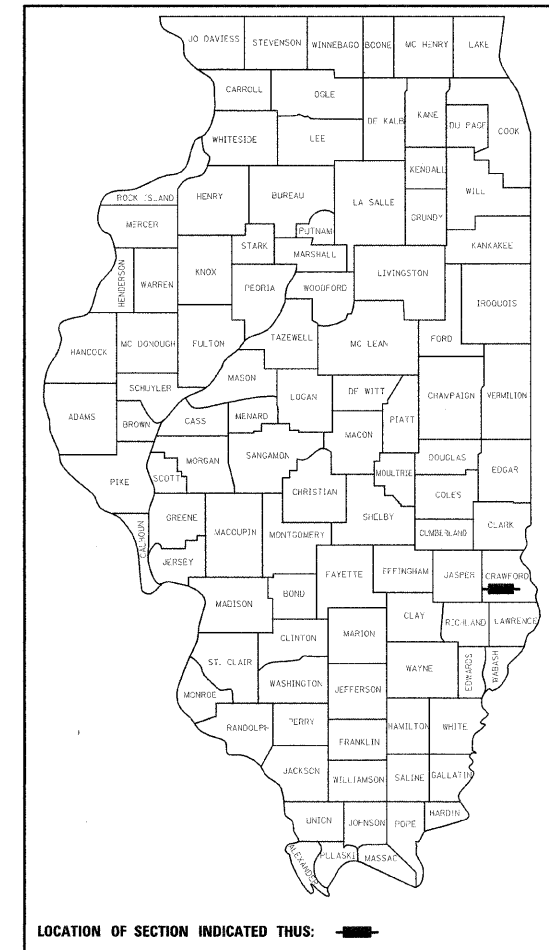


| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 1 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

D-97-035-97



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

F.A.P. ROUTE 781 (IL. ROUTE 33)
SECTION 1YRS

C-97-007-98

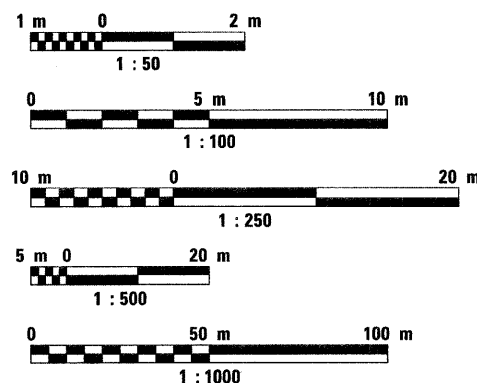
CRAWFORD COUNTY
PROJECT: ACF-0781(029)

FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR THE LIST OF APPLICABLE IDOT HIGHWAY STANDARDS, SEE SHEET NO. 2

| | | |
|--|--------------|--------------------------|
| STRUCTURAL DESIGN TRAFFIC: | YEAR | 2019 |
| PV = | 8173 | SU = 145 MU = 377 |
| ROAD/STREET CLASSIFICATION: | CLASS | II |
| PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE: | | |
| P = | 50 | S = 50 M = 50 |
| TRAFFIC FACTOR: | Actual TF = | 1.63 |
| | Minimum TF = | 3.81 |
| SUBGRADE SUPPORT RATING: | | |
| SSR = | Poor | (STA. 13+143 TO 13+750) |
| SSR = | | (STA. _____ TO _____) |

DESIGN DESIGNATION:
8695(19) Arterial 3.81 (FD-20)
2019 ADT = 8695

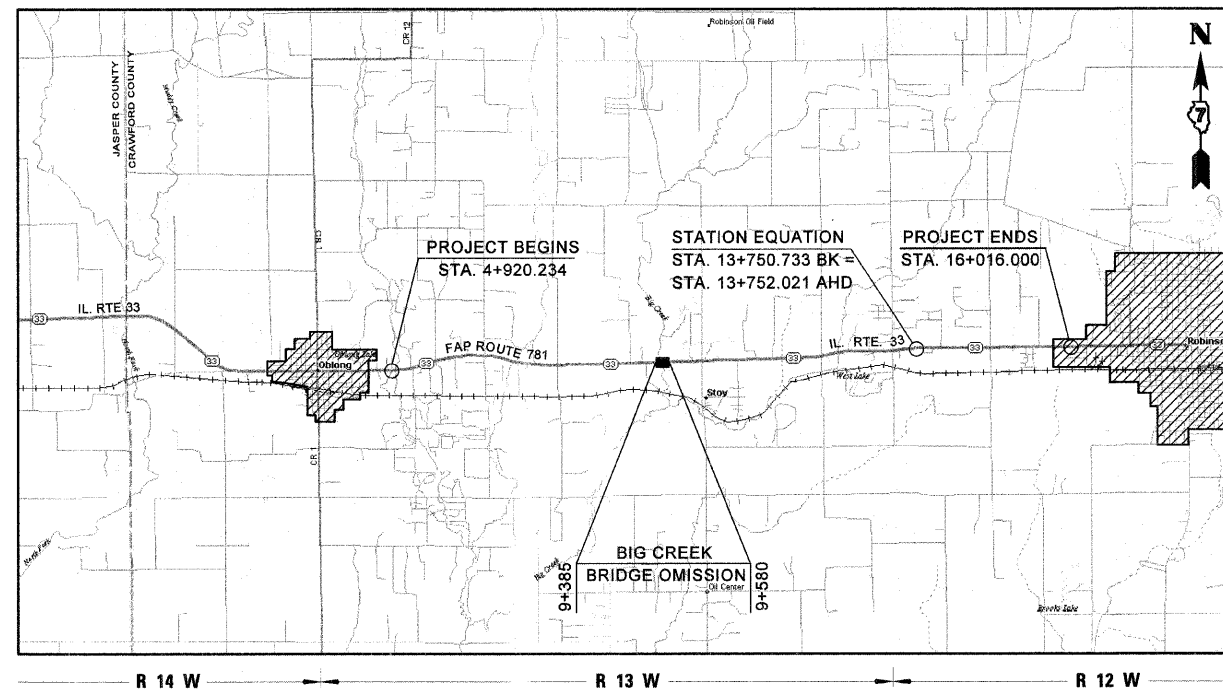
METRIC RATIOS



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

CONTRACT NO. 94533



LOCATION MAP
SCALE 1:80,000

GROSS LENGTH OF IMPROVEMENT = 11,095.766 METERS = 11.096 KILOMETERS
STATION EQUATION OMISSION = 1.288 METERS = 0.001 KILOMETERS
ROAD AND BRIDGE OMISSIONS = 195.000 METERS = 0.195 KILOMETERS
NET LENGTH OF IMPROVEMENT = 10,899.476 METERS = 10.899 KILOMETERS

PLANS PREPARED BY



HENRY, MEISENHEIMER & GENDE, INC.
ENGINEERS
CARLYLE, ILLINOIS 62231
www.hmgengineers.com

(618) 594-3711 Fax (618) 594-8217



Scott A. Rakers 01/30/09
SCOTT A. RAKERS, P.E.
ILLINOIS REGISTERED ENGINEER NO. 062-049177
REGISTRATION EXPIRES NOV. 30, 2009

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED January 30, 2009

Roger L. Schiella
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

March 27, 2009
Charles G. Ingemolte
ENGINEER OF DESIGN AND ENVIRONMENT

March 27, 2009
Christine M. Reed
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

Project Engineer: Mark Daugherty
Telephone: (217) 342-8341

PLOT DATE = 12/22/2008
FILE NAME = H:\4428\Contract 94533\SH1_081\cover.dgn

HMG JOB NO. 4420

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 2 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

INDEX OF SHEETS

| SHEET NO. | DESCRIPTION |
|-----------|--|
| 1 | COVER SHEET |
| 2 | INDEX OF SHEETS AND LIST OF STANDARDS |
| 3 | GENERAL NOTES |
| 4-8 | SUMMARY OF QUANTITIES |
| 9-11 | TYPICAL SECTIONS |
| 12-22 | MISCELLANEOUS QUANTITY SCHEDULES |
| 23-26 | PIPE CULVERT SCHEDULE |
| 27-28 | LOCAL TIES |
| 29-33 | RURAL ENTRANCE DETAILS & SCHEDULES |
| 34-66 | PLAN AND PROFILE SHEETS - EAST OF OBLONG |
| 67 | STAGE CONSTRUCTION AND TRAFFIC CONTROL AT ACROSS ROAD BOX CULVERT REPLACEMENTS & INSTALLATIONS |
| 68-70 | RECONSTRUCTION SECTION STAGE 1 |
| 71-73 | RECONSTRUCTION SECTION STAGE 2 |
| 74-76 | RECONSTRUCTION SECTION STAGE 3 |
| 77-81 | SIDE ROAD PROFILES |
| 82-83 | SUPERELEVATION TRANSITION DETAILS AND DATA TABLES |
| 84 | BUTT JOINT, TAPER AND ASPHALT MILLING DETAILS |
| 85 | EMBANKMENT BENCHING DETAILS |
| 86 | PERMANENT EROSION CONTROL DETAILS |
| 87-89 | PRECAST CONCRETE BOX CULVERT DETAILS |
| 90-93 | PRECAST CONCRETE BOX CULVERT SECTIONS |
| 94-104 | BOX CULVERT EXTENSIONS - PLANS AND ELEVATIONS |
| 105-108 | BOX CULVERT EXTENSIONS - DETAILS |
| 109-212 | IL. ROUTE 33 CROSS SECTIONS RIGHT-OF-WAY PLANS |

APPLICABLE HIGHWAY STANDARDS

| STD. NO. | TITLE | STD. NO. | TITLE |
|-----------|--|-----------------------|---|
| 280001-04 | TEMPORARY EROSION CONTROL SYSTEMS | 667101-01 | PERMANENT SURVEY MARKERS |
| 420001-07 | PAVEMENT JOINTS | 701006-03 | OFF-ROAD OPERATIONS, 2-LANE, 2-WAY, 4.5 m TO 600 mm FROM PAVEMENT EDGE |
| 420111-02 | PCC PAVEMENT ROUNDOUTS | 701011-02 | OFF ROAD MOVING OPERATIONS, 2-LANE, 2-WAY, DAY ONLY |
| 442201-03 | CLASS C AND D PATCHES | 701201-03 | LANE CLOSURE, 2-LANE, 2-WAY, DAY ONLY, FOR SPEEDS MORE THAN OR EQUAL 45 MPH |
| 482001-02 | BITUMINOUS SHOULDER ADJACENT TO FLEXIBLE PAVEMENT | 701306-02 | LANE CLOSURE, 2-LANE, 2-WAY, SLOW MOVING OPERATIONS - DAY ONLY FOR SPEEDS MORE THAN OR EQUAL 45 MPH |
| 482011-03 | BITUMINOUS SHOULDER STRIPS / SHOULDERS WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS | 701311-03 | LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY |
| 542301-02 | PRECAST REINFORCED CONCRETE FLARED END SECTION | 701321-10 | LANE CLOSURE, 2L, 2W, BRIDGE REPAIR WITH BARRIER |
| 542311-01 | GRATING FOR CONCRETE FLARED END SECTION (FOR 600 mm THRU 1350 mm PIPE) | 701326-03 | LANE CLOSURE, 2-LANE, 2-WAY, PAVEMENT WIDENING FOR SPEEDS MORE THAN OR EQUAL 45 MPH |
| 542401-01 | METAL END SECTION FOR PIPE CULVERTS | 701331-03 | LANE CLOSURE, 2-LANE, 2-WAY, WITH RUN-AROUND, FOR SPEEDS ≥ 45 MPH |
| 542606 | REINFORCED CONCRETE PIPE TEE | 701901-01 | TRAFFIC CONTROL DEVICES |
| 602301-02 | INLET - TYPE A | 704001-05 | TEMPORARY CONCRETE BARRIER |
| 604036-02 | GRATE - TYPE B | 780001-02 | TYPICAL PAVEMENT MARKINGS |
| 630001-08 | STEEL PLATE BEAM GUARDRAIL | 781001-03 | TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS |
| 630201-06 | PCC/HMA STABILIZATION AT STEEL PLATE BEAM GUARDRAIL | 000001-05 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS |
| 630301-05 | SHOULDER WIDENING FOR TYPE 1, (SPECIAL) GUARDRAIL TERMINALS | 001001-02 | AREAS OF REINFORCEMENT REBARS |
| 631011-05 | TRAFFIC BARRIER TERMINAL, TYPE 2 | B.L.R. 2-2 | COMBINATION SECTION AND RIGHT OF WAY MARKER |
| 635006-03 | REFLECTOR AND TERMINAL MARKER PLACEMENT | B.L.R. 6-5 | SECTION MARKERS |
| 635011-02 | REFLECTOR MARKER AND MOUNTING DETAILS | B.L.R. 23.3 | TRAFFIC BARRIER TERMINAL, TYPE 1 |
| 666001-01 | RIGHT OF WAY MARKERS | | |

PLOT DATE = 12/22/2008
 FILE NAME = H:\4420\Contract\94533\SHI_002_IndexofShts.dgn

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|--|
| NAME | DATE | |
| | | INDEX OF SHEETS AND LIST OF HIGHWAY STANDARDS SCALE NO SCALE DRAWN BY EDW DATE DECEMBER 22, 2008 CHECKED BY LWJ |
| | | |
| | | |
| | | |
| | | |
| | | |

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 3 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

GENERAL NOTES

- WORK INCLUDED IN SECTION 1YRS CONSISTS OF RESURFACING THE EXISTING 6.7 METER WIDE TRAVELED WAY AND WIDENING THE SECTION TO INCLUDE 0.9 METER WIDE HOT-MIX ASPHALT SHOULDERS AND 1.5 METER WIDE AGGREGATE SHOULDERS; CONSTRUCT NEW DITCHES AND EXTEND/REPLACE CULVERTS DOGWOOD CREEK TO 954 METERS EAST OF TOWNSHIP ROAD 900E, THERE IS ALSO ONE BRIDGE OMISSIONS ON THE PROJECT OVER BIG CREEK. ALSO INCLUDED IS 607.2 METERS OF FULL-DEPTH PAVEMENT CONSTRUCTION AT THE INTERSECTION OF TOWNSHIP ROAD 800E.
- UTILITIES**
 - ABANDONED UNDERGROUND UTILITIES THAT CONFLICT WITH CONSTRUCTION SHALL BE DISPOSED OUTSIDE THE LIMITS OF THE RIGHT OF WAY ACCORDING TO ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. THE COST OF SUCH WORK WILL BE INCLUDED IN THE COST FOR EARTH EXCAVATION.
 - THE LOCATION OF ALL UTILITIES ARE BASED ON INFORMATION PROVIDED BY OTHERS AND ARE INTENDED TO BE APPROXIMATE ONLY. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO COORDINATE HIS CONSTRUCTION ACTIVITIES WITH THE VARIOUS UTILITY OWNERS. ALL POTENTIAL CONFLICTS SHALL BE INVESTIGATED AND REMEDIAL ACTION TAKEN PRIOR TO INTERRUPTION OF THE CONTRACTORS PROGRESS.
 - ALL UTILITY FACILITIES THAT REQUIRE RELOCATION WITHIN STATE ROW SHALL BE COMPLETED BY THE UTILITY COMPANY UNLESS OTHERWISE SHOWN ON THE PLANS.
 - THE J.U.L.I.E. NUMBER IS 1-800-892-0123. A MINIMUM OF FORTY-EIGHT HOURS ADVANCE NOTICE IS REQUIRED. MEMBERS OF J.U.L.I.E. KNOWN TO BE WITHIN THE LIMITS OF THE IMPROVEMENT ARE:

| | |
|-------------------------------|------------------|
| CITY OF OBLONG (WATER, SEWER) | ENSTAR CABLE |
| AMEREN IP (ELECTRIC, GAS) | MCI/WORLDCOM |
| LEVEL 3 COMMUNICATIONS | AT&T |
| FRONTIER COMMUNICATIONS | CRAWFORD CO. OIL |
- EXISTING FACILITIES AND VARIATIONS. IN ADDITION TO FIELD SURVEYS AND AERIAL SURVEYS, PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING FACILITIES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND DETAILS IN THE FIELD. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION DUE TO CHANGE IN THE SCOPE OF WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE ITEM OF WORK.
- STA/OFFSET REFERENCES AND HORIZONTAL/VERTICAL CONTROLS**
 - ALL STATION AND OFFSET REFERENCES ARE TO THE ROADWAY CENTERLINE (IL 33 OR SIDE ROADS) UNLESS OTHERWISE NOTED IN THE PLANS. ALL HORIZONTAL CONTROL POINTS ARE BASED ON THE STATE PLANE COORDINATE SYSTEM AND WERE PROVIDED BY IDOT-DISTRICT 7.
 - VERTICAL CONTROL. ALL CONTROL POINTS AND ELEVATIONS SHOWN ON THE PLANS ARE BASED ON U.S.G.S. MEAN SEA LEVEL DATUM.
 - ALL PAVEMENT GRADES AND ELEVATIONS SHOWN ON THE PLANS ARE TO THE TOP OF THE PAVEMENT SURFACE UNLESS OTHERWISE SHOWN ON THE PLANS.
 - ALL DIMENSIONS SHOWN ON THE PLANS ARE IN MILLIMETERS (mm) EXCEPT AS NOTED.
 - ALL EARTH SLOPES THROUGHOUT THE PLANS ARE SHOWN AS V/H (VERTICAL TO HORIZONTAL).
- HIGHWAY STANDARDS. ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER LISTED ON THE PREVIOUS SHEET OR THE COPY OF THE STANDARD INCLUDED IN THESE PLANS.
- BITUMINOUS MATERIAL**
 - BITUMINOUS MATERIALS (PRIME COAT) SHALL BE EITHER RC-70 OR SS-1HP
- TREE PLANTING. THE CONTRACTOR SHALL NOTIFY THE IDOT DISTRICT ARCHITECT ONE MONTH PRIOR TO TREE PLANTING OPERATIONS IN ORDER TO DETERMINE THE FINAL PLANTING LOCATIONS.
- TRANSITIONS TO EXISTING. 3 METER TRANSITIONS SHALL BE USED TO MATCH PROPOSED ITEMS OF WORK TO EXISTING ITEMS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEM OF WORK SPECIFIED.
- ALL DRILLING, BARS, GROUTING, AND EXPANSION JOINTS REQUIRED FOR CONCRETE PAVEMENTS SHALL BE INCLUDED IN THE COST OF PCC BASE COURSE.
- EXISTING CONCRETE PADS OR BASES, SIGNS, POLES, FENCES, RAILROAD TIES, MISC. CONCRETE OR OTHER UNCLASSIFIED ITEMS NOT SPECIFICALLY SHOWN ON THE PLANS OR PAID FOR THAT INTERFERE WITH THE PROPOSED IMPROVEMENTS SHALL BE REMOVED. THE COST FOR SUCH WORK SHALL BE INCLUDED IN THE COST OF EARTH EXCAVATION.
- EXISTING CONCRETE CURB OR CONCRETE CURB AND GUTTER NOT ON THE PLANS TO BE REMOVED OR THAT INTERFERES WITH THE PROPOSED IMPROVEMENTS SHALL BE REMOVED. THE COST FOR SUCH WORK SHALL BE INCLUDED IN THE COST OF PAVEMENT REMOVAL.
- MONUMENTATION**
 - THE CONTRACTOR WILL BE RESPONSIBLE FOR HAVING AN LICENSED SURVEYOR REESTABLISH ANY SECTION OR SUBSECTION MONUMENTS DESTROYED BY HIS OPERATIONS.
 - RIGHT-OF-WAY MARKERS SHALL BE ERECTED WITH THE BACK FACE OF THE MARKER ON THE RIGHT-OF-WAY LINE UNLESS THE NEW RIGHT-OF-WAY LINE HAS BEEN SURVEYED AND PINNED, IN WHICH INSTANCE THE RIGHT-OF-WAY MARKER WILL BE ERECTED 305mm (12 INCHES) INSIDE THE NEW RIGHT-OF-WAY LINE.
- AGGREGATE MATERIALS**
 - AGGREGATE SHOULDERS SHALL BE CRUSHED STONE, CRUSHED CONCRETE, OR RAP.
 - AGGREGATE SURFACE COURSE SHALL BE CRUSHED STONE OR CRUSHED CONCRETE.
 - SUBBASE GRANULAR MATERIAL TYPE B AND AGGREGATE BASE COURSE TYPE B SHALL BE CRUSHED STONE.
- STORM SEWER & DRAINAGE**
 - OFFSETS FOR DRAINAGE STRUCTURES IS FROM THE ROADWAY BASELINE TO THE CENTER OF THE STRUCTURE. FOR STRUCTURES BUILT IN CONJUNCTION WITH CURB AND GUTTER, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO SET THE STRUCTURE SUCH THAT THE FRAME AND/OR GRATE MATCHES THE CURB LINE.
 - THE COST OF CONNECTING EXISTING DRAIN TILE OR SEWER TO PROPOSED DRAINAGE STRUCTURES SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED DRAINAGE STRUCTURES.
 - ABANDONED SEWERS AND DRAIN LINES SHOWN ON THE PLANS OR AS DESIGNATED BY THE ENGINEER SHALL BE PLUGGED WITH CLASS SI CONCRETE ACCORDING TO ARTICLE 550.05 OF THE STANDARD SPECIFICATIONS. THIS WORK SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST FOR PROPOSED STORM SEWER.
 - WHEN THE REQUIRED VERTICAL AND/OR HORIZONTAL CLEARANCES, AS SPECIFIED BY THE IEPA, BETWEEN PROPOSED STORM SEWER AND EXISTING/PROPOSED WATER MAINS CANNOT BE MET, WATER MAIN QUALITY STORM SEWER SHALL BE INSTALLED AS SPECIFIED IN THE SPECIAL PROVISIONS. THIS PIPE WILL BE PAID FOR AS "STORM SEWER (WATER MAIN QUALITY)" OF THE TYPE AND DIAMETER SPECIFIED.
 - WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL DRAINS, SEWERS AND/OR STRUCTURES. HE SHALL PROVIDE A MEANS FOR OUTLETING ALL DISCHARGES FROM THESE FACILITIES UNTIL SUCH TIME AS THE PERMANENT CONNECTIONS WITH SEWERS ARE BUILT AND IN SERVICE. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF PROPOSED STORM SEWER.
 - EXISTING ROOF AND YARD DRAINS CURRENTLY CONNECTED TO THE EXISTING STORM SEWER SHALL BE RECONNECTED TO THE PROPOSED DRAINAGE SYSTEM AS DIRECTED BY THE ENGINEER. THE COST FOR SUCH WORK SHALL BE INCLUDED IN THE COST OF PROPOSED STORM SEWER.
 - EXISTING YARD AND FIELD DRAINS AT LOCATIONS WITH DITCH DRAINAGE SHALL BE CUT OFF FLUSH WITH THE PROPOSED DITCH BACKSLOPE OR AS DIRECTED BY THE ENGINEER. THE COST FOR SUCH WORK SHALL BE INCLUDED IN THE COST OF EARTH EXCAVATION.
- PAVEMENT MARKING. NO PASSING ZONES TO BE FIELD VERIFIED BY THE BUREAU OF OPERATIONS. THE RESIDENT ENGINEER SHALL NOTIFY THE BUREAU OF OPERATIONS 14 DAYS PRIOR TO PERMANENT PAVEMENT MARKINGS.
- NPDES COMPLIANCE. THE CONTRACTOR SHALL BE REQUIRED TO COMPLY WITH THE PROVISIONS OF THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) STORM WATER PERMIT AND IMPLEMENT THE EROSION CONTROL PLAN INCLUDED IN THESE PLANS AND SPECIFIED HEREIN.

- TRAFFIC CONTROL**
 - FOR STABILIZATION, ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR SAND BAGS PER BARRICADE.
 - THE CONTRACTOR SHALL KEEP LANE CLOSURES TO A MINIMUM. HE SHALL WORK EXPEDITIOUSLY TO OPEN TRAFFIC LANES CLOSED DUE TO CONSTRUCTION. THE ENGINEER SHALL BE THE SOLE JUDGE OF WHEN A LANE IS READY TO BE OPENED TO TRAFFIC.
 - THE FOLLOWING SHALL BE NOTIFIED FOURTEEN (14) CALENDAR DAYS PRIOR TO ANY ROAD CLOSURES ON THE PROJECT:

| | |
|-----------------------------|--------------------------------|
| CITY OF OBLONG POLICE DEPT. | OBLONG TOWNSHIP COMMISSIONER |
| CITY OF OBLONG FIRE DEPT. | ROBINSON TOWNSHIP COMMISSIONER |
| CRAWFORD COUNTY ENGINEER | |
- REMOVALS**
 - REMOVAL OF HOT-MIX ASPHALT, OIL AND CHIP, AND AGGREGATE SURFACING NOT ON A RIGID TYPE BASE SHALL BE PAID FOR AS EARTH EXCAVATION.
 - REMOVAL OF EXISTING GRAVEL OR CRUSHED STONE BASE COURSE SHALL BE PAID FOR AS EARTH EXCAVATION.
 - EXISTING SIDEWALKS AND STEPS TO BE REMOVED MADE OF CONCRETE, BRICK OR ASPHALT SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR SIDEWALK REMOVAL.
- HOT-MIX ASPHALT OPERATIONS**
 - THE ENGINEER SHALL BE THE SOLE JUDGE CONCERNING CURING TIME FOR THE VARIOUS HOT-MIX ASPHALT LIFTS.
 - CONTINUOUS PAVING OPERATIONS ON THE MAIN ROADWAY SHALL BE MAINTAINED AT ALL TIMES DURING THE CONSTRUCTION OF THE HOT-MIX ASPHALT SURFACE. NO INTERRUPTIONS FOR SIDE ROADS, ENTRANCES, TURN LANES, ETC. WILL BE ALLOWED.
- EARTH EXCAVATION & EMBANKMENTS**
 - THE FINISHED EARTHWORK SHALL HAVE A VEGETATION SUSTAINING SOIL COVERING THE TOP 100mm (TOPSOIL) IN AREAS TO BE SEEDED OR SODDED. THE TOPSOIL REQUIRED SHALL BE OBTAINED BY STRIPPING OPERATIONS AS SHOWN IN THE PLANS OR FURNISHED FROM OFFSITE AS REQUIRED.
 - ALL EMBANKMENT WIDENING SHALL BE SUFFICIENTLY BENCHED INTO EXISTING EMBANKMENTS/SLOPES PER SECTION 205 OF THE STANDARD SPECIFICATIONS.
- EXISTING PAVEMENT SHALL BE PATCHED AS SHOWN ON THE PLANS AND IN ACCORDANCE WITH SECTION 442 OF THE STANDARD SPECIFICATIONS. THE QUANTITY OF PATCHING SHOWN ON THE PLANS IS AN ESTIMATE ONLY AND FINAL QUANTITIES SHALL BE DETERMINED BY THE ENGINEER.
- ALL SAW CUTTING OF EXISTING PAVEMENT NOT RELATED TO PAVEMENT PATCHING SHALL BE INCLUDED IN THE COST OF THE VARIOUS ITEMS OF WORK INVOLVED. THE MINIMUM CUT DEPTH INTO THE PAVEMENT SHALL BE 40mm UNLESS OTHERWISE SPECIFIED IN THE PLANS.
- SEEDING SHALL BE PLACED ON ALL AREAS DISTURBED BY CONSTRUCTION OPERATIONS NOT OTHERWISE TO BE SODDED. NUTRIENTS AND MULCH SHALL BE APPLIED TO ALL SEEDING AREAS.
- DO NOT INCLUDE MULCH IN EROSION CONTROL BLANKET AREAS.
- EROSION CONTROL SHALL BE PLACED AROUND INLETS, END SECTIONS AND HEADWALLS IN ACCORDANCE WITH THE DETAILS IN THE PLANS.
- THE CONTRACTOR SHALL PROVIDE INTERNET ACCESS TO THE HOT-MIX PLANT QUALITY CONTROL LAB SO THAT HOT-MIX PLANT REPORTS CAN BE E-MAILED TO THE DISTRICT HEADQUARTERS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICES FOR OTHER ITEMS IN THE CONTRACT.

APPLICATION RATES

THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN CALCULATING PLAN QUANTITIES:

| | |
|---|---------------------------|
| GRANULAR MATERIALS | 2.4 metric ton/cu m |
| BITUMINOUS MATERIALS PRIME COAT | |
| HOT-MIX ASPHALT BASES | 0.35 L/sq m |
| AGGREGATE BASES | 1.5 L/sq m |
| AGGREGATE PRIME COAT | 1.5 kg/sq m |
| HOT-MIX ASPHALT SURFACE & BINDER COURSE | 0.0024 metric ton/sq m/mm |
| NITROGEN FERTILIZER NUTRIENT | 100 kg/ha |
| PHOSPHORUS FERTILIZER NUTRIENT | 100 kg/ha |
| POTASSIUM FERTILIZER NUTRIENT | 100 kg/ha |
| LIME FOR MODIFIED SOIL | 0.06856 metric ton /cu m |

MIXTURE DESIGN

| MIXTURE USE(S) | SURFACE | LEVEL BINDER | INCIDENTAL | BITUMINOUS SHOULDERS (BOTTOM 93 mm) | BITUMINOUS SHOULDERS (TOP 57 mm) | BITUMINOUS BASE COURSE (PRA'S AND COMMERCIAL) | PAVEMENT PATCHING |
|---|-------------------|----------------|----------------|-------------------------------------|----------------------------------|---|-------------------|
| AC/PG | PG 64-22 | PG 64-22 | PG 64-22 | PG 58-22 | PG 58-22 | PG 64-22 | PG 64-22 |
| RAP % (MAX.) | 10% | 15% | 20% | 20% | 20% | 20% | 20% |
| DESIGN AIR VOIDS | 4.0% @ Ndes=70 | 4.0% @ Ndes=70 | 4.0% @ Ndes=50 | 4.0% @ Ndes=70 | 2.0% @ Ndes=30 | 4.0% @ Ndes=70 | 4.0% @ Ndes=70 |
| MIXTURE COMPOSITION (GRADATION MIXTURE) | IL 9.5 or IL 12.5 | IL 9.5 | IL 9.5 | IL 19.0 | IL 9.5 | IL 19.0 | IL 19.0 |
| FRICITION AGGREGATE | MIXTURE D | N/A | MIXTURE C | N/A | N/A | N/A | N/A |

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|--|
| NAME | DATE | |
| | | <p align="center">GENERAL NOTES</p> <p>SCALE NO SCALE DRAWN BY KOJ</p> <p>DATE DECEMBER 22, 2008 CHECKED BY LWJ</p> |
| | | |
| | | |
| | | |
| | | |
| | | |

PLOT DATE = 12/22/2008
 FILE NAME = H:\1428\Contract 94533\SH_003.gnmod.dgn

H. M. & G. NO. 4420

| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 4 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

SUMMARY OF QUANTITIES

| CODE NO. | ITEM DESCRIPTION | UNIT | TOTAL QUANTITY | 80% FED 20% STATE | |
|----------|---|-------|----------------|--------------------------|--------------------------|
| | | | | F.A.P. 781 (IL. RTE. 33) | |
| | | | | 4+920.234 TO 15+545.000 | 15+545.000 TO 16+016.000 |
| | | | | CONSTRUCTION CODE TYPE | |
| | | | | 1000-2A RURAL | 1000-2A URBAN |
| A2001016 | TREE, ACER RUBRUM (RED MAPLE), 2" CALIPER, BALLED AND BURLAPPED | EACH | 20 | 15 | 5 |
| A2001316 | TREE, ACER SACCHARINUM (SILVER MAPLE), 2" CALIPER, BALLED AND BURLAPPED | EACH | 15 | 15 | 0 |
| A2006416 | TREE, QUERCUS ALBA (WHITE OAK), 2" CALIPER, BALLED AND BURLAPPED | EACH | 20 | 15 | 5 |
| A2006916 | TREE, QUERCUS PALUSTRIS (PIN OAK), 2" CALIPER, BALLED AND BURLAPPED | EACH | 20 | 15 | 5 |
| D2002288 | EVERGREEN, PICEA PUNGENS GLAUCA(COLORADO BLUE SPRUCE), 8' HEIGHT, B&B | EACH | 15 | 0 | 15 |
| M2010110 | TREE REMOVAL (6 TO 15 UNITS DIAMETER) | UNIT | 153 | 153 | 0 |
| M2010210 | TREE REMOVAL (OVER 15 UNITS DIAMETER) | UNIT | 490 | 490 | 0 |
| M2010500 | TREE REMOVAL, HECTARES | HA | 0.5 | 0.5 | 0.0 |
| M2020010 | EARTH EXCAVATION | CU M | 60,365 | 57,950 | 2,415 |
| M2020050 | EARTH EXCAVATION (WIDENING) | CU M | 2,915 | 2,788 | 127 |
| M2021200 | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | CU M | 680 | 680 | 0.0 |
| M2080150 | TRENCH BACKFILL | CU M | 143 | 138.5 | 4.5 |
| M2112500 | TOPSOIL EXCAVATION AND PLACEMENT | CU M | 20,220 | 19,478.0 | 742.0 |
| M2500200 | SEEDING, CLASS 2 | HA | 16.0 | 15.4 | 0.6 |
| M2500400 | NITROGEN FERTILIZER NUTRIENT | KG | 1,600 | 1,540 | 60 |
| M2500500 | PHOSPHORUS FERTILIZER NUTRIENT | KG | 1,600 | 1,540 | 60 |
| M2500600 | POTASSIUM FERTILIZER NUTRIENT | KG | 1,600 | 1,540 | 60 |
| M2510115 | MULCH, METHOD 2 | HA | 16.0 | 15.4 | 0.6 |
| M2510630 | EROSION CONTROL BLANKET | SQ M | 4,547 | 4,547 | 0 |
| M2800255 | TEMPORARY EROSION CONTROL SEEDING | HA | 16.0 | 15.4 | 0.6 |
| M2800400 | PERIMETER EROSION BARRIER | METER | 1,175 | 1,175 | 0 |
| M2800800 | MULCH, METHOD 2 | HA | 16.0 | 15.4 | 0.6 |
| M2810707 | STONE DUMPED RIPRAP, CLASS A4 | SQ M | 3,655 | 3,609 | 46 |
| M2820200 | FILTER FABRIC | SQ M | 3,655 | 3,609 | 46 |
| M3020456 | PROCESSING MODIFIED SOIL MIXTURE, 300MM | SQ M | 7,712 | 7,712 | 0 |
| M3021500 | LIME | M TON | 158.7 | 158.7 | 0.0 |
| M3111100 | SUB-BASE GRANULAR MATERIAL, TYPE B, 100MM | SQ M | 7,712 | 7,712 | 0 |
| M3511010 | AGGREGATE BASE COURSE, TYPE B | M TON | 1,610 | 1,416 | 194 |
| M3550500 | HOT-MIX ASPHALT BASE COURSE, 200MM | SQ M | 7,084 | 6,770 | 314 |
| M3552100 | HOT-MIX ASPHALT BASE COURSE, VARIABLE DEPTH | M TON | 2,589 | 2,589 | 0 |
| M4021010 | AGGREGATE SURFACE COURSE, TYPE B | M TON | 2,271 | 2,205 | 66 |
| M4021200 | AGGREGATE FOR TEMPORARY ACCESS | M TON | 1,084 | 1,048 | 36 |
| M4060100 | BITUMINOUS MATERIALS (PRIME COAT) | LITER | 76,609 | 73,806 | 2,803 |

Δ Specialty Items
 * SEE SPECIAL PROVISIONS

CONTINUED

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|--|
| NAME | DATE | |
| | | <p>SUMMARY OF QUANTITIES</p> <p>SCALE _____ DRAWN BY KOJ</p> <p>DATE DECEMBER 22, 2008 CHECKED BY LWJ</p> |
| | | |
| | | |
| | | |
| | | |

FILE DATE = 1/25/2009
 FILE NAME = H:\4428\Contract 94533\SHI_004_sumq_V8.dgn

H. M. & G. NO. 4420

| | | | | |
|---------------------|----------|------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 5 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

SUMMARY OF QUANTITIES

| CODE NO. | ITEM DESCRIPTION | UNIT | TOTAL QUANTITY | 80% FED 20% STATE F.A.P. 781 (IL. RTE. 33) | |
|----------|---|-------|----------------|---|--------------------------|
| | | | | 4+920.234 TO 15+545.000 | 15+545.000 TO 16+016.000 |
| | | | | CONSTRUCTION CODE TYPE | |
| | | | | I000-2A RURAL | I000-2A URBAN |
| M4060300 | AGGREGATE (PRIME COAT) | M TON | 279 | 267 | 12 |
| M4060895 | CONSTRUCTING TEST STRIP | EACH | 3 | 3 | 0 |
| M4060982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SQ M | 447 | 278 | 169 |
| M4060990 | TEMPORARY RAMP | SQ M | 60 | 43 | 17 |
| M4062135 | LEVELING BINDER (MACHINE METHOD), N70 | M TON | 4,954 | 4,705 | 249 |
| M4063340 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 | M TON | 8,199 | 7,833 | 366 |
| M4075350 | HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH) 350MM | SQ M | 6,984 | 6,984 | 0 |
| M4080500 | INCIDENTAL HOT-MIX ASPHALT SURFACING | M TON | 1,851 | 1,517 | 134 |
| M4230150 | PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 150MM | SQ M | 333 | 333 | 0 |
| M4230200 | PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 200MM | SQ M | 1,471 | 1,471 | 0 |
| M4400715 | HOT-MIX ASPHALT SURFACE REMOVAL, 15MM | SQ M | 66,450 | 63,038 | 3,412 |
| M4400740 | HOT-MIX ASPHALT SURFACE REMOVAL, 40MM | SQ M | 4,479 | 4,479 | 0 |
| M4400950 | HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH | SQ M | 8,339 | 8,339 | 0 |
| M4402000 | PAVEMENT REMOVAL | SQ M | 5,263 | 5,263 | 0 |
| M4402010 | DRIVEWAY PAVEMENT REMOVAL | SQ M | 5,864 | 5,461 | 403 |
| M4405000 | PAVED DITCH REMOVAL | METER | 871 | 871 | 0 |
| M4420300 | PAVEMENT PATCHING, TYPE I 300MM | SQ M | 188 | 168 | 20 |
| M4422300 | PAVEMENT PATCHING, TYPE II 300MM | SQ M | 100 | 90 | 10 |
| M4812150 | AGGREGATE SHOULDERS, TYPE B, 150MM | SQ M | 25,534 | 24,298 | 1,236 |
| M4820550 | HOT-MIX ASPHALT SHOULDERS, 150MM | SQ M | 19,583 | 18,789 | 794 |
| M5010521 | REMOVE EXISTING CULVERTS | METER | 84 | 75 | 9 |
| M5010522 | PIPE CULVERT REMOVAL | METER | 1,966 | 1,841 | 125 |
| M5030350 | CONCRETE STRUCTURES | CU M | 3.9 | 3.9 | 0.0 |
| M5050405 | FURNISHING AND ERECTING STRUCTURAL STEEL | KG | 130 | 130 | 0 |
| M5080205 | REINFORCEMENT BARS, EPOXY COATED | KG | 690 | 690 | 0 |
| M5401002 | PRECAST CONCRETE BOX CULVERT 600MM x 450MM | METER | 15.1 | 15.1 | 0.0 |
| M5401005 | PRECAST CONCRETE BOX CULVERT 600MM x 600MM | METER | 5.7 | 5.7 | 0.0 |
| M5401010 | PRECAST CONCRETE BOX CULVERT 900MM x 600MM | METER | 5.7 | 5.7 | 0.0 |
| M5402020 | PRECAST CONCRETE BOX CULVERT 1.2M x 0.6M (M273) | METER | 30.6 | 30.6 | 0.0 |
| M5403220 | EXPANSION BOLTS M20 | EACH | 161 | 161 | 0 |
| M5421210 | PIPE CULVERTS, TYPE 1 RCCP, 375MM | METER | 126.5 | 110.0 | 16.5 |
| M5421215 | PIPE CULVERTS, TYPE 1 RCCP, 450MM | METER | 47.5 | 35.0 | 12.5 |
| M5421225 | PIPE CULVERTS, TYPE 1 RCCP, 600MM | METER | 118.5 | 118.5 | 0.0 |

* SEE SPECIAL PROVISIONS

CONTINUED

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|--|
| NAME | DATE | |
| | | <p>SUMMARY OF QUANTITIES</p> <p>SCALE _____ DRAWN BY KOJ</p> <p>DATE DECEMBER 22, 2008 CHECKED BY LWJ</p> |
| | | |
| | | |
| | | |
| | | |

PLOT DATE = 1/29/2009
FILE NAME = H:\1420\Contract 94533\SHT_004_sump_VB.dgn

H. M. & G. NO. 4420

| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 6 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

SUMMARY OF QUANTITIES

| CODE NO. | ITEM DESCRIPTION | UNIT | TOTAL QUANTITY | 80% FED 20% STATE F.A.P. 781 (IL. RTE. 33) | |
|----------|---|-------|----------------|---|--------------------------|
| | | | | 4+920.234 TO 15+545.000 | 15+545.000 TO 16+016.000 |
| | | | | CONSTRUCTION CODE TYPE | |
| | | | | I000-2A RURAL | I000-2A URBAN |
| M5421235 | PIPE CULVERTS, TYPE 1 RCCP, 750MM | METER | 32.5 | 32.5 | 0.0 |
| M5421240 | PIPE CULVERTS, TYPE 1 RCCP, 900MM | METER | 55.5 | 55.5 | 0.0 |
| M542C216 | REINFORCED CONCRETE PIPE TEE, 375MM PIPE WITH 300MM RISER | EACH | 1 | 1 | 0 |
| M542C328 | REINFORCED CONCRETE PIPE TEE, 600MM PIPE WITH 375MM RISER | EACH | 1 | 1 | 0 |
| M542C336 | REINFORCED CONCRETE PIPE TEE, 750MM PIPE WITH 375MM RISER | EACH | 1 | 1 | 0 |
| M542E012 | END SECTIONS, 300MM | EACH | 1 | 1 | 0 |
| M542E016 | END SECTIONS, 375MM | EACH | 102 | 100 | 2 |
| M542E020 | END SECTIONS, 450MM | EACH | 60 | 48 | 12 |
| M542E024 | END SECTIONS, 525MM | EACH | 14 | 14 | 0 |
| M542E028 | END SECTIONS, 600MM | EACH | 46 | 46 | 0 |
| M542E032 | END SECTIONS, 675MM | EACH | 6 | 6 | 0 |
| M542E036 | END SECTIONS, 750MM | EACH | 12 | 12 | 0 |
| M542E044 | END SECTIONS, 900MM | EACH | 14 | 14 | 0 |
| M542E048 | END SECTIONS, 1050MM | EACH | 4 | 4 | 0 |
| M542E116 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS, 375MM | EACH | 10 | 10 | 0 |
| M542E120 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS, 450MM | EACH | 6 | 4 | 2 |
| M542E128 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS, 600MM | EACH | 14 | 14 | 0 |
| M542E136 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS, 750MM | EACH | 4 | 4 | 0 |
| M542E144 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS, 900MM | EACH | 6 | 6 | 0 |
| M542H420 | PIPE CULVERTS, CLASS D, TYPE 1, 300MM | METER | 2.5 | 2.5 | 0.0 |
| M542H425 | PIPE CULVERTS, CLASS D, TYPE 1, 375MM | METER | 106.0 | 89.5 | 16.5 |
| M542H430 | PIPE CULVERTS, CLASS D, TYPE 1, 450MM | METER | 444.0 | 363.0 | 81.0 |
| M542H435 | PIPE CULVERTS, CLASS D, TYPE 1, 525MM | METER | 93.5 | 93.5 | 0.0 |
| M542H440 | PIPE CULVERTS, CLASS D, TYPE 1, 600MM | METER | 337.5 | 337.5 | 0.0 |
| M542H445 | PIPE CULVERTS, CLASS D, TYPE 1, 675MM | METER | 37.0 | 37.0 | 0.0 |
| M542H450 | PIPE CULVERTS, CLASS D, TYPE 1, 750MM | METER | 90.0 | 90.0 | 0.0 |
| M542H455 | PIPE CULVERTS, CLASS D, TYPE 1, 900MM | METER | 76.0 | 76.0 | 0.0 |
| M542H460 | PIPE CULVERTS, CLASS D, TYPE 1, 1050MM | METER | 24.4 | 24.4 | 0.0 |
| M542T220 | PIPE CULVERTS, CLASS C, TYPE 1, 375MM (TEMPORARY) | METER | 58.0 | 58.0 | 0.0 |
| M542T225 | PIPE CULVERTS, CLASS C, TYPE 1, 450MM (TEMPORARY) | METER | 6.5 | 6.5 | 0.0 |
| M542T235 | PIPE CULVERTS, CLASS C, TYPE 1 600MM (TEMPORARY) | METER | 6.5 | 6.5 | 0.0 |
| M5900200 | EPOXY CRACK INJECTION | METER | 3.6 | 3.6 | 0.0 |
| M6300100 | STEEL PLATE BEAM GUARD RAIL, TYPE A | METER | 438.15 | 438.15 | 0.00 |

* *Specialty Hems*
* SEE SPECIAL PROVISIONS

CONTINUED

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|--------------|---------------------------------------|
| NAME | DATE | |
| | | SUMMARY OF QUANTITIES |
| | | |
| | | |
| | | |
| | | |
| | | |
| SCALE | DRAWN BY KOJ | DATE DECEMBER 22, 2008 CHECKED BY LWJ |
| | | |

PLT DATE = 1/29/2009
FILE NAME = H:\1428\Contract 94533\SHI_004_summ_VB.dgn

H. M. & G. NO. 4420

| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 7 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

SUMMARY OF QUANTITIES

| CODE NO. | ITEM DESCRIPTION | UNIT | TOTAL QUANTITY | 80% FED 20% STATE | |
|----------|--|-------|----------------|--------------------------|--------------------------|
| | | | | F.A.P. 781 (IL. RTE. 33) | |
| | | | | 4+920.234 TO 15+545.000 | 15+545.000 TO 16+016.000 |
| | | | | CONSTRUCTION CODE TYPE | |
| | | | | 1000-2A RURAL | 1000-2A URBAN |
| M6320030 | GUARDRAIL REMOVAL | METER | 5 | 5 | 0 |
| M7030100 | SHORT-TERM PAVEMENT MARKING | METER | 4,360 | 4,171 | 189 |
| M7030620 | TEMPORARY PAINT PAVEMENT MARKING LINE, 100 MM | METER | 29,454 | 28,479 | 975 |
| M7040100 | TEMPORARY CONCRETE BARRIER | METER | 1,230 | 1,130 | 100 |
| M7040200 | RELOCATE TEMPORARY CONCRETE BARRIER | METER | 1,130 | 1,030 | 100 |
| M7800205 | PAINT PAVEMENT MARKING LINE, 100 MM | METER | 29,454 | 28,479 | 975 |
| MX030199 | TEMPORARY PAVEMENT | SQ M | 3,451 | 3,451 | 0 |
| MX030203 | TEMPORARY PAVEMENT REMOVAL | SQ M | 3,451 | 3,451 | 0 |
| MX032196 | POLYMER MODIFIED PORTLAND CEMENT MORTAR | SQ M | 17.9 | 17.9 | 0.0 |
| MX033694 | STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 125 MM) | SQ M | 2.5 | 2.5 | 0.0 |
| MX033731 | PRECAST CONCRETE BOX CULVERT 900MM x 450MM | METER | 10.0 | 10.0 | 0.0 |
| MX033732 | PRECAST CONCRETE BOX CULVERT 3.0M x 1.65M | METER | 10.9 | 10.9 | 0.0 |
| MX033735 | PRECAST CONCRETE BOX CULVERT 3.0M x 0.9M | METER | 43.2 | 43.2 | 0.0 |
| MX033736 | PRECAST CONCRETE BOX CULVERT 750MM x 450MM (M273) | METER | 51.0 | 36.6 | 14.4 |
| MX033748 | TEMPORARY EMBANKMENT PLACEMENT AND REMOVAL | CU M | 855 | 855 | 0 |
| Z0007601 | BUILDING REMOVAL NO. 1 | L SUM | 1 | 1 | 0 |
| Z0030250 | IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3 | EACH | 12 | 10 | 2 |
| Z0030350 | IMPACT ATTENUATOR, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3 | EACH | 11 | 9 | 2 |
| Z0076600 | TRAINEES | HOUR | 500 | 500 | 0 |
| 28000300 | TEMPORARY DITCH CHECKS | EACH | 408 | 398 | 10 |
| 28000500 | INLET AND PIPE PROTECTION | EACH | 158 | 149 | 9 |
| 50100300 | REMOVAL OF EXISTING STRUCTURES NO. 1 | EACH | 1 | 1 | 0 |
| 50100400 | REMOVAL OF EXISTING STRUCTURES NO. 2 | EACH | 1 | 1 | 0 |
| 50100500 | REMOVAL OF EXISTING STRUCTURES NO. 3 | EACH | 1 | 1 | 0 |
| 50100600 | REMOVAL OF EXISTING STRUCTURES NO. 4 | EACH | 1 | 0 | 1 |
| 50100700 | REMOVAL OF EXISTING STRUCTURES NO. 5 | EACH | 1 | 0 | 1 |
| 54001008 | BOX CULVERT END SECTION, CULVERT NO. 8 | EACH | 2 | 2 | 0 |
| 54001009 | BOX CULVERT END SECTION, CULVERT NO. 9 | EACH | 2 | 2 | 0 |
| 54001010 | BOX CULVERT END SECTION, CULVERT NO. 10 | EACH | 2 | 2 | 0 |
| 54001011 | BOX CULVERT END SECTION, CULVERT NO. 11 | EACH | 2 | 2 | 0 |
| 54001012 | BOX CULVERT END SECTION, CULVERT NO. 12 | EACH | 2 | 2 | 0 |
| 54001013 | BOX CULVERT END SECTION, CULVERT NO. 13 | EACH | 2 | 2 | 0 |
| 54001014 | BOX CULVERT END SECTION, CULVERT NO. 14 | EACH | 2 | 2 | 0 |

Ⓞ Y080

* SEE SPECIAL PROVISIONS

▲ Specialty Items

CONTINUED

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|---|
| NAME | DATE | |
| | | SUMMARY OF QUANTITIES SCALE _____ DRAWN BY KOJ DATE DECEMBER 22, 2008 CHECKED BY LWJ |
| | | |
| | | |
| | | |
| | | |

PLOT DATE = 1/29/2009
 FILE NAME = H:\1420\Contract 94533\SHT_084.sumq.vb.dgn

H. M. & G. NO. 4420

| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 8 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

SUMMARY OF QUANTITIES

| CODE NO. | ITEM DESCRIPTION | UNIT | TOTAL QUANTITY | 80% FED 20% STATE F.A.P. 781 (IL. RTE. 33) | |
|--------------|---|--------|----------------|---|--------------------------|
| | | | | 4+920.234 TO 15+545.000 | 15+545.000 TO 16+016.000 |
| | | | | CONSTRUCTION CODE TYPE | |
| | | | | I000-2A RURAL | I000-2A URBAN |
| 54001015 | BOX CULVERT END SECTION, CULVERT NO. 15 | EACH | 2 | 2 | 0 |
| 54001016 | BOX CULVERT END SECTION, CULVERT NO. 16 | EACH | 2 | 2 | 0 |
| 54001017 | BOX CULVERT END SECTION, CULVERT NO. 17 | EACH | 2 | 2 | 0 |
| 54001018 | BOX CULVERT END SECTION, CULVERT NO. 18 | EACH | 2 | 2 | 0 |
| 54001019 | BOX CULVERT END SECTION, CULVERT NO. 19 | EACH | 2 | 2 | 0 |
| 54001020 | BOX CULVERT END SECTION, CULVERT NO. 20 | EACH | 2 | 2 | 0 |
| 54001021 | BOX CULVERT END SECTION, CULVERT NO. 21 | EACH | 2 | 2 | 0 |
| 54001022 | BOX CULVERT END SECTION, CULVERT NO. 22 | EACH | 2 | 0 | 2 |
| 60236200 | INLETS, TYPE A, TYPE 8 GRATE | EACH | 2 | 2 | 0 |
| 60500060 | REMOVING INLETS | EACH | 1 | 1 | 0 |
| Δ * 63100045 | TRAFFIC BARRIER TERMINAL TYPE 2 | EACH | 1 | 1 | 0 |
| Δ * 63100167 | TRAFFIC BARRIER TERMINAL TYPE 1, SPECIAL (TANGENT) | EACH | 10 | 10 | 0 |
| Δ * 63100169 | TRAFFIC BARRIER TERMINAL TYPE 1, SPECIAL (FLARED) | EACH | 1 | 1 | 0 |
| 66600105 | FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS | EACH | 109 | 96 | 13 |
| 66700205 | PERMANENT SURVEY MARKERS, TYPE 1 | EACH | 39 | 39 | 0 |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 12 | 11 | 1 |
| 67100100 | MOBILIZATION | L SUM | 1 | 1 | 0 |
| * 70100450 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701201 | L SUM | 1 | 1 | 0 |
| * 70100460 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701306 | L SUM | 1 | 1 | 0 |
| * 70100500 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701326 | L SUM | 1 | 1 | 0 |
| * 70101000 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701331 (SPECIAL) | EACH | 1 | 1 | 0 |
| * 70101205 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701321 (SPECIAL) | EACH | 5 | 4 | 1 |
| 70103815 | TRAFFIC CONTROL SURVEILLANCE | CAL DA | 25 | 20 | 5 |
| 70106500 | TEMPORARY BRIDGE TRAFFIC SIGNALS | EACH | 5 | 4 | 1 |
| 70106700 | TEMPORARY RUMBLE STRIP | EACH | 30 | 24 | 6 |
| Δ 78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 449 | 429 | 20 |
| Δ 78200410 | GUARDRAIL MARKERS, TYPE A | EACH | 24 | 24 | 0 |
| Δ 78201000 | TERMINAL MARKER - DIRECT APPLIED | EACH | 12 | 12 | 0 |
| MX033776 | PRECAST CONCRETE BOX CULVERT 900MM x 450MM (M273) | METER | 29.7 | 29.7 | 0.0 |

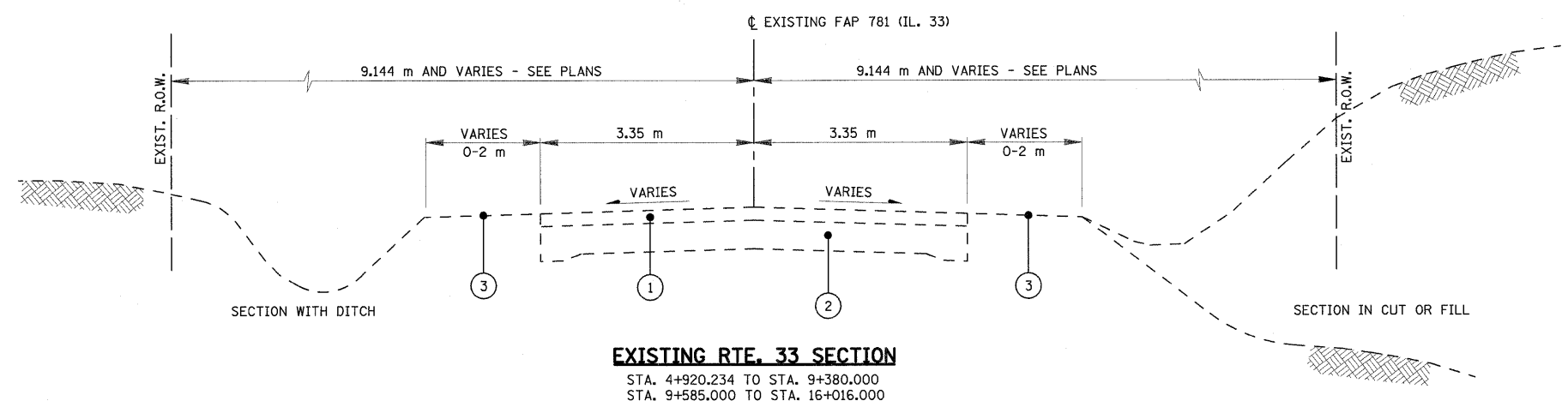
Δ Specialty Items
* SEE SPECIAL PROVISIONS

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|--------------|---------------------------------------|
| NAME | DATE | |
| | | SUMMARY OF QUANTITIES |
| | | |
| | | |
| | | |
| | | |
| SCALE | DRAWN BY KOJ | DATE DECEMBER 22, 2008 CHECKED BY LWJ |
| | | |

PLOT DATE = 1/29/2009
 FILE NAME = H:\4420\Contract 94533\SH_004_sump_VB.dgn

H. M. & G. NO. 4420

| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | LYRS | CRAWFORD | 212 | 9 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



LEGEND

- ① EXISTING BITUMINOUS SURFACE
- ② EXISTING P.C. CONCRETE BASE
- ③ EXISTING EARTH SHOULDER
- ④ EXISTING BITUMINOUS PAVEMENT
- ⑤ EXISTING BITUMINOUS SHOULDER
- ⑥ M4400715 - HOT-MIX ASPHALT SURFACE REMOVAL, 15mm
- ⑦ M4400950 - HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
- ⑧ M4402000 - PAVEMENT REMOVAL
- ⑨ M4400740 - HOT-MIX ASPHALT SURFACE REMOVAL, 40mm
- ⑩ M3552100 - HOT-MIX ASPHALT BASE COURSE, VARIABLE DEPTH
- ⑪ M4820550 - HOT-MIX ASPHALT SHOULDERS, 150mm
- ⑫ M4063340 - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (38mm)
- ⑬ M4062135 - LEVELING BINDER (MACHINE METHOD), N70
- ⑭ EXISTING AGGREGATE SHOULDER
- ⑮ M4075350 - HOT-MIX ASPHALT PAVEMENT (FULL DEPTH), 350mm (312mm BASE COURSE, 38mm SURFACE COURSE)
- ⑯ M3111100 - SUB-BASE GRANULAR MATERIAL, TYPE B, 100mm
- ⑰ M3021500 - LIME
- ⑱ M3020456 - PROCESSING MODIFIED SOIL MIXTURE, 300mm
- ⑲ M4810150 - AGGREGATE SHOULDERS, TYPE B 150mm
- ⑳ M2112500 - TOPSOIL EXCAVATION
- ㉑ M2112500 - TOPSOIL PLACEMENT
- ㉒ M2021200 - REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL

PLOT DATE = 12/22/2008
 FILE NAME = P:\V428\Contract\94533\SHI_2009.dgn

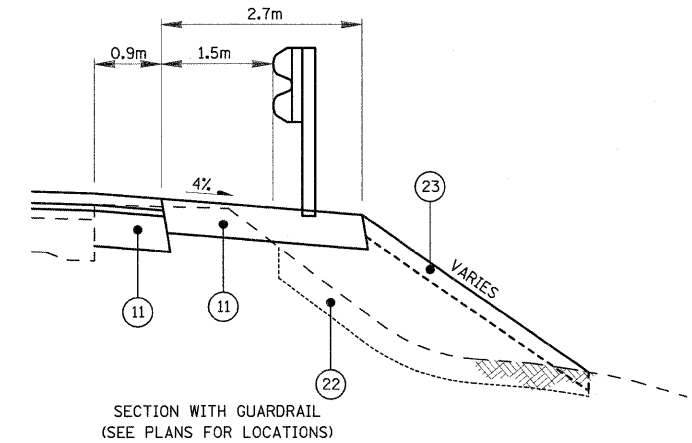
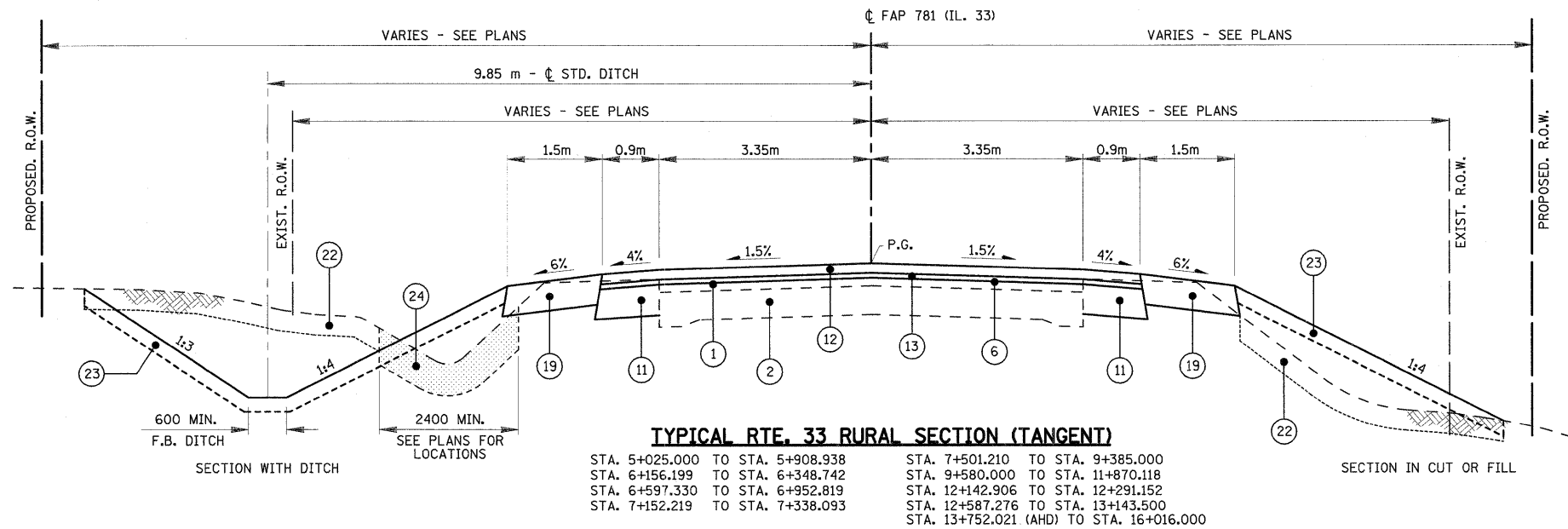
| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
F.A.P. 781

SCALE NO SCALE DRAWN BY KOJ
 DATE DECEMBER 22, 2008 CHECKED BY LWJ

| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | YRS | CRAWFORD | 212 | 10 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



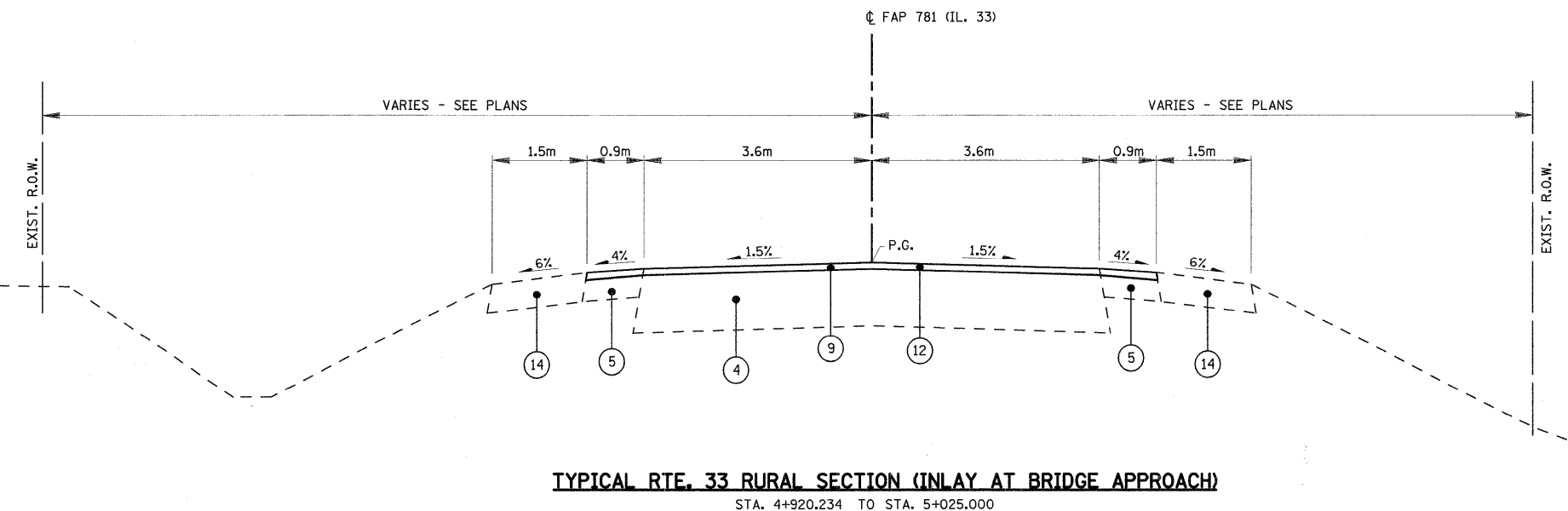
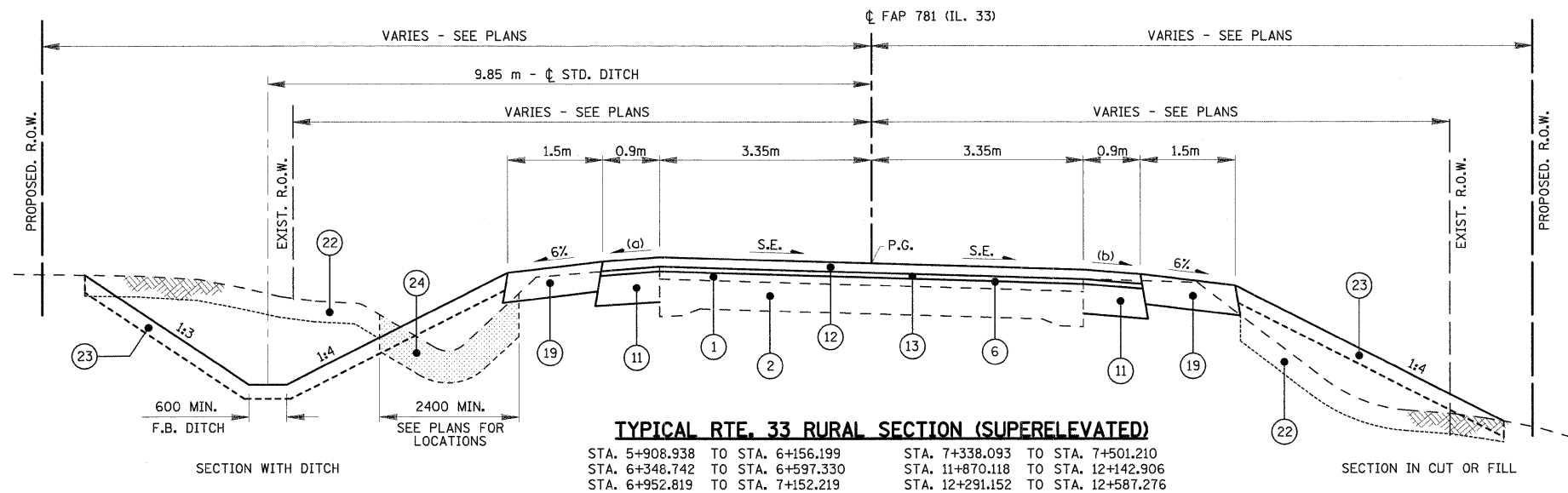
LEGEND

- ① EXISTING BITUMINOUS SURFACE
- ② EXISTING P.C. CONCRETE BASE
- ③ EXISTING EARTH SHOULDER
- ④ EXISTING BITUMINOUS PAVEMENT
- ⑤ EXISTING BITUMINOUS SHOULDER
- ⑥ M4400715 - HOT-MIX ASPHALT SURFACE REMOVAL, 15mm
- ⑦ M4400950 - HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
- ⑧ M4402000 - PAVEMENT REMOVAL
- ⑨ M4400740 - HOT-MIX ASPHALT SURFACE REMOVAL, 40mm
- ⑩ M3552100 - HOT-MIX ASPHALT BASE COURSE, VARIABLE DEPTH
- ⑪ M4820550 - HOT-MIX ASPHALT SHOULDERS, 150mm
- ⑫ M4063340 - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (38mm)
- ⑬ M4062135 - LEVELING BINDER (MACHINE METHOD), N70
- ⑭ EXISTING AGGREGATE SHOULDER
- ⑮ M4075350 - HOT-MIX ASPHALT PAVEMENT (FULL DEPTH), 350mm (312mm BASE COURSE, 38mm SURFACE COURSE)
- ⑯ M3111100 - SUB-BASE GRANULAR MATERIAL, TYPE B, 100mm
- ⑰ M3021500 - LIME
- ⑱ M3020456 - PROCESSING MODIFIED SOIL MIXTURE, 300mm
- ⑲ M4810150 - AGGREGATE SHOULDERS, TYPE B 150mm
- ⑳ M2112500 - TOPSOIL EXCAVATION
- ㉑ M2112500 - TOPSOIL PLACEMENT
- ㉒ M2021200 - REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL

NOTES:

- LEVELING BINDER THICKNESS TO BE 19 mm AT CENTERLINE AND 19 mm OR GREATER AT PAVEMENT EDGES, EXCEPT AT THE BRUSH CREEK VERTICAL REALIGNMENT AREA IN WHICH THE LEVELING BINDER THICKNESS WILL VARY.
- DITCH DEPTH AND BACKSLOPES VARY IN SPECIAL DITCH LOCATIONS AND BY CERTAIN LANDOWNERS; SEE PLANS FOR EXCEPTION AREAS
- WIDENING WIDTH IS BASED ON EDGE OF UNDERLAYING CONCRETE PAVEMENT. VISABLE EDGE OF PAVEMENT MAY VARY IN DISTANCE FROM CENTERLINE DUE TO SUBSEQUENT OVERLAYS.

