

82+1=83

ITEM 15A

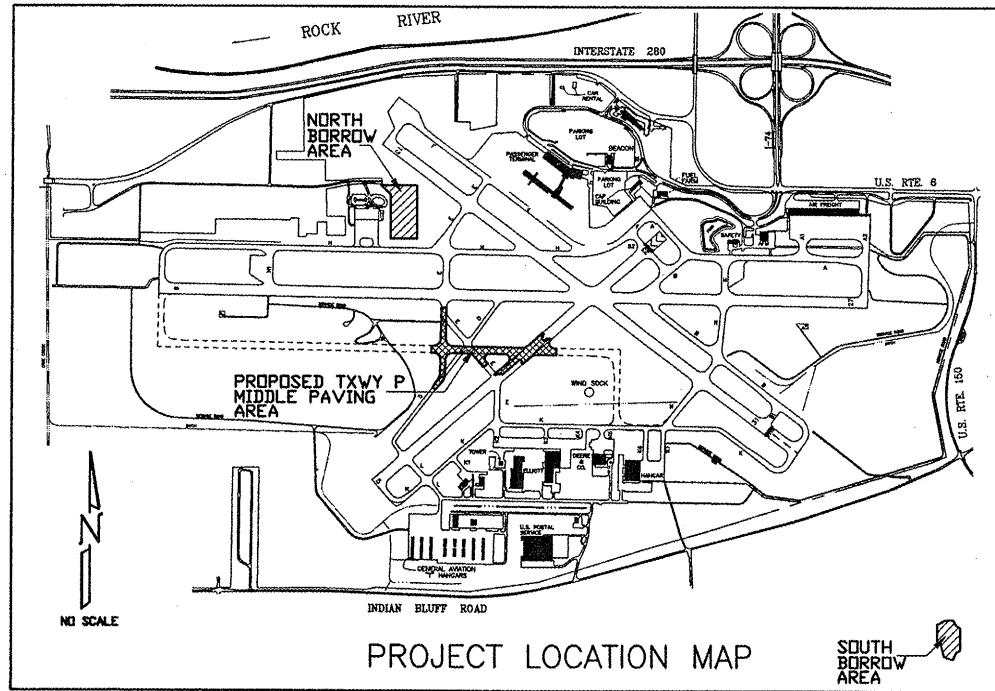
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF AERONAUTICS

CONSTRUCTION PLANS

FOR

QUAD-CITY
INTERNATIONAL
AIRPORT

ROCK ISLAND COUNTY, ILLINOIS



INDEX OF SHEETS	
SHEET NO.	TITLE
1	COVER SHEET AND INDEX OF SHEETS
2-3	SUMMARY OF QUANTITIES / GENERAL NOTES
4-7	PROPOSED SAFETY PLAN
8-12	TYPICAL SECTIONS
13	PROJECT AREA PLAN
14	TAXIWAY P PLAN & PROFILE, STA 625+00 TO STA 635+80
15	TAXIWAY P PLAN & PROFILE, STA 635+80 TO STA 646+80
16	RELOCATED TAXIWAY E PLAN & PROFILE, STA 848+80 TO STA 857+80
17	RELOCATED TAXIWAY D PLAN & PROFILE, STA 857+80 TO STA 865+20
18	EXISTING TAXIWAY E PLAN & PROFILE, STA 880+00 TO STA 885+60
19	PROPOSED NORTH ON-SITE BORROW AREA PLAN
20	PROPOSED SOUTH OFFSITE BORROW EXCAVATION AREA PLAN
21-22	TAXIWAY P JOINT PLAN
23	JOINT DETAILS
24-27	TAXIWAY P STAKING PLAN
28-31	DRAINAGE PLAN
32-36	DRAINAGE - PIPE PROFILES
37-39	DRAINAGE DETAILS
40-41	PROPOSED PAVEMENT MARKINGS
42-45	LIGHTING PLAN
45A	TEMPORARY GUIDANCE SIGN PLAN FOR PROJECT STAGING
46-48	PERMANENT GUIDANCE SIGN PLAN
49-52	LIGHTING DETAILS
53	MISCELLANEOUS DETAILS
54-55	PROPOSED STORM WATER POLLUTION PREVENTION PLAN
56-62	PROPOSED TAXIWAY P (WITH ADDITIVE ALTERNATES) CROSS SECTIONS
63-67	PROPOSED RELOCATED TAXIWAY D / E CROSS SECTIONS
68-70	PROPOSED RUNWAY 5-23 BASE BID DRAINAGE SWALE CROSS SECTIONS
71-75	PROPOSED RUNWAY 5-23 BASE BID SHOULDER EMBANKMENT CROSS SECTIONS
76-78	PROPOSED NORTH ON-SITE BORROW AREA CROSS SECTIONS
79-82	PROPOSED SOUTH OFFSITE BORROW EXCAVATION AREA CROSS SECTIONS

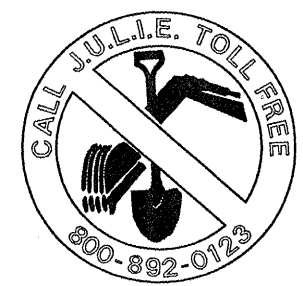
TAXIWAY P, PHASE II - MIDDLE PAVING

TAXIWAY P (1,515' X 100') PAVEMENT AND RUNWAY 5-23 (900' X 150') PAVEMENT REMOVAL / REPLACEMENT INCLUDING EARTHWORK, TAXIWAY D & E TIE-INS, LIGHTING, UNDERDRAINS, MARKING, GROOVING, UTILITY ADJUSTMENTS AND TURFING.

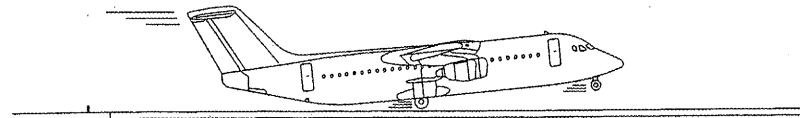
ILLINOIS PROJECT MLI-3854
A.I.P. PROJECT NO. 3-17-0068-XX
AIRPORT CLASSIFICATION - AIR CARRIER

AIRCRAFT APPROACH CATEGORY - C
AIRPLANE DESIGN GROUP - III
LATITUDE 41°26'52.4", LONGITUDE 90°30'33.9", ELEVATION 589'M.S.L.

WARNING



CALL BEFORE YOU DIG



<p>CONSULTING ENGINEERS MISSMAN, STANLEY & ASSOC.</p> <p>ENGINEER'S SIGNATURE <i>Dennis R. Martin</i></p> <p>SEALED & SIGNED 3/31/09</p> <p>DATE OF LICENSE EXPIRATION 11/30/09</p>	<p>METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND COUNTY, ILLINOIS</p> <p><i>Bruce Carter</i></p> <p>4/1/09 DATE APPROVED</p>
<p>MARCH 20, 2009 OFFICIAL DATE OF PLANS</p>	<p>PREPARED BY MISSMAN, STANLEY & ASSOCIATES Consulting Civil Engineers ROCK ISLAND, ILLINOIS</p>

SUMMARY OF QUANTITIES

ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITIES	
			AS AWARDED	AS BUILT
BASE BID:				
AR108158	1/C #8 5 KV UG CABLE IN UD	L.F.	8,540	
AR108258	2/C #8 5 KV UG CABLE IN UD	L.F.	3,580	
AR108962	REPLACE CABLE	L.F.	950	
AR110502	2-WAY CONCRETE ENCASED DUCT	L.F.	670	
AR110901	CONCRETE DUCT REMOVAL	L.F.	260	
AR125443	TAXI GUIDANCE SIGN, 3 CHARACTER	EACH	2	
AR125444	TAXI GUIDANCE SIGN, 4 CHARACTER	EACH	2	
AR125446	TAXI GUIDANCE SIGN, 6 CHARACTER	EACH	2	
AR125510	MIRL, BASE MOUNTED	EACH	2	
AR125520	HIRLQ, BASED MOUNTED	EACH	1	
AR125565	SPLICE CAN	EACH	2	
AR125902	REMOVE BASE MOUNTED LIGHT	EACH	7	
AR125903	REMOVE INPAVEMENT LIGHT	EACH	1	
AR125904	REMOVE TAXI GUIDANCE SIGN	EACH	8	
AR125942	ADJUST BASE MOUNTED LIGHT	EACH	7	
AR125944	ADJUST TAXI GUIDANCE SIGN	EACH	2	
AR125962	RELOCATE BASE MOUNTED LIGHT	EACH	68	
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1	
AR150530	TRAFFIC MAINTENANCE	L.S.	1	
AR151450	CLEARING AND GRUBBING	ACRE	2.5	
AR152410	UNCLASSIFIED EXCAVATION	C.Y.	15,825	
AR152441	ON-SITE BORROW	C.Y.	34,800	
AR152442	OFFSITE BORROW EXCAVATION	C.Y.	46,600	
AR156500	TEMPORARY EROSION CONTROL	L.S.	1	
AR156540	RIPRAP	S.Y.	410	
AR209510	CRUSHED AGGREGATE BASE COURSE	TON	16,795	
AR209600	GEOTEXTILE FABRIC	S.Y.	20,641	
AR401610	BITUMINOUS SURFACE COURSE	TON	6,140	
AR401630	BITUMINOUS SURFACE TEST SECTION	EACH	1	
AR401650	BITUMINOUS PAVEMENT MILLING	S.Y.	1,175	
AR401900	REMOVE BITUMINOUS PAVEMENT	S.Y.	14,900	
AR501512	12" PCC PAVEMENT	S.Y.	20,124	
AR501530	PCC TEST BATCH	EACH	1	
AR501540	PCC PAVEMENT GROOVING	S.Y.	9,345	
AR501900	REMOVE PCC PAVEMENT	S.Y.	10,800	
AR602510	BITUMINOUS PRIME COAT	GAL.	3,735	
AR603510	BITUMINOUS TACK COAT	GAL.	123	

BASE BID NOTE:

THE BASE BID INCLUDES:

TAXIWAY P : STA. 627+15 TO STA. 635+75.
 RUNWAY 5-23 : STA. 33+25 TO STA. 42+25.
 RELOCATED TAXIWAY E : STA. 851+85 TO STA. 857+20.
 RELOCATED TAXIWAY D : STA. 858+20 TO STA. 863+65.
 EXISTING TAXIWAY E : STA. 880+00 TO STA. 885+00.

SUMMARY OF QUANTITIES, CONTINUED

ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITIES	
			AS AWARDED	AS BUILT
BASE BID (CONTINUED):				
AR620510	PAVEMENT MARKING	S.F.	9,700	
AR701442	42" RCP, CLASS III	L.F.	535	
AR701512	12" RCP, CLASS IV	L.F.	126	
AR701524	24" RCP, CLASS IV	L.F.	895	
AR701713	RCEP SPAN 30 RISE 19	L.F.	302	
AR701900	REMOVE PIPE	L.F.	160	
AR705506	6" PERFORATED UNDERDRAIN	L.F.	3,108	
AR751410	INLET	EACH	1	
AR751415	INLET-SPECIAL	EACH	4	
AR751540	MANHOLE 4'	EACH	2	
AR751550	MANHOLE 5'	EACH	5	
AR751560	MANHOLE 6'	EACH	1	
AR751570	MANHOLE - SPECIAL	EACH	4	
AR751903	REMOVE MANHOLE	EACH	2	
AR751906	REMOVE CATCH BASIN	EACH	1	
AR751945	ADJUST MANHOLE - NON PAVEMENT	EACH	2	
AR751963	RELOCATE MANHOLE	EACH	1	
AR752424	PRECAST REINFORCED CONC. FES 24"	EACH	1	
AR752442	PRECAST REINFORCED CONC. FES 42"	EACH	1	
AR752724	P R CONC. FES EQ. ROUND SIZE 24"	EACH	1	
AR752900	REMOVE END SECTION	EACH	1	
AR801605	REPLACE TAXI GUIDANCE SIGN PANEL	EACH	60	
AR801628	RELOCATE TAXI GUIDANCE SIGN PANEL	EACH	8	
AR901510	SEEDING	ACRE	35	
AR908513	MULCHING - METHOD 3	ACRE	35	
AR908520	EXCELSIOR BLANKET	S.Y.	1,500	

SUMMARY OF QUANTITIES, CONTINUED

ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITIES	
			AS AWARDED	AS BUILT
ADDITIVE ALTERNATE 1 (TAXIWAY P, STA 635+75 TO STA 640+21.24):				
AS108158	1/C #8 5 KV UG CABLE IN UD	L.F.	725	
AS108258	2/C #8 5 KV UG CABLE IN UD	L.F.	595	
AS110502	2-WAY CONCRETE ENCASED DUCT	L.F.	115	
AS125448	TAXI GUIDANCE SIGN, 8 CHARACTER	EACH	1	
AS125510	MIRL, BASE MOUNTED	EACH	6	
AS152410	UNCLASSIFIED EXCAVATION	C.Y.	175	
AS209510	CRUSHED AGGREGATE BASE COURSE	TON	3,410	
AS209600	GEOTEXTILE FABRIC	S.Y.	5,847	
AS501512	12" PCC PAVEMENT	S.Y.	5,748	
AS620510	PAVEMENT MARKING	S.F.	900	
AS705506	6" PERFORATED UNDERDRAIN	L.F.	1,175	
AS751570	MANHOLE - SPECIAL	EACH	2	
ADDITIVE ALTERNATE 2 (TAXIWAY P, STA 642+44 TO STA 645+30):				
AT108158	1/C #8 5 KV UG CABLE IN UD	L.F.	575	
AT108258	2/C #8 5 KV UG CABLE IN UD	L.F.	475	
AT110501	1-WAY CONC. ENCASED DUCT	L.F.	425	
AT110502	2-WAY CONCRETE ENCASED DUCT	L.F.	115	
AT110710	ELECTRICAL MANHOLE	EACH	2	
AT110907	REMOVE ELECTRICAL MANHOLE	EACH	1	
AT125510	MIRL, BASE MOUNTED	EACH	4	
AT152410	UNCLASSIFIED EXCAVATION	C.Y.	1,050	
AT152441	ON-SITE BORROW	C.Y.	1,200	
AT152442	OFFSITE BORROW EXCAVATION	C.Y.	2,750	
AT209510	CRUSHED AGGREGATE BASE COURSE	TON	2,195	
AT209600	GEOTEXTILE FABRIC	S.Y.	3,762	
AT501512	12" PCC PAVEMENT	S.Y.	3,678	
AT501540	PCC PAVEMENT GROOVING	S.Y.	13,155	
AT705506	6" PERFORATED UNDERDRAIN	L.F.	469	
AT751570	MANHOLE - SPECIAL	EACH	2	
AT801614	SUPPLY TAXI GUIDANCE SIGN PANEL	EACH	20	
AT901510	SEEDING	ACRE	2	
AT908513	MULCHING - METHOD 3	ACRE	2	

GENERAL NOTES:

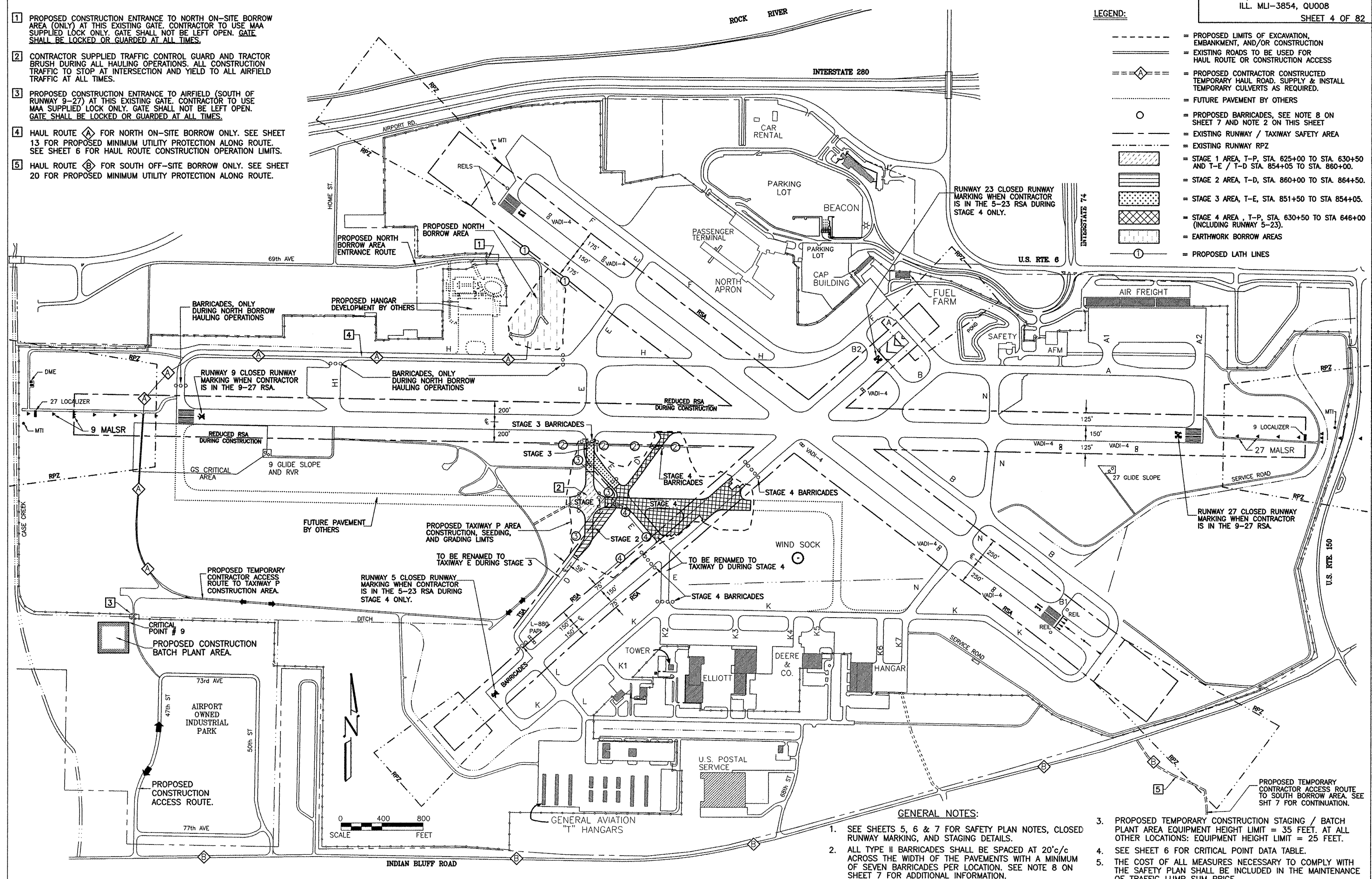
1. MAXIMUM PAY WIDTH FOR 209510 CRUSHED AGGREGATE BASE COURSE SHALL BE 12 INCHES BEYOND THE EDGE OF PAVEMENT. IF THE CONTRACTOR REQUIRES ADDITIONAL WIDTH FOR PAVEMENT INSTALLATION, THE ADDITIONAL MATERIALS SHALL MEET THE SAME SPECIFICATIONS, BUT WILL BE CONSIDERED INCIDENTAL.
2. THE CONTRACTOR SHALL SALVAGE EXISTING AIRFIELD LIGHTING EQUIPMENT AS DETAILED IN THE CONSTRUCTION PLANS AND SPECIAL PROVISIONS PRIOR TO THE START OF EARTHWORK AND/OR PAVING ACTIVITIES. SALVAGED EQUIPMENT SHALL BE CLEANED AND REUSED OR DELIVERED TO THE METROPOLITAN AIRPORT AUTHORITY.
3. CROSS SECTION SLOPES, CENTERLINE PROFILE GRADES, AND ALL SPOT GRADES SHALL BE SUBJECT TO CHANGE, AS APPROVED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION.
4. THE CONTRACTOR SHALL EXCAVATE TEMPORARY EROSION CONTROL DRAINAGE SWALES, AS REQUIRED BY THE RESIDENT ENGINEER, TO CONTROL STORM WATER RUN-OFF.
5. THE CONTRACT 152410-UNCLASSIFIED EXCAVATION / 152441-ON-SITE BORROW / 152442-OFFSITE BORROW EXCAVATION ITEMS SHALL INCLUDE ALL COSTS ASSOCIATED WITH EXCAVATION OF SOILS, HAULING OF SOILS, STOCKPILING SOILS, INSTALLATION OF SOILS, COMPACTING OF SOILS, GRADING OF SOILS, INSTALLATION AND REMOVAL OF HAUL ROADS OR ROUTES, RESTORATION OF HAUL ROADS OR ROUTES, DISPOSAL OF WASTE SOILS, CLEANING OF PAVEMENTS, AND ALL OTHER ITEMS THAT ARE REQUIRED TO COMPLETE THE EARTHWORK. THESE ITEMS SHALL BE PAID FOR BASED UPON THE CUBIC YARDS OF MATERIALS REMOVED AS ACCEPTED BY THE RESIDENT ENGINEER. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
6. AFTER CONSTRUCTION HAS BEEN COMPLETED, THE CONTRACTOR SHALL SEED AND HYDRAULIC MULCH ALL DISTURBED AREAS PER SPECIAL PROVISIONS 901 AND 908. ONLY SEED AND MULCHING AREAS WITHIN THE LIMITS OF CONSTRUCTION/SEEDING WILL BE ELIGIBLE FOR PAYMENT UNDER THESE CONTRACT PAY ITEMS. AREAS OUTSIDE OF THE LIMITS OF CONSTRUCTION/SEEDING SHALL BE SEEDED AND MULCHED BY THE CONTRACTOR PER SPECIAL PROVISION 901/908, BUT SHALL NOT BE MEASURED FOR PAYMENT.
7. ITEM 908513 MULCHING - METHOD 3 SHALL BE ACCOMPLISHED FOLLOWING THE METHODS AND PROCEDURES OUTLINED IN THE IDOT-DOA SUPPLEMENTAL SPECIFICATIONS FOR HYDRAULIC MULCHING AND IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION SECTION 251, METHOD 3 (HYDRAULIC MULCH).
8. ITEM 908520 EXCELSIOR BLANKET SHALL BE ACCOMPLISHED FOLLOWING THE METHODS AND PROCEDURES OUTLINED IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION SECTION 251, EROSION CONTROL BLANKET (EXCELSIOR BLANKET). THE LOCATION OF THE PROPOSED EXCELSIOR BLANKET SHALL BE DETERMINED BY THE RESIDENT ENGINEER, IN THE FIELD, AT THE TIME OF CONSTRUCTION. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

NUMBERED LEGEND:

- 1 PROPOSED CONSTRUCTION ENTRANCE TO NORTH ON-SITE BORROW AREA (ONLY) AT THIS EXISTING GATE. CONTRACTOR TO USE MAA SUPPLIED LOCK ONLY. GATE SHALL NOT BE LEFT OPEN. GATE SHALL BE LOCKED OR GUARDED AT ALL TIMES.
- 2 CONTRACTOR SUPPLIED TRAFFIC CONTROL GUARD AND TRACTOR BRUSH DURING ALL HAULING OPERATIONS. ALL CONSTRUCTION TRAFFIC TO STOP AT INTERSECTION AND YIELD TO ALL AIRFIELD TRAFFIC AT ALL TIMES.
- 3 PROPOSED CONSTRUCTION ENTRANCE TO AIRFIELD (SOUTH OF RUNWAY 9-27) AT THIS EXISTING GATE. CONTRACTOR TO USE MAA SUPPLIED LOCK ONLY. GATE SHALL NOT BE LEFT OPEN. GATE SHALL BE LOCKED OR GUARDED AT ALL TIMES.
- 4 HAUL ROUTE (A) FOR NORTH ON-SITE BORROW ONLY. SEE SHEET 13 FOR PROPOSED MINIMUM UTILITY PROTECTION ALONG ROUTE. SEE SHEET 6 FOR HAUL ROUTE CONSTRUCTION OPERATION LIMITS.
- 5 HAUL ROUTE (B) FOR SOUTH OFF-SITE BORROW ONLY. SEE SHEET 20 FOR PROPOSED MINIMUM UTILITY PROTECTION ALONG ROUTE.

LEGEND:

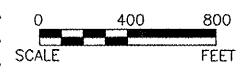
- - - - - = PROPOSED LIMITS OF EXCAVATION, EMBANKMENT, AND/OR CONSTRUCTION
- — — — — = EXISTING ROADS TO BE USED FOR HAUL ROUTE OR CONSTRUCTION ACCESS
- ◆ — — — — = PROPOSED CONTRACTOR CONSTRUCTED TEMPORARY HAUL ROAD. SUPPLY & INSTALL TEMPORARY CULVERTS AS REQUIRED.
- = FUTURE PAVEMENT BY OTHERS
- = PROPOSED BARRICADES, SEE NOTE 8 ON SHEET 7 AND NOTE 2 ON THIS SHEET
- — — — — = EXISTING RUNWAY / TAXIWAY SAFETY AREA
- — — — — = EXISTING RUNWAY RPZ
- [Diagonal Hatching] = STAGE 1 AREA, T-P, STA. 625+00 TO STA. 630+50 AND T-E / T-D STA. 854+05 TO STA. 860+00.
- [Horizontal Hatching] = STAGE 2 AREA, T-D, STA. 860+00 TO STA. 864+50.
- [Vertical Hatching] = STAGE 3 AREA, T-E, STA. 851+50 TO STA. 854+05.
- [Cross-hatching] = STAGE 4 AREA, T-P, STA. 630+50 TO STA. 646+00 (INCLUDING RUNWAY 5-23).
- [Stippled] = EARTHWORK BORROW AREAS
- ① = PROPOSED LATH LINES



GENERAL NOTES:

1. SEE SHEETS 5, 6 & 7 FOR SAFETY PLAN NOTES, CLOSED RUNWAY MARKING, AND STAGING DETAILS.
2. ALL TYPE II BARRICADES SHALL BE SPACED AT 20'c/c ACROSS THE WIDTH OF THE PAVEMENTS WITH A MINIMUM OF SEVEN BARRICADES PER LOCATION. SEE NOTE 8 ON SHEET 7 FOR ADDITIONAL INFORMATION.

3. PROPOSED TEMPORARY CONSTRUCTION STAGING / BATCH PLANT AREA EQUIPMENT HEIGHT LIMIT = 35 FEET. AT ALL OTHER LOCATIONS: EQUIPMENT HEIGHT LIMIT = 25 FEET.
4. SEE SHEET 6 FOR CRITICAL POINT DATA TABLE.
5. THE COST OF ALL MEASURES NECESSARY TO COMPLY WITH THE SAFETY PLAN SHALL BE INCLUDED IN THE MAINTENANCE OF TRAFFIC LUMP SUM PRICE.

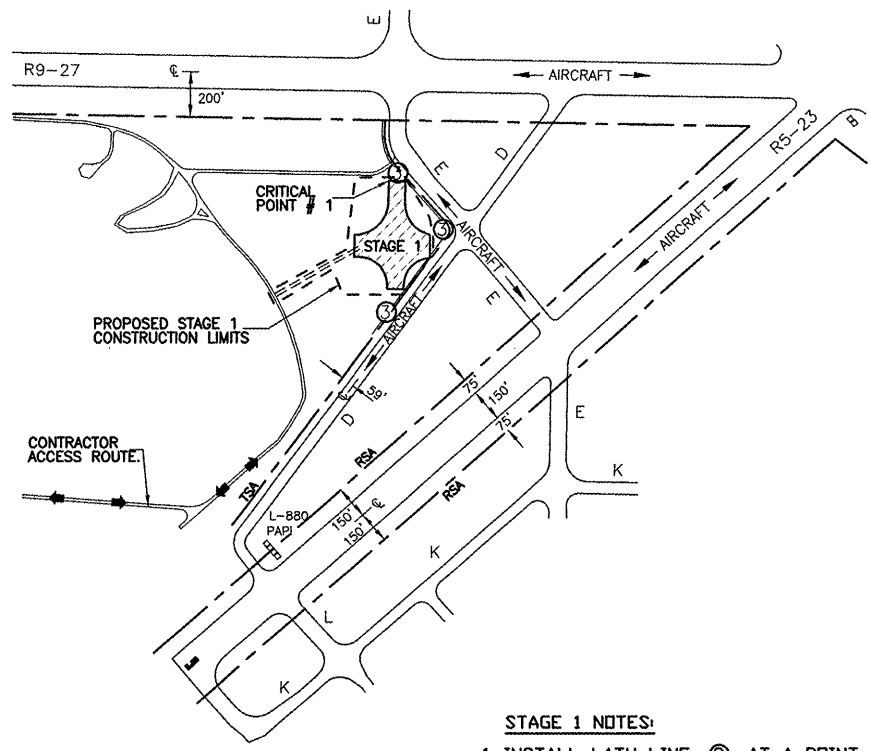


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STAGE 1 AIRFIELD STATUS

1. ALL RUNWAYS AND TAXIWAYS OPEN TO AIRCRAFT TRAFFIC.
2. CONTRACTOR SHALL NOT ENTER ANY SAFETY AREA.
3. CONTRACTOR SHALL NOT ENTER ANY CRITICAL AREA UNLESS AUTHORIZED BY THE MAA.

○ = PROPOSED BARRICADES



STAGE 1 BARRICADES, LATH LINES, AND CONSTRUCTION OPERATION LIMITS

STAGE 1 NOTES:

1. INSTALL LATH LINE ③ AT A POINT THAT IS 5' OFF OF THE TAXIWAY D / E SAFETY AREAS PRIOR TO THE START OF WORK.
2. SEE HAUL ROUTE CONSTRUCTION LIMITS FOR EARTHWORK SAFETY REQUIREMENTS WHEN HAULING BORROW SOILS DURING STAGE 1.

STAGE 3 AIRFIELD STATUS

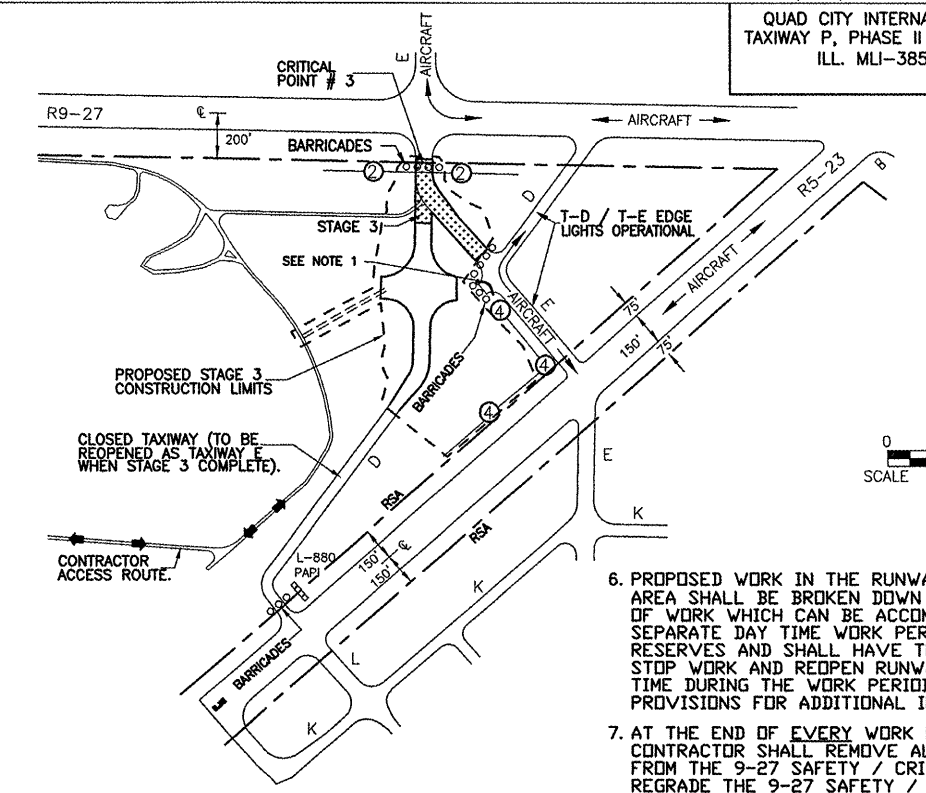
1. RUNWAY 9-27 CLOSED TO AIRCRAFT TRAFFIC DURING DAY TIME CONSTRUCTION WORK PERIODS ONLY. THE WORK PERIODS WILL BE SELECTED BY THE MAA BASED ON WEATHER AND AIR TRAFFIC CONDITIONS. RUNWAY 9-27 TO BE OPENED TO AIRCRAFT TRAFFIC BETWEEN THE DAY TIME WORK PERIODS. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
2. TAXIWAY D CLOSED BETWEEN RUNWAY 5-23 AND TAXIWAY E.
3. TAXIWAY E CLOSED BETWEEN RUNWAY 9-27 AND TAXIWAY D.
4. ALL OTHER PAVEMENTS OPEN TO AIRCRAFT.

○ = PROPOSED BARRICADES

STAGE 3 BARRICADES, LATH LINES, AND CONSTRUCTION OPERATION LIMITS

STAGE 3 NOTES:

1. INSTALL STAGE 3 TEMPORARY ELECTRICAL JUMPER CABLES (3 EACH) AS SHOWN ON SHEET 45 TO KEEP EDGE LIGHTS ENERGIZED ON ACTIVE TAXIWAYS.
2. SEE HAUL ROUTE CONSTRUCTION LIMITS FOR EARTHWORK SAFETY REQUIREMENTS WHEN HAULING BORROW SOILS DURING STAGE 3.



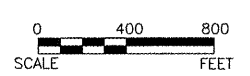
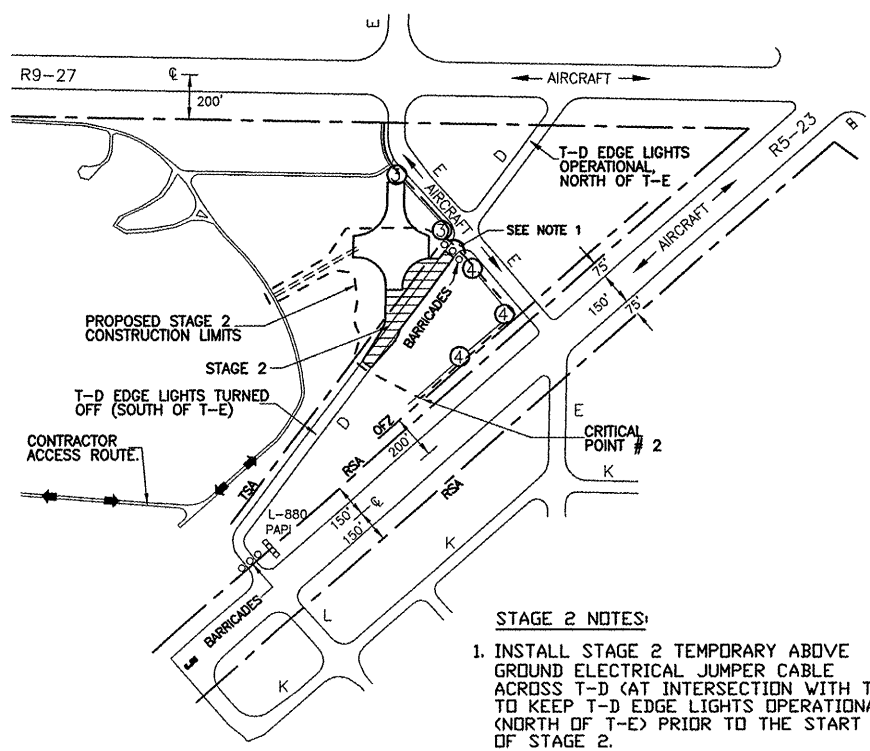
3. CONTRACTOR SHALL MINIMIZE THE TIME SPENT IN THE RUNWAY 9-27 SAFETY AREA.
4. RUNWAY 13-31 SHALL BE OPEN TO AIRCRAFT WHEN RUNWAY 9-27 IS CLOSED.
5. RUNWAY 9-27 CLOSED RUNWAY MARKINGS IN PLACE WHEN CONTRACTOR IS IN THE R9-27 RSA (BOTH ENDS OF RUNWAY).

6. PROPOSED WORK IN THE RUNWAY 9-27 SAFETY AREA SHALL BE BROKEN DOWN INTO UNITS OF WORK WHICH CAN BE ACCOMPLISHED IN SEPARATE DAY TIME WORK PERIODS. THE MAA RESERVES AND SHALL HAVE THE RIGHT TO STOP WORK AND REDPEN RUNWAY 9-27 AT ANY TIME DURING THE WORK PERIODS. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
7. AT THE END OF EVERY WORK PERIOD, THE CONTRACTOR SHALL REMOVE ALL ITEMS FROM THE 9-27 SAFETY / CRITICAL AREAS, REGRADE THE 9-27 SAFETY / CRITICAL AREAS AND REDPEN THE PAVEMENTS TO AIRCRAFT TRAFFIC. RUNWAY 9-27 SHALL BE OPEN TO AIRCRAFT TRAFFIC AT ALL TIMES WHEN THE CONTRACTOR IS NOT IN THE R9-27 SAFETY / CRITICAL AREAS. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
8. TEMPORARY GUIDANCE SIGNS ARE REQUIRED TO BE IN-PLACE PRIOR TO THE START OF STAGE 3. SEE SHEET 45A FOR THE STAGE 3 TEMPORARY GUIDANCE SIGN PLAN.

STAGE 2 AIRFIELD STATUS

1. TAXIWAY D CLOSED BETWEEN RUNWAY 5-23 AND TAXIWAY E.
2. ALL OTHER PAVEMENTS OPEN TO AIRCRAFT.

○ = PROPOSED BARRICADES



STAGE 2 BARRICADES, LATH LINES, AND CONSTRUCTION OPERATION LIMITS

STAGE 2 NOTES:

1. INSTALL STAGE 2 TEMPORARY ABOVE GROUND ELECTRICAL JUMPER CABLE ACROSS T-D (AT INTERSECTION WITH T-E) TO KEEP T-D EDGE LIGHTS OPERATIONAL (NORTH OF T-E) PRIOR TO THE START OF STAGE 2.
2. SEE HAUL ROUTE CONSTRUCTION LIMITS FOR EARTHWORK SAFETY REQUIREMENTS WHEN HAULING BORROW SOILS DURING STAGE 2.
3. TEMPORARY GUIDANCE SIGNS ARE REQUIRED TO BE IN-PLACE PRIOR TO THE START OF STAGE 2. SEE SHEET 45A FOR THE STAGE 2 TEMPORARY GUIDANCE SIGN PLAN.

STAGE 4 AIRFIELD STATUS

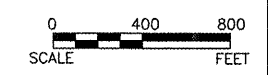
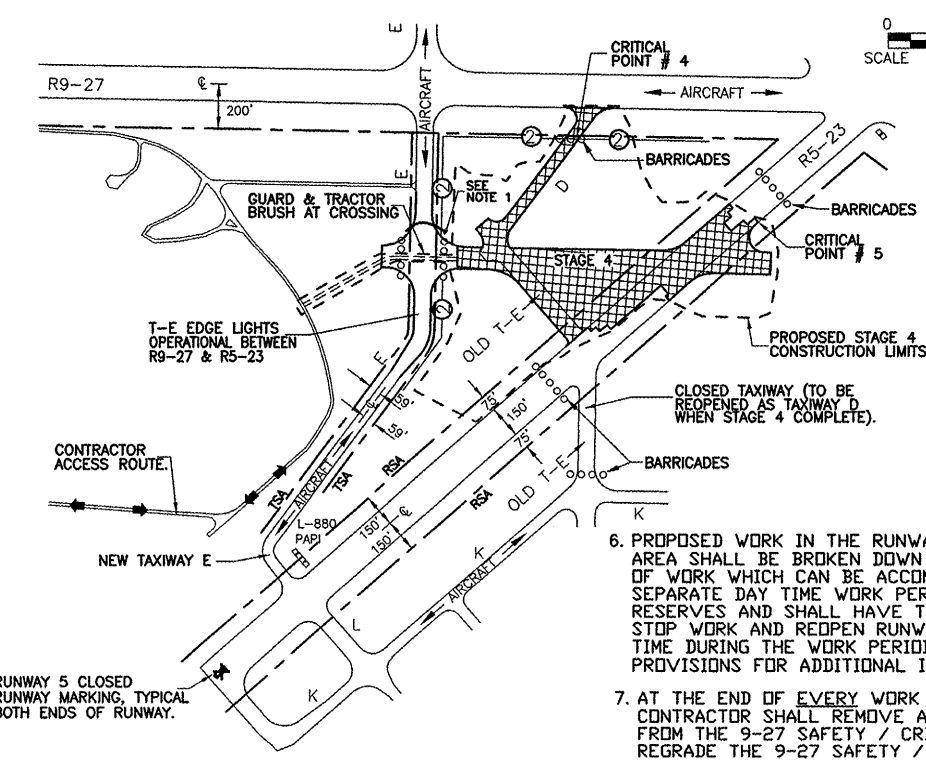
1. RUNWAY 9-27 CLOSED TO AIRCRAFT TRAFFIC DURING DAY TIME CONSTRUCTION WORK PERIODS ONLY. THE WORK PERIODS WILL BE SELECTED BY THE MAA BASED ON WEATHER AND AIR TRAFFIC CONDITIONS. RUNWAY 9-27 TO BE OPENED TO AIRCRAFT TRAFFIC BETWEEN THE DAY TIME WORK PERIODS. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
2. RUNWAY 5-23 CLOSED TO AIRCRAFT TRAFFIC.
3. TAXIWAY D CLOSED BETWEEN RUNWAY 9-27 AND OLD TAXIWAY E.
4. OLD TAXIWAY E CLOSED BETWEEN RUNWAY 5-23 AND TAXIWAY K.
5. NEW TAXIWAY E OPEN BETWEEN RUNWAY 9-27 AND RUNWAY 5-23.
6. ALL OTHER PAVEMENTS OPEN TO AIRCRAFT.

○ = PROPOSED BARRICADES

STAGE 4 BARRICADES, LATH LINES, AND CONSTRUCTION OPERATION LIMITS

STAGE 4 NOTES:

1. INSTALL STAGE 4 TEMPORARY ELECTRICAL JUMPER CABLE AS SHOWN ON SHEET 45 TO KEEP EDGE LIGHTS ENERGIZED ON ACTIVE TAXIWAYS.
2. SEE HAUL ROUTE CONSTRUCTION LIMITS FOR EARTHWORK SAFETY REQUIREMENTS WHEN HAULING BORROW SOILS DURING STAGE 4.



3. CONTRACTOR SHALL MINIMIZE THE TIME SPENT IN THE RUNWAY 9-27 SAFETY AREA.
4. RUNWAY 13-31 SHALL BE OPEN TO AIRCRAFT WHEN RUNWAY 9-27 IS CLOSED.
5. RUNWAY 9-27 CLOSED RUNWAY MARKINGS IN PLACE WHEN CONTRACTOR IS IN THE R9-27 RSA (BOTH ENDS OF RUNWAY).

6. PROPOSED WORK IN THE RUNWAY 9-27 SAFETY AREA SHALL BE BROKEN DOWN INTO UNITS OF WORK WHICH CAN BE ACCOMPLISHED IN SEPARATE DAY TIME WORK PERIODS. THE MAA RESERVES AND SHALL HAVE THE RIGHT TO STOP WORK AND REDPEN RUNWAY 9-27 AT ANY TIME DURING THE WORK PERIODS. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
7. AT THE END OF EVERY WORK PERIOD, THE CONTRACTOR SHALL REMOVE ALL ITEMS FROM THE 9-27 SAFETY / CRITICAL AREAS, REGRADE THE 9-27 SAFETY / CRITICAL AREAS AND REDPEN THE PAVEMENTS TO AIRCRAFT TRAFFIC. RUNWAY 9-27 SHALL BE OPEN TO AIRCRAFT TRAFFIC AT ALL TIMES WHEN THE CONTRACTOR IS NOT IN THE R9-27 SAFETY / CRITICAL AREAS. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
8. TEMPORARY AND PERMANENT GUIDANCE SIGNS ARE REQUIRED TO BE IN-PLACE PRIOR TO THE START OF STAGE 4. SEE SHEET 45A FOR THE STAGE 4 GUIDANCE SIGN PLAN.

TEMPORARY HAUL ROUTE

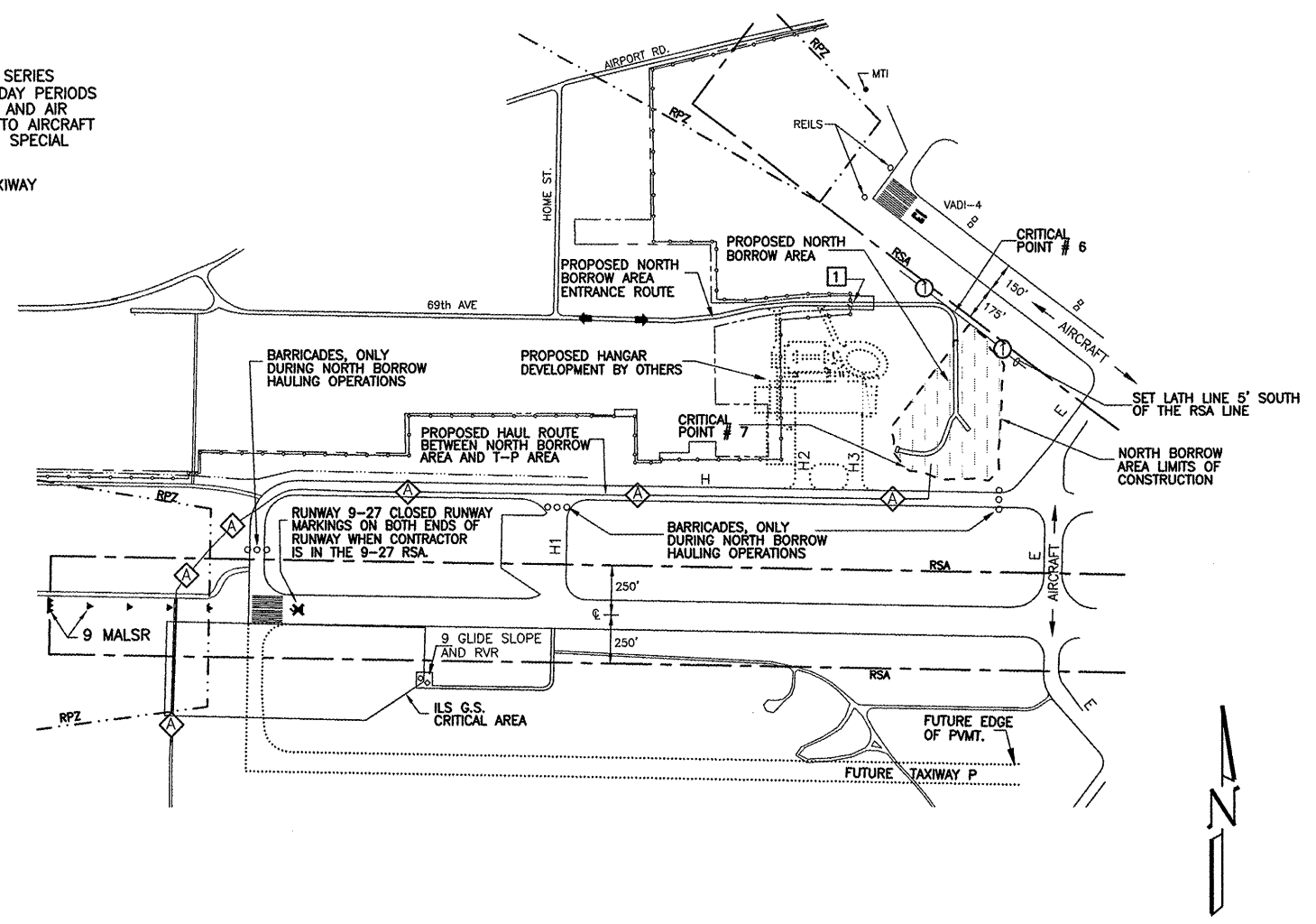
AIRFILED STATUS

1. RUNWAY 9-27 CLOSED TO AIRCRAFT TRAFFIC FOR A SERIES OF NON-CONSECUTIVE TWO DAY PERIODS. THE TWO DAY PERIODS WILL BE SELECTED BY THE MAA BASED ON WEATHER AND AIR TRAFFIC CONDITIONS. RUNWAY 9-27 TO BE OPENED TO AIRCRAFT TRAFFIC BETWEEN THE TWO DAY WORK PERIODS. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
2. TAXIWAY H CLOSED BETWEEN RUNWAY 9-27 AND TAXIWAY E DURING NORTH BORROW HAULING OPERATIONS.
3. RUNWAY 13-31 OPEN TO AIRCRAFT TRAFFIC.
4. ALL OTHER PAVEMENTS OPEN TO AIRCRAFT TRAFFIC.

○ = PROPOSED BARRICADES

TEMPORARY HAUL ROUTE NOTES:

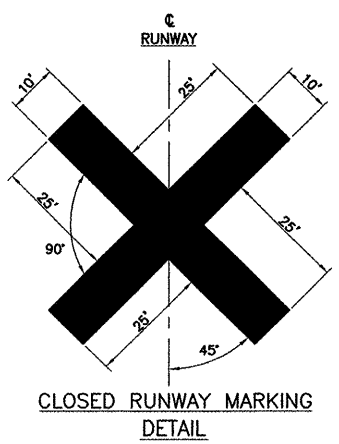
1. RUNWAY 9-27 CLOSED RUNWAY MARKINGS IN PLACE WHEN CONTRACTOR IS IN THE R9-27 RSA.
2. RUNWAY 13-31 SHALL BE OPEN TO AIRCRAFT WHEN RUNWAY 9-27 IS CLOSED.
3. CONTRACTOR SHALL MINIMIZE THE TIME SPENT IN THE RUNWAY 9-27 SAFETY AND G.S. CRITICAL AREAS. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
4. THE HAULING OPERATIONS OUT OF THE NORTH BORROW AREA SHALL BE BROKEN DOWN INTO UNITS OF WORK WHICH CAN BE ACCOMPLISHED IN SEPARATE TWO DAY PERIODS. THE MAA RESERVES AND SHALL HAVE THE RIGHT TO STOP WORK AND REOPEN RUNWAY 9-27 / TAXIWAY H AT ANY TIME DURING THE WORK PERIODS. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
5. AT THE END OF EVERY WORK PERIOD, THE CONTRACTOR SHALL REMOVE ALL ITEMS FROM THE R9-27 AND TAXIWAY H SAFETY / CRITICAL AREAS, REGRADE THE R9-27 AND TAXIWAY H SAFETY / CRITICAL AREAS AND REOPEN THE R9-27 & T-H PAVEMENTS TO AIRCRAFT TRAFFIC. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.



TEMPORARY HAUL ROUTE BARRICADES AND CONSTRUCTION OPERATION LIMITS



NUMBER	LOCATION	LATITUDE	LONGITUDE	GROUND ELEVATION	
				EXISTING	PROPOSED
1	NW CORNER OF STAGE 1	41D 26' 51.90"	90D 30' 45.27"	575.8'	580.9'
2	SW CORNER OF STAGE 2	41D 26' 42.50"	90D 30' 42.10"	576.5'	576.5'
3	N EDGE OF STAGE 3 (WITH RUNWAY 9-27 OPEN)	41D 26' 58.41"	90D 30' 45.09"	583.9'	584.0'
4	N EDGE OF STAGE 4 (WITH RUNWAY 9-27 OPEN)	41D 26' 58.24"	90D 30' 34.14"	584.5'	582.6'
5	NE CORNER OF STAGE 4	41D 26' 50.19"	90D 30' 24.74"	582.5'	582.5'
6	NE CORNER OF NORTH BORROW AREA	41D 27' 12.04"	90D 30' 50.97"	587.0'	587.0'
7	SW CORNER OF NORTH BORROW AREA	41D 27' 03.61"	90D 30' 55.20"	584.7'	584.7'
8	NE CORNER OF SOUTH BORROW AREA	41D 26' 11.43"	90D 29' 07.75"	631.0'	631.0'
9	NE CORNER OF BATCH PLANT STAGING AREA	41D 26' 57.24"	90D 31' 43.54"	580.0'	580.0'



CLOSED RUNWAY MARKING DETAIL

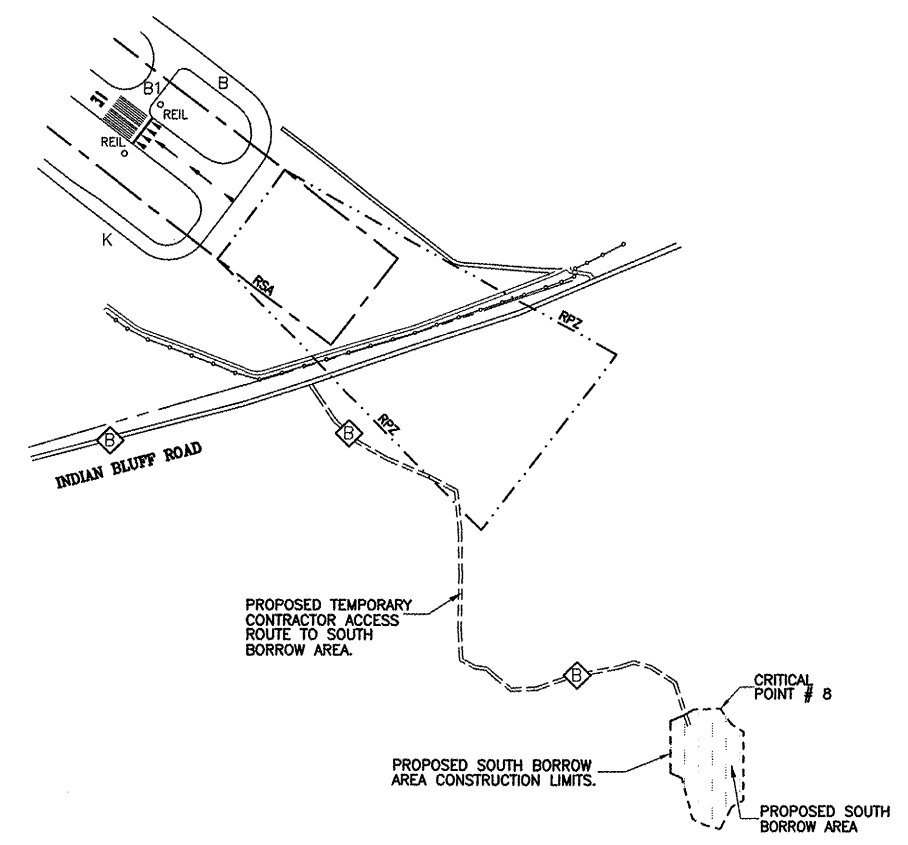
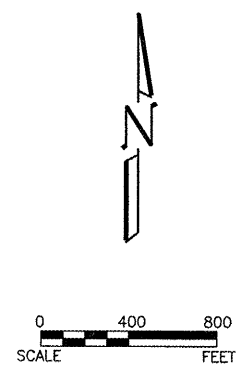
CLOSED RUNWAY MARKINGS NOTES:

1. SOLID CROSS FOR CLOSED RUNWAY IS REQUIRED AT EACH END OF THE RUNWAY DURING ALL RUNWAY CLOSURES AND SHALL BE PLACED DIRECTLY OVER THE RUNWAY IDENTIFICATION NUMERALS.
2. THE CONTRACTOR SHALL INSTALL, REMOVE AND REINSTALL THE CROSSES AS REQUIRED BY WORKING CONDITIONS AND AS APPROVED BY THE RESIDENT ENGINEER.
3. COLOR OF ALL CROSSES SHALL BE AVIATION YELLOW.
4. SOLID CROSSES ARE TO BE CONSTRUCTED OF PLYWOOD, CANVAS, OR ANY OTHER APPROVED SOLID MATERIALS AND SHALL BE ADHERED TO THE RUNWAY IN A MANNER ACCEPTABLE TO THE RESIDENT ENGINEER. THE CONTRACTOR SHALL MAINTAIN THE CROSSES IN EXCELLENT CONDITION.

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SAFETY PLAN NOTES:

1. THE METROPOLITAN AIRPORT AUTHORITY (MAA) SHALL DETERMINE WHEN AND WHERE THE CONTRACTOR WILL BE ALLOWED TO WORK. THE CONTRACTOR SHALL BE PREPARED TO EXIT THE RESTRICTED AREAS, SAFETY AREAS AND/OR AIR FIELD AT ALL TIMES WHEN DIRECTED TO DO SO BY THE MAA. ALL PAVEMENTS SHALL BE OPEN TO AIRCRAFT TRAFFIC UNLESS "NOTAMED" OTHERWISE DURING THE CONSTRUCTION OF THIS PROJECT. SEE THE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION ON WORKING RESTRICTIONS AND CONDITIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLATION OF ALL BARRICADES AND LATH LINES AS SHOWN, PRIOR TO ANY EQUIPMENT ENTERING THE AIRFIELD OUTSIDE OF THE CONSTRUCTION STAGING AREA.
2. SOLID CLOSED RUNWAY MARKING CROSSES ARE REQUIRED AT EACH END OF THE RUNWAY DURING ALL RUNWAY CLOSURES. SEE SPECIAL PROVISIONS FOR DETAILS OF CROSSES.
3. THE CONTRACTOR SHALL NOT TRAVEL IN OR THROUGH THE RESTRICTED AREAS AND/OR SAFETY AREAS UNLESS PERMISSION IS RECEIVED AND CONTACT HAS BEEN MADE WITH THE FAA CONTROL TOWER.
4. THE CONTRACTOR'S EMPLOYEES SHALL PARK IN THE PROPOSED CONSTRUCTION STAGING AREA. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRANSPORTING EMPLOYEES TO THE PROPOSED CONSTRUCTION SITE. ONLY CONTRACTOR'S MARKED VEHICLES AND EQUIPMENT SHALL BE ALLOWED ON THE AIRFIELD. ALL CONSTRUCTION VEHICLES AND EQUIPMENT, EXCEPT THE PAVING TRAIN, SHALL BE PARKED IN THE CONSTRUCTION STAGING AREA DURING ALL NON-WORKING HOUR. THE PAVING TRAIN MAY BE PARKED ON THE AIRFIELD OUTSIDE ALL RESTRICTED AREAS IN A LOCATION AUTHORIZED BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL LOCATE HIS TRAILER, THE ENGINEERS FIELD OFFICE, AND ALL OTHER NECESSARY FACILITIES AND MATERIALS IN THE PROPOSED CONSTRUCTION STAGING AREA. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONSTRUCT WHATEVER ACCESS ROAD HE DEEMS NECESSARY BETWEEN THE EXISTING ROADS AND THE CONSTRUCTION AREAS. OVERNIGHT PARKING OF EMPLOYEE VEHICLES IN THE CONSTRUCTION STAGING AREA WILL NOT BE ALLOWED UNLESS AUTHORIZED BY THE MAA.
5. THE CONTRACTOR SHALL PROCURE ENOUGH QUAD CITY AIRPORT SECURITY/IDENTIFICATION BADGES FOR HIS EMPLOYEES AND SUBCONTRACTOR EMPLOYEES FROM THE AIRPORT AUTHORITY TO GUARANTEE AT LEAST ONE MEMBER OF EACH CONSTRUCTION CREW WILL HAVE A BADGE. ANY CONSTRUCTION CREW WITHOUT A BADGED MEMBER SHALL NOT BE ALLOWED ON THE AIRFIELD SITE. ALL INDIVIDUALS WHO ARE ISSUED SECURITY BADGES MUST CORRECTLY WEAR THEIR OWN BADGE WHILE ON THE AIRFIELD. BADGES MAY BE OBTAINED, AFTER MEETING SECURITY REQUIREMENTS, FROM THE SECURITY OFFICE AT THE QCI AIRPORT. A FIFTY DOLLAR (\$50.00) REFUNDABLE DEPOSIT IS REQUIRED FOR EACH BADGE.
6. THE CONTRACTOR SHALL USE THE DESIGNATED HAUL ROUTES, CONSTRUCTION STAGING AREA, AND ENTRANCE TO THE AIRFIELD AS SHOWN ON THE SAFETY PLAN SHEET. NO CONSTRUCTION TRAFFIC SHALL BE ALLOWED ON THE AIRFIELD OUTSIDE THE HAUL ROUTES AND WORK AREAS UNLESS OTHERWISE AUTHORIZED BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL SUPPLY AND INSTALL TEMPORARY LOCKS ON EXISTING GATES AT THE PROPOSED CONSTRUCTION ENTRANCE. THE CONTRACTOR SHALL BE RESPONSIBLE TO KEEP THE CONSTRUCTION ENTRANCE GATE CLOSED AT ALL TIMES. VIOLATIONS ARE SUBJECT TO FINES/PENALTIES AND THE CONTRACTOR SHALL PAY ANY FINES INCURRED, INCLUDING FINES INCURRED BY THE RESIDENT ENGINEER AND/OR MAA DUE TO THE CONTRACTOR'S NEGLIGENCE. ALL VEHICULAR TRAFFIC SHALL BE KEPT TO A MINIMUM. ALL VEHICLES ON THE APRONS, RAMPS, TAXIWAYS, OR RUNWAYS REQUIRE THE APPROVAL OF THE RESIDENT ENGINEER / MAA.
7. THE CONTRACTOR AND EACH SUBCONTRACTOR SHALL DESIGNATE, PRIOR TO BEGINNING CONSTRUCTION, A PERSON OR PERSONS WHO CAN BE CONTACTED IN AN EMERGENCY INVOLVING THEIR WORK OR EQUIPMENT. THESE DESIGNATED PEOPLE SHALL BE AVAILABLE ON A 24-HOUR / 7 DAYS PER WEEK BASIS.
8. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO INSTALL TYPE II BARRICADES EQUIPPED WITH FLASHING RED LIGHTS AND 20" X 20" ORANGE FLAGS AS SHOWN ON THE SAFETY PLAN OR AS DIRECTED BY THE RESIDENT ENGINEER. THE CONTRACTOR WILL FURNISH, MAINTAIN, AND MOVE THE BARRICADES AS REQUIRED BY THE RESIDENT ENGINEER. THE BARRICADES SHALL BE SUFFICIENTLY WEIGHTED WITH SANDBAGS OR OTHER APPROPRIATE METHOD TO WITHSTAND HIGH WINDS AND/OR JET BLAST WITHOUT DISLOCATION. BARRICADES SHALL BE CHECKED DAILY BY THE CONTRACTOR FOR PROPER PLACEMENT, ADEQUATE BALLAST, PROPER LIGHTING, PROPER FLAGGING AND WORKING BATTERIES. ALL DISCREPANCIES SHALL BE CORRECTED IMMEDIATELY. BARRICADES SHALL BE RELOCATED UPON THE COMPLETION OF EACH STAGE OF CONSTRUCTION. COST TO BE INCLUDED IN MAINTENANCE OF TRAFFIC LUMP SUM PRICE.
9. WHEN THE CONTRACTOR'S VEHICLES AND EQUIPMENT ARE ON THE AIRFIELD, THEY SHALL BE PROPERLY MARKED. THE MARKING SHALL CONSIST OF A THREE FOOT (3') SQUARE FLAG WITH A CHECKERED PATTERN OF INTERNATIONAL ORANGE AND WHITE SQUARES OF NOT LESS THAN ONE FOOT (1') ON EACH SIDE, DISPLAYED IN FULL VIEW ABOVE THE VEHICLE OR EQUIPMENT. EACH VEHICLE SHALL HAVE A FLASHING YELLOW LIGHT MOUNTED ON TOP OF THE ROOF.
10. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO RESTORE THE CONSTRUCTION STAGING AREA, SERVICE ROADS, ACCESS ROADS, AND HAUL ROADS TO THEIR ORIGINAL CONDITIONS FOLLOWING COMPLETION OF CONSTRUCTION. THIS MAY INCLUDE, BUT NOT BE LIMITED TO, REGRADING, FERTILIZING, SEEDING AND MULCHING OF EARTH SURFACES AND/OR REGRADING, GRAVELING & SEAL COATING OF TREATED SURFACES, AS REQUIRED, TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL GROUND SURFACES, GRAVEL ROADS, PAVEMENTS, AND OTHER FACILITIES DAMAGED BY THE CONTRACTOR WHILE COMPLETING THE PROPOSED WORK SHALL BE REPAIRED OR RETURNED TO ITS ORIGINAL STATE. COST TO BE INCLUDED IN THE MAINTENANCE OF TRAFFIC LUMP SUM PRICE.
11. THE CONTRACTOR SHALL IMMEDIATELY SWEEP OR PICK UP ANY SOIL, DEBRIS, AGGREGATE CHIPS OR ROCK, OR LOOSE MATERIALS WHICH HAS BEEN DROPPED ONTO AIRPORT ROADS, RUNWAYS, TAXIWAYS, OR SODDED AREAS.
12. THE DISPOSAL OF ALL MATERIALS NOT TO BE INCORPORATED IN EMBANKMENTS ON THE PROJECT SHALL BE ACCOMPLISHED BY THE CONTRACTOR AT A LOCATION OFF AIRPORT PROPERTY.
13. THE SEQUENCE OF CONSTRUCTION OPERATIONS AND DESCRIPTION OF CONDITIONS ARE OUTLINED IN THE PROJECT SPECIAL PROVISIONS. THE CONTRACTOR SHALL GIVE THE MAA A 96 HOUR NOTICE PRIOR TO THE START OF ANY WORK REQUIRING THE CLOSING OF ANY PAVEMENTS TO AIRCRAFT TRAFFIC SO THAT A NOTAM CAN BE ISSUED.
14. WORKING HOURS OF THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL CONFORM TO ALL APPLICABLE LOCAL LAWS, INCLUDING ANY NOISE CONTROL.
15. NO MOUNDS OF DIRT OR IRREGULARITIES GREATER THAN 3" WHICH, IN THE OPINION OF THE RESIDENT ENGINEER, COULD INTERFERE WITH ANY AIRFIELD OPERATIONS WILL BE PERMITTED ON THE AIRFIELD. NO EXPOSED FACES IN EXCESS OF ONE AND ONE-HALF (1-1/2) INCHES IN HEIGHT AND 2:1 SLOPES ON ANY EXCAVATION WILL BE PERMITTED WITHIN THE RESTRICTED AREAS.
16. DUST ABATEMENT MEASURES WILL BE REQUIRED, WHEN IN THE OPINION OF THE RESIDENT ENGINEER, A HAZARD TO AIR TRAFFIC, LOCAL RESIDENCES, OR CONSTRUCTION PROJECT PERSONNEL EXISTS. PREVENTIVE MEASURES TO BE ACCOMPLISHED BY THE CONTRACTOR SHALL INCLUDE, BUT NOT BE LIMITED TO, WATERING AND TREATMENT WITH CALCIUM CHLORIDE.
17. BY THE END OF EACH WORK DAY AND PRIOR TO LEAVING THE AIRFIELD, THE CONTRACTOR SHALL HAVE THOROUGHLY SWEEPED THE AIR TRAFFIC CORRIDORS ADJACENT TO THE WORK AREAS TO REMOVE DUST AND DEBRIS. IN ADDITION, ALL AIR TRAFFIC AREAS USED BY CONSTRUCTION PERSONNEL AND EQUIPMENT MUST BE CONTINUOUSLY SWEEPED AND MAINTAINED FREE OF DEBRIS. SWEEPERS SHALL BE PROVIDED BY THE CONTRACTOR FOR THE ENTIRE LENGTH OF THE CONTRACT AND SHALL BE OF A TYPE CAPABLE OF REMOVING ALL DUST AND DEBRIS TO THE SATISFACTION OF THE MAA. SWEEPERS MUST BE COMMERCIAL QUALITY AND APPROVED BY THE RESIDENT ENGINEER AND MAA PRIOR TO THE START OF CONSTRUCTION.
18. THE CONTRACTOR SHALL INSTALL AND MAINTAIN LATH LINES DURING THE LENGTH OF THE PROJECT AS SHOWN OR DIRECTED BY THE RESIDENT ENGINEER. SEE PROJECT SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
19. NO CHANGES SHALL BE MADE IN ANY PROVISIONS OF THIS SAFETY PLAN UNLESS APPROVED IN WRITING BY THE METROPOLITAN AIRPORT AUTHORITY, THE ILLINOIS DEPARTMENT OF TRANSPORTATION, DIVISION OF AERONAUTICS, AND THE FEDERAL AVIATION ADMINISTRATION. THE COST OF ALL MEASURES NECESSARY TO COMPLY WITH THE SAFETY PLAN SHALL BE INCLUDED IN THE MAINTENANCE OF TRAFFIC LUMP SUM PRICE.



