</u>	I R			22	
53+09	53	+57 +37		<u> </u>		48	
54+21 54+67	54	+37 +91	R			16	
55+49		+91 +67	L.			24 18	
1 33 7 49	1 22.	+ 0/	l R			10	
			TO		1	285	
			TO		1	285	
		D CE		TAL			
PORT	ΓLAN		MEN	TAL CO	NC	RETE	
PORT	[LANI RIVE\	VAY	MENT REMO	TAL CO OVAL	NC	RETE	
PORT	[LANI RIVE\	VAY	MEN	TAL CO OVAL	NC	RETE	
PORT	TLANI RIVE\ F	WAY REPLA	MENT REMO	TAL CO OVAL	NC	RETE ID	
PORT	TLANI RIVE\ F	WAY REPLA	MENT REMO	TAL CO OVAL	NC AN	RETE ID ARE	
PORT	TLANI RIVE\ F	WAY REPLA	MENT REMO	TAL CO VAL ENT	NC AN	RETE ID ARE (SQ Y	D)
PORT DI STATION	TLANI RIVE\ F	WAY REPLA LENC	MENT REMO ACEM	TAL CO VAL ENT	NC AN	RETE ID ARE (SQ Y	D)
PORT DI STATION 2+40 10+50	TLANI RIVE\ F	VAY REPLA LENC	MENT REMO ACEM STH	TAL CO VAL ENT	NC AN H	RETE ID ARE (SQ Y 22.2 8.50	(D)
PORT DI STATION 2+40 10+50 12+50	TLANI RIVE\ F O/S LT RT LT	VAY REPLA LENG 10.0	MENT REMO ACEM STH	TAL T CO VAL ENT WIDT 20.0 7.00 7.00	NC AN H	RETE ID ARE. (SQ Y 22.2 8.50 12.5	(D)
PORT DI STATION 2+40 10+50 12+50 14+32	CLANI RIVEN O/S LT RT LT RT	VAY REPLA LENC 10.0 11.0 6.5	MENT REMO ACEM OTH	TAL T CO VAL ENT WIDT 20.0 7.00 6.00	NC AN H	RETE ID ARE (SQ Y 22.2 8.50 12.5 4.10	(D)
PORT DI STATION 2+40 10+50 12+50 14+32 22+00	CLANIRIVEN FOOS OS LT RT LT RT LT	MAY REPLA LENG 10.0 11.0 16.0 10.0	EMENT REMO ACEM OTH DO DO DO	TAL T CO VAL ENT WIDT 20.0 7.00 6.00	NC AN H	RETE ID ARE (SQ Y 22.2 8.5(12.5 4.1(7.1((D) 0 0
PORT DI STATION 2+40 10+50 12+50 14+32 22+00 22+33	FLANIRIVEN FOSSIBLE O/S LT RT LT RT LT RT	MAY REPLA 10.0 11.0 16.0 10.0 16.0	MENT REMO ACEM OO OO OO	TAL F CO OVAL ENT WIDT 20.0 7.00 6.00 6.40 3.30	H O	RETE ID ARE (SQ Y 22.2 8.5(12.5 4.1(7.1(5.80	(D) 0 0 0)
PORT DI STATION 2+40 10+50 12+50 14+32 22+03 22+33 25+30	LANI RIVEV F O/S LT RT LT RT LT RT LT	MAY REPLA 10.0 11.0 16.0 10.0 15.0	MENT REMO ACEM OO OO OO OO	TAL T CO VAL ENT WIDT 20.0 7.00 6.00 6.40 3.30 6.00	H O	RETE ID ARE (SQ Y 22.2 8.5(12.5 4.1(5.8(9.3((D) 0 0 0 0 0
PORT DI STATION 2+40 10+50 12+50 22+33 25+30 27+80	FLANI RIVEV FO/S O/S LT RT LT RT LT LT	MAY REPLA 10.0 11.0 16.0 16.0 16.0	REMO ACEM OO OO OO OO OO	TAL T CO VAL ENT WIDT 20.0 7.00 6.00 6.40 3.30 6.00 6.00	H O	RETE ID AREJ (SQ Y 22.2 8.50 12.5 4.10 7.11 5.80 9.30 8.70	(D) 0 0 0 > > >
PORT DI STATION 2+40 10+50 12+50 14+32 22+90 22+33 25+30 27+80 28+40	ILANI RIVEN O/S LT RT LT LT LT LT LT	MAY REPLA 10.0 11.0 16.0 16.0 16.0 17.0	MENT REMO ACEM OO OO OO OO OO OO OO	TAL T CO VAL ENT 20.0 7.00 6.00 6.40 6.00 6.00 6.00	NC AN	RETE ID ARE (SQ Y 22.2 8.5(12.5 4.1(5.8(9.3(8.7(11.9	(D) 0 0 0 0 0 0 0 0 0 0 0 0 0 0
PORTI DI 2+40 10+50 12+50 14+32 22+00 22+33 27+80 28+40 33+00	FLANI RIVEV FOO/S LT RT LT RT LT LT LT RT	MAY REPLA 10.0 11.0 16.0 16.0 15.0 17.0 16.0	MENT REMO ACEM OD DO DO DO DO DO DO DO DO DO DO DO DO	TAL T CO VAL ENT 20.0 7.00 6.00 6.40 6.00 6.00 6.00	NC AN	RETE ID ARE (SQ Y 22.2 8.5(12.5 4.1(7.1(5.8(9.3(8.7(11.9 3.6((D)
PORTI DI 2+40 10+50 12+50 14+32 22+00 22+33 25+30 27+80 28+40 33+00 34+24 34+24	ILANI RIVEN O/S LT RT LT LT LT LT LT	MAY REPLA 10.0 11.0 16.0 16.0 15.0 17.0 16.0 18.0	MENT REMO ACEM STH DO DO DO DO DO DO DO DO DO DO DO DO DO	TAL T CO VAL ENT 20.0 7.00 6.00 6.40 6.00 6.00 6.00	NC AN	RETE ID ARE. (SQ Y 22.2 8.5(12.5 4.1(5.8(9.3(8.7(11.9 3.6(4.00	(D) 0 0 0 0 0 0 0 0 0 0 0
PORI DI	FLANI RIVE F O/S LT RI LT RI LT LT LT LT RT RT	MAY REPLA 10.0 11.0 16.0 16.0 15.0 17.0 16.0	MENT REMCACEM STH 00 00 00 00 00 00 00 00 00 00 00 00 00	TAL T CO VAL ENT WIDT 20.0 7.00 6.00 6.00 6.00 6.00 6.00 6.00	H O O O O O O O O O O O O O O O O O O O	RETE ID ARE (SQ Y 22.2 8.5(12.5 4.1(7.1(5.8(9.3(8.7(11.9 3.6((D) 0 0) 0))))
PORT DI	FLANI RIVE F O/S LT RI LT RI LT LT RT RT RT RT	MAY REPLA 10.0 11.0 16.0 16.0 16.0 17.0 18.0 18.0 VARI	MENT REMC ACEM 5TH 00 00 00 00 00 00 00 00 00 00 00 00 00	TAL T CO VAL ENT WIDT 20.0 7.00 6.00 6.00 6.00 6.00 2.00 2.00 VARIE 2.00	H O D D D D D D D D D D D D D D D D D D	RETE ID AREJ (SQ Y 22.2 8.5(12.5 4.1(5.8(9.3(8.7(11.9) 3.6(4.0(4.0((D) 0 0 0 0 0 0 0 0 0 0 0 0 0
PORI DI	FLANIRIVEV FOST STATE OS STATE	MAY REPLA 10.0 11.0 16.0 16.0 15.0 14.0 16.0 18.0 VAR	MENT REMC ACEM 5TH 00 00 00 00 00 00 00 00 00 00 00 00 00	TAL T CO DVAL ENT WIDT 20.00 7.00 6.00 6.00 6.00 6.00 2.00 2.00 2.00 WARIE	H O D D D D D D D D D D D D D D D D D D	RETE ID AREJ (SQ Y 22.2 8.50 12.5 4.10 7.10 5.80 9.30 11.9 3.60 4.00 4.00 40.0	(D) 0 0 0 0 0 0 0 0 0 0
PORT DI	FLANI RIVEU F O/S LT RT LT LT LT RT LT RT LT LT RT LT	MAY REPLA 10.0 11.0 16.0 16.0 15.0 14.0 18.0 18.0 VARI	MENT REMC ACEM 5TH 00 00 00 00 00 00 00 00 00 00 00 00 00	TAL T CO VAL ENT WIDT 20.0 7.00 6.00 6.00 6.00 6.00 2.00 2.00 VARIE 2.00	H O D D D D D D D D D D D D D D D D D D	RETE ID AREJ (SQ Y 22.2 8.50 12.5 4.10 7.18 9.30 8.70 11.9 4.00 4.00 40.0 3.60	(D) 0 0 0 0 0 0 0 0 0 0
PORT DI	FLANI RIVEU F O/S LT RT LT LT LT RT LT RT LT LT RT LT	MAY REPLA 10.0 11.0 16.0 16.0 15.0 14.0 18.0 18.0 VARI	MENT REMC ACEM 5TH 00 00 00 00 00 00 00 00 00 00 00 00 00	TAL T CO OVAL ENT WIDT 20.0 7.00 7.00 6.40 6.00 6.00 2.00 2.00 2.00 2.00 4.00 4.00	H O D D D D D D D D D D D D D D D D D D	RETE ID AREJ (SQ Y 22.2 8.5(12.5 4.1(7.1(5.8(9.3(8.7(11.9) 3.6(4.0(40.0) 3.6(14.7)	(D) 0 0 0 0 0 0 0 0 0 0
PORT DI	FLANI RIVEU F O/S LT RT LT LT LT RT LT RT LT LT RT LT	MAY REPLA 10.0 11.0 16.0 16.0 15.0 14.0 18.0 18.0 VARI	MENT REMC ACEM 5TH 00 00 00 00 00 00 00 00 00 00 00 00 00	TAL T CO OVAL ENT WIDT 20.0 7.00 7.00 6.40 6.00 6.00 2.00 2.00 2.00 2.00 4.00 4.00	H O D D D D D D D D D D D D D D D D D D	RETE ID AREJ (SQ Y 22.2 8.5(12.5 4.1(7.1(5.8(9.3(8.7(11.9) 3.6(4.0(40.0) 3.6(14.7)	(D) 0 0 0 0 0 0 0 0 0 0
PORT DI	FLANI RIVEU F O/S LT RT LT LT LT RT LT RT LT LT RT LT	MAY REPLA 10.0 11.0 16.0 16.0 15.0 14.0 18.0 18.0 VARI	MENT REMC ACEM 5TH 00 00 00 00 00 00 00 00 00 00 00 00 00	TAL T CO OVAL ENT WIDT 20.0 7.00 7.00 6.40 6.00 6.00 2.00 2.00 2.00 2.00 4.00 4.00	H O D D D D D D D D D D D D D D D D D D	RETE ID AREJ (SQ Y 22.2 8.5(12.5 4.1(7.1(5.8(9.3(8.7(11.9) 3.6(4.0(40.0) 3.6(14.7)	(D) 0 0 0 0 0 0 0 0 0 0

