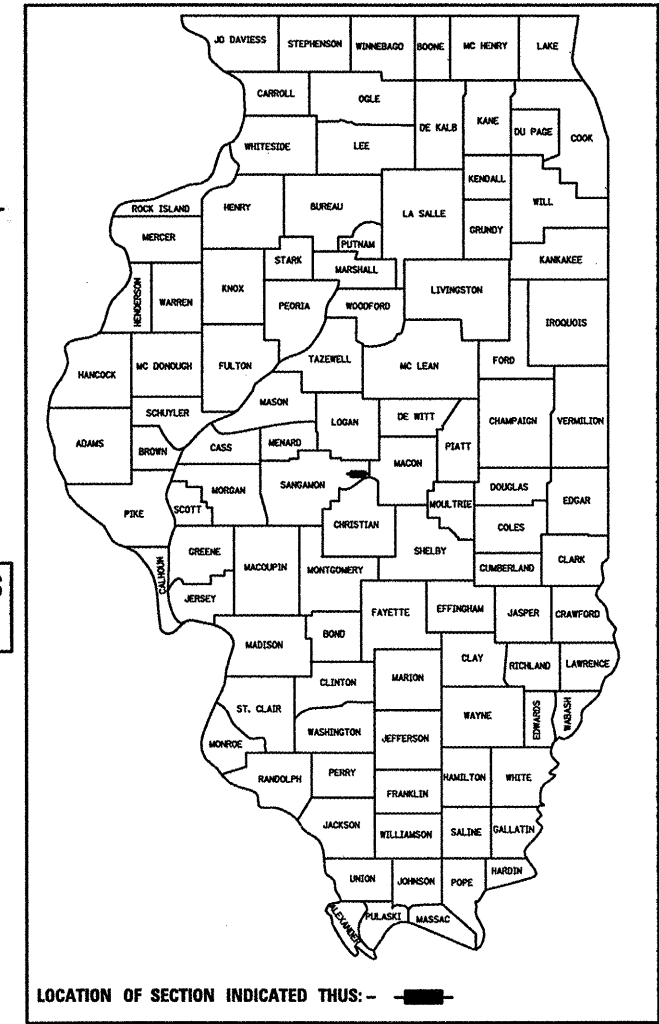


F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
72	*, **	SANGAMON	61	1
		ILLINOIS	CONTRACT NO. 72910	

• (84-10-2)RS-3
 •• (84-10-2)RS-4

D-96-539-04



LOCATION OF SECTION INDICATED THUS: - [shaded box] -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

SUBMITTED Feb 11 20 10
Rey D Park
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

March 19 20 10
Scott E. Stitt, P.E. /a
 ASSISTANT ENGINEER OF DESIGN AND ENVIRONMENT

March 19 20 10
Christine M. Reed /a
 DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
 OF THE STATE OF ILLINOIS

DISTRICT 6, SPRINGFIELD

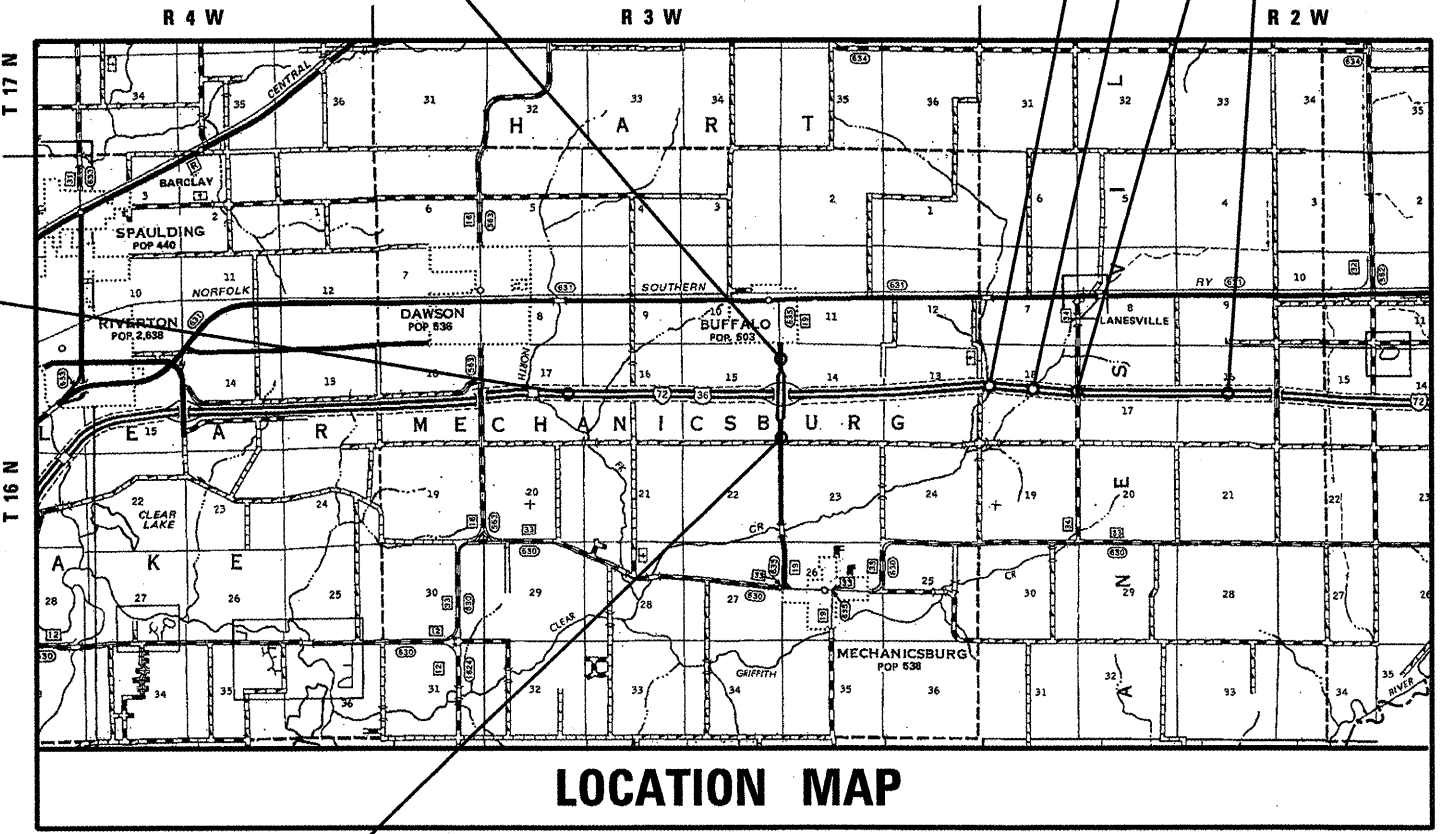
DR
 DAVID R. BOOHER ILLINOIS P.E. 062-043769 DATE 2-10-2010
 EXPIRES 11/30/2011

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS
**PROPOSED
 HIGHWAY PLANS**

F.A.I. ROUTE 72 (I-72)
 SECTION (84-10-2)RS-3, (84-10-3)RS-4

SANGAMON COUNTY

JOB No.
 C-96-141-10
 4TH P.M.



LOCATION MAP

APPROXIMATE SCALE: 0 2 4 MILES

I-72 TOTAL LENGTH = 36,691.0 FEET = 6.95 MILES (I-72)
 NET LENGTH OF PROJECT = 36,691.0 FEET = 6.95 MILES
 I-72 HIGHWAY CLASSIFICATION = INTERSTATE
 ADT = 11,800
 CH 19 TOTAL LENGTH = 3,600 FEET = 0.68 MILES

BEGIN IMPROVEMENTS
 STA. 1932+00 CH 19

BEGIN IMPROVEMENTS
 STA. 171+09 I-72

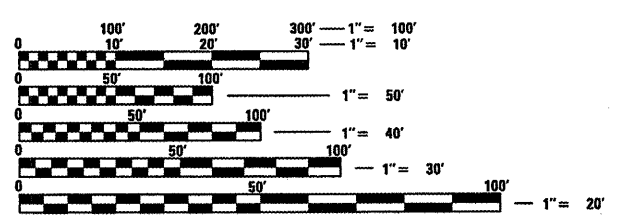
END IMPROVEMENTS
 STA. 1968+00 CH 19



SHEET NO.	DESCRIPTION
1	COVER SHEET
2	GENERAL NOTES
3 - 12	SUMMARY OF QUANTITIES
13 - 21	SCHEDULE OF QUANTITIES
22 - 24	TYPICAL SECTIONS
25 - 28	ALIGNMENT & TIES
29 - 46	PLAN SHEETS
47	BRIDGE APPROACH SHOULDER PAVEMENT DRAIN DETAIL
48 - 50	MAINTENANCE OF TRAFFIC PLANS
51 - 61	STRUCTURE PLANS

000001-05	604106-01	642001-01	701411-06
420001-07	606001-04	701101-02	701451-01
420401-08	606301-04	701106-02	701456
442001-04	630001-08	701201-03	701901-01
442101-07	630301-05	701306-02	704001-06
482011-03	631031-08	701400-04	720011-01
542301-02	635001-01	701401-05	780001-02
604001-03	635006-03	701402-07	781001-03
604101-01	635011-02	701406-05	

ie CONSULTANTS, INC.
 8420 SOUTH SIXTH STREET
 SPRINGFIELD, ILLINOIS 62712
 TEL. (217) 529-8027
 FAX (217) 529-4543
 WWW.IE-CONSULTANTS.COM



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

CALL **J.U.L.I.E.** (JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS)
 48 Hours (2 working days) Before You Dig.
 TOLL FREE: 1 (800) 892-0123 OR 811

CONTRACT NO. 72910

PROJECT ENGINEER: JOHN C. NEGANARD (217) 782-6990 / SENIOR SQUAD LEADER: VICTOR YOUNG (217) 557-7897

GENERAL NOTES

1. ALL ELEVATIONS SHOWN IN THE PLANS ARE U. S. G. S. MEAN SEA LEVEL DATUM.
2. ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS SHALL BE INTERRUPTED TO BE THE LATEST STANDARDS OF THE DEPARTMENT AS SHOWN IN THE PLANS.
3. THE THICKNESS OF HOT-MIX ASPHALT SHOWN ON THE PLANS IS THE NORMAL THICKNESS. DEVIATIONS FROM THE NORMAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HOT-MIX ASPHALT IS PLACED.
4. SEEDING WILL NOT BE PERMITTED AT ANY TIME WHEN THE GROUND IS FROZEN, WET OR IN AN UNTILLABLE CONDITION. AREAS TO BE SEEDED SHALL BE DETERMINED BY THE ENGINEER.
5. EXISTING PAVEMENT DAMAGED DUE TO THE CONTRACTOR'S OPERATIONS, AND NOT OTHER WISE NECESSARY TO REPLACE, SHALL BE REPLACED AT THE EXPENSE OF THE CONTRACTOR.
6. THE LOCATIONS OF THOSE BURIED AND ABOVE GROUND UTILITIES SHOWN ARE APPROXIMATE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS AS OUTLINE IN ARTICLE 107.26 OF THE STANDARD SPECIFICATIONS. THE J. U. L. I. E. NUMBER IS 800-892-0123. A MINIMUM OF FORTY-EIGHT HOURS ADVANCED NOTICE IS REQUIRED.
7. THE EXISTING ROAD SIGNS THAT INTERFERE WITH CONSTRUCTION SHALL BE RELOCATED AS DIRECTED BY THE ENGINEER. AFTER THE CONSTRUCTION IS COMPLETED, THE CONTRACTOR WILL REPLACE THE SIGNS AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
8. ABANDONED UNDERGROUND UTILITIES THAT CONFLICT WITH CONSTRUCTION SHALL BE DISPOSED OUTSIDE THE LIMITS OF RIGHT-OF-WAY ACCORDING TO ARTICLE 202.03 OF THE STANDARDS SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF PAVING AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
9. WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE DEPARTMENT SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKERS AND MONUMENTS UNTIL THE OWNER AND AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION. IF THE ENGINEER DECIDES TO HAVE THE CONTRACTOR RESET THE MONUMENT, THIS WORK WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04.

10. THE FOLLOWING APPLICATION RATES WERE USED FOR QUANTITY CALCULATIONS.

ALL AGGREGATE ITEMS	2.05 TON / CU YD
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N**	112 lbs/sq. yd. Inch
HOT-MIX ASPHALT SURFACE COURSE, MIX "A", N**	112 lbs/sq. yd. Inch
HOT-MIX ASPHALT SHOULDER	112 lbs/sq. yd. Inch
INCIDENTAL BITUMINOUS SURFACE	112 lbs/sq. yd. Inch
HOT-MIX ASPHALT SURFACE COURSE, AGGREGATE (PRIME COAT)	0.056 TON / SQ YD * IN
BITUMINOUS MATERIAL (PRIME COAT) (ON BITUMINOUS)	0.02 TON / SQ YD
BITUMINOUS MATERIAL (PRIME COAT) (ON AGGREGATE)	0.00038 TON / SQ YD
NITROGEN	0.001425 TON / SQ YD
PHOSPHOROUS	90 LBS / ACRE
POTASSIUM	90 LBS / ACRE
MULCH	90 LBS / ACRE
AGRICULTURAL GROUND LIMESTONE	2 TON / ACRE
TEMPORARY EROSION CONTROL SEEDING	2.0 TON / ACRE
RIPRAP	100 LB / ACRE
AGGREGATE SHOULDERS TYPE B	1.50 TON / CU YD
	1.89 TON / CU YD

11. THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT.

MIXTURE USE(S):	SURFACE I-72	BINDER I-72	SHOULDERS I-72	SURFACE & LONG JOINT REPAIR C.H. 19	BINDER C.H. 19
AC/PG	SBS PG-76-22	SBS PG-70-22	PG-64-22	PG-64-22	PG-64-22
DESIGN AIR VOIDS	4.0% @ N DESIGN = 90	4.0% @ N DESIGN = 90	4.0% @ N DESIGN = 50	4.0% @ N DESIGN = 50	4.0% @ N DESIGN = 50
MIXTURE COMPOSITION (GRADATION MIXTURE)	IL 9.5 OR 12.5	IL 19.0	IL 9.5 OR 12.5	IL 9.5 OR 12.5	IL 19.0
FRICITION AGGREGATE	MIX "D"	N/A	MIX "C"	MIX "C"	N/A

12. EXCAVATION, GRADING, SEEDING, FERTILIZER, AND MULCH NECESSARY TO RESTORE THE SIDE SLOPES WILL NOT BE PAID FOR SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE COST OF DRAINAGE ITEMS.

13. THE CONTRACTOR SHALL NOTIFY THE DISTRICT 6 BUREAU OF OPERATIONS AT (217) 785-5312 THREE WEEKS PRIOR TO IMPLEMENTING ANY TRAFFIC CONTROL.

COMMITMENTS

THE FIELD/RESIDENT ENGINEER SHALL CONTACT STUDIES & PLANS CONCERNING ANY MAJOR PLAN CHANGES TO MAKE SURE NO PREVIOUS COMMITMENTS (NOT LISTED) WERE MADE AFFECTING THE DESIGN AND ALLOW AN IMPROVED DESIGN FOR FUTURE PROJECTS.

EXAMINED <u>Feb 3</u> 20 <u>10</u>
<i>[Signature]</i>
PROGRAM IMPLEMENTATION ENGINEER
EXAMINED <u>February 10</u> 20 <u>10</u>
<i>[Signature]</i>
PROGRAM DEVELOPMENT ENGINEER

DISTRICT SIX	
EXAMINED <u>Feb 5</u> 20 <u>10</u>	
<i>[Signature]</i>	
OPERATIONS ENGINEER	

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

100% STATE

DESCRIPTIONS				CONSTRUCTION TYPE CODE			
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY 1000	STRUCTURES I-72 OVER CLEAR CREEK X031-2A	STRUCTURES I-72 OVER CLEAR CREEK TRIBUTARY X028-2A	STRUCTURES CH 34 OVER I-72 X231-2A
28100707	STONE DUMPED RIPRAP, CLASS A4	SQ YD	391	391			
28200200	FILTER FABRIC	SQ YD	391	391			
31101000	SUB-BASE GRANULAR MATERIAL, TYPE B	TON	232	232			
35501332	HOT-MIX ASPHALT BASE COURSE 12"	SQ YD	3,556	3,556			
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	141.7	141.7			
40600300	AGGREGATE (PRIME COAT)	TON	746	746			
40600895	CONSTRUCTING TEST STRIP	EACH	2	2			
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	556	556			
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	347	347			
40600990	TEMPORARY RAMP	SQ YD	1,240	1,240			
40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	1,308	1,308			
40603240	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90	TON	51,897	51,897			
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	1,028	1,028			
40603545	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90	TON	17,639	17,639			
42001300	PROTECTIVE COAT	SQ YD	2,167.7		2,167.7		

FILE NAME =	USER NAME = [aughl1nr]	DESIGNED -	REVISED -
c:\pwork\pwidot\LAUGHLINRL\0184389\216SUM01.SHT		DRAWN - AAD	REVISED -
		CHECKED - JAC	REVISED -
		DATE - 06/11/07	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	3
CONTRACT NO. 72910				
FED. ROAD DIST. NO. 2 (ILLINOIS) FED. AID PROJECT				

Rev.

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

100% STATE

DESCRIPTIONS				CONSTRUCTION TYPE CODE			
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY 1000	STRUCTURES I-72 OVER CLEAR CREEK X031-2A	STRUCTURES I-72 OVER CLEAR CREEK TRIBUTARY X028-2A	STRUCTURES CH 34 OVER I-72 X231-2A
42001420	BRIDGE APPROACH PAVEMENT CONNECTOR (PCC)	SQ YD	1,778	1,778			
44000100	PAVEMENT REMOVAL	SQ YD	2,312	2,312			
44000153	HOT-MIX ASPHALT SURFACE REMOVAL, 1"	SQ YD	41,701	41,701			
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	3,807	3,807			
44000162	HOT-MIX ASPHALT SURFACE REMOVAL, 3 1/4"	SQ YD	146,483	146,483			
44000169	HOT-MIX ASPHALT SURFACE REMOVAL, 5"	SQ YD	63,508	63,508			
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	2,134	2,134			
44000915	HOT-MIX ASPHALT SURFACE REMOVAL (DECK)	SQ YD	1,574		1,574		
44002020	CONCRETE MEDIAN SURFACE REMOVAL	SQ FT	3,943	3,943			
44004250	PAVED SHOULDER REMOVAL	SQ YD	5,056	5,056			
44212899	PAVEMENT PATCHING (FULL DEPTH)	SQ YD	300	300			
44200565	CLASS A PATCHES, TYPE II, 11 INCH	SQ YD	303	303			
44212900	PAVEMENT PATCHING (PARTIAL DEPTH)	SQ YD	3,000	3,000			
44200569	CLASS A PATCHES, TYPE III, 11 INCH	SQ YD	484	484			
44200571	CLASS A PATCHES, TYPE IV, 11 INCH	SQ YD	2,327	2,327			
44200574	CLASS A PATCHES, TYPE I, 13 INCH	SQ YD	4	4			

FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -
c:\pwwork\PIWIDOT\LAUGHLINR1\0184389	016SUM02.SHT	DRAWN - AAD	REVISED -
	PLOT SCALE = NONE	CHECKED - JAC	REVISED -
	PLOT DATE = Feb-11-2010 08:45:04AM	DATE - 06/11/07	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	4
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT			CONTRACT NO. 72910	

Rev.

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

100% STATE

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE			
				ROADWAY 1000	STRUCTURES I-72 OVER CLEAR CREEK X031-2A	STRUCTURES I-72 OVER CLEAR CREEK TRIBUTARY X028-2A	STRUCTURES CH 34 OVER I-72 X231-2A
44200934	CLASS B PATCHES, TYPE II, 8 INCH	SQ YD	737	737			
44200942	CLASS B PATCHES, TYPE III, 8 INCH	SQ YD	75	75			
44200944	CLASS B PATCHES, TYPE IV, 8 INCH	SQ YD	161	161			
44200982	CLASS B PATCHES, TYPE II, 11 INCH	SQ YD	182	182			
44200986	CLASS B PATCHES, TYPE III, 11 INCH	SQ YD	25	25			
44213000	PATCHING REINFORCEMENT	SQ YD	3,391	3,391			
44213100	PAVEMENT FABRIC	SQ YD	260	260			
44213200	SAW CUTS	FOOT	5,991	5,991			
48101200	AGGREGATE SHOULDERS, TYPE B	TON	8,187	8,187			
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	3,083	3,083			
48203042	HOT-MIX ASPHALT SHOULDERS, 11 1/4"	SQ YD	1,200	1,200			
48203100	HOT-MIX ASPHALT SHOULDERS	TON	21,162	21,162			
48203049	HOT-MIX ASPHALT SHOULDERS, 13"	SQ YD	300	300			
50102400	CONCRETE REMOVAL	CU YD	32.2		24.2	8.0	
50200100	STRUCTURE EXCAVATION	CU YD	515			515	
50300225	CONCRETE STRUCTURES	CU YD	57.1		57.1		

Rev.

FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
es:\pvc\work\PI\DOT\LAUGHLINRL\08184389\7	216SUM03.SHT	DRAWN - AAD	REVISED -			72	184-10-21RS-3, 184-10-31RS-4	SANGAMON	61	5	
PLOT SCALE = NONE	CHECKED - JAC	REVISED -				CONTRACT NO. 72910					
PLOT DATE = Feb-11-2010 08:45:05AM	DATE - 06/11/07	REVISED -				FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT					

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

100% STATE

DESCRIPTIONS				CONSTRUCTION TYPE CODE			
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY I000	STRUCTURES I-72 OVER CLEAR CREEK X031-2A	STRUCTURES I-72 OVER CLEAR CREEK TRIBUTARY X028-2A	STRUCTURES CH 34 OVER I-72 X231-2A
50300255	CONCRETE SUPERSTRUCTURE	CU YD	270.6		270.6		
50300260	BRIDGE DECK GROOVING	SQ YD	2,167.7		2,167.7		
50800105	REINFORCEMENT BARS	POUND	770	40		730	
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	63,460		63,320	140	
50800515	BAR SPLICERS	EACH	492		492		
52000110	PREFORMED JOINT STRIP SEAL	FOOT	180		180		
54003000	CONCRETE BOX CULVERTS	CU YD	16.7			16.7	
54213669	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 24"	EACH	9	9			
54215547	METAL END SECTIONS 12"	EACH	2	2			
54223459	REMOVE AND RELAY PIPE CULVERTS 24"	FOOT	64	64			
54223471	REMOVE AND RELAY PIPE CULVERTS 36"	FOOT	8	8			
54248510	CONCRETE COLLAR	CU YD	1	1			
54390180	INSERTION CULVERT LINER 24"	FOOT	118	118			
59300100	CONTROLLED LOW-STRENGTH MATERIAL	CU YD	4		4		

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

100% STATE

DESCRIPTIONS				CONSTRUCTION TYPE CODE			
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY 1000	STRUCTURES I-72 OVER CLEAR CREEK X031-2A	STRUCTURES I-72 OVER CLEAR CREEK TRIBUTARY X028-2A	STRUCTURES CH 34 OVER I-72 X231-2A
60100945	PIPE DRAINS 12"	FOOT	20	20			
60260100	INLETS TO BE ADJUSTED	EACH	5	5			
60260300	INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	1	1			
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	3,041	3,041			
60618760	CONCRETE MEDIAN, TYPE M-4.12	SQ FT	902	902			
60900240	TYPE C INLET BOX, STANDARD 609006	EACH	1	1			
60900315	TYPE D INLET BOX, STANDARD 609006	EACH	1	1			
* 63000001	STEEL PLATE BEAM GUARD RAIL, TYPE A, 6 FOOT POSTS	FOOT	550	550			
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	4	4			
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	4	4			
63200310	GUARDRAIL REMOVAL	FOOT	550	550			
63500105	DELINEATORS	EACH	312	312			
64200105	SHOULDER RUMBLE STRIP	FOOT	145,848	145,848			
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	14	14			

*Specialty Items Rev.

FILE NAME = c:\p\work\PWIDOT\LAUHLINRL\j0184389\16SUM05.SHT	USER NAME = laughlinr1	DESIGNED -	REVISED -
PLOT SCALE = NONE	CHECKED - JAC	DATE - 06/11/07	REVISED -
PLOT DATE = Feb-11-2010 08:45:09AM			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE. 72	SECTION (84-10-2RS-3, 84-10-3RS-4)	COUNTY SANGAMON	TOTAL SHEETS 61	SHEET NO. 7
CONTRACT NO. 72910				
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

100% STATE

DESCRIPTIONS				CONSTRUCTION TYPE CODE			
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY I000	STRUCTURES I-72 OVER CLEAR CREEK X031-2A	STRUCTURES I-72 OVER CLEAR CREEK TRIBUTARY X028-2A	STRUCTURES CH 34 OVER I-72 X231-2A
67100100	MOBILIZATION	L SUM	1	1			
70100207	TRAFFIC CONTROL AND PROTECTION, STANDARD 701402	EACH	2	2			
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	4	4			
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1			
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1			
70100700	TRAFFIC CONTROL AND PROTECTION, STANDARD 701406	L SUM	1	1			
70100800	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	L SUM	1	1			
70100820	TRAFFIC CONTROL AND PROTECTION, STANDARD 701451	L SUM	1	1			
70100825	TRAFFIC CONTROL AND PROTECTION, STANDARD 701456	L SUM	1	1			
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	20	20			
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	28	28			
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	16,600	16,600			
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	62.4	62.4			
70300230	TEMPORARY PAVEMENT MARKING - LINE 5"	FOOT	190,934	190,934			

FILE NAME = c:\pwork\pwork\LAUGHLIN\RL\081843897	USER NAME = loughlinr	DESIGNED -	REVISED -
	DIGSUM06.SHT	DRAWN - AAD	REVISED -
	PLOT SCALE = NONE	CHECKED - JAC	REVISED -
	PLOT DATE = Feb-11-2010 08:45:11AM	DATE - 06/11/07	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	8
CONTRACT NO. 72910				
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

100% STATE

DESCRIPTIONS				CONSTRUCTION TYPE CODE			
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY 1000	STRUCTURES I-72 OVER CLEAR CREEK X031-2A	STRUCTURES I-72 OVER CLEAR CREEK TRIBUTARY X028-2A	STRUCTURES CH 34 OVER I-72 X231-2A
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	3,788	3,788			
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	337	337			
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	72	72			
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	5,541	5,541			
70400100	TEMPORARY CONCRETE BARRIER	FOOT	1,400		1,400		
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	1,400		1,400		
* 78004200	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LETTERS AND SYMBOLS	SQ FT	62.4	62.4			
* 78004220	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	FOOT	18,653	18,653			
* 78004240	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 8"	FOOT	3,788	3,788			
* 78004250	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 12"	FOOT	337	337			
* 78004280	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 24"	FOOT	72	72			
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1,053	1,053			
* 78200410	GUARDRAIL MARKERS, TYPE A	EACH	10	10			
* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	4	4			

*Specialty Items

FILE NAME -	USER NAME - laughlinr1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES	F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
e:\pwwork\p\1007\LAUGHLINRL\j0184389\	116SUM07.SHT	DRAWN - AAD	REVISED -			72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	9
PLOT SCALE = NONE	CHECKED - JAC	REVISED -				CONTRACT NO. 72910				
PLOT DATE = Feb-11-2010 08:45:13AM	DATE - 06/11/07	REVISED -				SCALE:	SHEET NO. OF SHEETS STA. TO STA.	FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT		

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

100% STATE

DESCRIPTIONS				CONSTRUCTION TYPE CODE			
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY 1000	STRUCTURES I-72 OVER CLEAR CREEK X031-2A	STRUCTURES I-72 OVER CLEAR CREEK TRIBUTARY X028-2A	STRUCTURES CH 34 OVER I-72 X231-2A
78300100	PAVEMENT MARKING REMOVAL	SQ FT	2,000	2,000			
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1,053	1,053			
Z0010555	DITCH CLEANING	FOOT	90	90			
Z0006204	BRIDGE DECK HYDRO-SCARIFICATION 1/2"	SQ YD	1,540		1,540		
Z0016001	DECK SLAB REPAIR (FULL DEPTH, TYPE I)	SQ YD	37		37		
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	37		37		
Z0017202	DOWEL BARS 1 1/2"	EACH	2,304	2,304			
Z0021400	EXPANSION JOINT (SPECIAL)	FOOT	64	64			
Z0021500	EXPANSION JOINT 3"	FOOT	32	32			
Z0030260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	2	2			
Z0030330	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE), TEST LEVEL 3	EACH	2	2			
Z0033600	LONGITUDINAL JOINT REPAIR	FOOT	116	116			
Z0053750	RETROFIT CONCRETE PARAPET	FOOT	85				85

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

1001 STATE

DESCRIPTIONS				CONSTRUCTION TYPE CODE			
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY 1000	STRUCTURES I-72 OVER CLEAR CREEK X031-2A	STRUCTURES I-72 OVER CLEAR CREEK TRIBUTARY X028-2A	STRUCTURES CH 34 OVER I-72 X231-2A
Z0075310	TIE BARS 3/4"	EACH	98	98			
XZ193500	BRIDGE DECK MICROSILICA CONCRETE OVERLAY 2 1/4"	SQ YD	1,540		1,540		
X0320157	CLEANING UNDERDRAIN OUTLETS	EACH	327	327			
X0320547	REMOVE AND REINSTALL END SECTION	EACH	12	12			
X0322279	OUTLET MARKER	EACH	327	327			
X0322729	MATERIAL TRANSFER DEVICE	TON	65,171	65,171			
X0326214	HANDRAIL MODIFICATION	L SUM	1				1
X0976500	END SECTIONS TO BE REMOVED	EACH	9	9			
X2503000	MAINTENANCE MOWING	ACRE	94	94			
44200610	CLASS A PATCHES, TYPE II, 13 INCH	SQ YD	131	131			
44200614	CLASS A PATCHES, TYPE IV, 13 INCH	SQ YD	143	143			
X6060500	CORRUGATED MEDIAN REMOVAL	SQ FT	1,402	1,402			
X6063401	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.12	FOOT	1,440	1,440			
X7030072	GROOVING FOR RECESSED PAVEMENT MARKING 6"	FOOT	172,281	172,281			

FILE NAME *	USER NAME = [ougltrn]	DESIGNED -	REVISED -
c:\pwork\pwidot\LAUGHLINRL\d01843897\016SUM09.SHT		DRAWN - AAD	REVISED -
PLOT SCALE = NONE		CHECKED - JAC	REVISED -
PLOT DATE = Feb-11-2010 01:30:26PM		DATE - 06/11/07	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	11
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				
CONTRACT NO. 72910				

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

100% STATE

DESCRIPTIONS

CONSTRUCTION TYPE CODE

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY I000	STRUCTURES I-72 OVER CLEAR CREEK X031-2A	STRUCTURES I-72 OVER CLEAR CREEK TRIBUTARY X028-2A	STRUCTURES CH 34 OVER I-72 X231-2A
X7200201	WIDTH RESTRICTION SIGNING	L SUM	1	1			
* X7800620	URETHANE PAVEMENT MARKING - LINE 5"	FOOT	172,281	172,281			

* specialty items

MAINLINE PAVING SCHEDULE (PAVEMENT AREA)															
LOCATION (STATION TO STATION)		LENGTH	WIDTH	AREA	HMA SURF REMOVAL BUTT JT	PCC SURF REMOVAL BUTT JT	HMA SURF REMOVAL 3.25"	HMA SURF REMOVAL 5"	BIT MATLS (P.C.)	AGG (P.C.)	HMA BINDER THICKNESS	POLYMER HMA BINDER	HMA BINDER	POLYMER HMA SURF 1.5"	HMA SURF 1.5"
STA	STA	FOOT	FOOT	SQ YD	SQ YD	SQ YD	SQ YD	TON	TON	INCHES	TON	TON	TON	TON	TON
I-72 EB															
171+09.00	171+64.00	55.00	24.00	146.67	0	0	146.67	0	0.06	0.29	1.75" TO 4.5"	25.67	0	12.32	0
171+64.00	292+60.00	12096.00	24.00	32256.00	0	0	32256.00	0	12.26	64.51	2 @ 2.25"	8128.51	0	2709.50	0
292+60.00	293+00.00	40.00	24.00	106.67	0	0	106.67	0	0.04	0.21	4.5" TO 3.5"	23.89	0	8.96	0
293+00.00	297+00.00	400.00	24.00	1066.67	0	0	1066.67	0	0.41	2.13	3.5"	209.07	0	89.60	0
297+00.00	297+40.00	40.00	24.00	106.67	0	0	106.67	0	0.04	0.21	3.5" TO 4.5"	23.89	0	8.96	0
297+40.00	404+83.61	10743.61	24.00	28649.63	0	0	28649.63	0	10.89	57.30	2 @ 2.25"	7219.71	0	2406.57	0
404+83.61	405+38.61	55.00	24.00	146.67	0	0	146.67	0	0.06	0.29	4.5" TO 1.75"	25.67	0	12.32	0
407+91.45	408+46.45	55.00	24.00	146.67	0	0	146.67	0	0.06	0.29	1.75" TO 4.5"	25.67	0	12.32	0
408+46.45	537+45.00	12898.55	24.00	34396.13	0	0	34396.13	0	13.07	68.79	2 @ 2.25"	8667.83	0	2889.28	0
537+45.00	538+00.00	55.00	24.00	146.67	0	0	146.67	0	0.06	0.29	4.5" TO 1.75"	25.67	0	12.32	0
I-72 WB															
171+09.00	171+64.00	55.00	24.00	146.67	0	0	146.67	0	0.06	0.29	1.75" TO 4.5"	25.67	0	12.32	0
171+64.00	292+60.00	12096.00	24.00	32256.00	0	0	32256.00	0	12.26	64.51	2 @ 2.25"	8128.51	0	2709.50	0
292+60.00	293+00.00	40.00	24.00	106.67	0	0	106.67	0	0.04	0.21	4.5" TO 3.5"	26.88	0	8.96	0
293+00.00	297+00.00	400.00	24.00	1066.67	0	0	1066.67	0	0.41	2.13	3.5"	268.80	0	89.60	0
297+00.00	297+40.00	40.00	24.00	106.67	0	0	106.67	0	0.04	0.21	3.5" TO 4.5"	26.88	0	8.96	0
297+40.00	304+87.00	747.00	24.00	1992.00	0	0	1992.00	0	0.76	3.98	2 @ 2.25"	501.98	0	167.33	0
304+87.00	404+68.55	9981.55	24.00	26617.47	0	0	26617.47	10.11	53.23	2 @ 2.25"	6707.60	0	2235.87	0	
404+68.55	404+88.55	20.00	24.00	53.33	0	0	53.33	0.02	0.11	4.5" TO 3.5"	11.95	0	4.48	0	
407+41.39	407+61.39	20.00	24.00	53.33	0	0	53.33	0.02	0.11	3.5" TO 4.5"	11.95	0	4.48	0	
407+61.39	537+80.00	13018.61	24.00	34716.29	0	0	34716.29	13.19	69.43	2 @ 2.25"	8748.51	0	2916.17	0	
537+80.00	538+00.00	20.00	24.00	53.33	0	0	53.33	0.02	0.11	4.5" TO 3.5"	11.95	0	4.48	0	
RAMP A															
500+00.00	512+35.00	1235.00	16.00 & VAR	1916.22	0	0	1916.22	0.73	3.83	2 @ 2.25"	482.89	0	160.96	0	
512+35.00	512+80.00	55.00	16.00	97.78	0	0	97.78	0.04	0.20	4.5" TO 2.25"	18.48	0	8.21	0	
512+80.00	520+75.27	795.27	16.00 & VAR	1758.00	0	0	1758.00	0.67	3.52	2.25"	221.51	0	147.67	0	
RAMP B															
600+23.00	608+00.00	777.00	16.00 & VAR	1692.44	0	0	1692.44	0.64	3.38	2.25"	213.25	0	142.16	0	
608+00.00	608+55.00	55.00	16.00	97.78	0	0	97.78	0.04	0.20	2.25" TO 4.5"	18.48	0	8.21	0	
608+55.00	625+07.14	1652.14	16.00 & VAR	2264.78	0	0	2264.78	0.86	4.53	2 @ 2.25"	570.72	0	190.24	0	
RAMP C															
700+00.00	712+35.00	1235.00	16.00 & VAR	1916.22	0	0	1916.22	0.73	3.83	2 @ 2.25"	482.89	0	160.96	0	
712+35.00	712+80.00	55.00	16.00	97.78	0	0	97.78	0.04	0.20	4.5" TO 2.25"	18.48	0	8.21	0	
712+80.00	720+75.27	795.27	16.00 & VAR	1758.00	0	0	1758.00	0.67	3.52	2.25"	221.51	0	147.67	0	
RAMP D															
800+23.00	810+00.00	777.00	16.00 & VAR	1692.44	0	0	1692.44	0.64	3.38	2.25"	213.25	0	142.16	0	
810+00.00	810+55.00	55.00	16.00	97.78	0	0	97.78	0.04	0.20	2.25" TO 4.5"	18.48	0	8.21	0	
810+55.00	825+07.14	1652.14	16.00 & VAR	2264.78	0	0	2264.78	0.86	4.53	2 @ 2.25"	570.72	0	190.24	0	
CH 19															
1932+00.00	1932+75.00	75.00	24.00	233.33	0	80.00	0	0	0.09	0.47	0"	0	0	0	24.36
1932+75.00	1947+62.00	1487.00	24.00 & VAR	5188.09	0	0	0	0	1.97	10.38	2.25"	0	653.70	0	435.80
1947+62.00	1948+37.00	75.00	28.00	233.33	0	93.33	0	0	0.09	0.47	0"	0	0	0	28.42
1951+63.00	1952+38.00	75.00	28.00	233.33	0	93.33	0	0	0.09	0.47	0"	0	0	0	28.42
1952+38.00	1967+25.00	1487.00	24.00 & VAR	5188.09	0	0	0	0	1.97	10.38	2.25"	0	653.70	0	435.80
1967+25.00	1968+00.00	75.00	24.00	233.33	0	80.00	0	0	0.09	0.47	0"	0	0	0	24.36
I-72 MEDIAN X-OVERS															
260+78.00				200.00	0	0	0	0	0.08	0.40	2.25"	0	0	0	16.80
327+75.00				200.00	0	0	0	0	0.08	0.40	2.25"	0	0	0	16.80
474+83.00				200.00	0	0	0	0	0.08	0.40	2.25"	0	0	0	16.80
TOTAL =					0.00	346.67	146483.09	63507.76	84.32	443.80		51896.89	1307.40	17639.23	1027.56

PCC CONNECTOR / PVMT REMOVAL SCHEDULE							
LOCATION			AVG WIDTH			PAV'T REMOVAL SQ YD	PCC CONNECTOR SQ YD
STA	STA	LENGTH	FOOT	SQ FT	SQ YD	TOTAL	TOTAL
I-72 (EB)							
403+ 97.00	404+ 97.00	100.00	40	4000	444.44	444.44	444.44
404+ 97.00	405+ 27.00	30.00	40	1200	133.33	133.33	
407+ 42.00	408+ 42.00	100.00	40	4000	444.44	444.44	444.44
407+ 12.00	407+ 42.00	30.00	40	1200	133.33	133.33	
I-72 (WB)							
404+ 38.00	405+ 38.00	100.00	40	4000	444.44	444.44	444.44
405+ 38.00	405+ 68.00	30.00	40	1200	133.33	133.33	
407+ 83.00	408+ 83.00	100.00	40	4000	444.44	444.44	444.44
407+ 53.00	407+ 83.00	30.00	40	1200	133.33	133.33	
TOTALS						2,312	1,778

MAINLINE PAVING SCHEDULE (SHOULDER AREA)															
LOCATION (STATION TO STATION)		LENGTH	WIDTH	AREA	EX OVERLAY THICKNESS	HMA SURF REMOVAL BUTT JT	HMA SURF REMOVAL 1"	HMA SURF REMOVAL 1.5"	BIT MATLS (P.C.)	AGG (P.C.)	HMA SHLD THICKNESS	HMA SHLD	AGG SHLD WIDTH	AGG SHLD THICKNESS @ EOS	AGG SHLD
STA	STA	FOOT	FOOT	SQ YD	INCHES	SQ YD	SQ YD	SQ YD	TON	TON	INCHES	TON	FOOT	INCHES	TON
I-72 EB MEDIAN															
171+09.00	171+64.00	55.00	6.00	36.67	3.25	26.67	0	0	0.01	0.07	2.75	5.65	5.00	2.75	3.87
171+64.00	292+60.00	12096.00	6.00	8064.00	3.25	0	0	0	3.06	16.13	2.75	1241.86	5.00	2.75	851.43
292+60.00	293+00.00	40.00	6.00	26.67	3.25	0	0	0	0.01	0.05	2.75 TO 1.75	3.36	5.00	2.75 TO 1.75	2.37
293+00.00	297+00.00	400.00	6.00	266.67	3.25	0	0	0	0.10	0.53	1.75	26.13	5.00	1.75	19.30
297+00.00	297+40.00	40.00	6.00	26.67	3.25	0	0	0	0.01	0.05	1.75 TO 2.75	3.36	5.00	1.75 TO 2.75	2.37
297+40.00	404+76.91	10736.91	6.00	7157.94	3.25	0	0	0	2.72	14.32	2.75	1102.32	5.00	2.75	755.77
404+76.91	405+31.91	55.00	6.00	36.67	3.25	26.67	0	0	0.01	0.07	2.75 TO 1.50	4.36	5.00	2.75 TO 1.50	3.11
407+84.75	408+39.75	55.00	6.00	36.67	3.25	26.67	0	0	0.01	0.07	1.50 TO 2.75	4.36	5.00	1.50 TO 2.75	3.11
408+39.75	537+45.00	12905.25	6.00	8603.50	3.25	0	0	0	3.27	17.21	2.75	1324.94	5.00	2.75	908.40
537+45.00	538+00.00	55.00	6.00	36.67	3.25	26.67	0	0	0.01	0.07	2.75	5.65	5.00	2.75	3.87
I-72 EB OUTSIDE															
171+09.00	171+64.00	55.00	10.00	61.11	3.25	44.44	0	0	0.02	0.12	2.75	9.41	3.00	2.75	1.96
171+64.00	275+85.00	10421.00	10.00	11578.89	3.25	0	0	0	4.40	23.16	2.75	1783.15	3.00	2.75	370.89
275+85.00	282+45.00	260.00	6.33 TO 20.00	380.47	3.25	0	0	0	0.14	0.76	2.75	58.59	0.00	0.00	0
282+45.00	292+60.00	1015.00	10.00	1127.78	3.25	0	0	0	0.43	2.26	2.75	173.68	3.00	2.75	36.12
292+60.00	293+00.00	40.00	10.00	44.44	3.25	0	0	0	0.02	0.09	2.75 TO 1.75	5.60	3.00	2.75 TO 1.75	1.23
293+00.00	297+00.00	400.00	10.00	444.44	3.25	0	0	0	0.17	0.89	1.75	43.56	3.00	1.75	10.44
297+00.00	297+40.00	40.00	10.00	44.44	3.25	0	0	0	0.02	0.09	1.75 TO 2.75	5.60	3.00	1.75 TO 2.75	1.23
297+40.00	306+95.00	955.00	10.00	1061.11	3.25	0	0	0	0.40	2.12	2.75	163.41	3.00	2.75	33.99
306+95.00	308+95.00	200.00	16.00 TO 4.00	222.22	3.25	0	0	0	0.08	0.44	2.75	34.22	0.00	0.00	0
318+45.00	404+91.21	8646.21	10.00	9606.90	3.25	0	0	0	3.65	19.21	2.75	1479.46	3.00	2.75	307.72
404+91.21	405+46.21	55.00	10.00	61.11	3.25	44.44	0	0	0.02	0.12	2.75 TO 1.50	7.27	3.00	2.75 TO 1.50	1.63
407+99.05	408+54.05	55.00	10.00	61.11	3.25	44.44	0	0	0.02	0.12	1.50 TO 2.75	7.27	3.00	1.50 TO 2.75	1.63
408+54.05	537+45.00	12890.95	10.00	14323.28	3.25	0	0	0	5.44	28.65	2.75	2205.78	3.00	2.75	458.79
537+45.00	538+00.00	55.00	10.00	61.11	3.25	44.44	0	0	0.02	0.12	2.75	9.41	3.00	2.75	1.96
I-72 WB MEDIAN															
171+09.00	171+64.00	55.00	6.00	36.67	3.25	26.67	0	0	0.01	0.07	2.75	5.65	5.00	2.75	3.87
171+64.00	292+60.00	12096.00	6.00	8064.00	3.25	0	0	0	3.06	16.13	2.75	1241.86	5.00	2.75	851.43
292+60.00	293+00.00	40.00	6.00	26.67	3.25	0	0	0	0.01	0.05	2.75 TO 1.75	3.36	5.00	2.75 TO 1.75	2.37
293+00.00	297+00.00	400.00	6.00	266.67	3.25	0	0	0	0.10	0.53	1.75	26.13	5.00	1.75	19.30
297+00.00	297+40.00	40.00	6.00	26.67	3.25	0	0	0	0.01	0.05	1.75 TO 2.75	3.36	5.00	1.75 TO 2.75	2.37
297+40.00	304+87.00	747.00	6.00	498.00	3.25	0	0	0	0.19	1.00	2.75	76.69	5.00	2.75	52.58
304+87.00	305+02.00	15.00	6.00	10.00	5.00	0	10.00	0	0.00	0.02	2.75 TO 2.00	1.33	0.00	2.75 TO 2.00	0.99
305+02.00	404+95.25	9993.25	6.00	6662.17	5.00	0	6662.17	0	2.53	13.32	2.00	746.16	5.00	2.00	537.45
404+95.25	404+95.25	55.00	6.00	36.67	5.00	13.33	23.33	0	0.01	0.07	2.00	4.11	5.00	2.00	2.96
407+48.09	408+03.09	55.00	6.00	36.67	5.00	13.33	23.33	0	0.01	0.07	2.00	4.11	5.00	2.00	2.96
408+03.09	537+80.00	12976.91	6.00	8651.27	5.00	0	8651.27	0	3.29	17.30	2.00	968.94	5.00	2.00	697.91
537+80.00	538+00.00	20.00	6.00	13.33	5.00	13.33	0	0	0.01	0.03	2.00	1.49	5.00	2.00	1.08
I-72 WB OUTSIDE															
171+09.00	171+64.00	55.00	10.00	61.11	3.25	44.44	0	0	0.02	0.12	2.75	9.41	3.00	2.75	1.96
171+64.00	271+80.00	10016.00	10.00	11128.89	3.25	0	0	0	4.23	22.26	2.75	1713.85	3.00	2.75	356.47
280+30.00	283+35.00	305.00	4.00 TO 16.00	338.89	3.25	0	0	0	0.13	0.68	2.75	52.19	0.00	0.00	0
283+35.00	292+60.00	925.00	10.00	1027.78	3.25	0	0	0	0.39	2.06	2.75	158.28	3.00	2.75	32.92
292+60.00	293+00.00	40.00	10.00	44.44	3.25	0	0	0	0.02	0.09	2.75 TO 1.75	5.60	3.00	2.75 TO 1.75	1.23
293+00.00	297+00.00	400.00	10.00	444.44	3.25	0	0	0	0.17	0.89	1.75	43.56	3.00	1.75	10.44
297+00.00	297+40.00	40.00	10.00	44.44	3.25	0	0	0	0.02	0.09	1.75 TO 2.75	5.60	3.00	1.75 TO 2.75	1.23
297+40.00	304+87.00	747.00	10.00	830.00	3.25	0	0	0	0.32	1.66	2.75	127.82	3.00	2.75	26.59
304+87.00	305+02.00	15.00	10.00	16.67	5.00	0	16.67	0	0.01	0.03	2.75 TO 2.00	2.22	3.00	2.75 TO 2.00	0.53
305+02.00	307+80.00	278.00	10.00	308.89	5.00	0	308.89	0	0.12	0.62	2.00	34.60	3.00	2.00	7.92
307+80.00	310+40.00	260.00	20.00 TO 6.33	380.47	5.00	0	380.47	0	0.14	0.76	2.00	42.61	0.00	0.00	0
314+40.00	404+25.95	8985.95	10.00	9984.39	5.00	0	9984.39	0	3.79	19.97	2.00	1118.25	3.00	2.00	255.85
404+25.95	404+80.95	55.00	10.00	61.11	5.00	22.22	38.89	16.67	0.02	0.12	2.00	6.84	3.00	2.00	1.57
407+33.79	407+88.79	55.00	10.00	61.11	5.00	22.22	38.89	16.67	0.02	0.12	2.00	6.84	3.00	2.00	1.57
407+88.79	537+80.00	12991.21	10.00	14434.68	5.00	0	14434.68	0	5.49	28.87	2.00	1616.68	3.00	2.00	369.89
537+80.00	538+00.00	20.00	10.00	22.22	5.00	22.22	0	0	0.01	0.04	2.00	2.49	3.00	2.00	0.57
RAMP A OUTSIDE															
500+00.00	501+45.00	145.00	9.00 TO 6.00	120.83	5.00	0	120.83	0	0.05	0.24	2.00	13.53	3.00 TO 5.00	2.00	7.80
501+45.00	512+35.00	1090.00	6.00	726.67	5.00	0	726.67	0	0.28	1.45	2.00	81.39	5.00	2.00	58.62
512+35.00	512+80.00	55.00	6.00	36.67	3.25	0	36.67	0	0.01	0.07	2.00	4.11	5.00	2.00	2.96
512+80.00	520+75.27	795.27	6.00	550.00	3.25	0	550.00	0.21	1.10	2.00	61.60	5.00	2.00	42.77	
RAMP A INSIDE															
506+60.00	512+35.00	575.00	4.00	255.56	5.00	0	255.56	0	0.10	0.51	2.00	28.62	5.00	2.00	30.92
512+35.00	512+80.00	55.00	4.00	24.44	5.00	0	24.44	0	0.01	0.05	2.00	2.74	5.00	2.00	2.96
512+80.00	520+75.27	795.27	4.00	351.11	3.25	0	351.11	0.13	0.70	2.00	39.32	5.00	2.00	42.77	
RAMP B OUTSIDE															
600+23.00	608+00.00	777.00	6.00	533.33	3.25	0	533.33	0.20	1.07	2.00	59.73	5.00	2.00	41.79	
608+00.00	608+55.00	55.00	6.00	36.67	3.25	0	36.67	0.01	0.07	2.00 TO 2.75	4.88	5.00	2.00 TO 2.75	3.41	
608+55.00	623+57.13	1502.13	6.00	1001.42	3.25	0	1001.42	0	0.38	2.00	2.75	154.22	5.00	2.75	105.73
623+57.13	625+07.14	150.00	6.00 TO 9.00	125.00	3.25	0	125.00	0	0.05	0.25	2.75	19.25	5.00 TO 3.00	2.75	10.56
RAMP B INSIDE															
600+23.00	608+00.00	777.00	4.00	342.22	3.25	0	342.22	0.13	0.68	2.00	38.33	5.00	2.00	41.79	
608+00.00	608+55.00	55.00	4.00	24.44	3.25	0	24.44	0.01	0.05	2.00 TO 2.75	3.25	5.00	2.00 TO 2.75	3.41	
608+55.00	613+57.14	502.14	4.00	223.17	3.25	0	223.17	0	0.08	0.4					

