

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2008-063 RS	COOK	42	1
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT NO. 60F40	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROPOSED
HIGHWAY PLANS

FAU 1350 / ILL 72 (TOUHY AVENUE / HIGGINS ROAD)
SECTION: 2008-063 RS
WOLF ROAD TO US 12 / 45 (MANNHEIM ROAD)
& AT WILLOW CREEK
RESURFACING (3P); BRIDGE REPAIR

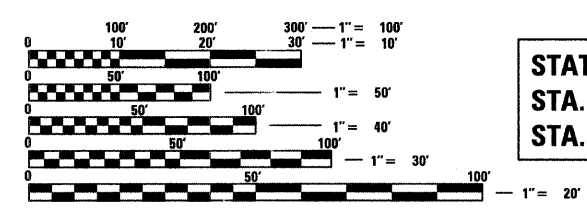
FOR INDEX OF SHEETS, SEE SHEET NO. 2

DESIGN DESIGNATION: OTHER PRINCIPAL ARTERIAL
SPEED LIMIT: 35 M.P.H. TO 45 M.P.H.
2007 ADT = 39,300

PROJECT IS LOCATED IN THE CITY OF DES PLAINES AND THE VILLAGE OF ROSEMONT

COOK COUNTY
C-91-071-09

MAINE TOWNSHIP
R. 11 E.



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS
1-800-892-0123
OR 811

DISTRICT ONE - PLAN PREP ENGINEER:
KEN ENG (847) 705-4247

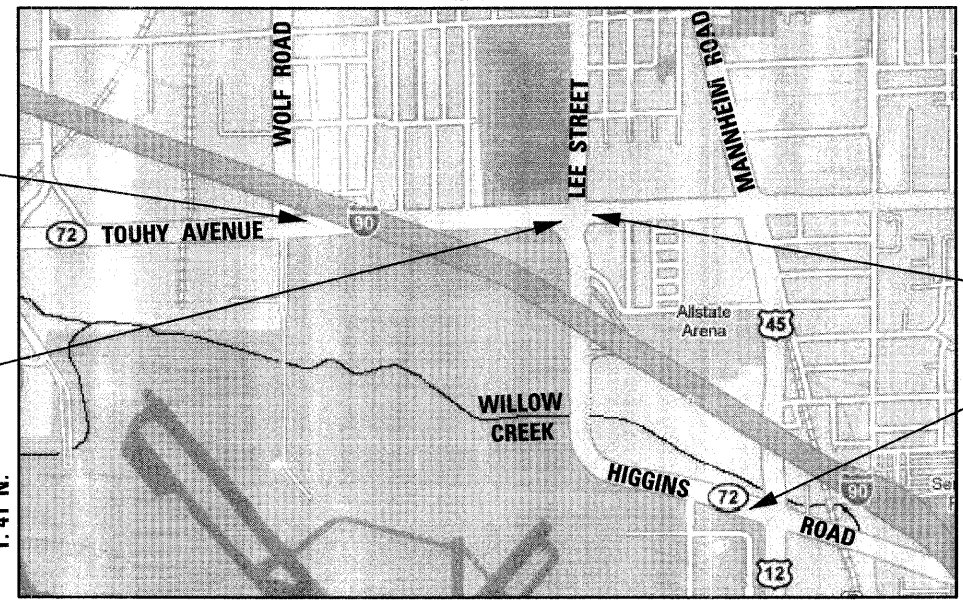
CONTRACT NO. 60F40

PROJECT BEGINS
STA. 101 + 21

STATION EQUATION
STA. 132 + 35.3 TOUHY AVE.
STA. 9 + 24.5 HIGGINS RD.

PROJECT ENDS
STA. 136 + 50

PROJECT ENDS
STA. 67 + 66



GROSS LENGTH OF PROJECT = 9,620 FEET = 1.822 MILES
NET LENGTH OF PROJECT = 9,620 FEET = 1.822 MILES

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED MARCH 5, 2009
Devin M. O'Keefe / DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER
March 27, 2009
Charles J. Ingersoll / ENGINEER OF DESIGN AND ENVIRONMENT
March 27, 2009
Christine M. Reed / DIRECTOR OF HIGHWAYS, CHIEF ENGINEER



Birinder S. Sachdeva / March 5, 2009
BIRINDER S. SACHDEVA, P.E. / DATE
EXPIRES: 11-30-2009



Bhadresh N. Shah / MARCH 05, 2009
BHADRESH N. SHAH, S.E., P.E. / DATE
EXPIRES: 11-30-2010

CR & A
CHRISTIAN-ROGE & ASSOCIATES, INC.
ENGINEERS - PLANNERS - SURVEYORS
211 W. WACKER DRIVE CHICAGO, IL 60606
TELEPHONE: 312-372-2023

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LIST OF STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-03	CLASS C AND D PATCHES
604001-03	FRAME AND LIDS, TYPE 1
604051-03	FRAME AND LIDS, TYPE 11
604091-02	FRAME AND LIDS, TYPE 24
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
606301-04	PC CONCRETE ISLANDS AND MEDIANS
606306-03	CORRUGATED PC CONCRETE MEDIANS
701601-06	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-01	TRAFFIC CONTROL DEVICES
704001-05	TEMPORARY CONCRETE BARRIER
780001-02	TYPICAL PAVEMENT MARKINGS

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
2. 10 FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER ITEMS OF WORK TO EXISTING CURBS AND GUTTERS AND CONDITIONS IN THE FIELD UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
3. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF DES PLAINES AND THE VILLAGE OF ROSEMONT.
4. WHEN ARTIFICIAL LIGHTING IS USED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
5. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
6. SAW CUTTING PRIOR TO ANY REMOVAL ITEMS NOTED ON THE PLANS OR DIRECTED BY THE ENGINEER SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEMS BEING REMOVED.
7. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
8. THE CONTRACTOR SHALL USE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH WILL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
9. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 / 2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 40 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
10. UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
11. PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE TYPE III SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKINGS.
12. THE CONTRACTOR SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.
13. THE CONTRACTOR SHALL CONTACT MR. WALTER CZARNY, THE AREA TRAFFIC FIELD ENGINEER, AT (773) 685-4342 AT LEAST TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
14. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
15. MATCH EXISTING PAVEMENT MARKINGS AT THE PROJECT AND OMISSION LIMITS.
16. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
17. PAY ITEM 55039700 - STORM SEWERS TO BE CLEANED - SHALL ONLY BE UTILIZED TO CLEAN STORM SEWER LATERAL PIPES BETWEEN INLETS OR CATCH BASINS AND MAIN SEWERS.

NOTE: BOXED ITEMS ARE INCLUDED IN THE COST OF THE WORK INVOLVED.

FILE NAME = D160F40-sht-gennote.dgn
PLOT DATE = 3/6/2009



DESIGNED - G.F.L.	REVISED -
DRAWN - B.K.	REVISED -
CHECKED - M.P.	REVISED -
DATE - MARCH 13, 2009	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, LIST OF STATE STANDARDS AND GENERAL NOTES
IL RTE. 72 (TOUHY AVE. / HIGGINS RD.)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. VARIOUS TO STA. VARIOUS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2008-063 RS	COOK	42	2
CONTRACT NO. 60F40				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SUMMARY OF QUANTITIES			URBAN		CONSTRUCTION TYPE CODE	
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	1000	SFTY-2A	
20200100	EARTH EXCAVATION	CU YD	52		52	
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	20	20		
20400800	FURNISHED EXCAVATION	CU YD	20	20		
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	564	564		
21400100	GRADING AND SHAPING DITCHES	FOOT	100	100		
25000210	SEEDING, CLASS 2A	ACRE	0.12	0.12		
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	11	11		
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	11	11		
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	11	11		
25100630	EROSION CONTROL BLANKET	SQ YD	564	564		
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	51	51		
40600300	AGGREGATE (PRIME COAT)	TON	256	256		
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	50	50		
40600895	CONSTRUCTING TEST STRIP	EACH	2	2		
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	570	570		
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	363	363		
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	33		33	
40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	6,244	6,208	36	
42001300	PROTECTIVE COAT	SQ YD	3,442	2,958	484	
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	290	290		
44000086	HOT-MIX ASPHALT SURFACE REMOVAL COMPLETE	SQ YD	347		347	
44000100	PAVEMENT REMOVAL	SQ YD	185		185	
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	63,352	63,352		
44000198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	360		360	
44000300	CURB REMOVAL	FOOT	1,360	530	830	
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	92	92		
44000600	SIDEWALK REMOVAL	SQ FT	290	290		
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	7,986	7,986		
44002020	CONCRETE MEDIAN SURFACE REMOVAL	SQ FT	2,433	1,188	1,245	
44002212	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3"	SQ YD	2,163	2,163		
44003100	MEDIAN REMOVAL	SQ FT	676	676		
44003510	MEDIAN REMOVAL PARTIAL DEPTH	SQ FT	1,000	1,000		
44201749	CLASS D PATCHES, TYPE I, 9 INCH	SQ YD	50	50		
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	860	860		
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	70	70		
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	160	160		
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	590	590		
50102400	CONCRETE REMOVAL	CU YD	47.7		47.7	
50300255	CONCRETE SUPERSTRUCTURE	CU YD	48.6		48.6	
50300260	BRIDGE DECK GROOVING	SQ YD	330		330	
50300300	PROTECTIVE COAT	SQ YD	484		484	
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	8,020		8,020	
50800515	BAR SPLICERS	EACH	68		68	
52000110	PREFORMED JOINT STRIP SEAL	FOOT	140		140	
55039700	STORM SEWERS TO BE CLEANED	FOOT	2,478	2,478		
60251740	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 24 FRAME AND GRATE	EACH	22	22		
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	2	2		

SUMMARY OF QUANTITIES			URBAN		CONSTRUCTION TYPE CODE	
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	1000	SFTY-2A	
60254100	CATCH BASINS TO BE RECONSTRUCTED WITH NEW TYPE 11 FRAME AND GRATE	EACH	1	1		
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	2	2		
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	25	25		
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	35	35		
60404950	FRAMES AND GRATES, TYPE 24	EACH	2	2		
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	2	2		
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	2	2		
60619121	CONCRETE MEDIAN, TYPE SB-6	SQ FT	1,660		1,660	
60619200	CONCRETE MEDIAN, TYPE SB-6.06	SQ FT	1,672	1,672		
60621911	CONCRETE MEDIAN, TYPE SM-2.06	SQ FT	314	314		
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	4	2	
67100100	MOBILIZATION	L SUM	1	0.5	0.5	
70101800	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1		1	
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1		
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1		
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	25,000	24,199	801	
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	1,201	1,201		
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	48,212	36,408	11,804	
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	4,012	4,012		
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	1,320	1,320		
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	326	326		
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	540	540		
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	4,300	4,300		
70400100	TEMPORARY CONCRETE BARRIER	FOOT	350		350	
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	1,201	1,201		
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	36,408	35,832	576	
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	4,012	4,012		
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	1,320	1,320		
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	326	326		
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	520	520		
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	706	706		
* 78100105	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	EACH	4		4	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	680	680		
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	840	840		
X0322185	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 1/4 INCHES	SQ YD	311		311	
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	52	52		
X0322879	GRADING AND SHAPING FORESLOPES	SQ YD	170	170		
X0325303	STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)	SQ FT	232		232	
X0325305	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	11		11	
X0656100	DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT	SQ YD	39	39		
X0712400	TEMPORARY PAVEMENT	SQ YD	185		185	
X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	2,661	2,661		
Z0006204	BRIDGE DECK HYDRO-SCARIFICATION 1/2"	SQ YD	311		311	
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	12		12	
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	82	82		
Z0030255	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	2		2	

* SPECIALTY ITEM

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PLOT DATE = 3/6/2009



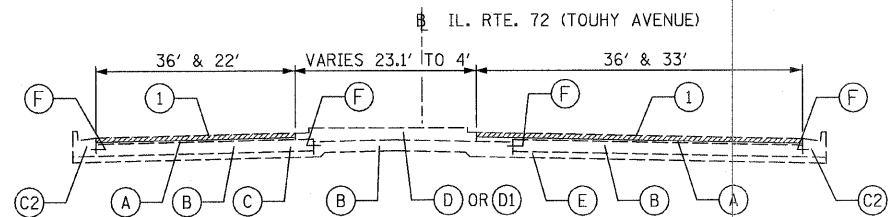
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

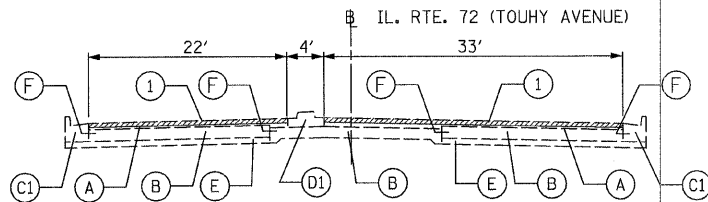
SUMMARY OF QUANTITIES
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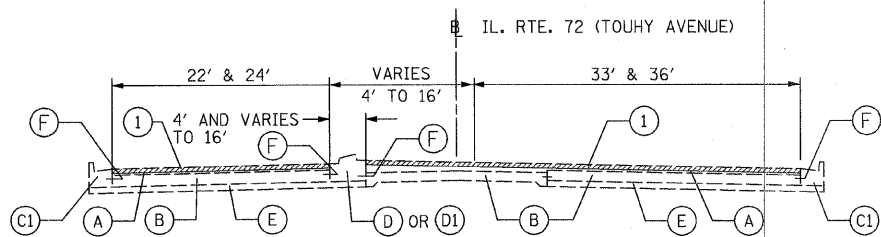
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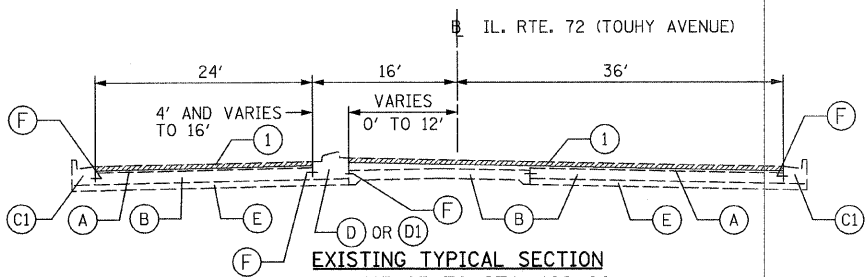
EXISTING TYPICAL SECTION
STA. 101+21 TO STA. 105+05



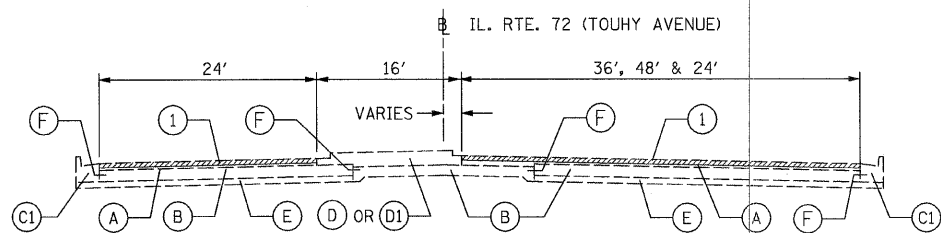
EXISTING TYPICAL SECTION
STA. 105+05 TO STA. 109+93



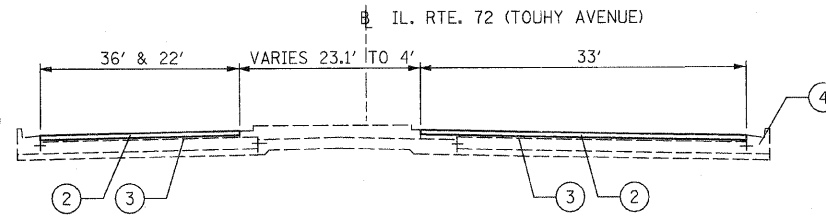
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STA. 109+93 TO STA. 115+85



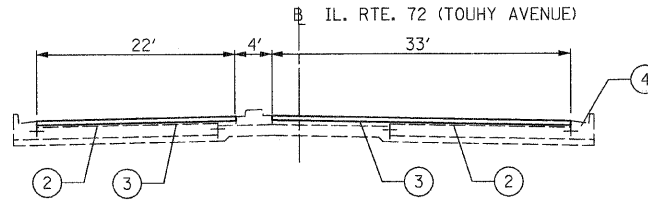
EXISTING TYPICAL SECTION
STA. 115+85 TO STA. 122+20



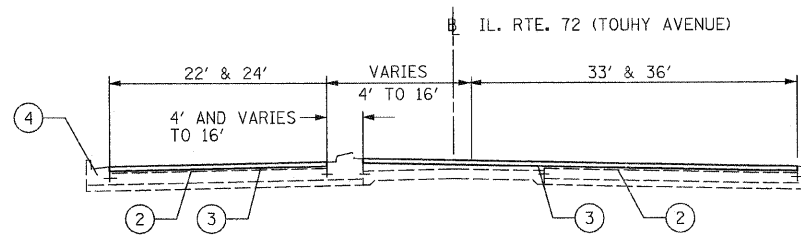
EXISTING TYPICAL SECTION
STA. 122+20 TO STA. 131+48



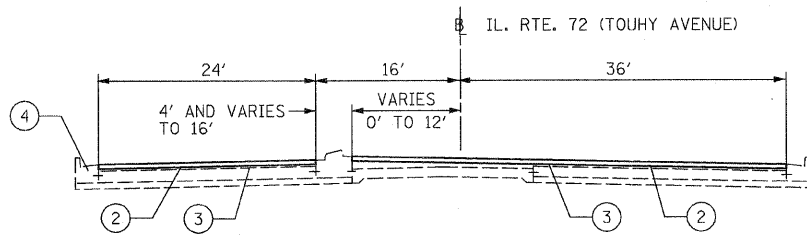
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STA. 101+21 TO STA. 105+05



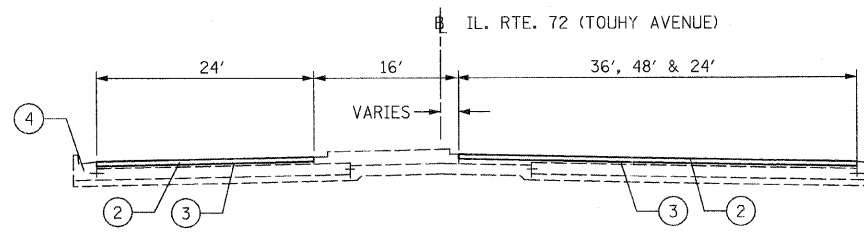
PROPOSED TYPICAL SECTION
STA. 105+05 TO STA. 109+93



PROPOSED TYPICAL SECTION
STA. 109+93 TO STA. 115+85



PROPOSED TYPICAL SECTION
STA. 115+85 TO STA. 122+20



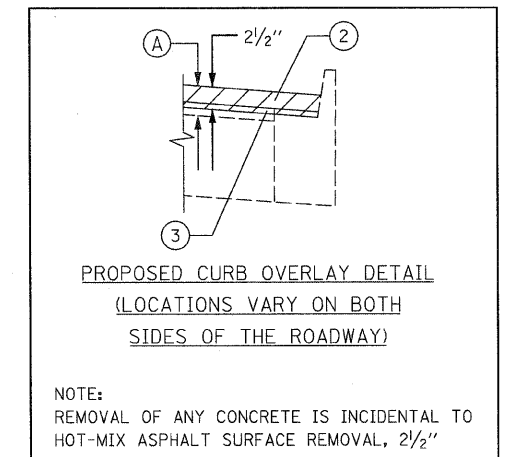
EXISTING TYPICAL SECTION
STA. 122+20 TO STA. 131+48

EXISTING CONDITIONS:

- (A) HOT-MIX ASPHALT SURFACE AND BINDER COURSE, 3" & VARIES
- (B) P.C.C. BASE COURSE, 9" OR P.C.C. PAVEMENT, 10"
- (C) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (C1) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
- (C2) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (C3) CONCRETE CURB, TYPE B-6
- (C4) CONCRETE CURB, TYPE B-6 (DOWELLED)
- (D) CONCRETE MEDIAN, BARRIER OR MOUNTABLE
- (D1) CONCRETE MEDIAN, CORRUGATED OR BARRIER (DOWELLED)
- (E) SUB-BASE GRANULAR, TYPE B, 4" OR 6"
- (F) TIE-BARS

PROPOSED IMPROVEMENTS:

- (1) HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- (2) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1 3/4"
- (3) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- (4) COMB. CONC. CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DETERMINED BY THE ENGINEER)



* WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22. THE UNIT WEIGHT USED TO CALCULATE ALL HMA MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

ROADWAY NAME	HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
	MIXTURE TYPE	AC TYPE	AIR VOIDS
IL 72 (TOUHY AVE./HIGGINS RD.)	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL-9.5mm)	SBS/SBR PG 70-22	4% @ 90 GYR
	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	SBS/SBR PG 76-28/22	4% @ 50 GYR
	TEMPORARY PAVEMENT (HMA SURFACE COURSE, MIX "D", N50, IL-9.5mm)	* PG 64-22	4% @ 50 GYR
	TEMPORARY PAVEMENT (HMA BINDER COURSE, IL-19, N50)	* PG 64-22/58-22	4% @ 50 GYR
	MOT STAGE I (HMA SURFACE COURSE, MIX "D", N50, IL 9.5mm)	* PG 64-22	4% @ 50 GYR
	CLASS D PATCHES (HMA BINDER, IL-19)	* PG 64-22	4% @ 70 GYR.
	HMA REPLACEMENT OVER PATCHES (HMA BINDER, IL-19)	* PG 64-22	4% @ 70 GYR.

NOTE:
THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING.

FILE NAME = D160F40-shr-typical.dgn
PLOT DATE = 3/6/2009

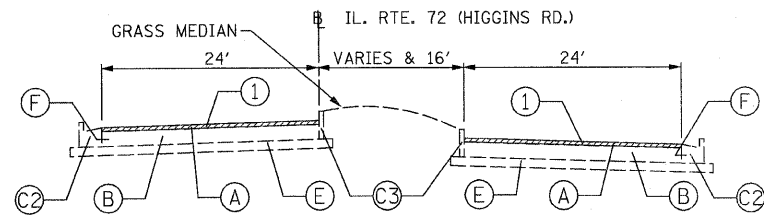


DESIGNED - G.F.L.	REVISED -
DRAWN - B.K.	REVISED -
CHECKED - M.P.	REVISED -
DATE - MARCH 13, 2009	REVISED -

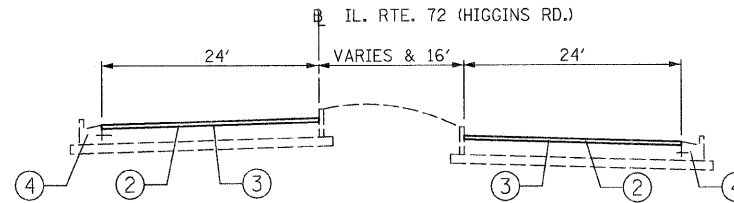
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS IL 72 (TOUHY AVE./HIGGINS RD.)		F.A.U. RTE. 1350	SECTION 2008-063 RS	COUNTY COOK	TOTAL SHEETS 42	SHEET NO. 4
SCALE: NONE	SHEET NO. 1 OF 3 SHEETS	STA. VARIOUS	TO STA. VARIOUS	CONTRACT NO. 60F40		

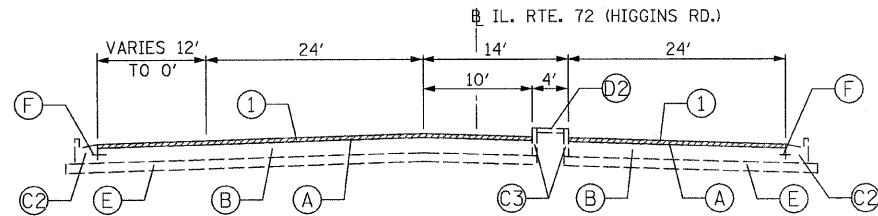
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT	
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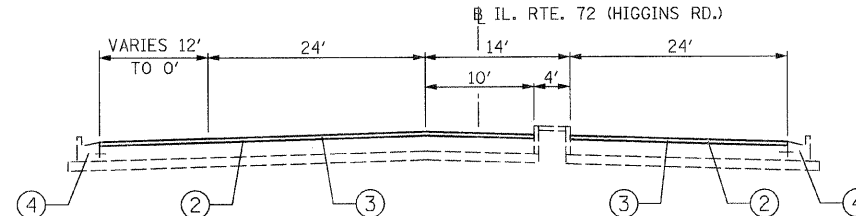
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STA. 9+24.50 TO STA. 12+80



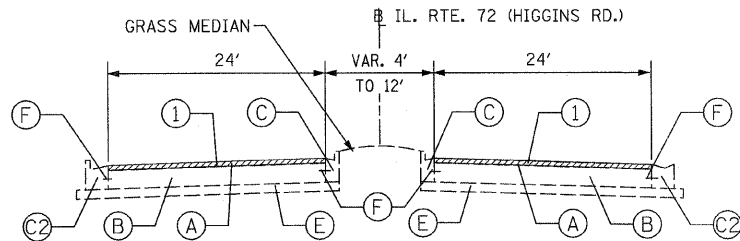
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STA. 9+24.5 TO STA. 12+80



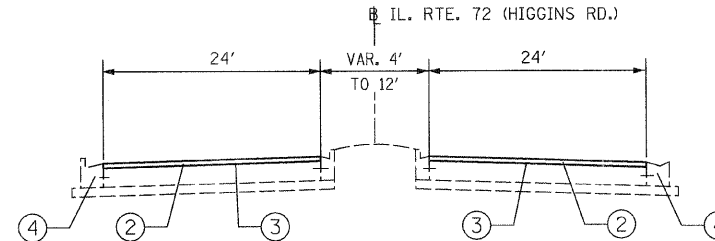
EXISTING TYPICAL SECTION
STA. 12+80 TO STA. 17+58



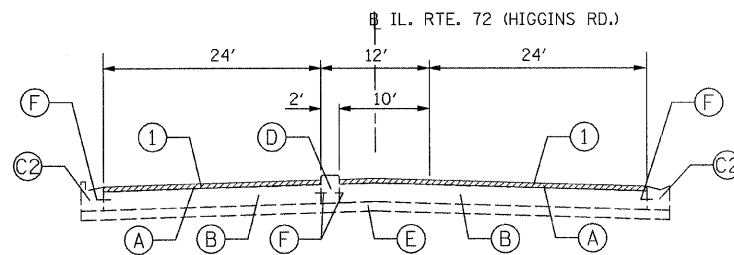
PROPOSED TYPICAL SECTION
STA. 12+80 TO STA. 17+58



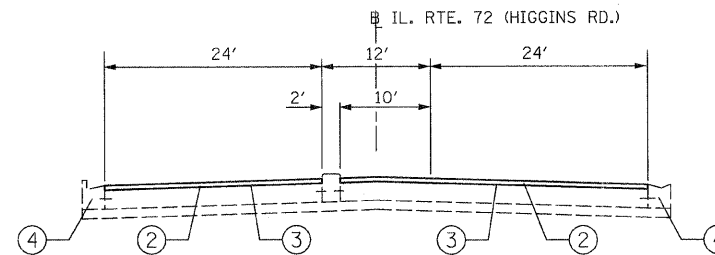
EXISTING TYPICAL SECTION
STA. 17+58 TO STA. 22+30
STA. 23+30 TO STA. 25+45 - BRIDGE OMISSION
STA. 28+50 TO STA. 31+87



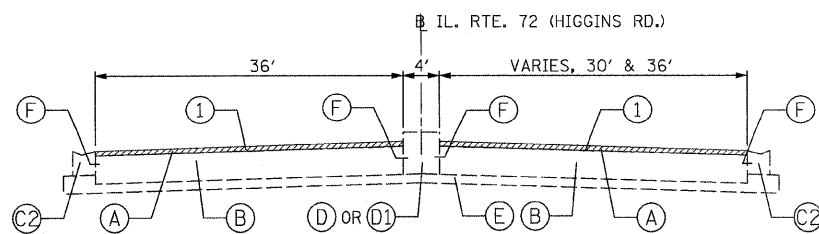
PROPOSED TYPICAL SECTION
STA. 17+58 TO STA. 22+30
STA. 23+30 TO STA. 25+45 - BRIDGE OMISSION
STA. 28+50 TO STA. 31+87



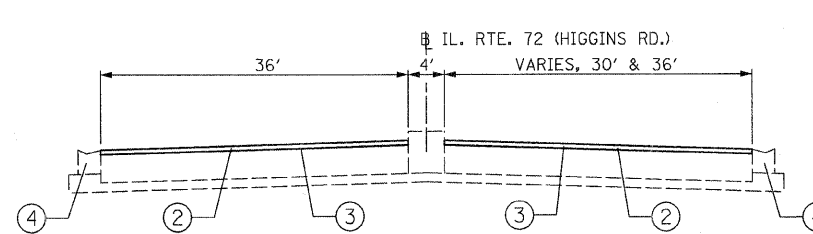
EXISTING TYPICAL SECTION
STA. 25+45 TO STA. 28+50



PROPOSED TYPICAL SECTION
STA. 25+45 TO STA. 28+50



EXISTING TYPICAL SECTION
STA. 31+87 TO STA. 36+64.40



PROPOSED TYPICAL SECTION
STA. 31+87 TO STA. 36+64.40

EXISTING CONDITIONS:

- (A) HOT-MIX ASPHALT SURFACE AND BINDER COURSE, 3" & VARIES
- (B) P.C.C. BASE COURSE, 9" OR P.C.C. PAVEMENT, 10"
- (C) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (C1) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
- (C2) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (C3) CONCRETE CURB, TYPE B-6
- (C4) CONCRETE CURB, TYPE B-6 (DOWELLED)
- (D) CONCRETE MEDIAN, BARRIER OR MOUNTABLE
- (D1) CONCRETE MEDIAN, CORRUGATED OR BARRIER (DOWELLED)
- (D2) CONCRETE MEDIAN SURFACE, 4" & VARIES
- (E) SUB-BASE GRANULAR, TYPE B, 4" OR 6"
- (F) TIE-BARS

PROPOSED IMPROVEMENTS:

- (1) HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2 "
- (2) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1 3/4 "
- (3) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4 "
- (4) COMB. CONC. CURB AND GUTTER REMOVAL AND REPLACEMENT
(AS DETERMINED BY THE ENGINEER)

FILE NAME = D168F40-sh1-typical.dgn
PLOT DATE = 3/6/2009

CHRISTIAN-ROGE & ASSOCIATES, INC.
ENGINEERS-PLANNERS-SURVEYORS
211 WEST WACKER DRIVE
CHICAGO, ILLINOIS 60606
PHONE: (312)372-2023 FAX: (312)372-5274

DESIGNED - G.F.L.
DRAWN - B.K.
CHECKED - M.P.
DATE - MARCH 13, 2009

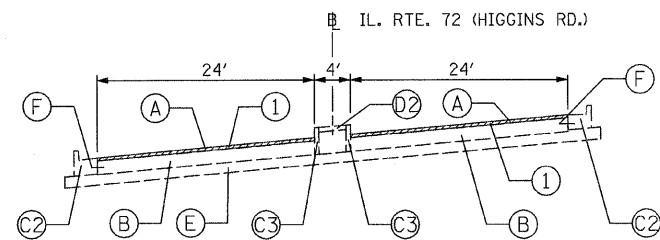
REVISED -
REVISED -
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REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

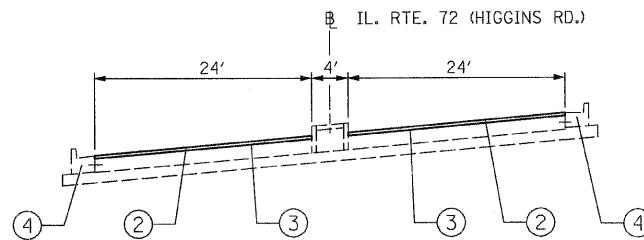
TYPICAL SECTIONS
IL 72 (TOUHY AVE./HIGGINS RD.)