SCHEDULE OF QUANTITIES

DETECTABLE WARNINGS — CAST IRON PLATES						
STATION	0/S	AREA (SQ FT)				
2+21	LT	12				
2+64	LT	12				
9+05	RT	12				
9+08	LT	12				
9+52	RT	12				
9+60	L	12				
19+23		8				
19+23	RT	8				
20+35	LT	8				
20+45	RT	8				
26+75	RT	12				
27+25	RT	12				
28+32	RT	10				
28+70	RT	10				
29+35	RT	12				
29+40	LT_	12				
51+52	LRT	10				
	IOIAL	182				

DOMESTIC SERVICE I BE ADJ	BOXES	
STATION	0/\$	
8+00	27'	LT
13+00	25'	LT
21+10	25'	LT
26+25	24'	
14+12	31'	RT
21+65	27'	LT
22+81	27'	LT
24+25	25'	LT
28+11	25'	LT
	TOTAL	9

DETECTABLE WARNINGS- BRICK PAVERS					
STA	0/S	SQ FT			
30+00	17	10			
30+12	ŔŤ	10			
34+12	 	10			
34+58	 	10			
35+43	ŘŤ	10			
35+93	ŘŤ	10			
41+45	ŔŤ	10			
41+67	l ii	1Ŏ			
41+80	ŘÍ	10			
42+14	LŤ	10			
45+54	LT	10			
45+67	LT	10			
45+29	RT	10			
46+30	LT	10			
46+45	LT	10			
46+08	RT	10			
46+20	RT	10			
51+00	RT	10			
51+32	┝	10 10			
51+81 54+87	├	10			
	<u> </u>	10			
55+45 58+47	├ ┼	10			
58+97	┸	10			
JU 131	TOTAL	240			
		· · · · · · · · · · · · · · · · · · ·			

CLASS	D PATCHES,		/, 12 INCH
STATION	TO STATION	0/8	AREA (SQ YD)
1+26	1+32	LT-RT	40
2+00	2+50	RT	33
2+19	2+65	LŤ	33
3+48	3+54	LT-RT	40
4+02	4+08	LT-RT	40
4+30	4+36	LT-RT	37
4+50	4+56	LT-RT	33
	4+76	LT-RT	43
4+68	5+90		27
5+85		LT-RT	150
5+80	9+30	LT	156
8+75	8+80	LT-RT	27
9+60	9+70	LT	27
11+70	11+76	LT-RT	32
13+60	16+60	LT	133
17+38	17+68	LT	52
18+01	18+08	LT-RT	36 37
18+60	18+67	LT-RT	37
18+67	19+67	LT	44
19+44	19+34	RT	40
20+36	20+44	LT-RT	50
20+68	20+75	LT-RT	39
21+50	21+57	LT-RT	37
20+65	21+57 20+78	LT-RT	45
21+57	22+60	LT	73
27 7 70	24105	 	44
23+30 25+68	24+05 25+75	LT-RT	39
25+00			78
25+75	26+88	LŢ	78
27+42	27+92 28+55	LT	33 27
28+50	28+55	LT-RT	2/
29+77	29+12	LT_	93
30+48	31+48	RT	67 27
33+54	33+60	LT-RT	27
33+90	33+97	LT-RT	28
35+05	35+10	LT-RT	27 32
35+95	36+01	LT-RT	32
37+08	37+16	LT-RT	32 28
42+13	42+18	LT-RT	28
42+81	43+21	LT	31
44+09	44+14	LT-RT	27
44+76	46+37	RT	53
45+00	45+20	LT	3.3
46+50	46+70	l ti	27
47+40	47+46	LT-RT	26
47+92	47+13	RT.	27 26 33
49+19	49+26	RT LT-RT	 3 0
51+06	51+14	LT-RT	35
51+93	51+98	LT-RT	<u> </u>
53+94	53+54	LT	57
56+55	56+61	LT-RT	30 35 25 27 27
59+13	59+18	LT-RT	25
28+12	1 23+10	TOTAL	2106
		IUIAL	2106

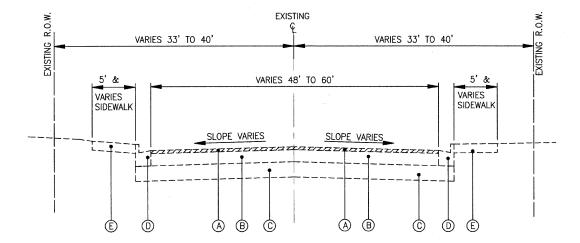
CLASS	D PATCHES,	TYPE II,	, 12 INCH
STATION	TO STATION	0/8 /	AREA (SQ YI
8+52	8+57	LT T	13
9+21	9+29	RT	11
16+00	16+05	LT	13
17+75	17+83	RT	11
19+56	19+60	RT	11
23+50	23+55	RT	13
29+36	29+56	RT	13
30+12	30+20	RT	11
36+53	36+59	RT	15
37+20	37+32	RT	8
38+27	38+33	RT	13
40+55	40+60	RT	13
42+73	42+79	RT	15
43+46	43+54	RT	11
45+00	45+10	RT	13
47+00	47+12	RT	13
53+33	53+45	L_L[11
		TOTAL	208

CLASS	D PATCHES,	TYPE II	l, 12 INCH
STATION	TO STATION	0/S	AREA (SQ YD)
1+92	1+97	LT-RT	20
1+92 3+12	3+17	LT-RT	20 20
8+16	8+21	LT-RT	20 21 21 21 21 21
9+74	9+82	LT	21
10+25	10+33	LT	21
10+56	10+64	LT-RT	21
12+72	12+80	LT-RT	21
12+72 13+08 13+25 13+80 15+12	12+80 13+13 13+38	LT-RT	20
13+25	13+38	LT	16
13+80		! LT	21
15+12	15+17	LT-RT	20
23+70 25+54	25+82	RT	16
25+54	25+66	CL	16
26+88	26+94	LT-RT	24
27+54	27+66	CL	16
28+62 29+16	28+67	LT-RT	20 21
29+16	29+24	LT	21
29+52 31+28	29+64	ČĹ	16
31+28	31+40	CL	16
33+U3 I	33+15	CL	16
33+30 34+50	33+41	LT	20
34+50	34+55	LT-RT	20
35+61 37+28	35+83	CL	16
37+28	37+68	LT	18
37+68	37+72	LT-RT	21
37+77	37+89	CL	16
38+80	38+92	CL	16
39+89 41+93	40+01	CL	16
<u>41+93</u>	42+05	CL	16
42+73 43+88	42+81	LT	21
43+88	44+00	CL	16
46+33	46+45	CL	16
46+50	46+65	RT	20
47+04	47+19	LT	20
48+13	48+33	LT	18
48+85	48+97	CL	16
50+65	50+85	LT	18
50+66	50+86	RT	18
51+68	51+73	LT-RT	22 16
54+05	54+17	CL	16
54+21	54+27	LT-RT	20
55+49	55+54	LT-RT	17
56+64	56+76	CL	16
		TOTAL	795

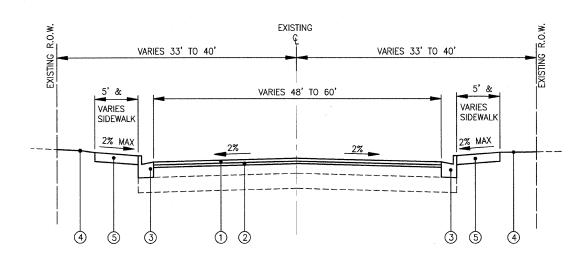
T E R	DESIGNED - TMS	REVISED - PER VILLAGE REVIEW 09-09-0
4	DRAWN - KAR	REVISED - PER VILLAGE REVIEW 09-16-0
.	CHECKED -	REVISED - PER IDOT REVIEW 09-17-0
M A N	DATE - 06-26-09	REVISED - PER IDOT FINAL COMMENTS 01-18-1

STATE	OF	ILLINOIS	
DEPARTMENT	OF 1	TRANSPO	RTATION

S	CHEDULE OF QUA	NTITIES		F.A. P. RTE.	SECTION		COUNTY	TOTAL SHEETS	SHEE NO.
	,			1397	09-00153-00-RS		DuPAGE	36	5
							CONTRACT	NO. 63	3351
	PROJECT NO: 090317	STA.	TO STA.	FED. ROA	D DIST. NO. 1 ILLINOIS I	FED. AID	PROJECT M-ARA-	9003(421)	



EXISTING TYPICAL SECTION STA. 1+00 TO STA. 36+00 ST. CHARLES ROAD (NOT TO SCALE)



PROPOSED TYPICAL SECTION
STA. 1+00 TO STA. 36+00
ST. CHARLES ROAD
(NOT TO SCALE)

EXISTING LEGEND

- A EXISTING HOT-MIX ASPHALT SURFACE REMOVAL 3 4"
- (B) EXISTING HOT-MIX ASPHALT PAVEMENT, VARIES 7.5" TO 12"
- © EXISTING AGGREGATE BASE COURSE
- D EXISTING CURB AND GUTTER, TYPE B-6.12
- (E) EXISTING SIDEWALK (PCC OR BRICK PAVERS)
- F EXISTING HOT-MIX ASPHALT SURFACE REMOVAL, 1½"
- © EXISTING PCC PAVEMENT, VARIES 6" TO 11"
- H CONCRETE PAVERS, 6CM
- () SAND CUSHION, 1"
- J SUB BASE GRANULAR MATERIAL, TYPE B, 6"
- (K) CONCRETE EDGING, 12" WIDE
- U SUB BASE GRANULAR MATERIAL, TYPE B, 4"
- M) GEOTECHNICAL STABILIZATION FABRIC

PROPOSED LEGEND

- HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 2 1/4"
- 2) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 1"
- \bigcirc COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, TYPE B-6.12 (AS DETERMINED BY THE ENGINEER)
- TOPSOIL FURNISH AND PLACE, 4" (AS DETERMINED BY THE ENGINEER)

 SODDING, SPECIAL (AS DETERMINED BY THE ENGINEER)
- (5) PORTLAND CEMENT CONCRETE SIDEWALK, 5", SPECIAL OR BRICK SIDEWALK REMOVAL AND REPLACEMENT (AS DETERMINED BY THE ENGINEER)
- 6) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 12"
- T) STRIP REFLECTIVE CRACK CONTROL

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING

MIX TYPE	AIR VOIDS @ Ndes
ROADWAY	
HOT-MIX ASPHALT SURFACE COURSE, MIX 'D' N70 (IL-9.5mm)	4% @ 70 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	4% @ 50 GYR.
PATCHING	
CLASS D PATCHES, 12 INCH, (HMA BINDER, IL-19mm)	4% @ 70 GYR.

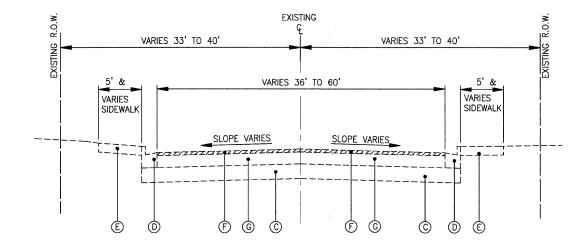
THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASHPALT SURFACE MIXTURE QUANTITIES IS 112 LB/SY-IN.