PAVEMENT MARKING SCHEDULE

LOCATION			URETHANE PAVEMENT MARKING - LINE 5"		PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE						TEMPORARY PAVEMENT MARKING - LINE								PAVEMENT MARKING REMOVAL	RAISED REFLECTIVE PAVEMENT MARKERS	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	SHORT-TERM PAVEMENT MARKING (2 APPS)	WORK ZONE PAVEMENT MARKING REMOVAL				
			SOLID WHITE	SOLID YELLOW		5" DOTTED WHITE	8" SOLID WHITE	8" SOLID YELLOW	12" SOLID WHITE	12" SOLID YELLOW	24" SOLID WHITE	LET & SYMB	5" SOLID WHITE	5" SKIP WHITE	5" DOTTED WHITE	5" SOLID YELLOW	8" SOLID WHITE	8" SOLID YELLOW	12" SOLID WHITE						12" SOLID YELLOW	24" SOLID WHITE	LET & SYMB	
STA.	STA.	LT/RT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	SQ FT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	SQ FT	SQ FT	EACH	EACH	FOOT	SQ FT		
I-72																												
171+09	196+00	LT & RT	4,982	4,982	1,260							4,982	1,260		4,982									64	64	1,120	374	
196+00	225+75	LT & RT	5,950	5,950	1,500							5,950	1,500		5,950								76	76	1,360	454		
225+75	256+00	LT & RT	6,050	6,050	1,520							6,050	1,520		6,050								76	76	1,360	454		
256+00	271+79	LT	1,579									1,579																
256+00	283+00	LT & RT		5,400	1,360								1,360		5,400								68	68	1,200	400		
256+00	275+86	RT	1,986									1,986																
279+28	283+00	LT	372									372																
282+66	283+00	RT	34									34																
278+89	282+66	RT				377								377														
283+00	298+00	LT & RT	3,000	3,000	760							3,000	760		3,000								38	38	680	227		
298+00	305+55	LT	755									755																
305+55	311+33	LT				578								578														
298+00	313+00	LT & RT		3,000	760								760		3,000								38	38	680	227		
298+00	310+76	RT	1,276									1,276																
313+00	343+00	LT & RT		6,000	1,500								1,500		6,000								76	76	1,360	454		
314+37	343+00	LT	2,863									2,863																
318+43	343+00	RT	2,457									2,457																
343+00	373+00	LT & RT	6,000	6,000	1,500							6,000	1,500		6,000								76	76	1,360	454		
373+00	403+00	LT & RT	6,000	6,000	1,500							6,000	1,500		6,000								76	76	1,360	454		
403+00	433+00	LT & RT	6,000	6,000	1,500							6,000	1,500		6,000						2,000		70	70	1,360	454		
433+00	448+00	LT & RT	3,000	3,000	760							3,000	760		3,000								38	38	680	227		
448+00	463+00	LT & RT	3,000	3,000	760							3,000	760		3,000								38	38	680	227		
463+00	493+00	LT & RT	6,000	6,000	1,500							6,000	1,500		6,000								76	76	1,360	454		
493+00	523+00	LT & RT	6,000	6,000	1,500							6,000	1,500		6,000								76	76	1,360	454		
523+00	538+00	LT & RT	3,000	3,000	760							3,000	760		3,000								38	38	680	227		
RAMP A																												
500+00	520+75					73.0									73													
500+00	513+50	RT	1,350									1,350																
500+00	506+62	RT				362								362									34	34				
503+00	506+62	LT					77															9	9					
506+62	512+26																					18	18					
506+62	513+50	LT		688											688													
513+50	520+33	LT		683											683													
513+50	519+56	RT	606									606																
520+33	520+75	LT		46											46													
519+56	520+75	RT	196									196																
520+64		LT																										
520+98		RT																										
ISLAND						170		70							170		70											
RAMP B																												
600+23	625+07																											
600+23	600+96	LT		80											80													
600+23	601+26	RT	170									170																
600+96	607+00	LT		604											604													
601+26	607+00	RT	574									574																
607+00	625+07	RT	1,807									1,807																
607+00	614+57	LT		757											757													
614+57	617+57	LT				300									300													
617+57	625+07	LT																										
SHEET TOTAL			75,007	76,240	18,440		1,860		147		36		75,007	18,440		76,240	1,860		147		36		2,000	985	985	16,600	5,541	

(CONTINUED ON NEXT PAGE)

(CONTINUED ON NEXT PAGE)

DELINEATORS SCHEDULE							
LOCATION			LENGTH OF NEED	SPACING	SINGLE	SINGLE EACH SIDE	DOUBLE
STA	STA	LT/RT	FOOT	FOOT	EACH	EACH	EACH
IL 72							
179+ 09.00	275+ 86.00	RT	9677.00	400	24		
179+ 09.00	271+ 78.79	LT	9269.79	400	23		
RAMP A							
500+ 00.00	506+ 62.40	RT	662.40	200			4
506+ 62.40	512+ 26.02	LT/RT	563.62	80		16	
512+ 62.14	514+ 66.04	LT/RT	203.90	125		2	
514+ 66.04	518+ 44.81	LT/RT	378.77	55		14	
518+ 44.81	519+ 55.00	LT/RT	110.19	125		2	
RAMP B							
601+ 28.16	602+ 38.46	LT/RT	110.30	125		2	
602+ 38.46	606+ 19.97	LT/RT	381.51	65		2	
606+ 19.97	608+ 59.99	LT/RT	240.02	125		4	
608+ 59.99	614+ 57.18	LT/RT	597.19	80		2	
614+ 57.18	625+ 07.14	RT	1049.96	200			6
RAMP C							
700+ 00.00	706+ 62.40	RT	662.40	200			4
706+ 62.40	712+ 26.05	LT/RT	563.65	80		14	
712+ 26.05	714+ 66.04	LT/RT	239.99	165		4	
714+ 66.04	718+ 44.82	LT/RT	378.78	65		2	
718+ 44.82	719+ 56.00	LT/RT	111.18	125		2	
RAMP D							
800+ 99.10	802+ 38.47	LT/RT	139.37	125		2	
802+ 38.47	806+ 19.97	LT/RT	381.50	65		12	
806+ 19.97	808+ 59.99	LT/RT	240.02	125		4	
808+ 59.99	814+ 57.19	LT/RT	597.20	80		16	
814+ 57.19	825+ 04.14	RT	1046.95	200			6
IL 72							
318+ 43.00	402+ 01.00	RT	8358.00	400	21		
407+ 58.00	538+ 00.00	RT	13042.00	400	33		
314+ 36.47	405+ 15.00	LT	9078.53	400	23		
410+ 70.00	538+ 00.00	LT	12730.00	400	32		

(CONTINUED ON NEXT TABLE)

SUB-BASE GRANULAR MAT, TY B SCHEDULE						
LOCATION			DEPTH			TON
STA	STA	LENGTH	SQ FT	INCH	CU YD	TOTALS
CH 19						
1938+ 69.00	1940+ 44.00	353.00	1092.00	12	40.44	83
1946+ 50.00	1948+ 37.00	187.00	470.00	12	17.41	36
1951+ 63.00	1953+ 50.00	375.00	466.00	12	17.26	36
1959+ 64.00	1961+ 31.00	338.00	1013.00	12	37.52	77
TOTALS						232

DELINEATORS SCHEDULE		
LOCATION	DELINEATORS	
	EACH	
MAINLINE		
173+30.00	110' RT	1
173+90.00	110' LT	1
199+90.00	148' RT	1
200+90.00	160' LT	1
210+95.00	115' RT	1
213+15.00	112' LT	1
240+30.00	92' LT	1
240+30.00	100' RT	1
248+70.00	105' RT	1
249+25.00	100' LT	1
261+00.00	103' RT	1
273+50.00	99' RT	1
286+00.00	94' RT	1
304+00.00	94' LT	1
316+00.00	110' LT	1
327+50.00	110' LT	1
327+50.00	105' RT	1
340+00.00	103' LT	1
348+75.00	98' LT	1
353+27.00	115' LT	1
354+27.00	125' RT	1
366+00.00	100' LT	1
366+00.00	110' RT	1
427+70.00	118' RT	1
430+75.00	120' RT	1
430+95.00	117' LT	1

(CONTINUED ON NEXT TABLE)

DELINEATORS SCHEDULE		
LOCATION	DELINEATORS	
	EACH	
RAMP A		
510+00.00	27' RT	1
510+00.00	43' LT	1
RAMP B		
611+00.00	27' LT	1
611+00.00	27' RT	1
615+56.00	36' RT	1
RAMP C		
713+00.00	45' RT	1
713+00.00	52' LT	1
RAMP D		
811+00.00	28' RT	1
811+00.00	45' LT	1
815+56.00	31' RT	1
TOTALS		312

CURB & GUTTER SCHEDULE									
LOCATION	COMBINATION CURB & GUTTER REMOVAL	CONCRETE MEDIAN SURFACE REMOVAL	COMBINATION CONCRETE CURB & GUTTER, TYPE M-4.12	CONCRETE MEDIAN, TYPE M-4.12	CONCRETE MEDIAN SURFACE, 4 INCH	CORRUGATED MEDIAN REMOVAL			
				FOOT	SQ FT	SQ FT	SQ FT		
CH 19									
1937+60.00	TO	1939+41.00	MEDIAN			701			
1938+69.00	TO	1940+44.00	MEDIAN	353	1,092	353			
1944+76.00	TO	1946+50.00	MEDIAN	343	446	446			
1946+50.00	TO	1948+37.00	MEDIAN	374	470	374			
1951+63.00	TO	1953+50.00	MEDIAN	375	466	375			
1953+50.00	TO	1955+23.00	MEDIAN	351	456	456			
1959+64.00	TO	1961+31.00	MEDIAN	338	1,013	338			
1960+59.00	TO	1962+40.00	MEDIAN			701			
TOTALS				2,134	3,943	1,440	902	3,041	1,402

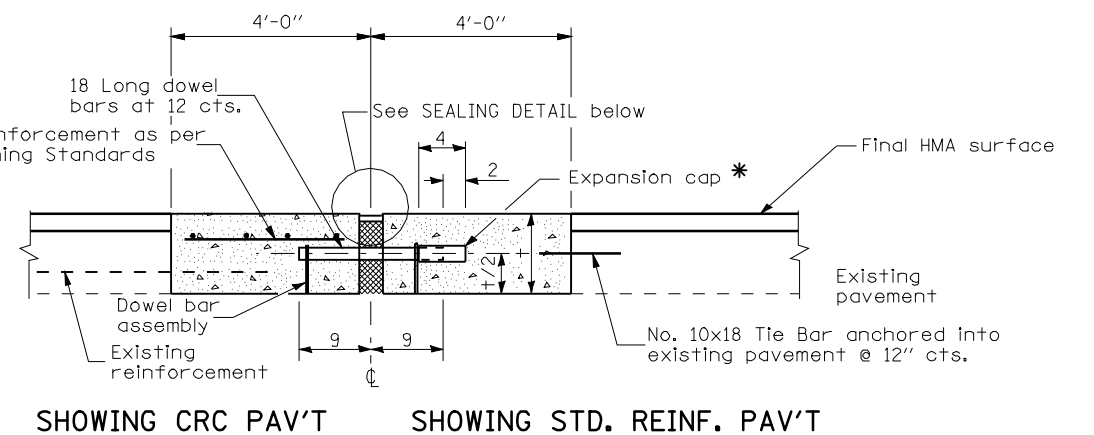
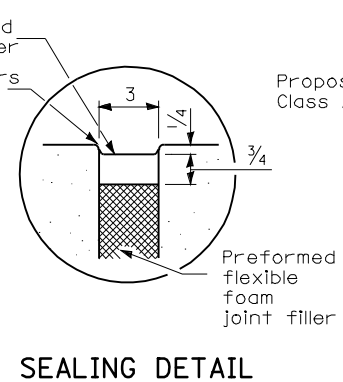
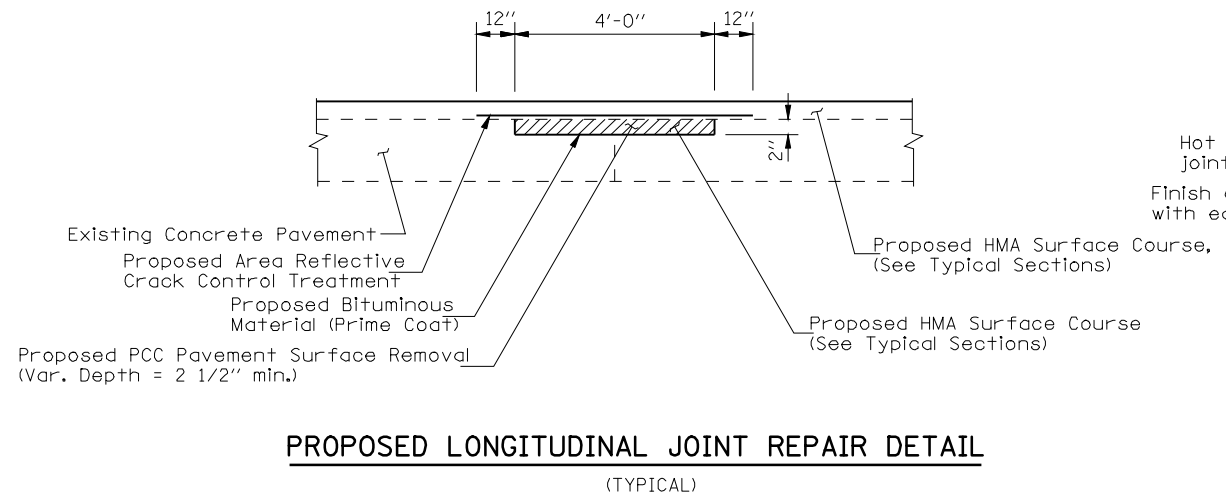
I-72 PATCHING SCHEDULE (EAST BOUND)									
STA	LANE	LENGTH	WIDTH	SAW CUTS	TIE BARS 3/4"	PATCHING REINFORCMENT	CLASS A PATCHES, TYPE II 11 1/4 IN	CLASS A PATCHES, TYPE III 11 1/4 IN	CLASS A PATCHES, TYPE IV 11 1/4 IN
				(FOOT)	(EACH)	(SQYD)	(SQYD)	(SQYD)	(SQYD)
175+12	DL	6	12	60		8.00	8.00		
175+87	DL	16	12	80		21.33		21.33	
178+69	DL	20	12	88	10	26.67			26.67
179+77	DL	15	12	78		20.00		20.00	
194+93	DL	20	12	88	10	26.67			26.67
200+38	DL	31	12	110	16	41.33			41.33
208+97	DL	6	12	60		8.00	8.00		
209+50	DL	10	12	68		13.33	13.33		
221+93	DL	22	12	92	11	29.33			29.33
270+59	DL	20	12	88	10	26.67			26.67
295+65	DL	20	12	88	10	26.67			26.67
314+35	DL	15	12	78		20.00		20.00	
317+50	DL	12	12	72		16.00		16.00	
318+65	DL	40	12	128	20	53.33			53.33
319+93	DL	100	12	248	50	133.33			133.33
323+31	DL	30	12	108	15	40.00			40.00
325+34	DL	12	12	72		16.00		16.00	
328+03	DL	8	12	64		10.67	10.67		
335+09	DL	22	12	92	11	29.33			29.33
337+09	DL	85	12	218	43	113.33			113.33
338+70	DL	30	12	108	15	40.00			40.00
343+32	DL	8	12	64		10.67	10.67		
343+98	DL	65	12	178	33	86.67			86.67
347+69	DL	33	12	114	17	44.00			44.00
371+18	DL	15	12	78		20.00		20.00	
374+69	DL	8	12	64		10.67	10.67		
376+10	DL	35	12	118	18	46.67			46.67
377+21	DL	35	12	118	18	46.67			46.67
379+94	DL	60	12	168	30	80.00			80.00
380+50	DL	10	12	68		13.33	13.33		
380+89	DL	12	12	72		16.00		16.00	
381+45	DL	10	12	68		13.33	13.33		
382+33	DL	45	12	138	23	60.00			60.00
383+02	DL	23	12	94	12	30.67			30.67
383+81	DL	52	12	152	26	69.33			69.33
384+04	DL	12	12	72		16.00		16.00	
384+43	DL	6	12	60		8.00	8.00		
224+62	DL	14	12	76		18.67		18.67	
393+62	DL	18	12	84		24.00		24.00	
394+21	DL	25	12	98	13	33.33			33.33
398+80	DL	15	12	78		20.00		20.00	
399+10	DL	12	12	72		16.00		16.00	
399+72	DL	8	12	64		10.67	10.67		
400+05	DL	10	12	68		13.33	13.33		
401+16	DL	20	12	88	10	26.67			26.67
404+28	DL	22	12	92	11	29.33			29.33
404+57	PL	20	12	88	11	26.67			26.67
408+83	PL	60	12	168	11	80.00			80.00
409+53	DL	175	12	398	35	233.33			233.33

CONTINUED

I-72 PATCHING SCHEDULE (EAST BOUND)									
STA	LANE	LENGTH	WIDTH	SAW CUTS	TIE BARS 3/4"	PATCHING REINFORCMENT	CLASS A PATCHES, TYPE II 11 1/4 IN	CLASS A PATCHES, TYPE III 11 1/4 IN	CLASS A PATCHES, TYPE IV 11 1/4 IN
				(FOOT)	(EACH)	(SQYD)	(SQYD)	(SQYD)	(SQYD)
412+81	DL	20	12	88	10	26.67			26.67
417+04	DL	25	12	98	13	33.33			33.33
417+54	DL	8	12	64		10.67	10.67		
420+16	DL	60	12	168	30	80.00			80.00
424+56	DL	10	12	68		13.33	13.33		
425+18	DL	50	12	148	25	66.67			66.67
427+35	DL	165	12	378	83	220.00			220.00
428+76	DL	55	12	158	28	73.33			73.33
447+10	DL	15	12	78		20.00		20.00	
447+42	DL	12	12	72		16.00		16.00	
456+64	DL	12	12	72		16.00		16.00	
456+94	DL	10	12	68		13.33	13.33		
457+46	DL	6	12	60		8.00	8.00		
458+05	DL	30	12	108	15	40.00			40.00
466+13	DL	6	12	60		8.00	8.00		
466+29	DL	12	12	72		16.00		16.00	
466+58	DL	6	12	60		8.00	8.00		
470+69	DL	7	12	62		9.33	9.33		
471+57	DL	18	12	84		24.00		24.00	
472+56	DL	25	12	98	13	33.33			33.33
473+31	DL	6	12	60		8.00	8.00		
474+29	DL	6	12	60		8.00	8.00		
474+59	DL	8	12	64		10.67	10.67		
476+30	DL	62	12	172	31	82.67			82.67
477+15	DL	25	12	98	13	33.33			33.33
481+58	DL	6	12	60		8.00	8.00		
481+94	DL	12	12	72		16.00		16.00	
487+09	DL	22	12	92	11	29.33			29.33
488+53	DL	12	12	72		16.00		16.00	
490+70	DL	16	12	80		21.33		21.33	
492+24	DL	12	12	72		16.00		16.00	
494+44	DL	23	12	94	12	30.67			30.67
494+73	DL	16	12	80		21.33		21.33	
496+54	DL	22	12	92	11	29.33			29.33
499+82	DL	16	12	80		21.33		21.33	
502+12	DL	6	12	60		8.00	8.00		
505+04	DL	16	12	80		21.33		21.33	
510+45	DL	12	12	72		16.00		16.00	
512+16	DL	6	12	60		8.00	8.00		
513+73	DL	10	12	68		13.33	13.33		
516+36	DL	6	12	60		8.00	8.00		
517+70	DL	26	12	100	13	34.67			34.67
518+19	DL	6	12	60		8.00	8.00		
525+80	DL	6	12	60		8.00	8.00		
528+13	DL	6	12	60		8.00	8.00		
530+27	DL	6	12	60		8.00	8.00		
532+17	DL	25	12	98	13	33.33			33.33
532+53	DL	14	12	76		18.67		18.67	
532+76	DL	6	12	60		8.00	8.00		
TOTALS				9374	810	3114	303	484	2327

I-72 PATCHING SCHEDULE (WEST BOUND)										
STA	LANE	LENGTH	WIDTH	SAW CUTS	TIE BARS 3/4"	PATCHING REINFORCEMENT	CLASS A PATCHES, TYPE I 13 IN	CLASS A PATCHES, TYPE II 13 IN	CLASS A PATCHES, TYPE IV 13 IN	
				(FOOT)	(EACH)	(SQYD)	(SQYD)	(SQYD)	(SQYD)	
385+29	DL	8	12	64		10.67		10.67		
378+13	DL	6	12	60		8.00		8.00		
377+28	DL	6	12	60		8.00		8.00		
373+34	DL	6	12	60		8.00		8.00		
352+25	DL	6	12	60		8.00		8.00		
340+73	DL	6	12	60		8.00		8.00		
339+06	DL	6	12	60		8.00		8.00		
334+14	DL	6	12	60		8.00		8.00		
330+27	DL	6	12	60		8.00		8.00		
329+22	DL	6	12	60		8.00		8.00		
326+26	DL	6	12	60		8.00		8.00		
325+38	DL	22	12	92	11	29.33			29.33	
324+92	DL	6	12	60		8.00		8.00		
324+33	DL	8	12	64		10.67		10.67		
323+84	DL	10	12	68		13.33		13.33		
287+71	DL	30	12	108	15	40.00			40.00	
283+28	DL	6	12	60		8.00		8.00		
273+08	DL	25	12	98	13	33.33			33.33	
172+92	DL	30	12	108	15	40.00			40.00	
RAMP B										
622+96	Stub	8	4	32	15	3.56	3.56			
TOTALS				1,354	69	277	4	131	143	

RAMP PATCHING SCHEDULE - IL 72 @ CH 19											
STA	LENGTH	WIDTH	CLASS B PATCHES, TYPE II 11 1/4 IN	CLASS B PATCHES, TYPE III 11 1/4 IN	PAVEMENT FABRIC	SAW CUTS	DOWEL BARS 1 1/2"	TIE BARS 3/4"	EXPANSION JOINT 3"	EXPANSION JOINT (SPECIAL)	
	(FOOT)	(FOOT)	(SQYD)	(SQYD)	(SQYD)	(FOOT)	(EACH)	(EACH)	(FOOT)	(FOOT)	
Ramp A											
506+60										16.0	
511+70	6.0	16.0	10.67			66.0	28.0				
518+80	6.0	16.0	10.67			66.0	28.0				
519+30	6.0	16.0	10.67			66.0	28.0				
520+20	6.0	16.0	10.67			66.0	28.0				
Ramp B											
601+40	8.0	16.0	14.22			72.0	14.0	14.0	16.0		
607+60	14.0	16.0		24.89	24.89	90.0	28.0				
608+60	8.0	16.0	14.22			72.0	28.0				
610+60	6.0	16.0	10.67			66.0	28.0				
614+60										16.0	
Ramp C											
706+60										16.0	
713+25	6.0	16.0	10.67			66.0	28.0				
718+45	6.0	16.0	10.67			66.0	14.0	14.0	16.0		
719+00	6.0	16.0	10.67			66.0	28.0				
Ramp D											
802+40	6.0	16.0	10.67			66.0	28.0				
802+60	6.0	16.0	10.67			66.0	28.0				
806+60	6.0	16.0	10.67			66.0	28.0				
807+60	6.0	16.0	10.67			66.0	28.0				
812+12	6.0	16.0	10.67			66.0	28.0				
814+50	8.0	16.0	14.22			72.0	28.0				
815+50										16.0	
TOTALS			182	25	25	1164	448	28	32	64	



* Expansion caps shall be installed on the exposed end of each dowel bar once the header has been removed and the joint filler material has been installed.

ESTIMATED QUANTITIES				
ESTIMATED QUANTITIES ARE INCLUDED TO OBTAIN A UNIT BID PRICE. THE CONTRACTOR SHALL UNDERSTAND THAT THESE QUANTITIES MAY INCREASE, DECREASE OR TOTALLY BE DELETED FROM THE CONTRACT. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.				
PAY ITEM NUMBER	PAY ITEM DESCRIPTION	UNIT	QUANTITY	LOCATION / REASON
44004250	PAVED SHOULDER REMOVAL	SQ YD	1500	FOR MAINLINE SHOULDER REPAIRS (PRE-EXISTING DAMAGE AND DAMAGE DURING CONSTRUCTION FROM TRAFFIC)
44212899	PAVEMENT PATCHING (FULL DEPTH)	SQ YD	300	FOR MAINLINE PAVEMENT REPAIRS AFTER HMA SURFACE REMOVAL (NO CLASS A PATCHING IN CONTRACT - MAXIMUM DEPTH 13" - REBAR REMOVAL REQUIRED - MINIMUM PATCH SIZE = 15 SQ YD) NOTE: PREVIOUS CLASS A PATCHING LOCATIONS ARE STILL INCLUDED IN THE PLANS ONLY TO GIVE LOCATIONS OF BAD PAVEMENT AREAS. THIS SURVEY WAS COMPLETED PRIOR TO IDOT DECIDING TO PROCEED WITH EXTRA THICKNES AND GO WITH PAVEMENT PATCHING AFTER MILLING (PARTIAL & FULL DEPTH).
44212900	PAVEMENT PATCHING (PARTIAL DEPTH)	SQ YD	3000	FOR MAINLINE PAVEMENT REPAIRS AFTER HMA SURFACE REMOVAL (NO CLASS A PATCHING IN CONTRACT - MINIMUM PATCH SIZE = HALF LANE WIDTH, 30 SQ YD)
48203042	HOT-MIX ASPHALT SHOULDERS, 11 1/4"	SQ YD	1200	FOR MAINLINE SHOULDER REPAIRS (PRE-EXISTING DAMAGE AND DAMAGE DURING CONSTRUCTION FROM TRAFFIC)
48203049	HOT-MIX ASPHALT SHOULDERS, 13"	SQ YD	300	FOR MAINLINE SHOULDER REPAIRS (PRE-EXISTING DAMAGE AND DAMAGE DURING CONSTRUCTION FROM TRAFFIC)

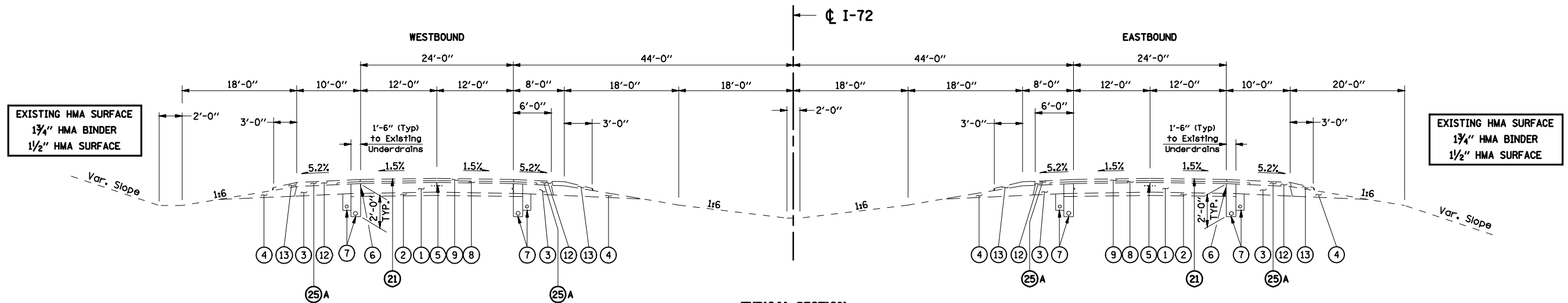
FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
e:\pwork\pwork\IDOT\LAUGHLINRL\0183140\10	DOTRevised_7016SCH10.SHT	DRAWN - AAD	REVISED -			72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON		20A	
	PLOT SCALE = NONE	CHECKED - JAC	REVISED -			CONTRACT NO. 72910					
	PLOT DATE = Mar-17-2010 08:16:07AM	DATE - 06/11/07	REVISED -			SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT

CH-19 PATCHING SCHEDULE (NORTH BOUND)									
STA	LENGTH	WIDTH	SAW CUTS	DOWEL BARS 1 1/2"	TIE BARS 3/4"	CLASS B PATCHES, TYPE II 8 IN	CLASS B PATCHES, TYPE III 8 IN	CLASS B PATCHES, TYPE IV 8 IN	PAVEMENT FABRIC
			(FOOT)	(EACH)	(EACH)	(SQYD)	(SQYD)	(SQYD)	(SQYD)
1933+12	6	12	48	20.00		8.00			
1933+60	6	12	48	20.00		8.00			
1934+12	6	12	48	20.00		8.00			
1935+14	6	12	48	20.00		8.00			
1936+15	6	13.3	51.9	20.00		8.87			
1937+15	6	15	57	24.00		10.00			
1938+75	6	15	57	24.00		10.00			
1939+15	6	15	57	24.00		10.00			
1940+05	40	15	125	24.00	20.00			66.67	66.67
1941+10	12	15	69	20.00			20.00		20.00
1942+17	6	15	57	10.00		10.00			
1942+90	12	15	69	10.00			20.00		20.00
1943+17	8	15	61	24.00		13.33			
1944+15	6	15	57	30.00		10.00			
1944+90	6	13	51	20.00		8.67			
1945+18	8	14	58	24.00		12.44			
1945+40	6	14	54	10.00		9.33			
1945+70	6	14	54	10.00		9.33			
1946+00	6	14	54	24.00		9.33			
1946+25	20	14	82	24.00	10.00			31.11	31.11
1946+55	6	14	54	24.00		9.33			
1946+70	6	14	54	24.00		9.33			
1946+88	6	14	54	24.00		9.33			
1947+15	8	14	58	24.00		12.44			
1947+50	6	14	54	24.00		9.33			
1948+70	6	14	54	24.00		9.33			
1952+02	6	12	48	20.00		8.00			
1953+02	6	14	54	24.00		9.33			
1954+00	6	12	48	20.00		8.00		LEFT TURN LANE	
1954+00	6	14	54	24.00		9.33			
1955+00	6	12	48	20.00		8.00		LEFT TURN LANE	
1955+00	6	14	54	24.00		9.33			
1956+00	8	14	58	24.00		12.44			
1957+00	6	14	54	24.00		9.33			
1957+50	6	14	54	24.00		9.33			
1958+00	6	14	54	24.00		9.33			
1959+00	6	13	51	20.00		8.67			
1961+04	6	15	57	24.00		10.00			
1963+16	6	14	54	24.00		9.33			
1965+00	6	12	48	20.00		8.00			
1966+00	6	12	48	20.00		8.00			
1967+00	6	12	48	20.00		8.00			
TOTALS			2366	902	30	357	40	98	138

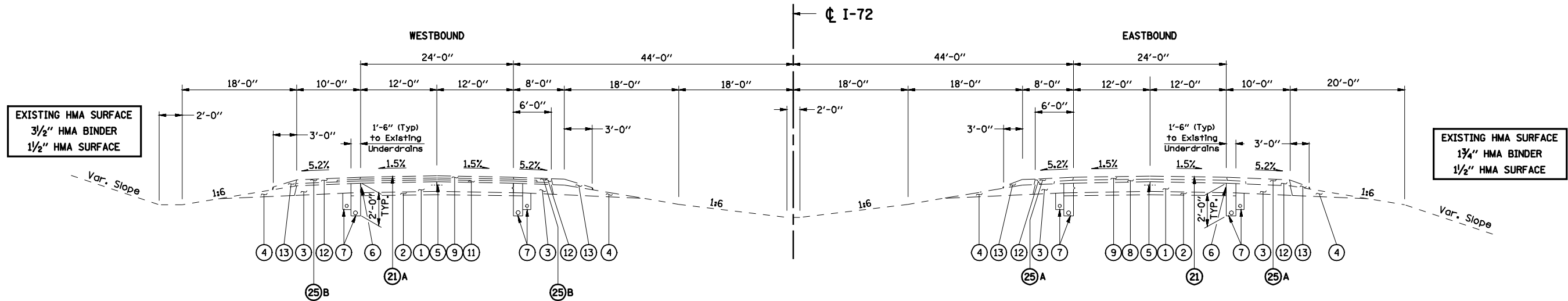
CH-19 PATCHING SCHEDULE (SOUTH BOUND)									
STA	LENGTH	WIDTH	SAW CUTS	DOWEL BARS 1 1/2"	TIE BARS 3/4"	CLASS B PATCHES, TYPE II 8 IN	CLASS B PATCHES, TYPE III 8 IN	CLASS B PATCHES, TYPE IV 8 IN	PAVEMENT FABRIC
			(FOOT)	(EACH)	(EACH)	(SQYD)	(SQYD)	(SQYD)	(SQYD)
1967+00	6	12	48	20.00		8.00			
1966+00	6	12	48	20.00		8.00			
1965+00	6	12	48	20.00		8.00			
1964+00	6	12	48	20.00		8.00			
1962+92	6	13	51	20.00		8.67			
1962+00	6	14	54	24.00		9.33			
1961+65	6	14	54	10.00		9.33			
1961+00	6	15	57	24.00		10.00			
1960+00	6	15	57	24.00		10.00			
1959+00	6	15	57	24.00		10.00			
1958+00	6	15	57	24.00		10.00			
1957+19	6	14	54	24.00		9.33			
1956+99	6	14	54	24.00		9.33			
1956+15	6	14	54	24.00		9.33			
1955+99	6	14	54	24.00		9.33			
1954+99	6	14	54	24.00		9.33			
1953+99	6	14	54	24.00		9.33			
1952+99	6	14	54	24.00		9.33			
1951+97	6	14	54	24.00		9.33			
1948+56	6	14	54	10.00		9.33			
1947+95	6	14	54	10.00		9.33			
1947+54	6	14	54	24.00		9.33			
1946+94	6	17	63	28.00		11.33			
1946+48	6	14	54	24.00		9.33			
1945+95	6	14	54	24.00		9.33			
1945+95	6	12	48	20.00		8.00			
1944+94	6	14	54	24.00		9.33			
1944+94	6	12	48	20.00		8.00		LEFT TURN LANE	
1944+39	6	12	48	20.00		8.00			
1944+02	6	14	54	10.00		9.33			
2943+93	6	12	48	20.00		8.00		LEFT TURN LANE	
2943+71	6	14	54	24.00		9.33			
2942+95	6	14	54	24.00		9.33			
2941+94	6	14	54	24.00		9.33			
2940+94	10	14	62	24.00			15.56		15.56
2939+93	12	14	66	24.00			18.67		18.67
1938+77	40	14	122	10.00	40.00			62.22	62.22
1937+95	6	14	54	24.00		9.33			
1936+93	6	15.5	58.5	24.00		10.33			
1935+87	6	13.5	52.5	20.00		9.00			
1933+84	6	12	48	20.00		8.00			
1933+90	6	12	48	20.00		8.00			
1933+39	6	12	48	20.00		8.00			
1932+91	6	12	48	20.00		8.00			
1932+59	6	12	48	20.00		8.00			
TOTALS			2461	954	40	380	35	63	97

LONGITUDINAL JOINT REPAIR - CH 19				
LOCATION	LENGTH	WIDTH	AREA	DESCRIPTION
	(FOOT)	(FOOT)	(SQYD)	
North Bound =	50	4	22.22	Joint between north bound DL and south bound left turn lane
North Bound =	60	4	26.67	Joint between north bound DL and left turn lane
South Bound =	60	4	26.67	Joint between south bound DL and north bound left turn lane
South Bound =	35	4	15.56	Joint in left turn lane pavement south bound
South Bound =	35	4	15.56	Joint between south bound DL and left turn lane
South Bound =	20	4	8.89	Outside edge of pavement at gore area/island of EB exit ramp.
TOTAL			116	

FOR "PROPOSED LONGITUDINAL JOINT REPAIR DETAIL" SEE SHEET 20 OF 59.



TYPICAL SECTION
STA. 171+09.00 TO STA. 304+87.00



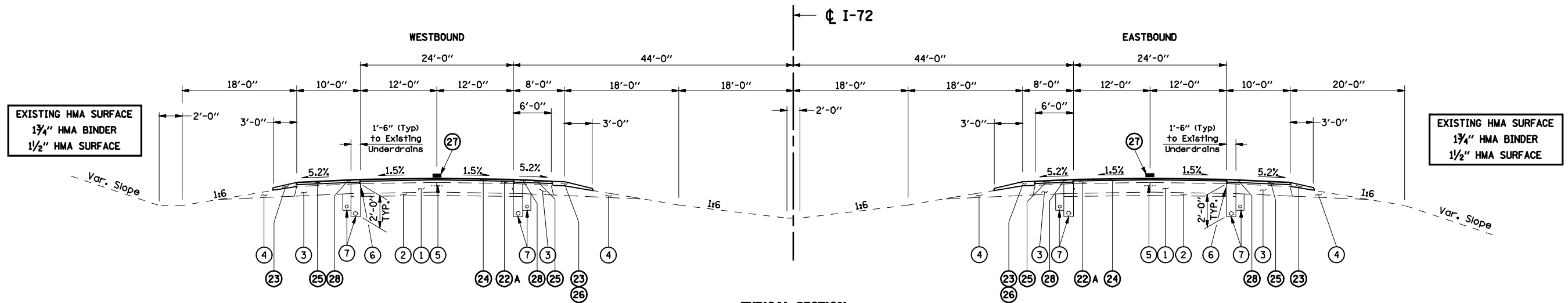
TYPICAL SECTION
STA. 304+87.00 TO STA. 538+00.00

EXISTING LEGEND

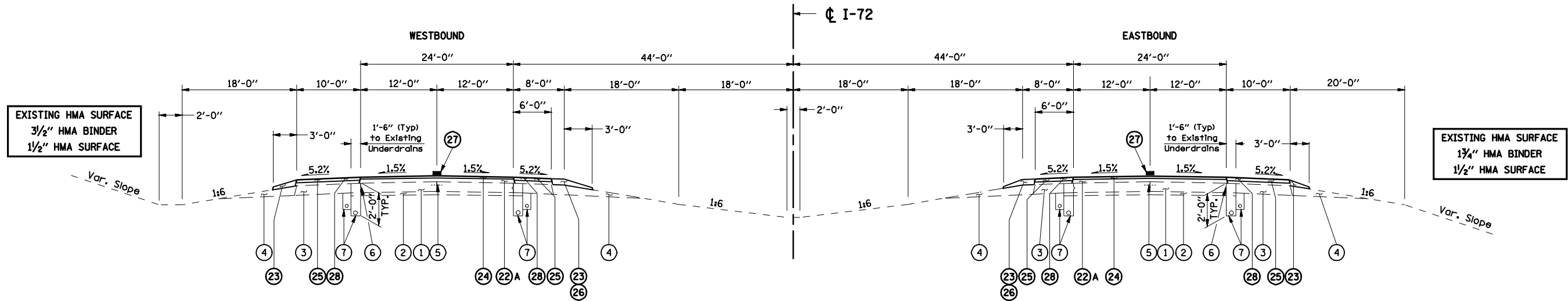
- ① EXIST. C.R.P.C.C. PAVEMENT 8"
- ② EXIST. STABILIZED SUB-BASE 4"
- ③ EXIST. STABILIZED SHOULDER 8"
- ④ EXIST. AGGREGATE SHOULDER, TY. A
- ⑤ EXIST. LONGITUDINAL JOINT
- ⑥ EXIST. KEYWAY
- ⑦ EXIST. PIPE UNDERDRAIN
- ⑧ EXIST. BITUMINOUS CONC. BINDER CSE., MIX. B, TY. 1, 1 3/4"
- ⑨ EXIST. BITUMINOUS CONC. SURF. CSE., MIX. D, CL. 1, TY. 1, 1 1/2"
- ⑩ EXIST. BITUMINOUS CONC. BINDER CSE., MIX. B, TY. 1, 3 1/2"
- ⑪ EXIST. BITUMINOUS SHOULDER (WITH RUMBLE STRIPS)
- ⑫ EXIST. AGGREGATE SHOULDER, TY. B
- ⑬ EXIST. S.R.P.C.C. PAVEMENT 8"
- ⑭ EXIST. COMB. CONC. CURB & GUTTER, TY. B-6.12
- ⑮ EXIST. MEDIAN SURFACE
- ⑯ EXIST. SUB-BASE GRANULAR MATERIAL, TY. C 4"
- ⑰ EXIST. BITUMINOUS MIXTURE COMPLETE 3"
- ⑱ EXIST. AGGREGATE BASE COURSE, TY. A 8"
- ⑳ EXIST. BITUMINOUS CONCRETE WIDENING 9"

PROPOSED LEGEND

- ⑳ PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4"
- ㉑ PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 5"
- ㉒ PROP. POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 2 1/4"
- ㉒A PROP. POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 2 @ 2 1/4", 4 1/2"
- ㉓ PROP. AGGREGATE WEDGE SHOULDER, TYPE B
- ㉔ PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 1 1/2"
- ㉕ PROP. HOT-MIX ASPHALT SHOULDERS, 2" / 2 3/4" (SEE SCHEDULE)
- ㉕A PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
- ㉕B PROP. HOT-MIX-ASPHALT SURFACE REMOVAL, 1"
- ㉖ PROP. AGGREGATE SHOULDER, TY. B
- ㉗ PROP. RAISED REFLECTIVE PAVEMENT MARKER
- ㉘ PROP. SHOULDER RUMBLE STRIP
- ㉙ PROP. HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT



TYPICAL SECTION
STA. 171+09.00 TO STA. 304+87.00



TYPICAL SECTION
STA. 304+87.00 TO STA. 538+00.00

STRUCTURE OMISSION
EASTBOUND STA. 405+68.05 TO STA. 407+52.99
WESTBOUND STA. 405+27.29 TO STA. 407+12.23

EXISTING LEGEND

- ① EXIST. C.R.P.C.C. PAVEMENT 8"
- ② EXIST. STABILIZED SUB-BASE 4"
- ③ EXIST. STABILIZED SHOULDER 8"
- ④ EXIST. AGGREGATE SHOULDER, TY. A
- ⑤ EXIST. LONGITUDINAL JOINT
- ⑥ EXIST. KEYWAY
- ⑦ EXIST. PIPE UNDERDRAIN
- ⑧ EXIST. BITUMINOUS CONC. BINDER CSE., MIX. B, TY. 1, 1 3/4"
- ⑨ EXIST. BITUMINOUS CONC. SURF. CSE., MIX. D, CL. 1, TY. 1, 1 1/2"
- ⑩ EXIST. BITUMINOUS CONC. BINDER CSE., MIX. B, TY. 1, 3 1/2"
- ⑪ EXIST. BITUMINOUS SHOULDER (WITH RUMBLE STRIPS)
- ⑫ EXIST. AGGREGATE SHOULDER, TY. B
- ⑬ EXIST. S.R.P.C.C. PAVEMENT 8"
- ⑭ EXIST. COMB. CONC. CURB & GUTTER, TY. B-6.12
- ⑮ EXIST. MEDIAN SURFACE
- ⑯ EXIST. SUB-BASE GRANULAR MATERIAL, TY. C 4"
- ⑰ EXIST. BITUMINOUS MIXTURE COMPLETE 3"
- ⑱ EXIST. AGGREGATE BASE COURSE, TY. A 8"
- ⑳ EXIST. BITUMINOUS CONCRETE WIDENING 9"