PROPOSED SOUTH BORROW AREA LOCATION MAP

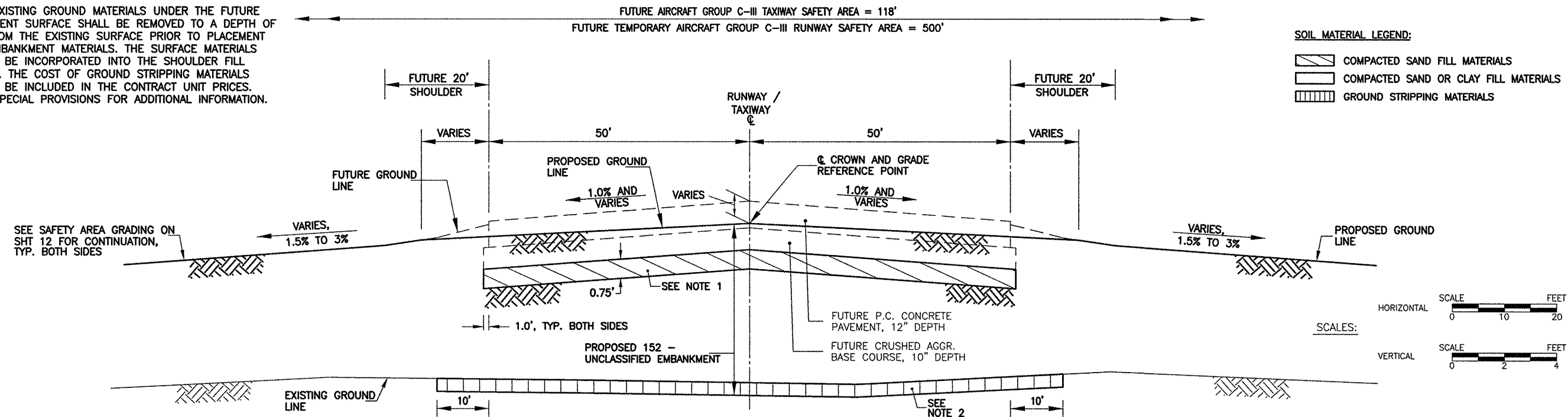
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NOTES:

1. INSTALL COMPACTED SAND FILL MATERIALS UNDER (AND WITHIN 1.0' OF) THE FUTURE PAVEMENT FROM A POINT 1.83' BELOW THE FUTURE PAVEMENT SURFACE TO A POINT 2.58' BELOW THE FUTURE PAVEMENT SURFACE.
2. THE EXISTING GROUND MATERIALS UNDER THE FUTURE PAVEMENT SURFACE SHALL BE REMOVED TO A DEPTH OF 6" FROM THE EXISTING SURFACE PRIOR TO PLACEMENT OF EMBANKMENT MATERIALS. THE SURFACE MATERIALS SHALL BE INCORPORATED INTO THE SHOULDER FILL AREAS. THE COST OF GROUND STRIPPING MATERIALS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICES. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

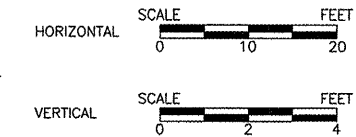
PROPOSED TYPICAL SECTION - EARTHWORK ONLY

TAXIWAY P / TEMPORARY RUNWAY 10-28,
STA. 627+15 TO STA. 627+65 & STA. 645+03 TO STA. 645+30



SOIL MATERIAL LEGEND:

- COMPACTED SAND FILL MATERIALS
- COMPACTED SAND OR CLAY FILL MATERIALS
- GROUND STRIPPING MATERIALS

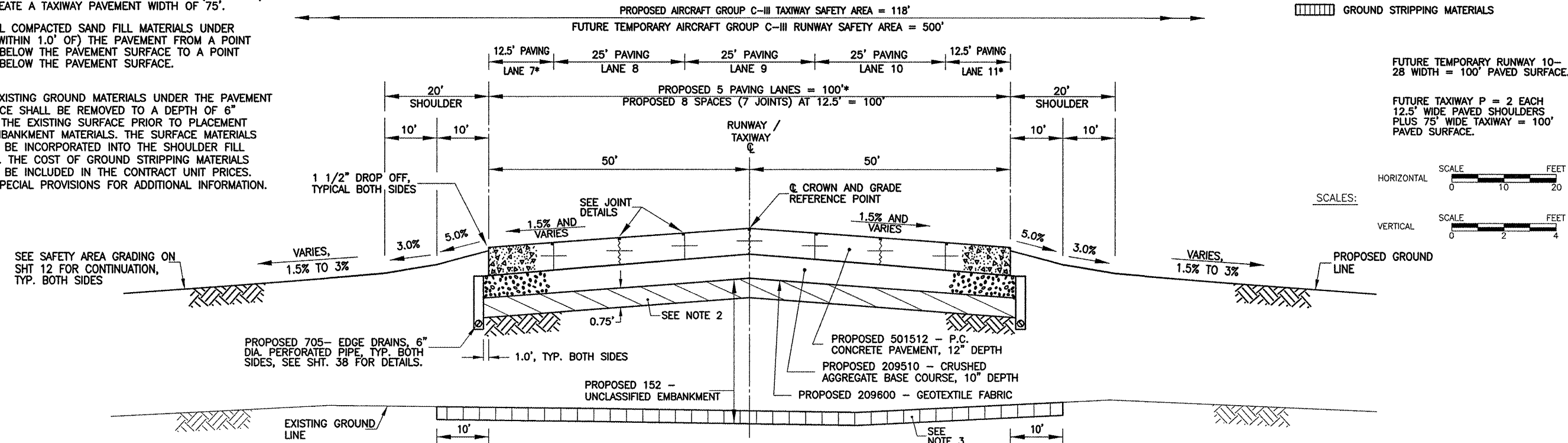


NOTES:

1. * = IF REQUIRED BY THE FAA, A FUTURE PROJECT COULD REMOVE THE OUTSIDE 12.5' LANES (7 AND 11) TO CREATE A TAXIWAY PAVEMENT WIDTH OF 75'.
2. INSTALL COMPACTED SAND FILL MATERIALS UNDER (AND WITHIN 1.0' OF) THE PAVEMENT FROM A POINT 1.83' BELOW THE PAVEMENT SURFACE TO A POINT 2.58' BELOW THE PAVEMENT SURFACE.
3. THE EXISTING GROUND MATERIALS UNDER THE PAVEMENT SURFACE SHALL BE REMOVED TO A DEPTH OF 6" FROM THE EXISTING SURFACE PRIOR TO PLACEMENT OF EMBANKMENT MATERIALS. THE SURFACE MATERIALS SHALL BE INCORPORATED INTO THE SHOULDER FILL AREAS. THE COST OF GROUND STRIPPING MATERIALS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICES. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

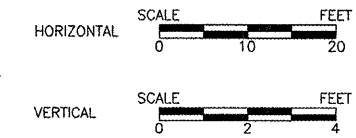
PROPOSED TYPICAL SECTION - PCC PAVING

TAXIWAY P / TEMPORARY RUNWAY 10-28,
STA. 627+65 TO STA. 640+21 & STA. 642+44 TO STA. 645+03



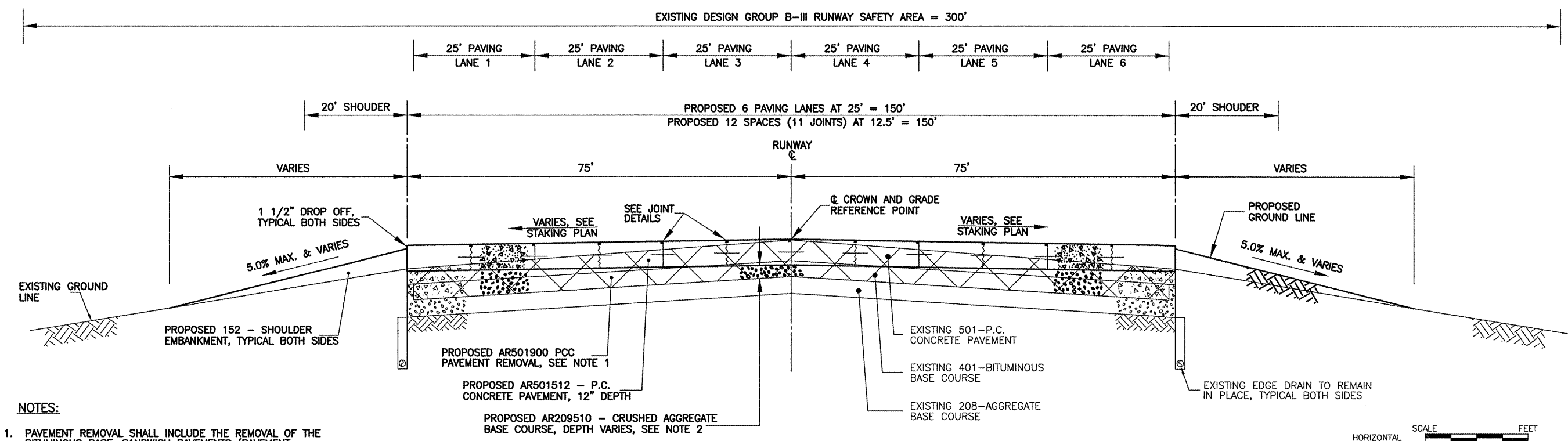
SOIL MATERIAL LEGEND:

- COMPACTED SAND FILL MATERIALS
- COMPACTED SAND OR CLAY FILL MATERIALS
- GROUND STRIPPING MATERIALS



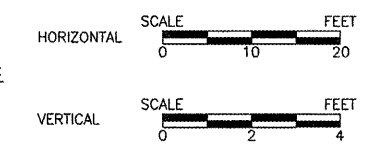
PROPOSED TYPICAL SECTION - REMOVE & REPLACE PCC PAVING
 RUNWAY 5-23, STA. 33+25 TO STA. 42+25

AIRCRAFT APPROACH CATEGORY B
 AIRPLANE DESIGN GROUP III
 VISUAL RUNWAY

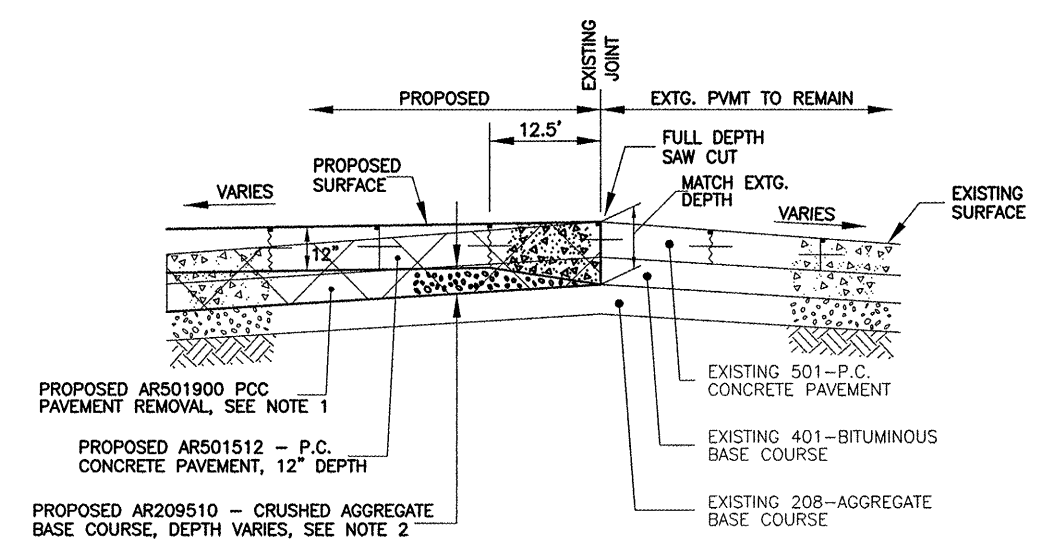


NOTES:

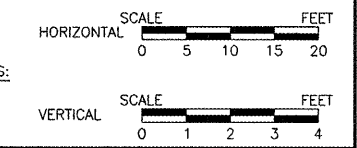
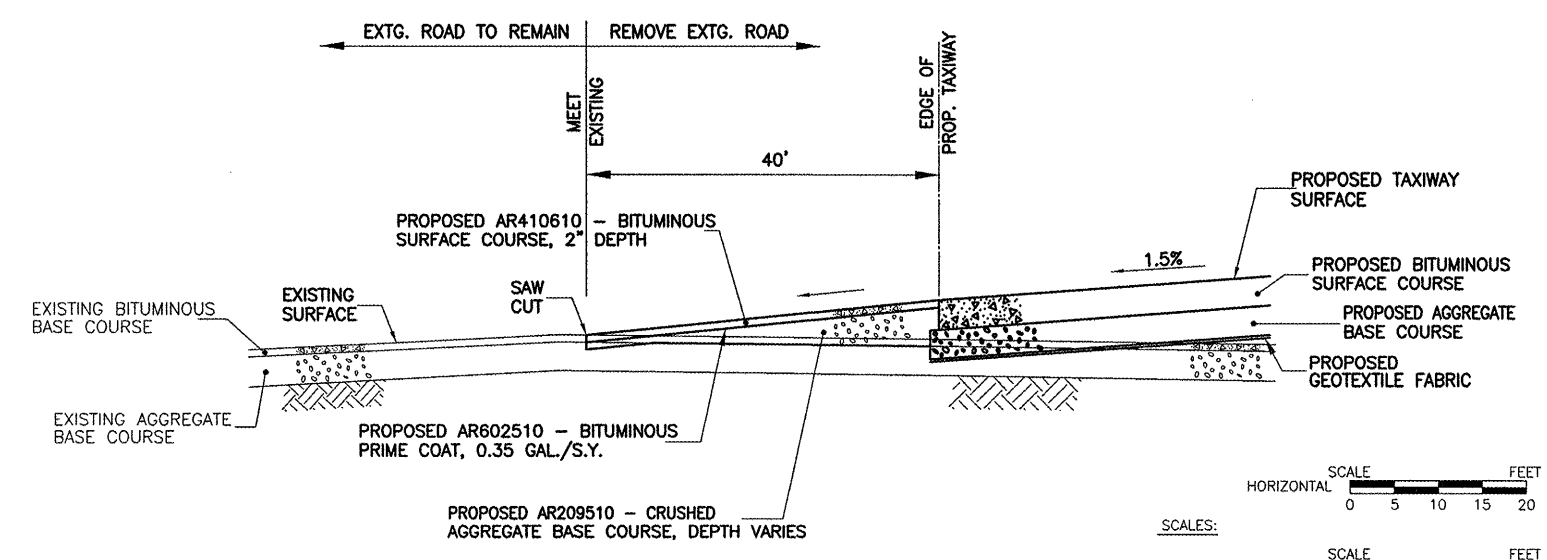
- PAVEMENT REMOVAL SHALL INCLUDE THE REMOVAL OF THE BITUMINOUS BASE. SANDWICH PAVEMENTS (PAVEMENT - ROCK - PAVEMENT) SHALL NOT BE ALLOWED.
- VARY DEPTH OF AR209510 CRHD AGGR BSE CRSE UNDER RUNWAY 5-23 TO FILL VOID BETWEEN THE BOTTOM OF THE PROPOSED PCC PAVEMENT AND THE TOP OF THE EXISTING BASE ROCK (THAT REMAINS IN PLACE).



PROPOSED TYPICAL DETAIL, TIE-IN TO EXISTING PAVEMENT
 RUNWAY 5-23, STA. 33+25 TO STA. 37+00 AND STA. 41+05 TO STA. 42+25



PROPOSED DETAIL, RECONSTRUCT EXTG. SERVICE ROAD
 RELOCATED T-E, STA. 854+00 RIGHT



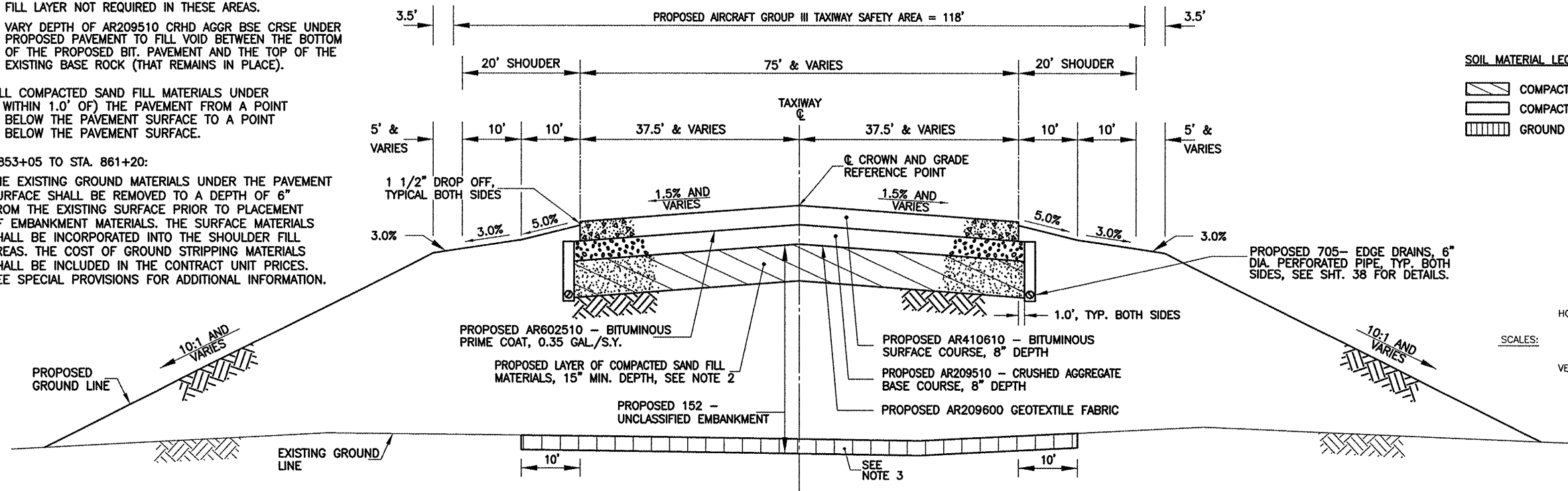
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NOTES:

1. STA. 851+85 TO STA. 853+05 AND STA. 861+20 TO STA. 862+15:
 - A. REMOVE THE EXISTING BITUMINOUS PAVEMENT IN THESE AREAS.
 - B. GEOTEXTILE FABRIC AND COMPACTED NON-FROST SAND FILL LAYER NOT REQUIRED IN THESE AREAS.
 - C. VARY DEPTH OF AR209510 CRHD AGGR BSE CRSE UNDER PROPOSED PAVEMENT TO FILL VOID BETWEEN THE BOTTOM OF THE PROPOSED BIT. PAVEMENT AND THE TOP OF THE EXISTING BASE ROCK (THAT REMAINS IN PLACE).
2. INSTALL COMPACTED SAND FILL MATERIALS UNDER (AND WITHIN 1.0' OF) THE PAVEMENT FROM A POINT 1.33' BELOW THE PAVEMENT SURFACE TO A POINT 2.58' BELOW THE PAVEMENT SURFACE.
3. STA. 853+05 TO STA. 861+20:

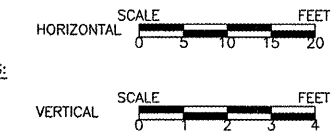
THE EXISTING GROUND MATERIALS UNDER THE PAVEMENT SURFACE SHALL BE REMOVED TO A DEPTH OF 6" FROM THE EXISTING SURFACE PRIOR TO PLACEMENT OF EMBANKMENT MATERIALS. THE SURFACE MATERIALS SHALL BE INCORPORATED INTO THE SHOULDER FILL AREAS. THE COST OF GROUND STRIPPING MATERIALS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICES. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

PROPOSED TYPICAL SECTION - BITUMINOUS PAVEMENT
RELOCATED TAXIWAY E, STA. 851+85 TO STA. 857+20, RELOCATED TAXIWAY D, STA. 858+20 TO STA. 862+15 AND EXISTING TAXIWAY E, STA. 880+67.57 TO STA. 882+40



SOIL MATERIAL LEGEND:

- COMPACTED SAND FILL MATERIALS
- COMPACTED SAND OR CLAY FILL MATERIALS
- GROUND STRIPPING MATERIALS

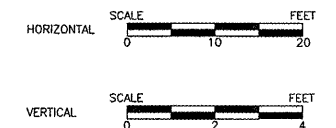
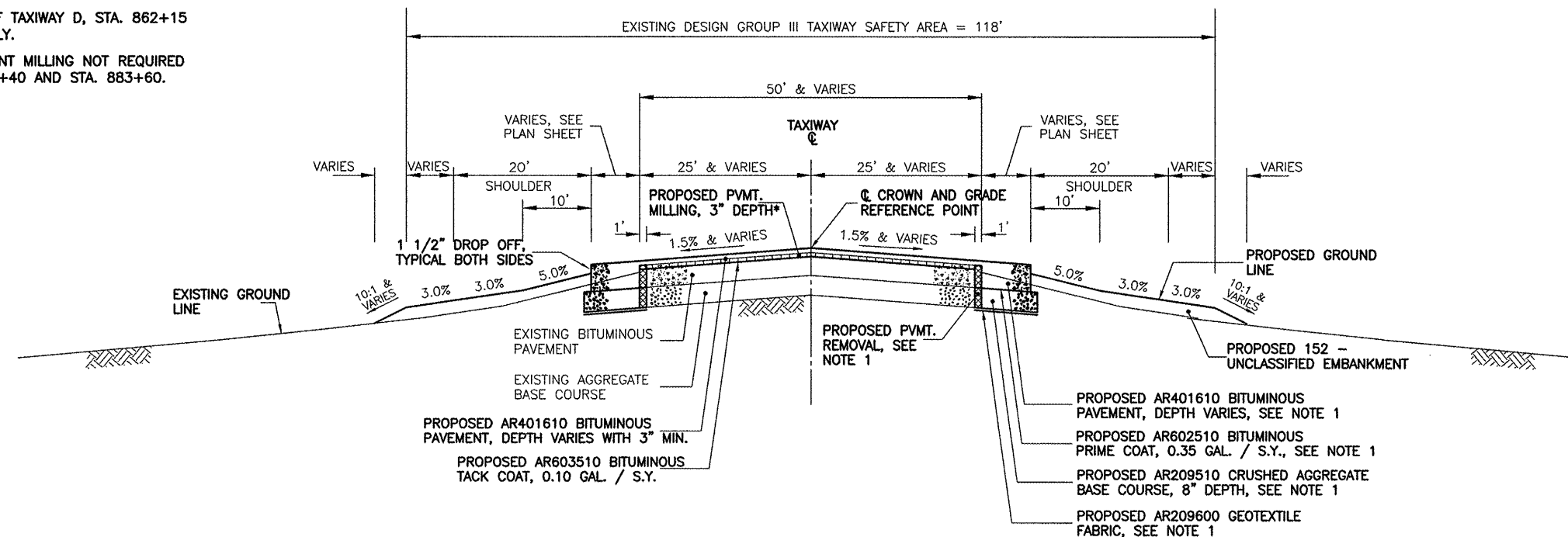


PROPOSED TYPICAL SECTION - BITUMINOUS OVERLAY / WIDENING

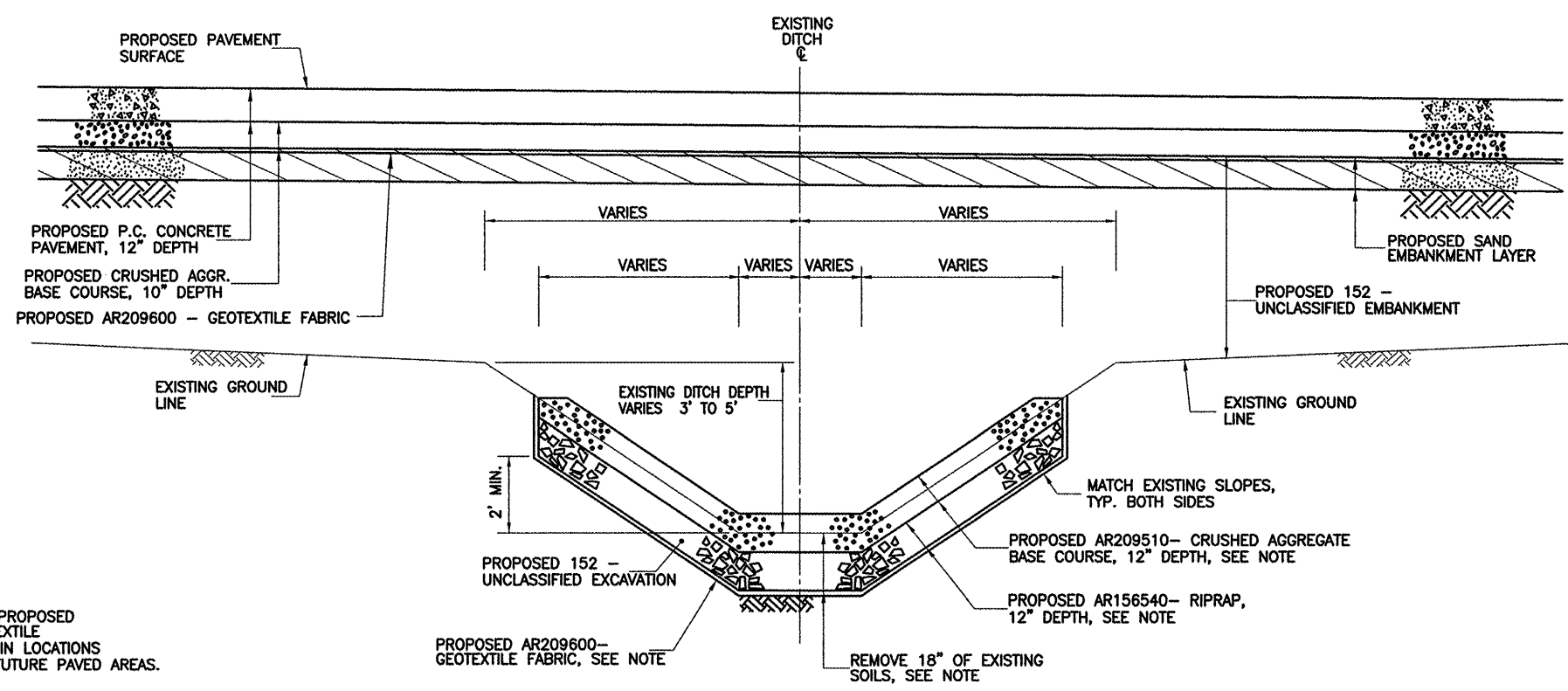
RELOCATED TAXIWAY D, STA. 862+15 TO STA. 863+62.62
AND EXISTING TAXIWAY E, STA. 882+40 TO STA. 884+31.11

NOTES:

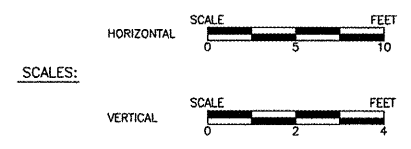
1. TYPICAL, BOTH SIDES OF TAXIWAY D, STA. 862+15 TO STA. 863+62.62 ONLY.
2. * = PROPOSED PAVEMENT MILLING NOT REQUIRED BETWEEN STA. 882+40 AND STA. 883+60.



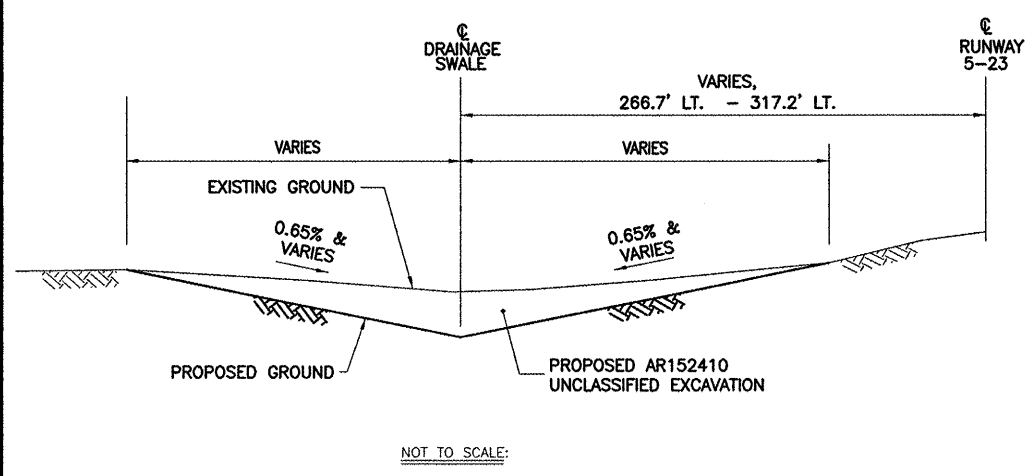
PROPOSED TYPICAL SECTION
 EXISTING DITCH TO BE ABANDONED IN PLACE
 STA. 629+70 LT. & RT.



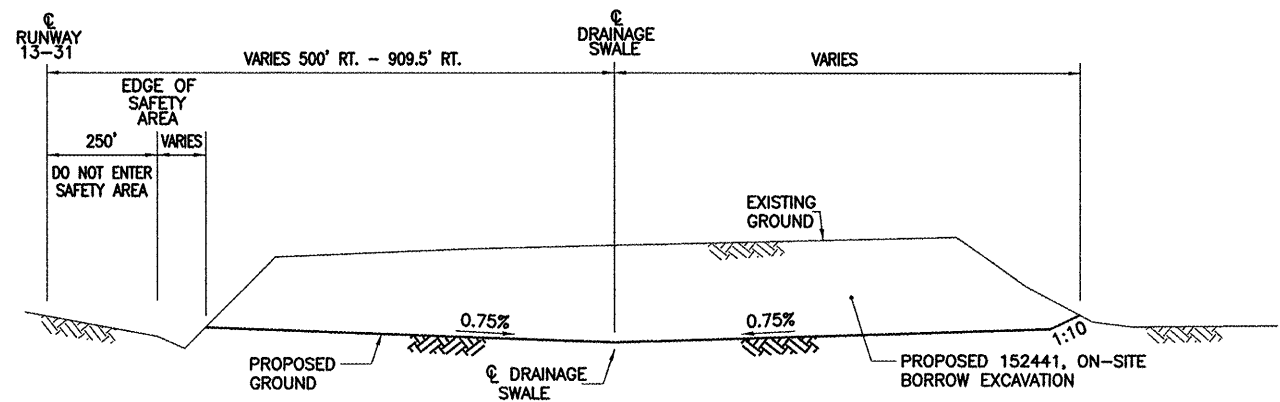
NOTE:
 REMOVE EXISTING SOIL MATERIALS AND INSTALL PROPOSED CRUSHED AGGREGATE BASE, RIP RAP AND GEOTEXTILE FABRIC IN BOTTOM OF ABANDONED DITCH ONLY IN LOCATIONS UNDER (OR WITHIN 10' OF) THE PROPOSED / FUTURE PAVED AREAS.



PROPOSED TYPICAL SECTION - RUNWAY 5-23 BASE BID DRAINAGE SWALE
 RUNWAY 5-23 LEFT, STA. 26+17 TO STA. 31+94



PROPOSED TYPICAL SECTION - NORTH ON-SITE BORROW AREA

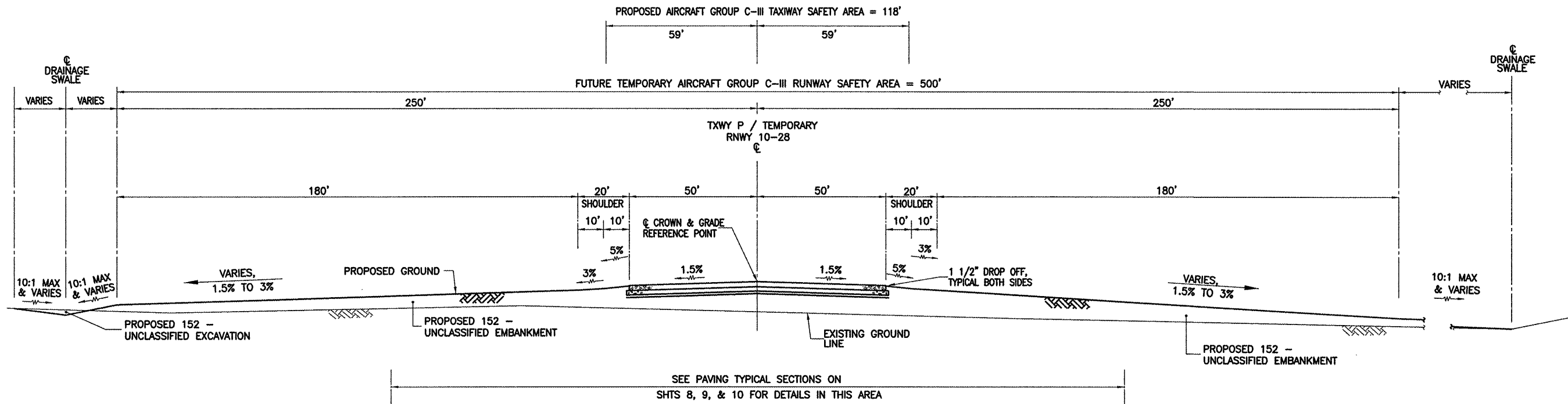


NOTE:
 SOILS HAULED FROM THE NORTH ON-SITE BORROW AREA TO THE TAXIWAY P CONSTRUCTION SITE AREA SHALL BE PAID FOR BY CUBIC YARDS REMOVED UNDER CONTRACT ITEM 152441, ON-SITE BORROW.

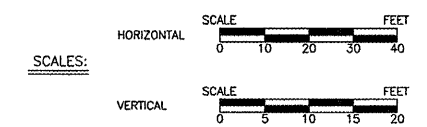
NOT TO SCALE:

PROPOSED TYPICAL SECTION - SAFETY AREA EARTHWORK

TAXIWAY P / TEMPORARY RUNWAY 10-28,
 STA. 627+65 TO STA. 639+00 & STA. 644+00 TO STA. 645+03



NOTE:
 ALL CROSS SECTION SLOPES SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER,
 AT THE TIME OF CONSTRUCTION. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.



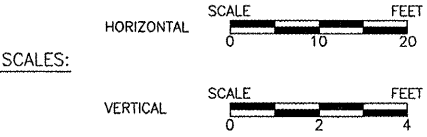
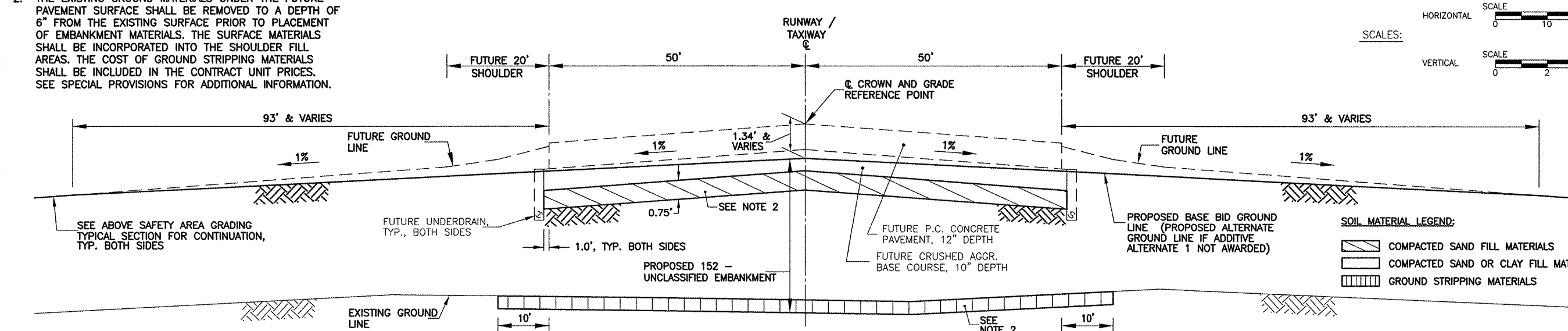
- NOTES:**
1. INSTALL COMPACTED SAND FILL MATERIALS UNDER (AND WITHIN 1.0' OF) THE FUTURE PAVEMENT FROM A POINT 1.83' BELOW THE FUTURE PAVEMENT SURFACE TO A POINT 2.58' BELOW THE FUTURE PAVEMENT SURFACE.
 2. THE EXISTING GROUND MATERIALS UNDER THE FUTURE PAVEMENT SURFACE SHALL BE REMOVED TO A DEPTH OF 6" FROM THE EXISTING SURFACE PRIOR TO PLACEMENT OF EMBANKMENT MATERIALS. THE SURFACE MATERIALS SHALL BE INCORPORATED INTO THE SHOULDER FILL AREAS. THE COST OF GROUND STRIPPING MATERIALS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICES. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

PROPOSED BASE BID TYPICAL SECTION - EARTHWORK

(USE ONLY IF ADDITIVE ALTERNATE 1 NOT AWARDED)

TAXIWAY P / TEMPORARY RUNWAY 10-28,
 STA. 635+75 TO STA. 639+09.86

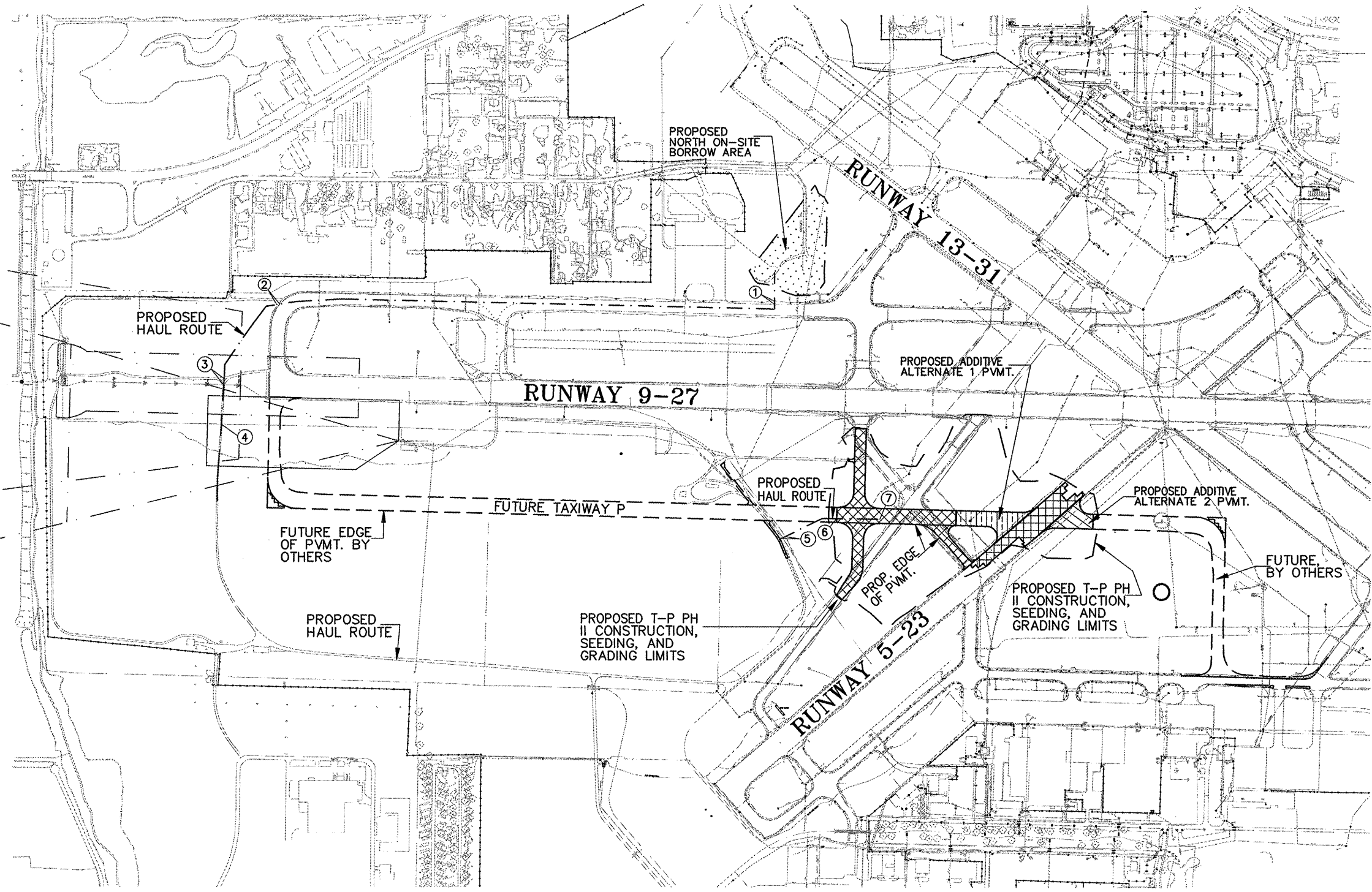
THIS TYPICAL SECTION IS TO BE USED ONLY
 IF ADDITIVE ALTERNATE 1 IS NOT AWARDED.



SOIL MATERIAL LEGEND:

- COMPACTED SAND FILL MATERIALS
- COMPACTED SAND OR CLAY FILL MATERIALS
- GROUND STRIPPING MATERIALS

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE II - MIDDLE PAVING
ILL. MLI-3854, QU008
SHEET 13 OF 82



PROPOSED MINIMUM UTILITY PROTECTION*		
LOCATION NUMBER	UTILITY DESCRIPTION	MINIMUM PROTECTION
①	EDGE LIGHT CABLE / EDGE OF PCC PVMT	STEEL PLATES
②	EDGE LIGHT CABLE / EDGE OF PCC PVMT	STEEL PLATES
③	MALSR CABLE / SERVICE ROAD	STEEL PLATES
④	LOCALIZER CONTROL CABLE	STEEL PLATES
⑤	SERVICE ROAD / R9 ILS CONTROL CABLE / SCAN CABLE	STEEL PLATES W/ 3" SOIL COVER
⑥	EDGE OF PROP. PCC PVMT	STEEL PLATES
⑦	EDGE OF PROP. PCC PVMT, IF ADD. ALTER. 1 NOT AWARDED	STEEL PLATES

* = MINIMUM THICKNESS OF ALL STEEL PLATES = 1" (ONE INCH), ALL STEEL PLATES TO HAVE A MINIMUM OF 3" SOIL COVER TO HOLD PLATES IN PLACE.

LEGEND:

- EDGE LIGHT
- GUIDANCE SIGN
- ELECTRICAL CABLE
- STORM SEWER OR UNDERDRAIN
- INLET
- OUTLET
- STORM MANHOLE
- SANITARY MANHOLE
- DRAINAGE DITCH
- CONDUIT OR DUCT BANK
- FUTURE PAVEMENT
- PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS
- PROPOSED HAUL ROUTE
- PROPOSED UTILITY PROTECTION LOCATION NUMBER.
- PROPOSED NORTH ON-SITE BORROW AREA.
- PROPOSED BASE BID PAVEMENT, SEE NOTE 3.
- PROPOSED ADDITIVE ALTERNATE 1 PAVEMENT, STA. 635+75 TO STA. 640+21.24 (WEST EDGE OF RNWY 5-23).
- PROPOSED ADDITIVE ALTERNATE 2 PAVEMENT, STA. 642+44 (EAST EDGE OF RNWY 5-23) TO STA. 645+03.



TEMPORARY BENCH MARKS		
B.M.	ELEV.	DESCRIPTION
TBM 200	587.10	HUB AT SE COR OF TXY H BORROW AREA, N1742722.2338, E2199623.7550, STATION 107+06.74, LT. 739.58
TBM K	578.34	TOP OF RCP, S SIDE OF SERVICE RD, N1741508.8290, E2201629.7690, STATION 127+42.16, RT. 424.08
TBM R	581.52	NE COR CONC BASE ELECT TRAN, S SIDE 9-27, BETWEEN SIGNS [2] - [7] N1741569.4966, E2200420.1701, STATION 115+31.40, RT. 393.12
TBM T	576.35	CHIS. □ SE COR CONC PAD W SIDE GS BLDG, N1741651.7212, E2198675.6748, STATION 97+85.40, RT. 354.03
TBM U	583.18	CHIS. □ HOLD SIGN BASE, NE COR, S SIDE R9, N1741775.92035, E2203365.21359, STATION 144+70.44, RT. 113.99

PROJECT CONTROL POINTS			
NUMBER	LOCATION	ELEV.	REMARK
5	STA. 108+94.45, RT. 339.57 N1741638.7660, E2199784.7412	580.05	
7	STA. 121+74.76, RT. 449.70 N1741496.8955, E2201061.9416	577.67	
8	STA. 127+73.19, RT. 413.69 N1741518.2450, E2201661.0745	579.45	
13	STA. 143+25.87, RT. 1163.46 N1740729.9858, E2203194.7470	574.40	1/2" REBAR
2000	STA. 107+69.21, LT. 736.44 N1742717.7720, E2199686.1370	587.72	SPK

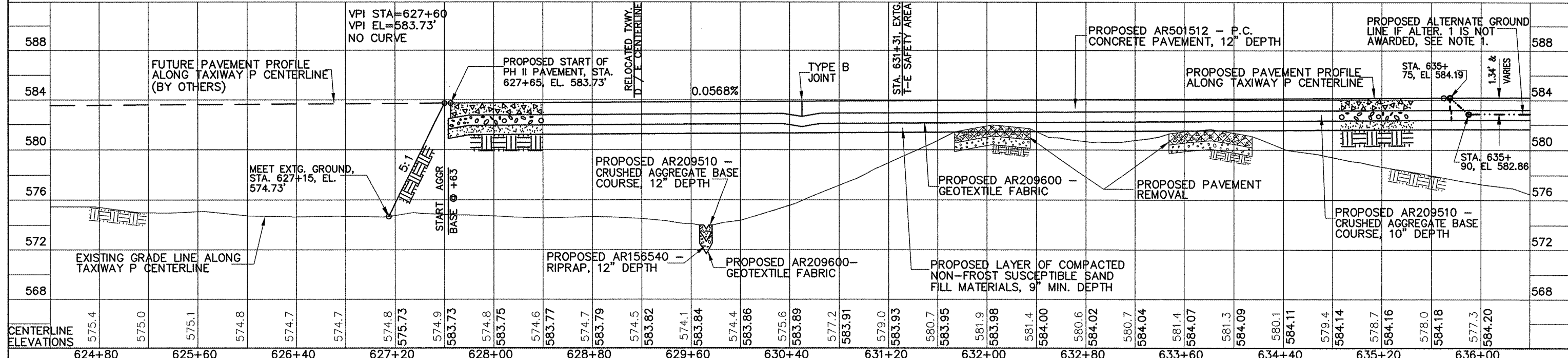
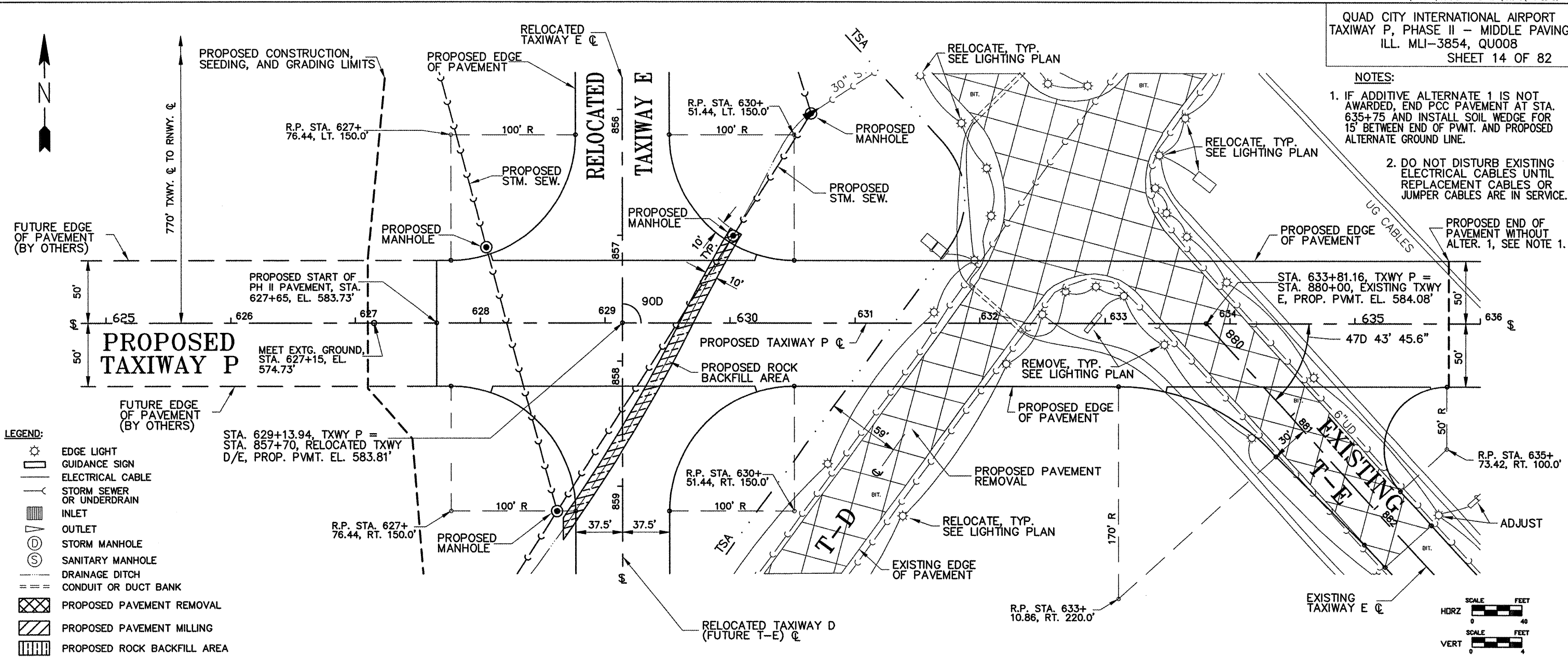
NOTES:

- SEE PLAN & PROFILE SHEETS FOR LOCATION OF EXISTING UNDERGROUND ELECTRICAL AND FIELD TILES.
- CONTRACTOR SHALL REPAIR OR REPLACE ANY IMPROVEMENTS DAMAGED DURING CONSTRUCTION AT HIS OWN EXPENSE.
- THE BASE BID PAVEMENT INCLUDES:
 - A. TAXIWAY P : STA. 627+65 TO STA. 635+75.
 - B. RUNWAY 5-23 : STA. 33+25 TO STA. 42+25.
 - C. RELOCATED TAXIWAY E : STA. 851+85 TO STA. 857+20.
 - D. RELOCATED TAXIWAY D : STA. 858+20 TO STA. 863+63.
 - E. EXISTING TAXIWAY E : STA. 880+67.6 TO STA. 884+32.

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QUAD CITY INTERNATIONAL AIRPORT
 TAXIWAY P, PHASE II - MIDDLE PAVING
 ILL. MLI-3854, QU008
 SHEET 14 OF 82

- NOTES:**
- IF ADDITIVE ALTERNATE 1 IS NOT AWARDED, END PCC PAVEMENT AT STA. 635+75 AND INSTALL SOIL WEDGE FOR 15' BETWEEN END OF PVMT. AND PROPOSED ALTERNATE GROUND LINE.
 - DO NOT DISTURB EXISTING ELECTRICAL CABLES UNTIL REPLACEMENT CABLES OR JUMPER CABLES ARE IN SERVICE.



TAXIWAY P PLAN & PROFILE, STA 625+00 TO STA 635+80

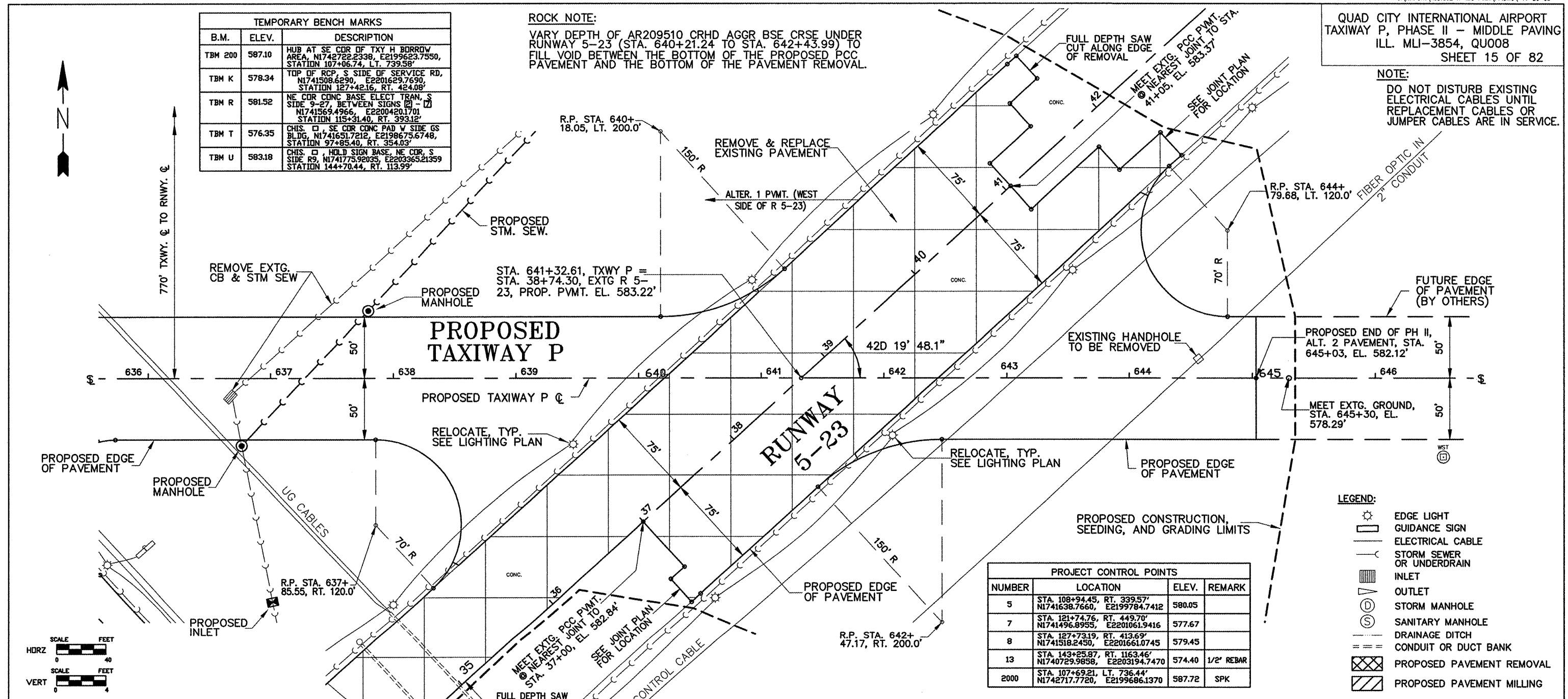
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QUAD CITY INTERNATIONAL AIRPORT
 TAXIWAY P, PHASE II - MIDDLE PAVING
 ILL. MLI-3854, QU008
 SHEET 15 OF 82

TEMPORARY BENCH MARKS		
B.M.	ELEV.	DESCRIPTION
TBM 200	587.10	HUB AT SE COR OF TXY H BDRROW AREA, N1742722.2338, E2199623.7550, STATION 107+06.74, LT. 739.58'
TBM K	578.34	TOP OF ROP, S SIDE OF SERVICE RD, N1741508.6290, E2201629.7690, STATION 127+42.16, RT. 424.08'
TBM R	581.52	NE COR CONC BASE ELECT TRAN, S SIDE 9-27, BETWEEN SIGNS (2) - (7) N1741569.4966, E2200420.1701, STATION 115+31.40, RT. 393.12'
TBM T	576.35	CHIS. □ SE COR CONC PAD W SIDE GS BLDG, N1741651.7212, E2198675.6748, STATION 97+83.40, RT. 354.03'
TBM U	583.18	CHIS. □ HLD B SIGN BASE, NE COR, S SIDE R9, N1741775.92035, E2203365.21359, STATION 144+70.44, RT. 113.99'

ROCK NOTE:
 VARY DEPTH OF AR209510 CRHD AGGR BSE CRSE UNDER RUNWAY 5-23 (STA. 640+21.24 TO STA. 642+43.99) TO FILL VOID BETWEEN THE BOTTOM OF THE PROPOSED PCC PAVEMENT AND THE BOTTOM OF THE PAVEMENT REMOVAL.

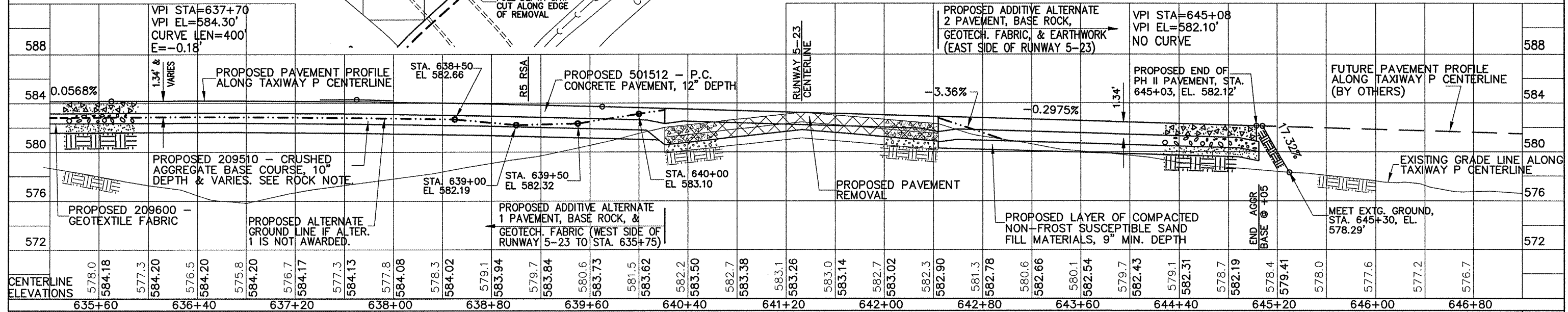
NOTE:
 DO NOT DISTURB EXISTING ELECTRICAL CABLES UNTIL REPLACEMENT CABLES OR JUMPER CABLES ARE IN SERVICE.



PROJECT CONTROL POINTS			
NUMBER	LOCATION	ELEV.	REMARK
5	STA. 108+94.45, RT. 339.57' N1741638.7660, E2199784.7412	580.05	
7	STA. 121+74.76, RT. 449.70' N1741496.8953, E2201061.9416	577.67	
8	STA. 127+73.19, RT. 413.69' N1741518.2450, E2201661.0745	579.45	
13	STA. 143+25.87, RT. 1163.46' N1740729.9858, E2203194.7470	574.40	1/2" REBAR
2000	STA. 107+69.21, LT. 736.44' N1742717.7720, E2199686.1370	587.72	SPK

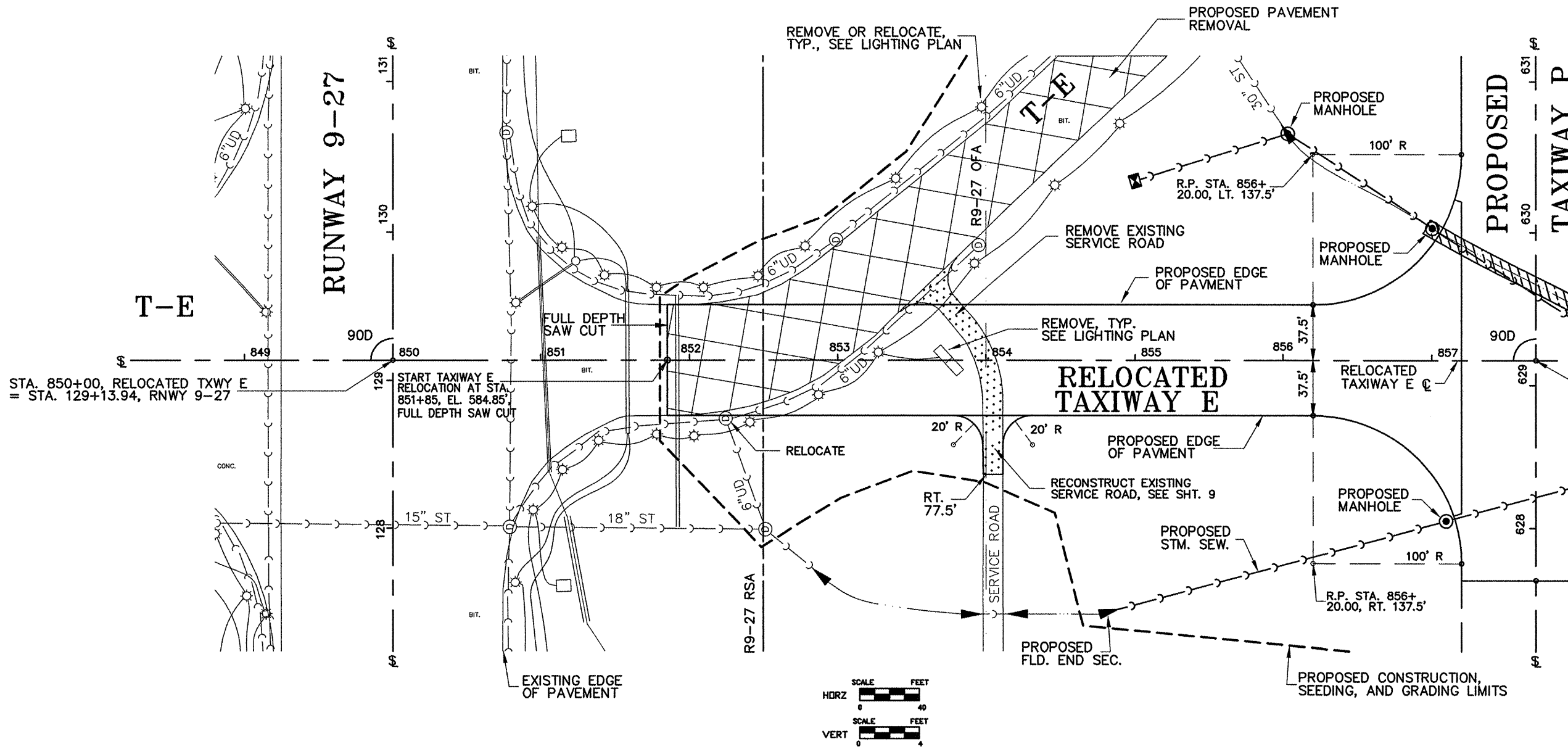
LEGEND:

- EDGE LIGHT
- GUIDANCE SIGN
- ELECTRICAL CABLE
- STORM SEWER OR UNDERDRAIN
- INLET
- OUTLET
- STORM MANHOLE
- SANITARY MANHOLE
- DRAINAGE DITCH
- CONDUIT OR DUCT BANK
- PROPOSED PAVEMENT REMOVAL
- PROPOSED PAVEMENT MILLING

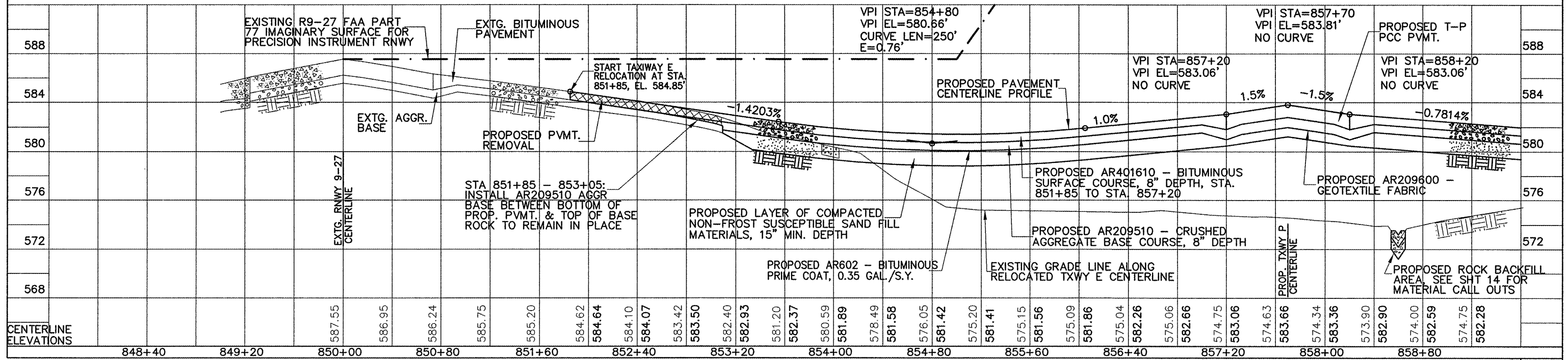
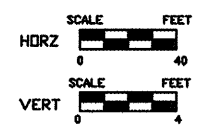


TAXIWAY P PLAN & PROFILE, STA 635+80 TO STA 646+60

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- LEGEND:**
- EDGE LIGHT
 - GUIDANCE SIGN
 - ELECTRICAL CABLE
 - STORM SEWER OR UNDERDRAIN INLET
 - STORM SEWER OR UNDERDRAIN OUTLET
 - STORM MANHOLE
 - SANITARY MANHOLE
 - DRAINAGE DITCH
 - CONDUIT OR DUCT BANK
 - PROPOSED PAVEMENT MILLING
 - PROPOSED PAVEMENT REMOVAL
 - PROPOSED ROCK BACKFILL AREA



RELOCATED TAXIWAY E PLAN & PROFILE, STA 848+80 TO STA 857+80

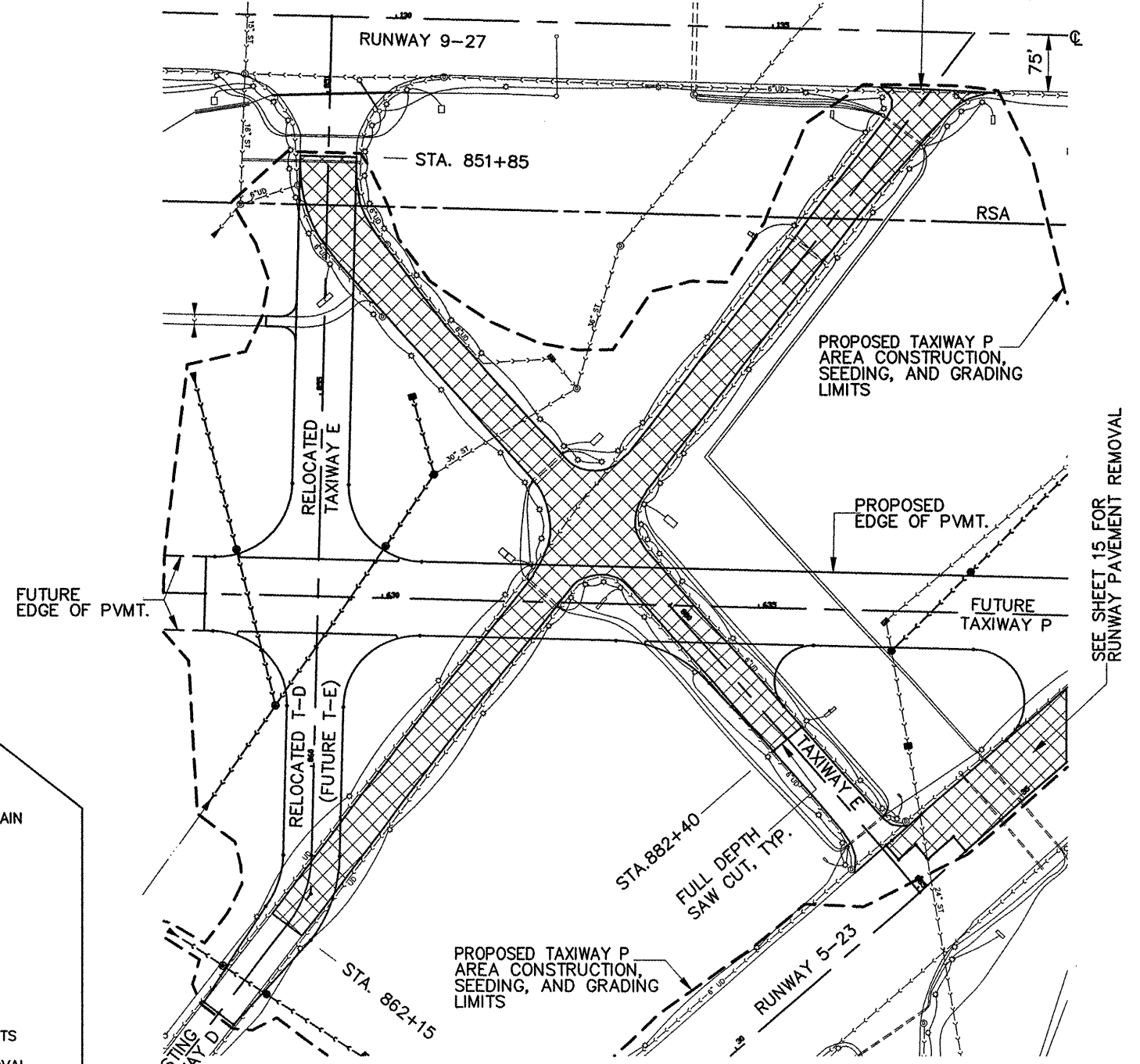
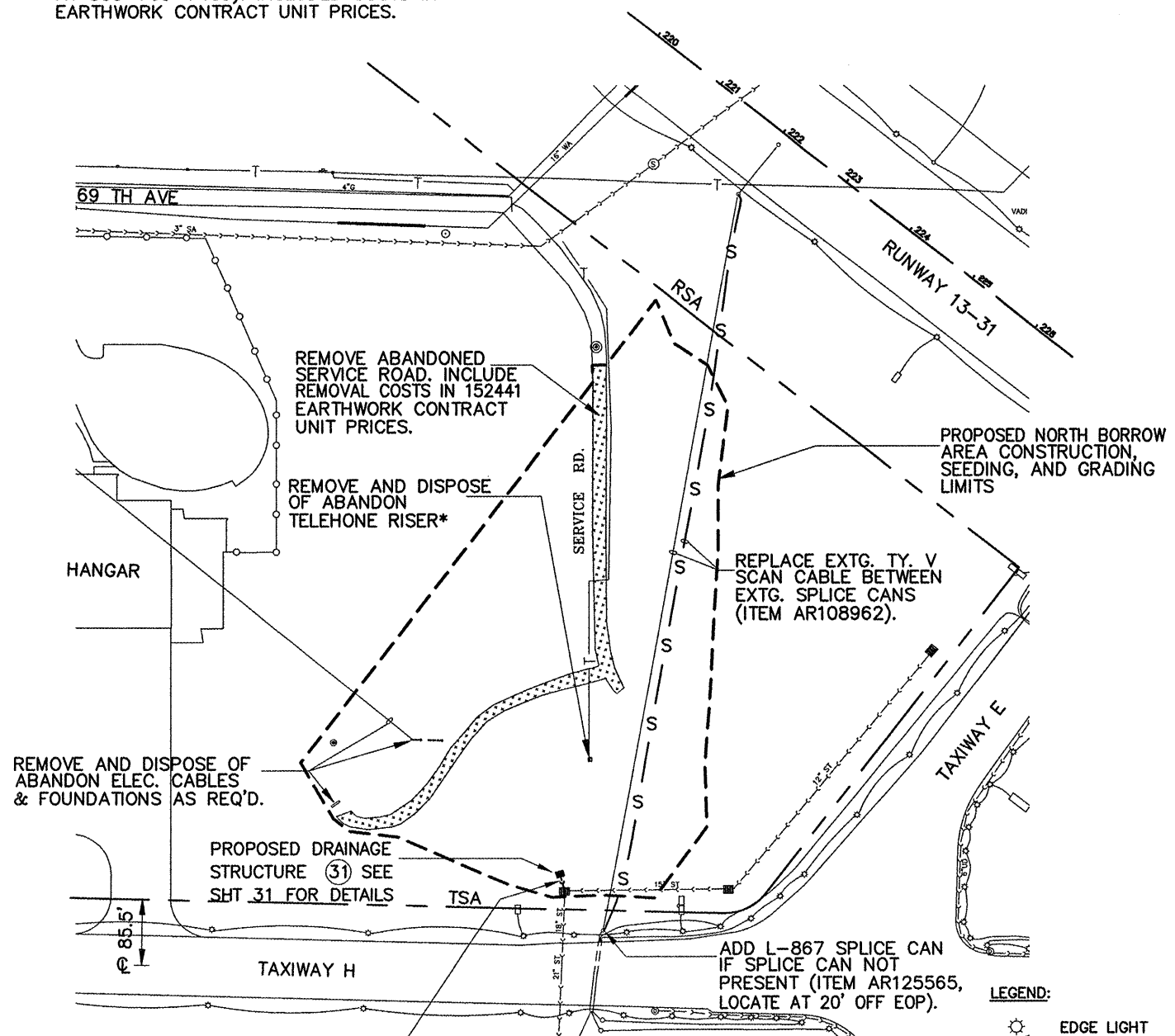
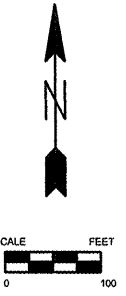
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NOTES:

1. SOILS HAULED FROM THE NORTH ON-SITE BORROW AREA TO THE TAXIWAY P CONSTRUCTION SITE AREA SHALL BE PAID FOR BY CUBIC YARDS REMOVED UNDER CONTRACT ITEM 152441, ON-SITE BORROW.
2. * = CONTRACTOR TO NOTIFY AND COORDINATE TELEPHONE RISER REMOVAL WITH AT&T (CALL DAVE GREEN AT 309-533-1473 OR RYAN BENZ AT 309-793-7405). INCLUDED COSTS IN EARTHWORK CONTRACT UNIT PRICES.

