4-23-10 LETTING ITEM 057

INDEX OF SHEETS

STATE STANDARDS

SEE SHEET 2

DESIGN DESIGNATION: 11,300(2009) MINOR ARTERIAL 0.88(COMP-20)

TRAFFIC DATA
ADT: CENTRAL AVENUE 11,300 (2009)

POSTED SPEED	DESIGN SPEED
25 MPH (EXISTING)	25 MPH (EXISTING)
25 MPH (PROPOSED)	25 MPH (PROPOSED)

0 10' 20' 30' 100' 200' 300' 1"=100' 1"=10'
0 50' 100' 1"=50'
0 50' 100' 1"=40'
0 50' 100' 1"=30'
0 50' 100' 1"=20'

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
CALL 811
811 Know what's below. Call before you dig.

Frank Novotny & Associates, Inc.
Civil Engineers/ Municipal Consultants
825 Midway Drive ◆ Willowbrook, IL ◆ 60527 ◆ Telephone: (630) 887-9640 ◆ Fax: (630) 887-0132
ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-000928

REVISIONS

NO.	BY	DATE	DESCRIPTION
1	AMS	1/22/10	PER IDOT REVIEW

CONTRACT NO. 63445

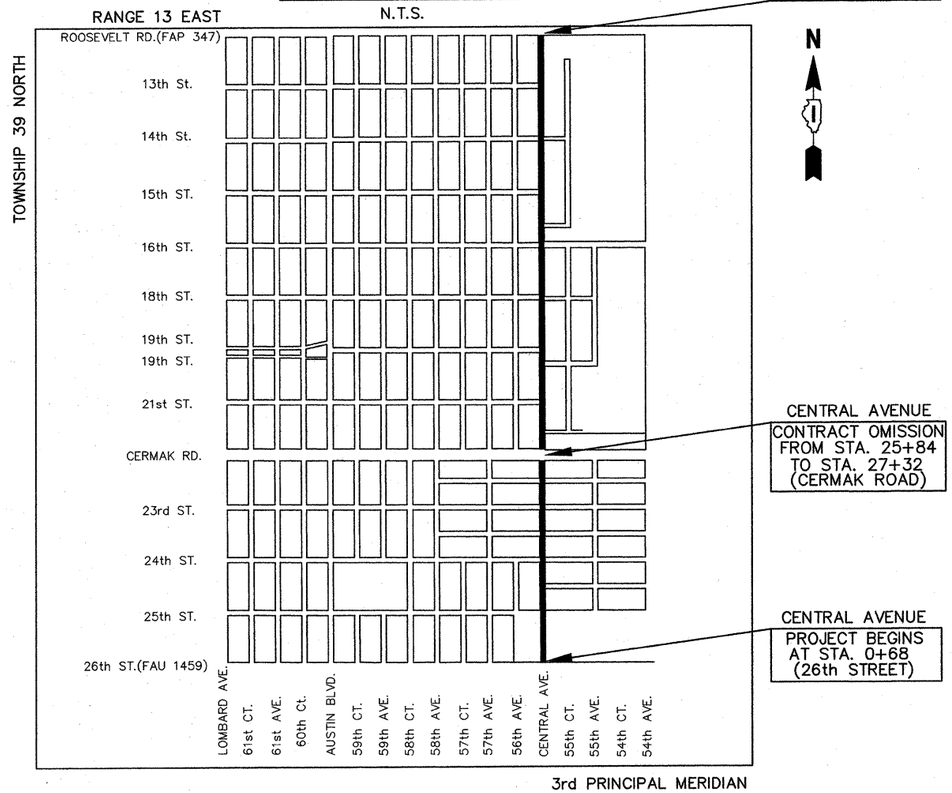
FIELD ENGINEER: MARILIN SOLOMON (847)705-4407

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

CENTRAL AVE. (FAU 2798)
26TH ST. (FAU 1459) TO ROOSEVELT RD. (FAP 347)
RESURFACING
PROJECT M-9003(488)
SECTION 03-00193-00-FP
TOWN OF CICERO
COOK COUNTY
C-91-076-10

PROJECT LOCATION MAP



DENOTES LOCATION OF IMPROVEMENT
IN CICERO TOWNSHIP

LENGTH OF PROJECT

GROSS LENGTH OF PROJECT	7,895 FEET (1.4953 MILES)
NET LENGTH OF PROJECT	7,747 FEET (1.4670 MILES)

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	1
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63445

LOCATION OF SECTION INDICATED THUS: -■-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED 1-21 2010

TOWN PRESIDENT _____
LARRY DOMINICK, PRESIDENT, CICERO

PASSED JANUARY 29, 2010

_____
CHRISTOPHER HOLT
DISTRICT #1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW FEBRUARY 2, 2010

_____
DIANE M. O'KEEFE
DEPUTY DIRECTOR OF HIGHWAYS, REGION #1 ENGINEER

_____
DANA M. SCHNABEL, P.E.
IL. P.E. NO. 062-054043
EXPIRES 11-30-2011
1-22-10
DATE

DANA M. SCHNABEL 062-054043 LICENSED PROFESSIONAL ENGINEER OF ILLINOIS
SEAL

_____
TIMOTHY H. KLASS, P.E.
IL. P.E. NO. 062-046111
EXPIRES 11-30-2011
1-22-10
DATE

TIMOTHY H. KLASS 062-046111 LICENSED PROFESSIONAL ENGINEER OF ILLINOIS
SEAL

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

FNA JOB # 05043

STATE STANDARDS

000001-05	STANDARD SYMBOLS, ABBREVATIONS, AND PATTERNS
280001-05	TEMPORARY EROSION CONTROL SYSTEMS
424001-05	CURB RAMPS FOR SIDEWALK
442201-03	CLASS C AND D PATCHES
604001-03	FRAMES & LIDS-TYPE 1
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB & GUTTER
701301-03	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS-DAY ONLY
701501-05	URBAN LANE CLOSURE, 2 L, 2 W UNDIVIDED
701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-04	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNT DETAILS
780001-02	TYPICAL PAVEMENT MARKINGS
814001-02	HANDHOLES
814006-02	DOUBLE HANDHOLES
857001-01	STANDARD PHASE DESIGNATION DIAGRAMS & PHASE SEQUENCES
862001-01	UPS
873001-02	TRAFFIC SIGNAL GROUNDING & BONDING
876001-01	PEDESTRIAN PUSH BUTTON POST
877001-04	STEEL MAST ARM ASSEMBLY & POLE 16' THROUGH 55'
878001-08	CONCRETE FOUNDATION DETAILS
880001-01	SPAN WIRE MOUNT SIGNALS & FLASHING BEACON INSTALLATION
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS

SPECIAL PROJECT NOTES

1) DRIVEWAY REPAIR BETWEEN THE CURB AND THE PROPERTY LINE SHALL BE COMPLETED PER THE TOWN ORDINANCE. THE INSTALLATION IS DETAILED IN THESE PLANS AND IN THE SPECIAL PROVISIONS.

2) ALL PATCHING WILL BE MARKED OUT AND CONSTRUCTED AFTER MILLING. A PROOF ROLL WILL BE REQUIRED.

3) ALL SAWCUTS SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEMS FOR WHICH THE WORK APPLIES.

4) ALL EXISTING FRAMES AND LIDS THAT ARE TO BE REPLACED (AS DIRECTED BY THE ENGINEER), SHALL BE SALVAGED TO THE CONTRACTOR.

5) ALL METERS, VALVES, AND BUFFALO BOXES WITHIN SIDEWALK AND DRIVEWAY REMOVAL LIMITS SHALL BE ADJUSTED.

6) ALL AT&T MANHOLES TO BE ADJUSTED (BY OTHERS).

7) MEET EXISTING CURB AND FLOW LINE ELEVATIONS AT SIDE STREET APPROACHES.

8) ALL CURBLINE INLETS AND CATCH BASINS ON THIS PROJECT FLOW TO A COMBINED SEWER. ALL WORK SHALL CONFORM TO ILLINOIS DEPARTMENT OF TRANSPORTATION AND THE METROPOLITAN WATER RECLAMATION DISTRICT OF GREATER CHICAGO (M.W.R.D.) STANDARDS. THE FINAL OUT FLOWING PIPE FROM ANY INLET OR CATCH BASIN STRUCTURE THAT WILL FLOW TO THE COMBINED SEWER SHALL BE TRAPPED AS DETAILED ON SHEET 5. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE REPLACEMENT OF THE PIPE.

9) ABANDONED STORM SEWER PIPE SHALL BE PLUGGED WITH CONCRETE MORTAR. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE REPLACEMENT OF THE PIPE.

10) METHOD 1, AS DESCRIBED IN ARTICLE 550.07 OF THE STANDARD SPECIFICATIONS SHALL BE USED TO COMPACT TRENCHES FOR ALL STORM SEWER PIPE INSTALLATION.

11) PROPOSED PORTLAND CEMENT CONCRETE SURFACE REMOVAL, VARIABLE DEPTH, SHALL BE MEASURED IN PLACE AT THE TIME OF CONSTRUCTION. DURING CONSTRUCTION OF THE HOT-MIX SURFACE REMOVAL, VARIABLE DEPTH, PAY ITEM, A SMALL AMOUNT OF PCC BASE COURSE IS EXPECTED TO BE ENCOUNTERED, (SEE CORE INFORMATION ON SHEET 4). 3,066 SY HAS BEEN ESTIMATED FOR THIS WORK FOR BIDDING. EXACT AREA WILL BE DETERMINED AT THE TIME OF CONSTRUCTION.

MWRDGC NOTES

METROPOLITAN WATER RECLAMATION DISTRICT OF GREATER CHICAGO
LOCAL SEWER SYSTEMS SECTION

TYPICAL GENERAL NOTES

1. The MWRD Local Sewer Systems Section Field Office must be notified at least two (2) working days prior to the commencement of any work (call 708/588-4055).
2. Elevation datum is U.S.G.S.
   Conversion equation is N/A
3. All floor drains shall discharge to the sanitary sewer system. (NOT APPLICABLE)
4. All downspouts and footing drains shall discharge to the storm sewer system. (NOT APPLICABLE)
5. All sanitary sewer pipe materials and joints (and storm sewer pipe materials and joints in a combined sewer area) shall conform to:

Pipe Material Spec.	Joint Spec.
Vitrified Clay Pipe	
VCP (C-700)	C-425
VCP (No-Bel)(C-700)	
Joint	C-425
Collar	D-1784
Concrete Pipe (C-14)	C-443
RCP (C-76)	C-443
ACP (C-428)	D-1869
ABS Sewer Pipe	
Solid Wall 6" dia. SDR 23.5	
ABS D-2751	D-2751
ABS Composite/Truss Pipe	
8" - 15" dia.	
ABS D-2680	D-2680
PVC Gravity Sewer Pipe	
6" - 15" dia. SDR 26	
D-2241	D-3139
AWWA-C-900	D-3139
18" - 27" dia. F/dy=46	
F-679	D-3212 or D-2855
CISP A-74	C-564
DIP A-21.51	A-21.11

(Note: The District has approved less common pipe materials on a qualified basis in addition to those above. Please contact the District if considering using pipe not listed above.)

6. All sanitary sewer construction (and storm sewer construction in combined sewer areas), requires stone bedding with stone 1/4" to 1" in size, with minimum bedding thickness equal to 1/4 the outside diameter of the sewer pipe, but not less than four (4) inches nor more than eight (8) inches. Materials shall be CA-11 or CA-13 and shall be extended at least 12" above the top of the pipe when using PVC.
7. "Band-Seal" or similar flexible-type couplings shall be used in the connection of sewer pipe of dissimilar materials.
8. When connecting to an existing sewer main by means other than an existing wye, tee, or an existing manhole, one of the following methods shall be used:
   1. Circular saw-cut of sewer main by proper tools ("Shewer-Tap" machine or similar) and proper installation of hub-wye saddle or hub-tee saddle.
   2. Remove an entire section of pipe (breaking only the top of one bell) and replace with a wye or tee branch section.
   3. With pipe cutter, neatly and accurately cut out desired length of pipe for insertion of proper fitting, using "Band-Seal" or similar couplings to hold it firmly in place.
9. Wherever a sanitary/combined sewer crosses under a watermain, the minimum vertical distance from the top of the sewer to the bottom of the water man shall be 18 inches. Furthermore, a minimum horizontal distance of 10 feet between sanitary/combined sewers and watermains shall be maintained unless: the sewer is laid in a separate trench, keeping a minimum 18" vertical separation; or the sewer is laid in the same trench with a watermain located at the opposite side on a bench of undisturbed earth, keeping a minimum 18" vertical separation. If either the vertical or horizontal distances described above cannot be maintained or the sewer crosses above the watermain, the sewer shall be constructed to watermain standards.
10. All existing septic systems shall be abandoned. Abandoned tanks shall be filled with granular material or removed.
11. All sanitary manholes, and also storm manholes in combined sewer areas, shall have a minimum inside diameter of 48 inches, and shall be cast-in-place or pre-cast reinforced concrete. Resilient connectors, conforming to ASTM C-923, shall be used between manhole and pipe(s) for all sanitary and combined sewer structures.

GENERAL CONSTRUCTION NOTES
PAVING AND STORM SEWERS

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	2
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63445

SPECIFICATIONS

THE LATEST EDITIONS OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", PREPARED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION AND THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" SHALL GOVERN ALL WORK ASSOCIATED WITH THIS PROJECT. THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" MAY GOVERN OTHER WORK ON THIS PROJECT AS INDICATED BY REFERENCE.

CARE IN EXCAVATION

CARE SHALL BE EXERCISED BY THE CONTRACTOR IN CARRYING OUT EARTH AND/OR TRENCHING OPERATIONS SO THAT LOCAL UTILITY SERVICES, WATER VALVES, MANHOLES, CATCH BASINS, INLETS, BUFFALO BOXES, AND OTHER STRUCTURES ARE NOT DAMAGED OR REMOVED. ANY DAMAGE DONE BY THE CONTRACTOR, WHETHER THE STRUCTURE OR SERVICE IS VISIBLE AT THE GROUND SURFACE OR NOT, SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR AS REQUIRED BY THE ENGINEER AT THE CONTRACTOR'S EXPENSE, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

NOTIFICATION OF PUBLIC UTILITIES

PRIOR TO STARTING CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE OFFICIALS OF THE PUBLIC WORKS DEPARTMENT OF THE LOCAL MUNICIPALITY, J.U.L.I.E. AT 1-800-892-0123 OR 811, AND OTHER PUBLIC AND PRIVATE UTILITIES SO THAT ARRANGEMENTS CAN BE MADE TO LOCATE THEIR VARIOUS FACILITIES WITHIN THE LIMITS OF CONSTRUCTION UNDER THIS CONTRACT, AS WELL AS TO PROVIDE ADEQUATE PROTECTION AND INSPECTION THERETO. IT SHALL BE THIS CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES IN THE FIELD.

TRAFFIC CONTROL DEVICES

BARRICADES AND WARNING SIGNS SHALL BE PROVIDED IN ACCORDANCE WITH ARTICLE 107.14 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION". ADEQUATE LIGHTING SHALL BE MAINTAINED FROM DUSK TO DAWN AT ALL LOCATIONS WHERE CONSTRUCTION OPERATIONS WARRANT, OR AS DESIGNATED BY THE ENGINEER.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVISES.

PROTECTION OF SIGNS AND PROPERTY

ALL TRAFFIC SIGNS, STREET SIGNS, ETC., THAT INTERFERE WITH THE CONSTRUCTION OPERATIONS SHALL BE REMOVED AND PLACED AT NEW LOCATIONS AS DESIGNATED BY THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE COMBINATION CURB AND GUTTER REMOVAL, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. IN ADDITION, ALL MAIL BOXES THAT INTERFERE WITH CONSTRUCTION SHALL BE SIMILARLY RELOCATED AT NO ADDITIONAL COST IN ACCORDANCE WITH ARTICLES 107.20 AND 107.21 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION".

SUPERINTENDENCE

SPECIAL ATTENTION IS DRAWN TO ARTICLE 105.06 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" WHICH REQUIRES THE CONTRACTOR TO HAVE A COMPETENT SUPERINTENDENT ON THE PROJECT SITE AT ALL TIMES, IRRESPECTIVE OF THE AMOUNT OF WORK SUBLET. THE SUPERINTENDENT SHALL BE CAPABLE OF READING AND UNDERSTANDING THE PLANS AND SPECIFICATIONS, SHALL HAVE FULL AUTHORITY TO EXECUTE ORDERS TO EXPEDITE THE PROJECT AND SHALL BE RESPONSIBLE FOR SCHEDULING AND HAVING CONTROL OF ALL THE WORK AS THE AGENT OF THE GENERAL CONTRACTOR. FAILURE TO COMPLY WITH THIS PROVISION WILL RESULT IN A SUSPENSION OF WORK AS PROVIDED IN ARTICLE 108.07.

SAWING EXISTING IMPROVEMENTS

ALL PERMANENT TYPE PAVEMENTS OR OTHER PERMANENT IMPROVEMENTS WHICH ABUT THE PROPOSED IMPROVEMENT AND MUST BE REMOVED, SHALL BE SAWED AS DIRECTED PRIOR TO REMOVAL. ALL ITEMS SO REMOVED SHALL BE REPLACED WITH SIMILAR CONSTRUCTION MATERIALS TO THEIR ORIGINAL CONDITION OR BETTER. PAYMENT FOR SAWING SHALL BE INCLUDED IN THE COST FOR THE REMOVAL OF EACH ITEM, AND REPLACEMENT WILL BE PAID FOR UNDER THE RESPECTIVE ITEMS IN THE CONTRACT UNLESS OTHERWISE INDICATED. SAWCUTTING FOR PATCHES WILL BE INCLUDED IN THE COST OF THE PATCHING ITEM.

PROJECT SAFETY

BARRICADES: THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SAND BAGS ON EACH TYPE I OR TYPE II BARRICADE USED ONE (1) WEIGHTED SAND BAG ACROSS EACH BOTTOM RAIL.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1-1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

THE CONTRACTOR SHALL COMPLY WITH AND OBSERVE THE RULES AND REGULATIONS OF O.S.H.A. AND APPROPRIATE AUTHORITIES REGARDING SAFETY PROVISIONS. THE CONTRACTOR, ENGINEER, AND OWNER SHALL EACH BE RESPONSIBLE FOR THEIR OWN RESPECTIVE AGENTS AND EMPLOYEES.

THE ENGINEER AND OWNER ARE NOT RESPONSIBLE FOR THE CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OR PROCEDURES, TIME OF PERFORMANCE, PROGRAMS, OR FOR ANY SAFETY PRECAUTIONS USED BY THE CONTRACTOR. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR EXECUTION OF HIS WORK IN ACCORDANCE WITH THE DOCUMENTS AND SPECIFICATIONS.

Civil Engineers
Frank Novotny & Associates, Inc.
825 Midway Drive ◆ Willowbrook, IL ◆ 60527 ◆ Telephone: (630) 887-8640 ◆ Fax: (630) 887-0132
Illinois Professional Design Firm No. 184-000928

PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS

NO.	BY	DATE	DESCRIPTION
1	AMS	1/22/10	PER IDOT REVIEW
2	AMS	2/8/10	PER IDOT REVIEW
3	AMS	3/03/10	PER IDOT REVIEW

STATE STANDARDS
GENERAL NOTES
MWRDGC NOTES
SPECIAL PROJECT NOTES

PROJECT NO. 05043	SCALE NONE	SHEET 2 OF 51 SHEETS	
DRAWN/DESIGNED JFP-JEP/THK	DATE DEC., 2009		
CHECKED/APPROVED JLC/THK	FIELD BOOK NO. FILE		

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	3
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63445

SUMMARY OF QUANTITIES

Specialty Item	Special Provision	Item No	Description	Unit	Construction Code I000 Quantity
	SP	20200410	EARTH EXCAVATION (SPECIAL)	CU YD	500
		20800150	TRENCH BACKFILL	CU YD	1,200
	SP	21101605	TOPSOIL FURNISH AND PLACE, 2"	SQ YD	4,460
		25000400	NITROGEN FERTILIZER NUTRIENT	POUND	115
		25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	115
		25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	115
		25200100	SODDING	SQ YD	5,937
		25200200	SUPPLEMENTAL WATERING	UNIT	60
		28000500	INLET AND PIPE PROTECTION	EACH	25
		28000510	INLET FILTERS	EACH	25
		35300300	PORTLAND CEMENT CONCRETE BASE COURSE 8"	SQ YD	1,750
	SP	40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	36
		40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	8
		40600300	AGGREGATE (PRIME COAT)	TON	37
		40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	2,067
		40600895	CONSTRUCTING TEST STRIP	EACH	1
		40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	448
		40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	4,134
		42000300	PORTLAND CEMENT CONCRETE PAVEMENT 8"	SQ YD	101
	SP	42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	846
	SP	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	28,771
	SP	42400800	DETECTABLE WARNINGS	SQ FT	884
		44000100	PAVEMENT REMOVAL	SQ YD	3,249
		44000169	HOT-MIX ASPHALT SURFACE REMOVAL, 5"	SQ YD	20,320
	SP	44000195	HOT-MIX ASPHALT DRIVEWAY PAVEMENT REMOVAL	SQ YD	30
	SP	44000198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	16,430
		44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	1,145
	SP	44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	15,740
	SP	44000600	SIDEWALK REMOVAL	SQ FT	28,200
	SP	44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	500
	SP	44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	800
	SP	44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	4,550
	SP	44300300	AREA REFLECTIVE CRACK CONTROL TREATMENT, SYSTEM A	SQ YD	36,462
	SP	55100300	STORM SEWER REMOVAL 8"	FOOT	2,535
*	SP	56500600	DOMESTIC WATER SERVICE BOXES TO BE ADJUSTED	EACH	61
		60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	9
		60200205	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	19
		60206905	CATCH BASINS, TYPE C, TYPE 1 FRAME, OPEN LID	EACH	29
		60234200	INLETS, TYPE A, TYPE 1 FRAME, OPEN LID	EACH	48
		60250200	CATCH BASINS TO BE ADJUSTED	EACH	26
		60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	6
		60255500	MANHOLES TO BE ADJUSTED	EACH	14
		60257900	MANHOLES TO BE RECONSTRUCTED	EACH	18
		60260100	INLETS TO BE ADJUSTED	EACH	12
		60265700	VALVE VAULTS TO BE ADJUSTED	EACH	5
		60266100	VALVE VAULTS TO BE RECONSTRUCTED	EACH	13
		60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	4
		60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	15
		60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	35
		60500050	REMOVING CATCH BASINS	EACH	74
		60500060	REMOVING INLETS	EACH	25
		60500105	FILLING MANHOLES	EACH	1
		60500205	FILLING CATCH BASINS	EACH	6
		67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	5
		67100100	MOBILIZATION	L SUM	1
		70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1
		70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1
		70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
		70300100	SHORT-TERM PAVEMENT MARKING	FOOT	16,008
		70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1,789
		72000100	SIGN PANEL - TYPE 1	SQ FT	54
*		78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	328
*		78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	23,598
*		78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3,059
*		78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	324
*		78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	493
*		81000600	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	160
*		81000700	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL	FOOT	42
*		81000800	CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL	FOOT	49
*		81001000	CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL	FOOT	40
*		81018500	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	281
*		81018900	CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL	FOOT	338
*		81400100	HANDHOLE	EACH	9
*	SP	81400115	HANDHOLE TO BE ADJUSTED	EACH	23
*		81400200	HEAVY-DUTY HANDHOLE	EACH	1
*		81400300	DOUBLE HANDHOLE	EACH	2
*		81900200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	275
*		85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2
*	SP	85700205	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	3
*	SP	85700305	FULL-ACTUATED CONTROLLER AND TYPE V CABINET, SPECIAL	EACH	1
*	SP	86000105	MASTER CONTROLLER (SPECIAL)	EACH	1
*		87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	2,333
*		87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	2,792
*		87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	2,878
*		87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	575
*		87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	531
*		87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	320
*		87502480	TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.	EACH	3
*		87700170	STEEL MAST ARM ASSEMBLY AND POLE, 26 FT.	EACH	1
*		87700180	STEEL MAST ARM ASSEMBLY AND POLE, 28 FT.	EACH	5
*		87700190	STEEL MAST ARM ASSEMBLY AND POLE, 30 FT.	EACH	1
*		87700210	STEEL MAST ARM ASSEMBLY AND POLE, 34 FT.	EACH	1
*		87800100	CONCRETE FOUNDATION, TYPE A	FOOT	16
*		87800150	CONCRETE FOUNDATION, TYPE C	FOOT	8
*	SP	87800400	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	120
*	SP	87900200	DRILL EXISTING HANDHOLE	EACH	3
*		88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	23
*		88030050	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	9
*		88030100	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	6
*		88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	7
*		88030240	SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED	EACH	1
*		88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	8
*		88102747	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	10
*		88200210	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	30
*		88500100	INDUCTIVE LOOP DETECTOR	EACH	20
*		88600100	DETECTOR LOOP, TYPE I	FOOT	1,533
*	SP	88600600	DETECTOR LOOP REPLACEMENT	FOOT	638
*		88700200	LIGHT DETECTOR	EACH	6
*		88700300	LIGHT DETECTOR AMPLIFIER	EACH	4
*		88800100	PEDESTRIAN PUSH-BUTTON	EACH	28
*		89000100	TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	2
*	SP	89501150	RELOCATE EXISTING TRAFFIC SIGNAL POST	EACH	1
*	SP	89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	707
*		89502350	REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	248
*		89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	4
*		89502380	REMOVE EXISTING HANDHOLE	EACH	10
*		89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	19
*	SP	X0320872	VIDEO VEHICLE DETECTION SYSTEM	EACH	1
	SP	X0321020	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (COLD MILLING) VARIABLE DEPTH	SQ YD	3,066
	SP	X0322124	STORM SEWER (WATER MAIN REQUIREMENTS) 8 INCH	FOOT	2,699
*	SP	X0322468	GAS VALVE TO BE ADJUSTED	EACH	20
	SP	X0322916	PROPOSED STORM SEWER CONNECTION TO EXISTING STORM SEWER	EACH	40
*	SP	X0323712	RADIO INTERCONNECT SYSTEM COMPLETE, MASTER	EACH	1
*	SP	X0323713	RADIO INTERCONNECT SYSTEM COMPLETE, LOCAL	EACH	2
*	SP	X0324007	OPTIMIZE TRAFFIC SIGNAL SYSTEM	EACH	1
	SP	X0324637	BASE COVER, LIGHT POLE	EACH	4
*	SP	X0325035	PAINT NEW COMBINATION MAST ARM AND POLE, UNDER 40 FT	EACH	8
*	SP	X0325037	PAINT NEW SIGNAL POST	EACH	2
*	SP	X0325737	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	2
	SP	X0712400	TEMPORARY PAVEMENT	SQ YD	1,400
*	SP	X8050015	SERVICE INSTALLATION - POLE MOUNTED	EACH	4
*	SP	X8140074	GROUNDING EXISTING HANDHOLE FRAME AND COVER	EACH	13
*	SP	X8620020	UNINTERRUPTIBLE POWER SUPPLY	EACH	4
*	SP	X8730027	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C	FOOT	1,804
*	SP	X8730250	ELECTRIC CABLE IN CONDUIT NO. 20 3/C,TWISTED, SHIELDED	FOOT	667
*	SP	X8870200	FIRE PREEMPTOR SWITCH	EACH	1
	SP	Z0004514	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 4"	SQ YD	30
	SP	Z0013798	CONSTRUCTION LAYOUT	L SUM	1
△	SP	Z0076600	TRAINEES	HOUR	1,500
	SP	XX003313	REMOVE AND REINSTALL BRICK PAVER	SQ FT	367
	SP	XX003424	CONNECTION TO EXISTING STRUCTURE	EACH	56
*	SP	XX006464	DOMESTIC WATER SERVICE BOX TO BE ADJUSTED (SPECIAL)	EACH	30
*	SP	XX006465	DOMESTIC WATER METER VAULT TO BE ADJUSTED (SPECIAL)	EACH	71
	SP	XX104800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-V.12	FOOT	15,740

△ Y080

Civil Engineers

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Illinois Professional Design Firm No. 184-000928

PROJECT

TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS

NO.	BY	DATE	DESCRIPTION
1	AMS	1/22/10	PER IDOT REVIEW
2	AMS	2/8/10	PER IDOT REVIEW
3	AMS	3/03/10	PER IDOT REVIEW

SUMMARY OF QUANTITIES

PROJECT NO. 05043	SCALE NONE
DRAWN/DESIGNED JFP-JEP/THK	DATE DEC., 2009
CHECKED/APPROVED JLC/THK	FIELD BOOK NO. FILE

SHEET 3 OF 51 SHEETS

NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

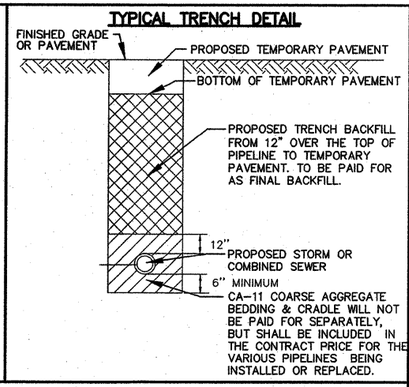
MIXTURE TYPE	PERCENT AIR VOIDS
ROADWAY	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2" (IL-9.5mm)	4% @ 70 GYR
LEVELING BINDER (MACHINE METHOD), N70, 1" (IL-9.5mm)	4% @ 70 GYR
DRIVEWAYS	
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 1-3/4" HOT-MIX ASPHALT BASE COURSE, HMA BINDER (IL-19.0mm, N50), 2-1/4"	4% @ 50 GYR
PATCHING	
CLASS D PATCHES, TYPE II-IV, 9", (HMA BINDER IL-19.0mm)	4% @ 70 GYR
TEMPORARY PAVEMENT	
HOT-MIX ASPHALT BINDER COURSE, IL-19.0mm, N50, 2-1/4"	4% @ 50 GYR

THE UNIT WEIGHT TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

"THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS"

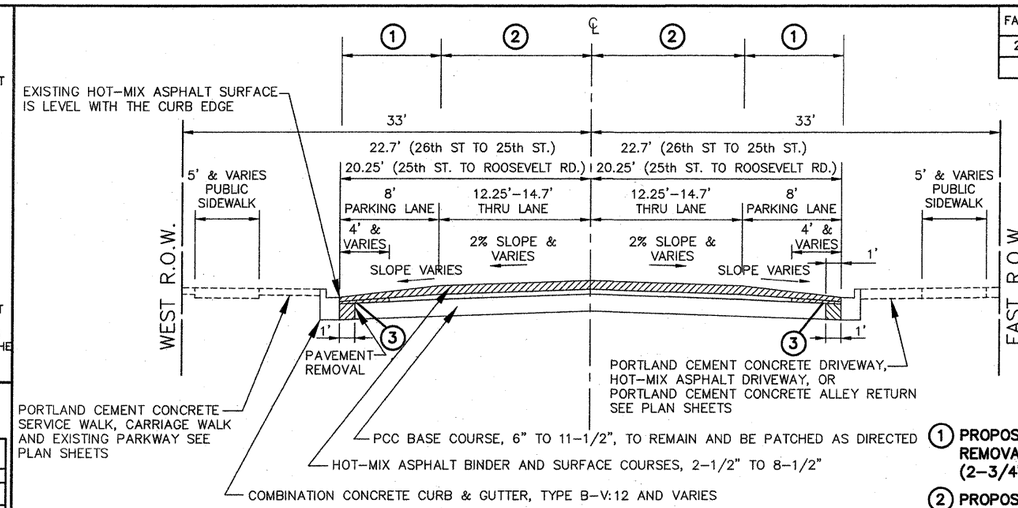
"FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS"

TYPICAL TRENCH DETAIL

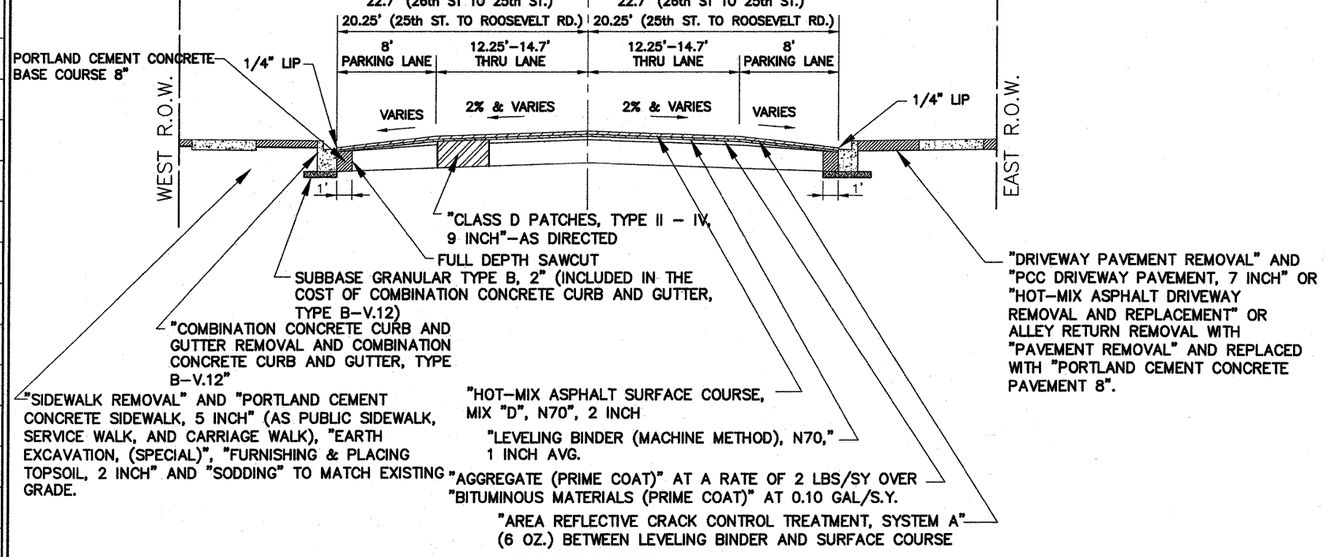


PAVEMENT CORES

WEST						EAST					
		<-- WEST CURBLINE CL -->						<-- CL EAST CURBLINE -->			
CORE #	STA	FROM EOP.	FROM EOP.	FROM EOP.		CORE #	STA	FROM EOP.	FROM EOP.	FROM EOP.	
1	2+25	3'			2.5" TOTAL ASPHALT THICKNESS 8.5" P.C. CONCRETE NO SUBBASE	4	9+02			2'	2.75" TOTAL ASPHALT THICKNESS 6" P.C. CONCRETE (FRACTURED) NO SUBBASE
2	2+31		12.5'		6.75" TOTAL ASPHALT THICKNESS 8.75" P.C. CONCRETE NO SUBBASE		9+12		11'		6" TOTAL ASPHALT THICKNESS 8.5" P.C. CONCRETE (TOP 1.5" FRACTURED) NO SUBBASE
3	2+35			22.5'	10.5" TOTAL ASPHALT THICKNESS (FRACTURED/DETERIORATED) 8" GRAVEL SUBBASE (ROUNDED STONE)	6	9+21	19'			6.5" TOTAL ASPHALT THICKNESS 11" P.C. CONCRETE (TOP 1" FRACTURED) NO SUBBASE
7	15+67			18'	5.25" TOTAL ASPHALT THICKNESS 9.5" P.C. CONCRETE (BOTTOM 2" FRACTURED) NO SUBBASE	10	22+39			2.5'	3" TOTAL ASPHALT THICKNESS 7" P.C. CONCRETE (FRACTURED) NO SUBBASE
8	15+68		8.5'		6" TOTAL ASPHALT THICKNESS 9" P.C. CONCRETE (DETERIORATED) NO SUBBASE	11	22+45		6'		3.75" TOTAL ASPHALT THICKNESS 8" P.C. CONCRETE (TOP 2" FRACTURED) NO SUBBASE
9	15+67	3'			3" TOTAL ASPHALT THICKNESS 8" P.C. CONCRETE (DETERIORATED) NO SUBBASE	12	22+50	18.5'			4.75" TOTAL ASPHALT THICKNESS 9.5" P.C. CONCRETE (TOP 2" AND BOTTOM 2" FRACTURED) NO SUBBASE
13	29+97	2.5'			2.25" TOTAL ASPHALT THICKNESS 7" P.C. CONCRETE (FRACTURED) NO SUBBASE	16	33+71		17.5		6.5" TOTAL ASPHALT THICKNESS 8.25" P.C. CONCRETE NO SUBBASE
14	30+02		10'		6.75" TOTAL ASPHALT THICKNESS 8" P.C. CONCRETE (TOP 2" FRACTURED) NO SUBBASE	16A	33+71	18.5			6.5" TOTAL ASPHALT THICKNESS P.C. CONCRETE
15	30+07			18.5'	8.5" TOTAL ASPHALT THICKNESS 10" P.C. CONCRETE (TOP 1" AND BOTTOM 2" FRACTURED) NO SUBBASE	16B	33+71			16.5	6" TOTAL ASPHALT THICKNESS P.C. CONCRETE
20	49+87			18.5'	6.25" TOTAL ASPHALT THICKNESS 9" P.C. CONCRETE (BOTTOM 3" FRACTURED) NO SUBBASE	17	41+02			3'	2.5" TOTAL ASPHALT THICKNESS 11.5" P.C. CONCRETE (DETERIORATED) NO SUBBASE
21	49+95		9'		5.5" TOTAL ASPHALT THICKNESS (FRACTURED BY CORING) 8.5" P.C. CONCRETE (FRACTURED) NO SUBBASE	18	41+05		8'		4.5" TOTAL ASPHALT THICKNESS 9" P.C. CONCRETE (FRACTURED) NO SUBBASE
22	50+00	1.5'			3.25" TOTAL ASPHALT THICKNESS 6.5" P.C. CONCRETE (DETERIORATED) NO SUBBASE	19	41+12	17'			6.25" TOTAL ASPHALT THICKNESS 8" P.C. CONCRETE (FRACTURED) NO SUBBASE
26	63+16			17'	4.5" TOTAL ASPHALT THICKNESS 8.5" P.C. CONCRETE (DETERIORATED) NO SUBBASE	23	56+49			2.5'	2.5" TOTAL ASPHALT THICKNESS 8.5" P.C. CONCRETE (DETERIORATED) NO SUBBASE
27	63+20		9.5'		4.5" TOTAL ASPHALT THICKNESS 9" P.C. CONCRETE (TOP 1.5" AND BOTTOM 1.5" FRACTURED) NO SUBBASE	24	56+65		7.5'		4.5" TOTAL ASPHALT THICKNESS 11.5" P.C. CONCRETE (DETERIORATED) NO SUBBASE
28	63+28	2'			1.25" TOTAL ASPHALT THICKNESS 8.5" P.C. CONCRETE (DETERIORATED) NO SUBBASE	25	56+68	19'			5" TOTAL ASPHALT THICKNESS 9.5" P.C. CONCRETE (FRACTURED) NO SUBBASE
						29	69+28			3'	2.5" TOTAL ASPHALT THICKNESS 9.5" P.C. CONCRETE (FRACTURED; PARTIALLY DETERIORATED) NO SUBBASE
						30	69+32		8'		5" TOTAL ASPHALT THICKNESS 10" P.C. CONCRETE (FRACTURED) NO SUBBASE
						31	69+42	18.5'			3.75" TOTAL ASPHALT THICKNESS 9.5" P.C. CONCRETE (DETERIORATED) NO SUBBASE
						32	76+78			2'	12.75" TOTAL ASPHALT THICKNESS NO SUBBASE
						33	76+78		8'		4.75" TOTAL ASPHALT THICKNESS 8" P.C. CONCRETE (FRACTURED) NO SUBBASE
						34	76+78	18.5'			4" TOTAL ASPHALT THICKNESS 9.5" P.C. CONCRETE (FRACTURED) NO SUBBASE



EXISTING
CENTRAL AVENUE
STA. 0+68 TO STA. 79+63



PROPOSED
CENTRAL AVENUE
STA. 0+68 TO STA. 79+63

1. PROPOSED "HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH" (2-3/4" AT GUTTER FLAG-5" AT 8' FROM EOP)
2. PROPOSED "HOT-MIX ASPHALT SURFACE REMOVAL, 5 INCH"
3. PROPOSED "PCC SURFACE REMOVAL (COLD MILLING) VARIABLE DEPTH SEE PAVEMENT CORE DATA THIS SHEET

TYPICAL CROSS SECTIONS

SCALE: 1"=6'

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	4
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63455

IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

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Illinois Professional Design Firm No. 184-000928

PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS			
NO.	BY	DATE	DESCRIPTION
1	AMS	1/22/10	PER IDOT REVIEW
2	AMS	3/8/10	PER IDOT REVIEW

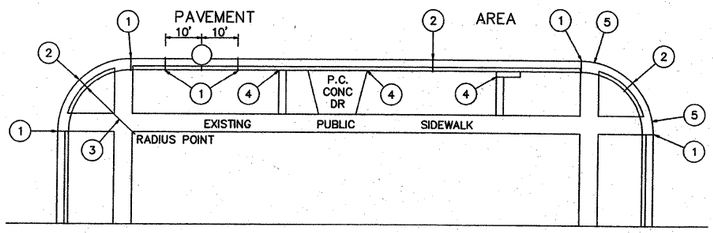
TYPICAL CROSS SECTIONS
HOT-MIX ASPHALT MIXTURE REQUIREMENTS
PAVEMENT CORES

PROJECT NO. 05043	SCALE 1"=6'
DRAWN/DESIGNED JFP-JEP/THK	DATE DEC., 2009
CHECKED/APPROVED JLC/THK	FIELD BOOK NO. FILE

SHEET 4 OF 51 SHEETS

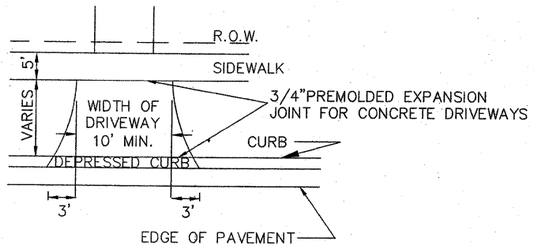
FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	5
F.H.W.A. REG.	ILLINOIS	PROJECT M-9003(488)		

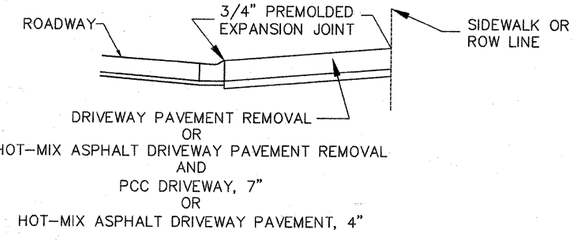
CONTRACT NO. 63445



1. EXPANSION JOINTS AT TANGENT POINTS AND 150 FOOT INTERVALS, CONSISTING OF ONE INCH PREMOLDED JOINT FILLER MATERIAL WITH #8 DOWEL BARS, 18" IN LENGTH, GREASED, PROVIDE EXPANSION CAP ON ONE END. ALSO CONSTRUCT THIS JOINT TEN FEET EACH SIDE OF PROPOSED UNDERGROUND STRUCTURE.
2. CONTRACTION JOINTS AT TWENTY-FIVE FOOT INTERVALS AND AT THE CENTER OF RETURNS.
3. ALL RADII SHALL BE 25 FEET TO THE BACK OF CURB UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
4. LONGITUDINAL EXPANSION JOINT CONSISTING OF ONE INCH PREMOLDED JOINT FILLER.
5. DEPRESS CURB AT LOCATIONS WHERE PUBLIC WALKS INTERSECT CURB LINE AT STREET INTERSECTIONS, ALLEYS, AND OTHER LOCATIONS AS DIRECTED, FOR THE CONSTRUCTION OF RAMPED SIDEWALKS FOR ACCESS BY THE HANDICAPPED.

JOINT DETAILS

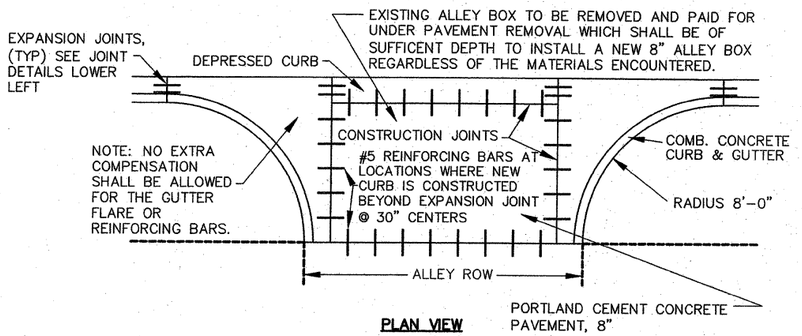


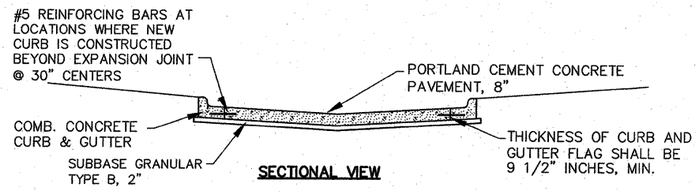


THICKNESS OF CURB AND GUTTER FLAG SHALL BE 9 1/2 INCHES, MIN.