SCALE: NONE SHEET NO. 2 OF 3 SHEETS STA. VARIOUS TO STA. VARIOUS

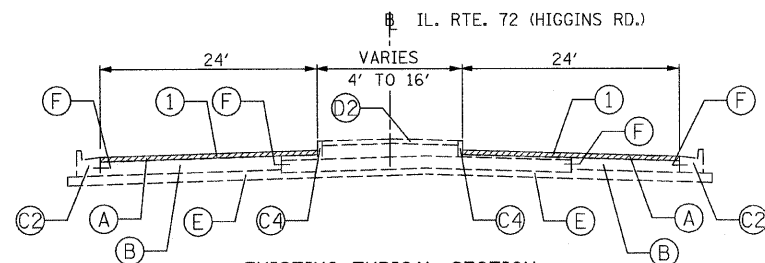
F.A.U. RTE. 1350	SECTION 2008-063 RS	COUNTY COOK	TOTAL SHEETS 42	SHEET NO. 5
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				CONTRACT NO. 60F40



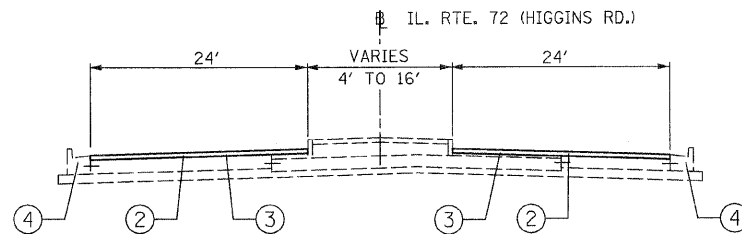
EXISTING TYPICAL SECTION
 STA. 36+64.41 TO STA. 37+86.50 (BRIDGE)
 STA. 37+86.50 TO STA. 47+88



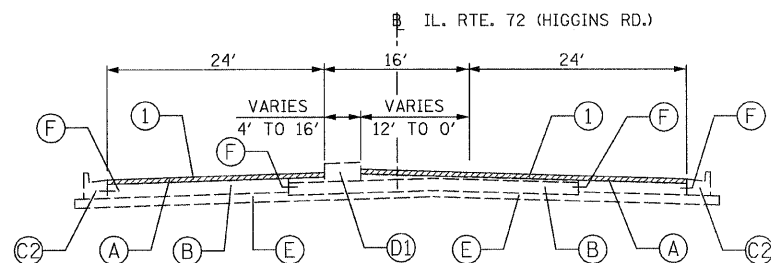
PROPOSED TYPICAL SECTION
 STA. 36+64.41 TO STA. 37+86.50 (BRIDGE)
 STA. 37+86.50 TO STA. 47+88



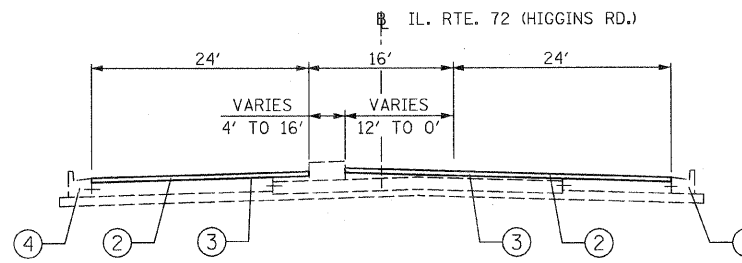
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 STA. 47+88 TO STA. 51+20



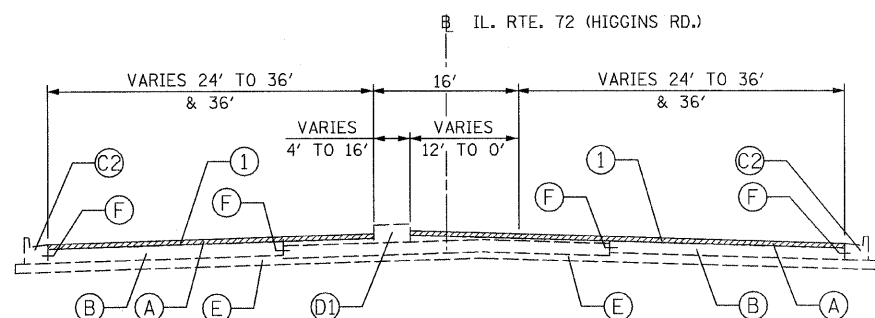
PROPOSED TYPICAL SECTION
 STA. 47+88 TO STA. 51+20



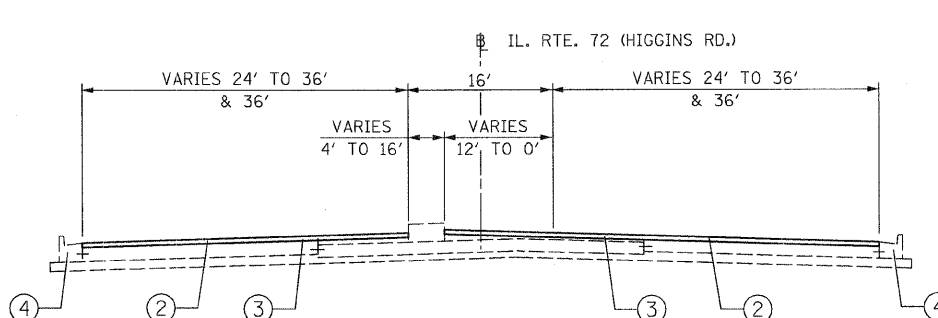
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 STA. 51+20 TO STA. 57+02



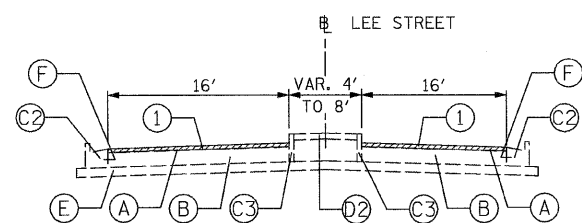
PROPOSED TYPICAL SECTION
 STA. 51+20 TO STA. 57+02



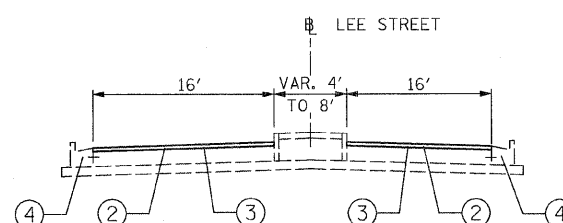
EXISTING TYPICAL SECTION
 STA. 57+02 TO STA. 67+66



PROPOSED TYPICAL SECTION
 STA. 57+02 TO STA. 67+66



EXISTING TYPICAL SECTION
 STA. 300+32 TO STA. 303+10



PROPOSED TYPICAL SECTION
 STA. 300+32 TO STA. 303+10

EXISTING CONDITIONS:

- (A) HOT-MIX ASPHALT SURFACE AND BINDER COURSE, 3" & VARIES
- (B) P.C.C. BASE COURSE, 9" OR P.C.C. PAVEMENT, 10"
- (C) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (C1) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
- (C2) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (C3) CONCRETE CURB, TYPE B-6
- (C4) CONCRETE CURB, TYPE B-6 (DOWELLED)
- (D) CONCRETE MEDIAN, BARRIER OR MOUNTABLE
- (D1) CONCRETE MEDIAN, CORRUGATED OR BARRIER (DOWELLED)
- (D2) CONCRETE MEDIAN SURFACE, 4" & VARIES
- (E) SUB-BASE GRANULAR, TYPE B, 4" OR 6"
- (F) TIE-BARS

PROPOSED IMPROVEMENTS:

- (1) HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2 "
- (2) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1 3/4 "
- (3) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4 "
- (4) COMB. CONC. CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DETERMINED BY THE ENGINEER)

FILE NAME = D168F40-shr-typical.dgn
 PLOT DATE = 3/6/2009

CHRISTIAN-ROGE & ASSOCIATES, INC.
 ENGINEERS-PLANNERS-SURVEYORS
 211 WEST WACKER DRIVE
 CHICAGO, ILLINOIS 60606
 PHONE: (312)372-2023 FAX: (312)372-5274

DESIGNED - G.F.L.
 DRAWN - B.K.
 CHECKED - M.P.
 DATE - MARCH 13, 2009

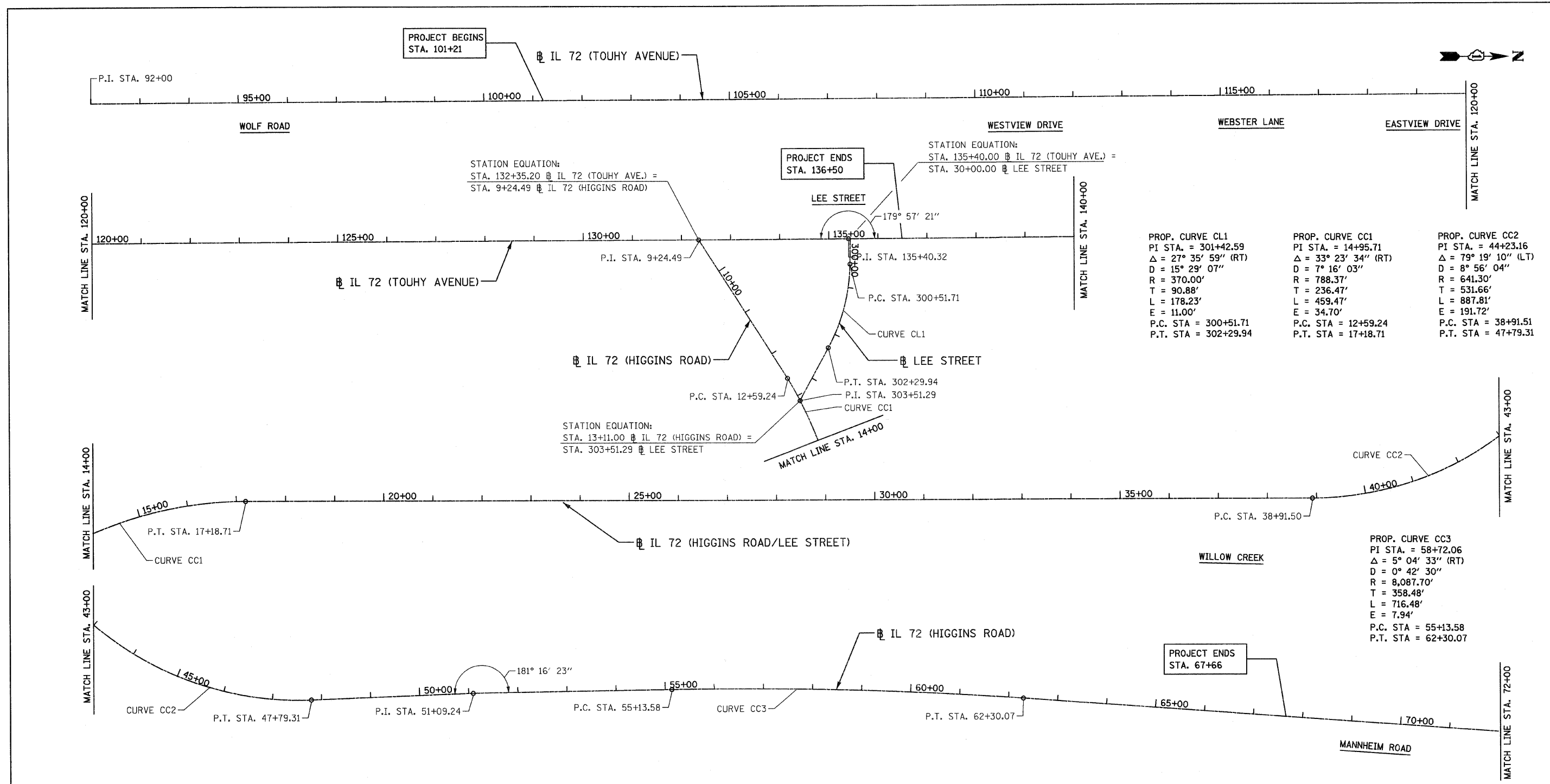
REVISED -
 REVISED -
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 REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
IL 72 (TOUHY AVE./HIGGINS RD.)

SCALE: NONE SHEET NO. 3 OF 3 SHEETS STA. VARIOUS TO STA. VARIOUS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2008-063 RS	COOK	42	6
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60F40	

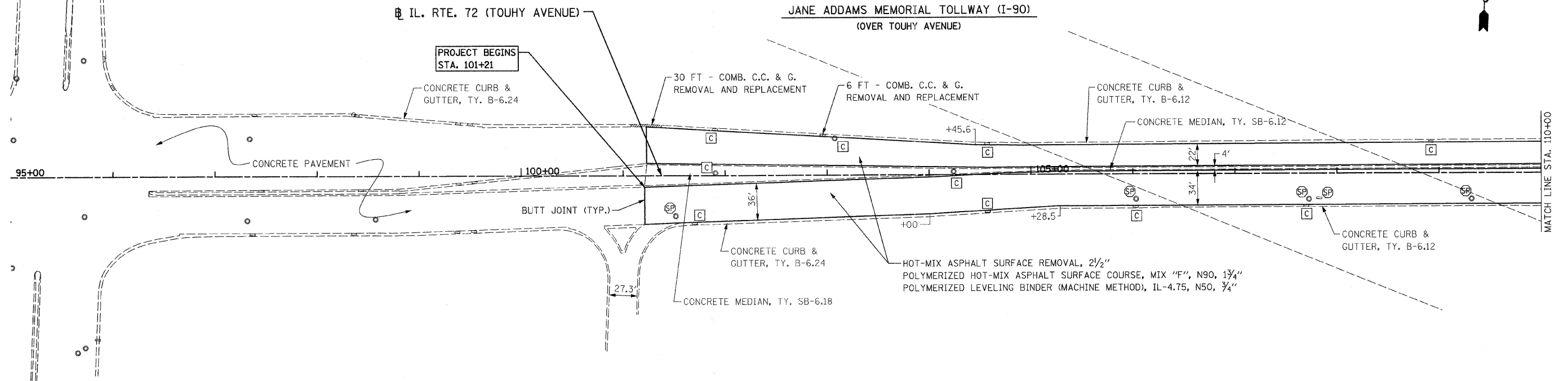


PROP. CURVE CL1	PROP. CURVE CC1	PROP. CURVE CC2
PI STA. = 301+42.59	PI STA. = 14+95.71	PI STA. = 44+23.16
$\Delta = 27^\circ 35' 59''$ (RT)	$\Delta = 33^\circ 23' 34''$ (RT)	$\Delta = 79^\circ 19' 10''$ (LT)
D = 15° 29' 07"	D = 7° 16' 03"	D = 8° 56' 04"
R = 370.00'	R = 788.37'	R = 641.30'
T = 90.88'	T = 236.47'	T = 531.66'
L = 178.23'	L = 459.47'	L = 887.81'
E = 11.00'	E = 34.70'	E = 191.72'
P.C. STA = 300+51.71	P.C. STA = 12+59.24	P.C. STA = 38+91.51
P.T. STA = 302+29.94	P.T. STA = 17+18.71	P.T. STA = 47+79.31

PROP. CURVE CC3
PI STA. = 58+72.06
$\Delta = 5^\circ 04' 33''$ (RT)
D = 0° 42' 30"
R = 8,087.70'
T = 358.48'
L = 716.48'
E = 7.94'
P.C. STA = 55+13.58
P.T. STA = 62+30.07

CONTROL POINTS - TOUHY AVENUE						CONTROL POINTS - HIGGINS ROAD					CONTROL POINTS - HIGGINS ROAD						
POINT #	STATION	OFFSET	NORTH (Y)	EAST (X)	DESCRIPTION	POINT #	STATION	OFFSET	NORTH (Y)	EAST (X)	DESCRIPTION	POINT #	STATION	OFFSET	NORTH (Y)	EAST (X)	DESCRIPTION
Y13	99+31.22	12.90	51867.1059	46397.7996	X-CUT	Y17	10+51.50	-34.97	51902.7572	49799.7931	PK NAIL	261	37+81.95	-34.10	49212.4699	50026.8304	BOLT
Y11	112+15.32	-3.96	51926.0612	47680.6551	X-CUT	Z8	13+12.33	73.72	51629.8827	49850.0330	X-CUT	Z3	38+31.35	-34.49	49163.0700	50026.7400	IR
Y15	114+98.66	56.60	51874.8198	47965.8272	PKN	Z5	19+69.53	-36.51	51024.7736	50047.1097	PK NAIL	64	41+63.65	29.20	48826.9018	50019.2069	X-CUT
Y16	120+42.87	49.24	51900.0222	48509.5125	PKN	Z9	25+76.99	-36.24	50417.3543	50040.8556	GPS MONUMENT	Z4	42+27.60	43.65	48760.0400	50036.5600	IR
Y9	123+26.74	-3.28	51961.8199	48791.4997	X-CUT	Z1	29+94.72	0.50	50000.0000	50000.0000	IR	63	42+30.00	29.17	48764.9796	50050.4056	X-CUT
Y7	134+00.70	41.32	51952.4581	49866.345	IR	503	30+56.81	-37.33	49937.5373	50037.2107	X-CUT	62	42+82.80	29.24	48718.1526	50079.6166	X-CUT
C2	135+40.32	0.00	51998.3371	50004.5373	PI	580	33+97.80	-65.82	49596.2848	50062.3400	C.P.	Z10	50+26.95	46.07	48374.4674	50741.7618	GPS MONUMENT
						268	35+74.15	46.72	49421.0540	49948.0674	X-CUT	C16	51+09.24	0.00	48403.6080	50831.4617	PI
						269	35+79.91	80.13	49415.6267	49914.6002	X-CUT	Z11	58+38.71	-45.75	48283.3629	51553.7900	PK NAIL
						Z2	36+09.22	56.26	49386.0800	49938.1800	GPS MONUMENT	Z12	68+65.29	-47.46	47983.1283	52537.7020	X-CUT
						267	36+15.04	43.40	49380.1299	49950.9827	X-CUT	Z13	70+17.19	53.81	47840.6848	52651.8852	CP

WOLF ROAD

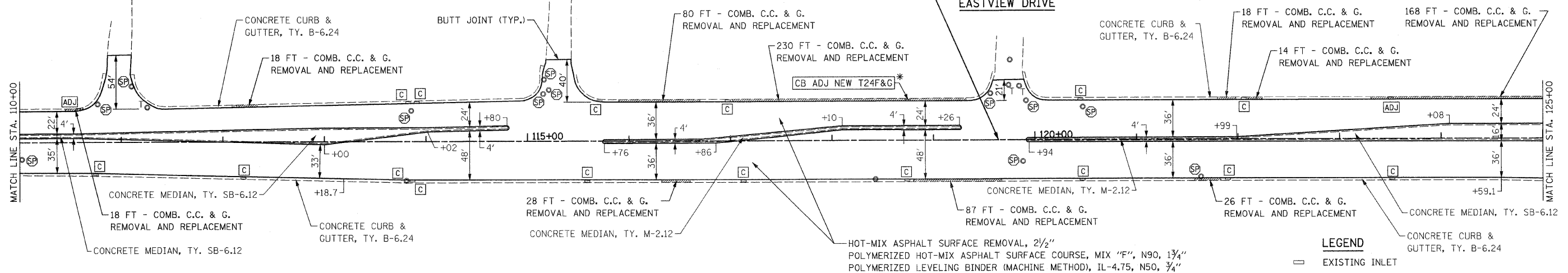


WESTVIEW DRIVE

WEBSTER LANE

IL. RTE. 72 (TOUHY AVENUE)

EASTVIEW DRIVE



LEGEND

- ⊖ EXISTING INLET
- EXISTING CATCH BASIN
- ⊙ EXISTING MANHOLE
- W WATER
- CE COM ED (ADJUSTMENTS PERFORMED BY OTHERS)
- T TELEPHONE (ADJUSTMENTS PERFORMED BY OTHERS)
- G GAS
- ⊕ SP FRAMES AND LIDS TO BE ADJUSTED, SPECIAL
- ⊠ DRAINAGE STRUCTURES TO BE CLEANED
- ⊠ ADJ FRAMES AND LIDS TO BE ADJUSTED

* [CB ADJ NEW T24F&G] CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 24 FRAME AND GRATE.

FILE NAME = D160F40-ehs-plan01.dgn
PLOT DATE = 3/5/2009

CHRISTIAN-ROGE & ASSOCIATES, INC.
ENGINEERS-PLANNERS-SURVEYORS
211 WEST WACKER DRIVE
CHICAGO, ILLINOIS 60606
PHONE: (312)372-2023 FAX: (312)372-5274

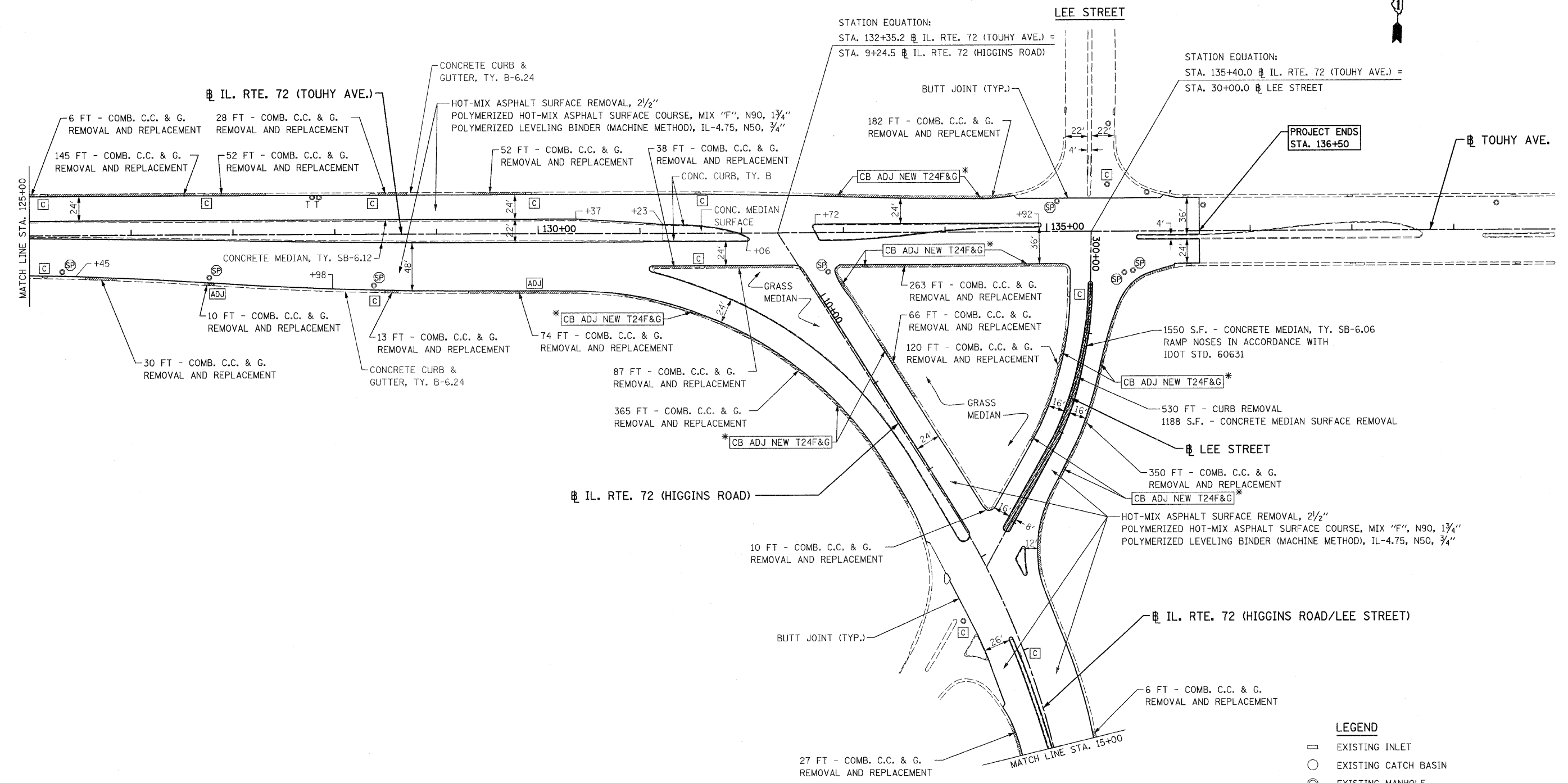
DESIGNED - G.F.L.
DRAWN - B.K.
CHECKED - M.P.
DATE - MARCH 13, 2009

REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN
IL. RTE. 72 (TOUHY AVE. / HIGGINS RD.)**
SCALE: 1" = 50' SHEET NO. 1 OF 4 SHEETS STA. 101+21 TO STA. 125+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2008-063 RS	COOK	42	8
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60F40	



LEGEND

- EXISTING INLET
- EXISTING CATCH BASIN
- ⊙ EXISTING MANHOLE
- W WATER
- CE COM ED (ADJUSTMENTS PERFORMED BY OTHERS)
- T TELEPHONE (ADJUSTMENTS PERFORMED BY OTHERS)
- G GAS
- ⊕ SP FRAMES AND LIDS TO BE ADJUSTED, SPECIAL
- ⊠ C DRAINAGE STRUCTURES TO BE CLEANED
- ⊠ ADJ FRAMES AND LIDS TO BE ADJUSTED

* CB ADJ NEW T24F&G — CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 24 FRAME AND GRATE.