NOTES:

1. DO NOT DISTURB EXISTING ELECTRICAL CABLES UNTIL REPLACEMENT CABLES OR JUMPER CABLES ARE IN SERVICE.
2. IN PAVEMENT REMOVAL AREAS WITHOUT NEW PAVEMENT: REMOVE EXISTING PAVEMENT AND BASE ROCK TO A POINT THAT IS AT LEAST 8" BELOW THE FINAL GRADE. INSTALL EARTH SOILS IN VOID CREATED BY THE REMOVAL. INCLUDE COST IN REMOVAL / EARTHWORK CONTRACT UNIT RATES.
3. IN PAVEMENT REMOVAL AREAS WITH NEW PAVEMENT: IN PAVEMENT REMOVAL LOCATIONS WHERE THE CONDITION OF THE EXISTING AGGREGATE BASE COURSE IS ACCEPTABLE TO THE RESIDENT ENGINEER, THE CONTRACTOR SHALL LEAVE THE EXISTING AGGREGATE BASE COURSE IN PLACE AND INSTALL NEW AGGREGATE BASE COURSE MATERIAL BETWEEN THE TOP OF THE EXISTING AGGREGATE BASE COURSE AND THE BOTTOM OF THE PROPOSED PAVEMENT. THE PROPOSED GEOTEXTILE FABRIC SHALL BE DELETED IN LOCATIONS WHERE NEW AGGREGATE IS PLACED ON THE EXISTING AGGREGATE.



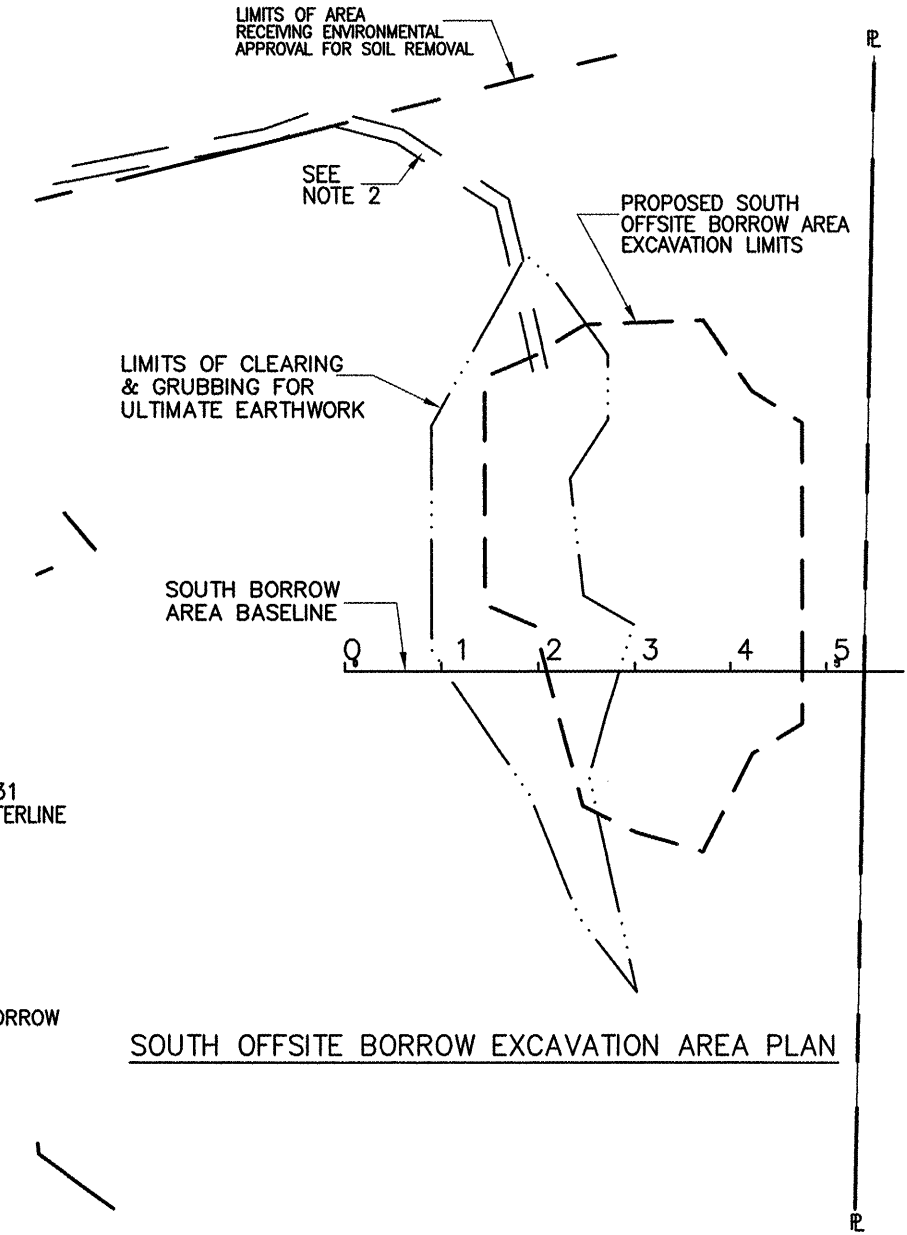
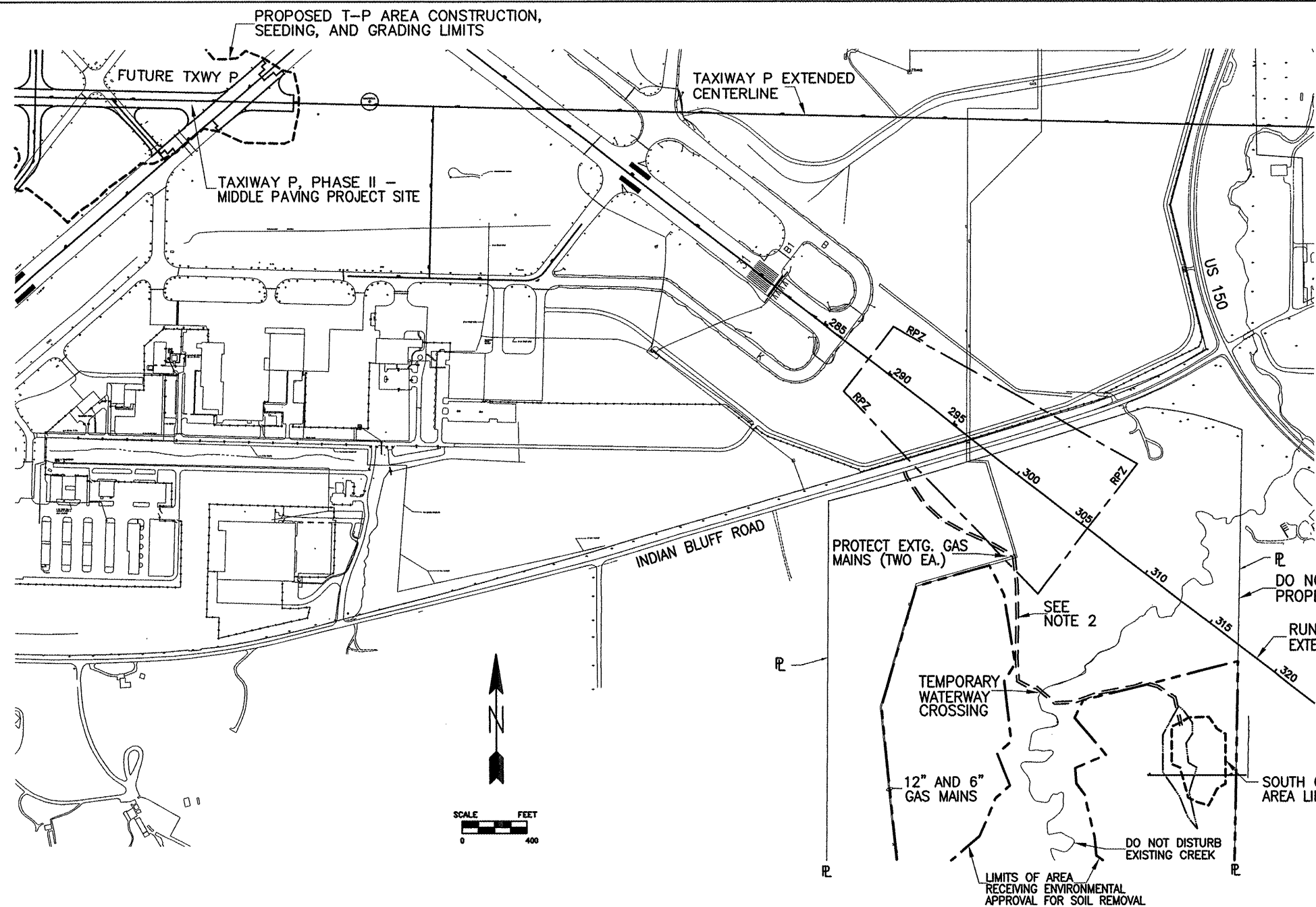
LEGEND:

	EDGE LIGHT
	GUIDANCE SIGN
	ELECTRICAL CABLE
	STORM SEWER OR UNDERDRAIN
	INLET
	OUTLET
	STORM MANHOLE
	SANITARY MANHOLE
	DRAINAGE DITCH
	CONDUIT OR DUCT BANK
	FUTURE PAVEMENT
	PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS
	PROPOSED PAVEMENT REMOVAL
	PROPOSED SERVICE ROAD REMOVAL

NORTH ON-SITE BORROW AREA PLAN

TAXIWAY REMOVAL PLAN

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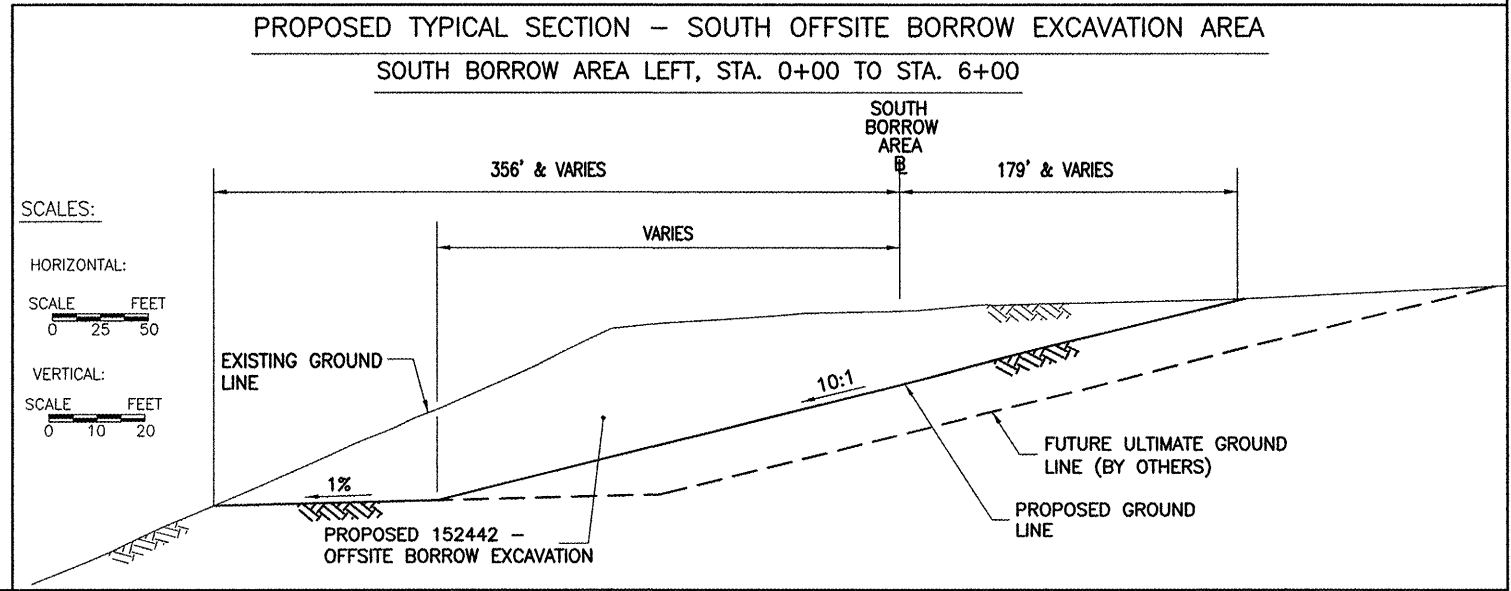


SOUTH OFFSITE BORROW EXCAVATION AREA LOCATION MAP

- LEGEND:**
- ☼ EDGE LIGHT
 - GUIDANCE SIGN
 - ELECTRICAL CABLE
 - STORM SEWER OR UNDERDRAIN
 - ▣ INLET
 - ▽ OUTLET
 - STORM MANHOLE
 - ⊙ SANITARY MANHOLE
 - DRAINAGE DITCH
 - CONDUIT OR DUCT BANK
 - FUTURE PAVEMENT
 - PROPOSED GRADING LIMITS
 - PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS

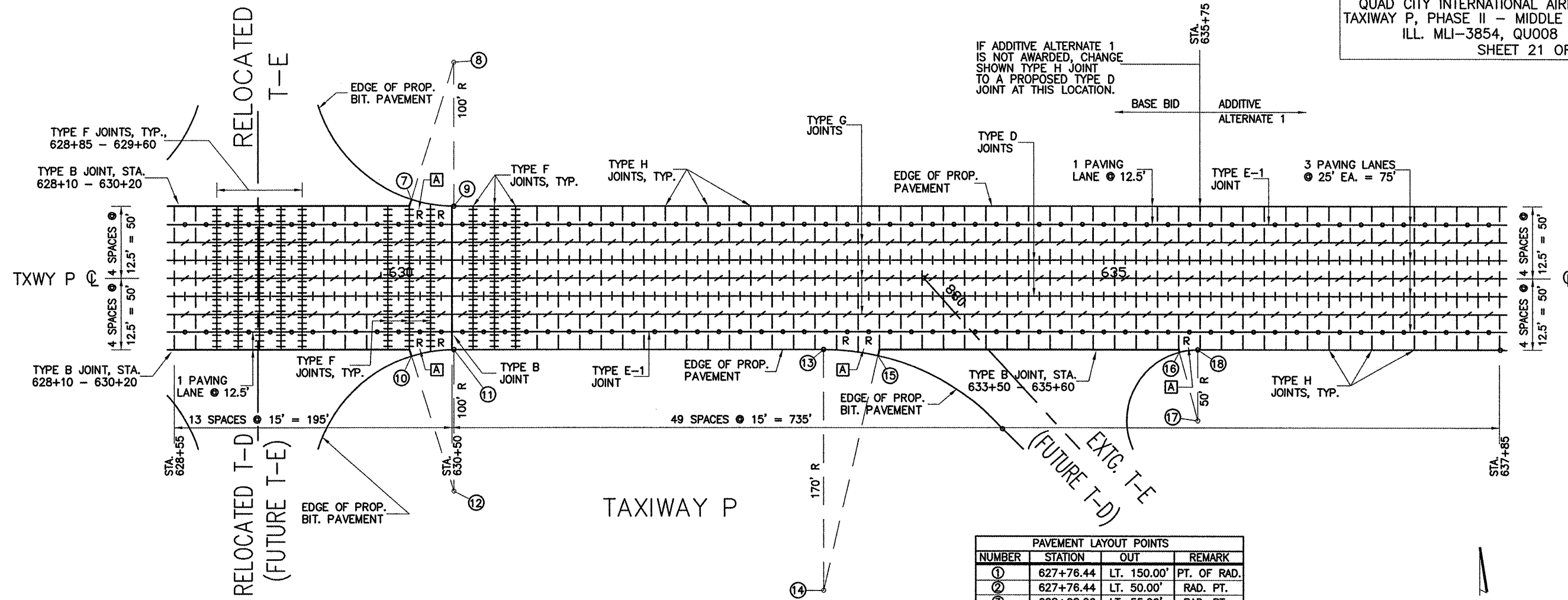
NOTES:

1. SOILS HAULED FROM THE SOUTH OFFSITE BORROW AREA TO THE TAXIWAY P CONSTRUCTION SITE AREA SHALL BE PAID FOR BY CUBIC YARDS REMOVED UNDER CONTRACT ITEM 152442, OFFSITE BORROW EXCAVATION .
2. PROPOSED CONTRACTOR CONSTRUCTED TEMPORARY HAUL ROAD. SUPPLY & INSTALL TEMPORARY CULVERTS AS REQUIRED. EXACT LOCATION TO BE DETERMINE IN THE FIELD AT THE TIME OF CONSTRUCTION. LOCATION OF HAUL ROAD SUBJECT TO THE APPROVAL OF THE RESIDENT ENGINEER AND THE MAA. REMOVE HAUL ROAD AFTER BORROW ACTIVITIES ARE DONE.

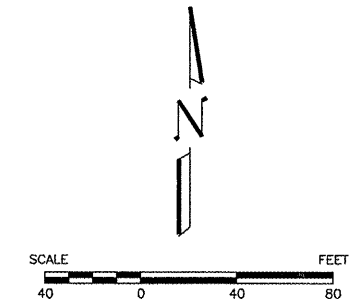


PROPOSED SOUTH OFFSITE BORROW EXCAVATION AREA PLAN

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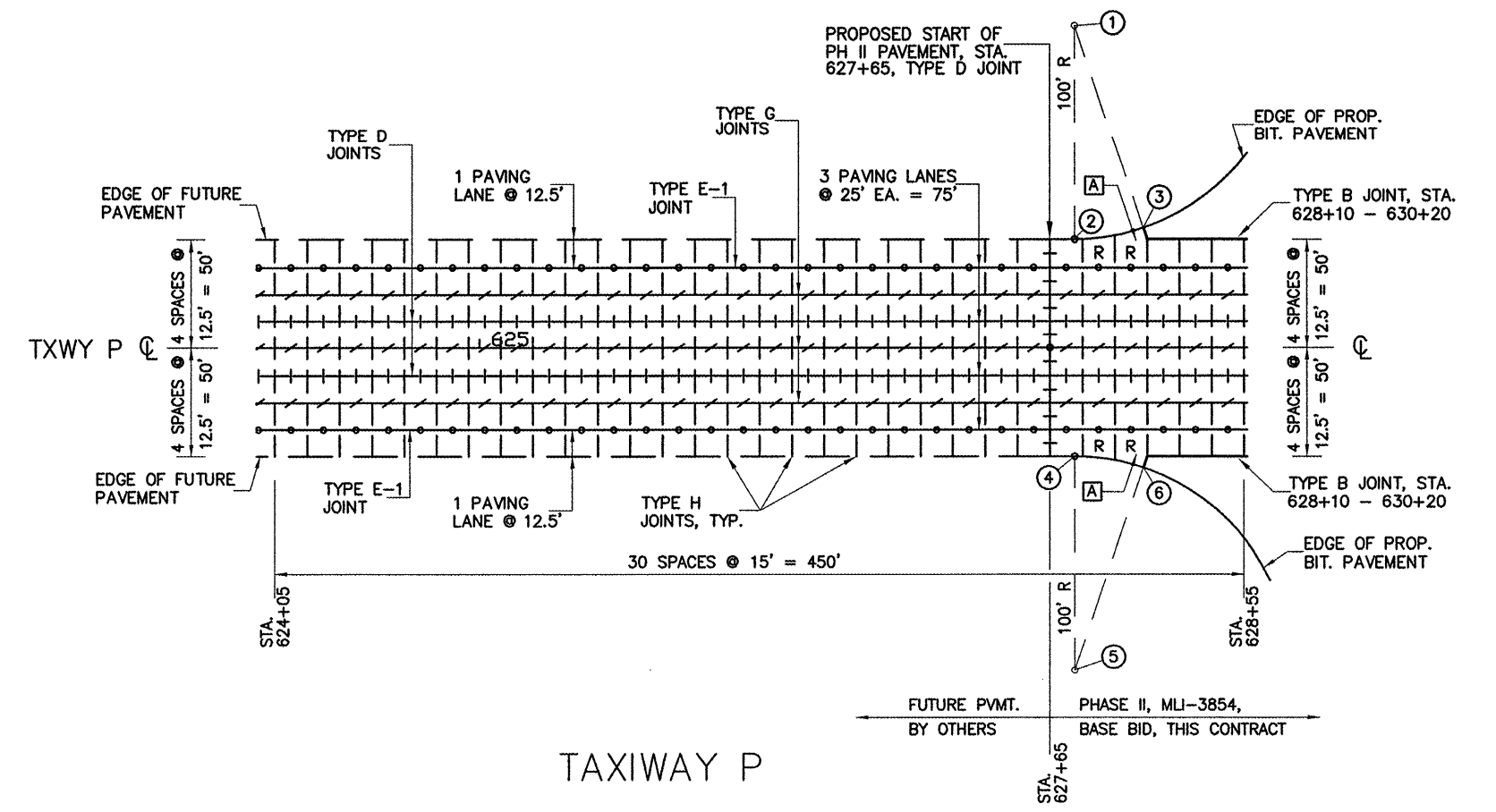
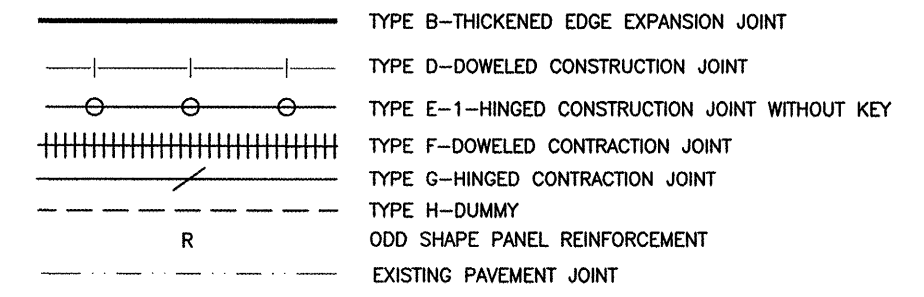


PAVEMENT LAYOUT POINTS			
NUMBER	STATION	OUT	REMARK
①	627+76.44	LT. 150.00'	PT. OF RAD.
②	627+76.44	LT. 50.00'	RAD. PT.
③	628+08.26	LT. 55.20'	RAD. PT.
④	627+76.44	RT. 50.00'	RAD. PT.
⑤	627+76.44	RT. 150.00'	PT. OF RAD.
⑥	628+08.26	RT. 55.20'	RAD. PT.
⑦	630+21.45	LT. 54.60'	RAD. PT.
⑧	630+51.44	LT. 150.00'	PT. OF RAD.
⑨	630+51.44	LT. 50.00'	RAD. PT.
⑩	630+21.45	RT. 54.60'	RAD. PT.
⑪	630+51.44	RT. 50.00'	RAD. PT.
⑫	630+51.44	RT. 150.00'	PT. OF RAD.
⑬	633+10.86	RT. 50.00'	RAD. PT.
⑭	633+10.86	RT. 220.00'	PT. OF RAD.
⑮	633+49.00	RT. 54.33'	RAD. PT.
⑯	635+60.46	RT. 51.71'	RAD. PT.
⑰	635+73.42	RT. 100.00'	PT. OF RAD.
⑱	635+73.42	RT. 50.00'	RAD. PT.



- NOTES:
- SEE SHEET 22 FOR JOINT NOTES.
 - SEE SHEET 23 FOR JOINT DETAILS.
 - [A] = STUB, SEE FILLET STUB DETAIL.

JOINT SYMBOL LEGEND



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JOINT SYMBOL LEGEND

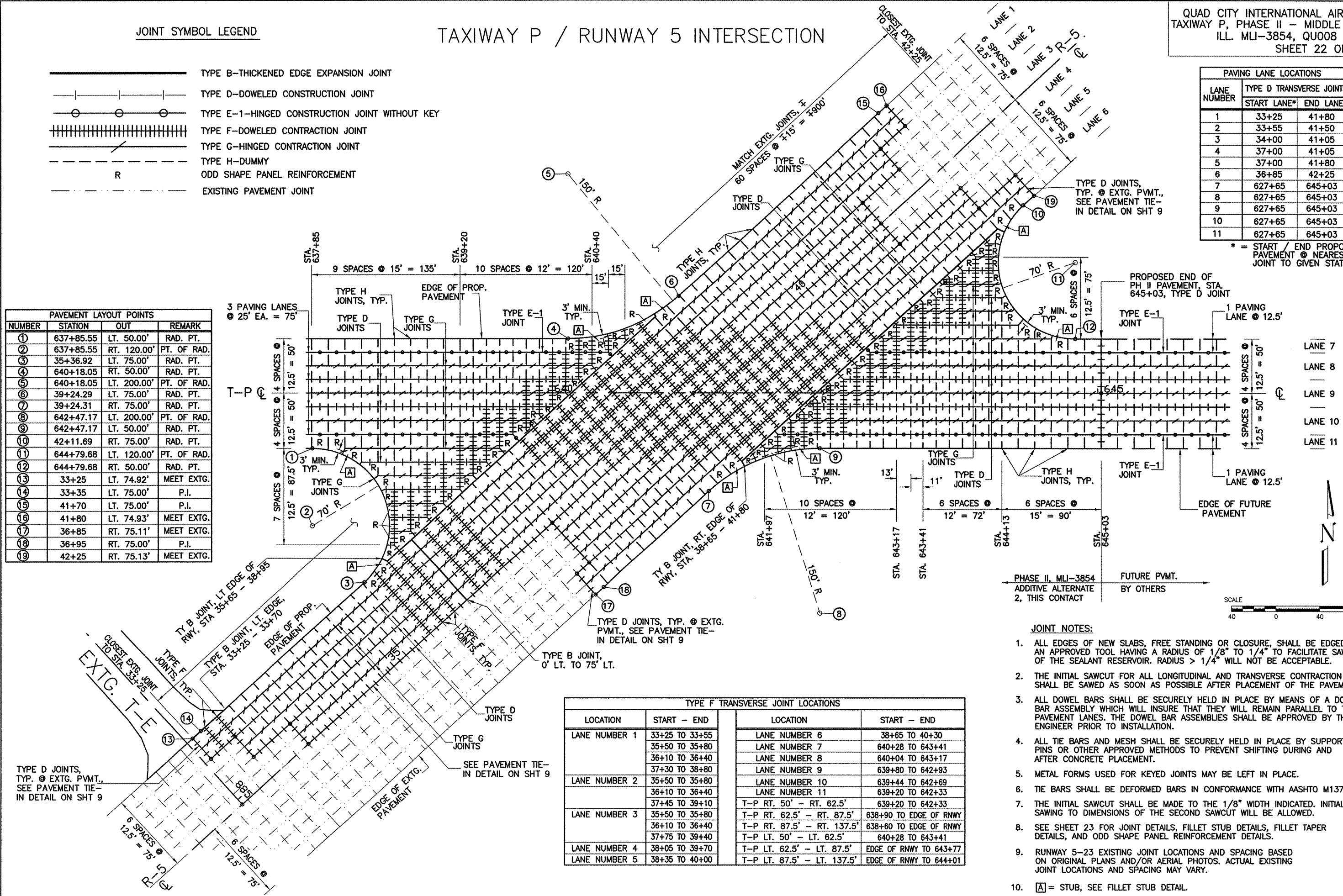
- TYPE B-THICKENED EDGE EXPANSION JOINT
- TYPE D-DOWELED CONSTRUCTION JOINT
- TYPE E-1-HINGED CONSTRUCTION JOINT WITHOUT KEY
- TYPE F-DOWELED CONTRACTION JOINT
- TYPE G-HINGED CONTRACTION JOINT
- TYPE H-DUMMY
- ODD SHAPE PANEL REINFORCEMENT
- EXISTING PAVEMENT JOINT

TAXIWAY P / RUNWAY 5 INTERSECTION

PAVING LANE LOCATIONS		
LANE NUMBER	TYPE D TRANSVERSE JOINTS START LANE*	END LANE*
1	33+25	41+80
2	33+55	41+50
3	34+00	41+05
4	37+00	41+05
5	37+00	41+80
6	36+85	42+25
7	627+65	645+03
8	627+65	645+03
9	627+65	645+03
10	627+65	645+03
11	627+65	645+03

* = START / END PROPOSED PAVEMENT @ NEAREST EXTG. JOINT TO GIVEN STATION.

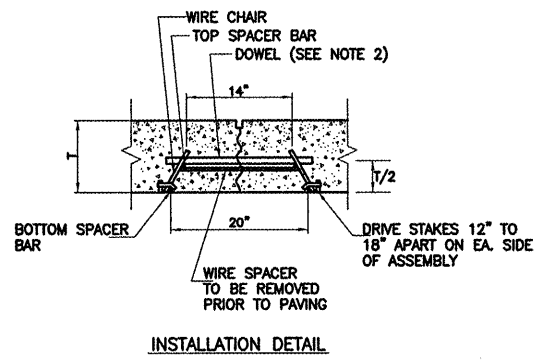
PAVEMENT LAYOUT POINTS			
NUMBER	STATION	OUT	REMARK
1	637+85.55	LT. 50.00'	RAD. PT.
2	637+85.55	RT. 120.00'	PT. OF RAD.
3	35+36.92	LT. 75.00'	RAD. PT.
4	640+18.05	RT. 50.00'	RAD. PT.
5	640+18.05	LT. 200.00'	PT. OF RAD.
6	39+24.29	LT. 75.00'	RAD. PT.
7	39+24.31	RT. 75.00'	RAD. PT.
8	642+47.17	LT. 200.00'	PT. OF RAD.
9	642+47.17	LT. 50.00'	RAD. PT.
10	42+11.69	RT. 75.00'	RAD. PT.
11	644+79.68	LT. 120.00'	PT. OF RAD.
12	644+79.68	RT. 50.00'	RAD. PT.
13	33+25	LT. 74.92'	MEET EXTG.
14	33+35	LT. 75.00'	P.I.
15	41+70	LT. 75.00'	P.I.
16	41+80	LT. 74.93'	MEET EXTG.
17	36+85	RT. 75.11'	MEET EXTG.
18	36+95	RT. 75.00'	P.I.
19	42+25	RT. 75.13'	MEET EXTG.



TYPE F TRANSVERSE JOINT LOCATIONS				
LOCATION	START - END	LOCATION	START - END	
LANE NUMBER 1	33+25 TO 33+55	LANE NUMBER 6	38+65 TO 40+30	
	35+50 TO 35+80		LANE NUMBER 7	640+28 TO 643+41
	36+10 TO 36+40		LANE NUMBER 8	640+04 TO 643+17
LANE NUMBER 2	35+50 TO 35+80	LANE NUMBER 9	639+80 TO 642+93	
	36+10 TO 36+40	LANE NUMBER 10	639+44 TO 642+69	
	37+45 TO 39+10	LANE NUMBER 11	639+20 TO 642+33	
LANE NUMBER 3	35+50 TO 35+80	T-P RT. 50' - RT. 62.5'	639+20 TO 642+33	
	36+10 TO 36+40	T-P RT. 62.5' - RT. 87.5'	638+90 TO EDGE OF RNNY	
	37+75 TO 39+40	T-P RT. 87.5' - RT. 137.5'	638+60 TO EDGE OF RNNY	
LANE NUMBER 4	38+05 TO 39+70	T-P LT. 50' - LT. 62.5'	640+28 TO 643+41	
	38+35 TO 40+00	T-P LT. 62.5' - LT. 87.5'	EDGE OF RNNY TO 643+77	
LANE NUMBER 5		T-P LT. 87.5' - LT. 137.5'	EDGE OF RNNY TO 644+01	

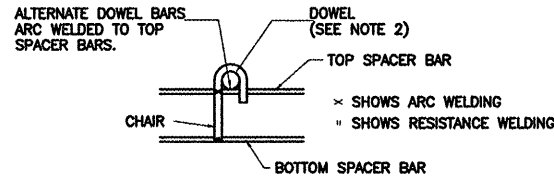
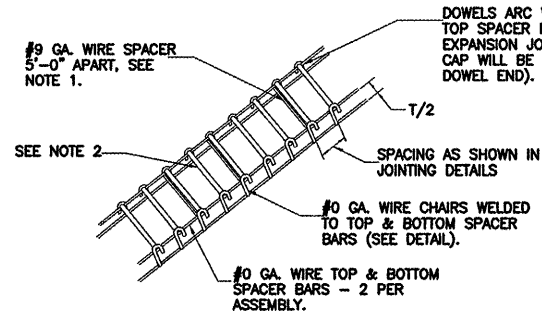
- JOINT NOTES:**
- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
 - THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
 - ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL INSURE THAT THEY WILL REMAIN PARALLEL TO THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.
 - ALL TIE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
 - METAL FORMS USED FOR KEYED JOINTS MAY BE LEFT IN PLACE.
 - TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH AASHTO M137.
 - THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO DIMENSIONS OF THE SECOND SAWCUT WILL BE ALLOWED.
 - SEE SHEET 23 FOR JOINT DETAILS, FILLET STUB DETAILS, FILLET TAPER DETAILS, AND ODD SHAPE PANEL REINFORCEMENT DETAILS.
 - RUNWAY 5-23 EXISTING JOINT LOCATIONS AND SPACING BASED ON ORIGINAL PLANS AND/OR AERIAL PHOTOS. ACTUAL EXISTING JOINT LOCATIONS AND SPACING MAY VARY.
 - [A] = STUB, SEE FILLET STUB DETAIL.

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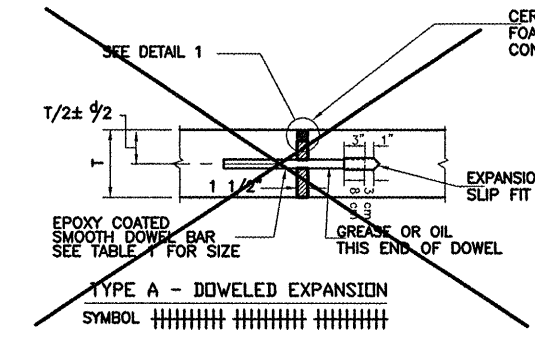


BASKET ASSEMBLY NOTES:

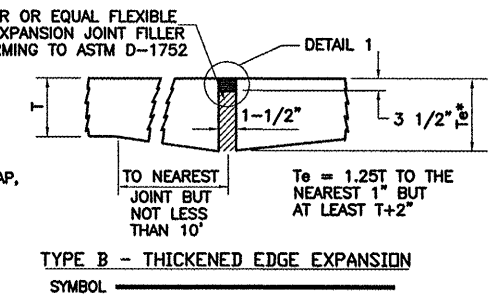
- 1.) #9 GA. WIRE SPACER BAR ARC WELDED TO THE BOTTOM OF TOP SPACER BAR. (MAY BE MECHANICALLY ATTACHED IN LIEU OF WELDING) 3 REQUIRED PER UNIT. THIS WIRE MUST BE CUT OR REMOVED PRIOR TO PAVING.
- 2.) DOWELS SHALL BE EPOXY COATED IN ACCORDANCE TO THE REQUIREMENTS OF AASHTO M254. IMMEDIATELY PRIOR TO PAVING, THE FREE END OF EACH DOWEL SHALL BE GREASED OR OILED, FOR HALF THE LENGTH OF THE DOWEL.
- 3.) ADJUST HEIGHT OF THE BASKET ASSEMBLIES (IN THE FIELD AT THE TIME OF CONSTRUCTION) SO THAT THE DOWELS ARE IN THE MIDDLE OF THE PROPOSED CONCRETE SLAB.



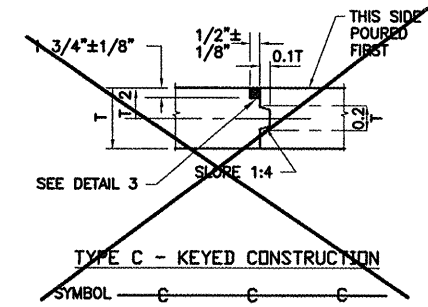
DOWEL BASKET ASSEMBLY DETAILS AND NOTES



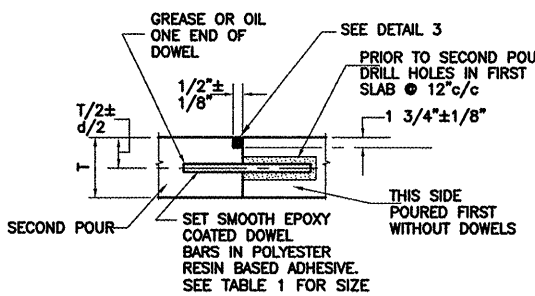
TYPE A - DOWELED EXPANSION
SYMBOL [Symbol]



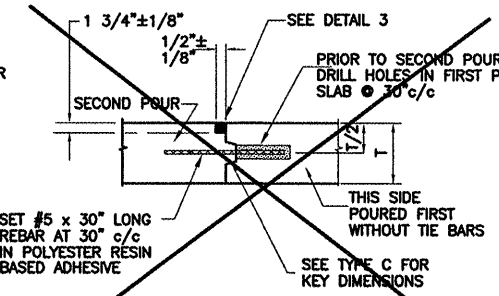
TYPE B - THICKENED EDGE EXPANSION
SYMBOL [Symbol]



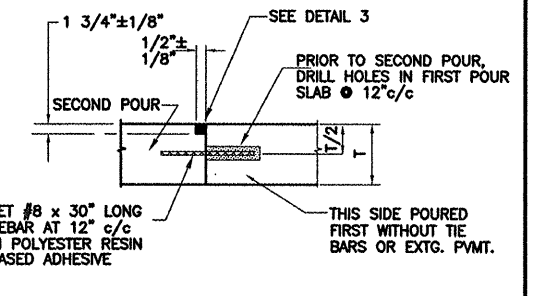
TYPE C - KEYED CONSTRUCTION
SYMBOL [Symbol]



TYPE D - DOWELED CONSTRUCTION
SYMBOL [Symbol]

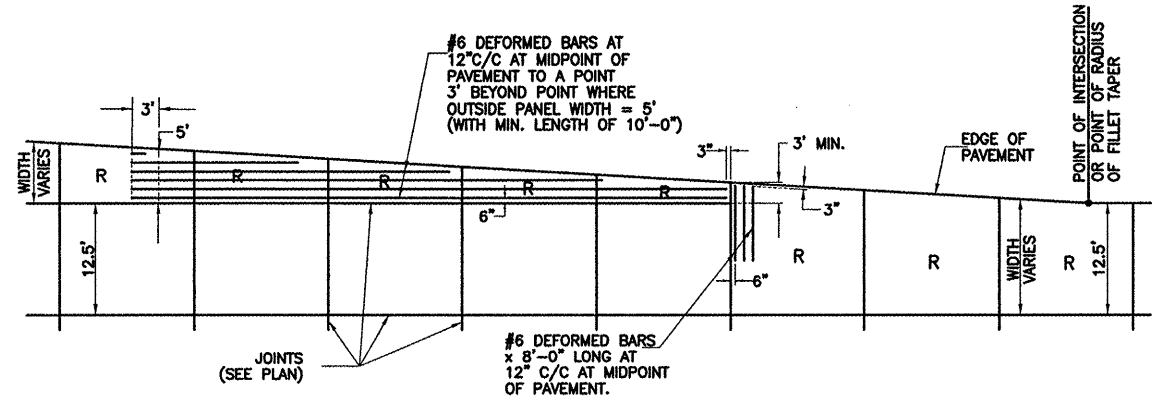


TYPE E - HINGED WITH KEY
SYMBOL [Symbol]

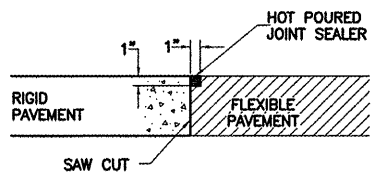


TYPE E-1 - HINGED WITHOUT KEY
SYMBOL [Symbol]

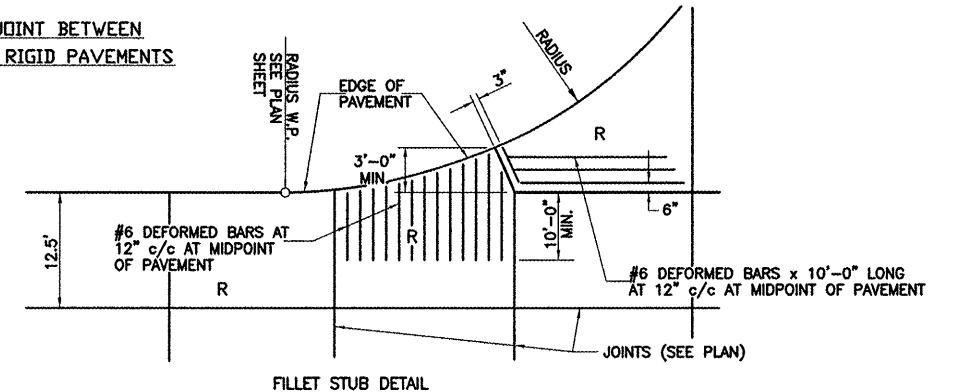
TYPE E-1 JOINT NOTE: THE TYPE E-1 JOINT SHALL BE A CONSTRUCTION BUTT JOINT WITH TIE BARS. THE TIE BARS WILL BE USED TO TRANSFER THE LOAD ACROSS THE JOINT.



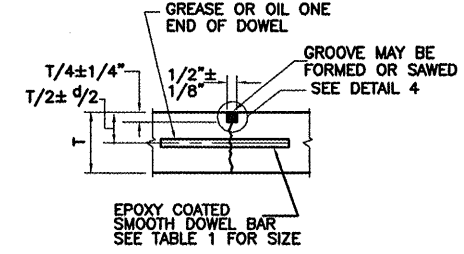
FILLET TAPER DETAIL



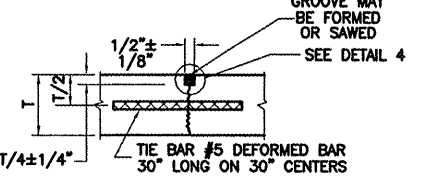
DETAIL - JOINT BETWEEN FLEXIBLE AND RIGID PAVEMENTS



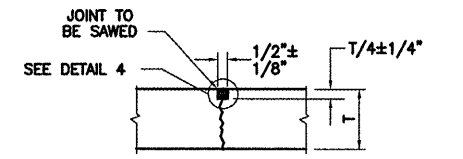
FILLET STUB DETAIL



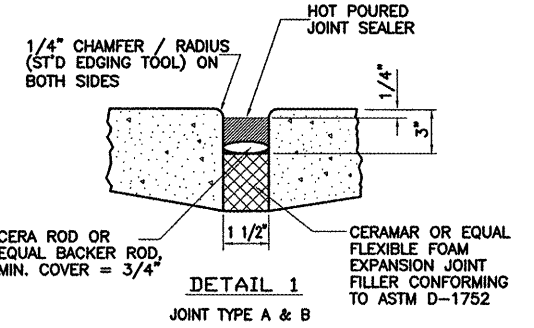
TYPE F - DOWELED CONTRACTION
SYMBOL [Symbol]



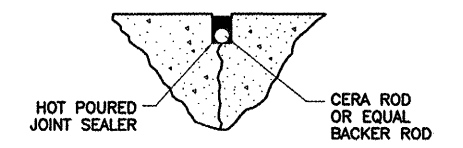
TYPE G - HINGED CONTRACTION
SYMBOL [Symbol]



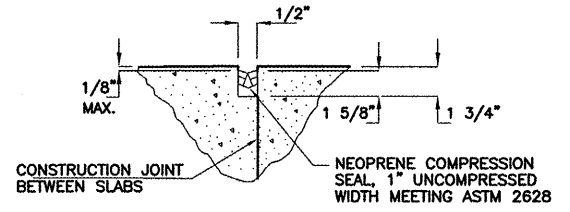
TYPE H - DUMMY CONTRACTION
SYMBOL [Symbol]



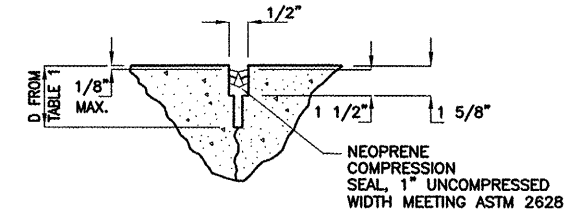
DETAIL 1
JOINT TYPE A & B



DETAIL 2



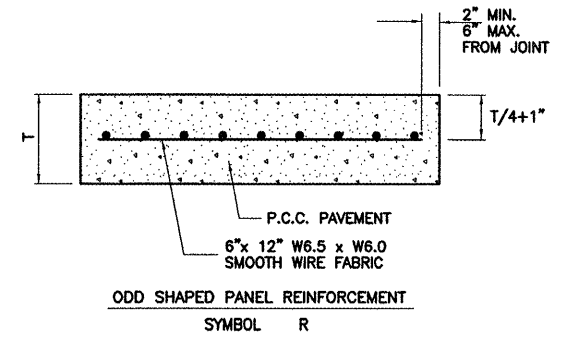
DETAIL 3
JOINT TYPES C, D, AND E



DETAIL 4
JOINT TYPES F, G, AND H

TABLE 1

PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT D, INCHES D=(T/4)±1/4"	STEEL DOWELS		
		DIAMETER (INCHES)	LENGTH (INCHES)	SPACING (INCHES)
8	2.00	1.00	19	12
9	2.25	1.00	19	12
10	2.50	1.00	19	12
12	3.00	1.00	19	12
15	3.75	1.25	20	15
17	4.25	1.50	20	18



ODD SHAPED PANEL REINFORCEMENT
SYMBOL R

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE II - MIDDLE PAVING
ILL. MLI-3854, QU008
SHEET 24 OF 82



SEE SHEET 26 FOR T-E
PROPOSED ELEVATIONS

EDGE OF
PROP. PVMT.

RELOCATED
T-E

T-E

STA. 630+00

STA. 631+00

STA. 632+00

STA. 633+00

STA. 634+00

STA. 635+00

STA. 636+00

STA. 637+00

SHOULDER EMBANKMENT
AREA, SEE NOTES 2 & 3.

LT. 50'
LT. 37.5'
LT. 12.5'
@ T-P
RT. 12.5'
@ CROWN
RT. 37.5'
RT. 50'

EDGE OF
PROPOSED PVMT.
LT. 50'
LT. 37.5'
LT. 12.5'
@ T-P
RT. 12.5'
@ CROWN
RT. 37.5'
RT. 50'

TXWY P

TAXIWAY P

SEE SHEET 26 FOR T-D
PROPOSED ELEVATIONS

EDGE OF
PROP. PVMT.

RELOCATED
T-D

T-D

EDGE OF
PROP. PVMT.

SHOULDER EMBANKMENT
AREA, SEE NOTES 2 & 3.

170' R
1/4
1/2
3/4
STA. 881+00

STA. 881+00

STA. 882+00

STA. 883+00

STA. 884+00

STA. 885+00

STA. 886+00

STA. 887+00

EXTG. T-E
(FUTURE T-D)

EDGE OF
PROP. PVMT.

EDGE OF
EXTG. PVMT.

PROPOSED CONSTRUCTION, SEEDING,
AND GRADING LIMITS.

EXTG. R 5-23

@ R 5-23

EDGE OF
FUTURE PVMT.
LT. 50'
LT. 37.5'
LT. 12.5'
@ T-P
RT. 12.5'
@ CROWN
RT. 37.5'
RT. 50'

EDGE OF
PROPOSED PVMT.

LT. 50'
LT. 37.5'
LT. 12.5'
@ T-P
RT. 12.5'
@ CROWN
RT. 37.5'
RT. 50'

TXWY P

TAXIWAY P

76.80
GRADE REFERENCE POINT

SHOULDER EMBANKMENT
AREA, SEE NOTES 2 & 3.

PROPOSED CONSTRUCTION, SEEDING,
AND GRADING LIMITS.

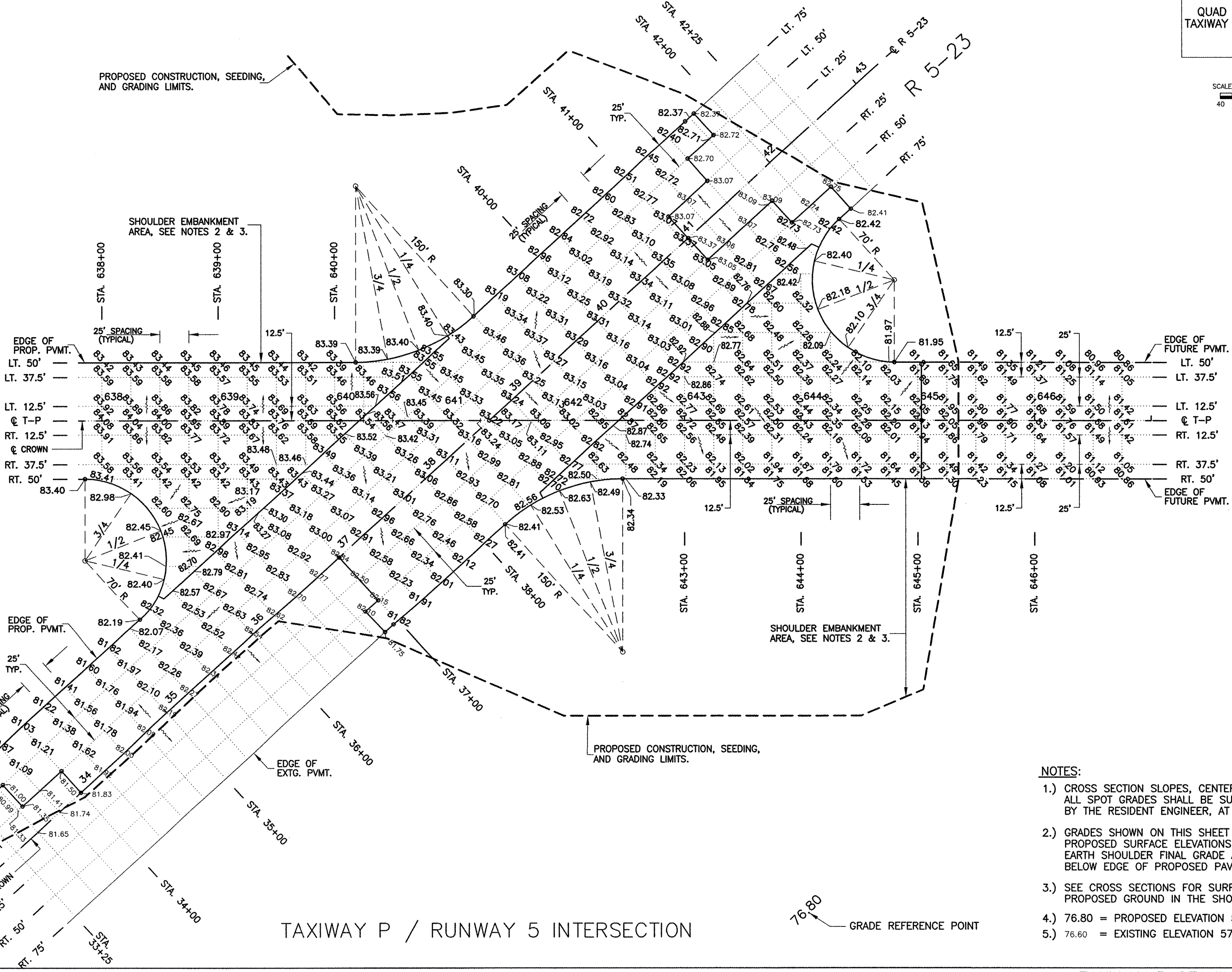
NOTES:

- 1.) CROSS SECTION SLOPES, CENTERLINE PROFILE GRADES, AND ALL SPOT GRADES SHALL BE SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION.
- 2.) GRADES SHOWN ON THIS SHEET AT EDGE OF PAVEMENT ARE PROPOSED SURFACE ELEVATIONS OF PAVEMENT. PROPOSED EARTH SHOULDER FINAL GRADE AT EDGE OF PVMT. IS 0.125' BELOW EDGE OF PROPOSED PAVEMENT GRADE.
- 3.) SEE CROSS SECTIONS FOR SURFACE ELEVATIONS OF THE PROPOSED GROUND IN THE SHOULDER AREA.
- 4.) 76.80 = PROPOSED ELEVATION 576.80.
- 5.) 76.60 = EXISTING ELEVATION 576.60.



PROPOSED CONSTRUCTION, SEEDING,
AND GRADING LIMITS.

SHOULDER EMBANKMENT
AREA, SEE NOTES 2 & 3.



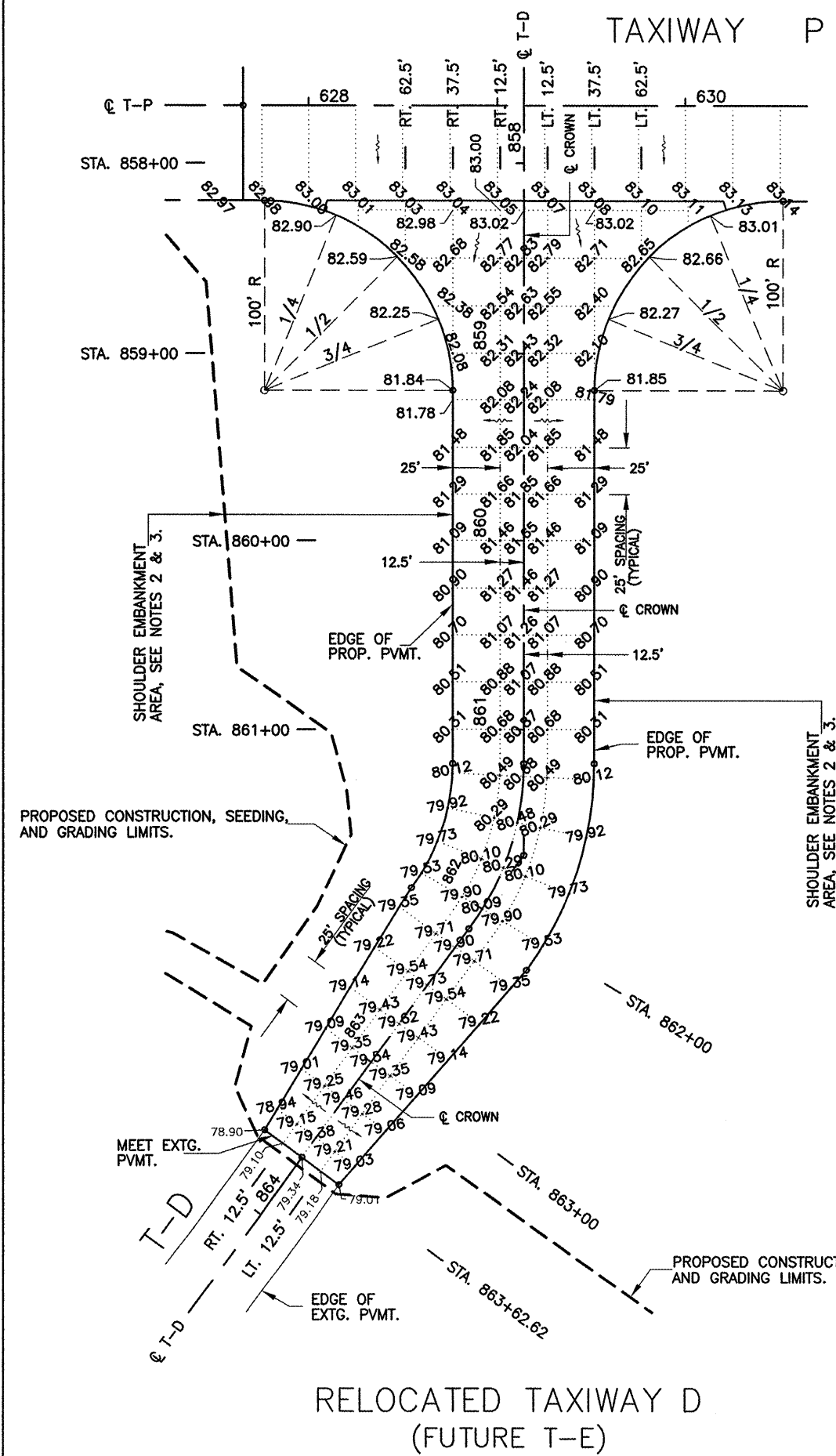
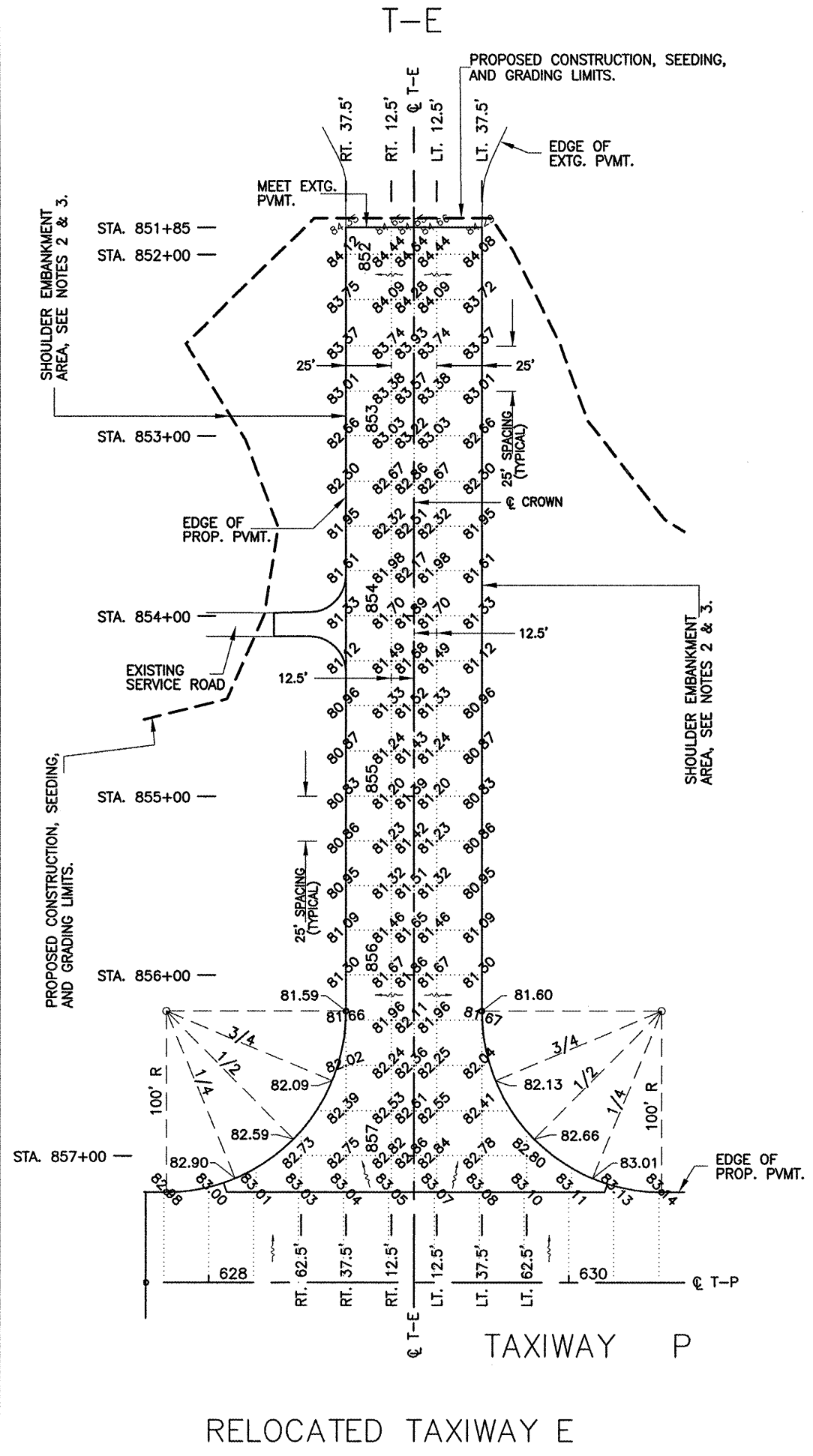
TAXIWAY P / RUNWAY 5 INTERSECTION

76.80 GRADE REFERENCE POINT

NOTES:

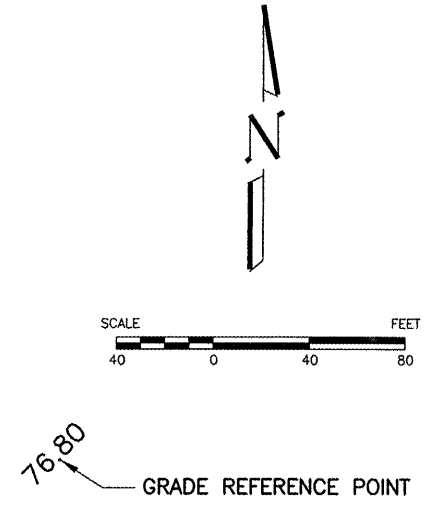
- 1.) CROSS SECTION SLOPES, CENTERLINE PROFILE GRADES, AND ALL SPOT GRADES SHALL BE SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION.
- 2.) GRADES SHOWN ON THIS SHEET AT EDGE OF PAVEMENT ARE PROPOSED SURFACE ELEVATIONS OF PAVEMENT. PROPOSED EARTH SHOULDER FINAL GRADE AT EDGE OF PVMT. IS 0.125' BELOW EDGE OF PROPOSED PAVEMENT GRADE.
- 3.) SEE CROSS SECTIONS FOR SURFACE ELEVATIONS OF THE PROPOSED GROUND IN THE SHOULDER AREA.
- 4.) 76.80 = PROPOSED ELEVATION 576.80.
- 5.) 76.60 = EXISTING ELEVATION 576.60.

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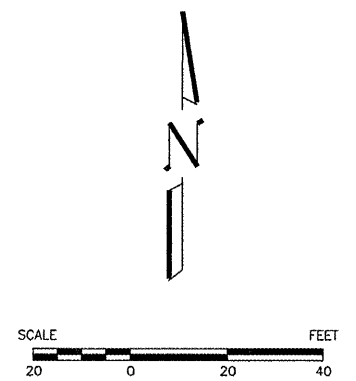
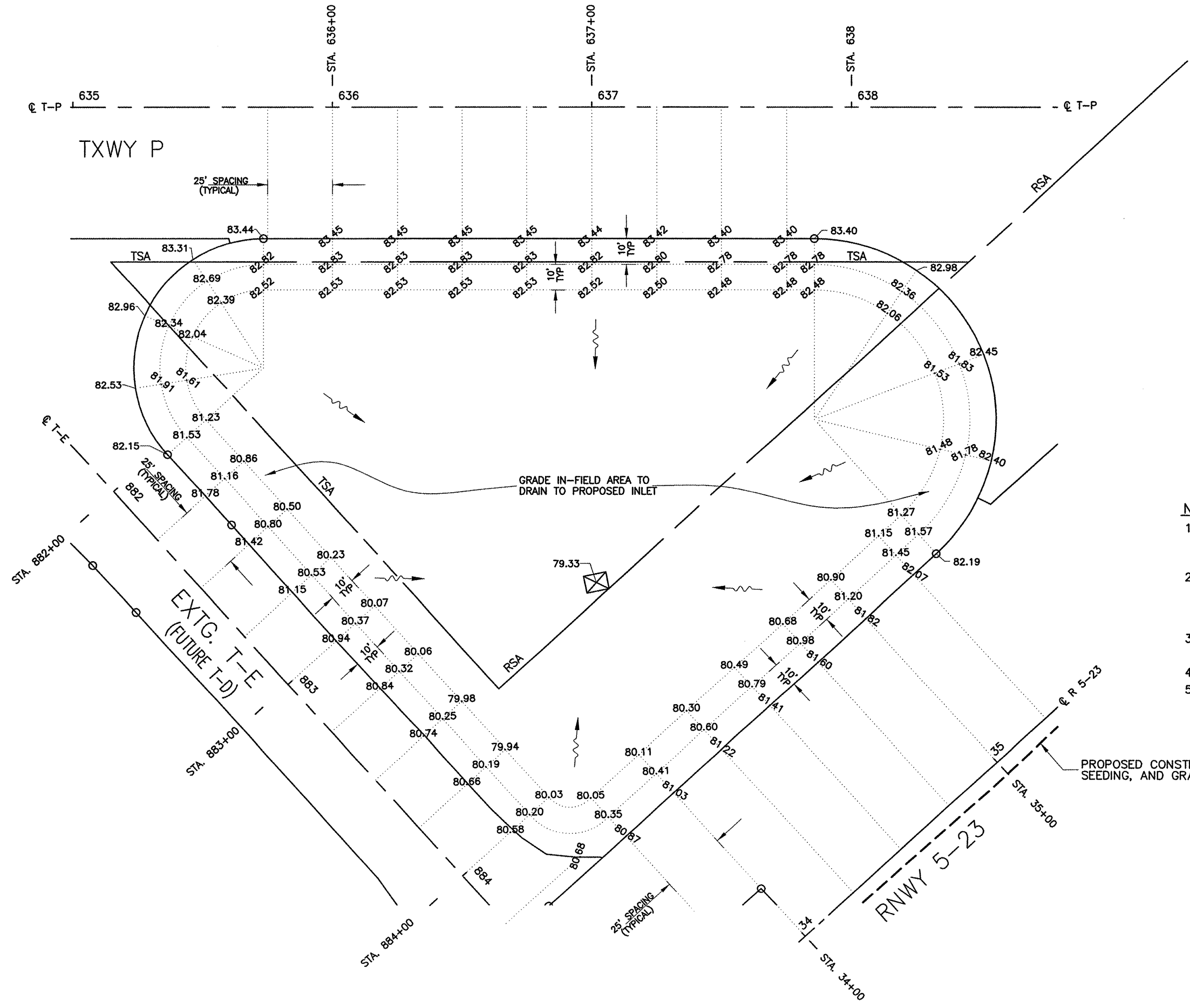


NOTES:

- 1.) CROSS SECTION SLOPES, CENTERLINE PROFILE GRADES, AND ALL SPOT GRADES SHALL BE SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION.
- 2.) GRADES SHOWN ON THIS SHEET AT EDGE OF PAVEMENT ARE PROPOSED SURFACE ELEVATIONS OF PAVEMENT. PROPOSED EARTH SHOULDER FINAL GRADE AT EDGE OF P.V.M.T. IS 0.125' BELOW EDGE OF PROPOSED PAVEMENT GRADE.
- 3.) SEE CROSS SECTIONS FOR SURFACE ELEVATIONS OF THE PROPOSED GROUND IN THE SHOULDER AREA.
- 4.) 76.80 = PROPOSED ELEVATION 576.80.
- 5.) 76.60 = EXISTING ELEVATION 576.60.

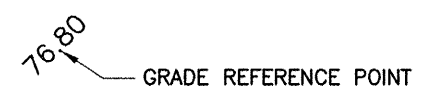


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NOTES:

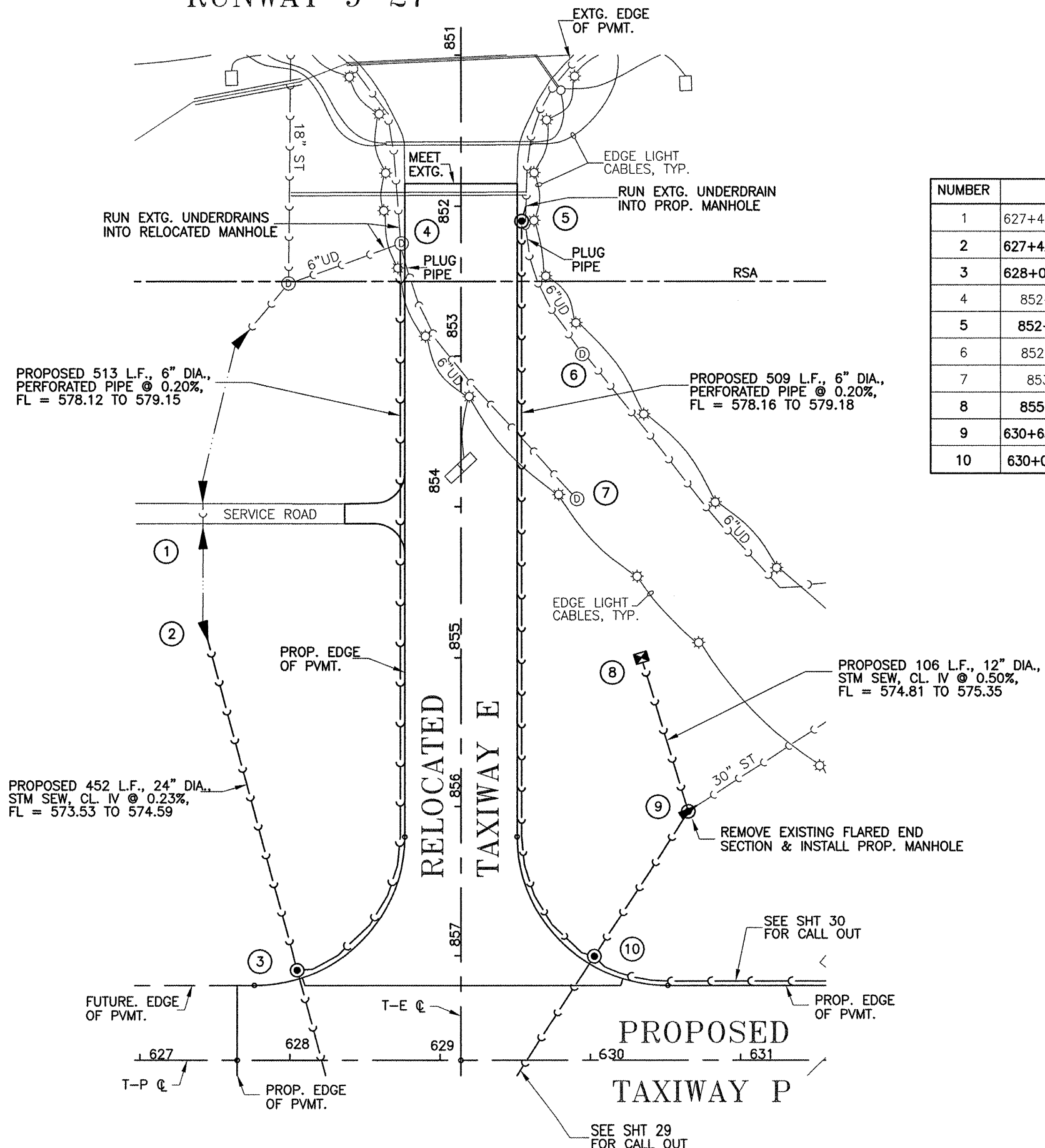
- 1.) CROSS SECTION SLOPES, CENTERLINE PROFILE GRADES, AND ALL SPOT GRADES SHALL BE SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION.
- 2.) GRADES SHOWN ON THIS SHEET AT EDGE OF PAVEMENT ARE PROPOSED SURFACE ELEVATIONS OF PAVEMENT. PROPOSED EARTH SHOULDER FINAL GRADE AT EDGE OF PVMT. IS 0.125' BELOW EDGE OF PROPOSED PAVEMENT GRADE.
- 3.) SEE CROSS SECTIONS FOR SURFACE ELEVATIONS OF THE PROPOSED GROUND IN THE SHOULDER AREA.
- 4.) 76.80 = PROPOSED ELEVATION 576.80.
- 5.) 76.60 = EXISTING ELEVATION 576.60.



