

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR INDEX OF HIGHWAY STANDARDS, SEE SHEET NO. 2

PROJECT LOCATED IN THE VILLAGE OF CARY

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY**

F.A.U. ROUTE 0116 CRYSTAL LAKE AVENUE

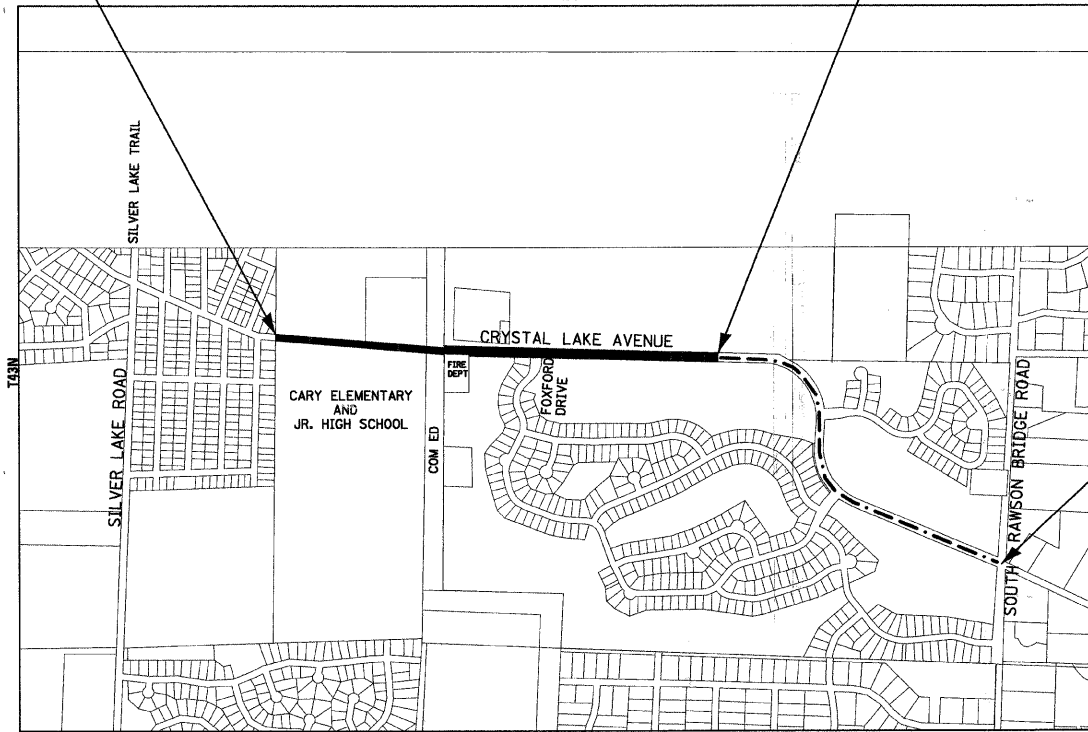
**WEST VILLAGE LIMITS TO SOUTH RAWSON BRIDGE ROAD
RESURFACING
PROJECT No.: M-9003 (169)
SECTION No.: 08-00055-00-RS
JOB No: C-91-202-09
VILLAGE OF CARY
McHENRY COUNTY**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00055-00-RS	McHENRY	18	1
STA. 13+45		TO STA. 88+93		
FED. ROAD DIST. NO C-91-202-09		ILLINOIS	FED AID PROJECT M-9003 (169)	
CONTRACT No: 63115				

PROJECT NUMBER: M-9003 (169)
F.A.U. ROUTE 0116
CRYSTAL LAKE AVENUE
BEGIN RESURFACING
STA. 13+45

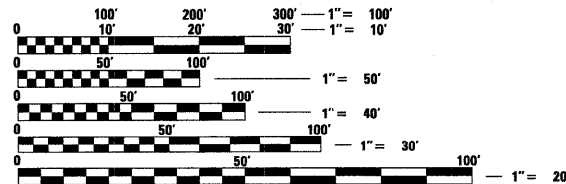
PROJECT NUMBER: M-9003 (169)
F.A.U. ROUTE 0116
CRYSTAL LAKE AVENUE
END RESURFACING / BEGIN OMISSION
STA. 53+30

LOCATION MAP
(NOT TO SCALE)
RBE



PROJECT NUMBER: M-9003 (169)
F.A.U. ROUTE 0116
CRYSTAL LAKE AVENUE
END OMISSION
STA. 88+93

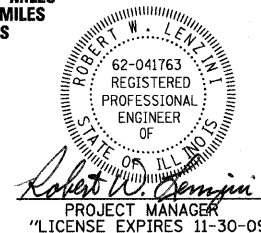
BAXTER & WOODMAN, INC
STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
LICENSE NO. - 184-001121 - EXPIRES 4/30/2009



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

ALGONQUIN TOWNSHIP
GROSS LENGTH OF IMPROVEMENT = 7,548 LF OR 1.430 MILES
NET LENGTH OF IMPROVEMENT = 3,985 LF OR 0.755 MILES
LENGTH OF OMISSION = 3,563 LF OR 0.675 MILES



TRAFFIC DATA
CRYSTAL LAKE AVENUE
POSTED & DESIGN SPEED LIMIT = 35 MPH
2008 ADT = 3,950

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED 02-08-09
[Signature]
LOCAL AGENCY REPRESENTATIVE

PASSED FEBRUARY 13, 2009
[Signature]
LOCAL ROADS ENGINEER

RELEASING FOR BID
BASED ON LIMITED
REVIEW FEBRUARY 13, 2009
[Signature]
DEPUTY DIRECTOR OF
HIGHWAYS, REGION 1 ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

CONTRACT NO. 63115

Burlington, Wisconsin 262.763.7834
 Crystal Lake, Illinois 815.459.1260
 DeKalb, Illinois 815.787.3111
 Itasca, Illinois 630.773.1870
 Mokena, Illinois 708.560.7070
 Graylake, Illinois 847.223.5088
 Plainfield, Illinois 815.609.7425
 Rockford, Illinois 815.489.1551
 Madison, Wisconsin 608.347.1542
 Chicago, Illinois 312.578.0050

B&W PROJECT NO.: 080936 DATE: 02/03/09



LOCATION OF SECTION INDICATED THUS: [Symbol]

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STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
LICENSE NO. - 184-001121 - EXPIRES 4/30/2009
BUREAU OF LOCAL ROADS AND STREETS: ALEX HOUSEN, P.E. / KEVIN STALLWORTH, P.E. / 847-705-1169 / SCHAUMBURG, IL

GENERAL NOTES

- ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS, AND THE LATEST EDITION OF THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO AS THE "STANDARD SPECIFICATIONS"), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" AND THE "MANUAL OF TEST PROCEDURES FOR MATERIALS".
- UTILITY LOCATIONS HAVE NOT BEEN SHOWN ON THESE PLANS. THE CONTRACTOR SHALL HAVE THE RESPECTIVE UTILITY COMPANIES FIELD LOCATE ALL THEIR FACILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL ALSO VERIFY THE DEPTHS OF THE EXISTING UTILITIES IF NECESSARY. ANY RELOCATION OR LOWERING OF UTILITIES SHALL BE COORDINATED BY THE CONTRACTOR
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE ENGINEER OR VILLAGE.
- THE CONTRACTOR SHALL NOTIFY THE ENGINEER AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK TO OBTAIN VILLAGE UTILITY LOCATIONS AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE ENGINEER.
- MATERIALS RESULTING FROM THE REMOVAL OF PAVEMENT, DRIVEWAYS, CURB AND GUTTER, AND HOT-MIX ASPHALT SURFACES ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IN THE JUDGMENT OF THE VILLAGE, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE VILLAGE WILL HAVE THE MATERIAL REMOVED AND THE CONTRACTOR WILL BE BILLED (CHARGED) ACCORDINGLY.
- THE CONTRACTOR MAY OBTAIN MUNICIPAL WATER IN BULK, AT NO CHARGE, AS LONG AS THERE IS NOT A "WATERING BAN" IN EFFECT. THE INDISCRIMINATE USE OF FIRE HYDRANTS IS STRICTLY PROHIBITED. WATER FOR CONSTRUCTION SHALL BE METERED OR OTHERWISE ACCOUNTED FOR AND A DAILY LOG MAINTAINED. THE CONTRACTOR SHALL PROVIDE THE WATER TRUCK AND DRIVER REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE VILLAGE RESERVES THE RIGHT TO RESTRICT OR REFUSE THE USE OF VILLAGE WATER IF DEEMED NECESSARY.
- IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS AND THE VILLAGE WHEN ACCESS TO THEIR DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE DRIVEWAY REPLACEMENT. THE CONTRACTOR SHALL DISTRIBUTE NOTICES PROVIDED BY THE VILLAGE TO RESIDENTS. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES INCLUDING KNOCKING ON DOORS WHEN DRIVEWAYS ARE ABOUT TO BE CLOSED.
- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE OWNERS, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- THE CONTRACTOR SHALL NOTIFY IDOT BUREAU OF MATERIALS (PHONE 847-705-4337) AT LEAST 24 HOURS PRIOR TO THE PLACEMENT OF HMA OR CONCRETE.
- ANY SIGNS OR MAILBOXES THAT ARE IN CONFLICT WITH THE PROPOSED CONSTRUCTION SHALL BE REMOVED AND REPLACED IN ACCORDANCE WITH VILLAGE STANDARDS AND INCLUDED IN THE COST OF THE CONTRACT. MAIL SERVICE SHALL BE MAINTAINED AT ALL TIMES.
- EXISTING PAVEMENT, DRIVEWAY PAVEMENT AND CURB AND GUTTER TO REMAIN IN PLACE SHALL BE SAW CUT FULL DEPTH TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND EXISTING AND SHALL BE INCLUDED IN THE PRICE OF THE APPROPRIATE REMOVAL PAY ITEM.
- IN AREAS WHERE THE EXISTING DRIVEWAY, OR CURB AND GUTTER IS TO BE REMOVED AND REPLACED, THE REMOVAL AND DISPOSAL OF ANY ADDITIONAL MATERIAL REQUIRED TO ESTABLISH THE PROPOSED DRIVEWAY, OR CURB AND GUTTER SUBGRADE ELEVATION SHALL BE INCLUDED IN THE PAY ITEMS, HMA SURFACE REMOVAL OF THE TYPE SPECIFIED OR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE USED. ONE (1) WEIGHTED SANDBAG SHALL BE PLACED ACROSS EACH BOTTOM RAIL.
- THE PRIME COAT APPLICATION RATE SHALL BE 0.1 GAL/SY. THE MC-30 PRIME COAT APPLICATION RATE SHALL BE 0.3 GAL/SY.
- THE LOCATIONS OF THE CLASS D PATCHES SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.
- ALL AGGREGATE USED ON THIS PROJECT SHALL BE CRUSHED MATERIAL.
- THE QUANTITIES FOR PATCHING WILL NOT EXCEED THE AMOUNT LISTED IN THE SUMMARY OF QUANTITIES.
- THE DAYS PAVING OPERATION SHALL RESULT IN A SINGLE TRANSVERSE JOINT. ANY COLD LONGITUDINAL JOINTS WILL NOT BE ACCEPTED. PROVIDING A SINGLE TRANSVERSE JOINT SHALL BE ACCOMPLISHED BY PAVING ONE LANE OF SUFFICIENT LENGTH THAT WILL ALLOW FOR THE PAVING OF THE ADJACENT LANE IN THE SAME DAY.

- FOR STEEL BARS CERTIFICATION, PLEASE CONTACT IDOT BUREAU OF MATERIALS AT (847) 705-4337.
- THE CONTRACTOR SHALL UTILIZE A MECHANICAL SWEEPER TO CLEAN STREETS AFFECTED BY CONTRACTORS OPERATIONS, INCLUDING HAUL ROUTES, AT LEAST TWICE PER WEEK AND ADDITIONALLY AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST OF CONSTRUCTION.
- NO DAMAGE TO PARKWAYS IS PLANNED AS PART OF THESE IMPROVEMENTS. ANY AND ALL RESTORATION OF DAMAGED PARKWAYS SHALL CONSIST OF NECESSARY SUB-GRADE MATERIAL AS APPROVED BY THE ENGINEER, A MINIMUM OF FOUR (4) INCHES OF TOPSOIL, SALT TOLERANT SEED (CLASS 2A), APPLICABLE FERTILIZER, AND EROSION CONTROL BLANKET. THIS WORK SHALL BE DONE IN ACCORDANCE WITH SECTIONS 211, 250, AND 251. THIS WORK SHALL BE INCLUDED IN THE COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT PAY ITEM.
- PAVEMENT CORES IDENTIFIED CLASS B PAVEMENT IN THE EXISTING SURFACE COURSES. REMOVAL AND APPROPRIATE DISPOSAL OF CLASS B PAVEMENT SHALL BE INCLUDED IN THE PAY ITEM CLASS D PATCHES OF THE TYPE AND THICKNESS SPECIFIED.
- THE DISTRICT 1 DETAIL FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT SHOWN IN THE PLANS SHALL BE MODIFIED TO INCLUDE THE FOLLOWING. THE WORK SHALL INCLUDE SAW-CUTTING AND REMOVING THE EXISTING PAVEMENT A MINIMUM OF 6-INCHES MEASURED FROM THE EXISTING EDGE OF PAVEMENT, AND FILLING THE 6" GAP WITH CLASS SI CONCRETE TO AN ELEVATION 2-1/2" BELOW THE PROPOSED CURB AND GUTTER FLAG. IF THE CONCRETE IS PLACED HIGHER THAN 2-1/2" FROM THE GUTTER FLAG FOR STREETS TO BE RESURFACED, THE CONTRACTOR WILL BE REQUIRED TO GRIND ADDITIONAL CONCRETE TO THE REQUIRED 2-1/2" DEPTH. THE AREA BEHIND THE PROPOSED CURB AND GUTTER SHALL BE BACKFILLED WITH SAND, OR OTHER SUITABLE BACKFILL MATERIAL APPROVED BY THE ENGINEER, TO THE TOP OF CURB & GUTTER AND NEATLY GRATED TO THE SATISFACTION OF THE ENGINEER. EXCESS SAND ADJACENT TO THE CURB & GUTTER SHALL BE REMOVED JUST PRIOR TO COMPLETING PARKWAY RESTORATION. THE AREA BEHIND THE PROPOSED CURB AND GUTTER SHALL BE RESTORED WITH A MINIMUM OF FOUR (4) INCHES OF TOPSOIL, SALT TOLERANT SEED (CLASS 2A), APPLICABLE FERTILIZER APPLIED, AND PROTECTED BY EROSION CONTROL BLANKET. THIS WORK SHALL BE DONE IN ACCORDANCE WITH SECTIONS 211, 250, AND 251. THIS WORK SHALL BE INCLUDED IN THE COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT PAY ITEM.

