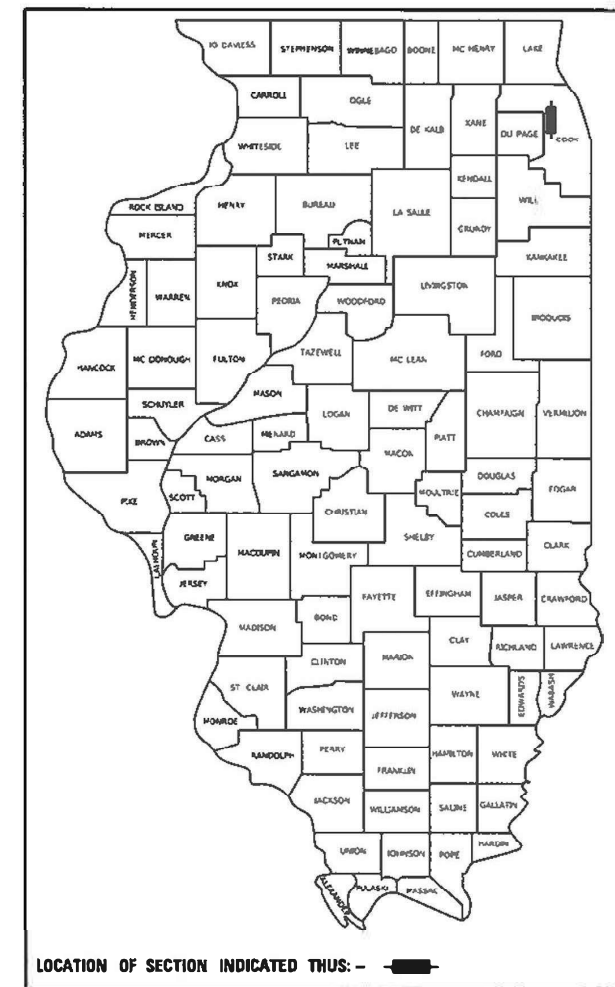


STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52 N	COOK	78	1
		ILLINOIS	CONTRACT NO.	60T17

D-91-305-12



FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED IN THE VILLAGE OF FRANKLIN PARK

PROPOSED  
HIGHWAY PLANS

FAU ROUTE 2710: DES PLAINES RIVER ROAD  
AT ROBINSON ROAD  
SECTION: 52-N  
PROJECT: STP-SLTD(858)  
INTERSECTION RECONSTRUCTION  
COOK COUNTY

C-91-305-12

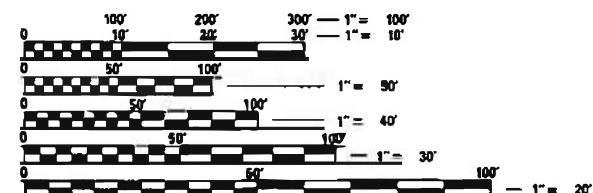
TRAFFIC DATA:

DES PLAINES RIVER ROAD:

PROJECT BEGIN TO PROJECT END:

ADT (2018) = 27,700

SPEED LIMIT = 40 MPH

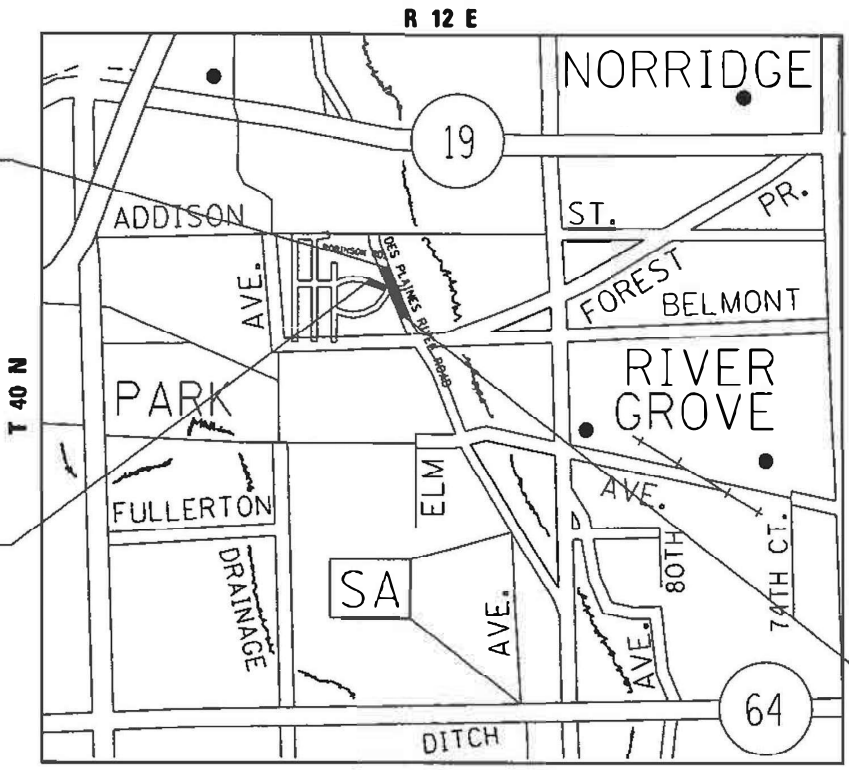
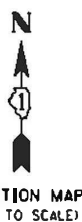


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS  
1-800-992-0123  
DR 811

PROJECT ENDS  
STA. 118 + 30

PROJECT ENDS  
STA. 202 + 37



PROJECT BEGINS  
STA. 106 + 19

LEYDEN TOWNSHIP

NET LENGTH = 1,374 FT. = 0.26 MILE

PROJECT ENGINEER: VESELIN VELICHKOV (847) 705-4432  
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

CONTRACT NO. 60T17

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED January 21, 2021

*[Signature]*  
REGIONAL ENGINEER

March 19, 2021

*[Signature]*  
ENGINEER OF DESIGN AND ENVIRONMENT

March 19, 2021

*[Signature]*  
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

**INDEX OF SHEETS**

**STATE STANDARDS**

**GENERAL NOTES**

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2-3	INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES	001001-02	AREAS OF REINFORCEMENT BARS
4-10	SUMMARY OF QUANTITIES	001006	DECIMAL OF AN INCH AND OF A FOOT
11-13	TYPICAL SECTIONS	280001-07	TEMPORARY EROSION CONTROL SYSTEMS
14-15	SCHEDULE OF QUANTITIES	424001-11	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
16	ALIGNMENT, TIES, AND BENCHMARKS	424021-06	DEPRESSED CORNER RAMPS FOR SIDEWALKS
17-18	EXISTING AND PROPOSED ROADWAY PLAN AND PROFILE	442201-03	CLASS C AND D PATCHES
19-24	MAINTENANCE OF TRAFFIC PLAN	542301-03	PRECAST REINFORCED FLARED END SECTION
25	EROSION CONTROL PLAN	601001-05	PIPE UNDERDRAINS
26-27	EXISTING AND PROPOSED DRAINAGE PLAN AND PROFILE	602001-02	CATCH BASIN, TYPE A
28	SUE INVESTIGATION AND UNDERGROUND UTILITIES	602011-02	CATCH BASIN, TYPE C
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33	PROPOSED PAVEMENT MARKING PLANS	604001-05	FRAME AND LIDS, TYPE 1
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35-51	TRAFFIC SIGNAL INSTALLATION PLANS	606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
52	GUARDRAIL PLAN (LOCATION 5B)	701006-05	OFF-ROAD OPERATIONS 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
53	DRIVEWAY DETAILS DISTANCE BETWEEN R.O.W. AND FACE OF CURB / EDGE OF SHOULDER >= 4.5 m (15') (BD-01)	701101-05	OFF-ROAD OPERATIONS, MULTILANE 15 FT (4.5 m) TO 24 IN. (600 mm) FROM PAVEMENT EDGE
54	DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND CURB OR EDGE GREATER THAN OR EQUAL TO 15 FT (4.5 m) (BD-02)	701106-02	OFF-ROAD OPERATIONS, MULTILANE, MORE THAN 15 FT. (4.5 m) AWAY
55	DETAILS OF STORM SEWER CONNECTION TO EXISTING SEWER (BD-07)	701301-04	LANE CLOSURE 2L, 2W SHORT TIME OPERATIONS
56	DETAILS FOR FRAMES AND LIDS ADJUSTMENTS WITH MILLING (BD-08)	701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS- DAY ONLY
57	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)	701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≤ 40 MPH
58	CURB OR GUTTER AND GUTTER REMOVAL AND REPLACEMENT (BD-24)	701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
59	BUTT JOINT AND HMA TAPER DETAILS (BD-32)	701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
60	BENCHMARK DETAILS FOR EMBANKMENT WIDENING (BD-51)	701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
61	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)	701701-10	URBAN LANE CLOSURE MULTILANE INTERSECTION
62	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11)	701801-06	LANE CLOSURE, MULTILANE, 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
63	DISTRICT ONE TYPICAL PAVEMENT MARKING (TC-13)	701901-08	TRAFFIC CONTROL DEVICES
64	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)	720001-01	SIGN PANEL MOUNTING DETAIL
65	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC (TC-16)	720016-04	MAST ARM MOUNTED STREET NAME SIGNS
66	ARTERIAL ROAD INFORMATION SIGN (TC-22)	780001-05	TYPICAL PAVEMENT MARKINGS
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68-78	CROSS SECTIONS	805001-01	ELECTRICAL SERVICE INSTALLATION DETAILS
		814001-03	HANDHOLES
		814006-03	DOUBLE HANDHOLES
		857001-01	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
		862001-01	UNINTERRUPTABLE POWER SUPPLY
		873001-02	TRAFFIC SIGNAL GROUNDING AND BONDING
		877001-08	STEEL MAST ARM ASSEMBLY AND POLE 16 FT. THROUGH 55 FT.
		877006-06	STEEL MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS
		878001-11	CONCRETE FOUNDATION DETAILS
		880006-01	TRAFFIC MOUNTING DETAILS
		886001-01	DETECTOR LOOP INSTALLATIONS
		886006-01	TYPICAL LAYOUT FOR DETECTION LOOPS

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF FRANKLIN PARK.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE RESIDENT ENGINEER.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1½ INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1V:3H.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
- PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER / TECHNICIAN.
- CATCH BASINS, MANHOLES, INLETS, DRAINAGE STRUCTURES AND VALVE VAULTS ADJUSTMENT AND/OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER / TECHNICIAN.
- LOCATIONS OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)), WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER / TECHNICIAN.
- FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
- THE CONTRACTOR SHALL THE CONTACT DISTRICT ONE ARTERIAL TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE RESIDENT ENGINEER SHALL CONTACT EMAD ALHUSSENI, AREA TRAFFIC FIELD ENGINEER, VIA E-MAIL AT EMAD.ALHUSSENI@ILLINOIS.GOV, A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.
- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS, UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENT FIRM TO CONTINUOUSLY MONITOR FOR WORKER SAFETY AND SOIL CONTAMINATION AT SEVERAL LOCATIONS. SEE SPECIAL PROVISION AND SUPPLEMENTAL SPECIFICATIONS FOR DETAILS.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- ALL CAST OPEN LIDS FOR FRAMES, TYPE 1, WITHIN CURB RAMPS FOR SIDEWALK, SHALL BE "ADA COMPLIANT" CAST OPEN LIDS PER HIGHWAY STANDARD 604001.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPLACEMENT OF ANY DETECTOR LOOPS DAMAGED DURING CONSTRUCTION.
- ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

**SEE SHEET 3 FOR CONTINUATION**

REV-SEP

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DRAWN -	REVISIED -	2710			52-N	COOK	78	2		
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 60T17					
PLOT DATE = 3/9/2021	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
				SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.

**GENERAL NOTES (CONTINUED)**

22. ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF CURB OR DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER, SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCIDENTAL.
23. PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH THE "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL. (TC-13)
24. UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE RESIDENT ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
25. THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS THAT WILL NOT BE REMOVED INCLUDING PREVIOUSLY SEEDED AREAS. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE TO THE SATISFACTION OF THE ENGINEER.
26. THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICT WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS
27. THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY AND EQUIPMENT, LEASED MUNICIPALITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATIONS.
28. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEMS SUCH AS FOUNDATION, CONDUIT, HANDHOLE, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACE SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT, ETC., SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS 250 AND 252 RESPECTIIVELY.
29. IF THIS CONTRACT REQUIRES THE SERVICES OF AN ELECTRICAL CONTRACTOR, THE CONTRACTOR SHALL BE RESPONSIBLE AT HIS/HER OWN EXPENSE FOR LOCATING IDOT EXISTING ELECTRICAL FACILITIES PRIOR TO PERFORMING ANY WORK. IF THIS CONTRACT DOES NOT REQUIRE THE SERVICES OF AN ELECTRICAL CONTRACTOR, THE CONTRACTOR MAY REQUEST ONE FREE LOCATE FOR EXISTING IDOT ELECTRICAL FACILITIES FROM THE DISTRICT ONE ELECTRICAL MAINTENANCE CONTRACTOR PRIOR TO THE START OF ANY WORK. ADDITIONAL REQUEST SHALL BE AT THE EXPENSE OF THE CONTRACTOR. THE LOCATION OF UNDERGROUND FACILITIES DOES NOT RELIEVE THE CONTRACTOR OF THEIR RESPONSIBILITY TO REPAIR ANY FACILITIES DAMAGED DURING CONSTRUCTION AT THEIR EXPENSE.
30. "AGGREGATE SUBGRADE IMPROVEMENT (CU.YD.) HAS BEEN PROVIDED FOR USE FOR SOILS THAT TEND TO BE UNSUITABLE OR UNSTABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH P.G.E.S. WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIAL UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.03 OF THE STANDARD SPECIFICATIONS AND GUIDELINES IN THE IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS ENCOUNTERED, THE SOIL SHALL BE REMOVED AND REPLACED WITH P.G.E.S. OR EMBANKMENT AS DETERMINED BY THE GEOTECHNICAL ENGINEER. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR."
31. PIPE UNDERDRAINS SHALL BE INSTALLED ACCORDING TO SECTION 601 OF THE SSRBC AND STANDARD 601001-05. TOP OF PIPE UNDERDRAINS SHALL BE PLACED MINIMUM 6" BELOW THE AGGREGATE SUBGRADE IMPROVEMENT LAYER. THE COST OF MAKING PIPE UNDERDRAINS CONNECTIONS TO DRAINAGE STRUCTURES SHALL BE INCLUDED IN THE COST OF THE PIPE UNDERDRAINS.
32. ANY AGGREGATE SUBGRADE IMPROVEMENT CONTAMINATED AND/OR DAMAGED BY THE CONTRACTOR'S VEHICLES AND/OR EQUIPMENTS IS TO BE REMOVED AND REPLACED AS DIRECT BY THE ENGINEER AT CONTRACTOR EXPENSE.
33. THIS PROJECT REQUIRES A US ARMY CORPS OF ENGINEERS (USACE) 404 PERMIT THAT WILL BE SECURED BY THE DEPARTMENT. AS A CONDITION OF THIS PERMIT, THE CONTRACTOR WILL NEED TO SUBMIT AN IN-STREAM WORK PLAN TO THE DEPARTMENT FOR APPROVAL. GUIDELINES ON ACCEPTABLE IN-STREAM WORK TECHNIQUES CAN BE FOUND ON THE USACE WEBSITE. THE USACE DEFINES AND DETERMINES IN-STREAM WORK. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT AN IN-STREAM WORK PLAN WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED WITH THE EXCEPTION OF COFFERDAMS WHICH WILL BE PAID FOR AS COFFERDAM (TYPE 1) (IN-STREAM/WETLAND WORK) WITH A BASIS OF PAYMENT OF EACH.
34. PHOSPHORUS FERTILIZER HAS BEEN INTENTIONALLY OMITTED FROM THE CONTRACT. A PHOSPHORUS FREE FERTILIZER SHALL BE USED (MIDDLE NUMBER SHOULD EQUAL 0).

35. LOCATIONS WITH PEDESTRIAN EQUIPMENT HAVE BEEN DESIGNED TO BE ADA COMPLIANT. ANY DEVIATION FROM THE PLANS FOR TRAFFIC SIGNAL MAST ARM/POSTS THAT HAVE PEDESTRIAN EQUIPMENT WILL HAVE TO BE APPROVED BY THE ENGINEER TO INSURE ADA COMPLIANCE.
36. PARTIAL PAYMENTS AS DESCRIBED IN ARTICLE 109.07 (b) OF THE STANDARD SPECIFICATIONS WILL NOT BE ALLOWED FOR ITEMS INCLUDED IN THIS CONTRACT.
37. BACKFILLING STORM SEWER CONSTRUCTED UNDER THE ROADWAY SPECIFIED UNDER ART. 550.07 (b, c) OF THE SSRBC WILL NOT BE ALLOWED.
38. ALL EXISTING R.O.W. SHOWN IS APPROXIMATE AND MAY NEED TO BE VERIFIED IN THE FIELD. ANY R.O.W. CONFLICTS SHALL BE COORDINATED WITH THE RESIDENT ENGINEER.
39. THE SUBGRADE STABILITY SHALL BE VERIFIED BY PROOF ROLLING WITH A FULLY LOADED TANDEM-AXLE TRUCK.
40. THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.
41. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS			QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR VOIDS @ Ndes		
<b>PAVEMENT WIDENING (DES PLAINES RIVER ROAD)</b>			
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, 9.5, MIX "F" N80, 1 3/4"	3.5% AT 80 GYR	QC/QA	
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 3/4"	3.5% AT 50 GYR.	QC/QA	
HOT-MIX ASPHALT BASE COURSE 7 1/2" (HMA BINDER IL-19.0)	4.0% AT 90 GYR.	QC/QA	
HOT-MIX ASPHALT BASE COURSE WIDENING, HMA BINDER IL-19.0, 7 1/2"	4.0% AT 90 GYR.	QC/QA	
<b>PAVEMENT RESURFACING (DES PLAINES RIVER ROAD)</b>			
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, 9.5, MIX "F" N80, 1 3/4"	3.5% AT 80 GYR	QC/QA	
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 3/4"	3.5% AT 50 GYR.	QC/QA	
<b>PAVEMENT WIDENING (ROBINSON ROAD)</b>			
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, 9.5, MIX "F" N80, 1 3/4"	3.5% AT 80 GYR	QC/QA	
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 3/4"	3.5% AT 50 GYR.	QC/QA	
HOT-MIX ASPHALT BASE COURSE 4 1/2" (HMA BINDER IL-19.0)	4.0% AT 90 GYR.	QC/QA	
HOT-MIX ASPHALT BASE COURSE WIDENING 4 1/2" (HMA BINDER IL-19.0)	4.0% AT 90 GYR.	QC/QA	
<b>PAVEMENT RESURFACING (ROBINSON ROAD)</b>			
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, 9.5, MIX "F" N80, 1 3/4"	3.5% AT 80 GYR	QC/QA	
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 3/4"	3.5% AT 50 GYR.	QC/QA	
<b>SHARED-USE PATH</b>			
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 mm) 2"	4.0% AT 50 GYR.	QC/QA	
<b>DRIVEWAY</b>			
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 mm), 2"	4.0% AT 50 GYR.	QC/QA	
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19 mm) : CE - 8"	4.0% AT 50 GYR.	QC/QA	
<b>PATCHING</b>			
CLASS D PATCHES (HOT-MIX ASPHALT BINDER IL-19 mm)	4.0% AT 70 GYR.	QC/QA	

QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA)

NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

NOTE 3: QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

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PLOT DATE = 3/9/2021	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES  
DES PLAINES RIVER ROAD AT ROBINSON ROAD**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	3
CONTRACT NO. 60T17				
ILLINOIS		FED. AID PROJECT		

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		0005 80% FED 20% STATE	0021 80% FED 13.3% STATE 6.7% VILLAGE TRAFFIC SIGNAL	0021 80% FED 20% STATE INTER CONNECT	0021 100% FRANKLIN PARK (EVP)	0005 100% STATE	0021 80% FED 20% VILLAGE FRANKLIN PARK
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	115	115					
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	35	35					
20200100	EARTH EXCAVATION	CU YD	2300	2300					
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	100	100					
20800150	TRENCH BACKFILL	CU YD	375	375					
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SO YD	150	150					
21101505	TOPSOIL EXCAVATION AND PLACEMENT	CU YD	300	300					
21101625	TOPSOIL FURNISH AND PLACE, 6"	SO YD	2740	2740					
25000210	SEEDING, CLASS 2A	ACRE	0.5	0.5					
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	85	85					
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	85	85					
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	85	85					
25100630	EROSION CONTROL BLANKET	SO YD	2740	2740					
25200100	SODDING	SQ YD	275	275					
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	85	85					
28000400	PERIMETER EROSION BARRIER	FOOT	1260	1260					

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		0005 80% FED 20% STATE	0021 80% FED 13.3% STATE 6.7% VILLAGE TRAFFIC SIGNAL	0021 80% FED 20% STATE INTER CONNECT	0021 100% FRANKLIN PARK (EVP)	0005 100% STATE	0021 80% FED 20% VILLAGE FRANKLIN PARK
28000500	INLET AND PIPE PROTECTION	EACH	15	15					
28001100	TEMPORARY EROSION CONTROL BLANKET	SO YD	1200	1200					
28100105	STONE RIPRAP, CLASS A3	SO YD	10	10					
28200200	FILTER FABRIC	SO YD	10	10					
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	100	100					
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SO YD	2340	2340					
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SO YD	665						665
35501302	HOT-MIX ASPHALT BASE COURSE, 4 1/2"	SO YD	160	160					
35501314	HOT-MIX ASPHALT BASE COURSE, 7 1/2"	SO YD	1080	1080					
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SO YD	356	356					
35600706	HOT-MIX ASPHALT BASE COURSE WIDENING, 7 1/2"	SO YD	320	320					
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	5860	5860					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	15	15					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	80	80					

REV-SEP

\* = SPECIALTY ITEMS  
 △ = NON-PARTICIPATING WORK (100% STATE)

FILE NAME =	USER NAME = qqr/rtm	DESIGNED -	REVISED -
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	PLOT DATE = 1/29/2021	DATE -	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES			
DES PLAINES RIVER ROAD AT ROBINSON ROAD			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	4
CONTRACT NO. 60T17			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		0005 80% FED 20% STATE	0021 80% FED 13.3% STATE 6.7% VILLAGE TRAFFIC SIGNAL	0021 80% FED 20% STATE INTER CONNECT	0021 100% FRANKLIN PARK (EVP)	0005 100% STATE	0021 80% FED 20% VILLAGE FRANKLIN PARK
40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	320	320					
40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	145	60					85
40605026	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80	TON	745	745					
42001300	PROTECTIVE COAT	SO YD	740	740					
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SO YD	166	166					
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	1140						1140
42400800	DETECTABLE WARNINGS	SO FT	30	16					14
44000100	PAVEMENT REMOVAL	SO YD	130	130					
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	7605	7605					
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	525	525					
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1320	1320					
44000600	SIDEWALK REMOVAL	SO FT	75	75					
44201798	CLASS D PATCHES, TYPE I, 13 INCH	SO YD	15	15					

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		0005 80% FED 20% STATE	0021 80% FED 13.3% STATE 6.7% VILLAGE TRAFFIC SIGNAL	0021 80% FED 20% STATE INTER CONNECT	0021 100% FRANKLIN PARK (EVP)	0005 100% STATE	0021 80% FED 20% VILLAGE FRANKLIN PARK
44201803	CLASS D PATCHES, TYPE II, 13 INCH	SO YD	340	340					
44201807	CLASS D PATCHES, TYPE III, 13 INCH	SO YD	105	105					
44201809	CLASS D PATCHES, TYPE IV, 13 INCH	SO YD	365	365					
48101620	AGGREGATE SHOULDERS, TYPE B 10"	SO YD	203	203					
54213663	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 18"	EACH	1	1					
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	395	395					
550A0090	STORM SEWERS, CLASS A, TYPE 1 18"	FOOT	6	6					
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	295	295					
550A0360	STORM SEWERS, CLASS A, TYPE 2 15"	FOOT	225	225					
56400100	FIRE HYDRANTS TO BE MOVED	EACH	2	2					
60108204	PIPE UNDERDRAINS, TYPE 2, 4"	FOOT	1440	1440					
60201340	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	7	7					
60206905	CATCH BASINS, TYPE C, TYPE 1 FRAME, OPEN LID	EACH	1	1					
60208240	CATCH BASINS, TYPE C, TYPE 24 FRAME AND GRATE	EACH	6	6					
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	1	1					

REV-SEP

\* = SPECIALTY ITEMS  
△ = NON-PARTICIPATING WORK (100% STATE)

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE						TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE							
CODE NO	ITEM	UNIT		0005 80% FED 20% STATE	0021 80% FED 13.3% STATE 6.7% VILLAGE TRAFFIC SIGNAL	0021 80% FED 20% STATE INTER CONNECT	0021 100% FRANKLIN PARK (EVP)	0005 100% STATE	0021 80% FED 20% VILLAGE FRANKLIN PARK		CODE NO	ITEM	UNIT	0005 80% FED 20% STATE	0021 80% FED 13.3% STATE 6.7% VILLAGE TRAFFIC SIGNAL	0021 80% FED 20% STATE INTER CONNECT	0021 100% FRANKLIN PARK (EVP)	0005 100% STATE
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	8	8						* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1	1				
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	2	2						* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	15	15				
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	2	2						67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	12				
60500050	REMOVING CATCH BASINS	EACH	2	2						67100100	MOBILIZATION	L SUM	1	1				
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	220	220						70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	55	55				
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	2790	2790						70107025	CHANGEABLE MESSAGE SIGN	CAL DA	60	60				
* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	87.5	87.5						70300100	SHORT TERM PAVEMENT MARKING	FOOT	6650	6650				
* 63000003	STEEL PLATE BEAM GUARDRAIL, TYPE A, 9 FOOT POSTS	FOOT	25	25						70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	2220	2220				
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	2	2						70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	110	110				
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	670	670						70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	4740	4740				
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	3	3						70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	590	590				
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1	1						70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	310	310				
										70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	100	100				
										70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	1670	1670				
										70300900	PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS	SO FT	110	110				

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SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE						SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE						
CODE NO	ITEM	UNIT	TOTAL QUANTITIES URBAN	0005 80% FED 20% STATE	0021 80% FED 13.3% STATE 6.7% VILLAGE TRAFFIC SIGNAL	0021 80% FED 20% STATE INTER CONNECT	0021 100% FRANKLIN PARK (EVP)	0005 100% STATE	0021 80% FED 20% VILLAGE FRANKLIN PARK	CODE NO	ITEM	UNIT	TOTAL QUANTITIES URBAN	0005 80% FED 20% STATE	0021 80% FED 13.3% STATE 6.7% VILLAGE TRAFFIC SIGNAL	0021 80% FED 20% STATE INTER CONNECT	0021 100% FRANKLIN PARK (EVP)	0005 100% STATE	0021 80% FED 20% VILLAGE FRANKLIN PARK	
70300904	PAVEMENT MARKING TAPE, TYPE IV 4"	FOOT	4740	4740						* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	100	100						
70300906	PAVEMENT MARKING TAPE, TYPE IV 6"	FOOT	590	590						* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	125	125						
70300924	PAVEMENT MARKING TAPE, TYPE IV 24"	FOOT	100	100						78100200	TEMPORARY RAISED REFLECTIVE PAVEMENT MARKER	EACH	125	125						
* 72000100	SIGN PANEL - TYPE 1	SO FT	92	75	17					78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL									
* 72000200	SIGN PANEL - TYPE 2	SO FT	13		13					* 81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	1705		785	920				
72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	4	4						* 81028220	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	130		130					
72400200	REMOVE SIGN PANEL ASSEMBLY - TYPE B	EACH	1	1						* 81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	213		213					
72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	3	3						* 81400100	HANDHOLE	EACH	6		4	2				
* 72501000	TERMINAL MARKER - DIRECT APPLIED	EACH	2	2						* 81400200	HEAVY-DUTY HANDHOLE	EACH	1		1					
72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	125	125						* 81400300	DOUBLE HANDHOLE	EACH	1		1					
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	110	110						* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2			2				
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	4740	4740						* 86400100	TRANSCEIVER - FIBER OPTIC	EACH	1			1				
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	590	590						* 87300925	ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	2554			2554				
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	310	310																

REV-SEP

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>SUMMARY OF QUANTITIES  DES PLAINES RIVER ROAD AT ROBINSON ROAD</b>			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	7
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT			CONTRACT NO. 60T17	

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE						SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		0005 80% FED 20% STATE	0021 80% FED 13.3% STATE 6.7% VILLAGE TRAFFIC SIGNAL	0021 80% FED 20% STATE INTER CONNECT	0021 100% FRANKLIN PARK (EVP)	0005 100% STATE	0021 80% FED 20% VILLAGE FRANKLIN PARK	CODE NO	ITEM	UNIT		0005 80% FED 20% STATE	0021 80% FED 13.3% STATE 6.7% VILLAGE TRAFFIC SIGNAL	0021 80% FED 20% STATE INTER CONNECT	0021 100% FRANKLIN PARK (EVP)	0005 100% STATE	0021 80% FED 20% VILLAGE FRANKLIN PARK
* 87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	183	183					* 87702210	STEEL MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS, 20 FT. AND 30 FT.	EACH	1	1						
* 87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	619	619					* 87800100	CONCRETE FOUNDATION, TYPE A	FOOT	12	12						
* 87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	2101	2101					* 87800150	CONCRETE FOUNDATION, TYPE C	FOOT	4	4						
* 87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	262	262					* 87800400	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	10	10						
* 87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	2173	2173					* 87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	23	23						
* 87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	197	197					* 87900200	DRILL EXISTING HANDHOLE	EACH	1		1					
* 87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	606	606					* 88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	8	8						
* 87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	2	2					* 88030050	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	3	3						
* 87700170	STEEL MAST ARM ASSEMBLY AND POLE, 26 FT.	EACH	1	1					* 88030100	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	1	1						
* 87700230	STEEL MAST ARM ASSEMBLY AND POLE, 38 FT.	EACH	1	1					* 88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	1	1						
									* 88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	2	2						

REV-SEP

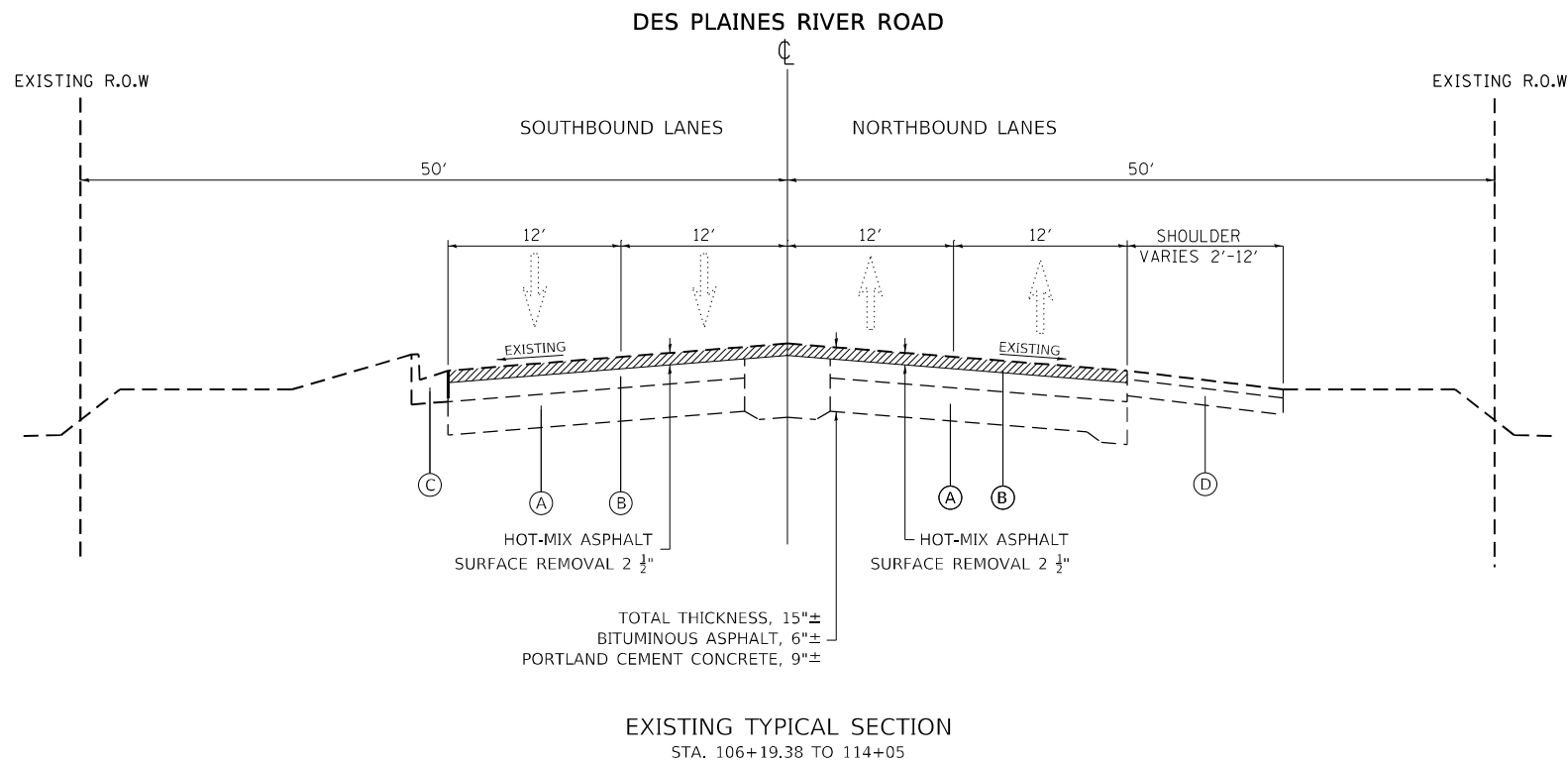
\* = SPECIALTY ITEMS  
 △ = NON-PARTICIPATING WORK (100% STATE)



SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE						SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		0005 80% FED 20% STATE	0021 80% FED 13.3% STATE 6.7% VILLAGE TRAFFIC SIGNAL	0021 80% FED 20% STATE INTER CONNECT	0021 100% FRANKLIN PARK (EVP)	0005 100% STATE	0021 80% FED 20% VILLAGE FRANKLIN PARK	CODE NO	ITEM	UNIT		0005 80% FED 20% STATE	0021 80% FED 13.3% STATE 6.7% VILLAGE TRAFFIC SIGNAL	0021 80% FED 20% STATE INTER CONNECT	0021 100% FRANKLIN PARK (EVP)	0005 100% STATE	0021 80% FED 20% VILLAGE FRANKLIN PARK
* 88200410	TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC	EACH	9	9					X0900075	COFFERDAM (TYPE 1) (IN-STREAM/WETLAND WORK)	EACH	1	1						
* 88500100	INDUCTIVE LOOP DETECTOR	EACH	5	5					* X1400081	FULL-ACTUATED CONTROLLER AND TYPE SUPER P CABINET (SPECIAL)	EACH	1	1						
* 88600100	DETECTOR LOOP, TYPE I	FOOT	303	303					* X1400150	SERVICE INSTALLATION, GROUND MOUNTED, METERED	EACH	1	1						
* 88700200	LIGHT DETECTOR	EACH	2			2			* X1400201	RADAR VEHICLE DETECTION SYSTEM, SINGLE APPROACH, STOP BAR	EACH	1	1						
* 88700300	LIGHT DETECTOR AMPLIFIER	EACH	1			1			X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	7	7						
* 88800100	PEDESTRIAN PUSH-BUTTON	EACH	2	2					△ X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	1450				1450			
* 89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	5604	524	5080				X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	5	5						
* 89502380	REMOVE EXISTING HANDHOLE	EACH	3		3				* X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1						
* X0324085	EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C	FOOT	436			436			X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SO FT	3060	3060						
* X0324599	ROD AND CLEAN EXISTING CONDUIT	FOOT	1057	219	838				* X8100105	CONDUIT SPLICE	EACH	1		1					
* X0325938	TEMPORARY WIRELESS INTERCONNECT, COMPLETE	L SUM	1		1				* X8620200	UNINTERRUPTABLE POWER SUPPLY, SPECIAL	EACH	1	1						
78300201	PAVEMENT MARKING REMOVAL - GRINDING	SO FT	2480	2480															
78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SO FT	1000	1000															

REV-SEP  
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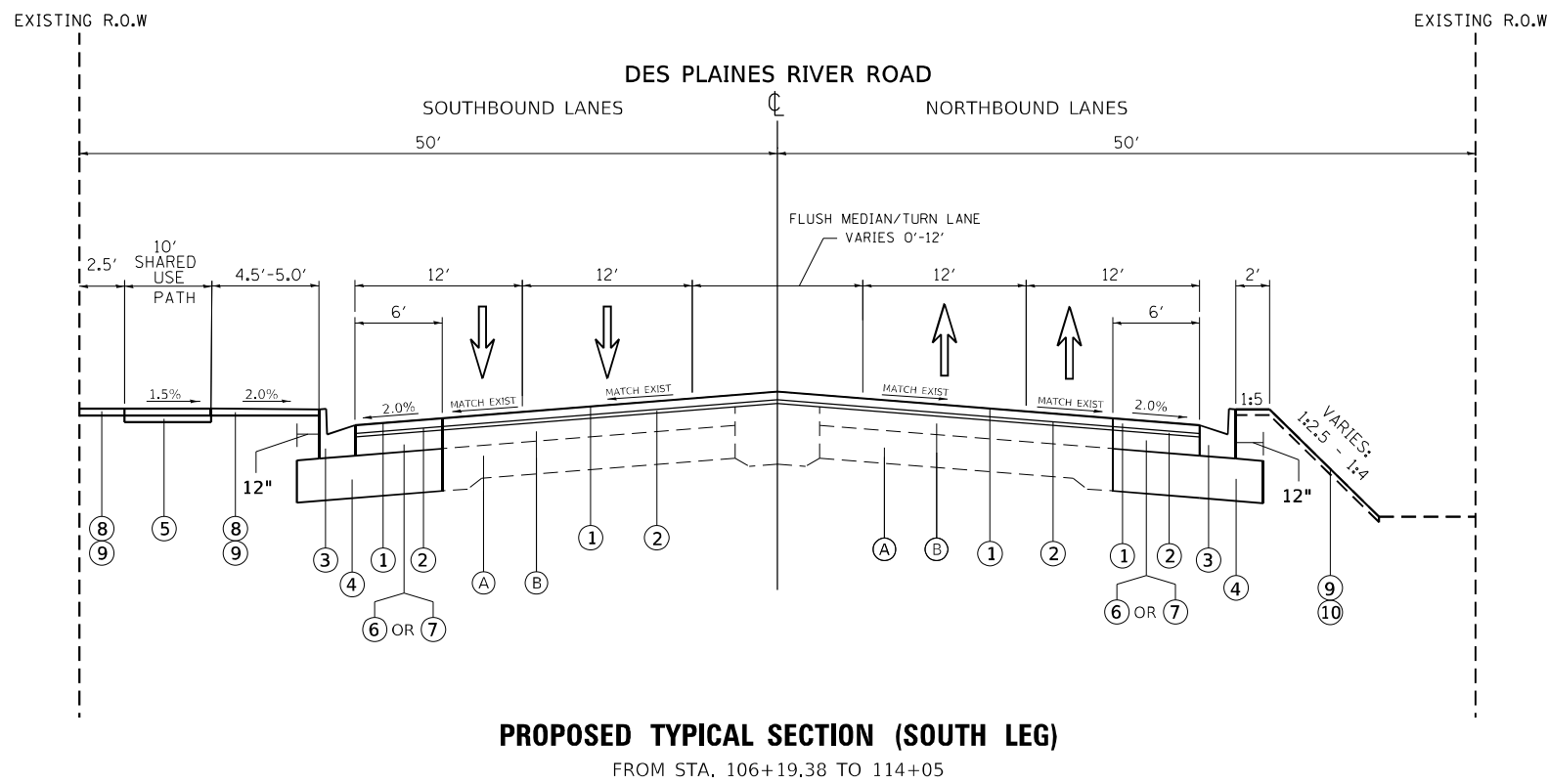


**LEGEND - EXISTING:**

- (A) PORTLAND CEMENT CONCRETE PAVEMENT ± 9"
- (B) HOT-MIX ASPHALT PAVEMENT ± 6"
- (C) COMBINATION CONCRETE CURB AND GUTTER
- (D) HOT-MIX ASPHALT BASE COURSE
- (E) HOT-MIX ASPHALT BASE COURSE WIDENING
- (F) AGGREGATE SHOULDER
- (G) PCC SIDEWALK

**NOTES:**

- 1.) THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING.



**LEGEND - PROPOSED**

- (1) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA 9.5 MIX "F" N80, 1 3/4"
- (2) POLYMERIZED HOT-MIX ASPHALT BINDER, IL-4.75, N50 3/4"
- (3) COMBINATION PCC CURB AND GUTTER, TYPE B-6.24
- (4) AGGREGATE SUBGRADE IMPROVEMENT, 12" (SQ YD)
- (5) SHARED-USE PATH, PAID AS:
  - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 2"
  - AGGREGATE BASE COURSE, TYPE B, 4"
- (6) HMA BASE COURSE WIDENING 7 1/2"
- (7) HMA BASE COURSE 7 1/2"
- (8) SODDING, SALT TOLERANT
- (9) TOPSOIL, 6"
- (10) SEEDING CLASS 2A
- (11) PORTLAND CEMENT CONCRETE SIDEWALK 5"

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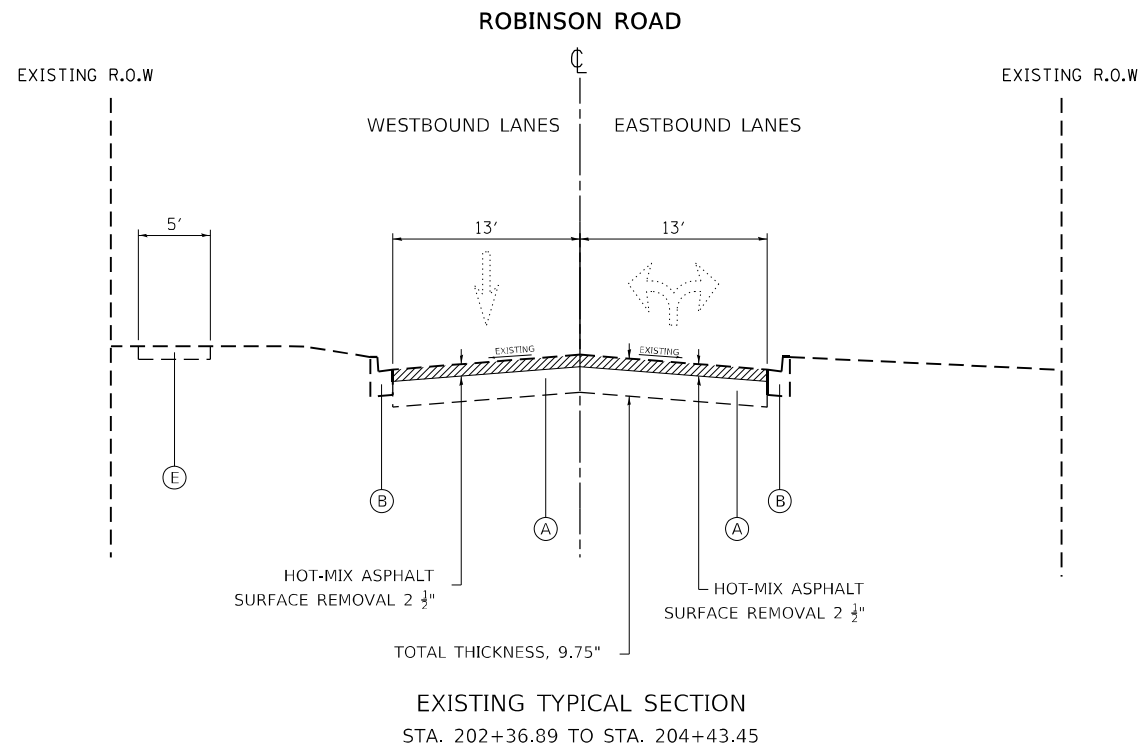
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS  
DES PLAINES RIVER ROAD AT ROBINSON ROAD**

SCALE: SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	11
CONTRACT NO. 60T17				
ILLINOIS FED. AID PROJECT				

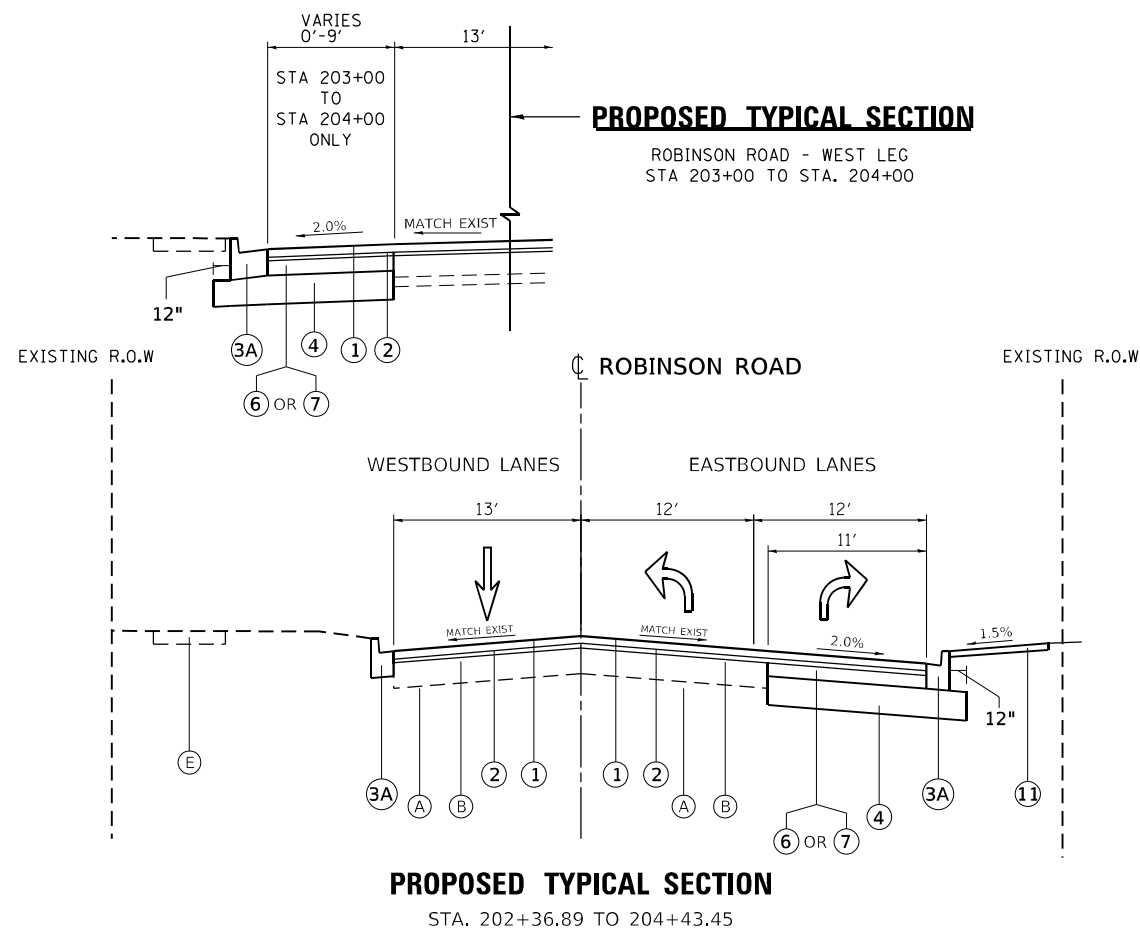


**LEGEND - EXISTING:**

- (A) HOT-MIX ASPHALT PAVEMENT 9.75" +/-
- (B) COMBINATION CONCRETE CURB AND GUTTER
- (C) HOT-MIX ASPHALT BASE COURSE
- (C2) HOT-MIX ASPHALT BASE COURSE WIDENING
- (D) PORTLAND CEMENT CONCRETE SIDEWALK
- (E) PCC SIDEWALK

**NOTES:**

- 1.) THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING.



**LEGEND - PROPOSED**

- ① POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA 9.5 MIX "F" N80, 1 3/4"
- ② POLYMERIZED HOT-MIX ASPHALT BINDER, IL-4.75, N50 3/4"
- ③ COMBINATION PCC CURB AND GUTTER, TYPE B-6.24
- ③A COMBINATION PCC CURB AND GUTTER, TYPE B-6.12
- ④ AGGREGATE SUBGRADE IMPROVEMENT, 12" (SQ YD)
- ⑤ SHARED-USE PATH, PAID AS:  
- HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 2"  
- AGGREGATE BASE COURSE, TYPE B, 4"
- ⑥ HMA BASE COURSE WIDENING 4 1/2"
- ⑦ HMA BASE COURSE 4 1/2"
- ⑧ SODDING, SALT TOLERANT
- ⑨ TOPSOIL, 6"
- ⑩ SEEDING CLASS 2A
- ⑪ PORTLAND CEMENT CONCRETE SIDEWALK 5"

② = LOCATIONS OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER/TECHNICIAN.

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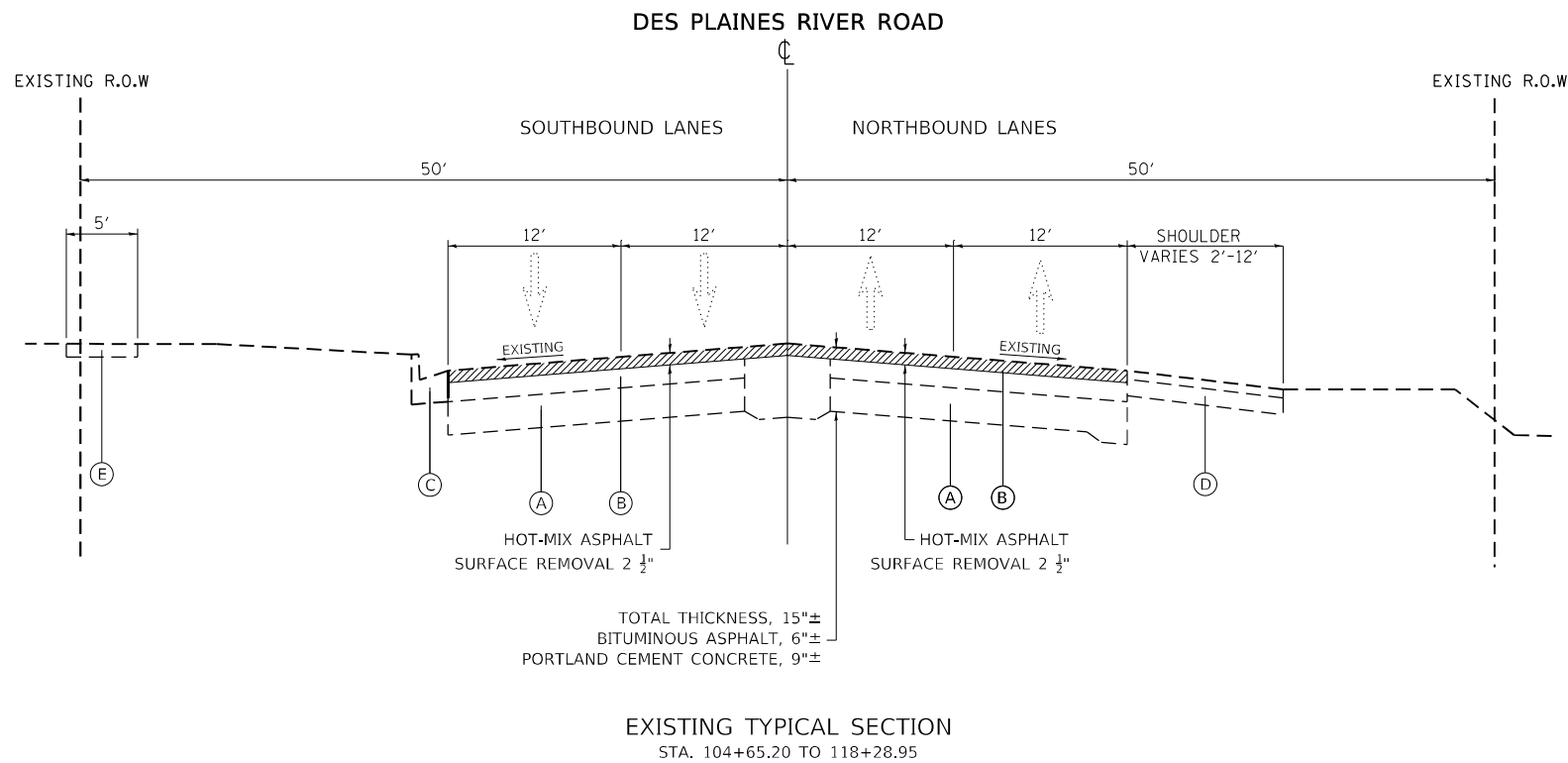
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS  
DES PLAINES RIVER ROAD AT ROBINSON ROAD**

SCALE: SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	12
CONTRACT NO. 60T17				
ILLINOIS FED. AID PROJECT				

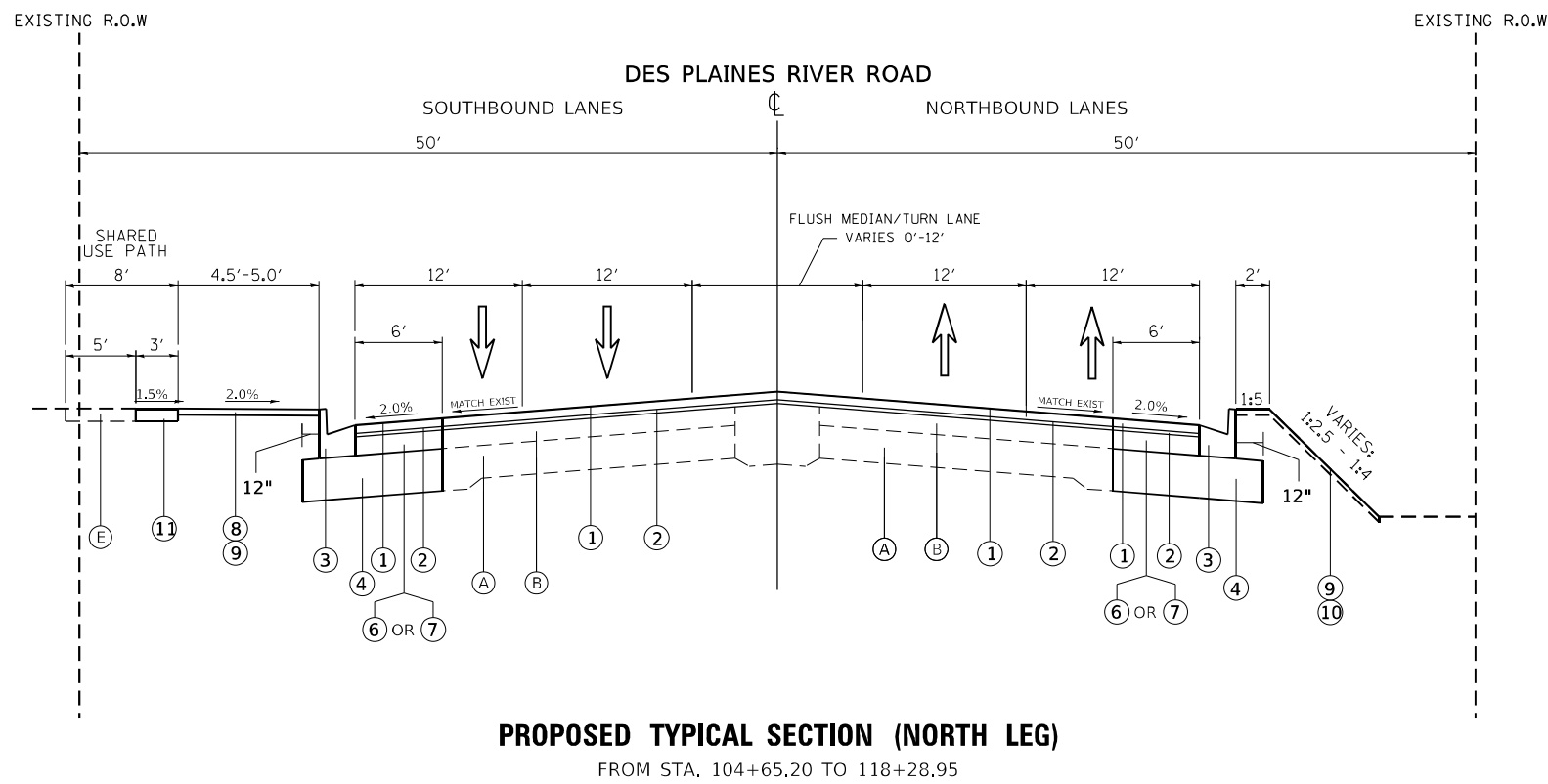


**LEGEND - EXISTING:**

- (A) PORTLAND CEMENT CONCRETE PAVEMENT ± 9"
- (B) HOT-MIX ASPHALT PAVEMENT ± 6"
- (C) COMBINATION CONCRETE CURB AND GUTTER
- (D) HOT-MIX ASPHALT BASE COURSE
- (E) HOT-MIX ASPHALT BASE COURSE WIDENING
- (F) AGGREGATE SHOULDER
- (G) PCC SIDEWALK

**NOTES:**

- 1.) THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING.