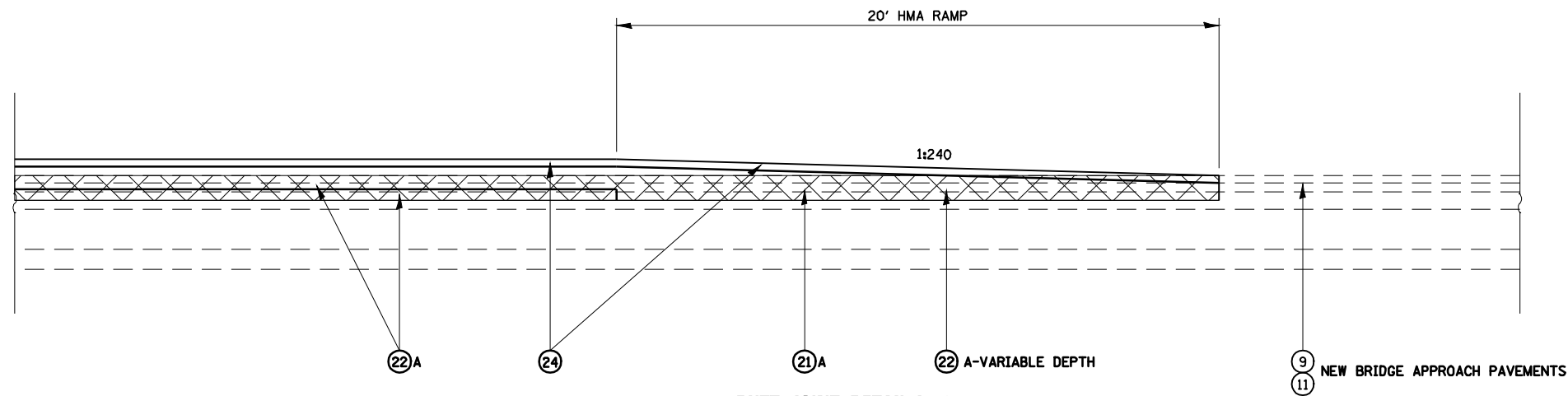
PROPOSED LEGEND

- ⑳ PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 3 1/4"
- ㉑ PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 5"
- ㉒ PROP. POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 2 1/4"
- ㉒A PROP. POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 2 @ 2 1/4", 4 1/2"
- ㉓ PROP. AGGREGATE WEDGE SHOULDER, TYPE B
- ㉔ PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 1 1/2"
- ㉕ PROP. HOT-MIX ASPHALT SHOULDERS, 2" / 2 3/4" (SEE SCHEDULE)
- ㉕A PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
- ㉕B PROP. HOT-MIX-ASPHALT SURFACE REMOVAL, 1"
- ㉖ PROP. AGGREGATE SHOULDER, TY. B
- ㉗ PROP. RAISED REFLECTIVE PAVEMENT MARKER
- ㉘ PROP. SHOULDER RUMBLE STRIP
- ㉙ PROP. HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

NOTE:
CONSTRUCT SHOULDER RUMBLE STRIPS ALONG I-72 SHOULDERS. (SEE STD 642001-01)

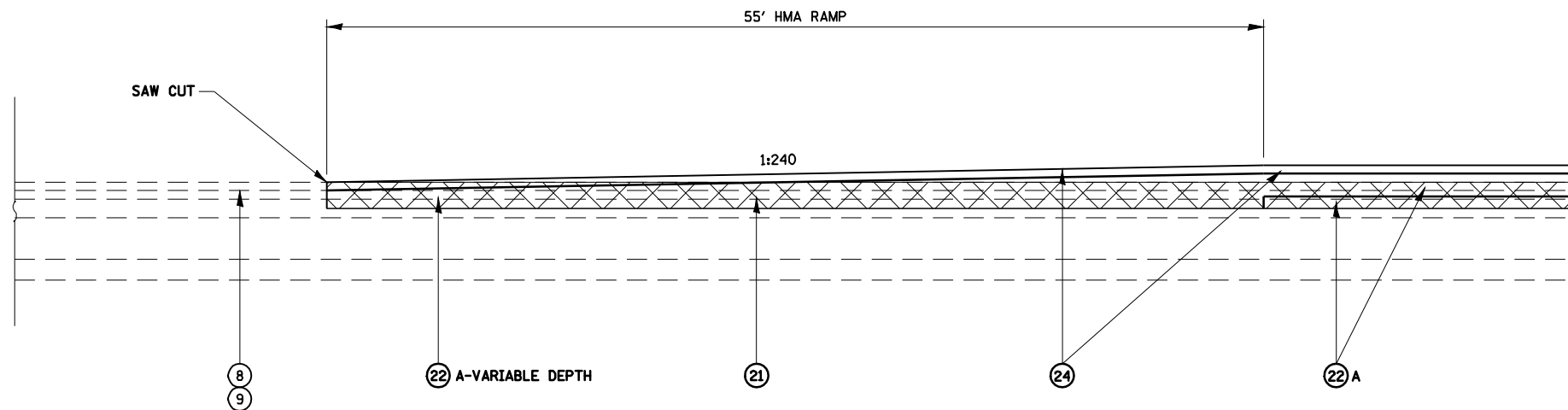
MEDIAN CROSS-OVERS RESURFACE WITH 2 3/4" HMA SURFACE COURSE, MIX "C", N50

FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED TYPICAL SECTIONS F.A.I. ROUTE 72 (I-72)			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pwwork\pwwid\LAUGHLINRL\0183140\101017016TS01.SHT	DRAWN - AAD	REVISED -	72					(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	23	
PLOT SCALE = NONE	CHECKED - JAC	REVISED -	CONTRACT NO. 72910									
PLOT DATE = Mar-17-2010 08:16:13AM	DATE - 06/11/07	REVISED -	FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT									



BUTT JOINT DETAIL I-72

WB STA. 404+68.55 TO STA. 404+88.55
 WB STA. 407+41.39 TO STA. 407+61.39
 WB STA. 537+80.00 TO STA. 538+00.00



BUTT JOINT DETAIL I-72

EB STA. 171+09.00 TO STA. 171+64.00
 EB STA. 404+83.61 TO STA. 405+38.61
 EB STA. 407+91.45 TO STA. 408+46.45
 EB STA. 537+45.00 TO STA. 538+00.00
 WB STA. 171+09.00 TO STA. 171+64.00

EXISTING LEGEND

- ① EXIST. C.R.P.C.C. PAVEMENT 8"
- ② EXIST. STABILIZED SUB-BASE 4"
- ③ EXIST. STABILIZED SHOULDER 8"
- ④ EXIST. AGGREGATE SHOULDER, TY. A
- ⑤ EXIST. LONGITUDINAL JOINT
- ⑥ EXIST. KEYWAY
- ⑦ EXIST. PIPE UNDERDRAIN
- ⑧ EXIST. BITUMINOUS CONC. BINDER CSE., MIX. B, TY. 1, 1 3/4"
- ⑨ EXIST. BITUMINOUS CONC. SURF. CSE., MIX. D, CL. 1, TY. 1, 1 1/2"
- ⑩ EXIST. BITUMINOUS CONC. BINDER CSE., MIX. B, TY. 1, 3 1/2"
- ⑪ EXIST. BITUMINOUS SHOULDER (WITH RUMBLE STRIPS)
- ⑫ EXIST. AGGREGATE SHOULDER, TY. B
- ⑬ EXIST. S.R.P.C.C. PAVEMENT 8"
- ⑭ EXIST. COMB. CONC. CURB & GUTTER, TY. B-6.12
- ⑮ EXIST. MEDIAN SURFACE
- ⑯ EXIST. SUB-BASE GRANULAR MATERIAL, TY. C 4"
- ⑰ EXIST. BITUMINOUS MIXTURE COMPLETE 3"
- ⑱ EXIST. AGGREGATE BASE COURSE, TY. A 8"
- ⑳ EXIST. BITUMINOUS CONCRETE WIDENING 9"

PROPOSED LEGEND

- ⑳ PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"
- ㉑ PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 5"
- ㉒ PROP. POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 2 1/4"
- ㉒A PROP. POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 2 @ 2 1/4", 4 1/2"
- ㉓ PROP. AGGREGATE WEDGE SHOULDER, TYPE B
- ㉔ PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 1 1/2"
- ㉕ PROP. HOT-MIX ASPHALT SHOULDERS, 2" / 2 3/4" (SEE SCHEDULE)
- ㉕A PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
- ㉕B PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 1"
- ㉖ PROP. AGGREGATE SHOULDER, TY. B
- ㉗ PROP. RAISED REFLECTIVE PAVEMENT MARKER
- ㉘ PROP. SHOULDER RUMBLE STRIP
- ㉙ PROP. HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

NOTE:
 CONSTRUCT SHOULDER RUMBLE STRIPS ALONG I-72 SHOULDERS. (SEE STD 642001-01)

 MEDIAN CROSS-OVERS RESURFACE WITH 2 3/4" HMA SURFACE COURSE, MIX "C", N50

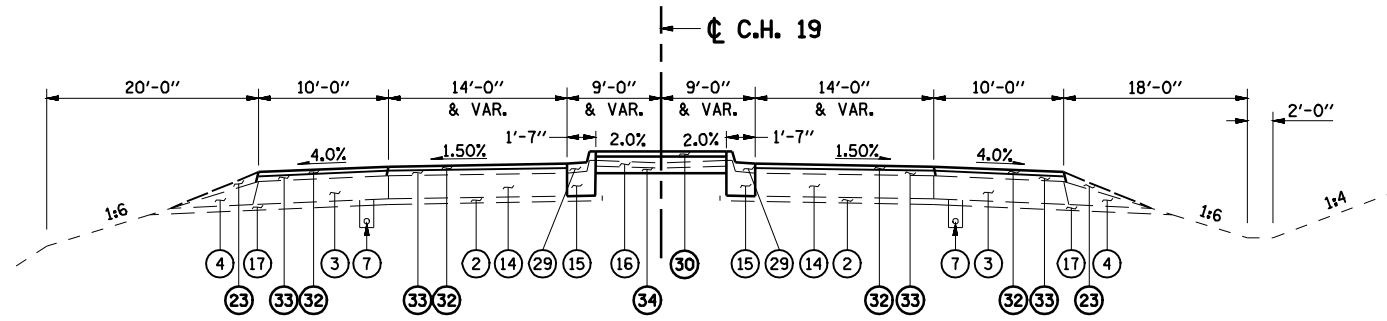
FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -
e:\pwwork\pwwid\LAUGHLINRL\0183140\10	OTRevised_7016TS01.SHT	DRAWN - AAD	REVISED -
	PLOT SCALE = NONE	CHECKED - JAC	REVISED -
	PLOT DATE = Mar-17-2010 08:16:16AM	DATE - 06/11/07	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

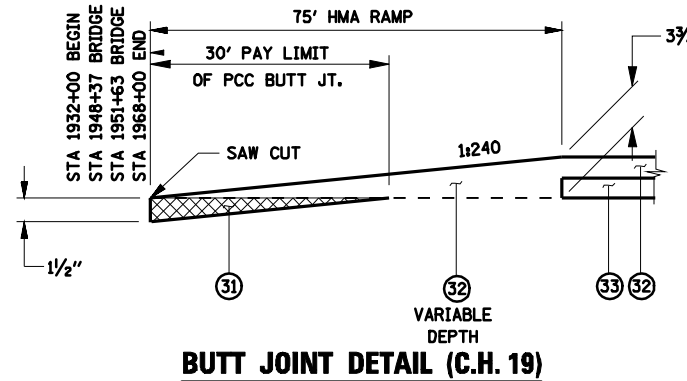
**PROPOSED TYPICAL SECTIONS
 F.A.I. ROUTE 72 (I-72) - BUTT JOINT DETAILS**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

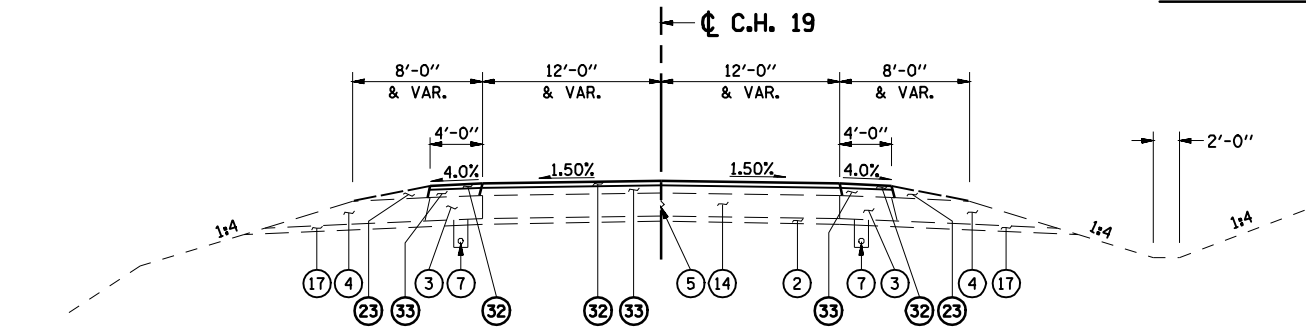
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	23A	23A
CONTRACT NO. 72910				
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				



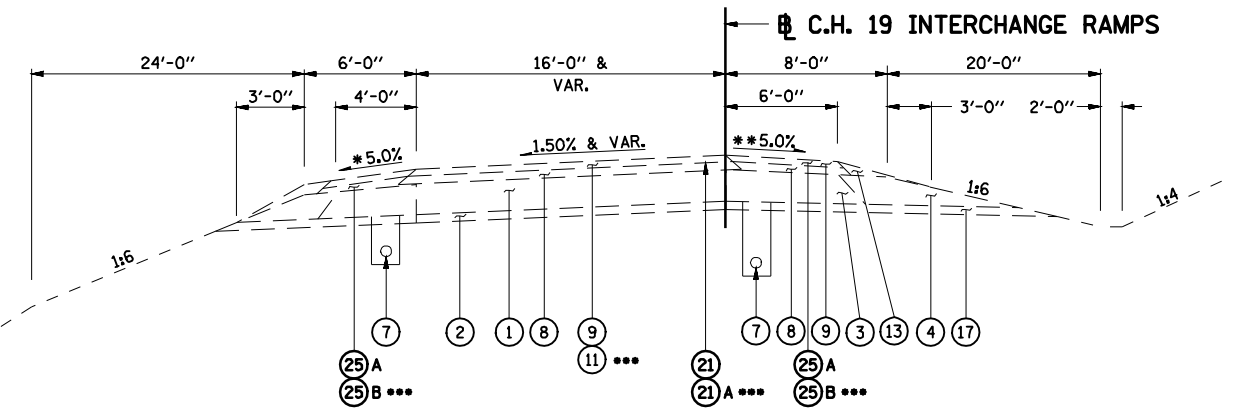
TYPICAL SECTION
 STA. 1938+69.00 TO STA. 1961+31.00
 STRUCTURE OMISSION
 STA. 1948+77.50 TO STA. 1951+22.50
 APPROACH SLAB OMISSIONS
 STA. 1948+37.50 TO STA. 1948+77.50
 STA. 1951+22.50 TO STA. 1951+62.50



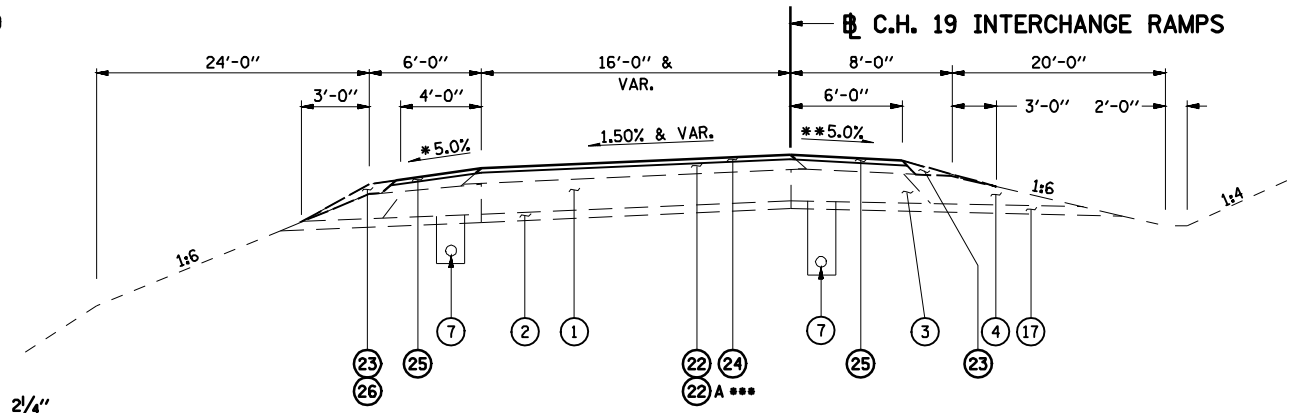
BUTT JOINT DETAIL (C.H. 19)



TYPICAL SECTION
 STA. 1932+00.00 TO STA. 1938+69.00
 STA. 1961+31.00 TO STA. 1968+00.00



EXISTING TYPICAL SECTION
 * - 5.2% OR S.E. RATE WHICHEVER IS GREATER
 ** - MAXIMUM PAVEMENT-SHOULDER BREAKOVER SHALL BE 8.0%
 *** RAMP A STA. 500+00.00 TO STA. 512+80.00
 RAMP A STA. 512+80.00 TO STA. 520+75.27
 RAMP B STA. 600+23.00 TO STA. 608+55.00
 *** RAMP B STA. 608+55.00 TO STA. 625+07.14
 *** RAMP C STA. 700+00.00 TO STA. 712+80.00
 RAMP C STA. 712+80.00 TO STA. 720+75.27
 *** RAMP D STA. 800+23.00 TO STA. 808+55.00



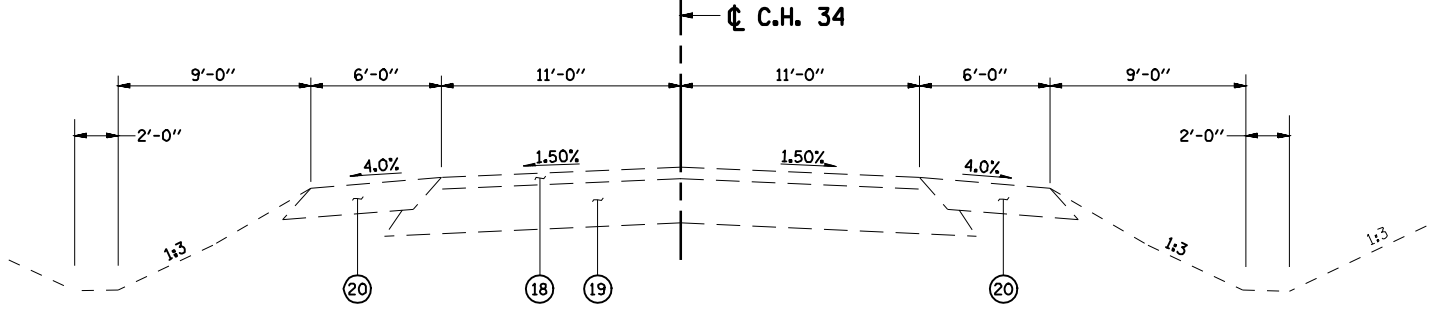
PROPOSED TYPICAL SECTION
 * - 5.2% OR S.E. RATE WHICHEVER IS GREATER
 ** - MAXIMUM PAVEMENT-SHOULDER BREAKOVER SHALL BE 8.0%
 *** RAMP A STA. 500+00.00 TO STA. 512+80.00
 RAMP A STA. 512+80.00 TO STA. 520+75.27
 RAMP B STA. 600+23.00 TO STA. 608+55.00
 *** RAMP B STA. 608+55.00 TO STA. 625+07.14
 *** RAMP C STA. 700+00.00 TO STA. 712+80.00
 RAMP C STA. 712+80.00 TO STA. 720+75.27
 *** RAMP D STA. 800+23.00 TO STA. 808+55.00
 *** RAMP D STA. 808+55.00 TO STA. 825+07.14

EXISTING LEGEND

- ① EXIST. C.R.P.C.C. PAVEMENT 8"
- ② EXIST. STABILIZED SUB-BASE 4"
- ③ EXIST. STABILIZED SHOULDER 8"
- ④ EXIST. AGGREGATE SHOULDER, TY. A
- ⑤ EXIST. LONGITUDINAL JOINT
- ⑥ EXIST. KEYWAY
- ⑦ EXIST. PIPE UNDERDRAIN
- ⑧ EXIST. BITUMINOUS CONC. BINDER CSE., MIX. B, TY. 1, 1 3/4"
- ⑨ EXIST. BITUMINOUS CONC. SURF. CSE., MIX. D, CL. 1, TY. 1, 1 1/2"
- ⑩ EXIST. BITUMINOUS CONC. BINDER CSE., MIX. B, TY. 1, 3"
- ⑪ EXIST. BITUMINOUS CONC. BINDER CSE., MIX. B, TY. 1, 3 1/2"
- ⑫ EXIST. BITUMINOUS SHOULDER (WITH RUMBLE STRIPS)
- ⑬ EXIST. AGGREGATE SHOULDER, TY. B
- ⑭ EXIST. S.R.P.C.C. PAVEMENT 8"
- ⑮ EXIST. COMB. CONC. CURB & GUTTER, TY. B-6.12
- ⑯ EXIST. MEDIAN SURFACE
- ⑰ EXIST. SUB-BASE GRANULAR MATERIAL, TY. C 4"
- ⑱ EXIST. BITUMINOUS MIXTURE COMPLETE 3"
- ⑲ EXIST. AGGREGATE BASE COURSE, TY. A 8"
- ⑳ EXIST. BITUMINOUS CONCRETE WIDENING 9"

PROPOSED LEGEND

- ⑳ PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4"
- ㉑ PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 5"
- ㉒ PROP. POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 2 1/4"
- ㉓ PROP. POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 2 @ 2 1/4", 4 1/2"
- ㉔ PROP. AGGREGATE WEDGE SHOULDER, TYPE B
- ㉕ PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 1 1/2"
- ㉖ PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 2 1/4"
- ㉗ PROP. HOT-MIX ASPHALT SHOULDERS, 2"/ 2 3/4" (SEE SCHEDULE)
- ㉘ PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
- ㉙ PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 1"
- ㉚ PROP. AGGREGATE SHOULDER, TY. B
- ㉛ PROP. RAISED REFLECTIVE PAVEMENT MARKER
- ㉜ PROP. SHOULDER RUMBLE STRIP
- ㉝ PROP. CONCRETE CURB AND GUTTER, TY M-4.12
- ㉞ PROP. CONCRETE MEDIAN SURFACE, 4 INCH
- ㉟ PROP. P.C.C. SURFACE REMOVAL - BUTT JOINT
- ㊱ PROP. HOT-MIX-ASPHALT SURFACE CSE, MIX C, N50, 1 1/2"
- ㊲ PROP. HOT-MIX-ASPHALT BINDER CSE, IL 19.0, N50, 2 1/4"
- ㊳ PROP. SUB-BASE GRANULAR MATERIAL, TYPE B



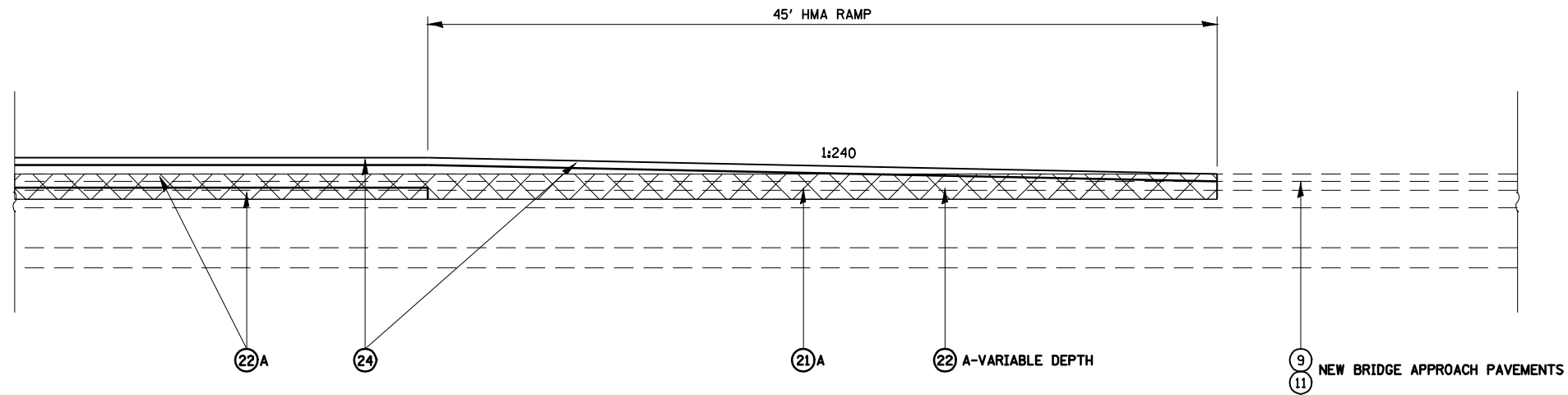
TYPICAL SECTION
 STA. 3447+54.00 TO STA. 3452+46.00
 STRUCTURE OMISSION
 STA. 3448+77.50 TO STA. 3451+22.50

FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -
es:\pwwork\p\WIDOT\LAUGHLINRL\0183140\101017\Revised_7016TS02.SHT		DRAWN - AAD	REVISED -
PLOT SCALE = NONE		CHECKED - JAC	REVISED -
PLOT DATE = Mar-17-2010 08:16:18AM		DATE - 06/11/07	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PROPOSED TYPICAL SECTIONS INTERCHANGE RAMPS, CH 19, CH 34			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	24
CONTRACT NO. 72910				
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				



HMA RAMP DETAIL (INTERCHANGE RAMPS)

RAMP A STA. 5512+35.00 TO STA. 512+80.00
 RAMP B STA. 608+00.00 TO STA. 608+55.00
 RAMP C STA. 712+35.00 TO STA. 712+80.00
 RAMP D STA. 808+00.00 TO STA. 808+55.00

EXISTING LEGEND

- 1 EXIST. C.R.P.C.C. PAVEMENT 8"
- 2 EXIST. STABILIZED SUB-BASE 4"
- 3 EXIST. STABILIZED SHOULDER 8"
- 4 EXIST. AGGREGATE SHOULDER, TY. A
- 5 EXIST. LONGITUDINAL JOINT
- 6 EXIST. KEYWAY
- 7 EXIST. PIPE UNDERDRAIN
- 8 EXIST. BITUMINOUS CONC. BINDER CSE., MIX. B, TY. 1, 1 3/4"
- 9 EXIST. BITUMINOUS CONC. SURF. CSE., MIX. D, CL. 1, TY. 1, 1 1/2"
- 10 EXIST. BITUMINOUS CONC. BINDER CSE., MIX. B, TY. 1, 3"
- 11 EXIST. BITUMINOUS CONC. BINDER CSE., MIX. B, TY. 1, 3 1/2"
- 12 EXIST. BITUMINOUS SHOULDER (WITH RUMBLE STRIPS)
- 13 EXIST. AGGREGATE SHOULDER, TY. B
- 14 EXIST. S.R.P.C.C. PAVEMENT 8"
- 15 EXIST. COMB. CONC. CURB & GUTTER, TY. B-6.12
- 16 EXIST. MEDIAN SURFACE
- 17 EXIST. SUB-BASE GRANULAR MATERIAL, TY. C 4"
- 18 EXIST. BITUMINOUS MIXTURE COMPLETE 3"
- 19 EXIST. AGGREGATE BASE COURSE, TY. A 8"
- 20 EXIST. BITUMINOUS CONCRETE WIDENING 9"

PROPOSED LEGEND

- 21 PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"
- 21 A PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 5"
- 22 PROP. POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 2 1/4"
- 22 A PROP. POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 2 @ 2 1/4", 4 1/2"
- 23 PROP. AGGREGATE WEDGE SHOULDER, TYPE B
- 24 PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 1 1/2"
- 24 A PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 2 1/4"
- 25 PROP. HOT-MIX ASPHALT SHOULDERS, 2" / 2 3/4" (SEE SCHEDULE)
- 25 A PROP HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
- 25 B PROP HOT-MIX ASPHALT SURFACE REMOVAL, 1"
- 26 PROP. AGGREGATE SHOULDER, TY. B
- 27 PROP. RAISED REFLECTIVE PAVEMENT MARKER
- 28 PROP. SHOULDER RUMBLE STRIP
- 29 PROP. CONCRETE CURB AND GUTTER, TY M-4.12
- 30 PROP. CONCRETE MEDIAN SURFACE, 4 INCH
- 31 PROP. P.C.C. SURFACE REMOVAL - BUTT JOINT
- 32 PROP. HOT-MIX-ASPHALT SURFACE CSE, MIX C, N50, 1 1/2"
- 33 PROP. HOT-MIX-ASPHALT BINDER CSE, IL 19.0, N50. 2 1/4"
- 34 PROP. SUB-BASE GRANULAR MATERIAL, TYPE B

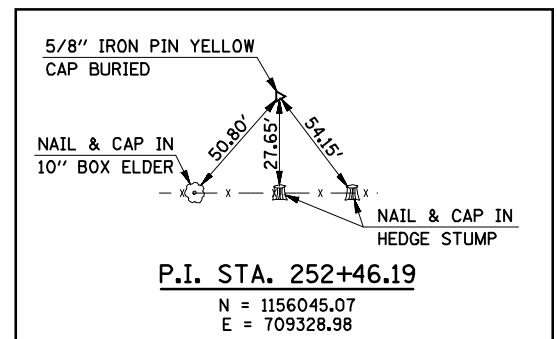
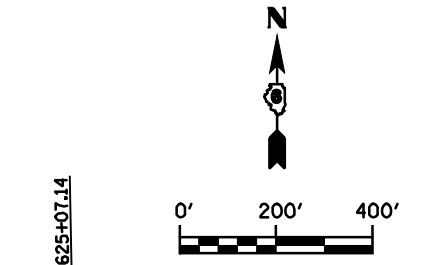
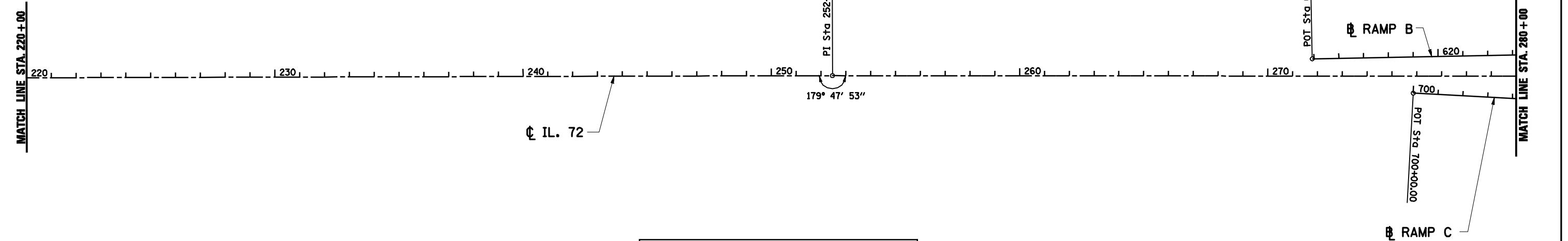
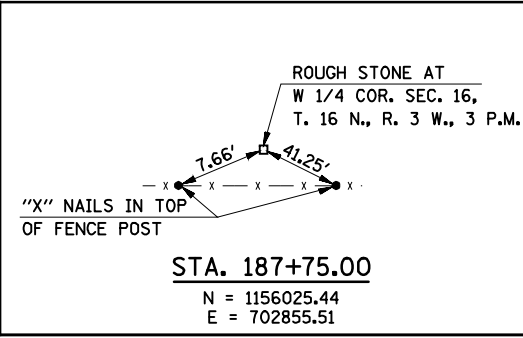
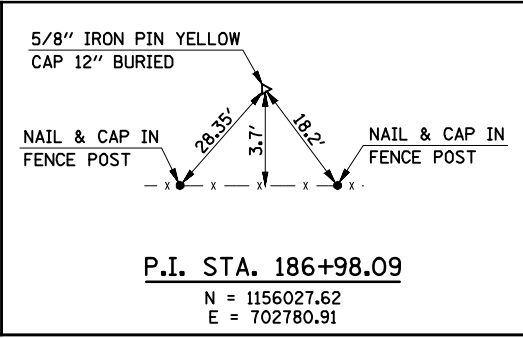
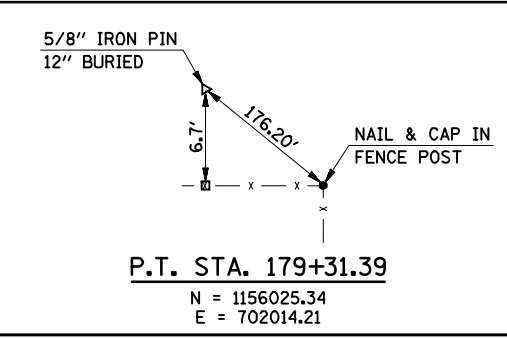
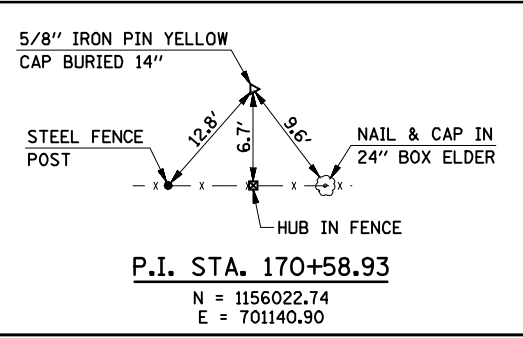
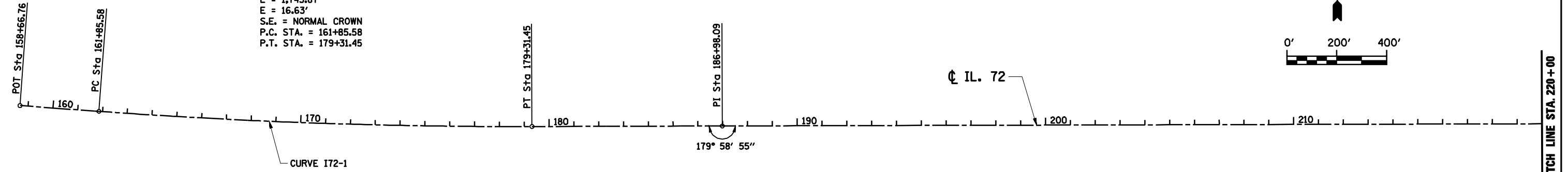
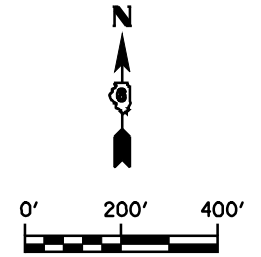
FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -
e:\pwork\pwork\DOT\LAUGHLINRL\0183140\DOT\Revised_7016TS02.SHT		DRAWN - AAD	REVISED -
PLOT SCALE = NONE		CHECKED - JAC	REVISED -
PLOT DATE = Mar-17-2010 08:16:21AM		DATE - 06/11/07	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROPOSED TYPICAL SECTIONS			
INTERCHANGE RAMPS, CH 19, CH 34 - HMA RAMP DETAIL			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	24A	24A
CONTRACT NO. 72910				
FED. ROAD DIST. NO. 2 (ILLINOIS) FED. AID PROJECT				

PROP. CURVE I72-1
 PI STA. = 170+58.94
 $\Delta = 4^\circ 21' 53''$ (LT)
 D = $0^\circ 15' 00''$
 R = 22,918.31'
 T = 873.36'
 L = 1,745.87'
 E = 16.63'
 S.E. = NORMAL CROWN
 P.C. STA. = 161+85.58
 P.T. STA. = 179+31.45



FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -
e:\pwwork\pwidot\LAUGHLINRL\0184389\7016AT01.SHT		DRAWN - AAD	REVISED -
		CHECKED - JAC	REVISED -
		DATE - 06/11/07	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**I-72 ALIGNMENT
STA. 159 + 00 TO STA. 280 + 00**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

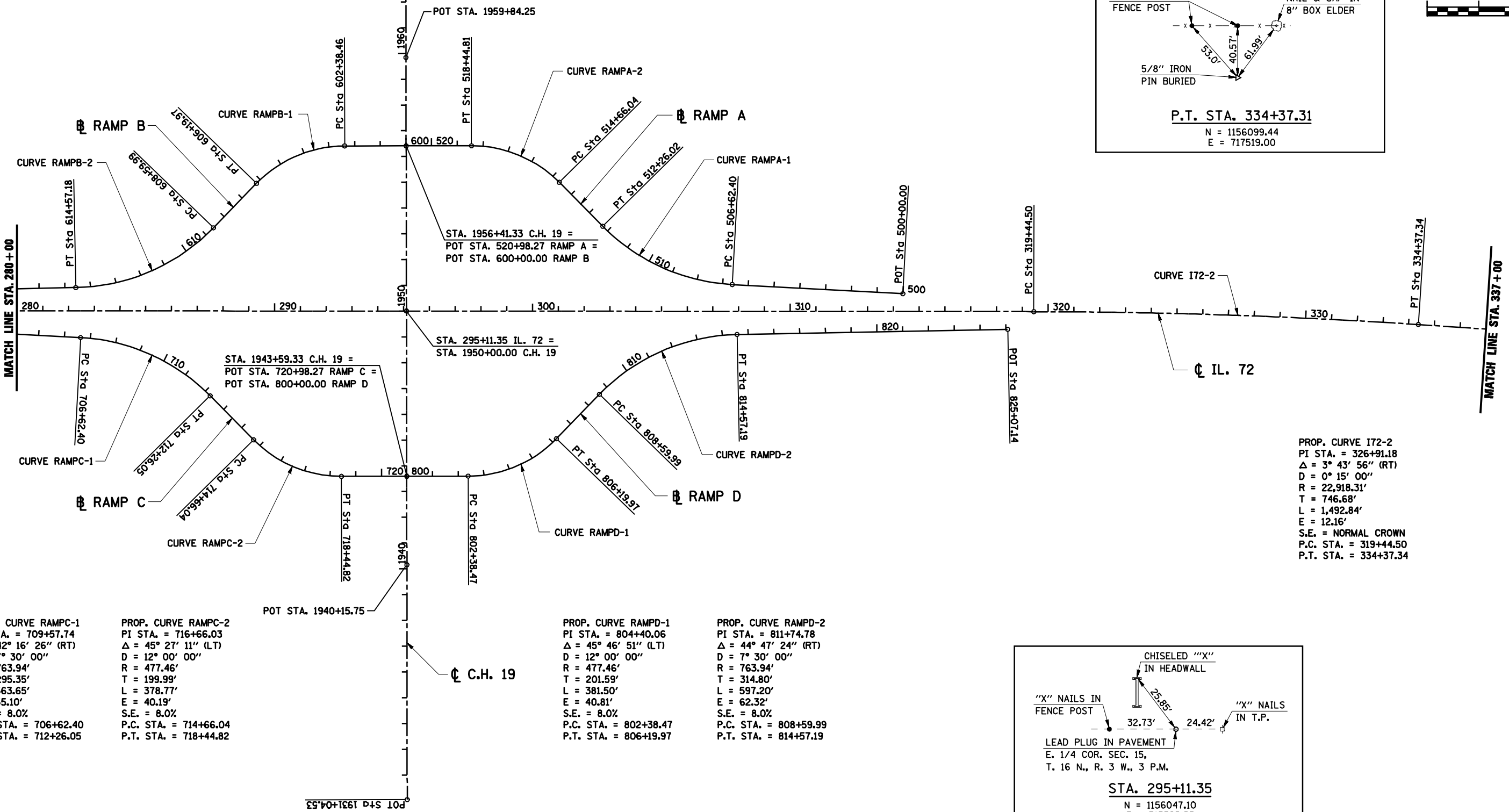
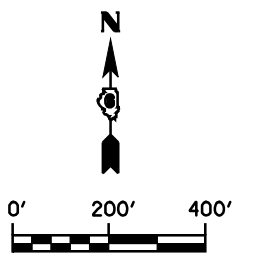
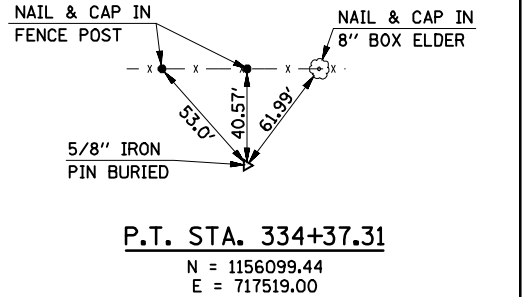
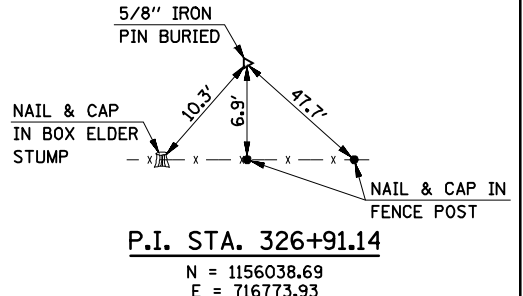
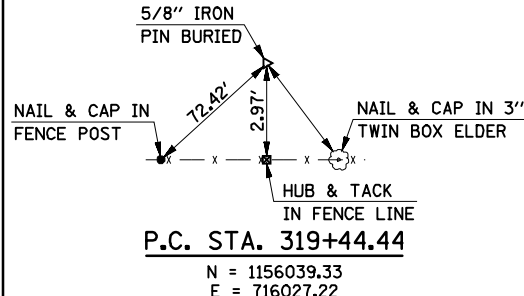
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	25
CONTRACT NO. 72910				
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

PROP. CURVE RAMPB-2
 PI STA. = 611+74.78
 $\Delta = 44^\circ 47' 23''$ (RT)
 D = 7° 30' 00"
 R = 763.94'
 T = 314.79'
 L = 597.19'
 E = 62.32'
 S.E. = 8.0%
 P.C. STA. = 608+59.99
 P.T. STA. = 614+57.18

PROP. CURVE RAMPB-1
 PI STA. = 604+40.05
 $\Delta = 45^\circ 46' 54''$ (LT)
 D = 12° 00' 00"
 R = 477.46'
 T = 201.60'
 L = 381.51'
 E = 40.82'
 S.E. = 8.0%
 P.C. STA. = 602+38.46
 P.T. STA. = 606+19.97

PROP. CURVE RAMP A-2
 PI STA. = 516+66.03
 $\Delta = 45^\circ 27' 09''$ (LT)
 D = 12° 00' 00"
 R = 477.46'
 T = 199.98'
 L = 378.77'
 E = 40.19'
 S.E. = 8.0%
 P.C. STA. = 514+66.04
 P.T. STA. = 518+44.81

PROP. CURVE RAMP A-1
 PI STA. = 509+57.73
 $\Delta = 42^\circ 16' 19''$ (RT)
 D = 7° 30' 00"
 R = 763.94'
 T = 295.33'
 L = 563.62'
 E = 55.10'
 S.E. = 8.0%
 P.C. STA. = 506+62.40
 P.T. STA. = 512+26.02



PROP. CURVE RAMP C-1
 PI STA. = 709+57.74
 $\Delta = 42^\circ 16' 26''$ (RT)
 D = 7° 30' 00"
 R = 763.94'
 T = 295.35'
 L = 563.65'
 E = 55.10'
 S.E. = 8.0%
 P.C. STA. = 706+62.40
 P.T. STA. = 712+26.05

PROP. CURVE RAMP C-2
 PI STA. = 716+66.03
 $\Delta = 45^\circ 27' 11''$ (LT)
 D = 12° 00' 00"
 R = 477.46'
 T = 199.99'
 L = 378.77'
 E = 40.19'
 S.E. = 8.0%
 P.C. STA. = 714+66.04
 P.T. STA. = 718+44.82

POT STA. 1940+15.75

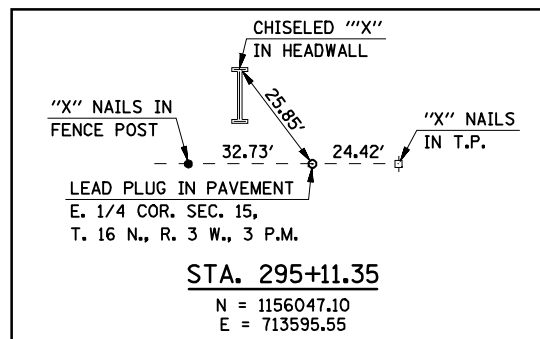
STA. 1943+59.33 C.H. 19 =
 POT STA. 720+98.27 RAMP C =
 POT STA. 800+00.00 RAMP D

STA. 1956+41.33 C.H. 19 =
 POT STA. 520+98.27 RAMP A =
 POT STA. 600+00.00 RAMP B

STA. 295+11.35 IL. 72 =
 STA. 1950+00.00 C.H. 19

PROP. CURVE RAMP D-1
 PI STA. = 804+40.06
 $\Delta = 45^\circ 46' 51''$ (LT)
 D = 12° 00' 00"
 R = 477.46'
 T = 201.59'
 L = 381.50'
 E = 40.81'
 S.E. = 8.0%
 P.C. STA. = 802+38.47
 P.T. STA. = 806+19.97

PROP. CURVE RAMP D-2
 PI STA. = 811+74.78
 $\Delta = 44^\circ 47' 24''$ (RT)
 D = 7° 30' 00"
 R = 763.94'
 T = 314.80'
 L = 597.20'
 E = 62.32'
 S.E. = 8.0%
 P.C. STA. = 808+59.99
 P.T. STA. = 814+57.19