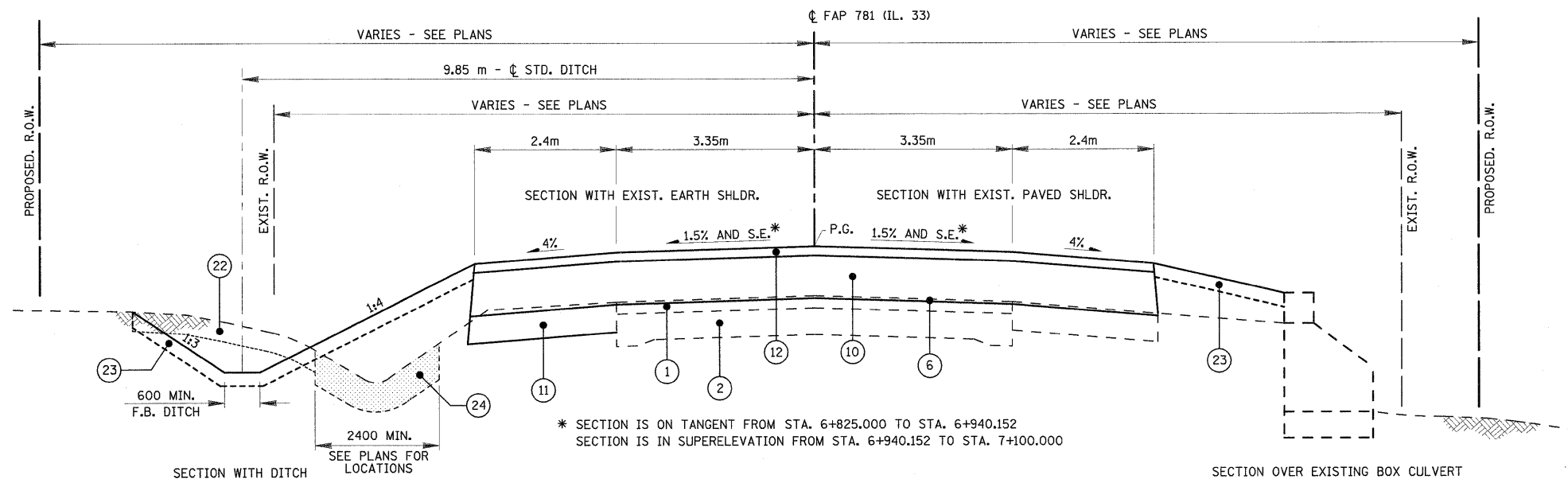
- (a) WHEN THE SUPERELEVATION RATE OF THE PAVEMENT IS BETWEEN 0% AND 4%, THE SHOULDER SLOPE SHALL BE 4%. WHEN THE SUPERELEVATION RATE OF THE PAVEMENT EXCEEDS 4%, THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT AND SHOULDER IS NOT GREATER THAN 8%.
- (b) SLOPE SHALL BE THE SAME AS THE SUPERELEVATION RATE, BUT NOT LESS THAN 4%.



| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|--|
| NAME | DATE | |
| | | TYPICAL SECTIONS F.A.P. 781 SCALE NO SCALE DRAWN BY KOJ DATE DECEMBER 22, 2008 CHECKED BY LWJ |
| | | |
| | | |
| | | |
| | | |

PLOT DATE = 12/22/2008
 FILE NAME = H:\4420\Contract\94533\SHIT_009_typsec.dgn

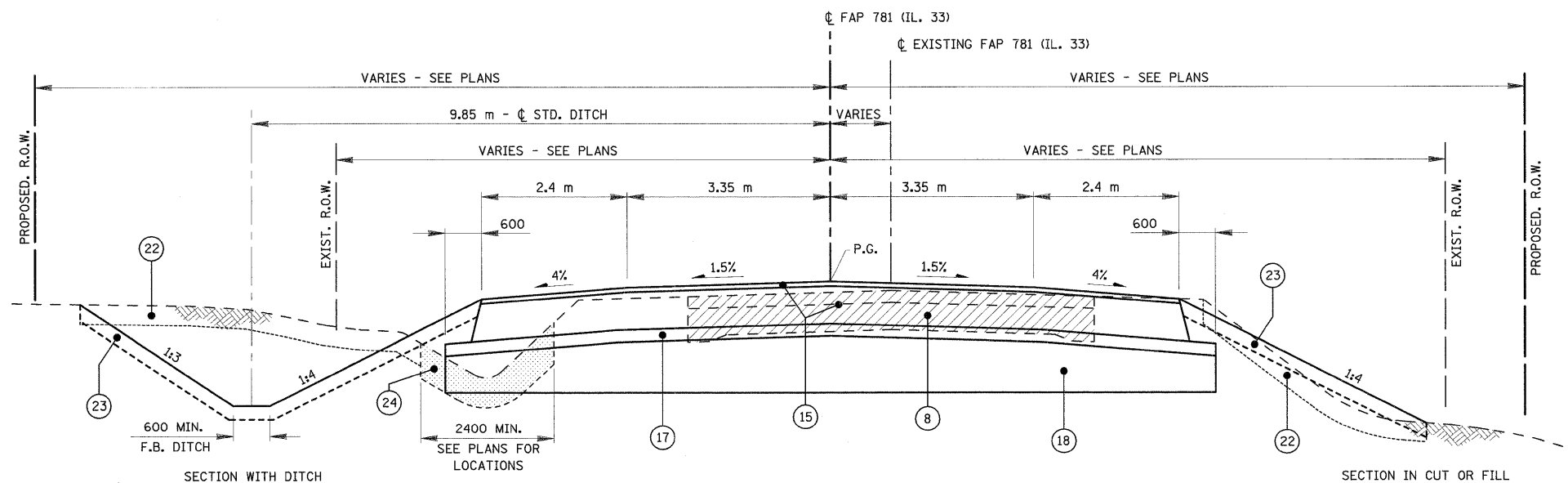
| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 11 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



TYPICAL RTE. 33 RURAL SECTION - VERTICAL REALIGNMENT
STA. 6+825.000 TO STA. 7+100.000

LEGEND

- ① EXISTING BITUMINOUS SURFACE
- ② EXISTING P.C. CONCRETE BASE
- ③ EXISTING EARTH SHOULDER
- ④ EXISTING BITUMINOUS PAVEMENT
- ⑤ EXISTING BITUMINOUS SHOULDER
- ⑥ M4400715 - HOT-MIX ASPHALT SURFACE REMOVAL, 15mm
- ⑦ M4400950 - HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
- ⑧ M4402000 - PAVEMENT REMOVAL
- ⑨ M4400740 - HOT-MIX ASPHALT SURFACE REMOVAL, 40mm
- ⑩ M3552100 - HOT-MIX ASPHALT BASE COURSE, VARIABLE DEPTH
- ⑪ M4820550 - HOT-MIX ASPHALT SHOULDERS, 150mm
- ⑫ M4063340 - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (38mm)
- ⑬ M4062135 - LEVELING BINDER (MACHINE METHOD), N70
- ⑭ EXISTING AGGREGATE SHOULDER
- ⑮ M4075350 - HOT-MIX ASPHALT PAVEMENT (FULL DEPTH), 350mm (312mm BASE COURSE, 38mm SURFACE COURSE)
- ⑯ M3111100 - SUB-BASE GRANULAR MATERIAL, TYPE B, 100mm
- ⑰ M3021500 - LIME
- ⑱ M3020456 - PROCESSING MODIFIED SOIL MIXTURE, 300mm
- ⑲ M4810150 - AGGREGATE SHOULDERS, TYPE B 150mm
- ⑳ M2112500 - TOPSOIL EXCAVATION
- ㉑ M2112500 - TOPSOIL PLACEMENT
- ㉒ M2021200 - REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL



TYPICAL RTE. 33 RURAL SECTION - RECONSTRUCTION
STA. 13+143.500 TO STA. 13+750.733 (BK)

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|--|
| NAME | DATE | |
| | | TYPICAL SECTIONS F.A.P. 781 |
| | | |
| | | |
| | | |
| | | |
| | | |

SCALE NO SCALE DRAWN BY KOJ
DATE DECEMBER 22, 2008 CHECKED BY LWJ

PLOT DATE = 12/22/2008
FILE NAME = H:\1420\Contract 94533\SH1_009.dwg

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------|------------------|-----------|
| 781 | LYRS | CRAWFORD | 212 | 12 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

**TREE REMOVAL (6 TO 15 UNITS DIAMETER)
TREE REMOVAL (OVER 15 UNITS DIAMETER)
TREE REMOVAL HECTARES**

| LOCATION | | | EXISTING OCTOBER, 1996 TREE SURVEY | COMMON NAME | TREE REMOVAL (6 TO 15 UNITS DIAMETER) | TREE REMOVAL (OVER 15 UNITS DIAMETER) | TREE REMOVAL, HECTARES |
|----------------------------------|--------|-------|------------------------------------|------------------|---------------------------------------|---------------------------------------|------------------------|
| STATION | OFFSET | RT/LT | MM | | UNIT | UNIT | HA |
| RURAL | | | | | | | |
| STA 6+732.0 | 10.8 | RT | 625 | RED MAPLE | | 25 | |
| STA 6+829.0 | 13.0 | RT | | ELM | 6 | | |
| STA 6+835.4 | 13.8 | RT | 675 | RED MAPLE | | 27 | |
| STA 6+845.0 | 14.0 | RT | | ELM | 6 | | |
| STA 6+850.5 | 12.3 | RT | 675 | RED MAPLE | | 27 | |
| STA 6+869.8 | 8.6 | RT | 150/75 TWIN | SLIPPERY ELM | 9 | | |
| STA 7+249.5 | 6.7 | LT | 425 | RED MAPLE | | 17 | |
| STA 7+257.7 | 7.2 | LT | 650 | RED MAPLE | | 26 | |
| STA 7+264.4 | 7.1 | LT | 325 | SUGAR MAPLE | 13 | | |
| STA 7+271.9 | 6.7 | LT | 550 | RED MAPLE | | 22 | |
| STA 7+294.0 TO 7+328.0 | 22.0 | LT | | | | | 0.07 |
| STA 7+405.7 | 8.1 | RT | 800 | AMERICAN ELM | | 32 | |
| STA 7+613.6 | 10.9 | RT | 175 | WHITE PINE | 7 | | |
| STA 7+616.2 | 10.8 | RT | 150 | WHITE PINE | 6 | | |
| STA 7+622.2 | 11.1 | RT | 175 | WHITE PINE | 7 | | |
| STA 7+627.3 | 11.4 | RT | 150 | WHITE PINE | 6 | | |
| STA 7+629.3 | 11.4 | RT | 150 | WHITE PINE | 6 | | |
| STA 7+632.2 | 10.7 | RT | 175 | WHITE PINE | 7 | | |
| STA 7+636.3 | 11.1 | RT | 150 | WHITE PINE | 6 | | |
| STA 7+641.0 | 11.1 | RT | | EVERGREEN | 7 | | |
| STA 7+644.7 | 11.0 | RT | | EVERGREEN | 6 | | |
| STA 8+046.1 | 11.0 | RT | 900 | SILVER MAPLE | | 36 | |
| STA 8+054.1 | 11.4 | RT | 600 | HACKBERRY | | 24 | |
| STA 8+746.3 | 9.5 | LT | 325 | RIVER BIRCH | 13 | | |
| STA 9+675.0 TO 9+904.0 | 14.0 | LT | | | | | 0.11 |
| STA 10+181.0 TO 10+231.0 | 14.0 | RT | | | | | 0.03 |
| STA 11+123.0 TO 11+143.0 | 10.0 | LT | | | | | 0.01 |
| STA 11+465.0 TO 11+555.0 | 16.0 | LT | | | | | 0.05 |
| STA 11+475.0 TO 11+508.0 | 16.0 | RT | | | | | 0.02 |
| STA 11+755.0 TO 11+786.0 | 19.0 | RT | | | | | 0.01 |
| STA 11+767.0 TO 11+792.0 | 13.0 | LT | | | | | 0.02 |
| STA 12+489.0 TO 12+615.0 | 15.0 | LT | | | | | 0.11 |
| STA 12+703.0 TO 12+753.0 | 10.0 | RT | | | | | 0.02 |
| STA 12+773.0 TO 12+846.0 | 20.0 | RT | | | | | 0.02 |
| STA 12+793.2 | 11.9 | LT | 950 | WHITE OAK | | 38 | |
| STA 12+809.6 | 12.1 | LT | 450 | SHAGBARK HKRY | | 18 | |
| STA 12+835.3 | 12.0 | LT | 800 | WHITE OAK | | 32 | |
| STA 13+033.0 TO 13+050.0 | 13.0 | RT | | | | | 0.01 |
| STA 13+109.4 | 10.5 | LT | 300 | WHITE OAK | 12 | | |
| STA 13+506.2 | 7.5 | RT | 425 | SUGAR MAPLE | | 17 | |
| STA 13+523.9 | 7.5 | RT | 425 | SUGAR MAPLE | | 17 | |
| STA 13+532.4 | 7.2 | RT | 400 | RED MAPLE | | 16 | |
| STA 13+564.2 | 9.0 | RT | 500 | WHITE PINE | | 20 | |
| STA 13+569.6 | 8.9 | RT | 550 | RED MULBERRY | | 22 | |
| STA 13+575.3 | 10.4 | RT | 250 | RED PINE | 10 | | |
| STA 13+585.0 | 9.7 | RT | 500 | ESTRN WHITE PINE | | 20 | |
| STA 13+595.0 | 9.5 | RT | 325 | CHINESE ELM | 13 | | |
| STA 13+620.9 | 9.2 | RT | 750 | SHINGLE OAK | | 30 | |
| STA 13+629.2 | 12.8 | RT | 325 | ESTRN RED CEDAR | 13 | | |
| STA 13+642.9 | 12.6 | RT | 600 | GREEN ASH | | 24 | |
| TOTALS | | | | | 153 | 490 | 0.48 |
| TOTALS ROUNDED FOR SUMMARY SHEET | | | | | 153 | 490 | 0.5 |

PAVED DITCH REMOVAL

| LOCATION | | | PAVED DITCH REMOVAL |
|----------------------------------|-------|--|---------------------|
| STATION | RT/LT | | M4405000 METER |
| RURAL | | | |
| STA. 5+008.0 TO 5+116.0 | LT | | 108.0 |
| STA. 9+063.0 TO 9+184.5 | RT | | 121.5 |
| STA. 9+195.0 TO 9+208.0 | RT | | 14.5 |
| STA. 9+652.0 TO 9+676.5 | RT | | 26.0 |
| STA. 9+687.5 TO 9+712.5 | RT | | 25.0 |
| STA. 9+704.6 TO 9+769.0 | LT | | 64.4 |
| STA. 9+727.5 TO 9+769.0 | RT | | 41.5 |
| STA. 10+066.5 TO 10+115.0 | RT | | 74.0 |
| STA. 10+193.3 TO 10+255.7 | LT | | 62.8 |
| STA. 11+440.3 TO 11+474.0 | RT | | 33.7 |
| STA. 11+441.8 TO 11+471.0 | LT | | 29.2 |
| STA. 11+500.0 TO 11+536.0 | LT | | 37.0 |
| STA. 11+534.0 TO 11+559.0 | RT | | 25.5 |
| STA. 11+760.0 TO 11+775.7 | RT | | 15.7 |
| STA. 11+805.8 TO 11+820.4 | LT | | 14.6 |
| STA. 12+357.5 TO 12+447.5 | LT | | 90.0 |
| STA. 12+358.5 TO 12+445.5 | RT | | 87.0 |
| TOTALS | | | 870.4 |
| TOTALS ROUNDED FOR SUMMARY SHEET | | | 871 |

GUARDRAIL REMOVAL

| LOCATION | | | GUARDRAIL REMOVAL |
|---------------------------------|-------|--|-------------------|
| STATION | RT/LT | | M6320030 METER |
| RURAL | | | |
| STA 6+323.800 TO 6+328.600 | RT | | 4.8 |
| TOTAL | | | 4.8 |
| TOTAL ROUNDED FOR SUMMARY SHEET | | | 5 |

PAVEMENT REMOVAL

| LOCATION | | | | PAVEMENT REMOVAL |
|----------------------------------|----|----------------|-------|------------------|
| STATION | TO | STATION | LT/RT | M4402000 SQ M |
| RURAL | | | | |
| 8+910.900 | | | LT | 252.52 |
| 13+143.500 | TO | 13+750.733(BK) | LT&RT | 4462.34 |
| 13+391.700 | | | LT | 330.95 |
| 15+062.400 | | | RT | 216.95 |
| TOTAL | | | | 5262.76 |
| TOTALS ROUNDED FOR SUMMARY SHEET | | | | 5,263 |

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION SCHEDULE OF QUANTITIES SCALE NO SCALE DRAWN BY KOJ DATE DECEMBER 22, 2008 CHECKED BY LWJ |
|-----------|------|--|
| NAME | DATE | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |

PLOT DATE = 12/22/2008
FILE NAME = H:\V\20\Contract\94533\SHI_014_sch_all_schedules_V8.dgn

H. M. & G. NO. 4420

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------|------------------|-----------|
| 781 | LYRS | CRAWFORD | 212 | 13 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

HOT-MIX ASPHALT SURFACE REMOVAL

| LOCATION | | | | HOT-MIX ASPHALT SURFACE REMOVAL | | | | TEMPORARY RAMP | | |
|---|----|------------|---------|---------------------------------|----------|-------------------|----------------|----------------|------------|----------|
| | | | | 15mm | | 40mm | VARIABLE DEPTH | | BUTT JOINT | M4060990 |
| | | | | M4400715 | M4400740 | 15mm @ CENTERLINE | M4060982 | | | |
| STATION | TO | STATION | RT/LT | SQ M | SQ M | SQ M | SQ M | SQ M | | |
| RURAL | | | | | | | | | | |
| 4+920.234 | TO | 5+025.000 | LT & RT | | | 4337.3 | | 14.4 | | |
| 5+025.000 | TO | 5+045.000 | LT & RT | | | 140.8 | | | | |
| 5+045.000 | TO | 5+370.000 | LT | 1244.2 | | | | | | |
| 5+045.000 | TO | 5+940.000 | RT | 3284.3 | | | | | | |
| 5+370.000 | TO | 5+730.000 | LT | | | 1424.6 | | | | |
| 5+730.000 | TO | 6+560.000 | LT | 2950.3 | | | | | | |
| 5+940.000 | TO | 6+120.000 | RT | | | 672.4 | | | | |
| 6+120.000 | TO | 6+230.000 | RT | 400.2 | | | | | | |
| 6+230.000 | TO | 6+690.000 | RT | | | 1799.8 | | | | |
| 6+560.000 | TO | 6+690.000 | LT | | | 471.6 | | | | |
| 6+690.000 | TO | 7+130.000 | RT | 1636.2 | | | | | | |
| 6+690.000 | TO | 7+370.000 | LT | 2414.1 | | | | | | |
| 7+130.000 | TO | 7+170.000 | RT | | | 143.6 | | | | |
| 7+170.000 | TO | 7+490.000 | RT | 1137.4 | | | | | | |
| 7+370.000 | TO | 7+470.000 | LT | | | 382.1 | | | | |
| 7+470.000 | TO | 8+420.000 | LT | 3574.4 | | | | | | |
| 7+490.000 | TO | 7+520.000 | RT | | | 106.0 | | | | |
| 7+520.000 | TO | 7+680.000 | RT | 597.1 | | | | | | |
| 7+680.000 | TO | 7+730.000 | RT | | | 192.2 | | | | |
| 7+730.000 | TO | 8+440.000 | RT | 2723.1 | | | | | | |
| 8+420.000 | TO | 8+460.000 | LT | | | 148.4 | | | | |
| 8+440.000 | TO | 8+500.000 | RT | | | 237.9 | | | | |
| 8+460.000 | TO | 9+365.000 | LT | 3243.8 | | | | | | |
| 8+500.000 | TO | 8+590.000 | RT | 361.8 | | | | | | |
| 8+590.000 | TO | 8+630.000 | RT | | | 159.7 | | | | |
| 8+630.000 | TO | 8+710.000 | RT | 315.7 | | | | | | |
| 8+710.000 | TO | 8+790.000 | RT | | | 310.5 | | | | |
| 8+790.000 | TO | 9+365.000 | RT | 2043.7 | | | | | | |
| 9+365.000 | TO | 9+385.000 | LT & RT | | | | 138.2 | 13.8 | | |
| BIG CREEK BRIDGE AND ROAD OMISSION | | | | | | | | | | |
| 9+580.000 | TO | 9+600.000 | LT & RT | | | | 140.0 | 14.0 | | |
| 9+600.000 | TO | 9+980.000 | LT | 1444.8 | | | | | | |
| 9+600.000 | TO | 12+165.000 | RT | 9271.0 | | | | | | |
| 9+980.000 | TO | 10+010.000 | LT | | | 120.1 | | | | |
| 10+010.000 | TO | 13+143.500 | LT | 11576.0 | | | | | | |
| 12+165.000 | TO | 12+355.000 | RT | | | 663.3 | | | | |
| 12+355.000 | TO | 13+143.500 | RT | 2847.7 | | | | | | |
| RELOCATION (NO SURFACE REMOVAL) | | | | | | | | | | |
| 13+752.021 | TO | 13+820.000 | LT | 254.4 | | | | | | |
| 13+752.021 | TO | 14+655.000 | RT | 3216.6 | | | | | | |
| 13+820.000 | TO | 13+885.000 | LT | | | 241.7 | | | | |
| 13+885.000 | TO | 14+460.000 | LT | 2230.1 | | | | | | |
| 14+460.000 | TO | 14+560.000 | LT | | | 386.1 | | | | |
| 14+560.000 | TO | 15+545.000 | LT | 3796.2 | | | | | | |
| 14+655.000 | TO | 14+870.000 | RT | | | 878.6 | | | | |
| 14+870.000 | TO | 15+545.000 | RT | 2475.0 | | | | | | |
| SUBTOTALS | | | | 63038.1 | 4478.1 | 8338.6 | 278.2 | 42.2 | | |
| URBAN | | | | | | | | | | |
| 15+545.000 | TO | 15+996.000 | LT | 1690.0 | | | | | | |
| 15+545.000 | TO | 15+996.000 | RT | 1721.4 | | | | | | |
| 15+996.000 | TO | 16+016.000 | LT & RT | | | 168.4 | 16.8 | | | |
| SUBTOTALS | | | | 3411.4 | 0.0 | 0.0 | 168.4 | 16.8 | | |
| TOTALS | | | | 66449.5 | 4478.1 | 8338.6 | 446.6 | 59.1 | | |
| TOTALS ROUNDED FOR SUMMARY SHEET | | | | 66450 | 4479 | 8339 | 447 | 60 | | |

DRIVEWAY PAVEMENT REMOVAL

| LOCATION | | | DRIVEWAY PAVEMENT REMOVAL |
|---|------------|-------|---------------------------|
| STATION | | RT/LT | M4402010 |
| | | | SQ M |
| RURAL | | | |
| STA | 6+483.500 | RT | 415.8 |
| STA | 6+572.500 | RT | 28.0 |
| STA | 6+784.500 | RT | 55.6 |
| STA | 6+819.200 | RT | 63.6 |
| STA | 7+090.000 | LT | 39.3 |
| STA | 7+328.600 | LT | 113.6 |
| STA | 7+509.000 | LT | 60.5 |
| STA | 8+063.000 | RT | 35.9 |
| STA | 8+607.000 | RT | 55.4 |
| STA | 8+651.000 | RT | 81.0 |
| STA | 8+705.600 | LT | 60.4 |
| STA | 8+739.700 | LT | 99.2 |
| STA | 8+770.500 | LT | 57.1 |
| STA | 8+792.000 | LT | 63.4 |
| STA | 8+793.300 | RT | 92.8 |
| STA | 8+843.000 | LT | 391.3 |
| STA | 9+073.000 | LT | 93.1 |
| STA | 9+948.600 | LT | 104.4 |
| STA | 10+500.000 | LT | 206.2 |
| STA | 11+571.600 | RT | 230.1 |
| STA | 11+698.000 | RT | 234.1 |
| STA | 11+756.500 | LT | 124.8 |
| STA | 13+097.000 | LT | 26.3 |
| STA | 13+140.500 | LT | 56.9 |
| STA | 13+163.500 | LT | 74.5 |
| STA | 13+802.800 | RT | 65.2 |
| STA | 14+242.400 | LT | 482.5 |
| STA | 14+265.500 | LT | 386.1 |
| STA | 14+308.900 | LT | 416.3 |
| STA | 14+447.300 | LT | 132.2 |
| STA | 14+447.300 | RT | 106.9 |
| STA | 14+600.800 | LT | 62.5 |
| STA | 14+629.900 | RT | 130.1 |
| STA | 14+664.800 | RT | 93.3 |
| STA | 14+711.600 | LT | 101.1 |
| STA | 14+725.800 | RT | 64.0 |
| STA | 14+763.300 | RT | 164.8 |
| STA | 14+926.300 | LT | 55.2 |
| STA | 15+321.000 | LT | 128.4 |
| STA | 15+454.800 | LT | 160.0 |
| TOWNSHIP ROAD TR 450E | | | |
| STA | 10+040.000 | RT | 49.3 |
| SUBTOTAL | | | 5,461.2 |
| URBAN | | | |
| STA | 15+660.800 | LT | 77.8 |
| STA | 15+723.100 | RT | 128.4 |
| STA | 15+931.300 | RT | 96.8 |
| STA | 15+960.300 | LT | 99.4 |
| SUBTOTAL | | | 402.4 |
| TOTALS | | | 5,863.6 |
| TOTALS ROUNDED FOR SUMMARY SHEET | | | 5,864 |

PLOT DATE = 12/22/2008
FILE NAME = H:\4428\Contract 94533\SHT_B14_sh_all_schedules_V8.dgn

H. M. & G. NO. 4420

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|---|
| NAME | DATE | |
| | | <p align="center">SCHEDULE OF QUANTITIES</p> <p>SCALE NO SCALE DRAWN BY KOJ</p> <p>DATE DECEMBER 22, 2008 CHECKED BY LWJ</p> |
| | | |
| | | |
| | | |
| | | |
| | | |

PIPE CULVERT REMOVAL/REMOVE EXISTING CULVERTS (CONTINUED)

| | | | | |
|---------------------|----------|------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | LYRS | CRAWFORD | 212 | 14 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

| LOCATION | LT/RT | EXISTING MATERIAL | FOR INFORMATION ONLY SIZE (mm) | EXISTING END TREATMENT | | REMOVE EXISTING CULVERTS | PIPE CULVERT REMOVAL |
|---------------|-------|-------------------|-----------------------------------|------------------------|------------|--------------------------|----------------------|
| | | | | UPSTREAM | DOWNSTREAM | M5010521 METER | M5010522 METER |
| RURAL | | | | | | | |
| STA 5+141.000 | LT | CMP | 380 | | | | 16.6 |
| STA 5+193.700 | LT | CMP | 380 | | | | 9.6 |
| STA 5+194.097 | RT | CMP | 380 | | | | 14.9 |
| STA 5+295.000 | AR | RCP | 760 | CONC HDWL | CONC HDWL | | 13.4 |
| STA 5+345.500 | RT | CMP | 380 | | | | 9.9 |
| STA 5+377.500 | LT | VCP | 380 | CONC HDWL | CONC HDWL | | 5.8 |
| STA 5+618.000 | RT | CMP | 380 | | | | 11.3 |
| STA 5+621.500 | LT | CMP | 460 | | | | 15.6 |
| STA 5+651.500 | RT | CMP | 380 | | | | 9.3 |
| STA 5+771.700 | LT | VCP | 460 | | | | 5.8 |
| STA 5+938.000 | RT | CMP | 380 | | | | 9.3 |
| STA 6+004.400 | RT | VCP | 380 | | | | 5.8 |
| STA 6+034.800 | RT | VCP | 380 | | | | 5.8 |
| STA 6+084.500 | RT | VCP | 380 | | | | 12.6 |
| STA 6+140.500 | RT | CMP | 380 | | | | 7.7 |
| STA 6+180.600 | LT | CMP | 380 | CONC HDWL | CONC HDWL | | 23.0 |
| STA 6+208.000 | LT | CMP | 380 | | | | 13.0 |
| STA 6+218.600 | RT | CMP | 380 | | | | 11.2 |
| STA 6+336.600 | RT | CMP | 380 | | | | 12.6 |
| STA 6+444.900 | LT | CONC BOX | 910X450 | CONC HDWL | CONC HDWL | 7.7 | |
| STA 6+446.400 | RT | CMP | 380 | | | | 7.6 |
| STA 6+461.200 | RT | RCP | 300 | | | | 15.2 |
| STA 6+470.500 | LT | STEEL | 230 | | | | 6.5 |
| STA 6+483.500 | RT | RCP | 300 | | | | 15.6 |
| STA 6+500.000 | LT | STEEL | 250 | | | | 11.0 |
| STA 6+524.000 | LT | CMP | 320 | | | | 8.3 |
| STA 6+561.800 | LT | CMP | 380 | | | | 8.5 |
| STA 6+572.500 | RT | CMP | 300 | | | | 9.0 |
| STA 6+592.500 | LT | CMP | 380 | | | | 7.7 |
| STA 6+632.800 | RT | CMP | 300 | | | | 10.2 |
| STA 6+636.700 | LT | CMP | 380 | | | | 8.3 |
| STA 6+652.100 | LT | VCP | 380 | CONC HDWL | CONC HDWL | | 6.0 |
| STA 6+746.500 | RT | CMP | 300 | | | | 12.5 |
| STA 6+784.500 | RT | CMP | 300 | | | | 6.0 |
| STA 6+819.200 | RT | CMP | 380 | | | | 13.0 |
| STA 6+905.000 | RT | CMP | 460 | | | | 9.0 |
| STA 6+914.500 | LT | VCP | 300 | | | | 6.0 |
| STA 7+015.000 | LT | CMP | 380 | | | | 7.7 |
| STA 7+046.600 | RT | CMP | 380 | | | | 9.5 |
| STA 7+177.000 | RT | CMP | 380 | | | | 13.7 |
| STA 7+281.500 | RT | CMP | 380 | | | | 11.5 |
| STA 7+330.600 | LT | CMP | 300 | | | | 15.7 |
| STA 7+330.600 | LT | CMP | 450 | | | | 7.0 |
| STA 7+346.500 | LT | CORR PVC | 380 | | | | 17.8 |
| STA 7+362.300 | LT | CORR PVC | 380 | | | | 10.0 |
| STA 7+390.500 | RT | VCP | 380 | | | | 5.4 |
| STA 7+392.900 | LT | RCP | 620 | | | | 5.5 |
| STA 7+435.500 | LT | CONC BOX | 480X250 | | | 1.4 | |
| STA 7+440.000 | RT | CMP | 460 | | | | 6.0 |
| STA 7+446.500 | LT | CONC BOX | 560 | CONC HDWL | CONC HDWL | 10.0 | |
| STA 7+471.600 | LT | CMP | 710 | | | | 8.8 |
| STA 7+478.800 | RT | VCP | 450 | | | | 5.5 |
| STA 7+509.000 | LT | CMP | 860 | | | | 9.6 |
| STA 7+509.000 | RT | VCP | 450 | | | | 7.7 |
| STA 7+681.000 | RT | PLASTIC | 100 | | | | 23.0 |
| STA 7+681.000 | | PLASTIC | 350 | | | | 25.5 |
| STA 7+686.320 | RT | CONC BOX | 450X450 | CONC HDWL | CONC HDWL | 9.7 | |
| STA 7+687.880 | LT | CONC BOX | 450X450 | CONC HDWL | CONC HDWL | 9.0 | |
| STA 7+695.500 | RT | VCP | 380 | | | | 5.5 |
| STA 7+714.000 | LT | VCP | 380 | CONC HDWL | CONC HDWL | | 4.0 |
| STA 7+755.000 | LT | VCP | 380 | CONC HDWL | CONC HDWL | | 6.3 |
| STA 7+902.000 | RT | CMP | 420 | | | | 18.1 |
| STA 7+999.500 | LT | CMP | 380 | | | | 9.8 |
| STA 8+063.000 | RT | CMP | 460 | | | | 9.8 |
| STA 8+078.300 | | CONC BOX | 960X3200 | CONC HDWL | CONC HDWL | 9.1 | |
| STA 8+161.200 | RT | VCP | 480 | | | | 5.5 |
| STA 8+296.000 | LT | CMP | 510 | | | | 11.1 |
| STA 8+311.000 | RT | CMP | 460 | CONC HDWL | | | 11.0 |
| STA 8+456.000 | LT | VCP | 380 | | | | 5.6 |
| STA 8+456.000 | RT | VCP | 380 | CONC HDWL | CONC HDWL | | 5.8 |
| STA 8+562.500 | LT | RCP | 300 | | | | 6.5 |
| STA 8+606.000 | LT | RCP | 380 | | | | 10.0 |
| STA 8+607.000 | RT | CMP | 380 | | | | 9.1 |
| STA 8+621.500 | LT | RCP | 380 | | | | 7.6 |
| STA 8+634.000 | LT | RCP | 360 | | | | 7.6 |
| STA 8+651.000 | RT | RCP | 380 | | | | 7.8 |
| STA 8+669.000 | RT | RCP | 300 | | | | 6.8 |
| STA 8+724.500 | RT | CMP | 380 | | | | 9.4 |
| STA 8+739.700 | LT | RCP | 380 | | | | 9.5 |
| STA 8+749.000 | RT | CMP | 380 | | | | 9.2 |
| STA 8+770.500 | LT | RCP | 380 | | | | 6.7 |
| STA 8+778.500 | RT | CMP | 380 | | | | 9.5 |

| LOCATION | LT/RT | EXISTING MATERIAL | FOR INFORMATION ONLY SIZE (mm) | EXISTING END TREATMENT | | REMOVE EXISTING CULVERTS | PIPE CULVERT REMOVAL |
|----------------|-------|-------------------|-----------------------------------|------------------------|---------------|--------------------------|----------------------|
| | | | | UPSTREAM | DOWNSTREAM | M5010521 METER | M5010522 METER |
| STA 8+792.000 | LT | RCP | 380 | | | | 8.0 |
| STA 8+793.300 | RT | STEEL | 300 | | | | 8.6 |
| STA 8+841.000 | RT | STEEL | 280 | | | | 15.1 |
| STA 8+843.000 | LT | RCP | 380 | CONC HDWL | CONC HDWL | | 13.0 |
| STA 8+884.200 | LT | CMP | 380 | | | | 9.7 |
| STA 8+949.000 | RT | CMP | 380 | | | | 11.3 |
| STA 9+073.000 | LT | CMP | 380 | | | | 9.6 |
| STA 9+192.500 | RT | CMP | 460 | | | | 10.0 |
| STA 9+682.500 | RT | CMP | 460 | | | | 11.2 |
| STA 9+721.000 | RT | CMP | 460 | RAILROAD TIES | | | 15.0 |
| STA 9+898.000 | LT | CMP | 380 | | | | 5.7 |
| STA 10+321.500 | RT | CMP | 560 | | | | 10.0 |
| STA 10+329.000 | LT | CMP | 610 | | | | 9.3 |
| STA 10+499.500 | RT | CMP | 380 | | | | 9.6 |
| STA 10+500.000 | LT | CMP | 380 | | | | 21.8 |
| STA 10+559.500 | RT | CMP | 380 | | | | 6.0 |
| STA 10+705.000 | LT | CMP | 380 | | | | 9.4 |
| STA 10+732.000 | LT | RCP | 380 | | | | 7.3 |
| STA 10+736.079 | RT | RCP | 440 | | | | 15.8 |
| STA 10+740.000 | LT | RCP | 380 | | | | 7.4 |
| STA 10+757.500 | RT | RCP | 380 | | | | 8.1 |
| STA 10+765.500 | LT | RCP | 300 | | | | 8.0 |
| STA 10+786.500 | LT | RCP | 300 | | | | 5.5 |
| STA 10+793.600 | LT | RCP | 300 | | | | 6.1 |
| STA 10+824.000 | LT | CMP | 380 | | | | 9.3 |
| STA 10+864.000 | RT | RCP | 300 | | | | 7.1 |
| STA 10+881.700 | RT | RCP | 380 | | | | 7.5 |
| STA 10+915.500 | LT | CMP | 380 | | | | 9.6 |
| STA 10+920.500 | RT | CMP | 380 | | | | 11.0 |
| STA 11+083.000 | RT | CMP | 340 | | | | 11.5 |
| STA 11+146.500 | LT | VCP | 380 | | | | 5.6 |
| STA 11+246.000 | RT | VCP | 380 | | | | 5.8 |
| STA 11+307.000 | LT | CMP | 380 | | | | 8.8 |
| STA 11+571.500 | RT | CMP | 380 | | | | 23.4 |
| STA 11+698.000 | RT | CMP | 380 | RAILROAD TIES | RAILROAD TIES | | 18.9 |
| STA 11+756.500 | LT | CMP | 380 | | | | 8.0 |
| STA 11+880.000 | RT | CMP | 380 | | | | 13.8 |
| STA 11+914.500 | RT | CMP | 380 | | | | 11.3 |
| STA 12+138.500 | AR | RCP | 740 | CONC HDWL | CONC HDWL | | 15.6 |
| STA 12+353.000 | LT | CMP | 460 | | | | 10.0 |
| STA 12+353.000 | RT | CMP | 460 | | | | 11.2 |
| STA 12+981.000 | LT | RCP | 380 | | | | 8.3 |
| STA 12+991.000 | LT | CMP | 300 | | | | 6.5 |
| STA 13+058.500 | LT | CMP | 380 | | | | 9.2 |
| STA 13+084.000 | RT | VCP | 380 | CONC HDWL | | | 8.6 |
| STA 13+097.000 | LT | RCP | 300 | | | | 16.0 |
| STA 13+127.500 | LT | CMP | 380 | | | | 8.4 |
| STA 13+140.500 | LT | CMP | 380 | | | | 7.4 |
| STA 13+163.500 | LT | CMP | 380 | | | | 12.0 |
| STA 13+205.500 | LT | CMP | 380 | | | | 11.0 |
| STA 13+245.600 | LT | STEEL | 200 | | | | 13.3 |
| STA 13+276.400 | LT | STEEL | 200 | | | | 12.0 |
| STA 13+293.500 | LT | CMP | 380 | | | | 7.5 |
| STA 13+328.500 | LT | RCP | 430 | | | | 8.5 |
| STA 13+383.600 | | RCP/CONC BOX/RCP | 600/600X450/600 | | | | 13.6 |
| STA 13+443.600 | LT | STEEL | 330 | | | | 14.6 |
| STA 13+480.000 | LT | STEEL | 330 | | | | 11.0 |
| STA 13+491.500 | RT | CMP | 300 | | | | 9.2 |
| STA 13+527.500 | RT | CMP | 300 | | | | 9.8 |
| STA 13+528.500 | LT | RCP | 380 | | | | 7.6 |
| STA 13+603.000 | RT | CMP | 300 | | | | 6.4 |
| STA 13+615.000 | LT | RCP | 380 | | | | 8.2 |
| STA 13+615.000 | RT | CMP | 300 | | | | 7.0 |
| STA 13+661.000 | | CONC BOX | 1070X2130 | CONC HDWL | CONC HDWL | 9.1 | |
| STA 13+672.000 | RT | RCP | 460 | | | | 10.1 |
| STA 13+748.000 | LT | RCP | 380 | | | | 7.7 |
| STA 13+750.400 | RT | RCP | 460 | | | | 7.2 |
| STA 13+802.800 | RT | VCP/RCP | 460 | | | | 7.5 |
| STA 13+803.800 | LT | CMP/VCP | 460 | CONC HDWL | CONC HDWL | | 9.8 |
| STA 13+981.300 | LT | VCP | 380 | CONC HDWL | CONC HDWL | | 5.5 |
| STA 14+217.800 | RT | VCP | 380 | CONC HDWL | CONC HDWL | | 10.2 |
| STA 14+221.800 | LT | RCP | 380 | | | | 9.0 |

CONTINUED NEXT SHEET

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|-------------------|---------------------------------------|
| NAME | DATE | |
| | | SCHEDULE OF QUANTITIES |
| | | |
| | | |
| | | |
| | | |
| | | |
| SCALE | NO SCALE | DRAWN BY KOJ |
| DATE | DECEMBER 22, 2008 | CHECKED BY LWJ |

PLT DATE = 12/22/2008
FILE NAME = H:\V420\Contract 94533\SH1_014_sch.plt scheduled_10.dgn

PIPE CULVERT REMOVAL/REMOVE EXISTING CULVERTS (CONTINUED)

| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 15 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

| LOCATION | LT/RT | EXISTING MATERIAL | FOR INFORMATION ONLY | EXISTING END TREATMENT | | REMOVE EXISTING CULVERTS M5010521 METER | PIPE CULVERT REMOVAL M5010522 METER |
|----------------------------------|-------|-------------------|----------------------|------------------------|------------|---|-------------------------------------|
| | | | | UPSTREAM | DOWNSTREAM | | |
| STA 14+234.800 | RT | CMP | 380 | | | | 7.2 |
| STA 14+242.400 | LT | RCP | 380 | CONC HDWL | CONC HDWL | | 15.0 |
| STA 14+265.600 | LT | RCP | 380 | | | | 14.5 |
| STA 14+308.900 | LT | RCP | 380 | | | | 14.8 |
| STA 14+447.300 | LT | CMP | 460 | | | | 12.0 |
| STA 14+447.300 | RT | CMP | 375 | | | | 12.0 |
| STA 14+461.300 | LT | CMP | 740 | | | | 7.5 |
| STA 14+542.500 | LT | CMP | 740 | | | | 7.8 |
| STA 14+553.800 | LT | CMP | 740 | | | | 7.3 |
| STA 14+600.800 | LT | CMP | 910 | STEEL HDWL | STEEL HDWL | | 6.5 |
| STA 14+628.800 | RT | RCP | 380 | | CONC HDWL | | 9.0 |
| STA 14+630.300 | LT | CMP | 910 | STEEL HDWL | CONC HDWL | | 7.5 |
| STA 14+659.300 | LT | CMP/CONC BOX | 790/640 | RAILROAD TIES | CONC HDWL | 9.6 | |
| STA 14+664.800 | RT | RCP | 380 | | CONC HDWL | | 10.0 |
| STA 14+696.800 | RT | RCP | 380 | CONC HDWL | CONC HDWL | | 8.0 |
| STA 14+711.600 | LT | CMP | 910 | | | | 12.3 |
| STA 14+725.800 | RT | RCP | 380 | | | | 6.0 |
| STA 14+756.000 | LT | ELLIP CMP | 860/1270 | | | | 9.0 |
| STA 17+763.300 | RT | CMP | 380 | | | | 22.5 |
| STA 14+770.800 | LT | ELLIP CMP | 860X1270 | | | | 9.0 |
| STA 14+784.000 | RT | CMP | 430 | | | | 20.3 |
| STA 14+861.000 | LT | CMP | 380 | | | | 11.5 |
| STA 14+861.800 | RT | CMP | 380 | | | | 9.4 |
| STA 14+886.300 | RT | CMP | 380 | | | | 11.5 |
| STA 14+926.300 | LT | RCP | 380 | | | | 12.2 |
| STA 14+926.300 | RT | VCP | 360 | CONC HDWL | CONC HDWL | | 5.6 |
| STA 14+981.300 | LT | RCP | 380 | | | | 11.3 |
| STA 15+002.000 | LT | RCP | 380 | CONC HDWL | CONC HDWL | | 5.2 |
| STA 15+172.500 | AR | CONC BOX | 460X460 | CONC HDWL | CONC HDWL | 9.0 | |
| STA 15+232.300 | LT | RCP | 460 | | | | 16.4 |
| STA 15+320.300 | LT | CMP | 430 | | | | 23.2 |
| STA 15+353.800 | LT | CMP | 380 | | | | 9.1 |
| STA 15+450.800 | RT | VCP | 380 | | CONC HDWL | | 10.0 |
| STA 15+454.800 | LT | CMP | 460 | | | | 13.6 |
| STA 15+511.800 | RT | VCP/CMP | 380 | | | | 9.4 |
| STA 15+544.900 | RT | VCP | 380 | CONC HDWL | | | 5.6 |
| SUBTOTAL | | | | | | 74.6 | 1840.9 |
| URBAN | | | | | | | |
| STA 15+595.800 | LT | CMP | 380/300 | | | | 13.1 |
| STA 15+660.800 | LT | CMP | 380 | | | | 9.2 |
| STA 15+661.300 | RT | CMP | 330 | | | | 10.6 |
| STA 15+723.100 | RT | CMP | 380 | | | | 13.7 |
| STA 15+870.300 | RT | VCP | 430 | CONC HDWL | CONC HDWL | | 5.6 |
| STA 15+871.400 | LT | RCP | 410 | | CONC HDWL | | 13.8 |
| STA 15+931.300 | RT | RCP | 410 | | | | 21.1 |
| STA 15+933.820 | | CONC BOX | 900X450 | CONC HDWL | CONC HDWL | 9.1 | 9.3 |
| STA 15+960.300 | LT | RCP | 410 | | | | 28.6 |
| SUBTOTAL | | | | | | 9.1 | 125.0 |
| TOTALS | | | | | | 83.7 | 1965.90 |
| TOTALS ROUNDED FOR SUMMARY SHEET | | | | | | 84 | 1,966 |

PLOT DATE = 12/22/2008
 FILE NAME = H:\V420\Contract 94533\SHT_014_sch.e11_schedules_VB.dgn

H. M. & G. NO. 4420

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|---|
| NAME | DATE | |
| | | SCHEDULE OF QUANTITIES SCALE NO SCALE DRAWN BY KOJ DATE DECEMBER 22, 2008 CHECKED BY LWJ |
| | | |
| | | |
| | | |
| | | |
| | | |

| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 16 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

EARTHWORK SCHEDULE

| LOCATION | | EARTH EXCAVATION | EARTH EXCAVATION (WIDENING) | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | EMBANKMENT REQUIRED | EMBANKMENT REQUIRED FOR ENTRANCES (SEE NOTE) | EARTH EXCAVATION AVAILABLE (ADJUSTED FOR SHRINKAGE - 25%) | EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) | TOPSOIL EXCAVATION AND PLACEMENT | |
|----------------------------------|--|------------------|-----------------------------|---|---------------------|--|---|---|----------------------------------|---------|
| STATION | | RT/LT | M2020010 CU. M. | M2020050 CU. M. | M2021200 CU. M. | CU. M. | CU. M. | CU. M. | M2112500 CU. M. | |
| RURAL | | | | | | | | | | |
| STA. 5+025 TO STA. 9+385 | | RT/LT | 19226.4 | 1177.2 | | 7737.8 | 6106.9 | 15302.7 | 1458.0 | 7666.1 |
| STA. 5+194.097 (TR300) | | RT. | 127.8 | | | 44.0 | | 95.9 | 51.9 | 19.0 |
| STA. 6+444.9 (TR375) | | LT. | 68.8 | | | 72.5 | | 51.6 | -20.9 | 34.3 |
| STA. 6+446 (TR375) | | RT. | 57.5 | | | 45.8 | | 43.1 | -2.7 | 37.5 |
| STA. 7+687.148 (TR 450) | | LT. | 62.8 | | | 18.7 | | 47.1 | 28.4 | 28.8 |
| STA. 7+687.148 (TR 450) | | RT. | 56.0 | | | 21.6 | | 42.0 | 20.4 | 33.9 |
| STA. 8+910.897 (TR 525) | | LT. | 65.6 | | | 12.0 | | 49.2 | 37.2 | 14.3 |
| STA. 9+580 TO STA. 15+545 | | RT/LT | 37501.7 | 1610.6 | 675.1 | 12074.9 | 13226.3 | 29334.2 | 4032.9 | 11454.2 |
| STA. 10+125.278 (TR 600) | | LT. | 40.8 | | | 7.0 | | 30.6 | 23.6 | 24.8 |
| STA. 10+125.278 (TR 600) | | RT. | 69.9 | | | 60.8 | | 52.4 | -8.4 | 25.3 |
| STA. 10+736.079 (FIREBAUGH) | | RT. | 53.7 | | | 16.8 | | 40.3 | 23.5 | 21.1 |
| STA. 12+164.026 (TR725) | | LT. | 103.9 | | | 31.0 | | 77.9 | 46.9 | 15.6 |
| STA. 12+164.026 (TR725) | | RT. | 123.8 | | | 14.0 | | 92.9 | 78.9 | 8.0 |
| STA. 13+391.693 (TR800) | | LT. | 66.4 | | | 17.0 | | 49.8 | 32.8 | 17.7 |
| STA. 13+391.693 (TR800) | | RT. | 155.2 | | | 3.0 | | 116.4 | 113.4 | 30.8 |
| STA. 15+062.4 (TR900) | | LT. | 103.1 | | | 23.0 | | 77.3 | 54.3 | 19.3 |
| STA. 15+062.4 (TR900) | | RT. | 63.6 | | | 3.0 | | 47.7 | 44.7 | 25.0 |
| SUBTOT | | | 57947.0 | 2787.8 | 675.1 | 20202.9 | 19333.2 | 45551.0 | 6014.9 | 19475.6 |
| URBAN | | | | | | | | | | |
| STA. 15+545 TO 16+016 | | RT/LT | 2306.6 | 127.2 | | 356.5 | 675.9 | 1825.4 | 792.9 | 720.9 |
| STA. 15+661.3 (PASADENA) | | RT. | 50.3 | | | 10.0 | | 37.7 | 27.7 | 11.3 |
| STA. 15+773.3 (GLENDALE) | | RT. | 56.7 | | | 3.0 | | 42.5 | 39.5 | 9.5 |
| SUBTOT | | | 2413.6 | 127.2 | 0.0 | 369.5 | 675.9 | 1905.6 | 860.2 | 741.7 |
| TOTALS | | | 60360.6 | 2914.9 | 675.1 | 20572.4 | 20009.2 | 47456.6 | 6875.0 | 20217.3 |
| TOTALS ROUNDED FOR SUMMARY SHEET | | | 60365 | 2915 | 680 | | | | | 20220 |

NOTE: QUANTITIES ASSUME ALL EXISTING ENTRANCES (NOT INCLUDING PUBLIC ROADS) WILL BE REMOVED DURING DITCH EXCAVATION.

PLOT DATE = 12/22/2008
 FILE NAME = H:\428\Contract\94533\SHT_014_sch_all_schedules_18.dgn

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|---|
| NAME | DATE | |
| | | <p align="center">SCHEDULE OF QUANTITIES</p> <p>SCALE NO SCALE DRAWN BY KOJ</p> <p>DATE DECEMBER 22, 2008 CHECKED BY LWJ</p> |
| | | |
| | | |
| | | |
| | | |
| | | |

H. M. & G. NO. 4420

| | | | | |
|---------------------|----------|------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 17 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

REMOVAL OF EXISTING STRUCTURES

| LOCATION | | DESCRIPTION | REMOVAL OF EXISTING STRUCTURES |
|--------------|-------|--|--------------------------------|
| STATION | RT/LT | | 50100X00 |
| RURAL | | | |
| STA 5+750 | LT | 1.8m X 7.3m STEEL PIPE BRIDGE AND FOUNDATION. THE BRIDGE CONSISTS OF 27 - 64mm DIAMETER STEEL PIPE | NO. 1 |
| STA 7+446 | LT | CONCRETE FOUNDATION TO BE REMOVED TO THE PROPOSED RIGHT-OF-WAY LINE. APPROX. 3.8m(L) X 3.4m(W) | NO. 2 |
| STA 13+661 | LT | CONCRETE WEIR; APPROX. 5.1 SQ. M | NO. 3 |
| URBAN | | | |
| STA 15+654 | RT | MASONRY COLUMN; APPROX. 600mm(L) X 600mm(W) X1200mm(H) | NO. 4 |
| STA 15+666 | RT | MASONRY COLUMN; APPROX. 600mm(L) X 600mm(W) X1200mm(H) | NO. 5 |

REMOVING MANHOLES/INLETS

| LOCATION | | | REMOVING INLETS |
|--------------|--------|-------|-----------------|
| STATION | DIST | RT/LT | 60500060 |
| RURAL | | | |
| STA 14+773.1 | 12.0 m | RT | 1 |
| TOTAL | | | 1 |

BUILDING REMOVAL

| LOCATION | | BUILDING REMOVAL NO. 1 |
|--------------|-------|------------------------|
| STATION | RT/LT | L. SUM |
| STA. 13+290 | LT | 1 |
| TOTAL | | 1 |

PLOT DATE = 12/22/2008
 FILE NAME = H:\428\Contract\94533\SHT_B\M_sch_all_schedules_V8.dgn

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|--|
| NAME | DATE | |
| | | <p align="center">SCHEDULE OF QUANTITIES</p> <p>SCALE _____ DRAWN BY KOJ</p> <p>DATE DECEMBER 22, 2008 CHECKED BY LWJ</p> |
| | | |
| | | |
| | | |
| | | |
| | | |

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------|------------------|-----------|
| 781 | LYRS | CRAWFORD | 212 | 18 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

TEMPORARY CONSTRUCTION ITEMS

| LOCATION | | | | TEMPORARY PAVEMENT | TEMPORARY PAVEMENT REMOVAL | PIPE CULVERTS, CLASS C, TYPE 1 (TEMPORARY) (CMP) | | | EARTH EXCAVATION (TEMPORARY PAVEMENT AND DITCH) | EARTH EXCAVATION AVAILABLE (ADJUSTED FOR SHRINKAGE - 25%) | EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) | TEMPORARY EMBANKMENT PLACEMENT AND REMOVAL |
|---|----|------------|-------|--------------------|----------------------------|--|-------|---------|---|---|---|--|
| STATION | TO | STATION | LT/RT | | | 375MM | 450MM | 600MM | | | | |
| RURAL | | | | | | | | | | | | |
| 13+013.500 | TO | 13+862.000 | RT | 3,451 | 3,451 | | | 913.049 | 684.787 | -168.199 | 852.986 | |
| ENTRANCES | | | | | | | | | | | | |
| 13+058.500 | | | LT | | | 6.10 | | | | | | |
| 13+097.000 | | | LT | | | 6.10 | | | | | | |
| 13+127.500 | | | LT | | | 12.20 | | | | | | |
| 13+140.500 | | | LT | | | 9.15 | | | | | | |
| 13+163.500 | | | LT | | | 6.10 | | | | | | |
| 13+205.500 | | | LT | | | 6.10 | | | | | | |
| 13+246.000 | | | LT | | | 6.10 | | | | | | |
| 13+328.500 | | | LT | | | 6.10 | | | | | | |
| 13+480.000 | | | LT | | | | 6.10 | | | | | |
| 13+803.800 | | | LT | | | | | 6.10 | | | | |
| TOTAL | | | | 3,451 | 3,451 | 57.95 | 6.10 | 6.10 | 913.049 | 684.787 | -168.199 | 852.986 |
| TOTALS ROUNDED FOR SUMMARY SHEET | | | | 3,451 | 3,451 | 58.0 | 6.5 | 6.5 | | | | 855 |

NOTE: ADDITIONAL EMBANKMENT NEEDED SHALL BE OBTAINED FROM EXCESS AT OTHER LOCATIONS OR A BORROW SITE APPROVED BY THE ENGINEER AT NO ADDITIONAL COST

EROSION CONTROL BLANKET

| LOCATION | AREA SQ METER |
|---------------------|---------------|
| 5+725 TO 5+745 LT | 40 |
| 5+725 TO 5+745 RT | 40 |
| 6+925 TO 6+945 RT | 40 |
| 6+960 TO 6+980 LT | 40 |
| 6+960 TO 6+980 RT | 40 |
| 10+065 TO 10+100 RT | 70 |
| 10+200 TO 10+225 RT | 50 |
| 10+225 TO 10+275 RT | 100 |
| 11+400 TO 11+447 RT | 94 |
| 11+450 TO 11+470 LT | 40 |
| 11+710 TO 11+775 RT | 130 |
| 11+720 TO 11+741 LT | 42 |
| 11+772 TO 11+777 LT | 10 |
| 11+800 TO 11+825 LT | 50 |
| 12+738 TO 12+870 LT | 264 |
| 12+738 TO 12+875 RT | 274 |
| 12+875 TO 12+887 RT | 24 |
| 12+900 TO 12+925 LT | 50 |
| 13+625 TO 13+655 RT | 60 |
| TOTAL | 1,458 |

SEEDING/SODDING/MULCH

| LOCATION | | | | SEEDING CLASS 2 | FERTILIZER NUTRIENTS | | | MULCH, METHOD 2 |
|---|----|------------|-------|-----------------|----------------------|-----------|------------|-----------------|
| STATION | TO | STATION | LT/RT | | NITROGEN | POTASSIUM | PHOSPHORUS | |
| | | | | M2500200 | M2500400 | M2500500 | M2500600 | M2510115 |
| | | | | HA | KG | KG | KG | HA |
| RURAL | | | | | | | | |
| STA 5+009 | TO | STA 9+385 | LT | 3.23 | 322.96 | 322.96 | 322.96 | 3.19 |
| STA 5+025 | TO | STA 9+385 | RT | 2.84 | 284.38 | 284.38 | 284.38 | 2.80 |
| STA 9+580 | TO | STA 15+545 | LT | 4.52 | 452.00 | 452.00 | 452.00 | 4.46 |
| STA 9+580 | TO | STA 15+545 | RT | 4.78 | 477.89 | 477.89 | 477.89 | 4.61 |
| SUBTOTAL | | | | 15.37 | 1537.23 | 1537.23 | 1537.23 | 15.06 |
| URBAN | | | | | | | | |
| STA 15+545 | TO | STA 16+016 | LT | 0.29 | 29.48 | 29.48 | 29.48 | 0.29 |
| STA 15+545 | TO | STA 16+016 | RT | 0.28 | 27.72 | 27.72 | 27.72 | 0.28 |
| SUBTOTAL | | | | 0.57 | 57.20 | 57.20 | 57.20 | 0.57 |
| TOTALS | | | | 15.94 | 1,594.43 | 1,594.43 | 1,594.43 | 15.64 |
| TOTALS ROUNDED FOR SUMMARY SHEET | | | | 16.0 | 1,600 | 1,600 | 1,600 | 15.7 |

PERIMETER EROSION BARRIER

| LOCATION | LENGTH METER |
|---------------------|--------------|
| 6+875 TO 6+940 LT | 65 |
| 7+200 TO 7+250 LT | 50 |
| 9+125 TO 9+385 LT | 260 |
| 9+200 TO 9+385 RT | 185 |
| 9+580 TO 9+725 LT | 145 |
| 9+580 TO 9+675 RT | 95 |
| 10+050 TO 10+100 LT | 50 |
| 10+150 TO 10+200 RT | 50 |
| 10+150 TO 10+250 LT | 100 |
| 11+450 TO 11+550 RT | 100 |
| 11+475 TO 11+500 LT | 25 |
| 12+575 TO 12+625 RT | 50 |
| TOTAL | 1,175 |

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|-------------------|---------------------------------------|
| NAME | DATE | |
| | | SCHEDULE OF QUANTITIES |
| | | |
| | | |
| | | |
| | | |
| | | |
| SCALE | NO SCALE | DRAWN BY KOJ |
| DATE | DECEMBER 22, 2008 | CHECKED BY LWJ |

PLOT DATE = 12/22/2008
 FILE NAME = H:\V\20\Contract 94533\SH1_014_sch.e11_sch.tbl.es18.dgn

H. M. & G. NO. 4420

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 19 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

INLET AND PIPE PROTECTION

| STATION | SIDE | QUANTITY EACH | STATION | SIDE | QUANTITY EACH |
|-------------|-------|---------------|-------------|-------|---------------|
| 5+096 | RT | 1 | 10+747 | LT | 1 |
| 5+153 | LT | 1 | 10+749 | RT | 1 |
| 5+203 | LT&RT | 2 | 10+773 | LT | 1 |
| 5+354 | LT | 1 | 10+801 | LT | 1 |
| 5+355 | RT | 1 | 10+833 | LT | 1 |
| 5+457 | LT | 1 | 10+889 | RT | 1 |
| 5+606 | RT | 1 | 10+922 | LT | 1 |
| 5+612 | LT | 1 | 10+927 | RT | 1 |
| 5+639 | RT | 1 | 11+137 | LT | 1 |
| 5+712 | LT | 1 | 11+256 | RT | 1 |
| 5+778 | LT | 1 | 11+315 | LT | 1 |
| 5+945 | RT | 1 | 11+584 | RT | 1 |
| 6+092 | RT | 1 | 11+741 | LT | 1 |
| 6+148 | RT | 1 | 11+894 | RT | 1 |
| 6+187 | LT | 1 | 11+926 | RT | 1 |
| 6+189 | RT | 1 | 12+345 | RT | 1 |
| 6+309 | RT | 1 | 12+346 | LT | 1 |
| 6+438 | RT | 1 | 13+000 | LT | 1 |
| 6+450 | LT | 1 | 13+068 | LT | 1 |
| 6+455 | RT | 1 | 13+095 | RT | 1 |
| 6+494 | RT | 1 | 13+105 | LT | 1 |
| 6+511 | LT | 1 | 13+147 | LT | 1 |
| 6+553 | LT | 1 | 13+172 | LT | 1 |
| 6+562 | RT | 1 | 13+214 | LT | 1 |
| 6+583 | LT | 1 | 13+255 | LT | 1 |
| 6+624 | RT | 1 | 13+339 | LT | 1 |
| 6+628 | LT | 1 | 13+472 | LT | 1 |
| 6+644 | LT | 1 | 13+482 | RT | 1 |
| 6+738 | RT | 1 | 13+518 | RT | 1 |
| 6+776 | RT | 1 | 13+595 | RT | 1 |
| 6+810 | RT | 1 | 13+680 | RT | 1 |
| 6+896 | RT | 1 | 13+762 | RT | 1 |
| 7+080 | RT | 1 | 13+812 | LT | 1 |
| 7+026 | LT | 1 | 13+813 | RT | 1 |
| 7+054 | RT | 1 | 13+992 | LT | 1 |
| 7+166 | RT | 1 | 14+088 | RT | 1 |
| 7+301 | LT | 1 | 14+224 | RT | 1 |
| 7+339 | LT | 1 | 14+230 | LT | 1 |
| 7+403 | LT | 1 | 14+241 | RT | 1 |
| 7+478 | LT | 1 | 14+252 | LT | 1 |
| 7+486 | RT | 1 | 14+434 | RT | 1 |
| 7+515 | LT | 1 | 14+439 | LT | 1 |
| 7+680 | RT | 1 | 14+544 | LT | 1 |
| 7+692 | RT | 1 | 14+592 | LT | 1 |
| 7+697 | LT&RT | 2 | 14+618 | RT | 1 |
| 7+762 | LT | 1 | 14+621 | LT | 1 |
| 8+050 | RT | 1 | 14+653 | LT | 1 |
| 8+170 | RT | 1 | 14+654 | RT | 1 |
| 8+303 | LT | 1 | 14+687 | RT | 1 |
| 8+320 | RT | 1 | 14+703 | LT | 1 |
| 8+463 | LT&RT | 2 | 14+718 | RT | 1 |
| 8+570 | LT | 1 | 14+751 | LT | 1 |
| 8+615 | RT | 1 | 14+754 | RT | 1 |
| 8+623 | LT | 1 | 14+797 | LT | 1 |
| 8+643 | LT | 1 | 14+873 | RT | 1 |
| 8+660 | RT | 1 | 14+896 | RT | 1 |
| 8+678 | RT | 1 | 14+935 | LT&RT | 2 |
| 8+717 | RT | 1 | 14+990 | LT | 1 |
| 8+731 | LT | 1 | 15+243 | LT | 1 |
| 8+741 | RT | 1 | 15+332 | LT | 1 |
| 8+761 | LT | 1 | 15+361 | LT | 1 |
| 8+770 | RT | 1 | 15+376 | LT&RT | 2 |
| 8+783 | LT | 1 | 15+460 | RT | 1 |
| 8+854 | RT | 1 | 15+465 | LT | 1 |
| 8+855 | LT | 1 | 15+519 | RT | 1 |
| 8+881 | RT | 1 | 15+520 | LT | 1 |
| 8+939 | RT | 1 | 15+535 | LT&RT | 2 |
| 9+065 | LT | 1 | 15+606 | LT | 1 |
| 9+181 | RT | 1 | 15+671 | LT&RT | 2 |
| 9+691 | RT | 1 | 15+733 | RT | 1 |
| 9+717 | RT | 1 | 15+863 | RT | 1 |
| 10+329 | RT | 1 | 15+864 | LT | 1 |
| 10+340 | LT | 1 | 15+884 | LT | 1 |
| 10+511 | LT | 1 | 15+924 | RT | 1 |
| 10+570 | RT | 1 | 15+951 | LT | 1 |
| 10+714 | RT | 1 | | | |
| SUB-TOTAL | | 79 | SUB-TOTAL | | 79 |
| GRAND TOTAL | | | GRAND TOTAL | | 158 |

BOX CULVERT SCHEDULE

| CULV. NO. | STATION | RT/LT/AR | TYPE | M273 | | | | PRECAST CONCRETE BOX CULVERT END SECTION | TRAFFIC CONTROL AND PROTECTION STANDARD 701321 |
|----------------------------------|----------------|----------|-------------|------------------------------------|-----------|-----------|-------------|--|--|
| | | | | 3.0m x 0.9m | 750 x 450 | 900 x 450 | 1.2m x 0.6m | | |
| RURAL | | | | | | | | | |
| 8 | STA 5+750.192 | AR | EXTENSION | See Structure Plans For Quantities | | | | | |
| 9 | STA 6+430.000 | AR | BOX CULVERT | | | 16.20 | | 2 | 1 |
| 10 | STA 7+189.092 | AR | EXTENSION | See Structure Plans For Quantities | | | | | |
| 11 | STA 7+421.533 | AR | EXTENSION | See Structure Plans For Quantities | | | | | |
| 12 | STA 7+687.147 | LT | BOX CULVERT | | 18.30 | | | 2 | |
| 13 | STA 7+687.147 | RT | BOX CULVERT | | 18.30 | | | 2 | |
| 14 | STA 8+080.000 | AR | BOX CULVERT | 21.60 | | | | 2 | 1 |
| 15 | STA 8+828.407 | AR | EXTENSION | See Structure Plans For Quantities | | | | | |
| 16 | STA 11+203.000 | AR | EXTENSION | See Structure Plans For Quantities | | | | | |
| 17 | STA 12+895.187 | AR | EXTENSION | See Structure Plans For Quantities | | | | | |
| 18 | STA 13+375.000 | AR | BOX CULVERT | | | 16.20 | | 2 | |
| 19 | STA 13+660.000 | AR | BOX CULVERT | 21.60 | | | | 2 | |
| 20 | STA 15+172.500 | AR | BOX CULVERT | | | | 14.40 | 2 | 1 |
| 21 | STA 15+376.500 | AR | BOX CULVERT | | | 13.50 | | 2 | 1 |
| URBAN | | | | | | | | | |
| 22 | STA 15+940.000 | AR | BOX CULVERT | | 14.40 | | | 2 | 1 |
| TOTALS | | | | 43.20 | 51.00 | 29.70 | 30.60 | 18 | 5 |
| TOTALS ROUNDED FOR SUMMARY SHEET | | | | 43.2 | 51.0 | 29.7 | 30.6 | 18 | 5 |

PAVEMENT MARKING SCHEDULE

| LOCATION | SHORT TERM PAVEMENT MARKING | TEMPORARY PAINT PAVEMENT MARKING LINE | | PAINT PAVEMENT MARKING LINE | | RAISED REFLECTIVE PAVEMENT MARKER (EACH) |
|----------------------------------|-----------------------------|---------------------------------------|----------|-----------------------------|----------|--|
| | | 100 MM | | 100 MM | | |
| | | WHITE | YELLOW | WHITE | YELLOW | |
| STATION TO STATION | METER | METER | METER | METER | METER | |
| RURAL | | | | | | |
| 4+920.234 TO 5+025.0 | 41.9 | 209.5 | 26.2 | 209.5 | 26.2 | 5 |
| 5+025.0 TO 9+385.0 | 1,744.0 | 8,503.6 | 3,771.0 | 8,503.6 | 3,771.0 | 179 |
| 9+580.0 TO 13+750.733 | 1,668.3 | 8,085.5 | 3,907.7 | 8,085.5 | 3,907.7 | 171 |
| 13+752.021 TO 15+545.0 | 717.2 | 3,527.4 | 448.2 | 3,527.4 | 448.2 | 74 |
| SUBTOTAL | | 4,171.4 | 20,326.0 | 8,153.1 | 20,326.0 | 429 |
| URBAN | | | | | | |
| 15+545.0 TO 16+016.0 | 188.4 | 856.9 | 117.8 | 856.9 | 117.8 | 20 |
| TOTALS | | 4,359.8 | 21,182.9 | 8,270.9 | 21,182.9 | 449 |
| TOTALS ROUNDED FOR SUMMARY SHEET | | 4,360 | 21,183 | 8,271 | 21,183 | 449 |

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|-------------------|---------------------------------------|
| NAME | DATE | |
| | | SCHEDULE OF QUANTITIES |
| | | |
| | | |
| | | |
| | | |
| | | |
| SCALE | NO SCALE | DRAWN BY KOJ |
| DATE | DECEMBER 22, 2008 | CHECKED BY LWJ |

PLOT DATE = 12/22/2008
 FILE NAME = H:\V\20\Contract 94533\SHT_014_sch.plt schedules_V8.dgn

H. M. & G. NO. 4420

| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | LYRS | CRAWFORD | 212 | 20 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

TEMPORARY DITCH CHECKS

| STATION | SIDE | QUANTITY EACH | STATION | SIDE | QUANTITY EACH | STATION | SIDE | QUANTITY EACH | STATION | SIDE | QUANTITY EACH | STATION | SIDE | QUANTITY EACH |
|-------------|-------|---------------|----------|-------|---------------|----------|-------|---------------|----------|-------|---------------|----------|-------|---------------|
| 5+025 | LT&RT | 2 | 6+980 | LT&RT | 2 | 9+985 | LT | 1 | 11+740 | RT | 1 | 13+210 | RT | 1 |
| 5+035 | LT | 1 | 7+025 | RT | 1 | 10+010 | RT | 1 | 11+765 | RT | 1 | 13+240 | LT&RT | 2 |
| 5+045 | LT&RT | 2 | 7+145 | RT | 1 | 10+055 | RT | 1 | 11+775 | LT&RT | 2 | 13+275 | LT&RT | 2 |
| 5+065 | LT&RT | 2 | 7+240 | RT | 1 | 10+100 | RT | 1 | 11+800 | LT | 1 | 13+345 | RT | 1 |
| 5+085 | LT | 1 | 7+285 | RT | 1 | 10+200 | RT | 1 | 11+808 | LT | 1 | 13+373 | LT | 1 |
| 5+105 | LT | 1 | 7+330 | RT | 1 | 10+206 | RT | 1 | 11+816 | LT | 1 | 13+374 | RT | 1 |
| 5+115 | RT | 1 | 7+400 | RT | 1 | 10+212 | RT | 1 | 11+824 | LT | 1 | 13+375 | LT | 1 |
| 5+135 | RT | 1 | 7+420 | RT | 1 | 10+218 | RT | 1 | 11+845 | LT | 1 | 13+455 | LT&RT | 2 |
| 5+155 | RT | 1 | 7+424 | LT&RT | 2 | 10+230 | RT | 1 | 11+850 | RT | 1 | 13+535 | LT&RT | 2 |
| 5+225 | LT&RT | 2 | 7+530 | LT&RT | 2 | 10+240 | RT | 1 | 11+865 | LT | 1 | 13+580 | LT&RT | 2 |
| 5+293 | LT&RT | 2 | 7+575 | LT&RT | 2 | 10+250 | RT | 1 | 11+885 | LT | 1 | 13+625 | LT | 1 |
| 5+297 | LT&RT | 2 | 7+620 | LT&RT | 2 | 10+260 | RT | 1 | 11+905 | LT | 1 | 13+658 | LT&RT | 2 |
| 5+400 | LT&RT | 2 | 7+665 | LT&RT | 2 | 10+270 | RT | 1 | 11+925 | LT | 1 | 13+662 | LT&RT | 2 |
| 5+480 | RT | 1 | 7+740 | RT | 1 | 10+290 | LT&RT | 2 | 11+945 | LT | 1 | 13+710 | LT&RT | 2 |
| 5+525 | LT&RT | 2 | 7+785 | LT&RT | 2 | 10+310 | LT&RT | 2 | 11+970 | LT | 1 | 13+755 | LT | 1 |
| 5+570 | LT&RT | 2 | 7+830 | LT&RT | 2 | 10+360 | LT&RT | 2 | 12+035 | LT&RT | 2 | 13+860 | LT&RT | 2 |
| 5+685 | LT&RT | 2 | 7+875 | LT&RT | 2 | 10+380 | LT&RT | 2 | 12+080 | LT&RT | 2 | 13+905 | LT&RT | 2 |
| 5+730 | LT&RT | 2 | 7+920 | LT&RT | 2 | 10+400 | LT&RT | 2 | 12+135 | LT&RT | 2 | 13+950 | LT&RT | 2 |
| 5+747 | LT&RT | 2 | 7+965 | LT&RT | 2 | 10+420 | LT&RT | 2 | 12+220 | LT&RT | 2 | 13+995 | RT | 1 |
| 5+752 | LT&RT | 2 | 8+025 | LT&RT | 2 | 10+440 | LT&RT | 2 | 12+240 | LT&RT | 2 | 14+040 | LT&RT | 2 |
| 5+765 | RT | 1 | 8+045 | LT | 1 | 10+485 | LT&RT | 2 | 12+260 | LT&RT | 2 | 14+085 | LT | 1 |
| 5+780 | RT | 1 | 8+078 | LT | 1 | 10+530 | RT | 1 | 12+280 | LT&RT | 2 | 14+130 | LT&RT | 2 |
| 5+825 | LT&RT | 2 | 8+082 | LT&RT | 2 | 10+555 | LT | 1 | 12+300 | LT&RT | 2 | 14+175 | LT&RT | 2 |
| 5+870 | LT&RT | 2 | 8+105 | RT | 1 | 10+600 | LT&RT | 2 | 12+320 | LT&RT | 2 | 14+345 | LT&RT | 2 |
| 5+915 | LT&RT | 2 | 8+125 | LT&RT | 2 | 10+645 | LT&RT | 2 | 12+380 | LT&RT | 2 | 14+390 | LT&RT | 2 |
| 5+960 | LT | 1 | 8+170 | LT | 1 | 10+690 | LT&RT | 2 | 12+400 | LT&RT | 2 | 14+515 | LT&RT | 2 |
| 5+990 | RT | 1 | 8+215 | LT&RT | 2 | 10+795 | RT | 1 | 12+420 | LT&RT | 2 | 14+560 | RT | 1 |
| 6+005 | LT | 1 | 8+260 | LT&RT | 2 | 10+840 | RT | 1 | 12+445 | LT&RT | 2 | 14+794 | RT | 1 |
| 6+035 | RT | 1 | 8+365 | LT&RT | 2 | 10+875 | LT | 1 | 12+500 | LT | 1 | 14+813 | RT | 1 |
| 6+050 | LT | 1 | 8+410 | LT&RT | 2 | 10+970 | LT&RT | 2 | 12+545 | LT | 1 | 14+835 | RT | 1 |
| 6+095 | LT | 1 | 8+510 | LT&RT | 2 | 11+015 | LT&RT | 2 | 12+590 | LT | 1 | 14+841 | LT | 1 |
| 6+140 | LT | 1 | 8+555 | RT | 1 | 11+100 | LT | 1 | 12+700 | RT | 1 | 14+860 | LT | 1 |
| 6+235 | LT&RT | 2 | 8+675 | LT | 1 | 11+150 | RT | 1 | 12+720 | RT | 1 | 14+880 | LT | 1 |
| 6+275 | LT&RT | 2 | 8+826 | LT | 1 | 11+201 | LT&RT | 2 | 12+740 | LT&RT | 2 | 14+900 | LT | 1 |
| 6+320 | LT | 1 | 8+945 | LT | 1 | 11+205 | LT&RT | 2 | 12+750 | LT&RT | 2 | 14+980 | RT | 1 |
| 6+380 | LT&RT | 2 | 8+975 | LT | 1 | 11+250 | LT | 1 | 12+760 | LT&RT | 2 | 15+025 | LT&RT | 2 |
| 6+428 | LT&RT | 2 | 9+000 | LT&RT | 2 | 11+300 | RT | 1 | 12+770 | LT&RT | 2 | 15+083 | RT | 1 |
| 6+600 | RT | 1 | 9+020 | LT&RT | 2 | 11+375 | LT&RT | 2 | 12+780 | LT&RT | 2 | 15+100 | RT | 1 |
| 6+660 | RT | 1 | 9+040 | LT&RT | 2 | 11+400 | LT&RT | 2 | 12+790 | LT&RT | 2 | 15+130 | LT | 1 |
| 6+680 | LT&RT | 2 | 9+060 | RT | 1 | 11+415 | RT | 1 | 12+800 | LT&RT | 2 | 15+145 | RT | 1 |
| 6+700 | LT&RT | 2 | 9+080 | RT | 1 | 11+420 | LT | 1 | 12+810 | LT&RT | 2 | 15+171 | LT&RT | 2 |
| 6+720 | LT&RT | 2 | 9+100 | LT&RT | 2 | 11+430 | RT | 1 | 12+820 | LT&RT | 2 | 15+174 | LT | 1 |
| 6+740 | LT | 1 | 9+120 | LT&RT | 2 | 11+440 | LT | 1 | 12+830 | LT&RT | 2 | 15+220 | RT | 1 |
| 6+760 | LT | 1 | 9+140 | RT | 1 | 11+445 | RT | 1 | 12+840 | LT&RT | 2 | 15+265 | LT&RT | 2 |
| 6+780 | LT | 1 | 9+160 | RT | 1 | 11+455 | LT | 1 | 12+850 | LT&RT | 2 | 15+310 | RT | 1 |
| 6+800 | LT | 1 | 9+745 | LT&RT | 2 | 11+460 | LT | 1 | 12+900 | LT&RT | 2 | 15+355 | RT | 1 |
| 6+820 | LT | 1 | 9+765 | LT&RT | 2 | 11+465 | LT | 1 | 12+925 | LT&RT | 2 | 15+420 | LT&RT | 2 |
| 6+840 | LT | 1 | 9+785 | LT&RT | 2 | 11+470 | LT | 1 | 12+950 | LT&RT | 2 | 15+575 | LT&RT | 2 |
| 6+850 | RT | 1 | 9+805 | LT&RT | 2 | 11+525 | LT | 1 | 12+975 | LT&RT | 2 | 15+620 | RT | 1 |
| 6+865 | LT | 1 | 9+825 | LT&RT | 2 | 11+545 | LT | 1 | 13+000 | RT | 1 | 15+715 | LT | 1 |
| 6+870 | RT | 1 | 9+845 | LT&RT | 2 | 11+565 | LT | 1 | 13+030 | LT&RT | 2 | 15+825 | LT&RT | 2 |
| 6+930 | RT | 1 | 9+865 | LT&RT | 2 | 11+585 | LT | 1 | 13+060 | RT | 1 | 15+931 | LT | 1 |
| 6+945 | LT&RT | 2 | 9+885 | LT&RT | 2 | 11+605 | LT&RT | 2 | 13+120 | RT | 1 | 15+937 | LT | 1 |
| 6+960 | LT&RT | 2 | 9+905 | LT&RT | 2 | 11+650 | LT&RT | 2 | 13+150 | RT | 1 | 15+939 | RT | 1 |
| 6+970 | LT&RT | 2 | 9+925 | LT&RT | 2 | 11+725 | RT | 1 | 13+180 | RT | 1 | 15+975 | RT | 1 |
| SUBTOTAL | | 81 | SUBTOTAL | | 89 | SUBTOTAL | | 74 | SUBTOTAL | | 86 | SUBTOTAL | | 78 |
| GRAND TOTAL | | | | | | | | | | | | | | 408 |

PERMANENT SURVEY MARKERS, TYPE 1

| | STATION | QUANTITY EACH | | STATION | QUANTITY EACH |
|-------|-----------|---------------|----|------------|---------------|
| PC | 4+986.943 | 1 | PT | 9+239.361 | 1 |
| PI | 5+262.393 | 1 | PC | 9+857.911 | 1 |
| PT | 5+536.529 | 1 | PI | 9+979.834 | 1 |
| PC | 5+944.938 | 1 | PT | 10+101.752 | 1 |
| PI | 6+033.238 | 1 | PC | 10+757.670 | 1 |
| PT | 6+120.199 | 1 | PI | 10+871.558 | 1 |
| PC | 6+380.742 | 1 | PT | 10+985.426 | 1 |
| PI | 6+473.669 | 1 | PC | 11+902.118 | 1 |
| PT | 6+565.330 | 1 | PI | 12+007.014 | 1 |
| PC | 6+986.819 | 1 | PT | 12+110.906 | 1 |
| PI | 7+052.666 | 1 | PC | 12+324.152 | 1 |
| PT | 7+118.219 | 1 | PI | 12+439.891 | 1 |
| PC | 7+373.093 | 1 | PT | 12+554.276 | 1 |
| PI | 7+419.751 | 1 | PC | 13+143.500 | 1 |
| PT | 7+466.210 | 1 | PI | 13+297.116 | 1 |
| PC | 8+841.374 | 1 | PT | 13+450.475 | 1 |
| PI | 8+936.594 | 1 | PC | 13+450.475 | 1 |
| PT | 8+936.594 | 1 | PI | 13+600.724 | 1 |
| PC | 9+157.766 | 1 | PT | 13+750.733 | 1 |
| PI | 9.198.572 | 1 | | | |
| TOTAL | | | | 39 | |

PLOT DATE = 12/22/2008
FILE NAME = H:\4426\Contract 94533\SHI_B14_sch_all_schedules_V8.dgn

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|----------------|--------------|---------------------------------------|
| NAME | DATE | |
| | | SCHEDULE OF QUANTITIES |
| | | |
| | | |
| | | |
| | | |
| SCALE NO SCALE | DRAWN BY KOJ | CHECKED BY |
| DATE | | |

| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 22 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

STEEL PLATE BEAM GUARDRAIL AND TRAFFIC BARRIER TERMINALS

| LOCATION | | | | STEEL PLATE BEAM GUARDRAIL, TYPE A | TRAFFIC BARRIER TERMINAL, TYPE 1, SPECIAL (TANGENT) | TRAFFIC BARRIER TERMINAL, TYPE 1 SPECIAL (FLARED) | TRAFFIC BARRIER TERMINAL, TYPE 2 | GUARDRAIL MARKERS, TYPE A | TERMINAL MARKER DIRECT APPLIED |
|---|----|-----------|-------|------------------------------------|---|---|----------------------------------|---------------------------|--------------------------------|
| STATION | TO | STATION | RT/LT | M6300100 METER | 63100167 EACH | 63100169 EACH | 63100045 EACH | 78200410 EACH | 78201000 EACH |
| RURAL | | | | | | | | | |
| 7+423.80 | TO | 7+472.80 | RT | 34.29 | 2 | | | 4 | 2 |
| 7+483.20 | TO | 7+531.00 | RT | 38.10 | 1 | | 1 | 4 | 2 |
| 10+130.00 | TO | 10+202.30 | RT | 72.39 | 1 | 1 | | 4 | 2 |
| 10+145.00 | TO | 10+270.70 | LT | 110.49 | 2 | | | 4 | 2 |
| 11+408.10 | TO | 11+507.10 | RT | 83.82 | 2 | | | 4 | 2 |
| 11+472.90 | TO | 11+587.2 | LT | 99.06 | 2 | | | 4 | 2 |
| TOTALS | | | | 438.15 | 10 | 1 | 1 | 24 | 12 |
| TOTALS ROUNDED FOR SUMMARY SHEET | | | | 438.15 | 10 | 1 | 1 | 24 | 12 |

PROPOSED PAVEMENT SCHEDULE

| LOCATION | | | | PROCESSING MODIFIED SOIL MIXTURE, 300MM | LIME | SUB-BASE GRANULAR MATERIAL, TYPE B, 100MM | HOT-MIX ASPHALT BASE COURSE, VARIABLE DEPTH | BITUMINOUS MATERIALS (PRIME COAT) | | AGGREGATE (PRIME COAT) | LEVEING BINDER (MACHINE METHOD), N70 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 | HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH) 350MM | AGGREGATE SHOULDERS, TYPE B, 150MM | HOT-MIX ASPHALT SHOULDERS, 150MM |
|---|----|----------------|-------|---|----------------|---|---|-----------------------------------|-------------------------|------------------------|--------------------------------------|--|---|------------------------------------|----------------------------------|
| STATION | TO | STATION | LT/RT | M3020456 SQ M | M3021500 M TON | M3111100 SQ M | M3552100 M TON | PRIOR TO BINDER/BASE COURSE | PRIOR TO SURFACE COURSE | M4060300 M TON | M4062135 M TON | M4063340 M TON | M4075350 SQ M | M4812150 SQ M | M4820550 SQ M |
| RURAL | | | | | | | | | | | | | | | |
| 4+920.234 | TO | 5+025.000 | LT&RT | | | | | | 330.01 | 1.41 | | 85.99 | | | |
| 5+025.000 | TO | 5+045.000 | LT&RT | | | | | 61.25 | 61.25 | 0.53 | 821.40 | 15.96 | | 60.00 | 36.00 |
| 5+045.000 | TO | 6+825.000 | LT&RT | | | | | 5295.50 | 5295.50 | 45.39 | | 1379.86 | | 4707.38 | 3112.92 |
| 6+825.000 | TO | 7+100.000 | LT&RT | | | | 2589.00 | 1106.88 | 1106.88 | 9.49 | | 288.42 | | | 495.00 |
| 7+100.000 | TO | 9+365.000 | LT&RT | | | | | 6738.38 | 6738.38 | 57.76 | 985.20 | 1755.83 | | 5878.15 | 4313.25 |
| 9+365.000 | TO | 9+385.000 | LT&RT | | | | | 59.50 | 59.50 | 0.51 | | 15.50 | | 24.37 | 36.00 |
| BIG CREEK BRIDGE AND ROAD OMMISION | | | | | | | | | | | | | | | |
| 9+580.000 | TO | 9+600.000 | LT&RT | | | | | 59.50 | 59.50 | 0.51 | | 15.50 | | 48.73 | 36.00 |
| 9+600.000 | TO | 13+143.500 | LT | | | | | 5536.25 | 5536.25 | 47.45 | 2061.38 | 1442.59 | | 4434.72 | 3905.75 |
| 9+600.000 | TO | 13+095.000 | RT | | | | | 5412.25 | 5412.25 | 46.39 | | 1410.28 | | 4489.80 | 3679.07 |
| 13+095.000 | TO | 13+143.500 | RT | | | | | 97.61 | 97.61 | 0.84 | | 25.43 | | | 43.65 |
| 13+143.500 | TO | 13+750.733(BK) | LT&RT | 7711.86 | 158.62 | 7711.86 | | 11567.79 | 2444.11 | 10.47 | | | 6983.18 | | |
| 13+752.021(AHD) | TO | 15+545.000 | LT | | | | | 2667.06 | 2667.06 | 22.86 | 418.50 | 694.96 | | 2287.33 | 1549.78 |
| 13+752.021(AHD) | TO | 13+810.000 | RT | | | | | 116.68 | 116.68 | 1.00 | 418.50 | 30.40 | | | 52.18 |
| 13+810.000 | TO | 15+545.000 | RT | | | | | 2580.81 | 2580.81 | 22.12 | | 672.49 | | 2367.42 | 1529.01 |
| SUBTOTAL | | | | 7711.86 | 158.62 | 7711.86 | 2589.00 | 41299.44 | 32505.78 | 266.73 | 4704.984 | 7833.21 | 6983.18 | 24297.90 | 18788.61 |
| URBAN | | | | | | | | | | | | | | | |
| 15+545.000 | TO | 15+996.000 | LT&RT | | | | | 1341.73 | 1341.73 | 11.50 | 248.71 | 349.62 | | 1181.04 | 757.53 |
| 15+996.000 | TO | 16+016.000 | LT&RT | | | | | 59.85 | 59.85 | 0.51 | | 15.60 | | 54.93 | 36.00 |
| SUBTOTAL | | | | | | | | 1401.58 | 1401.58 | 12.01 | 248.712 | 365.21 | | 1235.97 | 793.53 |
| TOTAL | | | | 7711.86 | 158.62 | 7711.86 | 2589.00 | 42701.01 | 33907.35 | 278.75 | 4953.70 | 8198.42 | 6983.18 | 25533.87 | 19582.14 |
| TOTALS ROUNDED FOR SUMMARY SHEET | | | | 7,712 | 158.7 | 7,712 | 2,589 | 76,609 | | 279 | 4,954 | 8,199 | 6,984 | 25,534 | 19,583 |

PLOT DATE = 12/22/2008
FILE NAME = H:\V420\Contract 94533\SH1_014_sch.e11_schedules_18.dgn

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|-------------------|---------------------------------------|
| NAME | DATE | |
| | | SCHEDULE OF QUANTITIES |
| | | |
| | | |
| | | |
| | | |
| | | |
| SCALE | NO SCALE | DRAWN BY KOJ |
| DATE | DECEMBER 22, 2008 | CHECKED BY LWJ |

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------|------------------|-----------|
| 781 | LYRS | CRAWFORD | 212 | 23 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