**LEGEND - PROPOSED**

- (1) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA 9.5 MIX "F" N80, 1 3/4"
- (2) POLYMERIZED HOT-MIX ASPHALT BINDER, IL-4.75, N50 3/4"
- (3) COMBINATION PCC CURB AND GUTTER, TYPE B-6.24
- (4) AGGREGATE SUBGRADE IMPROVEMENT, 12" (SQ YD)
- (5) SHARED-USE PATH, PAID AS:
  - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 2"
  - AGGREGATE BASE COURSE, TYPE B, 4"
- (6) HMA BASE COURSE WIDENING 7 1/2"
- (7) HMA BASE COURSE 7 1/2"
- (8) SODDING, SALT TOLERANT
- (9) TOPSOIL, 6"
- (10) SEEDING CLASS 2A
- (11) PORTLAND CEMENT CONCRETE SIDEWALK 5"

MODEL: Default  
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USER NAME = qarutnm	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 1/28/2021	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS  
DES PLAINES RIVER ROAD AT ROBINSON ROAD**

SCALE: SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	13
CONTRACT NO. 60T17				
ILLINOIS FED. AID PROJECT				

PIPES SCHEDULE

PIPE NUMBER	ALIGNMENT	PIPE TYPE	DIAMETER (in)	LENGTH (FT)	SLOPE	UPSTREAM STRUCTURE	DOWNSTREAM STRUCTURE	UPSTREAM INVERT ELEV.	DOWNSTREAM INVERT ELEV.
1	RIVER ROAD	STORM SEWER, CLASS A, TYPE 1	12	60	0.5%	1	2	619.47	619.17
2	RIVER ROAD	STORM SEWER, CLASS A, TYPE 2	15	100	0.4%	2	4	618.92	618.52
3	RIVER ROAD	STORM SEWER, CLASS A, TYPE 1	18	6	0.4%	4	H	618.27	618.25
4	RIVER ROAD	STORM SEWER, CLASS A, TYPE 1	12	60	0.5%	3	4	618.57	618.27
5	RIVER ROAD	STORM SEWER, CLASS A, TYPE 2	15	125	0.4%	6	4	619.02	618.52
6	RIVER ROAD	STORM SEWER, CLASS A, TYPE 1	12	60	0.5%	5	6	621.23	620.93
7	RIVER ROAD	STORM SEWER, CLASS A, TYPE 2	12	205	1.4%	8	6	622.14	619.27
8	RIVER ROAD	STORM SEWER, CLASS A, TYPE 1	12	60	0.5%	7	8	622.44	622.14
9	ROBINSON RD.	STORM SEWER, CLASS A, TYPE 1	12	22	5.0%	9	EX MH	623.98	622.88
10	ROBINSON RD.	STORM SEWER, CLASS A, TYPE 1	12	3	MIN. 0.5%	10	FX SS	*	MATCH EXIST SS
11	RIVER ROAD	STORM SEWER, CLASS A, TYPE 1	12	54	1.0%	11	12	624.42	623.88
12	RIVER ROAD	STORM SEWER, CLASS A, TYPE 1	12	25	1.1%	12	EX MH	623.88	623.60
13	RIVER ROAD	STORM SEWER, CLASS A, TYPE 2	12	88	1.0%	14	EX MH	622.36	621.48
14	RIVER ROAD	STORM SEWER, CLASS A, TYPE 1	12	49	1.0%	13	14	622.85	622.36
								* = SET PIPE AT MIN. 0.5%	

STRUCTURES SCHEDULE

STRUCTURE NO.	LOCATION			STRUCTURE TYPE			Rim Elev	Invert East	Invert South	Invert North	Invert West
	ALIGNMENT	STATION	OFFSET								
1	RIVER ROAD	108+50	30.6' LT	Catch Basin Type C	24"	Type 24 Frame & Grate	623.74	619.47			
2	RIVER ROAD	108+50	29.4' RT	Catch Basin Type A	48"	Type 24 Frame & Grate	623.75			618.92	619.17
3	RIVER ROAD	109+50	30.0' LT	Catch Basin Type C	24"	Type 24 Frame & Grate	623.42	618.57			
4	RIVER ROAD	109+50	30.0' RT	Catch Basin Type A	48"	Type 24 Frame & Grate	623.42	618.25	618.52	618.52	618.27
H	RIVER ROAD	109+50	39.0' RT	(TO CENTER OF FES)	PRECAST REINFORCED CONCRETE FLARED END SECTION 18"		618.25				
5	RIVER ROAD	110+75	30.9' LT	Catch Basin Type C	24"	Type 24 Frame & Grate	624.53	621.23			
6	RIVER ROAD	110+75	30.0' RT	Catch Basin Type A	48"	Type 24 Frame & Grate	624.53		619.02	619.27	620.93
7	RIVER ROAD	112+80	30.9' LT	Catch Basin Type C	24"	Type 24 Frame & Grate	627.41	622.44			
8	RIVER ROAD	112+80	30.0' RT	Catch Basin Type A	48"	Type 24 Frame & Grate	627.41		622.14		619.27
9	ROBINSON RD.	203+79	19.3' RT	Catch Basin Type C	24"	Type 24 Frame & Grate	628.10	623.98			
10	ROBINSON RD.	203+79	31.2' LT	Catch Basin Type A	24"	Type 24 Frame & Grate	627.63	MATCH EXIST SS			
11	RIVER ROAD	118+30	23.9' LT	Catch Basin Type C	24"	Type 24 Frame & Grate	627.42	624.42			
12	RIVER ROAD	118+30	24.0' RT	Catch Basin Type A	48"	Type 24 Frame & Grate	627.42			623.88	
13	RIVER ROAD	117+10	27.6' LT	Catch Basin Type C	24"	Type I Frame Open Lid	625.85	622.85			
14	RIVER ROAD	117+10	26.6' RT	Catch Basin Type A	48"	Type 24 Frame & Grate	625.85		622.36		

TREE REMOVAL SCHEDULE

STATION	OFFSET (FT.)	TREE NO.	6 TO 15 UNIT DIAMETERS	OVER 15 UNIT DIAMETERS	SPECIES	IMPACT
105+40	35' (RT)	339	7		Honey Locust	Remove
105+75	33' (RT)	338	10, 6		Siberian Elm	Remove
106+19	45' (RT)	337	12		Honey Locust	Remove
107+70	42' (RT)	335		19	Eastern Cottonwood	Remove
108+40	40' (RT)	334	9		American Elm	Remove
108+64	38' (RT)	333	14, 12		White Mulberry	Remove
109+25	37' (RT)	331	10		American Elm	Remove
109+35	37' (RT)	332	9		American Elm	Remove
109+62	34' (RT)	330		15	Black Locust	Remove
117+10	38' (LT)	351	10		Norway Maple	Remove
117+95	32' (RT)	328	9		American Elm	Remove
118+00	34' (RT)	327	7		Green Ash	Remove
<b>TOTAL</b>			115	34		

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 OFFICE: DISTRICT 1  
 PLOT DATE: 1/29/2021

USER NAME = qarutnm	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 1/29/2021	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SCHEDULE OF QUANTITIES  
DES PLAINES RIVER ROAD AT ROBINSON ROAD**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	14
CONTRACT NO. 60T17				
ILLINOIS FED. AID PROJECT				

EARTHWORK SCHEDULE FOR DES PLAINES RIVER RD.

LOCATION	EARTH EXCAVATION	EARTH EXCAVATION ADJUSTED FOR SHRINKAGE (SHRINKAGE FACTOR) 15%	EMBANKMENT	EMBANKMENT BALANCE WASTE (+) OR SHORTAGE (-)
	CUBIC YARDS	CUBIC YARDS	CUBIC YARDS	CUBIC YARDS
STA. 106+19.38 TO 106+50	37.37	31.76	2.88	+28.88
STA. 106+50 TO 107+00	65.26	55.47	4.50	+50.97
STA. 107+00 TO 107+50	73.24	62.25	5.31	+56.94
STA. 107+50 TO 108+00	86.17	73.24	6.81	+66.43
STA. 108+00 TO 108+50	82.22	69.89	6.50	+63.39
STA. 108+50 TO 108+83.44	50.95	43.31	7.16	+36.15
STA. 108+83.44 TO 109+61.96	118.91	101.07	50.43	+50.64
STA. 109+61.96 TO 110+16	87.62	74.48	26.42	+48.06
STA. 110+16 TO 110+50	54.17	46.04	1.27	+44.77
STA. 110+50 TO 111+00	82.91	70.47	1.56	+68.91
STA. 111+00 TO 111+29.30	60.81	51.69	0	+51.69
STA. 111+29.30 TO 112+00	159.44	135.52	0	+135.52
STA. 112+00 TO 112+48.08	107.04	90.98	0	+90.98
STA. 112+48.08 TO 113+00	107.42	91.31	0	+91.31
STA. 113+00 TO 113+50	89.91	76.42	0.67	+75.75
STA. 113+50 TO 114+08.01	121.37	103.16	2.38	+100.78
STA. 114+08.01 TO 115+00	170.22	144.69	5.96	+138.73
STA. 115+00 TO 115+50	61.56	52.33	3.67	+48.66
STA. 115+50 TO 116+00	73.07	62.11	3.61	+58.50
STA. 116+00 TO 116+62.73	117.37	99.76	2.30	+97.46
STA. 116+62.73 TO 117+00	67.86	57.68	0.03	+57.65
STA. 117+00 TO 117+49.75	77.46	65.84	0.28	+65.56
STA. 117+49.75 TO 117+84.98	62.51	53.13	0.20	+52.93
STA. 117+84.98 TO 118+28.94	67.83	57.66	3.63	+54.03
TOTAL	2082.69	1770.26	135.57	+1634.69

EARTHWORK SCHEDULE FOR ROBINSON RD.

LOCATION	EARTH EXCAVATION	EARTH EXCAVATION ADJUSTED FOR SHRINKAGE (SHRINKAGE FACTOR) 15%	EMBANKMENT	EMBANKMENT BALANCE WASTE (+) OR SHORTAGE (-)
	CUBIC YARDS	CUBIC YARDS	CUBIC YARDS	CUBIC YARDS
STA. 202+36.89 TO 202+50	7.53	6.40	0	+6.40
STA. 202+50 TO 202+68.78	13.50	11.47	0	+11.47
STA. 202+68.78 TO 203+00	31.17	26.49	0	+26.49
STA. 203+00 TO 203+50	89.91	76.42	0	+76.42
STA. 203+50 TO 204+00	55.80	47.43	0	+47.43
TOTAL	197.91	168.21	0	+168.21

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USER NAME = qarutnm	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SCHEDULE OF QUANTITIES DES PLAINES RIVER ROAD AT ROBINSON ROAD</b>		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -				2710	52-N	COOK	78	15
PLOT DATE = 1/25/2021	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.		CONTRACT NO. 60T17		ILLINOIS FED. AID PROJECT		
	DATE -	REVISED -								

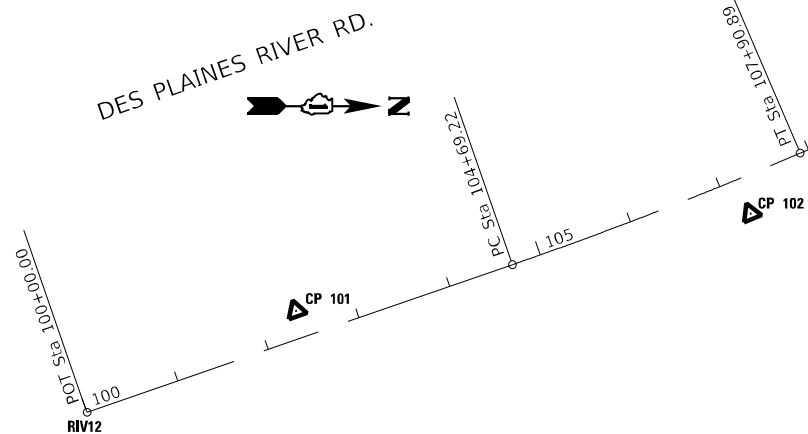
EXIST. CURVE RIVER\_RD-1  
 PI STA. = 106+30.13  
 ! = 4° 05' 44" (LT)  
 D = 1° 16' 24"  
 R = 4,500.00'  
 T = 160.90'  
 L = 321.67'  
 E = 2.88'  
 e = \_\_\_\_\_  
 T.R. = \_\_\_\_\_  
 S.E. RUN = \_\_\_\_\_  
 P.C. STA. = 104+69.22  
 P.T. STA. = 107+90.89

EXIST. CURVE RIVER\_RD-2  
 PI STA. = 113+24.68  
 ! = 7° 12' 07" (RT)  
 D = 0° 57' 18"  
 R = 6,000.00'  
 T = 377.58'  
 L = 754.17'  
 E = 11.87'  
 e = \_\_\_\_\_  
 T.R. = \_\_\_\_\_  
 S.E. RUN = \_\_\_\_\_  
 P.C. STA. = 109+47.10  
 P.T. STA. = 117+01.27

EXIST. CURVE RIVER\_RD-3  
 PI STA. = 117+79.84  
 ! = 1° 30' 02" (LT)  
 D = 0° 57' 18"  
 R = 6,000.00'  
 T = 78.57'  
 L = 157.13'  
 E = 0.51'  
 e = \_\_\_\_\_  
 T.R. = \_\_\_\_\_  
 S.E. RUN = \_\_\_\_\_  
 P.C. STA. = 117+01.27  
 P.T. STA. = 118+58.40

EXIST. CURVE RIVER\_RD-4  
 PI STA. = 126+50.36  
 ! = 6° 29' 05" (RT)  
 D = 2° 17' 31"  
 R = 2,500.00'  
 T = 141.63'  
 L = 282.95'  
 E = 4.01'  
 e = \_\_\_\_\_  
 T.R. = \_\_\_\_\_  
 S.E. RUN = \_\_\_\_\_  
 P.C. STA. = 125+08.73  
 P.T. STA. = 127+91.69

EXIST. CURVE ROBINSON\_RD-1  
 PI STA. = 201+40.26  
 ! = 25° 25' 22" (LT)  
 D = 11° 27' 33"  
 R = 500.00'  
 T = 112.78'  
 L = 221.85'  
 E = 12.56'  
 e = \_\_\_\_\_  
 T.R. = \_\_\_\_\_  
 S.E. RUN = \_\_\_\_\_  
 P.C. STA. = 200+27.48  
 P.T. STA. = 202+49.33

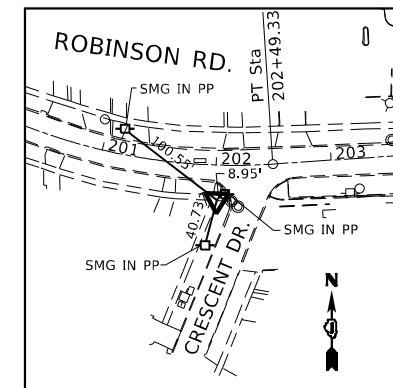


**ALIGNMENT COORDINATES**

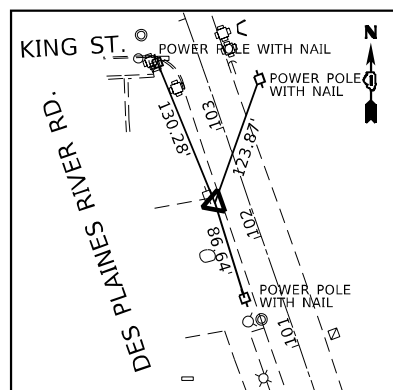
	N	E	STATION
PT# RBRD01	1,922,307.0089	1,114,153.3415	200+00.00
PC CURVE 1	1,922,296.8910	1,114,178.8858	200+27.48
PI CURVE 1	1,922,255.3575	1,114,283.7436	201+40.26
PT CURVE 1	1,922,262.8605	1,114,396.2776	202+49.33
PT# RBRD02	1,922,275.7742	1,114,589.9629	204+43.45

**ALIGNMENT COORDINATES**

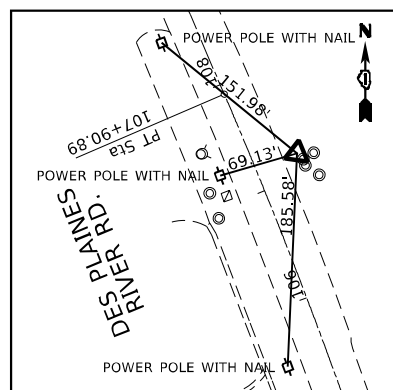
	N	E	STATION
PT# RIV12	1,920,956.5886	1,115,088.0206	100+00.00
PC CURVE 1	1,921,399.8603	1,114,934.1341	104+69.22
PI CURVE 1	1,921,551.8651	1,114,881.3640	106+30.13
PT CURVE 1	1,921,699.7129	1,114,817.8722	107+90.89
PC CURVE 2	1,921,843.2400	1,114,756.2359	109+47.10
PI CURVE 2	1,922,190.1843	1,114,607.2440	113+24.68
PT CURVE 2	1,922,553.0699	1,114,502.9221	117+01.27
PC CURVE 3	1,922,553.0699	1,114,502.9221	117+01.27
PI CURVE 3	1,922,628.5798	1,114,481.2146	117+79.84
PT CURVE 3	1,922,703.4955	1,114,457.5373	118+58.40
PC CURVE 4	1,923,323.6003	1,114,261.5514	125+08.73
PI CURVE 4	1,923,458.6436	1,114,218.8706	126+50.36
PT CURVE 4	1,923,597.6433	1,114,191.7146	127+91.69
PT# RIV13	1,923,715.1902	1,114,168.7498	129+11.46



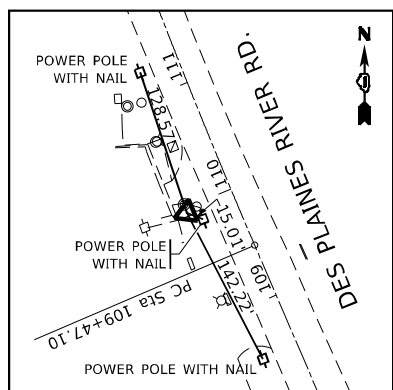
**CONTROL POINT #105**  
 SET "X" CROSS CUT IN SIDEWALK  
 202+01.90, 30.47' RT  
 ELEV. 629.23



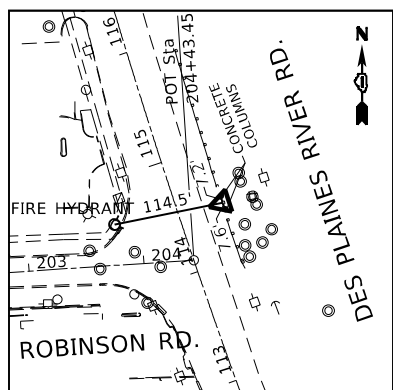
**CONTROL POINT #101**  
 MAGNETIC NAIL IN PAVEMENT  
 102+40.44, 28.39' LT  
 N. 1921174.420  
 E. 1114982.345  
 ELEV. 627.12



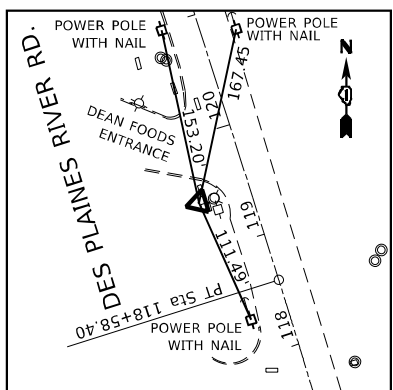
**CONTROL POINT #102**  
 IRON PIPE, WITH CAP  
 107+19.23, 38.31' RT  
 N. 1921648.1912  
 E. 1114881.0657  
 ELEV. 624.35



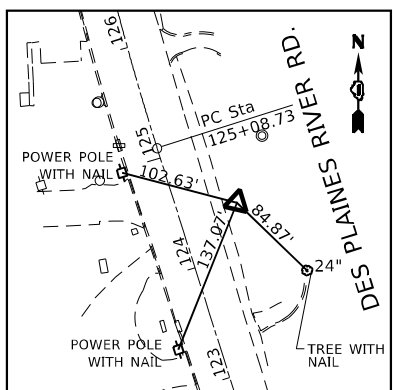
**CONTROL POINT #103**  
 IRON ROD WITH CAP  
 109+95.77, 43.18' LT  
 N. 1921871.3288  
 E. 1114697.3954  
 ELEV. 624.14



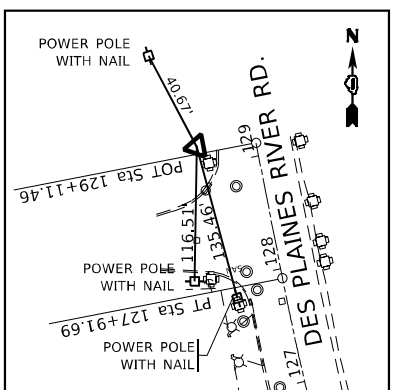
**CONTROL POINT #104**  
 FOUND "X" CROSS CUT IN SIDEWALK  
 114+50.16, 39.25' RT  
 N. 1922325.6689  
 E. 1114614.5655  
 ELEV. 627.88



**CONTROL POINT #106**  
 IRON ROD WITH CAP  
 119+43.24, 45.90' LT  
 N. 1922770.565  
 E. 1114388.203  
 ELEV. 625.40



**CONTROL POINT #107**  
 IRON ROD WITH CAP  
 124+42.90, 51.69' RT  
 N. 1923276.4043  
 E. 1114330.6763  
 ELEV. 626.10



**CONTROL POINT #108**  
 IRON ROD WITH CAP  
 129+17.94, 51.24' LT  
 N. 1923711.726  
 E. 1114117.217  
 ELEV. 629.80

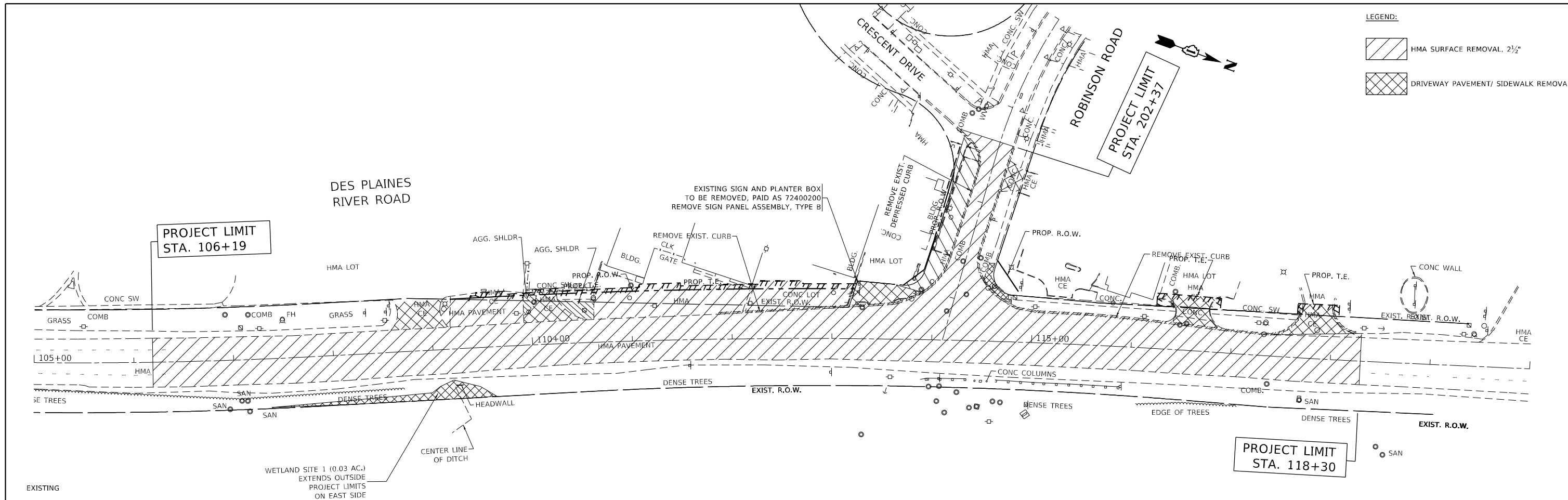
**BENCHMARK #1**  
 ELEV. 629.22  
 SOUTHWEST FLANGE BOLT OF FIRE HYDRANT  
 AT NORTHWEST CORNER OF DES PLAINES RIVER ROAD  
 AND KING AVENUE

**BENCHMARK #2**  
 ELEV. 631.17  
 TOP FLANGE BOLT OF FIRE HYDRANT AT  
 NORTHWEST CORNER OF DES PLAINES RIVER ROAD  
 AND ROBINSON ROAD

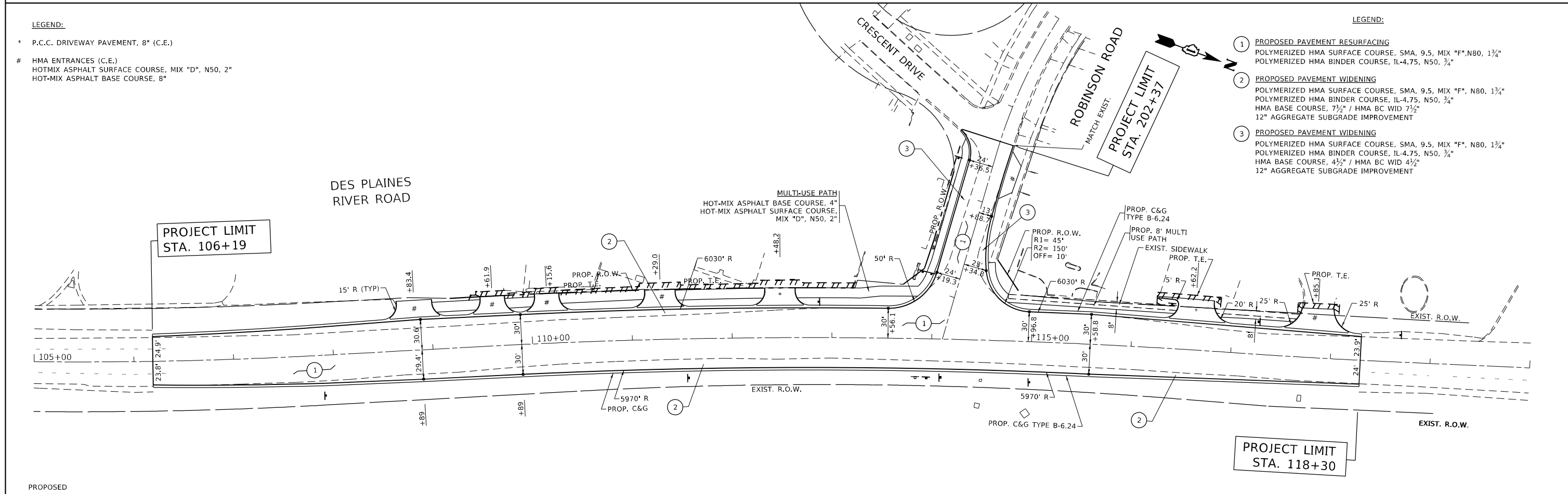
**BENCHMARK #3**  
 ELEV. 630.47  
 TOP FLANGE BOLT OF FIRE HYDRANT  
 215' SOUTH OF IVANHOE ROAD  
 ON WEST SIDE OF DES PLAINES RIVER ROAD

**BENCHMARK #4**  
 ELEV. 644.73  
 BRASS DISK ON NORTH FACE  
 FOUNDATION OF SCHOOL  
 DISTRICT 83 BUILDING (ME1663)





- LEGEND:
- HMA SURFACE REMOVAL, 2 1/2"
  - DRIVEWAY PAVEMENT/ SIDEWALK REMOVAL

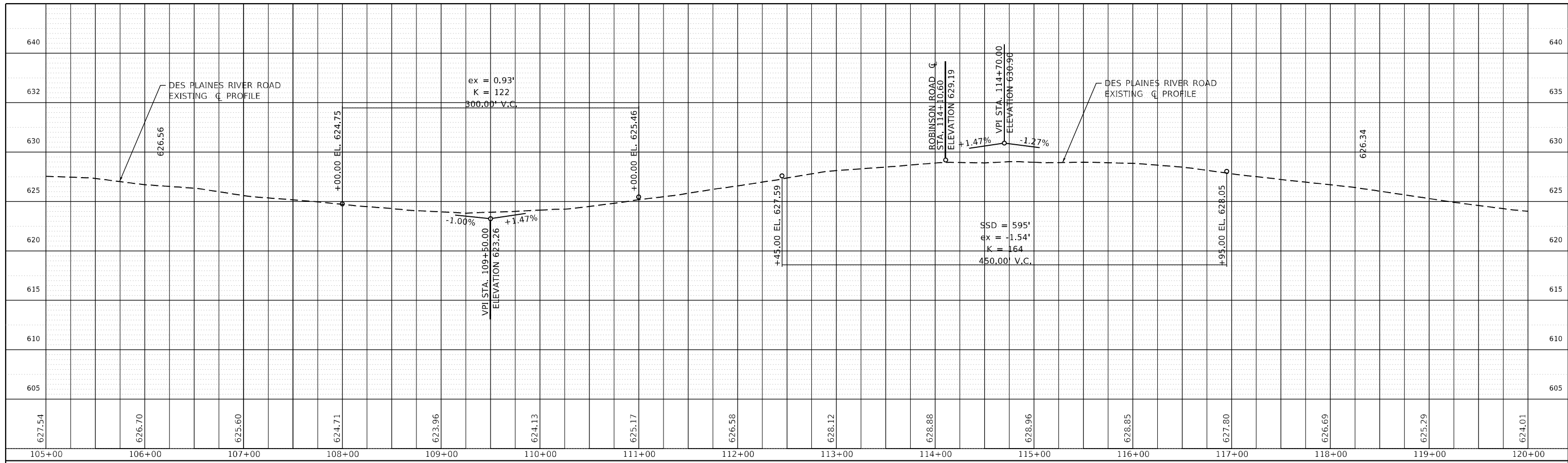


- LEGEND:
- \* P.C.C. DRIVEWAY PAVEMENT, 8" (C.E.)
  - # HMA ENTRANCES (C.E.)  
HOTMIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"  
HOT-MIX ASPHALT BASE COURSE, 8"

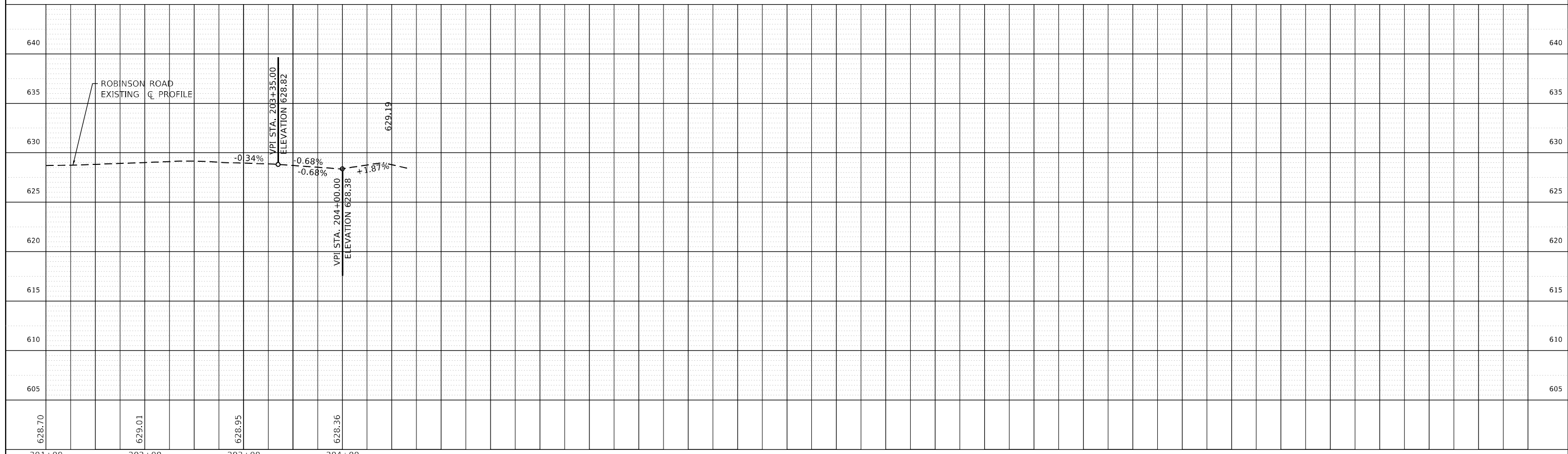
- LEGEND:
- 1 PROPOSED PAVEMENT RESURFACING  
POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 1 3/4"  
POLYMERIZED HMA BINDER COURSE, IL-4.75, N50, 3/4"
  - 2 PROPOSED PAVEMENT WIDENING  
POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 1 3/4"  
POLYMERIZED HMA BINDER COURSE, IL-4.75, N50, 3/4"  
HMA BASE COURSE, 7 1/2" / HMA BC WID 7 1/2"  
12" AGGREGATE SUBGRADE IMPROVEMENT
  - 3 PROPOSED PAVEMENT WIDENING  
POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 1 3/4"  
POLYMERIZED HMA BINDER COURSE, IL-4.75, N50, 3/4"  
HMA BASE COURSE, 4 1/2" / HMA BC WID 4 1/2"  
12" AGGREGATE SUBGRADE IMPROVEMENT

FILE NAME =	USER NAME = qerutnm	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EXISTING AND PROPOSED ROADWAY PLAN DES PLAINES RIVER ROAD AT ROBINSON ROAD</b>	F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\plnroom\dot.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\PI72009\Drawings\CADsheets\PI72009-shr-plnpr.f.d		CHECKED -	REVISED -			2710	52-N	COOK	78	17
PLOT SCALE = 100.0000' / in.		DATE -	REVISED -			CONTRACT NO. 60T17				
Default		PLOT DATE = 1/25/2021	DATE -			ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	NO.		
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NOTE BOOK	PLOTTED		
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FILE NAME=	USER NAME= qarutm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED ROADWAY PROFILE				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\planroom.dot.illinois.gov:PWIDOTDocuments\IDOT Offices\District 1\Projects\172009\172009 Data\CADsheets\P172009-sht-ph		CHECKED -	REVISED -		DES PLAINES RIVER ROAD AT ROBINSON ROAD				2710	52-N	COOK	78	18
Default	PLOT SCALE= 100.0000 ' / in.	DATE -	REVISED -		HOR. SCALE= 1"= 50'				CONTRACT NO. 60T17				
	PLOT DATE = 1/25/2021		REVISED -		VER. SCALE= 1"= 5'				SHEET	OF	SHEETS	STA.	TO

ILLINOIS AID PROJECT

**SUGGESTED STAGE CONSTRUCTION GENERAL NOTES**

1. THE CONTRACTOR WILL GIVE THE ENGINEER AT LEAST 10 DAYS NOTICE PRIOR TO ANY TRAFFIC STAGING CHANGES.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COVERING OR REMOVING ANY EXISTING ROADWAY SIGNAGE THAT CONFLICTS WITH THE STAGED TRAFFIC PATTERN TO THE SATISFACTION OF THE RESIDENT ENGINEER. TEMPORARY TRAFFIC CONTROL BARRIERS AND SIGNAGE SHALL BE IN PLACE PRIOR TO TRAFFIC STAGING.
3. PEDESTRIAN AND BICYCLE ACCESS MUST BE MAINTAINED ON ALL EXISTING FACILITIES AND ON NEW FACILITIES AS THEY BECOME AVAILABLE FOR PEDESTRIAN AND BICYCLE TRAFFIC.
4. A MONO-DIRECTIONAL FLASHING AMBER BEACON SHALL BE MOUNTED TO THE FIRST TWO WARNING SIGNS ON EACH APPROACH DURING HOURS OF DARKNESS.
5. STOP SIGNS AND STOP BARS ARE TO BE MAINTAINED FOR UNSIGNALIZED SIDE STREETS AND DRIVEWAYS THROUGH ALL CONSTRUCTION STAGES IN WHICH THEY ARE TO BE MAINTAINED.
6. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE INFORMATIONAL SIGNS ON TEMPORARY SUPPORTS FOR DRIVEWAYS. THESE SIGNS SHALL BE WHITE ON GREEN IN ACCORDANCE WITH THE MANUAL FOR UNIFORM TRAFFIC CONTROL DEVICES. ALSO, "CAUTION NEW LANES OPEN STOP HERE" SIGNS WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE AT THE APPROPRIATE LOCATIONS. THIS WORK WILL BE PER DISTRICT DETAIL TC-26.
7. POSITIVE DRAINAGE WITHIN THE WORK ZONE MUST BE MAINTAINED AT ALL TIMES TO THE SATISFACTION OF THE RESIDENT ENGINEER. WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, INCLUDING THE FLOW LINE OF DITCHES, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY INLETS, OUTLETS, AND CONNECTIONS FOR ALL EXISTING AND PROPOSED FACILITIES INCLUDING TEMPORARY PUMPING IF NECESSARY. TEMPORARY ACCOMMODATIONS SHALL BE MAINTAINED UNTIL SUCH TIME AS THE PERMANENT CONNECTIONS WITH SEWERS ARE BUILT AND IN SERVICE AND THE FINAL SHAPING AND GRADING OF DITCHES IS PERFORMED. THE COST OF ALL LABOR, EQUIPMENT, AND MATERIALS (TEMPORARY OR PERMANENT USED AS TEMPORARY) TO COMPLY WITH THIS REQUIREMENT WILL NOT BE PAID FOR DIRECTLY, BUT THE COST SHALL BE CONSIDERED INCLUDED IN THE PROPOSED ITEMS OF WORK IN THE CONTRACT.
8. IF THE CONTRACTOR MUST FULLY CLOSE AN EXISTING DRIVEWAY OR SIDE STREET, THE CONTRACTOR MUST MAINTAIN A TEMPORARY ACCESS. THE CONTRACTOR SHALL GIVE AT LEAST ONE WEEK PRIOR WRITTEN NOTICE OF DRIVEWAY OR SIDE STREET CLOSURES TO THE ENGINEER, THE VILLAGE, EMERGENCY SERVICES, SCHOOLS, AND THE LOCAL POST OFFICE. DIRECTIONAL SIGNAGE SHALL BE PROVIDED TO REDIRECT DRIVERS AND PATRONS OF AFFECTED BUSINESSES TO ACCESS PROPERTIES BY ALTERNATE ROUTES. THIS WORK SHALL BE COORDINATED BY THE RESIDENT ENGINEER. ANY REDIRECTING SIGNAGE SHALL BE PAID FOR AS "TEMPORARY INFORMATION SIGNAGE" AND SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE. ANY ITEMS FOR CLOSURE OF THE DRIVES INCLUDING TYPE III BARRICADES SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED WITH THE COST OF WORK ITEMS.
9. THE CONTRACTOR SHALL MAINTAIN ALL DRIVEWAY AND SIDE STREET ENTRANCES AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. ACCESS TO EXISTING DRIVEWAY ENTRANCES SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "TEMPORARY ACCESS (PRIVATE OR COMMERCIAL)".
10. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL SIDE STREETS AND DRIVEWAYS BY UTILIZING STAGED CONSTRUCTION, FLAGGERS, TEMPORARY ACCESSES, OR OTHER METHODS APPROVED BY THE ENGINEER. THIS WORK SHALL NOT BE CONSIDERED FOR ADDITIONAL PAYMENT, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEMS OF WORK.
11. THE CONTRACTOR SHALL ENSURE THATS ALL DROP-OFFS GREATER THAN 24" LOCATED WITHIN 8 FEET OF THE EDGE OF THE NEAREST OPEN TRAFFIC LANE WILL NOT REMAIN FOR MORE THAN 24 HOURS.

**SUGGESTED SEQUENCE OF CONSTRUCTION:  
DES PLAINES RIVER ROAD AT ROBINSON ROAD**

PRE-STAGE

INSTALL ADVANCE NOTICE CONSTRUCTION SIGNS

PROPOSED WORK TO BE COMPLETED WITH DAILY LANE CLOSURES USING HIGHWAY STANDARDS: 701606, 701611, 701701, 701801, 701901 TO PROVIDE SAFE WORK ZONE AREAS.

STAGE 1\*

PRIOR TO THE START OF STAGE 1, REMOVE ALL CONFLICTING PAVEMENT MARKINGS.

INSTALL TEMPORARY EROSION CONTROL.

REMOVAL OF EXISTING PAVEMENT, COMBINATION CURB & GUTTER, DRAINAGE PIPES & STRUCTURES AND SIDEWALK WITHIN THE WORK ZONE.

CONSTRUCT PROPOSED DRAINAGE PIPES & STRUCTURES, EARTH EXCAVATION, PAVEMENT WIDENING, AND COMBINATION CURB & GUTTER. PAVEMENT WIDENING SHALL BE INSTALLED ON DES PLAINES RIVER ROAD NB LANES ONLY TO BINDER.

TOPSOIL PLACEMENT

INSTALL TEMPORARY SEEDING, OR TEMPORARY EROSION CONTROL BLANKET ON AREAS AFFECTED BY THE IMPROVEMENTS.

DAILY LANE CLOSURES FOR PROVIDING SAFE WORK ZONE AREAS MAY BE DONE WITH HIGHWAY STANDARDS 701606, 701611, 701701, 701801 AND 701901.

STAGE 2\*

PRIOR TO THE START OF STAGE 2, REMOVE ALL CONFLICTING PAVEMENT MARKINGS.

REMOVAL OF EXISTING PAVEMENT, COMBINATION CURB & GUTTER, DRAINAGE PIPES & STRUCTURES AND SIDEWALK WITHIN THE WORK ZONE.

CONSTRUCT PROPOSED DRAINAGE PIPES & STRUCTURES, EARTH EXCAVATION, PAVEMENT WIDENING, COMBINATION CURB & GUTTER, SIDEWALK/MULTI-USE PATH WITHIN THE WORK ZONE. PAVEMENT WIDENING SHALL BE INSTALLED ON DES PLAINES RIVER ROAD SB LANES ONLY INCLUDING EB OF ROBINSON ROAD TO BINDER ONLY.

RECONSTRUCT DRIVEWAYS AND ENTRANCES

TOPSOIL PLACEMENT

INSTALL TEMPORARY SEEDING, OR TEMPORARY EROSION CONTROL BLANKET ON AREAS AFFECTED BY THE IMPROVEMENTS.

DAILY LANE CLOSURES FOR PROVIDING SAFE WORK ZONE AREAS MAY BE DONE WITH HIGHWAY STANDARDS 701606, 701611, 701701, 701801 AND 701901.

STAGE 3\*

PRIOR TO THE START OF STAGE 2, REMOVE ALL CONFLICTING PAVEMENT MARKINGS.

INSTALL TEMPORARY EROSION CONTROL.

REMOVAL OF EXISTING PAVEMENT, COMBINATION CURB & GUTTER, DRAINAGE PIPES & STRUCTURES AND SIDEWALK WITHIN THE WORK ZONE.

CONSTRUCT PROPOSED DRAINAGE PIPES & STRUCTURES, EARTH EXCAVATION, PAVEMENT WIDENING, COMBINATION CURB & GUTTER, SIDEWALK/MULTI-USE PATH WITHIN THE WORK ZONE. PAVEMENT WIDENING SHALL BE INSTALLED ON DES PLAINES RIVER ROAD SB LANES ONLY INCLUDING WB OF ROBINSON ROAD TO BINDER ONLY.

RECONSTRUCT DRIVEWAYS AND ENTRANCES

TOPSOIL PLACEMENT

INSTALL TEMPORARY SEEDING, OR TEMPORARY EROSION CONTROL BLANKET ON AREAS AFFECTED BY THE IMPROVEMENTS.

DAILY LANE CLOSURES FOR PROVIDING SAFE WORK ZONE AREAS MAY BE DONE WITH HIGHWAY STANDARDS 701606, 701611, 701701, 701801 AND 701901.

INSTALL PERMANENT TRAFFIC SIGNALS

INSTALL HMA SURFACE COURSE

INSTALL PERMANENT PAVEMENT MARKINGS

INSTALL FINAL ROADWAY SIGNS

OPEN ALL LANES TO TRAFFIC

INSTALL SOD

ACTIVATE PERMANENT TRAFFIC SIGNALS

REMOVE CONSTRUCTION SIGNS

\* = MOVING CLOSURES DURING OFF-PEAK HOURS MAY BE NEEDED TO PROVIDE SAFE WORK ZONE AREAS.

FOR STATIONARY LANE CLOSURES USE HIGHWAY STANDARDS 701602, 701606, 701611, 701701, 701801 AND 701901.

FOR STRIPING ONLY, UTILIZE HIGHWAY STANDARD 701427.