THE 'AC TYPE' FOR POLYMERIZED HMA MIXES SHALL BE 'SBS/SBR PG 70-22' AND FOR NON-POLYERNIZED HMA THE 'AC TYPE' SHALL BE 'PG 64-22' UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR 'PERCENT OF RAP' SEE DISTRICT ONE SPECIAL PROVISIONS.

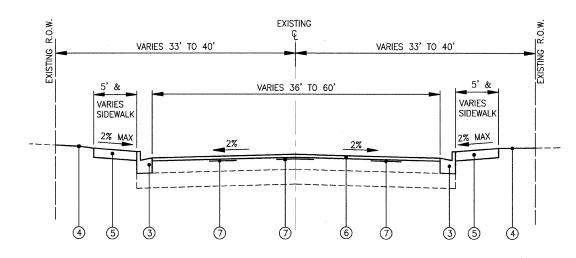
By: 560kar	& Woodman,	Design Firm Expires 4-30	
ted: 2-08-10 @ 8:44am By: 560kar	right O 2009, By Baxter	e of Illinois - Professional Design Firm nse No 184-001121 - Expires 4-30	

DESIGNED - TMS	REVISED - PER VILLAGE REVIEW 09-09-09
DRAWN - KAR	REVISED - PER VILLAGE REVIEW 09-16-0
CHECKED -	REVISED - PER IDOT REVIEW 09-17-0
DATE - 06-26-09	PEVASED - PER INCLEMAL COMMENTS 01-18-18

ST. CHARLES ROAD		F.A.P. SECTION		COUNTY	TOTAL	SHEET NO.	
EXISTING AND PROPOSED TYPICAL SECTIONS		1397	09-0015	300-RS	DuPAGE	36	6
					CONTRACT	NO. 63	3351
PROJECT NO: 090317 STA	TO STA	EED PO	AD DIST MO 1	LILLINOIS FED AIR	PROJECT MARA	9003(421)	



EXISTING TYPICAL SECTION STA. 36+00 TO STA. 59+25 ST. CHARLES ROAD (NOT TO SCALE)



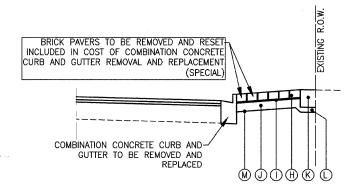
PROPOSED TYPICAL SECTION
STA. 36+00 TO STA. 59+25
ST. CHARLES ROAD
(NOT TO SCALE)

EXISTING LEGEND

- A EXISTING HOT-MIX ASPHALT SURFACE REMOVAL 3 4"
- B EXISTING HOT-MIX ASPHALT PAVEMENT, VARIES 7.5" TO 12"
- © EXISTING AGGREGATE BASE COURSE
- D EXISTING CURB AND GUTTER, TYPE B-6.12
- E EXISTING SIDEWALK (PCC OR BRICK PAVERS)
- EXISTING HOT-MIX ASPHALT SURFACE REMOVAL, 12"
- G EXISTING PCC PAVEMENT, VARIES 6" TO 11"
- (H) CONCRETE PAVERS, 6CM
- SAND CUSHION, 1"
- SUB BASE GRANULAR MATERIAL, TYPE B, 6"
- K CONCRETE EDGING, 12" WIDE
- SUB BASE GRANULAR MATERIAL, TYPE B, 4"
- GEOTECHNICAL STABILIZATION FABRIC

PROPOSED LEGEND

- 1 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 2 1/4"
- 2 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 1"
- 3 COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, TYPE B-6.12 (AS DETERMINED BY THE ENGINEER)
- TOPSOIL FURNISH AND PLACE, 4" (AS DETERMINED BY THE ENGINEER)
 SODDING, SPECIAL (AS DETERMINED BY THE ENGINEER)
- 5 PORTLAND CEMENT CONCRETE SIDEWALK, 5", SPECIAL OR BRICK SIDEWALK REMOVAL AND REPLACEMENT (AS DETERMINED BY THE ENGINEER)
- 6 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 12"
- 7) STRIP REFLECTIVE CRACK CONTROL



COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (SPECIAL)

(NOT TO SCALE)



WOOD NAN

 DESIGNED - TMS
 REVISED - PER VILLAGE REVIEW 09-09-0

 DRAWN - KAR
 REVISED - PER VILLAGE REVIEW 09-16-0

 CHECKED - REVISED - PER IDOT REVIEW 09-17-0
 DATE - 06-26-09

 REVISED - PER IDOT RINAL COMMENTS 01-18-1

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ST. CHARLES ROAD

EXISTING AND PROPOSED TYPICAL SECTIONS

| PROJECT NO: 090317 | STA. TO

 F.A.P. RTE.
 SECTION
 COUNTY
 TOTAL SHEETS
 SHEE NO.

 1397
 09-00153-00-RS
 DuPAGE
 36
 7

 CONTRACT NO.
 63351

HORIZ. 1"=20"

PROJECT NO: 090317 STA. 0+00

TO STA. 8+50

06-26-09

VISED - PFR IDOT FINAL REVIEW 01-18-10

MBD (1903) 7-51 Char LAPP (ANDL-SURVEP (UKANINUS) (UNUS) (U903) 7-51 -CHRLES-PUX et 2-08-10 @ 9.20am By: 560kar right @2030 By Boxter & Woodman, Inc.

OWED\090317-St Char LAPP\CADD-SURVEY\PRAWINGS\PWGS\090317-ST-CHRLES-Ited: 2-08-10 ® 9:02om By: 560kar tted: 2-08-10 ® 9:02om By: 560kar yright © 2009, By Boaket & Woodman, Inc. ite of illingis — Paritessional Design Firm

PROJECT NO: 090317 STA. 30+50

TO STA. 41+00

HORIZ. 1"=20'

-COMBINATION CONCRETE CURB AND GUTTER

I:\LOMBAL\UBUSI 17—31 Char LAFP(JUD-SKKPF\DARWINGS\UBUSI \J 09US 17—51—CHRLES-FLAN Plotted: 2—08—10 @ 9JOZam By: 560kar Copyright @ 2DOS, By Baxter & Woodman, Inc. State of Illinois — Professional Design Firm

06-26-09

- PER IDOT FINAL COMMENTS 01~18~

HORIZ. 1"=20"

PROJECT NO: 090317 STA. 41+00

TO STA. 53+00

. LOMBD\Q90317—SI Char LAPP\CADD—SURVEY\DRAWINGS\DWGS\Q90317—SI—CHRLES—PLA Itted: 2-08—10 @ 9010m Bp: 360kar Syright @ 2009, By Boxter & Woodman, Inc.

- 06-26-09

EVISED - PER IDOT FINAL COMMENTS 01-18-1

HORIZ. 1"=20'

PROJECT NO: 090317 STA. 53+00

TO STA. 61+00

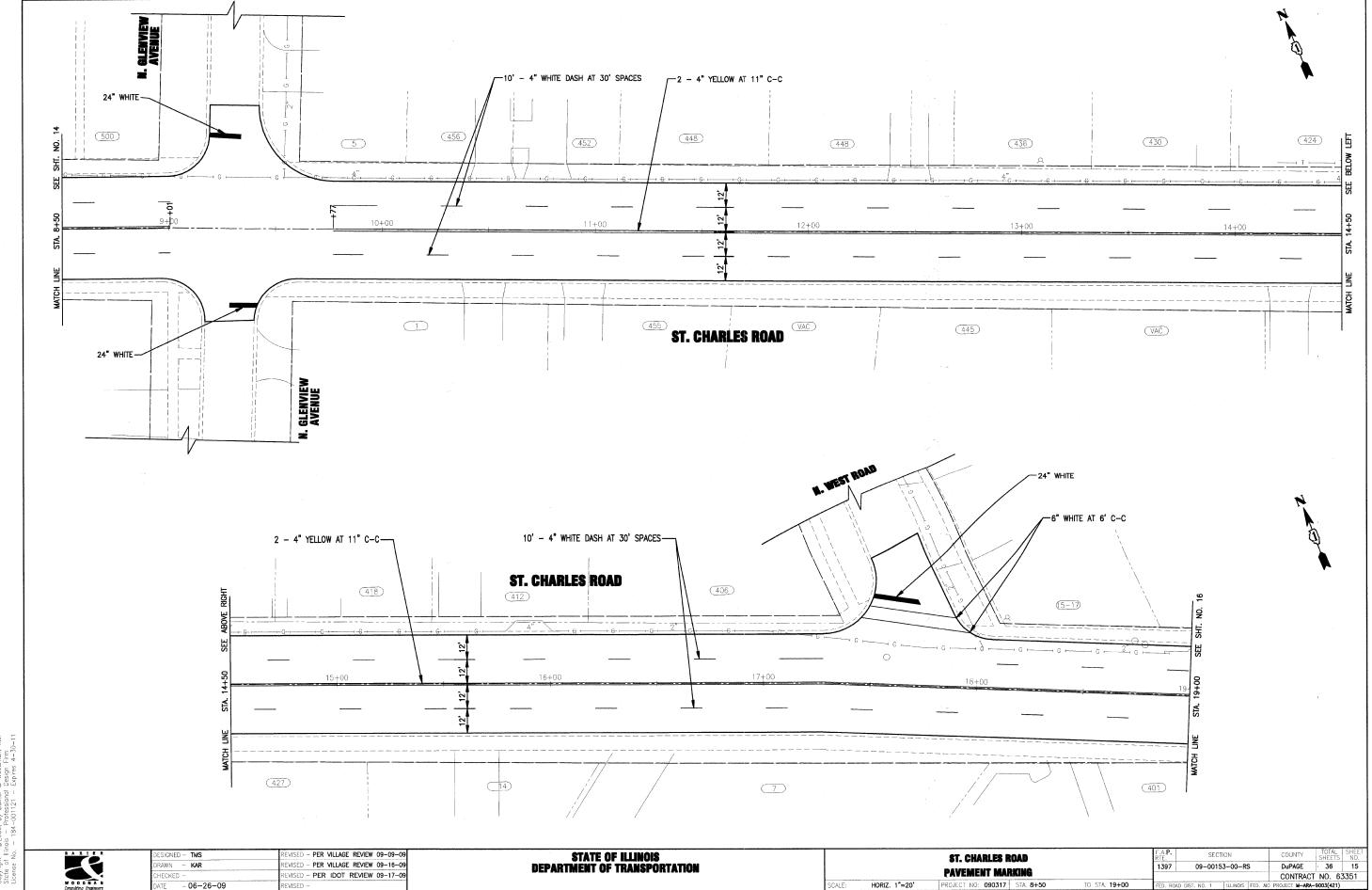
- 06-26-09

VISED - PER IDOT FINAL REVIEW 01-18-1

PROJECT BEGINS STA 1+00 10' - 4" WHITE DASH AT 30' SPACES-ST. CHARLES ROAD 560 (544) 548 6" WHITE-(547) (567) 563 (555) (557) _____2 - 4" YELLOW AT 11" C-C -LETTERS AND SYMBOLS -10' - 4" WHITE DASH AT 30' SPACES 12" YELLOW AT 45" AT 50' SPACES ______2 - 4" YELLOW AT 11" C--C 10' - 4" WHITE DASH AT 30' SPACES -2 - 4" YELLOW AT 11" C-C (526) (540) (530) (508) (504) (518) (514) (8) (517) (511) (523) (529) STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 COUNTY
 TOTAL SHEETS NO.