PIPE CULVERT SCHEDULE

| LOCATION | TYPE | TRENCH BACKFILL | PIPE CULVERTS | | | | | | | | | | | | | | PRECAST REINFORCED CONCRETE FLARED END SECTIONS | | | | | END SECTIONS | | | | | | | | | | PIPE TEES | | | INLETS | | | |
|---------------|-----------|-----------------|---------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|-------------|---|-------|-------|-------|-------|--------------|-------|-------|-------|-------|-------|-------|-------|-------|------------------------------|------------------------------|------------------------------|-------------------------|--------|-------|-----|-----|
| | | | RCCP | | | | | | | CLASS D | | | | | | | 375 | 450 | 600 | 750 | 900 | 300 | 375 | 450 | 525 | 600 | 675 | 750 | 900 | 1050 | 375 PIPE WITH 300 RISER EACH | 600 PIPE WITH 375 RISER EACH | 750 PIPE WITH 375 RISER EACH | TYPE A, TY 8 GRATE EACH | | | | |
| | | | TYPE 1 375 | TYPE 1 450 | TYPE 1 600 | TYPE 1 750 | TYPE 1 900 | TYPE 1 300 | TYPE 1 375 | TYPE 1 450 | TYPE 1 525 | TYPE 1 600 | TYPE 1 675 | TYPE 1 750 | TYPE 1 900 | TYPE 1 1050 | | | | | | | | | | | | | | | | | | | 375 | 450 | 600 | 750 |
| STATION | RT/LT /AR | CUBIC METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | METER | | |
| RURAL | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 5+141.000 | LT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 5+193.700 | LT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 5+194.097 | RT | 6.40 | 22.56 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 5+295.000 | AR | 7.14 | | | 17.07 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 5+345.500 | LT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 5+345.500 | RT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 5+466.700 | LT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 5+618.000 | RT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 5+621.500 | LT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 5+651.500 | RT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 5+720.000 | LT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 5+771.700 | LT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 5+938.000 | RT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 6+084.500 | RT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 6+140.500 | RT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 6+180.600 | LT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 6+181.600 | RT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 6+441.000 | RT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 6+444.936 | LT | 2.04 | | | 11.64 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 6+483.500 | RT | 5.38 | 16.76 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 6+500.000 | LT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 6+562.500 | LT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 6+572.500 | RT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 6+592.700 | LT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 6+632.800 | RT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 6+636.700 | LT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 6+643.500 | LT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 6+652.100 | LT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 6+746.500 | RT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 6+784.500 | RT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 6+819.200 | RT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 6+907.000 | RT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 7+015.000 | LT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 7+071.000 | RT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 7+177.000 | RT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 7+291.800 | LT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 7+328.600 | LT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 7+392.900 | LT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 7+471.600 | LT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 7+478.000 | RT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 7+509.000 | LT | 3.73 | | | 9.14 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 7+537.000 | LT | 11.22 | | | 23.16 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 7+682.000 | RT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 7+755.000 | LT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 8+063.000 | RT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 8+161.200 | RT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 8+296.000 | LT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STA 8+311.000 | RT | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

CONTINUED

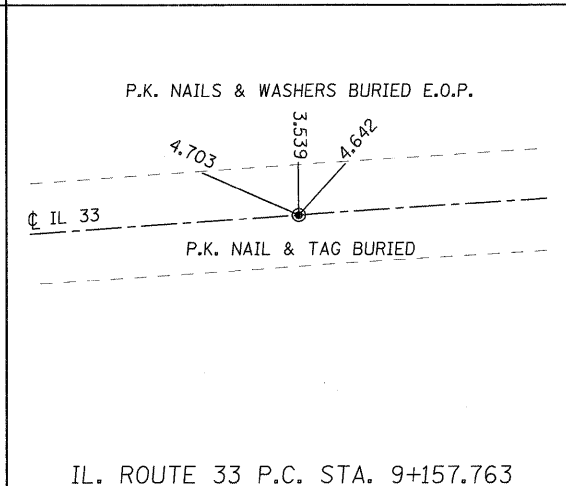
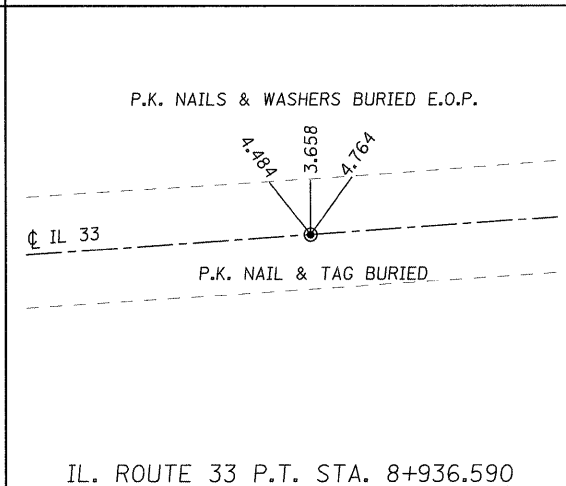
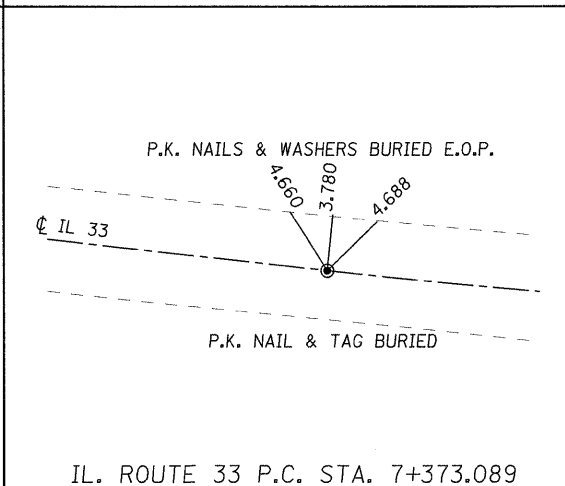
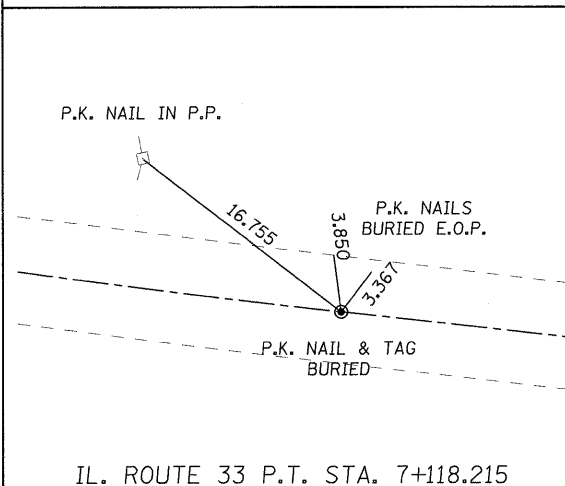
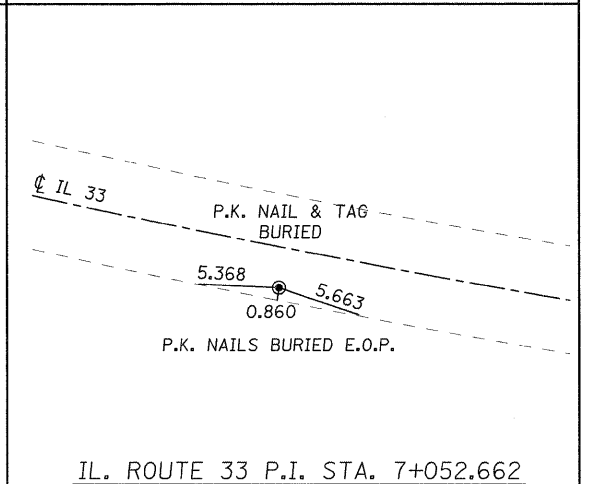
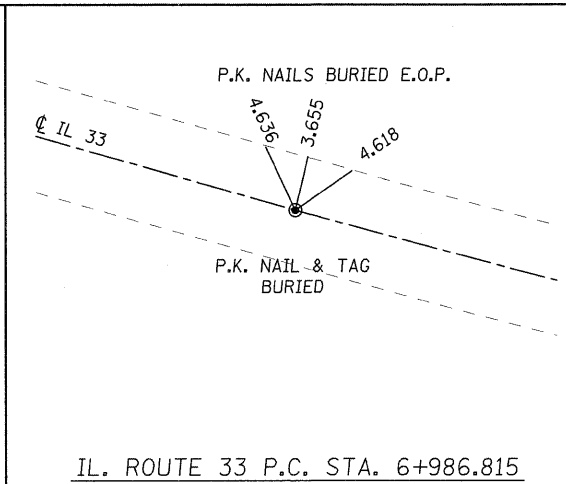
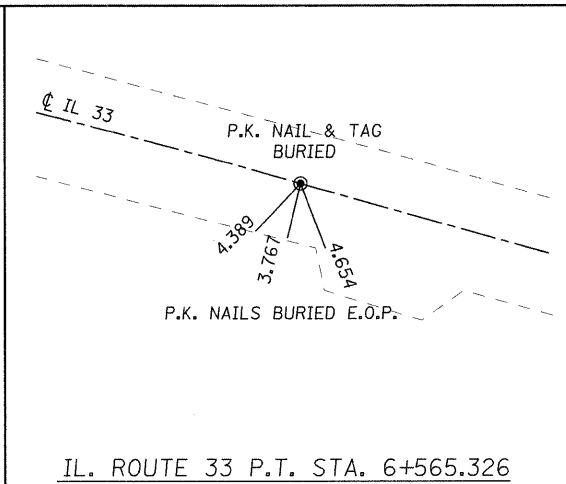
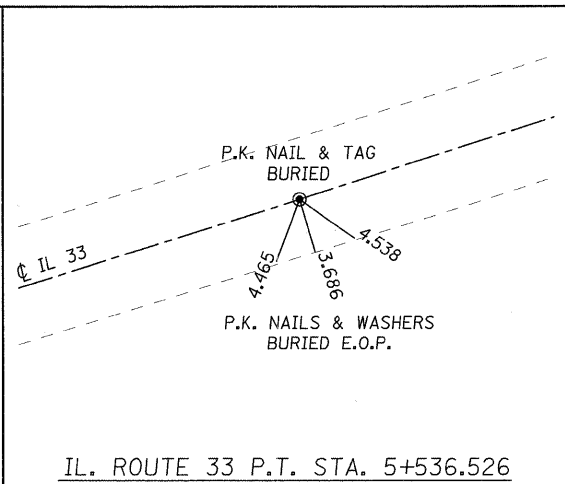
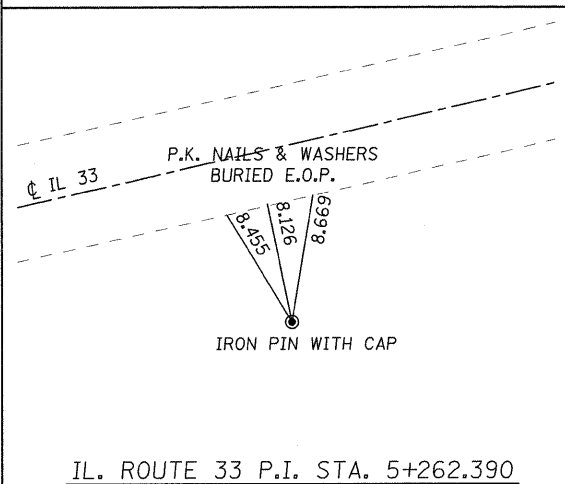
PLOT DATE = 12/22/2008
FILE NAME = H:\4428\Cont-est_94533\SH-024_sch-pipeculverts_V8.dgn

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|---|
| NAME | DATE | |
| | | <p align="center">PIPE CULVERT SCHEDULE</p> <p>SCALE _____ DRAWN BY KOJ</p> <p>DATE DECEMBER 22, 2008 CHECKED BY LWJ</p> |
| | | |
| | | |
| | | |
| | | |

| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | LYRS | CRAWFORD | 212 | 27 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



NOTE: NORTH ARROW APPLIES TO ALL TIES



PLOT DATE = 12/22/2008
FILE NAME = H:\4426\Contract 94533\SHIT-027.dwg

H. M. & G. NO. 4420

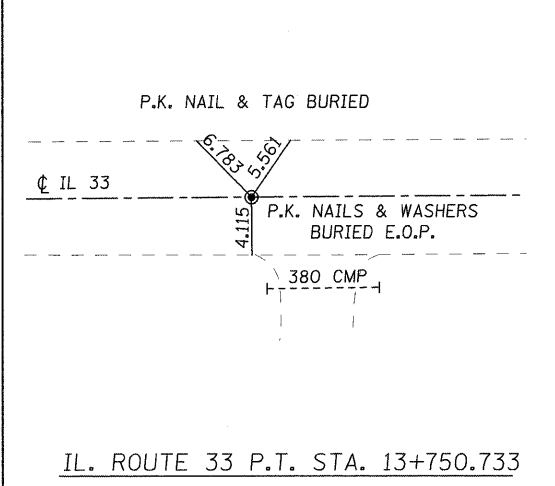
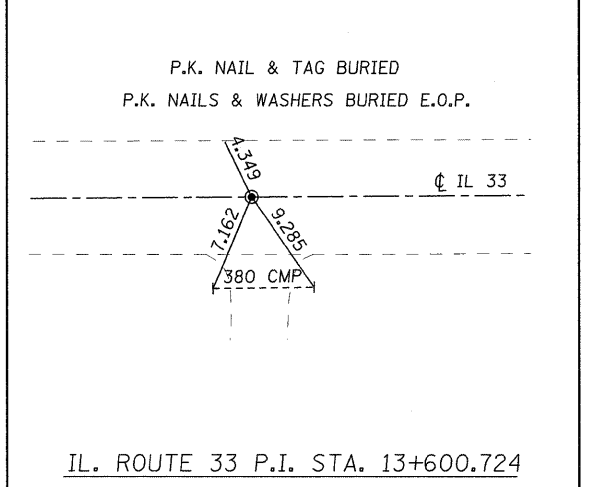
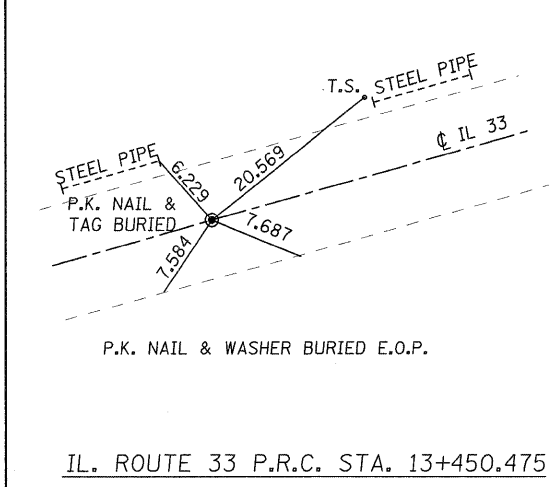
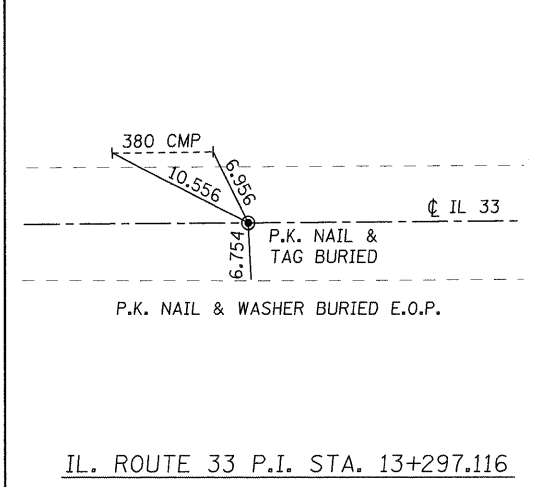
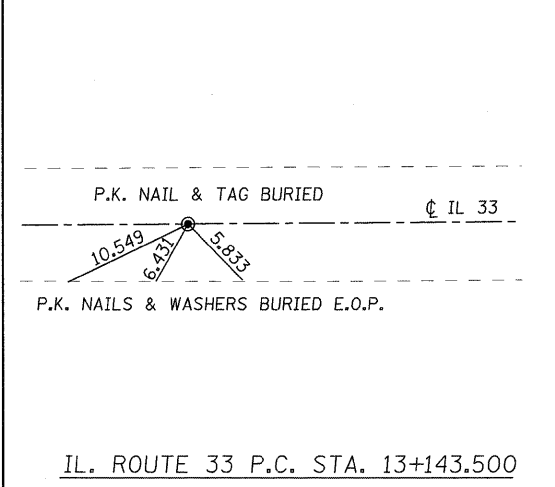
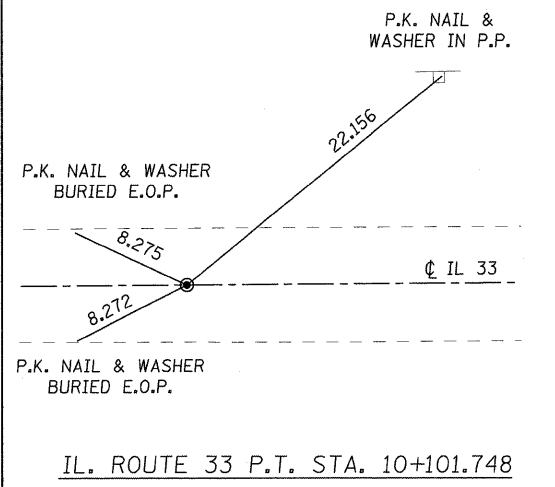
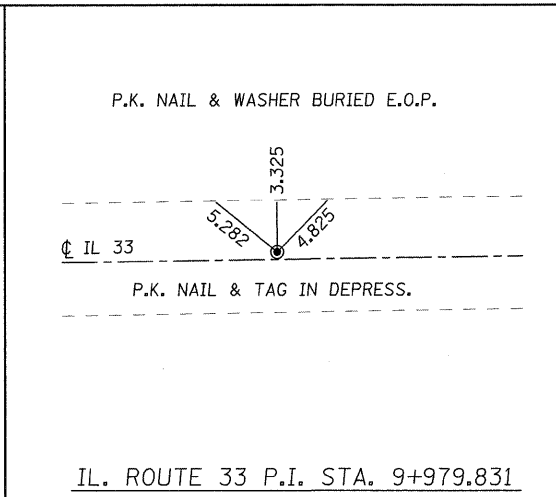
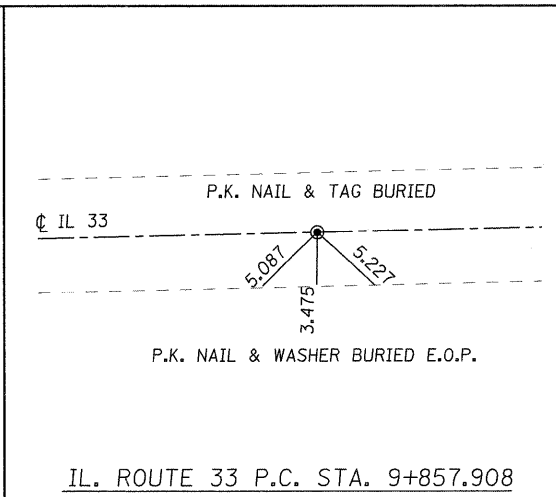
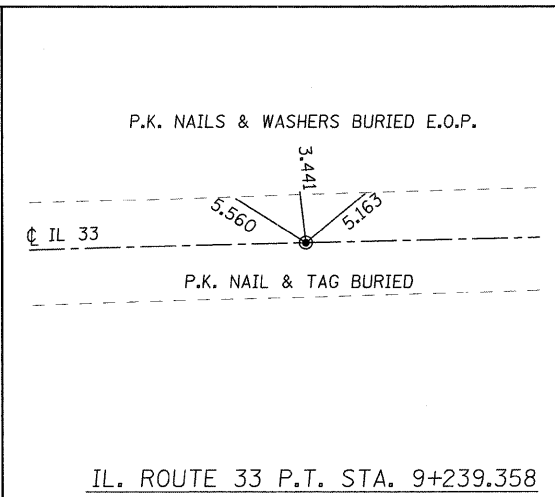
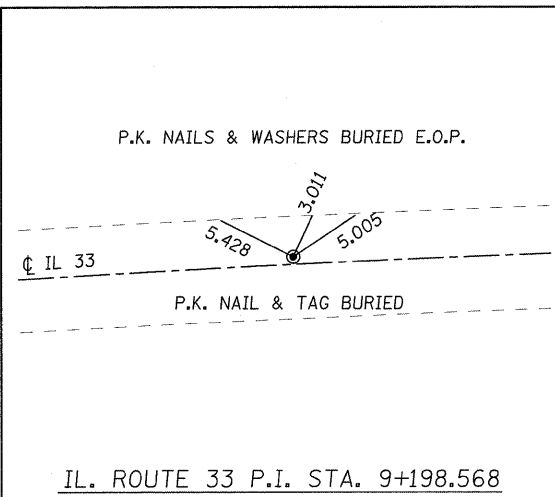
| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
LOCAL TIES
SCALE NO SCALE
DATE DECEMBER 22, 2008
DRAWN BY EDW
CHECKED BY LWJ

| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | LYRS | CRAWFORD | 212 | 28 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



NOTE: NORTH ARROW APPLIES TO ALL TIES



PLOT DATE = 12/22/2008
FILE NAME = H:\4428\Contract_94533\SH1_028.dwg

H. M. & G. NO. 4420

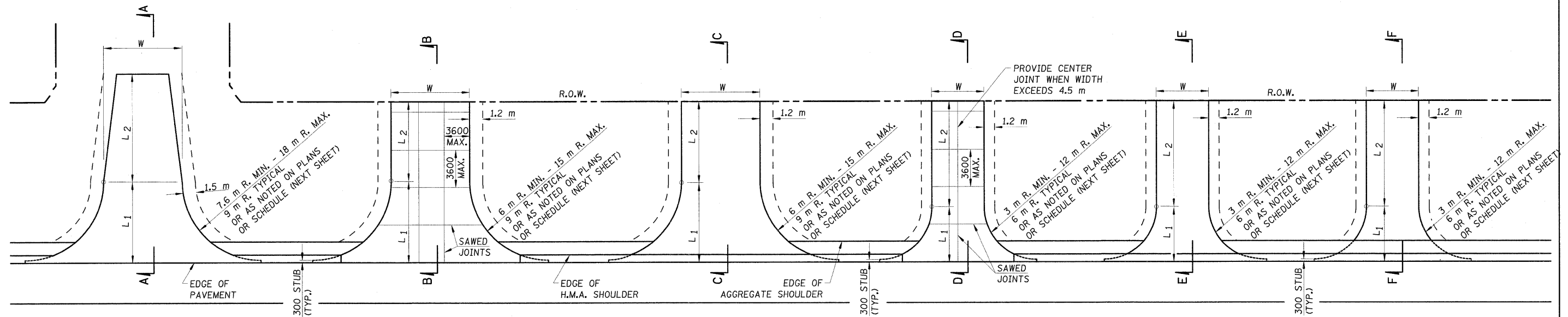
| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION

LOCAL TIES

SCALE NO SCALE DRAWN BY EDW
DATE DECEMBER 22, 2008 CHECKED BY LWJ

| | | | | |
|---------------------|----------|------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 29 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



PUBLIC ROAD

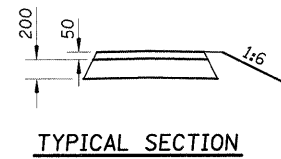
COMMERCIAL (P.C.C.)

COMMERCIAL (H.M.A.)

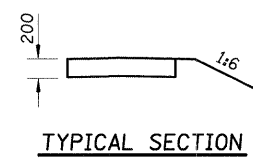
PRIVATE ENTRANCE (P.C.C.)

PRIVATE ENTRANCE (H.M.A.)

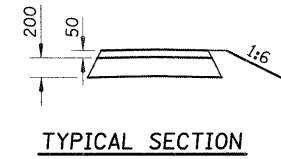
FIELD ENTRANCE (AGGREGATE)



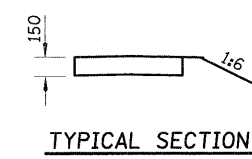
TYPICAL SECTION



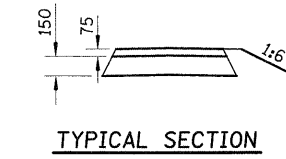
TYPICAL SECTION



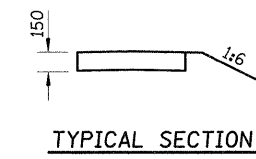
TYPICAL SECTION



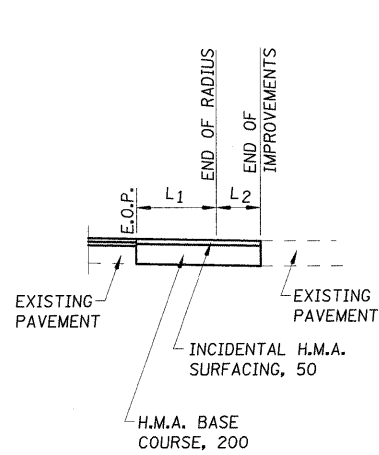
TYPICAL SECTION



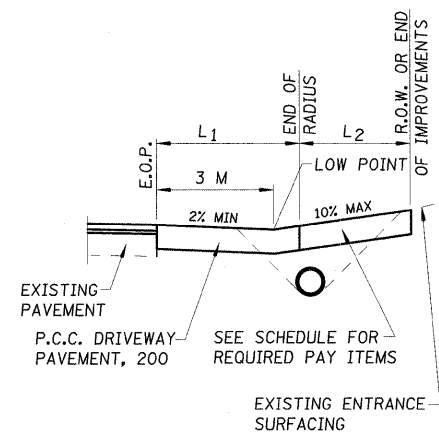
TYPICAL SECTION



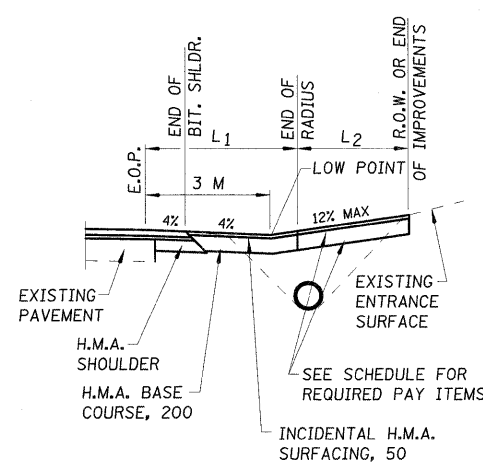
TYPICAL SECTION



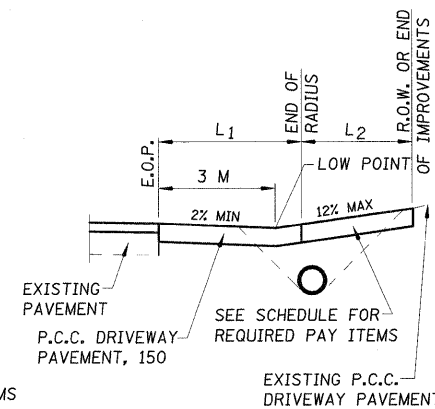
SECTION A-A



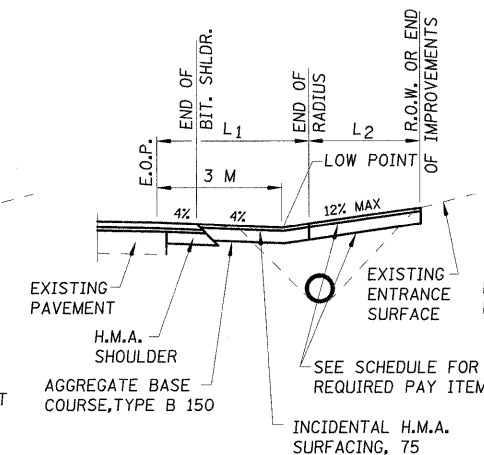
SECTION B-B



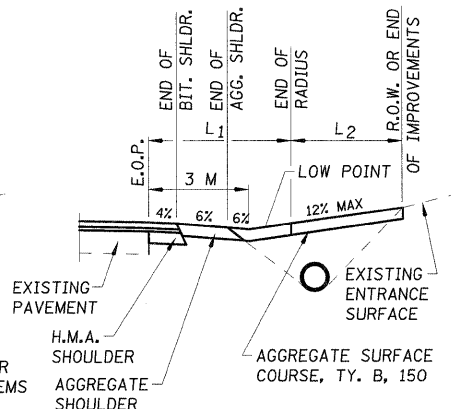
SECTION C-C



SECTION D-D



SECTION E-E



SECTION F-F

NOTES:

- L₁ = DISTANCE FROM EDGE OF PAVEMENT TO RADIUS POINT
- L₂ = DISTANCE FROM RADIUS POINT TO R.O.W. OR END OF IMPROVEMENT
- MATERIAL USED TO CONSTRUCT L₂ LENGTH SHALL BE OF THE SAME TYPE OF MATERIAL AS THE EXISTING ENTRANCE
- ALL DIMENSIONS ARE IN MILLIMETERS UNLESS OTHERWISE SHOWN.
- APPLICATION OF BITUMINOUS MATERIALS (PRIMECOAT) AND AGGREGATE (PRIMECOAT) SHALL BE INCLUDED IN COST OF INCIDENTAL HMA SURFACING.

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
RURAL ENTRANCE DETAILS
 SCALE AS SHOWN DRAWN BY EDW
 DATE DECEMBER 22, 2008 CHECKED BY LWJ

| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | LYRS | CRAWFORD | 212 | 30 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

RURAL ENTRANCE SCHEDULE

| STATION | LT/RT | TYPE | EXISTING MATERIAL | WIDTH | LENGTH | | | RADIUS | AGGREGATE BASE COURSE, TYPE B | HOT-MIX ASPHALT BASE COURSE | AGGREGATE SURFACE COURSE, TYPE B | BITUMINOUS MATERIALS (PRIME COAT) | | AGGREGATE (PRIME COAT) | INCIDENTAL HOT-MIX ASPHALT SURFACING | | PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT | |
|---------------|-------|------|-------------------|-------|-----------|-----------|----------|--------|-------------------------------|-----------------------------|----------------------------------|-----------------------------------|----------|------------------------|--------------------------------------|-------|--|--------|
| | | | | | 150MM | 200MM | 150MM | | PRIOR TO BINDER/BASE COURSE | PRIOR TO SURFACE COURSE | 50MM | 75MM | 150 MM | | 200 MM | | | |
| | | | | | M3511010 | M3550500 | M4021010 | | INFORMATION ONLY | | M4080500 | M4230150 | M4230200 | | | | | |
| | | | | W1 | L1 | L2 | | M TON | SQ. M | M TON | LITER | LITER | M TON | M TON | M TON | SQ. M | SQ. M | |
| | | | | METER | METER | METER | METER | | | | | | | | | | | |
| RURAL | | | | | | | | | | | | | | | | | | |
| STA 5+088.700 | RT | PE | OIL & CHIP | 4.0 | 6.30 | 8.64 | 6 | 23.95 | | | | | | | | 11.98 | | |
| STA 5+141.000 | LT | CE | AGGREGATE | 9.0 | 9.30 | 3.35 | 9 | | 102.13 | 10.85 | | 153.19 | 35.74 | 0.153 | 12.26 | | | |
| STA 5+193.700 | LT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 | 10.72 | | 11.19 | | | 44.69 | | | 5.36 | | |
| STA 5+194.097 | RT | ROAD | OIL & CHIP | 6.0 | See Plans | See Plans | 15 | | 227.25 | | | 340.87 | 79.54 | 0.341 | 27.27 | | | |
| STA 5+345.500 | LT | FE | DIRT | 3.6 | 6.30 | 8.64 | 6 | | | 17.51 | | | | | | | | |
| STA 5+345.500 | RT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 | 10.72 | | 11.19 | | | 44.69 | | | 5.36 | | |
| STA 5+466.700 | LT | PE | AGGREGATE | 4.6 | 6.30 | 8.64 | 6 | 12.67 | | 14.30 | | | 52.79 | | | 6.33 | | |
| STA 5+618.000 | RT | CE | AGGREGATE | 10.6 | 9.30 | 0.00 | 9 | | 115.64 | | | 173.46 | 40.47 | 0.173 | 13.88 | | | |
| STA 5+621.500 | LT | PE | AGGREGATE | 7.2 | 6.30 | 5.59 | 6 | 17.73 | | 14.49 | | | 73.86 | | | 8.86 | | |
| STA 5+651.500 | RT | CE | AGGREGATE | 10.6 | 9.30 | 0.00 | 9 | | 115.64 | | | 173.46 | 40.47 | 0.173 | 13.88 | | | |
| STA 5+720.000 | LT | FE | DIRT | 4.0 | 4.30 | 7.58 | 4 | | | 13.86 | | | | | | | | |
| STA 5+771.700 | LT | FE | DIRT | 3.6 | 4.30 | 7.59 | 4 | | | 12.51 | | | | | | | | |
| STA 5+938.000 | RT | FE | DIRT | 4.0 | 6.30 | 5.59 | 6 | | | 14.94 | | | | | | | | |
| STA 6+084.500 | RT | PE | AGGREGATE | 3.6 | 6.30 | 5.59 | 6 | 10.70 | | 7.24 | | | 44.58 | | | 5.35 | | |
| STA 6+140.500 | RT | PE | AGGREGATE | 4.2 | 6.30 | 5.59 | 6 | 11.89 | | 8.45 | | | 49.56 | | | 5.95 | | |
| STA 6+180.600 | LT | FE | AGGREGATE | 4.2 | 6.30 | 5.59 | 6 | | | 15.62 | | | | | | | | |
| STA 6+181.600 | RT | PE | AGGREGATE | 4.2 | 6.30 | 5.59 | 6 | 11.89 | | 8.45 | | | 49.56 | | | 5.95 | | |
| STA 6+309.000 | RT | CE | AGGREGATE | 10.6 | 11.89 | 0.00 | 15 | | 165.80 | | | 248.70 | 58.03 | 0.249 | 19.90 | | | |
| STA 6+342.500 | RT | CE | AGGREGATE | 10.6 | 11.89 | 0.00 | 15 | | 165.80 | | | 248.70 | 58.03 | 0.249 | 19.90 | | | |
| STA 6+444.900 | LT | ROAD | OIL & CHIP | 6.0 | See Plans | See Plans | 15 | | 241.02 | | | 361.54 | 84.36 | 0.362 | 28.92 | | | |
| STA 6+446.400 | RT | PE | AGGREGATE | 5.0 | 6.30 | 2.90 | 6 | 18.70 | | 5.21 | | | 77.91 | | | 5.21 | | |
| STA 6+446.400 | RT | ROAD | OIL & CHIP | 6.0 | See Plans | See Plans | 15 | | 290.99 | | | 436.49 | 101.85 | 0.436 | 34.92 | | | |
| STA 6+483.500 | RT | CE | CONCRETE | 10.6 | 6.30 | 5.59 | 6 | | | | | | | | | | | 145.07 |
| STA 6+500.000 | LT | CE | AGGREGATE | 10.6 | 6.30 | 5.59 | 6 | | 67.71 | 21.33 | 101.57 | 23.70 | 0.102 | 8.13 | | | | |
| STA 6+524.000 | LT | PE | AGGREGATE | 5.9 | 6.30 | 5.59 | 6 | 15.23 | | 11.87 | | | 63.47 | | | 7.62 | | |
| STA 6+524.500 | RT | CE | AGGREGATE | 10.6 | 6.30 | 5.59 | 6 | | 67.49 | 21.33 | 101.24 | 23.62 | 0.101 | 8.10 | | | | |
| STA 6+562.500 | LT | PE | AGGREGATE | 3.9 | 6.30 | 5.59 | 6 | 11.33 | | 7.85 | | | 47.22 | | | 5.67 | | |
| STA 6+572.500 | RT | CE | BIT/AGGREGATE | 7.3 | 6.30 | 5.59 | 6 | | 49.78 | 14.69 | 74.67 | 17.42 | 0.075 | 5.97 | | | | |
| STA 6+592.700 | LT | PE | AGGREGATE | 3.8 | 6.30 | 5.59 | 6 | 11.12 | | 7.65 | | | 46.32 | | | 5.56 | | |
| STA 6+632.800 | RT | FE | DIRT | 4.0 | 6.30 | 5.59 | 6 | | | 14.94 | | | | | | | | |
| STA 6+636.700 | LT | PE | AGGREGATE | 3.6 | 6.30 | 5.59 | 6 & 5 | 10.06 | | 7.24 | | | 41.94 | | | 5.03 | | |
| STA 6+652.100 | LT | PE | AGGREGATE | 7.2 | 6.30 | 5.59 | 5 & 6 | 17.06 | | 14.49 | | | 71.09 | | | 8.53 | | |
| STA 6+746.500 | RT | PE | AGGREGATE | 5.0 | 6.30 | 5.59 | 6 | 13.45 | | 10.06 | | | 56.04 | | | 6.73 | | |
| STA 6+784.500 | RT | PE | AGGR/BIT | 3.6 | 6.30 | 5.59 | 6 | 17.97 | | | | | 74.88 | | | 8.99 | | |
| STA 6+819.200 | RT | PE | BITUMINOUS | 4.2 | 6.30 | 5.59 | 6 | 20.35 | | | | | 84.77 | | | 10.17 | | |
| STA 6+907.000 | RT | FE | AGGREGATE | 3.6 | 6.30 | 13.35 | 6 | | | 23.63 | | | | | | | | |
| STA 7+015.000 | LT | CE | AGGREGATE | 7.2 | 6.30 | 8.64 | 6 | | 49.17 | 22.39 | 73.76 | 17.21 | 0.074 | 5.90 | | | | |
| STA 7+071.000 | RT | PE | ----- | 3.7 | 6.30 | 8.68 | 6 | 10.96 | | 11.56 | | | 45.66 | | | 5.48 | | |
| STA 7+090.000 | LT | CE | BIT/CONC/AGGR | 7.2 | 6.30 | 8.64 | 6 | | 49.17 | 22.39 | 73.76 | 17.21 | 0.074 | 5.90 | | | | |
| STA 7+177.000 | RT | FE | AGGREGATE | 6.0 | 9.30 | 5.65 | 9 | | | 31.99 | | | | | | | | |
| STA 7+291.800 | LT | CE | AGGREGATE | 4.6 | 6.30 | 23.88 | 6 | | 35.20 | 39.54 | 52.80 | 12.32 | 0.053 | 4.22 | | | | |
| STA 7+328.600 | LT | PE | BITUMINOUS | 4.6 | 6.30 | 23.88 | 6 | 52.21 | | | | 217.55 | | | 26.11 | | | |
| STA 7+355.000 | RT | PE | AGGREGATE | 5.5 | 6.30 | 5.59 | 6 | 14.42 | | 11.07 | | | 60.09 | | | 7.21 | | |
| STA 7+392.900 | LT | PE | AGGREGATE | 4.5 | 6.30 | 18.74 | 6 | 12.45 | | 30.36 | | | 51.87 | | | 6.22 | | |
| STA 7+471.600 | LT | PE | AGGREGATE | 3.7 | 6.30 | 5.59 | 6 | 10.92 | | 7.44 | | | 45.50 | | | 5.46 | | |
| STA 7+478.000 | RT | FE | AGGREGATE | 3.7 | 6.30 | 5.59 | 6 | | | 11.45 | | | | | | | | |
| STA 7+509.000 | LT | PE | CONCRETE | 3.7 | 6.30 | 5.59 | 6 | | | | | | | | | 63.04 | | |
| STA 7+537.000 | RT | CE | CONCRETE | 7.2 | 9.30 | 2.59 | 9 | | | | | | | | | | 122.57 | |
| STA 7+682.000 | RT | PE | BITUMINOUS | 5.3 | 4.30 | 3.55 | 4 | 18.33 | | | | 76.38 | | | 9.17 | | | |

CONTINUED

PLOT DATE = 12/22/2008
FILE NAME = H:\V\20\Contract\94533\SHI_030.ent.-2.V8.dgn

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION

RURAL ENTRANCE SCHEDULE

SCALE NO SCALE DRAWN BY KOJ
DATE DECEMBER 22, 2008 CHECKED BY LWJ

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------|------------------|-----------|
| 781 | LYRS | CRAWFORD | 212 | 31 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

RURAL ENTRANCE SCHEDULE - CONTINUED

| LOCATION | | | | WIDTH | LENGTH | | | RADIUS | AGGREGATE BASE COURSE, TYPE B | HOT-MIX ASPHALT BASE COURSE | AGGREGATE SURFACE COURSE, TYPE B | BITUMINOUS MATERIALS (PRIME COAT) | | AGGREGATE (PRIME COAT) | INCIDENTAL HOT-MIX ASPHALT SURFACING | | PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT | | | | | |
|----------------|-------|------|-------------------|-------|-----------|-----------|-----------|----------|-------------------------------|-----------------------------|----------------------------------|-----------------------------------|------------------|------------------------|--------------------------------------|-------|--|----------|----------|--------|-------|-------|
| STATION | LT/RT | TYPE | EXISTING MATERIAL | W1 | L1 | L2 | METER | 150MM | 200MM | 150MM | PRIOR TO BINDER/BASE COURSE | PRIOR TO SURFACE COURSE | M TON | LITER | LITER | M TON | 50MM | 75MM | 150 MM | 200 MM | | |
| | | | | METER | METER | METER | | M3511010 | M3550500 | M4021010 | | | | | | | M4080500 | M4230150 | M4230200 | | | |
| | | | | | | | | | | | | | INFORMATION ONLY | | | | | | | | | |
| | | | | | | | | | | | | | M TON | SQ. M | M TON | LITER | LITER | M TON | M TON | M TON | SQ. M | SQ. M |
| STA 7+687.147 | LT | ROAD | OIL & CHIP | 6.2 | See Plans | See Plans | 15 | | | | | | | 312.70 | 72.96 | 0.313 | | | | | | |
| STA 7+687.147 | RT | ROAD | OIL & CHIP | 6.2 | See Plans | See Plans | 15 | | | | | | | 314.40 | 73.36 | 0.314 | | | | | | |
| STA 7+755.000 | LT | FE | DIRT | 3.6 | 6.30 | 5.59 | 6 | | | | | | 13.57 | | | | | | | | | |
| STA 7+999.500 | LT | FE | DIRT | 6.0 | 6.30 | 5.59 | 6 | | | | | | 21.77 | | | | | | | | | |
| STA 8+063.000 | RT | FE | BIT/AGGREGATE | 6.0 | 6.30 | 5.59 | 6 | | | | | | 20.73 | | | | | | | | | |
| STA 8+161.200 | RT | FE | AGGREGATE | 3.6 | 6.30 | 5.59 | 6 | | | | | | 13.57 | | | | | | | | | |
| STA 8+296.000 | LT | FE | DIRT | 3.6 | 6.30 | 8.70 | 6 | | | | | | 17.60 | | | | | | | | | |
| STA 8+311.000 | RT | FE | AGGREGATE | 7.2 | 6.30 | 5.59 | 6 | | | | | | 25.87 | | | | | | | | | |
| STA 8+456.000 | LT | FE | DIRT | 3.6 | 6.30 | 5.59 | 6 | | | | | | 13.57 | | | | | | | | | |
| STA 8+456.000 | RT | FE | DIRT | 3.6 | 6.30 | 5.59 | 6 | | | | | | 13.57 | | | | | | | | | |
| STA 8+563.000 | LT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 | 10.73 | | | | | 11.19 | | 44.70 | | | | 5.36 | | | |
| STA 8+607.000 | RT | PE | BITUMINOUS | 4.8 | 6.30 | 5.59 | 6 | 22.72 | | | | | | 94.66 | | | | | 6.53 | | | |
| STA 8+613.500 | LT | CE | AGGREGATE | 7.2 | 6.30 | 8.64 | 6 | | | | | | 73.86 | | 17.23 | 0.074 | 5.91 | | | | | |
| STA 8+634.000 | LT | PE | AGGREGATE | 6.5 | 6.30 | 8.64 | 6 | 16.37 | | 49.24 | 22.39 | | | | 68.19 | | | | 8.18 | | | |
| STA 8+651.000 | RT | PE | BITUMINOUS | 5.7 | 6.30 | 8.64 | 6 | 32.53 | | | | | | | 135.56 | | | | 16.27 | | | |
| STA 8+669.000 | RT | PE | AGGREGATE | 5.4 | 6.30 | 8.64 | 6 | 14.23 | | | 16.79 | | | | 59.28 | | | | 7.11 | | | |
| STA 8+705.600 | LT | PE | BITUMINOUS | 3.9 | 6.30 | 8.64 | 6 | 23.44 | | | | | | | 97.66 | | | | 11.72 | | | |
| STA 8+710.500 | RT | PE | AGGREGATE | 6.5 | 6.30 | 8.64 | 6 | 16.57 | | | 20.21 | | | | 69.03 | | | | 8.28 | | | |
| STA 8+724.500 | RT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 | 10.93 | | | 11.19 | | | | 45.54 | | | | 5.47 | | | |
| STA 8+739.700 | LT | PE | BITUMINOUS | 5.6 | 6.30 | 8.64 | 6 | 32.03 | | | | | | | 133.45 | | | | 16.01 | | | |
| STA 8+749.000 | RT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 | 10.73 | | | 11.19 | | | | 44.70 | | | | 5.36 | | | |
| STA 8+770.500 | LT | PE | BITUMINOUS | 6.0 | 6.30 | 8.64 | 6 | 34.05 | | | | | | | 141.87 | | | | 17.02 | | | |
| STA 8+778.500 | RT | PE | AGGREGATE | 4.0 | 6.30 | 8.64 | 6 | 11.64 | | | 12.44 | | | | 48.48 | | | | 5.82 | | | |
| STA 8+792.000 | LT | PE | BITUMINOUS | 4.3 | 6.30 | 8.64 | 6 | 25.46 | | | | | | | 106.08 | | | | 12.73 | | | |
| STA 8+793.300 | RT | PE | BITUMINOUS | 6.0 | 6.30 | 8.64 | 6 | 34.20 | | | | | | | 142.49 | | | | 17.10 | | | |
| STA 8+841.000 | RT | CE | AGGREGATE | 10.6 | 6.30 | 8.64 | 6 | | | | 67.61 | 32.96 | 101.42 | | 23.66 | 0.101 | 8.11 | | | | | |
| STA 8+843.000 | LT | CE | BITUMINOUS | 10.6 | 6.30 | 8.64 | 6 | | | | 159.14 | | 238.72 | | 55.70 | 0.239 | 19.10 | | | | | |
| STA 8+869.500 | RT | CE | AGGREGATE | 10.6 | 6.30 | 8.64 | 6 | | | | 67.62 | 32.96 | 101.43 | | 23.67 | 0.101 | 8.11 | | | | | |
| STA 8+910.897 | LT | ROAD | OIL & CHIP | 6.0 | See Plans | See Plans | 15 | | | | | | 309.33 | | 72.18 | 0.309 | 24.75 | | | | | |
| STA 8+949.000 | RT | PE | AGGREGATE | 8.6 | 6.30 | 8.64 | 6 | 20.45 | | | 26.74 | | | | 85.20 | | | | 10.22 | | | |
| STA 9+073.000 | LT | PE | BITUMINOUS | 4.0 | 6.30 | 8.64 | 6 | 23.94 | | | | | | | 99.76 | | | | 11.97 | | | |
| STA 9+192.500 | RT | CE | AGGREGATE | 10.6 | 6.30 | 8.64 | 6 | | | | 67.56 | 32.96 | 101.34 | | 23.65 | 0.101 | 8.11 | | | | | |
| STA 9+682.500 | RT | PE | AGGREGATE | 5.3 | 6.30 | 8.64 | 6 | 14.03 | | | | | | | 58.47 | | | | 7.02 | | | |
| STA 9+707.500 | RT | PE | AGGREGATE | 7.4 | 6.30 | 8.64 | 6 | 18.12 | | | 23.01 | | | | 75.48 | | | | 9.06 | | | |
| STA 9+948.600 | LT | PE | OIL & CHIP | 3.6 | 6.30 | 20.83 | 6 | 37.73 | | | | | | | 157.20 | | | | 18.86 | | | |
| STA 9+951.000 | RT | CE | AGGREGATE | 10.6 | 6.30 | 17.78 | 6 | | | | 67.60 | 67.85 | 101.40 | | 23.66 | 0.101 | 8.11 | | | | | |
| STA 10+125.278 | LT | ROAD | OIL & CHIP | 7.0 | See Plans | See Plans | 15 | | | | | | 257.13 | | 90.00 | 0.386 | 30.86 | | | | | |
| STA 10+125.278 | RT | ROAD | OIL & CHIP | 6.7 | See Plans | See Plans | 15 | | | | | | 284.71 | | 99.65 | 0.427 | 34.17 | | | | | |
| STA 10+321.500 | RT | FE | AGGREGATE | 4.0 | 6.30 | 11.69 | 6 | | | | | 23.72 | | | | | | | | | | |
| STA 10+329.000 | LT | CE | AGGREGATE | 10.6 | 6.30 | 8.64 | 6 | | | | 67.60 | 32.96 | 101.40 | | 23.66 | 0.101 | 8.11 | | | | | |
| STA 10+500.000 | LT | CE | CONC/AGG | 10.6 | 6.30 | 8.64 | 6 | | | | | | | | | | | | | 177.38 | | |
| STA 10+559.500 | RT | CE | AGGREGATE | 10.6 | 6.30 | 8.64 | 6 | | | | 67.60 | 32.96 | 101.40 | | 23.66 | 0.101 | 8.11 | | | | | |
| STA 10+705.000 | LT | FE | DIRT | 7.2 | 6.30 | 8.64 | 6 | | | | | | | | 33.77 | | | | | | | |
| STA 10+732.000 | LT | PE | AGGREGATE | 3.9 | 6.30 | 8.64 | 6 & 2.125 | 12.41 | | | | | | | 51.70 | | | | 6.20 | | | |
| STA 10+736.079 | RT | ROAD | OIL & CHIP | 6.0 | 15.30 | 0.35 | 15 | | | | | | 199.48 | | 299.22 | | 69.82 | 0.299 | 23.94 | | | |
| STA 10+740.000 | LT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 2.125 & 6 | 11.79 | | | | | | | 49.11 | | | | 5.89 | | | |
| STA 10+765.500 | LT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 | 10.73 | | | | | | | 44.70 | | | | 5.36 | | | |
| STA 10+784.800 | LT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 & 2.65 | 12.05 | | | | | | | 50.21 | | | | 6.03 | | | |
| STA 10+793.900 | LT | PE | AGGREGATE | 4.0 | 6.30 | 8.64 | 2.65 & 6 | 12.81 | | | | | | | 53.36 | | | | 6.40 | | | |
| STA 10+819.600 | RT | CE | AGGREGATE | 7.2 | 6.30 | 4.95 | 6 | | | | | | 49.25 | | 12.83 | | 73.88 | 17.24 | 0.074 | 5.91 | | |

CONTINUED

PLOT DATE = 12/22/2008
FILE NAME = H:\V\20\Contract\94533\SHT_289.ent-2.V8.dgn

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|-------------------|---------------------------------------|
| NAME | DATE | |
| | | RURAL ENTRANCE SCHEDULE |
| | | |
| | | |
| | | |
| | | |
| | | |
| SCALE | NO SCALE | DRAWN BY KOJ |
| DATE | DECEMBER 22, 2008 | CHECKED BY LWJ |

| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | LYRS | CRAWFORD | 212 | 32 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

RURAL ENTRANCE SCHEDULE - CONTINUED

| LOCATION | | | | WIDTH | LENGTH | | | RADIUS | AGGREGATE BASE COURSE, TYPE B | HOT-MIX ASPHALT BASE COURSE | AGGREGATE SURFACE COURSE, TYPE B | BITUMINOUS MATERIALS (PRIME COAT) | | AGGREGATE (PRIME COAT) | INCIDENTAL HOT-MIX ASPHALT SURFACING | | PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT | |
|----------------|-------|------|-------------------|-------|-----------|-----------|----------|----------|-------------------------------|-----------------------------|----------------------------------|-----------------------------------|--------|------------------------|--------------------------------------|--------|--|--------|
| STATION | LT/RT | TYPE | EXISTING MATERIAL | W1 | L1 | L2 | METER | 150MM | 200MM | 150MM | PRIOR TO BINDER/BASE COURSE | PRIOR TO SURFACE COURSE | M TON | 50MM | 75MM | 150 MM | 200 MM | |
| | | | | METER | METER | METER | | M3511010 | M3550500 | M4021010 | | | | | | | | LITER |
| STA 10+824.000 | LT | PE | DIRT | 7.0 | 6.30 | 8.64 | 6 | 17.34 | | 21.77 | | | | | | | | |
| STA 10+864.000 | RT | CE | AGGREGATE | 7.2 | 6.30 | 8.64 | 6 | | 49.25 | 22.39 | | | 73.88 | 17.24 | 0.074 | 5.91 | | |
| STA 10+915.500 | LT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 | 10.73 | | 11.19 | | | | | | | 5.36 | |
| STA 10+920.500 | RT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 | 10.73 | | 11.19 | | | | | | | 5.37 | |
| STA 11+073.840 | LT | FE | AGGREGATE | 4.0 | See Plans | See Plans | 6 & 12 | | | 27.87 | | | | | | | | |
| STA 11+073.840 | RT | FE | AGGREGATE | 3.9 | See Plans | See Plans | 12 & 6 | | | 24.39 | | | | | | | | |
| STA 11+146.500 | LT | PE | DIRT | 3.6 | 6.30 | 8.64 | 6 | 10.73 | | 11.19 | | | | | | | 5.36 | |
| STA 11+246.000 | RT | FE | DIRT | 3.6 | 6.30 | 8.64 | 6 | | | 17.52 | | | | | | | | |
| STA 11+306.500 | LT | PE | AGGREGATE | 4.4 | 6.30 | 8.64 | 6 | 12.18 | | 13.68 | | | | | | | 6.09 | |
| STA 11+346.500 | RT | CE | OIL & CHIP | 7.2 | 6.30 | 11.69 | 6 | | 133.37 | | | | 200.06 | 46.68 | 0.200 | 16.00 | | |
| STA 11+350.000 | LT | CE | AGGREGATE | 7.2 | 6.30 | 8.64 | 6 | | 49.24 | 22.39 | | | 73.86 | 17.23 | 0.074 | 5.91 | | |
| STA 11+571.600 | RT | CE | CONCRETE | 10.6 | 6.30 | 11.69 | 6 | | | | | | | | | | | 209.69 |
| STA 11+698.000 | RT | CE | CONCRETE | 10.6 | 6.30 | 15.93 | 6 | | | | | | | | | | | 254.67 |
| STA 11+713.000 | LT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 | 10.73 | | 11.19 | | | | | | | 5.36 | |
| STA 11+756.500 | LT | PE | BITUMINOUS | 3.6 | 6.30 | 14.73 | 6 | 29.82 | | | | | | | | | 14.91 | |
| STA 11+880.000 | RT | CE | AGGREGATE | 10.6 | 9.30 | 8.69 | 9 | | 115.61 | 33.14 | | | 173.41 | 40.46 | 0.173 | 13.87 | | |
| STA 11+914.500 | RT | CE | AGGREGATE | 7.2 | 9.30 | 8.69 | 9 | | 87.19 | 22.51 | | | 130.78 | 30.51 | 0.131 | 10.46 | | |
| STA 11+975.500 | RT | CE | AGGREGATE | 10.6 | 12.30 | 5.69 | 12 | | 171.52 | 21.69 | | | 257.29 | 60.03 | 0.257 | 20.58 | | |
| STA 12+164.026 | LT | ROAD | OIL & CHIP | 6.7 | See Plans | See Plans | 15 | | 263.68 | | | | 395.52 | 92.29 | 0.396 | 31.64 | | |
| STA 12+164.026 | RT | ROAD | AGGREGATE | 6.7 | See Plans | See Plans | 12 | | 219.07 | | | | 328.61 | 76.68 | 0.329 | 26.29 | | |
| STA 12+353.000 | LT | FE | DIRT | 3.6 | 6.30 | 6.60 | 6 | | | 14.94 | | | | | | | | |
| STA 12+353.000 | RT | FE | DIRT | 3.6 | 6.30 | 7.10 | 6 | | | 15.48 | | | | | | | | |
| STA 12+647.500 | LT | FE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 | | | 17.52 | | | | | | | | |
| STA 12+880.500 | LT | CE | AGGREGATE | 7.2 | 9.30 | 5.64 | 9 | | 87.05 | 14.61 | | | 130.57 | 30.47 | 0.131 | 10.45 | | |
| STA 12+991.000 | LT | PE | AGGREGATE | 4.0 | 6.30 | 8.64 | 6 | 11.51 | | 12.44 | | | | | | | 5.75 | |
| STA 13+058.500 | LT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 | 10.73 | | 11.19 | | | | | | | 5.36 | |
| STA 13+084.000 | RT | CE | AGGREGATE | 7.2 | 6.30 | 8.64 | 6 | | 49.24 | 22.39 | | | 73.86 | 17.23 | 0.074 | 5.91 | | |
| STA 13+097.000 | LT | PE | CONCRETE | 3.6 | 6.30 | 8.64 | 6 | | | | | | | | | | | 72.82 |
| STA 13+127.500 | LT | PE | AGGREGATE | 5.2 | 6.30 | 8.64 | 6 & 4.3 | 15.11 | | 16.17 | | | | | | | | 7.56 |
| STA 13+140.500 | LT | PE | BITUMINOUS | 3.6 | 6.30 | 8.64 | 4.3 & 6 | 23.20 | | | | | | | | | | 11.60 |
| STA 13+163.500 | LT | PE | BITUMINOUS | 3.6 | 6.30 | 8.64 | 6 | 21.92 | | | | | | | | | | 10.96 |
| STA 13+205.500 | LT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 | 10.72 | | 11.19 | | | | | | | | 5.36 |
| STA 13+246.000 | LT | PE | AGGREGATE | 6.0 | 6.30 | 8.64 | 6 | 15.39 | | 18.66 | | | | | | | | 7.69 |
| STA 13+290.000 | LT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 | 10.72 | | 11.20 | | | | | | | | 5.36 |
| STA 13+328.500 | LT | CE | AGGREGATE | 7.2 | 6.30 | 8.64 | 6 | | 49.21 | 22.39 | | | 73.82 | 17.22 | 0.074 | 5.91 | | |
| STA 13+391.693 | LT | ROAD | BITUMINOUS | 6.7 | See Plans | See Plans | 15 | | 253.59 | | | | 380.39 | 88.76 | 0.380 | 30.43 | | |
| STA 13+391.693 | RT | ROAD | OIL & CHIP | 6.2 | See Plans | See Plans | 15 | | 240.63 | | | | 360.94 | 84.22 | 0.361 | 28.88 | | |
| STA 13+480.000 | LT | FE | DIRT | 6.0 | 6.30 | 8.64 | 6 | | | 28.37 | | | | | | | | |
| STA 13+490.500 | RT | PE | AGGREGATE | 3.9 | 6.30 | 8.64 | 6 | 11.30 | | 12.13 | | | | | | | | 5.65 |
| STA 13+527.500 | RT | PE | AGGREGATE | 3.9 | 6.30 | 8.64 | 6 | 11.28 | | 12.13 | | | | | | | | 5.64 |
| STA 13+602.700 | RT | PE | AGGREGATE | 3.9 | 6.30 | 8.64 | 6 & 4.25 | 10.74 | | 12.11 | | | | | | | | 5.37 |
| STA 13+615.000 | RT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 4.25 & 6 | 10.16 | | 11.19 | | | | | | | | 10.08 |
| STA 13+672.000 | RT | CE | AGGREGATE | 7.2 | 6.30 | 8.64 | 6 | | 49.22 | 22.39 | | | 73.83 | 17.23 | 0.074 | 5.91 | | |
| STA 13+750.400 | RT | PE | AGGREGATE | 4.3 | 6.30 | 8.64 | 6 | 12.09 | | 13.37 | | | | | | | | 6.04 |
| STA 13+802.800 | RT | PE | BITUMINOUS | 3.6 | 6.30 | 8.64 | 6 | 21.92 | | | | | | | | | | 10.96 |
| STA 13+803.800 | LT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 | 10.71 | | 11.19 | | | | | | | | 5.35 |
| STA 13+981.300 | LT | FE | DIRT | 3.6 | 6.30 | 8.64 | 6 | | | 17.52 | | | | | | | | |
| STA 14+077.100 | RT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 | 10.73 | | 11.19 | | | | | | | | 5.36 |
| STA 14+217.800 | RT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 | 10.73 | | 11.19 | | | | | | | | 5.36 |
| STA 14+221.800 | LT | CE | AGGREGATE | 7.2 | 6.30 | 8.64 | 6 | | 49.10 | 22.39 | | | 73.65 | 17.19 | 0.074 | 5.89 | | |

CONTINUED

PLOT DATE = 12/22/2008
FILE NAME = P:\1422\Cont+est_94533\SHI_838.ent.v2.18.dgn

H. M. & G. NO. 4420

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|---------------------------------------|
| NAME | DATE | |
| | | RURAL ENTRANCE SCHEDULE |
| | | |
| | | |
| | | |
| | | |
| | | |

SCALE NO SCALE DRAWN BY KOJ
DATE DECEMBER 22, 2008 CHECKED BY LWJ

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------|------------------|-----------|
| 781 | LYRS | CRAWFORD | 212 | 33 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

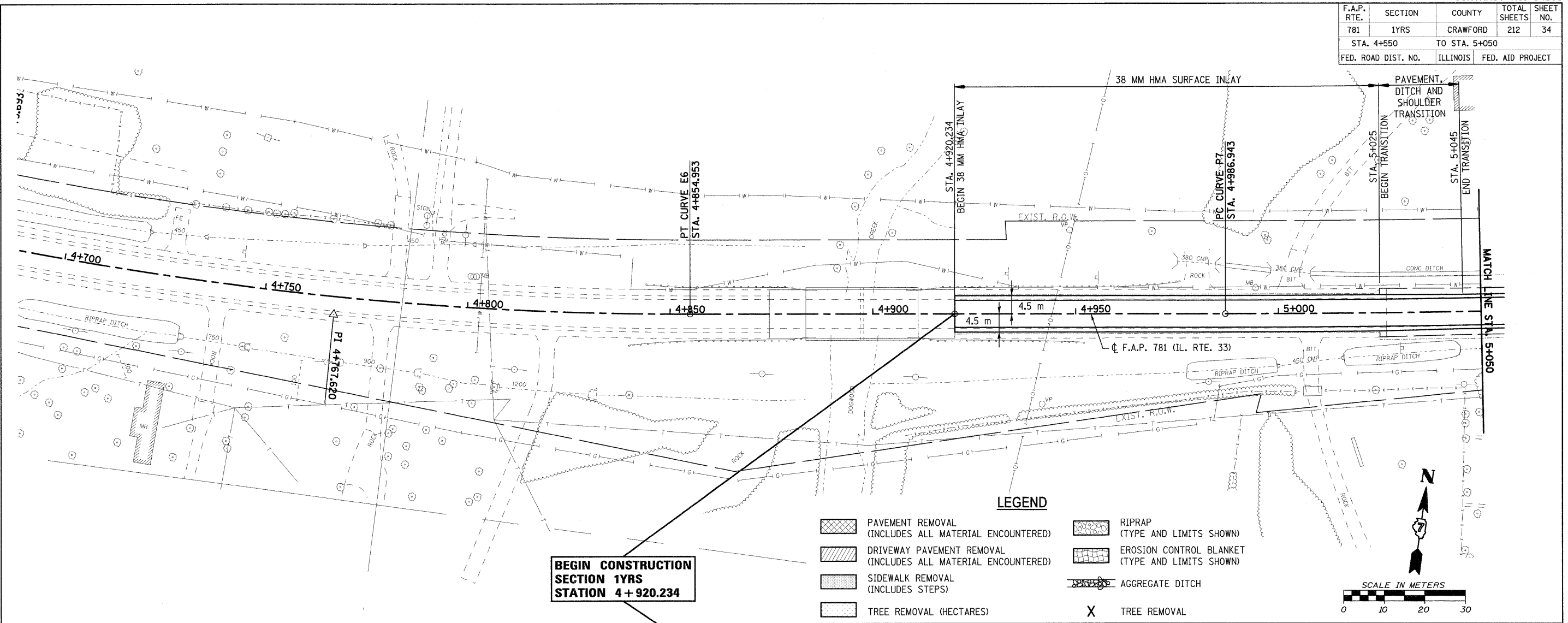
RURAL ENTRANCE SCHEDULE - CONTINUED

| LOCATION | | | | WIDTH | LENGTH | | | RADIUS | AGGREGATE BASE COURSE, TYPE B | HOT-MIX ASPHALT BASE COURSE | AGGREGATE SURFACE COURSE, TYPE B | BITUMINOUS MATERIALS (PRIME COAT) | | AGGREGATE (PRIME COAT) | INCIDENTAL HOT-MIX ASPHALT SURFACING | | PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT | | | |
|----------------------------------|-------|------|-------------------|-----------|--------|-------|---------|----------|-------------------------------|-----------------------------|----------------------------------|-----------------------------------|-------------------------|------------------------|--------------------------------------|--------|--|--------|------------------|-------|
| | | | | | | | | | | | | PRIOR TO BINDER/BASE COURSE | PRIOR TO SURFACE COURSE | | 50MM | 75MM | 150 MM | 200 MM | | |
| | | | | | | | | | | | | | | | | | | | INFORMATION ONLY | M TON |
| STATION | LT/RT | TYPE | EXISTING MATERIAL | W1 | L1 | L2 | METER | M3511010 | M3550500 | M4021010 | M TON | LITER | LITER | M TON | M TON | M TON | SQ. M | SQ. M | | |
| STA 14+234.800 | RT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 | | | | 10.73 | | 44.70 | | | 5.36 | | | | |
| STA 14+242.400 | LT | CE | BITUMINOUS | 10.0 | 6.30 | 8.64 | 6 | | | | | 225.89 | 52.71 | 0.226 | 18.07 | | | | | |
| STA 14+308.900 | LT | CE | BITUMINOUS | 10.6 | 6.30 | 8.64 | 6 | | | | 159.19 | 238.78 | 55.72 | 0.239 | 19.10 | | | | | |
| STA 14+447.300 | LT | CE | BITUMINOUS | 7.2 | 6.30 | 8.64 | 6 & 3.4 | | | | 109.52 | 164.28 | 38.33 | 0.164 | 13.14 | | | | | |
| STA 14+447.300 | RT | CE | CONC/AGG | 10.6 | 6.30 | 8.64 | 6 | | | | 32.96 | | | | | | | 85.83 | | |
| STA 14+461.300 | LT | CE | AGGREGATE | 7.2 | 6.30 | 8.64 | 3.4 & 6 | | 47.31 | 22.18 | | 70.97 | 16.56 | 0.071 | 5.68 | | | | | |
| STA 14+553.800 | LT | PE | AGGREGATE | 5.6 | 6.30 | 8.64 | 6 | | 14.62 | | | | 60.90 | | | 7.31 | | | | |
| STA 14+600.800 | LT | PE | BITUMINOUS | 5.6 | 6.30 | 8.64 | 6 | | 32.03 | | | | 133.45 | | | 16.01 | | | | |
| STA 14+629.900 | RT | PE | BITUMINOUS | 5.0 | 6.30 | 8.64 | 6 | | | | | | | | | | 93.74 | | | |
| STA 14+630.300 | LT | CE | AGGREGATE | 7.2 | 6.30 | 8.64 | 6 | | | 49.24 | 22.39 | 73.86 | 17.23 | 0.074 | 5.91 | | | | | |
| STA 14+659.300 | LT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 | | 10.73 | | | | 44.70 | | | 5.36 | | | | |
| STA 14+664.800 | RT | PE | BIT/CONC | 5.6 | 6.30 | 8.64 | 6 | | | | | | | | | | 102.70 | | | |
| STA 14+696.800 | RT | PE | AGGREGATE | 5.6 | 6.30 | 8.64 | 6 | | 14.62 | | 17.41 | | 60.90 | | | 7.31 | | | | |
| STA 14+711.600 | LT | CE | CONCRETE | 7.2 | 6.30 | 8.64 | 6 | | | | | | | | | | | 126.60 | | |
| STA 14+725.800 | RT | PE | BITUMINOUS | 3.6 | 6.30 | 8.64 | 6 | | 21.92 | | | | 91.34 | | | 10.96 | | | | |
| STA 14+756.800 | LT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 & 5 | | | | 11.19 | | 44.98 | | | 5.40 | | | | |
| STA 14+763.300 | RT | PE | BITUMINOUS | 5.0 | 6.30 | 8.64 | 6 | | 29.00 | | | | 120.82 | | | 14.50 | | | | |
| STA 14+770.800 | LT | PE | AGGREGATE | 4.2 | 6.30 | 8.64 | 5 & 6 | | 11.96 | | 13.06 | | 49.84 | | | 5.98 | | | | |
| STA 14+803.300 | LT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 | | 10.73 | | | | 44.70 | | | 5.36 | | | | |
| STA 14+861.800 | RT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 | | 10.73 | | | | 44.70 | | | 5.36 | | | | |
| STA 14+886.300 | RT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 | | 10.73 | | | | 44.70 | | | 5.36 | | | | |
| STA 14+926.300 | LT | PE | BITUMINOUS | 3.6 | 6.30 | 8.64 | 6 | | 21.92 | | | | 91.34 | | | 10.96 | | | | |
| STA 14+926.300 | RT | FE | DIRT | 3.6 | 6.30 | 8.64 | 6 | | | | 17.52 | | | | | | | | | |
| STA 14+981.900 | LT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 | | 10.73 | | | | 44.70 | | | 5.36 | | | | |
| STA 15+062.400 | LT | ROAD | OIL & CHIP | See Plans | 15.30 | 6.35 | 15 | | 256.21 | | | 384.31 | 89.67 | 0.384 | 30.74 | | | | | |
| STA 15+062.400 | RT | ROAD | BITUMINOUS | See Plans | 15.30 | 6.35 | 15 | | 258.07 | | | 387.11 | 90.32 | 0.387 | 30.97 | | | | | |
| STA 15+232.300 | LT | CE | AGGREGATE | 10.6 | 6.30 | 8.64 | 6 | | 67.60 | 32.96 | | 101.40 | 23.66 | 0.101 | 8.11 | | | | | |
| STA 15+321.000 | LT | CE | CONC/AGG | 10.6 | 6.30 | 8.64 | 6 | | | | | | | | | | | 177.38 | | |
| STA 15+353.800 | LT | CE | AGGREGATE | 4.3 | 6.30 | 8.64 | 6 | | 33.58 | 13.37 | | 50.37 | 11.75 | 0.050 | 4.03 | | | | | |
| STA 15+450.800 | RT | PE | AGGREGATE | 7.2 | 6.30 | 4.07 | 6 | | 17.73 | | 10.54 | | 73.86 | | | 8.86 | | | | |
| STA 15+454.800 | LT | CE | CONC/AGG | 10.6 | 6.30 | 8.64 | 6 | | | | 32.96 | | | | | | | 85.83 | | |
| STA 15+509.800 | LT | CE | AGGREGATE | 10.6 | 6.30 | 8.64 | 6 | | | | 32.96 | | | | | | | 85.83 | | |
| STA 15+511.800 | RT | PE | AGGREGATE | 4.0 | 6.30 | 8.64 | 6 | | 11.51 | | 12.44 | | 47.94 | | | 5.75 | | | | |
| SUBTOTAL | | | | | | | | 1,416.18 | 6,769.92 | 2,204.29 | 10,154.88 | 8,270.22 | 10,155 | 812.39 | 704.12 | 332.30 | 1,470.86 | | | |
| URBAN | | | | | | | | | | | | | | | | | | | | |
| STA 15+595.800 | LT | PE | AGGREGATE | 7.2 | 6.30 | 8.64 | 6 | | 17.73 | | 22.39 | | 73.86 | | | 8.86 | | | | |
| STA 15+660.800 | LT | CE | BITUMINOUS | 7.2 | 6.30 | 5.59 | 6 | | | 89.49 | | 134.23 | 31.32 | 0.134 | 10.74 | | | | | |
| STA 15+661.300 | RT | ROAD | OIL & CHIP | 5.8 | 9.30 | 2.60 | 9 | | | 109.19 | | 163.78 | 38.21 | 0.164 | 13.10 | | | | | |
| STA 15+723.100 | RT | PE | BITUMINOUS | 6.0 | 6.30 | 8.64 | 6 | | 34.05 | | | | 141.87 | | | 17.02 | | | | |
| STA 15+773.300 | RT | ROAD | OIL & CHIP | 5.9 | 9.30 | 2.35 | 9 | | | 114.80 | | 172.20 | 40.18 | 0.172 | 13.78 | | | | | |
| STA 15+870.300 | RT | PE | AGGREGATE | 3.6 | 6.30 | 8.64 | 6 | | 10.73 | | 11.19 | | 44.70 | | | 5.36 | | | | |
| STA 15+871.400 | LT | ROAD | AGGREGATE | 3.6 | 9.30 | 2.59 | 9 | | 20.45 | | 3.36 | | 85.21 | | | 10.22 | | | | |
| STA 15+890.000 | LT | PE | AGGREGATE | 3.6 | 6.30 | 22.41 | 6 | | 10.73 | | 29.04 | | 44.70 | | | 5.36 | | | | |
| STA 15+916.600 | RT | PE | BITUMINOUS | 3.6 | 6.30 | 8.64 | 6 | | 21.92 | | | | 91.34 | | | 10.96 | | | | |
| STA 15+931.300 | RT | PE | BITUMINOUS | 3.6 | 6.30 | 8.64 | 6 | | 21.92 | | | | 91.34 | | | 10.96 | | | | |
| STA 15+960.300 | LT | PE | BIT/AGGREGATE | 10.3 | 6.30 | 8.64 | 6 | | 55.78 | | | | 232.41 | | | 27.89 | | | | |
| SUBTOTAL | | | | | | | | 193.31 | 313.47 | 65.98 | 470.21 | 915.16 | 0,470 | 37.62 | 96.65 | 0.00 | 0.00 | | | |
| TOTALS | | | | | | | | 1,609.49 | 7,083.39 | 2,270.27 | 10,625.09 | 9,185.38 | 10.63 | 850.01 | 800.77 | 332.30 | 1,470.86 | | | |
| TOTALS ROUNDED FOR SUMMARY SHEET | | | | | | | | 1,610 | 7,084 | 2,271 | 19,811 | 11 | 1,651 | 333 | 1,471 | | | | | |

FILE NAME = H:\V420\Cont+act 94533\SHT_830.ent-2_V8.dgn

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION RURAL ENTRANCE SCHEDULE SCALE NO SCALE DRAWN BY KOJ DATE DECEMBER 22, 2008 CHECKED BY LWJ |
|-----------|------|---|
| NAME | DATE | |
| | | |
| | | |
| | | |
| | | |
| | | |

| | | | | |
|---------------------|----------|------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 34 |
| STA. 4+550 | | TO STA. 5+050 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



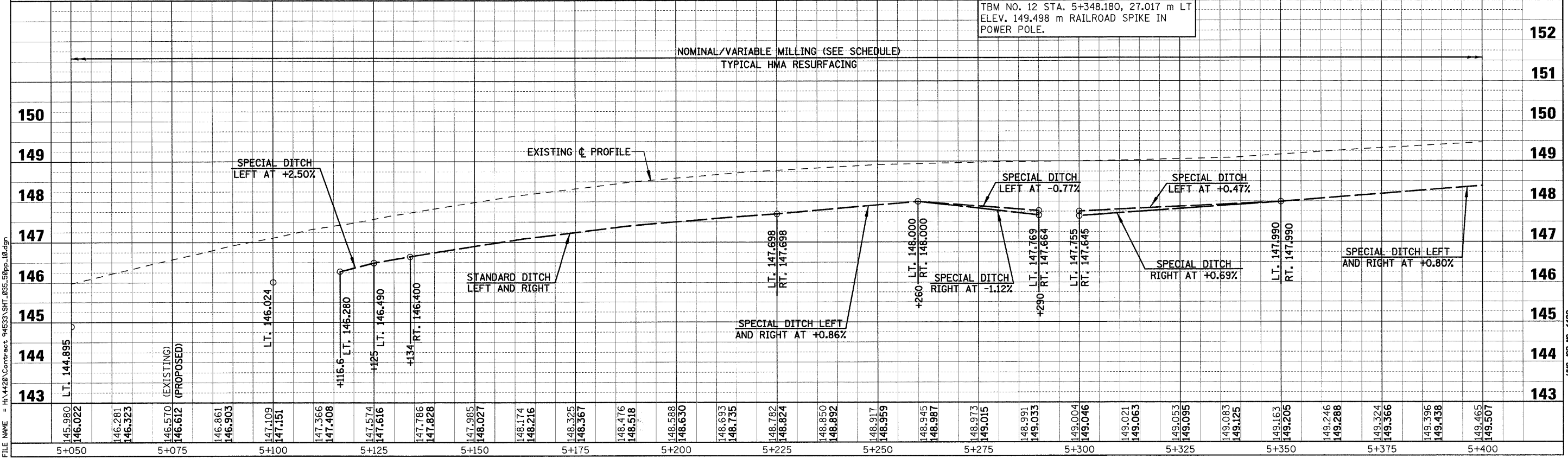
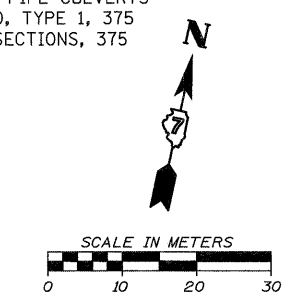
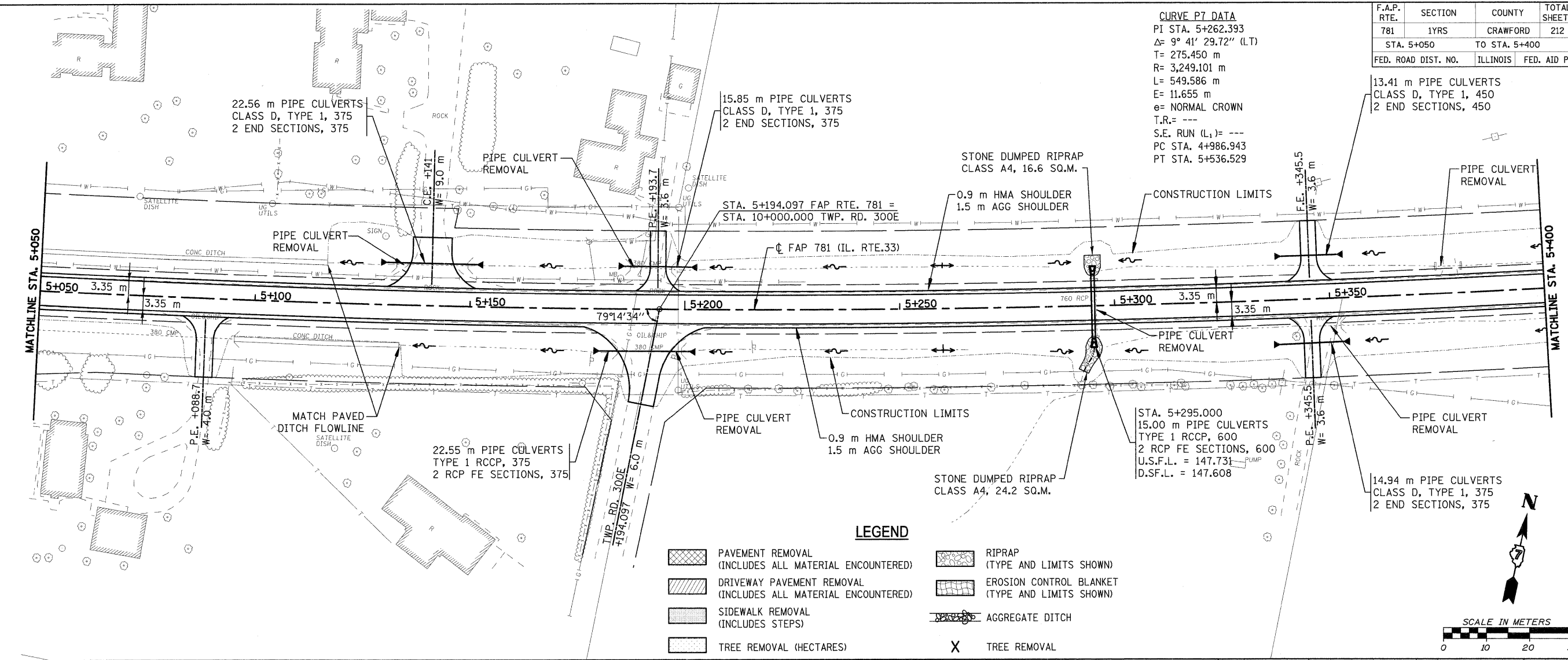
PLOT DATE = 12/22/2008
 FILE NAME = P:\1422\Contract\94533\SH1_234_50pp_09.dgn