TYPICAL DRIVEWAY DETAIL

SAWCUTTING AND THE 3/4" PREMOLDED EXPANSION JOINT WILL NOT BE PAID SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO PCC DRIVEWAY PAVEMENT, 7"

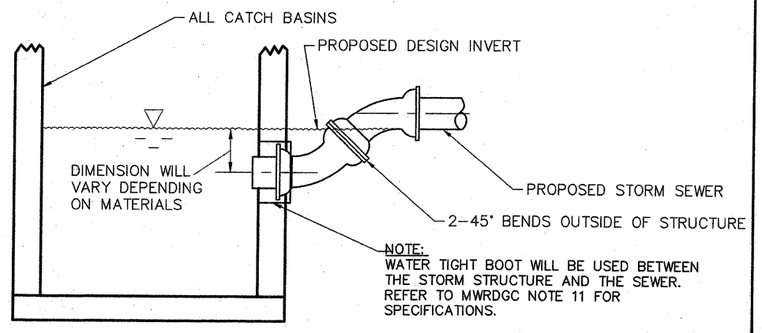




TYPICAL ALLEY RETURN DETAIL

EARTH EXCAVATION (SPECIAL) SCHEDULE

STATION	TO	STATION	EAST PARKWAY	WEST PARKWAY
7+25		8+25	3 CY	
7+80		10+70		8 CY
10+30		13+00	7 CY	
11+75		13+00		3 CY
13+90		14+25		2 CY
14+00		16+05	6 CY	
14+45		15+75		4 CY
15+90		16+30		2 CY
16+98		17+25	1 CY	
16+98		19+65		3 CY
17+55		19+35	5 CY	
20+30		22+10		5 CY
20+30		22+95	8 CY	
29+64		32+95		9 CY
29+81		38+00	22 CY	
33+60		39+60		16 CY
38+60		46+25	20 CY	
40+25		46+25		16 CY
46+80		51+90		14 CY
47+10		51+90	13 CY	
56+40		63+80	20 CY	
56+85		59+65		8 CY
60+30		66+30		16 CY
64+10		66+38	6 CY	
66+87		76+43	26 CY	
66+95		72+85		16 CY
73+60		78+17		12 CY
			137 CY	134 CY
			TOTAL 271 CY	



NOTE:
ALL PROPOSED OUTFLOW INVERTS SHOWN IN THE STRUCTURE SCHEDULE ARE TO THE TRAPPED ELEVATION OF THE WATER INSIDE AND OUTSIDE OF THE CATCH BASIN.

NOTE:
THE ACTUAL POSITION OF THE OUTFLOW PIPE IN ALL CATCH BASINS WILL BE LOWER THAN THE DESIGN INVERT ELEVATION, DEPENDING ON THE CONSTRUCTION OF THE TRAP. ALL CONNECTIONS AND PIPING CONSTRUCTION SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE MATERIALS ARE ORDERED.

CATCH BASIN WITH HALF TRAP

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Illinois Professional Design Firm No. 184-000928

PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS			
NO.	BY	DATE	DESCRIPTION
1	AMS	1/22/10	PER IDOT REVIEW
2	AMS	2/8/10	PER IDOT REVIEW

SPECIAL PROJECT DETAILS

PROJECT NO. 05043	SCALE NONE	SHEET 5 OF 51 SHEETS	
DRAWN/DESIGNED JFP-JEP/THK	DATE DEC., 2009		
CHECKED/APPROVED JLC/THK	FIELD BOOK NO. FILE		

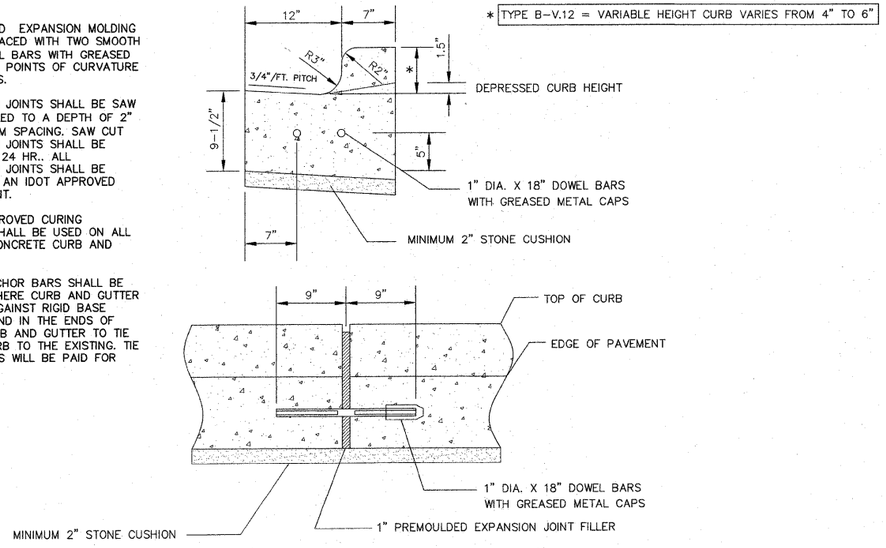
NOTES:

1" PREFORMED EXPANSION MOLDING SHALL BE PLACED WITH TWO SMOOTH 1" DIA. DOWEL BARS WITH GREASED CAPS AT ALL POINTS OF CURVATURE AND CORNERS.

CONTRACTION JOINTS SHALL BE SAW CUT OR TOOLED TO A DEPTH OF 2" @ 15' MINIMUM SPACING. SAW CUT CONTRACTION JOINTS SHALL BE DONE WITHIN 24 HR.. ALL CONTRACTION JOINTS SHALL BE SEALED WITH AN IDOT APPROVED JOINT SEALANT.

AN IDOT APPROVED CURING COMPOUND SHALL BE USED ON ALL PROPOSED CONCRETE CURB AND GUTTER.

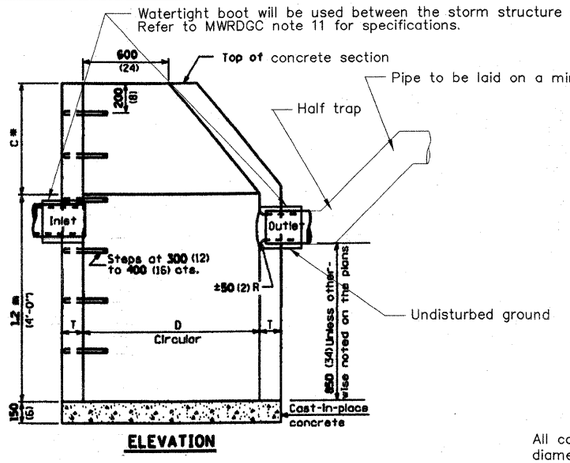
3/4" TIE ANCHOR BARS SHALL BE INSTALLED WHERE CURB AND GUTTER IS PLACED AGAINST RIGID BASE MATERIALS AND IN THE ENDS OF EXISITNG CURB AND GUTTER TO TIE THE NEW CURB TO THE EXISTING. TIE ANCHOR BARS WILL BE PAID FOR SEPARATELY.



* TYPE B-V.12 = VARIABLE HEIGHT CURB VARIES FROM 4" TO 6"

EXPANSION JOINT DETAIL

COMBINATION CONCRETE CURB AND GUTTER, TYPE B-V.12

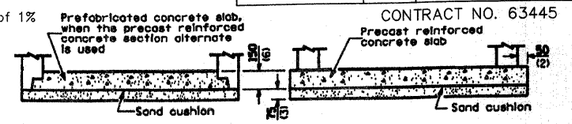


NOTE: CATCH BASIN, TYPE C, SPECIAL SHALL BE 2'-0" IN DIAMETER WITH A 15" SUMP

CATCH BASIN, TYPE A

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	6
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63445



ALTERNATE BOTTOM SLAB

~~ALTERNATE~~ MATERIALS FOR WALLS	D	C	T (min.)
Precast Reinforced Concrete Section	1.2 m (4'-0") 1.5 m (5'-0")	750 (30) 1.15 m (3'-9")	100 (4) 125 (5)
~~Cast-in-place Concrete~~	~~1.2 m (4'-0") 1.5 m (5'-0")~~	~~750 (30) 1.15 m (3'-9")~~	~~125 (5) 150 (6)~~

GENERAL NOTES

All catch basins shall be 1.2m (4'-0") in diameter unless otherwise noted on the plans.

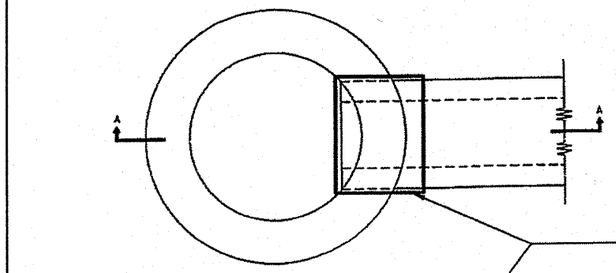
*Dimension C for precast reinforced concrete section may vary form the dimension given to plus 150mm (6").

See Standard IDOT 602601 for optional precast reinforced concrete flat slab top.

See Standard IDOT 602701 for details of steps.

All dimensions are in millimeters (inches) unless otherwise shown.

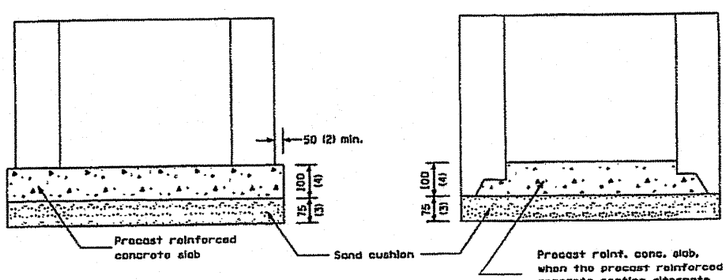
CATCH BASIN, TYPE C



TOP OF PRECAST STRUCTURE

ALTERNATE MATERIALS FOR WALLS	T
~~BRICK MASONRY~~	~~200 (8)~~
~~CAST-IN-PLACE CONCRETE~~	~~150 (6)~~
~~CONCRETE MASONRY UNIT~~	~~125 (5)~~
PRECAST REINFORCED CONCRETE SECTION	75 (3)

A watertight boot will be used between the storm structure and the sewer. Refer to MWRDGC note 11 for specifications.

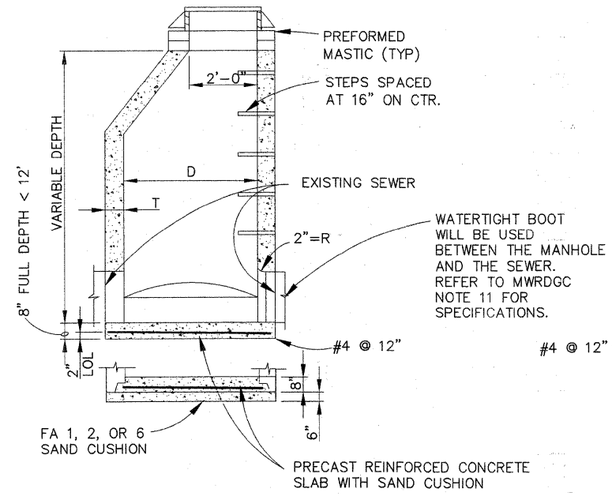


All dimensions are in millimeters (inches) unless otherwise shown.

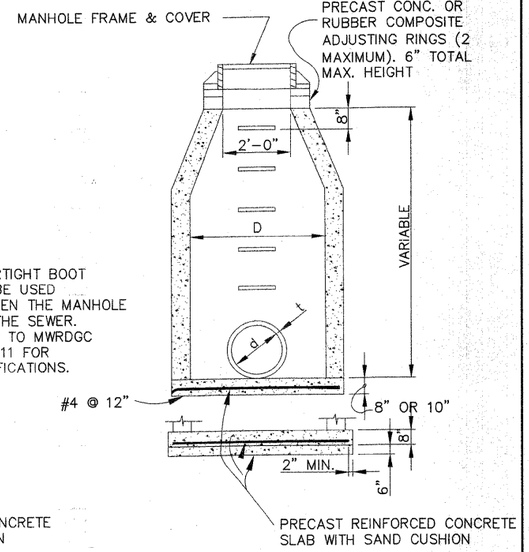
INLET, TYPE A

NOTES:

PROVIDE CA-6 AGGREGATE BACKFILL AROUND MANHOLE TO SUBGRADE ELEVATION IN PAVED AREAS. LIFT HOLES ON MANHOLES TO BE SEALED WITH CONCRETE PLUG AND MASTIC



SECTION-ECCENTRIC



SECTION-CONCENTRIC

FOR DIAMETER, D= 4 ft. THICKNESS, T= 5 in.
5 ft. 6 in.
6 ft. 6 in.

MANHOLE, TYPE A

Civil Engineers

Frank Novotny & Associates, Inc.

825 Midway Drive ◆ Willowbrook, IL ◆ 60527 ◆ Telephone: (630) 887-8640 ◆ Fax: (630) 887-0132

Illinois Professional Design Firm No. 184-000928

PROJECT

TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS

NO.	BY	DATE	DESCRIPTION
1	AMS	1/22/10	PER IDOT REVIEW

SPECIAL PROJECT DETAILS

PROJECT NO.	SCALE
05043	NONE
DRAWN/DESIGNED	DATE
JFP-JEP/THK	DEC., 2009
CHECKED/APPROVED	FIELD BOOK NO.
JLC/THK	FILE

SHEET 6 OF 51 SHEETS

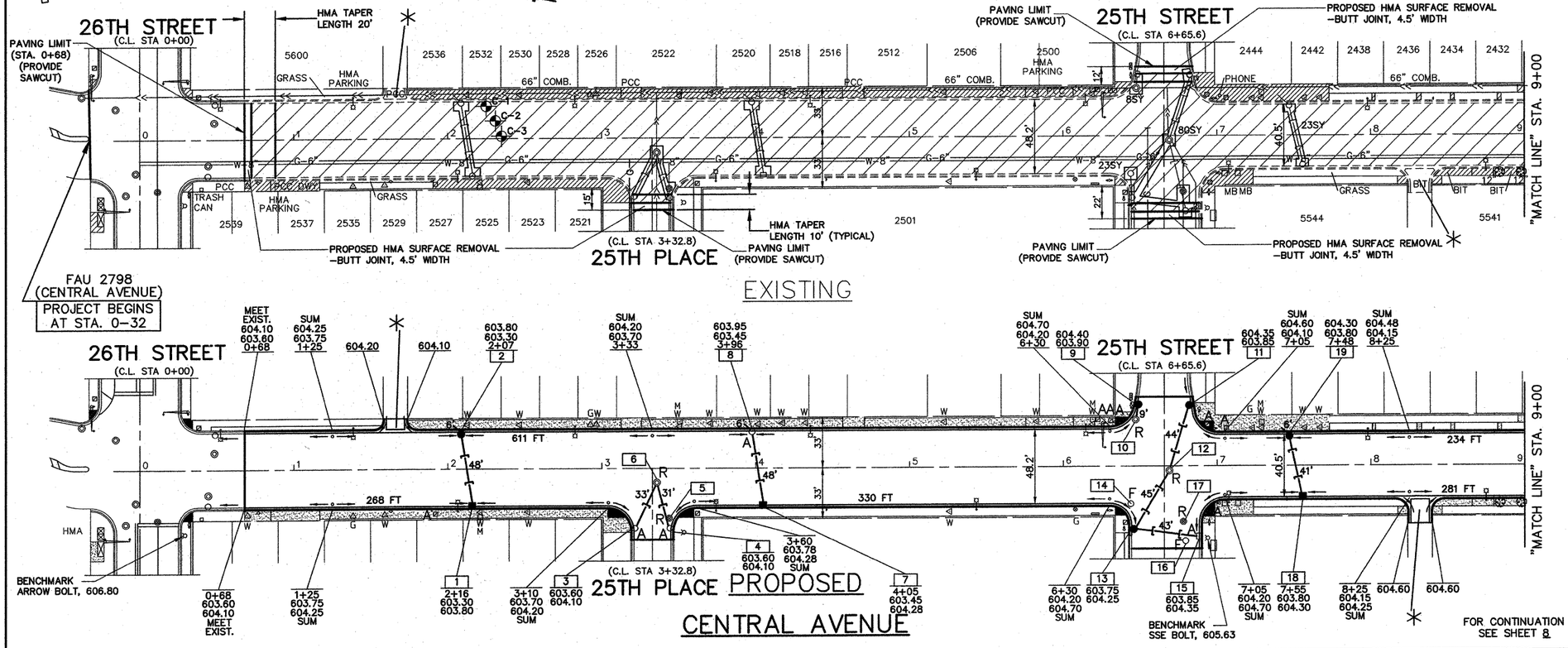
LEGEND

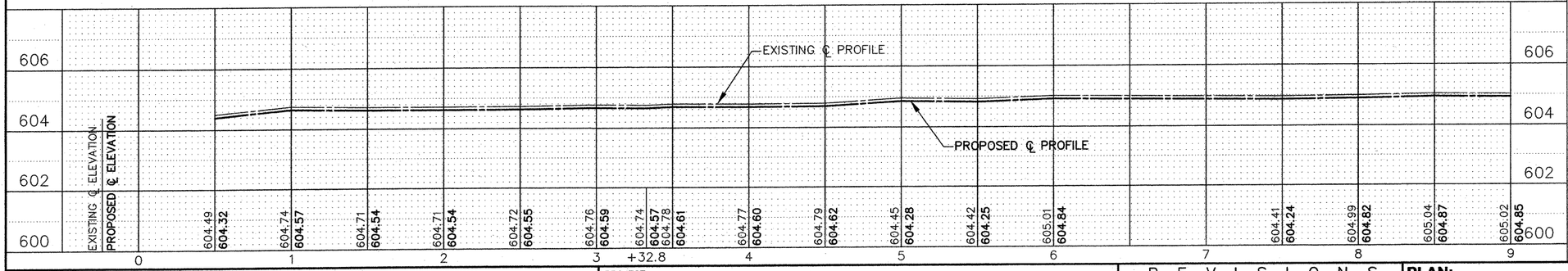
- DENOTES HOT-MIX ASPHALT SURFACE REMOVAL, (SEE TYPICAL CROSS SECTIONS), TO BE REPLACED WITH "LEVELING BINDER (MACHINE METHOD), N70," 1 INCH AVG., "AREA REFLECTIVE CRACK CONTROL TREATMENT, SYSTEM A" AND "HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70", 2 INCH"
- DENOTES "DRIVEWAY REMOVAL" AND "SIDEWALK REMOVAL" (AS DIRECTED BY THE ENGINEER)
- DENOTES P.C. CONCRETE SIDEWALK, 5 INCH, WITH DETECTABLE WARNING
- DENOTES "P.C. CONCRETE DRIVEWAY PAVEMENT, 7 INCH" AND "P.C. CONCRETE SIDEWALK, 5 INCH" ON SUBBASE GRANULAR MATERIAL, TYPE B, 2 INCH"
- DENOTES HOT-MIX DRIVEWAY REMOVAL AND REPLACEMENT
- DENOTES CLASS D PATCHES, TYPE II - IV, 12", - AS DIRECTED
- "A" DENOTES EXISTING VALVE VAULTS, VALVE BOXES, INLETS, CATCH BASINS AND MANHOLES TO BE CLEANED, GROUTED, AND ADJUSTED TO GRADE WITH NEW ADJUSTING RINGS. (ALL TOP BANDS OF FALLEN, COMMON OR CONCRETE BRICK TO BE REPLACED WITH PRECAST CONCRETE ADJUSTING RINGS)
- "S" DENOTES EXISTING FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) (SEE SHEET 41 FOR DETAIL)
- "R" DENOTES EXISTING STRUCTURES TO BE RECONSTRUCTED
- "F" DENOTES EXISTING STRUCTURES TO BE FILLED
- DENOTES ALLEY RETURN TO BE REMOVED WITH "PAVEMENT REMOVAL" AND REPLACED WITH "PORTLAND CEMENT CONCRETE PAVEMENT, 8 INCH".
- DENOTES "COMBINATION CURB AND GUTTER REMOVAL"
- DENOTES "COMBINATION CURB AND GUTTER, TYPE B-V.12"
- 20' STORM SEWER (WATER MAIN QUALITY) 8 INCH (LENGTH AS NOTED)

BENCHMARK

ARROW BOLT ON HYDRANT AT NORTHEAST CORNER OF 26TH STREET AND CENTRAL AVENUE
ELEV. 606.80

SSE BOLT ON HYDRANT AT NORTHEAST CORNER OF 25TH STREET AND CENTRAL AVENUE
ELEV. 605.63





FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET 4

IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	7
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63455

STRUCTURE SCHEDULE

1. PROPOSED INLET
   TYPE A, 2' DIA.,
   TYPE 1 FR. & O.L.
   RIM=603.30
   INV(W)=600.10
2. PROPOSED CATCH BASIN
   TYPE A, 4' DIA.,
   TYPE 1 FR. & O.L.
   RIM=603.30
   INV(E)=601.25
3. CATCH BASIN ADJUSTMENT
   NEW TYPE 1 FR. & O.L.
   RIM=603.60
   INV(NW)=599.85
4. CATCH BASIN ADJUSTMENT
   RIM=603.60
   INV(SW)=599.92
5. VALVE VAULT RECONSTRUCTION
   NEW TYPE 1 FR. & C.L.
   RIM=603.80
6. MANHOLE RECONSTRUCTION
   NEW TYPE 1 FR. & C.L.
   RIM=604.40
   INV(SE)=597.70
   INV(NE)=598.78
7. PROPOSED INLET
   TYPE A, 2' DIA.,
   TYPE 1 FR. & O.L.
   RIM=603.45
   INV(E)=600.73
8. CATCH BASIN ADJUSTMENT
   NEW TYPE 1 FR. & O.L.
   RIM=603.45
   INV(E)=600.23
   INV(SW)=600.13
9. PROPOSED CATCH BASIN
   TYPE C, 2' DIA.,
   TYPE 1 FR. & O.L.
   RIM=603.90
   INV(E)=601.40
10. MANHOLE RECONSTRUCTION
    NEW TYPE 1 FR. & C.L.
    RIM=604.05
    INV(NW)=601.40
11. PROPOSED CATCH BASIN
    TYPE C, 2' DIA.,
    TYPE 1 FR. & O.L.
    RIM=603.85
    INV(E)=598.89
12. MANHOLE RECONSTRUCTION
    NEW TYPE 1 FR. & C.L.
    RIM=604.20
    INV(SE)=598.86
    INV(NW)=598.78
13. PROPOSED CATCH BASIN
    TYPE A, 4' DIA.,
    TYPE 1 FR. & O.L.
    RIM=603.75
    INV(NW)=600.31
    INV(N)=600.41
14. CATCH BASIN TO BE FILLED
15. INLET ADJUSTMENT
    RIM= 603.85
    INV(S)=601.59
16. CATCH BASIN TO BE FILLED
17. VALVE VAULT RECONSTRUCTION
    NEW TYPE 1 FR. & C.L.
    RIM=604.45
18. PROPOSED INLET
    TYPE A, 2' DIA.,
    TYPE 1, FR. & O.L.
    RIM=603.80
    INV(W)=600.80
19. PROPOSED CATCH BASIN
    TYPE A, 4' DIA.,
    TYPE 1 FR. & O.L.
    RIM=603.80
    INV(E)=600.80

NOTE: ALL SCHEDULED RIM ELEVATIONS ARE PROPOSED.

Civil Engineers
Frank Novotny & Associates, Inc.
825 Midway Drive ◆ Willowbrook, IL ◆ 60527 ◆ Telephone: (630) 887-8640 ◆ Fax: (630) 887-0132
Illinois Professional Design Firm No. 184-000928

PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS

NO.	BY	DATE	DESCRIPTION
1	AMS	1/22/10	PER IDOT REVIEW
2	AMS	3/03/10	PER IDOT REVIEW

PLAN:
CENTRAL AVENUE-
26th ST. TO STA. 9+00

(RESURFACING)

PROJECT NO. 05043	SCALE H.1"=40': V.1"=2'
DRAWN/DESIGNED JFP-JEP/THK	DATE DEC., 2009
CHECKED/APPROVED JLC/THK	FIELD BOOK NO. FILE

SHEET 7 OF 51 SHEETS

LEGEND

- DENOTES HOT-MIX ASPHALT SURFACE REMOVAL, (SEE TYPICAL CROSS SECTIONS), TO BE REPLACED WITH "LEVELING BINDER (MACHINE METHOD), N70," 1 INCH AVG., "AREA REFLECTIVE CRACK CONTROL TREATMENT, SYSTEM A" AND "HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70", 2 INCH"
- DENOTES "DRIVEWAY REMOVAL" AND "SIDEWALK REMOVAL" (AS DIRECTED BY THE ENGINEER)
- DENOTES P.C. CONCRETE SIDEWALK, 5 INCH, WITH DETECTABLE WARNING
- DENOTES "P.C. CONCRETE DRIVEWAY PAVEMENT, 7 INCH" AND "P.C. CONCRETE SIDEWALK, 5 INCH" ON SUBBASE GRANULAR MATERIAL, TYPE B, 2 INCH"
- DENOTES HOT-MIX DRIVEWAY REMOVAL AND REPLACEMENT
- DENOTES CLASS D PATCHES, TYPE II - IV, 12", - AS DIRECTED

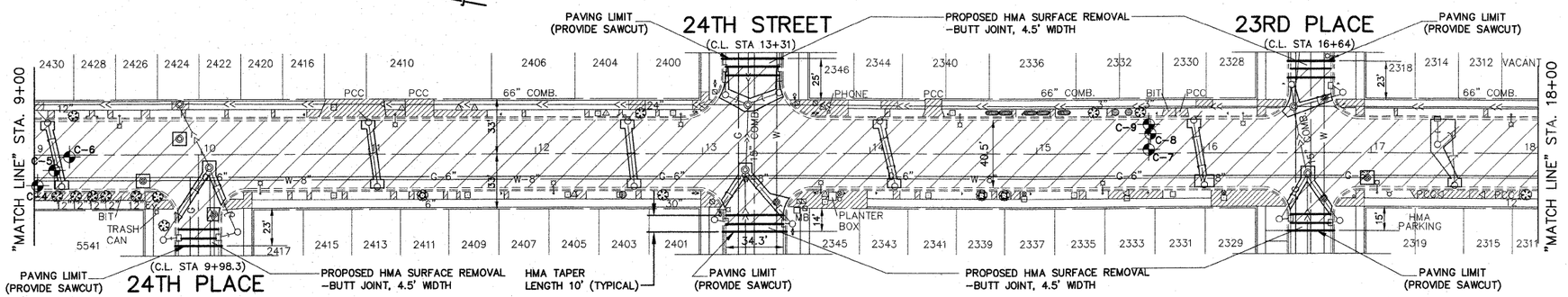
- "A" DENOTES EXISTING VALVE VAULTS, VALVE BOXES, INLETS, CATCH BASINS AND MANHOLES TO BE CLEANED, GROUTED, AND ADJUSTED TO GRADE WITH NEW ADJUSTING RINGS. (ALL TOP BANDS OF FALLEN, COMMON OR CONCRETE BRICK TO BE REPLACED WITH PRECAST CONCRETE ADJUSTING RINGS)
- "S" DENOTES EXISTING FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) (SEE SHEET 41 FOR DETAIL)
- "R" DENOTES EXISTING STRUCTURES TO BE RECONSTRUCTED
- "F" DENOTES EXISTING STRUCTURES TO BE FILLED
- DENOTES ALLEY RETURN TO BE REMOVED WITH "PAVEMENT REMOVAL" AND REPLACED WITH "PORTLAND CEMENT CONCRETE PAVEMENT, 8 INCH".
- DENOTES "COMBINATION CURB AND GUTTER REMOVAL"
- DENOTES "COMBINATION CURB AND GUTTER, TYPE B-V.12"
- 20' STORM SEWER (WATER MAIN QUALITY) 8 INCH (LENGTH AS NOTED)

BENCHMARK

NE BOLT ON HYDRANT AT NORTHEAST CORNER OF 24TH PLACE AND CENTRAL AVENUE
ELEV. 605.98

ARROW BOLT ON HYDRANT AT NORTHEAST CORNER OF 24TH STREET AND CENTRAL AVENUE
ELEV. 606.99

S BOLT ON HYDRANT AT NORTHEAST CORNER OF 23RD PLACE AND CENTRAL AVENUE
ELEV. 606.49



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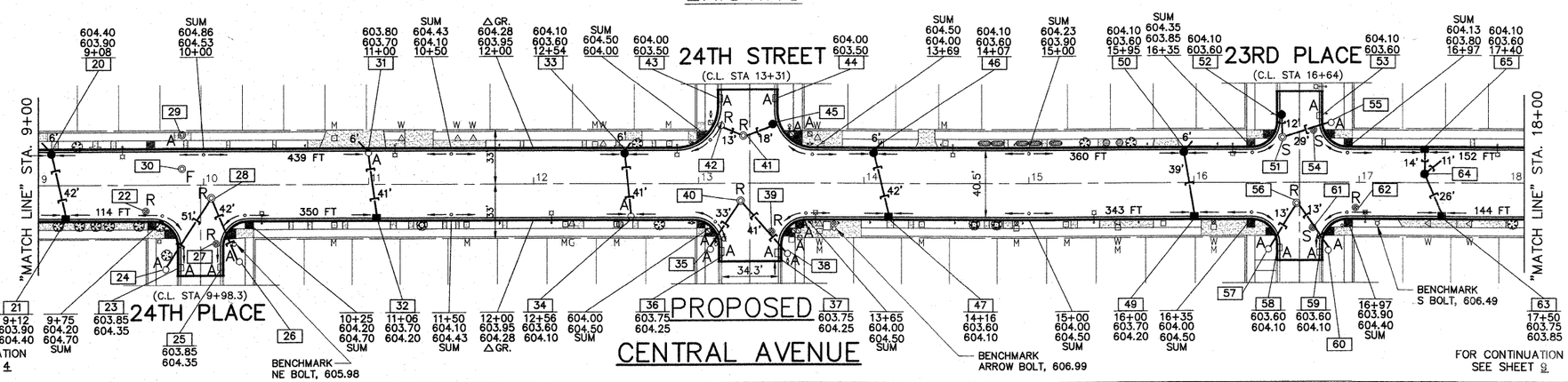

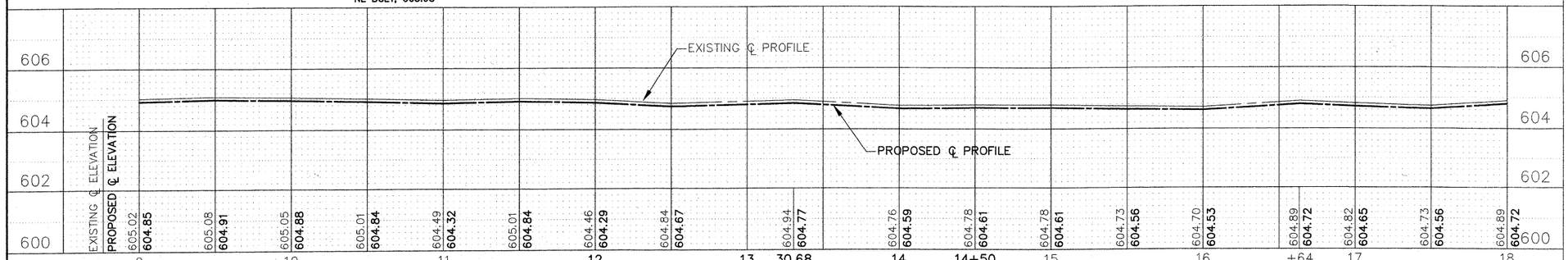

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	8
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63445

STRUCTURE SCHEDULE

20 PROPOSED CATCH BASIN
TYPE A, 4' DIA.,
TYPE 1 FR. & O.L.
RIM=603.90
INV(E)=600.50

21 PROPOSED INLET
TYPE A, 2' DIA.
TYPE 1 FR. & O.L.
RIM=603.90
INV(SW)=598.99

22 VALVE VAULT RECONSTRUCTION
RIM=604.30

23 INLET ADJUSTMENT
NEW TYPE 1 FR. & O.L.
RIM=603.85

24 CATCH BASIN ADJUSTMENT
RIM=604.40, INV(NW)=598.84

25 INLET ADJUSTMENT
NEW TYPE 1 FR. & O.L.
RIM=603.85

26 CATCH BASIN ADJUSTMENT
NEW TYPE 1 FR. & O.L.
RIM=604.55, INV(SW)=599.11

27 VALVE VAULT RECONSTRUCTION
NEW TYPE 1 FR. & C.L.
RIM=604.05

28 MANHOLE RECONSTRUCTION
RIM=604.85
INV(SE)=597.93
INV(NE)=599.31

29 MANHOLE ADJUSTMENT
NEW TYPE 1 FR. & C.L.
RIM=604.42

30 ABANDONED MANHOLE
FIELD VERIFY AND FILL

31 CATCH BASIN ADJUSTMENT
RIM=603.70, INV(E)=595.86

32 PROPOSED INLET
TYPE A, 2' DIA.,
TYPE 1 FR. & O.L.
RIM= 603.70
INV(W)=596.27

33 PROPOSED CATCH BASIN
TYPE A, 4' DIA.,
TYPE 1 FR. & O.L.
RIM=603.60
INV(E)=598.81

34 CATCH BASIN ADJUSTMENT
RIM=603.60, INV(W)=599.07

35 CATCH BASIN ADJUSTMENT
RIM=604.40, INV(NW)=599.92

36 INLET ADJUSTMENT
NET TYPE 1 FR. & O.L.
RIM=603.75

37 INLET ADJUSTMENT
RIM=603.75

38 CATCH BASIN ADJUSTMENT
RIM=604.40, INV(SW)=599.95

39 VALVE VAULT RECONSTRUCTION
NEW TYPE 1 FR. & C.L.
RIM=604.30

40 MANHOLE RECONSTRUCTION
NEW TYPE 1 FR. & C.L.
RIM=604.70
INV(W)=592.55
INV(SE)=599.66
INV(NE)=598.16
INV(E)=595.11

41 MANHOLE RECONSTRUCTION
NEW TYPE 1 FR. & C.L.
RIM=604.20
INV(NW)=599.82
INV(SW)=600.22

42 CATCH BASIN RECONSTRUCTION
NEW TYPE 1 FR. & C.L.
RIM=604.00
INV(NE)=598.01

43 INLET ADJUSTMENT
RIM=603.50

44 INLET ADJUSTMENT
RIM=603.50

45 PROPOSED CATCH BASIN
TYPE A, 4' DIA.,
TYPE 1 FR. & C.L.
RIM=603.70
INV(W)=600.94
INV(SE)=599.26

46 PROPOSED CATCH BASIN
TYPE A, 4' DIA.,
TYPE 1 FR. & O.L.
RIM=603.60
INV(E)=597.97

47 PROPOSED INLET
TYPE A, 2' DIA.,
TYPE 1 FR. & O.L.
RIM=603.60
INV(W)=598.55

49 PROPOSED INLET
TYPE A, 2' DIA.,
TYPE 1 FR. & O.L.
RIM=603.70
INV(SW)=599.10

50 PROPOSED CATCH BASIN
TYPE A, 4' DIA.,
TYPE 1 FR. & O.L.
RIM=603.60
INV(NE)=598.66

51 MANHOLE ADJUSTMENT (SPECIAL)
NEW TYPE 1 FR. & C.L.
RIM=603.75
INV(W)=598.36
INV(NW)=598.56

52 PROPOSED CATCH BASIN
TYPE C, 2' DIA.
TYPE 1 FR. & O.L.
RIM= 603.60
INV(E)=597.90

53 INLET ADJUSTMENT
RIM=603.60

54 VALVE VAULT ADJUSTMENT (SPECIAL)
RIM=603.75

55 CATCH BASIN ADJUSTMENT
RIM=604.30
INV(SE)=600.72

56 MANHOLE RECONSTRUCTION
RIM=604.60
INV(SE)=597.64
INV(NE)=599.84

57 CATCH BASIN ADJUSTMENT
NEW TYPE 1 FR. & C.L.
RIM=604.20
INV(NE)=600.05

58 INLET ADJUSTMENT
NEW TYPE 1 FR. & O.L.
RIM=603.60

59 INLET ADJUSTMENT
NEW TYPE 1 FR. & O.L.
RIM=603.60

60 CATCH BASIN ADJUSTMENT
RIM=604.30, INV(SW)=600.66

61 VALVE VAULT ADJUSTMENT (SPECIAL)
NEW TYPE 1 FR. & C.L.
RIM=603.90

62 VALVE VAULT RECONSTRUCTION
RIM=604.05

63 PROPOSED INLET
TYPE C, 2' DIA.,
TYPE 1 FR. & O.L.
RIM=603.75
INV(W)=599.00

64 PROPOSED CATCH BASIN
TYPE A, 4' DIA.,
TYPE 1 FR. & C.L.
RIM= 604.50
INV(E)=598.45
INV(W)=599.45

65 PROPOSED INLET
TYPE A, 2' DIA.
TYPE 1 FR. & O.L.
RIM= 603.60
INV(E)=600.25

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET 4

IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

NOTE: ALL SCHEDULED RIM ELEVATIONS ARE PROPOSED.

Frank Novotny & Associates, Inc.
Civil Engineers
825 Midway Drive ◆ Willowbrook, IL ◆ 60527 ◆ Telephone: (630) 887-8640 ◆ Fax: (630) 887-0132
Illinois Professional Design Firm No. 184-000928

PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS			
NO.	BY	DATE	DESCRIPTION
1	AMS	1/22/10	PER IDOT REVIEW

PLAN:
CENTRAL AVENUE-
STA. 9+00 TO STA. 18+00

(RESURFACING)

PROJECT NO. 05043	SCALE 1"=40'
DRAWN/DESIGNED JFP-JEP/THK	DATE DEC., 2009
CHECKED/APPROVED JLC/THK	FIELD BOOK NO. FILE

SHEET 8 OF 51 SHEETS

LEGEND

- DENOTES HOT-MIX ASPHALT SURFACE REMOVAL, (SEE TYPICAL CROSS SECTIONS), TO BE REPLACED WITH "LEVELING BINDER (MACHINE METHOD), N70," 1 INCH AVG., "AREA REFLECTIVE CRACK CONTROL TREATMENT, SYSTEM A" AND "HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70", 2 INCH"
- DENOTES "DRIVEWAY REMOVAL" AND "SIDEWALK REMOVAL" (AS DIRECTED BY THE ENGINEER)
- DENOTES P.C. CONCRETE SIDEWALK, 5 INCH, WITH DETECTABLE WARNING
- DENOTES "P.C. CONCRETE DRIVEWAY PAVEMENT, 7 INCH" AND "P.C. CONCRETE SIDEWALK, 5 INCH" ON SUBBASE GRANULAR MATERIAL, TYPE B, 2 INCH"
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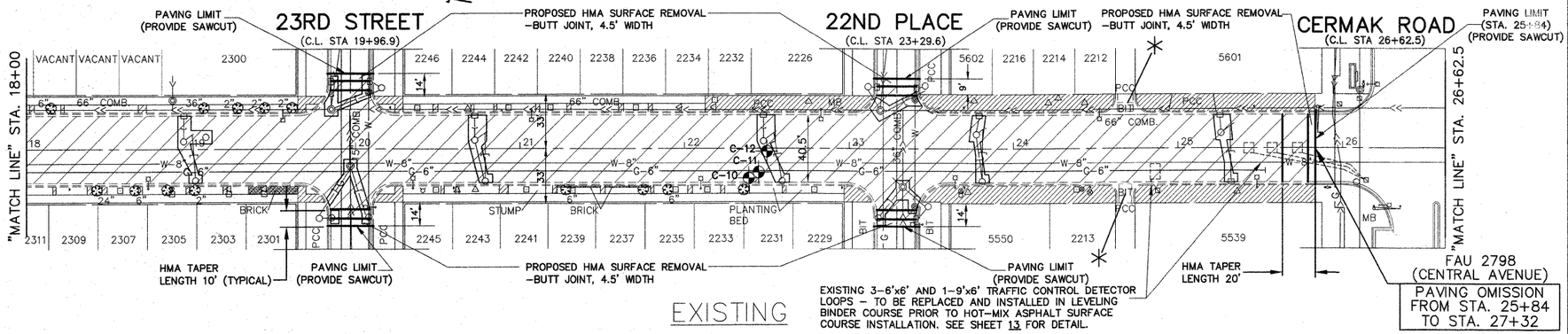
- "R" DENOTES EXISTING STRUCTURES TO BE RECONSTRUCTED
- "F" DENOTES EXISTING STRUCTURES TO BE FILLED
- DENOTES ALLEY RETURN TO BE REMOVED WITH "PAVEMENT REMOVAL" AND REPLACED WITH "PORTLAND CEMENT CONCRETE PAVEMENT, 8 INCH".
- DENOTES "COMBINATION CURB AND GUTTER REMOVAL"
- DENOTES "COMBINATION CURB AND GUTTER, TYPE B-V.12"
- 20' STORM SEWER (WATER MAIN QUALITY) 8 INCH (LENGTH AS NOTED)

BENCHMARK

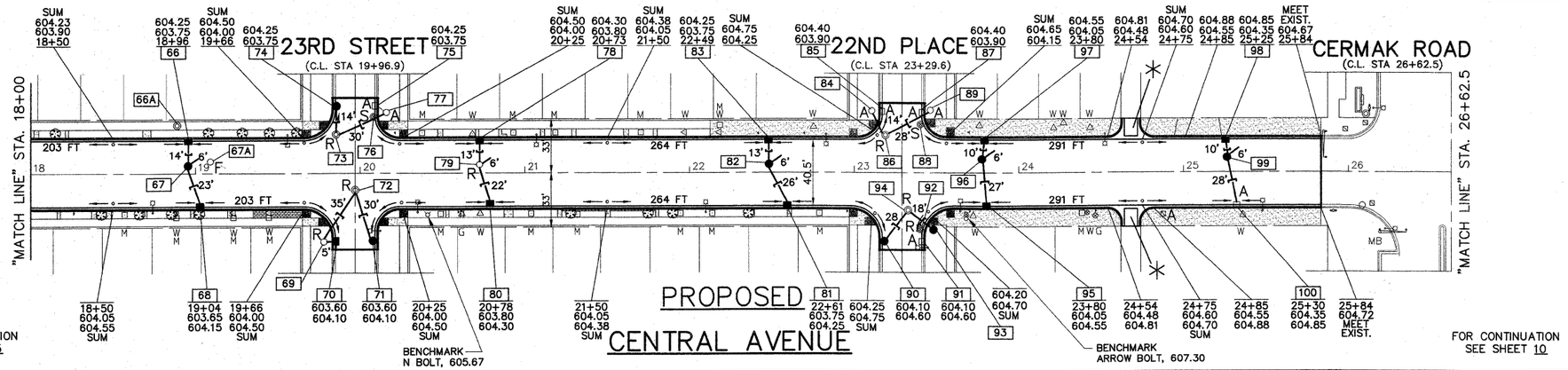
N BOLT ON HYDRANT AT NORTHEAST CORNER OF 23RD STREET AND CENTRAL AVENUE
ELEV. 605.67

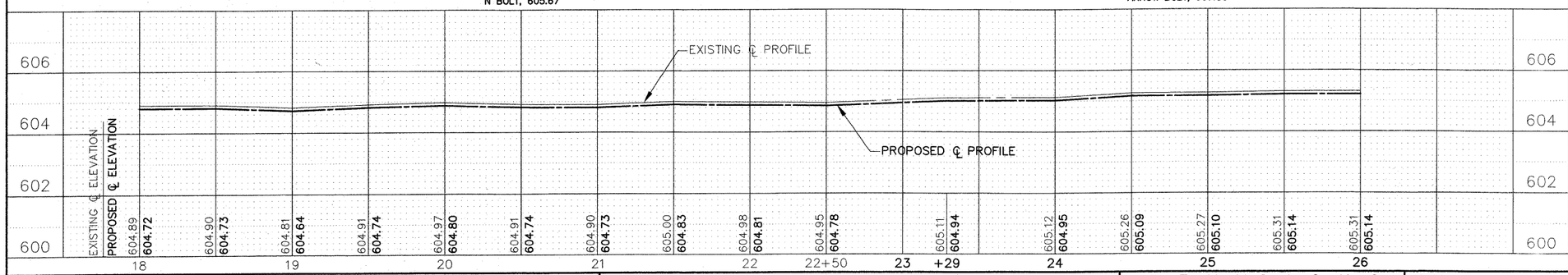
ARROW BOLT ON HYDRANT AT NORTHEAST CORNER OF 22ND PLACE AND CENTRAL AVENUE
ELEV. 607.30



EXISTING

PROPOSED CENTRAL AVENUE





FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	9
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63485

STRUCTURE SCHEDULE

66 PROPOSED INLET
TYPE A, 2' DIA.
TYPE 1 FR. & O.L.
RIM= 603.75
INV(E)=599.93

66A SANITARY MANHOLE NO WORK

67 PROPOSED CATCH BASIN
TYPE A, 4' DIA.,
TYPE 1 FR. & C.L.
RIM= 604.70
INV(E)=599.04
INV(W)=599.78
INV(NW)=598.10

67A ABANDONED CATCH BASIN
FIELD VERIFY AND FILL

68 PROPOSED INLET
TYPE A, 2' DIA.
TYPE 1 FR. & O.L.
RIM= 603.65
INV(W)=599.29

69 CATCH BASIN RECONSTRUCTION
RIM= 604.50
INV(NW)=599.70
INV(N)=600.85

70 PROPOSED INLET
TYPE A, 2' DIA.
TYPE 1 FR. & O.L.
RIM= 603.60
INV(S)=601.02

71 PROPOSED CATCH BASIN
TYPE C, 2' DIA.,
TYPE 1 FR. & O.L.
RIM= 603.60
INV(SW)=598.77

72 MANHOLE RECONSTRUCTION
NEW TYPE 1 FR. & O.L.
RIM= 604.60
INV(SE)=598.53
INV(NE)=597.08

73 MANHOLE RECONSTRUCTION
NEW TYPE 1 FR. & C.L.
RIM= 604.10
INV(W)=598.75
INV(NW)=597.65

74 PROPOSED CATCH BASIN
TYPE C, 2 DIA.,
TYPE A, FR. & O.L.
RIM= 603.75
INV(E)=599.05

75 INLET ADJUSTMENT
NEW TYPE 1 FR. & O.L.
RIM= 603.75

76 VALVE VAULT ADJUSTMENT
(SPECIAL)
RIM= 603.90

77 CATCH BASIN ADJUSTMENT
RIM= 604.30
INV(SE)=599.31
INV(SW)=600.24

78 PROPOSED INLET
TYPE A, 2' DIA.,
TYPE 1 FR. & O.L.
RIM= 603.80
INV(E)=601.10

79 CATCH BASIN RECONSTRUCTION
RIM= 604.70
INV(W)=600.00
INV(E)=598.20
INV(NW)=597.99

80 PROPOSED INLET
TYPE A, 2' DIA.,
TYPE 1 FR. & O.L.
RIM= 603.80
INV(W)=598.70

81 PROPOSED INLET
TYPE A, 2' DIA.,
TYPE 1 FR. & O.L.
RIM= 603.75
INV(SW)=600.50

82 PROPOSED CATCH BASIN
TYPE A, 4' DIA.
TYPE 1 FR. & C.L.
RIM= 604.80
INV(E)=600.00
INV(W)=600.00
INV(NW)=598.26

83 PROPOSED INLET
TYPE A, 2' DIA.
TYPE 1 FR. & O.L.
RIM= 603.75
INV(E)=601.34

84 CATCH BASIN ADJUSTMENT
NEW TYPE 1 FR. & C.L.
RIM= 604.50
INV(N)=600.10
INV(NE)=600.00

85 INLET ADJUSTMENT
NEW TYPE 1 FR. & O.L.
RIM= 603.90

86 MANHOLE RECONSTRUCTION
NEW TYPE 1 FR. & C.L.
RIM= 604.35
INV(SW)= 600.97
INV(SW)= 600.97

87 INLET ADJUSTMENT
NEW TYPE 1 FR. & O.L.
RIM= 603.90

88 VALVE VAULT ADJUSTMENT
(SPECIAL)
NEW TYPE 1 FR. & C.L.
RIM= 604.10

89 CATCH BASIN ADJUSTMENT
NEW TYPE 1 FR. & C.L.
RIM= 604.50
INV(S)=599.99
INV(SE)=599.90

90 PROPOSED CATCH BASIN
TYPE C, 2' DIA.,
TYPE 1 FR. & O.L.
RIM= 604.10
INV(NW)=599.17

91 INLET ADJUSTMENT
NEW TYPE 1 FRA. & O.L.
RIM= 604.10

92 VALVE VAULT RECONSTRUCTION
NEW TYPE 1 FR. & C.L.
RIM= 604.20

93 PROPOSED CATCH BASIN
TYPE A, 4' DIA.,
TYPE 1 FR. & C.L.
RIM= 604.60
INV(SW)=599.64
INV(SE)=600.19

94 MANHOLE RECONSTRUCTION
TYPE 1 FR. & C.L.
RIM= 604.50
INV(NE)=598.80
INV(SE)=598.72

95 PROPOSED INLET
TYPE A, 2' DIA.,
TYPE 1 FR. & O.L.
RIM= 604.05
INV(E)=600.68

96 PROPOSED CATCH BASIN
TYPE A, 4' DIA.,
TYPE 1 FR. & C.L.
RIM= 604.90
INV(E)=598.92
INV(W)=600.42
INV(NW)=598.42

97 PROPOSED INLET
TYPE A, 2' DIA.,
TYPE 1 FR. & O.L.
RIM= 604.05
INV(E)=600.68

98 PROPOSED INLET
TYPE A, 2' DIA.
TYPE 1 FR. & O.L.
RIM= 604.35
INV(E)=602.49

99 PROPOSED CATCH BASIN
TYPE A, 4' DIA.,
TYPE 1 FR. & C.L.
RIM= 604.60
INV(E)=599.00
INV(W)=600.73
INV(NW)=598.75

100 PROPOSED INLET
TYPE A, 2' DIA.
TYPE 1 FR. & O.L.
RIM= 604.35
INV(W)=599.50

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET 4

IMPORTANT!
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

NOTE: ALL SCHEDULED RIM ELEVATIONS ARE PROPOSED.