FILE NAME = D168F40-sht-plan02.dgn
 PLOT DATE = 3/6/2009

CHRISTIAN-ROGE & ASSOCIATES, INC.
 ENGINEERS-PLANNERS-SURVEYORS
 211 WEST WACKER DRIVE
 CHICAGO, ILLINOIS 60606
 PHONE: (312)372-2023 FAX: (312)372-5274

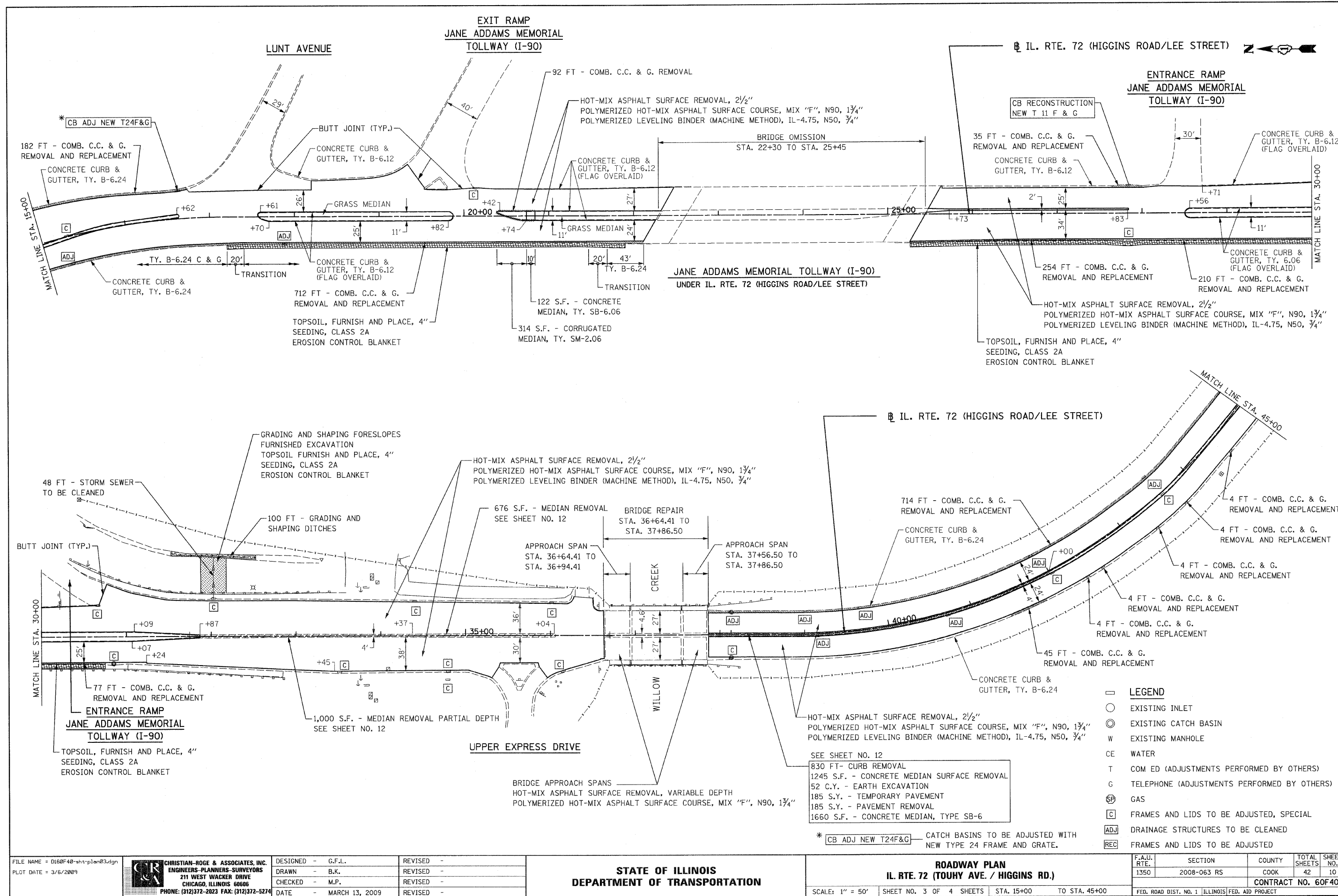
DESIGNED - G.F.L.	REVISED -
DRAWN - B.K.	REVISED -
CHECKED - M.P.	REVISED -
DATE - MARCH 13, 2009	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN
 IL. RTE. 72 (TOUHY AVE. / HIGGINS RD.)**

SCALE: 1" = 50' SHEET NO. 2 OF 4 SHEETS STA. 125+00 TO STA. 15+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2008-063 RS	COOK	42	9
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT			CONTRACT NO. 60F40	



FILE NAME = D:\60F40-shr-plan\3.dgn
PLOT DATE = 3/6/2009

CHRISTIAN-ROGE & ASSOCIATES, INC.
ENGINEERS-PLANNERS-SURVEYORS
211 WEST WACKER DRIVE
CHICAGO, ILLINOIS 60606
PHONE: (312)372-2023 FAX: (312)372-9274

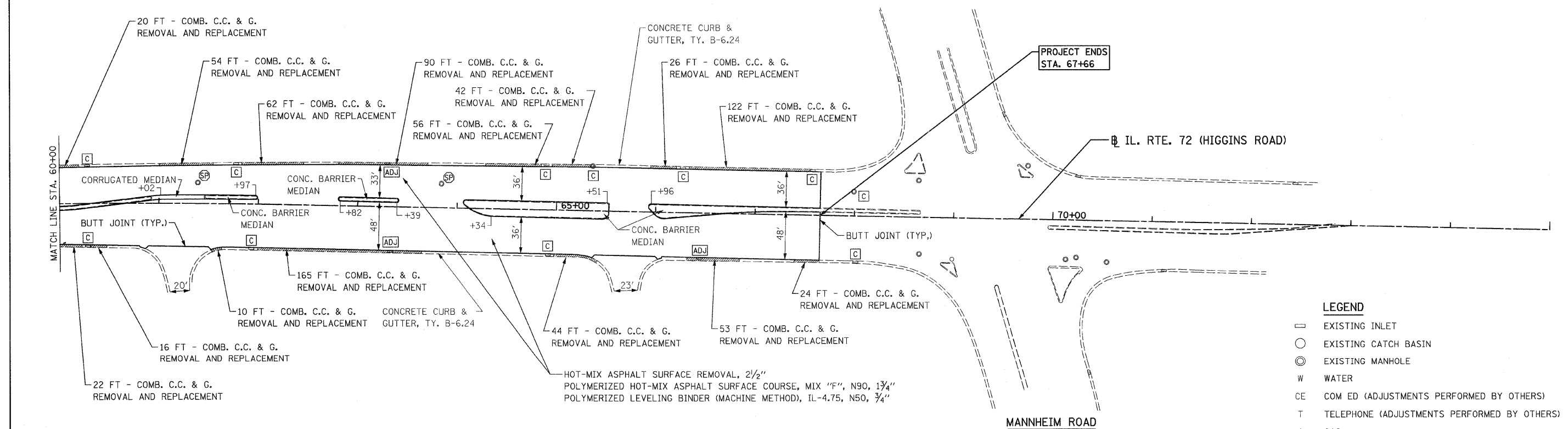
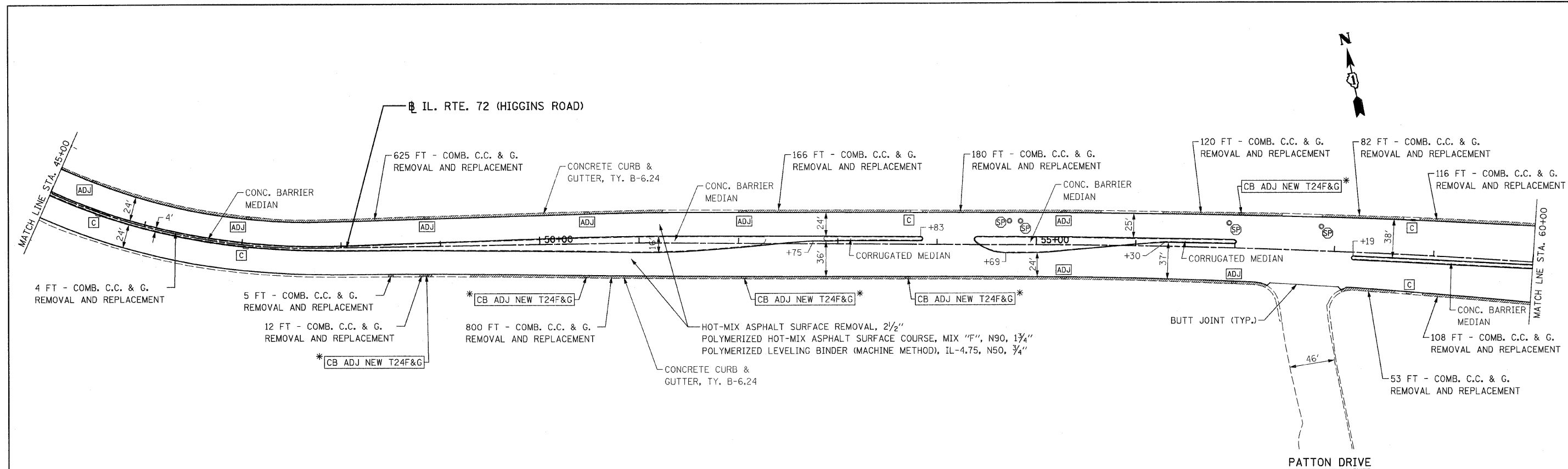
DESIGNED - G.F.L.	REVISED -
DRAWN - B.K.	REVISED -
CHECKED - M.P.	REVISED -
DATE - MARCH 13, 2009	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN
IL. RTE. 72 (TOUHY AVE. / HIGGINS RD.)**

SCALE: 1" = 50'
SHEET NO. 3 OF 4 SHEETS
STA. 15+00 TO STA. 45+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2008-063 RS	COOK	42	10
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60F40	



- LEGEND**
- EXISTING INLET
 - EXISTING CATCH BASIN
 - ⊙ EXISTING MANHOLE
 - W WATER
 - CE COM ED (ADJUSTMENTS PERFORMED BY OTHERS)
 - T TELEPHONE (ADJUSTMENTS PERFORMED BY OTHERS)
 - G GAS
 - ⊕ SP FRAMES AND LIDS TO BE ADJUSTED, SPECIAL
 - ⊕ C DRAINAGE STRUCTURES TO BE CLEANED
 - ⊕ ADJ FRAMES AND LIDS TO BE ADJUSTED

* CB ADJ NEW T24F&G — CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 24 FRAME AND GRATE.

FILE NAME = D168F40-shr-plan04.dgn
 PLOT DATE = 3/6/2009

CHRISTIAN-ROGE & ASSOCIATES, INC.
 ENGINEERS-PLANNERS-SURVEYORS
 211 WEST WACKER DRIVE
 CHICAGO, ILLINOIS 60606
 PHONE: (312)372-2023 FAX: (312)372-5274

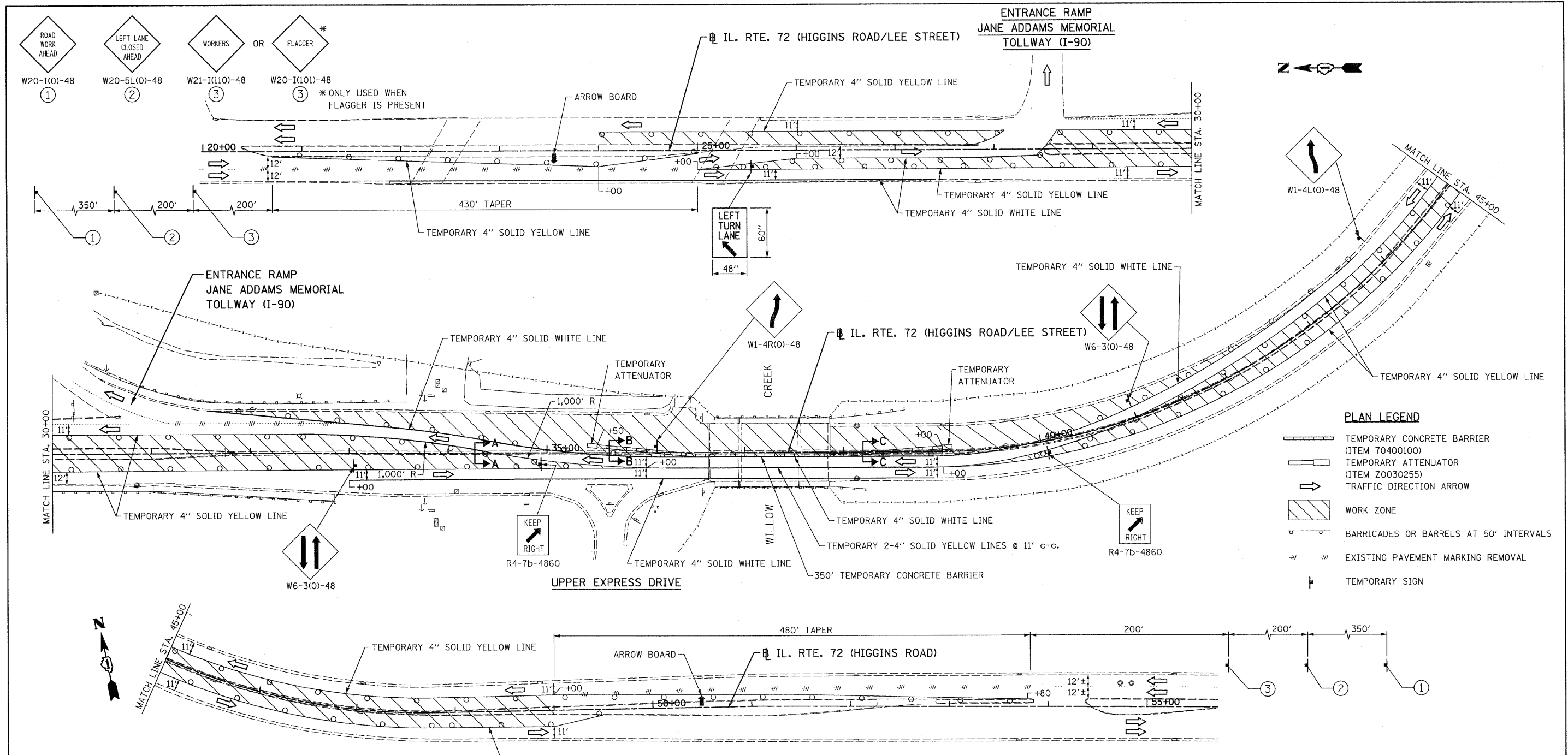
DESIGNED - G.F.L.	REVISED -
DRAWN - B.K.	REVISED -
CHECKED - M.P.	REVISED -
DATE - MARCH 13, 2009	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN
IL. RTE. 72 (TOUHY AVE. / HIGGINS RD.)

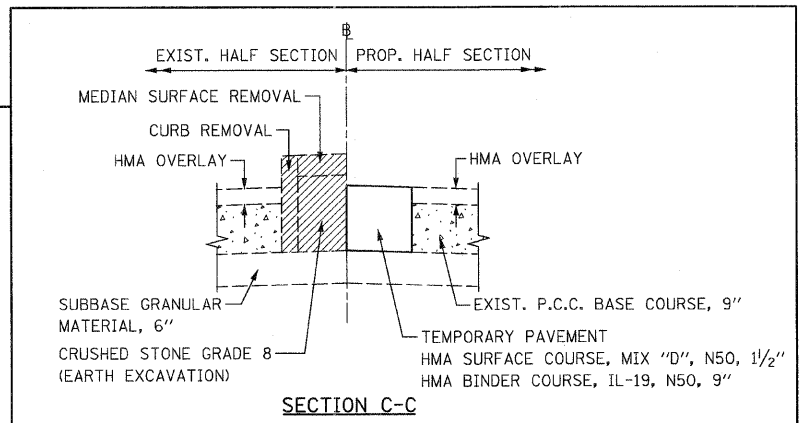
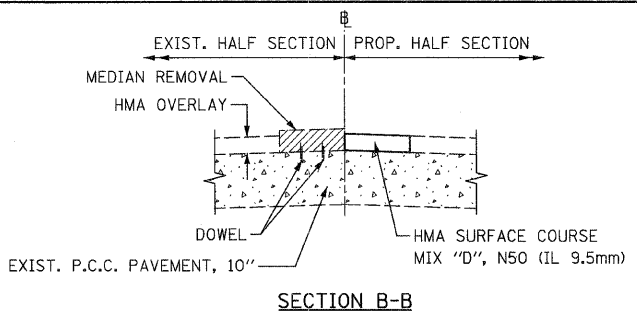
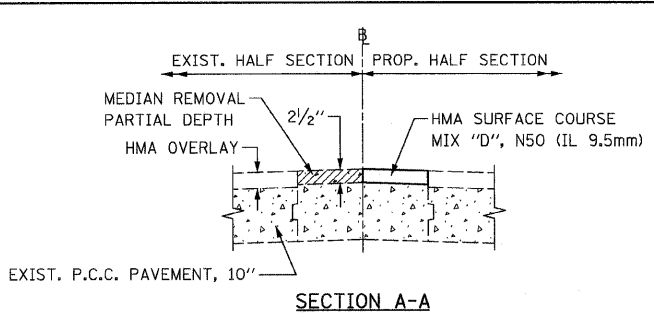
SCALE: 1" = 50' SHEET NO. 4 OF 4 SHEETS STA. 45+00 TO STA. 67+66

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2008-063 RS	COOK	42	11
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60F40	



- PLAN LEGEND**
- TEMPORARY CONCRETE BARRIER (ITEM 70400100)
 - TEMPORARY ATTENUATOR (ITEM Z0030255)
 - TRAFFIC DIRECTION ARROW
 - WORK ZONE
 - BARRICADES OR BARRELS AT 50' INTERVALS
 - EXISTING PAVEMENT MARKING REMOVAL
 - TEMPORARY SIGN

NOTE:
MOT-STAGE I SHALL BE PAID FOR IN ACCORDANCE WITH S.P. "WORK ZONE TRAFFIC CONTROL (LUMP SUM PAYMENT)"



- WORKERS OR FLAGGER *
W21-I(110)-48 (3) W20-I(101)-48 (3) * ONLY USED WHEN FLAGGER IS PRESENT
- LEFT LANE CLOSED AHEAD
W20-5L(O)-48 (2)
- ROAD WORK AHEAD
W20-I(O)-48 (1)

FILE NAME = D160F40-sht-staging1.dgn
PLOT DATE = 3/6/2009
CHRISTIAN-ROGE & ASSOCIATES, INC.
ENGINEERS-PLANNERS-SURVEYORS
211 WEST WACKER DRIVE
CHICAGO, ILLINOIS 60606
PHONE: (312)372-2023 FAX: (312)372-5274

DESIGNED - G.F.L.
DRAWN - B.K.
CHECKED - M.P.
DATE - MARCH 13, 2009

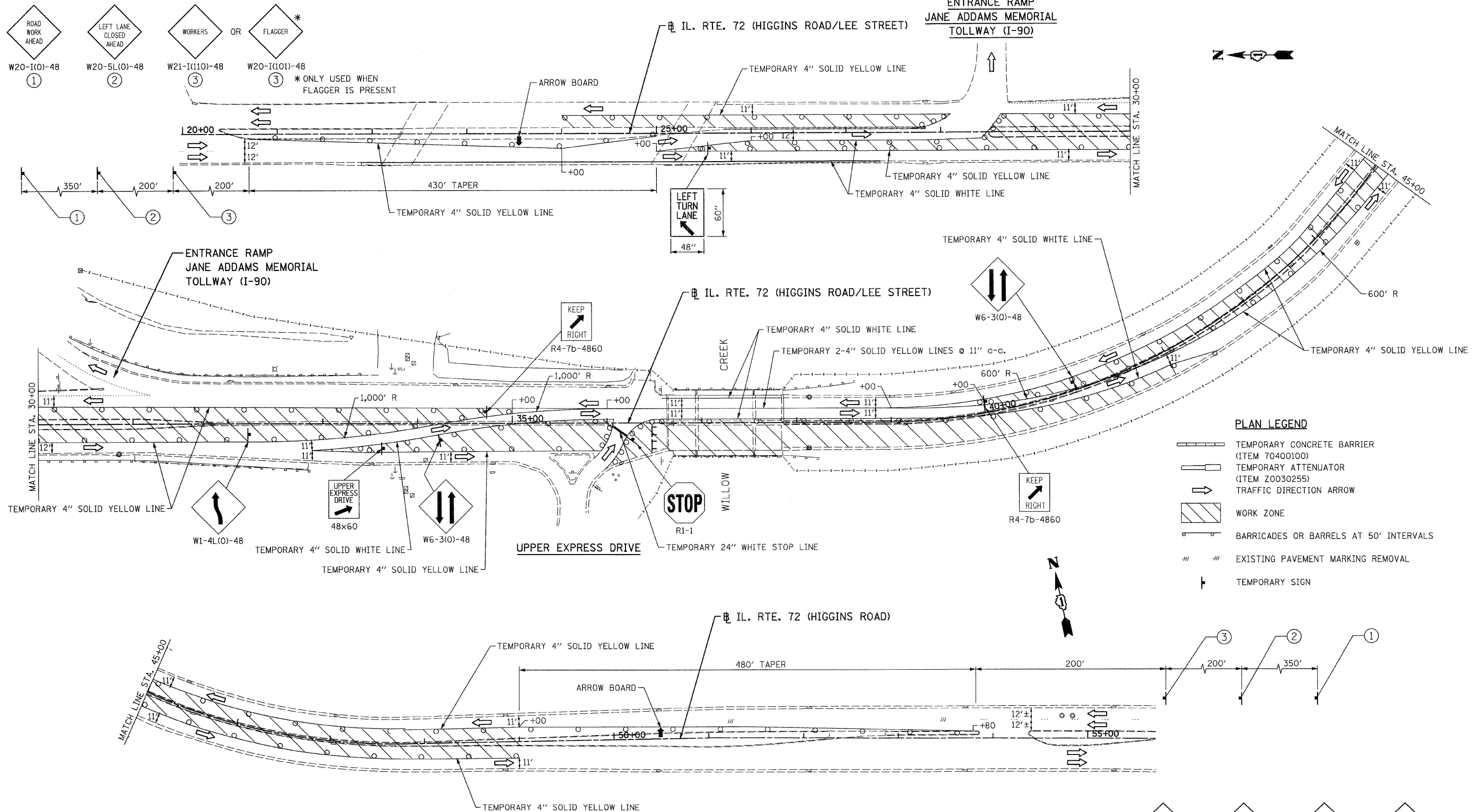
REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC - STAGE I
IL. RTE. 72 (TOUHY AVE. / HIGGINS RD.)**

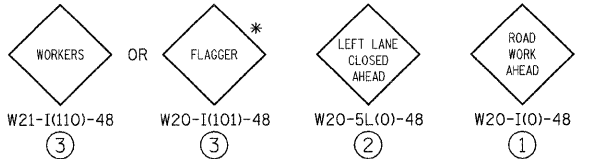
SCALE: 1" = 50' SHEET NO. 1 OF 2 SHEETS STA. 20+00 TO STA. 55+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2008-063 RS	COOK	42	12
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60F40	



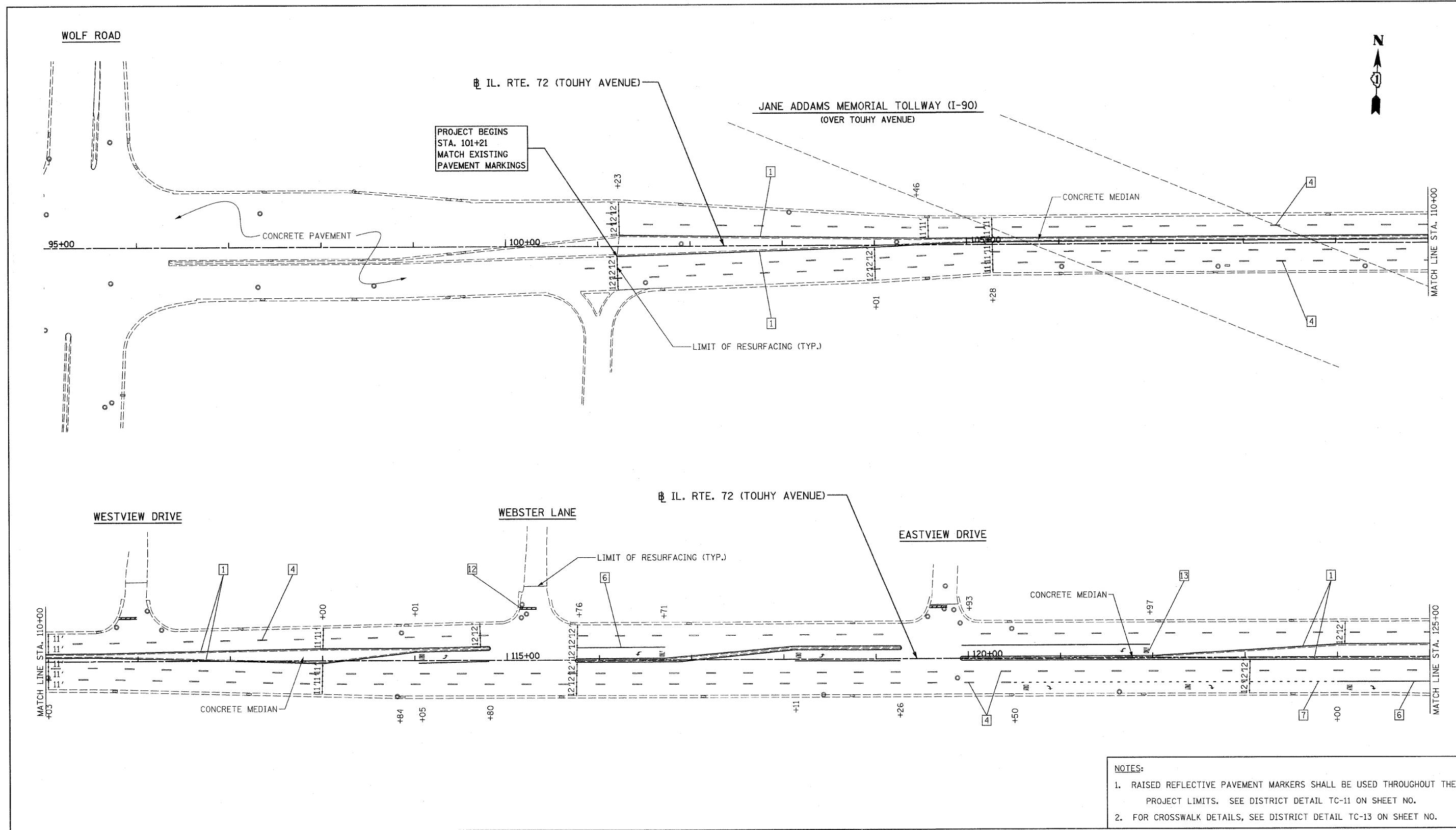
- PLAN LEGEND**
- TEMPORARY CONCRETE BARRIER (ITEM 70400100)
 - TEMPORARY ATTENUATOR (ITEM Z0030255)
 - TRAFFIC DIRECTION ARROW
 - WORK ZONE
 - BARRICADES OR BARRELS AT 50' INTERVALS
 - EXISTING PAVEMENT MARKING REMOVAL
 - TEMPORARY SIGN

NOTE:
 MOT-STAGE II SHALL BE PAID FOR IN ACCORDANCE WITH S.P. "WORK ZONE TRAFFIC CONTROL (LUMP SUM PAYMENT)"



* ONLY USED WHEN FLAGGER IS PRESENT

FILE NAME = D168F40-sht-staging2.dgn PLOT DATE = 3/6/2009	CHRISTIAN-ROGE & ASSOCIATES, INC. ENGINEERS-PLANNERS-SURVEYORS 211 WEST WACKER DRIVE CHICAGO, ILLINOIS 60606 PHONE: (312)372-2023 FAX: (312)372-5274	DESIGNED - G.F.L.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUGGESTED MAINTENANCE OF TRAFFIC - STAGE II IL. RTE. 72 (TOUHY AVE. / HIGGINS RD.)	F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEET NO.
		DRAWN - B.K.	REVISED -			1350	2008-063 RS	COOK	42 13
		CHECKED - M.P.	REVISED -			SCALE: 1" = 50'		SHEET NO. 2 OF 2 SHEETS	STA. 20+00 TO STA. 55+00
		DATE - MARCH 13, 2009	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		CONTRACT NO. 60F40	



NOTES:

1. RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE USED THROUGHOUT THE PROJECT LIMITS. SEE DISTRICT DETAIL TC-11 ON SHEET NO.
2. FOR CROSSWALK DETAILS, SEE DISTRICT DETAIL TC-13 ON SHEET NO.

LEGEND	
1	THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW SOLID LINE)
2	THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW SOLID LINES)
3	THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW - 10' DASH, 30' SKIP)
4	THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE LANE LINE - 10' DASH, 30')
5	THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE SOLID LINE)
6	THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE SOLID LINE)
7	THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE LANE LINE - 2' DASH, 6' SKIP)
8	THERMOPLASTIC PAVEMENT MARKING - LINE 8" (WHITE SOLID LINE)
9	THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE SOLID LINE)
10	THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE DIAGONAL LINE)
11	THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW DIAGONAL LINE)
12	THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE STOP BAR)
13	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (TYP.)