Sheet: 09
 Angle: 356.4734
 Chain: P_IL33

HMG JOB NO. 4420

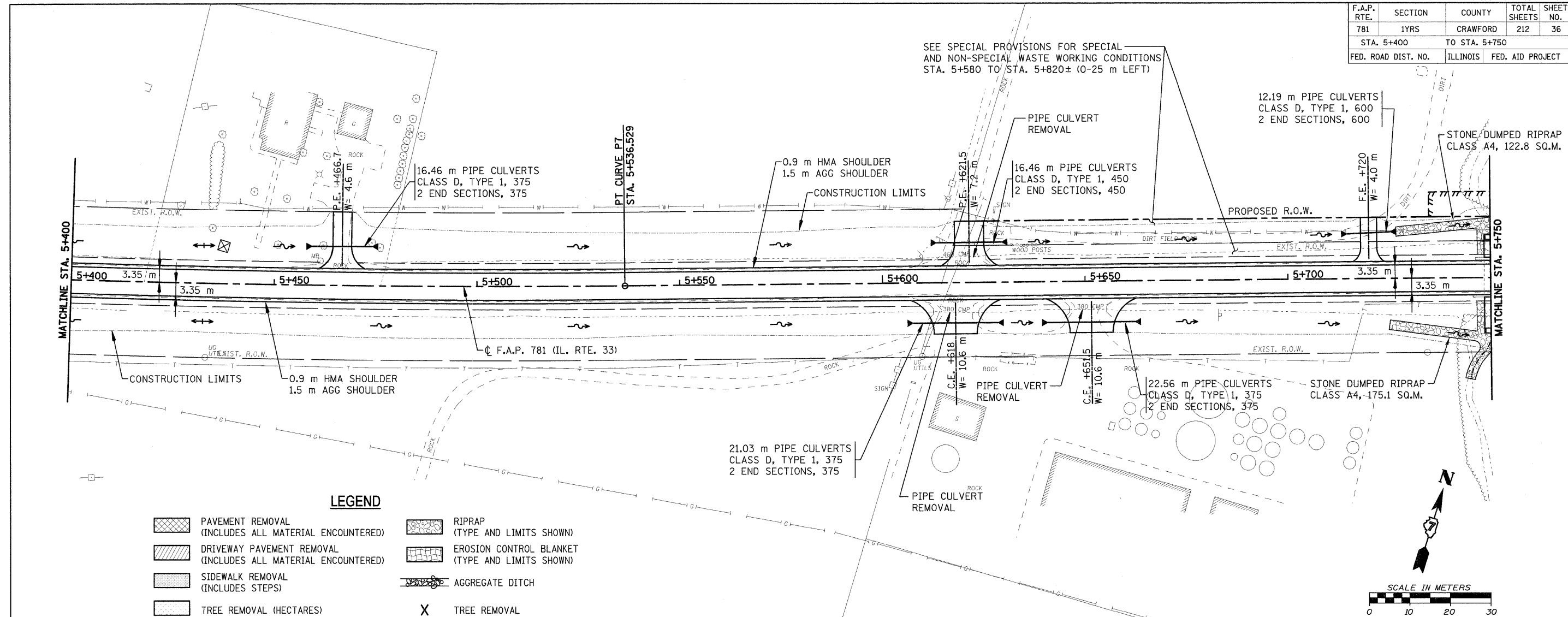
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|------------------|--------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 35 |
| STA. 5+050 | | TO STA. 5+400 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

CURVE P7 DATA
 PI STA. 5+262.393
 $\Delta = 9^\circ 41' 29.72''$ (LT)
 T = 275.450 m
 R = 3,249.101 m
 L = 549.586 m
 E = 11.655 m
 $\phi =$ NORMAL CROWN
 T.R. = ---
 S.E. RUN (L₁) = ---
 PC STA. 4+986.943
 PT STA. 5+536.529



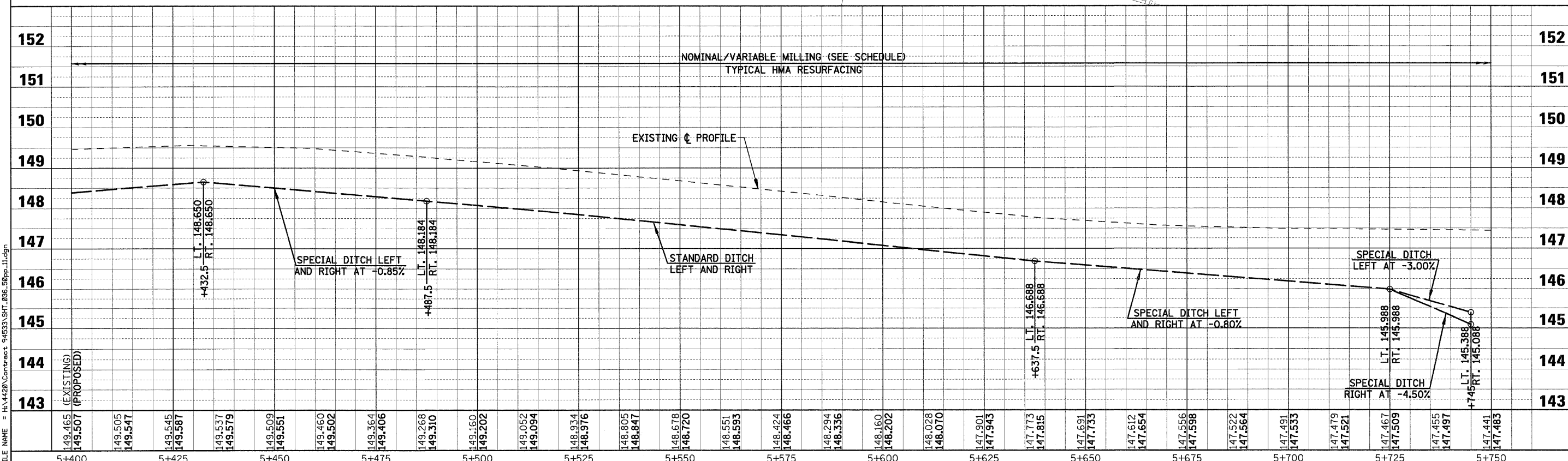
PLOT DATE = 12/22/2008
 FILE NAME = P:\4428\Contract\94533\SHT_035.dwg

| | | | | |
|---------------------|----------|------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 36 |
| STA. 5+400 | | TO STA. 5+750 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



LEGEND

| | | | |
|--|--|--|--|
| | PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED) | | RIPRAP (TYPE AND LIMITS SHOWN) |
| | DRIVEWAY PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED) | | EROSION CONTROL BLANKET (TYPE AND LIMITS SHOWN) |
| | SIDWALK REMOVAL (INCLUDES STEPS) | | AGGREGATE DITCH |
| | TREE REMOVAL (HECTARES) | | TREE REMOVAL |

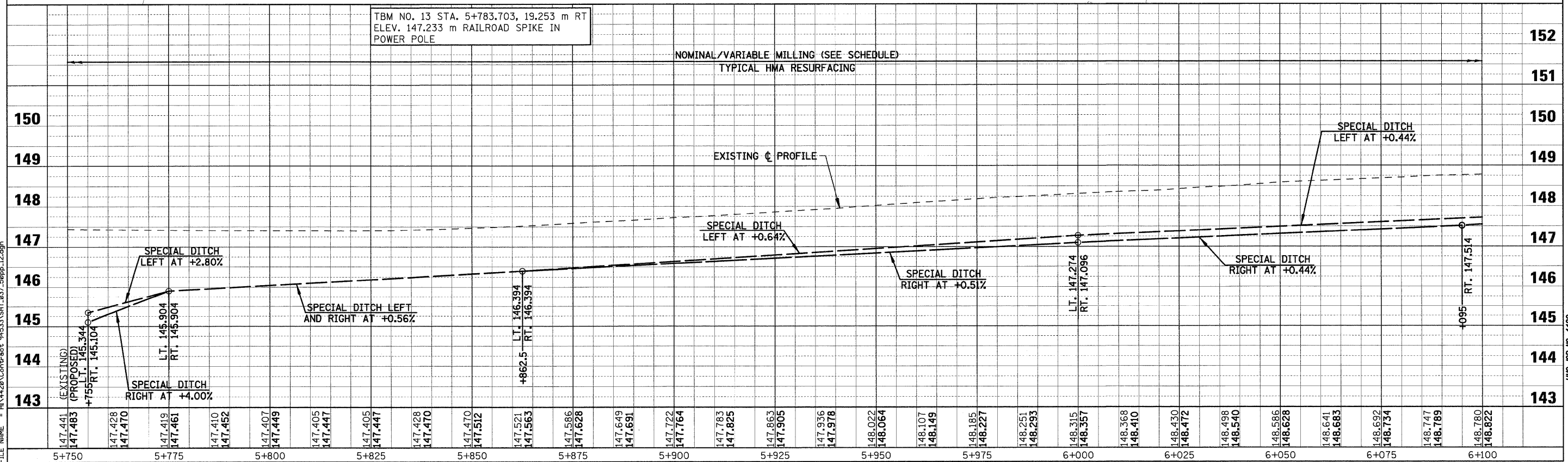
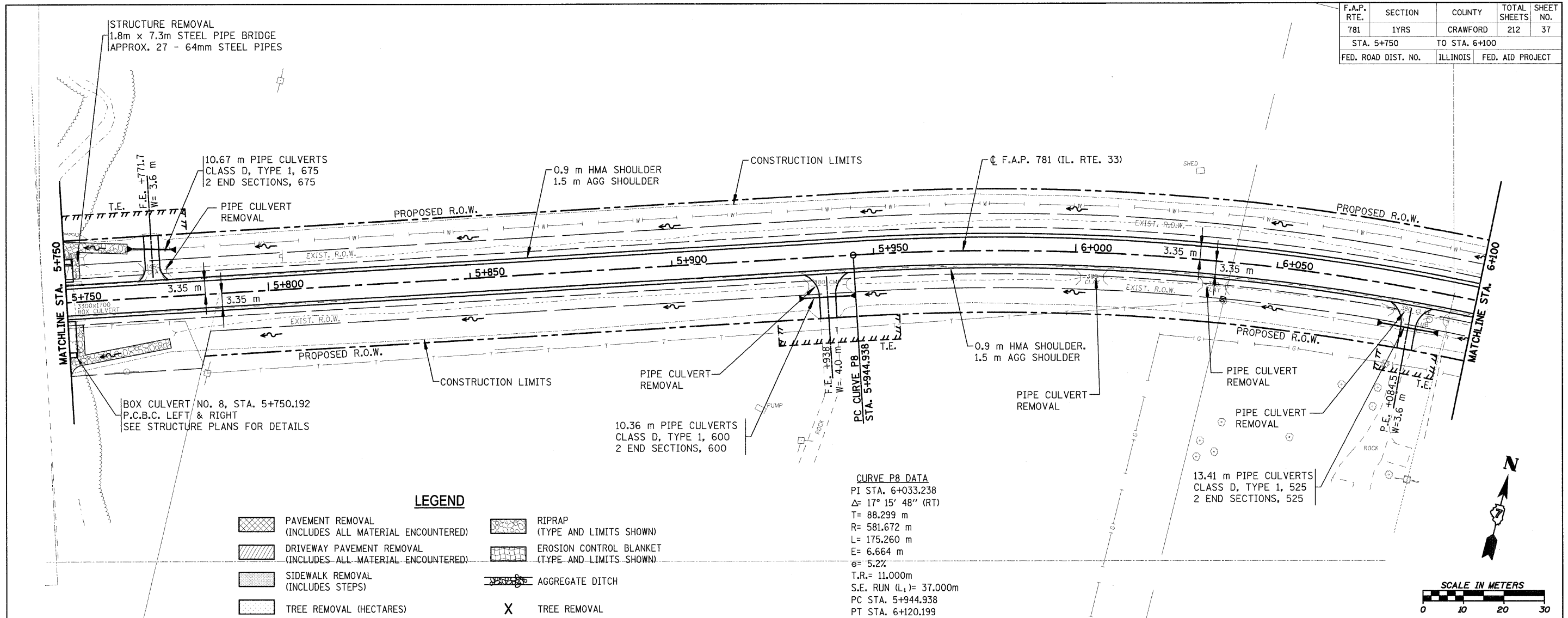


PLOT DATE = 12/22/2008
 FILE NAME = P:\V\20\Contract\94533\SH1_206_50pp.ll.dgn

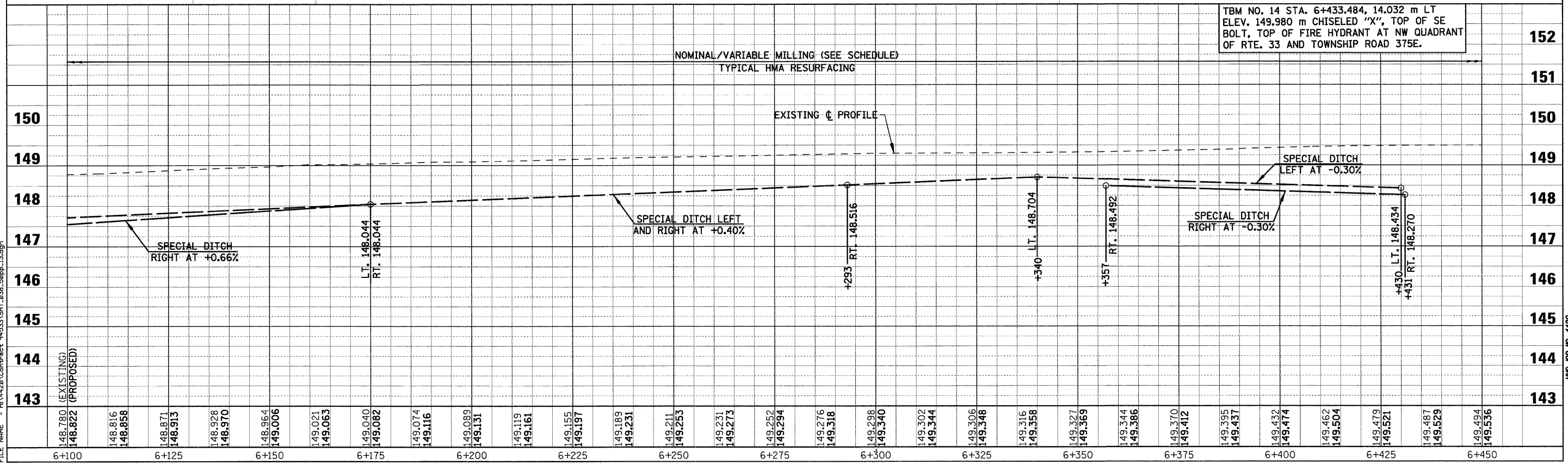
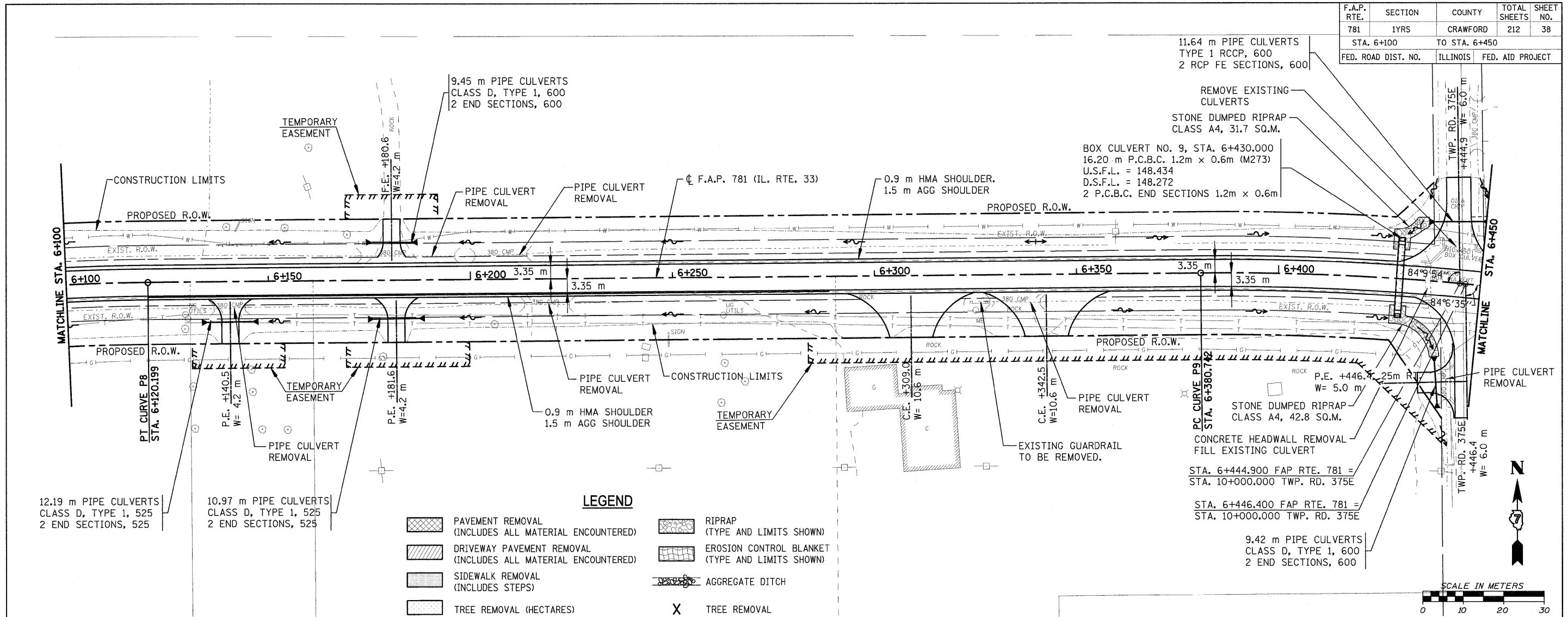
Sheet: 11
 Angle: 17.2144
 Chain: P_IL33

HMG JOB NO. 4420

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|------------------|--------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 37 |
| STA. 5+750 | | TO STA. 6+100 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------------|---------|---------------------|--------------|------------------|
| 781 | 1YRS | CRAWFORD | 212 | 38 |
| STA. 6+100 TO STA. 6+450 | | FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT |

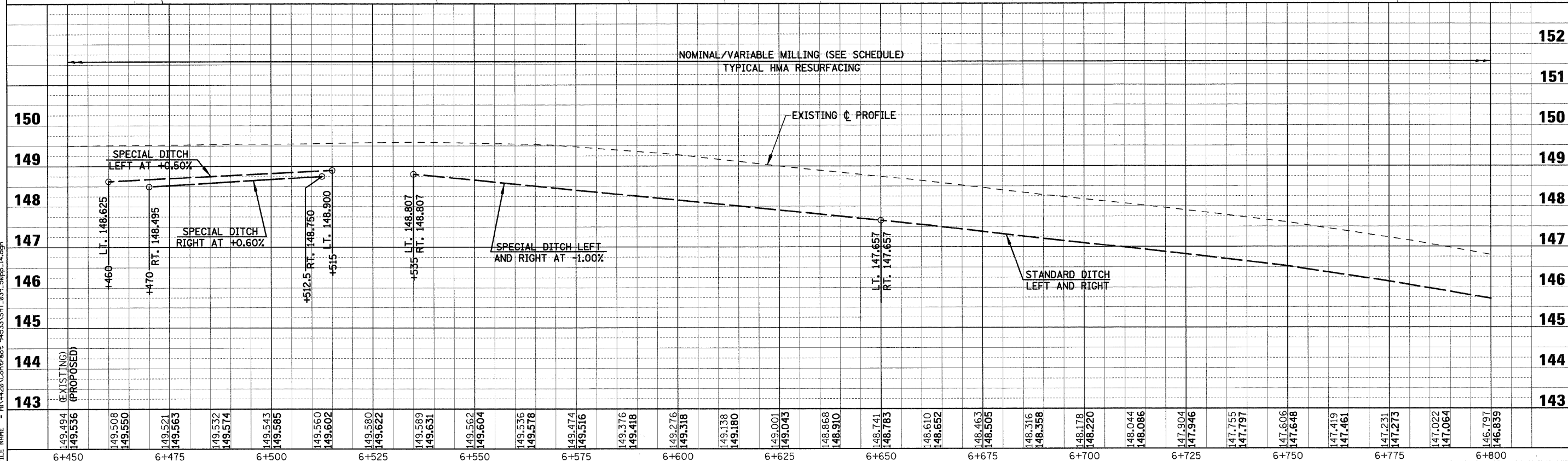
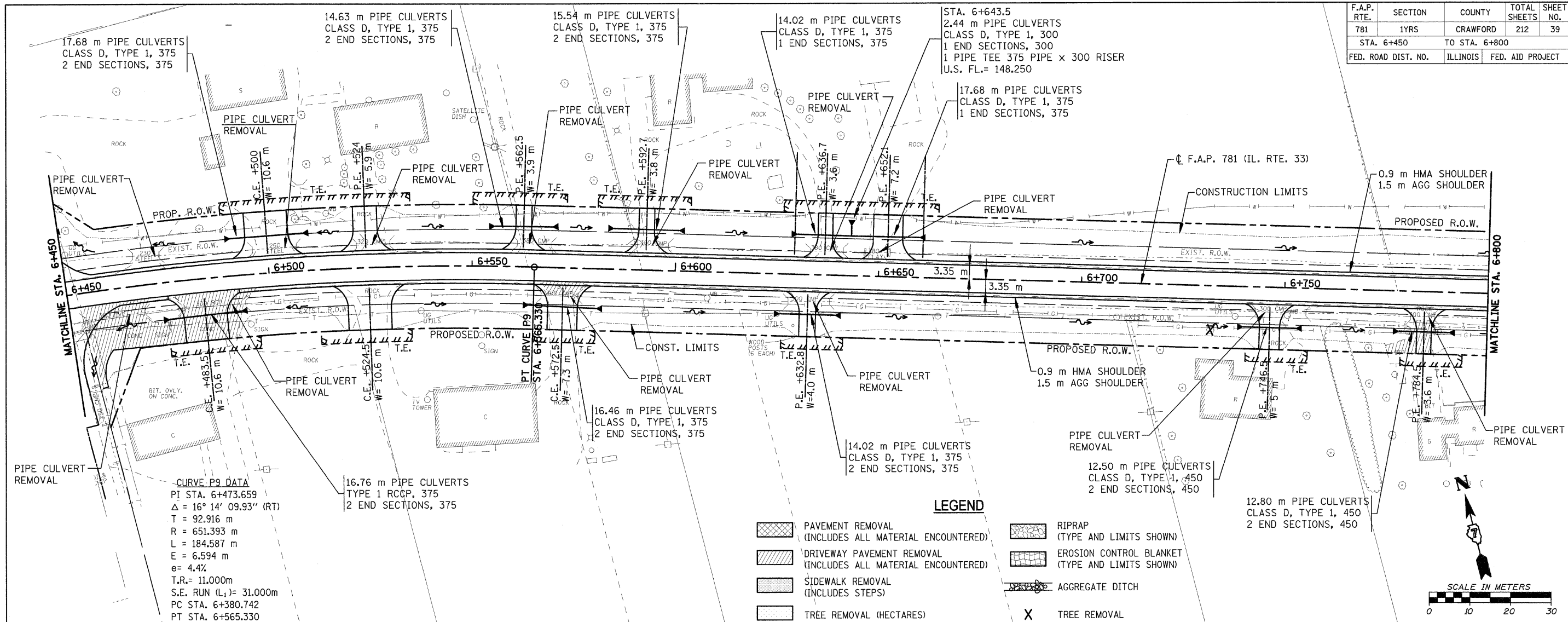


PLOT DATE = 12/22/2006
 FILE NAME = H:\V420\Contract\94533\SH1_038_58pp_13.dgn

Sheet: 13
 Angle: 359.9948
 Chain: P_IL33

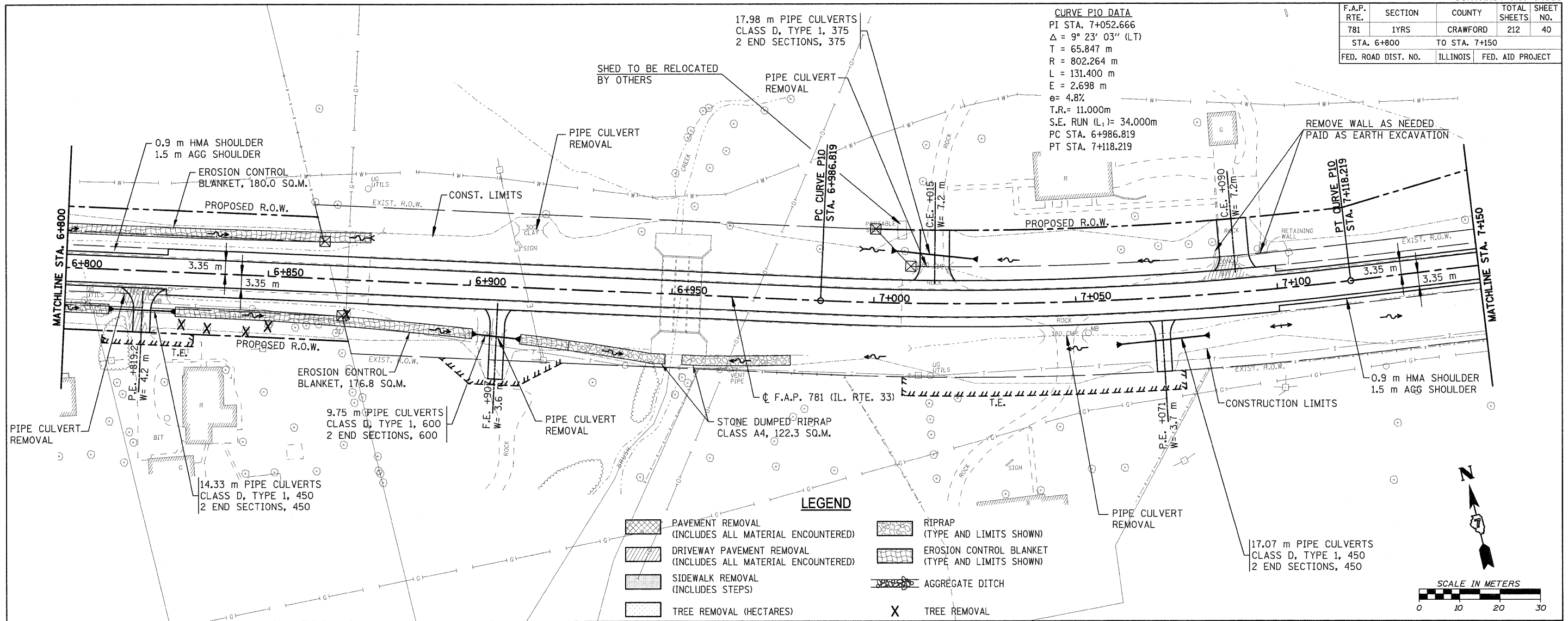
HMG JOB NO. 4420

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------------|----------|------------------|--------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 39 |
| STA. 6+450 TO STA. 6+800 | | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



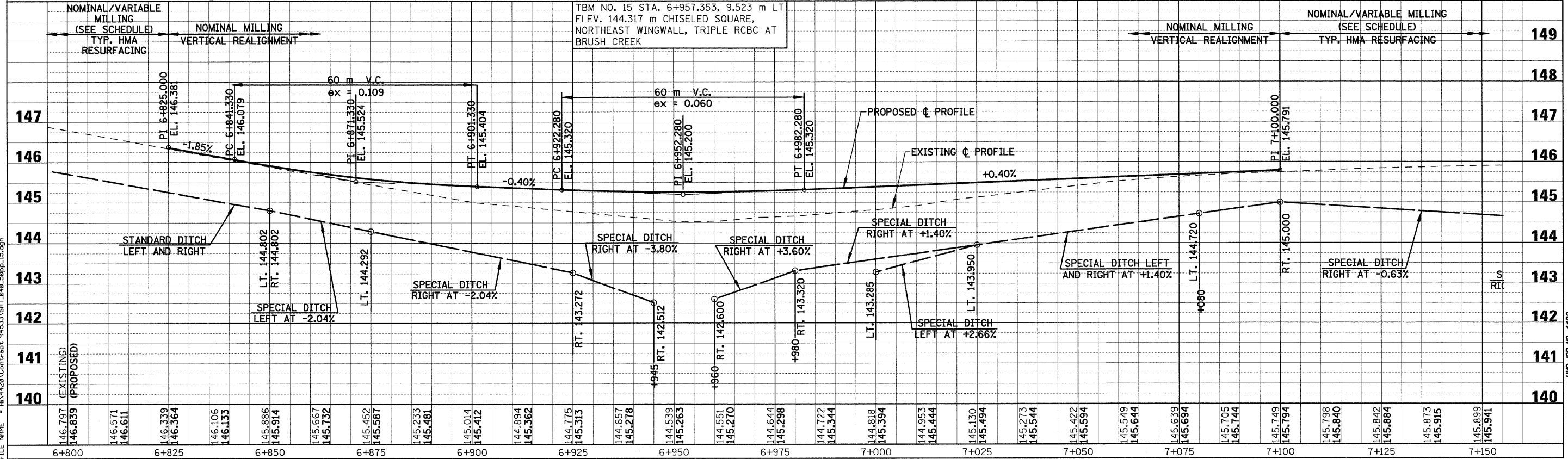
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|---------------|--------------|------------------|
| 781 | LYRS | CRAWFORD | 212 | 40 |
| STA. 6+800 | | TO STA. 7+150 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | | FED. AID PROJECT |

CURVE P10 DATA
 PI STA. 7+052.666
 $\Delta = 9^\circ 23' 03''$ (LT)
 T = 65.847 m
 R = 802.264 m
 L = 131.400 m
 E = 2.698 m
 e = 4.8%
 T.R. = 11.000m
 S.E. RUN (L) = 34.000m
 PC STA. 6+986.819
 PT STA. 7+118.219



LEGEND

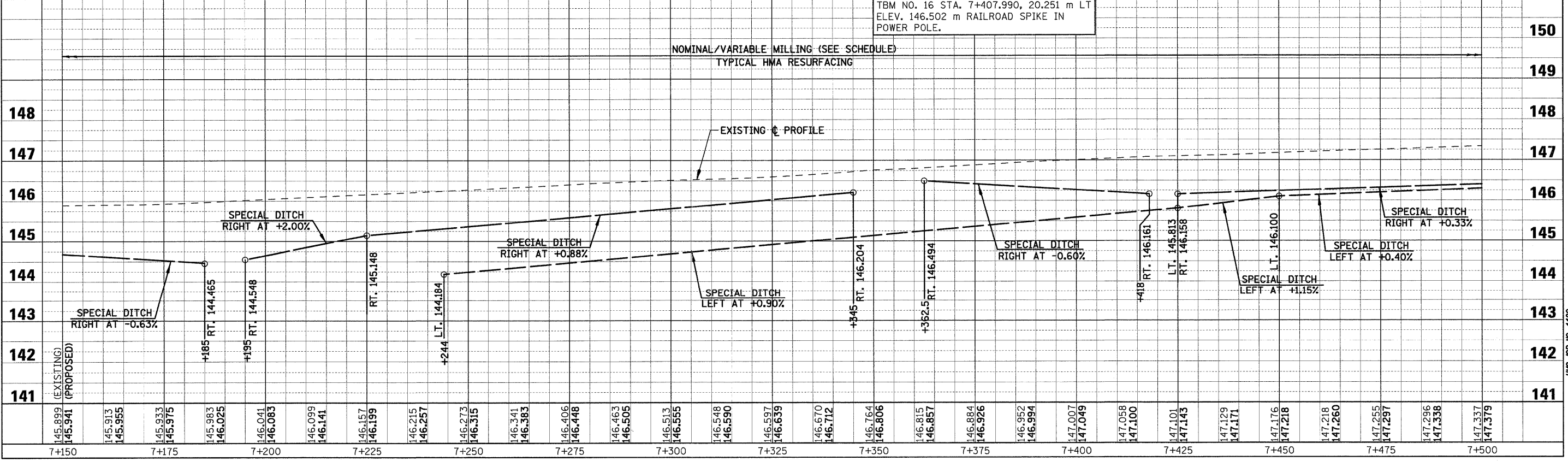
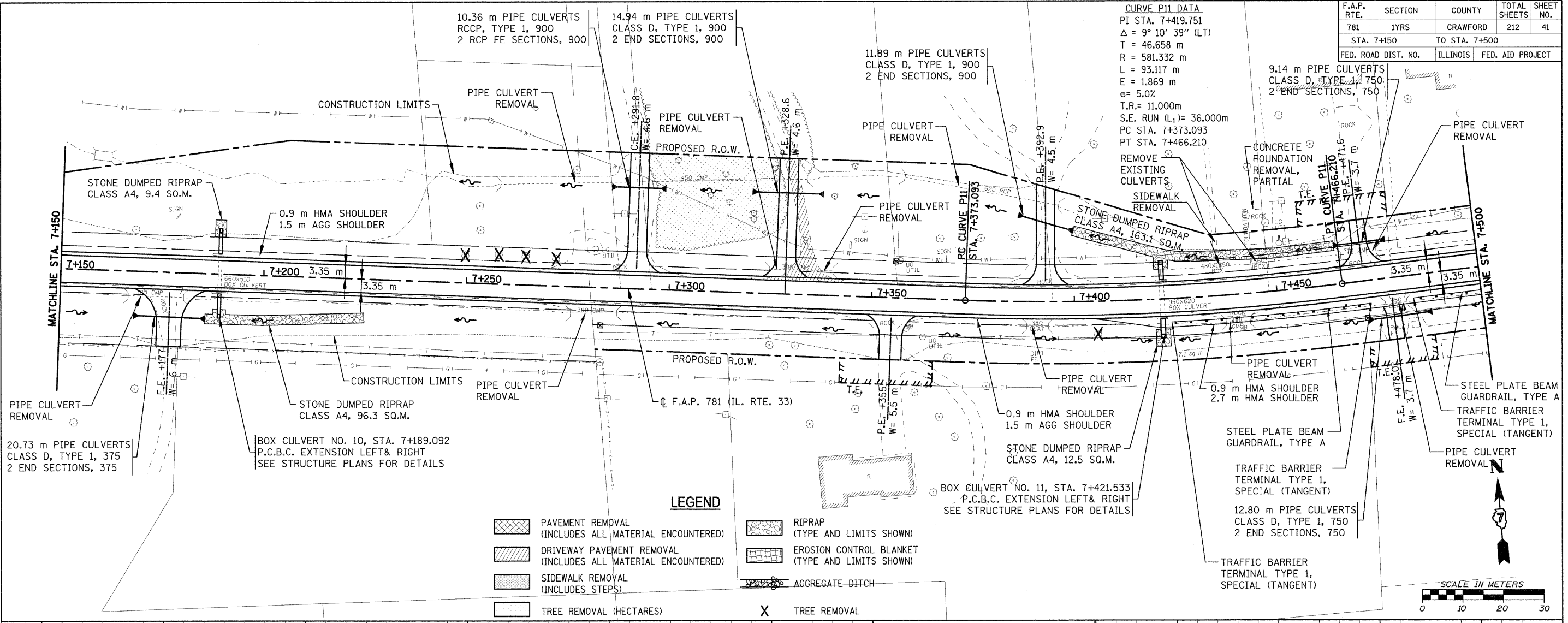
- PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED)
- DRIVEWAY PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED)
- SIDEWALK REMOVAL (INCLUDES STEPS)
- TREE REMOVAL (HECTARES)
- RIPRAP (TYPE AND LIMITS SHOWN)
- EROSION CONTROL BLANKET (TYPE AND LIMITS SHOWN)
- AGGREGATE DITCH
- TREE REMOVAL (X)



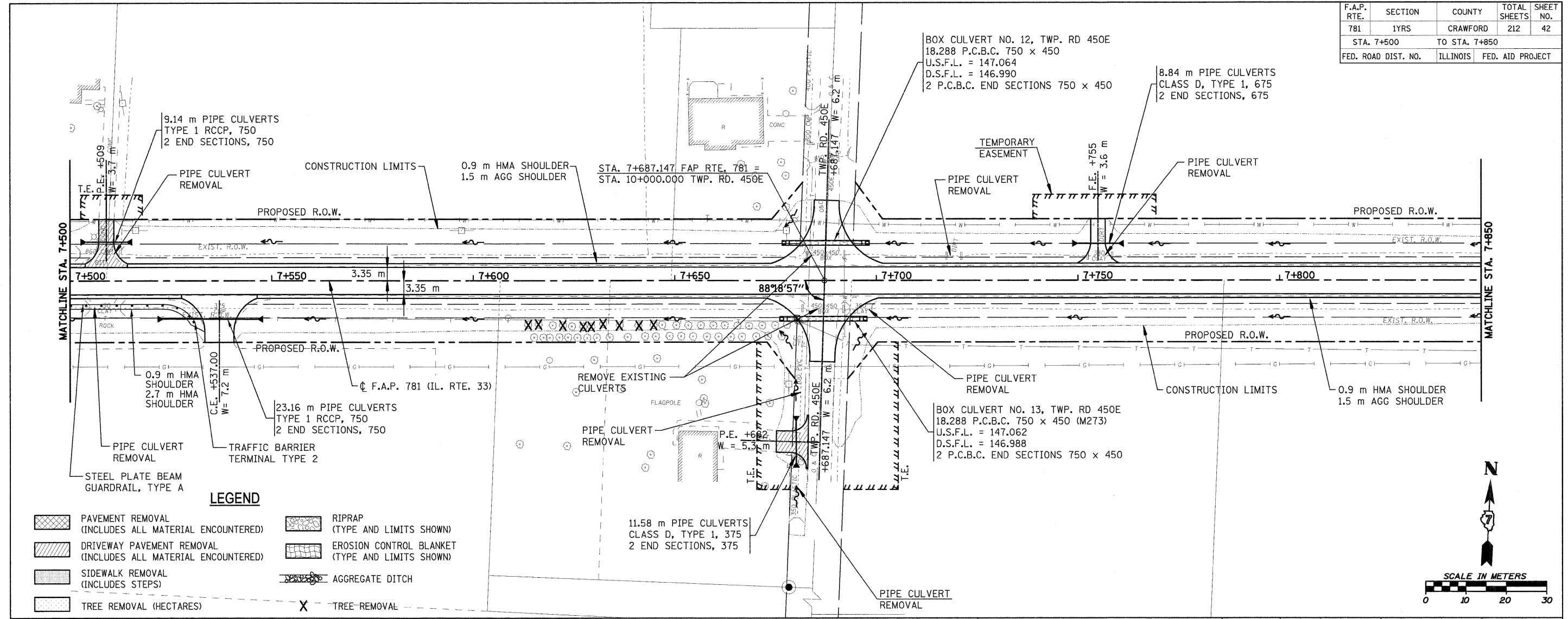
PLOT DATE = 12/22/2008
 FILE NAME = P:\4428\Contract\94533\SHT_40.dwg

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|------------------|--------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 41 |
| STA. 7+150 | | TO STA. 7+500 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

CURVE P11 DATA
 PI STA. 7+419.751
 $\Delta = 9^\circ 10' 39''$ (LT)
 T = 46.658 m
 R = 581.332 m
 L = 93.117 m
 E = 1.869 m
 $e = 5.0\%$
 T.R. = 11.000m
 S.E. RUN (L₁) = 36.000m
 PC STA. 7+373.093
 PT STA. 7+466.210

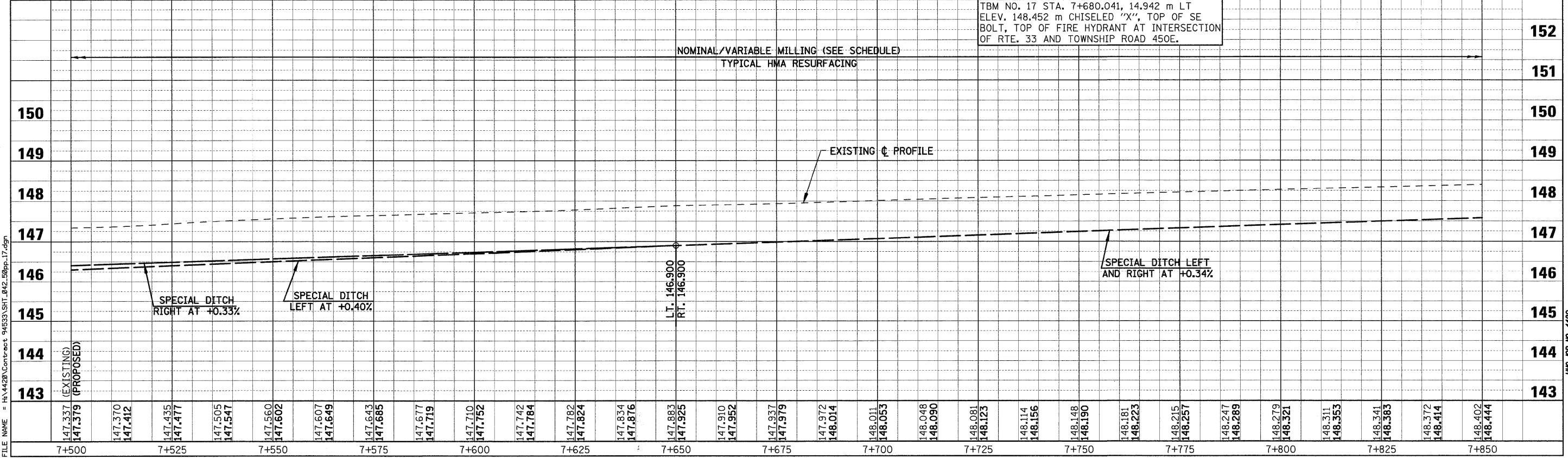
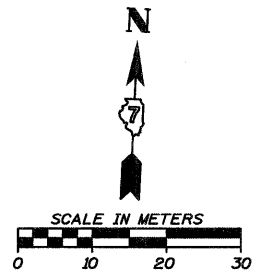


| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|------------------|--------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 42 |
| STA. 7+500 | | TO STA. 7+850 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

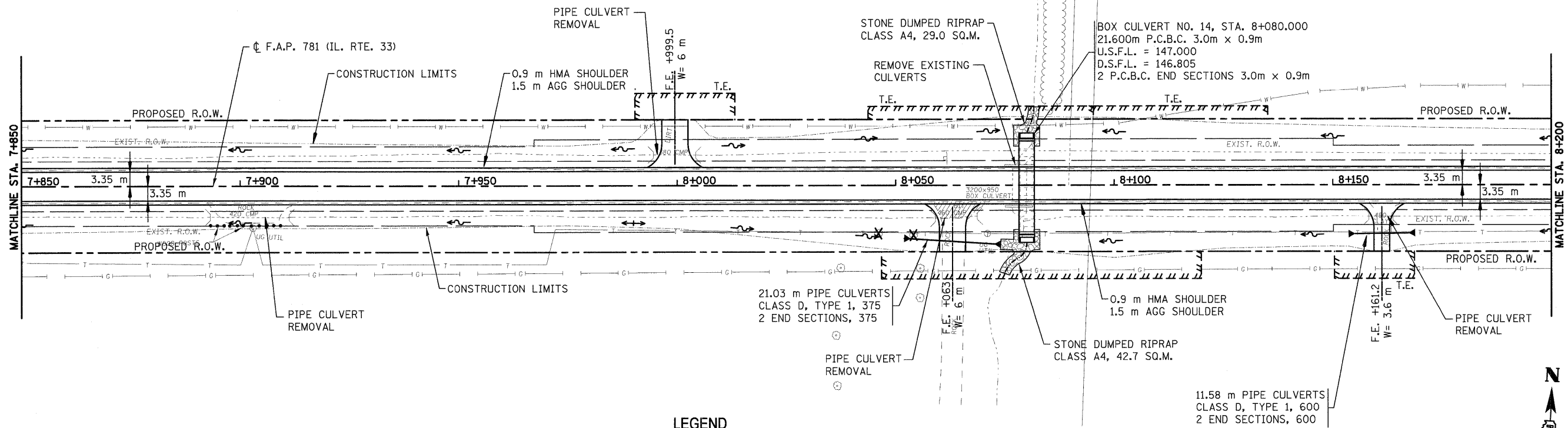


LEGEND

| | |
|---|---|
| PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED) | RIPRAP (TYPE AND LIMITS SHOWN) |
| DRIVEWAY PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED) | EROSION CONTROL BLANKET (TYPE AND LIMITS SHOWN) |
| SIDEWALK REMOVAL (INCLUDES STEPS) | AGGREGATE DITCH |
| TREE REMOVAL (HECTARES) | TREE-REMOVAL |

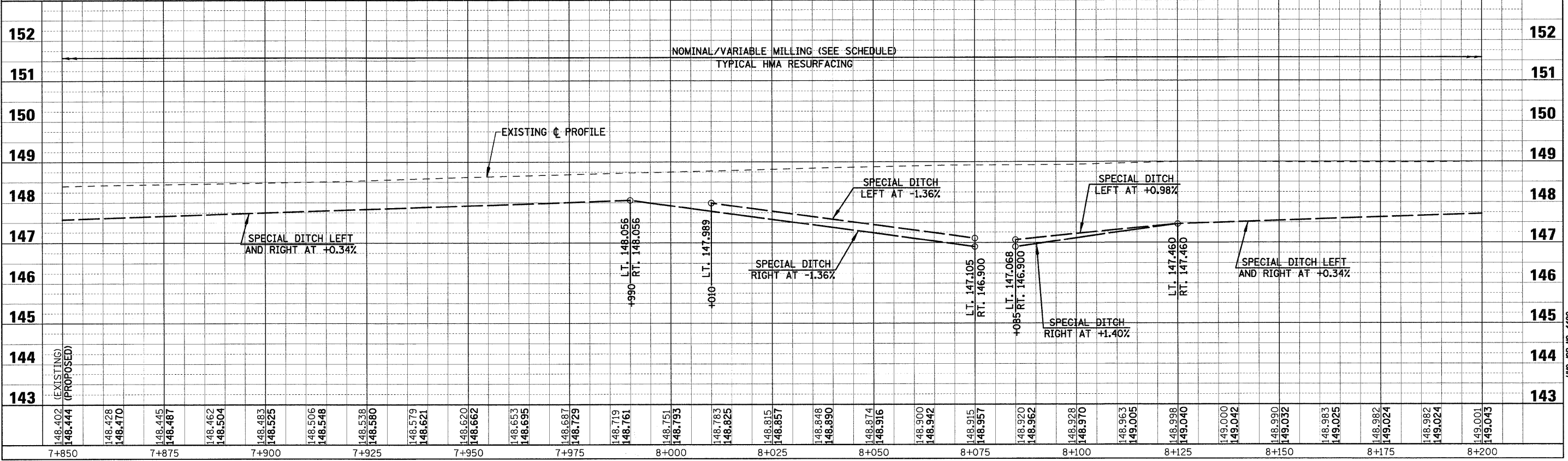
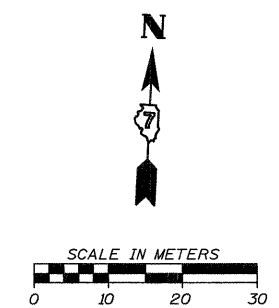


| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|---------------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 43 |
| STA. 7+850 | | TO STA. 8+200 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



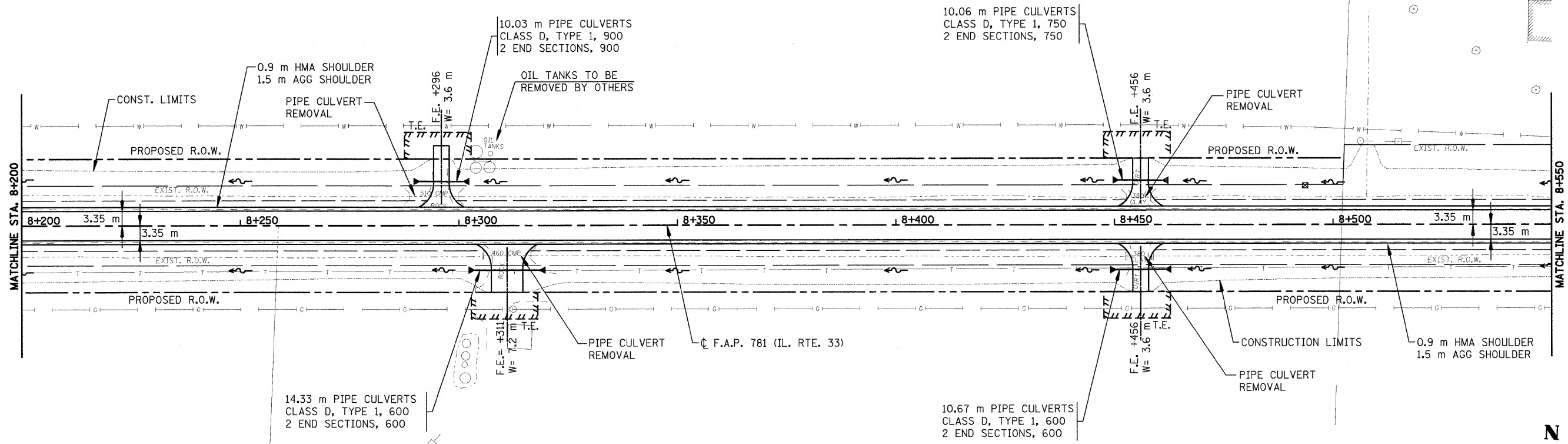
LEGEND

- PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED)
- DRIVEWAY PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED)
- SIDEWALK REMOVAL (INCLUDES STEPS)
- TREE REMOVAL (HECTARES)
- RIPRAP (TYPE AND LIMITS SHOWN)
- EROSION CONTROL BLANKET (TYPE AND LIMITS SHOWN)
- AGGREGATE DITCH
- TREE REMOVAL



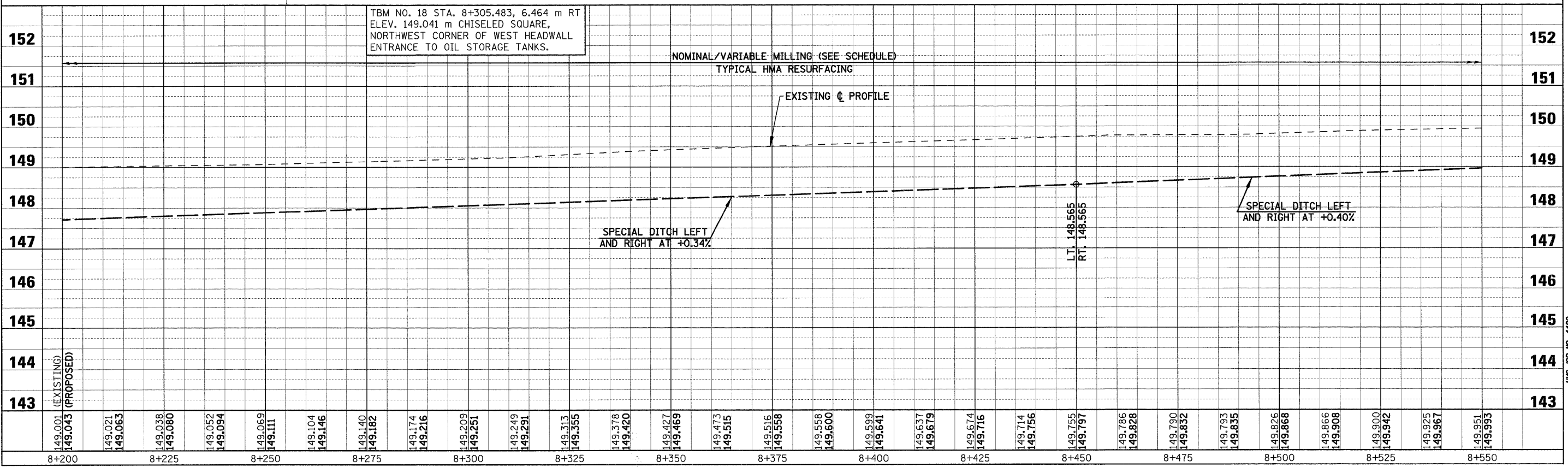
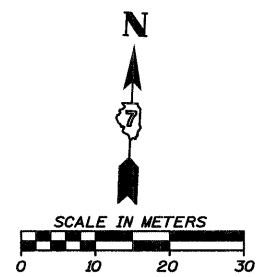
PLOT DATE = 12/22/2008
 FILE NAME = H:\V42\Cont-est-94533\SHT_043.dwg

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|---------------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 44 |
| STA. 8+200 | | TO STA. 8+550 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



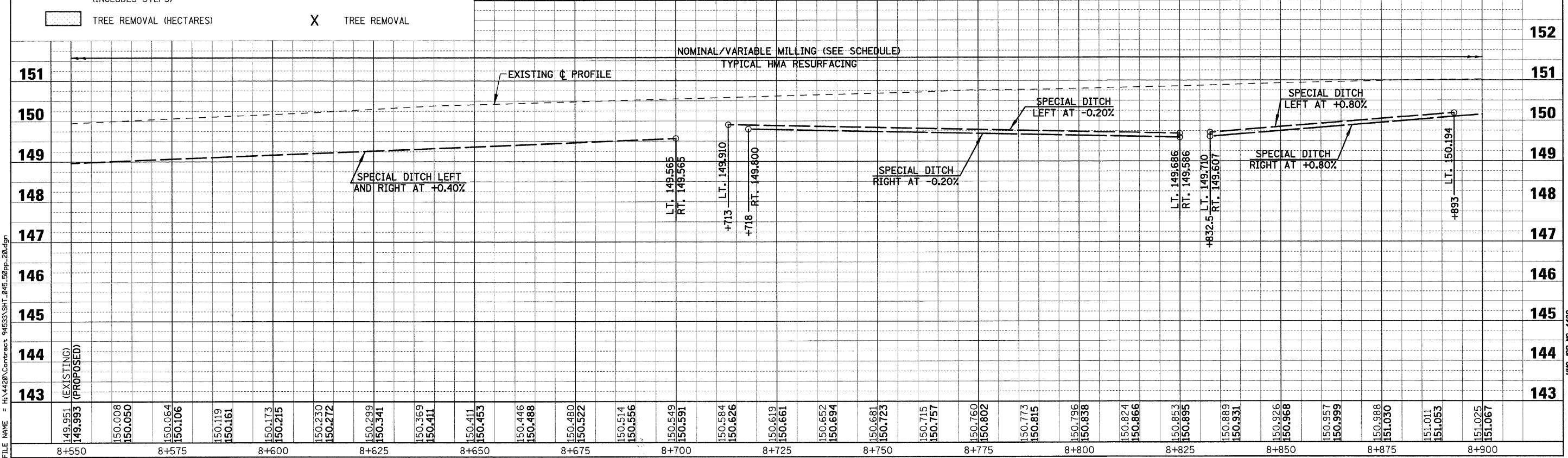
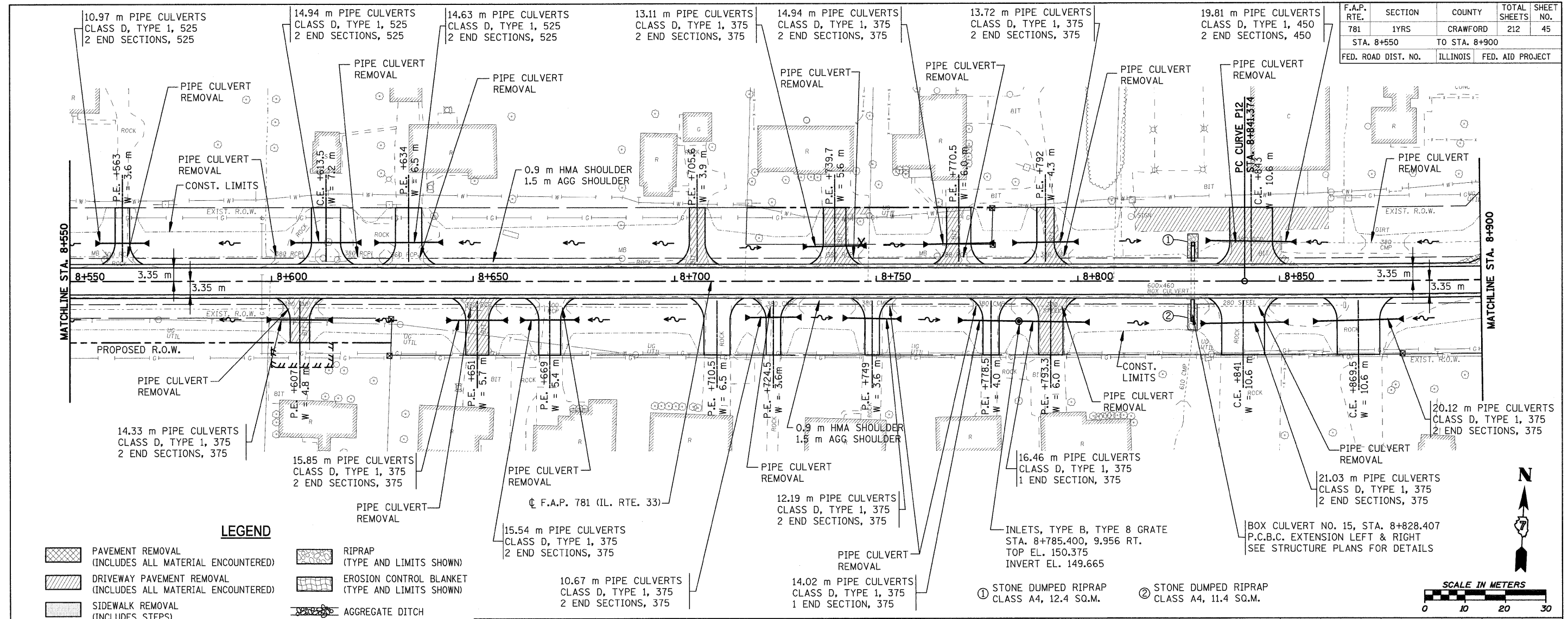
LEGEND

| | | | |
|--|--|--|--|
| | PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED) | | RIPRAP (TYPE AND LIMITS SHOWN) |
| | DRIVEWAY PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED) | | EROSION CONTROL BLANKET (TYPE AND LIMITS SHOWN) |
| | SIDEWALK REMOVAL (INCLUDES STEPS) | | AGGREGATE DITCH |
| | TREE REMOVAL (HECTARES) | | TREE REMOVAL |



PLOT DATE = 12/22/2008
 FILE NAME = P:\V\20\Contract 94533\SHT_044_50pp_19.dgn

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|---------------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 45 |
| STA. 8+550 | | TO STA. 8+900 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

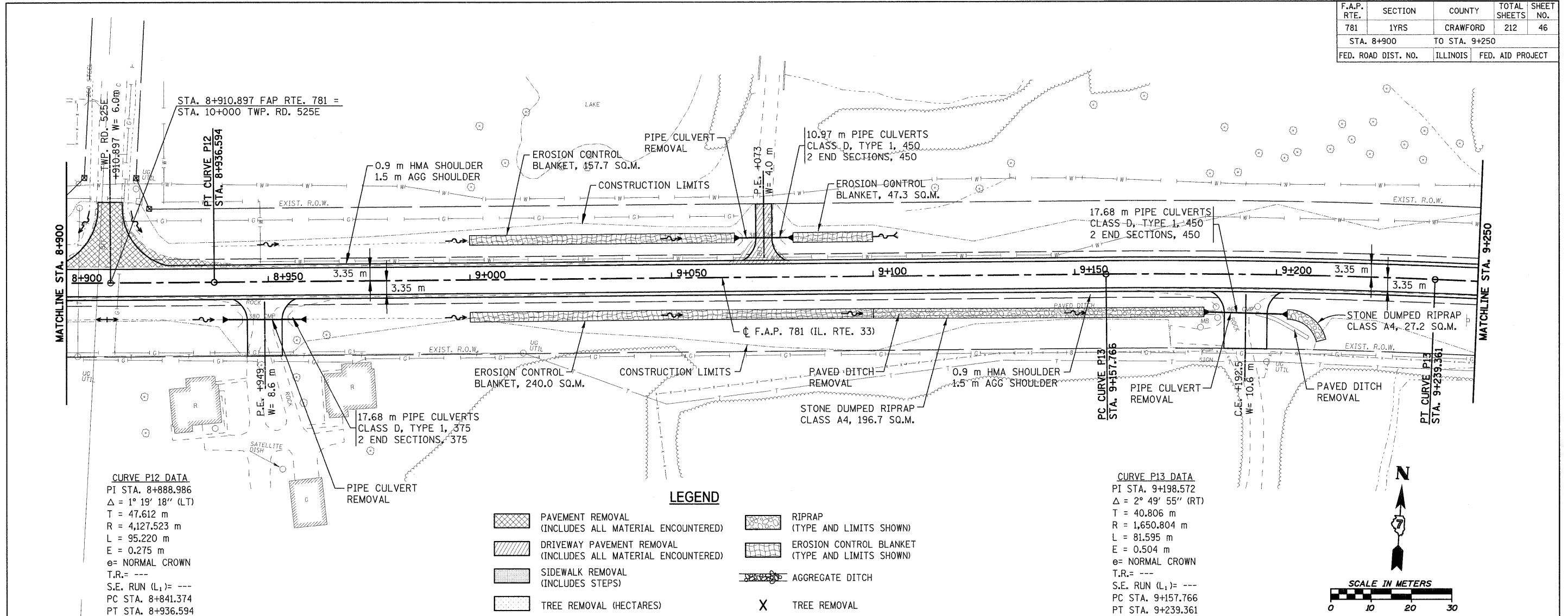


Sheet: 20
Angle: 2.8540
Chain: P_IL33

PLOT DATE = 12/22/2008
FILE NAME = H:\V\20\CONTRACT\94533\SH1_845_50pp_20.dgn

HMG JOB NO. 4420

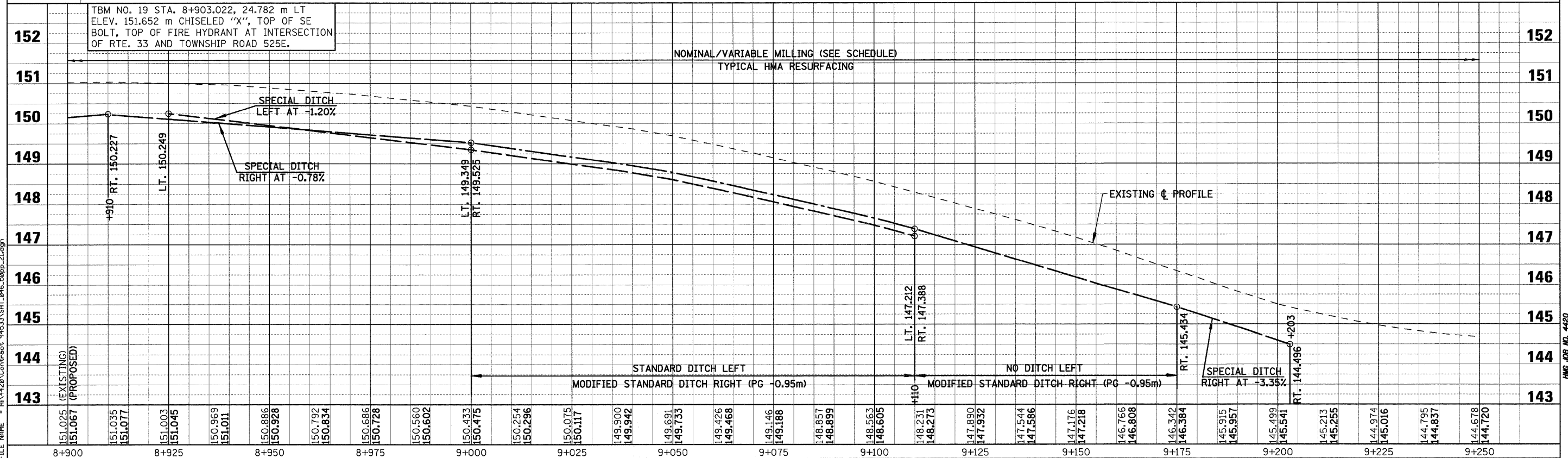
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|---------------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 46 |
| STA. 8+900 | | TO STA. 9+250 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



CURVE P12 DATA
 PI STA. 8+888.986
 $\Delta = 1^\circ 19' 18''$ (LT)
 T = 47.612 m
 R = 4,127.523 m
 L = 95.220 m
 E = 0.275 m
 e = NORMAL CROWN
 T.R. = ---
 S.E. RUN (L₁) = ---
 PC STA. 8+841.374
 PT STA. 8+936.594

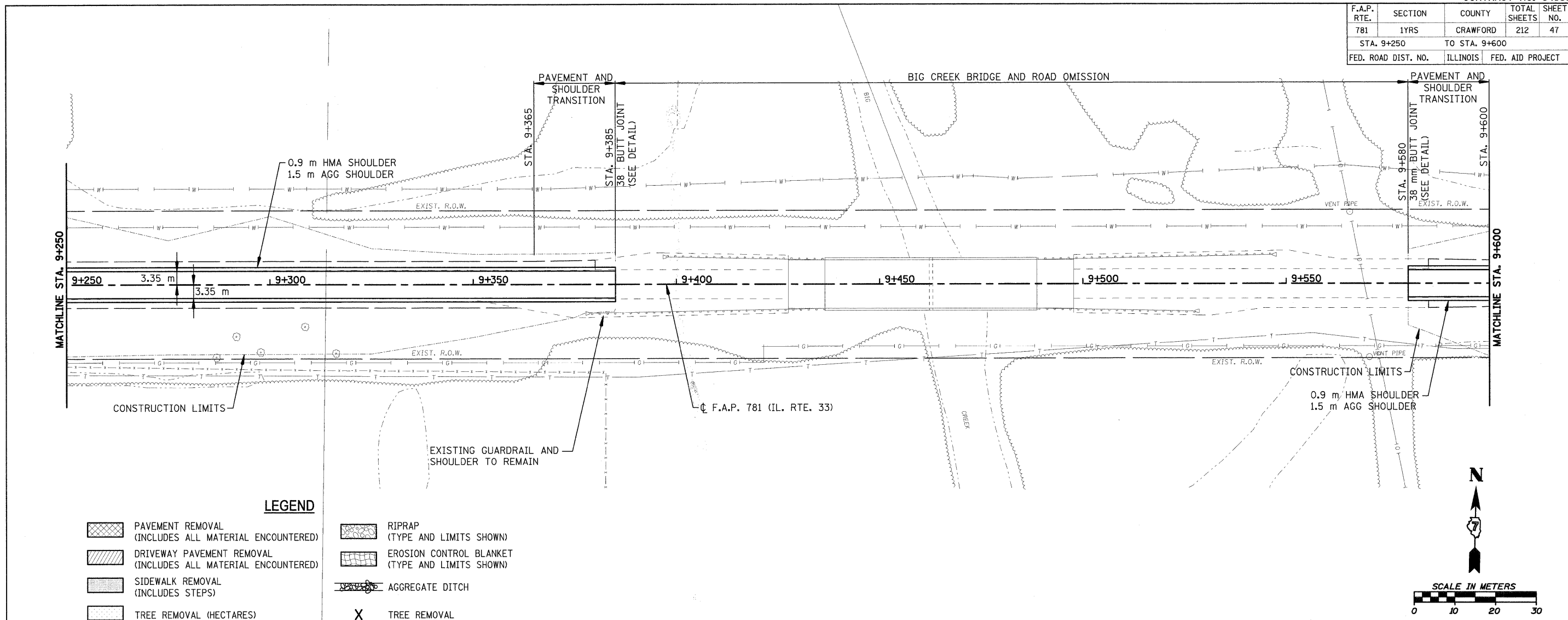
CURVE P13 DATA
 PI STA. 9+198.572
 $\Delta = 2^\circ 49' 55''$ (RT)
 T = 40.806 m
 R = 1,650.804 m
 L = 81.595 m
 E = 0.504 m
 e = NORMAL CROWN
 T.R. = ---
 S.E. RUN (L₁) = ---
 PC STA. 9+157.766
 PT STA. 9+239.361

- LEGEND**
- PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED)
 - DRIVEWAY PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED)
 - SIDEWALK REMOVAL (INCLUDES STEPS)
 - TREE REMOVAL (HECTARES)
 - RIPRAP (TYPE AND LIMITS SHOWN)
 - EROSION CONTROL BLANKET (TYPE AND LIMITS SHOWN)
 - AGGREGATE DITCH
 - TREE REMOVAL



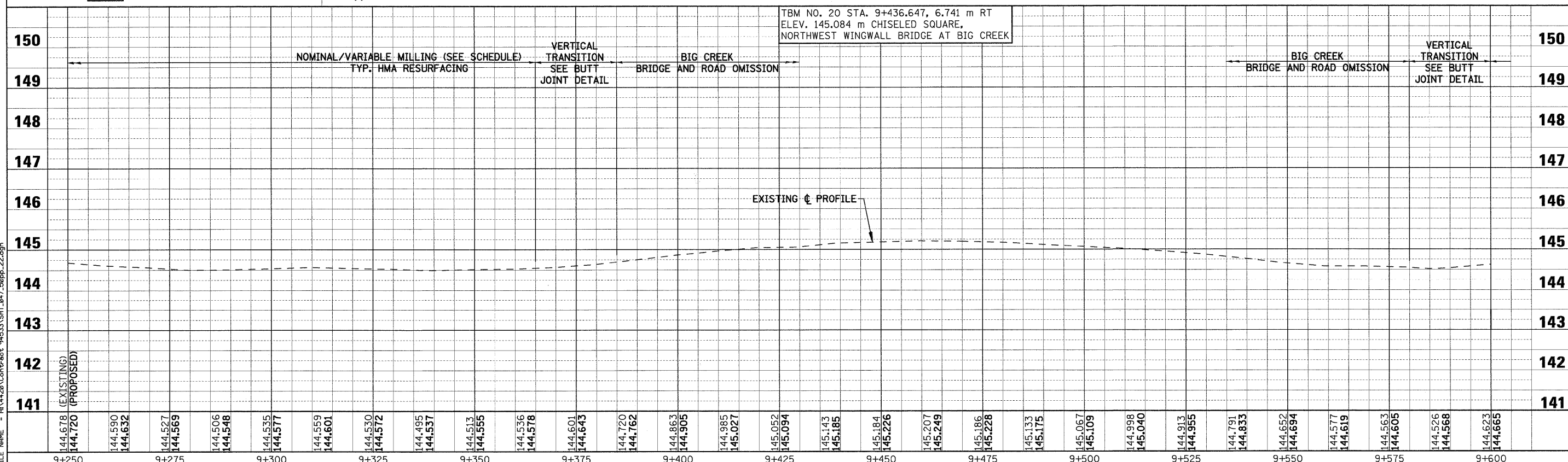
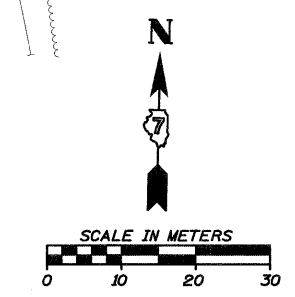
PLOT DATE = 12/22/2008
 FILE NAME = H:\1420\Contract 94533\SH1_046_50pp_21.dgn

| | | | | |
|---------------------|----------|------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 47 |
| STA. 9+250 | | TO STA. 9+600 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



LEGEND

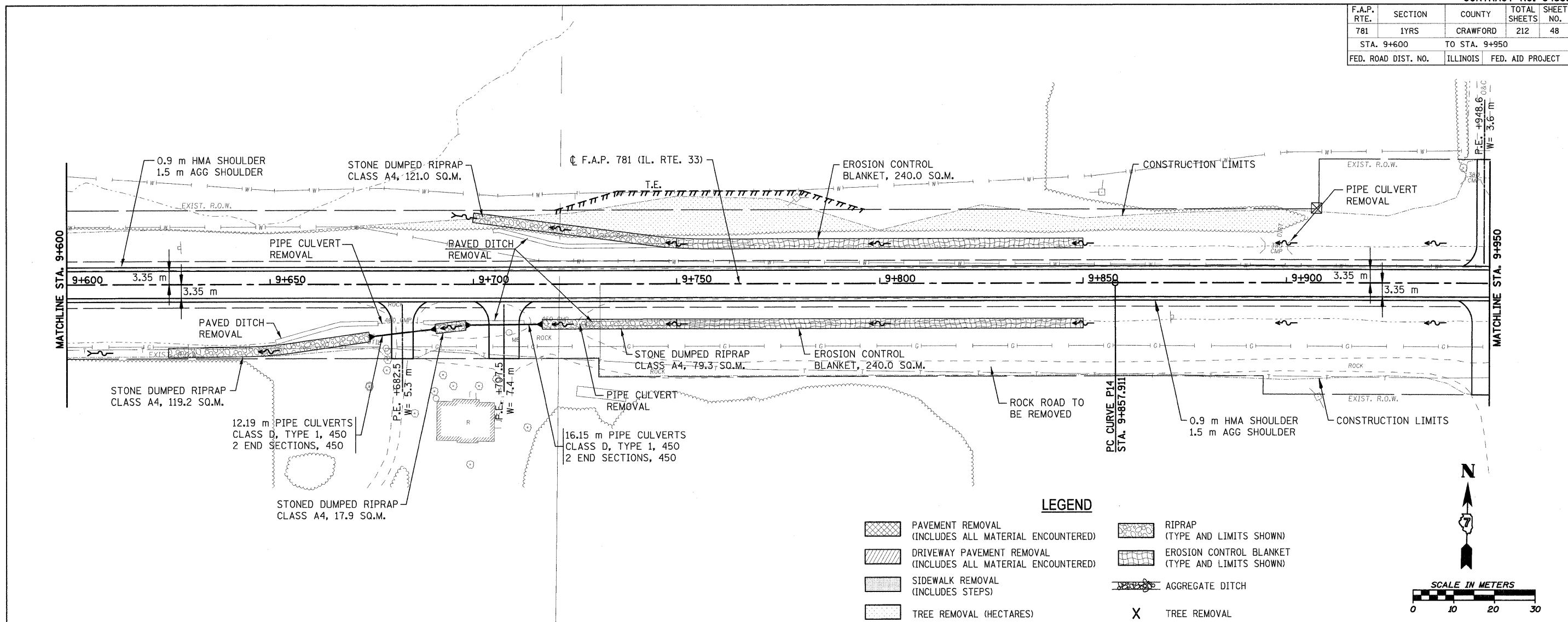
| | | | |
|--|--|--|--|
| | PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED) | | RIPRAP (TYPE AND LIMITS SHOWN) |
| | DRIVEWAY PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED) | | EROSION CONTROL BLANKET (TYPE AND LIMITS SHOWN) |
| | SIDEWALK REMOVAL (INCLUDES STEPS) | | AGGREGATE DITCH |
| | TREE REMOVAL (HECTARES) | | TREE REMOVAL |



PLOT DATE = 12/22/2008
 FILE NAME = H:\V42\Cont+est_94533\SHT_047_50pp_22.dgn

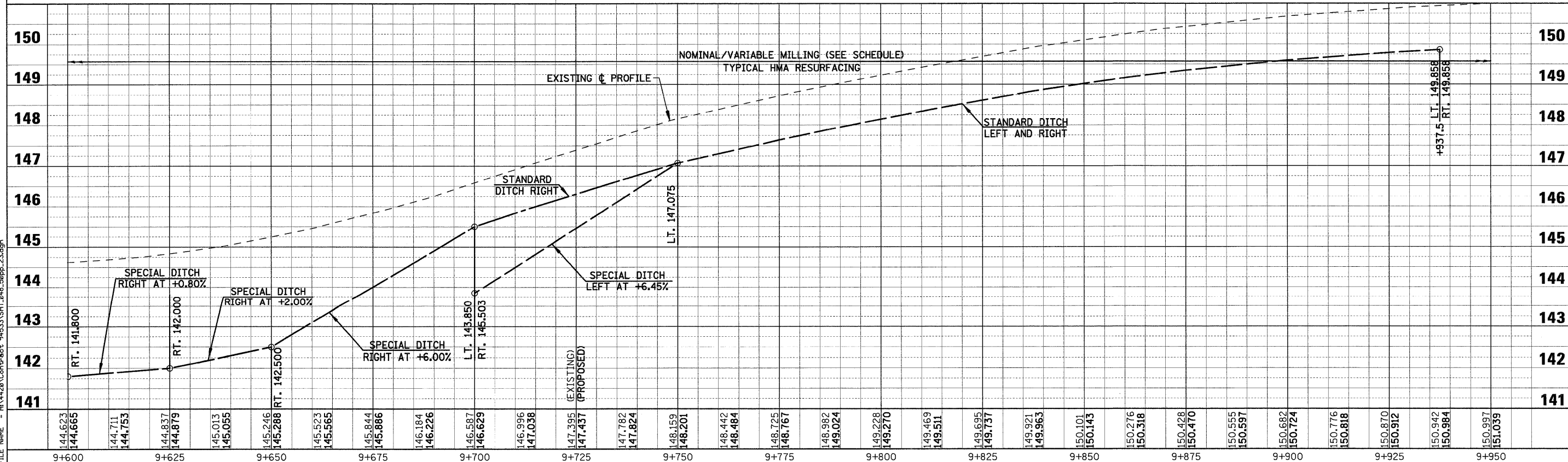
HMG JOB NO. 4420

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|---------------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 48 |
| STA. 9+600 | | TO STA. 9+950 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



LEGEND

- PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED)
- DRIVEWAY PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED)
- SIDEWALK REMOVAL (INCLUDES STEPS)
- TREE REMOVAL (HECTARES)
- RIPRAP (TYPE AND LIMITS SHOWN)
- EROSION CONTROL BLANKET (TYPE AND LIMITS SHOWN)
- AGGREGATE DITCH
- TREE REMOVAL

