

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

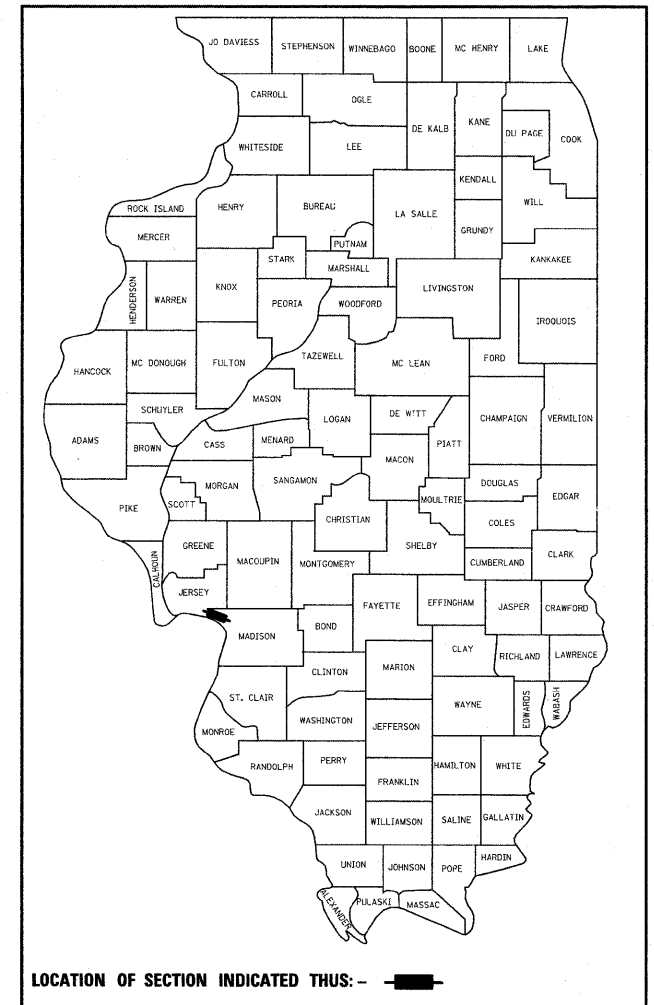
FAU ROUTE 8956 (IL ROUTE 3)
SECTION 59BR-1
PROJECT: *BRM-8956(003)*
STRUCTURE REPLACEMENT
MADISON COUNTY

C-98-033-09

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
8956	59BR-1	MADISON	75	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO. 76B18		

* 75 + 4 = 79

D-98-021-06



LOCATION OF SECTION INDICATED THIS: - [shaded area] -

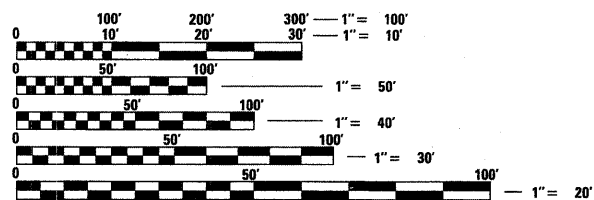
FOR INDEX OF SHEETS, SEE SHEET NO. 2

FAU 8956 (IL 3)

ADT: 5500 (2007)
ADT: 6700 (2027)
SU: 2.3%
MU: 3.2%

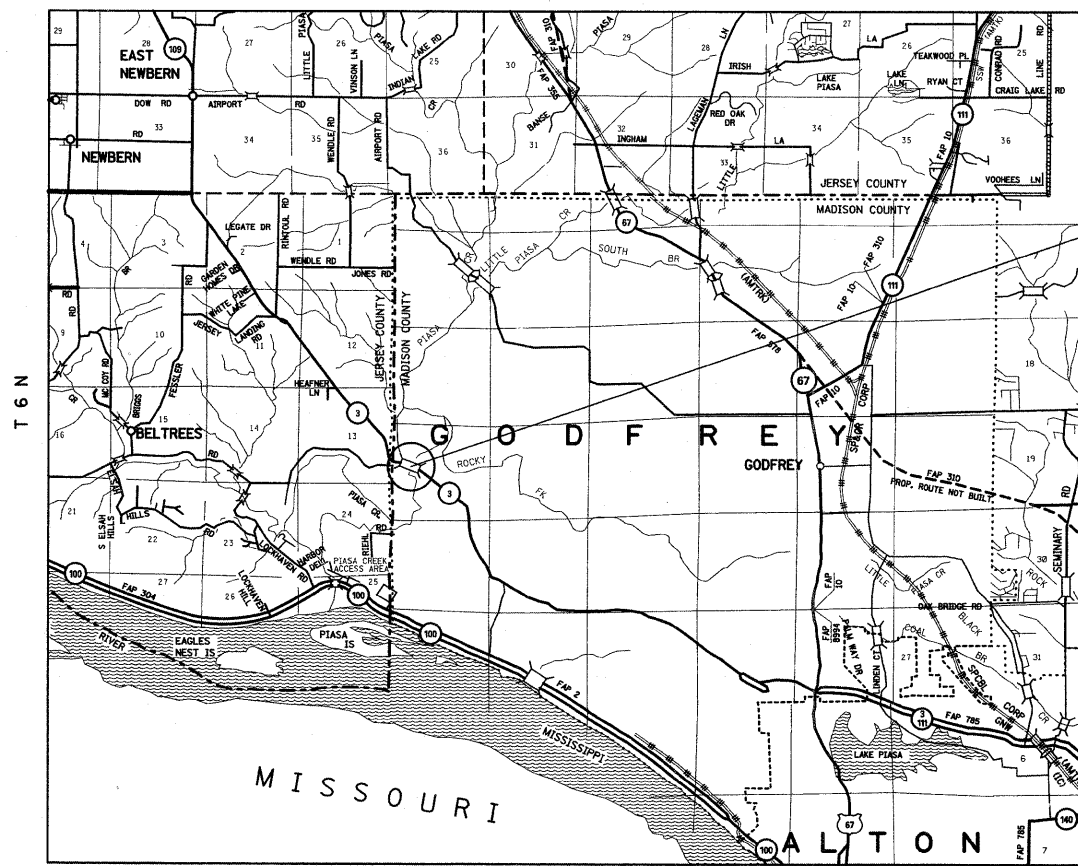
DESIGN DESIGNATION

NA



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

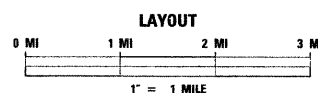
J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811



FIVE SPAN PPC DECK BEAM
TO BE REPLACED WITH THREE
SPAN PLATE GIRDER STRUCTURE
OVER PIASA CREEK
STA 636 + 82.5
SN 060-0153 (E)
SN 060-0343 (P)

PROJECT ENGINEER: PATTI LeBEAU (618) 346-3178
PROJECT MANAGER: ARTHUR MUEHLFELD (618) 346-3209

CONTRACT NO. 76B18



LATITUDE: 38.9739°
LONGITUDE: 90.2722°

NET /GROSS LENGTH = 225.5 FT. = 0.043 MILES

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED *January 30, 2009*

Mary C. Jamie
DEPUTY DIRECTOR OF HIGHWAYS, REGION 5 ENGINEER

March 27, 2009
Charles A. Ingersoll
ENGINEER OF DESIGN AND ENVIRONMENT

March 27, 2009
Christine M. Reed
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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OF THE STATE OF ILLINOIS**

INDEX OF SHEETS

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HIGHWAY STANDARDS

000001-05	630001-08	701301-03
001001-02	630301-05	701311-03
001006	631031-07	701321-10
202001-01	635006-03	701326-03
280001-04	635011-02	701901-01
420401-07	666001-01	704001-05
482001-02	667001-01	780001-02
515001-03	667101-01	781001-03
	<i>601101-01</i>	

GENERAL NOTES:

1. THE STANDARDS AND REVISION NUMBERS SHALL APPLY TO THIS PROJECT.
 2. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
 3. ILLINOIS STATE LAW REQUIRES A 48-HOUR NOTICE BE GIVEN TO ALL UTILITIES WITHIN THE PROJECT AREA BEFORE DIGGING. FIELD MARKING OF FACILITIES MAY BE OBTAINED BY CONTACTING J.U.L.I.E. OR FOR NON-MEMBERS, THE UTILITY COMPANY DIRECTLY. AGENCIES KNOWN TO HAVE FACILITIES WITHIN THE PROJECT AREA ARE AS FOLLOWS:
 - AMERENCIPS (ELECTRIC)
 - AMERENCIPS (GAS & ELECTRIC)
 - VILLAGE OF GODFREY (WATER)
 - ILLINOIS AMERICAN WATER COMPANY (WATER)
 - JERSEY COUNTY RURAL WATER COMPANY (WATER)
 - AT&T ILLINOIS (COMMUNICATIONS)
- MEMBERS OF J.U.L.I.E. (800) 892-0123 ARE INDICATED BY *.
NON-MEMBERS MUST BE NOTIFIED INDIVIDUALLY.
4. THE THICKNESS OF THE HOT-MIX ASPHALT MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE MINIMAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED.
 5. NO OPEN TRENCHES SHALL BE PERMITTED DURING WINTER SHUTDOWN OR AS DIRECTED BY THE ENGINEER.
 6. THE CONTRACTOR SHALL SAWCUT ALONG THE EDGE OF THE ROADWAY PRIOR TO PLACING THE WIDENING FOR STAGE CONSTRUCTION AS DETAILED IN THE PLANS. THE COST SHALL BE INCLUDED IN THE COST OF THE REMOVAL ITEM.
 7. RIGHT-OF-WAY MARKERS SHALL BE SET SO THE BACK OF THE POST IS TWELVE INCHES (12") INSIDE THE RIGHT OF WAY BOUNDARY. RIGHT OF WAY PROPERTY CORNERS ARE MARKED BY A 5/8" IRON ROD WITH IDOT ALUMINUM CAP AND SHALL NOT BE REMOVED OR DAMAGED WHEN SETTING THE RIGHT OF WAY MARKERS.
 8. BRIDGE APPROACH PAVEMENT IS INCLUDED IN PAVEMENT REMOVAL.

TRAFFIC CONTROL NOTES:

1. THE CONTRACTOR SHALL FURNISH AND INSTALL WOOD SIGN SUPPORTS IN ACCORDANCE WITH SECTION 730 OF THE STANDARD SPECIFICATIONS.
2. "ROAD CONSTRUCTION AHEAD" SIGNS SHALL BE PLACED AT EACH END OF THE PROJECT, AND WILL BE INCLUDED IN THE TRAFFIC CONTROL PAY ITEMS. ALL CONSTRUCTION SIGNS SHALL BE FLOURESCENT ORANGE (48").
3. ALL EXCAVATION ADJACENT TO THE EDGE OF PAVEMENT SHALL BE PROTECTED WITH EXTENDED LEG BARRICADES AND APPROPRIATE LIGHTS.
4. THE BOTTOM 6" OF TEMPORARY CONCRETE BARRIER SHALL BE PAINTED WHITE. A QUANTITY OF 562.5 FT SHALL BE PAID FOR AS "TEMPORARY PAVEMENT MARKING - LINE 6" ".

COMMITMENTS

WETLAND SHALL BE STAKED PRIOR TO CONSTRUCTION SO THAT NO IMPACT TO WETLAND IS ASSURED. ENVIRONMENTAL UNIT (JENNIFER HUNT: 618-346-3156) SHOULD BE INFORMED WHEN STAKING IS TAKING PLACE.

FILE NAME =	USER NAME = herbaughrd	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, HIGHWAY STANDARDS, GENERAL NOTES AND COMMITMENTS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 76B18				
	PLOT DATE = 3/16/2009	DATE -	REVISED -			SCALE: SHEET NO. OF SHEETS STA. TO STA.		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		

Rev.

SUMMARY OF QUANTITIES

SUMMARY OF QUANTITIES			URBAN 80% FED. 20% STATE TOTAL	CONSTRUCTION TYPE CODE			SUMMARY OF QUANTITIES			URBAN 80% FED. 20% STATE TOTAL	CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT		QUANTITIES	X071-2A		CODE NO	ITEM	UNIT		QUANTITIES	X028-2A	
20100500	TREE REMOVAL, ACRES	ACRE	0.75	0.75		50500505	STUD SHEAR CONNECTORS	EACH	4011	4011			
20200100	EARTH EXCAVATION	CU YD	390	390		50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	102230	102230			
20400800	FURNISHED EXCAVATION	CU YD	4385	4385		50800515	BAR SPLICERS	EACH	971	971			
20700400	POROUS GRANULAR EMBANKMENT, SPECIAL	CU YD	175	175		51201600	FURNISHING STEEL PILES HP12X53	FOOT	912	912			
25000200	SEEDING, CLASS 2	ACRE	0.75	0.75		51201700	FURNISHING STEEL PILES HP12X74	FOOT	1246	1246			
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	76.5	76.5		51202305	DRIVING PILES	FOOT	2158	2158			
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	76.5	76.5		51203600	TEST PILE STEEL HP12X53	EACH	2	2			
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	76.5	76.5		51203700	TEST PILE STEEL HP12X74	EACH	1	1			
25100115	MULCH, METHOD 2	ACRE	0.25	0.25		51204650	PILE SHOES	EACH	30	30			
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	150	150		51500100	NAME PLATES	EACH	1	1			
25100630	EROSION CONTROL BLANKET	SQ YD	2901	2901		52100520	ANCHOR BOLTS, 1"	EACH	56	56			
28000300	TEMPORARY DITCH CHECKS	EACH	11	11		54200220	PIPE CULVERTS, CLASS D, TYPE 1 15"	FOOT	30	30			
28000400	PERIMETER EROSION BARRIER	FOOT	752	752		59100100	GEOCOMPOSITE WALL DRAIN	SQ YD	90	90			
28100109	STONE RIPRAP, CLASS A5	SQ YD	2550	2550		60109580	PIPE UNDERDRAINS FOR STRUCTURES 4"	FOOT	119	119			
28200200	FILTER FABRIC	SQ YD	2550	2550		*63000000	STEEL PLATE BEAM GUARD RAIL, TYPE A	FOOT	337.5	337.5			
30200650	PROCESSING MODIFIED SOIL 12"	SQ YD	436	436		*63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	4	4			
30201500	LIME	TON	8.4	8.4		*63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	2	2			
40200500	AGGREGATE SURFACE COURSE, TYPE A 6"	SQ YD	325	325		*63100169	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) FLARED	EACH	2	2			
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	0.1	0.1		63200310	GUARDRAIL REMOVAL	FOOT	707	707			
40600300	AGGREGATE (PRIME COAT)	TON	0.5	0.5		66600105	FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS	EACH	7	7			
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	231	231		66700205	PERMANENT SURVEY MARKERS, TYPE I	EACH	3	3			
40600990	TEMPORARY RAMP	SQ YD	33	33		67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	13	13			
40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	298	298		67100100	MOBILIZATION	L SUM	1	1			
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	54	54		70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1			
42001165	BRIDGE APPROACH PAVEMENT	SQ YD	223	223		70100500	TRAFFIC CONTROL AND PROTECTION, STANDARD 701326	L SUM	1	1			
42001430	BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE)	SQ YD	45	45		70101205	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321 (SPECIAL)	EACH	1	1			
44004250	PAVED SHOULDER REMOVAL	SQ YD	150	150		70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1	1			
48100500	AGGREGATE SHOULDERS, TYPE A 6"	SQ YD	211	211		70106700	TEMPORARY RUMBLE STRIP	EACH	6	6			
50100100	REMOVAL OF EXISTING STRUCTURES	EACH	1	1		70300100	SHORT-TERM PAVEMENT MARKING	FOOT	158	158			
50105220	PIPE CULVERT REMOVAL	FOOT	64	64		70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	2075	2075			
50200100	STRUCTURE EXCAVATION	CU YD	298	298		70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	562.5	562.5			
50300100	FLOOR DRAINS	EACH	28	28		70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	744	744			
50300225	CONCRETE STRUCTURES	CU YD	200	200		70400100	TEMPORARY CONCRETE BARRIER	FOOT	562.5	562.5			
50300255	CONCRETE SUPERSTRUCTURE	CU YD	351.8	351.8		70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	487.5	487.5			
50300260	BRIDGE DECK GROOVING	SQ YD	1022	1022		*78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	2298	2298			
50300280	CONCRETE ENCASEMENT	CU YD	10.6	10.6									
50300300	PROTECTIVE COAT	SQ YD	1279	1279									
50500105	FURNISHING AND ERECTING STRUCTURAL STEEL	L SUM	1	1									

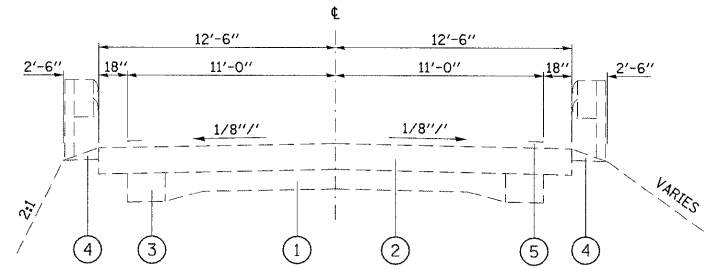
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#FILE#		DRAWN -	REVISED -			8956	59BR-1	MADISON	75	3	
		CHECKED -	REVISED -			CONTRACT NO. 76B18					
		DATE -	REVISED -			FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT					

SUMMARY OF QUANTITIES

SUMMARY OF QUANTITIES			URBAN 80% FED. 20% STATE TOTAL	CONSTRUCTION TYPE CODE			SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE			
CODE NO	ITEM	UNIT		QUANTITIES	X028-2A			CODE NO	ITEM		UNIT	QUANTITIES	X028-2A	
*78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	1208	1208										
*78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	2	2										
*78100105	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	EACH	4	4										
*78100300	REPLACEMENT REFLECTOR	EACH	6	6										
*78200410	GUARDRAIL MARKERS, TYPE A	EACH	4	4										
*78200520	BARRIER WALL MARKERS, TYPE B	EACH	6	6										
*78200530	BARRIER WALL MARKERS, TYPE C	EACH	6	6										
*78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	4	4										
78300100	PAVEMENT MARKING REMOVAL	SQ FT	750	750										
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	8	8										
X0323988	TEMPORARY SOIL RETENTION SYSTEM	SQ FT	172	172										
X7200200	WIDE LOAD SIGNING	L SUM	1	1										
X5020501	UNDERWATER STRUCTURE EXCAVATION PROTECTION - LOCATION 1	EACH	1	1										
X5020502	UNDERWATER STRUCTURE EXCAVATION PROTECTION - LOCATION 2	EACH	1	1										
Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2	2										
Z0030350	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2	2										
Z0001900	ASBESTOS BEARING PAD REMOVAL	EACH	66	66										

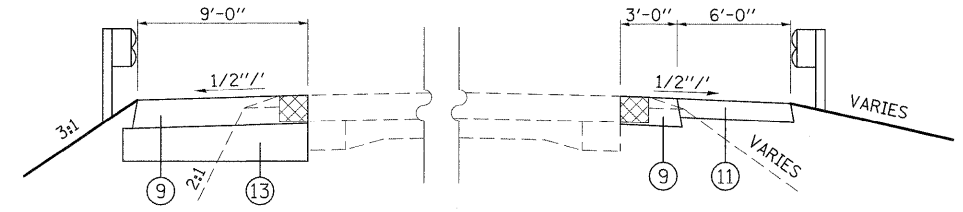
*SPECIALTY ITEMS

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		DRAWN -	REVISED -							
		CHECKED -	REVISED -							
		DATE -	REVISED -							
				SCALE: SHEET NO. 2 OF 2 SHEETS STA. TO STA.		FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT CONTRACT NO. 76B18		



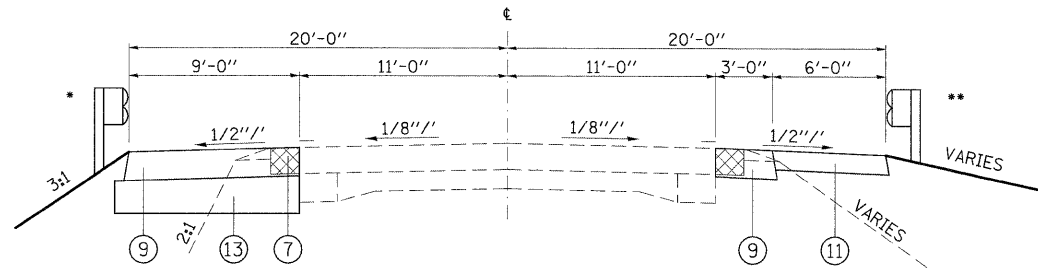
EXISTING TYPICAL SECTION

STA. 633+00 TO STA. 636+57.5
 STA. 639+42.5 TO STA. 642+00



PROPOSED WIDENING FOR STAGE CONSTRUCTION

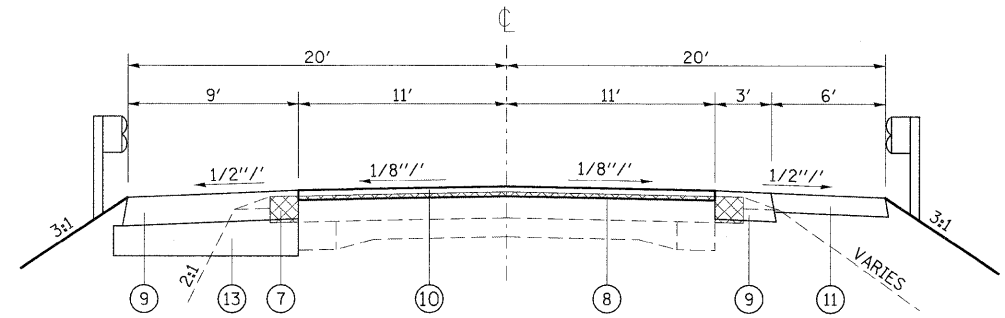
STA. 634+50 TO STA. 636+57.5
 STA. 639+60.5 TO STA. 642+00



PROPOSED TYPICAL SECTION

STA. 634+50 TO STA. 635+90

- STA. 635+14.5 TO STA. 635+90
- STA. 634+89.5 TO STA. 635+90



PROPOSED TYPICAL SECTION

STA. 635+90 TO STA. 636+46.5

LEGEND

- ① EXISTING PCC PAVEMENT - 9'-6"-9"
- ② EXISTING BITUMINOUS SURFACE - ±8"
- ③ EXISTING WIDENING - 9"
- ④ EXISTING AGGREGATE SHOULDER
- ⑤ EDGE LINE STRIPE
- ⑥ EXISTING AGGREGATE BASE COURSE
- ⑦ PROPOSED PAVED SHOULDER REMOVAL
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- ⑨ PROPOSED HOT-MIX ASPHALT BINDER COURSE - 9"
- ⑩ PROPOSED HOT-MIX ASPHALT SURFACE COURSE (VARIES)
- ⑪ PROPOSED AGGREGATE SHOULDER - 6"
- ⑫ PROPOSED AGGREGATE SURFACE COURSE - 6"
- ⑬ PROPOSED PROCESSING MODIFIED SOIL 12"

MIXTURE USE	SURFACE	BINDER / WIDENING	SHOULDERS	TOP LIFT SHOULDERS
AC/PG	PG 64-22	PG 64-22	PG 64-22	PG 64-22
RAP % (MAX)	10%	15%	30%	30%
DESIGN AIR VOIDS	4.0% @ Ndes=70	4.0% @ Ndes=70	2.0% @ Ndes=30	••2.0% @ Ndes=30
MIX COMPOSITION (GRADATION MIXTURE)		IL 19.0		
FRICITION AGG	MIXTURE "D"	MIXTURE "B"	BAM	BAM

•• TOP LIFT SHOULDERS - DESIGN THIS MIX AT 2.0% VOIDS AND ADD ASPHALT TO REDUCE VOIDS TO 1.5%.

PLAN QUANTITIES FOR BITUMINOUS CONCRETE SURFACE COURSE ITEMS ARE CALCULATED USING A UNIT WEIGHT OF 112 LB/SQ YD/IN (59.8 KG/SQ M/25 MM THICKNESS).

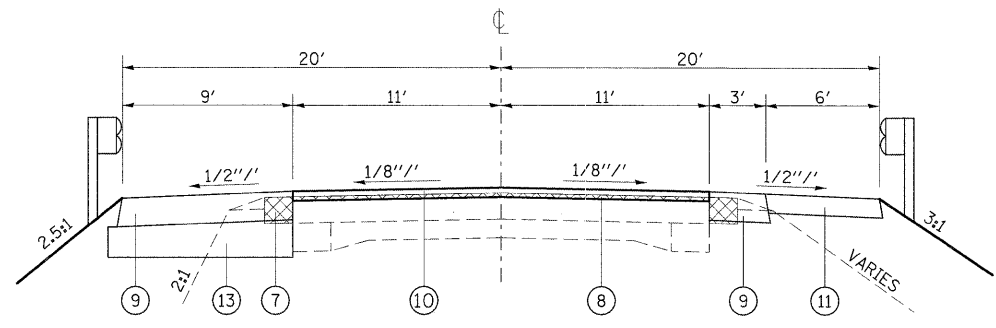
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	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 3/16/2009	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

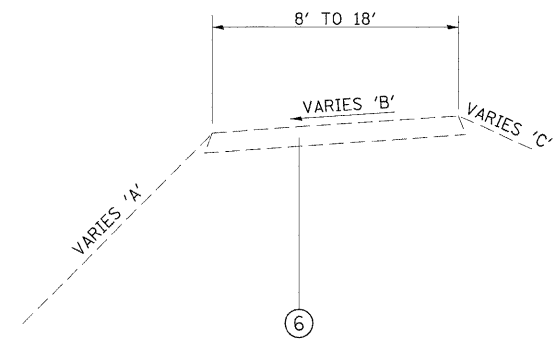
MAINLINE TYPICAL SECTIONS

SCALE: SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
8956	59BR-1	MADISON	75	5
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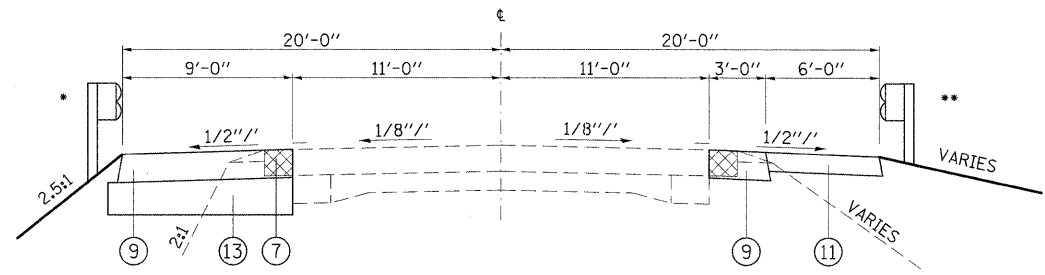


PROPOSED TYPICAL SECTION
STA. 639+60.5 TO STA. 640+50

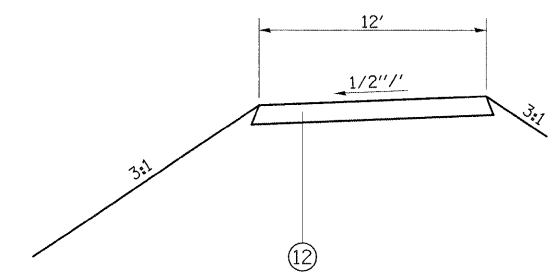


EXISTING TYPICAL SECTION
ENTRANCE @ STA. 633+90, LT.

VARIES 'A': 1:1 TO 3:1
VARIES 'B': 3/8''/1' TO 3/4''/1'
VARIES 'C': 3:1 TO 6:1



PROPOSED TYPICAL SECTION
STA. 640+50 TO STA. 642+00
• STA. 640+50 TO STA. 641+42.5
•• STA. 640+50 TO STA. 641+55



PROPOSED TYPICAL SECTION
ENTRANCE @ STA. 633+90, LT.

LEGEND

- ① EXISTING PCC PAVEMENT - 9'-6"-9"
- ② EXISTING BITUMINOUS SURFACE - ±8"
- ③ EXISTING WIDENING - 9"
- ④ EXISTING AGGREGATE SHOULDER
- ⑤ EDGE LINE STRIPE
- ⑥ EXISTING AGGREGATE BASE COURSE
- ⑦ PROPOSED PAVED SHOULDER REMOVAL
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- ⑨ PROPOSED HOT-MIX ASPHALT BINDER COURSE - 9"
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- ⑬ PROPOSED PROCESSING MODIFIED SOIL 12"

FILE NAME =	USER NAME = harbaughrd	DESIGNED -	REVISED -
ct:\pwork\pedit\harbaughrd\dms52187\p	482186a.dgn	DRAWN -	REVISED -
	PLOT SCALE = 50.0000' / 1"	CHECKED -	REVISED -
	PLOT DATE = 3/16/2009	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

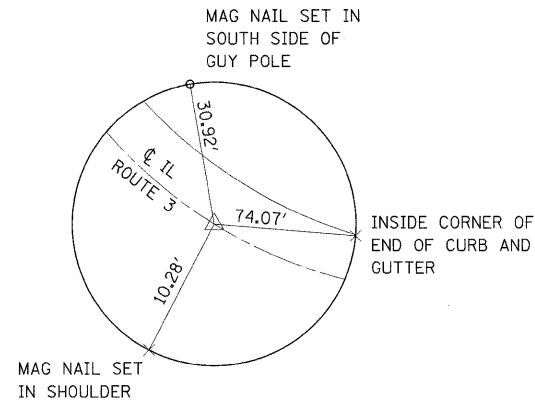
TYPICAL SECTIONS

SCALE: SHEET NO. 2 OF 2 SHEETS STA. TO STA.

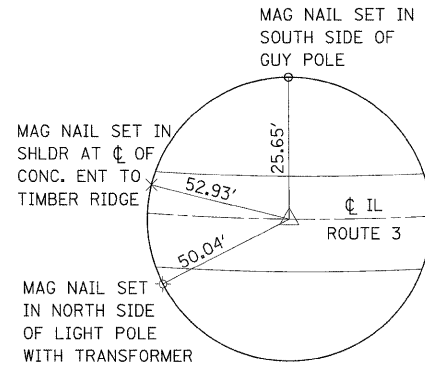
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
8956	59BR-1	MADISON	75	6
CONTRACT NO. 76B18				
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

BENCH MARK 203: RR SPIKE DRIVEN IN NORTH SIDE OF POWER POLE ON SOUTH SIDE RT. 3, NEAR NORTHWEST CORNER OF GRAVEL TURN-A-ROUND, 600' WEST OF CENTER OF BRIDGE, SN 060-0153 AT PIASA CREEK
APPROX. STA.: ±632+00, RT, ELEV.= 449.141

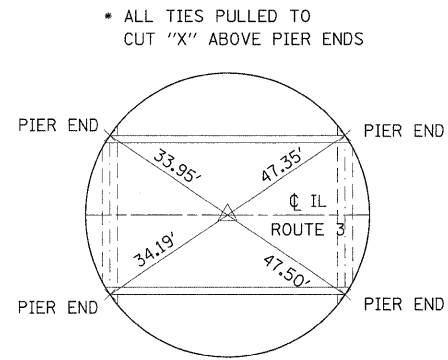
BENCH MARK 204: RR SPIKE DRIVEN IN NORTH SIDE OF POWER POLE ON SOUTH SIDE RT. 3, ON WEST SIDE OF GRAVEL SERVICE ROAD, 232' EAST OF CENTER OF BRIDGE, SN 060-0153 AT PIASA CREEK
APPROX. STA.: ±640+32, RT, ELEV.= 443.672



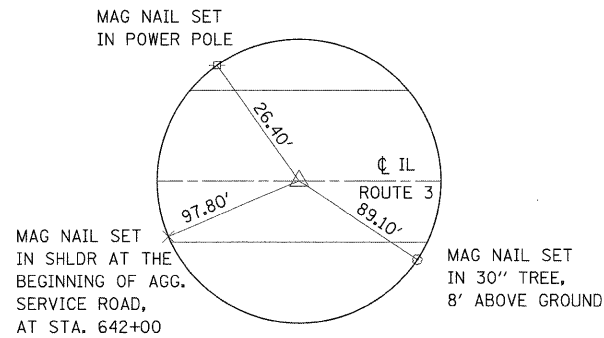
TIE POINT
MAG NAIL
P.C. STA. 629+29.69



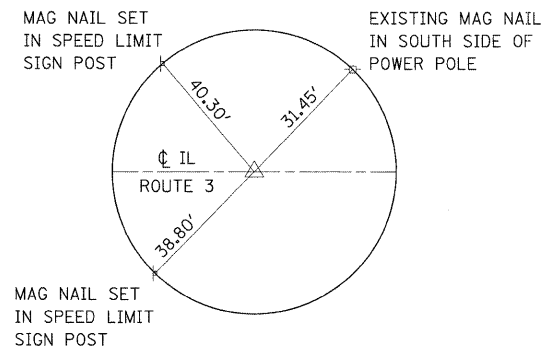
TIE POINT
MAG NAIL
P.T. STA. 631+77.44



TIE POINT
MAG NAIL
BRIDGE CL STA 638+00



TIE POINT
MAG NAIL
P.C. STA. 642+97.06
±175' WEST OF CL HIGH MEADOW LANE



TIE POINT
MAG NAIL
P.O.C. STA. 646+22.35
±162' EAST OF CL OF HIGH MEADOW LANE

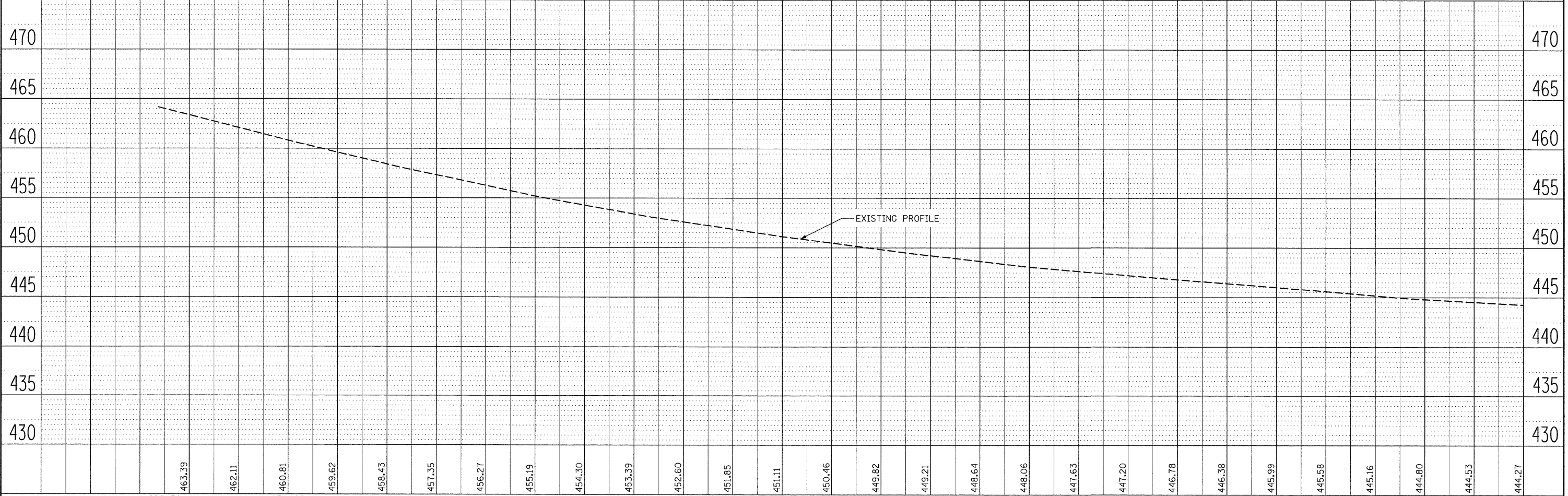
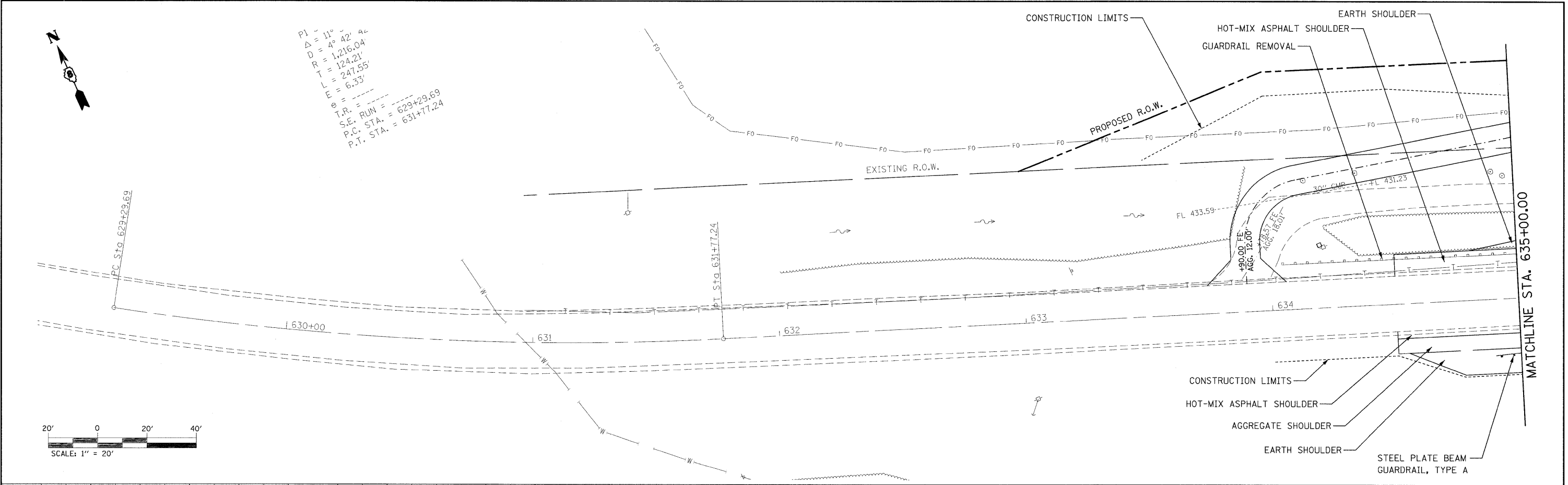
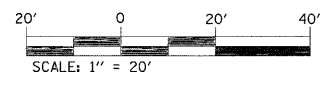
NOTES: ALL TIES PULLED DIRECT
NOT TO SCALE

FILE NAME =	USER NAME = herbaughrd	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TIE POINTS & BENCHMARKS			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw_work\pwidot\harbaughrd\dms52107\p	02106a.dgn	DRAWN -	REVISED -		8956	59BR-1	MADISON	75	8			
PLOT SCALE = 50,0000' / IN.	CHECKED -	REVISED -	REVISED -		CONTRACT NO. 76B18							
PLOT DATE = 3/16/2009	DATE -	REVISED -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	ALIGNED		
	CHECKED		
	NO.		
	FILE NAME		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	STRUCTURE		
	NOTATIONS CHECKED		
	NO.		

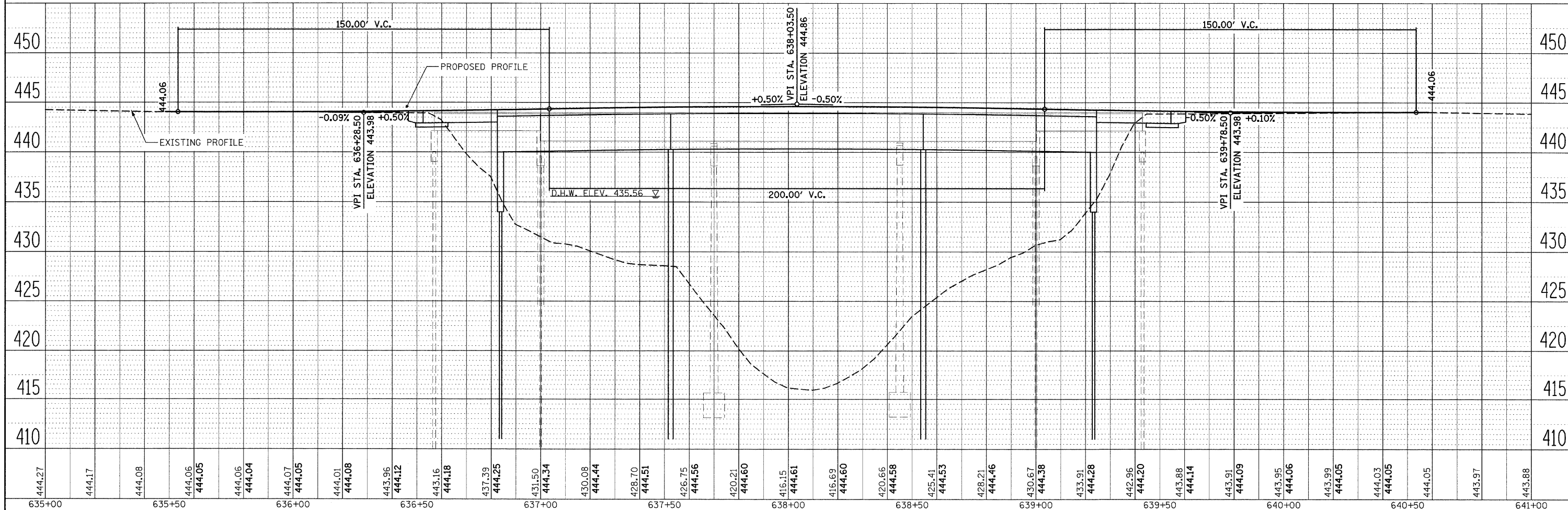
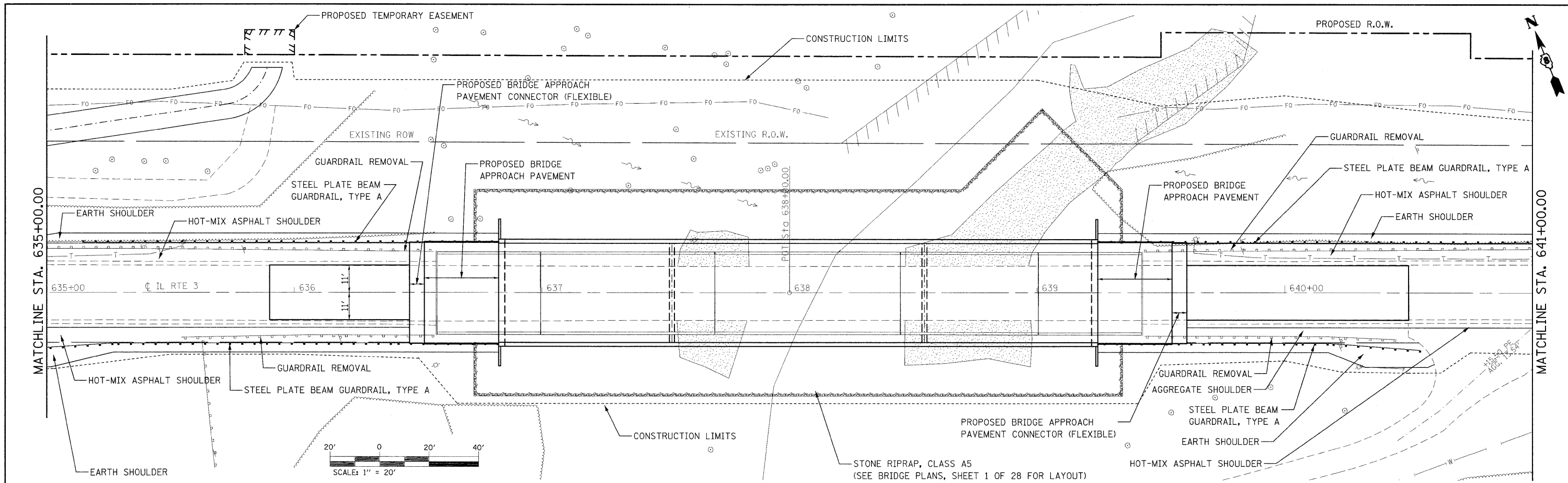
$P1 = 11^\circ$
 $\Delta = 4^\circ 42' 42''$
 $D = 1,216.04'$
 $R = 124.21'$
 $T = 247.55'$
 $L = 6.33'$
 $e =$
 $T.R. =$
 $S.E. STA. = 629+29.69$
 $P.C. STA. = 631+77.24$
 $P.T. STA. = 631+77.24$



FILE NAME =	USER NAME = harbaughrd	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PIASA CREEK PLAN AND PROFILE	SCALE: 20 SHEET NO. 1 OF 4 SHEETS STA. 629+29.69 TO STA. 635+00	CONTRACT NO. 76B18
DESIGNED BY	DRAWN -	REVISED -					
CHECKED -	CHECKED -	REVISED -					
DATE -	DATE -	REVISED -					
F.A.I.L. RTE. 8956 SECTION 59BR-1 COUNTY MADISON TOTAL SHEETS 75 SHEET NO. 9 ILLINOIS FED. AID PROJECT							

PLAN SURVEYED BY DATE
 PLOTTED BY
 NOTE BOOK NO.
 RT. OF WAY CHECKED BY
 CAD FILE NAME

PROFILE SURVEYED BY DATE
 PLOTTED BY
 NOTE BOOK NO.
 BR. NOTED BY
 STRUCTURE NOTATIONS CHKD



444.27	444.17	444.08	444.06	444.05	444.06	444.04	444.07	444.05	444.01	444.08	443.96	444.12	443.16	444.18	443.39	444.25	443.50	444.34	443.08	444.44	442.70	444.51	442.75	444.56	442.21	444.60	441.15	444.61	441.69	444.60	442.66	444.58	442.41	444.53	442.21	444.46	443.67	444.38	443.91	444.28	442.96	444.20	443.88	444.14	443.91	444.09	443.95	444.06	443.99	444.05	444.03	444.05	444.05	443.97	443.88
635+00		635+50			636+00			636+50			637+00			637+50			638+00			638+50			639+00			639+50			640+00			640+50			641+00																				

FILE NAME = #FILE#

USER NAME = #USER#
 PLOT SCALE = #SCALE#
 PLOT DATE = #DATE#

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SCALE: 20 SHEET NO. 2 OF 4 SHEETS STA. 635+00 TO STA. 641+00

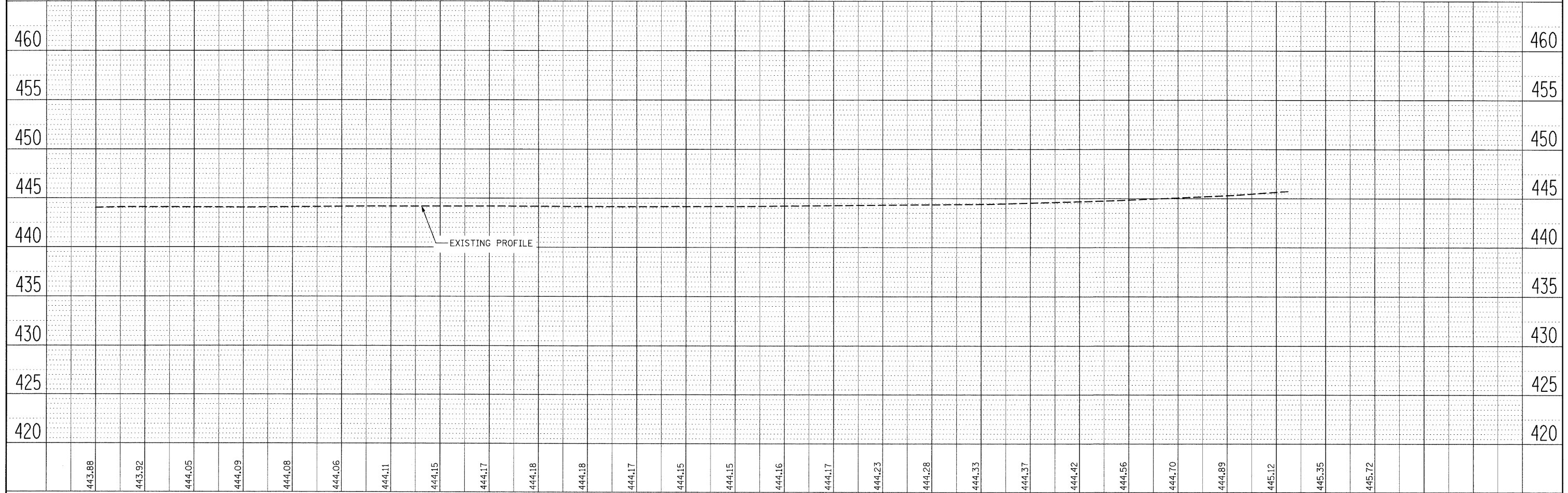
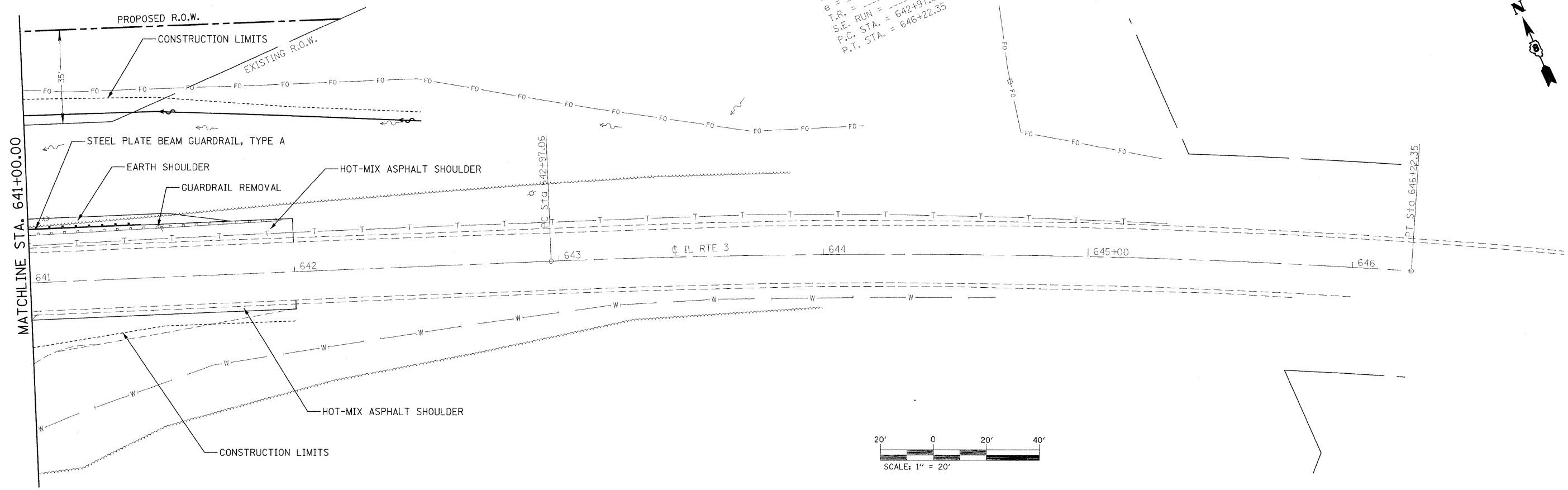
PIASA CREEK PLAN AND PROFILE

F.A.I.L. RTE. 8956	SECTION 59BR-1	COUNTY MADISON	TOTAL SHEETS 75	SHEET NO. 10
CONTRACT NO. 76B18				
ILLINOIS FED. AID PROJECT				

PLAN SURVEYED BY DATE
 PLOTTED BY CHECKED BY DATE
 NOTE BOOK NO. FILE NAME

PROFILE SURVEYED BY DATE
 PLOTTED BY CHECKED BY DATE
 NOTE BOOK NO. STRUCTURE NOTATIONS CHKD

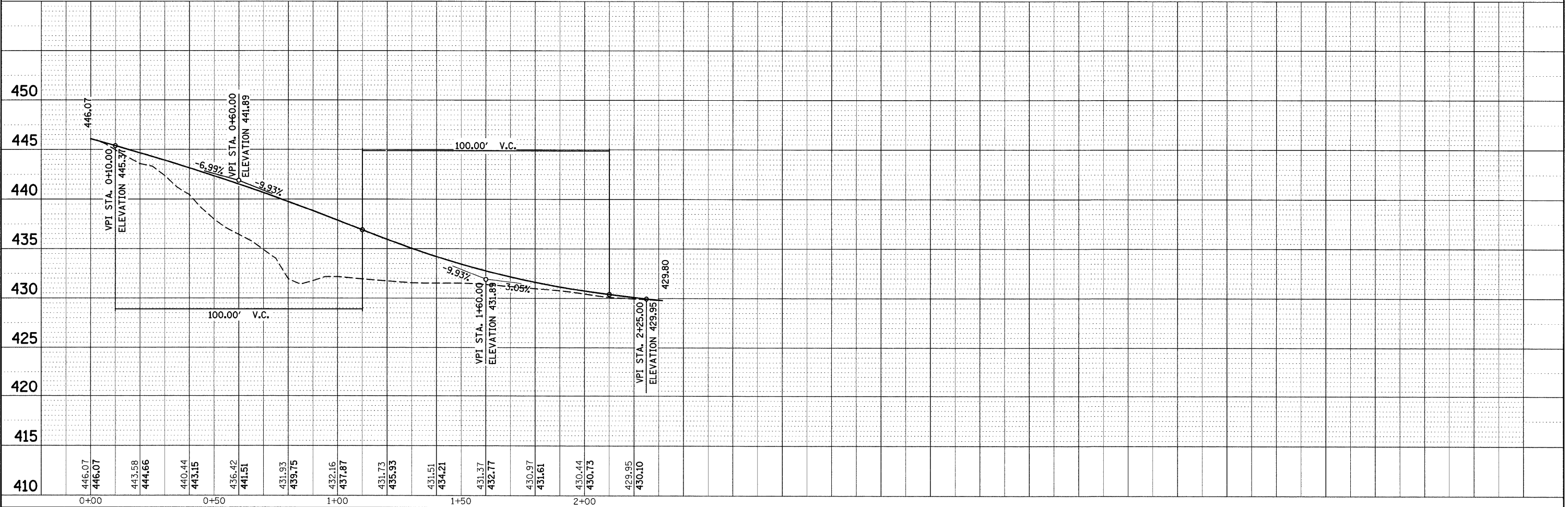
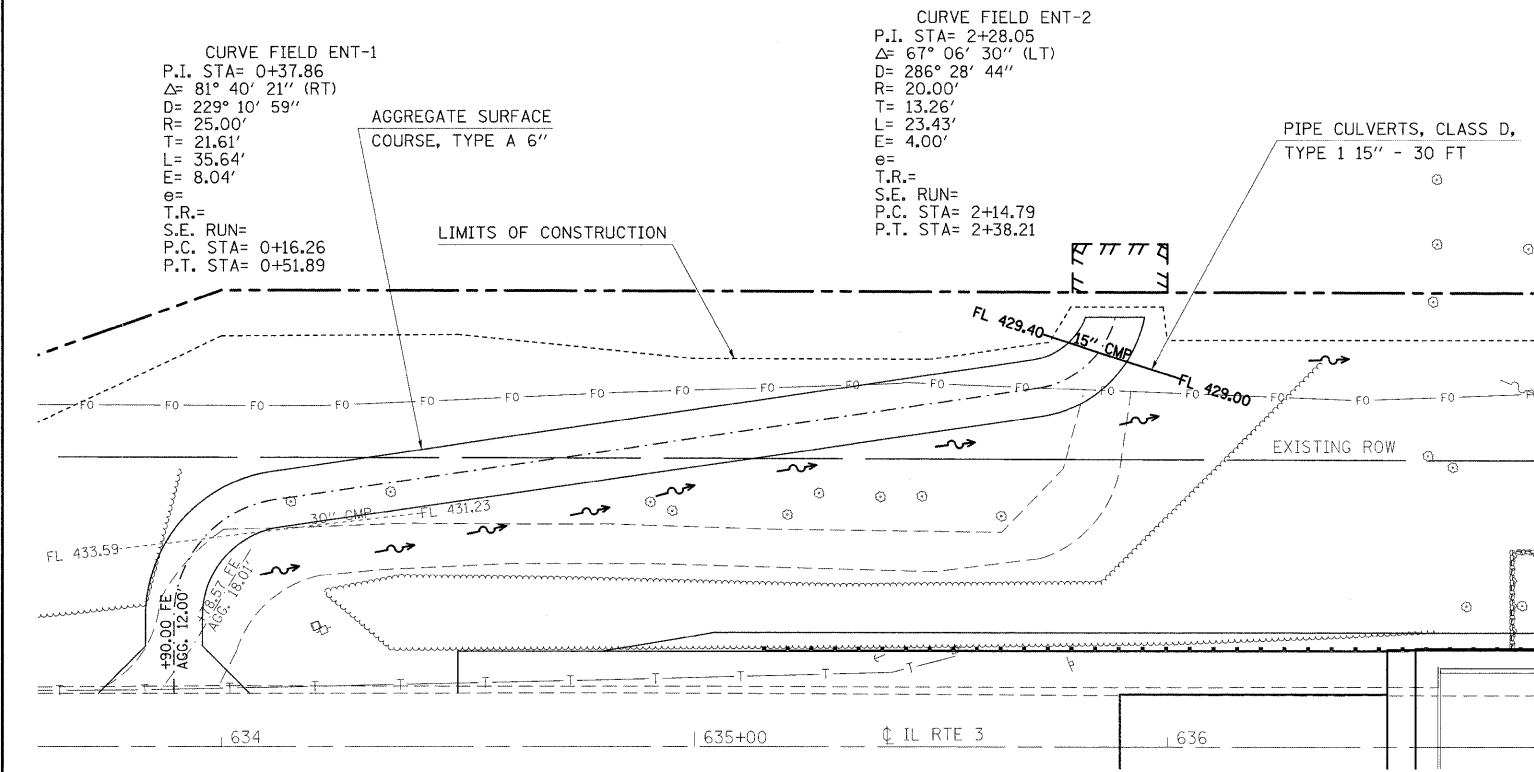
L =
 T.R. =
 S.E. RUN = 642+97.00
 P.C. STA. = 646+22.35
 P.T. STA. = 646+22.35



FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PIASA CREEK PLAN AND PROFILE	SCALE: 20 SHEET NO. 3 OF 4 SHEETS STA. 641+00 TO STA. 646+22.35	ILL. RTE. 8956 SECTION 598R-1 COUNTY MADISON TOTAL SHEETS 75 SHEET NO. 11 CONTRACT NO. 76B18 ILLINOIS FED. AID PROJECT
#FILEL#		DRAWN -	REVISED -				
		CHECKED -	REVISED -				
		DATE -	REVISED -				

PLAN	SUBMITTED	BY	DATE
	PLOTTED		
	ALIGNED		
	CHECKED		
	FILED		
	FILE NAME		
	NO.		