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Illinois Professional Design Firm No. 184-000928

PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS			
NO.	BY	DATE	DESCRIPTION
1	AMS	1/22/10	PER IDOT REVIEW

PLAN:
CENTRAL AVENUE-
STA. 18+00 TO CERMAK RD.
(RESURFACING)

PROJECT NO. 05043	SCALE 1"=40'
DRAWN/DESIGNED JFP-JEP/THK	DATE DEC., 2009
CHECKED/APPROVED JLC/THK	FIELD BOOK NO. FILE

SHEET 9 OF 51 SHEETS

LEGEND

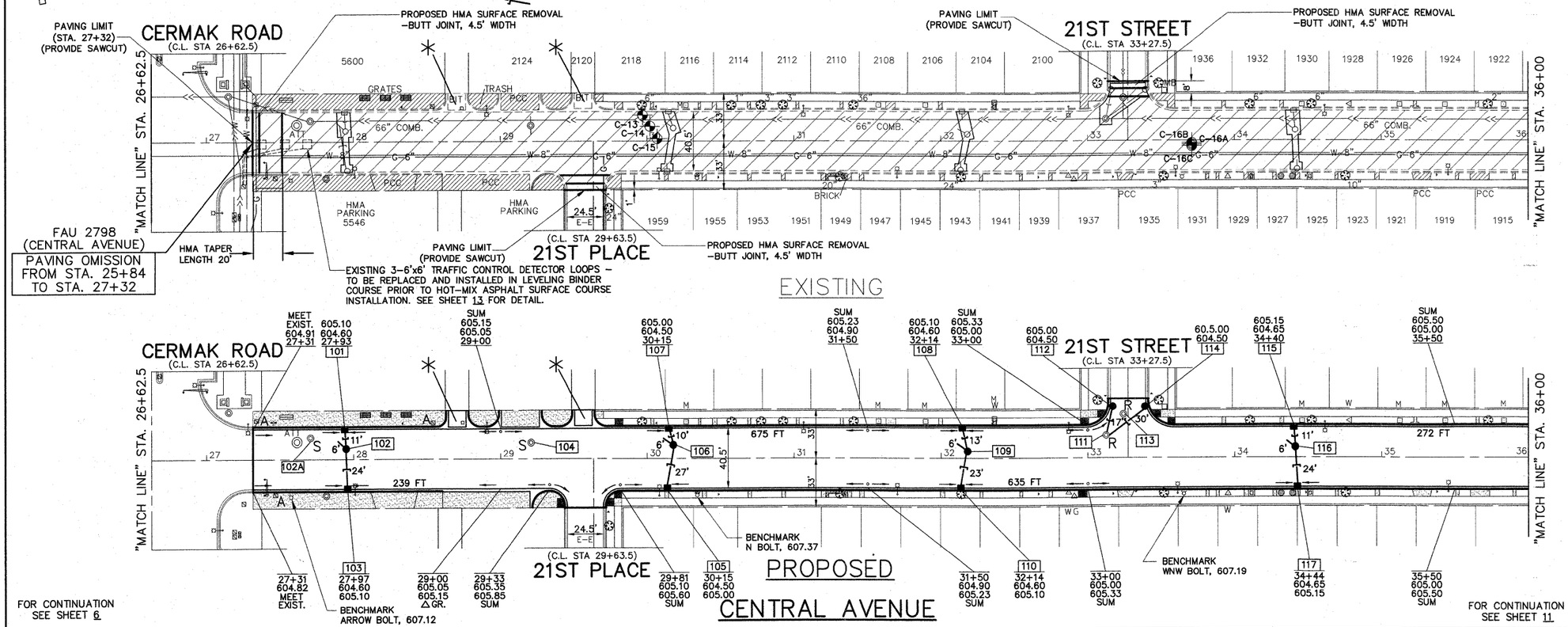
- DENOTES HOT-MIX ASPHALT SURFACE REMOVAL, (SEE TYPICAL CROSS SECTIONS), TO BE REPLACED WITH "LEVELING BINDER (MACHINE METHOD), N70," 1 INCH AVG., "AREA REFLECTIVE CRACK CONTROL TREATMENT, SYSTEM A" AND "HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70", 2 INCH"
- DENOTES "DRIVEWAY REMOVAL" AND "SIDEWALK REMOVAL" (AS DIRECTED BY THE ENGINEER)
- DENOTES P.C. CONCRETE SIDEWALK, 5 INCH, WITH DETECTABLE WARNING
- DENOTES "P.C. CONCRETE DRIVEWAY PAVEMENT, 7 INCH" AND "P.C. CONCRETE SIDEWALK, 5 INCH" ON SUBBASE GRANULAR MATERIAL, TYPE B, 2 INCH"
- DENOTES HOT-MIX DRIVEWAY REMOVAL AND REPLACEMENT
- DENOTES CLASS D PATCHES, TYPE II - IV, 12", - AS DIRECTED
- "A" DENOTES EXISTING VALVE VAULTS, VALVE BOXES, INLETS, CATCH BASINS AND MANHOLES TO BE CLEANED, GROUTED, AND ADJUSTED TO GRADE WITH NEW ADJUSTING RINGS. (ALL TOP BANDS OF FALLEN, COMMON OR CONCRETE BRICK TO BE REPLACED WITH PRECAST CONCRETE ADJUSTING RINGS)
- "S" DENOTES EXISTING FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) (SEE SHEET 41 FOR DETAIL)
- "R" DENOTES EXISTING STRUCTURES TO BE RECONSTRUCTED
- "F" DENOTES EXISTING STRUCTURES TO BE FILLED
- DENOTES ALLEY RETURN TO BE REMOVED WITH "PAVEMENT REMOVAL" AND REPLACED WITH "PORTLAND CEMENT CONCRETE PAVEMENT, 8 INCH".
- DENOTES "COMBINATION CURB AND GUTTER REMOVAL"
- DENOTES "COMBINATION CURB AND GUTTER, TYPE B-V.12"
- 20' STORM SEWER (WATER MAIN QUALITY) 8 INCH (LENGTH AS NOTED)

BENCHMARK

ARROW BOLT ON HYDRANT AT NORTHEAST CORNER OF CERMAK ROAD AND CENTRAL AVENUE
ELEV. 607.12

N BOLT ON HYDRANT AT 1955 S. CENTRAL AVENUE
ELEV. 607.37

WNW BOLT ON HYDRANT AT 1931 S. CENTRAL AVENUE
ELEV. 607.19



FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	10
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63495

STRUCTURE SCHEDULE

101 PROPOSED INLET
TYPE A, 2' DIA., TYPE 1 FR. & O.L.
RIM= 604.60
INV(E)= 602.28

102 PROPOSED CATCH BASIN
TYPE A, 4' DIA., TYPE 1 FR. & C.L.
RIM= 605.30
INV(W)= 600.66
INV(SW)= 598.76
INV(E)= 599.25

102A MANHOLE ADJUSTMENT (SPECIAL)
RIM=605.30

103 PROPOSED INLET
TYPE A, 2' DIA., TYPE 1 FR. & O.L.
RIM= 604.60
INV(W)= 599.51

104 MANHOLE ADJUSTMENT (SPECIAL)
NEW TYPE 1 FR. & C.L.
RIM= 605.30

105 PROPOSED INLET
TYPE A, 2' DIA., TYPE 1 FR. & O.L.
RIM= 604.50
INV(W)= 600.09

106 CATCH BASIN ADJUSTMENT (SPECIAL)
RIM= 605.50
INV(W)= 599.80
INV(SW)= 598.60
INV(E)= 599.80

107 PROPOSED INLET
TYPE A, 2' DIA., TYPE 1 FR. & O.L.
RIM= 604.50
INV(E)= 601.90

108 PROPOSED INLET
TYPE A, 2' DIA., TYPE 1 FR. & O.L.
RIM= 604.60
INV= 601.40

109 PROPOSED CATCH BASIN
TYPE A, 4' DIA., TYPE 1 FR. & C.L.
RIM= 605.52
INV(W)= 599.27
INV(SW)= 598.40
INV(E)= 598.50

110 PROPOSED INLET
TYPE A, 2' DIA., TYPE 1 FR. & O.L.
RIM= 604.60
INV(W)= 598.73

111 MANHOLE RECONSTRUCTION
NEW TYPE 1 FR. & C.L.
RIM= 605.10
INV(W)= 599.72
INV(NW)= 599.55

112 PROPOSED CATCH BASIN
TYPE C, 2' DIA., TYPE 1 FR. & O.L.
RIM= 604.50
INV(SE)= 600.00

113 MANHOLE RECONSTRUCTION
TYPE 1 FR. & C.L.
RIM= 605.40

114 PROPOSED CATCH BASIN
TYPE C, 2' DIA., TYPE 1 FR. & O.L.
RIM= 604.50
INV(SE)= 599.80

115 PROPOSED INLET
TYPE A, 2' DIA., TYPE 1 FR. & O.L.
RIM= 604.65
INV= 601.73

116 PROPOSED CATCH BASIN
TYPE A, 4' DIA., TYPE 1 FR. & C.L.
RIM= 605.68
INV(E)= 600.91
INV(W)= 600.91
INV(SW)= 598.61

117 PROPOSED INLET
TYPE A, 2' DIA., TYPE 1 FR. & O.L.
RIM= 604.65
INV(W)= 601.73

NOTE: ALL SCHEDULED RIM ELEVATIONS ARE PROPOSED.

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET 4

IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

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Illinois Professional Design Firm No. 184-000928

PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS

NO.	BY	DATE	DESCRIPTION
1	AMS	1/22/10	PER IDOT REVIEW

PLAN:
CENTRAL AVENUE-
CERMAK RD. TO STA. 36+00
(RESURFACING)

PROJECT NO.	SCALE
05043	1"=40'
DRAWN/DESIGNED	DATE
JFP-JEP/THK	DEC., 2009
CHECKED/APPROVED	FIELD BOOK NO.
JLC/THK	FILE

SHEET 10 OF 51 SHEETS

LEGEND

- DENOTES HOT-MIX ASPHALT SURFACE REMOVAL, (SEE TYPICAL CROSS SECTIONS), TO BE REPLACED WITH "LEVELING BINDER (MACHINE METHOD), N70," 1 INCH AVG., "AREA REFLECTIVE CRACK CONTROL TREATMENT, SYSTEM A" AND "HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70", 2 INCH"
- DENOTES "DRIVEWAY REMOVAL" AND "SIDEWALK REMOVAL" (AS DIRECTED BY THE ENGINEER)
- DENOTES P.C. CONCRETE SIDEWALK, 5 INCH, WITH DETECTABLE WARNING
- DENOTES "P.C. CONCRETE DRIVEWAY PAVEMENT, 7 INCH" AND "P.C. CONCRETE SIDEWALK, 5 INCH" ON SUBBASE GRANULAR MATERIAL, TYPE B, 2 INCH"
- DENOTES HOT-MIX DRIVEWAY REMOVAL AND REPLACEMENT
- DENOTES CLASS D PATCHES, TYPE II – IV, 12", – AS DIRECTED

"A" DENOTES EXISTING VALVE VAULTS, VALVE BOXES, INLETS, CATCH BASINS AND MANHOLES TO BE CLEANED, GROUTED, AND ADJUSTED TO GRADE WITH NEW ADJUSTING RINGS. (ALL TOP BANDS OF FALLEN, COMMON OR CONCRETE BRICK TO BE REPLACED WITH PRECAST CONCRETE ADJUSTING RINGS)

"S" DENOTES EXISTING FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) (SEE SHEET 41 FOR DETAIL)

"R" DENOTES EXISTING STRUCTURES TO BE RECONSTRUCTED

"F" DENOTES EXISTING STRUCTURES TO BE FILLED

DENOTES ALLEY RETURN TO BE REMOVED WITH "PAVEMENT REMOVAL" AND REPLACED WITH "PORTLAND CEMENT CONCRETE PAVEMENT, 8 INCH".

DENOTES "COMBINATION CURB AND GUTTER REMOVAL"

DENOTES "COMBINATION CURB AND GUTTER, TYPE B-V.12"

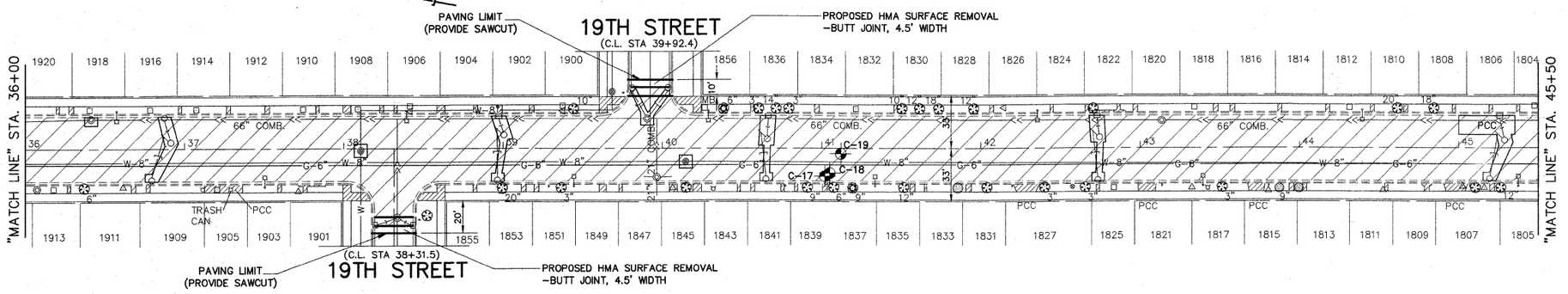
20' STORM SEWER (WATER MAIN QUALITY) 8 INCH (LENGTH AS NOTED)

BENCHMARK

E BOLT ON HYDRANT AT 1909 S. CENTRAL AVENUE
ELEV. 607.39

ARROW BOLT ON HYDRANT AT 1843 S. CENTRAL AVENUE
ELEV. 607.83

N BOLT ON HYDRANT AT 1817 S. CENTRAL AVENUE
ELEV. 607.58



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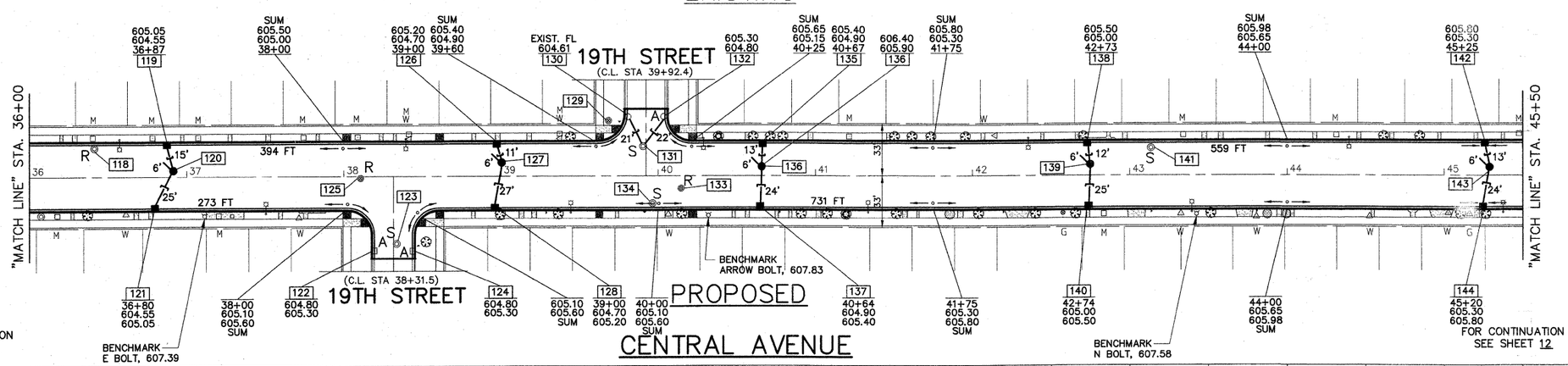

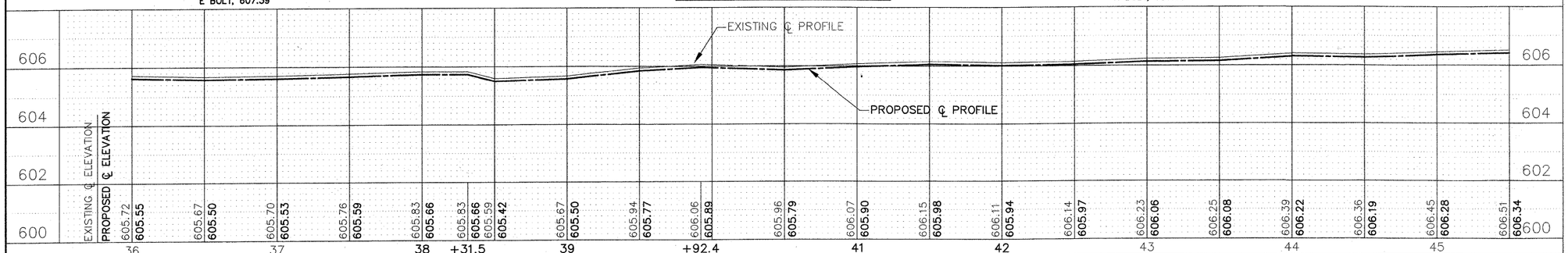

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	11
F.H.W.A. REG.	ILLINOIS	PROJECT M-9003(488)		

CONTRACT NO. 63455

STRUCTURE SCHEDULE 4

118 MANHOLE RECONSTRUCTION
NEW TYPE 1 FR. & C.L.
RIM= 604.70

119 PROPOSED INLET
TYPE A, 2' DIA
TYPE 1 FR. & O.L.
RIM= 604.55
INV(E)= 601.60

120 PROPOSED CATCH BASIN
TYPE A, 4' DIA.,
TYPE 1 FR. & C.L.
RIM= 605.70
INV(E)= 599.58
INV(SW)= 599.48
INV(W)= 601.38

121 PROPOSED INLET
TYPE A, 2' DIA
TYPE 1 FR. & O.L.
RIM= 604.55
INV(W)= 599.83

122 INLET ADJUSTMENT
RIM= 604.80

123 MANHOLE ADJUSTMENT (SPECIAL)
NEW TYPE 1 FR. & C.L.
RIM= 605.15

124 INLET ADJUSTMENT
NEW TYPE 1 FR. & O.L.
RIM= 604.80

125 VALVE VAULT RECONSTRUCTION
RIM= 605.80

126 PROPOSED INLET
TYPE A, 2' DIA
TYPE 1 FR. & O.L.
RIM= 604.70
INV(E)= 601.68

127 PROPOSED CATCH BASIN
TYPE A, 4' DIA.,
TYPE 1 FR. & C.L.
RIM= 605.70
INV(E)= 599.50
INV(SW)= 598.62
INV(W)= 601.17

128 PROPOSED INLET
TYPE A, 2' DIA
TYPE 1 FR. & O.L.
RIM= 604.70
INV(W) = 599.79

129 VALVE VAULT ADJUSTMENT
NEW TYPE 1 FR. & C.L.
RIM= 605.55

130 CATCH BASIN
NO WORK
RIM= 604.61
INV(NE)= 599.99

131 MANHOLE ADJUSTMENT (SPECIAL)
NEW TYPE 1 FR. & C.L.
RIM= 605.60
INV(SW)= 599.70
INV(NW)= 599.70

132 CATCH BASIN ADJUSTMENT
NEW TYPE 1 FR. & O.L.
RIM= 604.80
INV(SE)= 599.92

133 VALVE VAULT RECONSTRUCTION
NEW TYPE 1 FR. & C.L.
RIM= 605.85

134 MANHOLE ADJUSTMENT (SPECIAL)
NEW TYPE 1 FR. & C.L.
RIM= 605.20

135 PROPOSED INLET
TYPE A, 2' DIA.
TYPE 1 FR. & O.L.
RIM= 604.90
INV= 601.68

136 PROPOSED CATCH BASIN
TYPE A, 4' DIA.,
TYPE 1 FR. & C.L.
RIM= 605.90
INV(E)= 599.30
INV(W)= 601.16
INV(SW)= 599.00

137 PROPOSED INLET
TYPE A, 2' DIA
TYPE 1 FR. & O.L.
RIM= 604.90
INV(W)= 599.53

138 PROPOSED INLET
TYPE A, 2' DIA
TYPE 1 FR. & O.L.
RIM= 605.00
INV(E)= 602.11

139 PROPOSED CATCH BASIN
TYPE A, 4' DIA.,
TYPE 1 FR. & C.L.
RIM= 605.75
INV(E)= 599.92
INV(W)= 601.54
INV(SW)= 599.82

140 PROPOSED INLET
TYPE A, 2' DIA
TYPE 1 FR. & O.L.
RIM= 605.00
INV(W)= 600.05

141 MANHOLE ADJUSTMENT (SPECIAL)
RIM= 605.15

142 PROPOSED INLET
TYPE A, 2' DIA
TYPE 1 FR. & O.L.
RIM= 605.30
INV(E)= 602.00

143 PROPOSED CATCH BASIN
TYPE A, 4' DIA.,
TYPE 1 FR. & C.L.
RIM= 606.14
INV(W)= 601.49
INV(SW)= 600.14
INV(E)= 600.30

144 PROPOSED INLET
TYPE A, 2' DIA
TYPE 1 FR. & O.L.
RIM= 605.30
INV(W)= 600.60

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET 4

NOTE: ALL SCHEDULED RIM ELEVATIONS ARE PROPOSED.

IMPORTANT!

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PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS

NO.	BY	DATE	DESCRIPTION
1	AMS	1/22/10	PER IDOT REVIEW

PLAN:
CENTRAL AVENUE-
STA. 36+00 TO STA. 45+50
(RESURFACING)

PROJECT NO.	SCALE
05043	1"=40'
DRAWN/DESIGNED	DATE
JFP-JEP/THK	DEC., 2009
CHECKED/APPROVED	FIELD BOOK NO.
JLC/THK	FILE

SHEET 11 OF 51 SHEETS

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	12
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63405

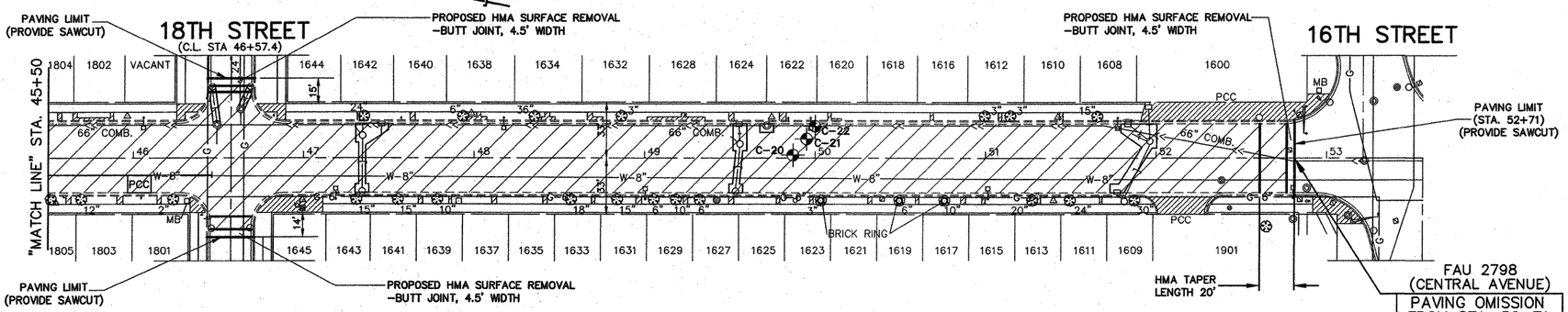
LEGEND

- DENOTES HOT-MIX ASPHALT SURFACE REMOVAL, (SEE TYPICAL CROSS SECTIONS), TO BE REPLACED WITH "LEVELING BINDER (MACHINE METHOD), N70," 1 INCH AVG., "AREA REFLECTIVE CRACK CONTROL TREATMENT, SYSTEM A" AND "HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70", 2 INCH"
- DENOTES "DRIVEWAY REMOVAL" AND "SIDEWALK REMOVAL" (AS DIRECTED BY THE ENGINEER)
- DENOTES P.C. CONCRETE SIDEWALK, 5 INCH, WITH DETECTABLE WARNING
- DENOTES "P.C. CONCRETE DRIVEWAY PAVEMENT, 7 INCH" AND "P.C. CONCRETE SIDEWALK, 5 INCH" ON SUBBASE GRANULAR MATERIAL, TYPE B, 2 INCH"
- DENOTES HOT-MIX DRIVEWAY REMOVAL AND REPLACEMENT
- DENOTES CLASS D PATCHES, TYPE II - IV, 12", - AS DIRECTED
- "A" DENOTES EXISTING VALVE VAULTS, VALVE BOXES, INLETS, CATCH BASINS AND MANHOLES TO BE CLEANED, GROUTED, AND ADJUSTED TO GRADE WITH NEW ADJUSTING RINGS. (ALL TOP BANDS OF FALLEN, COMMON OR CONCRETE BRICK TO BE REPLACED WITH PRECAST CONCRETE ADJUSTING RINGS)
- "S" DENOTES EXISTING FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) (SEE SHEET 41 FOR DETAIL)
- "R" DENOTES EXISTING STRUCTURES TO BE RECONSTRUCTED
- "F" DENOTES EXISTING STRUCTURES TO BE FILLED
- DENOTES ALLEY RETURN TO BE REMOVED WITH "PAVEMENT REMOVAL" AND REPLACED WITH "PORTLAND CEMENT CONCRETE PAVEMENT, 8 INCH".
- DENOTES "COMBINATION CURB AND GUTTER REMOVAL."
- DENOTES "COMBINATION CURB AND GUTTER, TYPE B-V.12"
- 20' STORM SEWER (WATER MAIN QUALITY) 8 INCH (LENGTH AS NOTED)

BENCHMARK

ARROW BOLT ON HYDRANT AT NORTHEAST CORNER OF 18TH STREET AND CENTRAL AVENUE
ELEV. 608.46

ARROW BOLT ON HYDRANT AT 1623 S. CENTRAL AVENUE
ELEV. 607.75



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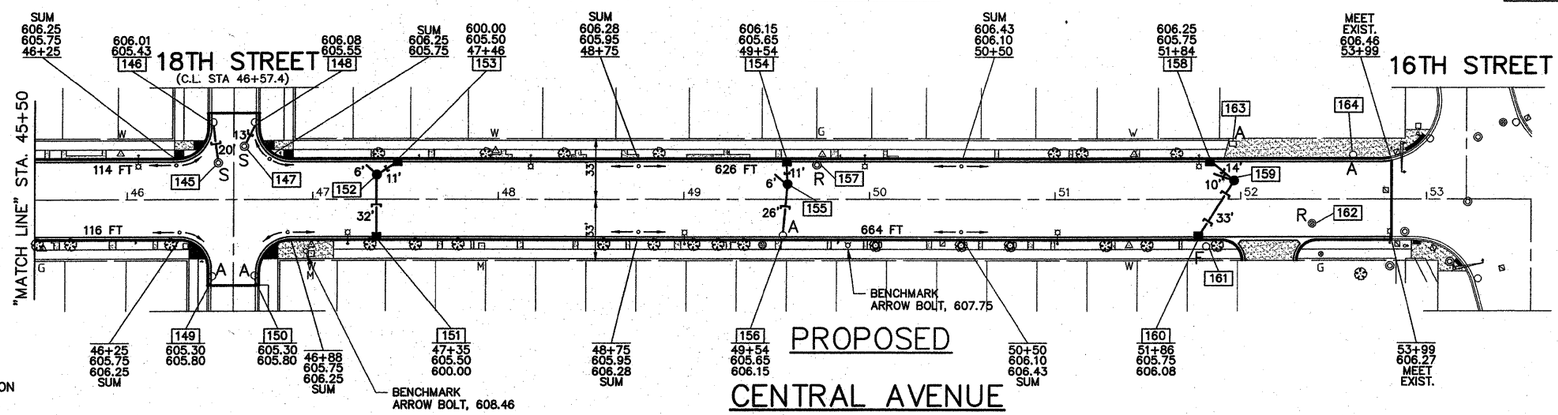
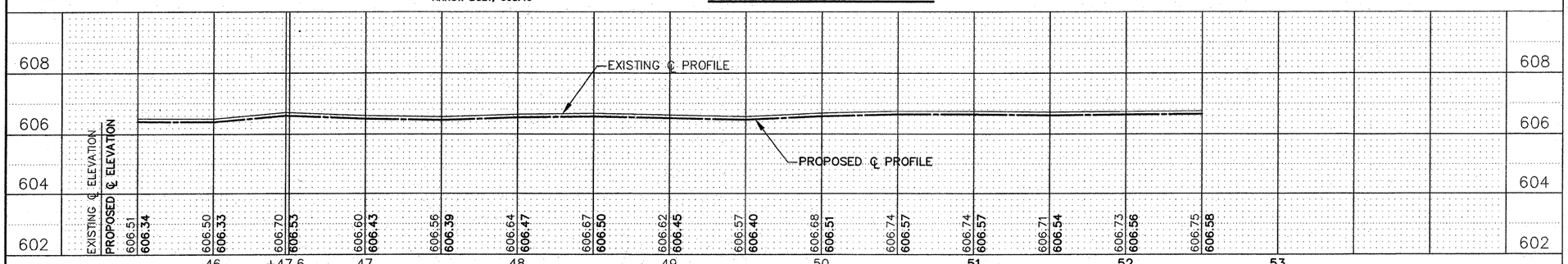


PROPOSED

CENTRAL AVENUE

FOR CONTINUATION SEE SHEET 8

FOR CONTINUATION SEE SHEET 13



STRUCTURE SCHEDULE

145 MANHOLE ADJUSTMENT (SPECIAL)
RIM= 606.15
INV(SW)= 600.19

146 CATCH BASIN NO WORK
RIM= 605.43
INV(NE)= 600.78

147 MANHOLE ADJUSTMENT (SPECIAL)
RIM= 605.95
INV(NW)= 600.27

148 CATCH BASIN NO WORK
RIM= 605.55
INV= 600.87

149 CATCH BASIN ADJUSTMENT
RIM= 605.30

150 CATCH BASIN ADJUSTMENT
RIM= 605.30

151 PROPOSED INLET
TYPE A, 2' DIA., TYPE 1 FR. & O.L.
RIM= 605.50
INV(W)= 600.10

152 PROPOSED CATCH BASIN
TYPE A, 4' DIA., TYPE 1 FR. & C.L.
RIM= 606.15
INV(E)= 599.99
INV(SW)= 599.89
INV(W)= 601.74

153 PROPOSED INLET
TYPE A, 2' DIA. TYPE 1 FR. & O.L.
RIM= 605.50
INV(E)= 602.38

154 PROPOSED INLET
TYPE A, 2' DIA., TYPE 1 FR. & O.L.
RIM= 605.65
INV(E)= 602.54

155 CATCH BASIN RECONSTRUCTION
RIM= 605.80
INV(E)= 602.00
INV(SW)= 600.34
INV(W)= 601.99

156 CATCH BASIN ADJUSTMENT
RIM= 605.65
INV(W)= 602.25

157 MANHOLE RECONSTRUCTION
NEW TYPE 1 FR. & C.L.
RIM= 605.85

158 PROPOSED INLET
TYPE 1 FR. & O.L.
RIM= 605.75
INV(E)= 602.79

159 CATCH BASIN RECONSTRUCTION
RIM= 606.20
INV(E)= 602.80
INV(W)= 600.92
(OUTFALL)INV(SW)= 599.77
INV(SW)= 601.82

160 PROPOSED INLET
TYPE 1 FR. & O.L.
RIM= 605.75
INV(W)= 603.13

161 CATCH BASIN TO BE FILLED

162 VALVE VAULT RECONSTRUCTION
NEW TYPE 1 FR. & C.L.
RIM=606.30

163 INLET ADJUSTMENT
RIM= 606.60

164 CATCH BASIN ADJUSTMENT
NEW TYPE 1 FR. & C.L.
RIM= 606.50

NOTE: ALL SCHEDULED RIM ELEVATIONS ARE PROPOSED.

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET 4

IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

Civil Engineers
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Illinois Professional Design Firm No. 184-000928

PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS			
NO.	BY	DATE	DESCRIPTION
1	AMS	1/22/10	PER IDOT REVIEW
2	AMS	2/8/10	PER IDOT REVIEW

PLAN:
CENTRAL AVENUE-
STA. 45+50 TO 16th ST.
(RESURFACING)

PROJECT NO. 05043	SCALE 1"=40'
DRAWN/DESIGNED JFP-JEP/THK	DATE DEC., 2009
CHECKED/APPROVED JLC/THK	FIELD BOOK NO. FILE

SHEET 12 OF 51 SHEETS

LEGEND

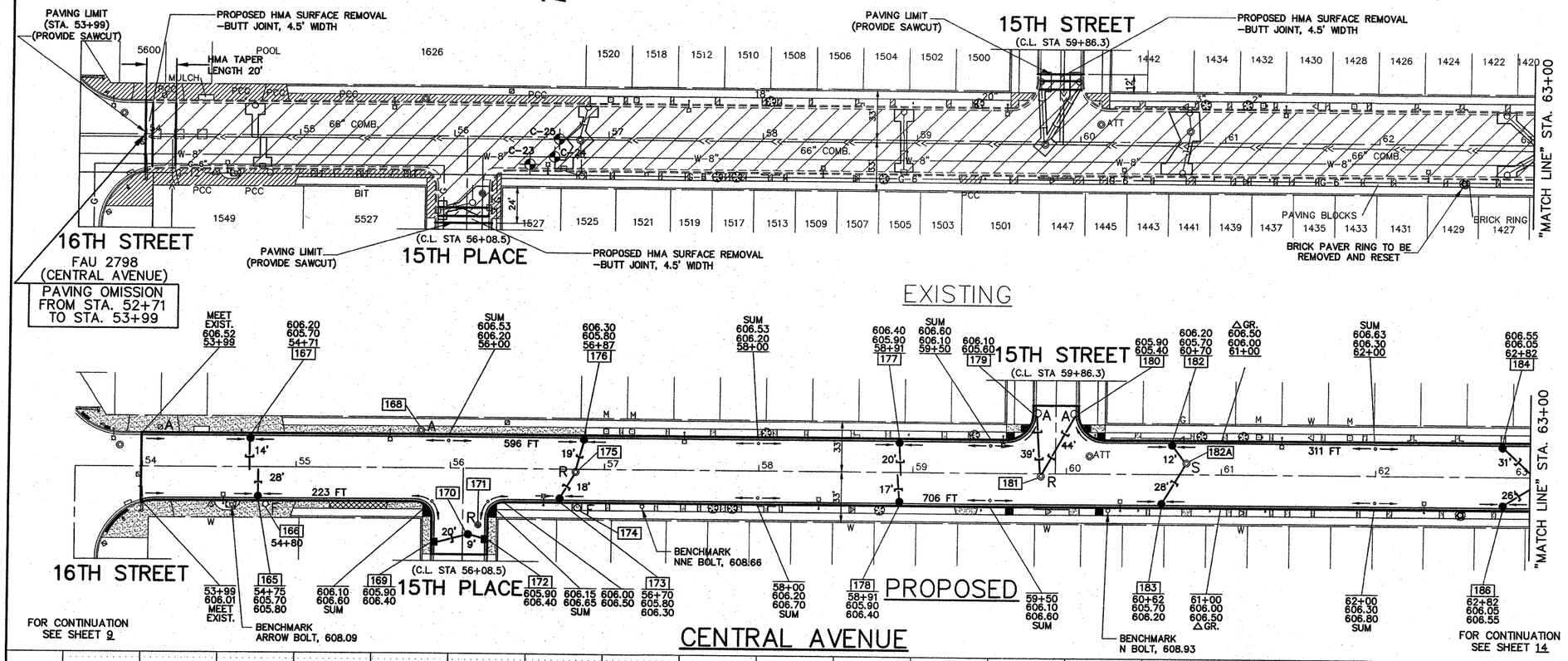
- DENOTES HOT-MIX ASPHALT SURFACE REMOVAL, (SEE TYPICAL CROSS SECTIONS), TO BE REPLACED WITH "LEVELING BINDER (MACHINE METHOD), N70," 1 INCH AVG., "AREA REFLECTIVE CRACK CONTROL TREATMENT, SYSTEM A" AND "HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70", 2 INCH"
- DENOTES "DRIVEWAY REMOVAL" AND "SIDEWALK REMOVAL" (AS DIRECTED BY THE ENGINEER)
- DENOTES P.C. CONCRETE SIDEWALK, 5 INCH, WITH DETECTABLE WARNING
- DENOTES "P.C. CONCRETE DRIVEWAY PAVEMENT, 7 INCH" AND "P.C. CONCRETE SIDEWALK, 5 INCH" ON SUBBASE GRANULAR MATERIAL, TYPE B, 2 INCH"
- DENOTES HOT-MIX DRIVEWAY REMOVAL AND REPLACEMENT
- DENOTES CLASS D PATCHES, TYPE II - IV, 12", - AS DIRECTED
- "A" DENOTES EXISTING VALVE VAULTS, VALVE BOXES, INLETS, CATCH BASINS AND MANHOLES TO BE CLEANED, GROUTED, AND ADJUSTED TO GRADE WITH NEW ADJUSTING RINGS. (ALL TOP BANDS OF FALLEN, COMMON OR CONCRETE BRICK TO BE REPLACED WITH PRECAST CONCRETE ADJUSTING RINGS)
- "S" DENOTES EXISTING FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) (SEE SHEET 41 FOR DETAIL)
- "R" DENOTES EXISTING STRUCTURES TO BE RECONSTRUCTED
- "F" DENOTES EXISTING STRUCTURES TO BE FILLED
- DENOTES ALLEY RETURN TO BE REMOVED WITH "PAVEMENT REMOVAL" AND REPLACED WITH "PORTLAND CEMENT CONCRETE PAVEMENT, 8 INCH".
- DENOTES "COMBINATION CURB AND GUTTER REMOVAL"
- DENOTES "COMBINATION CURB AND GUTTER, TYPE B-V.12"
- 20' STORM SEWER (WATER MAIN QUALITY) 8 INCH (LENGTH AS NOTED)

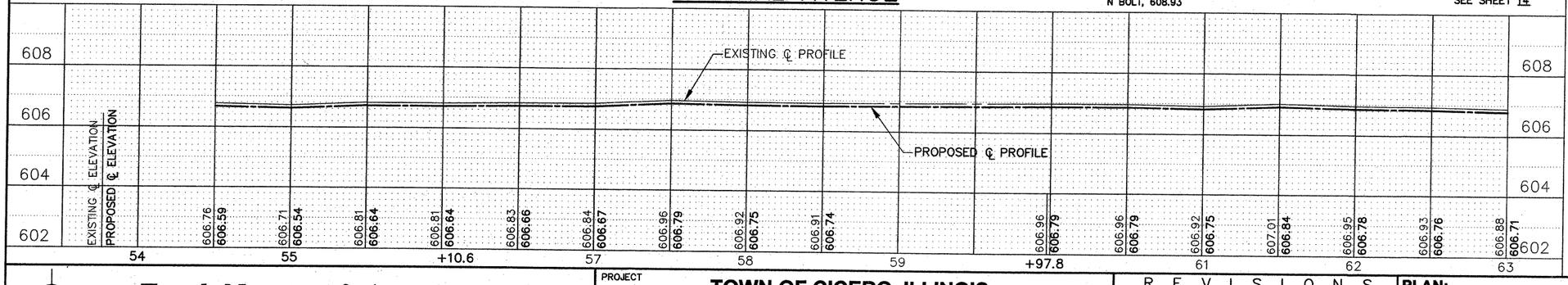
BENCHMARK

ARROW BOLT ON HYDRANT AT 1549 S. CENTRAL AVENUE
ELEV. 608.09

NNE BOLT ON HYDRANT AT 1521 S. CENTRAL AVENUE
ELEV. 608.66

N BOLT ON HYDRANT AT 1445 S. CENTRAL AVENUE
ELEV. 608.93





STRUCTURE SCHEDULE

- 165 PROPOSED CATCH BASIN TYPE C, 2' DIA., TYPE 1 FR. & O.L. RIM= 605.70 INV(NE)= 601.45
- 166 CATCH BASIN TO BE FILLED
- 167 PROPOSED CATCH BASIN TYPE C, 2' DIA., TYPE 1 FR. & O.L. RIM= 605.70 INV(E)= 600.09
- 168 MANHOLE ADJUSTMENT (SPECIAL) NEW TYPE 1 FR. & C.L. RIM= 606.55
- 169 PROPOSED INLET TYPE A, 2' DIA. TYPE 1 FR. & O.L. RIM= 605.90 INV(NW)= 604.17
- 170 PROPOSED CATCH BASIN TYPE A, 4' DIA., TYPE 1 FR. & C.L. RIM= 606.25 INV(SE)= 602.15 INV(NE)= 602.07
- 171 VALVE VAULT RECONSTRUCTION RIM= 606.25
- 172 PROPOSED INLET TYPE A, 2' DIA. TYPE 1 FR. & O.L. RIM= 605.90 INV(SW)= 603.64
- 173 PROPOSED CATCH BASIN TYPE C, 2' DIA, TYPE 1 FR. & O.L. RIM= 605.80 INV(NW)= 602.99
- 174 CATCH BASIN TO BE FILLED
- 175 MANHOLE RECONSTRUCTION RIM= 606.60 INV(E)= 602.75 INV(W)= 597.63
- 176 PROPOSED CATCH BASIN TYPE C, 2' DIA., TYPE 1 FR. & O.L. RIM= 605.80 INV= 600.26
- 177 PROPOSED CATCH BASIN TYPE C, 2' DIA., TYPE 1 FR. & O.L. RIM= 605.90 INV(E)= 600.17
- 178 PROPOSED CATCH BASIN TYPE C, 2' DIA., TYPE 1 FR. & O.L. RIM= 605.90 INV(W)= 599.39
- 179 CATCH BASIN ADJUSTMENT RIM= 605.60 INV(NE)= 600.30
- 180 CATCH BASIN ADJUSTMENT RIM= 605.40 INV(SE)= 600.10
- 181 MANHOLE RECONSTRUCTION RIM= 606.80 INV(NW)= 598.36 INV(W)= 598.21
- 182 PROPOSED CATCH BASIN TYPE C, 2' DIA., TYPE 1 FR. & O.L. RIM= 605.70 INV(NE)= 599.61
- 182A MANHOLE ADJUSTMENT (SPECIAL) RIM= 606.50 INV(SW)= 599.20 INV(SE)= 601.00
- 183 PROPOSED CATCH BASIN TYPE C, 2' DIA., TYPE 1 FR. & O.L. RIM= 605.70 INV(NW)= 601.23
- 184 PROPOSED CATCH BASIN TYPE C, 2' DIA., TYPE 1 FR. & O.L. RIM= 606.05 INV(NW)= 602.64
- 186 PROPOSED CATCH BASIN TYPE C, 2' DIA., TYPE 1 FR. & O.L. RIM= 606.05 INV(NW)= 602.64

NOTE: ALL SCHEDULED RIM ELEVATIONS ARE PROPOSED.