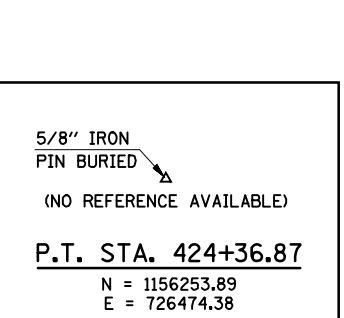
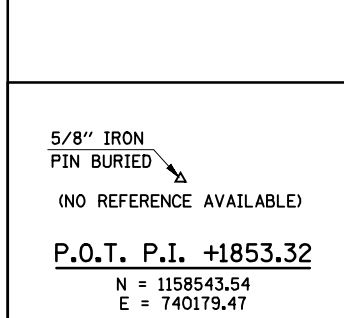
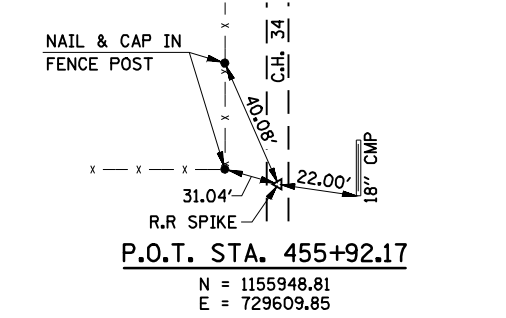
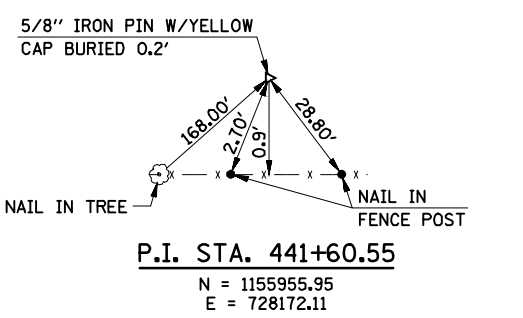
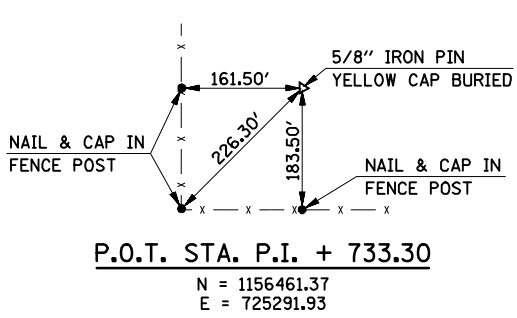
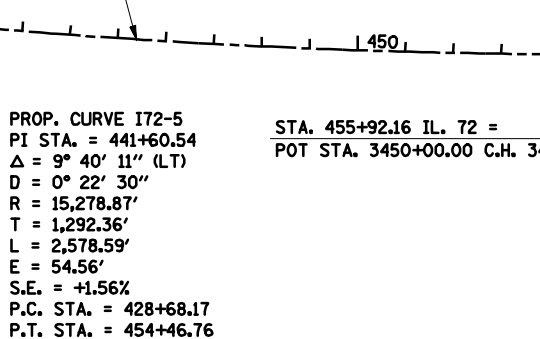
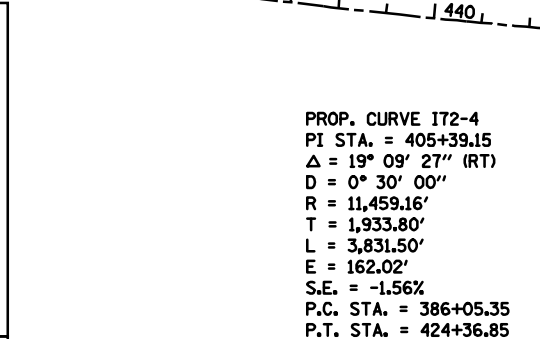
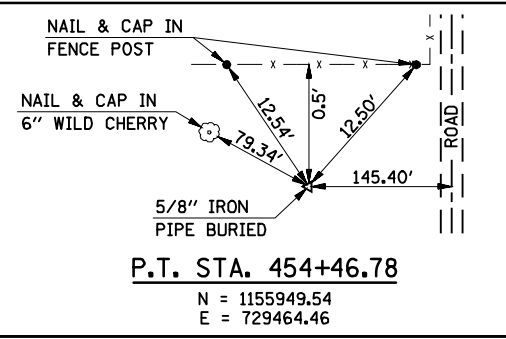
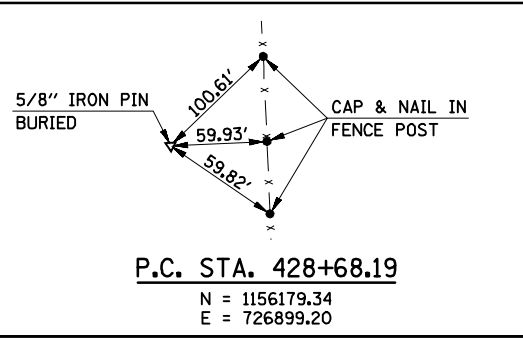
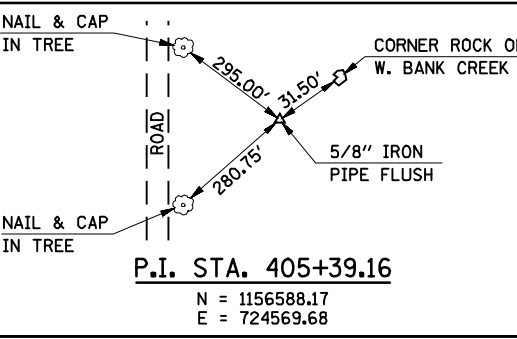
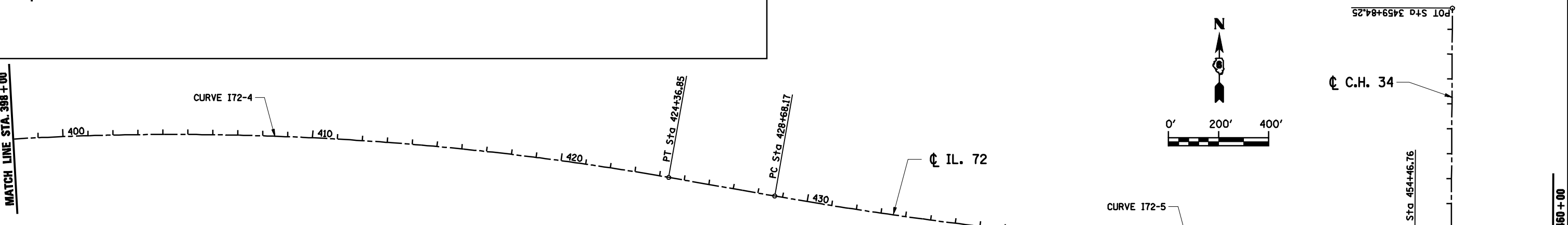
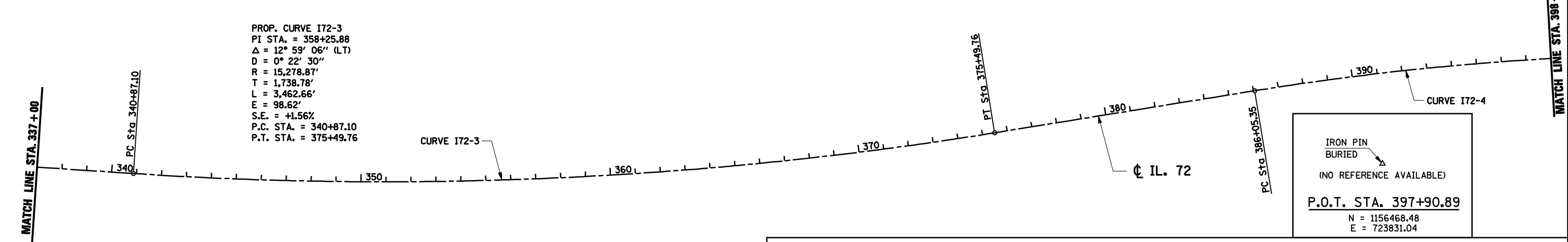
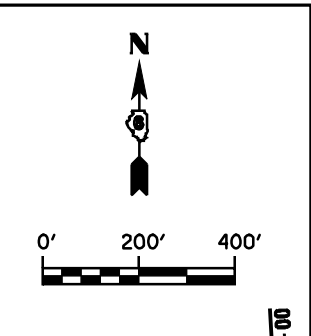
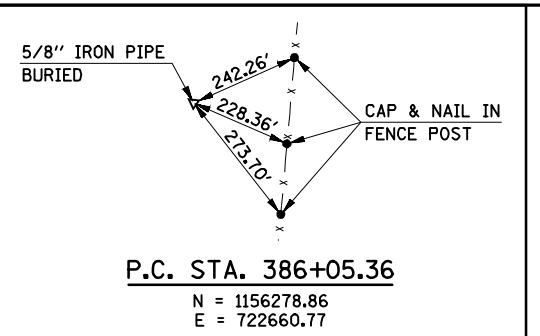
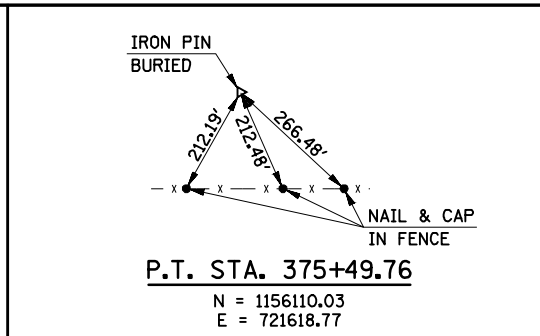
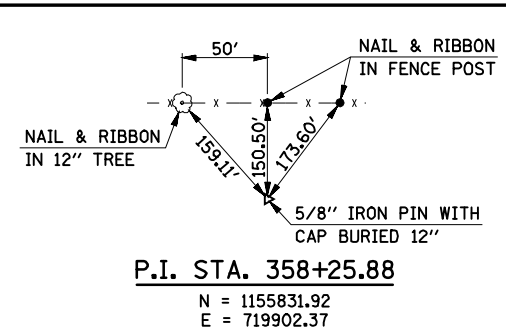
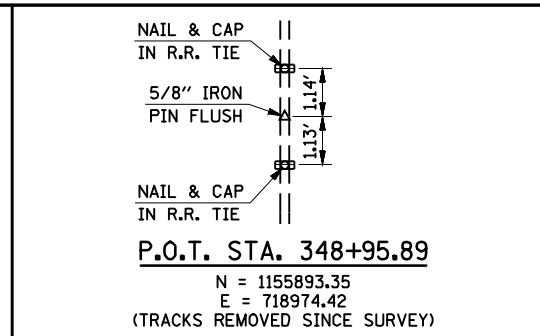
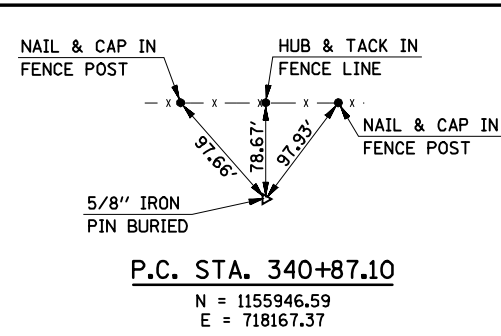
PROP. CURVE I72-2
 PI STA. = 326+91.18
 $\Delta = 3^\circ 43' 56''$ (RT)
 D = 0° 15' 00"
 R = 22,918.31'
 T = 746.68'
 L = 1,492.84'
 E = 12.16'
 S.E. = NORMAL CROWN
 P.C. STA. = 319+44.50
 P.T. STA. = 334+37.34

FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -
es:\pwwork\p\WIDOT\LAUGHLINRL\0184389\16AT02.SHT		DRAWN - AAD	REVISED -
		CHECKED - JAC	REVISED -
		DATE - 06/11/07	REVISED -

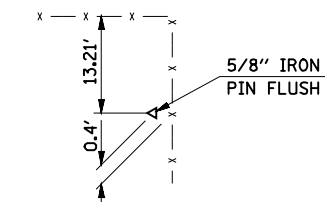
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

I-72 ALIGNMENT			
STA. 280 + 00 TO STA. 337 + 00			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	26
CONTRACT NO. 72910				
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

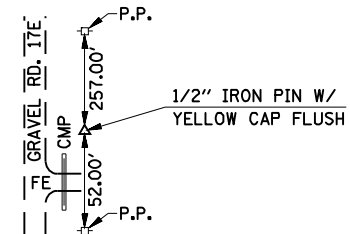


FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-72 ALIGNMENT STA. 337+00 TO STA. 460+00	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
es:\pwwork\pwwork\LAUGHLINRL\0184389\7016AT03.SHT		DRAWN - AAD	REVISED -			72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	27	
PLOT SCALE = NONE		CHECKED - JAC	REVISED -			CONTRACT NO. 72910					
PLOT DATE = Feb-11-2010 08:58:27AM		DATE - 06/11/07	REVISED -			FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT					
				SCALE:	SHEET NO.	OF	SHEETS	STA.	TO	STA.	



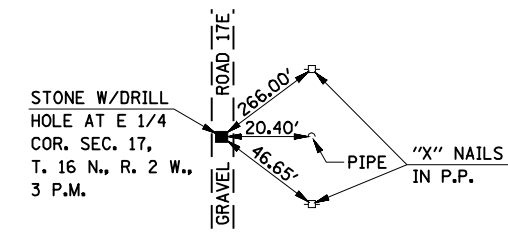
P.O.T. STA. 482+47.39

N = 1155935.64
E = 732265.09



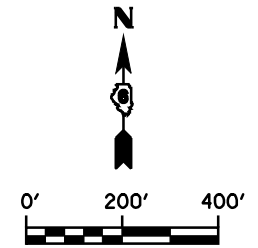
P.O.T. STA. 509+23.00

N = 1155922.37
E = 734940.61

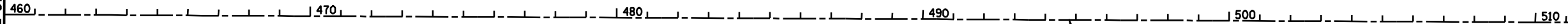


STA. 509+04.68, 8.60' RT.

N = 1155913.86
E = 734922.28



MATCH LINE STA. 460+00



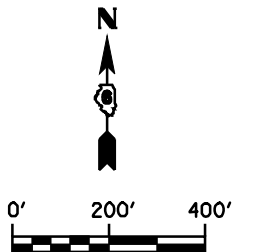
CL IL. 72

MATCH LINE STA. 520+00

5/8" IRON
PIN BURIED
(NO REFERENCE AVAILABLE)

P.O.T. STA. 536+70.77

N = 1155908.74
E = 737688.35



MATCH LINE STA. 520+00



CL IL. 72

FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -
e:\pwwork\pwidot\LAUGHLINRL\184389\7016AT04.SHT		DRAWN - AAD	REVISED -
PLOT SCALE = NONE		CHECKED - JAC	REVISED -
PLOT DATE = Feb-11-2010 08:58:29AM		DATE - 06/11/07	REVISED -

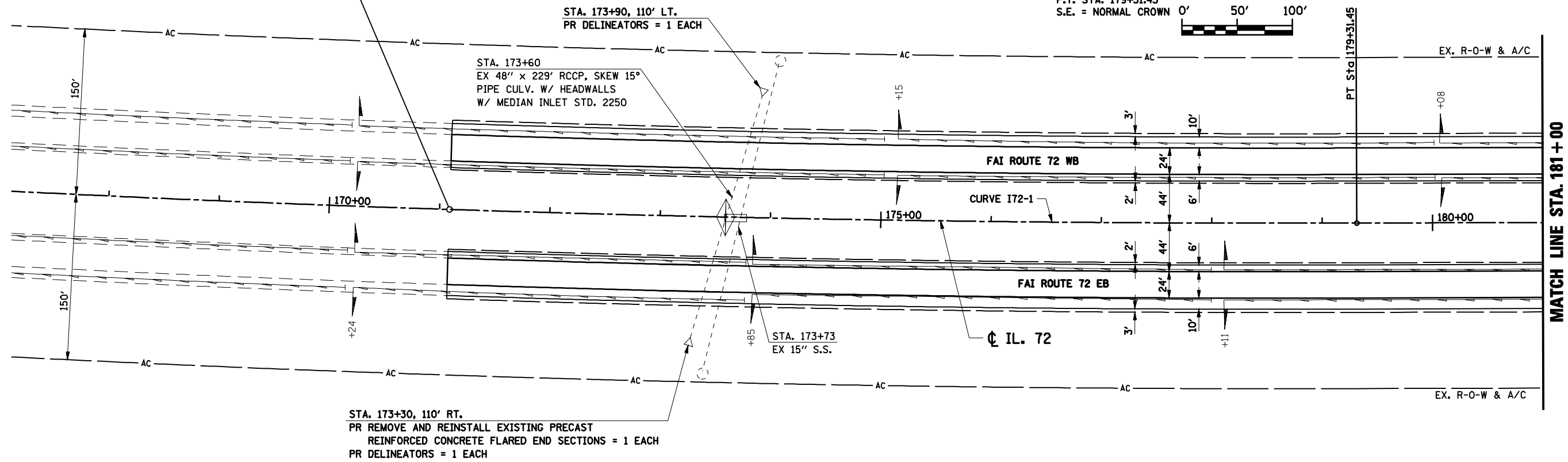
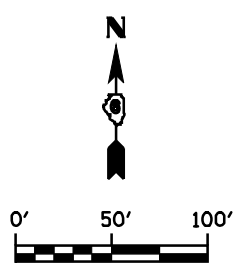
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL-72 ALIGNMENT			
STA. 460+00 TO 570+00			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	28
CONTRACT NO. 72910				
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

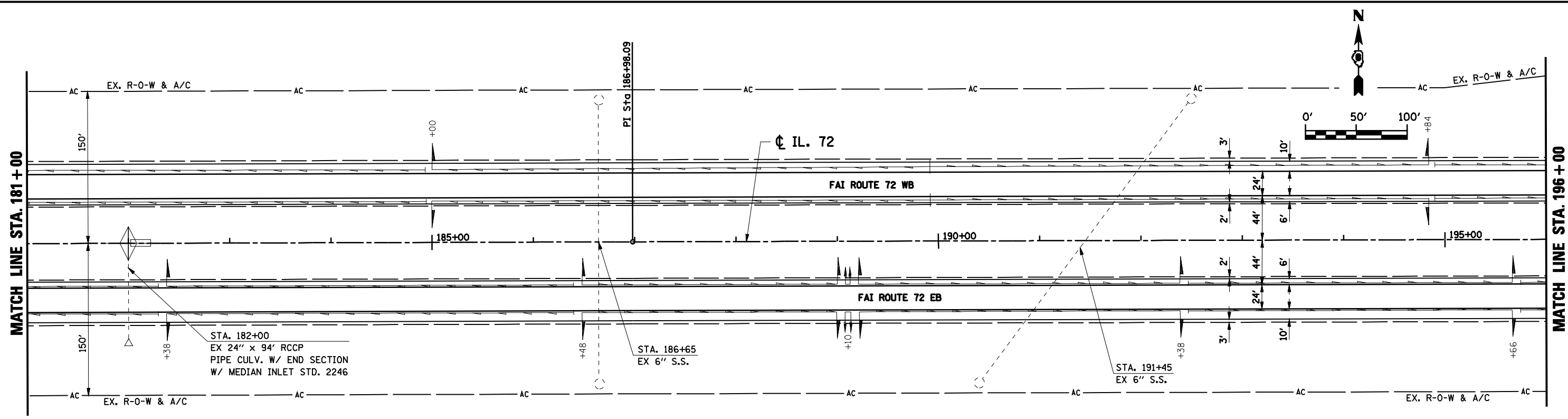
**BEGIN RESURFACING
PROJECT STA. 171+09**

CURVE I72-1
 PI STA. 170+58.94
 $\Delta = 4^\circ 21' 53''$ (LT)
 D = 0° 15' 00"
 R = 22,918.31'
 T = 873.36'
 L = 1,745.87'
 E = 16.63'
 P.C. STA. 161+85.58
 P.T. STA. 179+31.45
 S.E. = NORMAL CROWN



MATCH LINE STA. 181+00

STA. 173+30, 110' RT.
 PR REMOVE AND REINSTALL EXISTING PRECAST REINFORCED CONCRETE FLARED END SECTIONS = 1 EACH
 PR DELINEATORS = 1 EACH



MATCH LINE STA. 181+00

MATCH LINE STA. 196+00

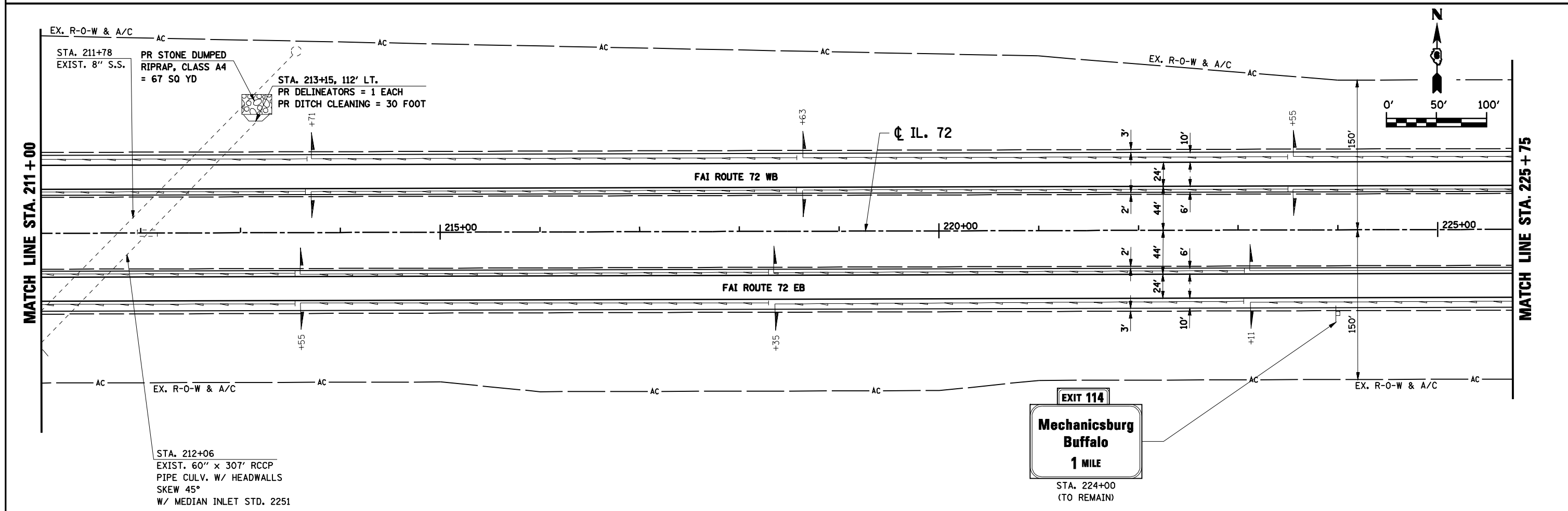
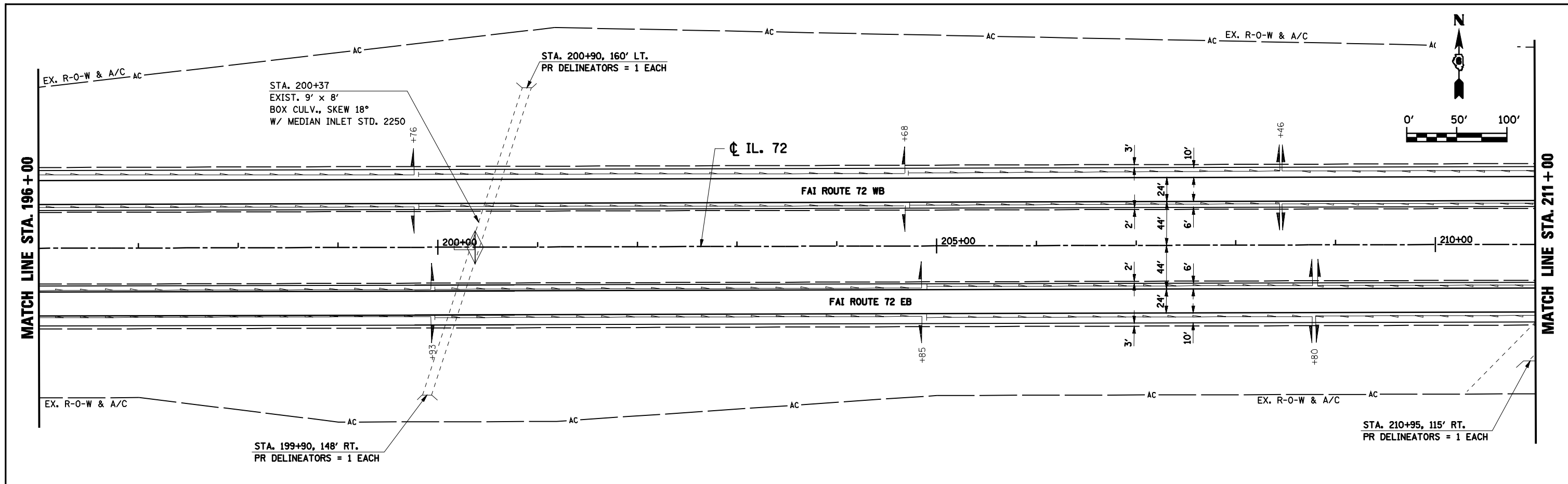
FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -
es:\pwwork\pwwid\LAUGHLINRL\laughlinr1\186PP01.SHT		DRAWN - AAD	REVISED -
PLOT SCALE = NONE		CHECKED - JAC	REVISED -
PLOT DATE = Feb-11-2010 08:58:32AM		DATE - 06/11/07	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

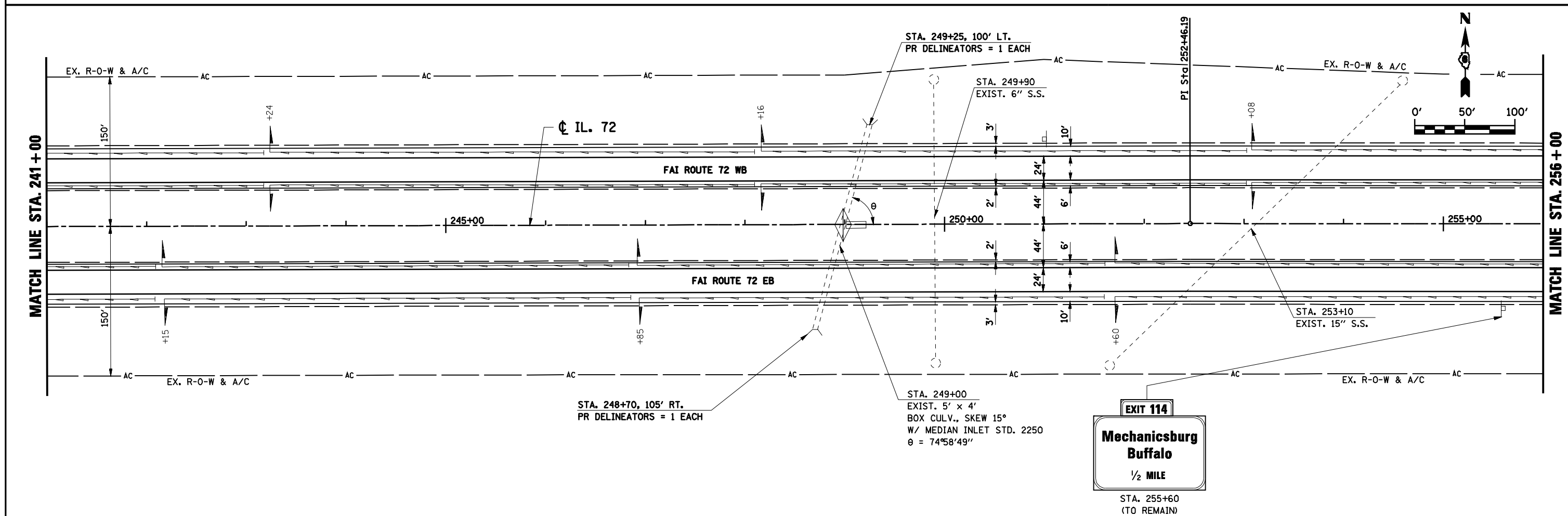
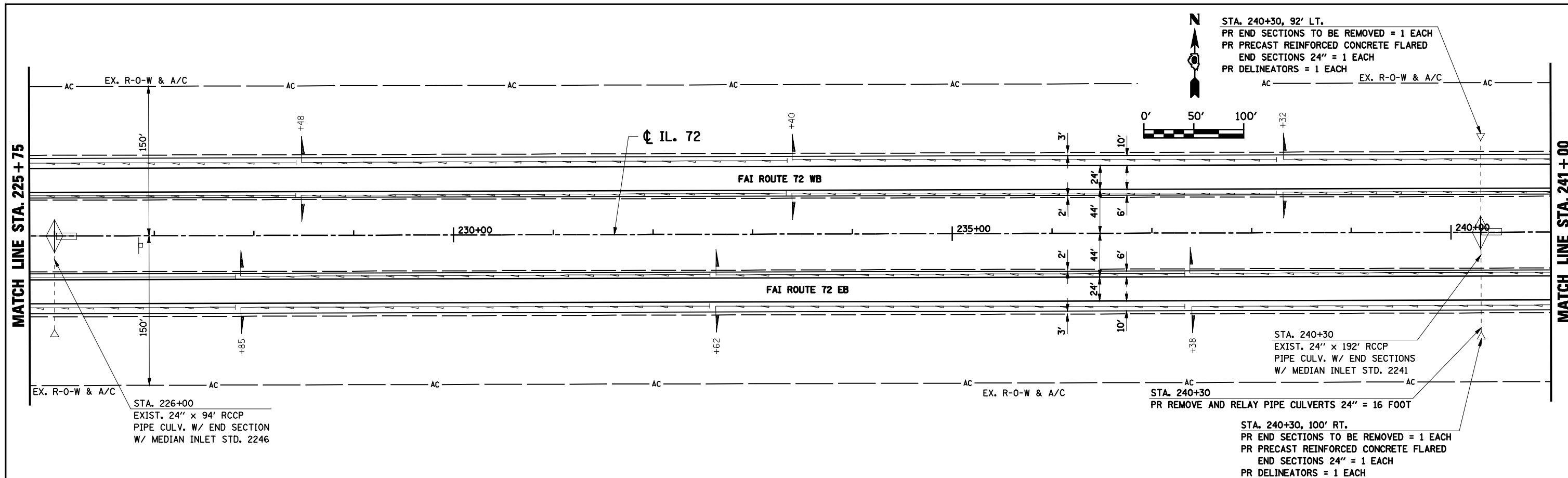
**I-72 PLAN
STA. 166+00 TO STA. 196+00**

SCALE: SHEET NO. OF SHEETS STA. 166+00 TO STA. 196+00

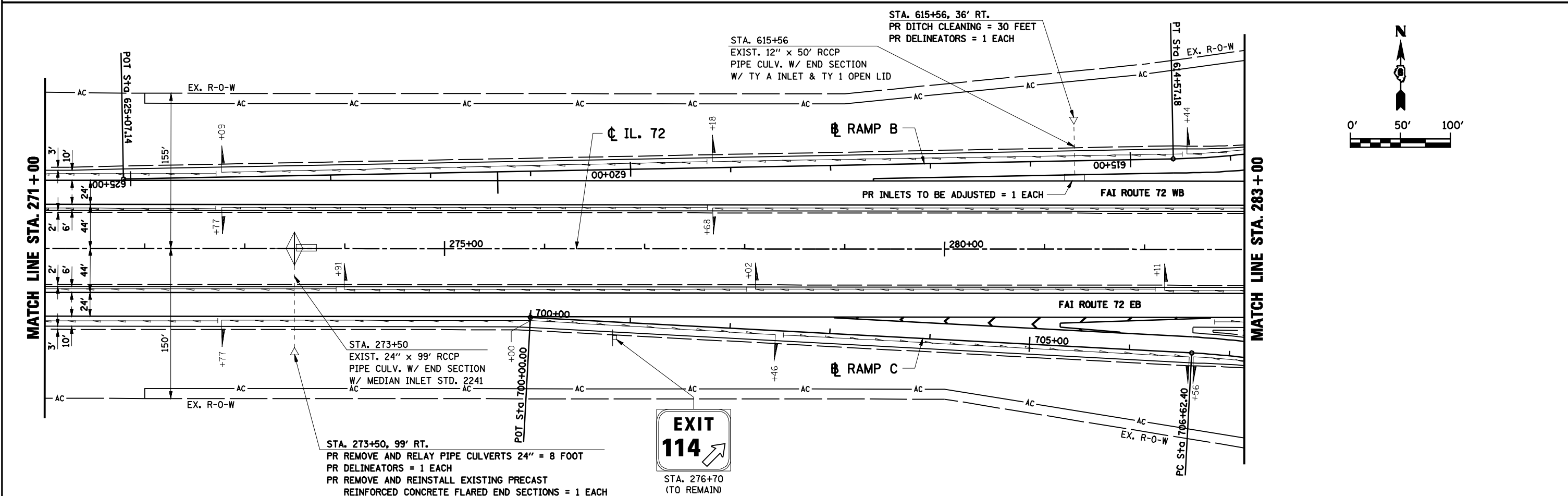
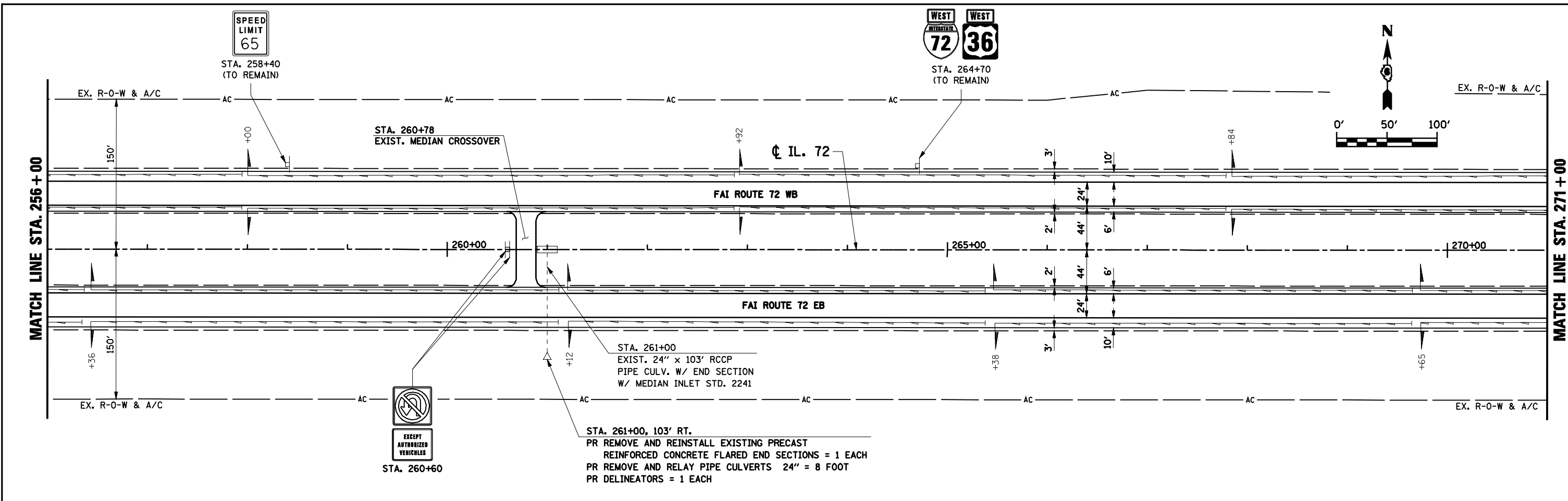
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	29
CONTRACT NO. 72910				
FED. ROAD DIST. NO. 2 (ILLINOIS) FED. AID PROJECT				



FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-72 PLAN STA. 196+00 TO STA. 225+75	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
e:\pwwork\p\WIDOT\LAUGHLINRL\0184389\7016PP02.SHT	DRAWN - AAD	REVISED -	72			(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	30	
PLOT SCALE = NONE	CHECKED - JAC	REVISED -	CONTRACT NO. 72910							
PLOT DATE = Feb-11-2010 08:58:34AM	DATE - 06/11/07	REVISED -	FED. ROAD DIST. NO. 2 (ILLINOIS) FED. AID PROJECT							

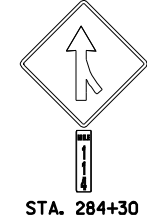


FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-72 PLAN STA. 225 + 75 TO STA. 256 + 00			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pwwork\pwwid001\LAUGHLINRL\0184389\7016PP03.SHT		DRAWN - AAD	REVISED -		72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	31			
PLOT SCALE = NONE		CHECKED - JAC	REVISED -		CONTRACT NO. 72910							
PLOT DATE = Feb-11-2010 08:58:37AM		DATE - 06/11/07	REVISED -		FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT							



FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-72 PLAN STA. 256 + 00 TO STA. 283 + 00			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pwwork\pwwid00\LAUGHLINRL\0184389\7016PP04.SHT		DRAWN - AAD	REVISED -		72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	32			
PLOT SCALE = NONE		CHECKED - JAC	REVISED -		CONTRACT NO. 72910							
PLOT DATE = Feb-11-2010 08:58:39AM		DATE - 06/11/07	REVISED -		FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT							

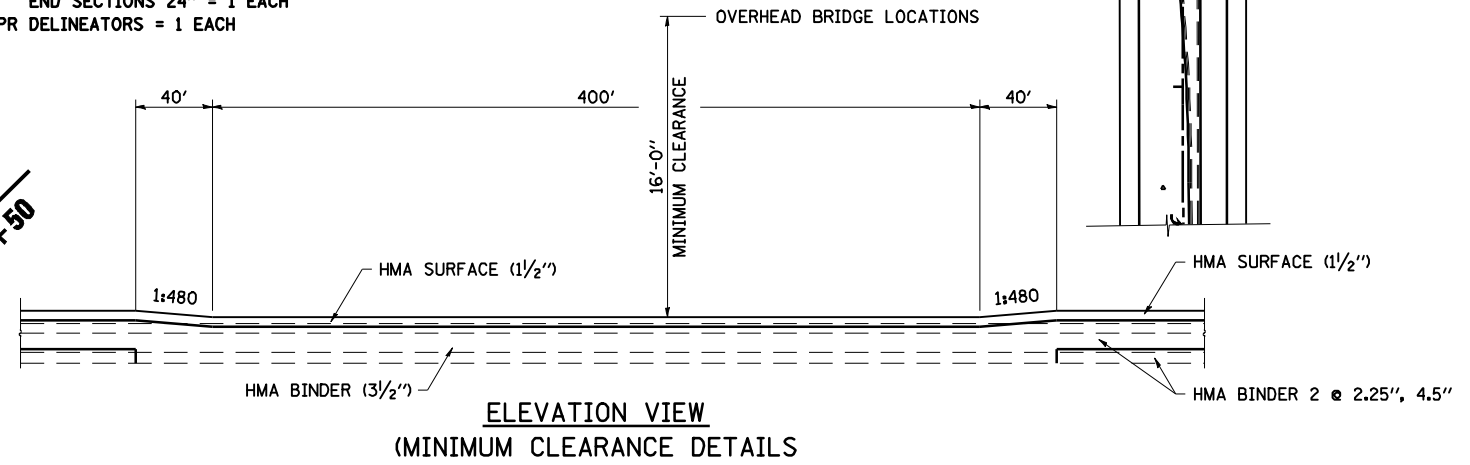
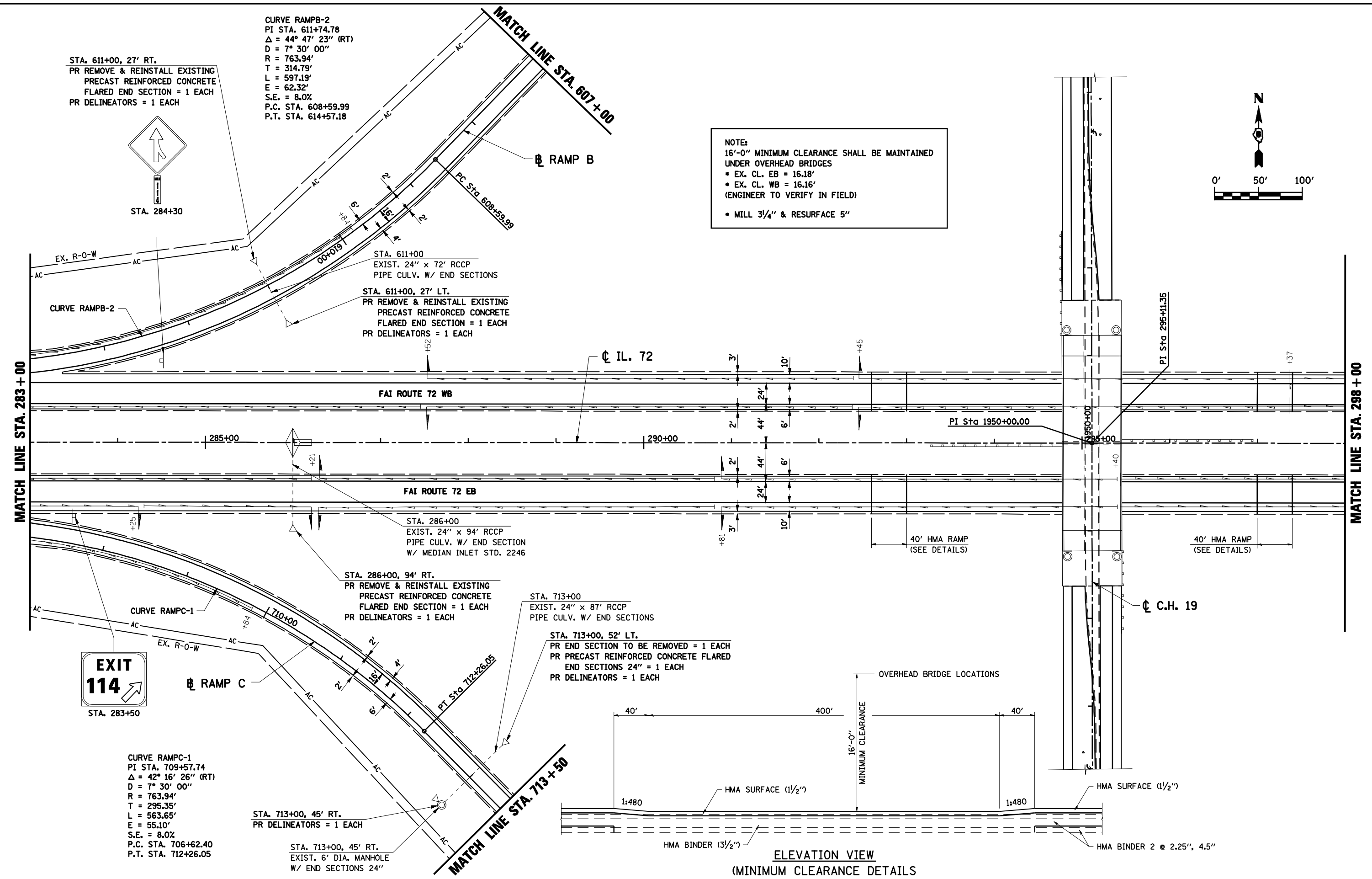
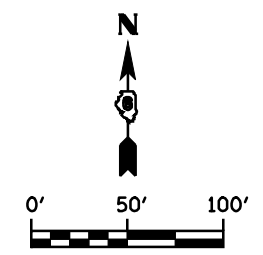
STA. 611+00, 27' RT.
 PR REMOVE & REINSTALL EXISTING
 PRECAST REINFORCED CONCRETE
 FLARED END SECTION = 1 EACH
 PR DELINEATORS = 1 EACH



STA. 284+30

CURVE RAMPB-2
 PI STA. 611+74.78
 $\Delta = 44^\circ 47' 23''$ (RT)
 $D = 7^\circ 30' 00''$
 $R = 763.94'$
 $T = 314.79'$
 $L = 597.19'$
 $E = 62.32'$
 $S.E. = 8.0\%$
 P.C. STA. 608+59.99
 P.T. STA. 614+57.18

NOTE:
 16'-0" MINIMUM CLEARANCE SHALL BE MAINTAINED
 UNDER OVERHEAD BRIDGES
 • EX. CL. EB = 16.18'
 • EX. CL. WB = 16.16'
 (ENGINEER TO VERIFY IN FIELD)
 • MILL 3/4" & RESURFACE 5"



ELEVATION VIEW
 (MINIMUM CLEARANCE DETAILS)

FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -
es:\pwwork\pwwid\LAUGHLINRL\08183140\1010TRRevised_7016PP05.SHT		DRAWN - AAD	REVISED -
PLOT SCALE = NONE		CHECKED - JAC	REVISED -
PLOT DATE = Mar-17-2010 08:16:24AM		DATE - 06/11/07	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

I-72 PLAN
 STA. 283+00 TO STA. 298+00
 SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	33
CONTRACT NO. 72910				
FED. ROAD DIST. NO. 2 (ILLINOIS) FED. AID PROJECT				

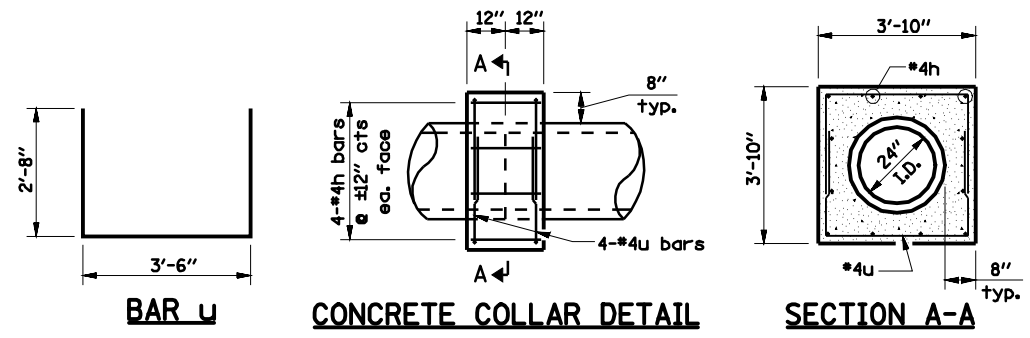
GENERAL NOTES FOR CONCRETE COLLAR

CONCRETE COLLARS SHALL BE CONSTRUCTED OF CLASS SI CONCRETE IN ACCORDANCE WITH SECTION 503 OF THE STANDARD SPECIFICATIONS.

REINFORCEMENT BARS SHALL CONFORM TO SECTION 508 OF THE STANDARD SPECIFICATIONS.

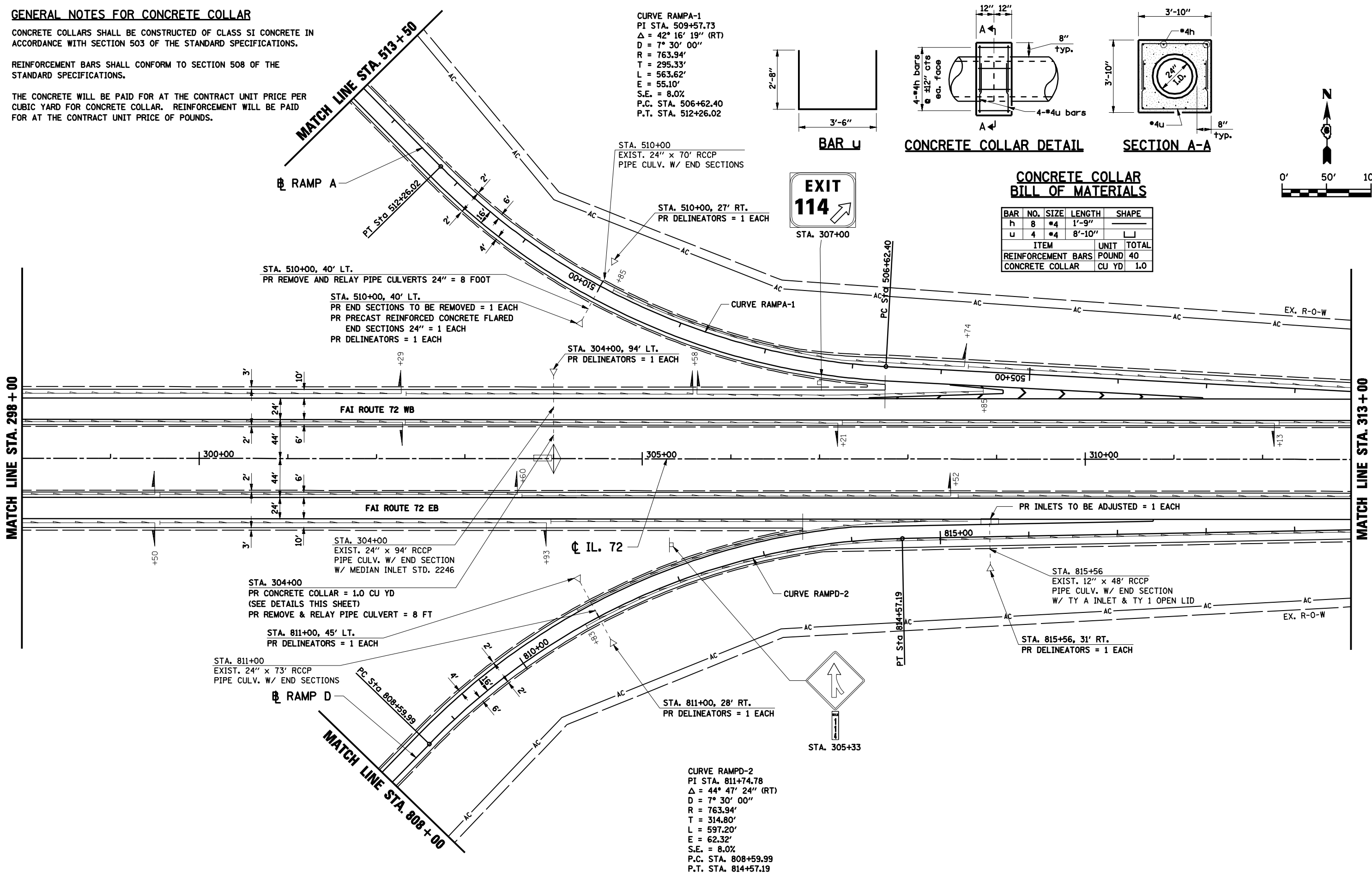
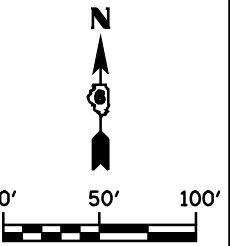
THE CONCRETE WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC YARD FOR CONCRETE COLLAR. REINFORCEMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE OF POUNDS.