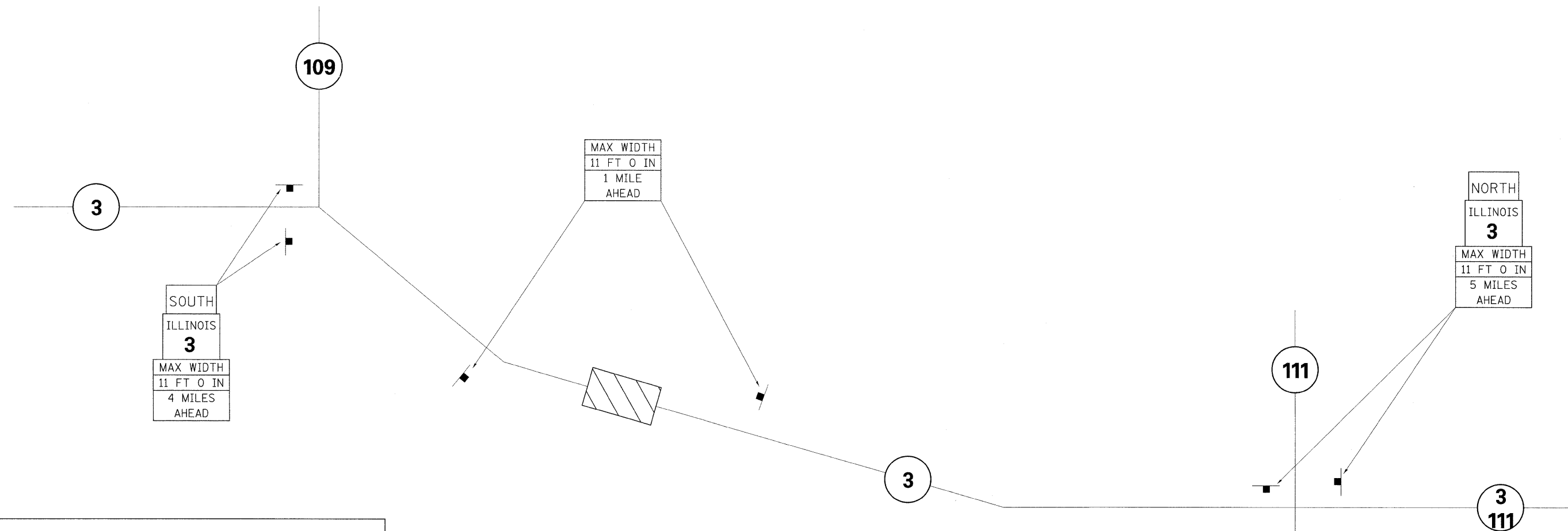
PROFILE	SUBMITTED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS CHKD		
	NO.		



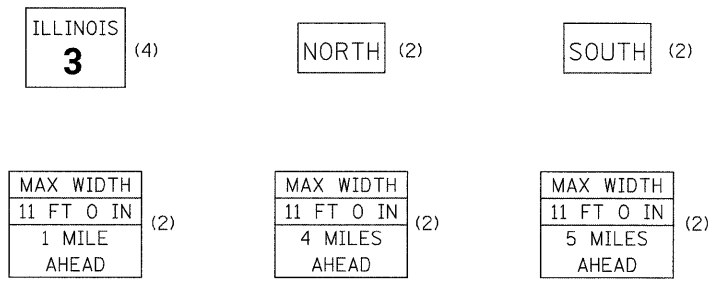
FILE NAME =	USER NAME = harbaughd	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCALE: SHEET NO. 4 OF 4 SHEETS STA. TO STA.	F.A. RTE. 8956	SECTION 59BR-1	COUNTY MADISON	TOTAL SHEETS	SHEET NO.
DESIGNED BY	DRAWN -	REVISED -	75						12	
CHECKED BY	CHECKED -	REVISED -	CONTRACT NO. 76B18							
DATE	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							

NOTES:

1. ALL SIGNS REQUIRED WILL BE SUPPLIED TO THE CONTRACTOR BY I.D.O.T.
2. THE CONTRACTOR SHALL FURNISH THE POSTS AND ERECT SIGNS AT THE LOCATIONS SHOWN ON THIS SHEET, AS DIRECTED BY THE R.E./R.T.
3. THE CONTRACTOR SHALL GIVE ILLINOIS DEPARTMENT OF TRANSPORTATION, BUREAU OF OPERATIONS TWO WEEKS NOTICE FOR SIGNS. THE CONTRACTOR SHALL PICK UP THE SIGNS AT THE T.M. BUILDING IN FAIRVIEW HEIGHTS, AND RETURN THEM UPON COMPLETION OF THE CONTRACT. CONTACT JEAN SLAPE AT 346-3289.
4. THE ABOVE NOTED WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE, LUMP SUM, FOR WIDE LOAD SIGNING AND NO OTHER COMPENSATION WILL BE ALLOWED.
5. SIGN SPACING WILL BE 400' OR TO FIT FIELD CONDITIONS.
6. THE HEIGHT TO THE BOTTOM OF THE LOWEST SIGN SHALL NOT BE LESS THAN 6'.

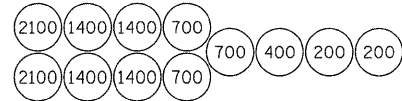


SIGNS REQUIRED

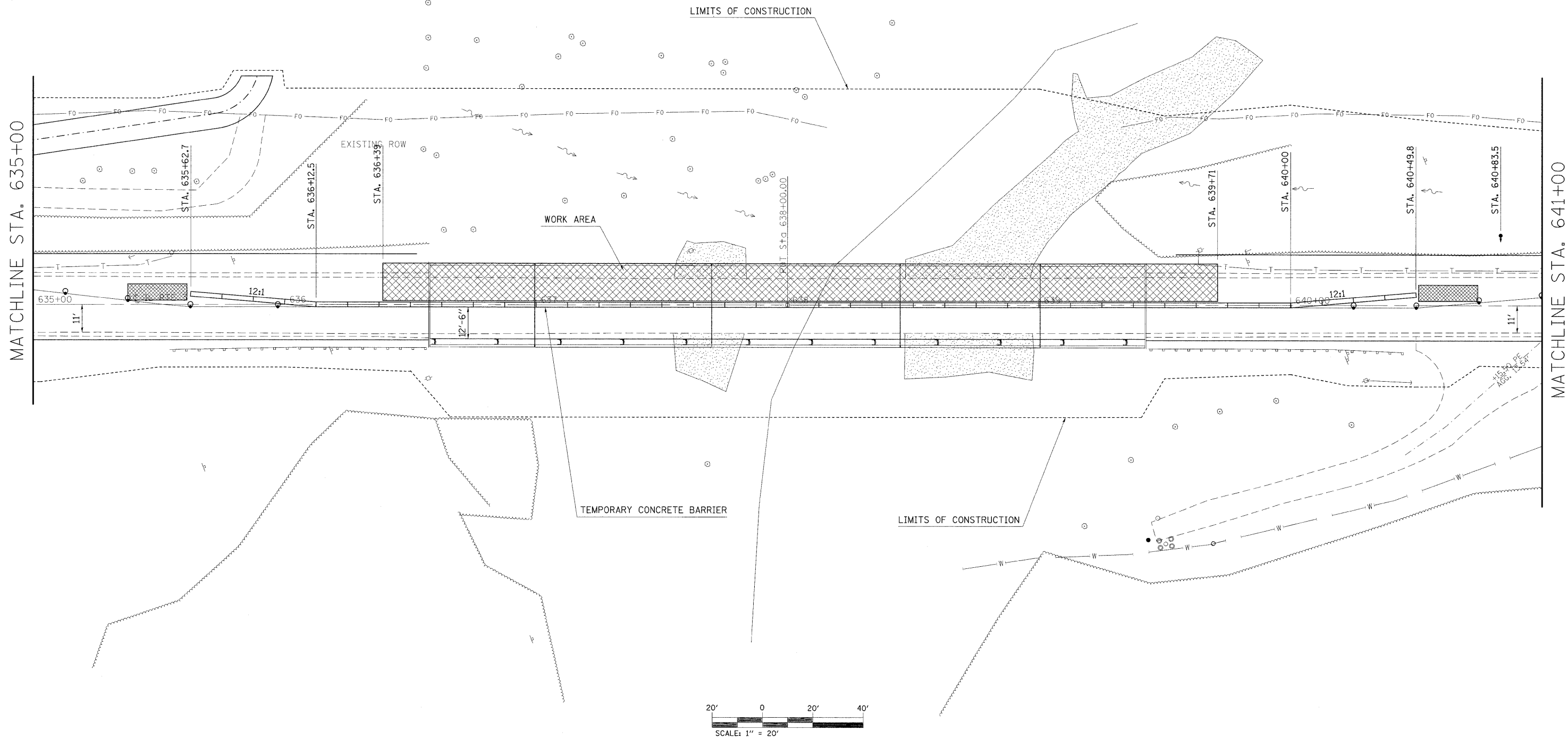


ALTON / GODFREY

FILE NAME =	USER NAME = harbaugh-rd	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	WIDE LOAD SIGNING STAGE CONSTRUCTION		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ci:\pwwork\pwwork\harbaugh-rd\dms52107\p	02106a.dgn	DRAWN -	REVISED -		8956	59BR-1	MADISON	75	13		
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -		CONTRACT NO. 76B18						
PLOT DATE = 3/16/2009		DATE -	REVISED -		SCALE:	SHEET NO. 1 OF 7 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	



SAND MODULE IMPACT ATTENUATOR LAYOUT
(IF OPTION USED)



MATCHLINE STA. 635+00

MATCHLINE STA. 641+00

FILE NAME =
#FILE#

USER NAME = #USER#
PLOT SCALE = #SCALE#
PLOT DATE = #DATE#

DESIGNED -
DRAWN -
CHECKED -
DATE -

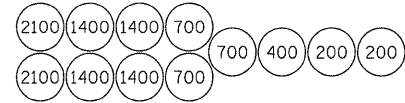
REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGE I CONSTRUCTION

SCALE: 20 SHEET NO. 2 OF 6 SHEETS STA. 635+00 TO STA. 641+00

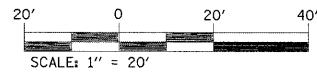
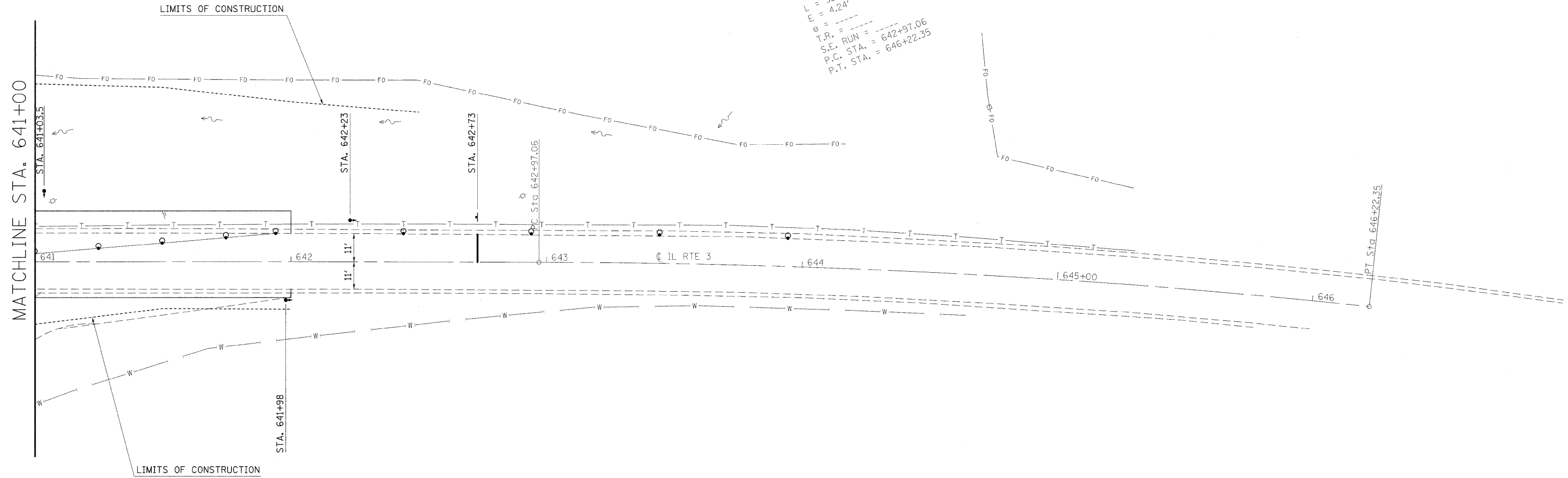
F.A.I.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
8956	59BR-1	MADISON	75	15
CONTRACT NO. 76B18				
ILLINOIS FED. AID PROJECT				



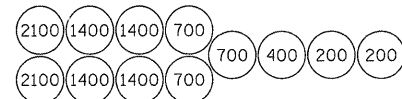
SAND MODULE IMPACT ATTENUATOR LAYOUT
(IF OPTION USED)



EXIST. CURVE C2
 PI STA. = 644+59.85
 $\Delta = 5^\circ 58' 26''$ (RT)
 $D = 1^\circ 50' 11''$
 $R = 3,120.01'$
 $T = 162.80'$
 $L = 325.30'$
 $E = 4.24'$
 $e = \dots$
 T.R. = \dots
 S.E. RUN = \dots
 P.C. STA. = 642+97.06
 P.T. STA. = 646+22.35

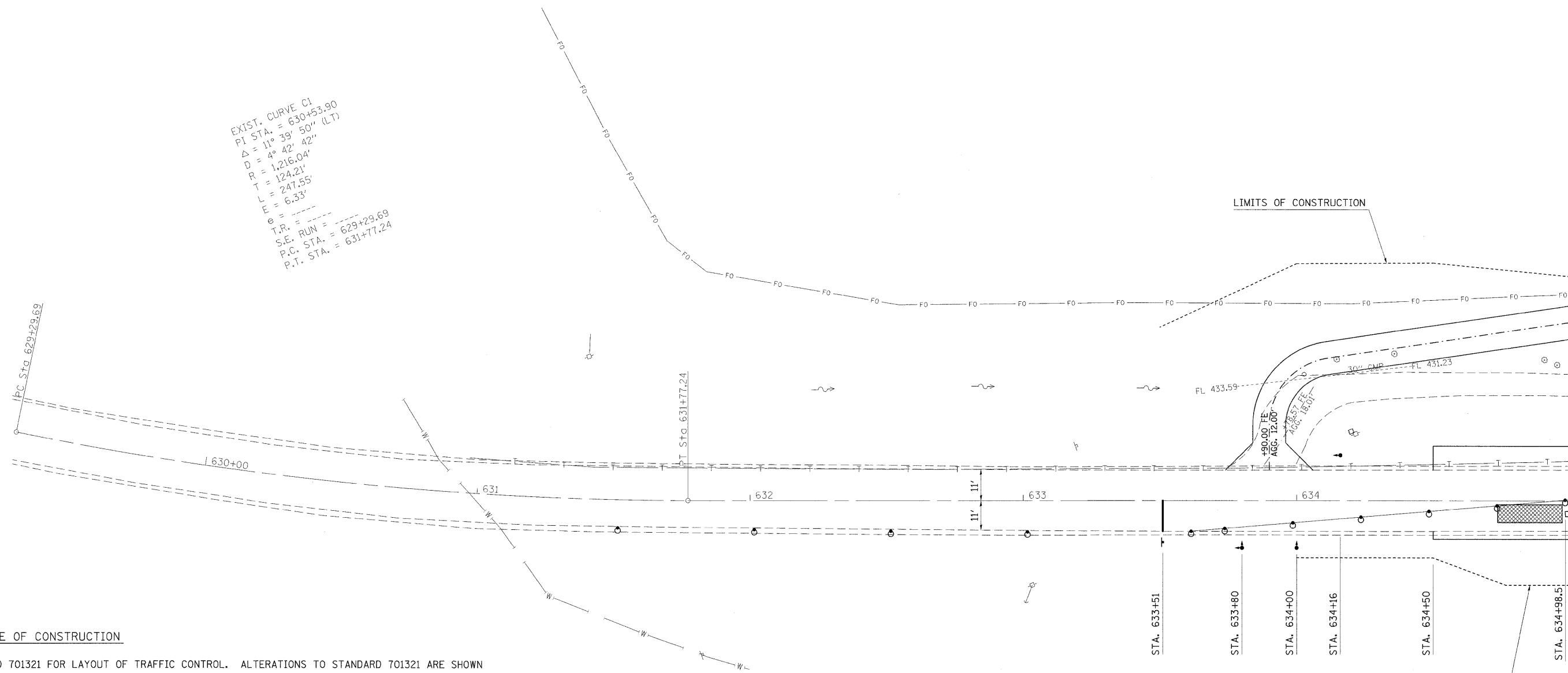


FILE NAME = #FILE#	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGE I CONSTRUCTION			F.A.I.L. RTE. 8956	SECTION 59BR-1	COUNTY MADISON	TOTAL SHEETS 75	SHEET NO. 16
	PLOT SCALE = #SCALE#	DRAWN -	REVISED -		SCALE: 20	SHEET NO. 3 OF 6 SHEETS	STA. 641+00	TO STA. 646+22.35	CONTRACT NO. 76B18			
	PLOT DATE = #DATE#	CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									



SAND MODULE IMPACT ATTENUATOR LAYOUT
(IF OPTION USED)

EXIST. CURVE C1
 PI STA. = 630+53.90
 $\Delta = 11^\circ 39' 50''$ (L.T.)
 $D = 4^\circ 42' 42''$
 $R = 1,216.04'$
 $T = 124.21'$
 $L = 247.55'$
 $E = 6.33'$
 $e =$
 T.R. =
 S.E. RUN =
 P.C. STA. = 629+29.69
 P.T. STA. = 631+77.24



MATCHLINE STA. 635+00

SEQUENCE OF CONSTRUCTION

SEE STANDARD 701321 FOR LAYOUT OF TRAFFIC CONTROL. ALTERATIONS TO STANDARD 701321 ARE SHOWN ON STAGE CONSTRUCTION SHEETS IN PLANS.

PLACE TRAFFIC BARRIER TERMINALS AND GUARDRAIL ALONG WESTBOUND LANE ON BOTH ENDS OF STRUCTURE.

PLACE TEMPORARY RAMPS ON EACH END OF THE STRUCTURE.

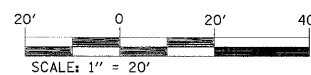
RELOCATE 487.5 FT AND PLACE 75 FT OF TEMPORARY CONCRETE BARRIER AND RELOCATE 2 EACH IMPACT ATTENUATORS, TEMPORARY.

UPON COMPLETION OF STRUCTURE CONSTRUCTION

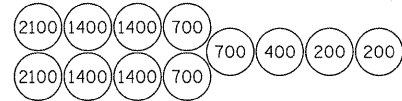
PLACE TRAFFIC BARRIER TERMINALS AND GUARDRAIL ALONG EASTBOUND LANE ON BOTH ENDS OF STRUCTURE.

REMOVE TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATORS.

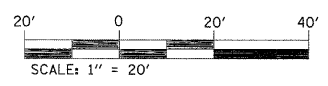
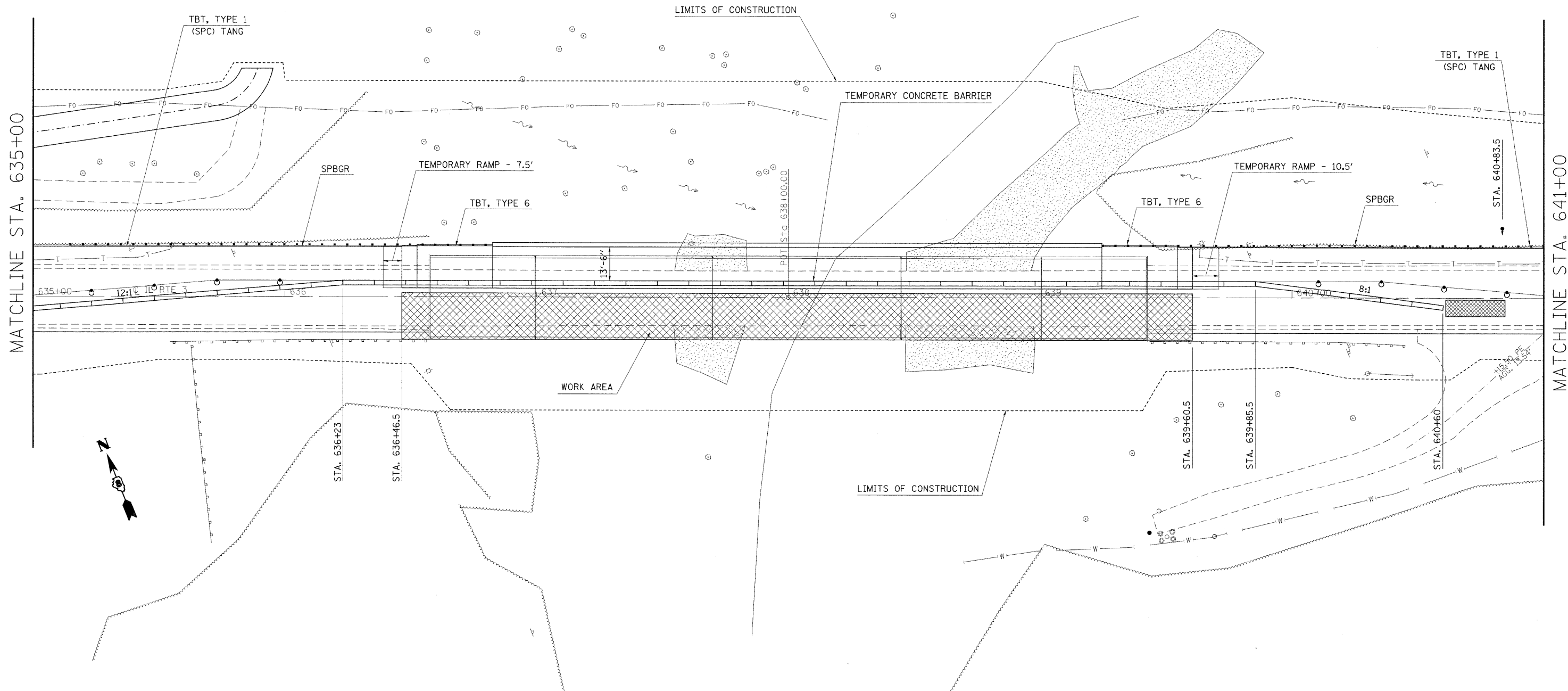
PLACE PERMANENT PAVEMENT MARKING ON ROADWAY AND BRIDGE ACCORDING TO PAVEMENT MARKING SHEETS.



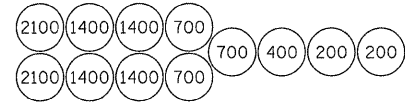
FILE NAME = #FILE#	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGE II CONSTRUCTION			F.A.I.L. RTE. 8956	SECTION 59BR-1	COUNTY MADISON	TOTAL SHEETS 75	SHEET NO. 17
	PLOT SCALE = #SCALE#	DRAWN -	REVISED -		SCALE: 20	SHEET NO. 4 OF 6 SHEETS	STA. 629+29.69 TO STA. 635+00	CONTRACT NO. 76B18				
	PLOT DATE = #DATE#	CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									



SAND MODULE IMPACT ATTENUATOR LAYOUT
(IF OPTION USED)



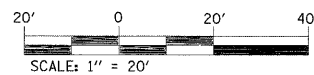
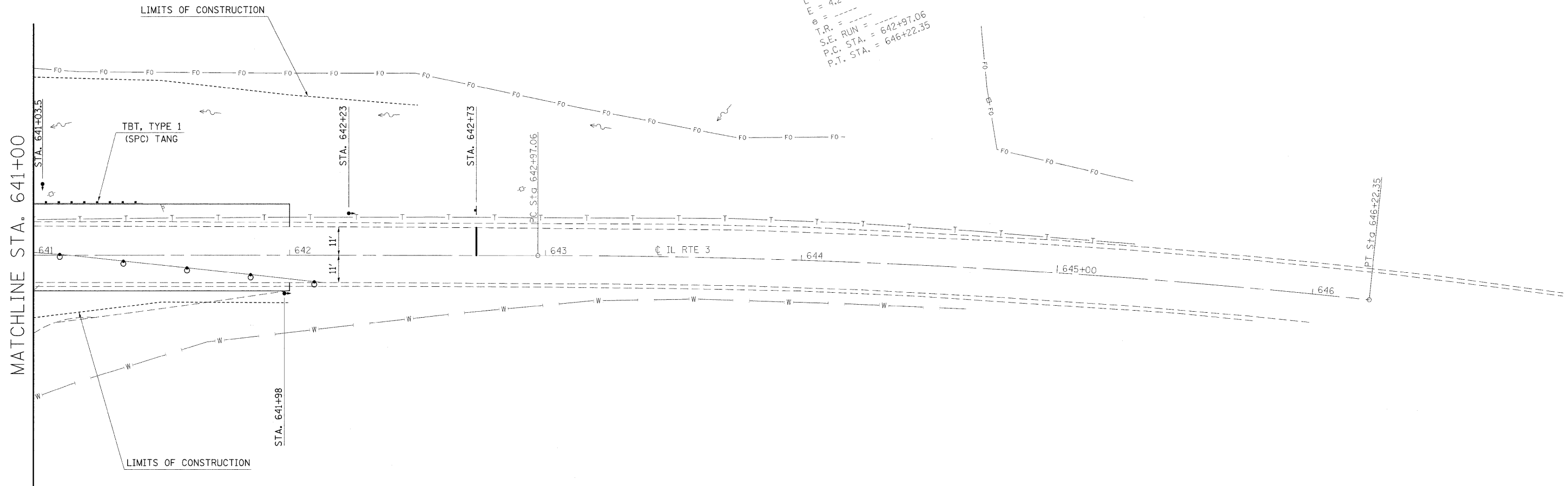
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	PLOT SCALE = #SCALE#	DRAWN -	REVISED -		SCALE: 20	SHEET NO. 5 OF 6 SHEETS	STA. 635+00	TO STA. 641+00	CONTRACT NO. 76B18			
	PLOT DATE = #DATE#	CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									



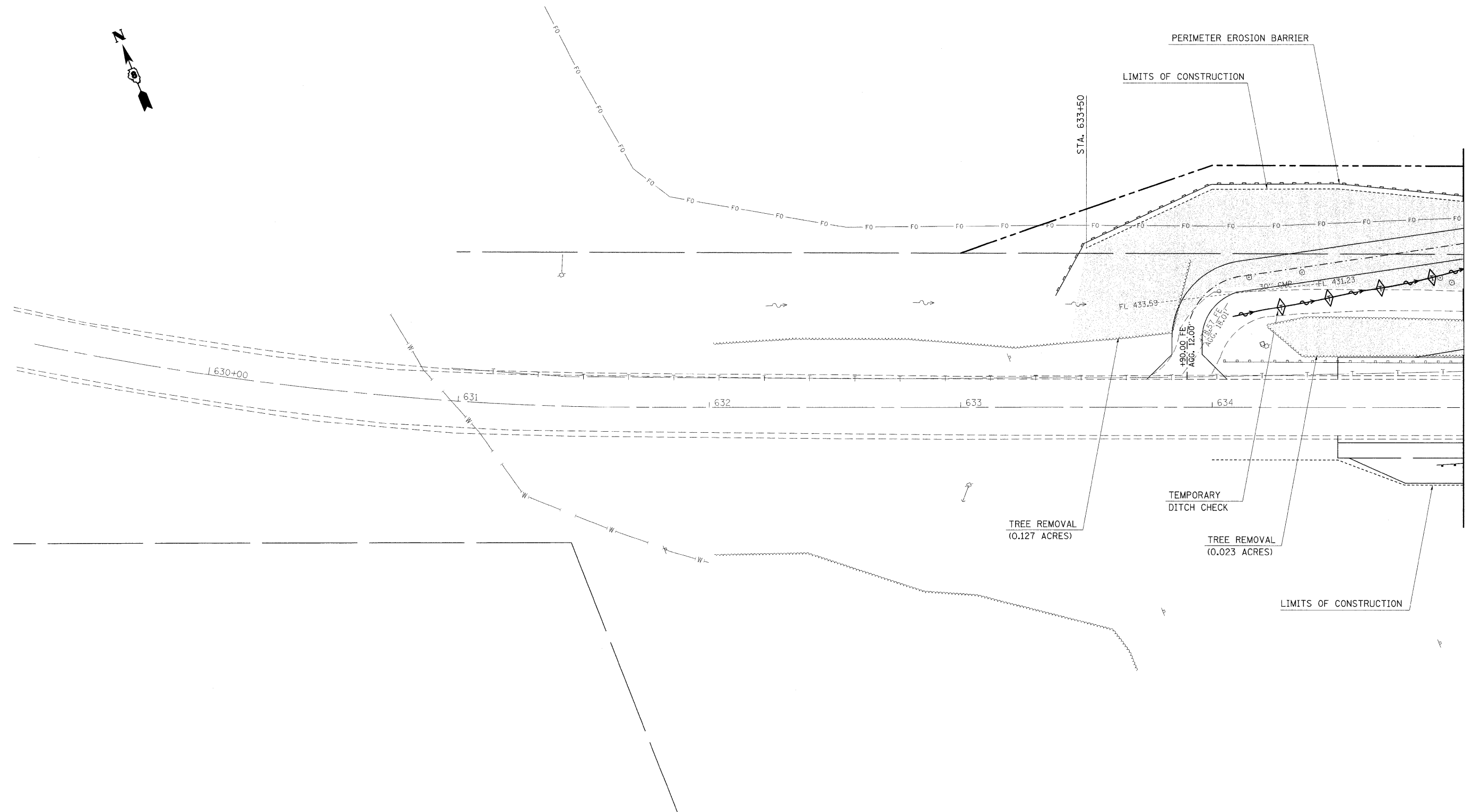
SAND MODULE IMPACT ATTENUATOR LAYOUT
(IF OPTION USED)



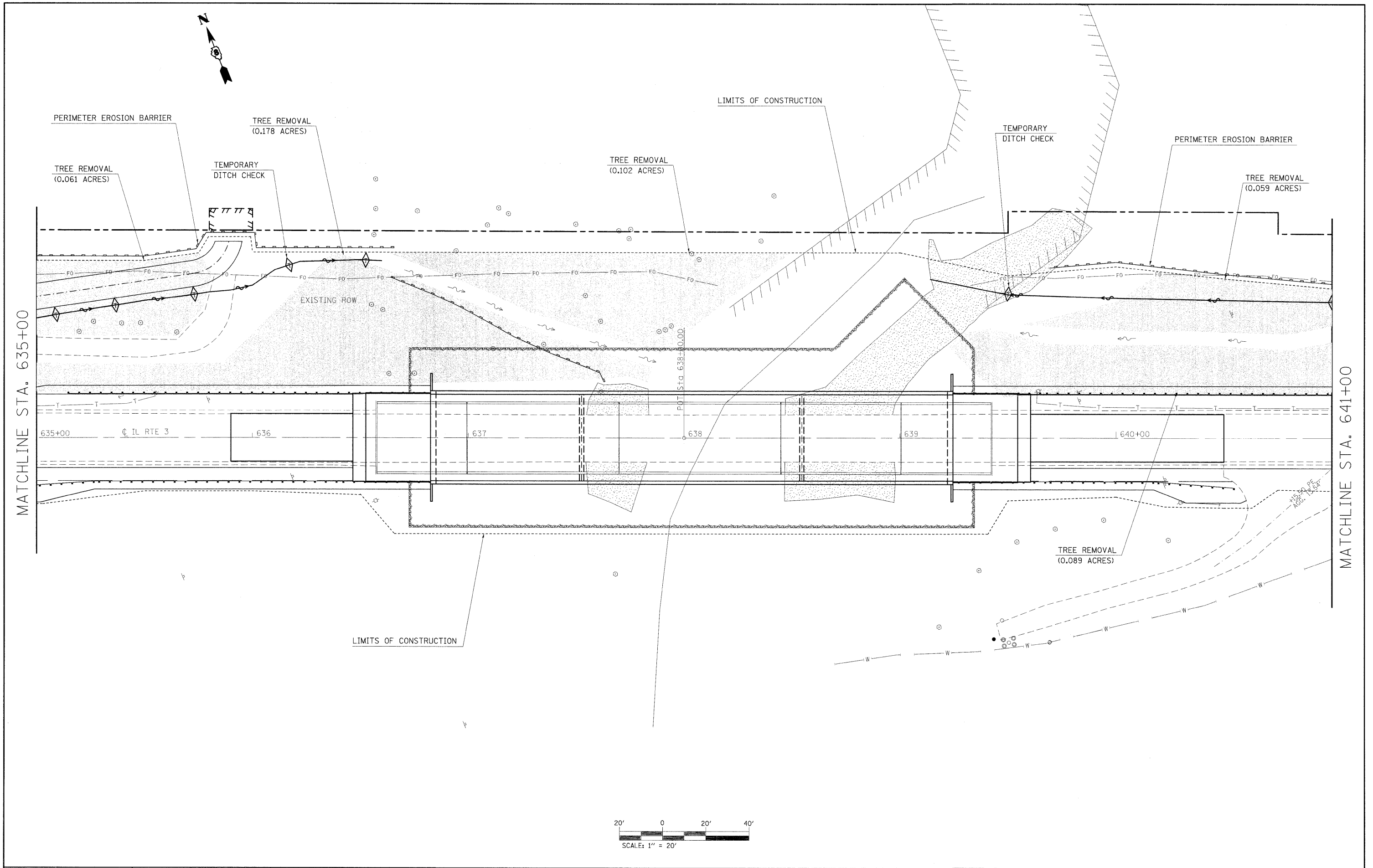
EXIST. CURVE C2
 PI STA. = 644+59.85
 $\Delta = 5^\circ 58' 26''$ (RT)
 $D = 1^\circ 50' 11''$
 $R = 3,120.01'$
 $T = 162.80'$
 $L = 325.30'$
 $E = 4.24'$
 $e =$
 T.R. RUN = 642+97.06
 S.E. STA. = 642+97.06
 P.C. STA. = 646+22.35
 P.T. STA. = 646+22.35



FILE NAME = #FILE#	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGE II CONSTRUCTION			F.A.I.L. RTE. 8956	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = #SCALE#	DRAWN -	REVISED -		59BR-1	MADISON	75		19			
	PLOT DATE = #DATE#	CHECKED -	REVISED -		SCALE: 20	SHEET NO. 6 OF 6 SHEETS	STA. 641+00	TO STA. 646+22.35	CONTRACT NO. 76B18			
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							



FILE NAME = #FILE#	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EROSION CONTROL			F.A.U. RTE. 8956	SECTION 598R-1	COUNTY MADISON	TOTAL SHEETS 75	SHEET NO. 20
	PLOT SCALE = #SCALE#	CHECKED -	REVISED -		SCALE: 20	SHEET NO. 1 OF 3 SHEETS	STA. 629+29.69 TO STA. 635+00	CONTRACT NO. 76B18				
PLOT DATE = #DATE#	DATE -	REVISED -	REVISED -		ILLINOIS FED. AID PROJECT							



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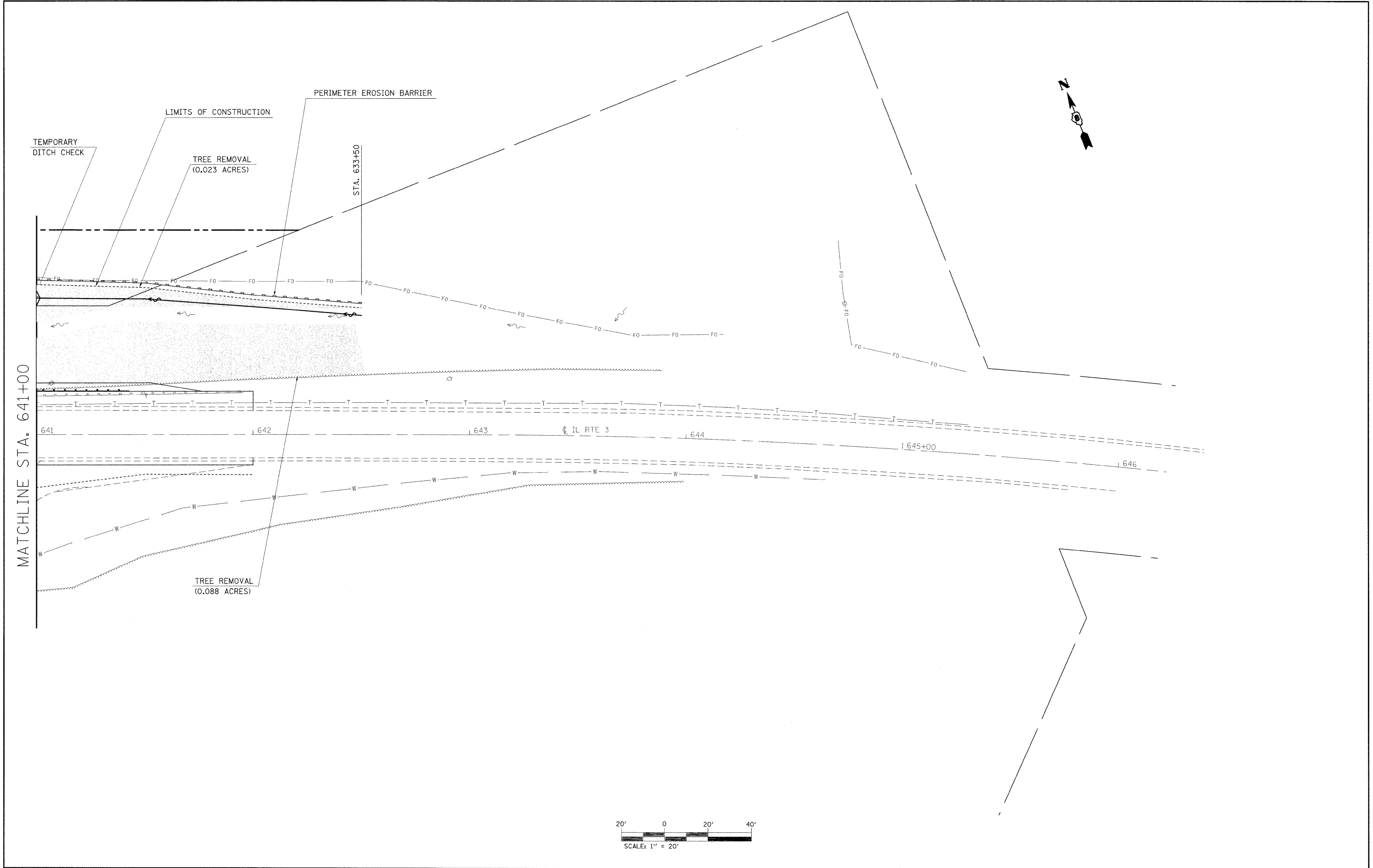
REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

EROSION CONTROL

SCALE: 20 SHEET NO. 2 OF 3 SHEETS STA. 629+35 TO STA. 641+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
8956	598R-1	MADISON	75	21
CONTRACT NO. 76B18				
ILLINOIS FED. AID PROJECT				



FILE NAME =
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USER NAME = #USER#
PLOT SCALE = #SCALE#
PLOT DATE = #DATE#

DESIGNED -
DRAWN -
CHECKED -
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

EROSION CONTROL

SCALE: 20 SHEET NO. 3 OF 3 SHEETS STA. 641+00 TO STA. 646+22.35

F.A.U. RTE. 8956	SECTION 598R-1	COUNTY MADISON	TOTAL SHEETS 75	SHEET NO. 22
CONTRACT NO. 76B18				
ILLINOIS FED. AID PROJECT				

LEGEND FOR EXISTING TOPOGRAPHIC SYMBOLS

TRAFFIC SIGNAL HANDHOLE	□	DRAINAGE FLOW LINE	≡
TRAFFIC SIGNAL GULFBOX	○	RIP RAP	▨
TRAFFIC SIGNAL HANDHOLE	□	HEADWALL	▬
TRAFFIC SIGNAL SIGNAL POST	○	CULVERT END SECTION	▬
TRAFFIC SIGNAL STEEL MAST ARM	—	DRAINAGE MANHOLE	○
TRAFFIC SIGNAL COMBINED MAST ARM	—	INLET	○
TRAFFIC SIGNAL PEDESTRIAN PUSH BUTTON	●	ROADWAY DITCH FLOW	≡
TRAFFIC SIGNAL WOODEN POLE	○	VEGETATION LINE	≡
TRAFFIC SIGNAL VEHICLE DETECTION PRIORITY	▴	STUMP	▲
TRAFFIC SIGNAL VEHICLE DETECTION MAGNET	▬	SHRUB	○
TRAFFIC SIGNAL JUNCTION BOX	□	EVERGREEN TREE	○
TRAFFIC SIGNAL CONTROLLER	□	DECIDUOUS TREE	○
TRAFFIC SIGNAL HEAVY DUTY HANDHOLE	▨	WOODS/BUSH PATTERN	≡
RAILROAD CANTILEVER MAST ARM	≡	TRAFFIC SIGN	▬
RAILROAD CROSSBUCK	≡	GAURDRAIL POST	▬
RAILROAD TRACK PATTERN	≡	GAURDRAIL PATTERN	≡
RAILROAD ABANDON PATTERN	≡	FIELD LINE	≡
RAILROAD CROSSGATE	≡	LEVEE/NOISE BARRIER	≡
RAILROAD CONTROL BOX	□	FENCE PATTERN	≡
RAILROAD FLASHING SIGNAL	≡	MAIL BOX	▬
TELEPHONE SPLICE BOX ABOVE GROUND	□	ADVERTISING SIGN	▬
UTILITY POWER POLE	▬	MARSH	≡
TELEPHONE POLE	○	LIGHTING HANDHOLE	○
UTILITY TRAFFIC SIGNAL	○	LIGHTING POWER POLE	○
UTILITY LIGHT POLE	≡	LIGHTING JUNCTION BOX	□
FIRE HYDRANT	○	LIGHTING HEAVYDUTY HANDHOLE	□
UTILITY MANHOLE	○	LIGHTING CONTROLLER	□
UTILITY TELEPHONE POLE	○	LIGHTING PULL POINT	○
UTILITY GUY POLE	≡	HIGHWAY LIGHTING ELECTRICAL GROUND	≡
PIPELINE WARNING SIGN	▬	HIGHWAY LIGHTING SINGLE UNIT	○
UTILITY HANDHOLE	▬	HIGHWAY LIGHTING DOUBLE UNIT	○
UTILITY SPLICE ABOVE GROUND	▬	EXISTING CONCRETE BARRIER	▬
UTILITY JUNCTION BOX	□	EXISTING CREEK OR DITCH	▬
UTILITY HEAVY DUTY HANDHOLE	▬	EXISTING EDGE OF PAVEMENT	▬
UTILITY DOUBLE HANDHOLE	▬		
UTILITY CONTROLLER	▬		
UTILITY WATER METER	○		

RIGHT OF WAY LEGEND

	SECTION CORNERS		QUARTER SECTION CORNERS
---	EXISTING CENTERLINE	---	EXISTING RIGHT OF WAY LINE
---	EXISTING IDOT EASEMENT LINE	---	EXISTING EASEMENT LINE
---	EXISTING ACCESS CONTROL LINE	---	EXISTING RIGHT OF WAY & PROPOSED ACCESS CONTROL LINE
---	PROPOSED ACCESS CONTROL LINE	---	PROPOSED CENTERLINE
---	PROPOSED RIGHT OF WAY LINE	---	PROPOSED TEMPORARY EASEMENT LINE
---	PROPOSED PERMANENT EASEMENT LINE	---	SECTION LINE
---	QUARTER SECTION LINE	---	QUARTER QUARTER SECTION LINE
---	PROPERTY (DEED) LINE	---	APPROPRIATE PROPERTY LINE
---	MEASURED DIMENSION	---	MEASURED DIMENSION
---	RECORDED DIMENSION	---	FOUND STONE
---	FOUND IRON PIPE OR IRON ROD AT CORNER UNLESS OTHERWISE NOTED	---	SET 5/8 INCH IRON ROD WITH PLASTIC CAP IDENTIFIED BY SURVEYORS LICENSE NUMBER AT CORNER UNLESS OTHERWISE NOTED
---	PERMANENT SURVEY MONUMENT, I.D.O.T. STD. 667(1) (TO BE SET BY OTHERS)	---	SET 5/8 INCH IRON ROD AS SURVEY CONTROL UNLESS OTHERWISE NOTED
---	FOUND CUT CROSS	---	SET CUT CROSS
---	SAME OWNERSHIP	---	EXISTING BUILDING

■ STAKING OF PROPOSED RIGHT OF WAY CORNERS. SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY ALUMINUM CAP TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS LICENSE NUMBER. (PROPOSED RIGHT OF WAY CORNERS SET IN CULTIVATED AREAS SHALL BE A MINIMUM OF 20 INCHES BELOW THE GROUND SURFACE).

LEGEND FOR ABBREVIATIONS

A/C	ACCESS CONTROL
AC	ACRE
AVE	AVENUE
BK	BOOK
BLVD	BOULEVARD
CL	CENTERLINE
CH	COUNTY HIGHWAY
Ch	CHAIN
DB	DEED BOOK
E	EAST
EX	EXISTING
FA	FEDERAL AID
FAI	FEDERAL AID INTERSTATE
FAP	FEDERAL AID PRIMARY
FAS	FEDERAL AID SECONDARY
FAUS	FEDERAL AID URBAN SECONDARY
FND	FOUND
ha	HECTARE
IP	IRON PIPE
IR	IRON ROD
LT	LEFT
m	METER
m ²	SQUARE METERS
N	NORTH
N & BC	NAIL AND BOTTLE CAP
N & C	NAIL AND CAP
N & W	NAIL AND WASHER
NE	NORTHEAST
NW	NORTHWEST
PB	PLAT BOOK
PG	PAGE
POB	POINT OF BEGINNING
POC	POINT OF COMMENCEMENT
POT	POINT OF TANGENT
PL	PROPERTY LINE
PR	PROPOSED
RD	ROAD
ROW	RIGHT OF WAY
RR	RAILROAD
RRS	RAILROAD SPIKE
RT	RIGHT
RTE	ROUTE
S	SOUTH
SBI	STATE BOND ISSUE
SE	SOUTHEAST
SO FT	SQUARE FEET
SR	STATE ROUTE
ST	STREET
STA	STATION
SMK	SURVEY MARKER
SW	SOUTHWEST
TWP	TOWNSHIP
TR	TOWNSHIP ROAD
USGS	U.S. GEOLOGICAL SURVEY
W	WEST

SPACE RESERVED FOR RECORDING OFFICER

PROPOSED PARCEL NUMBER LEGEND

8001001	PROPOSED FEE SIMPLE ACQUISITION
8001001PE	PROPOSED PERMANENT EASEMENT
8001001TE	PROPOSED TEMPORARY EASEMENT
8001001DED	PROPOSED DEDICATION
8001001AC	PROPOSED ACCESS CONTROL LINE

CURVE ABBREVIATIONS

PC	POINT OF CURVATURE
PI	POINT OF INTERSECTION
PT	POINT OF TANGENCY
PRC	POINT OF REVERSE CURVE
PCC	POINT OF COMPOUND CURVE
CB	CHORD BEARING
R	RADIUS OF CURVE
L	CURVE LENGTH
CB	CHORD BEARING
C	CHORD LENGTH
D	DEGREE OF CURVE
e	EXTERNAL
Δ	CENTRAL ANGLE



500 South 17th Street
Paducah, Kentucky 42003
Phone: (270) 443-1905
Fax: (270) 443-1904

401 North Market
Marion, IL 62859
Phone: (618) 987-9190

REGISTRATION NO. 184-003258

STATE OF KENTUCKY)
) SS
COUNTY OF McCRACKEN)

I, RONALD S. BACON, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, CERTIFY THAT I HAVE SURVEYED THE PLAT OF HIGHWAY SHOWN HEREON AND THAT THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY FOR THE PROPOSED PARCELS TO BE ACQUIRED BY THE STATE OF ILLINOIS, DEPARTMENT OF TRANSPORTATION, SHOWN HEREON.

DATED

Nov 10, 2008
Ronald S. Bacon
RONALD S. BACON, PLS NO. 035-003586
LICENSE EXPIRATION DATE: 11/30/2008



TOTAL HOLDING AREA SOURCE TABLE

1	AREA ACCORDING TO THE SURVEY PERFORMED BY THE CONSULTANT.
2	AREA LISTED IN RECORDED DEED.
3	AREA ACCORDING TO A RECORDED SUBDIVISION PLAT.
4	AREA ACCORDING TO A PLAT OF SURVEY.
5	AREA CALCULATED FROM RECORDED DEEDS OR TITLE COMMITMENTS - NOT SURVEYED.
6	AREA ACCORDING TO COUNTY TAX MAPS AND COUNTY ASSESSMENT RECORDS.
7	AREA ACCORDING TO OTHER RECORDS, SEE NOTE ON THE PLAT OF HIGHWAYS.

TOPOGRAPHIC STATEMENT

THE TOPOGRAPHY SHOWN HEREON WAS PROVIDED TO THE SURVEYOR BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION. THE SURVEYOR VISUALLY FIELD VERIFIED THE EXISTENCE OF THE TOPOGRAPHY SHOWN HEREON. IN ADDITION THE SURVEYOR PHYSICALLY LOCATED IN THE FIELD THE FOLLOWING ITEMS ON 6/6/08:

1. SECTIONS CORNERS
2. PROPERTY CORNERS
- 3.

BASIS OF COORDINATE & BEARING STATEMENT

COORDINATES AND BEARINGS SHOWN HEARON ARE BASED ON SURVEY CONTROL DATA AS PROVIDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, PROJECT COORDINATES ARE ON GROUND DATA.

**ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS**

FAU ROUTE 8956 (IL ROUTE 3)
SECTION 59BR-1
MADISON COUNTY
JOB NO. R-98-027-08

SHEET 2 OF 5

ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS/REGION 5/DISTRICT 8
1102 EASTPORT PLAZA DRIVE
COLLINSVILLE, ILLINOIS 62234-6198

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
8956	59BR-1	MADISON	75	23
CONTRACT NO. 76B18				

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

PART OF THE N.W. 1/4 OF SECTION 19, T6N, R10W, OF THE 3RD PM, MADISON COUNTY, ILLINOIS

CONST. @ CURVE C1
 PI STA. = 630+53.90
 Δ = 11° 39' 50" (LT)
 D = 4° 42' 42"
 R = 1,216.04'
 T = 124.21'
 L = 247.55'
 E = 6.33'
 P.C. STA. = 629+29.69
 P.T. STA. = 631+77.24

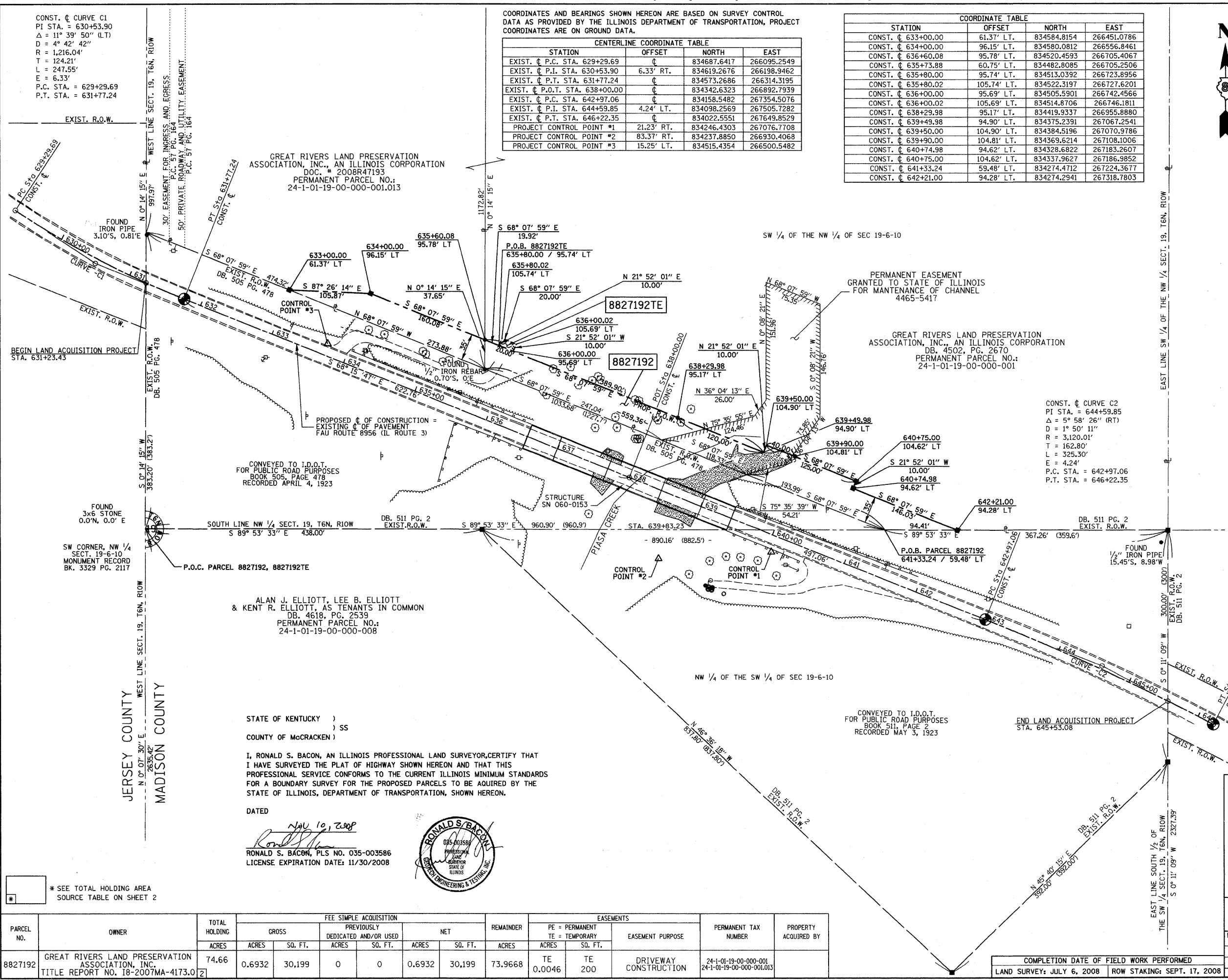
COORDINATES AND BEARINGS SHOWN HEREON ARE BASED ON SURVEY CONTROL DATA AS PROVIDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION. PROJECT COORDINATES ARE ON GROUND DATA.

