

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

**PROPOSED
 HIGHWAY PLANS**

F.A.P. ROUTE 783 (IL. RTE. 33)
 SECTION (51-24-2,1-2)RS-1 & (1-X-1)RS-3

RESURFACING (3P)
 LAWRENCE COUNTY
 C-97-128-09

FOR INDEX OF SHEETS, SEE SHEET NO. 2

STATION EQUATION:

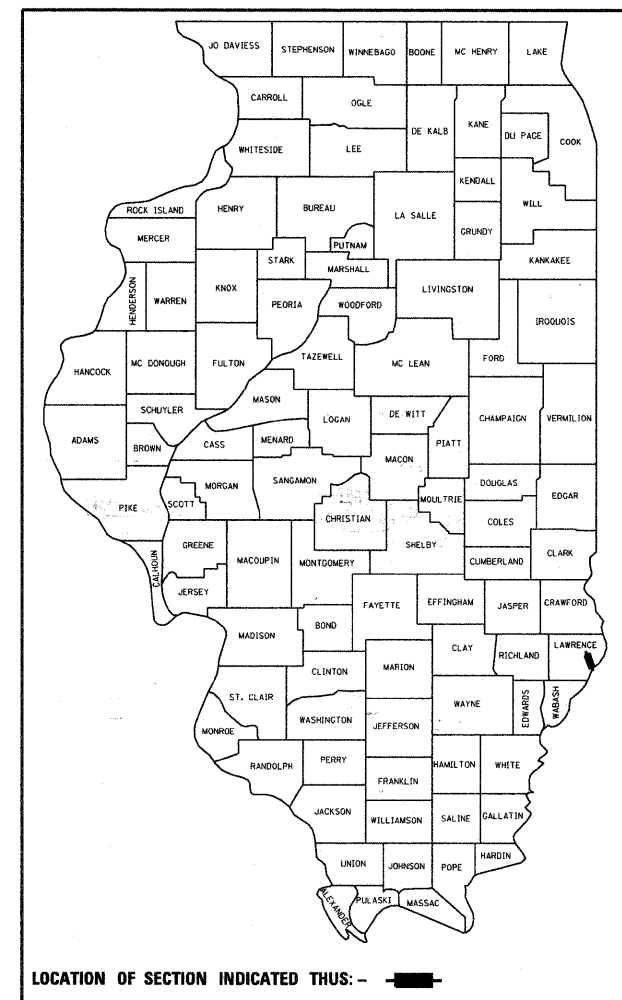
STA. 427+11.90 (AH) = STA. 25+70.10 (BK)
 STA. 2+00.00 (AH) = STA. 376+31.00 (BK)

ADT = 5,200 (2009)

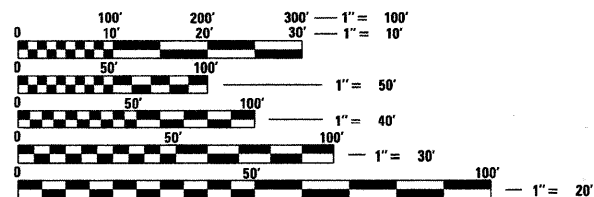
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
783		LAWRENCE	42	1
		ILLINOIS	CONTRACT NO. 74414	

• (51-24-2,1-2)RS-1 & (1-X-1)RS-3

D-97-072-09



F.A.P. ROUTE 783 (IL. RTE. 33)
 SECTION (51-24-2,1-2)RS-1 & (1-X-1)RS-3
 LAWRENCE COUNTY
 ENDS STATION 41+40.94

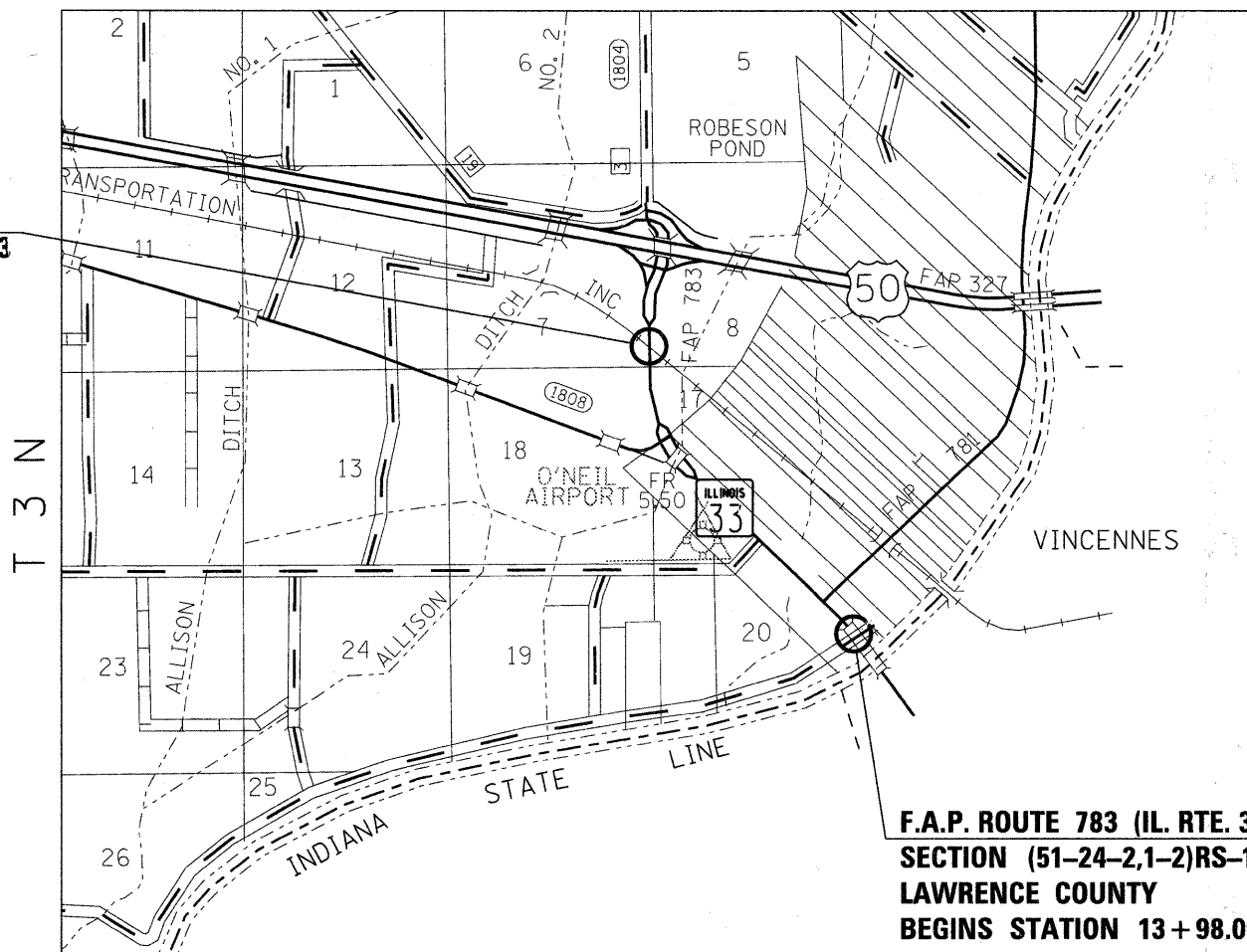


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123
 OR 811

PROJECT ENGINEER: TOM RONAN
 PROJECT MANAGER: LUKE WASSON

CONTRACT NO. 74414



F.A.P. ROUTE 783 (IL. RTE. 33)
 SECTION (51-24-2,1-2)RS-1 & (1-X-1)RS-3
 LAWRENCE COUNTY
 BEGINS STATION 13+98.00

R 10 W

GROSS LENGTH = 9,943 FT. = 1.88 MILE
 NET LENGTH = 9,943 FT. = 1.88 MILE

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

SUBMITTED January 29, 2010

Regan L. Seibel
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

March 19, 2010

Scott E. Stitt, P.E.
 ACTING ENGINEER OF DESIGN AND ENVIRONMENT

March 19, 2010

Christine M. Reed
 DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
 OF THE STATE OF ILLINOIS

INDEX OF SHEETS

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27-30	PAVEMENT MARKING DETAILS
31-41	STRUCTURAL DETAILS
42	DISTRICT DETAILS

THE FOLLOWING STANDARDS ARE A PART OF THESE PLANS AND ARE INCLUDED AFTER SHEET NO. 42:

STD. NO.	DESCRIPTION
000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
406201-01	MAILBOX TURNOUT
442201-03	CLASS C AND D PATCHES
701006-03	OFF-RD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701011-02	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701301-03	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-02	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS >= 45 MPH
701311-03	LANE CLOSURE 2L, 2W, MOVING OPERATIONS - DAY ONLY
701321-10	LANE CLOSURE 2L, 2W, BRIDGE REPAIR WITH BARRIER
701326-03	LANE CLOSURE, 2L, 2W, PAVEMENT WIDENING, FOR SPEEDS >= 45 MPH
701336-05	LANE CLOSURE, 2L, 2W, WORK AREA IN SERIES, FOR SPEEDS >= 45 MPH
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS
781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS

GENERAL NOTES

THIS SECTION SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE PLANS; THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2007; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2010, AND THE SPECIAL PROVISIONS INCLUDED IN THIS PROPOSAL.

THE WORK INCLUDED IN SECTION (51-24-2,1-2)RS-1 & (1-X-1)RS-3 CONSISTS OF PAVEMENT PATCHING, MILLING, OVERLAYING THE PAVEMENT WITH HOT-MIX ASPHALT, CURB REMOVAL, PCC BASE COURSE, AGGREGATE SHOULDERS, PAVEMENT MARKING, B-SMART BRIDGE REPAIRS, AND ANY OTHER WORK NECESSARY TO COMPLETE THIS SECTION LOCATED ON ILLINOIS ROUTE 33 IN LAWRENCE COUNTY FROM 0.75 MILES SOUTH OF US ROUTE 50 TO THE INDIANA STATE LINE.

PAVEMENT MARKING SHALL BE APPLIED IN ACCORDANCE WITH SECTION 780 OF THE STANDARD SPECIFICATIONS. SHORT TERM PAVEMENT MARKING SHALL BE APPLIED TO THE MILLED SURFACE, BITUMINOUS MATERIALS (PRIME COAT), HOT-MIX ASPHALT LEVELING BINDER, AND HOT-MIX ASPHALT SURFACE COURSE AS SPECIFIED IN SECTION 703 OF THE STANDARD SPECIFICATIONS. TEMPORARY TAPE SHALL BE USED ON THE SURFACE COURSE AND PAINT SHALL BE USED ON MILLED SURFACES.

THE TOTAL QUANTITY OF PAINT PAVEMENT MARKING - LINE 4" CONSISTS OF 2,181 FEET OF YELLOW AND 12,822 FEET OF WHITE.

THE TOTAL QUANTITY OF THERMOPLASTIC PAVEMENT MARKING - LETTERS & SYMBOLS CONSISTS OF 125 SQUARE FEET OF WHITE.

THE TOTAL QUANTITY OF THERMOPLASTIC PAVEMENT MARKING - LINE 4" CONSISTS OF 14,730 FEET OF YELLOW AND 9,144 FEET OF WHITE.

THE TOTAL QUANTITY OF THERMOPLASTIC PAVEMENT MARKING - LINE 6" CONSISTS OF 693 FEET OF WHITE.

THE TOTAL QUANTITY OF THERMOPLASTIC PAVEMENT MARKING - LINE 12" CONSISTS OF 1,143 FEET OF YELLOW AND 547 FEET OF WHITE.

THE EXISTING PAVEMENT SHALL BE PATCHED IN ACCORDANCE WITH SECTION 442 OF THE STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS. THE QUANTITY OF PATCHING SHOWN IN THE PLANS IS AN ESTIMATE ONLY. THE FINAL LOCATIONS AND QUANTITY SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 781 OF THE STANDARD SPECIFICATIONS. THE TOTAL QUANTITY OF RAISED REFLECTIVE PAVEMENT MARKERS CONSISTS OF 14 ONE-WAY CRYSTAL MARKERS, 162 ONE-WAY AMBER MARKERS, 156 TWO-WAY AMBER MARKERS, AND 3 TWO-WAY AMBER MARKERS (BRIDGE).

THE CONTRACTOR SHALL PROVIDE INTERNET ACCESSIBILITY TO THE HOT-MIX ASPHALT PLANT QUALITY CONTROL LAB SO THAT HOT-MIX ASPHALT PLANT REPORTS CAN BE E-MAILED TO THE DISTRICT HEADQUARTERS. THIS WORK SHALL BE INCLUDED IN THE COST OF ALL HOT-MIX ASPHALT PAY ITEMS.

THE CONTRACTOR SHALL USE EITHER RC-70 SSIH OR AN EMULSIFIED POLYMER PRIME SS-1HP FOR THE PAY ITEM BITUMINOUS MATERIALS (PRIME COAT).

THE RESIDENT ENGINEER SHALL BE THE SOLE JUDGE CONCERNING THE CURING TIME FOR ALL HOT-MIX ASPHALT.

THE MATERIAL USED FOR AGGREGATE SHOULDERS, TYPE B AND AGGREGATE SURFACE COURSE, TYPE B SHALL BE CRUSHED STONE, CRUSHED CONCRETE, OR RAP.

THE PAY ITEM TEMPORARY RAMP HAS BEEN INCLUDED FOR THE CONSTRUCTION OF TEMPORARY RAMPS IN ACCORDANCE WITH ARTICLE 406.08 OF THE STANDARD SPECIFICATIONS. THE CONTRACT UNIT PRICE PER SQUARE YARD FOR TEMPORARY RAMP SHALL INCLUDE BOTH THE INSTALLATION AND THE REMOVAL OF THE RAMPS.

THE BASE COURSE WIDENING SHALL, AT THE CONTRACTOR'S OPTION, BE CONSTRUCTED OF EITHER PORTLAND CEMENT CONCRETE, 8" THICK, OR HOT-MIX ASPHALT, 10" THICK. ANY EXCAVATION AND PAVED SHOULDER REMOVAL REQUIRED FOR PLACEMENT OF THE BASE COURSE WIDENING SHALL BE INCLUDED IN THE COST OF THE BASE COURSE WIDENING. THE BASE COURSE WIDENING SHALL REMAIN IN PLACE AT THE COMPLETION OF THE PROJECT.

GENERAL NOTES (Cont'd)

THE BASE COURSE SHALL BE CONSTRUCTED OF PORTLAND CEMENT CONCRETE, 13" THICK. ANY EXCAVATION BETWEEN THE CURB REMOVAL REQUIRED FOR PLACEMENT OF THE BASE COURSE SHALL BE INCLUDED IN THE COST OF THE BASE COURSE.

THE PAY ITEM CURB REMOVAL HAS BEEN INCLUDED FOR THE REMOVAL OF EXISTING PORTLAND CEMENT CONCRETE CURB, INLETS, AND PORTLAND CEMENT CONCRETE MEDIAN NOSE IN ACCORDANCE WITH ARTICLE 440 OF THE STANDARD SPECIFICATIONS.

IN AREAS OF HOT-MIX ASPHALT REMOVAL, 2 1/4", WHERE DROP-OFFS OF ≥ 3" EXIST BETWEEN THE PAVEMENT REMOVAL AND EXISTING SHOULDERS, THE CONTRACTOR SHALL PROVIDE TYPE 1 OR TYPE II BARRICADES IN ORDER TO PROTECT THE DROP-OFF IN ACCORDANCE WITH ARTICLE 701.07 OF THE STANDARD SPECIFICATIONS. THE COST OF THE BARRICADES SHALL BE INCLUDED IN THE COST OF HOT-MIX ASPHALT REMOVAL, 2 1/4".

WHERE PROPOSED CONSTRUCTION ABUTS EXISTING APPURTENANCES, A SAW CUT SHALL BE MADE TO ACHIEVE A NEAT BUTT JOINT. THE SAW CUT IS TO BE INCLUDED IN THE COST OF THE BUTT JOINT.

THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT:

SURFACE COURSE (1 1/2")

APPLICATION:	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70
PG GRADE:	PG 64-22
DESIGN AIR VOIDS:	4.0% @ DESIGN = 70
MIXTURE COMPOSITION:	IL-9.5
FRICTION AGGREGATE:	MIXTURE C

LEVEL BINDER (3/4")

APPLICATION:	LEVELING BINDER (MACHINE METHOD) N70
PG GRADE:	PG 64-22
DESIGN AIR VOIDS:	4.0% @ DESIGN = 70
MIXTURE COMPOSITION:	IL-9.5
FRICTION AGGREGATE:	N/A

INCIDENTAL HMA

APPLICATION:	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70
PG GRADE:	PG 64-22
DESIGN AIR VOIDS:	4.0% @ DESIGN = 70
MIXTURE COMPOSITION:	IL-9.5
FRICTION AGGREGATE:	MIXTURE C

PAVEMENT PATCHING & BASE COURSE WIDENING

APPLICATION:	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70
PG GRADE:	PG 64-22
DESIGN AIR VOIDS:	4.0% @ DESIGN = 70
MIXTURE COMPOSITION:	IL-19.0
FRICTION AGGREGATE:	N/A

THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN CALCULATING PLAN QUANTITIES:

HOT MIX ASPHALT	112 LBS/SQ YD/IN
BITUMINOUS MATERIALS PRIME COAT	0.10 GALLONS/SQ YD
AGGREGATE PRIME COAT	2.00 LBS/SQ YD
GRANULAR MATERIAL	2.05 TONS/CU YD

FILE NAME =	USER NAME = swartzw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS GENERAL NOTES		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -			SCALE: NA SHEET NO. 1 OF 1 SHEETS STA. TO STA.		CONTRACT NO. 74414				
PLOT DATE = 1/29/2010	DATE -	REVISED -					ILLINOIS FED. AID PROJECT				

• (51-24-2,1-2)RS-1 & (1-X-1)RS-3

SUMMARY OF QUANTITIES			100% STATE TOTAL QUANTITIES	CONSTRUCTION TYPE CODE	
CODE NO	ITEM	UNIT		RURAL 1000	SN 051-0032 SFTY-2A
35300720	PORTLAND CEMENT CONCRETE BASE COURSE 13"	SQ YD	1624	1624	
35650700	BASE COURSE WIDENING	SQ YD	125	125	
40200800	AGGREGATE SURFACE COURSE, TYPE B	TON	199	199	
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	7128	7128	
40600300	AGGREGATE (PRIME COAT)	TON	143	143	
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	1456	1456	
40600895	CONSTRUCTING TEST STRIP	EACH	1	1	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	93	93	
40600990	TEMPORARY RAMP	SQ YD	121	121	
40603315	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70	TON	2920	2920	
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	295	295	
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	13273	13273	
44000198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	21396	21396	
44000300	CURB REMOVAL	FOOT	3190	3190	
44200152	PAVEMENT PATCHING, TYPE I, 13 INCH	SQ YD	18	18	
44200156	PAVEMENT PATCHING, TYPE II, 13 INCH	SQ YD	650	650	
44200160	PAVEMENT PATCHING, TYPE III, 13 INCH	SQ YD	40	40	
48101200	AGGREGATE SHOULDERS, TYPE B	TON	621	621	
50102400	CONCRETE REMOVAL	CU YD	18.1	18.1	
50157300	PROTECTIVE SHIELD	SQ YD	267	267	
50300255	CONCRETE SUPERSTRUCTURE	CU YD	20	20	
50300260	BRIDGE DECK GROOVING	SQ YD	642	642	
50300300	PROTECTIVE COAT	SQ YD	712	712	
50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	1680	1680	
50500715	JACK AND REMOVE EXISTING BEARINGS	EACH	12	12	
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	1310	1310	
50800515	BAR SPLICERS	EACH	24	24	
52000110	PREFORMED JOINT STRIP SEAL	FOOT	92	92	
52100010	ELASTOMERIC BEARING ASSEMBLY, TYPE I	EACH	6	6	
52100020	ELASTOMERIC BEARING ASSEMBLY, TYPE II	EACH	6	6	
52100520	ANCHOR BOLTS, 1"	EACH	24	24	
60500305	FILLING INLETS	EACH	3	3	
67000500	ENGINEER'S FIELD OFFICE, TYPE B	CAL MO	5	5	
67100100	MOBILIZATION	L SUM	1	1	
70100405	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321	EACH	1	1	

SUMMARY OF QUANTITIES (Cont'd)			100% STATE TOTAL QUANTITIES	CONSTRUCTION TYPE CODE	
CODE NO	ITEM	UNIT		RURAL 1000	SN 051-0032 SFTY-2A
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1	
70100500	TRAFFIC CONTROL AND PROTECTION, STANDARD 701326	L SUM	1	1	
70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	L SUM	1	1	
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	15	15	
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1	1	
70106700	TEMPORARY RUMBLE STRIP	EACH	6	6	
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	18294	18294	
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	125	125	
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	32488	32488	
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	693	693	
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1690	1690	
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	2033	2033	
70400100	TEMPORARY CONCRETE BARRIER	FOOT	444	444	
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	444	444	
*78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	125	125	
*78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	23874	23874	
*78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	693	693	
*78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1690	1690	
*78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	15003	15003	
*78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	332	332	
*78100105	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	EACH	3		3
X0325305	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	47		47
XZ193500	BRIDGE DECK MICROSILICA CONCRETE OVERLAY 2 1/4"	SQ YD	685		685
Z0006204	BRIDGE DECK HYDRO-SCARIFICATION 1/2"	SQ YD	685		685
Z0016001	DECK SLAB REPAIR (FULL DEPTH, TYPE I)	SQ YD	3		3
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	8		8
Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2		2
Z0030350	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2		2
E0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1		1

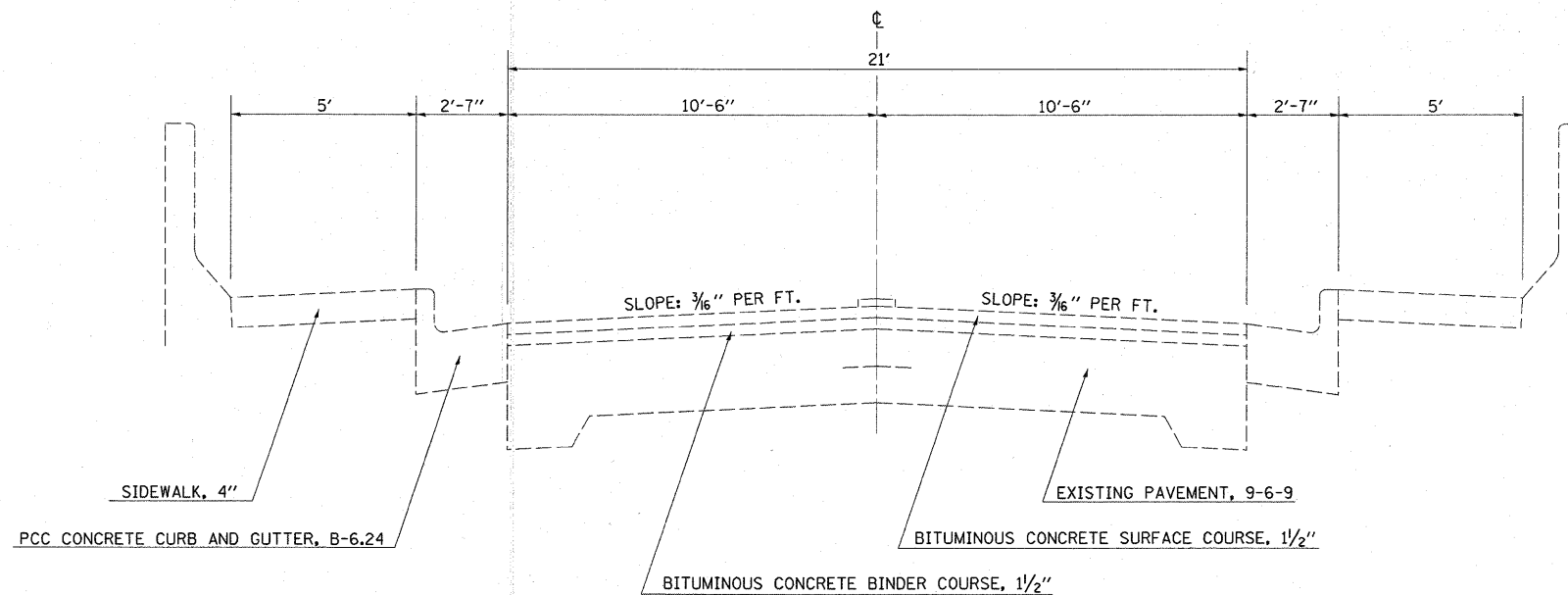
* Specialty Items

• (51-24-2,1-2)RS-1 & (1-X-1)RS-3

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	PLOT SCALE = #SCALE#	CHECKED -	REVISED -		CONTRACT NO. 74414								
	PLOT DATE = #DATE#	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								

EXISTING TYPICAL CROSS SECTION

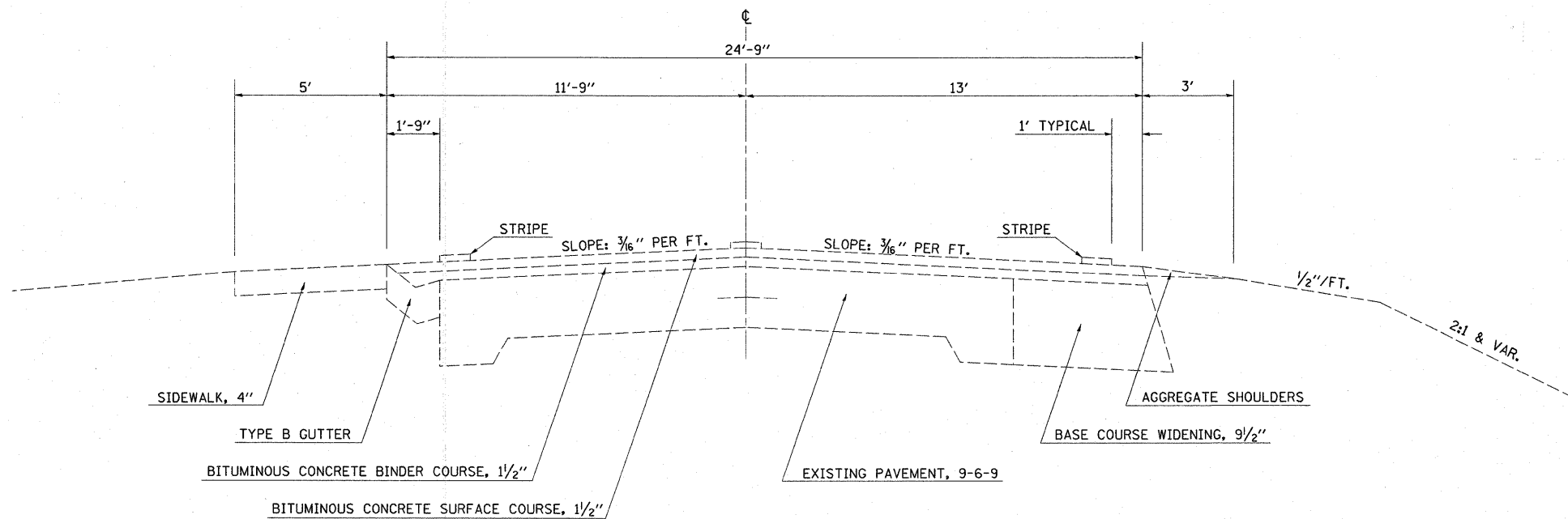
STA. 13+98.00 TO STA. 16+11.00



NOTE: TYPICALS NOT DRAWN TO SCALE

EXISTING TYPICAL CROSS SECTION

STA. 16+11.00 TO STA. 22+50.00



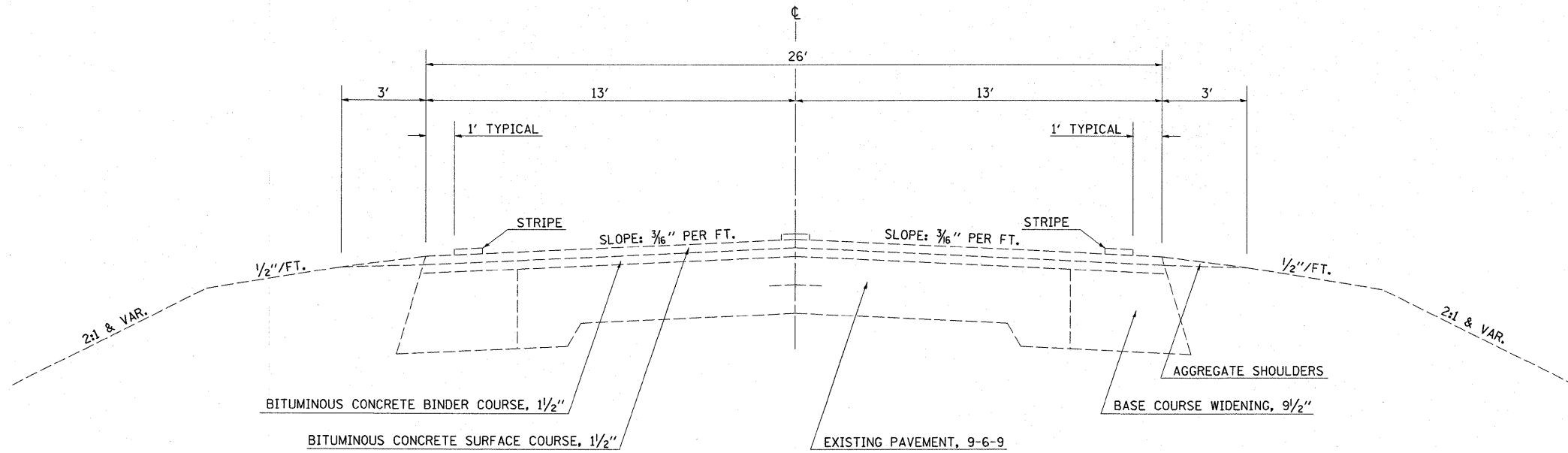
NOTE: TYPICALS NOT DRAWN TO SCALE

• (51-24-2.1-2)RS-1 & (1-X-1)RS-3

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	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -										
	PLOT DATE = 1/29/2010	DATE -	REVISED -										
CONTRACT NO. 74414													
ILLINOIS FED. AID PROJECT													