CURVE RAMPA-1
 PI STA. 509+57.73
 $\Delta = 42^\circ 16' 19''$ (RT)
 $D = 7^\circ 30' 00''$
 $R = 763.94'$
 $T = 295.33'$
 $L = 563.62'$
 $E = 55.10'$
 $S.E. = 8.0\%$
 P.C. STA. 506+62.40
 P.T. STA. 512+26.02



CONCRETE COLLAR BILL OF MATERIALS

BAR NO.	SIZE	LENGTH	SHAPE
h	#4	1'-9"	
u	#4	8'-10"	
ITEM		UNIT	TOTAL
REINFORCEMENT BARS		POUND	40
CONCRETE COLLAR		CU YD	1.0



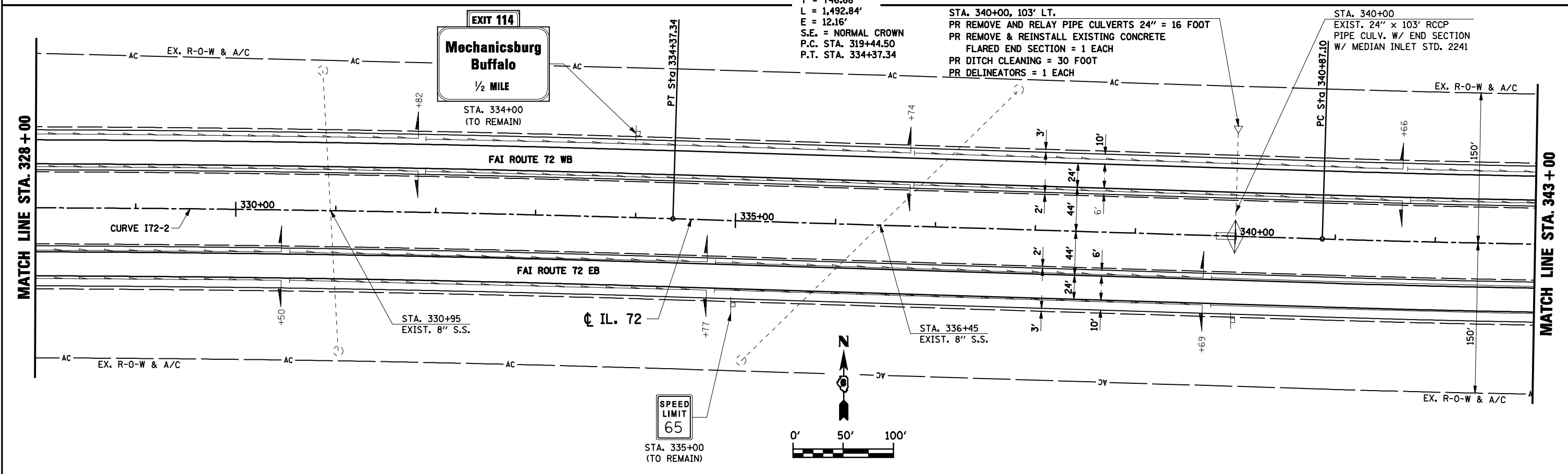
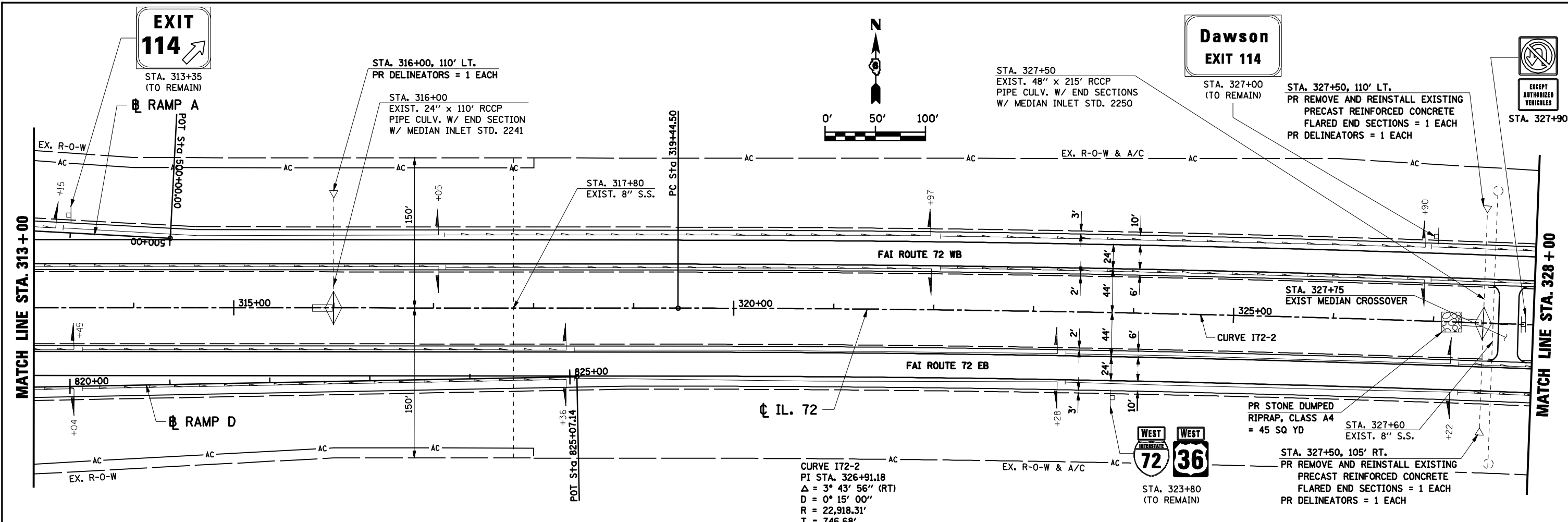
FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -
e:\pwwork\p\WIDOT\LAUGHLINRL\0184389\7016PP06.SHT		DRAWN - AAD	REVISED -
PLOT SCALE = NONE		CHECKED - JAC	REVISED -
PLOT DATE = Feb-11-2010 08:58:44AM		DATE - 06/11/07	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

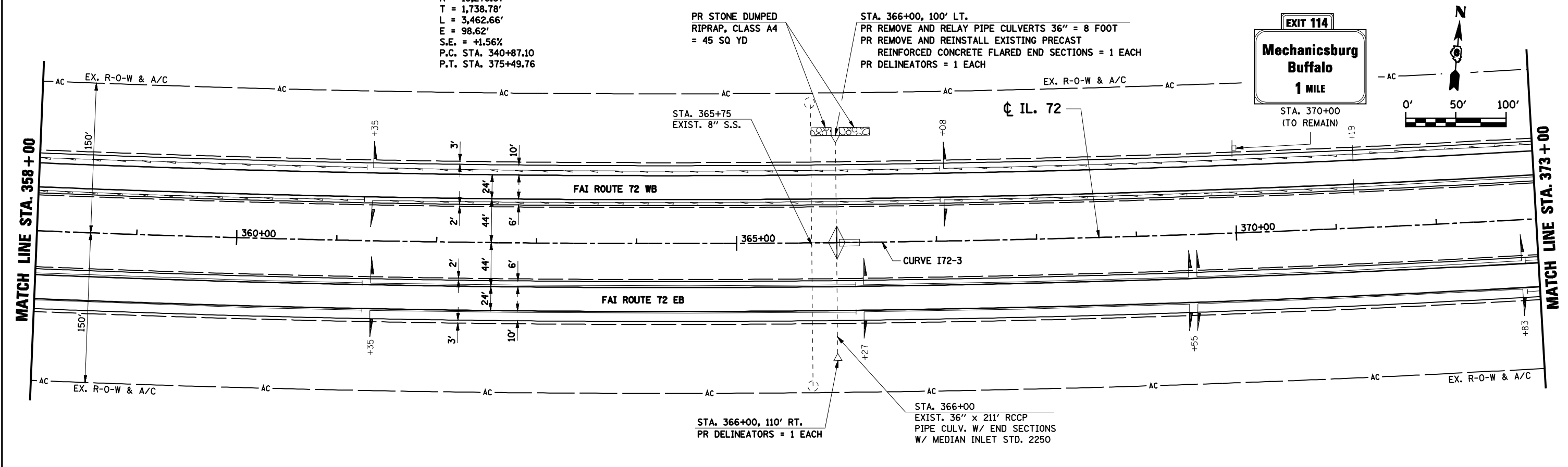
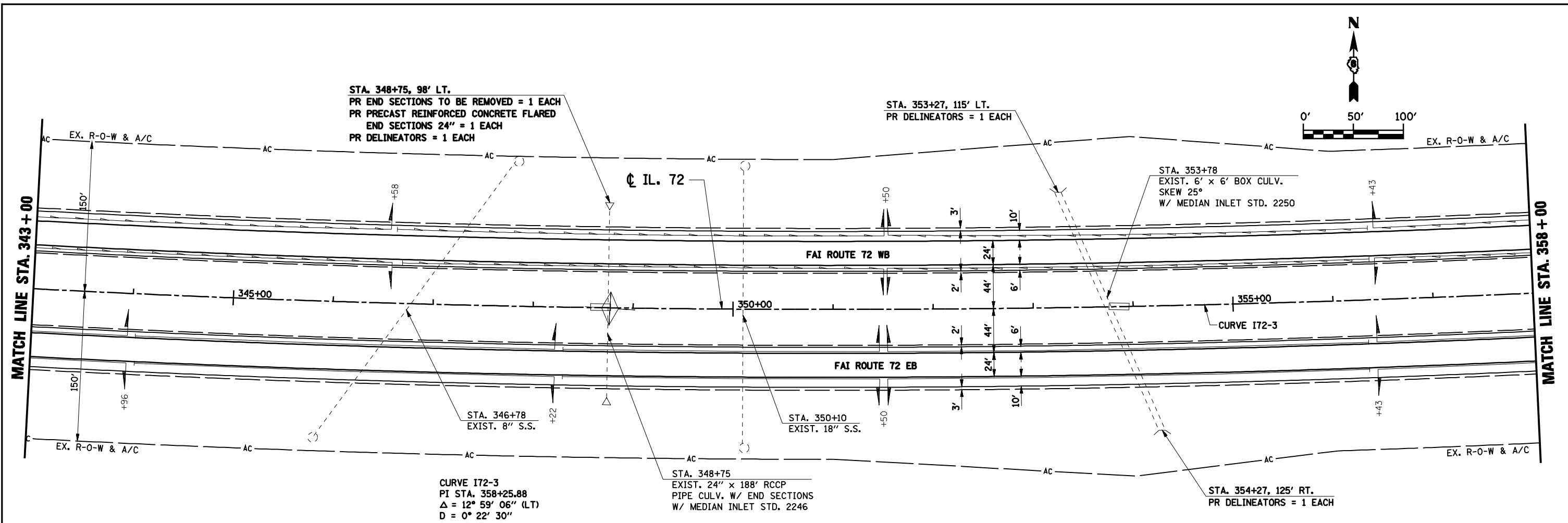
**I-72 PLAN
 STA. 298 + 00 TO STA. 313 + 00**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

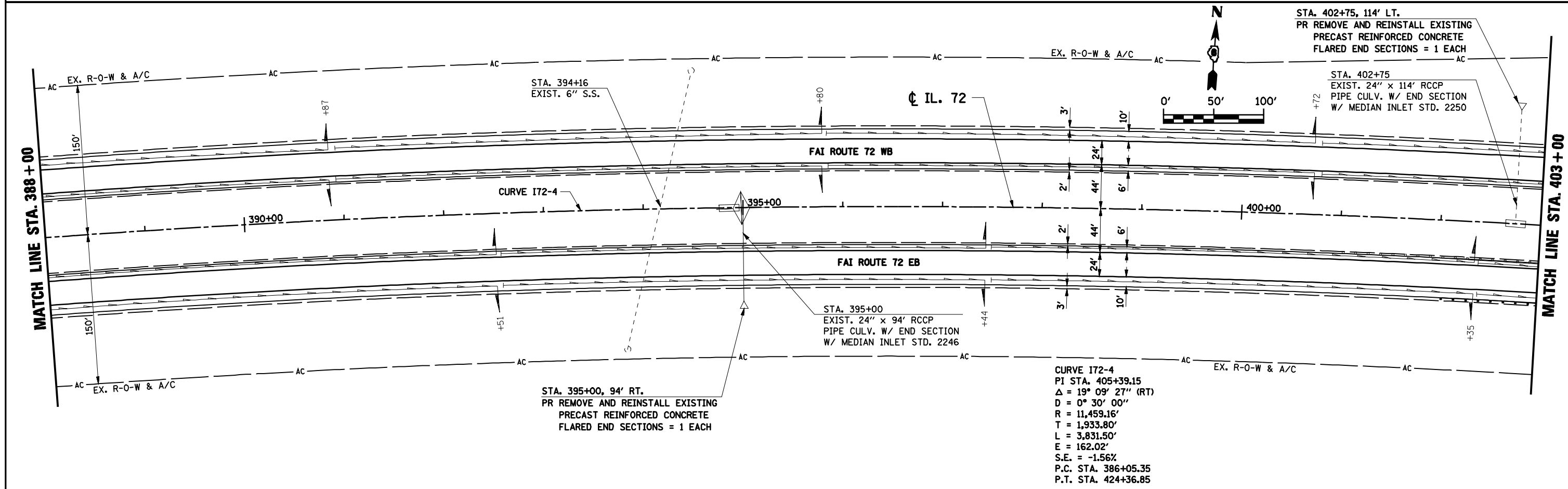
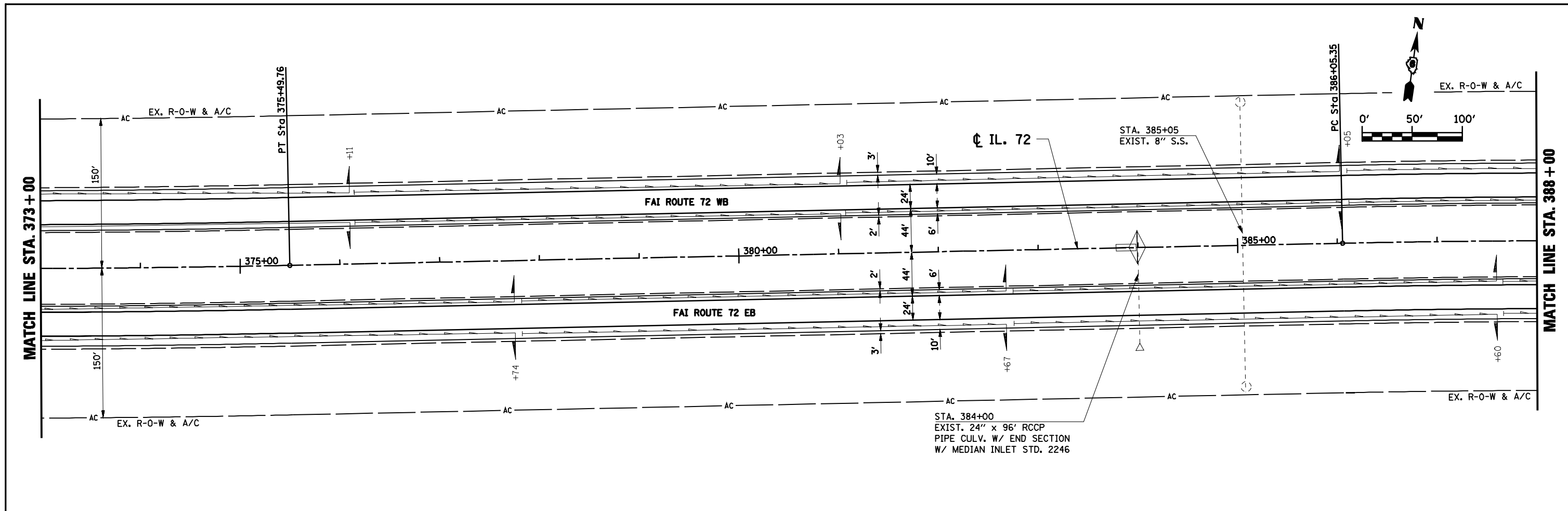
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	34
CONTRACT NO. 72910				
FED. ROAD DIST. NO. 2 (ILLINOIS) FED. AID PROJECT				



FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-72 PLAN STA. 313 + 00 TO STA. 343 + 00			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pwork\pwork\LAUGHLINRL\0184389\0184389.DWG	0184389.DWG	DRAWN - AAD	REVISED -		72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	35			
		CHECKED - JAC	REVISED -		CONTRACT NO. 72910							
		DATE - 06/11/07	REVISED -		FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT							



FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-72 PLAN STA. 343+00 TO STA. 373+00			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pwwork\pwwid\LAUGHLINRL\0184389\7016PP08.SHT		DRAWN - AAD	REVISED -		72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	36			
PLOT SCALE = NONE		CHECKED - JAC	REVISED -		CONTRACT NO. 72910							
PLOT DATE = Feb-11-2010 08:58:49AM		DATE - 06/11/07	REVISED -		FED. ROAD DIST. NO. 2 (ILLINOIS) FED. AID PROJECT							

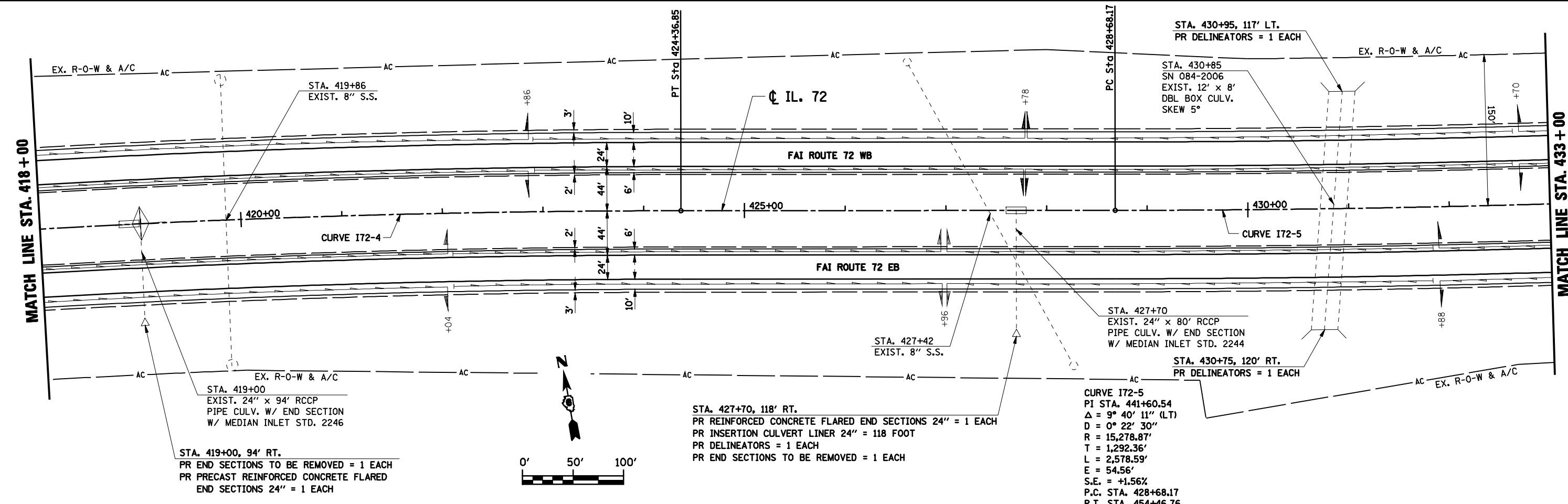
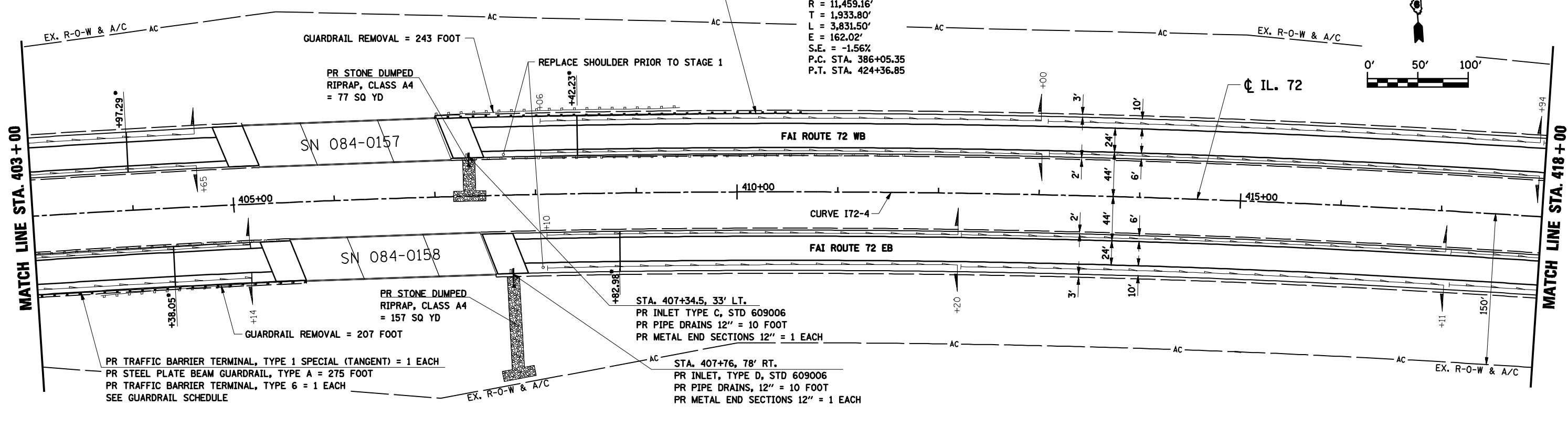
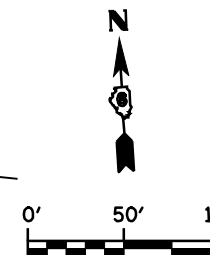


FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-72 PLAN STA. 373 + 00 TO STA. 403 + 00			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pwwork\pwwidot\LAUGHLINRL\0184389\7016PP09.SHT		DRAWN - AAD	REVISED -		72	184-10-2/RS-3, 184-10-3/RS-4	SANGAMON	61	37			
PLOT SCALE = NONE		CHECKED - JAC	REVISED -		CONTRACT NO. 72910							
PLOT DATE = Feb-11-2010 08:58:52AM		DATE - 06/11/07	REVISED -		FED. ROAD DIST. NO. 2 [ILLINOIS] FED. AID PROJECT							

• LIMITS OF PR PCC PAVEMENT CONNECTOR

PR TRAFFIC BARRIER TERMINAL, TYPE 6 = 1 EACH
 PR STEEL PLATE BEAM GUARDRAIL, TYPE A = 275 FOOT
 PR TRAFFIC BARRIER TERMINAL, TYPE 1 SPECIAL (TANGENT) = 1 EACH
 SEE GUARDRAIL SCHEDULE

CURVE I72-4
 PI STA. 405+39.15
 $\Delta = 19^\circ 09' 27''$ (RT)
 $D = 0^\circ 30' 00''$
 $R = 11,459.16'$
 $T = 1,933.80'$
 $L = 3,831.50'$
 $E = 162.02'$
 $S.E. = -1.56\%$
 P.C. STA. 386+05.35
 P.T. STA. 424+36.85



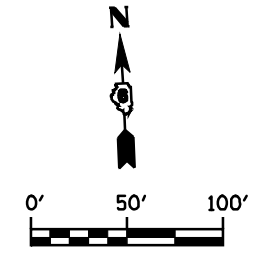
FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -
er:\pwwork\pwwid001\LAUGHLINRL\0184389\7016PP10.SHT		DRAWN - AAD	REVISED -
		CHECKED - JAC	REVISED -
		DATE - 06/11/07	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

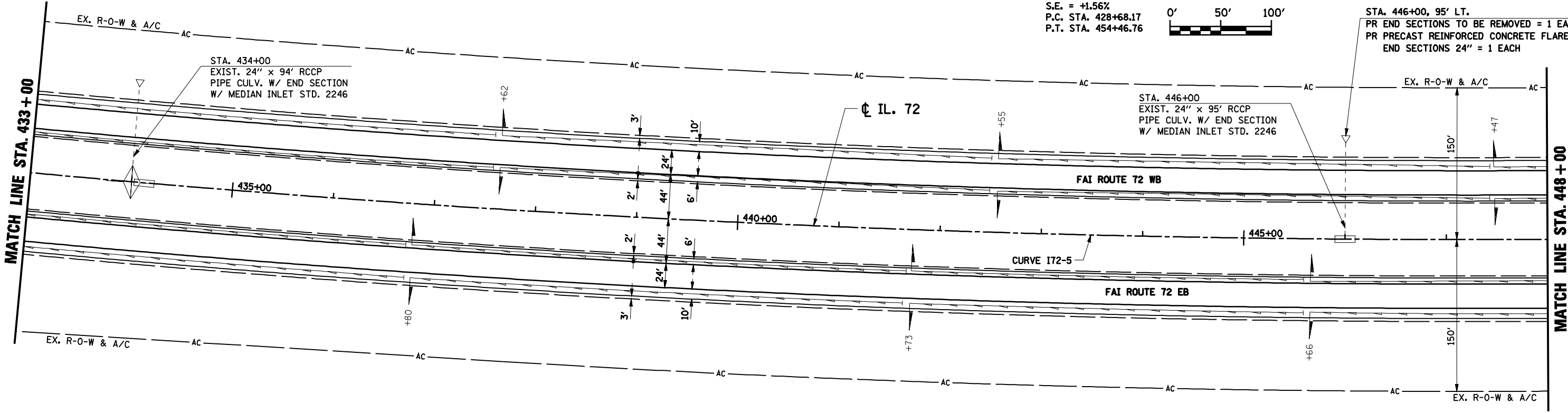
I-72 PLAN			
STA. 403+00 TO STA. 433+00			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	38
CONTRACT NO. 72910				
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

CURVE I72-5
 PI STA. 441+60.54
 $\Delta = 9^\circ 40' 11''$ (LT)
 $D = 0^\circ 22' 30''$
 $R = 15,278.87'$
 $T = 1,292.36'$
 $L = 2,578.59'$
 $E = 54.56'$
 $S.E. = +1.56\%$
 P.C. STA. 428+68.17
 P.T. STA. 454+46.76



STA. 446+00, 95' LT.
 PR END SECTIONS TO BE REMOVED = 1 EACH
 PR PRECAST REINFORCED CONCRETE FLARED
 END SECTIONS 24" = 1 EACH



FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -
e:\pwwork\pwidot\LAUGHLINRL\0184389\7016PP11.SHT		DRAWN - AAD	REVISED -
PLOT SCALE = NONE		CHECKED - JAC	REVISED -
PLOT DATE = Feb-11-2010 08:58:57AM		DATE - 06/11/07	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

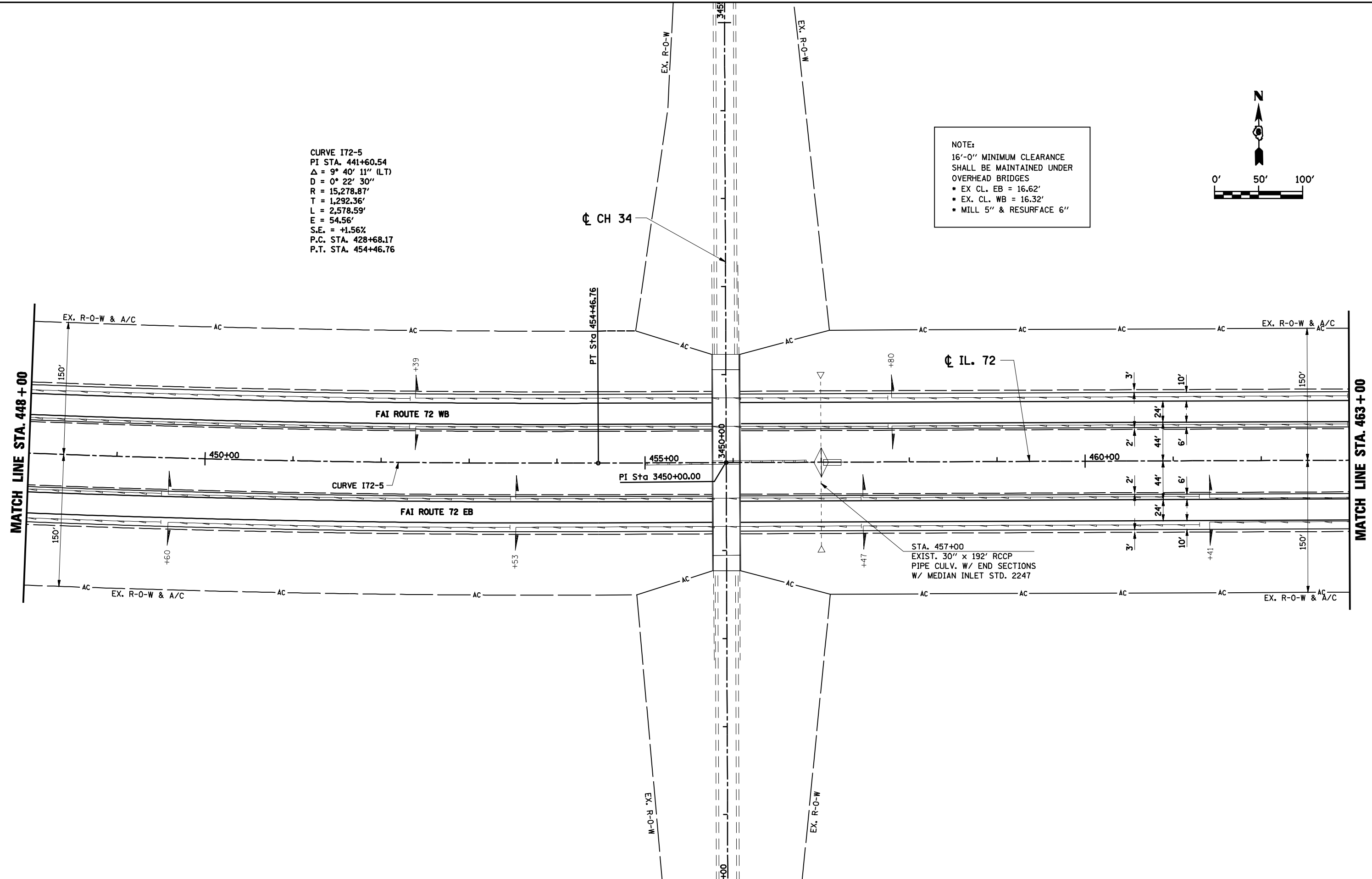
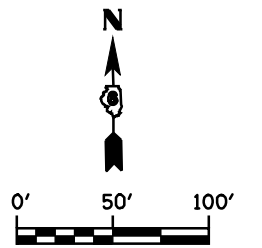
**I-72 PLAN
 STA. 433+00 TO STA. 448+00**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	39
CONTRACT NO. 72910				
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

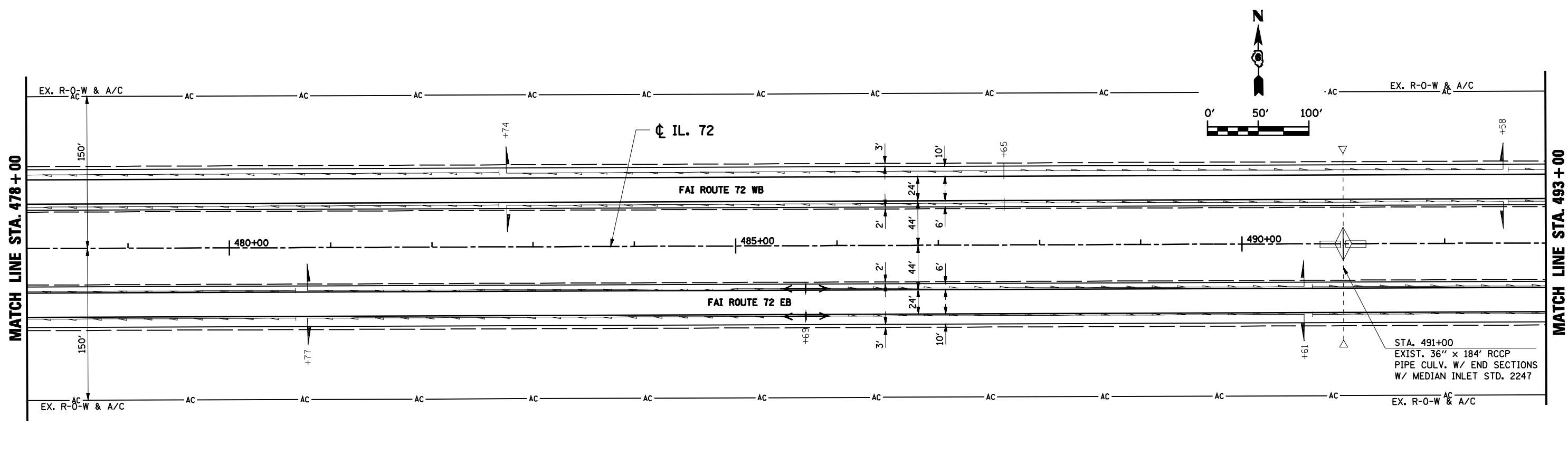
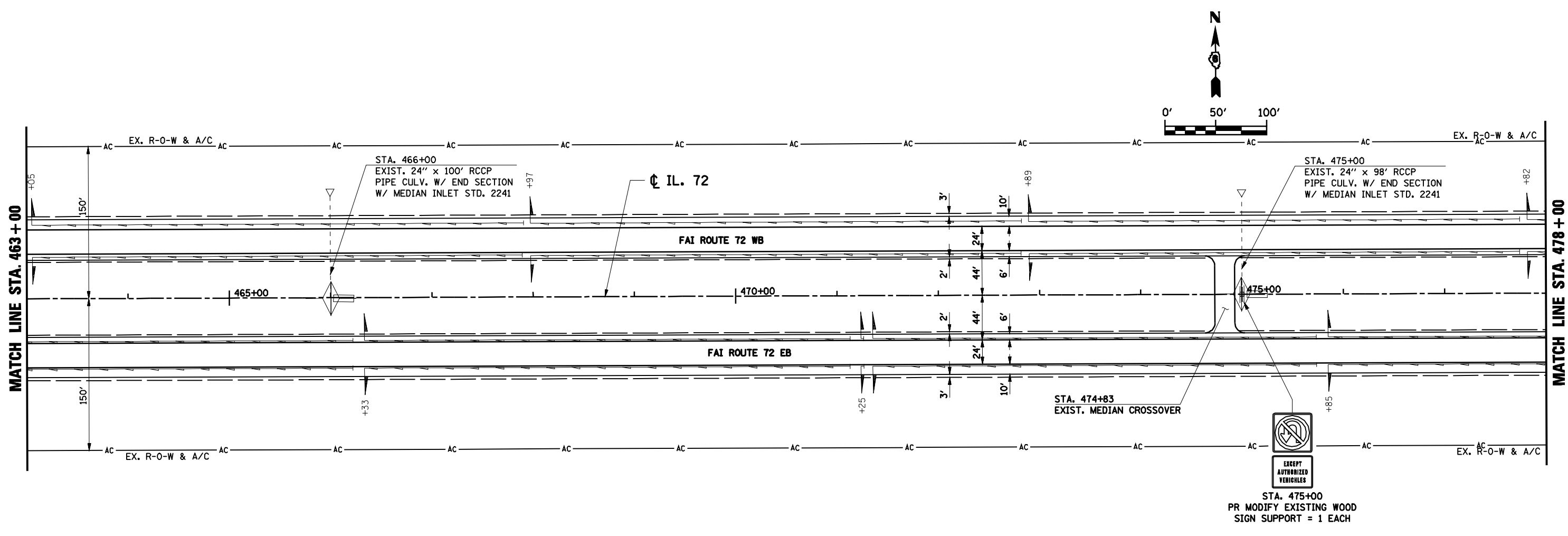
CURVE I72-5
 PI STA. 441+60.54
 $\Delta = 9^\circ 40' 11''$ (LT)
 $D = 0^\circ 22' 30''$
 $R = 15,278.87'$
 $T = 1,292.36'$
 $L = 2,578.59'$
 $E = 54.56'$
 $S.E. = +1.56\%$
 P.C. STA. 428+68.17
 P.T. STA. 454+46.76

NOTE:
 16'-0" MINIMUM CLEARANCE
 SHALL BE MAINTAINED UNDER
 OVERHEAD BRIDGES
 • EX. CL. EB = 16.62'
 • EX. CL. WB = 16.32'
 • MILL 5" & RESURFACE 6"

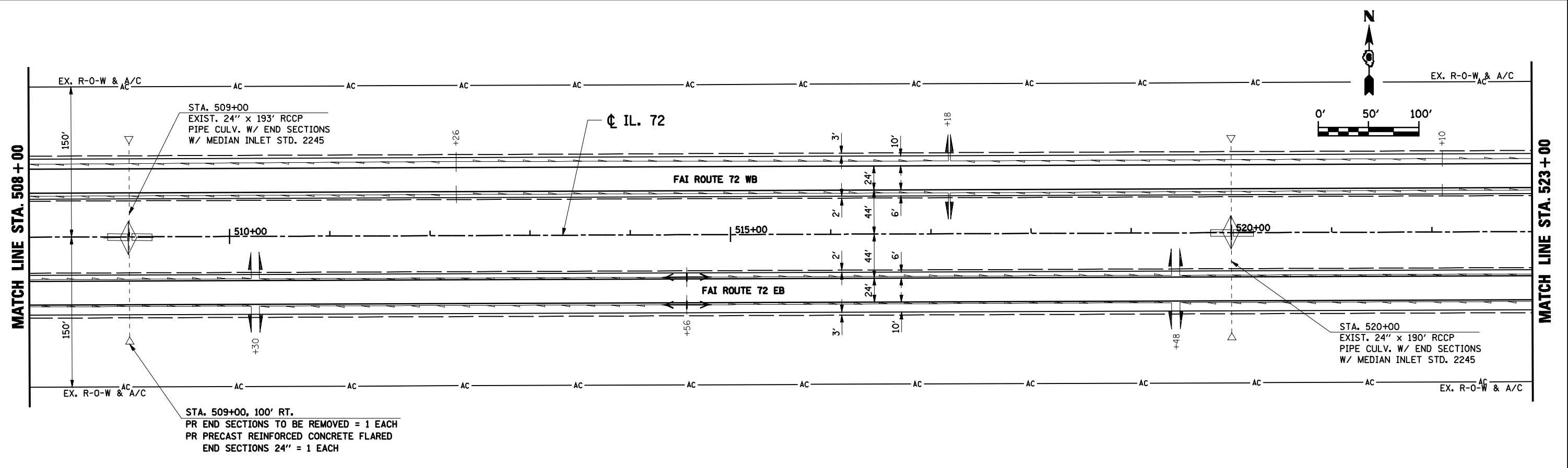
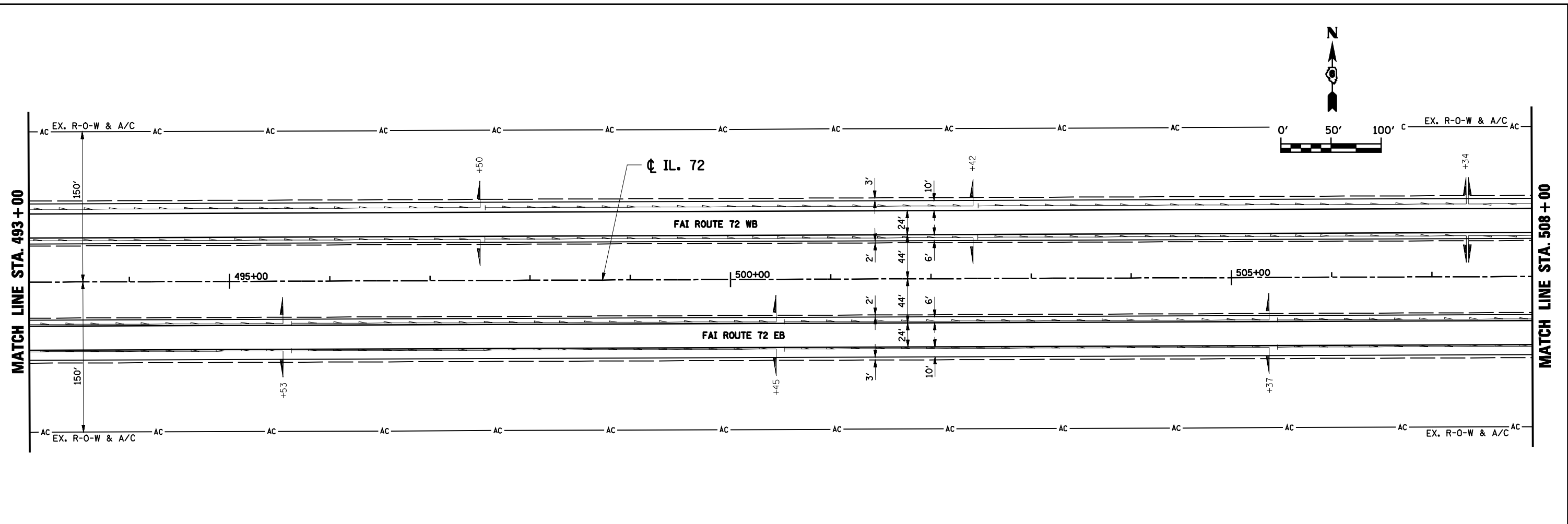


STA. 457+00
 EXIST. 30" x 192' RCCP
 PIPE CULV. W/ END SECTIONS
 W/ MEDIAN INLET STD. 2247

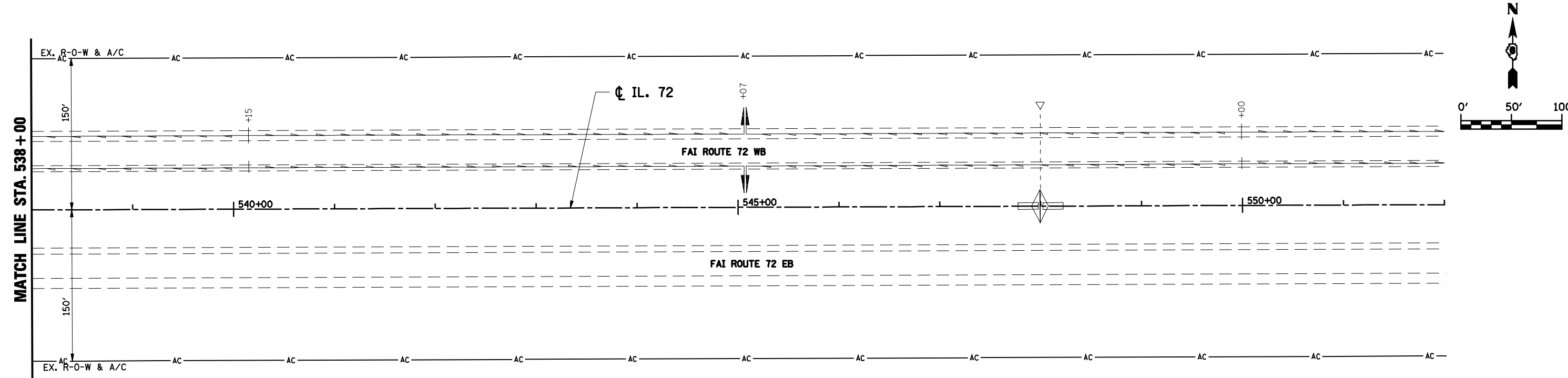
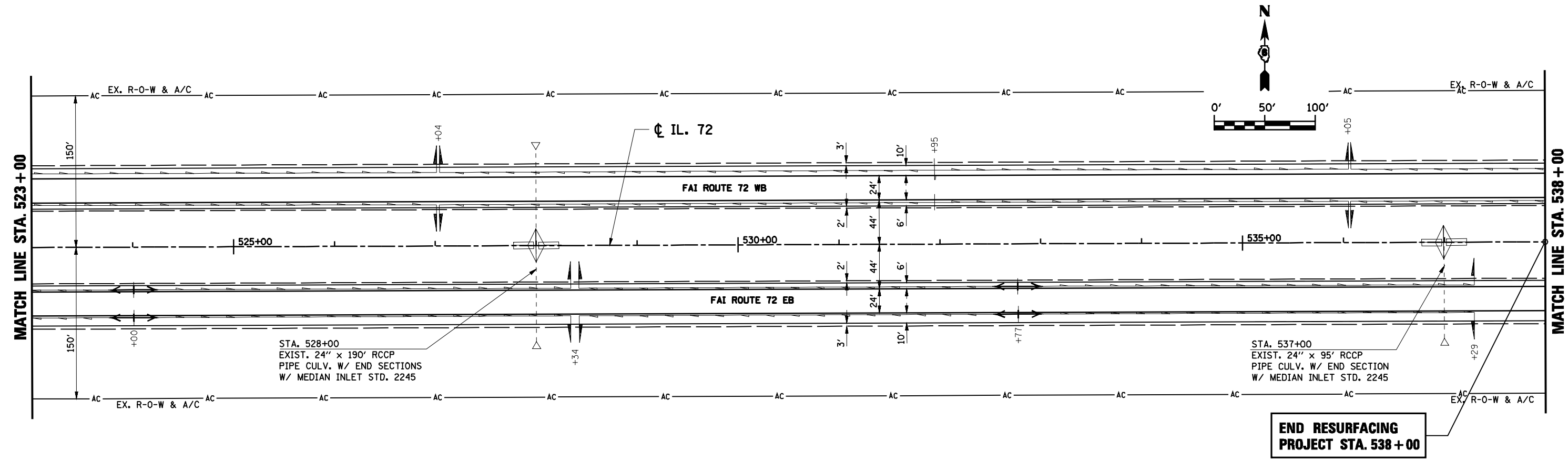
FILE NAME = e:\pwork\pwidot\LAUGHLINRL\0183140\DOTRevised_7016PP12.SHT	USER NAME = laughlinr1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-72 PLAN STA. 448+00 TO STA. 463+00			F.A.I. RTE. = 72	SECTION = (84-10-2)RS-3, (84-10-3)RS-4	COUNTY = SANGAMON	TOTAL SHEETS = 61	SHEET NO. = 40
	PLOT SCALE = NONE	CHECKED - JAC	REVISED -		SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	CONTRACT NO. 72910				
PLOT DATE = Mar-17-2010 08:16:26AM	DATE = 06/11/07	REVISED -	REVISED -	FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT								



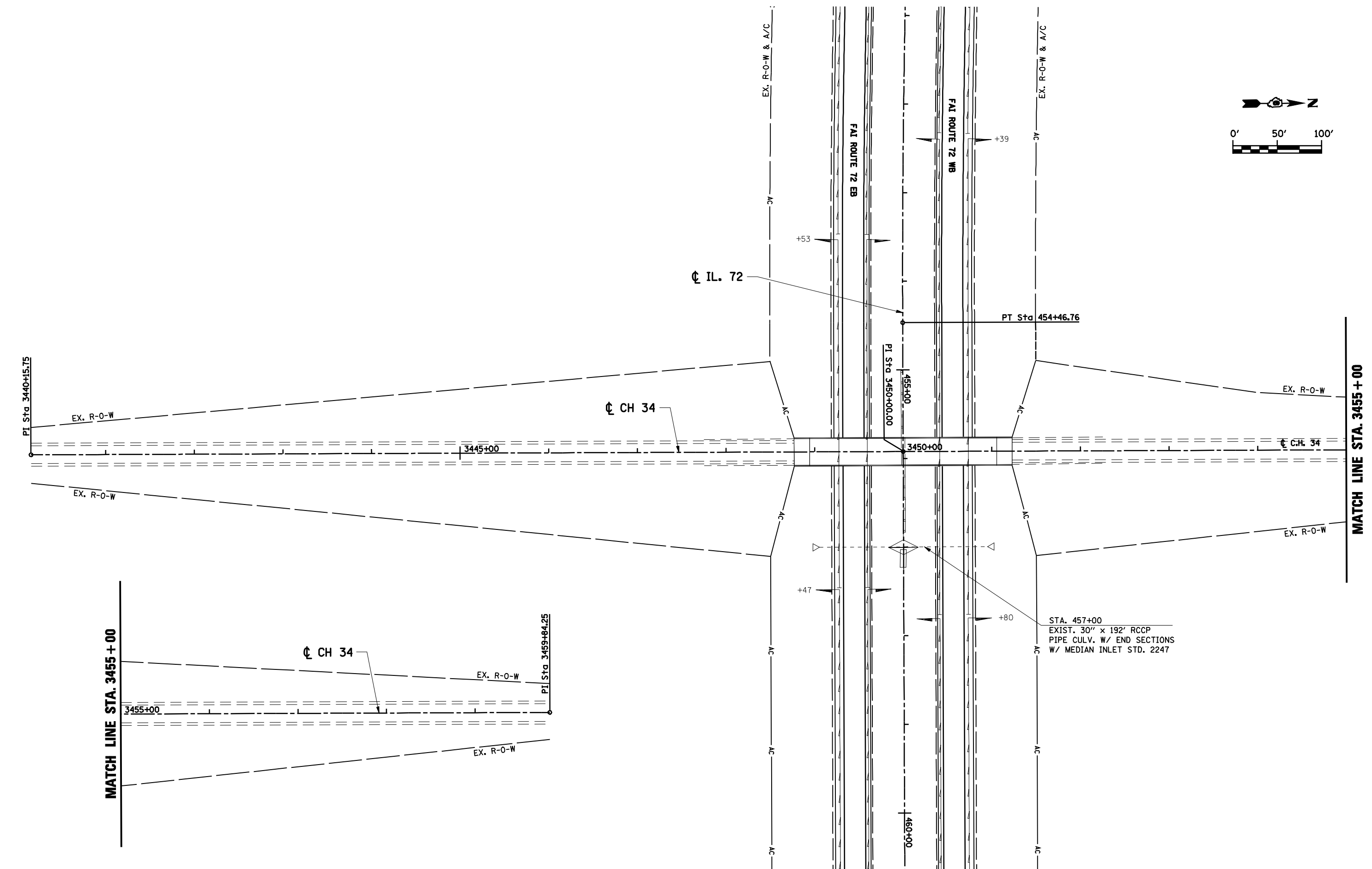
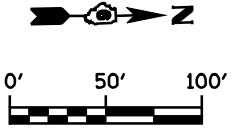
FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-72 PLAN STA. 463+00 TO STA. 493+00		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
er\pwwork\PWIDOT\LAUGHLINRL\0184389\7016PP13.SHT		DRAWN - AAD	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	41
		CHECKED - JAC	REVISED -									CONTRACT NO. 72910		
		DATE - 06/11/07	REVISED -									FED. ROAD DIST. NO. 2 [ILLINOIS] FED. AID PROJECT		



FILE NAME = e:\pwwork\pwwid00\LAUGHLINRL\0184389\0184389.dwg	USER NAME = laughlinr1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-72 PLAN STA. 493+00 TO STA. 523+00			F.A.I. RTE. = 72	SECTION = (84-10-2)RS-3, (84-10-3)RS-4	COUNTY = SANGAMON	TOTAL SHEETS = 61	SHEET NO. = 42
	PLOT SCALE = NONE	CHECKED - JAC	REVISED -		SCALE: _____	SHEET NO. _____	OF SHEETS _____	STA. _____	TO STA. _____	CONTRACT NO. 72910		
PLOT DATE = Feb-11-2010 08:59:04AM	DATE = 06/11/07	REVISED -	REVISED -	FED. ROAD DIST. NO. 2 (ILLINOIS) FED. AID PROJECT								



FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-72 PLAN STA. 523+00 TO STA. 552+00			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
er:\pwork\pwork\LAUGHLINRL\0184389\7016PP15.SHT		DRAWN - AAD	REVISED -		72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	43			
PLOT SCALE = NONE		CHECKED - JAC	REVISED -		CONTRACT NO. 72910							
PLOT DATE = Feb-11-2010 08:59:06AM		DATE - 06/11/07	REVISED -		FED. ROAD DIST. NO. 2 [ILLINOIS] FED. AID PROJECT							
				SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.				