CENTERLINE COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
EXIST. @ P.C. STA. 629+29.69	0	834687.6417	266095.2549
EXIST. @ P.I. STA. 630+53.90	6.33' RT.	834619.2676	266198.9462
EXIST. @ P.T. STA. 631+77.24	0	834573.2686	266314.3195
EXIST. @ P.O.T. STA. 638+00.00	0	834342.6323	266892.7939
EXIST. @ P.C. STA. 642+97.06	0	834158.5482	267354.5076
EXIST. @ P.I. STA. 644+59.85	4.24' LT.	834098.2569	267505.7282
EXIST. @ P.T. STA. 646+22.35	0	834022.5551	267649.8529
PROJECT CONTROL POINT #1	21.23' RT.	834246.4303	267076.7708
PROJECT CONTROL POINT #2	83.37' RT.	834237.8850	266930.4068
PROJECT CONTROL POINT #3	15.25' LT.	834515.4354	266500.5482

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
CONST. @ 633+00.00	61.37' LT.	834584.8154	266451.0786
CONST. @ 634+00.00	96.15' LT.	834580.0812	266556.8461
CONST. @ 636+00.00	95.78' LT.	834520.4593	266705.4067
CONST. @ 635+73.88	60.75' LT.	834482.8085	266705.2506
CONST. @ 635+80.00	95.74' LT.	834513.0392	266723.8956
CONST. @ 635+80.02	105.74' LT.	834522.3197	266727.6201
CONST. @ 636+00.00	95.69' LT.	834505.5901	266742.4566
CONST. @ 636+00.02	105.69' LT.	834514.8706	266746.1811
CONST. @ 638+29.98	95.17' LT.	834419.9337	266955.8880
CONST. @ 639+49.98	94.90' LT.	834375.2391	267067.2541
CONST. @ 639+50.00	104.90' LT.	834384.5196	267070.9786
CONST. @ 639+90.00	104.81' LT.	834369.6214	267108.1006
CONST. @ 640+74.98	94.62' LT.	834328.6822	267183.2607
CONST. @ 640+75.00	104.62' LT.	834337.9627	267186.9852
CONST. @ 641+33.24	59.48' LT.	834274.4712	267224.3677
CONST. @ 642+21.00	94.28' LT.	834274.2941	267318.7803



SPACE RESERVED FOR RECORDING OFFICER



STATE OF KENTUCKY)
) SS
 COUNTY OF McCRACKEN)

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DATED Nov 10, 2008

 RONALD S. BACON, PLS NO. 035-003586
 LICENSE EXPIRATION DATE: 11/30/2008



GEOTECH
 ENGINEERING & TESTING, INC.

500 South 17th Street
 Paducah, Kentucky 42003
 Phone: (270) 443-1995
 Fax: (270) 443-1804
 REGISTRATION NO. 184-003258

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS

FAU ROUTE 8956 (IL ROUTE 3)
 SECTION 59BR-1
 MADISON COUNTY
 JOB NO. R-98-027-08

STATION 631+23.43 TO STATION 645+53.08
 SCALE: 1" = 60'
 SHEET 3 OF 5

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS/REGION 5/DISTRICT 8
 1102 EASTPORT PLAZA DRIVE
 COLLINSVILLE, ILLINOIS 62234-6198

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
8956	59BR-1	MADISON	75	24

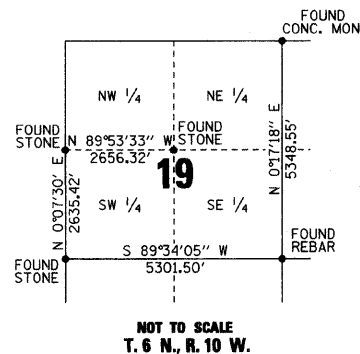
CONTRACT NO. 76B18

PARCEL NO.	OWNER	TOTAL HOLDING ACRES	FEE SIMPLE ACQUISITION				REMAINDER ACRES	EASEMENTS		PERMANENT TAX NUMBER	PROPERTY ACQUIRED BY		
			GROSS ACRES	SO. FT.	PREVIOUSLY DEDICATED AND/OR USED ACRES	SO. FT.		NET ACRES	SO. FT.			PE = PERMANENT ACRES	TE = TEMPORARY ACRES
8827192	GREAT RIVERS LAND PRESERVATION ASSOCIATION, INC. TITLE REPORT NO. 18-2007MA-4173.0	74.66	0.6932	30,199	0	0	0.6932	30,199	73.9668	TE 0.0046	TE 200	DRIVEWAY CONSTRUCTION	24-1-01-19-00-000-001 24-1-01-19-00-000-001.013

PART OF THE N.W. 1/4 OF SECTION 19, T6N, R10W, OF THE 3RD PM, MADISON COUNTY, ILLINOIS

BEARINGS SHOWN HEARON ARE BASED ON SURVEY CONTROL DATA AS PROVIDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, PROJECT COORDINATES ARE ON GROUND DATA.

SECTION CORNER COORDINATE TABLE		
CORNER DESCRIPTION	NORTH	EAST
SOUTHWEST CORNER SECTION 19	831640.8624	266257.7189
WEST QUARTER SECTION 19	834276.2741	266263.4694
CENTER SECTION 19	834271.2913	268919.7885
SOUTHEAST CORNER SECTION 19	831600.8982	271559.0695
NORTHEAST CORNER SECTION 19	836949.3796	271585.9785



NOT TO SCALE
T. 6 N., R. 10 W.

GREAT RIVERS LAND PRESERVATION ASSOCIATION, INC.
DOC # 2008R47193
PERMANENT PARCEL NO.: 24-1-01-19-00-000-001.013

GREAT RIVERS LAND PRESERVATION ASSOCIATION, INC.
DB. 4502, PG. 2670
PERMANENT PARCEL NO.: 24-1-01-19-00-000-001

SW CORNER, NW 1/4 SECT. 19-6-10 MONUMENT RECORD BK. 3329 PG. 2117

ALAN J. ELLIOTT, LEE B. ELLIOTT & KENT R. ELLIOTT
SALE BK. 4618, PG. 2539
PERMANENT PARCEL NO.: 24-1-01-19-00-000-008

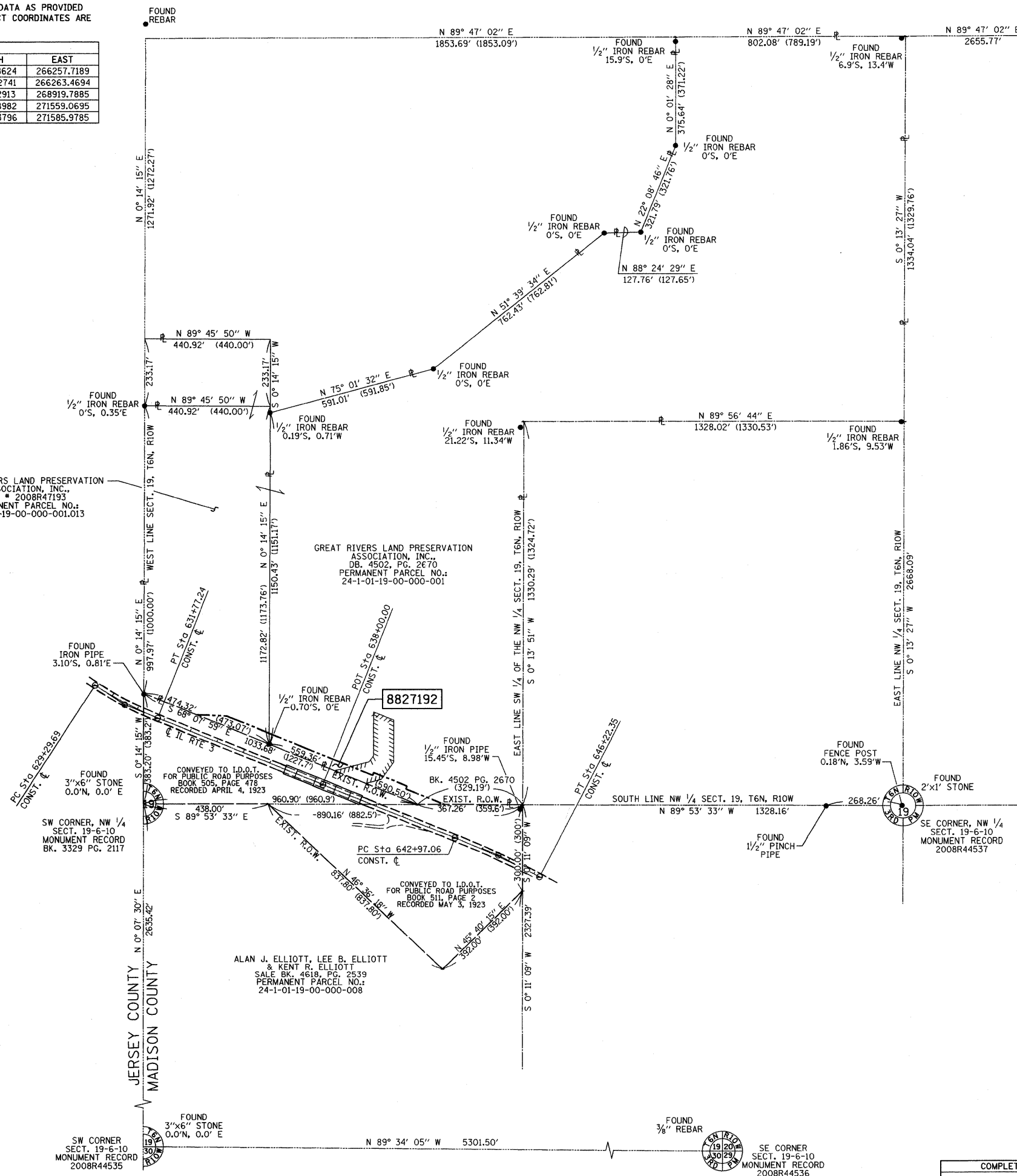
SW CORNER SECT. 19-6-10 MONUMENT RECORD 2008R44535

SE CORNER SECT. 19-6-10 MONUMENT RECORD 2008R44536

FOUND 3"x3" CON. MON.
NE CORNER SECT. 19-6-10 MONUMENT RECORD 2008R44538



SPACE RESERVED FOR RECORDING OFFICER



STATE OF KENTUCKY)
) SS
COUNTY OF McCRACKEN)

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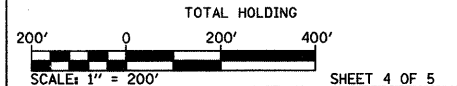
DATED Nov 10, 2008
RONALD S. BACON, PLS NO. 035-003586
LICENSE EXPIRATION DATE: 11/30/2008



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ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS
FAU ROUTE 8956 (IL ROUTE 3)
SECTION 59BR-1
MADISON COUNTY
JOB NO. R-98-027-08

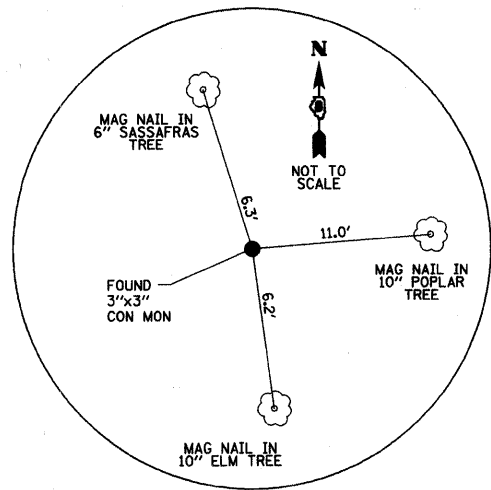


ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS/REGION 5/DISTRICT 8
1102 EASTPORT PLAZA DRIVE
COLLINGSVILLE, ILLINOIS 62234-6198

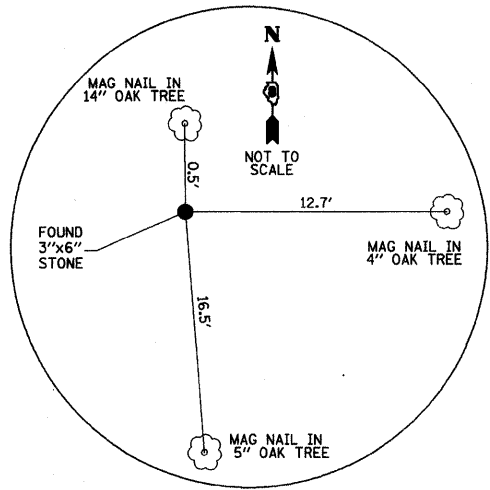
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
8956	59BR-1	MADISON	7.5	2.5

COMPLETION DATE OF FIELD WORK PERFORMED		CONTRACT NO. 76B18	
LAND SURVEY: JULY 6, 2008	ROW STAKING: SEPT. 17, 2008	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

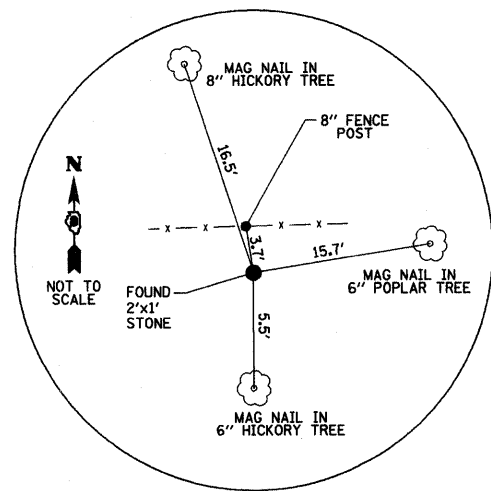
**MONUMENT RECORD
SECTION CORNER CONTROL TIES**



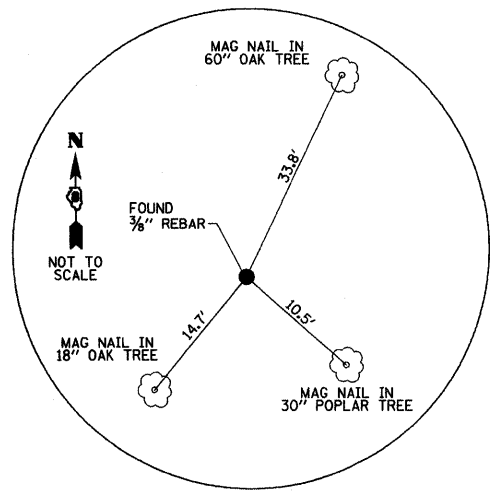
MON. REC.: DOC # 2008R44538
FILED: SEPTEMBER 22, 2008
N.E. CORNER SECTION 19
N 836949.3796 E 271585.9785



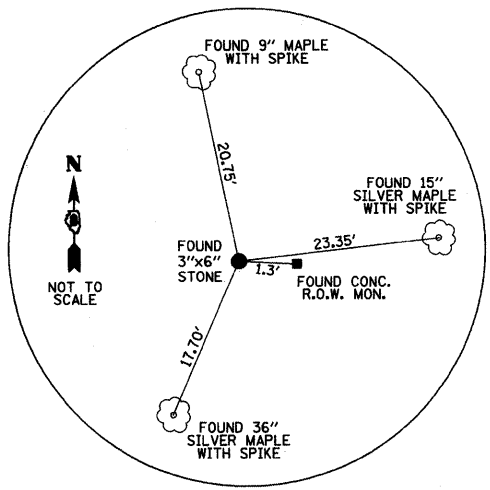
MON. REC.: DOC # 2008R44535
FILED: SEPTEMBER 22, 2008
S.W. CORNER SECTION 19
N 831640.8624 E 266257.7189



MON. REC.: DOC # 2008R44537
FILED: SEPTEMBER 22, 2008
CENTER SECTION 19
N 834271.2913 E 268919.7885

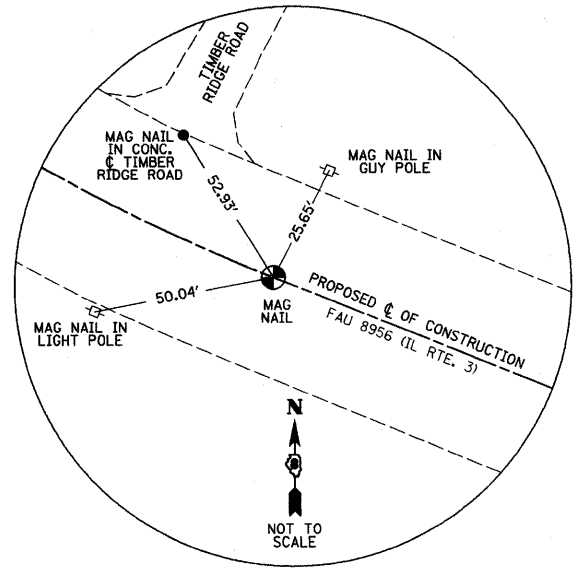


MON. REC.: DOC # 2008R44536
FILED: SEPTEMBER 22, 2008
S.E. CORNER SECTION 19
N 831600.8982 E 271559.0695

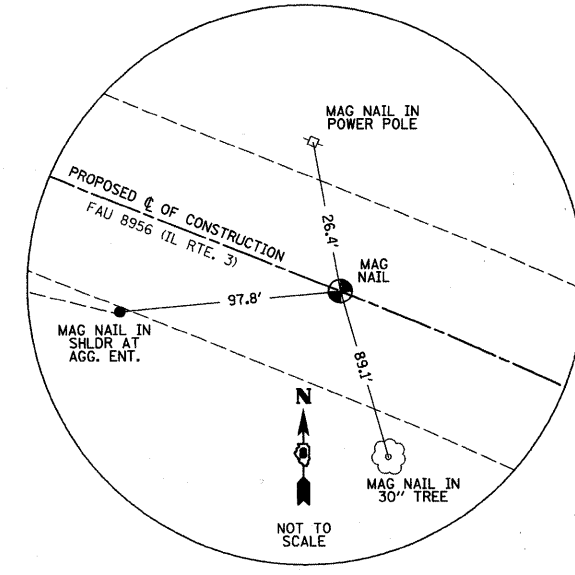


MON. REC.: BOOK 3329, PAGE 2117
FILED: JULY 8, 1985
WEST QUARTER SECTION 19
N 834276.2741 E 266263.4694

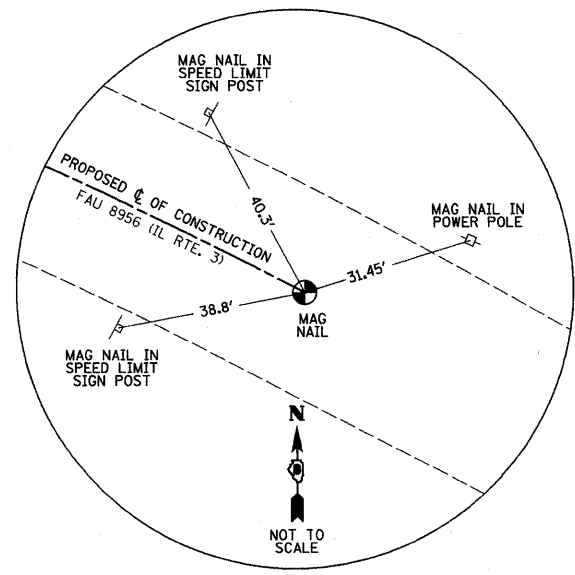
PROPOSED CENTERLINE OF CONSTRUCTION CONTROL TIES



P.T. STA. 631+77.44 PROPOSED \dot{C} OF CONSTRUCTION
FAU ROUTE 8956 / IL RT. 3
SET MAGNAIL



P.C. STA. 642+97.06 PROPOSED \dot{C} OF CONSTRUCTION
FAU ROUTE 8956 / IL RT. 3
SET MAGNAIL



P.T. STA. 646+22.35 PROPOSED \dot{C} OF CONSTRUCTION
FAU ROUTE 8956 / IL RT. 3
SET MAGNAIL

SPACE RESERVED FOR RECORDING OFFICER

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Phone: (270) 443-1995 Fax: (270) 443-1904
401 North Market Marion, IL 62959
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REGISTRATION NO. 184-003268

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS
FAU ROUTE 8956 (IL ROUTE 3)
SECTION 59BR-1
MADISON COUNTY
JOB NO. R-98-027-08
TIE DIAGRAMS

NOT TO SCALE SHEET 5 OF 5

ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS/REGION 5/DISTRICT 8
1102 EASTPORT PLAZA DRIVE
COLLINSVILLE, ILLINOIS 62234-6198

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
8956	59BR-1	MADISON	75	26
COMPLETION DATE OF FIELD WORK PERFORMED				CONTRACT NO. 76B18
LAND SURVEY: JULY 6, 2008		ROW STAKING: SEPT. 17, 2008		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

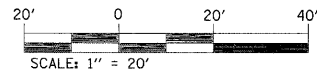
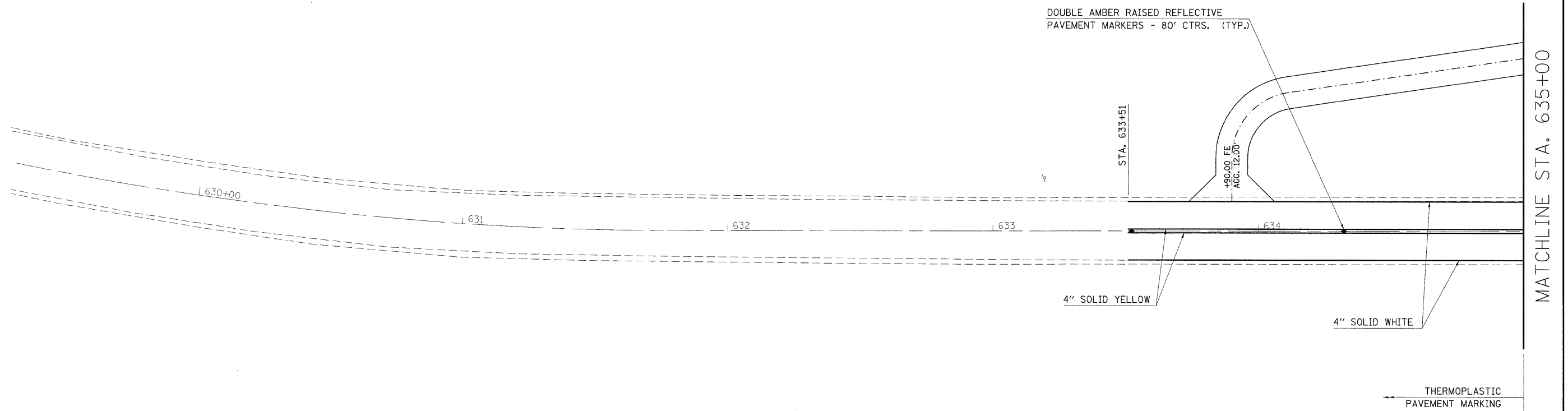
STATE OF KENTUCKY)
) SS
COUNTY OF McCRACKEN)

I, RONALD S. BACON, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, CERTIFY THAT I HAVE SURVEYED THE PLAT OF HIGHWAY SHOWN HEREON AND THAT THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY FOR THE PROPOSED PARCELS TO BE ACQUIRED BY THE STATE OF ILLINOIS, DEPARTMENT OF TRANSPORTATION, SHOWN HEREON.

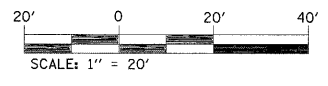
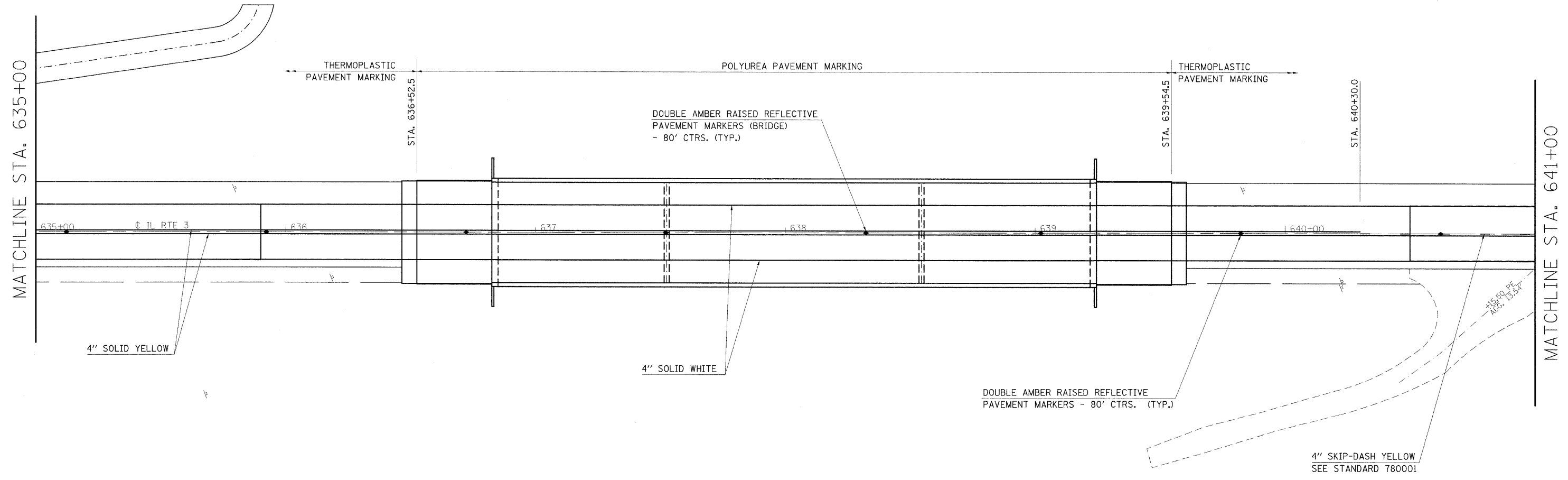
DATED

Nov 10, 2008
RONALD S. BACON, PLS NO. 035-003586
LICENSE EXPIRATION DATE: 11/30/2008

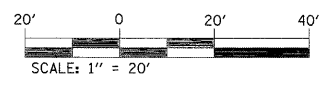
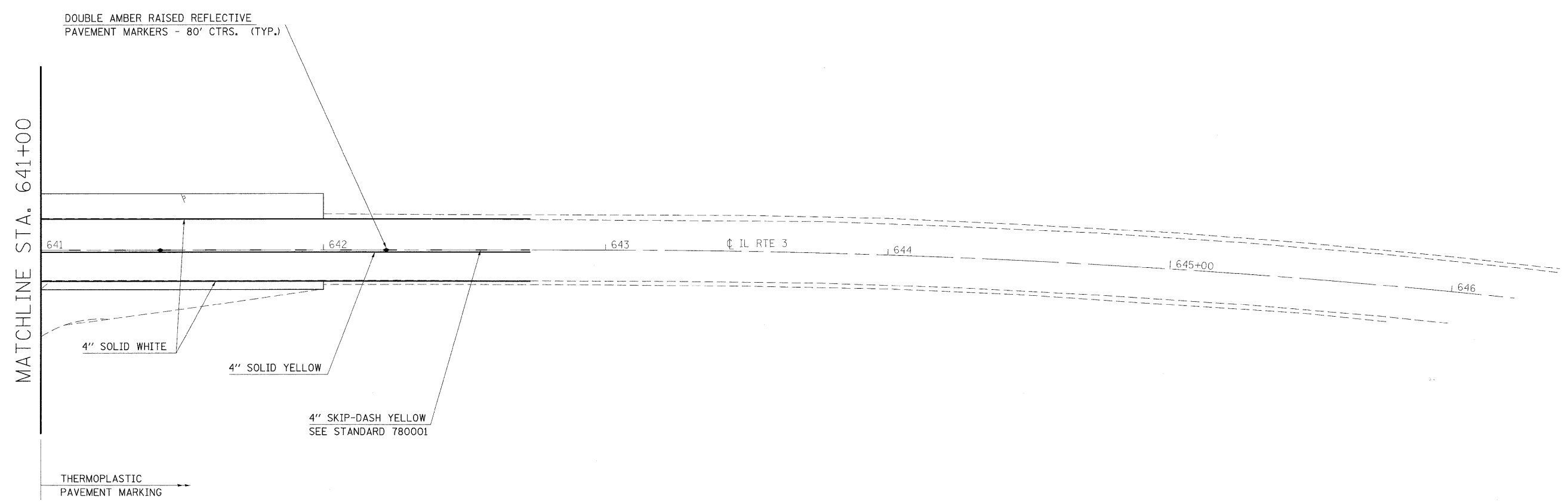




FILE NAME = #FILE#	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING			F.A.I.L. R.T.E. 8956	SECTION 59BR-1	COUNTY MADISON	TOTAL SHEETS 75	SHEET NO. 27
	PLOT SCALE = #SCALE#	CHECKED -	REVISED -					CONTRACT NO. 76B18				
	PLOT DATE = #DATE#	DATE -	REVISED -		SCALE: 20	SHEET NO. 1 OF 3 SHEETS	STA. 629+29.69 TO STA. 635+00	ILLINOIS FED. AID PROJECT				



FILE NAME = #FILEL#	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING			F.A.I.L. RTE. 8956	SECTION 59BR-1	COUNTY MADISON	TOTAL SHEETS 75	SHEET NO. 28
	PLOT SCALE = #SCALE#	DRAWN -	REVISED -		SCALE: 20	SHEET NO. 2 OF 3 SHEETS	STA. 635+00	TO STA. 641+00	CONTRACT NO. 76B18			
	PLOT DATE = #DATE#	CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
		DATE	REVISED -									



FILE NAME = #FILE#	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING			F.A.I.L. RTE. 8956	SECTION 59BR-1	COUNTY MADISON	TOTAL SHEETS 75	SHEET NO. 29
	PLOT SCALE = #SCALE#	DRAWN -	REVISED -		SCALE: 20	SHEET NO. 3 OF 3 SHEETS	STA. 641+00	TO STA. 646+22.35	CONTRACT NO. 76B18			
	PLOT DATE = #DATE#	CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

Bench Mark 203: WEST SITE RR spike driven in N side of power pole on S side Rte. 3, near NW corner of gravel turn-a-round, 600' W of center of Bridge SN:060-0153 at Piassa Creek, Elev. 449.14
 Bench Mark 204: EAST SITE RR spike driven in N side of power pole on S side Rte. 3, on W side of gravel service road, 232' E of center of Bridge SN:060-0153 over Piassa Creek, Elev. 443.67

Existing Structure: S.N. 060-0153 was built in 1922 under SBI Rte. 3, Section 59BR at Sta. 638+00.00. In 1979 the original single span steel truss bridge was removed and replaced with a 5 span PPC deck beam bridge. The open vault abutments were widened to accommodate the wider superstructure. Solid wall piers and approach bents, supported by concrete piles were added. In 2002, riprap was placed along the channel north of, and beneath the structure. The O. to O. width is 33'-0" and the total structure length is 286'-0" Bk. to Bk. Approach Bents. The structure is to be replaced using stage construction.

No salvage

STATION 638+11.00
 BUILT 20 BY
 STATE OF ILLINOIS
 F.A.U. RTE 8956 SEC. 59BR-1
 LOADING HL93
 STRUCTURE NO. 060-0343

NAME PLATE
 See Std. 515001

DESIGN SPECIFICATIONS

2007 AASHTO LRFD Bridge Design Specifications

LOADING HL-93

Allow 50#/sq. ft. for future wearing surface.

DESIGN STRESSES

FIELD UNITS

f'c = 3,500 psi
 fy = 60,000 psi (Reinforcement)
 fy = 50,000 psi (M270 Grade 50W)

SEISMIC DATA

Seismic Performance Zone (SPZ) = 1
 Bedrock Acceleration Coefficient (A) = 7.75%
 Site Coefficient (S) = 1.5

Fasteners shall be AASHTO M164 Type 1, mechanically galvanized bolts in painted areas and M164 Type 3 in unpainted areas. Bolts $\frac{7}{8}$ in. ϕ , holes $\frac{15}{16}$ in. ϕ , unless otherwise noted.

Calculated weight of Structural Steel M270 Grade 50W = 332,950 lbs.
 Calculated weight of Structural Steel - Misc. Items = 5040 lbs.

All structural steel shall be AASHTO M 270 Grade 50W.

No field welding is permitted except as specified in the contract documents.

Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.

Reinforcement bars designated (E) shall be epoxy coated.

Bearing seat surfaces shall be constructed or adjusted to their designated elevations within a tolerance of $\frac{1}{8}$ inch (0.01 ft.). Adjustment shall be made either by grinding the surface or by shimming the bearings.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

Structural steel shall only be painted for a distance equal to the depth of embedment into the concrete cap plus 3 inches. Those areas shall be primed in the shop with a Department approved zinc rich primer. No field painting shall be required. All structural steel shall be cleaned as specified in the Special Provision for "Surface Preparation and Painting Requirements for Weathering Steel".

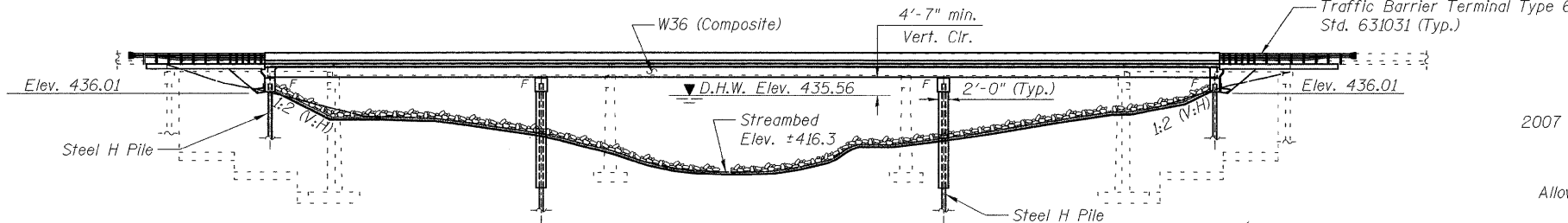
Layout of slope protection system may be varied in the field to suit ground conditions as directed by the Engineer.

The Contractor shall drive test piles to 110% of the nominal required bearing specified in production locations at substructures specified or approved by the Engineer before ordering the remainder of piles.

If a portion of the pier wall or concrete encasement is underwater, reinforcement may be placed underwater into forms. Concrete shall be tremied according to Article 503.08 of the Standard Specifications to an elevation of 1'-0" above the water line at the time of construction.

The Contractor is advised that the existing PPC Deck beams are in a deteriorated condition with reduced load carrying capacity. It is the Contractor's responsibility to account for the condition of the beams when developing construction procedures for removal and replacement of the superstructure.

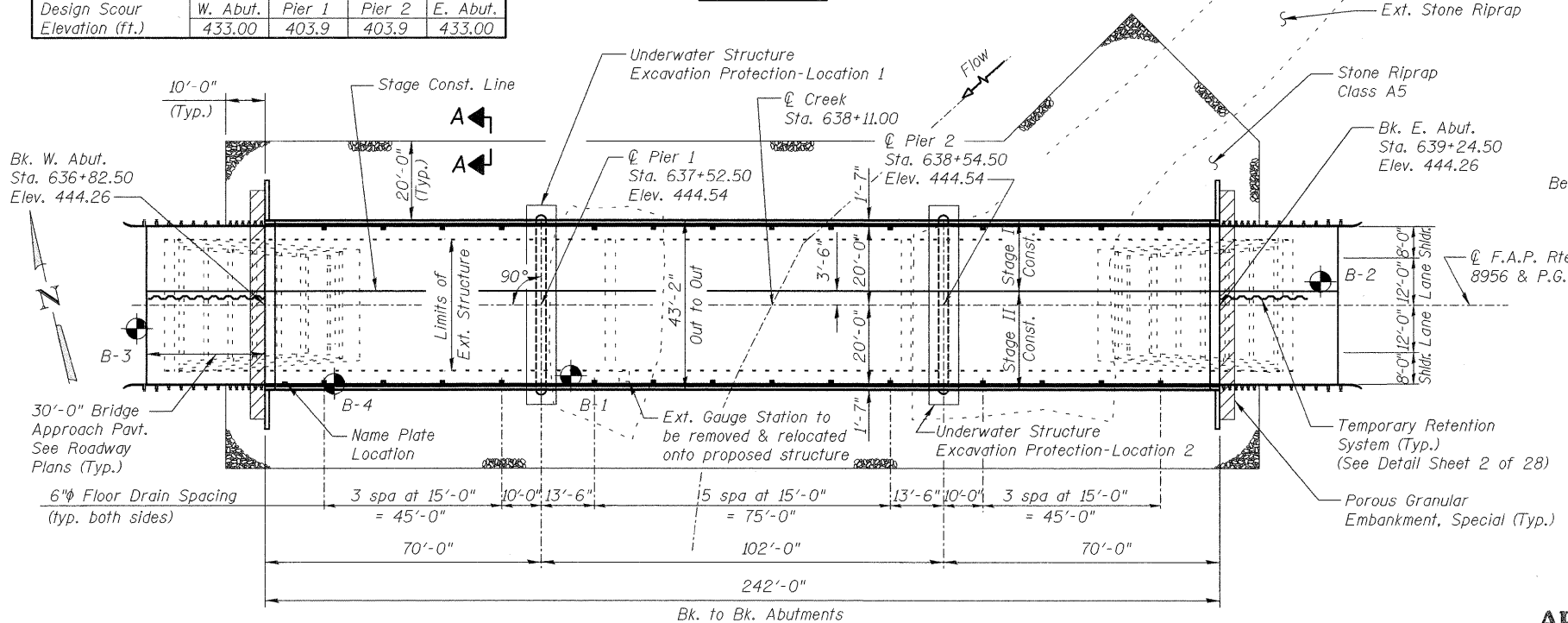
If the Contractor's procedures for existing beam removal involves placement of heavy equipment on the existing deck beams, a detailed procedure shall be submitted to the Engineer for approval. The procedure shall include calculations, sealed by an Illinois Licensed Structural Engineer, verifying the structural adequacy of the beams for the proposed loads. Cost included with Removal of Existing Structures.



DESIGN SCOUR ELEVATION TABLE

Design Scour Elevation (ft.)	W. Abut.	Pier 1	Pier 2	E. Abut.
	433.00	403.9	403.9	433.00

ELEVATION



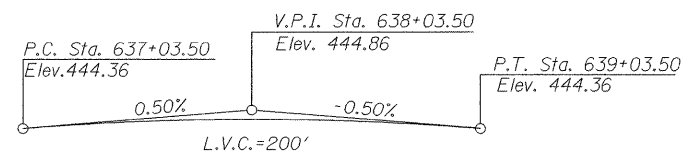
PLAN

WATERWAY INFORMATION

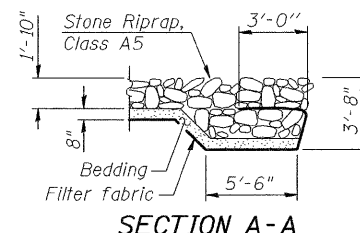
Drainage Area = 102.43 mi² Low Grade Elev. 443.85 @ Sta. 641+09.00

Flood	Freq. Yr.	Q C.F.S.	Opening Sq. Ft.		Nat. H.W.E.		Head - Ft.		Headwater El.	
			Exist.	Prop.	Exist.	Prop.	Exist.	Prop.	Exist.	Prop.
Scour	10	7,908	1,651	1,676	433.86	0.89	0.83	434.75	434.69	
Design	50	12,421	1,974	2,027	435.56	1.46	1.33	437.02	436.89	
Base	100	14,491	2,101	2,169	436.23	1.69	1.52	437.92	437.75	
Overtopping										
Max. Calc.	500	19,484	2,373	2,477	437.66	2.27	2.00	439.93	439.66	

Note: Existing Velocity (f/s) / Frequency (yr) 4.63/10; 6.04/50; 6.61/100; 7.85/500
 Proposed Velocity (f/s) / Frequency (yr) 4.57/10; 5.90/50; 6.43/100; 7.55/500



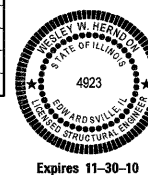
PROFILE GRADE
 (along ϕ roadway)



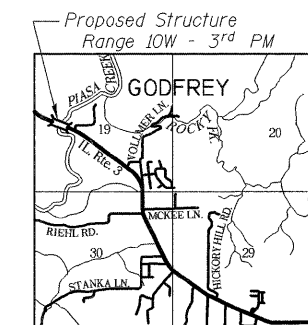
SECTION A-A

APPROVED
 FOR STRUCTURAL ADEQUACY ONLY

Ralph E. Anderson
 ENGINEER OF BRIDGES AND STRUCTURES



Wesley W. Herndon
 WESLEY W. HERNDON, S.E. DATE 3/20/09



LOCATION SKETCH

GENERAL PLAN AND ELEVATION
 ILLINOIS ROUTE 3 OVER
 PIASSA CREEK
 F.A.U. ROUTE 8956 SEC 59BR-1
 MADISON COUNTY
 STATION 638+11.00
 STRUCTURE NO. 060-0343

DESIGNED	DB
CHECKED	RS
DRAWN	ER
CHECKED	WWH



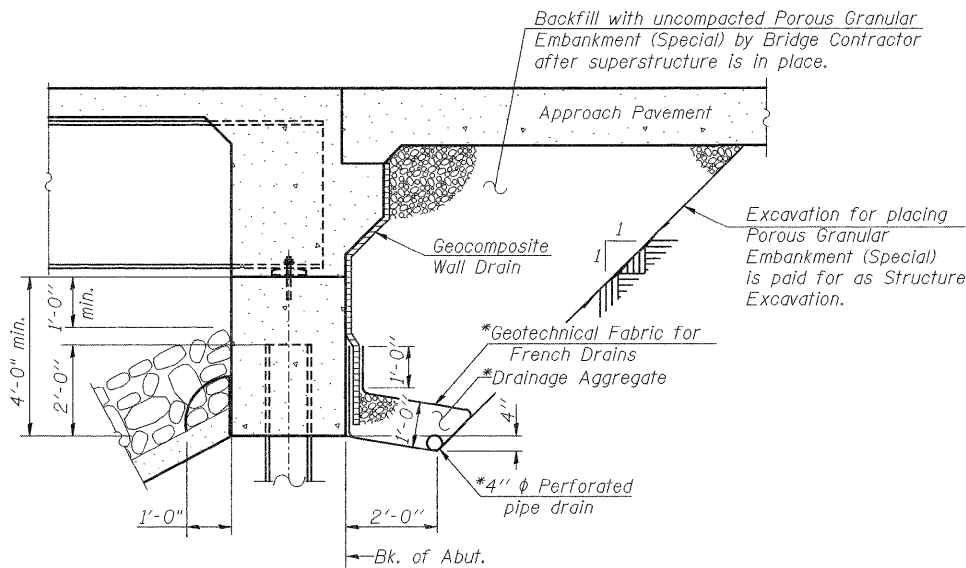
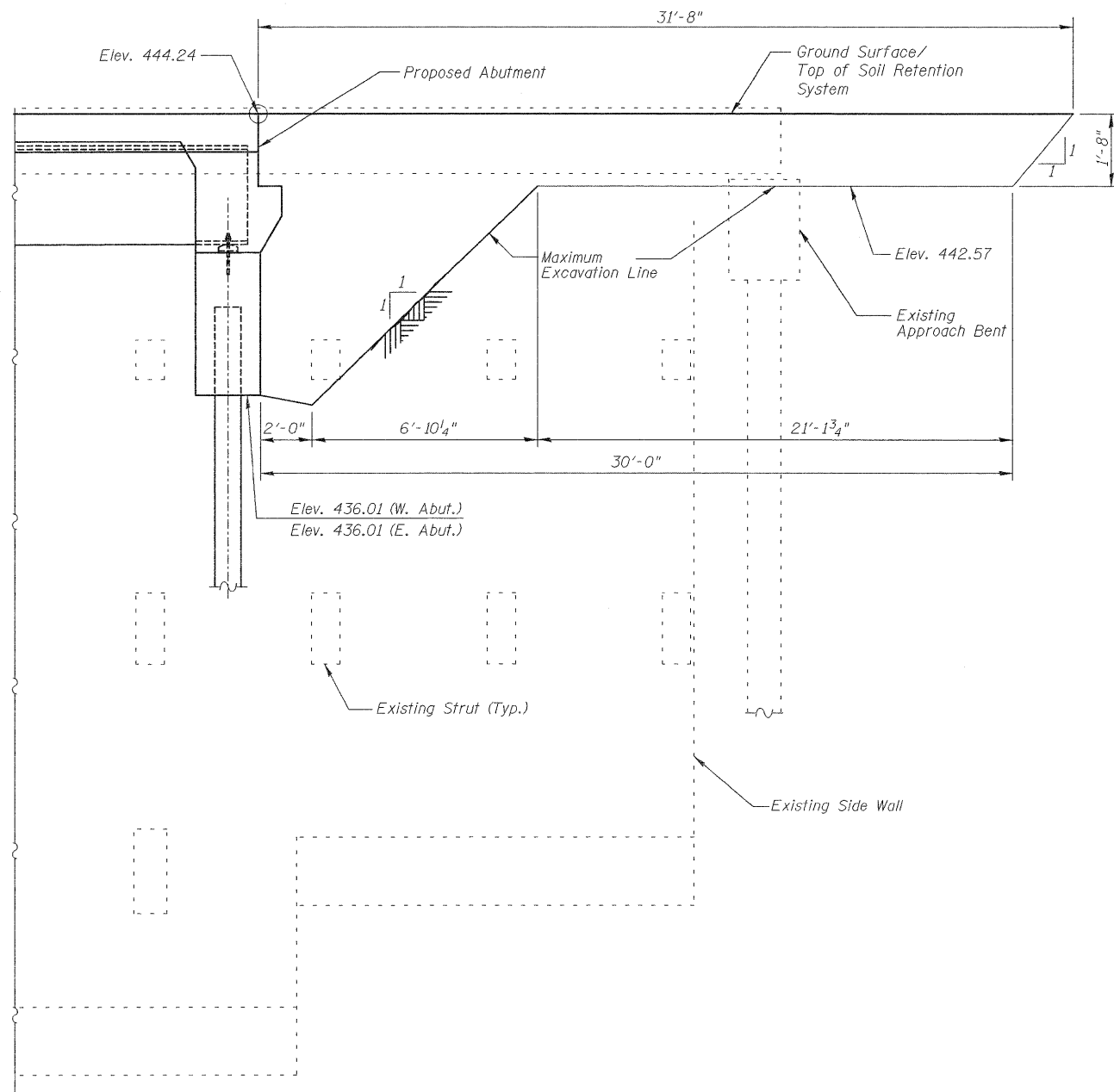
Juneau Associates, Inc. P.C.
 CONSULTING ENGINEERS AND LAND SURVEYORS
 2100 State Street
 Granite City, Illinois 62040
 Phone: (618) 977-1400
 Fax: (618) 462-5541
 Registered Professional Service Corporation License No. 184-003389
 100 North Research Drive
 Edwardsville, Illinois 62025
 Phone: (618) 959-0900 / (618) 650-0000
 Fax: (618) 959-0941

SHEET NO. 1	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
28 SHEETS	8956	59BR-1	MADISON		30
STRUCTURE NO. 060-0343		CONTRACT NO. 76B18			
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

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2. General Details
3. Stage Construction Details
4. Substructure Layout
5. Top of Slab Elevations
6. Top of Slab Elevations
7. Top of Slab Elevations
8. Top of Slab Elevations
9. Top of Approach Slab Elevations
10. Top of Approach Slab Elevations
11. Superstructure
12. Superstructure Details
13. Diaphragm Details
14. Structural Steel
15. Bearing Details
16. West Abutment
17. East Abutment
18. Pier 1
19. Pier 1 Details
20. Pier 2
21. Pier 2 Details
22. HP Pile Details
23. Temporary Concrete Barrier Details
24. Bar Splicer Details
25. Concrete Parapet Slipforming Option
26. Boring Logs
27. Boring Logs
28. Boring Logs



SECTION THRU INTEGRAL ABUTMENTS

(Horiz. dim. @ Rt. L's)

*Included in the cost of Pipe Underdrains for Structures.

Note:
All drainage system components shall extend to 2'-0" from the end of each wingwall except an outlet pipe shall extend until intersecting with the side slopes. The pipes shall drain into concrete headwalls. (See Article 601.05 of the Standard Specifications and Highway Standard 601101).

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Porous Granular Embankment, Special	Cu. Yd.		175	175
Stone Riprap, Class A5	Sq. Yd.		2550	2550
Filter Fabric	Sq. Yd.		2550	2550
Removal of Existing Structures	Each			1
Structure Excavation	Cu. Yd.		298	298
Floor Drains	Each	28		28
Concrete Structures	Cu. Yd.		200.0	200.0
Concrete Superstructure	Cu. Yd.	351.8		351.8
Bridge Deck Grooving	Sq. Yd.	1022		1022
Concrete Encasement	Cu. Yd.		10.6	10.6
Protective Coat	Sq. Yd.	1279		1279
Furnishing and Erecting Structural Steel	Lump Sum	1		1
Stud Shear Connectors	Each	4011		4011
Reinforcement Bars, Epoxy Coated	Pound	83110	19120	102230
Bar Splicers	Each	835	136	971
Furnishing Steel Piles HP 12x53	Foot		912	912
Furnishing Steel Piles HP 12x74	Foot		1246	1246
Driving Piles	Foot		2158	2158
Test Pile Steel HP 12x53	Each		2	2
Test Pile Steel HP 12x74	Each		1	1
Pile Shoes	Each		30	30
Name Plates	Each	1		1
Anchor Bolts, 1"	Each	56		56
Geocomposite Wall Drain	Sq. Yd.		90	90
Pipe Underdrain for Structures 4"	Foot		119	119
Temporary Soil Retention System	Sq. Ft.		172	172
Underwater Structure Excavation Protection-Location 1	Each		1	1
Underwater Structure Excavation Protection-Location 2	Each		1	1
Asbestos Bearing Pad Removal	Each	66		66

TEMPORARY SOIL RETENTION
SYSTEM AT ABUTMENTS

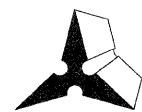
Note:
A cantilever sheet piling design does not appear feasible and additional members or other retention systems may be necessary. The Contractor shall submit a temporary soil retention system design including plan details and calculations for review and acceptance by the Engineer.

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Temporary Soil Retention System	Sq. Ft.	172

GENERAL DETAILS
ILLINOIS ROUTE 3 OVER
PIASA CREEK
F.A.U. ROUTE 8956 SEC 59BR-1
MADISON COUNTY
STATION 638+11.00
STRUCTURE NO. 060-0343

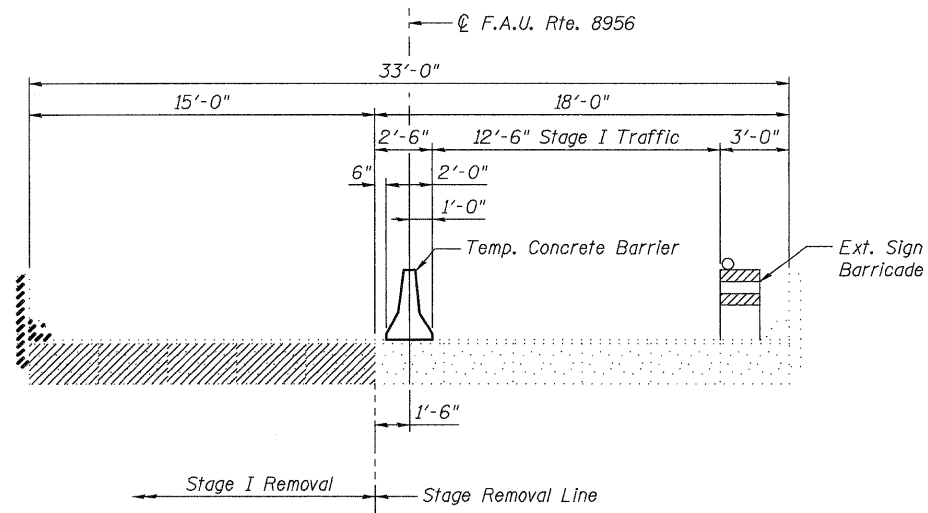
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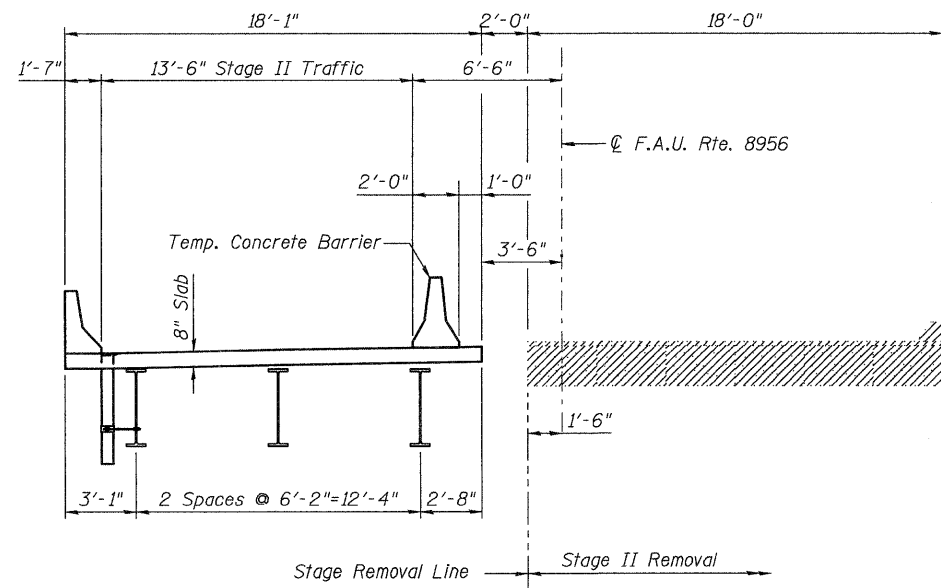
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CONSULTING ENGINEERS AND LAND SURVEYORS
2106 State Street
Granite City, Illinois 62040
Phone: (618)577-1400
Fax: (618)452-5541
100 North Research Drive
Edwardsville, Illinois 62025
Phone: (618)959-9000/(618) 659-0800
Fax: (618)959-0941
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SHEET NO. 2 28 SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	8956	59BR-1	MADISON		31
STRUCTURE NO. 060-0343		CONTRACT NO. 76B18			
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT		

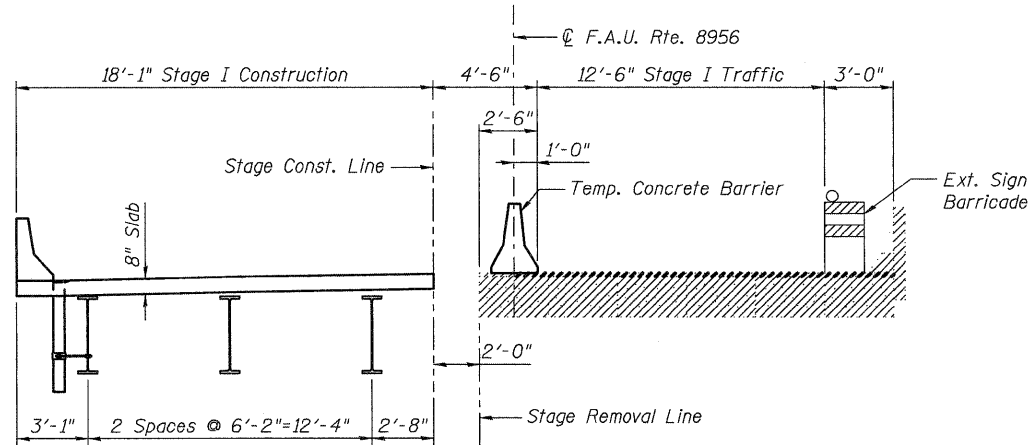
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



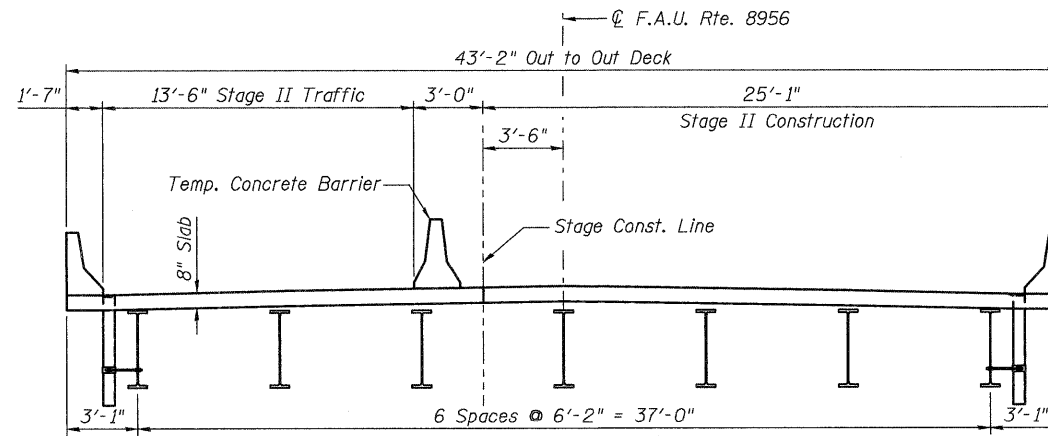
STAGE 1 REMOVAL
(Looking East)



STAGE 2 REMOVAL
(Looking East)



STAGE 1 CONSTRUCTION
(Looking East)



STAGE 2 CONSTRUCTION
(Looking East)

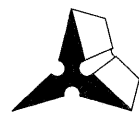
STAGE CONSTRUCTION
ILLINOIS ROUTE 3 OVER
PIASA CREEK
F.A.U. ROUTE 8956 SEC 59BR-1
MADISON COUNTY
STATION 638+11.00
STRUCTURE NO. 060-0343

Notes:

Hatched area indicates "Removal of Existing Structures".

