






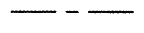
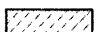
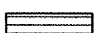

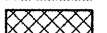
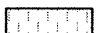
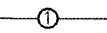

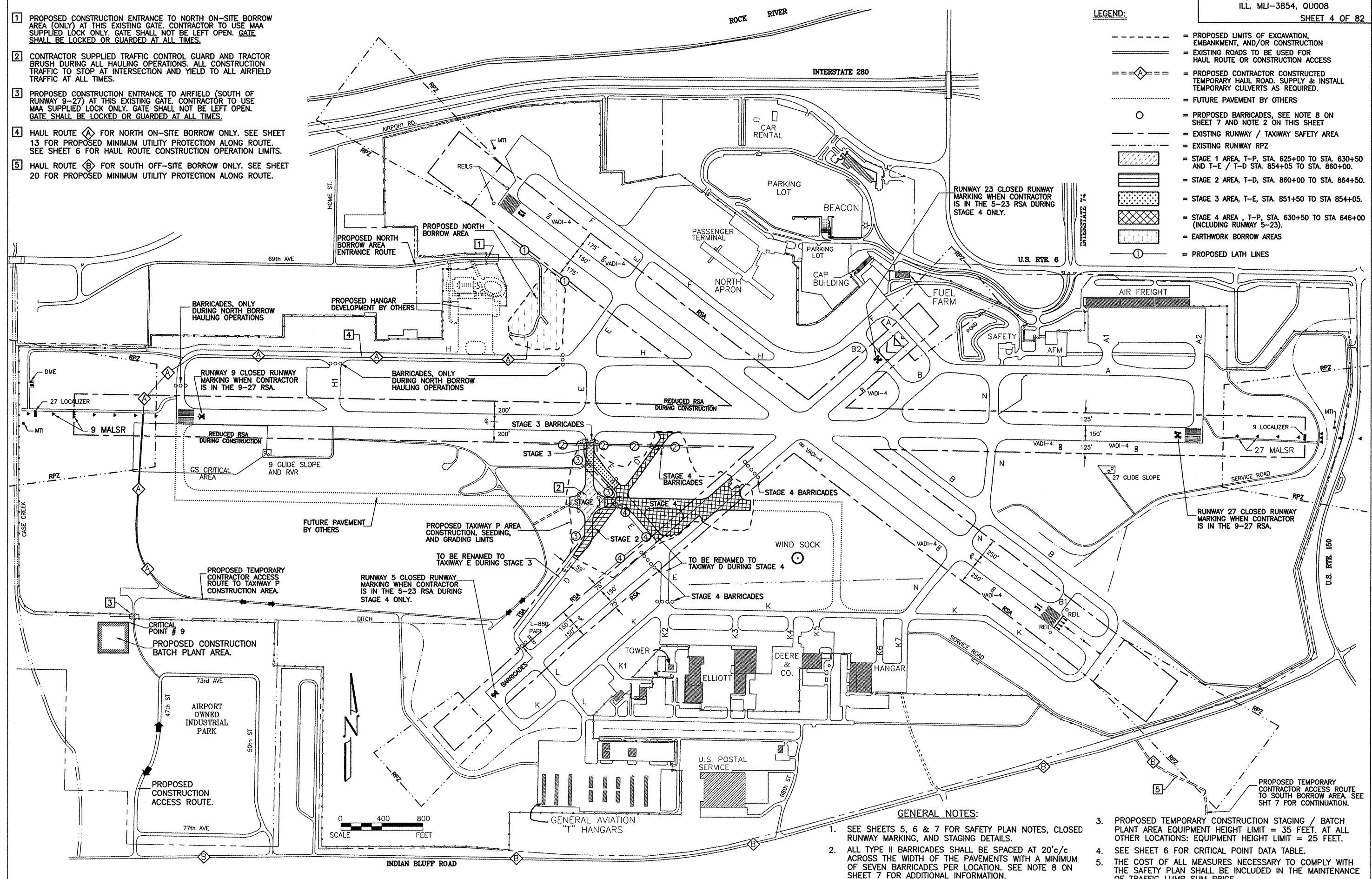


**NUMBERED LEGEND:**

- 1 PROPOSED CONSTRUCTION ENTRANCE TO NORTH ON-SITE BORROW AREA (ONLY) AT THIS EXISTING GATE. CONTRACTOR TO USE MAA SUPPLIED LOCK ONLY. GATE SHALL NOT BE LEFT OPEN. GATE SHALL BE LOCKED OR GUARDED AT ALL TIMES.
- 2 CONTRACTOR SUPPLIED TRAFFIC CONTROL GUARD AND TRACTOR BRUSH DURING ALL HAULING OPERATIONS. ALL CONSTRUCTION TRAFFIC TO STOP AT INTERSECTION AND YIELD TO ALL AIRFIELD TRAFFIC AT ALL TIMES.
- 3 PROPOSED CONSTRUCTION ENTRANCE TO AIRFIELD (SOUTH OF RUNWAY 9-27) AT THIS EXISTING GATE. CONTRACTOR TO USE MAA SUPPLIED LOCK ONLY. GATE SHALL NOT BE LEFT OPEN. GATE SHALL BE LOCKED OR GUARDED AT ALL TIMES.
- 4 HAUL ROUTE  FOR NORTH ON-SITE BORROW ONLY. SEE SHEET 13 FOR PROPOSED MINIMUM UTILITY PROTECTION ALONG ROUTE. SEE SHEET 6 FOR HAUL ROUTE CONSTRUCTION OPERATION LIMITS.
- 5 HAUL ROUTE  FOR SOUTH OFF-SITE BORROW ONLY. SEE SHEET 20 FOR PROPOSED MINIMUM UTILITY PROTECTION ALONG ROUTE.

**LEGEND:**

-  = PROPOSED LIMITS OF EXCAVATION, EMBANKMENT, AND/OR CONSTRUCTION