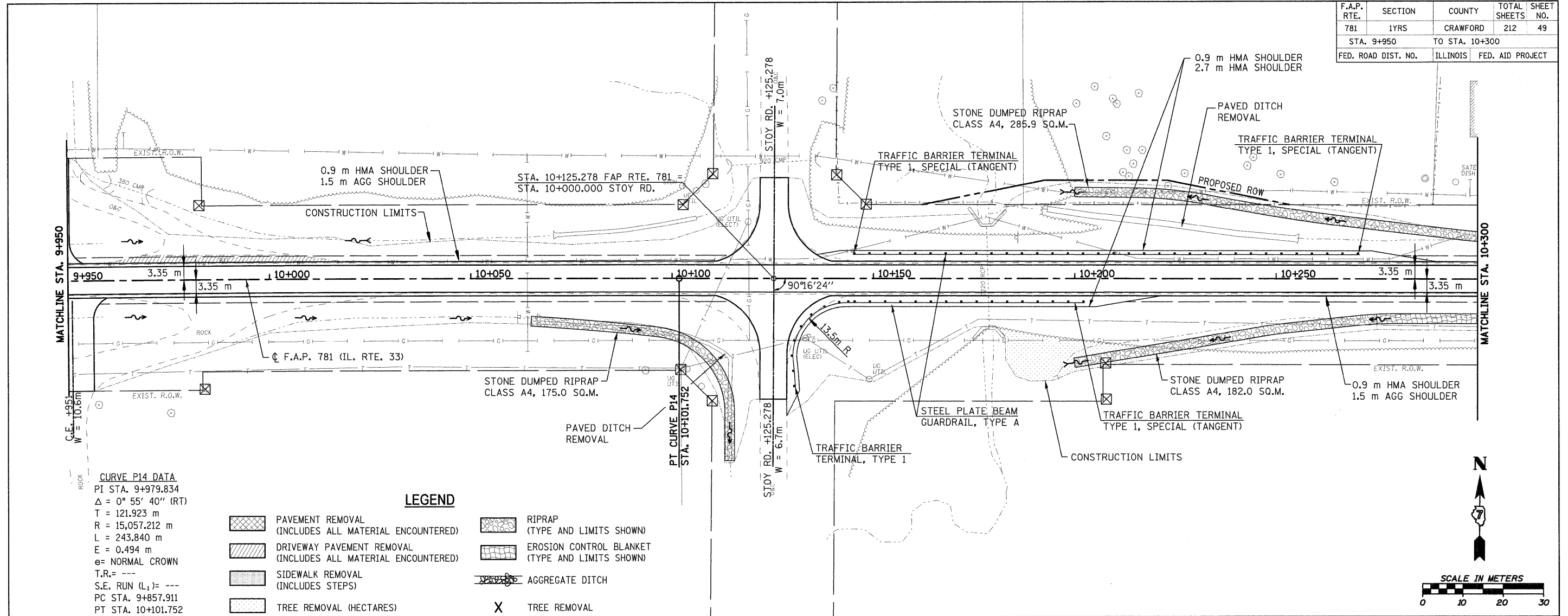


Sheet: 23
 Angle: 1.2233
 Chain: P_IL33

PLOT DATE = 12/22/2006
 FILE NAME = H:\V420\Contract 94533\SH1_046_50pp_23.dgn

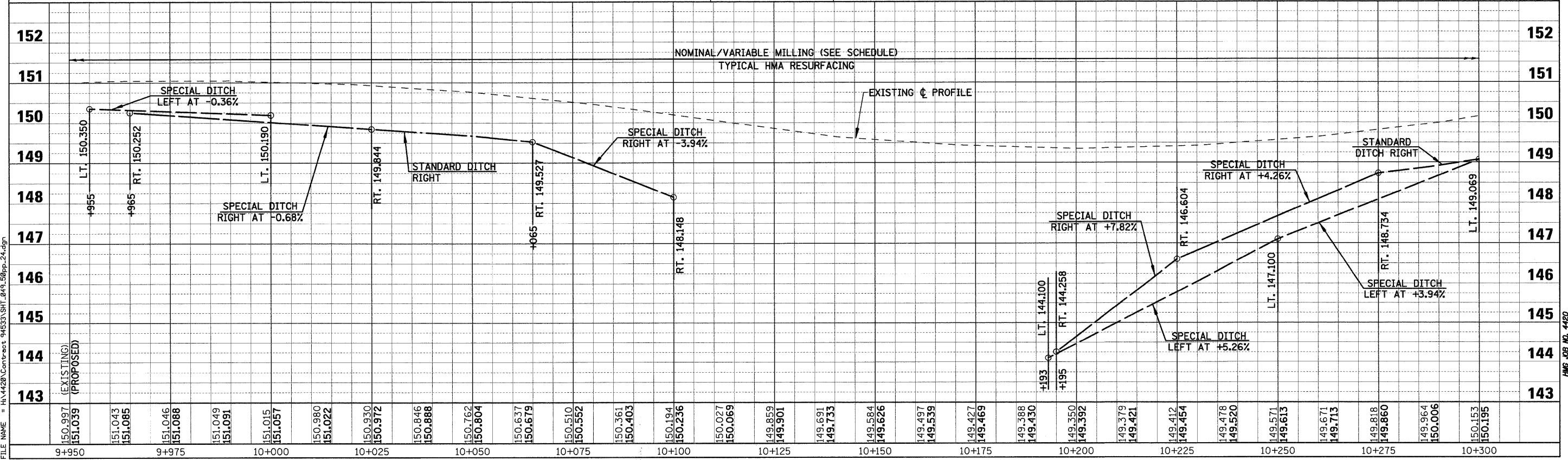
HMG JOB NO. 4420

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|------------------|--------------|-----------|
| 781 | LYRS | CRAWFORD | 212 | 49 |
| STA. 9+950 | | TO STA. 10+300 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

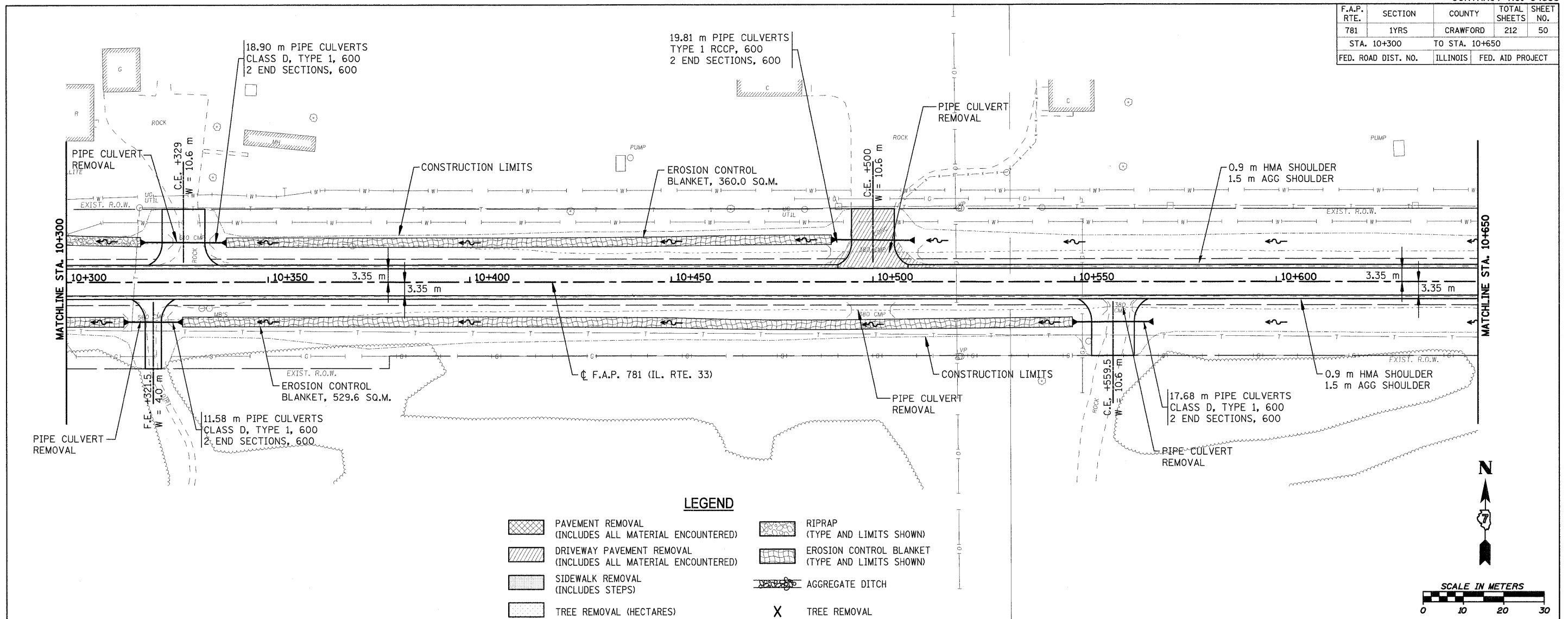


CURVE P14 DATA
 PI STA. 9+979.834
 $\Delta = 0^\circ 55' 40''$ (RT)
 T = 121.923 m
 R = 15,057.212 m
 L = 243.840 m
 E = 0.494 m
 e = NORMAL CROWN
 T.R. = ---
 S.E. RUN (L₁) = ---
 PC STA. 9+857.911
 PT STA. 10+101.752

- LEGEND**
- PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED)
 - DRIVEWAY PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED)
 - SIDEWALK REMOVAL (INCLUDES STEPS)
 - TREE REMOVAL (HECTARES)
 - RIPRAP (TYPE AND LIMITS SHOWN)
 - EROSION CONTROL BLANKET (TYPE AND LIMITS SHOWN)
 - AGGREGATE DITCH
 - TREE REMOVAL

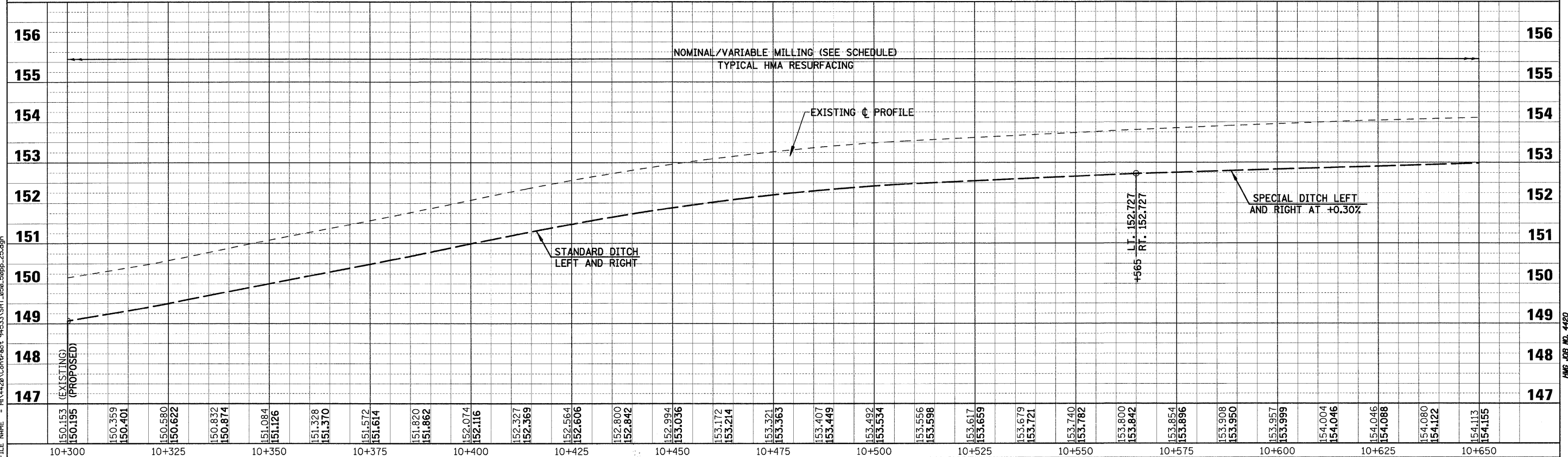


| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 50 |
| STA. 10+300 | | TO STA. 10+650 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



LEGEND

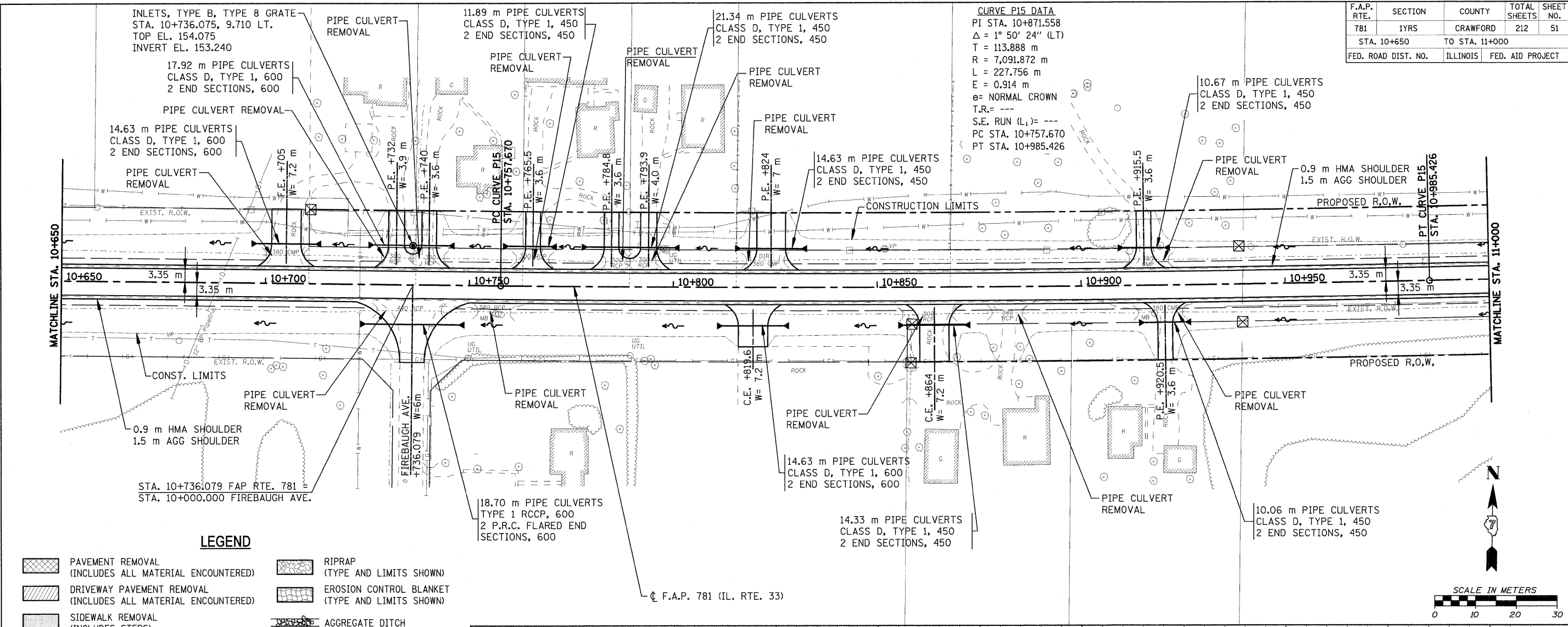
- PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED)
- DRIVEWAY PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED)
- SIDEWALK REMOVAL (INCLUDES STEPS)
- TREE REMOVAL (HECTARES)
- RIPRAP (TYPE AND LIMITS SHOWN)
- EROSION CONTROL BLANKET (TYPE AND LIMITS SHOWN)
- AGGREGATE DITCH
- TREE REMOVAL



PLOT DATE = 12/22/2008
 FILE NAME = H:\1420\Contract 94533\SH1_050_50pp_25.dgn

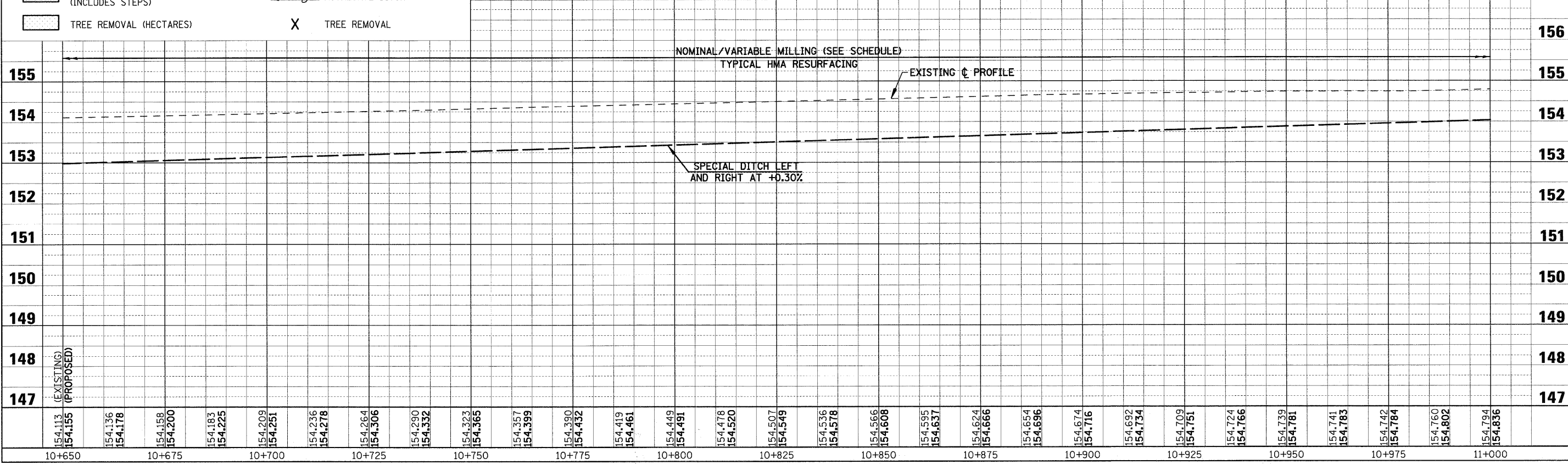
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------------|------------------|-----------|
| 781 | | CRAWFORD | 212 | 51 |
| STA. 10+650 | | TO STA. 11+000 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

CURVE P15 DATA
 PI STA. 10+871.558
 $\Delta = 1^\circ 50' 24''$ (LT)
 T = 113.888 m
 R = 7,091.872 m
 L = 227.756 m
 E = 0.914 m
 e = NORMAL CROWN
 T.R. = ---
 S.E. RUN (L₁) = ---
 PC STA. 10+757.670
 PT STA. 10+985.426



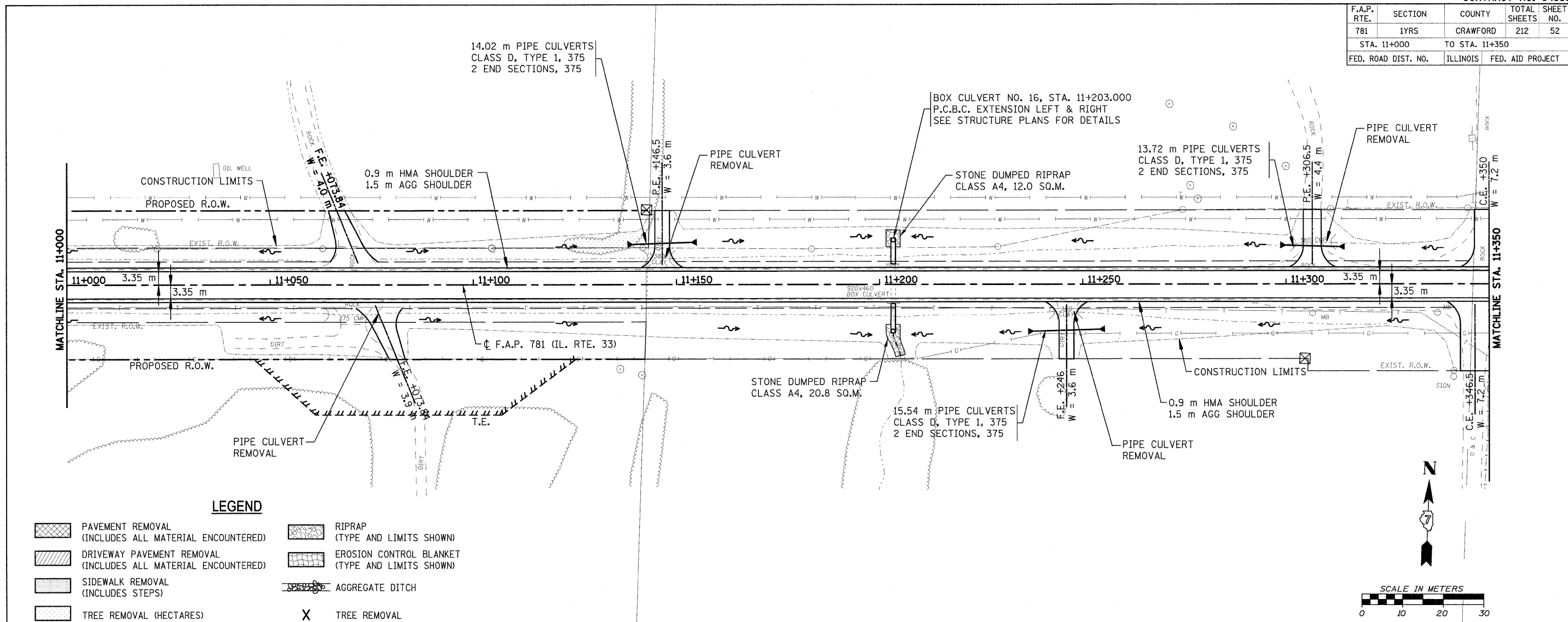
LEGEND

- PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED)
- DRIVEWAY PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED)
- SIDEWALK REMOVAL (INCLUDES STEPS)
- TREE REMOVAL (HECTARES)
- RIPRAP (TYPE AND LIMITS SHOWN)
- EROSION CONTROL BLANKET (TYPE AND LIMITS SHOWN)
- AGGREGATE DITCH
- TREE REMOVAL



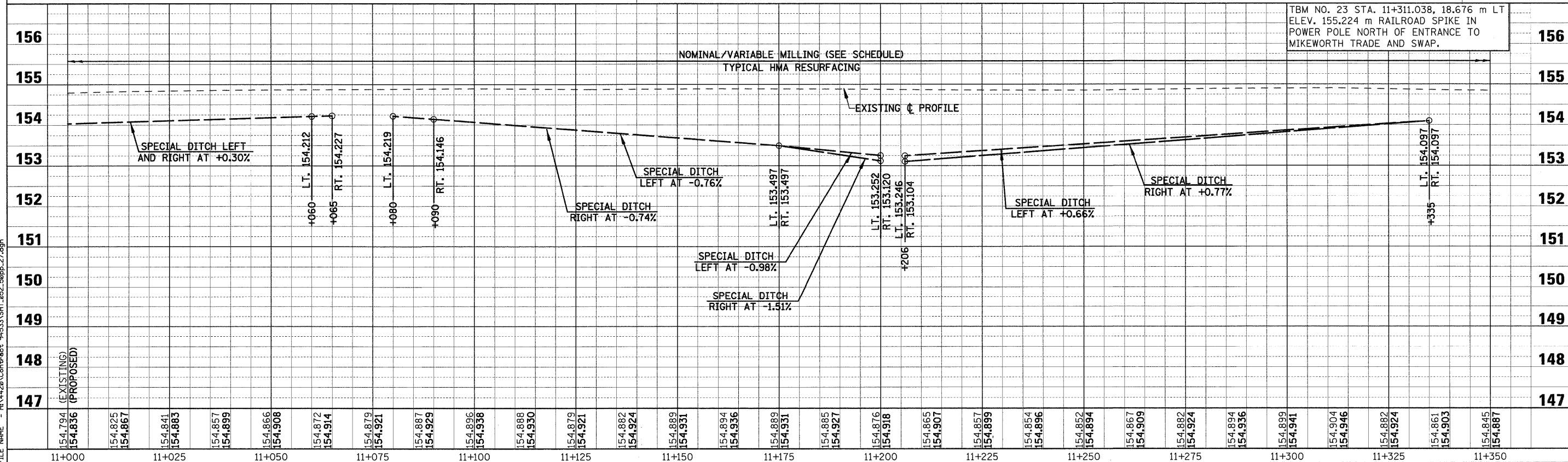
PLOT DATE = 12/22/2008
 FILE NAME = H:\V420\Conte\cont 94533\SH1_081_50pp_26.dgn

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|------------------|--------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 52 |
| STA. 11+000 | | TO STA. 11+350 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



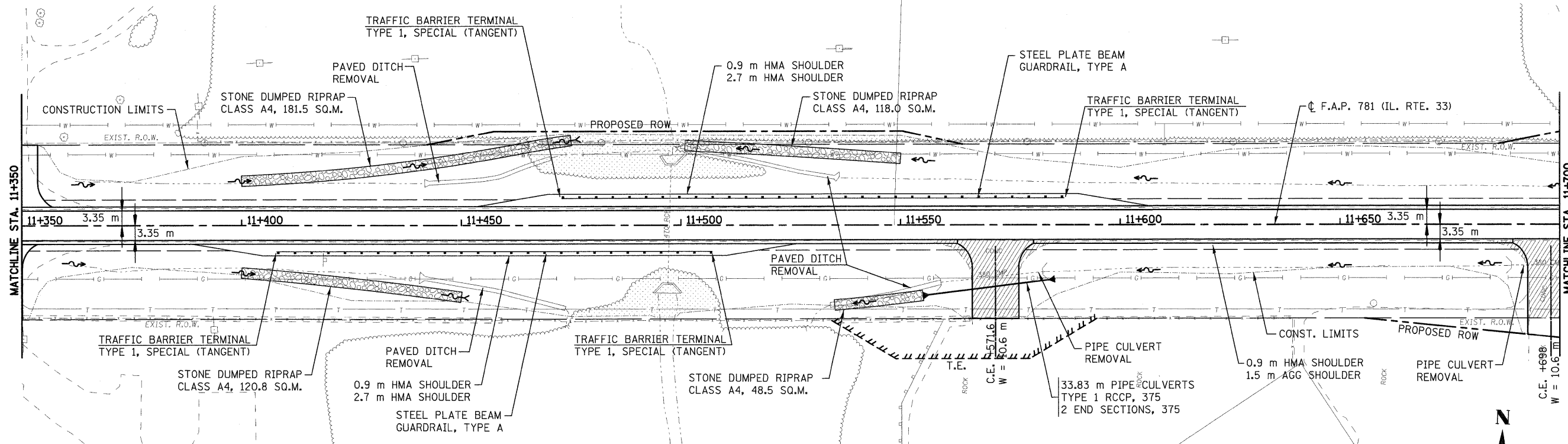
LEGEND

- PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED)
- DRIVEWAY PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED)
- SIDEWALK REMOVAL (INCLUDES STEPS)
- TREE REMOVAL (HECTARES)
- RIPRAP (TYPE AND LIMITS SHOWN)
- EROSION CONTROL BLANKET (TYPE AND LIMITS SHOWN)
- AGGREGATE DITCH
- TREE REMOVAL



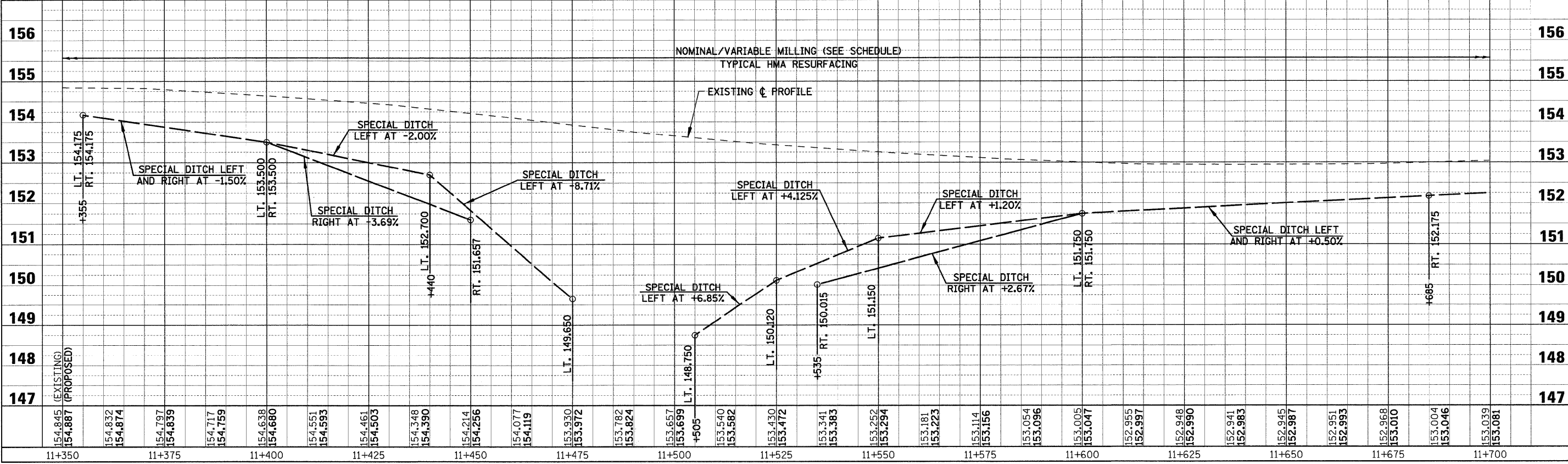
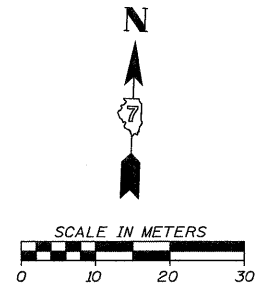
PLOT DATE = 12/22/2008
FILE NAME = H:\V428\Contract 94533\SR1_062_50pp_27.dgn

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 53 |
| STA. 11+350 | | TO STA. 11+700 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



LEGEND

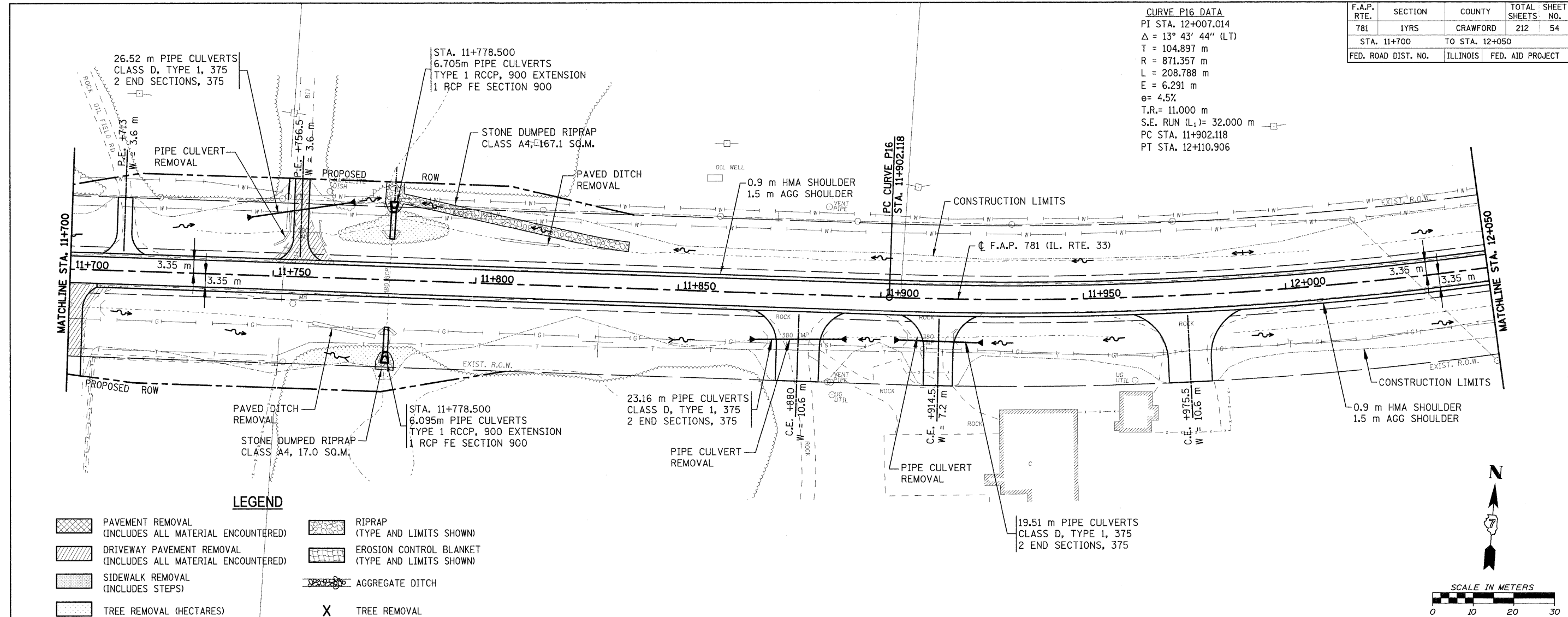
- PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED)
- DRIVEWAY PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED)
- SIDEWALK REMOVAL (INCLUDES STEPS)
- TREE REMOVAL (HECTARES)
- RIPRAP (TYPE AND LIMITS SHOWN)
- EROSION CONTROL BLANKET (TYPE AND LIMITS SHOWN)
- AGGREGATE DITCH
- TREE REMOVAL



PLOT DATE = 12/22/2008
 FILE NAME = H:\V420\Contract\94533\SHT_053_50ppr_2b.dgn

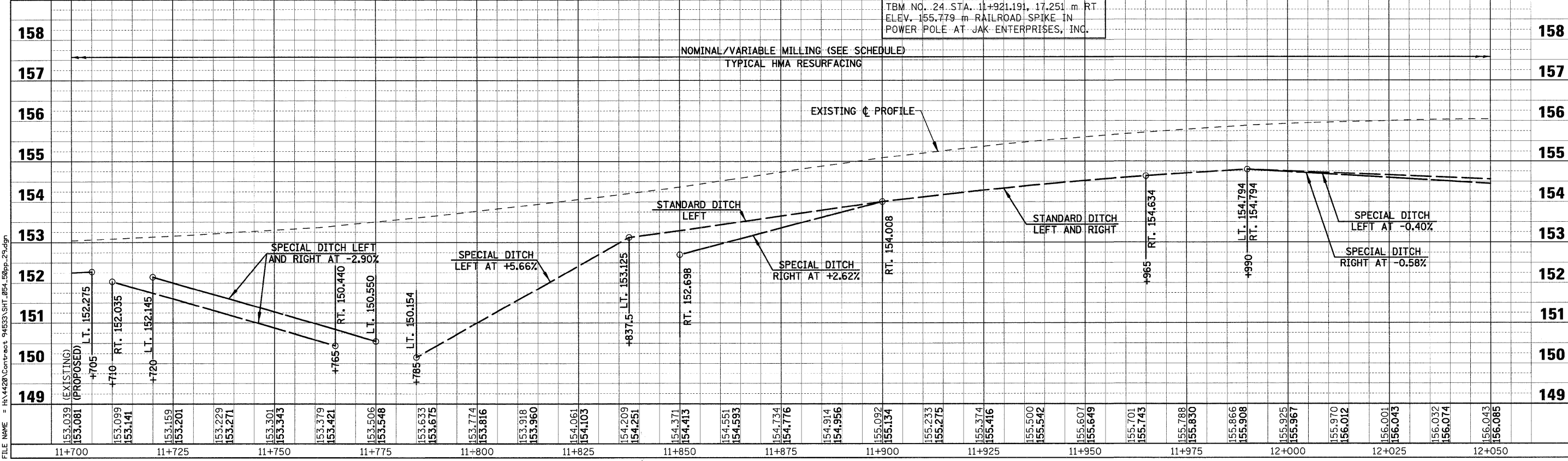
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 54 |
| STA. 11+700 | | TO STA. 12+050 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

CURVE P16 DATA
 PI STA. 12+007.014
 $\Delta = 13^\circ 43' 44''$ (LT)
 $T = 104.897$ m
 $R = 871.357$ m
 $L = 208.788$ m
 $E = 6.291$ m
 $e = 4.5\%$
 $T.R. = 11.000$ m
 $S.E. RUN (L_1) = 32.000$ m
 PC STA. 11+902.118
 PT STA. 12+110.906



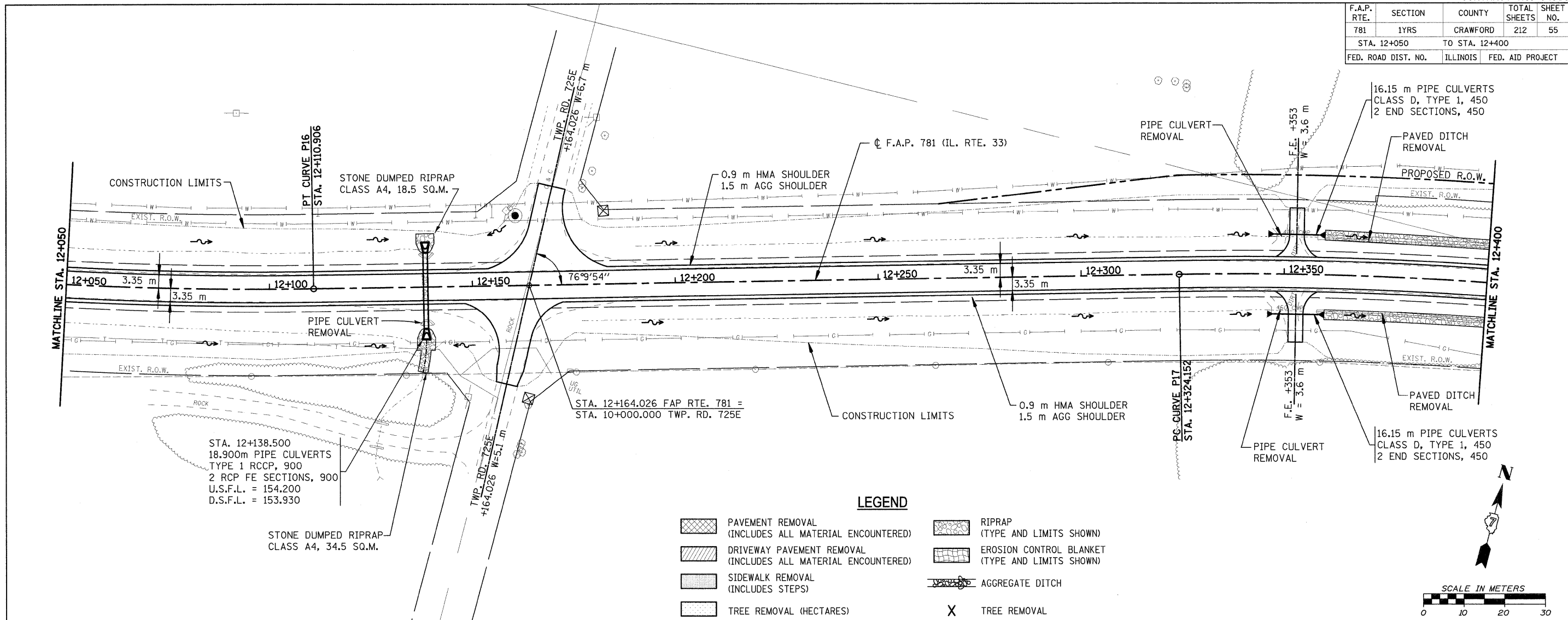
LEGEND

- PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED)
- DRIVEWAY PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED)
- SIDEWALK REMOVAL (INCLUDES STEPS)
- TREE REMOVAL (HECTARES)
- RIPRAP (TYPE AND LIMITS SHOWN)
- EROSION CONTROL BLANKET (TYPE AND LIMITS SHOWN)
- AGGREGATE DITCH
- TREE REMOVAL



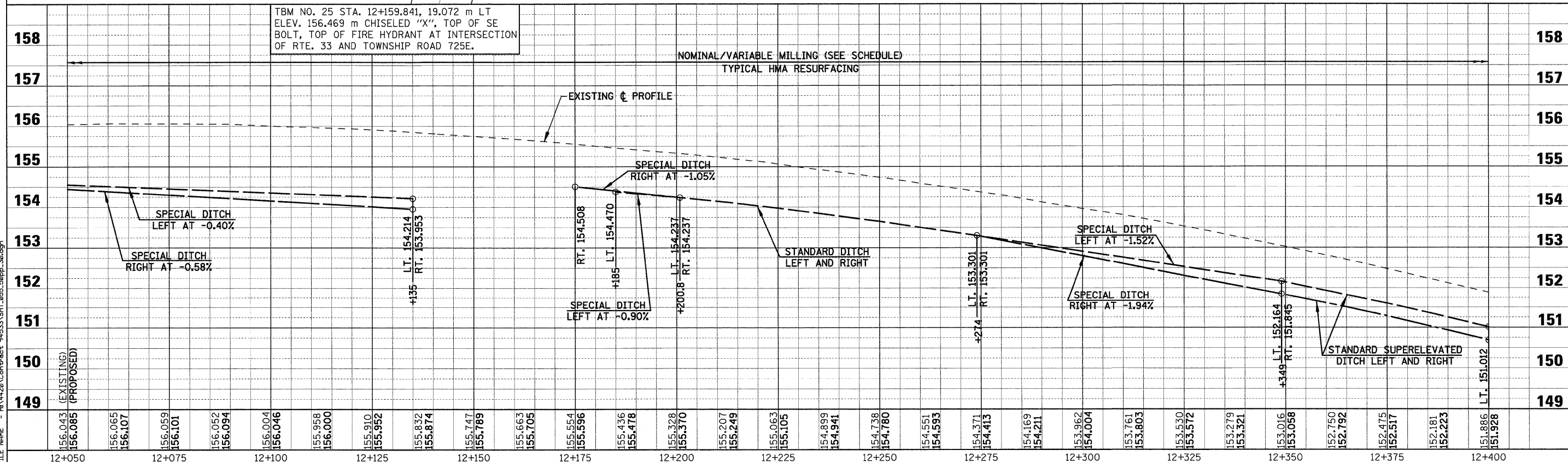
PLOT DATE = 12/22/2008
 FILE NAME = H:\V\20\Contract\94533\SH1_054_50pp_2.dwg

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 55 |
| STA. 12+050 | | TO STA. 12+400 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



LEGEND

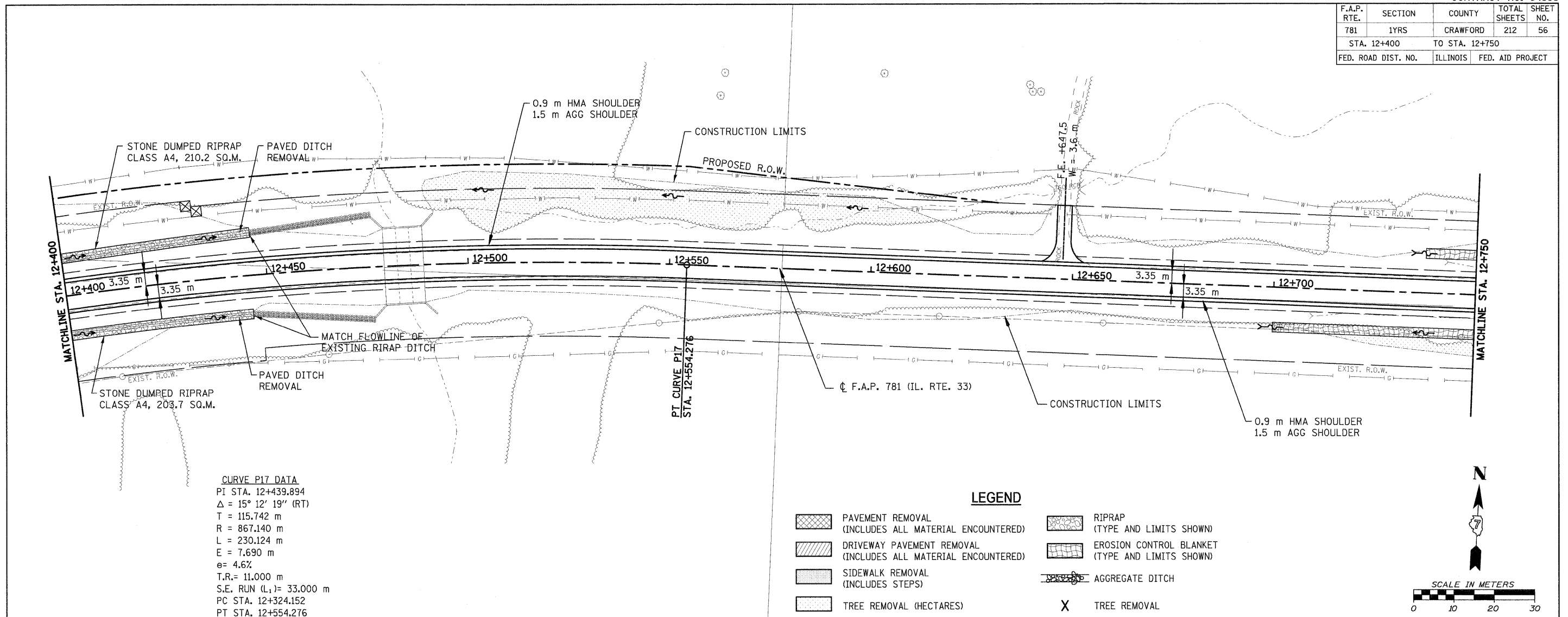
| | | | |
|--|---|--|---|
| | PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED) | | RIPRAP (TYPE AND LIMITS SHOWN) |
| | DRIVEWAY PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED) | | EROSION CONTROL BLANKET (TYPE AND LIMITS SHOWN) |
| | SIDEWALK REMOVAL (INCLUDES STEPS) | | AGGREGATE DITCH |
| | TREE REMOVAL (HECTARES) | | TREE REMOVAL |



TBM NO. 25 STA. 12+159.841, 19.072 m LT ELEV. 156.469 m CHISELED "X", TOP OF SE BOLT, TOP OF FIRE HYDRANT AT INTERSECTION OF RTE. 33 AND TOWNSHIP ROAD 725E.

PLOT DATE = 12/22/2006
FILE NAME = P:\V\20\Contract\94533\SH1_065_50pp_30.dgn

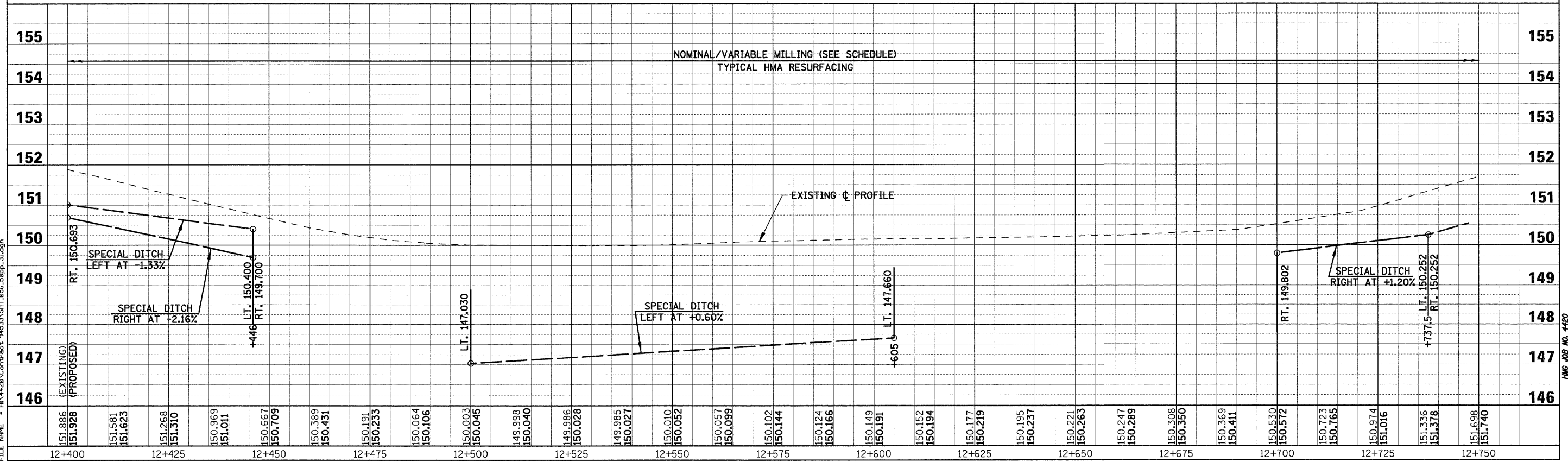
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 56 |
| STA. 12+400 | | TO STA. 12+750 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



CURVE P17 DATA
 PI STA. 12+439.894
 $\Delta = 15^\circ 12' 19''$ (RT)
 T = 115.742 m
 R = 867.140 m
 L = 230.124 m
 E = 7.690 m
 e = 4.6%
 T.R. = 11.000 m
 S.E. RUN (L₁) = 33.000 m
 PC STA. 12+324.152
 PT STA. 12+554.276

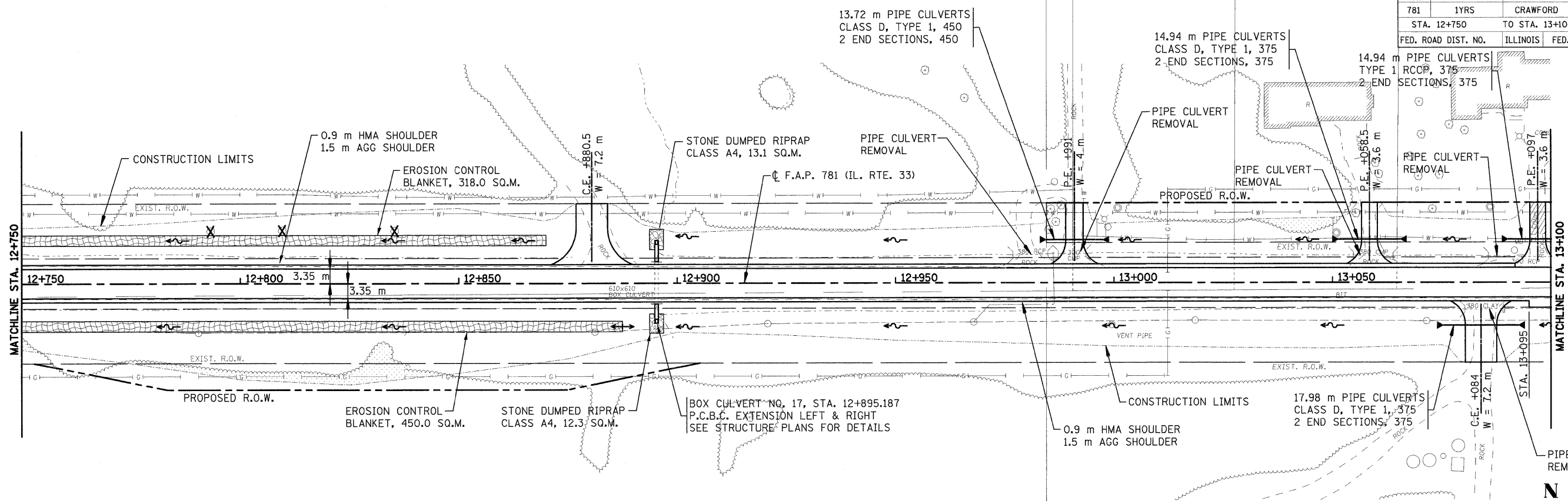
LEGEND

| | | | |
|--|--|--|--|
| | PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED) | | RIPRAP (TYPE AND LIMITS SHOWN) |
| | DRIVEWAY PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED) | | EROSION CONTROL BLANKET (TYPE AND LIMITS SHOWN) |
| | SIDEWALK REMOVAL (INCLUDES STEPS) | | AGGREGATE DITCH |
| | TREE REMOVAL (HECTARES) | | TREE REMOVAL |



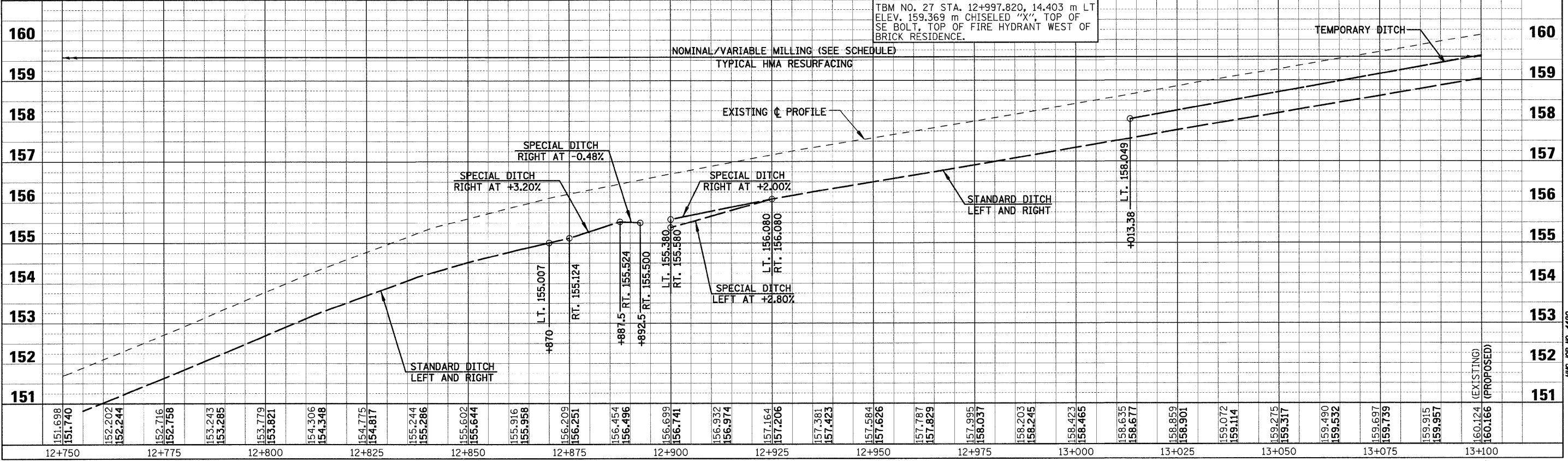
PLOT DATE = 12/22/2008
 FILE NAME = P:\V\20\Contract\94533\SH1_086_50pp_31.dgn

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|------------------|--------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 57 |
| STA. 12+750 | | TO STA. 13+100 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



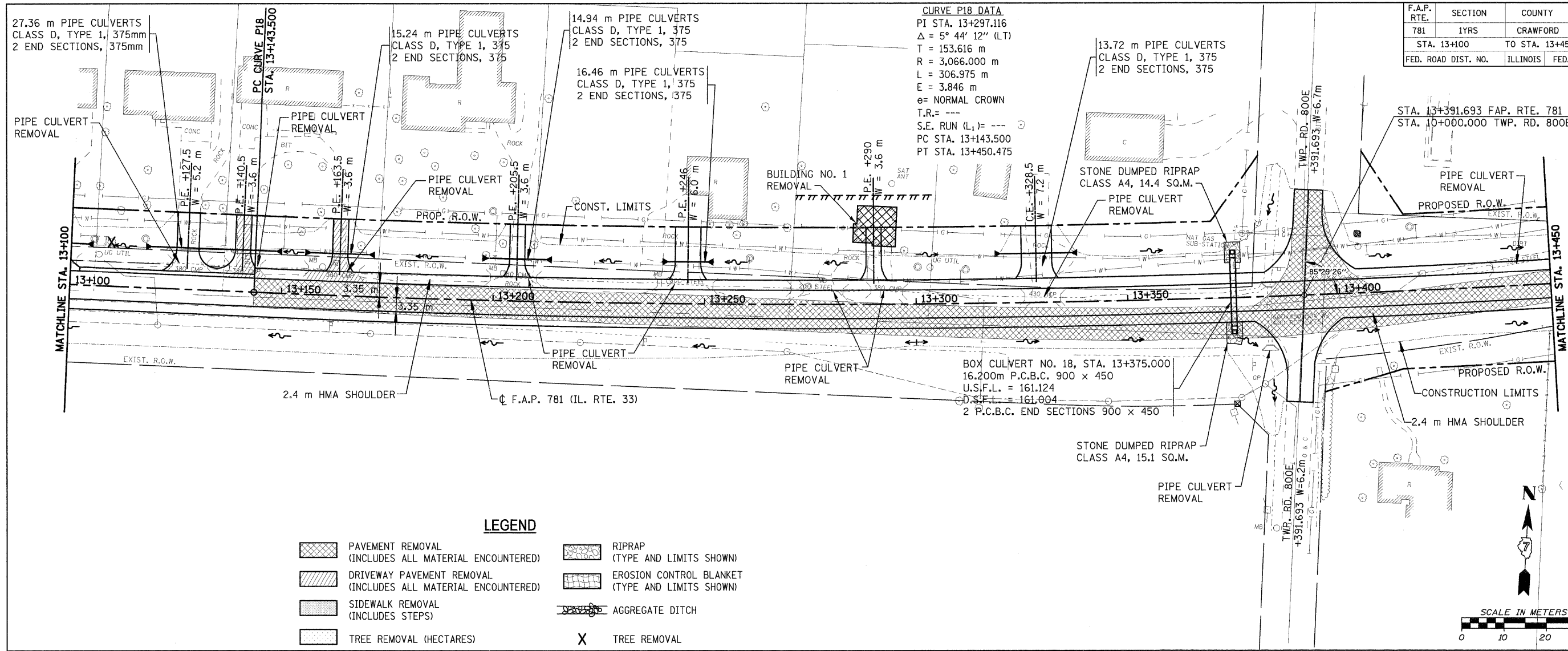
LEGEND

| | | | |
|--|--|--|--|
| | PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED) | | RIPRAP (TYPE AND LIMITS SHOWN) |
| | DRIVEWAY PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED) | | EROSION CONTROL BLANKET (TYPE AND LIMITS SHOWN) |
| | SIDEWALK REMOVAL (INCLUDES STEPS) | | AGGREGATE DITCH |
| | TREE REMOVAL (HECTARES) | | TREE REMOVAL |

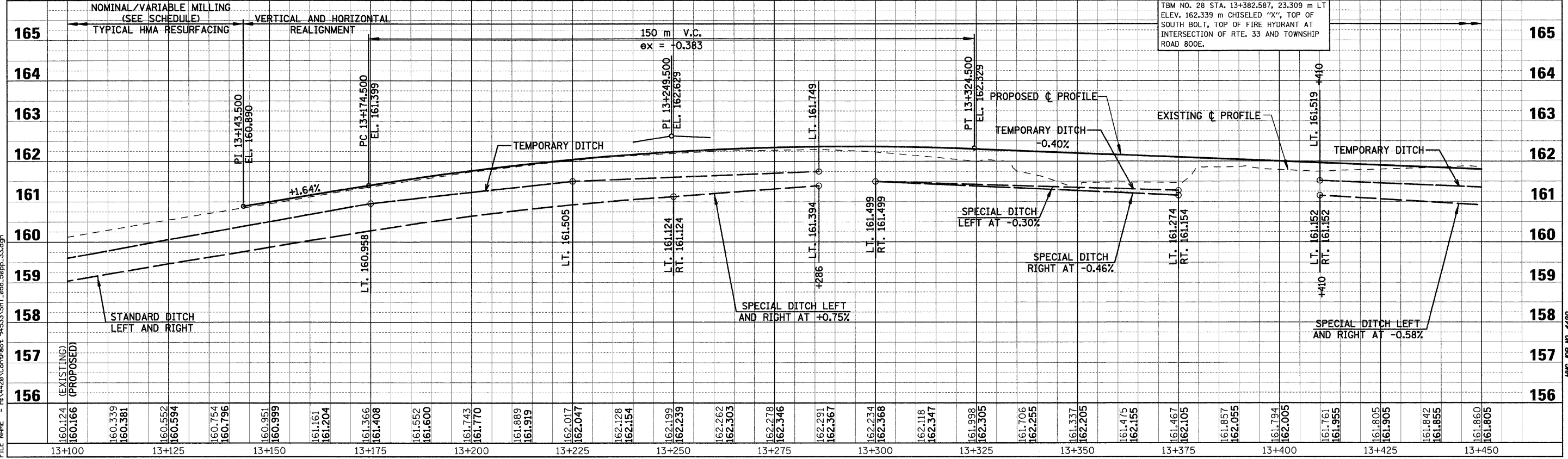


PLOT DATE = 12/22/2008
 FILE NAME = H:\4428\Cont-act_94533\SIT_057_50pp_32.dgn

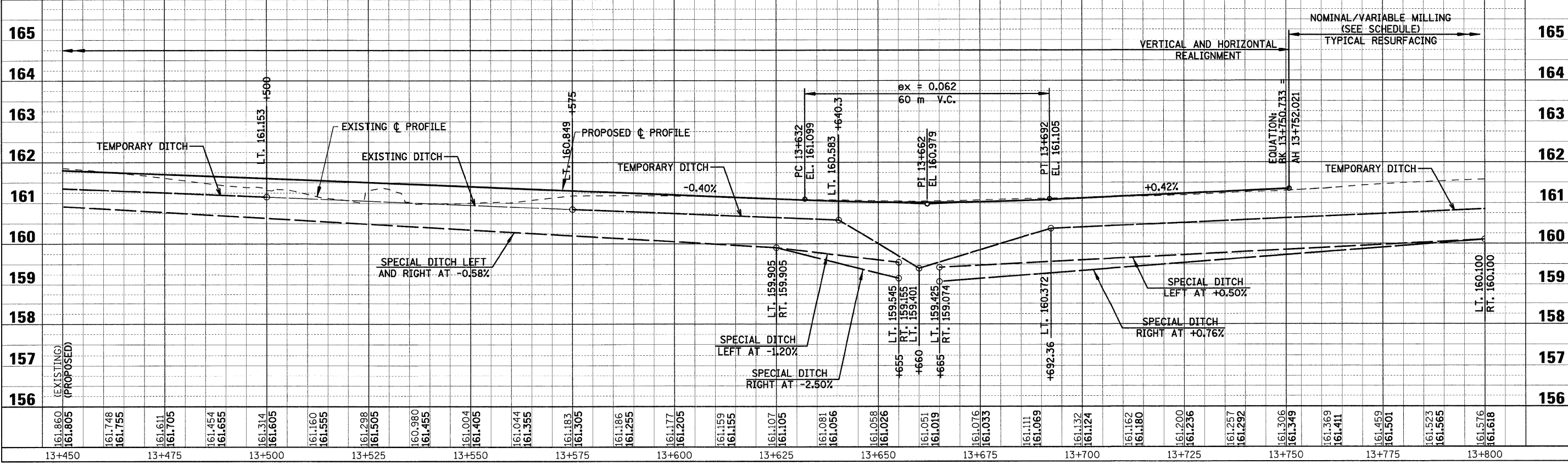
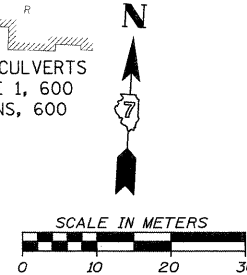
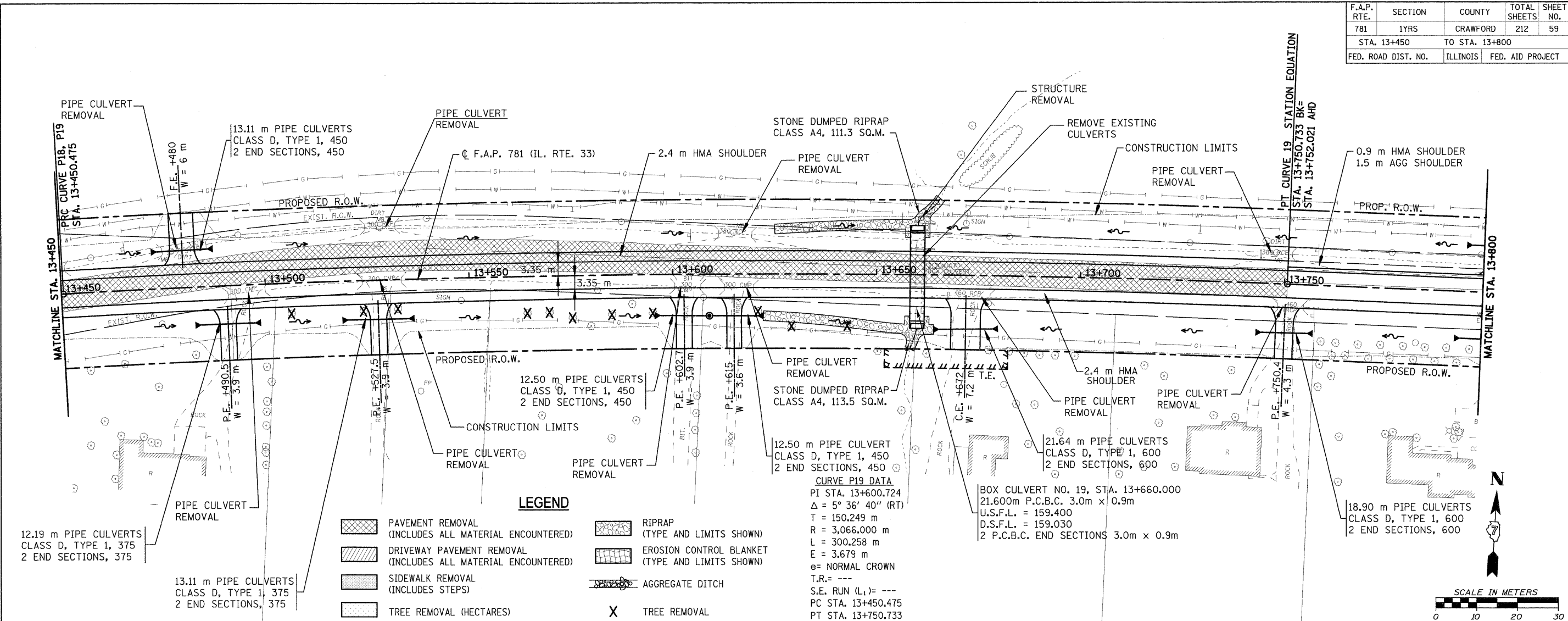
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 58 |
| STA. 13+100 | | TO STA. 13+450 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



CURVE P18 DATA
 PI STA. 13+297.116
 $\Delta = 5^\circ 44' 12''$ (LT)
 T = 153.616 m
 R = 3,066.000 m
 L = 306.975 m
 E = 3.846 m
 e = NORMAL CROWN
 T.R. = ---
 S.E. RUN (L₁) = ---
 PC STA. 13+143.500
 PT STA. 13+450.475



| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 59 |
| STA. 13+450 | | TO STA. 13+800 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

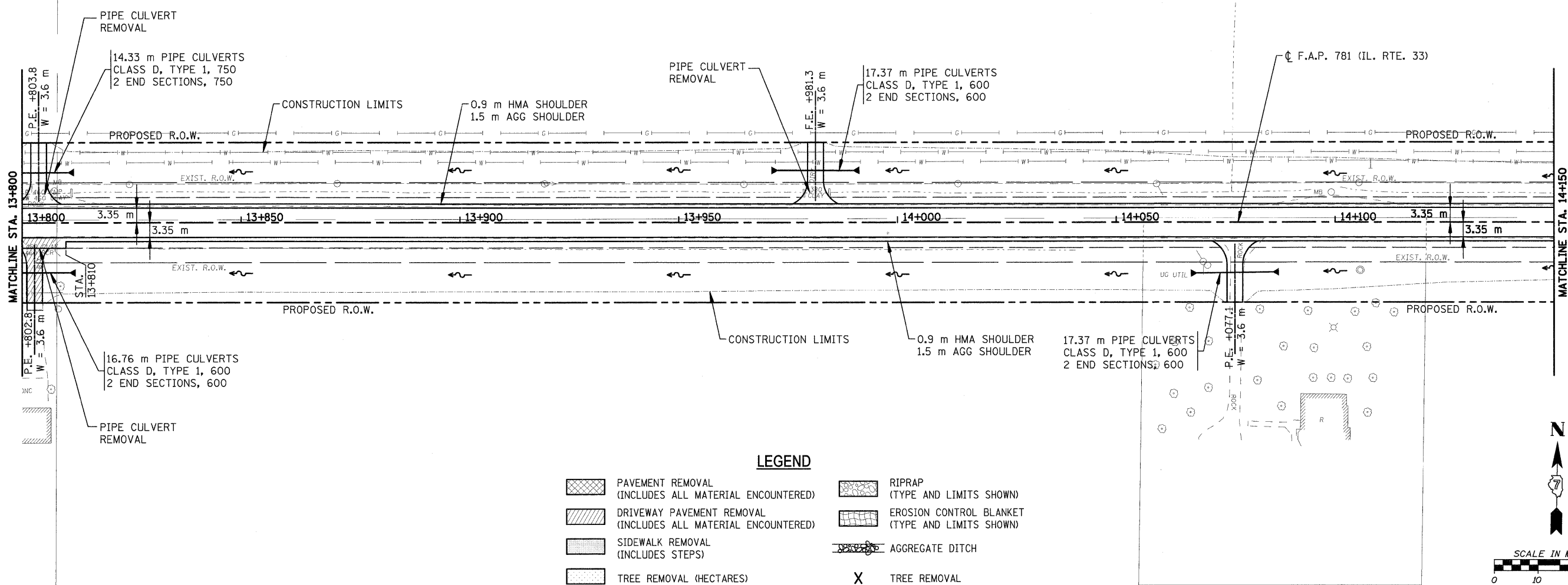


Sheet: 50pp_34.dgn
 Angle: 3.2509
 Chain: P_IL53
 12/22/2008

Plot Date = 12/22/2008
 File Name = P:\V\20\Contract\94533\SH1_089_50pp_34.dgn

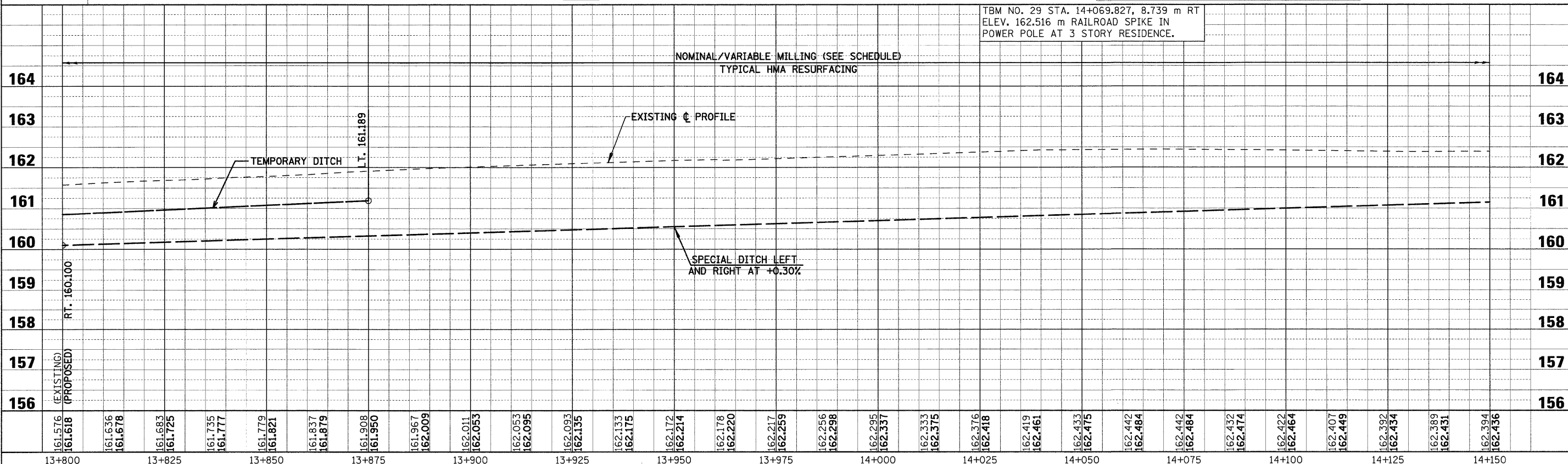
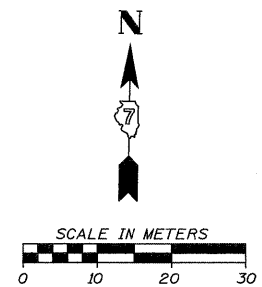
HMG JOB NO. 4420

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 60 |
| STA. 13+800 | | TO STA. 14+150 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



LEGEND

| | | | |
|--|---|--|---|
| | PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED) | | RIPRAP (TYPE AND LIMITS SHOWN) |
| | DRIVEWAY PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED) | | EROSION CONTROL BLANKET (TYPE AND LIMITS SHOWN) |
| | SIDEWALK REMOVAL (INCLUDES STEPS) | | AGGREGATE DITCH |
| | TREE REMOVAL (HECTARES) | | TREE REMOVAL |



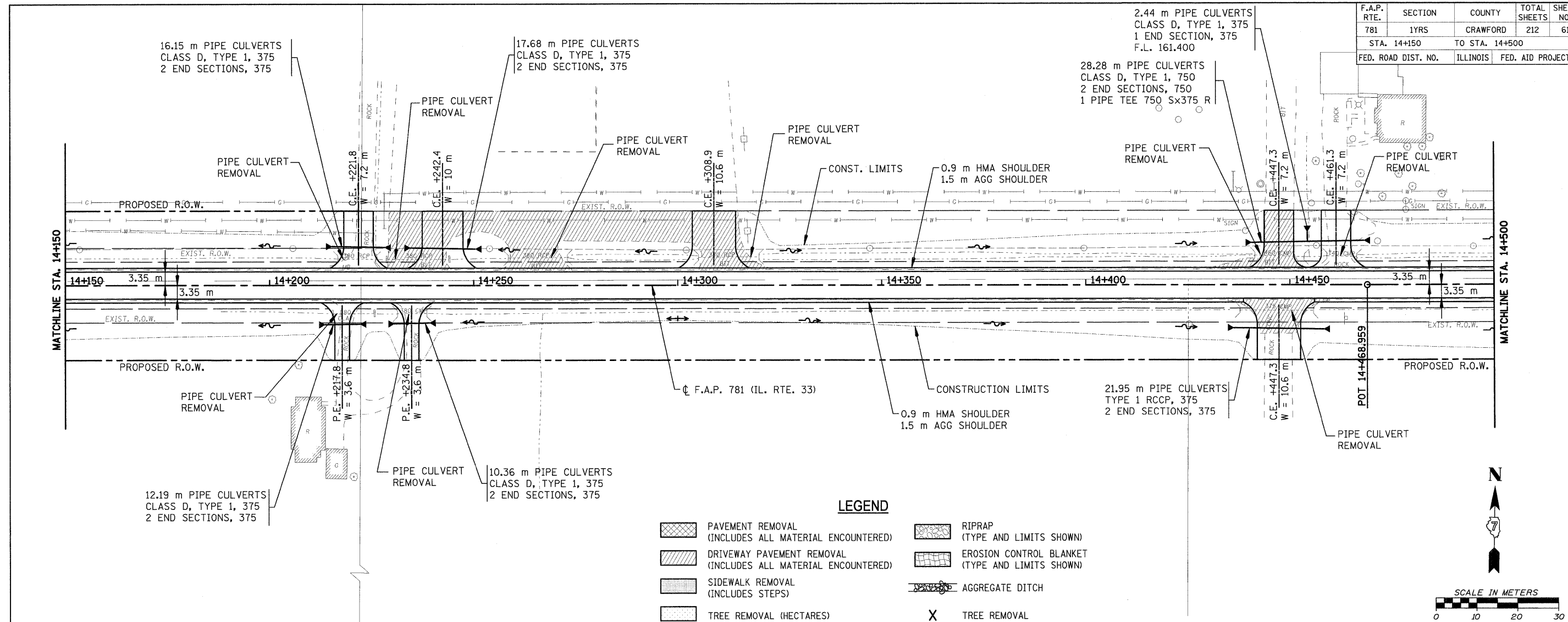
TBM NO. 29 STA. 14+069.827, 8.739 m RT ELEV. 162.516 m RAILROAD SPIKE IN POWER POLE AT 3 STORY RESIDENCE.