For quantity of Temporary Concrete Barrier, see Roadway Plans.

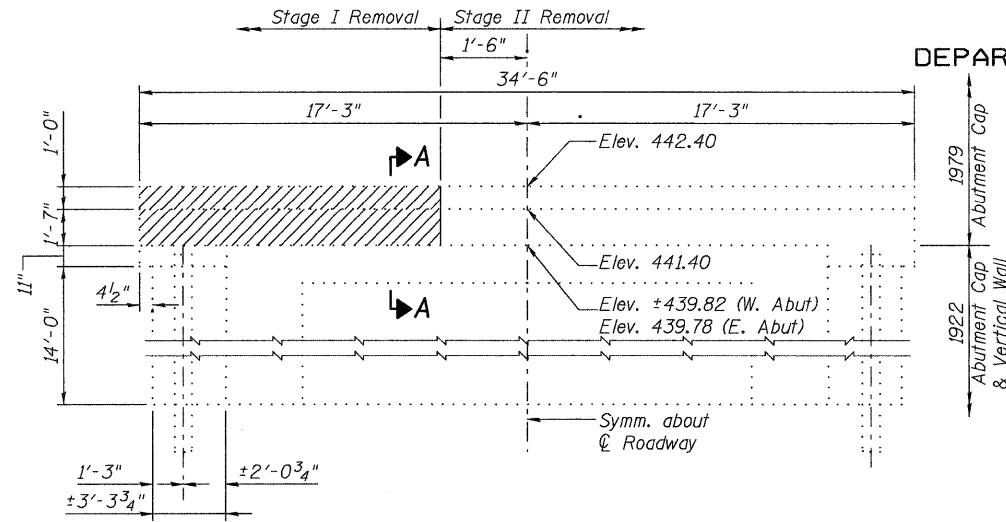
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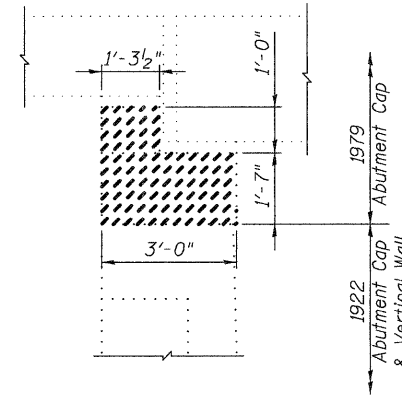
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Granite City, Illinois 62040
Phone: (618) 977-1400
Fax: (618) 462-6641
100 North Research Drive
Edwardsville, Illinois 62025
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SHEET NO. 3 28 SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	8956	59BR-1	MADISON	75	32
STRUCTURE NO. 060-0343			CONTRACT NO. 76B18		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



ELEVATION - ABUTMENT
(East Abutment - Looking East)
(West Abutment similar, but opposite hand)



Section A-A

Removal of Existing Abutments Notes:

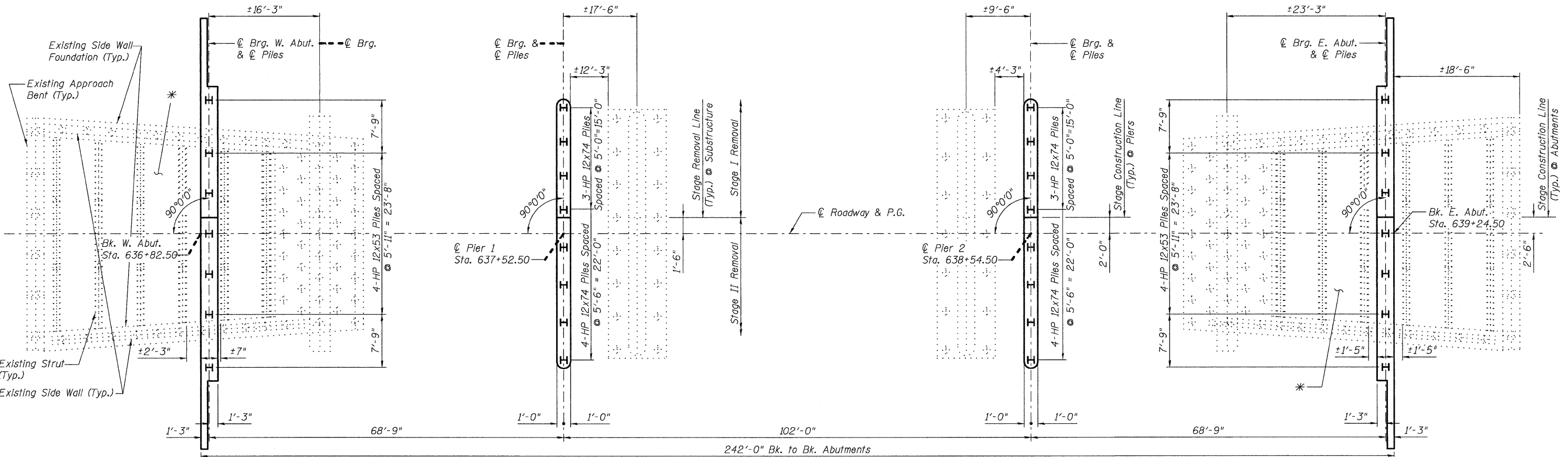
Stage 1 Removal

Removal of the westbound portion of the struts, side walls and approach bents, located behind the proposed abutments, shall be the minimum amount necessary for the placement of the soil retention system and the construction of the proposed abutments and bridge approach slabs, in accordance with Section 501 of the Standard Specifications.

Removal of the westbound portion of the abutment cap and side walls, located in front of the proposed abutments, shall extend down to the elevation shown corresponding to the top of the original 1922 abutment cap. The original 1922 abutment cap, the vertical wall, the side walls and struts, shall remain in place through Stage 1 Construction.

Stage 2 Removal

The removal of the remaining portion of the Abutments shall be in accordance with Section 501 of the Standard Specifications.



* Based on as-built drawings, the area confined by the existing abutment vertical wall, side walls and approach bent contains earth material.

SUBSTRUCTURE LAYOUT
ILLINOIS ROUTE 3 OVER
PIASA CREEK
F.A.U. ROUTE 8956 SEC 59BR-1
MADISON COUNTY
STATION 638+11.00
STRUCTURE NO. 060-0343

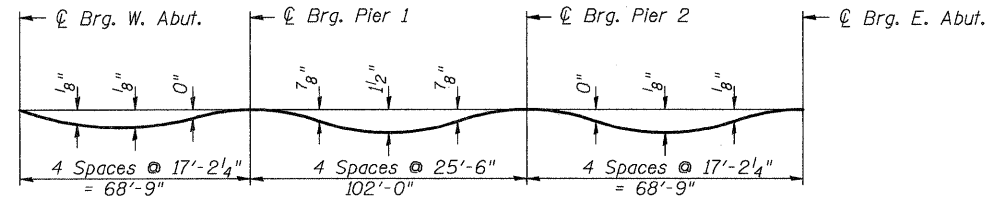
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DRAWN	ER
CHECKED	WWH



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Granite City, Illinois 62040
Phone: (618) 977-1400
Fax: (618) 452-5541
100 North Research Drive
Edwardsville, Illinois 62025
Phone: (618) 659-9900/(618) 659-0800
Fax: (618) 659-4941
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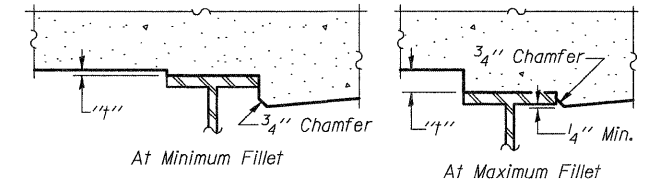
SHEET NO. 4 28 SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	8956	59BR-1	MADISON	75	33
STRUCTURE NO. 060-0343			CONTRACT NO. 76B18		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



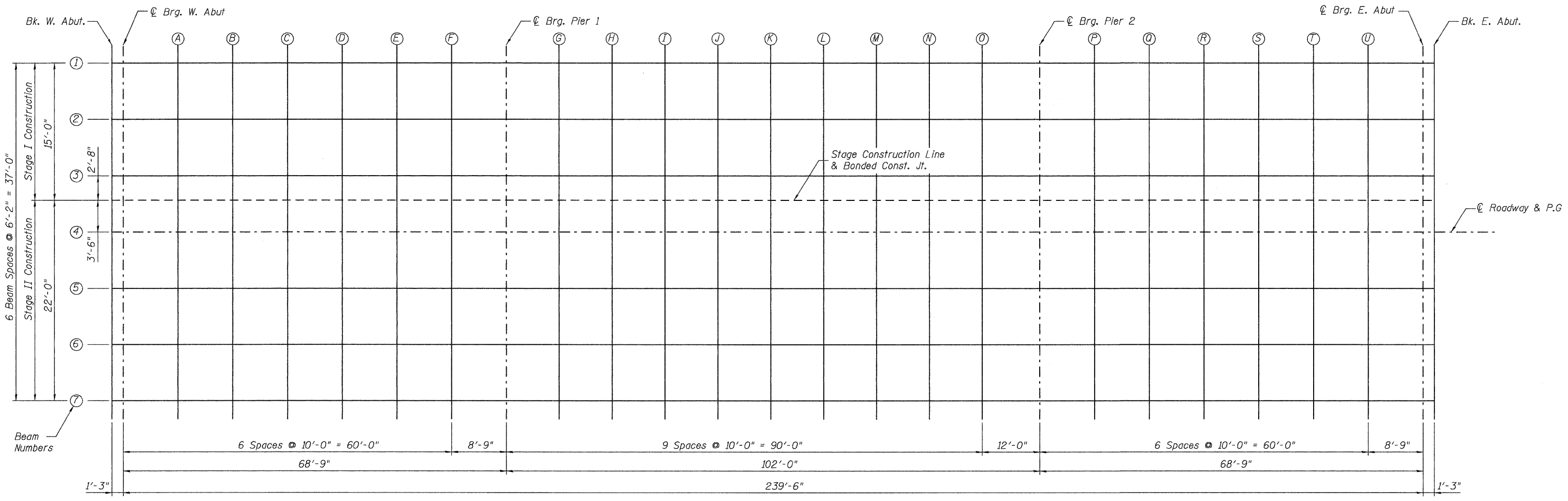
DEAD LOAD DEFLECTION DIAGRAM
(Includes weight of concrete only.)

Note: The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown on sheets 6, 7, & 8 of 28.



To determine "t": After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals, see sheet 6, 7, & 8 of 28. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown on sheets 6, 7, & 8 of 28, minus slab thickness, equals the fillet heights "t" above top flange of beams.

FILLET HEIGHTS



PLAN

TOP OF SLAB ELEVATIONS
ILLINOIS ROUTE 3 OVER
PIASA CREEK
F.A.U. ROUTE 8956 SEC 59BR-1
MADISON COUNTY
STATION 638+11.00
STRUCTURE NO. 060-0343

DESIGNED	DB
CHECKED	RS
DRAWN	ER
CHECKED	WWH

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2100 State Street
Granite City, Illinois 62040
Phone: (618) 977-1400
Fax: (618) 452-5541

100 North Research Drive
Edwardsville, Illinois 62025
Phone: (618) 659-0900 / (618) 659-0800
Fax: (618) 659-0841

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Note: Work this sheet with sheet 6, 7, & 8 of 28.

SHEET NO. 5 28 SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	8956	59BR-1	MADISON	75	34
STRUCTURE NO. 060-0343		CONTRACT NO. 76B18			
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT		

Printed 2:44:05 PM 11/27/09 IL Rfs 3:\Drawing\Revised Final\Drawing with DOT Comment\05 Top of Slab Elevations.dgn

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BEAM 1

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abutment	636+82.50	-18.50	443.93	443.93
☉ Brg. W. Abutment	636+83.75	-18.50	443.94	443.94
A	636+93.75	-18.50	443.99	444.00
B	637+03.75	-18.50	444.04	444.05
C	637+13.75	-18.50	444.09	444.10
D	637+23.75	-18.50	444.13	444.14
E	637+33.75	-18.50	444.17	444.16
F	637+43.75	-18.50	444.20	444.19
☉ Brg. Pier 1	637+52.50	-18.50	444.22	444.22
G	637+62.50	-18.50	444.25	444.27
H	637+72.50	-18.50	444.26	444.33
I	637+82.50	-18.50	444.28	444.38
J	637+92.50	-18.50	444.28	444.42
K	638+02.50	-18.50	444.29	444.44
L	638+12.50	-18.50	444.29	444.43
M	638+22.50	-18.50	444.28	444.39
N	638+32.50	-18.50	444.27	444.34
O	638+42.50	-18.50	444.25	444.28
☉ Brg. Pier 2	638+54.50	-18.50	444.22	444.22
P	638+64.50	-18.50	444.19	444.19
Q	638+74.50	-18.50	444.16	444.16
R	638+84.50	-18.50	444.12	444.13
S	638+94.50	-18.50	444.08	444.09
T	639+04.50	-18.50	444.03	444.05
U	639+14.50	-18.50	443.98	443.99
☉ Brg. E Abutment	639+23.25	-18.50	443.94	443.94
Bk. E. Abutment	639+24.50	-18.50	443.93	443.93

BEAM 2

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abutment	636+82.50	-12.33	444.06	444.06
☉ Brg. W. Abutment	636+83.75	-12.33	444.07	444.07
A	636+93.75	-12.33	444.12	444.13
B	637+03.75	-12.33	444.17	444.18
C	637+13.75	-12.33	444.21	444.23
D	637+23.75	-12.33	444.26	444.26
E	637+33.75	-12.33	444.29	444.29
F	637+43.75	-12.33	444.33	444.32
☉ Brg. Pier 1	637+52.50	-12.33	444.35	444.35
G	637+62.50	-12.33	444.37	444.40
H	637+72.50	-12.33	444.39	444.46
I	637+82.50	-12.33	444.40	444.51
J	637+92.50	-12.33	444.41	444.55
K	638+02.50	-12.33	444.42	444.56
L	638+12.50	-12.33	444.41	444.55
M	638+22.50	-12.33	444.41	444.52
N	638+32.50	-12.33	444.39	444.47
O	638+42.50	-12.33	444.38	444.41
☉ Brg. Pier 2	638+54.50	-12.33	444.35	444.35
P	638+64.50	-12.33	444.32	444.32
Q	638+74.50	-12.33	444.29	444.29
R	638+84.50	-12.33	444.25	444.26
S	638+94.50	-12.33	444.21	444.22
T	639+04.50	-12.33	444.16	444.18
U	639+14.50	-12.33	444.11	444.12
☉ Brg. E Abutment	639+23.25	-12.33	444.07	444.07
Bk. E. Abutment	639+24.50	-12.33	444.06	444.06

BEAM 3

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abutment	636+82.50	-6.17	444.16	444.16
☉ Brg. W. Abutment	636+83.75	-6.17	444.16	444.16
A	636+93.75	-6.17	444.21	444.22
B	637+03.75	-6.17	444.26	444.28
C	637+13.75	-6.17	444.31	444.33
D	637+23.75	-6.17	444.35	444.36
E	637+33.75	-6.17	444.39	444.39
F	637+43.75	-6.17	444.42	444.42
☉ Brg. Pier 1	637+52.50	-6.17	444.45	444.45
G	637+62.50	-6.17	444.47	444.50
H	637+72.50	-6.17	444.49	444.56
I	637+82.50	-6.17	444.50	444.61
J	637+92.50	-6.17	444.51	444.65
K	638+02.50	-6.17	444.51	444.66
L	638+12.50	-6.17	444.51	444.65
M	638+22.50	-6.17	444.50	444.62
N	638+32.50	-6.17	444.49	444.57
O	638+42.50	-6.17	444.48	444.51
☉ Brg. Pier 2	638+54.50	-6.17	444.45	444.45
P	638+64.50	-6.17	444.42	444.41
Q	638+74.50	-6.17	444.39	444.39
R	638+84.50	-6.17	444.35	444.36
S	638+94.50	-6.17	444.31	444.32
T	639+04.50	-6.17	444.26	444.27
U	639+14.50	-6.17	444.21	444.22
☉ Brg. E Abutment	639+23.25	-6.17	444.16	444.16
Bk. E. Abutment	639+24.50	-6.17	444.16	444.16

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Notes:

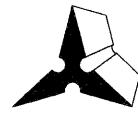
Elevations are at top of concrete.

Work this sheet with sheet 5, 7, & 8 of 28.

All stations and offsets are given with respect to centerline of the profile grade.

**TOP OF SLAB ELEVATIONS
ILLINOIS ROUTE 3 OVER
PIASA CREEK
F.A.U. ROUTE 8956 SEC 59BR-1
MADISON COUNTY
STATION 638+11.00
STRUCTURE NO. 060-0343**

DESIGNED	DB
CHECKED	RS
DRAWN	ER
CHECKED	WWH



Juneau Associates, Inc. P.C.
CONSULTING ENGINEERS AND LAND SURVEYORS
2100 State Street
Granite City, Illinois 62040
Phone: (618) 977-1400
Fax: (618) 652-5541
100 North Research Drive
Edwardsville, Illinois 62025
Phone: (618) 659-0900 / (618) 659-0800
Fax: (618) 659-0941
Registered Professional Service Corporation License No. 184-003389

SHEET NO. 6	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
28 SHEETS	8956	59BR-1	MADISON	75	35
STRUCTURE NO. 060-0343			CONTRACT NO. 76B18		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE CONSTRUCTION LINE

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abutment	636+82.50	-3.50	444.20	444.20
☉ Brg. W. Abutment	636+83.75	-3.50	444.21	444.21
A	636+93.75	-3.50	444.26	444.27
B	637+03.75	-3.50	444.31	444.32
C	637+13.75	-3.50	444.35	444.37
D	637+23.75	-3.50	444.40	444.40
E	637+33.75	-3.50	444.43	444.43
F	637+43.75	-3.50	444.47	444.46
☉ Brg. Pier 1	637+52.50	-3.50	444.49	444.49
G	637+62.50	-3.50	444.51	444.54
H	637+72.50	-3.50	444.53	444.60
I	637+82.50	-3.50	444.54	444.65
J	637+92.50	-3.50	444.55	444.69
K	638+02.50	-3.50	444.55	444.70
L	638+12.50	-3.50	444.55	444.69
M	638+22.50	-3.50	444.55	444.66
N	638+32.50	-3.50	444.53	444.61
O	638+42.50	-3.50	444.52	444.55
☉ Brg. Pier 2	638+54.50	-3.50	444.49	444.49
P	638+64.50	-3.50	444.46	444.46
Q	638+74.50	-3.50	444.43	444.43
R	638+84.50	-3.50	444.39	444.40
S	638+94.50	-3.50	444.35	444.36
T	639+04.50	-3.50	444.30	444.32
U	639+14.50	-3.50	444.25	444.26
☉ Brg. E. Abutment	639+23.25	-3.50	444.21	444.21
Bk. E. Abutment	639+24.50	-3.50	444.20	444.20

☉ ROADWAY, P.G. & BEAM 4

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abutment	636+82.50	0.00	444.25	444.25
☉ Brg. W. Abutment	636+83.75	0.00	444.26	444.26
A	636+93.75	0.00	444.31	444.32
B	637+03.75	0.00	444.36	444.38
C	637+13.75	0.00	444.41	444.42
D	637+23.75	0.00	444.45	444.46
E	637+33.75	0.00	444.49	444.49
F	637+43.75	0.00	444.52	444.51
☉ Brg. Pier 1	637+52.50	0.00	444.54	444.54
G	637+62.50	0.00	444.57	444.59
H	637+72.50	0.00	444.59	444.65
I	637+82.50	0.00	444.60	444.71
J	637+92.50	0.00	444.61	444.74
K	638+02.50	0.00	444.61	444.76
L	638+12.50	0.00	444.61	444.75
M	638+22.50	0.00	444.60	444.71
N	638+32.50	0.00	444.59	444.66
O	638+42.50	0.00	444.57	444.61
☉ Brg. Pier 2	638+54.50	0.00	444.54	444.54
P	638+64.50	0.00	444.52	444.51
Q	638+74.50	0.00	444.48	444.48
R	638+84.50	0.00	444.45	444.45
S	638+94.50	0.00	444.40	444.42
T	639+04.50	0.00	444.35	444.37
U	639+14.50	0.00	444.30	444.31
☉ Brg. E. Abutment	639+23.25	0.00	444.26	444.26
Bk. E. Abutment	639+24.50	0.00	444.25	444.25

Notes:

Elevations are at top of concrete.

Work this sheet with sheet 5, 6, & 8 of 28.

All stations and offsets are given with respect to centerline of the profile grade.

TOP OF SLAB ELEVATIONS
ILLINOIS ROUTE 3 OVER
PIASA CREEK
F.A.U. ROUTE 8956 SEC 59BR-1
MADISON COUNTY
STATION 638+11.00
STRUCTURE NO. 060-0343

DESIGNED	DB
CHECKED	RS
DRAWN	ER
CHECKED	WWH



Juneau Associates, Inc. P.C.
CONSULTING ENGINEERS AND LAND SURVEYORS
2100 State Street
Granite City, Illinois 62040
Phone: (618) 977-1400
Fax: (618) 452-9541
100 North Research Drive
Edwardsville, Illinois 62025
Phone: (618) 659-0900 / (618) 659-0800
Fax: (618) 659-0941
Registered Professional Service Corporation License No. 184-003389

SHEET NO. 7	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
28 SHEETS	8956	59BR-1	MADISON	75	36
STRUCTURE NO. 060-0343			CONTRACT NO. 76B18		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BEAM 5

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abutment	636+82.50	6.17	444.16	444.16
☉ Brg. W. Abutment	636+83.75	6.17	444.16	444.16
A	636+93.75	6.17	444.21	444.22
B	637+03.75	6.17	444.26	444.28
C	637+13.75	6.17	444.31	444.33
D	637+23.75	6.17	444.35	444.36
E	637+33.75	6.17	444.39	444.39
F	637+43.75	6.17	444.42	444.42
☉ Brg. Pier 1	637+52.50	6.17	444.45	444.45
G	637+62.50	6.17	444.47	444.50
H	637+72.50	6.17	444.49	444.56
I	637+82.50	6.17	444.50	444.61
J	637+92.50	6.17	444.51	444.65
K	638+02.50	6.17	444.51	444.66
L	638+12.50	6.17	444.51	444.65
M	638+22.50	6.17	444.50	444.62
N	638+32.50	6.17	444.49	444.57
O	638+42.50	6.17	444.48	444.51
☉ Brg. Pier 2	638+54.50	6.17	444.45	444.45
P	638+64.50	6.17	444.42	444.41
Q	638+74.50	6.17	444.39	444.39
R	638+84.50	6.17	444.35	444.36
S	638+94.50	6.17	444.31	444.32
T	639+04.50	6.17	444.26	444.27
U	639+14.50	6.17	444.21	444.22
☉ Brg. E Abutment	639+23.25	6.17	444.16	444.16
Bk. E. Abutment	639+24.50	6.17	444.16	444.16

BEAM 6

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abutment	636+82.50	12.33	444.06	444.06
☉ Brg. W. Abutment	636+83.75	12.33	444.07	444.07
A	636+93.75	12.33	444.12	444.13
B	637+03.75	12.33	444.17	444.18
C	637+13.75	12.33	444.21	444.23
D	637+23.75	12.33	444.26	444.26
E	637+33.75	12.33	444.29	444.29
F	637+43.75	12.33	444.33	444.32
☉ Brg. Pier 1	637+52.50	12.33	444.35	444.35
G	637+62.50	12.33	444.37	444.40
H	637+72.50	12.33	444.39	444.46
I	637+82.50	12.33	444.40	444.51
J	637+92.50	12.33	444.41	444.55
K	638+02.50	12.33	444.42	444.56
L	638+12.50	12.33	444.41	444.55
M	638+22.50	12.33	444.41	444.52
N	638+32.50	12.33	444.39	444.47
O	638+42.50	12.33	444.38	444.41
☉ Brg. Pier 2	638+54.50	12.33	444.35	444.35
P	638+64.50	12.33	444.32	444.32
Q	638+74.50	12.33	444.29	444.29
R	638+84.50	12.33	444.25	444.26
S	638+94.50	12.33	444.21	444.22
T	639+04.50	12.33	444.16	444.18
U	639+14.50	12.33	444.11	444.12
☉ Brg. E Abutment	639+23.25	12.33	444.07	444.07
Bk. E. Abutment	639+24.50	12.33	444.06	444.06

BEAM 7

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abutment	636+82.50	18.50	443.93	443.93
☉ Brg. W. Abutment	636+83.75	18.50	443.94	443.94
A	636+93.75	18.50	443.99	444.00
B	637+03.75	18.50	444.04	444.05
C	637+13.75	18.50	444.09	444.10
D	637+23.75	18.50	444.13	444.14
E	637+33.75	18.50	444.17	444.16
F	637+43.75	18.50	444.20	444.19
☉ Brg. Pier 1	637+52.50	18.50	444.22	444.22
G	637+62.50	18.50	444.25	444.27
H	637+72.50	18.50	444.26	444.33
I	637+82.50	18.50	444.28	444.38
J	637+92.50	18.50	444.28	444.42
K	638+02.50	18.50	444.29	444.44
L	638+12.50	18.50	444.29	444.43
M	638+22.50	18.50	444.28	444.39
N	638+32.50	18.50	444.27	444.34
O	638+42.50	18.50	444.25	444.28
☉ Brg. Pier 2	638+54.50	18.50	444.22	444.22
P	638+64.50	18.50	444.19	444.19
Q	638+74.50	18.50	444.16	444.16
R	638+84.50	18.50	444.12	444.13
S	638+94.50	18.50	444.08	444.09
T	639+04.50	18.50	444.03	444.05
U	639+14.50	18.50	443.98	443.99
☉ Brg. E Abutment	639+23.25	18.50	443.94	443.94
Bk. E. Abutment	639+24.50	18.50	443.93	443.93

**TOP OF SLAB ELEVATIONS
ILLINOIS ROUTE 3 OVER
PIASA CREEK
F.A.U. ROUTE 8956 SEC 59BR-1
MADISON COUNTY
STATION 638+11.00
STRUCTURE NO. 060-0343**

Notes:

Elevations are at top of concrete.

Work this sheet with sheet 5, 6, & 7 of 28.

All stations and offsets are given with respect
to centerline of the profile grade.

DESIGNED	DB
CHECKED	RS
DRAWN	ER
CHECKED	WWH



Juneau Associates, Inc. P.C.
CONSULTING ENGINEERS AND LAND SURVEYORS
2100 State Street
Galesburg, Illinois 62040
Phone: (618) 977-1400
Fax: (618) 452-3541
100 North Research Drive
Edwardsville, Illinois 62025
Phone: (618) 659-0900 / (618) 659-0800
Fax: (618) 659-0941
Registered Professional Service Corporation License No. 184-003389

SHEET NO. 8	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
28 SHEETS	8956	59BR-1	MADISON	75	37
STRUCTURE NO. 060-0343		CONTRACT NO. 76B18			
FED. ROAD DIST. NO.		ILLINOIS		FED. AID PROJECT	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NORTH EDGE OF SHOULDER

Location	Station	Offset	Theoretical Grade Elevations
West End of West Approach Pavement	636+52.50	-20.00	443.75
A1	636+62.50	-20.00	443.80
A2	636+72.50	-20.00	443.85
East End of West Approach Pavement	636+82.50	-20.00	443.90

NORTH EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
West End of West Approach Pavement	636+52.50	-12.00	443.92
A1	636+62.50	-12.00	443.97
A2	636+72.50	-12.00	444.02
East End of West Approach Pavement	636+82.50	-12.00	444.07

STAGE CONSTRUCTION JOINT

Location	Station	Offset	Theoretical Grade Elevations
West End of West Approach Pavement	636+52.50	-3.50	444.05
A1	636+62.50	-3.50	444.10
A2	636+72.50	-3.50	444.15
East End of West Approach Pavement	636+82.50	-3.50	444.20

☉ ROADWAY & PROFILE GRADE

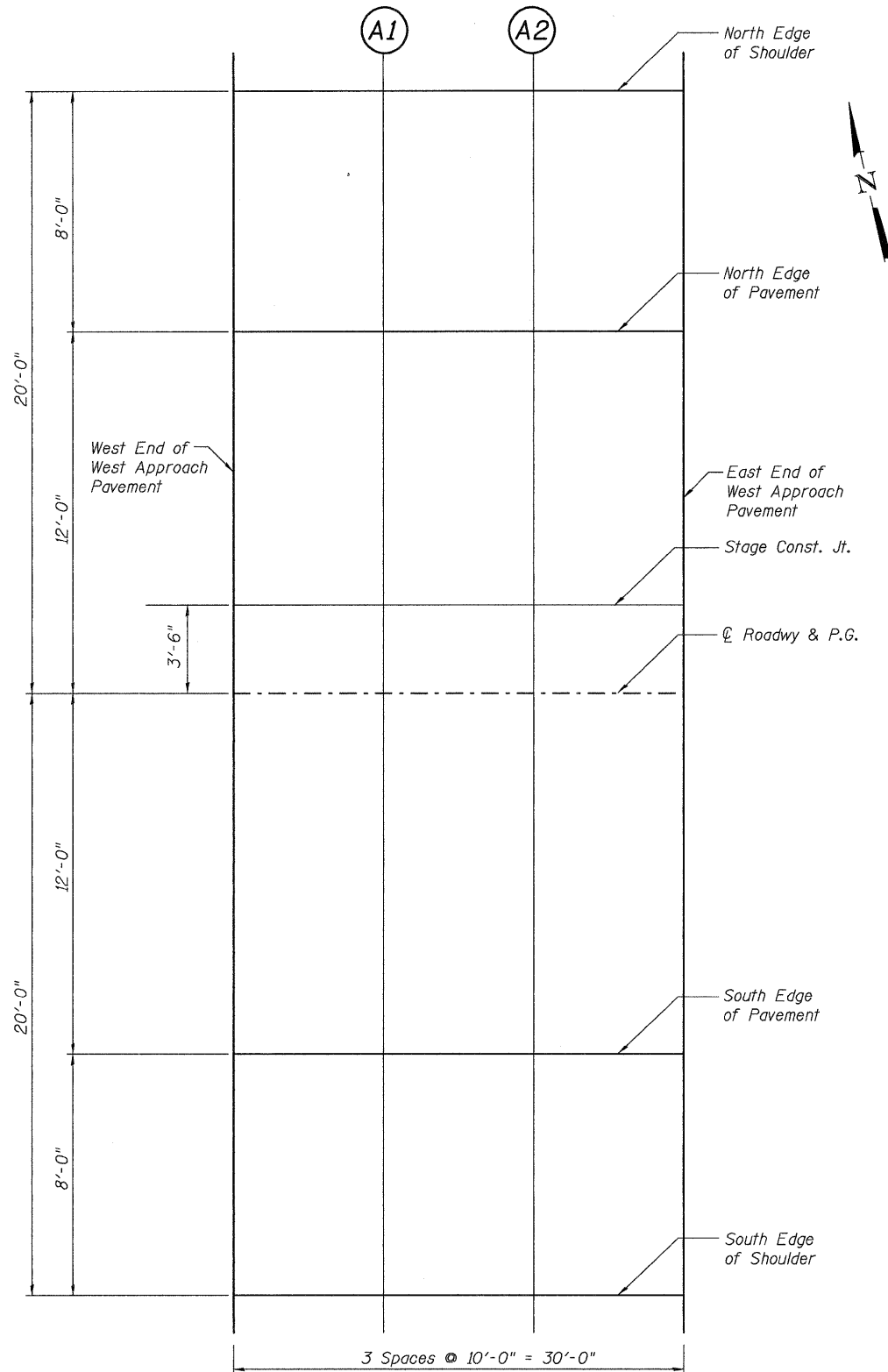
Location	Station	Offset	Theoretical Grade Elevations
West End of West Approach Pavement	636+52.50	0.00	444.11
A1	636+62.50	0.00	444.16
A2	636+72.50	0.00	444.21
East End of West Approach Pavement	636+82.50	0.00	444.26

SOUTH EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
West End of West Approach Pavement	636+52.50	-12.00	443.92
A1	636+62.50	-12.00	443.97
A2	636+72.50	-12.00	444.02
East End of West Approach Pavement	636+82.50	-12.00	444.07

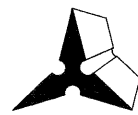
SOUTH EDGE OF SHOULDER

Location	Station	Offset	Theoretical Grade Elevations
West End of West Approach Pavement	636+52.50	-20.00	443.75
A1	636+62.50	-20.00	443.80
A2	636+72.50	-20.00	443.85
East End of West Approach Pavement	636+82.50	-20.00	443.90



PLAN
West Approach

DESIGNED	DB
CHECKED	RS
DRAWN	ER
CHECKED	WWH



Juneau Associates, Inc. P.C.
CONSULTING ENGINEERS AND LAND SURVEYORS
2100 State Street
Grafton City, Illinois 62040
Phone: (618) 877-1400
Fax: (618) 452-5541
100 North Research Drive
Edwardsville, Illinois 62025
Phone: (618) 659-0800 / (618) 659-0800
Fax: (618) 659-0941
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Notes:

Elevations are at top of concrete.

All stations and offsets are given with respect to centerline of profile grade.

**TOP OF APPROACH
SLAB ELEVATIONS
ILLINOIS ROUTE 3 OVER
PIASA CREEK
F.A.U. ROUTE 8956 SEC 59BR-1
MADISON COUNTY
STATION 638+11.00
STRUCTURE NO. 060-0343**

SHEET NO. 9 28 SHEETS	F.A.U. RTE. 8956	SECTION 59BR-1	COUNTY MADISON	TOTAL SHEETS 75	SHEET NO. 38
	STRUCTURE NO. 060-0343		CONTRACT NO. 76B18		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NORTH EDGE OF SHOULDER

Location	Station	Offset	Theoretical Grade Elevations
West End of East Approach Pavement	639+24.50	-20.00	443.90
A3	639+34.50	-20.00	443.85
A4	639+44.50	-20.00	443.80
East End of East Approach Pavement	639+54.50	-20.00	443.75

NORTH EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
West End of East Approach Pavement	639+24.50	-12.00	444.07
A3	639+34.50	-12.00	444.02
A4	639+44.50	-12.00	443.97
East End of East Approach Pavement	639+54.50	-12.00	443.92

STAGE CONSTRUCTION JOINT

Location	Station	Offset	Theoretical Grade Elevations
West End of East Approach Pavement	639+24.50	-3.50	444.20
A3	639+34.50	-3.50	444.15
A4	639+44.50	-3.50	444.10
East End of East Approach Pavement	639+54.50	-3.50	444.05

☉ ROADWAY & PROFILE GRADE

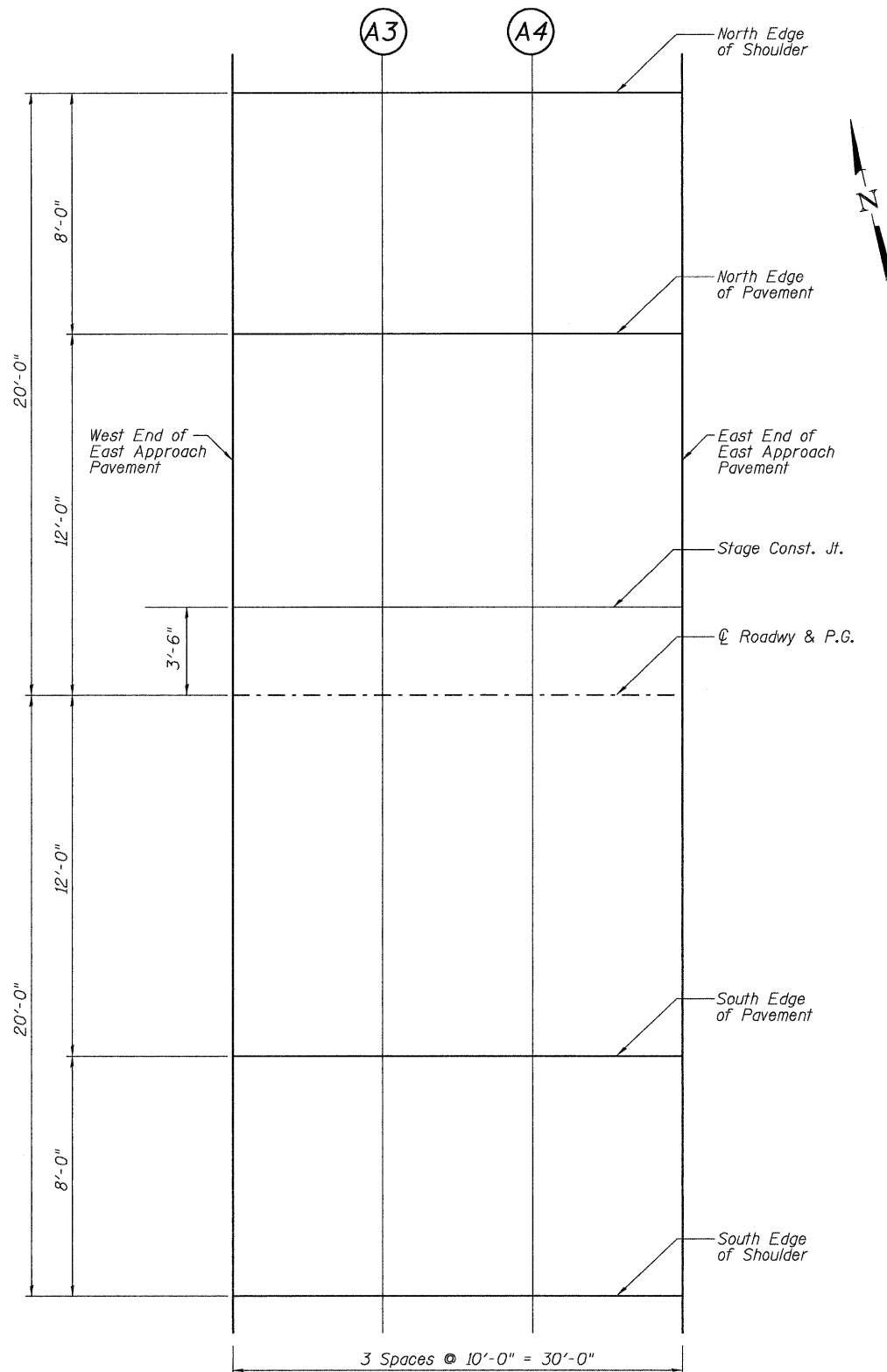
Location	Station	Offset	Theoretical Grade Elevations
West End of East Approach Pavement	639+24.50	0.00	444.26
A3	639+34.50	0.00	444.21
A4	639+44.50	0.00	444.16
East End of East Approach Pavement	639+54.50	0.00	444.11

SOUTH EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
West End of East Approach Pavement	639+24.50	-12.00	444.07
A3	639+34.50	-12.00	444.02
A4	639+44.50	-12.00	443.97
East End of East Approach Pavement	639+54.50	-12.00	443.92

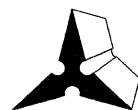
SOUTH EDGE OF SHOULDER

Location	Station	Offset	Theoretical Grade Elevations
West End of East Approach Pavement	639+24.50	-20.00	443.90
A3	639+34.50	-20.00	443.85
A4	639+44.50	-20.00	443.80
East End of East Approach Pavement	639+54.50	-20.00	443.75



PLAN
East Approach

DESIGNED	DB
CHECKED	RS
DRAWN	ER
CHECKED	WWH



Juneau Associates, Inc. P.C.
CONSULTING ENGINEERS AND LAND SURVEYORS
2130 State Street
Granite City, Illinois 62040
Phone: (618) 977-1400
Fax: (618) 452-5941
Registered Professional Service Corporation License No. 184-003389

100 North Research Drive
Edwardsville, Illinois 62025
Phone: (618) 659-0900/(618) 659-0800
Fax: (618) 659-0941

Notes:

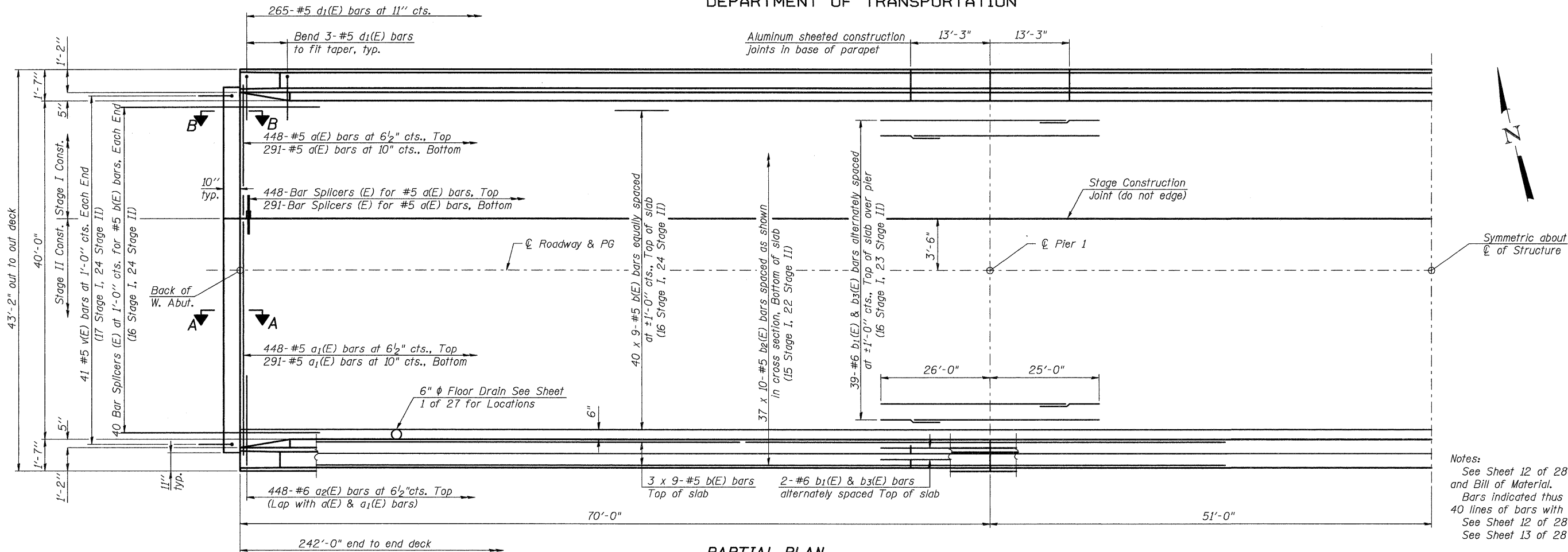
Elevations are at top of concrete.

All stations and offsets are given with respect to centerline of profile grade.

TOP OF APPROACH
SLAB ELEVATIONS
ILLINOIS ROUTE 3 OVER
PIASA CREEK
F.A.U. ROUTE 8956 SEC 59BR-1
MADISON COUNTY
STATION 638+11.00
STRUCTURE NO. 060-0343

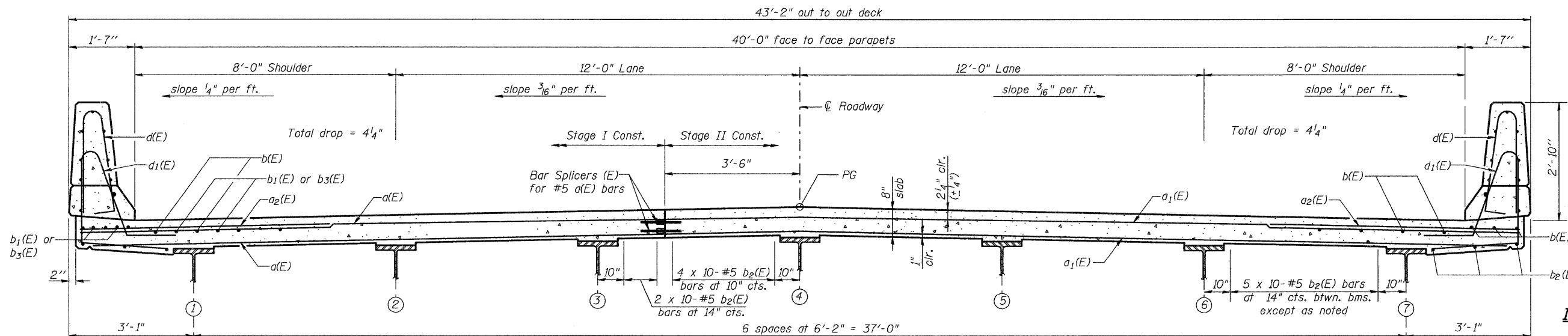
SHEET NO. 10 28 SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	8956	59BR-1	MADISON	75	39
STRUCTURE NO. 060-0343		CONTRACT NO. 76B18			
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



Notes:
See Sheet 12 of 28 for superstructure details and Bill of Material.
Bars indicated thus 40 x 6-#5 etc. indicates 40 lines of bars with 6 lengths per line.
See Sheet 12 of 28 for parapet reinforcement.
See Sheet 13 of 28 for Sec. A-A and Sec. B-B.

PARTIAL PLAN



MIN. BAR LAP
#5 bars = 1'-8"
#6 bars = 2'-0"

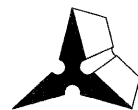
NEAR PIER

CROSS SECTION
(Looking East)

NEAR MIDSPAN

SUPERSTRUCTURE
ILLINOIS ROUTE 3 OVER
PIASA CREEK
F.A.U. ROUTE 8956 SEC 59BR-1
MADISON COUNTY
STATION 638+11.00
STRUCTURE NO. 060-0343

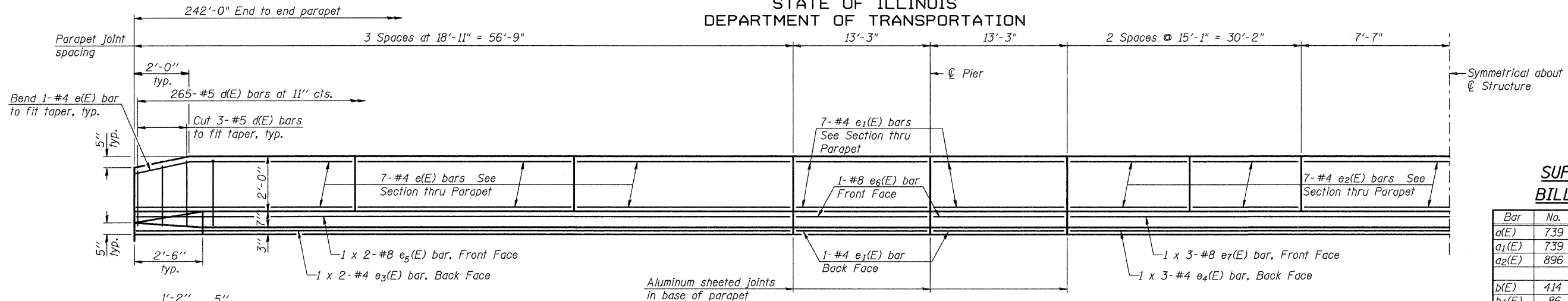
DESIGNED	DB
CHECKED	RS
DRAWN	ER
CHECKED	WWH



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100 North Research Drive
Edwardsville, Illinois 62025
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SHEET NO. 11	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	8956	59BR-1	MADISON	75	40
28 SHEETS	STRUCTURE NO. 060-0343		CONTRACT NO. 76B18		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

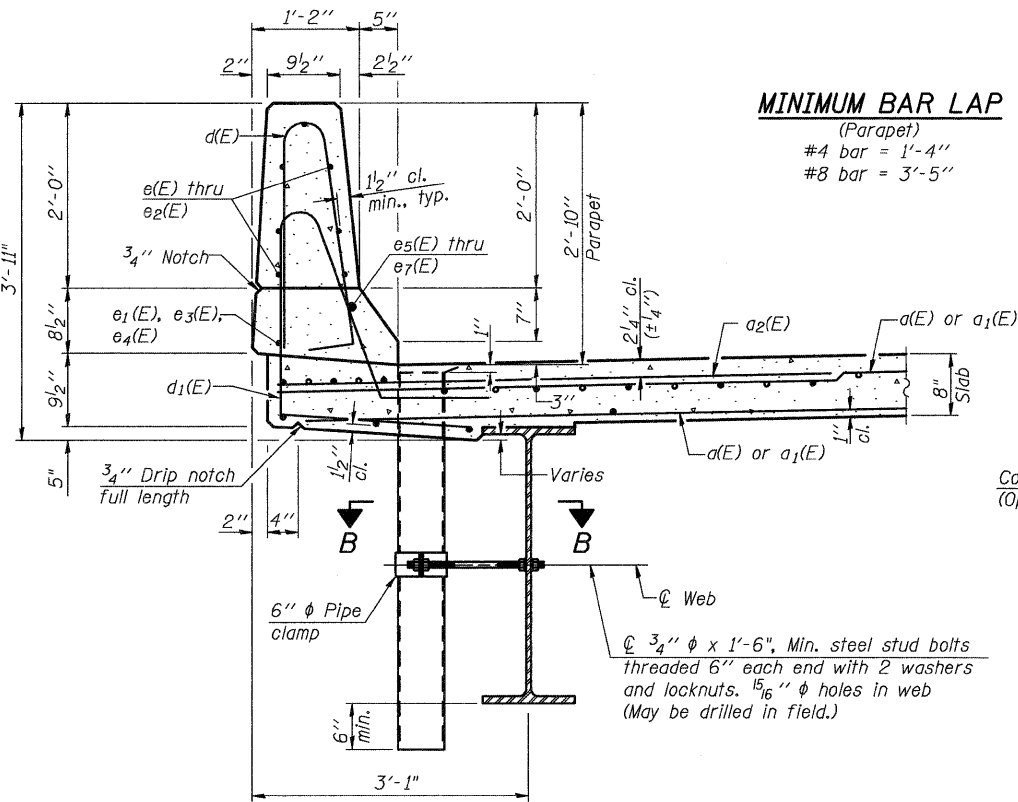


INSIDE ELEVATION OF PARAPET

**SUPERSTRUCTURE
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
d(E)	739	#5	17'-3"	—
a1(E)	739	#5	24'-3"	—
a2(E)	896	#6	6'-0"	—
b(E)	414	#5	28'-6"	—
b1(E)	86	#6	36'-0"	—
b2(E)	370	#5	25'-9"	—
b3(E)	86	#6	17'-0"	—
d(E)	530	#5	5'-7"	⌒
d1(E)	530	#5	7'-10"	⌒
e(E)	84	#4	18'-7"	—
e1(E)	64	#4	13'-0"	—
e2(E)	70	#4	14'-10"	—
e3(E)	8	#4	28'-11"	—
e4(E)	6	#4	26'-0"	—
e5(E)	8	#8	30'-0"	—
e6(E)	8	#8	13'-0"	—
e7(E)	6	#8	27'-4"	—
m(E)	4	#6	24'-0"	—
m1(E)	6	#6	24'-10"	—
m2(E)	20	#6	9'-0"	—
m3(E)	10	#6	5'-10"	—
m4(E)	4	#6	2'-10"	—
m5(E)	4	#6	17'-0"	—
m6(E)	6	#6	17'-10"	—
m7(E)	4	#6	8'-0"	—
m8(E)	4	#6	7'-0"	—
m9(E)	2	#6	3'-3"	—
m10(E)	2	#6	2'-5"	—
s(E)	96	#5	6'-9"	⌒
s1(E)	84	#4	10'-0"	⌒
v(E)	82	#5	3'-4"	⌒
Reinforcement Bars, Epoxy Coated		Pound	83110	
Concrete Superstructure		Cu. Yds.	351.8	
Bar Splicers		Each	835	

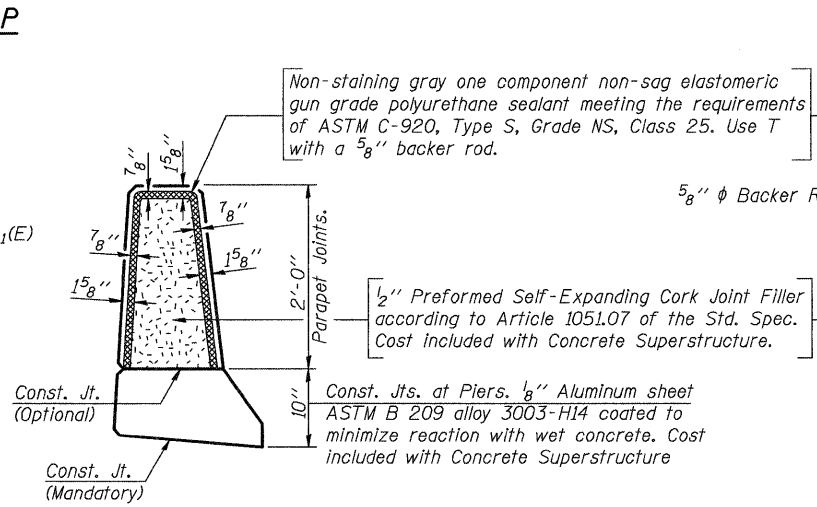
Bars indicated thus 1 x 2-#5 etc. Indicates 1 line of bars with 2 lengths per line.



SECTION THRU PARAPET

MINIMUM BAR LAP

(Parapet)
#4 bar = 1'-4"
#8 bar = 3'-5"



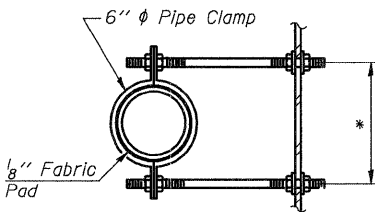
PARAPET JOINT DETAILS

Notes:

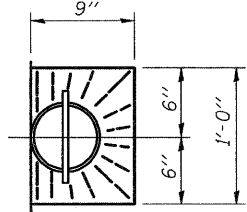
Drains shall be located clear of all diaphragms.