EXISTING TYPICAL CROSS SECTION

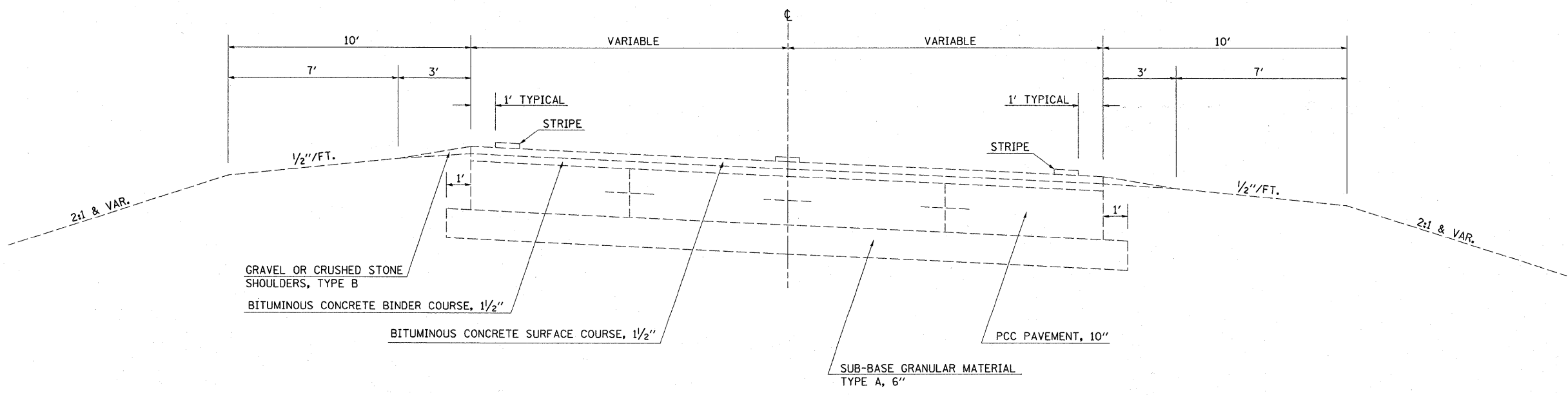
STA. 22+50.00 TO STA. 25+70.10
 STA. 427+11.90 TO STA. 376+31.00



NOTE: TYPICALS NOT DRAWN TO SCALE

EXISTING TYPICAL CROSS SECTION

STA. 2+00.00 TO STA. 3+00.00

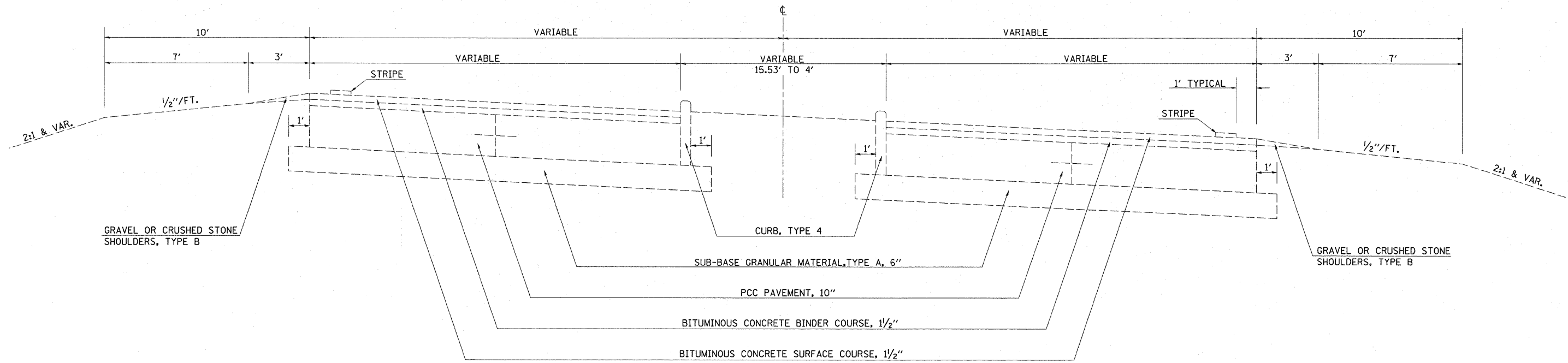


NOTE: TYPICALS NOT DRAWN TO SCALE

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ct:\pw\work\PIWIDOT\SWARTZRW\d0148574\d7	4414-sht-typicals.dgn	DRAWN -	REVISED -		783		LAWRENCE	42	5			
	PLOT SCALE = 50.0000' / 1"	CHECKED -	REVISED -		SCALE: NA			CONTRACT NO. 74414				
	PLOT DATE = 1/29/2010	DATE -	REVISED -		SHEET NO. 2 OF 8 SHEETS			ILLINOIS FED. AID PROJECT				

EXISTING TYPICAL CROSS SECTION

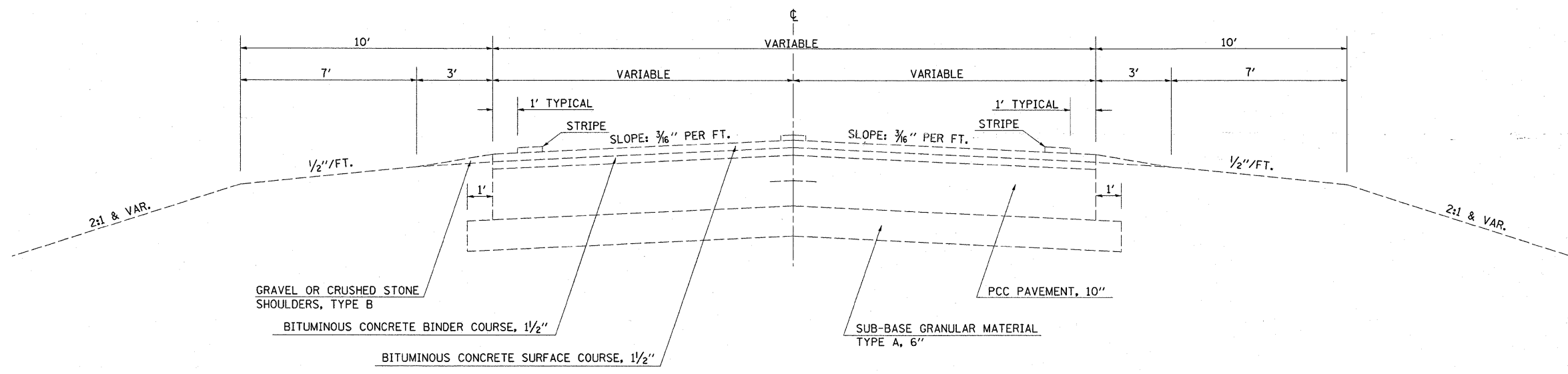
STA. 3+00.00 TO STA. 21+00.00



NOTE: TYPICALS NOT DRAWN TO SCALE

EXISTING TYPICAL CROSS SECTION

STA. 21+00.00 TO STA. 33+00.55



NOTE: TYPICALS NOT DRAWN TO SCALE

• (51-24-2.1-2)RS-1 & (1-X-1)RS-3

FILE NAME =	USER NAME = swartzw
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PLOT SCALE = 50.0000' / IN.	
PLOT DATE = 1/29/2010	

DESIGNED -	REVISED -
DRAWN -	REVISED -
CHECKED -	REVISED -
DATE -	REVISED -

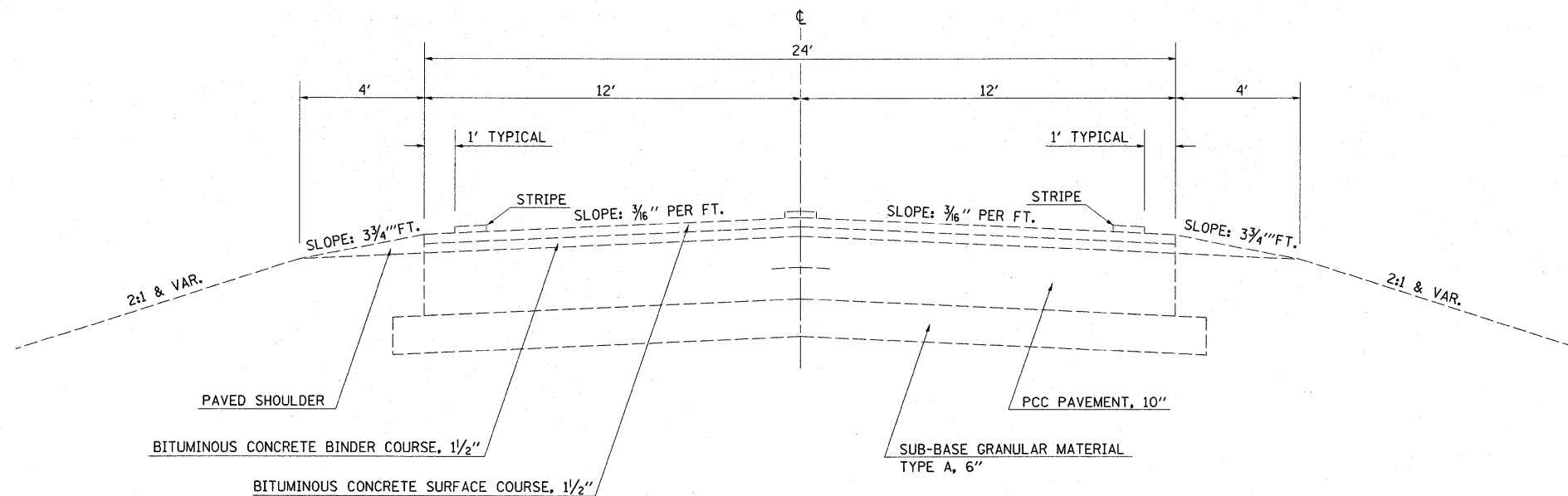
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTIONS	
SCALE: NA	SHEET NO. 3 OF 8 SHEETS
STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
783		LAWRENCE	42	6
CONTRACT NO. 74414				
ILLINOIS FED. AID PROJECT				

EXISTING TYPICAL CROSS SECTION

STA. 33+00.55 TO STA. 38+90.20



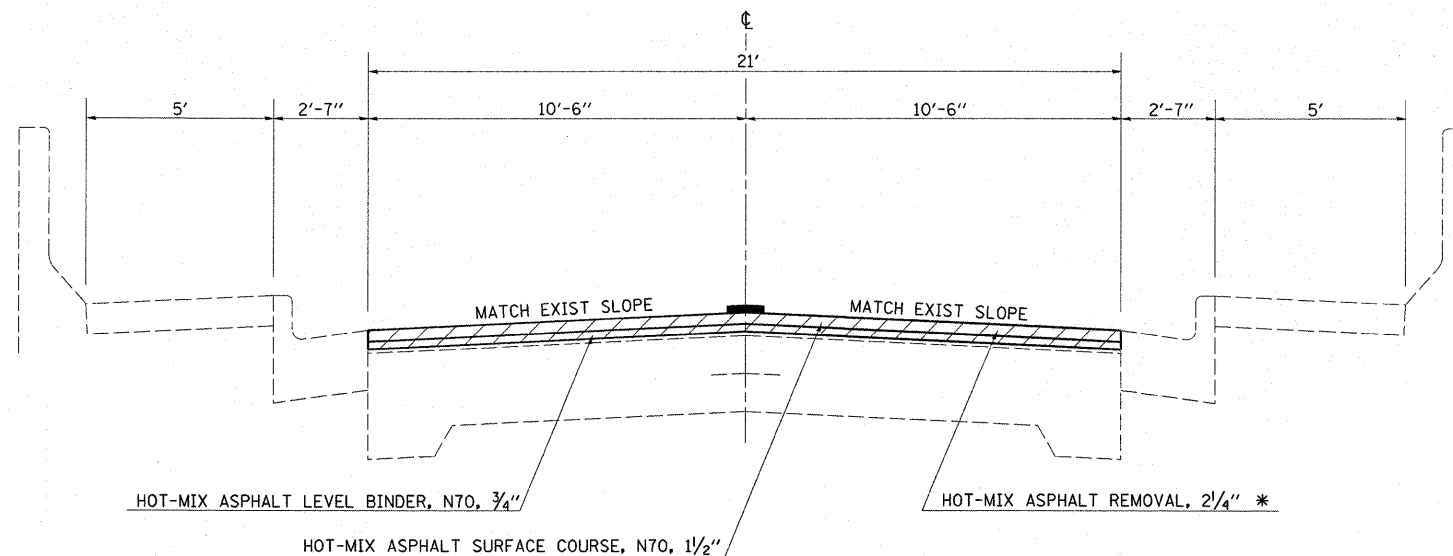
NOTE: TYPICALS NOT DRAWN TO SCALE

FILE NAME =	USER NAME = swartzw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL CROSS SECTIONS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cr:\pw\work\PIWIDOT\SWARTZRW\d8148574\d7	4414-ght-typicals.dgn	DRAWN -	REVISED -		783		LAWRENCE	42	7			
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -		SCALE: NA			SHEET NO. 4 OF 8 SHEETS			STA. TO STA.	
	PLOT DATE = 1/29/2010	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

• (51-24-2,1-2)RS-1 & (1-X-1)RS-3

PROPOSED TYPICAL CROSS SECTION

STA. 13+98.00 TO STA. 16+11.00

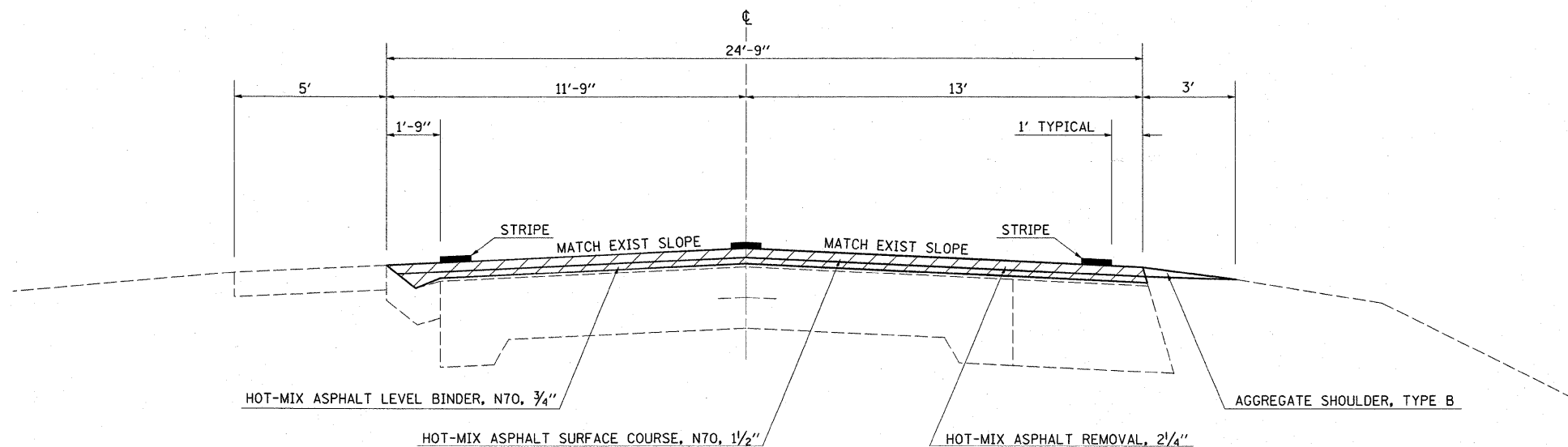


* TO BE COMPLETED IN TWO PASSES TO PREVENT DROP OFF >2" AT CENTERLINE

NOTE: TYPICALS NOT DRAWN TO SCALE

PROPOSED TYPICAL CROSS SECTION

STA. 16+11.00 TO STA. 22+50.00



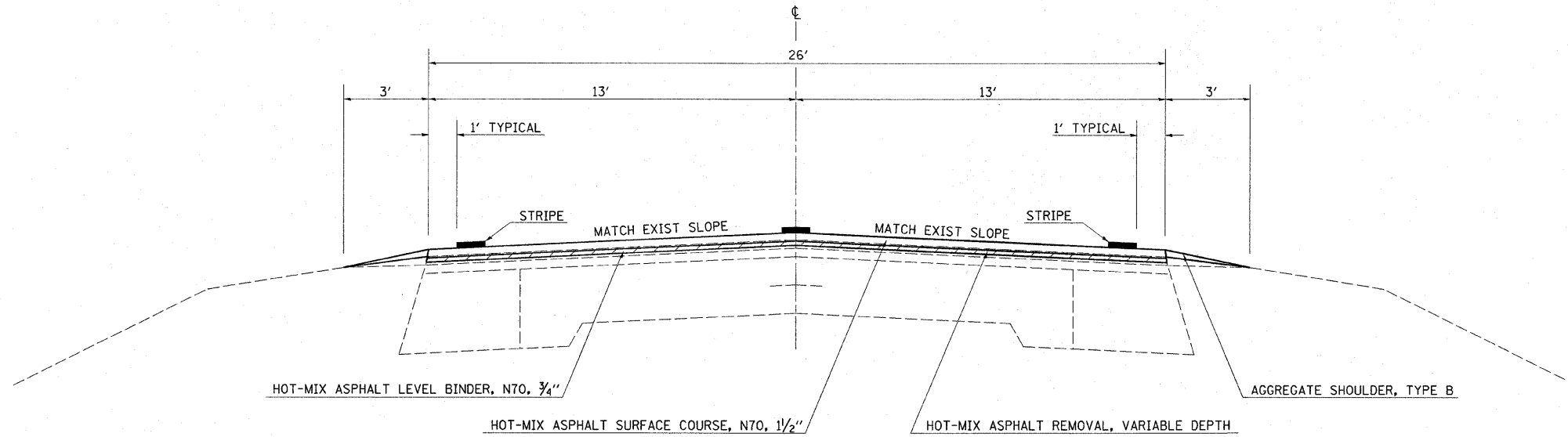
NOTE: TYPICALS NOT DRAWN TO SCALE

• (51-24-2,1-2)RS-1 & (1-X-1)RS-3

FILE NAME =	USER NAME = swartzw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL CROSS SECTIONS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
cr:\pw\work\PIWIDOT\SWARTZRW\0148574\d7	4414-sht-typicals.dgn	DRAWN -	REVISED -		783	*	LAWRENCE	42	8				
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -		SCALE: NA			SHEET NO. 5 OF 8 SHEETS			STA.	TO STA.	CONTRACT NO. 74414
	PLOT DATE = 1/29/2010	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								

PROPOSED TYPICAL CROSS SECTION

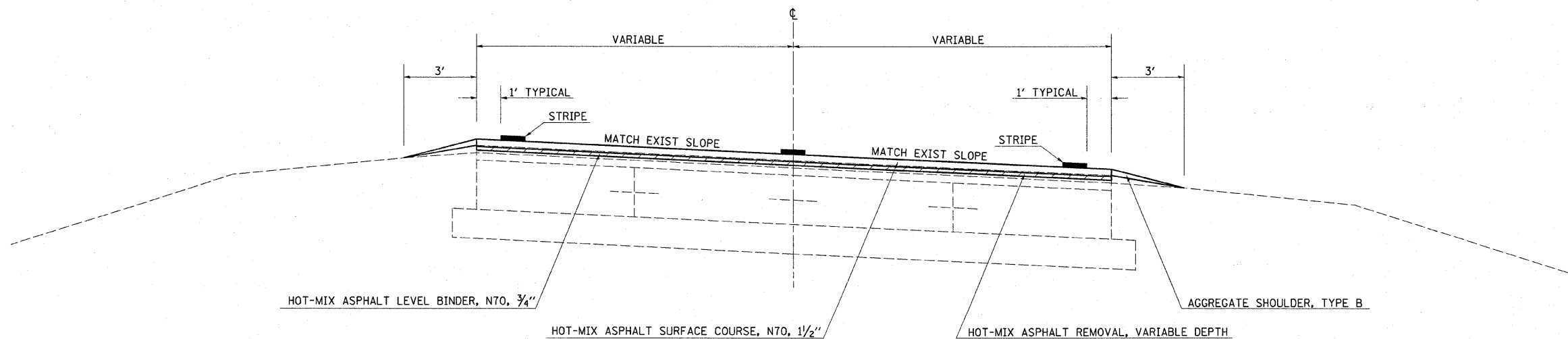
STA. 22+50.00 TO STA. 25+70.10
 STA. 427+11.90 TO STA. 376+31.00



NOTE: TYPICALS NOT DRAWN TO SCALE

PROPOSED TYPICAL CROSS SECTION

STA. 2+00.00 TO STA. 3+00.00



NOTE: TYPICALS NOT DRAWN TO SCALE

• (51-24-2,1-2)RS-1 & (1-X-1)RS-3

FILE NAME =	USER NAME = swartzrw	DESIGNED -	REVISED -
ct:\pw_work\PW\DOT\SWARTZRW\08148574\d7	4414-shr-typicals.dgn	DRAWN -	REVISED -
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 1/29/2010	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

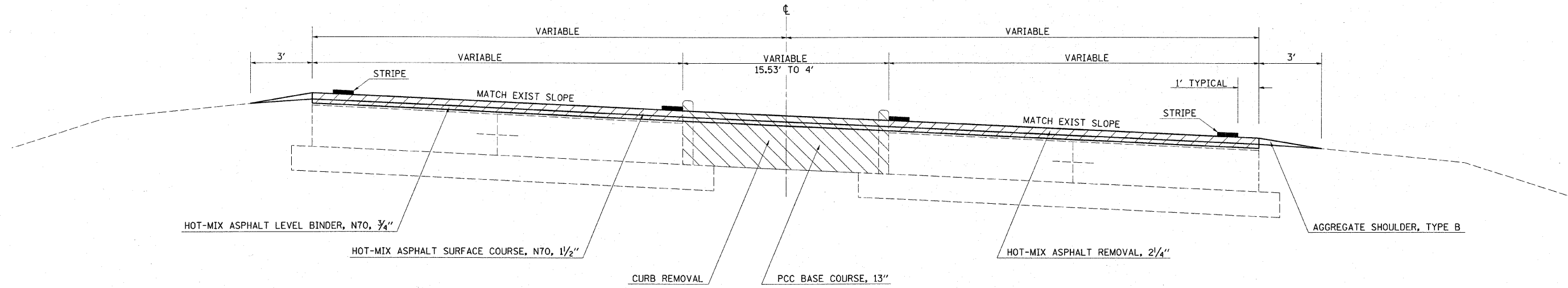
TYPICAL CROSS SECTIONS

SCALE: NA SHEET NO. 6 OF 8 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
783	*	LAWRENCE	42	9
CONTRACT NO. 74414				
ILLINOIS FED. AID PROJECT				

PROPOSED TYPICAL CROSS SECTION

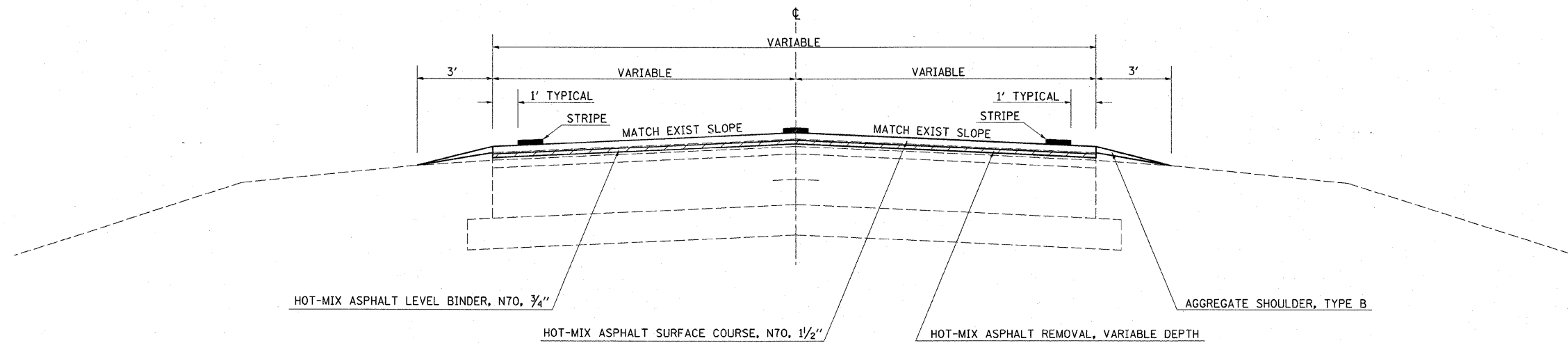
STA. 3+00.00 TO STA. 21+00.00



NOTE: TYPICALS NOT DRAWN TO SCALE

PROPOSED TYPICAL CROSS SECTION

STA. 21+00.00 TO STA. 33+00.55

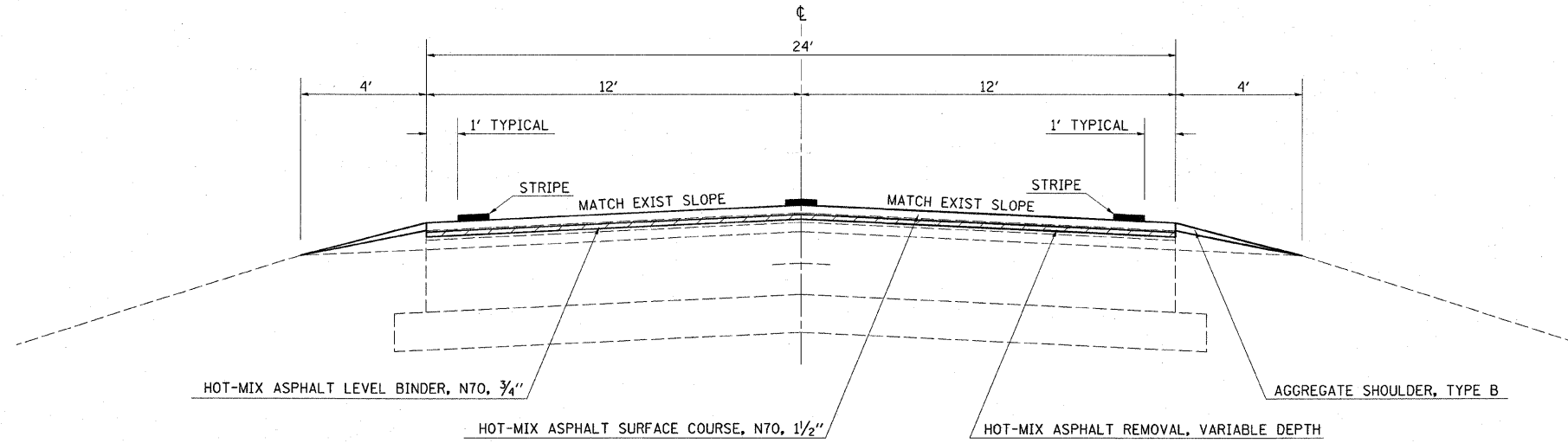


NOTE: TYPICALS NOT DRAWN TO SCALE

FILE NAME =	USER NAME = swartzrw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL CROSS SECTIONS		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwork\pwork\SWARTZRW\0148574\d7	4414-sht-typicals.dgn	DRAWN -	REVISED -		SCALE: NA	SHEET NO. 7 OF 8 SHEETS	STA.	783	•	LAWRENCE	42	10
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -				TO STA.					
	PLOT DATE = 1/29/2010	DATE -	REVISED -									
• (51-24-2,1-2IRS-1 & (1-X-1RS-3												
ILLINOIS FED. AID PROJECT												