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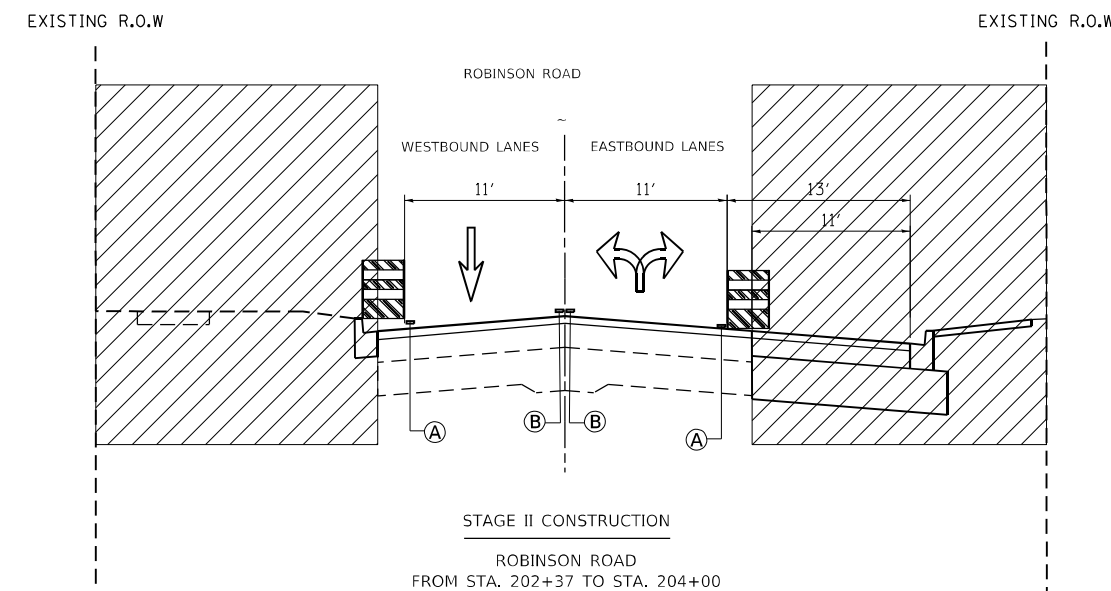
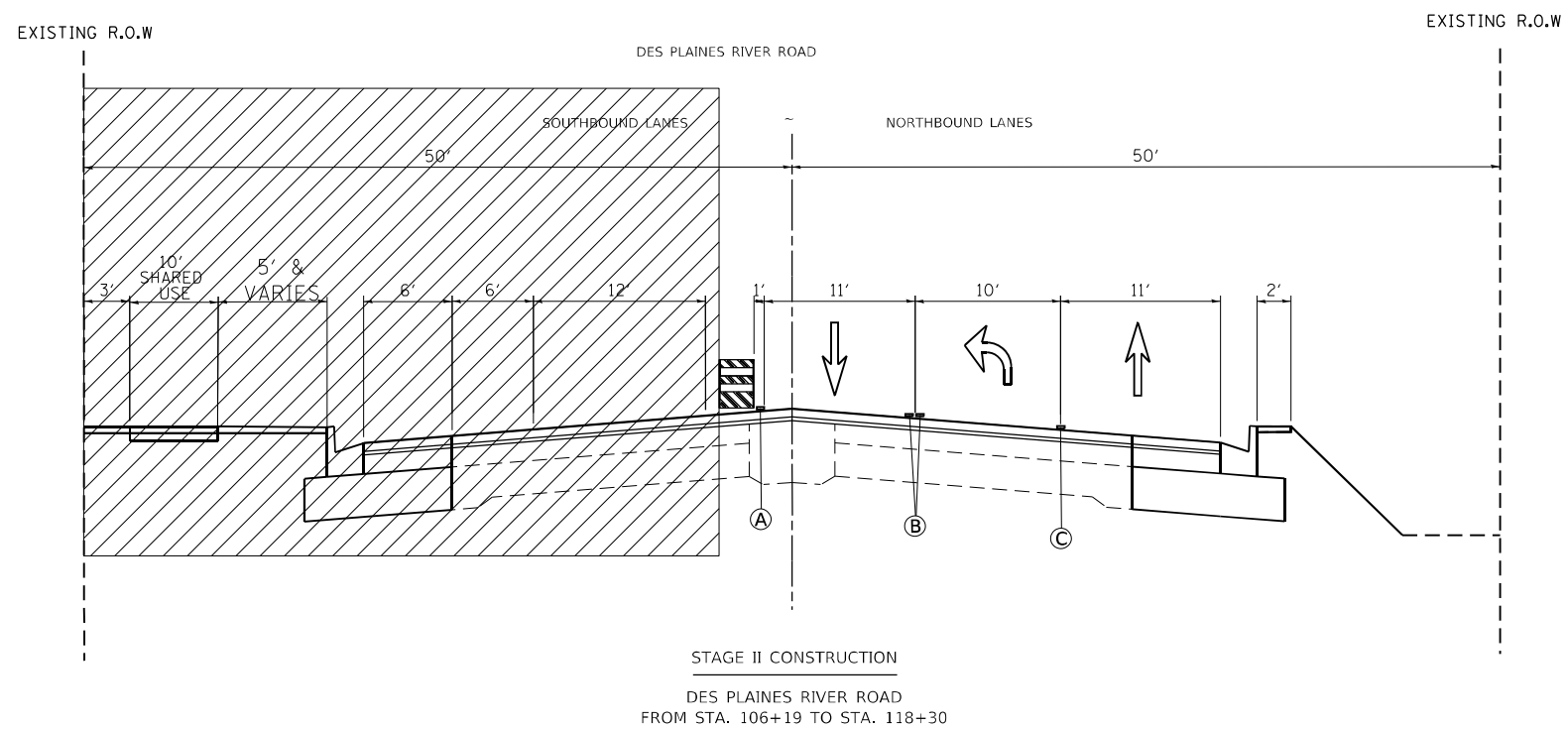
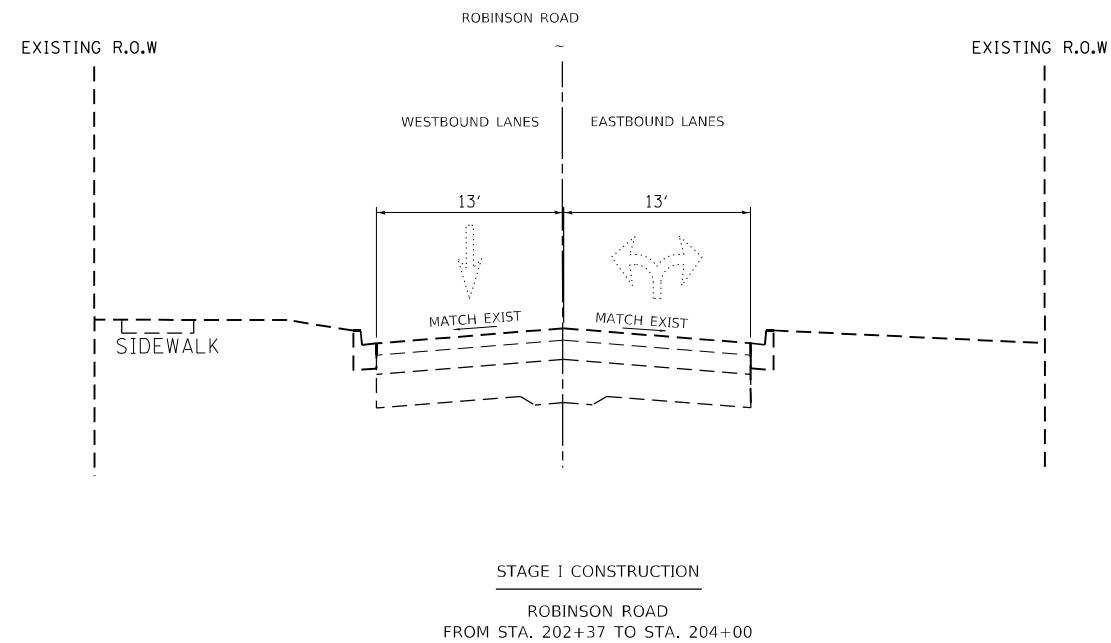
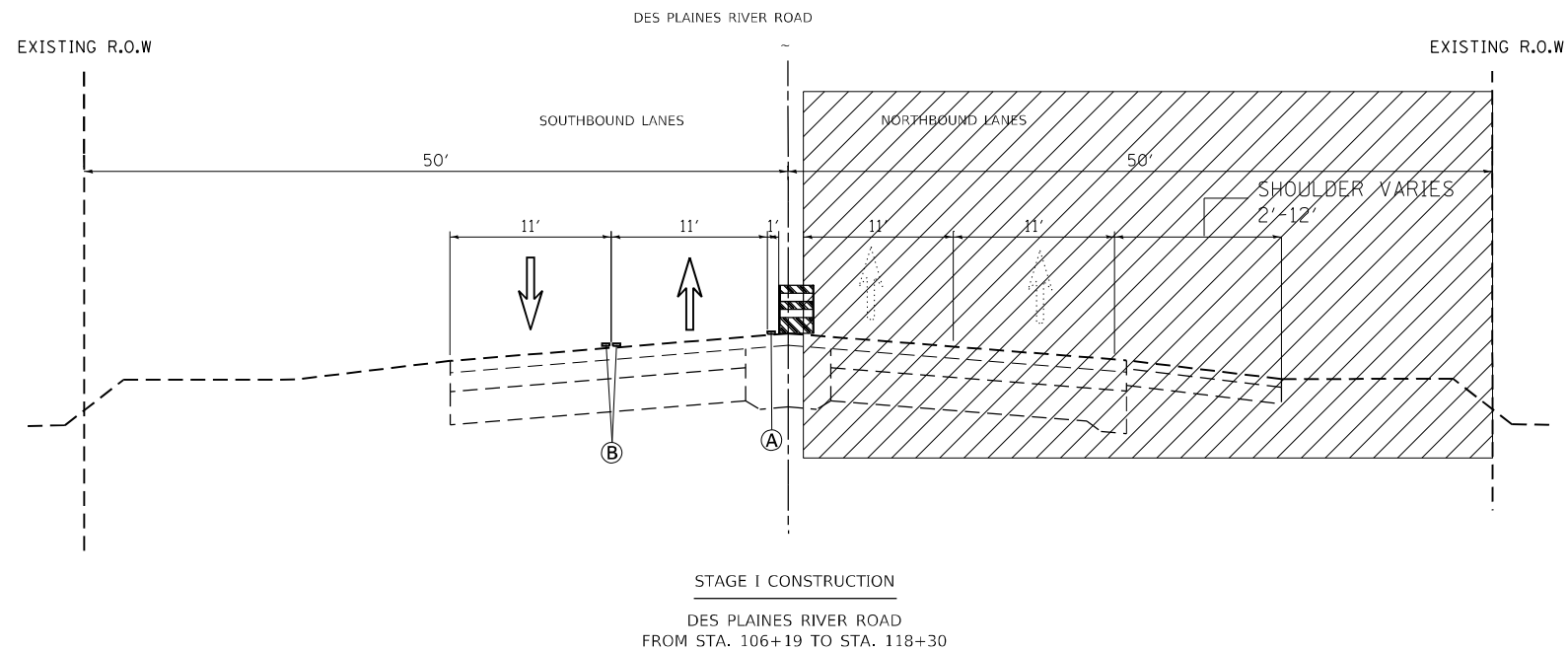
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC GENERAL NOTES  
DES PLAINES RIVER ROAD AT ROBINSON ROAD**






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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	19
				CONTRACT NO. 60T17
		ILLINOIS	FED. AID PROJECT	



\* NOTE: SEE ROADWAY TYPICAL SECTIONS FOR DETAILED PROPOSED TYPICAL SECTION

LEGEND:

-  WORK ZONE
-  DRUMS OR TY II BARRICADE AT 50 FT SPACING  
DEVICES IN TAPER (20 FT C-C)  
DEVICES IN CURVES & RADII (25 FT C-C)
-  TEMPORARY PAVEMENT MARKING, TYPE IV  
4" SOLID WHITE EDGE LINE
-  4" SOLID DOUBLE YELLOW @ 11" C-C WITH  
TEMPORARY RAISED PAVEMENT MARKER
-  TEMPORARY PAVEMENT MARKING, TYPE IV  
6" SOLID WHITE EDGE

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

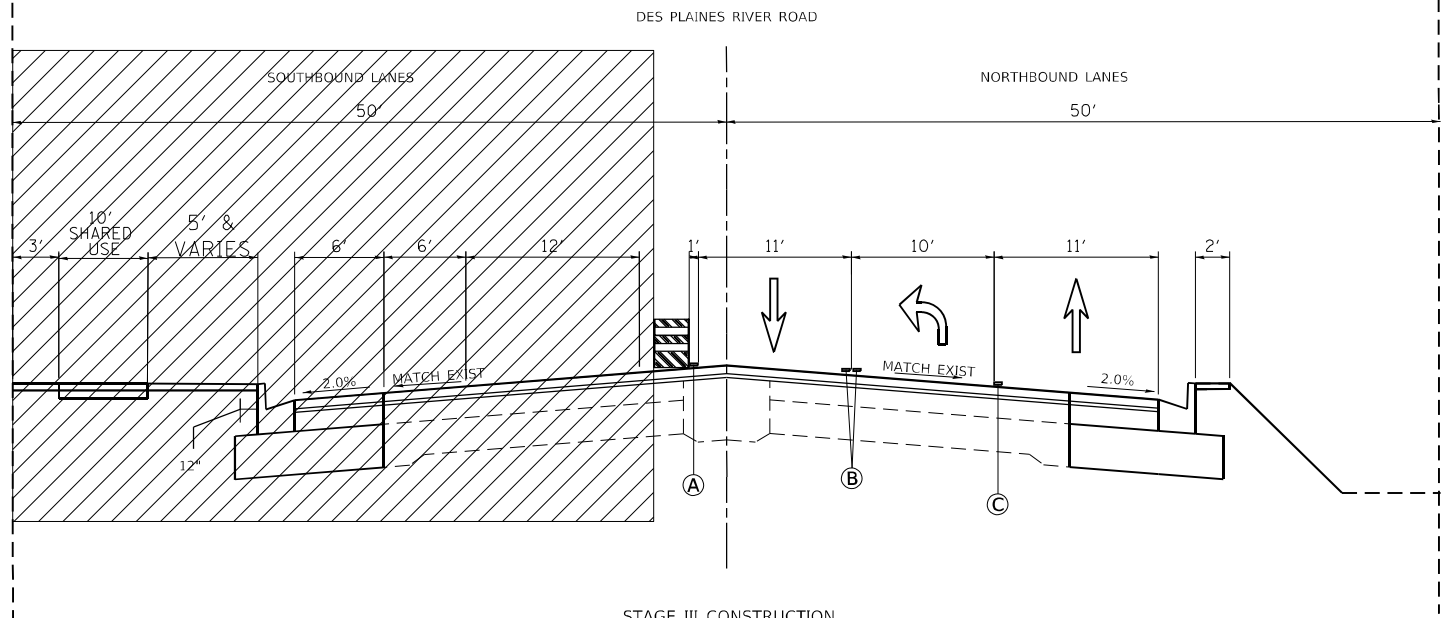
MAINTENANCE OF TRAFFIC TYPICAL SECTIONS  
DES PLAINES RIVER ROAD AT ROBINSON ROAD

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	20
CONTRACT NO. 60T17				
ILLINOIS FED. AID PROJECT				

EXISTING R.O.W

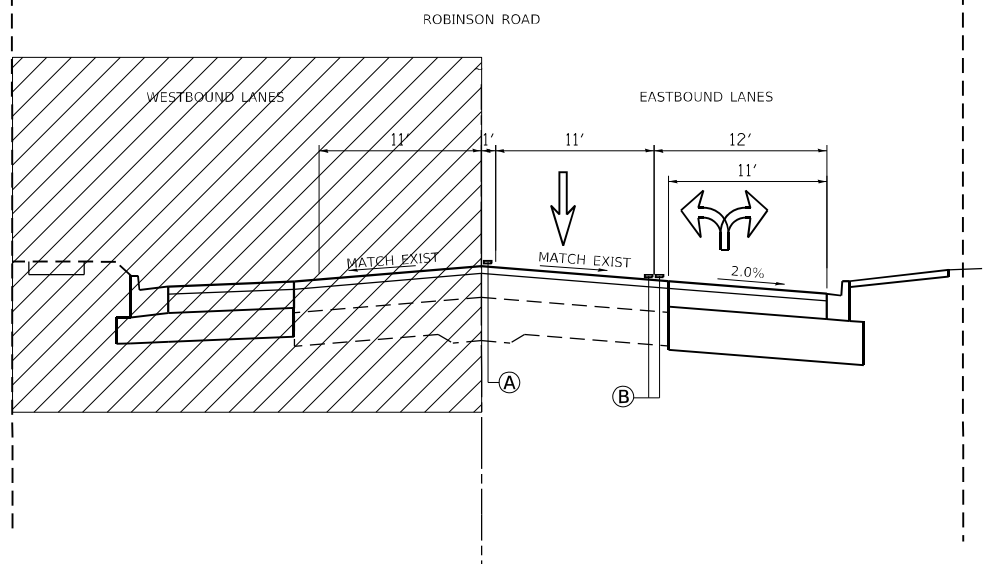
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STAGE III CONSTRUCTION  
 DES PLAINES RIVER ROAD  
 FROM STA. 106+19 TO STA. 118+30

EXISTING R.O.W



EXISTING R.O.W



STAGE III CONSTRUCTION  
 DES PLAINES RIVER ROAD  
 FROM STA. 106+19 TO STA. 118+30

\* NOTE: SEE ROADWAY TYPICAL SECTIONS  
 FOR DETAILED PROPOSED TYPICAL SECTION

LEGEND:

-  WORK ZONE
-  DRUMS OR TY II BARRICADE AT 50 FT SPACING  
DEVICES IN TAPER (20 FT C-C)  
DEVICES IN CURVES & RADII (25 FT C-C)
- Ⓐ TEMPORARY PAVEMENT MARKING, TYPE IV  
4" SOLID WHITE EDGE LINE
- Ⓑ 4" SOLID DOUBLE YELLOW @ 11" C-C WITH  
TEMPORARY RAISED PAVEMENT MARKER
- Ⓒ TEMPORARY PAVEMENT MARKING, TYPE IV  
6" SOLID WHITE EDGE

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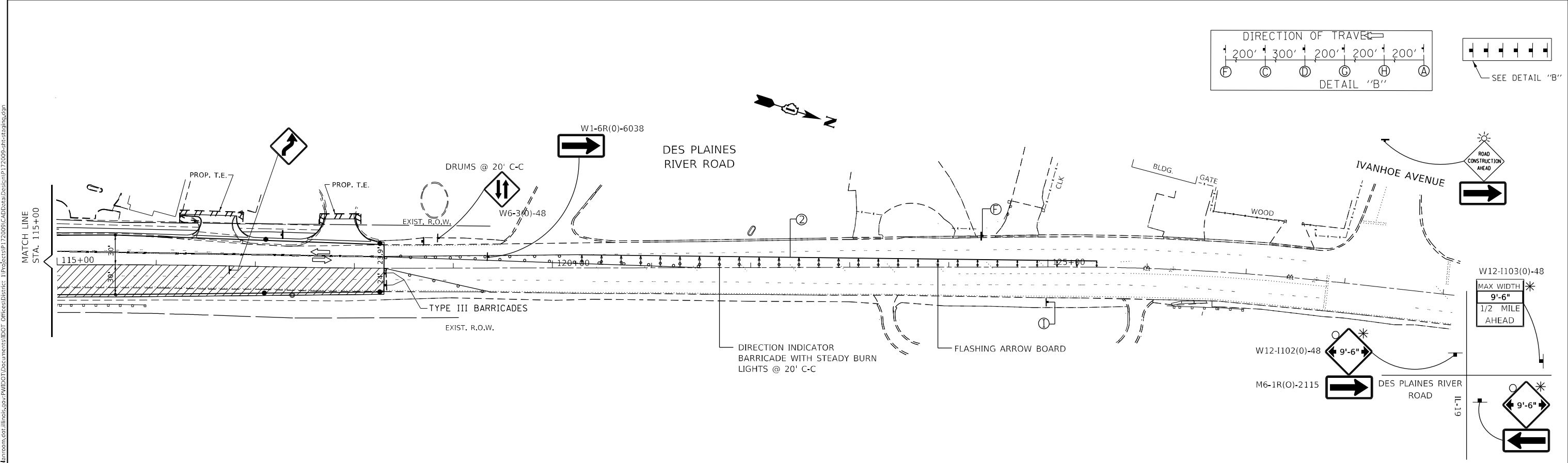
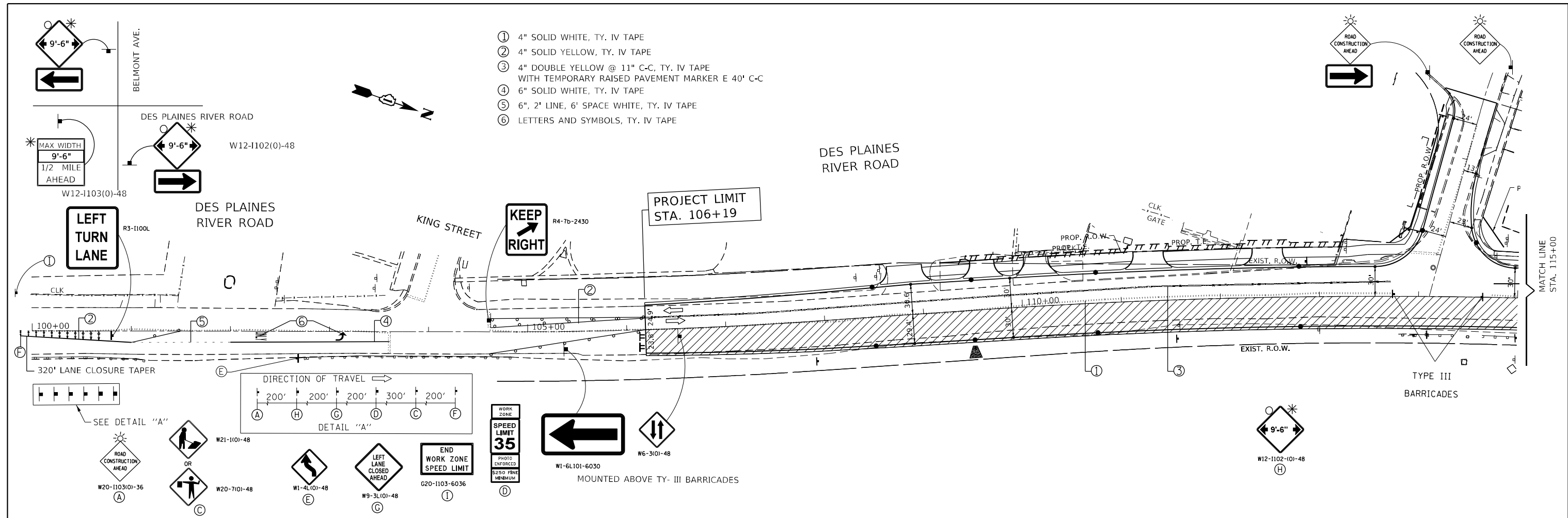
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC TYPICAL SECTIONS  
 DES PLAINES RIVER ROAD AT ROBINSON ROAD

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	21
CONTRACT NO. 60T17				
ILLINOIS FED. AID PROJECT				



- ① 4" SOLID WHITE, TY. IV TAPE
- ② 4" SOLID YELLOW, TY. IV TAPE
- ③ 4" DOUBLE YELLOW @ 11" C-C, TY. IV TAPE WITH TEMPORARY RAISED PAVEMENT MARKER E 40' C-C
- ④ 6" SOLID WHITE, TY. IV TAPE
- ⑤ 6", 2' LINE, 6' SPACE WHITE, TY. IV TAPE
- ⑥ LETTERS AND SYMBOLS, TY. IV TAPE

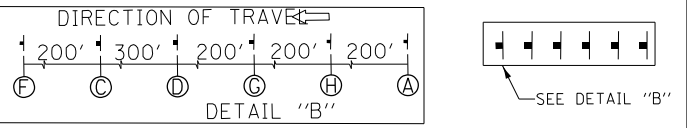
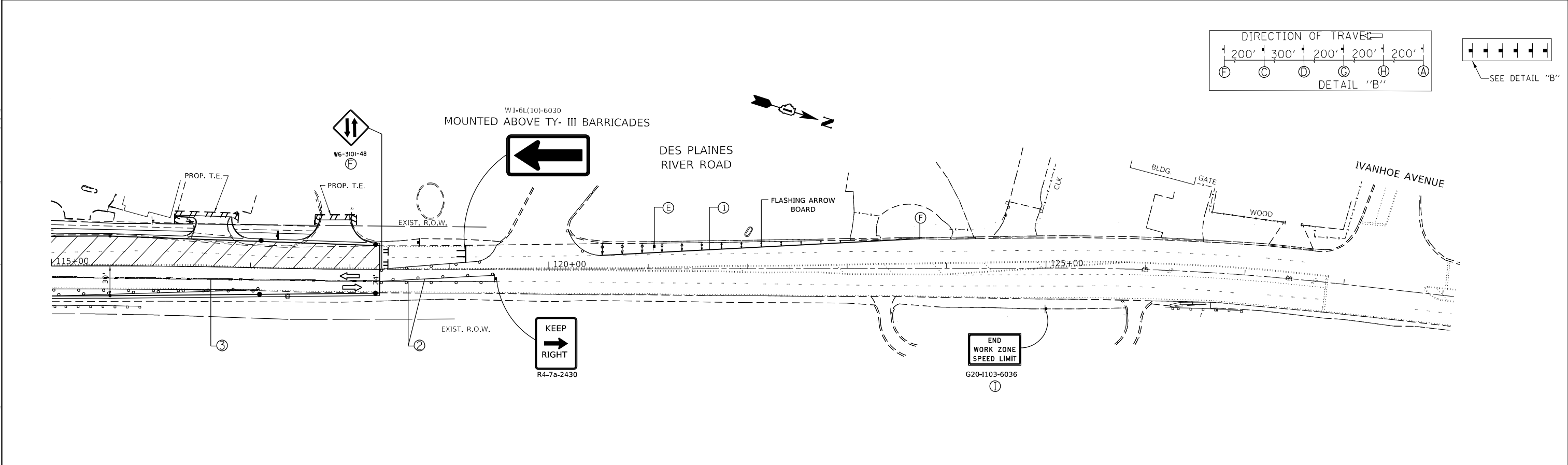
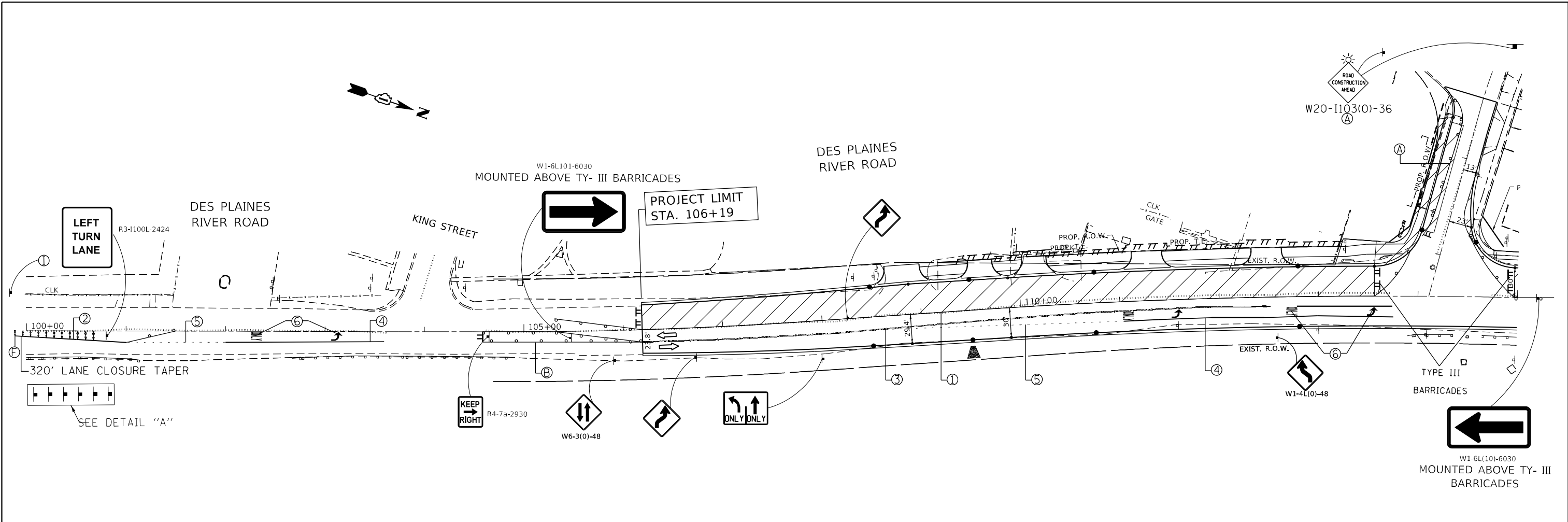
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	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>MAINTENANCE OF TRAFFIC PLAN STAGE 1 DES PLAINES RIVER ROAD AT ROBINSON ROAD</b>	
SCALE:	SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	22
CONTRACT NO. 60T17				
ILLINOIS FED. AID PROJECT				

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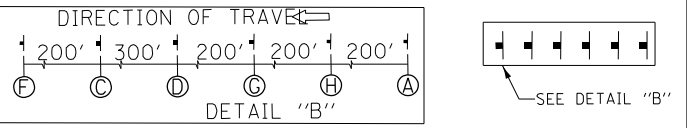
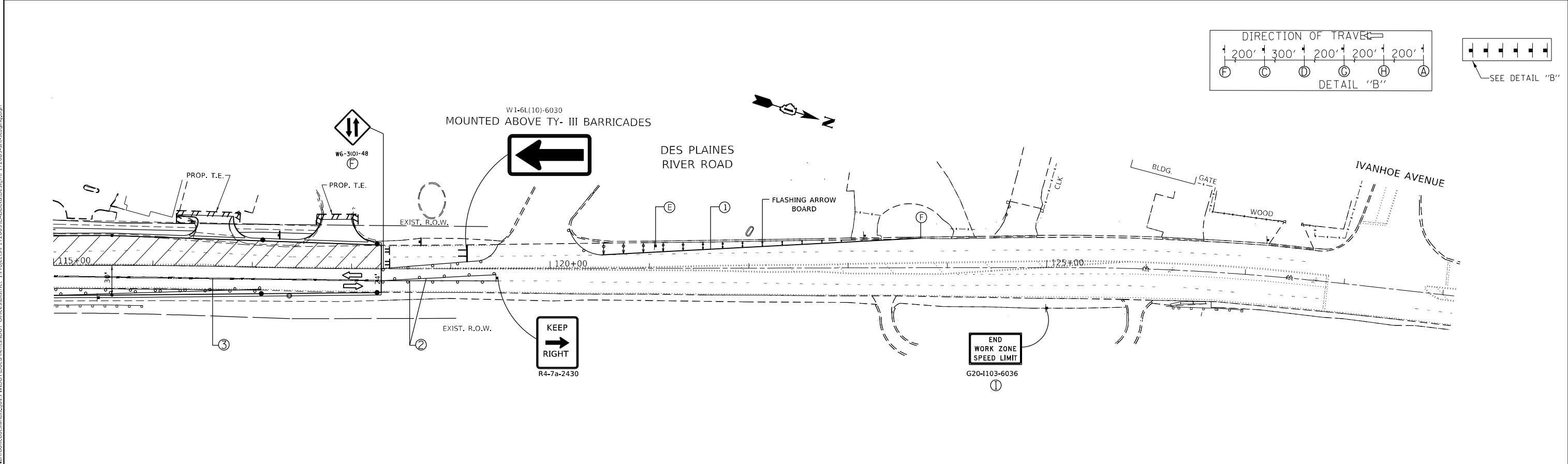
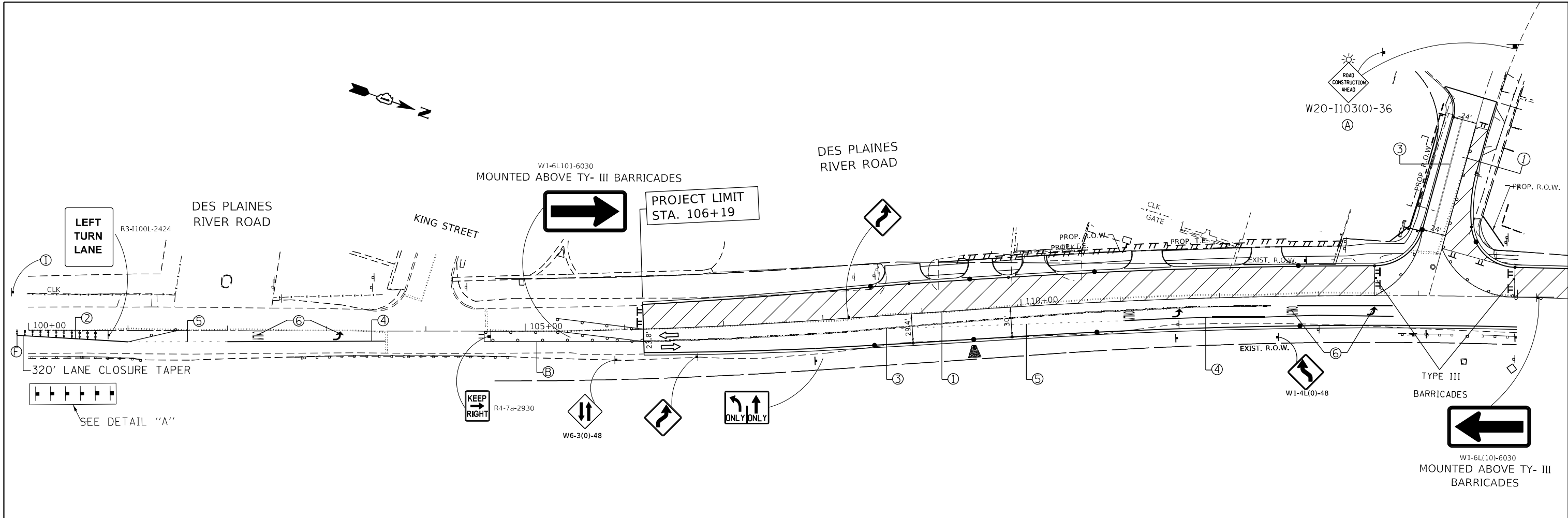
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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

<b>MAINTENANCE OF TRAFFIC PLAN STAGE 2</b> <b>DES PLAINES RIVER ROAD AT ROBINSON ROAD</b>			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	23
CONTRACT NO. 60T17				
ILLINOIS FED. AID PROJECT				



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 DATE: 1/25/2021

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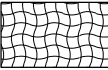

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

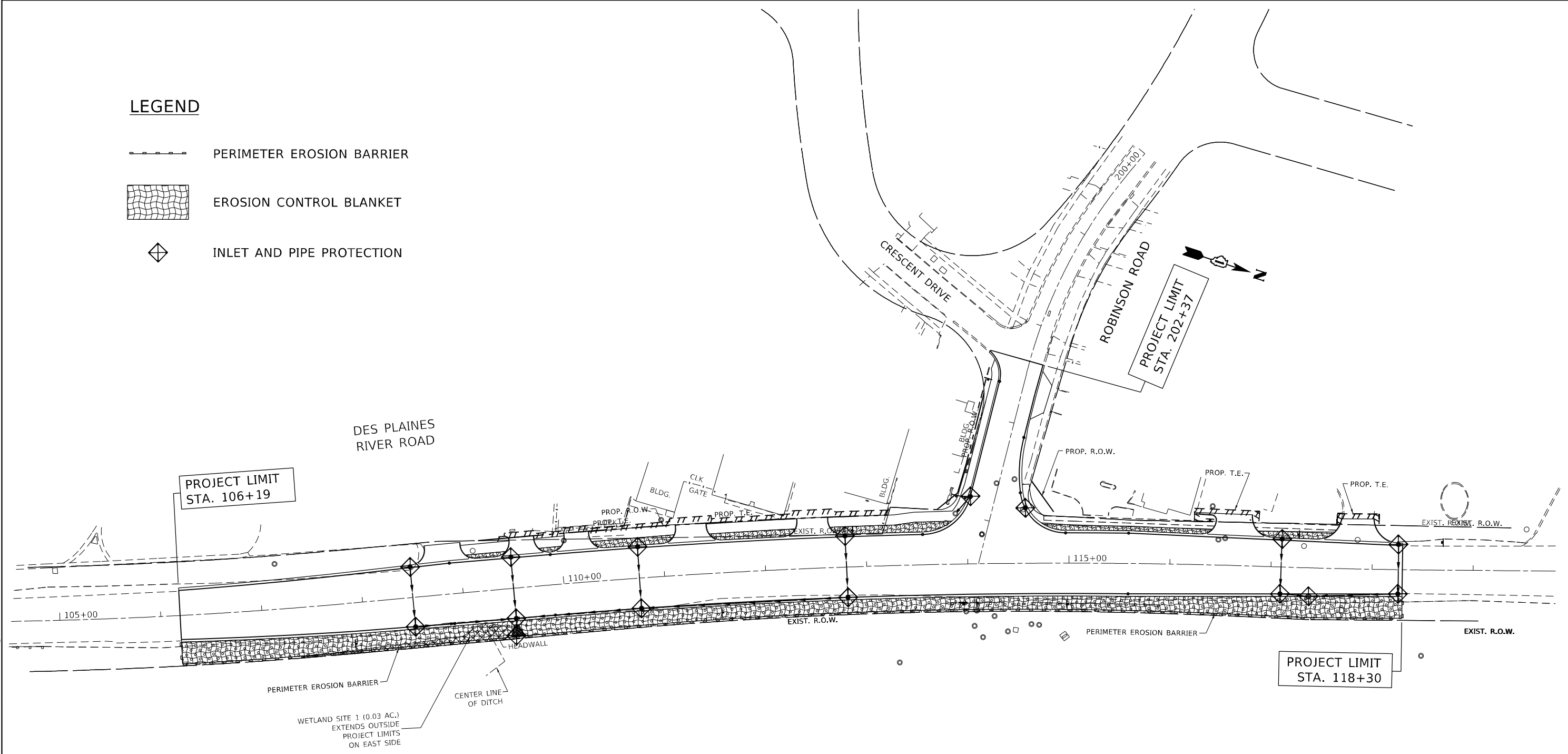
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	24
CONTRACT NO. 60T17				
ILLINOIS FED. AID PROJECT				



**LEGEND**

-  PERIMETER EROSION BARRIER
-  EROSION CONTROL BLANKET
-  INLET AND PIPE PROTECTION



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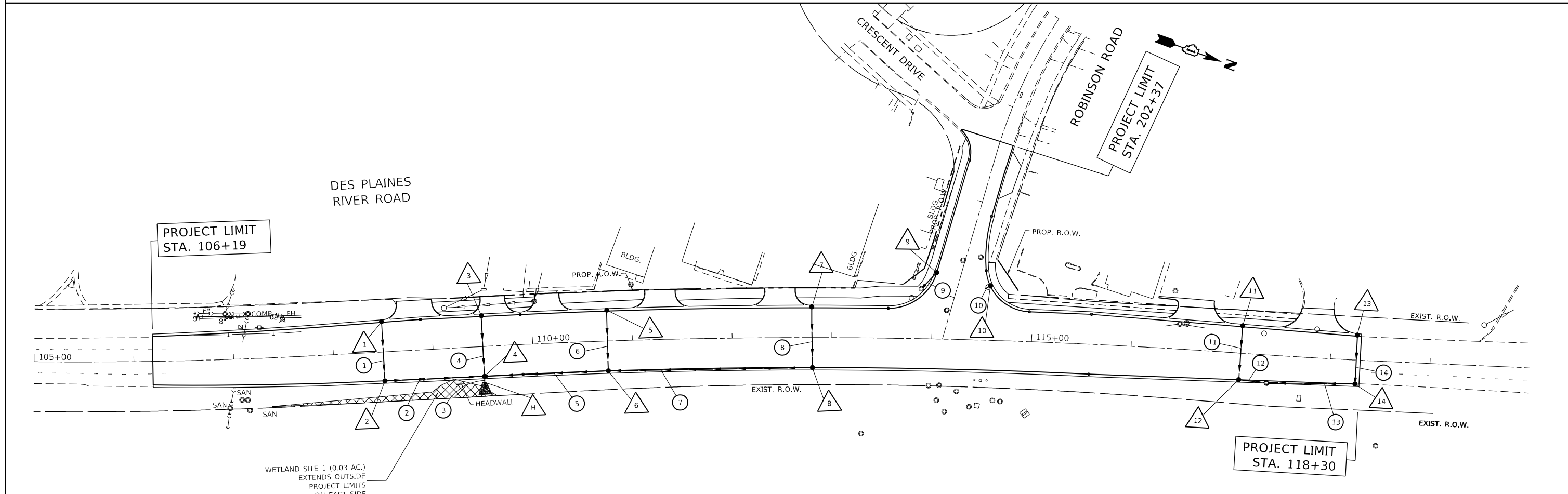
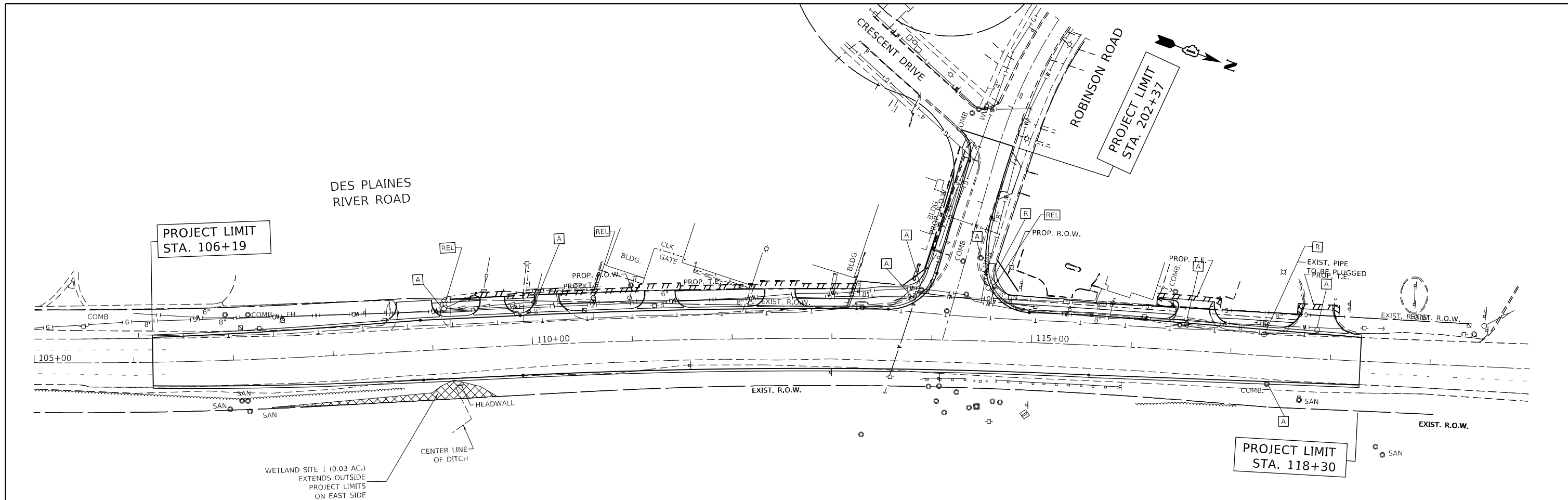
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PLOT DATE = 1/25/2021	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EROSION CONTROL PLAN DES PLAINES RIVER RD.  
AT ROBINSON RD.**

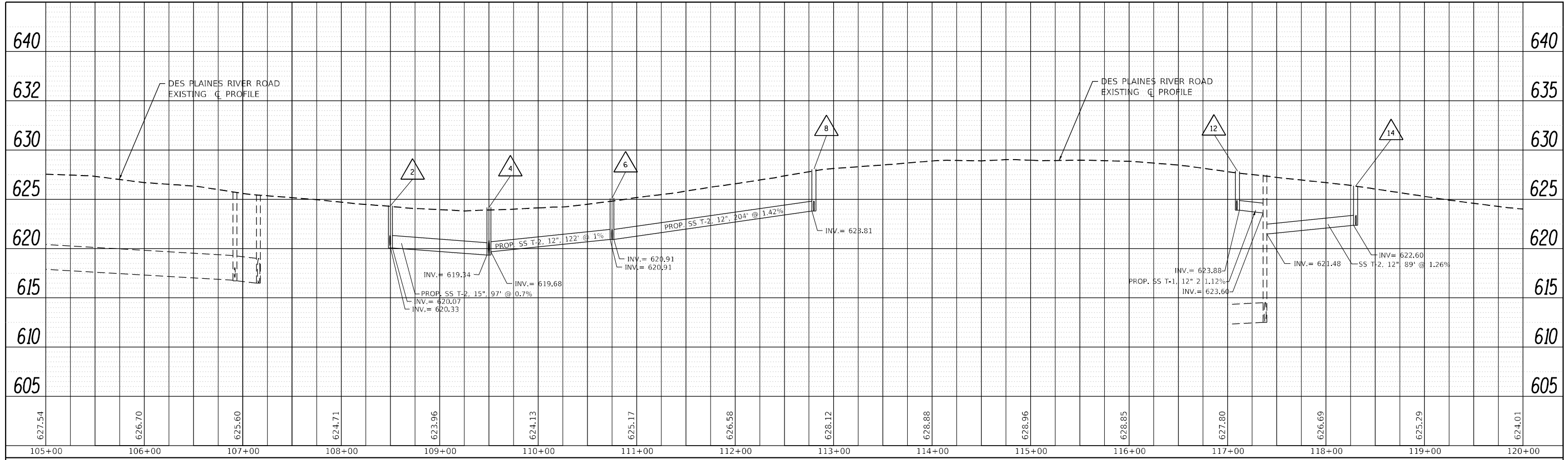
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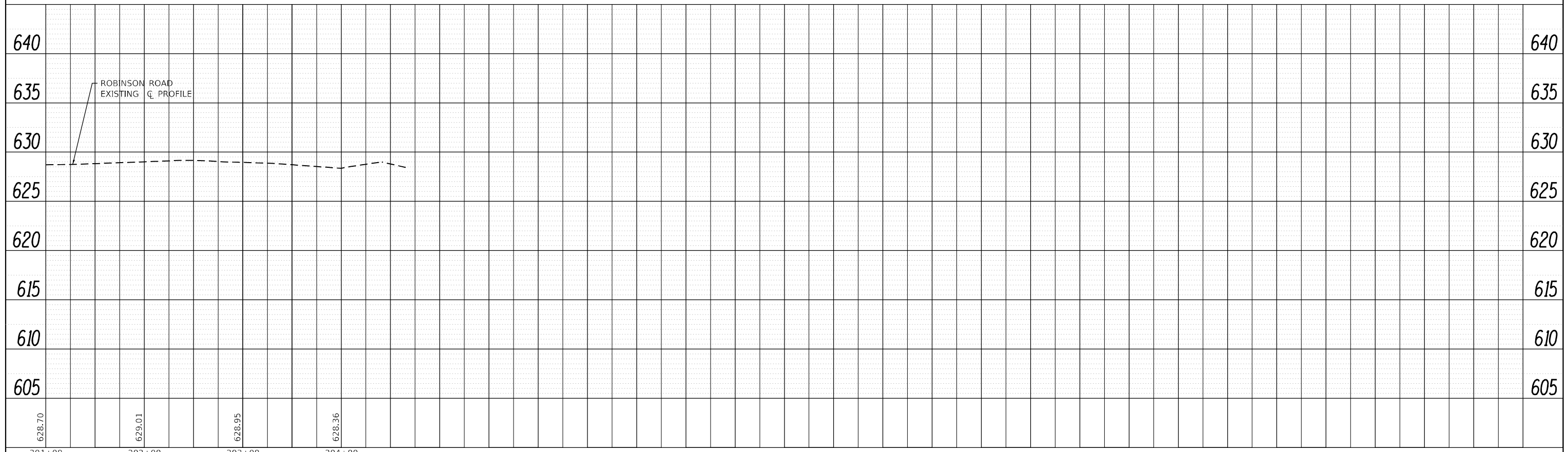


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Default	PLOT DATE = 1/25/2021	CHECKED -	REVISED -			CONTRACT NO. 60T17				
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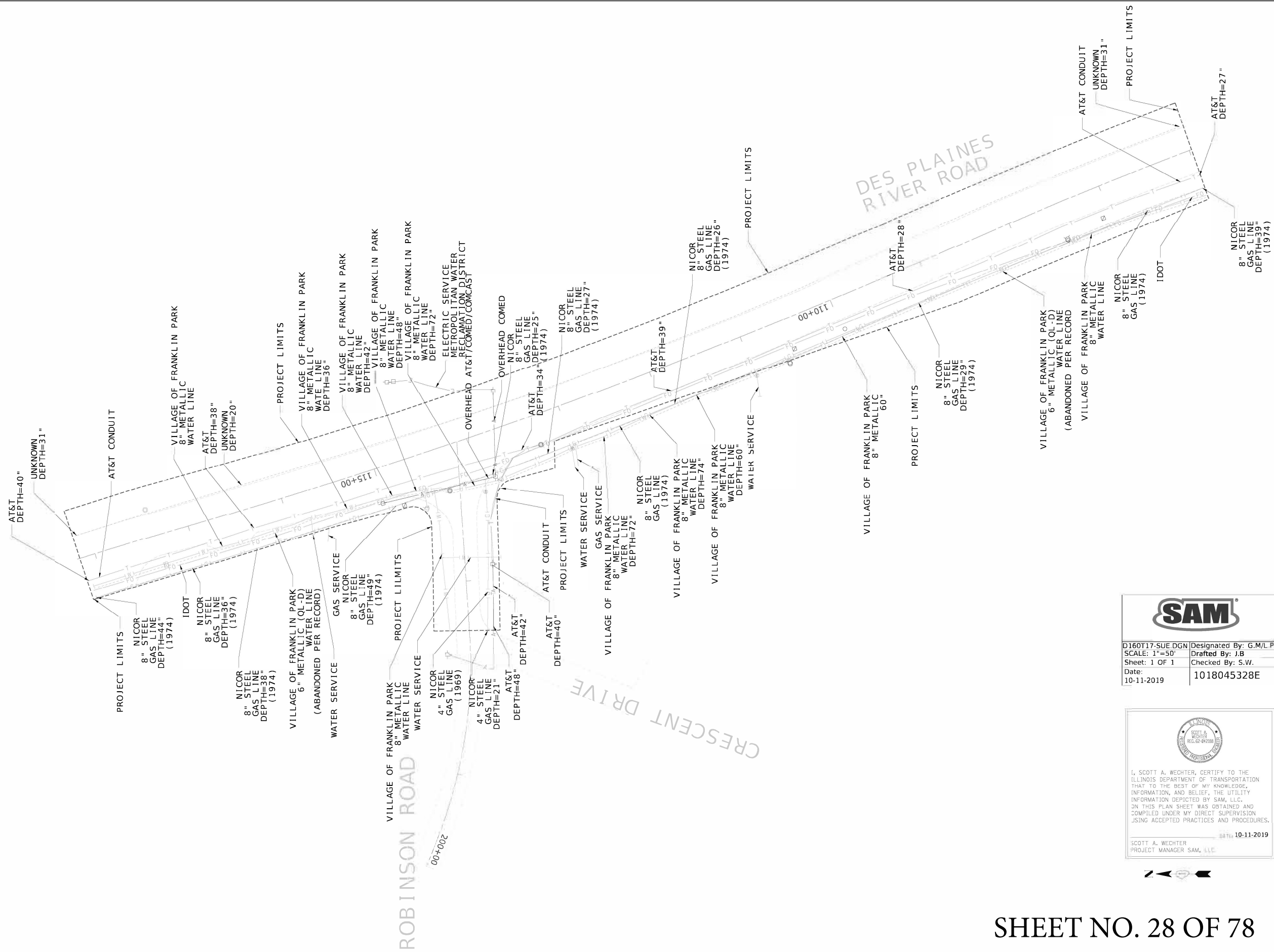


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	PLOT DATE = 8/20/2020	DATE -	REVISED -	VERTICAL SCALE: 1" = 5'				ILLINOIS FED. AID PROJECT				

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FILE NAME: 60117



**UTILITY OWNERS**

COMED  
ATTN: LISA ARGAST  
COMED PUBLIC RELOCATION  
LINCOLN CENTER ONE  
630-437-3381  
LISA.ARGAST@COMED.COM

COMCAST (RECORD NOT RECEIVED)  
ATTN: ROBERT SCHULTER  
688 INDUSTRIAL DRIVE  
ELMHURST, IL 60126  
224-229-5861  
BOB\_SCHULTER@CABLE.COMCAST.COM

ILLINOIS DEPARTMENT OF TRANSPORTATION  
ATTN: SUDUD MAHMOUD  
REGION ONE  
UTILITIES COORDINATOR  
BUREAU OF DESIGN  
847-705-4258  
SUDUD.MAHMOUD@ILLINOIS.GOV

METROPOLITAN WATER RECLAMATION DISTRICT  
ATTN: JOE SCHUESSLER  
100 EAST ERIE STREET  
CHICAGO, IL 60611  
312-751-3236  
SCHUESSLER@MWRD.ORG

AT&T LEGAL MANDATE ENGINEERING  
ATTN: URMU PICONE  
1000 COMMERCE DRIVE  
OAK BROOK, IL 60523  
UB2591@ATT.COM

VILLAGE OF FRANKLIN PARK  
ATTN: PETE CAJIGAS  
9300 BELMONT AVENUE  
FRANKLIN PARK, IL 60131  
847-671-8269

NICOR GAS  
ATTN: BRUCE KOPPANG  
1844 FERRY ROAD  
NAPERVILLE, IL 60563  
630-388-3046  
bkoppang@southernco.com



**SAM**

D16017-SUE.DGN Designated By: G.M/L.P  
SCALE: 1"=50' Drafted By: J.B  
Sheet: 1 OF 1 Checked By: S.W.  
Date: 10-11-2019 1018045328E

SCOTT A. WECHTER  
PROJECT MANAGER SAM, LLC

DATE: 10-11-2019

**LEGEND**

ALL UTILITY INFORMATION HEREON IS DEPICTED TO QUALITY LEVEL "B" (QL-B) UNLESS OTHERWISE NOTED. QL-B INFORMATION IS OBTAINED THROUGH THE APPLICATION OF APPROPRIATE SURFACE GEOPHYSICAL METHODS TO IDENTIFY THE EXISTENCE AND APPROXIMATE HORIZONTAL POSITION OF SUBSURFACE UTILITIES. QL-B DATA ARE PRODUCED BY SURFACE GEOPHYSICS AT ANY POINT OF THEIR DEPICTION, THIS INFORMATION IS SURVEYED TO APPLICABLE TOLERANCES AND REDUCED ONTO PLAN DOCUMENTS.

SIZE INFORMATION SHOWN HEREON IS TAKEN FROM AVAILABLE UTILITY RECORDS.

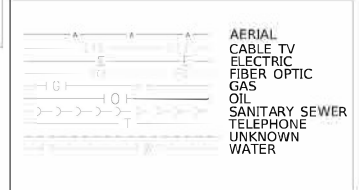
ABBREVIATIONS:

- QL-C DEPICTED ACCORDING TO RECORD INFORMATION AND EXISTING ASSOCIATED UTILITY STRUCTURES. NO ELECTRONIC INFORMATION WAS OBTAINED.
- QL-D DEPICTED ACCORDING TO RECORD INFORMATION. NO ELECTRONIC INFORMATION WAS OBTAINED.
- (GATED) DEPICTED ACCORDING TO FIELD INSPECTION
- (FO) FIBER OPTIC
- (AATUR) ABANDONED ACCORDING TO UTILITY RECORDS
- (AATFD) ABANDONED ACCORDING TO FIELD INSPECTION
- (EATFD) EMPTY ACCORDING TO FIELD INSPECTION
- EOI END OF ELECTRONIC DESIGNATING INFORMATION
- EORI END OF UTILITY RECORD INFORMATION
- (NAC) NO ASSOCIATED CABLE FOUND FROM UTILITY STRUCTURE
- (NAP) NO ASSOCIATED PIPING FOUND FROM UTILITY STRUCTURE
- UTILITY ENDPOINT

UNLESS OTHERWISE NOTED, UTILITY LINE LIMITS DEPICTED REPRESENT FIELD DESIGNATING LIMITS AND NOT ENDPOINTS OF UTILITIES.

UTILITY INFORMATION LABELED "QL-C" OR "QL-D" IS DERIVED FROM FURNISHED RECORDS, SUCH INFORMATION MAY NOT BE ACCURATE OR RELIABLE. SAM, LLC, EXPRESSLY DISCLAIMS RESPONSIBILITY FOR THE ACCURACY OR RELIABILITY OF UTILITY INFORMATION DEPICTED ACCORDING TO RECORDS.

ELECTRONIC DEPTH READINGS WERE TAKEN DIRECTLY FROM ELECTRONIC DESIGNATING INSTRUMENTS AND HAVE NOT BEEN VERIFIED BY ANY OTHER MEANS. EQUIPMENT MANUFACTURERS WILL NOT GUARANTEE AND ACCURACY FOR THIS DATA. THEREFORE, THE DEPTH READINGS ARE NOT TO BE CONSIDERED SUITABLE FOR DESIGN DECISIONS. SAM, LLC, DOES NOT WARRANT OR GUARANTEE THE ACCURACY OF RELIABILITY OF ANY ELECTRONIC DEPTH READINGS.