Floor drains need not be painted.

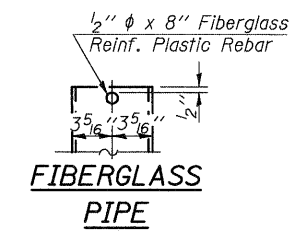
Fiberglass pipe shall conform to ASTM D 2996, with short-time rupture strength hoop tensile stress of 30,000 p.s.i. minimum.



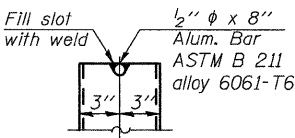
SECTION B-B
*Dimension as required by Pipe Clamp



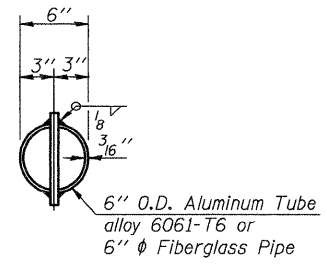
TOP PLAN



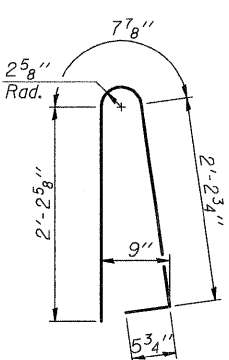
FIBERGLASS PIPE



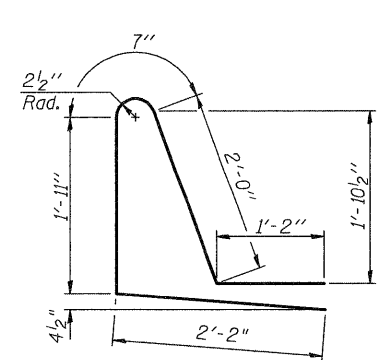
ALUMINUM TUBE



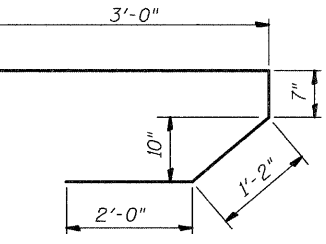
TOP PLAN
(Showing Aluminum Tube)



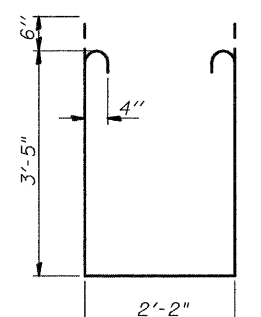
BAR d(E)



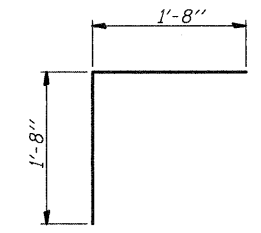
BAR d1(E)



BAR s(E)



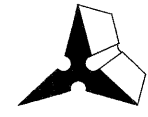
BAR s1(E)



BAR v(E)

**SUPERSTRUCTURE DETAILS
ILLINOIS ROUTE 3 OVER
PIASA CREEK
F.A.U. ROUTE 8956 SEC 59BR-1
MADISON COUNTY
STATION 638+11.00
STRUCTURE NO. 060-0343**

DESIGNED	DB
CHECKED	RS
DRAWN	ER
CHECKED	WWH

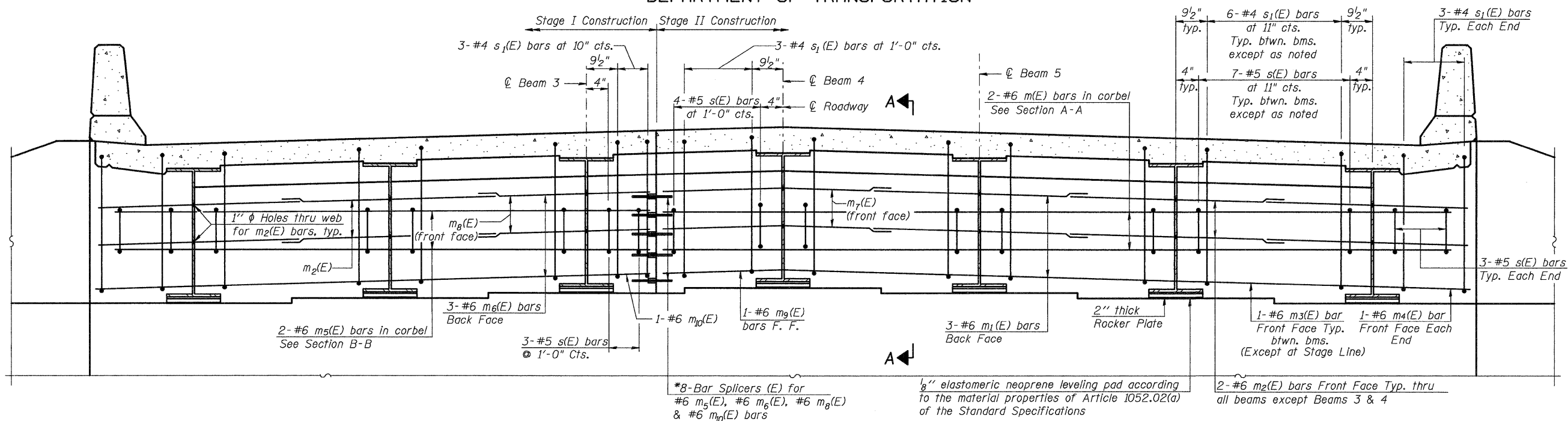


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100 North Research Drive
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Phone: (618) 659-0800 / (618) 659-0800
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SHEET NO. 12 28 SHEETS	F.A.U. RTE. 8956	SECTION 59BR-1	COUNTY MADISON	TOTAL SHEETS 75	SHEET NO. 41
	STRUCTURE NO. 060-0343		CONTRACT NO. 76B18		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT		

244431.PWA\STR\060-0343-01.DWG with DDT Comment\02 Superstructure Details.dgn

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



DIAPHRAGM ELEVATION AT ABUTMENT

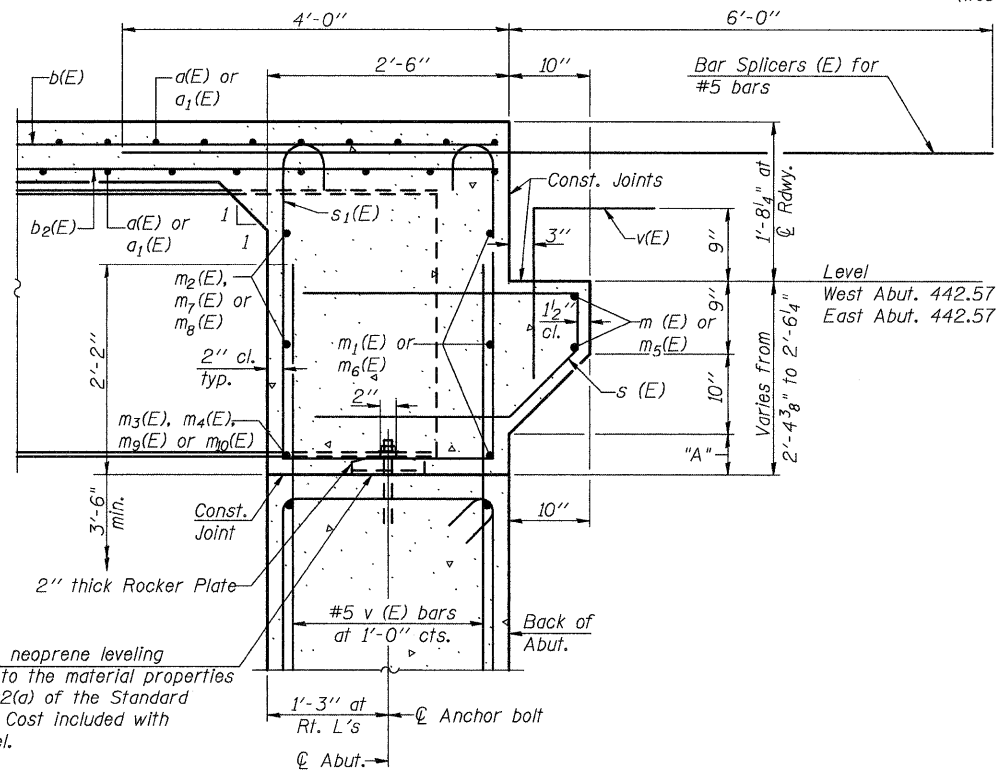
East Abutment Looking East
(West Abutment Similar, But Opposite Hand)

* Cut Bar Splicer for m₆(E) bars to fit.

Notes:
Reinforcement bars in diaphragm are billed with superstructure on sheet 12 of 28
Concrete in diaphragm is included with Concrete Superstructure on sheet 12 of 28
For details of bars s(E) & s₁(E) see sheet 12 of 28
The s(E) and s₁(E) bars shall be placed parallel to the beams. Spacing for these bars shall be at right angles to the beams.

MIN. BAR LAP

#6 bar = 2'-7"



1/8" elastomeric neoprene leveling pad according to the material properties of Art. 1052.02(a) of the Standard Specifications. Cost included with Structural Steel.

SECTION A-A

Dimensions at right angles to abutment, except as shown

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100 North Research Drive
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DIMENSION "A"

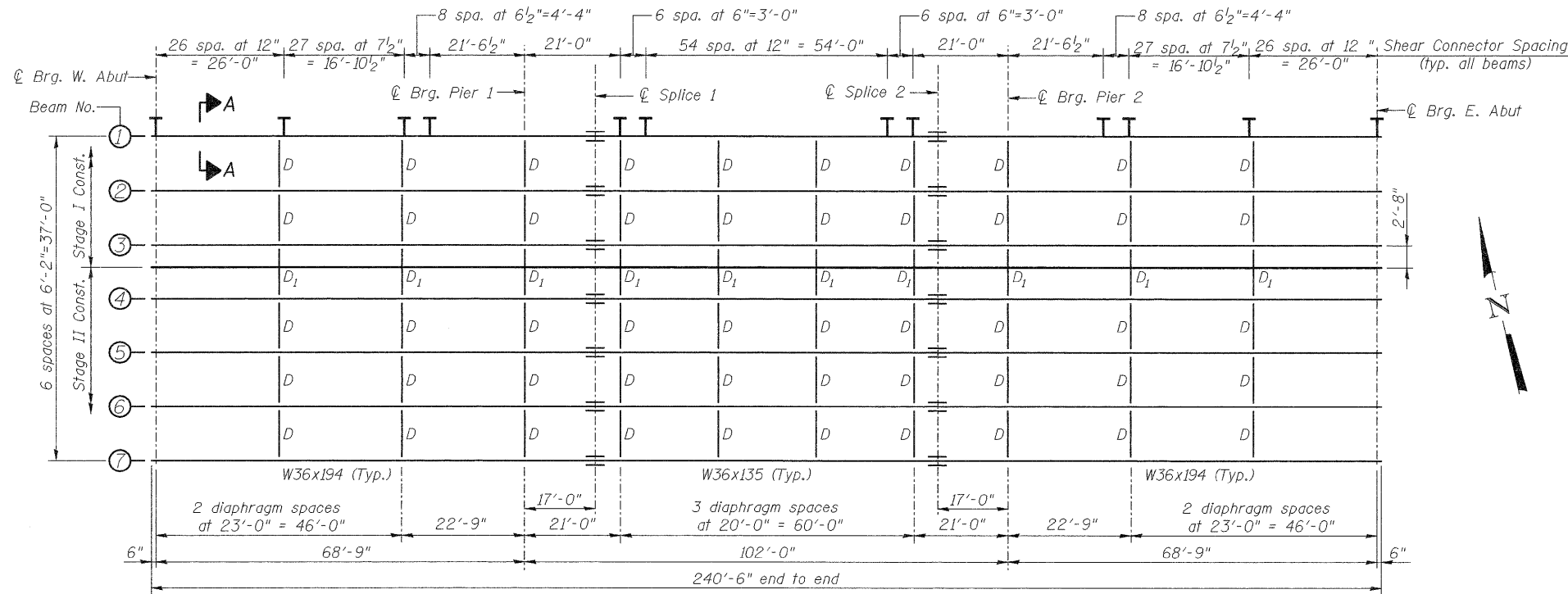
Beam	1	2	3	4	5	6	7
W. Abut	11 7/8"	10 1/4"	9 9/8"	8"	9 9/8"	10 1/4"	11 7/8"
E. Abut	11 7/8"	10 1/4"	9 9/8"	8"	9 9/8"	10 1/4"	11 7/8"

See Section A-A and Section B-B for Dimension "A" locations.

DESIGNED	DB
CHECKED	RS
DRAWN	ER
CHECKED	WWH

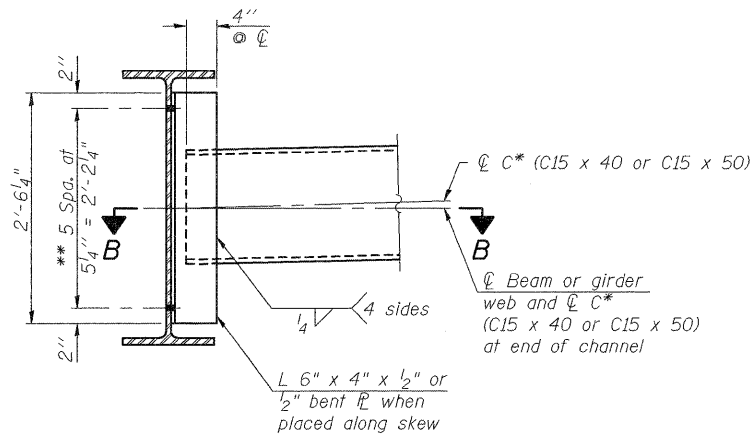
SHEET NO. 13 28 SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	8956	59BR-1	MADISON	75	42
STRUCTURE NO. 060-0343		CONTRACT NO. 76B18			
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



PLAN

All beams shall be (N.T.R.) M270 Grade 50W

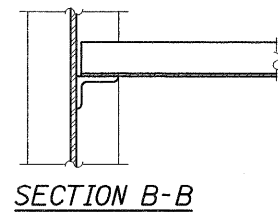


INTERIOR DIAPHRAGM D
(50 Required)

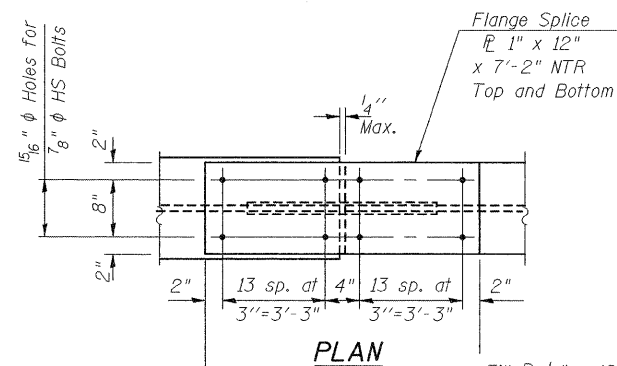
Note:
Two hardened washers required for each set of oversized holes.

* Alternate channels are permitted to facilitate material acquisition. Calculated weight of structural steel is based on the lighter section. The alternate, if utilized, shall be provided at no additional cost to the department.

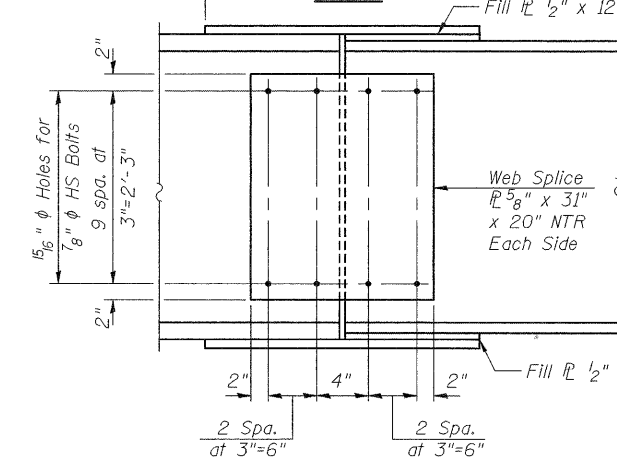
** 3/4" φ HS bolts, 1 5/16" φ holes



SECTION B-B



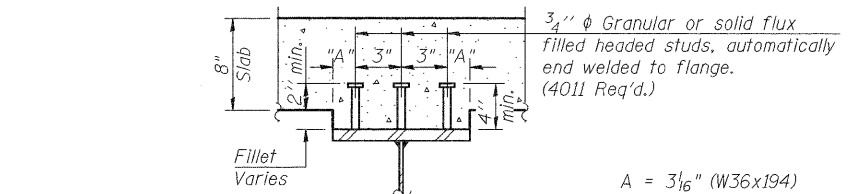
PLAN



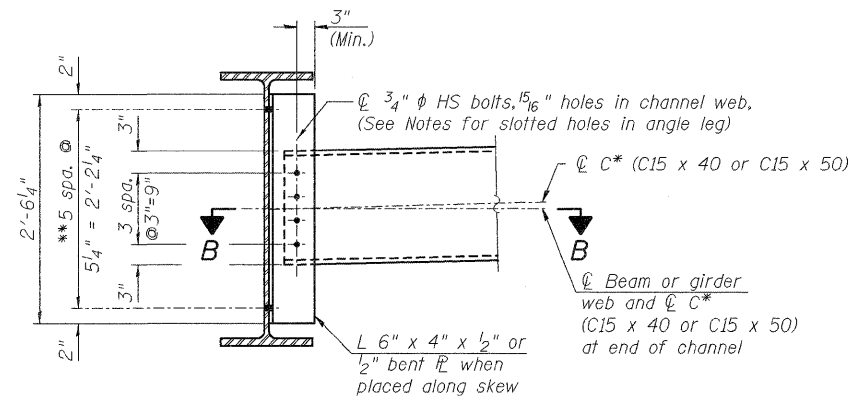
SPLICE

TYPICAL SPLICE
(14 Required)

Splice 1 Shown (Splice 2 similar, but opposite hand)



SECTION A-A



INTERIOR DIAPHRAGM D1
(10 Required)

TOP OF BEAM ELEVATIONS
(For fabrication only)

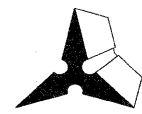
Location	Beam 1	Beam 2	Beam 3	Beam 4	Beam 5	Beam 6	Beam 7
℄ Brg. W. Abut.	443.23	443.36	443.46	443.55	443.46	443.36	443.23
℄ Brg. Pier 1	443.47	443.60	443.69	443.79	443.69	443.60	443.47
℄ Splice 1	443.53	443.65	443.75	443.85	443.75	443.65	443.53
℄ Splice 2	443.53	443.65	443.75	443.85	443.75	443.65	443.53
℄ Brg. Pier 2	443.47	443.60	443.69	443.79	443.69	443.60	443.47
℄ Brg. E. Abut.	443.23	443.36	443.46	443.55	443.46	443.36	443.23

NOTES

Load carrying components designated "NTR" shall conform to the Supplemental Requirements for Notch Toughness, Zone 2.
All cross frames or diaphragms shall be installed as steel is erected and secured with erection pins and bolts except as otherwise noted. Individual cross frames or diaphragms at supports may be temporarily disconnected to install bearing anchor rods.
Provide 1 5/16" x 1 1/2" long slotted vertical holes in outstanding leg of diaphragm connection angles of Diaphragm D1 at Beams 3 and 4. Slotted holes shall have 5/16" structural plate washers placed over them. Bolts in slots shall be finger tight until the second stage pour is complete and fully tightened after completion of the deck pour for Stage II Construction. Position slots so bolts start at one end with no concrete load and finish near the opposite end under deck load, allowing maximum displacement without laterally stressing main members.

STRUCTURAL STEEL DETAILS
ILLINOIS ROUTE 3 OVER
PIASA CREEK
F.A.U. ROUTE 8956 SEC 59BR-1
MADISON COUNTY
STATION 638+11.00
STRUCTURE NO. 060-0343

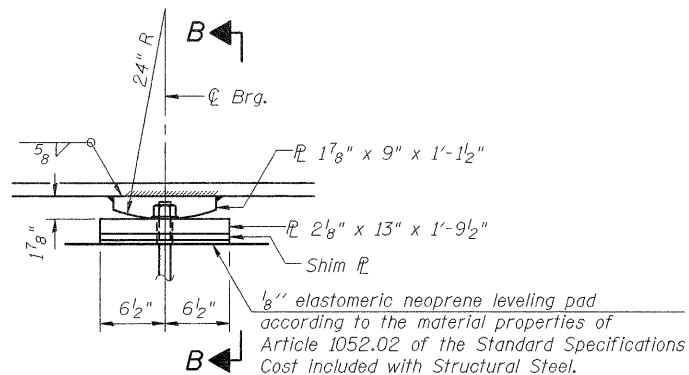
DESIGNED	DB
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CHECKED	WWH



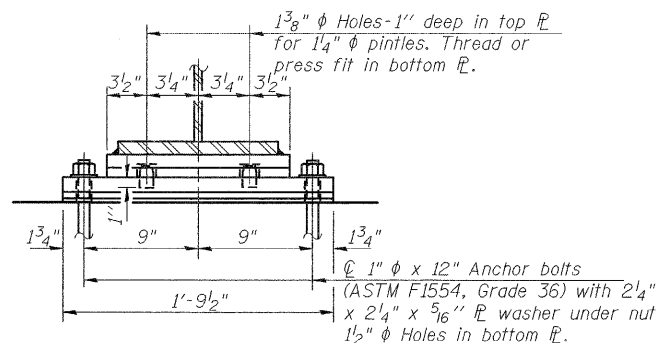
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100 North Research Drive
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SHEET NO. 14 28 SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	8956	59BR-1	MADISON	43	
STRUCTURE NO. 060-0343		CONTRACT NO. 76B18			
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

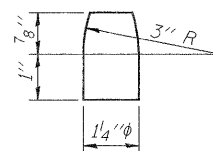
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



ELEVATION AT PIER



SECTION B-B



PINTLE

Notes:

Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.

Anchor bolts at fixed bearings may be either cast in place or installed in holes drilled after the supported member is in place.

Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.

Two 1/8 inch adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details.

FIXED BEARING

14 Required

INTERIOR GIRDER MOMENT TABLE			
		0.4 Sp. 1 or 0.6 Sp. 3	Pier 1 or 2 / 0.5 Span 2
I_s	(in ⁴)	12100	7800
$I_c(n)$	(in ⁴)	28752	20768
$I_c(3n)$	(in ⁴)	20629	15079
S_s	(in ³)	664	439
$S_c(n)$	(in ³)	935	648
$S_c(3n)$	(in ³)	834	581
DC1	(k/ft)	0.871	0.830
M _{DC1}	(k)	213.8	378.5
DC2	(k/ft)	0.128	0.128
M _{DC2}	(k)	36.6	76.6
DW	(k/ft)	0.285	0.285
M _{DW}	(k)	81.5	170.4
M _{ℓ + IM}	(k)	791.2	922.0
M _u (Strength I)	(k)	1820.0	2439.0
φ _r M _n , φ _r M _{nc}	(k)	4578	3149
f _s DC1	(ksi)	3.86	10.35
f _s DC2	(ksi)	0.47	1.42
f _s DW	(ksi)	1.17	3.52
f _s 1.3(ℓ + IM)	(ksi)	13.2	22.2
f _s (Service II)	(ksi)	18.7	37.5
V _r	(k)	25.7	21.5

I_s, S_s : Non-composite moment of inertia and section modulus of the steel section used for computing f_s (Total-Strength I, and Service II) due to non-composite dead loads (in⁴ and in³).

$I_c(n), S_c(n)$: Composite moment of inertia and section modulus of the steel and deck based upon the modular ratio, "n", used for computing f_s (Total-Strength I, and Service II) due to short-term composite live loads (in⁴ and in³).

$I_c(3n), S_c(3n)$: Composite moment of inertia and section modulus of the steel and deck based upon 3 times the modular ratio, "3n", used for computing f_s (Total-Strength I, and Service II) due to long-term composite (superimposed) dead loads (in⁴ and in³).

DC1: Un-factored non-composite dead load (kips/ft.).

M_{DC1}: Un-factored moment due to non-composite dead load (kip-ft.).

DC2: Un-factored long-term composite (superimposed excluding future wearing surface) dead load (kips/ft.).

M_{DC2}: Un-factored moment due to long-term composite (superimposed excluding future wearing surface) dead load (kip-ft.).

DW: Un-factored long-term composite (superimposed future wearing surface only) dead load (kips/ft.).

M_{DW}: Un-factored moment due to long-term composite (superimposed future wearing surface only) dead load (kip-ft.).

M_{ℓ + IM}: Un-factored live load moment plus dynamic load allowance (impact) (kip-ft.).

M_u (Strength I): Factored design moment (kip-ft.).

1.25 (M_{DC1} + M_{DC2}) + 1.5 M_{DW} + 1.75 M_{ℓ + IM}

φ_rM_n: Compact composite positive moment capacity computed according to Article 6.10.7.1 (kip-ft.).

φ_rM_{nc}: Compact non-composite negative moment capacity computed according to Article A6.1.1 (kip-ft.).

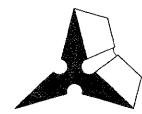
f_s (Service II): Sum of stresses as computed from the moments below (ksi).

M_{DC1} + M_{DC2} + M_{DW} + 1.3 M_{ℓ + IM}

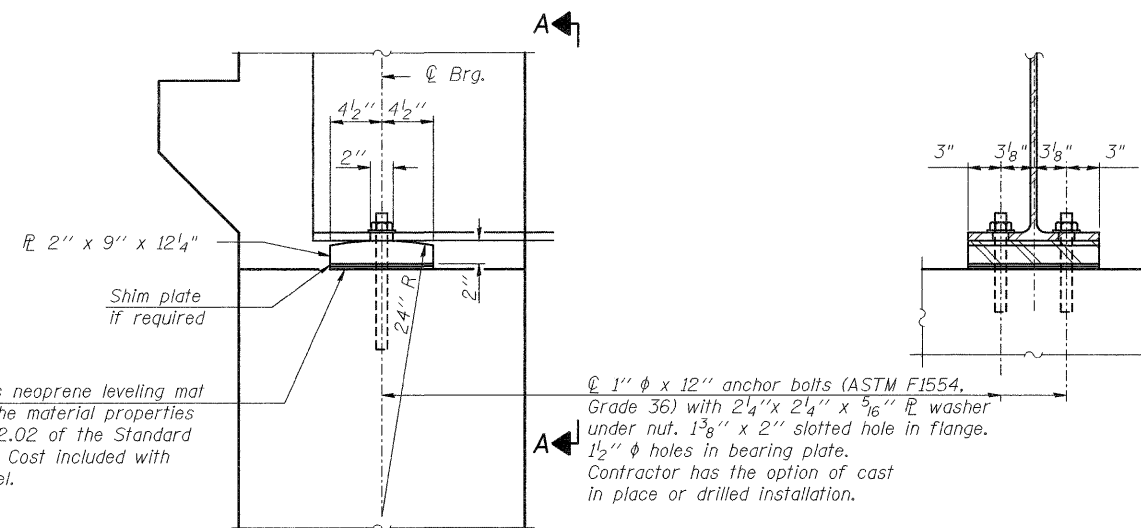
V_r: Maximum factored shear range in composite portion of span computed according to Article 6.10.10.

INTERIOR GIRDER REACTION TABLE HL93 LOADING		
	Abut.	Pier
R _{DC1}	(k) 19.7	82.4
R _{DC2}	(k) 3.1	12.2
R _{DW}	(k) 6.9	27.3
R _{ℓ + IM}	(k) 69.6	134.9
R _{Total}	(k) 99.3	256.8

DESIGNED	DB
CHECKED	RS
DRAWN	ER
CHECKED	WWH



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2100 State Street
Granite City, Illinois 62040
Phone: (618) 977-1400
Fax: (618) 452-5541
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SECTION A-A

BEARING AT INTEGRAL ABUTMENTS

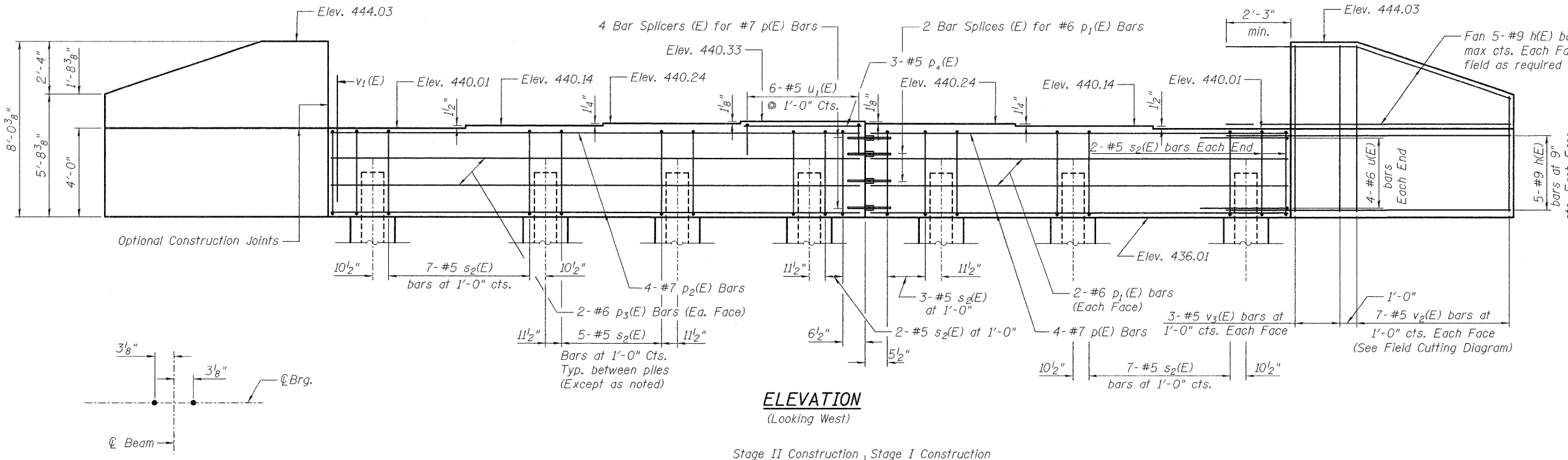
14 Required

BEARING DETAILS
ILLINOIS ROUTE 3 OVER
PIASA CREEK
F.A.U. ROUTE 8956 SEC 59BR-1
MADISON COUNTY
STATION 638+11.00
STRUCTURE NO. 060-0343

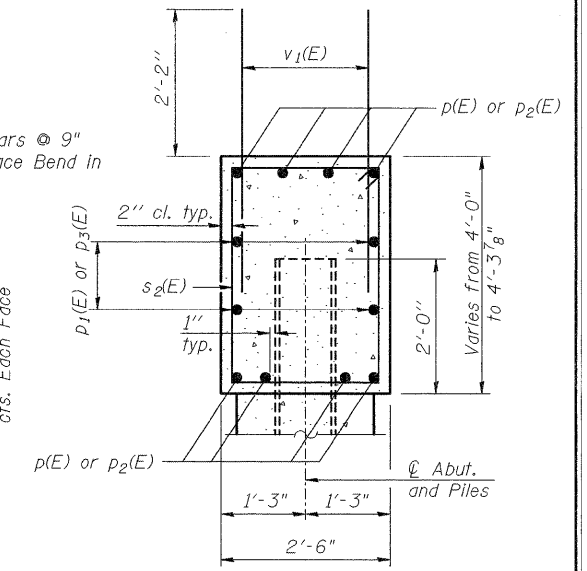
SHEET NO. 15 28 SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	8956	59BR-1	MADISON		44
STRUCTURE NO. 060-0343			CONTRACT NO. 76B18		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT		

Notes: Four steps monolithically with cap.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



ELEVATION
(Looking West)



SECTION THRU ABUT.

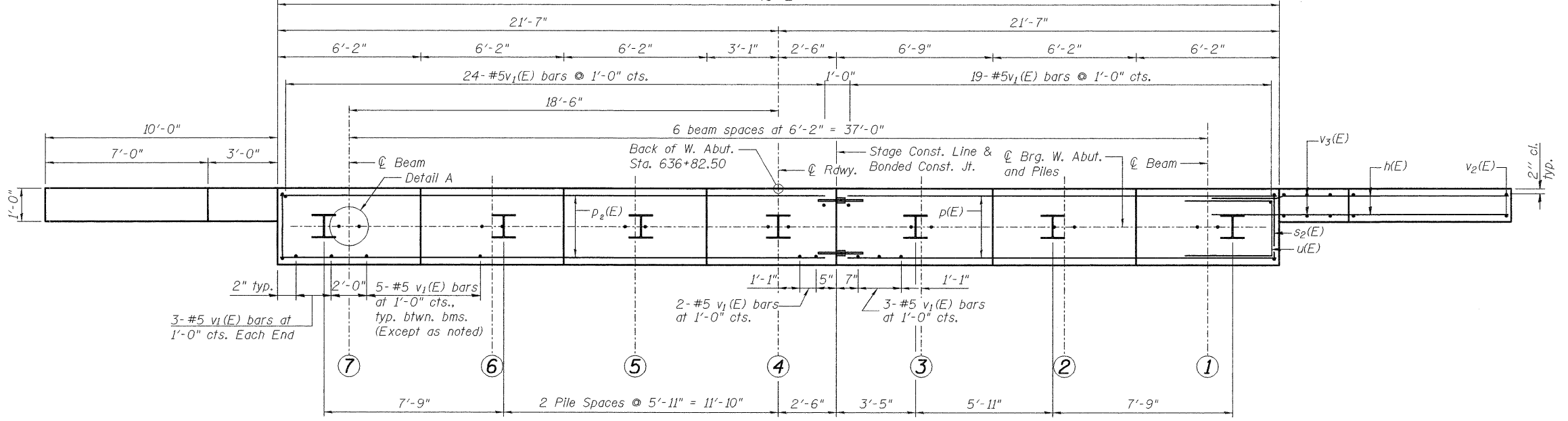
**WEST ABUTMENT
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
h(E)	40	#9	12'-6"	—
p (E)	8	#7	18'-10"	—
p1 (E)	4	#6	18'-10"	—
p2 (E)	8	#7	23'-10"	—
p3 (E)	4	#6	23'-10"	—
p4 (E)	3	#5	5'-4"	—
s2 (E)	38	#5	12'-7"	□
u (E)	8	#6	8'-1"	—
u1 (E)	6	#5	4'-2"	—
v1 (E)	79	#5	4'-4"	—
v2 (E)	14	#5	13'-0"	—
v3 (E)	12	#5	7'-8"	—
Structure Excavation		Cu. Yd.	136	
Concrete Structures		Cu. Yd.	22.0	
Reinforcement Bars, Epoxy Coated		Pound	3940	
Furnishing Steel		Foot	468	
Piles HP12x53		Foot	468	
Test Pile Steel HP12x53		Each	1	
Concrete Encasement		Cu. Yd.	2.5	
Bar Splicers		Each	12	
Pile Shoes		Each	7	

For details of Bar Splicers, see sheet 24 of 28.
For HP Pile Details, see sheet 22 of 28.
Space reinforcement in cap to miss anchor bolts.

**WEST ABUTMENT
ILLINOIS ROUTE 3 OVER
PIASA CREEK
F.A.U. ROUTE 8956 SEC 59BR-1
MADISON COUNTY
STATION 638+11.00
STRUCTURE NO. 060-0343**

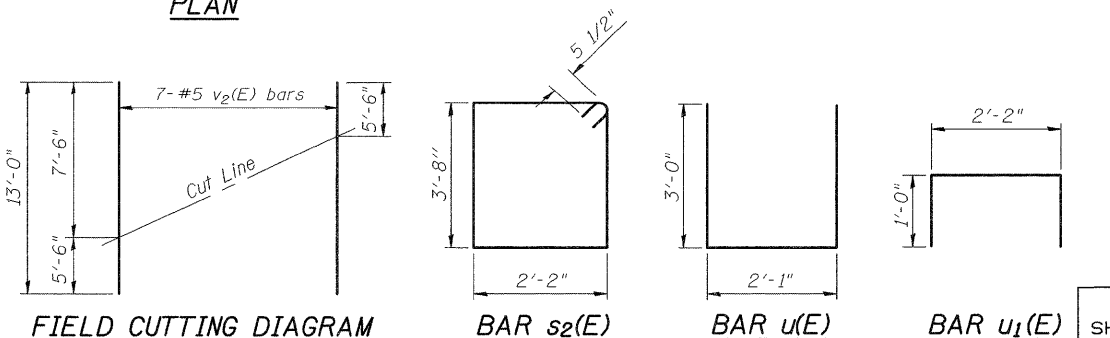
DETAIL A



PLAN

PILE DATA

Type: HP 12x53
Nominal Required Bearing: 340 kips
Factored Resistance Available: 170 kips
Est. Length: 78 ft.
No. Production Piles: 6
No. Test Piles: 1



FIELD CUTTING DIAGRAM

BAR s2(E)

BAR u(E)

BAR u1(E)

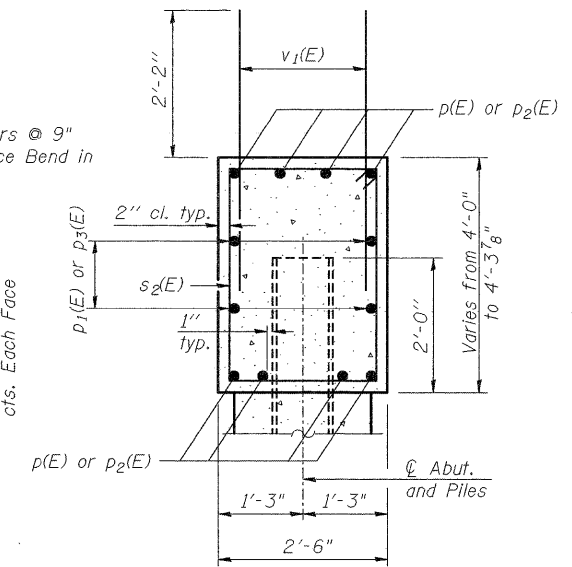
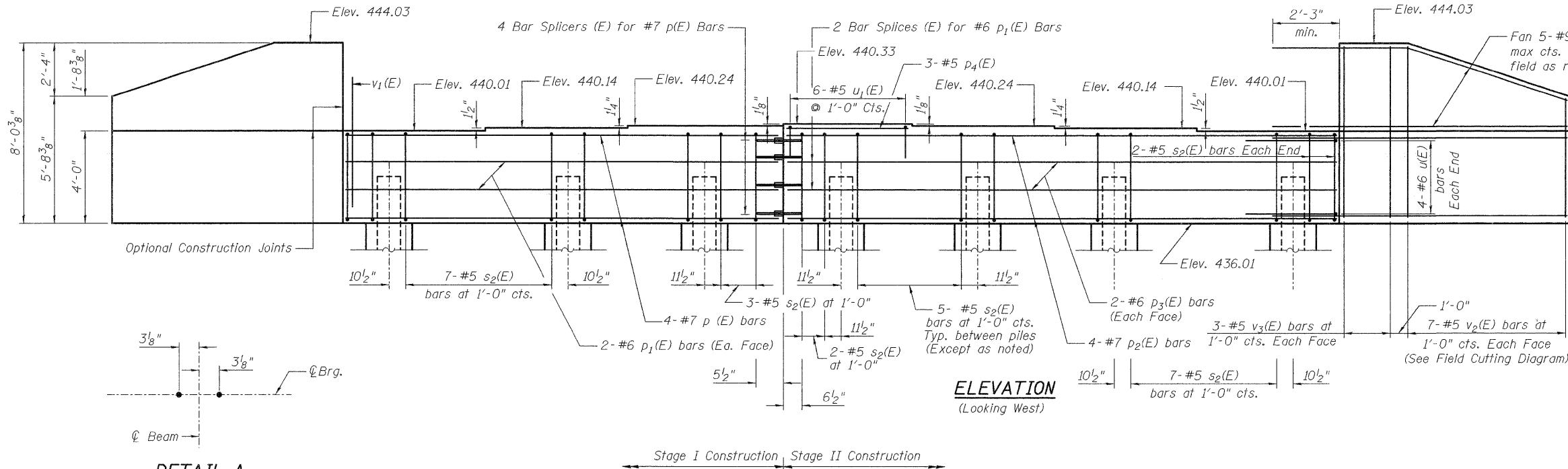
DESIGNED	DB
CHECKED	RS
DRAWN	ER
CHECKED	WWH

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Phone: (618)658-0000(618) 658-0000 Fax: (618)658-0041
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SHEET NO. 16 28 SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	8956	59BR-1	MADISON		45
STRUCTURE NO. 060-0343		CONTRACT NO. 76B18			
FED. ROAD DIST. NO.		ILLINOIS		FED. AID PROJECT	

Notes: Four steps monolithically with cap.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h(E)	40	#9	12'-6"	—
p (E)	8	#7	18'-10"	—
p ₁ (E)	4	#6	18'-10"	—
p ₂ (E)	8	#7	23'-10"	—
p ₃ (E)	4	#6	23'-10"	—
p ₄ (E)	3	#5	5'-4"	—
s ₂ (E)	38	#5	12'-7"	□
u ₁ (E)	8	#6	8'-1"	—
u ₂ (E)	6	#5	4'-2"	—
v ₁ (E)	79	#5	4'-4"	—
v ₂ (E)	14	#5	13'-0"	—
v ₃ (E)	12	#5	7'-8"	—
Structure Excavation		Cu. Yd.	136	
Concrete Structures		Cu. Yd.	22.0	
Reinforcement Bars, Epoxy Coated		Pound	3940	
Furnishing Steel Piles HP12x53		Foot	444	
Driving Piles		Foot	444	
Test Pile Steel HP12x53		Each	1	
Concrete Encasement		Cu. Yd.	2.5	
Bar Splicers		Each	12	
Pile Shoes		Each	7	

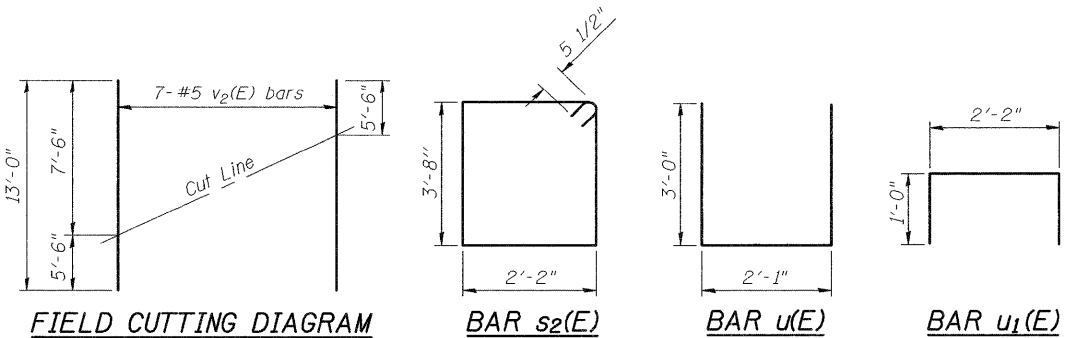
For details of Bar Splicers, see sheet 24 of 28.
For HP Pile Details see sheet 22 of 28.
Space reinforcement in cap to miss anchor bolts.

EAST ABUTMENT
ILLINOIS ROUTE 3 OVER
PIASA CREEK
F.A.U. ROUTE 8956 SEC 59BR-1
MADISON COUNTY
STATION 638+11.00
STRUCTURE NO. 060-0343

DETAIL A

PLAN

PILE DATA
Type: HP 12x53
Nominal Required Bearing: 340 kips
Factored Resistance Available: 170 kips
Est. Length: 74 ft.
No. Production Piles: 6
No. Test Piles: 1



DESIGNED	DB
CHECKED	RS
DRAWN	ER
CHECKED	WWH

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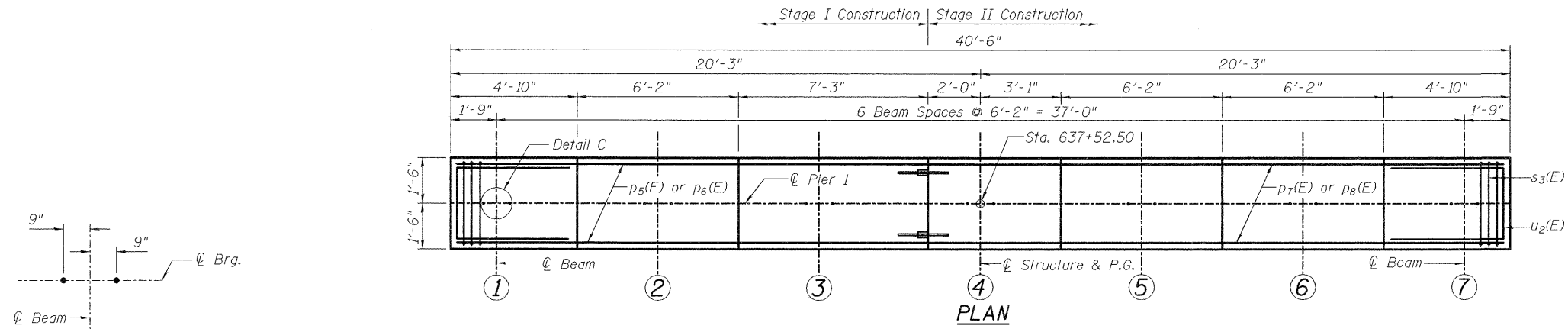
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Fax: (618)859-0941

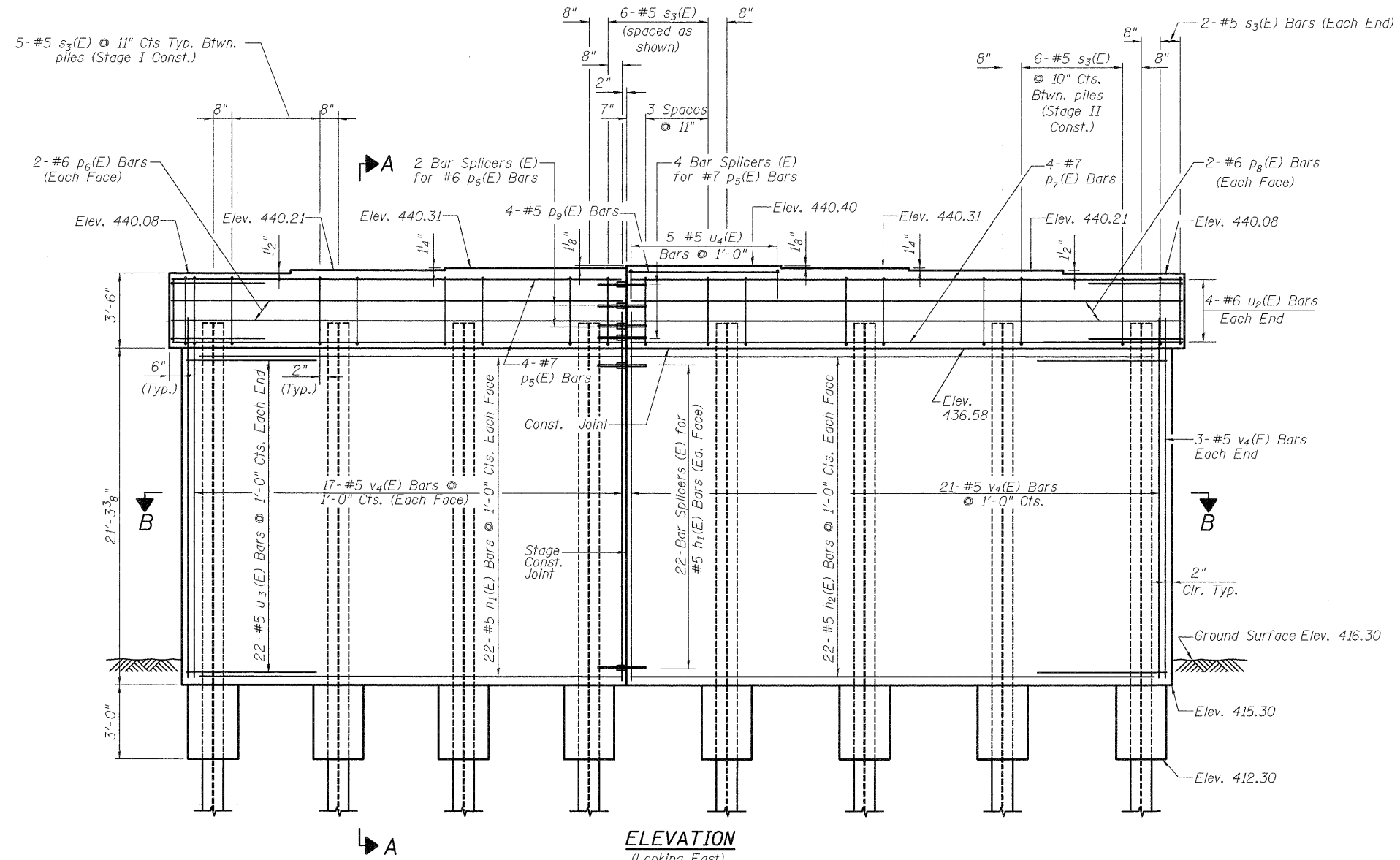
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SHEET NO. 17 28 SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	8956	59BR-1	MADISON		46
STRUCTURE NO. 060-0343		CONTRACT NO. 76B18			
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



DETAIL C



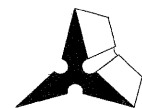
ELEVATION
(Looking East)

PILE DATA

Type: HP 12x74
Nominal Required Bearing: 584 kips
Factored Resistance Available: 290 kips
Est. Length: 84 ft.
No. Production Piles: 8

PIER 1
ILLINOIS ROUTE 3 OVER
PIASA CREEK
F.A.U. ROUTE 8956 SEC 59BR-1
MADISON COUNTY
STATION 638+11.00
STRUCTURE NO. 060-0343

DESIGNED	DB
CHECKED	RS
DRAWN	ER
CHECKED	WWH



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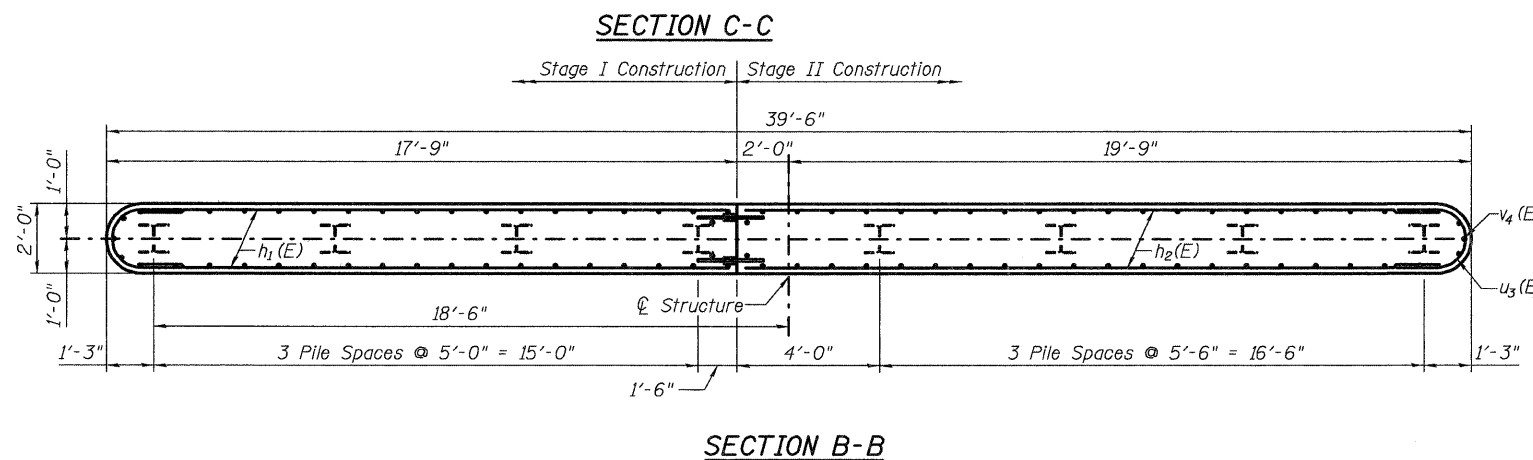
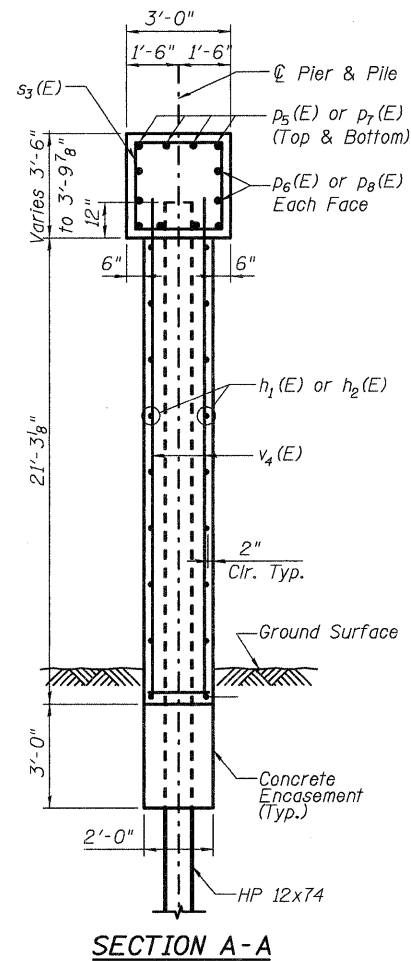
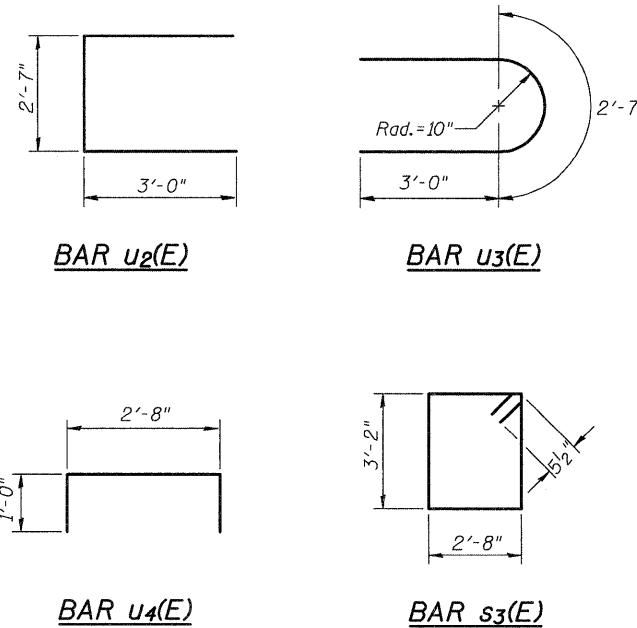
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SHEET NO. 18	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	8956	59BR-1	MADISON		47
28 SHEETS	STRUCTURE NO. 060-0343		CONTRACT NO. 76B18		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 1
BILL OF MATERIAL

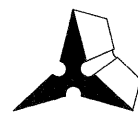
Bar	No.	Size	Length	Shape
$h_1(E)$	44	#5	16'-6"	—
$h_2(E)$	44	#5	20'-6"	—
$p_5(E)$	8	#7	18'-0"	—
$p_6(E)$	4	#6	18'-0"	—
$p_7(E)$	8	#7	22'-0"	—
$p_8(E)$	4	#6	22'-0"	—
$p_9(E)$	4	#5	4'-10"	—
$s_3(E)$	43	#5	12'-7"	□
$u_2(E)$	8	#6	8'-7"	—
$u_3(E)$	44	#5	8'-7"	—
$u_4(E)$	5	#5	4'-8"	—
$v_4(E)$	82	#5	22'-6"	—
Structure Excavation		Cu. Yd.	13	
Concrete Structures		Cu. Yd.	78.0	
Reinforcement Bars, Epoxy Coated		Pound	5620	
Furnishing Steel Piles HP12x74		Foot	672	
Driving Piles		Foot	672	
Bar Splicers		Each	56	
Concrete Encasement		Cu. Yd.	2.8	
Pile Shoes		Each	8	



Notes:
Space reinforcement in cap to miss anchor bolts.
Pour steps monolithically with cap.
For HP Pile Details, see sheet 22 of 28.
For details of Bar Splicers, see Sheet 24 of 28.