RUNWAY 5-23 / TAXIWAY P IN-FIELD AREA

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RUNWAY 9-27



STRUCTURE SCHEDULE

NUMBER	LOCATION	TYPE	E. ELEV.	LID OR GRATE ELEV.	REMARK
1	627+42.16, LT 342.45'	EXISTING 18" DIA. FES	576.56'	-	EXISTING, TO REMAIN
2	627+42.06, LT 293.81'	PRC FLARED END SECTION, 24" DIA.	574.60'	-	AR752424
3	628+04.85, LT 60.46'	MANHOLE, 5' DIA.	NW= SE= 574.04' NE UD= 578.12'	582.53'	AR751550
4	852+25, RT 40.5'	EXISTING MANHOLE	MEET EXTG. +578.55' S = 579.15'	EXTG =583.67' PROP =583.48'	RELOCATE EXISTING*, AR751963
5	852+10, LT 40.5'	MANHOLE, SPL., 2' DIA.	MEET EXTG. +580.68' S = 579.18'	583.67'	AR751570
6	852+99, LT 80.8'	EXISTING MANHOLE	579.37'	EXTG =581.52'	REMOVE EXISTING, AR751903
7	853+92, LT 76'	EXISTING MANHOLE	579.05'	EXTG =581.21'	REMOVE EXISTING, AR751903
8	855+00, LT 120'	INLET SPECIAL (SINGLE)	575.35'	577.35'	NEENAH R-3475, AR751415
9	630+65.09, LT 166.80'	MANHOLE, 5' DIA. WITH FLAT SLAB TOP	574.74'	580.79'	MH & REMOVE EXTG F.E.S., AR751550 & AR752900
10	630+02.64, LT 69.66'	MANHOLE, 5' DIA. WITH FLAT SLAB TOP	574.37'	582.25'	AR751550

* = INCLUDED COST FOR FRAME & GRATE ADJUSTMENT IN COST OF RELOCATION (AR751963).

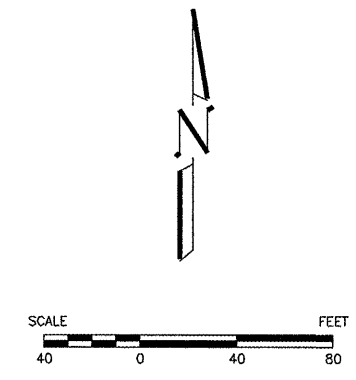
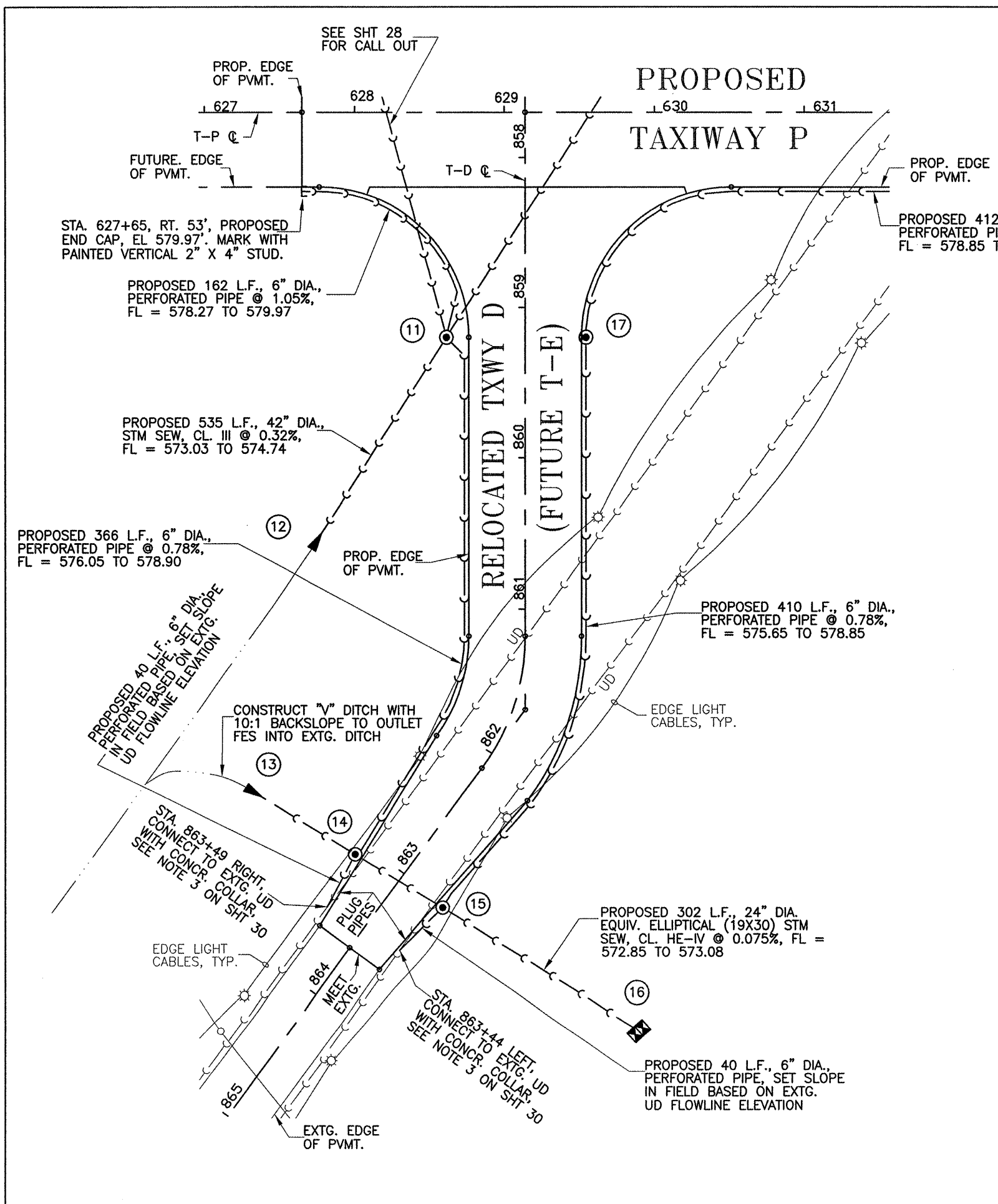
LEGEND:

- EXTG EDGE LIGHT
- EXTG GUIDANCE SIGN
- EXTG ELECTRICAL CABLE
- EXTG STORM SEWER, FIELD TILE OR UNDERDRAIN
- EXTG INLET
- EXTG OUTLET
- EXTG STORM MANHOLE
- EXTG SANITARY MANHOLE
- EXTG DRAINAGE DITCH
- EXTG CONDUIT OR DUCT BANK
- PROPOSED MANHOLE
- PROPOSED INLET
- PROPOSED FLARED END SECTION
- PROPOSED STORM SEWER, SANITARY SEWER OR UNDERDRAIN
- PROPOSED / EXISTING STRUCTURE NUMBER



NOTE:

1. SEE SHEET 30 FOR GENERAL DRAINAGE.



NOTE:
 1. SEE SHEET 30 FOR GENERAL DRAINAGE.

- LEGEND:**
- EXTG EDGE LIGHT
 - EXTG GUIDANCE SIGN
 - EXTG ELECTRICAL CABLE
 - EXTG STORM SEWER, FIELD TILE OR UNDERDRAIN
 - EXTG INLET
 - EXTG OUTLET
 - EXTG STORM MANHOLE
 - EXTG SANITARY MANHOLE
 - EXTG DRAINAGE DITCH
 - EXTG CONDUIT OR DUCT BANK
 - PROPOSED MANHOLE
 - PROPOSED INLET
 - PROPOSED FLARED END SECTION
 - PROPOSED STORM SEWER, SANITARY SEWER OR UNDERDRAIN
 - PROPOSED / EXISTING STRUCTURE NUMBER

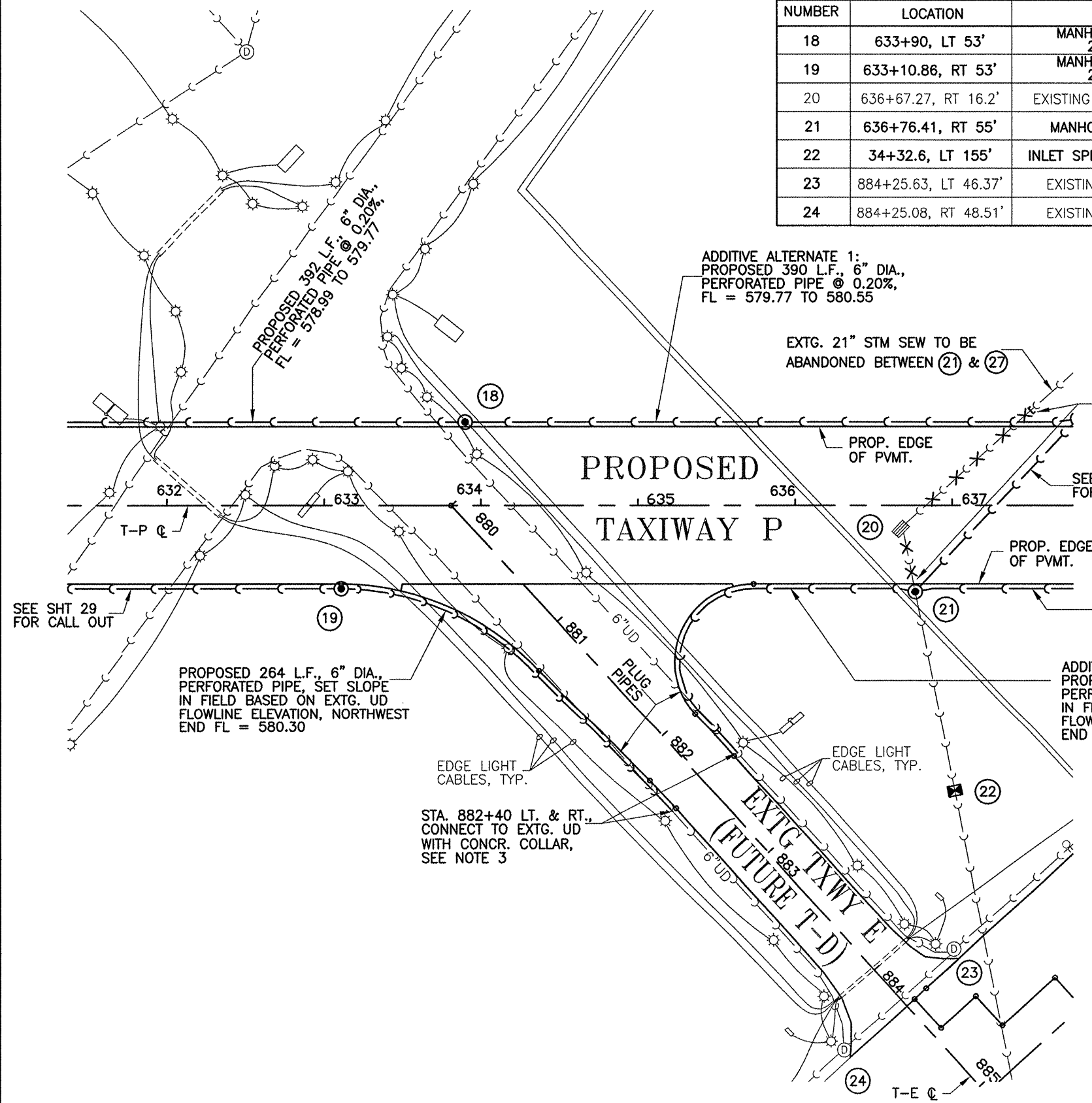
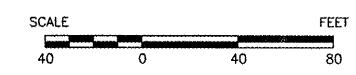
STRUCTURE SCHEDULE

NUMBER	LOCATION	TYPE	R. ELEV.	LID OR GRATE ELEV.	REMARK
11	628+61.44, RT 150'	MANHOLE, 6" DIA. WITH FLAT SLAB TOP	573.53'	581.07'	AR751560
12	627+71.44, RT 290'	PRC FLARED END SECTION, 42" DIA.	573.00'	-	AR752442
13	863+16.06, RT 119.23'	PRC FLARED END SECTION, ELLIPTICAL, EQ ROUND SIZE 24"	572.85'	-	AR752724
14	863+09.59, RT 34.21'	MANHOLE, 5" DIA. WITH FLAT SLAB TOP	572.91'	578.69'	AR751550
15	863+04.35, LT 34.60'	MANHOLE, 5" DIA. WITH FLAT SLAB TOP	572.97'	578.72'	AR751550
16	862+92.85, LT 188'	INLET (DOUBLE)	573.08'	575.32'	NEENAH R-3475-3, AR751410
17	859+20, LT 40.50'	MANHOLE, SPL., 2' DIA.	578.85'	581.58'	AR751570

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STRUCTURE SCHEDULE

NUMBER	LOCATION	TYPE	℄ ELEV.	LID OR GRATE ELEV.	REMARK
18	633+90, LT 53'	MANHOLE, SPL., 2' DIA.	579.77'	583.07'	AR751570
19	633+10.86, RT 53'	MANHOLE, SPL., 2' DIA.	580.30'	583.02'	AR751570
20	636+67.27, RT 16.2'	EXISTING CATCH BASIN	EXTG = 570.96'	EXTG = 575.54'	REMOVE EXISTING, AR751906
21	636+76.41, RT 55'	MANHOLE, 4' DIA.	N = 570.81' MEET EXTG S = 570.76'	583.08'	AR751540
22	34+32.6, LT 155'	INLET SPECIAL (SINGLE)	MEET EXTG = 570.11'	579.33'	NEENAH R-3475, AR751415
23	884+25.63, LT 46.37'	EXISTING MANHOLE	MEET EXTG, 573.86'	EXTG = 580.34' PROP = 580.58'	ADJUST EXISTING, AR751945
24	884+25.08, RT 48.51'	EXISTING MANHOLE	MEET EXTG, 573.86'	EXTG = 580.02' PROP = 580.09'	ADJUST EXISTING, AR751945



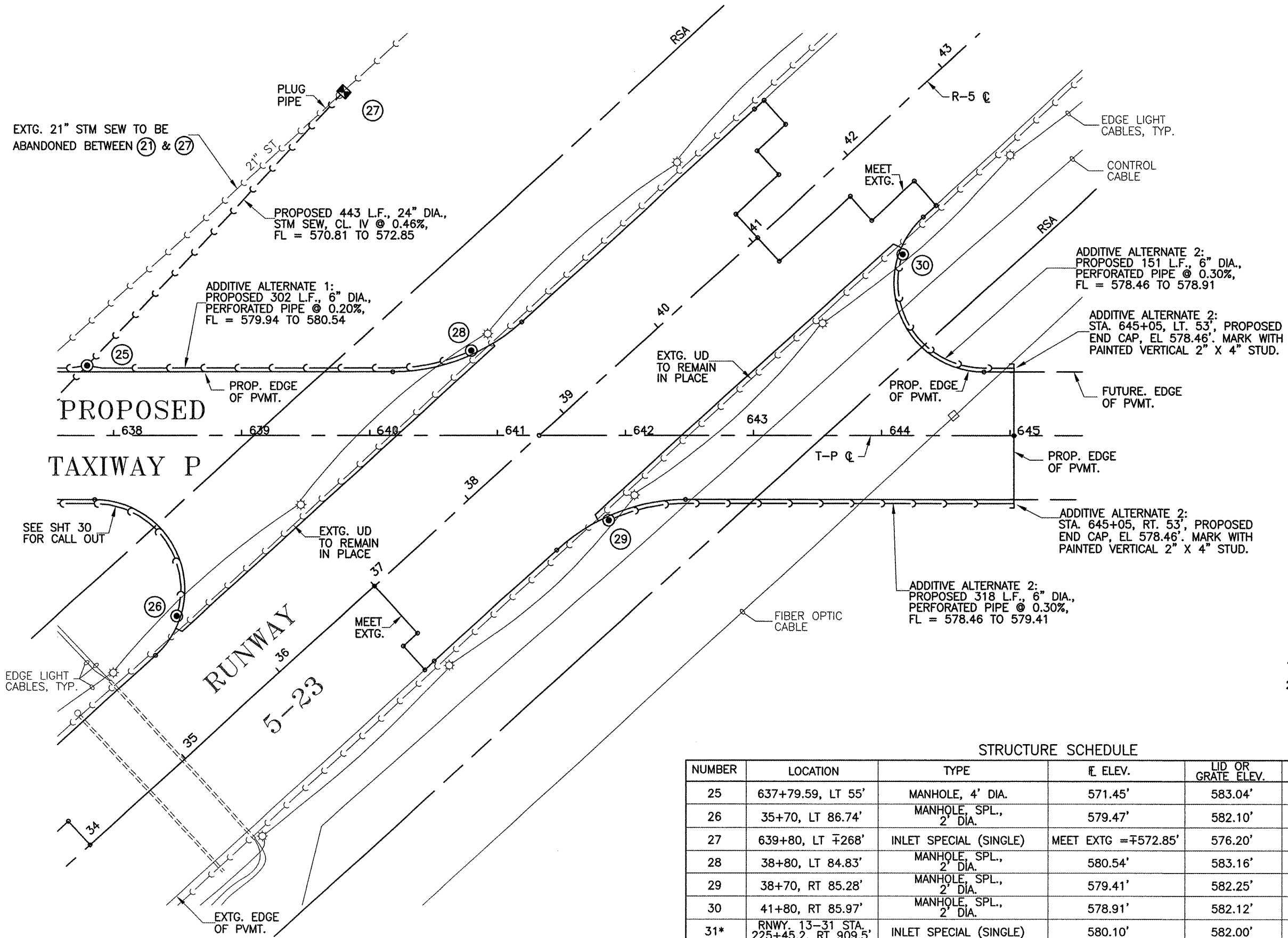
LEGEND:

- EXTG EDGE LIGHT
- EXTG GUIDANCE SIGN
- EXTG ELECTRICAL CABLE
- EXTG STORM SEWER, FIELD TILE OR UNDERDRAIN
- EXTG INLET
- EXTG OUTLET
- EXTG STORM MANHOLE
- EXTG SANITARY MANHOLE
- EXTG DRAINAGE DITCH
- EXTG CONDUIT OR DUCT BANK
- PROPOSED MANHOLE
- PROPOSED INLET
- PROPOSED FLARED END SECTION
- PROPOSED STORM SEWER, SANITARY SEWER OR UNDERDRAIN
- PROPOSED / EXISTING STRUCTURE NUMBER
- PROPOSED PIPE REMOVAL

GENERAL DRAINAGE NOTES:

1. SEE PLAN & PROFILE SHEETS FOR THE LOCATION OF EXISTING UNDERGROUND UTILITIES. CONTRACTOR SHALL HAND DIG AROUND ALL EXISTING UNDERGROUND UTILITIES. CONTRACTOR SHALL BE RESPONSIBLE FOR COST OF REPAIRING ALL DAMAGE UTILITIES.
2. OUTLET PROPOSED UNDER DRAIN INTO EXISTING / PROPOSED STM. SEW. PER DETAIL.
3. CONTRACTOR TO EXCAVATE AND EXPOSE EXISTING STORM PIPES, ELEC. CABLES, AND ELEC. DUCT BANKS AT PROPOSED MANHOLE, INLET, AND PIPE CROSSING LOCATIONS TO FIELD VERIFY VERTICAL LOCATIONS OF EXISTING UTILITIES. ENGINEER MAY ADJUST SLOPES AND TIE-INS AT THE TIME OF CONSTRUCTION AS REQUIRED.
4. THE LOCATIONS OF THE EXISTING UNDERDRAINS SHOWN ON THESE DRAWINGS WERE TAKEN FROM AIRPORT RECORDS AND ARE APPROXIMATE. PRIOR TO ORDERING MATERIALS, THE CONTRACTOR SHALL EXCAVATE, EXPOSE, AND FIELD VERIFY THE HORIZONTAL LOCATION, VERTICAL LOCATION, AND SIZE OF THE EXISTING CLAY UNDERDRAINS. ELEVATIONS, SLOPES, AND PIPE SIZES SHALL BE ADJUSTED BY THE RESIDENT ENGINEER IN THE FIELD AT THE TIME OF CONSTRUCTION. THE COST OF THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT RATES.
5. PLUG EXISTING PIPES TO BE ABANDONED IN PLACE. THE PLUGGING OF EXISTING PIPES TO BE ABANDONED IN PLACE SHALL BE ACCOMPLISHED WITH BRICKS AND GROUT (3,500 LBS. @ 28 DAY MIX) TO THE SATISFACTION OF THE RESIDENT ENGINEER. COST FOR PLUGGING EXISTING PIPES SHALL BE INCLUDED IN THE CONTRACT 701 UNIT PRICES. NO SEPARATE PAYMENT WILL BE MADE FOR THIS WORK.

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LEGEND:

- EXTG EDGE LIGHT
- EXTG GUIDANCE SIGN
- EXTG ELECTRICAL CABLE
- EXTG STORM SEWER, FIELD TILE OR UNDERDRAIN
- EXTG INLET
- EXTG OUTLET
- EXTG STORM MANHOLE
- EXTG SANITARY MANHOLE
- EXTG DRAINAGE DITCH
- EXTG CONDUIT OR DUCT BANK
- PROPOSED MANHOLE
- PROPOSED INLET
- PROPOSED FLARED END SECTION
- PROPOSED STORM SEWER, SANITARY SEWER OR UNDERDRAIN
- PROPOSED / EXISTING STRUCTURE NUMBER

NOTES:

1. SEE SHEET 30 FOR GENERAL DRAINAGE.
2. SEE SHEET 19 (NORTH BORROW AREA) FOR LOCATION OF DRAINAGE STRUCTURE NUMBER 31.

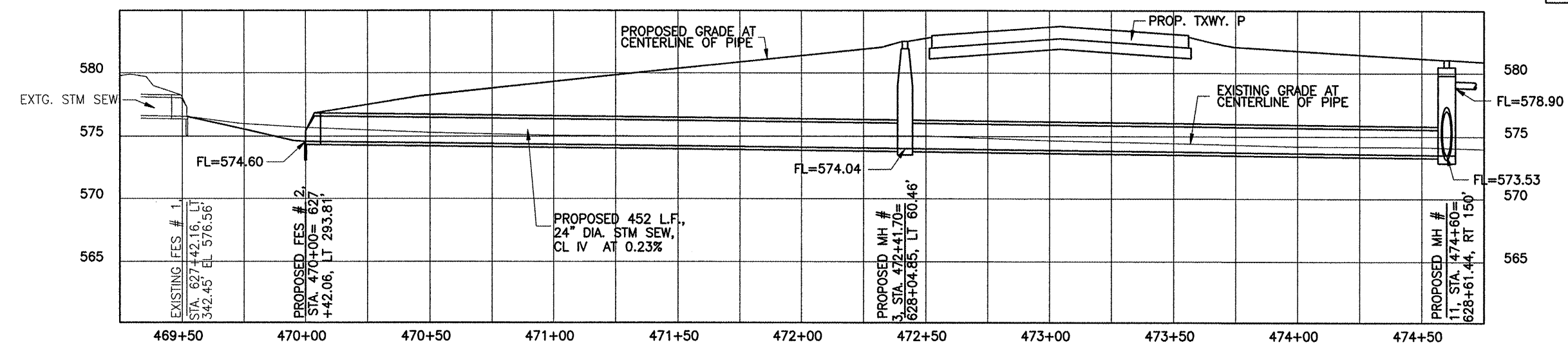
STRUCTURE SCHEDULE

NUMBER	LOCATION	TYPE	℄ ELEV.	LID OR GRATE ELEV.	REMARK	ADDITIVE ALTERNATE
25	637+79.59, LT 55'	MANHOLE, 4' DIA.	571.45'	583.04'	AR751540	
26	35+70, LT 86.74'	MANHOLE, SPL., 2' DIA.	579.47'	582.10'	AS751570, EXTG. UN INTO PROP. MH	1
27	639+80, LT 7268'	INLET SPECIAL (SINGLE)	MEET EXTG = 572.85'	576.20'	NEENAH R-3475, AR751415	
28	38+80, LT 84.83'	MANHOLE, SPL., 2' DIA.	580.54'	583.16'	AS751570, EXTG. UN INTO PROP. MH	1
29	38+70, RT 85.28'	MANHOLE, SPL., 2' DIA.	579.41'	582.25'	AT751570, EXTG. UN INTO PROP. MH	2
30	41+80, RT 85.97'	MANHOLE, SPL., 2' DIA.	578.91'	582.12'	AT751570, EXTG. UN INTO PROP. MH	2
31*	RNWX 13-31 STA. 225+45.2, RT 909.5'	INLET SPECIAL (SINGLE)	580.10'	582.00'	NEENAH R-3475, AR751415, SEE NOTE 2	

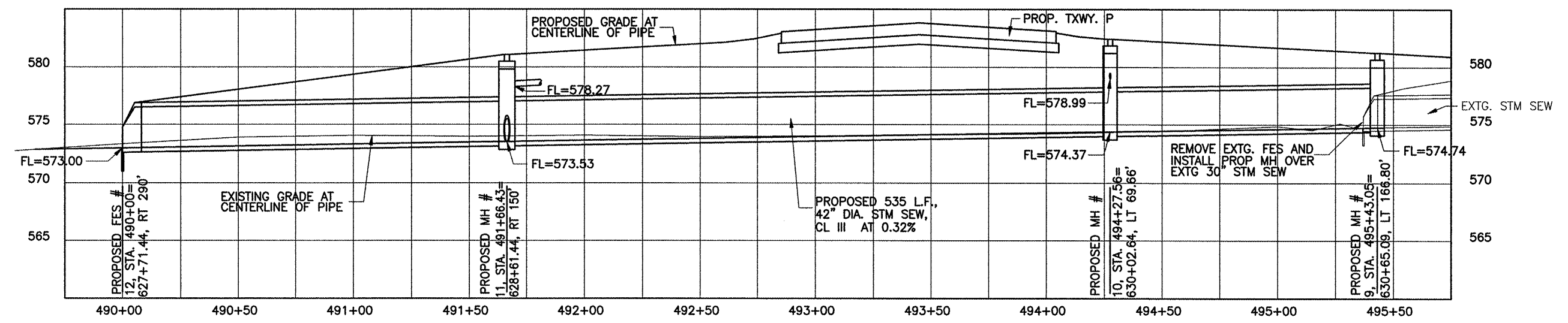
* = SEE NOTE 2

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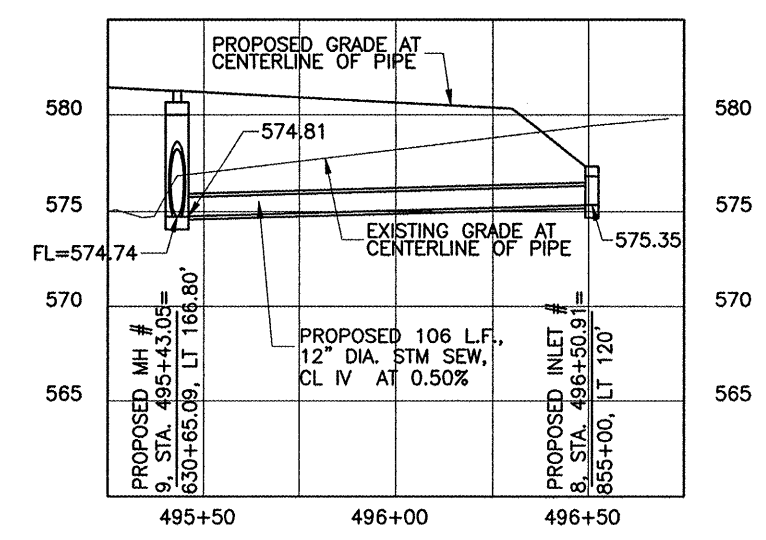
TAXIWAY P, STA. 627+42.06 LEFT 293.81' TO STA. 628+04.85 LEFT 60.46'
 STORM SEWER CENTERLINE PROFILE



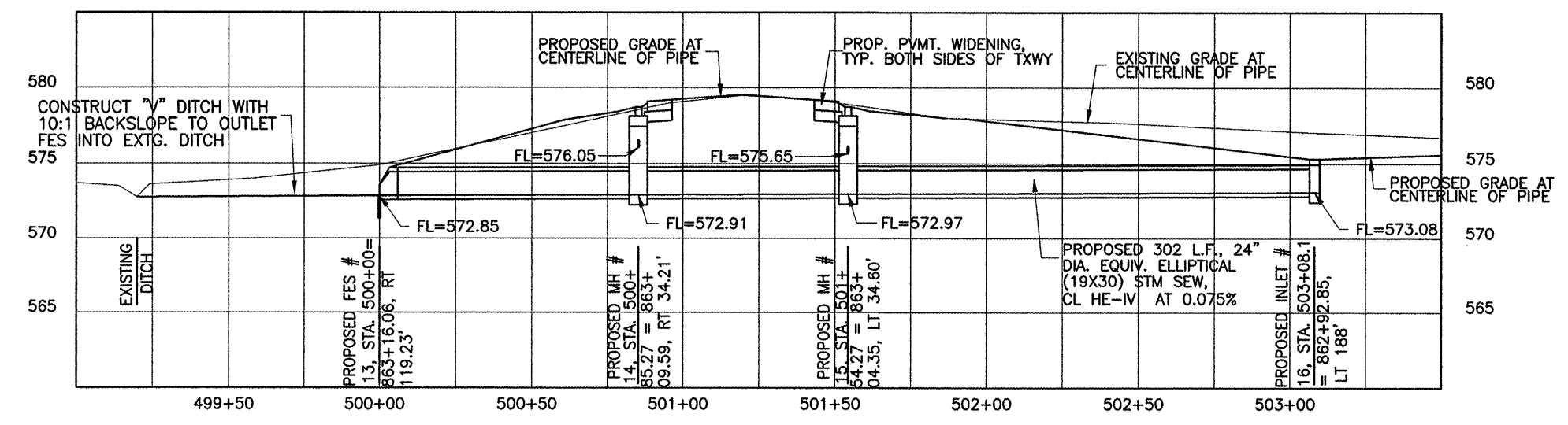
TAXIWAY P, STA. 627+71.44 RIGHT 290' TO STA. 630+65.09 LEFT 166.80'
 STORM SEWER CENTERLINE PROFILE



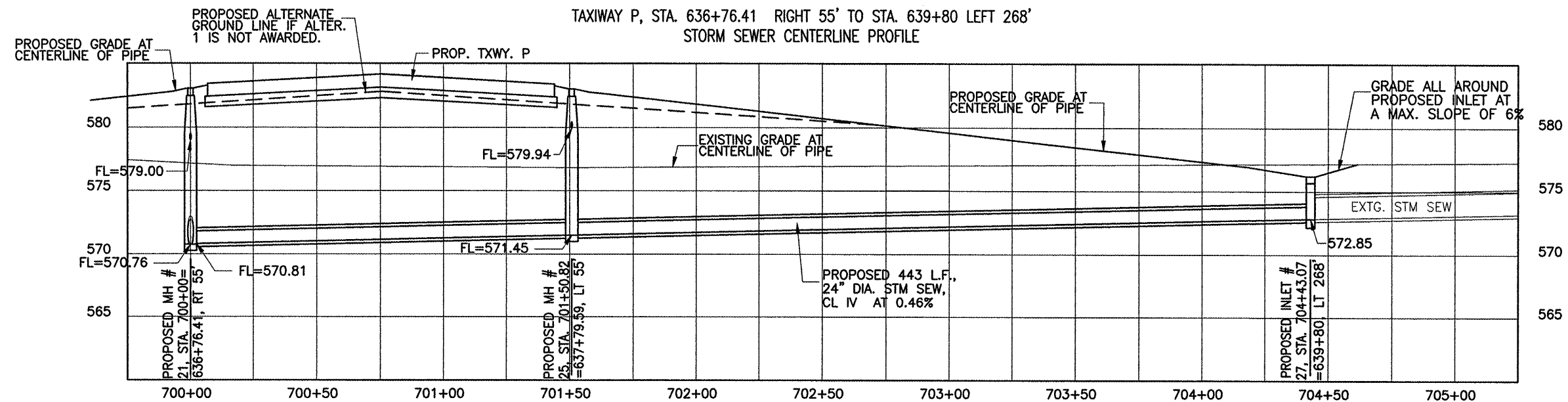
TAXIWAY P, STA. 630+65.09 LEFT 166.80' TO STA. 855+00 LEFT 120'
 STORM SEWER CENTERLINE PROFILE



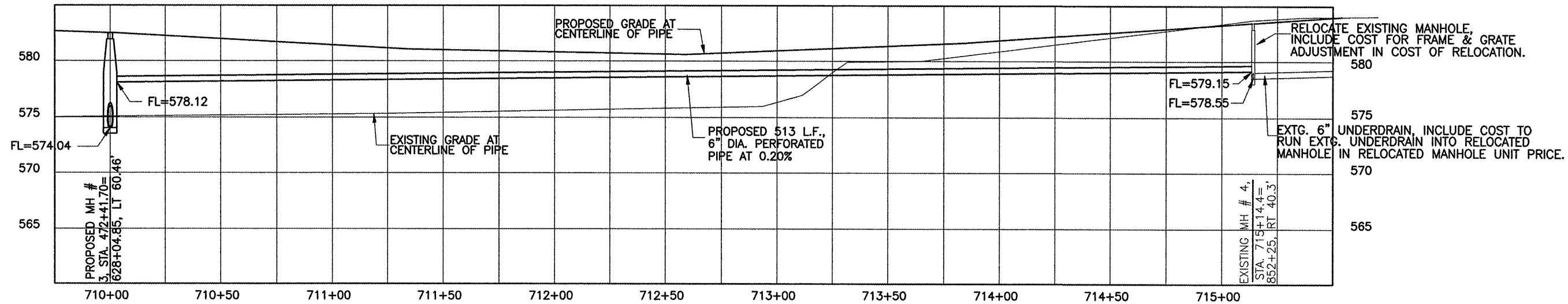
TAXIWAY D, STA. 862+92.85 LEFT 188' TO STA. 863+16.06 RIGHT 119.23'
 STORM SEWER CENTERLINE PROFILE



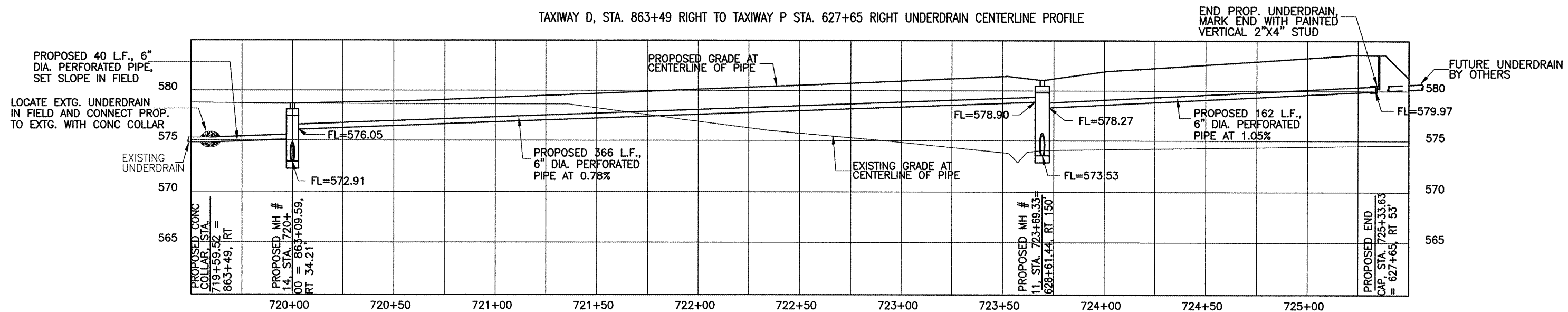
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TAXIWAY E, STA. 852+25 RIGHT TO TAXIWAY P STA. 628+04.85 LEFT UNDERDRAIN CENTERLINE PROFILE



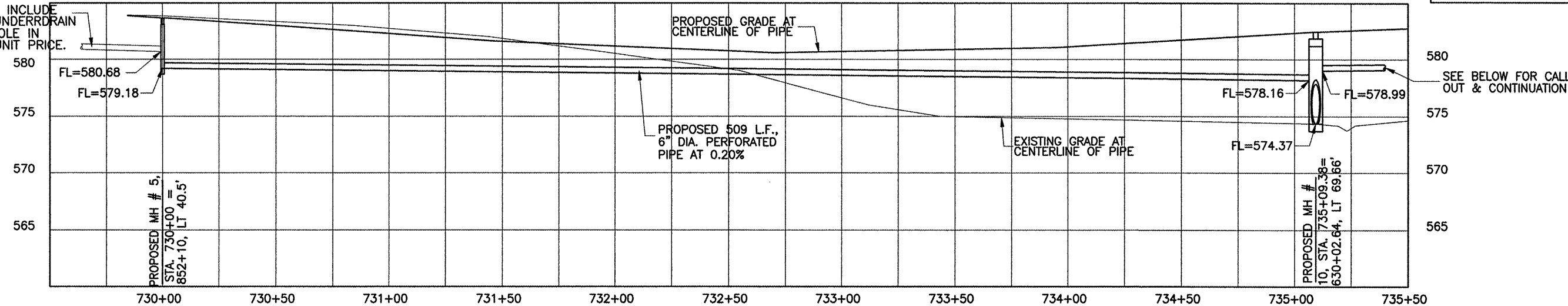
TAXIWAY D, STA. 863+49 RIGHT TO TAXIWAY P STA. 627+65 RIGHT UNDERDRAIN CENTERLINE PROFILE



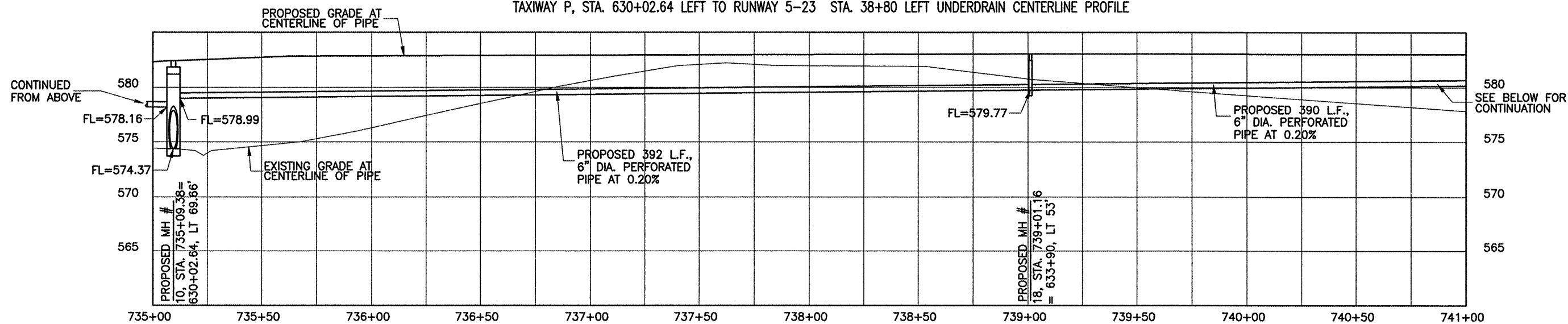
TAXIWAY E, STA. 852+10 LEFT TO TAXIWAY P STA. 630+02.64 LEFT UNDERDRAIN CENTERLINE PROFILE

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE II - MIDDLE PAVING
ILL. MLI-3854, QU008
SHEET 34 OF 82

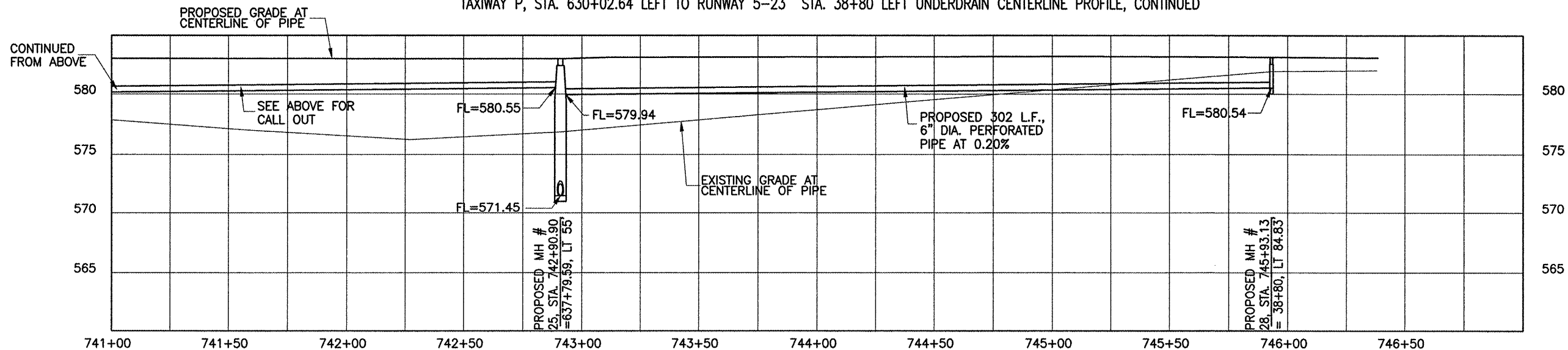
EXTG. 6" UNDERDRAIN, INCLUDE COST TO RUN EXTG. UNDERDRAIN INTO PROPOSED MANHOLE IN PROPOSED MANHOLE UNIT PRICE.



TAXIWAY P, STA. 630+02.64 LEFT TO RUNWAY 5-23 STA. 38+80 LEFT UNDERDRAIN CENTERLINE PROFILE

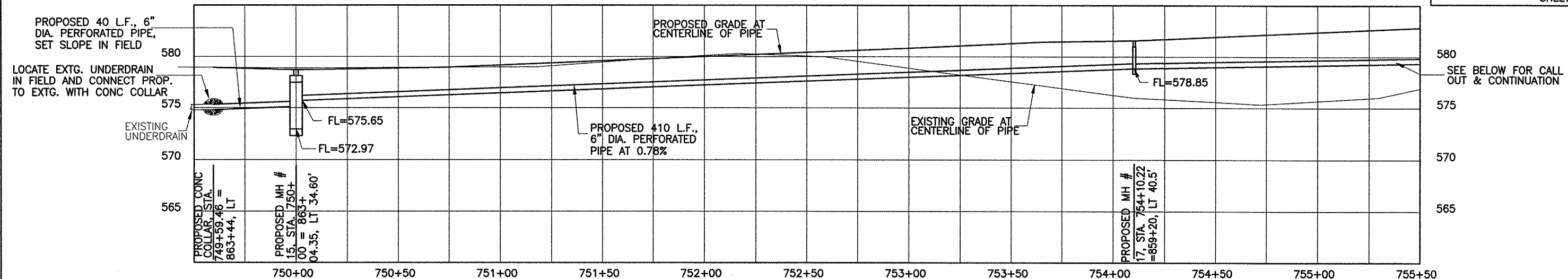


TAXIWAY P, STA. 630+02.64 LEFT TO RUNWAY 5-23 STA. 38+80 LEFT UNDERDRAIN CENTERLINE PROFILE, CONTINUED

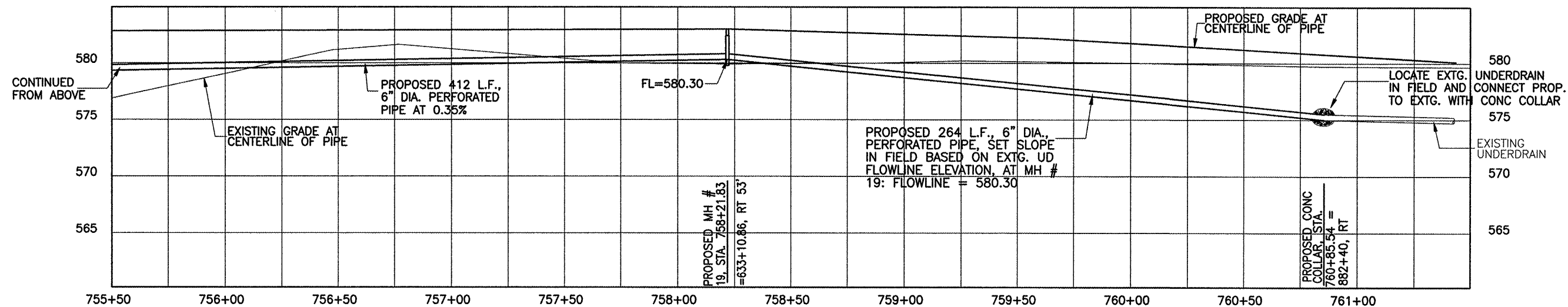


TAXIWAY D, STA. 863+44 LEFT TO TAXIWAY E STA. 882+40 RIGHT UNDERDRAIN CENTERLINE PROFILE

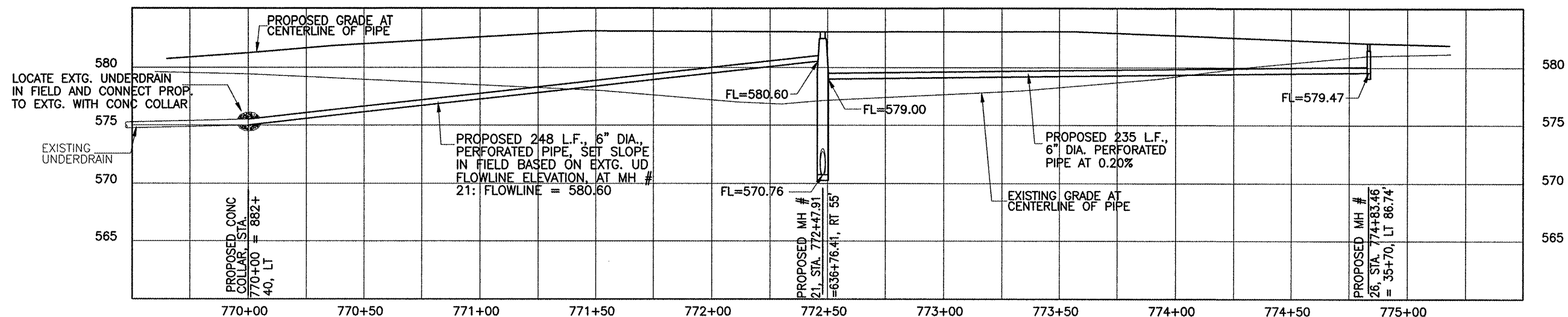
QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE II - MIDDLE PAVING
ILL. MLI-3854, QU008
SHEET 35 OF 82



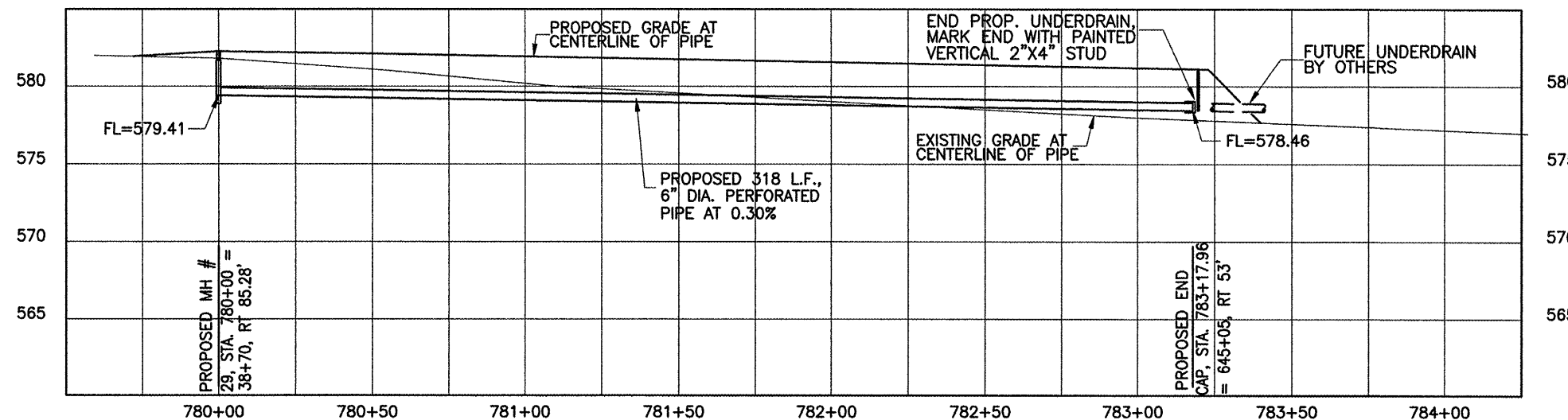
TAXIWAY D, STA. 863+44 LEFT TO TAXIWAY E STA. 882+40 RIGHT UNDERDRAIN CENTERLINE PROFILE, CONTINUED



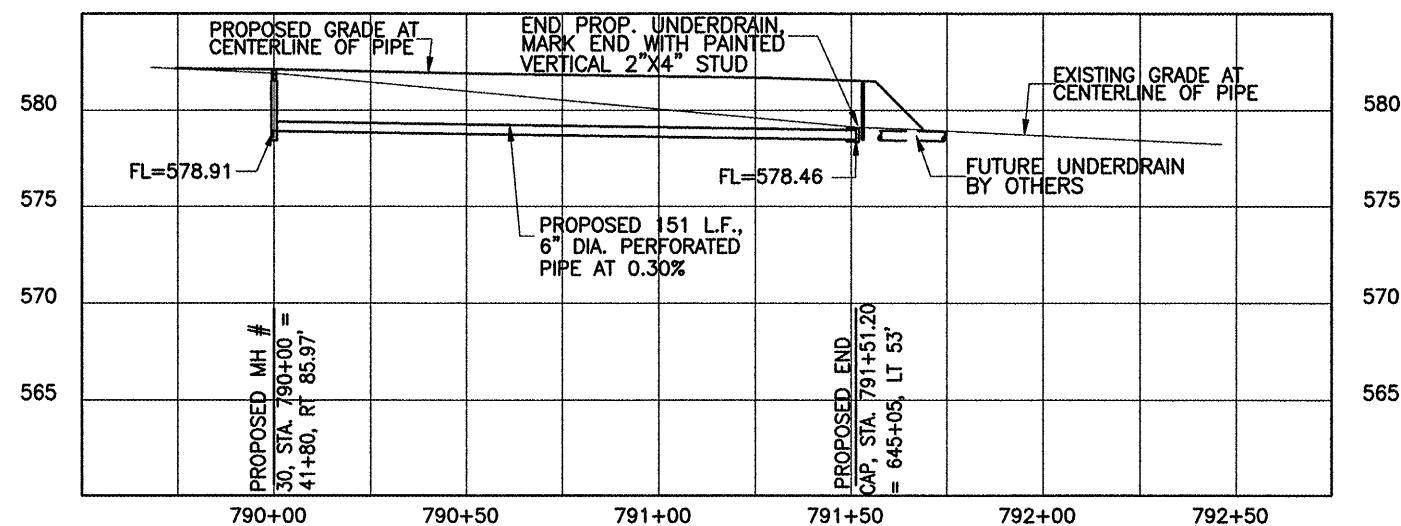
TAXIWAY E, STA. 882+40 LEFT TO RUNWAY 5-23, STA. 35+70 LEFT UNDERDRAIN CENTERLINE PROFILE



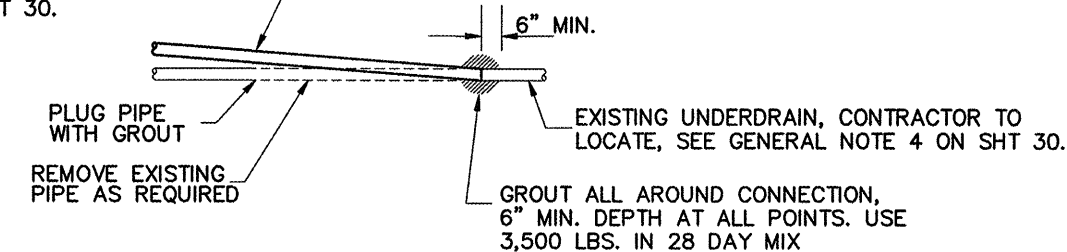
RUNWAY 5-23, STA. 38+70 RIGHT TO TAXIWAY P STA. 645+05 RIGHT UNDERDRAIN CENTERLINE PROFILE



RUNWAY 5-23, STA. 41+80 RIGHT TO TAXIWAY P STA. 645+05 LEFT UNDERDRAIN CENTERLINE PROFILE

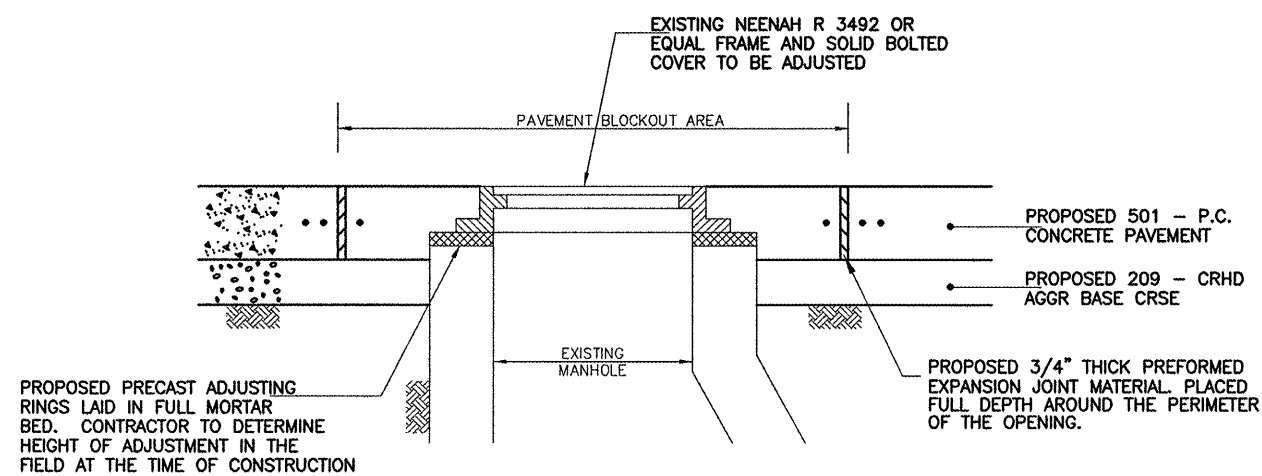


PROPOSED 6" PERFORATED PIPE,
SEE GENERAL NOTES 3 & 4
ON SHT 30.

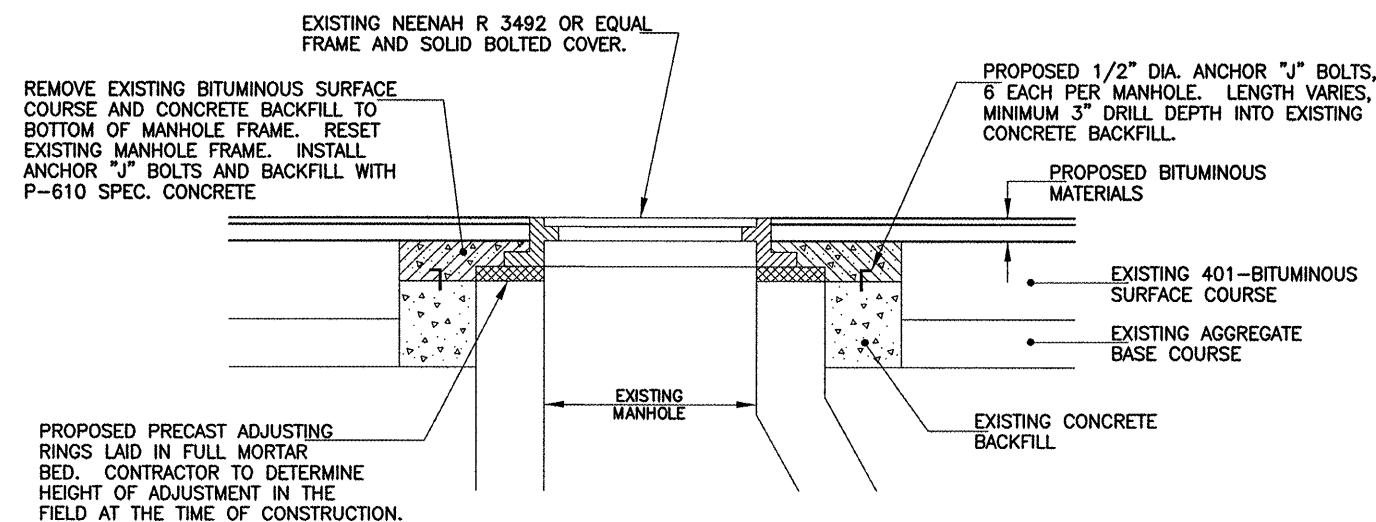


TYPICAL DETAIL - CONCRETE COLLAR

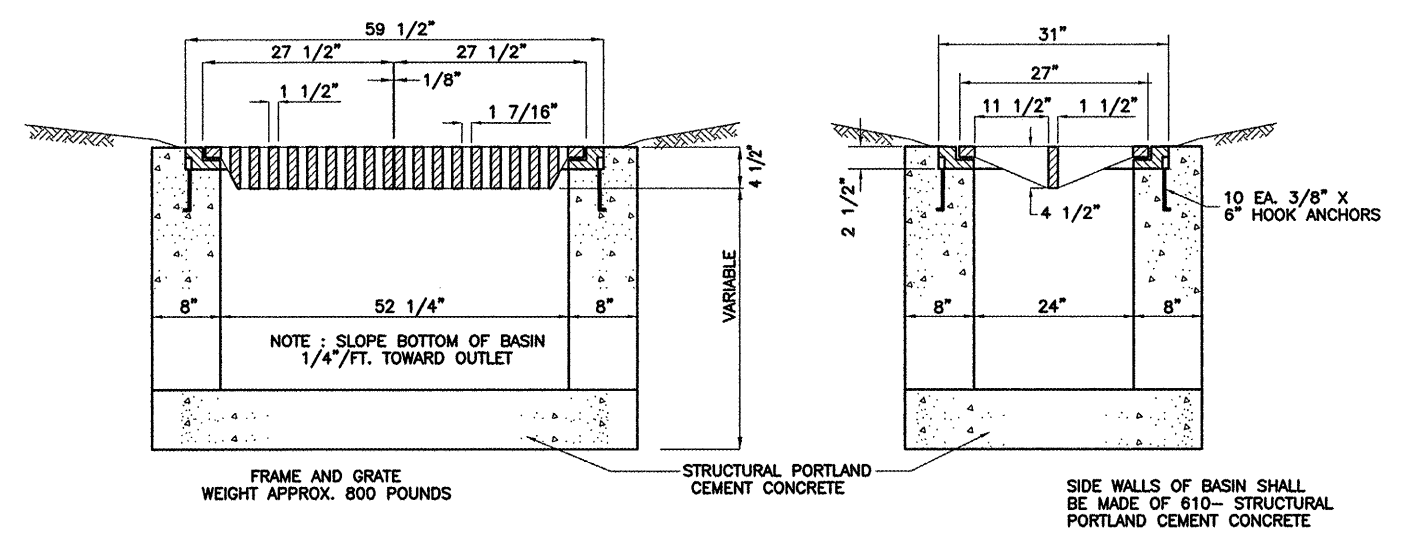
PROPOSED CONNECTION, EXISTING UNDERDRAIN
TO PROPOSED UNDERDRAIN, TYPICAL



TYPICAL DETAIL - MANHOLE
ADJUSTMENT IN P.C.C. PAVEMENT
NO SCALE

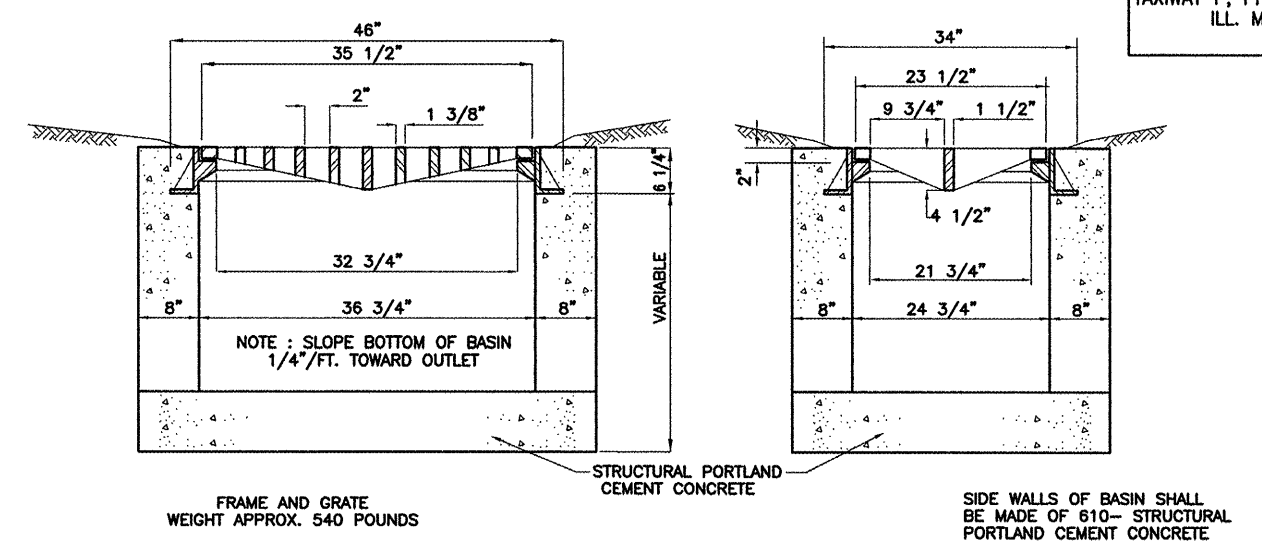


TYPICAL DETAIL - MANHOLE
ADJUSTMENT IN BITUMINOUS PAVEMENT
NO SCALE



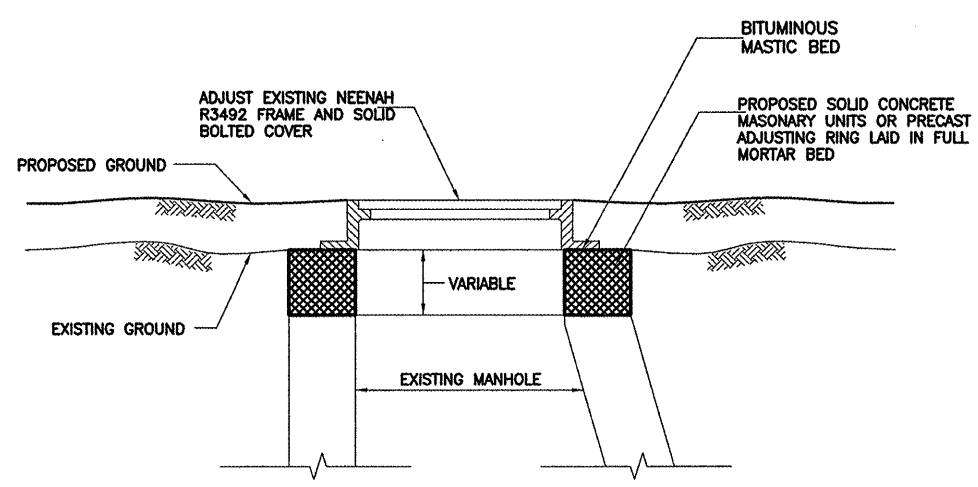
INLET (DOUBLE), ITEM 751410

- NOTES :
1. INLET SPECIAL SHALL INCLUDE FRAME WITH DOUBLE GRATE OF TYPE SIMILAR AND EQUAL TO NO. R 3475-3 AS SHOWN BY CATALOG "R" 14 TH EDITION OF NEENAH FOUNDRY CO.
 2. REMOVE BOLTS THAT HOLD THE GRATE TO THE FRAME AND DO NOT REINSTALL.



INLET SPECIAL, (SINGLE) ITEM 751415

- NOTES :
1. INLET SHALL INCLUDE FRAME WITH SINGLE GRATE OF TYPE SIMILAR AND EQUAL TO NO. R 3475 AS SHOWN BY CATALOG "R" ELEVENTH EDITION OF NEENAH FOUNDRY CO.
 2. REMOVE BOLTS THAT HOLD THE GRATE TO THE FRAME AND DO NOT REINSTALL.



TYPICAL DETAIL - MANHOLE
ADJUST - IN TURF

PRECAST REINFORCED CONCRETE FLARED END SECTION

PIPE DIA.	APPROX. WT. (LBS.)	WALL	A	B	C	D	E	G	R	SLOPE
12"	530	2"	4"	2'-0"	4'-0-7/8"	6'-0-7/8"	2'-0"	2"	9"	3:1
15"	740	2-1/4"	6"	2'-3"	3'-10"	6'-1"	2'-6"	2-1/4"	11"	3:1
18"	990	2-1/2"	9"	2'-3"	3'-10"	6'-1"	3'-0"	2-1/2"	12"	3:1
21"	1280	2-3/4"	9"	2'-11"	3'-2"	6'-1"	3'-6"	2-3/4"	13"	3:1
24"	1520	3"	9-1/2"	3'-7-1/2"	2'-6"	6'-1-1/2"	4'-0"	3"	14"	3:1
27"	1930	3-1/4"	10-1/2"	4'-0"	2'-1-1/2"	6'-1-1/2"	4'-6"	3-1/4"	14-1/2"	3:1
30"	2190	3-1/2"	1'-0"	4'-6"	1'-7-3/4"	6'-1-3/4"	5'-0"	3-1/2"	15"	3:1
33"	3200	3-3/4"	1'-1-1/2"	4'-10-1/2"	3'-3-1/4"	8'-1-3/4"	5'-6"	3-3/4"	17-1/2"	3:1
36"	4100	4"	1'-3"	5'-3"	2'-10-3/4"	8'-1-3/4"	6'-0"	4"	20"	3:1
42"	5380	4-1/2"	1'-9"	5'-3"	2'-11"	8'-2"	6'-6"	4-1/2"	22"	3:1
48"	6550	5"	2'-0"	6'-0"	2'-2"	8'-2"	7'-0"	5"	22"	3:1
54"	8240	5-1/2"	2'-3"	5'-5"	2'-11"	8'-4"	7'-6"	5-1/2"	24"	2.4:1
60"	8730	6"	2'-11"	5'-0"	3'-3"	8'-3"	8'-0"	5"	*	2:1
66"	10710	6-1/2"	2'-6"	6'-0"	2'-3"	8'-3"	8'-6"	5-1/2"	*	2:1
72"	12520	7"	3'-0"	6'-6"	1'-9"	8'-3"	9'-0"	6"	*	1.86:1
78"	14770	7-1/2"	3'-0"	7'-6"	1'-9"	9'-3"	9'-6"	6-1/2"	*	1.82:1
84"	18160	8"	3'-0"	7'-6-1/2"	1'-9"	9-3-1/2"	10'-0"	6-1/2"	*	1.5:1

RADIUS AS FURNISHED BY MANUFACTURER

NOTES:

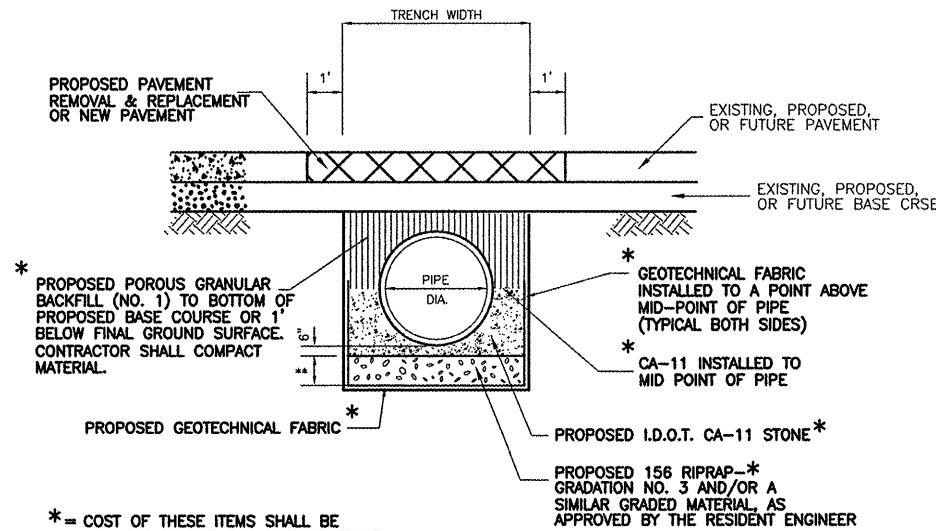
1. PRECAST CONCRETE FLARED END SECTIONS SHALL CONFORM TO THE APPLICABLE REQUIREMENTS OF AASHTO M-170 CLASS III, WALL B REINFORCED CONCRETE PIPE.
2. PRECAST CONCRETE FLARED END SECTION FOR PIPE DIAMETER REQUIRED SHALL BE AS INDICATED ON DETAIL PLAN FOR EACH INDIVIDUAL INSTALLATION.
3. THE END BLOCK SHALL BE PLACED PRIOR TO THE INSTALLATION OF THE FLARED END SECTION. THE END BLOCK SHALL BE BACKFILLED IN ACCORDANCE WITH ART. 701-3.7 OF THE STANDARD SPECIFICATIONS, COST INCIDENTAL TO END SECTION.
4. BEDDING MATERIALS AND GEOTECH FABRIC PER 701 PIPE SPECIFICATION IN THE SPECIAL PROVISIONS AS SHOWN IN THIS DETAIL SHALL BE INCLUDED IN THE 752 PIPE END SECTION CONTRACT UNIT PRICE.