INDEX OF SHEETS

SHEET NO.	TITLE
1	COVER SHEET
2	INDEX OF SHEETS, HIGHWAY STANDARDS, GENERAL NOTES, CONSTRUCTION STAGING AND DETAILS
3	SUMMARY OF QUANTITIES AND SCHEDULE OF QUANTITIES
4 - 6	TYPICAL SECTIONS AND HMA MIXTURE REQUIREMENTS
7 - 10	PLAN - CRYSTAL LAKE AVENUE
11	DISTRICT 1 DETAIL - BD-22 PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
12	DISTRICT 1 DETAIL - BD-24 CURB AND CURB AND GUTTER REMOVAL AND REPLACEMENT
13	DISTRICT 1 DETAIL - BD-32 BUTT JOINTS AND HMA TAPER DETAILS
14	DISTRICT 1 DETAIL - TC-10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
15	DISTRICT 1 DETAIL - TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS
16	DISTRICT 1 DETAIL - TC-16 PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
17	DISTRICT 1 DETAIL - TC-18 SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS
18	DISTRICT 1 DETAIL - TC-22 ARTERIAL ROAD INFORMATION SIGN

CONSTRUCTION STAGING

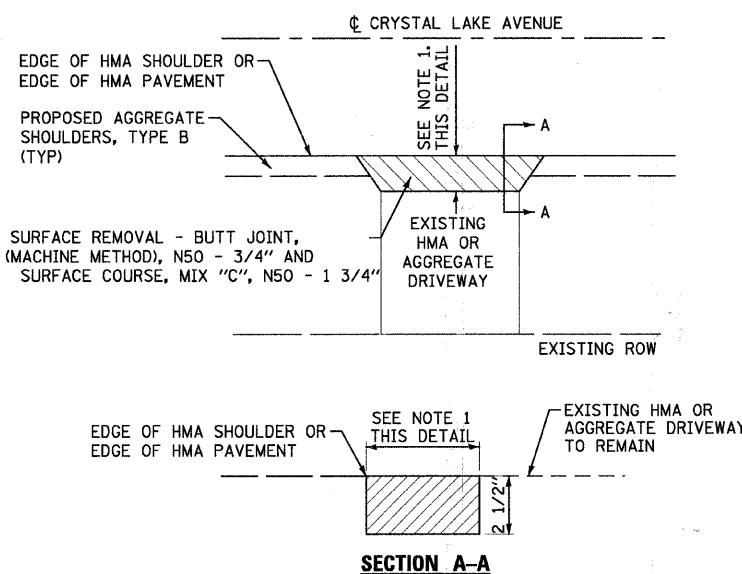
- BEGIN CONSTRUCTION ON OR BEFORE JUNE 22, 2009.
- COMPLETE MILLING, PATCHING, CURB AND GUTTER, RESURFACING AND AGGREGATE SHOULDER WORK.
- COMPLETION DATE AUGUST 21, 2009 (61 CALENDAR DAYS).
- COMPLETE THERMOPLASTIC PAVEMENT MARKING AND LANDSCAPING.
- WALK THROUGH INSPECTION.
- COMPLETE FINAL PUNCH LIST ITEMS.
- FINAL COMPLETION 10 WORKING DAYS AFTER COMPLETION DATE.

LEGEND

/////// BUTT JOINT OR DRIVEWAY BUTT JOINT

HIGHWAY STANDARDS

000001-05	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
406201-01	MAILBOX TURNOUT
442201-03	CLASS C AND D PATCHES
482011-03	HMA SHLD. STRIPS/SHLDS. WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701301-03	LANE CLOSURE, 2L 2W, SHORT TIME OPERATIONS
701501-05	URBAN LANE CLOSURE, 2L,2W, UNDIVIDED
701502-03	URBAN LANE CLOSURE, 2L,2W, WITH BIDIRECTIONAL LEFT TURN LANE
701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-04	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS
BLR-24-2	MAILBOX TURNOUT FOR LOCAL ROADS



- NOTES: 1. 5' MIN. WHEN MATCHING EXISTING HMA DRIVEWAY UNLESS OTHERWISE DIMENSIONED.
3' MIN. WHEN MATCHING EXISTING AGGREGATE DRIVEWAY UNLESS OTHERWISE DIMENSIONED.

HMA DRIVEWAY DETAIL

NO SCALE

VILLAGE OF CARY, ILLINOIS
CRYSTAL LAKE AVENUE
STP (LAPP) IMPROVEMENTS

INDEX OF SHEETS, HIGHWAY STANDARDS,
GENERAL NOTES, CONSTRUCTION STAGING AND DETAILS

SCALE: NONE

STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00055-00-RS	McHENRY	18	2
FED. ROAD DIST. NO. 1			CONTRACT NO. 63115	
JOB NO. C-91-202-09 ILLINOIS FED. AID PROJECT M-9003 (169)				



DESIGNED - MWP	REVISED - IDOT REVIEW 02/03/09
DRAWN - CJC	REVISED -
CHECKED - RWL	REVISED -
DATE - 10/29/08	FILE - 080936-gen-notes.shx

SUMMARY OF QUANTITIES

CODE NO.	PAY ITEM	UNIT	CONSTRUCTION CODE 1000
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	59
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	3,357
40600300	AGGREGATE (PRIME COAT)	TON	69
40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	631
40600895	CONSTRUCTING TEST STRIP	EACH	2
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	1,233
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	1,600
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	16,785
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	175
44201705	CLASS D PATCHES, TYPE II, 5 INCH	SQ YD	129
44201709	CLASS D PATCHES, TYPE III, 5 INCH	SQ YD	328
44201711	CLASS D PATCHES, TYPE IV, 5 INCH	SQ YD	735
44201718	CLASS D PATCHES, TYPE II, 6 1/2 INCH	SQ YD	82
44201721	CLASS D PATCHES, TYPE III, 6 1/2 INCH	SQ YD	199
44201724	CLASS D PATCHES, TYPE IV, 6 1/2 INCH	SQ YD	434
44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	108
44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	215
44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	394
44300100	AREA REFLECTIVE CRACK CONTROL TREATMENT	SQ YD	16,785
48101200	AGGREGATE SHOULDERS, TYPE B	TON	387
67100100	MOBILIZATION	LSUM	1

CODE NO.	PAY ITEM	UNIT	CONSTRUCTION CODE 1000
70101700	TRAFFIC CONTROL AND PROTECTION	LSUM	1
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	13,239
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1,486
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	293
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	18,052
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1,235
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1,032
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	78
X4420092	CLASS D PATCHES, TYPE II, 9 1/2 INCH	SQ YD	32
X4420093	CLASS D PATCHES, TYPE III, 9 1/2 INCH	SQ YD	95
X4420099	CLASS D PATCHES, TYPE IV, 9 1/2 INCH	SQ YD	190
X4420452	CLASS D PATCHES, TYPE II, 4 INCH	SQ YD	64
X4420454	CLASS D PATCHES, TYPE III, 4 INCH	SQ YD	128
X4420456	CLASS D PATCHES, TYPE IV, 4 INCH	SQ YD	234
△ Z0076600	TRAINEES	Hour	500

* SPECIALTY ITEMS

△ Y080

SCHEDULE OF QUANTITIES

HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

LOCATION	AREA (SQ YD)
CRYSTAL LAKE AVENUE (STA 13+45)	50
ELEMENTARY SCHOOL DRIVEWAY (STA 17+04.5)	125
ELEMENTARY SCHOOL DRIVEWAY (STA 19+90)	145
JR HIGH DRIVEWAY (STA 23+10)	200
GRAVEL DRIVEWAY (STA 26+18)	11
FIRE DEPARTMENT DRIVEWAY (STA 29+04)	94
HMA DRIVEWAY (STA 29+78)	11
FIRE DEPARTMENT EAST DRIVEWAY (STA 30+58)	27
FOXFORD HILLS MAINTENANCE DRIVEWAY (STA 31+02)	24
HMA DRIVEWAY (STA 32+36)	12
HMA DRIVEWAY (STA 34+39)	34
HMA SERVICE DRIVEWAY (STA 35+38)	20
FOXFORD DRIVE (STA 36+90)	80
MCCD PARKING LOT DRIVEWAY (STA 37+95)	54
BIKEPATH (STA 51+09)	296
CRYSTAL LAKE AVENUE (STA 53+30)	50
TOTAL	1,233

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 STATE OF ILLINOIS PROFESSIONAL ENGINEER
 LICENSE NO. 181-00172003
 4/8/08
 PROJECT NO. 08-0055-00-RS
 DRAWING NO. 4803C
 DATE: 10/29/08
 FILE: 080936-soq.shp



DESIGNED - MWP	REVISED - IDOT REVIEW 02/03/09
DRAWN - CJC	REVISED -
CHECKED - RWL	REVISED -
DATE - 10/29/08	FILE - 080936-soq.shp

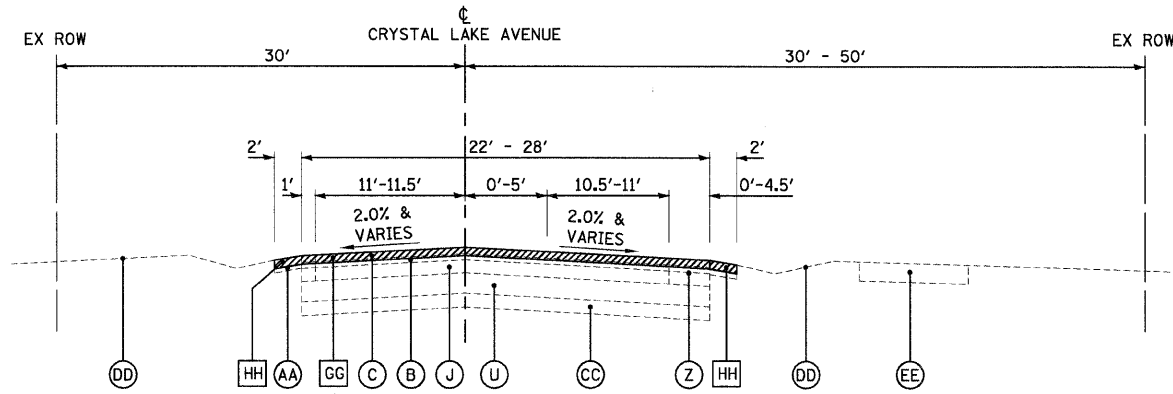
VILLAGE OF CARY, ILLINOIS
CRYSTAL LAKE AVENUE
STP (LAPP) IMPROVEMENTS

SUMMARY OF QUANTITIES AND
SCHEDULE OF QUANTITIES

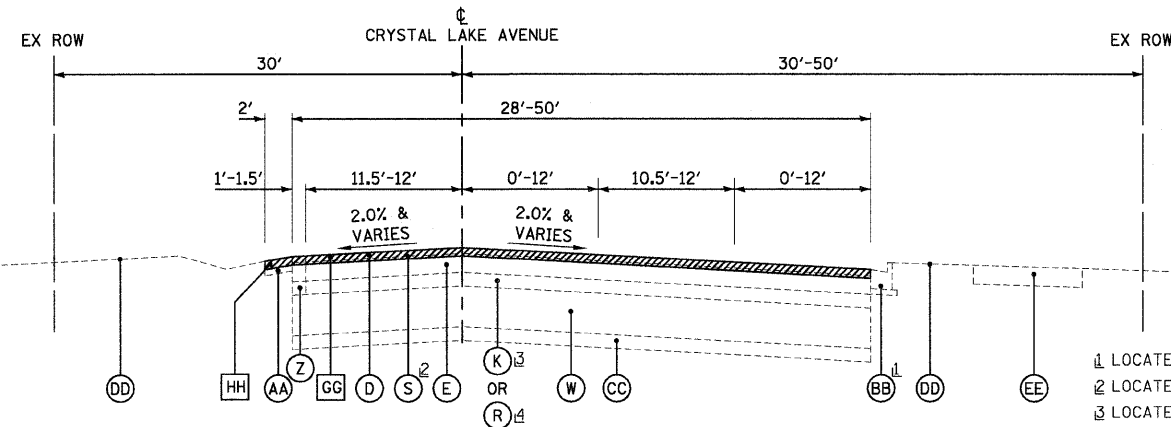
SCALE: NONE

STA. TO STA.