 DuPAGE
 36
 14

 CONTRACT
 NO.
 63351
 VISED - PER VILLAGE REVIEW 09-09-0 WOODNAN SECTION ST. CHARLES ROAD RAWN -- KAR EVISED - PER VILLAGE REVIEW 09-16-09 09-00153-00-RS **PAVEMENT MARKING** EVISED - PER IDOT REVIEW 09-17-09 06-26-09 HORIZ. 1"=20' PROJECT NO: 090317 STA. 0+00 TO STA. 8+50



PROJECT NO: 090317 STA. 19+00

TO STA. 30+50

I: (LVMBN) (980.377-51 Char LAPP (AND-SURVEY L) (PRAWINGS (190.317-51-CHILES-PM.dwg -Plotted: 2-08-10 ® 8.53 cm Bi: (S55,53 cm Bi: (S55,53 cm Bi: (B55,53 c

HORIZ. 1"=20'

TO STA. 53+00

- 06-26-09

ED - PER IDOT FINAL REVIEW 01-18-10

\LoMBD\090317-St Char LAPP\CADD-SURVEY\DRAWINGS\DWGS\090317-SiG-Pi\)
lighted: 2-08-10 @ 10-049dm By: 560km
lighted: 2-08-10 By Baxter & Woodman, inc.
sight @ 2009, By Baxter & Woodman, inc.
state of Illinois and notation of the control of the con

SECTION

09-00153-00-RS

1397

TO STA.

36

CONTRACT NO. 63351

JECT M-ARA-9003(421)

DuPAGE

TRAFFIC SIGNAL MODIFICATION CABLE PLAN

ST. CHARLES ROAD

PROJECT NO: 090317 STA.

CALE: NONE

ELECTRIC SERVICE 1 SERVICE TO GROUND

GROUND CABLE 1 POST MOUNTED

VISED - PER VILLAGE REVIEW 09-16-09

ISED - PER IDOT REVIEW 09-17-0

13.5

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

TMS

KAR

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06-26-09

HECKED -

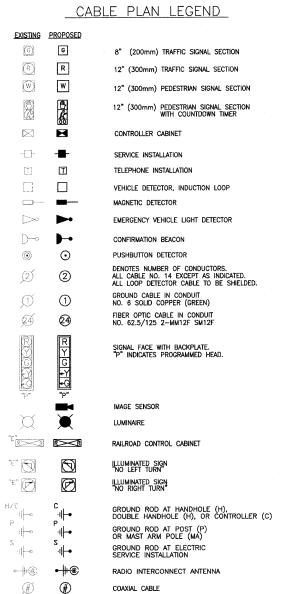
HORIZ. 1"=20'

PROJECT NO: 090317 STA.

TO STA.

(LOMBD\Q90517-5S Char LAPP\(AND-SURVEY\)DRAWINGS\DWGS\Q905317-SIG-PLAN.dwg - ST CHI obted: 2-08-10 @ 84-6am Py: 550km rs. psynight = 22009, by Boxter & Woodman, Inc. tet of Illinois - Professional Design Film.

- 06-26-09



	TRAFFIC S	I.D.O.T. SIGNAL INSTALL	ATION			
ELECTRICAL SERVICE REQUIREMENTS						
TYPE	NO LAMPS	LED WATTAGE :	× %OPERATION	TOTAL WATTAGE		
SIGNAL (RED)	12	17	0.50	102		
(YELLOW)	12	25	0.25	75		
(GREEN)	12	15	0.25	45		
ARROW	4	12	0.10	4.8		
PED. SIGNAL	8	25	1.00	200		
CONTROLLER	1	100	1.00	100		
ILLUM. SIGN	-	40	0.50	-		
STREET LIGHTS	-	250	0.50	-		
FLASHER		-	0.50	_		
DET. CAMERA	4	25	1.0	100		
	•		TOTAL =	326.8		

(ADDRESS) VILLAGE OF LOMBARD 255 E. WILSON AVE. LOMBARD, IL 60148-3931

COMMONWEALTH EDISION

OUNDATION (DEPTH) CABLE SLACK (FT.) (FT.) VERTICAL YPE A - POST HANDHOLE 6.5 ALL FOUNDATIONS D — CONTROLLER 4 DOUBLE HANDHOLE 13 MAST ARM (L) POLE 20'+L-2= E - M. ARM POLE SIGNAL POST 2 BRACKET MOUNTED 13 10" CONTROLLER CAB. 6 PED. PUSHBUTTON FIBER OPTIC ELECTRIC SERVICE 13.5 13 ELECTRIC SERVICE 1 SERVICE TO GROUND 13.5 GROUND CABLE 1 POST MOUNTED

(3)(3)(2)(5) (3)(2)R (R) G G G G ≺ 70 — (5) -(2)-ST. CHARLES ROAD (5) (E) (D) G (₹ (₹) (5) -(3)-**→**3 (7)— (x) > (0) (H) (H) റ ≺ മ — 5 -(2)- (7)- R (テ)の(H)(4) (3) (3) K Z (D)-(2) (3) \delta "D" **(2)** "C" (5) (2)(2)

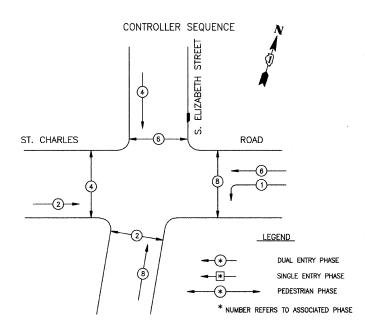
CABLE DIAGRAM

NOTE: PUSH BUTTON "A" PLACES A CALL TO PHASES 6 AND 8
PUSH BUTTON "B" PLACES A CALL TO PHASES 6 AND 4
PUSH BUTTON "C" PLACES A CALL TO PHASES 4 AND 2
PUSH BUTTON "D" PLACES A CALL TO PHASES 2 AND 8

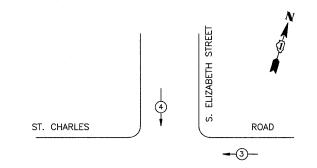
SCHEDULE OF QUANTITIES

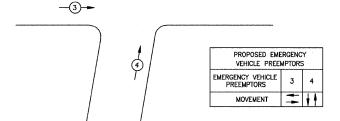
SCALE: NONE

DESCRI	PTION	UNIT	QUANTITY
MAINTE	NANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
MODIFY	EXISTING CONTROLLER	EACH	1
REMOVI	ELECTRIC CABLE FROM CONDUIT	FOOT	876
VIDEO	DETECTION SYSTEM, COMPLETE INTERSECTION	EACH	1
ELECTR	IC CABLE IN CONDUIT NO. 18 3C	FOOT	687
VIDEO	Transmission system	EACH	1



PHASE DESIGNATION DIAGRAM





EMERGENCY VEHICLE PREEMPTION SEQUENCE

OMBD\090317-St Char LAPP\CADD-SURVEY\DRAWINGS\DWGS\0903 ted: 2--08-10 @ 8456m BY: 560kar yright @-2008 By Baxter & Woodman, Inc. s of Illinois - Professional Design Firm nse No. - 184-001121 - Expires 4-30-09

B A X

ENERGY COSTS BILLED TO:

ENERGY SUPPLY CONTACT:

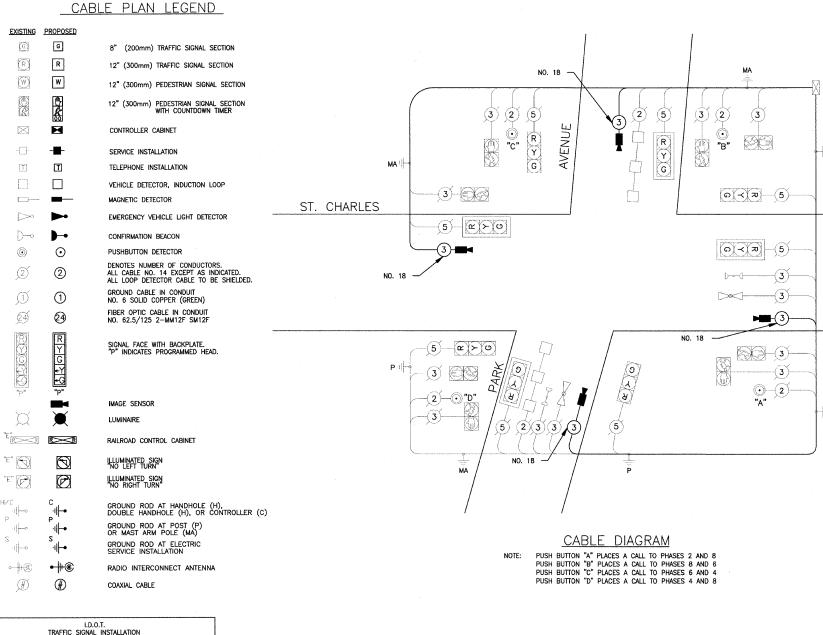
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL MODIFICATION CABLE PLAN				SECT	ON	COUNTY	TOTAL	SHEET NO.
ST. CHARLES ROAD		1397	09-0015	3-00-RS	DuPAGE	36	23	
OII OIRINEEO NOMB						CONTRACT	NO. 63	3351
PROJECT NO: 090317	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-ARA-1			9003(421)		

HORIZ. 1"=20'

PROJECT NO: 090317 STA.

- 06-26-09

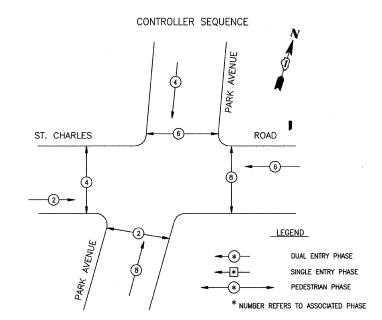


ELECTRICAL SERVICE REQUIREMENTS					SCHEDULE OF QUANTIT	IFS	
	NO LAMPS	LED × WATTAGE	x %OPERATION	TOTAL WATTAGE		ILO	
FD)	8	17	0.50	68	DESCRIPTION	UNIT	QUAN
LLOW)	8	25	0.25 0.25	50 30	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
REEN)	8	15	0.25		MODIFY EXISTING CONTROLLER	EACH	
	_	-		-	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	18
AL	8	25	1.00	200	VIDEO DETECTION SYSTEM, COMPLETE INTERSECTION	EACH	1
R	1	100	1.00	100	ELECTRIC CABLE IN CONDUIT, NO. 18 3C	FOOT	55
N	_	40	0.50		VIDEO TRANSMISSION SYSTEM	EACH	
SHTS	-	250	0.50	-	VIDEO INVANSAION SISTEM	DACI	1
	1	1	1	1 1			

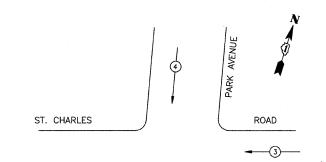
CABLE SLACK (FT.)