FILE NAME = D168F40-ah-tr-pml.dgn
 PLOT DATE = 3/6/2009

CHRISTIAN-ROGE & ASSOCIATES, INC.
 ENGINEERS-PLANNERS-SURVEYORS
 211 WEST WACKER DRIVE
 CHICAGO, ILLINOIS 60606
 PHONE: (312)372-2023 FAX: (312)372-5274

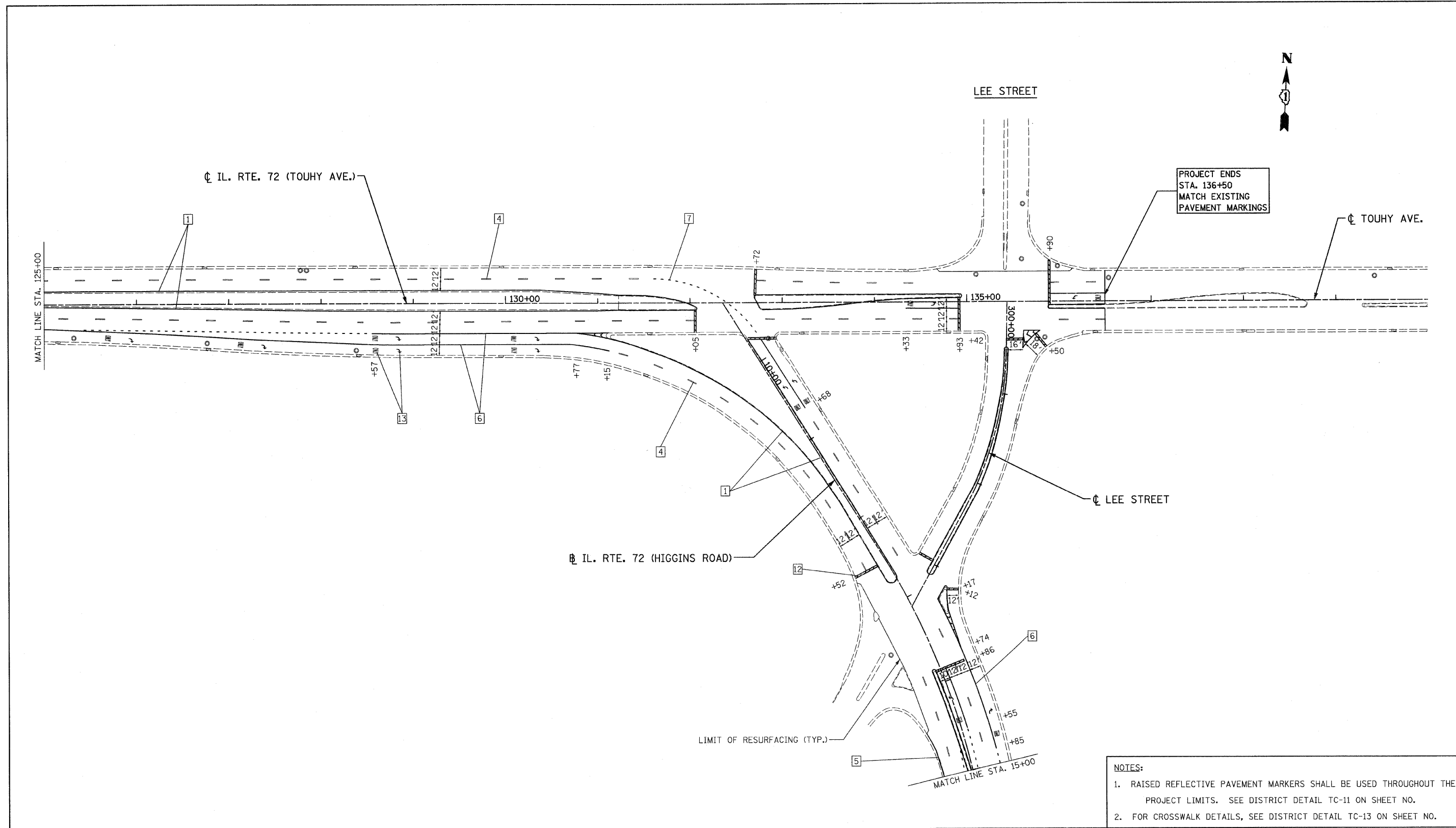
DESIGNED - G.F.L.	REVISED -
DRAWN - B.K.	REVISED -
CHECKED - M.P.	REVISED -
DATE - MARCH 13, 2009	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING PLAN
 IL. RTE. 72 (TOUHY AVE. / HIGGINS RD.)**

SCALE: 1" = 50' SHEET NO. 1 OF 4 SHEETS STA. 101+21 TO STA. 125+00

F.A.U. RTE. 1350	SECTION 2008-063 RS	COUNTY COOK	TOTAL SHEETS 42	SHEET NO. 14
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 60F40	

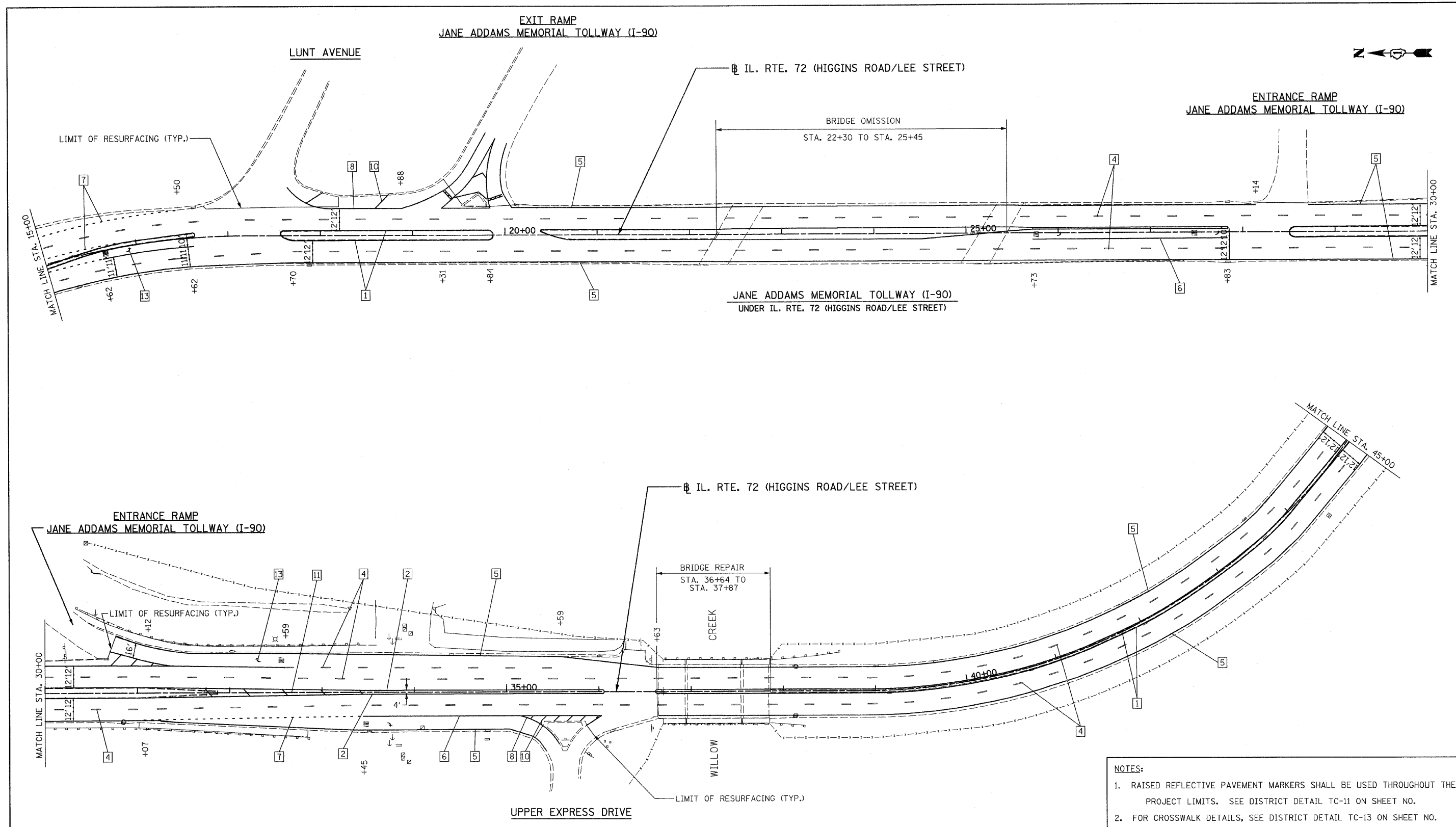


NOTES:

1. RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE USED THROUGHOUT THE PROJECT LIMITS. SEE DISTRICT DETAIL TC-11 ON SHEET NO.
2. FOR CROSSWALK DETAILS, SEE DISTRICT DETAIL TC-13 ON SHEET NO.

LEGEND									
1	THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW SOLID LINE)	6	THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE SOLID LINE)	10	THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE DIAGONAL LINE)				
2	THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW SOLID LINES)	7	THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE LANE LINE - 2' DASH, 6' SKIP)	11	THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW DIAGONAL LINE)				
3	THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW - 10' DASH, 30' SKIP)	8	THERMOPLASTIC PAVEMENT MARKING - LINE 8" (WHITE SOLID LINE)	12	THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE STOP BAR)				
4	THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE LANE LINE - 10' DASH, 30')	9	THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE SOLID LINE)	13	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (TYP.)				
5	THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE SOLID LINE)								

FILE NAME = 0160F40-shit-pm2.dgn		DESIGNED - G.F.L.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING PLAN IL. RTE. 72 (TOUHY AVE. / HIGGINS RD.)	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT DATE = 3/6/2009		DRAWN - B.K.	REVISED -			1350	2008-063 RS	COOK	42	15
	CHECKED - M.P.	REVISED -	CONTRACT NO. 60F40							
	DATE - MARCH 13, 2009	REVISED -	SCALE: 1" = 50'			SHEET NO. 2 OF 4 SHEETS	STA. 125+00 TO STA. 15+00	FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT	



NOTES:

1. RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE USED THROUGHOUT THE PROJECT LIMITS. SEE DISTRICT DETAIL TC-11 ON SHEET NO.
2. FOR CROSSWALK DETAILS, SEE DISTRICT DETAIL TC-13 ON SHEET NO.

LEGEND			
1	THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW SOLID LINE)	10	THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE DIAGONAL LINE)
2	THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW SOLID LINES)	7	THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE LANE LINE - 2' DASH, 6' SKIP)
3	THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW - 10' DASH, 30' SKIP)	8	THERMOPLASTIC PAVEMENT MARKING - LINE 8" (WHITE SOLID LINE)
4	THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE LANE LINE - 10' DASH, 30')	9	THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE SOLID LINE)
5	THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE SOLID LINE)	11	THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW DIAGONAL LINE)
		12	THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE STOP BAR)
		13	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (TYP.)

FILE NAME = D160F40-sh1-tr-pm3.dgn
 PLOT DATE = 3/6/2009

CHRISTIAN-ROGE & ASSOCIATES, INC.
 ENGINEERS-PLANNERS-SURVEYORS
 211 WEST WACKER DRIVE
 CHICAGO, ILLINOIS 60606
 PHONE: (312)372-2023 FAX: (312)372-5274

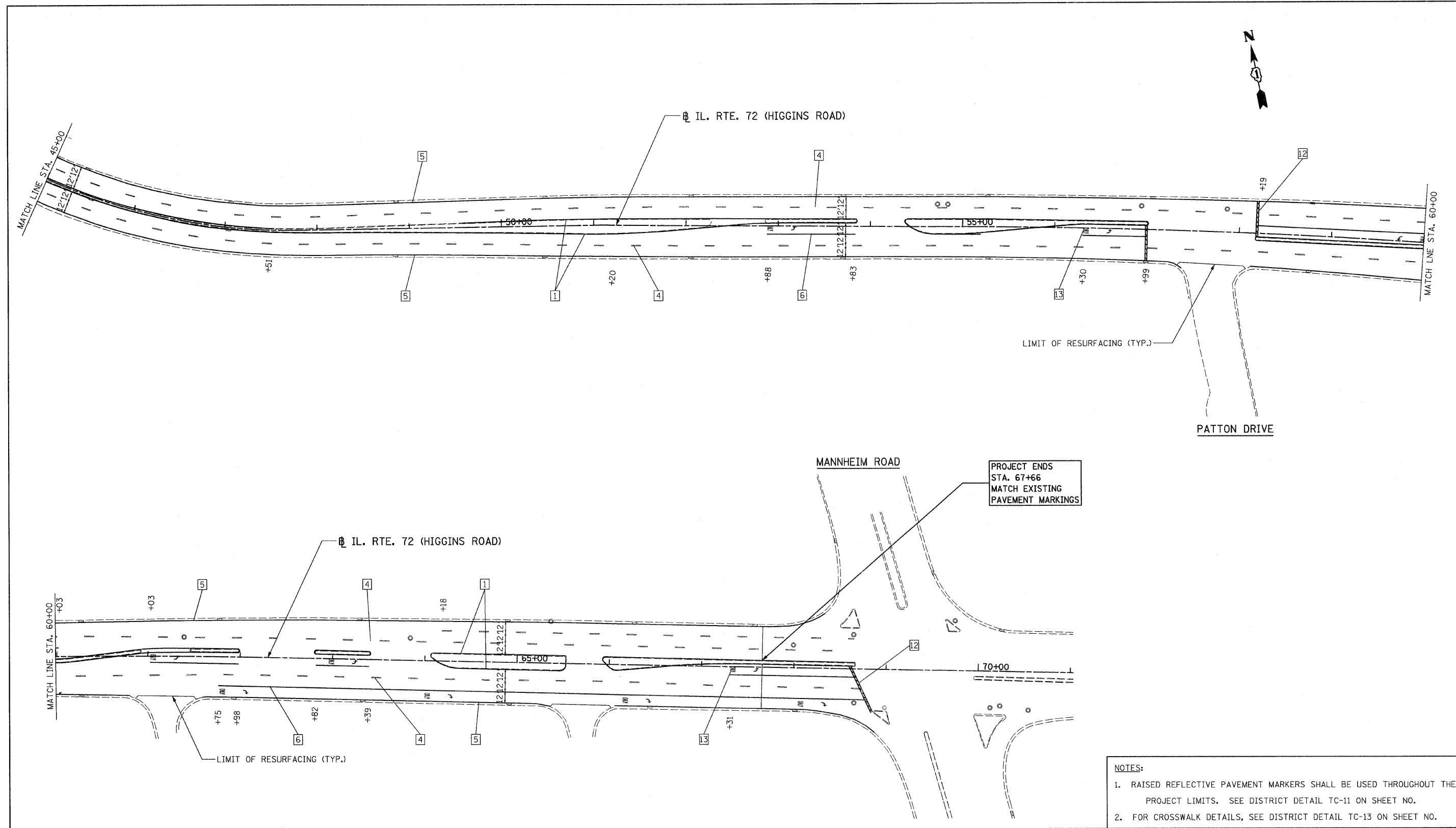
DESIGNED -	G.F.L.	REVISED -	
DRAWN -	B.K.	REVISED -	
CHECKED -	M.P.	REVISED -	
DATE -	MARCH 13, 2009	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING PLAN
IL. RTE. 72 (TOUHY AVE. / HIGGINS RD.)

SCALE: 1" = 50' SHEET NO. 3 OF 4 SHEETS STA. 15+00 TO STA. 45+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2008-063 RS	COOK	42	16
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 60F40	



NOTES:

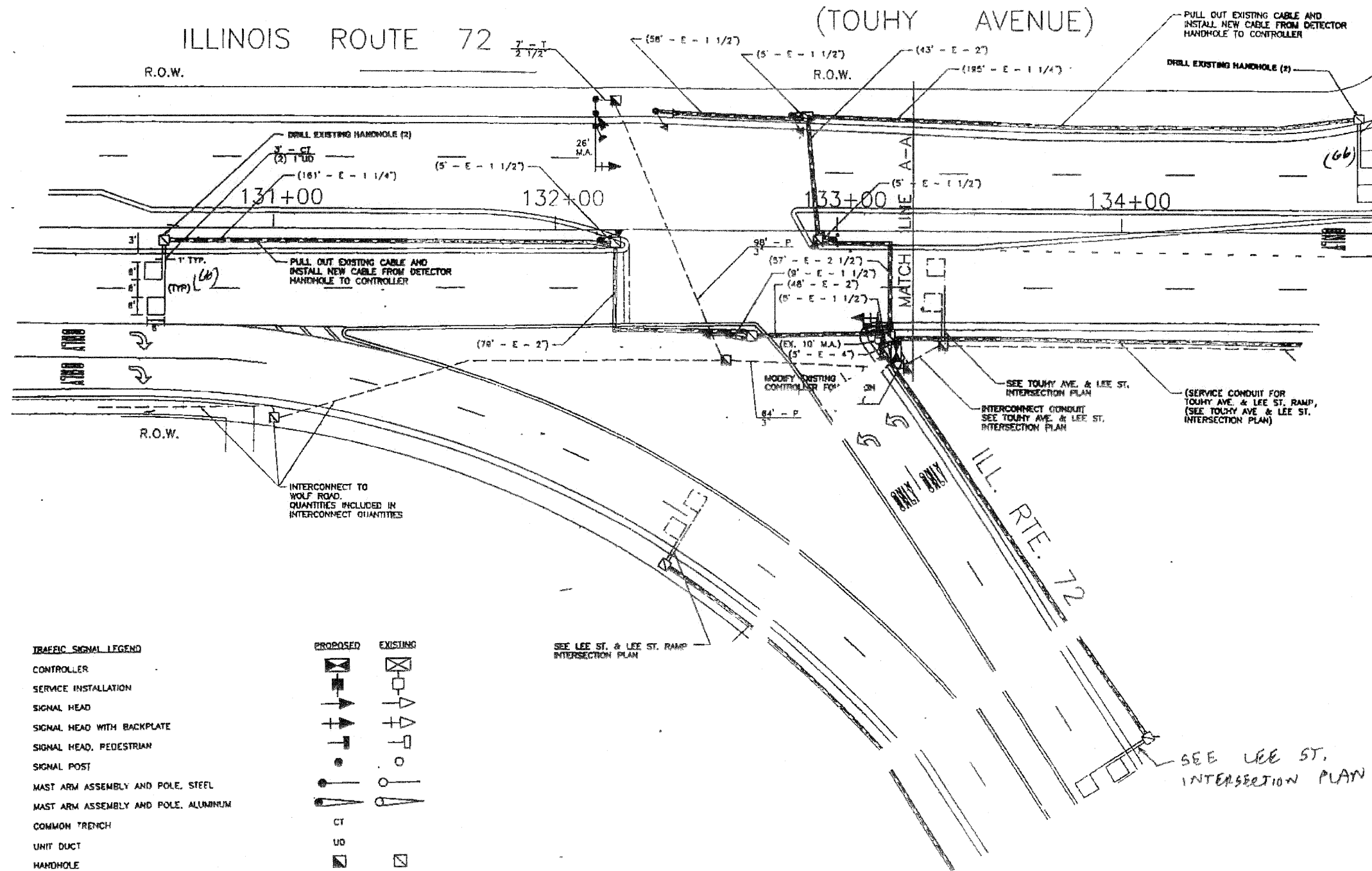
1. RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE USED THROUGHOUT THE PROJECT LIMITS. SEE DISTRICT DETAIL TC-11 ON SHEET NO.
2. FOR CROSSWALK DETAILS, SEE DISTRICT DETAIL TC-13 ON SHEET NO.

LEGEND			
1	THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW SOLID LINE)	10	THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE DIAGONAL LINE)
2	THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW SOLID LINES)	11	THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW DIAGONAL LINE)
3	THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW - 10' DASH, 30' SKIP)	12	THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE STOP BAR)
4	THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE LANE LINE - 10' DASH, 30')	13	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (TYP.)
5	THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE SOLID LINE)		
6	THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE SOLID LINE)		
7	THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE LANE LINE - 2' DASH, 6' SKIP)		
8	THERMOPLASTIC PAVEMENT MARKING - LINE 8" (WHITE SOLID LINE)		
9	THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE SOLID LINE)		

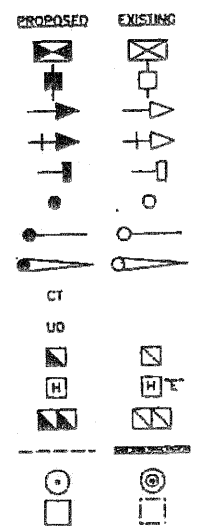
FILE NAME = D:\B\F\40-shr-pm4.dgn PLOT DATE = 3/6/2009	CHRISTIAN-ROGE & ASSOCIATES, INC. ENGINEERS-PLANNERS-SURVEYORS 211 WEST WACKER DRIVE CHICAGO, ILLINOIS 60606 PHONE: (312)372-2023 FAX: (312)372-5274	DESIGNED - G.F.L.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING PLAN IL. RTE. 72 (TOUHY AVE. / HIGGINS RD.)	F.A.U. RTE. 1350	SECTION 2008-063 RS	COUNTY COOK	TOTAL SHEETS 42	SHEET NO. 17
		DRAWN - B.K.	REVISED -			SCALE: 1" = 50'		SHEET NO. 4 OF 4 SHEETS	STA. 45+00	TO STA. 67+66
		CHECKED - M.P.	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
		DATE - MARCH 13, 2009	REVISED -							

P.A. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2008-063	Cook	42	18
STA		TO STA		
FED. ROAD DIST. NO. 7		ILLINOIS		PER AIR PROJECT

CONTRACT NO. 60F40



- TRAFFIC SIGNAL LEGEND**
- CONTROLLER
 - SERVICE INSTALLATION
 - SIGNAL HEAD
 - SIGNAL HEAD WITH BACKPLATE
 - SIGNAL HEAD, PEDESTRIAN
 - SIGNAL POST
 - MAST ARM ASSEMBLY AND POLE, STEEL
 - MAST ARM ASSEMBLY AND POLE, ALUMINUM
 - COMMON TRENCH
 - UNIT DUCT
 - HANDHOLE
 - HEAVY DUTY HANDHOLE
 - DOUBLE HANDHOLE
 - G.S. CONDUIT IN TRENCH OR PUSHED
 - PEDESTRIAN PUSHBUTTON DETECTOR
 - DETECTOR LOOP



REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
80500600	132	Foot	Detector Loop Replacement

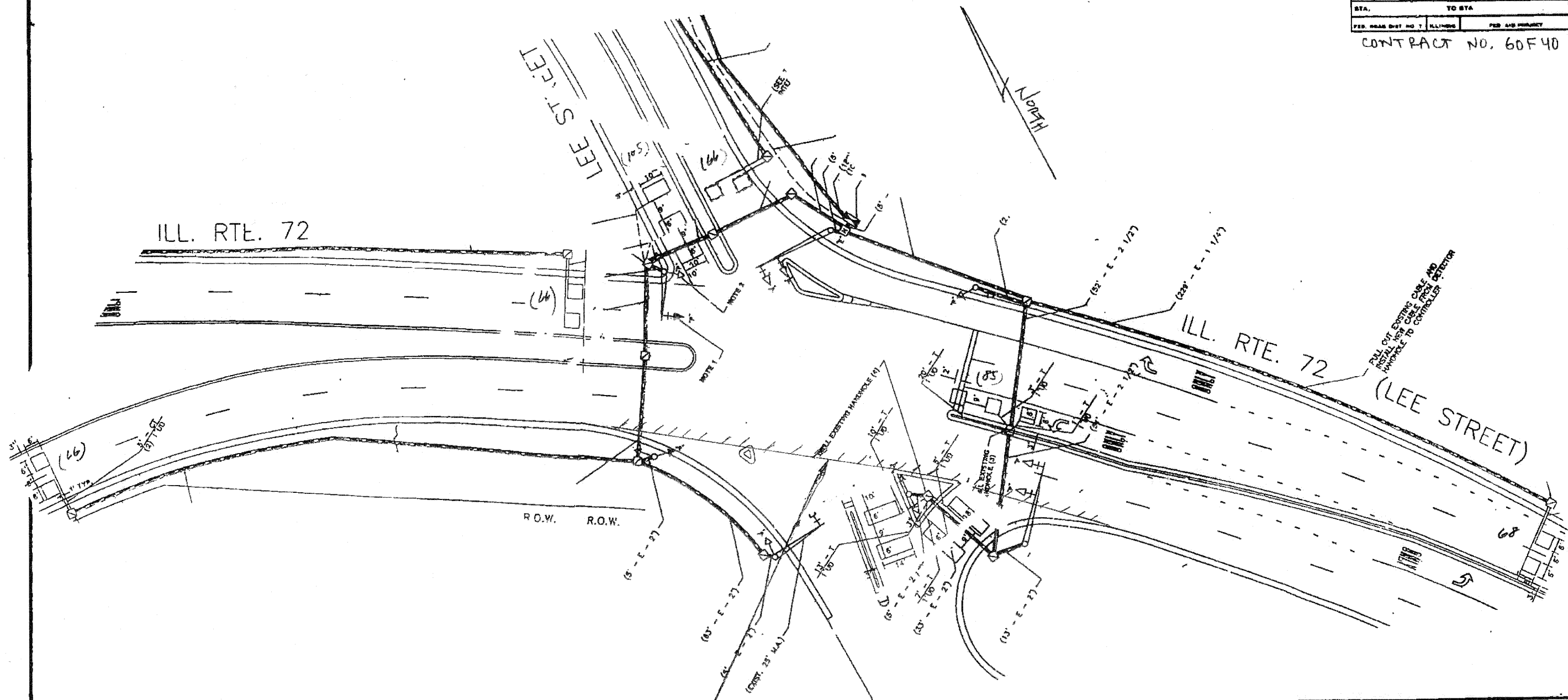
REVISIONS	
NAME	DATE

NOTE:
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
ILL. RTE. 72 @ TOUHY AVE
SCALE: NONE
DATE: NOV. 08
DRAWN BY: JHE
DESIGNED BY: JHE
CHECKED BY: DAD

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350 2008-063 RB	COOK	42	19
STA. TO STA.		PER. DRAW. DIST. NO. 7	
		ILLINOIS PER. AND PROJECT	

CONTRACT NO. 60F40



RESURFACING LIMITS

REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

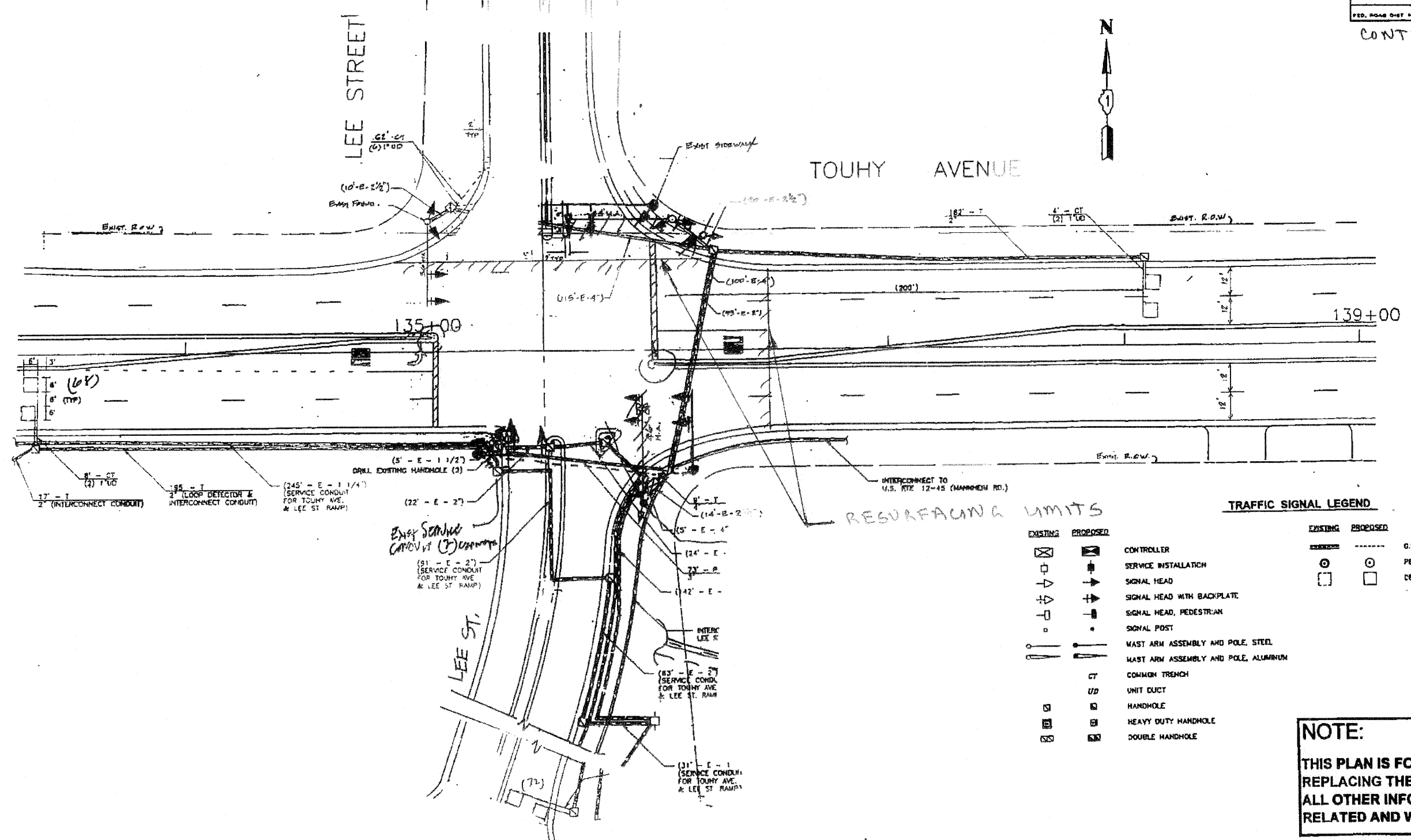
CODE NO.	QUANTITY	UNIT	ITEM
80600600	456	Foot	Detector Loop Replacement

REVISIONS	
NAME	DATE

NOTE:
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
ILL. RTE. 72 (HIGGINS) @ LEE STREET
SCALE: NONE
DATE: Nov. 2008
DRAWN BY: JHE
DESIGNED BY: JHE
CHECKED BY: DAB

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350 2.008-063R6	COOK	42	20
STA.		TO STA.	
FED. ROAD DIST. NO. 7 ILLINOIS		JOB AND PROJECT	
CONTRACT NO. 60F40			



TRAFFIC SIGNAL LEGEND

EXISTING	PROPOSED	DESCRIPTION
⊗	⊗	G.S. CONDUIT IN TRENCH OR PUSHED
⊠	⊠	PEDESTRIAN PUSHBUTTON DETECTOR
⊡	⊡	DETECTOR LOOP

EXISTING	PROPOSED	DESCRIPTION
⊠	⊠	CONTROLLER
⊠	⊠	SERVICE INSTALLATION
⊠	⊠	SIGNAL HEAD
⊠	⊠	SIGNAL HEAD WITH BACKPLATE
⊠	⊠	SIGNAL HEAD, PEDESTRIAN
⊠	⊠	SIGNAL POST
⊠	⊠	MAST ARM ASSEMBLY AND POLE, STEEL
⊠	⊠	MAST ARM ASSEMBLY AND POLE, ALUMINUM
CT	CT	COMMON TRENCH
UD	UD	UNIT DUCT
⊠	⊠	HANDHOLE
⊠	⊠	HEAVY DUTY HANDHOLE
⊠	⊠	DOUBLE HANDHOLE