PLOT DATE = 12/22/2006
FILE NAME = H:\1420\Contract 94533_SHT_060_56pp_35.dgn

Sheet: 35
Angle: 0.9431
Chain: P_IL33

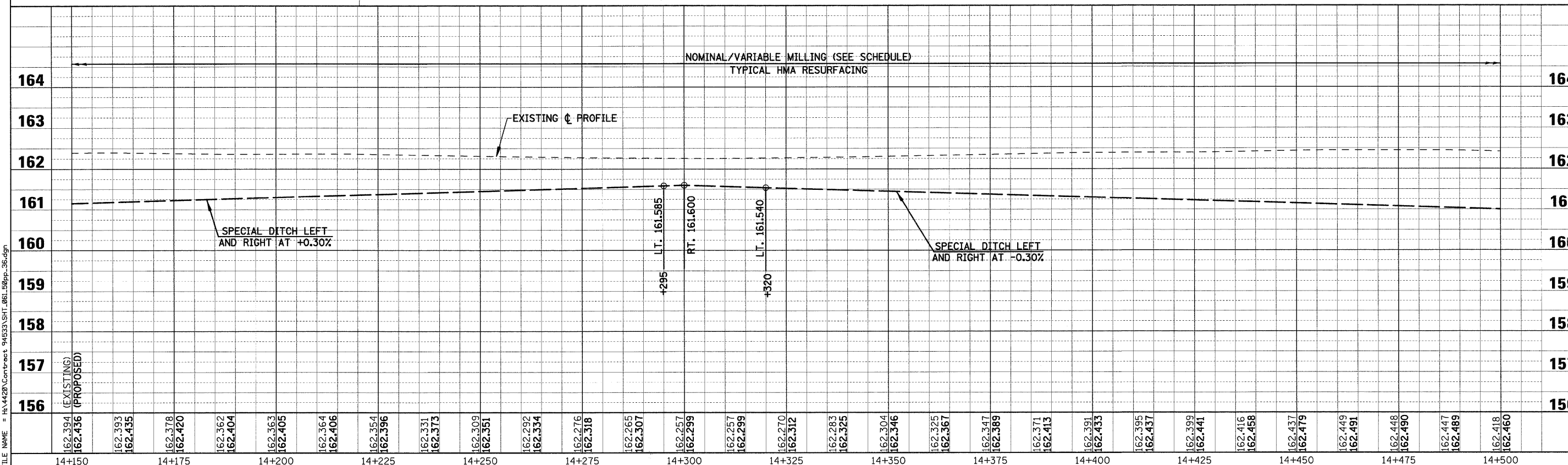
HMG JOB NO. 4420

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 61 |
| STA. 14+150 | | TO STA. 14+500 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



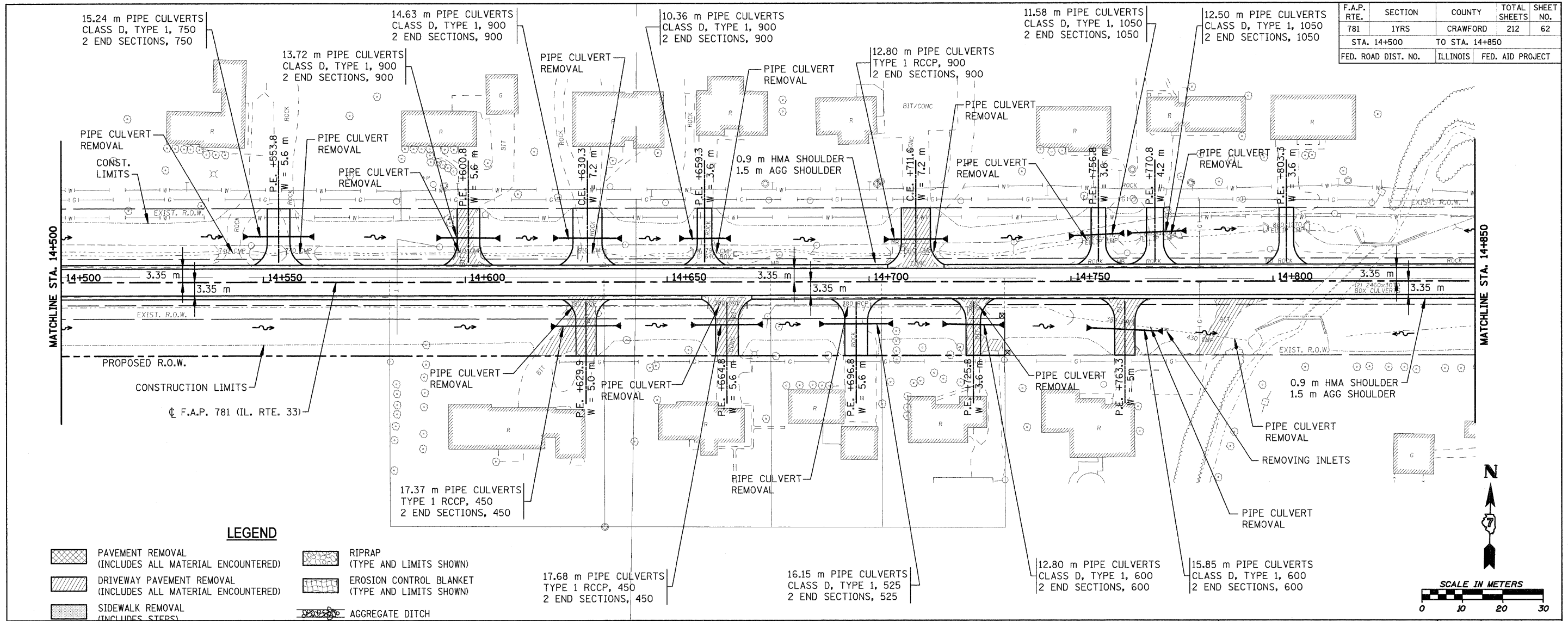
LEGEND

| | | | |
|--|--|--|--|
| | PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED) | | RIPRAP (TYPE AND LIMITS SHOWN) |
| | DRIVEWAY PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED) | | EROSION CONTROL BLANKET (TYPE AND LIMITS SHOWN) |
| | SIDEWALK REMOVAL (INCLUDES STEPS) | | AGGREGATE DITCH |
| | TREE REMOVAL (HECTARES) | | TREE REMOVAL |



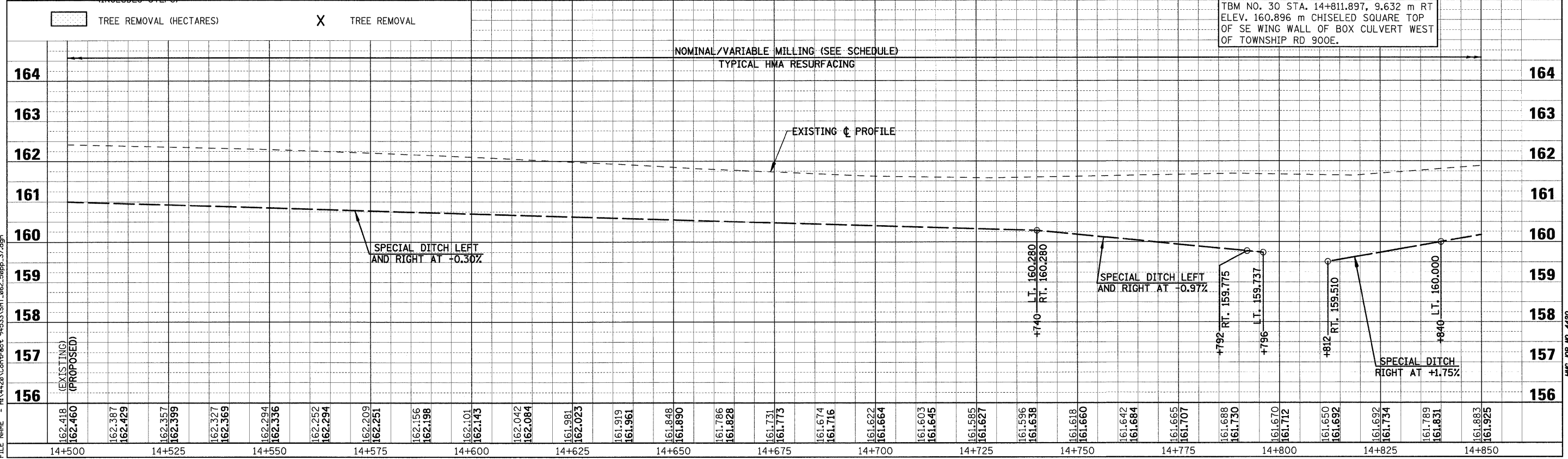
PLOT DATE = 12/22/2008
 FILE NAME = H:\V\20\Contract\94533\SH1_061_50pp_36.dgn

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------------|--------------|------------------|
| 781 | | CRAWFORD | 212 | 62 |
| STA. 14+500 | | TO STA. 14+850 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | | FED. AID PROJECT |



LEGEND

- PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED)
- DRIVEWAY PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED)
- SIDEWALK REMOVAL (INCLUDES STEPS)
- TREE REMOVAL (HECTARES)
- RIPRAP (TYPE AND LIMITS SHOWN)
- EROSION CONTROL BLANKET (TYPE AND LIMITS SHOWN)
- AGGREGATE DITCH
- TREE REMOVAL

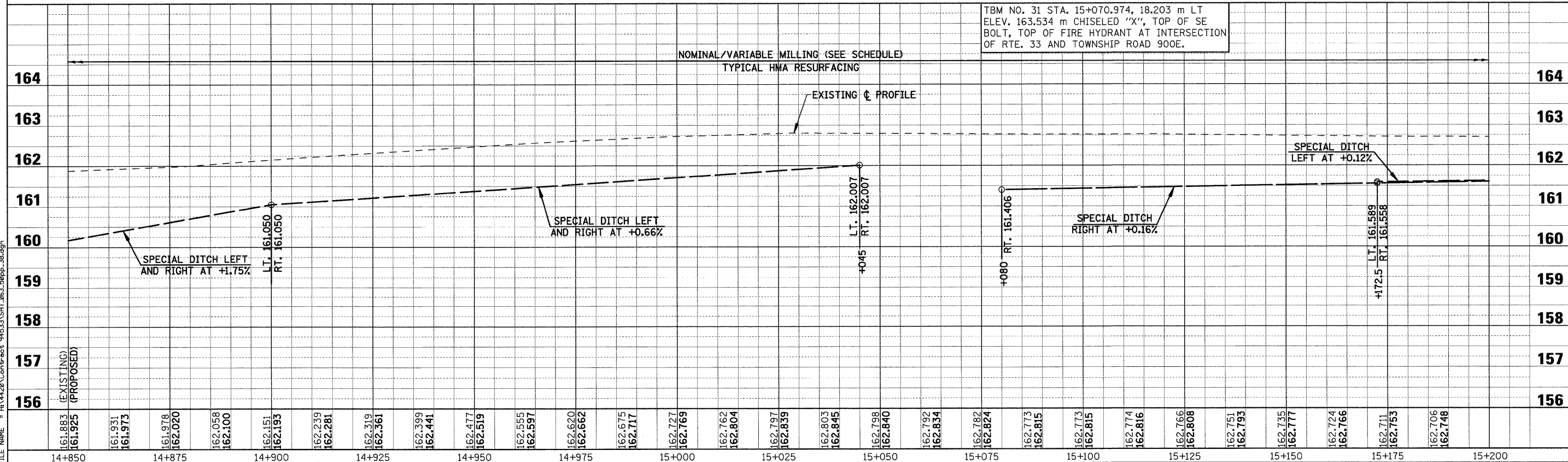
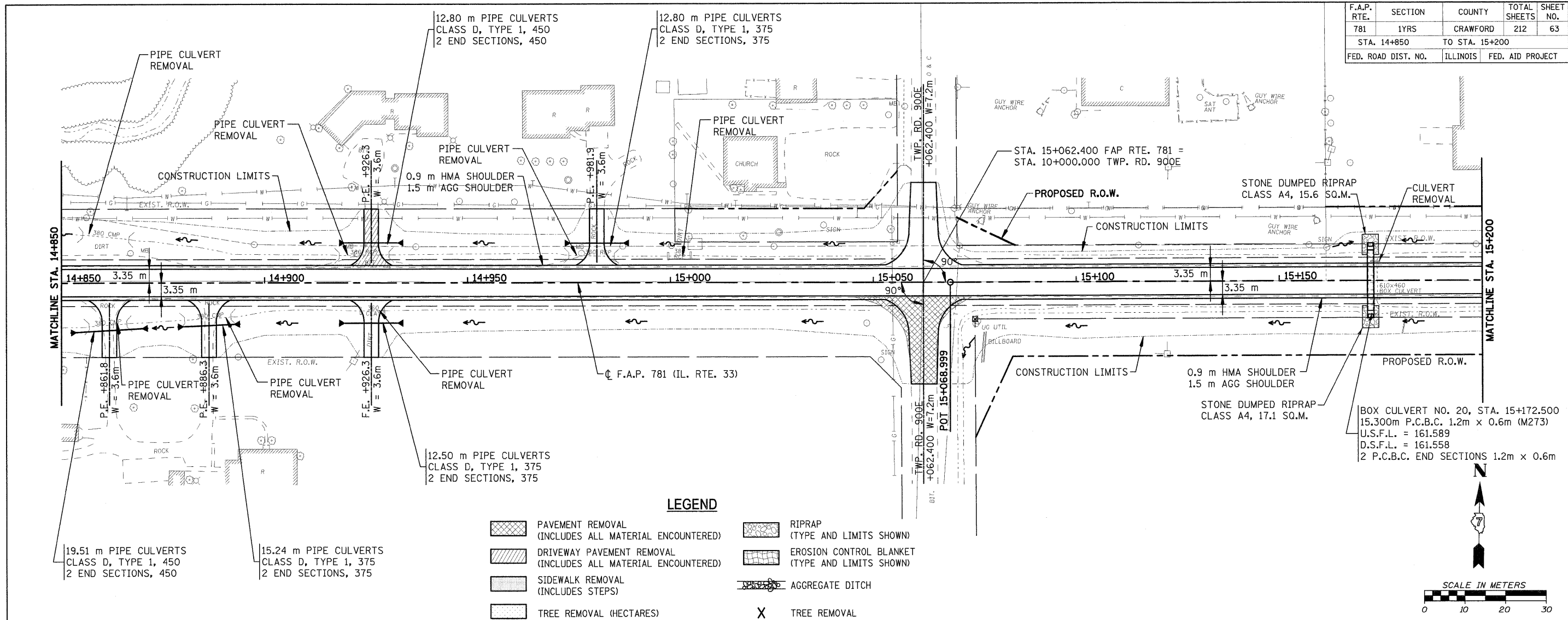


TBM NO. 30 STA. 14+811.897, 9.632 m RT
ELEV. 160.896 m CHISELED SQUARE TOP
OF SE WING WALL OF BOX CULVERT WEST
OF TOWNSHIP RD 900E.

PLOT DATE = 12/22/2008
 FILE NAME = H:\4420\Contract\94533\SHT_062_50ppp_37.dgn

HMG JOB NO. 4420

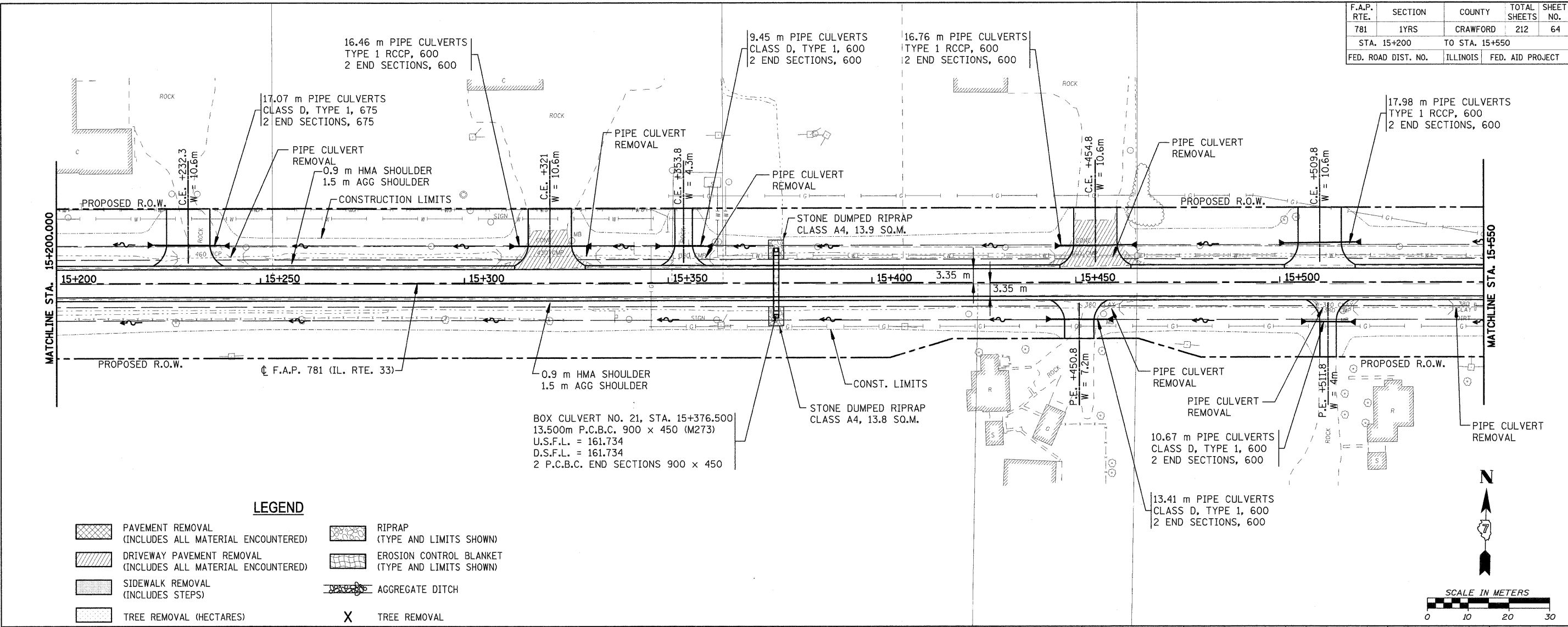
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------------|------------------|-----------|
| 781 | | CRAWFORD | 212 | 63 |
| STA. 14+850 | | TO STA. 15+200 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



TBM NO. 31 STA. 15+070.974, 18.203 m LT ELEV. 163.534 m CHISELED "X", TOP OF SE BOLT, TOP OF FIRE HYDRANT AT INTERSECTION OF RTE. 33 AND TOWNSHIP ROAD 900E.

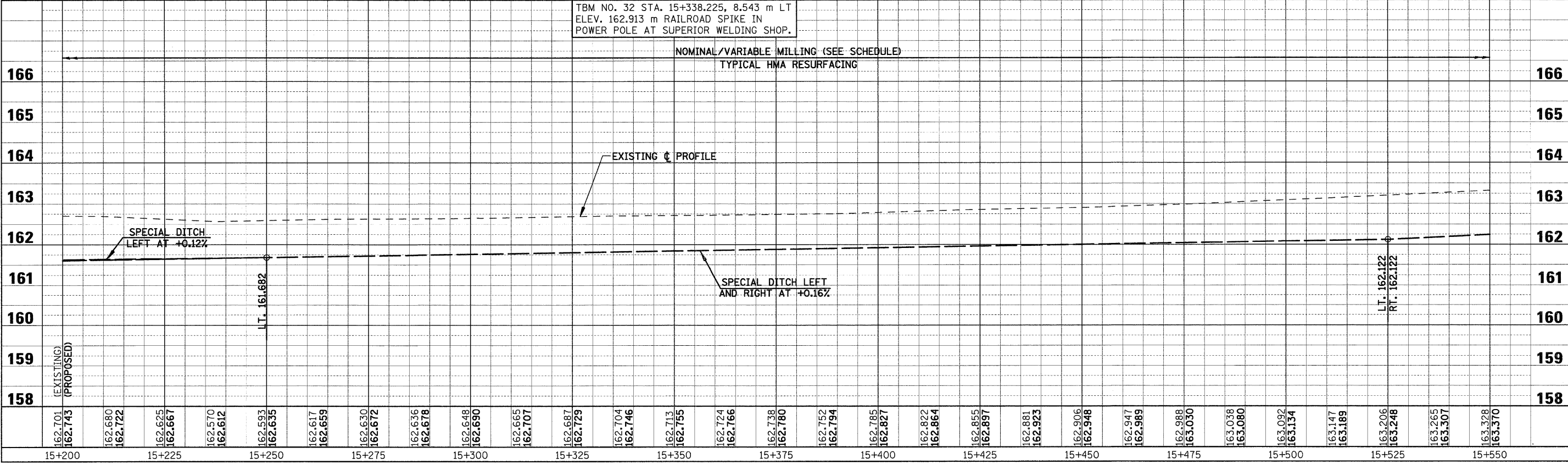
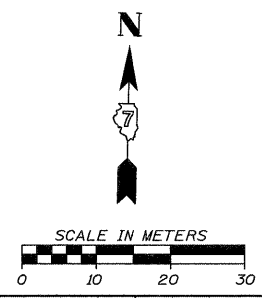
PLOT DATE = 12/22/2008
FILE NAME = H:\442\Contract\94533\SH1_063_50pp_30.dgn

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|----------------------------|---------|----------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 64 |
| STA. 15+200 TO STA. 15+550 | | | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



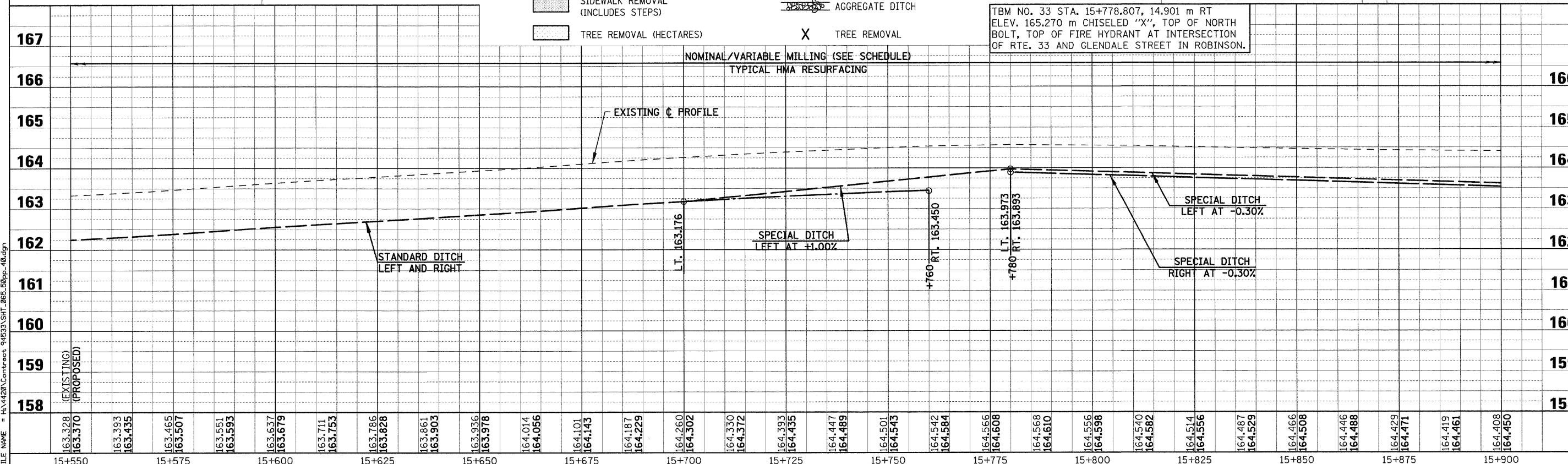
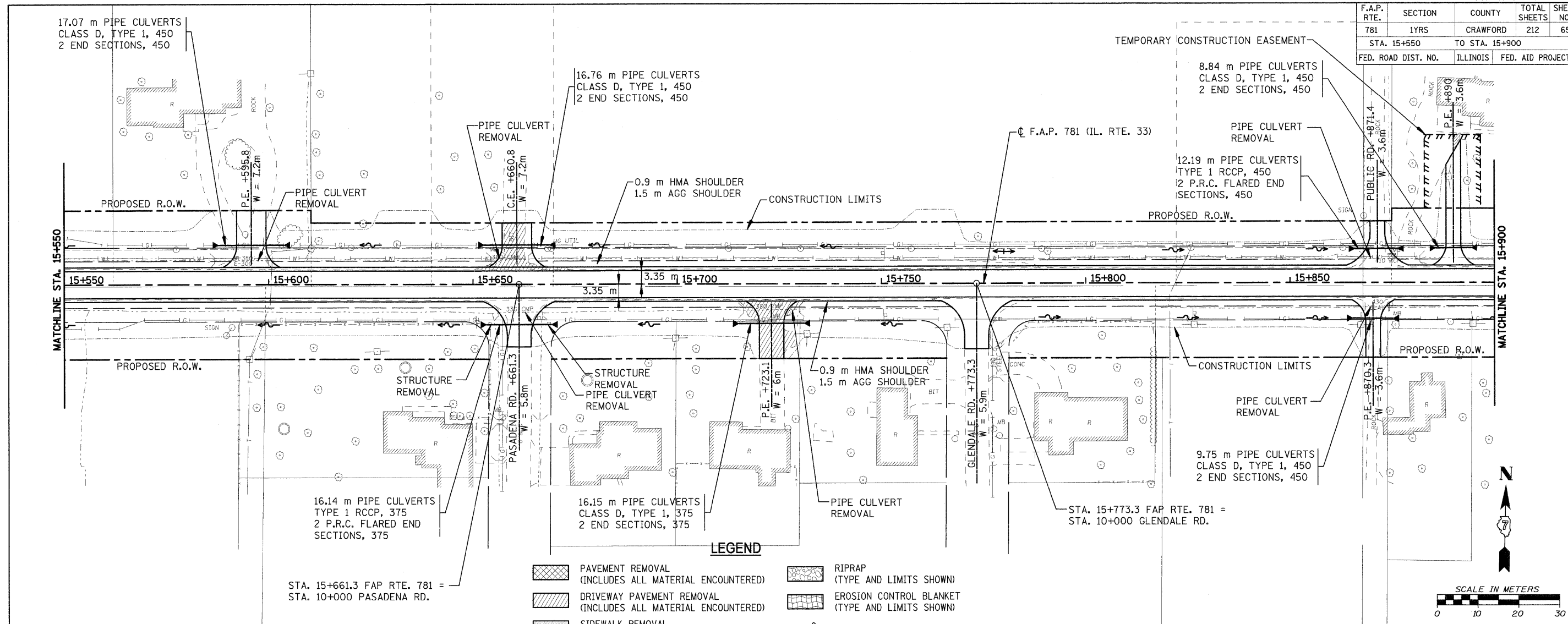
LEGEND

- PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED)
- DRIVEWAY PAVEMENT REMOVAL (INCLUDES ALL MATERIAL ENCOUNTERED)
- SIDEWALK REMOVAL (INCLUDES STEPS)
- TREE REMOVAL (HECTARES)
- RIPRAP (TYPE AND LIMITS SHOWN)
- EROSION CONTROL BLANKET (TYPE AND LIMITS SHOWN)
- AGGREGATE DITCH
- TREE REMOVAL



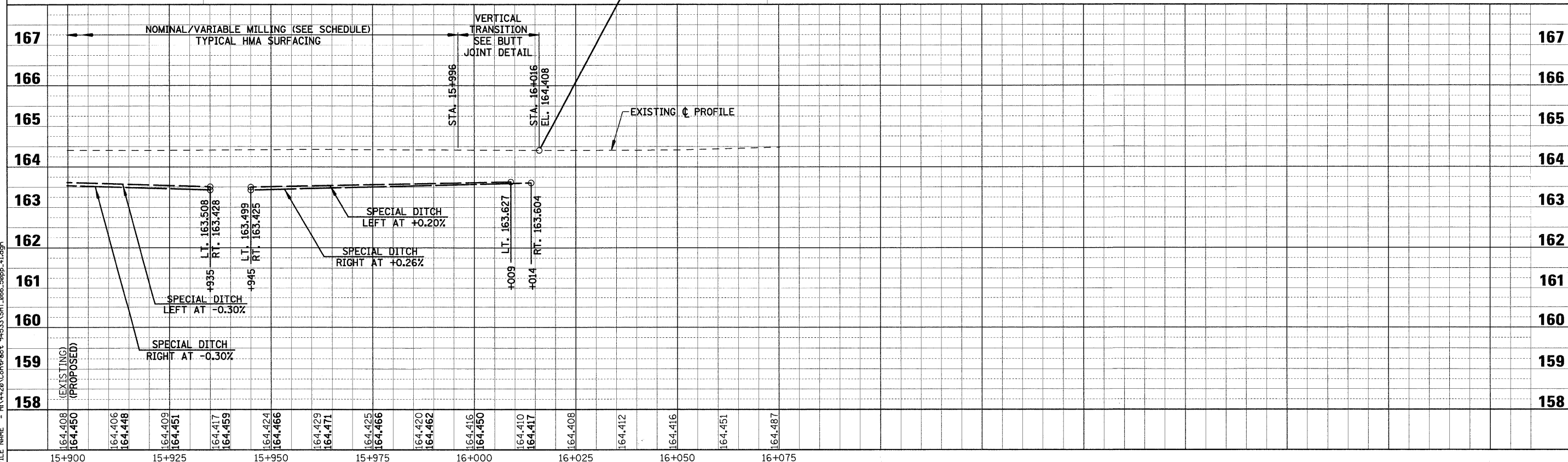
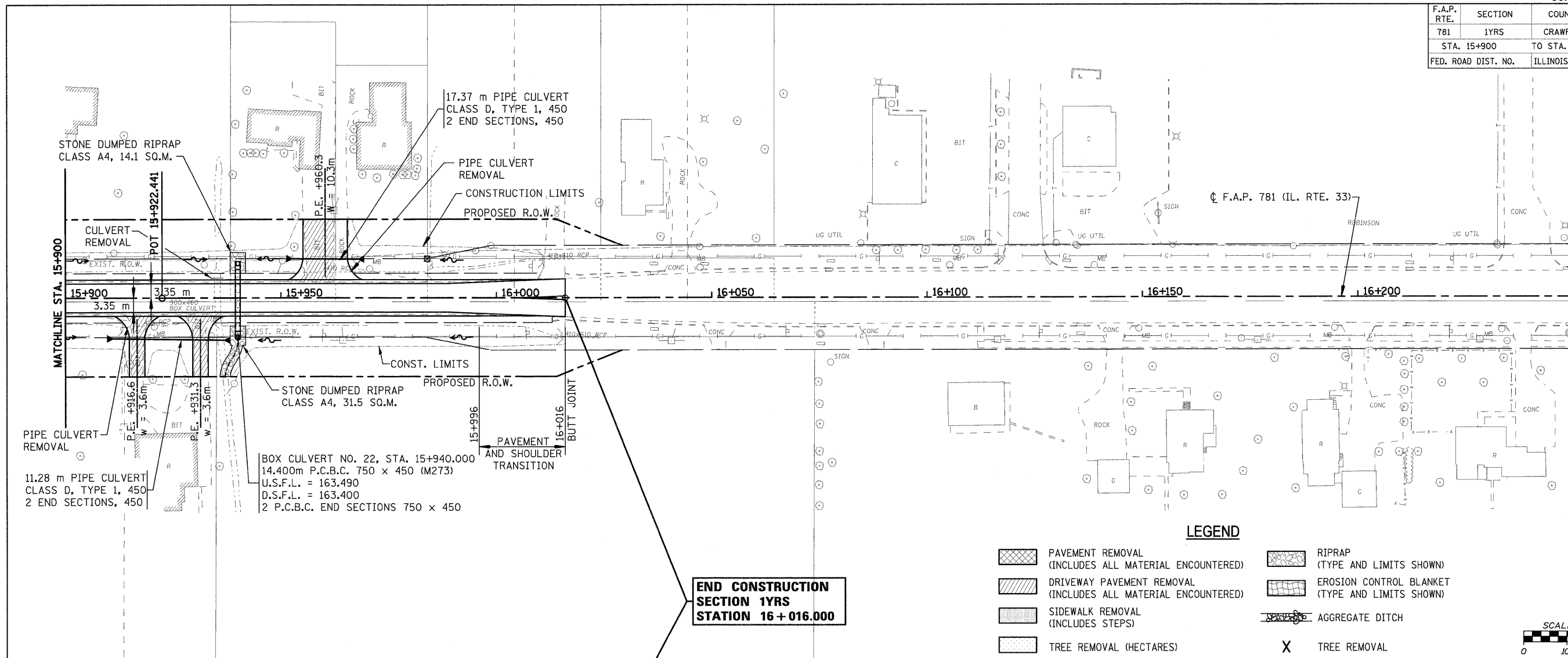
PLOT DATE = 12/22/2008
 FILE NAME = H:\V\20\Contract\sect 94533\SHT_064_50app_39.dgn

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|------------------|--------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 65 |
| STA. 15+550 | | TO STA. 15+900 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



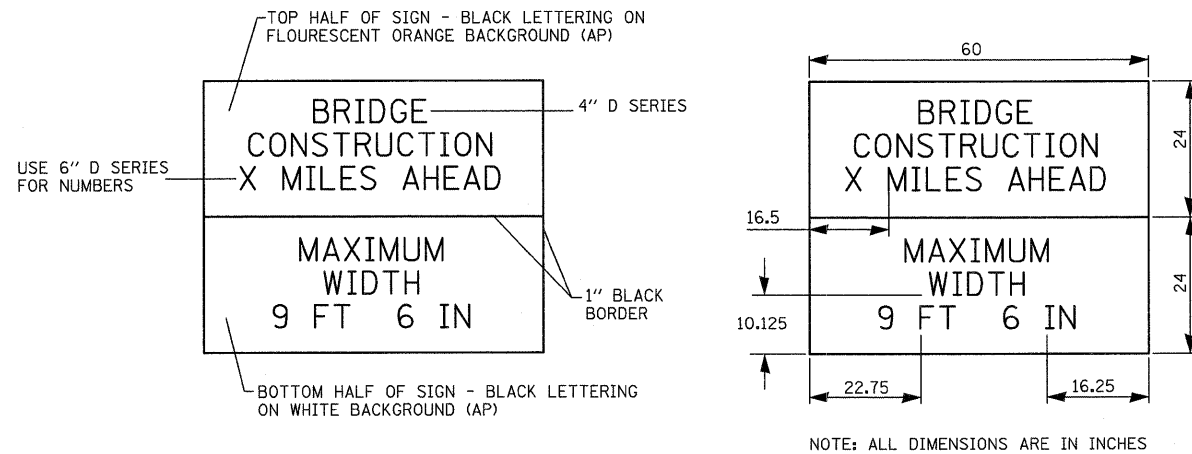
PLOT DATE = 12/22/2008
 FILE NAME = H:\4422\Contra\cont_94533\SHT_265.dwg

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|------------------|--------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 66 |
| STA. 15+900 | | TO STA. 16+016 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

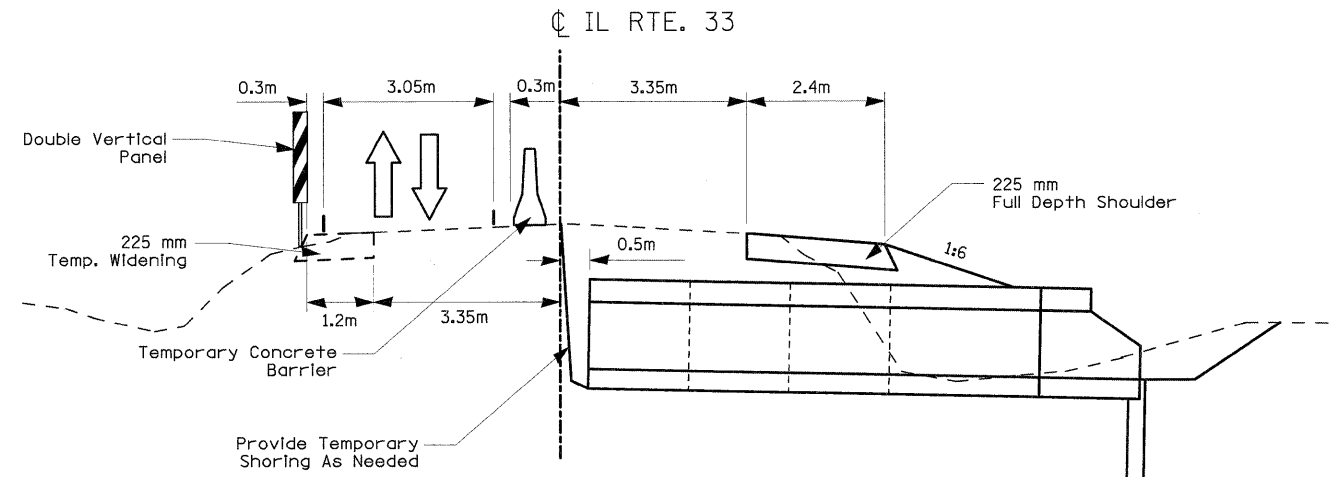


PLOT DATE = 12/22/2008
 FILE NAME = H:\V420\Contract 94533\SHR_066_58pp_41.dgn

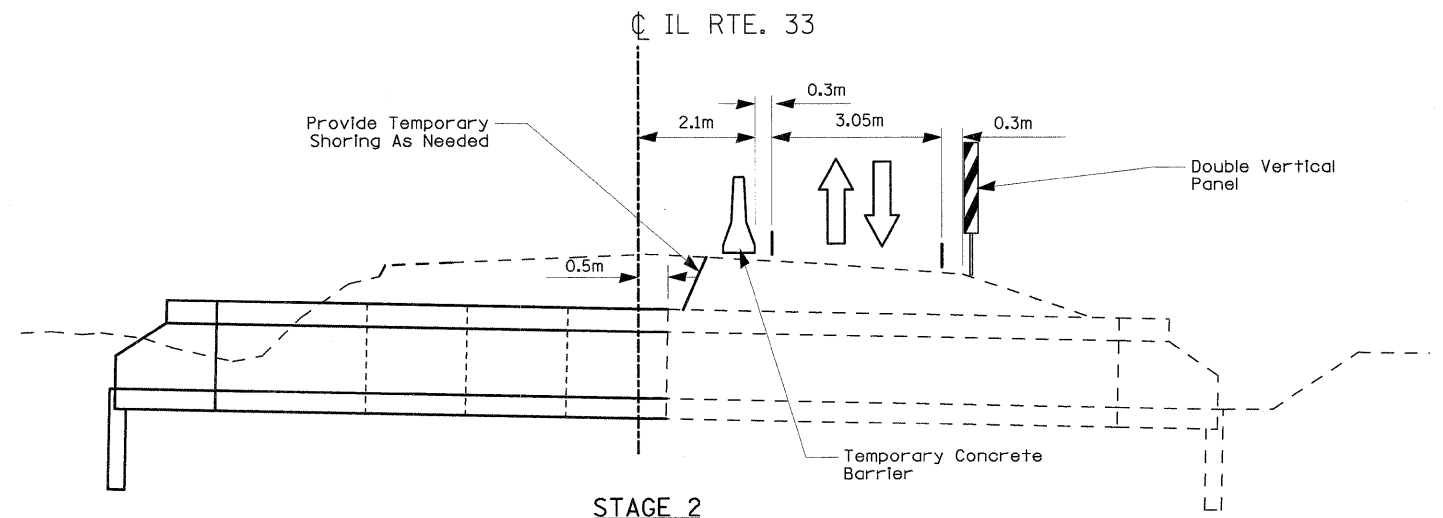
| | | | | |
|---------------------|----------|------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 67 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



WIDTH RESTRICTION SIGN



STAGE 1



STAGE 2

TRAFFIC CONTROL NOTES:

THE CONTRACTOR SHALL UTILIZE TRAFFIC CONTROL STANDARD 701321 DURING EXECUTION OF THIS WORK. TEMPORARY BRIDGE TRAFFIC SIGNALS, TEMPORARY RUMBLE STRIPS, TEMPORARY CONCRETE BARRIER, AND IMPACT ATTENUATORS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR SAID ITEMS, ALL OTHER STRIPING, STRIPING REMOVAL, SIGNAGE, ETC. REQUIRED BY THE STANDARD SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER EACH FOR TRAFFIC CONTROL AND PROTECTION, STANDARD 701321 (SPECIAL) ONCE FOR EACH CULVERT LOCATION. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR ANY CHANGES OR ADJUSTMENTS TO THE TRAFFIC CONTROL DURING THE CONVERSION TO STAGE 2 OF THE WORK.

THE CONTRACTOR SHALL SUPPLY, SET, AND MAINTAIN TWO (2) WIDTH RESTRICTION WARNING SIGNS, AS SHOWN IN THE DETAIL, PRIOR TO COMMENCING WORK UNLESS THEY ARE ALREADY IN PLACE FOR OTHER ITEMS OF WORK. THE SIGNS SHALL BE PLACED AT THE INTERSECTIONS OF IL 33 AND IL 49 AND IL 33 AND IL 1, OR OTHER LOCATION, AS DIRECTED BY THE ENGINEER.

ALL LABOR AND MATERIALS REQUIRED TO SUPPLY, SET, AND MAINTAIN THESE SIGNS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER EACH FOR TRAFFIC CONTROL AND PROTECTION, STANDARD 701321 (SPECIAL).

DOUBLE VERTICAL PANELS, AS SHOWN IN THE DETAIL, SHALL BE USED IF GUARDRAIL IS NOT PRESENT. ALL LABOR AND MATERIALS REQUIRED TO SUPPLY, SET, AND MAINTAIN THE PANELS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER EACH FOR TRAFFIC CONTROL AND PROTECTION, STANDARD 701321 (SPECIAL).

THE CONTRACT QUANTITIES INCLUDE 100 METERS OF TEMPORARY CONCRETE BARRIER AND TWO (2) IMPACT ATTENUATORS FOR EACH CULVERT LOCATION.

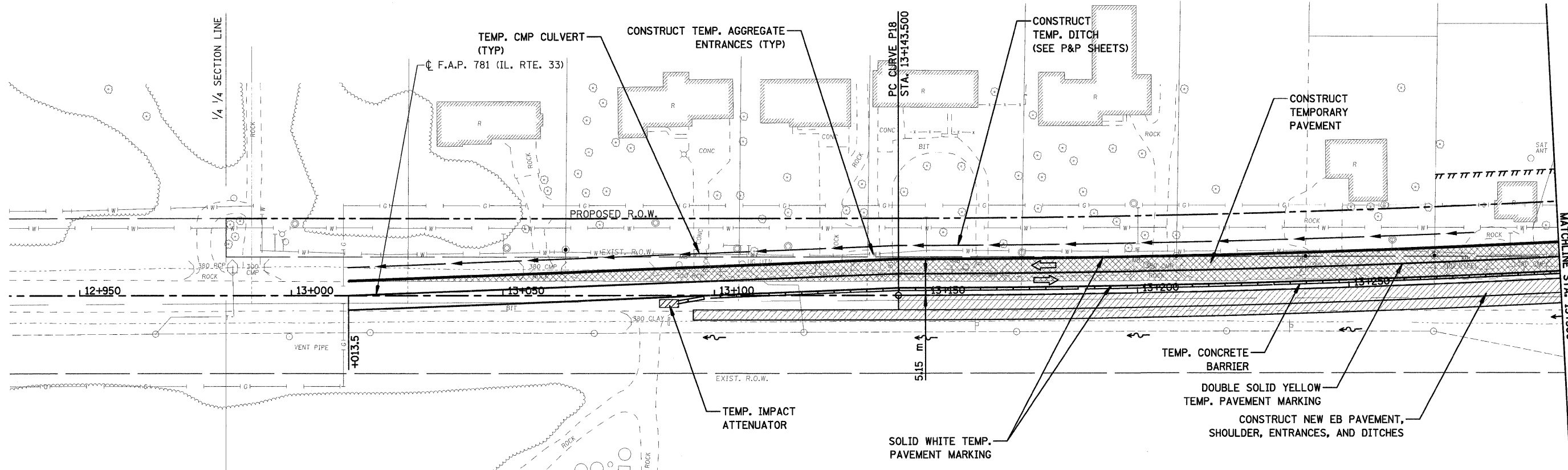
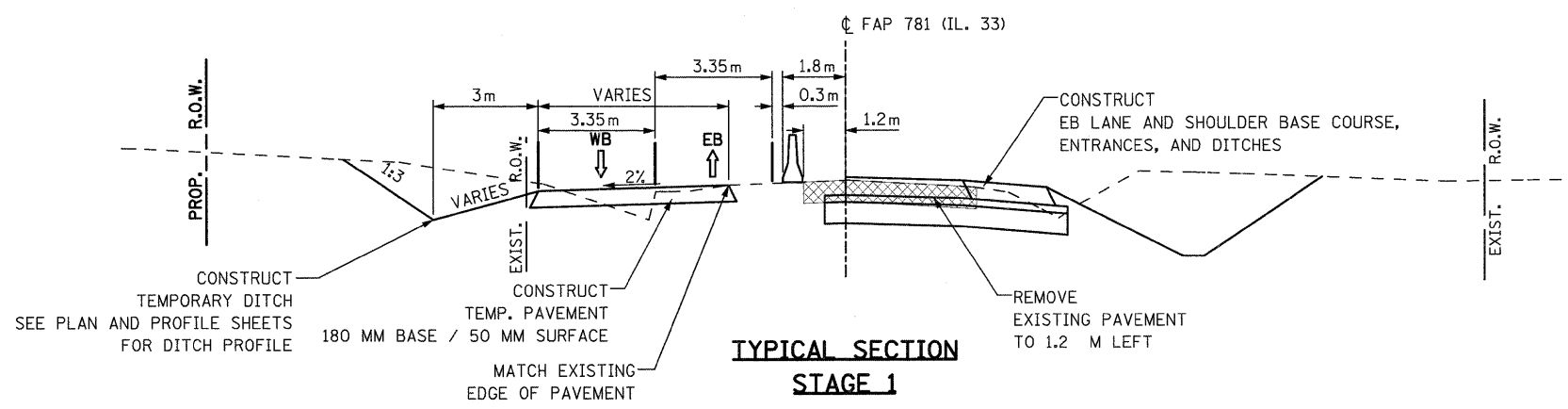
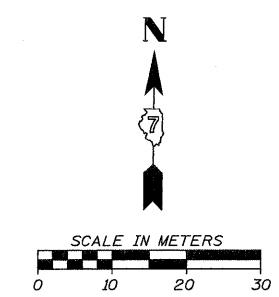
| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
CULVERT REPLACEMENT AND INSTALLATION
STAGING PLAN
 SCALE NO SCALE DRAWN BY EDW
 DATE DECEMBER 22, 2008 CHECKED BY LWJ

PLOT DATE = 12/22/2008
 FILE NAME = P:\1420\Contract 94533\SH1_067_rural_culvert_staging.dgn

H. M. & G. NO. 4420

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------|---------|---|--------------|-----------|
| 781 | LYRS | CRAWFORD | 212 | 68 |
| STA. TO STA. | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | |

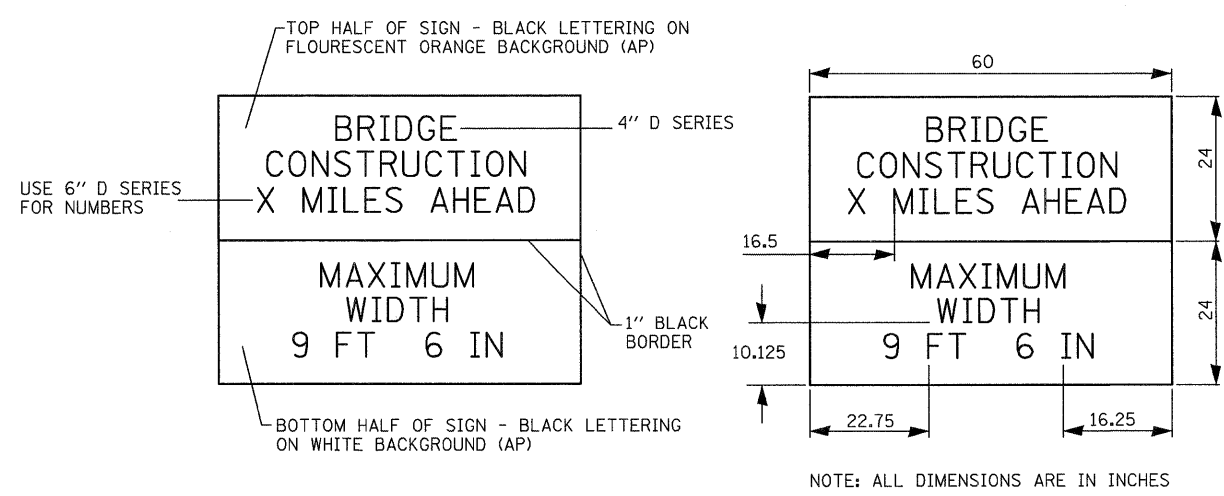


TRAFFIC CONTROL NOTES:

THE CONTRACTOR SHALL UTILIZE TRAFFIC CONTROL STANDARD 701331 DURING EXECUTION OF THIS WORK. TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATORS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR SAID ITEMS, ALL OTHER STRIPING, SIGNAGE, ETC. REQUIRED BY THE STANDARD SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER EACH FOR TRAFFIC CONTROL AND PROTECTION, STANDARD 701331 (SPECIAL). ALL THREE STAGES OF CONSTRUCTION SHALL CONSTITUTE ONE (1) OCCURANCE OF THE STANDARD. NO ADDITIONAL COMPENSATION, OTHER THAN RELOCATION OF CONCRETE BARRIER AND IMPACT ATTENUATORS WILL BE ALLOWED FOR ANY CHANGES OR ADJUSTMENTS TO THE TRAFFIC CONTROL DURING THE CONVERSION FROM ONE STAGE TO THE NEXT. LIGHTS ON BARRELS OR BARRICADES SHALL BE BIDIRECTIONAL AND STEADY BURN, EXCEPT, LIGHTS ON THE TAPERS SHALL BE MONODIRECTIONAL.

THE CONTRACTOR SHALL SUPPLY, SET, AND MAINTAIN TWO (2) WIDTH RESTRICTION WARNING SIGNS, AS SHOWN IN THE DETAIL, PRIOR TO COMMENCING WORK UNLESS THEY ARE ALREADY IN PLACE DUE TO CULVERT CONSTRUCTION. THE SIGNS SHALL BE PLACED AT THE INTERSECTIONS OF IL 33 AND IL 49 AND IL 33 AND IL 1, OR OTHER LOCATION, AS DIRECTED BY THE ENGINEER. ALL LABOR AND MATERIALS REQUIRED TO SUPPLY, SET, AND MAINTAIN THESE SIGNS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER EACH FOR TRAFFIC CONTROL AND PROTECTION, STANDARD 701331 (SPECIAL).

CONSTRUCTION AND SUBSEQUENT REMOVAL OF THE TEMPORARY EMBANKMENT, TEMPORARY PAVEMENT, AND TEMPORARY ENTRANCES, INCLUDING CULVERTS WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR SAID ITEMS. EXCAVATION AND GRADING FOR THE TEMPORARY DITCH AND TEMPORARY PAVEMENT WILL NOT BE PAID FOR SEPARATELY, AS THE VOLUMES ARE ACCOUNTED FOR IN THE "EARTH EXCAVATION" AND "EARTH EXCAVATION (WIDENING)" PAY ITEMS.



STAGING NOTES:

1. CONSTRUCT TEMPORARY AGGREGATE ENTRANCES WITH CMP CULVERTS AS NEEDED TO MAINTAIN ACCESS.
2. TWO-WAY / TWO-LANE TRAFFIC SHALL BE RESTORED AT THE END OF THE WORK DAY. TWO-WAY / ONE-LANE TRAFFIC SHALL BE USED ON A LIMITED BASIS, WHEN REQUIRED, UTILIZING THE APPROPRIATE STANDARD.
3. FINAL HMA SURFACE COURSE SHALL BE PLACED THROUGHOUT THE PROJECT LIMITS AS A SEPARATE STAGE.



PLOT DATE = 12/22/2006 FILE NAME = H:\V4\20\Contract\94533\SH1_068.dwg

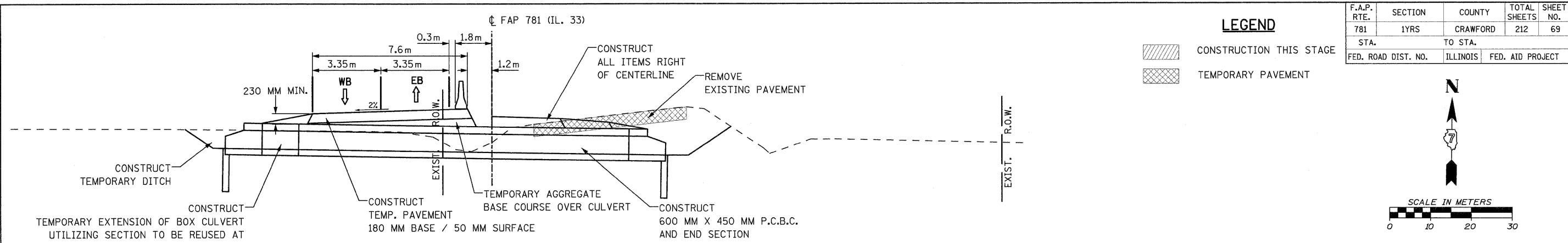
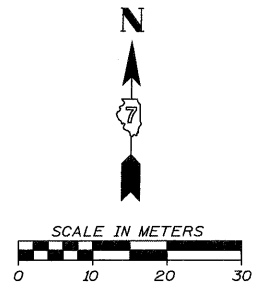
HMG JOB NO. 4420

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|---------|----------|--------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 69 |

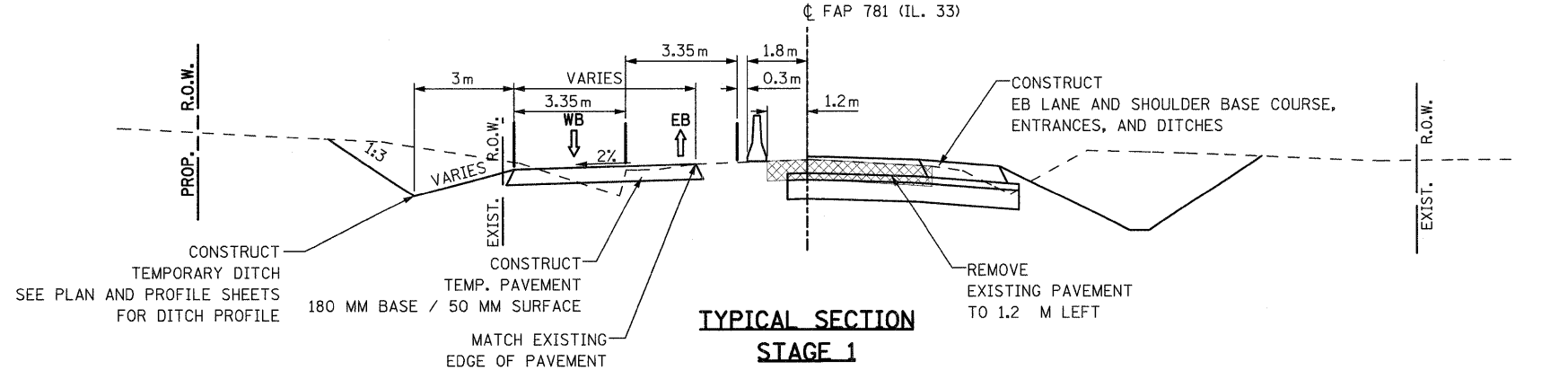
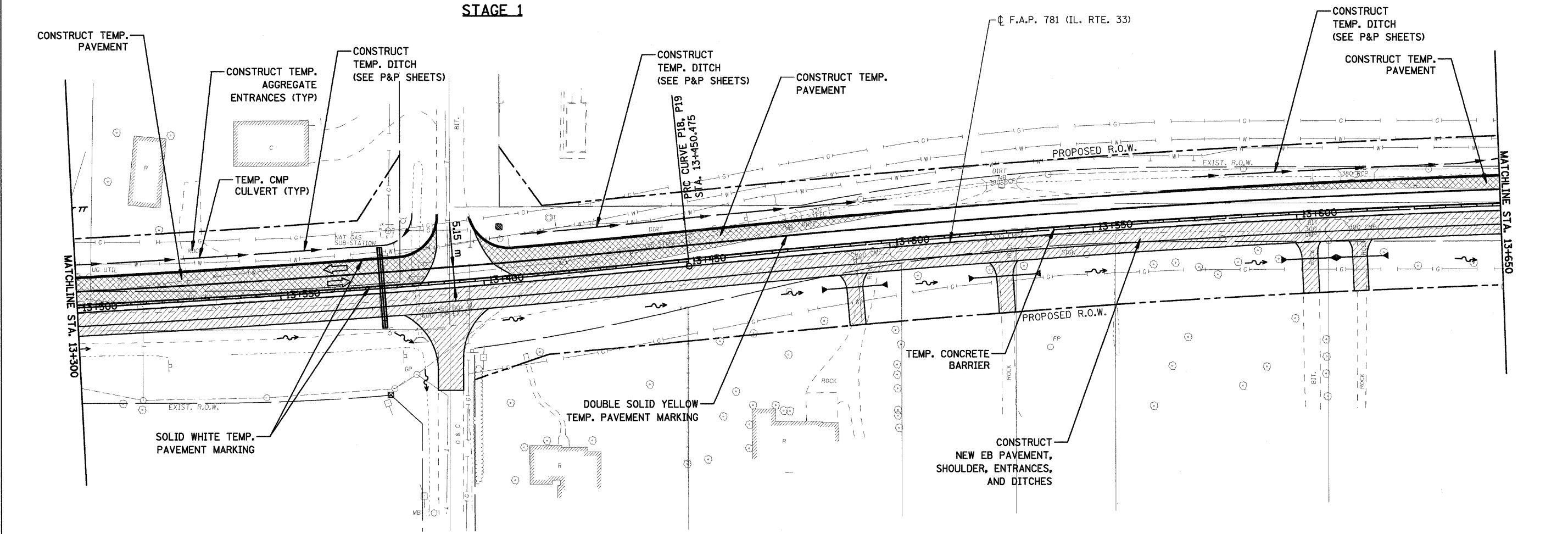
| STA. | TO STA. | FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT |
|------|---------|---------------------|----------|------------------|
| | | | | |

LEGEND

-  CONSTRUCTION THIS STAGE
-  TEMPORARY PAVEMENT



**BOX CULVERT STA. 13+375
STAGE 1**



STAGING NOTES:



1. CONSTRUCT TEMPORARY AGGREGATE ENTRANCES WITH CMP CULVERTS AS NEEDED TO MAINTAIN ACCESS.
2. TWO-WAY / TWO-LANE TRAFFIC SHALL BE RESTORED AT THE END OF THE WORK DAY. TWO-WAY / ONE-LANE TRAFFIC SHALL BE USED ON A LIMITED BASIS, WHEN REQUIRED, UTILIZING THE APPROPRIATE STANDARD.
3. FINAL HMA SURFACE COURSE SHALL BE PLACED THROUGHOUT THE PROJECT LIMITS AS A SEPARATE STAGE.

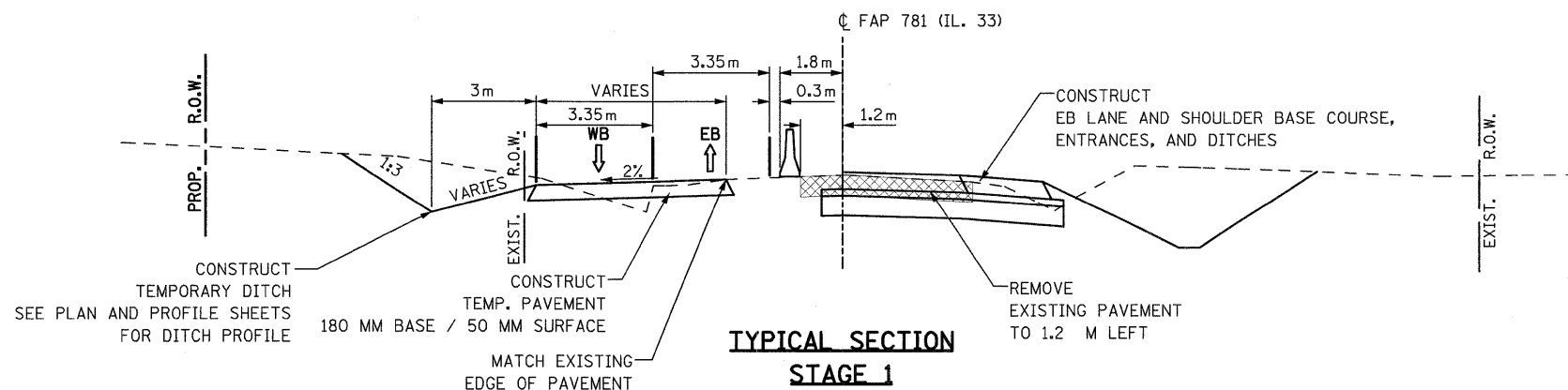
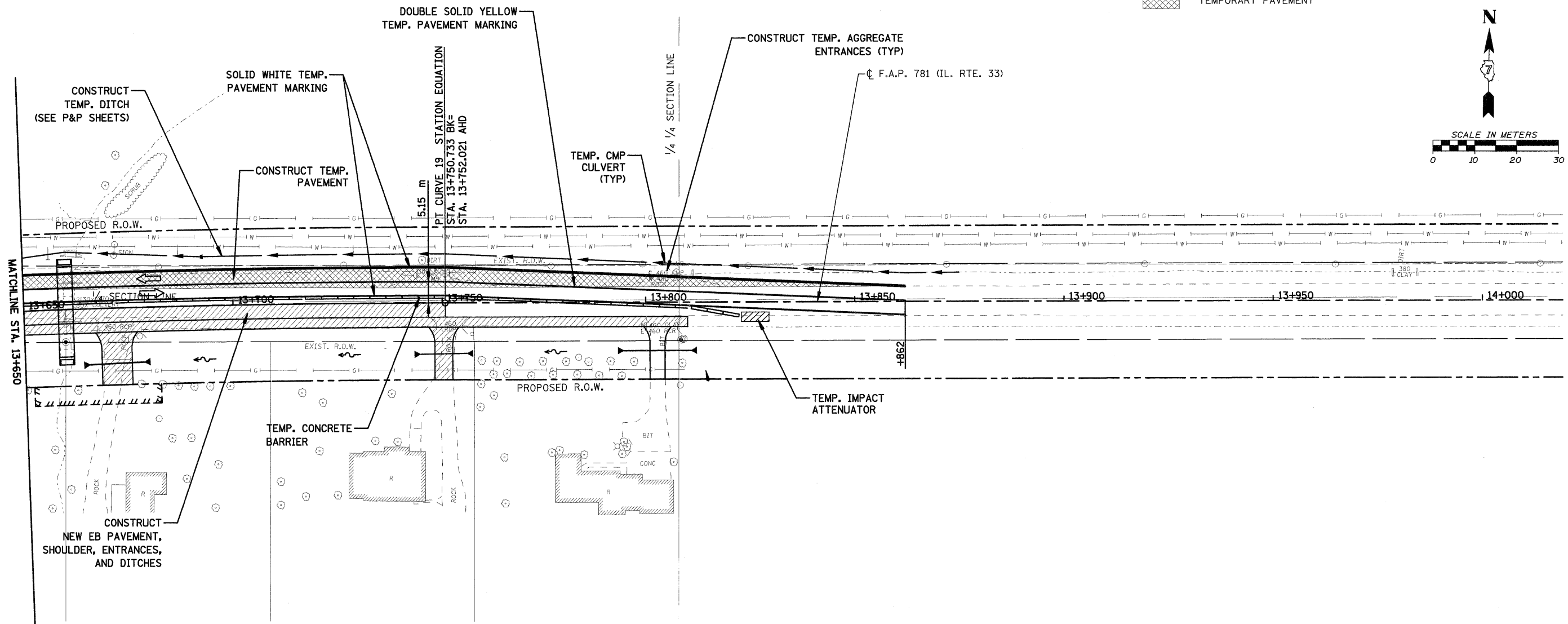
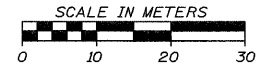
PLOT DATE = 12/22/2008
 FILE NAME = P:\4428\Contract\94533\SH1_069_stage1_2.dgn

HMG JOB NO. 4420

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------|------------------|-----------|
| 781 | LYRS | CRAWFORD | 212 | 70 |
| STA. TO STA. | | | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

LEGEND

-  CONSTRUCTION THIS STAGE
-  TEMPORARY PAVEMENT



STAGING NOTES:


1. CONSTRUCT TEMPORARY AGGREGATE ENTRANCES WITH CMP CULVERTS AS NEEDED TO MAINTAIN ACCESS.
2. TWO-WAY / TWO-LANE TRAFFIC SHALL BE RESTORED AT THE END OF THE WORK DAY. TWO-WAY / ONE-LANE TRAFFIC SHALL BE USED ON A LIMITED BASIS, WHEN REQUIRED, UTILIZING THE APPROPRIATE STANDARD.
3. FINAL HMA SURFACE COURSE SHALL BE PLACED THROUGHOUT THE PROJECT LIMITS AS A SEPARATE STAGE.

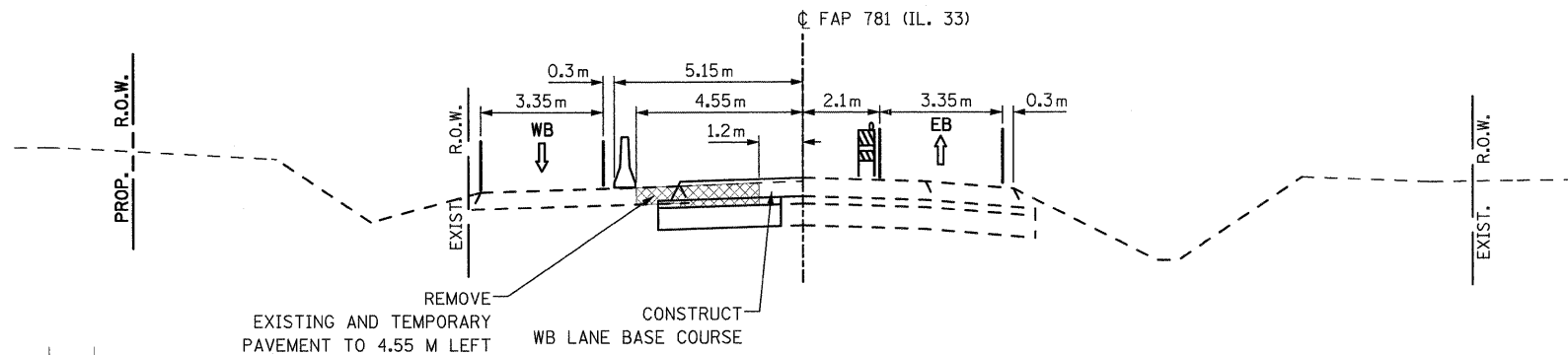
PLOT DATE = 12/22/2006
 FILE NAME = H:\1428\Contract 94533\SH1_070_stage1_3.dgn

HMG JOB NO. 4420

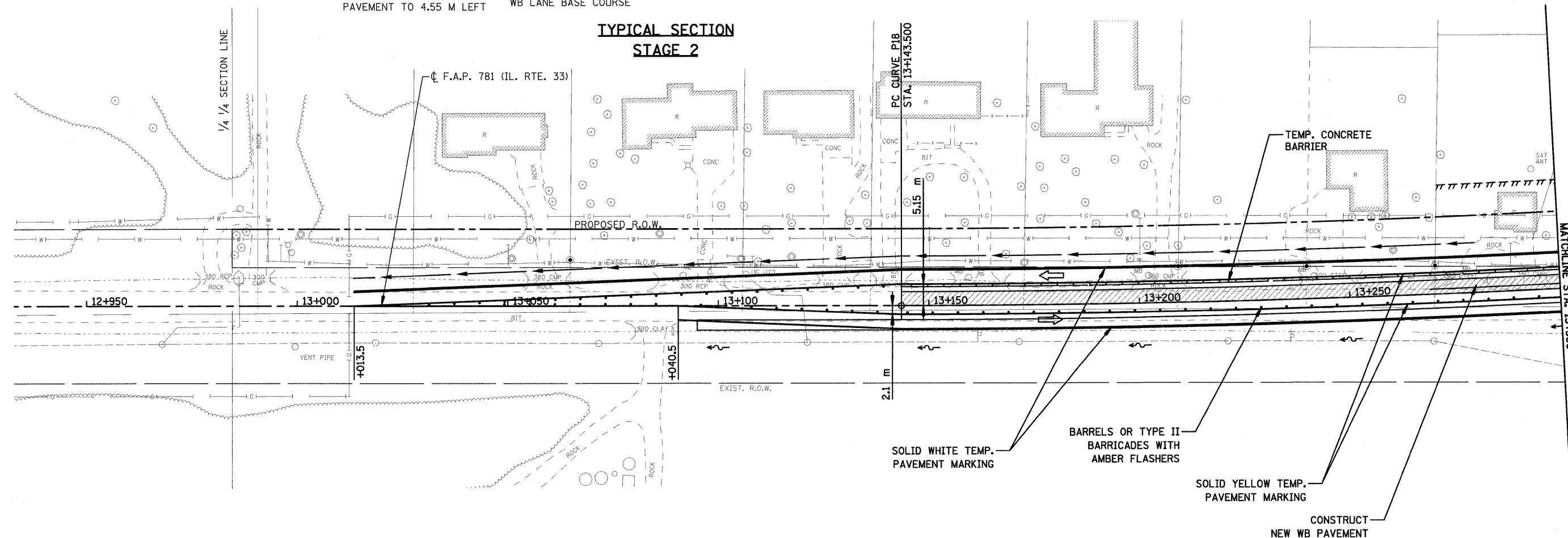
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 71 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

LEGEND

 CONSTRUCTION THIS STAGE



**TYPICAL SECTION
STAGE 2**



STAGING NOTES:

1. MAINTAIN TEMPORARY AGGREGATE ENTRANCES AS NEEDED TO MAINTAIN ACCESS.
2. TWO-WAY / TWO-LANE TRAFFIC SHALL BE RESTORED AT THE END OF THE WORK DAY. TWO-WAY / ONE-LANE TRAFFIC SHALL BE USED ON A LIMITED BASIS, WHEN REQUIRED, UTILIZING THE APPROPRIATE STANDARD.
3. FINAL HMA SURFACE COURSE SHALL BE PLACED THROUGHOUT THE PROJECT LIMITS AS A SEPARATE STAGE.

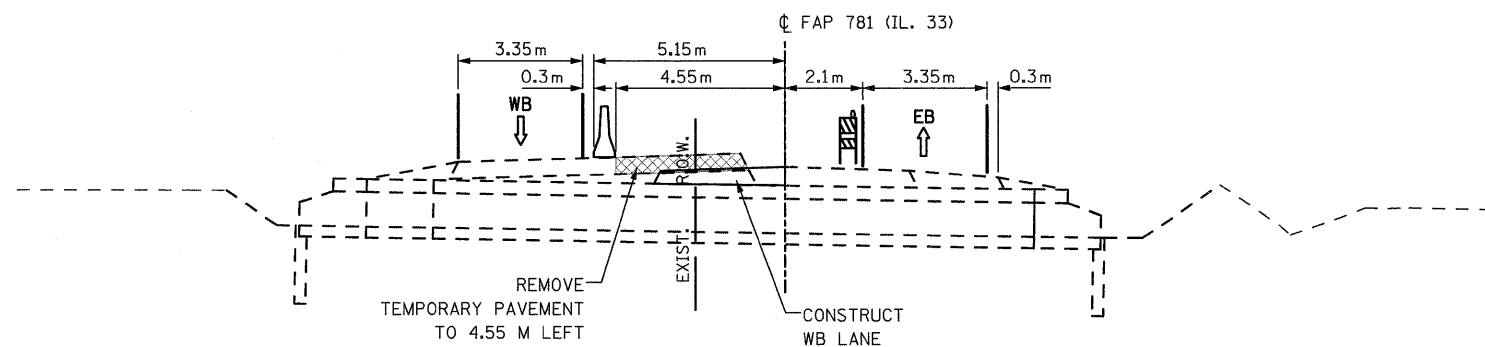
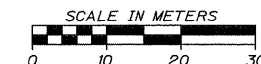
PLOT DATE = 12/22/2008
FILE NAME = H:\4420\Contract\94533\SH1_071_stage2.dgn

HMG JOB NO. 4420

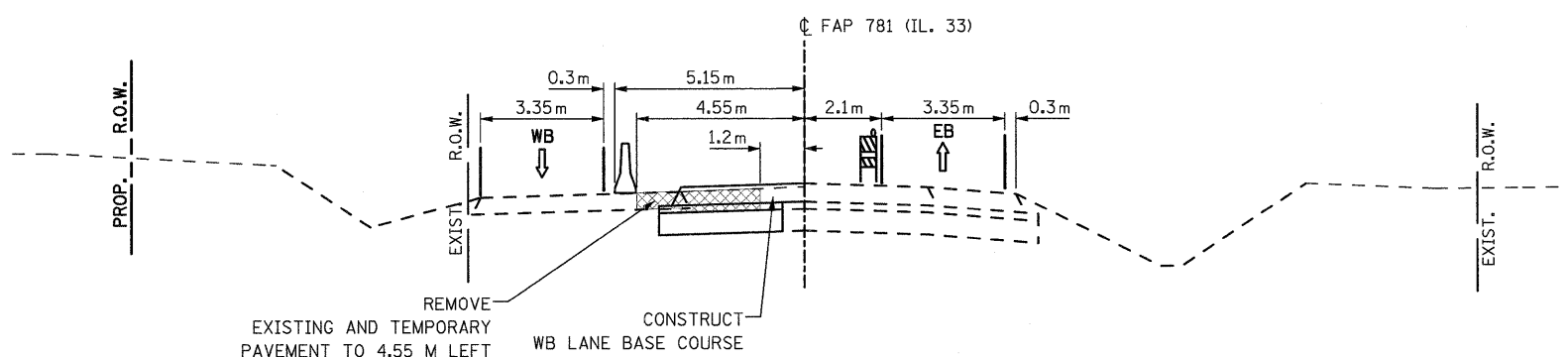
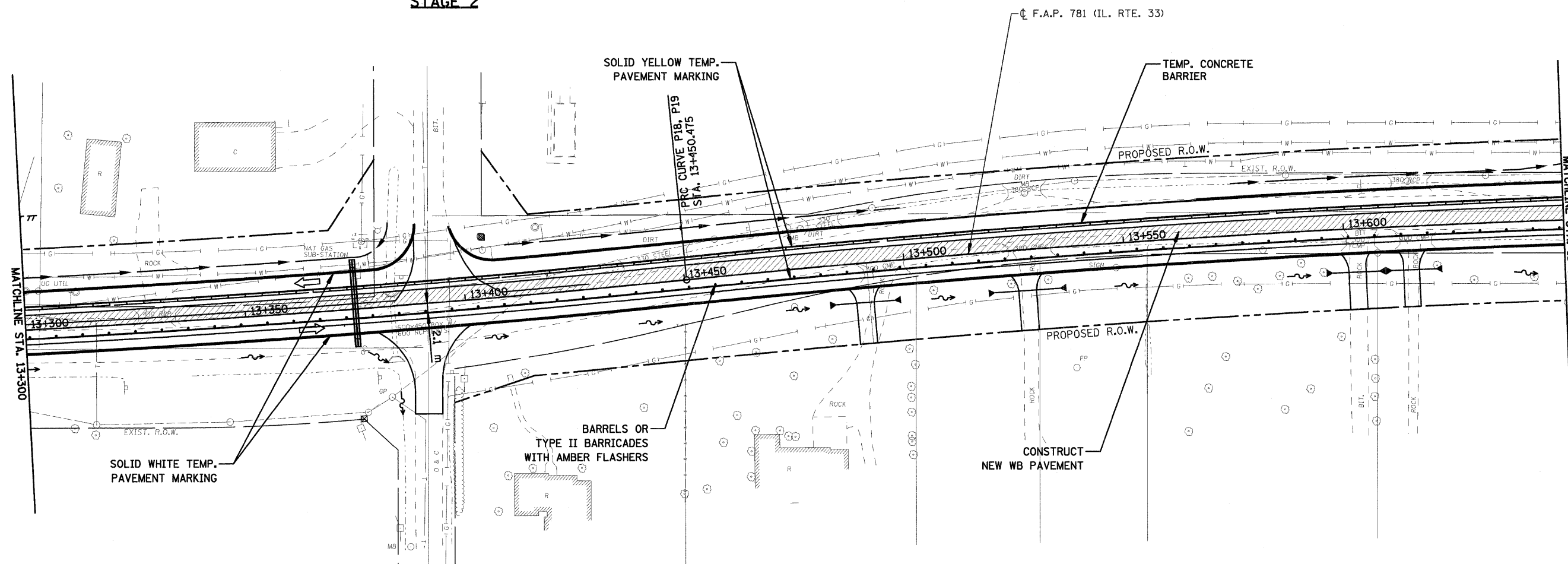
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------|---------|---|--------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 72 |
| STA. TO STA. | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | |

LEGEND

 CONSTRUCTION THIS STAGE



**BOX CULVERT STA. 13+375
STAGE 2**



**TYPICAL SECTION
STAGE 2**

STAGING NOTES:

1. MAINTAIN TEMPORARY AGGREGATE ENTRANCES AS NEEDED TO MAINTAIN ACCESS.
2. TWO-WAY / TWO-LANE TRAFFIC SHALL BE RESTORED AT THE END OF THE WORK DAY. TWO-WAY / ONE-LANE TRAFFIC SHALL BE USED ON A LIMITED BASIS, WHEN REQUIRED, UTILIZING THE APPROPRIATE STANDARD.
3. FINAL HMA SURFACE COURSE SHALL BE PLACED THROUGHOUT THE PROJECT LIMITS AS A SEPARATE STAGE.

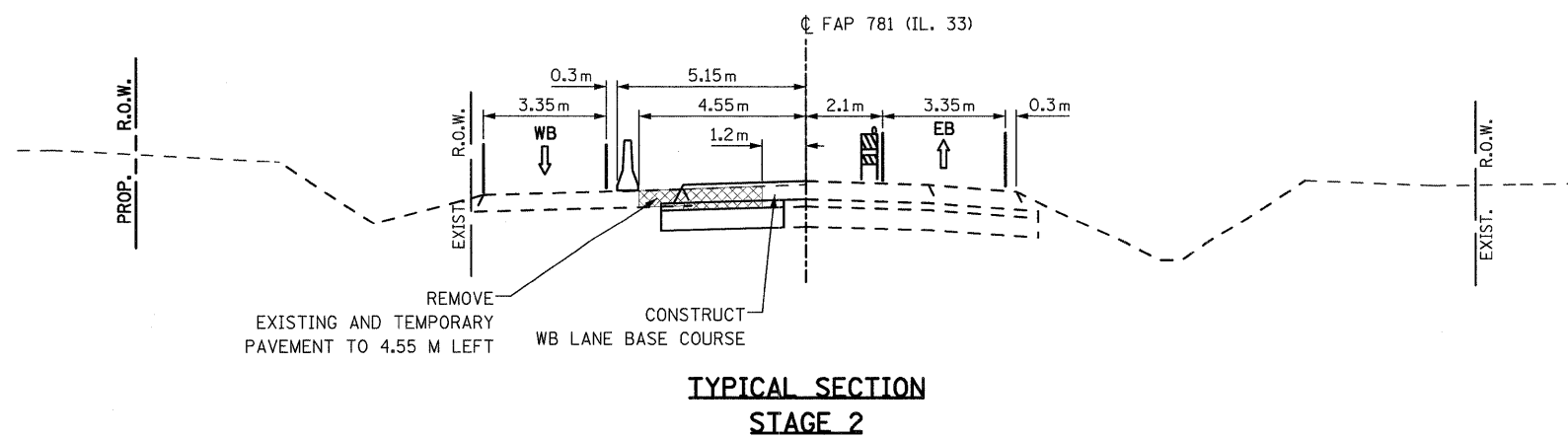
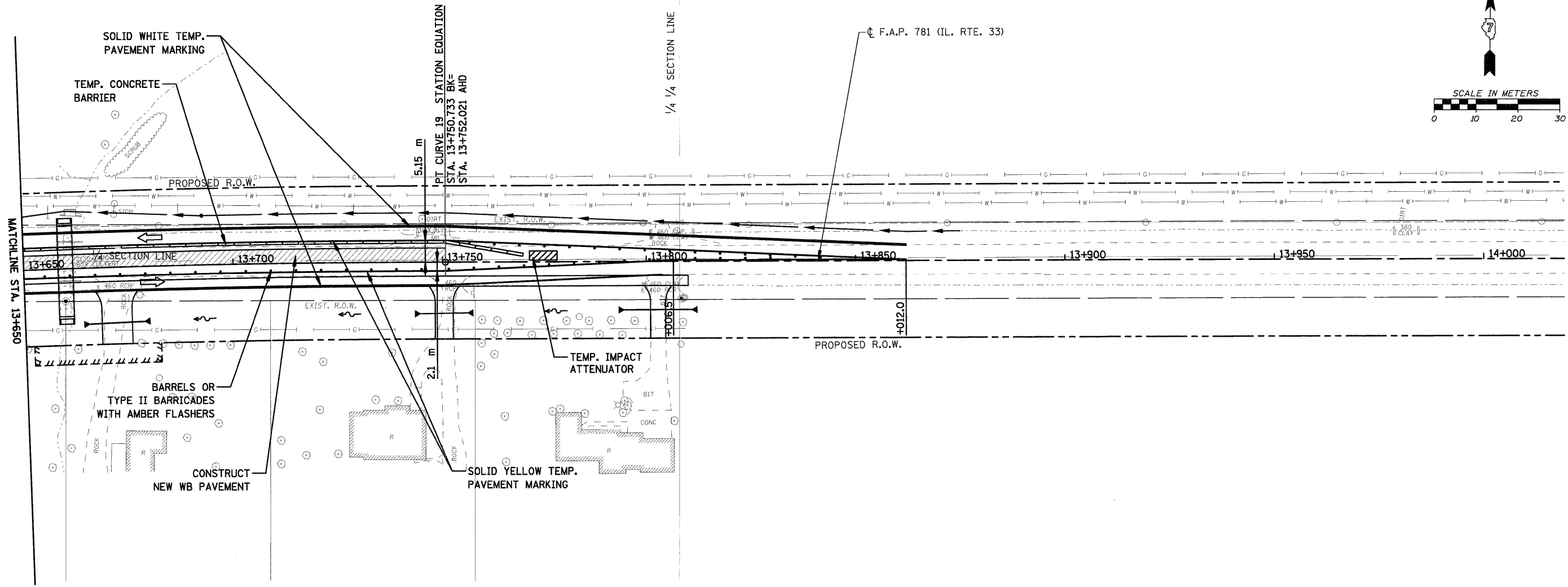
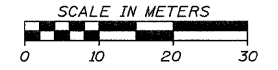
PLOT DATE = 12/22/2006
FILE NAME = H:\V\20\Contract\94533\SH2_072_stage2.2.dgn

HMG JOB NO. 4420

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------|---------|---|--------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 73 |
| STA. TO STA. | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | |

LEGEND

 CONSTRUCTION THIS STAGE



NOTES:

1. MAINTAIN TEMPORARY AGGREGATE ENTRANCES AS NEEDED TO MAINTAIN ACCESS.
2. TWO-WAY / TWO-LANE TRAFFIC SHALL BE RESTORED AT THE END OF THE WORK DAY. TWO-WAY / ONE-LANE TRAFFIC SHALL BE USED ON A LIMITED BASIS, WHEN REQUIRED, UTILIZING THE APPROPRIATE STANDARD.
3. FINAL HMA SURFACE COURSE SHALL BE PLACED THROUGHOUT THE PROJECT LIMITS AS A SEPARATE STAGE.