**SHEET NO. 28 OF 78**

USER NAME = UNTITLED	DESIGNED - S.W.	REVISED -
PLOT SCALE = 1"=50'	DRAWN - J.B.	REVISED -
PLOT DATE = 10-11-2019	CHECKED - S.W.	REVISED -
	DATE - 10-11-2019	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

SCALE: 1"=50' SHEET 1 OF 1 SHEETS STA. 119+00 TO STA. 106+00

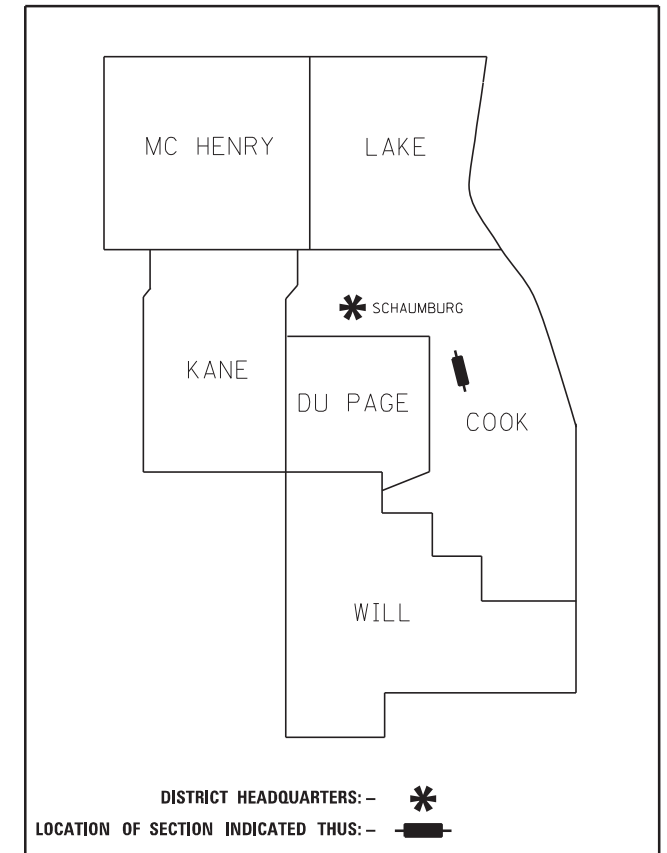
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		COOK	1	1
CONTRACT NO. 60117				
ILLINOIS FED. AID PROJECT				

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS**

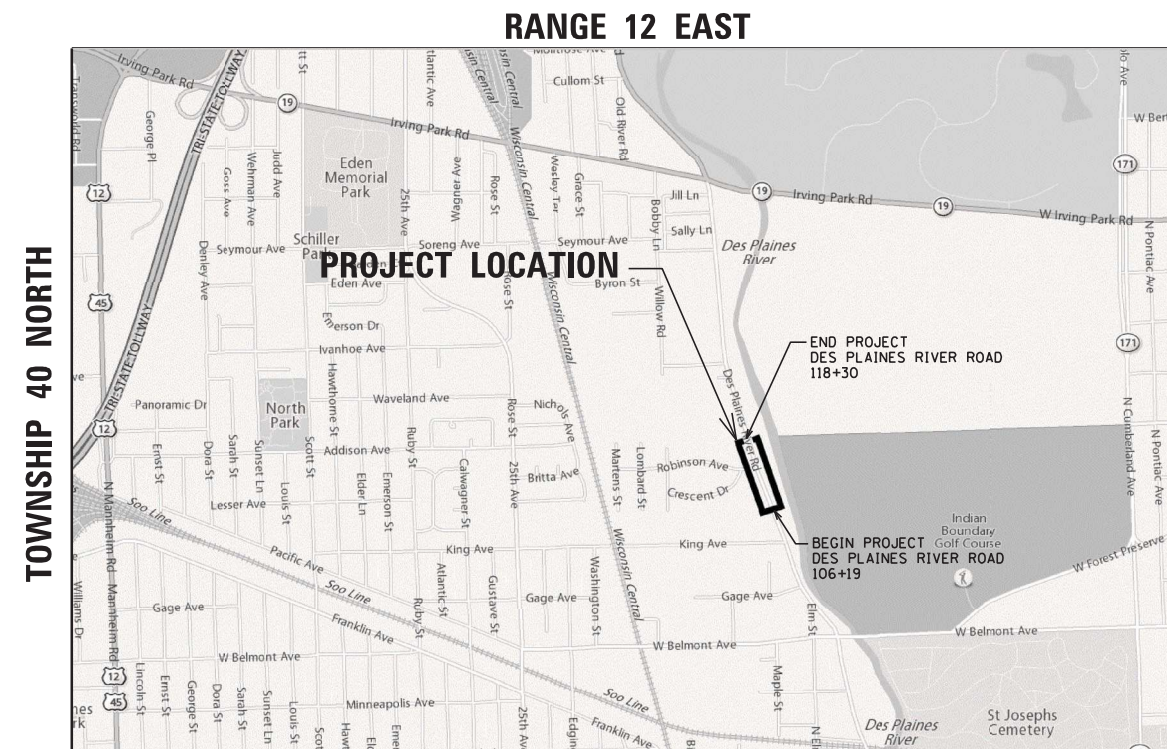
# PLAT OF HIGHWAYS

**ROUTE: DES PLAINES RIVER ROAD  
SECTION:  
COUNTY: COOK  
LIMITS: AT ROBINSON ROAD  
JOB NO.: R-90-007-12**

PARCEL NUMBER	OWNER	SHEET NUMBER	PROPERTY ACQUIRED BY
OJR0001 OJR0001TE	ROBERT J. LEONARD AS TRUSTEE OF THE ROBERT LEONARD TRUST U/A/D APRIL 22, 2014	2	
OJR0002 OJR0002TE	3450 CORPORATION, AN ILLINOIS CORPORATION	2	
OJR0003 OJR0003TE	DANIEL A. BAPTIST	2	
OJR0005 OJR0005TE	LCF & CGF, LLC, AN ILLINOIS LIMITED LIABILITY COMPANY	2	
OJR0006 OJR0006TE	U.N. CLUB, INC.	2	
OJR0008 OJR0008TE	TASTE OF THE MIDDLE EAST, INC., AN ILLINOIS CORPORATION	3	
OJR0009TE	DEAN FOOD COMPANY, AN ILLINOIS CORPORATION	3, 4	



**PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS**



**LOCATION MAP**

GROSS LENGTH = 980 FT. = 0.186 MILE  
NET LENGTH = 980 FT. = 0.186 MILE

**IDOT USE ONLY**

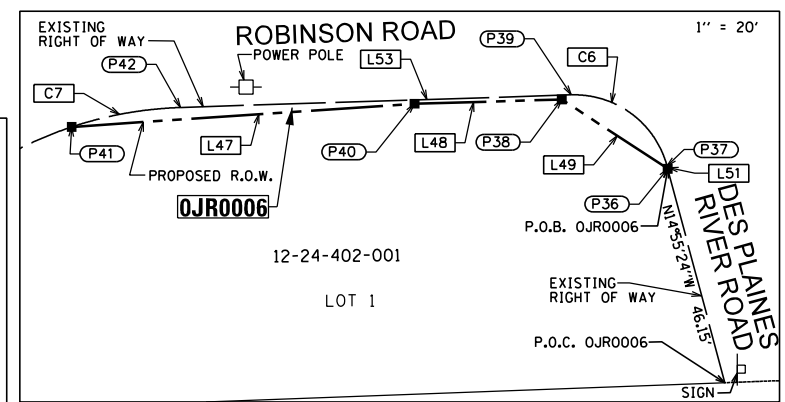
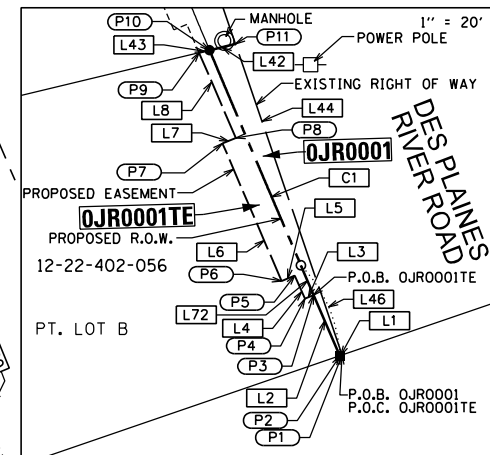
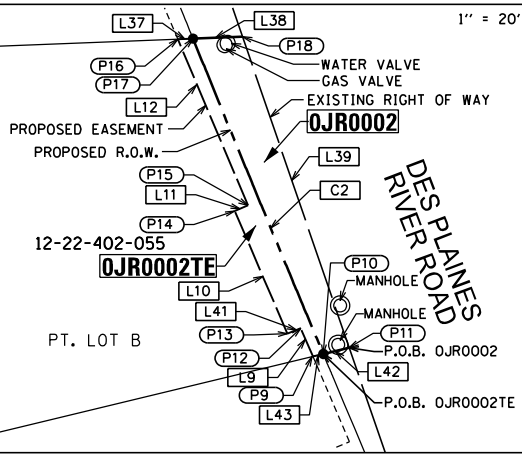
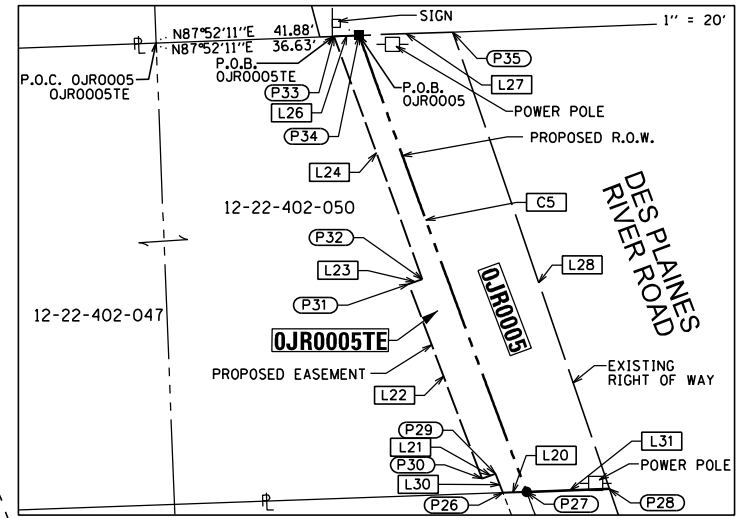
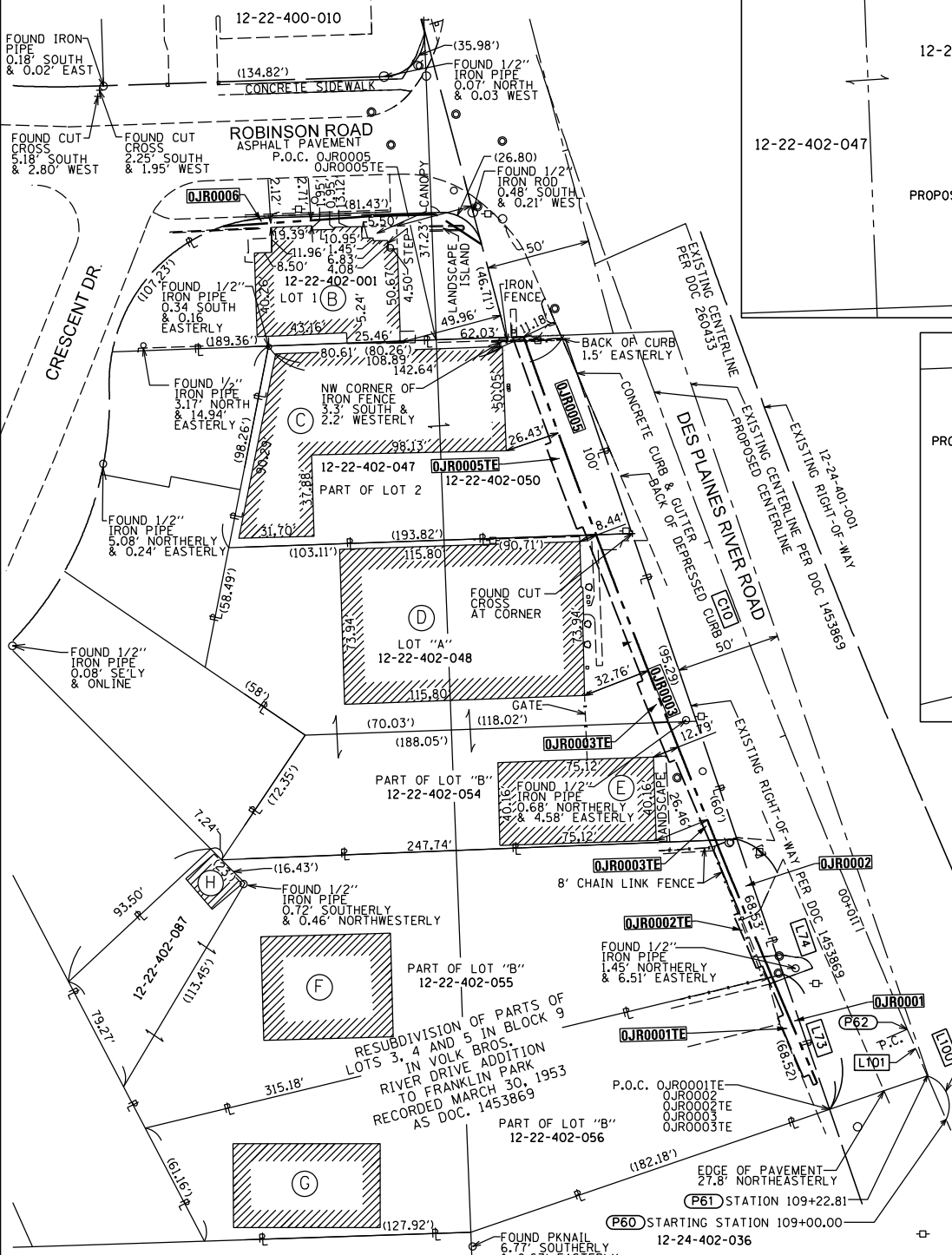
RECEIVED  
OCT 19 2018  
PLATS & LEGALS

PART OF THE SE 1/4 AND FRACTIONAL SECTION 22 AND PART OF THE SOUTH SECTION OF ROBINSONS RESERVE TWP. 40 N., R. 12 E. OF THE 3RD. P.M., IN COOK COUNTY, ILLINOIS.

**PROPOSED CENTERLINE LINE TABLE**

L100	22.81'	N23° 14'26"W
L101	24.29'	N23° 14'26"W

CONTINUED ON SHEET 3



**SANCHEZ**  
8654 W. CATALPA AVE STE 912  
CHICAGO, IL 60619  
PHONE 773-494144  
FAX 773-494144

**BUILDING TYPE TABLE**

- (B) 2-STORY FRAME COMMERCIAL BUILDING
- (C) 1-STORY BRICK & CONCRETE INDUSTRIAL BUILDING
- (D) 1-STORY BRICK INDUSTRIAL BUILDING
- (E) 1-STORY BRICK INDUSTRIAL BUILDING
- (F) 1-STORY BRICK INDUSTRIAL BUILDING
- (G) 1-STORY STEEL SERVICE BUILDING
- (H) 1-STORY FRAME GARAGE

**CURVE TABLE**

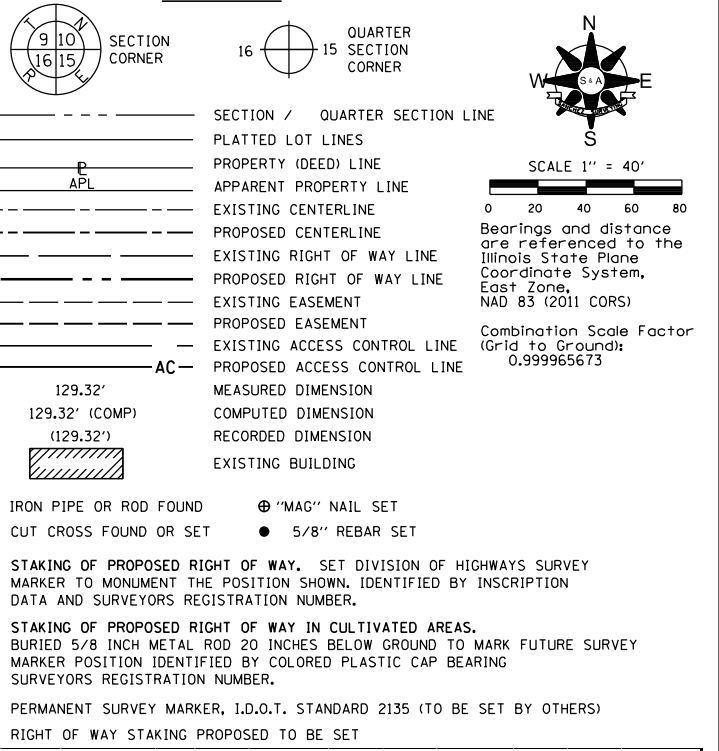
C1 R=6050.00' L=48.69' CB=N23°00'36"W CH=48.69'	C2 R=6050.00' L=71.10' CB=N22°26'34"W CH=71.10	C3 R=6050.00' L=61.05' CB=N21°49'01"W CH=61.05'	C4 R=6050.00' L=96.46' CB=N21°04'16"W CH=96.46'	C5 R=6050.00' L=101.32' CB=N20°08'05"W CH=101.32'	C6 R=20.00' L=26.85' CB=S53°23'22"E CH=24.88'	C7 R=80.00' L=23.11' CB=N19°52'11"E CH=23.03'
---	--	---	---	---	---	---

**PROPOSED CENTERLINE CURVE TABLE**

Δ = 07° 12'07"	Δ = 01° 30'02"
R = 6000.00'	R = 6000.00'
Δ = 0° 57'18"	Δ = 0° 57'18"
L = 754.17'	L = 157.13'
L = 377.58'	L = 78.57'
P.T.C. = 109+47.10	P.T.C. = 117+01.27
P.R.C. = 117+01.27	P.T. = 118+58.40

REVISION DATE: 10/17/14 REVISION MADE BY: WG REVISION DATE: 8/5/14 REVISION MADE BY: WG REVISION DATE: 3/10/14 REVISION MADE BY: WG  
REVISION DATE: 10/19/18 REVISION MADE BY: GC REVISION DATE: 4/30/14 REVISION MADE BY: WG REVISION DATE: 2/21/14 REVISION MADE BY: WG

**LEGEND**



**LINE TABLE**

LINE	DISTANCE	BEARING	LINE	DISTANCE	BEARING	LINE	DISTANCE	BEARING	LINE	DISTANCE	BEARING
L1	0.54'	S71°04'37"W	L14	3.00'	S68°18'02"W	L27	19.50'	N87°52'11"E	L44	68.52'	S18°56'11"E
L2	19.79'	N23°14'26"W	L15	20.56'	N22°07'44"W	L28	100.69'	S18°56'11"E	L45	20.54'	N23°14'26"W
L3	2.00'	S66°45'24"W	L16	7.29'	N87°59'01"E	L29	4.27'	N21°22'05"W	L46	71.60'	S88°07'24"W
L4	5.29'	N23°14'26"W	L17	20.63'	N22°07'44"W	L30	17.30'	N87°59'01"E	L47	30.69'	S88°20'51"W
L5	3.00'	S66°45'24"W	L18	3.00'	N68°41'25"E	L31	95.29'	S18°56'11"E	L48	26.38'	N56°50'43"W
L6	31.21'	N23°14'26"W	L19	75.05'	N21°22'05"W	L32	13.54'	S87°59'01"W	L49	46.15'	N14°55'24"W
L7	3.00'	N67°02'28"E	L20	4.93'	N87°59'01"E	L33	60.00'	S18°56'11"E	L50	0.57'	S14°55'24"E
L8	19.33'	N23°14'26"W	L21	3.00'	S59°26'27"W	L34	3.16'	N87°44'35"E	L51	31.45'	N88°08'40"E
L9	6.41'	N23°14'26"W	L22	43.31'	N20°35'49"W	L35	10.32'	N87°44'35"E	L52	6.75'	S23°14'26"E
L10	27.86'	N23°14'26"W	L23	1.00'	N69°51'02"E	L36	3.00'	N69°51'02"E	L53	68.52'	S18°56'11"W
L11	3.00'	N67°32'53"E	L24	53.85'	N19°59'08"W	L37	4.93'	N19°59'08"W	L54	137.05'	N18°56'11"W
L12	37.60'	N22°52'29"W	L25	42.34'	N87°52'11"E	L38	5.56'	N76°27'45"E			
L13	41.91'	N22°52'29"W	L26	5.24'	N87°52'11"E	L39	2.23'	N76°27'45"E			

STATE OF ILLINOIS )  
COUNTY OF COOK )SS

THIS IS TO CERTIFY THAT I, GERARDO P. SANCHEZ, AN ILLINOIS PROFESSIONAL LAND SURVEYOR LICENSE NO. 035-003486, HAVE SURVEYED THE PARCEL PLAT SHOWN HEREON IN PART OF THE NORTHEAST QUARTER OF SECTION 16, TWP. 40 NORTH, RANGE 12 EAST, 3RD. P.M. IN COOK COUNTY, ILLINOIS., THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, AND MADE FOR THE ILLINOIS STATE TOLL HIGHWAY AUTHORITY.

DATED AT CHICAGO, ILLINOIS THIS 19TH DAY OF OCTOBER 2018 A.D.

*Gerardo P. Sanchez*  
GERARDO P. SANCHEZ, PRESIDENT

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-003486  
LICENSE EXPIRATION DATE: NOVEMBER 30, 2020

ILLINOIS PROFESSIONAL DESIGN FIRM REGISTRATION NO.: 184-004601

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

ALL DIMENSIONS ARE MEASURED UNLESS OTHERWISE SPECIFIED

BEARINGS AND DISTANCES SHOWN HEREON REFERENCE THE ILLINOIS STATE PLANE COORDINATE SYSTEM, EAST ZONE, NORTH AMERICAN DATUM OF 1983, ADJUSTMENT - 2011 (CORS).  
ALL MEASURED AND CALCULATED VALUES ARE "GRID" NOT "GROUND".

COMBINED SCALE FACTOR  
(GRID SCALE FACTOR X ELEV. FACTOR) = 0.999965673

FIELD WORK COMPLETE 08/30/2018

**PLAT OF HIGHWAYS**  
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
DES PLAINES RIVER ROAD

LIMITS: AT ROBINSON ROAD COUNTY: COOK  
SECTION: STATION 109+26.53 JOB NO.: R-90-007-12  
SCALE: 1"=40' TO STATION 118+06.30 SHEET 2 OF 4

**DOT USE ONLY**

**BUREAU OF LAND ACQUISITION**  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196

PART OF THE SE 1/4 AND FRACTIONAL SECTION 22 AND PART OF THE SOUTH SECTION OF ROBINSONS RESERVE  
TWP. 40 N., R. 12 E. OF THE 3RD. P.M., IN COOK COUNTY, ILLINOIS.

### LEGEND

SECTION / QUARTER SECTION LINE  
 PLATTED LOT LINES  
 PROPERTY (DEED) LINE  
 APPARENT PROPERTY LINE  
 EXISTING CENTERLINE  
 PROPOSED CENTERLINE  
 EXISTING RIGHT OF WAY LINE  
 PROPOSED RIGHT OF WAY LINE  
 EXISTING EASEMENT  
 PROPOSED EASEMENT  
 EXISTING ACCESS CONTROL LINE  
 PROPOSED ACCESS CONTROL LINE  
 MEASURED DIMENSION  
 COMPUTED DIMENSION  
 RECORDED DIMENSION  
 EXISTING BUILDING

SECTION CORNER  
 QUARTER SECTION CORNER

SCALE 1" = 40'

Bearings and distance are referenced to the Illinois State Plane Coordinate System, East Zone, NAD 83 (2011 CORS)  
 Combination Scale Factor (Grid to Ground): 0.999965673

IRON PIPE OR ROD FOUND  
 CUT CROSS FOUND OR SET

STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

PERMANENT SURVEY MARKER, I.D.O.T. STANDARD 2135 (TO BE SET BY OTHERS)  
 RIGHT OF WAY STAKING PROPOSED TO BE SET

STATE OF ILLINOIS )  
 ) SS  
 COUNTY OF COOK )

THIS IS TO CERTIFY THAT I, GERARDO P. SANCHEZ, AN ILLINOIS PROFESSIONAL LAND SURVEYOR LICENSE NO. 035-003486, HAVE SURVEYED THE PARCEL PLAT SHOWN HEREON IN PART OF THE NORTHEAST QUARTER OF SECTION 16, TWP. 40 NORTH, RANGE 12 EAST, 3RD P.M. IN COOK COUNTY, ILLINOIS. THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE ILLINOIS STATE TOLL HIGHWAY AUTHORITY.  
 DATED AT CHICAGO, ILLINOIS THIS 19TH DAY OF OCTOBER 2018 A.D.

*Gerardo P. Sanchez*  
 GERARDO P. SANCHEZ, PRESIDENT

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-003486  
 LICENSE EXPIRATION DATE: NOVEMBER 30, 2020

ILLINOIS PROFESSIONAL DESIGN FIRM REGISTRATION NO.: 184-004601

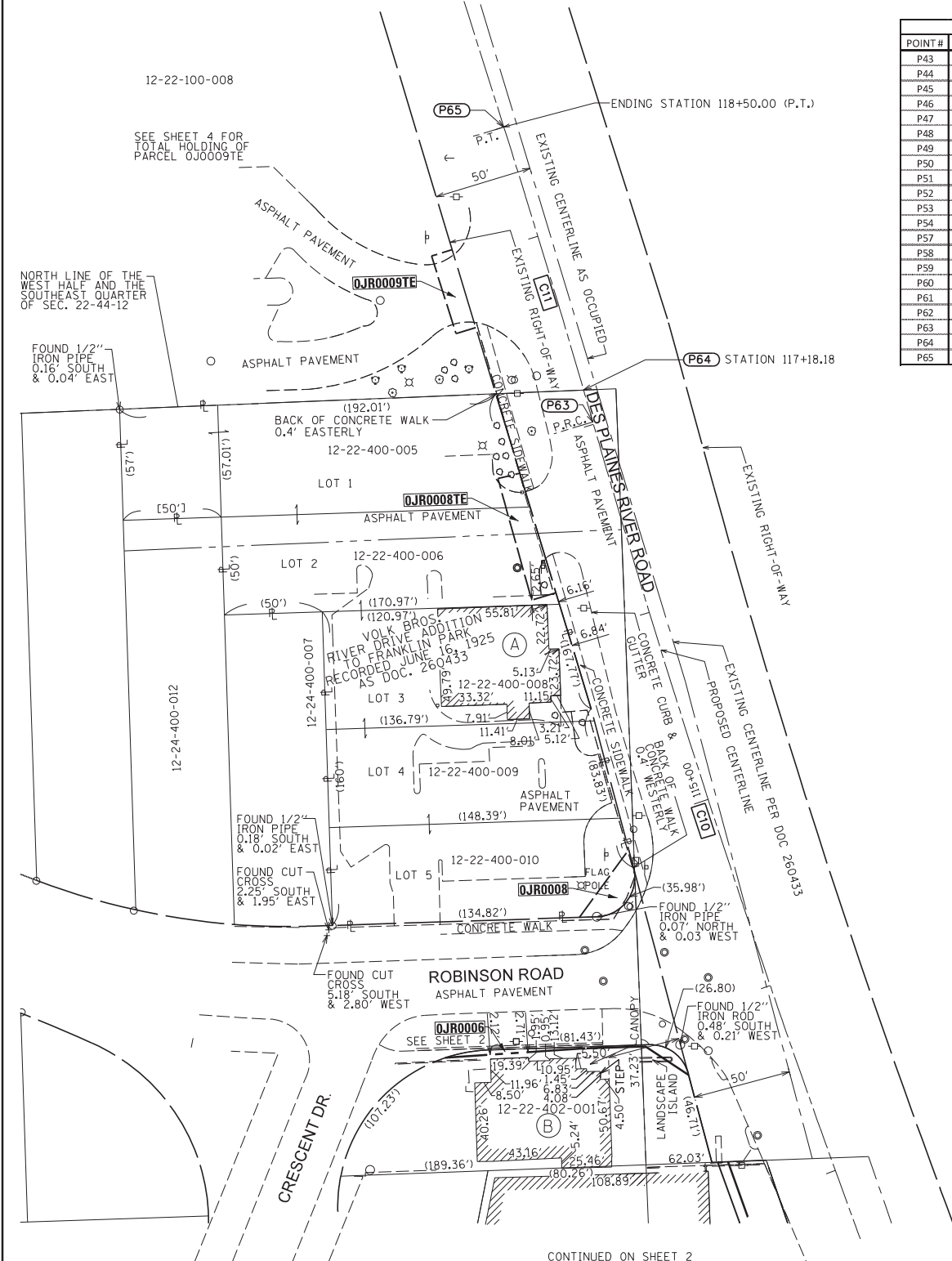
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

ALL DIMENSIONS ARE MEASURED UNLESS OTHERWISE SPECIFIED

BEARINGS AND DISTANCES SHOWN HEREON REFERENCE THE ILLINOIS STATE PLANE COORDINATE SYSTEM, EAST ZONE, NORTH AMERICAN DATUM OF 1983, ADJUSTMENT - 2011 (CORS).  
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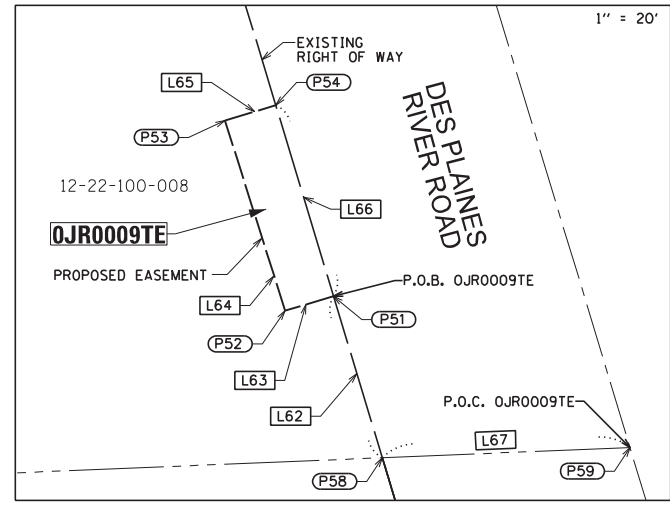
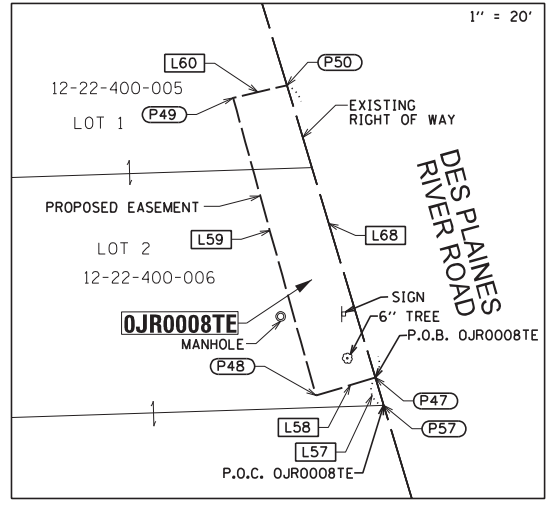
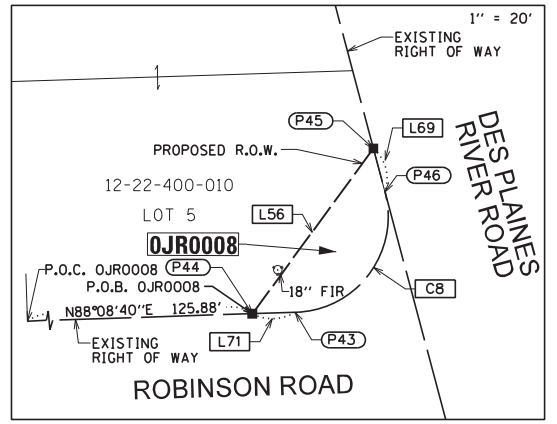
COMBINED SCALE FACTOR  
 (GRID SCALE FACTOR X ELEV. FACTOR) = 0.999965673

FIELD WORK COMPLETE 08/30/2018



POINT#	NORTHING	EASTING	STATION	OFFSET
P43	1922300.700	1114504.940	114+61.01	72.65' LT
P44	1922300.410	1114495.990	114+63.52	81.24' LT
P45	1922334.880	1114521.210	114+88.13	46.52' LT
P46	1922325.840	1114523.620	114+78.86	47.04' LT
P47	1922465.970	1114484.140	116+23.30	42.62' LT
P48	1922462.240	1114471.760	116+23.30	55.55' LT
P49	1922524.270	1114454.500	116+87.10	54.51' LT
P50	1922526.900	1114465.710	116+86.51	43.02' LT
P51	1922601.120	1114443.250	117+64.40	43.75' LT
P52	1922598.110	1114433.160	117+64.40	54.27' LT
P53	1922637.700	1114420.650	118+06.30	54.78' LT
P54	1922640.930	1114431.200	118+06.30	43.74' LT
P57	1922460.169	1114485.900	116+17.41	42.62' LT
P58	1922567.460	1114453.430	117+30.93	48.99' LT
P59	1922569.590	1114505.030	117+16.45	6.61' RT
P60	1921799.961	1114774.822	START	109+00.00
P61	1921820.92	1114765.821	POL	109+22.81
P62	1921843.24	1114756.236	PC	109+47.10
P63	1922553.07	1114502.922	PRC	117+01.27
P64	1922569.31	1114498.229	POC	117+18.18
P65	1922703.495	1114457.537	PT/END	118+58.40

LINE	DISTANCE	BEARING
L56	42.71'	N36°11'13"E
L57	6.06'	N16°50'04"W
L58	12.93'	S73°13'00"W
L59	64.39'	N15°33'05"W
L60	11.51'	N76°48'37"E
L62	35.17'	N16°50'04"W
L63	10.53'	S73°21'30"W
L64	41.52'	N17°32'21"W
L65	11.04'	N72°57'30"E
L66	41.59'	S16°50'04"E
L67	51.64'	S87°38'07"W
L68	63.66'	S16°50'04"E
L69	9.36'	S14°55'24"E
L71	8.95'	S88°08'40"W
L72	6.75'	S23°14'26"E
L100	22.81'	N23°14'26"W
L101	24.29'	N23°14'26"W



CURVE TABLE	PROPOSED CENTERLINE CURVE TABLE
CB R=20.00' L=35.98' CB=S36°36'38"W CH=31.32'	CURVE 10 Δ = 07° 12' 07" R = 6000.00' D = 0° 57' 18" L = 754.17' T = 377.58' E = 11.87' P.C. = 109+47.10 P.R.C. = 117+01.27
	CURVE 11 Δ = 01° 30' 02" R = 6000.00' D = 0° 57' 18" L = 157.13' T = 78.57' E = 0.52' P.C. = 117+01.27 P.T. = 118+58.40

PARCEL NUMBER	TOTAL HOLDINGS ACRES	PART TAKEN		AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA		PARCEL INDEX NUMBER
		ACRES	SQ. FT.			ACRES	SQ. FT.	
OJR0008	0.967	0.010	430	N/A	0.957			12-22-400-008, 12-22-400-009, 12-22-400-010, 12-22-400-011
OJR0008TE						0.018		12-22-400-005, 12-22-400-006
OJR0009TE	10.951			0.632		0.010		12-22-100-008, 12-22-100-143

BUILDING TYPE TABLE

(A) 1-STORY FRAME RESTAURANT  
 (B) 2-STORY FRAME COMMERCIAL BUILDING

SHEET NO. 31 OF 78

REVISION DATE: 10/19/18  
 REVISION DATE: 4/30/14  
 REVISION DATE: 3/10/14  
 REVISION DATE: 2/21/14

REVISION MADE BY: GC  
 REVISION MADE BY: WG  
 REVISION MADE BY: WG  
 REVISION MADE BY: WG

RECEIVED  
 OCT 19 2018  
 PLATS & LEGALS

### PLAT OF HIGHWAYS

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 DES PLAINES RIVER ROAD

LIMITS: AT ROBINSON ROAD COUNTY: COOK  
 SECTION: TO STATION 118+06.30  
 STATION: 109+26.53  
 SCALE: 1"=40'

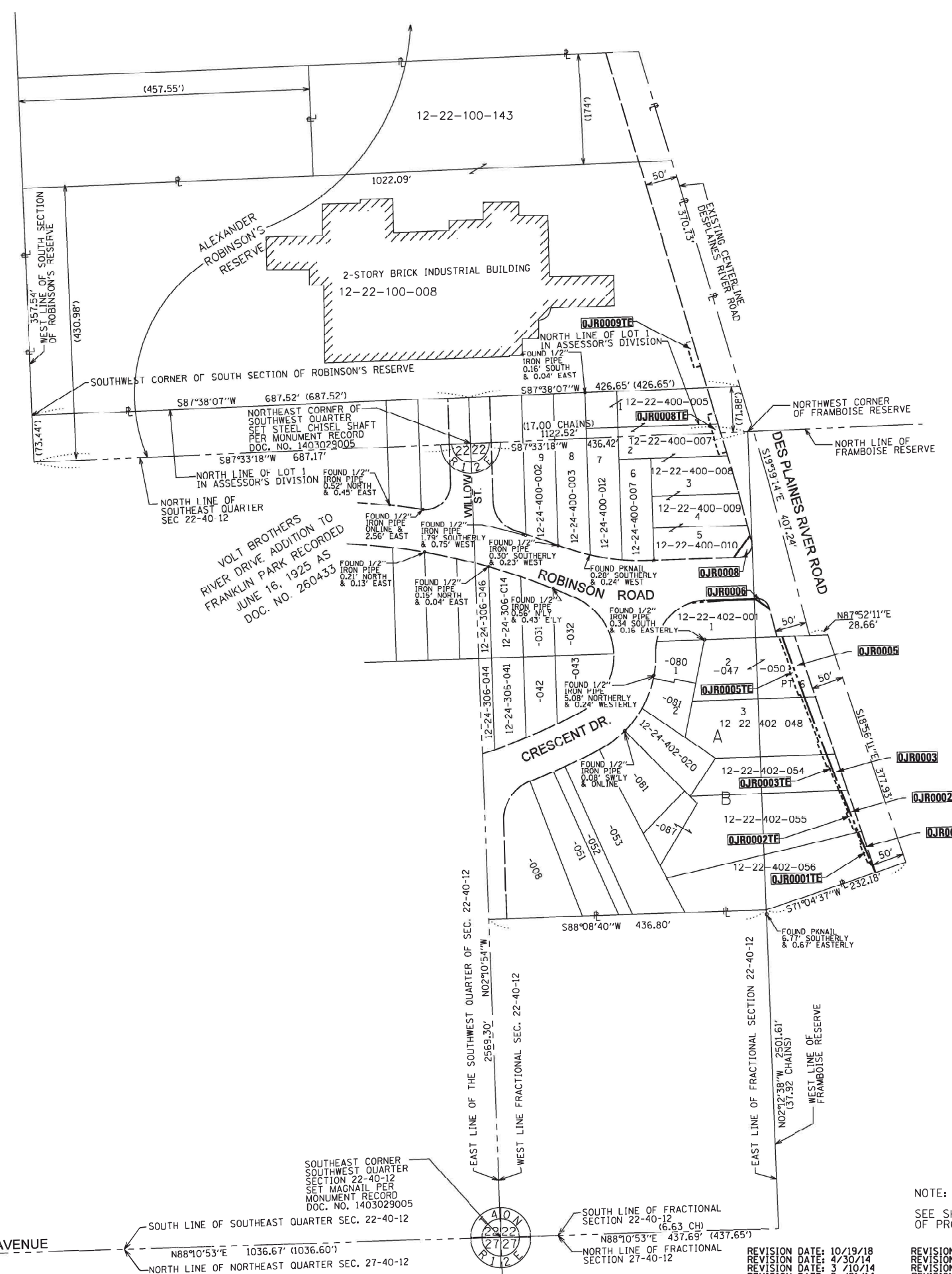
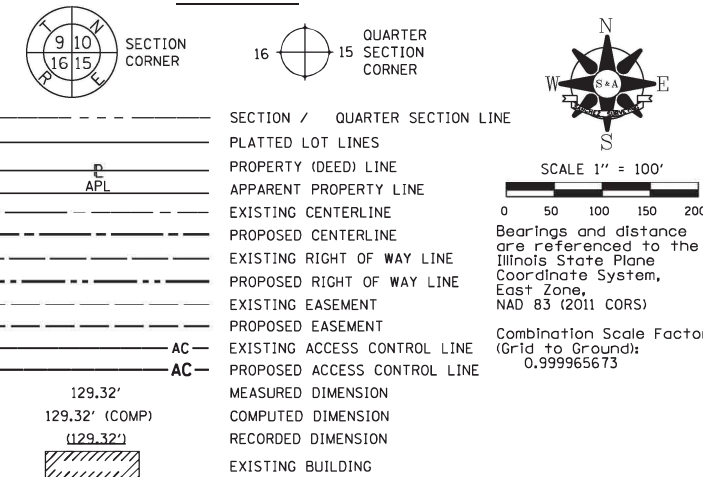
JOB NO.: R-90-007-12  
 TO STATION 118+06.30  
 SHEET 3 OF 4

BUREAU OF LAND ACQUISITION  
 201 WEST CENTER COURT  
 SCHAMBURG, ILLINOIS 60196

8604 W. CATALPA AVE STE 912  
 CHICAGO, IL 60656  
 PHONE 773-444-0144  
 FAX 847-232-3104

PART OF THE SE 1/4 AND FRACTIONAL SECTION 22 AND PART OF THE SOUTH SECTION OF ROBINSONS RESERVE TWP. 40 N., R. 12 E. OF THE 3RD. P.M., IN COOK COUNTY, ILLINOIS.

LEGEND



- IRON PIPE OR ROD FOUND
- ⊕ "MAG" NAIL SET
- + CUT CROSS FOUND OR SET
- 5/8" REBAR SET
- STAKING OF PROPOSED RIGHT OF WAY, SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS, BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ⊙ PERMANENT SURVEY MARKER, I.D.O.T. STANDARD 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET

STATE OF ILLINOIS )  
 ) JSS  
 COUNTY OF COOK )

THIS IS TO CERTIFY THAT I, GERARDO P. SANCHEZ, AN ILLINOIS PROFESSIONAL LAND SURVEYOR LICENSE NO. 035-003486, HAVE SURVEYED THE PARCEL PLAT SHOWN HEREON IN PART OF THE NORTHEAST QUARTER OF SECTION 16, TWP. 40 NORTH, RANGE 12 EAST, 3RD P.M. IN COOK COUNTY, ILLINOIS., THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE ILLINOIS STATE TOLL HIGHWAY AUTHORITY.  
 DATED AT CHICAGO, ILLINOIS THIS 19TH DAY OF OCTOBER 2018 A.D.

*Gerardo P. Sanchez*  
 GERARDO P. SANCHEZ, PRESIDENT

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-003486  
 LICENSE EXPIRATION DATE: NOVEMBER 30, 2018

ILLINOIS PROFESSIONAL DESIGN FIRM REGISTRATION NO.: 184-004601

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

ALL DIMENSIONS ARE MEASURED UNLESS OTHERWISE SPECIFIED

BEARINGS AND DISTANCES SHOWN HEREON REFERENCE THE ILLINOIS STATE PLANE COORDINATE SYSTEM, EAST ZONE, NORTH AMERICAN DATUM OF 1983, ADJUSTMENT - 2011 (CORS). ALL MEASURED AND CALCULATED VALUES ARE "GRID" NOT "GROUND".

COMBINED SCALE FACTOR (GRID SCALE FACTOR X ELEV. FACTOR) = 0.999965673

FIELD WORK COMPLETE 08/30/2018

SHEET NO. 32 OF 78

RECEIVED  
 OCT 19 2018  
 PLATS & LEGALS

**PLAT OF HIGHWAYS**  
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**DES PLAINES RIVER ROAD**

LIMITS: AT ROBINSON ROAD COUNTY: COOK  
 SECTION: JOB NO.: R-90-007-12  
 STATION 109+26.53 TO STATION 118+06.30  
 SCALE: 1"=100' SHEET 4 OF 4

**BUREAU OF LAND ACQUISITION**  
**201 WEST CENTER COURT**  
**SCHAUMBURG, ILLINOIS 60196**

NOTE:  
 SEE SHEETS 2-3 FOR DIMENSIONS OF PROPOSED PARCELS.

REVISION DATE: 10/19/18  
 REVISION DATE: 4/30/14  
 REVISION DATE: 3/10/14  
 REVISION DATE: 2/21/14

REVISION MADE BY: GC  
 REVISION MADE BY: WG  
 REVISION MADE BY: WG  
 REVISION MADE BY: WG

MINNEAPOLIS ST. PAUL ROAD & SAULT STE MARIE RIVER

FOUND 1/2" IRON ROD 50.01' N & ONLINE

SOUTHWEST CORNER SOUTHWEST QUARTER SECTION 22-40-12 SET MAGNAIL PER MONUMENT RECORD DOC. NO. 1403029005

SOUTH LINE OF SOUTHWEST QUARTER SEC. 22-40-12  
 N88°10'53"E 1036.67' (1036.60')

NORTH LINE OF NORTHEAST QUARTER SEC. 27-40-12

EAST LINE OF THE SOUTHWEST QUARTER OF SEC. 22-40-12  
 2569.30' N02°10'54"W

WEST LINE FRACTIONAL SEC. 22-40-12

EAST LINE OF FRACTIONAL SECTION 22-40-12  
 N02°12'38"W 2501.61' (37.92 CHAINS)

WEST LINE OF FRACTIONAL SECTION 22-40-12

SOUTH LINE OF FRACTIONAL SECTION 22-40-12  
 (6.63 CH) N88°10'53"E 437.69' (437.65')

NORTH LINE OF FRACTIONAL SECTION 27-40-12

FOUND 1/2" IRON PIPE ONLINE & 2.56' EAST  
 FOUND 1/2" IRON PIPE 1.79' SOUTHERLY & 0.75' WEST  
 FOUND 1/2" IRON PIPE 0.30' SOUTHERLY & 0.23' WEST  
 FOUND 1/2" IRON PIPE 0.21' NORTH & 0.19' EAST  
 FOUND 1/2" IRON PIPE 0.15' NORTH & 0.04' EAST  
 FOUND 1/2" IRON PIPE 5.08' NORTHERLY & 0.24' WESTERLY  
 FOUND 1/2" IRON PIPE 0.08' SWLY & ONLINE  
 FOUND 1/2" IRON PIPE 0.12' SOUTHERLY & 0.6' EASTERLY

VOL 7 BROTHERS RIVER DRIVE ADDITION TO FRANKLIN PARK RECORDED JUNE 16, 1925 AS DOC. NO. 260433

ALEXANDER'S ROBINSONS RESERVE

2-STORY BRICK INDUSTRIAL BUILDING 12-22-100-008

ROBINSON ROAD

CRESCENT DR.

DES PLAINES RIVER ROAD

BELMONT AVENUE

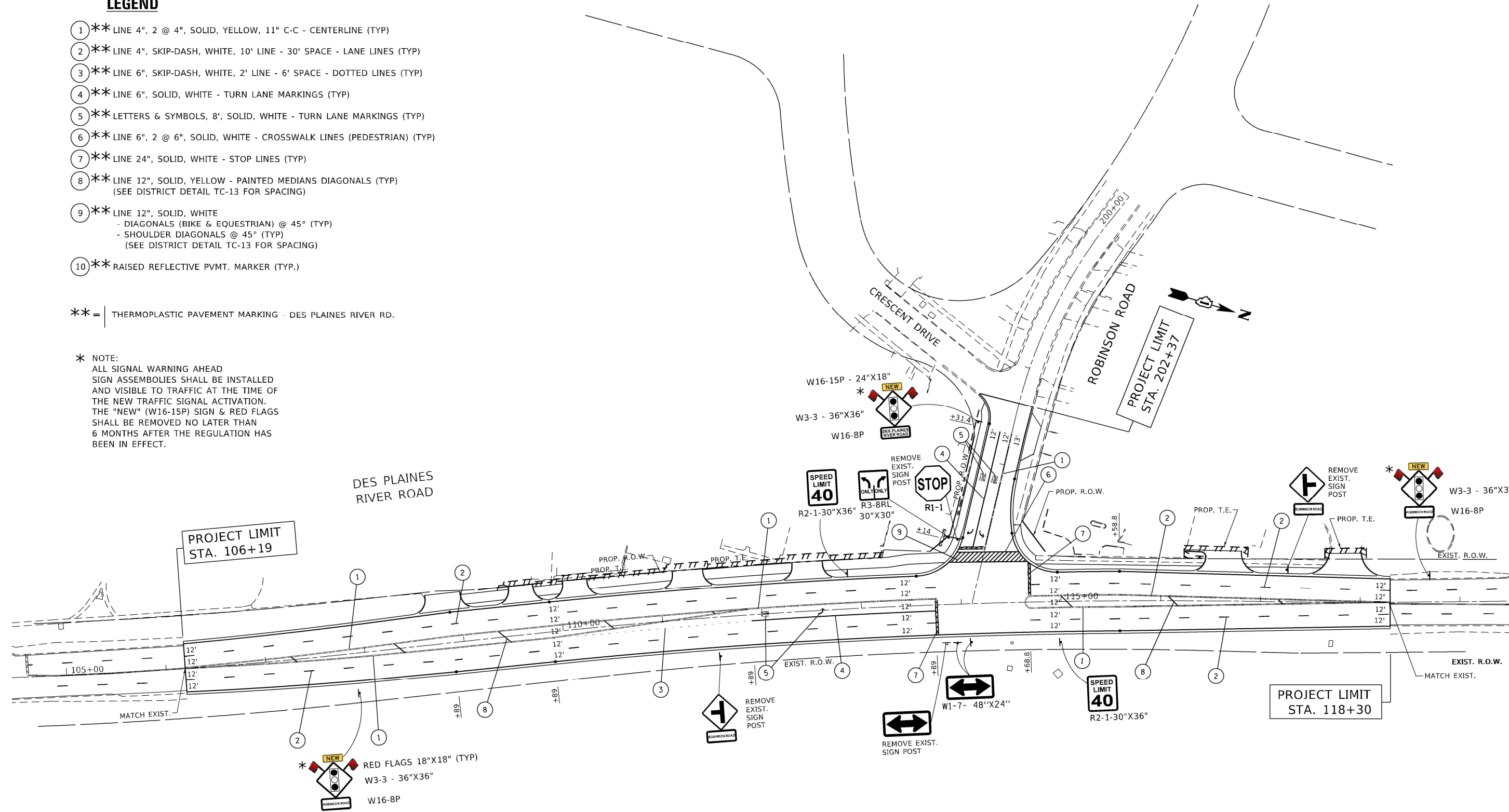


**LEGEND**

- ① \*\* LINE 4", 2 @ 4", SOLID, YELLOW, 11" C-C - CENTERLINE (TYP)
- ② \*\* LINE 4", SKIP-DASH, WHITE, 10' LINE - 30' SPACE - LANE LINES (TYP)
- ③ \*\* LINE 6", SKIP-DASH, WHITE, 2' LINE - 6' SPACE - DOTTED LINES (TYP)
- ④ \*\* LINE 6", SOLID, WHITE - TURN LANE MARKINGS (TYP)
- ⑤ \*\* LETTERS & SYMBOLS, 8', SOLID, WHITE - TURN LANE MARKINGS (TYP)
- ⑥ \*\* LINE 6", 2 @ 6", SOLID, WHITE - CROSSWALK LINES (PEDESTRIAN) (TYP)
- ⑦ \*\* LINE 24", SOLID, WHITE - STOP LINES (TYP)
- ⑧ \*\* LINE 12", SOLID, YELLOW - PAINTED MEDIANS DIAGONALS (TYP)  
(SEE DISTRICT DETAIL TC-13 FOR SPACING)
- ⑨ \*\* LINE 12", SOLID, WHITE  
- DIAGONALS (BIKE & EQUESTRIAN) @ 45° (TYP)  
- SHOULDER DIAGONALS @ 45° (TYP)  
(SEE DISTRICT DETAIL TC-13 FOR SPACING)
- ⑩ \*\* RAISED REFLECTIVE PVMT. MARKER (TYP.)

\*\* = THERMOPLASTIC PAVEMENT MARKING - DES PLAINES RIVER RD.

\* NOTE:  
ALL SIGNAL WARNING AHEAD SIGN ASSEMBLIES SHALL BE INSTALLED AND VISIBLE TO TRAFFIC AT THE TIME OF THE NEW TRAFFIC SIGNAL ACTIVATION. THE "NEW" (W16-15P) SIGN & RED FLAGS SHALL BE REMOVED NO LATER THAN 6 MONTHS AFTER THE REGULATION HAS BEEN IN EFFECT.