PROPOSED TYPICAL CROSS SECTION

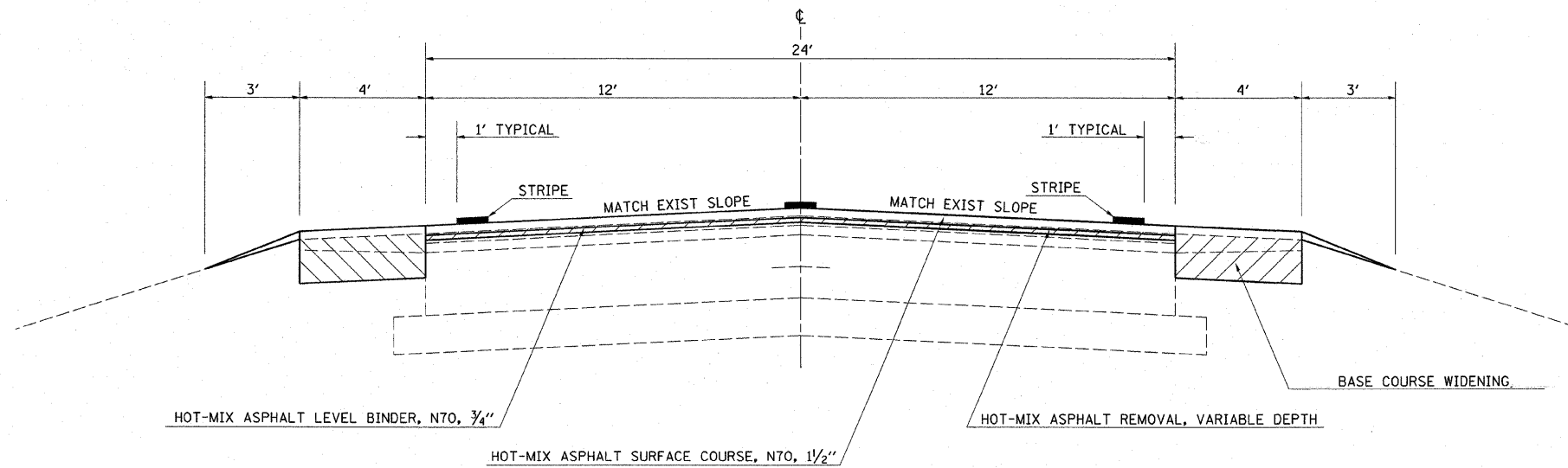
STA. 33+00.55 TO STA. 38+00.00



NOTE: TYPICALS NOT DRAWN TO SCALE

PROPOSED TYPICAL CROSS SECTION

STA. 38+00.00 TO STA. 39+54.00



NOTE: TYPICALS NOT DRAWN TO SCALE

• (51-24-2,1-2)RS-1 & (1-X-1)RS-3

FILE NAME = c:\pwork\pwork\SWARTZRW\d0148574\d7	USER NAME = swartzrw 4414-sht-typicals.dgn	DESIGNED - DRAWN -	REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL CROSS SECTIONS			F.A.P. RTE. 783	SECTION •	COUNTY LAWRENCE	TOTAL SHEETS 42	SHEET NO. 11
PLOT SCALE = 58.0000 ' / IN.		CHECKED -	REVISED -		SCALE: NA	SHEET NO. 8	OF 8	SHEETS	STA.	TO STA.	CONTRACT NO. 74414	
PLOT DATE = 1/29/2010		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

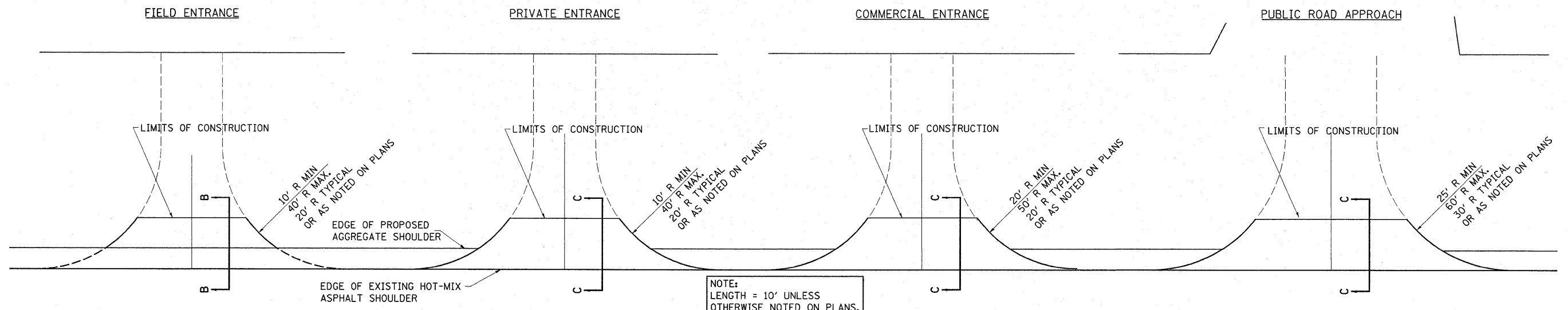
RESURFACING SCHEDULE	LENGTH	AVERAGE PAVEMENT WIDTH	AREA	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	BITUMINOUS MATERIALS (PRIME COAT)	AGGREGATE (PRIME COAT)	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70	LEVELING BINDER (MACHINE METHOD), N70	AGGREGATE SHOULDERS, TYPE B	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	TEMPORARY RAMP
	STATION TO STATION	FOOT	FOOT	SQ YD	SQ YD	SQ YD	GALLON	TON	TON	TON	TON	SQ YD
13+98.00 TO 14+33.00	35.0	21.0	81.7	0.0	81.7	16.3	0.3	6.9	1.7	0.0	0.0	11.7
14+33.00 TO 16+11.00	178.0	21.0	415.3	0.0	415.3	83.1	1.7	34.9	17.4	0.0	0.0	0.0
16+11.00 TO 22+50.00	639.0	24.8	1757.3	0.0	1757.3	527.2	10.5	147.6	73.8	27.3	0.0	0.0
22+50.00 TO 22+85.00	35.0	26.0	101.1	101.1	0.0	20.2	0.4	8.5	4.2	1.5	0.0	0.0
22+85.00 TO 25+70.10	285.1	26.0	823.6	823.6	0.0	164.7	3.3	69.2	34.6	17.9	0.0	0.0
25+70.10 = 427+11.90	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
427+11.90 TO 376+31.00	5080.9	26.0	14678.2	14678.2	0.0	2935.6	58.7	1233.0	616.5	258.8	0.0	0.0
376+31.00 = 2+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
2+00.00 TO 3+00.00	100.0	VAR	336.1	336.1	0.0	67.2	1.3	28.2	14.1	8.5	0.0	0.0
3+00.00 TO 3+35.00	35.0	VAR	138.1	138.1	0.0	27.6	0.6	11.6	5.8	3.0	0.0	0.0
3+35.00 TO 21+00.00	1765.0	VAR	9394.5	0.0	9394.5	1878.9	37.6	789.1	394.6	139.4	0.0	95.6
21+00.00 TO 21+35.00	35.0	VAR	148.8	148.8	0.0	29.8	0.6	12.5	6.2	3.0	0.0	0.0
21+35.00 TO 33+00.55	1165.6	VAR	3691.0	3691.0	0.0	738.2	14.8	310.0	155.0	99.6	0.0	0.0
33+00.55 TO 38+55.20	554.7	24.0	1479.1	1479.1	0.0	295.8	5.9	124.2	62.1	47.4	0.0	0.0
38+55.20 TO 38+90.20	35.0	24.0	93.3	0.0	0.0	18.7	0.4	7.8	2.0	15.0	93.3	13.3
MEDIAN REMOVAL			1624.0	0.0	1624.0	324.8	6.5	136.4	68.2	0.0	0.0	0.0
TOTALS			34762.0	21396.0	13273.0	7128.0	143.0	2920.0	1456.0	621.0	93.0	121.0

• (51-24-2,1-2)RS-1 & (1-X-1)RS-3

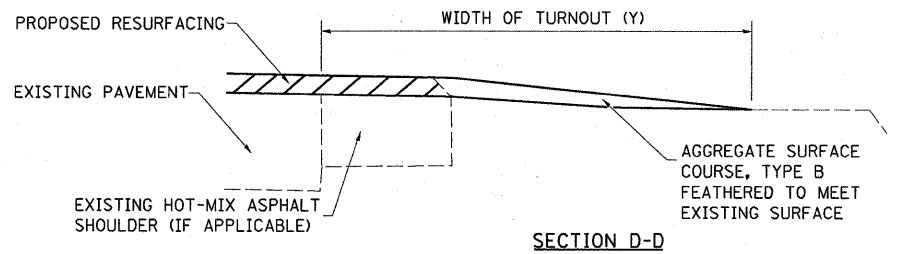
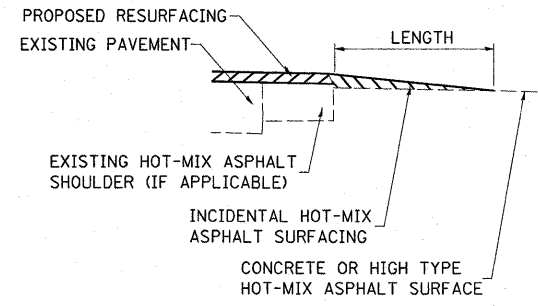
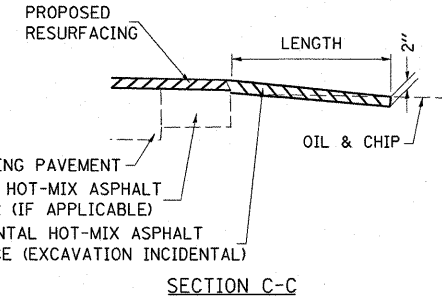
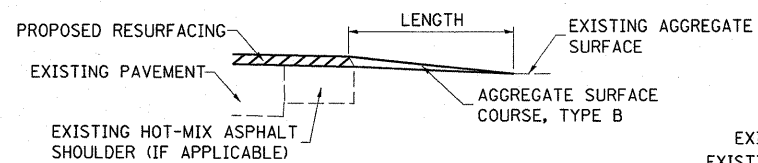
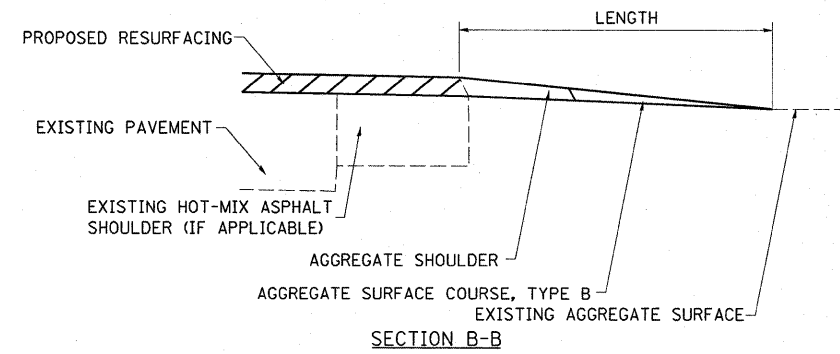
PAVEMENT MARKING SCHEDULE			LENGTH	PAINT PAVEMENT MARKING - LINE 4"	TEMPORARY PAVEMENT MARKING - LINE 4"	TEMPORARY PAVEMENT MARKING - LINE 6"	TEMPORARY PAVEMENT MARKING - LINE 12"	SHORT-TERM PAVEMENT MARKING	WORK ZONE PAVEMENT MARKING REMOVAL	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	RAISED REFLECTIVE PAVEMENT MARKERS	RAISED REFLECTIVE PAVEMENT MARKERS (BRIDGE)
13+98.00	TO	14+33.00	35.0	140.0	140.0	0.0	0.0	12.0	1.3	0.0	0.0	0.0	0.0	0.0	2.0	0.0
14+33.00	TO	16+10.00	177.0	708.0	708.0	0.0	0.0	48.0	5.3	0.0	0.0	0.0	0.0	0.0	6.0	0.0
16+10.00	TO	22+50.00	640.0	1440.0	1440.0	0.0	0.0	192.0	21.3	0.0	0.0	0.0	0.0	0.0	8.0	0.0
22+50.00	TO	22+85.00	35.0	78.8	78.8	0.0	0.0	12.0	1.3	0.0	0.0	0.0	0.0	0.0	1.0	0.0
22+85.00	TO	25+70.10	285.1	641.5	641.5	0.0	0.0	84.0	9.3	0.0	0.0	0.0	0.0	0.0	4.0	0.0
25+70.10	=	427+11.90	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
427+11.90	TO	377+36.00	4975.9	11010.8	11010.8	0.0	0.0	1488.0	165.3	0.0	0.0	0.0	0.0	0.0	63.0	0.0
376+31.00	TO	377+36.00	0.0	420.0	420.0	0.0	0.0	36.0	4.0	0.0	0.0	420.0	0.0	0.0	4.0	0.0
376+31.00	=	2+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
2+00.00	TO	3+00.00	100.0	0.0	400.0	0.0	0.0	27.0	3.0	0.0	0.0	400.0	0.0	0.0	4.0	0.0
3+00.00	TO	3+35.00	35.0	0.0	70.0	0.0	0.0	9.0	1.0	0.0	0.0	70.0	0.0	0.0	2.0	0.0
3+35.00	TO	21+00.00	1765.0	0.0	5083.0	528.0	547.0	15885.0	1765.0	124.8	124.8	5083.0	528.0	547.0	14.0	0.0
21+00.00	TO	21+35.00	35.0	0.0	70.0	0.0	0.0	9.0	1.0	0.0	0.0	70.0	0.0	0.0	2.0	0.0
21+35.00	TO	33+00.55	1165.6	0.0	4662.2	0.0	0.0	261.0	29.0	0.0	0.0	4662.2	0.0	0.0	30.0	0.0
33+00.55	TO	38+55.20	554.7	0.0	2218.6	0.0	0.0	168.0	18.7	0.0	0.0	2218.6	0.0	0.0	14.0	0.0
38+55.20	TO	41+40.94	285.7	564.2	140.0	0.0	0.0	63.0	7.0	0.0	0.0	140.0	0.0	0.0	2.0	3.0
MEDIAN REMOVAL			0.0	0.0	5405.0	165.3	1143.0	0.0	0.0	0.0	0.0	10810.0	165.3	1143.0	176.0	0.0
TOTALS			10089.0	15003.0	32488.0	693.0	1690.0	18294.0	2033.0	125.0	125.0	23874.0	693.0	1690.0	332.0	3.0

• (51-24-2,1-2)RS-1 & (1-X-1)RS-3

FILE NAME =	USER NAME = swartzw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pw\work\PIWIDOT\SWARTZRW\d0148574\d7	4414-sht-soh.dgn	DRAWN -	REVISED -					783	*	LAWRENCE	42	13
PLOT SCALE = 50.0000 ' / IN.		CHECKED -	REVISED -		SCALE: NA SHEET NO. 2 OF 2 SHEETS STA. TO STA.			CONTRACT NO. 74414				
PLOT DATE = 1/29/2010		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							



NOTE:
LENGTH = 10' UNLESS
OTHERWISE NOTED ON PLANS.



TYPICAL SECTION AT MAILBOX TURNOUT
NOTE: SEE STANDARD 406201 FOR MAILBOX TURNOUT DETAILS

NOTES

THE COST OF THE ASPHALT MATERIALS, AND AGGREGATE (PRIME COAT) FOR ENTRANCES AND PUBLIC ROADS AND APPROACHES SHALL BE INCLUDED IN THE PAY ITEM INCIDENTAL HOT-MIX ASPHALT SURFACING.

THE COST OF EXCAVATION IS INCLUDED IN THE PAY ITEM INCIDENTAL HOT-MIX ASPHALT SURFACING.

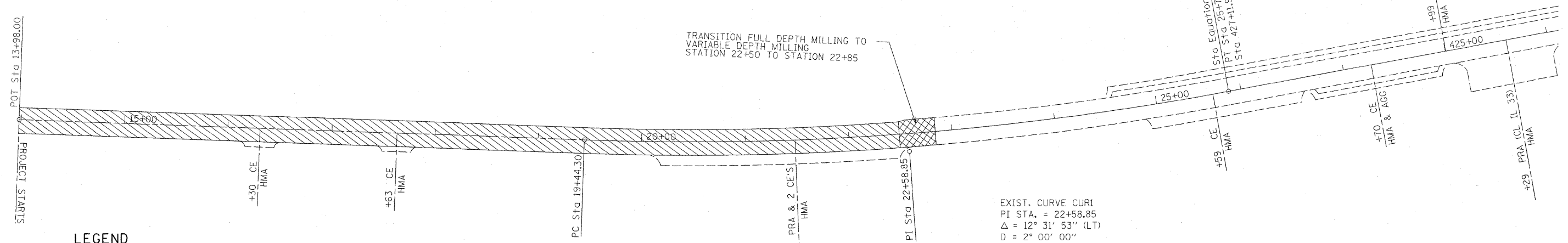
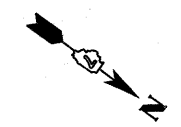
IF THERE IS NOT EXISTING HOT-MIX ASPHALT SHOULDER THEN THE ENTRANCE TAPER STARTS AT THE EDGE OF EXISTING PAVEMENT.

THE COST OF THE BITUMINOUS MATERIALS AND AGGREGATE (PRIME COAT) FOR ENTRANCES AND PUBLIC ROAD APPROACHES SHALL BE INCLUDED IN THE PAY ITEM INCIDENTAL HOT-MIX ASPHALT SURFACING.

SIDE	STATION	TYPE	INCIDENTAL HOT-MIX ASPHALT SURFACING		TYPE	AGGREGATE SURFACE COURSE, TYPE B		SIDE	STATION	TYPE	INCIDENTAL HOT-MIX ASPHALT SURFACING		TYPE	AGGREGATE SURFACE COURSE, TYPE B		SIDE	STATION	TYPE	INCIDENTAL HOT-MIX ASPHALT SURFACING		TYPE	AGGREGATE SURFACE COURSE, TYPE B		SIDE	STATION	TYPE	INCIDENTAL HOT-MIX ASPHALT SURFACING		TYPE	AGGREGATE SURFACE COURSE, TYPE B		
			TON	TON		TON	TON				TON	TON		TON	TON				TON	TON		TON	TON				TON	TON		TON		
RT	16+30	CE	2.8					LT	400+83	CE	24.1																					
RT	17+63	CE	2.8					RT	400+61	PRA	24.4																					
RT	21+48	PRA	34.9					LT	398+37	CE	4.9																					
RT	25+59	CE	21.3					RT	397+63	CE		21.1																				
LT	425+70	CE	10.5					RT	397+63	CE	36.6																					
LT	425+70	CE		5.5				LT	397+55	CE	6.1																					
RT	424+99	CE	45.7					LT	396+66	MBT		4.0																				
RT	424+99	CE		25.0				LT	396+24	FE		4.6																				
LT	424+29	PRA	14.4					RT	392+93	CE	3.5																					
LT	422+88	CE	12.8					RT	392+93	CE		1.6																				
LT	422+88	CE		7.2				RT	390+70	CE	5.0																					
LT	417+62	FE		2.0				LT	390+00	FE		3.6																				
RT	416+22	CE	22.6					RT	389+41	CE		25.7																				
RT	416+22	CE		12.8				LT	377+06	PE		11.0																				
RT	411+01	CE		58.1				LT	8+04	MBT1		4.0																				
RT	404+04	CE	22.3					RT	13+50	FE		2.0																				
RT	402+02	CE		7.5				LT	20+00	PE		2.8																				
									TOTALS		294.7	198.5																				

FE=FIELD ENTRANCE
PE=PRIVATE ENTRANCE
CE=COMMERCIAL ENTRANCE
PRA - PUBLIC ROAD APPROACH
MBT - MAILBOX TURNOUT

NOTE: STATIONING RUNS BACKWARDS FROM STATION EQUATION:
 25+70.90 (BK) = 427+11.90 (AH)
 TO STATION EQUATION:
 376+31.00 (BK) = 2+00.00 (AH)

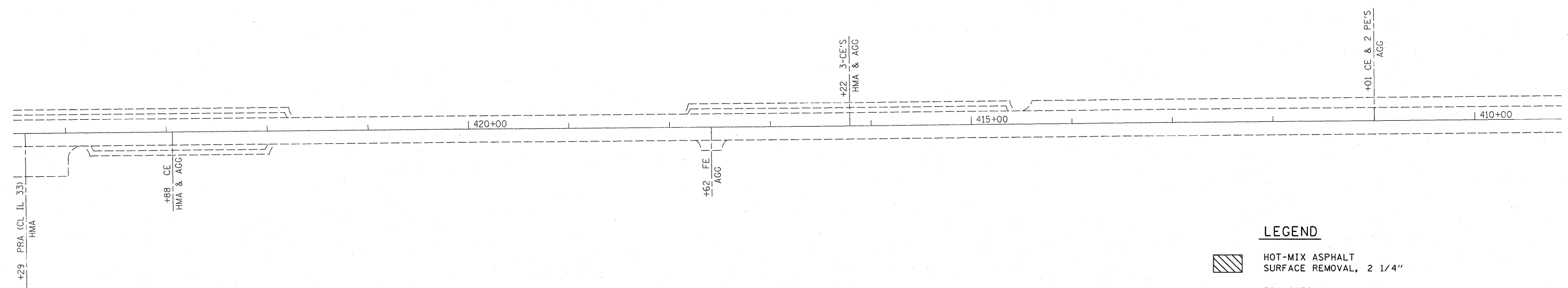
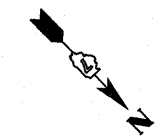


LEGEND

- HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- TRANSITION FULL DEPTH MILLING TO VARIABLE DEPTH MILLING
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

EXIST. CURVE CUR1
 PI STA. = 22+58.85
 $\Delta = 12^\circ 31' 53''$ (LT)
 $D = 2^\circ 00' 00''$
 $R = 2,864.90'$
 $T = 314.55'$
 $L = 626.60'$
 $E = 17.22'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 19+44.30$
 $P.T. STA. = 25+70.90$

NOTE: STATIONING RUNS BACKWARDS FROM STATION EQUATION:
 25+70.90 (BK) = 427+11.90 (AH)
 TO STATION EQUATION:
 376+31.00 (BK) = 2+00.00 (AH)



LEGEND

- HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- TRANSITION FULL DEPTH MILLING TO VARIABLE DEPTH MILLING
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

FILE NAME =	USER NAME = swartzrw	DESIGNED -	REVISED -
ca\pwwork\pwwid01\SWARTZRW\08148574\07	4414-sht-plan.dgn	DRAWN -	REVISED -
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 1/29/2010	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

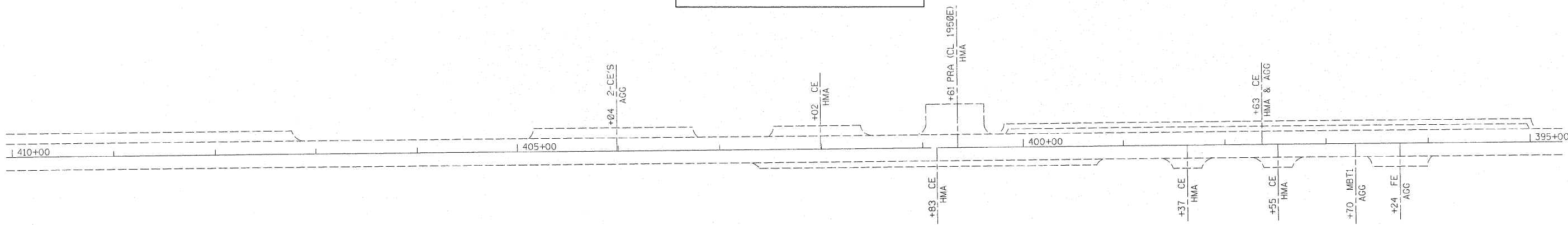
PLANS SHEETS

SCALE: 50 SHEET NO. 1 OF 4 SHEETS STA. 13+98 TO STA. 410+00




• (51-24-2,1-2)RS-1 & (1-X-1)RS-3

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
783		LAWRENCE	42	15
			CONTRACT NO. 74414	
ILLINOIS FED. AID PROJECT				

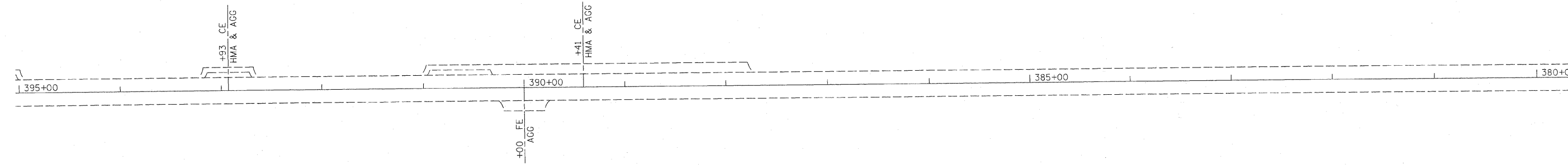
NOTE: STATIONING RUNS BACKWARDS FROM
 STATION EQUATION:
 25+70.90 (BK) = 427+11.90 (AH)
 TO STATION EQUATION:
 376+31.00 (BK) = 2+00.00 (AH)