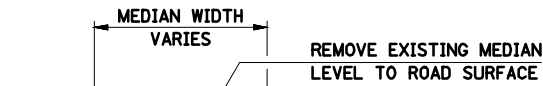
FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -
e:\pwwork\pwidot\LAUGHLINRL\0184389\7016PP34.SHT		DRAWN - AAD	REVISED -
		CHECKED - JAC	REVISED -
		DATE - 06/11/07	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**C.H. 34 PLAN
STA. 3440 + 15.75 TO STA. 3459 + 84.25**

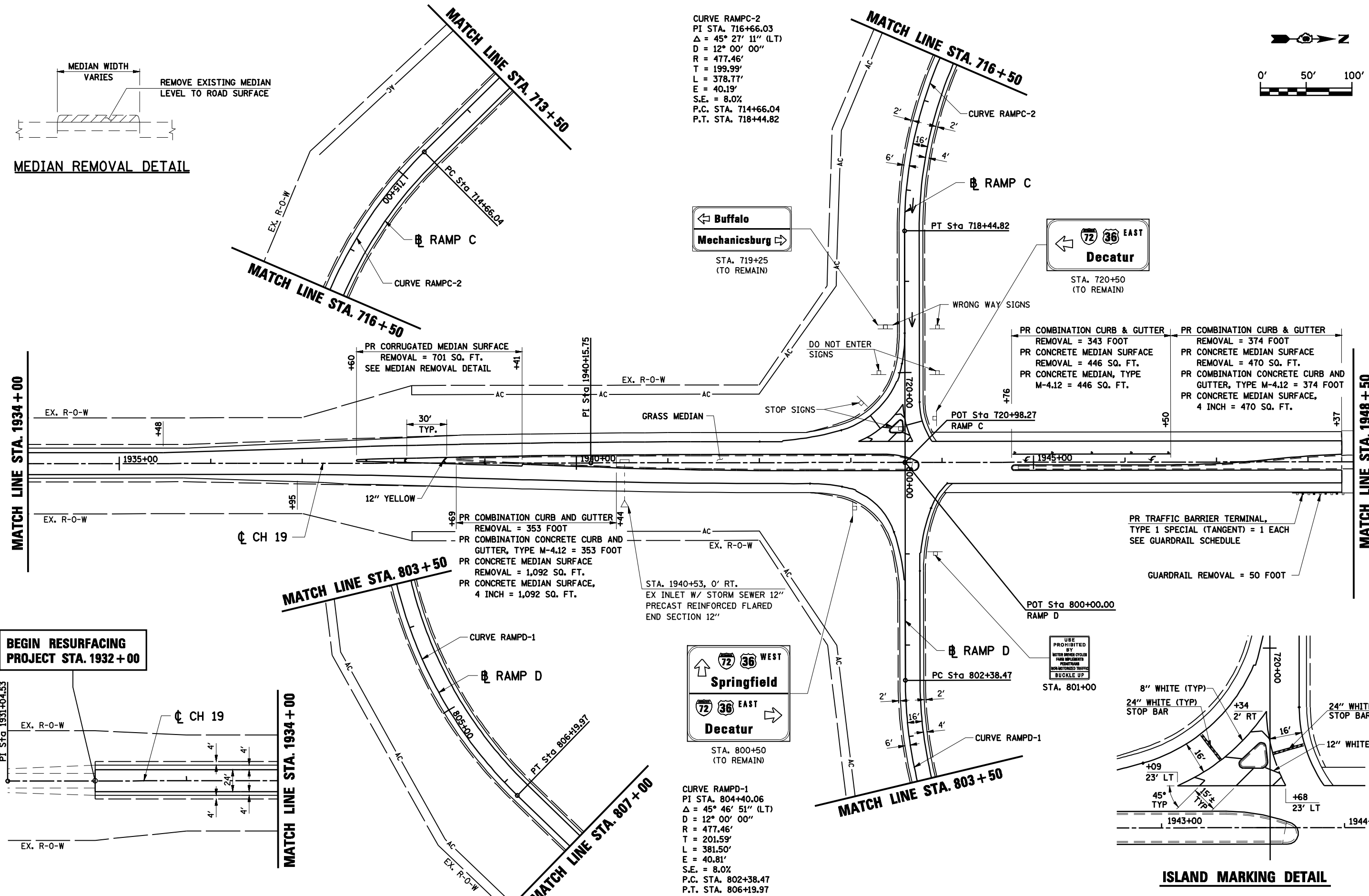
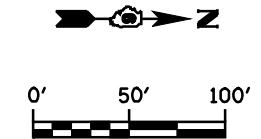
SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	44
CONTRACT NO. 72910				
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				



MEDIAN REMOVAL DETAIL

CURVE RAMP C-2
 PI STA. 716+66.03
 $\Delta = 45^\circ 27' 11''$ (LT)
 $D = 12^\circ 00' 00''$
 $R = 477.46'$
 $T = 199.99'$
 $L = 378.77'$
 $E = 40.19'$
 $S.E. = 8.0\%$
 P.C. STA. 714+66.04
 P.T. STA. 718+44.82



BEGIN RESURFACING PROJECT STA. 1932 + 00

ISLAND MARKING DETAIL

Springfield
 Decatur
 STA. 800+50 (TO REMAIN)

CURVE RAMP D-1
 PI STA. 804+40.06
 $\Delta = 45^\circ 46' 51''$ (LT)
 $D = 12^\circ 00' 00''$
 $R = 477.46'$
 $T = 201.59'$
 $L = 381.50'$
 $E = 40.81'$
 $S.E. = 8.0\%$
 P.C. STA. 802+38.47
 P.T. STA. 806+19.97

FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -
er:\pwwork\VPWIDOT\LAUGHLINRL\vd0184389\7016PP19a.SHT		DRAWN - AAD	REVISED -
		CHECKED - JAC	REVISED -
		DATE - 06/11/07	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CH 19 PLAN
 STA. 1931 + 04.53 TO STA. 1948 + 50.00**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

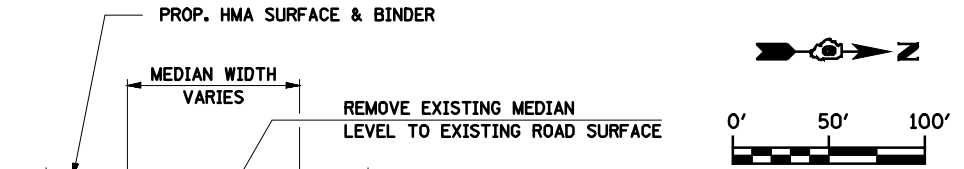
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	45
CONTRACT NO. 72910				
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

* PR TRAFFIC BARRIER TERMINAL,
TYPE 1 SPECIAL (TANGENT) = 1 EACH
SEE GUARDRAIL SCHEDULE
GUARDRAIL REMOVAL = 50 FOOT

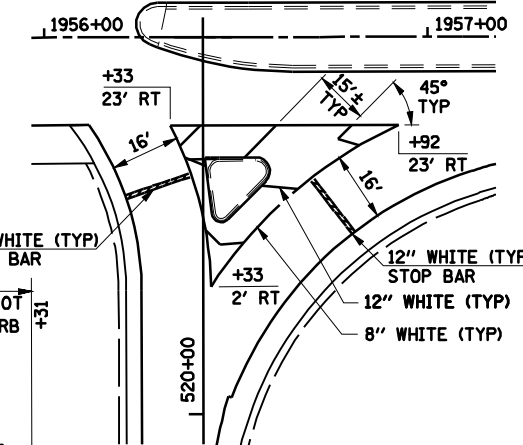
STA. 1948+70, 28' LT.
INLETS TO BE ADJUSTED WITH NEW
TYPE 1 FRAME, OPEN LID = 1 EACH

STA. 1948+70, 28' RT.
INLETS TO BE ADJUSTED = 1 EACH

CURVE RAMPB-1
PI STA. 604+40.05
 $\Delta = 45^\circ 46' 54''$ (LT)
D = 12° 00' 00"
R = 477.46'
T = 201.60'
L = 381.51'
E = 40.82'
S.E. = 8.0%
P.C. STA. 602+38.46
P.T. STA. 606+19.97

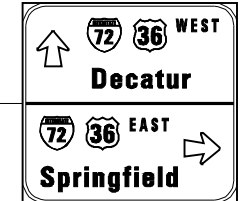


MEDIAN REMOVAL DETAIL



ISLAND MARKING DETAIL

STA. 1959+54, 0' RT.
EX INLET W/ STORM SEWER 12"
PRECAST REINFORCED FLARED
END SECTION 12"



STA. 600+50
(TO REMAIN)

PR COMBINATION CURB AND
GUTTER REMOVAL = 338 FOOT
PR COMBINATION CONCRETE CURB
AND GUTTER, TYPE B-4.12
= 338 FOOT
PR CONCRETE MEDIAN SURFACE
REMOVAL = 1,013 SQ. FT.
PR CONCRETE MEDIAN SURFACE,
4 INCH = 1,013 SQ. FT.

PR COMBINATION CURB & GUTTER
REMOVAL = 375 FOOT
PR CONCRETE MEDIAN SURFACE
REMOVAL = 466 SQ. FT.
PR COMBINATION CONCRETE CURB AND
GUTTER, TYPE B-4.12 = 375 FOOT
PR CONCRETE MEDIAN SURFACE,
4 INCH = 466 SQ. FT.

PR COMBINATION CURB & GUTTER
REMOVAL = 351 FOOT
PR CONCRETE MEDIAN SURFACE
REMOVAL = 456 SQ. FT.
PR CONCRETE MEDIAN, TYPE
M-4.12 = 456 SQ. FT.



STA. 520+55
(TO REMAIN)

END RESURFACING
PROJECT STA. 1968 + 00



STA. 519+25
(TO REMAIN)

CURVE RAMP A-2
PI STA. 516+66.03
 $\Delta = 45^\circ 27' 09''$ (LT)
D = 12° 00' 00"
R = 477.46'
T = 199.98'
L = 378.77'
E = 40.19'
S.E. = 8.0%
P.C. STA. 514+66.04
P.T. STA. 518+44.81

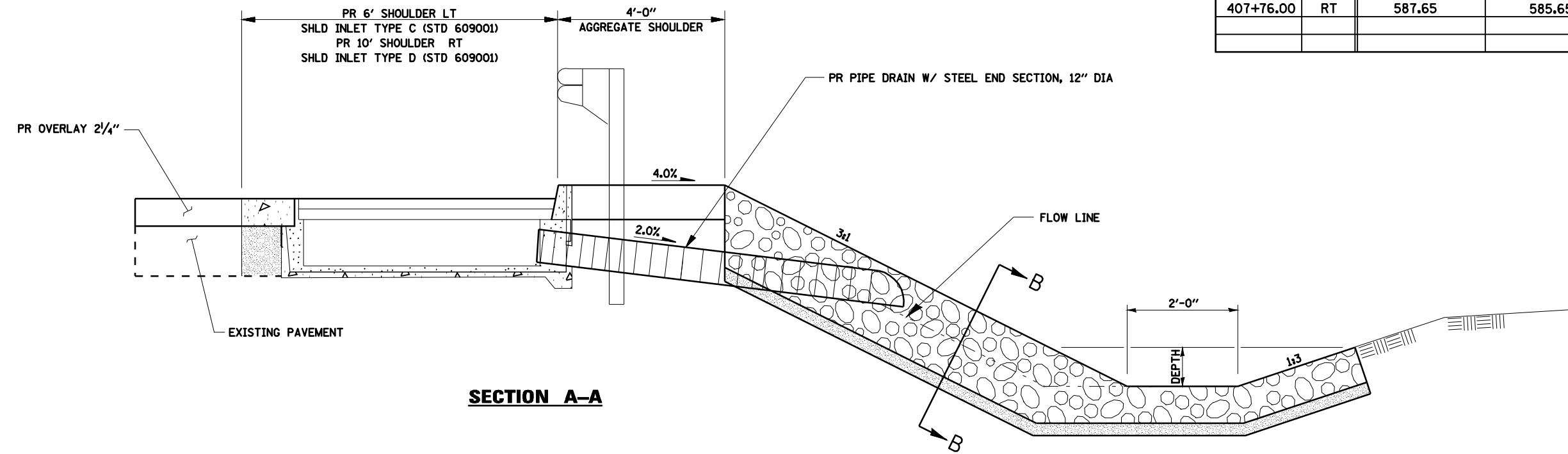
MATCH LINE STA. 1948 + 50

MATCH LINE STA. 1963 + 50

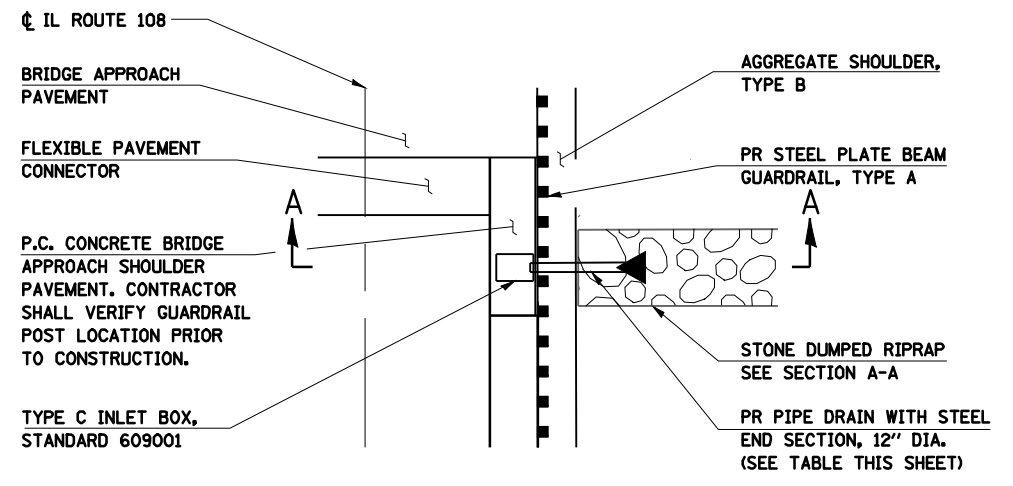
FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CH 19 PLAN STA. 1948 + 50.00 TO STA. 1968 + 88.94			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pwwork\pwwid\LAUGHLINRL\0184389\7016PP196.SHT		DRAWN - AAD	REVISED -		72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	46			
PLOT SCALE = NONE		CHECKED - JAC	REVISED -		CONTRACT NO. 72910							
PLOT DATE = Feb-11-2010 08:59:14AM		DATE - 06/11/07	REVISED -		FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT							

BRIDGE APPROACH SHOULDER DRAINS

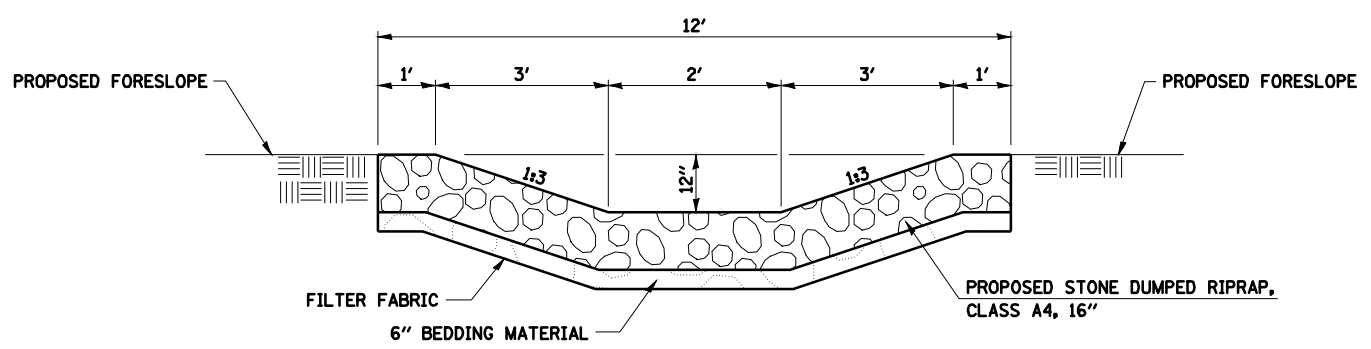
LOCATION	LT/RT	T.O.G. FLOW LINE	INLET INV.
407+34.50	LT	588.31	586.31
407+76.00	RT	587.65	585.65



SECTION A-A

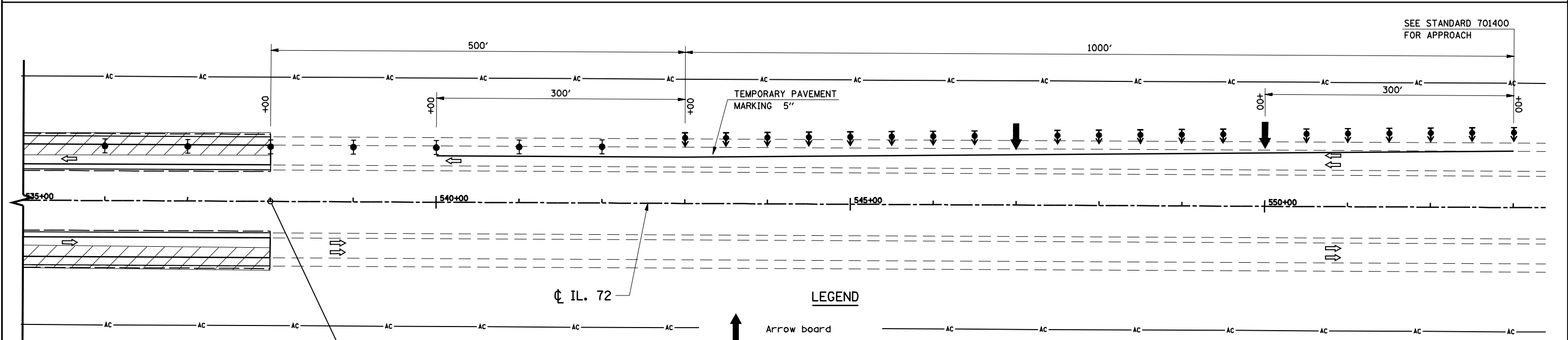
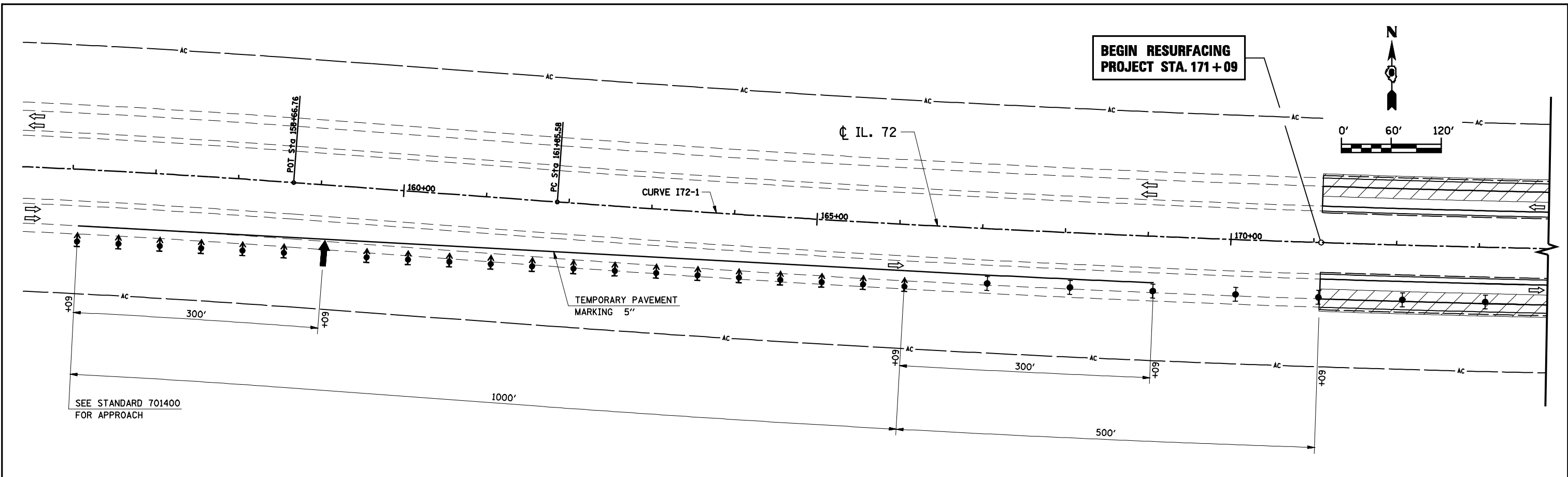


PLAN



SECTION B-B

NOTE:
SEE STD 609001 FOR BRIDGE APPROACH SHOULDER DRAIN DETAILS IN ADDITION TO DETAILS ON THIS SHEET.



**END RESURFACING
PROJECT STA. 538 + 00**

- LEGEND**
- Arrow board
 - Work area
 - Worker
 - Sign
 - Direction indicator barricade with steady burn monodirectional light
 - Type II barricade, drum, or vertical barricade with steady burn monodirectional light
 - Flagger with traffic control sign

NOTES:
1. SEE STANDARD 701401 FOR ADDITIONAL INFORMATION

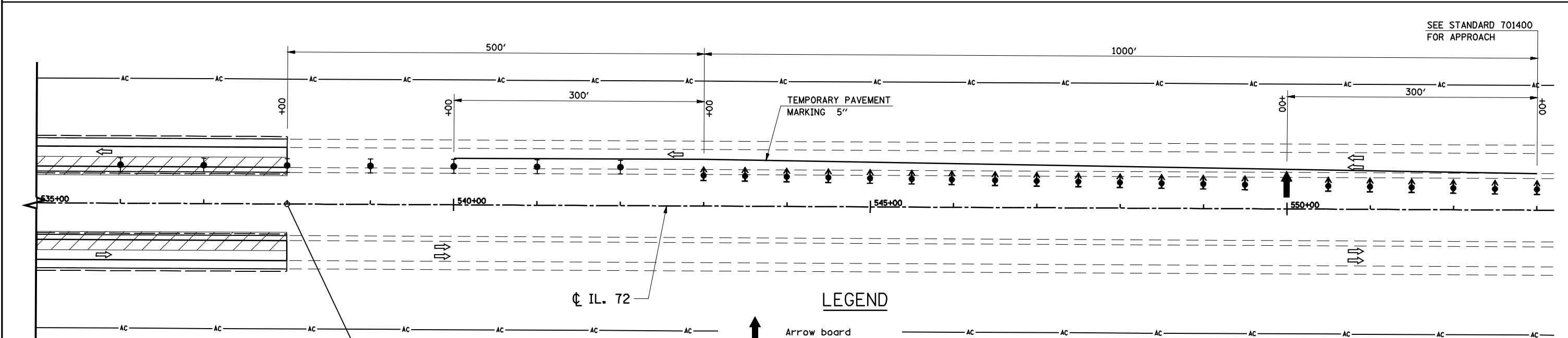
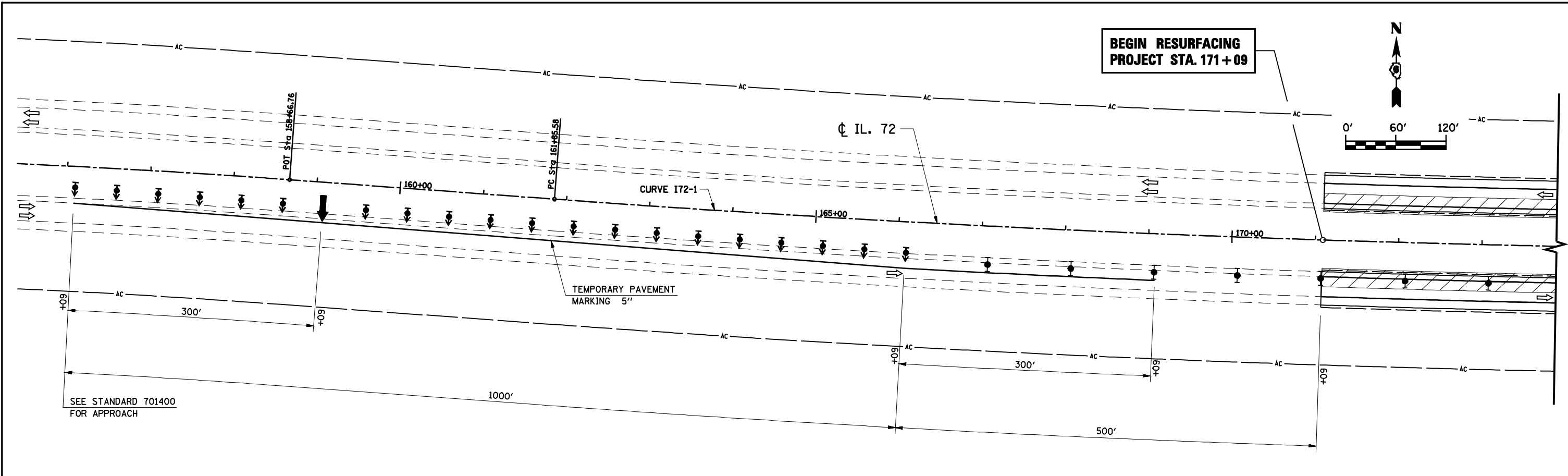
FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -
e:\pwork\pwidot\LAUGHLINRL\0184389\7016MOT.1.SHT		DRAWN - AAD	REVISED -
		CHECKED - JAC	REVISED -
		DATE - 06/11/07	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**








**I-72 MOT
STAGE 1**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	48
CONTRACT NO. 72910				
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				



LEGEND

-  Arrow board
-  Work area
-  Worker
-  Sign
-  Direction indicator barricade with steady burn monodirectional light
-  Type II barricade, drum, or vertical barricade with steady burn monodirectional light
-  Flagger with traffic control sign

NOTES:
 1. SEE STANDARD 701401 FOR ADDITIONAL INFORMATION

FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -
e:\pwwork\pwidot\LAUGHLINRL\0184389\7016MOT_2.SHT		DRAWN - AAD	REVISED -
PLOT SCALE = NONE		CHECKED - JAC	REVISED -
PLOT DATE = Feb-11-2010 08:59:21AM		DATE - 06/11/07	REVISED -

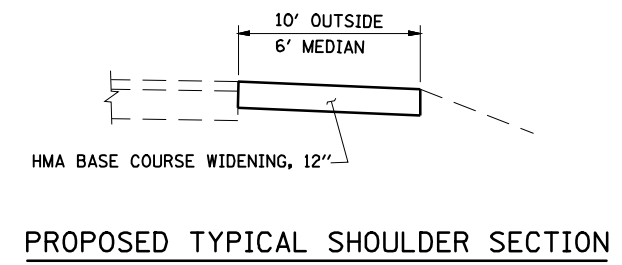
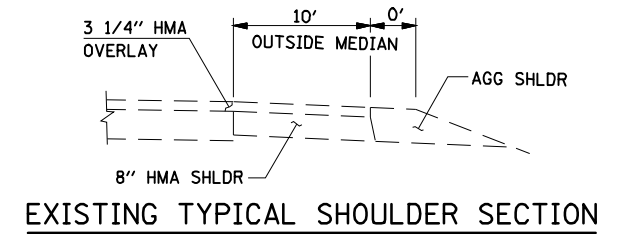
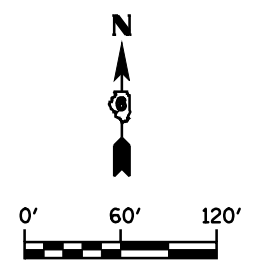
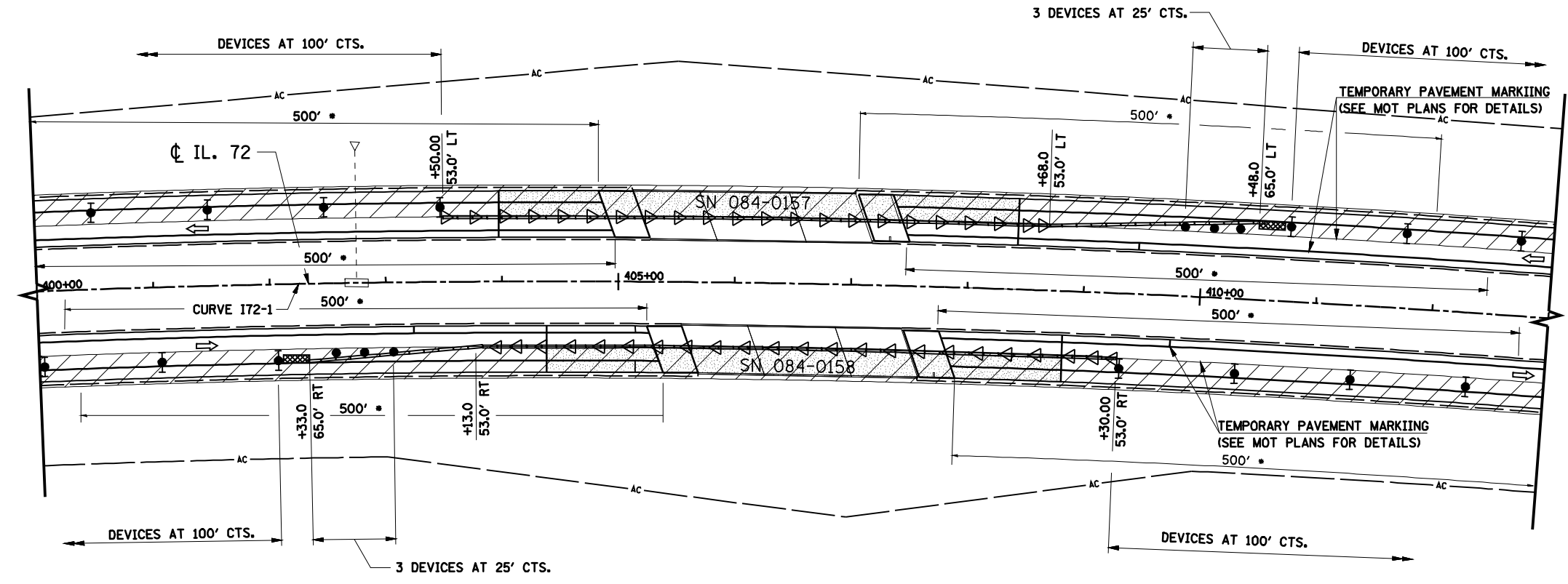
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

I-72 MOT STAGE 2			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	49
CONTRACT NO. 72910				
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

STAGE 1

* REMOVE & REPLACE 500' OF MEDIAN & OUTSIDE SHOULDER PRIOR TO STAGE 1 - EACH END OF EACH BRIDGE
 PR PAVED SHOULDER REMOVAL = 3,556 SQ YDS
 PR HMA BASE COURSE WIDENING (12") = 3,556 SQ YD
 COVER EXISTING APPROACH DRAIN STA. 407+80, LT



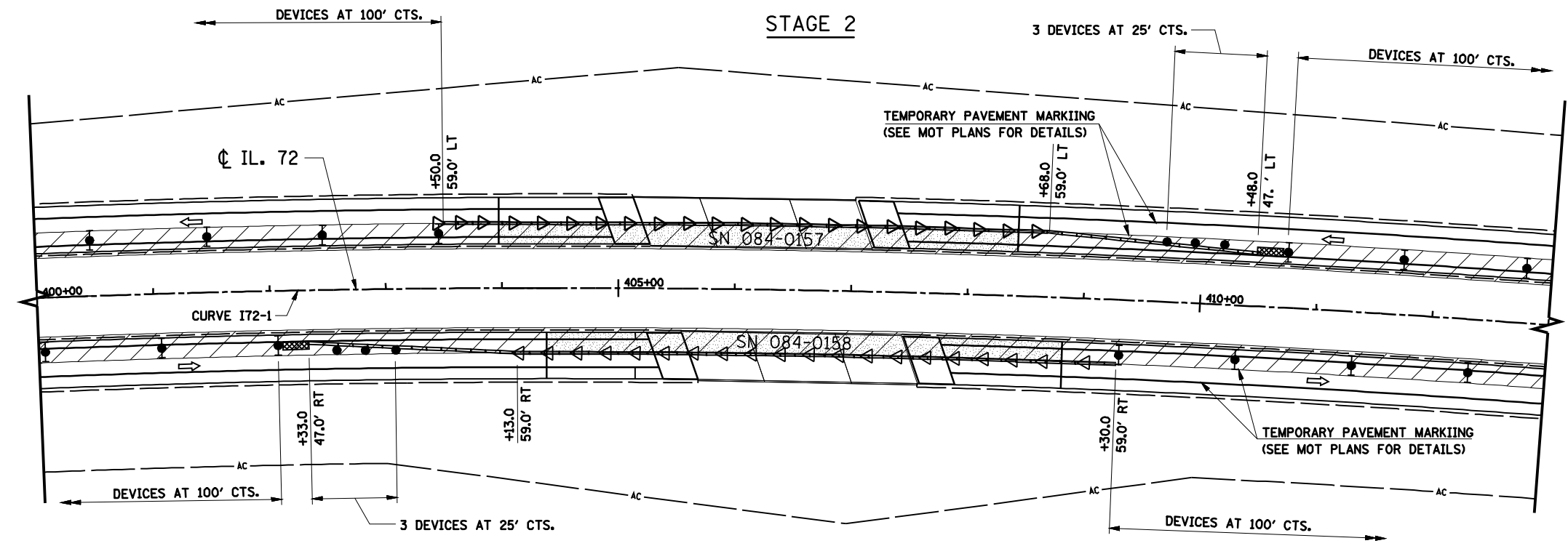
LEGEND

- ↑ Arrow board
- ▨ Mainline Work area
- ▤ Bridge Work Area
- ⊥ Sign
- ↑ Direction Indicator barricade with steady burn monodirectional light
- ⊥ Type II barricade, drum, or vertical barricade with steady burn monodirectional light
- Vertical panel with steady burn monodirectional light
- Temporary concrete barrier
- ◁ Monodirectional barrier wall guard marker
- ▨ Impact attenuator

NOTES:

1. SEE "I-72 MOT STAGE 1 & 2 PLANS FOR ADDITIONAL MAINTENANCE OF TRAFFIC LAYOUT.
2. SEE STANDARD 701402 FOR ADDITIONAL INFORMATION
3. OFFSETS TO CONC. BARRIER GIVEN TO TRAFFIC FACE

STAGE 2

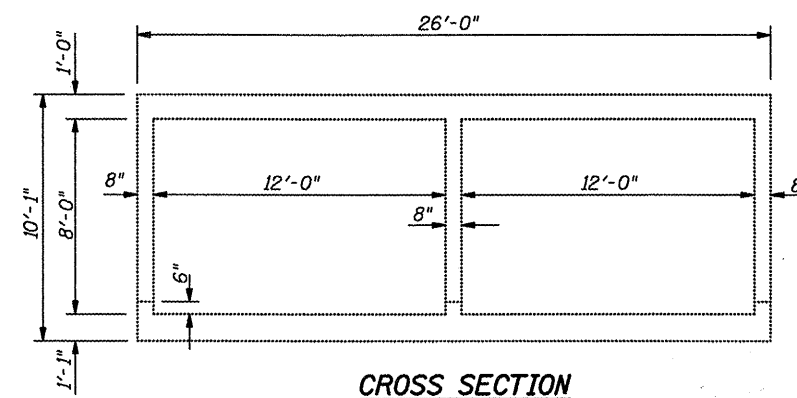
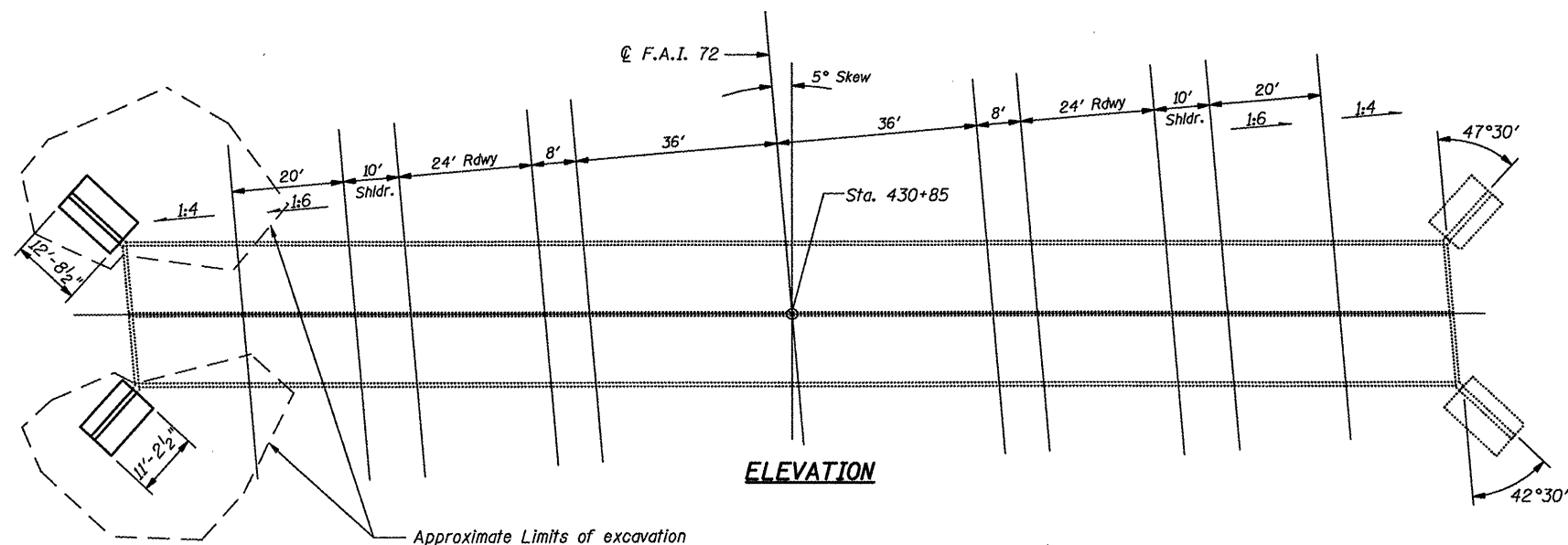


FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-72 BRIDGE MOT STAGE 1 & 2			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pwork\pwork\LAUGHLINRL\08183140\10	DOTRevised_7016MOT_BRDG.SHT	DRAWN - AAD	REVISED -					72	(84-10-2)RS-3, (84-10-3)RS-4	SANGAMON	61	50
	PLOT SCALE = NONE	CHECKED - JAC	REVISED -					CONTRACT NO. 72910				
	PLOT DATE = Mar-17-2010 08:16:28AM	DATE - 06/11/07	REVISED -					FED. ROAD DIST. NO. 2 [ILLINOIS] FED. AID PROJECT				
				SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.				

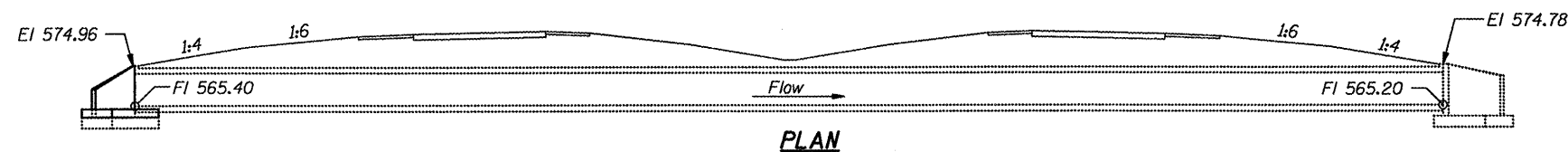
EXISTING STRUCTURE

The existing structure is a double 12' x 8' box culvert constructed on a 5.0-degree skew. The box culvert has an end to end barrel length of 237.75'. The wingwalls are a T-type vertical cantilever wingwall with a steel H pile-supported footing.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



Existing Northwest & Northeast Wingwalls to be Removed and Replaced. Existing Footings to Remain & be Reused.

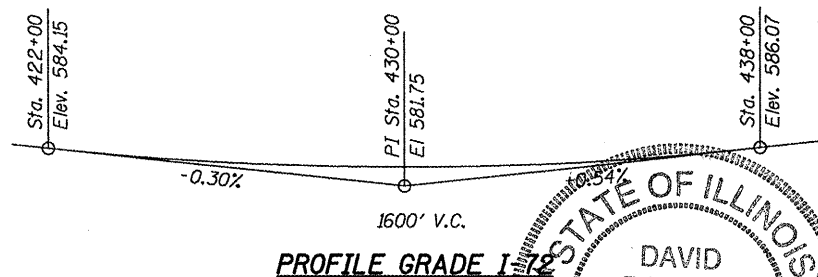


GENERAL NOTES
Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions

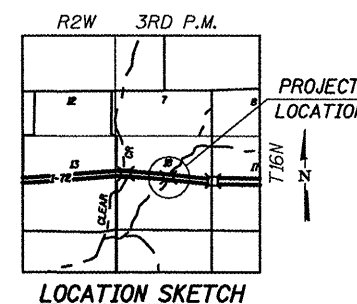
Reinforcement bars designated (E) shall be epoxy coated.
Plan dimensions and details relative to existing plans are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished based upon the unit price bid for the work.

DESIGNED	SCD
CHECKED	DRB
DRAWN	THW
CHECKED	-

ie consultants
IE CONSULTANTS, INC
6420 SOUTH SIXTH STREET
SPRINGFIELD, ILLINOIS 62712
TEL. (217) 529-8027
FAX (217) 529-4543
IESPRINGFIELD@IE-CONSULTANTS.COM
WWW.IE-CONSULTANTS.COM



STATE OF ILLINOIS
DAVID BOOHER
4775
LICENSED STRUCTURAL ENGINEER
Springfield, Ill.
David Booher, Illinois S.E. 081-004775
Expires 11-30-2010



DESIGN SPECIFICATIONS
2002 AASHTO Standard Specifications - 17th ed.

LOADING HS20-44

DESIGN STRESS

Cast-in-Place Unit
f'c = 3500 psi
f's = 60,000 psi
n = 9

PROPOSED WORK
1. Remove and replace NE & NW wingwalls.

EXISTING PILE DATA

STEEL PILES: HP 10x42
20 TON CAPACITY
EST. LENGTH - 18' NORTH W.W.
13' SOUTH W.W.
NUMBER REQ'D 24 (6 PER W.W.)