PIER 1 DETAILS
ILLINOIS ROUTE 3 OVER
PIASA CREEK
F.A.U. ROUTE 8956 SEC 59BR-1
MADISON COUNTY
STATION 638+11.00
STRUCTURE NO. 060-0343

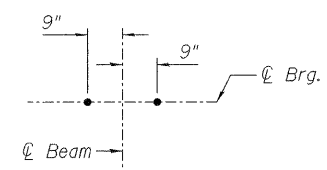
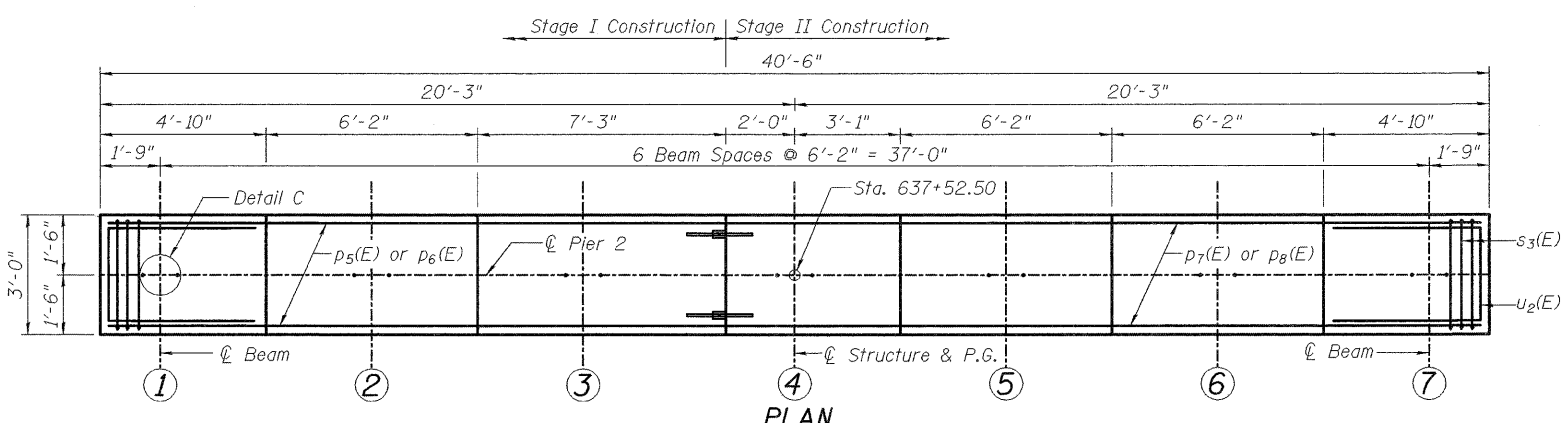
DESIGNED	DB
CHECKED	RS
DRAWN	ER
CHECKED	WWH



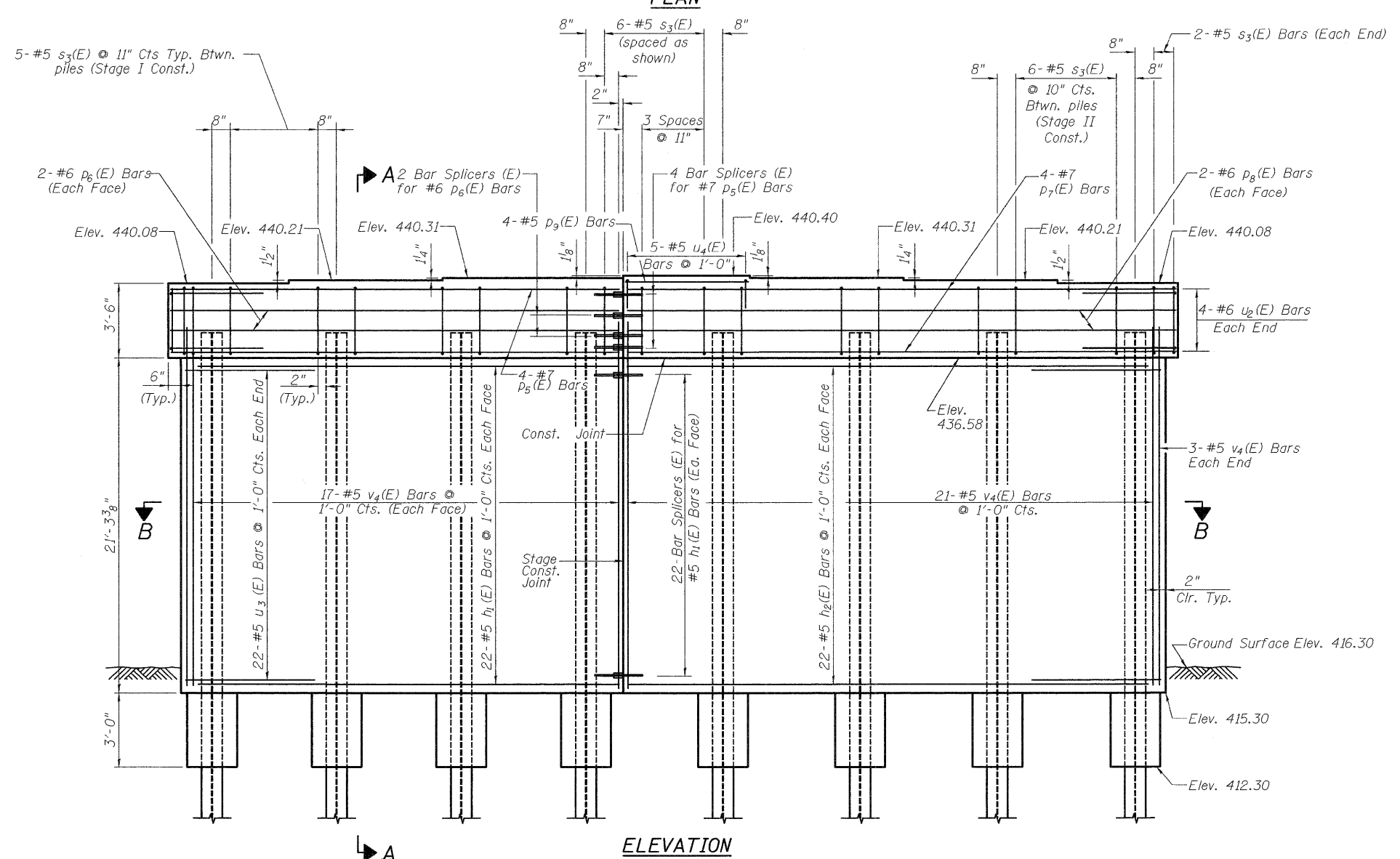
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SHEET NO. 19	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
28 SHEETS	8956	59BR-1	MADISON	75	48
STRUCTURE NO. 060-0343		CONTRACT NO. 76B18			
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



DETAIL C



ELEVATION
(Looking East)

PILE DATA

Type: HP 12x74
Nominal Required Bearing: 584 kips
Factored Resistance Available: 290 kips
Est. Length: 82 ft.
No. Production Piles: 7
No. Test Piles: 1

PIER 2
ILLINOIS ROUTE 3 OVER
PIASA CREEK
F.A.U. ROUTE 8956 SEC 59BR-1
MADISON COUNTY
STATION 638+11.00
STRUCTURE NO. 060-0343

DESIGNED	DB
CHECKED	RS
DRAWN	ER
CHECKED	WWH

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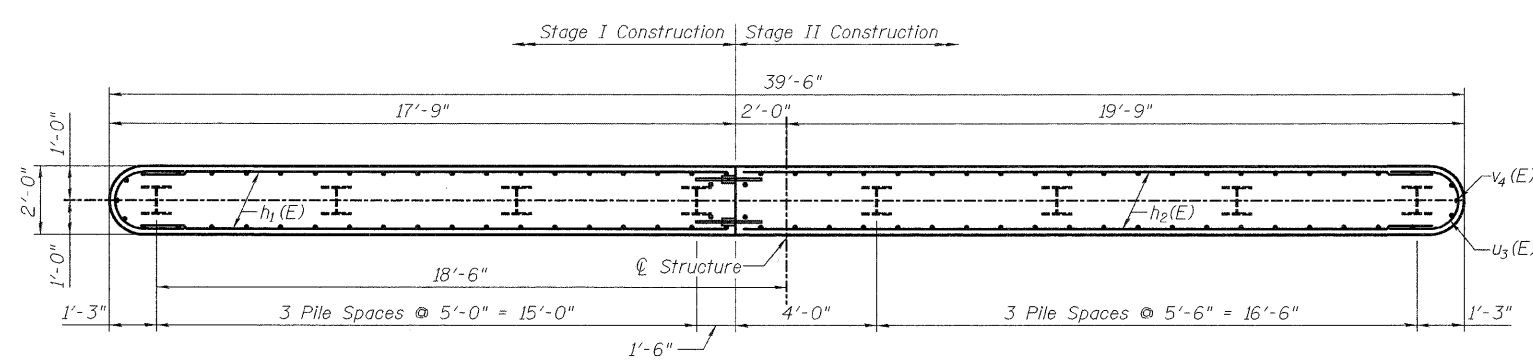
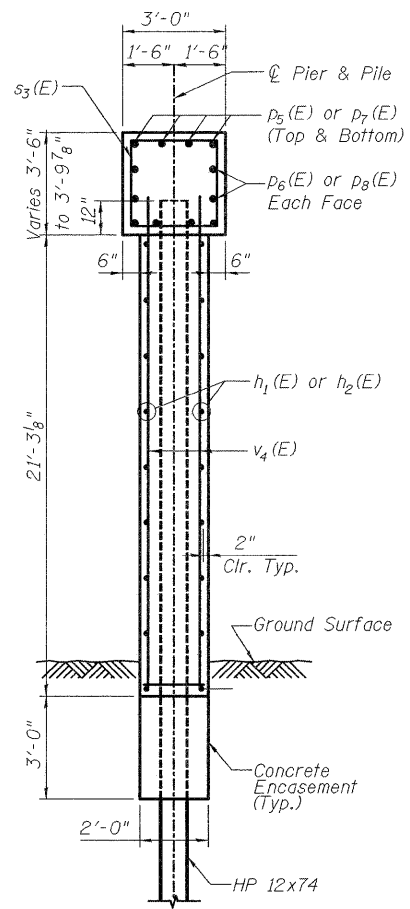
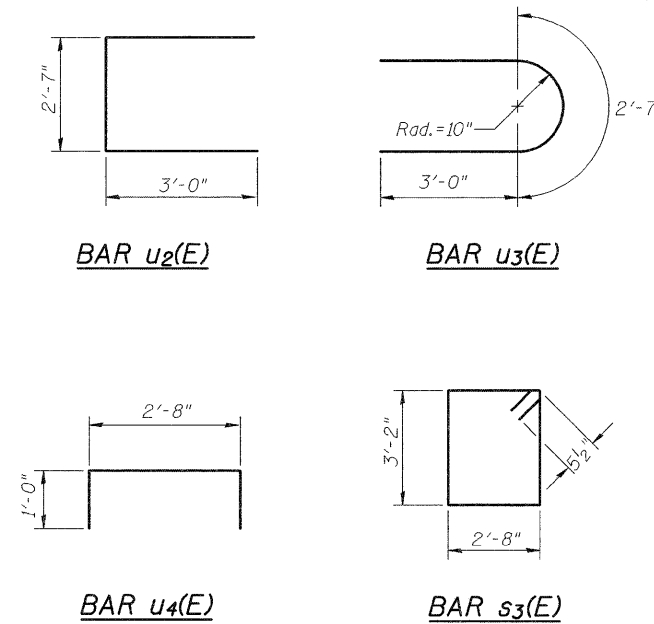
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Fax: (618) 659-0941

SHEET NO. 20	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	8956	59BR-1	MADISON		49
28 SHEETS	STRUCTURE NO. 060-0343		CONTRACT NO. 76B18		
FED. ROAD DIST. NO.		ILLINOIS		FED. AID PROJECT	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 2
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
$h_1(E)$	44	#5	16'-6"	—
$h_2(E)$	44	#5	20'-6"	—
$p_5(E)$	8	#7	18'-0"	—
$p_6(E)$	4	#6	18'-0"	—
$p_7(E)$	8	#7	22'-0"	—
$p_8(E)$	4	#6	22'-0"	—
$p_9(E)$	4	#5	4'-10"	—
$s_3(E)$	43	#5	4'-10"	□
$u_2(E)$	8	#6	8'-7"	—
$u_3(E)$	44	#5	8'-7"	—
$u_4(E)$	5	#5	4'-8"	—
$v_4(E)$	82	#5	22'-6"	—
Structure Excavation			Cu. Yd.	13
Concrete Structures			Cu. Yd.	78.0
Reinforcement Bars, Epoxy Coated			Pound	5620
Furnishing Steel Piles HP12x74			Foot	574
Driving Piles			Foot	574
Test Pile Steel HP 12x74			Each	1
Bar Splicers			Each	56
Concrete Encasement			Cu. Yd.	2.8
Pile Shoes			Each	8



Notes:
Space reinforcement in cap to miss anchor bolts.
Pour steps monolithically with cap.
For HP Pile Details, see sheet 22 of 28.
For details of Bar Splicers, see Sheet 24 of 28.

PIER 2 DETAILS
ILLINOIS ROUTE 3 OVER
PIASA CREEK
F.A.U. ROUTE 8956 SEC 59BR-1
MADISON COUNTY
STATION 638+11.00
STRUCTURE NO. 060-0343

DESIGNED	DB
CHECKED	RS
DRAWN	ER
CHECKED	WWH

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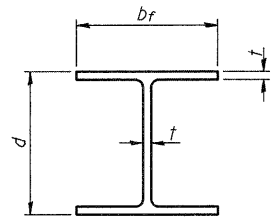
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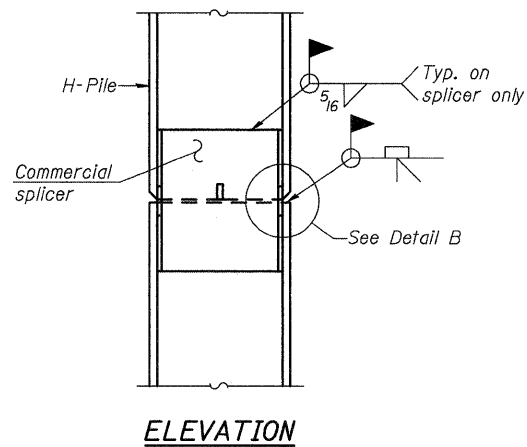
SHEET NO. 21 28 SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	8956	59BR-1	MADISON		50
STRUCTURE NO. 060-0343		CONTRACT NO. 76B18			
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

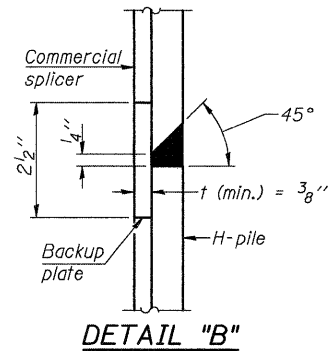


STEEL PILE TABLE

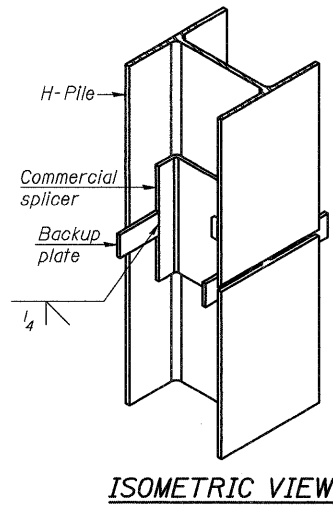
Designation	Depth d	Flange width bf	Web and Flange thickness t	Encasement diameter A
HP 14x117	14 1/4"	14 7/8"	13/16"	30"
x102	14"	14 3/4"	1/16"	30"
x89	13 7/8"	14 3/4"	5/8"	30"
x73	13 5/8"	14 5/8"	1/2"	30"
HP 12x84	12 1/4"	12 1/4"	1/16"	24"
x74	12 1/8"	12 1/4"	5/8"	24"
x63	12"	12 1/8"	1/2"	24"
x53	11 3/4"	12"	7/16"	24"
HP 10x57	10"	10 1/4"	9/16"	24"
x42	9 3/4"	10 1/8"	7/16"	24"
HP 8x36	8"	8 1/8"	7/16"	18"



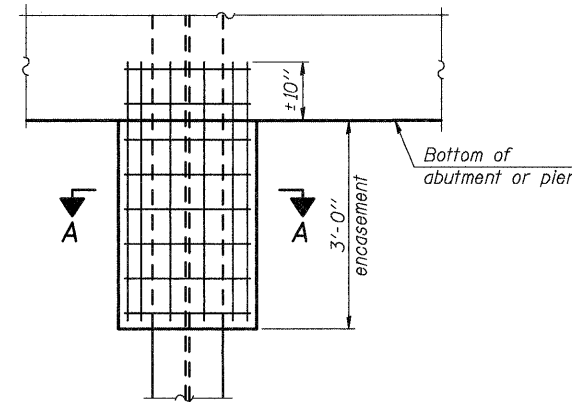
ELEVATION



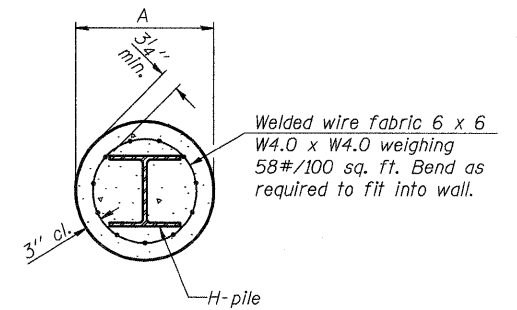
DETAIL "B"



ISOMETRIC VIEW



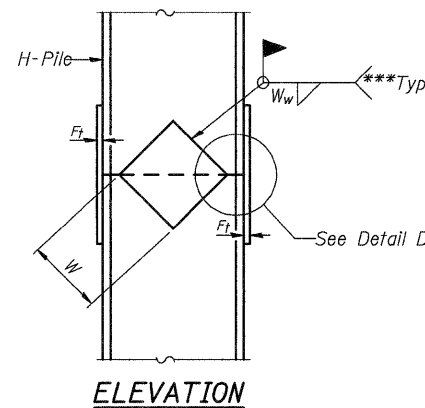
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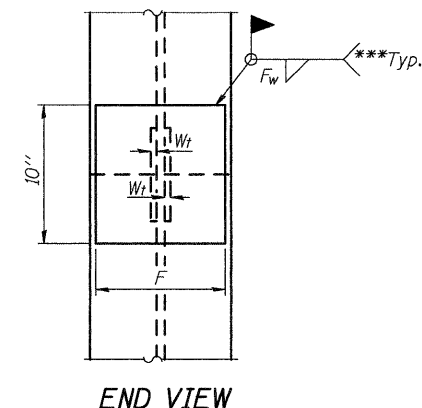
SECTION A-A

Note: Forms for encasement may be omitted when soil conditions permit.

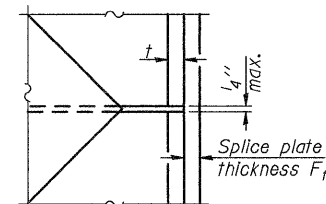
PILE ENCASEMENT



ELEVATION



END VIEW

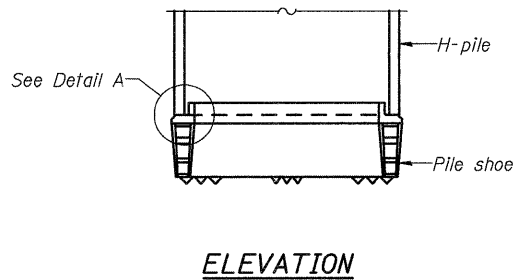


DETAIL D

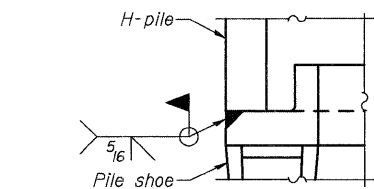
Designation	F	Ft	Fw	W	Wt	Ww
HP 14x117	12 1/2"	1"	7/8"	7 3/4"	5 1/2"	1/2"
x102	12 1/2"	7/8"	3/4"	7 3/4"	5 1/2"	1/2"
x89	12 1/2"	3/4"	1/16"	7 3/4"	5 1/2"	1/2"
x73	12 1/2"	5/8"	9/16"	7 3/4"	5 1/2"	1/2"
HP 12x84	10"	7/8"	1/16"	6 1/2"	5 1/2"	1/2"
x74	10"	7/8"	1/16"	6 1/2"	5 1/2"	1/2"
x63	10"	5/8"	1/2"	6 1/2"	5 1/2"	3/8"
x53	10"	5/8"	1/2"	6 1/2"	5 1/2"	3/8"
HP 10x57	8"	3/4"	9/16"	5 1/4"	5 1/2"	3/8"
x42	8"	5/8"	9/16"	5 1/4"	5 1/2"	3/8"
HP 8x36	7"	5/8"	7/16"	4 1/4"	5 1/2"	3/8"

HP PILE DETAILS
ILLINOIS ROUTE 3 OVER
PIASA CREEK
F.A.U. ROUTE 8956 SEC 59BR-1
MADISON COUNTY
STATION 638+11.00
STRUCTURE NO. 060-0343

Note: The steel H-piles shall be according to AASHTO M270 Grade 50.

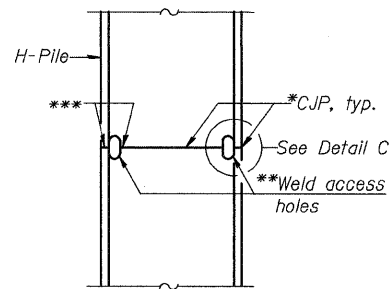


ELEVATION

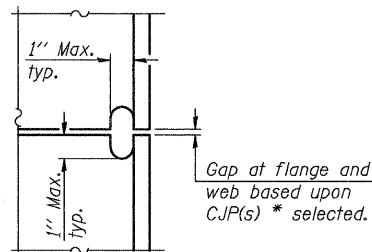


DETAIL A

H-PILE SHOE ATTACHMENT



ELEVATION



DETAIL C

COMPLETE PENETRATION WELD SPLICE

WELDED PLATE FIELD SPLICE

DESIGNED	DB
CHECKED	RS
DRAWN	ER
CHECKED	WWH

F-HP

5-16-08



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Fax: (618) 452-5541
100 North Research Drive
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*Use joint conforming to Figure 3.4 in AWS D1.1, Structure Welding Code-Steel.

**Preparation per Fig. 5.2 in AWS D1.1, Structure Welding Code-Steel.

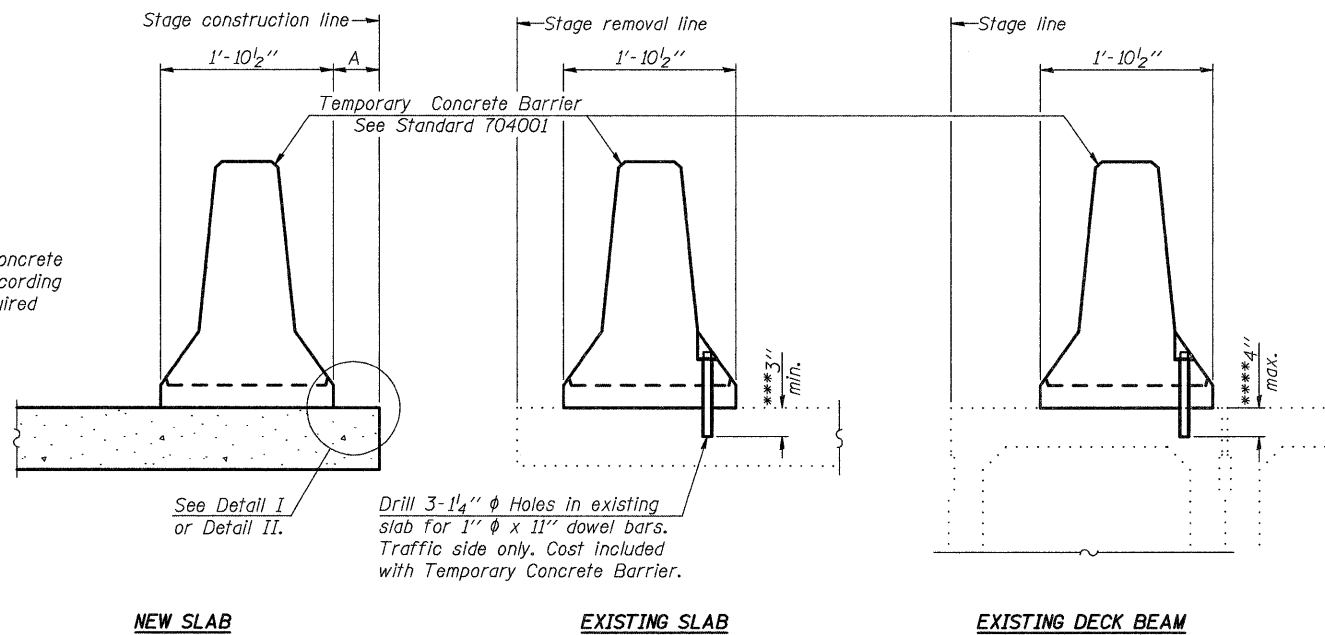
***Interrupt welds 1/4" from end of each pile.

SHEET NO. 22 28 SHEETS	F.A.U. RTE. 8956	SECTION 59BR-1	COUNTY MADISON	TOTAL SHEETS 75	SHEET NO. 51
	STRUCTURE NO. 060-0343		CONTRACT NO. 76B18		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT		

12/14/2006 10:07:36 AM IL Rte 3 Drawings - Weld Final Drawing with DDT Comment V22 HP Pile Details.dgn 2:45:23 PM Printed

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DEPARTMENT OF TRANSPORTATION

When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".



Drill 3-1/4" ϕ Holes in existing slab for 1" ϕ x 11" dowel bars. Traffic side only. Cost included with Temporary Concrete Barrier.

SECTIONS THRU SLAB OR DECK BEAM

NOTES

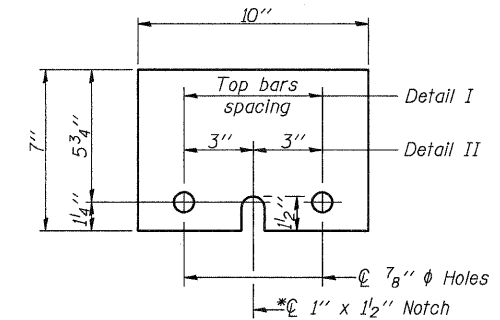
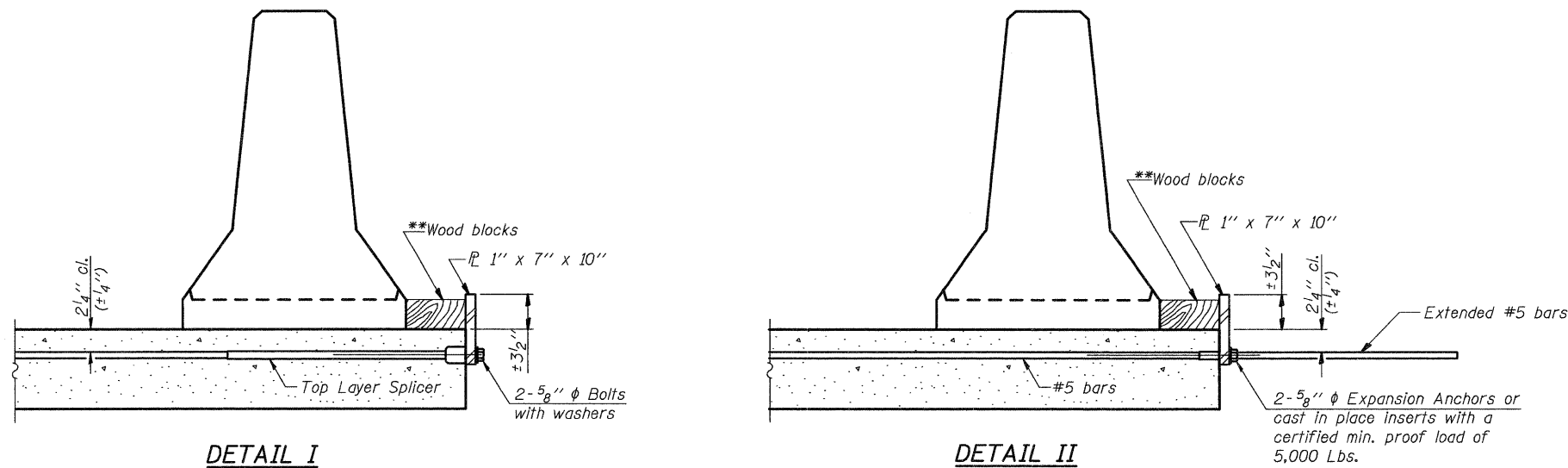
Detail I - With Bar Splicer or Couplers:
Connect one (1) 1"x7"x10" steel \bar{L} to the top layer of couplers with 2-5/8" ϕ bolts screwed to coupler at approximate \bar{C} of each barrier panel.

Detail II - With Extended Reinforcement Bars:
Connect one (1) 1"x7"x10" steel \bar{L} to the concrete slab or concrete wearing surface with 2-5/8" ϕ Expansion Anchors or cast in place Inserts spaced between the top layer of reinforcement at approximate \bar{C} of each barrier panel.

Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x 10" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

***Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

***If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



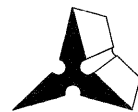
STEEL RETAINER \bar{L} 1" x 7" x 10"

*Required only with Detail II

**TEMPORARY CONCRETE
BARRIER DETAILS
ILLINOIS ROUTE 3 OVER
PIASA CREEK
F.A.U. ROUTE 8956 SEC 59BR-1
MADISON COUNTY
STATION 638+11.00
STRUCTURE NO. 060-0343**

**Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

DESIGNED	DB
CHECKED	RS
DRAWN	ER
CHECKED	WWH



Juneau Associates, Inc. P.C.
CONSULTING ENGINEERS AND LAND SURVEYORS
2120 State Street
Grafton City, Illinois 62040
Phone: (618) 977-1400
Fax: (618) 452-5541
100 North Research Drive
Edwardsville, Illinois 62025
Phone: (618) 659-0900/(618) 659-0800
Fax: (618) 659-0941
Registered Professional Service Corporation License No. 184-003389

SHEET NO. 23	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
28 SHEETS	8956	59BR-1	MADISON	75	52
STRUCTURE NO. 060-0343		CONTRACT NO. 76B18			
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NOTES

Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.
Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.
All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.
Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

- ① Minimum Capacity = $1.25 \times f_y \times A_1$
(Tension in kips)
 - ② Minimum *Pull-out Strength = $0.66 \times f_y \times A_1$
(Tension in kips)
- Where f_y = Yield strength of lapped reinforcement bars in ksi.
 A_1 = Tensile stress area of lapped reinforcement bars.
* = 28 day concrete

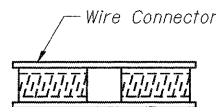
The diameter of this part is equal or larger than the diameter of bar spliced.

The diameter of this part is the same as the diameter of the bar spliced.

ROLLED THREAD DOWEL BAR



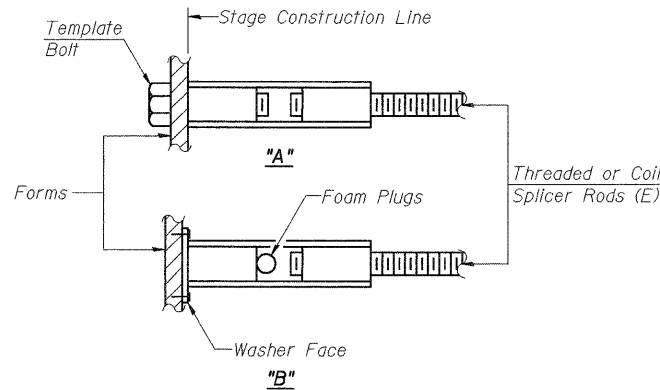
** ONE PIECE



WELDED SECTIONS

BAR SPLICER ASSEMBLY ALTERNATIVES

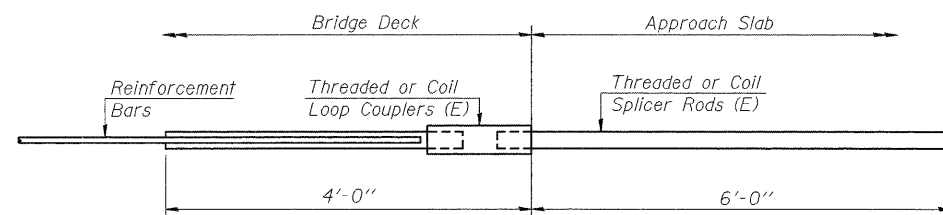
**Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



INSTALLATION AND SETTING METHODS

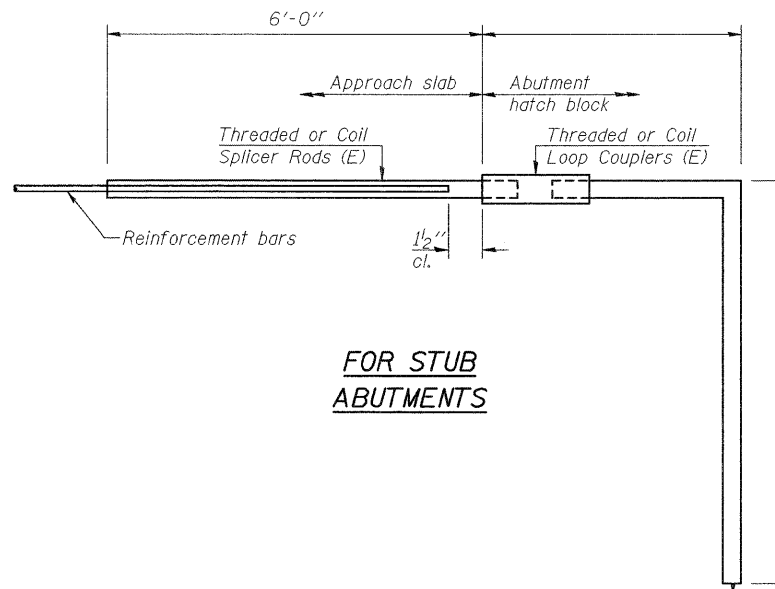
"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.

Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	7.9
#5	2'-2"	23.0	12.3
#6	2'-7"	33.1	17.4
#7	3'-5"	45.1	23.8
#8	4'-6"	58.9	31.3
#9	5'-9"	75.0	39.6
#10	7'-3"	95.0	50.3
#11	9'-0"	117.4	61.8



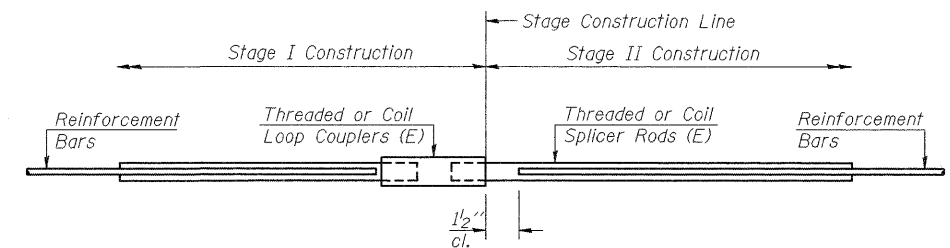
FOR INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required = 80



FOR STUB ABUTMENTS

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required =



STANDARD

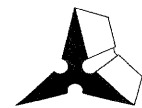
Bar Size	No. Assemblies Required	Location
#5	739	Deck
#5	44	Pier 1
#5	44	Pier 2
#6	16	Diaphragm
#6	4	W. Abutment
#6	4	E. Abutment
#6	4	Pier 1
#6	4	Pier 2
#7	8	W. Abutment
#7	8	E. Abutment
#7	8	Pier 1
#7	8	Pier 2

BAR SPLICER ASSEMBLY DETAILS
ILLINOIS ROUTE 3 OVER
PIASA CREEK
F.A.U. ROUTE 8956 SEC 59BR-1
MADISON COUNTY
STATION 638+11.00
STRUCTURE NO. 060-0343

DESIGNED DB
CHECKED RS
DRAWN ER
CHECKED WWH

BSD-1

10-1-08



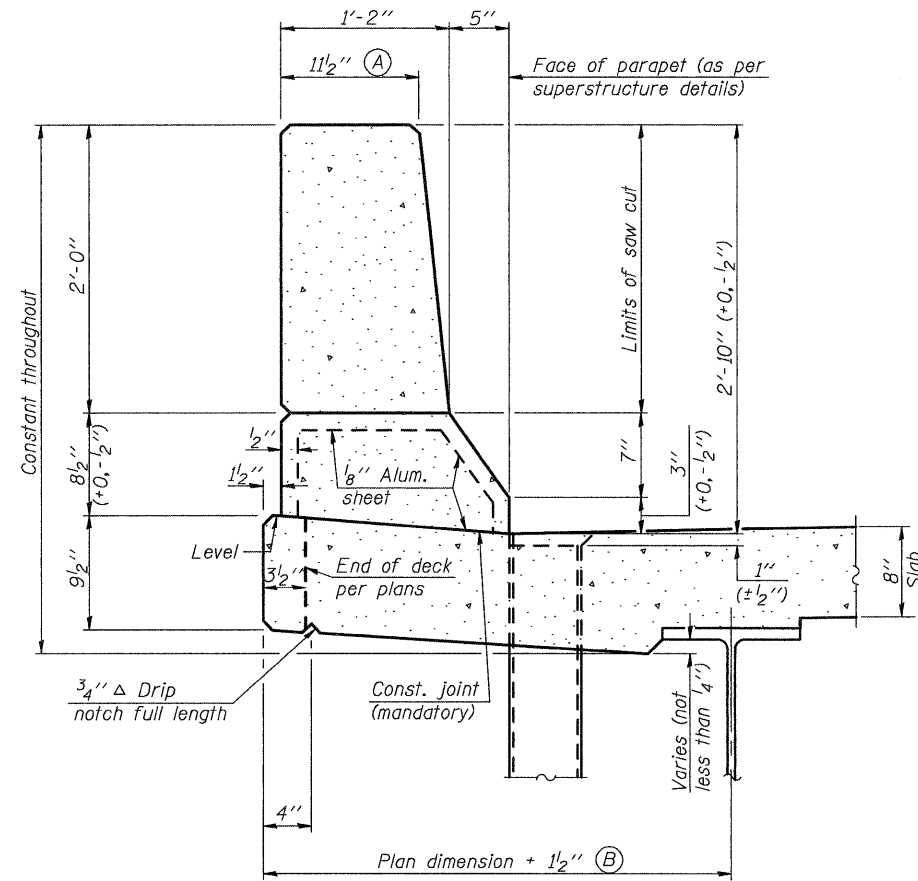
Juneau Associates, Inc. P.C.
CONSULTING ENGINEERS AND LAND SURVEYORS

2100 State Street
Glenview, Illinois 60040
Phone: (815) 777-1400
Fax: (815) 452-3541
Registered Professional Service Corporation License No. 184-003389

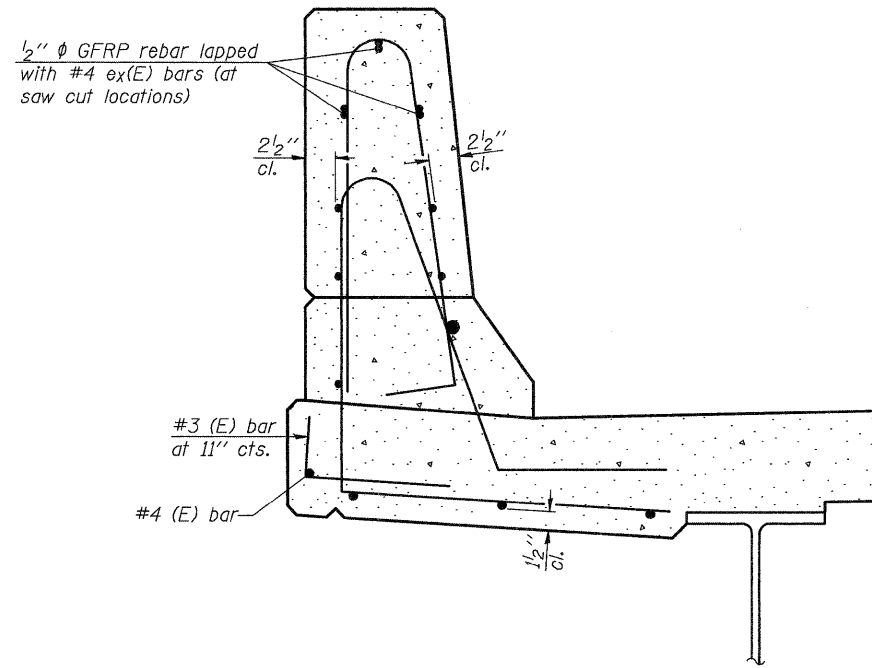
100 North Research Drive
Edwardsville, Illinois 62026
Phone: (618) 659-0900 (410) 659-0900
Fax: (618) 659-5841

SHEET NO. 24 28 SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	8956	59BR-1	MADISON		53
STRUCTURE NO. 060-0343		CONTRACT NO. 76B18			
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

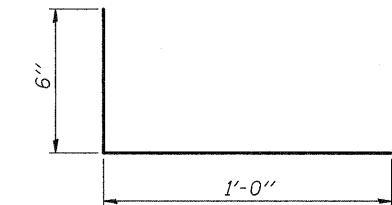
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



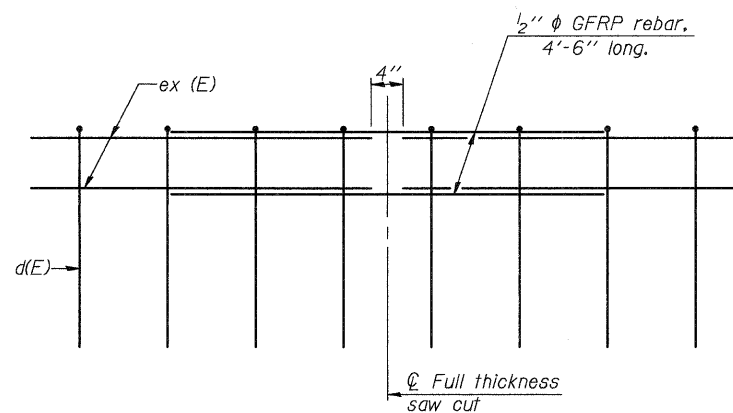
SECTION
(Showing dimensions)



SECTION
(Showing reinforcement clearances for slip forming and additional reinforcement bars)



#3 (E) BAR



GFRP REBAR STIFFENING DETAIL

(Place as shown in parapet section at each parapet joint location.)

GENERAL NOTES
All dimensions shall remain the same as shown on contract plans, except dimensions A and B which are to be revised as shown to provide additional clearance. Additional concrete needed to revise dimension A and B= 0.0165 cu. yds./ft. of parapet.
Place aluminum sheet in curb portion at and near piers. Full thickness saw cut at all joint locations in lieu of cork joint filler.

**CONCRETE PARAPET
SLIPFORMING OPTION
ILLINOIS ROUTE 3 OVER
PIASA CREEK
F.A.U. ROUTE 8956 SEC 59BR-1
MADISON COUNTY
STATION 638+11.00
STRUCTURE NO. 060-0343**

DESIGNED	DB
CHECKED	RS
DRAWN	ER
CHECKED	WWH



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CONSULTING ENGINEERS AND LAND SURVEYORS

2100 State Street
Granite City, Illinois 62040
Phone: (618) 977-1400
Fax: (618) 652-5541

100 North Research Drive
Edwardsville, Illinois 62025
Phone: (618) 659-0900/(618) 659-0800
Fax: (618) 659-0941

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SFP-34 5-16-08

SHEET NO. 25 28 SHEETS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	8956	59BR-1	MADISON	75	54
STRUCTURE NO. 060-0343		CONTRACT NO. 76B18			
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT		

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION



SOIL BORING LOG

ROUTE FAU 8956 DESCRIPTION IL 3 over Piassa Creek LOGGED BY

SECTION 59BR-1 LOCATION SW 1/4, NW 1/4, SEC. 19, TWP. 6N, RNG. 10W, 3 PM

COUNTY Madison DRILLING METHOD HAMMER TYPE

Table with columns for Soil Properties (D, B, U, M, O, I, S, T) and Depth. Includes groundwater elevations and soil descriptions like 'Red Brown and Gray CLAY', 'Brown and Gray Coarse SAND with Gravel', 'Brown Sandy Silty CLAY', and 'Brown and Gray Coarse SAND with Gravel'.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) BBS, from 137 (Rev. 8-99)



SOIL BORING LOG

ROUTE FAU 8956 DESCRIPTION IL 3 over Piassa Creek LOGGED BY

SECTION 59BR-1 LOCATION SW 1/4, NW 1/4, SEC. 19, TWP. 6N, RNG. 10W, 3 PM

COUNTY Madison DRILLING METHOD HAMMER TYPE

Table with columns for Soil Properties and Depth. Includes groundwater elevations and soil descriptions like 'Brown and Gray Silty CLAY', 'Brown Medium SAND with some Gravel', 'Brown and Gray Sandy SILT', and 'Brown and Gray Coarse SAND with some Gravel'.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) The SPT (H value) is the sum of the last two blow values in each sampling zone (AASHTO T206) BBS, from 137 (Rev. 8-99)



SOIL BORING LOG

ROUTE FAU 8956 DESCRIPTION IL 3 over Piassa Creek LOGGED BY

SECTION 59BR-1 LOCATION SW 1/4, NW 1/4, SEC. 19, TWP. 6N, RNG. 10W, 3 PM

COUNTY Madison DRILLING METHOD HAMMER TYPE

Table with columns for Soil Properties and Depth. Includes groundwater elevations and soil descriptions like 'Red Brown and Gray CLAY', 'Brown and Gray Coarse SAND with Gravel', 'Brown and Gray Coarse SAND with Gravel', and 'Large CHERT Cobbles with Gravel'.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) BBS, from 137 (Rev. 8-99)

P:\STRUCTURED\8609... 8/14/2008

Printed 8:49 AM 8/14/2008

Table with design and check status: DESIGNED DB, CHECKED RS, DRAWN ER, CHECKED WWH

Juneau Associates, Inc. P.C. CONSULTING ENGINEERS AND LAND SURVEYORS. 2108 State Street, Granite City, Illinois 62040

BORING LOGS ILLINOIS ROUTE 3 OVER PIASSA CREEK F.A.U. ROUTE 8956 SEC 59BR-1 MADISON COUNTY STATION 638+11.00 STRUCTURE NO. 060-0343

Summary table: SHEET NO. 27 / 28 SHEETS, F.A.U. RTE. 8956, SECTION 59BR-1, COUNTY MADISON, TOTAL SHEETS 75, SHEET NO. 56, STRUCTURE NO. 060-0343, CONTRACT NO. 76B18, FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



Illinois Department of Transportation
Division of Highways
Illinois Department of Transportation

SOIL BORING LOG

Page 1 of 2

Date 9/13/76

ROUTE FAU 8956 DESCRIPTION IL 3 over Piassa Creek LOGGED BY _____
SECTION 59BR-1 LOCATION SW 1/4, NW 1/4, SEC. 19, TWP. 6N, RNG. 10W, 3 PM
COUNTY Madison DRILLING METHOD _____ HAMMER TYPE _____

STRUCT. NO. Station	BORING NO. Station Offset Ground Surface Elev.	DEPTH ft	BLOW (ft)	UCS (tsf)	MOIST (%)	Surface Water Elev. Stream Bed Elev. Groundwater Elev.: First Encounter Upon Completion After Hrs.	DEPTH ft	BLOW (ft)	UCS (tsf)	MOIST (%)	Description
060-0153 (E) / 060-0343 (P) 638+00	4 W. Abut 637+00 20.00 ft RT 430.8										
											Brown and Gray Silty CLAY
							1	NC			Brown Fine to Medium SAND (continued) with some Gravel
			4	0.5 S	17		6	NC			
			4	0.6 S	17		10	NC			
			3	0.5 S	18		9	0.8 S	24		Gray Silty CLAY with some Gravel
			3	0.6 S	18		7	0.7 B	24		Brown and Gray Sandy Silty CLAY
			2	0.3 B	28		19	NC			Brown and Gray Medium SAND
			3	0.4 B	30		12	2.3 B	27		Red Brown and Gray CLAY
			4	1.0 B	27		10	1.6 B	26		
											Red Brown Sandy Silty CLAY

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) BBS, from 137 (Rev. 8-99)



Illinois Department of Transportation
Division of Highways
Illinois Department of Transportation

SOIL BORING LOG

Page 2 of 2

Date 9/13/76

ROUTE FAU 8956 DESCRIPTION IL 3 over Piassa Creek LOGGED BY _____
SECTION 59BR-1 LOCATION SW 1/4, NW 1/4, SEC. 19, TWP. 6N, RNG. 10W, 3 PM
COUNTY Madison DRILLING METHOD _____ HAMMER TYPE _____

STRUCT. NO. Station	BORING NO. Station Offset Ground Surface Elev.	DEPTH ft	BLOW (ft)	UCS (tsf)	MOIST (%)	Surface Water Elev. Stream Bed Elev. Groundwater Elev.: First Encounter Upon Completion After Hrs.	DEPTH ft	BLOW (ft)	UCS (tsf)	MOIST (%)	Description
060-0153 (E) / 060-0343 (P) 638+00	4 W. Abut 637+00 20.00 ft RT 430.8										
											Red Brown Sandy Silty CLAY (continued)
							8	0.6 S	27		Brown and Gray Coarse Clayey SAND with Gravel (continued)
											Brown and Gray Medium SAND
											Red Brown and Gray CLAY
											Brown and Gray Broken LIMESTONE
											REFUSAL - END OF BORING
											Brown and Gray Medium to Coarse SAND with some Gravel
											Brown and Gray Coarse Clayey SAND with Gravel

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) BBS, from 137 (Rev. 8-99)

BORING LOGS
ILLINOIS ROUTE 3 OVER
PIASSA CREEK
F.A.U. ROUTE 8956 SEC 59BR-1
MADISON COUNTY
STATION 638+11.00
STRUCTURE NO. 060-0343

SHEET NO. 28	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
28 SHEETS	8956	59BR-1	MADISON	75	57
	STRUCTURE NO. 060-0343		CONTRACT NO. 76B18		
	FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

DESIGNED <i>DB</i>
CHECKED <i>RS</i>
DRAWN <i>ER</i>
CHECKED <i>WWH</i>



Juneau Associates, Inc. P.C.
CONSULTING ENGINEERS AND LAND SURVEYORS

2195 State Street
Grafton City, Illinois 62340
Phone: (618) 947-1400
Fax: (618) 452-4541
100 North Research Drive
Edwardsville, Illinois 62025
Phone: (618) 668-0000/(618) 659-0000
Fax: (618) 659-4941
Registered Professional Service Corporation License No. 194-003883

B.M. "d" Chiseled on Southeast corner of West Abut. Bridge Seat Elev. +437.82
 Existing Structure: No. 000-0153 built as S.B.I. Rte. 3, Sec. 59 B. & C. in 1924 @ Sta. 638+00.00. 200'-0" Steel Truss and part of Substructure to be removed as shown. Detour to be provided 60'-0" South of Existing Structure. No Salvage.

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PROJECT NO.	SECTION	SHEET NO.	TOTAL SHEETS
8956	59BR	21	12

GENERAL NOTES

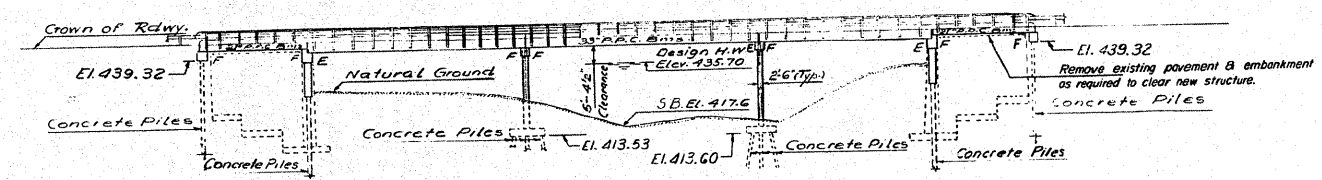
See Proposal for Boring Data.
 It shall be the responsibility of the Contractor to verify all dimensions and conditions existing in the field prior to construction and ordering of materials.
 The top surface of the beams shall be finished in accordance with Article 503.06 of the Standard Specifications except that the surface shall not be roughened by brooming. The finished surface shall be free of depressions or high spots with sharp corners.
 Protective Coat shall not be applied to surfaces to which Waterproofing Membrane System is applied.
 Expansion Bolts shall consist of self drilling expansion anchors and 3/4" x 12" hooked bolts.
 All structural steel shall be shop painted with two coats of basic lead silico chromate paint.
 Expansion guards which are not cast in the precast unit shall be fabricated and erected in accordance with Article 503.07 (c) of the Standard Specifications and are included in quantity of structural steel.
 The Contractor shall drive 2 concrete test piles in permanent locations, one at Pier #2 and one at the East Approach Bent, as directed by the Engineer before ordering the remainder of piles.
 Cost of required concrete removal on the substructure shall be incidental to "Removal of Existing Superstructure".

STATION 638+00
 BUILT 197 BY
 STATE OF ILLINOIS
 S.B.I. RT. 3 SEC. 59 BR
 LOADING HS20
 See Standard 213
 NAME PLATE

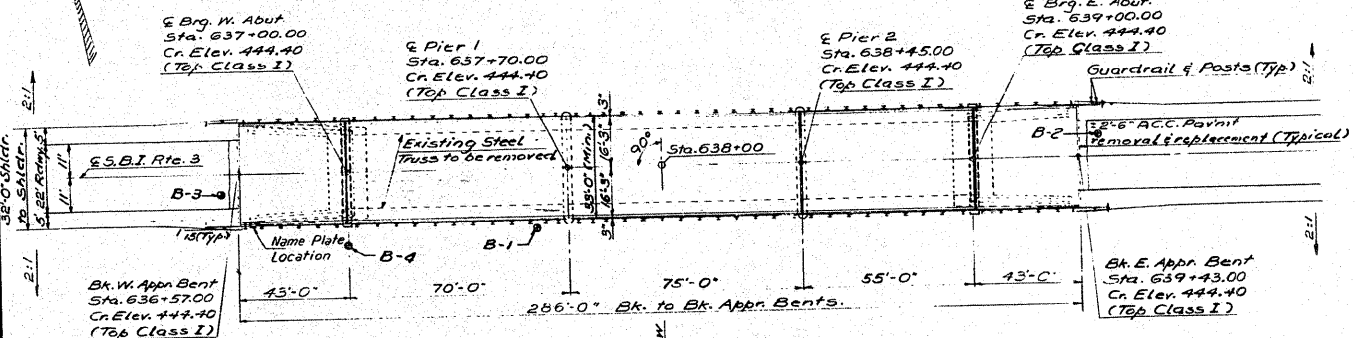
TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Removal of Existing Superstructure	Each	1		1
Preformed Joint Sealer (2'S)	Lin. Ft.	99		99
Pavement Removal & P.C.C. Replacement, Type 2 (10')	Sq. Yds.	12		12
Structure Excavation	Tons	72	56	128
Bituminous Concrete Surface Course, Class 2	Sq. Yds.	990		990
Waterproofing Membrane System	Sq. Yds.		140	140
Expansion Bolts (3/4" #)	Each	95		95
Protective Coat	Sq. Yds.	21.9	272.3	294.2
Class X Concrete	Cu. Yds.	2746		2746
Precast Prestressed Concrete Deck Beams (21")	Sq. Ft.	6631		6631
Precast Prestressed Concrete Deck Beams (33")	Sq. Ft.	1380	14,910	16,290
Reinforcement Bars *	Pounds		1	1
Name Plates	Each		1	1
Portland Cement Mortar Fairing Course	Lin. Ft.	2840		2840
Concrete Piles	Lin. Ft.		2072	2072
Test Piles Concrete	Each		2	2
Steel Railing Type T	Lin. Ft.	572		572
Structural Steel	Pounds	6680		6680
Cofferdam (Pier #1)	Each		1	1
Cofferdam (Pier #2)	Each		1	1
Cofferdam Excavation	Cu. Yds.		304	304
Pavement Removal	Sq. Yds.	232		232

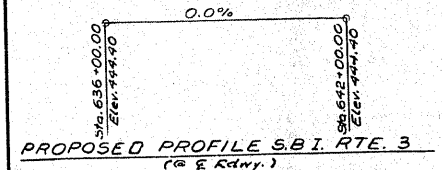
All reinforcement bars shall conform with AASHTO M-31 or M-53, Grade 60.