NOTE:
 THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

REPLACE ALL DETECTOR LOOPS AS SHOWN
 (WITHIN THE RESURFACING LIMITS)

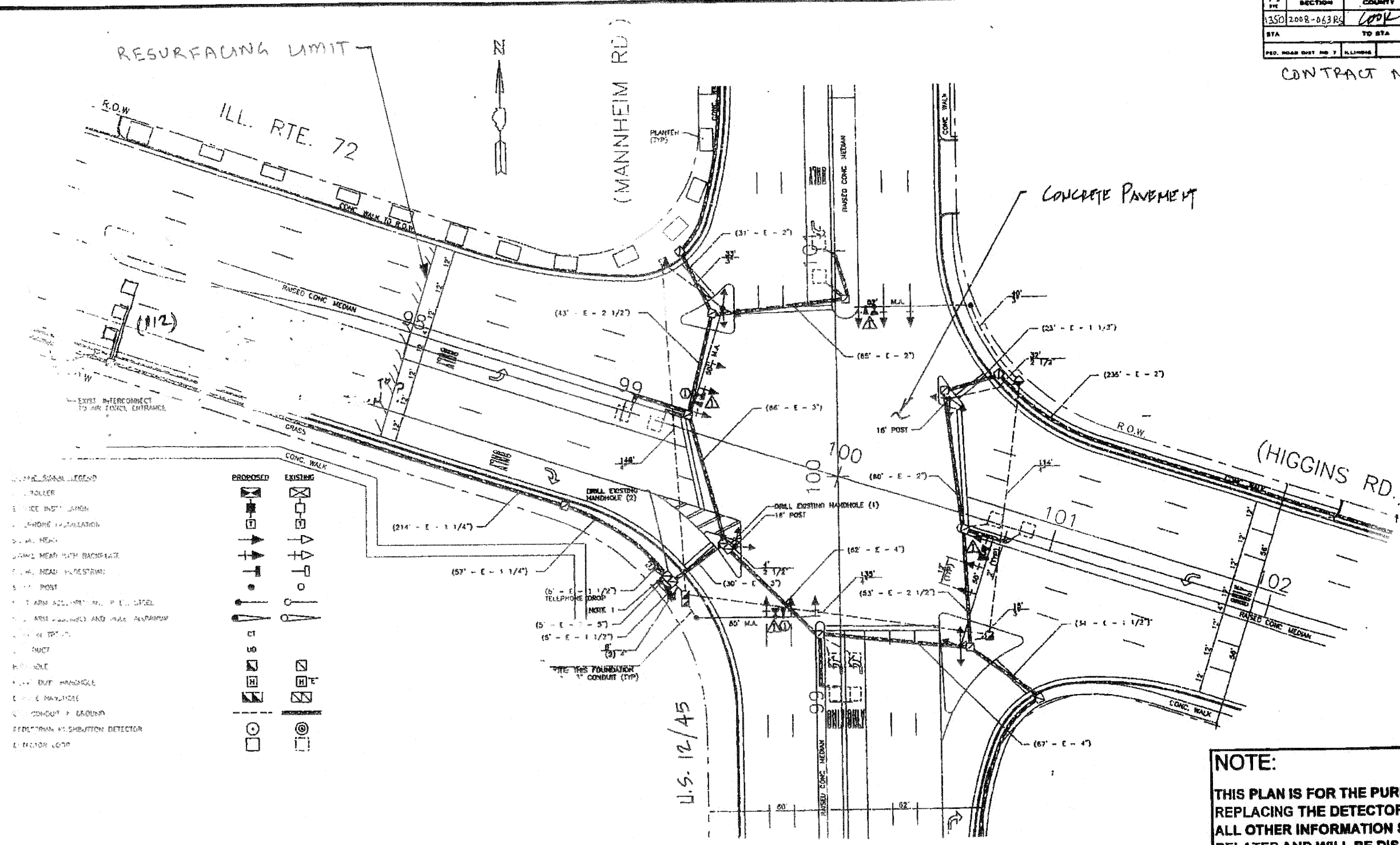
CODE NO.	QUANTITY	UNIT	ITEM
80600600	140	Foot	Detector Loop Replacement

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
 TOUHY AVE. @ LEE ST.
 SCALE: NONE
 DATE: Nov. 08
 DRAWN BY: JHE
 DESIGNED BY: JHE
 CHECKED BY: DAD

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350 2008-063 RS	COOK	42	21
STA		TO STA	
PED. ROAD DIST NO. 7		PED. AND PROJECT	

CONTRACT NO. 60F40



SYMBOL	PROPOSED	EXISTING
ROLLER	[Symbol]	[Symbol]
EDGE IMP. LAMP	[Symbol]	[Symbol]
PHONE INSTALLATION	[Symbol]	[Symbol]
CONC. MED.	[Symbol]	[Symbol]
DRIVE MED. WITH BACKPLATE	[Symbol]	[Symbol]
CONC. MED. RESTRICTOR	[Symbol]	[Symbol]
18" POST	[Symbol]	[Symbol]
1" ARM ACCUMULATOR P.E. LEVEL	[Symbol]	[Symbol]
1" ARM SIGNALS AND WALK SIGNATURE	[Symbol]	[Symbol]
1" SIGNAL	[Symbol]	[Symbol]
HANDHOLE	[Symbol]	[Symbol]
1" DUT. HANDHOLE	[Symbol]	[Symbol]
1" CONDUIT	[Symbol]	[Symbol]
1" CONDUIT IN GROUND	[Symbol]	[Symbol]
1" DETECTOR DISTRIBUTION DETECTOR	[Symbol]	[Symbol]
1" DETECTOR LOOP	[Symbol]	[Symbol]

REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
82600800	112	Foot	Detector Loop Replacement

NOTE:
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

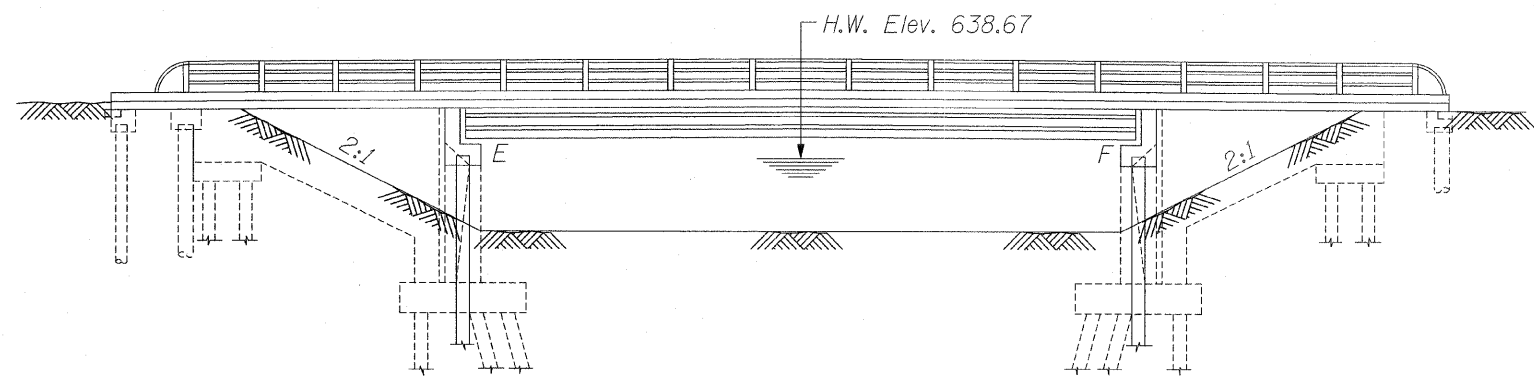
ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT

U.S. 12/45 (MANNHEIM) @ ILL. RTE. 72 (HIGGINS)

SCALE: NONE
DATE: Nov. 2008

DRAWN BY: JRE
DESIGNED BY: JRE
CHECKED BY: DAD

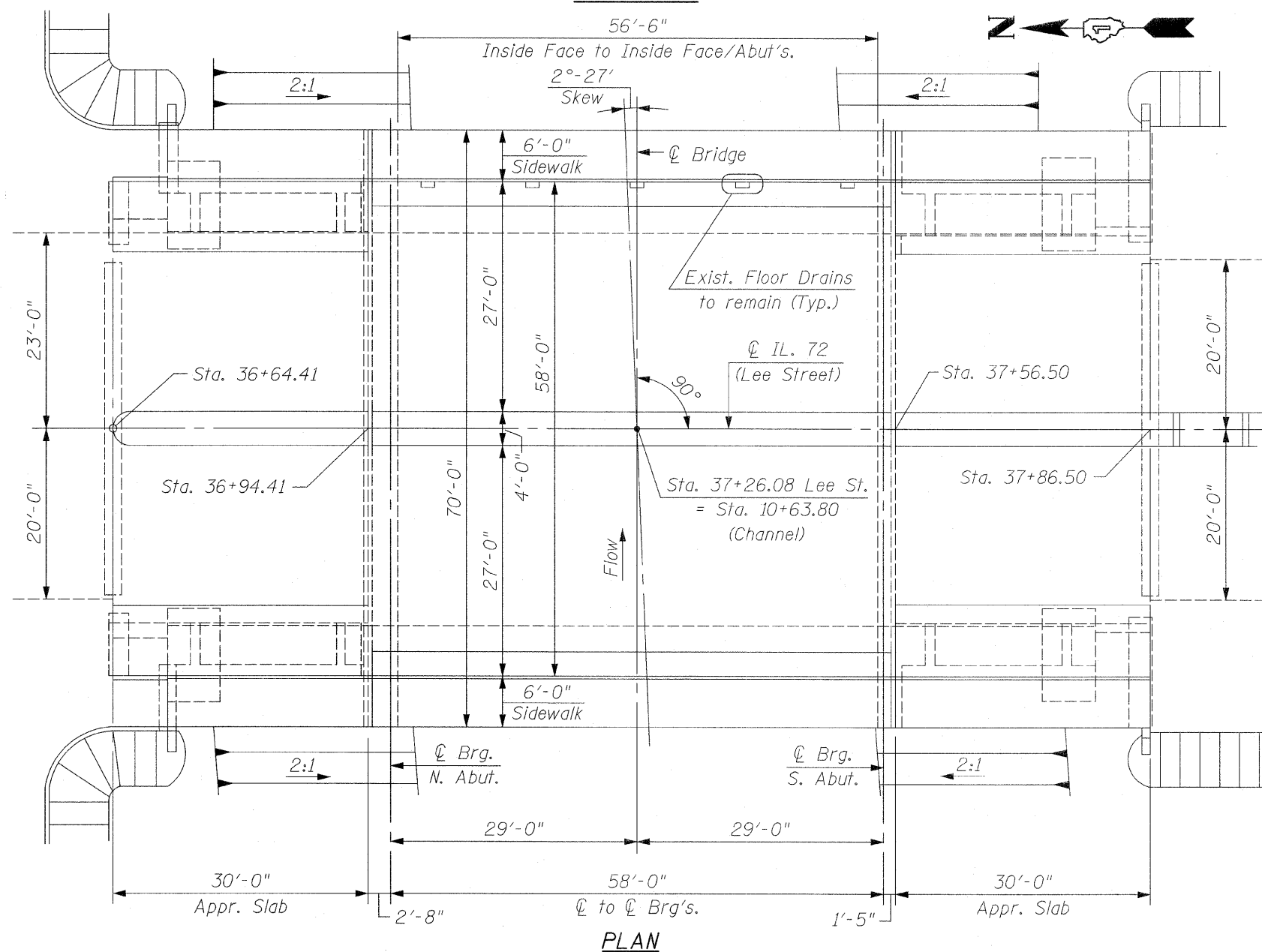
REVISIONS	
NAME	DATE



NORTH ABUTMENT

ELEVATION

SOUTH ABUTMENT



PLAN

GENERAL NOTES:

Plan dimensions and details relative to existing plans are subject to Nominal Construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Joint Openings Shall be adjusted according to Article 520.04 of the Standard Specifications when the Deck is poured at an ambient temperature other than 50°F.

Existing Reinforcement Bars extending into the removal areas shall be cleaned, straightened and incorporated into the New Construction. Any Reinforcement Bars that are damaged during Concrete removal shall be replaced with an approved Bar Splicer or Anchorage System. Cost is included with Concrete Removal.

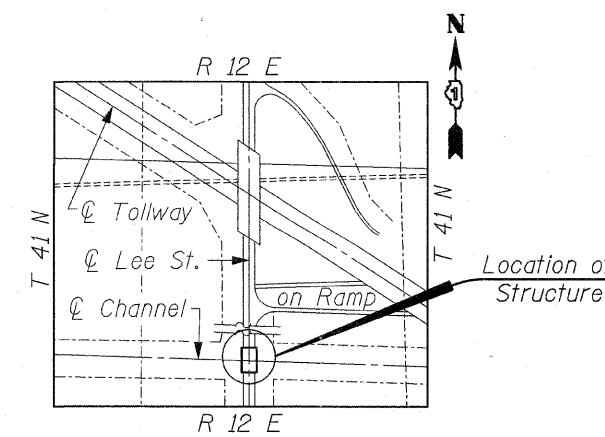
The Contractor shall use extreme care during Concrete removal so as not to damage the P.P.C. I-Beams. Any damage done to the P.P.C. I-Beams is to be repaired at the Contractor's expense.

Reinforcement Bars shall conform to the requirements of ASTM A-706, Grade 60. See Special Provisions.

Reinforcement Bars designated (E) shall be Epoxy Coated.

TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Hot-Mix Asphalt Surface Removal (Complete)	Sq. Yd.	347
Bridge Deck Hydro-Scarification, 1/2 Inch	Sq. Yd.	311
Bridge Deck Latex Concrete Overlay, 2 1/4 Inches	Sq. Yd.	311
Bridge Deck Grooving	Sq. Yd.	330
Protective Coat	Sq. Yd.	484
Reinforcement Bars, Epoxy Coated	Pound	8,020
Concrete Removal	Cu. Yd.	47.7
Concrete Superstructure	Cu. Yd.	48.6
Preformed Joint Strip Seal	Foot	140
Bar Splicers	Each	68
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	12
Structural Repair of Concrete (Depth Greater Than 5 in.)	Sq. Ft.	232
Structural Repair of Concrete (Depth Equal to or Less Than 5 in.)	Sq. Ft.	11



LOCATION SKETCH



Bhadresh N. Shah
03/23/2009
BHADRESH N. SHAH
LICENSED STRUCTURAL ENGINEER
STATE OF ILLINOIS LIC. No. 081-004476
EXPIRES: 11-30-10

FILE NAME = plan_elevation.dgn
PLOT DATE = 3/23/2009

CHRISTIAN-ROGE & ASSOCIATES, INC.
ENGINEERS-PLANNERS-SURVEYORS
211 WEST WACKER DRIVE
CHICAGO, ILLINOIS 60606
PHONE: (312)372-2023 FAX: (312)372-5274

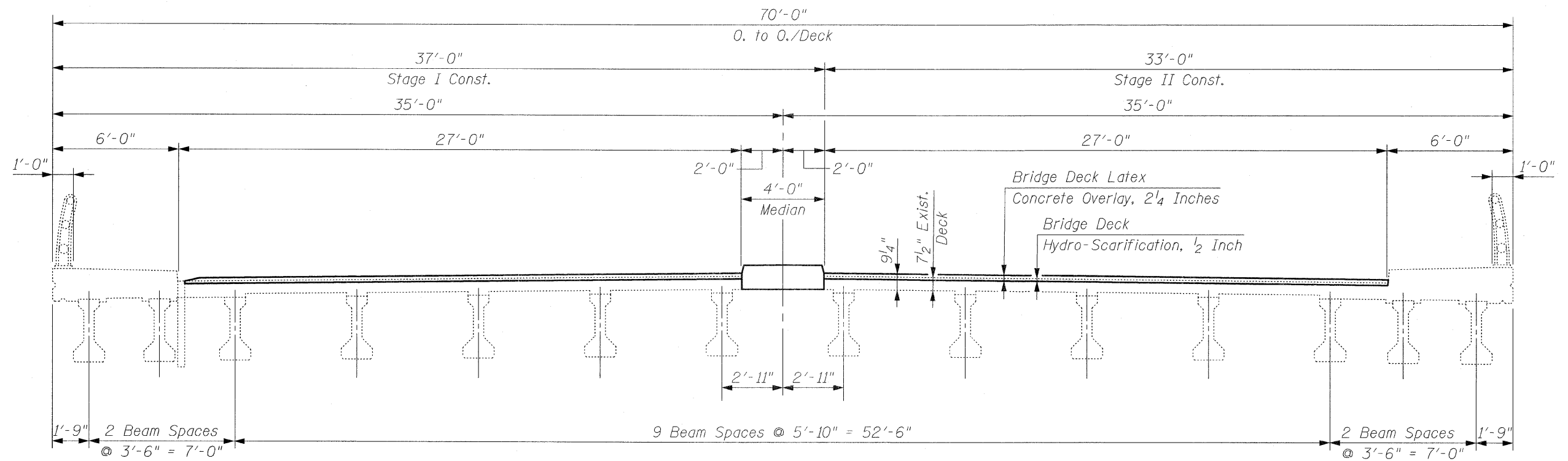
DESIGNED - B.N.S./J.C.N.	REVISED -
DRAWN - F.M.	REVISED -
CHECKED - B.N.S./J.C.N.	REVISED -
DATE - MARCH, 2009	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

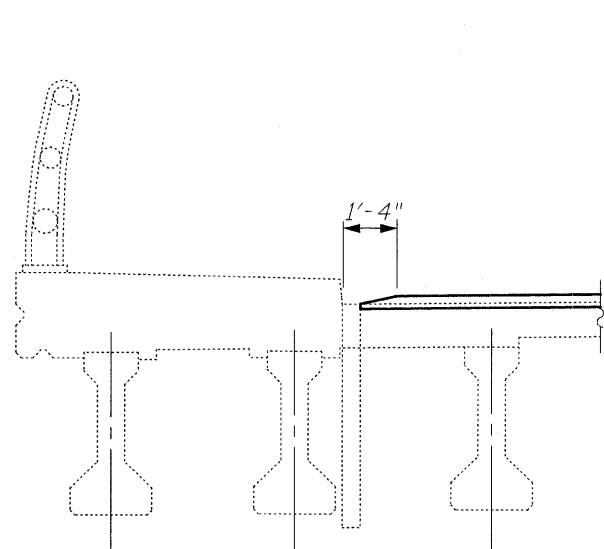
**GENERAL PLAN & ELEVATION
IL 72 (LEE STREET) OVER WILLOW CREEK S.N. 016-0506**

SCALE: SHEET NO. S1 OF S10 SHEETS STA. TO STA.

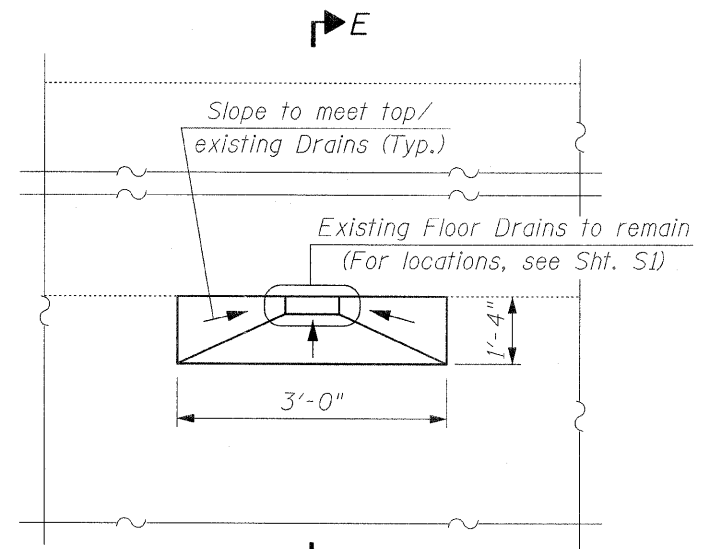
F.A.I. RTE. 1350	SECTION 2008-063 RS	COUNTY COOK	TOTAL SHEETS 42	SHEET NO. 22
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 60F40	



DECK CROSS SECTION
(Looking South)



SECTION E-E



DETAIL "A"

PLAN
(Showing Overlay taper at Existing Floor Drains)

FILE NAME = deck_cross_section.dgn
PLOT DATE = 3/23/2009

CHRISTIAN-ROGE & ASSOCIATES, INC.
ENGINEERS-PLANNERS-SURVEYORS
211 WEST WACKER DRIVE
CHICAGO, ILLINOIS 60606
PHONE: (312)372-2023 FAX: (312)372-5274

DESIGNED - B.N.S./J.C.N.	REVISED -
DRAWN - F.M.	REVISED -
CHECKED - B.N.S./J.C.N.	REVISED -
DATE - MARCH, 2009	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

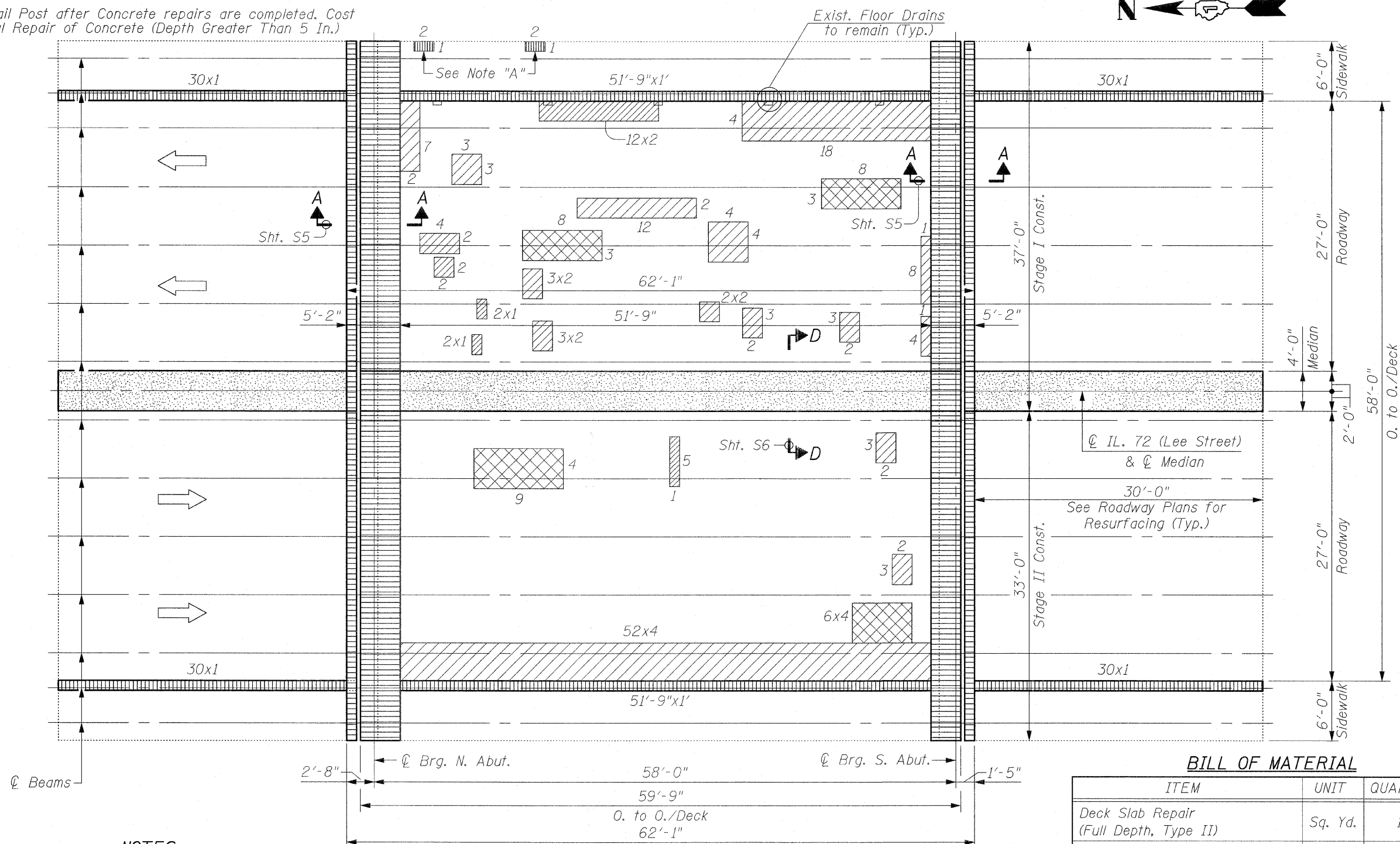
DECK CROSS SECTION
IL. 72 (LEE STREET) OVER WILLOW CREEK S.N. 016-0506

SCALE: SHEET NO. S2 OF S10 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2008-063 RS	COOK	42	23
CONTRACT NO. 60F40				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

NOTE "A"

Re-install the Existing Rail Post after Concrete repairs are completed. Cost is included with Structural Repair of Concrete (Depth Greater Than 5 In.)



DECK PLAN

NOTES:

For Joint Reconstruction Details, See Sht.'s. S4 and S5.

For Median Replacement Details, See Sht. S6.

Deck Slab Repair (Partial) is shown for Information Only.

Areas of Deck Repairs shown are estimated. The Engineer shall show actual locations of Deck Repairs on As-Built Plans.

Proposed Overlay to be tapered at Existing Floor Drains, see Detail "A", Sht. S2

All Patch dimensions are in feet.

LEGEND:

- Deck Slab Repair (Full Depth, Type II)
- Deck Slab Repair (Partial)
- Joint Reconstruction
- Median Replacement
- Structural Repair of Concrete (Depth Greater Than 5 In.)

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	12
Structural Repair of Concrete (Depth Greater Than 5 In.)	Sq. Ft.	224

NOTES:

INSPECTION WAS CONDUCTED IN SEPT., 2008 BY I.D.O.T. FIELD MAINTENANCE ENGINEERS.

INFRARED INSPECTION WAS CONDUCTED ON JULY 10, 2008 BY EARTH TECH/AECOM.

NOTE:

SURFACE TYPE: HOT-MIX ASPHALT OVERLAY

FILE NAME = deck_repairs.dgn
PLOT DATE = 3/23/2009

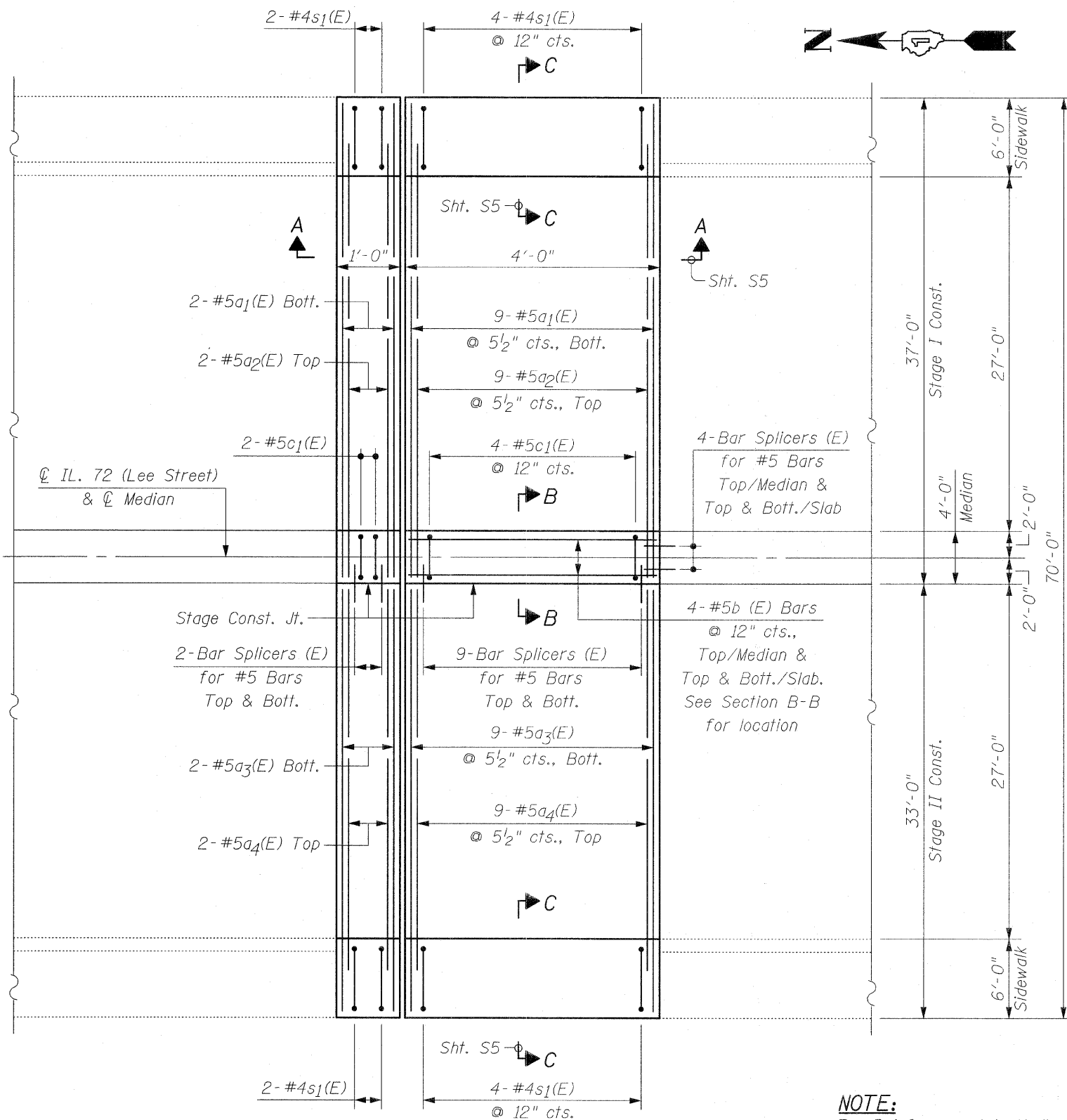
CHRISTIAN-ROGE & ASSOCIATES, INC.
ENGINEERS-PLANNERS-SURVEYORS
211 WEST WACKER DRIVE
CHICAGO, ILLINOIS 60606
PHONE: (312)372-2023 FAX: (312)372-5274

DESIGNED - B.N.S./J.C.N.	REVISED -
DRAWN - F.M.	REVISED -
CHECKED - B.N.S./J.C.N.	REVISED -
DATE - MARCH, 2009	REVISED -

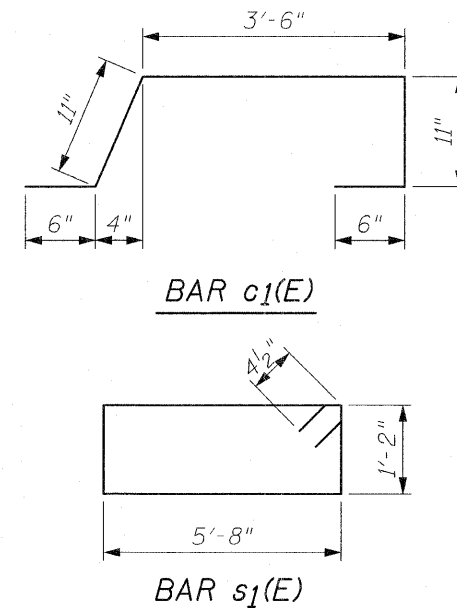
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DECK PLAN
IL. 72 (LEE STREET) OVER WILLOW CREEK S.N. 016-0506
SCALE: SHEET NO. S3 OF S10 SHEETS STA. TO STA.