LEGEND

-  HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
-  TRANSITION FULL DEPTH MILLING TO VARIABLE DEPTH MILLING
-  HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

NOTE: STATIONING RUNS BACKWARDS FROM
 STATION EQUATION:
 25+70.90 (BK) = 427+11.90 (AH)
 TO STATION EQUATION:
 376+31.00 (BK) = 2+00.00 (AH)



LEGEND

-  HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
-  TRANSITION FULL DEPTH MILLING TO VARIABLE DEPTH MILLING
-  HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

FILE NAME =	USER NAME = swartzw	DESIGNED -	REVISED -
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	PLOT SCALE = 50.0000 1/4" IN.	CHECKED -	REVISED -
	PLOT DATE = 1/29/2010	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

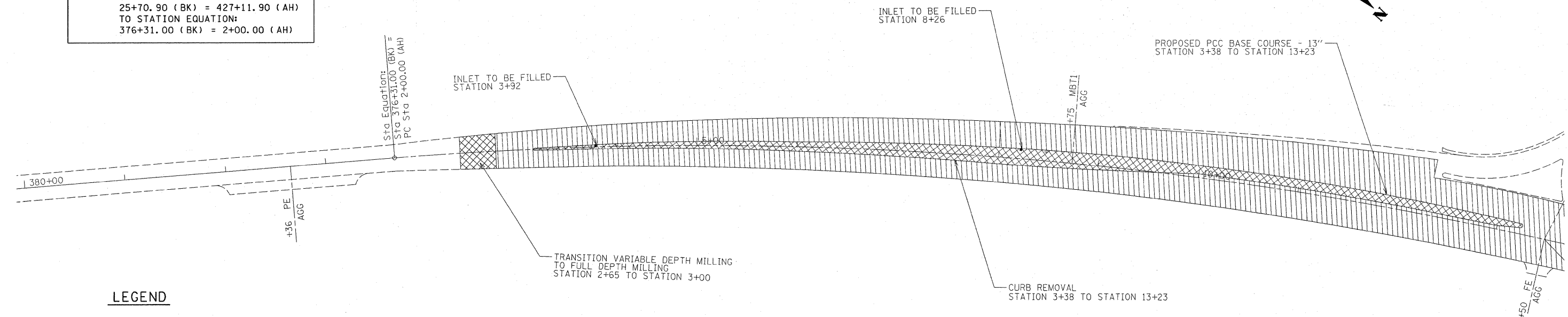
PLAN SHEETS

SCALE: 50 SHEET NO. 2 OF 4 SHEETS STA. 410+00 TO STA. 380+00

• (51-24-2,1-2)RS-1 & (1-X-1)RS-3

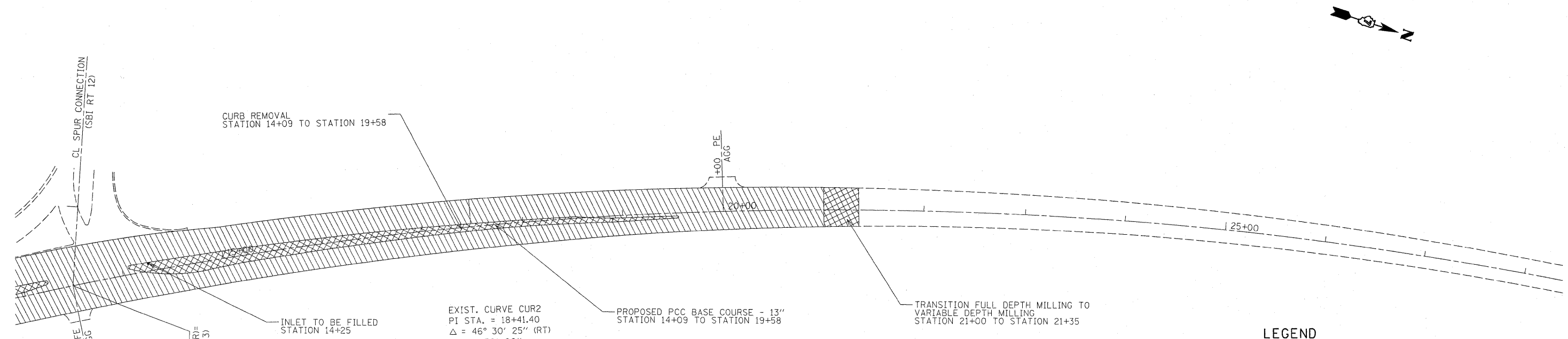
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
783	•	LAWRENCE	42	16
			CONTRACT NO. 74414	
ILLINOIS FED. AID PROJECT				

NOTE: STATIONING RUNS BACKWARDS FROM
 STATION EQUATION:
 25+70.90 (BK) = 427+11.90 (AH)
 TO STATION EQUATION:
 376+31.00 (BK) = 2+00.00 (AH)



LEGEND

- HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- TRANSITION FULL DEPTH MILLING TO VARIABLE DEPTH MILLING
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT



LEGEND

- HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- TRANSITION FULL DEPTH MILLING TO VARIABLE DEPTH MILLING
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

FILE NAME =	USER NAME = swartzrw	DESIGNED -	REVISED -
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	PLOT DATE = 1/29/2010	DATE -	REVISED -

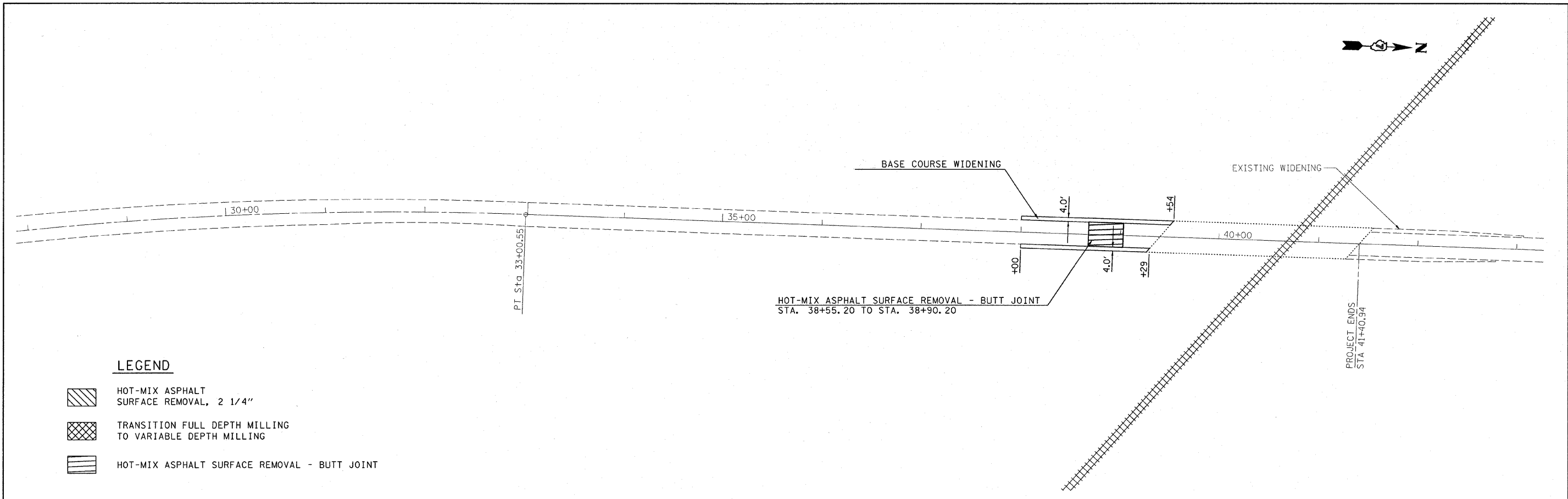
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PLAN SHEETS

SCALE: 50 SHEET NO. 3 OF 4 SHEETS STA. 380+00 TO STA. 28+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
783		LAWRENCE	42	17
CONTRACT NO. 74414				
ILLINOIS FED. AID PROJECT				

• (51-24-2.1-2)RS-1 & (1-X-1)RS-3



LEGEND

- HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- TRANSITION FULL DEPTH MILLING TO VARIABLE DEPTH MILLING
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

FILE NAME =	USER NAME = swartzw	DESIGNED -	REVISED -
cs:\pw\work\PWIDOT\SWARTZRW\d0148574\07	4414-sht-plan.dgn	DRAWN -	REVISED -
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	PLOT DATE = 1/29/2010	DATE -	REVISED -

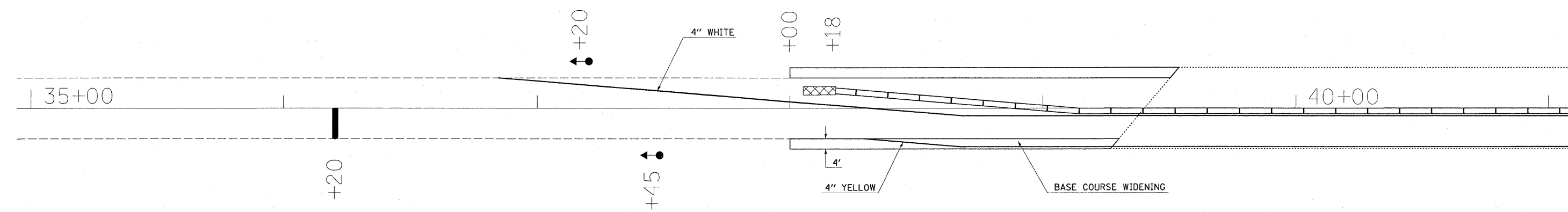
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN SHEETS

SCALE: 50 SHEET NO. 4 OF 4 SHEETS STA. 28+00 TO STA. 41+40.94

• (51-24-2,1-2)RS-1 & (1-X-1)RS-3

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
783	•	LAWRENCE	42	18
CONTRACT NO. 74414				
ILLINOIS FED. AID PROJECT				

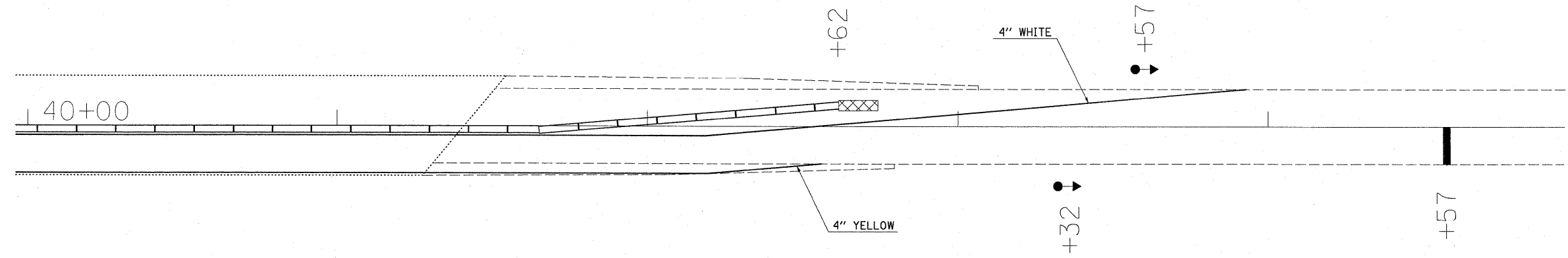


LEGEND

- TEMPORARY BRIDGE TRAFFIC SIGNALS
- ▨▨▨▨ IMPACT ATTENUATOR, NRD-TL3
- ▨▨▨▨ TEMPORARY CONCRETE BARRIER
- ▬ STOP BAR (24" WHITE)

FILE NAME =		USER NAME = swartzw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL STAGE 1			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca\pw\work\PWIDD\SWARTZRW\d8148574\d7		4414-sht-traffic.dgn	DRAWN -	REVISED -		783		LAWRENCE	42	19			
PLOT SCALE = 20,0000' / IN.			CHECKED -	REVISED -		CONTRACT NO. 74414			ILLINOIS FED. AID PROJECT				
PLOT DATE = 1/29/2010			DATE -	REVISED -		SCALE: 20	SHEET NO. 1 OF 2 SHEETS	STA.	TO STA.				

• (51-24-2,1-2)RS-1 & (1-X-1)RS-3



LEGEND

- TEMPORARY BRIDGE TRAFFIC SIGNALS
- ▤ IMPACT ATTENUATOR, NRD-TL3
- ▭ TEMPORARY CONCRETE BARRIER
- ▬ STOP BAR (24" WHITE)

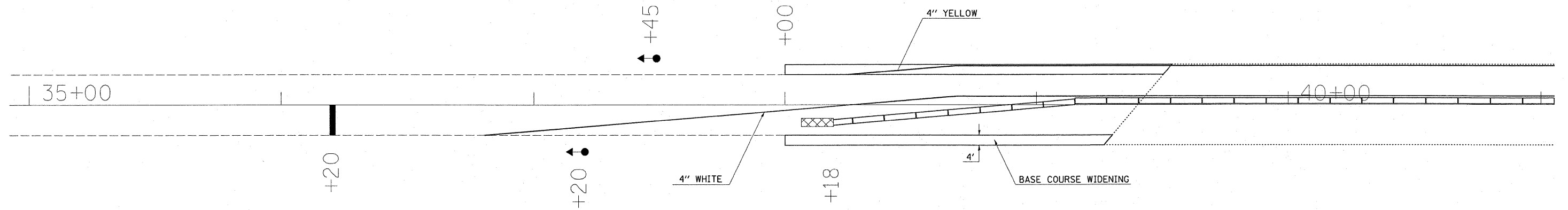
• (51-24-2,1-2)RS-1 & (1-X-1)RS-3

FILE NAME = c:\pw_work\PWIDOT\SWARTZRW\d0148574\d7	USER NAME = swartzrw 4414-ah-t-trafficstg1.dgn	DESIGNED -	REVISED -
		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL STAGE 1			
SCALE: 20	SHEET NO. 2 OF 2 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
783	•	LAWRENCE	42	20
CONTRACT NO. 74414				
ILLINOIS FED. AID PROJECT				



LEGEND

- TEMPORARY BRIDGE TRAFFIC SIGNALS
- ⊠ IMPACT ATTENUATOR, NRD-TL3
- ▭ TEMPORARY CONCRETE BARRIER
- ▬ STOP BAR (24" WHITE)

• (51-24-2,1-2)RS-1 & (1-X-1)RS-3

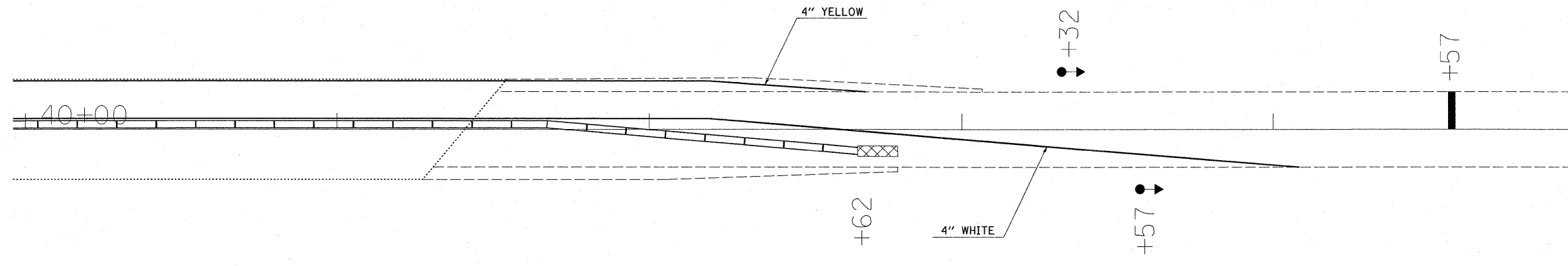
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4414-ght-trafficstg2.dgn		DRAWN -	REVISED -
PLOT SCALE = 20.0000 ' / IN.		CHECKED -	REVISED -
PLOT DATE = 1/29/2010		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL STAGE 2

SCALE: 20 SHEET NO. 1 OF 2 SHEETS STA. TO STA.

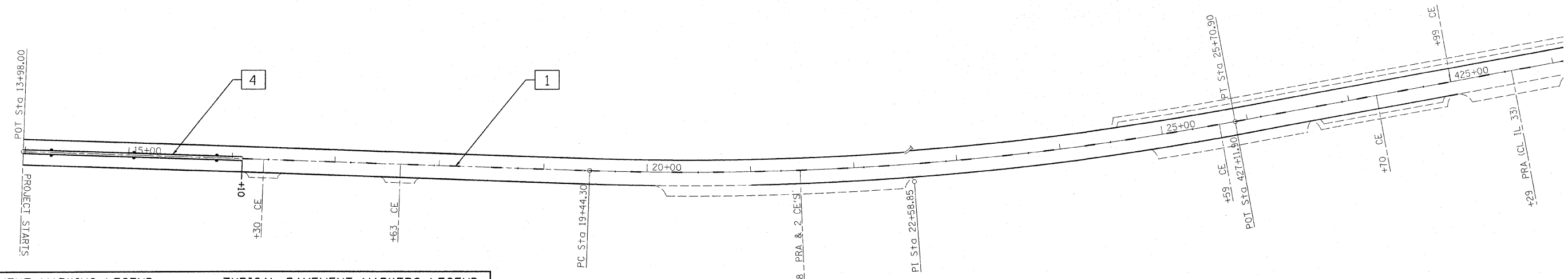
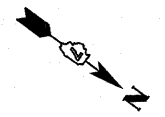
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
783		LAWRENCE	42	21
CONTRACT NO. 74414			ILLINOIS FED. AID PROJECT	



- LEGEND**
- TEMPORARY BRIDGE TRAFFIC SIGNALS
 - ▣▣▣▣ IMPACT ATTENUATOR, NRD-TL3
 - ▭▭▭▭ TEMPORARY CONCRETE BARRIER
 - ▬ STOP BAR (24" WHITE)

• (51-24-2.1-2)RS-1 & (1-X-1)RS-3

FILE NAME =	USER NAME = swartzrw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL STAGE 2		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca\pwork\PWIDOT\SWARTZRW\d8148574\d7	4414-sht-trafficstg2.dgn	DRAWN -	REVISED -		783		LAWRENCE	42	22			
	PLOT SCALE = 20.0000' / IN.	CHECKED -	REVISED -		SCALE: 20		SHEET NO. 2 OF 2 SHEETS		STA. TO STA.		CONTRACT NO. 74414	
	PLOT DATE = 1/29/2010	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

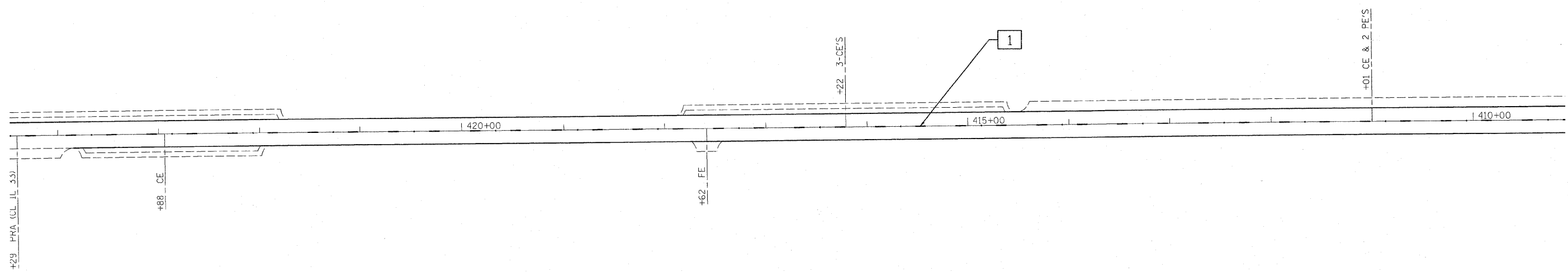
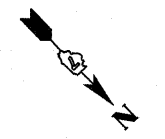


PAVEMENT MARKING LEGEND

- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 RESERVED
- 6 RESERVED
- 7 6" (100) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 6" (150) LANE LINE EXTENSIONS (WHITE)

TYPICAL PAVEMENT MARKERS LEGEND

- ◆ TWO-WAY AMBER MARKER
- ▶ ONE-WAY AMBER MARKER
- ▽ ONE-WAY CRYSTAL MARKER



FILE NAME =	USER NAME = swartzw	DESIGNED -	REVISED -
c:\pwork\pwork\swartzw\d0148574\1477414-shit-striping.dgn		DRAWN -	REVISED -
PLOT SCALE = 50.0000' / 1" IN.		CHECKED -	REVISED -
PLOT DATE = 1/29/2010		DATE -	REVISED -

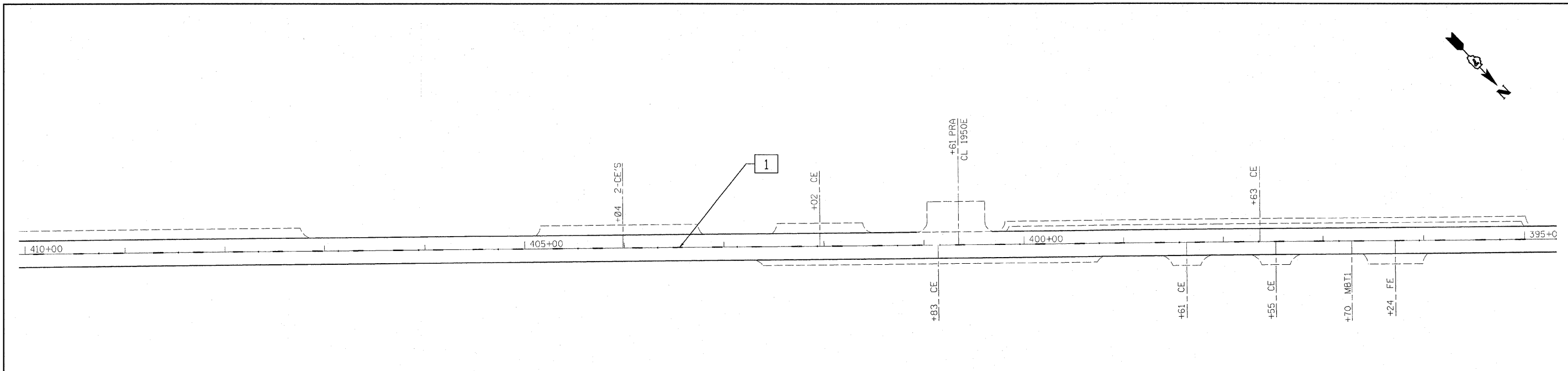
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STRIPING DETAILS

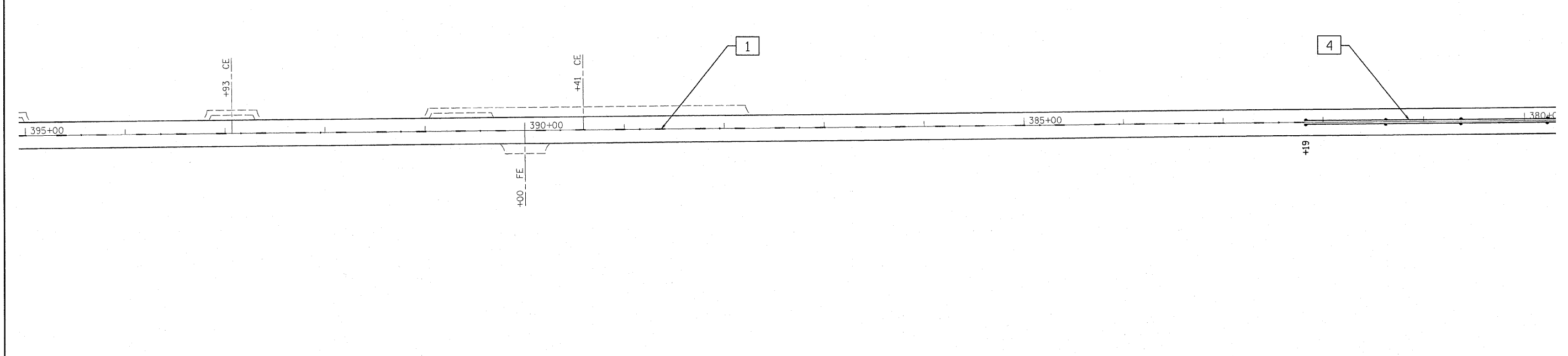
SCALE: 50 SHEET NO. 1 OF 4 SHEETS STA. 13+98 TO STA. 410+00

• (51-24-2,1-2)RS-1 & (1-X-1)RS-3

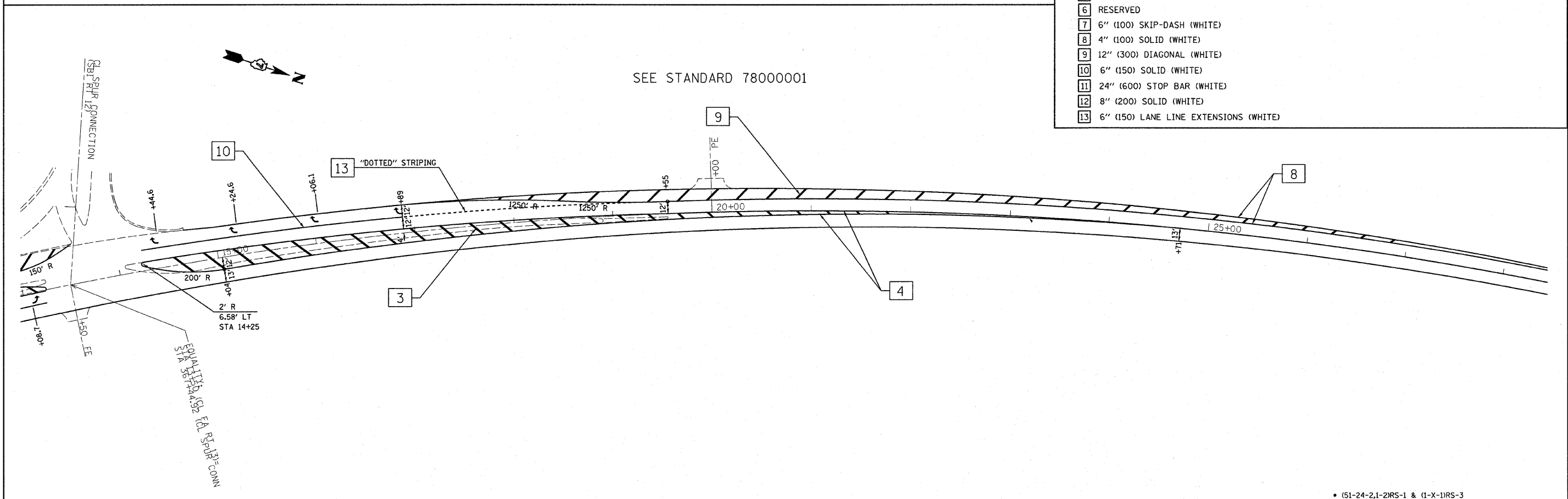
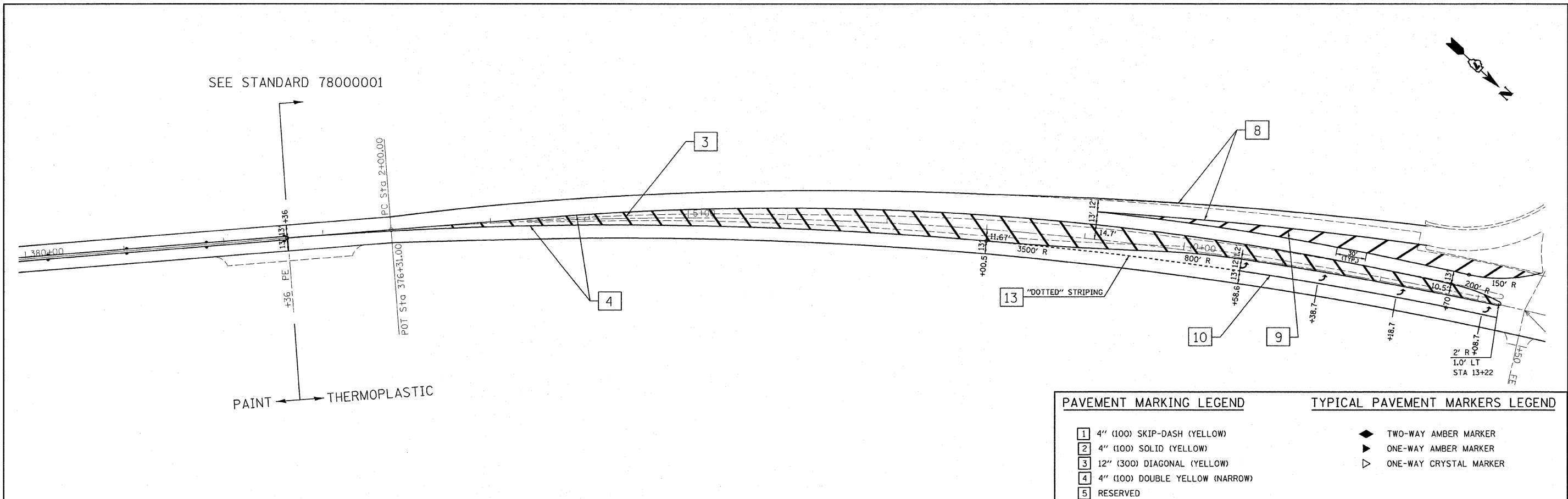
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
783		LAWRENCE	42	23
CONTRACT NO. 74414			ILLINOIS FED. AID PROJECT	