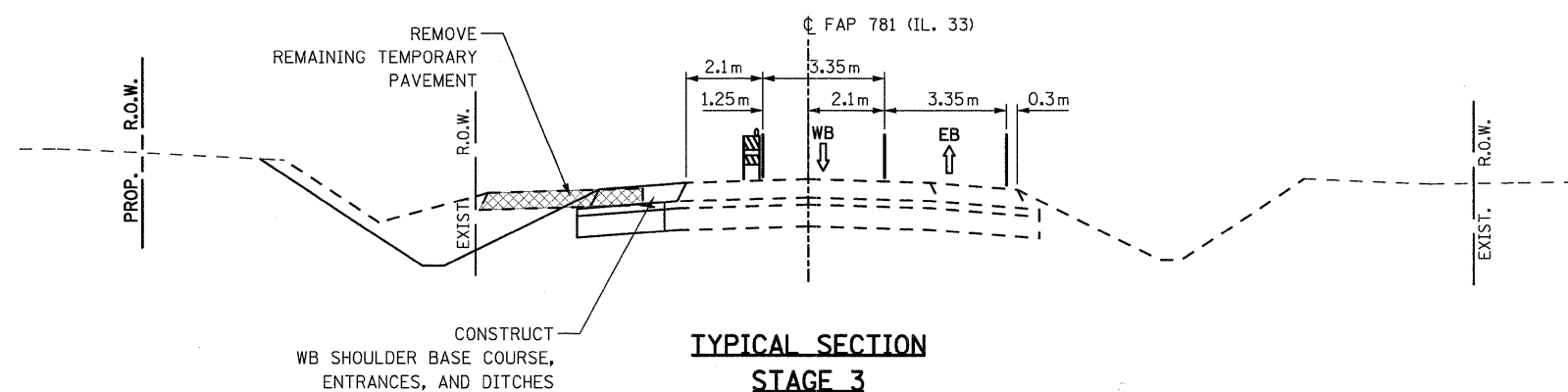
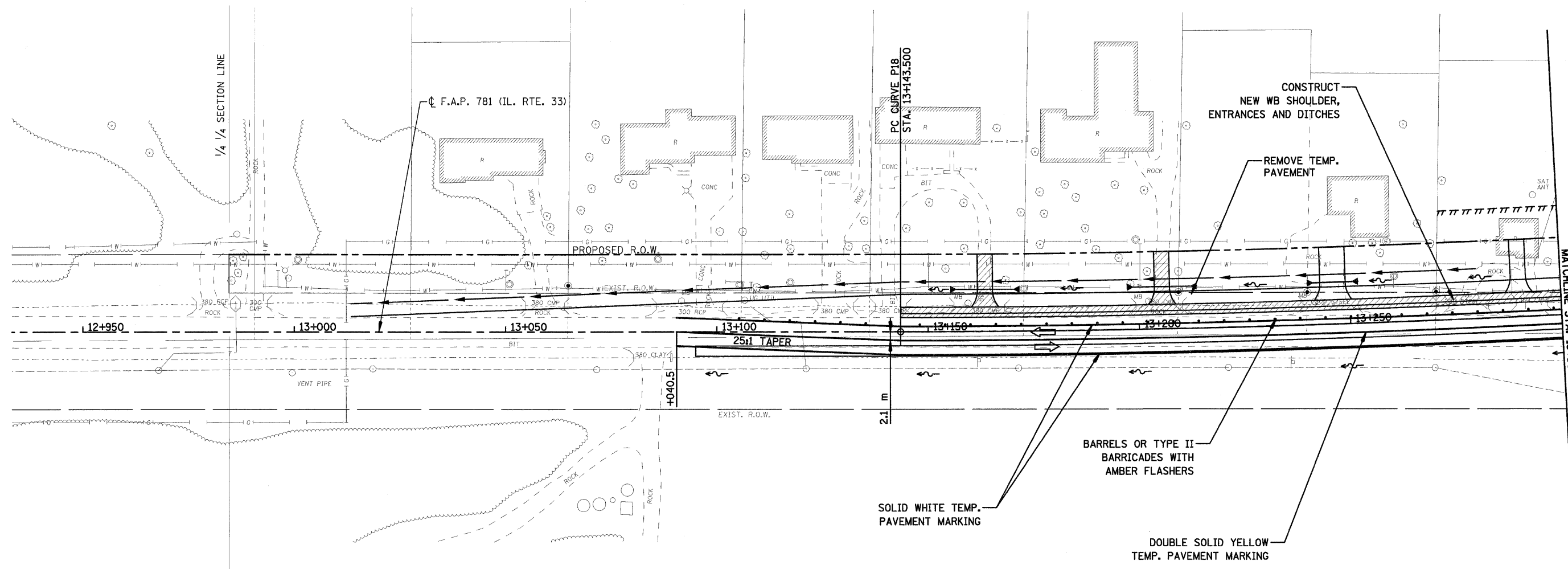
PLOT DATE = 12/22/2008
 FILE NAME = P:\V428\Cont-act_94533\SH1_073_stage2_3.dgn

HMG JOB NO. 4420

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|------------------|--------------|-----------|
| 781 | LYRS | CRAWFORD | 212 | 74 |
| STA. TO STA. | | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

LEGEND

 CONSTRUCTION THIS STAGE



NOTES:


1. MAINTAIN TEMPORARY AGGREGATE ENTRANCES AS NEEDED TO MAINTAIN ACCESS.
2. TWO-WAY / TWO-LANE TRAFFIC SHALL BE RESTORED AT THE END OF THE WORK DAY. TWO-WAY / ONE-LANE TRAFFIC SHALL BE USED ON A LIMITED BASIS, WHEN REQUIRED, UTILIZING THE APPROPRIATE STANDARD.
3. FINAL HMA SURFACE COURSE SHALL BE PLACED THROUGHOUT THE PROJECT LIMITS AS A SEPARATE STAGE.

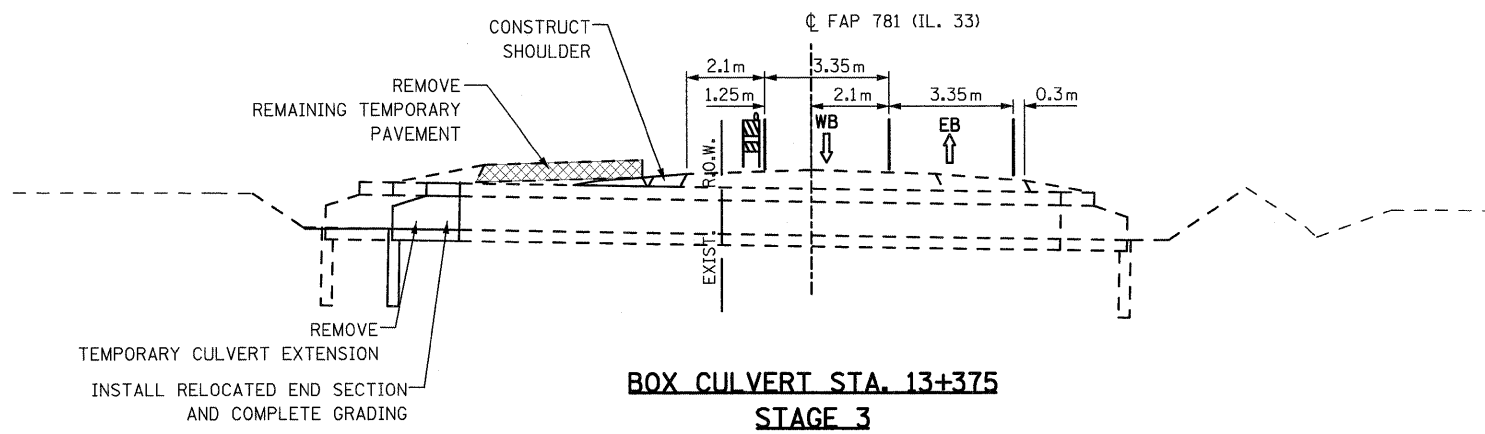
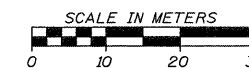
PLOT DATE = 12/22/2008
FILE NAME = H:\1420\Contract\94533\SH1_074_stage3_1.dgn

HMG JOB NO. 4420

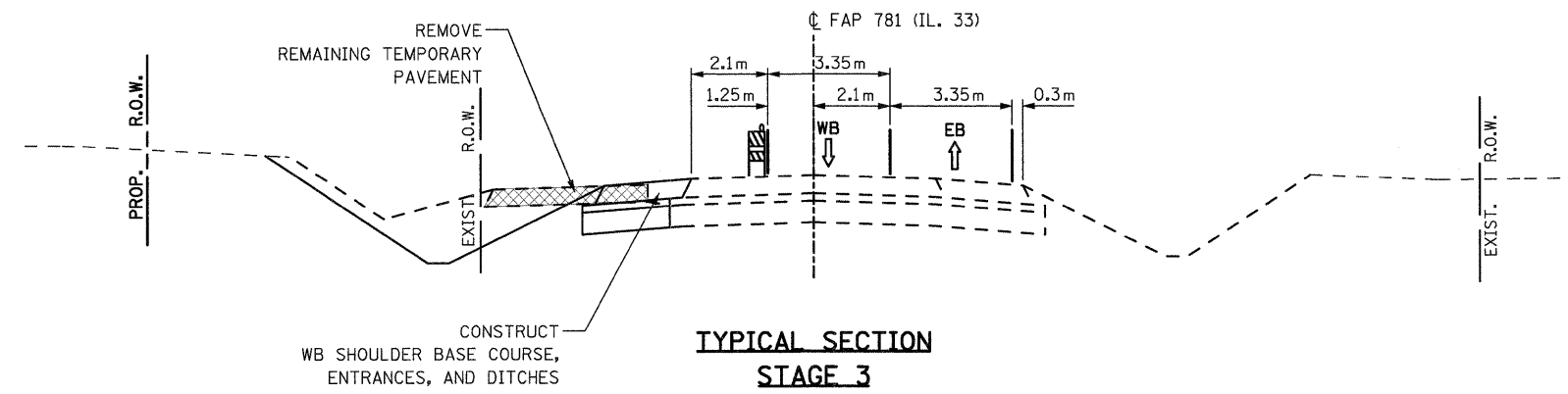
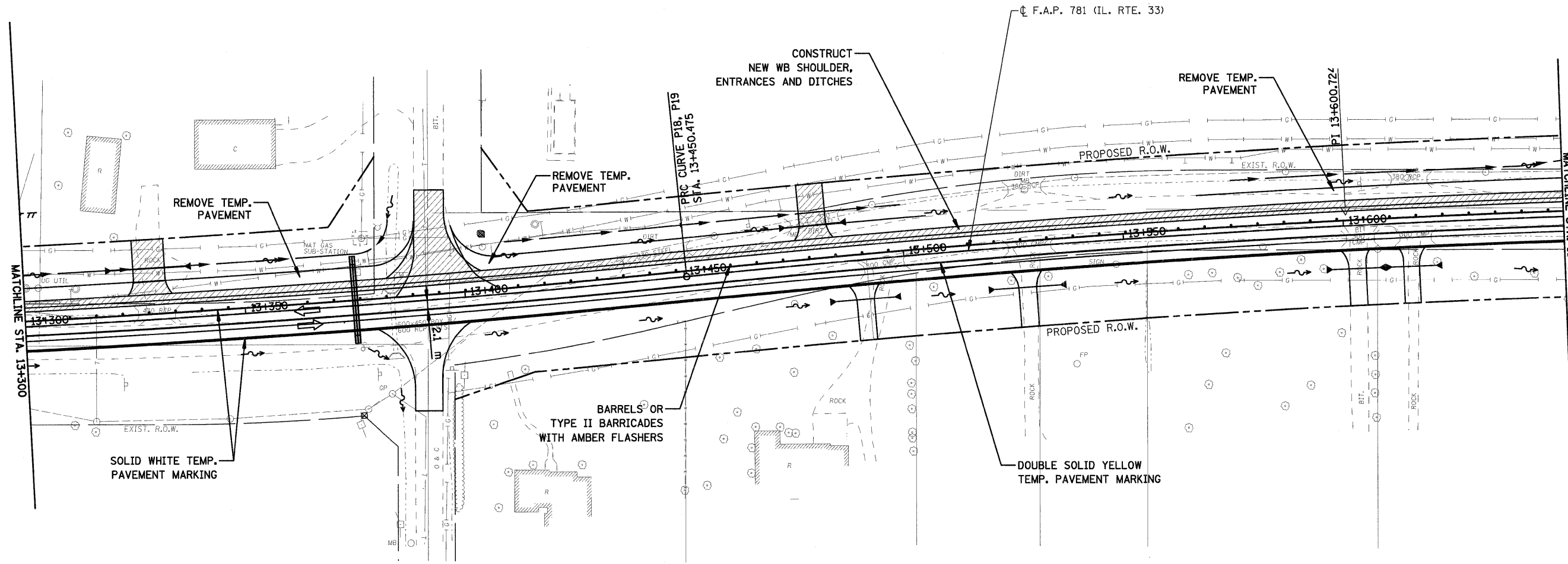
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 75 |
| STA. TO STA. | | ILLINOIS | FED. AID PROJECT | |
| FED. ROAD DIST. NO. | | | | |

LEGEND

 CONSTRUCTION THIS STAGE



**BOX CULVERT STA. 13+375
STAGE 3**



NOTES:

1. MAINTAIN TEMPORARY AGGREGATE ENTRANCES AS NEEDED TO MAINTAIN ACCESS.
2. TWO-WAY / TWO-LANE TRAFFIC SHALL BE RESTORED AT THE END OF THE WORK DAY. TWO-WAY / ONE-LANE TRAFFIC SHALL BE USED ON A LIMITED BASIS, WHEN REQUIRED, UTILIZING THE APPROPRIATE STANDARD.
3. FINAL HMA SURFACE COURSE SHALL BE PLACED THROUGHOUT THE PROJECT LIMITS AS A SEPARATE STAGE.

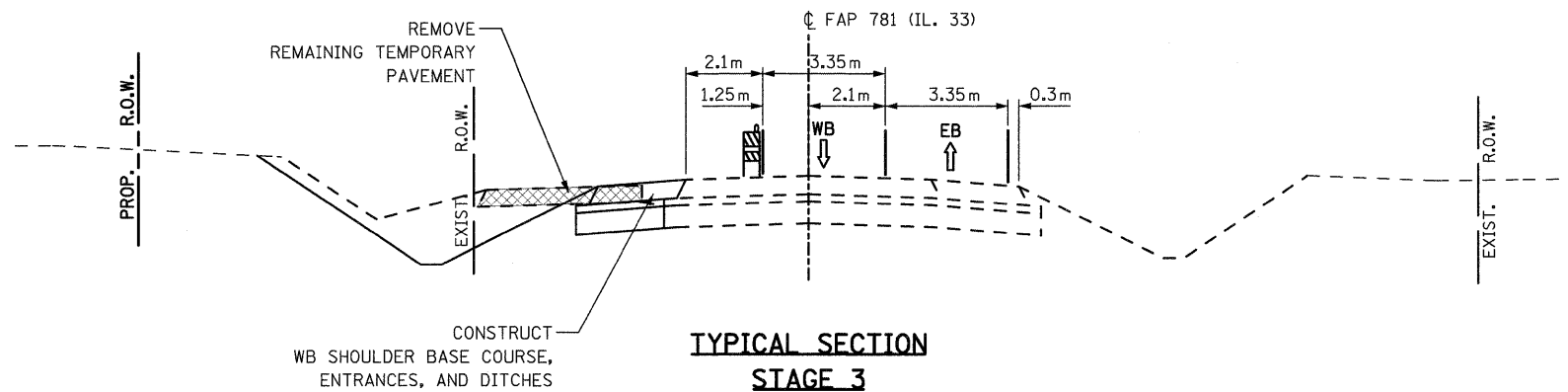
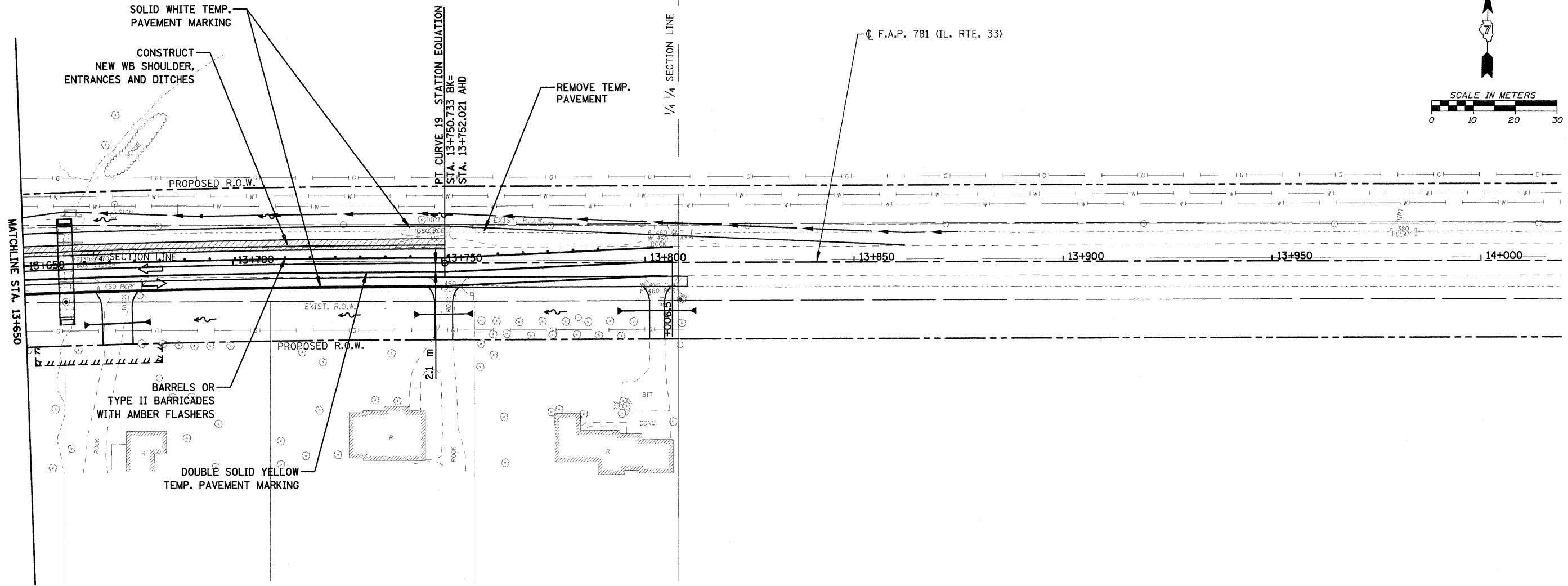
PLOT DATE = 12/22/2008
FILE NAME = H:\4420\Contract\94533\SH3_075_stage3.2.dgn

HMG JOB NO. 4420

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 76 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

LEGEND

 CONSTRUCTION THIS STAGE



**TYPICAL SECTION
STAGE 3**

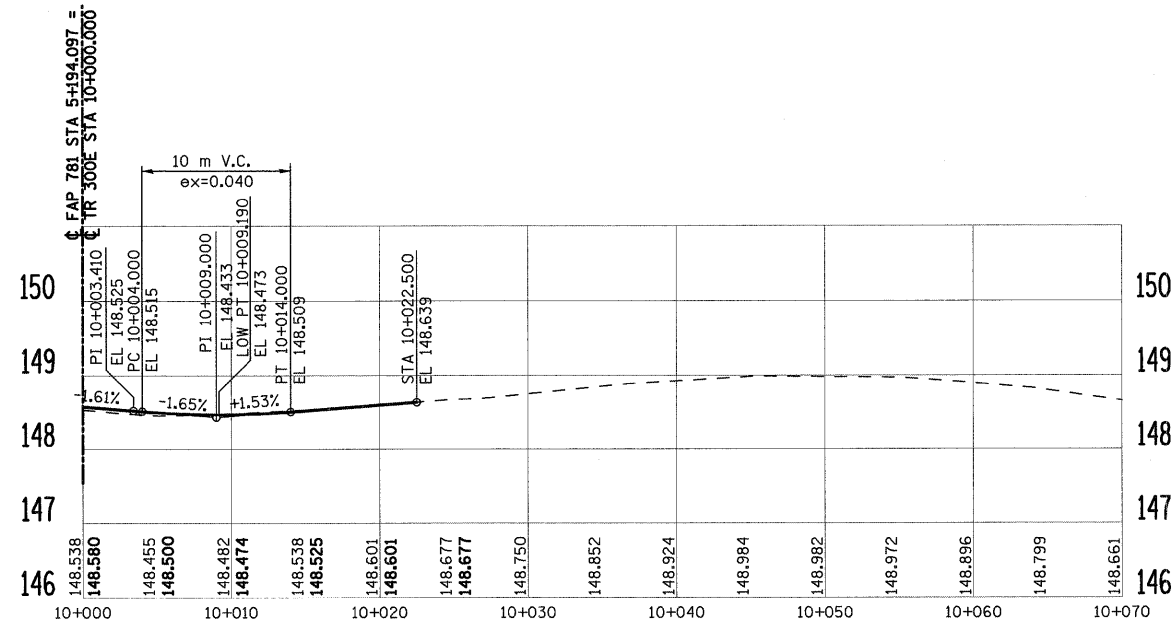
NOTES:

1. MAINTAIN TEMPORARY AGGREGATE ENTRANCES AS NEEDED TO MAINTAIN ACCESS.
2. TWO-WAY / TWO-LANE TRAFFIC SHALL BE RESTORED AT THE END OF THE WORK DAY. TWO-WAY / ONE-LANE TRAFFIC SHALL BE USED ON A LIMITED BASIS, WHEN REQUIRED, UTILIZING THE APPROPRIATE STANDARD.
3. FINAL HMA SURFACE COURSE SHALL BE PLACED THROUGHOUT THE PROJECT LIMITS AS A SEPARATE STAGE.

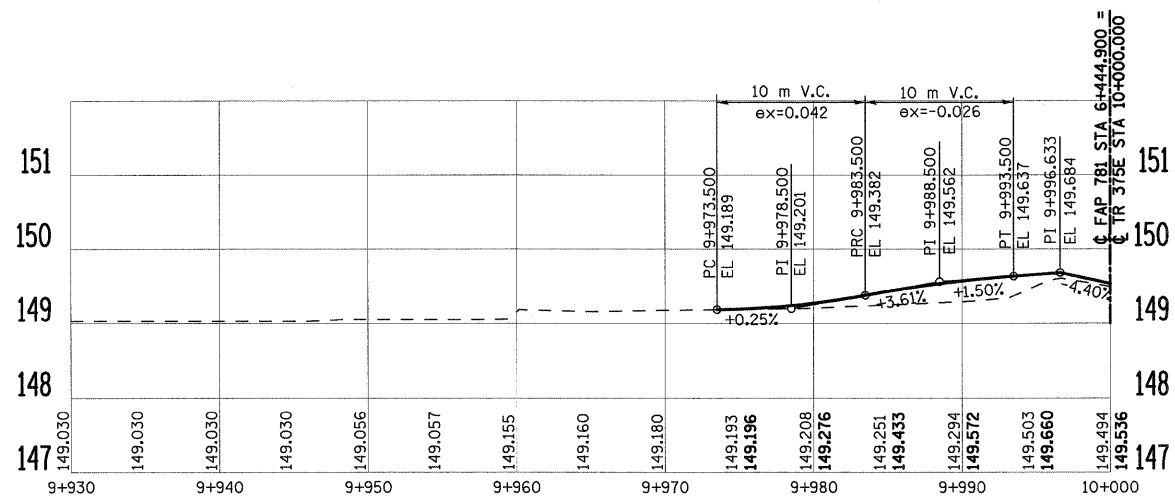
PLOT DATE = 12/22/2008
FILE NAME = H:\4428\Contract_94533\SHT_076_stage3.dgn

HMG JOB NO. 4420

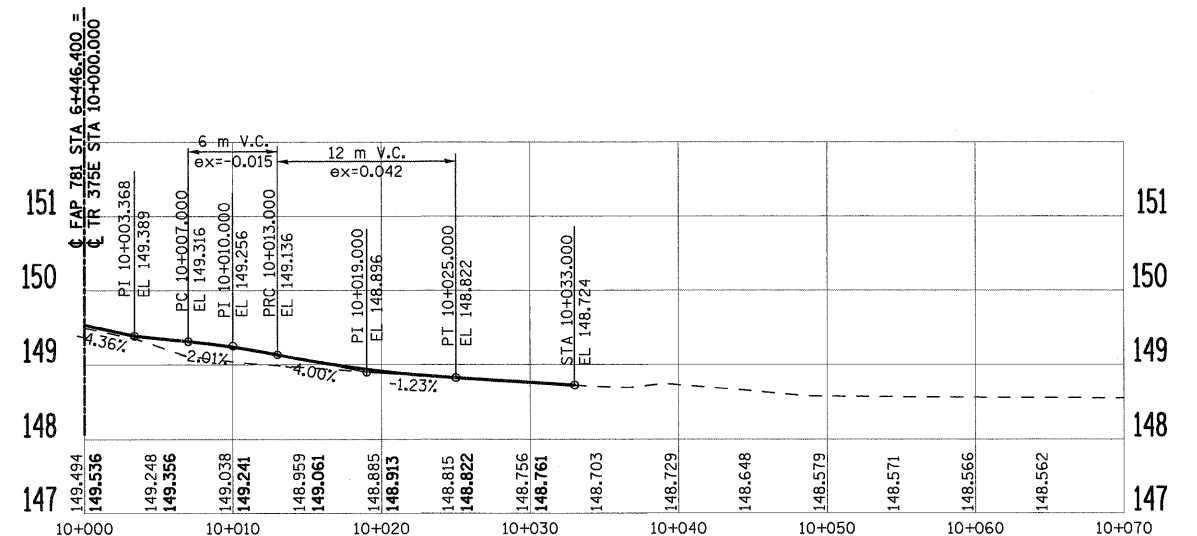
| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 77 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



TR 300E (RT)



TR 375 (LT)



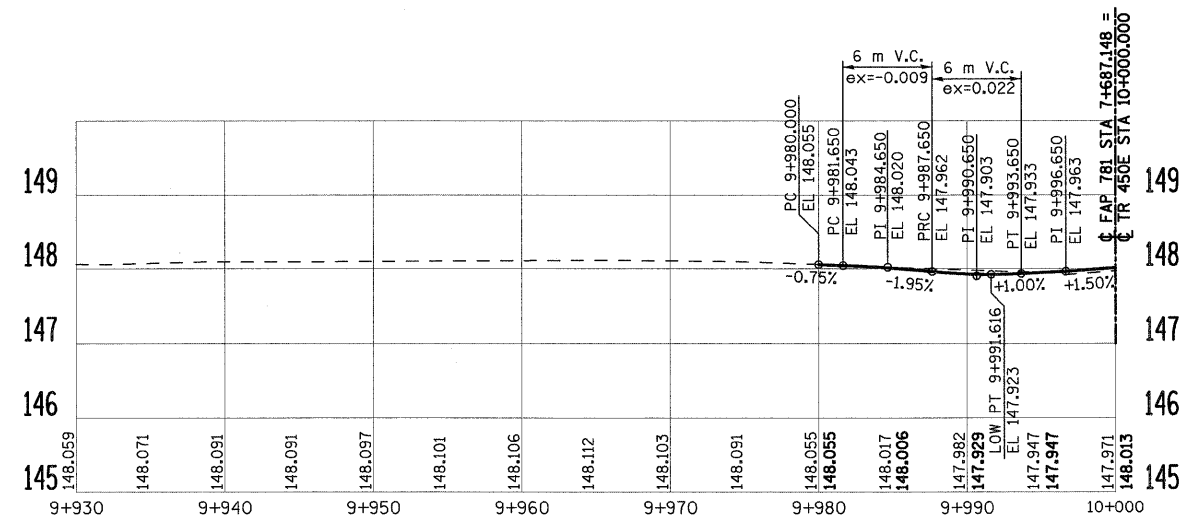
TR 375E (RT)

PLOT DATE = 12/22/2008
FILE NAME = R:\V420\com+est 94533\SH1_077.srf\01.dgn

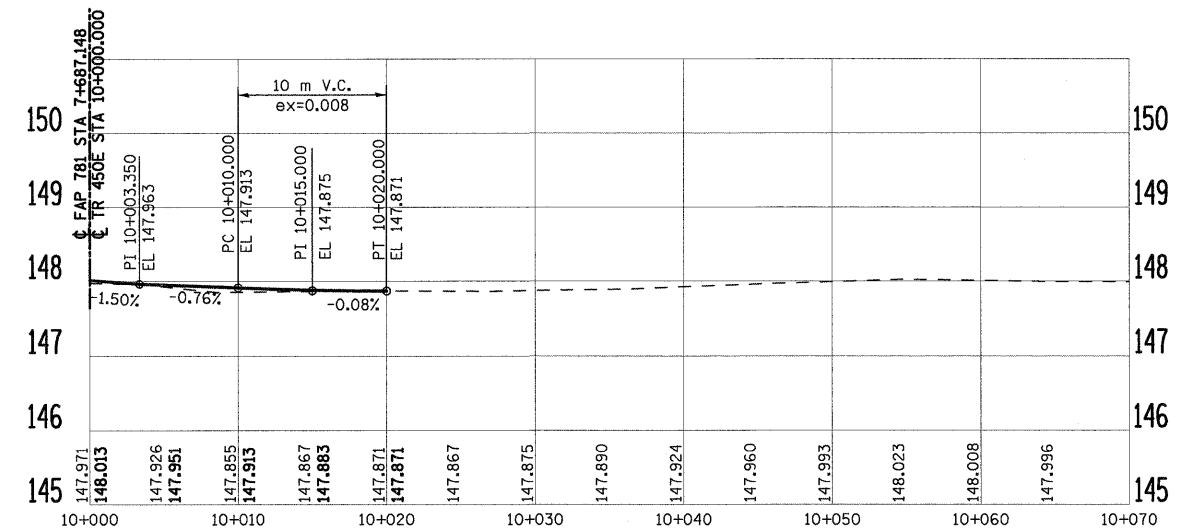
| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
PROPOSED SIDE ROAD PROFILES
SCALE AS SHOWN DRAWN BY EDW
DATE DECEMBER 22, 2008 CHECKED BY LWJ

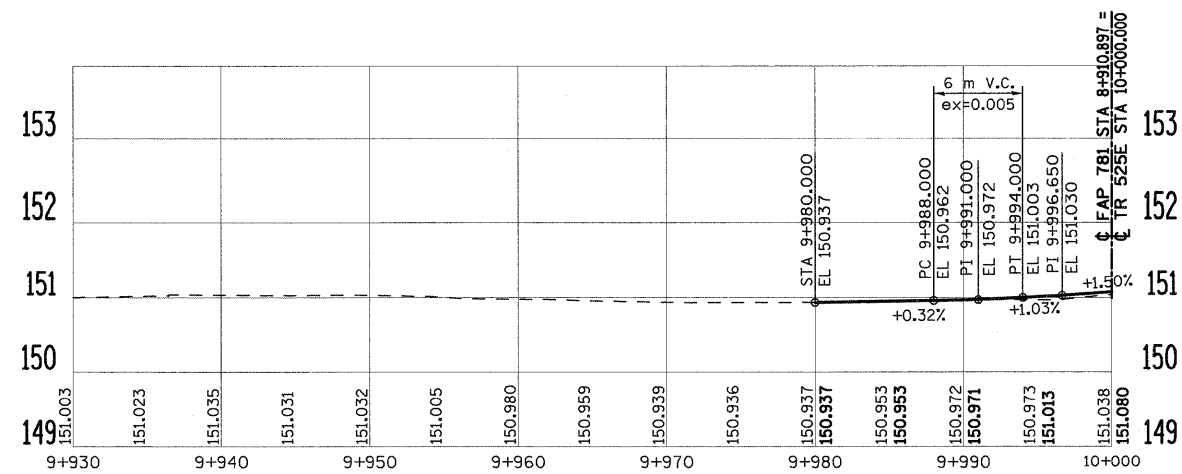
| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 78 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



TR 450E (LT)



TR 450E (RT)

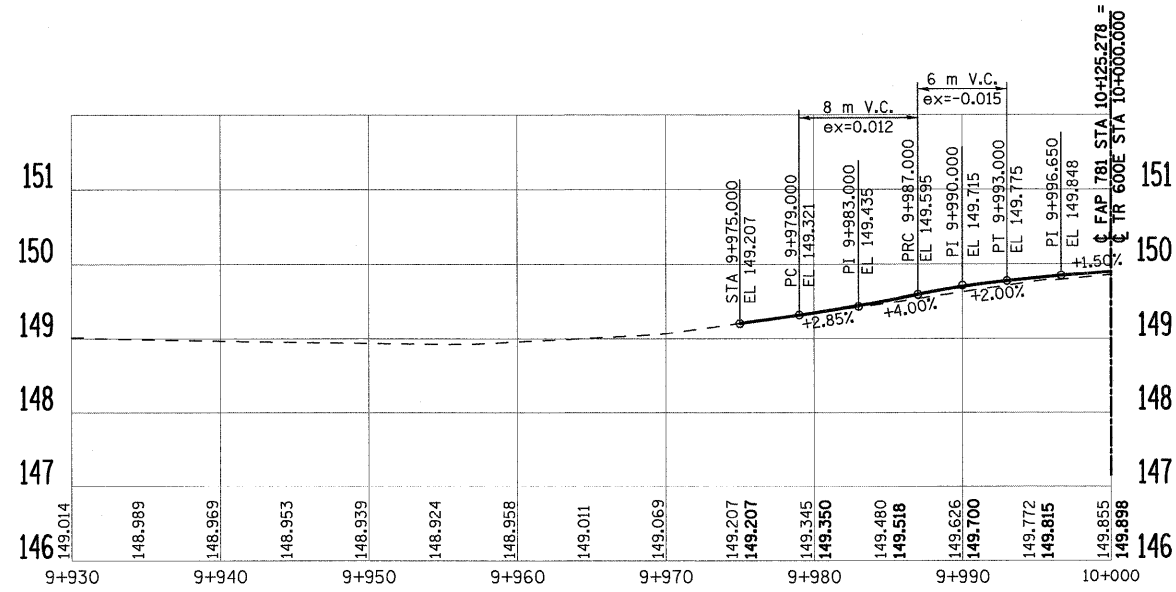


TR 525E (LT)

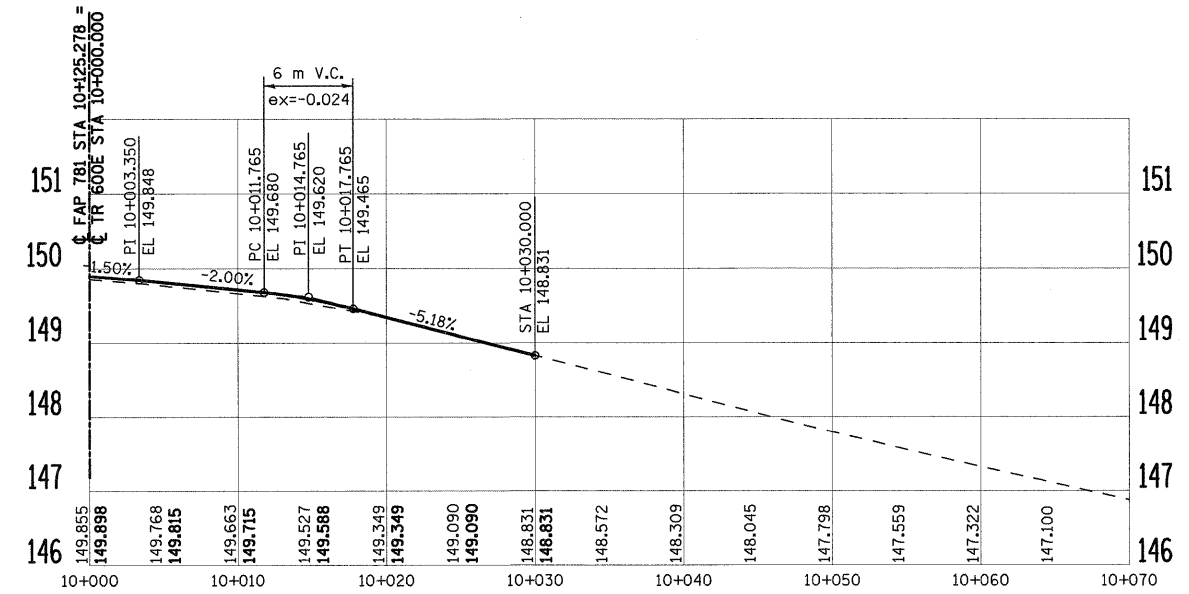
PLOT DATE = 12/22/2008
FILE NAME = H:\1420\Contract 94533\SH1_078.dwg

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|--|
| NAME | DATE | |
| | | PROPOSED SIDE ROAD PROFILES |
| | | |
| | | |
| | | |
| | | |
| | | SCALE AS SHOWN DATE DECEMBER 22, 2008 |
| | | DRAWN BY EDW CHECKED BY LWJ |

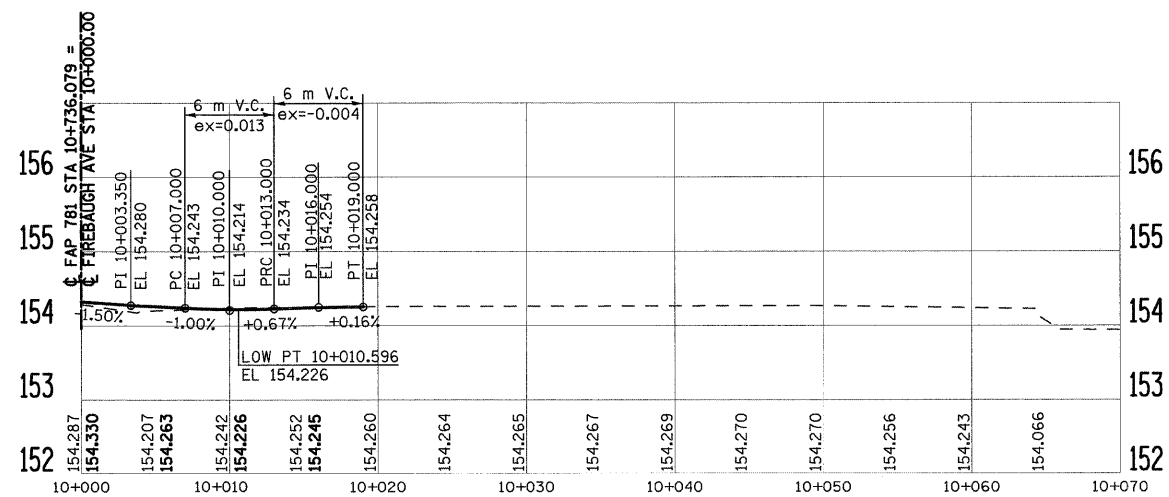
| | | | | |
|--------------|---------|---|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 79 |
| STA. TO STA. | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | |



STOY RD. TR 600 E (LT.)



STOY RD. TR 600 E (RT.)

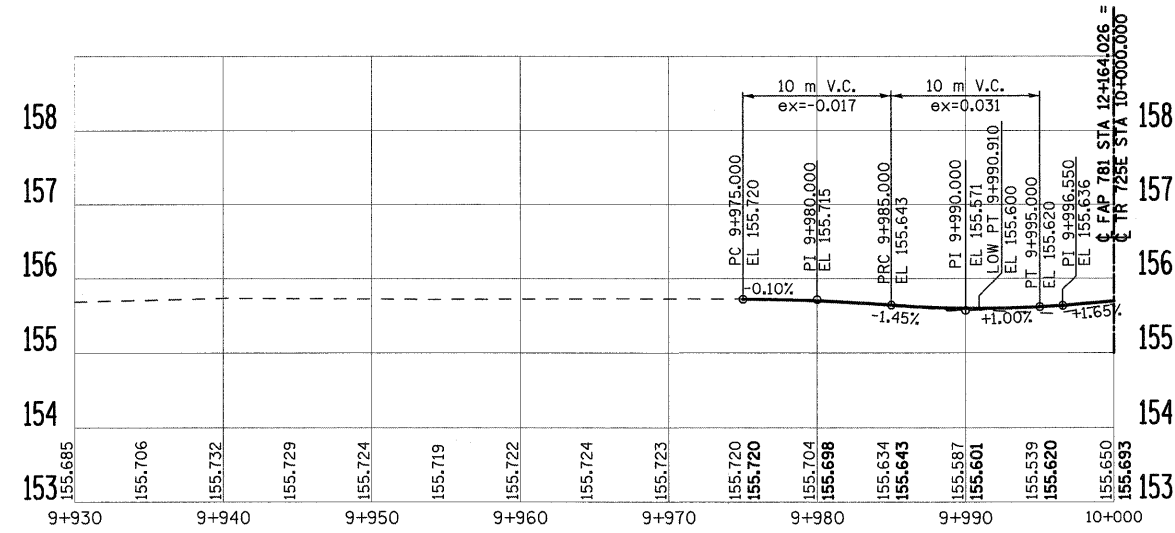


FIREBAUGH AVE. (RT.)

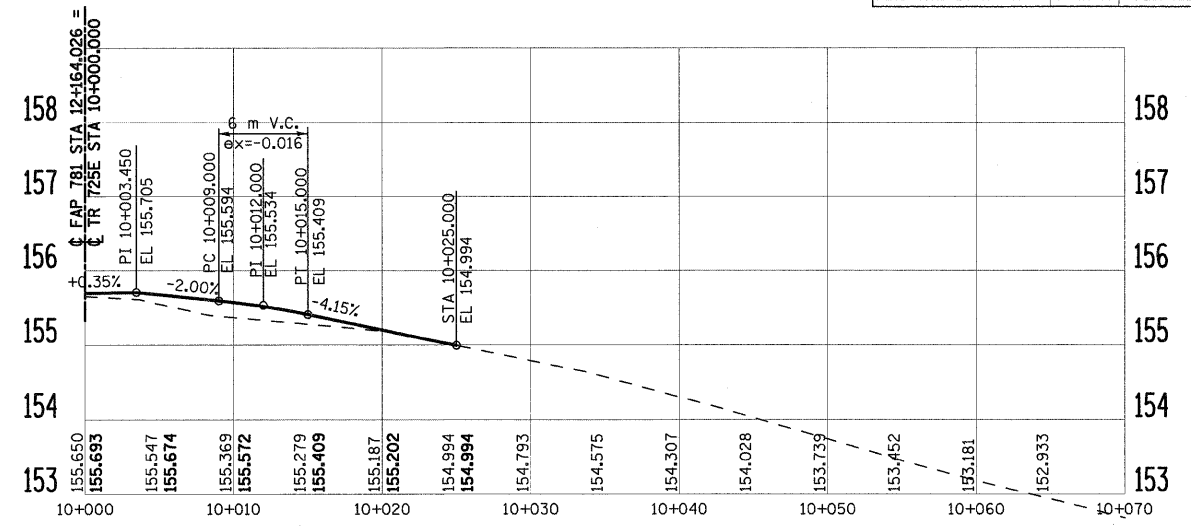
PLOT DATE = 12/22/2008
FILE NAME = H:\4426\Cont-est_94533\SHT_879.dgn

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|------------------------|------|---------------------------------------|
| NAME | DATE | |
| | | PROPOSED SIDE ROAD PROFILES |
| | | |
| | | |
| | | |
| | | |
| | | |
| SCALE AS SHOWN | | DRAWN BY EDW |
| DATE DECEMBER 22, 2008 | | CHECKED BY LWJ |

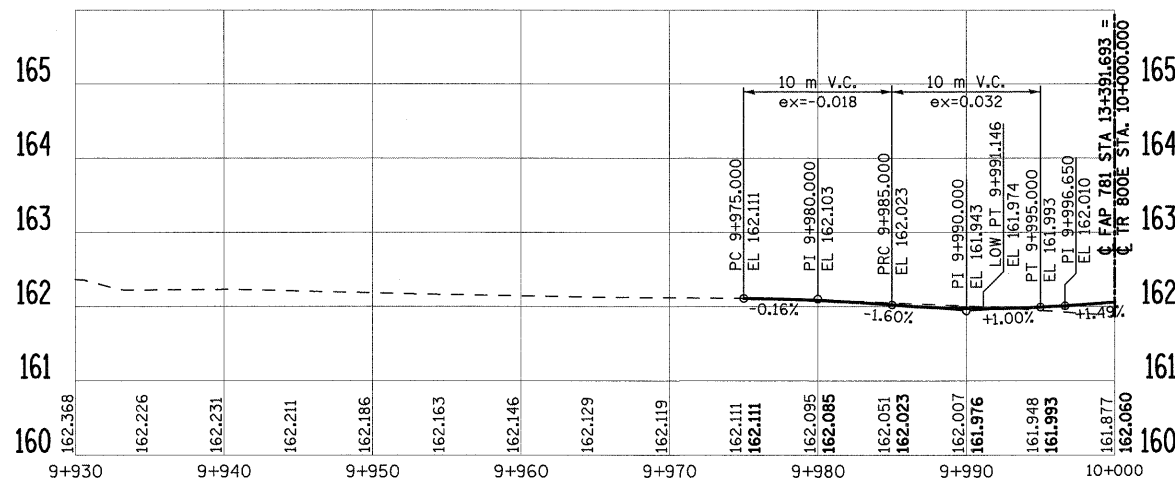
| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 80 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



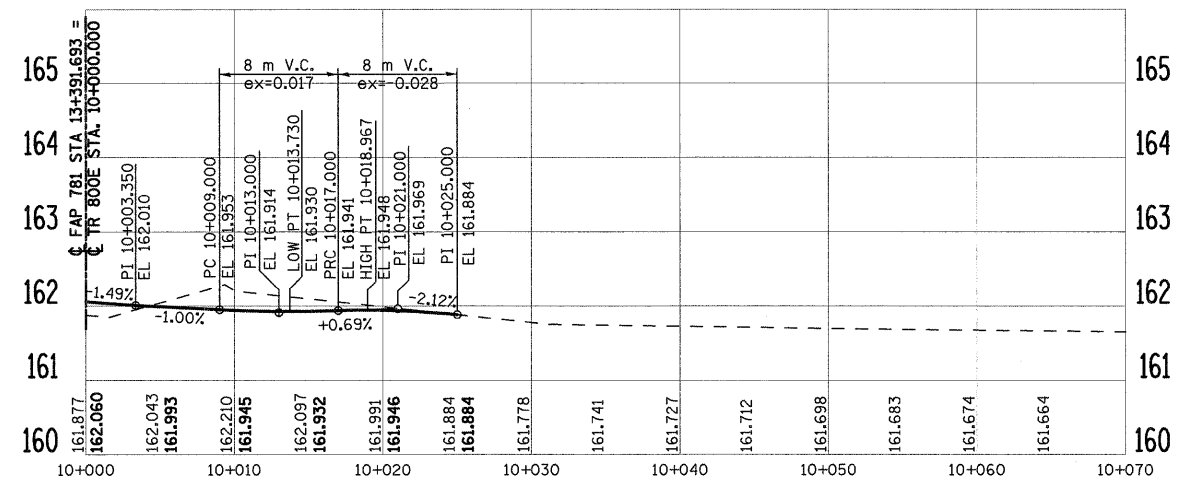
TR 725 E (LT.)



TR 725 E (RT.)



TR 800 E (LT.)

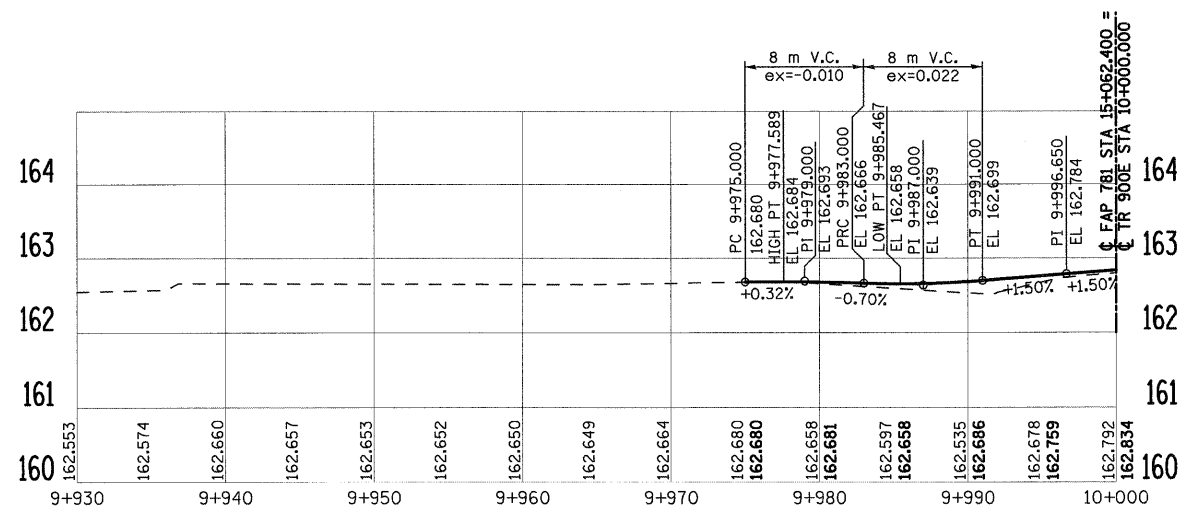


TR 800 E (RT.)

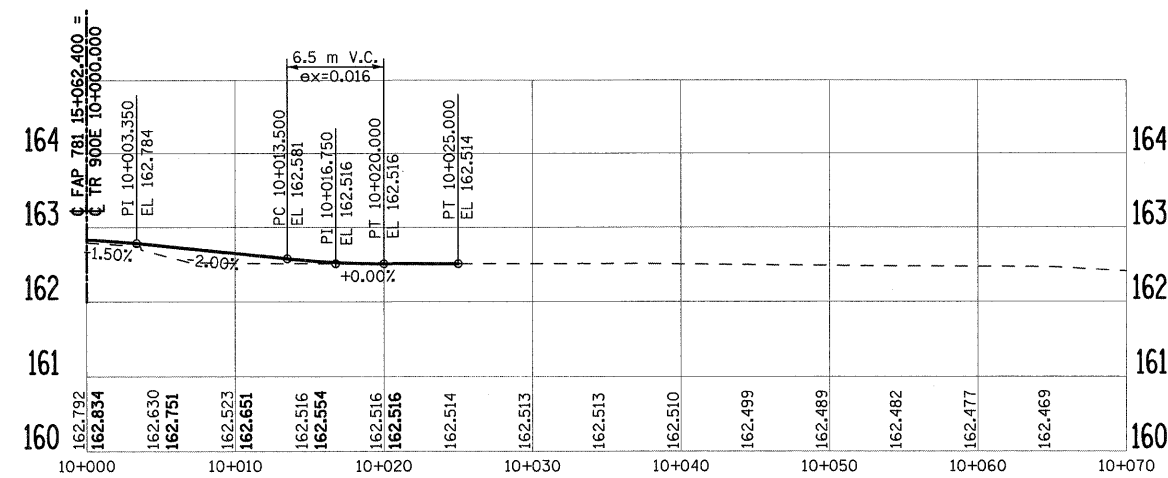
PLOT DATE = 12/22/2008
FILE NAME = H:\1420\Contract 94533\SH1_800.apr\F04.dgn

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|---|
| NAME | DATE | |
| | | <p style="text-align: center;">PROPOSED SIDE ROAD PROFILES</p> <p>SCALE AS SHOWN DRAWN BY EDW</p> <p>DATE DECEMBER 22, 2008 CHECKED BY LWJ</p> |
| | | |
| | | |
| | | |
| | | |

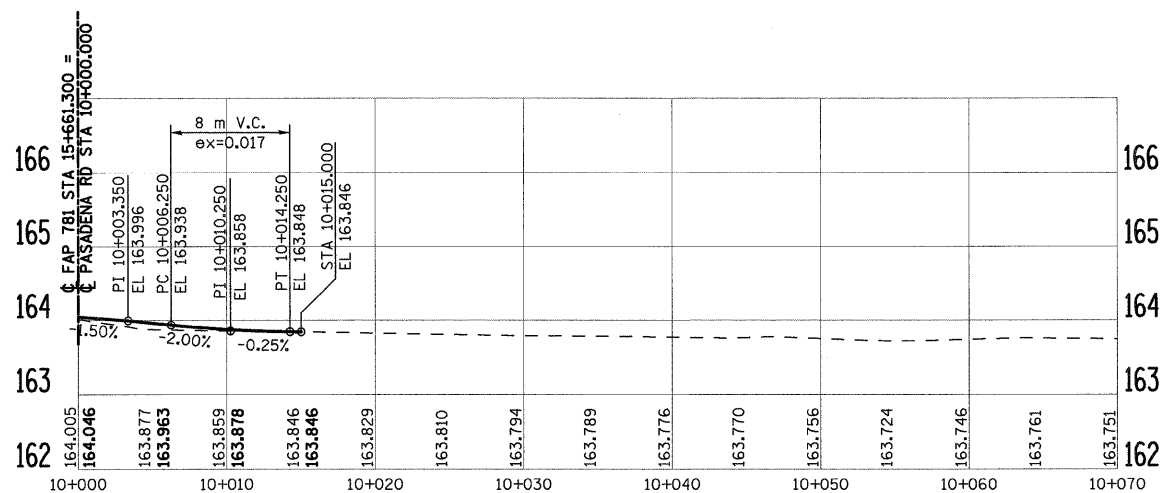
| | | | | |
|--------------|---------|---|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | LYRS | CRAWFORD | 212 | 81 |
| STA. TO STA. | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | |



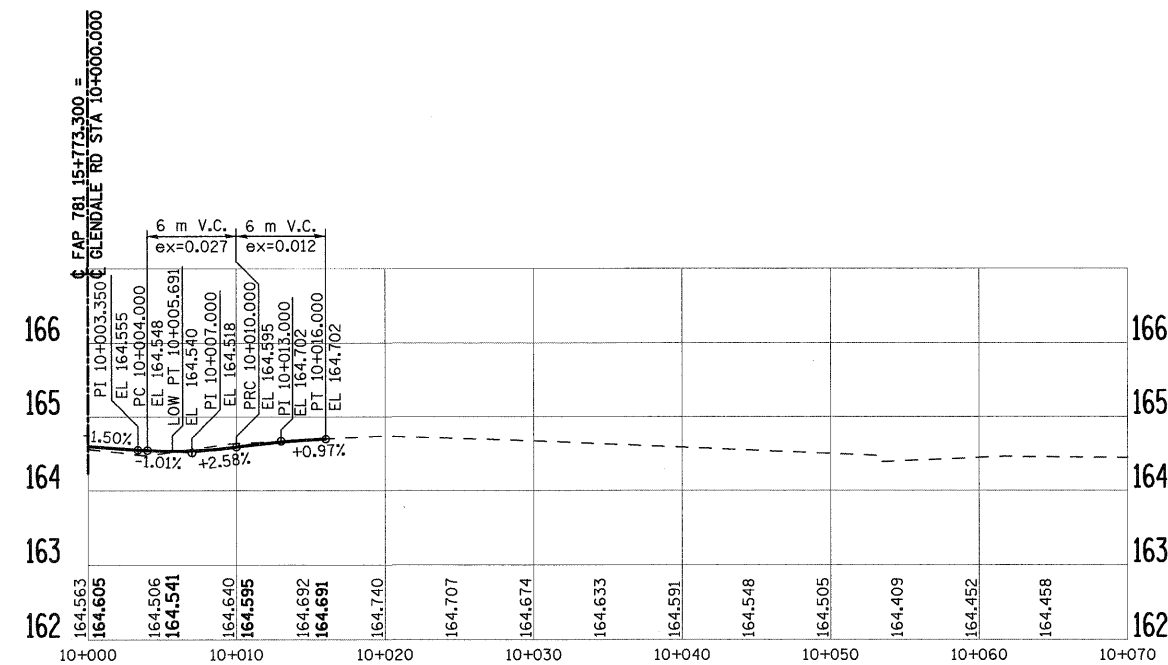
TR 900E (LT.)



TR 900E (RT.)



PASADENA RD. (RT.)



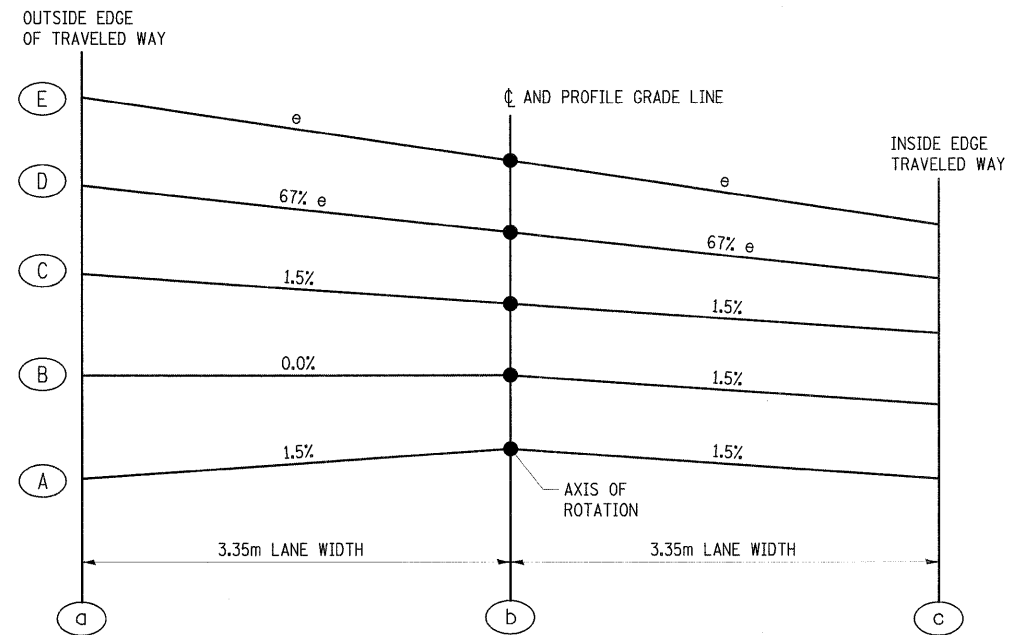
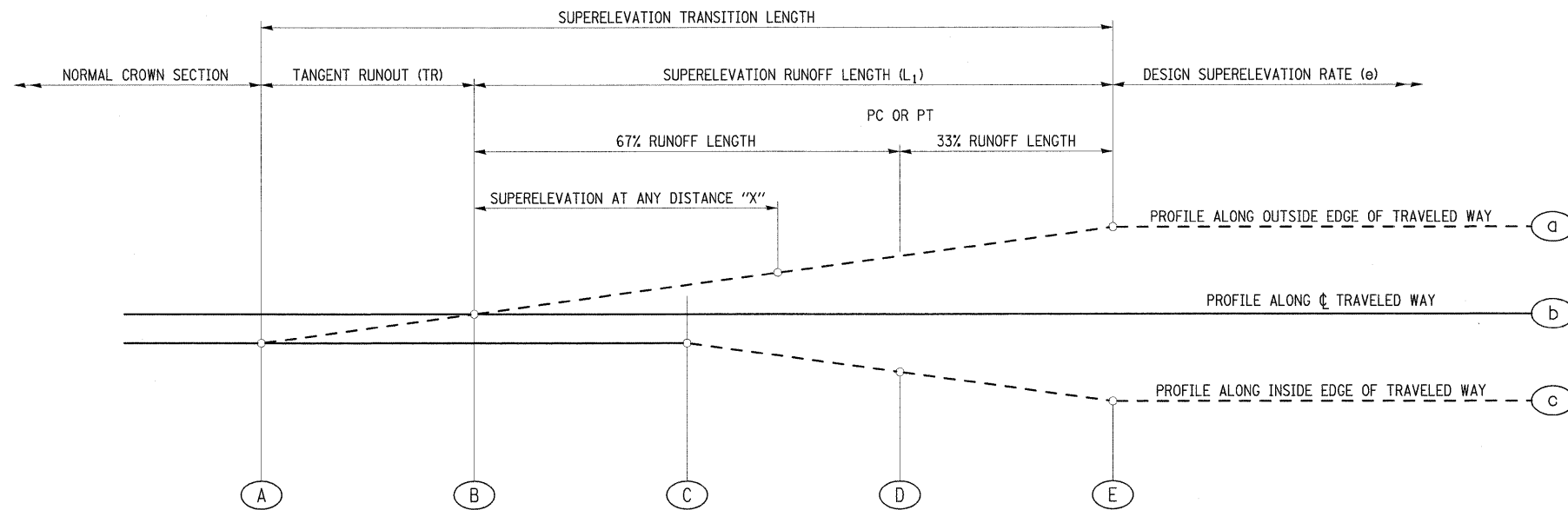
GLENDALE RD. (RT.)

PLOT DATE = 12/22/2008
FILE NAME = H:\4428\Contract\94533\SHT_801.dgn

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
PROPOSED SIDE ROAD PROFILES
SCALE AS SHOWN DRAWN BY EDW
DATE DECEMBER 22, 2008 CHECKED BY LWJ

| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 82 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



NOTES

1. REFER TO THE TYPICAL SECTIONS, PLAN & PROFILE SHEETS AND THE SUPERELEVATION DATA TABLES ON THE FOLLOWING SHEET FOR DETAILED INFORMATION AT EACH CURVE.
2. TO COMPUTE THE SUPERELEVATION AT ANY DISTANCE "X" FROM POINT B, USE THE FOLLOWING:

$$\text{SUPERELEVATION AT ANY DISTANCE "X"} = \frac{(3.35e/L_1) \times \text{DISTANCE "X"}}{36}$$

TYPICAL SUPERELEVATION TRANSITION DETAILS

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|-------------------|--|
| NAME | DATE | |
| | | TYPICAL SUPERELEVATION TRANSITION DETAIL |
| | | |
| | | |
| | | |
| | | |
| | | |
| SCALE | NO SCALE | DRAWN BY KOJ |
| DATE | DECEMBER 22, 2008 | CHECKED BY LWJ |

PLOT DATE = 12/22/2008
 FILE NAME = P:\4428\Contract\94533\SHT_082.dwg

H. M. & G. NO. 4420

| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 83 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

CURVE P8 TRANSITION DETAILS

R = 581.672 NC = -1.5% LANE WIDTH = 3.35m

CURVE = RIGHT SE = 5.2%

| | | | |
|---------|-----|-----|----------------|
| ROUNDED | 1/3 | 2/3 | CURVE STATIONS |
| L1 37 | 12 | 25 | PC 5+944.938 |
| TR 11 | --- | --- | PT 6+120.199 |

| STATION | SUPERELEVATION | |
|-------------|----------------|--------|
| | LEFT | RIGHT |
| A 5+908.938 | -1.50% | -1.50% |
| B 5+919.938 | -1.50% | 0.0% |
| C 5+930.938 | -1.50% | 1.50% |
| D 5+944.938 | -3.49% | 3.49% |
| E 5+956.938 | -5.2% | 5.2% |
| | | |
| E 6+108.199 | -5.2% | 5.2% |
| D 6+120.199 | -3.49% | 3.49% |
| C 6+134.199 | -1.50% | 1.50% |
| B 6+145.199 | -1.50% | 0.0% |
| A 6+156.199 | -1.50% | -1.50% |

CURVE P9 TRANSITION DETAILS

R = 651.393 NC = -1.5% LANE WIDTH = 3.35m

CURVE = RIGHT SE = 4.4%

| | | | |
|---------|-----|-----|----------------|
| ROUNDED | 1/3 | 2/3 | CURVE STATIONS |
| L1 31 | 10 | 21 | PC 6+380.742 |
| TR 11 | --- | --- | PT 6+565.330 |

| STATION | SUPERELEVATION | |
|-------------|----------------|--------|
| | LEFT | RIGHT |
| A 6+348.742 | -1.50% | -1.50% |
| B 6+359.742 | -1.50% | 0.0% |
| C 6+370.742 | -1.50% | 1.50% |
| D 6+380.742 | -2.95% | 2.95% |
| E 6+390.742 | -4.4% | 4.4% |
| | | |
| E 6+555.330 | -4.4% | 4.4% |
| D 6+565.330 | -2.95% | 2.95% |
| C 6+575.330 | -1.50% | 1.50% |
| B 6+586.330 | -1.50% | 0.0% |
| A 6+597.330 | -1.50% | -1.50% |

CURVE P10 TRANSITION DETAILS

R = 802.264 NC = -1.5% LANE WIDTH = 3.35m

CURVE = LEFT SE = 4.8%

| | | | |
|---------|-----|-----|----------------|
| ROUNDED | 1/3 | 2/3 | CURVE STATIONS |
| L1 34 | 11 | 23 | PC 6+986.819 |
| TR 11 | --- | --- | PT 7+118.219 |

| STATION | SUPERELEVATION | |
|-------------|----------------|--------|
| | LEFT | RIGHT |
| A 6+952.819 | -1.50% | -1.50% |
| B 6+963.819 | -1.50% | 0.0% |
| C 6+974.819 | -1.50% | 1.50% |
| D 6+986.819 | -3.22% | 3.22% |
| E 6+997.819 | -4.8% | 4.8% |
| | | |
| E 7+107.219 | -4.8% | 4.8% |
| D 7+118.219 | -3.22% | 3.22% |
| C 7+130.219 | -1.50% | 1.50% |
| B 7+141.219 | -1.50% | 0.0% |
| A 7+152.219 | -1.50% | -1.50% |

CURVE P11 TRANSITION DETAILS

R = 581.332 NC = -1.5% LANE WIDTH = 3.35m

CURVE = LEFT SE = 5.0%

| | | | |
|---------|-----|-----|----------------|
| ROUNDED | 1/3 | 2/3 | CURVE STATIONS |
| L1 36 | 12 | 24 | PC 7+373.093 |
| TR 11 | --- | --- | PT 7+466.210 |

| STATION | SUPERELEVATION | |
|-------------|----------------|--------|
| | LEFT | RIGHT |
| A 7+338.093 | -1.50% | -1.50% |
| B 7+349.093 | -1.50% | 0.0% |
| C 7+360.093 | -1.50% | 1.50% |
| D 7+373.093 | -3.32% | 3.32% |
| E 7+385.093 | -5.0% | 5.0% |
| | | |
| E 7+454.210 | -5.0% | 5.0% |
| D 7+466.210 | -3.32% | 3.32% |
| C 7+479.210 | -1.50% | 1.50% |
| B 7+490.210 | -1.50% | 0.0% |
| A 7+501.210 | -1.50% | -1.50% |

CURVE P16 TRANSITION DETAILS

R = 871.357 NC = -1.5% LANE WIDTH = 3.35m

CURVE = LEFT SE = 4.5%

| | | | |
|---------|-----|-----|----------------|
| ROUNDED | 1/3 | 2/3 | CURVE STATIONS |
| L1 32 | 11 | 21 | PC 11+902.118 |
| TR 11 | --- | --- | PT 12+110.906 |

| STATION | SUPERELEVATION | |
|--------------|----------------|--------|
| | LEFT | RIGHT |
| A 11+870.118 | -1.50% | -1.50% |
| B 11+881.118 | -1.50% | 0.0% |
| C 11+892.118 | -1.50% | 1.50% |
| D 11+902.118 | -2.93% | 2.93% |
| E 11+913.118 | -4.5% | 4.5% |
| | | |
| E 12+099.906 | -4.5% | 4.5% |
| D 12+110.906 | -2.93% | 2.93% |
| C 12+120.906 | -1.50% | 1.50% |
| B 12+131.906 | -1.50% | 0.0% |
| A 12+142.906 | -1.50% | -1.50% |

CURVE P17 TRANSITION DETAILS

R = 867.140 NC = -1.5% LANE WIDTH = 3.35m

CURVE = RIGHT SE = 4.6%

| | | | |
|---------|-----|-----|----------------|
| ROUNDED | 1/3 | 2/3 | CURVE STATIONS |
| L1 33 | 11 | 22 | PC 12+324.152 |
| TR 11 | --- | --- | PT 12+554.276 |

| STATION | SUPERELEVATION | |
|--------------|----------------|--------|
| | LEFT | RIGHT |
| A 12+291.152 | -1.50% | -1.50% |
| B 12+302.152 | -1.50% | 0.0% |
| C 12+313.152 | -1.50% | 1.50% |
| D 12+324.152 | -3.05% | 3.05% |
| E 12+335.152 | -4.6% | 4.6% |
| | | |
| E 12+543.276 | -4.6% | 4.6% |
| D 12+554.276 | -3.05% | 3.05% |
| C 12+565.276 | -1.50% | 1.50% |
| B 12+576.276 | -1.50% | 0.0% |
| A 12+587.276 | -1.50% | -1.50% |

PLOT DATE = 12/22/2008
FILE NAME = P:\V\20\Cont\sect 94533\SHT_082.dwg

H. M. & G. NO. 4420

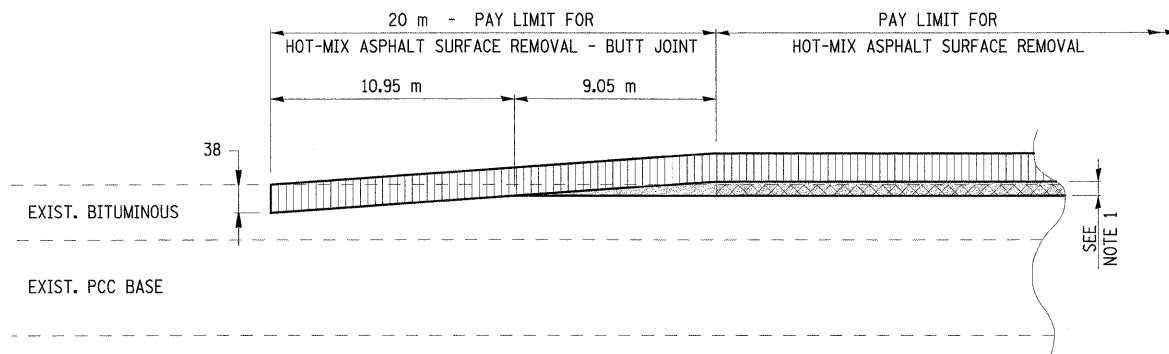
| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUPERELEVATION TRANSITION DATA TABLES

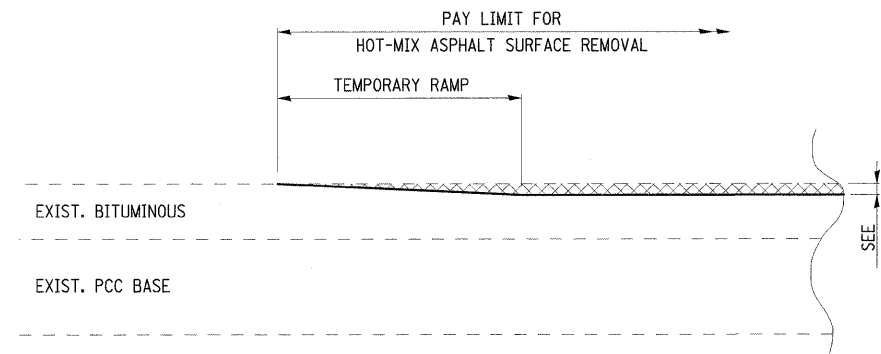
SCALE NO SCALE DRAWN BY KOJ
DATE DECEMBER 22, 2008 CHECKED BY LWJ

| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 84 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



- HOT-MIX ASPHALT SURFACE REMOVAL
- HOT-MIX ASPHALT SURFACE COURSE
- HOT-MIX ASPHALT BINDER COURSE OR LEVEL BINDER

BUTT JOINT DETAIL



- HOT-MIX ASPHALT SURFACE REMOVAL

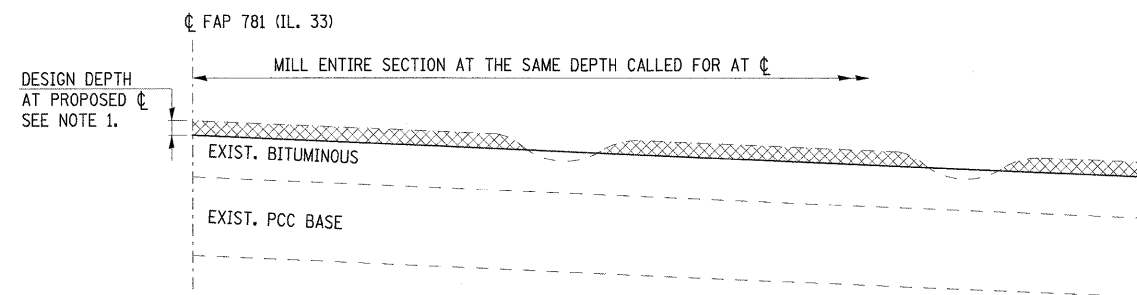
MILLED TEMPORARY RAMP

NOTES

- REFER TO THE HOT-MIX ASPHALT SURFACE REMOVAL DETAILS BELOW AND THE HOT-MIX ASPHALT SURFACE REMOVAL SCHEDULE FOR MILLING THICKNESSES.
- BUTT JOINTS AND TEMPORARY RAMPS SHALL BE PLACED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- BUTT JOINTS SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO THE PLACING OF THE PROPOSED HOT-MIX ASPHALT COURSES.
- TAPER THE TEMPORARY RAMP AT A RATE OF 900mm (3') PER 25mm (1") OF MILLING THICKNESS.
- TEMPORARY RAMPS SHALL BE CONSTRUCTED AT ALL BUTT JOINT LOCATIONS. AT ALL OTHER PAVEMENT TRANSITIONS THE MILLED TEMPORARY RAMP SHALL BE USED. THE INSTALLATION AND SUBSEQUENT REMOVAL OF THE TEMPORARY RAMP WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE METER FOR TEMPORARY RAMP. MILLED TEMPORARY RAMPS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF HOT-MIX ASPHALT SURFACE REMOVAL.

NOTES

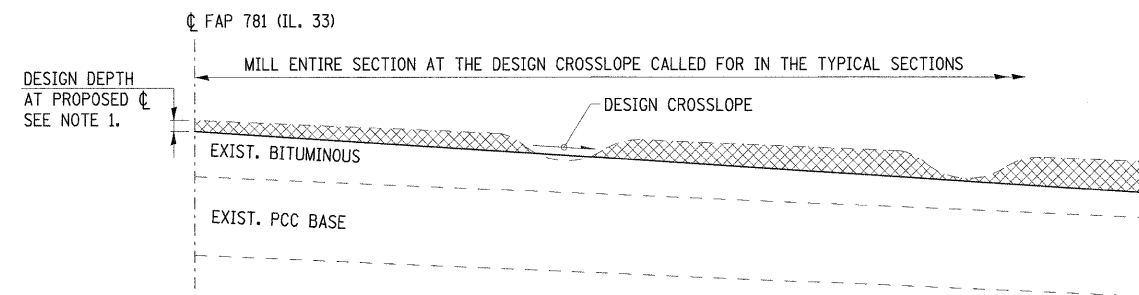
- REFER TO THE HOT-MIX ASPHALT SURFACE REMOVAL SCHEDULE FOR MILLING THICKNESSES AT ϕ AND LOCATIONS WHERE EACH OCCURS.
- MILLING SHALL BE DONE AT THE NOMINAL THICKNESS CALLED FOR AT ϕ ACROSS THE SECTION.
- MILLING TO THE BOTTOM OF WHEEL RUTS SHALL NOT BE NECESSARY UNLESS REQUIRED TO OBTAIN SLOPE OR THE DESIGN DEPTH AT CENTERLINE.
- NOMINAL MILLING AREAS SHALL BE PAID FOR UNDER THE FOLLOWING:
 M4400715 - HOT-MIX ASPHALT SURFACE REMOVAL, 15mm
 M4400725 - HOT-MIX ASPHALT SURFACE REMOVAL, 25mm
 M4400740 - HOT-MIX ASPHALT SURFACE REMOVAL, 40mm



HOT-MIX ASPHALT SURFACE REMOVAL DETAIL (NOMINAL DEPTH)

NOTES

- REFER TO THE HOT-MIX ASPHALT SURFACE REMOVAL SCHEDULE FOR MILLING THICKNESSES AT ϕ AND LOCATIONS WHERE EACH OCCURS.
- MILLING SHALL BE DONE TO ATTAIN THE DESIRED DESIGN CROSSLOPE ACROSS THE SECTION.
- MILLING TO THE BOTTOM OF WHEEL RUTS SHALL NOT BE NECESSARY UNLESS REQUIRED TO OBTAIN SLOPE OR THE DESIGN DEPTH AT CENTERLINE.
- VARIABLE MILLING AREAS SHALL BE PAID FOR UNDER THE FOLLOWING:
 M4400950 - HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH



HOT-MIX ASPHALT SURFACE REMOVAL DETAIL (VARIABLE DEPTH)

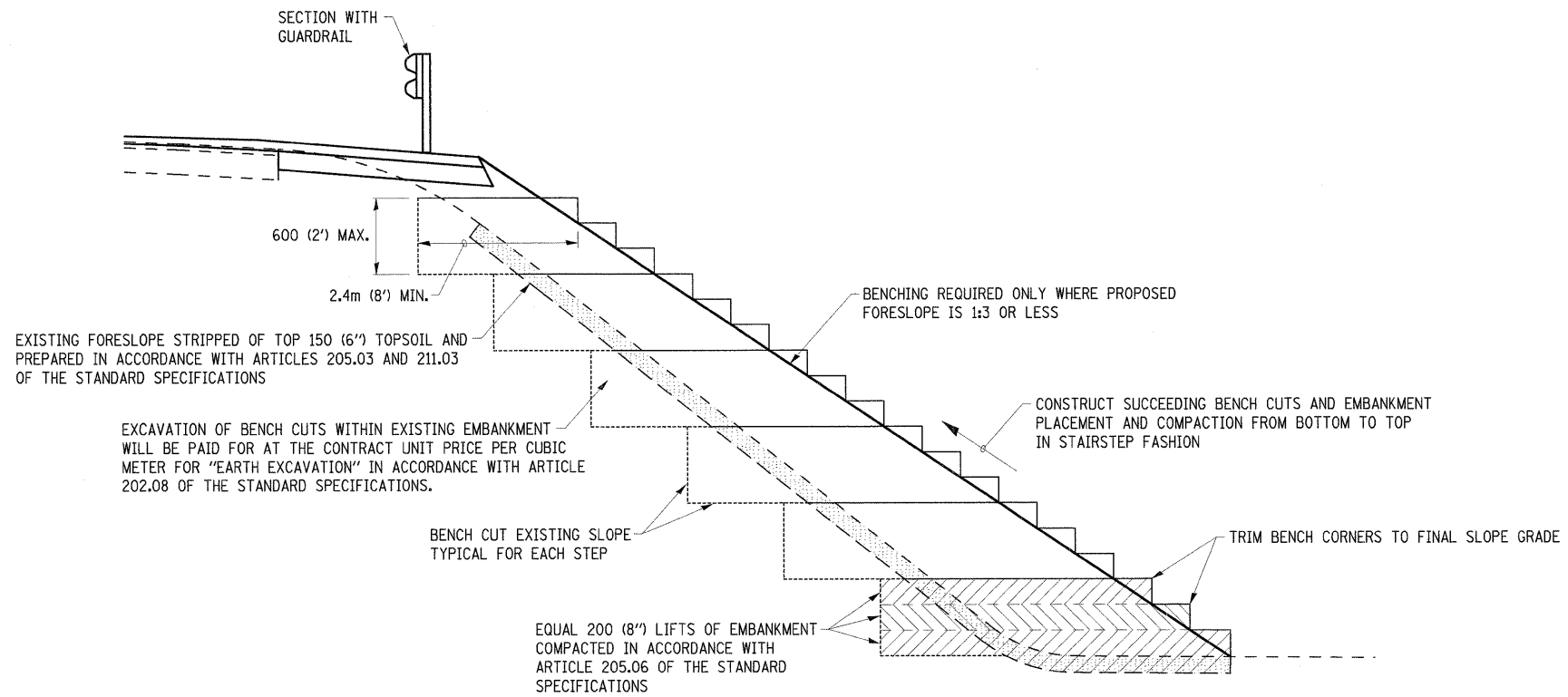
ALL DIMENSIONS ARE IN MILLIMETERS UNLESS OTHERWISE SHOWN.