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0116	08-0055-00-RS	McHENRY	18	3
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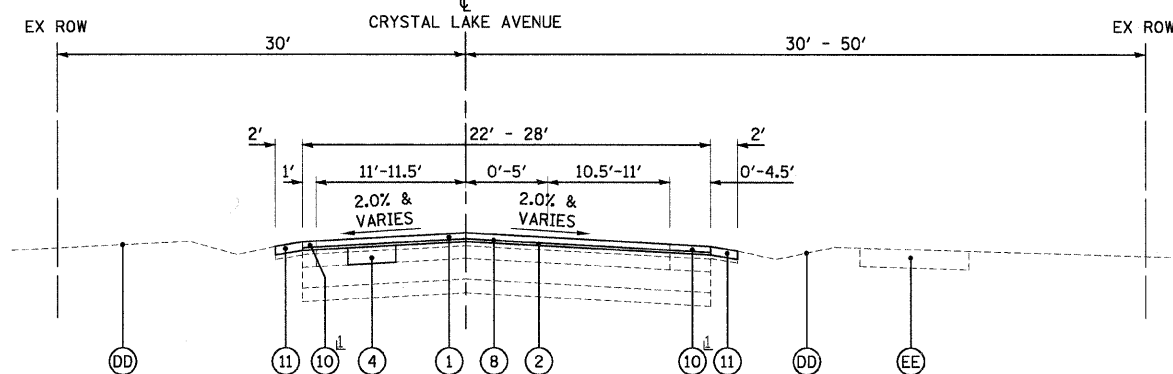


**EXISTING TYPICAL SECTION
CRYSTAL LAKE AVENUE
STA. 13+45 TO STA. 14+90**

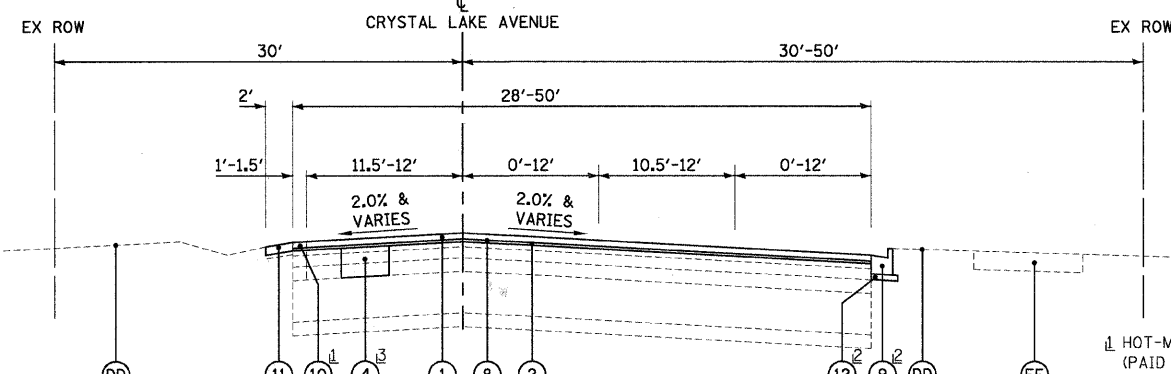


**EXISTING TYPICAL SECTION
CRYSTAL LAKE AVENUE
STA. 14+90 TO STA. 28+05**

- 1 LOCATED FROM STA. 16+54 TO STA. 19+50
- 2 LOCATED FROM STA. 20+12 TO STA. 28+05
- 3 LOCATED FROM STA. 20+12 TO STA. 24+09
- 4 LOCATED FROM STA. 24+09 TO STA. 28+05



**PROPOSED TYPICAL SECTION
CRYSTAL LAKE AVENUE
STA. 13+45 TO STA. 14+90**



**PROPOSED TYPICAL SECTION
CRYSTAL LAKE AVENUE
STA. 14+90 TO STA. 28+05**

- 1 HOT-MIX ASPHALT SHOULDERS (PAID FOR AS LEVELING BINDER (MACHINE METHOD), N50 - 3/4" AND HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 - 1 3/4")
- 2 LOCATED FROM STA. 16+54 TO STA. 19+50
- 3 LOCATED FROM STA. 14+90 TO STA. 20+12
- 4 LOCATED FROM STA. 20+12 TO STA. 24+09
- 5 LOCATED FROM STA. 24+09 TO STA. 28+05

1 HOT-MIX ASPHALT SHOULDERS (PAID FOR AS LEVELING BINDER (MACHINE METHOD), N50 - 3/4" AND HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 - 1 3/4")

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AC TYPE	VOIDS
RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL 9.5mm); 1 3/4"	PG 64-22	4% @ 50 GYR.
LEVELING BINDER (MACHINE METHOD), N50	PG 64-22*/ 58-22	4% @ 50 GYR.
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 mm); TYPE II - IV - 4-INCH, TYPE II - IV - 5-INCH, TYPE II - IV - 6 1/2-INCH, TYPE II - IV - 8-INCH, TYPE II - IV - 9 1/2-INCH	PG 64-22*/ 58-22	4% @ 70 GYR.
SHOULDERS		
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL 9.5mm); 1 3/4"	PG 64-22	4% @ 50 GYR.
LEVELING BINDER (MACHINE METHOD), N50; 3/4"	PG 64-22*/ 58-22	4% @ 50 GYR.
DRIVEWAYS		
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL 9.5mm); 1 3/4"	PG 64-22	4% @ 50 GYR.
LEVELING BINDER (MACHINE METHOD), N50; 3/4"	PG 64-22*/ 58-22	4% @ 50 GYR.

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ. YD./IN
 • WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22

EXISTING LEGEND

- (A) HOT-MIX ASPHALT SURFACE COURSE, 1 1/2"
- (B) HOT-MIX ASPHALT SURFACE COURSE, 1 3/4"
- (C) HOT-MIX ASPHALT SURFACE COURSE, 2"
- (D) HOT-MIX ASPHALT SURFACE COURSE, 1 3/4" - 2"
- (E) HOT-MIX ASPHALT SURFACE COURSE, 2 1/4" - 6"
- (F) HOT-MIX ASPHALT SURFACE COURSE, 2 3/4"
- (G) HOT-MIX ASPHALT SURFACE COURSE, 4 1/4" - 5 1/2"
- (H) HOT-MIX ASPHALT SURFACE COURSE, 3 1/4"
- (J) HOT-MIX ASPHALT SURFACE COURSE, 3 3/4"
- (K) HOT-MIX ASPHALT BINDER COURSE, 4"
- (L) HOT-MIX ASPHALT BASE COURSE, 9"
- (M) BITUMINOUS STABILIZED GRANULAR MATERIAL, 1 3/4"
- (N) BITUMINOUS STABILIZED GRANULAR MATERIAL, 2"
- (O) BITUMINOUS STABILIZED GRANULAR MATERIAL, 2 1/4"
- (P) BITUMINOUS STABILIZED GRANULAR MATERIAL, 2 3/4"
- (R) BITUMINOUS STABILIZED GRANULAR MATERIAL, 4 1/4"
- (S) AREA REFLECTIVE CRACK CONTROL TREATMENT (PETROMAT)

EXISTING LEGEND (CONT.)

- (T) CRUSHED GRAVEL, 5 1/2"
- (U) CRUSHED AND UNCRUSHED GRAVEL, 4 1/2"
- (V) CRUSHED AND UNCRUSHED GRAVEL, 7 1/2" - 8 1/2"
- (W) CRUSHED AND UNCRUSHED GRAVEL, 12" - 15"
- (X) UNCRUSHED GRAVEL AND SAND, 10 1/2"
- (Y) UNCRUSHED GRAVEL AND SAND, 16"
- (Z) HOT-MIX ASPHALT SHOULDER (FULL DEPTH)
- (AA) AGGREGATE SHOULDER, 4"±
- (BB) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (CC) EXISTING SUB-GRADE
- (DD) GROUND SURFACE
- (EE) HMA BIKE PATH
- (FF) CENTERLINE HMA DITCH
- (GG) HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- (HH) AGGREGATE SHOULDER REMOVAL, 2 1/2" (NOT PAID FOR SEPARATELY)
- (I) ITEM TO BE REMOVED

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 - 1 3/4"
- (2) LEVELING BINDER (MACHINE METHOD), N50 - 3/4" MIN.
- (3) CLASS D PATCHES, TYPE II - IV, 4-INCH
- (4) CLASS D PATCHES, TYPE II - IV, 5-INCH
- (5) CLASS D PATCHES, TYPE II - IV, 6 1/2-INCH
- (6) CLASS D PATCHES, TYPE II - IV, 8-INCH
- (7) CLASS D PATCHES, TYPE II - IV, 9 1/2-INCH
- (8) AREA REFLECTIVE CRACK CONTROL TREATMENT
- (9) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- (10) HOT-MIX ASPHALT SHOULDERS
- (11) AGGREGATE SHOULDERS, TYPE B - 2 1/2"
- (12) AGGREGATE BASE COURSE, TYPE B - 4"
- (13) CENTERLINE HMA DITCH

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CHECKED - RWL	REVISED -
DATE - 10/29/08	FILE - 080936-TYP.sht

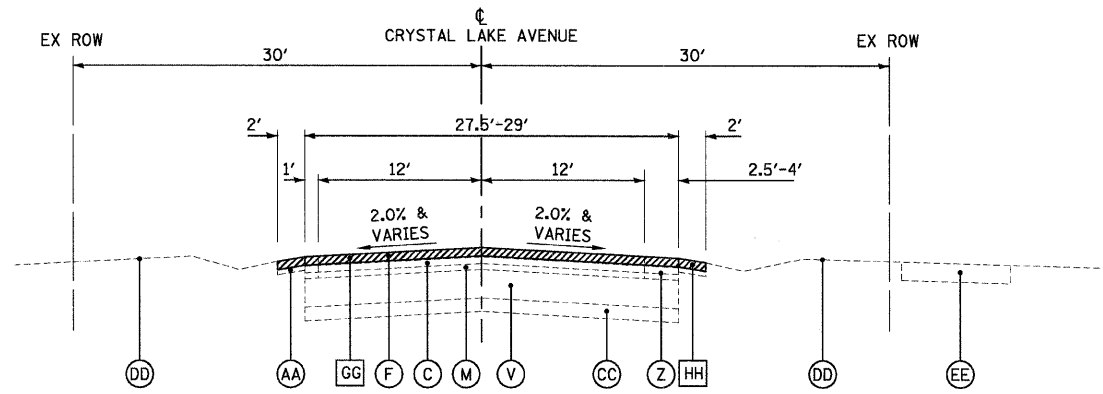
**VILLAGE OF CARY, ILLINOIS
CRYSTAL LAKE AVENUE
STP (LAPP) IMPROVEMENTS**

**TYPICAL SECTIONS AND
HMA MIXTURE REQUIREMENTS**

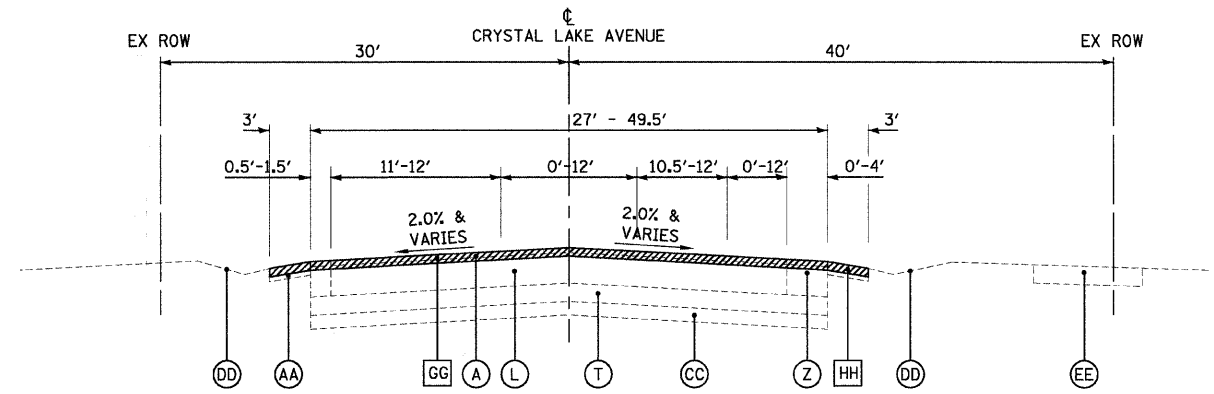
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STA. TO STA.

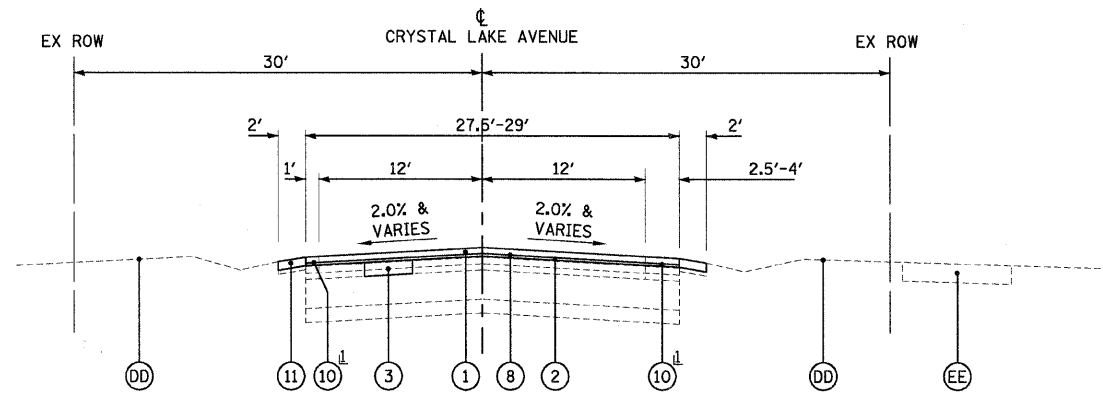
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FED. ROAD DIST. NO. 1			CONTRACT NO. 63115	
JOB NO. C-91-202-09 ILLINOIS FED. AID PROJECT M-9003 (169)				



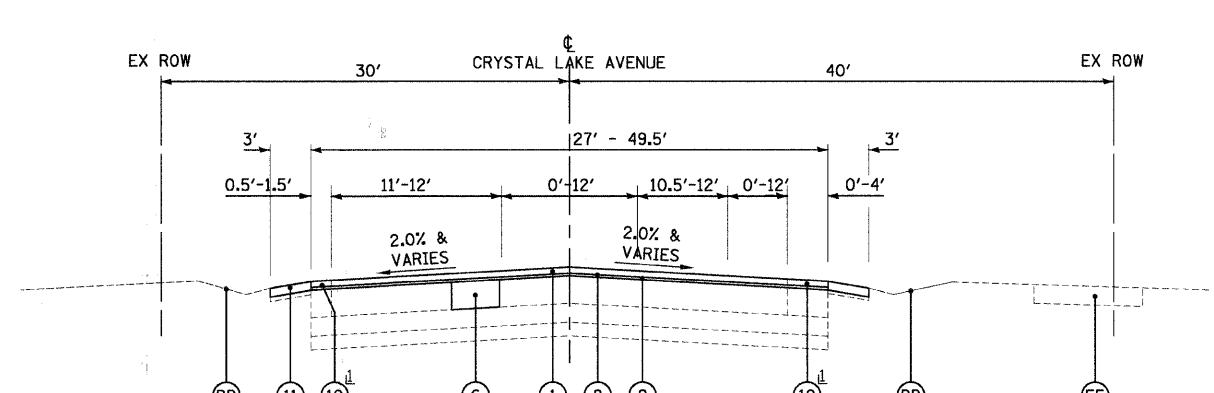
**EXISTING TYPICAL SECTION
CRYSTAL LAKE AVENUE
STA. 28+05 TO STA. 34+14**



**EXISTING TYPICAL SECTION
CRYSTAL LAKE AVENUE
STA. 34+14 TO STA. 41+86**



**PROPOSED TYPICAL SECTION
CRYSTAL LAKE AVENUE
STA. 28+05 TO STA. 34+14**



**PROPOSED TYPICAL SECTION
CRYSTAL LAKE AVENUE
STA. 34+14 TO STA. 41+86**

↓ HOT-MIX ASPHALT SHOULDERS
(PAID FOR AS LEVELING BINDER (MACHINE METHOD), N50 - 3/4" AND HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 - 1 3/4")

↓ HOT-MIX ASPHALT SHOULDERS
(PAID FOR AS LEVELING BINDER (MACHINE METHOD), N50 - 3/4" AND HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 - 1 3/4")

EXISTING LEGEND

- (A) HOT-MIX ASPHALT SURFACE COURSE, 1 1/2"
- (B) HOT-MIX ASPHALT SURFACE COURSE, 1 3/4"
- (C) HOT-MIX ASPHALT SURFACE COURSE, 2"
- (D) HOT-MIX ASPHALT SURFACE COURSE, 1 3/4" - 2"
- (E) HOT-MIX ASPHALT SURFACE COURSE, 2 1/4" - 6"
- (F) HOT-MIX ASPHALT SURFACE COURSE, 2 3/4"
- (G) HOT-MIX ASPHALT SURFACE COURSE, 4 1/4" - 5 1/2"
- (H) HOT-MIX ASPHALT SURFACE COURSE, 3 1/4"
- (J) HOT-MIX ASPHALT SURFACE COURSE, 3 3/4"
- (K) HOT-MIX ASPHALT BINDER COURSE, 4"
- (L) HOT-MIX ASPHALT BASE COURSE, 9"
- (M) BITUMINOUS STABILIZED GRANULAR MATERIAL, 1 3/4"
- (N) BITUMINOUS STABILIZED GRANULAR MATERIAL, 2"
- (O) BITUMINOUS STABILIZED GRANULAR MATERIAL, 2 1/4"
- (P) BITUMINOUS STABILIZED GRANULAR MATERIAL, 2 3/4"
- (R) BITUMINOUS STABILIZED GRANULAR MATERIAL, 4 1/4"
- (S) AREA REFLECTIVE CRACK CONTROL TREATMENT (PETROMAT)

EXISTING LEGEND (CONT.)