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PLOT DATE = 1/25/2021	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>PAVEMENT MARKING &amp; SIGNING PLAN</b>			
<b>DES PLAINES RIVER RD. AT ROBINSON RD.</b>			
SCALE: 1" = 50'	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	33
CONTRACT NO. 60T17				
ILLINOIS FED. AID PROJECT				

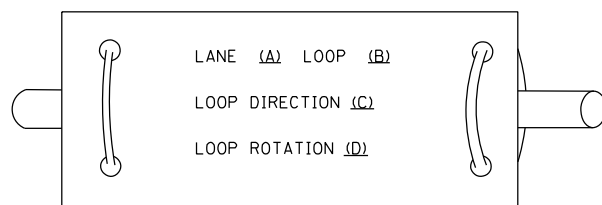




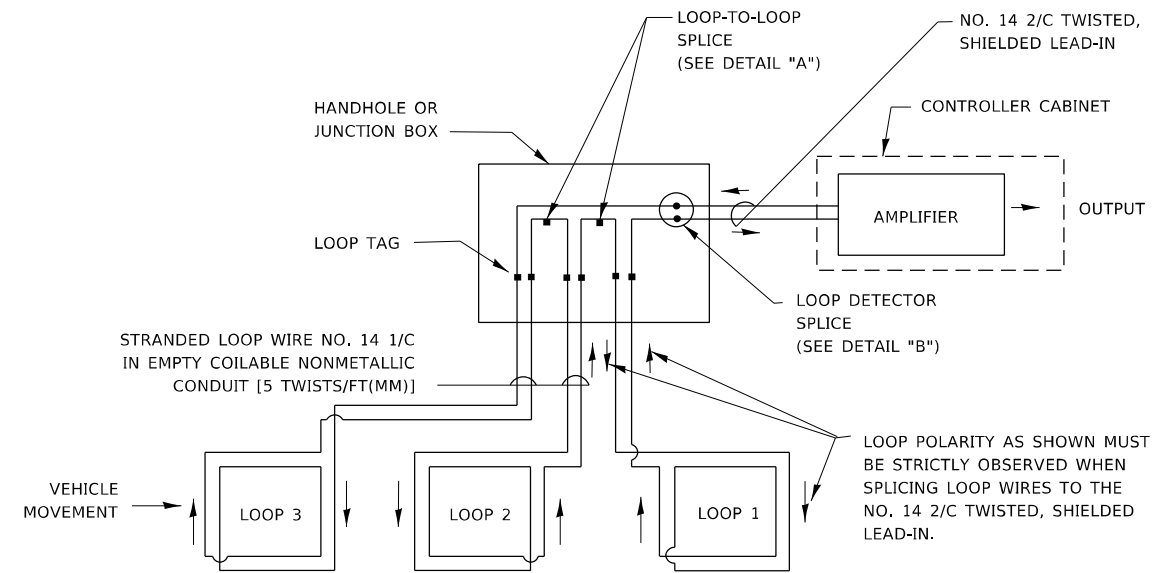
**LOOP DETECTOR NOTES**

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVESHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

**LOOP LEAD-IN CABLE TAG**

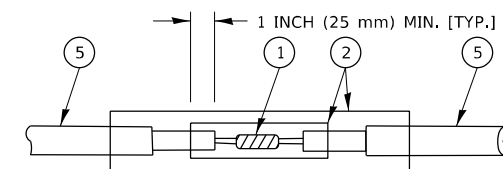


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

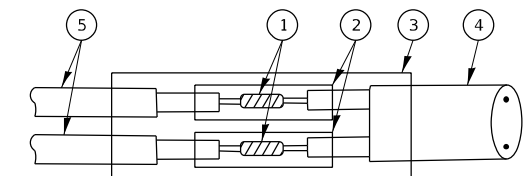


**DETECTOR LOOP WIRING SCHEMATIC**

- LOOPS SHALL BE SPLICED IN SERIES. SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm), IF IN CONCRETE.
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

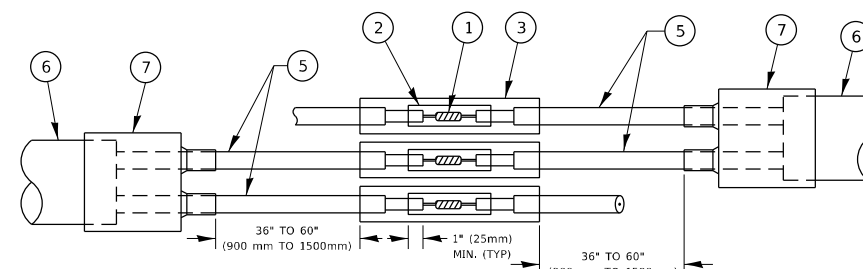


DETAIL "A"  
LOOP-TO-LOOP SPLICE

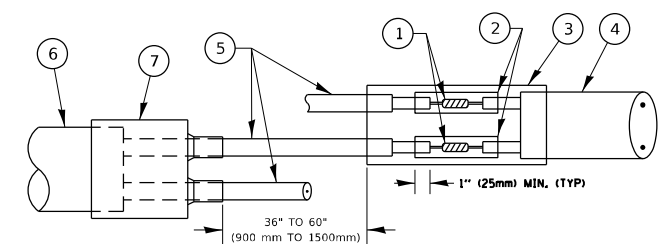


DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

**TYPE I LOOP**



DETAIL "A"  
LOOP-TO-LOOP SPLICE



DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

**PREFORMED LOOP**

**LOOP DETECTOR SPLICE**

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE. PREFORMED LOOP
- 6 XL POLYOLEFIN 2 CONDUCTOR
- 7 BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

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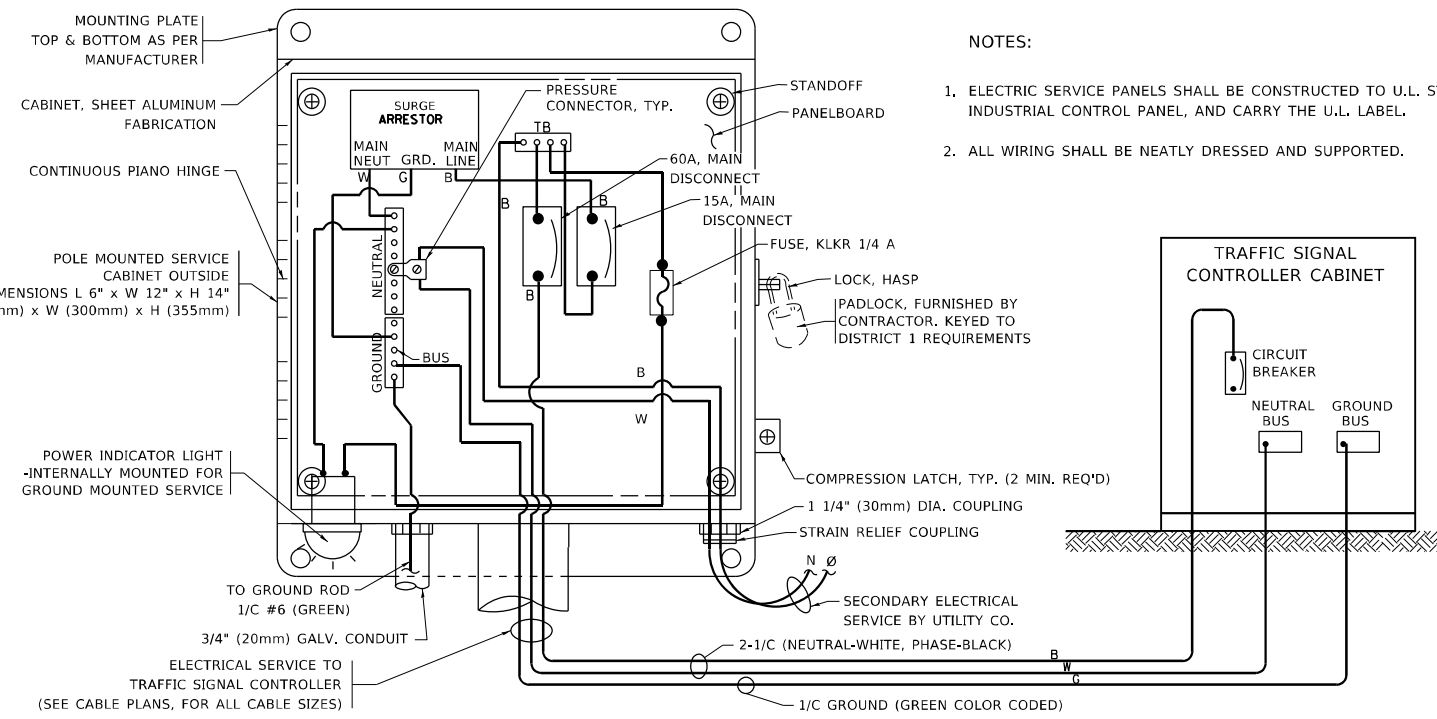
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE  
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

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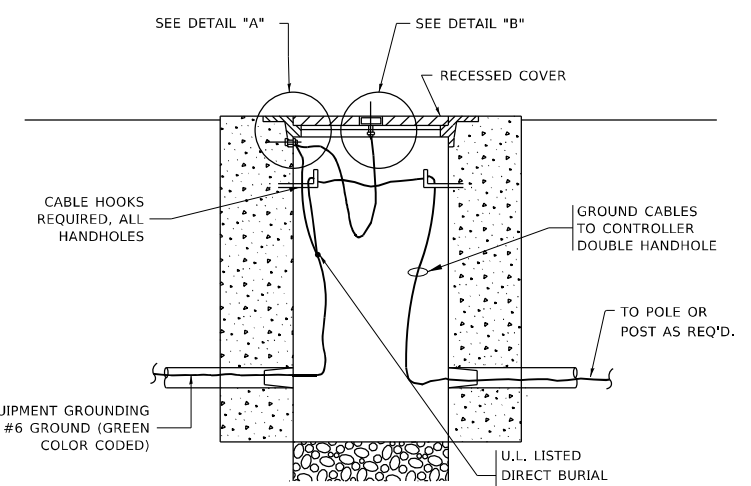
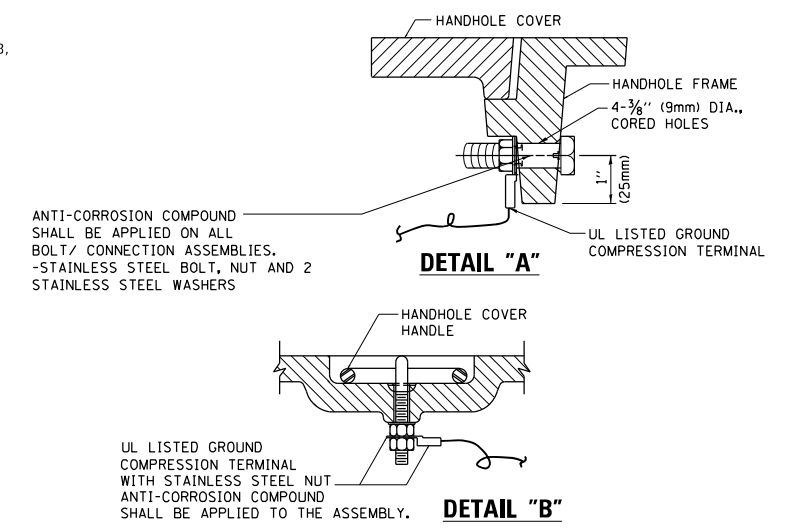
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	36
<b>TS-05</b>		CONTRACT NO. 60T17		
ILLINOIS FED. AID PROJECT				





**ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)**  
 SERVICE INSTALLATION POLE MOUNT (SHOWN)  
 (NOT TO SCALE)

- NOTES:
1. ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508, INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL.
  2. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.



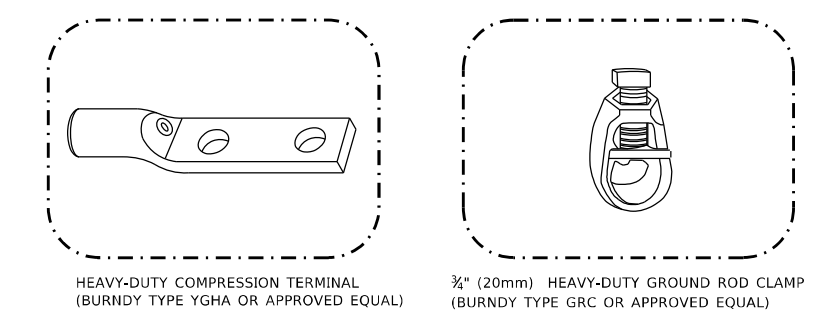
**HANDHOLE COVER & FRAME - GROUNDING DETAIL**  
 (NOT TO SCALE)

- (2) 1/2" x 1 1/4" STAINLESS STEEL BOLT WITH SPLIT LOCK WASHER AND NYLON INSERT LOCKOUT WELDED TO FRAME AND TO COVER. (TYPICAL). ANTI-CORROSION COMPOUND SHALL BE APPLIED TO EACH ASSEMBLY.

**EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL**  
 (NOT TO SCALE)

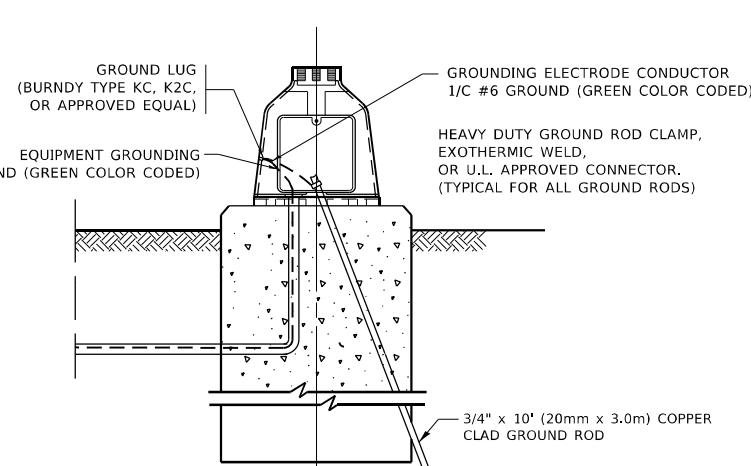
**NOTES:**  
**GROUNDING SYSTEM**

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.

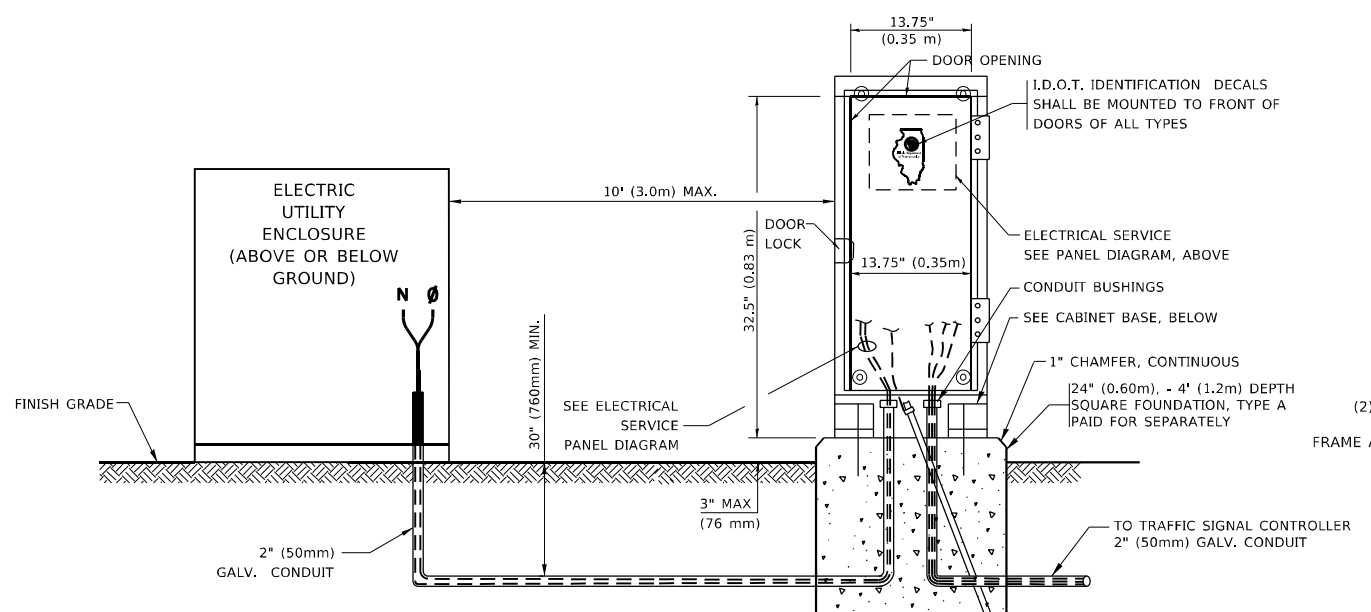


**NOTES:**

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES
- 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES
- 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES.
- 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.

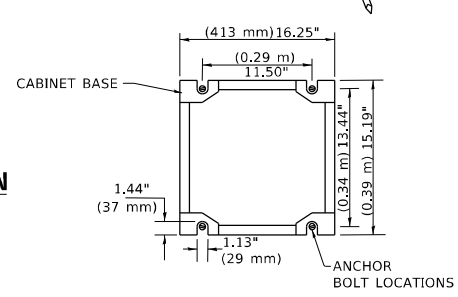


**MAST ARM POLE / POST-GROUNDING DETAIL**  
 (NOT TO SCALE)



**SERVICE INSTALLATION GROUND MOUNT**  
 (NOT TO SCALE)

**CABINET - BASE BOLT PATTERN**  
 (NOT TO SCALE)



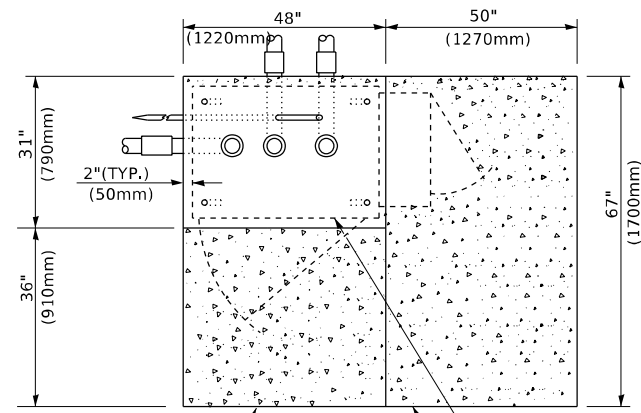
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**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

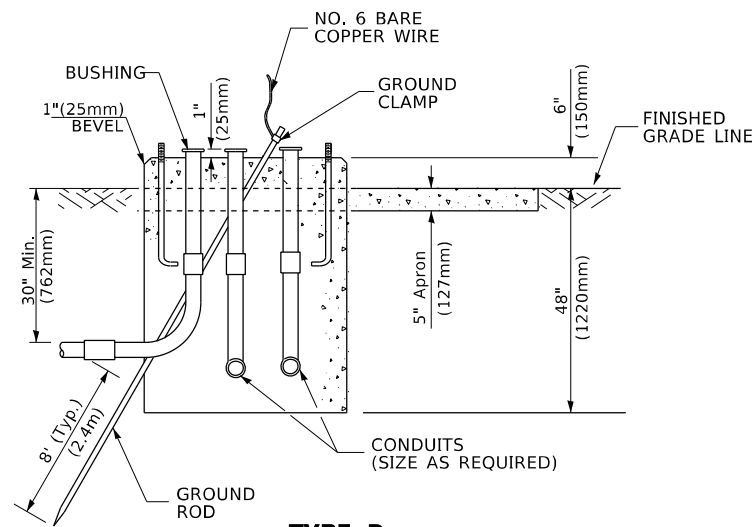
**DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE	SHEET 4	OF 7 SHEETS	STA. TO STA.
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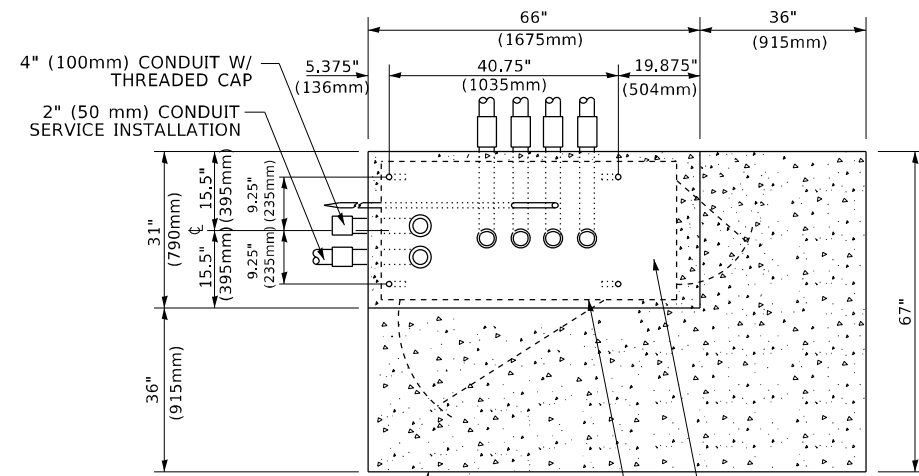
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	38
TS-05		CONTRACT NO. 60T17		
ILLINOIS FED. AID PROJECT				



**TOP VIEW**



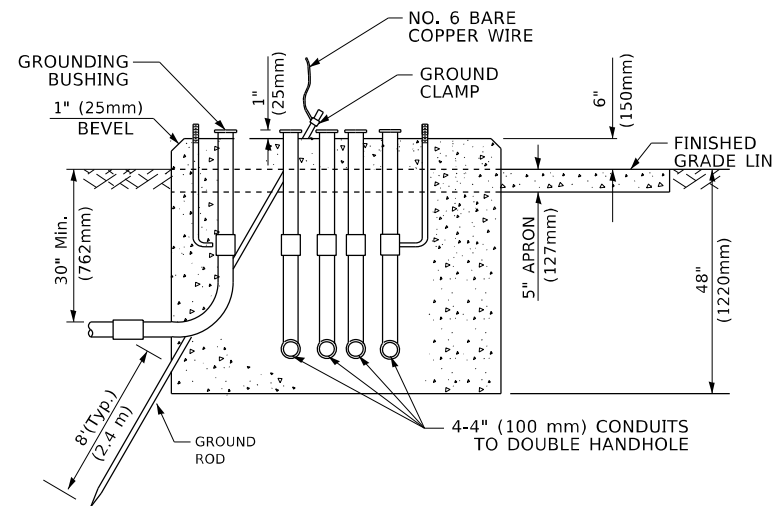
**TYPE D  
FOR GROUND MOUNTED  
CONTROLLER CABINET  
AND UPS BATTERY CABINET**



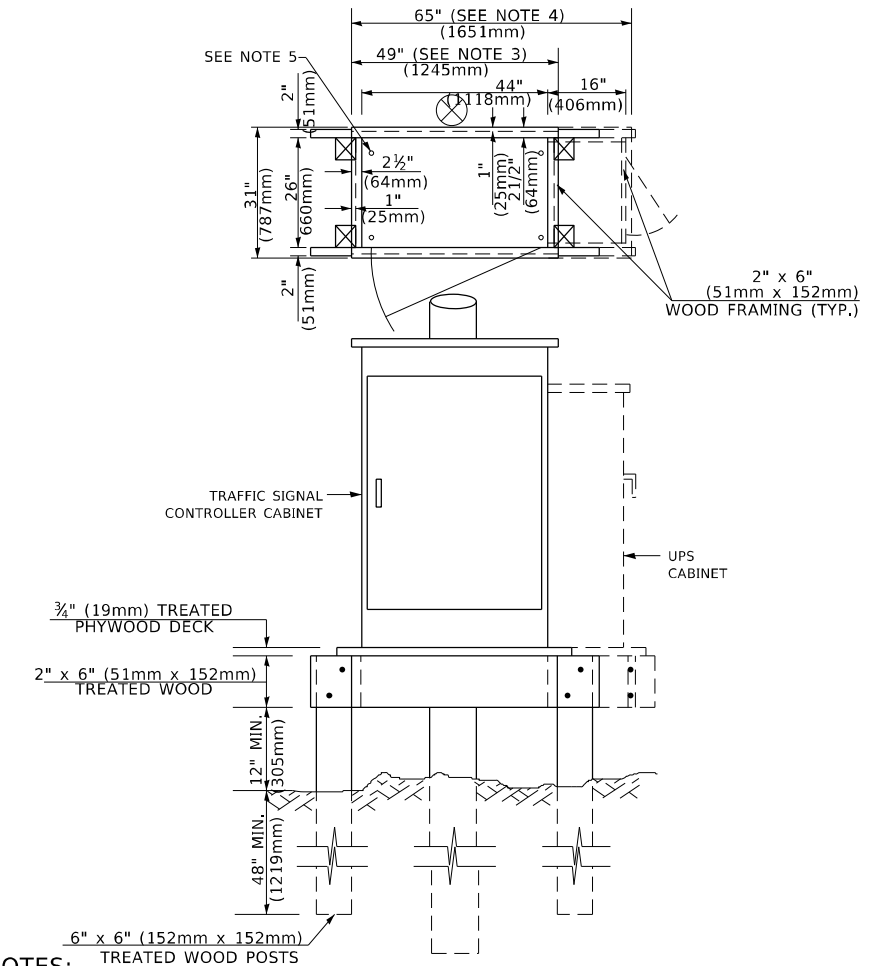
**TOP VIEW**

**NOTE:**

TOP OF FOUNDATION SHALL BE HIGHER THAN TOP OF DOUBLE HANDHOLE



**TYPE C  
FOR GROUND MOUNTED  
SUPER P (TYPE IV) AND SUPER R (TYPE V)  
CONTROLLER CABINETS**



**NOTES:**

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION..

**TEMPORARY SIGNAL CONTROLLER  
WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

**CABLE SLACK**

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE ( MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

**VERTICAL CABLE LENGTH**

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

**DEPTH OF FOUNDATION**

MAST ARM LENGTH	① FOUNDATION DEPTH	FOUNDATION DIAMETER	SPIRAL DIAMETER	QUANTITY OF REBARS	SIZE OF REBARS
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and less than 65' (19.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

**NOTES:**

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average unconfined compressive strength (qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001..

**DEPTH OF MAST ARM FOUNDATIONS, TYPE E**

TS SHT NO. 5  
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PROJECT: 172009\CADD\DATA\Traffic\172009-08-05.dwg

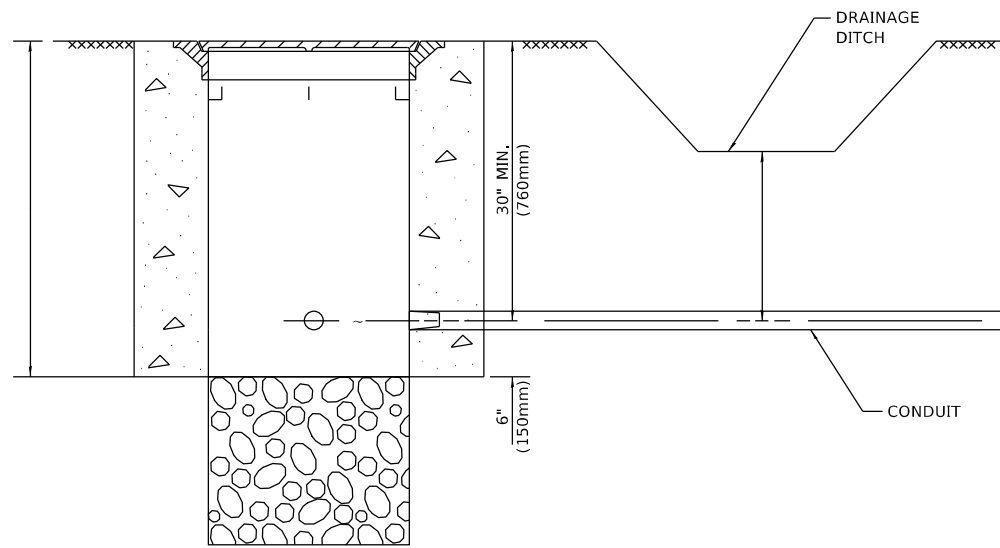
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	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 1/26/2021	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE  
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET 5 OF 7 SHEETS STA. TO STA.

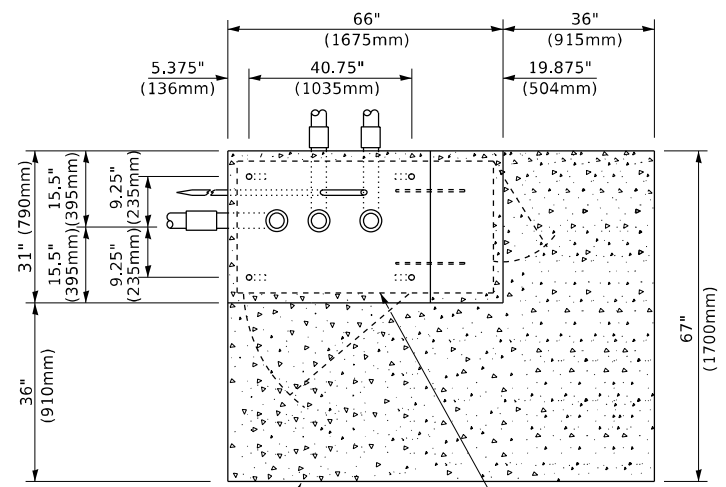
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	39
<b>TS-05</b>			CONTRACT NO. 60T17	
ILLINOIS FED. AID PROJECT				



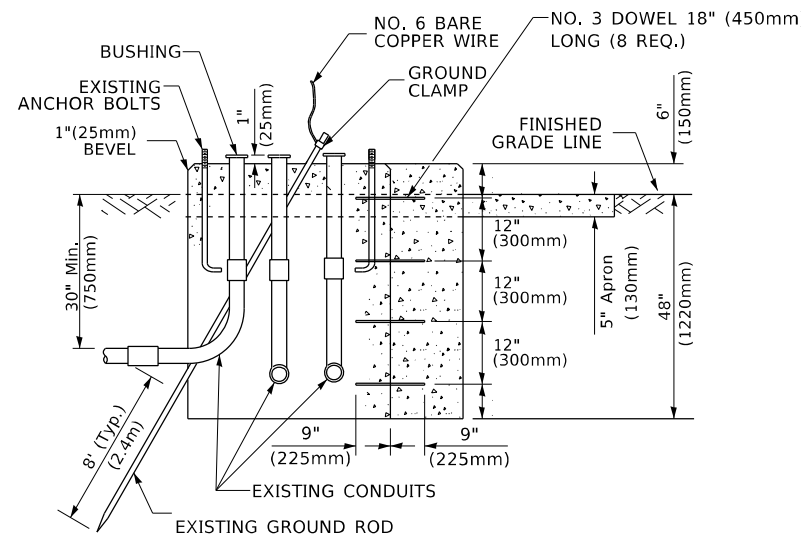
**NOTES:**

1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

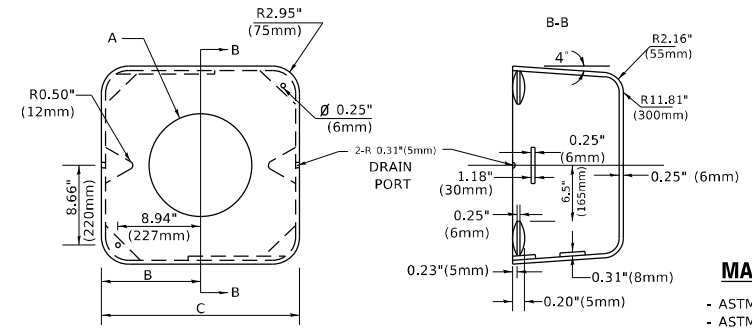
**HANDHOLE WITH MINIMUM CONDUIT DEPTH**  
(NOT TO SCALE)



**TOP VIEW**  
(NOT TO SCALE)



**MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION**  
(NOT TO SCALE)



**MATERIAL**  
- ASTM A36 STEEL  
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIES	9.5"(241mm)	19"(483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75"(273mm)	21.5"(546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0"(330mm)	26"(660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18.5"(470mm)	37"(940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

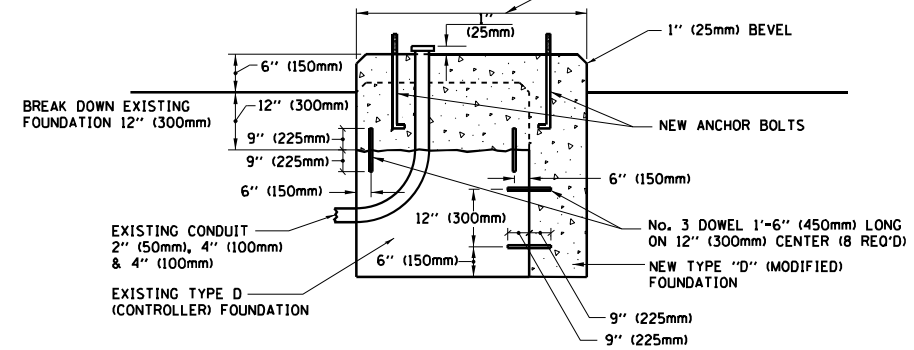
**SHROUD**

**NOTES:**

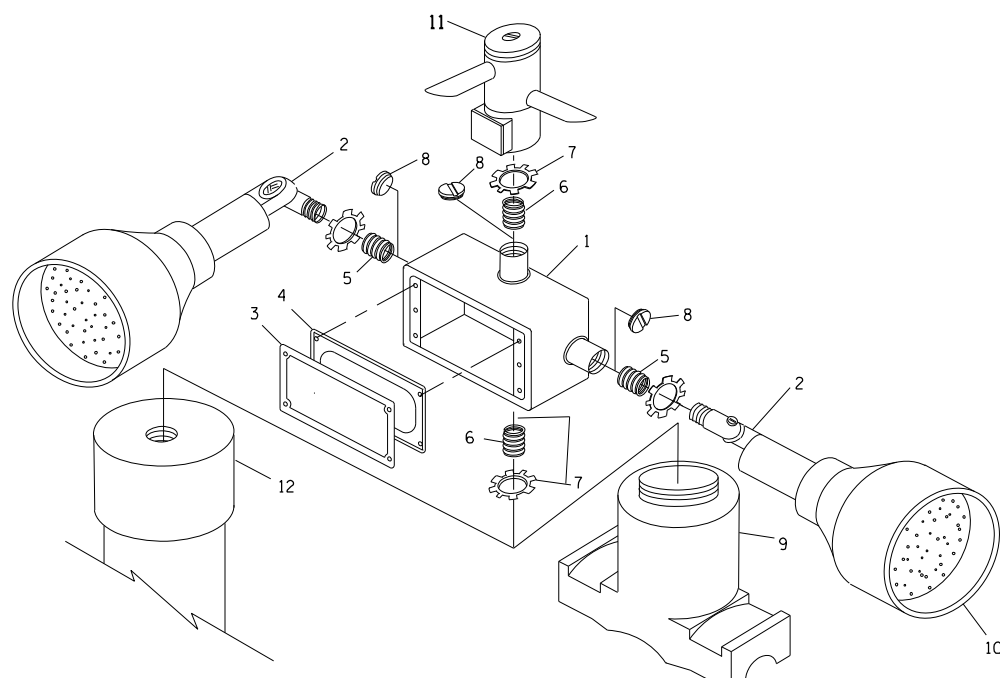
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

**NOTE:**

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



**MODIFY EXISTING TYPE "D" FOUNDATION**

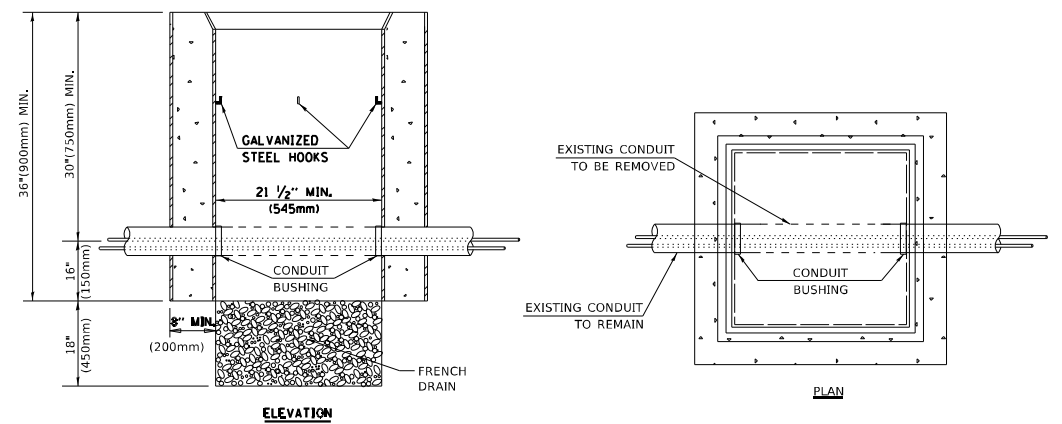


**EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL**

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

**NOTES:**

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT  
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT  
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



**NOTES:**

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

**HANDHOLE TO INTERCEPT EXISTING CONDUIT**

TS SHT NO. 6  
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DRAWN -	REVISED -	REVISED -
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PLOT DATE = 1/26/2021	DATE -	REVISED -

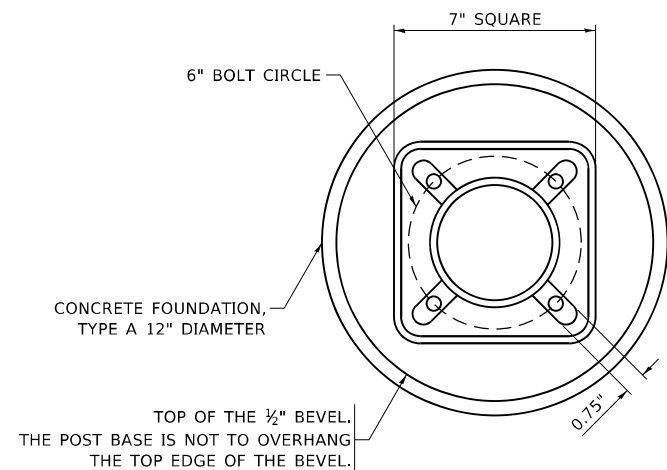
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE  
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET 6 OF 7 SHEETS STA. TO STA.

F.A.U. RTE. 2710	SECTION 52-N	COUNTY COOK	TOTAL SHEETS 78	SHEET NO. 40
TS-05		CONTRACT NO. 60T17		
ILLINOIS FED. AID PROJECT				

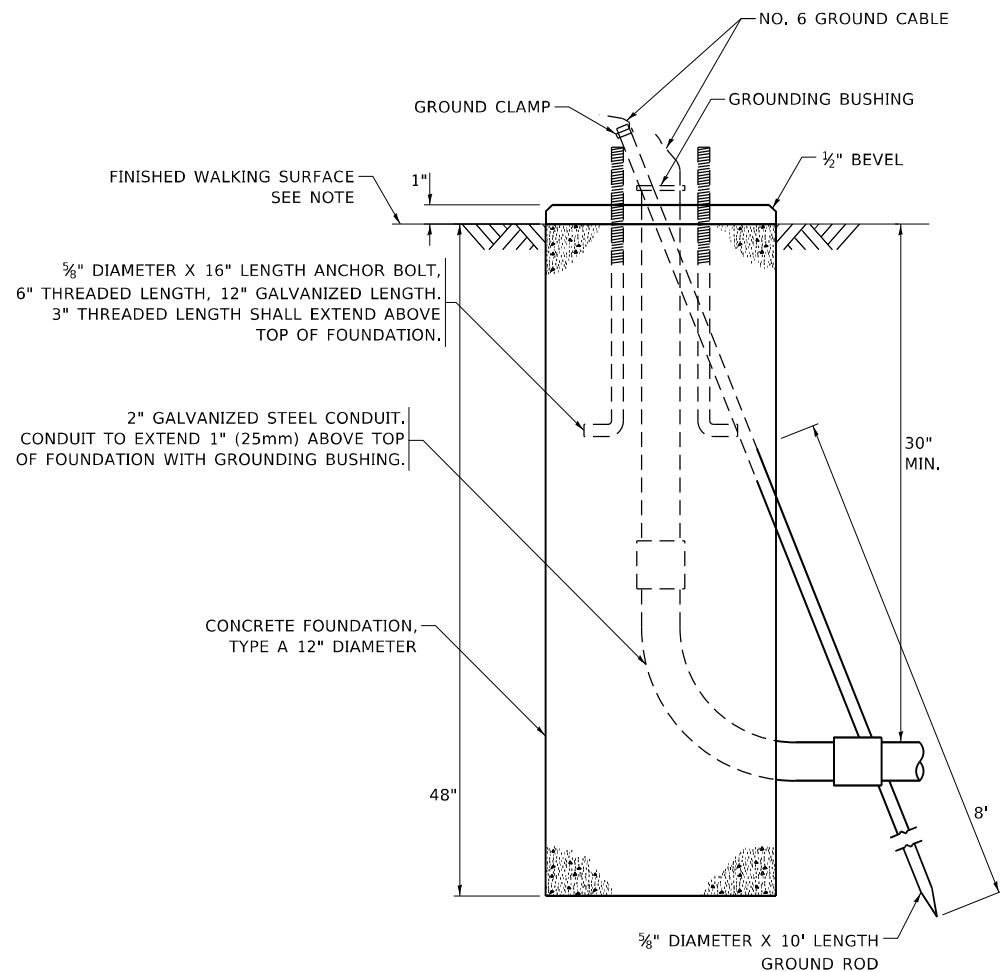




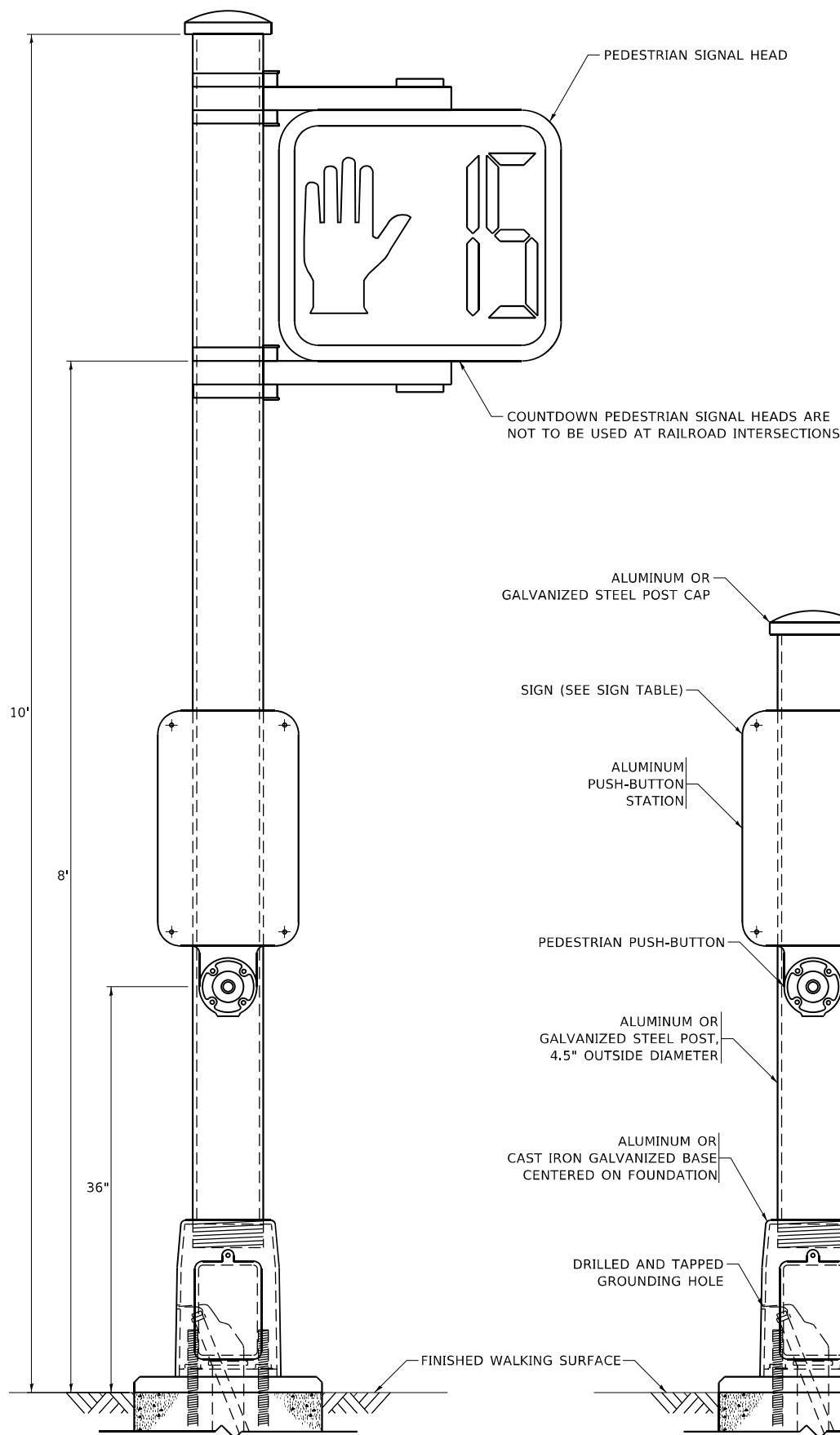
**BOLT PATTERN**

**NOTE:**

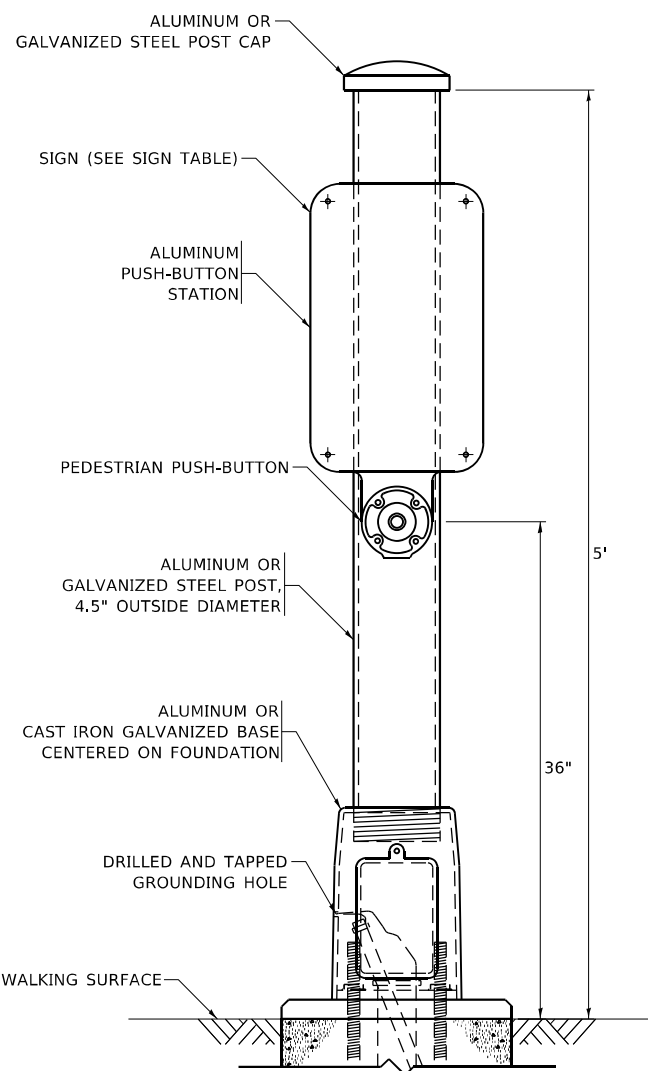
1. IF THE PEDESTRIAN SIGNAL POST FOUNDATION IS INSTALLED WITHIN OR BEHIND A BARRIER CURB, THE TOP OF THE FOUNDATION SHALL BE INSTALLED FLUSH WITH THE TOP OF THE BARRIER CURB.



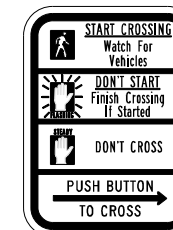
**CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER**



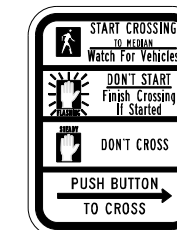
**PEDESTRIAN SIGNAL POST, 10 FT.**



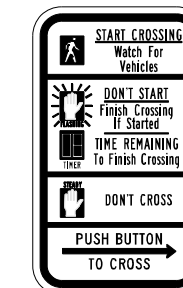
**PEDESTRIAN SIGNAL POST, 5 FT.**



**R10-3b**



**R10-3d**



**R10-3e**

**SIGN TABLE**

SIGN	DIMENSIONS
R10-3b (RAILROAD ONLY)	9" X 12"
R10-3d (RAILROAD ONLY)	9" X 12"
R10-3e	9" X 12"

**NOTES:**

1. THE SIGN PANELS SHALL BE TYPE AP SHEETING.
2. THE ARROW ON SIGNS FOR PUSH-BUTTONS SERVING TWO DIRECTIONS ON THE SAME PHASE SHALL BE BI-DIRECTIONAL.
3. THE SIGN FOR DUAL-CALL PUSH-BUTTONS SHALL HAVE NO ARROW.

TS SHT NO. 7

MODEL: Default  
FILE NAME: p:\pub\amcon\out\illinois\dot\Documents\DOT\_Offices\Distric...  
PROJECT: P172009\CADD\DATA\Traffic\P172009-ht-ss.dgn

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	DATE - 10-15-2018	REVISED -

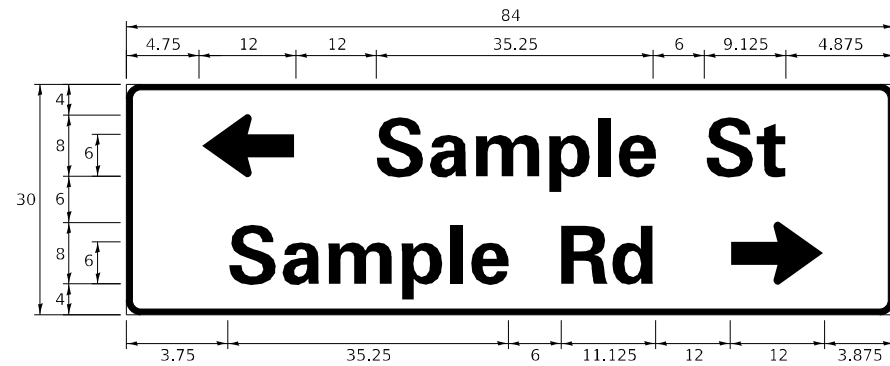
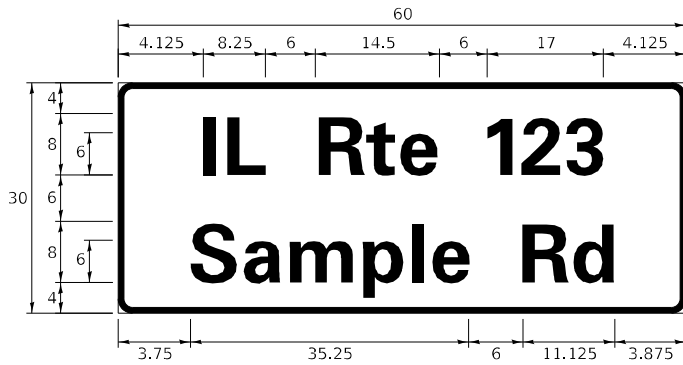
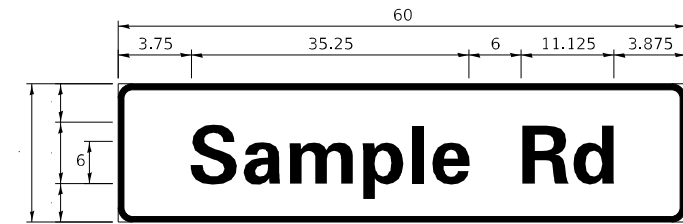
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE  
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET 7 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	41
TS-05		CONTRACT NO. 60T17		
ILLINOIS FED. AID PROJECT				

**SIGN PANEL – TYPE 1 OR TYPE 2**



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
D OR C	-	1 OR 2	ZZ	-

**COMMON STREET NAME ABBREVIATIONS AND WIDTHS**

NAME	ABBREVIATION	WIDTH (INCH)	
		SERIES "C"	SERIES "D"
AVENUE	Ave	15.000	18.250
BOULEVARD	Blvd	17.125	20.000
CIRCLE	Cir	11.125	13.000
COURT	Ct	8.250	9.625
DRIVE	Dr	8.625	10.125
HIGHWAY	Hwy	18.375	22.000
ILLINOIS	IL	7.000	8.250
LANE	Ln	9.125	10.750
PARKWAY	Pkwy	23.375	27.375
PLACE	Pl	7.125	7.750
ROAD	Rd	9.625	11.125
ROUTE	Rte	12.625	14.500
STREET	St	8.000	9.125
TERRACE	Ter	12.625	14.625
TRAIL	Tr	7.750	9.125
UNITED STATES	US	10.375	12.250

**GENERAL NOTES**

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011 AND 877012, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL CONSIST OF A WHITE LEGEND AND BORDER (TYPE ZZ SHEETING) ON A GREEN BACKGROUND (TYPE ZZ SHEETING)
- THE SIGN LENGTH SHALL BE IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHALL NOT EXCEED 8'-0". ALL BORDERS IF POSSIBLE, BUT MAY BE REDUCED TO 5" WHEN SPACING IS CRITICAL. A MINIMUM OF 2-1/2" SHALL BE INCLUDED BETWEEN THE WORD AND THE RIGHT AND LEFT EDGES OF THE SIGN.
- A PREFERRED METHOD FOR THE SIGN DESIGN IS TO USE SERIES "D" LETTER ON A ONE-LINE SIGN 18" IN HEIGHT AND A MAXIMUM OF 8'-0" IN WIDTH. IF SERIES "D" DOES NOT FIT ON A 8'-0" SIGN, THEN SERIES "C" SHOULD BE TRIED. IF SERIES "C" DOES NOT FIT ON A 8'-0" SIGN, A 30" HIGH TWO-LINE SIGN CAN BE USED. THE CROSSROAD DESIGNATION AS TO STREET, AVENUE, ETC. SHOULD BE SPELLED OUT ON THE SECOND LINE, IF THE ABBREVIATION CANNOT FIT ON THE FIRST LINE.
- LED ILLUMINATED STREET NAME SIGNS CAN BE USED IN PLACE OF REGULAR SIGN PANELS BUT ANY SPECIAL WORDING AND SYMBOLOGY MUST BE APPROVED BY THE DEPARTMENT. GENERAL DESIGN REQUIREMENT AS LISTED ABOVE (COLOR, FONT, SIZE, ETC.) MUST BE FOLLOWED.
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS.

**LOCAL SUPPLIERS:**

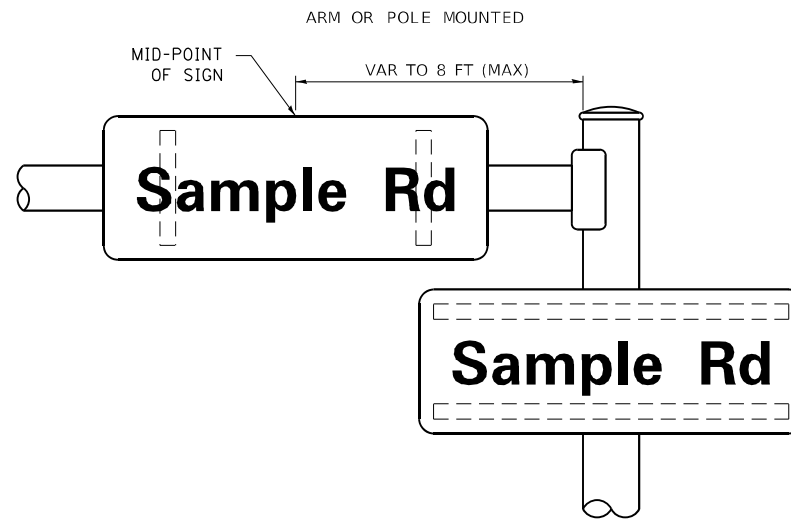
- I.O. HERBERT COMPANY, INC  
MIDLOTHIAN, VA
- WESTERN REMAC, INC.  
WOODRIDGE, IL

**PARTS LISTING:**

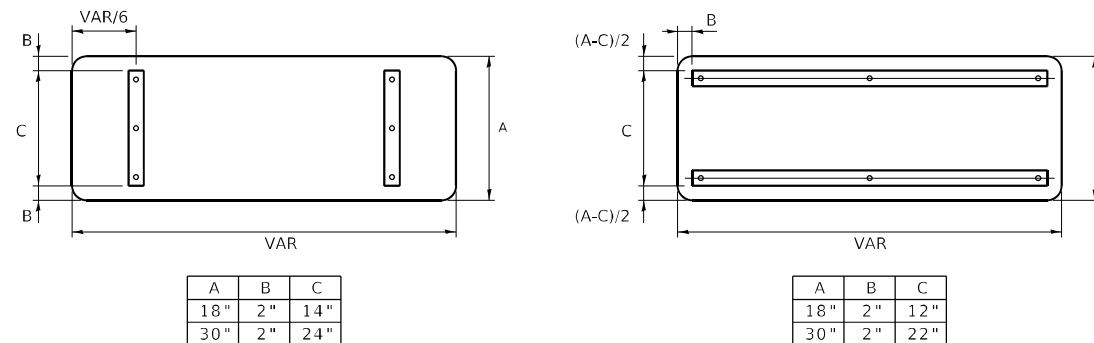
- SIGN CHANNEL PART #HPN053 (MED. CHANNEL)
- SIGN SCREWS 1/4" x 14 x 1" H.W.H. #3
- BRACKETS SELF TAPPING WITH NEOPRENE WASHER PART #HPN034 (UNIVERSAL)
- CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.