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET 4

IMPORTANT!
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	13
F.H.W.A. REG.	ILLINOIS	PROJECT M-9003(488)		

CONTRACT NO. 63465

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Illinois Professional Design Firm No. 184-000928

PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS

NO.	BY	DATE	DESCRIPTION
1	AMS	1/22/10	PER IDOT REVIEW
2	AMS	2/8/10	PER IDOT REVIEW

PLAN:
CENTRAL AVENUE-
16th ST. TO STA. 63+50
(RESURFACING)

PROJECT NO.	SCALE	SHEET
05043	1"=40'	13 OF 51 SHEETS
DRAWN/DESIGNED: JFP-JEP/THK	DATE: DEC., 2009	
CHECKED/APPROVED: JLC/THK	FIELD BOOK NO.: FILE	

LEGEND

- DENOTES HOT-MIX ASPHALT SURFACE REMOVAL, (SEE TYPICAL CROSS SECTIONS), TO BE REPLACED WITH "LEVELING BINDER (MACHINE METHOD), N70," 1 INCH AVG., "AREA REFLECTIVE CRACK CONTROL TREATMENT, SYSTEM A" AND "HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70", 2 INCH"
- DENOTES "DRIVEWAY REMOVAL" AND "SIDEWALK REMOVAL" (AS DIRECTED BY THE ENGINEER)
- DENOTES P.C. CONCRETE SIDEWALK, 5 INCH, WITH DETECTABLE WARNING
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- DENOTES HOT-MIX DRIVEWAY REMOVAL AND REPLACEMENT
- DENOTES CLASS D PATCHES, TYPE II - IV, 12", - AS DIRECTED

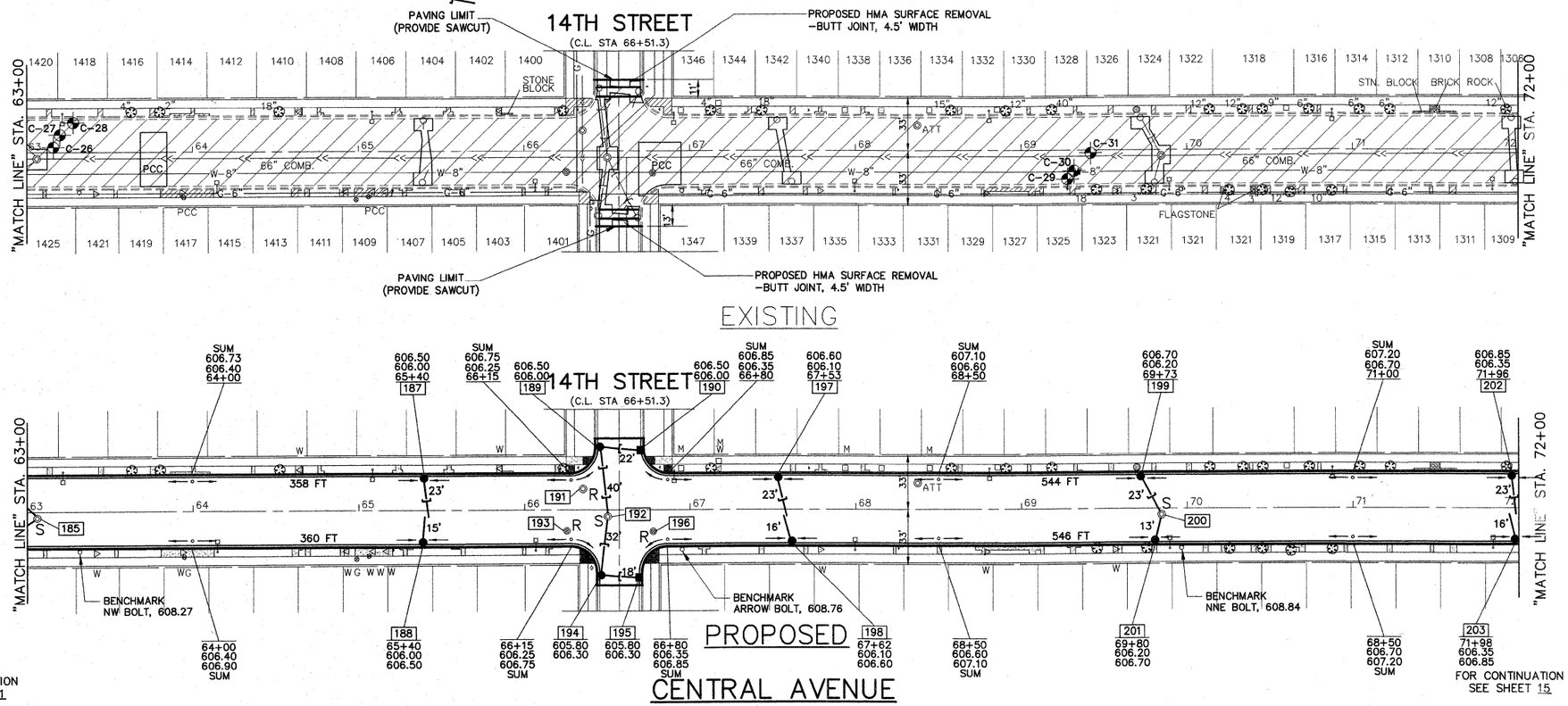
- "A" DENOTES EXISTING VALVE VAULTS, VALVE BOXES, INLETS, CATCH BASINS AND MANHOLES TO BE CLEANED, GROUTED, AND ADJUSTED TO GRADE WITH NEW ADJUSTING RINGS. (ALL TOP BANDS OF FALLEN, COMMON OR CONCRETE BRICK TO BE REPLACED WITH PRECAST CONCRETE ADJUSTING RINGS)
- "S" DENOTES EXISTING FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) (SEE SHEET 41 FOR DETAIL)
- "R" DENOTES EXISTING STRUCTURES TO BE RECONSTRUCTED
- "F" DENOTES EXISTING STRUCTURES TO BE FILLED
- DENOTES ALLEY RETURN TO BE REMOVED WITH "PAVEMENT REMOVAL" AND REPLACED WITH "PORTLAND CEMENT CONCRETE PAVEMENT, 8 INCH".
- DENOTES "COMBINATION CURB AND GUTTER REMOVAL"
- DENOTES "COMBINATION CURB AND GUTTER, TYPE B-V.12"
- 20' STORM SEWER (WATER MAIN QUALITY) 8 INCH (LENGTH AS NOTED)

BENCHMARK

NW BOLT ON HYDRANT AT 1421 S. CENTRAL AVENUE
ELEV. 608.27

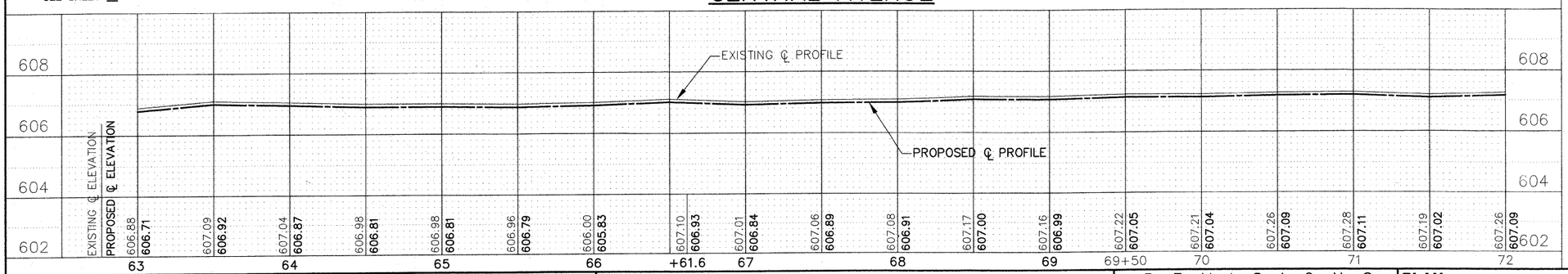
ARROW BOLT ON HYDRANT AT NORTHEAST CORNER OF 14TH STREET AND CENTRAL AVENUE
ELEV. 608.76

NNE BOLT ON HYDRANT AT 1321 S. CENTRAL AVENUE
ELEV. 608.84



FOR CONTINUATION SEE SHEET 11

FOR CONTINUATION SEE SHEET 15



FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	14
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63485

STRUCTURE SCHEDULE

185 MANHOLE ADJUSTMENT (SPECIAL)
NEW TYPE 1 FR. & C.L.
RIM= 606.95
INV(SW)= 599.67
INV(SE)= 600.75

187 PROPOSED CATCH BASIN
TYPE C, 2' DIA.,
TYPE 1 FR. & O.L.
RIM= 606.00
INV= 600.58

188 PROPOSED CATCH BASIN
TYPE C, 2' DIA.,
TYPE 1 FR. & O.L.
RIM= 606.00
INV= 600.40

189 PROPOSED CATCH BASIN
TYPE A, 4' DIA.,
TYPE 1 FR. & O.L.
RIM= 606.00
INV(N)= 600.74
INV(E)= 600.64

190 PROPOSED INLET
TYPE A, 2' DIA.,
TYPE 1 FR. & O.L.
RIM= 601.00

191 MANHOLE RECONSTRUCTION
NEW TYPE 1 FR. & C.L.
RIM= 606.60

192 MANHOLE ADJUSTMENT (SPECIAL)
RIM= 607.15
INV(W)= 599.82
INV(E)= 599.49

193 VALVE VAULT RECONSTRUCTION
NEW TYPE 1 FR. & C.L.
RIM= 606.50

194 PROPOSED CATCH BASIN
TYPE A, 4' DIA.,
TYPE 1 FR. & O.L.
RIM= 605.80
INV(W)= 600.55
INV(N)= 600.65

195 PROPOSED INLET
TYPE A, 2' DIA.,
TYPE 1 FR. & O.L.
RIM= 605.80
INV(S)= 600.90

196 VALVE VAULT RECONSTRUCTION
RIM= 606.65

197 PROPOSED CATCH BASIN
TYPE C, 2' DIA.,
TYPE 1 FR. & O.L.
RIM= 606.10
INV= 601.69

198 PROPOSED CATCH BASIN
TYPE C, 2' DIA.,
TYPE 1 FR. & O.L.
RIM= 606.10
INV= 601.32

199 PROPOSED CATCH BASIN
TYPE C, 2' DIA.,
TYPE 1 FR. & O.L.
RIM= 606.20
INV(E)= 600.95

200 MANHOLE ADJUSTMENT (SPECIAL)
NEW TYPE 1 FR. & C.L.
RIM= 607.05
INV(E)= 600.65
INV(W)= 600.70

201 PROPOSED CATCH BASIN
TYPE C, 2' DIA.,
TYPE 1 FR. & O.L.
RIM= 606.20
INV(W)= 600.88

202 PROPOSED CATCH BASIN
TYPE C, 2' DIA.,
TYPE 1 FR. & O.L.
RIM= 606.10
INV(E)= 600.45

203 PROPOSED CATCH BASIN
TYPE C, 2' DIA.,
TYPE 1 FR. & O.L.
RIM= 606.10
INV(W)= 601.28

NOTE: ALL SCHEDULED RIM ELEVATIONS ARE PROPOSED.

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET 4

IMPORTANT!
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

Civil Engineers
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Illinois Professional Design Firm No. 184-000928

PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS

NO.	BY	DATE	DESCRIPTION
1	AMS	1/22/10	PER IDOT REVIEW

PLAN:
CENTRAL AVENUE-
STA. 63+50 TO STA. 72+00
(RESURFACING)

PROJECT NO. 05043	SCALE 1"=40'
DRAWN/DESIGNED JFP-JEP/THK	DATE DEC., 2009
CHECKED/APPROVED JLC/THK	FIELD BOOK NO. FILE

SHEET 14 OF 51 SHEETS

LEGEND

- DENOTES HOT-MIX ASPHALT SURFACE REMOVAL, (SEE TYPICAL CROSS SECTIONS), TO BE REPLACED WITH "LEVELING BINDER (MACHINE METHOD), N70," 1 INCH AVG., "AREA REFLECTIVE CRACK CONTROL TREATMENT, SYSTEM A" AND "HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70", 2 INCH"
- DENOTES "DRIVEWAY REMOVAL" AND "SIDEWALK REMOVAL" (AS DIRECTED BY THE ENGINEER)
- DENOTES P.C. CONCRETE SIDEWALK, 5 INCH, WITH DETECTABLE WARNING
- DENOTES "P.C. CONCRETE DRIVEWAY PAVEMENT, 7 INCH" AND "P.C. CONCRETE SIDEWALK, 5 INCH" ON SUBBASE GRANULAR MATERIAL, TYPE B, 2 INCH"
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- DENOTES CLASS D PATCHES, TYPE II - IV, 12", - AS DIRECTED
- "A" DENOTES EXISTING VALVE VAULTS, VALVE BOXES, INLETS, CATCH BASINS AND MANHOLES TO BE CLEANED, GROUTED, AND ADJUSTED TO GRADE WITH NEW ADJUSTING RINGS. (ALL TOP BANDS OF FALLEN, COMMON OR CONCRETE BRICK TO BE REPLACED WITH PRECAST CONCRETE ADJUSTING RINGS)
- "S" DENOTES EXISTING FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) (SEE SHEET 41 FOR DETAIL)
- "R" DENOTES EXISTING STRUCTURES TO BE RECONSTRUCTED
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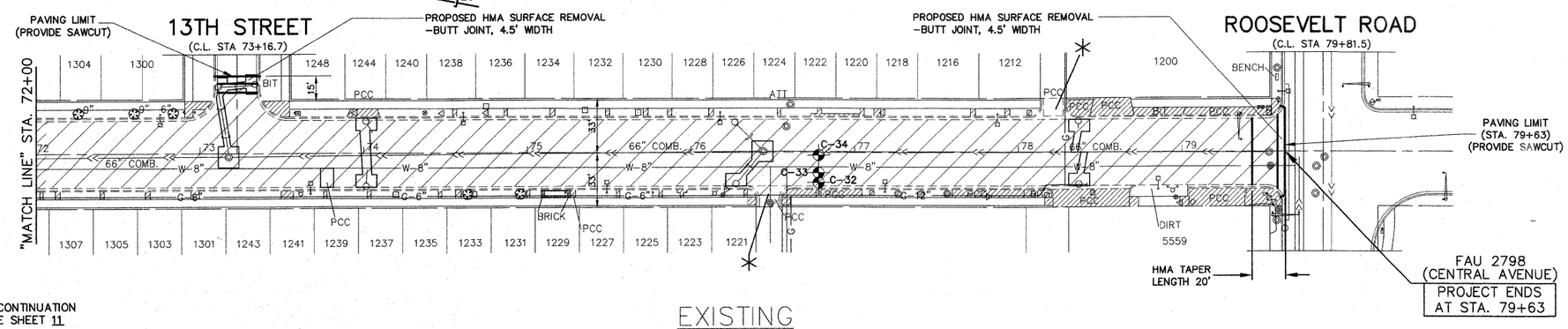
- DENOTES ALLEY RETURN TO BE REMOVED WITH "PAVEMENT REMOVAL" AND REPLACED WITH "PORTLAND CEMENT CONCRETE PAVEMENT, 8 INCH".
- DENOTES "COMBINATION CURB AND GUTTER REMOVAL"
- DENOTES "COMBINATION CURB AND GUTTER, TYPE B-V.12"
- 20' STORM SEWER (WATER MAIN QUALITY) 8 INCH (LENGTH AS NOTED)

BENCHMARK

S BOLT ON HYDRANT AT 1241 S. CENTRAL AVENUE
ELEV. 608.86

ARROW BOLT ON HYDRANT AT 1221 S. CENTRAL AVENUE
ELEV. 608.61

NW BOLT ON HYDRANT AT 5559 W. ROOSEVELT ROAD (HYDRANT ON CENTRAL AVENUE)
ELEV. 609.72



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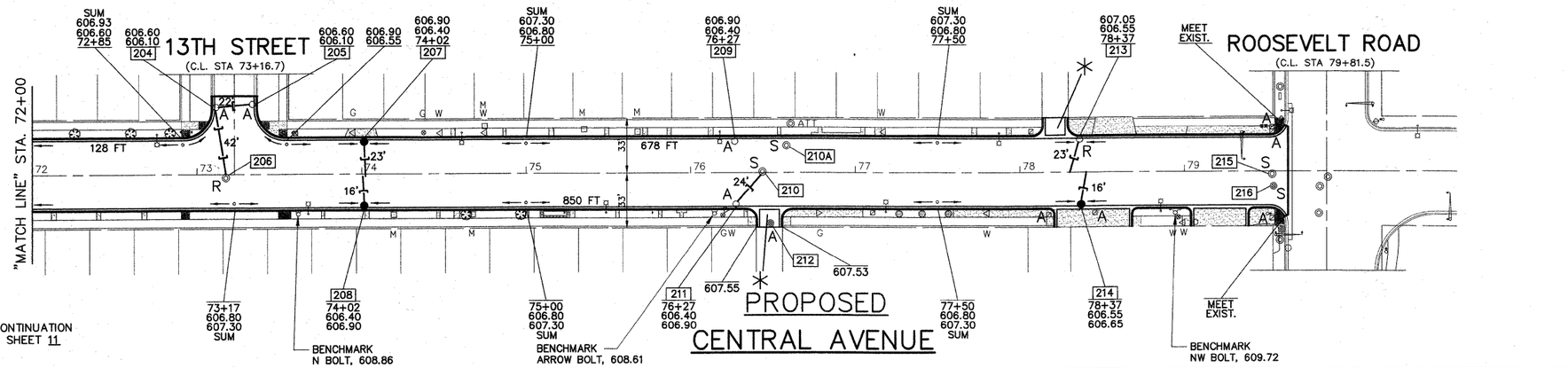

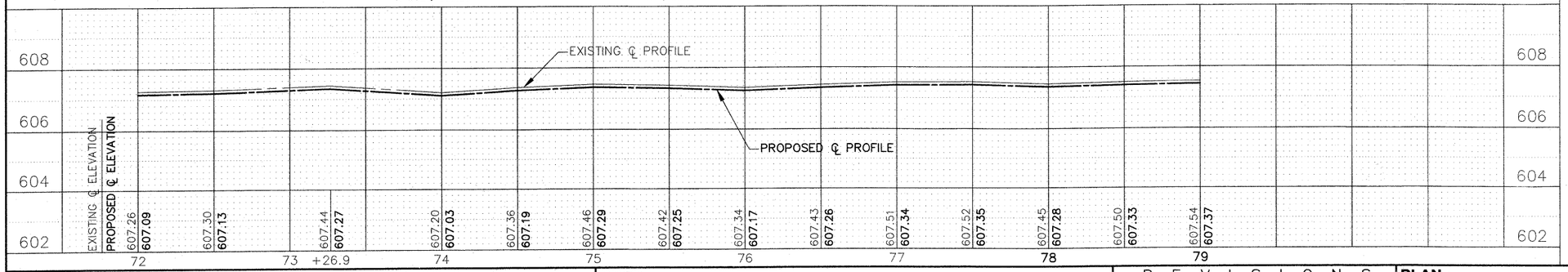

STRUCTURE SCHEDULE

- 204 CATCH BASIN ADJUSTMENT RIM= 606.10 INV(E)= 600.65 INV(N)= 600.75
- 205 CATCH BASIN ADJUSTMENT TYPE 1 FR. & O.L. RIM= 606.10 INV(S)= 601.05
- 206 MANHOLE RECONSTRUCTION RIM= 607.60 INV(W)= 599.31
- 207 PROPOSED CATCH BASIN TYPE C, 2' DIA., TYPE 1 FR. & O.L. RIM= 606.40 INV(E)= 603.51
- 208 PROPOSED INLET TYPE A, 2' DIA., TYPE 1 FR. & O.L. RIM= 606.40 INV(W)= 600.98
- 209 CATCH BASIN ADJUSTMENT RIM= 606.40
- 210 MANHOLE ADJUSTMENT (SPECIAL) RIM= 607.20 INV(SE)= 600.82
- 210A MANHOLE ADJUSTMENT (SPECIAL) RIM= 606.80
- 211 PROPOSED CATCH BASIN TYPE C, 2' DIA., TYPE 1 FR. & O.L. RIM= 606.40 INV(W)= 601.24
- 212 VALVE VAULT ADJUSTMENT RIM= 607.72
- 213 CATCH BASIN RECONSTRUCTION RIM= 606.55 INV(E)= 603.33
- 214 PROPOSED CATCH BASIN TYPE C, 2' DIA., TYPE 1 FR. & O.L. RIM= 606.55 INV(W)= 601.76
- 215 MANHOLE ADJUSTMENT (SPECIAL) RIM= 607.60
- 216 VALVE VAULT ADJUSTMENT (SPECIAL) RIM= 607.50

NOTE: ALL SCHEDULED RIM ELEVATIONS ARE PROPOSED.

FOR TYPICAL CROSS SECTION OF NEW PAVEMENT WORK SEE SHEET 4

IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	15
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63485
4

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Illinois Professional Design Firm No. 184-000928

PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS

NO.	BY	DATE	DESCRIPTION
1	AMS	1/22/10	PER IDOT REVIEW

PLAN:
CENTRAL AVENUE-
STA. 72+00 TO ROOSEVELT RD.
(RESURFACING)

PROJECT NO. 05043	SCALE 1"=40'
DRAWN/DESIGNED JFP-JEP/THK	DATE DEC., 2009
CHECKED/APPROVED JLC/THK	FIELD BOOK NO. FILE

SHEET 15 OF 51 SHEETS

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	16
F.H.W.A. REG.	ILLINOIS	PROJECT M-9003(488)		

CONTRACT NO. 63455

THERMOPLASTIC STRIPING CODE

- (A) PROPOSED DOUBLE SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING – CENTER LINE 4", 11' O/C
- (B) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING – LINE 4" – (PARKING LINE)
- (C) PROPOSED SKIP–DASH YELLOW THERMOPLASTIC PAVEMENT MARKING – LINE 4" – 10' DASH, 30' SKIP (LANE LINE)
- (D) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING – LINE 6" (LANE LINE)
- (E) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING – LINE 6" (CROSSWALK LINE)
- (F) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING – LINE 6" – 2' DASH, 6' SKIP (LANE LINE)
- (G) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING – LINE 12" (PEDESTRIAN SCHOOL CROSSING)
- (H) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING – LINE 24" (STOP BAR)
- (I) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING – LETTERS & SYMBOLS

DETECTOR LOOPS

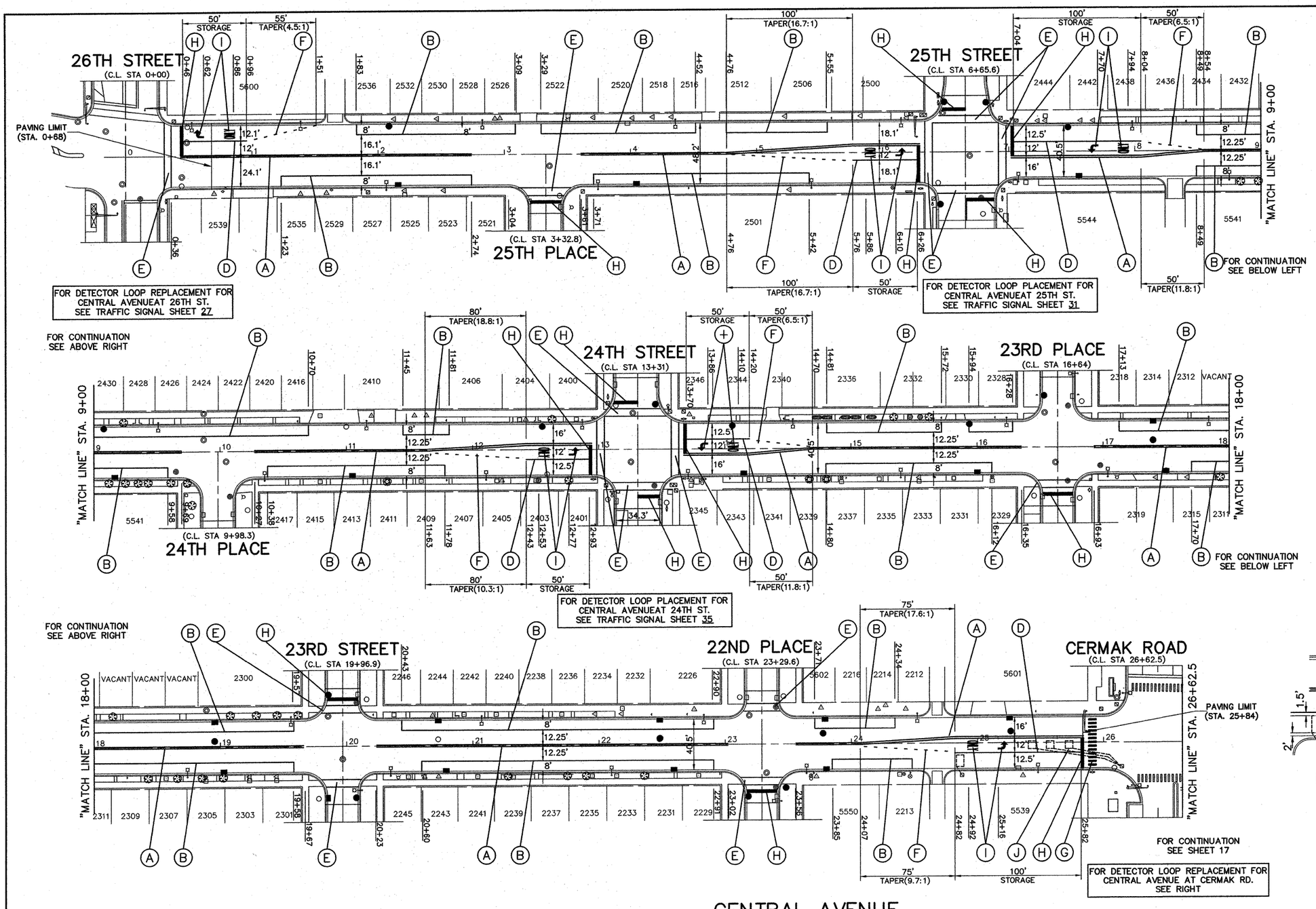
- (J) PROPOSED DETECTOR LOOP REPLACEMENT

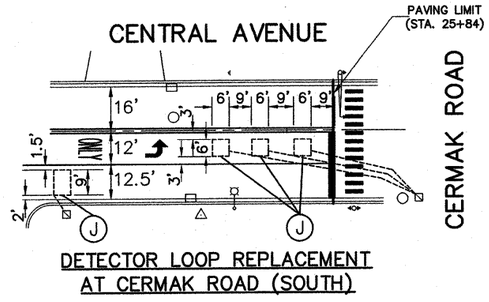
NOTE:
ALL "ARROW" AND "ONLY" MARKINGS SHALL BE 8' IN HEIGHT.

NOTE:
PROPOSED DOUBLE SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING – CENTER LINE 4", 11' O/C (NOTE: (A)) IS MEASURED PER LINE. TOTAL QUANTITY AND PAYMENT LENGTH IS FOR EACH LINEAR FOOT OF SINGLE 4" STRIPE INSTALLED.

NOTE:
SHORT–TERM PAVEMENT MARKING IS PROPOSED ON THE MILLED PAVEMENT, NEW BINDER AND ON THE NEW SURFACE. SEE ARTICLE 703.04 OF THE LATEST EDITION OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION".

NOTE:
PROPOSED STRIPING IS BEING REPLACED IN THE SAME LOCATION AS EXISTING STRIPING.





DETECTOR LOOP REPLACEMENT AT CERMAK ROAD (SOUTH)

IMPORTANT!
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

CENTRAL AVENUE

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PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS

NO.	BY	DATE	DESCRIPTION
1	AMS	1/22/10	PER IDOT REVIEW
2	AMS	3/03/10	PER IDOT REVIEW

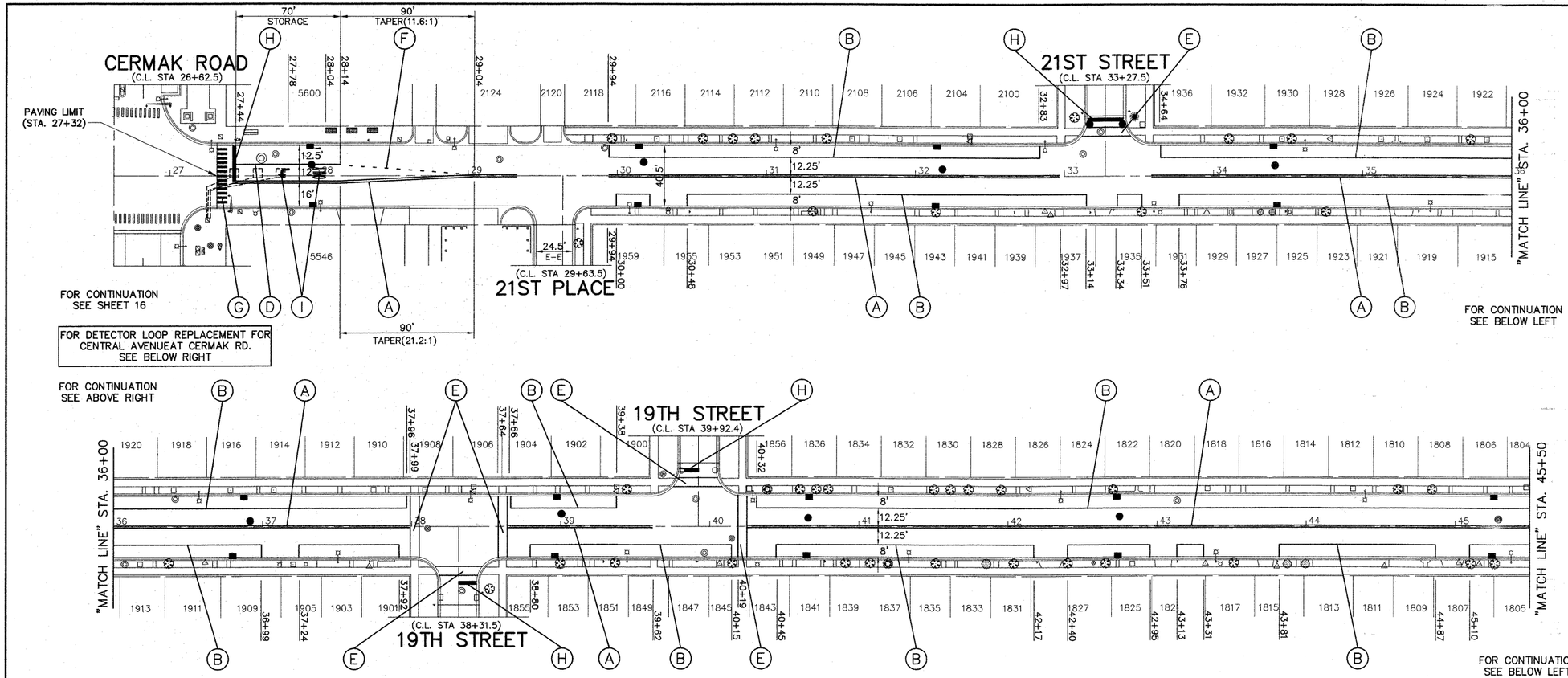
PLAN:
CENTRAL AVENUE-
26th ST. TO CERMAK RD.
(PAVEMENT MARKING)

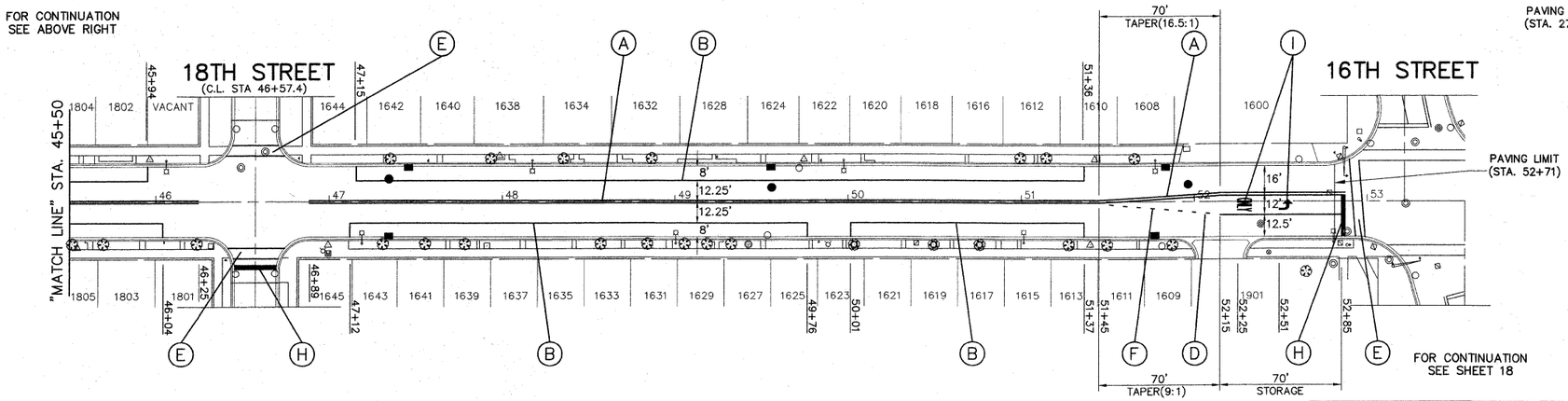
PROJECT NO.	SCALE
05043	1"=40'
DRAWN/DESIGNED	DATE
JFP–JEP/THK	DEC., 2009
CHECKED/APPROVED	FIELD BOOK NO.
JLC/THK	FILE

SHEET **16** OF **51** SHEETS

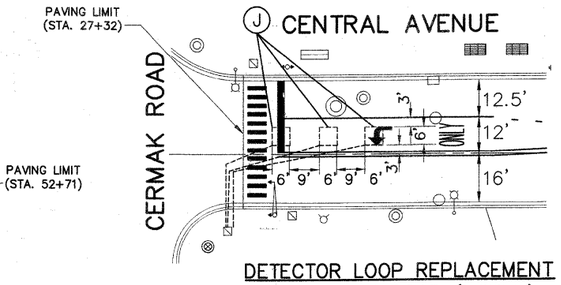
FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	17
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63455
4





CENTRAL AVENUE



DETECTOR LOOP REPLACEMENT AT CERMAK ROAD (NORTH)

THERMOPLASTIC STRIPING CODE

- (A) PROPOSED DOUBLE SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING – CENTER LINE 4", 11' O/C
- (B) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING – LINE 4" – (PARKING LINE)
- (C) PROPOSED SKIP-DASH YELLOW THERMOPLASTIC PAVEMENT MARKING – LINE 4" – 10' DASH, 30' SKIP (LANE LINE)
- (D) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING – LINE 6" (LANE LINE)
- (E) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING – LINE 6" (CROSSWALK LINE)
- (F) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING – LINE 6" – 2' DASH, 6' SKIP (LANE LINE)
- (G) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING – LINE 12" (PEDESTRIAN SCHOOL CROSSING)
- (H) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING – LINE 24" (STOP BAR)
- (I) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING – LETTERS & SYMBOLS

DETECTOR LOOPS

- (J) PROPOSED DETECTOR LOOP REPLACEMENT

NOTE:
ALL "ARROW" AND "ONLY" MARKINGS SHALL BE 8' IN HEIGHT.

NOTE:
PROPOSED DOUBLE SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING – CENTER LINE 4", 11' O/C (NOTE: (A)) IS MEASURED PER LINE. TOTAL QUANTITY AND PAYMENT LENGTH IS FOR EACH LINEAR FOOT OF SINGLE 4" STRIPE INSTALLED.

NOTE:
SHORT-TERM PAVEMENT MARKING IS PROPOSED ON THE MILLED PAVEMENT, NEW BINDER AND ON THE NEW SURFACE. SEE ARTICLE 703.04 OF THE LATEST EDITION OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION".

NOTE:
PROPOSED STRIPING IS BEING REPLACED IN THE SAME LOCATION AS EXISTING STRIPING.

IMPORTANT!
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

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PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

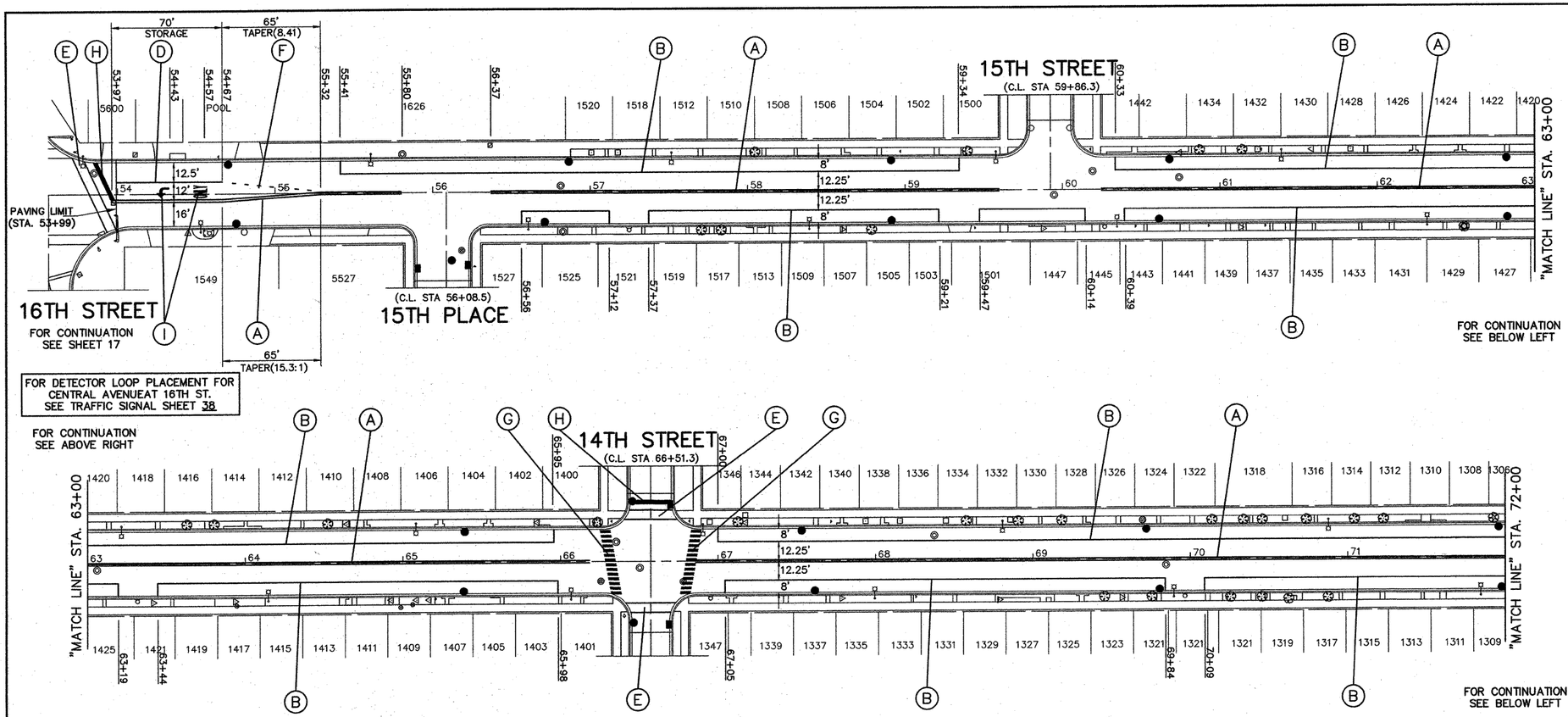
REVISIONS			
NO.	BY	DATE	DESCRIPTION
1	AMS	1/22/10	PER IDOT REVIEW

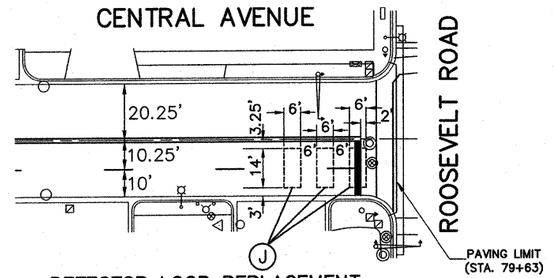
PLAN:
CENTRAL AVENUE-
CERMAK RD. TO 16th ST.

(PAVEMENT MARKING)

PROJECT NO. 05043	SCALE 1"=40'
DRAWN/DESIGNED JFP-JEP/THK	DATE DEC., 2009
CHECKED/APPROVED JLC/THK	FIELD BOOK NO. FILE

SHEET 17 OF 51 SHEETS





DETECTOR LOOP REPLACEMENT AT ROOSEVELT ROAD

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	18
F.H.W.A. REG.	ILLINOIS	PROJECT M-9003(488)		

CONTRACT NO. 63455

THERMOPLASTIC STRIPING CODE

- (A) PROPOSED DOUBLE SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING – CENTER LINE 4", 11' O/C
- (B) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING – LINE 4" – (PARKING LINE)
- (C) PROPOSED SKIP-DASH YELLOW THERMOPLASTIC PAVEMENT MARKING – LINE 4" – 10' DASH, 30' SKIP (LANE LINE)
- (D) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING – LINE 6" (LANE LINE)
- (E) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING – LINE 6" (CROSSWALK LINE)
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- (H) PROPOSED SOLID WHITE THERMOPLASTIC PAVEMENT MARKING – LINE 24" (STOP BAR)
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DETECTOR LOOPS

- (J) PROPOSED DETECTOR LOOP REPLACEMENT

NOTE:
ALL "ARROW" AND "ONLY" MARKINGS SHALL BE 8' IN HEIGHT.

NOTE:
PROPOSED DOUBLE SOLID YELLOW THERMOPLASTIC PAVEMENT MARKING – CENTER LINE 4", 11' O/C (NOTE: (A)) IS MEASURED PER LINE. TOTAL QUANTITY AND PAYMENT LENGTH IS FOR EACH LINEAR FOOT OF SINGLE 4" STRIPE INSTALLED.

NOTE:
SHORT-TERM PAVEMENT MARKING IS PROPOSED ON THE MILLED PAVEMENT, NEW BINDER AND ON THE NEW SURFACE. SEE ARTICLE 703.04 OF THE LATEST EDITION OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION".

NOTE:
PROPOSED STRIPING IS BEING REPLACED IN THE SAME LOCATION AS EXISTING STRIPING.

IMPORTANT!
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

CENTRAL AVENUE

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PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS

NO.	BY	DATE	DESCRIPTION
1	AMS	1/22/10	PER IDOT REVIEW
2	AMS	3/03/10	PER IDOT REVIEW

PLAN:
CENTRAL AVENUE-
16th ST. TO ROOSEVELT RD.
(PAVEMENT MARKING)

PROJECT NO. 05043	SCALE 1"=40'
DRAWN/DESIGNED JFP-JEP/THK	DATE DEC., 2009
CHECKED/APPROVED JLC/THK	FIELD BOOK NO. FILE

SHEET 18 OF 51 SHEETS

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	19
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63445

STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM SEWER WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE UNDER NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE CONSTRUCTION SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIME FRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN IN STANDARD 280001 OF THE PLANS.

SECTION 280. TEMPORARY EROSION CONTROL, OF THE STANDARD SPECIFICATIONS ADDITIONALLY SUPPLEMENTS THIS PLAN.