- (T) CRUSHED GRAVEL, 5 1/2"
- (U) CRUSHED AND UNCRUSHED GRAVEL, 4 1/2"
- (V) CRUSHED AND UNCRUSHED GRAVEL, 7 1/2" - 8 1/2"
- (W) CRUSHED AND UNCRUSHED GRAVEL, 12" - 15"
- (X) UNCRUSHED GRAVEL AND SAND, 10 1/2"
- (Y) UNCRUSHED GRAVEL AND SAND, 16"
- (Z) HOT-MIX ASPHALT SHOULDER (FULL DEPTH)
- (AA) AGGREGATE SHOULDER, 4"±
- (BB) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (CC) EXISTING SUB-GRADE
- (DD) GROUND SURFACE
- (EE) HMA BIKE PATH
- (FF) CENTERLINE HMA DITCH
- (GG) HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- (HH) AGGREGATE SHOULDER REMOVAL, 2 1/2"
- (NOT PAID FOR SEPARATELY)
- ▨ ITEM TO BE REMOVED

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 - 1 3/4"
- (2) LEVELING BINDER (MACHINE METHOD), N50 - 3/4" MIN.
- (3) CLASS D PATCHES, TYPE II - IV, 4-INCH
- (4) CLASS D PATCHES, TYPE II - IV, 5-INCH
- (5) CLASS D PATCHES, TYPE II - IV, 6 1/2-INCH
- (6) CLASS D PATCHES, TYPE II - IV, 8-INCH
- (7) CLASS D PATCHES, TYPE II - IV, 9 1/2-INCH
- (8) AREA REFLECTIVE CRACK CONTROL TREATMENT
- (9) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- (10) HOT-MIX ASPHALT SHOULDERS
- (11) AGGREGATE SHOULDERS, TYPE B - 2 1/2"
- (12) AGGREGATE BASE COURSE, TYPE B - 4"
- (13) CENTERLINE HMA DITCH

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CHECKED - RWL	REVISED -
DATE - 10/29/08	FILE - 080936-TYP.sht

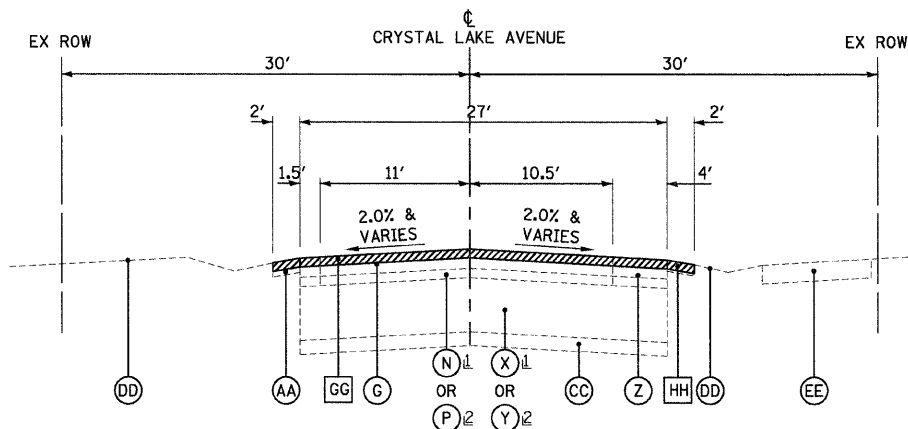
**VILLAGE OF CARY, ILLINOIS
CRYSTAL LAKE AVENUE
STP (LAPP) IMPROVEMENTS**

TYPICAL SECTIONS

SCALE: NONE

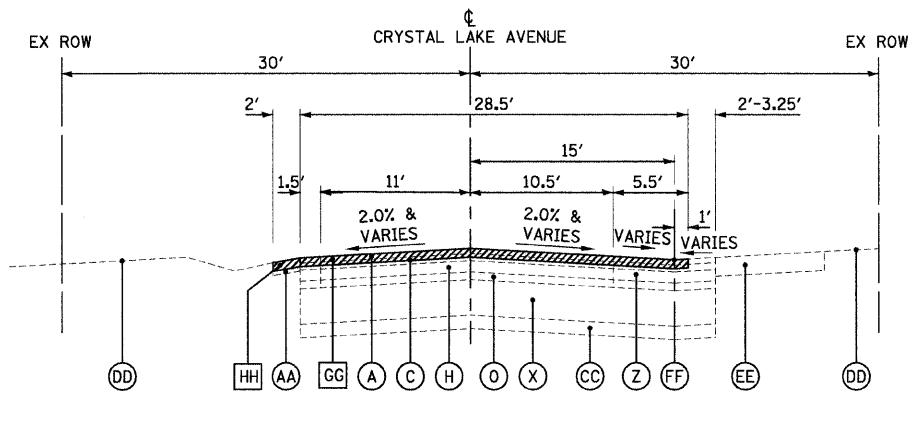
STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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JOB NO. C-91-202-09 ILLINOIS FED. AID PROJECT M-9003 (169)				

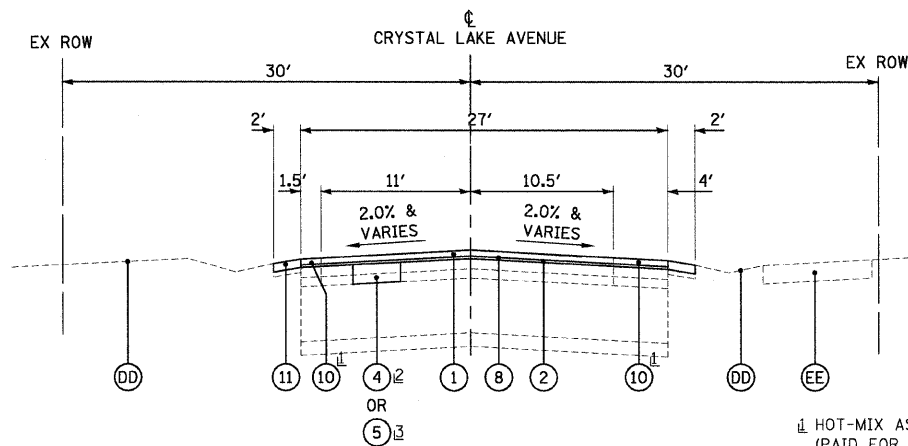


**EXISTING TYPICAL SECTION
CRYSTAL LAKE AVENUE
STA. 41+86 TO STA. 48+87**

1 LOCATED FROM STA. 41+86 TO STA. 48+05
2 LOCATED FROM STA. 48+05 TO STA. 48+87

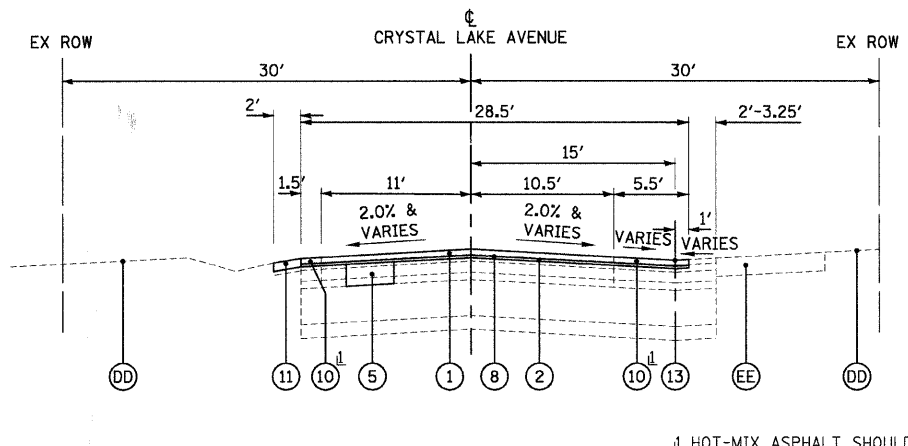


**EXISTING TYPICAL SECTION
CRYSTAL LAKE AVENUE
STA. 48+87 TO STA. 53+30**



**PROPOSED TYPICAL SECTION
CRYSTAL LAKE AVENUE
STA. 41+86 TO STA. 48+87**

1 HOT-MIX ASPHALT SHOULDERS
(PAID FOR AS LEVELING BINDER (MACHINE METHOD), N50 - 3/4" AND HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 - 1 3/4")
2 LOCATED FROM STA. 41+86 TO STA. 48+05
3 LOCATED FROM STA. 48+05 TO STA. 48+87



**PROPOSED TYPICAL SECTION
CRYSTAL LAKE AVENUE
STA. 48+87 TO STA. 53+30**

1 HOT-MIX ASPHALT SHOULDERS
(PAID FOR AS LEVELING BINDER (MACHINE METHOD), N50 - 3/4" AND HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 - 1 3/4")

EXISTING LEGEND

- (A) HOT-MIX ASPHALT SURFACE COURSE, 1 1/2"
- (B) HOT-MIX ASPHALT SURFACE COURSE, 1 3/4"
- (C) HOT-MIX ASPHALT SURFACE COURSE, 2"
- (D) HOT-MIX ASPHALT SURFACE COURSE, 1 3/4" - 2"
- (E) HOT-MIX ASPHALT SURFACE COURSE, 2 1/4" - 6"
- (F) HOT-MIX ASPHALT SURFACE COURSE, 2 3/4"
- (G) HOT-MIX ASPHALT SURFACE COURSE, 4 1/4" - 5 1/2"
- (H) HOT-MIX ASPHALT SURFACE COURSE, 3 1/4"
- (J) HOT-MIX ASPHALT SURFACE COURSE, 3 3/4"
- (K) HOT-MIX ASPHALT BINDER COURSE, 4"
- (L) HOT-MIX ASPHALT BASE COURSE, 9"
- (M) BITUMINOUS STABILIZED GRANULAR MATERIAL, 1 3/4"
- (N) BITUMINOUS STABILIZED GRANULAR MATERIAL, 2"
- (O) BITUMINOUS STABILIZED GRANULAR MATERIAL, 2 1/4"
- (P) BITUMINOUS STABILIZED GRANULAR MATERIAL, 2 3/4"
- (R) BITUMINOUS STABILIZED GRANULAR MATERIAL, 4 1/4"
- (S) AREA REFLECTIVE CRACK CONTROL TREATMENT (PETROMAT)

EXISTING LEGEND (CONT.)

- (T) CRUSHED GRAVEL, 5 1/2"
- (U) CRUSHED AND UNCRUSHED GRAVEL, 4 1/2"
- (V) CRUSHED AND UNCRUSHED GRAVEL, 7 1/2" - 8 1/2"
- (W) CRUSHED AND UNCRUSHED GRAVEL, 12" - 15"
- (X) UNCRUSHED GRAVEL AND SAND, 10 1/2"
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- (Z) HOT-MIX ASPHALT SHOULDER (FULL DEPTH)
- (AA) AGGREGATE SHOULDER, 4"±
- (BB) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (CC) EXISTING SUB-GRADE
- (DD) GROUND SURFACE
- (EE) HMA BIKE PATH
- (FF) CENTERLINE HMA DITCH
- (GG) HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- (HH) AGGREGATE SHOULDER REMOVAL, 2 1/2" (NOT PAID FOR SEPARATELY)
- (I) ITEM TO BE REMOVED

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 - 1 3/4"
- (2) LEVELING BINDER (MACHINE METHOD), N50 - 3/4" MIN.
- (3) CLASS D PATCHES, TYPE II - IV, 4-INCH
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- (5) CLASS D PATCHES, TYPE II - IV, 6 1/2-INCH
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- (8) AREA REFLECTIVE CRACK CONTROL TREATMENT
- (9) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- (10) HOT-MIX ASPHALT SHOULDERS
- (11) AGGREGATE SHOULDERS, TYPE B - 2 1/2"
- (12) AGGREGATE BASE COURSE, TYPE B - 4"
- (13) CENTERLINE HMA DITCH

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DATE - 10/29/08	FILE - 080936-TYP.shd