MODIFIED I.D.O.T. STANDARD 542301

FOR EQUIV ELLIPTICAL PRC FES, SEE IDOT STANDARD 542306. ADDITIONAL BEDDING REQUIREMENTS THAT ARE SHOWN ON THIS SHEET SHALL BE APPLIED TO IDOT STANDARD 542306.

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PROPOSED TYPICAL SECTION - PIPE TRENCH
(ALL PROPOSED PIPES)

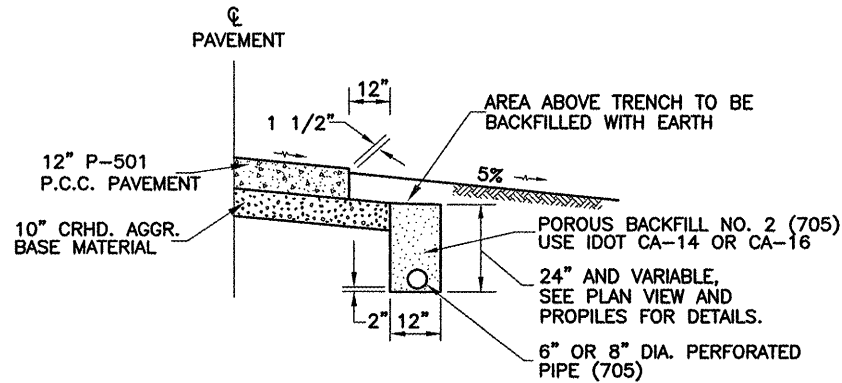


* = COST OF THESE ITEMS SHALL BE INCLUDED IN THE 701 PIPE CONTRACT UNIT PRICES

GENERAL PIPE NOTES:

- GROUND WATER IS EXPECTED. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DEWATERING, TO THE SATISFACTION OF THE ENGINEER, TO INSURE PROPER INSTALLATION OF PIPES. NO ADDITIONAL COMPENSATION BEYOND THE CONTRACT UNIT PRICES WILL BE ALLOWED FOR DEWATERING COSTS.
- COMPACTED BEDDING AND BACKFILL MATERIALS SHALL BE REQUIRED AS SHOWN IN DETAIL. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

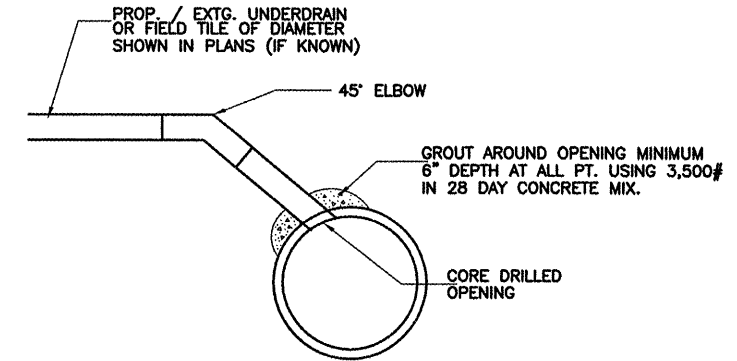
PIPE DIA.	TRENCH WIDTH	PVMT. R.W.M. WIDTH	** FOUNDATION DEPTH
6" / 8"	3.75'	6'	10"
12"	4.17'	6.5'	10"
18"	4.75'	7'	10"
ELLIPTICAL 24" EQUIV	5.87'	8'	18"
24"	5.33'	7.5'	18"
30"	5.92'	8'	18"
42"	7.08'	9.5'	18"



NOTES :

- PIPE UNDERDRAIN MATERIAL SHALL MEET THE MATERIAL REQUIREMENTS OF ITEM 705 AND THE SPECIAL PROVISIONS.
- PIPE UNDERDRAIN TO BE INSTALLED BEFORE PLACEMENT OF PAVEMENT & CRUSHED AGGR.
- COST OF POROUS BACKFILL NO. 2 (CA-14 OR CA-16), BENDS AND FITTINGS TO BE INCLUDED IN THE UNIT PRICE FOR UNDERDRAINS.
- NO ADDITIONAL COMPENSATION SHALL BE MADE FOR CHANGES IN ELEVATIONS MADE BY THE RESIDENT ENGINEER.
- PIPE UNDERDRAIN TO BE INSTALLED ON BOTH SIDES OF PAVEMENT.

PERFORATED PIPE UNDERDRAIN DETAIL

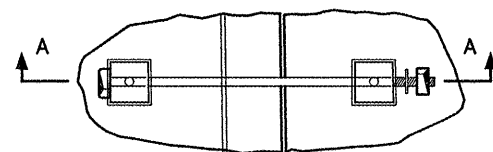


DETAIL OF PIPE UNDERDRAIN / FIELD TILE OUTLET INTO STORM SEWER OR PIPE CULVERT

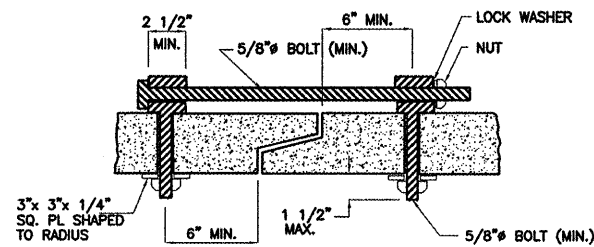
NOTES :

COST OF NEW PIPE, ELBOW AND GROUTING TO BE INCLUDED IN CONTRACT UNIT PRICES. ANY DAMAGE TO EXISTING STORM SEWER, TILE, OR PIPE CULVERT SHALL BE REPAIRED TO THE SATISFACTION OF THE RESIDENT ENGINEER AT THE CONTRACTOR'S EXPENSE.

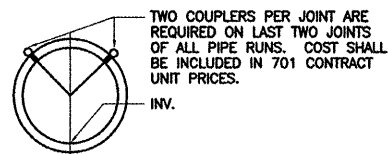
PROPOSED PIPE COUPLERS



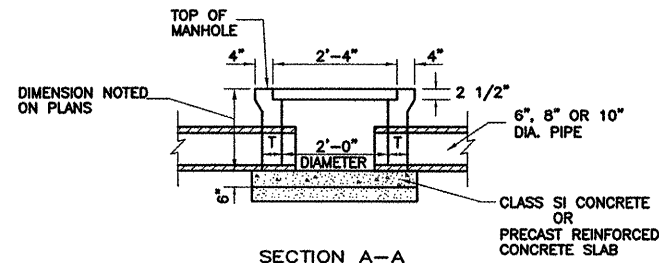
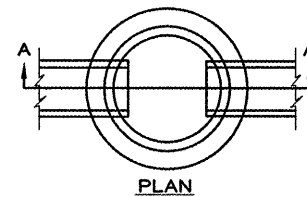
PLAN OF ONE COUPLER



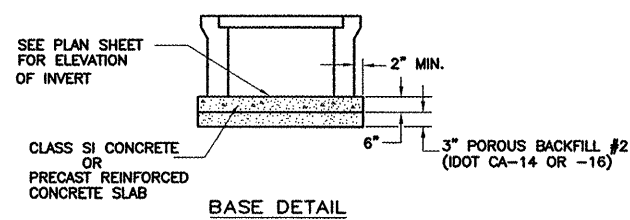
SECTION A-A



TWO COUPLERS PER JOINT ARE REQUIRED ON LAST TWO JOINTS OF ALL PIPE RUNS. COST SHALL BE INCLUDED IN 701 CONTRACT UNIT PRICES.



SECTION A-A



BASE DETAIL

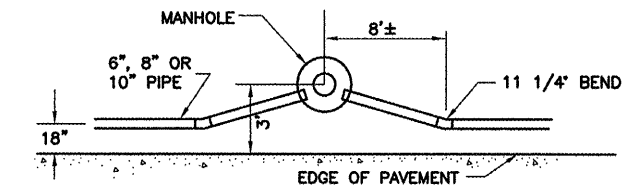
DETAIL OF MANHOLE SPECIAL ITEM 751570

ALTERNATE MATERIALS FOR WALLS	T
PRECAST REINFORCED CONCRETE RINGS	5"
CAST IN PLACE CONCRETE	6"

NOTES :

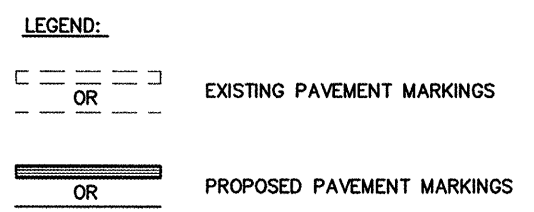
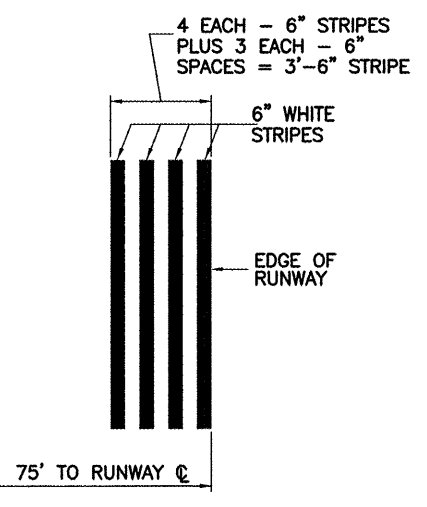
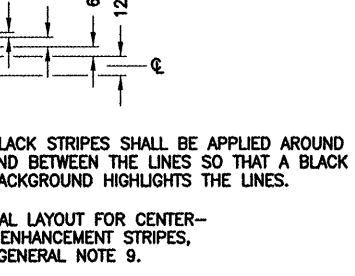
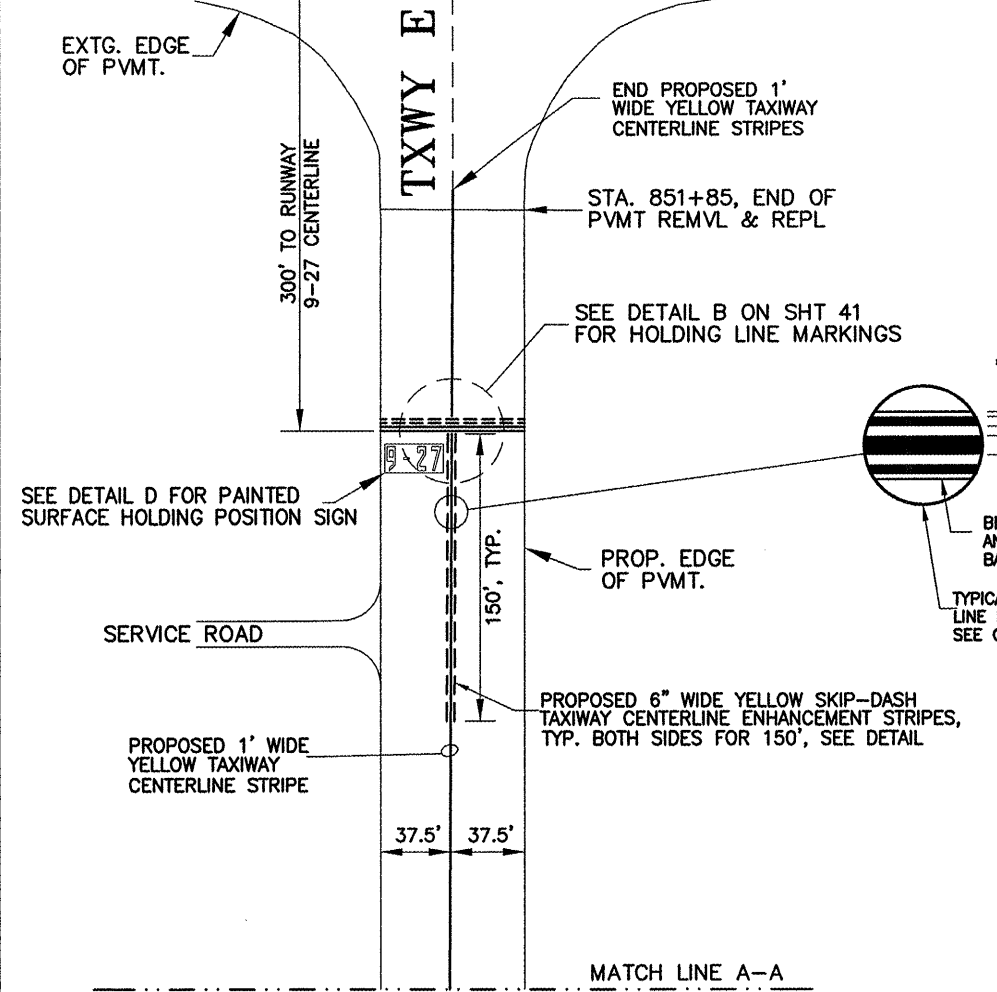
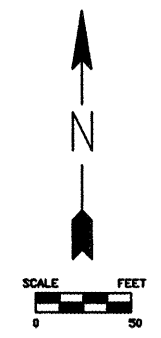
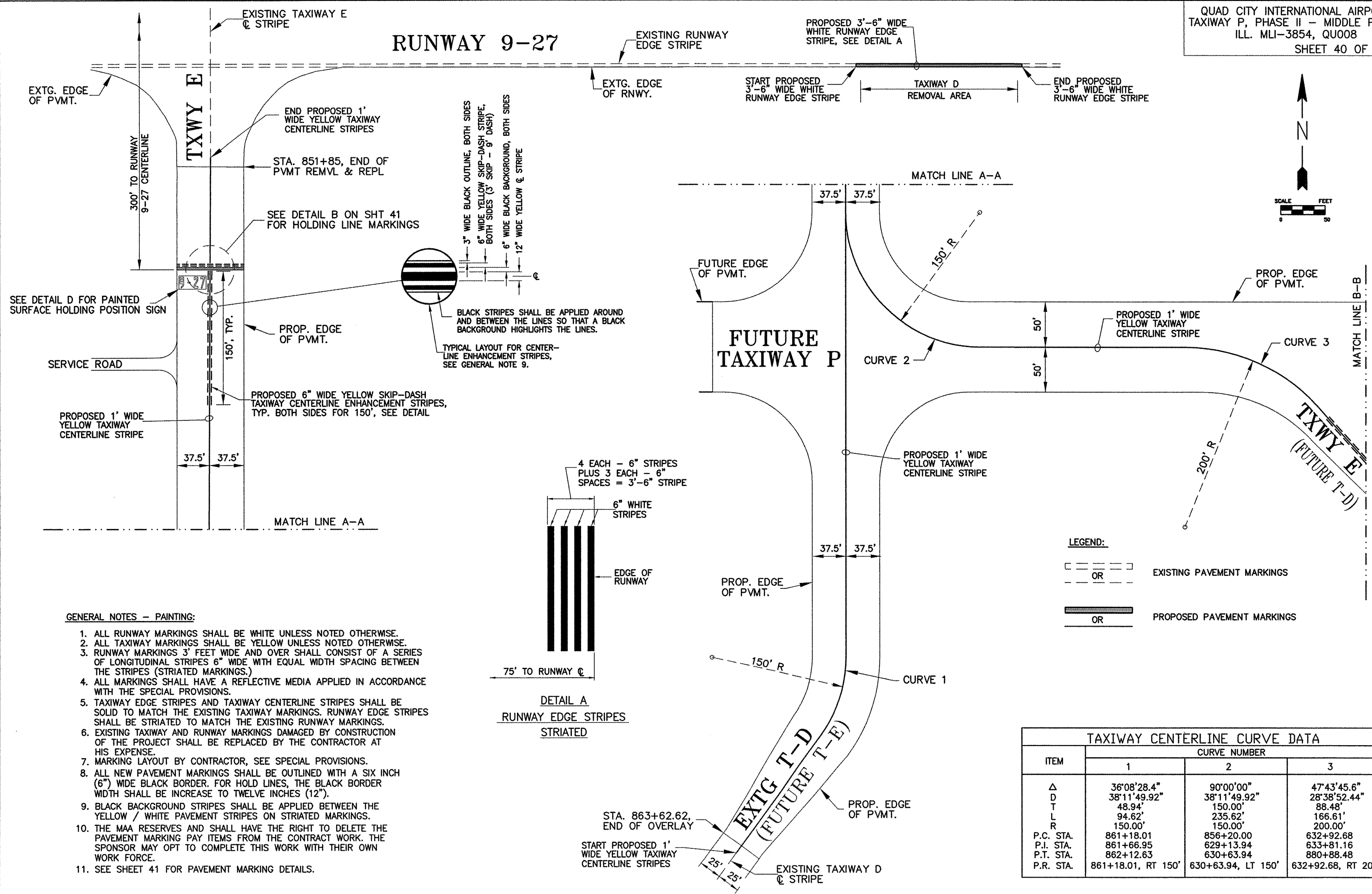
- PRECAST REINFORCED CONCRETE RINGS AND MANHOLE FRAMES SHALL BE LAID IN FULL MORTAR BEDS WITH FLUSH JOINTS.
- MORTAR MIX SHALL CONSIST OF 1 PART PORTLAND CEMENT AND 3 PARTS SAND.
- ALL CONCRETE SHALL HAVE A MIN. STRENGTH AT 28 DAYS OF 3500 P.S.I.
- THE CONTRACT UNIT PRICE FOR MANHOLE SPECIAL SHALL INCLUDE FURNISHING AND INSTALLING THE FRAME AND LID. THE SAND CUSHION, AND COMPACTING THE BACKFILL MATERIAL. SEE PLAN SHEETS FOR FRAME AND LID TYPE.

MODIFIED IDOT STANDARD 602301



COST OF FITTINGS TO BE INCLUDED IN THE UNIT PRICE FOR UNDERDRAINS.

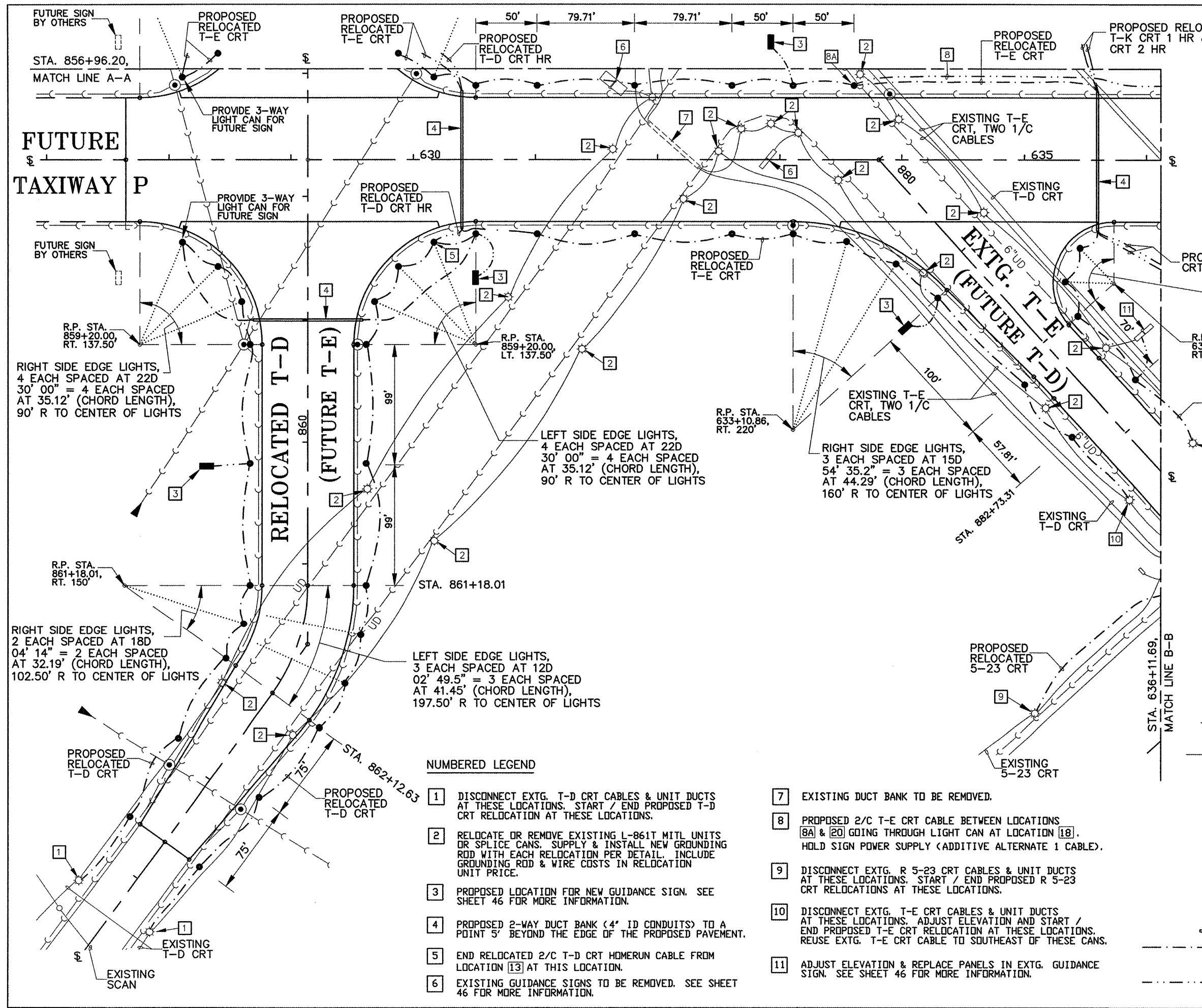
PLAN VIEW OF MANHOLE SPECIAL ADJACENT TO EDGE OF PVMT.



- GENERAL NOTES - PAINTING:**
1. ALL RUNWAY MARKINGS SHALL BE WHITE UNLESS NOTED OTHERWISE.
 2. ALL TAXIWAY MARKINGS SHALL BE YELLOW UNLESS NOTED OTHERWISE.
 3. RUNWAY MARKINGS 3' FEET WIDE AND OVER SHALL CONSIST OF A SERIES OF LONGITUDINAL STRIPES 6" WIDE WITH EQUAL WIDTH SPACING BETWEEN THE STRIPES (STRIATED MARKINGS.)
 4. ALL MARKINGS SHALL HAVE A REFLECTIVE MEDIA APPLIED IN ACCORDANCE WITH THE SPECIAL PROVISIONS.
 5. TAXIWAY EDGE STRIPES AND TAXIWAY CENTERLINE STRIPES SHALL BE SOLID TO MATCH THE EXISTING TAXIWAY MARKINGS. RUNWAY EDGE STRIPES SHALL BE STRIATED TO MATCH THE EXISTING RUNWAY MARKINGS.
 6. EXISTING TAXIWAY AND RUNWAY MARKINGS DAMAGED BY CONSTRUCTION OF THE PROJECT SHALL BE REPLACED BY THE CONTRACTOR AT HIS EXPENSE.
 7. MARKING LAYOUT BY CONTRACTOR, SEE SPECIAL PROVISIONS.
 8. ALL NEW PAVEMENT MARKINGS SHALL BE OUTLINED WITH A SIX INCH (6") WIDE BLACK BORDER. FOR HOLD LINES, THE BLACK BORDER WIDTH SHALL BE INCREASE TO TWELVE INCHES (12").
 9. BLACK BACKGROUND STRIPES SHALL BE APPLIED BETWEEN THE YELLOW / WHITE PAVEMENT STRIPES ON STRIATED MARKINGS.
 10. THE MAA RESERVES AND SHALL HAVE THE RIGHT TO DELETE THE PAVEMENT MARKING PAY ITEMS FROM THE CONTRACT WORK. THE SPONSOR MAY OPT TO COMPLETE THIS WORK WITH THEIR OWN WORK FORCE.
 11. SEE SHEET 41 FOR PAVEMENT MARKING DETAILS.

ITEM	TAXIWAY CENTERLINE CURVE DATA		
	1	2	3
Δ	36°08'28.4"	90°00'00"	47°43'45.6"
D	38°11'49.92"	38°11'49.92"	28°38'52.44"
T	48.94'	150.00'	88.48'
L	94.62'	235.62'	166.61'
R	150.00'	150.00'	200.00'
P.C. STA.	861+18.01	856+20.00	632+92.68
P.I. STA.	861+66.95	629+13.94	633+81.16
P.T. STA.	862+12.63	630+63.94	880+88.48
P.R. STA.	861+18.01, RT 150'	630+63.94, LT 150'	632+92.68, RT 200'

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GENERAL NOTES:

1. SEE SHEET 49 FOR ELECTRICAL GENERAL NOTES.
2. SEE SHEETS 49-52 FOR ELECTRICAL DETAILS
3. SEE SHEET 46 FOR SIGN DETAILS.
4. ALL EXISTING ELECTRICAL CIRCUITS TO REMAIN ACTIVE AT ALL TIMES. CONTRACTOR TO PROVIDE, INSTALL, AND MAINTAIN TEMPORARY ABOVE GROUND JUMPER CABLES AS REQUIRED TO PROVIDE CIRCUIT CONTINUITY IN ALL CIRCUITS AT ALL TIMES DURING CONSTRUCTION. COSTS TO BE INCLUDED IN THE LIGHTING CONTRACT UNIT PRICES. LENGTH OF JUMPER CABLES SHALL NOT BE MEASURED FOR PAYMENT AND / OR PAYED FOR.
5. DO NOT DISTURB EXISTING ELECTRICAL CABLES UNTIL REPLACEMENT CABLES OR TEMPORARY JUMPER CABLES ARE IN SERVICE.

LEFT SIDE EDGE LIGHTS, 3 EACH SPACED AT 44D 05' 24.8" = 3 EACH SPACED AT 30.03' (CHORD LENGTH), 40' R TO CENTER OF LIGHTS

RIGHT SIDE EDGE LIGHTS, 4 EACH SPACED AT 22D 30' 00" = 4 EACH SPACED AT 35.12' (CHORD LENGTH), 90' R TO CENTER OF LIGHTS

LEFT SIDE EDGE LIGHTS, 4 EACH SPACED AT 22D 30' 00" = 4 EACH SPACED AT 35.12' (CHORD LENGTH), 90' R TO CENTER OF LIGHTS

RIGHT SIDE EDGE LIGHTS, 3 EACH SPACED AT 15D 54' 35.2" = 3 EACH SPACED AT 44.29' (CHORD LENGTH), 160' R TO CENTER OF LIGHTS

RIGHT SIDE EDGE LIGHTS, 2 EACH SPACED AT 18D 04' 14" = 2 EACH SPACED AT 32.19' (CHORD LENGTH), 102.50' R TO CENTER OF LIGHTS

LEFT SIDE EDGE LIGHTS, 3 EACH SPACED AT 12D 02' 49.5" = 3 EACH SPACED AT 41.45' (CHORD LENGTH), 197.50' R TO CENTER OF LIGHTS

SYMBOL LEGEND

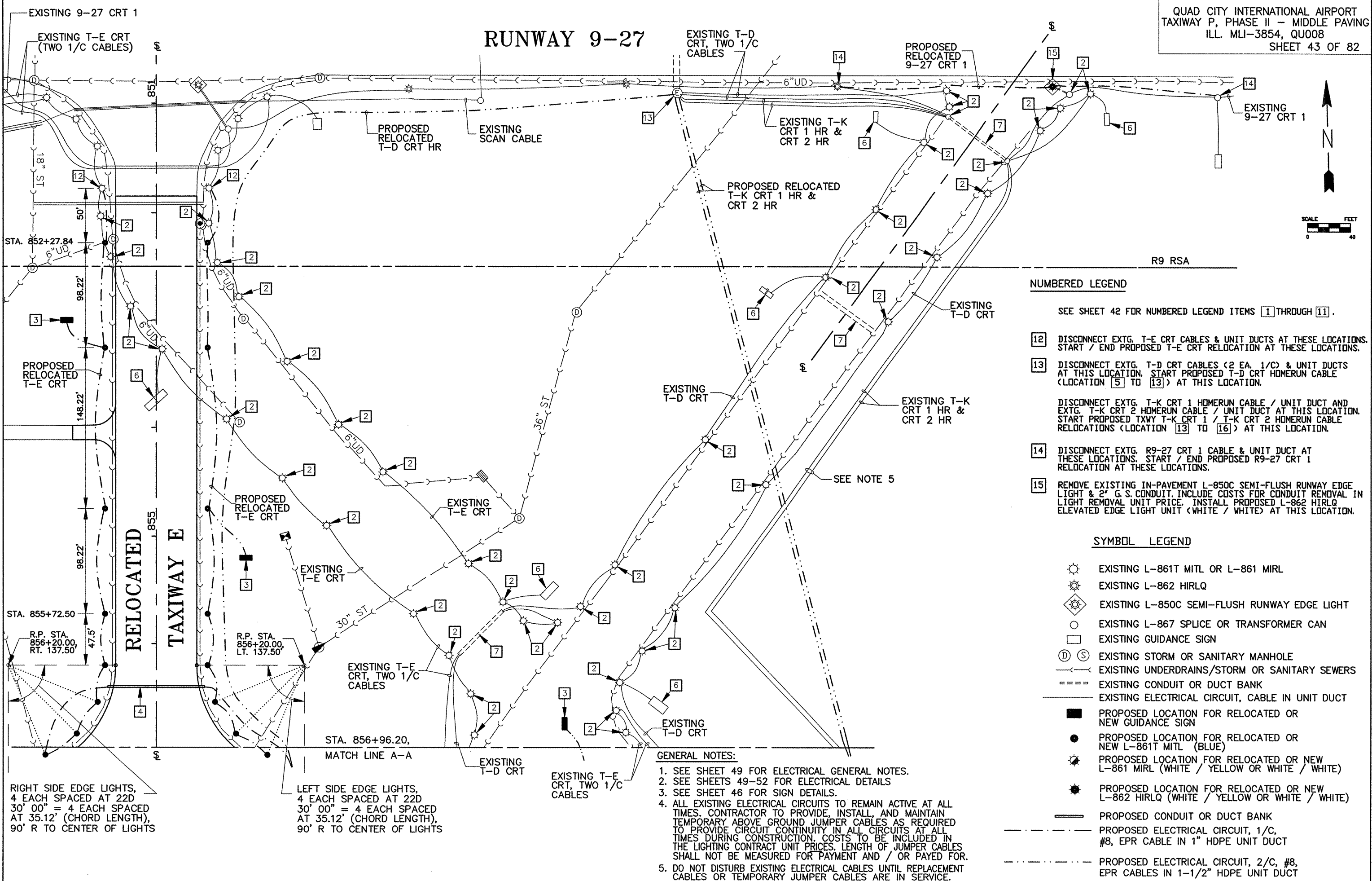
- ☉ EXISTING L-861T MITL OR L-861 MIRL
- ☉ EXISTING L-862 HIRLQ
- ◊ EXISTING L-850C SEMI-FLUSH RUNWAY EDGE LIGHT
- EXISTING L-867 SPLICE OR TRANSFORMER CAN
- EXISTING GUIDANCE SIGN
- Ⓧ EXISTING STORM OR SANITARY MANHOLE
- EXISTING UNDERDRAINS/STORM OR SANITARY SEWERS
- EXISTING CONDUIT OR DUCT BANK
- EXISTING ELECTRICAL CIRCUIT, CABLE IN UNIT DUCT
- PROPOSED LOCATION FOR RELOCATED OR NEW GUIDANCE SIGN
- PROPOSED LOCATION FOR RELOCATED OR NEW L-861T MITL (BLUE)
- ☉ PROPOSED LOCATION FOR RELOCATED OR NEW L-861 MIRL (WHITE / YELLOW OR WHITE / WHITE)
- ☉ PROPOSED LOCATION FOR RELOCATED OR NEW L-861E MIRL (GREEN / RED OR RED / GREEN)
- ☉ PROPOSED LOCATION FOR RELOCATED OR NEW L-862 HIRLQ (WHITE / YELLOW OR WHITE / WHITE)
- PROPOSED L-867 SPLICE OR TRANSFORMER CAN
- PROPOSED CONDUIT OR DUCT BANK
- PROPOSED ELECTRICAL CIRCUIT, 1/C, #8, EPR CABLE IN 1" HDPE UNIT DUCT
- PROPOSED ELECTRICAL CIRCUIT, 2/C, #8, EPR CABLES IN 1-1/2" HDPE UNIT DUCT

NUMBERED LEGEND

- 1 DISCONNECT EXTG. T-D CRT CABLES & UNIT DUCTS AT THESE LOCATIONS. START / END PROPOSED T-D CRT RELOCATION AT THESE LOCATIONS.
- 2 RELOCATE OR REMOVE EXISTING L-861T MITL UNITS OR SPLICE CANS. SUPPLY & INSTALL NEW GROUNDING ROD WITH EACH RELOCATION PER DETAIL. INCLUDE GROUNDING ROD & WIRE COSTS IN RELOCATION UNIT PRICE.
- 3 PROPOSED LOCATION FOR NEW GUIDANCE SIGN. SEE SHEET 46 FOR MORE INFORMATION.
- 4 PROPOSED 2-WAY DUCT BANK (4" ID CONDUITS) TO A POINT 5' BEYOND THE EDGE OF THE PROPOSED PAVEMENT.
- 5 END RELOCATED 2/C T-D CRT HOMERUN CABLE FROM LOCATION [13] AT THIS LOCATION.
- 6 EXISTING GUIDANCE SIGNS TO BE REMOVED. SEE SHEET 46 FOR MORE INFORMATION.
- 7 EXISTING DUCT BANK TO BE REMOVED.
- 8 PROPOSED 2/C T-E CRT CABLE BETWEEN LOCATIONS [8A] & [20] GOING THROUGH LIGHT CAN AT LOCATION [18]. HOLD SIGN POWER SUPPLY (ADDITIVE ALTERNATE 1 CABLE).
- 9 DISCONNECT EXTG. R 5-23 CRT CABLES & UNIT DUCTS AT THESE LOCATIONS. START / END PROPOSED R 5-23 CRT RELOCATIONS AT THESE LOCATIONS.
- 10 DISCONNECT EXTG. T-E CRT CABLES & UNIT DUCTS AT THESE LOCATIONS. ADJUST ELEVATION AND START / END PROPOSED T-E CRT RELOCATION AT THESE LOCATIONS. REUSE EXTG. T-E CRT CABLE TO SOUTHEAST OF THESE CANS.
- 11 ADJUST ELEVATION & REPLACE PANELS IN EXTG. GUIDANCE SIGN. SEE SHEET 46 FOR MORE INFORMATION.

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RUNWAY 9-27



NUMBERED LEGEND

- SEE SHEET 42 FOR NUMBERED LEGEND ITEMS [1] THROUGH [11].
- [12] DISCONNECT EXTG. T-E CRT CABLES & UNIT DUCTS AT THESE LOCATIONS. START / END PROPOSED T-E CRT RELOCATION AT THESE LOCATIONS.
 - [13] DISCONNECT EXTG. T-D CRT CABLES (2 EA. 1/C) & UNIT DUCTS AT THIS LOCATION. START PROPOSED T-D CRT HOMERUN CABLE (LOCATION [5] TO [13]) AT THIS LOCATION.
 - DISCONNECT EXTG. T-K CRT 1 HOMERUN CABLE / UNIT DUCT AND EXTG. T-K CRT 2 HOMERUN CABLE / UNIT DUCT AT THIS LOCATION. START PROPOSED TXWY T-K CRT 1 / T-K CRT 2 HOMERUN CABLE RELOCATIONS (LOCATION [13] TO [16]) AT THIS LOCATION.
 - [14] DISCONNECT EXTG. R9-27 CRT 1 CABLE & UNIT DUCT AT THESE LOCATIONS. START / END PROPOSED R9-27 CRT 1 RELOCATION AT THESE LOCATIONS.
 - [15] REMOVE EXISTING IN-PAVEMENT L-850C SEMI-FLUSH RUNWAY EDGE LIGHT & 2" G.S. CONDUIT. INCLUDE COSTS FOR CONDUIT REMOVAL IN LIGHT REMOVAL UNIT PRICE. INSTALL PROPOSED L-862 HIRLQ ELEVATED EDGE LIGHT UNIT (WHITE / WHITE) AT THIS LOCATION.

SYMBOL LEGEND

- EXISTING L-861T MITL OR L-861 MIRL
- EXISTING L-862 HIRLQ
- EXISTING L-850C SEMI-FLUSH RUNWAY EDGE LIGHT
- EXISTING L-867 SPLICE OR TRANSFORMER CAN
- EXISTING GUIDANCE SIGN
- EXISTING STORM OR SANITARY MANHOLE
- EXISTING UNDERDRAINS/STORM OR SANITARY SEWERS
- EXISTING CONDUIT OR DUCT BANK
- EXISTING ELECTRICAL CIRCUIT, CABLE IN UNIT DUCT
- PROPOSED LOCATION FOR RELOCATED OR NEW GUIDANCE SIGN
- PROPOSED LOCATION FOR RELOCATED OR NEW L-861T MITL (BLUE)
- PROPOSED LOCATION FOR RELOCATED OR NEW L-861 MIRL (WHITE / YELLOW OR WHITE / WHITE)
- PROPOSED LOCATION FOR RELOCATED OR NEW L-862 HIRLQ (WHITE / YELLOW OR WHITE / WHITE)
- PROPOSED CONDUIT OR DUCT BANK
- PROPOSED ELECTRICAL CIRCUIT, 1/C, #8, EPR CABLE IN 1" HDPE UNIT DUCT
- PROPOSED ELECTRICAL CIRCUIT, 2/C, #8, EPR CABLES IN 1-1/2" HDPE UNIT DUCT

GENERAL NOTES:

1. SEE SHEET 49 FOR ELECTRICAL GENERAL NOTES.
2. SEE SHEETS 49-52 FOR ELECTRICAL DETAILS
3. SEE SHEET 46 FOR SIGN DETAILS.
4. ALL EXISTING ELECTRICAL CIRCUITS TO REMAIN ACTIVE AT ALL TIMES. CONTRACTOR TO PROVIDE, INSTALL, AND MAINTAIN TEMPORARY ABOVE GROUND JUMPER CABLES AS REQUIRED TO PROVIDE CIRCUIT CONTINUITY IN ALL CIRCUITS AT ALL TIMES DURING CONSTRUCTION. COSTS TO BE INCLUDED IN THE LIGHTING CONTRACT UNIT PRICES. LENGTH OF JUMPER CABLES SHALL NOT BE MEASURED FOR PAYMENT AND / OR PAYED FOR.
5. DO NOT DISTURB EXISTING ELECTRICAL CABLES UNTIL REPLACEMENT CABLES OR TEMPORARY JUMPER CABLES ARE IN SERVICE.

RIGHT SIDE EDGE LIGHTS,
4 EACH SPACED AT 22D
30' 00" = 4 EACH SPACED
AT 35.12' (CHORD LENGTH),
90' R TO CENTER OF LIGHTS

LEFT SIDE EDGE LIGHTS,
4 EACH SPACED AT 22D
30' 00" = 4 EACH SPACED
AT 35.12' (CHORD LENGTH),
90' R TO CENTER OF LIGHTS

NUMBERED LEGEND

- SEE SHEETS 42 & 43 FOR NUMBERED LEGEND ITEMS 1 THROUGH 15.
- 16 END T-K CRT 1 & T-K CRT 2 HR CABLE REPLACEMENTS IN PROPOSED SPLICE CAN AT THIS LOCATION.
 - 17 START / END SOUTH R 10-28 CIRCUIT CABLE AT THIS LOCATION. FUTURE CABLE TO WEST OF THIS CAN BY OTHERS.
 - 18 START / END NORTH R 10-28 CIRCUIT CABLE AT THIS LOCATION DO NOT CONNECT PROPOSED RWY 10-28 L-861 MIRL UNIT TO T-E CIRCUIT.
 - 19 SEE DETAIL BELOW FOR CABLE ROUTING.
 - 20 PROPOSED LOCATION FOR NEW GUIDANCE SIGN. SEE SHEET 46 FOR MORE INFO.
 - 21 START / END NORTH R 10-28 CIRCUIT CABLE AT THIS 3-WAY LIGHT BASE. FUTURE CABLE TO EAST OF THIS CAN BY OTHERS.
 - 22 START / END SOUTH R 10-28 CIRCUIT CABLE AT THIS 3-WAY LIGHT BASE. FUTURE CABLE TO EAST OF THIS CAN BY OTHERS.
 - 23 EXISTING FAA CABLE AND PROPOSED ONE WAY DUCT BANK. EXISTING FAA CABLE TO BE LOWERED AS REQUIRED. USE 4" SPLIT DUCT IN PROPOSED ONE WAY DUCT BANK UNDER PROPOSED PAVEMENT. CABLES TO BE LOWERED TO A POINT THAT IS 36" BELOW THE PROPOSED FINAL GRADE.

- 24 EXISTING MAA FIBER OPTIC CABLE IN 2" CONDUIT AND PROPOSED ELECTRICAL MANHOLES (TWO EACH). LOWER EXTG. CABLE IN CONDUIT TO A POINT THAT IS 36" BELOW THE PROPOSED FINAL GRADE. REMOVE EXTG. HANDHOLE AND INSTALL SPLIT DUCT AROUND CABLE WHERE HANDHOLE REMOVED. CONCRETE ENCASE CONDUIT UNDER PROPOSED PAVEMENT PER UG DUCT (ONE WAY) DETAIL. INSTALL REPLACEMENT MANHOLES AROUND CABLE AT LOCATIONS THAT ARE 5' BEYOND THE PROPOSED RWY 10-28 SAFETY AREA.
- 25 REMOVE CABLES TO BE ABANDONED FROM EXTG. DUCT.
- 26 RELOCATE OR REMOVE EXISTING L-861 MIRL UNITS OR SPLICE CANS. SUPPLY & INSTALL NEW GROUNDING ROD WITH EACH RELOCATION PER DETAIL. INCLUDE GROUNDING ROD & WIRE COSTS IN RELOCATION UNIT PRICE.
- 27 ADJUST ELEVATION OF EXTG. SIGN, LIGHT, OR SPLICE CAN

QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE II - MIDDLE PAVING
ILL. MLI-3854, QU008
SHEET 44 OF 82



GENERAL NOTES:

1. SEE SHEET 49 FOR ELECTRICAL GENERAL NOTES.
2. SEE SHEETS 49-52 FOR ELECTRICAL DETAILS
3. SEE SHEET 46 FOR SIGN DETAILS.
4. ALL EXISTING ELECTRICAL CIRCUITS TO REMAIN ACTIVE AT ALL TIMES. CONTRACTOR TO PROVIDE, INSTALL, AND MAINTAIN TEMPORARY ABOVE GROUND JUMPER CABLES AS REQUIRED TO PROVIDE CIRCUIT CONTINUITY IN ALL CIRCUITS AT ALL TIMES DURING CONSTRUCTION. COSTS TO BE INCLUDED IN THE LIGHTING CONTRACT UNIT PRICES. LENGTH OF JUMPER CABLES SHALL NOT BE MEASURED FOR PAYMENT AND / OR PAYED FOR.
5. DO NOT DISTURB EXISTING ELECTRICAL CABLES UNTIL REPLACEMENT CABLES OR TEMPORARY JUMPER CABLES ARE IN SERVICE.

PROPOSED LOCATIONS FOR RUNWAY 10-28 EDGE LIGHTS*

REFERENCE NUMBER	LOCATION	TYPE	REMARK	ADDITIVE ALTERNATE
54 & 55	636+13.50, LT. & RT. 60'	L-861 MIRL	W FACE = YELLOW & E FACE = WHITE	1
56 & 57	638+10.50, LT. & RT. 60'	L-861 MIRL	W FACE = YELLOW & E FACE = WHITE	1
58	639+32.50, LT. 60'	L-861 MIRL	W FACE = YELLOW & E FACE = WHITE	1
59	640+54.50, LT. 60'	L-861 MIRL	W FACE = YELLOW & E FACE = WHITE	1
60	642+10.50, RT. 60'	L-861 MIRL	W FACE = YELLOW & E FACE = WHITE	2
61	643+32.50, RT. 60'	L-861 MIRL	W FACE = YELLOW & E FACE = WHITE	2
62 & 63	644+54.50, LT. & RT. 60'	L-861 MIRL	W FACE = YELLOW & E FACE = WHITE	2

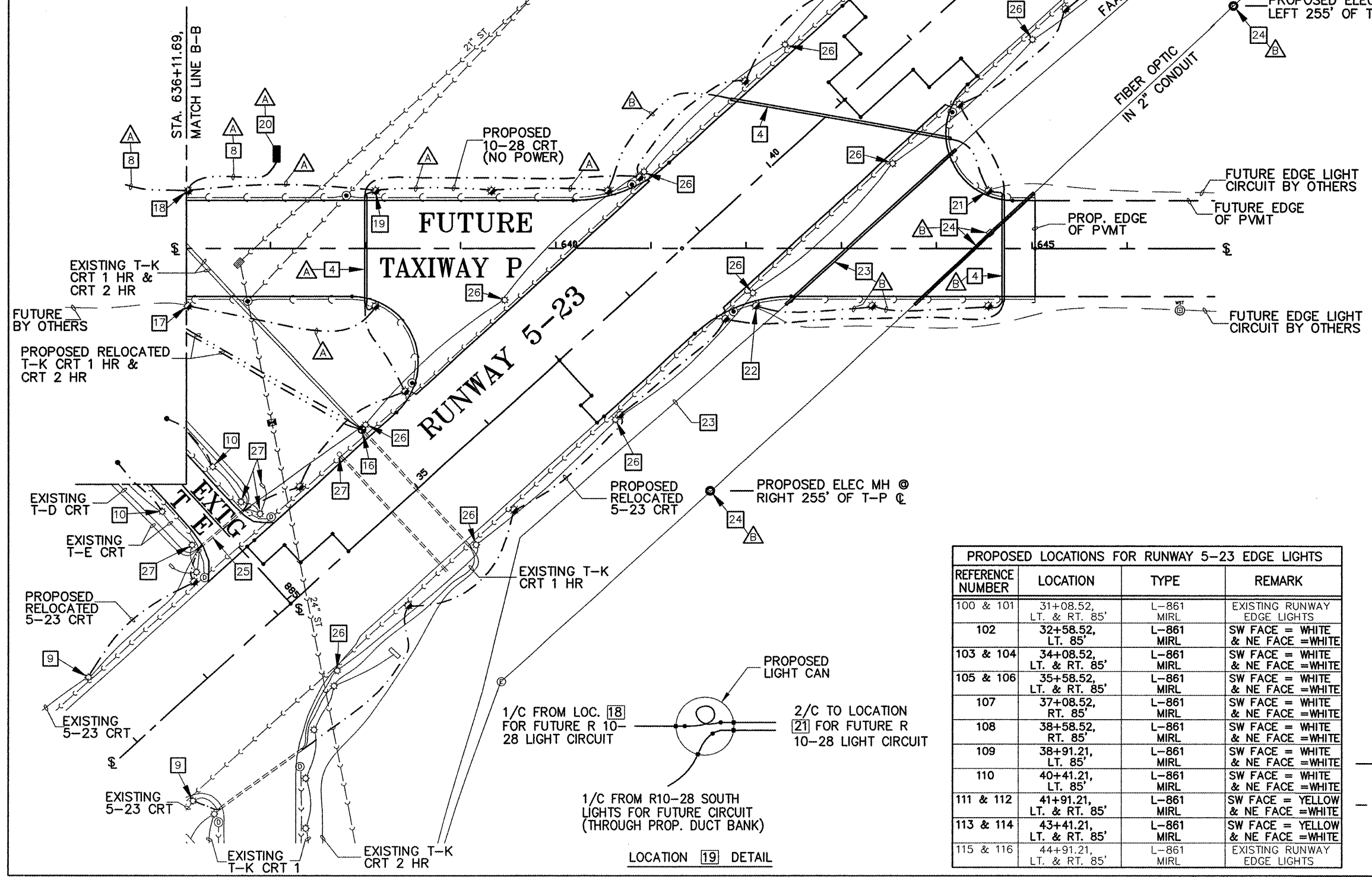
* = DO NOT CONNECT RWY 10-28 EDGE LIGHTS TO AN ACTIVE CIRCUIT. THESE LIGHTS ARE TO REMAIN OFF.

SYMBOL LEGEND

- EXISTING L-861T MITL OR L-861 MIRL
- EXISTING L-862 HIRLQ
- EXISTING L-850C SEMI-FLUSH RUNWAY EDGE LIGHT
- EXISTING L-867 SPLICE OR TRANSFORMER CAN
- EXISTING GUIDANCE SIGN
- EXISTING STORM OR SANITARY MANHOLE
- EXISTING UNDERDRAINS/STORM OR SANITARY SEWERS
- EXISTING CONDUIT OR DUCT BANK
- EXISTING ELECTRICAL CIRCUIT, CABLE IN UNIT DUCT
- PROPOSED LOCATION FOR RELOCATED OR NEW GUIDANCE SIGN
- PROPOSED LOCATION FOR RELOCATED OR NEW L-861T MITL (BLUE)
- PROPOSED LOCATION FOR RELOCATED OR NEW L-861 MIRL (WHITE / YELLOW OR WHITE / WHITE)
- PROPOSED LOCATION FOR RELOCATED OR NEW L-861E MIRL (GREEN / RED OR RED / GREEN)
- PROPOSED LOCATION FOR RELOCATED OR NEW L-862 HIRLQ (WHITE / YELLOW OR WHITE / WHITE)
- PROPOSED L-867 SPLICE OR TRANSFORMER CAN
- PROPOSED CONDUIT OR DUCT BANK
- PROPOSED ELECTRICAL CIRCUIT, 1/C, #8, EPR CABLE IN 1" HDPE UNIT DUCT
- PROPOSED ELECTRICAL CIRCUIT, 2/C, #8, EPR CABLES IN 1-1/2" HDPE UNIT DUCT
- ADDITIVE ALTERNATE 1 ITEM
- ADDITIVE ALTERNATE 2 ITEM

PROPOSED LOCATIONS FOR RUNWAY 5-23 EDGE LIGHTS

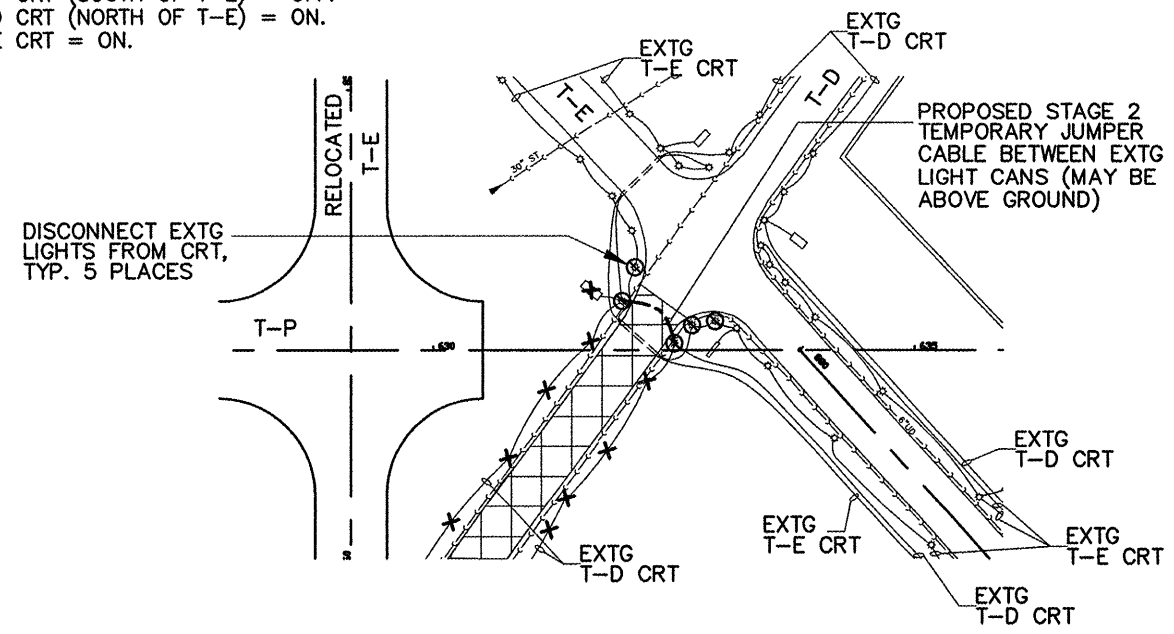
REFERENCE NUMBER	LOCATION	TYPE	REMARK
100 & 101	31+08.52, LT. & RT. 85'	L-861 MIRL	EXISTING RUNWAY EDGE LIGHTS
102	32+58.52, LT. 85'	L-861 MIRL	SW FACE = WHITE & NE FACE = WHITE
103 & 104	34+08.52, LT. & RT. 85'	L-861 MIRL	SW FACE = WHITE & NE FACE = WHITE
105 & 106	35+58.52, LT. & RT. 85'	L-861 MIRL	SW FACE = WHITE & NE FACE = WHITE
107	37+08.52, RT. 85'	L-861 MIRL	SW FACE = WHITE & NE FACE = WHITE
108	38+58.52, RT. 85'	L-861 MIRL	SW FACE = WHITE & NE FACE = WHITE
109	38+91.21, LT. 85'	L-861 MIRL	SW FACE = WHITE & NE FACE = WHITE
110	40+41.21, LT. 85'	L-861 MIRL	SW FACE = WHITE & NE FACE = WHITE
111 & 112	41+91.21, LT. & RT. 85'	L-861 MIRL	SW FACE = YELLOW & NE FACE = WHITE
113 & 114	43+41.21, LT. & RT. 85'	L-861 MIRL	SW FACE = YELLOW & NE FACE = WHITE
115 & 116	44+91.21, LT. & RT. 85'	L-861 MIRL	EXISTING RUNWAY EDGE LIGHTS



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STAGE 2 NOTES:

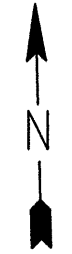
- 1. SEE SAFETY PLAN FOR ADDITIONAL DETAILS.
- 2. T-D CRT (SOUTH OF T-E) = OFF.
- 3. T-D CRT (NORTH OF T-E) = ON.
- 4. T-E CRT = ON.



STAGE 2 TEMPORARY JUMPER CABLE

SYMBOL LEGEND

- EXISTING EDGE LIGHT
- EXISTING L-867 SPLICE OR TRANSFORMER CAN
- EXISTING GUIDANCE SIGN
- EXISTING STORM OR SANITARY MANHOLE
- EXISTING UNDERDRAINS/STORM OR SANITARY SEWERS
- EXISTING CONDUIT OR DUCT BANK
- EXISTING ELECTRICAL CIRCUIT, CABLE IN UNIT DUCT
- PROPOSED LOCATION FOR RELOCATED OR NEW L-861T MITL (BLUE)
- PROPOSED CONDUIT OR DUCT BANK
- PROPOSED ELECTRICAL CIRCUIT, 1/C, #8, EPR CABLE IN 1" HDPE UNIT DUCT
- PROPOSED TEMPORARY ELECTRICAL JUMPER CABLE, 1/C, #8
- OR NO POWER TO LIGHT, SIGN, AND / OR CABLE
- DISCONNECT EDGE LIGHT FROM CIRCUIT



NOTES FOR ALL STAGES:

- 1. REMOVE TEMPORARY JUMPER CABLES WHEN NO LONGER NEEDED.
- 2. ADDITIONAL TEMPORARY JUMPER CABLES MAY BE REQUIRED DEPENDING ON THE PROPOSED CONSTRUCTION SEQUENCING.
- 3. COST FOR ITEMS SHOWN ON THIS SHEET SHALL BE INCLUDED IN THE LIGHTING CONTRACT UNIT PRICES. THESE ITEMS SHALL NOT BE MEASURED FOR PAYMENT AND / OR PAYED FOR UNLESS NOTED OTHERWISE.

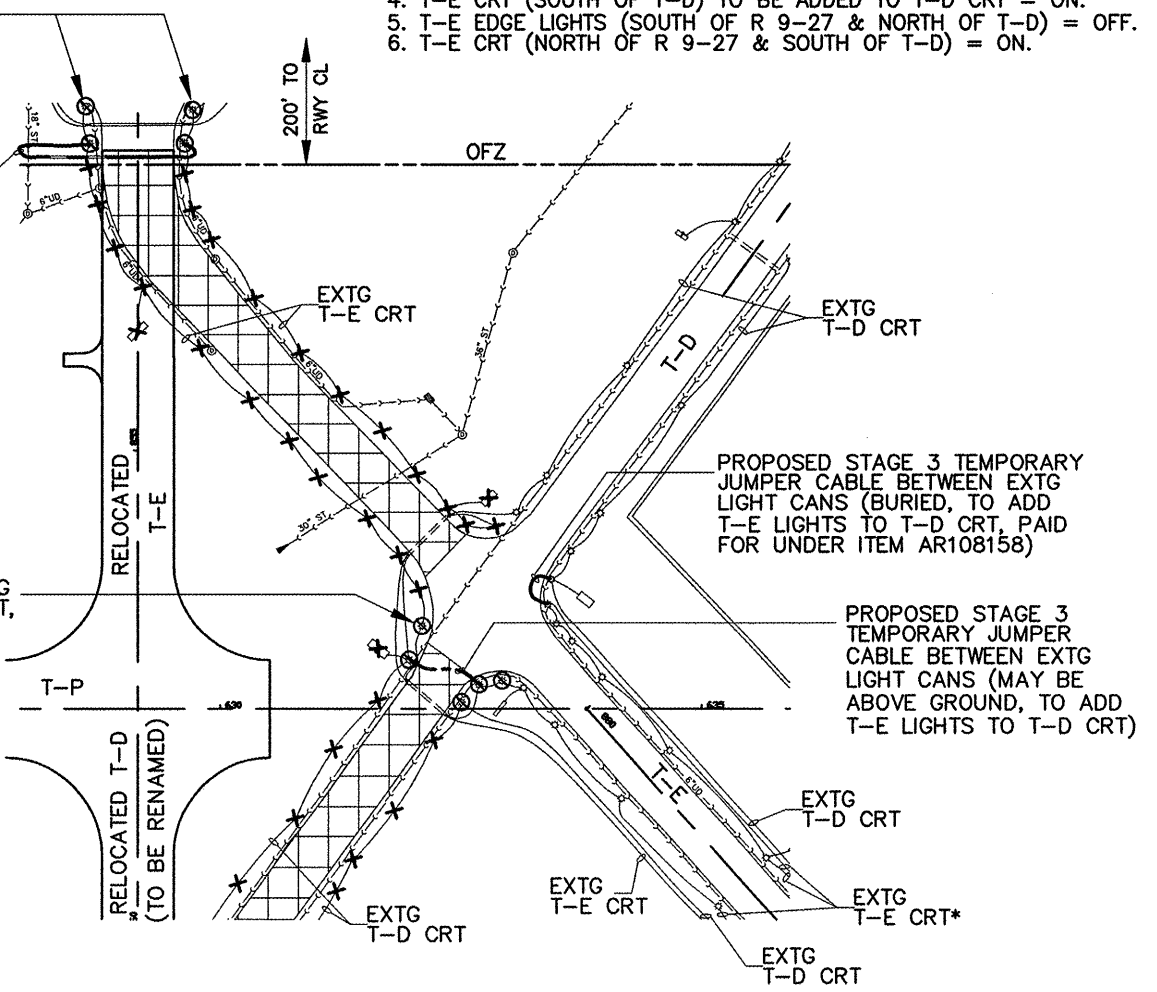
STAGE 3 NOTES:

- 1. SEE SAFETY PLAN FOR ADDITIONAL DETAILS.
- 2. T-D CRT (SOUTH OF T-E) = OFF.
- 3. T-D CRT (NORTH OF T-E) = ON.
- 4. T-E CRT (SOUTH OF T-D) TO BE ADDED TO T-D CRT = ON.
- 5. T-E EDGE LIGHTS (SOUTH OF R 9-27 & NORTH OF T-D) = OFF.
- 6. T-E CRT (NORTH OF R 9-27 & SOUTH OF T-D) = ON.

DISCONNECT EXTG LIGHTS FROM CRT, TYP. ALL T-E LIGHTS BETWEEN R9-27 AND TEMPORARY JUMPER (8 LOCATIONS)

PROPOSED STAGE 3 TEMPORARY JUMPER CABLE BETWEEN EXTG LIGHT CANS (BURIED & THROUGH EXTG DUCT, PAID FOR UNDER ITEM AR108158)

DISCONNECT EXTG LIGHTS FROM CRT, TYP. 5 PLACES



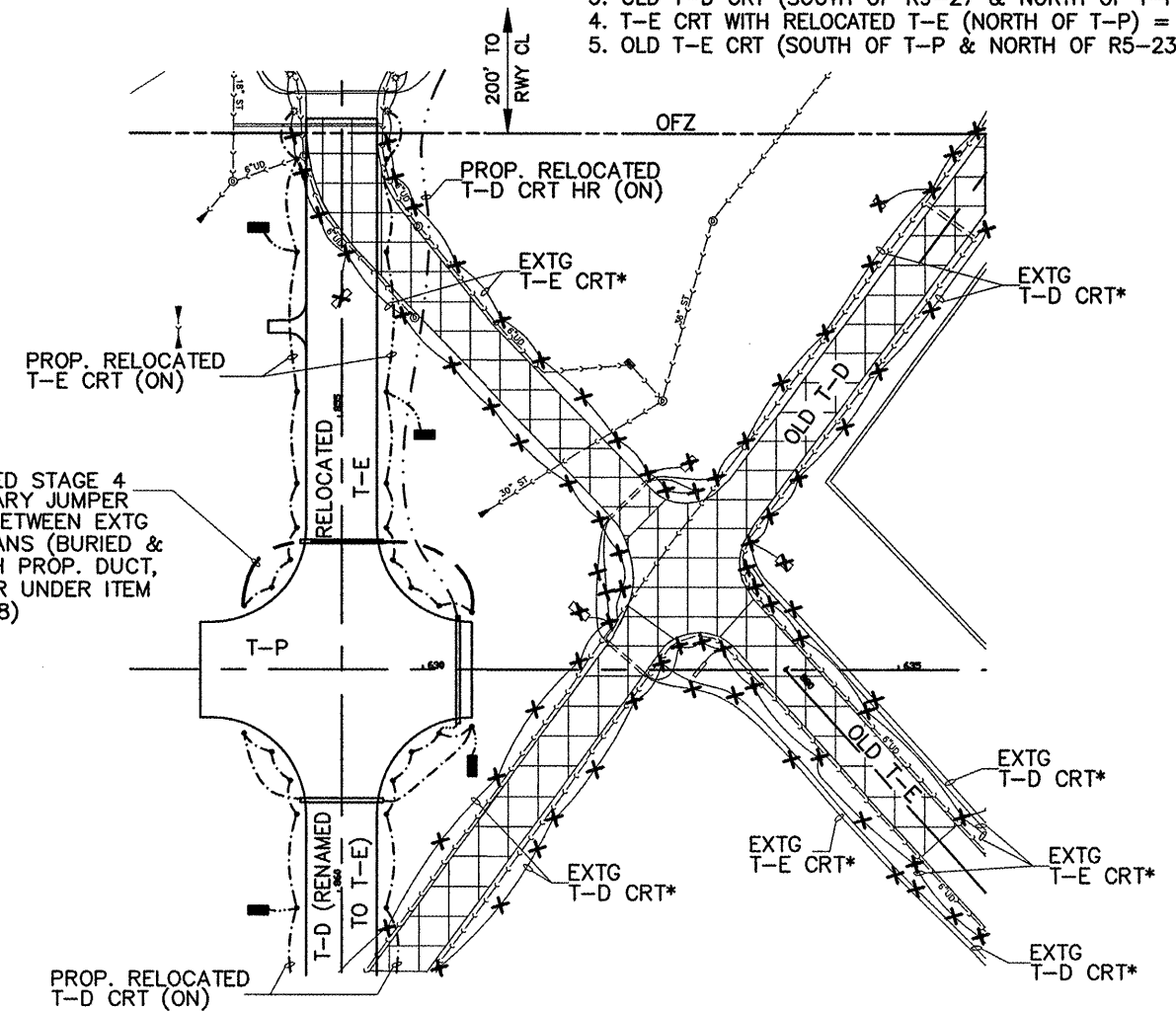
STAGE 3 TEMPORARY JUMPER CABLES

* = ADD TO EXTG T-D CRT

STAGE 4 NOTES:

- 1. SEE SAFETY PLAN FOR ADDITIONAL DETAILS.
- 2. RELOCATED T-D CRT (SOUTH OF T-P) = ON.
- 3. OLD T-D CRT (SOUTH OF R9-27 & NORTH OF T-P) = OFF.
- 4. T-E CRT WITH RELOCATED T-E (NORTH OF T-P) = ON.
- 5. OLD T-E CRT (SOUTH OF T-P & NORTH OF R5-23) = OFF.

PROPOSED STAGE 4 TEMPORARY JUMPER CABLE BETWEEN EXTG LIGHT CANS (BURIED & THROUGH PROP. DUCT, PAID FOR UNDER ITEM AR108158)

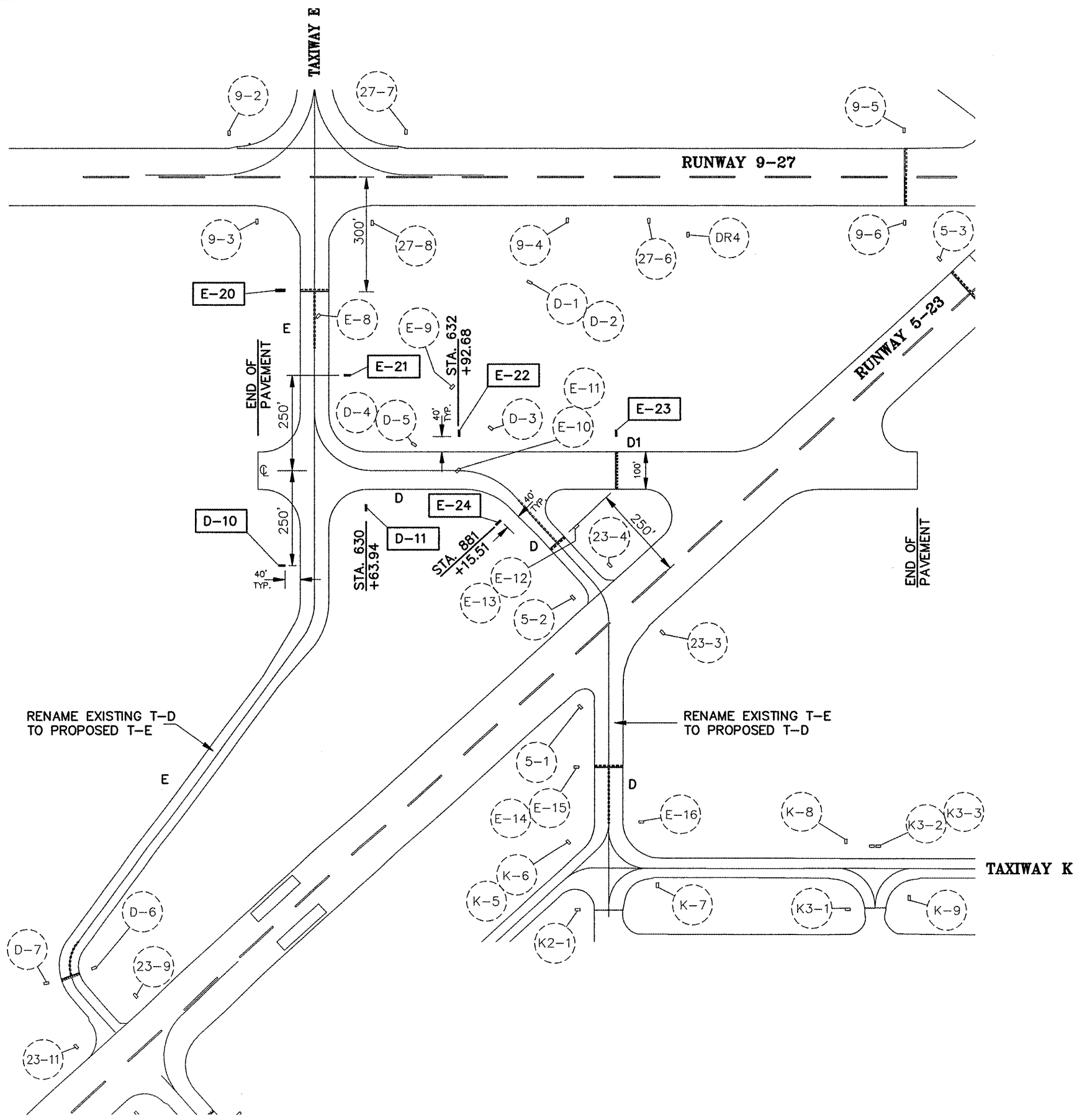


STAGE 4 TEMPORARY JUMPER CABLES

* = OFF

TEMPORARY JUMPER CABLES FOR PROJECT STAGING

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- LEGEND:**
- ☆ EXISTING EDGE LIGHT
 - ▭ EXISTING GUIDANCE SIGN
 - 23-4 EXISTING SIGN NUMBER AT EXISTING SIGN LOCATION
 - 23-4 EXISTING SIGN NUMBER AT RELOCATED OR NEW SIGN LOCATION
 - ▭ PROPOSED LOCATION FOR RELOCATED OR NEW GUIDANCE SIGN
 - ▭ E-20 PROPOSED NEW SIGN NUMBER

GUIDANCE SIGN GENERAL NOTES:

- 1) "LEGEND" COLUMN IN TABLE (FOUND ON SHTS 47 & 48) INDICATES NUMBER OF PANELS (MODULES), SPECIFIC CHARACTERS PER PANEL AND BLANK PANELS. SIGNS ARE TO BE MANUFACTURED AND SUPPLIED AS SHOWN IN THE LEGEND COLUMN UNLESS OTHERWISE APPROVED BY THE MAA.
- 2) ALL SIGNS SHALL COMPLY WITH FAA ADVISORY CIRCULAR 150/5345-44E.
- 3) CONTRACTOR SHALL VERIFY ALL BASE DIMENSIONS WITH MANUFACTURER PRIOR TO INSTALLATION OF ANCHOR BOLTS.
- 4) THE CONTRACTOR SHALL DISASSEMBLE EXISTING GUIDANCE SIGNS AS REQUIRED TO INSTALL PROPOSED NEW SIGN PANELS AND REUSE EXISTING PANELS AS INDICATED IN TABLE. EXISTING PANELS REMOVED MAY ONLY BE REUSED, IF THE CONDITION OF THE PANEL IS ACCEPTABLE TO THE MAA AND THE RESIDENT ENGINEER. THE CONTRACTOR SHALL REPLACE ALL DAMAGED EXISTING PANELS AND EQUIPMENT AS REQUIRED BY THE RESIDENT ENGINEER. ONCE THE SIGNS HAVE BEEN REBUILT, THE CONTRACTOR SHALL SEAL SIGNS AND RETURN THE EXISTING SIGNS TO THEIR ORIGINAL CONDITION. EXISTING PANELS REMOVED AND NOT REUSED SHALL BE DELIVERED BY THE CONTRACTOR TO THE OWNER (MAA).
- 5) SIGNS SHALL BE DOUBLE FACED AS INDICATED IN TABLE (TYPE L-858Y, L-858R, L-858L, OR L-858B). SIGNS AND REPLACEMENT PANELS SHALL BE COMPATIBLE IN ALL RESPECTS WITH EACH OTHER AND WITH THE EXISTING SIGNS CURRENTLY IN PLACE AT THE QUAD CITY INTERNATIONAL AIRPORT. SIGNS AND REPLACEMENT PANELS SHALL BE LUMACURVE, OR APPROVED EQUAL.
- 6) "FACE" COLUMN INDICATES DIRECTION OF SIGN FACE.
- 7) SEE SHEET 49 - 52 FOR ELECTRICAL GENERAL NOTES AND ELECTRICAL DETAILS.
- 8) SEE SHEET 50 FOR SIGN DETAILS.
- 9) WHERE PROPOSED SIGNS ARE INSTALLED BESIDE EXISTING SIGNS PROVIDE 1' SPACE BETWEEN SIGNS.
- 10) THE CONTRACTOR SHALL SUPPLY AND INSTALL TWO NEW REGULATOR NAME PLATES IN THE EXISTING ELECTRICAL VAULT. REGULATOR # 11 (CURRENTLY LABELED T-D CRT) WILL BE RENAMED TO T-E CRT 2. REGULATOR # 14 (CURRENTLY LABELED T-E CRT) WILL BE RENAMED TO T-E CRT 1. NEW NAME PLATES TO MATCH TYPE AND STYLE OF THE EXISTING NAME PLATES. INFORMATION ON THE NEW NAME PLATES TO MATCH INFORMATION ON THE EXISTING NAME PLATES. EXACT WORDING ON THE NAME PLATES TO BE DETERMINED BY THE MAA IN THE FIELD AT THE TIME OF CONSTRUCTION.