PAVEMENT MARKING LEGEND		TYPICAL PAVEMENT MARKERS LEGEND	
1	4" (100) SKIP-DASH (YELLOW)	◆	TWO-WAY AMBER MARKER
2	4" (100) SOLID (YELLOW)	▶	ONE-WAY AMBER MARKER
3	12" (300) DIAGONAL (YELLOW)	▷	ONE-WAY CRYSTAL MARKER
4	4" (100) DOUBLE YELLOW (NARROW)		
5	RESERVED		
6	RESERVED		
7	6" (100) SKIP-DASH (WHITE)		
8	4" (100) SOLID (WHITE)		
9	12" (300) DIAGONAL (WHITE)		
10	6" (150) SOLID (WHITE)		
11	24" (600) STOP BAR (WHITE)		
12	8" (200) SOLID (WHITE)		
13	6" (150) LANE LINE EXTENSIONS (WHITE)		



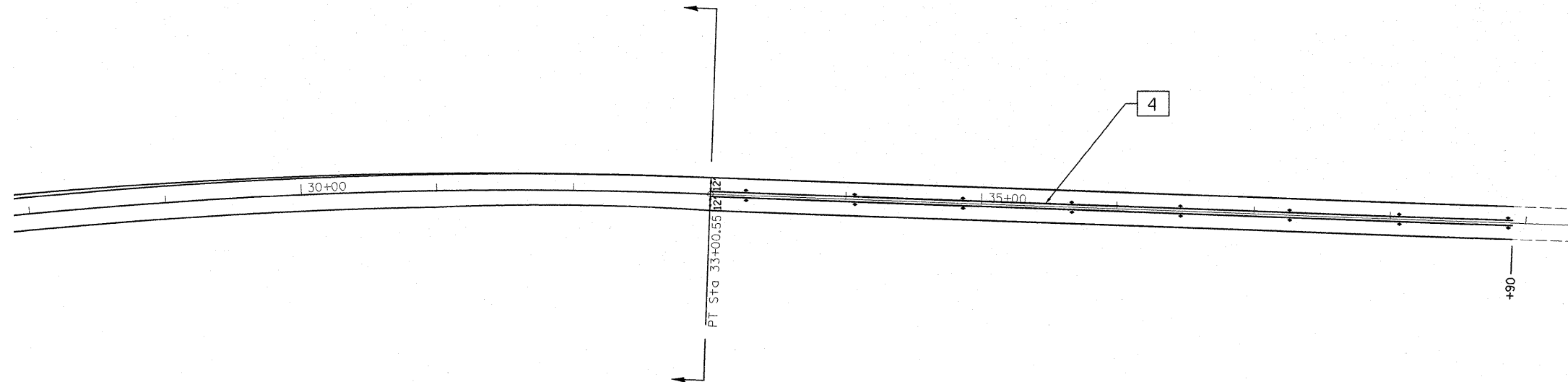
FILE NAME =	USER NAME = swartzrw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STRIPING DETAILS		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwork\pwork\DOT\SWARTZRW\d0146574\d7	4414-shl-striping.dgn	DRAWN -	REVISED -		SCALE: 50	SHEET NO. 2 OF 4 SHEETS	STA. 410+00 TO STA. 380+00	783	*	LAWRENCE	42	24
	PLOT SCALE = 50,0000' / IN.	CHECKED -	REVISED -									
	PLOT DATE = 1/29/2010	DATE -	REVISED -									
• (51-24-2,1-2)RS-1 & (1-X-1)RS-3												
ILLINOIS FED. AID PROJECT												



PAVEMENT MARKING LEGEND		TYPICAL PAVEMENT MARKERS LEGEND	
1	4" (100) SKIP-DASH (YELLOW)	◆	TWO-WAY AMBER MARKER
2	4" (100) SOLID (YELLOW)	▶	ONE-WAY AMBER MARKER
3	12" (300) DIAGONAL (YELLOW)	▷	ONE-WAY CRYSTAL MARKER
4	4" (100) DOUBLE YELLOW (NARROW)		
5	RESERVED		
6	RESERVED		
7	6" (100) SKIP-DASH (WHITE)		
8	4" (100) SOLID (WHITE)		
9	12" (300) DIAGONAL (WHITE)		
10	6" (150) SOLID (WHITE)		
11	24" (600) STOP BAR (WHITE)		
12	8" (200) SOLID (WHITE)		
13	6" (150) LANE LINE EXTENSIONS (WHITE)		

FILE NAME =	USER NAME = swartzw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STRIPING DETAILS		• (51-24-2,1-2RS-1 & (1-X-1RS-3	
c:\pw_work\PWIDOT\SWARTZRW\d0148574.dwg	4414-sht-striping.dgn	DRAWN -	REVISED -		F.A.P. SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 58.0000' / IN.	CHECKED -	REVISED -	REVISED -		783	•	LAWRENCE	42 25
PLOT DATE = 1/29/2010	DATE -	REVISED -	REVISED -		CONTRACT NO. 74414			
					ILLINOIS FED. AID PROJECT			

SEE STANDARD 78000001



PAVEMENT MARKING LEGEND

- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 RESERVED
- 6 RESERVED
- 7 6" (100) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 6" (150) LANE LINE EXTENSIONS (WHITE)

TYPICAL PAVEMENT MARKERS LEGEND

- ◆ TWO-WAY AMBER MARKER
- ▶ ONE-WAY AMBER MARKER
- ▷ ONE-WAY CRYSTAL MARKER

FILE NAME =	USER NAME = swartzw	DESIGNED -	REVISED -
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	PLOT DATE = 1/29/2010	DATE -	REVISED -

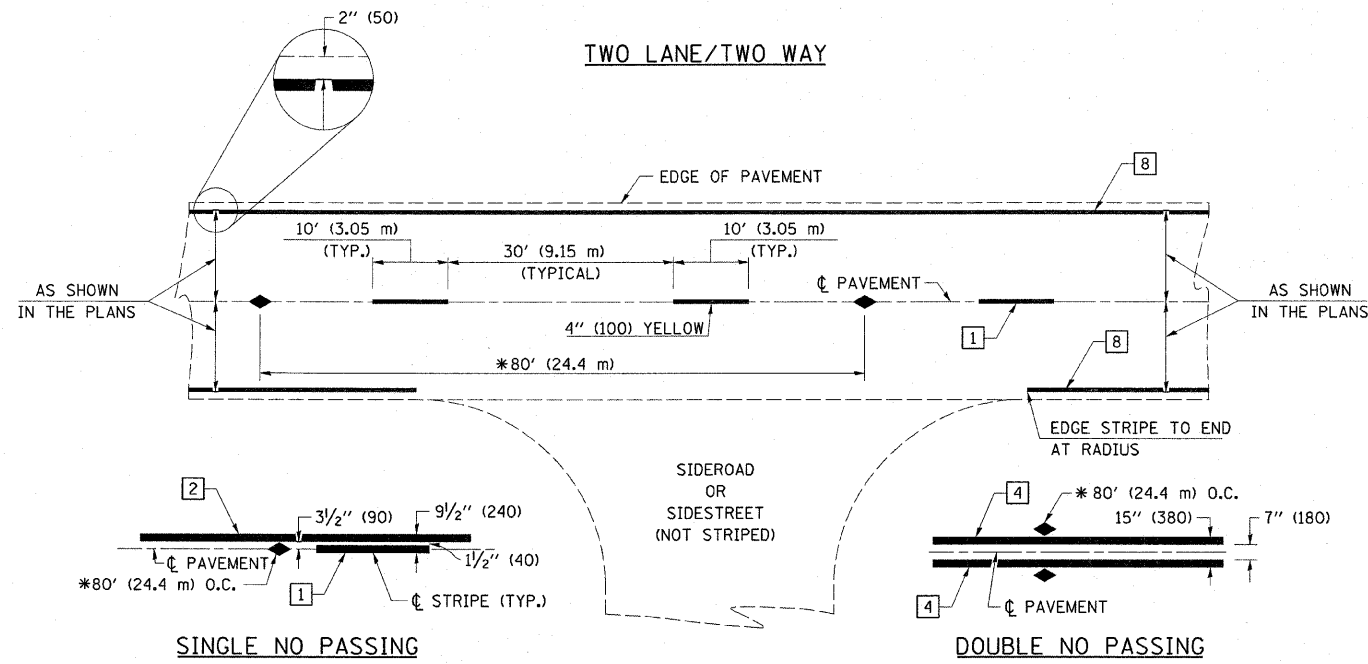
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STRIPING DETAILS

SCALE: 50 SHEET NO. 4 OF 4 SHEETS STA. 28+00 TO STA. 41+40.94

F.A.P. RTE.		SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
783		*	LAWRENCE	42	26
				CONTRACT NO. 74414	
ILLINOIS FED. AID PROJECT					

• (51-24-2,1-2)RS-1 & (1-X-1)RS-3



* REDUCE TO 40' (12.2 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEEDS OF 45 mph (70 km/h) OR LESS.

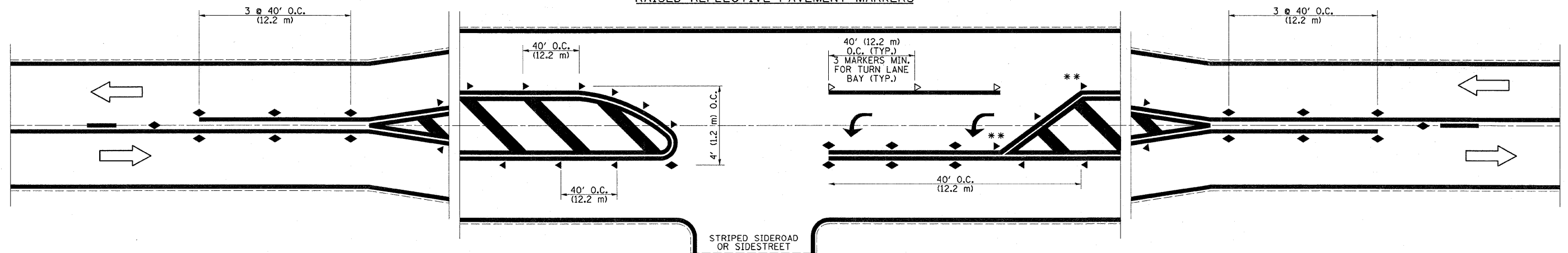
PAVEMENT MARKING LEGEND

- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 RESERVED
- 6 RESERVED
- 7 6" (150) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 6" (100) LANE LINE EXTENSIONS (WHITE)
- 14 4" (100) PARKING WHITE

TYPICAL PAVEMENT MARKERS LEGEND

- ◆ TWO-WAY AMBER MARKER
- ▶ ONE-WAY AMBER MARKER
- ▷ ONE-WAY CRYSTAL MARKER

RAISED REFLECTIVE PAVEMENT MARKERS

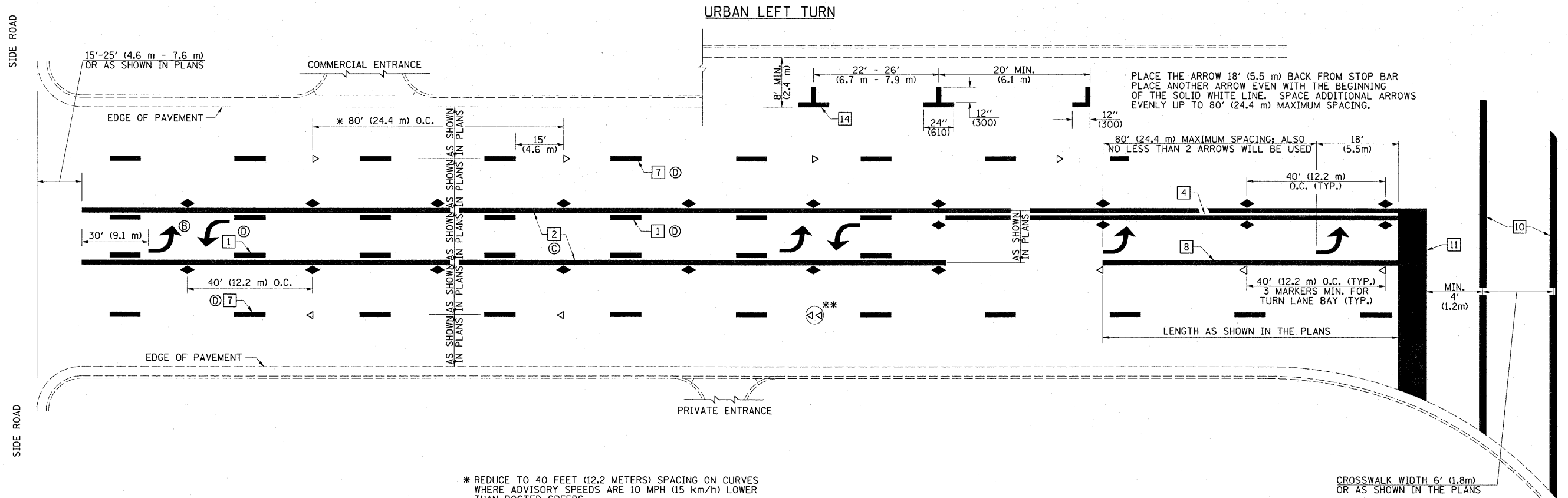


** REDUCE SPACING IF NECESSARY TO ASSURE MARKERS AT CORNER POINTS.

NOT TO SCALE
Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 7 DETAIL NO. 7800001

FILE NAME =	USER NAME = swartzrk	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS (RURAL & URBAN APPLICATIONS)			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cr:\pw\work\pwidot\swartzrk\d0148574\d77	414-sht-details.dgn	DRAWN -	REVISED -					783	.	LAWRENCE	42	27
PLOT SCALE = 20.0000" / IN.		CHECKED -	REVISED -					CONTRACT NO. 74414				
PLOT DATE = 1/29/2018		DATE -	REVISED -					ILLINOIS FED. AID PROJECT				
								SCALE: NA	SHEET NO. 1 OF 4 SHEETS	STA. TO STA.		



* REDUCE TO 40 FEET (12.2 METERS) SPACING ON CURVES WHERE ADVISORY SPEEDS ARE 10 MPH (15 km/h) LOWER THAN POSTED SPEEDS.

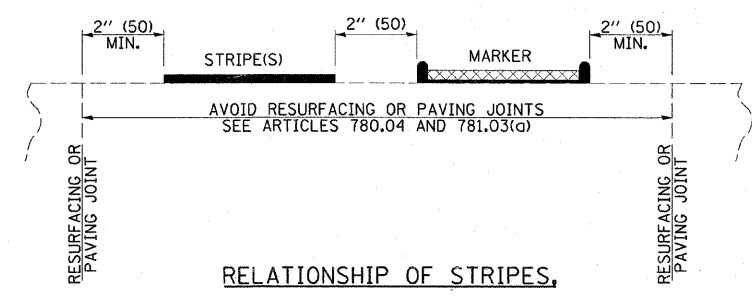
** DOUBLE LANE LINE MARKERS SHALL BE SPECIFIED AND SPACED AS SHOWN IN HIGHWAY STANDARD 781001 FOR MULTI-LANE DIVIDED AND UNDIVIDED HIGHWAYS.

PAVEMENT MARKING LEGEND

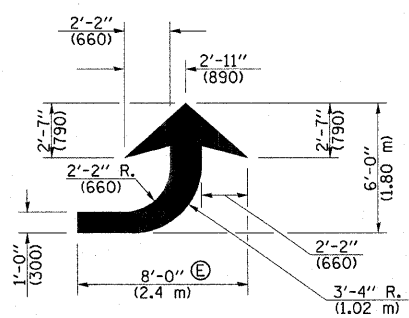
- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 RESERVED
- 6 RESERVED
- 7 6" (150) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 6" (100) LANE LINE EXTENSIONS (WHITE)
- 14 4" (100) PARKING WHITE

GENERAL NOTES

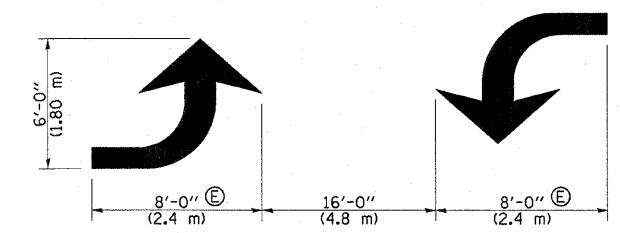
- (B) TURN ARROW PAIRS SHALL BE PLACED AT 250' (75 m) INTERVALS AND SHALL BE EVENLY SPACED BETWEEN BOTH ENDS OF THE BIDIRECTIONAL LEFT TURN LANE. USE A MINIMUM OF TWO PAIRS PER BLOCK.
- (C) THE SOLID YELLOW PAVEMENT MARKINGS (2) SHOULD GENERALLY START OR END NEAR THE RADIUS POINT OF EACH STREET RETURN EXCEPT WHERE ONE OR BOTH ENDS WOULD INCLUDE STOP BARS.
- (D) THE SKIP-DASH PAVEMENT MARKINGS (1) OR (7) SHOULD BE CENTERED BETWEEN BOTH ENDS OF EACH CITY BLOCK AND SHALL BE PLACED SO THEY LINE UP ACROSS FROM EACH OTHER.
- (E) USE LARGE ARROW SIZE FOR BOTH RURAL AND URBAN LOCATIONS. (SEE LAST PAGE OF SECTION 780x FOR SYMBOLS TABLE)



RELATIONSHIP OF STRIPES, MARKERS AND JOINTS



LEFT ARROW
REVERSE FOR RIGHT ARROW
AREA = 15.6 SQ. FT. (1.47 m²)
(WHITE)



TYPICAL DOUBLE TURN ARROWS (WHITE)

NOT TO SCALE

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 7 DETAIL NO. 7800001

FILE NAME =	USER NAME = swartzw	DESIGNED -	REVISED -
aspx_work\p\d01\swartzw\d0148574\d7741414-ahs-details.dgn		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

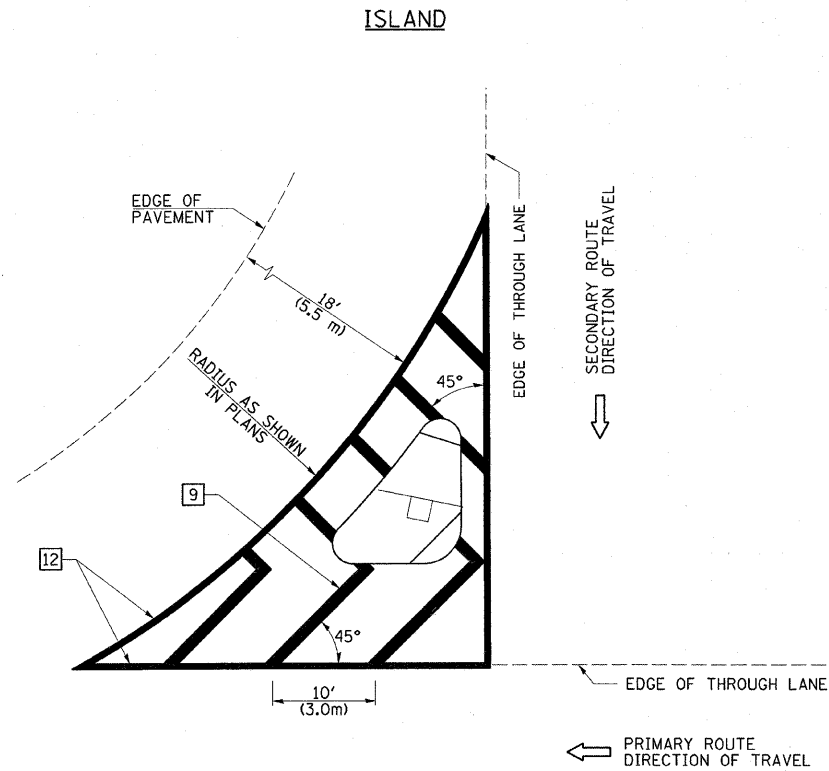
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS
(RURAL & URBAN APPLICATIONS)**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
783	*	LAWRENCE	42	28
CONTRACT NO. 74414				
ILLINOIS FED. AID PROJECT				

SCALE: NA SHEET NO. 2 OF 4 SHEETS STA. TO STA.

(51-24-2,1-2)RS-1 & (1-X)RS-3



GENERAL NOTES

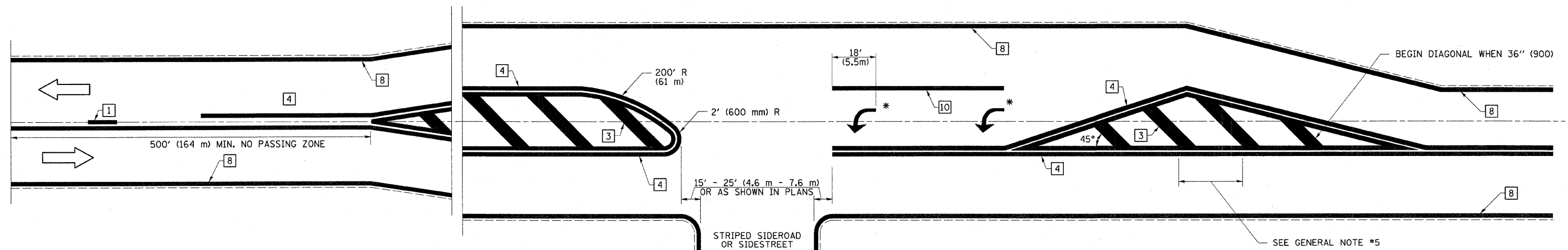
1. RAISED AND CORRUGATED MEDIANS SHALL BE OUTLINED WITH [2] IF PRESENT.
2. SOME OF THE INFORMATION INCLUDED WITH THIS DETAIL MAY NOT BE APPLICABLE TO THIS IMPROVEMENT.
3. PAVEMENT MARKINGS ARE TO BE EXTENDED THROUGH OMISSIONS WHEN APPLICABLE.
4. FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING ANY RAISED REFLECTIVE PAVEMENT MARKERS.
5. THE FOLLOWING CRITERIA SHALL BE USED FOR SELECTING THE DIAGONAL PAVEMENT MARKING SPACING:

< 30 MPH (<50 km/h)	15' (4.5 m)
30-45 MPH (50-75 km/h)	20' (6.0 m)
>45 MPH (>75 km/h)	30' (9.0 m)

PAVEMENT MARKING LEGEND

- [1] 4" (100) SKIP-DASH (YELLOW)
- [2] 4" (100) SOLID (YELLOW)
- [3] 12" (300) DIAGONAL (YELLOW)
- [4] 4" (100) DOUBLE YELLOW (NARROW)
- [5] RESERVED
- [6] RESERVED
- [7] 6" (150) SKIP-DASH (WHITE)
- [8] 4" (100) SOLID (WHITE)
- [9] 12" (300) DIAGONAL (WHITE)
- [10] 6" (150) SOLID (WHITE)
- [11] 24" (600) STOP BAR (WHITE)
- [12] 8" (200) SOLID (WHITE)
- [13] 6" (100) LANE LINE EXTENSIONS (WHITE)
- [14] 4" (100) PARKING WHITE

RURAL LEFT TURN STRIPING



* PLACE AN ARROW 18' (5.5 m) BACK FROM STOP BAR. PLACE ANOTHER ARROW EVEN WITH THE BEGINNING OF THE SOLID WHITE LINE. SPACE ADDITIONAL ARROWS EVENLY UP TO 80' (24.4 m) MAXIMUM SPACING. USE MINIMUM OF 2 ARROWS.

SEE GENERAL NOTE #5

NOT TO SCALE

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 7 DETAIL NO. 7800001

FILE NAME =	USER NAME = swartzrw	DESIGNED -	REVISED -
ca:\pw_work\pwwork\swartzrw\d0146574\14774	414-sht-details.dgn	DRAWN -	REVISED -
	PLOT SCALE = 20,000 / 1" IN.	CHECKED -	REVISED -
	PLOT DATE = 1/29/2010	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS
(RURAL & URBAN APPLICATIONS)**

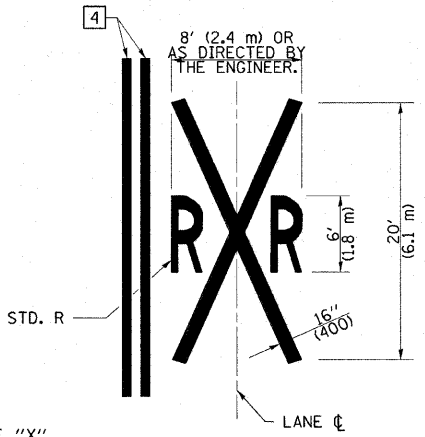
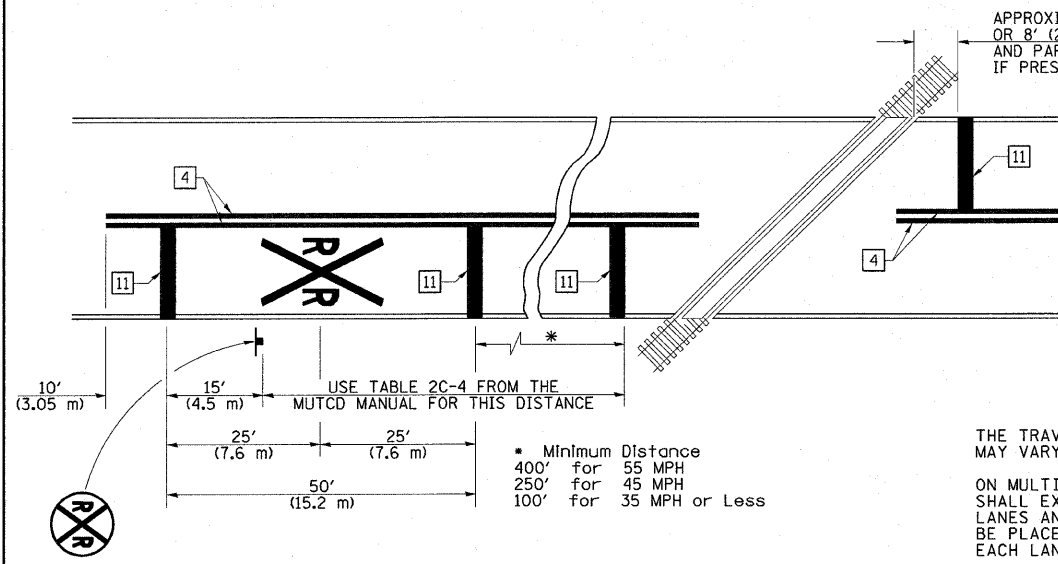
SCALE: NA SHEET NO. 3 OF 4 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
783	*	LAWRENCE	42	29
CONTRACT NO. 74414				
ILLINOIS FED. AID PROJECT				

(51-24-2.1-2)RS-1 & (1-X-1)RS-3

PAVEMENT MARKINGS AT RAILROAD-HIGHWAY GRADE CROSSING

PAVEMENT MARKING LEGEND



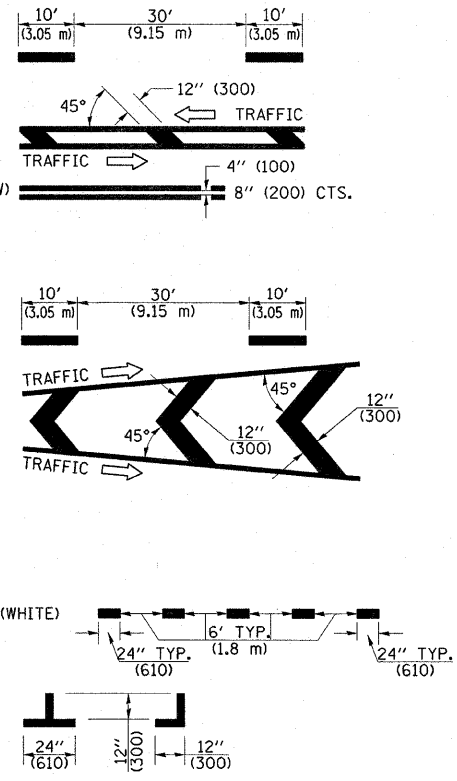
NOTES

THE TRAVERSE SPREAD OF THE "X" MAY VARY ACCORDING TO LANE WIDTH.

