

FOR INDEX OF SHEETS, SEE SHEET NO. 3

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

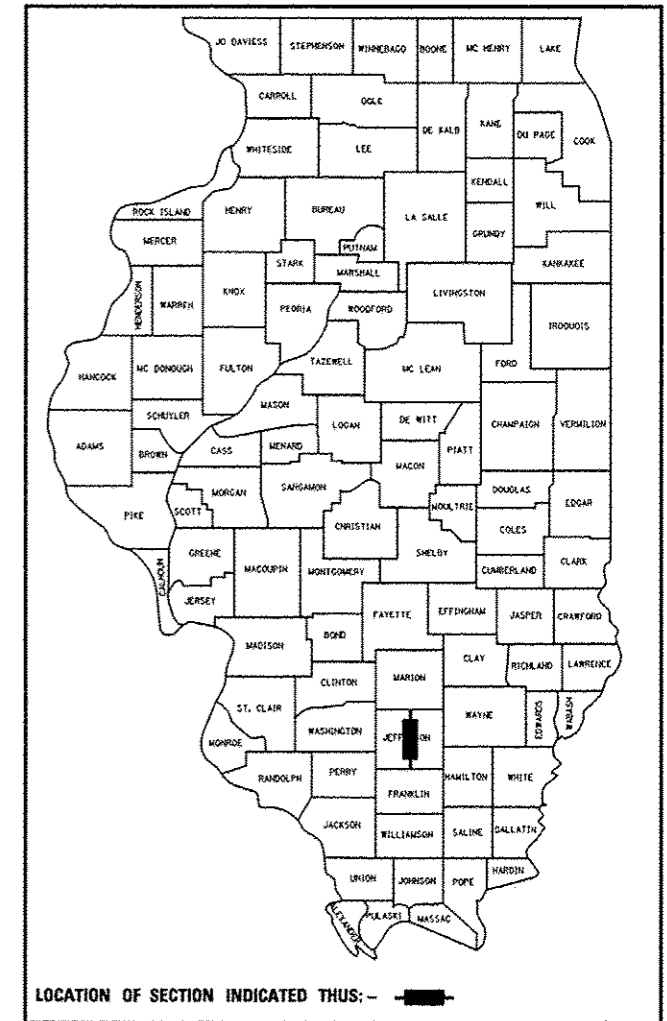
**PROPOSED
HIGHWAY PLANS**

**FAI ROUTE 57 (I-57)
SECTION (41-1-1)RS-1
JEFFERSON COUNTY
PROJ. NO. ACNHPP-0057(316)
RESURFACING**

C-99-042-14

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(41-1-1)RS-1	JEFFERSON	53	1
		ILLINOIS	CONTRACT NO. 78416	

D-99-036-14



TRAFFIC DATA

I-57 TRAFFIC DATA
JEFFERSON COUNTY
ADT 29,070 (2014)
PV = 17,340
MU = 10,405
SU = 1,325
TRUCK % = 40%

EQUATION STATION:
EQ. STA. 557+48.74 (BK) = STA. 582B+06.07 (AH) NBL
EQ. STA. 624B+10.01 (BK) = STA. 624B+10.00 (AH) NBL

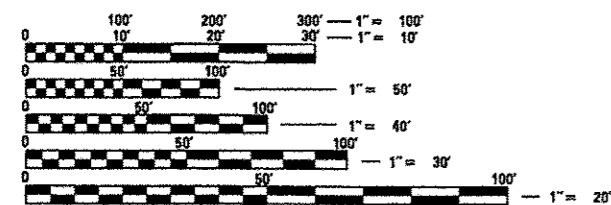
OMISSIONS STATIONS:
STA. 490+97.19 TO STA. 494+62.81 (OMISSION) NBL
STA. 607B+78.01 TO STA. 610B+15.41 (OMISSION) NBL

PROP. PROJECT ENDS
FAI 57 STA. 634B+10 (NB LANES)
FAI 57 STA. 636+72 (SB LANES)

PROP. PROJECT ENDS
FAI 64 STA. 2670+65
(EB & WB LANES)