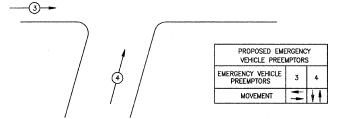
HANDHOLF

SIGNAL POST



PHASE DESIGNATION DIAGRAM DUAL ENTRY





EMERGENCY VEHICLE PREEMPTION SEQUENCE

		SERVICE REQUI		
TYPE	NO LAMPS	LED X		TOTAL WATTAGE
SIGNAL (RED)	8	17	0.50	68
(YELLOW)	8	25	0.25	50
(GREEN)	8	15	0.25	30
ARROW	-	-	-	-
PED. SIGNAL	8	25	1.00	200
CONTROLLER	1	100	1.00	100
ILLUM. SIGN	-	40	0.50	-
STREET LIGHTS	-	250	0.50	_
FLASHER	 		0.50	_
DET CAMERA	4	25	1.0	100

ENERGY COSTS BILLED TO: (ADDRESS) VILLAGE OF LOMBARD 255 E. WILSON AVE. LOMBARD, IL 60148-3931

PHONE: COMPANY: COMMONWEALTH EDISION

GROUND CABLE 1 POST MOUNTED IGNED - TMS SED - PER VILLAGE REVIEW 09-16-09 PER IDOT REVIEW 09-17-0 KAR HECKED -JJF 06-26-09

TOTAL = 548.0 FOUNDATION (DEPTH)

TYPE A - POST

D - CONTROLLER

30"

E - M. ARM POLE

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

TRAFFIC SIGNAL MODIFICATION CABLE PLAN ST. CHARLES ROAD

SECTION COUNTY 09-00153-00-RS DuPAGE 36 CONTRACT NO. 63351

(FT.)

CONTROLLER CAB. 6 PED. PUSHBUTTON

FIBER OPTIC 13 ELECTRIC SERVICE

ELECTRIC SERVICE 1 SERVICE TO GROUND

VERTICAL

2 BRACKET MOUNTED

DOUBLE HANDHOLE 13 MAST ARM (L) POLE 20'+L-2=

6.5 ALL FOUNDATIONS 3.5

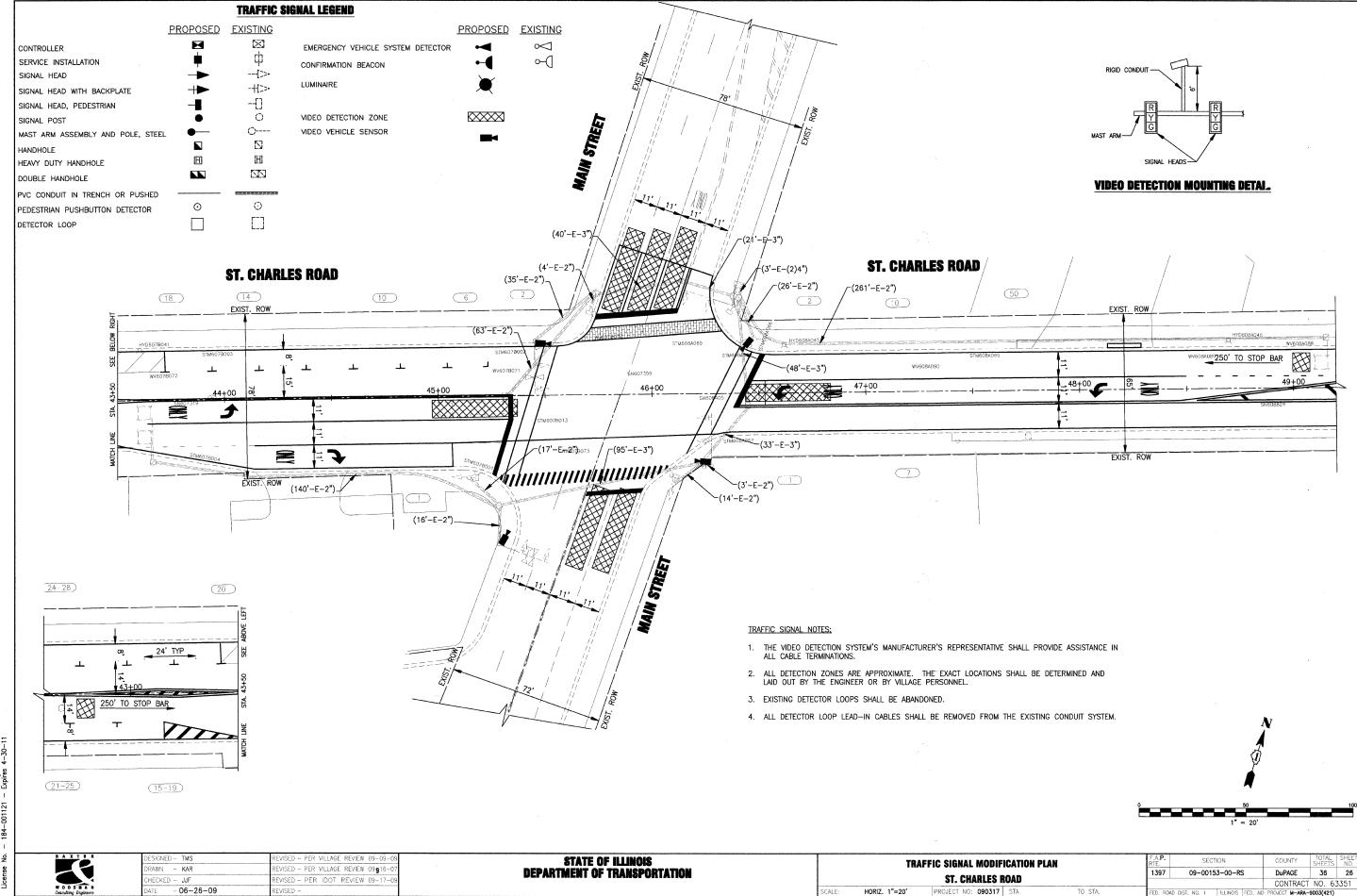
13.5

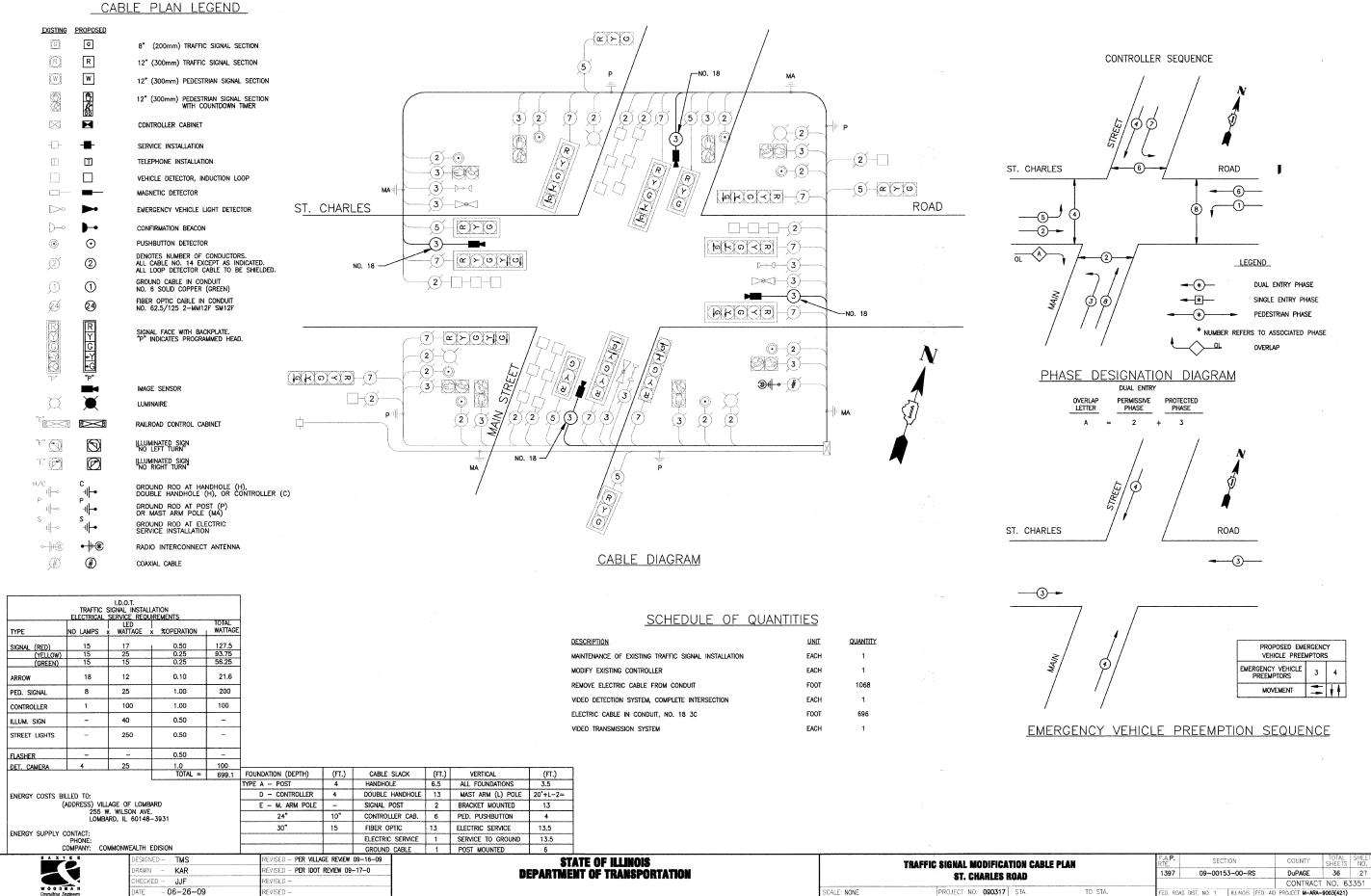
13.5

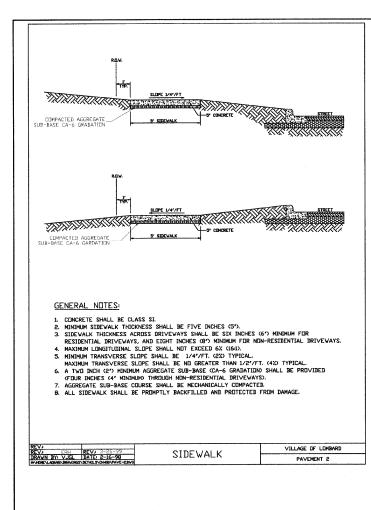
CALE: NONE

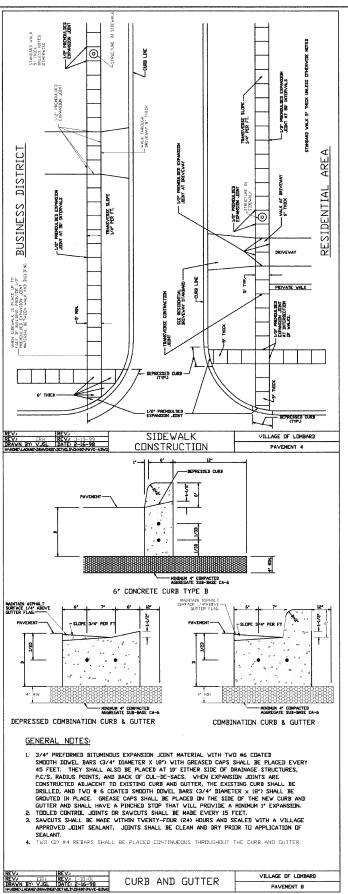
ROAD

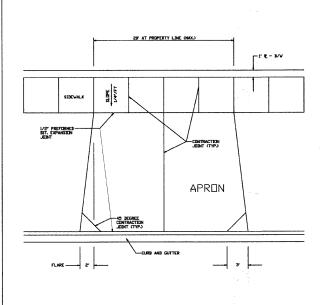
PROJECT NO: 090317 STA.