INDEX OF SHEETS

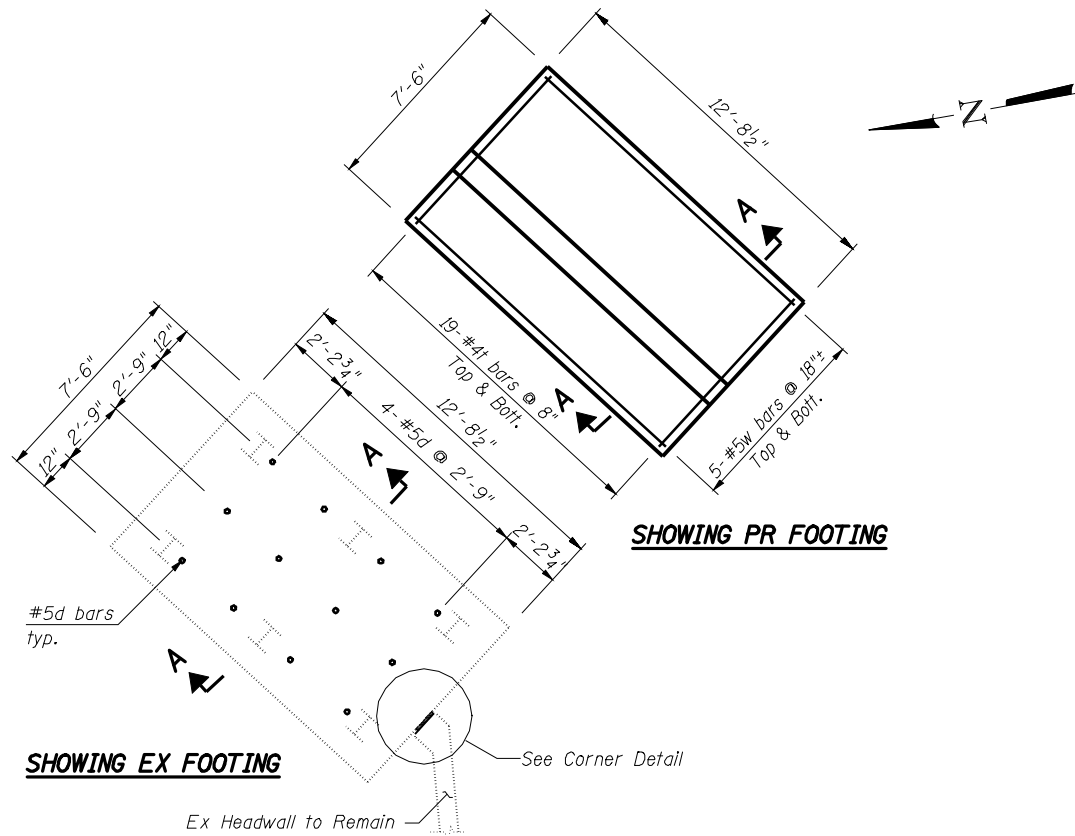
- General Plan
- Wingwall Detail

GENERAL PLAN
F.A.I. RTE. 72 OVER
CLEAR CREEK TRIBUTARY
F.A.I. 72 SECTION (84-10-3) RS-4
SANGAMON COUNTY
STATION 430+85.00
S.N. 084-2006

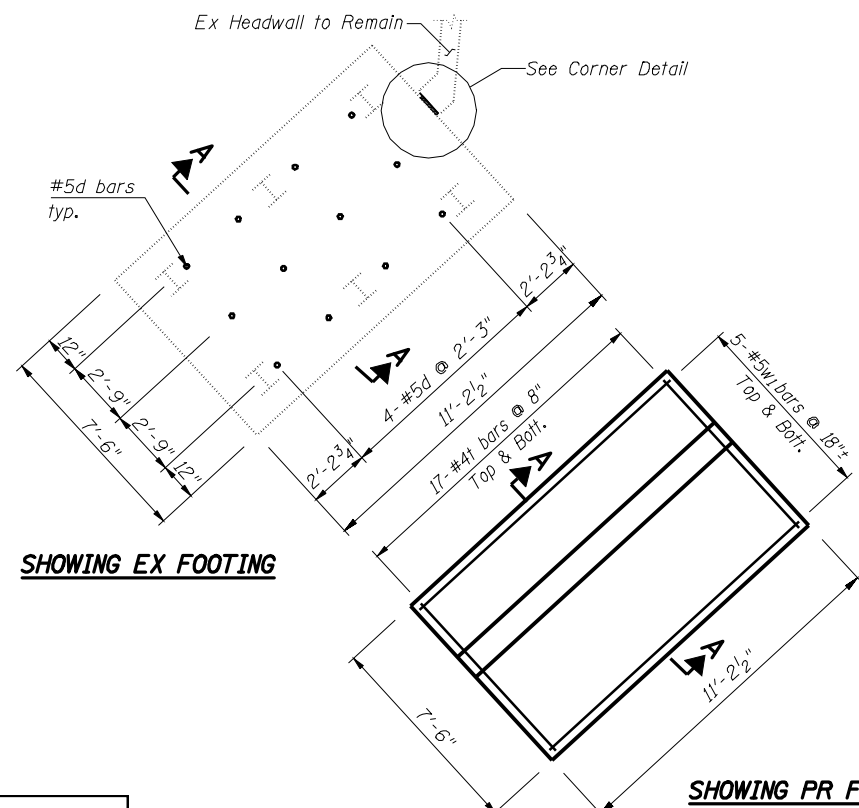
SHEET NO.	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1 OF	72	(84-10-2) RS-3, (84-10-3) RS-4	SANGAMON	61	51
2 SHEETS			CONTRACT NO. 72910		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT					

2-19-2010
Date

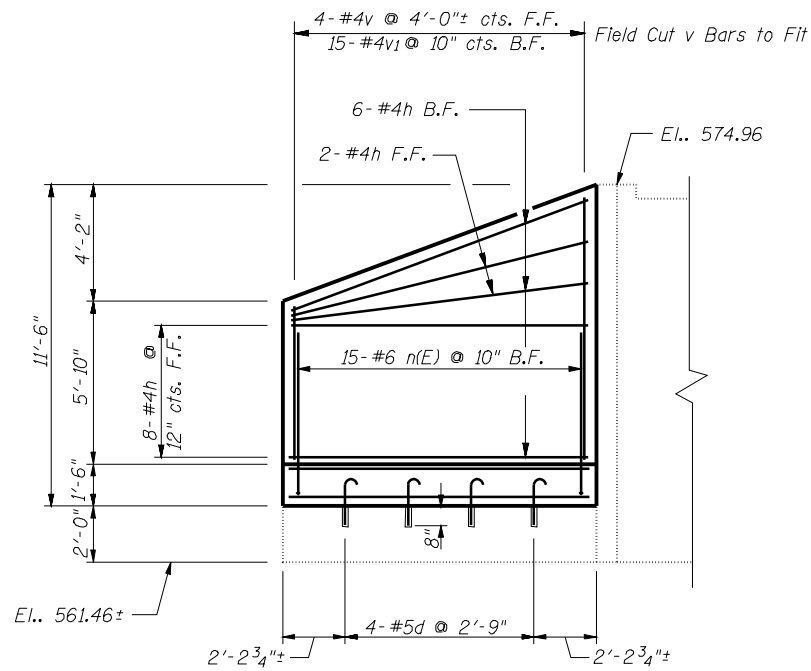
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



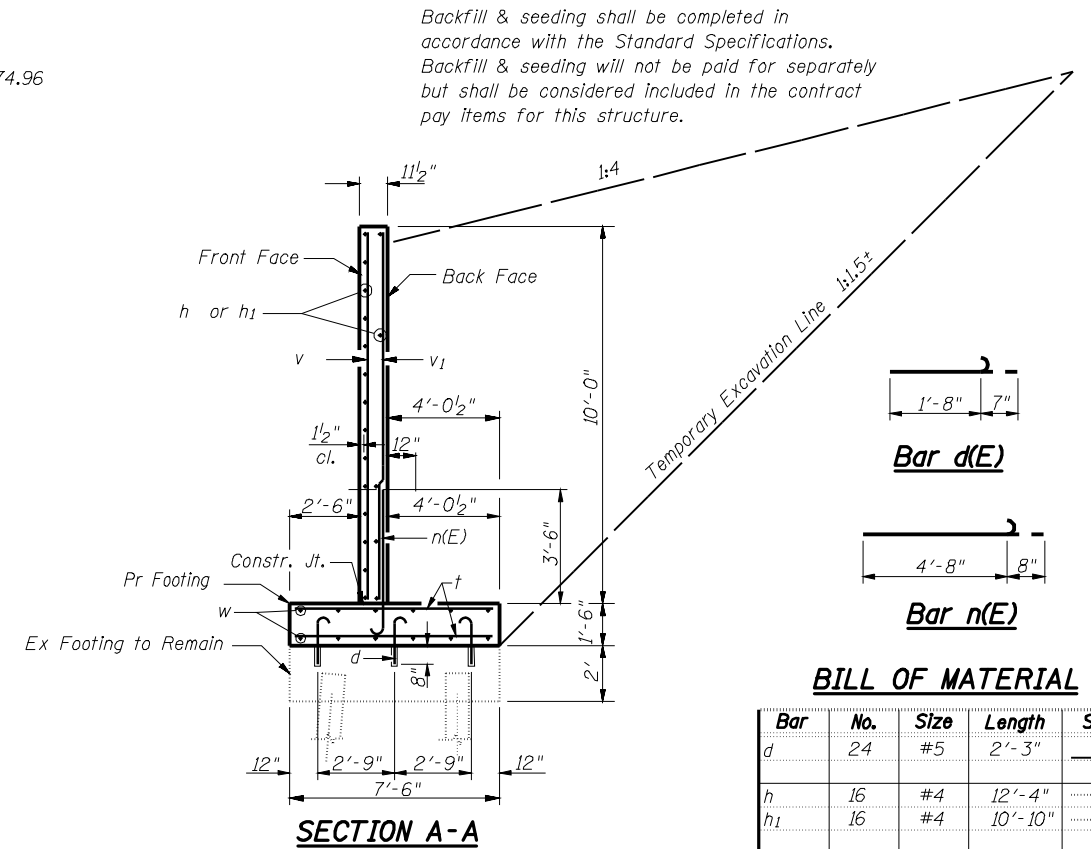
PLAN-NORTHEAST WINGWALL



PLAN - NORTHWEST WINGWALL



ELEVATION-NORTHEAST WINGWALL



BILL OF MATERIAL

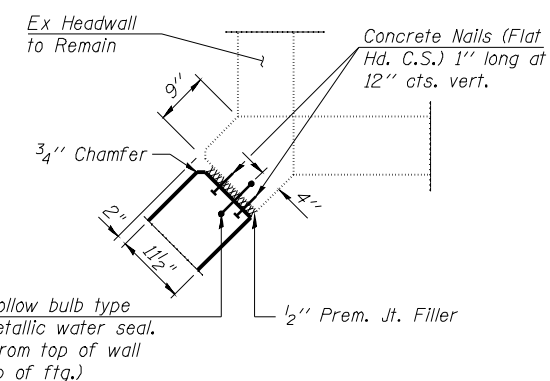
Bar	No.	Size	Length	Shape
d	24	#5	2'-3"	U
h	16	#4	12'-4"	—
h1	16	#4	10'-10"	—
n(E)	29	#6	5'-4"	U
t	36	#4	7'-2"	—
v	8	#4	9'-9"	—
v1	29	#4	9'-9"	—
w	5	#5	12'-4"	—
w1	5	#5	10'-10"	—
Concrete Box Culverts			Cu. Yd.	16.7
Concrete Removal			Cu. Yd.	8.0
Reinforcement Bars, Epoxy Coated			Pound	240
Reinforcement Bars			Pound	840
Structure Excavation			Cu. Yd.	515

Notes:
 Reinforcement bars designated (E) shall be epoxy coated.
 Bars indicated thus 12x4-#5 etc. indicates 12 lines of bars with 4 lengths per line.

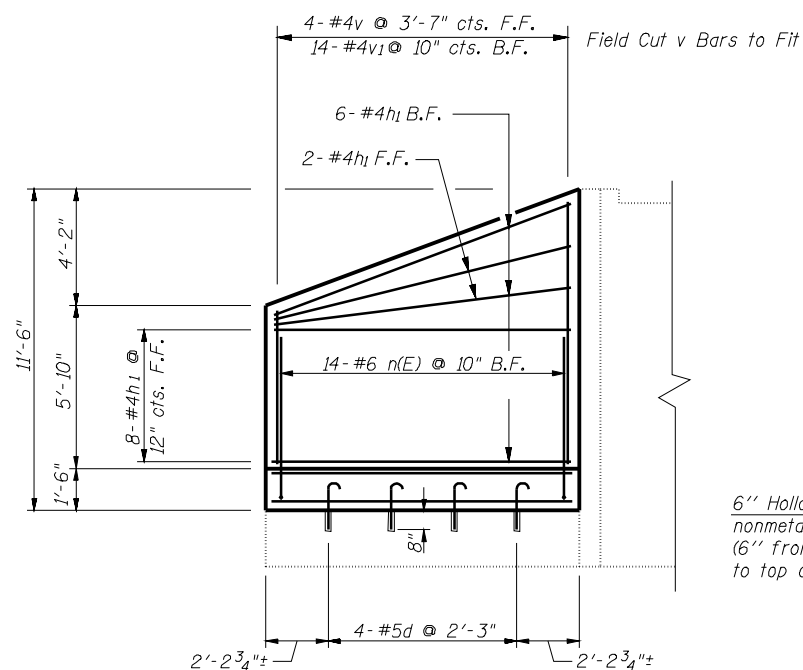
MIN. BAR LAP

#6 2'-7"

**WINGWALL DETAIL
STRUCTURE NO. 084-2006**



CORNER DETAIL



ELEVATION - NORTHWEST WINGWALL

DESIGNED	SCD
CHECKED	DRB
DRAWN	THW
CHECKED	-



IE CONSULTANTS, INC
 6420 SOUTH SIXTH STREET
 SPRINGFIELD, ILLINOIS 62712
 TEL. (217) 529-8027
 FAX (217) 529-4543
 IESPRINGFIELD@IE-CONSULTANTS.COM
 WWW.IE-CONSULTANTS.COM

SHEET NO. 2 OF 2 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	72	(84-10-2) RS-3, (84-10-3) RS-4	SANGAMON	61	52
			CONTRACT NO. 72910		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT					

EXISTING STRUCTURE:

The existing bridges are dual structures constructed on a tangent vertical alignment. The roadway is constructed on a horizontal curve with the bridges constructed parallel to the local tangent. Each bridge is 186.00' back to back of abutments with an out to out width of 42.00'. Clear width is 38.50'. The span lengths from west to east are 57.00' - 66.83' - 57.00'. The bridges are constructed on a 20.00 degree skew right ahead.

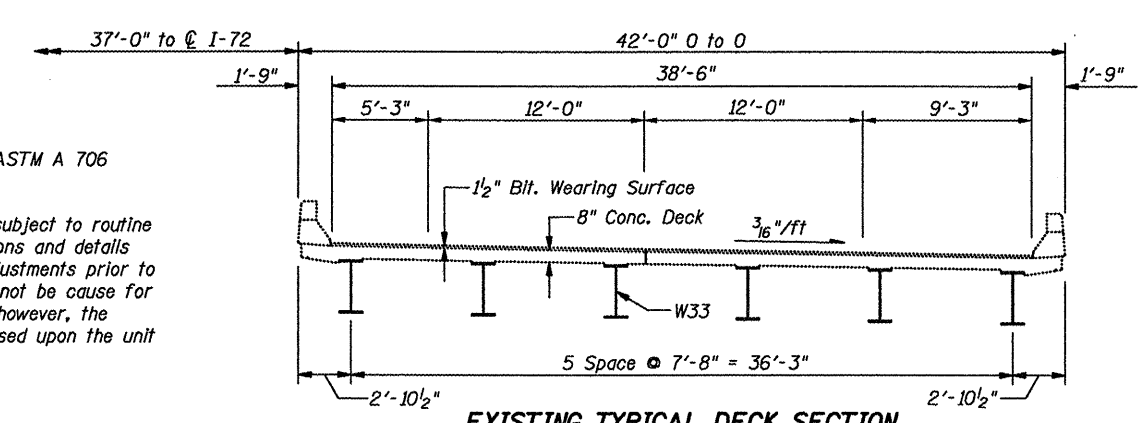
The superstructures consist of three span continuous composite steel wide flange beams and an 8" reinforced concrete deck with a 1.50" bituminous overlay. All the beams are 33" steel wide flange beams.

The substructure consists of two hammerhead type reinforced concrete piers, which rest on timber pile-supported footings and reinforced concrete pile bent abutments on concrete piles.

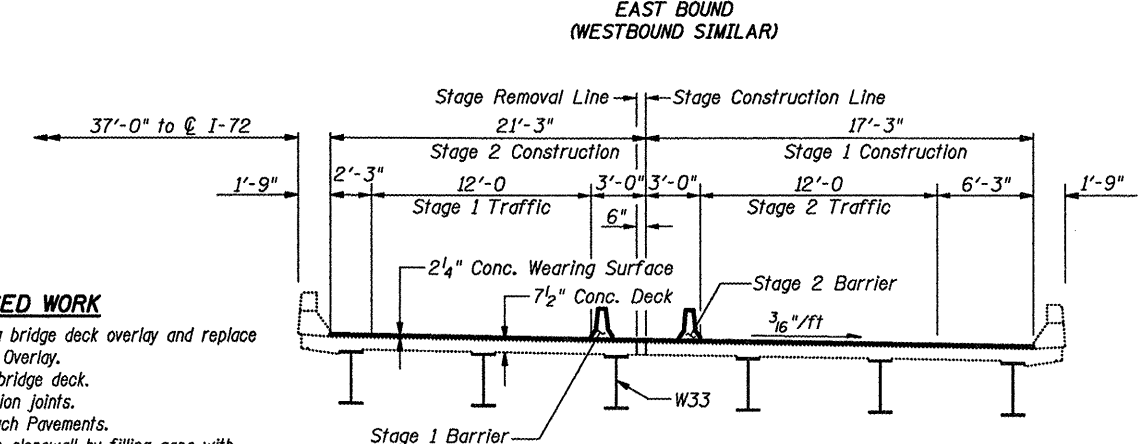
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions
Reinforcement bars designated (E) shall be epoxy coated.
Plan dimensions and details relative to existing plans are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished based upon the unit price bid for the work.



EXISTING TYPICAL DECK SECTION



PROPOSED TYPICAL DECK SECTION

PROPOSED WORK

1. Remove existing bridge deck overlay and replace with Microsilica Overlay.
2. Patch existing bridge deck.
3. Replace expansion joints.
4. Replace Approach Pavements.
5. Repair concrete slopewall by filling gaps with controlled Low Strength Material.

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
HMA Surface Removal (Deck)	Sq. Yd.	1,574		1,574
Concrete Removal	Cu. Yd.	24.2		24.2
Concrete Structures	Cu. Yd.	57.1		57.1
Concrete Superstructures	Cu. Yd.	270.6		270.6
Reinforcement Bars, Epoxy Coated	Pound	63,320		63,320
Bar Splicers	Each	492		492
Preformed Joint Strip Seal	Foot	180		180
Temporary Concrete Barrier	Foot	1,400		1,400
Relocate Temporary Concrete Barrier	Foot	1,400		1,400
* Deck Slab Repair (Full Depth Type I)	Sq. Yd.	37		37
* Deck Slab Repair (Full Depth Type II)	Sq. Yd.	37		37
Bridge Deck Microsilica Concrete Overlay, 2 1/4"	Sq. Yd.	1,540		1,540
Bridge Deck Hydro-Scarification, 1/2"	Sq. Yd.	1,540		1,540
Controlled Low Strength Material	Cu. Yd.	4.0		4.0
Bridge Deck Grooving	Sq. Yd.	2,167.7		2,167.7
Protective Coat	Sq. Yd.	2,167.7		2,167.7

* Full Depth Repair quantities are estimated. Actual location and size are to be determined by the Field Engineer. Estimated quantity of Deck Slab Repair (Partial) is 156 Sq Yd.

DESIGN SPECIFICATIONS

2002 AASHTO Standard Specifications - 17th ed.

LOADING HS20-44

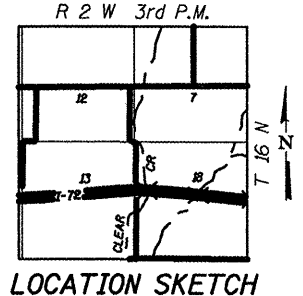
DESIGN STRESSES

Cast-in Place Units
f'c = 3,500 psi
f's = 60,000 psi
n = 9

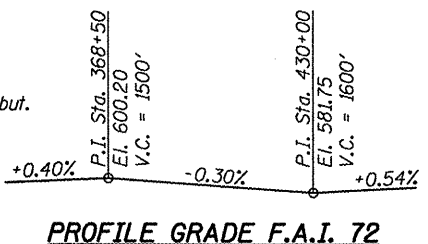
INDEX OF SHEETS

1. General Plan
2. Preformed Joint Strip Seal
3. Joint Replacement Details
4. Deck Plan
5. Bridge Approach Slab Details
6. Bridge Approach Slab Details
7. Bar Splicer Assembly and Mechanical Splicer Details

GENERAL PLAN
E.A.I. RTE. 72 OVER CLEAR CREEK
SECTION (84-10-3) RS-4
SANGAMON COUNTY
STATION 406+40.00
S.N. 084-0157 (W.B.) & 084-0158 (E.B.)

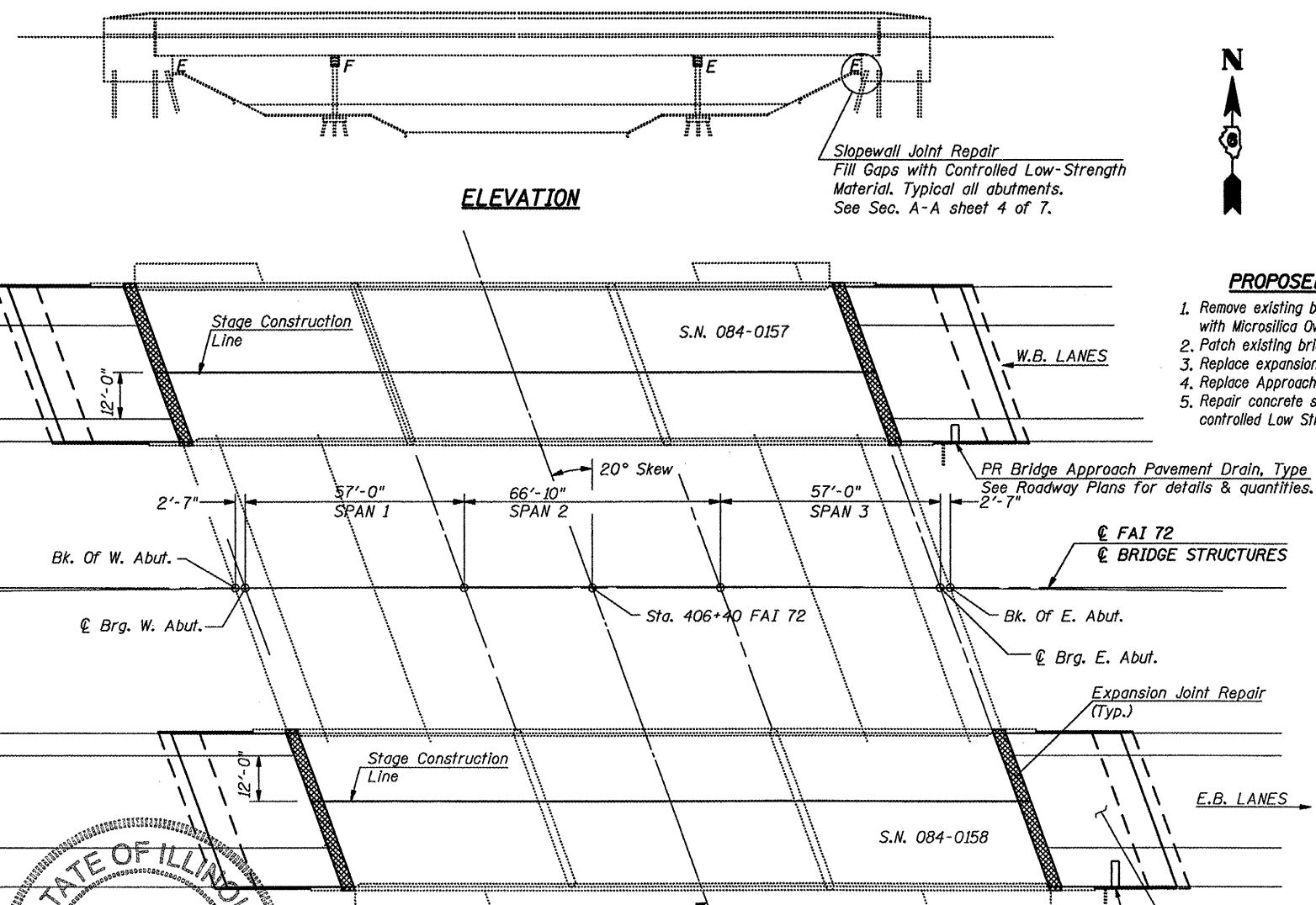


LOCATION SKETCH



PROFILE GRADE F.A.I. 72

SHEET NO. 1 OF 7 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	72	(84-10-2) RS-3, (84-10-3) RS-4	SANGAMON	61	53
			CONTRACT NO. 72910		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT					

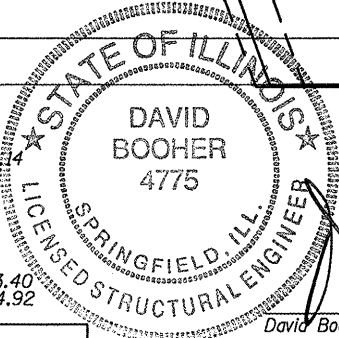


ELEVATION

PLAN

CURVE DATA

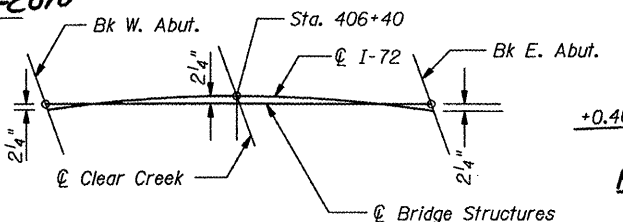
P.I. STA = 405+39.34
Δ = 19°09'27" RT
D = 0°30'00"
R = 11,459.16
T = 1933.81'
L = 3831.52'
E = 162.03'
P.C. STA = 386+23.40
P.T. STA = 424+54.92



David Booher, Illinois S.E. 081-004775
Expires 11-30-2010
IE CONSULTANTS, INC
6420 SOUTH SIXTH STREET
SPRINGFIELD, ILLINOIS 62712
TEL. (217) 529-8027
FAX (217) 529-4543
IESPRINGFIELD@IE-CONSULTANTS.COM
WWW.IE-CONSULTANTS.COM

DESIGNED	SCD
CHECKED	DRB
DRAWN	THW
CHECKED	-

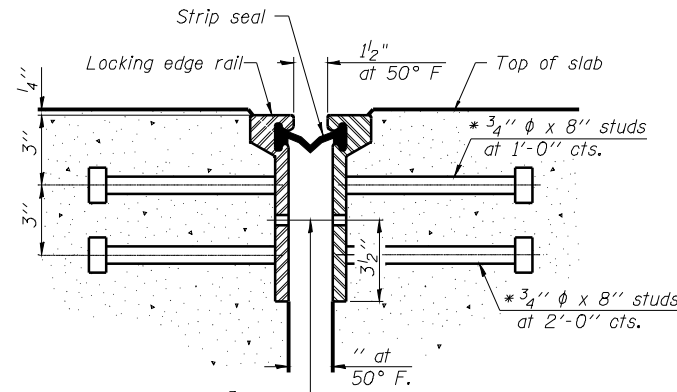
Date 2-19-2010



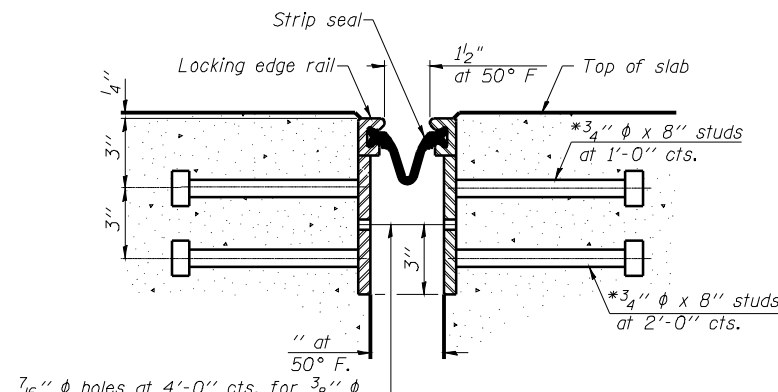
PLAN

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

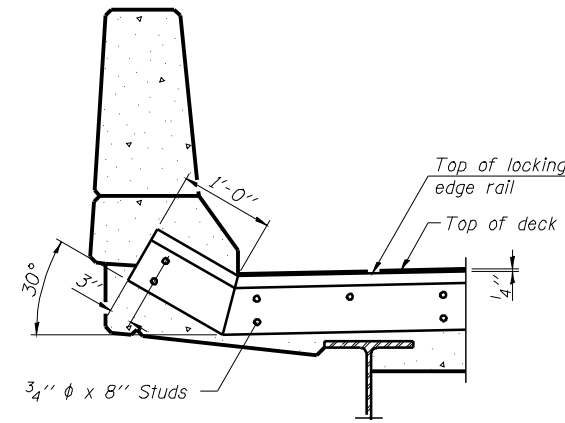
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



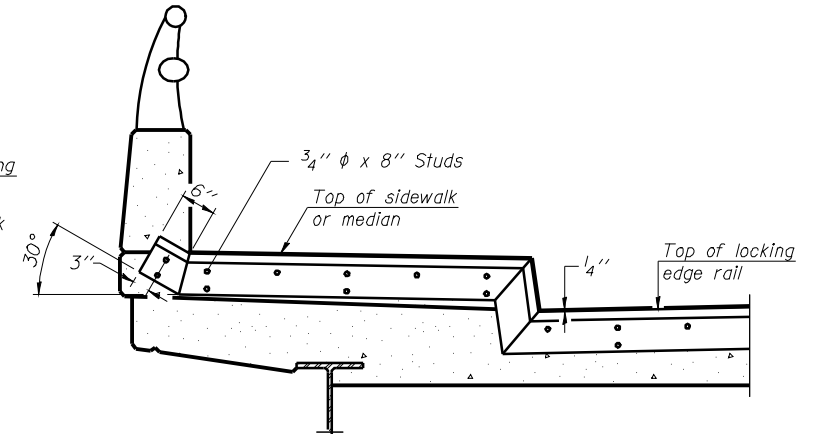
**SECTION THRU
ROLLED RAIL JOINT**



**SECTION THRU
WELDED RAIL JOINT**



AT PARAPET
See Section A-A for end treatment of skews > 30°.



AT SIDEWALK OR MEDIAN
Shorter plates with a single row of studs at 12 inch cts. may be necessary on medians which are shallower than 9 inch. See manufacturer's recommendation.

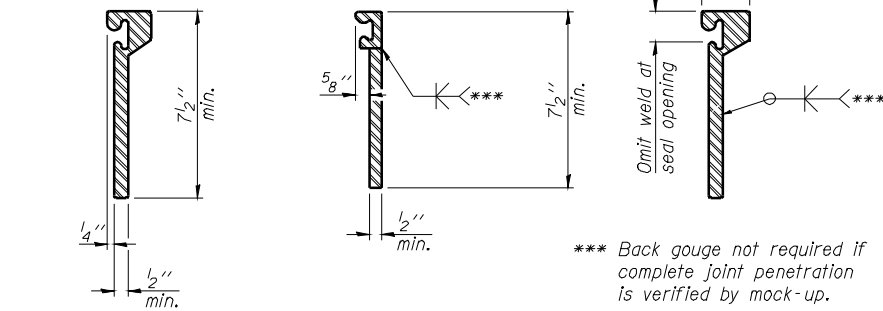
7/16 inch diameter holes at 4-foot centers for 3/8 inch diameter bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

7/16 inch diameter holes at 4-foot centers for 3/8 inch diameter bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

TYPICAL END TREATMENTS

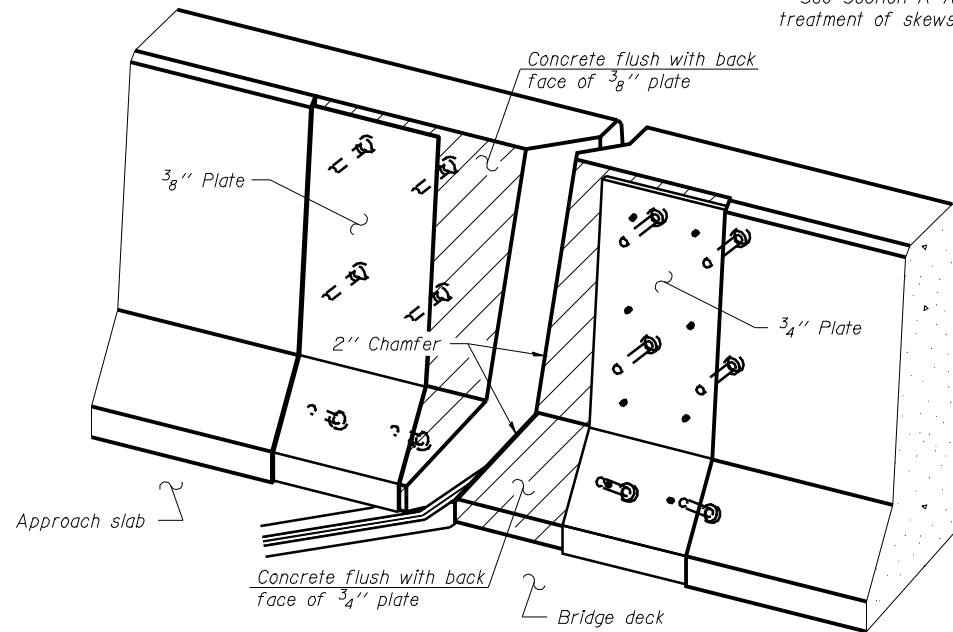
Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4 inch. The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches. The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities. The manufacturer's recommended installation methods shall be followed. The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State. All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications. Maximum space between rail segments at stage lines shall be 3/16 inch, sealed with a suitable sealant.



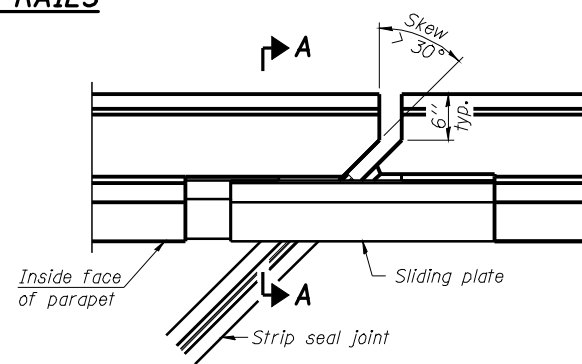
**ROLLED
EXTRUDED RAIL** **WELDED RAIL**

**LOCKING EDGE
RAIL SPLICE**
The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

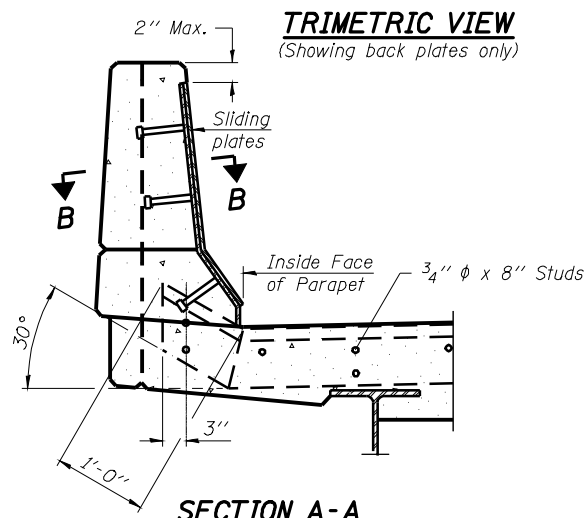


TRIMETRIC VIEW
(Showing back plates only)

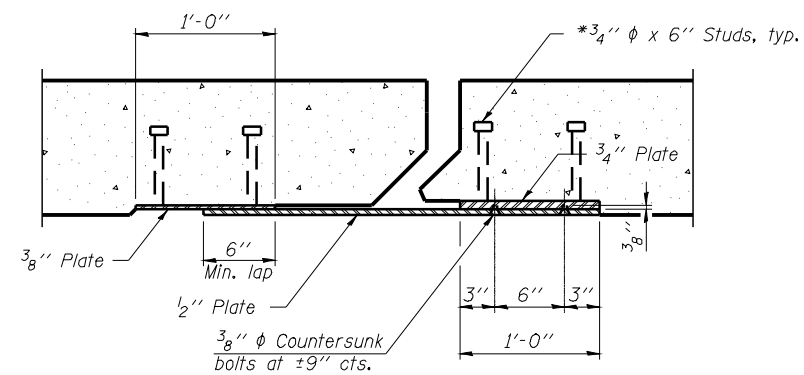
LOCKING EDGE RAILS



PLAN



SECTION A-A
POINT BLOCK DETAILS
(for skews > 30°)



SECTION B-B

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	180

PREFORMED JOINT STRIP SEAL
S.N. 084-0157 (W.B.) & 084-0158 (E.B.)

DESIGNED	SCD
CHECKED	DRB
DRAWN	THW
CHECKED	-



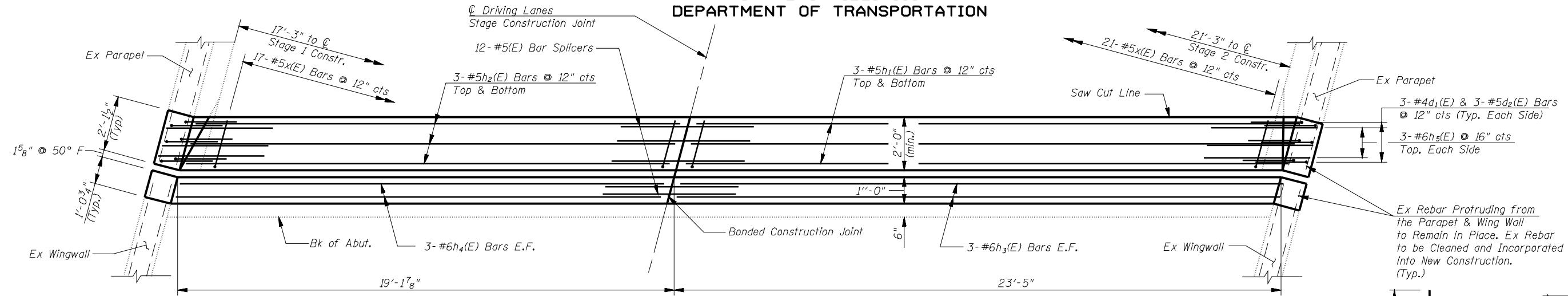
IE CONSULTANTS, INC.
6420 SOUTH SIXTH STREET
SPRINGFIELD, ILLINOIS 62712
TEL. (217) 529-8027
FAX (217) 529-4543
IESPRINGFIELD@IE-CONSULTANTS.COM
WWW.IE-CONSULTANTS.COM

EJ-SSJ

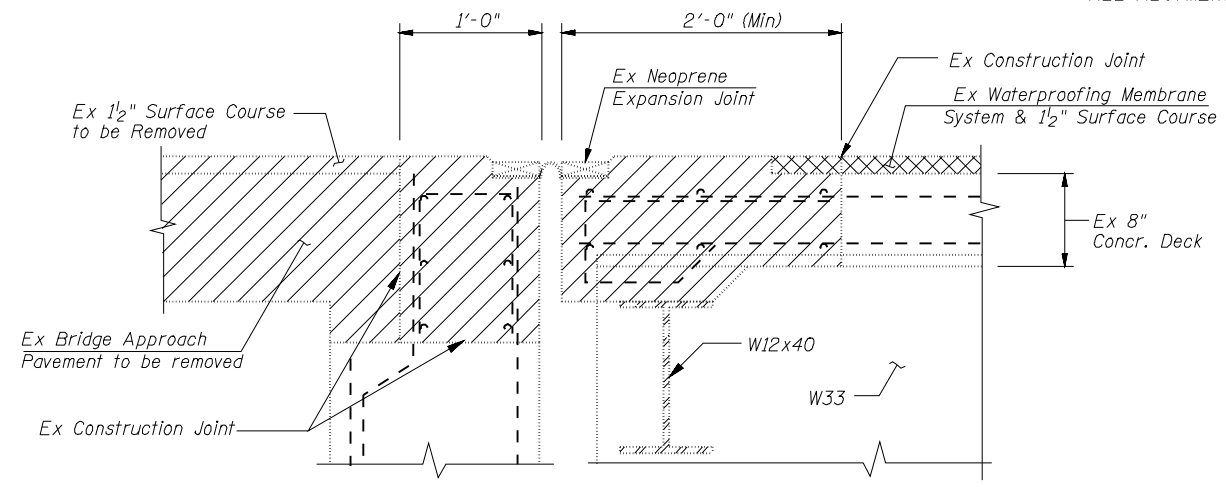
11-1-09

SHEET NO.	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2 OF 7 SHEETS	72	(84-10-2) RS-3, (84-10-3) RS-4	SANGAMON	61	54
CONTRACT NO. 72910					
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT					

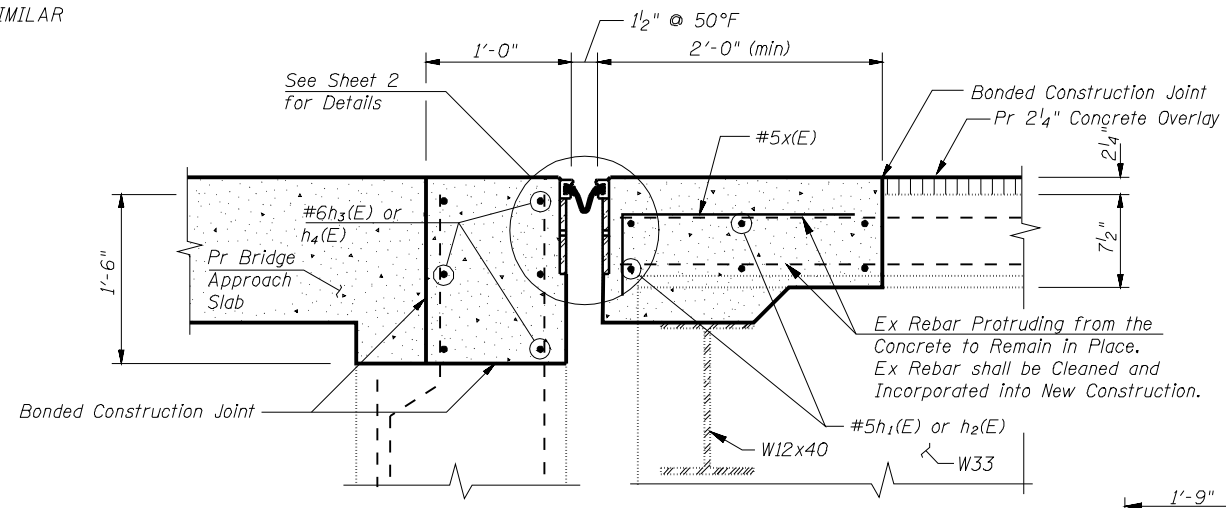
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



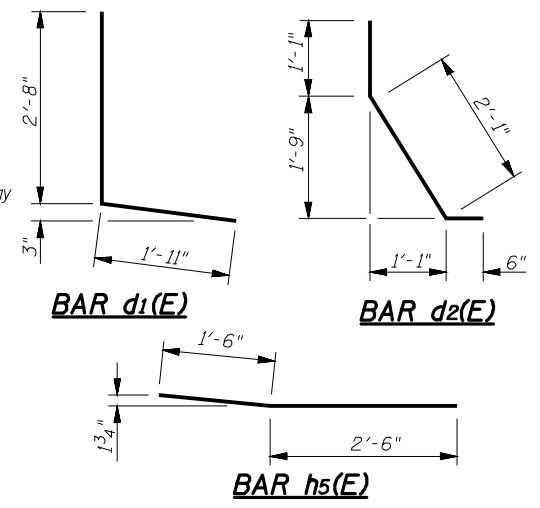
PLAN
(WEST ABUTMENT ON SOUTH BRIDGE LOOKING EAST)
ALL ABUTMENTS SIMILAR



SECTION THRU EXISTING ABUTMENT
ALL DIMENSIONS PERPENDICULAR TO JOINT



SECTION THRU PROPOSED ABUTMENT

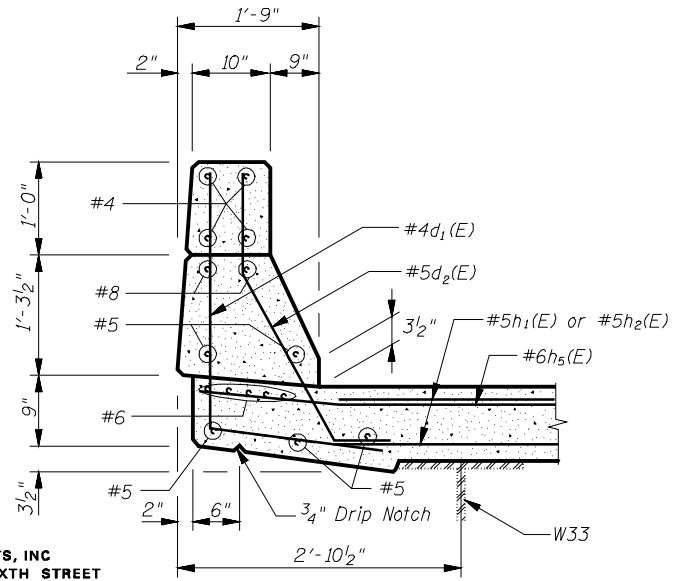


BILL OF MATERIAL

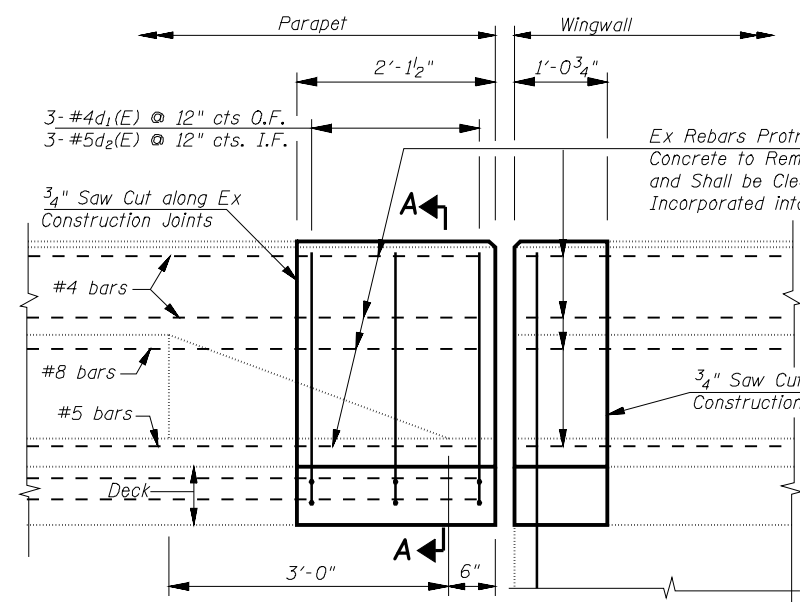
Bar	No.	Size	Length	Shape	
d ₁ (E)	24	#4	4'-7"		
d ₂ (E)	24	#5	3'-8"		
h ₁ (E)	24	#5	22'-6"		
h ₂ (E)	24	#5	18'-2"		
h ₃ (E)	24	#6	23'-0"		
h ₄ (E)	24	#6	19'-0"		
h ₅ (E)	24	#6	4'-0"		
x (E)	152	#5	2'-7"		
Concrete Removal				Cu. Yd.	24.2
Concrete Superstructures				Cu. Yd.	24.2
Reinforcement Bars, Epoxy Coated				Pound	3,260
Bar Splicers				Each	48

Notes:
Reinforcement bars designated (E) shall be epoxy coated.
Bars indicated thus 12x4- #5 etc. indicates 12 lines of bars with 4 lengths per line.

Note
Existing reinforcement shall be cleaned and incorporated into the new construction. Cost included with Concrete Removal.