SITE DESCRIPTION.

DESCRIPTION OF CONSTRUCTION ACTIVITY:

1. THE PROJECT IS LOCATED ON CENTRAL AVENUE IN CICERO, ILLINOIS, FROM 26TH STREET TO ROOSEVELT ROAD.

2. CONSTRUCTION INCLUDES EARTH EXCAVATION, STORM SEWERS, MANHOLES, CATCH BASINS, INLETS, VARIOUS PAVEMENT ITEMS, TRAFFIC SIGNALS AND OTHER MISCELLANEOUS ITEMS OF CONSTRICTION.

DESCRIPTION OF INTENDED SEQUENCE FOR MAJOR CONSTRUCTION ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTION OF THE CONSTRUCTION SITE:

1. EXCAVATION WILL BE COMPLETED ALONG THE JOB SITE TO GRADE OUT FOR THE PROPOSED ROADWAY WIDENING AND PARKWAY RESTORATION..
2. STORM SEWERS, MANHOLES, CATCH BASINS, AND INLETS.
3. PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL, SUCH AS INLET AND PIPE PROTECTION, TEMPORARY SEEDING, ETC.
4. PAVEMENT WIDENING AND RESURFACING WORK.
5. FINAL GRADING, PAVING, AND OTHER MISCELLANEOUS ITEMS.
6. PLACEMENT OF PERMANENT EROSION CONTROL, AND EROSION CONTROL BLANKET, SODDING, ETC.

AREA OF CONSTRUCTION SITE:

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 2.5 ACRES BY WHICH 2.5 ACRES WILL BE DISTURBED BY EXCAVATION, GRADING, AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS, WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILITIES FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.

2. PROJECT PLAN DOCUMENTS, SPECIFICATIONS AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

1. STORM SEWER OUTLETS TRIBUTARY TO THE CITYS EXISTING COMBINED SEWER SYSTEM.

CONTROLS, EROSION CONTROLS AND SEDIMENT CONTROL:

1. THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE TEMPORARY SEEDING, PERMANENT SEEDING, MULCHING, PROTECTION OF TREES, PRESERVATION OF NATURE VEGETATION, AND OTHER APPROPRIATE MEASURES AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

(a.) AREAS OF EXISTING VEGETATION, WOOD AND GRASSLANDS, OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE IDENTIFIED BY THE ENGINEER FOR PRESERVING AND SHALL BE PROTECTED FROM CONSTRUCTION ACTIVITIES.

(b.) DEAD, DISEASED, OR UNSUITABLE VEGETATION WITHIN THE SITE SHALL BE REMOVED AS DIRECTED BY THE ENGINEER, ALONG WITH REQUIRED TREE REMOVAL.

(c.) AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, TEMPORARY DITCH CHECKS, INLET AND PIPE PROTECTION, AND PERIMETER EROSION BARRIER SHALL BE INSTALLED AS CALLED OUT IN THIS PLAN AND DIRECTED BY THE ENGINEER.

(d.) BARE AND SPARSELY VEGETATED GROUND IN HIGH ERODABLE AREAS AS DETERMINED BY THE ENGINEER SHALL BE TEMPORARILY SEEDED AT THE BEGINNING OF CONSTRUCTION WHERE NO CONSTRUCTION ACTIVITIES ARE EXPECTED WITHIN SEVEN (7) DAYS.

(e.) IMMEDIATELY AFTER TREE REMOVAL IS COMPLETED, AREAS WHICH ARE HIGHLY ERODABLE AS DETERMINED BY THE ENGINEER, SHALL BE TEMPORARILY SEEDED WHEN NO CONSTRUCTION ACTIVITIES ARE EXPECTED WITHIN SEVEN (7) DAYS.

(f.) AT LOCATIONS WHERE A SIGNIFICANT AMOUNT OF WATER DRAINS INTO THE CONSTRUCTION ZONE FROM OUTSIDE AREAS ON ADJACENT LANDOWNERS, TEMPORARY DITCH CHECKS WILL BE UTILIZED TO LOCALLY DIVERT WATER, REDUCE FLOW RATES, AND COLLECT OUTSIDE SILTATION INSIDE THE RIGHT-OF-WAY LINE.

2. ESTABLISHMENT OF THESE TEMPORARY EROSION CONTROL MEASURES WILL HAVE ADDITIONAL BENEFITS TO THE PROJECT. DESIRABLE GRASS SEED WILL BECOME ESTABLISHED IN THESE AREAS AND WILL SPREAD SEEDS ONTO THE CONSTRUCTION SITE UNTIL PERMANENT SEEDING/MOWING AND OVERSEEDING CAN BE COMPLETED.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10 ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

I CERTIFY UNDER PENALTY OF LAW THAT THIS DOCUMENT AND ALL ATTACHMENTS WERE PREPARED UNDER MY DIRECTION OR SUPERVISION IN ACCORDANCE WITH A SYSTEM DESIGNED TO ASSURE THAT QUALIFIED PERSONNEL PROPERLY GATHERED AND EVALUATED THE INFORMATION SUBMITTED. BASED ON MY INQUIRY OF THE PERSON OR PERSONS WHO MANAGE THE SYSTEM, OR THOSE PERSONS DIRECLTY RESPONSIBLE FOR GATHERING THE INFOMRATION, THE INFOMRATION SUBMITTED IS TO THE BEST OF MY KNOWLEDGE AND BELIEF, TRUE, ACCURATE AND COMPLETE. I AM AWARE THAT THERE ARE SIGNIFICAN PENALTIES FOR SUBMITTING FALSE INFOMRATION, INLCUDING THE POSSIBILITY OF FINE AND IMPRISONMENT FOR KNOWING VIOLATIONS.

Frank Novotny 1-22-10

ENGINEER DATE

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

1. DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING, EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER, PARKING OF VEHICLES OF CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS OR OTHER CONSTRUCTION RELATED ACTIVITIES.

(a.) WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.

(b.) EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN (14) DAYS.

(c.) AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER.

i. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.

ii. TEMPORARILY SEED ERODABLE BARE EARTH ON A WEEKLY BASIS TO MINIMIZE THE AMOUNT OF ERODABLE SURFACE AREA WITHIN THE CONTRACT LIMITS.

iii. CONSTRUCT ROADSIDE DITCHES AND PROVIDE TEMPORARY EROSION CONTROL SYSTEMS.

iv. TEMPORARILY DIVERT WATER AROUND PROPOSED CULVERT LOCATIONS.

v. BUILD NECESSARY EMBANKMENT AT CULVERT LOCATIONS AND THEN EXCAVATE AND PLACE CULVERT.

vi. CONTINUE BUILDING UP THE EMBANKMENT TO THE PROPOSED GRADE WHILE AT THE SAME TIME, PLACING PERMANENT EROSION CONTROL SUCH AS RIPRAP DITCH LINING AND CONDUCTING FINAL SHAPING TO THE SLOPES.

(b.) EXCAVATED AREAS AND EMBANKMENT SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN (7) DAYS.

(c.) CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR OTHER POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

(f.) THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT DAILY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2-INCH OR GREATER OR EQUIVALENT SNOWFALL AND DURING THE WINTER SHUTDOWN PERIOD. THE PROJECT SHALL ADDITIONALLY BE INSPECTED BY THE CONSTRUCTION FIELD ENGINEER ON A BI-WEEKLY BASIS TO DETERMINE THAT EROSION CONTROL EFFORTS ARE IN PLACE AND EFFECTIVE AND IF OTHER EROSION CONTROL WORK IS NECESSARY.

(g.) SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON THE SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR EARTH EXCAVATION FOR EROSION CONTROL.

(h.) THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

1. TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS SEEDED AND ESTABLISHED.

2. ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

MAINTENANCE AFTER CONSTRUCTION:

CONSTRUCTION IS COMPLETE AFTER ACCEPTANCE BY THE MUNICIPALITY. MAINTENANCE UP TO THIS DATE WILL BE BY THE CONTRACTOR.

MISCELLANEOUS:

1. TEMPORARY DITCH CHECKS SHALL BE LOCATED AT EVERY 1.5-FEET FALL/RISE IN DITCH GRADE.
2. TEMPORARY EROSION CONTROL SEEDING SHALL BE APPLIED AT A RATE OF 100 LBS/ACRES, IF DIRECTED.
3. STRAW BALES, HAY BALES, PERIMETER EROSION BARRIER AND SILT FENCES WILL NOT BE PERMITTED FOR TEMPORARY OR PERMANENT DITCH CHECKS. DITCH CHECKS SHALL BE COMPOSED OF AGGREGATE, SILT PANELS, ROLLED EXCELSIOR, URETHANE FORM/GEOTEXTILE SILT WEDGES, AND/OR ANY OTHER MATERIAL APPROVED BY THE EROSION AND SEDIMENT CONTROL COORDINATOR.
4. SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON THE SITE ON A REGULAR BASIS, AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC YARD FOR EARTH EXCAVATION FOR EROSION CONTROL.
5. ALL EROSION CONTROL PRODUCTS FURNISHED SHALL BE SPECIFICALLY RECOMMENDED BY THE MANUFACTURER FOR THE USE SPECIFIED IN THE EROSION CONTROL PLAN. PRIOR TO THE APPROVAL AND USE OF THE PROJECT, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER A NOTARIZED CERTIFICATION BY THE PRODUCER STATING THE INTENDED USE OF THE PRODUCT AND THAT THE PHYSICAL PROPERTIES REQUIRED FOR THIS APPLICATION ARE MET OR EXCEEDED. THE CONTRACTOR SHALL PROVIDE MANUFACTURER INSTALLATION PROCEDURES TO FACILITATE THE ENGINEER IN CONSTRUCTION INSPECTION.

LEGEND

- TEMPORARY DITCH CHECK
- EROSION CONTROL BLANKET
- PERIMETER EROSION BARRIER - SILT FILTER FENCE OR OTHER AS APPROVED BY THE ENGINEER
- INLET AND PIPE PROTECTION
- SEDIMENT BASIN

NOTE: ALL ITEMS SHALL BE CONSTRUCTED AS SHOWN ON STANDARD 280001 AND AS DIRECTED BY THE ENGINEER. MAINTENANCE AND CLEANING OF THE EROSION CONTROL ITEMS SHALL BE INCLUDED IN THE RESPECTIVE EROSION CONTROL PAY ITEM.

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PROJECT

TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS

NO.	BY	DATE	DESCRIPTION

STORM WATER POLLUTION PREVENTION PLAN

PROJECT NO. 05043	SCALE NONE
DRAWN/DESIGNED JFP-JEP/THK	DATE DEC., 2009
CHECKED/APPROVED JLC/THK	FIELD BOOK NO. FILE

SHEET 19 OF 51 SHEETS

LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.

2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.

3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.

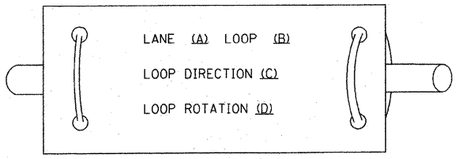
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.

5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.

6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.

7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

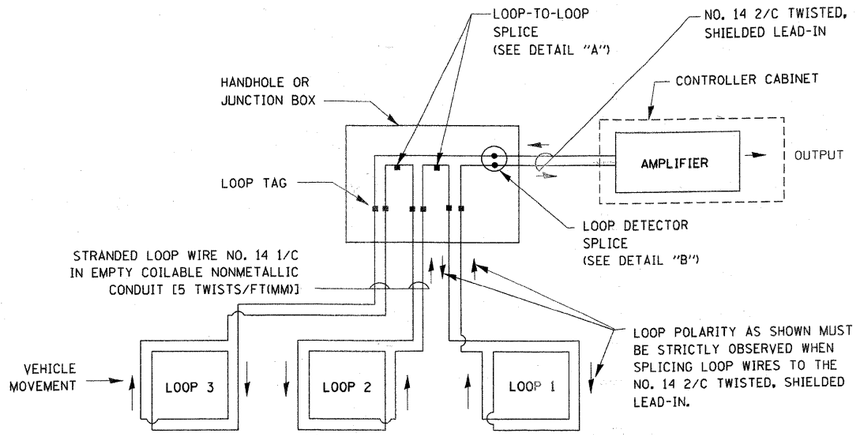


A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY

B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.

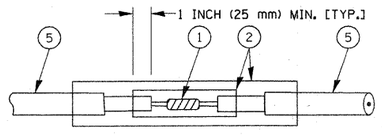
C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".

D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

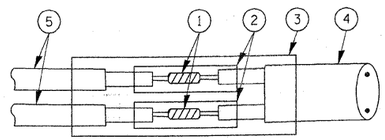


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

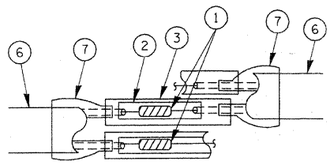


DETAIL "A"
LOOP-TO-LOOP SPLICE



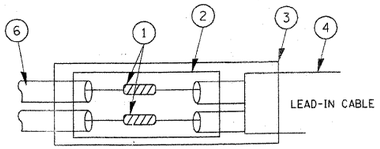
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE

PRE-FORMED LOOP



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

(1) WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.

(2) WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.

(3) WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.

(4) NO. 14 2/C TWISTED, SHIELDED CABLE.

(5) LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

(6) PRE-FORMED LOOP

(7) XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

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	PLOT SCALE = 20.0000 ' / IN.	CHECKED - DAD	REVISED -
	PLOT DATE = 10/6/2009	DATE - 10/28/09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

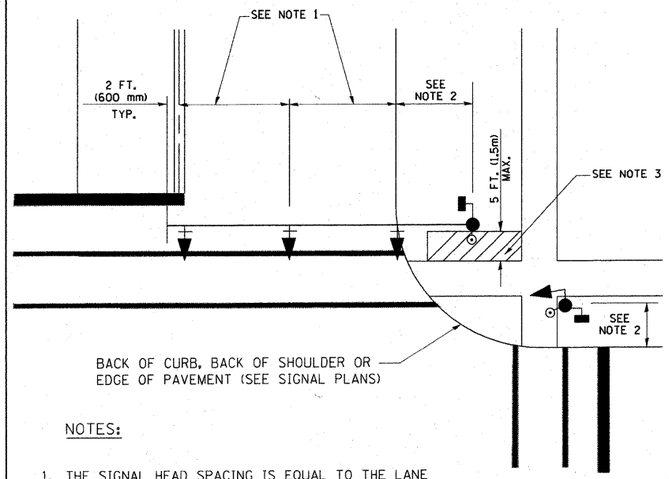
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: | SHEET NO. 1 OF 6 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	20
		CONTRACT NO. 63445		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	M-9003(488)	

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

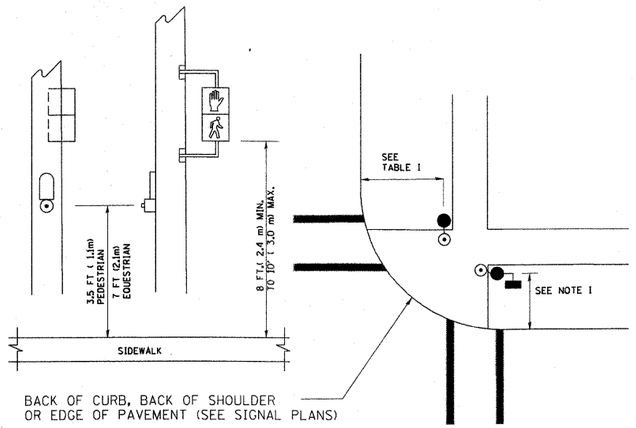
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

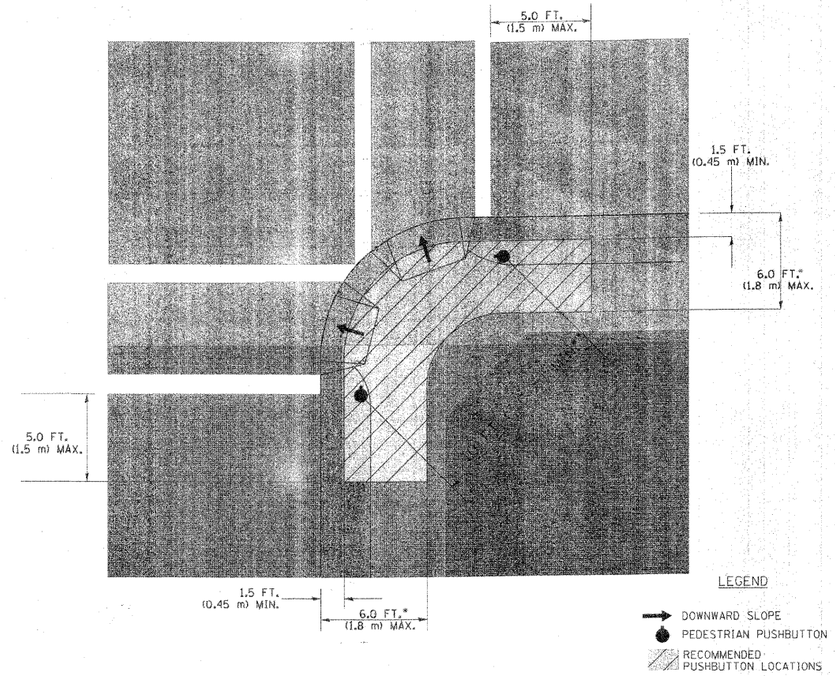
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.

** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

CONTRACT NO. 63445

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: | SHEET NO. 2 OF 6 SHEETS | STA. | TO STA.

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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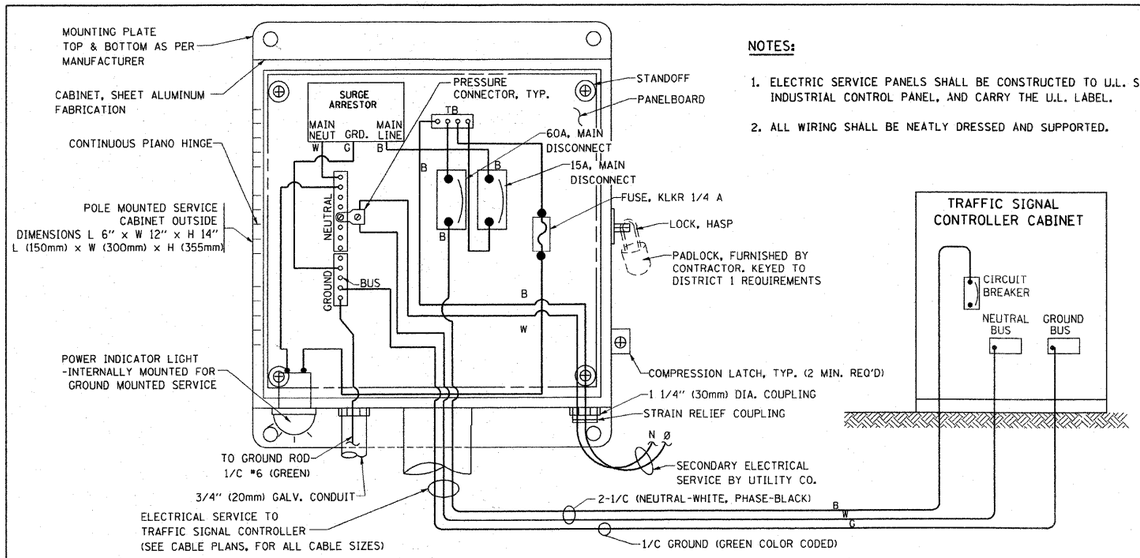
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	



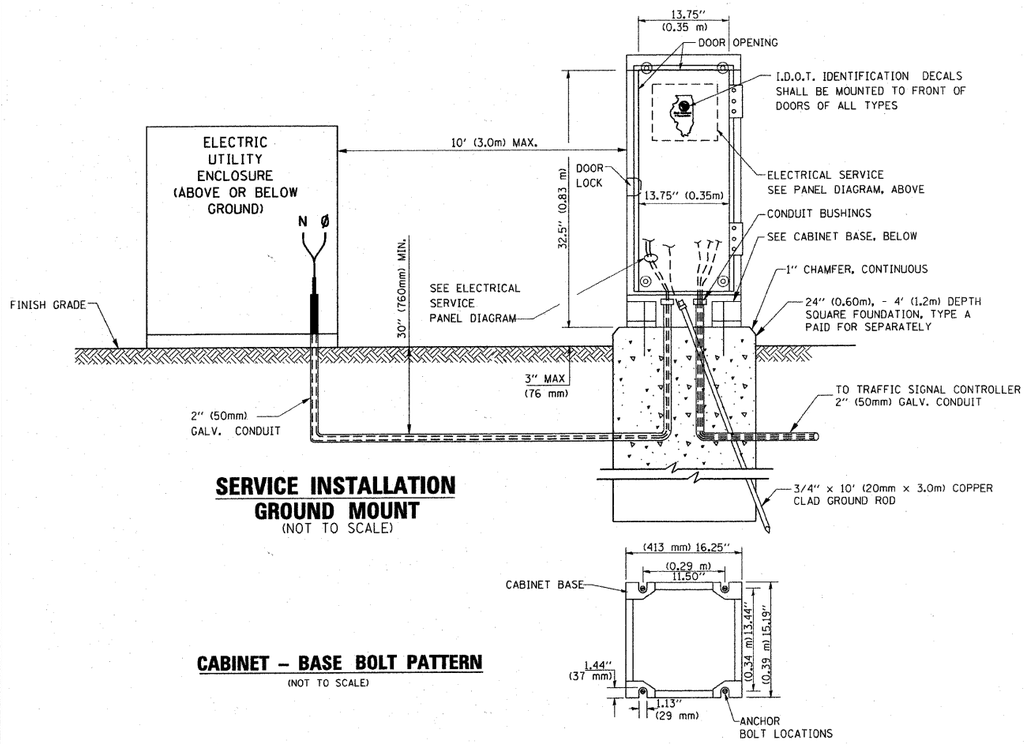
NOTES:

1. ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508, INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL.
2. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.

ELECTRICAL SERVICE – PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)

SERVICE INSTALLATION POLE MOUNT (SHOWN)

(NOT TO SCALE)

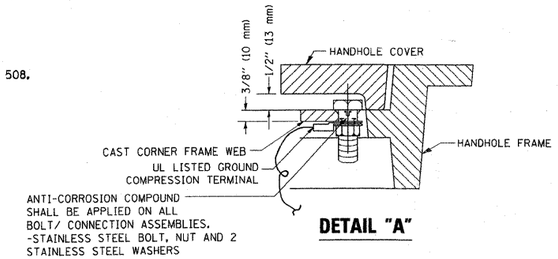


SERVICE INSTALLATION GROUND MOUNT

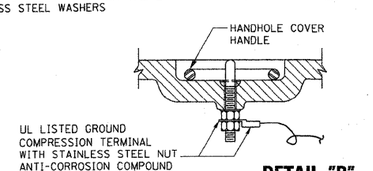
(NOT TO SCALE)

CABINET – BASE BOLT PATTERN

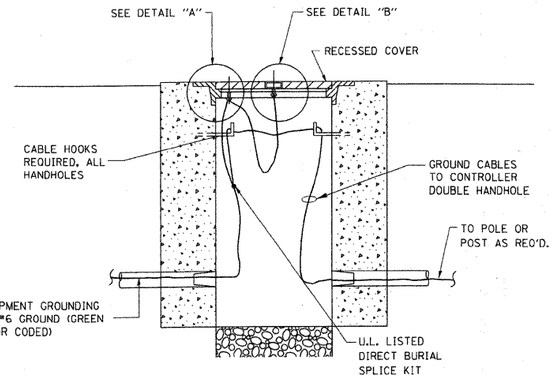
(NOT TO SCALE)



DETAIL "A"

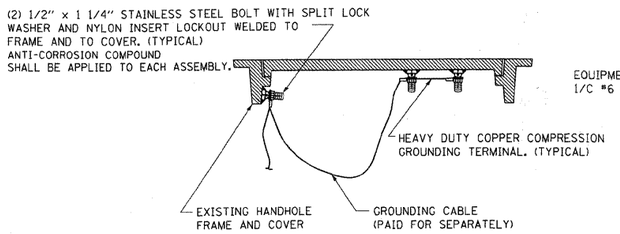


DETAIL "B"



HANDHOLE COVER & FRAME – GROUNDING DETAIL

(NOT TO SCALE)



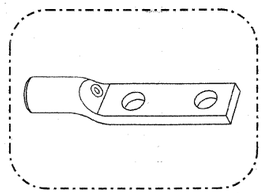
EXISTING HANDHOLE COVER & FRAME – GROUNDING DETAIL

(NOT TO SCALE)

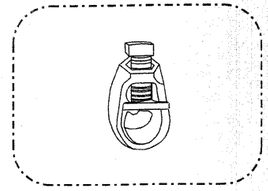
NOTES:

GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



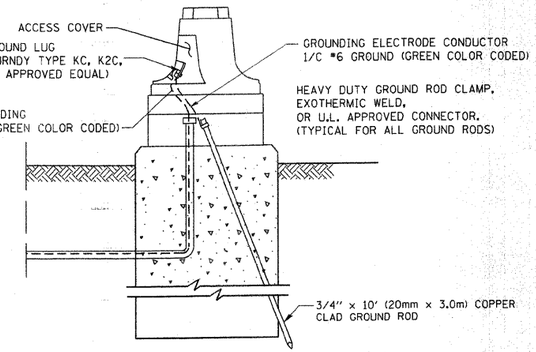
HEAVY-DUTY COMPRESSION TERMINAL (BURNDY TYPE YGHA OR APPROVED EQUAL)



3/4" (20mm) HEAVY-DUTY GROUND ROD CLAMP (BURNDY TYPE GRC OR APPROVED EQUAL)

NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES
  6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES
  13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES.
  5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



MAST ARM POLE / POST–GROUNDING DETAIL

(NOT TO SCALE)

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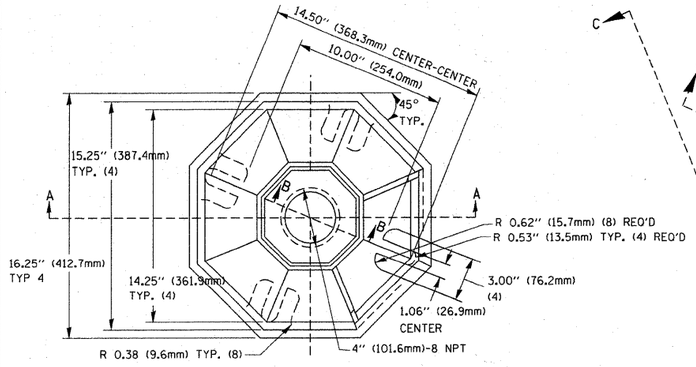
	PLOT DATE = 10/6/2009	DATE - 10/28/09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

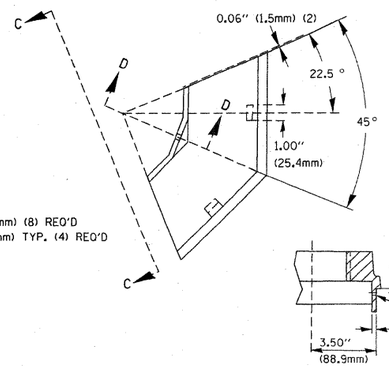
DISTRICT 1
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: | SHEET NO. 3 OF 6 SHEETS | STA. | TO STA.

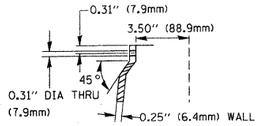
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	22
		CONTRACT NO. 62445		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	M-9003(488)	



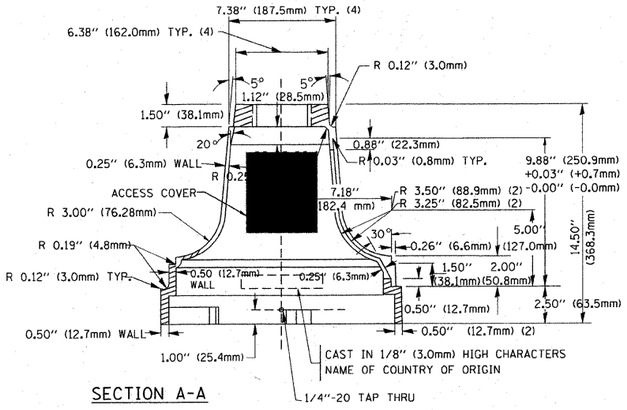
TOP VIEW



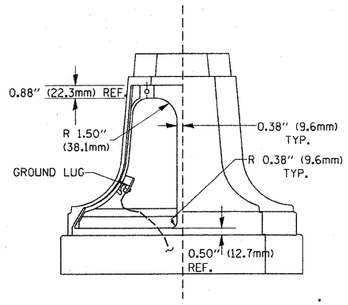
SECTION B-B



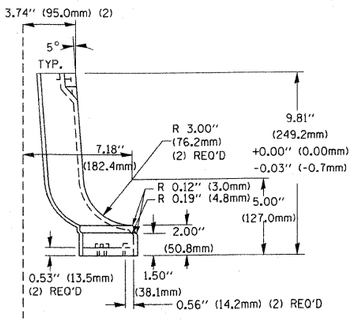
SECTION D-D



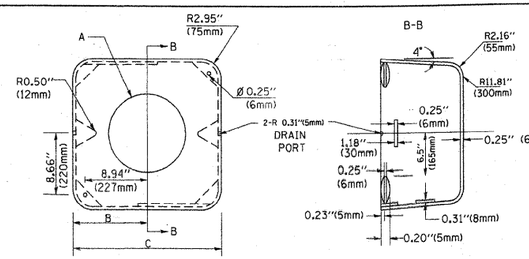
SECTION A-A



VIEW C-C



TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A



MATERIAL:
- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

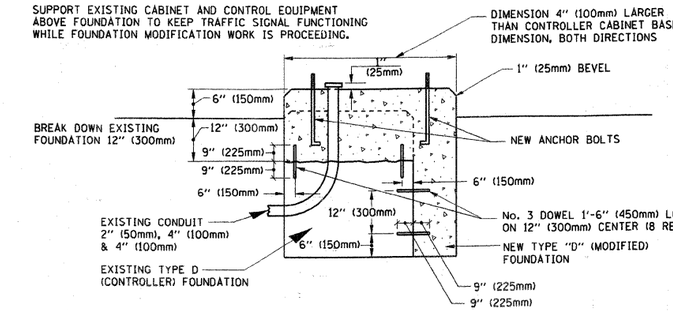
A	B	C	HEIGHT	WEIGHT
VARIES	9.5"(241mm)	19"(483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75"(273mm)	21.5"(546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0"(330mm)	26"(660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18.5"(470mm)	37"(940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

SHROUD

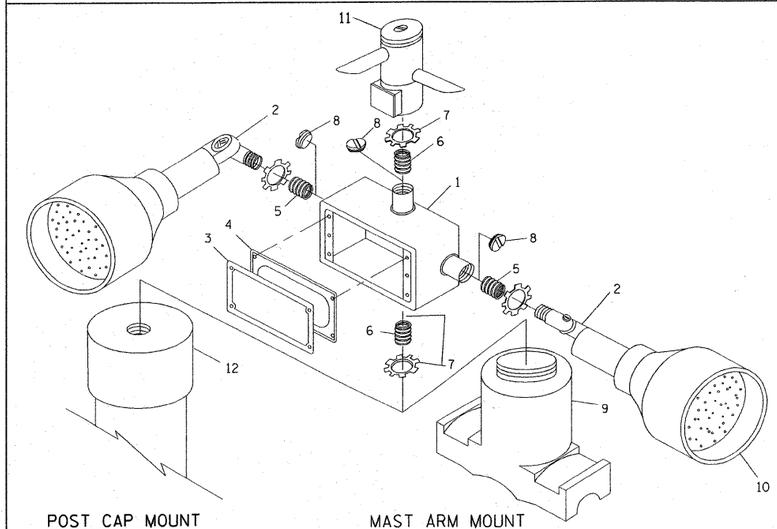
NOTES:

1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:
SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



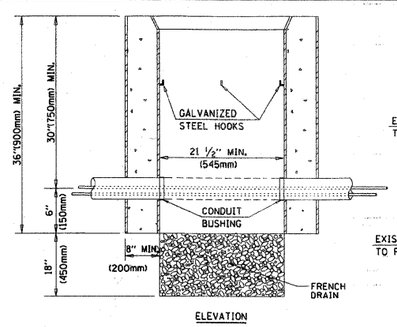
POST CAP MOUNT MAST ARM MOUNT

EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL

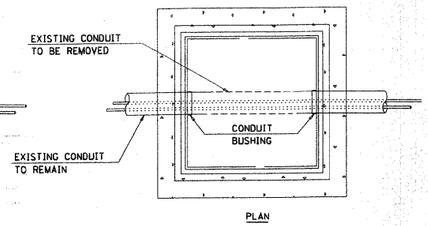
ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	¾"(19 mm) CLOSE NIPPLE
7	¾"(19 mm) LOCKNUT
8	¾"(19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP (18 FT. (5.4 m) POST MIN.)

NOTES:

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
   ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
   ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A ¾"(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



ELEVATION



PLAN

NOTES:

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCIDENTAL TO THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

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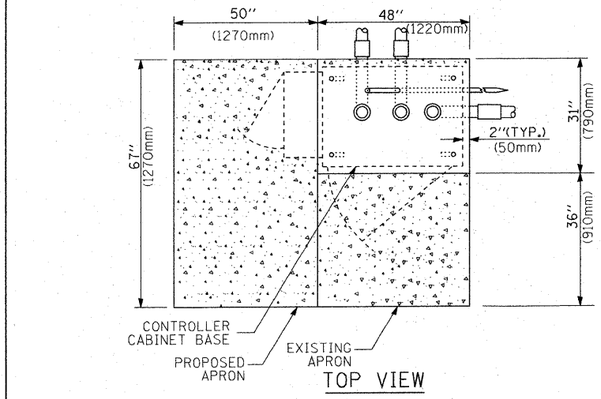
		DRAWN - BCK	REVISED -
	PLOT SCALE = 20.0000 ' / IN.	CHECKED - DAD	REVISED -
	PLOT DATE = 10/6/2009	DATE - 10/28/09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

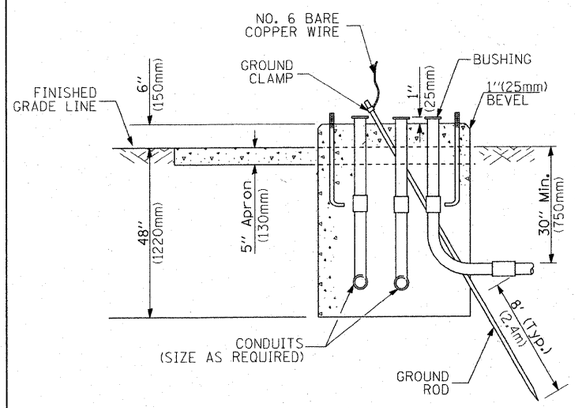
DISTRICT 1
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: | SHEET NO. 4 OF 6 SHEETS | STA. | TO STA.

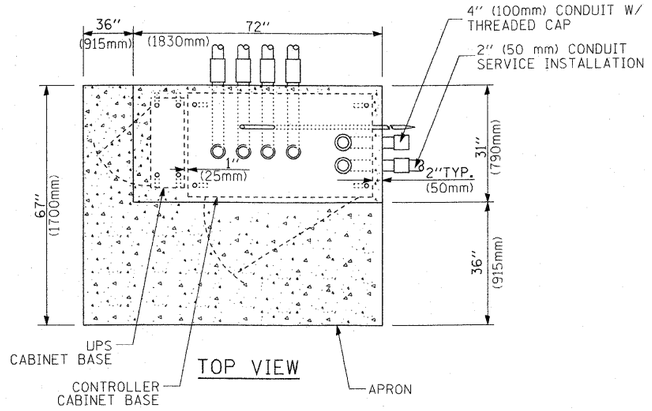
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	23
		CONTRACT NO. 63445		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	M-9003(488)	



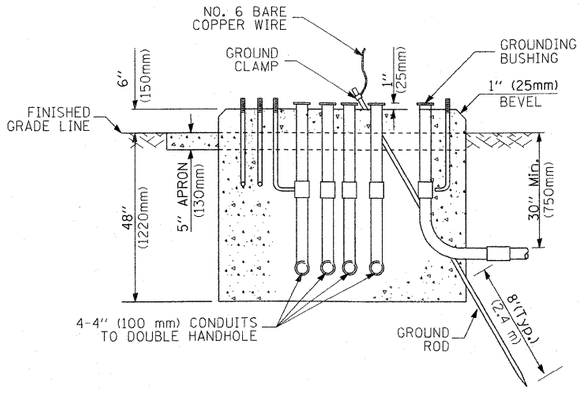
TOP VIEW



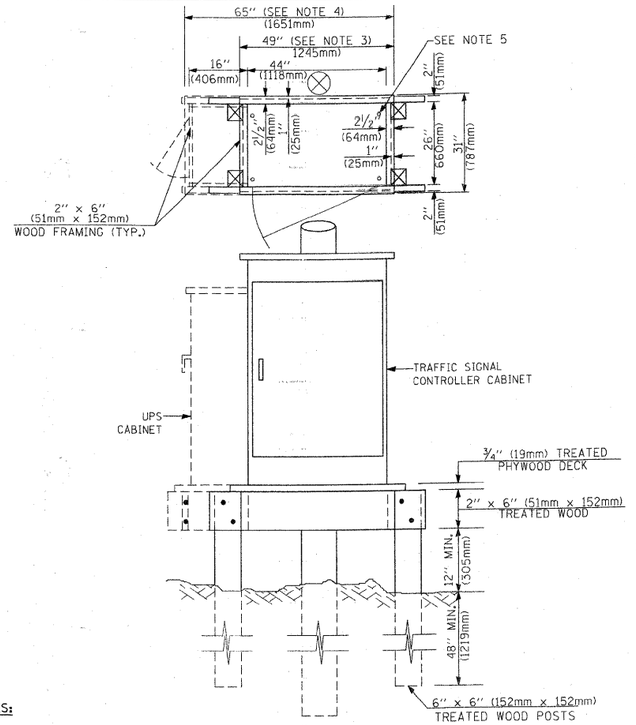
TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET



TOP VIEW



TYPE C
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001.

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

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		DRAWN - BCK	REVISED -
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	PLOT DATE = 10/6/2009	DATE - 10/28/09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: | SHEET NO. 5 OF 6 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	24
		CONTRACT NO. 63445		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	M-9003(488)	

TRAFFIC SIGNAL LEGEND

ITEM	REMOVAL	EXISTING	PROPOSED
CONTROLLER CABINET	R		
RAILROAD CONTROL CABINET		RC	RC
COMMUNICATIONS CABINET	R CC	ECC	CC
MASTER CONTROLLER		EMC	MC
MASTER MASTER CONTROLLER		EMMC	MMC
UNINTERRUPTIBLE POWER SUPPLY	R UPS	EUPS	UPS
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT	R	P	P
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT	R T	P T	P T
STEEL MAST ARM ASSEMBLY AND POLE	R		
ALUMINUM MAST ARM ASSEMBLY AND POLE	R		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE	R		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA	R PTZ	PTZ	PTZ
SIGNAL POST	R		
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM	R		
GUY WIRE	R		
SIGNAL HEAD	R		
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)			2
SIGNAL HEAD WITH BACKPLATE	R		
SIGNAL HEAD OPTICALLY PROGRAMMED	R "P"	"P"	"P"
FLASHER INSTALLATION (S DENOTES SOLAR POWER)	R "F"	"F"	"F"
PEDESTRIAN SIGNAL HEAD	R		
PEDESTRIAN PUSHBUTTON DETECTOR	R		
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR	R APS	APS	APS
ILLUMINATED SIGN "NO LEFT TURN"	R		
ILLUMINATED SIGN "NO RIGHT TURN"	R		
DETECTOR LOOP, TYPE I			
PREFORMED DETECTOR LOOP		P	P
MICROWAVE VEHICLE SENSOR	R M	M	M
VIDEO DETECTION CAMERA	R V	V	V
VIDEO DETECTION ZONE			
PAN, TILT, ZOOM CAMERA	R PTZ	PTZ	PTZ
WIRELESS DETECTOR SENSOR	R W	W	W
WIRELESS ACCESS POINT	R		

ITEM	REMOVAL	EXISTING	PROPOSED
EMERGENCY VEHICLE LIGHT DETECTOR	R		
CONFIRMATION BEACON	R		
HANDHOLE	R		
HEAVY DUTY HANDHOLE	R H	H	H
DOUBLE HANDHOLE	R		
JUNCTION BOX	R		
GALVANIZED STEEL CONDUIT IN TRENCH (T) OR PUSHED (P)			
TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE	R		
COMMON TRENCH			CT
COILABLE NONMETALLIC CONDUIT (EMPTY)			CNC
SYSTEM ITEM		S	S
INTERSECTION ITEM		I	IP
REMOVE ITEM	R		
RELOCATE ITEM	RL		
ABANDON ITEM	A		
12" (300mm) TRAFFIC SIGNAL SECTION		R	R
12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE		R Y G	
SIGNAL FACE		R Y G ←Y ←G	R Y G ←Y ←G
SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD		R Y G ←Y ←G "P"	R Y G ←Y ←G "P"
12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL		DW W	
12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED			
12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID			
PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER		C D	C D
RADIO INTERCONNECT	R		
RADIO REPEATER	R ERR	ERR	RR
DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED		5	5
GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)		1	1

ITEM	REMOVAL	EXISTING	PROPOSED
ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE		1	1
COAXIAL CABLE		C	C
VENDOR CABLE FOR CAMERA		V	V
COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED		6	6
FIBER OPTIC CABLE NO. 62.5/125, MM12F		12F	
FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F		24F	24F
FIBER OPTIC CABLE NO. 62.5/125, (NUMBER OF FIBERS & TYPE TO BE NOTED ON PLANS)			
GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE		C	C
CONTROLLER CABINET AND FOUNDATION TO BE REMOVED	RCF		
STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED	RMF		
ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED	RMF		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED	RMF		
SIGNAL POST AND FOUNDATION TO BE REMOVED	RMF		
INTERSECTION & SAMPLING (SYSTEM) DETECTOR		IS	IS
SAMPLING (SYSTEM) DETECTOR		S	S
EXISTING INTERSECTION LOOP DETECTOR PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR		P	
EXISTING PREFORMED INTERSECTION LOOP DETECTOR PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR		PP	
PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR		PIS	PIS
PREFORMED SAMPLING (SYSTEM) DETECTOR		PS	PS

RAILROAD SYMBOLS

	EXISTING	PROPOSED
RAILROAD CONTROL CABINET	RC	RC
RAILROAD CANTILEVER MAST ARM		
FLASHING SIGNAL		
CROSSING GATE		
CROSSBUCK		

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		DRAWN - BCK	REVISED -
	PLOT SCALE = 20.0000 ' / IN.	CHECKED - DAD	REVISED -
	PLOT DATE = 10/6/2009	DATE - 10/28/09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET NO. 6 OF 6 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	25
		CONTRACT NO. 63445		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	M-9003(488)	

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	26
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63455

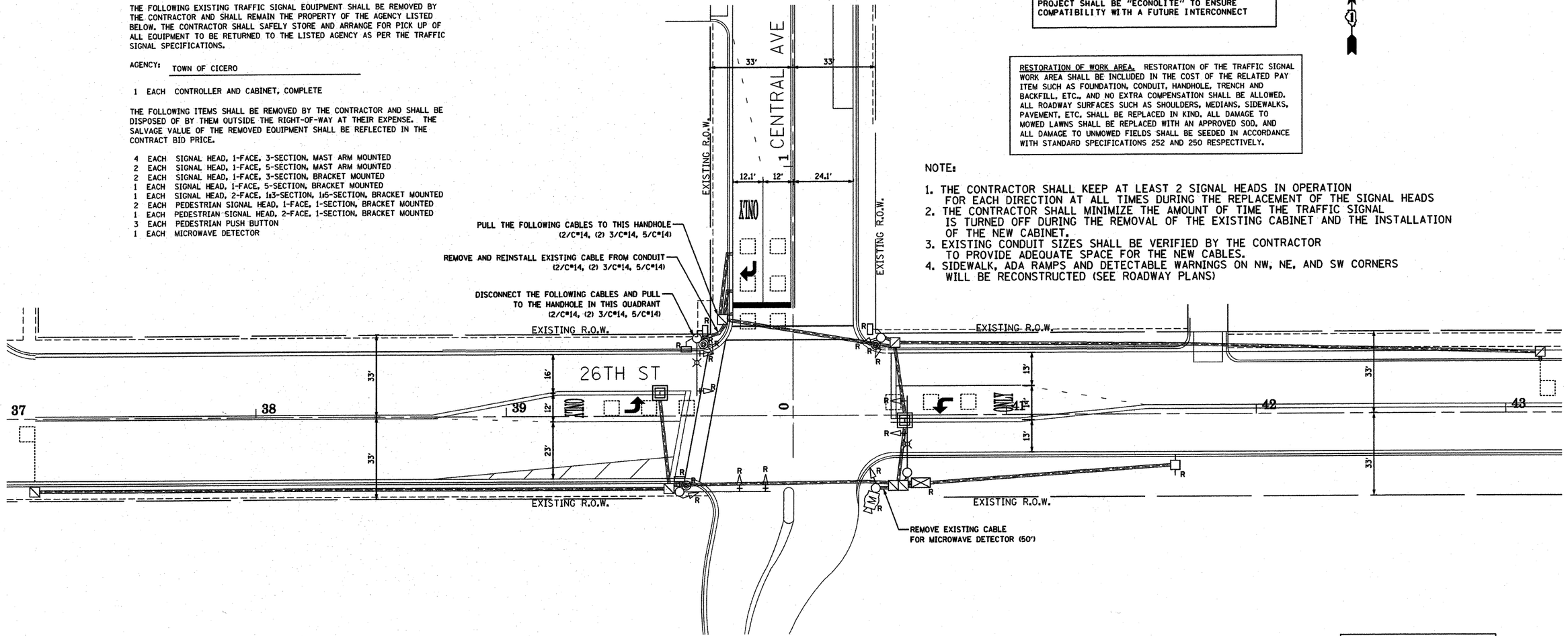
THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE AGENCY LISTED BELOW. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE LISTED AGENCY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