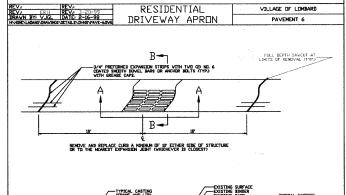
GENERAL NOTES:

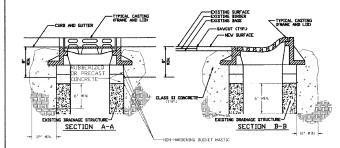
- I. APRONS SHALL NIT EXCEED 20 FEET IN VIDTH MEASURED AT THE RIGHT-DF-VAY LINE.
 2. ALL AGGREGATE SUB-BASE SHALL BE MECHANICALLY COMPACTED.
 3. MINIMUM THICKNESS FOR APRONS 6' P.C. CONCRETE ON 2" COMPACTED
 AGGREGATE SUB-BASE CAG-6 GRADATION, OR 3" BITUMINOUS SURFACE ON 6" COMPACTED
 AGGREGATE SUB-BASE CCA-6 GRADATION,
 4. SUB-VALX SHALL EXTEND THROUGH THE DRIVEVAY.
 5. DRIVEVAYS SHALL HAVE A MINIMUM SLOPE OF 2X AND A MAXIMUM SLOPE OF 8X.
 6. DRIVEVAYS SHALL HAVE A MINIMUM SLOPE OF 2X AND A MAXIMUM SLOPE OF 5X.
 7. PATCHES ARE NOT ALLOYDE IN NEV APRONS.

- RESIDENTIAL

VILLAGE OF LOMBARD

PAVEMENT 6

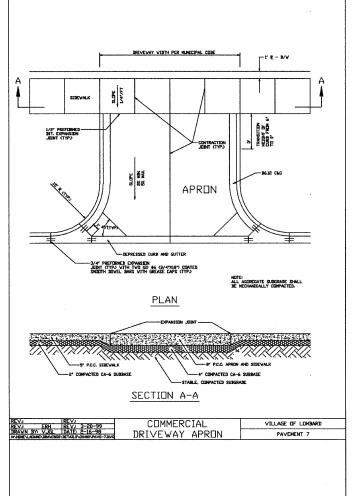


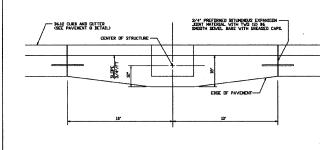


GENERAL NOTES:

- 2. PRECAST ADJUSTING RINGS SHALL BE REINFORCED WITH NO. 3 GAUGE WIRE OR EQUIVALENT AND SHALL HAVE A MINIMUM THICKNESS OF TWO (2) INCHES.
- MORTAR SHALL NOT BE USED TO DRESS UP ADJUSTING RINGS.
 ALL REMOVABLE CASTINGS SHALL BE DRIENTED SO THE OPENING IN THE GRATE PROVIDES THE MAXIMUM HYDRAULIC EFFICIENCY.

REV. 2 ERH REV. 12-06-05	CASTING ADJUSTMENTS FOR	VILLAGE OF LOMBARD
REV. ERH REV. 3~16-99	CHOLING HOOGSTHENTS LOK	VILLIAGE DI EDITORIA
DRAVN BY: VJGL DATE: 2-16-98	STRUCTURES IN THE CURB LINE	8 MADTS
	SINGEIGNES IN THE COND LINE	DIDNI O





GENERAL NOTES:

STORM SEVER CASTING SHALL BE NEENAH R-3278-1, EAST JORDAN 7221, OR EQUIVALENT AS APPROVED BY THE VILLAGE ENGINEER.

REV. ERH REV. 3-20-99	STORM	SEWER	? INLET	VILLAGE OF LOMBARD
DRAWN BY: VJGL DATE: 2-16-98	CURB	VND G	HTTER	PAVEMENT 9
NYEMENLACIANGNORANDIGSNOETAX.SNCH400NPAVE-9.DWG	COND	עווא ע	UTTEN	THYCHEN

RAWN - KAR REVISED -HECKED - JUF REVISED -- 06-26-09 EVISED

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

SECTION **VILLAGE OF LOMBARD STANDARD DETAILS** 1397 09-00153-00-RS DuPAGE · 36 28 CONTRACT NO. 63351 LE: HORIZ. 1"=20' / VERT. 1"=5' PROJECT NO: 090317 | STA. TO STA

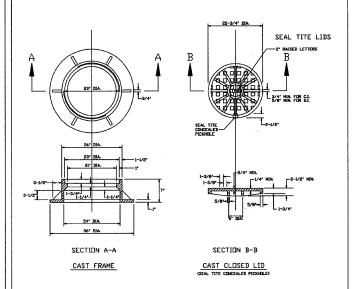
GENERAL NOTES:

- PROVIDE PRECAST REINFORCED CONCRETE BARREL AND RISER SECTION.
 CONCRETE BLOCK CONSTRUCTION IS NOT PERHITED.
 PROVIDE GRANULAR BACKFILL AROUND INLET TO SUBGRADE ELEVATION IN PAVED AREAS, MATERIAL SHALL MEET THE REQUIREMENTS OF IDDIT "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" FOR COARSE AGGREGATE (CA-6 GRADATION.
- 3. WHEN THE FRAME DOES NOT MEET PROPOSED ELEVATION, A HINNHAM OF TVD TAPERED RUSSERIZED ADJUSTING RINGS SHALL BE USED FOR FINAL ADJUSTMENT. DHE CONCRETE RING NOT LESS THAN TVD INCHES THICK HAY ALSD BE USED. A MAXIMUM OF THREE ADJUSTING RINGS MAY BE USED TO A MAXIMUM HEIGHT OF 12 INCHES. EACH RING AND THE FRAME SHALL BE SET IN A BED OF NON-PREFORMED MASTIC.

 4. PRECAST ADJUSTING RINGS SHALL BE RETRIRECED VITH ND. 3 GAUGE VIRE OR EQUIVALENT AND SHALL HAYE A MINIMUM THICKNESS OF TVD INCHES.
- 5. VITHIN NON-PAVED AREAS, MORTAR SHALL DNLY BE USED TO DRESS UP ADJUSTING RINGS AND/OR FRAME ON THE EXTERIOR OF THE STRUCTURE MORTAR IS NOT PERMITTED ON THE INSIDE OF THE RINGS AND/OR FRAME.

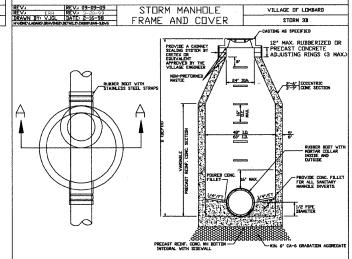
 6. IN PAVED AREAS, DRAIN HOLES/VEEP HOLES SHALL BE COVERED VITH FILTER FABRIC. FILTER FABRIC SHALL BE SECURED TO THE OUTSIDE OF STRUCTURE PRIOR TO BACKFILL.
- 7. IN GRASSED AREAS, DRAIN HOLES/WEEP HOLES SHALL BE PLUGGED WITH HYDRAULIC CEMENT.
- 8. IF AN IDDT TYPE 8 GRATE CASTING IS CALLED DUT, THE FRAME SHALL BE SET IN A BED OF PREFORMED MASTIC.

REV ERH REV 01-06-09 REV ERH REV 3-14-99 DRAVN BY VJG. DATE: 2-16-98 VILLAGE OF LONBARD INLET TYPE A



GENERAL NOTES

- DUCTILE IRON CASTING SHALL BE TESTED IN ACCORDANCE WITH FEDERAL SPECIFICATIONS.
 ALL FRAMES AND COVERS SHALL HAVE A MACHINED HORIZONTAL AND VERTICAL BEARING
 SURFACES. PICK HOLES IN THE COVER SHALL NOT BE OPEN.
 THE MANHOLE COVERS SHALL HAVE RAISED LETTERS AS SHOWN.
 DIMENSIONS FOR CASTINGS ARE CUPPARABLE TO EAST JORDAN IRON VORKS, INC. 1022—3 OR NEENAH
 FOUNDRY 1772—C FURNISHED WITH TYPE F CONCEALED PICK HOLES.
- 5. VATERPROOF, BOLTDOWN FRAME AND COVER SHALL BE USED IN ANY LOCATION SUBJECT TO INVINDATION. CNEMAR R-1916-F, EAST JORDAN 1022-3 WT VITH TYPE 5 CLOSED PICK HOLES OR APPROVED EQUAL).



PLAN GENERAL NOTES

1. PRIVIDE PRECAST REINFORCED CONCRETE BARREL AND RISER SECTION.

2. PRIVIDE SELECT GRANULAR BACKFILL ARDIND MANNICLE TO SUBGRADE ELEVATION IN PAVED AREAS.

MATERIAL SHALL MEET THE REQUIREMENTS OF IDDIT "STANDARD SPECIFICATIONS" FOR RADIA AND BRIDGE CONSTRUCTION" COURSE AGGREGATE CA—6 GRADATION, OR AS CINERAVISE DIRECTED BY THE VILLAGE.

SECTION A-A

- ENGINEER

 3 APPLY A CONTINUOUS LAYER OF NON-HARDENING PREFORMED BITUMINOUS MASTIC MATERIAL (RUB-R-NEK OR EZ STICK) TO EACH JOINT BELOW THE BOTTOM OF CONE OR FLATTOP TO PREVENT INFLOV.
- 4. WHEN THE FRAME DOES NOT MEET PROPOSED ELEVATION, A MINIMUM OF TVO TAPERED RUBBERIZED ADJUSTING RINGS SHALL BE USED FOR FINAL ADJUSTMENT. DINE CONCRETE RING RUT LESS THAN TVO INCHES THICK MAY ALSO BE USED, A MAXIMUM OF THREE ADJUSTING RINGS MAY BE USED TO A MAXIMUM HEIGHT OF 12 INCHES, EACH RING AND THE FRAME SHALL BE SET ON A BED OF NON-PREFORMEN MASTIC.
- 5. PRECAST ADJUSTING RINGS SHALL BE REINFORCED WITH NO. 3 GAUGE WIRE OR EQUIVALENT.
- EGOLVALENT.