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PERMANENT TAXIWAY D / TAXIWAY E GUIDANCE SIGN SCHEDULE
SIZE 3, STYLE 2, CLASS 2

SIGN NUMBER	FACE	EXISTING LEGEND	LETTERS BACKGROUND	PROPOSED LEGEND	LETTERS BACKGROUND	CIRCUIT	PAY ITEMS	REMARK
D-1 / D-2	NE SW	5 ↑ D 9-27	B/Y B/B B/B Y/B W/R W/R			T-D	AR125904	REMOVE
D-3	NE SW	D ← E →	B/Y B/B B/B B/B B/B B/B			T-D	AR125904	REMOVE
D-4 / D-5	NE SW	5 ↑ D ← E →	B/Y B/B B/B Y/B B/Y B/Y			T-D	AR125904	REMOVE
D-6	NW SE	D 23 - 5 D	Y/B W/R W/R Y/B B/B B/B	E 23 - 5 E	Y/B W/R W/R Y/B B/B B/B	T-D	AR801605	REPLACE 6 PANELS
D-7	NW SE	23 - 5 D D	W/R W/R Y/B B/B B/B Y/B	23 - 5 E E	W/R W/R Y/B B/B B/B Y/B	T-D	AR801605	REPLACE 6 PANELS
D-10	N S			E D →	B/B B/B B/B B/B Y/B B/Y	T-D	AR125443	PROPOSED NEW 3 CHARACTER SIGN
D-11	W E			D ← E →	B/B B/B B/B Y/B B/Y B/Y	T-D	AR125444	PROPOSED NEW 4 CHARACTER SIGN
E-8	NW SE	E E 9-27	Y/B B/B B/B Y/B W/R W/R			T-E	AR125904	REMOVE
E-9	NW SE	E ← D → 9-13 ↑	B/Y B/B B/B B/Y B/Y B/B			T-E	AR125904	REMOVE
E-10 / E-11	NW SE	E E ← D →	Y/B B/B B/B Y/B B/Y B/Y			T-E	AR125904	REMOVE
E-12 / E-13	NW SE	E 23 - 5 E	Y/B W/R W/R Y/B B/B B/B	D 23 - 5 D	Y/B W/R W/R Y/B B/B B/B	T-E	AR125944 & AR801605	ADJUST ELEVATION OF EXTG SIGN AND REPLACE 6 PANELS
E-14 / E-15	N S	E E 5-23	Y/B B/B B/B Y/B W/R W/R	D D 5-23	Y/B B/B B/B Y/B W/R W/R	T-K-1	AR801605	REPLACE 6 PANELS
E-16	N S	← K E K →	B/Y Y/B B/Y B/B B/B B/B	← K D K →	B/Y Y/B B/Y B/B B/B B/B	T-K-1	AR801605	REPLACE 6 PANELS
E-20	N S	E E 9-27	Y/B B/B B/B Y/B W/R W/R	E E 9-27	Y/B B/B B/B Y/B W/R W/R	T-E	AR125446	PROPOSED NEW 6 CHARACTER SIGN
E-21	N S	← D E	B/B B/Y Y/B B/B B/B B/B			T-E	AR125443	PROPOSED NEW 3 CHARACTER SIGN
E-22	W E	D D ↗ D	Y/B B/B Y/B Y/B B/B B/B			T-E	AR125444	PROPOSED NEW 4 CHARACTER SIGN
E-23	W E	D1 23 - 5 D1	Y/B W/R W/R Y/B B/B B/B			T-E	AS125448	PROPOSED NEW 8 CHARACTER SIGN
E-24	NW SE		B/B B/B B/B B/Y Y/B B/Y	↖ D D D1 ↘	B/B B/B B/B B/Y Y/B B/Y	T-E	AR125446	PROPOSED NEW 6 CHARACTER SIGN

NUMBER OF MODULES	TRANSFORMER WATTAGE
1	100
2	300
3	500
4	500
DISTANCE REMAINING	500

TRANSFORMERS SHALL BE 6.6/6.6 AMP.
* = OR AS REQUIRED BY SIGN MANUFACTURER.

N = NORTH
S = SOUTH
E = EAST
W = WEST
NW = NORTHWEST
SE = SOUTHEAST
NE = NORTHEAST
SW = SOUTHWEST

B/Y = BLACK LETTERS ON YELLOW BACKGROUND (TYPE L-858Y)
Y/B = YELLOW LETTERS ON BLACK BACKGROUND (TYPE L-858L)
B/B = BLACK BLANK PANEL
Y/Y = YELLOW BLANK PANEL
W/R = WHITE LETTERS ON RED BACKGROUND (TYPE L-858R)
W/B = WHITE LETTERS ON BLACK BACKGROUND (TYPE L-858B)

PERMANENT TAXIWAY D / TAXIWAY E GUIDANCE SIGN SCHEDULE CONTINUED
SIZE 3, STYLE 2, CLASS 2

SIGN NUMBER	FACE	EXISTING LEGEND	LETTERS BACKGROUND	PROPOSED LEGEND	LETTERS BACKGROUND	CIRCUIT	PAY ITEMS	REMARK
K-5 / K-6	NE	K	Y/B	K	Y/B	T-K-1	AR801605	REPLACE 10 PANELS
	SW	E K K2	B/Y	D K K2	B/Y			
K-7	W		B/B	NO CHANGE	NO CHANGE	T-K-1	AR801605	REPLACE 3 PANELS
	E	K K E	B/Y	K K D	B/Y			
K2-1	N		B/B	NO CHANGE	NO CHANGE	T-K-1	AR801605	REPLACE 4 PANELS
	S	K K2 E K	B/Y	K K2 D K	B/Y			
5-1	NE		B/B	NO CHANGE	NO CHANGE	T-K-1	AR801605	REPLACE 1 PANELS
	SW	E	B/Y	D	B/Y			
5-2	NE		B/B	NO CHANGE	NO CHANGE	T-E	AR801605	REPLACE 1 PANELS
	SW	E	B/Y	D	B/Y			
9-3	W	E	B/Y			T-E	NONE	NO CHANGE
	E	27	Y/B					
9-4	W	D	B/Y			T-D	AR125904	REMOVE
	E	27	Y/B					
23-3	NE	E	B/Y	D	B/Y	T-K-1	AR801605	REPLACE 1 PANELS
	SW		B/B	NO CHANGE	NO CHANGE			
23-4	NE	E	B/Y	D	B/Y	T-E	AR125944 & AR801605	ADJUST ELEVATION OF EXTG SIGN AND REPLACE 1 PANEL
	SW		B/B	NO CHANGE	NO CHANGE			
23-9	NE	D	B/Y	E	B/Y	T-D	AR801605	REPLACE 2 PANELS
	SW	5	Y/B	5	Y/B			
23-11	NE		B/B	NO CHANGE	NO CHANGE	T-D	AR801605	REPLACE 1 PANELS
	SW	D	B/Y	E	B/Y			
27-6	W	9	Y/B			T-D	AR125904	REMOVE
	E	D	B/Y					
27-8	W	9	Y/B			T-E	NONE	NO CHANGE
	E	E	B/Y					

NUMBER OF MODULES	TRANSFORMER WATTAGE
1	100
2	300
3	500
4	500
DISTANCE REMAINING	500

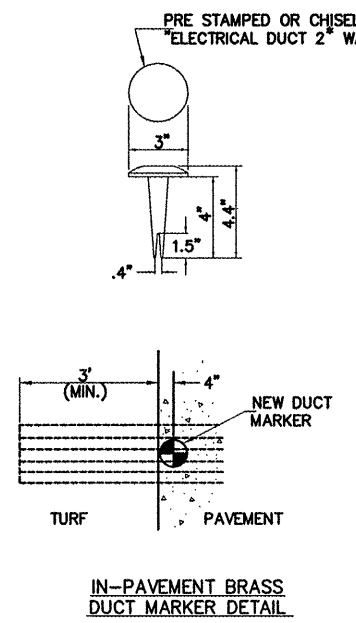
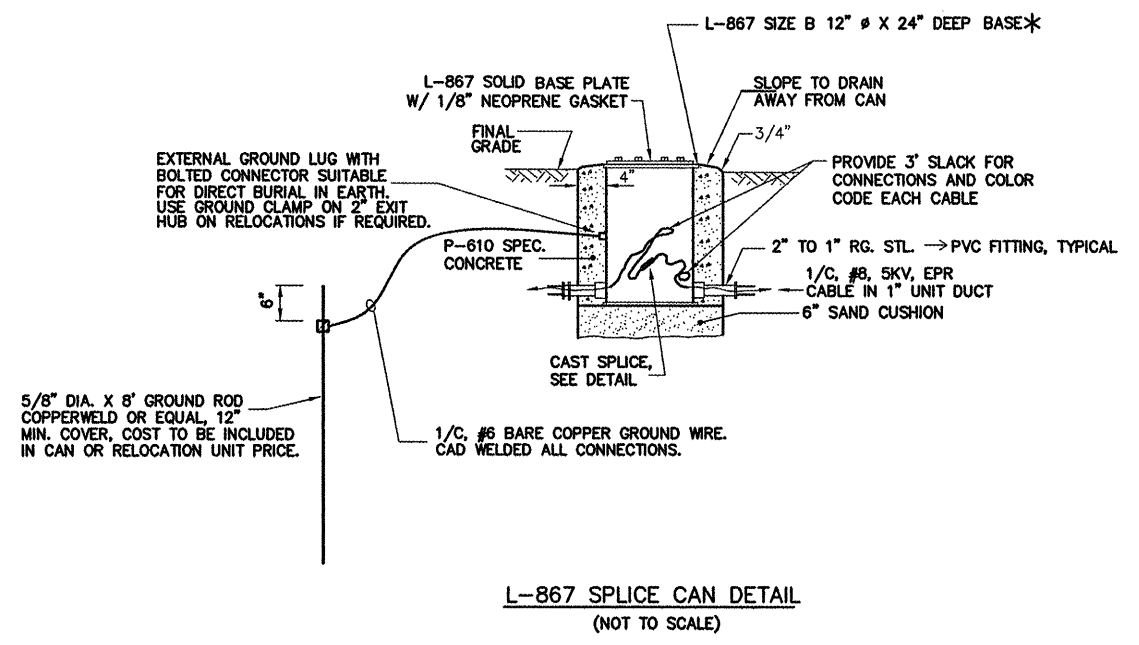
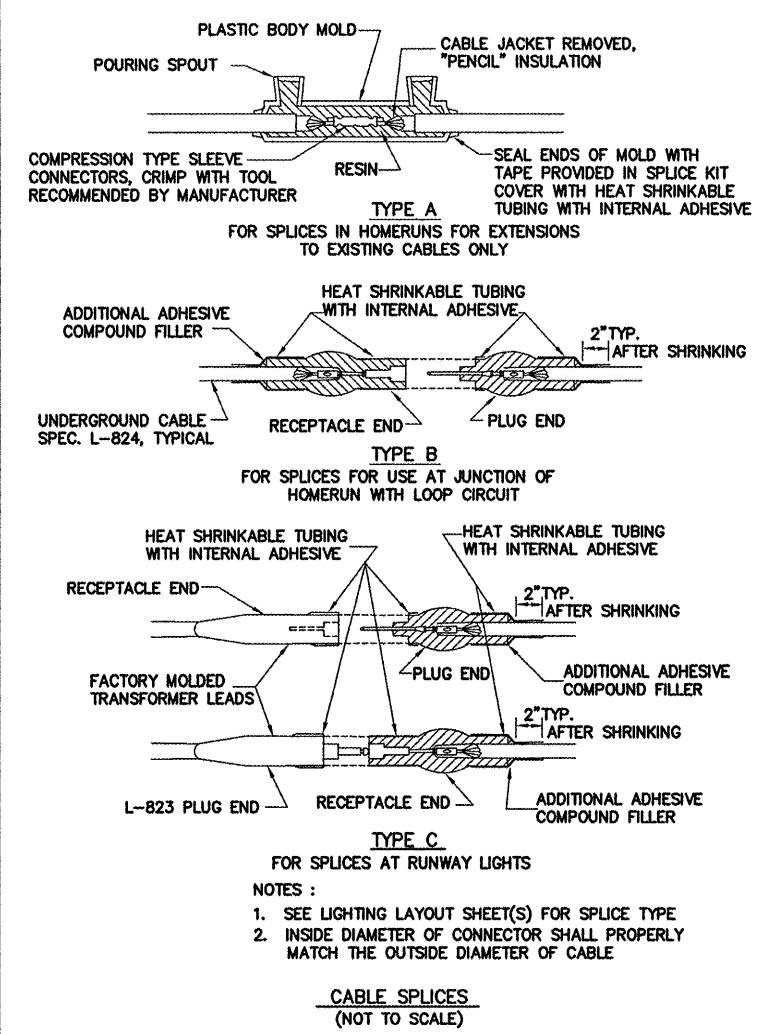
TRANSFORMERS SHALL BE 6.6/6.6 AMP.
* - OR AS REQUIRED BY SIGN MANUFACTURER.

N = NORTH
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NE = NORTHEAST
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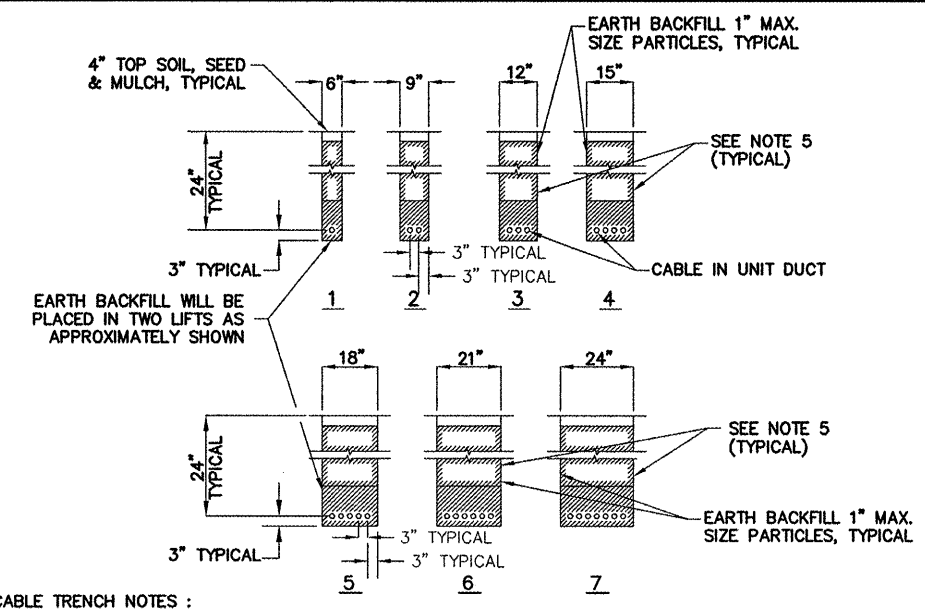
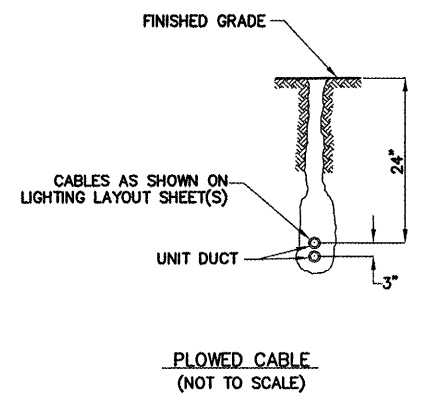
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Y/B = YELLOW LETTERS ON BLACK BACKGROUND (TYPE L-858L)
B/B = BLACK BLANK PANEL
Y/Y = YELLOW BLANK PANEL
W/R = WHITE LETTERS ON RED BACKGROUND (TYPE L-858R)
W/B = WHITE LETTERS ON BLACK BACKGROUND (TYPE L-858B)

GENERAL ELECTRICAL NOTES:

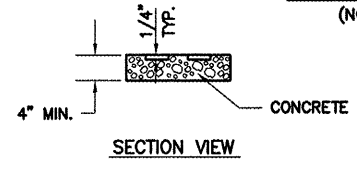
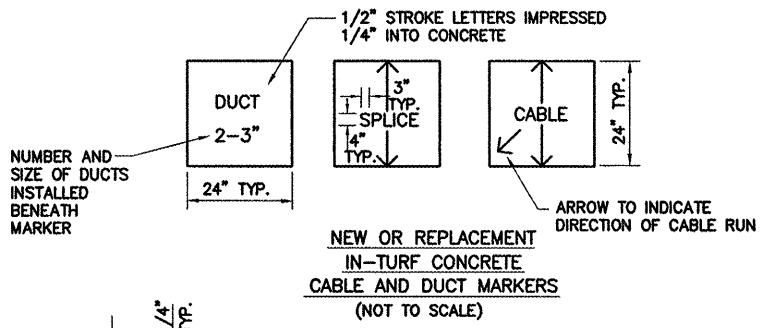
- ALL EXCAVATION SHALL BE DONE VERY CAREFULLY. EXCAVATION BY HAND DIGGING SHALL BE REQUIRED AROUND ALL EXISTING DUCT BANKS, SPLICE CANS, MANHOLES, AND EXISTING CABLES. MANY EXISTING ACTIVE UNDERGROUND CABLES, WHOSE EXACT LOCATIONS CANNOT BE DETERMINED, ARE FOUND IN THE PROJECT AREA. IN ORDER TO AVOID EXISTING UNDERGROUND CABLES, THE CONTRACTOR SHALL CONNECT A THUMPER TO ALL EXISTING CIRCUITS AFTER WHICH THEY SHALL BE STAKED IN ALL AREAS REQUIRING TRENCHING OR EXCAVATION. CONTRACTOR SHALL ALSO NOTE THAT LOW VOLTAGE, FAA CABLES ALSO RUN UNDERGROUND THROUGHOUT THESE AREAS. ANY CABLE DAMAGED SHALL BE REPAIRED OR REPLACED TO ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE.
- A MINIMUM OF 3 FEET OF SLACK SHALL BE PROVIDED IN THE CABLES AT EACH TRANSFORMER, CONNECTOR, OR SPLICE POINT. ALL CABLE SPLICES SHALL OCCUR IN MANHOLES, LIGHT WELLS OR SPLICE CANS, UNLESS NOTED OTHERWISE.
- THE ELECTRICAL INSTALLATION, AS A MINIMUM, SHALL MEET THE NATIONAL ELECTRICAL CODE AND LOCAL REGULATIONS.
- ALL MANUFACTURERS FOR SUPPLYING AIRPORT LIGHTING EQUIPMENT SHALL APPEAR ON THE CURRENT FAA "APPROVED AIRPORT EQUIPMENT" LIST FOUND IN AC 150/5345-53B. THE EQUIPMENT SHALL COMPLY WITH THE APPLICABLE CURRENT FAA ADVISORY CIRCULAR LISTED IN THE FAA "APPROVED AIRPORT EQUIPMENT" LIST FOUND IN AC 150/5345-2 (AIRPORTS ELECTRONIC BULLETIN BOARD NUMBER 14).
- THE CONTRACTOR SHALL ASCERTAIN THAT ALL LIGHTING SYSTEM COMPONENTS FURNISHED BY HIM (INCLUDING FAA APPROVED EQUIPMENT) ARE COMPATIBLE IN ALL RESPECTS WITH EACH OTHER AND THE REMAINDER OF THE NEW/EXISTING SYSTEM. ANY NONCOMPATIBLE COMPONENTS FURNISHED BY THIS CONTRACTOR SHALL BE REPLACED BY HIM AT NO ADDITIONAL COST TO THE AIRPORT SPONSOR WITH A SIMILAR UNIT, APPROVED BY THE ENGINEER (DIFFERENT MODEL OR DIFFERENT MANUFACTURER) THAT IS COMPATIBLE WITH THE REMAINDER OF THE AIRPORT LIGHTING SYSTEM.
- IF THE CONTRACTOR SELECTS TO FURNISH AND INSTALL AIRPORT LIGHTING EQUIPMENT WHICH REQUIRES ADDITIONAL WIRING, TRANSFORMERS, ADAPTERS, MOUNTINGS, ETC., BEYOND THAT SHOWN ON THE DRAWINGS AND/OR LISTED IN THE SPECIFICATIONS, THEN THE COST FOR THE ADDITIONAL ITEMS SHALL BE INCLUDED TO THE CONTRACT UNIT PRICES.
- THE CONTRACTOR INSTALLED EQUIPMENT (INCLUDING FAA APPROVED) SHALL NOT GENERATE ANY ELECTROMAGNETIC INTERFERENCE IN THE EXISTING AND/OR NEW COMMUNICATIONS, WEATHER, AIR NAVIGATION, AND AIR TRAFFIC CONTROL EQUIPMENT. ANY EQUIPMENT GENERATING SUCH INTERFERENCE SHALL BE REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST WITH THE EQUIPMENT MEETING THE APPLICABLE SPECIFICATIONS AND NOT GENERATING ANY INTERFERENCE.
- WHEN A SPECIFIC TYPE, STYLE, CLASS, ETC. OF FAA APPROVED EQUIPMENT IS SPECIFIED ONLY THAT TYPE, STYLE, CLASS, WILL BE ACCEPTABLE, EVEN THOUGH EQUIPMENT OF OTHER TYPES, STYLES, CLASSES, ETC. MAY BE FAA APPROVED.
- ALL CONCRETE FOR ELECTRICAL EQUIPMENT SHALL COMPLY WITH SPECIFICATION 610-STRUCTURAL PC CONCRETE 3500 PSI AT 28 DAYS, AIR ENTRAINED CONCRETE MIX SHALL BE USED.
- BASE MOUNTED BREAKABLE COUPLINGS SHALL NOT HAVE WEEP HOLES TO THE OUTSIDE. PLUGGED UP HOLES SHALL NOT BE ACCEPTABLE. IT SHALL HAVE A 1/4" DIAMETER OR EQUIVALENT OPENING FOR DRAINAGE FROM THE SPACE AROUND THE CONNECTOR INTO THE BASE.
- THE ELEVATION OF THE BREAKABLE COUPLING GROOVE SHALL NOT EXCEED 1 1/2" ABOVE THE EDGE OF THE COVER IN THE CASE OF A BASE MOUNTED COUPLING.
- ALL PERMANENT CABLE SPLICES SHALL OCCUR IN MANHOLES, LIGHT WELLS, OR SPLICE CANS, UNLESS NOTED OTHERWISE.
- MIMIC PANEL COLORS : RUNWAY R9-27 CIRCUIT 1 = WHITE, RUNWAY R5-23 CIRCUIT = WHITE, TAXIWAY D CIRCUIT = ORANGE, TAXIWAY E CIRCUIT = YELLOW, TAXIWAY F-2 CIRCUIT = LIME, AND SIGN CIRCUIT = WHITE.



NOTE: PAVING CONTRACTOR SHALL INSTALL NEW BRASS DUCT MARKERS IN THE PROPOSED PAVEMENT AT ALL LOCATIONS WHERE THE PROPOSED PAVEMENT CROSSES EXISTING OR PROPOSED ELECTRICAL DUCTS. COST OF DUCT MARKERS SHALL BE INCLUDED IN THE 401 AND / OR 501 CONTRACT UNIT PRICES.



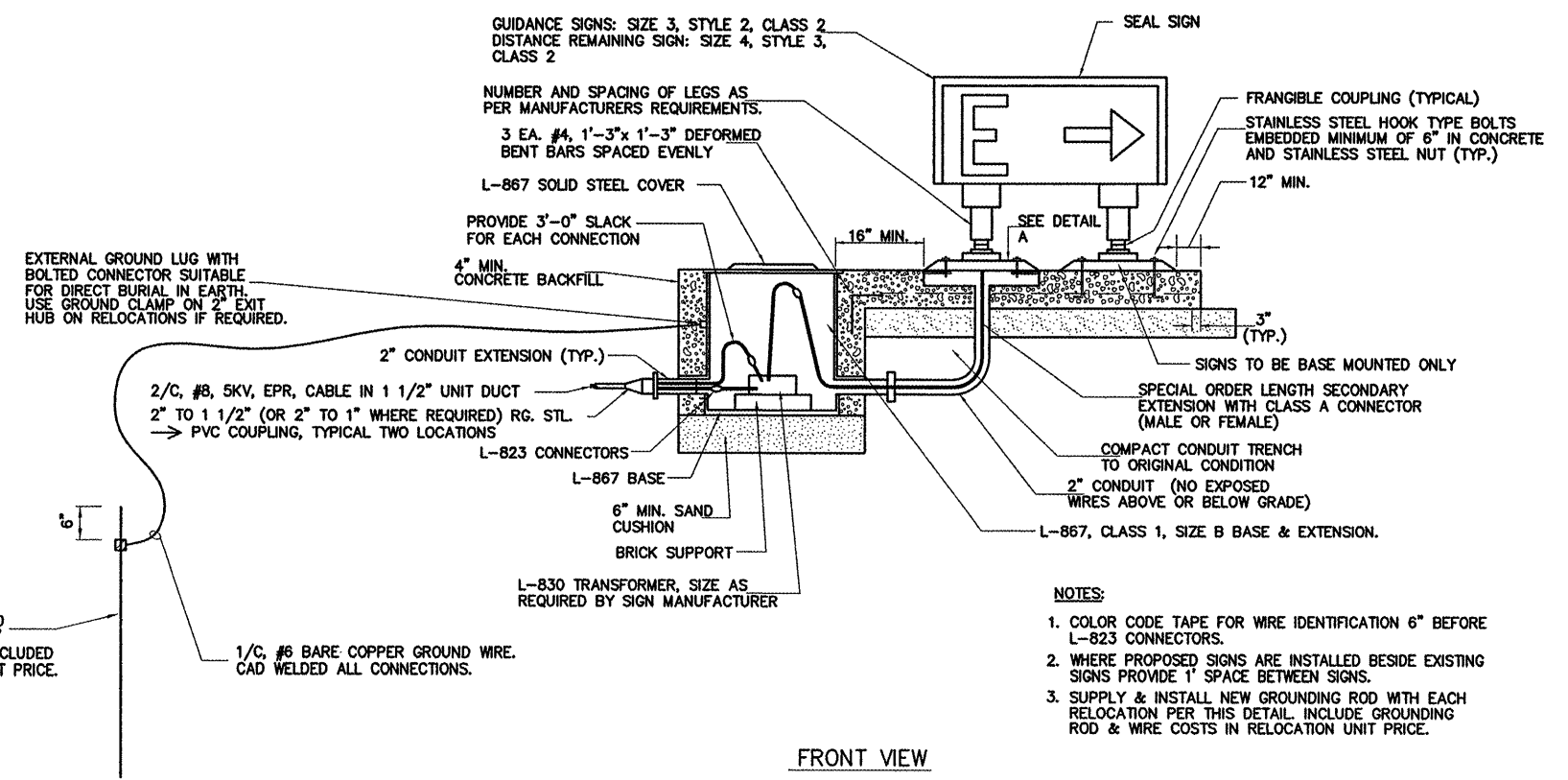
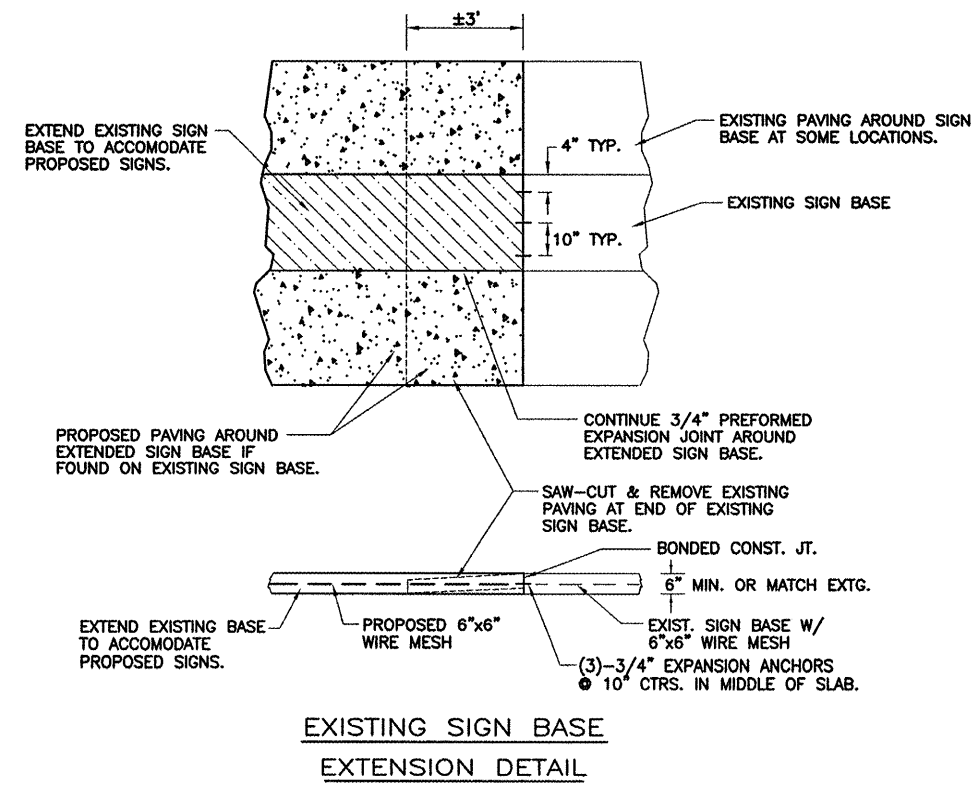
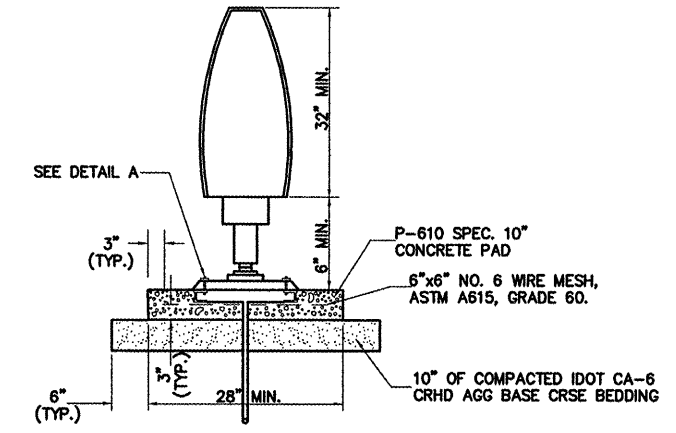
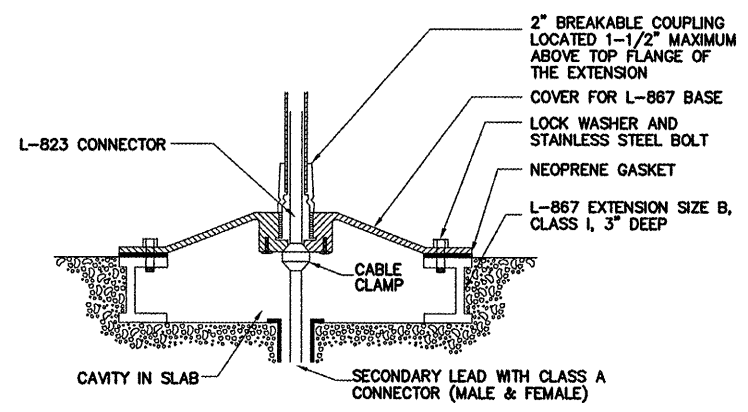
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- NOTES:
1. NEW MARKERS ARE REQUIRED ON ALL FAA CABLES. CABLE MARKERS SHALL BE INSTALLED BY THE CONTRACTOR AT ALL LOCATIONS SELECTED BY THE RESIDENT ENGINEER AND / OR THE FAA. THE CONTRACTOR SHALL BE REQUIRED TO ADJUST THE ELEVATION OF EXISTING MARKERS AND / OR REPLACE EXISTING MARKERS DAMAGED DURING CONSTRUCTION.
 2. COST OF CONCRETE MARKERS IS INCIDENTAL TO THE ASSOCIATED ITEMS OF DUCT OR CABLE.
 3. EDGE EXPOSED CONCRETE WITH A 1/4" RADIUS TOOL.
 4. WHERE ADDITIONAL SPACE TO FIT THE LEGEND IS REQUIRED, SOME OF THE FOLLOWING METHODS SHALL BE EMPLOYED.
 - A. REDUCE LETTER SIZE TO 3" HIGH, 2" WIDE.
 - B. INCREASE THE MARKER SIZE TO 30" X 30" MAX.
 - C. PROVIDE ADDITIONAL MARKERS PLACED SIDE BY SIDE.

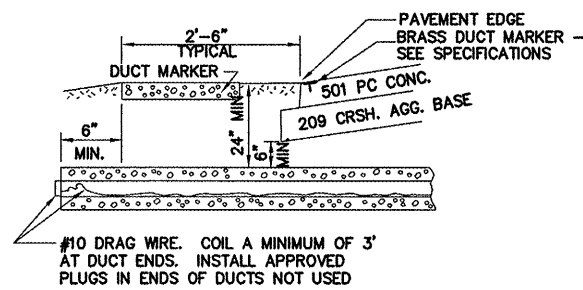
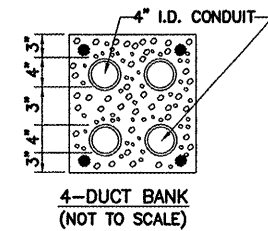
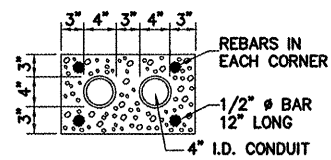
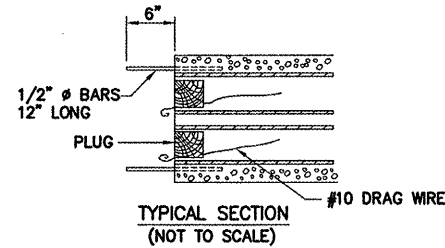
TAXI GUIDANCE SIGNS ISOLATION TRANSFORMER DATA*	
NUMBER OF MODULES	TRANSFORMER WATTAGE
1	100
2	300
3	500
4	500

TRANSFORMERS SHALL BE 6.6/6.6 AMP.
* = OR AS REQUIRED BY SIGN MANUFACTURER.

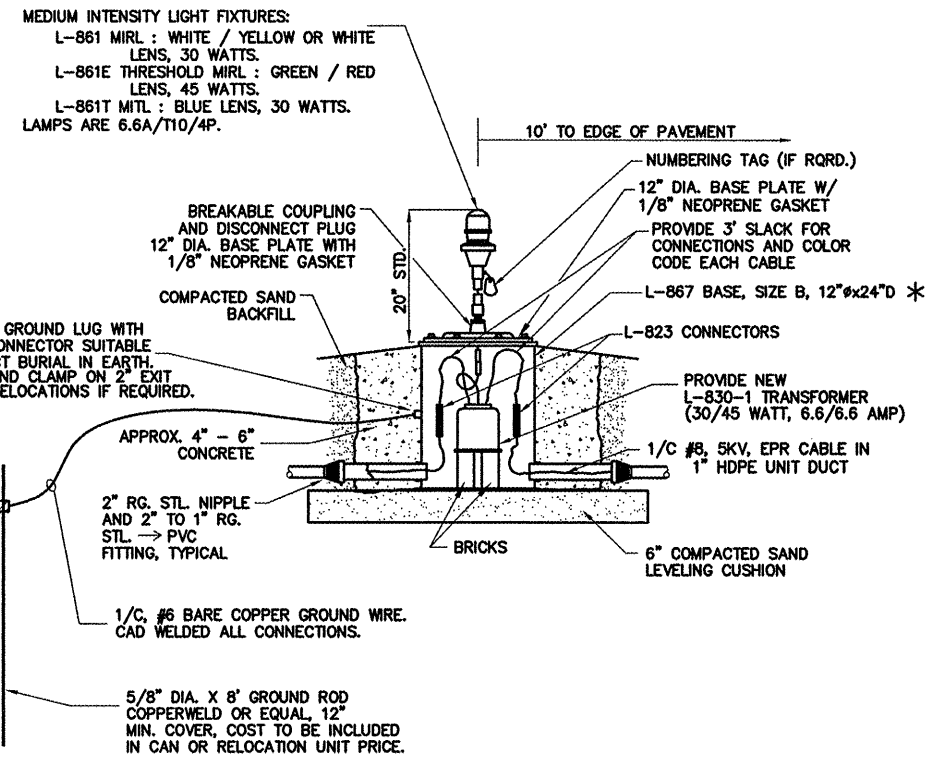


- NOTES:
1. COLOR CODE TAPE FOR WIRE IDENTIFICATION 6" BEFORE L-823 CONNECTORS.
 2. WHERE PROPOSED SIGNS ARE INSTALLED BESIDE EXISTING SIGNS PROVIDE 1' SPACE BETWEEN SIGNS.
 3. SUPPLY & INSTALL NEW GROUNDING ROD WITH EACH RELOCATION PER THIS DETAIL. INCLUDE GROUNDING ROD & WIRE COSTS IN RELOCATION UNIT PRICE.

- NOTES :
- DIMENSIONS SHOWN ARE MINIMUM.
 - TOP OF CONCRETE ENCASEMENT TO BE NOT LESS THAN 24" BELOW FINISHED SUBGRADE.
 - DUCT CONCRETE SHALL BE 610 STRUCTURAL P.C. CONC.
 - PLASTIC DUCT (PVC) SHALL BE TYPE 1 CONFORMING TO FEDERAL SPEC. W-C-1094.
 - ALL DUCT SHALL BE 4" INSIDE DIA.
 - WHERE EDGE DRAINS ARE USED, THE LENGTH OF THE DUCT SHALL BE SUCH THAT THE ENDS OF THE DUCTS WILL NOT BE LESS THAN TWO FEET FROM THE OUTSIDE EDGE OF ANY POROUS GRANULAR BACKFILL MATERIAL.
 - WHERE EDGE DRAINS ARE NOT USED, THE LENGTH OF THE DUCT SHALL BE SUCH THAT THE ENDS OF THE DUCTS WILL NOT BE LESS THAN THREE FEET FROM THE EDGE OF ANY PAVED SURFACE.
 - CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE HOMERUN (TURF CABLE MARKER)
 - USE SPLIT DUCT IN DUCT BANKS AS REQUIRED WHERE EXISTING CABLES ARE PRESENT. COST OF SPLIT DUCT TO BE INCLUDED IN THE UNIT PRICE FOR DUCT BANK.



UNDERGROUND ELECTRICAL DUCT
(NOT TO SCALE)

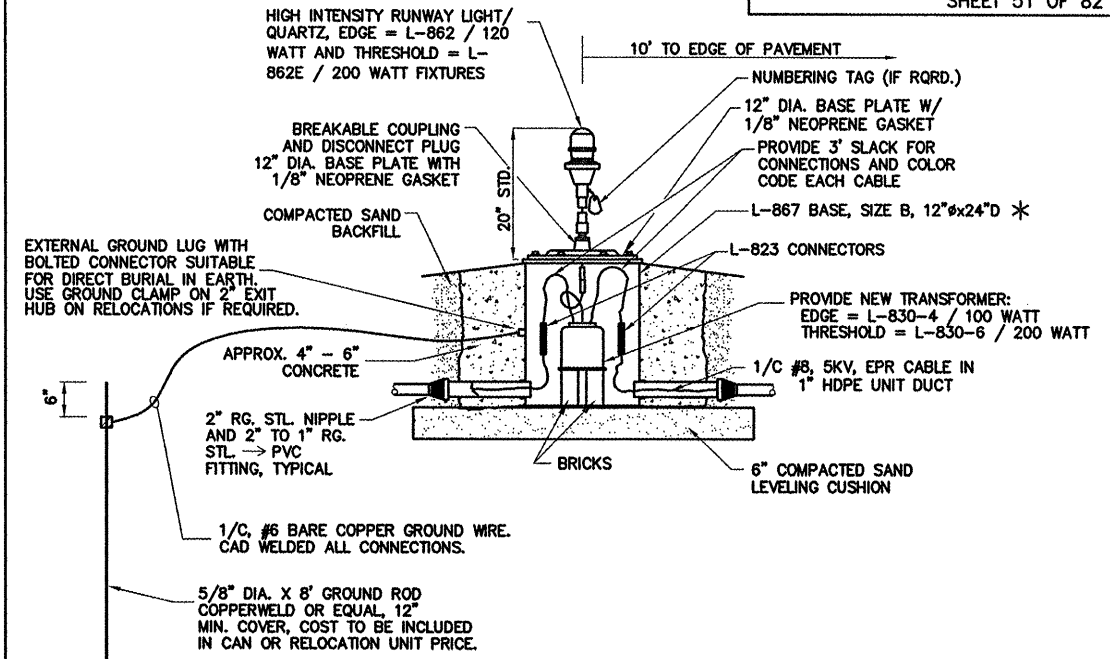


L-861 EDGE LIGHT INSTALLATION / RELOCATION DETAILS

MEDIUM INTENSITY RUNWAY LIGHT (MIRL)
MEDIUM INTENSITY TAXIWAY LIGHT (MITL)

BASE MOUNTED, 6.6 AMP SERIES CIRCUIT
NOT TO SCALE

- NOTES :
- BREAKING GROOVE OF BREAKABLE COUPLING SHALL BE 3" TO 3 1/2" ABOVE FINISHED GRADE.
 - * FOR ALL NEW EDGE LIGHTS: ORDER L-867 BASE WITH ADDITIONAL 2" CONDUIT HUB APPROXIMATELY 90° FROM MAIN ENTRANCE HUB.
 - IF THE CONDITION OF THE EXISTING LIGHTING EQUIPMENT TO BE REMOVED IS ACCEPTABLE TO THE RESIDENT ENGINEER, THE CONTRACTOR MAY REUSE THE REMOVED LIGHT FIXTURES, BASE PLATES, CANS, AND TRANSFORMERS AT THE PROPOSED LIGHT LOCATIONS. IF THE EXISTING EDGE LIGHT EQUIPMENT IS REUSED, THEN THE CONTRACTOR SHALL SUPPLY AND INSTALL 6" COMPACTED SAND LEVELING CUSHION, EXTEND THE EXISTING STEEL CONDUIT TO A POINT OUTSIDE OF THE PROPOSED CONCRETE BACKFILL, ADDITIONAL P.C. CONCRETE BACKFILL ALL AROUND (MIN. 4" THICKNESS), COMPACTED SAND BACKFILL, NEW 1/8" NEOPRENE GASKETS, RETAP BASE HOLES AS REQD., NEW STAINLESS STEEL BOLTS, NUTS, & WASHERS, AND NEW LENS & LAMPS AS REQUIRED. CONTRACTOR SHALL REPLACE ANY DAMAGED EQUIPMENT AS DIRECTED BY THE RESIDENT ENGINEER. CONTRACTOR SHALL SUPPLY AND INSTALL NEW EDGE LIGHT EQUIPMENT AS REQUIRED.
 - SUPPLY & INSTALL NEW GROUNDING ROD WITH EACH RELOCATION PER THIS DETAIL. INCLUDE GROUNDING ROD & WIRE COSTS IN RELOCATION UNIT PRICE.

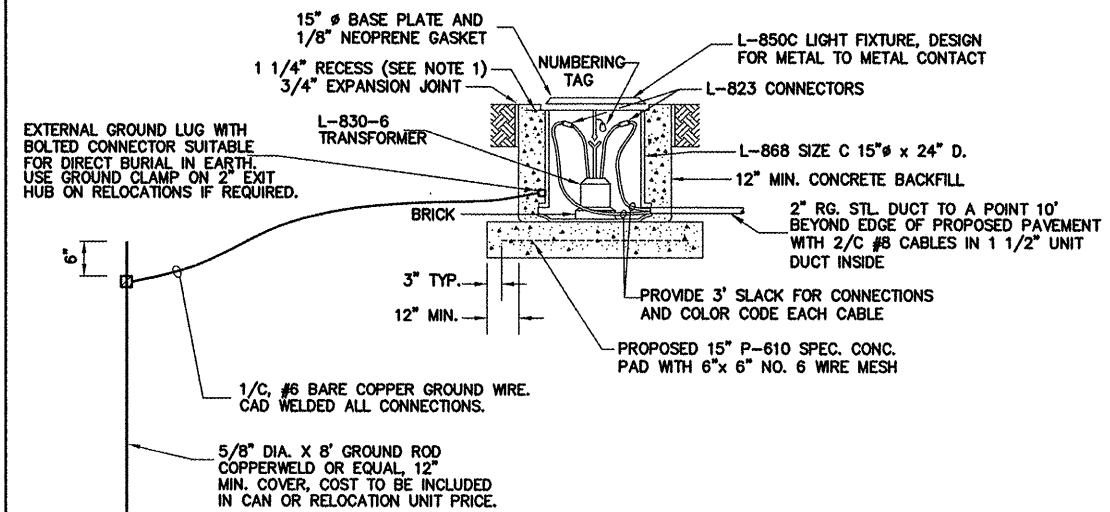


L-862 EDGE LIGHT INSTALLATION / RELOCATION DETAILS

HIGH INTENSITY RUNWAY LIGHT / QUARTZ (HIRLQ)

BASE MOUNTED, SERIES CIRCUIT
NOT TO SCALE

NOTE : SEE NOTES UNDER L-861T TAXIWAY EDGE LIGHT DETAIL. THESE NOTES ALSO APPLIES TO TYPE L-862 RUNWAY EDGE LIGHTS. SUPPLY 3-WAY BASE CANS ON ALL NEW INSTALLATIONS.

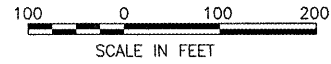


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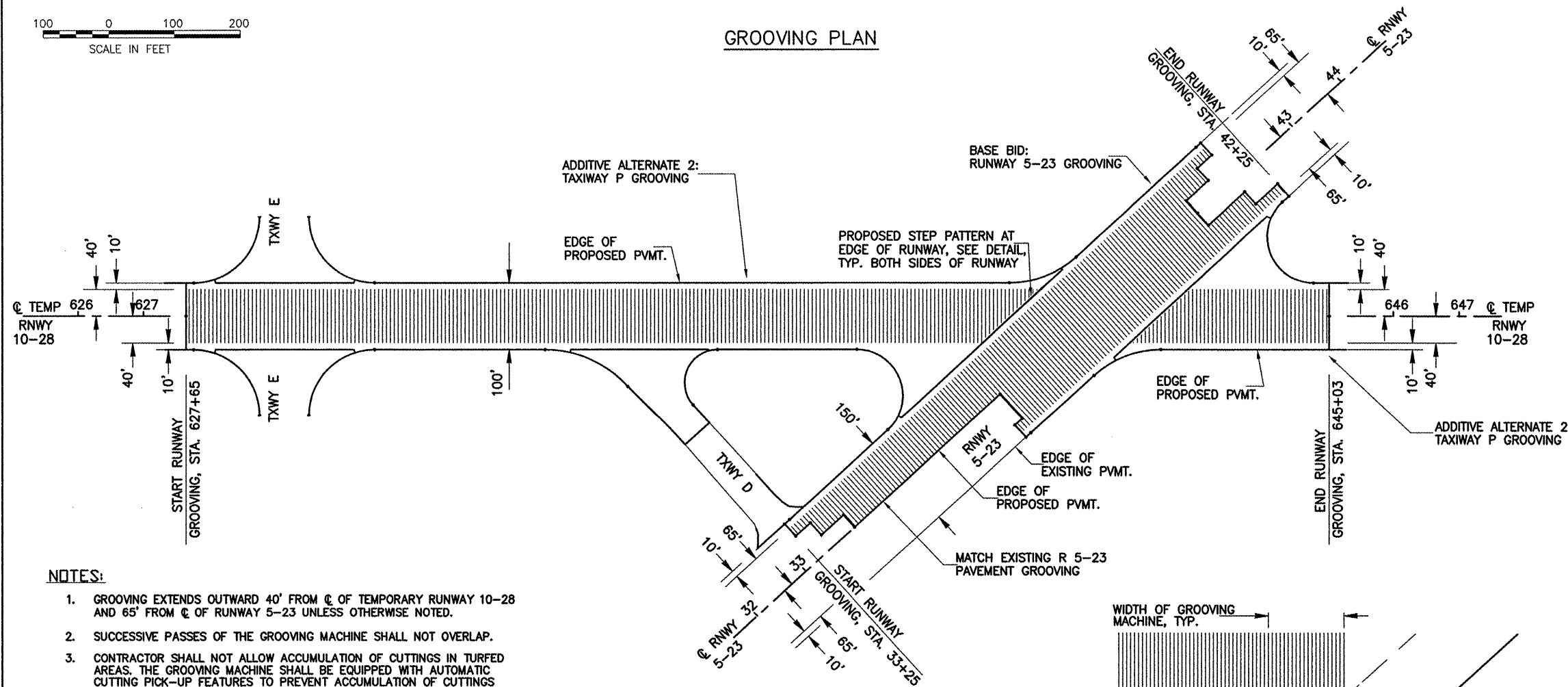
- IF INSTALLATION IS IN BITUMINOUS PAVEMENT, LEAVE CONCRETE BACKFILL 3-4 INCHES LOW FOR BACKFILL WITH BITUMINOUS MATERIAL.
- SEE NOTES 3 AND 4 UNDER L-861T TAXIWAY EDGE LIGHT DETAIL. THESE NOTES ALSO APPLIES TO TYPE L-850C RUNWAY EDGE LIGHTS.

TYPICAL INSTALLATION OF L-850C HIRLQ
EDGE LIGHT IN NEW PAVEMENT
ON L-868 BASE CAN

NOT TO SCALE

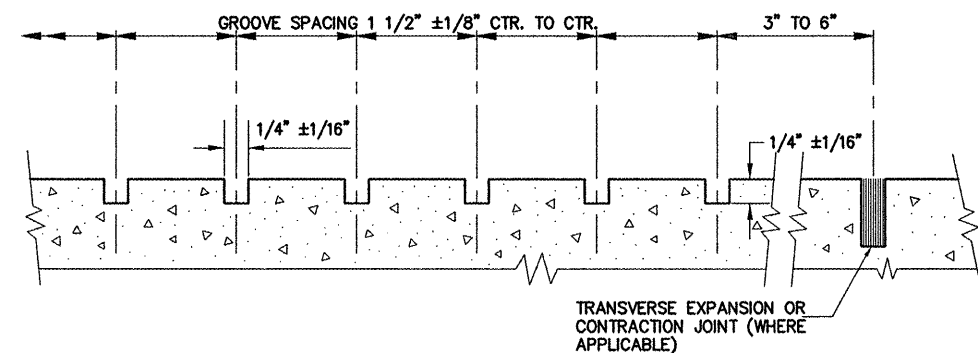
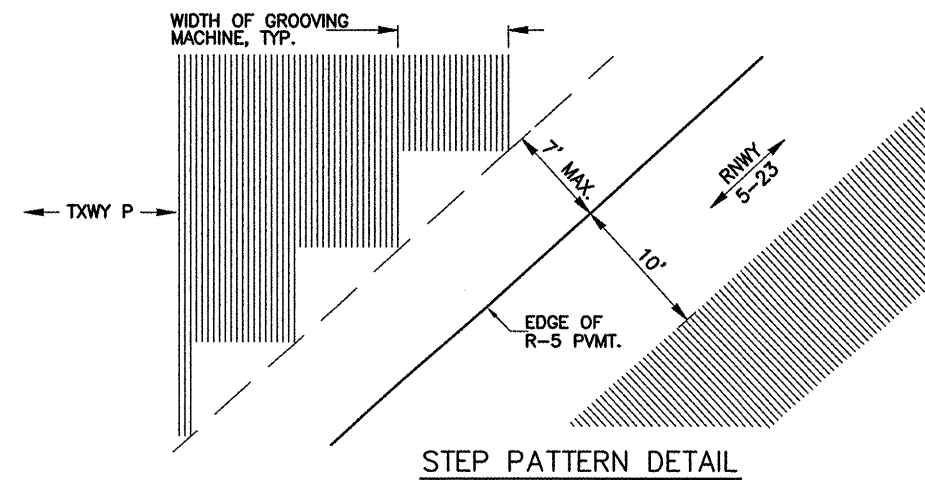


GROOVING PLAN

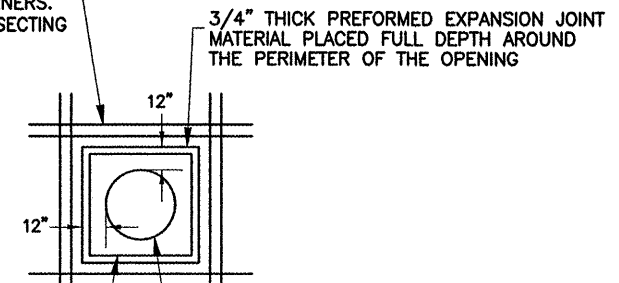


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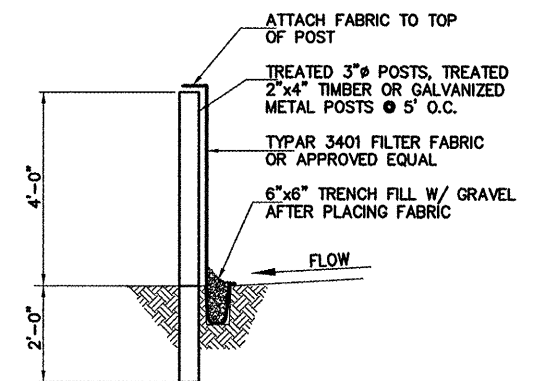
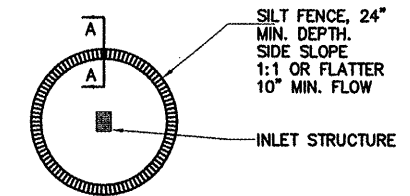
- GROOVING EXTENDS OUTWARD 40' FROM C. OF TEMPORARY RUNWAY 10-28 AND 65' FROM C. OF RUNWAY 5-23 UNLESS OTHERWISE NOTED.
- SUCCESSIVE PASSES OF THE GROOVING MACHINE SHALL NOT OVERLAP.
- CONTRACTOR SHALL NOT ALLOW ACCUMULATION OF CUTTINGS IN TURFED AREAS. THE GROOVING MACHINE SHALL BE EQUIPPED WITH AUTOMATIC CUTTING PICK-UP FEATURES TO PREVENT ACCUMULATION OF CUTTINGS IN THE TURFED AREAS.
- GROOVING SHALL BE COMPLETED PRIOR TO THE INSTALLATION OF NEOPRENE COMPRESSION SEALS IN THE LONGITUDINAL JOINTS.
- CONTRACTOR SHALL REPLACE AND / OR RE-SEAL ALL EXISTING JOINTS THAT ARE DAMAGED BY THE PAVEMENT GROOVING OPERATIONS. COSTS TO BE INCLUDED IN THE CONTRACT UNIT PRICE FOR PAVEMENT.
- GROOVES SHALL BE SAWED NO LESS THAN 6 INCHES AND NO MORE THAN 18 INCHES FROM PROPOSED OR FUTURE IN-PAVEMENT LIGHT FIXTURES.
- THE RUNWAY 5-23 GROOVES SHALL EXTEND ACROSS THE INTERSECTION OF RUNWAY 5-23 / TAXIWAY P. THE TAXIWAY P GROOVES SHALL BE CUT IN A STEP PATTERN AS SHOWN ABOVE.
- BOTH PCC PAVEMENT GROOVING AND BITUMINOUS PAVEMENT GROOVING SHALL BE MEASURED AND PAID FOR UNDER CONTRACT ITEM 501540.
- MATCH SPACING OF THE EXISTING GROOVES ON RUNWAY 5-23.
- THE RUNWAY 5-23 GROOVING SHALL BE MEASURED AND PAID FOR UNDER CONTRACT ITEM AR501540. THE TAXIWAY P GROOVING, IF AWARDED, SHALL BE MEASURED AND PAID FOR UNDER CONTRACT ITEM AS501540.

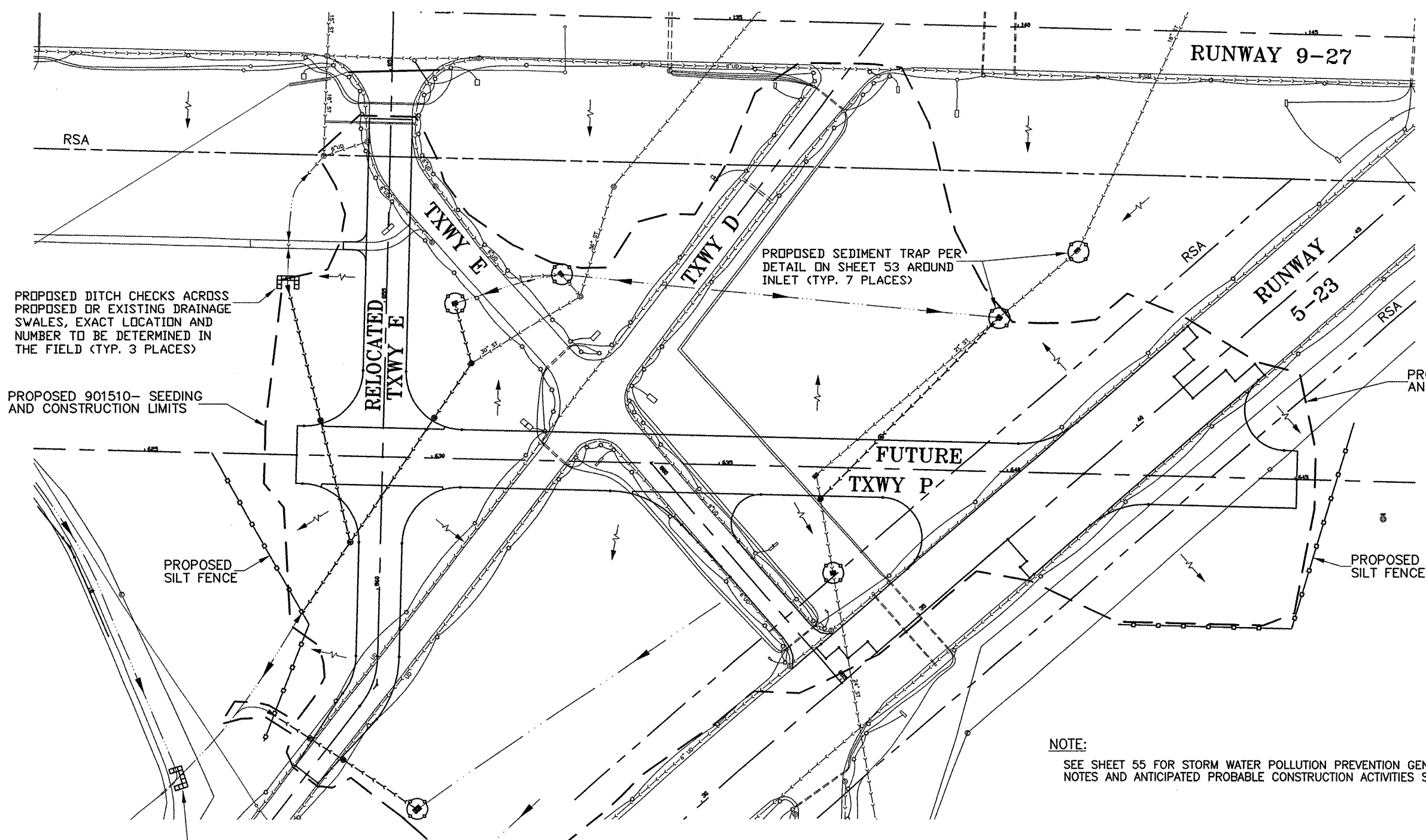
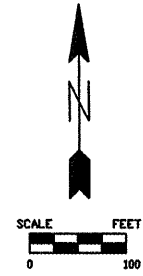


2 - #6 REBARS (8 TOTAL) PLACED AT MID-DEPTH OF PAVEMENT 3" FROM EXPANSION JOINT ON 6" C TO C. EXTEND BARS 2'-0" BEYOND EXPANSION CORNERS. DISCONTINUE BARS 3" EITHER SIDE OF INTERSECTING EXPANSION JOINTS.



#6 REBARS (4 TOTAL) PLACED AT MID-DEPTH OF PAVEMENT 3" FROM EXPANSION JOINT. PLACE CASTING TO GRADE AND FILL BLOCKED OUT AREA WITH FULL DEPTH CONCRETE AFTER PAVING INSTALLED





PROPOSED DITCH CHECKS ACROSS PROPOSED OR EXISTING DRAINAGE SWALES, EXACT LOCATION AND NUMBER TO BE DETERMINED IN THE FIELD (TYP. 3 PLACES)

PROPOSED 901510- SEEDING AND CONSTRUCTION LIMITS

PROPOSED SILT FENCE

PROPOSED SEDIMENT TRAP PER DETAIL ON SHEET 53 AROUND INLET (TYP. 7 PLACES)

PROPOSED 901510- SEEDING AND CONSTRUCTION LIMITS

PROPOSED DITCH CHECKS ACROSS PROPOSED OR EXISTING DRAINAGE SWALES, EXACT LOCATION AND NUMBER TO BE DETERMINED IN THE FIELD (TYP. 3 PLACES)

EXISTING DITCH TO BE REMOVED WITHIN THE CONSTRUCTION LIMITS. PROPOSED GROUND SURFACE TO DRAIN TO PROPOSED INLET.

LEGEND:

- EXTG. EDGE LIGHT
- EXTG. GUIDANCE SIGN
- EXTG. ELECTRICAL CABLE
- EXTG. STORM SEWER OR UNDERDRAIN
- EXTG. INLET
- EXTG. OUTLET
- EXTG. MANHOLE
- EXTG. DRAINAGE DITCH
- PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS
- EXISTING FENCE
- EXISTING OR PROPOSED DRAINAGE SWALE
- PROPOSED SILT FENCE
- PROPOSED TEMPORARY STRAW BALE DITCH CHECK
- PROPOSED SEDIMENT TRAP

NOTE:

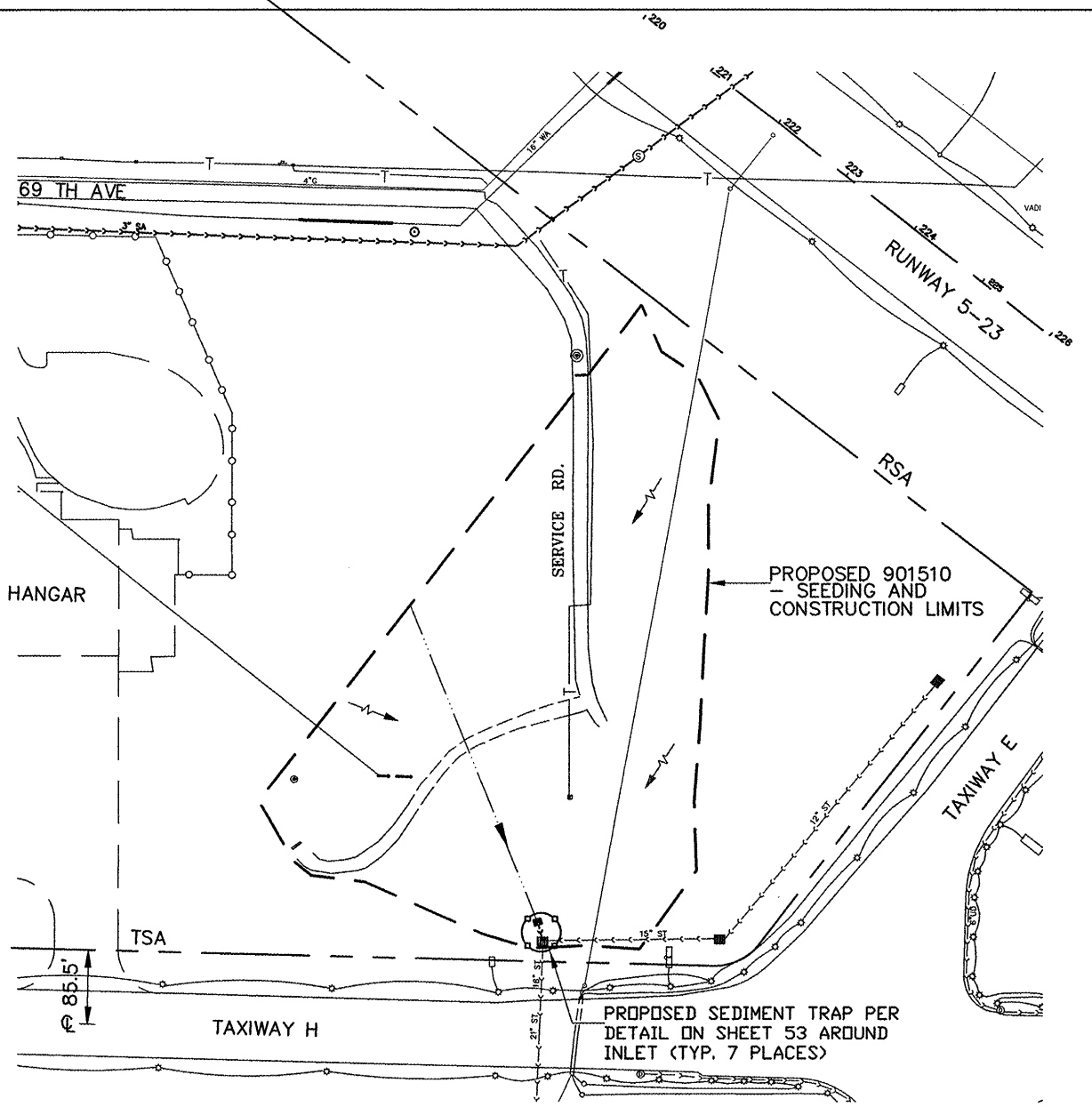
SEE SHEET 55 FOR STORM WATER POLLUTION PREVENTION GENERAL NOTES AND ANTICIPATED PROBABLE CONSTRUCTION ACTIVITIES SCHEDULE.