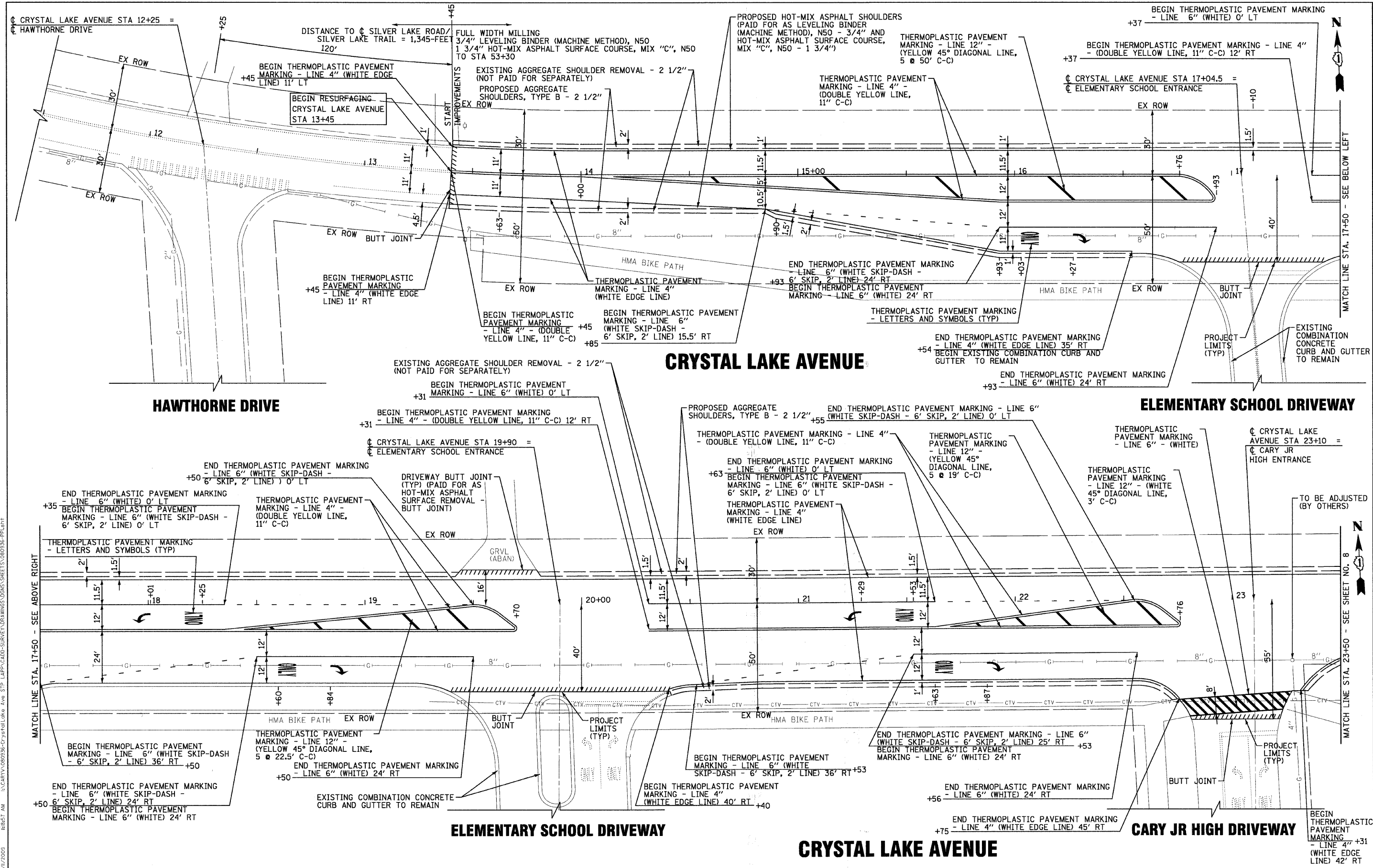
**VILLAGE OF CARY, ILLINOIS
CRYSTAL LAKE AVENUE
STP (LAPP) IMPROVEMENTS**

TYPICAL SECTIONS

SCALE: NONE

STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00055-00-RS	McHENRY	18	6
FED. ROAD DIST. NO. 1		CONTRACT NO. 63115		
JOB NO. C-91-202-09 ILLINOIS FED. AID PROJECT M-9003 (1169)				



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 PROJECT NO. 08-00055-00-RS
 SHEET NO. 18 OF 18



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DATE - 10/29/08	FILE - 080936-PP1.sht

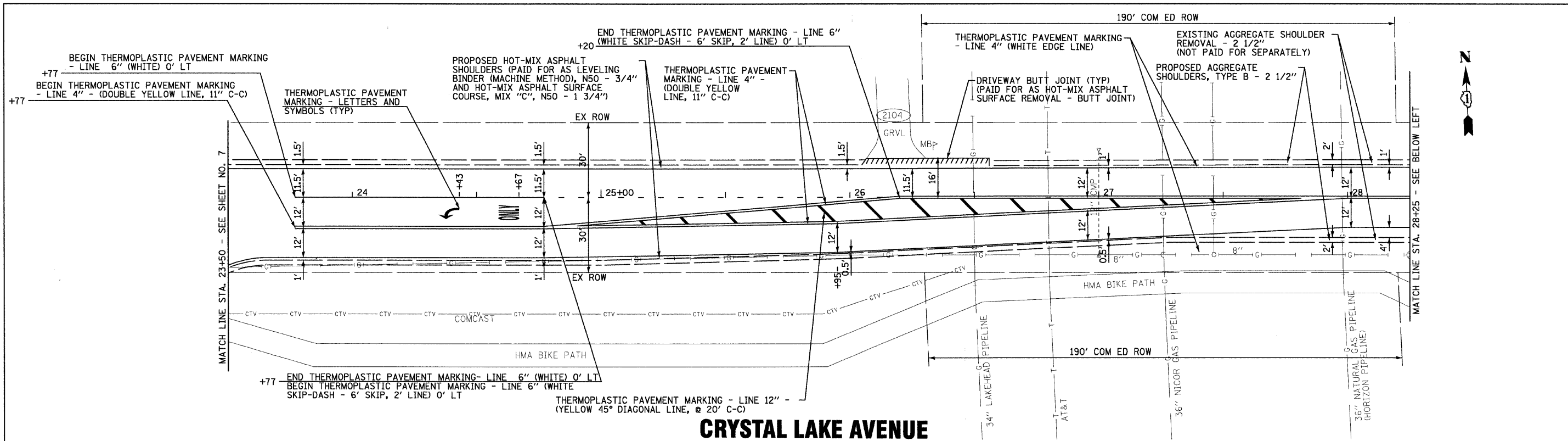
**VILLAGE OF CARY, ILLINOIS
CRYSTAL LAKE AVENUE
STP (LAPP) IMPROVEMENTS**

CRYSTAL LAKE AVENUE

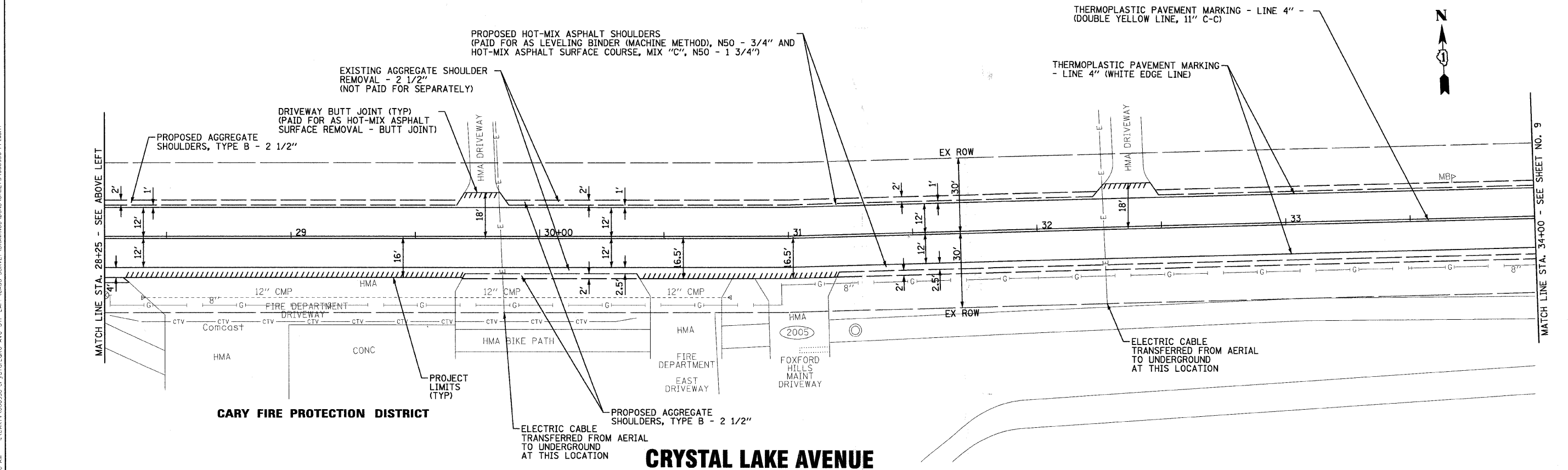
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STA. 11+50 TO STA. 23+50

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FED. ROAD DIST. NO. 1		CONTRACT NO. 63115		
JOB NO. C-91-202-09 ILLINOIS FED. AID PROJECT M-9003 (I69)				



CRYSTAL LAKE AVENUE



CRYSTAL LAKE AVENUE

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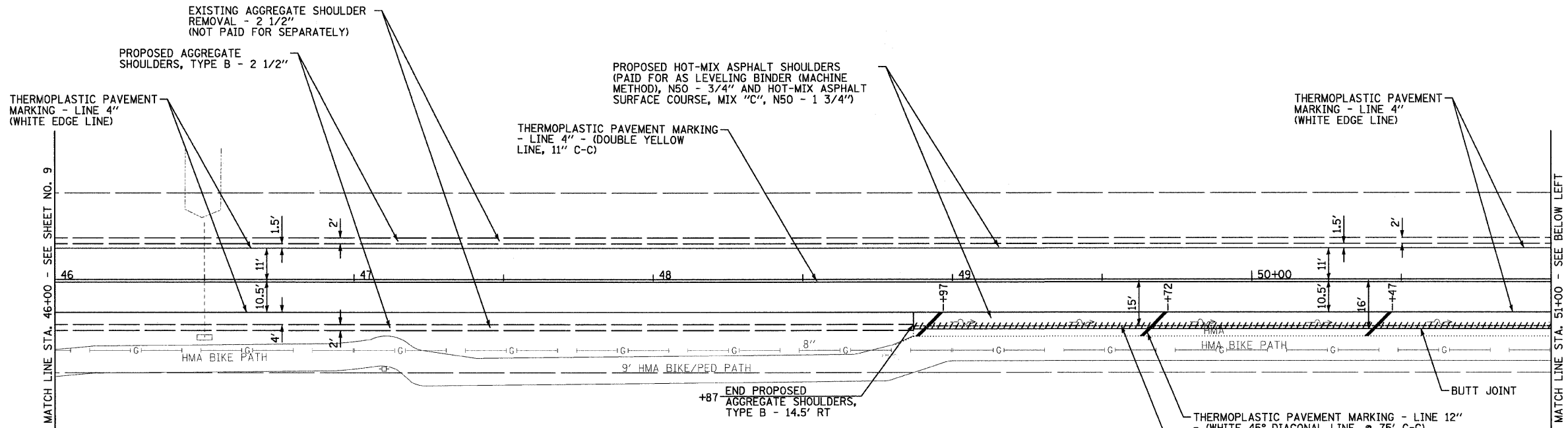


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**VILLAGE OF CARY, ILLINOIS
CRYSTAL LAKE AVENUE
STP (LAPP) IMPROVEMENTS**

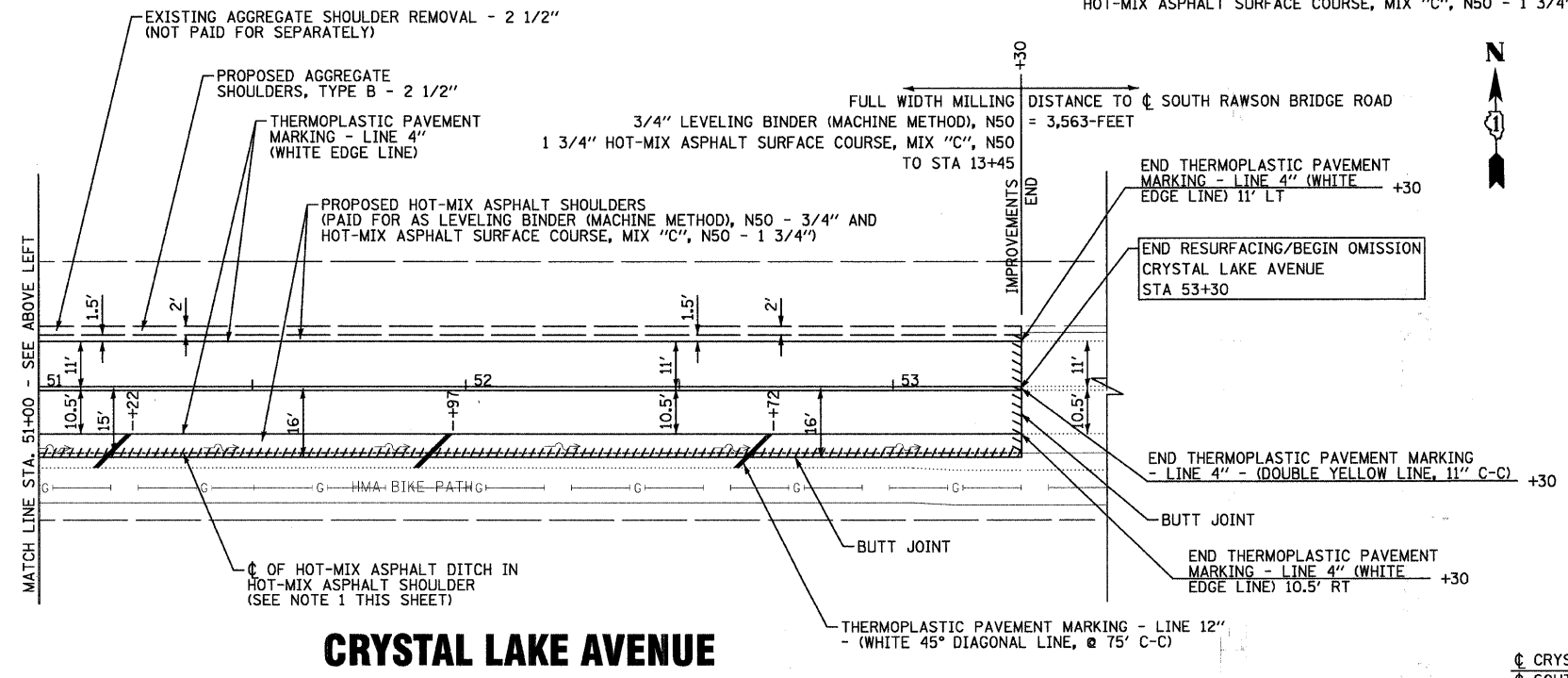
CRYSTAL LAKE AVENUE	
SCALE: 1" = 20'	STA. 23+50 TO STA. 34+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00055-00-RS	McHENRY	18	8
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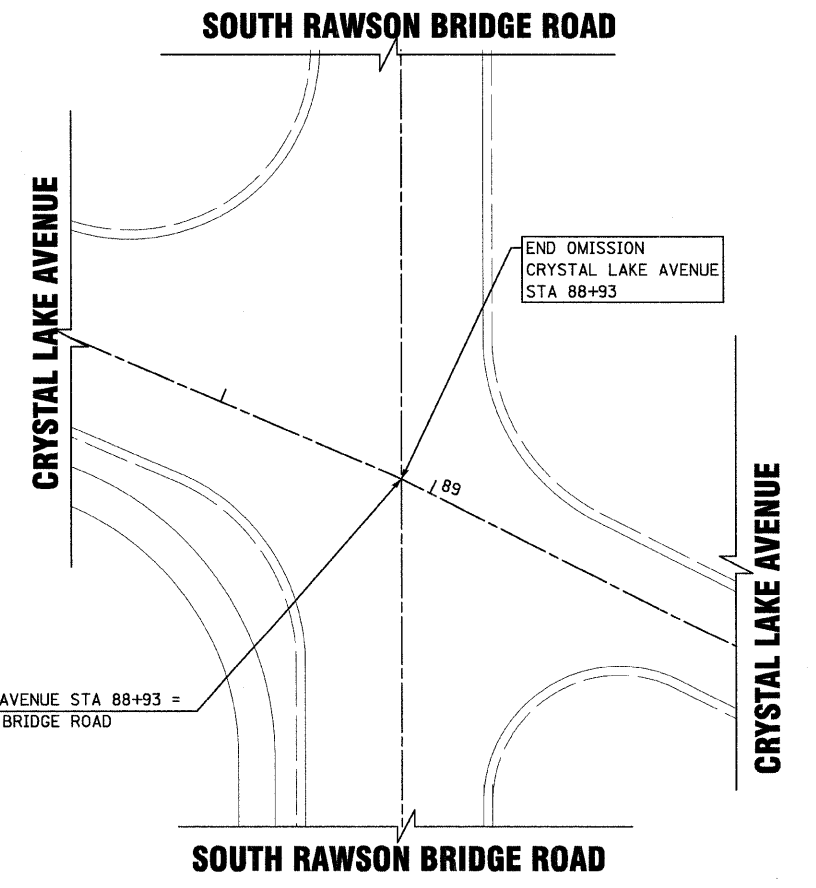