AGENCY: TOWN OF CICERO

1 EACH CONTROLLER AND CABINET, COMPLETE

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

4 EACH SIGNAL HEAD, 1-FACE, 3-SECTION, MAST ARM MOUNTED
2 EACH SIGNAL HEAD, 1-FACE, 5-SECTION, MAST ARM MOUNTED
2 EACH SIGNAL HEAD, 1-FACE, 3-SECTION, BRACKET MOUNTED
1 EACH SIGNAL HEAD, 1-FACE, 5-SECTION, BRACKET MOUNTED
1 EACH SIGNAL HEAD, 2-FACE, 1x3-SECTION, 1x5-SECTION, BRACKET MOUNTED
2 EACH PEDESTRIAN SIGNAL HEAD, 1-FACE, 1-SECTION, BRACKET MOUNTED
1 EACH PEDESTRIAN SIGNAL HEAD, 2-FACE, 1-SECTION, BRACKET MOUNTED
3 EACH PEDESTRIAN PUSH BUTTON
1 EACH MICROWAVE DETECTOR



NOTE:

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO ENSURE COMPATIBILITY WITH A FUTURE INTERCONNECT

N

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE COST OF THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

NOTE:

1. THE CONTRACTOR SHALL KEEP AT LEAST 2 SIGNAL HEADS IN OPERATION FOR EACH DIRECTION AT ALL TIMES DURING THE REPLACEMENT OF THE SIGNAL HEADS
2. THE CONTRACTOR SHALL MINIMIZE THE AMOUNT OF TIME THE TRAFFIC SIGNAL IS TURNED OFF DURING THE REMOVAL OF THE EXISTING CABINET AND THE INSTALLATION OF THE NEW CABINET.
3. EXISTING CONDUIT SIZES SHALL BE VERIFIED BY THE CONTRACTOR TO PROVIDE ADEQUATE SPACE FOR THE NEW CABLES.
4. SIDEWALK, ADA RAMPS AND DETECTABLE WARNINGS ON NW, NE, AND SW CORNERS WILL BE RECONSTRUCTED (SEE ROADWAY PLANS)

KLOA
Kenig,Lindgren,O'Hara,Aboona,Inc.
9575 West Higgins Road, Suite 400
Rosemont, Illinois 60018
P: (847) 518-9990 F: (847) 518-9987
PROJECT # 09-158

IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

Civil Engineers
Frank Novotny & Associates, Inc.
825 Midway Drive ◆ Willowbrook, IL ◆ 60527 ◆ Telephone: (630) 887-8640 ◆ Fax: (630) 887-0132
Illinois Professional Design Firm No. 184-000928

PROJECT

TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS

NO.	BY	DATE	DESCRIPTION
1	THK/DMS	1-22-10	PER IDOT REVIEW
2	THK/DMS	3-03-10	PER IDOT REVIEW

TRAFFIC SIGNAL REMOVAL PLAN
CENTRAL AVE AT 26TH ST

PROJECT NO. 05043	SCALE 1"=20'
DRAWN/DESIGNED JFP/THK-DMS	DATE DEC., 2009
CHECKED/APPROVED TPG/THK	FIELD BOOK NO. FILE

SHEET 26 OF 51 SHEETS

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	27
F.H.W.A. REG.	ILLINOIS	PROJECT M-9003(488)		

CONTRACT NO. 63455

NOTE:

1. THE CONTRACTOR SHALL KEEP AT LEAST 2 SIGNAL HEADS IN OPERATION FOR EACH DIRECTION AT ALL TIMES DURING THE REPLACEMENT OF THE SIGNAL HEADS
2. THE CONTRACTOR SHALL MINIMIZE THE AMOUNT OF TIME THE TRAFFIC SIGNAL IS TURNED OFF DURING THE REMOVAL OF THE EXISTING CABINET AND THE INSTALLATION OF THE NEW CABINET.
3. EXISTING CONDUIT SIZES SHALL BE VERIFIED BY THE CONTRACTOR TO PROVIDE ADEQUATE SPACE FOR THE NEW CABLES.
4. SIDEWALK, ADA RAMPS AND DETECTABLE WARNINGS ON NW, NE, AND SW CORNERS WILL BE RECONSTRUCTED (SEE ROADWAY PLANS)

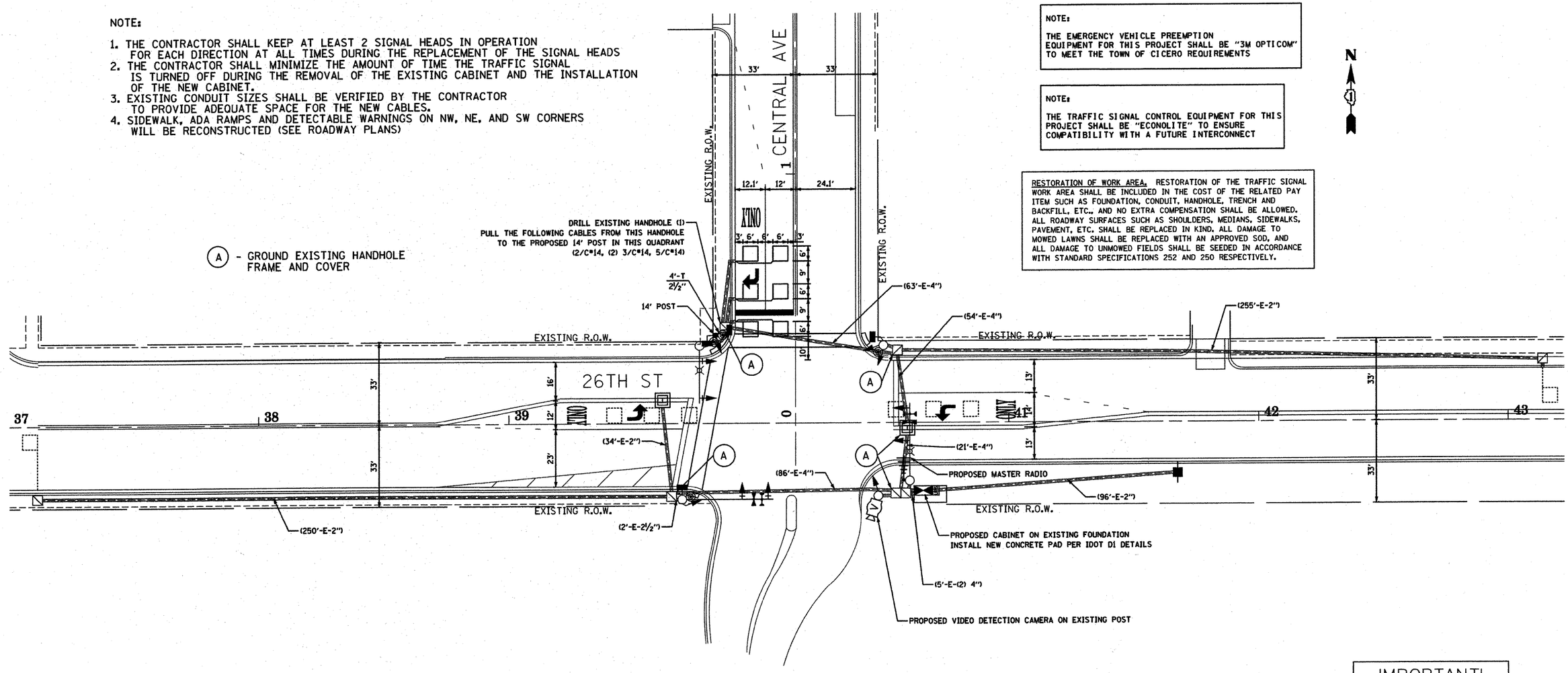
NOTE:

THE EMERGENCY VEHICLE PREEMPTION EQUIPMENT FOR THIS PROJECT SHALL BE "3M OPTICOM" TO MEET THE TOWN OF CICERO REQUIREMENTS

NOTE:

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO ENSURE COMPATIBILITY WITH A FUTURE INTERCONNECT

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE COST OF THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



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Rosemont, Illinois 60018
P: (847) 518-9990 F: (847) 518-9987
PROJECT # 09-158

IMPORTANT!

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

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Illinois Professional Design Firm No. 184-000928

PROJECT

TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS

NO.	BY	DATE	DESCRIPTION
1	THK/DMS	1-22-10	PER IDOT REVIEW
2	THK/DMS	3-03-10	PER IDOT REVIEW

TRAFFIC SIGNAL MODIFICATION PLAN CENTRAL AVE AT 26TH ST

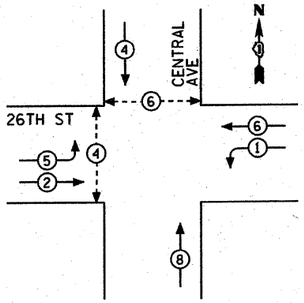
PROJECT NO.	SCALE
05043	1"=20'
DRAWN/DESIGNED	DATE
JFP/THK-DMS	DEC., 2009
CHECKED/APPROVED	FIELD BOOK NO.
TPG/THK	FILE

SHEET 27 OF 51 SHEETS

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	28
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63455

CONTROLLER SEQUENCE

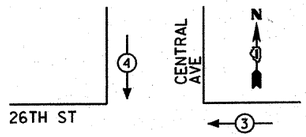


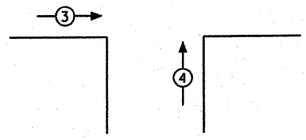
PHASE DESIGNATION DIAGRAM

LEGEND

- DUAL ENTRY PHASE
- SINGLE ENTRY PHASE
- OL OVERLAP
- PEDESTRIAN PHASE
- NUMBER REFERS TO ASSOCIATED PHASE

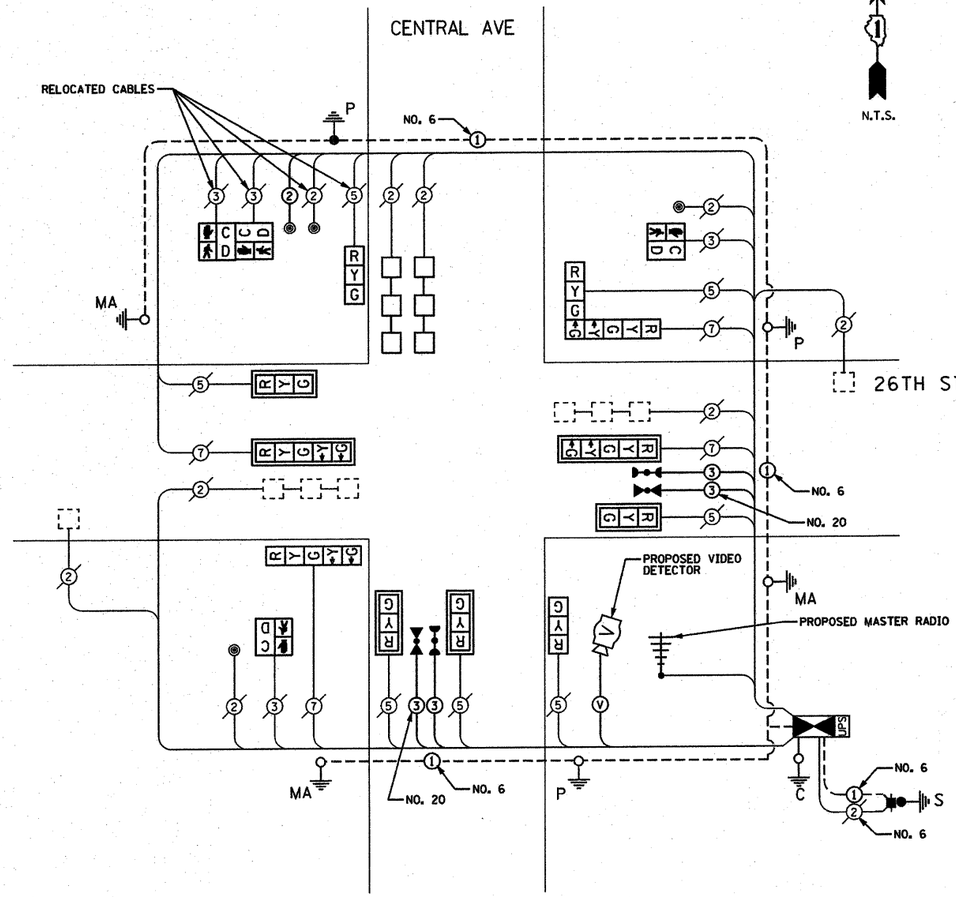
EMERGENCY VEHICLE PREEMPTION SEQUENCE





PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	← →	↓ ↑

PROPOSED CABLE PLAN



N.T.S.

NOTE:

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO ENSURE COMPATIBILITY WITH A FUTURE INTERCONNECT

NOTE:

THE EMERGENCY VEHICLE PREEMPTION EQUIPMENT FOR THIS PROJECT SHALL BE "3M OPTICOM" TO MEET THE TOWN OF CICERO REQUIREMENTS

I.D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	WATTAGE LED	%OPERATION	
SIGNAL (RED)	11		17	0.50	93.5
(YELLOW)	11		25	0.25	68.75
(GREEN)	11		15	0.25	41.25
ARROW	8		12	0.10	9.6
PED. SIGNAL	4		25	1.00	100.0
CONTROLLER	1		100	1.00	100.0
ILLUM. SIGN				0.05	
FLASHER				0.50	
ENERGY COSTS TO: TOWN OF CICERO 4949 WEST CERMAK ROAD CICERO, ILLINOIS 60804				TOTAL =	413.1
ENERGY SUPPLY CONTACT: MIKE BELL PHONE: (708) 410-5314 COMPANY: COMED					

SCHEDULE OF QUANTITIES

QTY	UNIT	ITEM DESCRIPTION
36	SQ FT	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
4	FOOT	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL
4	FOOT	TRENCH AND BACKFILL FOR ELECTRICAL WORK
1	EACH	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION
1	EACH	FULL-ACTUATED CONTROLLER AND TYPE V CABINET, SPECIAL
1	EACH	MASTER CONTROLLER (SPECIAL)
188	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
249	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
1	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.
4	FOOT	CONCRETE FOUNDATION, TYPE A
1	EACH	DRILL EXISTING HANDHOLE
4	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED
2	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED
1	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED
2	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED
1	EACH	SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED
2	EACH	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
1	EACH	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
6	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM
6	EACH	INDUCTIVE LOOP DETECTOR
183	FOOT	DETECTOR LOOP, TYPE I
2	EACH	LIGHT DETECTOR
1	EACH	LIGHT DETECTOR AMPLIFIER
4	EACH	PEDESTRIAN PUSH-BUTTON
50	FOOT	REMOVE ELECTRIC CABLE FROM CONDUIT
144	FOOT	REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
1	EACH	VIDEO VEHICLE DETECTION SYSTEM
1	EACH	SERVICE INSTALLATION, POLE MOUNTED
5	EACH	GROUNDING EXISTING HANDHOLE FRAME AND COVER
1	EACH	UNINTERRUPTABLE POWER SUPPLY
294	FOOT	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C
249	FOOT	ELECTRIC CABLE IN CONDUIT, NO. 20 3/C, TWISTED, SHIELDED

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Kenig,Lindgren,O'Hara,Aboona,Inc.
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PROJECT # 09-158

Civil Engineers
Frank Novotny & Associates, Inc.
825 Midway Drive ◆ Willowbrook, IL ◆ 60527 ◆ Telephone: (630) 887-8640 ◆ Fax: (630) 887-0132
Illinois Professional Design Firm No. 184-000928

PROJECT

TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS

NO.	BY	DATE	DESCRIPTION
1	THK/DMS	1-22-10	PER IDOT REVIEW
2	THK/DMS	3-03-10	PER IDOT REVIEW

CABLE PLAN, PHASE DESIGNATION DIAGRAM, EVP SEQUENCE AND SCHEDULE OF QUANTITIES CENTRAL AVE AT 26TH ST

PROJECT NO.	SCALE	SHEET
05043	NONE	28
DRAWN/DESIGNED	DATE	OF
JFP/THK-DMS	DEC., 2009	51
CHECKED/APPROVED	FIELD BOOK NO.	SHEETS
TPG/THK	FILE	

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	29
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63485
4

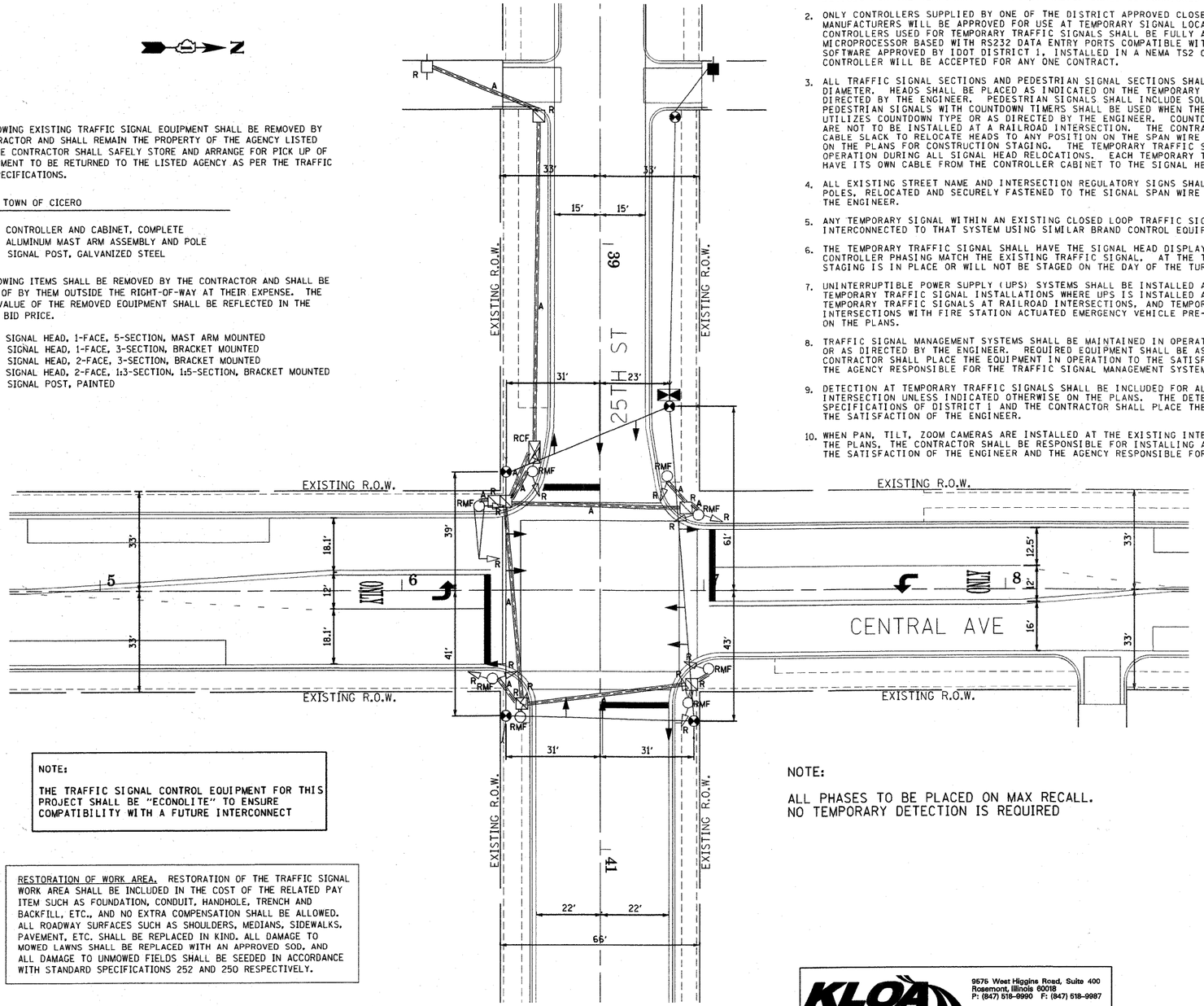
THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE AGENCY LISTED BELOW. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE LISTED AGENCY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

AGENCY: TOWN OF CICERO

1 EACH CONTROLLER AND CABINET, COMPLETE
1 EACH ALUMINUM MAST ARM ASSEMBLY AND POLE
3 EACH SIGNAL POST, GALVANIZED STEEL

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

1 EACH SIGNAL HEAD, 1-FACE, 5-SECTION, MAST ARM MOUNTED
4 EACH SIGNAL HEAD, 1-FACE, 3-SECTION, BRACKET MOUNTED
3 EACH SIGNAL HEAD, 2-FACE, 3-SECTION, BRACKET MOUNTED
1 EACH SIGNAL HEAD, 2-FACE, 1:3-SECTION, 1:5-SECTION, BRACKET MOUNTED
4 EACH SIGNAL POST, PAINTED



NOTE:

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO ENSURE COMPATIBILITY WITH A FUTURE INTERCONNECT

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE COST OF THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD. AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

NOTES FOR TEMPORARY TRAFFIC SIGNALS

1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE LED AND 12" (300mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT A RAILROAD INTERSECTION. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
7. UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEMS SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL, TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
8. TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
9. DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
10. WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.

NOTE:

ALL PHASES TO BE PLACED ON MAX RECALL.
NO TEMPORARY DETECTION IS REQUIRED

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P: (847) 518-9990 F: (847) 518-9987
PROJECT # 09-158

IMPORTANT!
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

Civil Engineers
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825 Midway Drive ◆ Willowbrook, IL ◆ 60527 ◆ Telephone: (630) 887-8640 ◆ Fax: (630) 887-0132
Illinois Professional Design Firm No. 184-000928

PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS			
NO.	BY	DATE	DESCRIPTION
1	THK/DMS	1-22-10	PER IDOT REVIEW

TEMPORARY TRAFFIC SIGNAL AND REMOVAL PLAN CENTRAL AVE AT 25TH ST

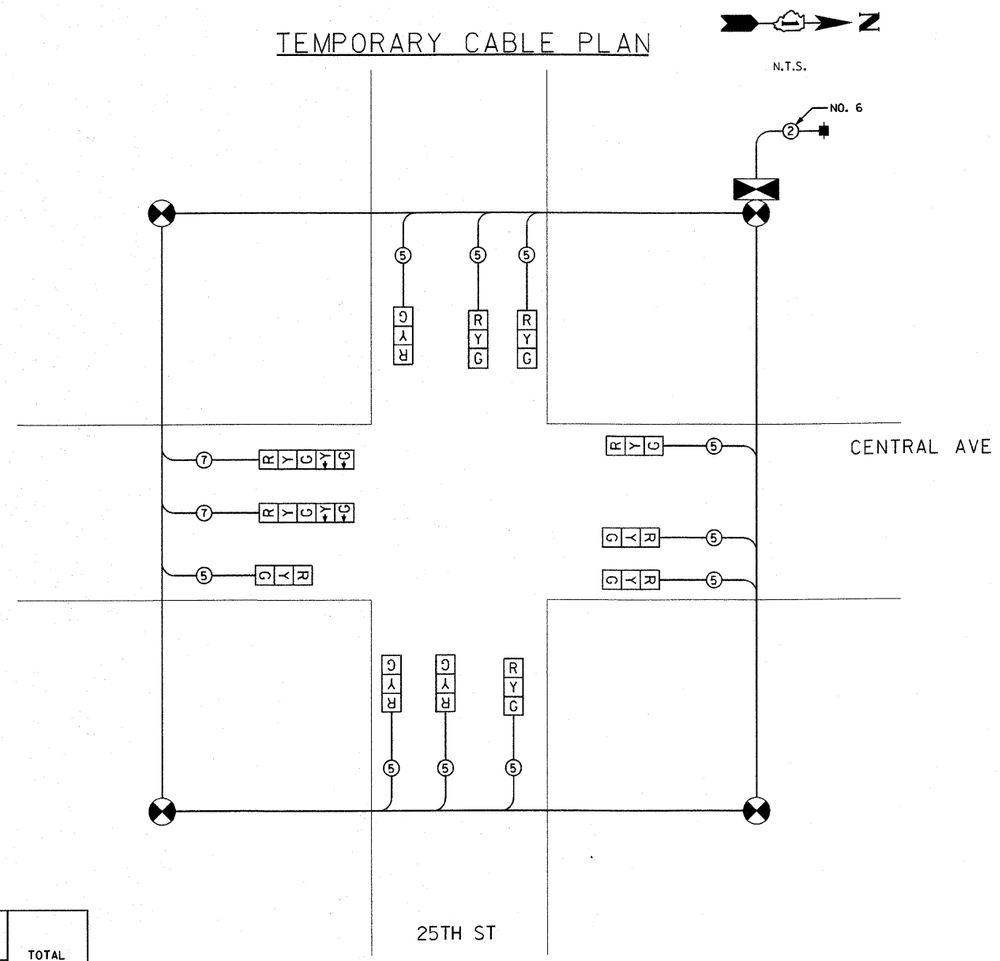
PROJECT NO. 05043	SCALE 1"=20'	SHEET 29 OF 51 SHEETS
DRAWN/DESIGNED JFP/THK-DMS	DATE DEC., 2009	
CHECKED/APPROVED TPG/THK	FIELD BOOK NO. FILE	

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	30
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

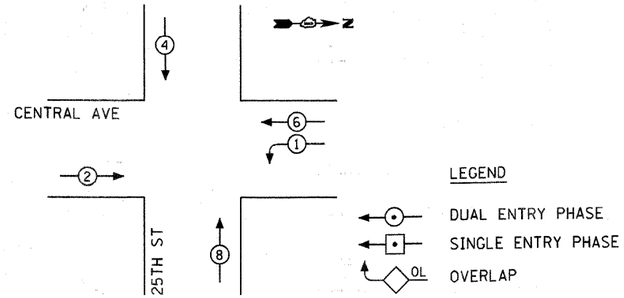
CONTRACT NO. 63455
4

TEMPORARY CABLE PLAN

N.T.S.



CONTROLLER SEQUENCE



PHASE DESIGNATION DIAGRAM

LEGEND

- DUAL ENTRY PHASE
- SINGLE ENTRY PHASE
- OL — OVERLAP
- PEDESTRIAN PHASE
- • NUMBER REFERS TO ASSOCIATED PHASE

NOTE:

ALL PHASES TO BE PLACED ON MAX RECALL.
NO TEMPORARY DETECTION IS REQUIRED

I.D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	WATTAGE LED	% OPERATION	
SIGNAL (RED)	12		17	0.50	102.0
(YELLOW)	12		25	0.25	75.0
(GREEN)	12		15	0.25	45.0
ARROW	4		12	0.10	4.8
PED. SIGNAL	-		25	1.00	
CONTROLLER	1		100	1.00	100.0
ILLUM. SIGN	-			0.05	
VIDEO SYSTEM	-			1.00	
FLASHER				0.50	
ENERGY COSTS TO:				TOTAL =	326.8

TOWN OF CICERO
4949 WEST CERMAK ROAD
CICERO, ILLINOIS 60804
ENERGY SUPPLY CONTACT: MIKE BELL
PHONE: (708) 410-5314
COMPANY: COM. ED.

KLOA
Kenig,Lindgren,O'Hara,Aboona,Inc.
9575 West Higgins Road, Suite 400
Rosemont, Illinois 60018
P: (847) 518-9990 F: (847) 518-9987
PROJECT # 09-158

...ORARY CABLE PLAN AND PHASE DESIGNATION DIAGRAM

Civil Engineers
Frank Novotny & Associates, Inc.
825 Midway Drive ◆ Willowbrook, IL ◆ 60527 ◆ Telephone: (630) 887-8640 ◆ Fax: (630) 887-0132
Illinois Professional Design Firm No. 184-000928

PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS			
NO.	BY	DATE	DESCRIPTION
1	THK/DMS	1-22-10	PER IDOT REVIEW

TEMPORARY CABLE PLAN AND PHASE DESIGNATION DIAGRAM CENTRAL AVE AT 25TH ST

PROJECT NO. 05043	SCALE NONE
DRAWN/DESIGNED JFP/THK-DMS	DATE DEC., 2009
CHECKED/APPROVED TPG/THK	FIELD BOOK NO. FILE

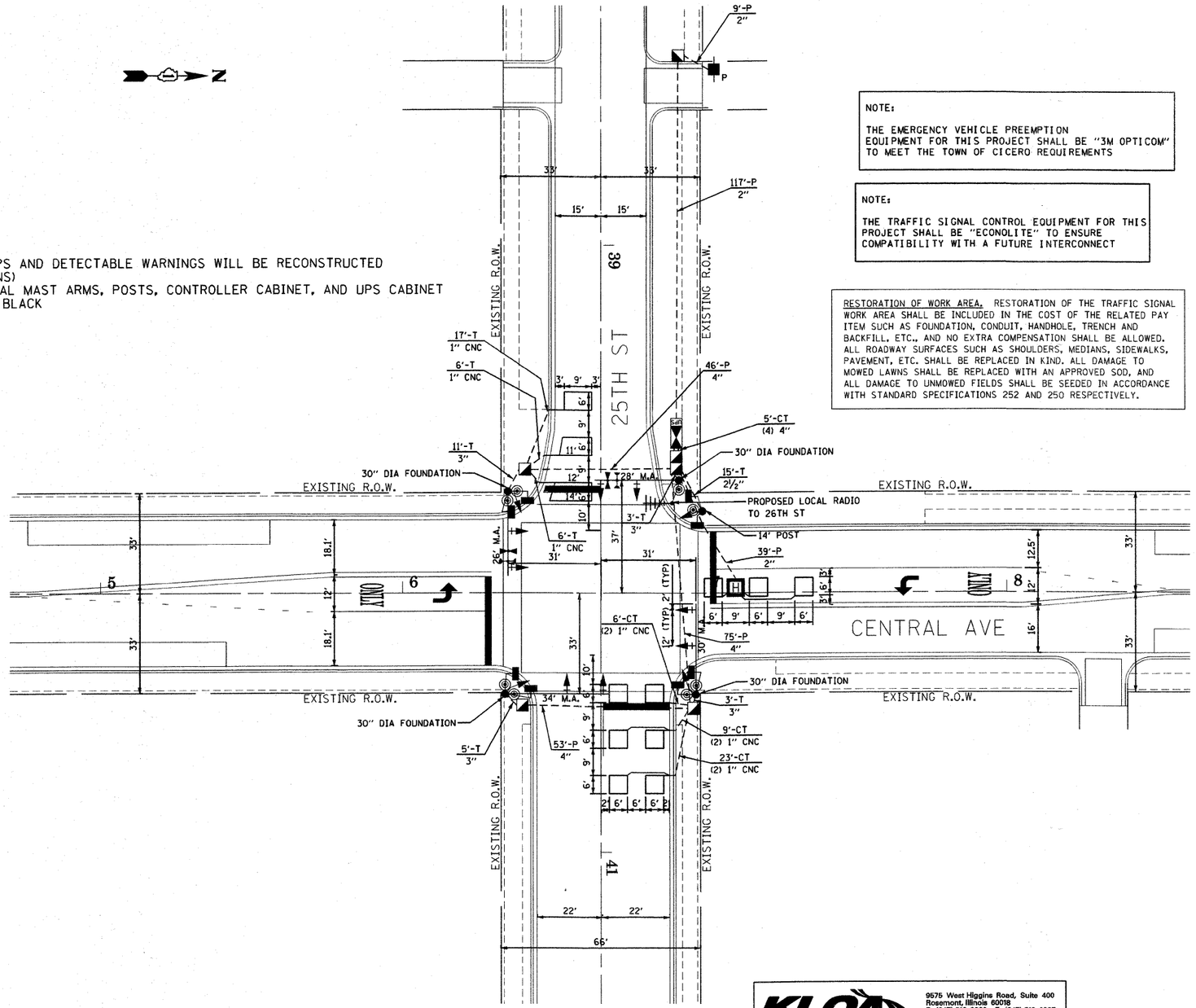
SHEET 30 OF 51 SHEETS

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	31
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63485
4

NOTE:

1. SIDEWALK, ADA RAMPS AND DETECTABLE WARNINGS WILL BE RECONSTRUCTED (SEE ROADWAY PLANS)
2. ALL PROPOSED SIGNAL MAST ARMS, POSTS, CONTROLLER CABINET, AND UPS CABINET SHALL BE PAINTED BLACK



NOTE:

THE EMERGENCY VEHICLE PREEMPTION EQUIPMENT FOR THIS PROJECT SHALL BE "3M OPTICOM" TO MEET THE TOWN OF CICERO REQUIREMENTS

NOTE:

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO ENSURE COMPATIBILITY WITH A FUTURE INTERCONNECT

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE COST OF THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

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PROJECT # 09-158

IMPORTANT!
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Illinois Professional Design Firm No. 184-000928

PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS			
NO.	BY	DATE	DESCRIPTION
1	THK/DMS	1-22-10	PER IDOT REVIEW

TRAFFIC SIGNAL INSTALLATION PLAN CENTRAL AVE AT 25TH ST

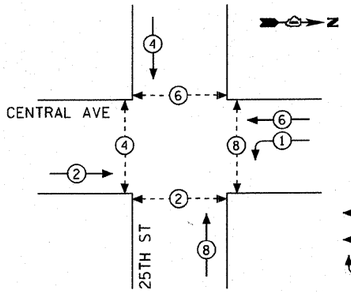
PROJECT NO. 05043	SCALE 1"=20'
DRAWN/DESIGNED JFP/THK-DMS	DATE DEC., 2009
CHECKED/APPROVED TPG/THK	FIELD BOOK NO. FILE

SHEET **31** OF **51** SHEETS

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	32
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63485

CONTROLLER SEQUENCE

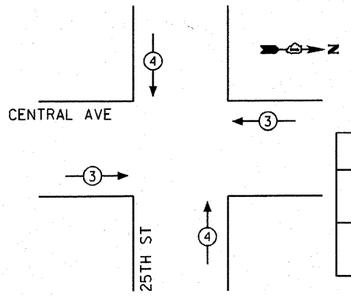


PHASE DESIGNATION DIAGRAM

LEGEND

- DUAL ENTRY PHASE
- SINGLE ENTRY PHASE
- OVERLAP
- PEDESTRIAN PHASE
- NUMBER REFERS TO ASSOCIATED PHASE

EMERGENCY VEHICLE PREEMPTION SEQUENCE



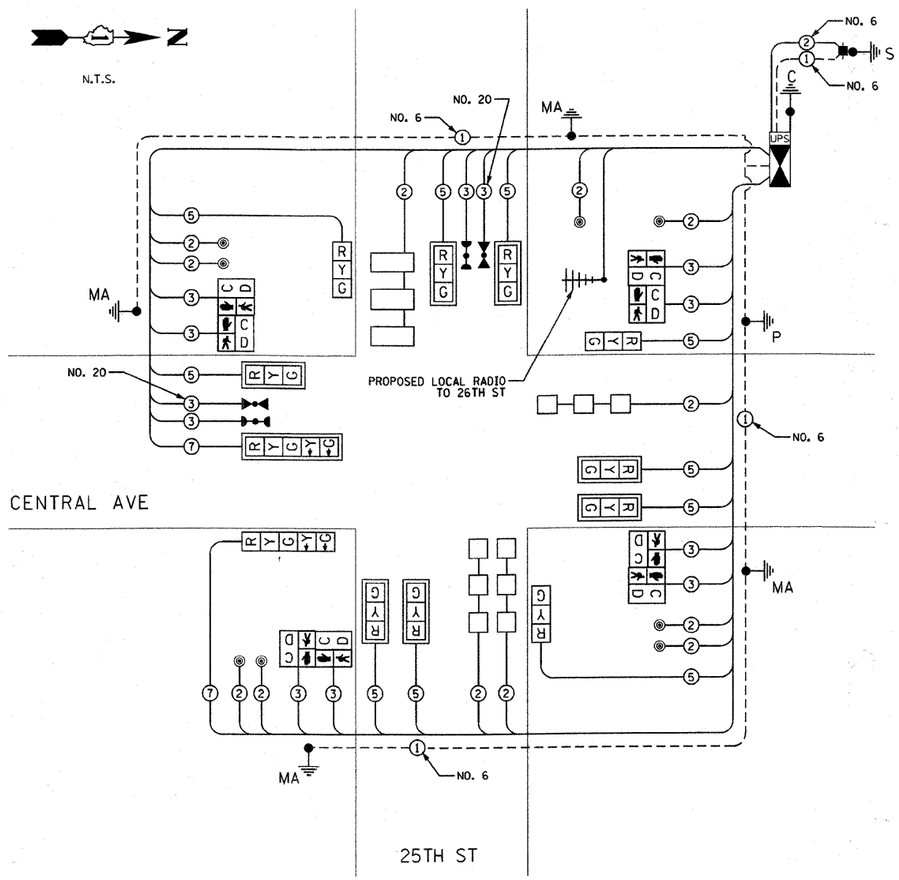
PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	←	→ ↓ ↑

I.D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	WATTAGE LED	*%OPERATION	
SIGNAL (RED)	12		17	0.50	102.0
(YELLOW)	12		25	0.25	75.0
(GREEN)	12		15	0.25	45.0
ARROW	4		12	0.10	4.8
PED. SIGNAL	8		25	1.00	200.0
CONTROLLER	1		100	1.00	100.0
ILLUM. SIGN				0.05	
FLASHER				0.50	
ENERGY COSTS TO:				TOTAL =	526.8

TOWN OF CICERO
4949 WEST CERMAK ROAD
CICERO, ILLINOIS 60804

ENERGY SUPPLY CONTACT: MIKE BELL
PHONE: (708) 410-5314
COMPANY: COMED

PROPOSED CABLE PLAN



NOTE:

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO ENSURE COMPATIBILITY WITH A FUTURE INTERCONNECT

NOTE:

THE EMERGENCY VEHICLE PREEMPTION EQUIPMENT FOR THIS PROJECT SHALL BE "3M OPTICOM" TO MEET THE TOWN OF CICERO REQUIREMENTS

SCHEDULE OF QUANTITIES

QTY	UNIT	ITEM DESCRIPTION
27	SQ FT	SIGN PANEL - TYPE 1
15	FOOT	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL
22	FOOT	CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL
20	FOOT	CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL
235	FOOT	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL
174	FOOT	CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL
4	EACH	HANDHOLE
1	EACH	HEAVY-DUTY HANDHOLE
1	EACH	DOUBLE HANDHOLE
42	FOOT	TRENCH AND BACKFILL FOR ELECTRICAL WORK
1	EACH	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL
846	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
1139	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
1293	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
319	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
351	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
151	FOOT	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C
1	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 26 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 28 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 30 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 34 FT.
4	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE C
60	FOOT	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER
7	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED
3	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED
1	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED
1	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED
4	EACH	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
8	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM
4	EACH	INDUCTIVE LOOP DETECTOR
397	FOOT	DETECTOR LOOP, TYPE I
2	EACH	LIGHT DETECTOR
1	EACH	LIGHT DETECTOR AMPLIFIER
8	EACH	PEDESTRIAN PUSH-BUTTON
1	EACH	TEMPORARY TRAFFIC SIGNAL INSTALLATION
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
5	EACH	REMOVE EXISTING HANDHOLE
9	EACH	REMOVE EXISTING CONCRETE FOUNDATION
4	EACH	PAINT NEW COMBINATION MAST ARM AND POLE, UNDER 40 FT
1	EACH	PAINT NEW SIGNAL POST
1	EACH	TEMPORARY TRAFFIC SIGNAL TIMING
1	EACH	SERVICE INSTALLATION, POLE MOUNTED
1	EACH	UNINTERRUPTABLE POWER SUPPLY
491	FOOT	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C
209	FOOT	ELECTRIC CABLE IN CONDUIT, NO. 20 3/C, TWISTED, SHIELDED

KLOA
Kenig,Lindgren,O'Hara,Aboona,Inc.
9575 West Higgins Road, Suite 400
Rosemont, Illinois 60018
P: (847) 518-9990 F: (847) 518-9987
PROJECT # 09-158

CABLE PLAN, PHASE DESIGNATION DIAGRAM, EVP SEQUENCE AND SCHEDULE OF QUANTITIES CENTRAL AVE AT 25TH ST

Civil Engineers
Frank Novotny & Associates, Inc.
825 Midway Drive ◆ Willowbrook, IL ◆ 60527 ◆ Telephone: (630) 887-8640 ◆ Fax: (630) 887-0132
Illinois Professional Design Firm No. 184-000928

PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS

NO.	BY	DATE	DESCRIPTION
1	THK/DMS	1-22-10	PER IDOT REVIEW

PROJECT NO.	SCALE
05043	NONE
DRAWN/DESIGNED: JFP/THK-DMS	DATE: DEC., 2009
CHECKED/APPROVED: TPG/THK	FIELD BOOK NO.: FILE

SHEET 32 OF 51 SHEETS

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	33
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63455
4

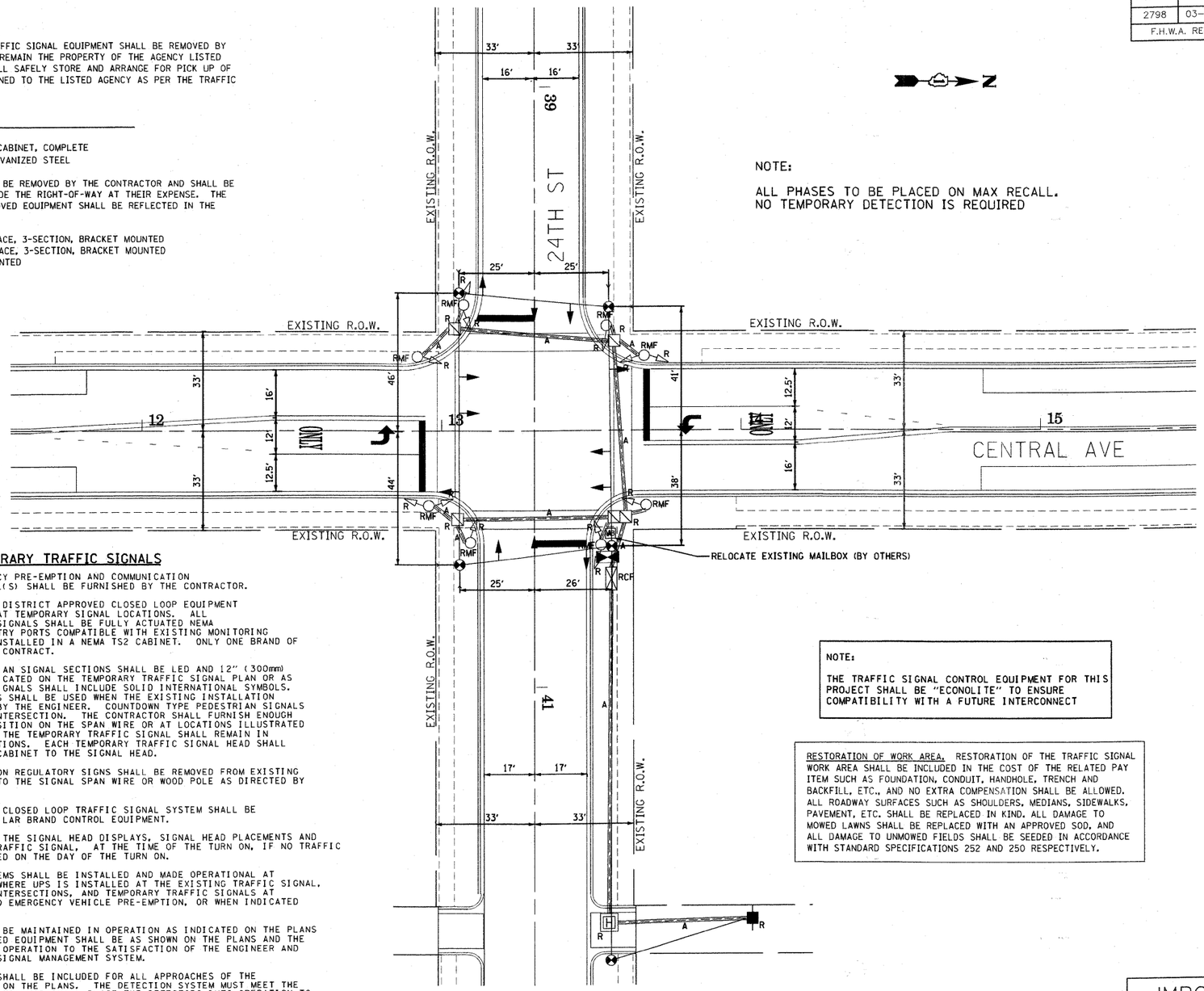
THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE AGENCY LISTED BELOW. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE LISTED AGENCY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

AGENCY: TOWN OF CICERO

1 EACH CONTROLLER AND CABINET, COMPLETE
4 EACH SIGNAL POST, GALVANIZED STEEL

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

4 EACH SIGNAL HEAD, 1-FACE, 3-SECTION, BRACKET MOUNTED
4 EACH SIGNAL HEAD, 2-FACE, 3-SECTION, BRACKET MOUNTED
4 EACH SIGNAL POST, PAINTED



NOTE:

ALL PHASES TO BE PLACED ON MAX RECALL.
NO TEMPORARY DETECTION IS REQUIRED

NOTE:

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO ENSURE COMPATIBILITY WITH A FUTURE INTERCONNECT

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE COST OF THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

NOTES FOR TEMPORARY TRAFFIC SIGNALS

1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE LED AND 12" (300mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT A RAILROAD INTERSECTION. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
7. UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEMS SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL, TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
8. TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
9. DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
10. WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.