ON MULTI-LANE ROADS, THE STOP LINES SHALL EXTEND ACROSS ALL APPROACH LANES AND SEPARATE RXR SYMBOLS SHALL BE PLACED ADJACENT TO EACH OTHER IN EACH LANE.

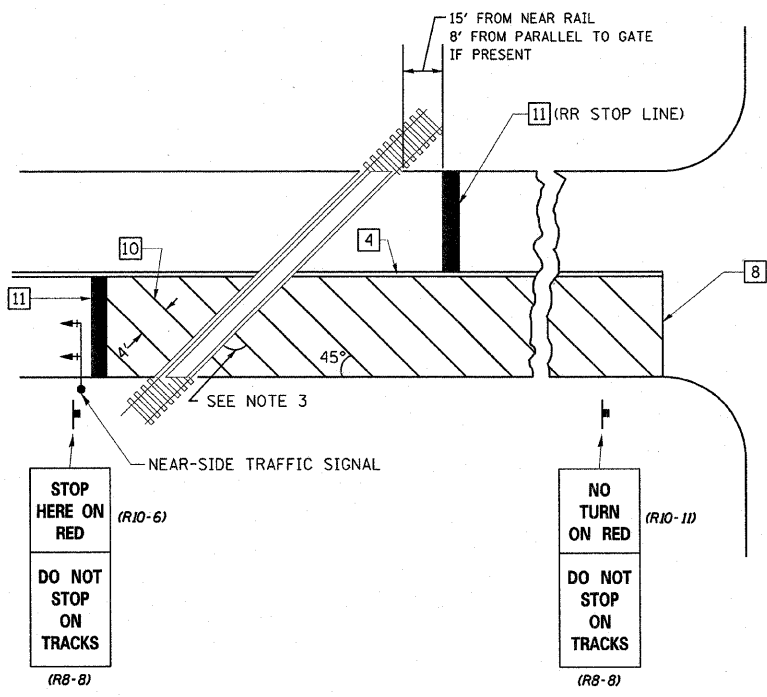
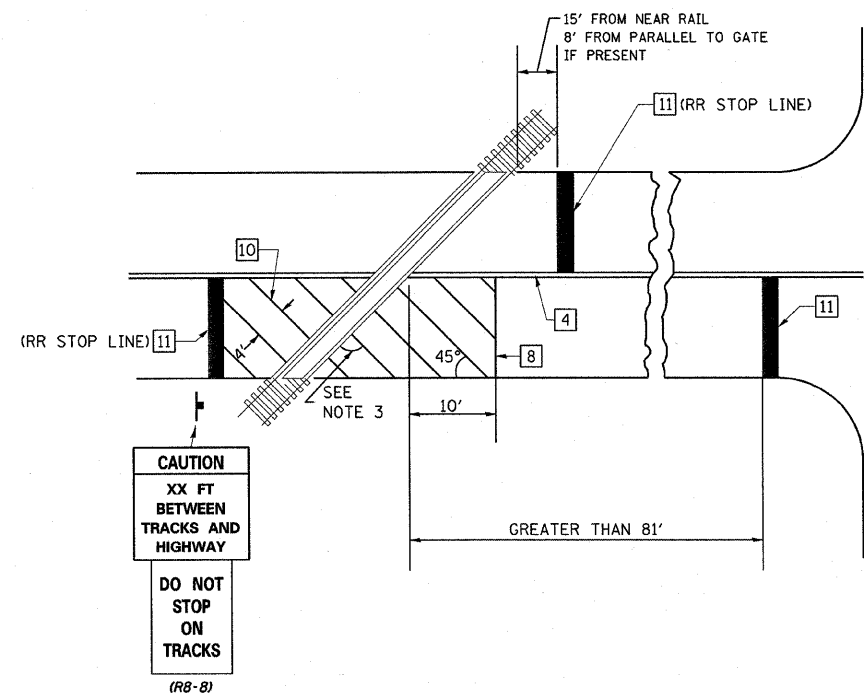
WHEN THE PAVEMENT MARKING SYMBOL IS USED, A PORTION OF THE SYMBOL SHOULD BE LOCATED DIRECTLY ADJACENT TO THE ADVANCE WARNING SIGN (W10-1) AS PLACED BY TABLE II-1, CONDITION B OF THE MUTCD.

- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 RESERVED
- 6 RESERVED
- 7 6" (150) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 6" (100) LANE LINE EXTENSIONS (WHITE)
- 14 4" (100) PARKING WHITE



RAILROAD CROSSING WITH INTERCONNECT ONLY

RAILROAD CROSSING WITH INTERCONNECT AND PRE-SIGNALS



GENERAL NOTES

1. SUPPLEMENTAL PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
2. EXTEND PAVEMENT MARKINGS TO THE INTERSECTION ONLY WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED.
3. WHERE THE ANGLE BETWEEN THE DIAGONAL PAVEMENT MARKINGS AND THE TRACK WOULD BE LESS THAN 20°, THE PAVEMENT MARKINGS SHOULD BE PLACED IN THE OPPOSITE DIRECTION FROM THAT SHOWN.

SUPPLEMENTAL PAVEMENT MARKING TREATMENT FOR RAILROAD-HIGHWAY GRADE CROSSING

NOT TO SCALE

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

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	PLOT DATE = 1/29/2010	DATE -	REVISED -

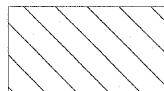
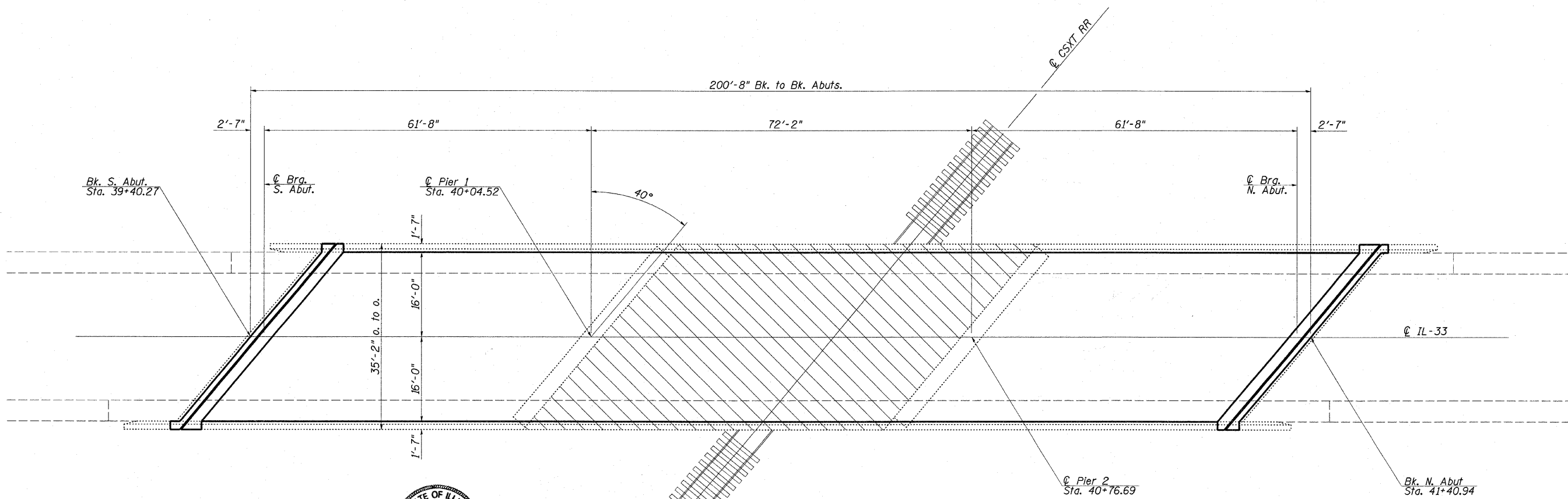
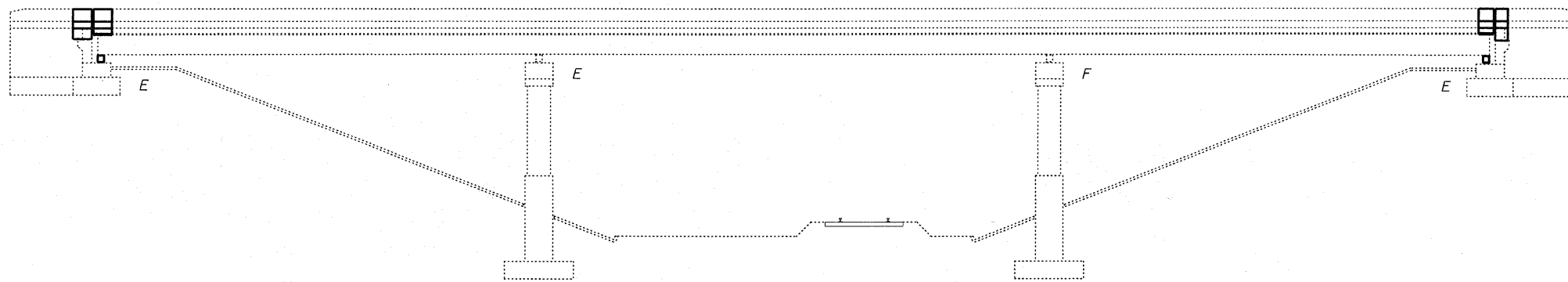
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS
(RURAL & URBAN APPLICATIONS)

DISTRICT 7 DETAIL NO. 7800001			
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS
783		LAWRENCE	42
			30
			CONTRACT NO. 74414
ILLINOIS FED. AID PROJECT			

• (S1-24-2.1-2)RS-1 & (1-X-1)RS-3

The existing three span steel continuous multi-beam structure was constructed in 1961 as section 1-2VB at station 40+40.6 carrying IL-33 over the CSXT RR. Structure rehabilitation was completed in 1992 and consisted of the placement of a new deck. The proposed project consists of replacement of the existing expansion joints, new bearings at the abutments, new wearing surface and formed concrete repair.



Protective Shield



Expires 11/30/2010

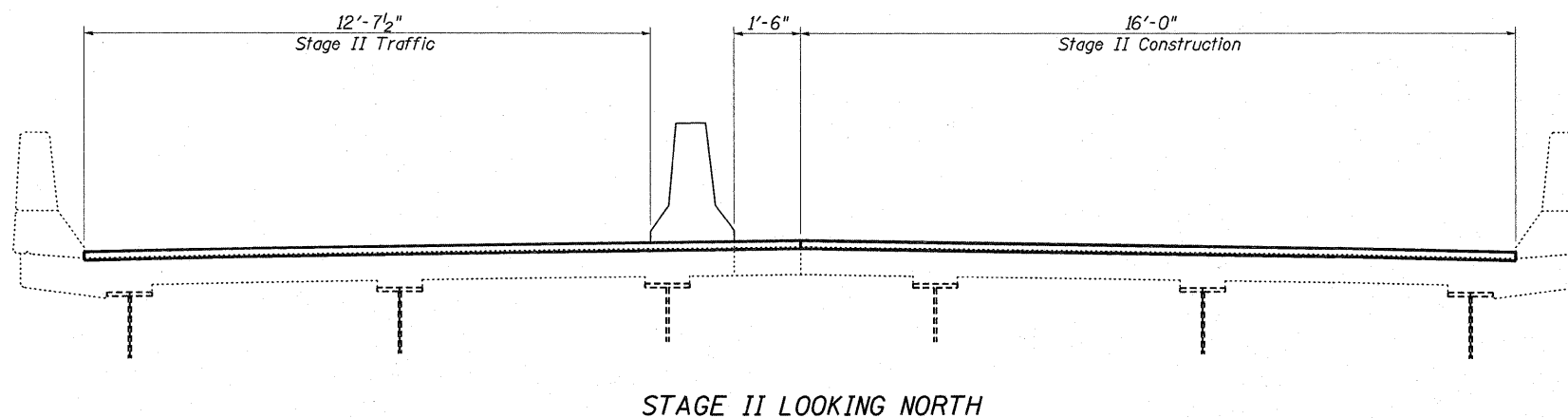
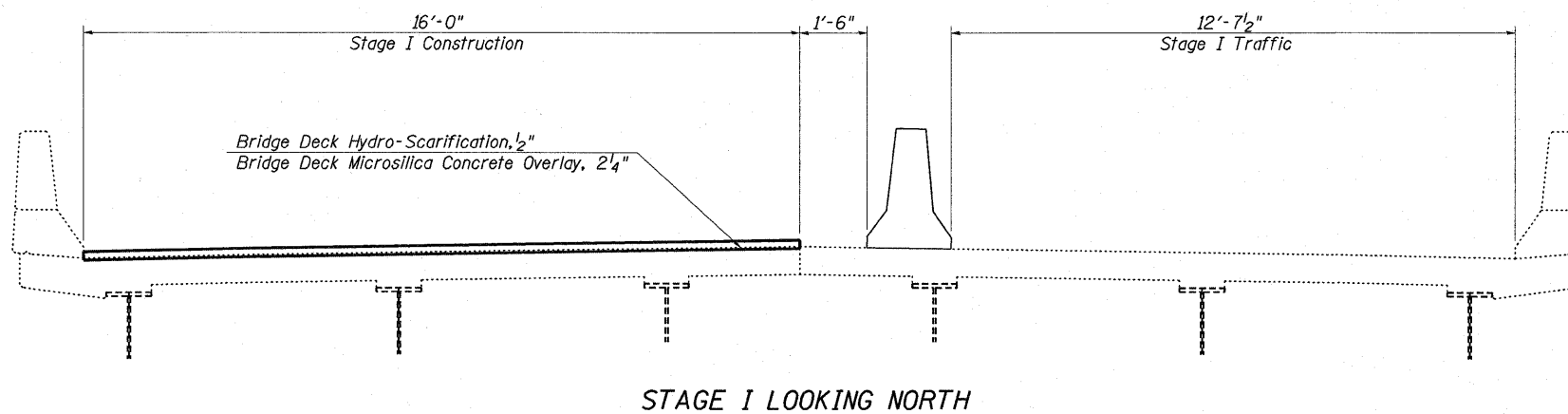
FILE NAME = c:\pwork\pwork\SWARTZRW\08148563\07	USER NAME = swartzrw 4414-brplnprf-0510032.dgn	DESIGNED - MEA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL PLAN & ELEVATION S.N. 051-0032		F.A.P. RTE. = 783	SECTION =	COUNTY = LAWRENCE	TOTAL SHEETS = 42	SHEET NO. = 31	
PLOT SCALE = 20,000 1 / IN.		CHECKED -	REVISED -		SCALE: NA	SHEET NO. 1 OF 4 SHEETS	STA. TO STA.	CONTRACT NO. 74414				
PLOT DATE = 1/29/2010		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							
(51-24-2.1-2)RS-1&(1-X-1)RS-3												

GENERAL NOTES

All structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted.
 Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost Included with Concrete Removal.
 Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50° F.
 Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.
 The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the lead on this project.
 Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.
 Reinforcement bars designated (E) shall be epoxy coated.
 Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work; however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
 Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the Special Provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures".
 All structural steel shall be shop painted with the inorganic zinc rich primer per AASHTO M300, Type 1. Cost included with Furnishing and Erecting Structural Steel.

TOTAL BILL OF MATERIALS

ITEM DESCRIPTION	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	18.1
Concrete Superstructure	Cu. Yd.	20
Reinforcement Bars, Epoxy Coated	Pound	1310
Bar Splicers	Each	24
Elastomeric Bearing Assembly Type I	Each	6
Elastomeric Bearing Assembly Type II	Each	6
Furnishing and Erecting Structural Steel	Pound	1680
Jack and Remove Existing Bearings	Each	12
Protective Shield	Sq. Yd.	267
Bridge Deck Hydro-Scarification, 1/2 "	Sq. Yd.	685
Bridge Deck Microsilica Concrete Overlay, 2 1/4 "	Sq. Yd.	685
Bridge Deck Grooving	Sq. Yd.	642
Protective Coat	Sq. Yd.	712
Deck Slab Repair (Full Depth Type I)	Sq. Yd.	3
Deck Slab Repair (Full Depth Type II)	Sq. Yd.	8
Preformed Joint Strip Seal	Foot	92
Structural Repair of Concrete <5"	Sq. Ft.	47
Anchor Bolts 1" ϕ	Each	24



(51-24-2J-2RS-1&(1-X-1)RS-3

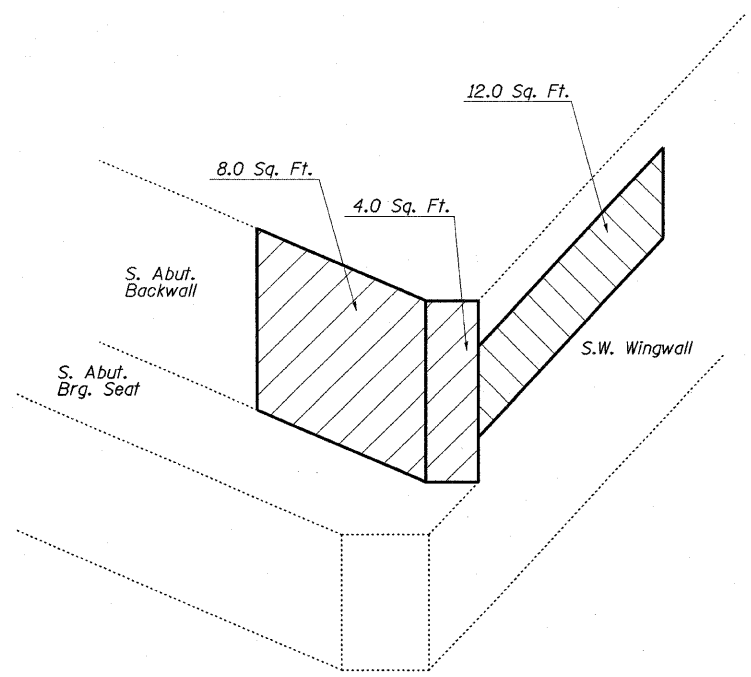
FILE NAME =	USER NAME = swartzw	DESIGNED - MEA	REVISED -
er\pw\work\NPWIDOT\SWARTZRW\d0148563\07	4414-birgenno tas-0510032.dgn	DRAWN - MEA	REVISED -
	PLOT SCALE = 20.0000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 1/29/2010	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

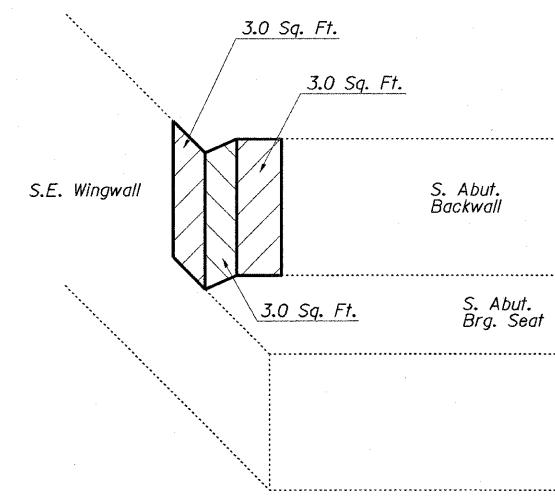
**GENERAL NOTES, BILL OF MATERIALS, STAGING PLAN
S.N. 051-0032**

SCALE: NA SHEET NO. 2 OF 4 SHEETS STA. TO STA.

F.A.P. RTE. 783	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		LAWRENCE	42	32
			CONTRACT NO. 74414	
ILLINOIS FED. AID PROJECT				

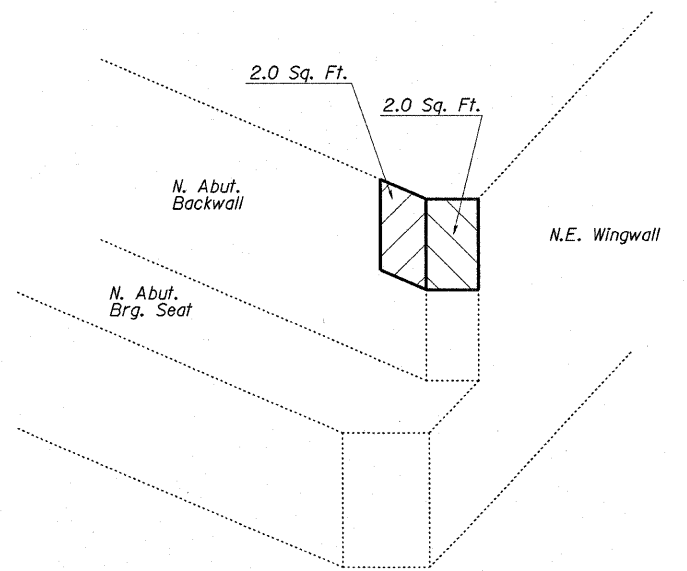


South Abut., West End

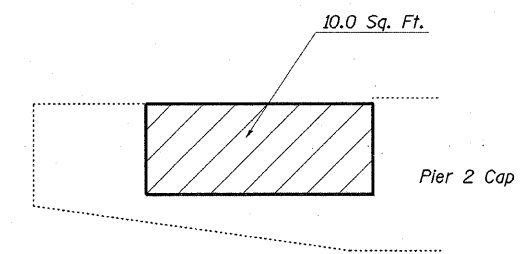


South Abut., East End

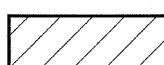
Note: Quantities are estimated. Actual quantities to be determined by the Resident Engineer.



North Abut., East End



Pier 2, East End, North Face

 Structural Repair of Concrete (Depth < 5")

FILE NAME =	USER NAME = swartzw	DESIGNED - MEA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STRUCTURAL REPAIR OF CONCRETE < 5" S.N. 051-0032	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pw\work\PW1001\SWARTZRW\d0140563\07	4414-brdetails-0510032.dgn	DRAWN - MEA	REVISED -			783	*	LAWRENCE	42	33	
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	PLOT DATE = 1/29/2010	DATE -	REVISED -			SCALE: NA	SHEET NO. 3 OF 4 SHEETS	STA.	TO STA.		

• (51-24-2,1-2)RS-1&(1-X-1)RS-3

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NOTES

All structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50° F.

Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.

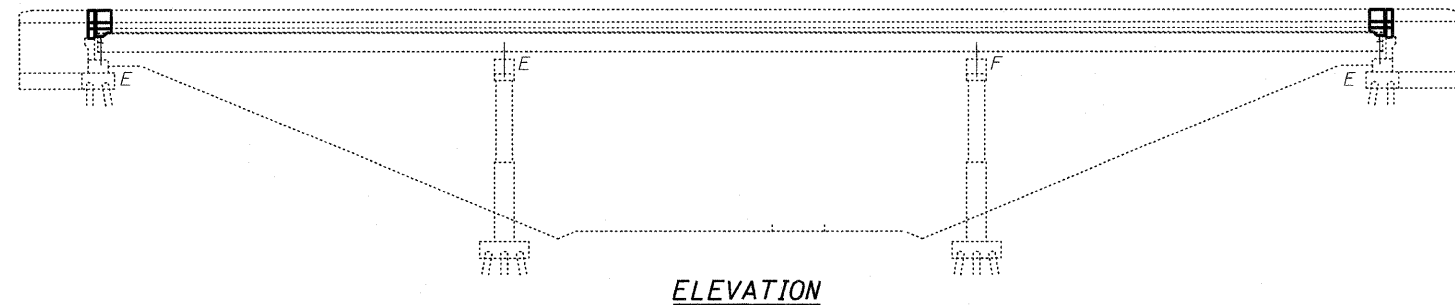
Reinforcement bars designated (E) shall be epoxy coated.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

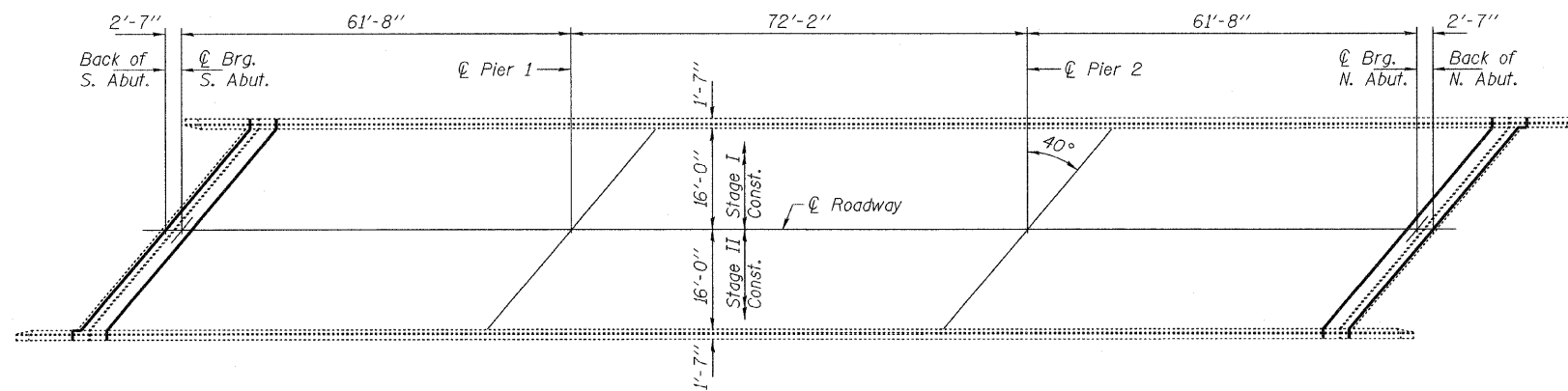
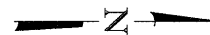
Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the Special Provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures".