SECTION A-A



CONCRETE REMOVAL & REPLACEMENT AT PARAPET JOINT

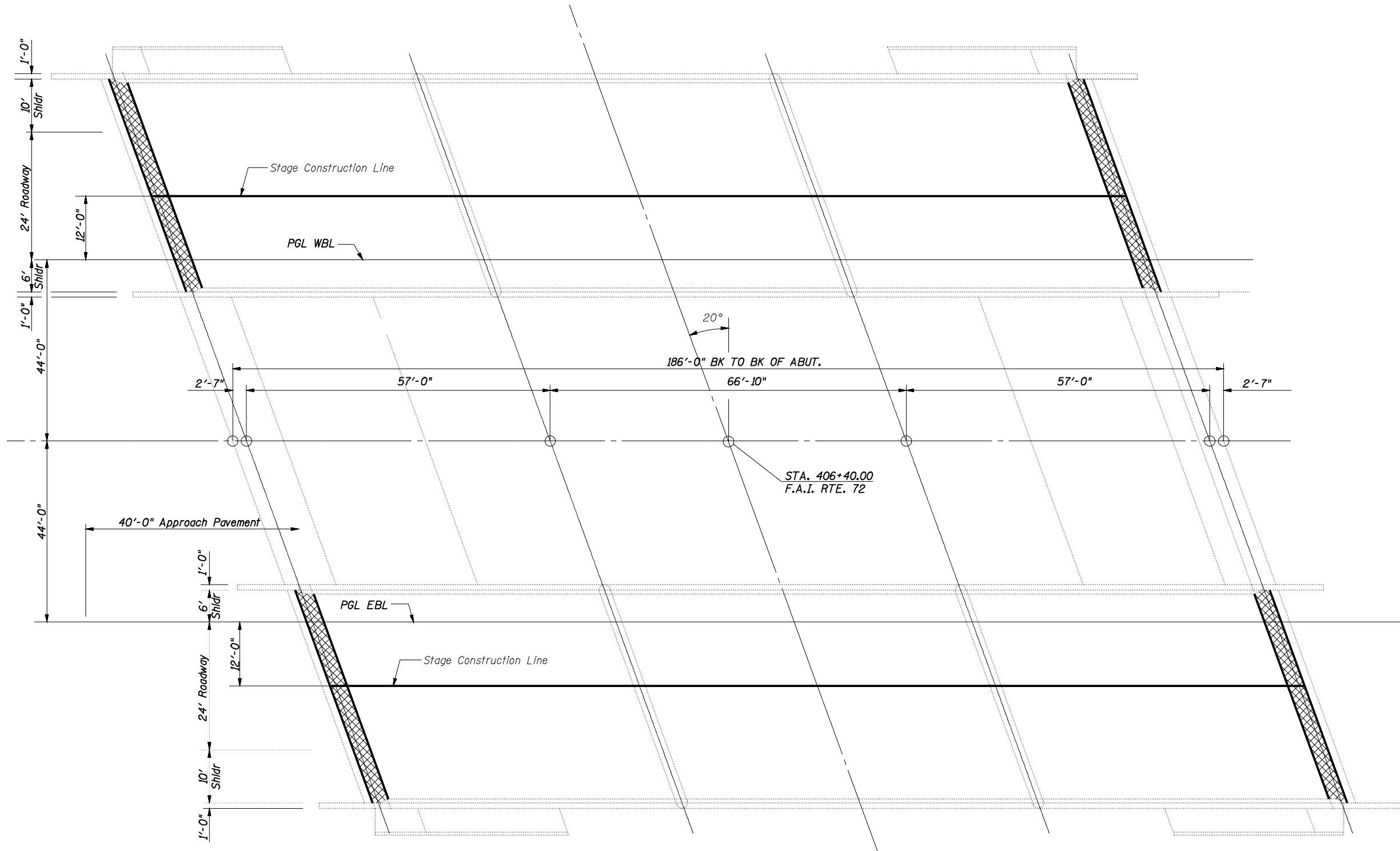
DESIGNED	SCD
CHECKED	DRB
DRAWN	THW
CHECKED	-

ie consultants
IE CONSULTANTS, INC
6420 SOUTH SIXTH STREET
SPRINGFIELD, ILLINOIS 62712
TEL. (217) 529-8027
FAX (217) 529-4543
IESPRINGFIELD@IE-CONSULTANTS.COM
WWW.IE-CONSULTANTS.COM

SHEET NO. 3 OF 7 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	72	(84-10-2) RS-3, (84-10-3) RS-4	SANGAMON	61	55
			CONTRACT NO. 72910		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT					

JOINT REPLACEMENT DETAILS
S.N. 084-0157 (W.B.) & 084-0158 (E.B.)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



GENERAL NOTES

Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions

Reinforcement bars designated (E) shall be epoxy coated.

Plan dimensions and details relative to existing plans are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished based upon the unit price bid for the work.

PLAN



EXPANSION JOINT REPLACEMENT
(SEE SHEET 2 & 3 FOR DETAILS)

DESIGNED	SCD
CHECKED	DRB
DRAWN	THW
CHECKED	-



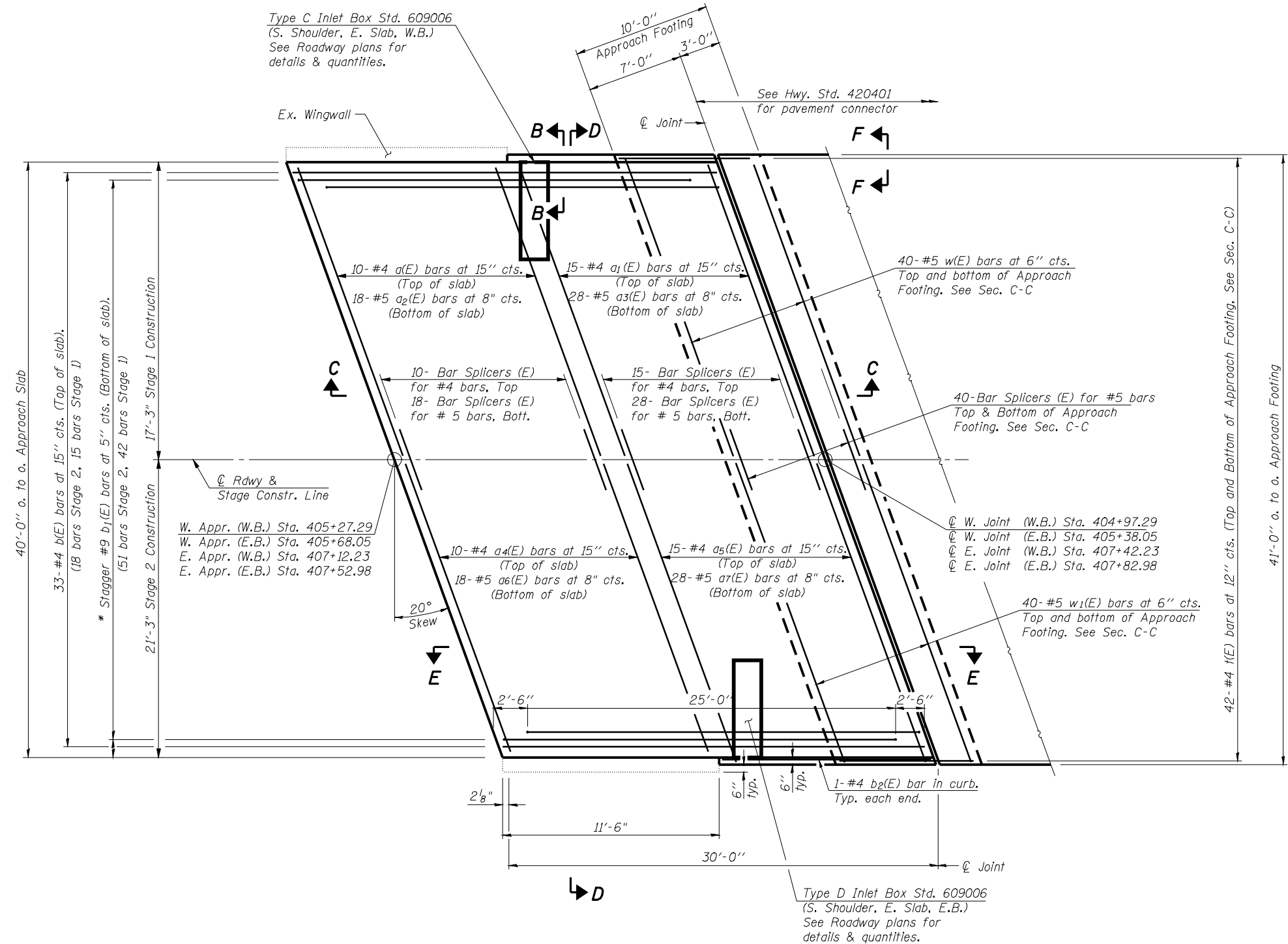
IE CONSULTANTS, INC
6420 SOUTH SIXTH STREET
SPRINGFIELD, ILLINOIS 62712
TEL. (217) 529-8027
FAX (217) 529-4543
IESPRINGFIELD@IE-CONSULTANTS.COM
WWW.IE-CONSULTANTS.COM

DECK PLAN
S.N. 084-0157 (W.B.) & 084-0158 (E.B.)

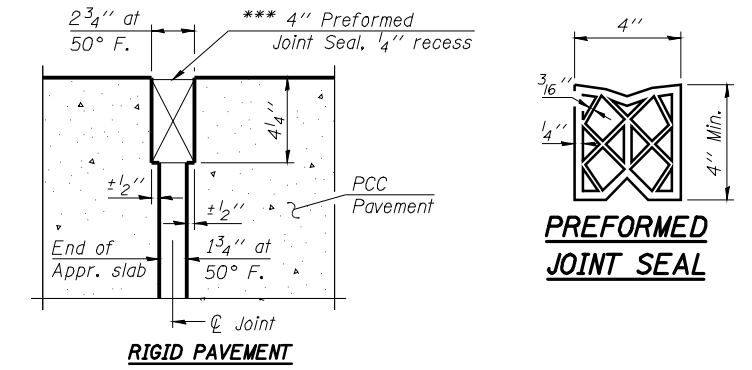
SHEET NO.	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4 OF 7 SHEETS	72	(84-10-2) RS-3, (84-10-3) RS-4	SANGAMON	61	56
CONTRACT NO. 72910					
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

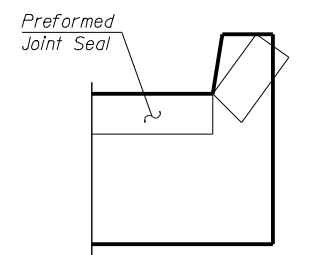
Notes:
See sheet 6 of 7 for Sections C-C & D-D and View E-E.
a(E) thru ar(E) bar spacings measured along ϕ Rdwy.



*** Cost included with Concrete Superstructure.



DETAIL A



VIEW F-F

Angle Preformed Joint Seal at 45° at curbs when req'd for drainage.

PLAN

(E. Slab W.B. - other slabs similar)
* Tilt #9 b₁(E) bars as required to maintain clearance.
Order a₁(E), a₃(E), a₅(E), a₇(E) b(E) and b₁(E) bars full length. Cut in field to fit Type C or Type D Inlet box, Hwy Std 609006.

DESIGNED	SCD
CHECKED	DRB
DRAWN	THW
CHECKED	-

ie consultants
IE CONSULTANTS, INC
6420 SOUTH SIXTH STREET
SPRINGFIELD, ILLINOIS 62712
TEL. (217) 529-8027
FAX (217) 529-4543
IESPRINGFIELD@IE-CONSULTANTS.COM
WWW.IE-CONSULTANTS.COM

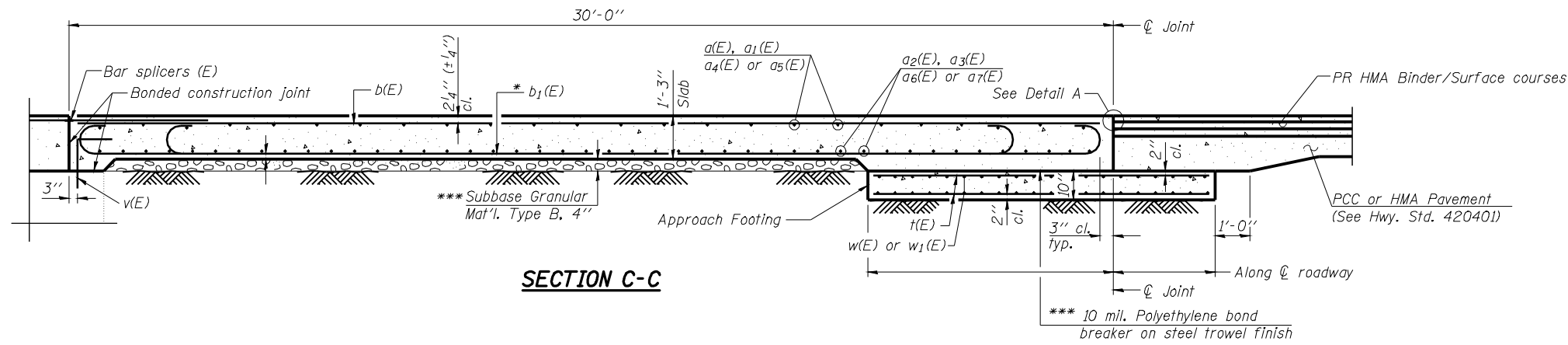
BA-R 11-1-09

BRIDGE APPROACH SLAB DETAILS
S.N. 084-0157 (W.B.) & 084-0158 (E.B.)
(Sheet 1 of 2)

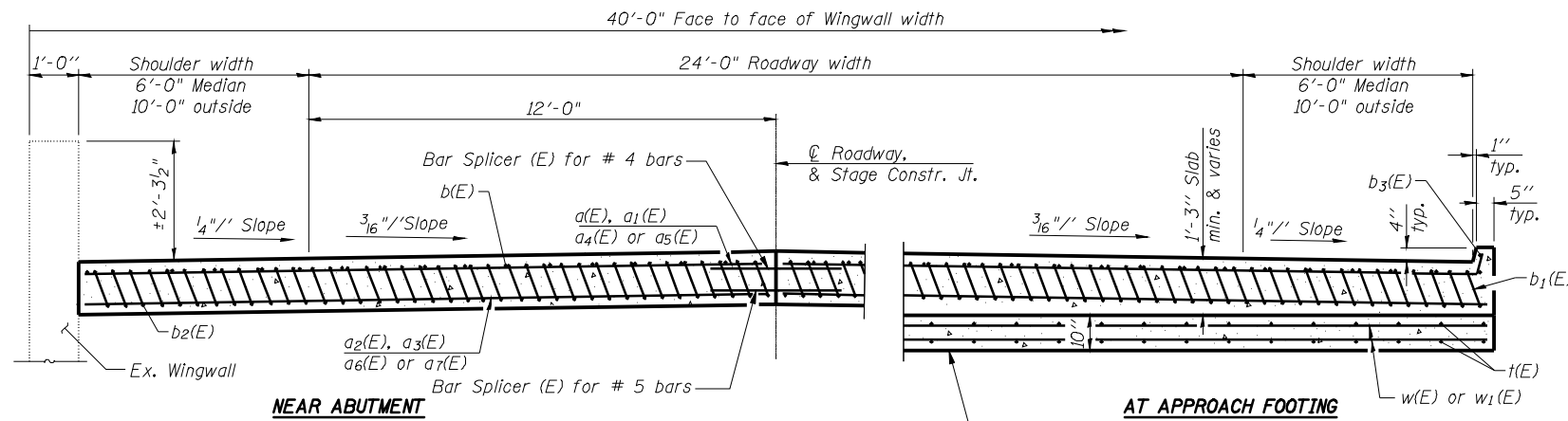
SHEET NO. 5 OF 7 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	72	(84-10-2) RS-3, (84-10-3) RS-4	SANGAMON	61	57
FED. ROAD DIST. NO. 2			ILLINOIS FED. AID PROJECT		
CONTRACT NO. 72910					

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

Notes:
See sheet 5 of 7 for Detail A and View B-B.
Approach slab and parapet concrete shall be paid for as Concrete Superstructure.
Approach footing concrete shall be paid for as Concrete Structures.
Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.
The approach footing maximum applied service bearing pressure (Q_{max}) = 2.0 ksf.
For bar splicer details, see sheet 7 of 7.
Cost of excavation for approach footing included with Concrete Structures.



SECTION C-C

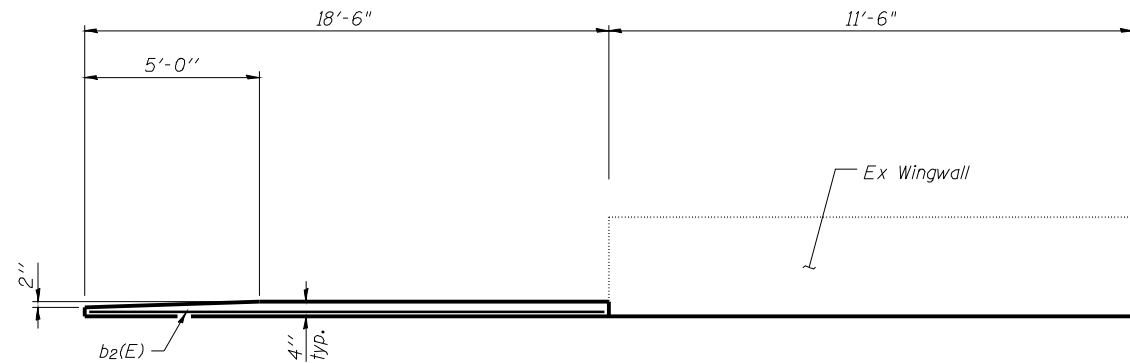


SECTION D-D

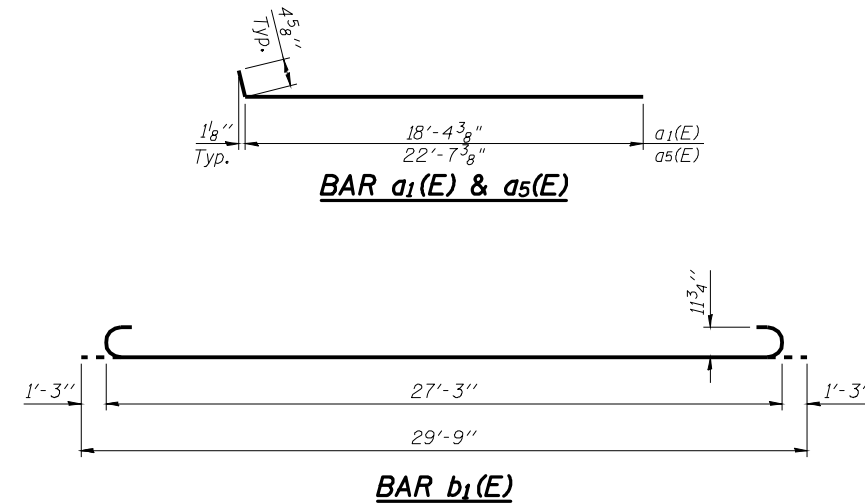
(See Plan for dimensions not shown)

W. Appr. W.B. Elev. 586.40
W. Appr. E.B. Elev. 586.81
E. Appr. W.B. Elev. 588.52
E. Appr. E.B. Elev. 588.40
(Level out to out)

* Tilt #9 b₁(E) bars as required to maintain clearance.
*** Cost included with Concrete Superstructure.



VIEW E-E



FOUR APPROACHES
BILL OF MATERIAL

Bar	No.	Size	Length	Shape	
a ₁ (E)	40	#4	18'-0"	—	
a ₁ (E)	60	#4	18'-9"	—	
a ₂ (E)	72	#5	18'-0"	—	
a ₃ (E)	112	#5	18'-6"	—	
a ₄ (E)	40	#4	22'-3"	—	
a ₅ (E)	60	#4	23'-0"	—	
a ₆ (E)	72	#5	22'-3"	—	
a ₇ (E)	112	#5	22'-10"	—	
b(E)	132	#4	29'-8"	—	
b ₁ (E)	372	#9	29'-9"	—	
b ₂ (E)	8	#4	18'-0"	—	
t(E)	336	#4	10'-4"	—	
w(E)	160	#5	18'-6"	—	
w ₁ (E)	160	#5	22'-10"	—	
Concrete Superstructure				Cu. Yd.	246.4
Concrete Structures				Cu. Yd.	57.1
Reinforcement Bars, Epoxy Coated				Pound	60,160

BRIDGE APPROACH SLAB DETAILS
S.N. 084-0157 (W.B.) & 084-0158 (E.B.)
(Sheet 2 of 2)

DESIGNED	SCD
CHECKED	DRB
DRAWN	THW
CHECKED	-

BA-L

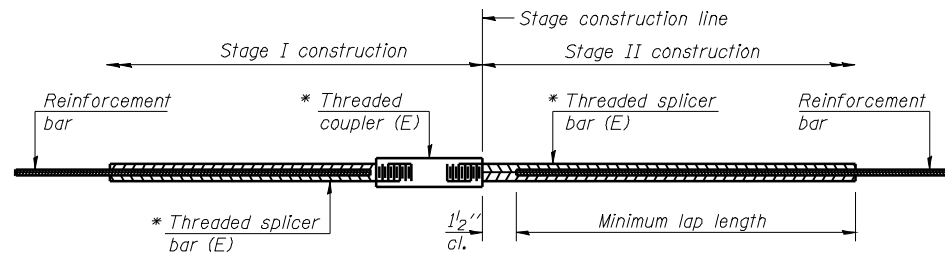
11-1-09



IE CONSULTANTS, INC
6420 SOUTH SIXTH STREET
SPRINGFIELD, ILLINOIS 62712
TEL. (217) 529-8027
FAX (217) 529-4543
IESPRINGFIELD@IE-CONSULTANTS.COM
WWW.IE-CONSULTANTS.COM

SHEET NO. 6 OF 7 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	72	(84-10-2) RS-3, (84-10-3) RS-4	SANGAMON	61	58
CONTRACT NO. 72910					
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



STANDARD BAR SPLICER ASSEMBLY

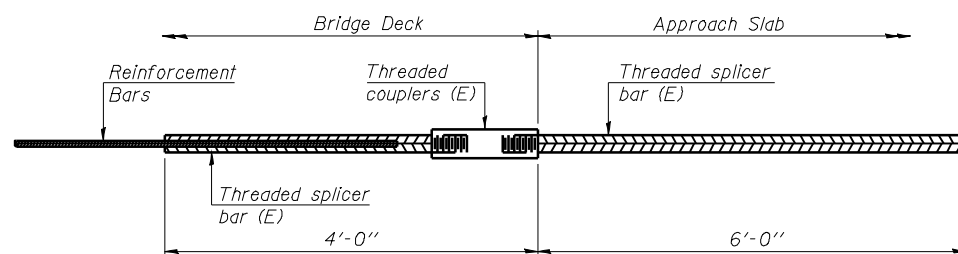
Minimum Lap Lengths				
Bar size to be spliced	Table 1	Table 2	Table 3	Table 4
3, 4	1'-5"	1'-11"	2'-1"	2'-4"
5	1'-9"	2'-5"	2'-7"	2'-11"
6	2'-1"	2'-11"	3'-1"	3'-6"
7	2'-9"	3'-10"	4'-2"	4'-8"
8	3'-8"	5'-1"	5'-5"	6'-2"
9	4'-7"	6'-5"	6'-10"	7'-9"

Table 1: Black bar, 0.8 Class C
Table 2: Black bar, Top bar lap, 0.8 Class C
Table 3: Epoxy bar, 0.8 Class C
Table 4: Epoxy bar, Top bar lap, 0.8 Class C

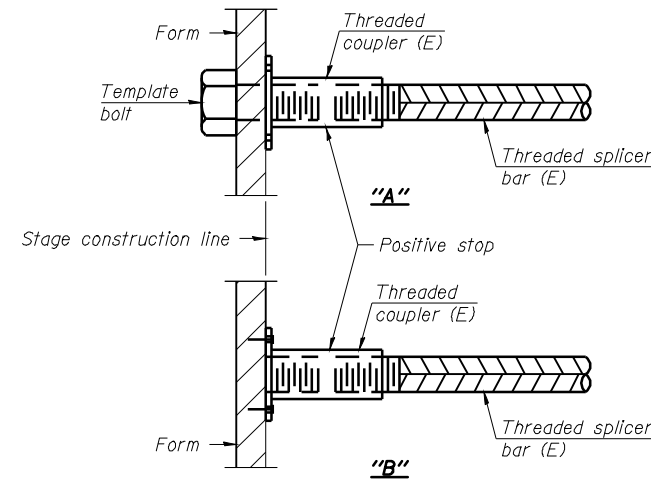
Threaded splicer bar length = min. lap length + 1/2" + thread length

* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length
W.B. West Abut.	#5	12	Table 4
W.B. East Abut.	#5	12	Table 4
E.B. West Abut.	#5	12	Table 4
E.B. East Abut.	#5	12	Table 4
W. Appr. W.B.	#4	25	Table 4
W. Appr. E.B.	#4	25	Table 4
E. Appr. W.B.	#4	25	Table 4
E. Appr. E.B.	#4	25	Table 4
W. Appr. W.B.	#5	86	Table 4
W. Appr. E.B.	#5	86	Table 4
E. Appr. W.B.	#5	86	Table 4
E. Appr. E.B.	#5	86	Table 4
Total		492	

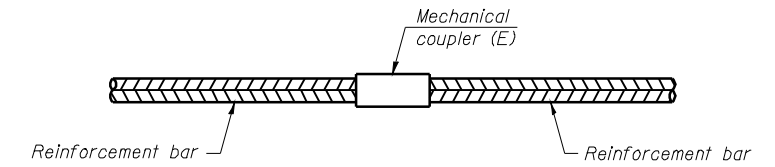


BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS



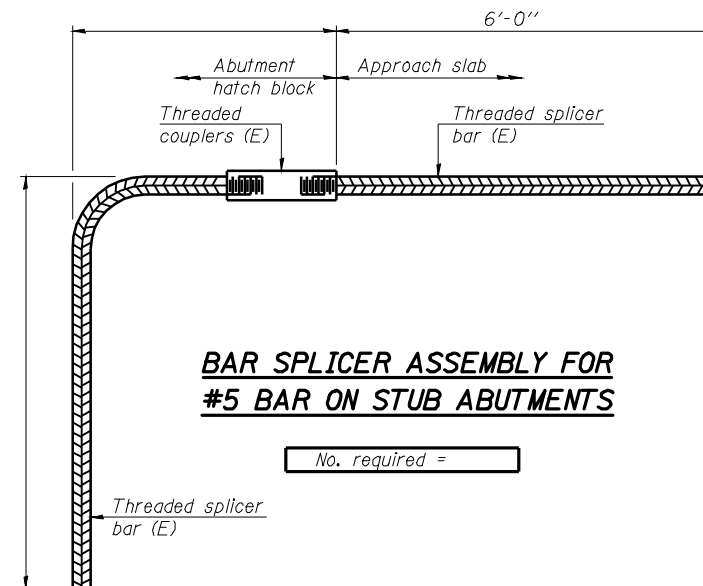
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required
Total		



NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
All reinforcement shall be lapped and tied to the splicer bars.
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
See special provision for Mechanical Splicers.
See approved list of bar splicer assemblies and mechanical splicers for alternatives.

**BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
S.N. 084-01557 (W.B.) & 084-0158 (E.B.)**

DESIGNED	SCD
CHECKED	DRB
DRAWN	THW
CHECKED	-



No. required =

IE CONSULTANTS, INC
6420 SOUTH SIXTH STREET
SPRINGFIELD, ILLINOIS 62712
TEL. (217) 529-8027
FAX (217) 529-4543
IESPRINGFIELD@IE-CONSULTANTS.COM
WWW.IE-CONSULTANTS.COM

BSD-1

11-1-09

SHEET NO.	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7 OF 7 SHEETS	72	(84-10-2) RS-3, (84-10-3) RS-4	SANGAMON	61	59
			CONTRACT NO. 72910		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT					

EXISTING STRUCTURE:

The existing bridge is a single structure constructed on a tangent horizontal alignment and a vertically curved alignment. The bridge is 246.0' back to back of abutments with an out to out width of 32.0'. Clear width is 28.5'. The span lengths from south to north are 105.5' & 105.5'. The bridge is constructed on a 0.0 degree skew.

The superstructure consists of two span continuous welded steel box girders and a 7.5" reinforced concrete deck with a 1.50" bituminous overlay.

The substructure consists of one trapezoidal type reinforced concrete pier, which rests on a concrete pile-supported footing and reinforced concrete vaulted abutments on concrete piles

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions

Reinforcement bars designated (E) shall be epoxy coated.

Plan dimensions and details relative to existing plans are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished based upon the unit price bid for the work.

PROPOSED WORK

1. Retrofit concrete parapet. Typical at abutment wingwalls.
2. Modify existing handrail to butt into retrofit parapet.

DESIGN SPECIFICATIONS

2002 AASHTO Standard Specifications - 17th ed.

LOADING HS20-44

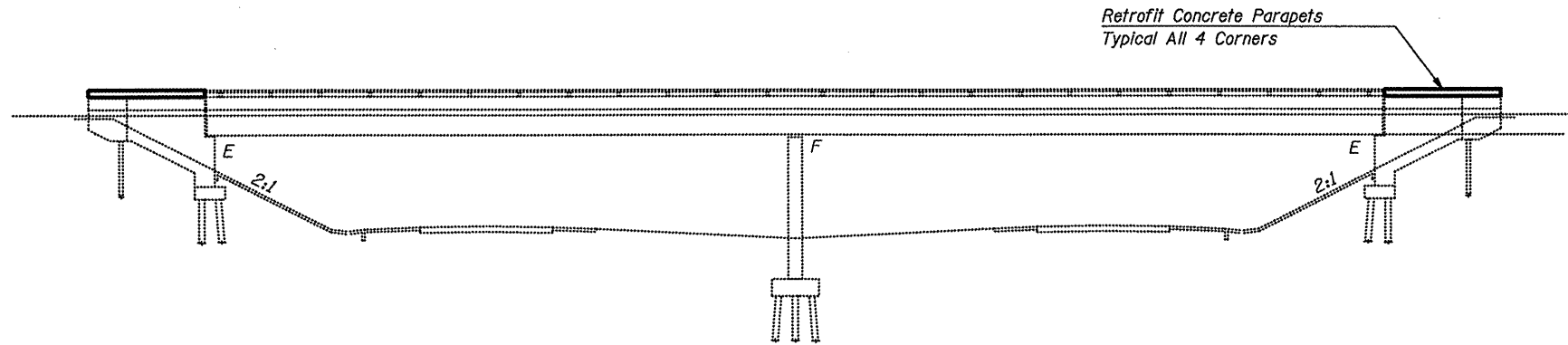
DESIGN STRESSES

Cast-in-Place Unit
f'c = 3500 psi
f's = 60,000 psi
n = 9

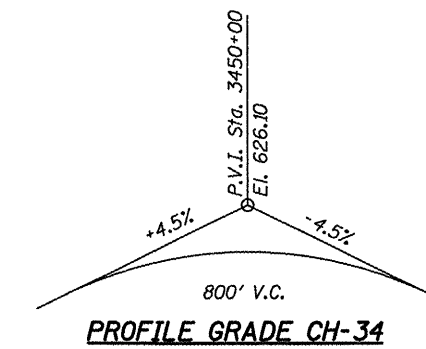
INDEX OF SHEETS

1. General Plan & Elevation
2. Retrofit Concrete Parapet Details

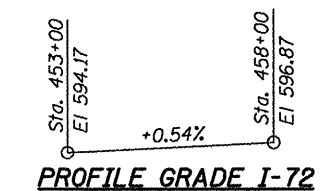
GENERAL PLAN & ELEVATION
C.H. 34 OVER F.A.I. RTE. 72
SEC. (84-10-3) RS-4
SANGAMON COUNTY
STATION 455+92.17
S.N. 084-0159



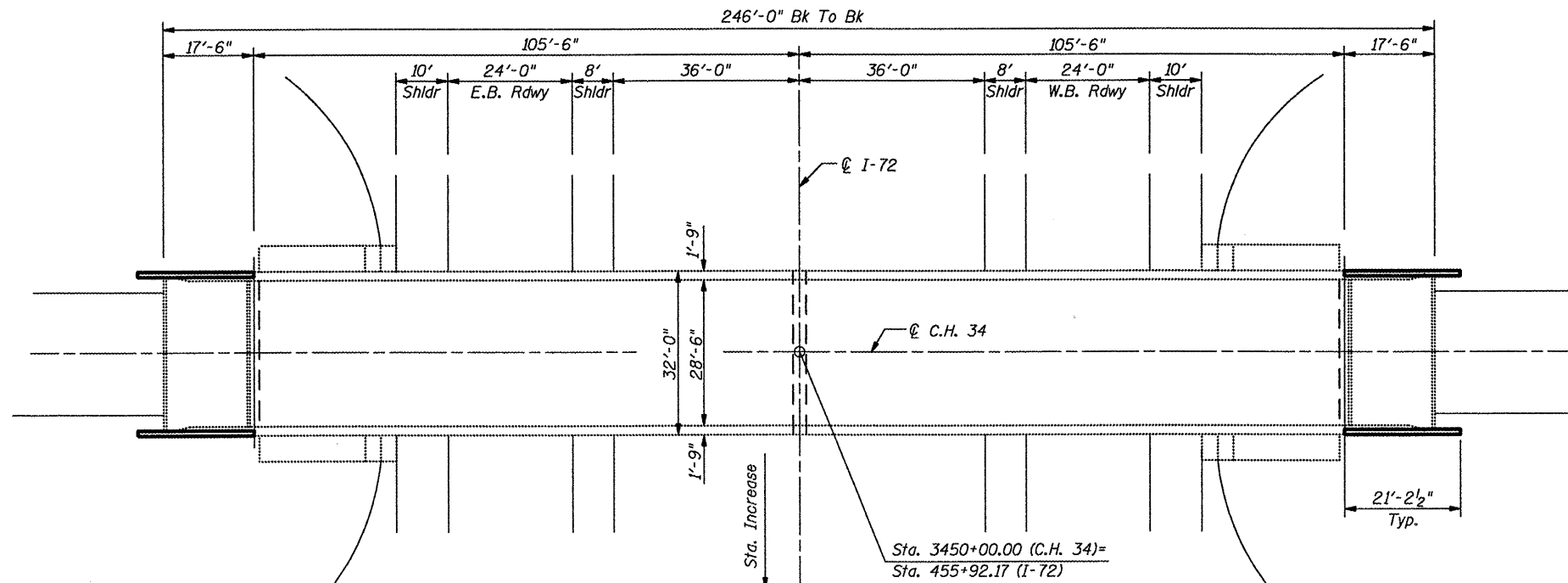
ELEVATION



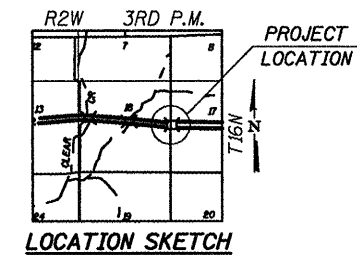
PROFILE GRADE CH-34



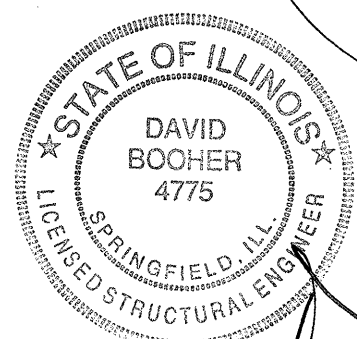
PROFILE GRADE I-72



PLAN



LOCATION SKETCH



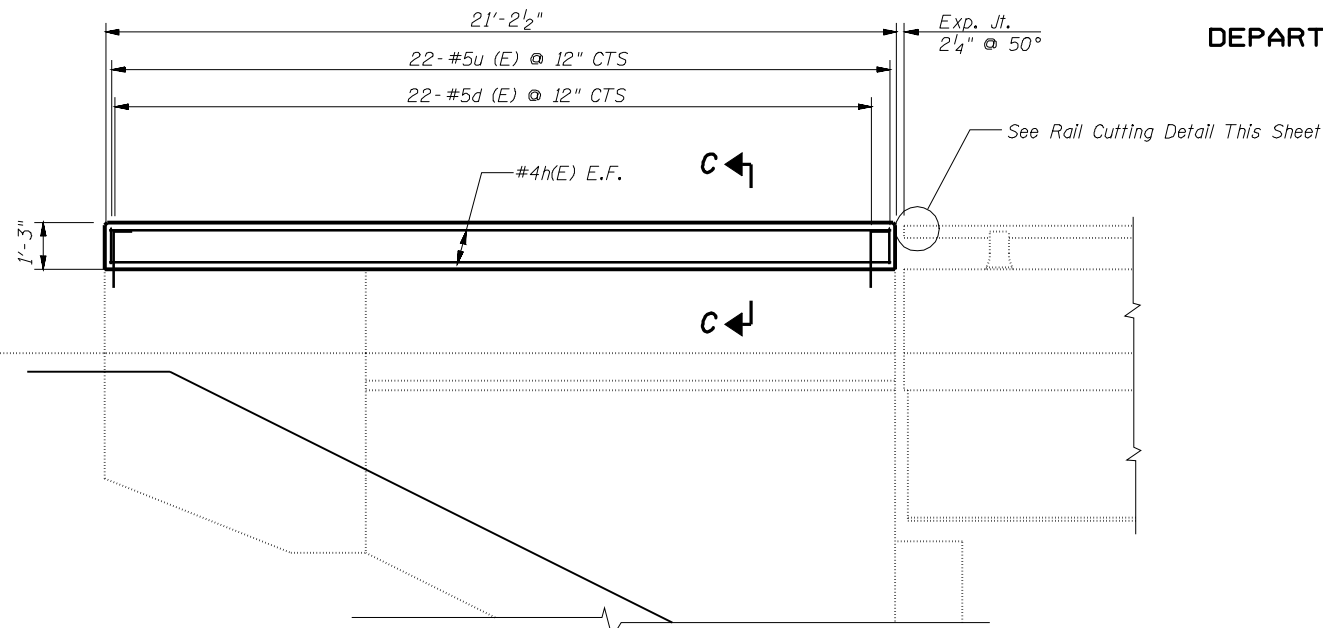
David Booher, Illinois S.E. 081-004775 Date **2-19-2010**
Expires 11-30-2010

DESIGNED	SCD
CHECKED	DRB
DRAWN	THW
CHECKED	-

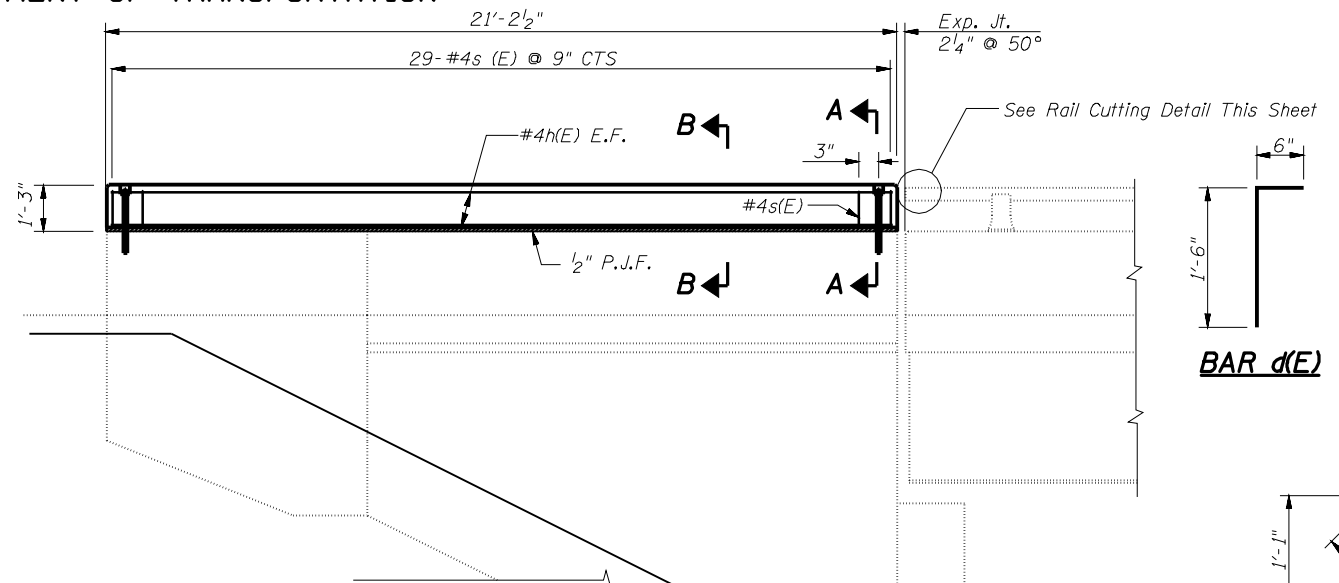
ie consultants
IE CONSULTANTS, INC
6420 SOUTH SIXTH STREET
SPRINGFIELD, ILLINOIS 62712
TEL. (217) 529-8027
FAX (217) 529-4543
IESPRINGFIELD@IE-CONSULTANTS.COM
WWW.IE-CONSULTANTS.COM

SHEET NO.	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1 OF 2 SHEETS	72	(84-10-2) RS-3, (84-10-3) RS-4	SANGAMON	61	60
			CONTRACT NO. 72910		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT					

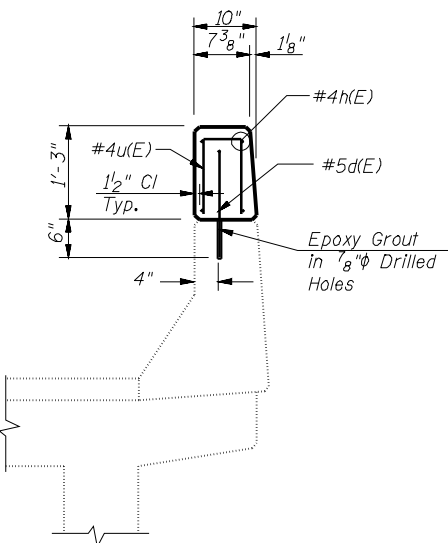
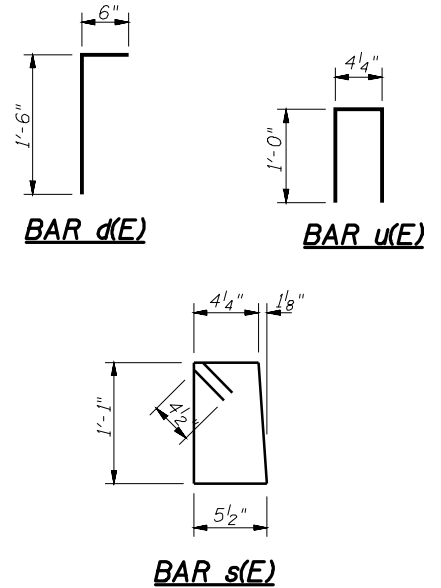
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



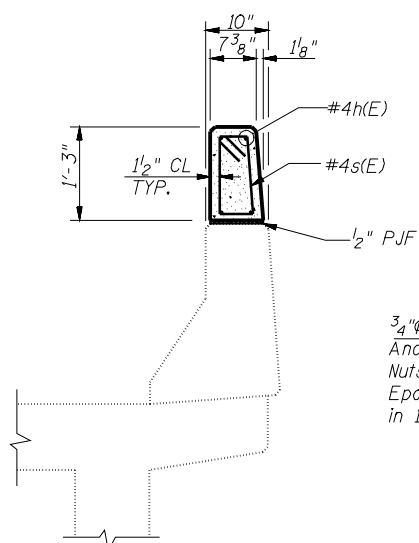
**PARAPET ELEVATION
CAST-IN-PLACE ALTERNATE**
(TYPICAL ALL 4 CORNERS)



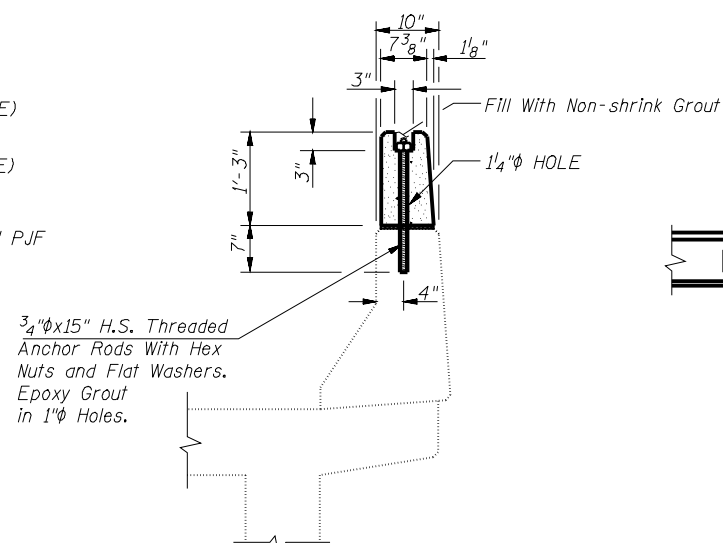
**PARAPET ELEVATION
PRECAST ALTERNATE**
(TYPICAL ALL 4 CORNERS)



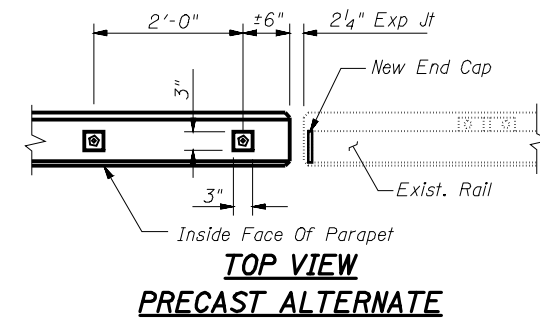
SECTION C-C



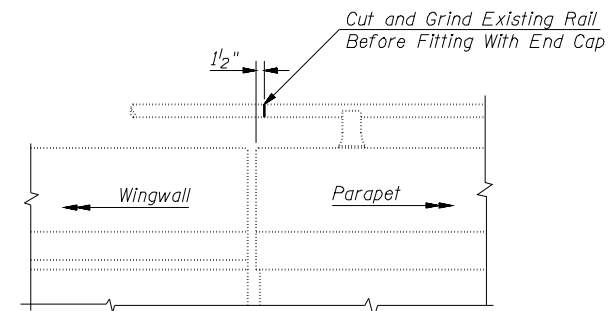
**SECTION B-B
(SHOWING REINFORCEMENT)**



**SECTION A-A
(SHOWING ATTACHMENT)**



**TOP VIEW
PRECAST ALTERNATE**



**RAIL CUTTING DETAIL
(TYPICAL ALL CORNERS)**

**CAST ALUMINUM END CAP
(4 REQUIRED)**

IE CONSULTANTS, INC.
6420 SOUTH SIXTH STREET
SPRINGFIELD, ILLINOIS 62712
TEL. (217) 529-8027
FAX (217) 529-4543
IESPRINGFIELD@IE-CONSULTANTS.COM
WWW.IE-CONSULTANTS.COM



DESIGNED	SCD
CHECKED	DRB
DRAWN	THW
CHECKED	-

**SECTION THRU EXISTING
ELLIPTICAL RAIL**

BILL OF MATERIALS

ITEM	UNIT	QUANTITY
RETROFIT CONCRETE PARAPET	FOOT	85
HANDRAIL MODIFICATION	L SUM	1

**RETROFIT CONCRETE PARAPET DETAILS
STRUCTURE NO. 084-0159**

SHEET NO.	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2 OF 2 SHEETS	72	(84-10-2) RS-3, (84-10-3) RS-4	SANGAMON	61	61
CONTRACT NO. 72910					
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT					

GENERAL NOTES

Class PC Concrete shall be used for the Precast Alternate and Class BS Concrete shall be used for the Cast-In Place Alternate.

All Exposed edges shall be Chamfered 3/4".

Class PC Concrete, Class BS Concrete, Reinforcement Bars (E), Threaded Anchor Rods, 1/2" P.J.F. and all other accessories needed for installation and Erection will be paid for at the Unit Price Per Lineal Foot for Retrofit Concrete Parapet.

Threaded Anchor Rods, Dowel Bars, Adhesive & other accessories needed for installation will be paid for at the Unit Price per each for Drill and Grout Dowel Bars.

The Contractor shall use the Capsule or the Adhesive Cartridge Type Anchor Rods that have been previously tested and given a Prior Approval by the Department. The Contractor shall install these Anchor Rods in Pre-Drilled Holes in accordance with the Manufacturer's Recommendations and Procedures.

The Capsule or the Adhesive Cartridge shall be a Sealed Glass Capsule or a Sealed Glass Adhesive Cartridge containing a premeasured amount of Adhesive Chemical. The Threaded Rod Stud with Nut and Washers shall conform to ASTM A-307.

The existing 3/4" and 1" Weld Studs shall be Burned, Sawed or Chipped Off Flush with the Top of Existing Parapet.

Holes for the Dowel Rods shall miss Existing Parapet Joints.