TAXIWAY P MIDDLE AREA PLAN

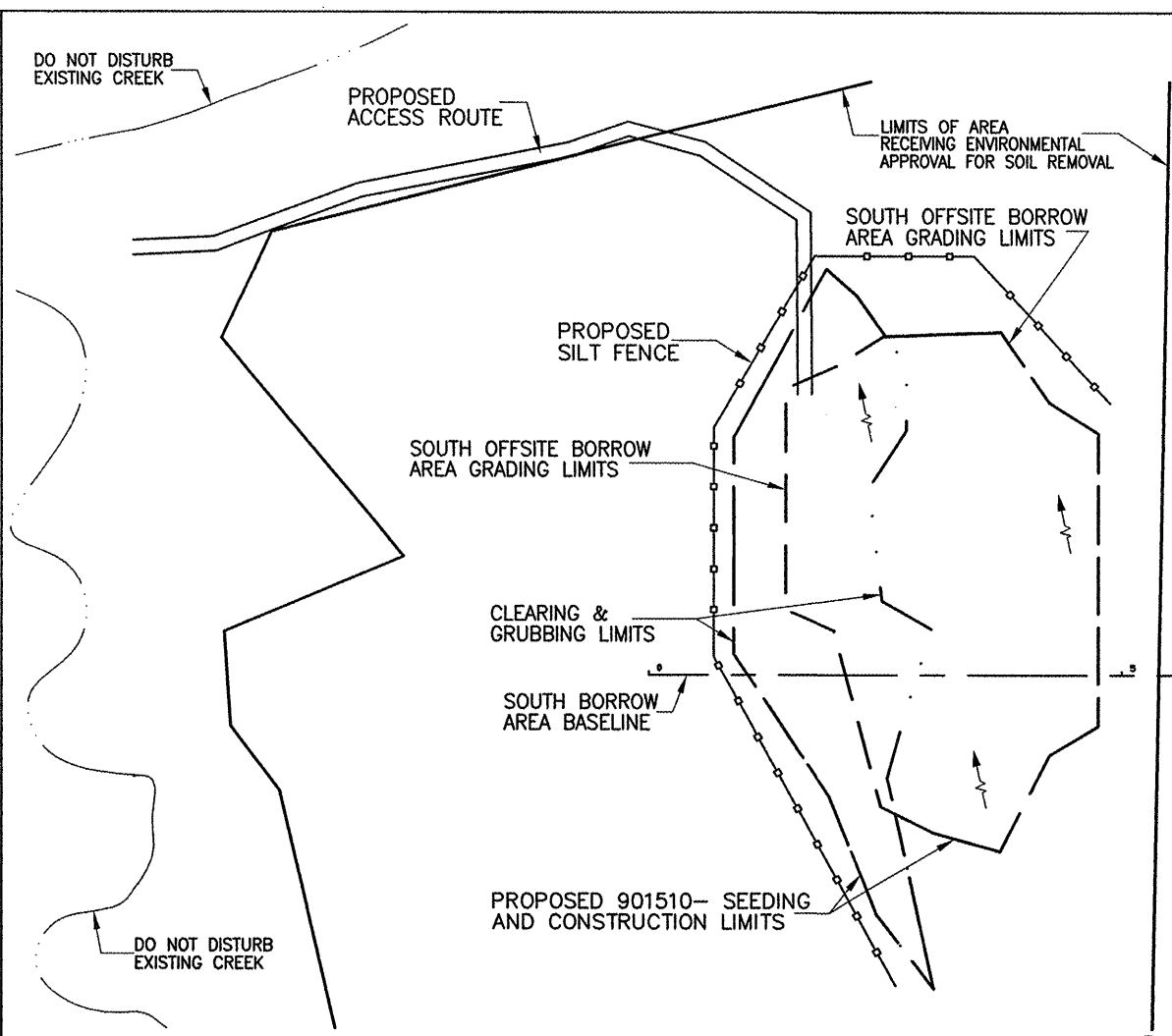
STORM WATER MANAGERS					
	NAME	ADDRESS	TELEPHONE NUMBER		SIGNATURE
			WORK	HOME	
CONTRACTOR					
SUBCONTRACTOR					
SUBCONTRACTOR					

EROSION AND SEDIMENT CONTROL MEASURES		
ITEM	LOCATION	SPECIAL PROVISION/PAY ITEM
TEMPORARY DITCH CHECKS	RUNWAY 5-23, STA. 25+25, LT. 160'; TAXIWAY P, STA. 627+42, LT. 294'; AND RELOCATED TAXIWAY D, STA. 865+14, RT. 175'.	156500
TEMPORARY SILT FENCE	AT ALL SEDIMENT TRAP & DITCH CHECK LOCATIONS AND T-P, STA. 626+20 LT. TO RELOCATED T-D STA. 863+63 RT.; T-P, STA. 642+00 RT. TO STA. 646+00 LT.; AND R13-31 STA. 318+88 RT. TO STA. 319+16 RT. (AROUND SOUTH BORROW AREA).	156500
TEMPORARY SEDIMENT TRAPS	RUNWAY 5-23, STA. 34+33, LT. 155'; RUNWAY 13-31, NORTH BORROW AREA, STA. 225+45, RT. 910'; TAXIWAY P, 632+20, LT. 323'; 639+80, LT. 268'; 641+12, LT. 388'; RELOCATED TAXIWAY E, 855+00 LT. 120' AND RELOCATED TAXIWAY D, STA. 862+93 LT. 188'.	156500

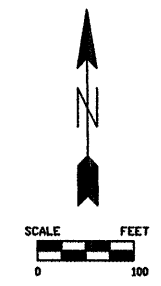
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NORTH ON-SITE BORROW AREA PLAN



SOUTH OFFSITE BORROW AREA PLAN



- LEGEND:**
- EXTG. EDGE LIGHT
 - EXTG. GUIDANCE SIGN
 - EXTG. ELECTRICAL CABLE
 - EXTG. STORM SEWER OR UNDERDRAIN
 - EXTG. INLET
 - EXTG. OUTLET
 - EXTG. MANHOLE
 - EXTG. DRAINAGE DITCH
 - PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS
 - EXISTING FENCE
 - EXISTING OR PROPOSED DRAINAGE SWALE
 - PROPOSED SILT FENCE
 - PROPOSED TEMPORARY STRAW BALE DITCH CHECK
 - PROPOSED SEDIMENT TRAP

GENERAL NOTES:

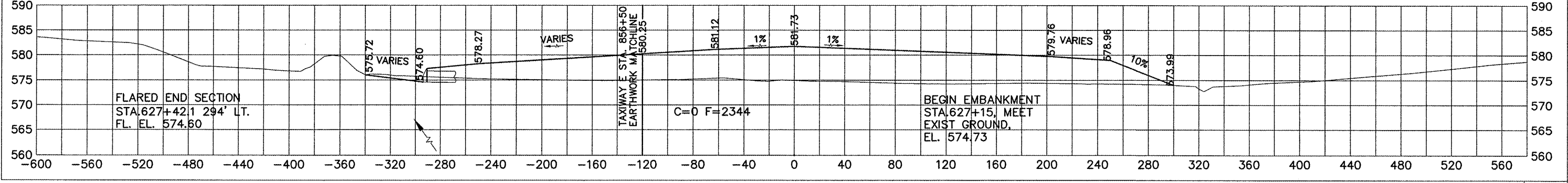
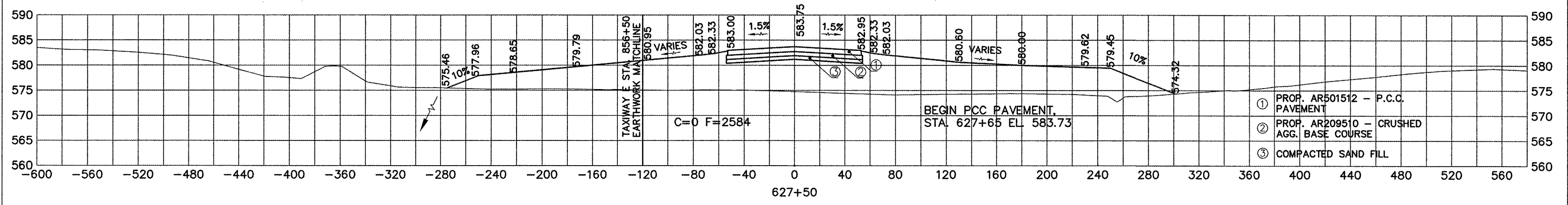
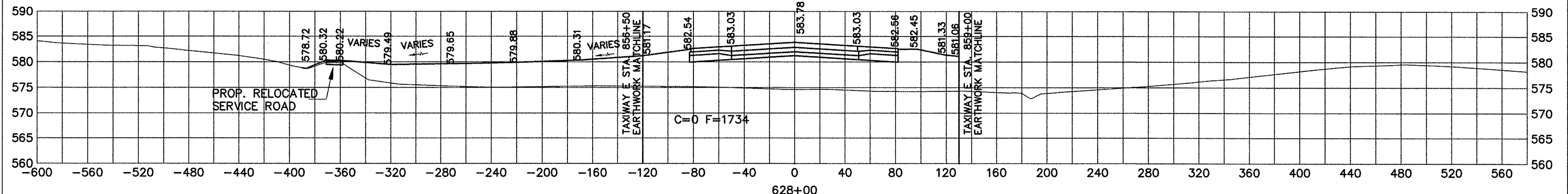
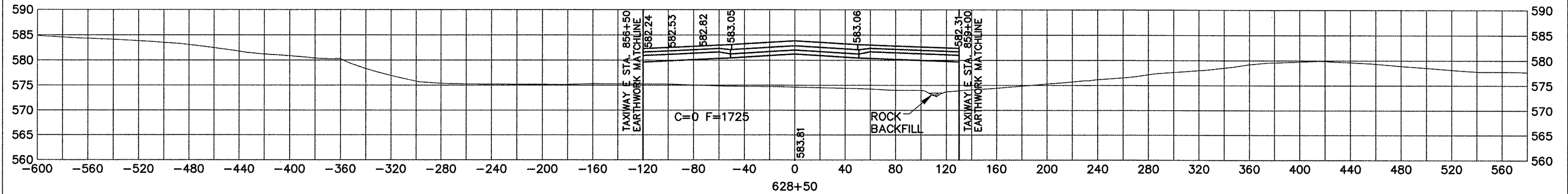
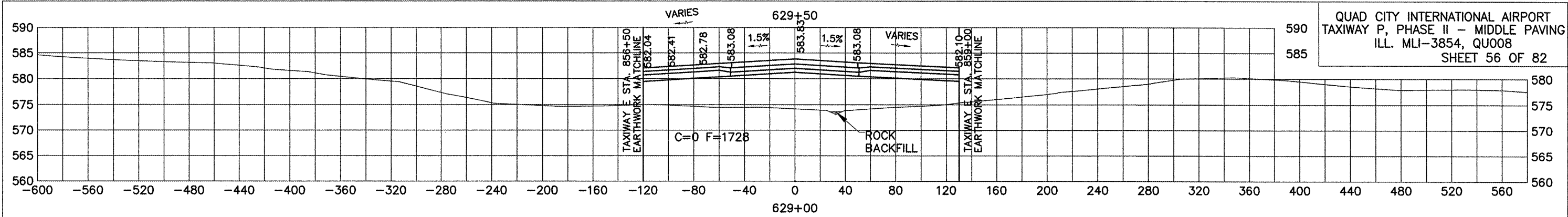
1. TEMPORARY DITCH CHECKS - TWO BALES HIGH WITH SILT FENCING SHALL BE REQUIRED PER IDOT STANDARD 280001 - TEMPORARY EROSION SITE CONTROL SYSTEM DRAWING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING, MAINTAINING, AND REMOVING DITCH CHECKS, SILT FENCE, AND SEDIMENT TRAPS TO THE SATISFACTION OF THE RESIDENT ENGINEER. THIS INCLUDES, BUT IS NOT LIMITED TO, CLEANING EROSION SOILS AS REQUIRED.
2. LOCATION OF THE DITCH CHECKS, SILT FENCES, AND SEDIMENT TRAPS SHOWN ARE APPROXIMATE. ACTUAL LOCATIONS TO BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION.
3. ONCE CONSTRUCTION HAS BEEN COMPLETED, OR TEMPORARILY SUSPENDED FOR LONGER THAN 21 DAYS (SUCH AS A WINTER SHUTDOWN), THE CONTRACTOR SHALL SEED ALL AREAS DISTURBED IN ACCORDANCE WITH ITEM 901510 WITHIN 14 DAYS OF THE LAST DISTURBANCE. DITCH CHECKS, SILT FENCES, AND SEDIMENT TRAPS SHALL REMAIN IN PLACE AND BE MAINTAINED UNTIL THE CONTRACTOR ESTABLISHES A GOOD STAND OF GRASS OF UNIFORM COLOR AND DENSITY TO THE SATISFACTION OF THE ENGINEER.
4. THE CONTRACTOR AND EACH SUBCONTRACTOR RESPONSIBLE FOR WATER POLLUTION CONTROL SHALL DESIGNATE, PRIOR TO BEGINNING CONSTRUCTION, A PERSON OR PERSONS WHO CAN BE CONTACTED IN AN EMERGENCY INVOLVING THEIR WATER POLLUTION CONTROL ITEMS. THESE DESIGNATED PEOPLE SHALL BE AVAILABLE TO REPAIR AND MAINTAIN WATER POLLUTION CONTROL DEVICES ON A 24-HOUR / 7 DAYS PER WEEK BASIS.
5. CONTRACTOR TO EXCAVATE TEMPORARY EROSION CONTROL DRAINAGE SWALE AS REQUIRED TO PREVENT RAIN WATER PONDING AND TO CONTROL STORM WATER RUN-OFF.
6. CONTRACTOR SHALL ADHERE TO THE CITY OF MOLINE'S EROSION AND SEDIMENT CONTROL REGULATIONS AND THE ILLINOIS MANUAL ON EROSION AND SEDIMENT CONTROL.

7. SEDIMENT AND EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO ANY CONSTRUCTION THAT DISTURBS EXISTING STORM WATER RUN-OFF CONDITIONS AND/OR GROUND VEGETATION.
8. EROSION CONTROL MEASURES SHALL BE INSPECTED WEEKLY AND AFTER EACH PRECIPITATION EVENT AND REPLACED OR REPAIRED AS NECESSARY.
9. RESIDENT ENGINEER SHALL CHECK THAT ALL FILL AREAS ARE TO A MINIMUM COMPACTION OF 95% OF THE MATERIALS STANDARD PROCTOR MAXIMUM DRY DENSITY.
10. SILT FENCE, SEDIMENT TRAPS, AND HAY BALES SHALL BE CLEANED OR REPLACED WHEN SILT BUILDS UP TO WITHIN ONE FOOT OF THE TOP OF THE SILT FENCE OR HAY BALES.
11. ADDITIONAL EROSION CONTROL DEVICES SHALL BE USED AS REQUIRED. THE COSTS OF ALL MEASURES NECESSARY TO COMPLY WITH THIS STORM WATER POLLUTION PREVENTION PLAN SHALL BE INCLUDED IN THE ITEM 156500 - TEMPORARY EROSION CONTROL LUMP SUM PRICES.
12. SEE SHEET 54 FOR STORM WATER MANAGERS SIGNATURE CHART AND EROSION / SEDIMENT CONTROL MEASURES LIST.

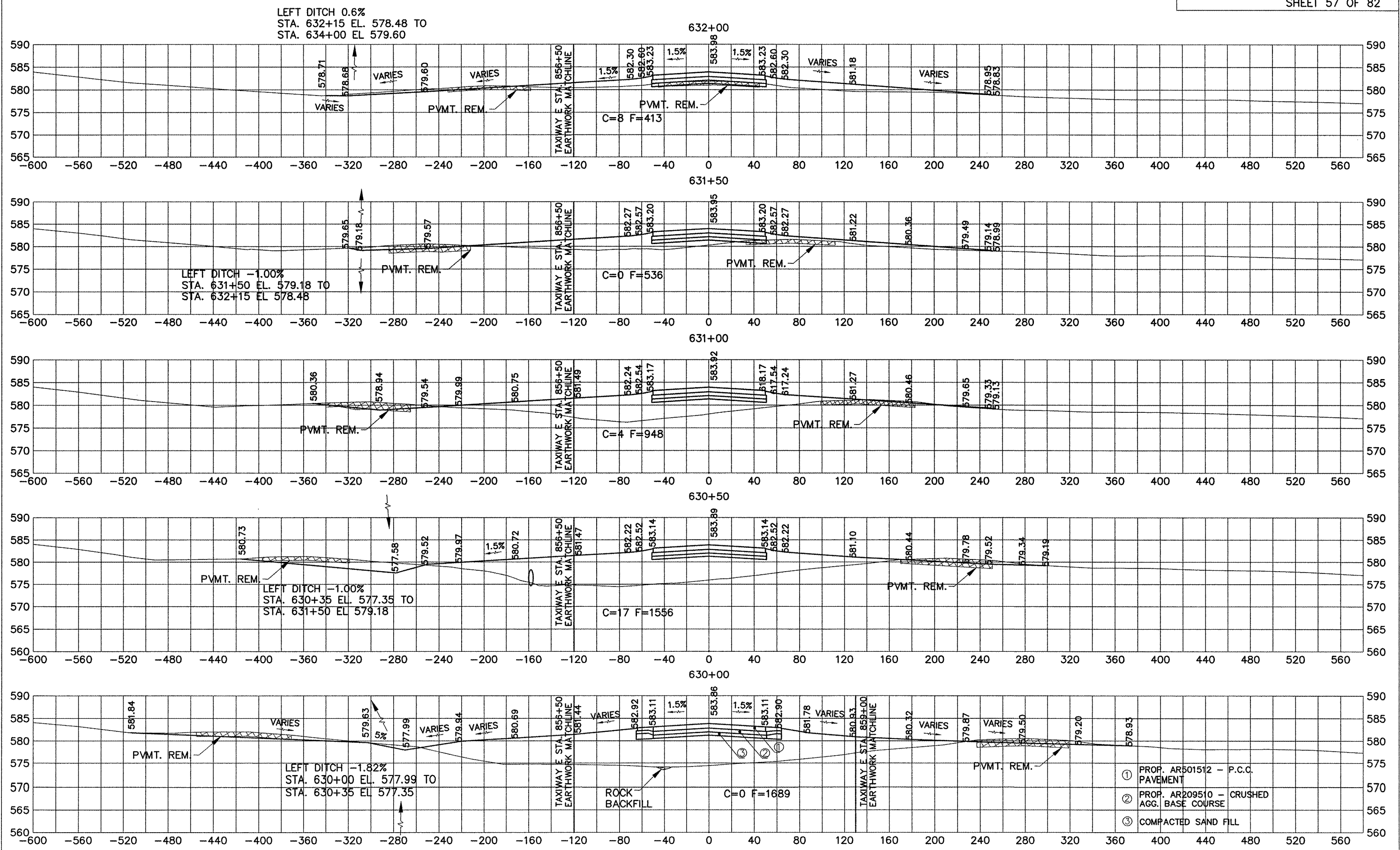
ANTICIPATED PROBABLE CONSTRUCTION ACTIVITIES SCHEDULE		WEEK															
NO.	ITEM DESCRIPTION	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
1	INSTALL SEDIMENT/EROSION CONTROLS	█															
2	CLEARING AND GRADING		█														
3	EXCAVATION AND EMBANKMENT			█	█	█	█	█	█	█	█	█	█	█	█	█	█
4	TURFING																
5	MAINTAIN SEDIMENT/EROSION CONTROL																
6	PAVING																
7	CLEAN-UP																

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QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE II - MIDDLE PAVING
ILL. MLI-3854, QU008
SHEET 56 OF 82

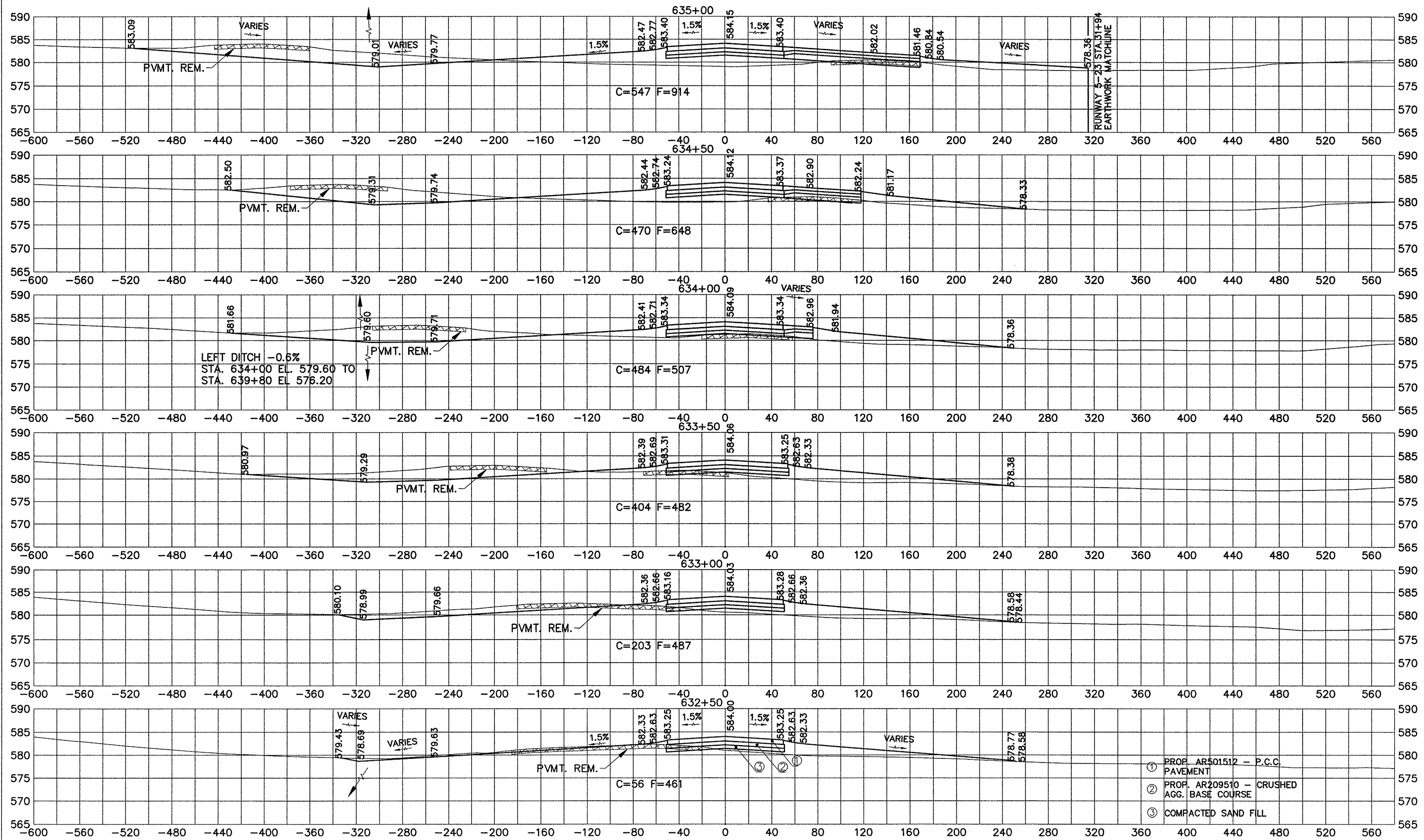


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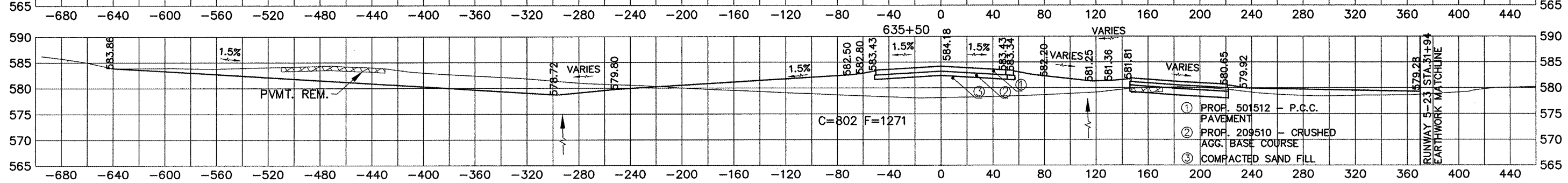
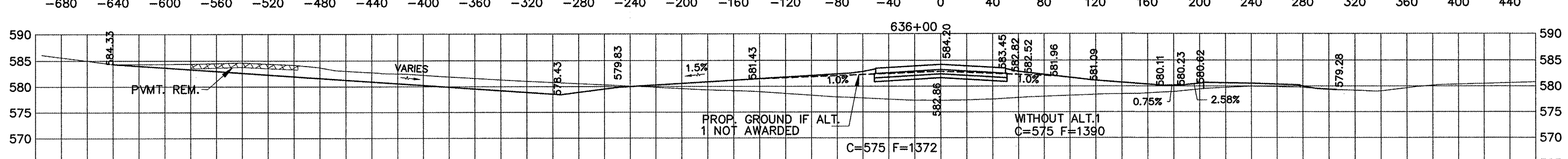
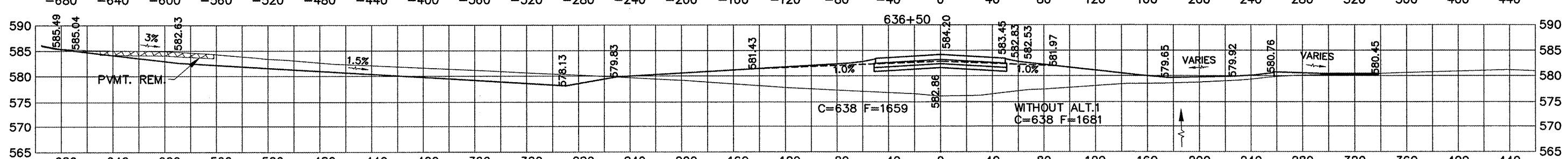
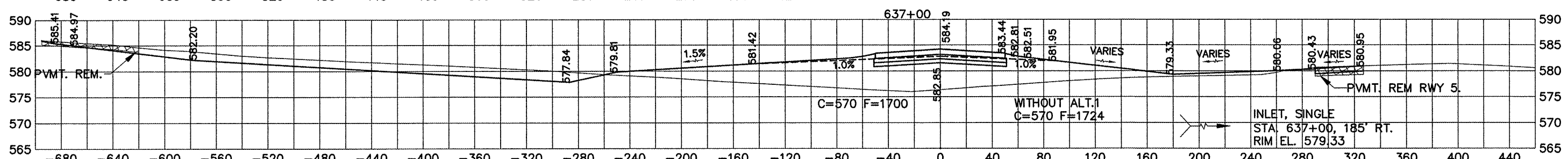
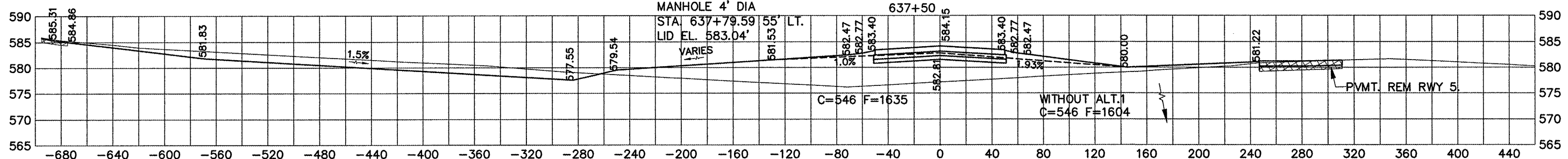
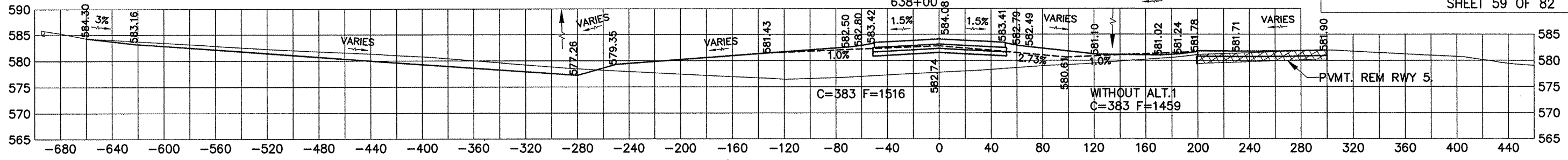
QUAD CITY INTERNATIONAL AIRPORT
 TAXIWAY P, PHASE II - MIDDLE PAVING
 ILL. MLI-3854, QU008
 SHEET 58 OF 82



- ① PROF. AR501512 - P.C.C. PAVEMENT
- ② PROF. AR209510 - CRUSHED AGG. BASE COURSE
- ③ COMPACTED SAND FILL

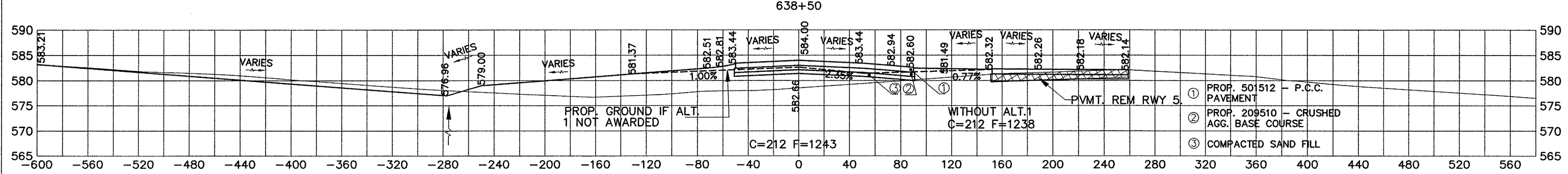
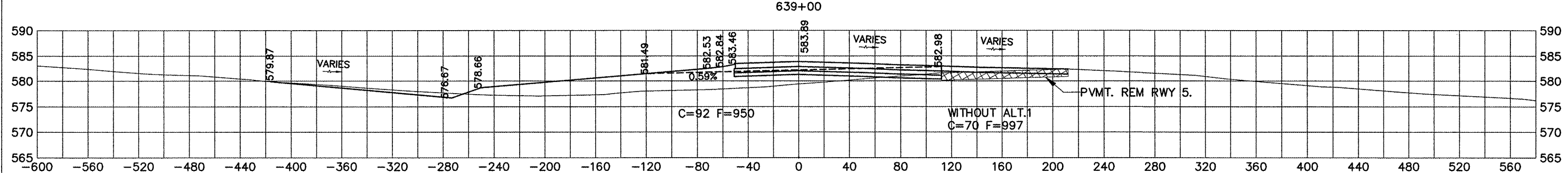
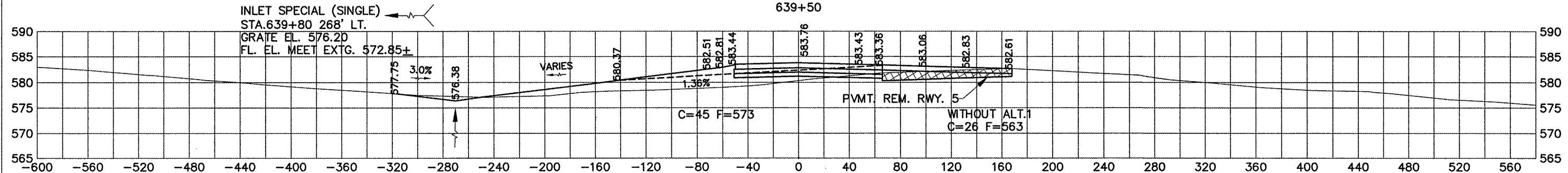
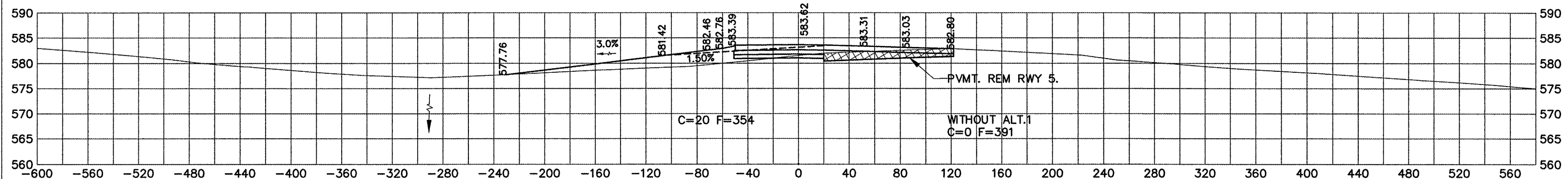
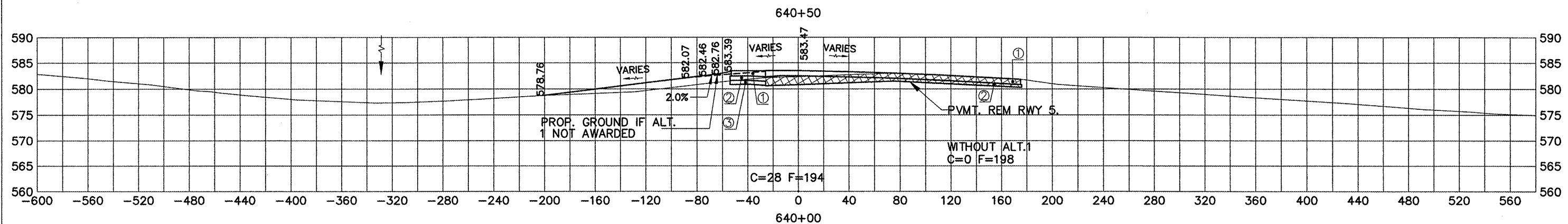
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QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE II - MIDDLE PAVING
ILL. MLI-3854, QU008
SHEET 59 OF 82



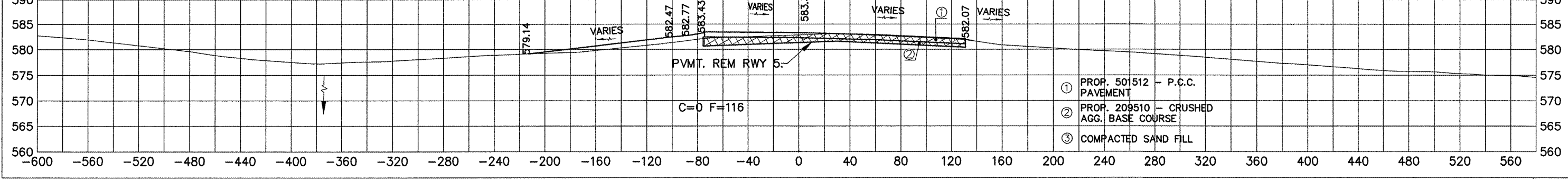
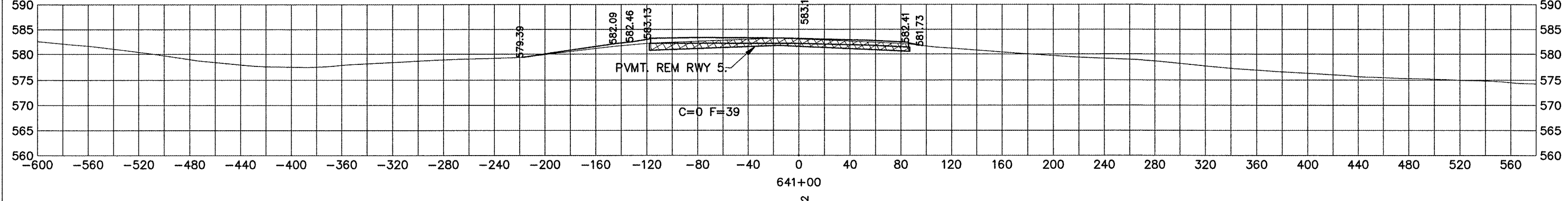
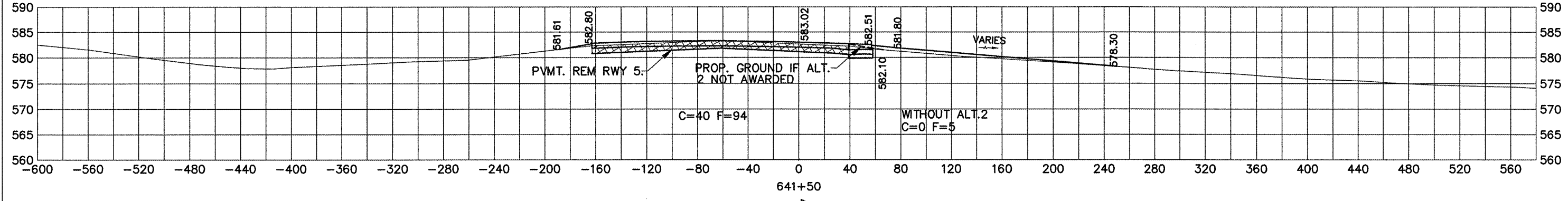
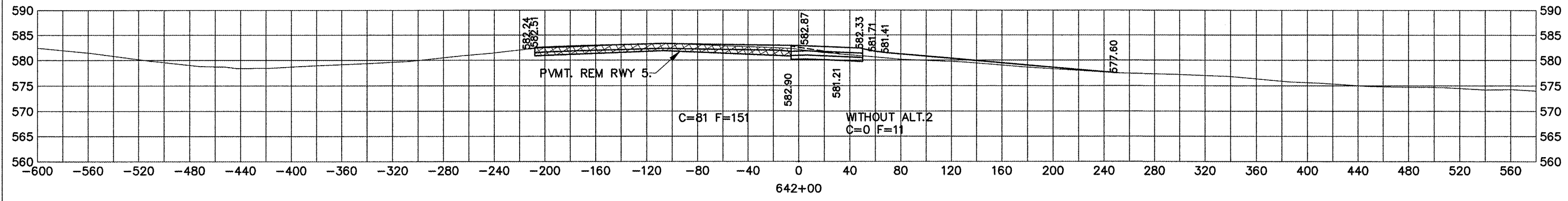
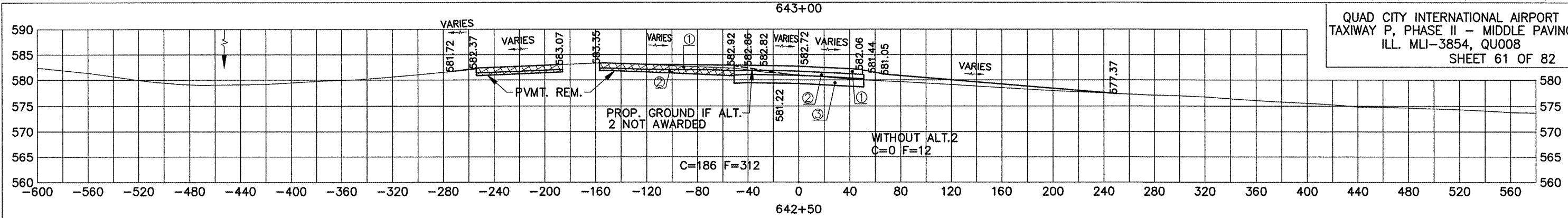
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QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE II - MIDDLE PAVING
ILL. MLI-3854, QU008
SHEET 60 OF 82



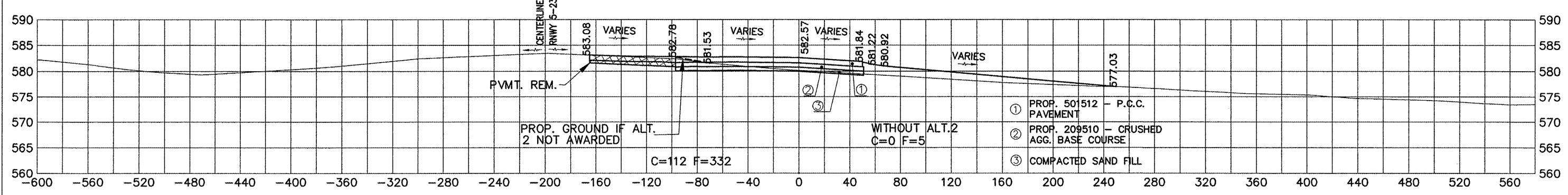
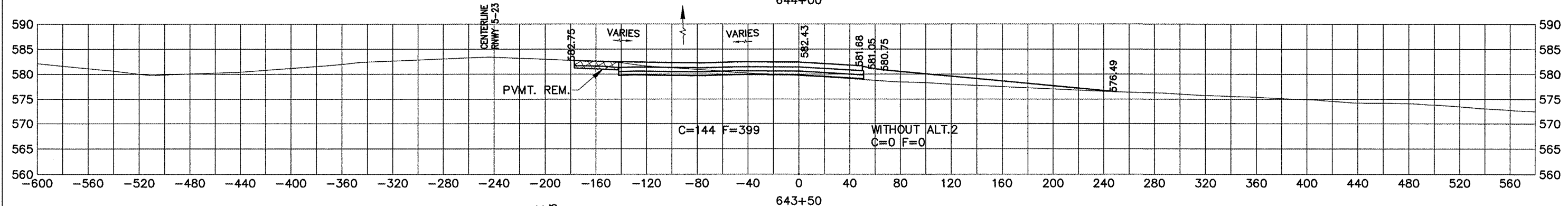
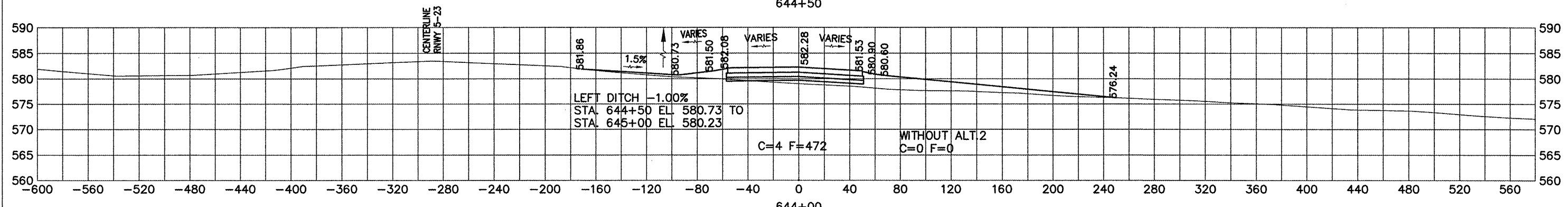
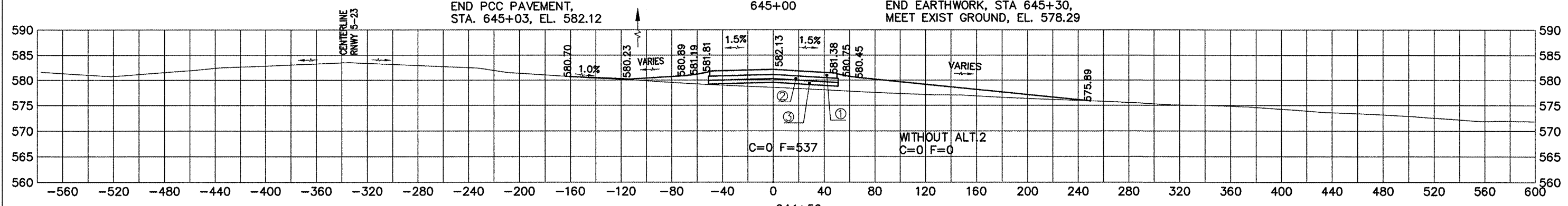
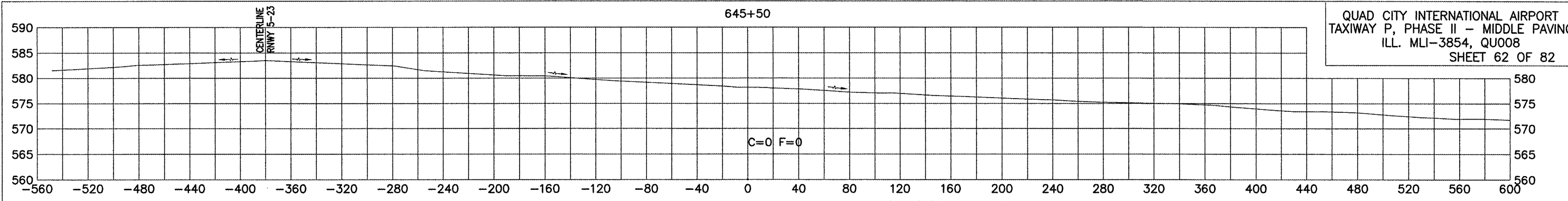
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QUAD CITY INTERNATIONAL AIRPORT
 TAXIWAY P, PHASE II - MIDDLE PAVING
 ILL. MLI-3854, QU008
 SHEET 61 OF 82



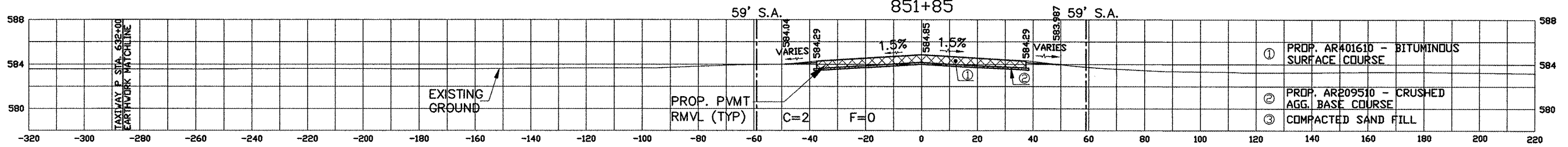
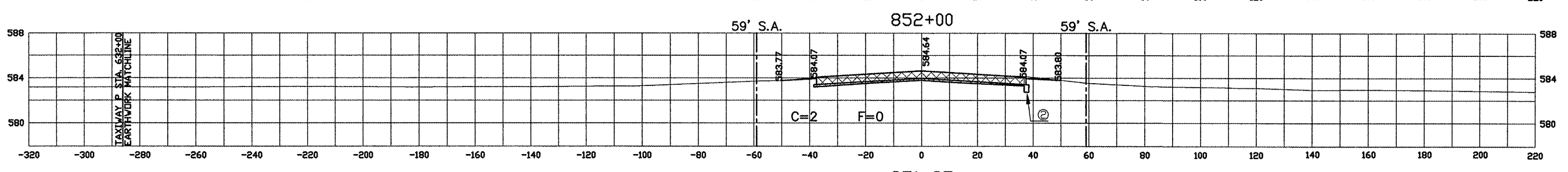
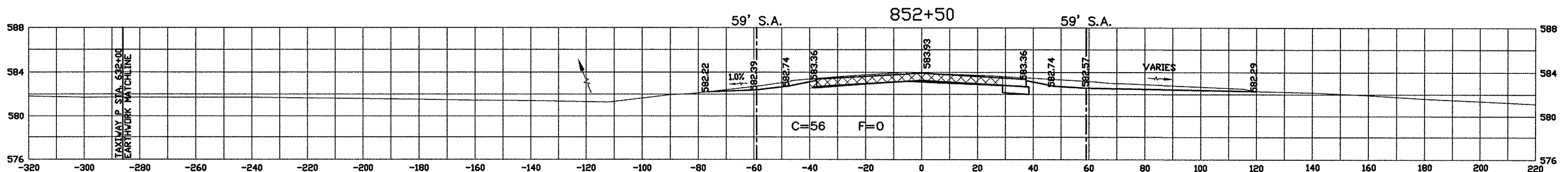
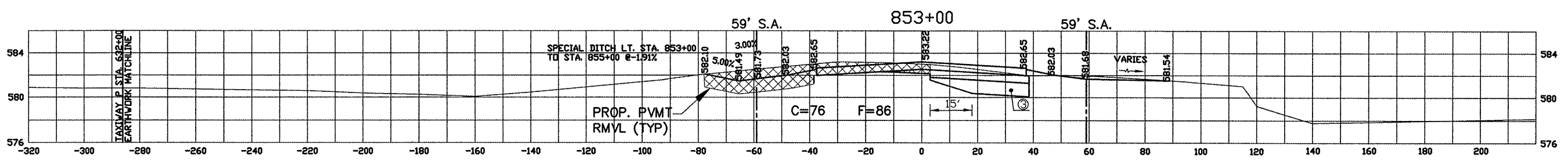
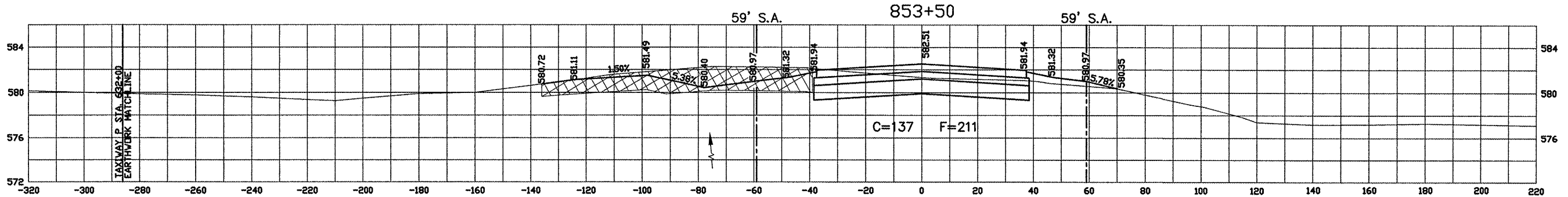
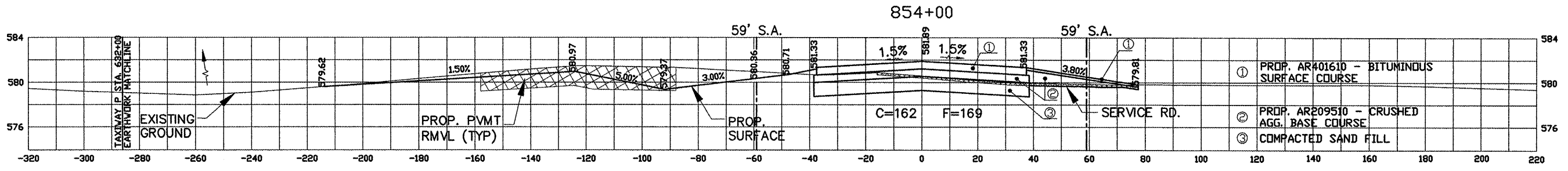
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QUAD CITY INTERNATIONAL AIRPORT
 TAXIWAY P, PHASE II - MIDDLE PAVING
 ILL. MLI-3854, QU008
 SHEET 62 OF 82



- ① PROP. 501512 - P.C.C. PAVEMENT
- ② PROP. 209510 - CRUSHED AGG. BASE COURSE
- ③ COMPACTED SAND FILL

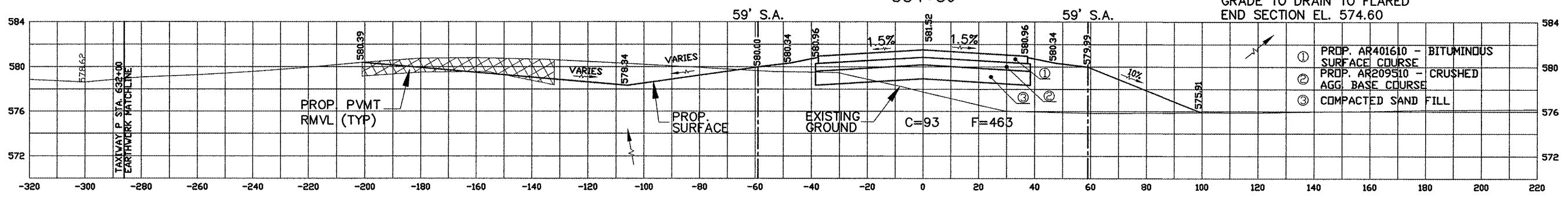
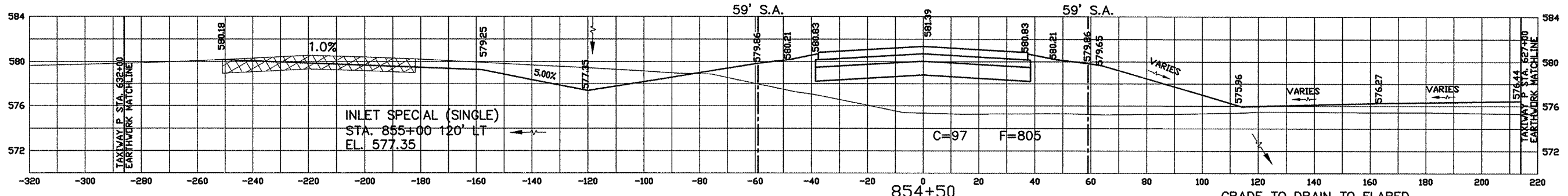
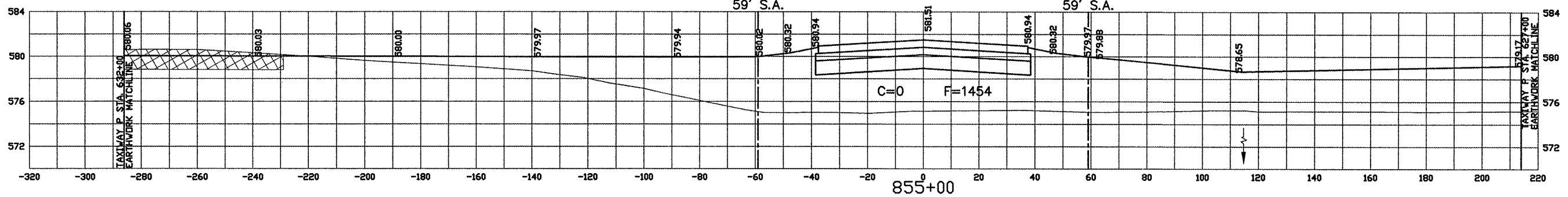
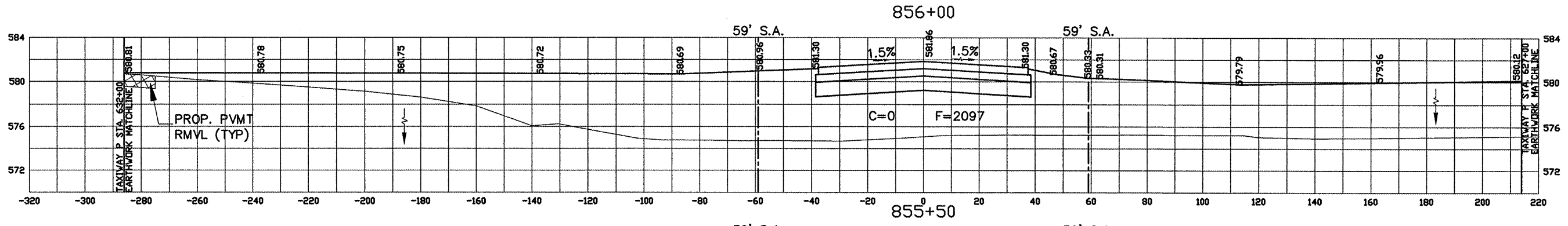
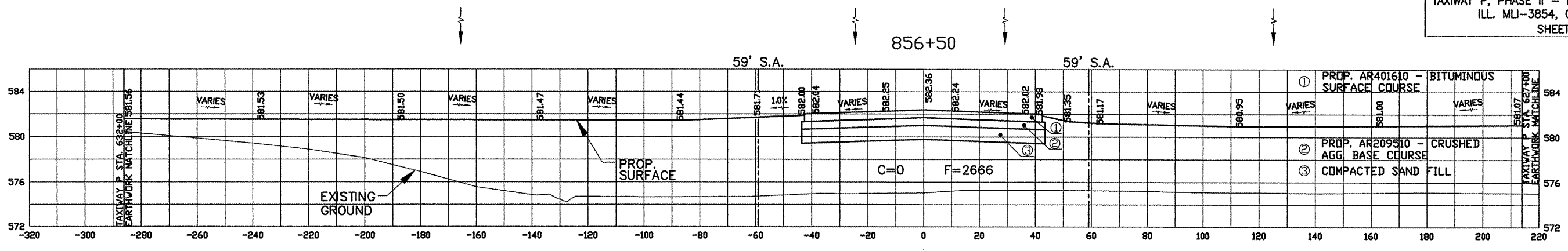
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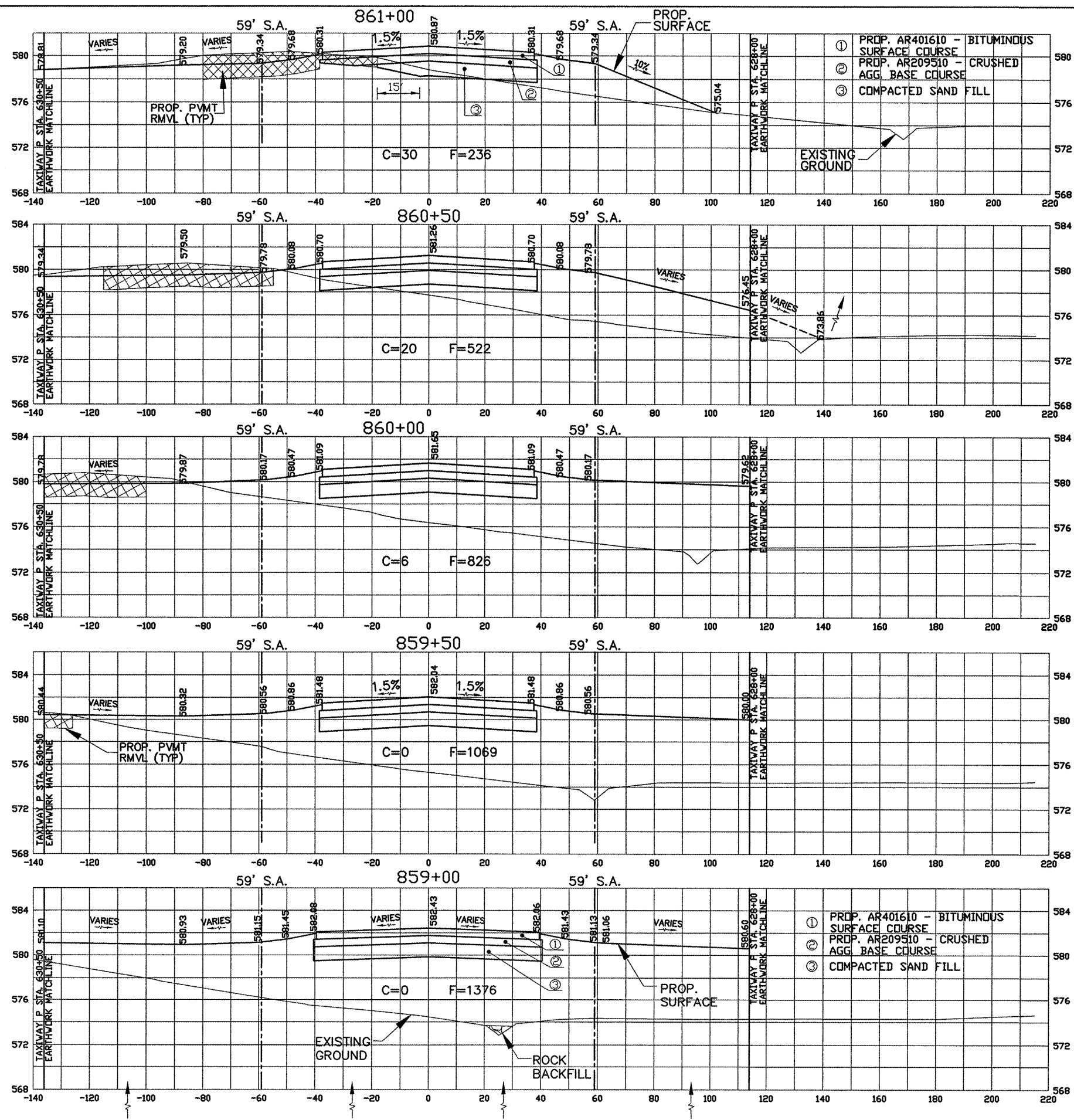


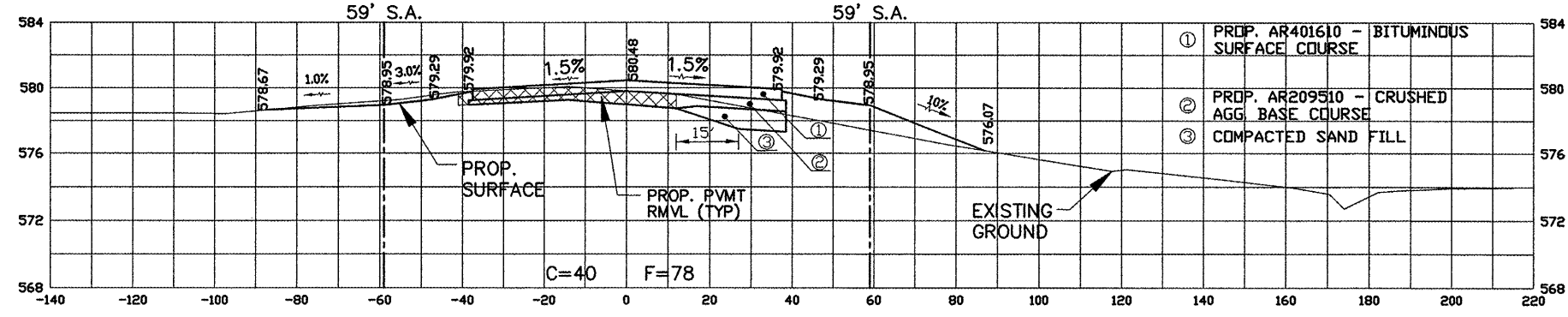
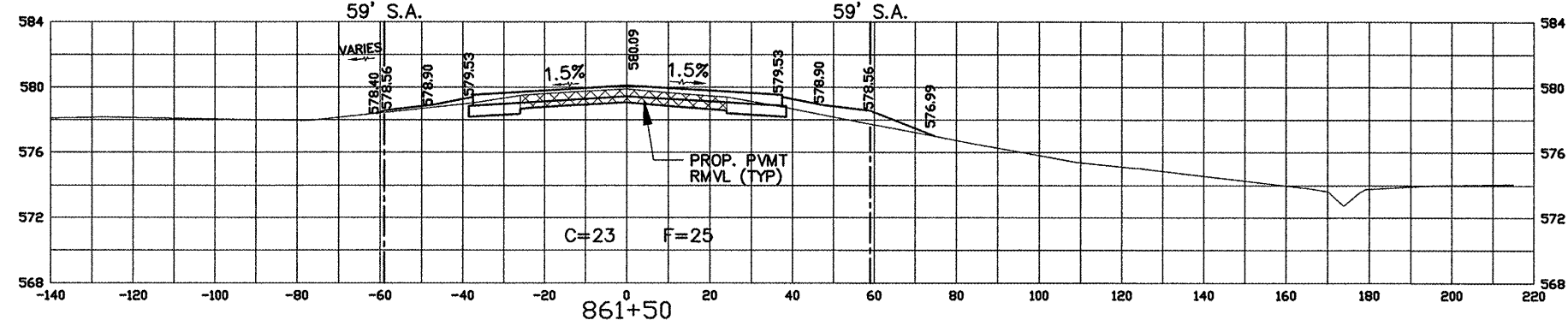
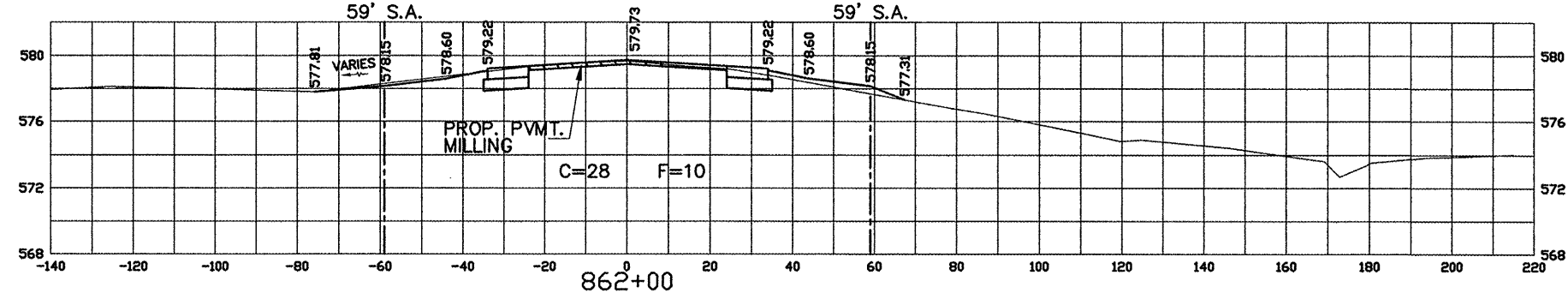
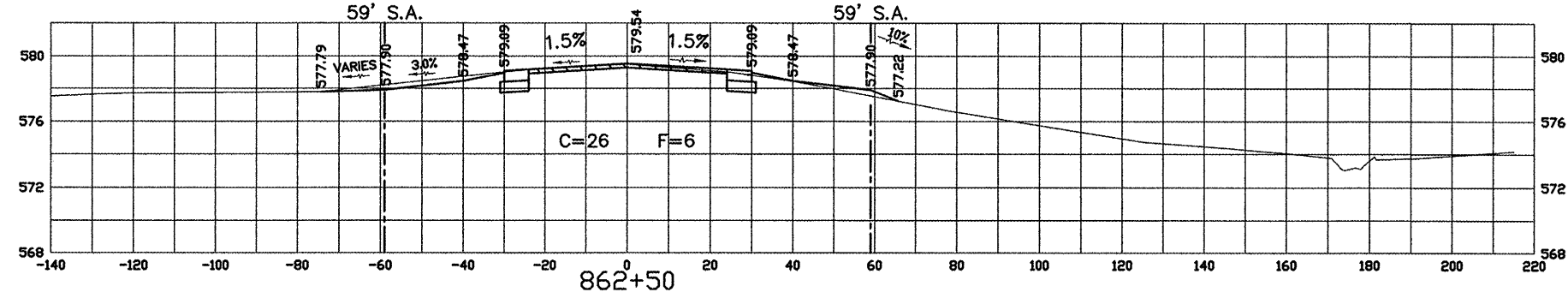
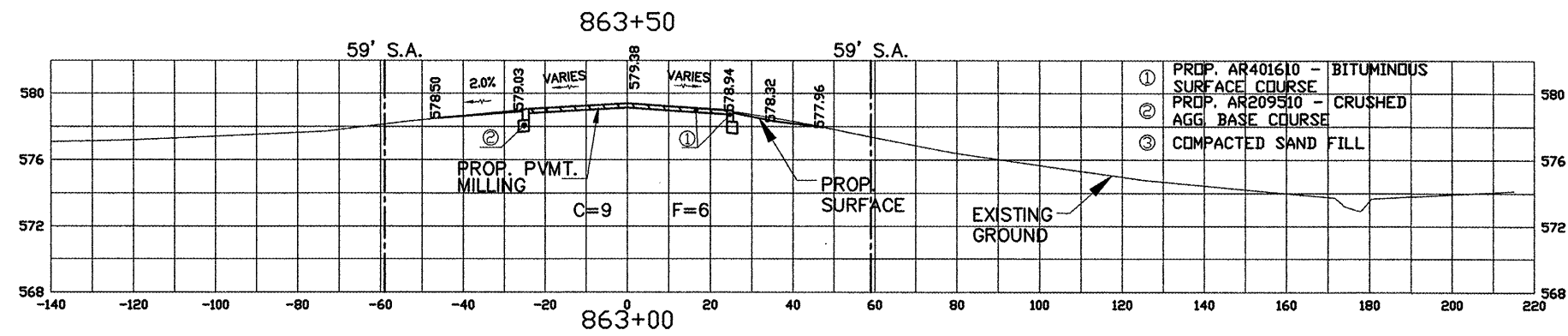
START EARTHWORK, STA 851+60, MEET EXIST GROUND

RELOCATED TAXIWAY E, STA 851+85 TO STA 854+00

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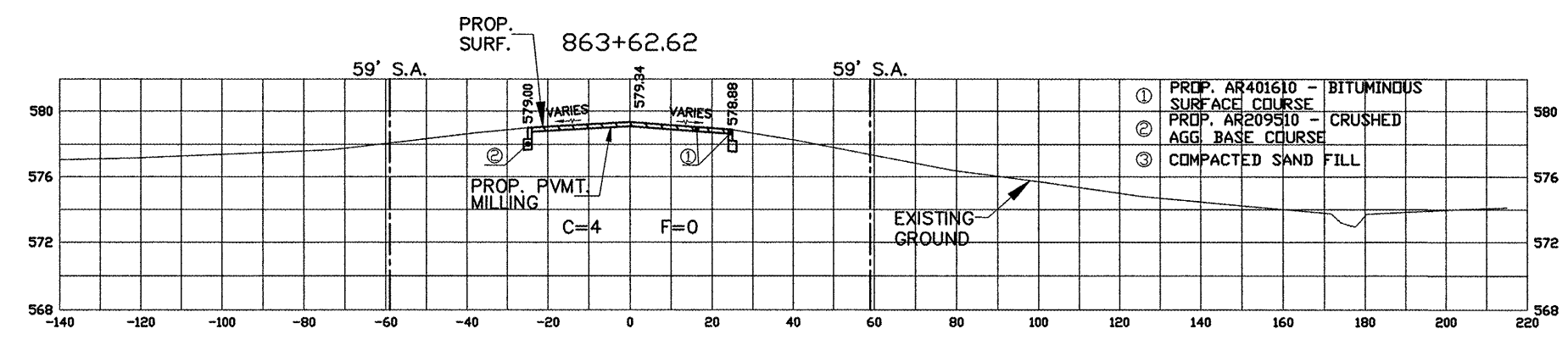