ELEVATION



PLAN



PROPOSED PROFILE S.B.I. RTE. 3
 (@ E. Rdwy.)

DESIGNED	Paul Barnett	EXAMINED	John Edmund
CHECKED	John Edmund	PASSED	John Edmund
DRAWN	R. Barnett	APPROVED	John Edmund
CHECKED	John Edmund		

WATERWAY INFORMATION

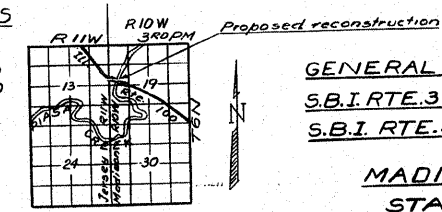
Drainage Area 102.2 Sq. Miles
 Existing Opening 2345 Sq. Ft.
 Required Opening 2345 Sq. Ft.
 Proposed Opening 2345 Sq. Ft.
 Design H.W.E. = 435.70
 Q(30) = 17,500 cfs.
 Created Head = 4.71'
 H.W.E.(100) = 437.4
 Q = 24,850 cfs
 Created Head = 2.79'

DESIGN STRESSES
 PRECAST PRESTRESSED UNITS

f'c = 5000 psi
 f'ci = 4000 psi
 f's = 270,000 psi (1/8" Strands)
 f'si = 189,000 psi (1/8" Strands)

FIELD UNITS

f'c = 1400 psi
 f's = 200,000 psi (Struct.)
 f'y = 60,000 psi (Reinf.)
 Loading HS20-44
 Allow 25% P/B for Future W.S.



LOCATION SKETCH

GENERAL PLAN & ELEVATION
 S.B.I. RTE. 3 OVER PIASA CREEK
 S.B.I. RTE. 3 SECTION 59 BR

MADISON COUNTY
 STA. 638+00.00

FOR INFORMATION ONLY

FILE NAME =
 c:\pw_work\pwwork\harbaughrd\dms52107.p

USER NAME = harbaughrd
 02106a.dgn
 PLOT SCALE = 50.0000' / IN.
 PLOT DATE = 1/23/2009

DESIGNED	-	REVISED	-
DRAWN	-	REVISED	-
CHECKED	-	REVISED	-
DATE	-	REVISED	-

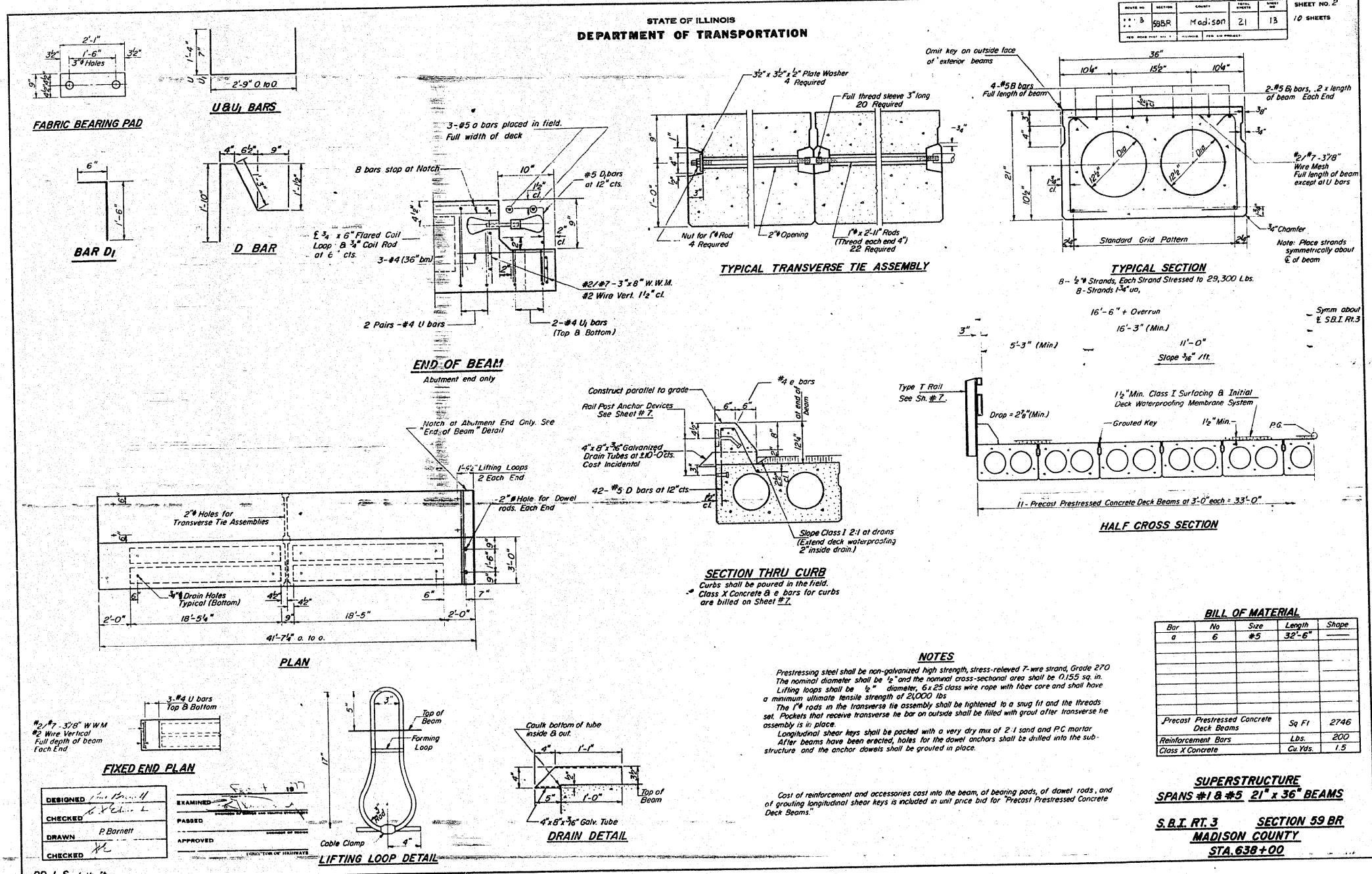
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EXISTING BRIDGE PLANS
 SCALE: SHEET NO. 1 OF 9 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
8956	59BR-1	MADISON	75	58
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 76B18	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
59BR	Madison	75	59

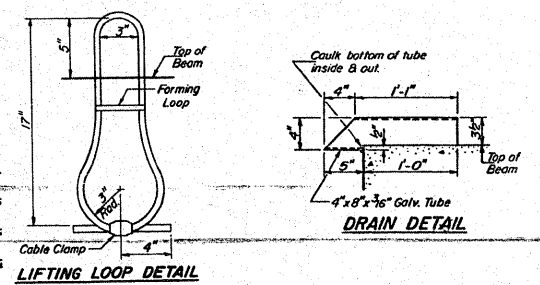
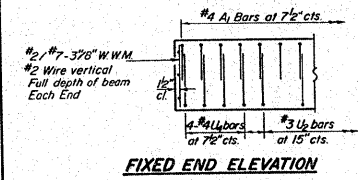
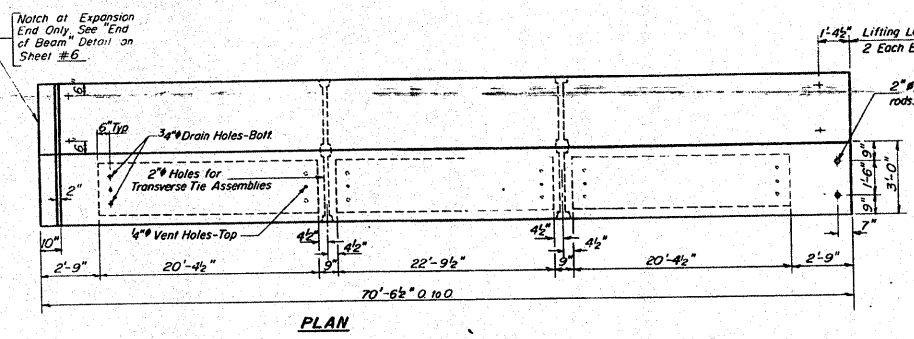
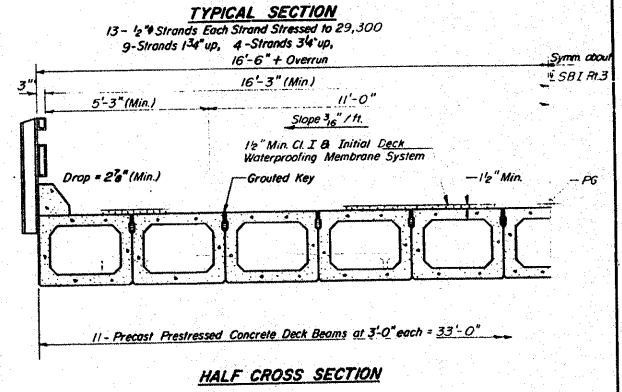
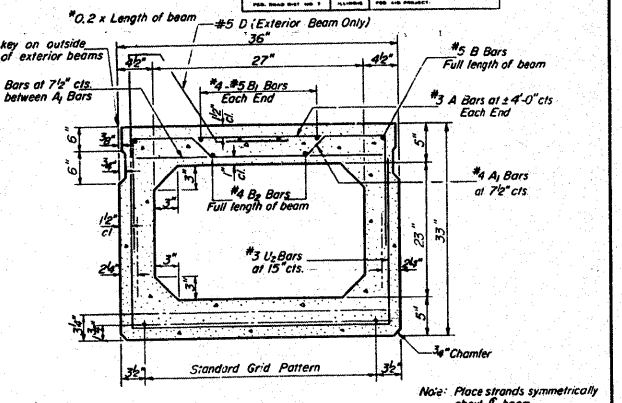
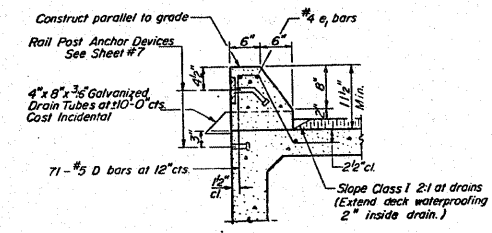
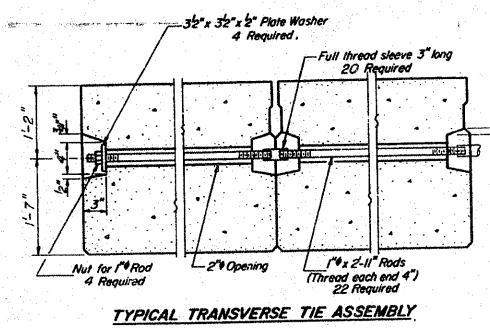
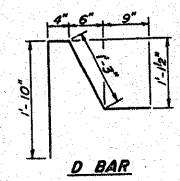
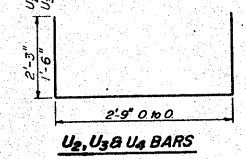
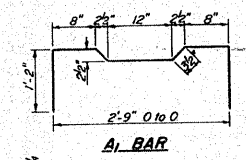
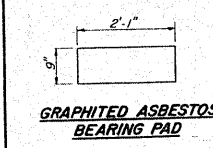
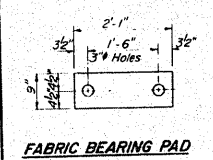


FOR INFORMATION ONLY

FILE NAME =	USER NAME = harbaughrd	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING BRIDGE PLANS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ce:\pw\work\pwidot\harbaughrd\dms52107\p	h02106a.dgn	DRAWN -	REVISED -			8956	59BR-1	MADISON	75	59	
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					
	PLOT DATE = 1/23/2009	DATE -	REVISED -			SCALE:	SHEET NO. 2 OF 9 SHEETS	STA.	TO STA.		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROJECT NO.	SECTION	COUNTY	SHEET NO.	TOTAL SHEETS
55BR	Madison	21	14	10 SHEETS



DESIGNED	Paul Bennett	EXAMINED	[Signature]
CHECKED	[Signature]	PASSED	[Signature]
DRAWN	P. Bennett	APPROVED	[Signature]
CHECKED	[Signature]	DIRECTOR OF HIGHWAYS	

NOTES
Prestressing steel shall be non-galvanized high strength, stress-relieved 7-wire strand, Grade 270. The nominal diameter shall be 1/2" and the nominal cross-sectional area shall be 0.155 sq. in. Lifting loops shall be 1/2" diameter, 6x25 class wire rope with fiber core and shall have a minimum ultimate tensile strength of 46,000 lbs. The 1/4" rods in the transverse tie assembly shall be tightened to a snug fit and the threads set. Pockets that receive transverse tie bar on outside shall be filled with grout after transverse tie assembly is in place. Longitudinal shear keys shall be packed with a very dry mix of 2:1 sand and P.C. mortar. After beams have been erected, holes for the dowel anchors shall be drilled into the sub-structure and the anchor dowels shall be grouted in place.

Cost of reinforcement and accessories cast into the beam, of bearing pads, of dowel rods, and of grouting longitudinal shear keys is included in unit price bid for "Precast Prestressed Concrete Deck Beams".

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a	3	#5	32'-6"	
Precast Prestressed Concrete Deck Beams	Sq Ft	2328		
Reinforcement Bars	Pounds	100		
Class X Concrete	Cu Yds.	8		

SUPERSTRUCTURE
SPAN#2 33" x 36" BEAMS
S.B.I. RT. 3 SECTION 59 BR
MADISON COUNTY
STA. 638+00

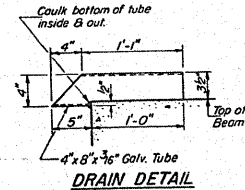
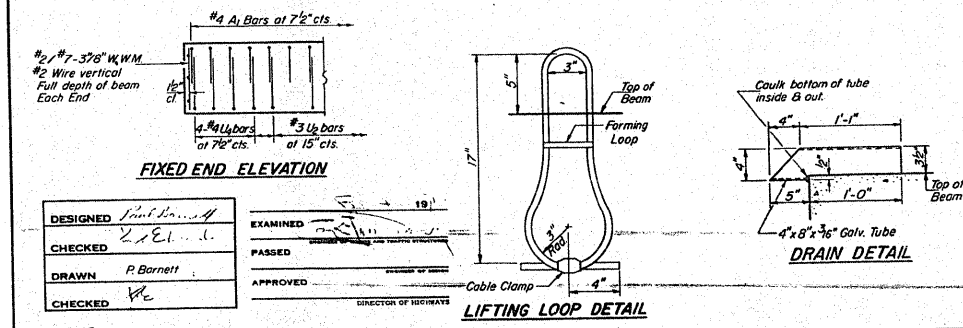
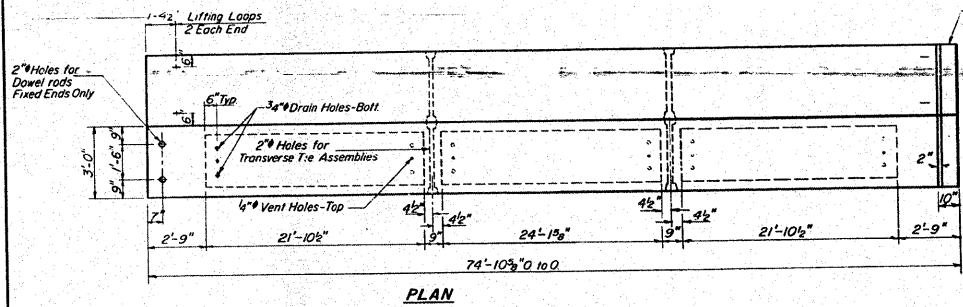
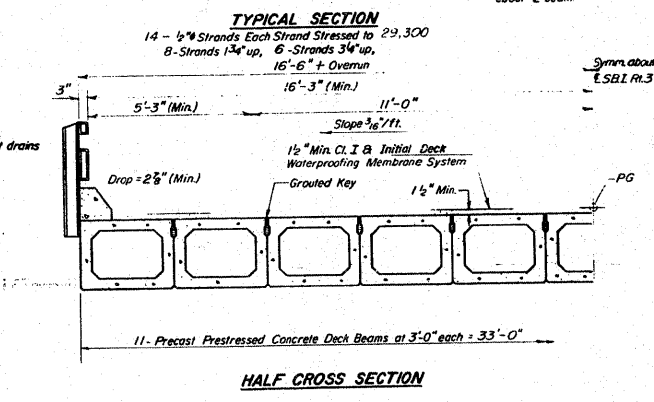
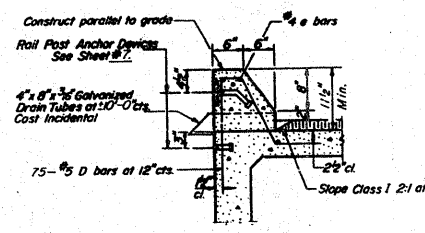
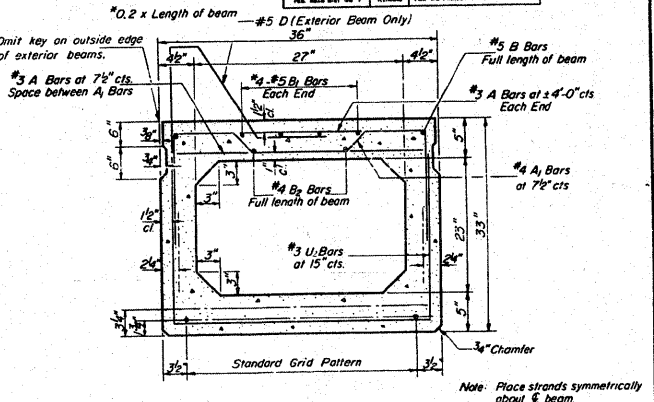
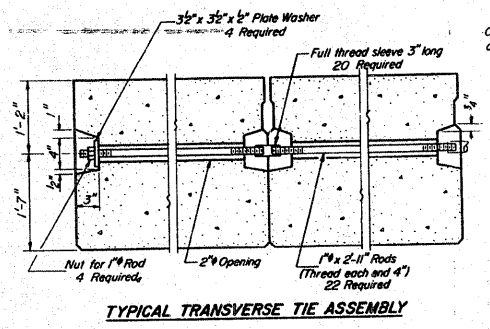
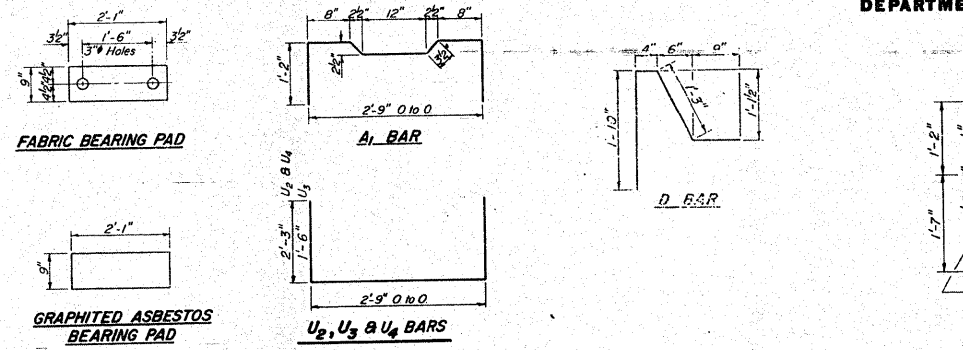
PD-3-S 7-15-75

FOR INFORMATION ONLY

FILE NAME =	USER NAME = harbaughrd	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING BRIDGE PLANS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\pwi\dot\harbaughrd\dms52107\p02106a.dgn	DRAWN -	REVISED -	8956			59BR-1	MADISON	75	60	
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -	CONTRACT NO. 76B18							
PLOT DATE = 1/23/2009	DATE -	REVISED -	SCALE:			SHEET NO. 3 OF 9 SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	(ILLINOIS) FED. AID PROJECT	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SECTION	COUNTY	SHEET NO.	TOTAL SHEETS
59 BR	MADISON	21	15



NOTES

Prestressing steel shall be non-galvanized high strength, stress-relieved 7-wire strand, Grade 270. The nominal diameter shall be 1/2" and the nominal cross-sectional area shall be 0.155 sq. in. Lifting loops shall be 3/4" diameter, 6 x 25 class wire rope with fiber core and shall have a minimum ultimate tensile strength of 45,000 lbs. The 1" rods in the transverse tie assembly shall be tightened to a snug fit and the threads set. Pockets that receive transverse tie bar on outside shall be filled with grout after transverse tie assembly is in place. Longitudinal shear keys shall be packed with a very dry mix of 2-1 sand and P.C. mortar. After beams have been erected, holes for the dowel anchors shall be drilled into the sub-structure and the anchor dowels shall be grouted in place.

Cost of reinforcement and accessories cast into the beam, of bearing pads, of dowel rods and of grouting longitudinal shear keys is included in unit price bid for "Precast Prestressed Concrete Deck Beams."

BILL OF MATERIAL

Bar	No	Size	Length	Shape
a	3	#5	32'-6"	
Precast Prestressed Concrete Deck Beams	Sq Ft.	2471		
Reinforcement Bars	Pounds	100		
Glass x Concrete	Cu Yds.	.8		

SUPERSTRUCTURE
SPAN #3 33" x 36" BEAMS
S.B.I. RT. 3 SECTION 59 BR
MADISON COUNTY
STA. 638+00

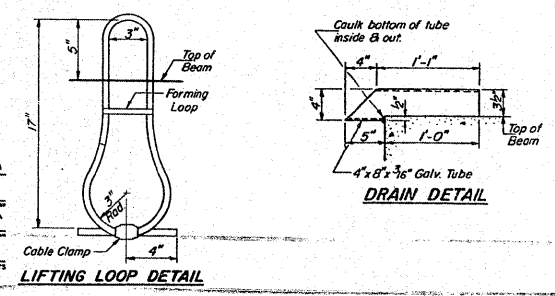
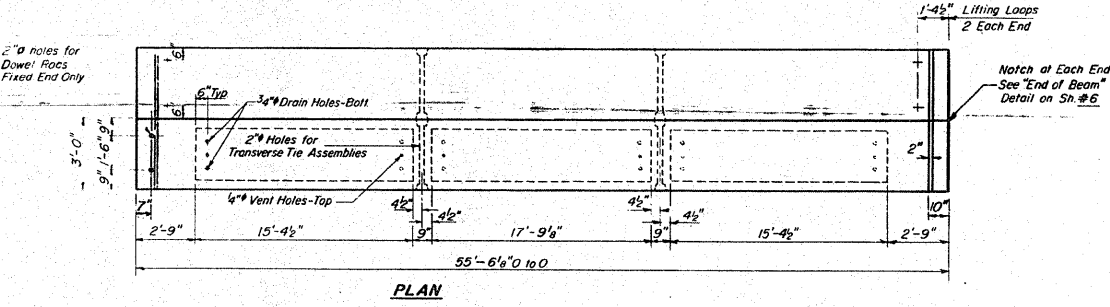
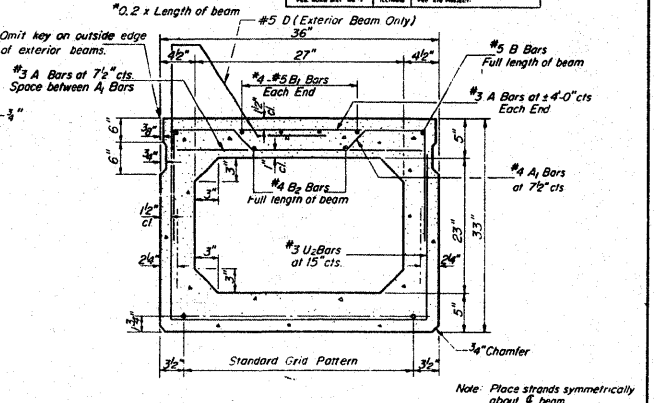
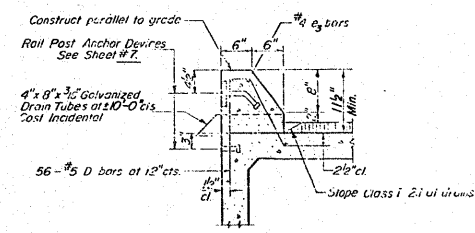
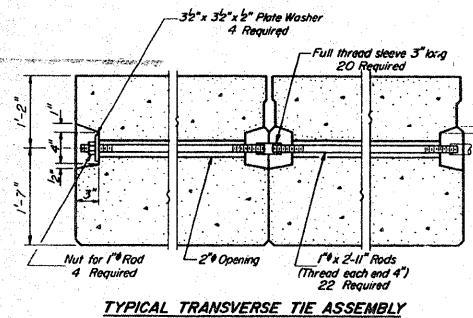
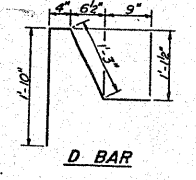
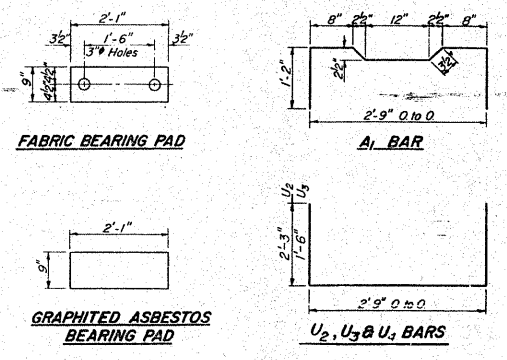
DESIGNED	PAUL BARNETT
CHECKED	P. BARNETT
DRAWN	P. BARNETT
CHECKED	P. BARNETT

EXAMINED	PAUL BARNETT
PASSED	PAUL BARNETT
APPROVED	PAUL BARNETT

FOR INFORMATION ONLY

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROJECT NO.	SECTION	COUNTY	SHEETS	SHEET NO.
8956	59BR	Madison	21	16
10 SHEETS				



DESIGNED: Paul K...
CHECKED: K...
DRAWN: P. Barnett
CHECKED: Y/C

EXAMINED: [Signature]
PASSED: [Signature]
APPROVED: [Signature]
DIRECTOR OF HIGHWAYS

PD-3-S 7-15-75

NOTES
Prestressing steel shall be non-galvanized high strength, stress-relieved 7-wire strand, Grade 270. The nominal diameter shall be 1/2" and the nominal cross-sectional area shall be 0.155 sq. in. Lifting loops shall be 3/4" diameter, 6 x 25 class wire rope with fiber core and shall have a minimum ultimate tensile strength of 33,000 lbs. The 1" rods in the transverse tie assembly shall be tightened to a snug fit and the threads set. Pockets that receive transverse tie bar on outside shall be filled with grout after transverse tie assembly is in place. Longitudinal shear keys shall be packed with a very dry mix of 2-1 sand and P.C. mortar. After beams have been erected, holes for the dowel anchors shall be drilled into the sub-structure and the anchor dowels shall be grouted in place.

Cost of reinforcement and accessories cast into the beam, of bearing pads, of dowel rods, and of grouting longitudinal shear keys is included in unit price bid for "Precast Prestressed Concrete Deck Beams."

BILL OF MATERIAL

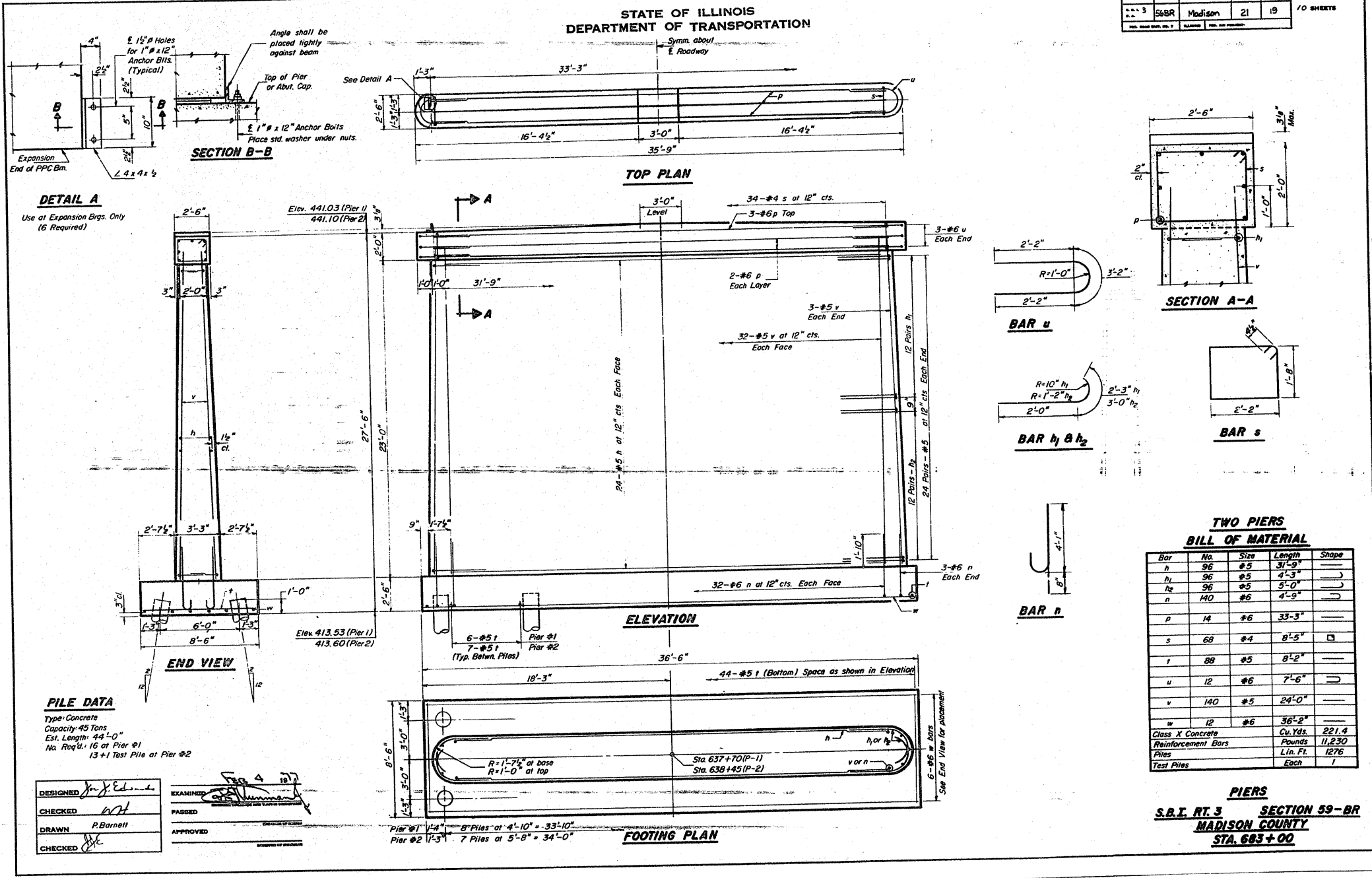
Bar	No	Size	Length	Shape	
a	6	#5	32'-6"		
Precast Prestressed Concrete Deck Beams					
				Sq Ft	1832
Reinforcement Bars				Pounds	200
Class X Concrete				Cu Yds	1.5

SUPERSTRUCTURE
SPAN #4 33" x 36" BEAMS
S.B.I. RT. 3 SECTION 59 BR
MADISON COUNTY
STA. 638+00

FOR INFORMATION ONLY

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROJECT NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
56BR	Madison	21	19	10



TWO PIERS
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h	96	#5	31'-9"	—
h ₁	96	#5	4'-3"	—
h ₂	96	#5	5'-0"	—
n	140	#6	4'-9"	—
p	14	#6	33'-3"	—
s	68	#4	8'-5"	□
i	88	#5	8'-2"	—
u	12	#6	7'-6"	—
v	140	#5	24'-0"	—
w	12	#6	36'-2"	—
Class X Concrete			Cu. Yds.	221.4
Reinforcement Bars			Pounds	11,230
Piles			Lin. Ft.	1276
Test Piles			Each	1

PIERS
S.B.I. RT. 3 SECTION 59-BR
MADISON COUNTY
STA. 683+00

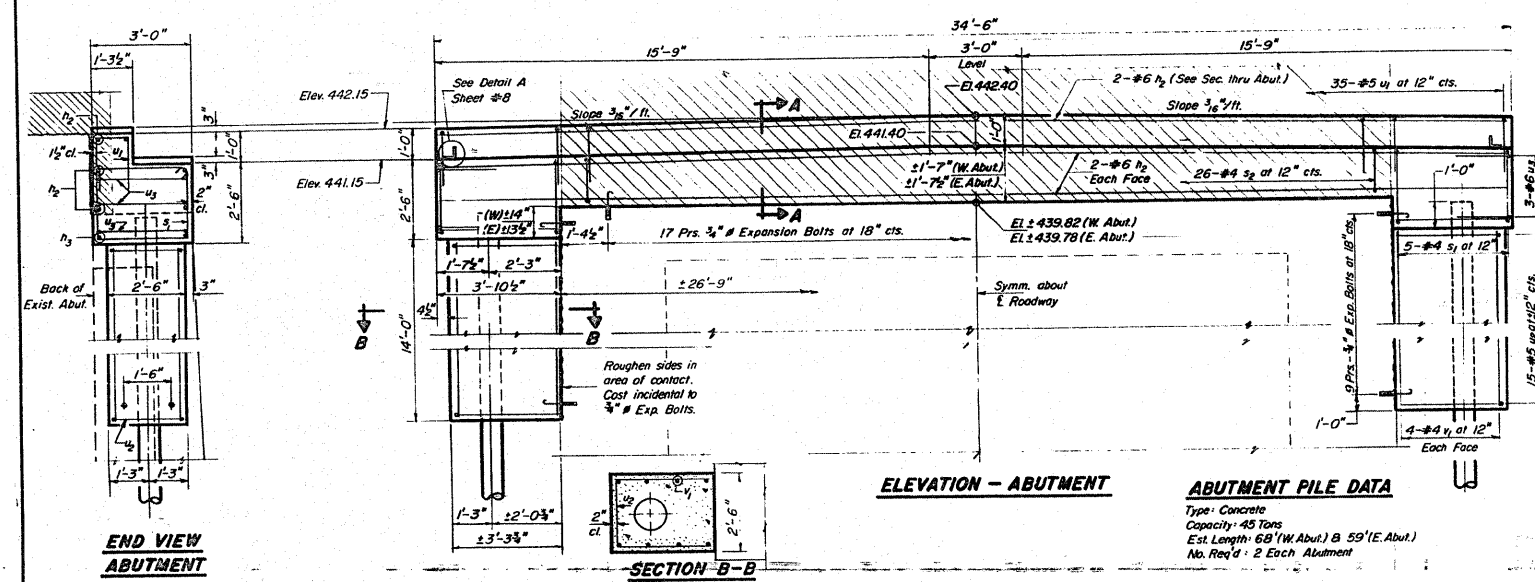
FOR INFORMATION ONLY

FILE NAME =	USER NAME = harbaughrd	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING BRIDGE PLANS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ct:\pw_work\pwr\dot\harbaughrd\dms52107\p	#2106a.dgn	DRAWN -	REVISED -			8956	59BR-1	MADISON	75	63	
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 76B18					
	PLOT DATE = 1/23/2009	DATE -	REVISED -			FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROJECT NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
54BR	Madison	21	20	10

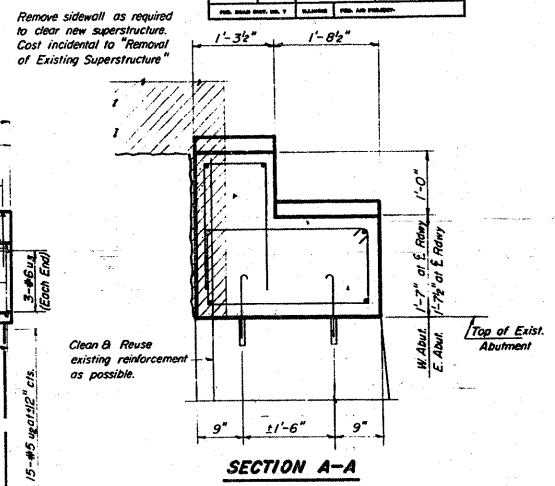
Hatched area to be removed prior to constructing new cap.
Cost of concrete removed shall be incidental to "Removal of Existing Superstructure."



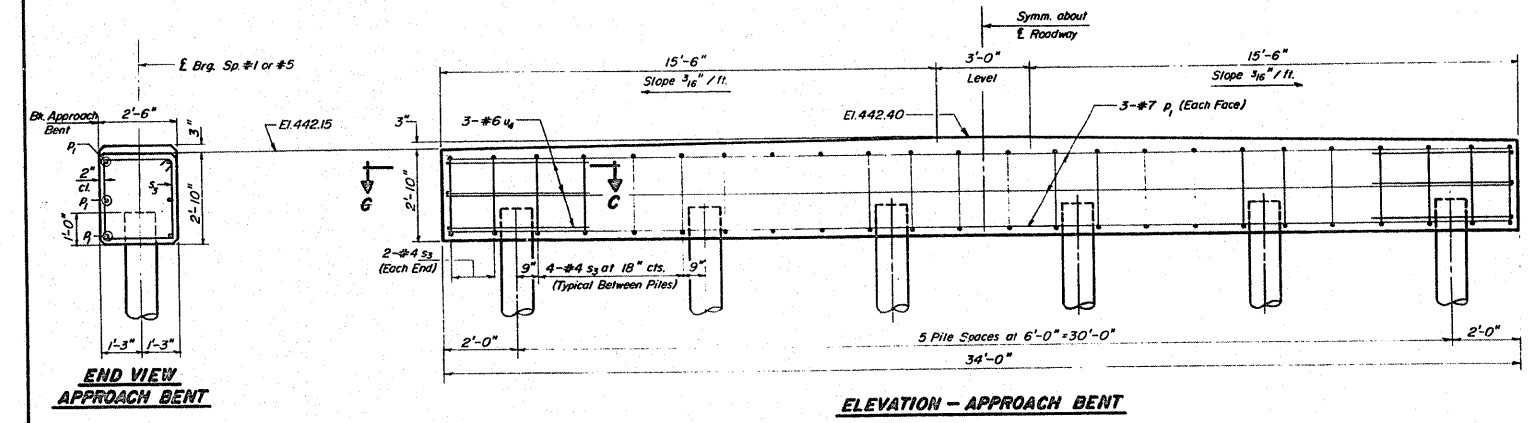
ELEVATION - ABUTMENT

ABUTMENT PILE DATA

Type: Concrete
Capacity: 43 Tons
Est. Length: 68' (W. Abut.) & 59' (E. Abut.)
No. Req'd: 2 Each Abutment



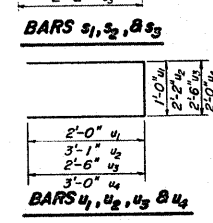
SECTION A-A



ELEVATION - APPROACH BENT

APPROACH BENT PILE DATA

Type: Concrete
Capacity: 30 Tons
Est. Length: 47' (W. Appra.) & 52' (E. Appra.)
No. Req'd: 6 (W. Appra.)
5 + 1 Test Pile (E. Appra.)



TWO ABUTMENTS
TWO APPROACH BENTS
BILL OF MATERIAL

Bar	No.	Size	Length	Shape	
ds	12	#6	34'-3"	—	
s1	20	#4	10'-5"	□	
s2	52	#4	8'-5"	□	
u1	70	#5	5'-0"	L	
u2	60	#5	8'-4"	L	
u3	12	#6	7'-6"	L	
u4	32	#4	14'-9"	—	
Class X Concrete				Cu. Yds.	32.6
Reinforcement Bars				Pounds	2390
Expansion Bolts (3/4")				Each	140
Concrete Piles				Lin. Ft.	254

Bar	No.	Size	Length	Shape	
ps	12	#7	33'-8"	—	
s3	48	#4	10'-1"	□	
u4	12	#6	8'-0"	L	
Class X Concrete				Cu. Yds.	18.3
Reinforcement Bars				Pounds	1290
Concrete Piles				Lin. Ft.	542
Test Piles (Concrete)				Each	1

DESIGNED	J. Edwards	EXAMINED	[Signature]
CHECKED	RAH	PASSED	[Signature]
DRAWN	R. Barnett	APPROVED	[Signature]
CHECKED	[Signature]		

SECTION C-C

ABUTMENTS &
APPROACH BENTS
SBI RT 3 SECTION 59 BR
MADISON COUNTY
STA. 638+00

FOR INFORMATION ONLY

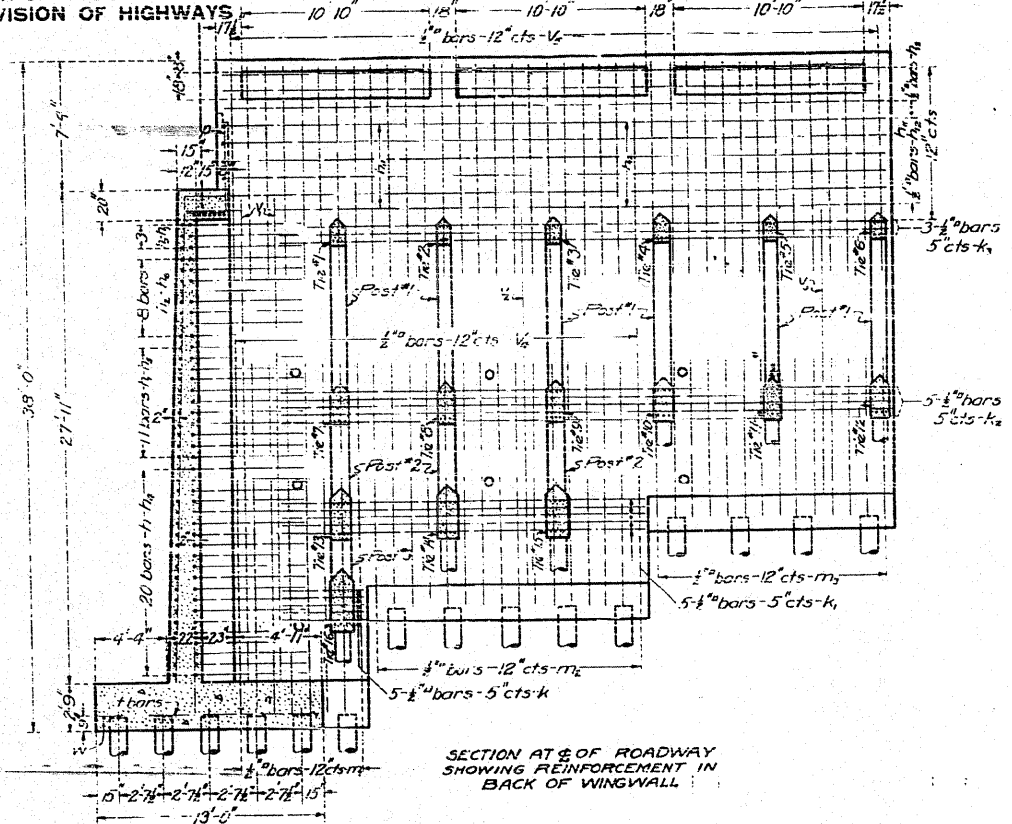
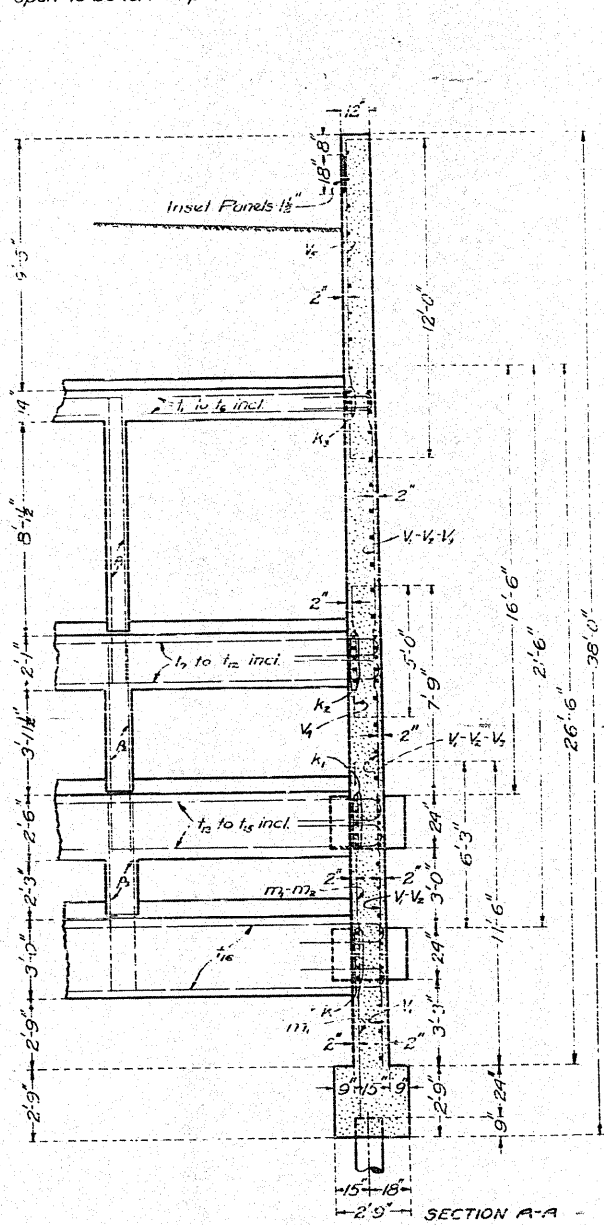
FILE NAME =	USER NAME = herbaughrd	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING BRIDGE PLANS	F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cd:\pw\work\p\dot\herbaughrd\dms52107\p\2106a.dgn		DRAWN -	REVISED -			8956	59BR-1	MADISON	75	64
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -			CONTRACT NO. 76B18				
PLOT DATE = 1/23/2009		DATE -	REVISED -			SCALE:	SHEET NO. 7 OF 9 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.

B.M. - See road plans
 New bridge on relocation.
 Present steel thru truss-80ft.
 span-to be left in place.

STATE OF ILLINOIS
 DEPARTMENT OF PUBLIC WORKS & BUILDINGS
 DIVISION OF HIGHWAYS

ROAD ISSUE	COUNTY	SEC.	TOTAL LENGTH
3	MADISON	1	64.63

SHEET NO. 3
 3 SHEETS



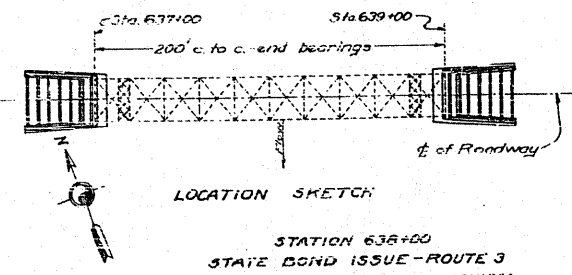
SECTION AT 1/2 OF ROADWAY
 SHOWING REINFORCEMENT IN
 BACK OF WINGWALL

BILL OF MATERIAL

Bar	No.	Size	Length	1	12	3/8"	33'-0"
V	60	1/2"	27'-6"	1/2	12	"	33'-6"
V	48	"	26'-6"	1/2	12	"	34'-6"
V	60	"	27'-6"	1/2	12	"	33'-0"
V	60	"	16'-6"	1/2	12	"	36'-0"
V	56	"	5'-0"	1/2	12	"	36'-6"
V	152	"	12'-0"	1/2	12	3/8"	34'-0"
n	40	1/2"	33'-6"	1/2	12	"	34'-6"
n	22	3/8"	32'-6"	1/2	12	"	36'-0"
n	16	3/8"	31'-6"	1/2	12	"	37'-0"
n	6	3/8"	30'-6"	1/2	12	"	37'-6"
n	60	1/2"	13'-0"	1/2	15	"	34'-0"
n	44	1/2"	13'-0"	1/2	16	"	34'-6"
n	32	3/8"	13'-0"	1/2	16	"	35'-0"
n	12	3/8"	13'-0"	1/2	16	"	34'-0"
n	8	1/2"	24'-6"	1/2	16	"	34'-0"
n	64	"	10'-6"	m	52	1/2"	4'-0"
n	32	"	18'-6"	m	32	"	13'-6"
n	104	"	22'-0"	m	64	"	7'-6"
n	128	"	20'-0"	m	56	"	9'-0"
K	20	1/2"	4'-6"	D	50	1/2"	7'-6"
K	20	"	23'-0"	d	14	1/2"	25'-0"
K	40	"	13'-0"	+	112	3/8"	12'-3"
K	24	"	10'-6"	w	8	1/2"	14'-6"
D	96	1/2"	11'-0"				
D	48	"	8'-0"				
D	16	"	7'-0"				

Rein. Steel-lbs. 46600
 Concrete-cu yds. 4710

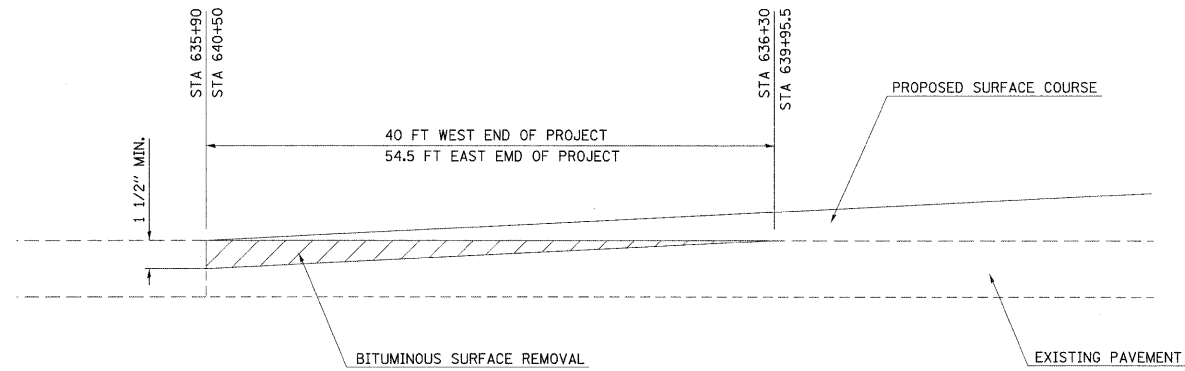
NOTE -
 All reinforcing steel shall be securely wired
 in place before concrete is poured.
 Class A concrete shall be used thruout.
 Proportions 1:2:4.
 15 ton piles-untreated-30 required
 12" butt-10" tip
 15 ton piles-creosoted-36 required
 12" butt-10" tip
 10 ton piles-creosoted-24 required
 10" butt-5" tip



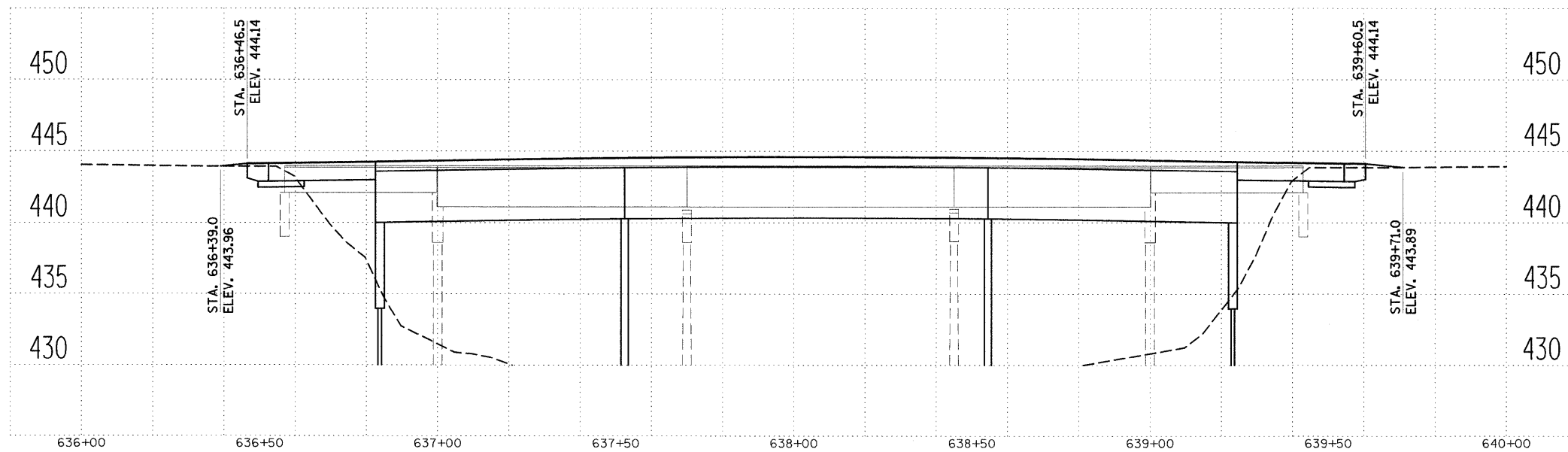
LOCATION SKETCH
 STATION 638+00
 STATE ROAD ISSUE-ROUTE 3
 SECTION 53-B-MADISON COUNTY
 8 C

COMPUTED	BY	EXAMINED	DATE
CHECKED	BY	APPROVED	DATE
DRAWN	BY		
CHECKED	BY		
ASSEMBLED	BY		
CHECKED	BY		

FOR INFORMATION ONLY



BUTT JOINT DETAIL
DETAIL 'A'

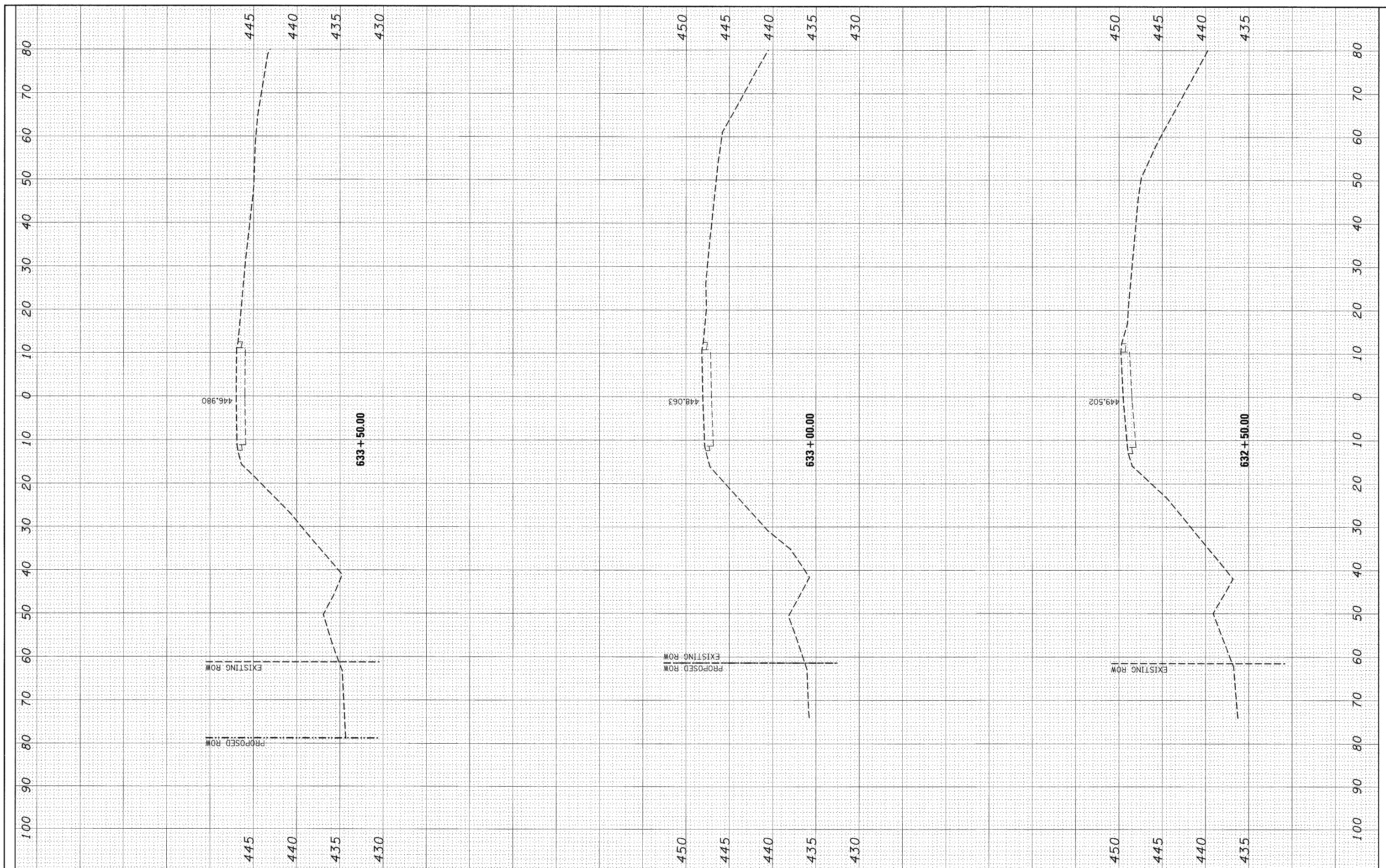


STAGING RAMP DETAIL

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								FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					