All structural steel shall be shop painted with the inorganic zinc rich primer per AASHTO M300, Type 1. Cost included with Furnishing and Erecting Structural Steel.

Cost of removal and re-installation of all members necessary to complete the work as detailed on the plans and as specified in the Special Provisions shall be included with Furnishing and Erecting Structural Steel.



ELEVATION



PLAN

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	18.1
Concrete Superstructure	Cu. Yd.	20.0
Elastomeric Bearing Assembly, Type I	Each	6
Elastomeric Bearing Assembly, Type II	Each	6
Furnishing and Erecting Structural Steel	Pound	1680
Jack and Remove Existing Bearings	Each	12
Reinforcement Bars, Epoxy Coated	Pound	1310
Bar Splicers	Each	24
Preformed Joint Strip Seal	Foot	92
Anchor Bolts 1"φ	Each	24

**PLAN AND ELEVATION
IL 33 OVER CSXT RAILROAD
SN 051-0032**

DESIGNED	<i>Jim J. Foley</i>
CHECKED	<i>W. J. ...</i>
DRAWN	baliva
CHECKED	IJL <i>WR</i>

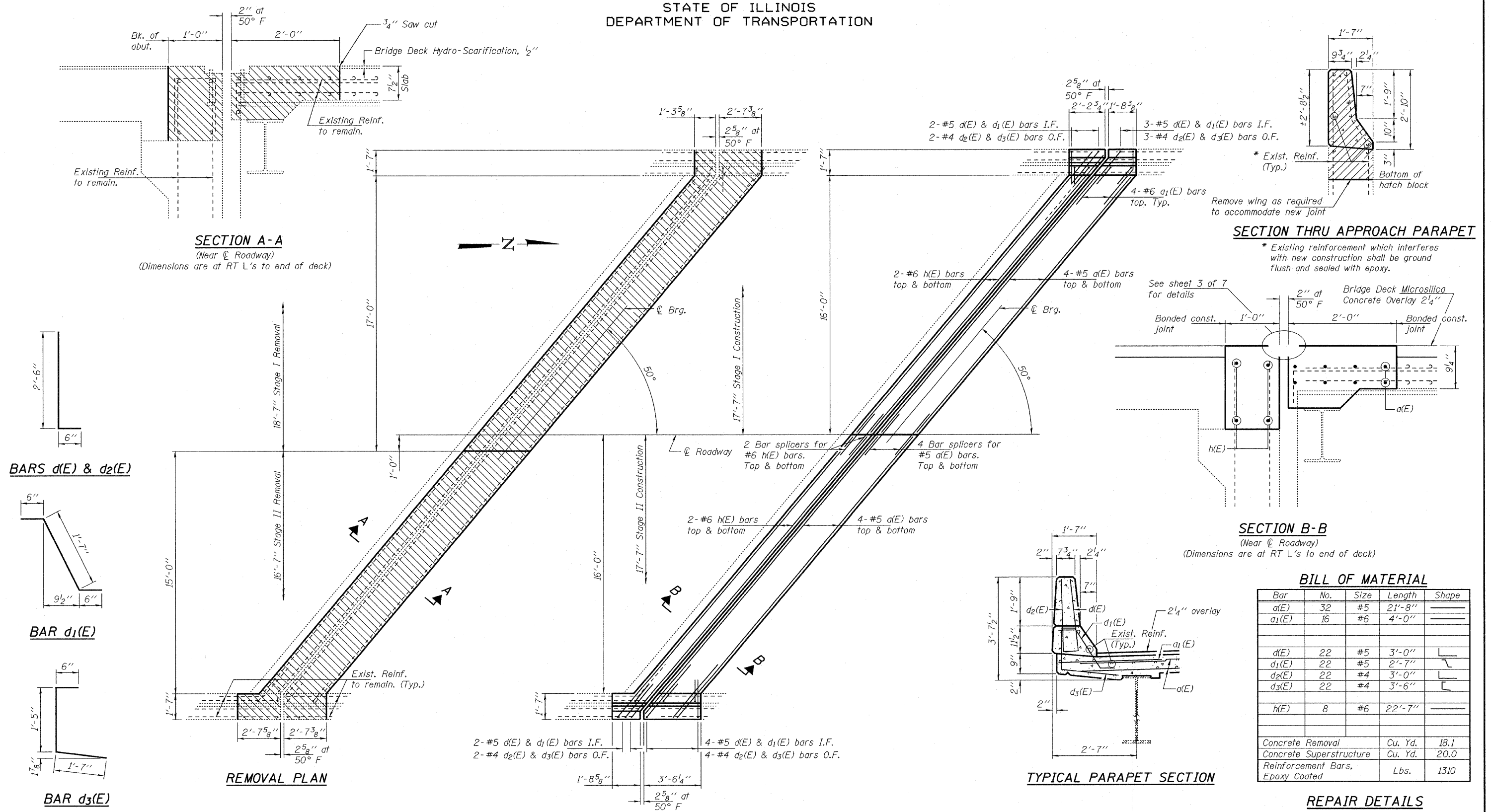
EXAMINED	MARCH 3, 2010
PASSED	<i>Ralph E. Anderson</i>



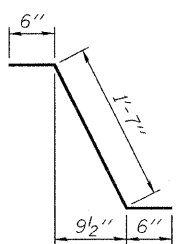
EXPIRES 11-30-2010

SHEET NO. 1 7 SHEETS	F.A.P. RTE. 783	SECTION (51-24-2.1-2)RS-1 &(1-X-1)RS-3	COUNTY LAWRENCE	TOTAL SHEETS 42	SHEET NO. 35
	CONTRACT NO. 74414				
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT		

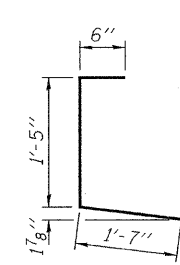
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



BARS d(E) & d2(E)



BAR d1(E)



BAR d3(E)

DESIGNED	IJL
CHECKED	GGE
DRAWN	baliva
CHECKED	IJL GGE

MARCH 3, 2010
 EXAMINED *Carl P. ...*
 PASSED *Ralph E. Anderson*
 ENGINEER OF STRUCTURAL SERVICES
 ENGINEER OF BRIDGES AND STRUCTURES

South abutment shown.
North abutment similar by rotation.

REPLACEMENT PLAN

SECTION THRU APPROACH PARAPET

* Existing reinforcement which interferes with new construction shall be ground flush and sealed with epoxy.

SECTION B-B
(Near ϕ Roadway)

(Dimensions are at RT L's to end of deck)

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d(E)	32	#5	21'-8"	
a1(E)	16	#6	4'-0"	
d(E)	22	#5	3'-0"	L
d1(E)	22	#5	2'-7"	L
d2(E)	22	#4	3'-0"	L
d3(E)	22	#4	3'-6"	L
h(E)	8	#6	22'-7"	
Concrete Removal			Cu. Yd.	18.1
Concrete Superstructure			Cu. Yd.	20.0
Reinforcement Bars, Epoxy Coated			Lbs.	1310

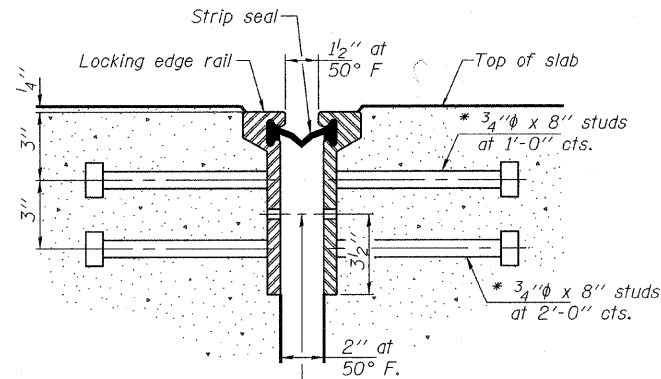
REPAIR DETAILS
IL 33 OVER CSXT RAILROAD
SN 051-0032

Note:
Hatched areas indicate removal.

SHEET NO. 2 7 SHEETS	F.A.P. RTE. 783	SECTION (51-24-2,1-2)RS-1 &(1-X-1)RS-3	COUNTY LAWRENCE	TOTAL SHEETS 42	SHEET NO. 36
	CONTRACT NO. 74414			ILLINOIS FED. AID PROJECT	

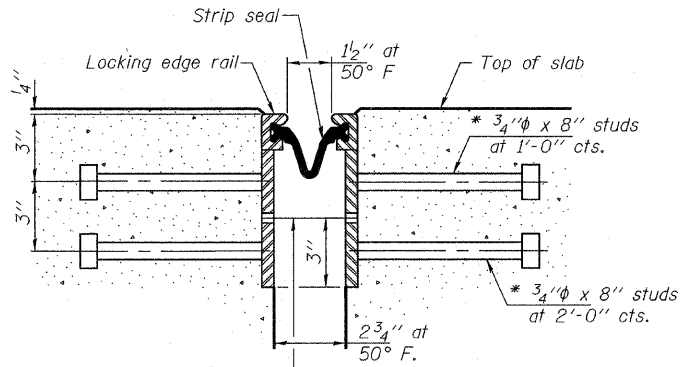
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



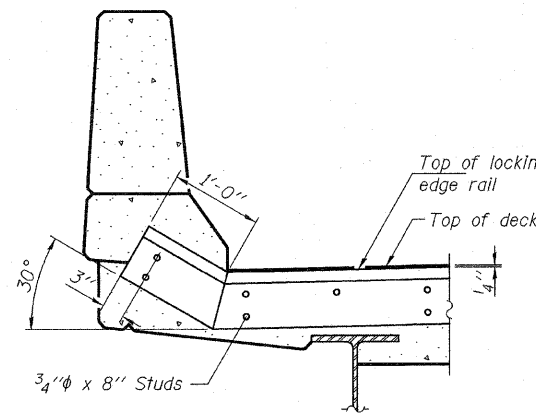
7/16" φ holes at 4'-0" cts. for 3/8" φ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

SECTION THRU
ROLLED RAIL JOINT

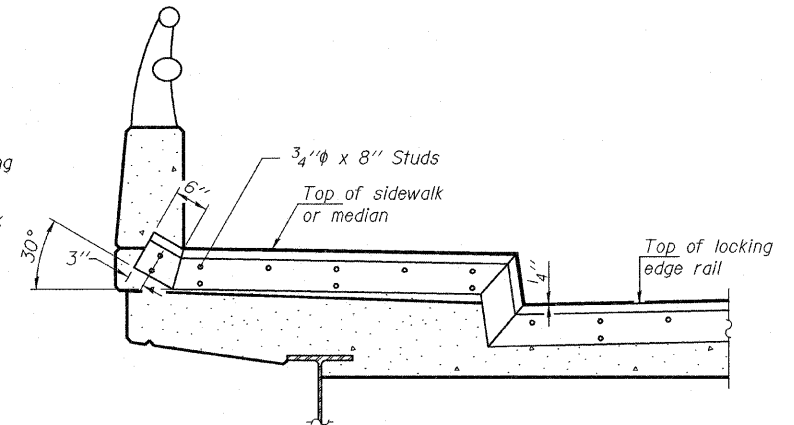


7/16" φ holes at 4'-0" cts. for 3/8" φ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

SECTION THRU
WELDED RAIL JOINT



AT PARAPET
See Section A-A for end treatment of skews > 30°.

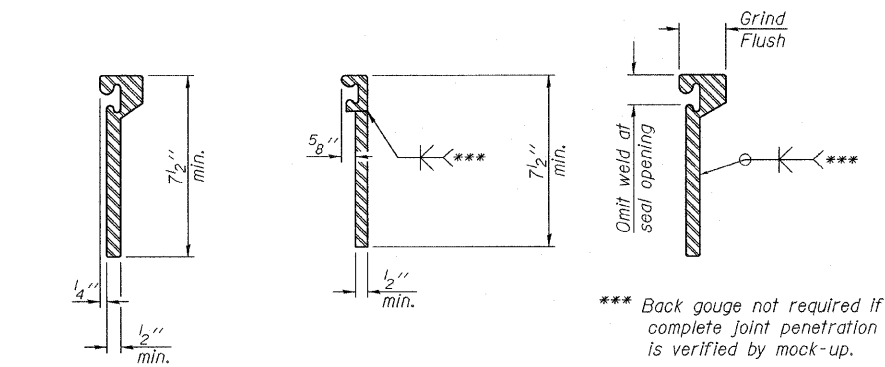


AT SIDEWALK OR MEDIAN
Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.

TYPICAL END TREATMENTS

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches. The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities. The manufacturer's recommended installation methods shall be followed. The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State. All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications. Maximum space between rail segments at stage lines shall be 3/16", sealed with a suitable sealant.

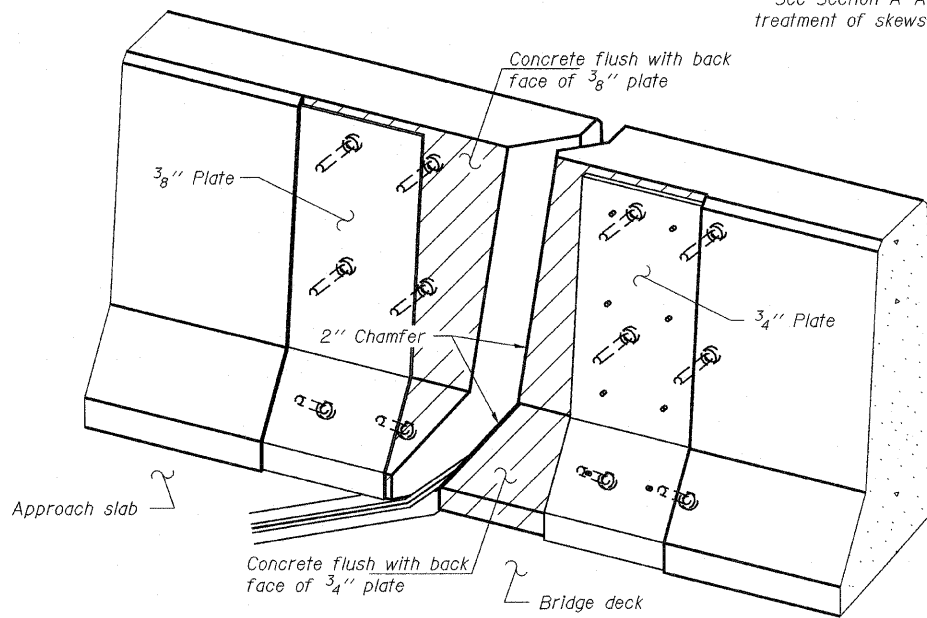


ROLLED
EXTRUDED RAIL

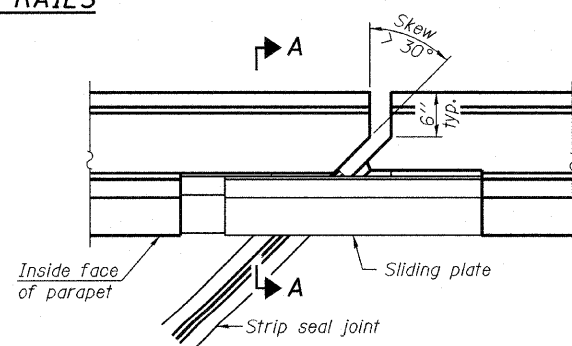
WELDED RAIL

LOCKING EDGE
RAIL SPLICE

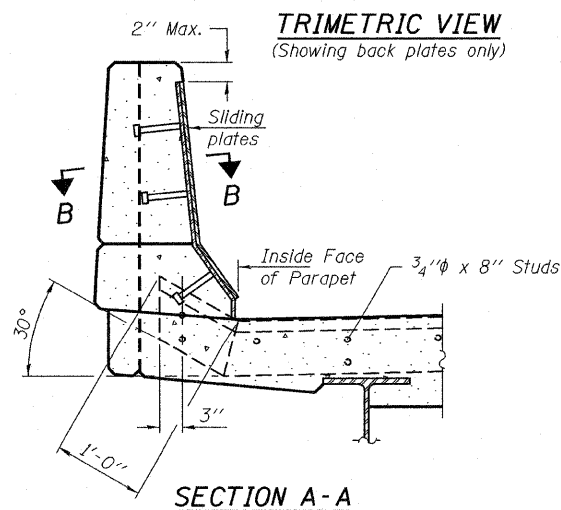
The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.



LOCKING EDGE RAILS

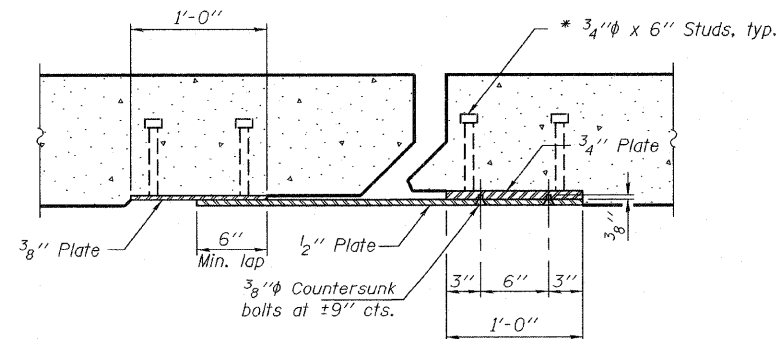


PLAN



SECTION A-A

POINT BLOCK DETAILS
(for skews > 30°)



SECTION B-B

BILL OF MATERIAL

Item	Unit	Total
Prefomed Joint Strip Seal	Foot	92

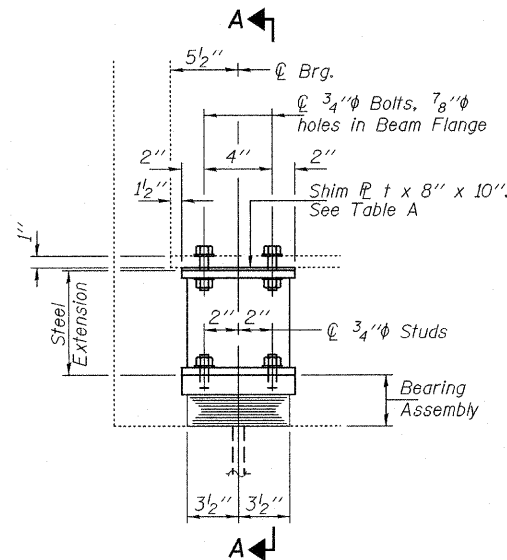
PREFORMED JOINT STRIP SEAL
IL 33 OVER CSXT RAILROAD
SN 051-0032

DESIGNED	IJL
CHECKED	GGE
DRAWN	balva
CHECKED	IJL GGE

MARCH 3, 2010
EXAMINED *Carl P. ...*
PASSED *Ralph E. Anderson*
ENGINEER OF STRUCTURAL SERVICES
ENGINEER OF BRIDGES AND STRUCTURES

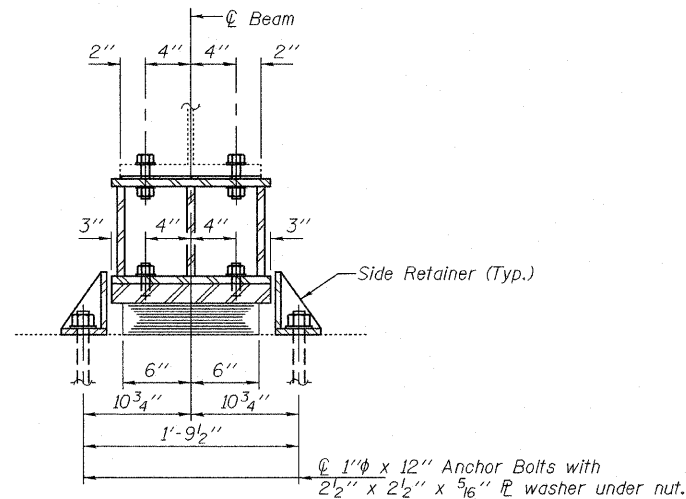
SHEET NO. 3	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
7 SHEETS	783	(51-24-2,1-2)RS-1&(1-X-1)RS-3	LAWRENCE	42	37
			CONTRACT NO. 74414		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



ELEVATION AT N. ABUTMENT

TYPE I ELASTOMERIC EXP. BRG.

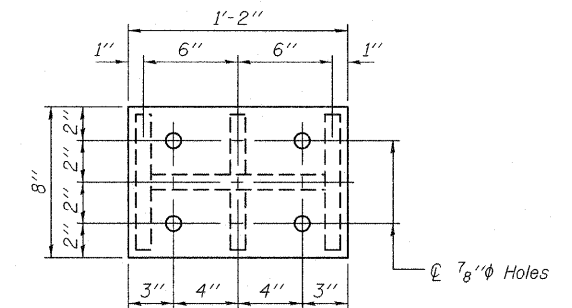


SECTION A-A

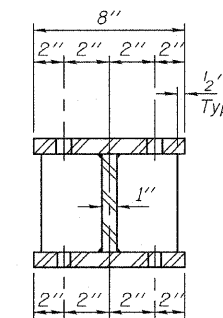
BEAM REACTIONS

RP	(K)	26.6
R _L	(K)	32.2
Imp.	(K)	8.5
R (Total)	(K)	67.3

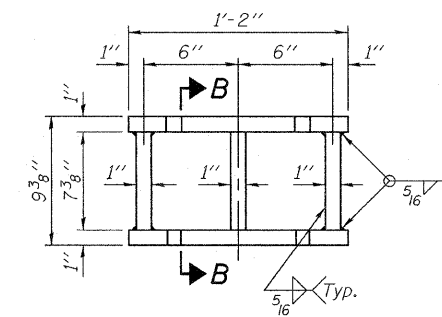
Notes:
Diaphragm removal and reinstallation may be required to facilitate drilling holes. Cost included with Furnishing and Erecting Structural Steel.
New steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel.
Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions. Min. jack capacity = 36Tons.
Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.
Side retainers shall be included in the cost of Elastomeric Bearing Assembly, Type I.



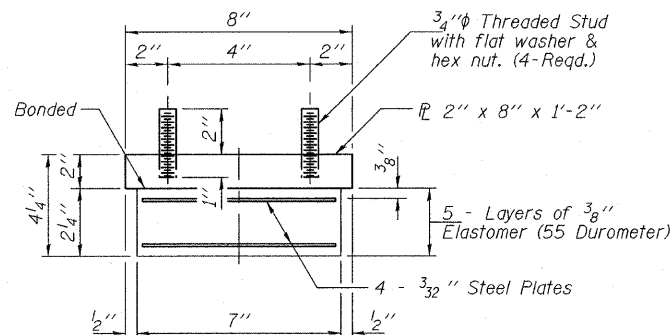
PLAN TOP AND BOTTOM PLATE



SECTION B-B



STEEL EXTENSION DETAIL

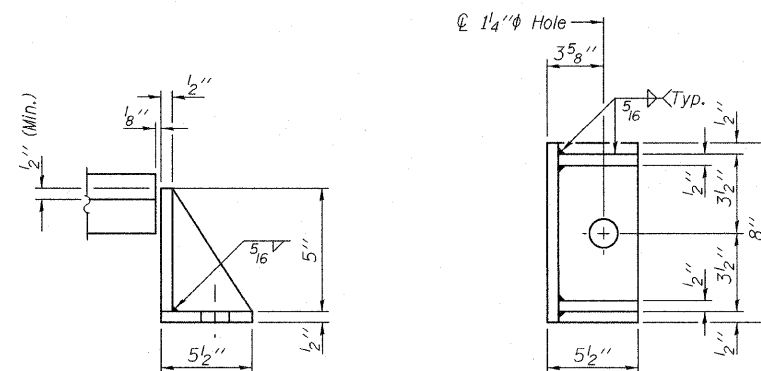


BEARING ASSEMBLY

Note:
Shim plates shall not be placed under Bearing Assembly.

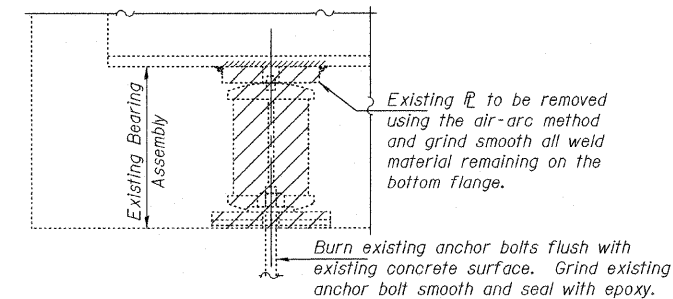
TABLE A

Beam	t
1	0
2	0
3	0
4	1/4"
5	3/4"
6	1 1/4"



SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



EXISTING BEARING REMOVAL DETAIL

Cost included with Jack and Remove Existing Bearings.

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type I	Each	6
Jack and Remove Existing Bearings	Each	6
Furnishing and Erecting Structural Steel	Pound	860
Anchor Bolts 1"φ	Each	12

NORTH ABUTMENT
BEARING REPLACEMENT DETAILS
IL 33 OVER CSXT RAILROAD
SN 051-0032

DESIGNED	IJL
CHECKED	GGE
DRAWN	baliva
CHECKED	IJL GGE

MARCH 3, 2010
EXAMINED *Carl P. ...*
PASSED *Ralph E. Anderson*

TYI/REPS 12-03-2008

SHEET NO. 4 7 SHEETS	F.A.P. RTE. 783	SECTION (51-24-2,1-2)RS- 1&(1-X-1)RS-3	COUNTY LAWRENCE	TOTAL SHEETS 42	SHEET NO. 38
	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 74414	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BEAM REACTIONS

RP	(K)	26.6
Rt	(K)	32.2
Imp.	(K)	8.5
R (Total)	(K)	67.3

TABLE A

Beam	t
1	1 1/4"
2	3/4"
3	1/4"
4	0
5	0
6	0

Notes:

Diaphragm removal and reinstallation may be required to facilitate drilling holes. Cost included with Furnishing and Erecting Structural Steel.

New steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel. Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions. Min. jack capacity = 36Tons.

Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.

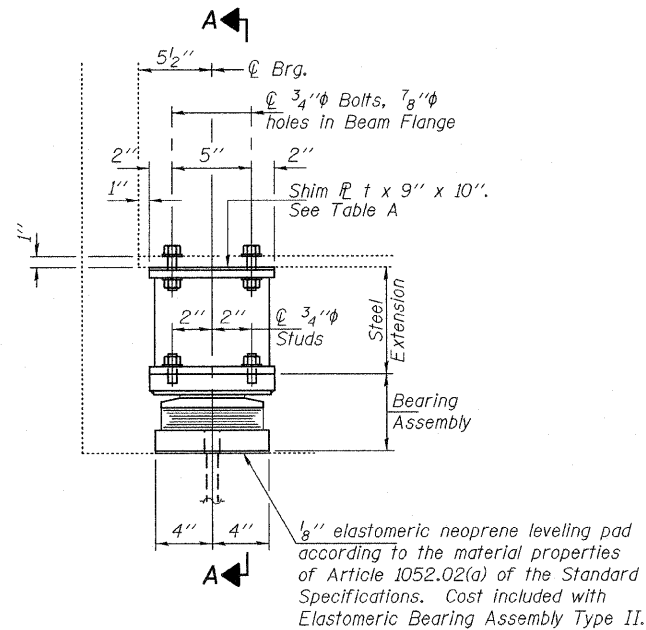
Anchor bolts for Type II bearings shall be placed in holes drilled through the bottom bearing plate after members are in place. Side retainers shall be placed after bolts are installed.

Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.

Side retainers shall be included in the cost of Elastomeric Bearing Assembly, Type II.