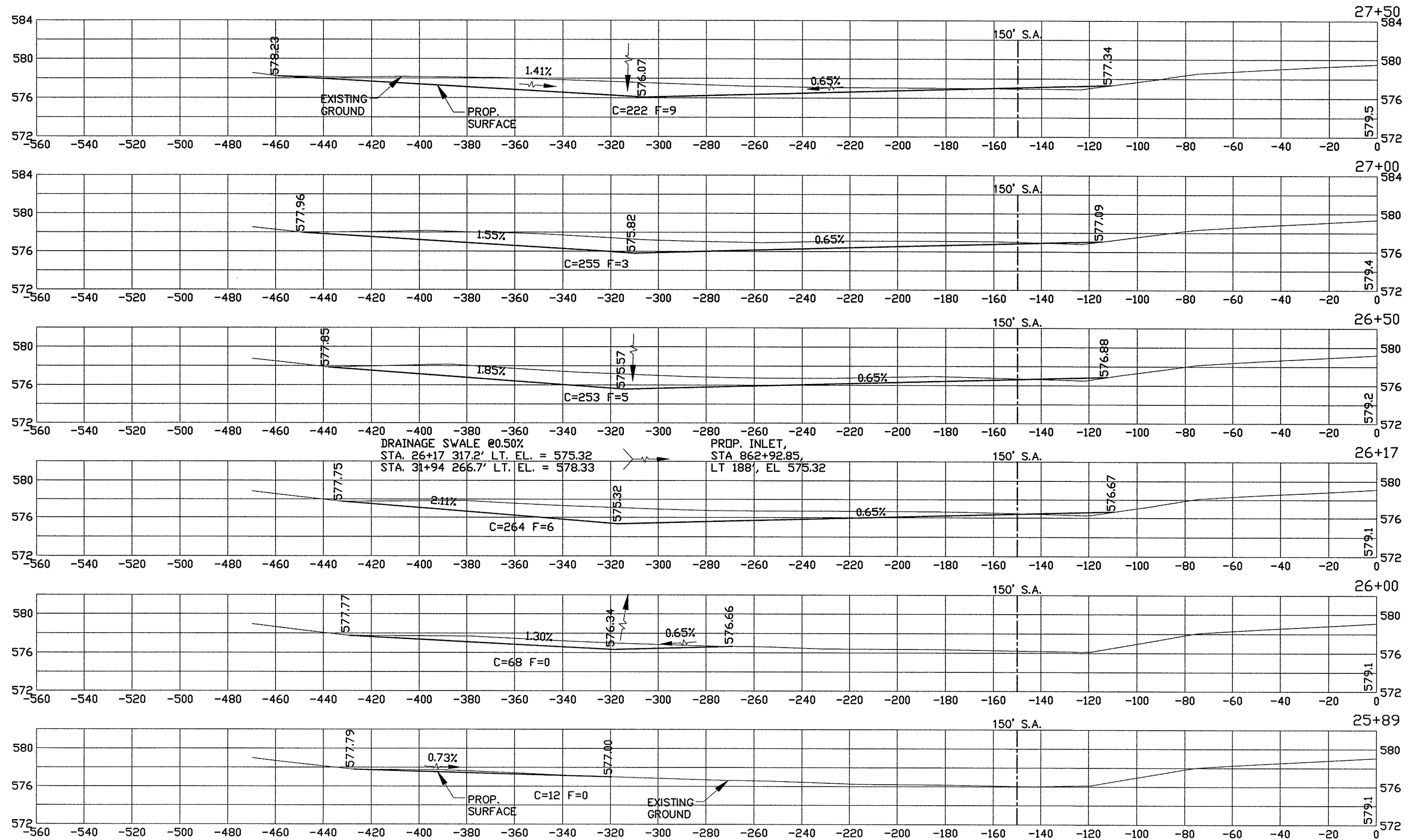


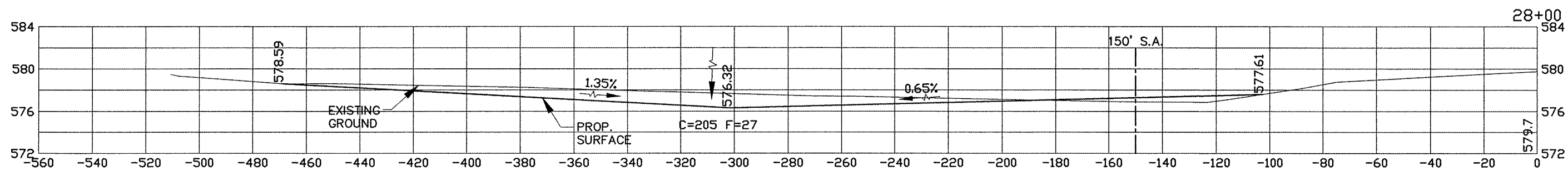
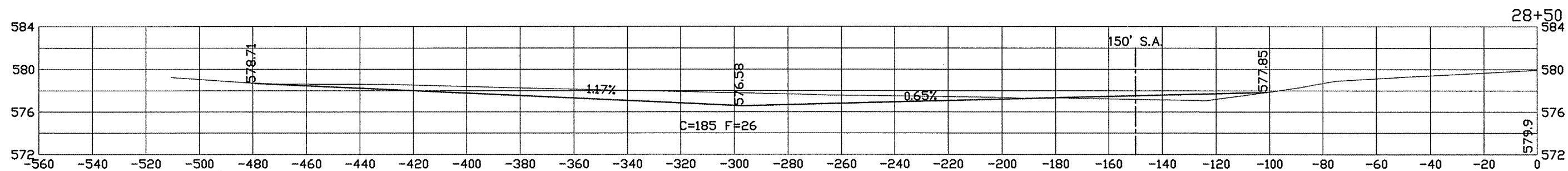
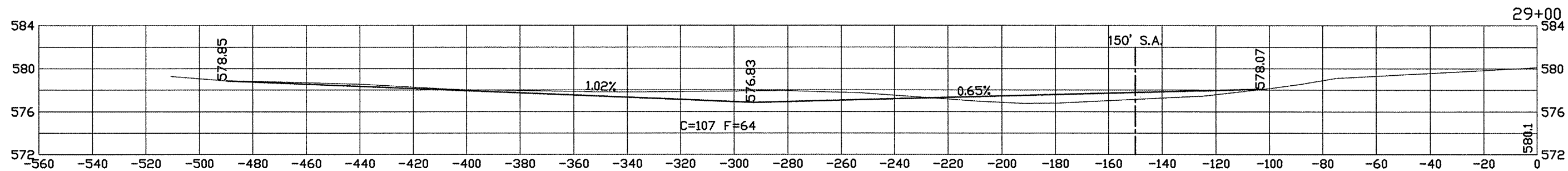
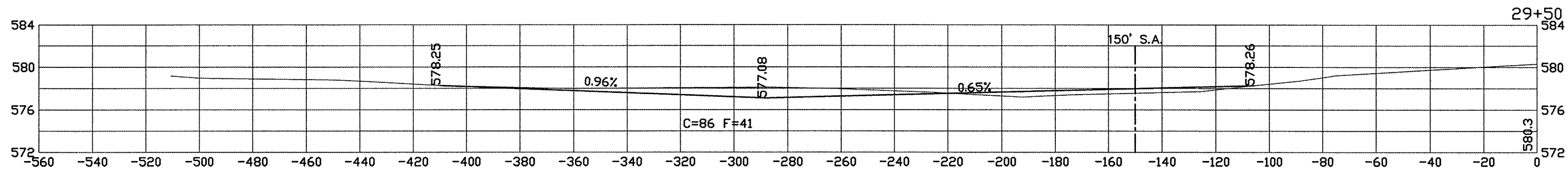
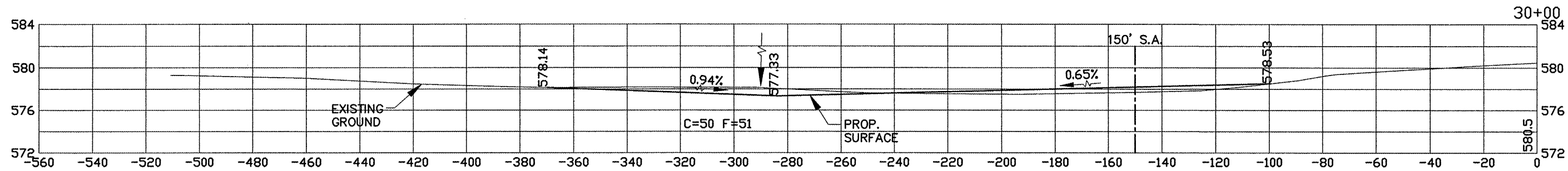


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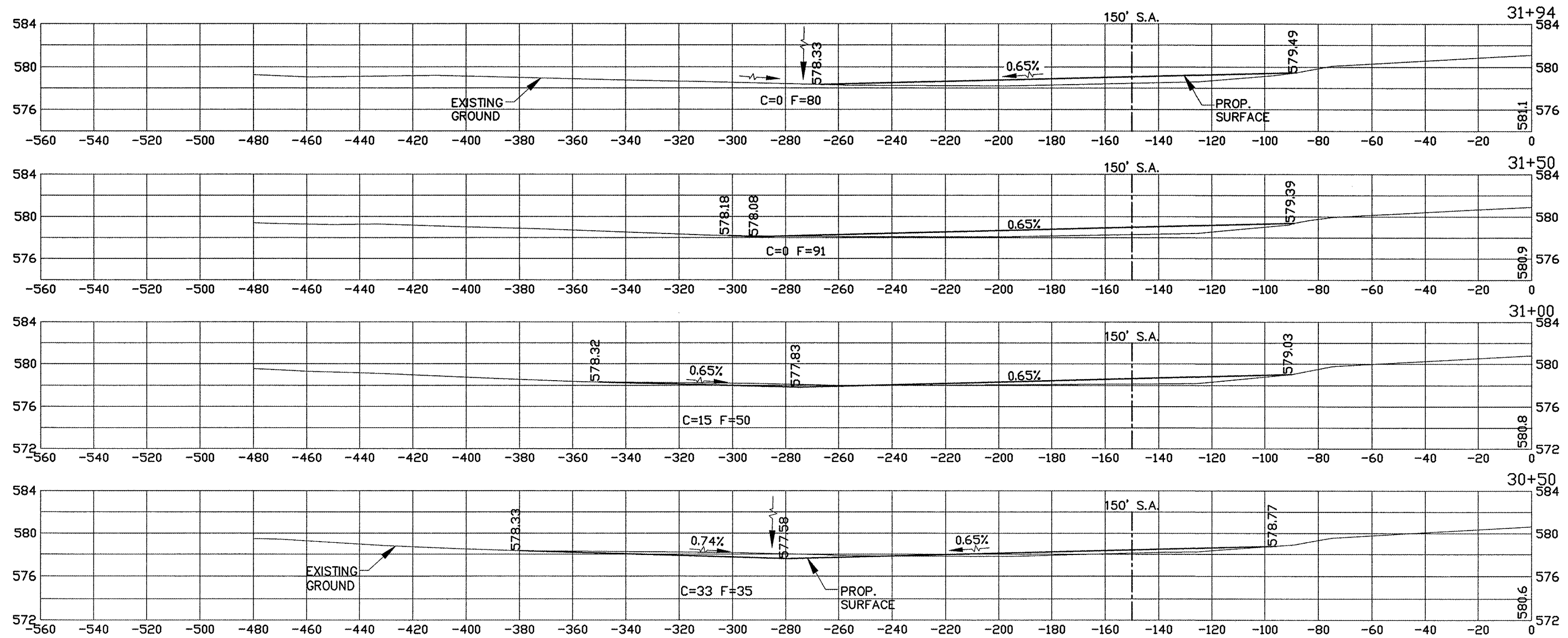
END EARTHWORK, STA 863+65,
MEET EXIST GROUND

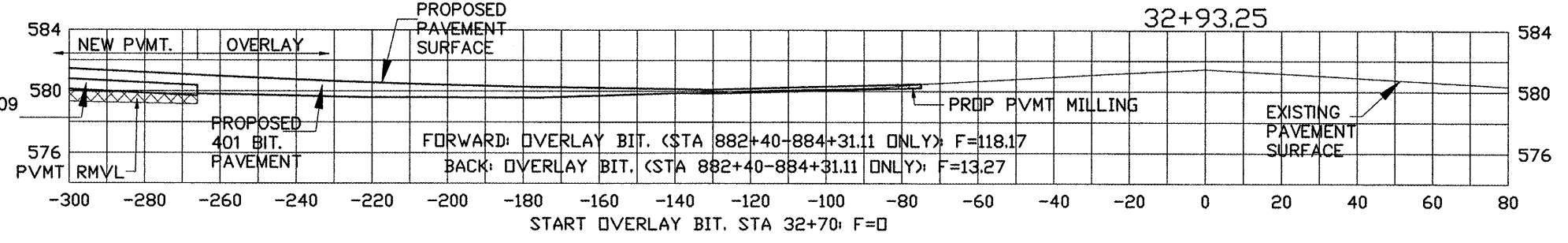
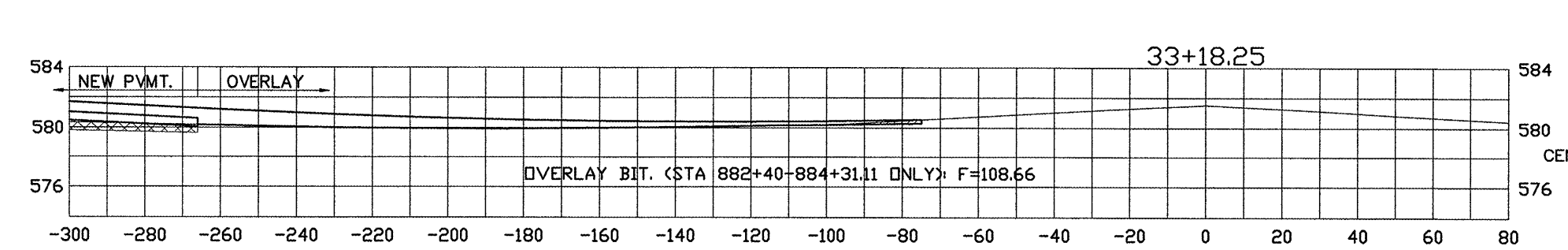
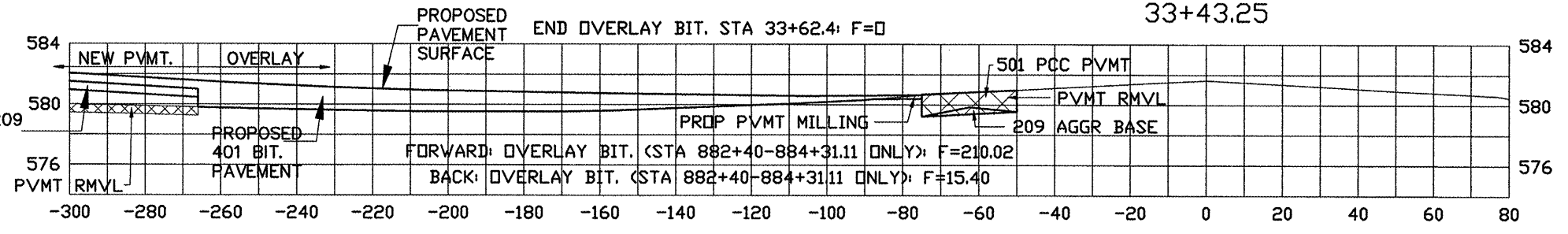
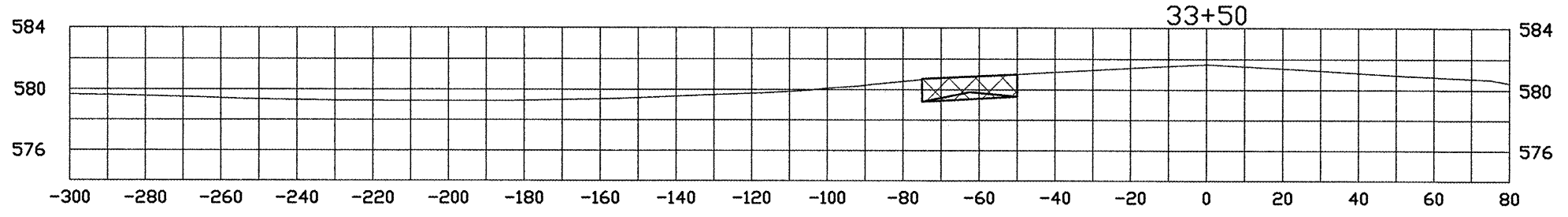
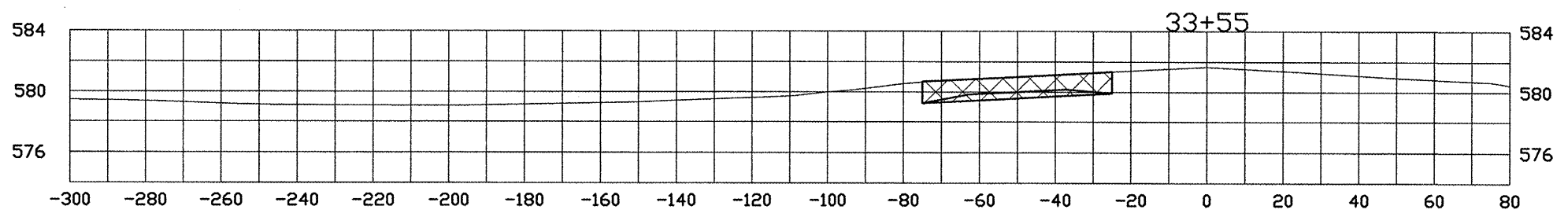
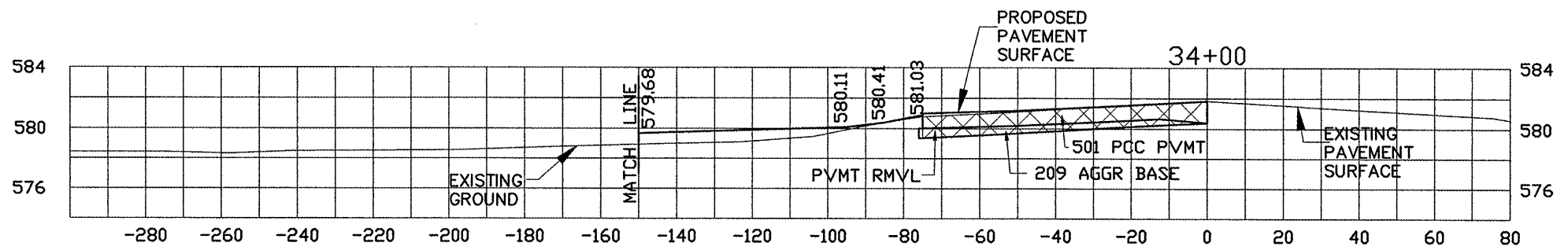






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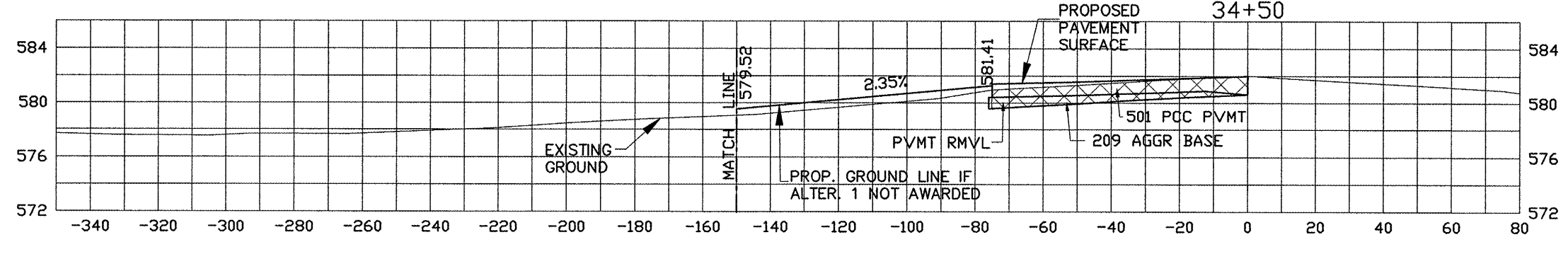
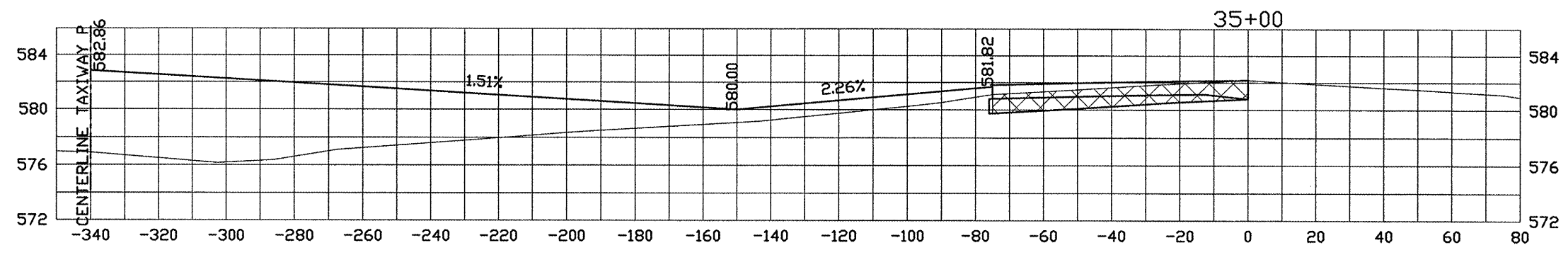
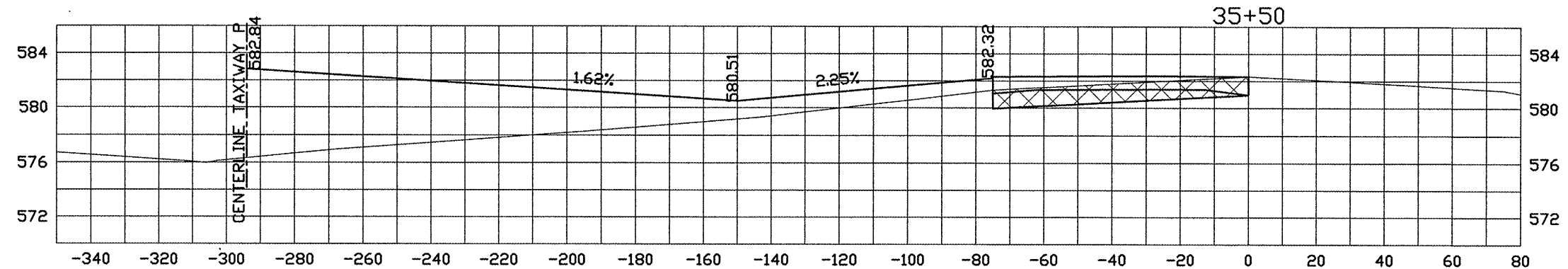
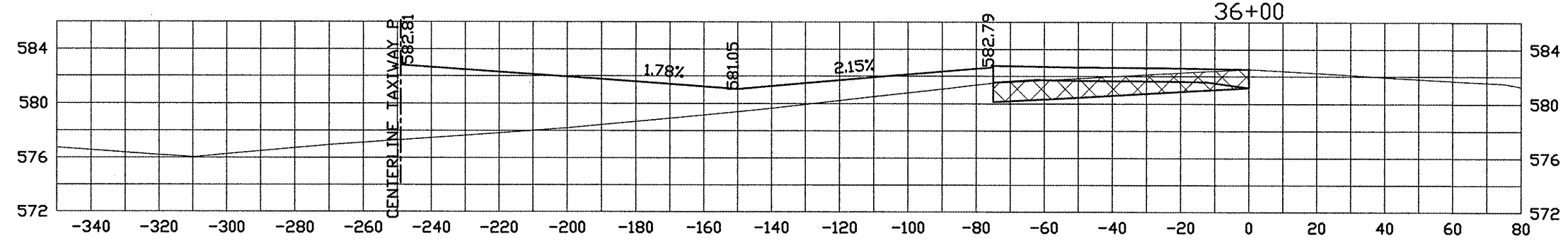
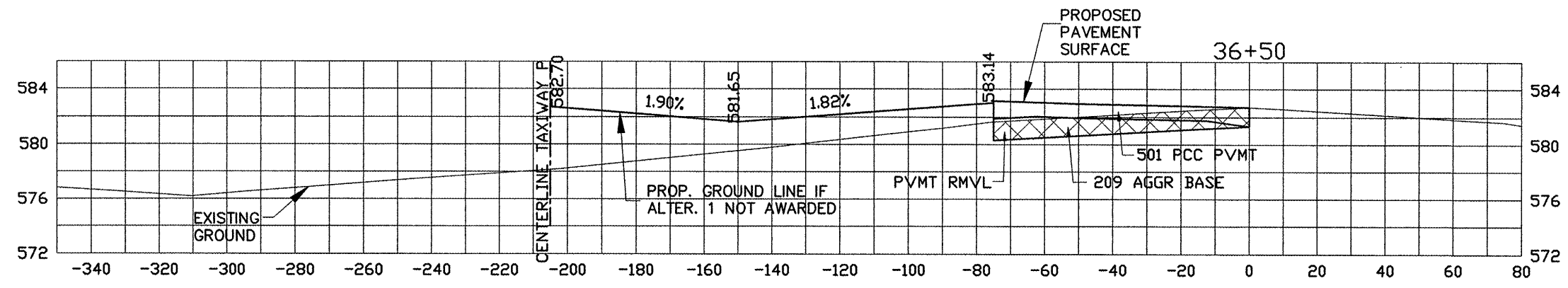


SHEETS 71 THROUGH 75 SHOW RUNWAY 5-23 SHOULDER / SAFETY AREA EMBANKMENT GRADES AND ELEVATIONS THAT ARE TO BE USED IF THE ADDITIVE ALTERNATES ARE NOT AWARDED. RUNWAY 5-23 EARTHWORK VOLUMES BETWEEN STATIONS 32+00 AND 42+50 ARE INCLUDED IN THE TAXIWAY P CROSS SECTIONS. SEE SHEETS 59 THROUGH 62 FOR T-P EARTHWORK END AREA DATA.

PROFILE OF NORTH EDGE OF OLD TAXY E

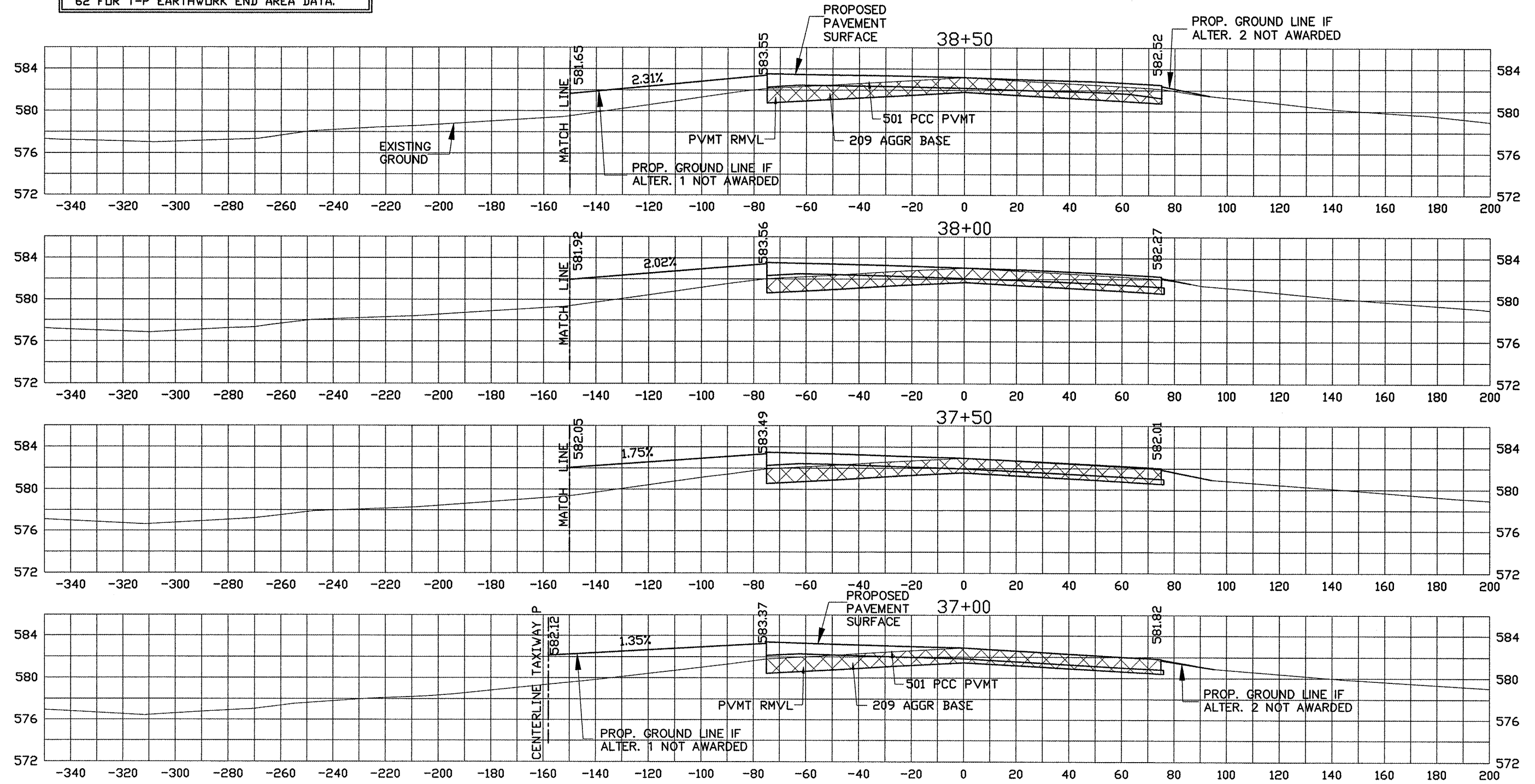
CENTERLINE PROFILE OF OLD TAXY E

PROFILE OF SOUTH EDGE OF OLD TAXY E

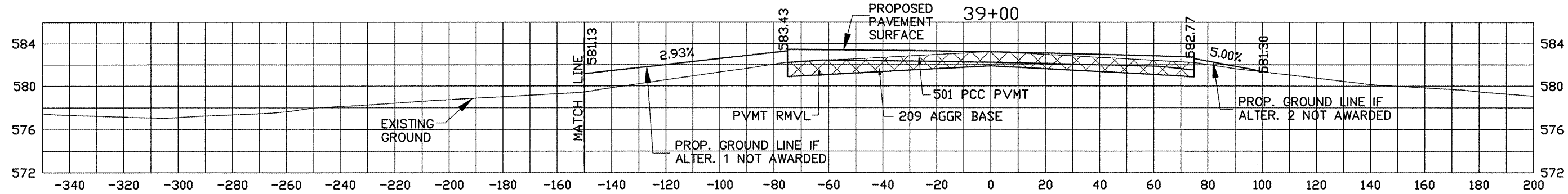
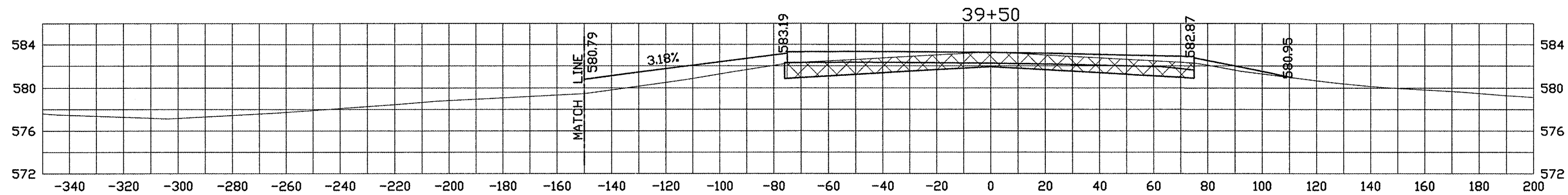
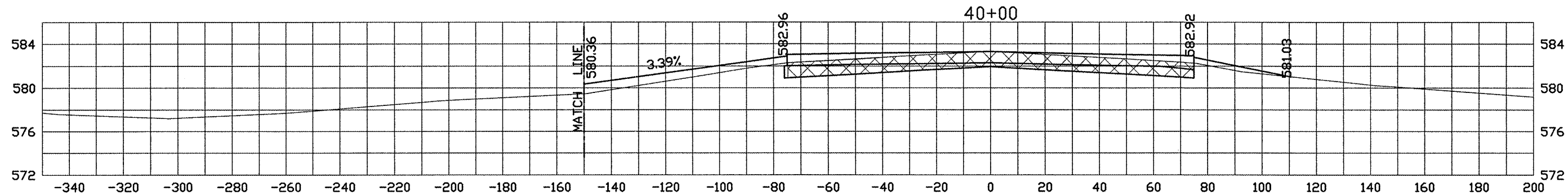
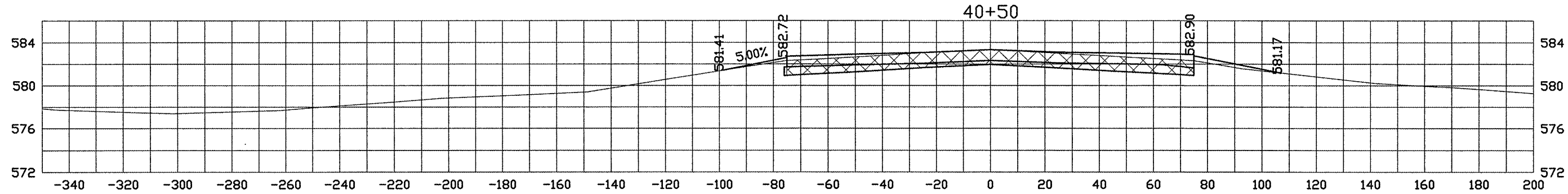
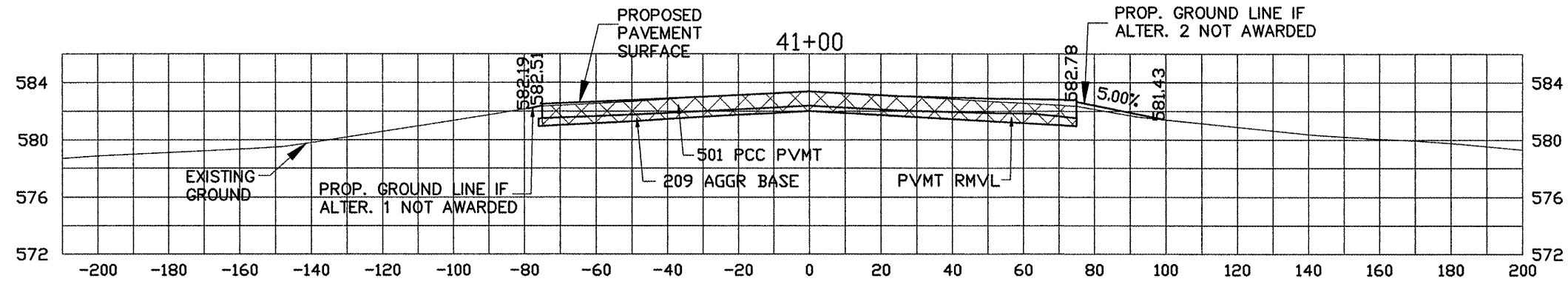


SHEETS 71 THROUGH 75 SHOW RUNWAY 5-23 SHOULDER / SAFETY AREA EMBANKMENT GRADES AND ELEVATIONS THAT ARE TO BE USED IF THE ADDITIVE ALTERNATES ARE NOT AWARDED. RUNWAY 5-23 EARTHWORK VOLUMES BETWEEN STATIONS 32+00 AND 42+50 ARE INCLUDED IN THE TAXIWAY P CROSS SECTIONS. SEE SHEETS 59 THROUGH 62 FOR T-P EARTHWORK END AREA DATA.

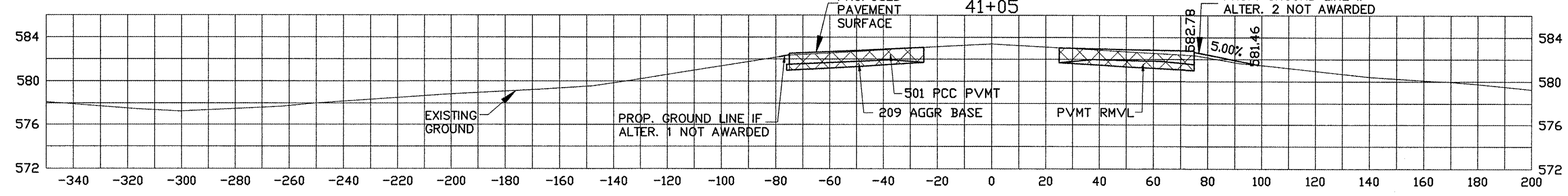
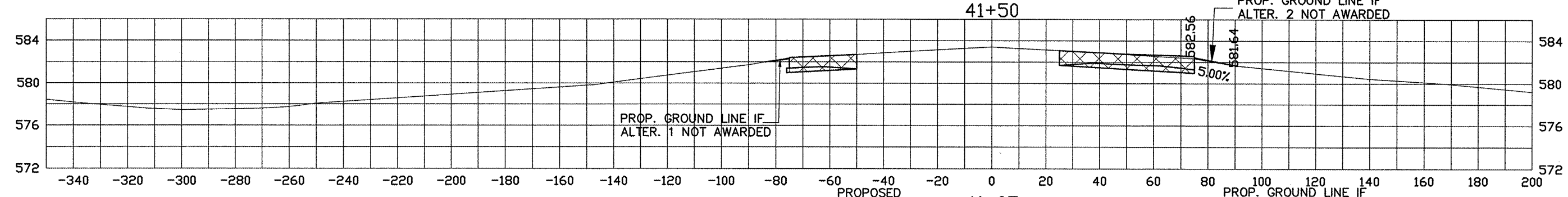
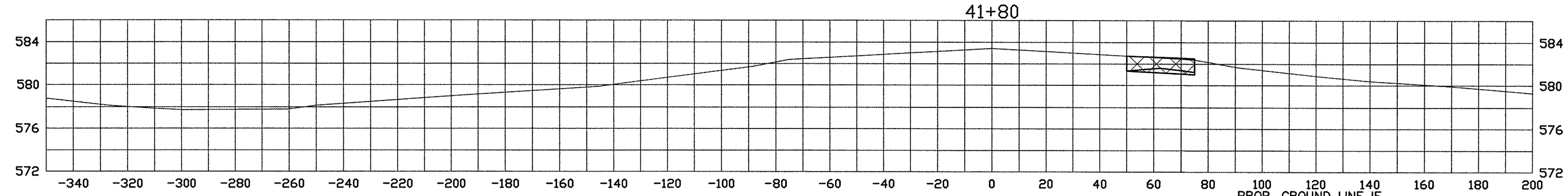
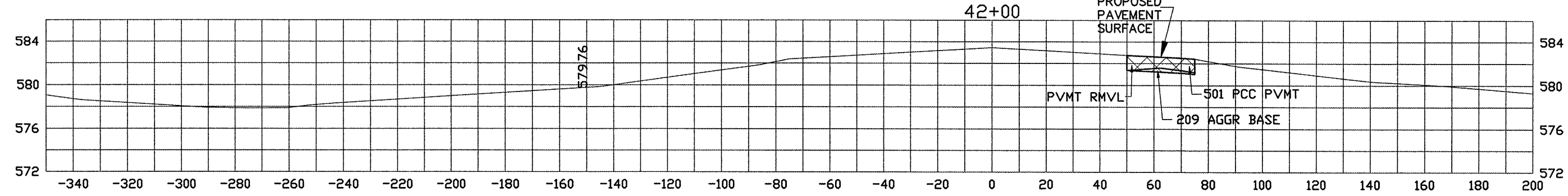
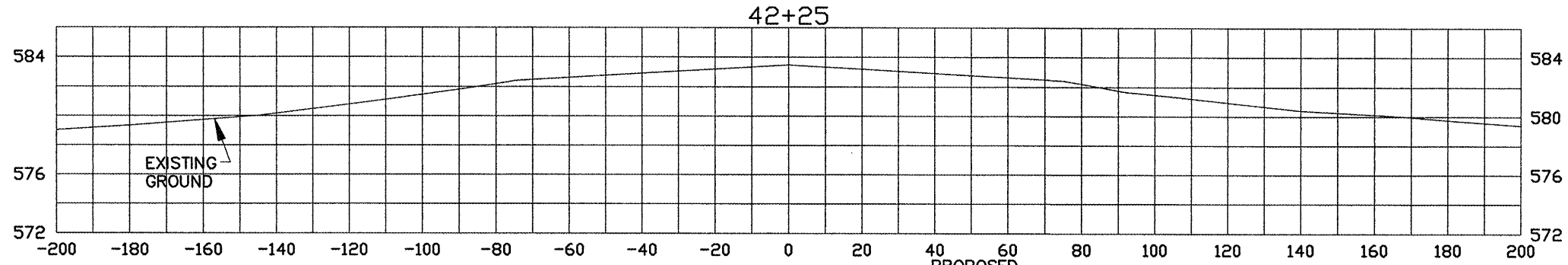
SHEETS 71 THROUGH 75 SHOW RUNWAY 5-23 SHOULDER / SAFETY AREA EMBANKMENT GRADES AND ELEVATIONS THAT ARE TO BE USED IF THE ADDITIVE ALTERNATES ARE NOT AWARDED. RUNWAY 5-23 EARTHWORK VOLUMES BETWEEN STATIONS 32+00 AND 42+50 ARE INCLUDED IN THE TAXIWAY P CROSS SECTIONS. SEE SHEETS 59 THROUGH 62 FOR T-P EARTHWORK END AREA DATA.



SHEETS 71 THROUGH 75 SHOW RUNWAY 5-23 SHOULDER / SAFETY AREA EMBANKMENT GRADES AND ELEVATIONS THAT ARE TO BE USED IF THE ADDITIVE ALTERNATES ARE NOT AWARDED. RUNWAY 5-23 EARTHWORK VOLUMES BETWEEN STATIONS 32+00 AND 42+50 ARE INCLUDED IN THE TAXIWAY P CROSS SECTIONS. SEE SHEETS 59 THROUGH 62 FOR T-P EARTHWORK END AREA DATA.

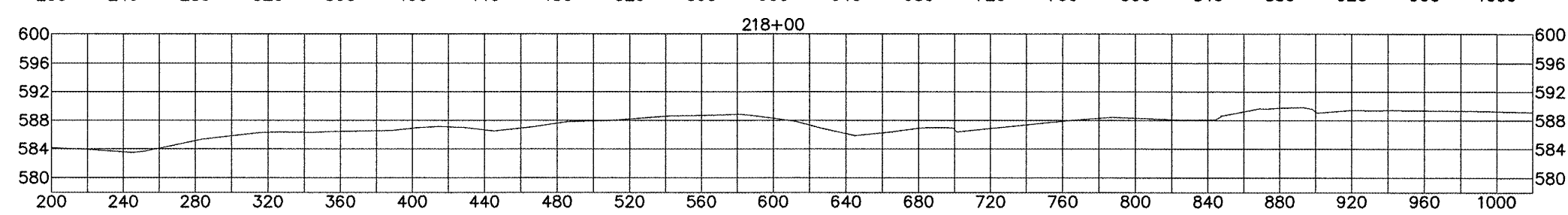
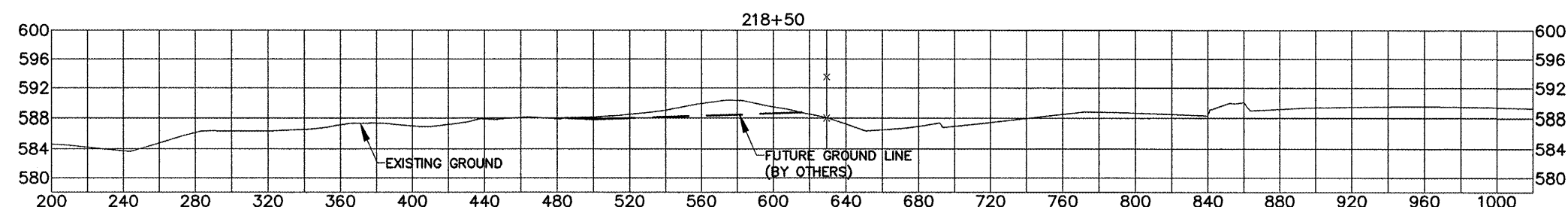
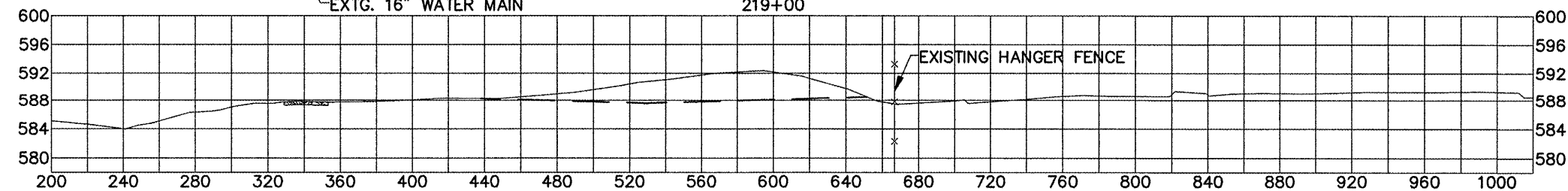
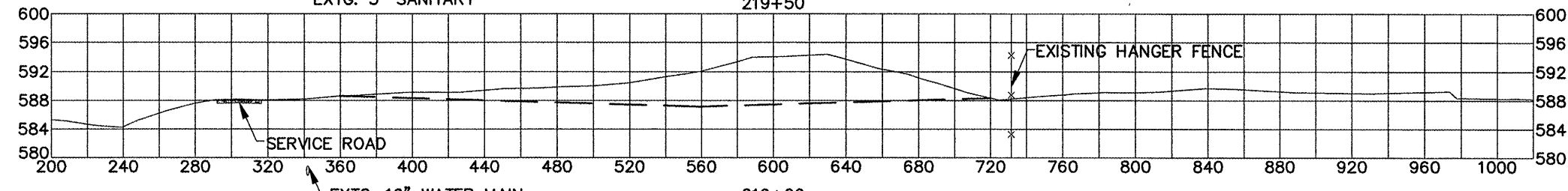
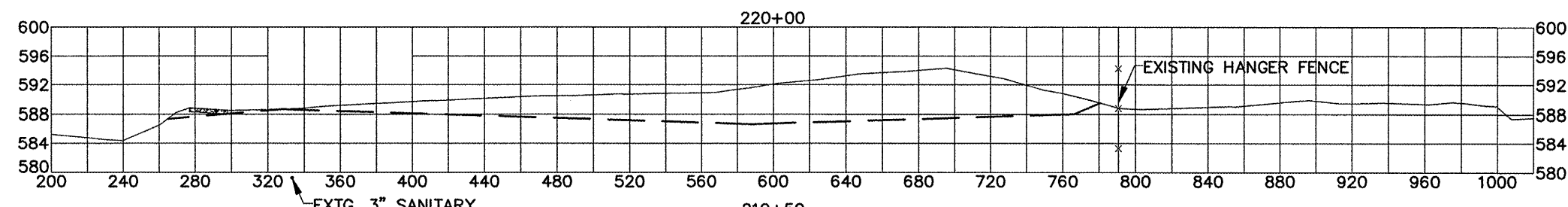
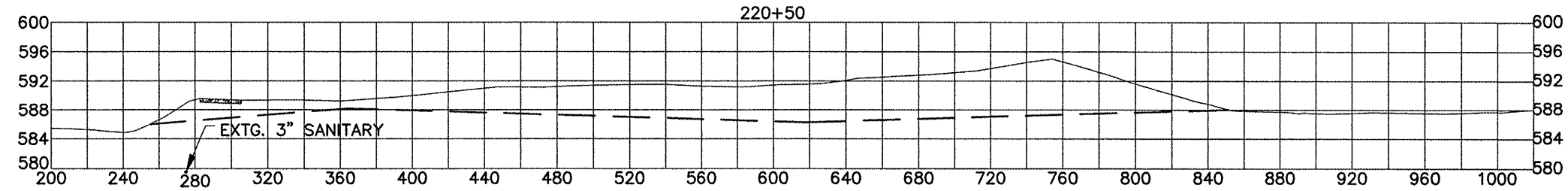
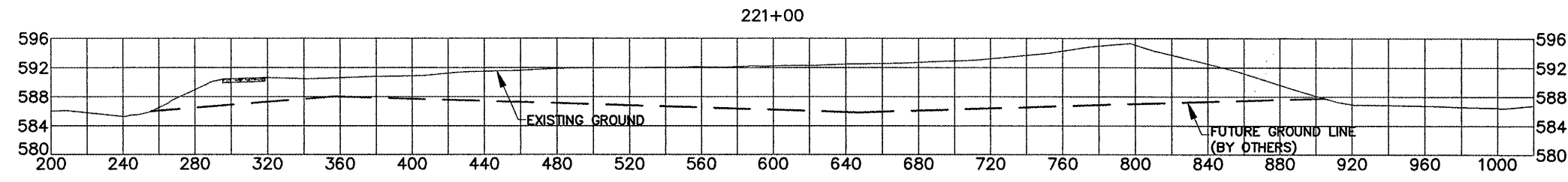


SHEETS 71 THROUGH 75 SHOW RUNWAY 5-23 SHOULDER / SAFETY AREA EMBANKMENT GRADES AND ELEVATIONS THAT ARE TO BE USED IF THE ADDITIVE ALTERNATES ARE NOT AWARDED. RUNWAY 5-23 EARTHWORK VOLUMES BETWEEN STATIONS 32+00 AND 42+50 ARE INCLUDED IN THE TAXIWAY P CROSS SECTIONS. SEE SHEETS 59 THROUGH 62 FOR T-P EARTHWORK END AREA DATA.



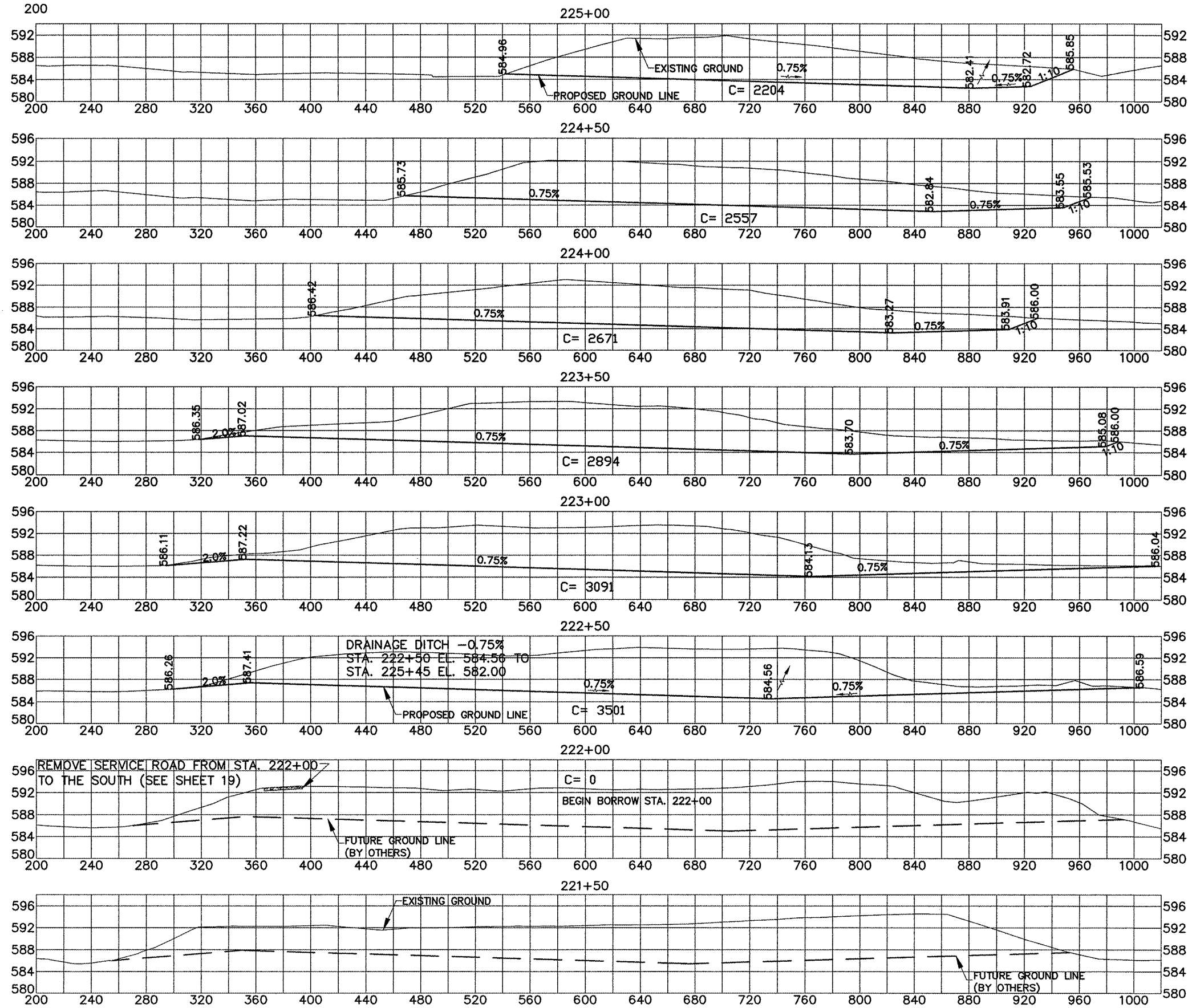
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QUAD CITY INTERNATIONAL AIRPORT
TAXIWAY P, PHASE II - MIDDLE PAVING
ILL. MLI-3854, QU008
SHEET 76 OF 82

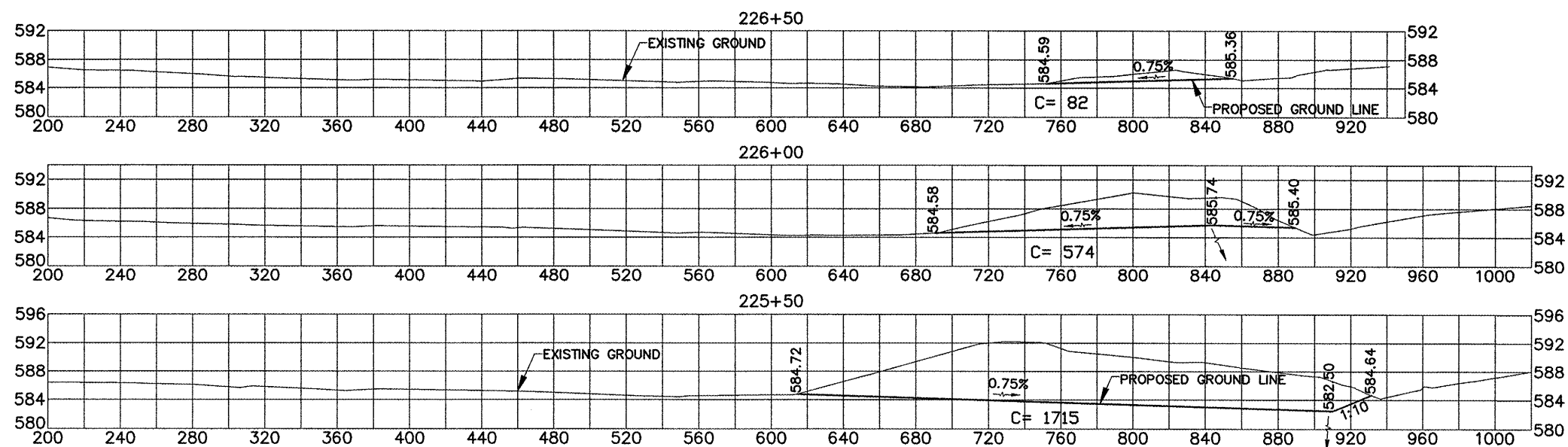


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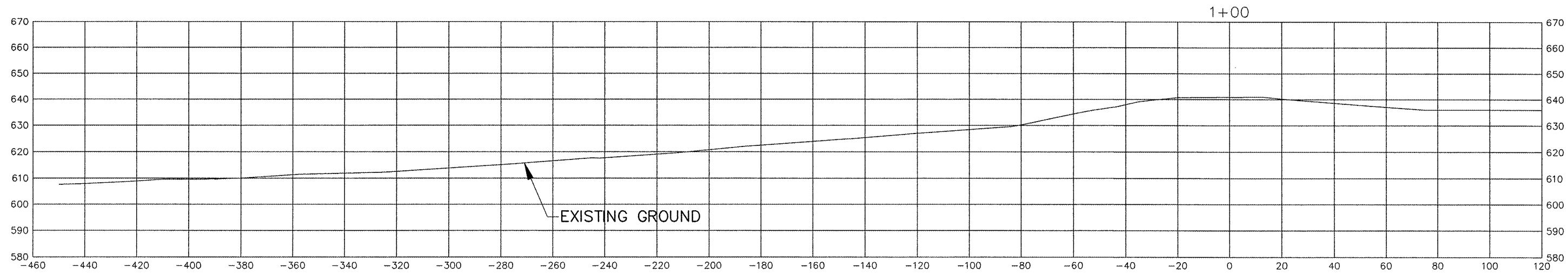
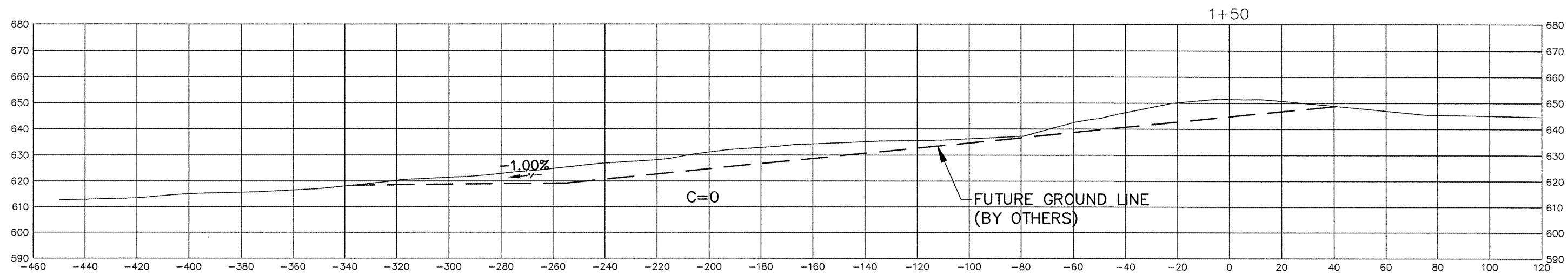
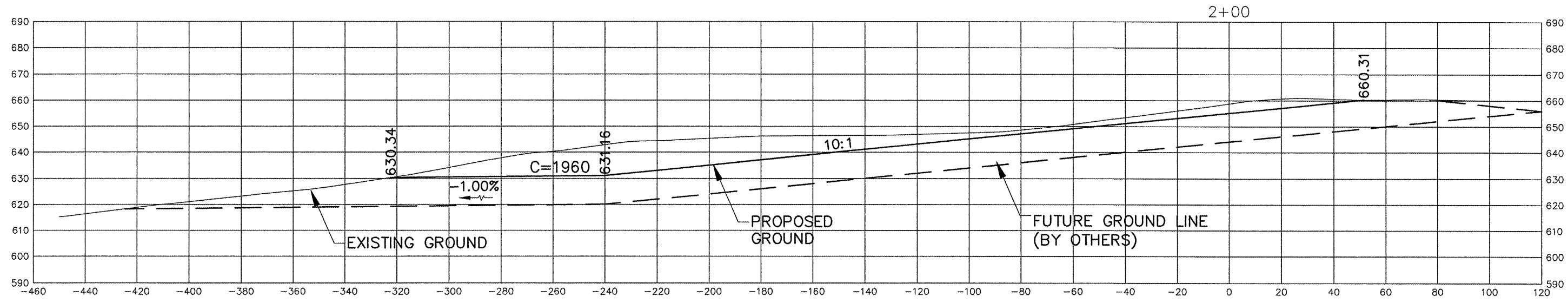
QUAD CITY INTERNATIONAL AIRPORT
 TAXIWAY P, PHASE II - MIDDLE PAVING
 ILL. MLI-3854, QU008
 SHEET 77 OF 82



QUAD CITY INTERNATIONAL AIRPORT
 TAXIWAY P, PHASE II - MIDDLE PAVING
 ILL. MLI-3854, QU008
 SHEET 78 OF 82

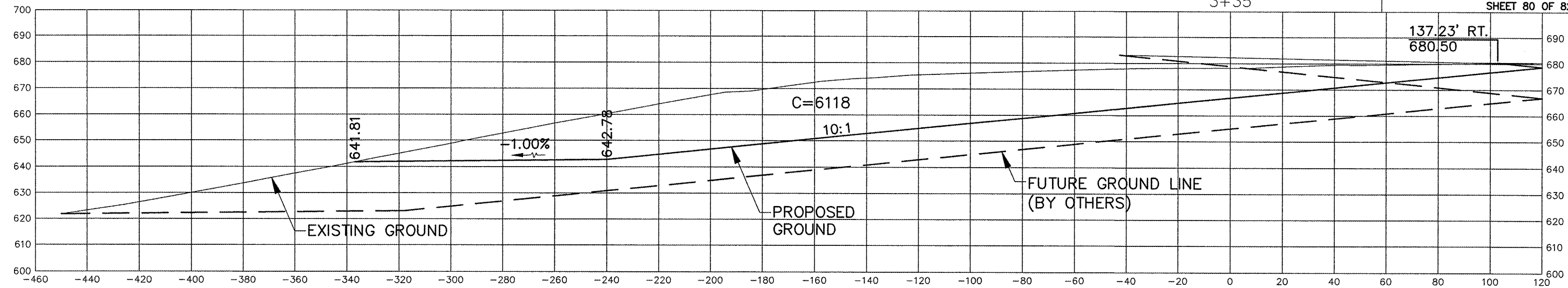


INLET SPECIAL (SINGLE)
 STA. 225+45.2 909.50' RT.
 GRATE EL. 582.00
 FL. EL. 580.10'

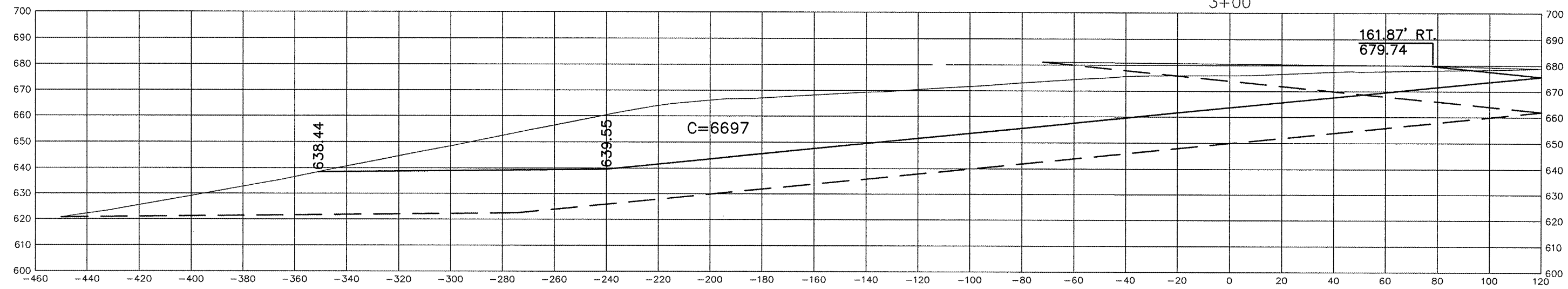


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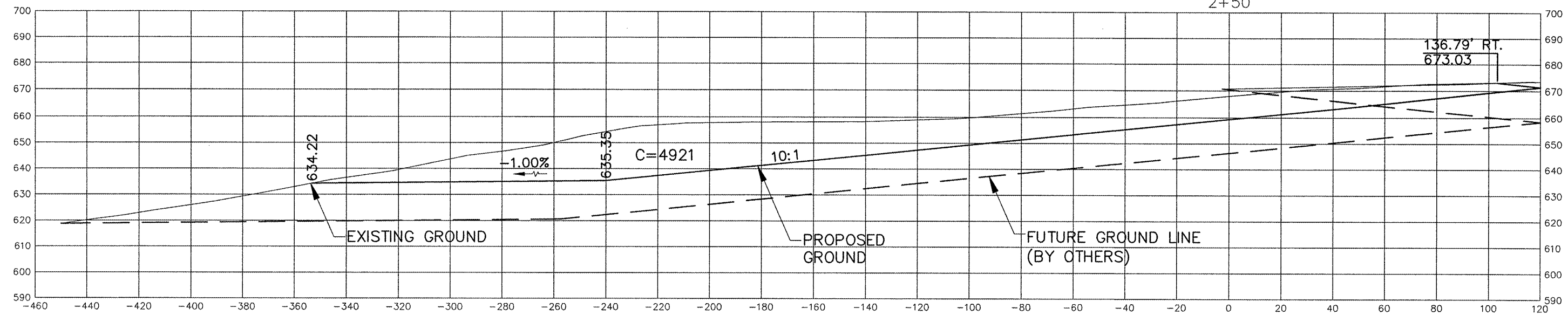
3+35



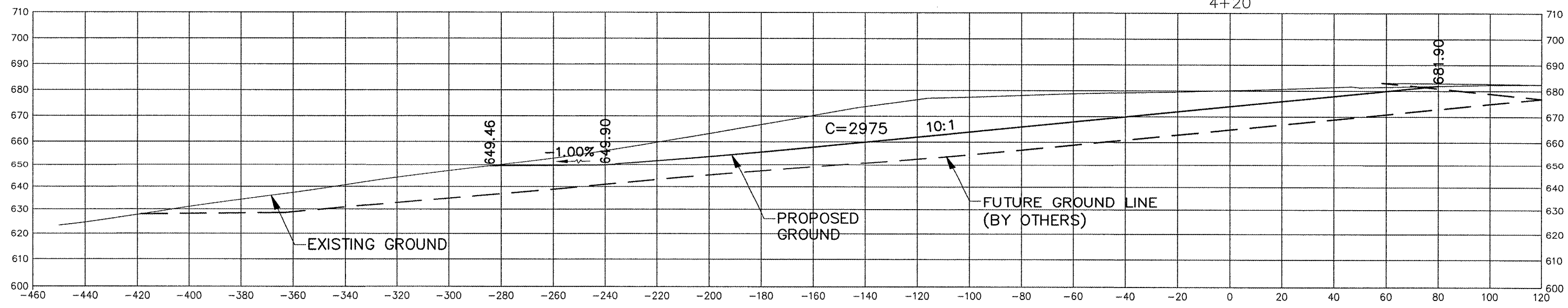
3+00



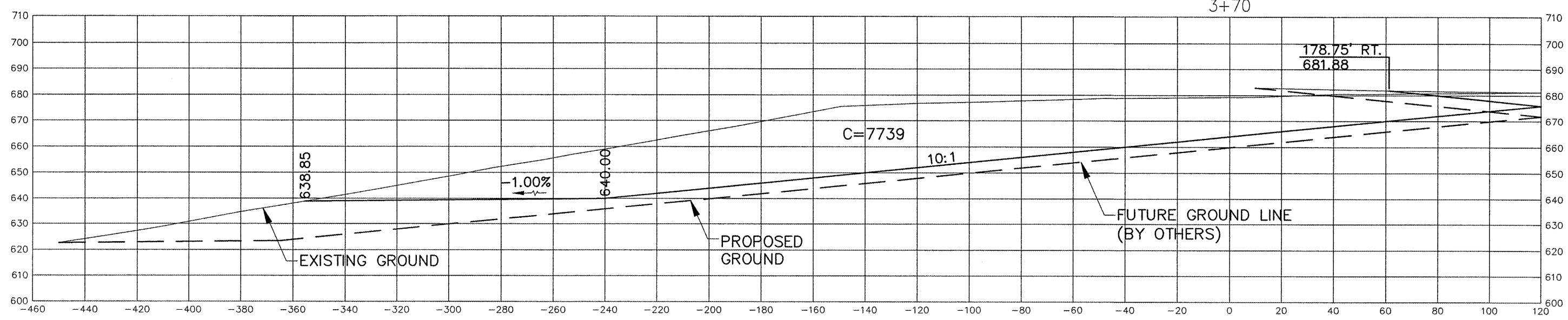
2+50



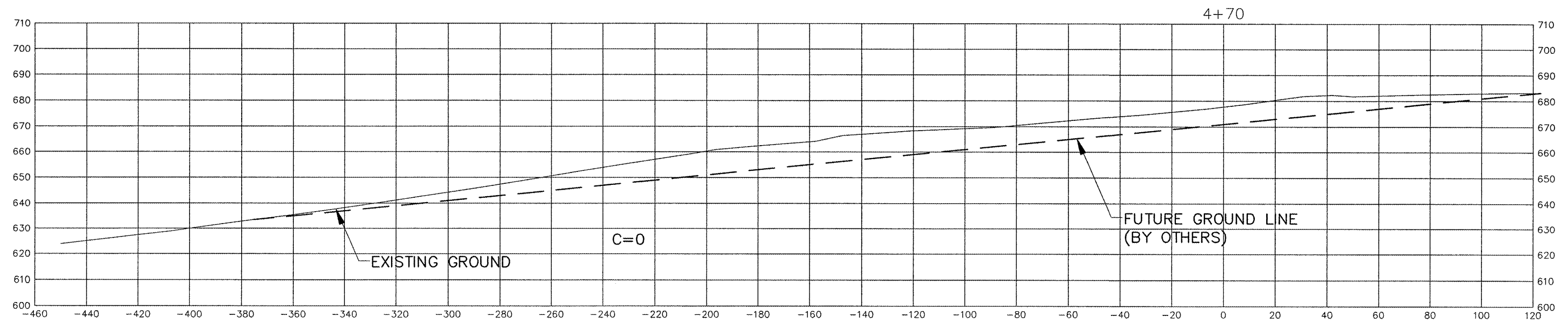
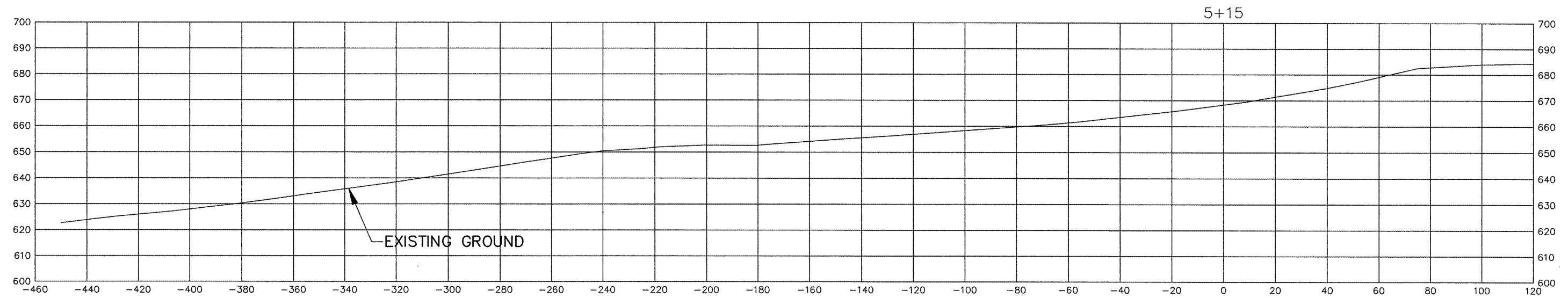
4+20



3+70



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SOUTH OFFSITE BORROW EXCAVATION AREA, STA 4+70 TO STA 5+15

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