CRYSTAL LAKE AVENUE

- NOTES:**
1. THE CONTRACTOR IS RESPONSIBLE TO REMOVE AND REPLACE (MATCH EXISTING HORIZONTALLY AND VERTICALLY) THE EXISTING HOT-MIX ASPHALT DITCH IN THE HOT-MIX ASPHALT SHOULDER. (PAID FOR AS HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2", LEVELING BINDER (MACHINE METHOD), N50 - 3/4" AND HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 - 1 3/4")



CRYSTAL LAKE AVENUE



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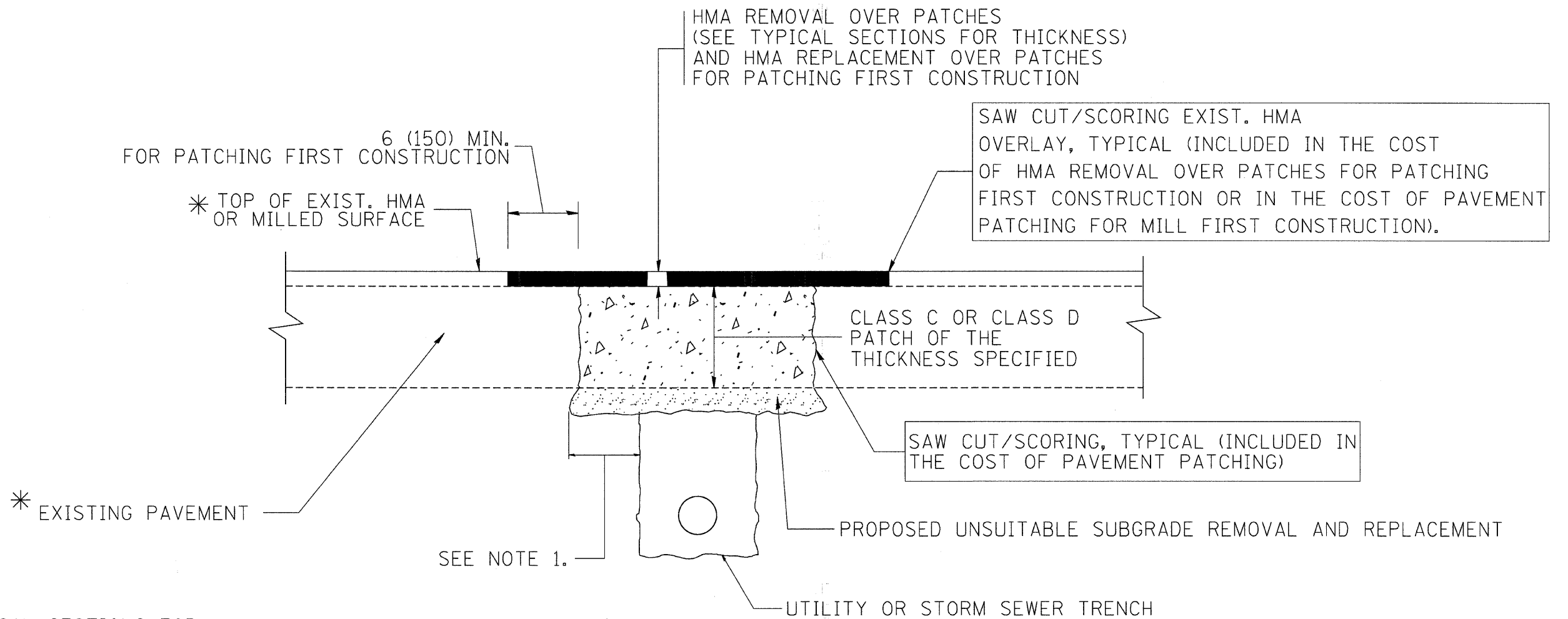


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DATE - 10/29/08	FILE - 080936-PP4.dht

**VILLAGE OF CARY, ILLINOIS
CRYSTAL LAKE AVENUE
STP (LAPP) IMPROVEMENTS**

CRYSTAL LAKE AVENUE
 SCALE: 1" = 20'
 STA. 46+00 TO STA. 53+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00055-00-RS	McHENRY	18	10
FED. ROAD DIST. NO. 1			CONTRACT NO. 63115	
JOB NO. C-91-202-09 ILLINOIS FED. AID PROJECT M-9003 (169)				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

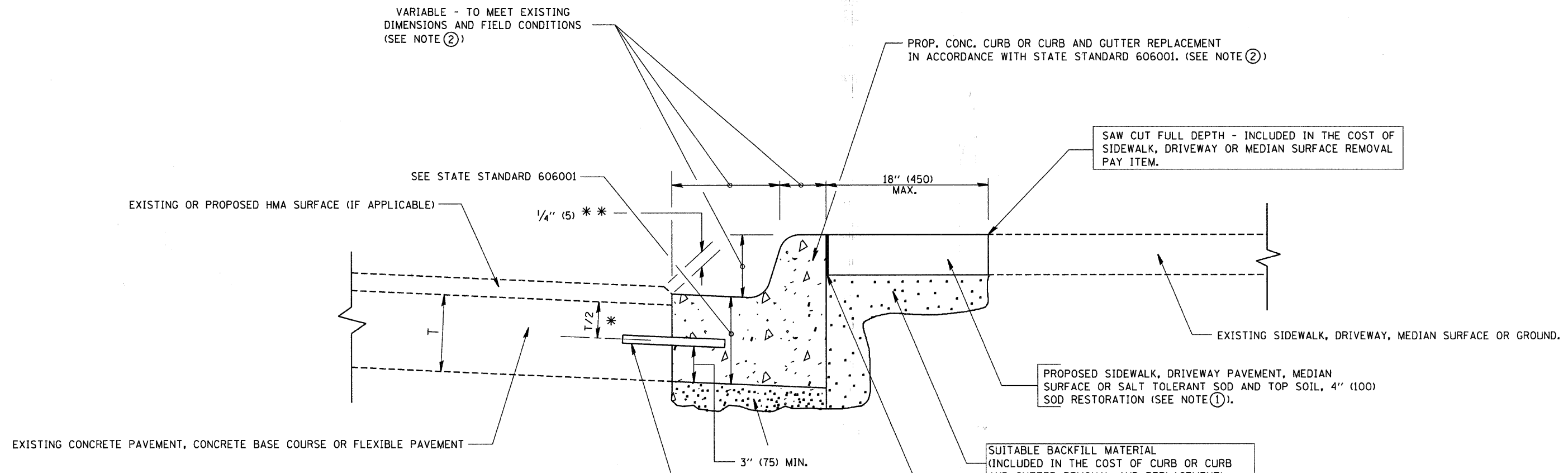
SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

JOB No. C-91-202-09

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\dststd22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.D. RTE. 0116	SECTION 08-00055-00-RS	COUNTY McHENRY	TOTAL SHEETS 18	SHEET NO. 11
	PLOT SCALE = 50.000' / IN.	DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BD400-04 (BD-22)		CONTRACT NO. 63115	
	PLOT DATE = 10/27/2008	CHECKED -	REVISED - R. BORO 09-04-07									
		DATE - 10-25-94	REVISED - K. ENG 10-27-08									



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- * * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

- ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

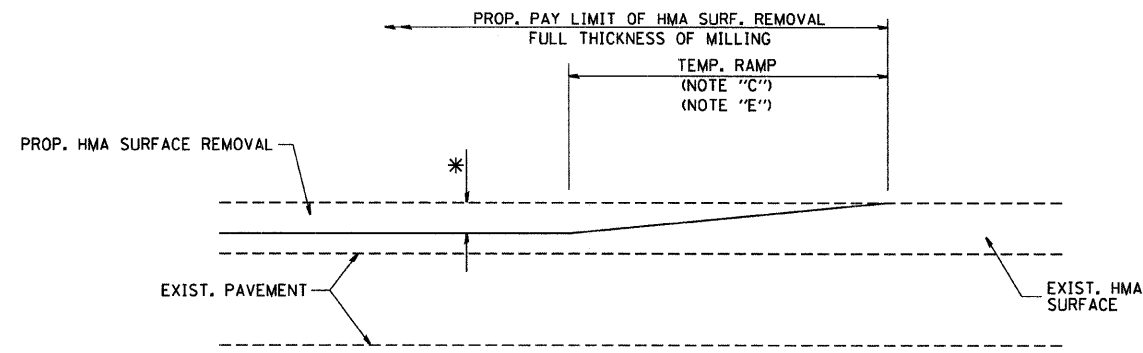
PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

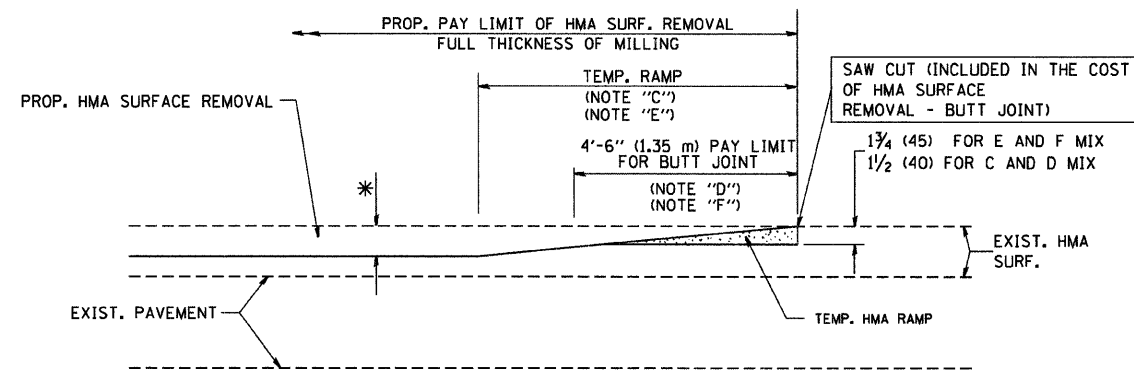
JOB No. C-91-202-09
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS				
		CHECKED -	REVISED - M. GOMEZ 01-22-01			STA.				
		DATE - 03-11-94	REVISED - R. BORO 01-01-07			TO STA.				
						BD600-06 (BD-24)		CONTRACT NO. 63115		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003 (169)



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

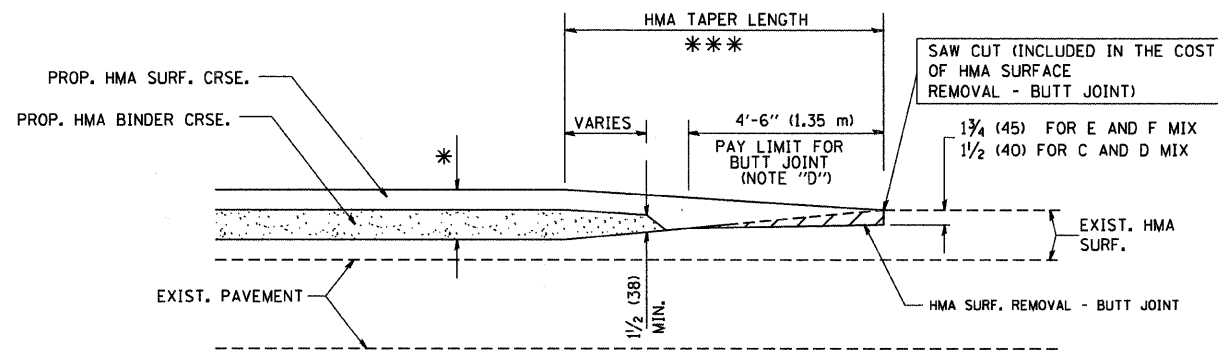
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