F.A.I. RTE. 1350	SECTION 2008-063 RS	COUNTY COOK	TOTAL SHEETS 42	SHEET NO. 24
FED. ROAD DIST. NO. ILLINOIS			FED. AID PROJECT	
CONTRACT NO. 60F40				



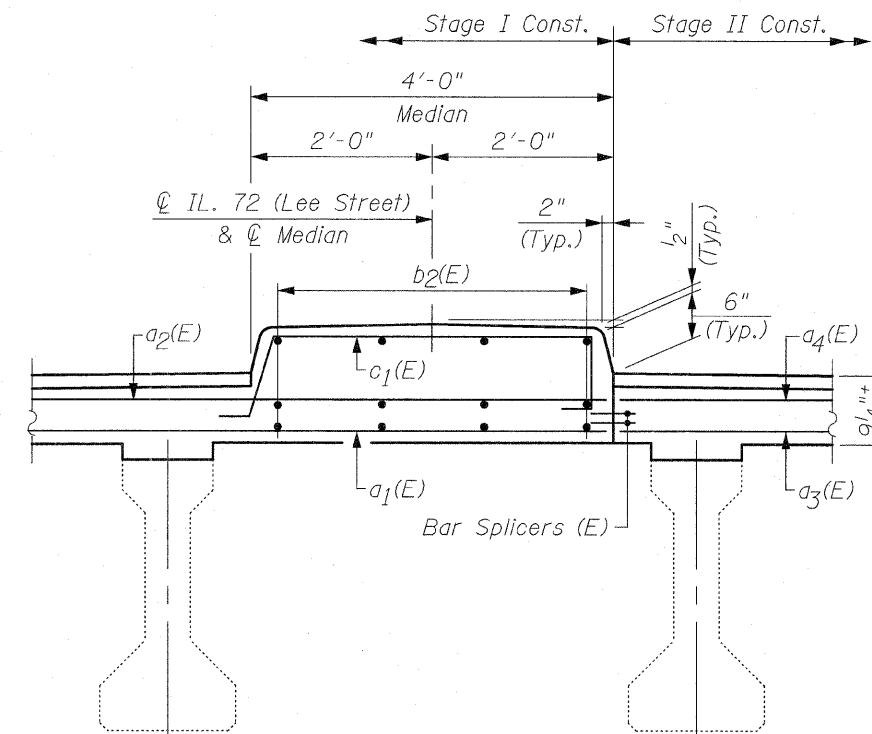
PLAN AT NORTH ABUTMENT
(South Abutment similar by 180° rotation)



BILL OF MATERIAL
(for one Abutment)

Bar	No.	Size	Length	Shape
a1(E)	11	#5	36'-8"	—
a2(E)	11	#5	33'-0"	—
a3(E)	11	#5	32'-8"	—
a4(E)	11	#5	29'-0"	—
b2(E)	12	#5	3'-10"	—
c1(E)	6	#5	6'-4"	U
s1(E)	12	#4	14'-5"	Rect
Reinforcement Bars, Epoxy Coated		Pound	1,710	
Concrete Removal		Cu. Yd.	12.7	
Concrete Superstructure		Cu. Yd.	13.8	
Bar Splicers		Each	34	

NOTE:
For Reinforcement in Median & for Bar Splicers (E) in Median, see Sht. S6



SECTION B-B

FILE NAME = jt_reconst_north.dgn
PLOT DATE = 3/23/2009

CHRISTIAN-ROGE & ASSOCIATES, INC.
ENGINEERS-PLANNERS-SURVEYORS
211 WEST WACKER DRIVE
CHICAGO, ILLINOIS 60606
PHONE: (312)372-2023 FAX: (312)372-5274

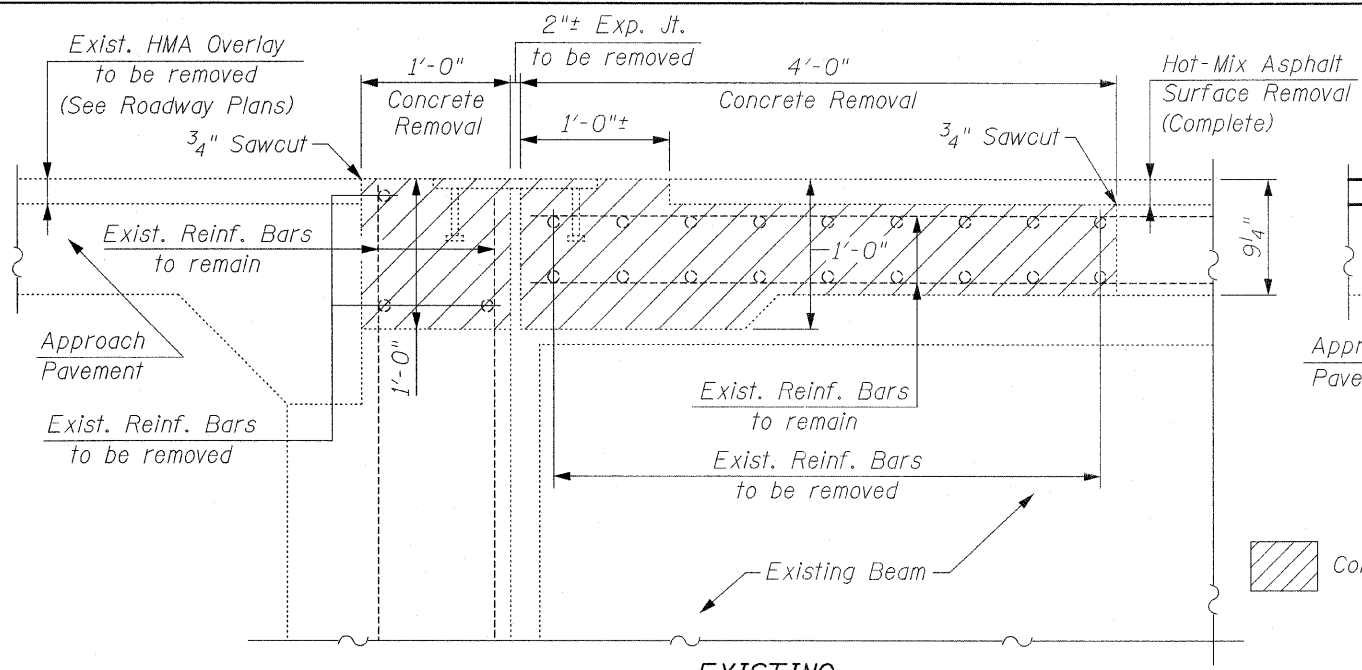
DESIGNED - B.N.S./J.C.N.	REVISED -
DRAWN - F.M.	REVISED -
CHECKED - B.N.S./J.C.N.	REVISED -
DATE - MARCH, 2009	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

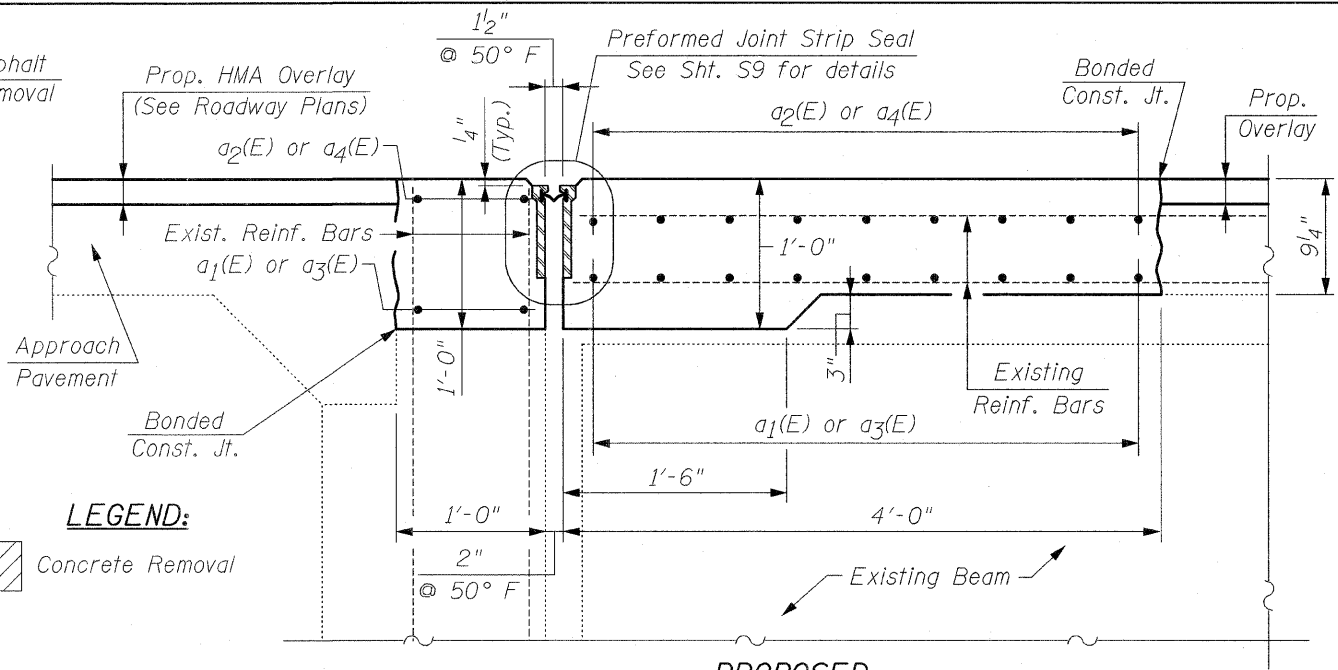
JOINT RECONSTRUCTION AT NORTH & SOUTH ABUTMENTS
IL 72 (LEE STREET) OVER WILLOW CREEK S.N. 016-0506

SCALE: SHEET NO. S4 OF S10 SHEETS STA. TO STA.

F.A.U. RTE. 1350	SECTION 2008-063 RS	COUNTY COOK	TOTAL SHEETS 42	SHEET NO. 25
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 60F40	



EXISTING SECTION A-A
NORTH ABUTMENT
 (South Abutment similar by 180° rotation)



PROPOSED SECTION A-A
NORTH ABUTMENT
 (South Abutment similar by 180° rotation)

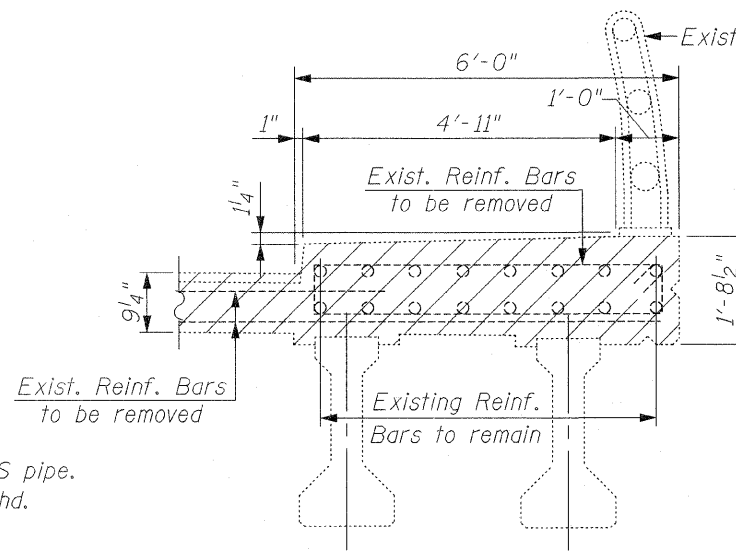
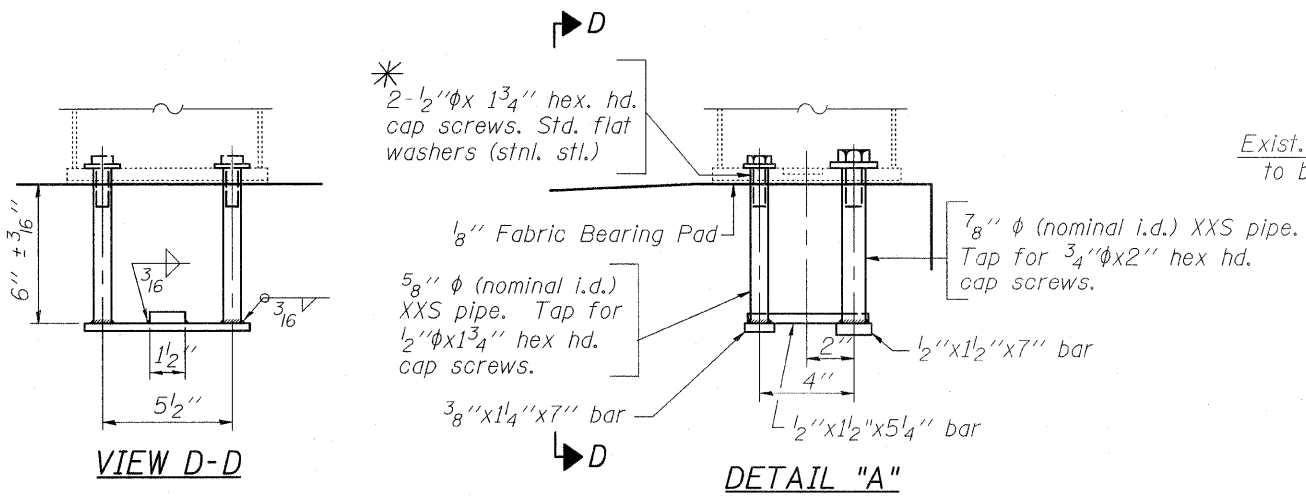
NOTE:
 For Bar Splicer Assembly details, see Sht. S8

LEGEND:
 Concrete Removal

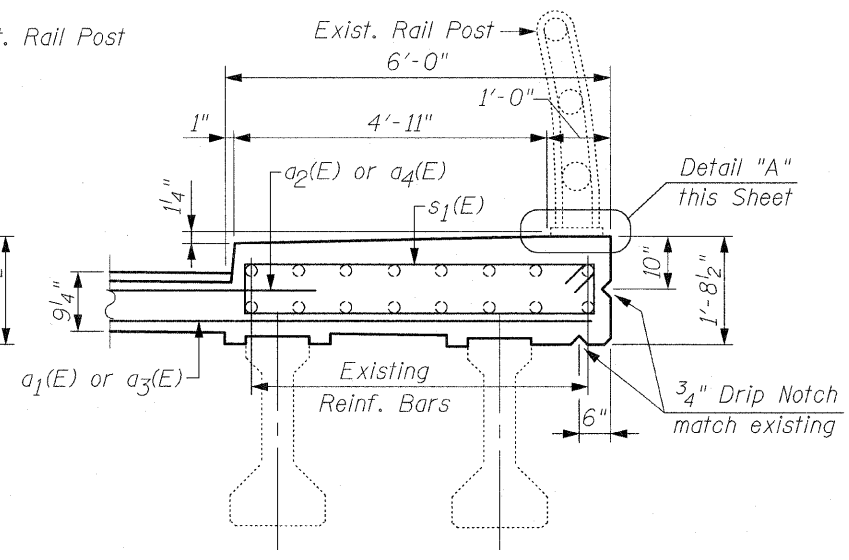
NOTE:
 Dimensions are based on a Rolled Rail Strip Seal Joint. If the Contractor elects to use the Welded Rail Strip Seal Joint, Deck dimensions may require adjustments to match the details on Sht. S9

NOTES:
 Remove and store rails in the removal area, prior to Concrete removal. Remove and store Rail Post that may be in the Concrete removal area. Reattach Rail Post as shown in Detail "A" and View D-D, the Contractor must verify locations & sizes of existing holes prior to ordering XXS pipes, bars, cap screws & Std. flat washers. Reinstall Rails after Concrete Superstructure is completed. Cost included with Concrete Superstructure.

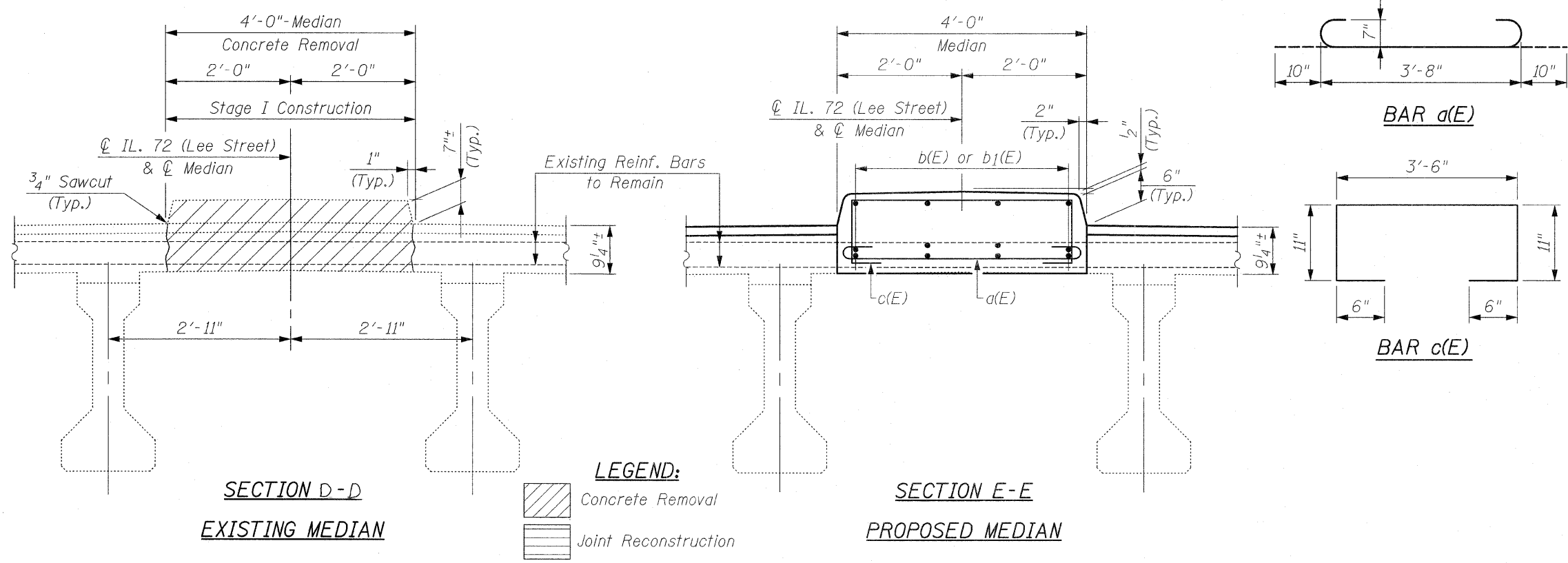
*
 In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting stainless steel anchor rods of the same diameter and grade as the specified cap screws according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications. Cost included with Concrete Superstructure.



EXISTING SECTION C-C
AT WEST SIDEWALK
 (Looking South)
 (East Sidewalk similar by 180° rotation)



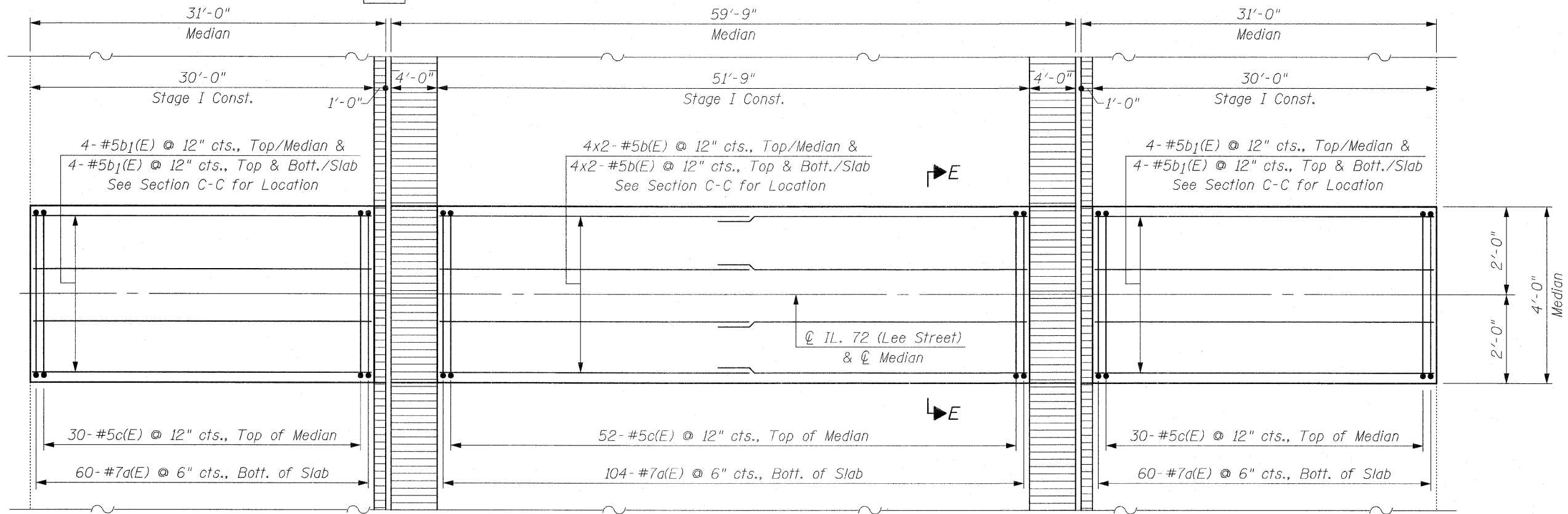
PROPOSED SECTION C-C
AT WEST SIDEWALK
 (Looking South)
 (East Sidewalk similar by 180° rotation)



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	224	#7	5'-4"	
b(E)	24	#5	29'-10"	—
b ₁ (E)	24	#5	26'-8"	—
c(E)	112	#5	6'-4"	
Reinforcement Bars, Epoxy Coated			Pound	4,600
Concrete Removal			Cu. Yd.	22.3
Concrete Superstructure			Cu. Yd.	21.0

Bars indicated thus 4x2-#5 etc., indicates 4 lines of bars with 2 lengths per line



MEDIAN PLAN
MIN. LAP:
#5 Bars = 2'-2"

FILE NAME = median_repair.dgn
PLOT DATE = 3/23/2009

CHRISTIAN-ROGE & ASSOCIATES, INC.
ENGINEERS-PLANNERS-SURVEYORS
211 WEST WACKER DRIVE
CHICAGO, ILLINOIS 60606
PHONE: (312)372-2023 FAX: (312)372-5274

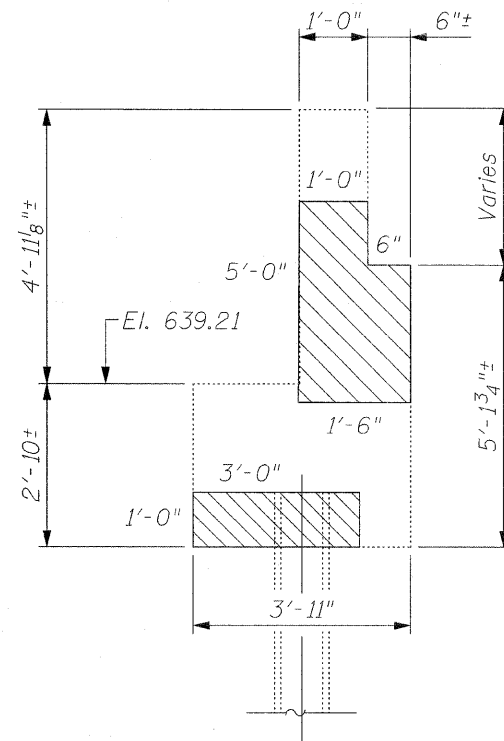
DESIGNED - B.N.S./J.C.N.	REVISED -
DRAWN - F.M.	REVISED -
CHECKED - B.N.S./J.C.N.	REVISED -
DATE - MARCH, 2009	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MEDIAN RECONSTRUCTION
IL. 72 (LEE STREET) OVER WILLOW CREEK S.N. 016-0506

SCALE: SHEET NO. S6 OF S10 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2008-063 RS	COOK	42	27
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 60F40	



EXISTING

END VIEW

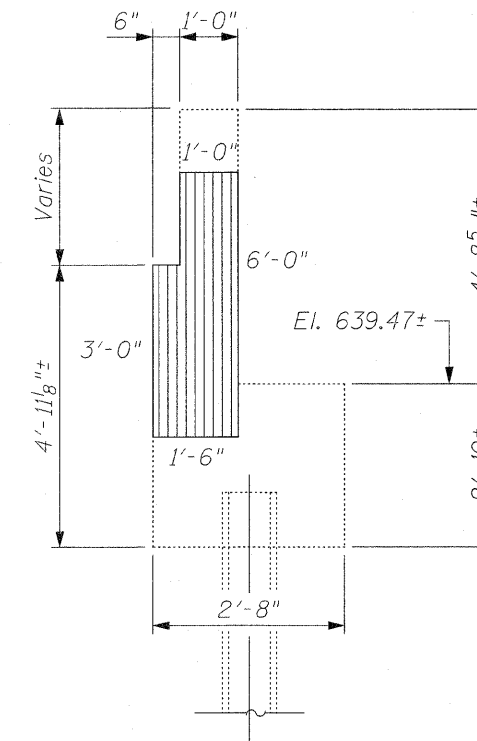
NORTH ABUTMENT
East Side

LEGEND:

 Structural Repair of Concrete
(Depth Equal to or Less Than 5 In.)

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal to or Less Than 5 In.)	Sq. Ft.	11



EXISTING

END VIEW

SOUTH ABUTMENT
East Side

LEGEND:

 Structural Repair of Concrete
(Depth Greater Than 5 In.)

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Greater Than 5 In.)	Sq. Ft.	8

FIELD NOTE:

INSPECTION WAS CONDUCTED IN SEPT., 2008
BY I.D.O.T. FIELD MAINTENANCE ENGINEERS

FILE NAME = substructure_repair.dgn
PLOT DATE = 3/23/2009

 **CHRISTIAN-ROGE & ASSOCIATES, INC.**
ENGINEERS-PLANNERS-SURVEYORS
211 WEST WACKER DRIVE
CHICAGO, ILLINOIS 60606
PHONE: (312)372-2023 FAX: (312)372-5274

DESIGNED - B.N.S./J.C.N.	REVISED -
DRAWN - F.M.	REVISED -
CHECKED - B.N.S./J.C.N.	REVISED -
DATE - MARCH, 2009	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

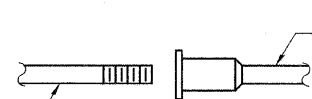
SUBSTRUCTURE CONCRETE REPAIRS
IL 72 (LEE STREET) OVER WILLOW CREEK S.N. 016-0506

SCALE: SHEET NO. S7 OF S10 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2008-063 RS	COOK	42	28
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 60F40	

The diameter of this part is equal or larger than the diameter of the bar spliced.

The diameter of this part is the same as the diameter of the bar spliced.



ROLLED THREAD DOWEL BAR



**** ONE PIECE**

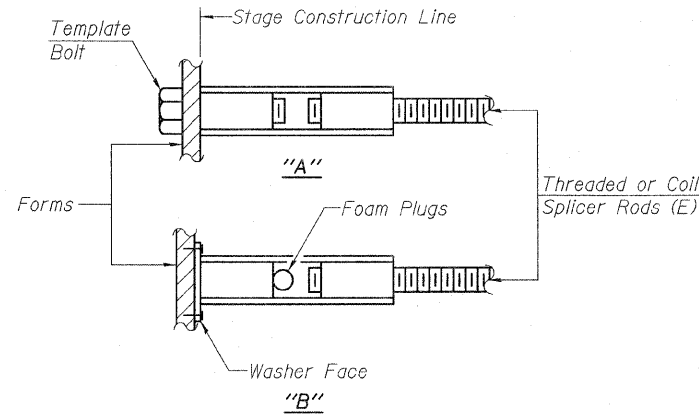
Wire Connector



WELDED SECTIONS

BAR SPLICER ASSEMBLY ALTERNATIVES

**Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



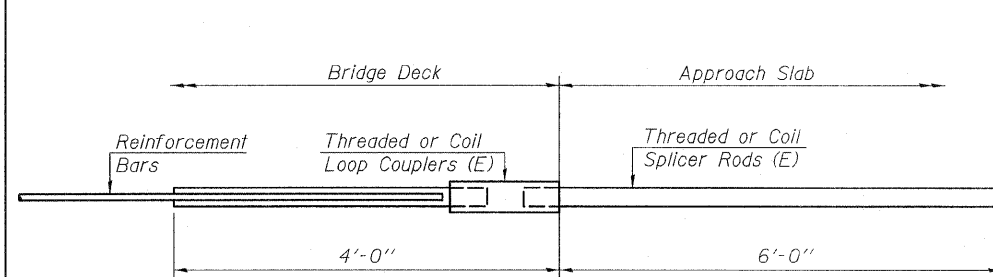
INSTALLATION AND SETTING METHODS

"A": Set bar splicer assembly by means of a template bolt.
 "B": Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.