**MOUNTING LOCATION**



**SUPPORTING CHANNELS**



A	B	C
18"	2"	14"
30"	2"	24"

A	B	C
18"	2"	12"
30"	2"	22"

**STANDARD ALPHABETS SPACING CHART**

(8") UPPER CASE AND (6") LOWER CASE

FHWA SERIES "C"				FHWA SERIES "D"			
CHARACTER	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)	CHARACTER	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)
A	0.240	5.122	0.240	A	0.240	6.804	0.240
B	0.880	4.482	0.480	B	0.960	5.446	0.400
C	0.720	4.482	0.720	C	0.800	5.446	0.800
D	0.880	4.482	0.720	D	0.960	5.446	0.800
E	0.880	4.082	0.480	E	0.960	4.962	0.400
F	0.880	4.082	0.240	F	0.960	4.962	0.240
G	0.720	4.482	0.720	G	0.800	5.446	0.800
H	0.880	4.482	0.880	H	0.960	5.446	0.960
I	0.880	1.120	0.880	I	0.960	1.280	0.960
J	0.240	4.082	0.880	J	0.240	5.122	0.960
K	0.880	4.482	0.480	K	0.960	5.604	0.400
L	0.880	4.082	0.240	L	0.960	4.962	0.240
M	0.880	5.284	0.880	M	0.960	6.244	0.960
N	0.880	4.482	0.880	N	0.960	5.446	0.960
O	0.720	4.722	0.720	O	0.800	5.684	0.800
P	0.880	4.482	0.720	P	0.960	5.446	0.240
Q	0.720	4.722	0.720	Q	0.800	5.684	0.800
R	0.880	4.482	0.480	R	0.960	5.446	0.400
S	0.480	4.482	0.480	S	0.400	5.446	0.400
T	0.240	4.082	0.240	T	0.240	4.962	0.240
U	0.880	4.482	0.880	U	0.960	5.446	0.960
V	0.240	4.962	0.240	V	0.240	6.084	0.240
W	0.240	6.084	0.240	W	0.240	7.124	0.240
X	0.240	4.722	0.240	X	0.400	5.446	0.400
Y	0.240	5.122	0.240	Y	0.240	6.884	0.240
Z	0.480	4.482	0.480	Z	0.400	5.446	0.400
a	0.320	3.842	0.640	a	0.400	4.562	0.720
b	0.720	4.082	0.480	b	0.800	4.802	0.480
c	0.480	4.002	0.240	c	0.480	4.722	0.240
d	0.480	4.082	0.720	d	0.480	4.802	0.800
e	0.480	4.082	0.320	e	0.480	4.722	0.320
f	0.320	2.480	0.160	f	0.320	2.882	0.160
g	0.480	4.082	0.720	g	0.480	4.802	0.800
h	0.720	4.082	0.640	h	0.800	4.722	0.720
i	0.720	1.120	0.720	i	0.800	1.280	0.800
j	0.000	2.320	0.720	j	0.000	2.642	0.800
k	0.720	4.322	0.160	k	0.800	5.122	0.160
l	0.720	1.120	0.720	l	0.800	1.280	0.800
m	0.720	6.724	0.640	m	0.800	7.926	0.720
n	0.720	4.082	0.640	n	0.800	4.722	0.720
o	0.480	4.082	0.480	o	0.480	4.882	0.480
p	0.720	4.082	0.480	p	0.800	4.802	0.480
q	0.480	4.082	0.720	q	0.480	4.802	0.800
r	0.720	2.642	0.160	r	0.800	3.042	0.160
s	0.320	3.362	0.240	s	0.320	3.762	0.240
t	0.080	2.882	0.080	t	0.080	3.202	0.080
u	0.640	4.082	0.720	u	0.720	4.722	0.800
v	0.160	4.722	0.160	v	0.160	5.684	0.160
w	0.160	7.524	0.160	w	0.160	9.046	0.160
x	0.000	5.202	0.000	x	0.000	6.244	0.000
y	0.160	4.962	0.160	y	0.160	6.004	0.160
z	0.240	3.362	0.240	z	0.240	4.002	0.240
1	0.720	1.680	0.880	1	0.800	2.000	0.960
2	0.480	4.482	0.480	2	0.800	5.446	0.800
3	0.480	4.482	0.480	3	1.440	5.446	0.800
4	0.240	4.962	0.720	4	0.160	6.004	0.960
5	0.480	4.482	0.480	5	0.800	5.446	0.800
6	0.720	4.482	0.720	6	0.800	5.446	0.800
7	0.240	4.482	0.720	7	0.560	5.446	0.560
8	0.480	4.482	0.480	8	0.800	5.446	0.800
9	0.480	4.482	0.480	9	0.800	5.446	0.800
0	0.720	4.722	0.720	0	0.800	5.684	0.800
-	0.240	2.802	0.240	-	0.240	2.802	0.240

**TS SHT NO. 8**

MODEL: Default  
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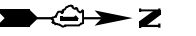
USER NAME = koby/ekka	DESIGNED - LP/IP	REVISED - LP 07/01/2015
PLOT SCALE = 100,0000' / in.	CHECKED - IP	REVISED -
PLOT DATE = 1/26/2021	DATE - 10/01/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE  
MAST ARM MOUNTED STREET NAME SIGNS**

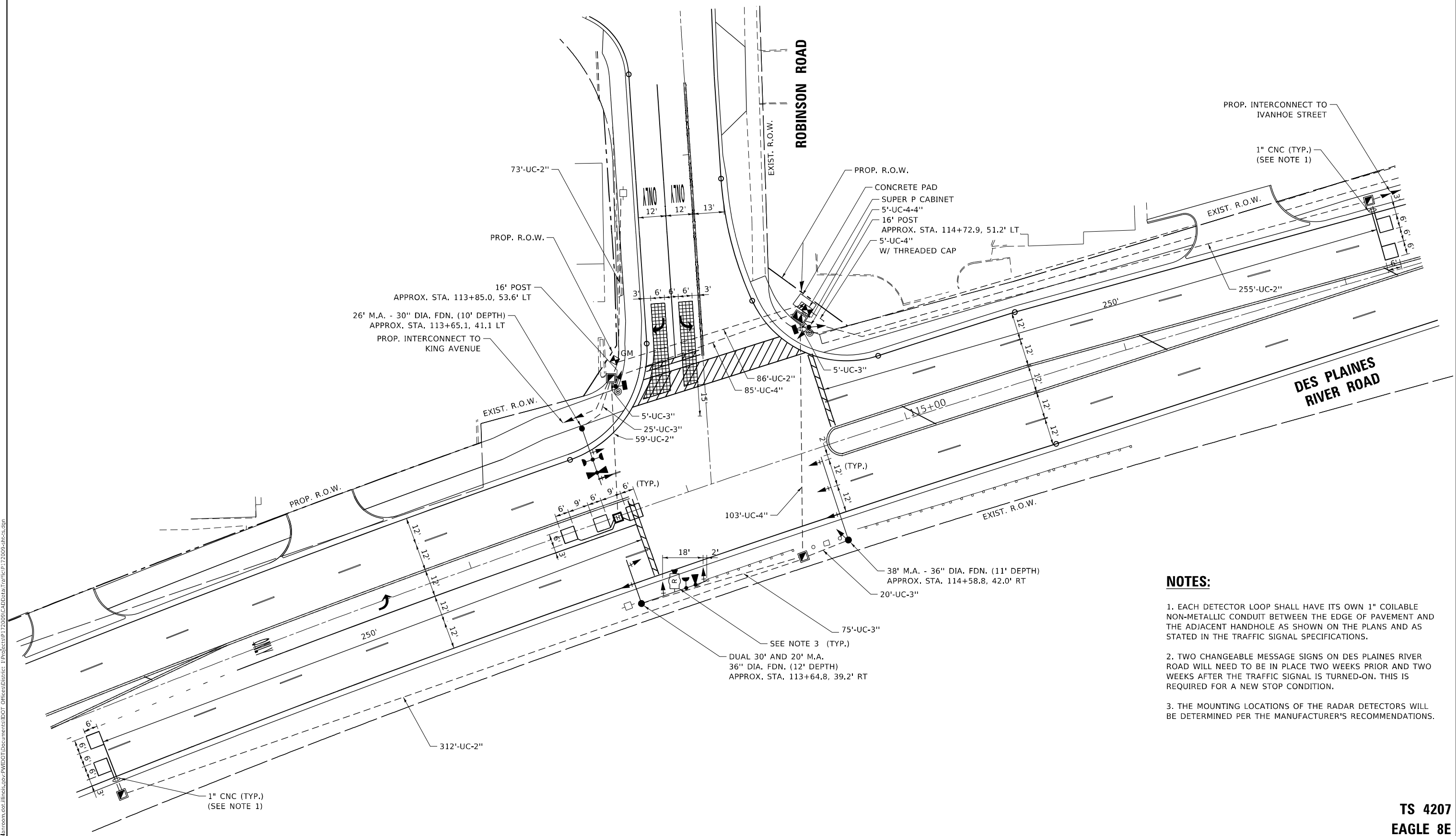
F.A.U. RTE. 2710	SECTION 52-N	COUNTY COOK	TOTAL SHEETS 78	SHEET NO. 42
TS-02		CONTRACT NO. 60T17		
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.



TS SHT NO. 9

MODEL: Default  
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**NOTES:**

1. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.
2. TWO CHANGEABLE MESSAGE SIGNS ON DES PLAINES RIVER ROAD WILL NEED TO BE IN PLACE TWO WEEKS PRIOR AND TWO WEEKS AFTER THE TRAFFIC SIGNAL IS TURNED-ON. THIS IS REQUIRED FOR A NEW STOP CONDITION.
3. THE MOUNTING LOCATIONS OF THE RADAR DETECTORS WILL BE DETERMINED PER THE MANUFACTURER'S RECOMMENDATIONS.

TS 4207  
EAGLE 8E

USER NAME = koby/kaka	DESIGNED - KK	REVISED -
	DRAWN - KK	REVISED -
PLOT SCALE = 40,0000 * / in.	CHECKED - LP	REVISED -
PLOT DATE = 1/26/2021	DATE - 02/20/2020	REVISED -

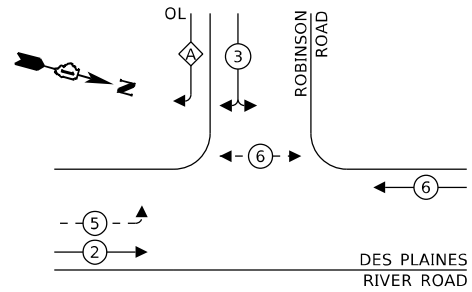
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC SIGNAL INSTALLATION PLAN  
DES PLAINES RIVER ROAD AT ROBINSON ROAD**

SCALE: 1"=20' SHEET OF SHEETS STA. 105+00.00 TO STA. 120+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	43
CONTRACT NO. 60T17				
ILLINOIS FED. AID PROJECT				

**PROPOSED CONTROLLER SEQUENCE**



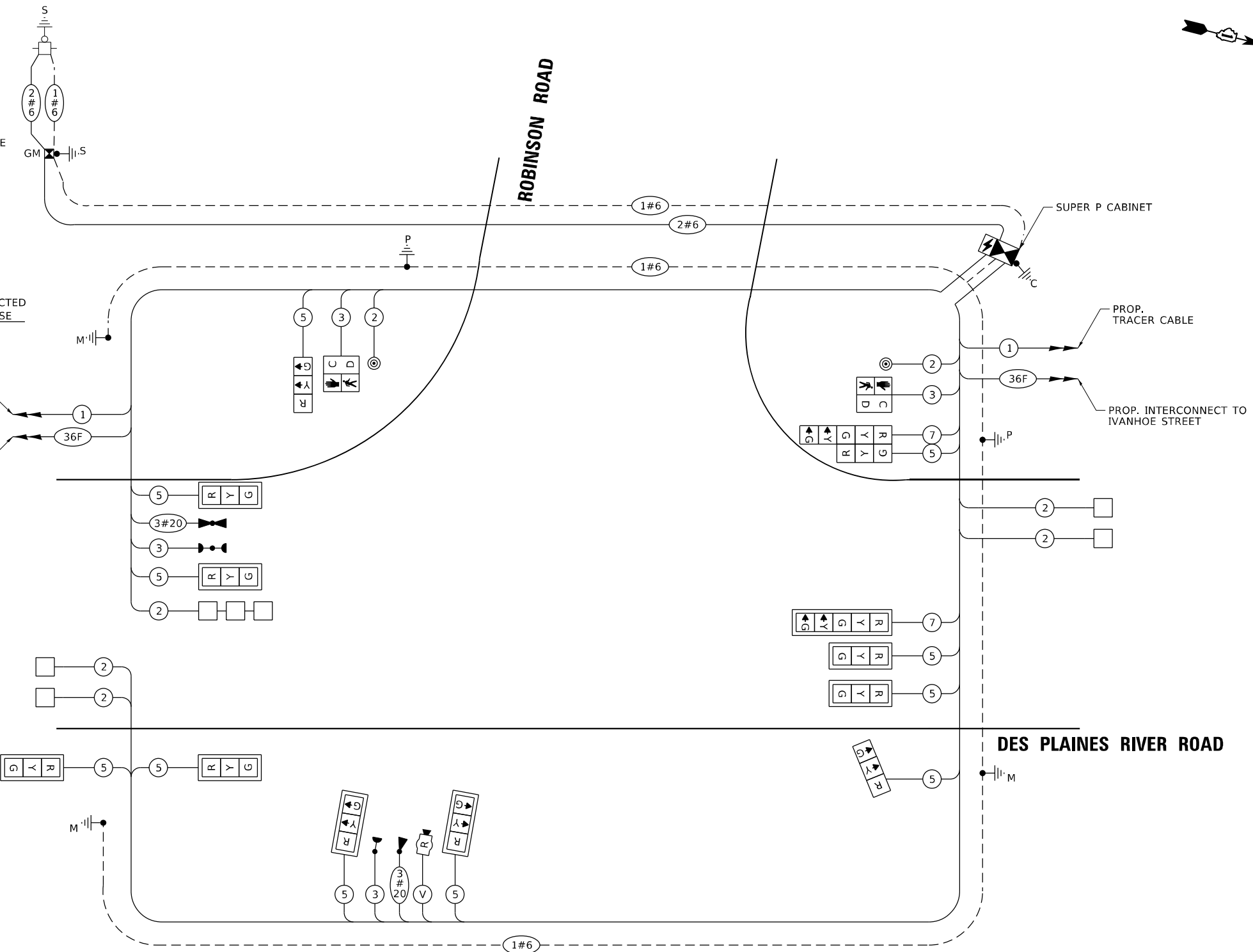
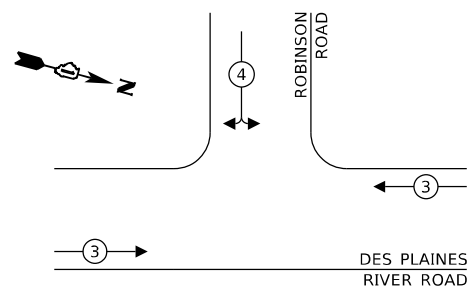
**LEGEND:**

- ← \* → PROTECTED PHASE
- ← \* - - \* → PROTECTED/PERMITTED PHASE
- ← \* → PEDESTRIAN PHASE
- ← \* → OL OVERLAP

**RIGHT TURN OVERLAP PHASE DESIGNATION:**

OVERLAP LETTER A = PERMISSIVE PHASE 3 + PROTECTED PHASE 5

**PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE**



**CABLE PLAN**  
(NOT TO SCALE)

**TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS**

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	13	11	50	71.5
(YELLOW)	13	20	5	13.0
(GREEN)	13	12	45	70.2
PERMISSIVE ARROW	4	10	10	4.0
PED. SIGNAL	2	20	100	40.0
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
<b>TOTAL =</b>				<b>323.7</b>

ENERGY COSTS TO:  
**VILLAGE OF FRANKLIN PARK**  
 9500 W BELMONT AVE  
 FRANKLIN PARK, IL 60131  
 ENERGY SUPPLY: CONTACT: JOE STACHO  
 PHONE: (630) 424-5704  
 COMPANY: COMMONWEALTH EDISON  
 ACCOUNT NUMBER: 22570-77046

**TS SHT NO. 10**

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DESIGNED -	KK	REVISED -	
DRAWN -	KK	REVISED -	
CHECKED -	LP	REVISED -	
DATE -	02/20/2020	REVISED -	

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CABLE PLAN, PHASE DESIGNATION DIAGRAM,  
AND EMERGENCY VEHICLE PREEMPTION SEQUENCE  
DES PLAINES RIVER ROAD AT ROBINSON ROAD**

SCALE: NONE SHEET OF SHEETS STA. 105+00.00 TO STA. 120+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	44
CONTRACT NO. 60T17				
ILLINOIS FED. AID PROJECT				

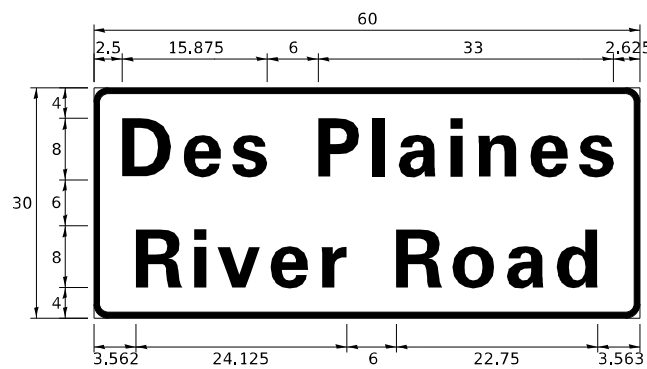
**TS 4207  
EAGLE 8E**

**SIGN PANEL – TYPE 1 OR TYPE 2**

ALL DIMENSIONS ARE IN INCHES UNLESS NOTED OTHERWISE



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
D	8.25	1	ZZ	2



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
D	12.50	2	ZZ	1

NOTE: FOR ADDITIONAL DESIGN AND INSTALLATION INFORMATION PLEASE SEE DISTRICT ONE MAST ARM MOUNTED STREET NAME SIGNS DETAIL.

**SCHEDULE OF QUANTITIES**

ITEM DESCRIPTION	UNITS	TOTAL QTY.
CHANGEABLE MESSAGE SIGN	CAL DA	60
SIGN PANEL - TYPE 1	SQ FT	16.5
SIGN PANEL - TYPE 2	SQ FT	12.5
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	785
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	130
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	213
HANDHOLE	EACH	4
HEAVY-DUTY HANDHOLE	EACH	1
DOUBLE HANDHOLE	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	183
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	619
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	2101
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	262
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	1649
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	197
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	606
TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	2
STEEL MAST ARM ASSEMBLY AND POLE, 26 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 38 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS, 20 FT. AND 30 FT.	EACH	1
CONCRETE FOUNDATION, TYPE A	FOOT	12
CONCRETE FOUNDATION, TYPE C	FOOT	4
CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	10
CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	23
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	8
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	3
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	1
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	1
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	2
TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC	EACH	9
INDUCTIVE LOOP DETECTOR	EACH	5
DETECTOR LOOP, TYPE I	FOOT	234
* LIGHT DETECTOR	EACH	2
* LIGHT DETECTOR AMPLIFIER	EACH	1
* PEDESTRIAN PUSH-BUTTON	EACH	2
* EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C	FOOT	436
FULL-ACTUATED CONTROLLER AND TYPE SUPER P CABINET (SPECIAL)	EACH	1
SERVICE INSTALLATION, GROUND MOUNTED, METERED	EACH	1
RADAR VEHICLE DETECTION SYSTEM, SINGLE APPROACH, STOP BAR	EACH	1
UNINTERRUPTABLE POWER SUPPLY, SPECIAL	EACH	1

\* 100% COST TO THE VILLAGE OF FRANKLIN PARK

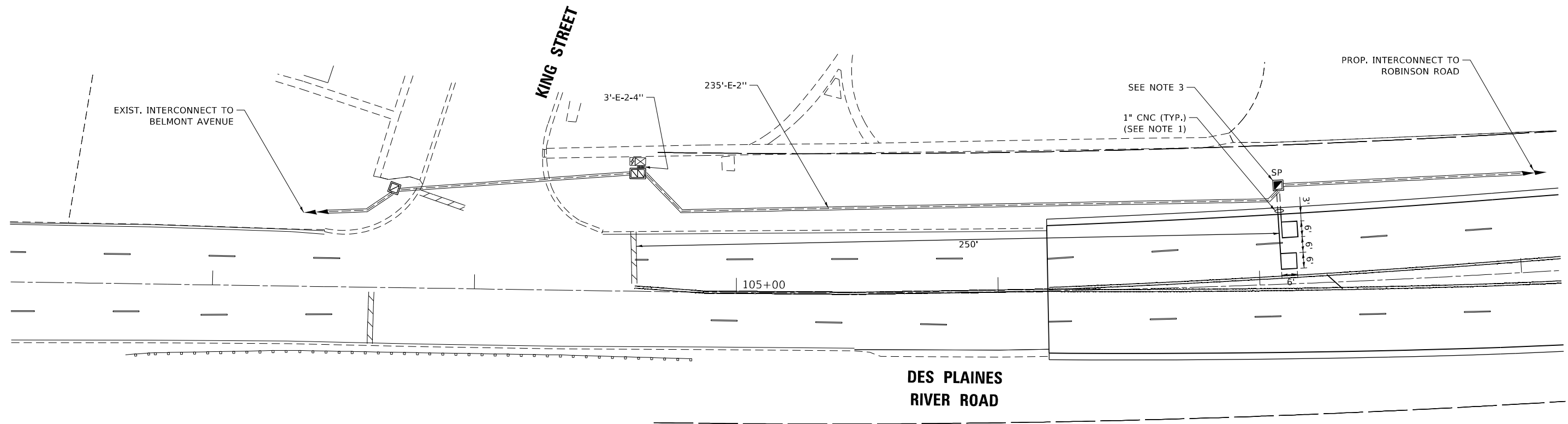
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PLOT DATE = 1/26/2021	CHECKED - LP	REVISED -
	DATE - 02/20/2020	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

MAST ARM MOUNTED STREET NAME SIGNS  
AND SCHEDULE OF QUANTITIES  
DES PLAINES RIVER ROAD AT ROBINSON ROAD

SCALE: NONE SHEET OF SHEETS STA. 105+00.00 TO STA. 120+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	45
CONTRACT NO. 60T17				
ILLINOIS FED. AID PROJECT				



**NOTES:**

1. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.
2. CONTRACTOR SHALL INSTALL NEW CABLE "LEAD-IN, NO. 14 1 PAIR" BETWEEN THE CONTROLLER AND THE PROPOSED DETECTOR LOOPS. EACH LOOP SHOULD HAVE ITS OWN SEPARATE LEAD-IN CABLE.
3. PROPOSED HANDHOLE TO BE PAID UNDER INTERCONNECT PLANS.
4. THIS PLAN SHEET IS ONLY FOR HANDHOLE AND DETECTOR LOOP REPLACEMENT.

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**TS SHT NO. 12**

**TS 11040  
EAGLE 8E**

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	DRAWN - KK	REVISED -
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PLOT DATE = 1/26/2021	DATE - 02/20/2020	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

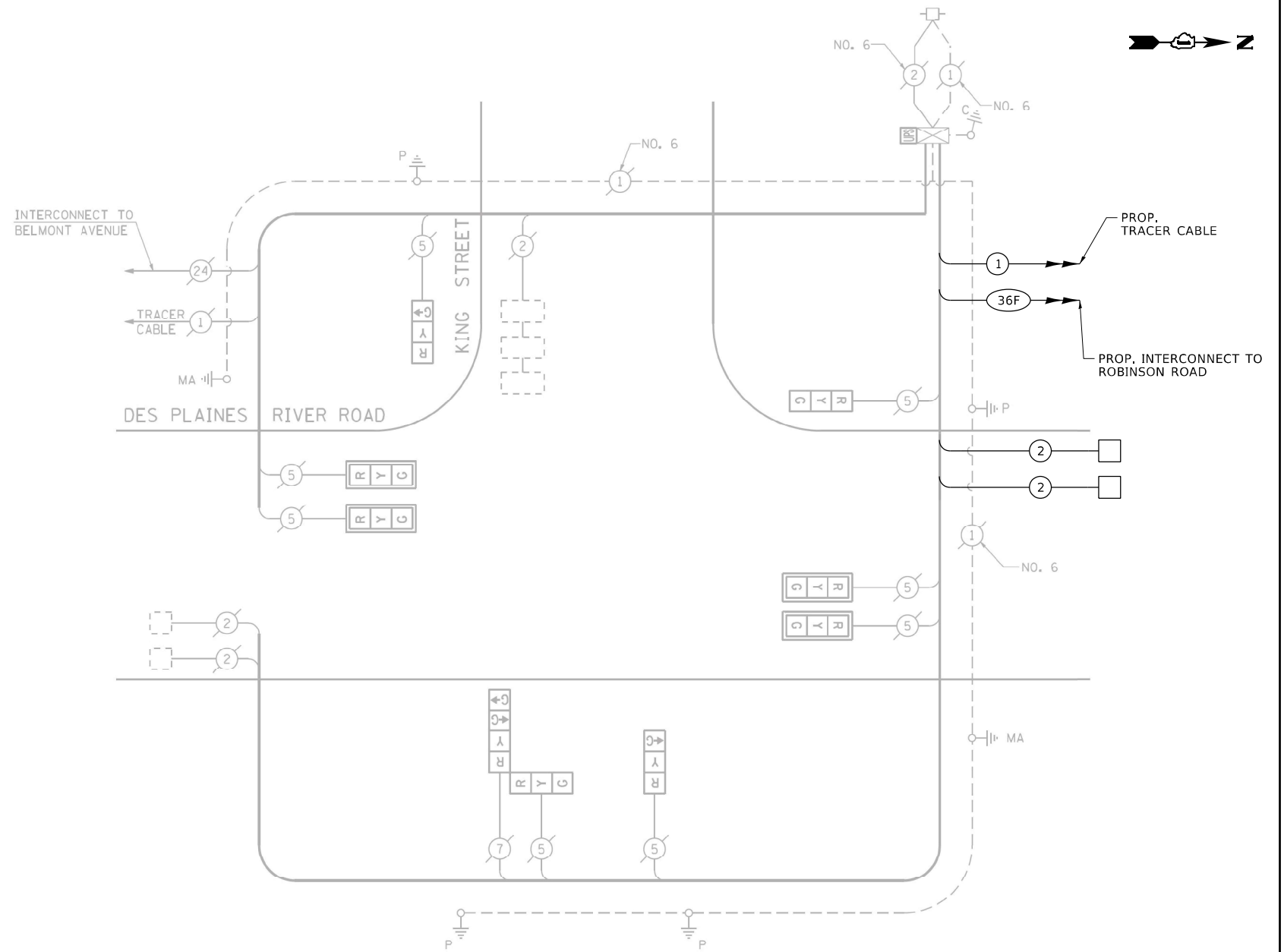
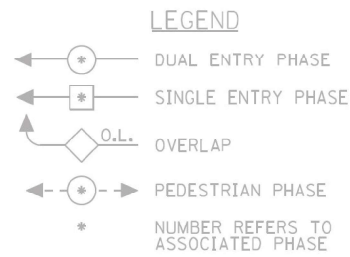
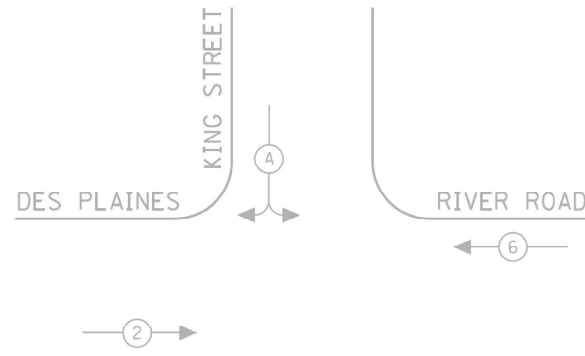
**TRAFFIC SIGNAL MODIFICATION PLAN  
DES PLAINES RIVER ROAD AT KING STREET**

SCALE: 1"=20'    SHEET    OF    SHEETS    STA. 105+00.00    TO STA. 120+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	46
CONTRACT NO. 60T17				
ILLINOIS FED. AID PROJECT				

**EXISTING PHASE DESIGNATION DIAGRAM**

**CONTROLLER SEQUENCE**



**CABLE PLAN**

(NOT TO SCALE)

**NOTE:**

1. CABLE PLAN IS FOR FAR BACK DETECTOR LOOPS AND LEAD-IN CABLE REPLACEMENT PURPOSE ONLY.

**SCHEDULE OF QUANTITIES**

ITEM DESCRIPTION	UNITS	TOTAL QTY.
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	524
DETECTOR LOOP, TYPE I	FOOT	69
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	524
ROD AND CLEAN EXISTING CONDUIT	FOOT	219

**TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS**

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	9	11	50	49,5
(YELLOW)	9	20	5	9,0
(GREEN)	9	12	45	48,6
PERMISSIVE ARROW	-	10	10	-
PED. SIGNAL	-	20	100	-
CONTROLLER	1	100	100	100,0
UPS	1	25	100	25,0
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
<b>TOTAL =</b>				<b>232.1</b>

ENERGY COSTS TO:

ILLINOIS DEPARTMENT OF TRANSPORTATION  
201 CENTER CT  
SCHAUMBURG, IL 60196

ENERGY SUPPLY: CONTACT: JOE STACHO  
PHONE: (630) 424-5704  
COMPANY: COMMONWEALTH EDISON  
ACCOUNT NUMBER: ---

TS SHT NO. 13

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PLOT DATE = 2/27/2020	CHECKED - LP	REVISED -
	DATE - 02/20/2020	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CABLE PLAN AND PHASE DESIGNATION DIAGRAM  
DES PLAINES RIVER ROAD AT KING STREET**

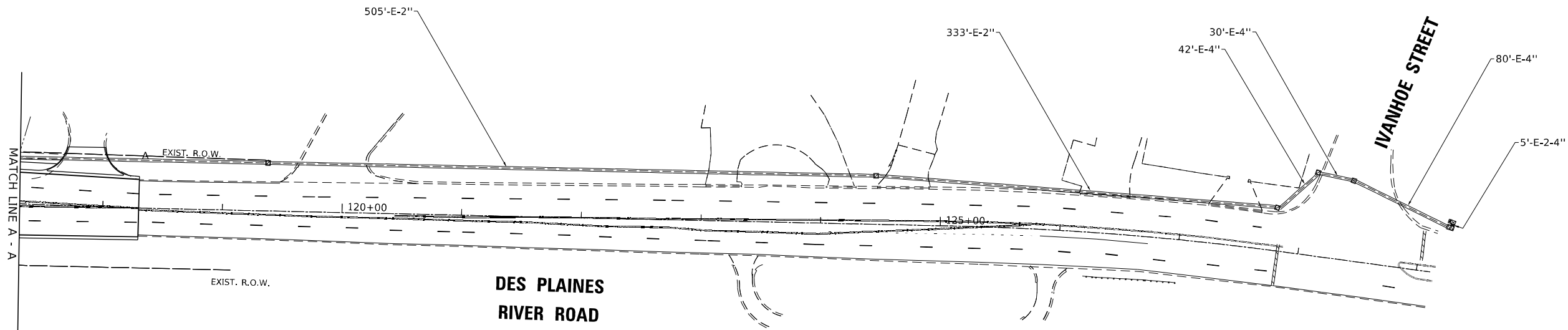
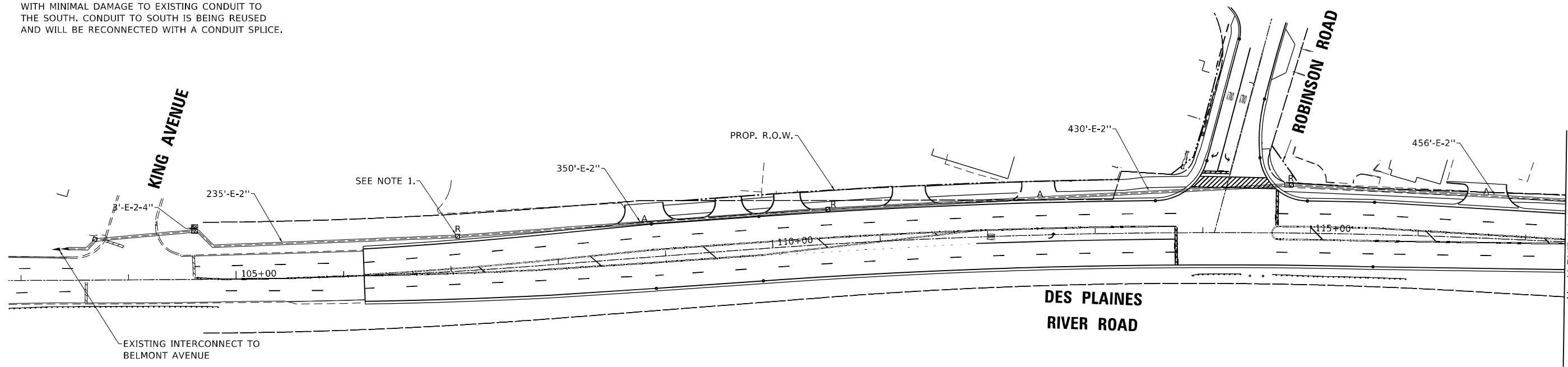
SCALE: 1"=20' SHEET OF SHEETS STA. 105+00.00 TO STA. 120+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	47
CONTRACT NO. 60T17				
ILLINOIS FED. AID PROJECT				

**TS 11040  
EAGLE 8E**

**NOTE:**

1. CONTRACTOR SHOULD REMOVE EXISTING HANDHOLE WITH MINIMAL DAMAGE TO EXISTING CONDUIT TO THE SOUTH. CONDUIT TO SOUTH IS BEING REUSED AND WILL BE RECONNECTED WITH A CONDUIT SPLICE.



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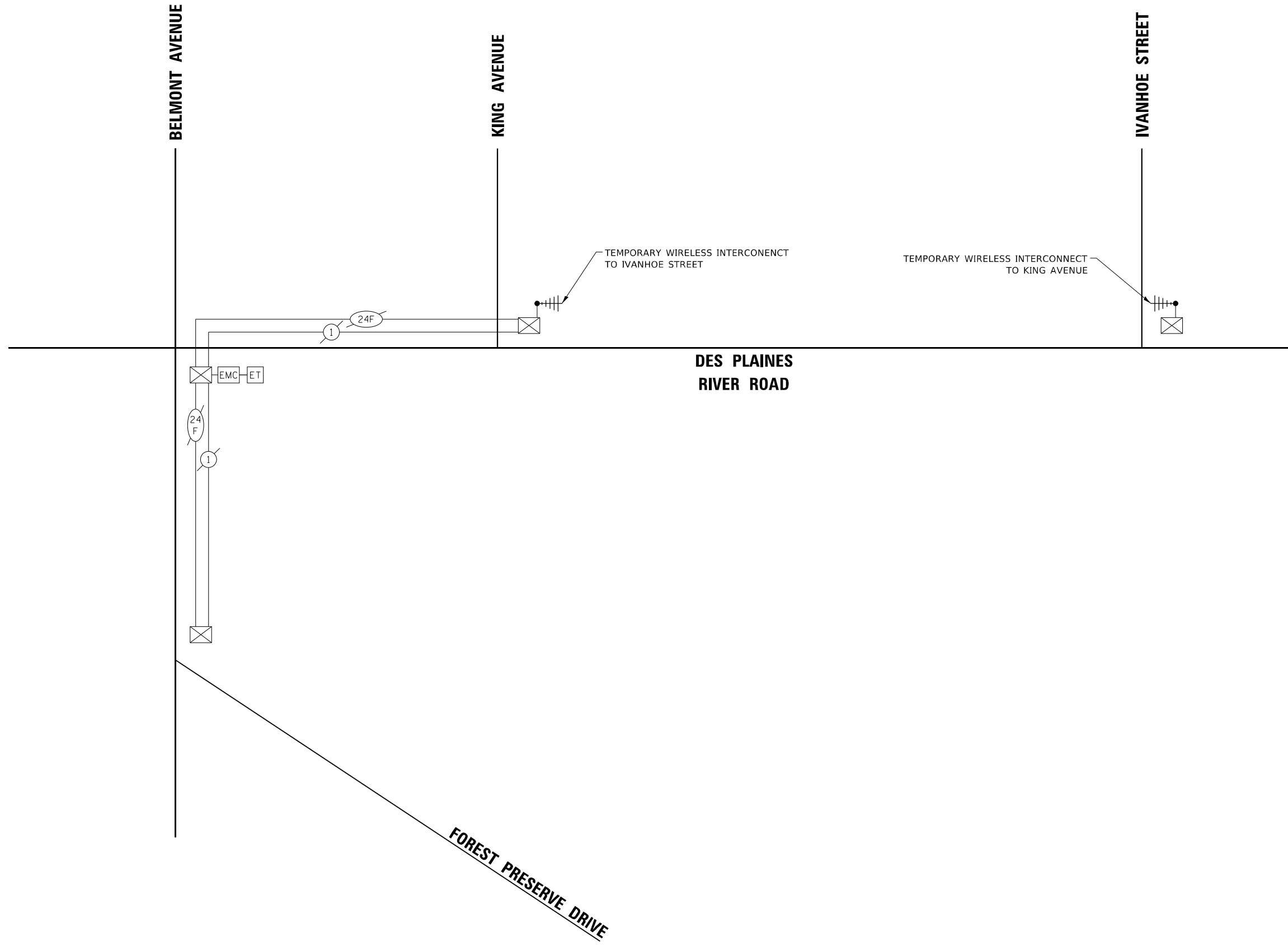
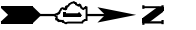
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

<b>INTERCONNECT REMOVAL PLAN</b> <b>DES PLAINES RIVER ROAD - IVANHOE STREET TO BELMONT AVENUE TO FOREST PRESERVE DRIVE</b>			
SCALE: 1"=50'	SHEET	OF	SHEETS
			STA. 105+00.00 TO STA. 129+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	48
			CONTRACT NO. 60T17	
ILLINOIS FED. AID PROJECT				

**EAGLE 8E**





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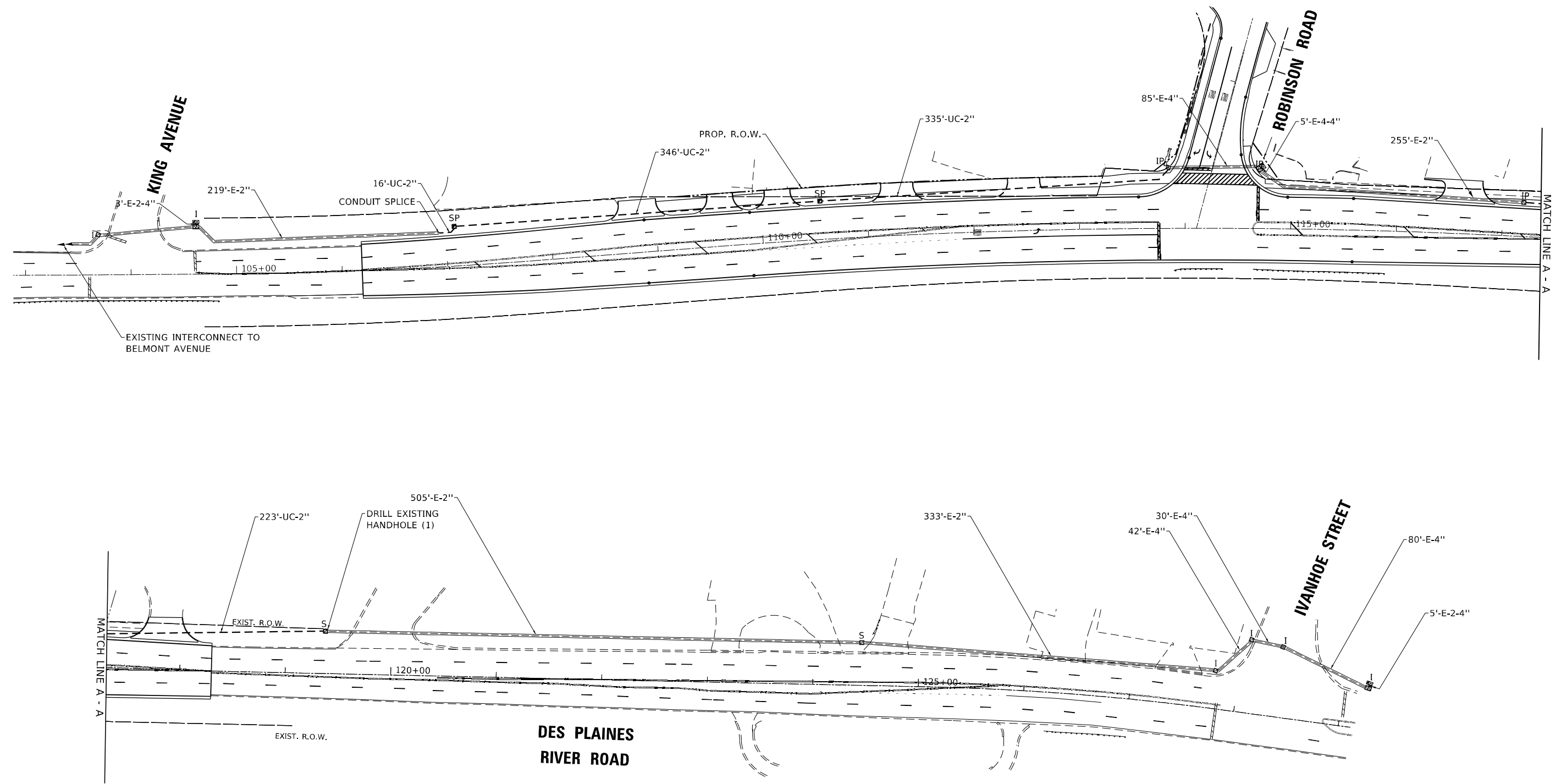
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PLOT DATE = 1/26/2021	DATE - 02/20/2020	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TEMPORARY INTERCONNECT SCHEMATIC			
DES PLAINES RIVER ROAD - IVANHOE STREET TO BELMONT AVENUE TO FOREST PRESERVE DRIVE			
SCALE: NONE	SHEET	OF	SHEETS
STA. 105+00.00		TO STA. 129+00.00	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	49
CONTRACT NO. 60T17				
ILLINOIS FED. AID PROJECT				

EAGLE 8E



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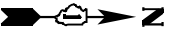
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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**PROPOSED INTERCONNECT PLAN**  
**DES PLAINES RIVER ROAD - IVANHOE STREET TO BELMONT AVENUE TO FOREST PRESERVE DRIVE**  
 SCALE: 1"=50'    SHEET    OF    SHEETS    STA. 105+00.00    TO    STA. 129+00.00

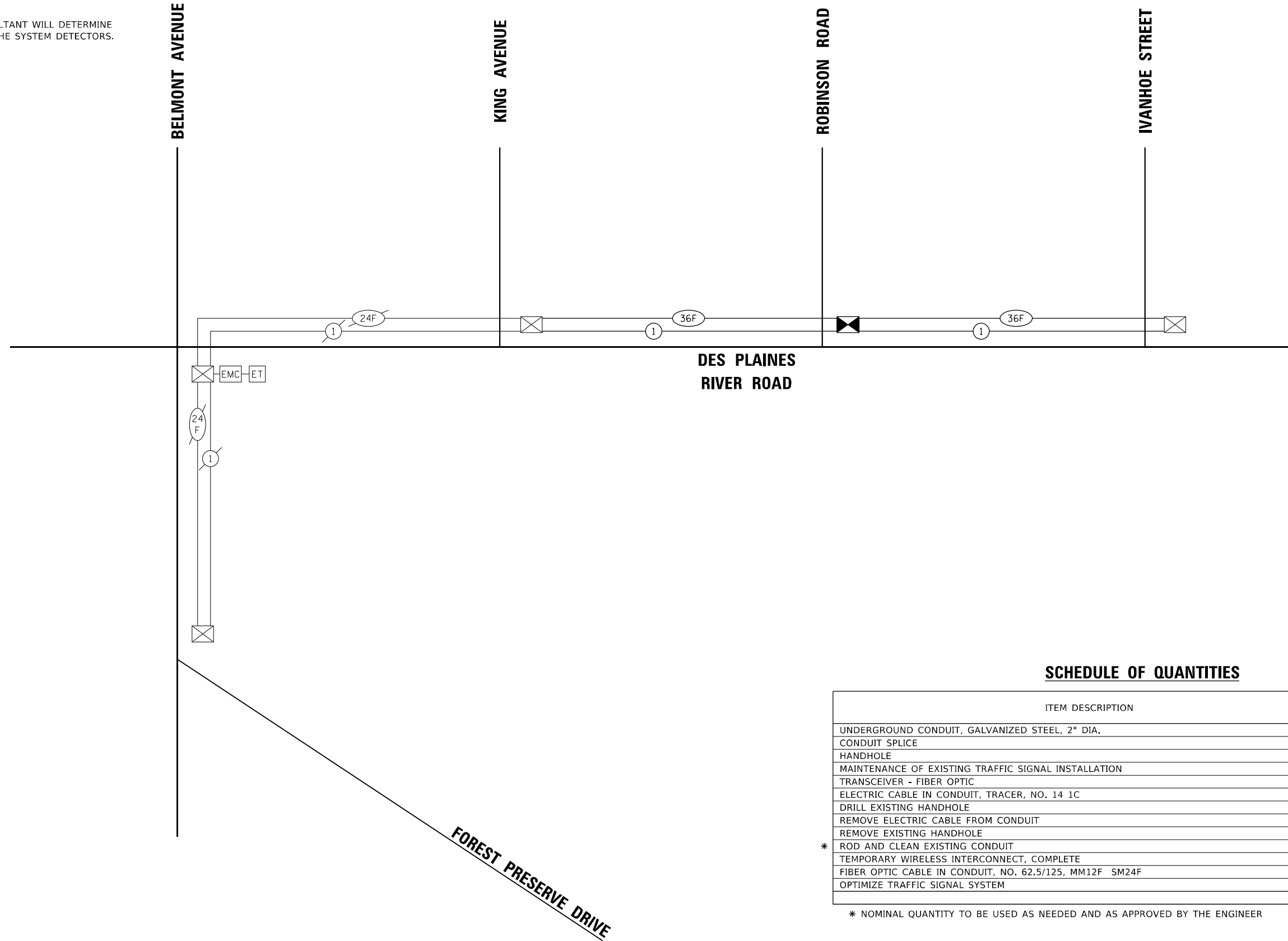
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	50
CONTRACT NO. 60T17				
ILLINOIS FED. AID PROJECT				

**EAGLE 8E**



**NOTE:**

1. THE SCAT CONSULTANT WILL DETERMINE THE LOCATION OF THE SYSTEM DETECTORS.



**SCHEDULE OF QUANTITIES**

ITEM DESCRIPTION	UNITS	TOTAL QTY.
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	920
CONDUIT SPLICE	EACH	1
HANDHOLE	EACH	2
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2
TRANSCEIVER - FIBER OPTIC	EACH	1
ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	2554
DRILL EXISTING HANDHOLE	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	5080
REMOVE EXISTING HANDHOLE	EACH	3
* ROD AND CLEAN EXISTING CONDUIT	FOOT	838
TEMPORARY WIRELESS INTERCONNECT, COMPLETE	L SUM	1
FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F	FOOT	2662
OPTIMIZE TRAFFIC SIGNAL SYSTEM	EACH	1

\* NOMINAL QUANTITY TO BE USED AS NEEDED AND AS APPROVED BY THE ENGINEER

**TS SHT NO.17**

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PLOT DATE = 1/26/2021	DATE - 02/20/2020	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROPOSED INTERCONNECT SCHEMATIC AND SCHEDULE OF QUANTITIES  
DES PLAINES RIVER ROAD - IVANHOE STREET TO BELMONT  
AVENUE TO FOREST PRESERVE DRIVE**

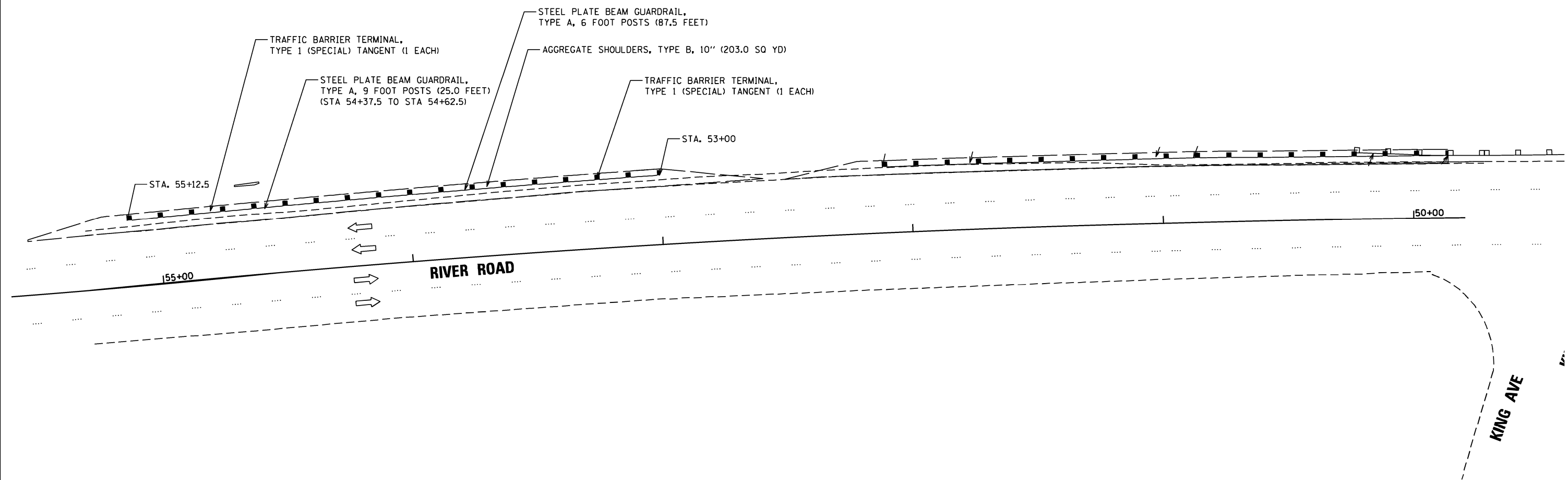
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	51

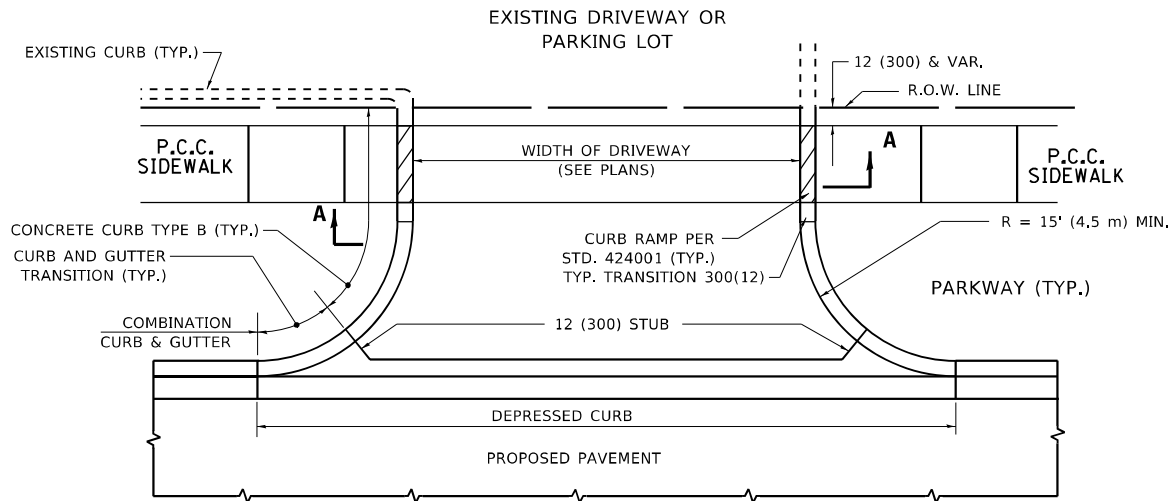
CONTRACT NO. 60T17  
ILLINOIS FED. AID PROJECT

**EAGLE 8E**

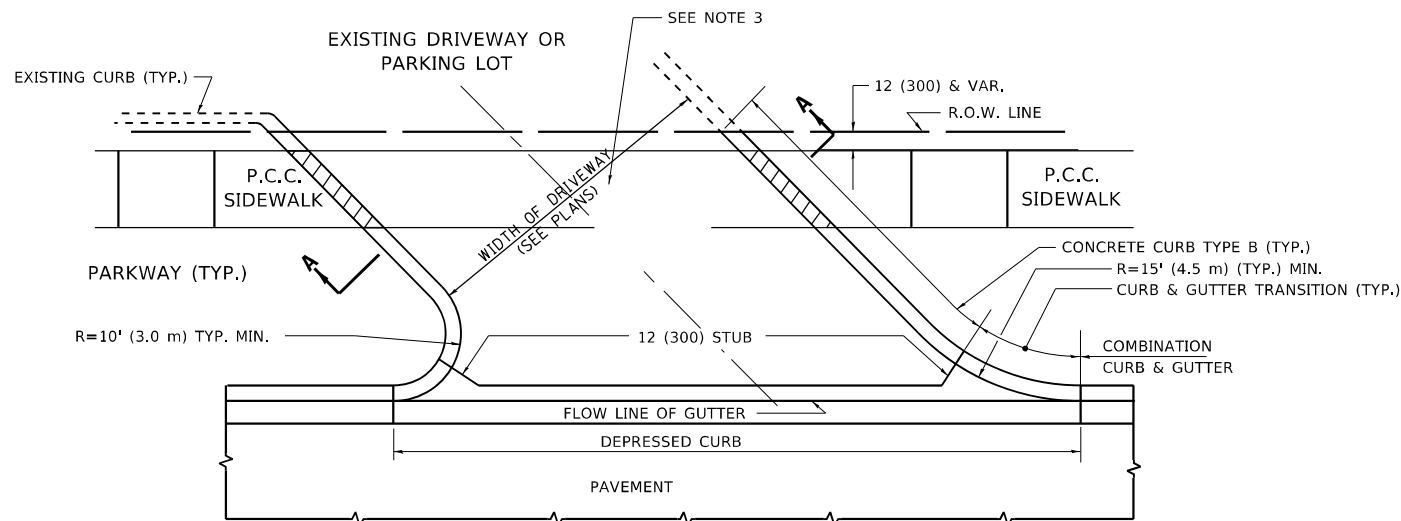
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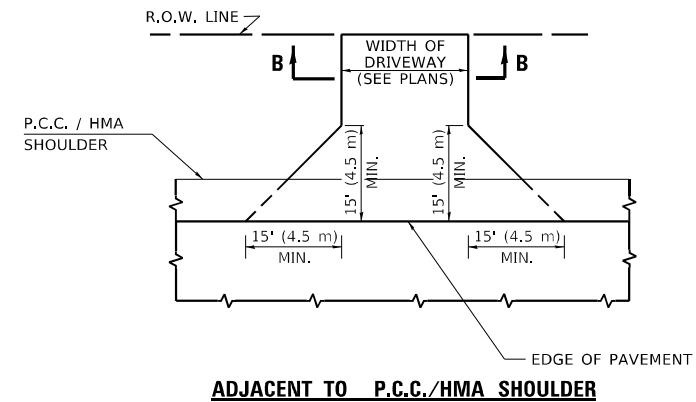
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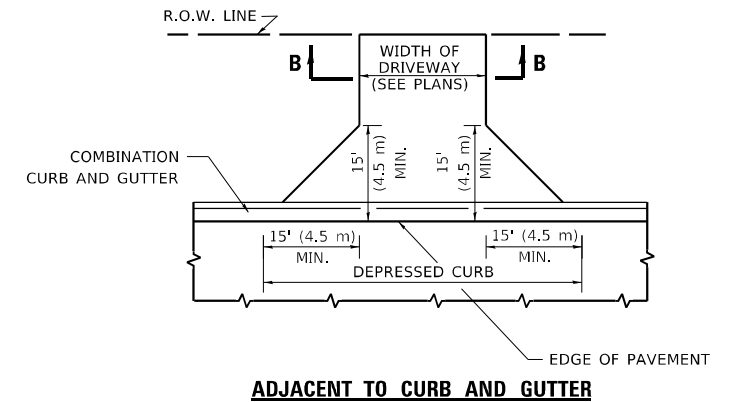
**WITH CONCRETE CURB, TYPE B**



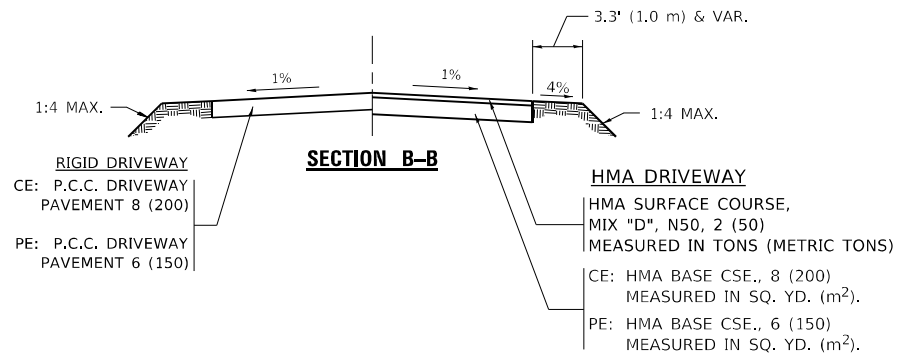
**WITH CONCRETE CURB, TYPE B**



**ADJACENT TO P.C.C./HMA SHOULDER**



**ADJACENT TO CURB AND GUTTER**



**GENERAL NOTES:**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

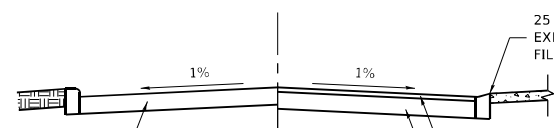
THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

**RIGID DRIVEWAY**  
 COMMERCIAL ENTRANCE (CE):  
 P.C.C. DRIVEWAY PAVEMENT 8 (200)  
 MEASURED IN SQ. YD. (m<sup>2</sup>)  
 NON-COMMERCIAL ENTRANCE (PE):  
 P.C.C. DRIVEWAY PAVEMENT 6 (150)  
 MEASURED IN SQ. YD. (m<sup>2</sup>)



**SECTION A-A**

**HMA DRIVEWAY**  
 HMA SURFACE COURSE,  
 MIX "D", N50, 2 (50)  
 MEASURED IN TONS (METRIC TONS)  
 CE: HMA BASE COURSE, 8 (200)  
 MEASURED IN SQ. YD. (m<sup>2</sup>).  
 PE: HMA BASE COURSE, 6 (150)  
 MEASURED IN SQ. YD. (m<sup>2</sup>).