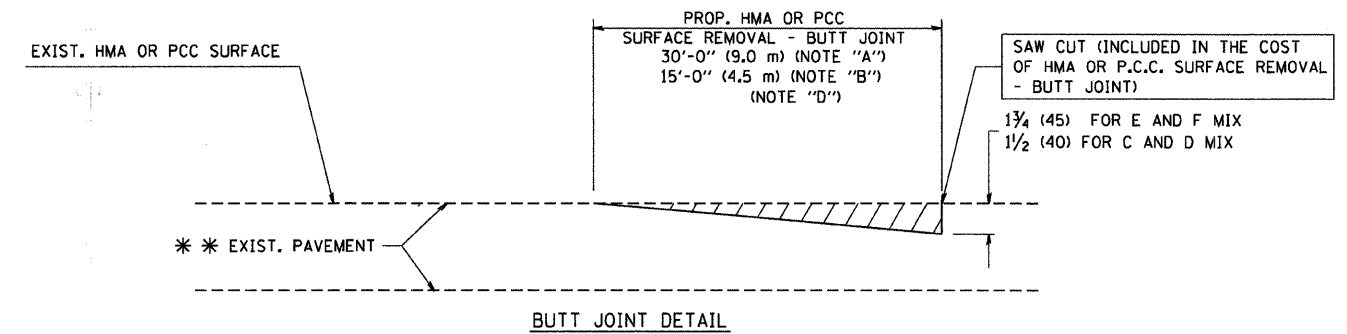
OPTION 2

TYPICAL TEMPORARY RAMP

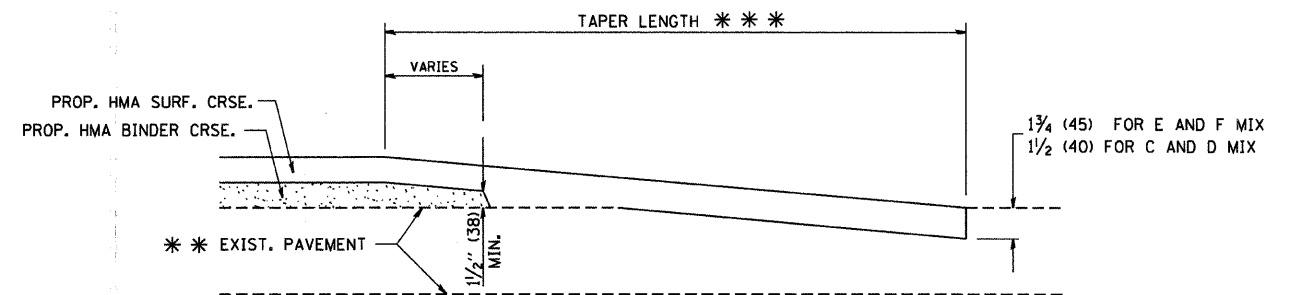


BUTT JOINT AND
HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- ** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

JOB No. C-91-202-09
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

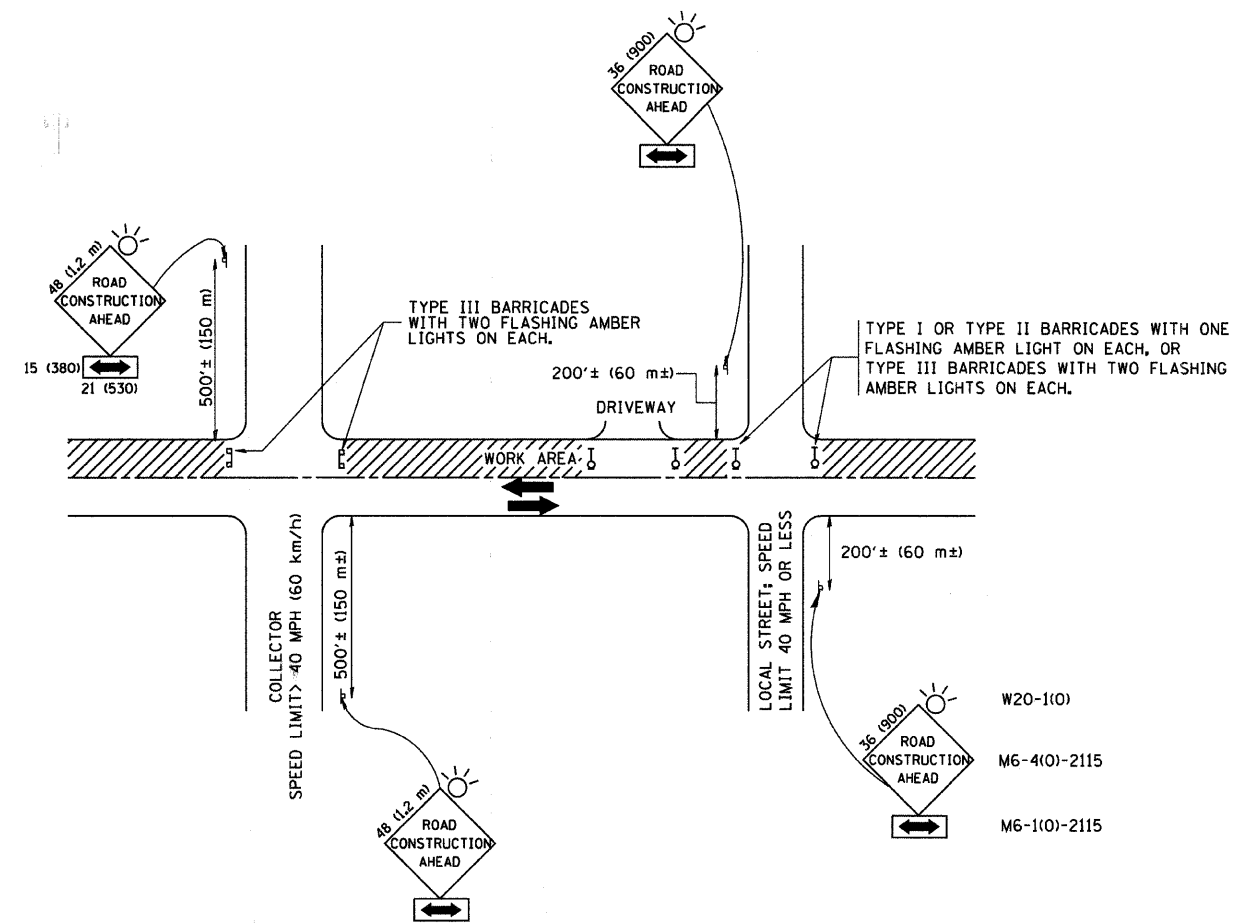
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	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.D. RTE. 0116	SECTION 08-00055-00-RS	COUNTY McHENRY	TOTAL SHEETS 18	SHEET NO. 13
BD400-05 BD32			CONTRACT NO. 63115	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003 (169)				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

JOB No. C-91-202-09
All dimensions are in millimeters (inches) unless otherwise shown.

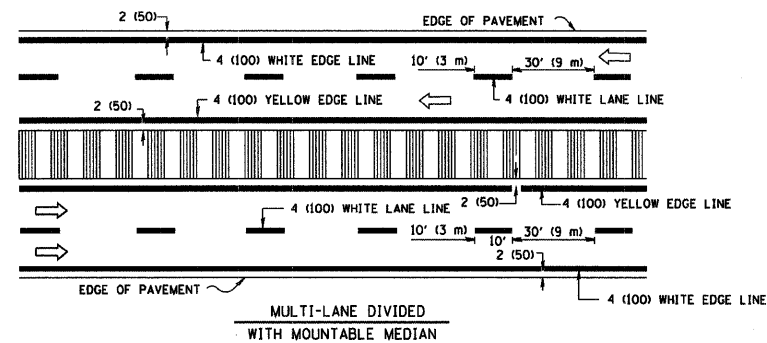
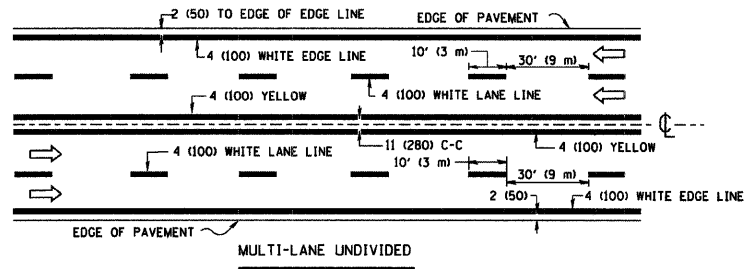
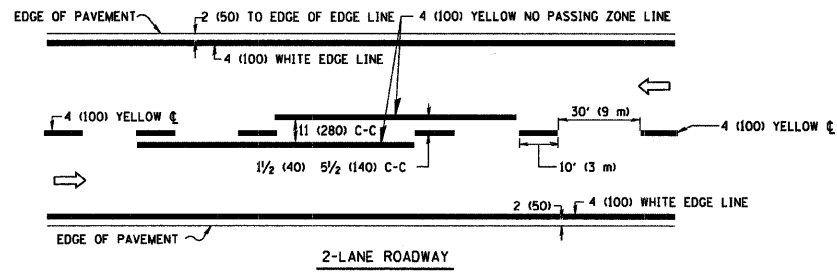
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	PLOT SCALE = 50,000 / / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

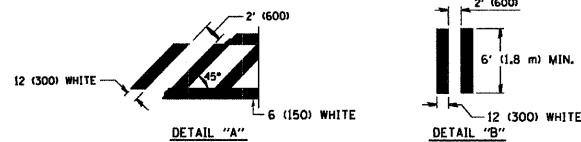
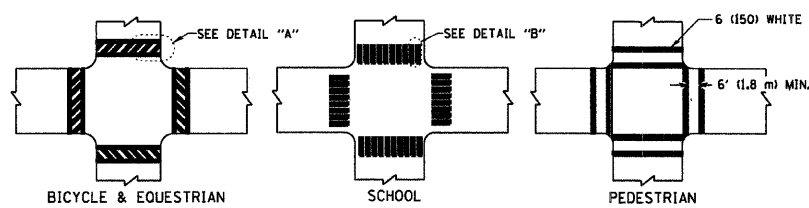
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F.A.U. RTE. 0116	SECTION 08-00053-00-RS	COUNTY McHENRY	TOTAL SHEETS 18	SHEET NO. 14
TC-10			CONTRACT NO. 63115	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003 (169)				

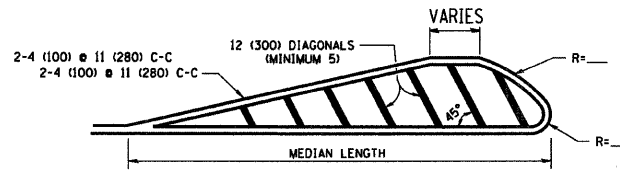
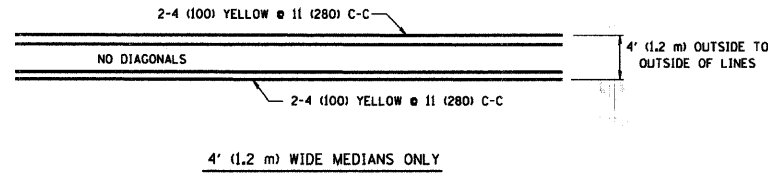


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



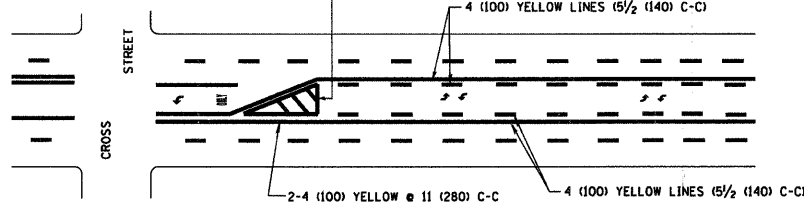
TYPICAL CROSSWALK MARKING



FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

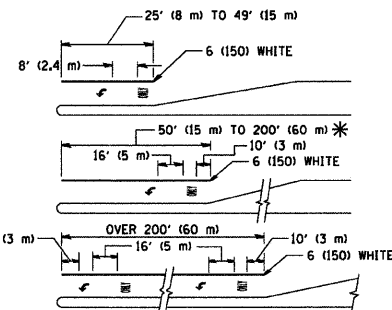


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

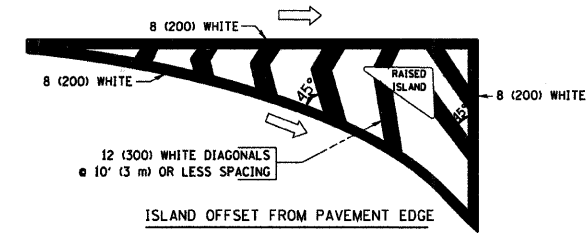


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
* AREA = 15.6 SQ. FT. (1.5 m²)

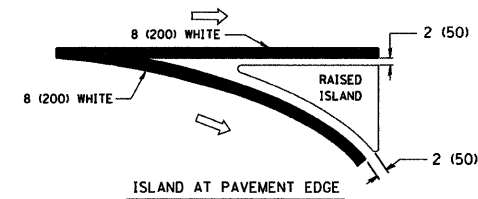
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS 8' (2.4m)	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID	WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

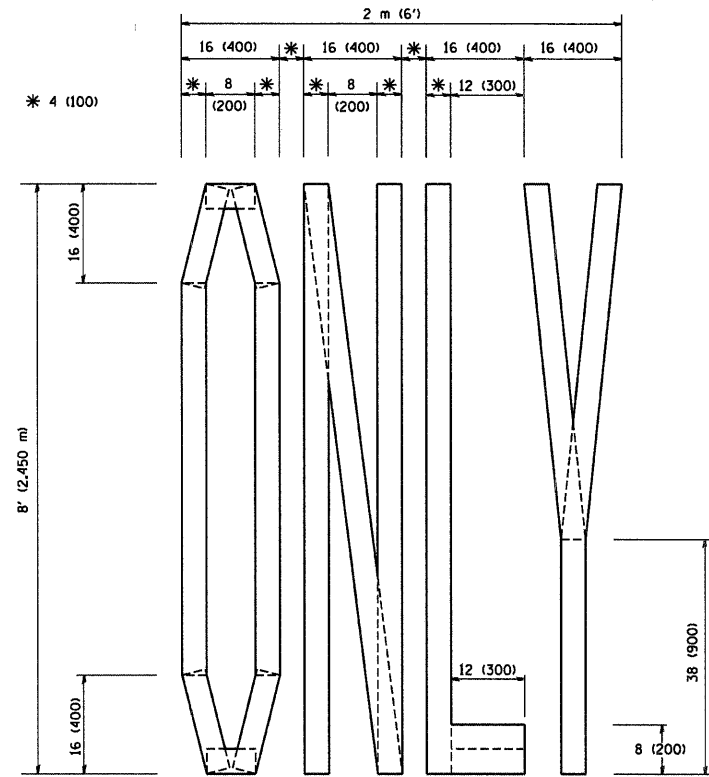
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	PLOT DATE = 1/4/2008	DATE - 03-19-90	REVISED - T. RAMMACHER 01-06-00