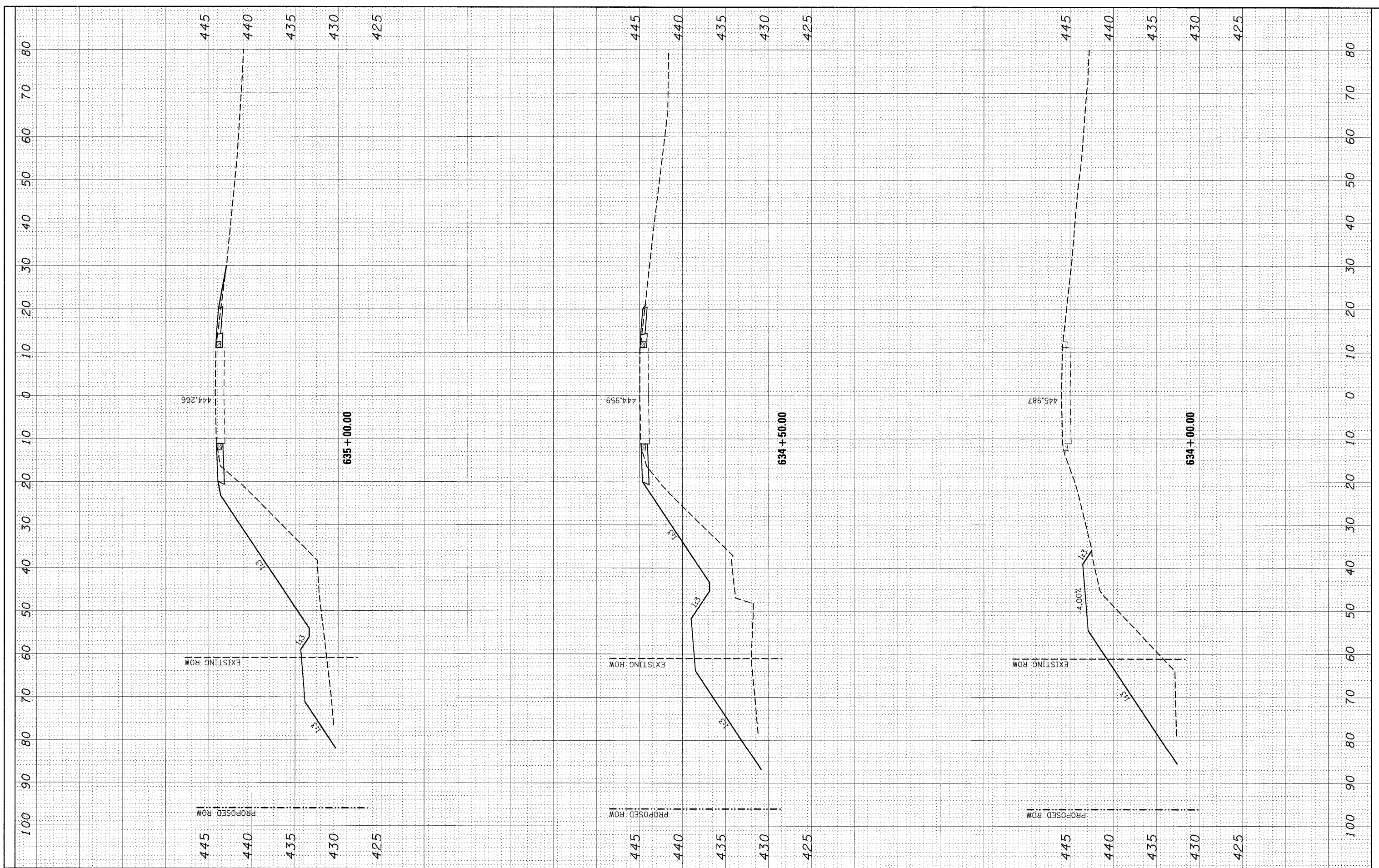
FINAL SURVEY NO.	SURVEYED BY	DATE
NOTE BOOK NO.	PLOTTED TEMPLATE	AREAS CHECKED

ORIGINAL SURVEY NO.	SURVEYED BY	DATE
NOTE BOOK NO.	PLOTTED TEMPLATE	AREAS CHECKED



FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		



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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

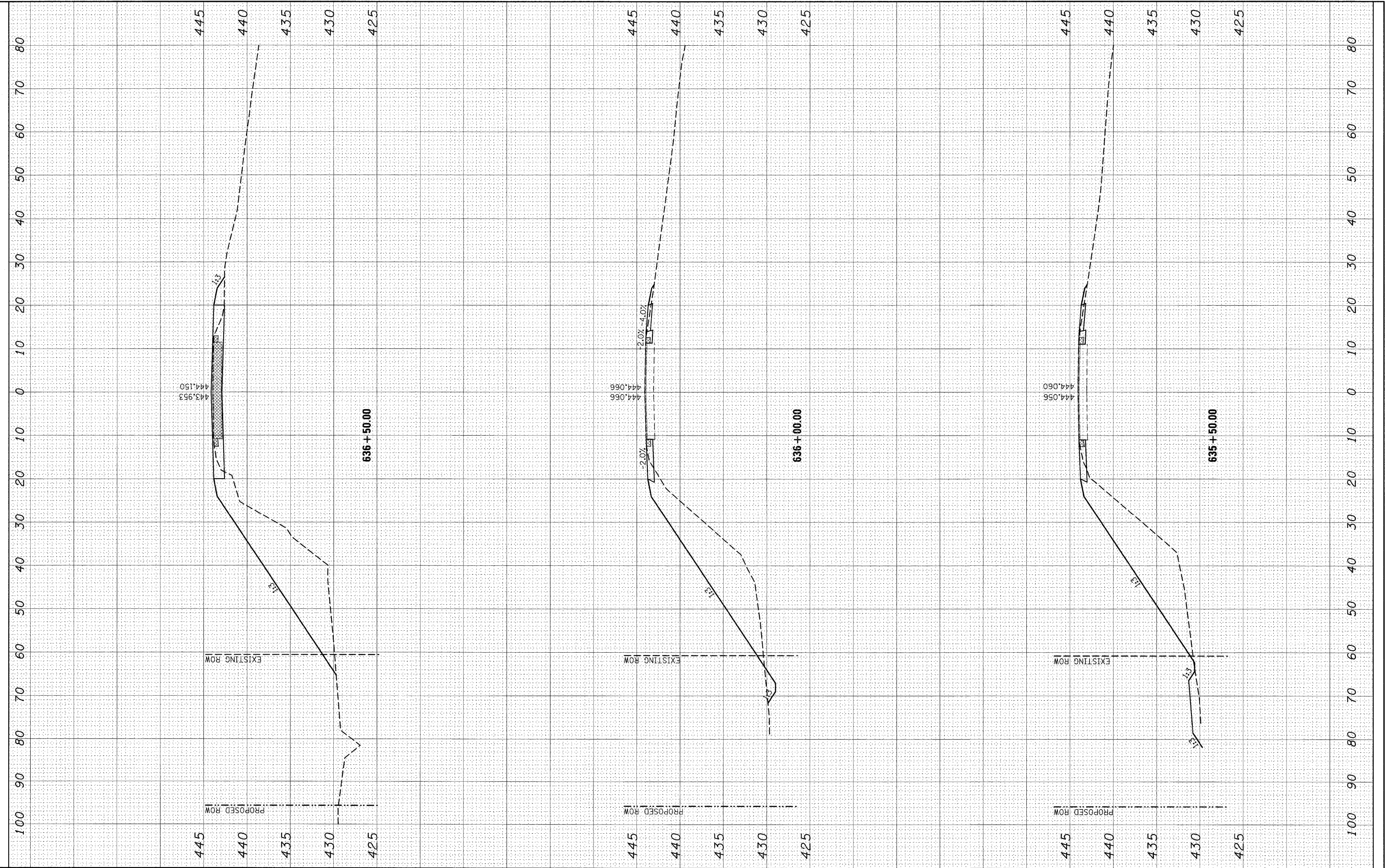
CROSS SECTIONS

SCALE: SHEET NO. 2 OF 8 SHEETS STA. 634+00 TO STA. 635+00

F.A.U. RTE. 8956	SECTION 59BR-1	COUNTY MADISON	TOTAL SHEETS 75	SHEET NO. 69
CONTRACT NO. 76B18				
ILLINOIS FED. AID PROJECT				

DATE	BY	SURVEYED	PLOTTED
		NOTE BOOK	TEMPLATE
		AREAS CHECKED	

DATE	BY	SURVEYED	PLOTTED
		NOTE BOOK	TEMPLATE
		AREAS CHECKED	



FILE NAME = #FILE#

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

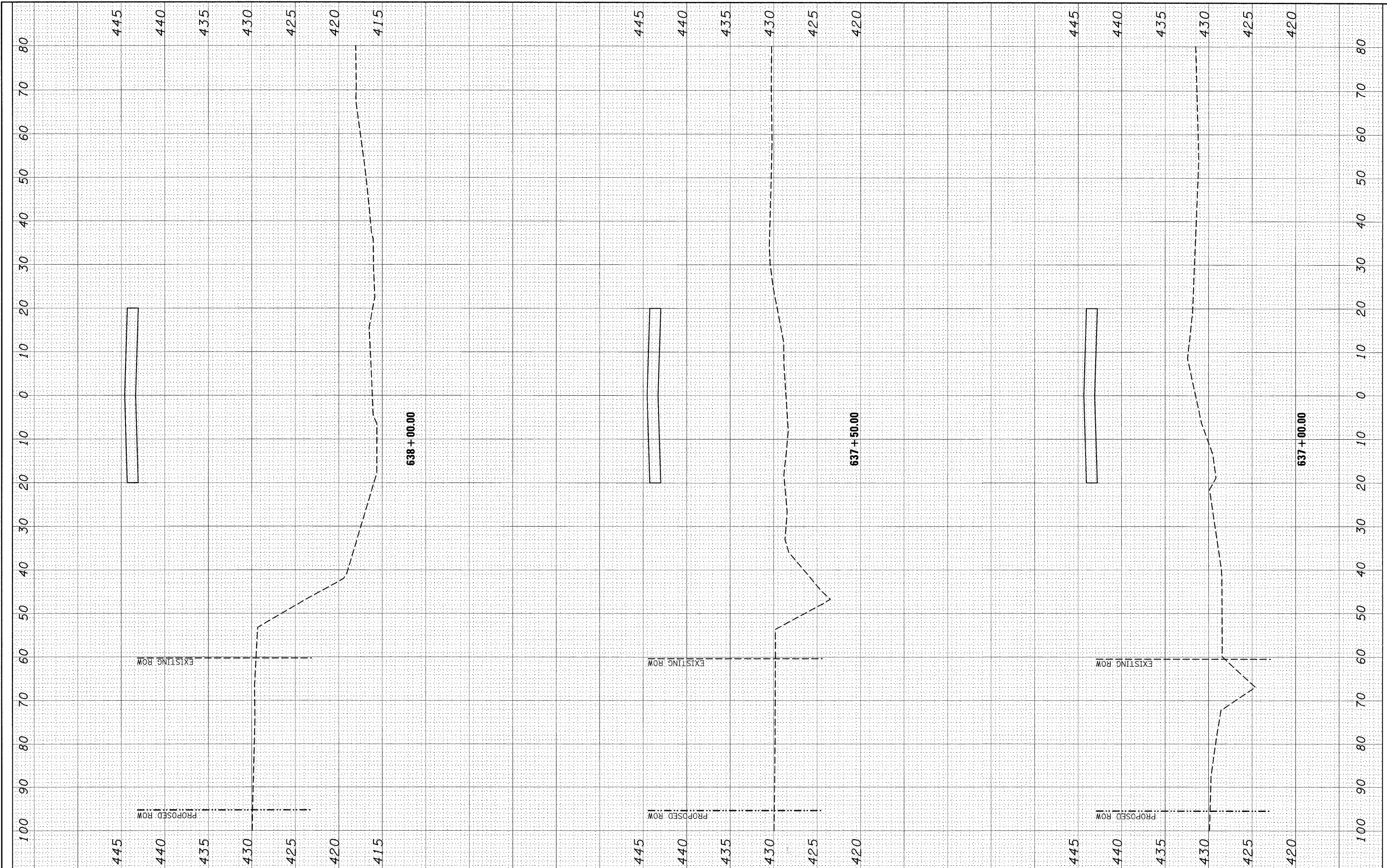
CROSS SECTIONS

SCALE: SHEET NO. 3 OF 8 SHEETS STA. 635+50 TO STA. 636+50

F.A.U. R.T.E. 8956	SECTION 59BR-1	COUNTY MADISON	TOTAL SHEETS 75	SHEET NO. 70
CONTRACT NO. 76B18				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY	DATE
SURVEYED	BY
NOTE BOOK	
NO.	
AREAS CHECKED	

ORIGINAL SURVEY	DATE
SURVEYED	BY
NOTE BOOK	
NO.	
AREAS CHECKED	



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 PLOT DATE = 3/16/2009

DESIGNED -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

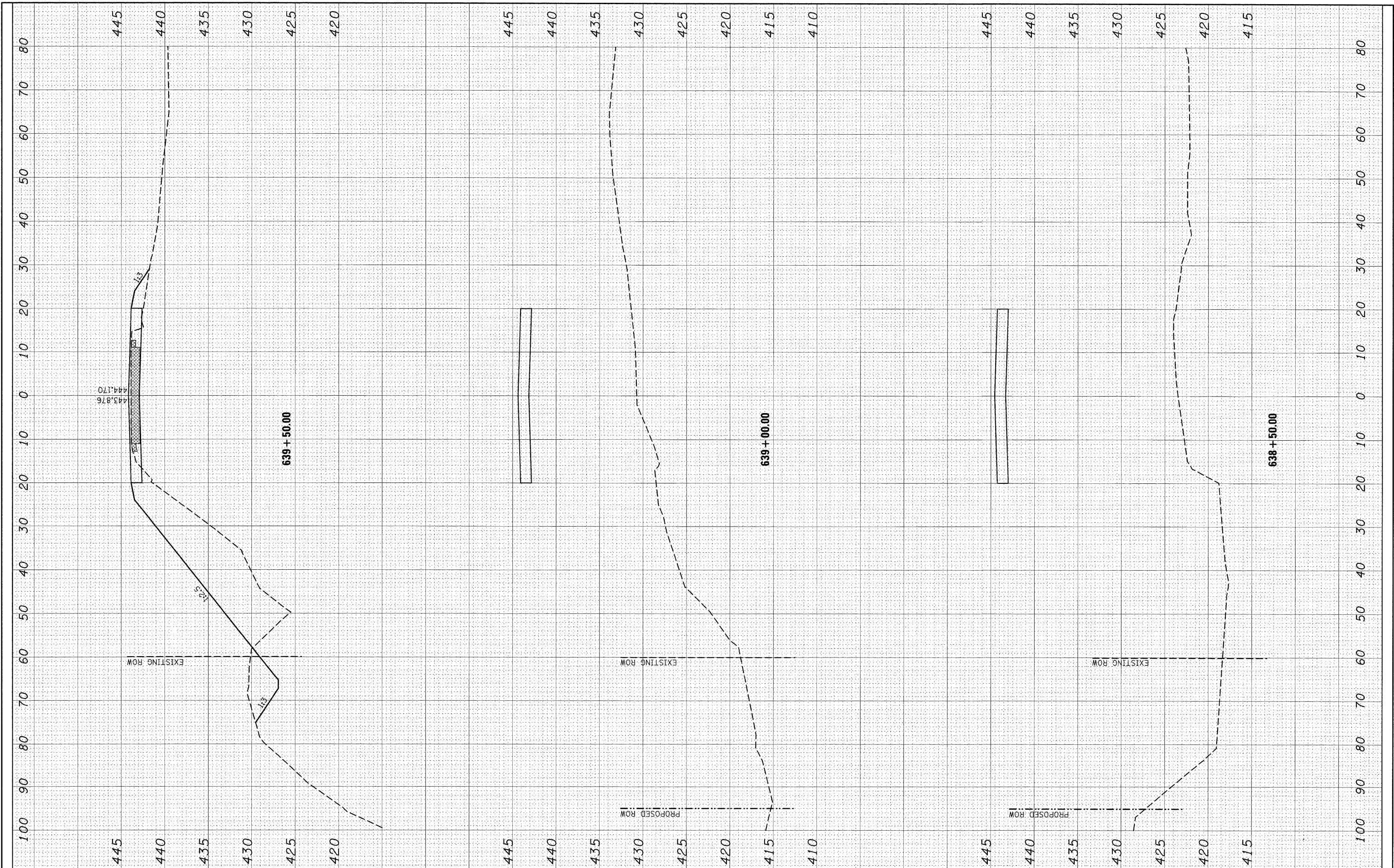
CROSS SECTIONS

SCALE: SHEET NO. 4 OF 8 SHEETS STA. 637+00 TO STA. 638+00

F.A.U. RTE. 8956	SECTION 59BR-1	COUNTY MADISON	TOTAL SHEETS 75	SHEET NO. 71
CONTRACT NO. 76B18				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY	BY	DATE
SURVEYED		
NOTE BOOK		
TEMPLATE		
AREAS		
AREAS CHECKED		
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SURVEYED		
NOTE BOOK		
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AREAS		
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REVISIONS
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

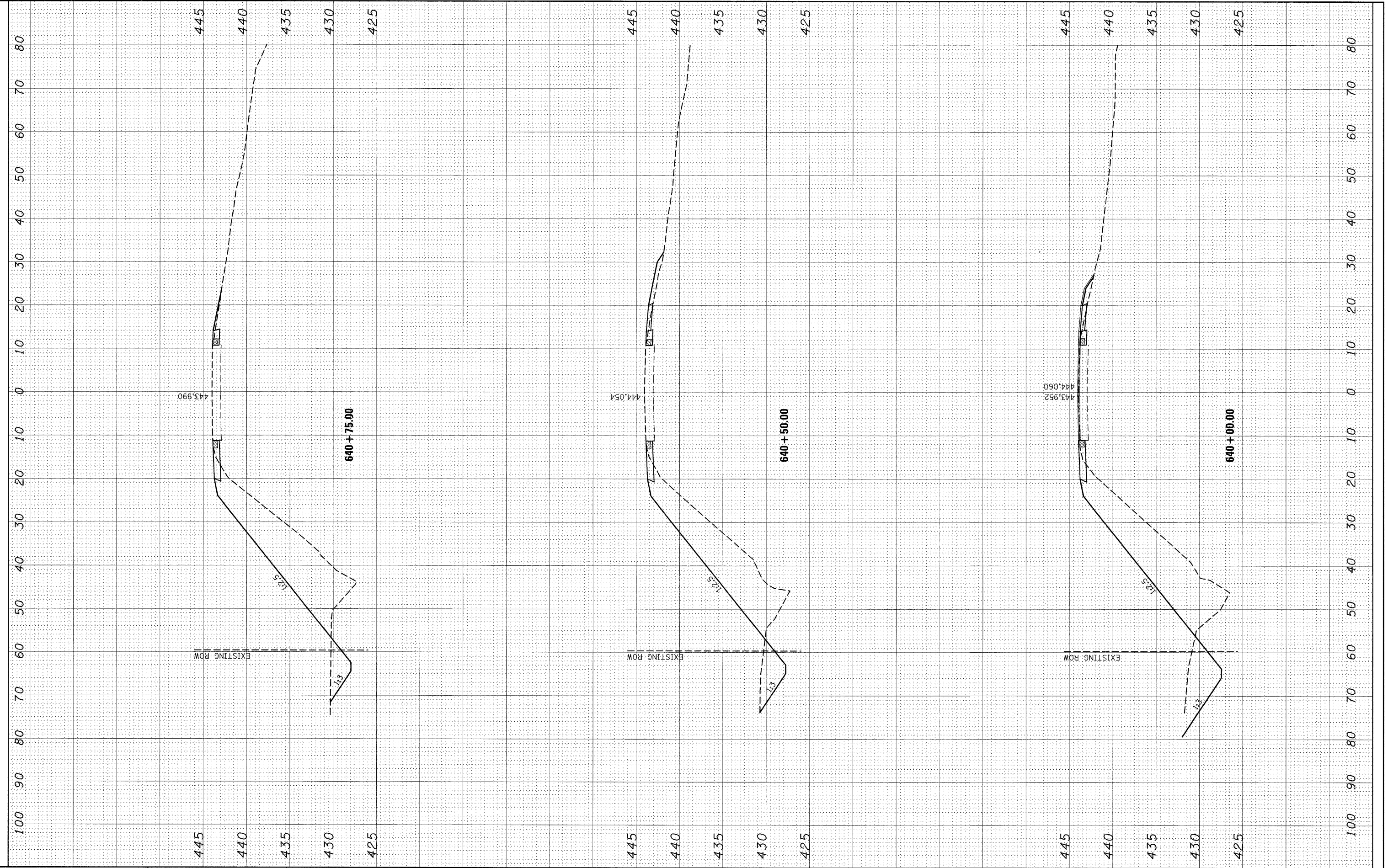
CROSS SECTIONS

SCALE: SHEET NO. 5 OF 8 SHEETS STA. 638+50 TO STA. 639+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
8956	59BR-1	MADISON	75	72
CONTRACT NO. 76818				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		



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DESIGNED -
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 DATE -

REVISED -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

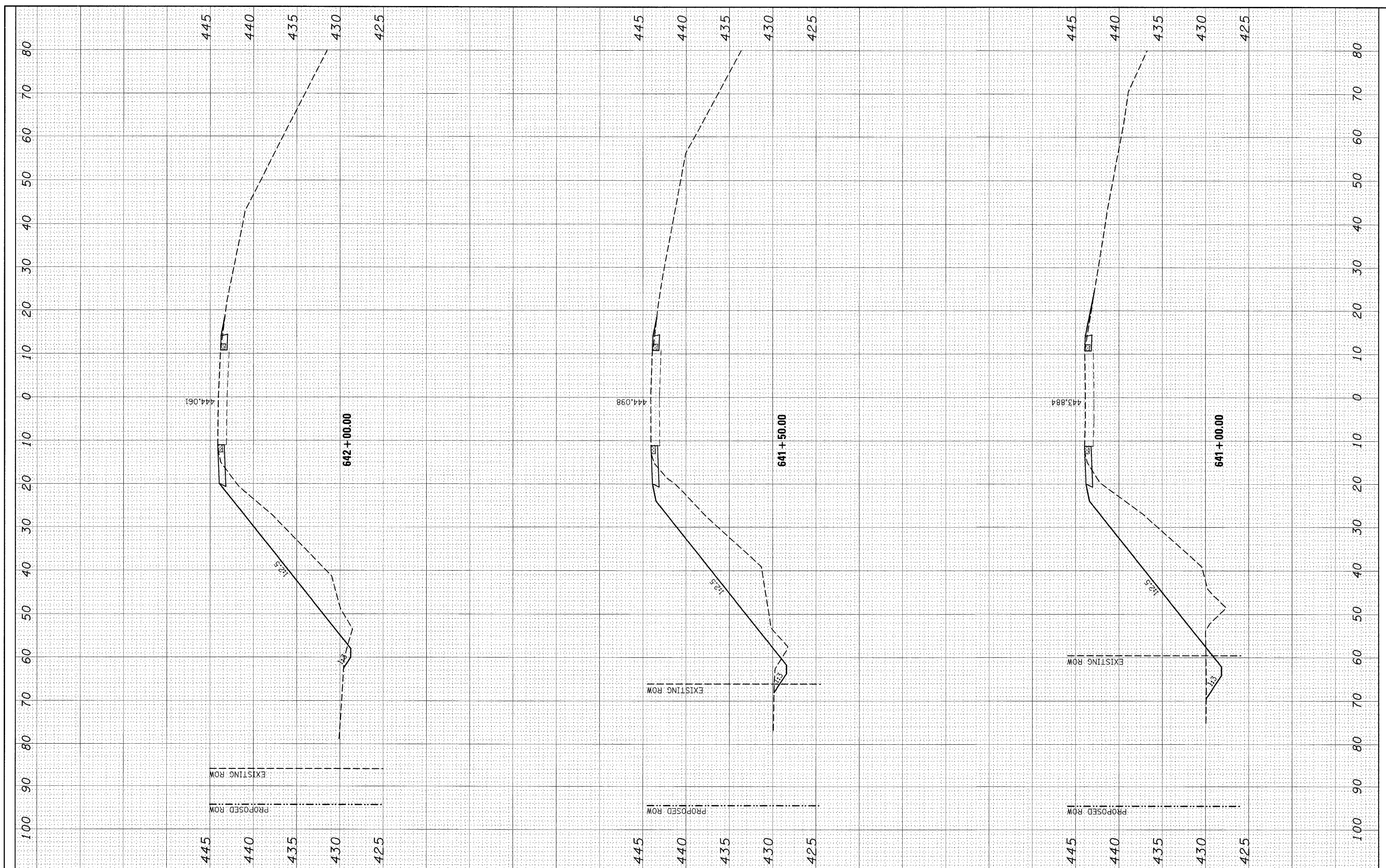
CROSS SECTIONS

SCALE: SHEET NO. 6 OF 8 SHEETS STA. 640+00 TO STA. 640+75

F.A.U. RTE. 8956	SECTION 59BR-1	COUNTY MADISON	TOTAL SHEETS 75	SHEET NO. 73
CONTRACT NO. 76B18				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
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	AREAS CHECKED		



FILE NAME = #FILEL#

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REVISIED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

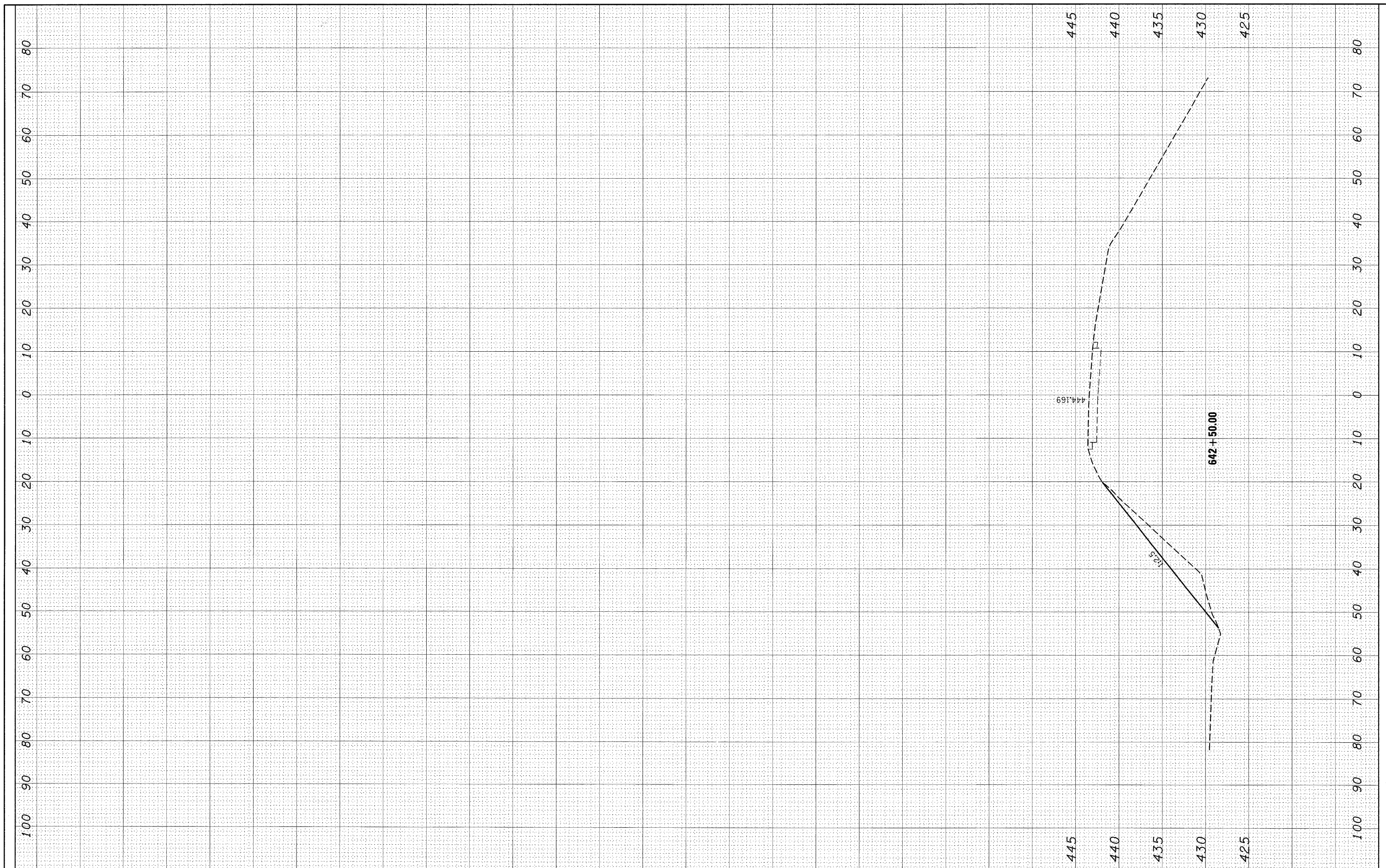
CROSS SECTIONS

SCALE: SHEET NO. 7 OF 8 SHEETS STA. 641+00 TO STA. 642+00

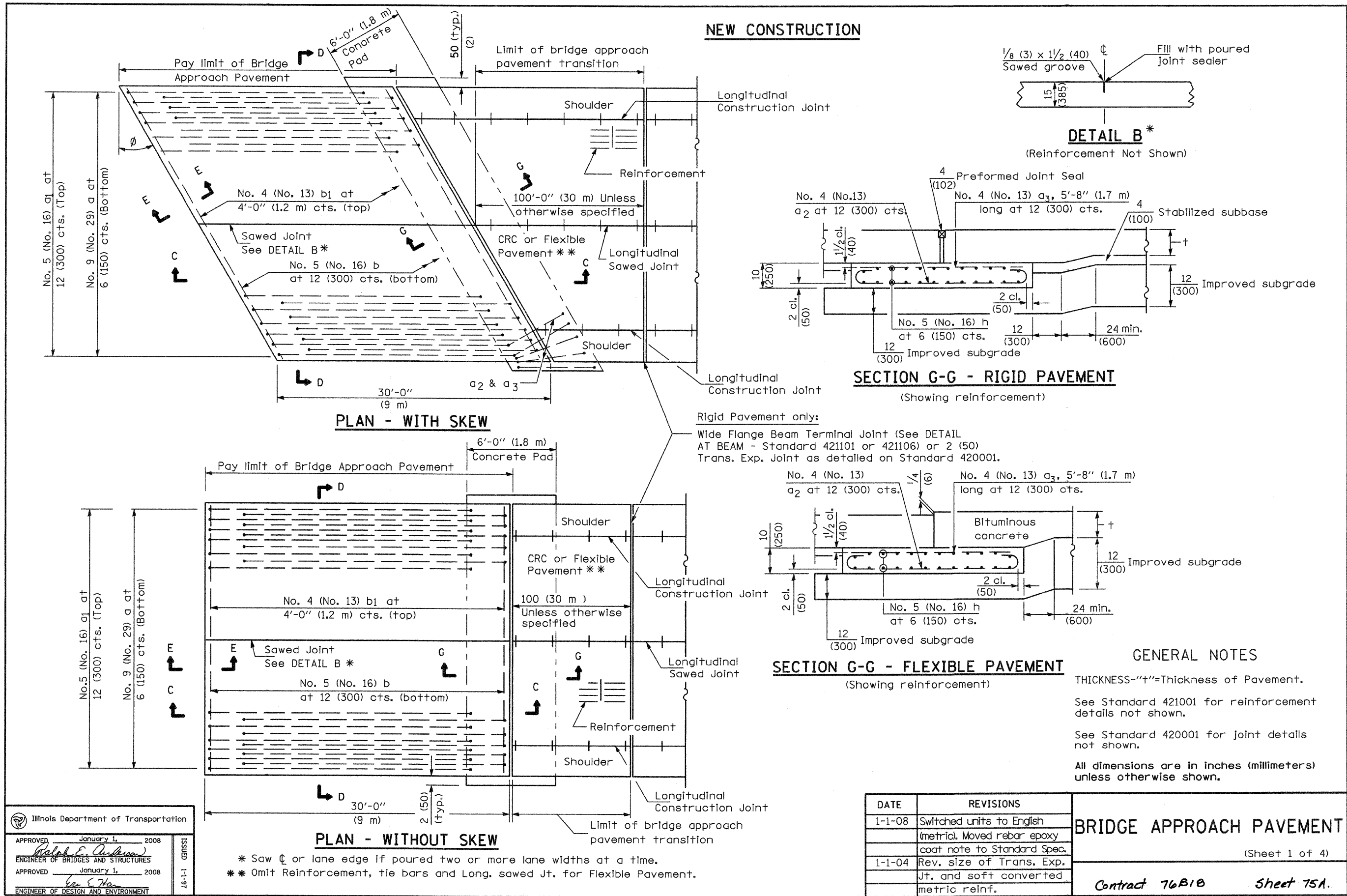
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CONTRACT NO. 76B18				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY NO.	SURVEYED	BY	DATE
	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY NO.	SURVEYED	BY	DATE
	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		



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	PLOT DATE = #DATE#	CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									



Illinois Department of Transportation

APPROVED January 1, 2008
Ralph E. Anderson
 ENGINEER OF BRIDGES AND STRUCTURES

APPROVED January 1, 2008
Ken E. Han
 ENGINEER OF DESIGN AND ENVIRONMENT

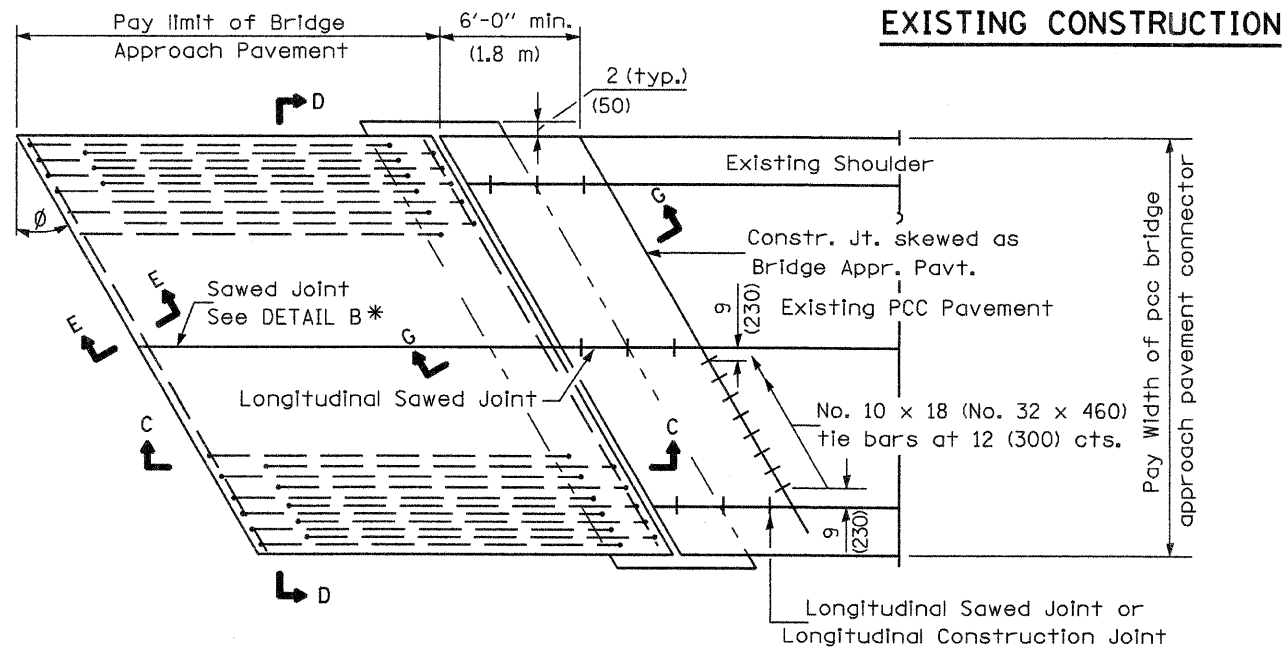
ISSUED 1-1-97

DATE	REVISIONS
1-1-08	Switched units to English (metric). Moved rebar epoxy coat note to Standard Spec.
1-1-04	Rev. size of Trans. Exp. Jt. and soft converted metric reinf.

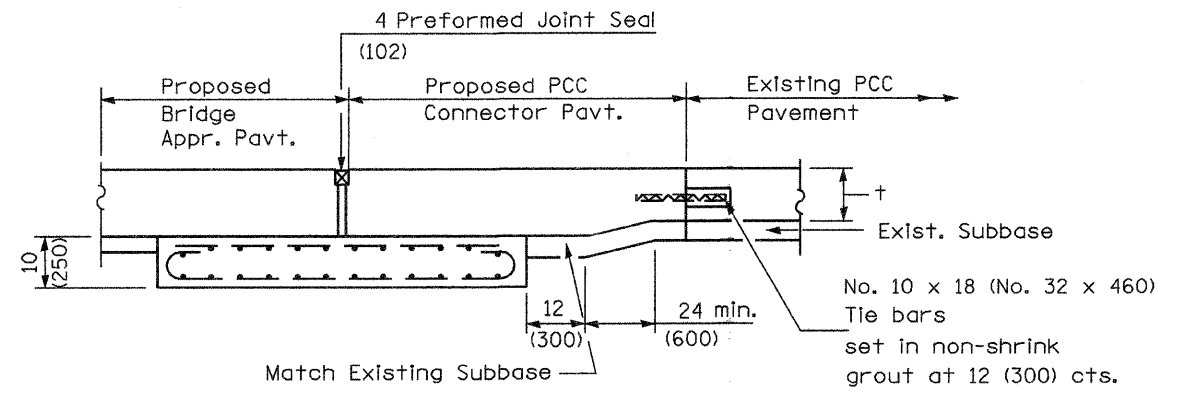
BRIDGE APPROACH PAVEMENT

(Sheet 1 of 4)

Contract 76B10 Sheet 75A.

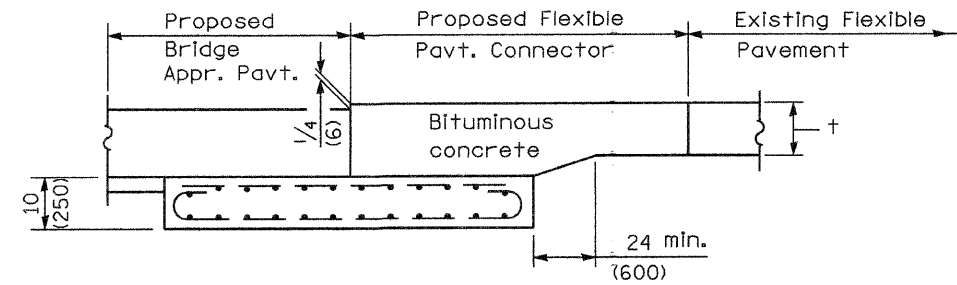
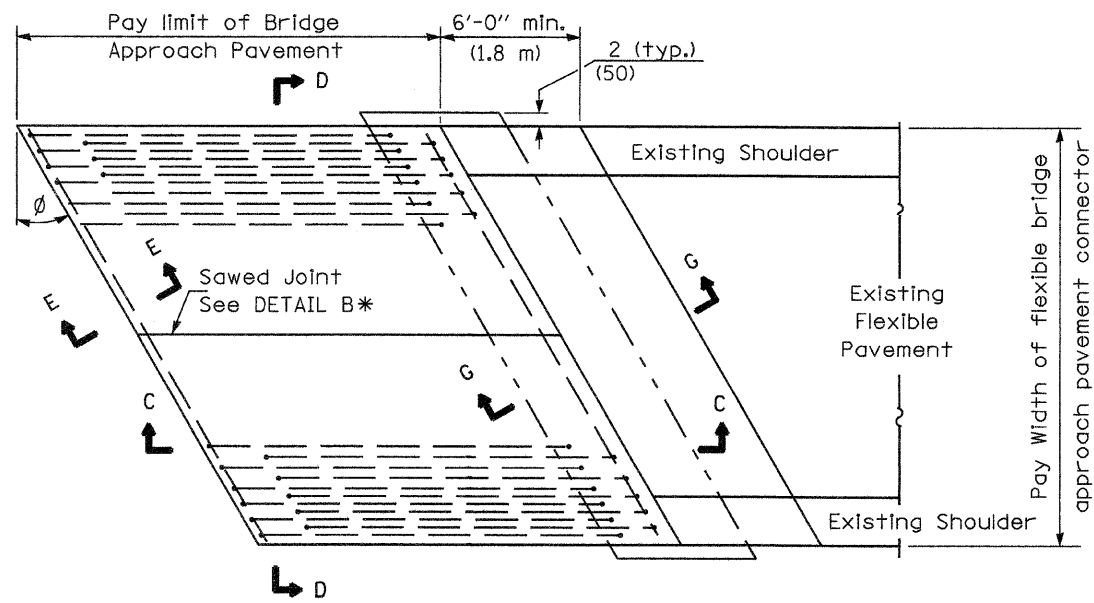


EXISTING CONSTRUCTION



SECTION G-G - RIGID PAVEMENT

BRIDGE APPROACH PAVEMENT CONNECTOR (PCC)



SECTION G-G - FLEXIBLE PAVEMENT

BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE)

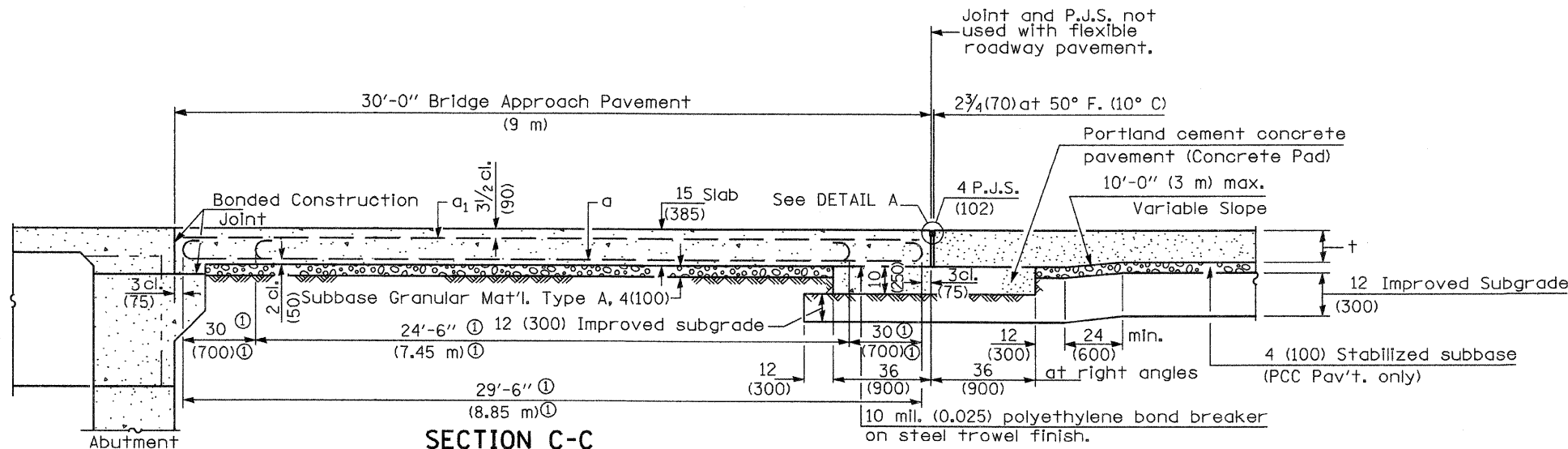
Illinois Department of Transportation
 APPROVED January 1, 2008
Ralph E. Anderson
 ENGINEER OF BRIDGES AND STRUCTURES
 APPROVED January 1, 2008
Ken C. Han
 ENGINEER OF DESIGN AND ENVIRONMENT
 ISSUED 1-1-97

BRIDGE APPROACH PAVEMENT

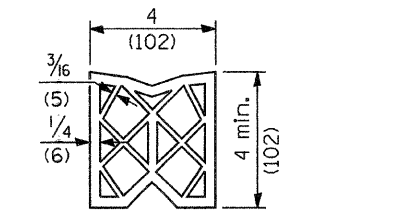
(Sheet 2 of 4)

Contract 76B18 Sheet 75B.

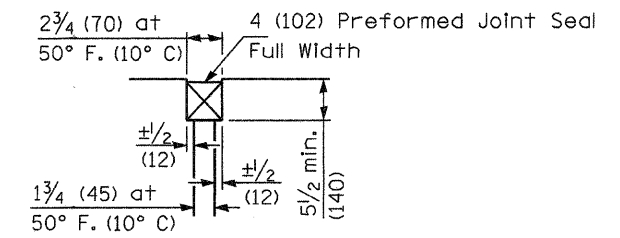
D.P. Sullivan



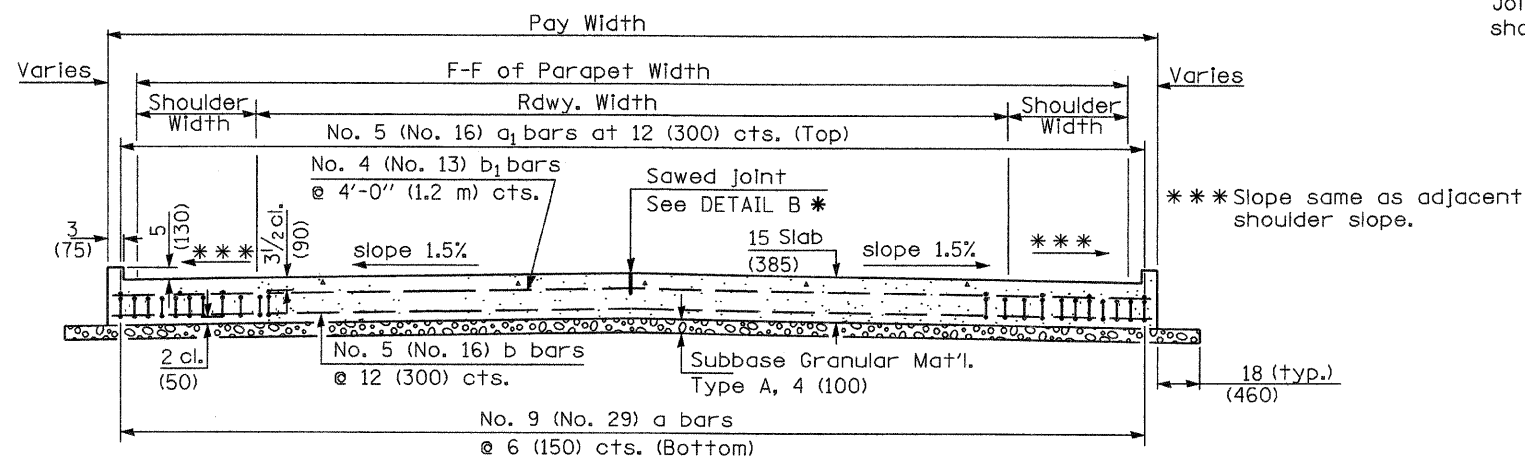
① Stagger No. 9 (No. 29) a bars as shown on plan - full width



PREFORMED JOINT SEAL



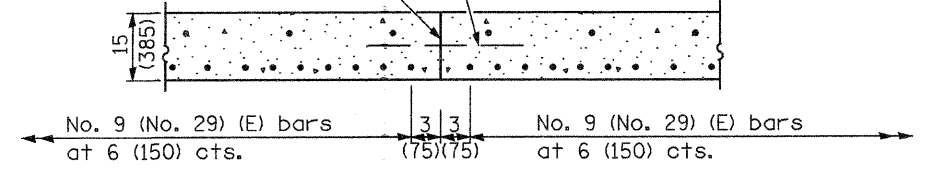
DETAIL A



SECTION D-D

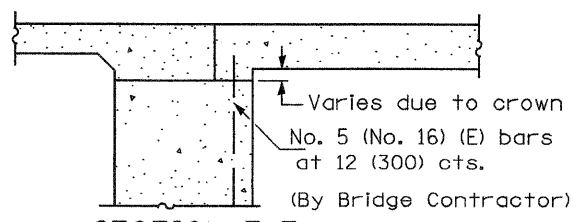
(See Plan for Dimensions not shown)

Longitudinal Construction Joint in accordance with details shown on Standard 420001.



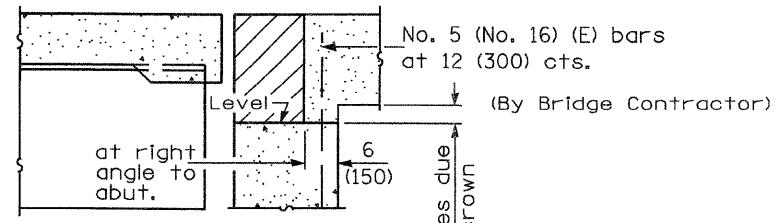
OPTIONAL LONGITUDINAL CONSTRUCTION JOINT

As approved by the Engineer, the Contractor may elect to reduce the widths of pour by use of the Optional Longitudinal Construction Joint shown. Joints shall be located at the edge of a traffic lane.



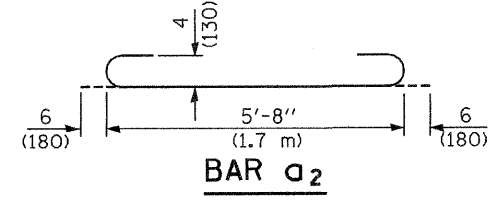
SECTION E-E

(Integral Abutments)

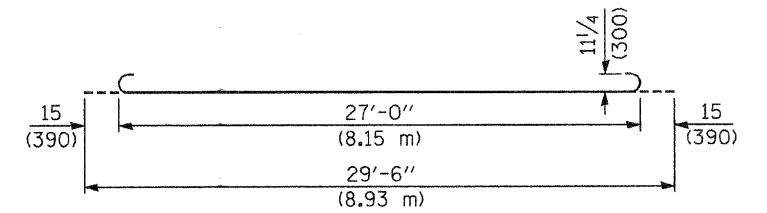


SECTION E-E

(Jointed Abutments)



BAR a



BAR a2

DESIGN STRESSES

$f_y = 60,000$ p.s.i. (400 MPa)
 $f'_c = 3,500$ p.s.i. (24 MPa)
 $n = 8.5$

BRIDGE APPROACH PAVEMENT

(Sheet 3 of 4)

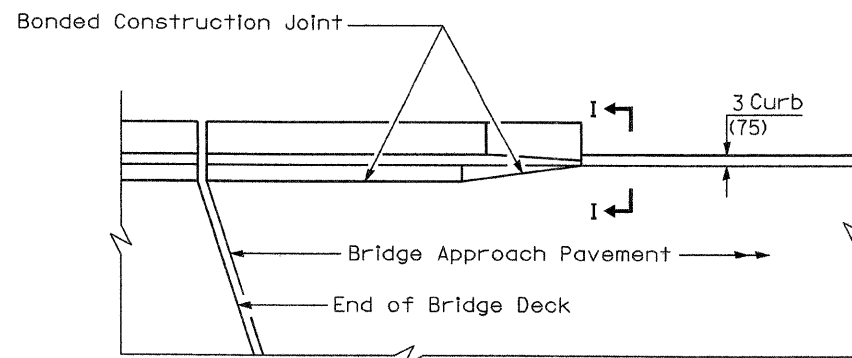
Contract 76B1B Sheet 75C.

Illinois Department of Transportation

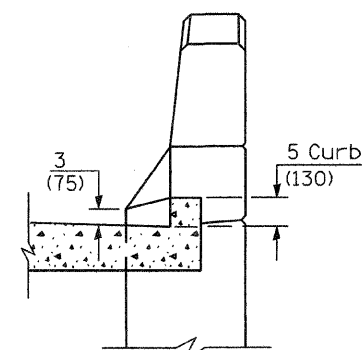
APPROVED January 1, 2008
Ralph E. Anderson
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APPROVED January 1, 2008
Ken E. Han
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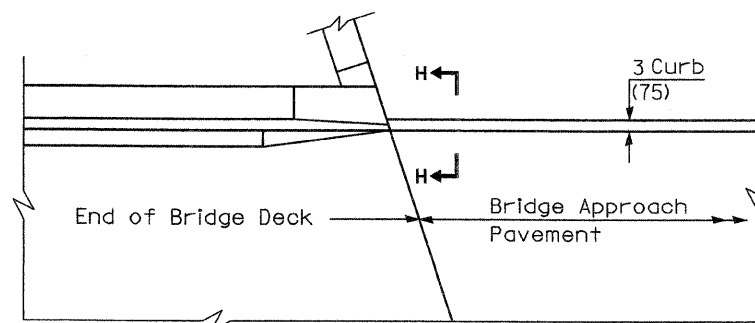
ISSUED 76-1-1



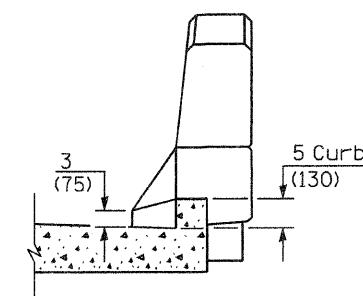
**PARAPET TO CURB TRANSITION
PILE BENT ABUTMENT**



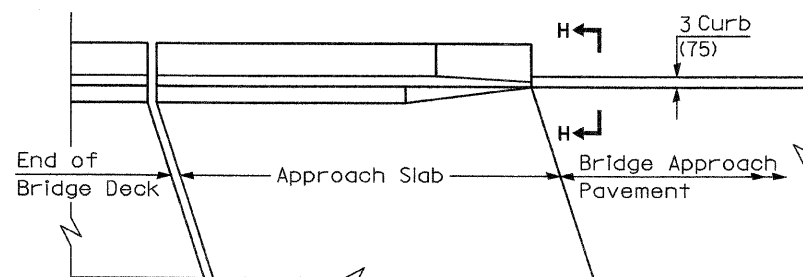
SECTION I - I



**PARAPET TO CURB TRANSITION
INTEGRAL ABUTMENT**



SECTION H - H



**PARAPET TO CURB TRANSITION
VAULTED ABUTMENT**

Illinois Department of Transportation
 APPROVED January 1, 2008
Ralph E. Anderson
 ENGINEER OF BRIDGES AND STRUCTURES
 APPROVED January 1, 2008
Tom E. Han
 ENGINEER OF DESIGN AND ENVIRONMENT
 ISSUED 1-1-97

BRIDGE APPROACH PAVEMENT

(Sheet 4 of 4)

Contract 76B18 Sheet 75D.