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PROJECT # 09-158

IMPORTANT!
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

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Illinois Professional Design Firm No. 184-000928

PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS

NO.	BY	DATE	DESCRIPTION
1	THK/DMS	1-22-10	PER IDOT REVIEW

TEMPORARY TRAFFIC SIGNAL AND REMOVAL PLAN CENTRAL AVE AT 24TH ST

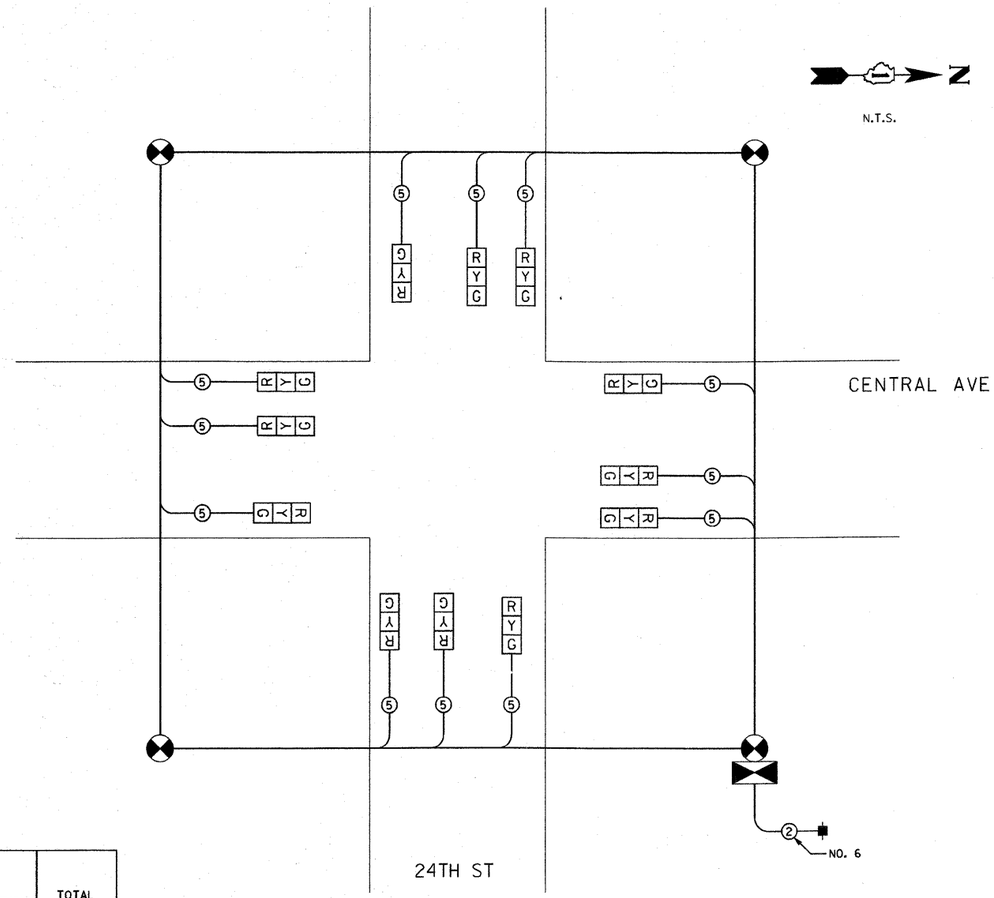
PROJECT NO. 05043	SCALE 1"=20'
DRAWN/DESIGNED JFP/THK-DMS	DATE DEC., 2009
CHECKED/APPROVED TPG/THK	FIELD BOOK NO. FILE

SHEET 33 OF 51 SHEETS

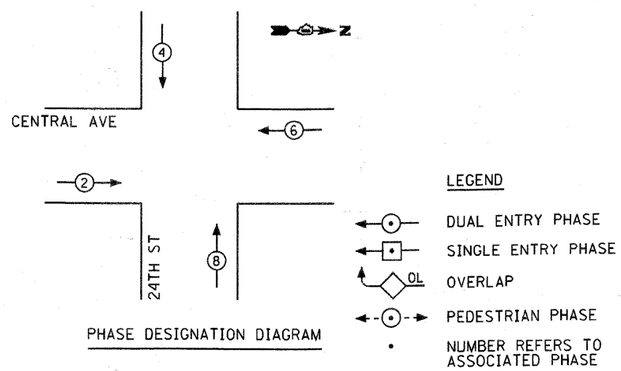
FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	34
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63485
4

TEMPORARY CABLE PLAN



CONTROLLER SEQUENCE



PHASE DESIGNATION DIAGRAM

LEGEND

- DUAL ENTRY PHASE
- SINGLE ENTRY PHASE
- OL — OVERLAP
- PEDESTRIAN PHASE
- NUMBER REFERS TO ASSOCIATED PHASE

NOTE:

ALL PHASES TO BE PLACED ON MAX RECALL.
NO TEMPORARY DETECTION IS REQUIRED

I.D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	WATTAGE LED	X %OPERATION	
SIGNAL (RED)	12		17	0.50	102.0
(YELLOW)	12		25	0.25	75.0
(GREEN)	12		15	0.25	45.0
ARROW	-		12	0.10	
PED. SIGNAL	-		25	1.00	
CONTROLLER	1		100	1.00	100.0
ILLUM. SIGN	-			0.05	
VIDEO SYSTEM	-			1.00	
FLASHER				0.50	
ENERGY COSTS TO:				TOTAL =	322.0

TOWN OF CICERO
4949 WEST CERMAK ROAD
CICERO, ILLINOIS 60804
ENERGY SUPPLY CONTACT: MIKE BELL
PHONE: (708) 410-5314
COMPANY: COM. ED.

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Rosemont, Illinois 60018
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PROJECT # 09-158

TEMPORARY CABLE PLAN AND PHASE DESIGNATION DIAGRAM

Civil Engineers
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Illinois Professional Design Firm No. 184-000928

PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS

NO.	BY	DATE	DESCRIPTION
1	THK/DMS	1-22-10	PER IDOT REVIEW

TEMPORARY CABLE PLAN AND PHASE DESIGNATION DIAGRAM CENTRAL AVE AT 24TH ST

PROJECT NO.	SCALE
05043	NONE
DRAWN/DESIGNED	DATE
JFP/THK-DMS	DEC., 2009
CHECKED/APPROVED	FIELD BOOK NO.
TPG/THK	FILE

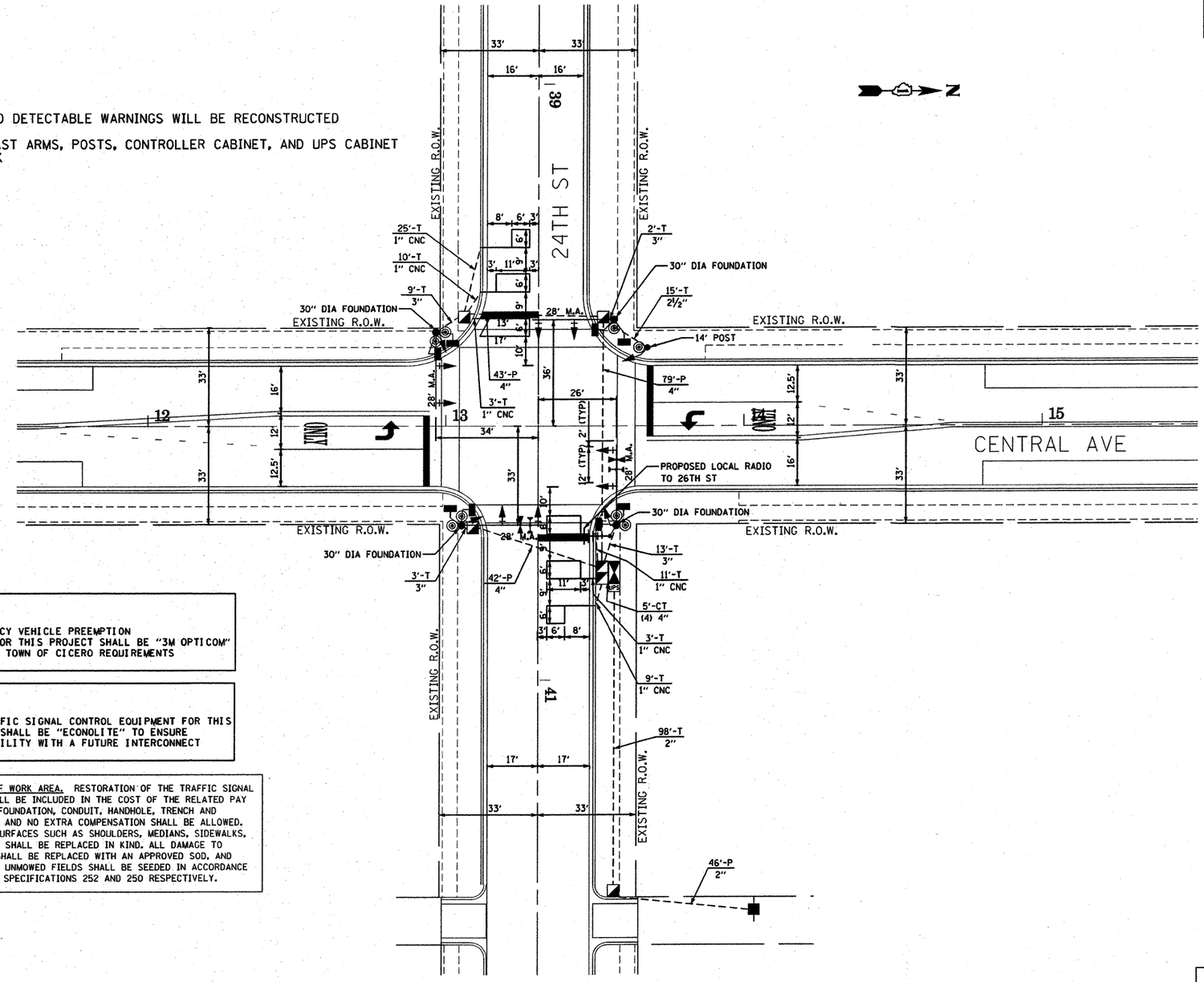
SHEET **34** OF **51** SHEETS

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	35
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63455
4

NOTE:

1. SIDEWALK, ADA RAMPS AND DETECTABLE WARNINGS WILL BE RECONSTRUCTED (SEE ROADWAY PLANS)
2. ALL PROPOSED SIGNAL MAST ARMS, POSTS, CONTROLLER CABINET, AND UPS CABINET SHALL BE PAINTED BLACK



NOTE:

THE EMERGENCY VEHICLE PREEMPTION EQUIPMENT FOR THIS PROJECT SHALL BE "3M OPTICOM" TO MEET THE TOWN OF CICERO REQUIREMENTS

NOTE:

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO ENSURE COMPATIBILITY WITH A FUTURE INTERCONNECT

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE COST OF THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

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Rosemont, Illinois 60018
P: (847) 518-9990 F: (847) 518-9987
PROJECT # 09-158

IMPORTANT!
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

Civil Engineers
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Illinois Professional Design Firm No. 184-000928

PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS

NO.	BY	DATE	DESCRIPTION
1	THK/DMS	1-22-10	PER IDOT REVIEW
2	THK/DMS	2-8-10	PER IDOT REVIEW

TRAFFIC SIGNAL INSTALLATION PLAN CENTRAL AVE AT 24TH ST

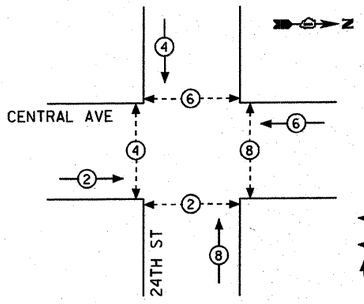
PROJECT NO.	SCALE
05043	1"=20'
DRAWN/DESIGNED: JFP/THK-DMS	DATE: DEC., 2009
CHECKED/APPROVED: TPG/THK	FIELD BOOK NO.: FILE

SHEET 35 OF 51 SHEETS

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	36
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63485
4

CONTROLLER SEQUENCE

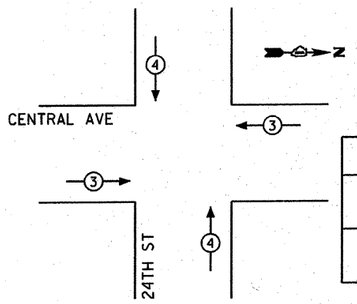


PHASE DESIGNATION DIAGRAM

LEGEND

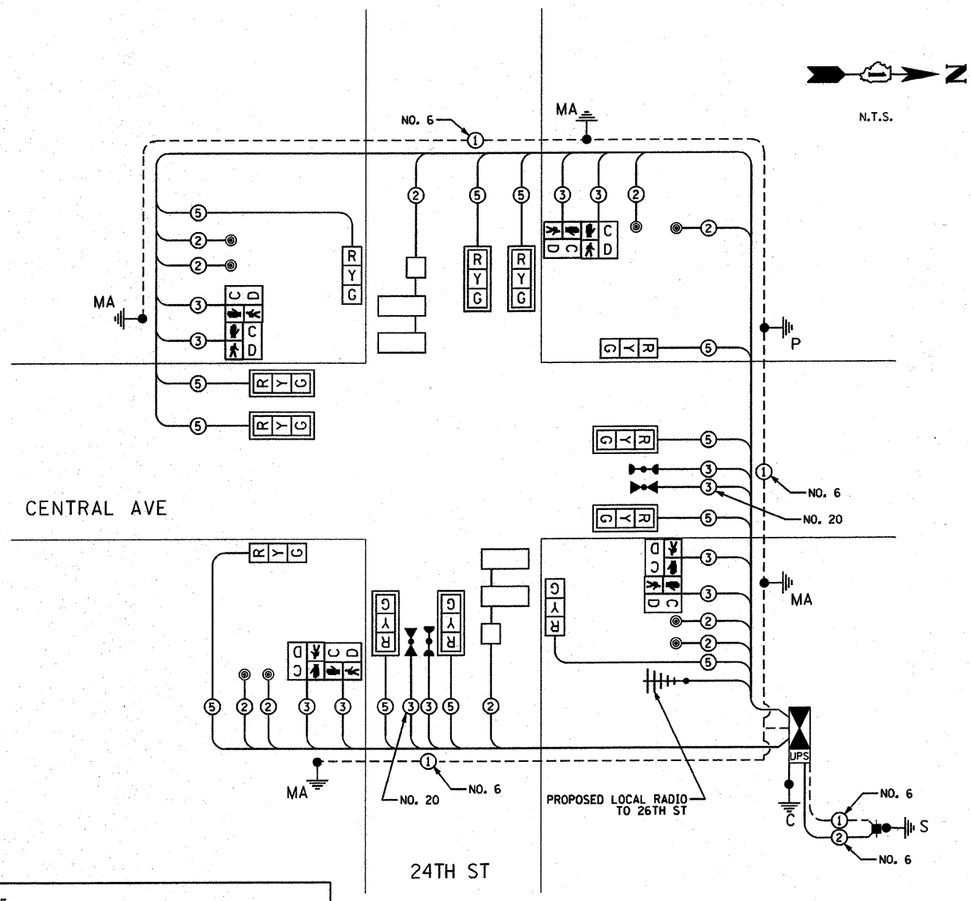
- DUAL ENTRY PHASE
- SINGLE ENTRY PHASE
- OL OVERLAP
- PEDESTRIAN PHASE
- NUMBER REFERS TO ASSOCIATED PHASE

EMERGENCY VEHICLE PREEMPTION SEQUENCE



PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	←	↓ ↑

PROPOSED CABLE PLAN



I.D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	WATTAGE LED	%OPERATION	
SIGNAL (RED)	12		17	0.50	102.0
(YELLOW)	12		25	0.25	75.0
(GREEN)	12		15	0.25	45.0
ARROW	-		12	0.10	-
PED. SIGNAL	8		25	1.00	200.0
CONTROLLER	1		100	1.00	100.0
ILLUM. SIGN				0.05	
FLASHER				0.50	
ENERGY COSTS TO:				TOTAL =	522.0

TOWN OF CICERO
4949 WEST CERMAK ROAD
CICERO, ILLINOIS 60804
ENERGY SUPPLY CONTACT: MIKE BELL
PHONE: (708) 410-5314
COMPANY: COMED

NOTE:

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO ENSURE COMPATIBILITY WITH A FUTURE INTERCONNECT

NOTE:

THE EMERGENCY VEHICLE PREEMPTION EQUIPMENT FOR THIS PROJECT SHALL BE "3M OPTICOM" TO MEET THE TOWN OF CICERO REQUIREMENTS

SCHEDULE OF QUANTITIES

QTY	UNIT	ITEM DESCRIPTION
27	SQ FT	SIGN PANEL - TYPE 1
98	FOOT	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL
15	FOOT	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL
27	FOOT	CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL
20	FOOT	CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL
46	FOOT	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL
164	FOOT	CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL
4	EACH	HANDHOLE
1	EACH	DOUBLE HANDHOLE
145	FOOT	TRENCH AND BACKFILL FOR ELECTRICAL WORK
1	EACH	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL
845	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
1139	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
1585	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
180	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
169	FOOT	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C
1	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.
4	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 28 FT.
4	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE C
60	FOOT	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER
8	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED
4	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED
4	EACH	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
8	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM
2	EACH	INDUCTIVE LOOP DETECTOR
220	FOOT	DETECTOR LOOP, TYPE I
2	EACH	LIGHT DETECTOR
1	EACH	LIGHT DETECTOR AMPLIFIER
8	EACH	PEDESTRIAN PUSH-BUTTON
1	EACH	TEMPORARY TRAFFIC SIGNAL INSTALLATION
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
5	EACH	REMOVE EXISTING HANDHOLE
9	EACH	REMOVE EXISTING CONCRETE FOUNDATION
4	EACH	PAINT NEW COMBINATION MAST ARM AND POLE, UNDER 40 FT
1	EACH	PAINT NEW SIGNAL POST
1	EACH	TEMPORARY TRAFFIC SIGNAL TIMING
1	EACH	SERVICE INSTALLATION, POLE MOUNTED
1	EACH	UNINTERRUPTABLE POWER SUPPLY
434	FOOT	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C
209	FOOT	ELECTRIC CABLE IN CONDUIT, NO. 20 3/C, TWISTED, SHIELDED

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PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS

NO.	BY	DATE	DESCRIPTION
1	THK/DMS	1-22-10	PER IDOT REVIEW
2	THK/DMS	2-8-10	PER IDOT REVIEW

CABLE PLAN, PHASE DESIGNATION DIAGRAM, EVP SEQUENCE AND SCHEDULE OF QUANTITIES CENTRAL AVE AT 24TH ST

PROJECT NO.	SCALE
05043	NONE
DRAWN/DESIGNED	DATE
JFP/THK-DMS	DEC., 2009
CHECKED/APPROVED	FIELD BOOK NO.
TPG/THK	FILE

SHEET 36 OF 51 SHEETS

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	37
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63455
4

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE AGENCY LISTED BELOW. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE LISTED AGENCY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

AGENCY: TOWN OF CICERO

1 EACH CONTROLLER AND CABINET, COMPLETE
1 EACH LIGHT DETECTOR

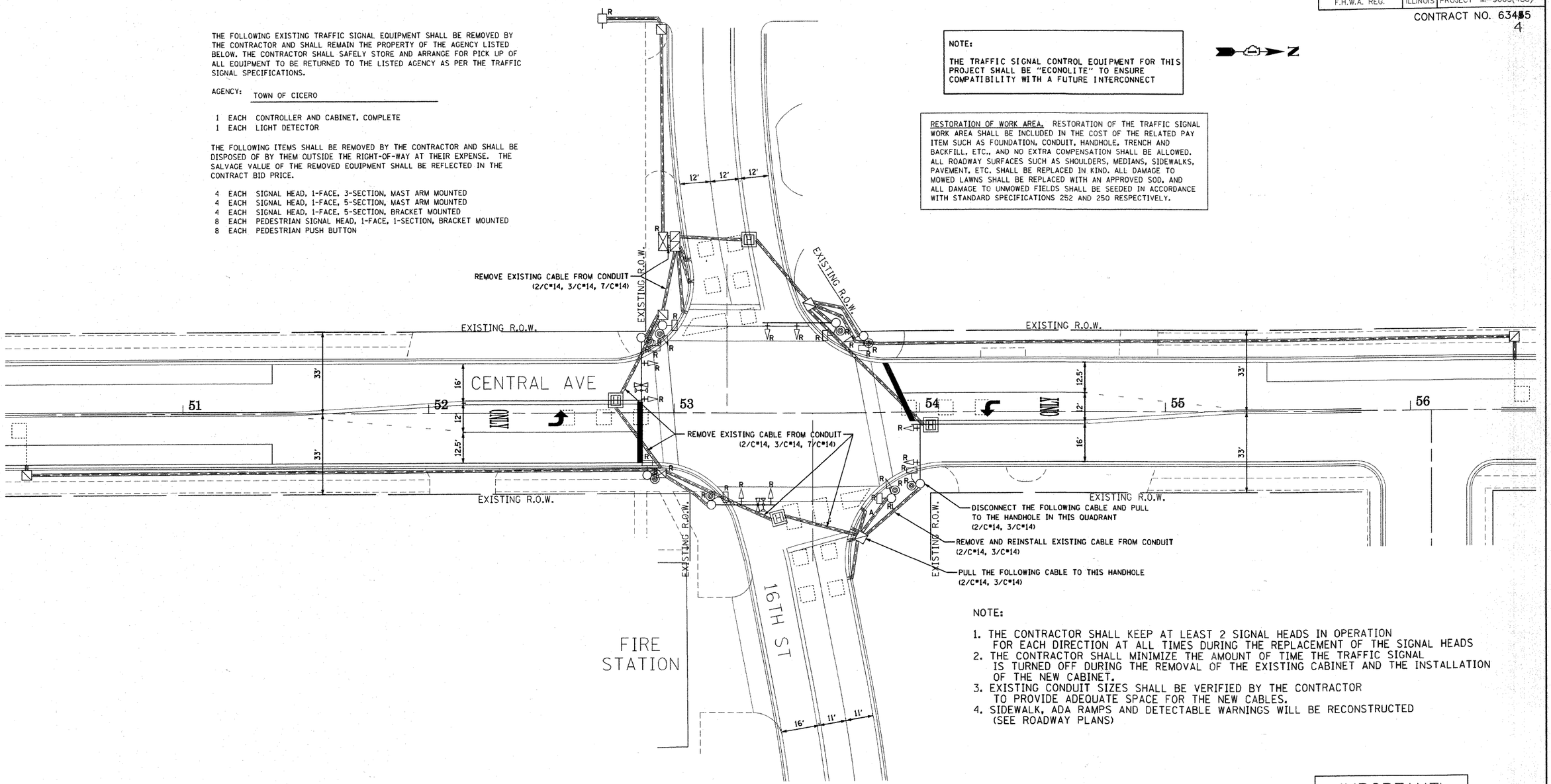
THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

4 EACH SIGNAL HEAD, 1-FACE, 3-SECTION, MAST ARM MOUNTED
4 EACH SIGNAL HEAD, 1-FACE, 5-SECTION, MAST ARM MOUNTED
4 EACH SIGNAL HEAD, 1-FACE, 5-SECTION, BRACKET MOUNTED
8 EACH PEDESTRIAN SIGNAL HEAD, 1-FACE, 1-SECTION, BRACKET MOUNTED
8 EACH PEDESTRIAN PUSH BUTTON

NOTE:

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO ENSURE COMPATIBILITY WITH A FUTURE INTERCONNECT

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE COST OF THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



NOTE:

1. THE CONTRACTOR SHALL KEEP AT LEAST 2 SIGNAL HEADS IN OPERATION FOR EACH DIRECTION AT ALL TIMES DURING THE REPLACEMENT OF THE SIGNAL HEADS
2. THE CONTRACTOR SHALL MINIMIZE THE AMOUNT OF TIME THE TRAFFIC SIGNAL IS TURNED OFF DURING THE REMOVAL OF THE EXISTING CABINET AND THE INSTALLATION OF THE NEW CABINET.
3. EXISTING CONDUIT SIZES SHALL BE VERIFIED BY THE CONTRACTOR TO PROVIDE ADEQUATE SPACE FOR THE NEW CABLES.
4. SIDEWALK, ADA RAMPS AND DETECTABLE WARNINGS WILL BE RECONSTRUCTED (SEE ROADWAY PLANS)

IMPORTANT!
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

KLOA
Konig,Lindgren,O'Hara,Aboona,Inc.
9575 West Higgins Road, Suite 400
Rosemont, Illinois 60018
P: (847) 518-9990 F: (847) 518-9987
PROJECT # 09-158

Civil Engineers
Frank Novotny & Associates, Inc.
825 Midway Drive ◆ Willowbrook, IL ◆ 60527 ◆ Telephone: (630) 887-8640 ◆ Fax: (630) 887-0132
Illinois Professional Design Firm No. 184-000928

PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS

NO.	BY	DATE	DESCRIPTION
1	THK/DMS	1-22-10	PER IDOT REVIEW

TRAFFIC SIGNAL REMOVAL PLAN
CENTRAL AVE AT 16TH ST

PROJECT NO.	SCALE
05043	1"=20'
DRAWN/DESIGNED	DATE
JFP/THK-DMS	DEC., 2009
CHECKED/APPROVED	FIELD BOOK NO.
TPG/THK	FILE

SHEET 37 OF 51 SHEETS

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	38
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63455
4

(A) - GROUND EXISTING HANDHOLE FRAME AND COVER

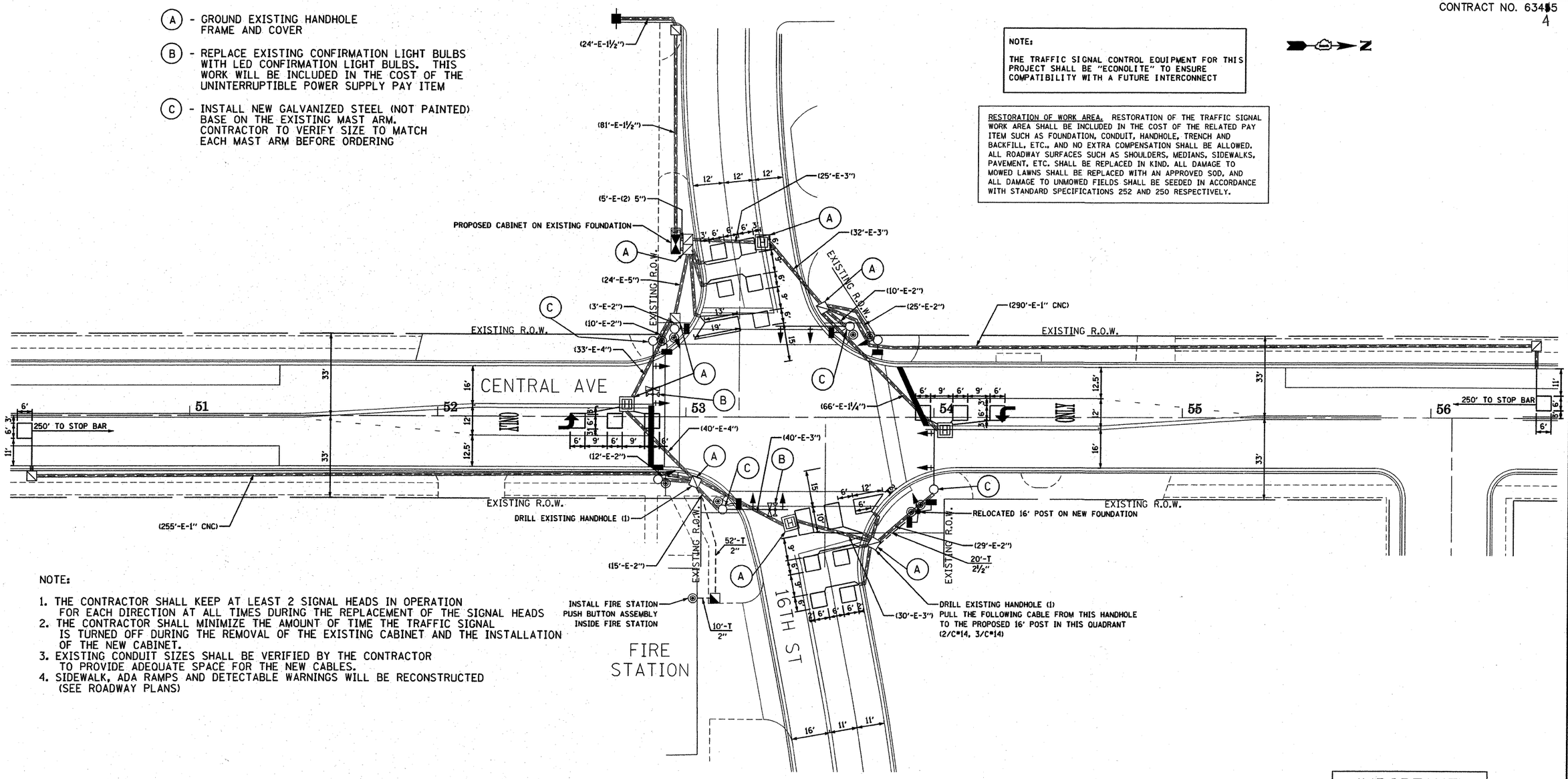
(B) - REPLACE EXISTING CONFIRMATION LIGHT BULBS WITH LED CONFIRMATION LIGHT BULBS. THIS WORK WILL BE INCLUDED IN THE COST OF THE UNINTERRUPTIBLE POWER SUPPLY PAY ITEM

(C) - INSTALL NEW GALVANIZED STEEL (NOT PAINTED) BASE ON THE EXISTING MAST ARM. CONTRACTOR TO VERIFY SIZE TO MATCH EACH MAST ARM BEFORE ORDERING

NOTE:

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO ENSURE COMPATIBILITY WITH A FUTURE INTERCONNECT

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE COST OF THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



NOTE:

1. THE CONTRACTOR SHALL KEEP AT LEAST 2 SIGNAL HEADS IN OPERATION FOR EACH DIRECTION AT ALL TIMES DURING THE REPLACEMENT OF THE SIGNAL HEADS
2. THE CONTRACTOR SHALL MINIMIZE THE AMOUNT OF TIME THE TRAFFIC SIGNAL IS TURNED OFF DURING THE REMOVAL OF THE EXISTING CABINET AND THE INSTALLATION OF THE NEW CABINET.
3. EXISTING CONDUIT SIZES SHALL BE VERIFIED BY THE CONTRACTOR TO PROVIDE ADEQUATE SPACE FOR THE NEW CABLES.
4. SIDEWALK, ADA RAMPS AND DETECTABLE WARNINGS WILL BE RECONSTRUCTED (SEE ROADWAY PLANS)

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IMPORTANT!
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES INDICATED IN TITLE BLOCK.

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PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS			
NO.	BY	DATE	DESCRIPTION
1	THK/DMS	1-22-10	PER IDOT REVIEW
2	THK/DMS	2-8-10	PER IDOT REVIEW

TRAFFIC SIGNAL MODIFICATION PLAN CENTRAL AVE AT 16TH ST

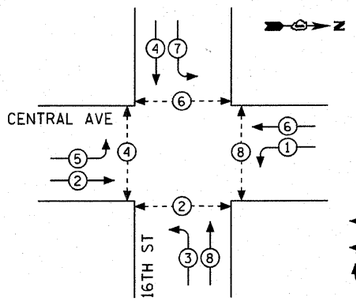
PROJECT NO. 05043	SCALE 1"=20'	SHEET 38 OF 51 SHEETS	
DRAWN/DESIGNED JFP/THK-DMS	DATE DEC., 2009		
CHECKED/APPROVED TPG/THK	FIELD BOOK NO. FILE		

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	39
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63485

4

CONTROLLER SEQUENCE

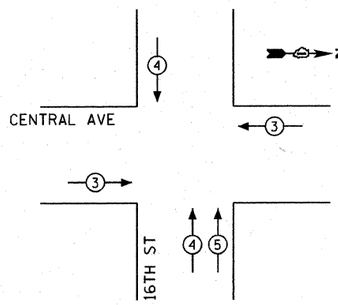


PHASE DESIGNATION DIAGRAM

LEGEND

- DUAL ENTRY PHASE
- SINGLE ENTRY PHASE
- OVERLAP
- PEDESTRIAN PHASE
- NUMBER REFERS TO ASSOCIATED PHASE

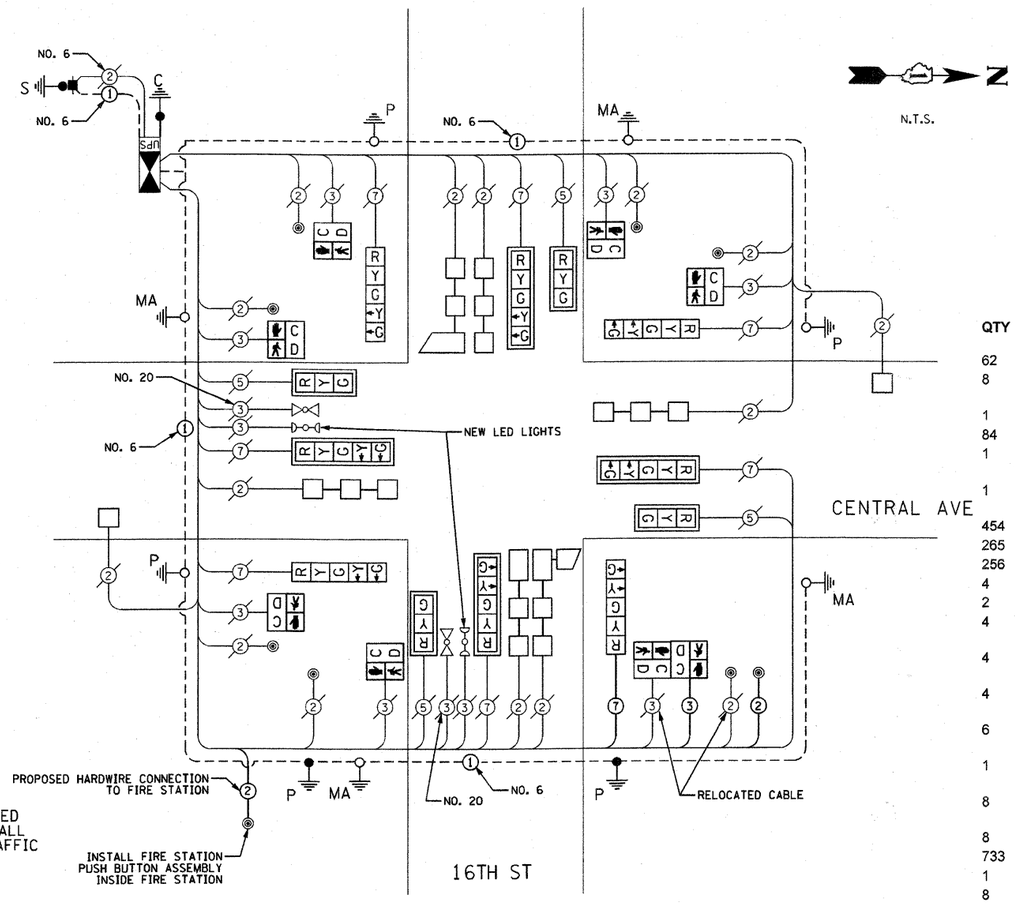
EMERGENCY VEHICLE PREEMPTION SEQUENCE



PROPOSED EMERGENCY VEHICLE PREEMPTORS			
EMERGENCY VEHICLE PREEMPTOR	3	4	5
MOVEMENT	←	↓ ↑	↑

EMERGENCY VEHICLE PREEMPTOR "5" SHALL ONLY BE ACTIVATED FROM PUSHBUTTON IN FIRE STATION AND WILL INITIATE AN ALL-RED CLEARANCE PHASE FOLLOWED BY A GREEN BALL AND LEFT TURN ARROW FOR WESTBOUND TRAFFIC

PROPOSED CABLE PLAN



NOTE:

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO ENSURE COMPATIBILITY WITH A FUTURE INTERCONNECT

SCHEDULE OF QUANTITIES

QTY	UNIT	ITEM DESCRIPTION
62	FOOT	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL
8	FOOT	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL
1	EACH	HANDHOLE
84	FOOT	TRENCH AND BACKFILL FOR ELECTRICAL WORK
1	EACH	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION
1	EACH	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL
454	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
265	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
256	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
4	FOOT	CONCRETE FOUNDATION, TYPE A
2	EACH	DRILL EXISTING HANDHOLE
4	EACH	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED
4	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED
4	EACH	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED
6	EACH	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
1	EACH	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
8	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM
8	EACH	INDUCTIVE LOOP DETECTOR
733	FOOT	DETECTOR LOOP, TYPE I
1	EACH	LIGHT DETECTOR AMPLIFIER
8	EACH	PEDESTRIAN PUSH-BUTTON
1	EACH	RELOCATE EXISTING TRAFFIC SIGNAL POST
657	FOOT	REMOVE ELECTRIC CABLE FROM CONDUIT
104	FOOT	REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
1	EACH	REMOVE EXISTING CONCRETE FOUNDATION
4	EACH	BASE COVER, LIGHT POLE
1	EACH	SERVICE INSTALLATION, POLE MOUNTED
8	EACH	GROUNDING EXISTING HANDHOLE FRAME AND COVER
1	EACH	UNINTERRUPTABLE POWER SUPPLY
585	FOOT	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C
1	EACH	FIRE PREEMPTOR SWITCH

I.D.O.T
TRAFFIC SIGNAL INSTALLATION
ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. LAMPS	WATTAGE INCAND.	WATTAGE LED	%OPERATION	TOTAL WATTAGE
SIGNAL (RED)	12		17	0.50	102.0
(YELLOW)	12		25	0.25	75.0
(GREEN)	12		15	0.25	45.0
ARROW	16		12	0.10	19.2
PED. SIGNAL	8		25	1.00	200.0
CONTROLLER	1		100	1.00	100.0
ILLUM. SIGN				0.05	
FLASHER				0.50	
				TOTAL =	541.2

ENERGY COSTS TO:
TOWN OF CICERO
4949 WEST CERMAK ROAD
CICERO, ILLINOIS 60804
ENERGY SUPPLY CONTACT: MIKE BELL
PHONE: (708) 410-5314
COMPANY: COMED

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PROJECT

TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS

NO.	BY	DATE	DESCRIPTION
1	THK/DMS	1-22-10	PER IDOT REVIEW

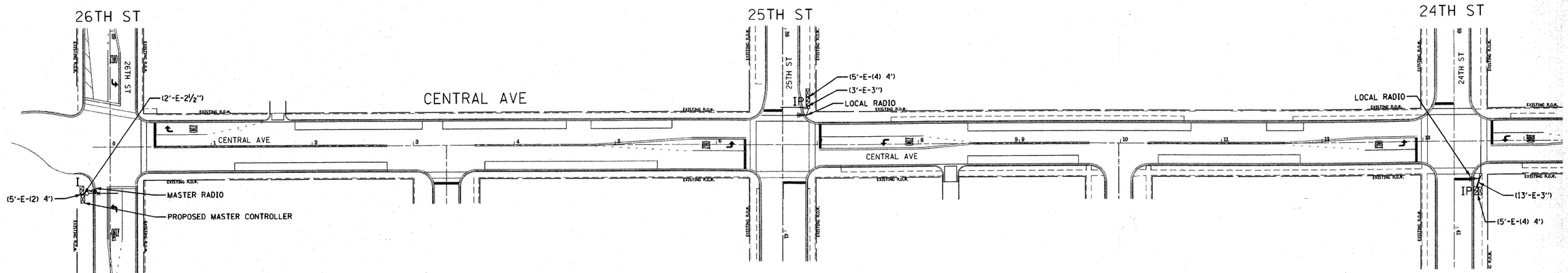
CABLE PLAN, PHASE DESIGNATION DIAGRAM, EVP SEQUENCE AND SCHEDULE OF QUANTITIES CENTRAL AVE AT 16TH ST

PROJECT NO. 05043	SCALE NONE
DRAWN/DESIGNED JFP/THK-DMS	DATE DEC., 2009
CHECKED/APPROVED TPG/THK	FIELD BOOK NO. FILE

SHEET 39 OF 51 SHEETS

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	40
F.H.W.A. REG.	ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63405
4



NOTE:

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RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE COST OF THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

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PROJECT
TOWN OF CICERO, ILLINOIS
CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS

NO.	BY	DATE	DESCRIPTION
1	THK/DMS	1-22-10	PER IDOT REVIEW

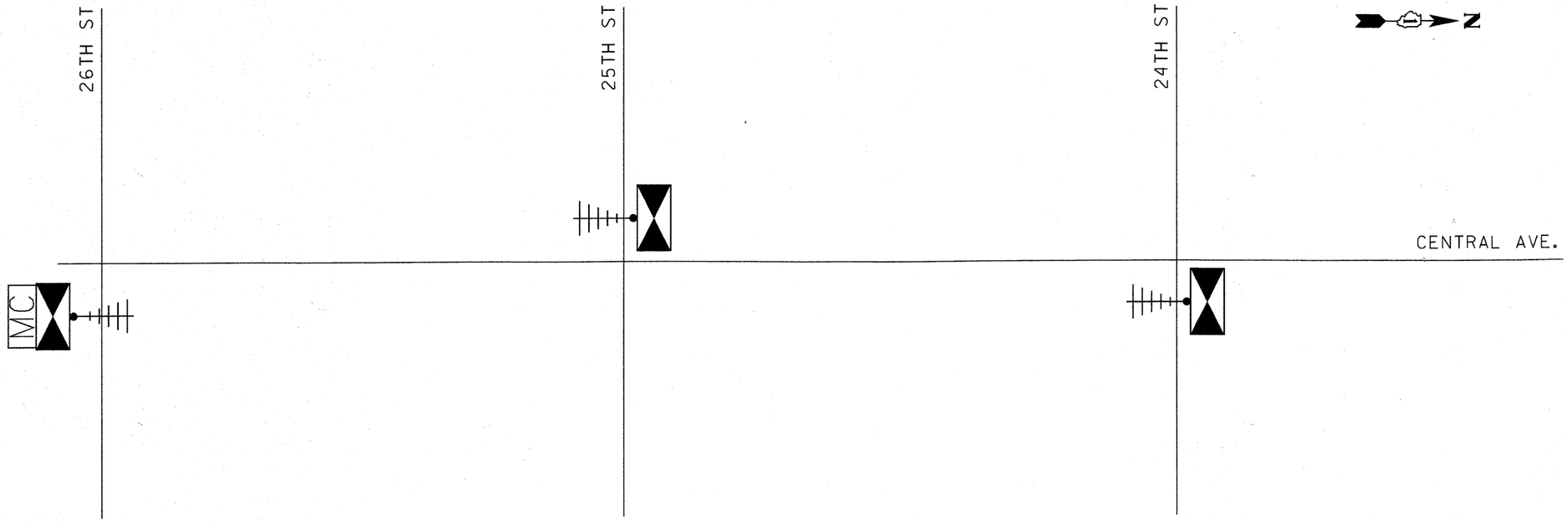
INTERCONNECT PLANS
CENTRAL AVE
(26TH ST TO 24TH ST)

PROJECT NO.	SCALE
05043	1"=50'
DRAWN/DESIGNED	DATE
JFP/THK-DMS	DEC., 2009
CHECKED/APPROVED	FIELD BOOK NO.
TPG/THK	FILE

SHEET **40** OF **51** SHEETS

FAU RTE	SECTION		COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP		COOK	51	41
F.H.W.A. REG.		ILLINOIS	PROJECT	M-9003(488)	

CONTRACT NO. 63485
4



SCHEDULE OF QUANTITIES

QTY	UNIT	ITEM DESCRIPTION
1	EACH	RADIO INTERCONNECT SYSTEM COMPLETE, MASTER
2	EACH	RADIO INTERCONNECT SYSTEM COMPLETE, LOCAL
1	EACH	OPTIMIZE TRAFFIC SIGNAL SYSTEM

NOTE:

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CENTRAL AVENUE RESURFACING
SECTION 03-00193-00-FP

REVISIONS

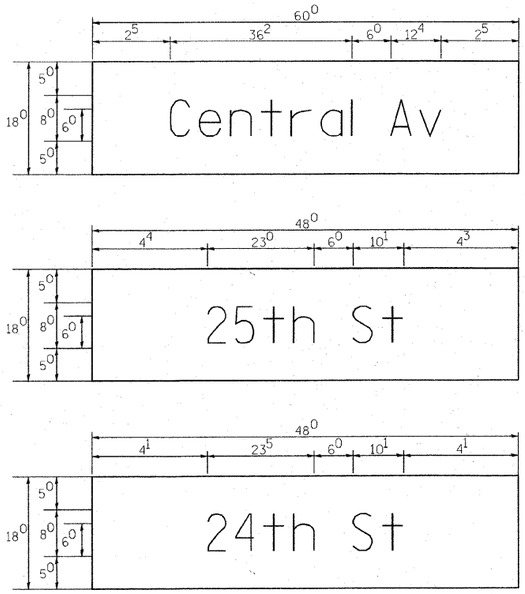
NO.	BY	DATE	DESCRIPTION
1	THK/DMS	1-22-10	PER IDOT REVIEW

INTERCONNECT SCHEMATIC
CENTRAL AVE
(26TH ST TO 24TH ST)

PROJECT NO. 05043	SCALE NONE
DRAWN/DESIGNED JFP/THK-DMS	DATE DEC., 2009
CHECKED/APPROVED TPG/THK	FIELD BOOK NO. FILE

SHEET **41** OF **51** SHEETS

PANEL SIGN DESIGN TYPE 1



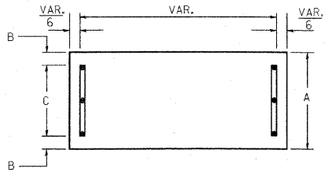
___ Sq. M. each
7.5 Sq. Ft. each
4 Required
Design Series D

___ Sq. M. each
6.0 Sq. Ft. each
2 Required
Design Series D

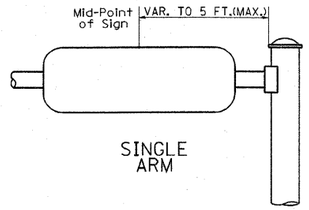
___ Sq. M. each
6.0 Sq. Ft. each
2 Required
Design Series D

NOTE: SIGN DIMENSIONS ARE IN ENGLISH UNITS

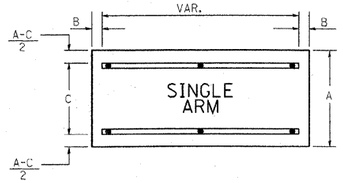
SUPPORTING CHANNELS



A	B	C
18"	2"	14"



SUPPORTING CHANNELS



A	B	C
18"	2"	12"
30"	2"	22"

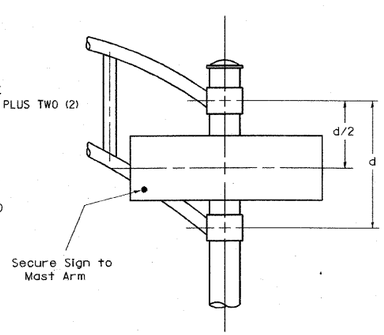
GENERAL NOTES

1. WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 834001, 834006 AND 834011, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 6'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
2. ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND, TYPE A SHEETING.
3. THE SIGN LENGTH SHOULD BE INCREASED IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHOULD NOT EXCEED 6'-0".
4. ALL BORDERS SHALL BE ¾" WIDE AND CORNER RADIUS SHALL BE 2-1/4 ".
5. SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS. LOCAL SUPPLIERS OF THE SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM ARE:
   - * A.K.T. CORPORATION, SCHAUMBURG, IL
   - * TUCKER COMPANY, INC., WAUWATOSA, WI
   - * AMERICAN FABRICATION CO., CHICAGO HEIGHTS, IL
   - * WESTERN TRAFFIC CONTROL INC., CICERO, IL

PARTS LISTING:

SIGN CHANNEL	PART #HPN053 (MED. CHANNEL)
SIGN SCREWS	¼" x 14 x 1" H.W.H. #3 SELF TAPPING WITH NEOPRENE WASHER
BRACKETS	PART #HPN034 (UNIVERSAL) CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BRACKET OF THE ABOVE PRODUCT.