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

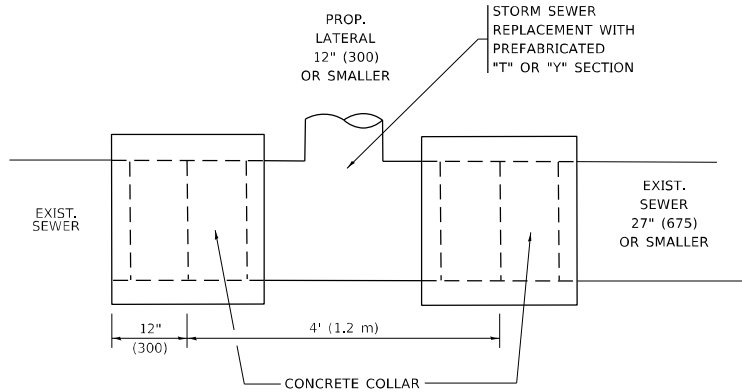
**DRIVEWAY DETAILS – DISTANCE BETWEEN R.O.W.  
 AND FACE OF CURB & EDGE OF SHOULDER ≥ 15'(4.5m)**  
 SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	53
BD400-01 (BD-01)			CONTRACT NO. 60T17	
ILLINOIS FED. AID PROJECT				

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		REVISED - R. BORO 09-06-11

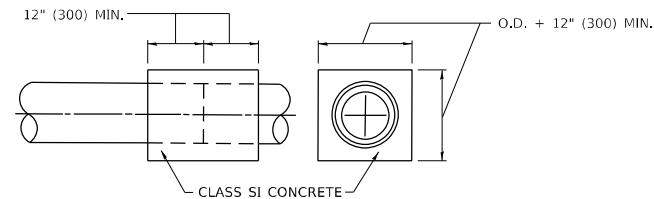
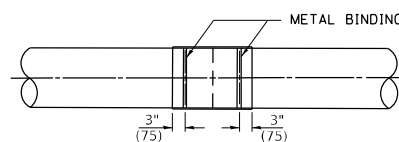
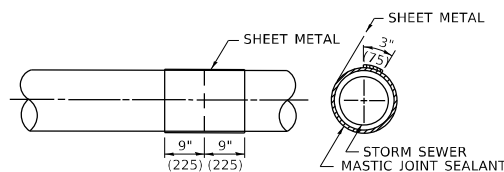
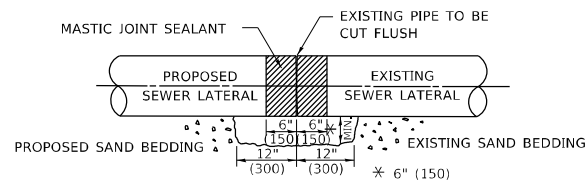
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**DETAIL "A"**

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER



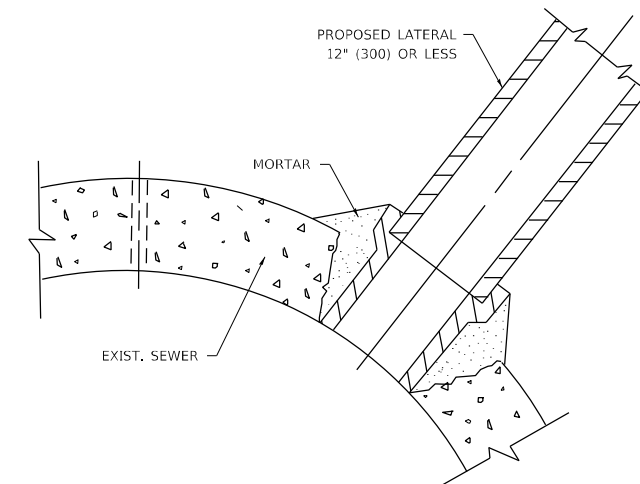
**DETAIL "B"**

CLASS SI CONCRETE COLLAR

**CONSTRUCTION SEQUENCE**

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT, BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12' x 6' (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
5. WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
6. LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
8. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
9. PLACE CLASS SI CONCRETE AROUND THE JOINT.

\* ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**DETAIL "C"**

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

**NOTES:**

**MATERIAL**

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

**CONSTRUCTION METHODS**

- I. THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- II. CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
  - A) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
  - B) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

**GENERAL**

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

**BASIS OF PAYMENT**

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

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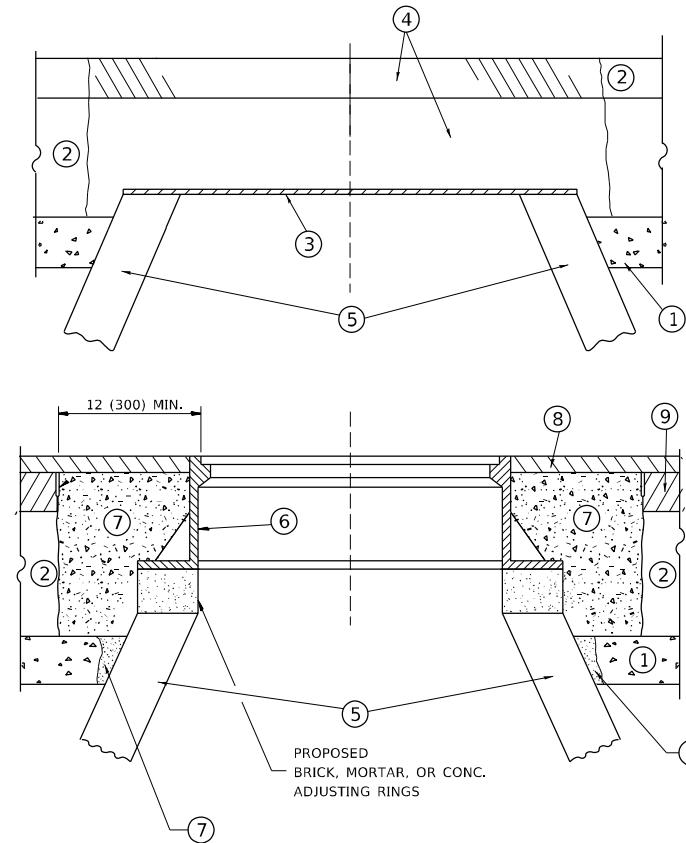
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	DRAWN -	REVISED - R. SHAH 09-09-94
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PLOT DATE = 1/25/2021	DATE - 07-25-90	REVISED - R. SHAH 06-12-96

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETAIL OF STORM SEWER  
CONNECTION TO EXISTING SEWER**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	55
BD500-01 (BD-7)			CONTRACT NO. 60T17	
ILLINOIS FED. AID PROJECT				



**NOTES**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1 \* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT \*THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER.\*

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ⑥ FRAME AND LID (SEE NOTES)
- ② EXISTING PAVEMENT
- ⑦ CLASS PP-1 \*CONCRETE
- ③ 36 (900) DIAMETER METAL PLATE
- ⑧ PROPOSED HMA SURFACE COURSE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑨ PROPOSED HMA BINDER COURSE
- ⑤ EXISTING STRUCTURE

**LOCATION OF STRUCTURES**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR \*FRAMES AND LIDS TO BE ADJUSTED (SPECIAL).\*

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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PLOT DATE = 1/25/2021	DATE - 10-25-94	REVISED - R. BORO 12-06-11

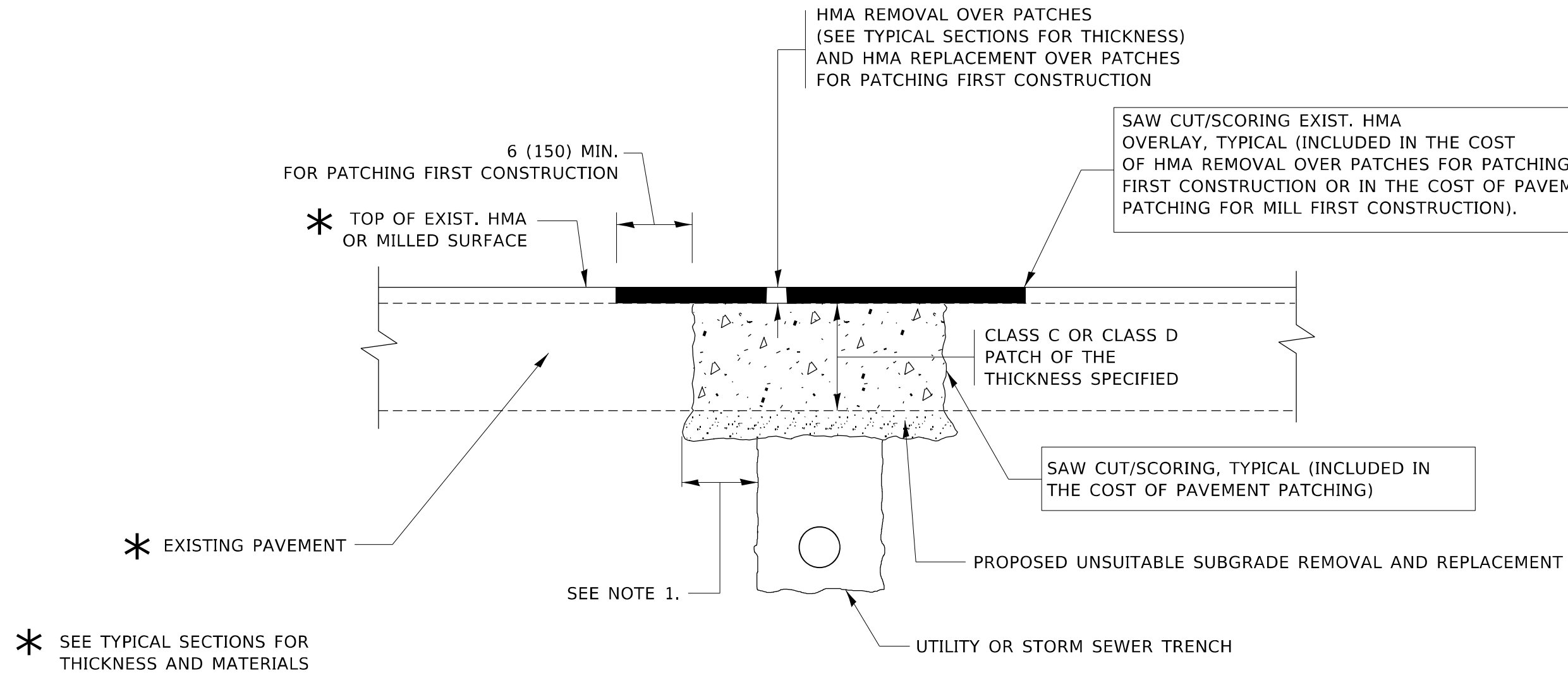
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR  
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	56
BD600-03 (BD-8)			CONTRACT NO. 60T17	
ILLINOIS FED. AID PROJECT				





**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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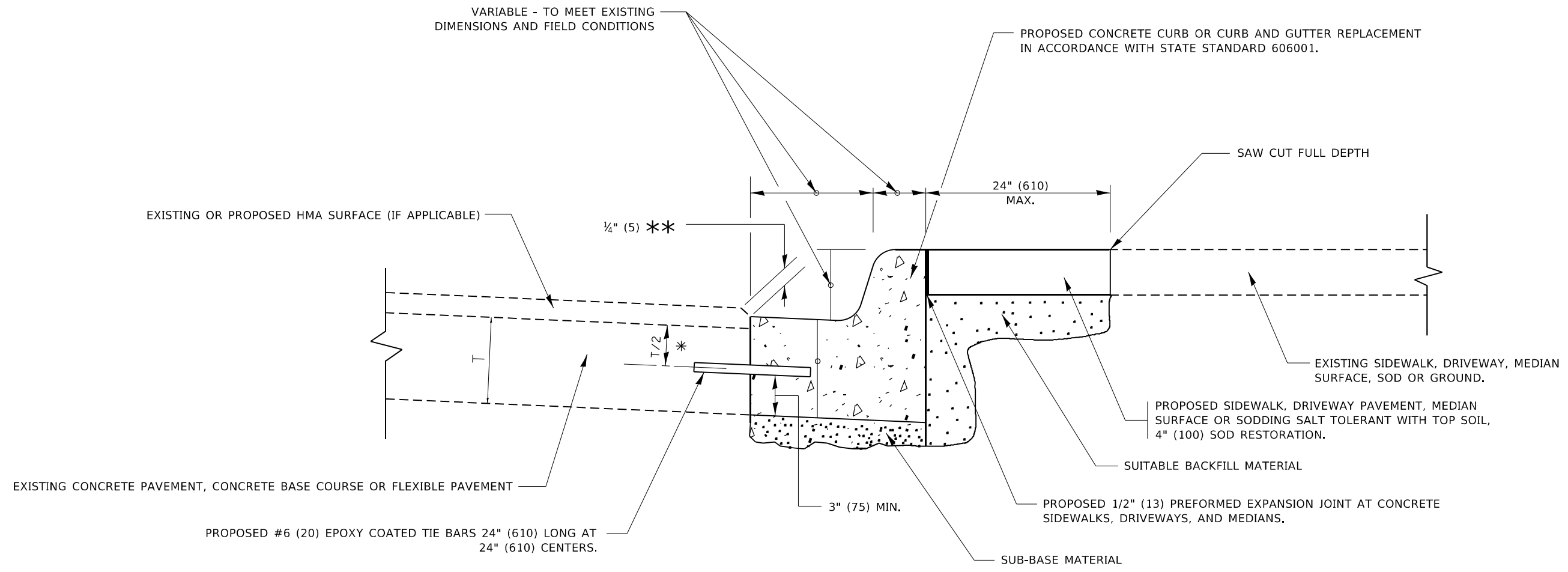
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PLOT DATE = 1/25/2021	DATE - 10-25-94	REVISED - K. ENG 10-27-08

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR  
HMA SURFACED PAVEMENT

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	57
BD400-04 (BD-22)			CONTRACT NO. 60T17	
ILLINOIS FED. AID PROJECT				



- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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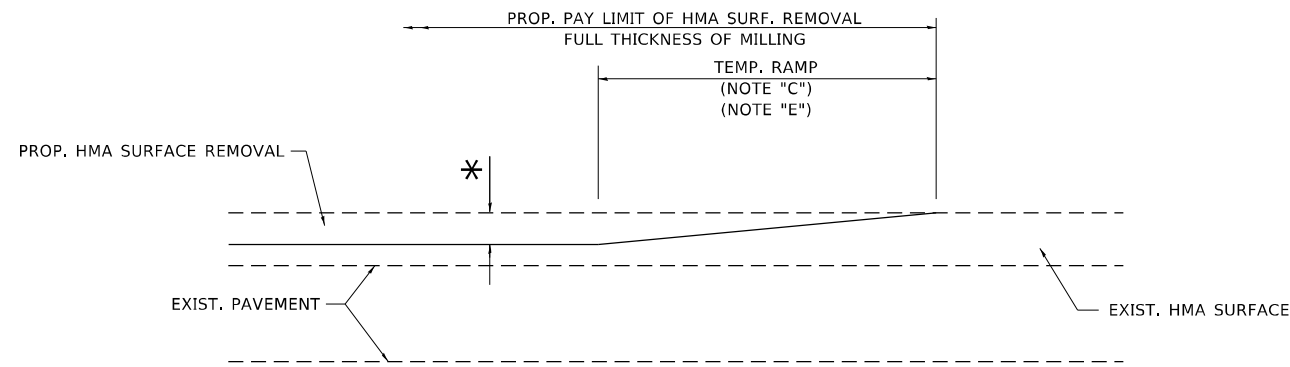
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PLOT DATE = 1/25/2021	DATE - 03-11-94	REVISED - K. SMITH 07-11-19

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CURB OR CURB AND GUTTER  
REMOVAL AND REPLACEMENT**

SCALE: NONE    SHEET 1 OF 1 SHEETS    STA.    TO STA.

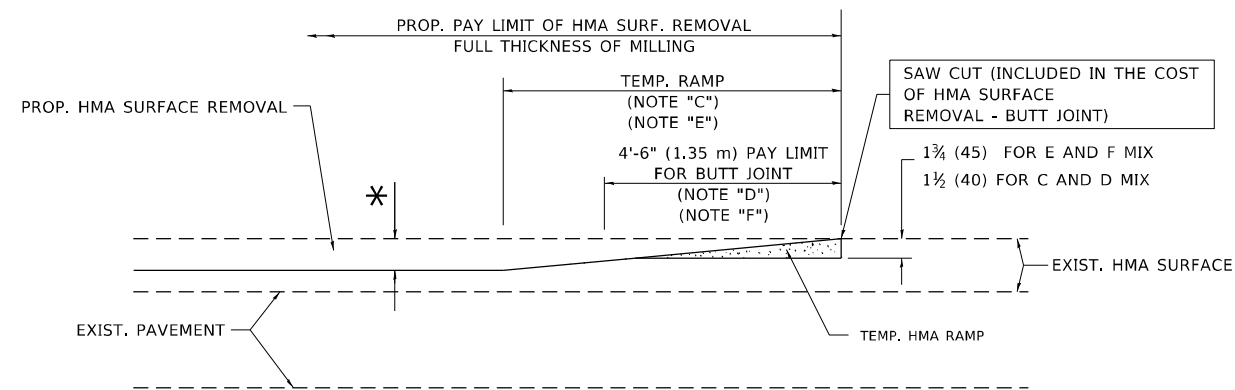
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2710	52-N	COOK	78	58
<b>BD600-06 (BD-24)</b>			CONTRACT NO. 60T17	
ILLINOIS FED. AID PROJECT				



**MILLED TEMPORARY RAMP**

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 1**

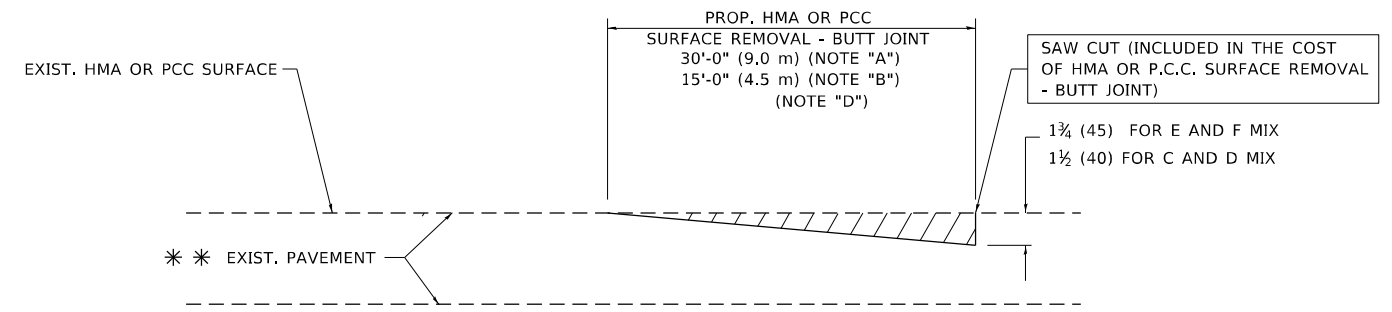


**HMA CONSTRUCTED TEMPORARY RAMP**

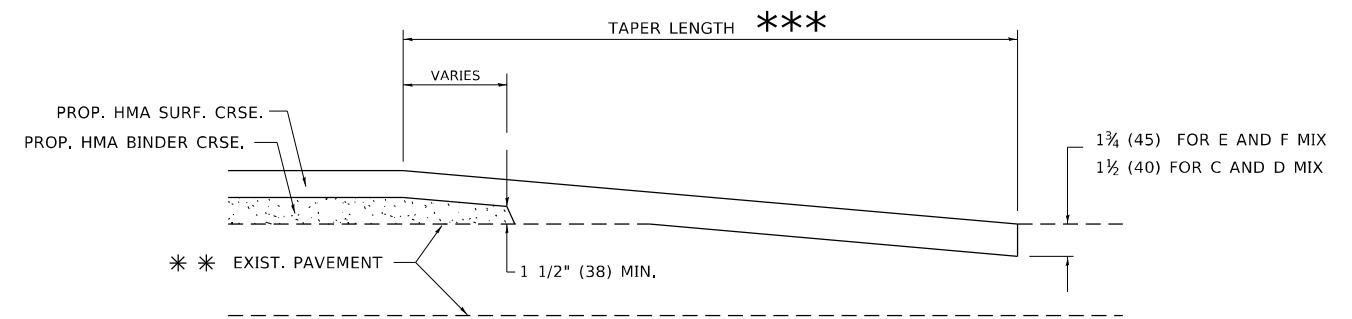
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

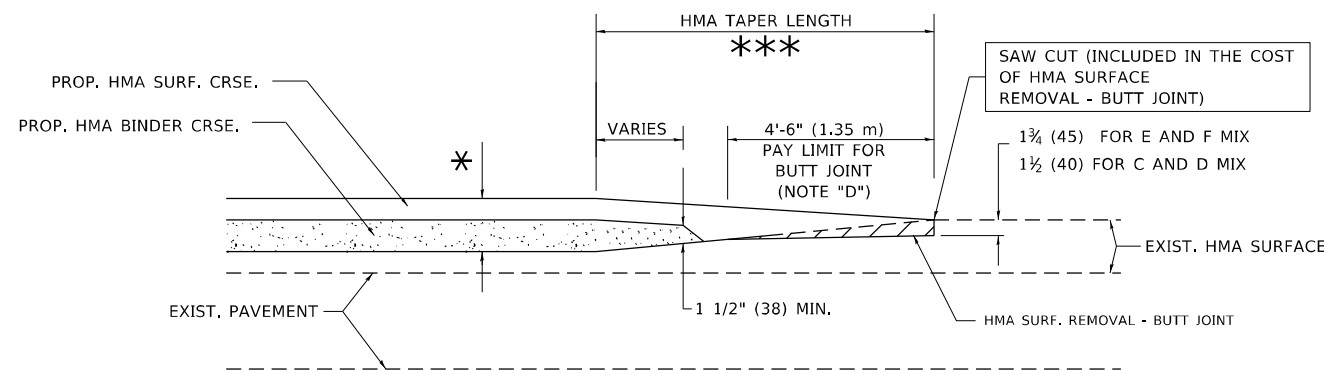
**NOTES**

- A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F. INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.  
\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- G. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".  
\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**BUTT JOINT AND HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

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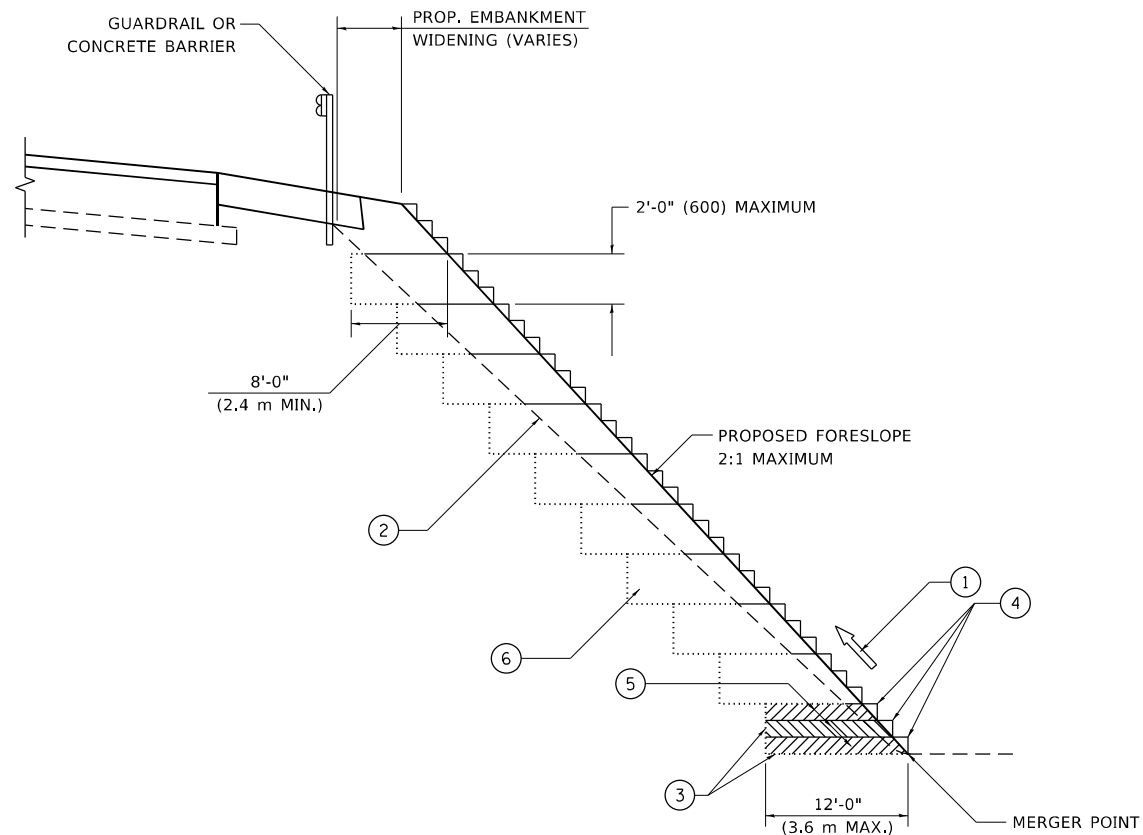
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PLOT DATE = 1/25/2021	DATE - 06-13-90	REVISED - R.BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND HMA TAPER DETAILS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	59
<b>BD400-05 BD32</b>			CONTRACT NO. 60T17	
ILLINOIS FED. AID PROJECT				



**TYPICAL BENCHING DETAIL  
FOR EMBANKMENT**

**NOTES:**

- ① CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- ② EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
- ③ BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- ④ TRIM TO FINAL SLOPE.
- ⑤ EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- ⑥ EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ⑦ SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)  
UNLESS OTHERWISE SHOWN.

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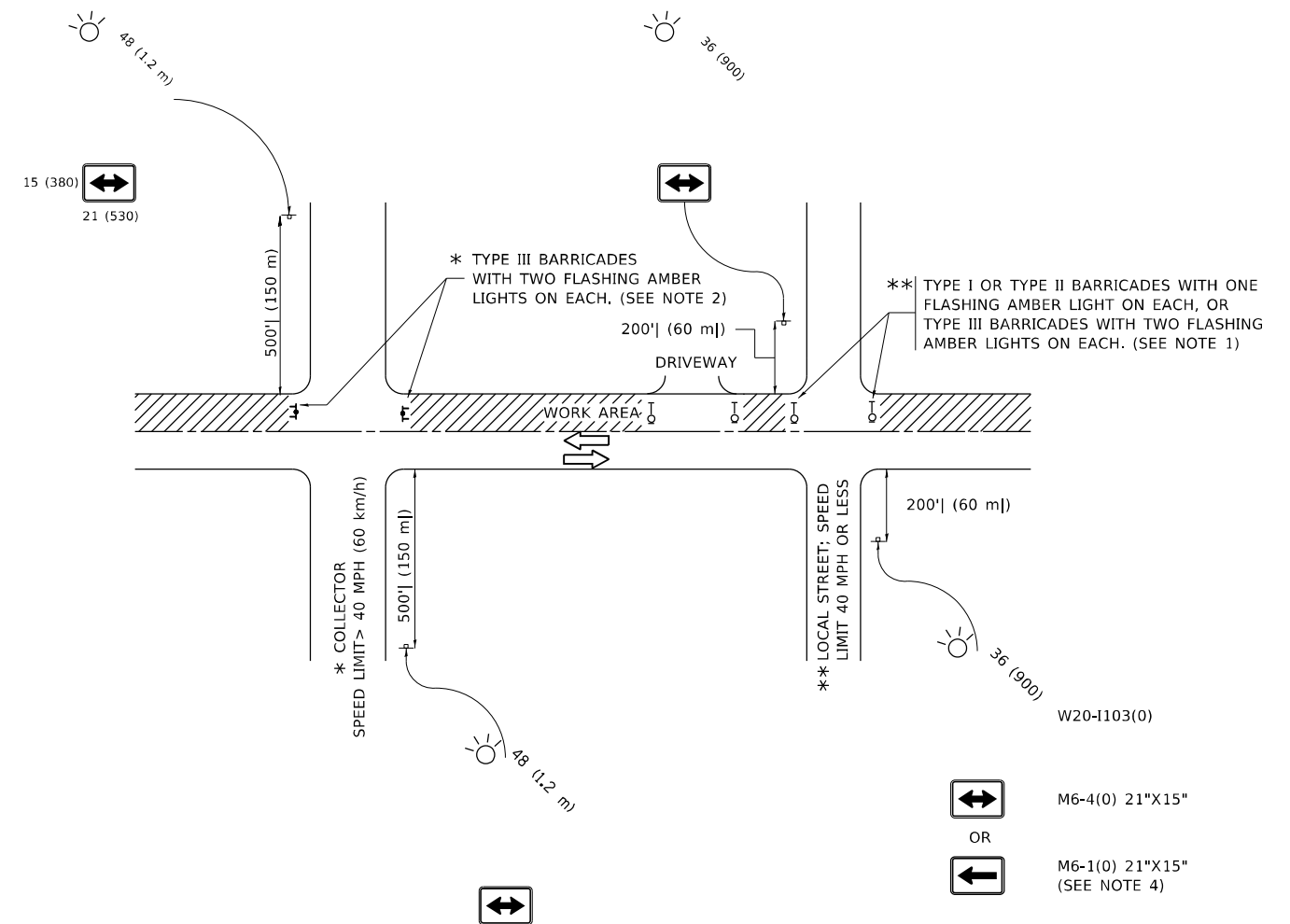
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	DATE - 06-16-04	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BENCHING DETAIL  
FOR EMBANKMENT WIDENING**

SCALE: NONE    SHEET 1 OF 1 SHEETS    STA.    TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	60
<b>BD-51</b>			CONTRACT NO. 60T17	
ILLINOIS FED. AID PROJECT				



**NOTES:**

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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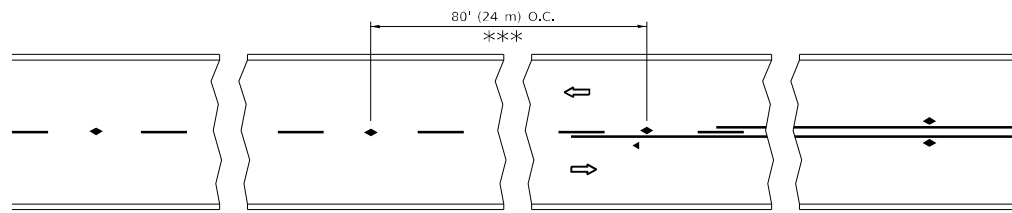
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PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
PLOT DATE = 1/25/2021	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

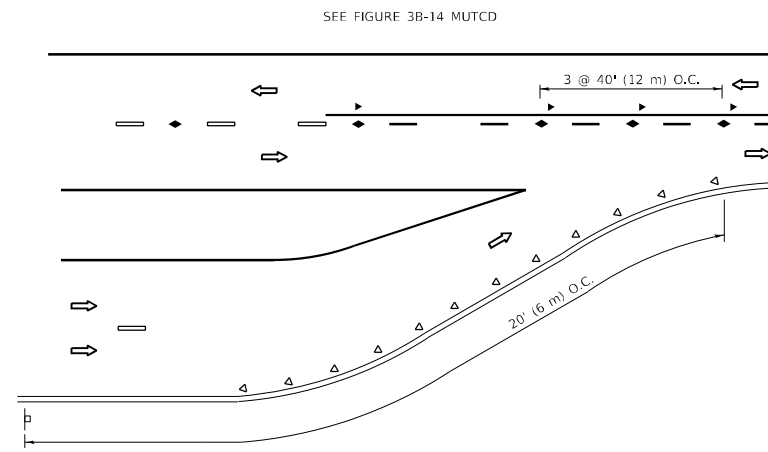
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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<b>TC-10</b>			CONTRACT NO. 60T17	
ILLINOIS FED. AID PROJECT				

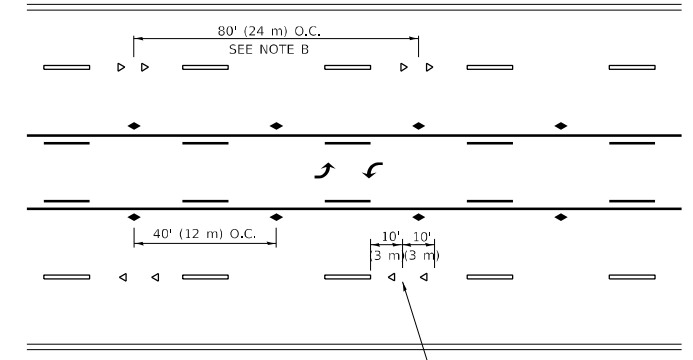


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

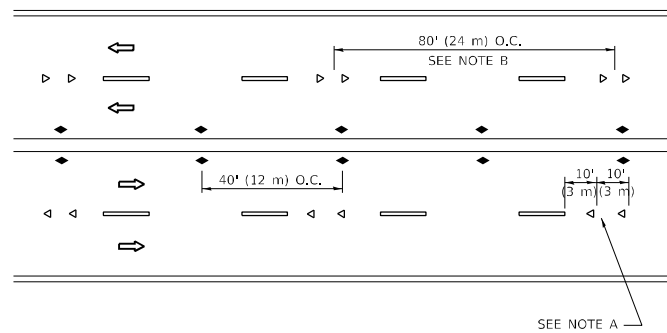
**TWO-LANE/TWO-WAY**



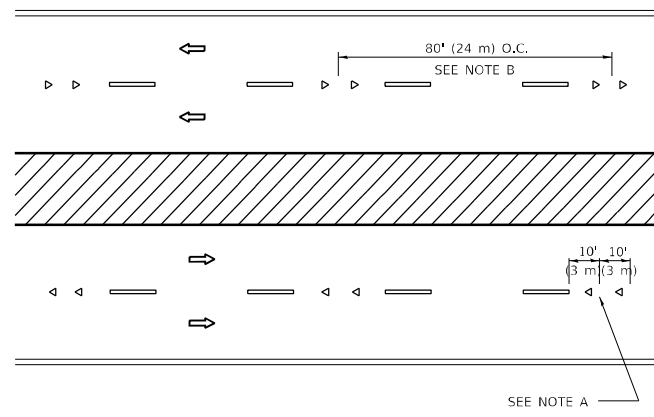
**LANE REDUCTION TRANSITION**



**TWO-WAY LEFT TURN**



**MULTI-LANE/UNDIVIDED**



**MULTI-LANE/DIVIDED**

**GENERAL NOTES**

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

**SYMBOLS**

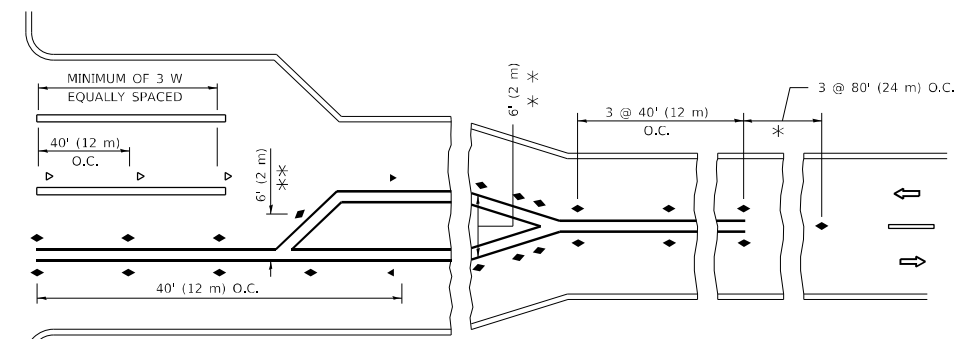
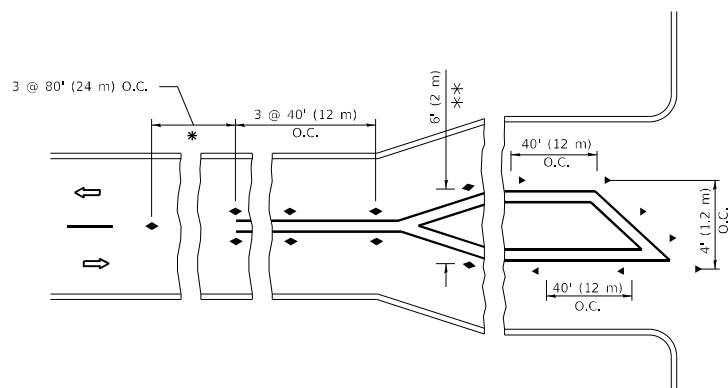
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

**LANE MARKER NOTES**

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

**DESIGN NOTES**

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



**TURN LANES**

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

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PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED - T. RAMMACHER 01-06-00
PLOT DATE = 1/25/2021	CHECKED -	REVISED - C. JUCIUS 09-09-09
	DATE -	REVISED - C. JUCIUS 07-01-13

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

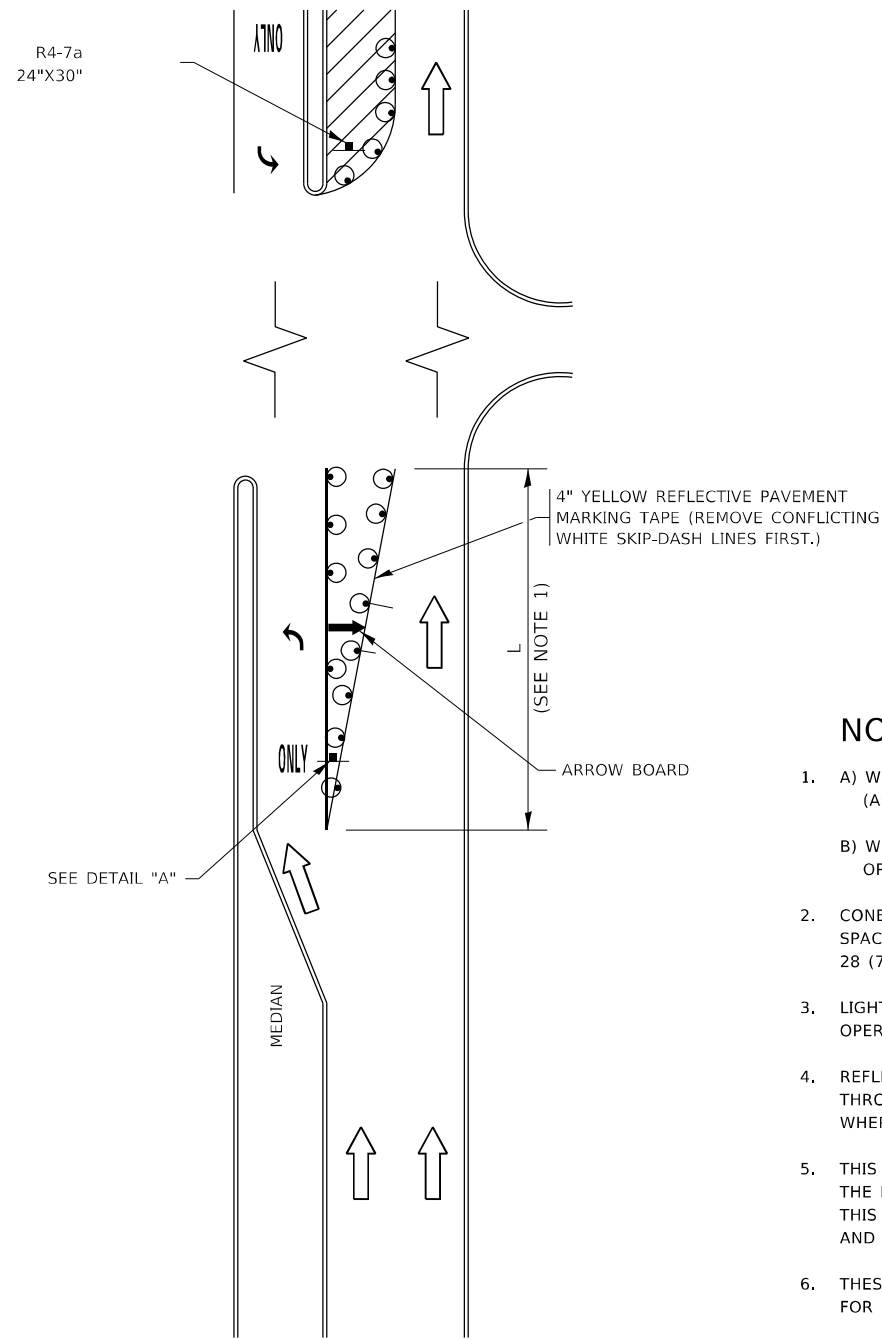
**TYPICAL APPLICATIONS  
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

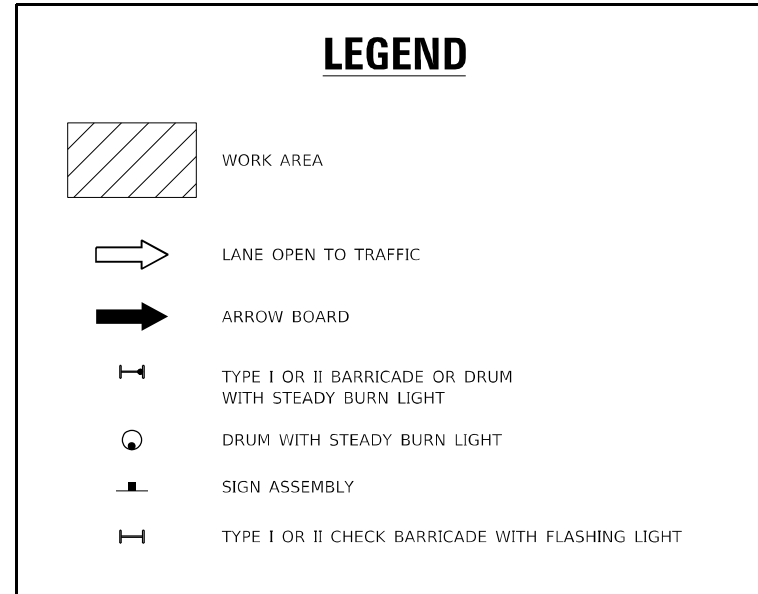
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2710	52-N	COOK	78	62
TC-11			CONTRACT NO. 60T17	
ILLINOIS FED. AID PROJECT				



# TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER



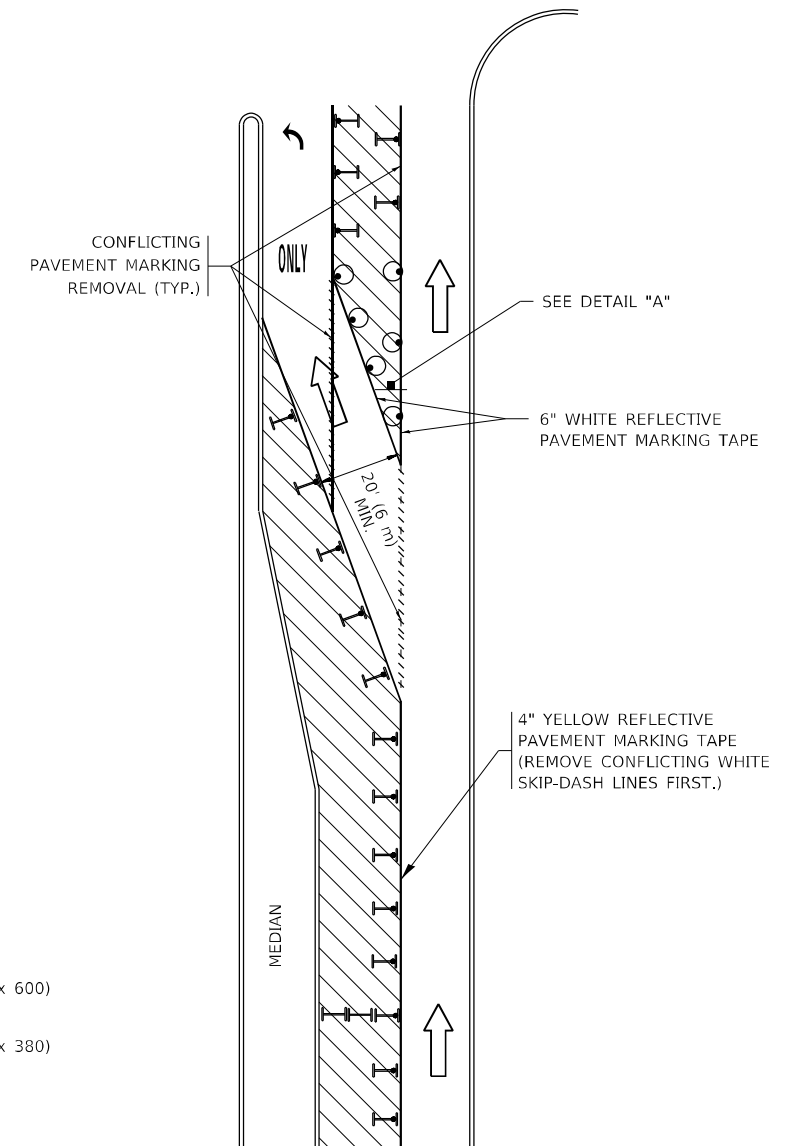
**FIGURE 1**



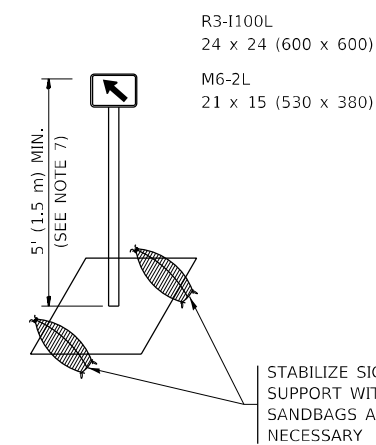
### NOTES:

1. A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

# TURN BAY ENTRANCE WITHIN A LANE CLOSURE



**FIGURE 2**



**DETAIL A**

All dimensions are in inches (millimeters) unless otherwise shown.