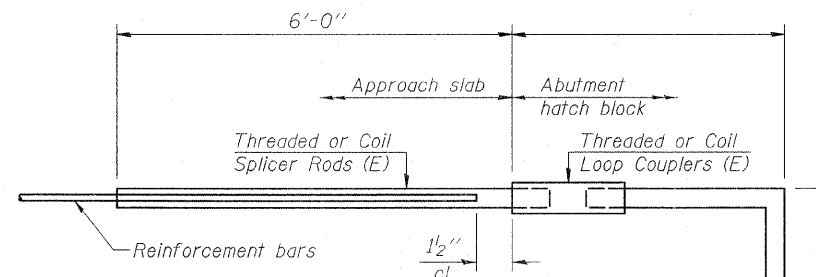
NOTES
 Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.
 Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.
 All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.
 Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

- ① Minimum Capacity (Tension in kips) = $1.25 \times f_y \times A_t$
 - ② Minimum *Pull-out Strength (Tension in kips) = $0.66 \times f_y \times A_t$
- Where f_y = Yield strength of lapped reinforcement bars in ksi.
 A_t = Tensile stress area of lapped reinforcement bars.
 * = 28 day concrete

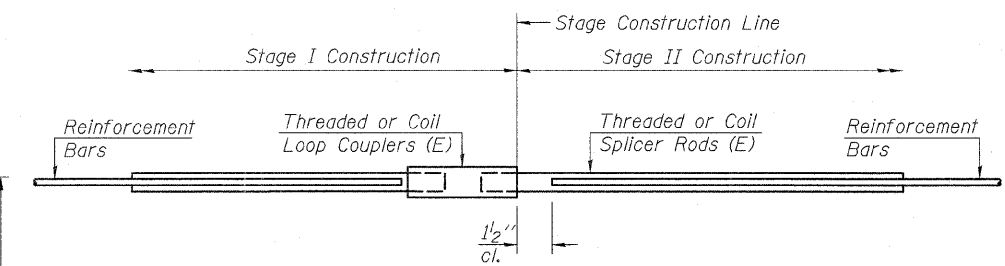
BAR SPLICER ASSEMBLIES			
Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	7.9
#5	2'-0"	23.0	12.3
#6	2'-7"	33.1	17.4
#7	3'-5"	45.1	23.8
#8	4'-6"	58.9	31.3
#9	5'-9"	75.0	39.6
#10	7'-3"	95.0	50.3
#11	9'-0"	117.4	61.8



FOR INTEGRAL OR SEMI-INTEGRAL ABUTMENTS



FOR STUB ABUTMENTS



STANDARD

Bar Size	No. Assemblies Required	Location
#5	34	North Abutment
#5	34	South Abutment

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required = -

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required = -

**BAR SPLICER ASSEMBLY DETAILS
 STRUCTURE NO. 016-0506**

BSD-1 10-1-08

FILE NAME = bar_splicer.dgn
 PLOT DATE = 3/23/2009

CHRISTIAN-ROGE & ASSOCIATES, INC.
 ENGINEERS-PLANNERS-SURVEYORS
 211 WEST WACKER DRIVE
 CHICAGO, ILLINOIS 60606
 PHONE: (312)372-2023 FAX: (312)372-5274

DESIGNED - B.N.S./J.C.N.	REVISED -
DRAWN - F.M.	REVISED -
CHECKED - B.N.S./J.C.N.	REVISED -
DATE - MARCH, 2009	REVISED -

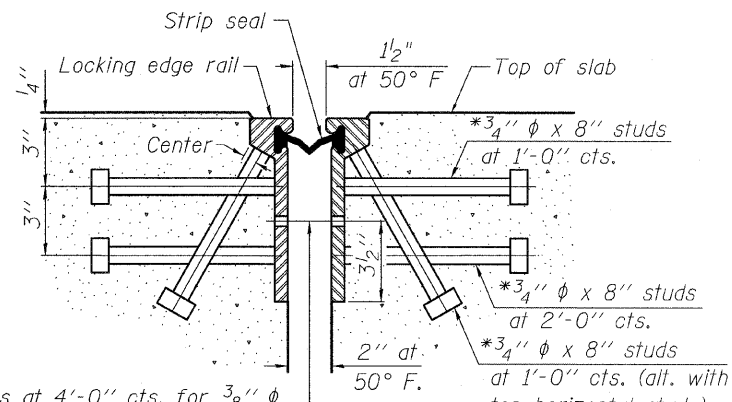
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**BAR SPLICER ASSEMBLY DETAILS
 IL. 72 (LEE STREET) OVER WILLOW CREEK S.N. 016-0506**

SCALE: SHEET NO. 58 OF 510 SHEETS STA. TO STA.

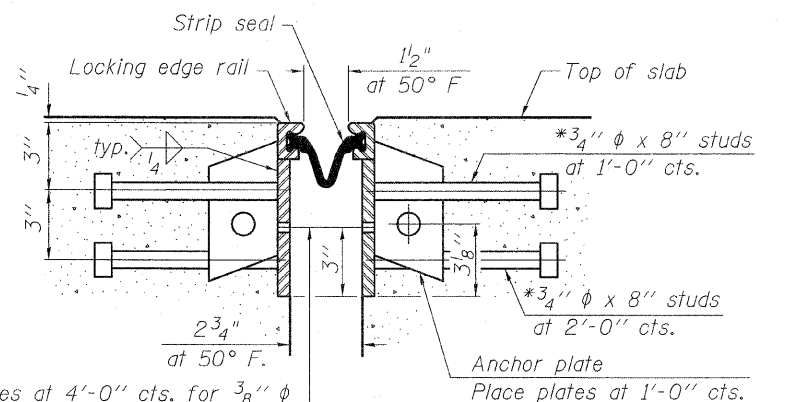
F.A.U. RTE. 1350	SECTION 2008-063 RS	COUNTY COOK	TOTAL SHEETS 42	SHEET NO. 29
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 60F40	

*Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



7/16" φ holes at 4'-0" cts. for 3/8" φ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

SECTION THRU ROLLED RAIL JOINT

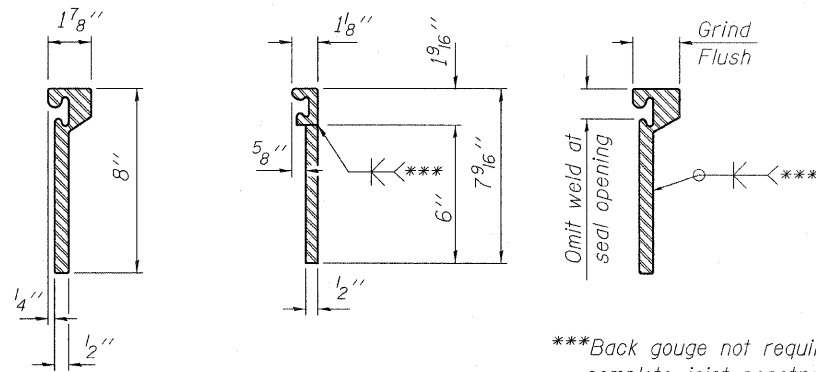


7/16" φ holes at 4'-0" cts. for 3/8" φ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

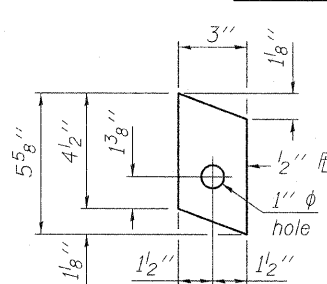
SECTION THRU WELDED RAIL JOINT

Notes:

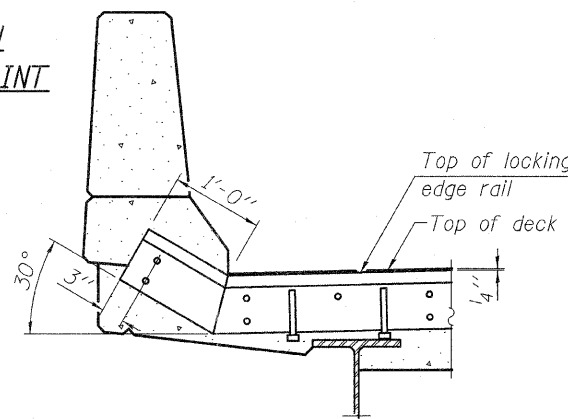
The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches. The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities and stage construction joints. The manufacturer's recommended installation methods shall be followed. The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State. All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.



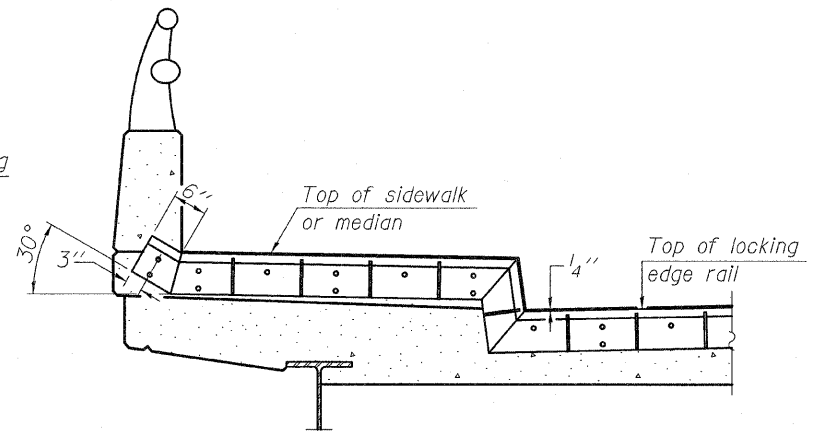
ROLLED (EXTRUDED) RAIL WELDED RAIL



ANCHOR PLATE (for welded rail)



AT PARAPET



AT SIDEWALK OR MEDIAN

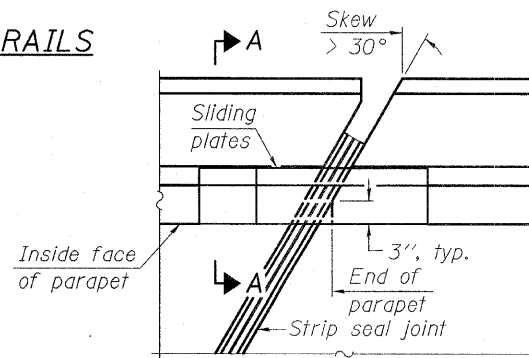
Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.

***Back gouge not required if complete joint penetration is verified by mock-up.

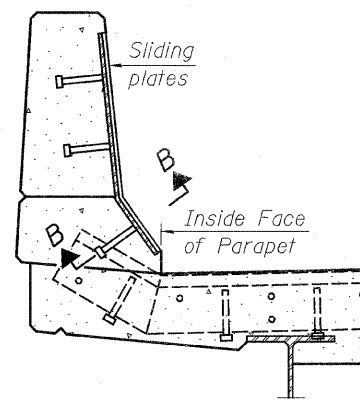
LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue.

LOCKING EDGE RAILS



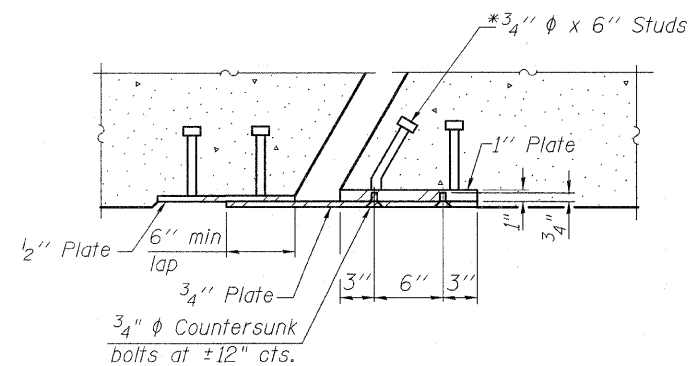
PLAN



SECTION A-A

POINT BLOCK DETAILS (for skews > 30°)

TYPICAL END TREATMENTS



SECTION B-B

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	140

NOTE:

Work this Sheet with Sht. S5

EJ-SSJ 10-1-08

FILE NAME = preformed_joint_seal.dgn
PLOT DATE = 3/23/2009

CHRISTIAN-ROGE & ASSOCIATES, INC.
ENGINEERS-PLANNERS-SURVEYORS
211 WEST WACKER DRIVE
CHICAGO, ILLINOIS 60606
PHONE: (312)372-2023 FAX: (312)372-5274

DESIGNED - B.N.S./J.C.N.
DRAWN - F.M.
CHECKED - B.N.S./J.C.N.
DATE - MARCH, 2009

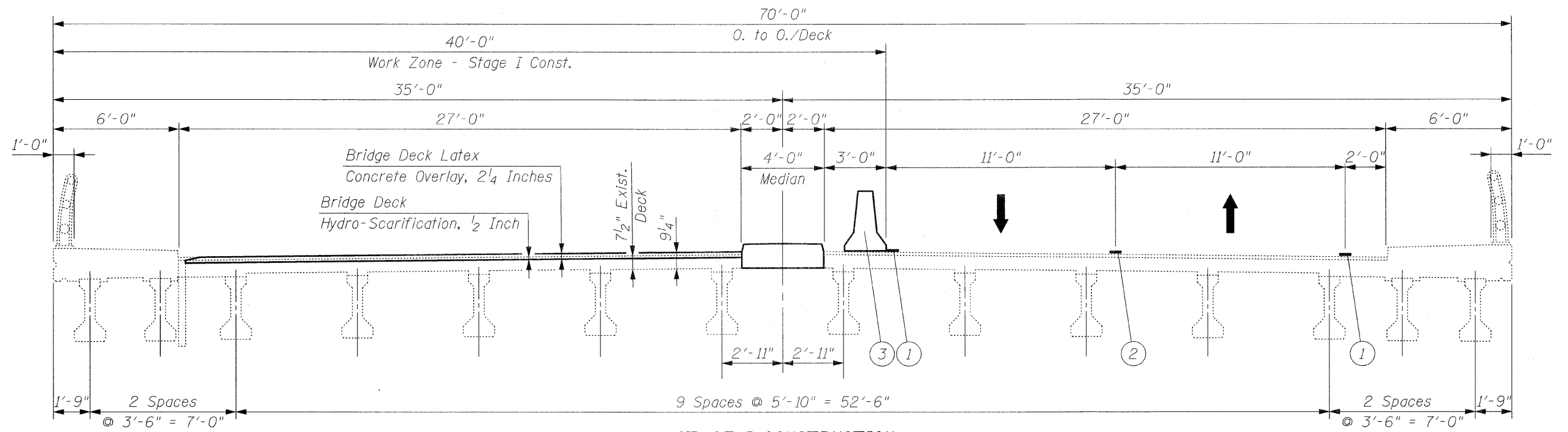
REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

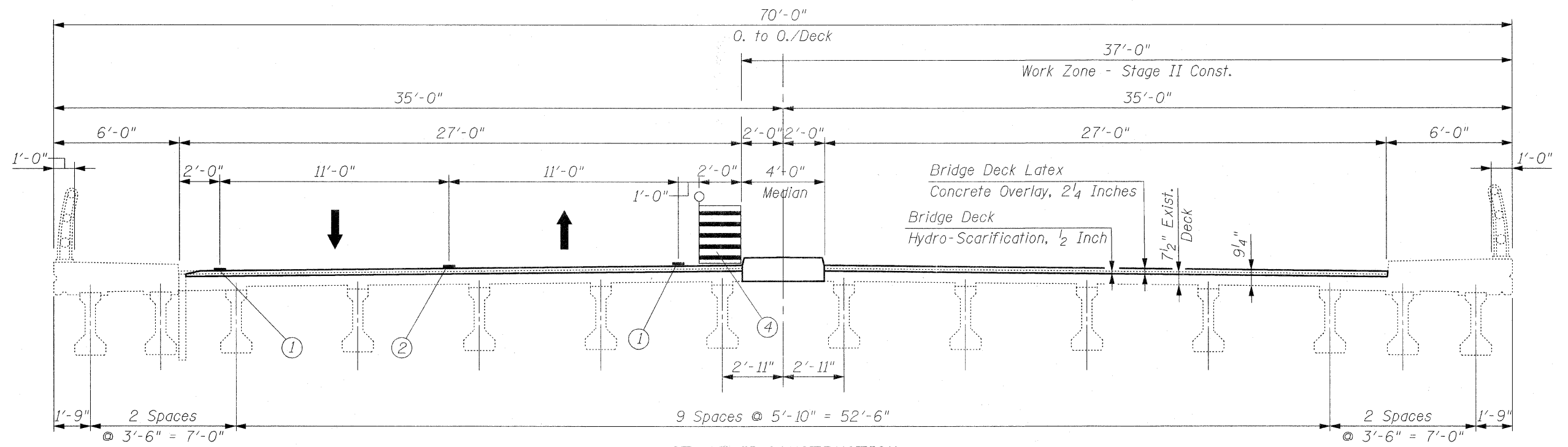
PREFORMED JOINT STRIP SEAL
IL. 72 (LEE STREET) OVER WILLOW CREEK S.N. 016-0506

SCALE: SHEET NO. S9 OF S10 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2008-063 RS	COOK	42	30
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 60F40	

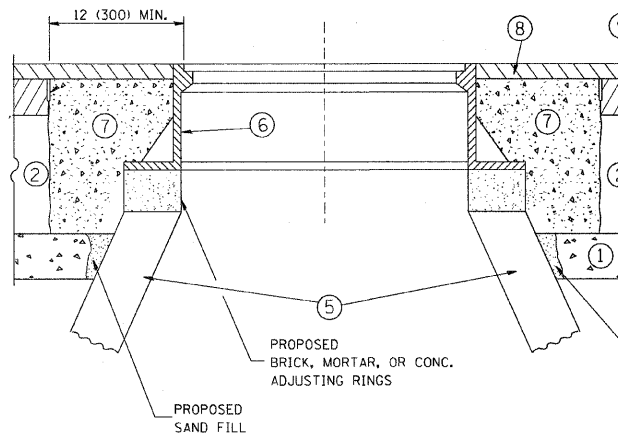
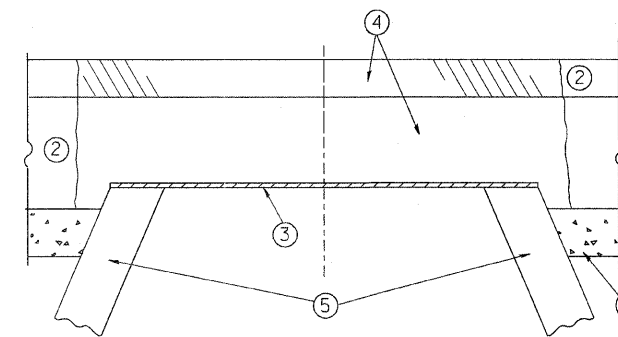


STAGE I CONSTRUCTION
(Looking South)



STAGE II CONSTRUCTION
(Looking South)

- LEGEND:**
- ① Temporary 4" White Edge Line
 - ② Temporary 2-4" Yellow Lines @ 11" C-C
 - ③ Temporary Concrete Barrier
 - ④ Drums With Steady Burning Lights @ 10'-0" C-C



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

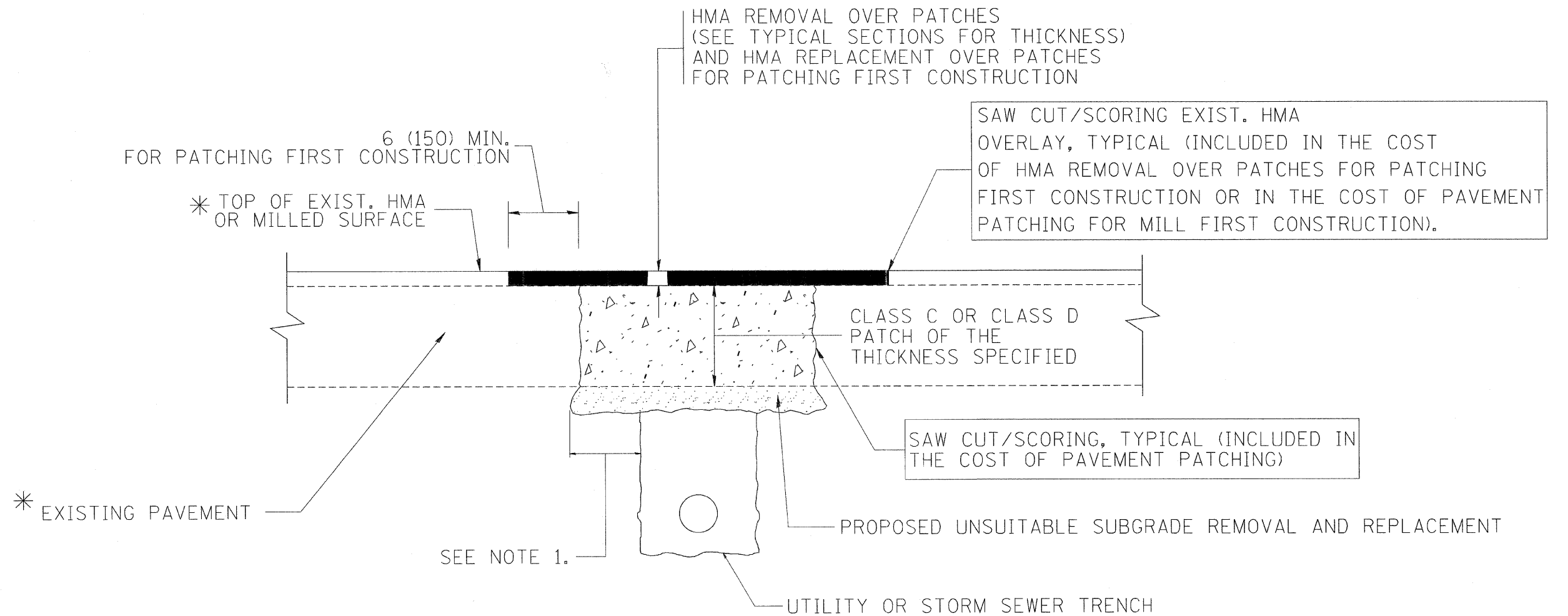
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = W:\diststd\22x34\bd08.dgn	USER NAME = goglionobt	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			F.A.U. RTE. 1350	SECTION 2008-063 RS	COUNTY COOK	TOTAL SHEETS 42	SHEET NO. 32
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - A. ABBAS 03-21-97	REVISED - R. WIEDEMAN 05-14-04		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	BD600-03 (BD-8)		CONTRACT NO. 60F40		
PLOT DATE = 1/4/2008	DATE - 10-25-94	REVISED - R. BORO 01-01-07			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

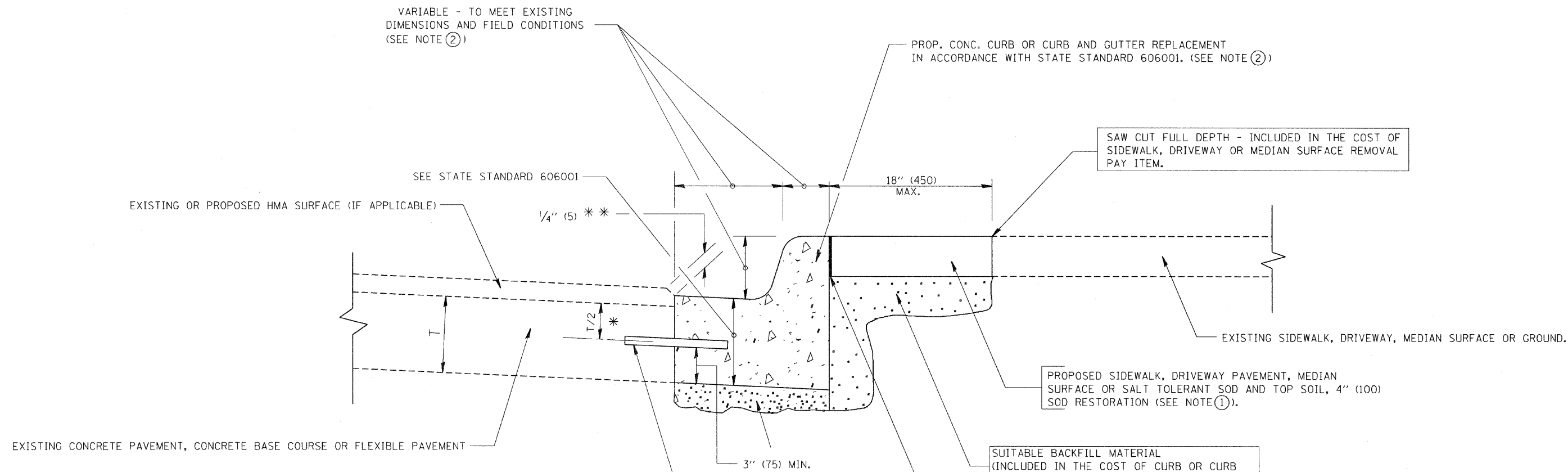
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	EAU R/L	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50,000 / IN.	CHECKED -	REVISED - R. BORO 01-01-07			1350	2008-063 RS	COOK	42	33
PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08	REVISED - R. BORO 09-04-07	SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT BD400-04 (BD-22) CONTRACT NO. 60F40				



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
 ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

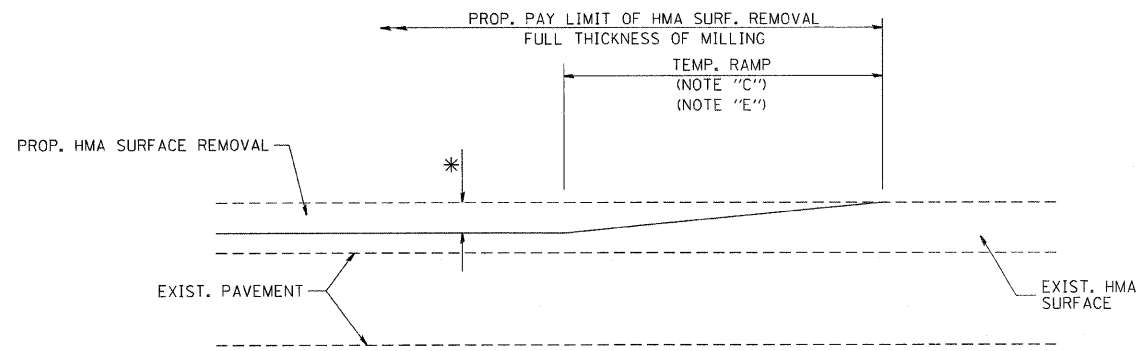
⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

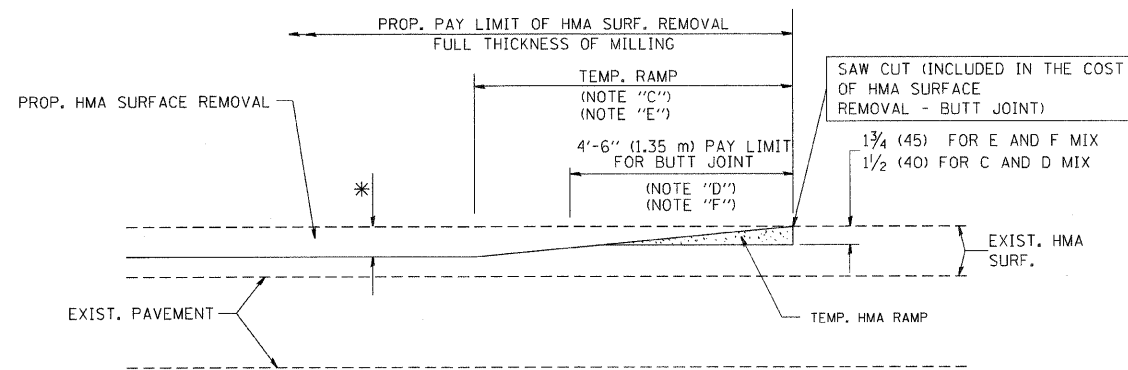
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\bd24.dgn	USER NAME = geglionobt	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. R/E: 1350	SECTION 2008-063 RS	COUNTY COOK	TOTAL SHEETS 42	SHEET NO. 34
PLOT SCALE = 50.000' / IN.		CHECKED -	REVISED - M. COMEZ 01-22-01	SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.		BD600-06 (BD-24)		CONTRACT NO. 60F40		
PLOT DATE = 1/4/2008		DATE - 03-11-94	REVISED - R. BORO 01-01-07	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