DUAL ARM

SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM
Shall be used. See Note #5.

Upper Case To Lower Case
Spacing Chart 8-6 Inch Series "C & D"

FIRST LETTER \ SECOND LETTER	a c d e g o q	b h i k l m n p r u	f w	j	s t	v y	x	z
SERIES	C D	C D	C D	C D	C D	C D	C D	C D
A W X	1^2 1^4	1^4 1^5	1^2 1^4	0^6 1^0	1^1 1^4	0^6 1^0	1^1 1^2	1^2 1^4
B	1^4 1^5	2^0 2^1	1^4 1^5	1^1 1^2	1^4 1^5	1^2 1^4	1^2 1^4	1^6 1^7
C E G	1^4 1^5	2^0 2^1	1^2 1^4	0^6 1^0	1^2 1^4	1^2 1^4	1^4 1^5	1^4 1^5
D O Q R	1^4 1^5	2^0 2^1	1^4 1^5	0^6 1^0	1^2 1^4	1^2 1^4	1^4 1^5	1^4 1^5
F	0^5 0^6	1^4 1^5	0^6 1^0	0^5 0^6	0^6 1^0	0^6 1^0	1^1 1^2	
H I M N	2^0 2^1	2^2 2^4	2^0 2^1	1^4 1^5	1^6 1^7	1^6 1^7	2^0 2^1	2^0 2^1
J U	2^0 2^1	2^0 2^1	1^6 1^7	1^4 1^5	1^6 1^7	1^6 1^7	1^6 1^7	2^0 2^1
K L	1^1 1^2	1^6 1^7	1^1 1^2	0^5 0^6	1^1 1^2	1^1 1^2	1^1 1^2	1^2 1^4
P	1^2 1^4	1^4 1^5	1^2 1^4	0^5 0^6	1^1 1^2	1^1 1^2	1^2 1^4	1^2 1^4
S	1^2 1^4	1^6 1^7	1^2 1^4	0^6 1^0	1^2 1^4	1^2 1^4	1^2 1^4	1^2 1^4
T	1^1 1^2	1^6 1^7	0^6 1^0	0^6 1^0	1^1 1^2	1^1 1^2	1^1 1^2	1^2 1^4
V	0^6 1^0	1^4 1^5	1^1 1^2	0^6 1^0	1^2 1^4	1^2 1^4	1^2 1^4	1^2 1^4
Y	0^5 0^6	1^4 1^5	0^6 1^0	0^5 0^6	0^5 0^7	0^5 0^6	0^6 1^0	1^1 1^2
Z	1^6 1^7	2^2 2^4	1^6 1^7	1^2 1^4	1^6 1^7	1^6 1^7	1^6 1^7	2^0 2^1

Lower Case To Lower Case
Spacing Chart 6 Inch Series "C & D"

FIRST LETTER \ SECOND LETTER	a c d e g o q	b h i k l m n p r u	f w	j	s t	v y	x	z
SERIES	C D	C D	C D	C D	C D	C D	C D	C D
a d h g i j l m n q u	1^6 1^7	2^2 2^4	1^6 1^7	1^2 1^4	1^4 1^5	1^4 1^5	1^6 1^7	1^6 1^7
b f k o p s	1^2 1^4	1^6 1^7	1^1 1^2	0^5 0^6	1^1 1^2	1^1 1^2	1^2 1^4	1^2 1^4
c e	1^2 1^4	1^6 1^7	1^2 1^4	0^6 1^0	1^2 1^4	1^2 1^4	1^2 1^4	1^2 1^4
r	0^6 1^0	1^2 1^4	0^6 1^0	0^3 0^3	0^5 0^6	0^5 0^6	1^0 0^6	1^0 0^6
t z	1^2 1^4	1^6 1^7	1^2 1^4	0^6 1^0	1^1 1^2	1^1 1^2	1^2 1^4	1^2 1^4
v y	1^1 1^2	1^4 1^5	1^1 1^2	0^5 0^6	0^6 1^0	0^6 1^0	1^1 1^2	1^1 1^2
w	1^1 1^2	1^4 1^5	1^1 1^2	0^5 0^6	1^1 1^2	1^1 1^2	1^1 1^2	1^2 1^4
x	1^2 1^4	1^6 1^7	1^1 1^2	0^5 0^6	1^1 1^2	1^1 1^2	1^2 1^4	1^2 1^4

Number To Number
Spacing Chart 8 Inch Series "C & D"

FIRST NUMBER \ SECOND NUMBER	0	1	2	3	4	5	6	7	8	9
SERIES	C D	C D	C D	C D	C D	C D	C D	C D	C D	C D
0 9	1^6 1^7	1^6 1^7	1^4 1^5	1^2 1^4	1^4 1^5	1^4 1^5	1^6 1^7	1^2 1^4	1^6 1^7	1^6 1^7
1	2^0 2^1	2^0 2^1	2^0 2^1	1^6 1^7	1^4 1^5	2^0 2^1	2^0 2^1	1^4 1^5	2^0 2^1	2^0 2^1
2 3 4	1^4 1^5	1^4 1^5	1^4 1^5	1^2 1^4	1^2 1^4	1^4 1^5	1^4 1^5	1^1 1^2	1^6 1^7	1^4 1^5
5	1^4 1^5	1^4 1^5	1^4 1^5	1^1 1^2	1^1 1^2	1^4 1^5	1^4 1^5	1^1 1^2	1^4 1^5	1^4 1^5
6	1^6 1^7	1^4 1^5	1^4 1^5	1^2 1^5	1^2 1^4	1^4 1^5	1^4 1^5	1^1 1^2	1^4 1^5	1^4 1^5
7	1^2 1^4	1^2 1^4	1^4 1^5	1^2 1^5	0^5 0^6	1^2 1^4	1^4 1^5	1^1 1^2	1^4 1^5	1^2 1^4
8	1^6 1^7	1^6 1^7	1^4 1^5	1^2 1^5	1^2 1^4	1^4 1^5	1^6 1^7	1^2 1^4	1^6 1^7	1^4 1^5

EXAMPLE, $2^{(3)}$ DENOTES $\frac{3}{8}$"

UPPER AND LOWER CASE LETTER WIDTHS

LETTERS	6 INCH UPPER CASE LETTERS SERIES C	D	8 INCH UPPER CASE LETTERS SERIES C	D	LETTERS	6 INCH LOWER CASE LETTERS SERIES C	D
A	3^6	5^0	5^0	6^5	a	3^5	4^2
B	3^2	4^0	4^3	5^3	b	3^5	4^2
C	3^2	4^0	4^3	5^3	c	3^5	4^1
D	3^2	4^0	4^3	5^3	d	3^5	4^2
E	3^0	3^5	4^0	4^7	e	3^5	4^2
F	3^0	3^5	4^0	4^7	f	2^3	2^6
G	3^2	4^0	4^3	5^3	g	3^5	4^2
H	3^2	4^0	4^3	5^3	h	3^5	4^2
I	0^7	0^7	1^1	1^2	i	1^1	1^1
J	3^0	3^6	4^0	5^0	j	2^0	2^2
K	3^2	4^1	4^3	5^4	k	3^5	4^2
L	3^0	3^5	4^0	4^7	l	1^1	1^1
M	3^7	4^5	5^1	6^1	m	6^0	7^0
N	3^2	4^0	4^3	5^3	n	3^5	4^2
O	3^4	4^2	4^5	5^5	o	3^6	4^3
P	3^2	4^0	4^3	5^3	p	3^5	4^2
Q	3^4	4^2	4^5	5^5	q	3^5	4^2
R	3^2	4^0	4^3	5^3	r	2^6	3^2
S	3^2	4^0	4^3	5^3	s	3^6	4^2
T	3^0	3^5	4^0	4^7	t	2^7	3^2
U	3^2	4^0	4^3	5^3	u	3^5	4^2
V	3^5	4^4	4^7	6^0	v	4^2	4^7
W	4^4	5^2	6^0	7^0	w	5^5	6^4
X	3^4	4^0	4^5	5^3	x	4^4	5^1
Y	3^6	5^0	5^0	6^6	y	4^6	5^3
Z	3^2	4^0	4^3	5^3	z	3^6	4^3

NUMBER	6 INCH SERIES C	D	8 INCH SERIES C	D
1	1^2	1^4	1^5	2^0
2	3^2	4^0	4^3	5^3
3	3^2	4^0	4^3	5^3
4	3^5	4^3	4^7	5^7
5	3^2	4^0	4^3	5^3
6	3^2	4^0	4^3	5^3
7	3^2	4^0	4^3	5^3
8	3^2	4^0	4^3	5^3
9	3^2	4^0	4^3	5^3
0	3^4	4^2	4^5	5^5

FILE NAME = ...\signal\17-stname.dgn	USER NAME =	DESIGNED - JHE	REVISED - D.A.Z./D.A.G. 11/90
		DRAWN - RDB	REVISED - D.A.Z./D.A.G. 6/98
	PLOT SCALE =	CHECKED - DAD	REVISED - CADD 10/00
	PLOT DATE = 1/20/2010	DATE -	REVISED -

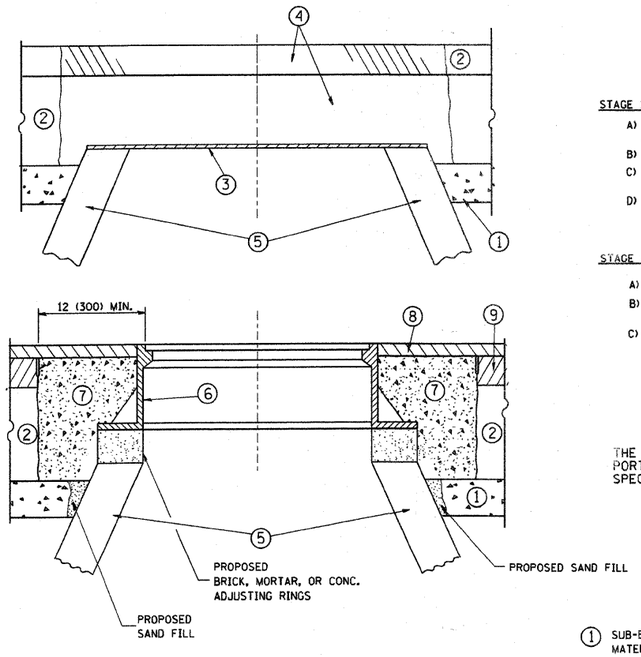
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAST ARM MOUNTED STREET NAME SIGNS

SCALE: NONE | SHEET NO. OF SHEETS | STA. | TO STA.

COUNTY	TOTAL SHEETS	SHEET NO.
COOK 03-00193-00-FP	51	42

CONTRACT NO. 63445



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.

B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.

C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.

D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.

B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.

C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

1. SUB-BASE GRANULAR MATERIAL
2. EXISTING PAVEMENT
3. 36 (900) DIAMETER METAL PLATE
4. PROPOSED CRUSHED STONE AND HMA SURFACE MIX
5. EXISTING STRUCTURE
6. FRAME AND LID (SEE NOTES)
7. CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
8. PROPOSED HMA SURFACE COURSE
9. PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

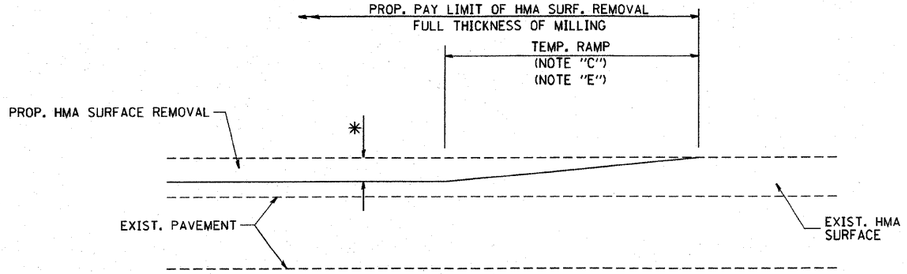
FILE NAME = W:\diststd\22x34\bd08.dgn	USER NAME = gaglionabt	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - R. WIEDEMAN 05-14-04
	PLOT DATE = 1/4/2008	DATE - 10-25-94	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

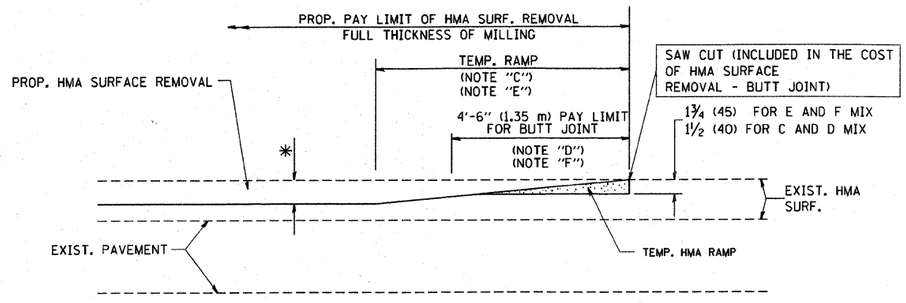
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	43
	BD600-03 (BD-8)	CONTRACT NO. 63445		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	M-9003(488)	



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

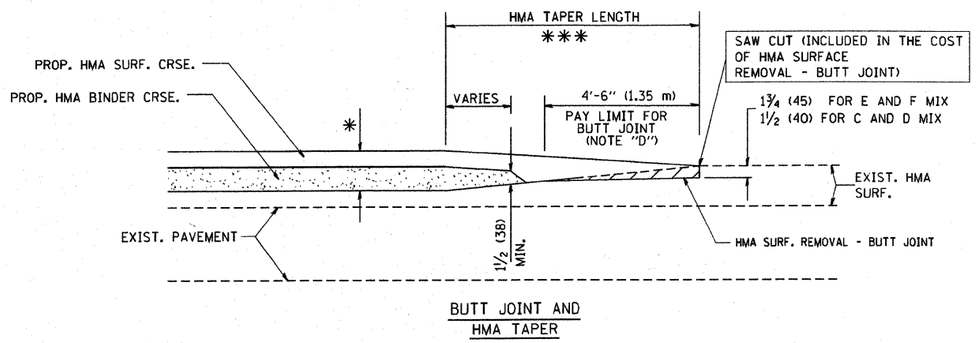


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

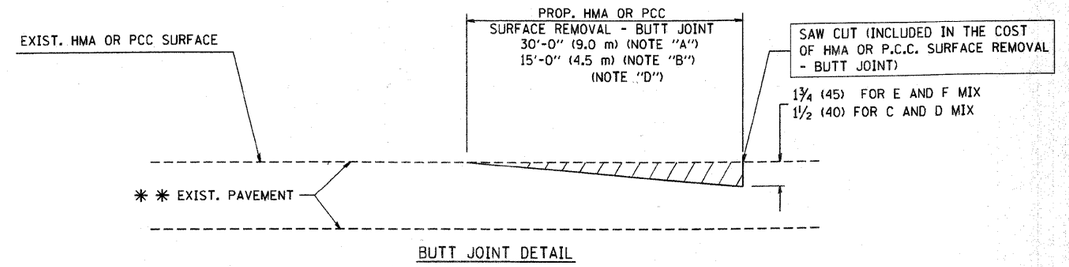
OPTION 2

TYPICAL TEMPORARY RAMP

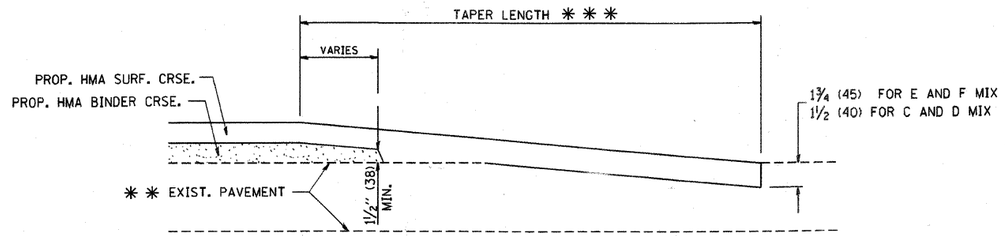


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

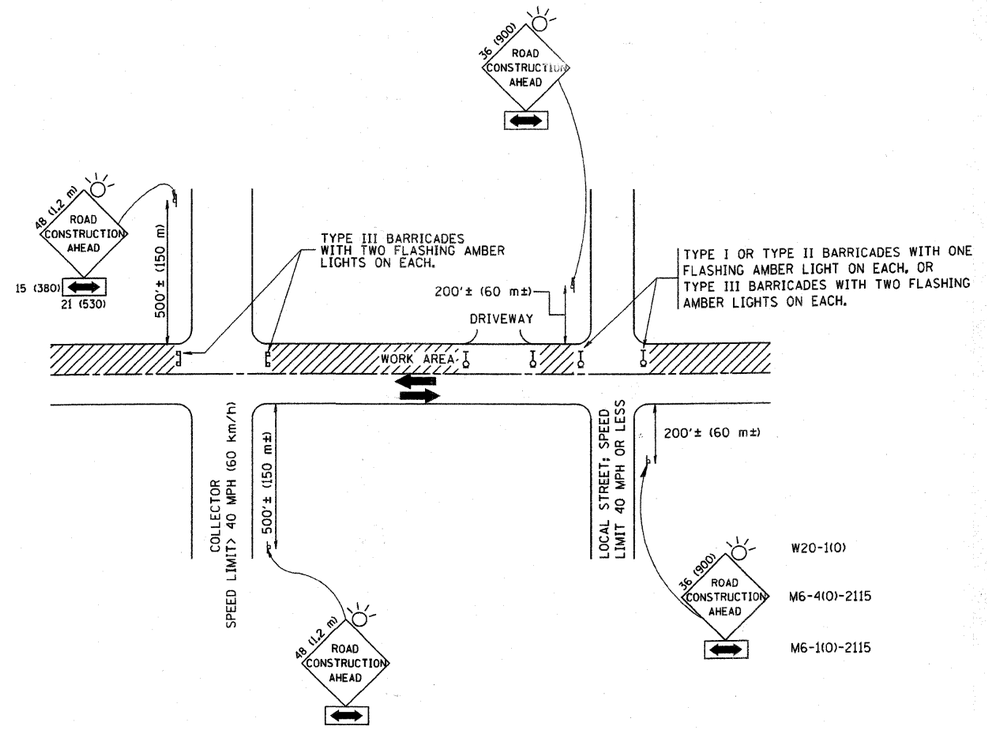
FILE NAME = W:\diststd\22x34\bd32.dgn	USER NAME = gaglianobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	44
	BD400-05 BD32	CONTRACT NO. 63445		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	M-9003(488)	



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

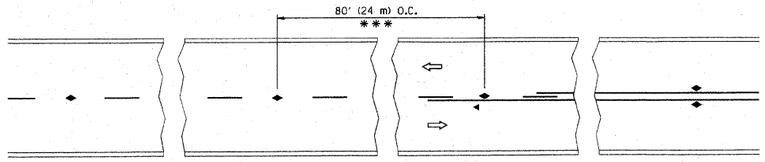
FILE NAME = W:\diststd\22x34\tc10.dgn	USER NAME = gaglianobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED -T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

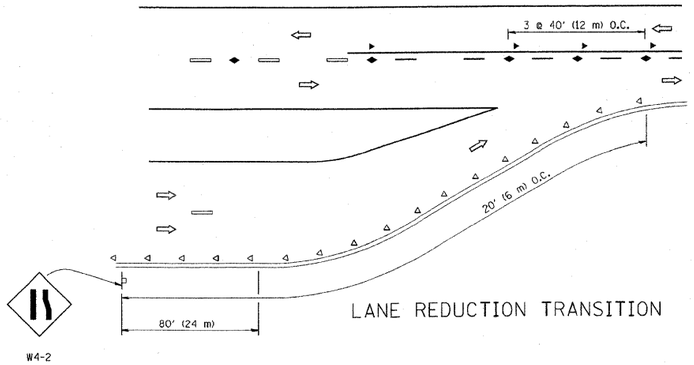
SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	45
	TC-10		CONTRACT NO. 63445	
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	M-9003(488)	

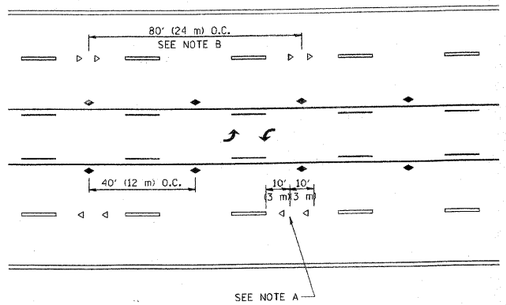


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

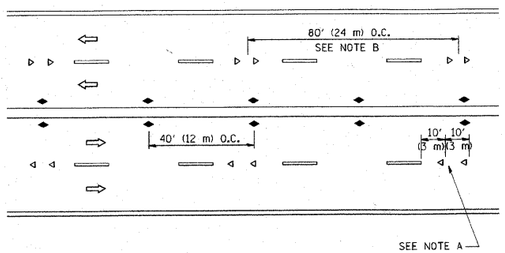
TWO-LANE/TWO-WAY



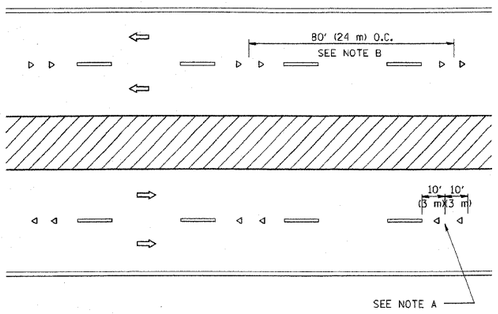
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

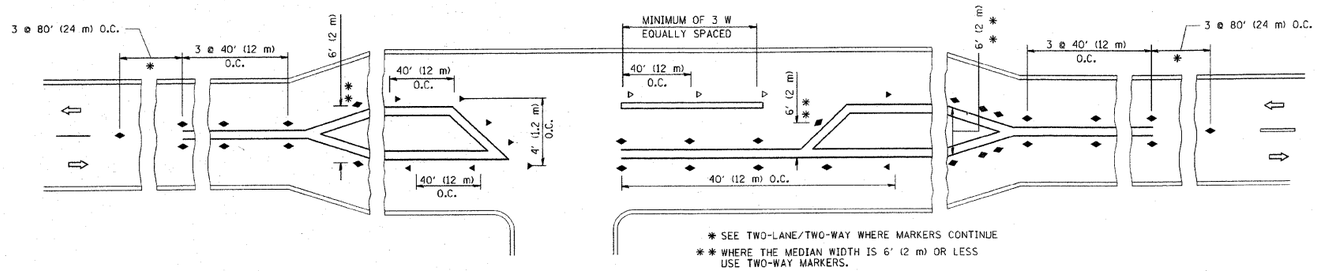
LANE MARKER NOTES

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE

** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

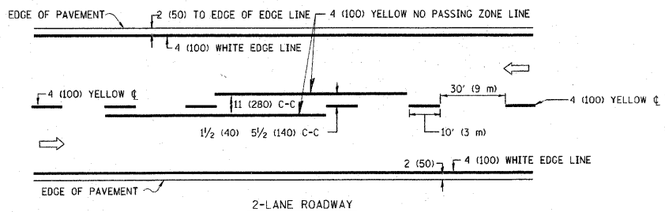
FILE NAME = c:\pw_work\pwidot\drivakosgn\d0108315\tc11.dgn	USER NAME = drivakosgn	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
	PLOT SCALE = 50.000 ' / IN.	DRAWN -	REVISED - T. RAMMACHER 03-12-99
	PLOT DATE = 9/9/2009	CHECKED -	REVISED - T. RAMMACHER 01-06-00
		DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

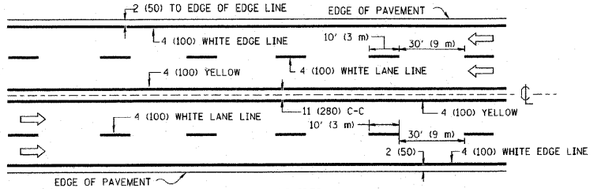
TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

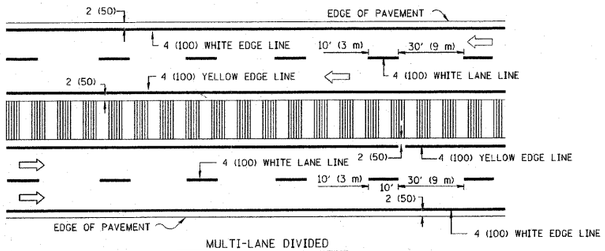
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	46
	TC-11	CONTRACT NO. 62445		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	M-9003(488)	



2-LANE ROADWAY



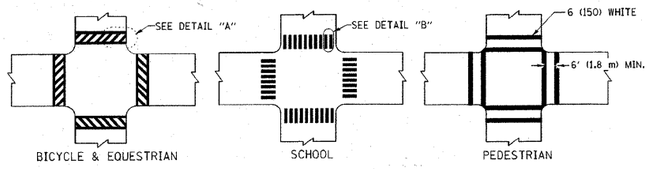
MULTI-LANE UNDIVIDED

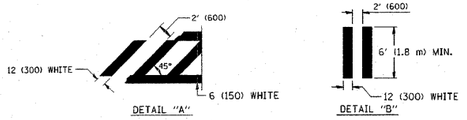


MULTI-LANE DIVIDED
WITH MOUNTABLE MEDIAN

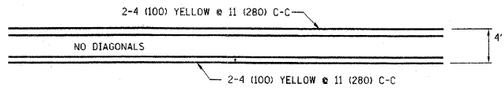
NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

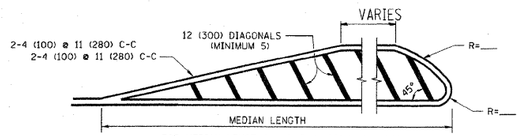




TYPICAL CROSSWALK MARKING



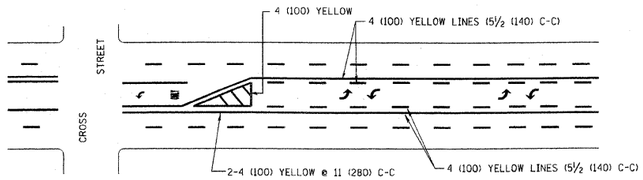
4' (1.2 m) WIDE MEDIANS ONLY



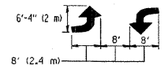
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

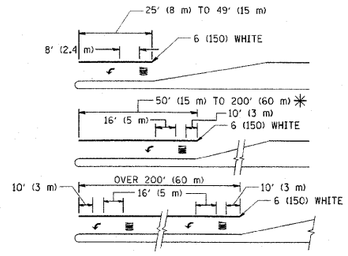


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

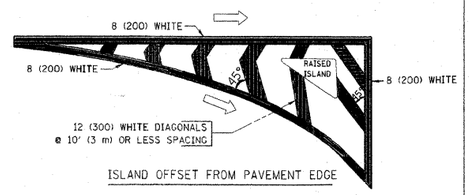


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m^2) **ONLY** AREA = 20.8 SQ. FT. (1.9 m^2)

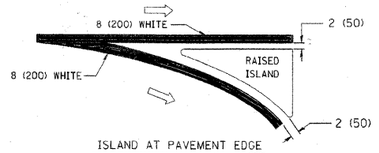
✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

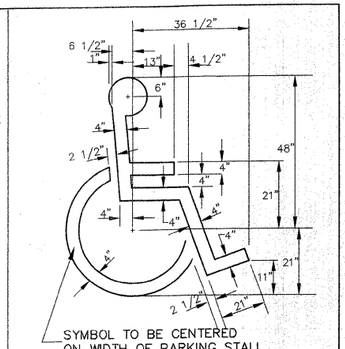


ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



AREA: 6.0 S.F.

HANDICAP SYMBOL

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m^2) EACH "X"=54.0 SQ. FT. (5.0 m^2)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

	LARGE SIZE	SMALL SIZE
THROUGH ARROW	1.07 (11.5)	0.60 (6.5)
LEFT OF RIGHT ARROW	1.47 (15.6)	0.60 (6.5)
COMBINATION LEFT (RIGHT) AND THROUGH ARROW	2.42 (26.0)	1.37 (14.7)
RAILROAD "R" 1.8m (6ft.)	0.33 (3.6)	—
RAILROAD "X" 6.1m (20ft.)	5.02(54.0)	—
HANDICAPPED SYMBOL	0.43 (4.6)	—

All dimensions are in inches (millimeters) unless otherwise shown.

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PLOT SCALE = 50.000 ' / IN.
PLOT DATE = 9/9/2009

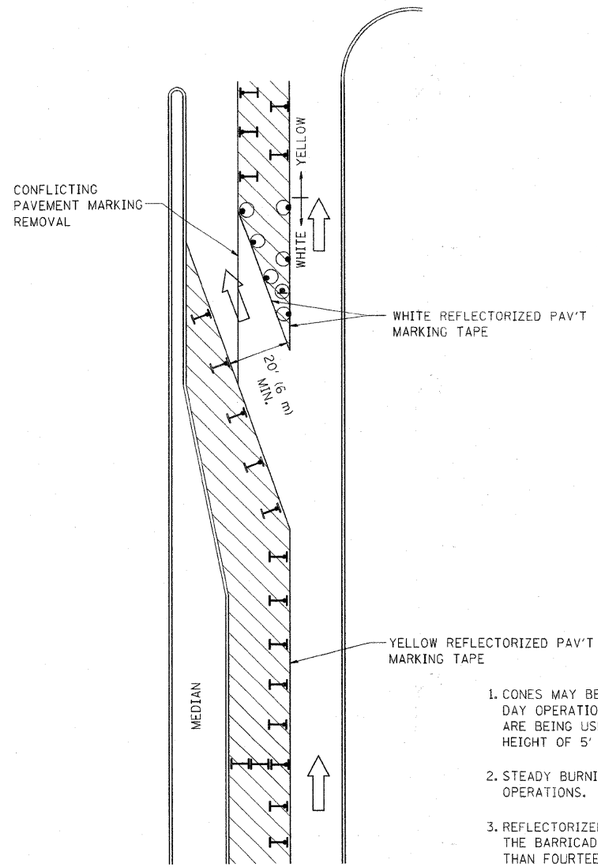
DESIGNED - EVERS
DRAWN -
CHECKED -
DATE - 03-19-90

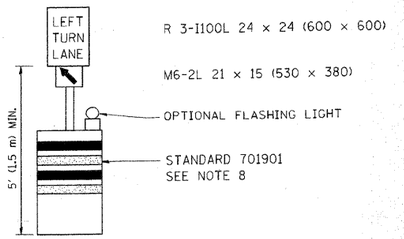
REVISED -T. RAMMACHER 10-27-94
REVISED -C. JUCIUS 09-09-09
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS
SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	47
	TC-13	CONTRACT NO. 62445		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	M-9003(488)	





GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHR 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

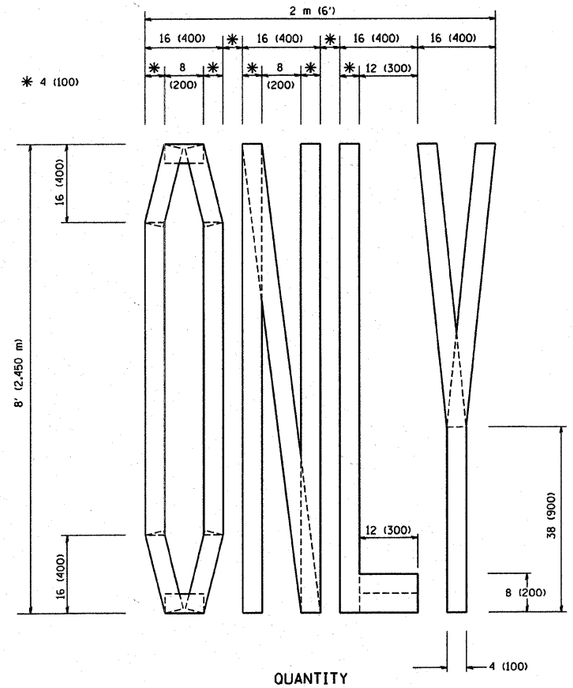
FILE NAME = c:\pw_work\PWIDOT\DRIVAKOSGN\d0100315\tc14.dgn	USER NAME = drivakosgn	REVISED -T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
	PLOT SCALE = 49.9999 ' / IN.	REVISED - A. HOUSEH 11-07-95	REVISED -
	PLOT DATE = 9/14/2009	REVISED - A. HOUSEH 10-12-96	REVISED -
		REVISED -T. RAMMACHER 01-06-00	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

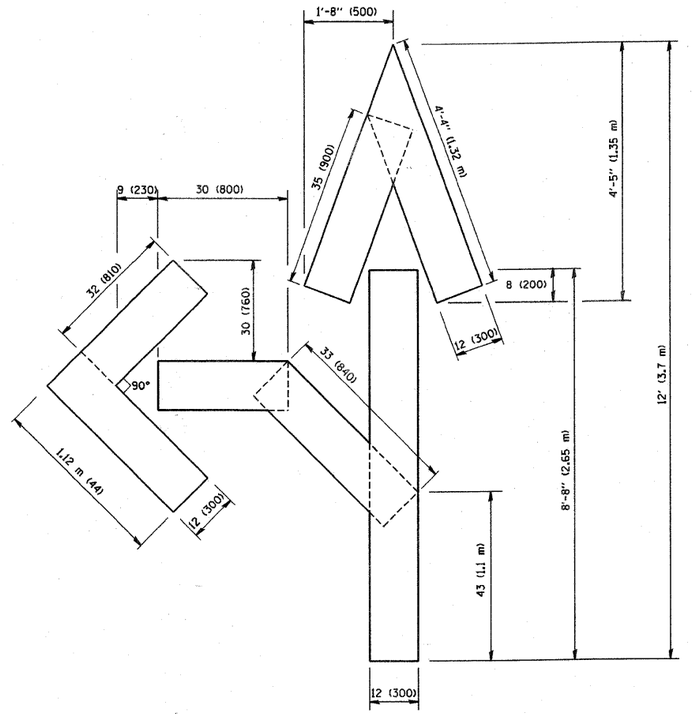
TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

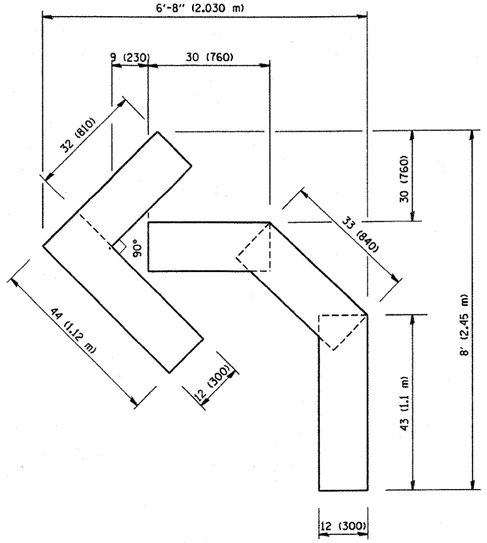
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	48
	TC-14	CONTRACT NO. 63445		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	M-9003(488)	



QUANTITY
4 (100) LINE = 64.1 ft. (19.7 m)
21.1 sq. ft. (1.97 sq. m)



QUANTITY
4 (100) LINE = 82.5 ft. (25.3 m)
27.5 sq. ft. (2.53 sq. m)



QUANTITY
4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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		DRAWN -	REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 1/4/2008	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

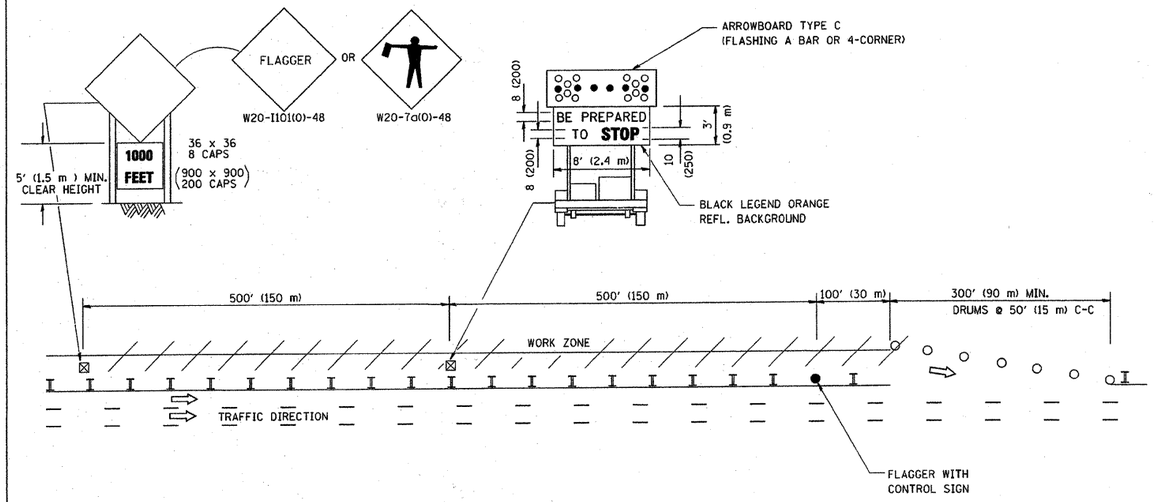
PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

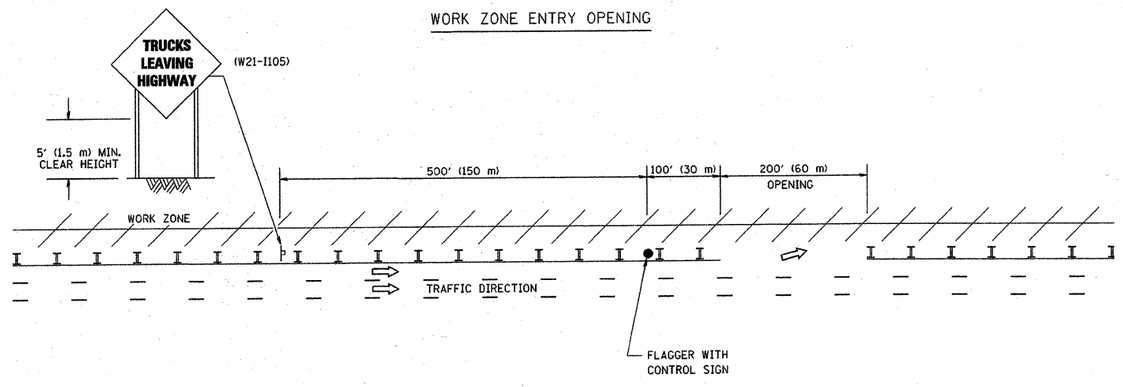
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	49
	TC-16	CONTRACT NO. 62445		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	M-9003(488)	

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. THE ARROWBOARD, THE FLAGGER AHEAD SIGN AND THE TRUCKS LEAVING HIGHWAY SIGN SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
2. WORK ZONE EXIT OPENINGS SHOULD BE A MINIMUM OF ONE HALF MILE APART.
3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

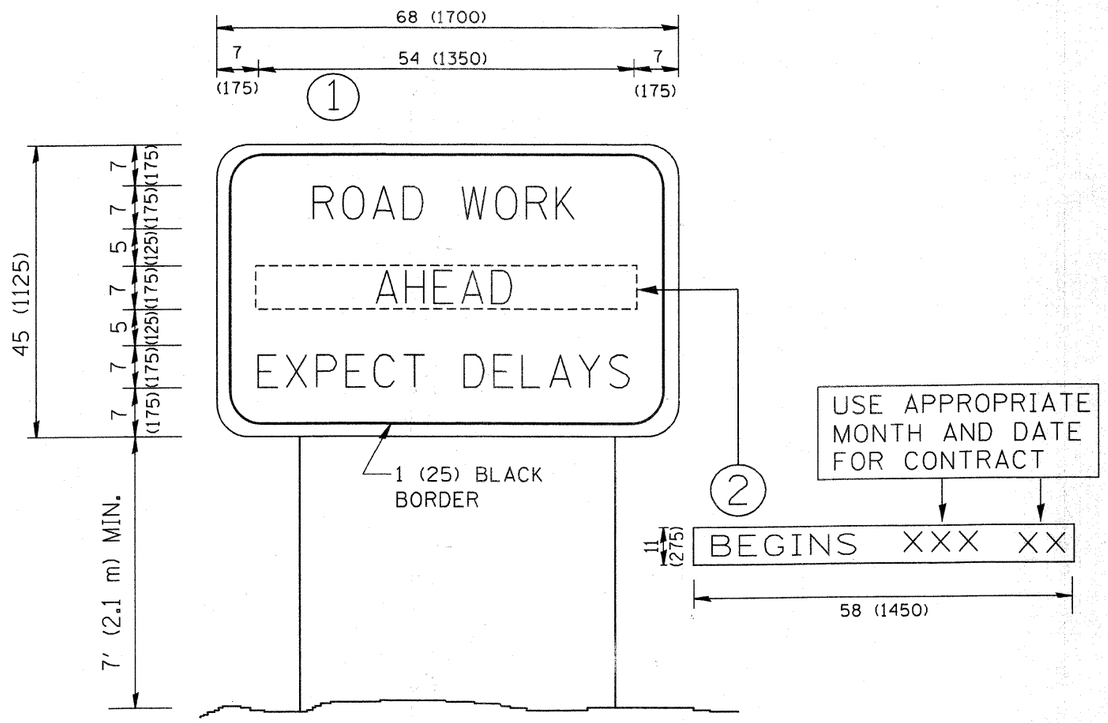
FILE NAME = W:\diststd\22x34\tc18.dgn	USER NAME = leyso	DESIGNED -	REVISED - J.A.F. 04-03
		DRAWN -	REVISED - J.A.F. 02-06
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - S.P.B. 01-07
	PLOT DATE = 1/26/2010	DATE -	REVISED - S.P.B. 12-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	50
	TC-18	CONTRACT NO. 63945		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	M-9003(488)	



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\tc22.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED -T. RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2798	03-00193-00-FP	COOK	51	51
	TC-22	CONTRACT NO. 63445		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	M-9003(488)	