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	DRAWN - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13
PLOT SCALE = 100,0000' / in.	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16
PLOT DATE = 1/25/2021	DATE - T. RAMMACHER 01-06-00	REVISED -

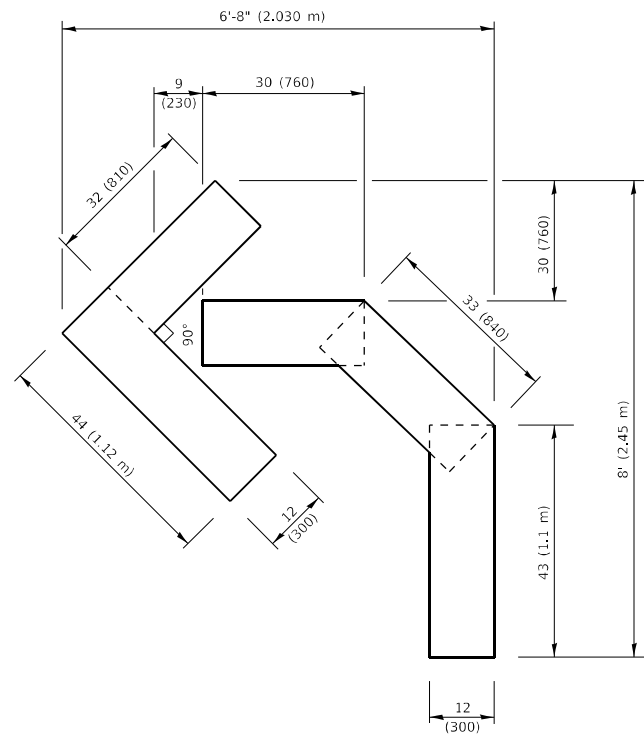
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS  
(TO REMAIN OPEN TO TRAFFIC)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

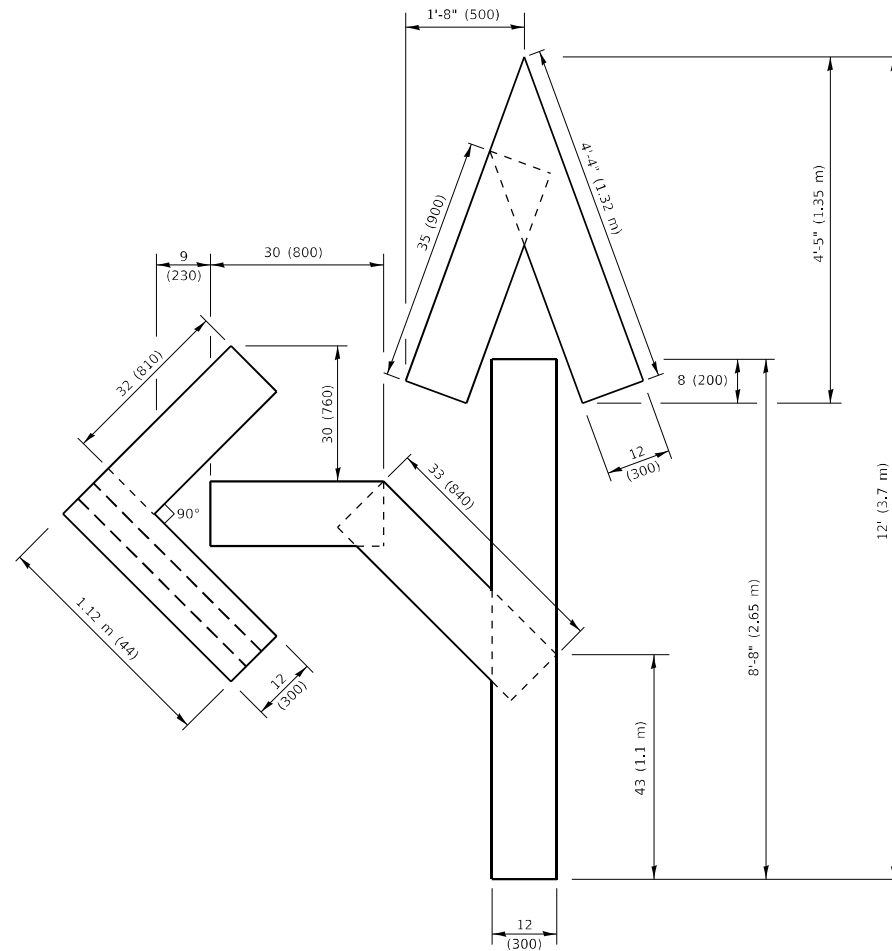
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TC-14		CONTRACT NO. 60T17		
ILLINOIS FED. AID PROJECT				





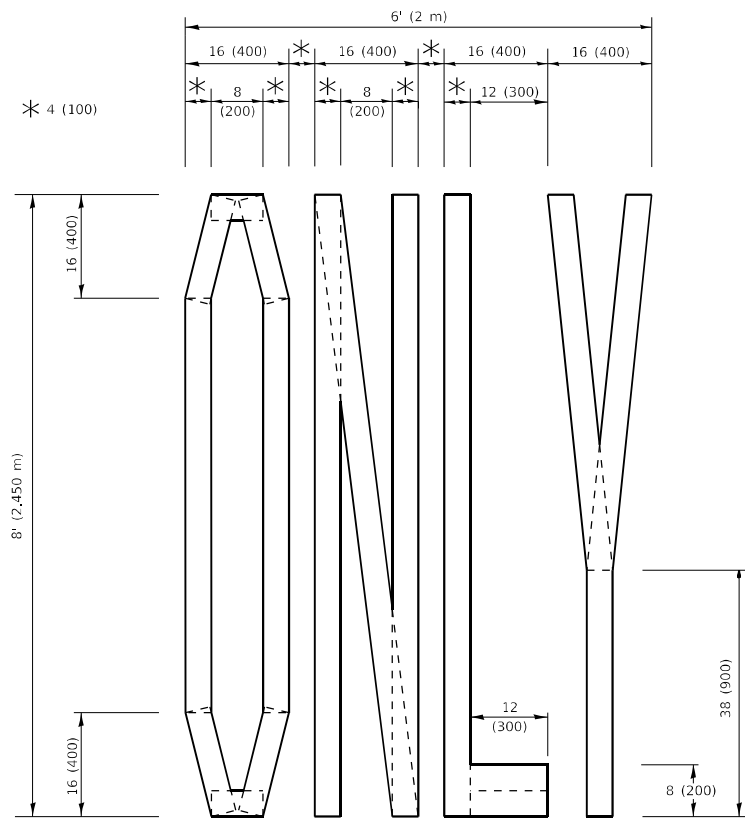
**QUANTITY**

4 (100) LINE = 45.5 ft. (13.9 m)  
15.2 sq. ft. (1.41 sq. m)



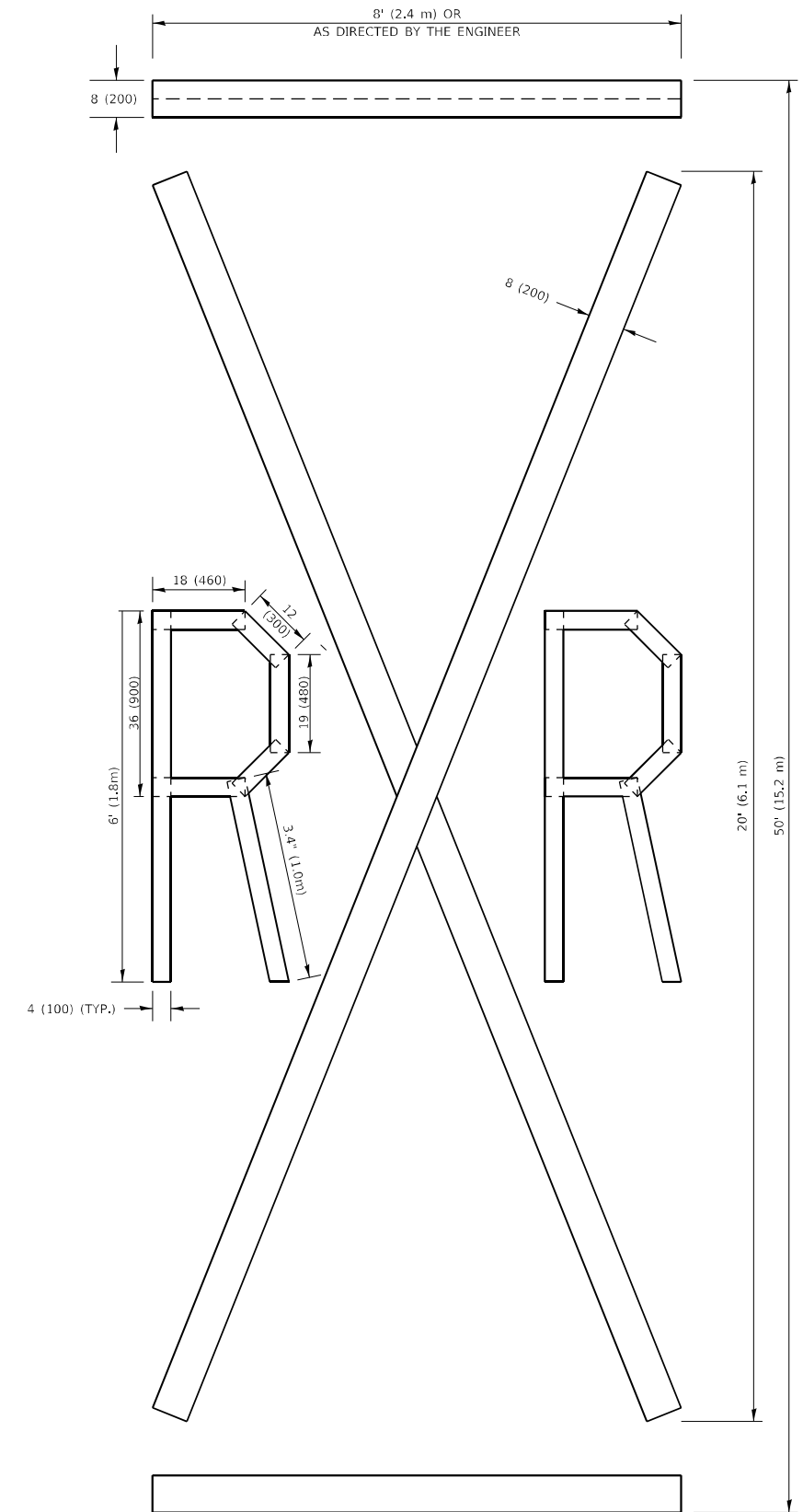
**QUANTITY**

4 (100) LINE = 82.5 ft. (25.1 m)  
27.5 sq. ft. (2.53 sq. m)



**QUANTITY**

4 (100) LINE = 64.1 ft. (19.5 m)  
21.4 sq. ft. (1.99 sq. m)



**QUANTITY**

4 (100) LINE = 225.9 ft. (68.9 m)  
75.3 sq. ft. (6.99 sq. m)

**NOTE:**

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.

All dimensions are in inches (millimeters) unless otherwise shown.

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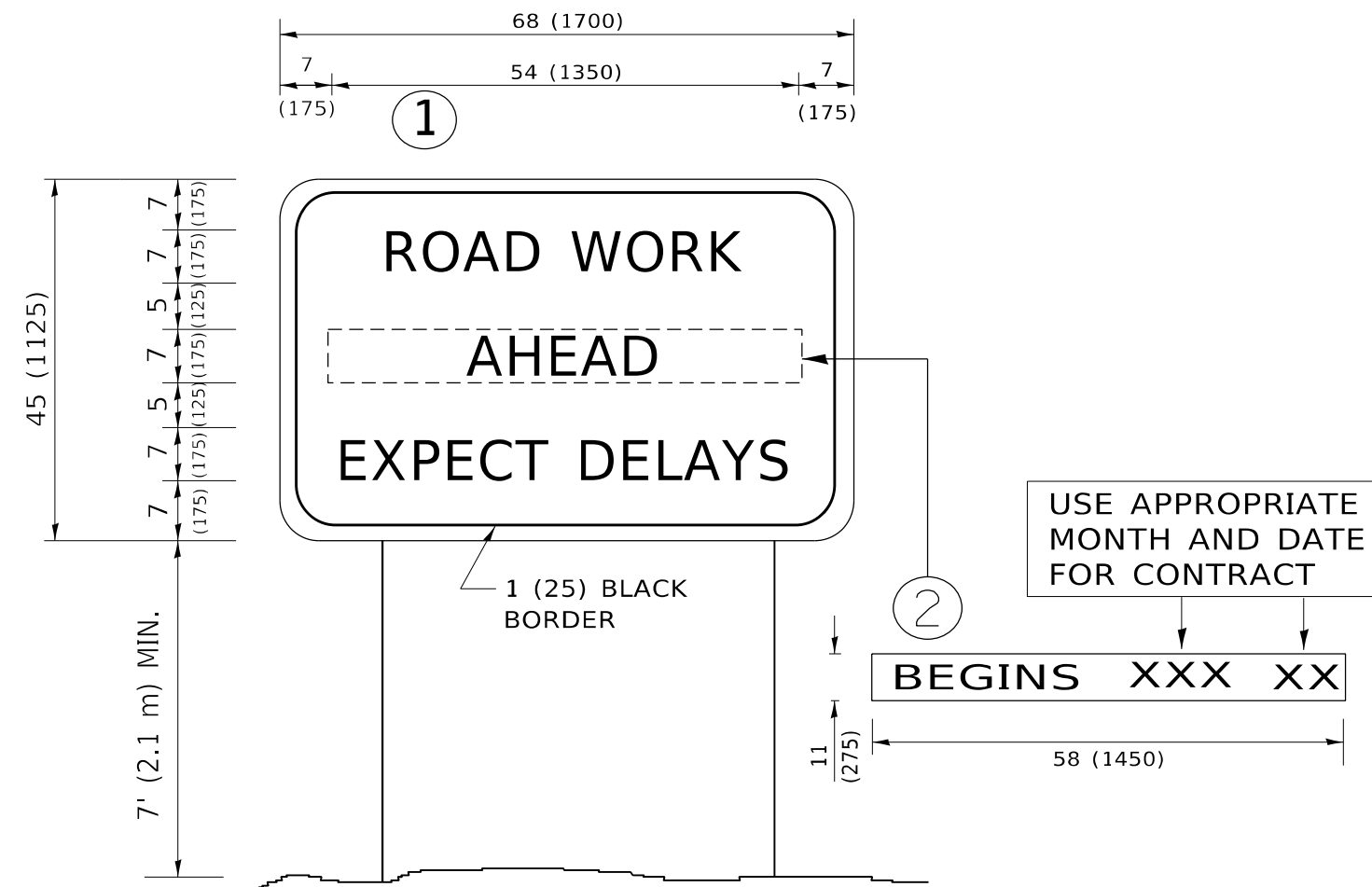
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PLOT DATE = 1/25/2021	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	65
<b>TC-16</b>			CONTRACT NO. 60T17	
ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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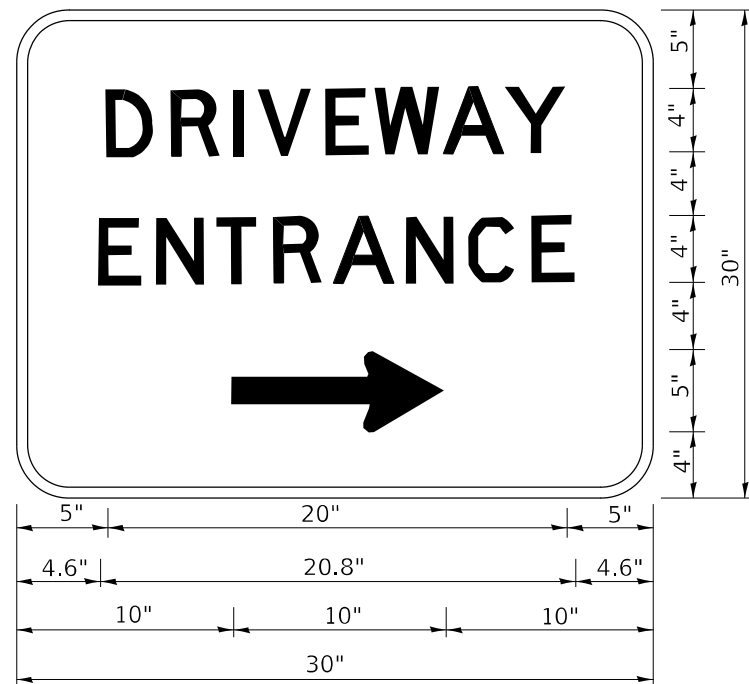
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PLOT DATE = 1/25/2021	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	66
<b>TC-22</b>			CONTRACT NO. 60T17	
ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED  
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

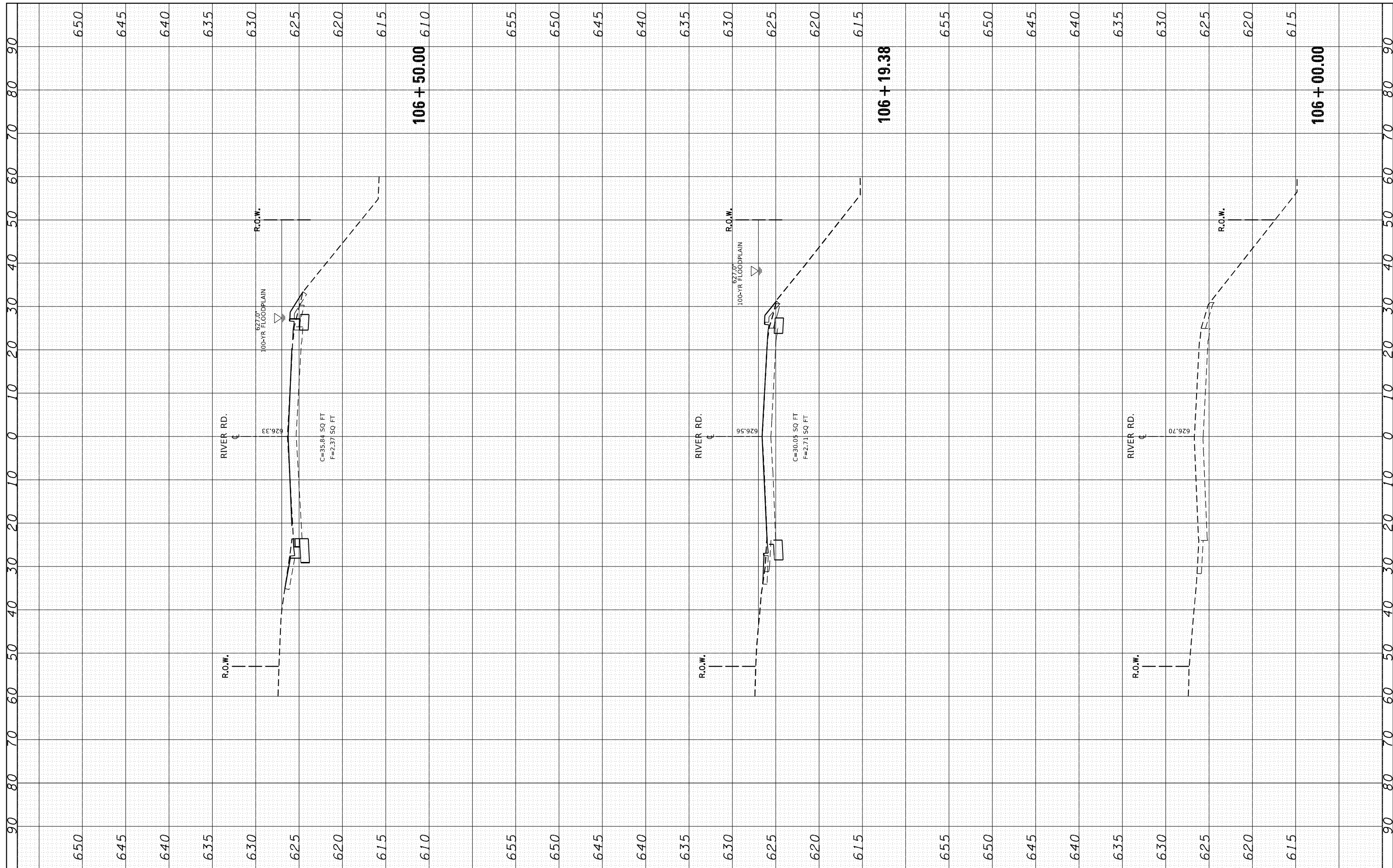
**DRIVEWAY ENTRANCE SIGNING**

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ILLINOIS FED. AID PROJECT				

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AREAS CHECKED	TEMPLATE	
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ORIGINAL SURVEY NO.	SURVEYED	DATE
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 PLOT DATE = 1/28/2021

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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

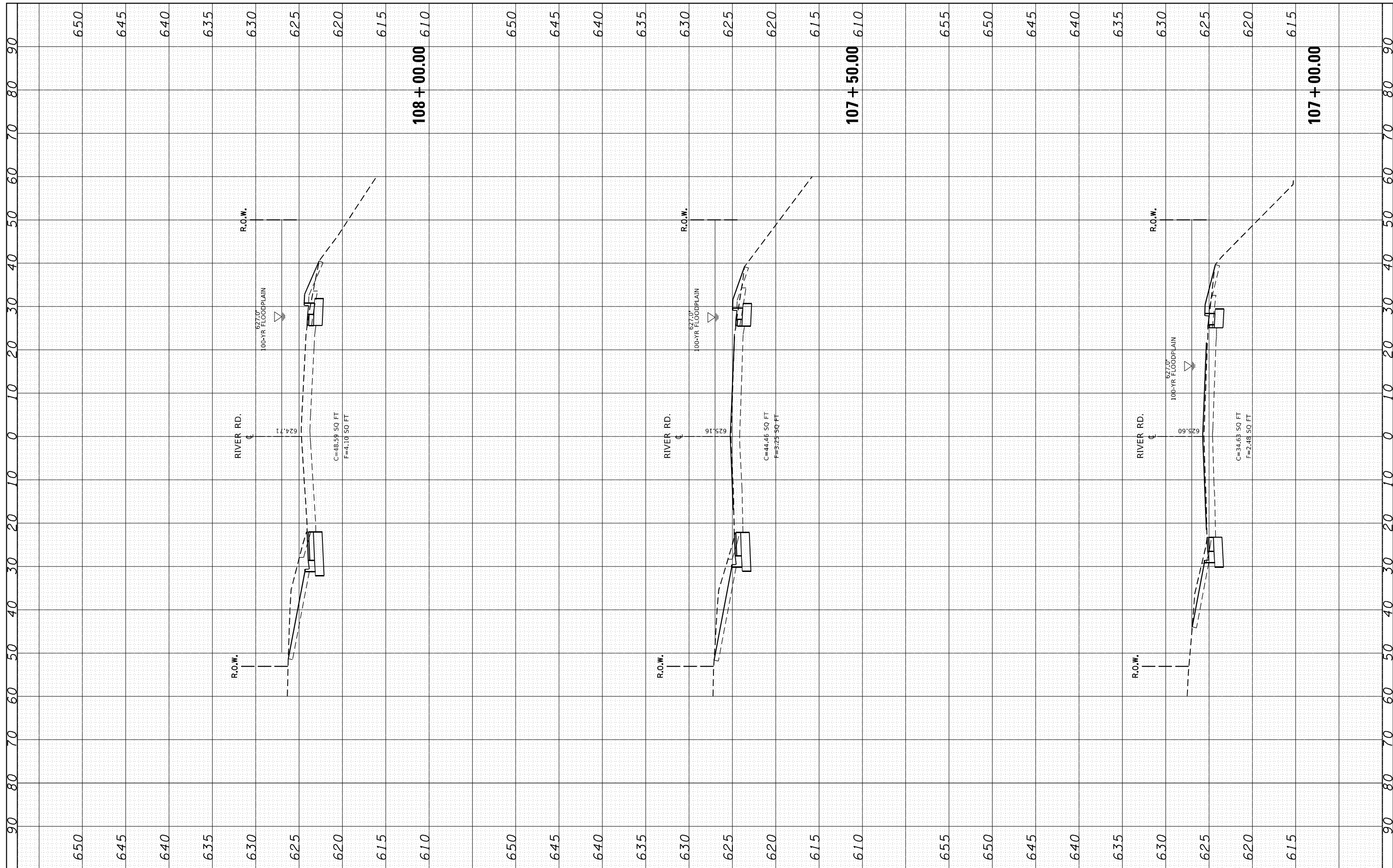
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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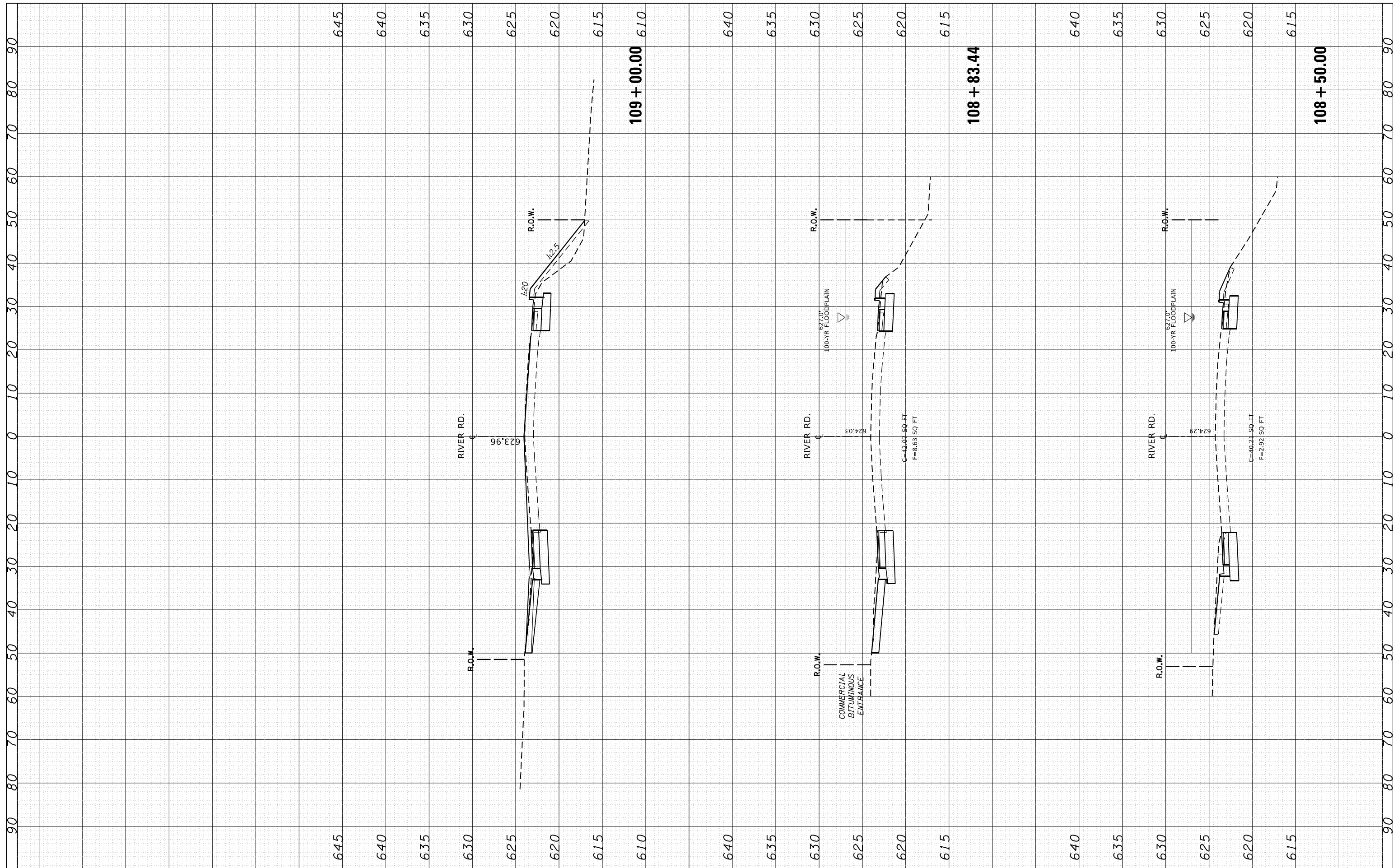
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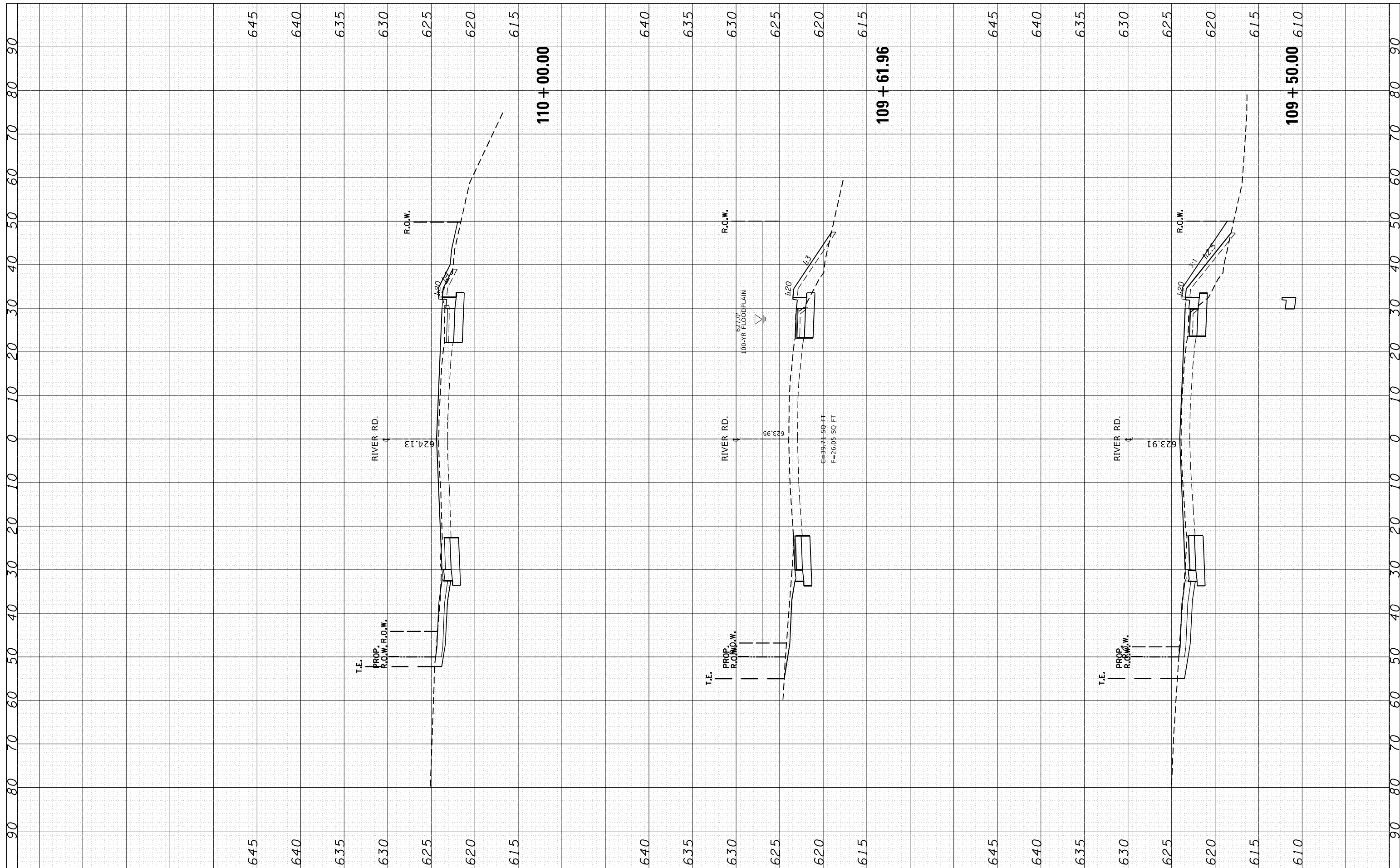
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											ILLINOIS FED. AID PROJECT		

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 DATE -

REVISED -  
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS  
 DES PLAINES RIVER RD. AT ROBINSON RD.**

SCALE: SHEET OF SHEETS STA. 109+50.00 TO STA. 110+00.00

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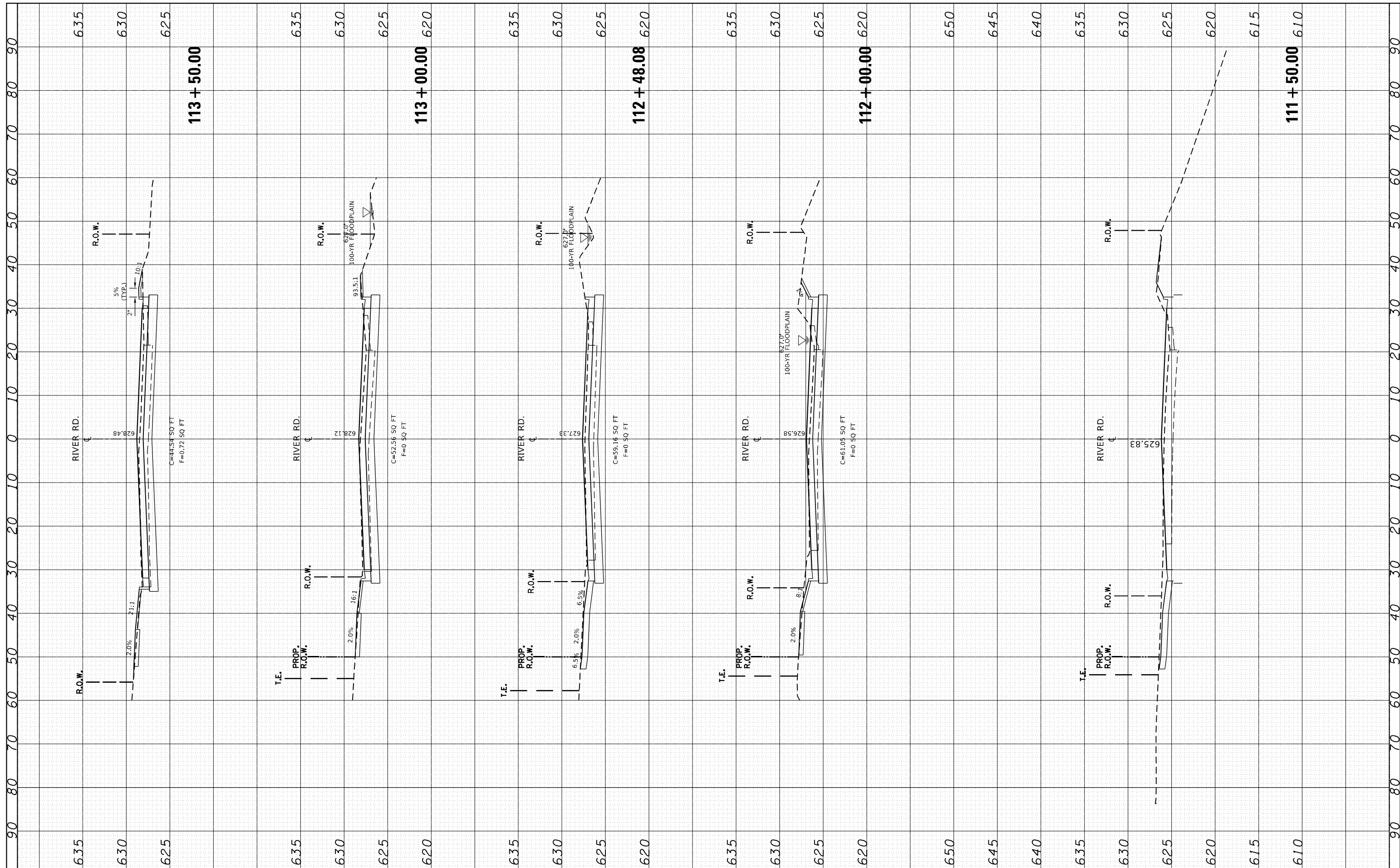
ILLINOIS FED. AID PROJECT





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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS  
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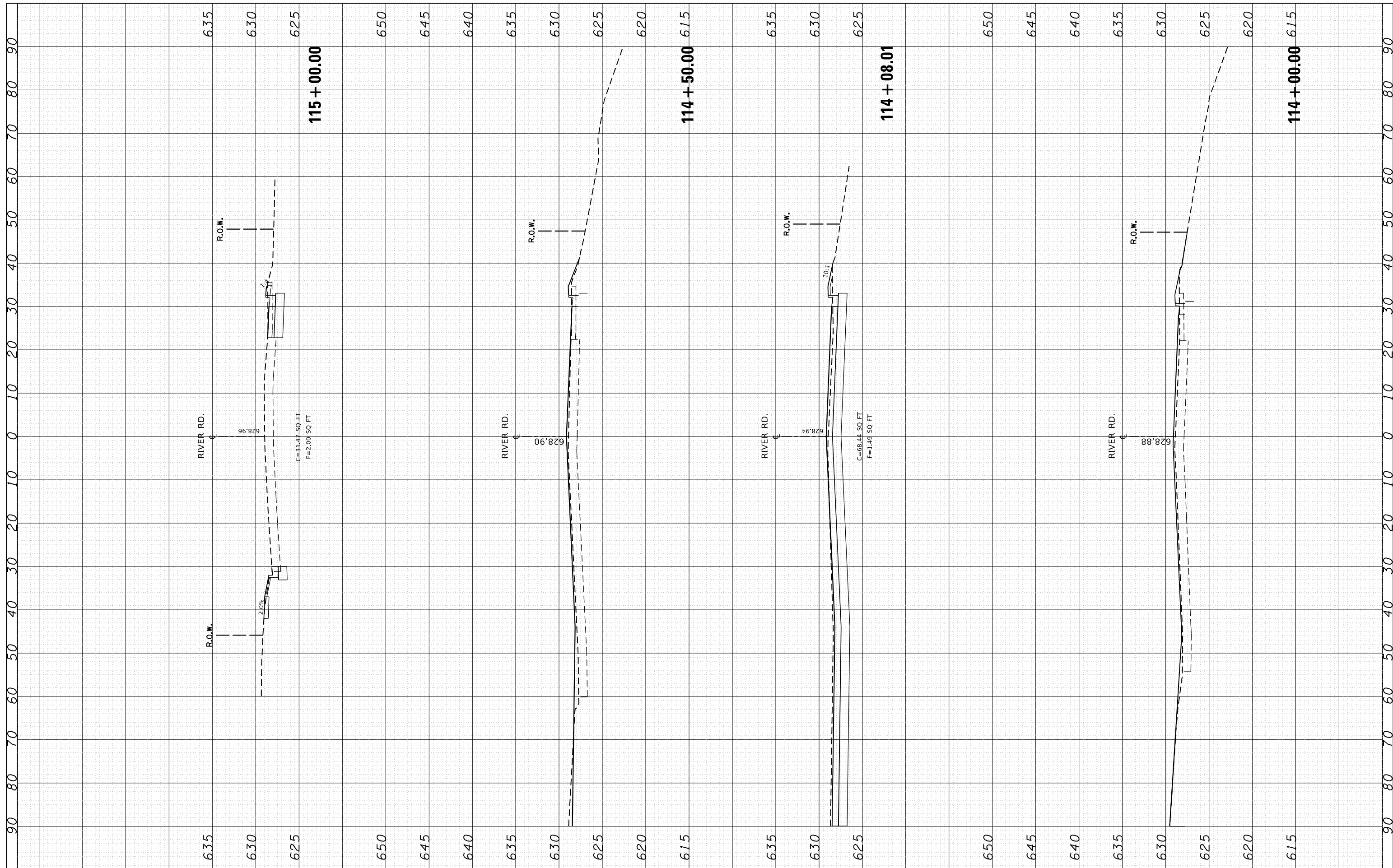
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CONTRACT NO. 60T17				

ILLINOIS FED. AID PROJECT

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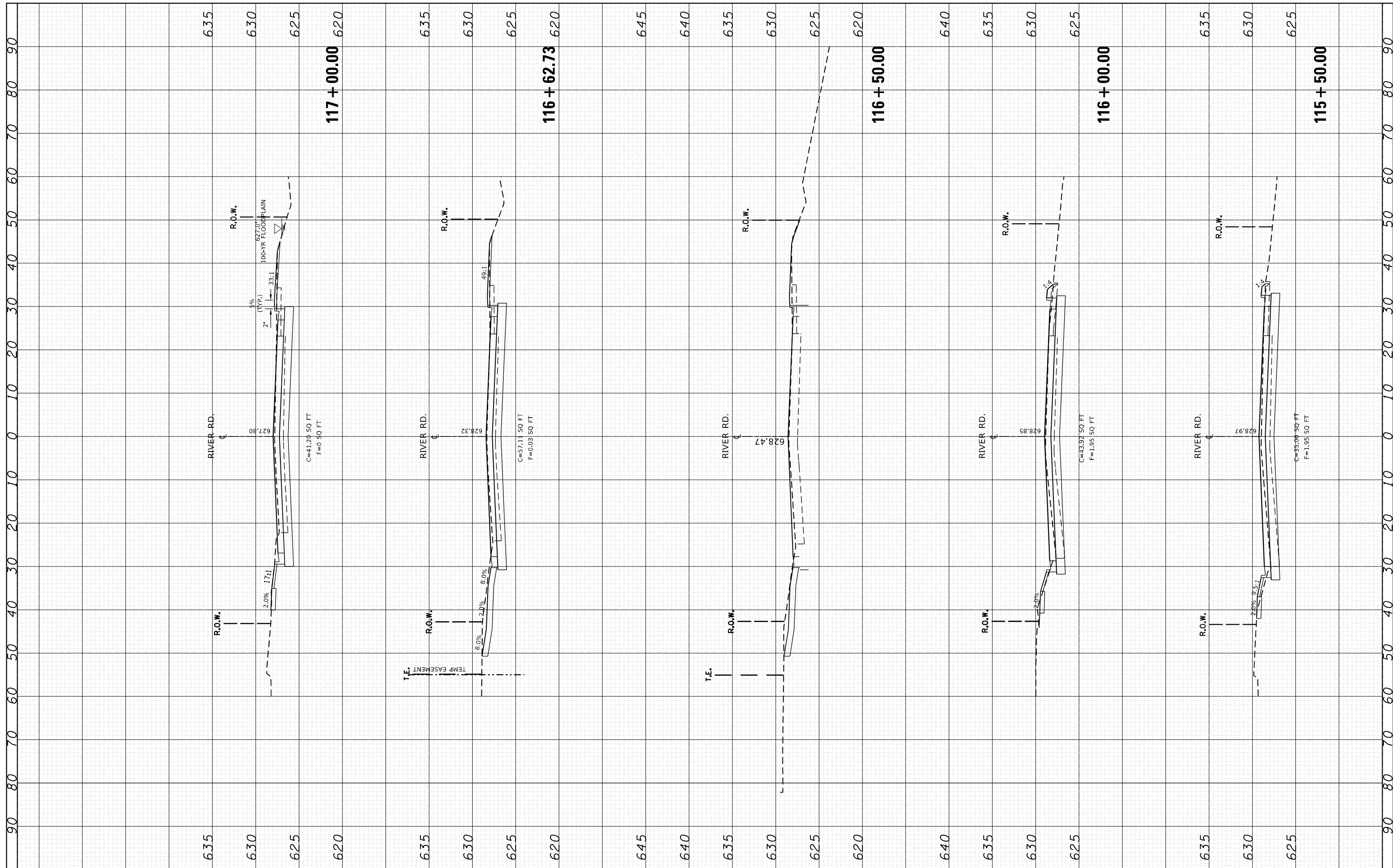
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	AREAS	
	CHECKED	



FILE NAME =	USER NAME = qarutnm	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CROSS SECTIONS DES PLAINES RIVER RD. AT ROBINSON RD.</b>				F.A.U. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
p:\planroom.dot\illinois.gov\PI72009\CA000\PI72009-sht-xssht-River.dgn		CHECKED -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA. 114+00.00	TO STA. 115+00.00	2710	52-N	COOK	78
Default	PLOT SCALE = 20.0000' / in.	DATE -	REVISED -											CONTRACT NO. 60717
	PLOT DATE = 1/28/2021													ILLINOIS FED. AID PROJECT

FINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

ORIGINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE



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 USER NAME = qarutnm  
 PLOT SCALE = 20.0000' / in.  
 PLOT DATE = 1/28/2021

DESIGNED -	REVISOR -
CHECKED -	REVISOR -
DATE -	REVISOR -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

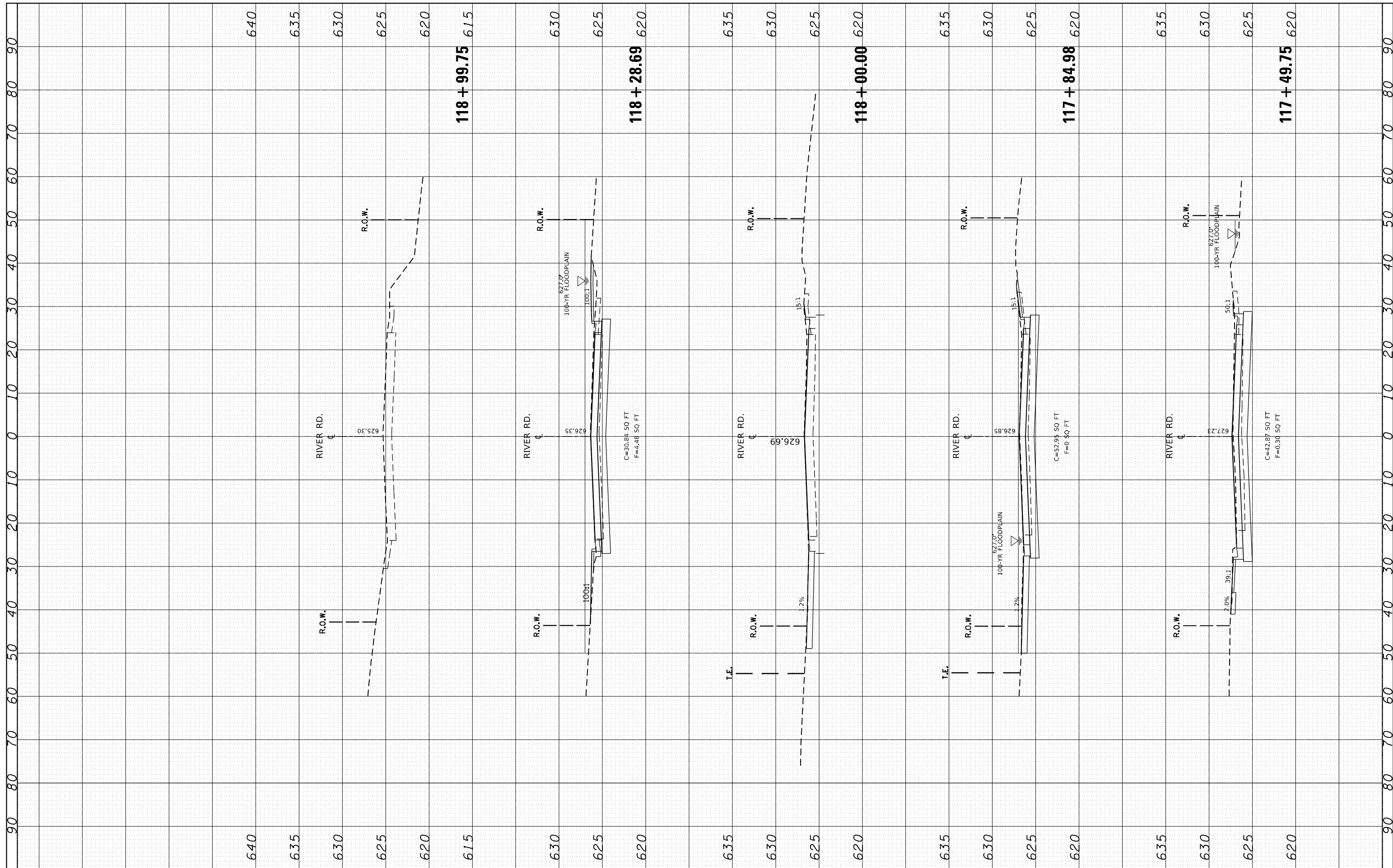
**CROSS SECTIONS  
DES PLAINES RIVER RD. AT ROBINSON RD.**

SCALE: SHEET OF SHEETS STA. 115+50.00 TO STA. 117+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	75
CONTRACT NO. 60T17				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY NO.	SURVEYED PLOTTED AREAS CHECKED	BY	DATE
NOTE BOOK NO.	TEMPLATE AREAS CHECKED		

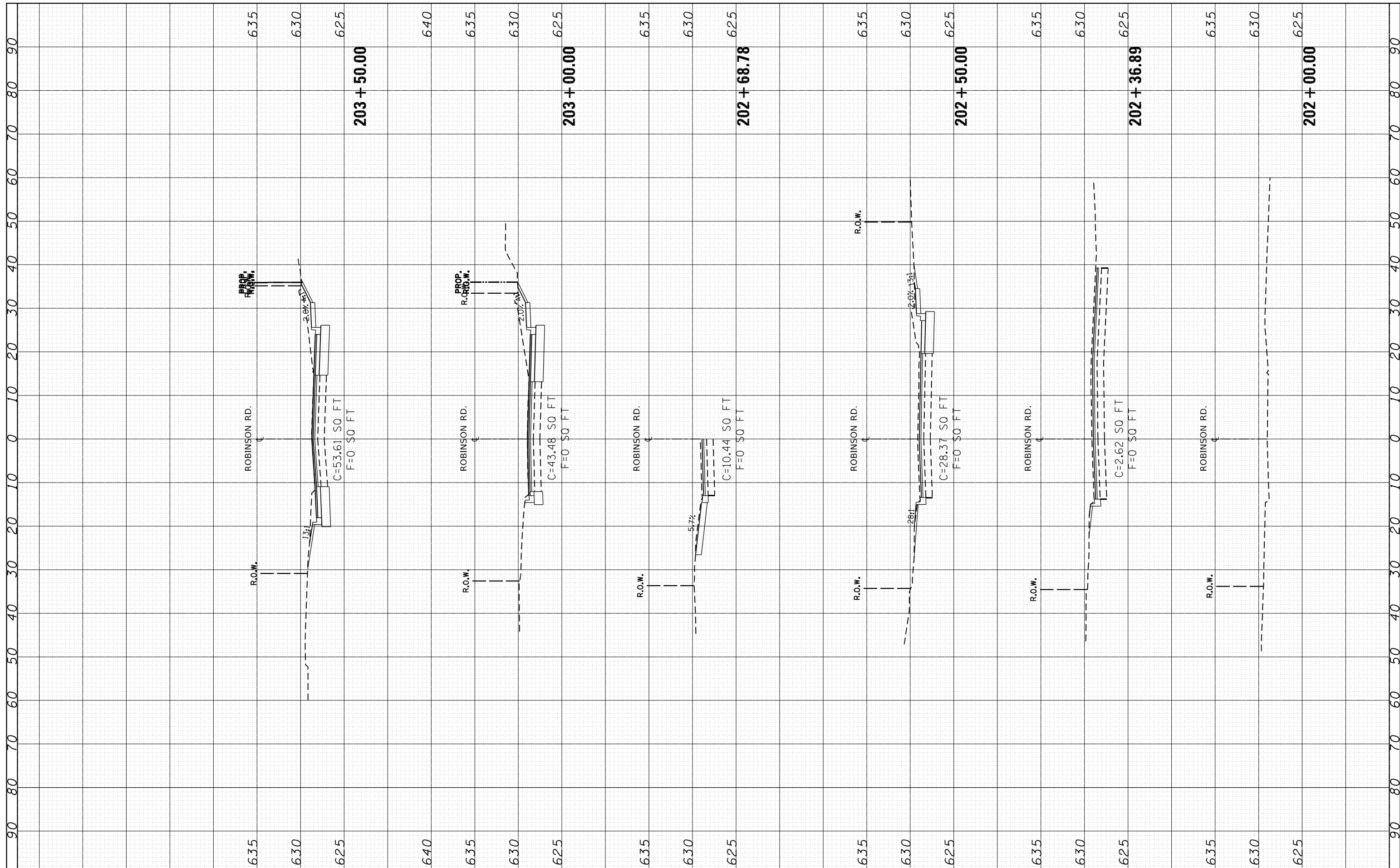
ORIGINAL SURVEY NO.	SURVEYED PLOTTED AREAS CHECKED	BY	DATE
NOTE BOOK NO.	TEMPLATE AREAS CHECKED		



FILE NAME =	USER NAME = qarutnm	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CROSS SECTIONS DES PLAINES RIVER RD. AT ROBINSON RD.</b>			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	DOT Offices\District 1\Projects\P172009\CADD\DRAWING\P172009-sht-xssht-River.dgn	CHECKED -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA. 117+49.75	TO STA. 118+99.75	COOK	78	76
	PLOT SCALE = 20.0000' / in.	DATE -	REVISED -								CONTRACT NO. 60T17		
	PLOT DATE = 1/28/2021										ILLINOIS FED. AID PROJECT		

FINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

ORIGINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE



FILE NAME =  
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 PLOT DATE = 1/28/2021

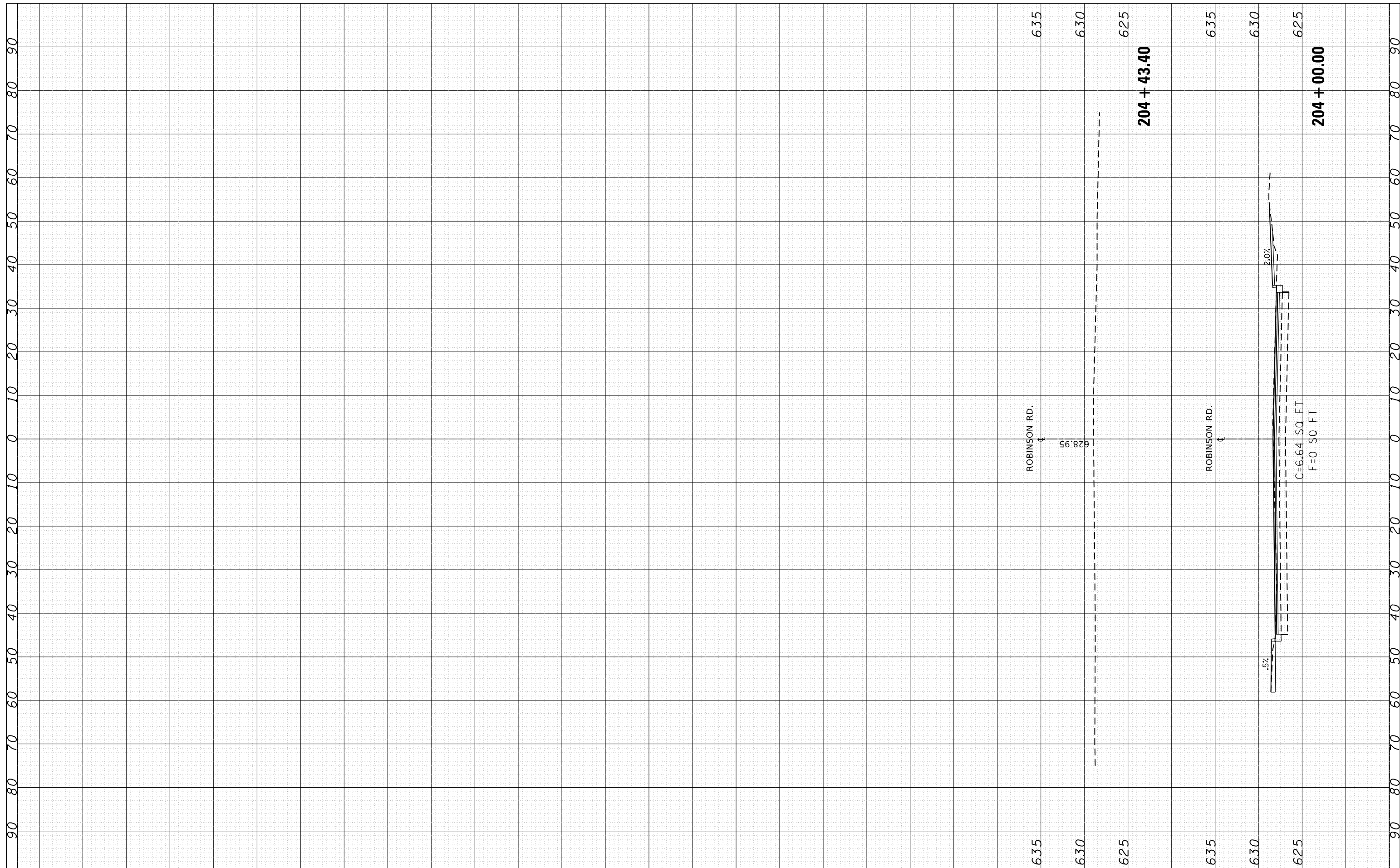
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS  
 DES PLAINES RIVER RD. AT ROBINSON RD.**  
 SCALE: SHEET OF SHEETS STA. 202+00.00 TO STA. 203+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	77
CONTRACT NO. 60T17				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY NO.	SURVEYED AREAS	BY	DATE

ORIGINAL SURVEY NO.	SURVEYED AREAS	BY	DATE



FILE NAME =	USER NAME = qarutnm	DESIGNED -	REVISED -
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Default	PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 1/28/2021	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>CROSS SECTIONS</b>	
<b>DES PLAINES RIVER RD. AT ROBINSON RD.</b>	
SCALE:	SHEET OF SHEETS STA. 204+00.00 TO STA. 204+43.40

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	52-N	COOK	78	78
CONTRACT NO. 60717			ILLINOIS FED. AID PROJECT	