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

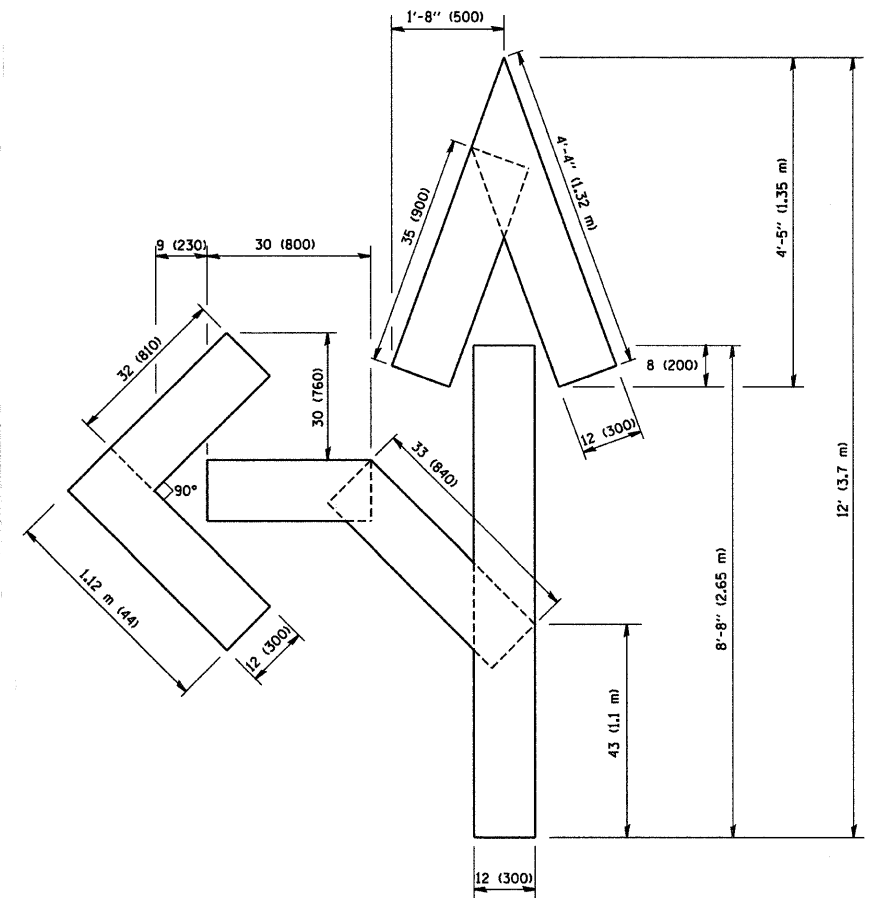
DISTRICT ONE TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. R.T.E. 0116	SECTION 08-00055-00-RS	COUNTY McHENR	TOTAL SHEETS 18	SHEET NO. 15
TC-13			CONTRACT NO. 63115	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT N-9003(169)				

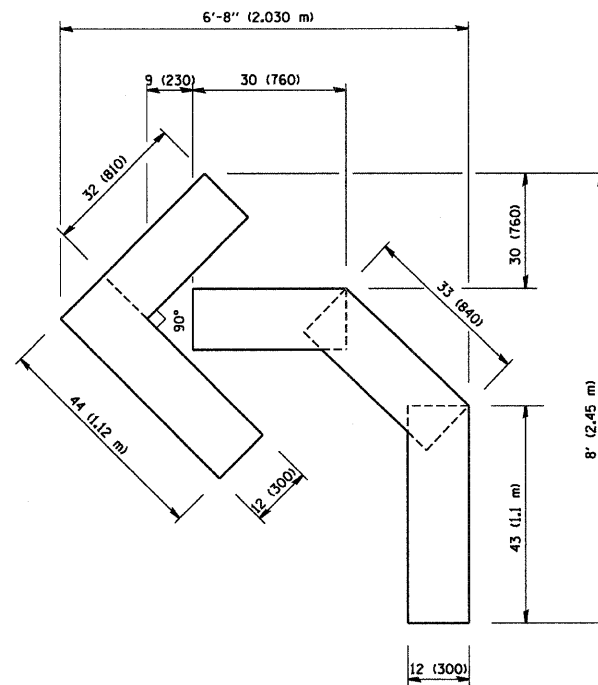
JOB No. C-91-202-09



QUANTITY
4 (100) LINE = 64.1 ft. (19.7 m)
21.1 sq. ft. (1.97 sq. m)



QUANTITY
4 (100) LINE = 82.5 ft. (25.3 m)
27.5 sq. ft. (2.53 sq. m)



QUANTITY
4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.39 sq. m)

JOB No. C-91-202-09
All dimensions are in inches (millimeters)
unless otherwise shown.

FILE NAME = W:\diststd\22x34\vc16.dgn	USER NAME = gegl.enobt	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
		DRAWN -	REVISED -T. RAMMACHER 11-04-97
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	PLOT DATE = 1/4/2008	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

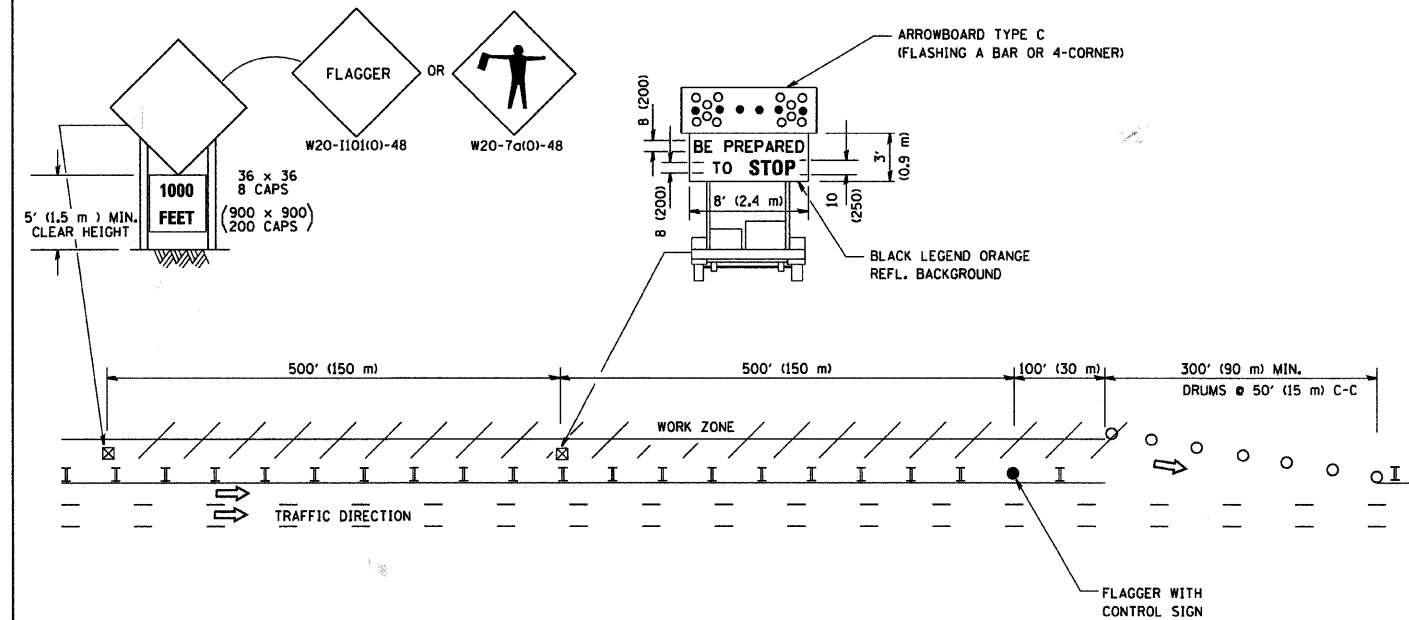
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

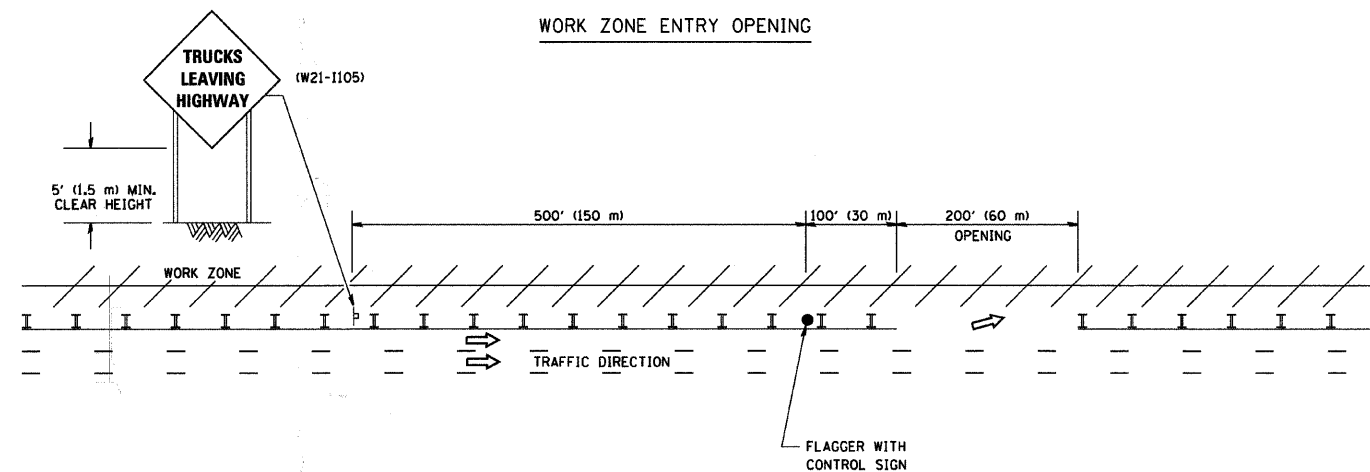
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0116	08-00055-00-RS	McHENR	18	16
TC-16			CONTRACT NO. 63115	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(169)				

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. The Arrowboard, the Flagger Ahead trailer mounted sign, and the Trucks Leaving Highway sign shall be removed or turned away from traffic and the exit and entry openings shall be closed when the flagging operation ceases.
2. Work Zone Exit Openings should be a minimum of one half mile apart.
3. Exiting the work zone at any place other than at a Work Zone Exit Opening will be prohibited.
4. All vehicles shall enter the work zone at entry openings, using their turn signals to warn motorists

JOB No. C-91-202-09
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN

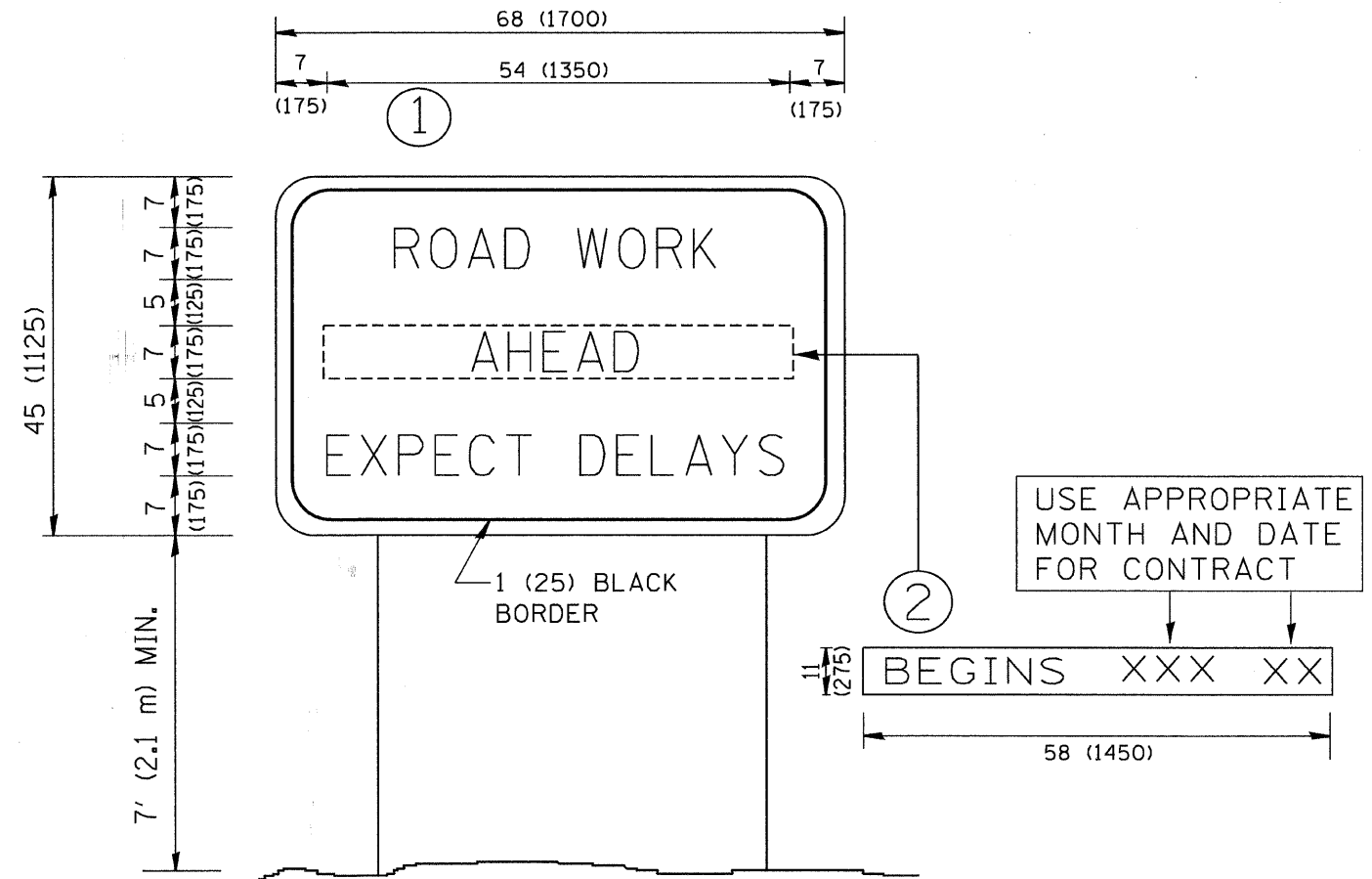
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		DRAWN -	REVISED - J.A.F. 04-03
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - J.A.F. 02-06
	PLOT DATE = 1/4/2008	DATE -	REVISED - S.P.B. 01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIGNING FOR FLAGGING OPERATIONS
AT WORK ZONE OPENINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.D. RTE. 0116	SECTION 08-00055-00-RS	COUNTY McHENRY	TOTAL SHEETS 18	SHEET NO. 17
TC-18		CONTRACT NO. 63115		
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-9003 (169)				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

JOB No. C-91-202-09
 ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
 UNLESS OTHERWISE SHOWN.

FILE NAME = M:\disto\22x34\to22.dgn	USER NAME = gaglienobt	DESIGNED - DRAWN -	REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A.D. RTE. 0116	SECTION 08-00055-00-RS	COUNTY McHENRY	TOTAL SHEETS 18	SHEET NO. 18
PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99	REVISED - C. JUCIUS 01-31-07	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TC-22	CONTRACT NO.	63115
PLOT DATE = 1/4/2008	DATE -							FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	M-9003 (169)	