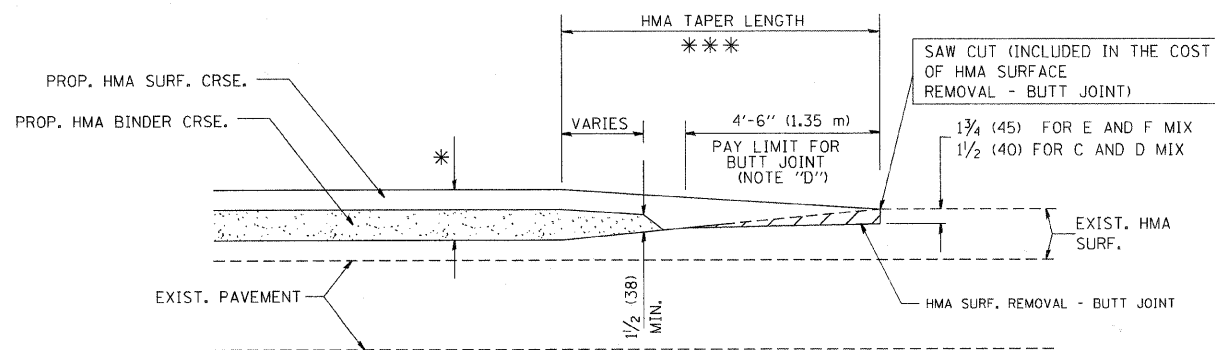
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

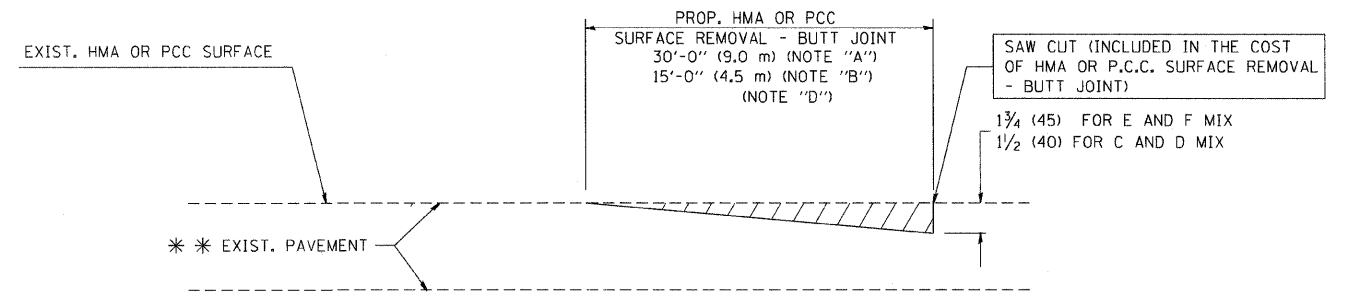
OPTION 2

TYPICAL TEMPORARY RAMP

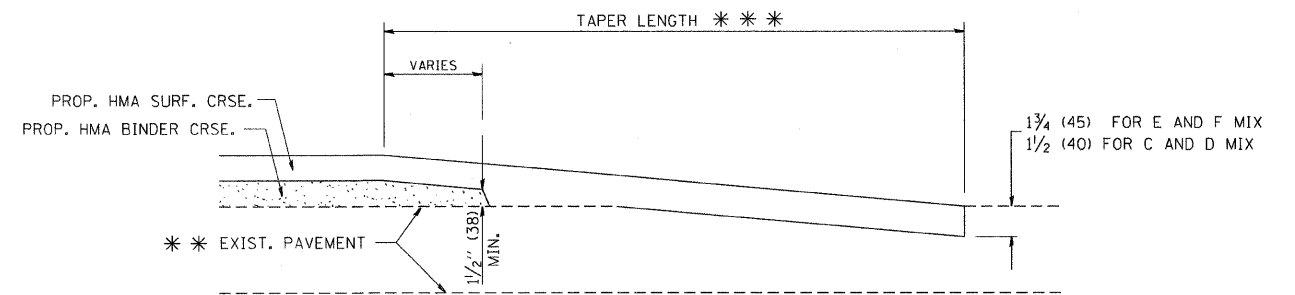


BUTT JOINT AND
HMA TAPER

**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

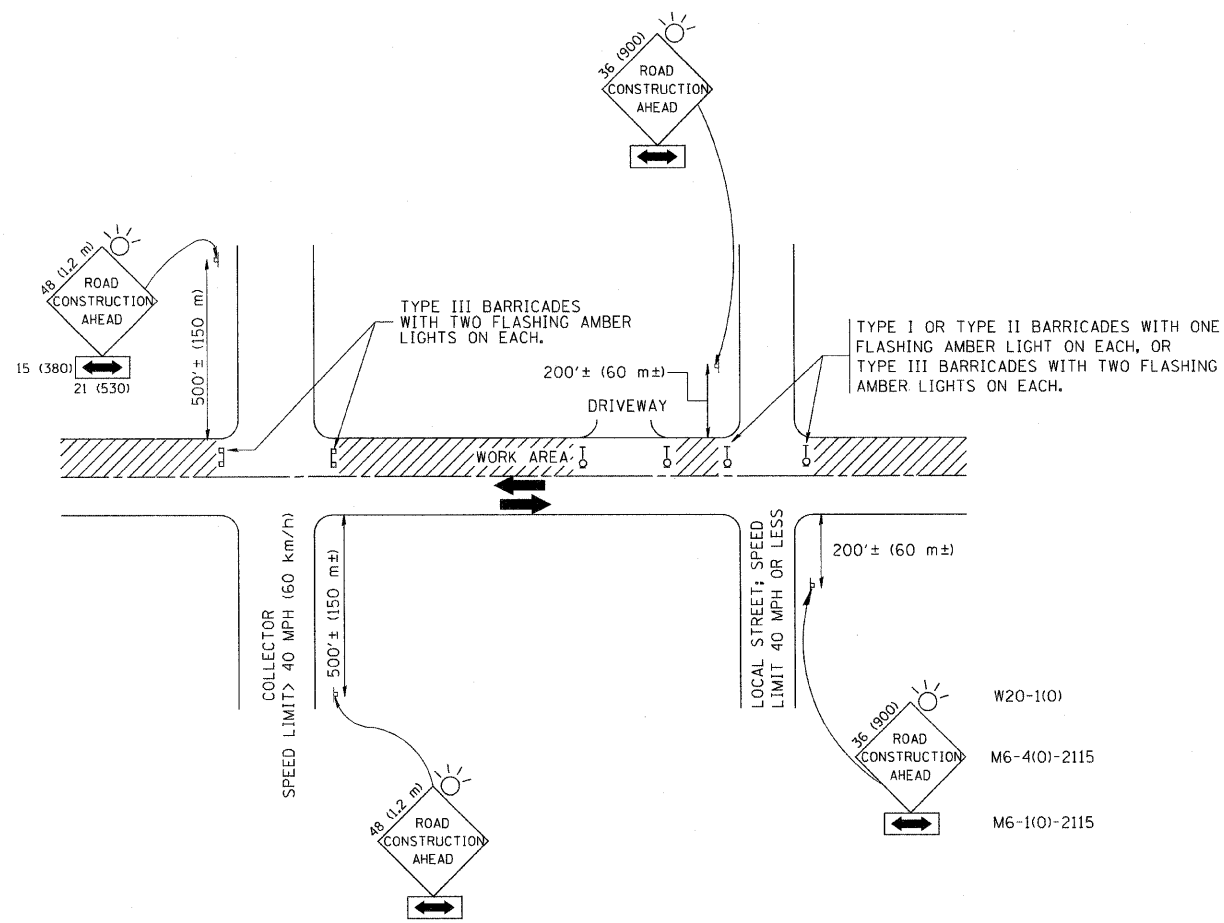
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		DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED - M. GOMEZ 04-06-01
PLOT DATE = 1/4/2008		DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2008-063 RS	COOK	42	35
BD400-05 BD32		CONTRACT NO. 60F40		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

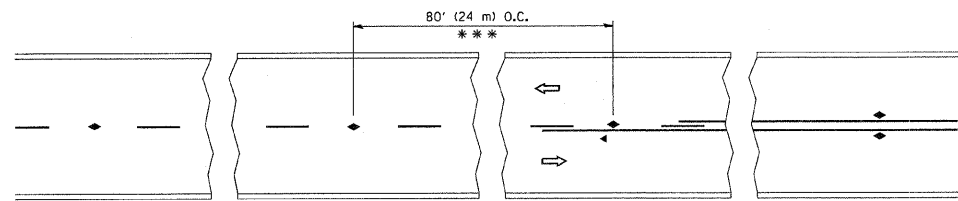
- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 - 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

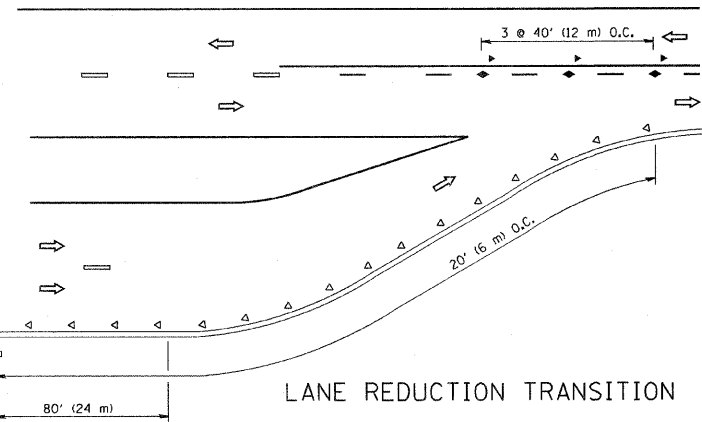
All dimensions are in millimeters (inches) unless otherwise shown.

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	PLOT SCALE = 50,000 / IN.	CHECKED -	REVISED - A. HOUSEH 03-06-96					1350	2008-063 RS	COOK	42	36
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.			TC-10		CONTRACT NO. 60F40		
								FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

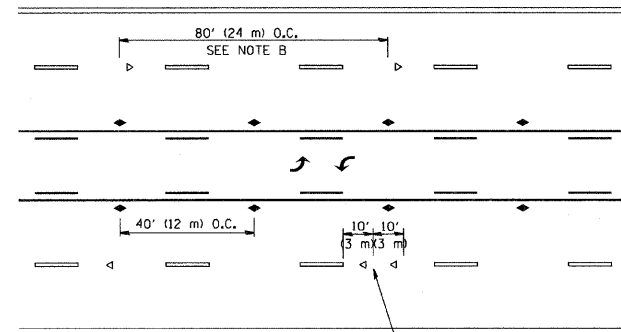


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

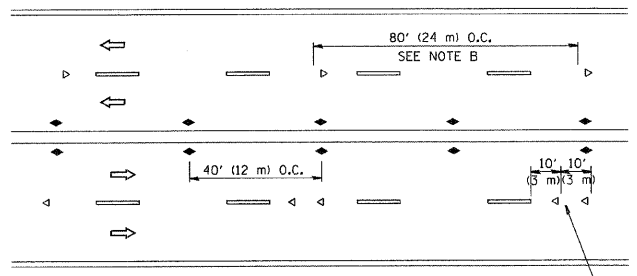
TWO-LANE/TWO-WAY



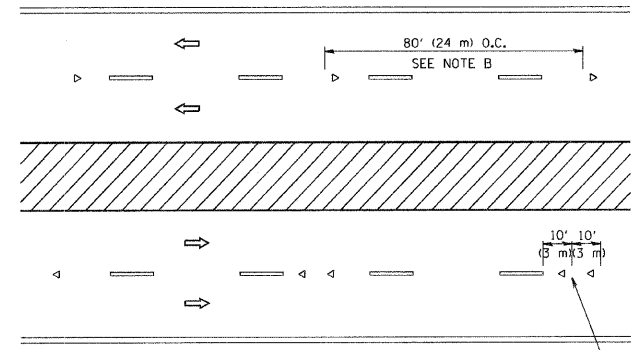
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

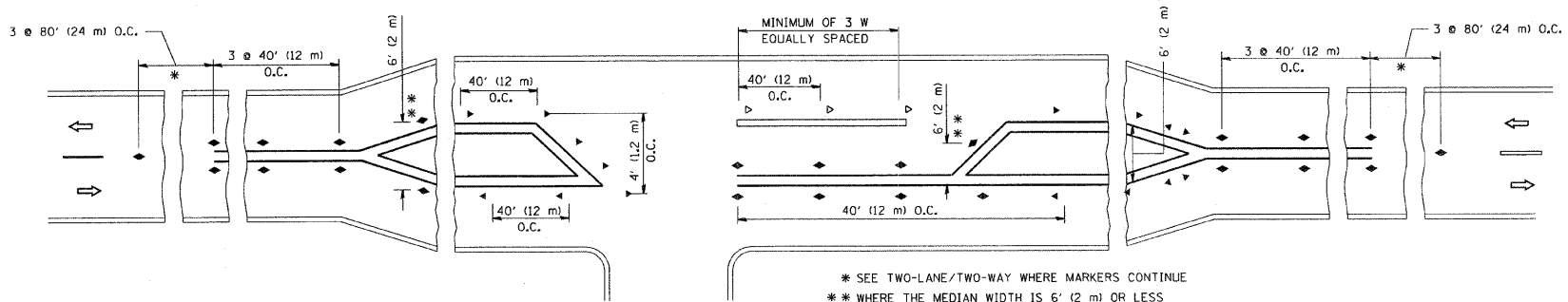
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

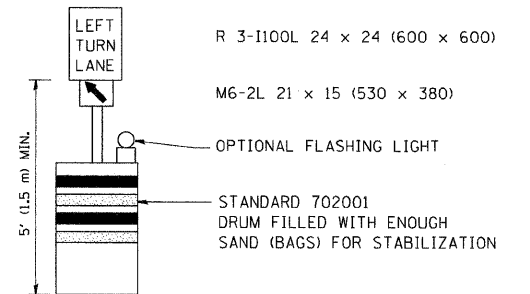
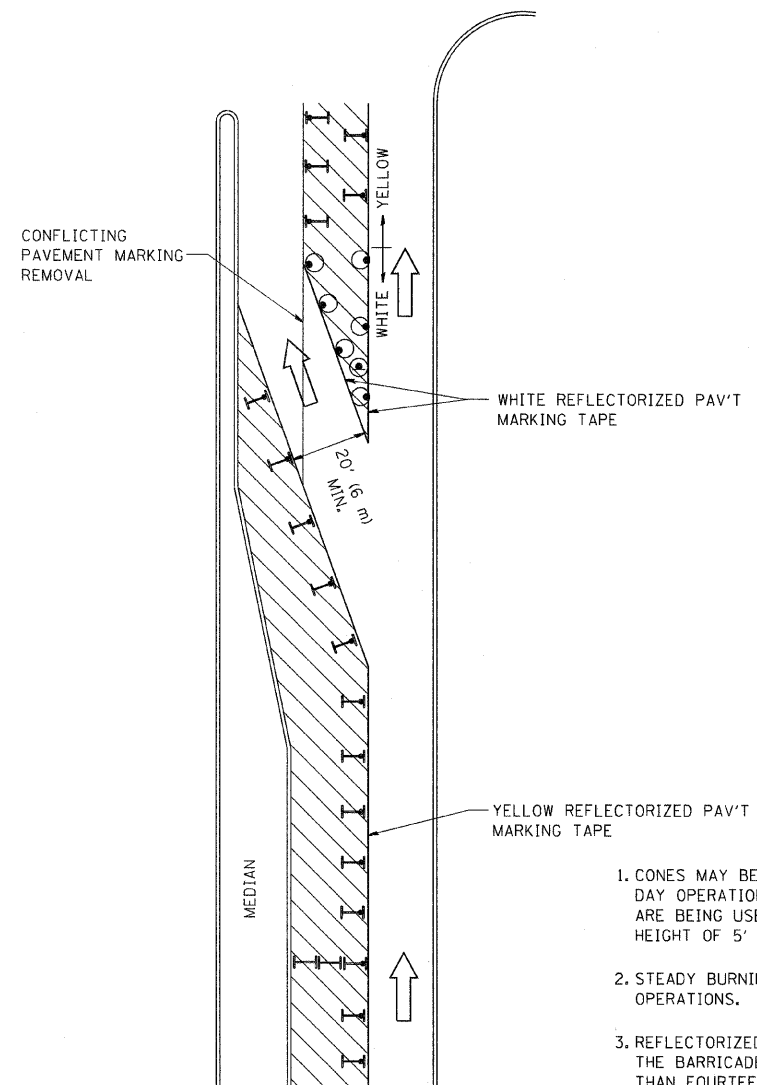


LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (1.2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\d:\stsd\22x34\1011.dgn	USER NAME = goglienobt	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			F.A.U. RTE. 1350	SECTION 2008-063 RS	COUNTY COOK	TOTAL SHEETS 42	SHEET NO. 37	
PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED - T. RAMMACHER 03-12-99	REVISED - T. RAMMACHER 01-06-00		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	TC-11					
PLOT DATE = 1/4/2008	DATE -	REVISED -	REVISED -		CONTRACT NO. 60F40								
											FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

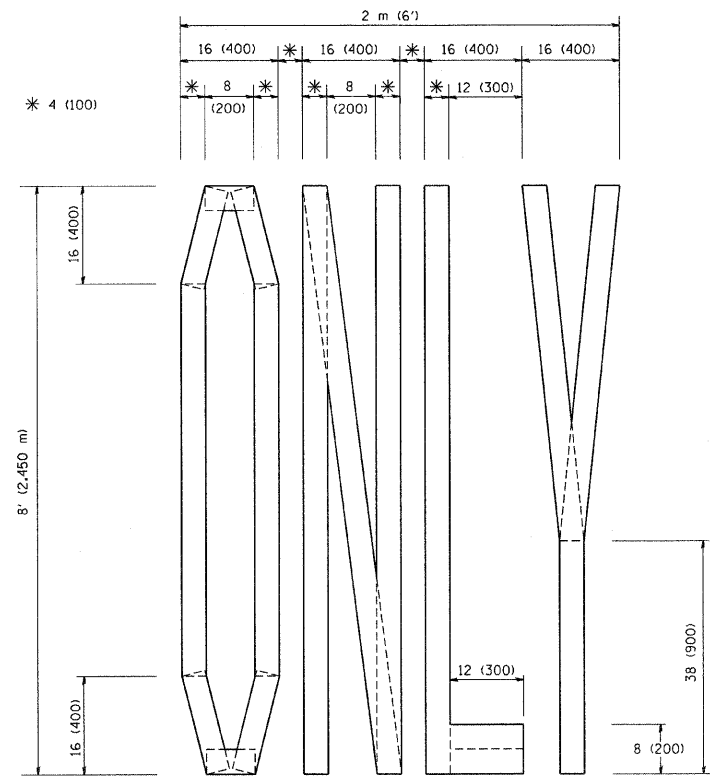
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		DRAWN -	REVISED - A. HOUSEH 11-07-95
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-12-96
	PLOT DATE = 1/4/2008	DATE -	REVISED -T. RAMMACHER 01-06-00

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

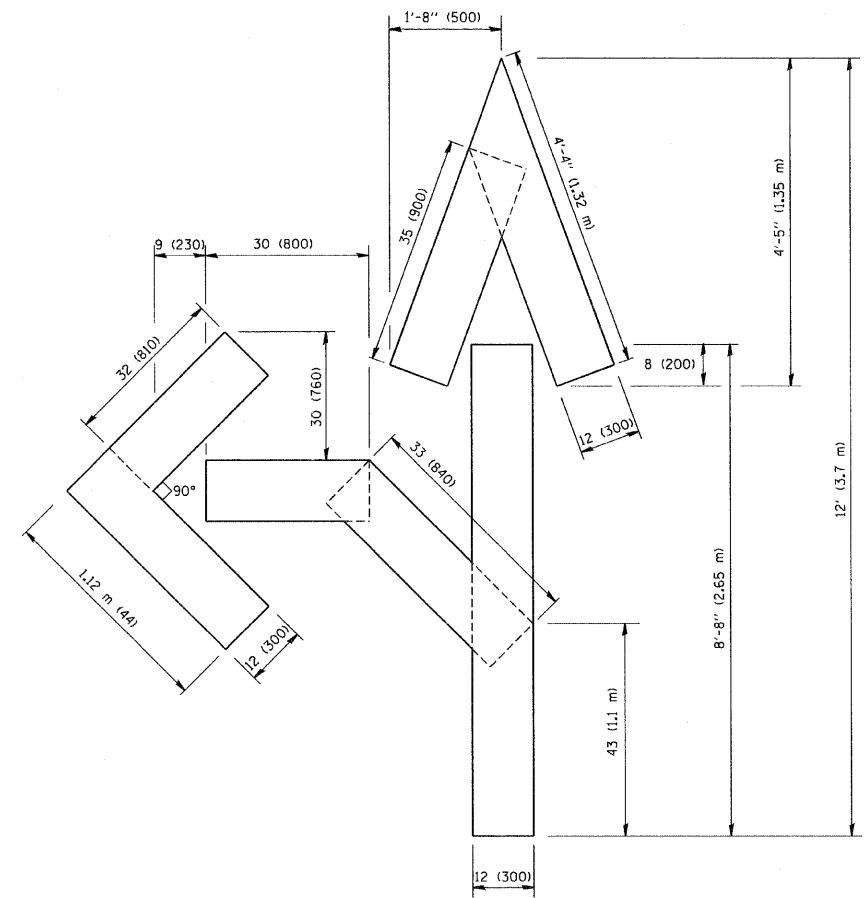
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

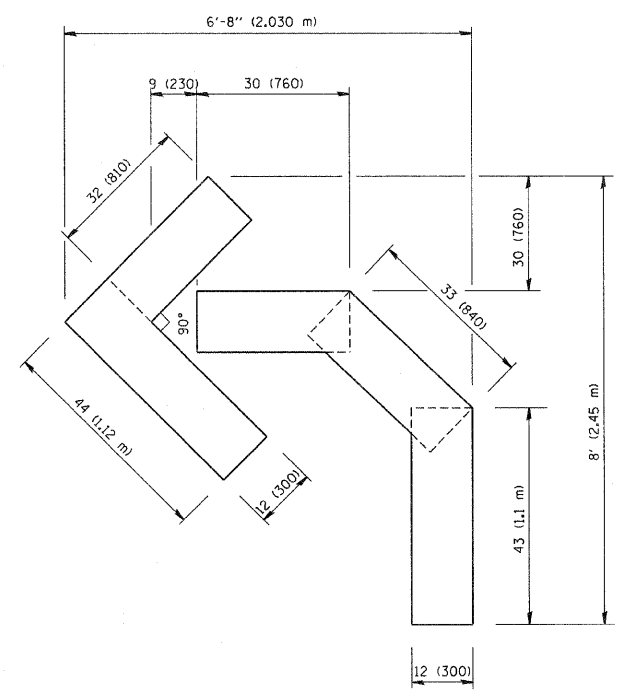
FAU. RTE. 1350	SECTION 2008-063 RS	COUNTY COOK	TOTAL SHEETS 42	SHEET NO. 39
TC-14		CONTRACT NO. 60F40		
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\diststd\22x34\1col6.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED -T, RAMMACHER 06-05-96
		DRAWN -	REVISED -T, RAMMACHER 11-04-97
		CHECKED -	REVISED -T, RAMMACHER 03-02-98
PLOT SCALE = 50.0000 / IN.	DATE - 09-18-94		REVISED -E, GOMEZ 08-28-00
PLOT DATE = 1/4/2008			

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

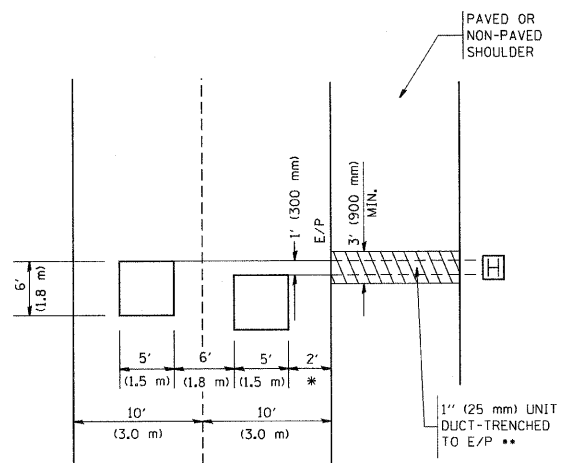
PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2008-063 RS	COOK	42	40
TC-16			CONTRACT NO. 60F40	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.

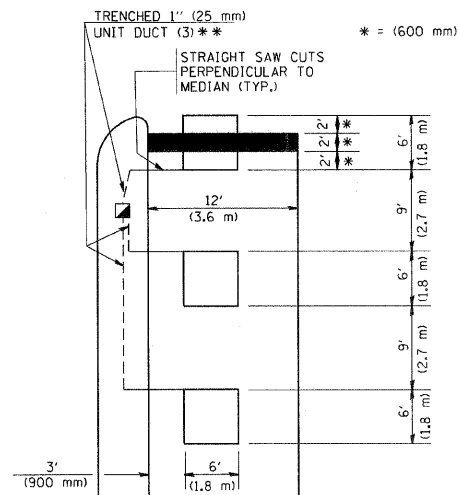


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

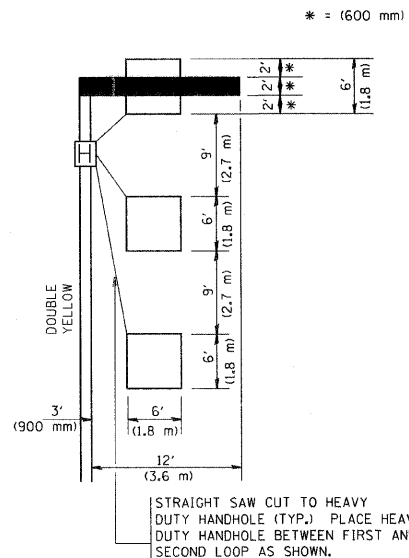
HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
814001 TO ENSURE THAT HANDHOLE
FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

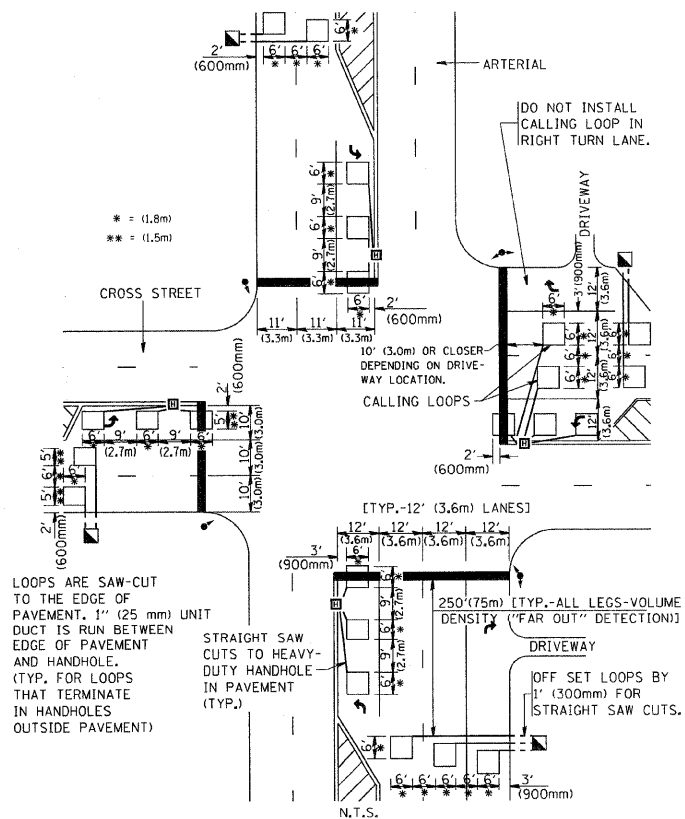
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



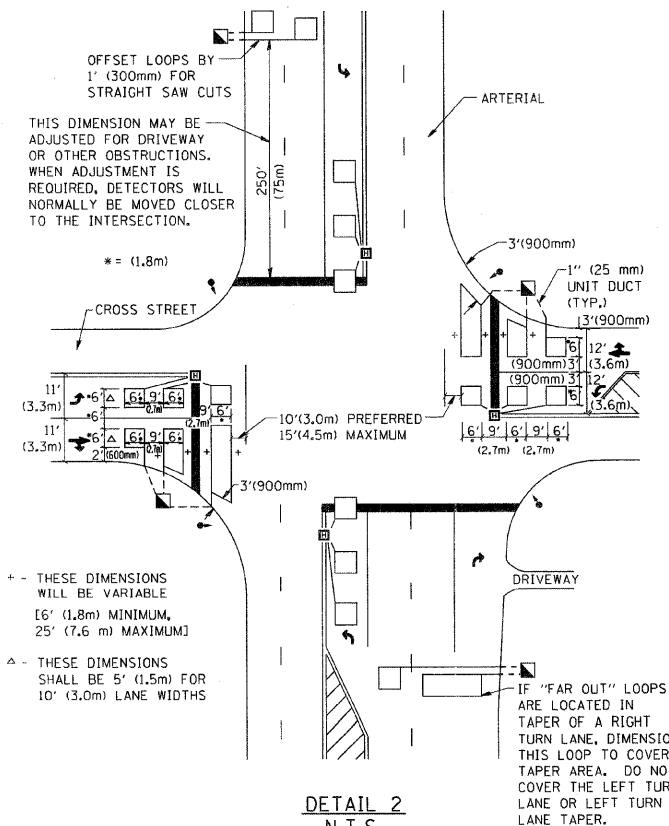
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\diststd\22x34\ts07.dgn	USER NAME = goglianob	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING			FA.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
	PLOT SCALE = 50,0000' / IN.	DRAWN -	REVISED -		SCALE: NONE	SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.	1350	2008-063 RS	COOK	42	42
	PLOT DATE = 1/4/2008	CHECKED - R.K.F.	REVISED -									TS-07			CONTRACT NO. 60F40
		DATE -	REVISED -									FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	