-  = EXISTING ROADS TO BE USED FOR HAUL ROUTE OR CONSTRUCTION ACCESS
-  = PROPOSED CONTRACTOR CONSTRUCTED TEMPORARY HAUL ROAD. SUPPLY & INSTALL TEMPORARY CULVERTS AS REQUIRED.
-  = FUTURE PAVEMENT BY OTHERS
-  = PROPOSED BARRICADES, SEE NOTE 8 ON SHEET 7 AND NOTE 2 ON THIS SHEET
-  = EXISTING RUNWAY / TAXIWAY SAFETY AREA
-  = EXISTING RUNWAY RPZ
-  = STAGE 1 AREA, T-P, STA. 625+00 TO STA. 630+50 AND T-E / T-D STA. 854+05 TO STA. 860+00.
-  = STAGE 2 AREA, T-D, STA. 860+00 TO STA. 864+50.
-  = STAGE 3 AREA, T-E, STA. 851+50 TO STA. 854+05.
-  = STAGE 4 AREA, T-P, STA. 630+50 TO STA. 646+00 (INCLUDING RUNWAY 5-23).
-  = EARTHWORK BORROW AREAS
-  = PROPOSED LATH LINES



**GENERAL NOTES:**

1. SEE SHEETS 5, 6 & 7 FOR SAFETY PLAN NOTES, CLOSED RUNWAY MARKING, AND STAGING DETAILS.
2. ALL TYPE II BARRICADES SHALL BE SPACED AT 20'c/c ACROSS THE WIDTH OF THE PAVEMENTS WITH A MINIMUM OF SEVEN BARRICADES PER LOCATION. SEE NOTE 8 ON SHEET 7 FOR ADDITIONAL INFORMATION.
3. PROPOSED TEMPORARY CONSTRUCTION STAGING / BATCH PLANT AREA EQUIPMENT HEIGHT LIMIT = 35 FEET. AT ALL OTHER LOCATIONS: EQUIPMENT HEIGHT LIMIT = 25 FEET.
4. SEE SHEET 6 FOR CRITICAL POINT DATA TABLE.
5. THE COST OF ALL MEASURES NECESSARY TO COMPLY WITH THE SAFETY PLAN SHALL BE INCLUDED IN THE MAINTENANCE OF TRAFFIC LUMP SUM PRICE.

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