 MORTAR SHALL NOT BE USED TO DRESS UP ADJUSTING RINGS AND/OR FRAME.

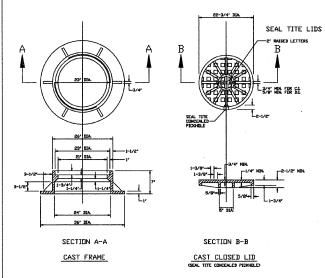
 ONLY PLASTIC POLYMER STEPS SHALL BE USED.
- 7. UNLT PLASTIC PULTMEN STEPS SHALL BE USED!

 8. WHEN MANHOLE DEPTH IS UTVER IS FEET, THE THICKNESS OF THE BASE SHALL BE A
 MITHIBUT OF 10 INCHES. WHEN MANHOLE DEPTH IS LESS THAN 12 FEET, THE THICKNESS
 SHALL BE A MINIMUM OF 8 INCHES.

 9. DRESS UP INTERIOR JOINTS WITH HYDRAULIC CEMENT.

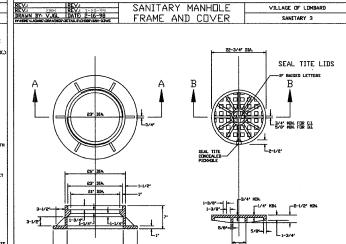
SANITARY MANHULE

VILLAGE OF LOMBARD SANITARY 1



GENERAL NOTES

- DUCTILE IRON CASTING SHALL BE TESTED IN ACCORDANCE WITH FEDERAL SPECIFICATIONS.
 ALL FRAMES AND COVERS SHALL HAVE A MACHINED HORIZONTAL AND VERTICAL BEARING SURFACES. PICK HOLES IN THE COVER SHALL NOT BE DPEN.
 THE MANHOLE COVERS SHALL HAVE RAISED LETTERS AS SHOWN.
 DIMENSIONS FOR CASTINGS ARE CUPMARABLE TO EAST JORDAN IRON VORKS, INC. 1022-3 OR NEENAH FOUNDRY 1772-C FURNISHED WITH TYPE F CONCEALED PICK HOLES.
- 5. WATERPROOF, BOLTDOWN FRAME AND COVER SHALL BE USED IN ANY LOCATION SUBJECT TO INUNDATION. (NEENAH R-1916-F, EAST JORDAN 1022-3 VT VITH TYPE 5 CLOSED PICK



1. DUCTILE IRON CASTING SHALL BE TESTED IN ACCORDANCE WITH FEDERAL SPECIFICATIONS.

SECTION B-B

CAST CLUSED LID

- 2. ALL FRAMES AND COVERS SHALL HAVE A MACHINED HDRIZONTAL AND VERTICAL BEARING
- SURFACES. PICK HOLES IN THE COVER SHALL NOT BE OPEN.

 THE MANHOLE COVERS SHALL HAVE RAISED LETTERS AS SHOWN.

SECTION A-A

CAST FRAME

- DIMENSIONS FOR CASTINGS ARE COMPARABLE TO EAST JORDAN 1022-3 OR NEENAH FOUNDRY 1772-C.
- WATERPROOF, BOLTDOWN FRAME AND COVER SHALL BE USED IN ANY LOCATION SUBJECT TO INUNDATION. (NEENAH R-1916-C, EAST JORDAN 1022-3 VT OR APPROVED EQUAL).

١			*
1			
ı	REV. REV. 3-20-99	VALVE VALLE	VILLAGE OF LONBARD
4	TRAVAL BY V ICI TATE 2-16-99	EDAME AND OFFICE	
1	HVHENEYLADANGYJRAVINGSYJETAILSYCHSOOYVATER-SJIVG	FRAME AND CLIVER	WATER 5



VISED - PER VILLAGE REVIEW 09-09-ESIGNED - TMS RAWN - KAR EVISED - PER VILLAGE REVEW 01-18-HECKED - JJF VISED -06-26-09

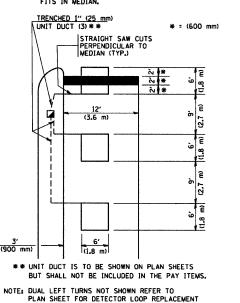
STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

SECTION COLINTY **VILLAGE OF LOMBARD STANDARD DETAILS** 1397 09-00153-00-RS DuPAGE 36 29 CONTRACT NO. 63351 CALE: HORIZ. 1"=20' / VERT. 1"=5' PROJECT NO: 090317 STA. TO STA.

<u>LEFT TURN LANES WITH MEDIANS</u> VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
BI4001 TO ENSURE THAT HANDHOLE



LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

= (600 mm)

| CELLOW | CELLO

STRAIGHT SAW CUT TO HEAVY
DUTY HANDHOLE (TYP.) PLACE HEAVY
DUTY HANDHOLE BETWEEN FIRST AND
SECOND LOOP AS SHOWN.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

SCALE: NONE

* = (600 mm)

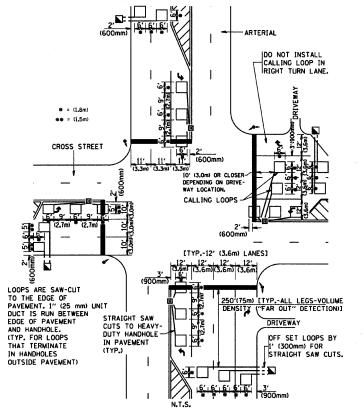
* * UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS

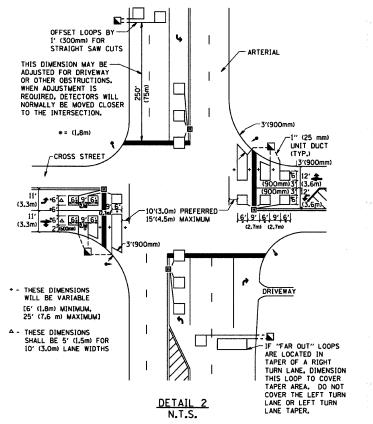
(3.0 m)

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)

1" (25 mm) UNIT DUCT-TRENCHED TO E/P ••

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)





NOTES

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX, EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF <u>ALL</u> DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- ** WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON $\underline{\mathsf{ALL}}$ SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

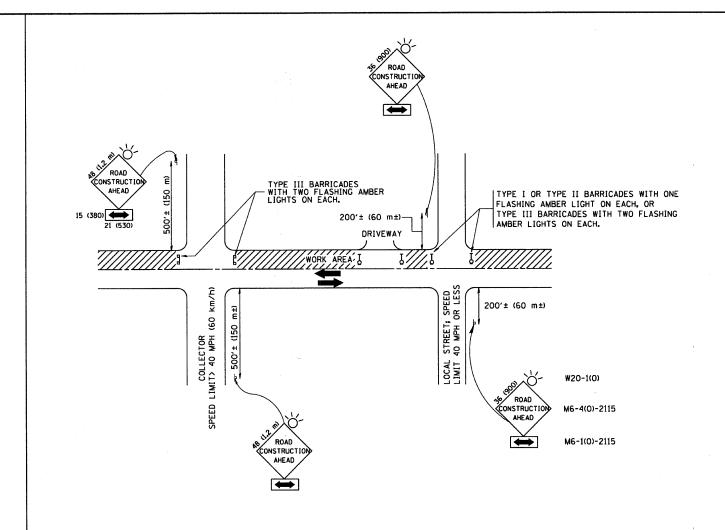
ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = gaglianobt	DESIGNED -	REVISED -				
W:\diststd\22x34\ts07.dgn		DRAWN -	REVISED -				
	PLOT SCALE = 50.0000 '/ IN.	CHECKED - R.K.F.	REVISED -				
	PLOT DATE = 1/4/2008	DATE -	REVISED -				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 – DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING		SECTION	COUNTY	TOTAL SHEETS	SHE
		09-00153-00-RS	DuPAGE	36	3
		TS07	CONTRACT	NO. 63	351
SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. R	DAD DIST. NO. 1 ILLINOIS FED. A	D PROJECT M-A	RA-900	3 (4:



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- O) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY
 BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF
 THE CROSS SECTION OF THE CLOSED PORTION.
- 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- O) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY
 BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION
 OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (MG-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (MG-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

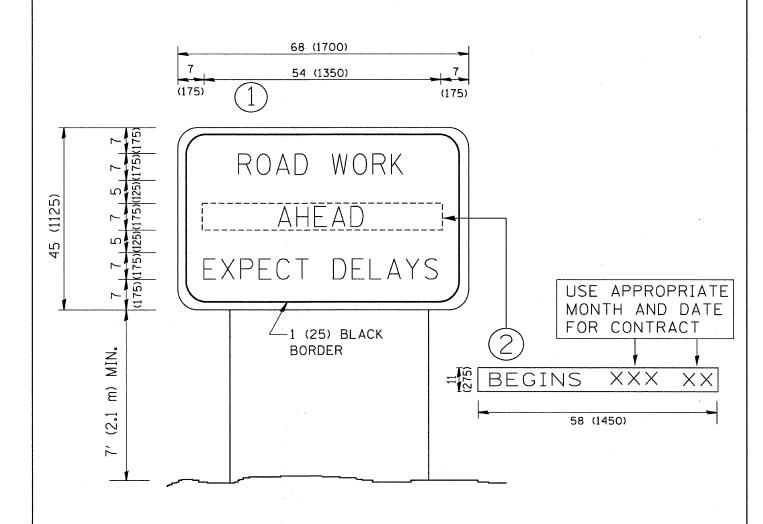
All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = USER NAME = geglionobt DESIGNED - LHA REVISED - J. OBERLE 10-18-95
Wi\distatd\22x34\to10.dgn

DRAWN - REVISED - A. HOUSEH 03-06-96
PLOT SCALE = 58.000 '/ IN. CHECKED - REVISED - A. HOUSEH 10-15-96
PLOT DATE = 1/4/2008 DATE - 06-89 REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	1397	09-00153-00-RS	DuPAGE	36	31
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS		TC-10	CONTRACT	NO. 63	351
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. R	OAD DIST. NO. 1 ILLINOIS FED. A	ID PROJECT M-7	RA-900	3(421)

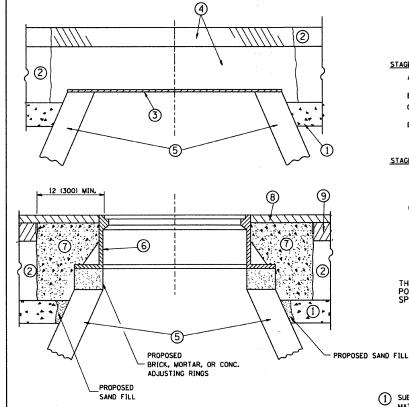


NOTES:

- 1. USE BLACK LETTERING ON ORANGE BACKGROUND.
- 2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
- 3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
- 4. REMOVE PANEL 2 SOON AFTER THE START OF CONSTRUCTION.
- 5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
- 6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
- 7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gaglianobt	DESIGNED -	REVISED - R. MIRS 09-	5-97					ADTED	IAL RO	A.D.		F.A.P.	SECTION	COUNTY	TOTAL SHEET
W:\diststd\22x34\tc22.dgn		DRAWN -	REVISED - R. MIRS 12-	1-97	STATE OF ILLINOIS								1397	09-00153-00-RS	DuPAGE	36 32
	PLOT SCALE = 50.000 '/ IN.	CHECKED -	REVISED -T. RAMMACHER	2-02-99	DEPARTMENT OF TRANSPORTATION			11/	NFUKM	IATION	SIGN			TC-22	CONTRAC	CT NO. 63351
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS O	-31-07		SCALE: NONE	SHEET NO.	. 1 OF	1 SH	HEETS	STA.	TO STA.	FED. R	OAD DIST. NO. 1 ILLINOIS FED.	AID PROJECT M	-ARA-9003(421



EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

NOTES:

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY AND THE CONTRACTOR SHALL NOTHEY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED,
THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL
NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE
COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 11/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

l F

- 1 SUB-BASE GRANULAR MATERIAL
- 2 EXISTING PAVEMENT
- 3 36 (900) DIAMETER METAL PLATE
- PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- 5 EXISTING STRUCTURE
- 6 FRAME AND LID (SEE NOTES)
- CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- 8 PROPOSED HMA SURFACE COURSE
- 9 PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

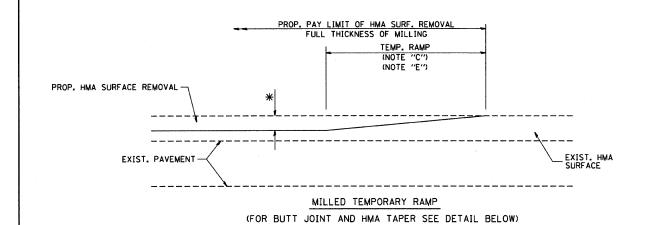
FILE NAME = USER NAME = gaglionobt DESIGNED - R, SHAH REVISED - R, SHAH 03-10-95

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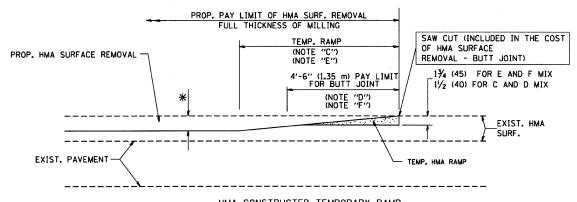
PLOT SCALE = 50.0000 '/ IN. CHECKED - REVISED - R, WIEDEMAN 05-14-04

PLOT DATE = 1/4/2000 DATE - 10-25-94 REVISED - R, BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



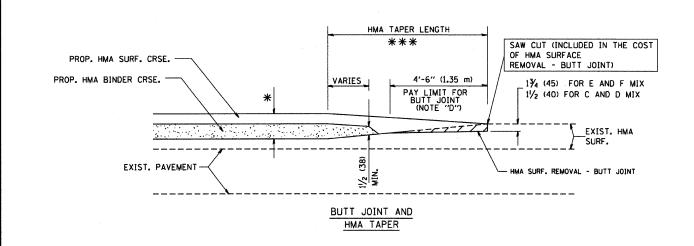
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP (FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP

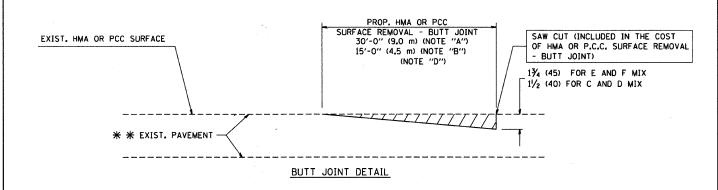


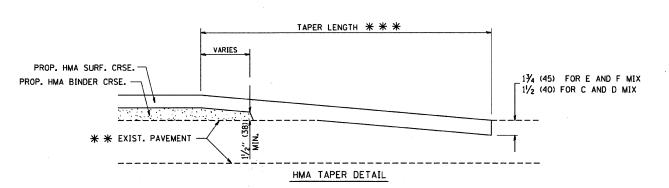
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

FILE NAME = DESIGNED - M. DE YONG REVISED - R. SHAH 10-25-94 USER NAME = gaglianobt v:\diststd\22×34\bd32.dgn REVISED - A. ABBAS 03-21-97 CHECKED -REVISED - M. GOMEZ 04-06-01 PLOT SCALE = 50.0000 '/ IN. PLOT DATE = 1/4/2008 DATE - 06-13-90 REVISED - R. BORO 01-01-07

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

TOTAL SHEE SHEETS NO. SECTION COUNTY **BUTT JOINT AND** 1397 09-00153-00-RS HMA TAPER DETAILS CONTRACT NO. 63351 BD400-05 BD32 SHEET NO. 1 OF 1 SHEETS STA. SCALE: NONE TO STA.





TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

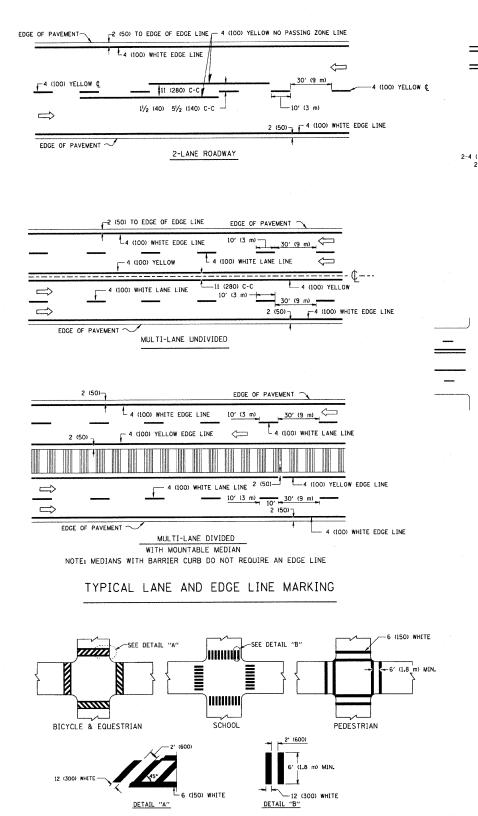
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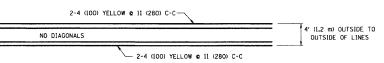
- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-O" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- ** * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A") 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

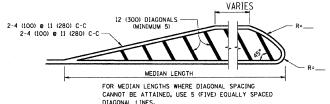
THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SOUARE YARD (SOUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



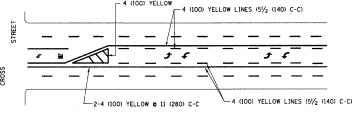


4' (1.2 m) WIDE MEDIANS ONLY

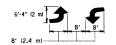


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

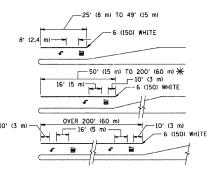


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

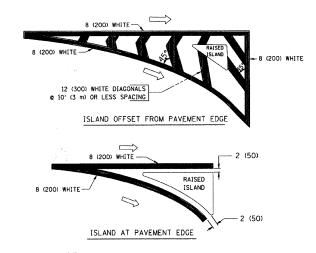


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. \P AREA = 15.6 SO. FT. (1.5 m²) \P AREA = 20.8 SO. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 & 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 e 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW: EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 & 4 (100) WITH 12 (300) DIAGONALS & 45° NO DIAGONALS USED FOR 4' (1,2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS & 45°	SOLID	WHITE	DIACONALS: 15' (4,5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) T0 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"-3.6 SO. FT. (0.33 m ²) EACH "X"-54.0 SO. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) & 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (0VER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

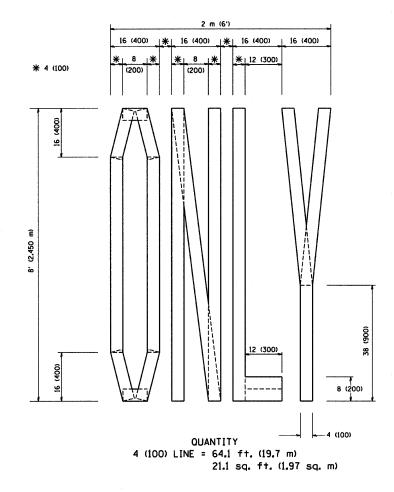
All dimensions are in inches (millimeters) unless otherwise shown.

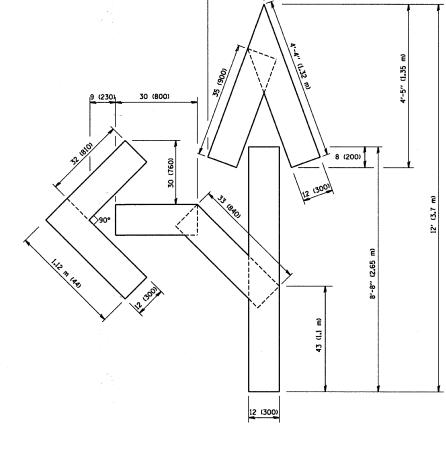
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c:\pw_work\pwidot\drivakosgn\d0108315\ta	3.dgn	DRAWN -	REVISED -C. JUCIUS 09-09-09
	PLOT SCALE = 50.000 '/ IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

TYPICAL CROSSWALK MARKING

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

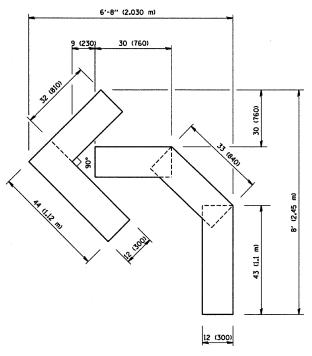
	DIS	STRICT OF	VE		F.A.P. RTE.	SECTION	COUNTY	TOTAL	SHEET NO.
	TYPICAL PA		MADVINGS		1397	09-00153-00-RS	DuPAGE	36	35
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SCALE: NONE	SHEET NO. 1 OF 1	SHEETS	STA.	TO STA.	FED. RO	AD DIST. NO. 1 ILLINOIS FED. A	ID PROJECT M-A	RA-9003	(421)





1'-8" (500)

QUANTITY 4 (100) LINE = 82.5 ft. (25.3 m) 27.5 sq. ft. (2.53 sq. m)



OUANTITY 4 (100) LINE = 45.5 ft. (13.9 m) 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = gaglianobt	DESIGNED -	REVISED -T. RAMMACHER 06-05-96		PAVEMENT MARKING LETTERS AND SYMBOLS	F.A.P. SECTION	COUNTY TOTAL SHEET
W:\diststd\22x34\tc16.dgn		DRAWN -	REVISED -T. RAMMACHER 11-04-97	STATE OF ILLINOIS		1397 09-00153-00-RS	DuPAGE 36 36
	PLOT SCALE = 50.0000 '/ IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98	DEPARTMENT OF TRANSPORTATION	FOR TRAFFIC STAGING	TC-16	CONTRACT NO. 63351
	PLOT DATE = 1/4/2008	DATE - 09-18-94	REVISED - E. GOMEZ 08-28-00		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. A	AID PROJECT M-ARA-9003 (421)