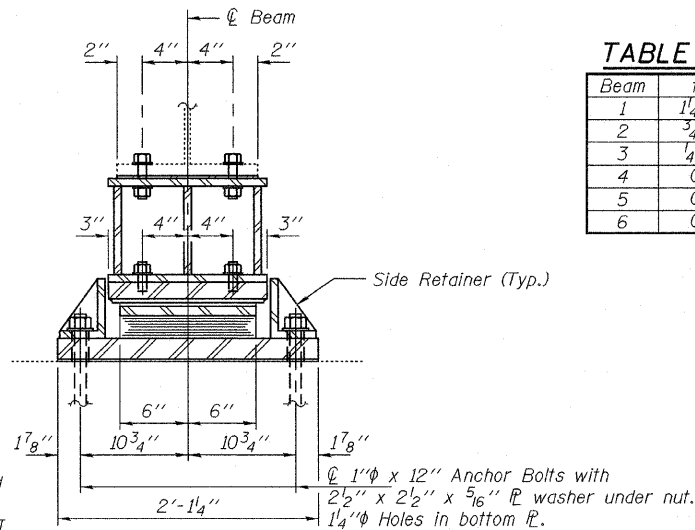
The 1/8" PTFE sheet shall be bonded directly to the top steel plate with a two-component, medium viscosity epoxy resin, conforming to the requirements of the Federal Specification MMM-A-134, Type I. The bond agent shall be applied on the full area of the contact surfaces.

Bonding of 1/8" PTFE sheet during vulcanizing process will be permitted provided the process and method of adjusting assembly height is approved by the Engineer.

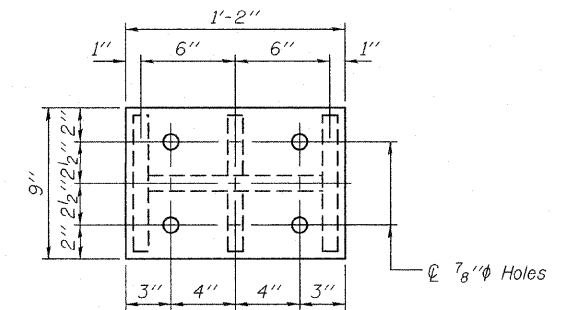


ELEVATION AT S. ABUTMENT

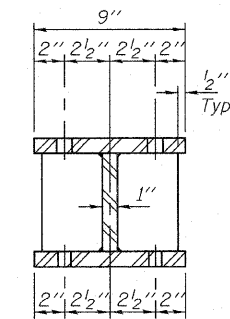
TYPE II TFE ELASTOMERIC EXP. BRG.



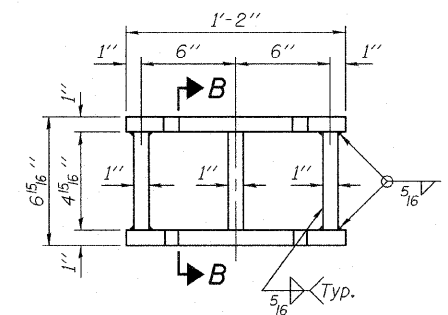
SECTION A-A



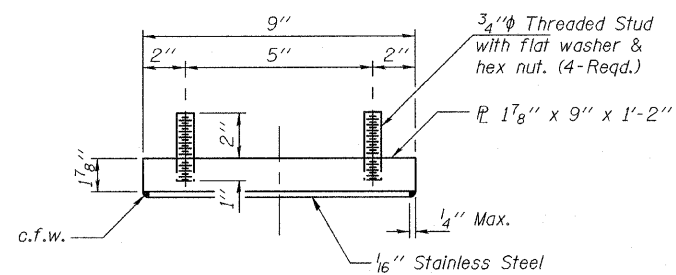
PLAN TOP AND BOTTOM PLATE



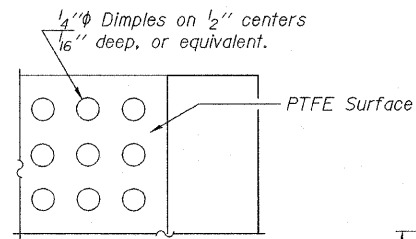
SECTION B-B



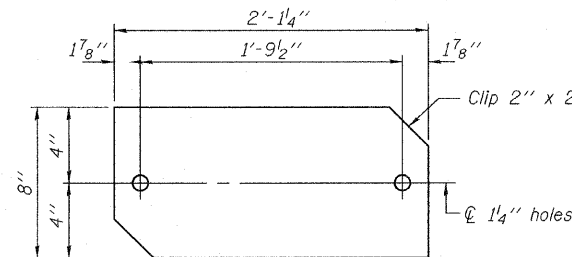
STEEL EXTENSION DETAIL



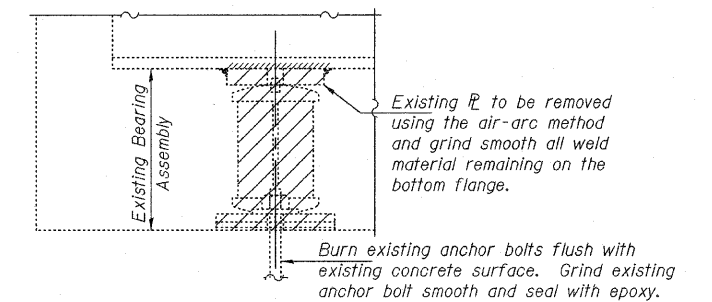
TOP BEARING ASSEMBLY



PLAN-PTFE SURFACE

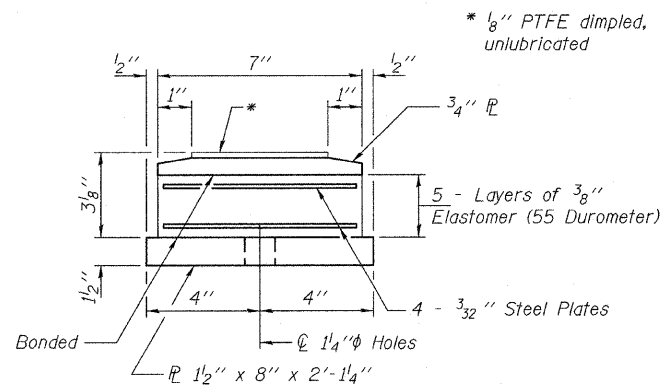


PLAN BOTTOM BEARING PLATE

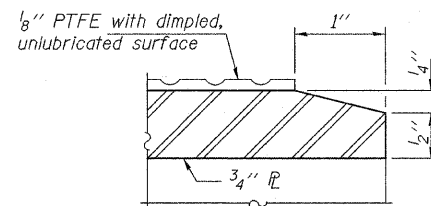


EXISTING BEARING REMOVAL DETAIL

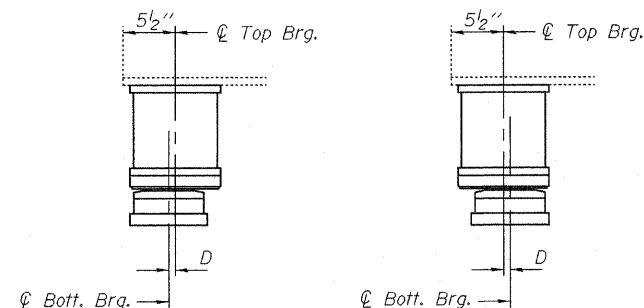
Cost included with Jack and Remove Existing Bearings.



BOTTOM BEARING ASSEMBLY



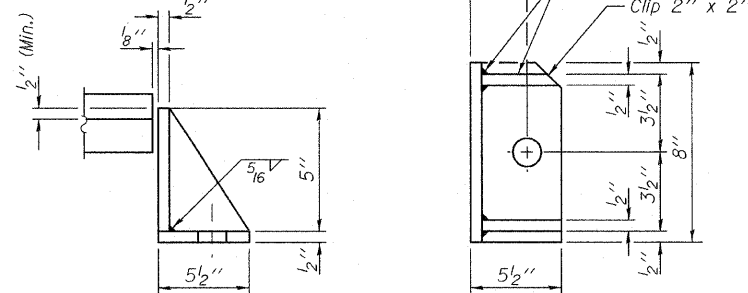
SECTION THRU PTFE



BELOW 50° F. (Move bott. brg. away from fixed brg.)
ABOVE 50° F. (Move bott. brg. toward fixed brg.)

SETTING ANCHOR BOLTS AT EXP. BRG.

D = 1/8" per each 100' of expansion for every 15° temp. change from the normal temp. of 50°F.



SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type II	Each	6
Jack and Remove Existing Bearings	Each	6
Furnishing and Erecting Structural Steel	Pound	820
Anchor Bolts 1"φ	Each	12

SOUTH ABUTMENT
BEARING REPLACEMENT DETAILS

IL 33 OVER CSXT RAILROAD
SN 051-0032

DESIGNED	IJL
CHECKED	GGE
DRAWN	baliva
CHECKED	IJL GGE

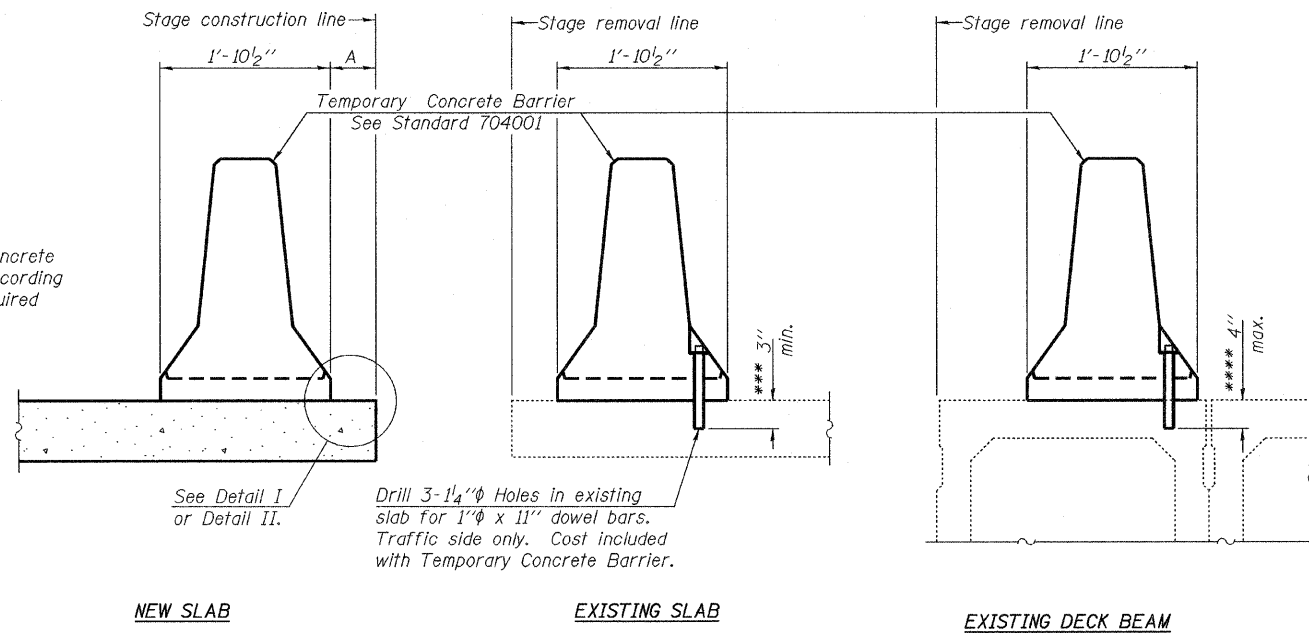
EXAMINED	<i>A. Carl Powers</i> ENGINEER OF STRUCTURAL SERVICES
PASSED	<i>Ralph E. Anderson</i> ENGINEER OF BRIDGES AND STRUCTURES

TYII/REPS 12-03-2008

SHEET NO. 5 7 SHEETS	F.A.P. RTE. 783	SECTION (51-24-2.1-2)RS- 1&(1-X-1)RS-3	COUNTY LAWRENCE	TOTAL SHEETS 42	SHEET NO. 39
	CONTRACT NO. 74414			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".



SECTIONS THRU SLAB OR DECK BEAM

NOTES

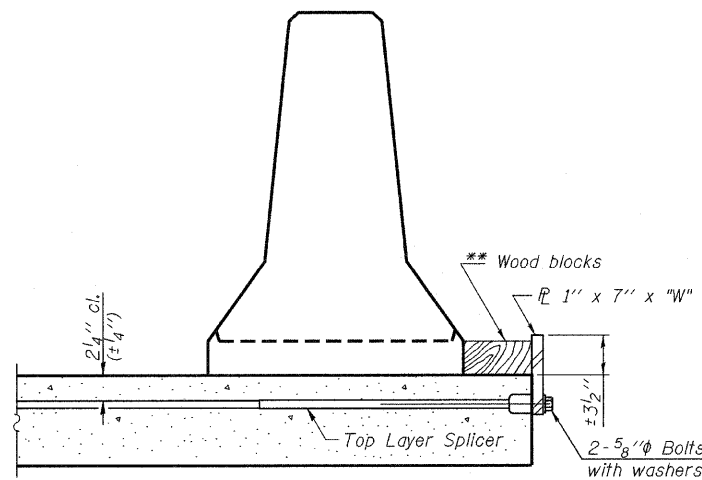
Detail I - With Bar Splicer or Couplers:
Connect one (1) 1"x7"x10" steel \bar{P} to the top layer of couplers with 2-5/8" ϕ bolts screwed to coupler at approximate \bar{C} of each barrier panel.

Detail II - With Extended Reinforcement Bars:
Connect one (1) 1"x7"x10" steel \bar{P} to the concrete slab or concrete wearing surface with 2-5/8" ϕ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate \bar{C} of each barrier panel.

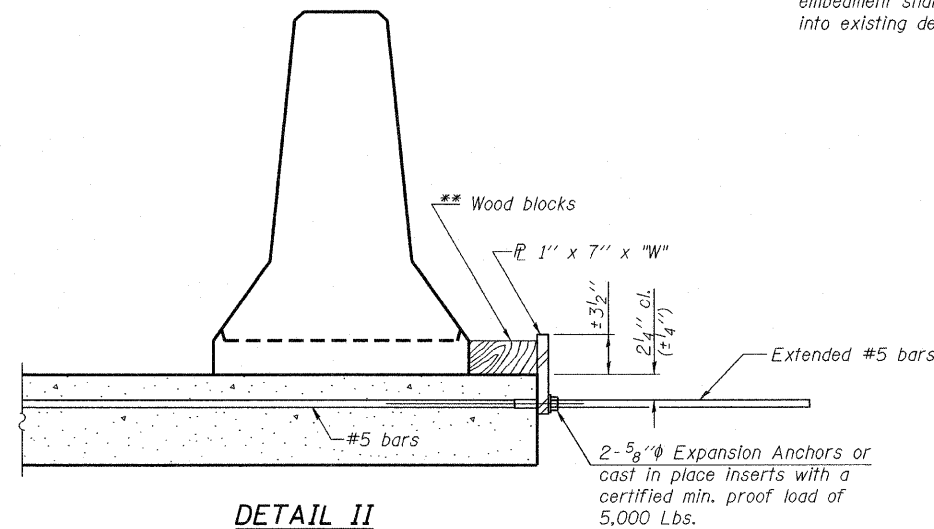
Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x 10" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

*** Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

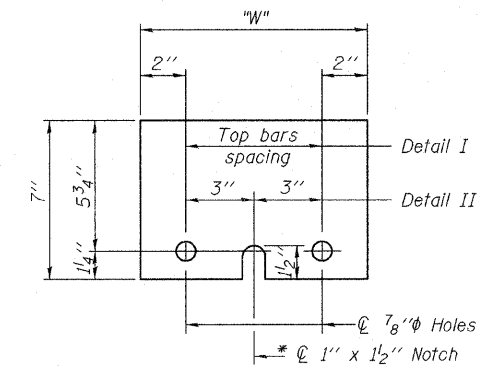
**** If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



DETAIL I



DETAIL II



STEEL RETAINER \bar{P} 1" x 7" x 10"

* Required only with Detail II

** Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

"W" = Top bars spacing + 4"

DESIGNED	IJL
CHECKED	GGE
DRAWN	baliva
CHECKED	IJL GGE

MARCH 3, 2010
EXAMINED *A. Carl Powers*
ENGINEER OF STRUCTURAL SERVICES
PASSED *Ralph E. Anderson*
ENGINEER OF BRIDGES AND STRUCTURES

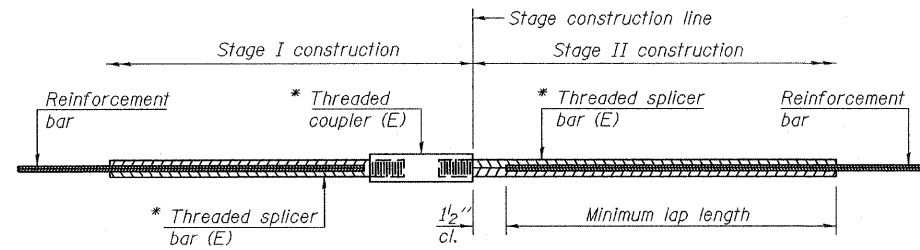
R-27

11-1-09

TEMPORARY CONCRETE BARRIER
IL 33 OVER CSXT RAILROAD
SN 051-0032

SHEET NO. 6 7 SHEETS	F.A.P. RTE. 783	SECTION (51-24-2,1-2)RS-1 &(1-X-1)RS-3	COUNTY LAWRENCE	TOTAL SHEETS 42	SHEET NO. 40
	CONTRACT NO. 74414				
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



STANDARD BAR SPLICER ASSEMBLY

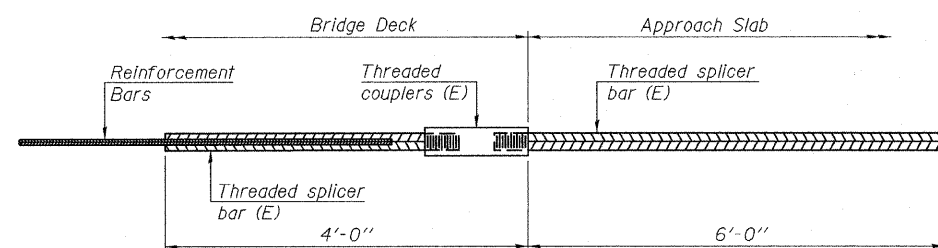
Minimum Lap Lengths				
Bar size to be spliced	Table 1	Table 2	Table 3	Table 4
3, 4	1'-5"	1'-11"	2'-1"	2'-4"
5	1'-9"	2'-5"	2'-7"	2'-11"
6	2'-1"	2'-11"	3'-1"	3'-6"
7	2'-9"	3'-10"	4'-2"	4'-8"
8	3'-8"	5'-1"	5'-5"	6'-2"
9	4'-7"	6'-5"	6'-10"	7'-9"

Table 1: Black bar, 0.8 Class C
Table 2: Black bar, Top bar lap, 0.8 Class C
Table 3: Epoxy bar, 0.8 Class C
Table 4: Epoxy bar, Top bar lap, 0.8 Class C

Threaded splicer bar length = min. lap length + 1/2" + thread length

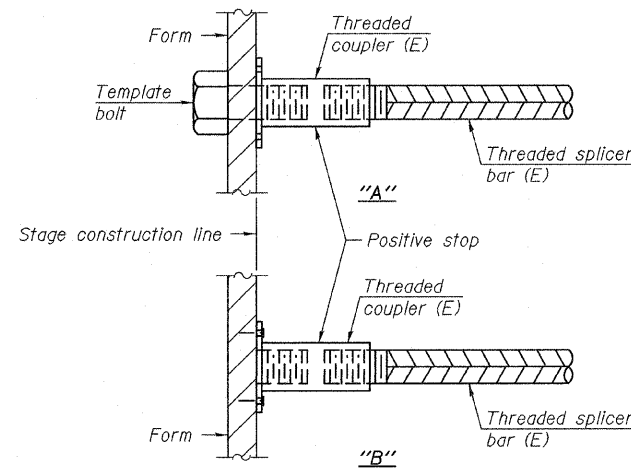
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length
North Abutment	#5	8	3
North Abutment	#6	4	3
South Abutment	#5	8	3
South Abutment	#6	4	3



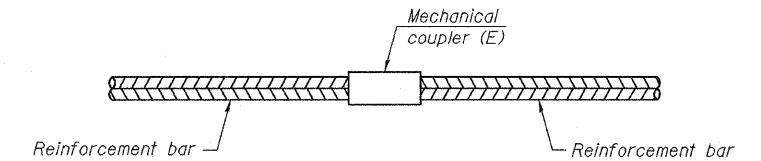
BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

No. required =



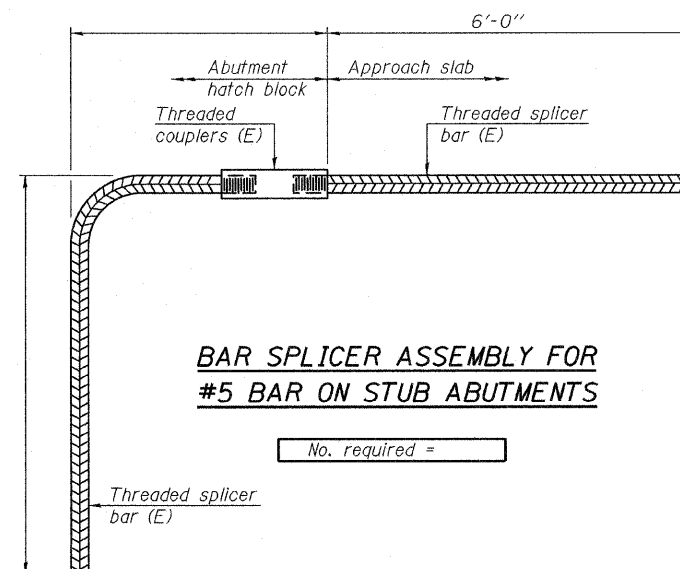
INSTALLATION AND SETTING METHODS

"A": Set bar splicer assembly by means of a template bolt.
"B": Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E): Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
All reinforcement shall be lapped and tied to the splicer bars.
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
See special provision for Mechanical Splicers.
See approved list of bar splicer assemblies and mechanical splicers for alternatives.

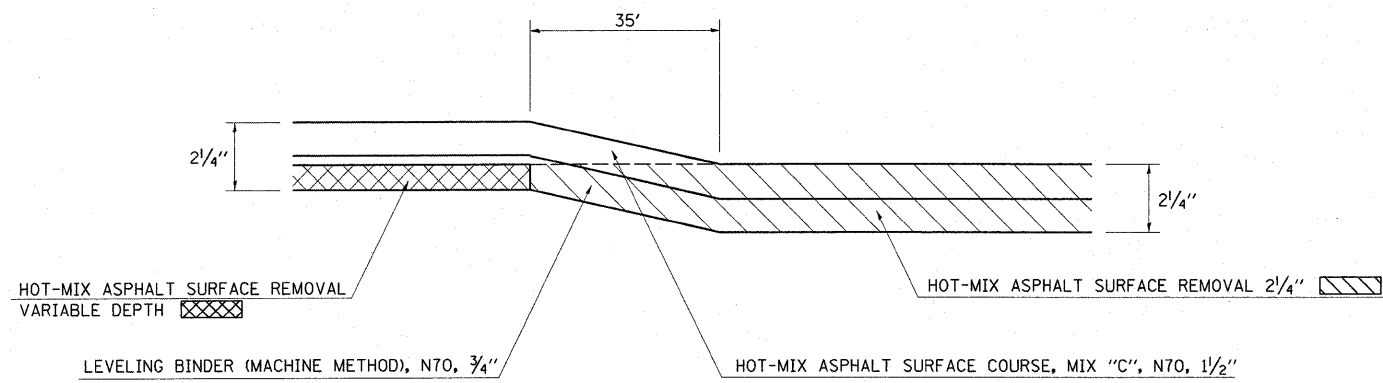
**BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
IL 33 OVER CSXT RAILROAD
SN 051-0032**

DESIGNED	IJL
CHECKED	GGE
DRAWN	balva
CHECKED	IJL GGE

MARCH 3, 2010
EXAMINED *A. Carl P...
ENGINEER OF STRUCTURAL SERVICES*
PASSED *Ralph E. Anderson
ENGINEER OF BRIDGES AND STRUCTURES*

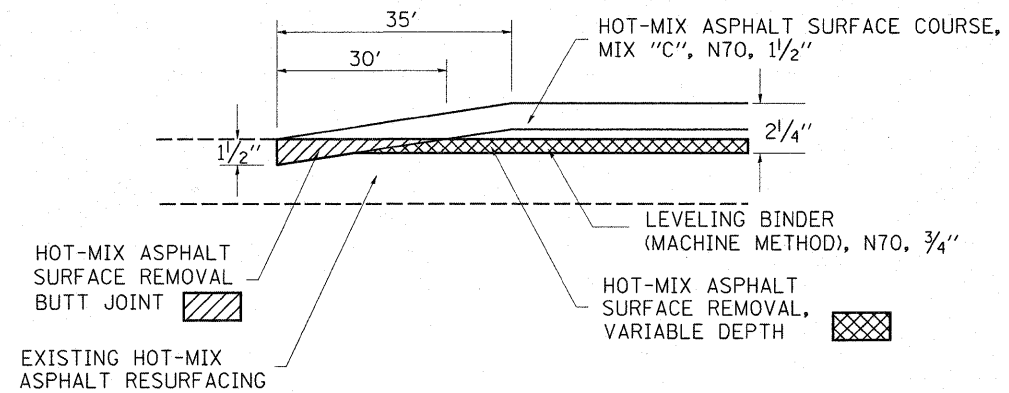
BSD-1 11-1-09

SHEET NO. 7 7 SHEETS	F.A.P. RTE. 783	SECTION (51-24-2.1-2)RS- 1&(1-X-1)RS-3	COUNTY LAWRENCE	TOTAL SHEETS 42	SHEET NO. 41
	FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO. 74414 FED. AID PROJECT		



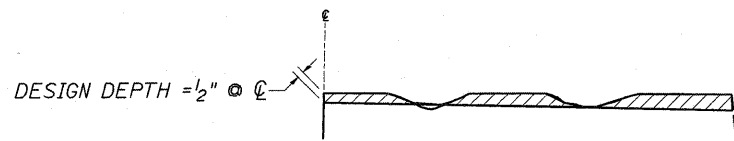
MILLING TRANSITION DETAIL

STATION 21+00.00 TO STATION 21+35.00
 STATION 2+65.00 TO STATION 3+00.00
 STATION 22+50.00 TO STATION 22+85.00



BUTT JOINT DETAIL

STATION 38+55.20 TO STATION 38+90.20



NOTES:

1. MILLING SHALL BE DONE TO ATTAIN A 3/16 "/FT. SLOPE IN CROWN SECTIONS
2. EXISTING S.E. AND S.E. TRANSITIONS SHALL BE MAINTAINED UNLESS OTHERWISE SHOWN ON THE PLANS.
3. MILLING TO THE BOTTOM OF WHEEL RUTS SHALL NOT BE NECESSARY UNLESS REQUIRED TO OBTAIN SLOPE OR THE DESIGN DEPTH AT CENTERLINE.
4. THE AVERAGE DEPTH OF MILLING IS ESTIMATED TO BE 1/2" BUT MAY VARY IN ISOLATED LOCATIONS.

HOT-MIX ASPHALT SURFACE REMOVAL (VARIABLE DEPTH) DETAIL

FILE NAME =	USER NAME = swartzw	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CONSTRUCTION DETAILS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pw_work\p\dot\swartzw\d0148574\d77	#14-shr-details.dgn	DRAWN -	REVISED -					783	.	LAWRENCE	42	42
	PLOT SCALE = 20.0000 ' / IN.	CHECKED -	REVISED -		SCALE: NA	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 74414	
	PLOT DATE = 1/29/2010	DATE -	REVISED -									

• (51-24-2,1-2)RS-1 & (1-X-1)RS-3