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|--|
| NAME | DATE | |
| | | BUTT JOINT, TAPER AND ASPHALT MILLING DETAILS SCALE NO SCALE DRAWN BY KOJ DATE DECEMBER 22, 2008 CHECKED BY LWJ |
| | | |
| | | |
| | | |
| | | |
| | | |

PLOT DATE = 12/22/2008
 FILE NAME = P:\1420\Contract\94533\SHT_082.dwg

H. M. & G. NO. 4420

| | | | | |
|---------------------|----------|------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 85 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



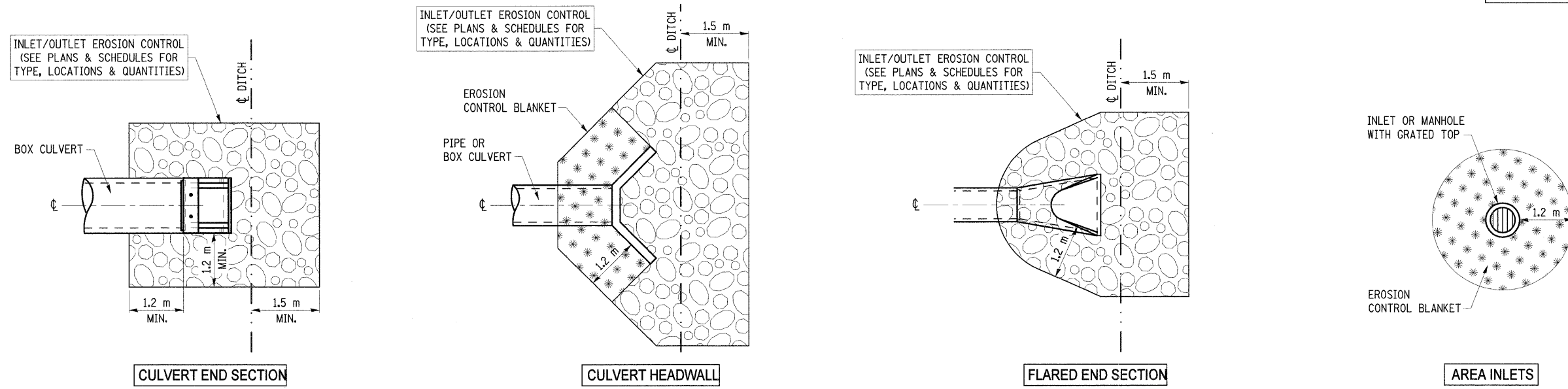
TYPICAL BENCHING DETAIL FOR EMBANKMENTS

PLOT DATE = 12/22/2008
FILE NAME = H:\442\Contract\94533\SH1_082.dwg

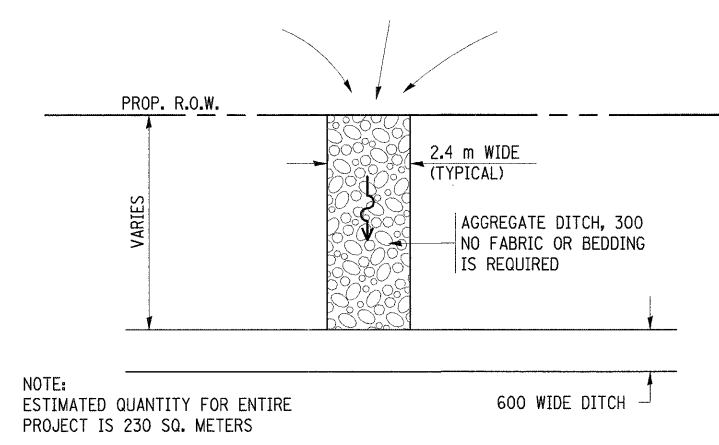
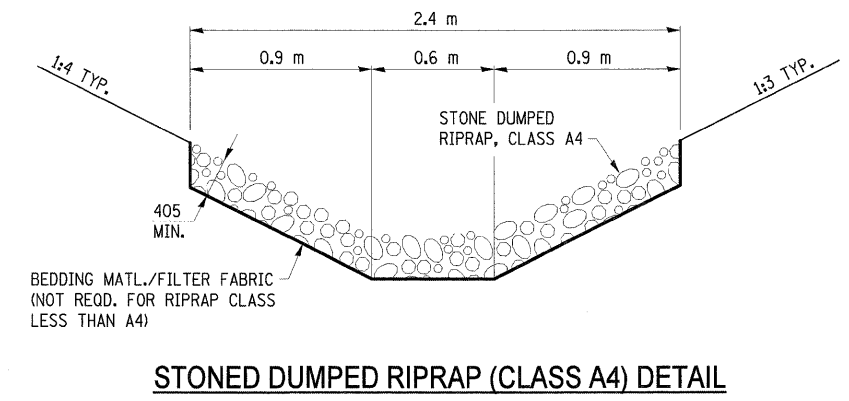
| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|------------------------|------|---------------------------------------|
| NAME | DATE | |
| | | EMBANKMENT BENCHING DETAILS |
| | | |
| | | |
| | | |
| | | |
| | | |
| SCALE NO SCALE | | DRAWN BY KOJ |
| DATE DECEMBER 22, 2008 | | CHECKED BY LWJ |

H. M. & G. NO. 4420

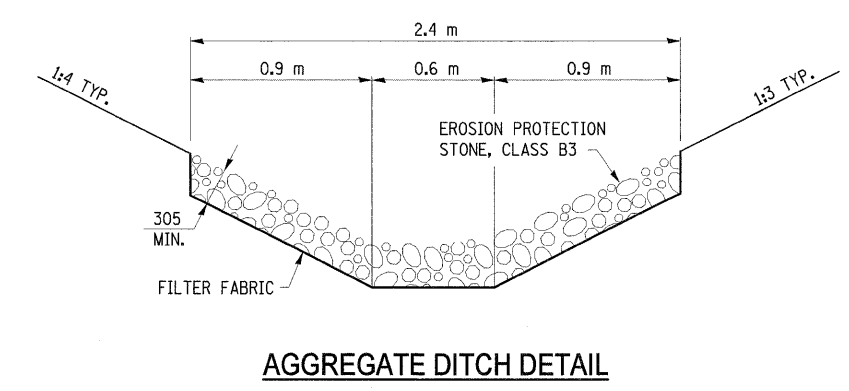
| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 86 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



PERMANENT EROSION CONTROL AT INLET/OUTLET STRUCTURES



NOTE:
ESTIMATED QUANTITY FOR ENTIRE PROJECT IS 230 SQ. METERS



ALL DIMENSIONS ARE IN MILLIMETERS UNLESS OTHERWISE SHOWN.

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION

PERMANENT EROSION CONTROL DETAILS

SCALE NO SCALE DRAWN BY KOJ
DATE DECEMBER 22, 2008 CHECKED BY LWJ

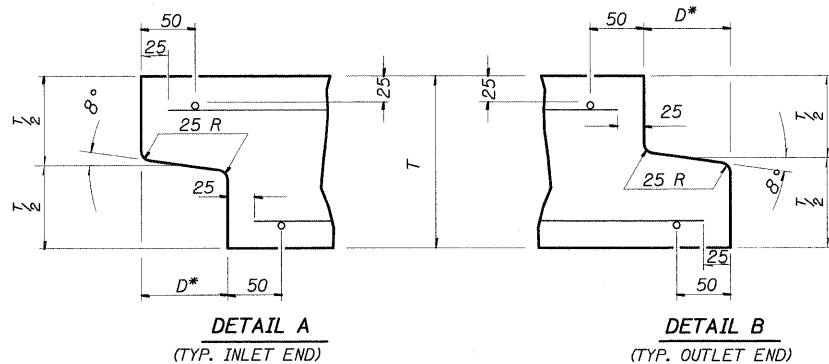
PLOT DATE = 12/22/2008
FILE NAME = P:\V\20\Contract\est_94533\SH1_082.dwg

H. M. & G. NO. 4420

| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 87 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

DETAIL OF PRECAST CONCRETE BOX CULVERT M259M

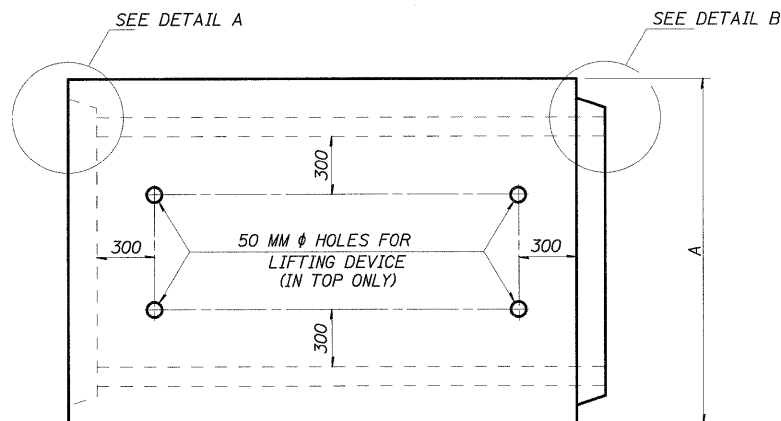
AASHO DESIGNATION M259M
(ASTM DESIGNATION C-789M)
DESIGN LOADING = MS18



DETAIL A
(TYP. INLET END)
DETAIL B
(TYP. OUTLET END)

NOTE: INLET AND OUTLET ENDS SHALL BE COMPATIBLE.

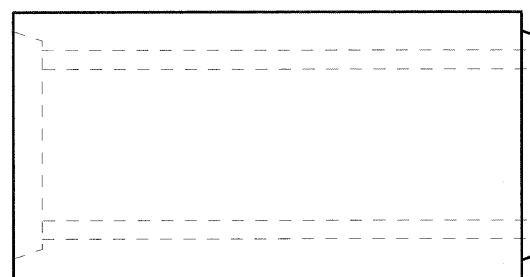
* THE D DIMENSION SHALL CONFORM TO THE MANUFACTURER'S STANDARDS.



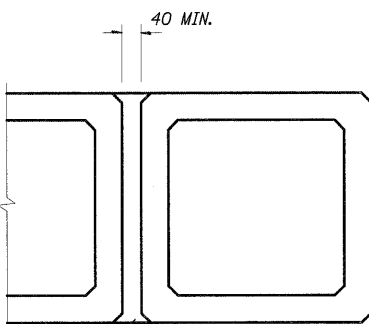
PLAN
LOCATION OF LIFTING HOLES MAY BE VARIED AS NEEDED TO CLEAR REINFORCEMENT.

SECTION LENGTH

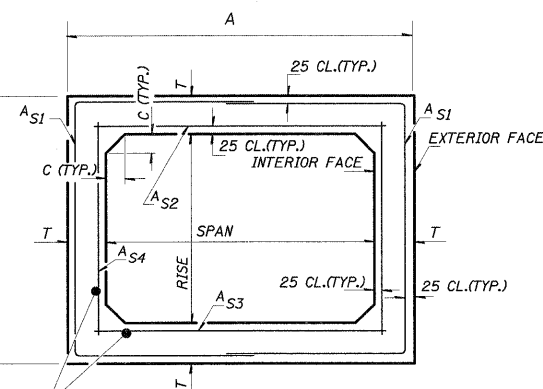
WHEN EXTENDING AN EXISTING BOX, PLACE THIS END AGAINST THE EXISTING HEADWALL.



ELEVATION



CLASS SI CONCRETE
MULTIPLE UNIT PLACEMENT



LONGITUDINAL REINFORCEMENT
CROSS SECTION

GENERAL NOTES

SHOP PLANS FOR THE REINFORCEMENT SHALL BE SUBMITTED IN ACCORDANCE WITH THE REQUIREMENTS OF ARTICLE 504.04(c) OF THE STANDARD SPECIFICATIONS.

MINIMUM CONCRETE STRENGTH SHALL BE 35 MPa AFTER 28 DAYS.

THE JOINTS OF THE PRECAST BOX SECTIONS SHALL BE SEALED IN ACCORDANCE WITH ARTICLE 542.04(d) OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

THE TERMS A_{S1}, A_{S3}, AND A_{S4} DENOTE THE REQUIRED STEEL AREAS FOR REINFORCEMENT AS SPECIFIED IN ASSHTO M259M. REINFORCEMENT SHALL BE WELDED WIRE FABRIC CONFORMING TO ASSHTO M55M.

LIFTING HOLES SHALL BE FILLED WITH CONCRETE PLUGS AND MASTIC AFTER THE BOX SECTIONS ARE IN PLACE.

DRAINAGE OPENINGS SHALL BE PROVIDED IN ACCORDANCE WITH ARTICLE 503.12 OF THE STANDARD SPECIFICATIONS. LOCATION AND SPACING OF THE OPENINGS SHALL BE SHOWN ON THE SHOP DRAWINGS.

ALL DIMENSIONS ARE DETAILED IN MILLIMETERS UNLESS OTHERWISE NOTED.

| CULVERT SIZE | | DIMENSIONS | | | |
|-------------------|-------------------|---------------|---------|---------|---------------|
| NOMINAL | ACTUAL | T | A | B | C |
| SPANxRISE (METER) | SPANxRISE (METER) | (MILLIMETERS) | (METER) | (METER) | (MILLIMETERS) |
| 0.6x0.6 | 0.610x0.610 | 100 | 0.810 | 0.810 | 100 |
| 0.9x0.6 | 0.914x0.610 | 100 | 1.114 | 0.810 | 100 |
| 0.9x0.9 | 0.914x0.914 | 100 | 1.114 | 1.114 | 100 |
| 1.2x0.6 | 1.219x0.610 | 125 | 1.469 | 0.860 | 125 |
| 1.2x0.9 | 1.219x0.914 | 125 | 1.469 | 1.164 | 125 |
| 1.2x1.2 | 1.219x1.219 | 125 | 1.469 | 1.469 | 125 |
| 1.5x0.6 | 1.524x0.610 | 150 | 1.824 | 0.910 | 150 |
| 1.5x0.9 | 1.524x0.914 | 150 | 1.824 | 1.214 | 150 |
| 1.5x1.2 | 1.524x1.219 | 150 | 1.824 | 1.519 | 150 |
| 1.5x1.5 | 1.524x1.524 | 150 | 1.824 | 1.824 | 150 |
| 1.8x0.6 | 1.829x0.610 | 175 | 2.179 | 0.960 | 175 |
| 1.8x0.9 | 1.829x0.914 | 175 | 2.179 | 1.264 | 175 |
| 1.8x1.2 | 1.829x1.219 | 175 | 2.179 | 1.569 | 175 |
| 1.8x1.5 | 1.829x1.524 | 175 | 2.179 | 1.874 | 175 |
| 1.8x1.8 | 1.829x1.829 | 175 | 2.179 | 2.179 | 175 |
| 2.1x1.2 | 2.134x1.219 | 200 | 2.534 | 1.619 | 200 |
| 2.1x1.5 | 2.134x1.524 | 200 | 2.534 | 1.924 | 200 |
| 2.1x1.8 | 2.134x1.829 | 200 | 2.534 | 2.229 | 200 |
| 2.1x2.1 | 2.134x2.134 | 200 | 2.534 | 2.534 | 200 |
| 2.4x1.2 | 2.438x1.219 | 200 | 2.838 | 1.619 | 200 |
| 2.4x1.5 | 2.438x1.524 | 200 | 2.838 | 1.924 | 200 |
| 2.4x1.8 | 2.438x1.829 | 200 | 2.838 | 2.229 | 200 |
| 2.4x2.1 | 2.438x2.134 | 200 | 2.838 | 2.534 | 200 |
| 2.4x2.4 | 2.438x2.438 | 200 | 2.838 | 2.838 | 200 |
| 2.7x1.5 | 2.743x1.524 | 225 | 3.193 | 1.974 | 225 |
| 2.7x1.8 | 2.743x1.829 | 225 | 3.193 | 2.279 | 225 |
| 2.7x2.1 | 2.743x2.134 | 225 | 3.193 | 2.584 | 225 |
| 2.7x2.4 | 2.743x2.438 | 225 | 3.193 | 2.888 | 225 |
| 2.7x2.7 | 2.743x2.743 | 225 | 3.193 | 3.193 | 225 |
| 3.0x1.5 | 3.048x1.524 | 250 | 3.548 | 2.024 | 250 |
| 3.0x1.8 | 3.048x1.829 | 250 | 3.548 | 2.329 | 250 |
| 3.0x2.1 | 3.048x2.134 | 250 | 3.548 | 2.634 | 250 |
| 3.0x2.4 | 3.048x2.438 | 250 | 3.548 | 2.938 | 250 |
| 3.0x2.7 | 3.048x2.743 | 250 | 3.548 | 3.243 | 250 |
| 3.0x3.0 | 3.048x3.048 | 250 | 3.548 | 3.548 | 250 |
| 3.3x1.2 | 3.353x1.219 | 275 | 3.903 | 1.769 | 275 |
| 3.3x1.8 | 3.353x1.829 | 275 | 3.903 | 2.379 | 275 |
| 3.3x2.4 | 3.353x2.438 | 275 | 3.903 | 2.988 | 275 |
| 3.3x3.0 | 3.353x3.048 | 275 | 3.903 | 3.598 | 275 |
| 3.3x3.3 | 3.353x3.353 | 275 | 3.903 | 3.903 | 275 |
| 3.6x1.2 | 3.658x1.219 | 300 | 4.258 | 1.819 | 300 |
| 3.6x1.8 | 3.658x1.829 | 300 | 4.258 | 2.429 | 300 |
| 3.6x2.4 | 3.658x2.438 | 300 | 4.258 | 3.038 | 300 |
| 3.6x3.0 | 3.658x3.048 | 300 | 4.258 | 3.648 | 300 |
| 3.6x3.6 | 3.658x3.658 | 300 | 4.258 | 4.258 | 300 |

REVISIONS

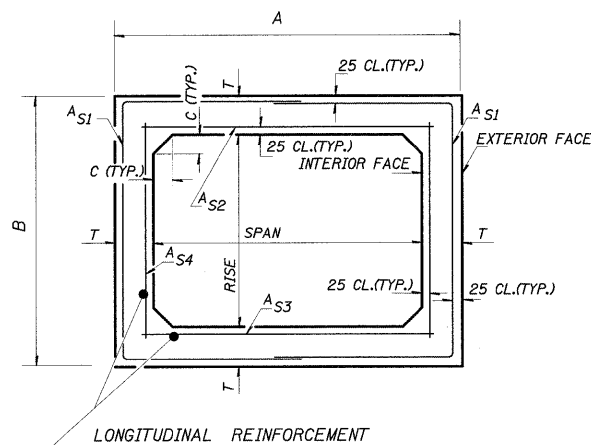
| NAME | DATE |
|------|------|
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETAIL OF PRECAST CONCRETE BOX CULVERT SECTION

SCALE NO SCALE
DATE DECEMBER 22, 2008
DRAWN BY KOJ
CHECKED BY LWJ

| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | LYRS | CRAWFORD | 212 | 89 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

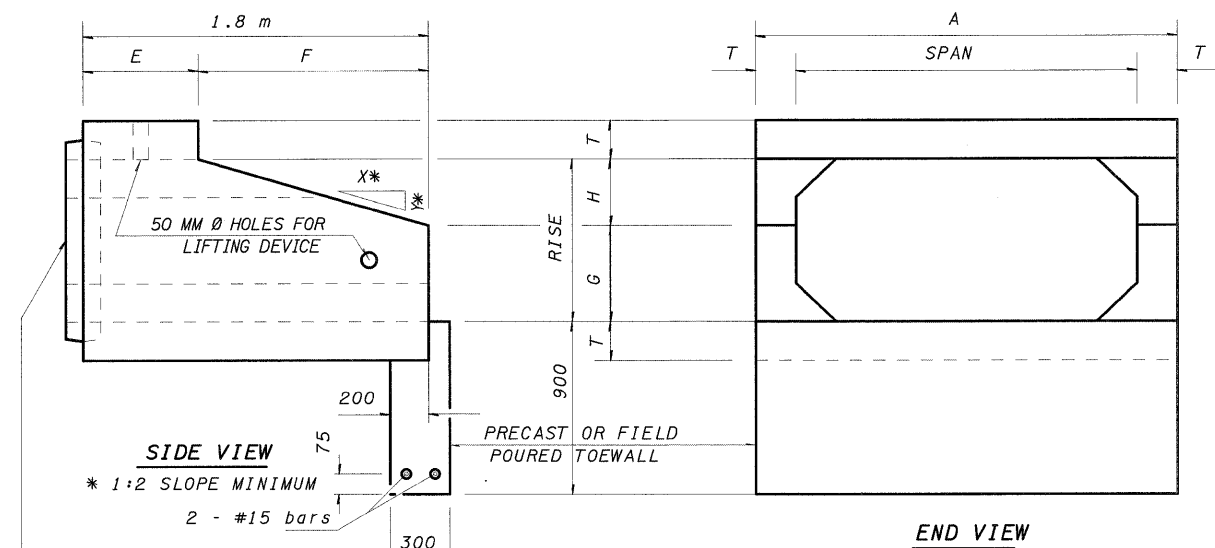
DETAIL OF PRECAST CONCRETE BOX CULVERT END SECTION



CROSS SECTION

AASHTO DESIGNATION M259M
(ASTM DESIGNATION C-789M)
DESIGN LOADING = MS18

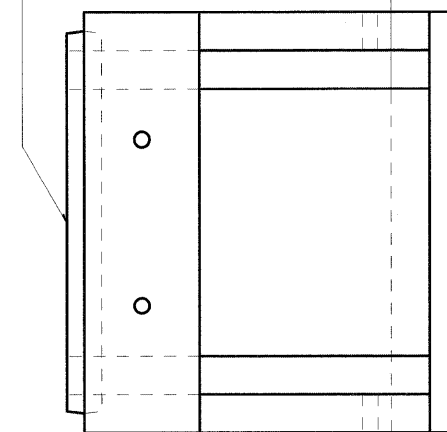
** NOTE: THE DIMENSIONS INDICATED ARE FOR END SECTIONS THAT ARE TO BE USED WITH PRECAST BOX CULVERT SECTIONS DESIGNED FOR 600 MM OR MORE OF FILL. THE DIMENSIONS MUST BE MODIFIED FOR THE END SECTION TO BE COMPATIBLE WITH PRECAST CULVERT SECTIONS DESIGNED FOR LESS THAN 600 MM OF FILL.



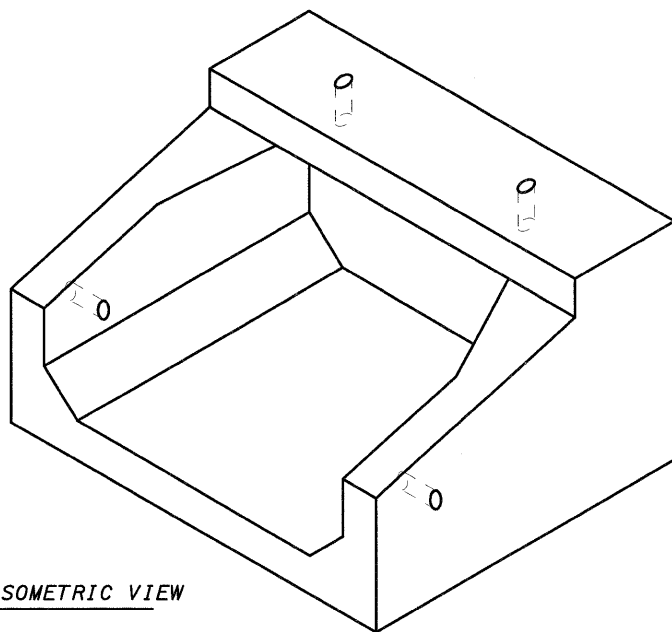
SIDE VIEW

END VIEW

END CONNECTION TO FIT PRECAST BOX CULVERT (BELL OR SPIGOT MAY BE OMITTED WHEN COLLARING TO AN EXISTING BOX OR HEADWALL).



PLAN



ISOMETRIC VIEW

| CULVERT SIZE | | DIMENSIONS ** | | | | | | | | |
|-------------------|-------------------|---------------|---------|---------|------|---------|---------|---------|---------|-------|
| NOMINAL | ACTUAL | T | A | B | C | E | F | G | H | SLOPE |
| SPANxRISE (METER) | SPANxRISE (METER) | (MM) | (METER) | (METER) | (MM) | (METER) | (METER) | (METER) | (METER) | (Y:X) |
| 0.6x0.6 | 0.610x0.610 | 100 | 0.810 | 0.810 | 100 | 0.900 | 0.900 | 0.310 | 0.300 | 1:3 |
| 0.9x0.6 | 0.914x0.610 | 100 | 1.114 | 0.810 | 100 | 0.900 | 0.900 | 0.310 | 0.300 | 1:3 |
| 0.9x0.9 | 0.914x0.914 | 100 | 1.114 | 1.114 | 100 | 0.600 | 1.200 | 0.514 | 0.400 | 1:3 |
| 1.2x0.6 | 1.219x0.610 | 125 | 1.469 | 0.860 | 125 | 0.900 | 0.900 | 0.310 | 0.300 | 1:3 |
| 1.2x0.9 | 1.219x0.914 | 125 | 1.469 | 1.164 | 125 | 0.600 | 1.200 | 0.514 | 0.400 | 1:3 |
| 1.2x1.2 | 1.219x1.219 | 125 | 1.469 | 1.469 | 125 | 0.600 | 1.200 | 0.619 | 0.600 | 1:2 |
| 1.5x0.6 | 1.524x0.610 | 125 | 1.774 | 0.860 | 150 | 0.900 | 0.900 | 0.310 | 0.300 | 1:3 |
| 1.5x0.9 | 1.524x0.914 | 150 | 1.824 | 1.214 | 150 | 0.600 | 1.200 | 0.514 | 0.400 | 1:3 |
| 1.5x1.2 | 1.524x1.219 | 150 | 1.824 | 1.519 | 150 | 0.600 | 1.200 | 0.619 | 0.600 | 1:2 |
| 1.5x1.5 | 1.524x1.524 | 150 | 1.824 | 1.824 | 150 | 0.600 | 1.200 | 0.924 | 0.600 | 1:2 |
| 1.8x0.6 | 1.829x0.610 | 175 | 2.179 | 0.960 | 175 | 0.900 | 0.900 | 0.310 | 0.300 | 1:3 |
| 1.8x0.9 | 1.829x0.914 | 175 | 2.179 | 1.264 | 175 | 0.600 | 1.200 | 0.514 | 0.400 | 1:3 |
| 1.8x1.2 | 1.829x1.219 | 175 | 2.179 | 1.569 | 175 | 0.600 | 1.200 | 0.619 | 0.600 | 1:2 |
| 1.8x1.5 | 1.829x1.524 | 175 | 2.179 | 1.874 | | | | | | |
| 1.8x1.8 | 1.829x1.829 | 175 | 2.179 | 2.179 | | | | | | |
| 2.1x1.2 | 2.134x1.219 | 200 | 2.534 | 1.619 | 200 | 0.600 | 1.200 | 0.619 | 0.600 | 1:2 |
| 2.1x1.5 | 2.134x1.524 | 200 | 2.534 | 1.924 | | | | | | |
| 2.1x1.8 | 2.134x1.829 | 200 | 2.534 | 2.229 | | | | | | |
| 2.1x2.1 | 2.134x2.134 | 200 | 2.534 | 2.534 | | | | | | |
| 2.4x1.2 | 2.438x1.219 | 200 | 2.838 | 1.619 | 200 | 0.600 | 1.200 | 0.619 | 0.600 | 1:2 |
| 2.4x1.5 | 2.438x1.524 | 200 | 2.838 | 1.924 | | | | | | |
| 2.4x1.8 | 2.438x1.829 | 200 | 2.838 | 2.229 | | | | | | |
| 2.4x2.1 | 2.438x2.134 | 200 | 2.838 | 2.534 | | | | | | |
| 2.4x2.4 | 2.438x2.438 | 200 | 2.838 | 2.838 | | | | | | |
| 2.7x1.5 | 2.743x1.524 | 225 | 3.193 | 1.974 | | | | | | |
| 2.7x1.8 | 2.743x1.829 | 225 | 3.193 | 2.279 | | | | | | |
| 2.7x2.1 | 2.743x2.134 | 225 | 3.193 | 2.584 | | | | | | |
| 2.7x2.4 | 2.743x2.438 | 225 | 3.193 | 2.888 | | | | | | |
| 2.7x2.7 | 2.743x2.743 | 225 | 3.193 | 3.193 | | | | | | |
| 3.0x1.5 | 3.048x1.524 | 250 | 3.548 | 2.024 | | | | | | |
| 3.0x1.8 | 3.048x1.829 | 250 | 3.548 | 2.329 | | | | | | |
| 3.0x2.1 | 3.048x2.134 | 250 | 3.548 | 2.634 | | | | | | |
| 3.0x2.4 | 3.048x2.438 | 250 | 3.548 | 2.938 | | | | | | |
| 3.0x2.7 | 3.048x2.743 | 250 | 3.548 | 3.243 | | | | | | |
| 3.0x3.0 | 3.048x3.048 | 250 | 3.548 | 3.548 | | | | | | |
| 3.3x1.2 | 3.353x1.219 | 275 | 3.903 | 1.769 | | | | | | |
| 3.3x1.8 | 3.353x1.829 | 275 | 3.903 | 2.379 | | | | | | |
| 3.3x2.4 | 3.353x2.438 | 275 | 3.903 | 2.988 | | | | | | |
| 3.3x3.0 | 3.353x3.048 | 275 | 3.903 | 3.598 | | | | | | |
| 3.3x3.3 | 3.353x3.353 | 275 | 3.903 | 3.903 | | | | | | |
| 3.6x1.2 | 3.658x1.219 | 300 | 4.258 | 1.819 | | | | | | |
| 3.6x1.8 | 3.658x1.829 | 300 | 4.258 | 2.429 | | | | | | |
| 3.6x2.4 | 3.658x2.438 | 300 | 4.258 | 3.038 | | | | | | |
| 3.6x3.0 | 3.658x3.048 | 300 | 4.258 | 3.648 | | | | | | |
| 3.6x3.6 | 3.658x3.658 | 300 | 4.258 | 4.258 | | | | | | |

GENERAL NOTES

SHOP PLANS FOR THE REINFORCEMENT SHALL BE SUBMITTED IN ACCORDANCE WITH THE REQUIREMENTS OF ARTICLE 504.04(d) OF THE STANDARD SPECIFICATIONS.

MINIMUM CONCRETE STRENGTH SHALL BE 35 MPa AFTER 28 DAYS.

THE JOINTS OF THE PRECAST BOX SECTIONS SHALL BE SEALED WITH ACCORDANCE WITH ARTICLE 542.04(d) OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

THE TERMS AS1, AS2, AS3, AND AS4 DENOTE THE REQUIRED STEEL AREAS FOR REINFORCEMENT AS SPECIFIED IN AASHTO M259M. REINFORCEMENT SHALL BE WELDED WIRE FABRIC CONFORMING TO AASHTO SPECIFICATIONS M55M.

ALL DIMENSIONS ARE DETAILED IN MILLIMETERS UNLESS OTHERWISE NOTED.

LIFTING HOLES SHALL BE FILLED WITH CONCRETE PLUGS AND MASTIC AFTER THE END SECTIONS ARE IN PLACE.

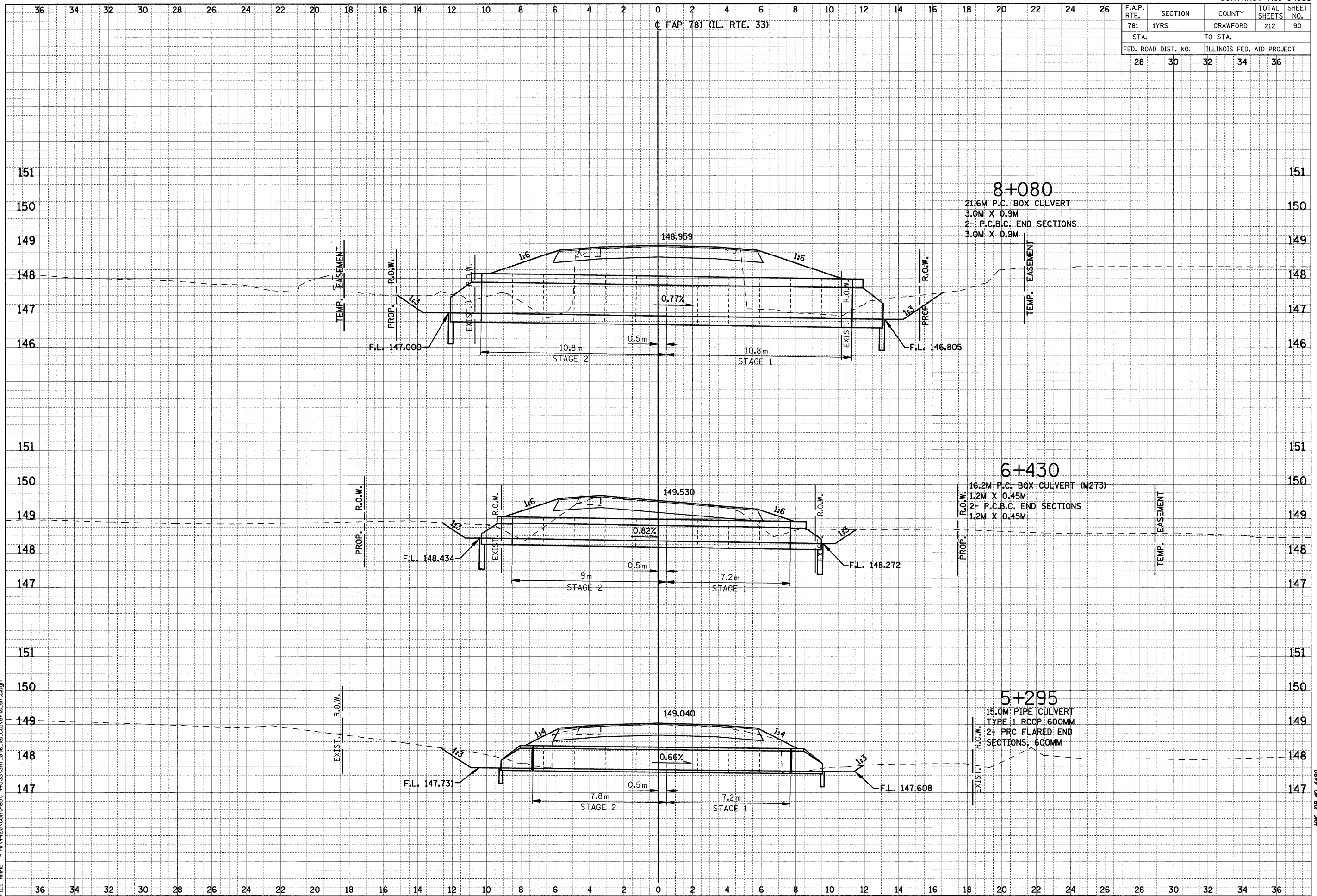
| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|-------------------|--|
| NAME | DATE | |
| | | DETAIL OF PRECAST CONCRETE BOX CULVERT END SECTION |
| | | |
| | | |
| | | |
| | | |
| SCALE | NO SCALE | DRAWN BY KOJ |
| DATE | DECEMBER 22, 2008 | CHECKED BY LWJ |

| | | | | |
|---------------------|---------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 90 |
| STA. | | TO STA. | | |
| 28 | | 30 32 34 36 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

| | | |
|--------------|----|------|
| FINAL SURVEY | BY | DATE |
| SURVEYED | | |
| PLOTTED | | |
| NOTE BOOK | | |
| AREAS | | |
| CHECKED | | |

| | | |
|-----------------|----|------|
| ORIGINAL SURVEY | BY | DATE |
| SURVEYED | | |
| PLOTTED | | |
| NOTE BOOK | | |
| AREAS | | |
| CHECKED | | |

PLOT DATE = 12/22/2008
 FILE NAME = H:\4420 Contract 94533\SHL_098_x.s.Culverts.shldgn



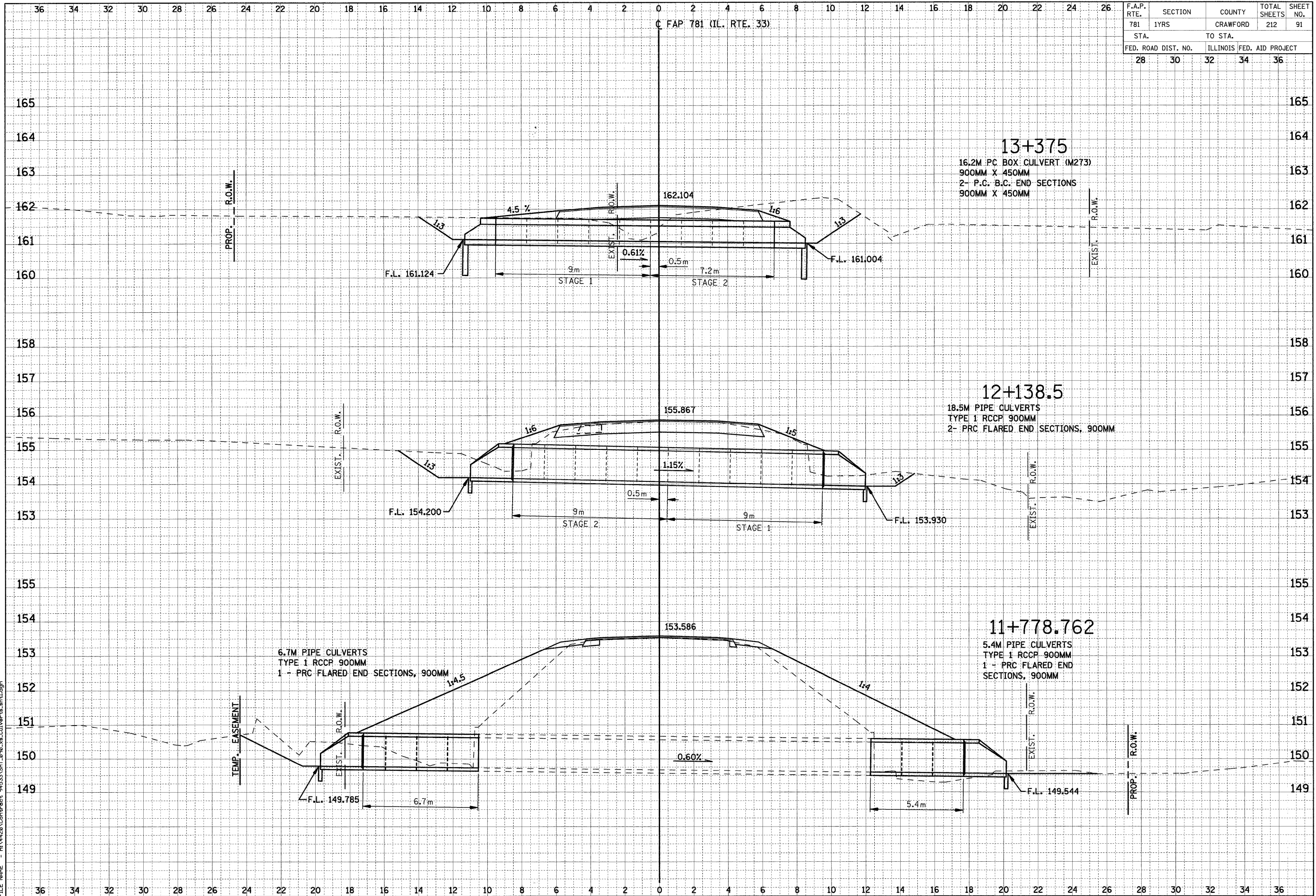
HMG JOB NO. 4420

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|---------------------------|--------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 91 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| 28 30 | | 32 34 36 | | |

| DATE | BY |
|------|----|
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

| DATE | BY |
|------|----|
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |

PLOT DATE = 12/22/2008
FILE NAME = H:\4420 Contract 94533\SR_099.xls\Culverts.sht.dgn



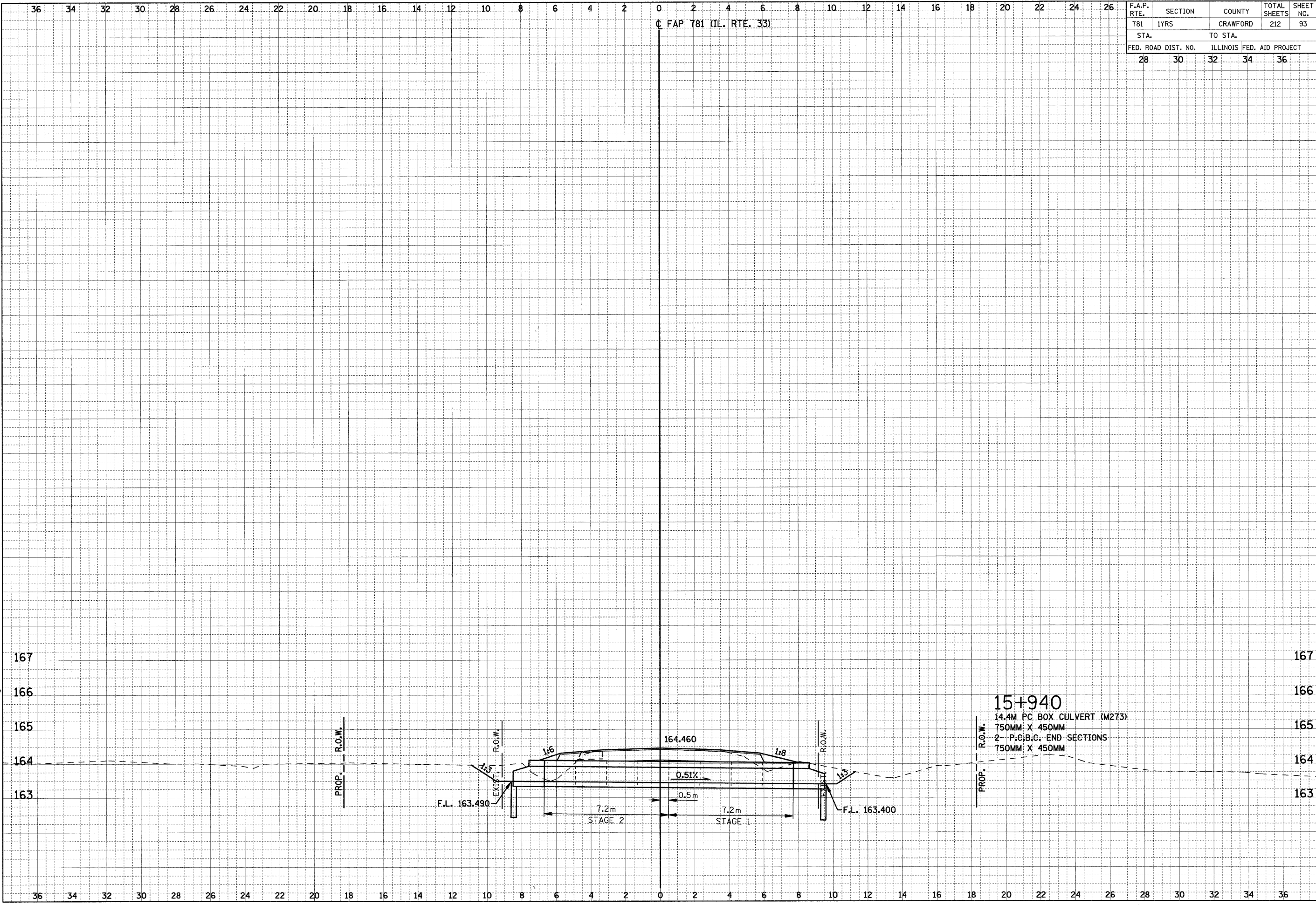
HMG JOB NO. 4420

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------------------|----------|--------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 93 |
| STA. | | | TO STA. | |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | | |
| 28 | 30 | 32 | 34 | 36 |

| | | |
|---------------|----|------|
| FINAL SURVEY | BY | DATE |
| NOTE BOOK | | |
| AREAS | | |
| AREAS CHECKED | | |

| | | |
|-----------------|----|------|
| ORIGINAL SURVEY | BY | DATE |
| NOTE BOOK | | |
| AREAS | | |
| AREAS CHECKED | | |

PLOT DATE = 12/22/2008
 FILE NAME = H:\4428\Contract 94533\SH_099_xe\Culverts.sht.dgn



HMG JOB NO. 4420

| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | LYRS | CRAWFORD | 212 | 94 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

GENERAL NOTES

- Plan dimensions and details relative to existing structure have been taken from existing plans and are subject to nominal construction variations. It shall be the Contractor's responsibility to verify such dimensions and details in the field and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in the scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
- Expansion bolts shall consist of approved expansion anchors, providing minimum certified proof load = 18.15 kN, and 20 mm ϕ hooked bolts shall extend a minimum of 250 mm into new concrete.
- Reinforcement bars shall conform to the requirements of AASHTO M-31M or M-322M, Grade 400.
- Reinforcement bars designated (E) shall be epoxy coated.
- All dimensions are in millimeters (mm) except as noted.
- Precast reinforced concrete box sections shall conform to AASHTO M-259M and M-273M.
- Layout of the slope protection system may be varied to suit ground conditions in the field as directed by the Engineer.

TOTAL BILL OF MATERIAL

(Box Culvert Nos. 8, 10, 11, 15, 16 & 17)

| | Item | Unit | Total | |
|----------|---|---|-----------|------|
| M5030350 | Concrete Structures | cu. m. | 3.9 | |
| M5080205 | Reinforcement Bars, Epoxy Coated | kg | 690 | |
| * | Precast Concrete Box Culvert (3000x1650) | m | 10.9 | |
| M5401010 | Precast Concrete Box Culvert (900x600) | m | 5.7 | |
| * | Precast Concrete Box Culvert (900x450) | m | 10.0 | |
| * | M5401005 | Precast Concrete Box Culvert (600x600) | m | 5.7 |
| * | M5401002 | Precast Concrete Box Culvert (600x450) | m | 15.1 |
| * | 5401008 | Box Culvert End Section, Culvert No. 8 | Each | 2 |
| * | 5401010 | Box Culvert End Section, Culvert No. 10 | Each | 2 |
| * | 5401011 | Box Culvert End Section, Culvert No. 11 | Each | 2 |
| * | 5401015 | Box Culvert End Section, Culvert No. 15 | Each | 2 |
| * | 5401016 | Box Culvert End Section, Culvert No. 16 | Each | 2 |
| * | 5401017 | Box Culvert End Section, Culvert No. 17 | Each | 2 |
| M5403220 | Expansion Bolts (M20x250) | Each | 161 | |
| M2810707 | Stone Riprap, Class A4 | sq. m. | See Plans | |
| M5050405 | Furnishing & Erecting Structural Steel | kg | 130 | |
| MX033694 | Structural Repair of Concrete (Depth Equal to or Less than 125mm) | sq. m. | 2.5 | |
| M5900200 | Epoxy Crack Injection | m | 3.6 | |
| MX032196 | Polymer Modified Portland Cement Mortar | sq. m. | 17.9 | |

* Non-standard size.

INDEX OF SHEETS

| | |
|-----|--|
| S1 | Gen. Notes & Bill of Material |
| S2 | General Plan & Elevation, Box Culvert No. 8 |
| S3 | Concrete Repair Details, Box Culvert No. 8 |
| S4 | General Plan & Elevation, Box Culvert No. 10 |
| S5 | Concrete Repair Details, Box Culvert No. 10 |
| S6 | General Plan & Elevation, Box Culvert No. 11 |
| S7 | Concrete Repair Details, Box Culvert No. 11 |
| S8 | General Plan & Elevation, Box Culvert No. 15 |
| S9 | General Plan & Elevation, Box Culvert No. 16 |
| S10 | General Plan & Elevation, Box Culvert No. 17 |
| S11 | Concrete Repair Details, Box Culvert No. 17 |
| S12 | Precast Section Details |
| S13 | Precast End Section Details |
| S14 | Cast-In-Place Collar Details |
| S15 | Miscellaneous Details |

DESIGN SPECIFICATIONS

2002 AASHTO Specifications

DESIGN STRESSES (New Construction)

| Field Units | Precast Units |
|------------------------------|--------------------------------------|
| $f'_c = 24$ MPa | $f'_c = 35$ MPa |
| $f_y = 400$ MPa (Reinf.) | $f_y = 450$ MPa (Welded wire fabric) |
| $f_y = 250$ MPa (Str. Steel) | |

LOADING MS18

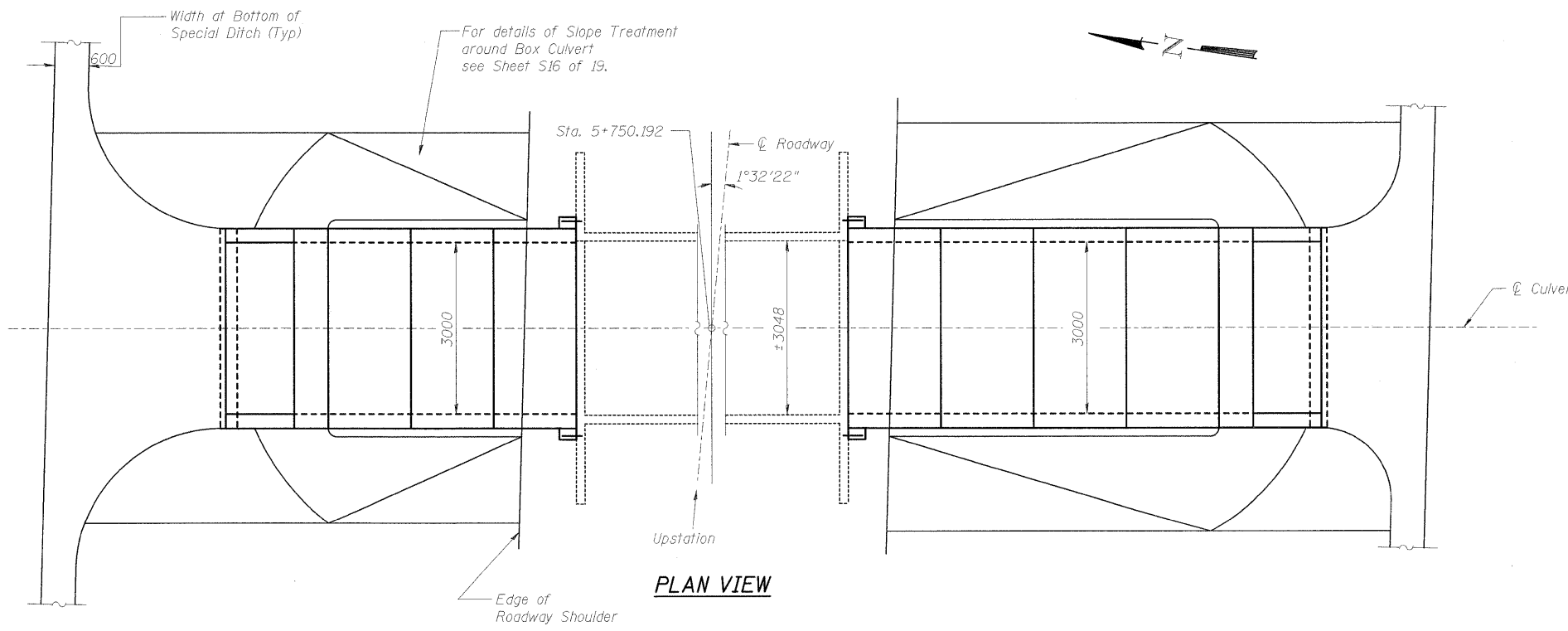
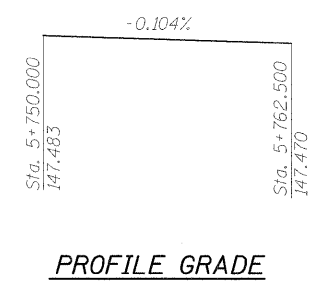
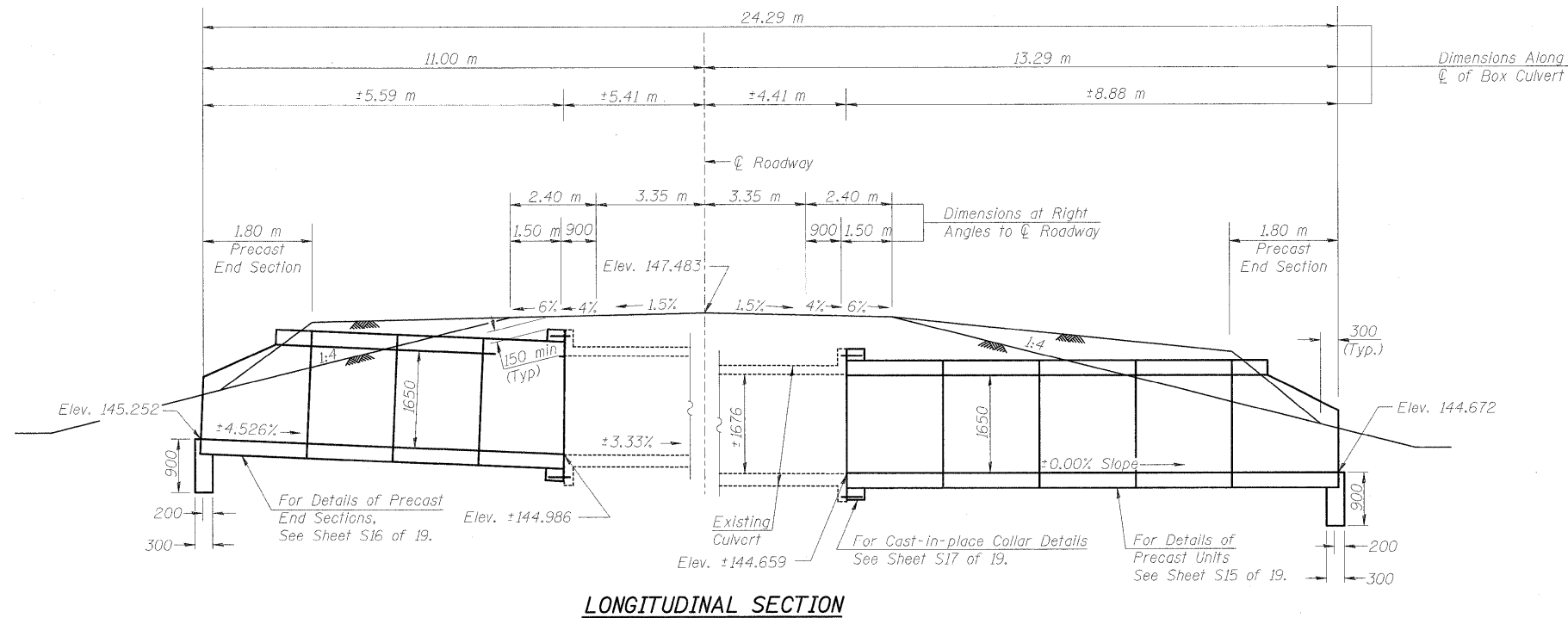
Allow 1.2 kN/m² for future wearing surface

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION BOX CULVERT EXTENSIONS GEN. NOTES & BILL OF MATERIAL |
|-----------|----------|--|
| NAME | DATE | |
| LWJ | 12-18-08 | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |

SCALE NONE DRAWN BY MLB
DATE DECEMBER 10, 2006 CHECKED BY MTH

| | | | | |
|---------------------|---------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 95 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

Existing Structure: The structure is a single barrel 3048 mm x 1676 mm box culvert at a skew of 1°32'22" right forward. The culvert has an overall length of 9.82 m. The contractor shall extend the culvert on each end for the dimensions shown.



BILL OF MATERIAL - BOX CULVERT NO. 8

| Item | Unit | Total |
|--|--------|-----------|
| M5030350 Concrete Structures | cu. m. | 1.5 |
| M5080205 Reinforcement Bars, Epoxy Coated | kg | 220 |
| Precast Concrete Box Culvert (3000x1650) | m | 10.9 |
| * 5401008 Box Culvert End Section, Culvert No. 8 | Each | 2 |
| M5403220 Expansion Bolts (M20x250) | Each | 52 |
| M2810707 Stone Riprap, Class A4 | sq. m. | See Plans |

* Non-standard size.

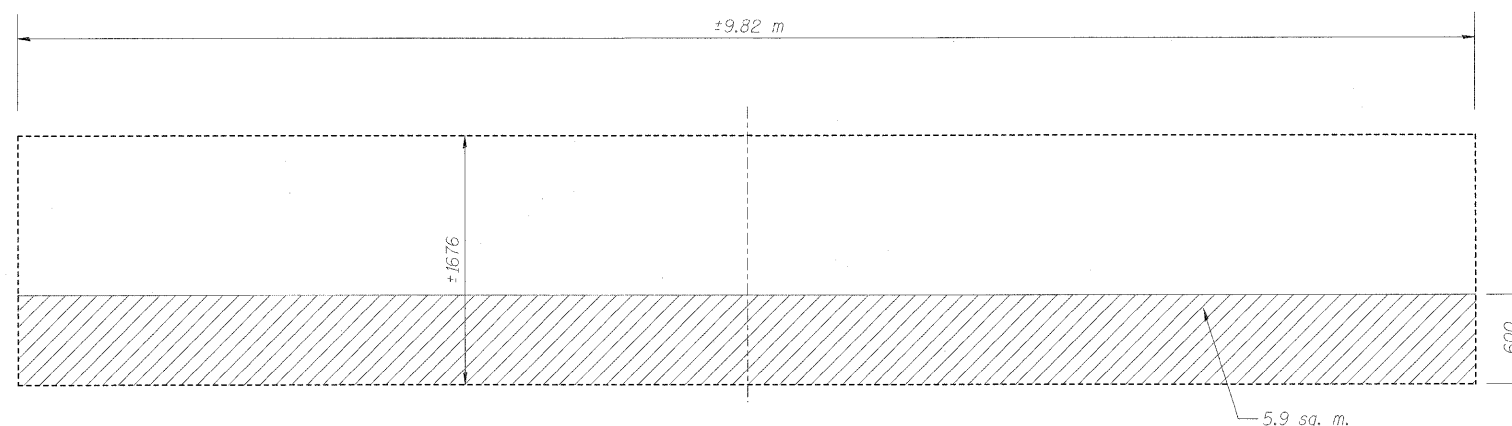
| REVISIONS | |
|-----------|----------|
| NAME | DATE |
| LWJ | 12-18-08 |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
BOX CULVERT EXTENSIONS
GENERAL PLAN & ELEVATION
BOX CULV. NO. 8, STA. 5+750.192

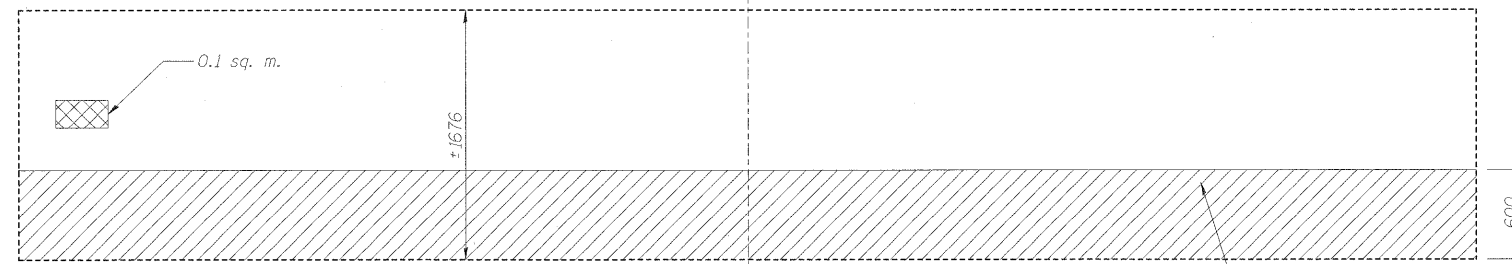
SCALE NONE DRAWN BY MLB
 DATE DECEMBER 10, 2006 CHECKED BY MTH

PLOT DATE = 12/22/2008
 FILE NAME = H:\4428\Cont-est 94533\SH1_095_S-02.dgn

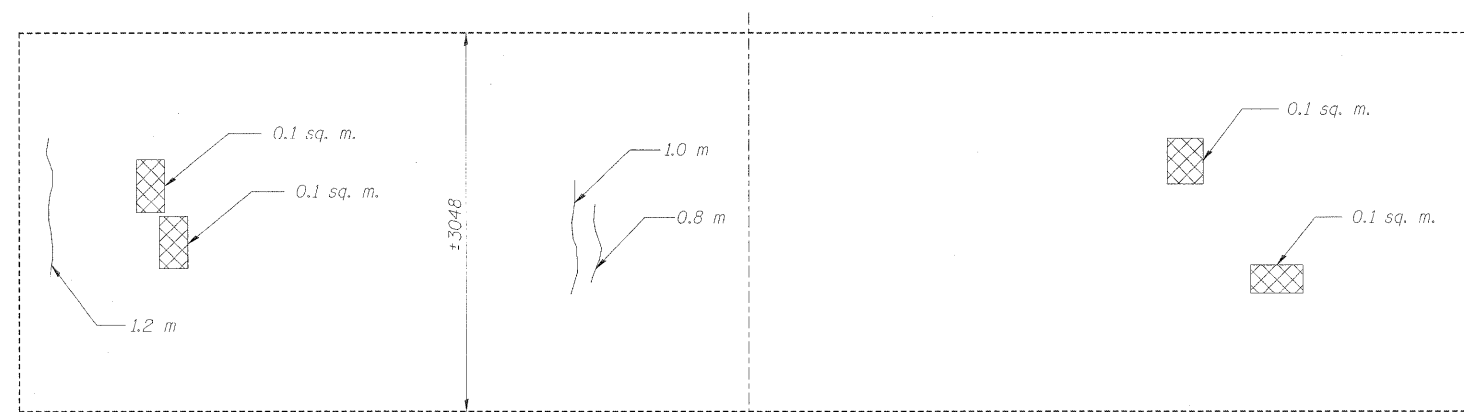
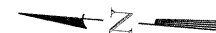
| | | | | |
|---------------------|----------|------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 781 | 1YRS | CRAWFORD | 212 | 96 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



WEST WALL
(Looking West)



EAST WALL
(Looking East)



TOP SLAB

Note:

The quantities shown are for estimating purposes only. The actual area to be repaired will be determined by the Engineer at the time of construction. Actual repair locations shall be shown on the "As Built" plans.

LEGEND

- Polymer Modified Portland Cement Mortar
- Structural Repair of Concrete (Depth Equal to or Less than 125mm)
- Epoxy Crack Injection
- m* - Meter
- sq. m.* - Square Meter

BILL OF MATERIAL - BOX CULVERT NO. 8

| Item | Unit | Total |
|--|--------|-------|
| MX033694 Structural Repair of Concrete (Depth Equal to or Less than 125mm) | sq. m. | 0.6 |
| M5900200 Epoxy Crack Injection | m | 3.6 |
| MX032196 Polymer Modified Portland Cement Mortar | sq. m. | 14.2 |

Quantity increased by 20% to account for deterioration since field inspection.

PLOT DATE = 12/22/2008
FILE NAME = H:\4420\Contract 94533\SH1_896_5_93.dgn

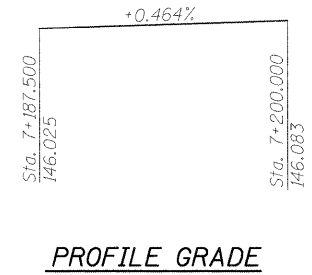
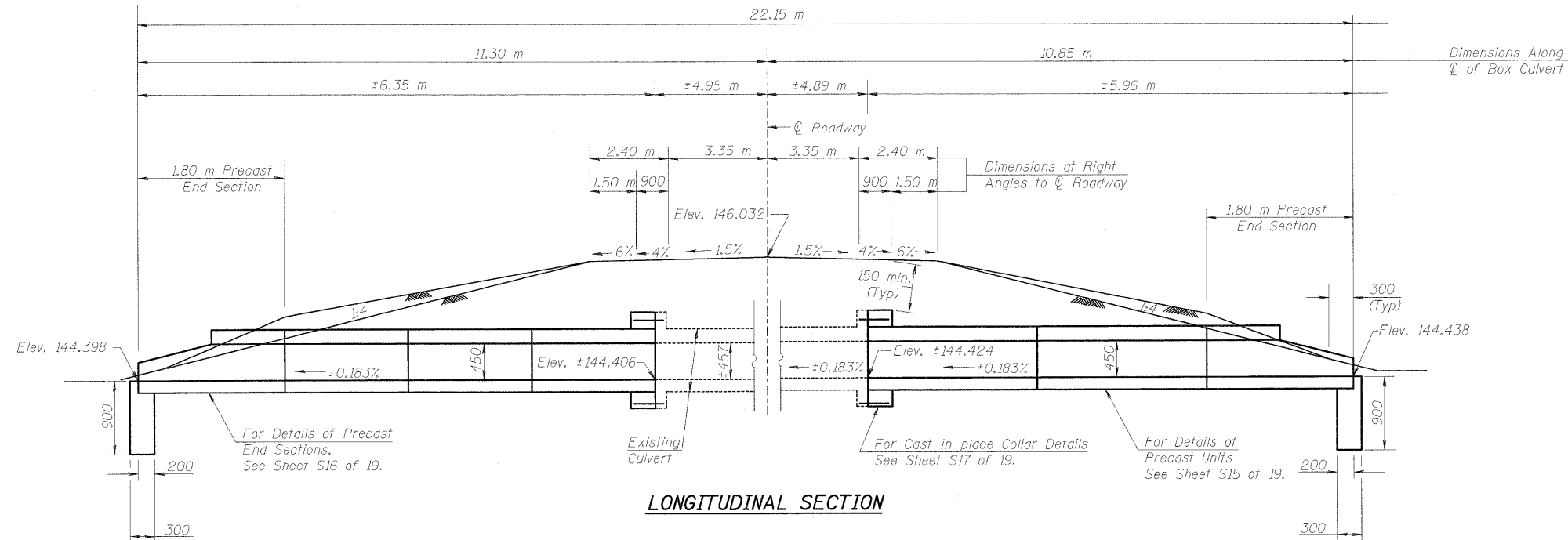
| REVISIONS | |
|-----------|----------|
| NAME | DATE |
| LWJ | 12-18-08 |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
**BOX CULVERT EXTENSIONS
CONCRETE REPAIR DETAILS
BOX CULV. NO. 8, STA. 5+750.192**

SCALE NONE DRAWN BY MLB
DATE DECEMBER 10, 2006 CHECKED BY MTH

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 97 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

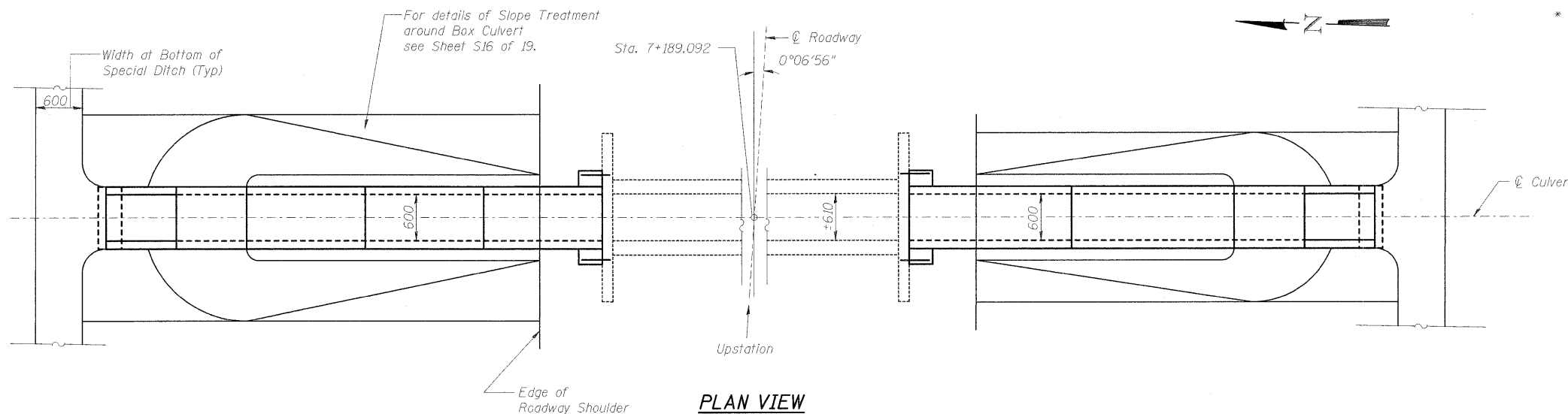
Existing Structure: The structure is a single barrel 610 mm x 457 mm box culvert at a skew of 0°6'56" right forward. The culvert has an overall length of 9.84 m. The contractor shall extend the culvert on each end for the dimensions shown.



BILL OF MATERIAL - BOX CULVERT NO. 10

| Item | Unit | Total |
|---|--------|-----------|
| M5030350 Concrete Structures | cu. m. | 0.4 |
| M5080205 Reinforcement Bars, Epoxy Coated | kg | 90 |
| M5401002 Precast Concrete Box Culvert (600x450) | m | 8.7 |
| * 5401010 Box Culvert End Section, Culvert No. 10 | Each | 2 |
| M5403220 Expansion Bolts (M20x250) | Each | 20 |
| M2810707 Stone Riprap, Class A4 | sq. m. | See Plans |

* Non-standard size.



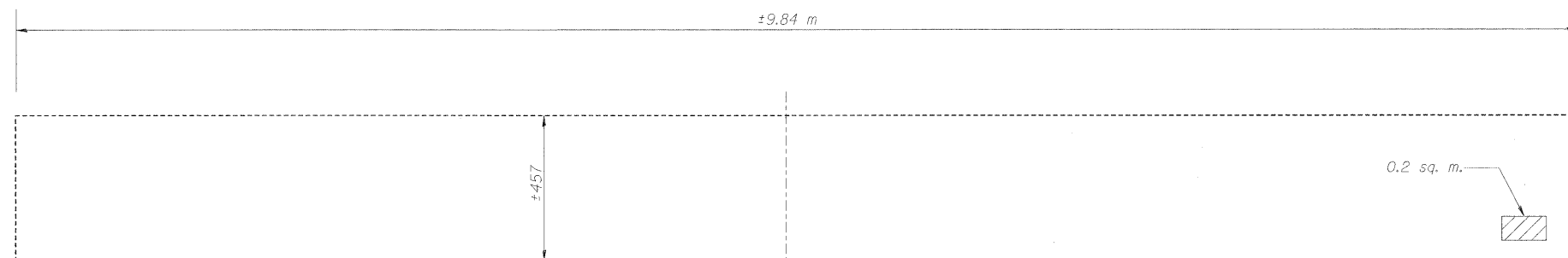
PLOT DATE = 12/22/2008
FILE NAME = H:\442\Contract\94533\SHT_097_5-94.dgn

| REVISIONS | |
|-----------|----------|
| NAME | DATE |
| LWJ | 12-18-08 |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
BOX CULVERT EXTENSIONS
GENERAL PLAN & ELEVATION
BOX CULV. NO. 10, STA. 7+189.092

SCALE NONE DRAWN BY MLB
 DATE DECEMBER 10, 2006 CHECKED BY MTH

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 98 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



EAST WALL
(Looking East)

Note:

The quantities shown are for estimating purposes only. The actual area to be repaired will be determined by the Engineer at the time of construction. Actual repair locations shall be shown on the "As Built" plans.

LEGEND

 Polymer Modified Portland Cement Mortar

sq. m. - Square Meter

BILL OF MATERIAL - BOX CULVERT NO. 10

| Item | Unit | Total |
|--|--------|-------|
| MX032196 Polymer Modified Portland Cement Mortar | sq. m. | 0.3 |

Quantity increased by 20% to account for deterioration since field inspection.

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION BOX CULVERT EXTENSIONS CONCRETE REPAIR DETAILS BOX CULV. NO. 10, STA. 7+189.092 |
|-----------|----------|--|
| NAME | DATE | |
| LWJ | 12-18-08 | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |

SCALE NONE DRAWN BY MLB
DATE DECEMBER 10, 2006 CHECKED BY MTH

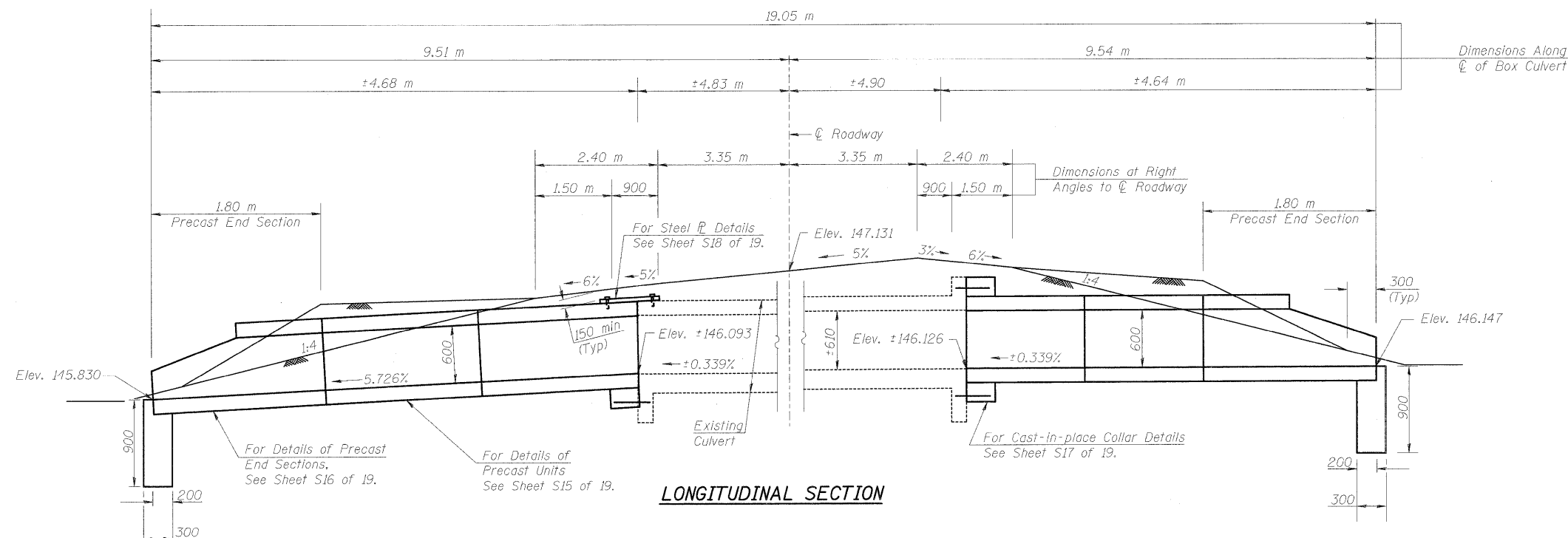
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 99 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

Existing Structure: The structure is a single barrel 914 mm x 610 mm box culvert at a skew of 0°07'08" right forward. The culvert has an overall length of 9.73 m. The contractor shall extend the culvert on each end for the dimensions shown.

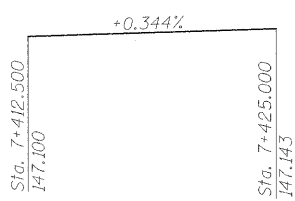
CURVE P11 DATA

PI STA. 7+419.751
 $\Delta = 9^\circ 10' 39''$ (LT)
 T = 46.658 m
 R = 581.332 m
 L = 93.117 m
 E = 1.869 m
 S.E. = 5.0%
 PC STA 7+373.093
 PT STA 7+466.210

Attain S.E. Sta. 7+338.093 to 7+385.093
 and Sta. 7+454.210 to 7+501.210



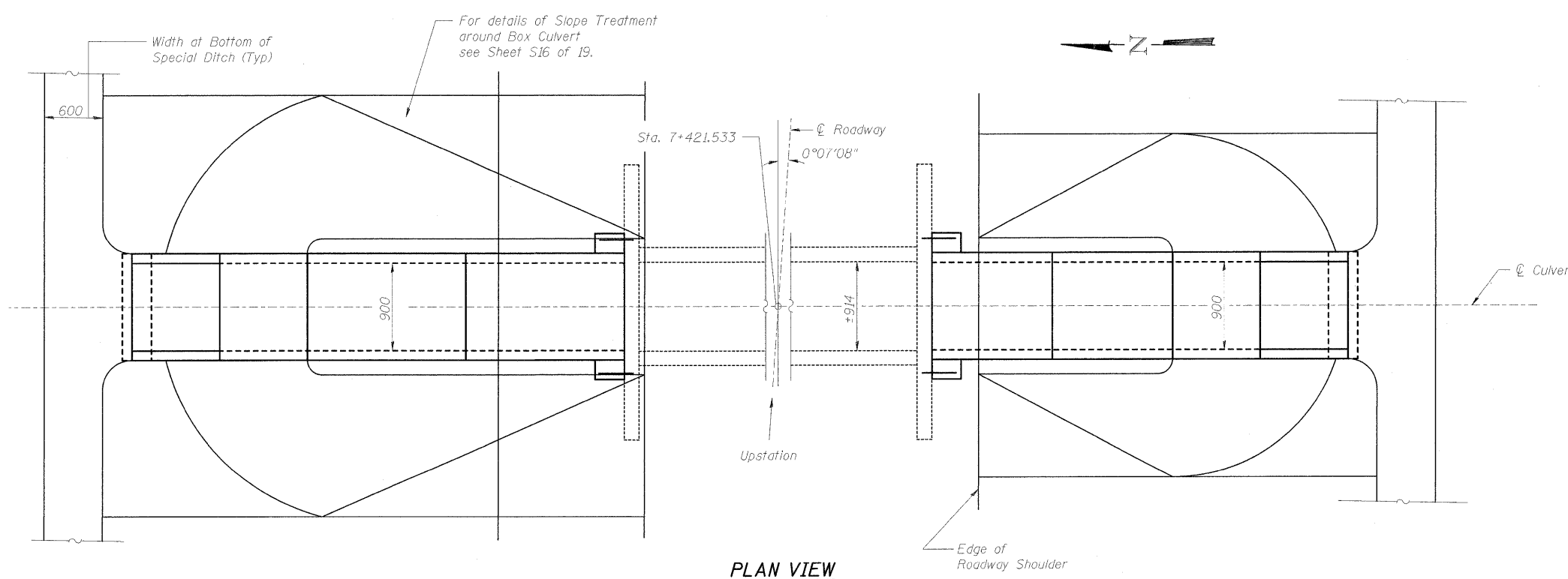
LONGITUDINAL SECTION



PROFILE GRADE

BILL OF MATERIAL - BOX CULVERT NO. 11

| Item | Unit | Total |
|---|--------|-----------|
| M5030350 Concrete Structures | cu. m. | 0.5 |
| M5080205 Reinforcement Bars, Epoxy Coated | kg | 90 |
| M5401005 Precast Concrete Box Culvert (900x600) | m | 5.7 |
| 5401011 Box Culvert End Section, Culvert No. 11 | Each | 2 |
| M5403220 Expansion Bolts (M20x250) | Each | 21 |
| M2810707 Stone Riprap, Class A4 | sq. m. | See Plans |
| M5050405 Furnishing & Erecting Structural Steel | kg | 130 |



PLAN VIEW

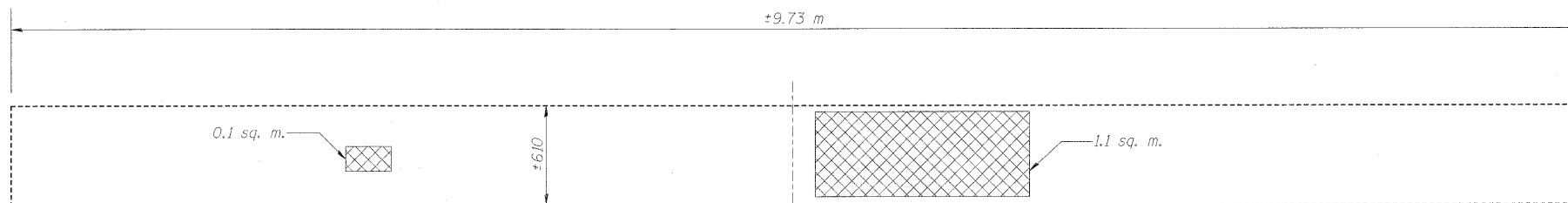
| REVISIONS | |
|-----------|----------|
| NAME | DATE |
| LWJ | 12-18-08 |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
BOX CULVERT EXTENSIONS
GENERAL PLAN & ELEVATION
BOX CULV. NO. 11, STA. 7+421.533

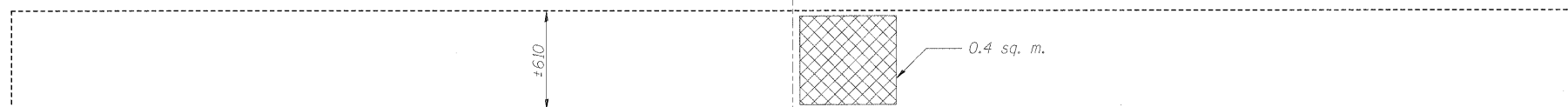
SCALE NONE DRAWN BY MLB
 DATE DECEMBER 10, 2006 CHECKED BY MTH

PLOT DATE = 12/22/2008
 FILE NAME = P:\V\20\Contract\94533\SHT_099_S-06.dgn

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------|----------|------------------|-----------|
| 781 | 1YRS | CRAWFORD | 212 | 100 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



WEST WALL
(Looking West)



EAST WALL
(Looking East)

Note:
The quantities shown are for estimating purposes only. The actual area to be repaired will be determined by the Engineer at the time of construction. Actual repair locations shall be shown on the "As Built" plans.

LEGEND

- Structural Repair of Concrete (Depth Equal to or Less than 125mm)
- sq. m. - Square Meter

BILL OF MATERIAL - BOX CULVERT NO. 11

| Item | Unit | Total |
|--|--------|-------|
| MX033694 Structural Repair of Concrete (Depth Equal to or Less than 125mm) | sq. m. | 1.9 |

Quantity increased by 20% to account for deterioration since field inspection.

PLOT DATE = 12/22/2008
FILE NAME = H:\1420\Contract 94533\ASHT_100_5-07.dgn

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION BOX CULVERT EXTENSIONS CONCRETE REPAIR DETAILS BOX CULV. NO. 11, STA. 7+421.533 |
|-----------|----------|--|
| NAME | DATE | |
| LWJ | 12-18-08 | SCALE NONE DATE DECEMBER 10, 2006 DRAWN BY MLB CHECKED BY MTH |
| | | |
| | | |
| | | |
| | | |

LIN NO. 9803