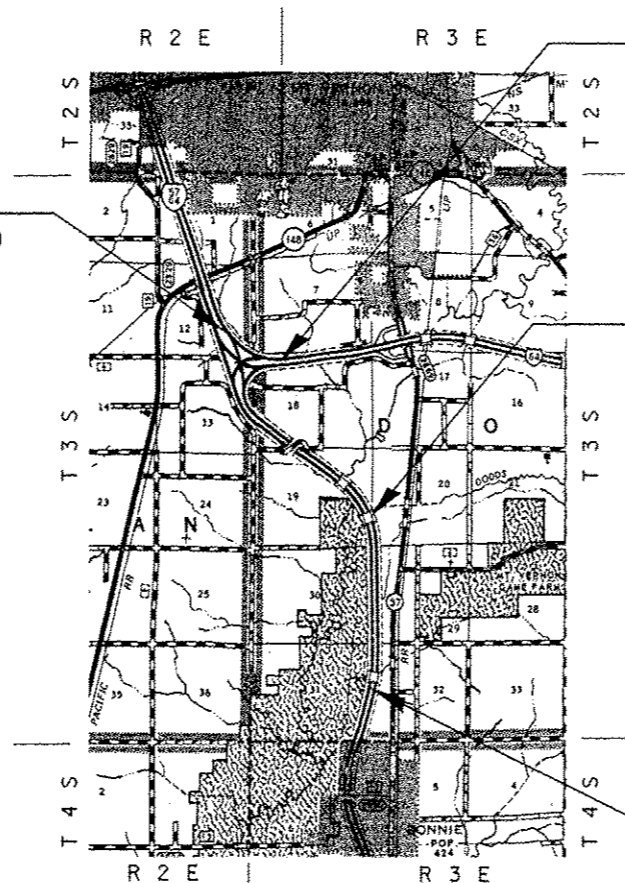
SN 041-0003 (NB)
SN 041-0004 (SB)
STA. 463+57

TOWNSHIPS: DODDS
MC CLELLAN



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811



PROP. PROJECT BEGINS
FAI 57 STA. 374+00
(NB & SB LANES)

GROSS LENGTH = 23,552.68 FT = 4.46 MILES (NBL)
OMISSIONS = 603.02 FT = 0.11 MILES (NBL)
NET LENGTH = 22,949.66 FT = 4.35 MILES (NBL)

PROJECT ENGINEER CHARLES STEIN - (618)351-5210
PROJECT DESIGNER JOHN A. BRANDON (RDWY) - (618)351-5291
PROJECT DESIGNER WAYNE HALSTEAD (STRUCTURES) - (618)351-5228

CONTRACT NO. 78416

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED Jan 30 20 15
Jeffrey L. Keim
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

Marco 20 15
John D. Baranzello, P.E.
ENGINEER OF DESIGN AND ENVIRONMENT

Mar 20 20 15
Omer Osman, P.E.
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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OF THE STATE OF ILLINOIS**

GENERAL NOTES

THE THICKNESS OF BITUMINOUS MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HOT-MIX ASPHALT MIXTURE IS PLACED.

FACTORS USED FOR ESTIMATING PLAN QUANTITIES ARE AS FOLLOWS AND SHALL NOT BE USED FOR THE BASIS OF FINAL QUANTITIES:

ALL HOT-MIX ASPHALT	2.016 TONS/CU. YD.
ALL AGGREGATE	2.05 TONS/CU. YD.

THE QUANTITY OF SHORT TERM PAVEMENT MARKING IS BASED ON ONE APPLICATION EACH FOR THE SURFACE REMOVAL AND BINDER COURSE.

AT ALL LOCATIONS WHERE THE PROPOSED HOT-MIX ASPHALT OR CONCRETE PAVEMENT JOINS AN EXISTING HOT MIX ASPHALT OR CONCRETE PAVEMENT, A FULL DEPTH SAWED JOINT SHALL BE CONSTRUCTED. THE COST OF THIS JOINT WILL BE INCLUDED IN THE COST OF THE TYPE OF SURFACE COURSE BEING CONSTRUCTED.

THE CONTRACTOR SHALL STAMP STATIONING IN THE PROPOSED HOT-MIX ASPHALT SURFACE AT 300 FT. INTERVALS ON THE OUTSIDE EDGE OF PAVEMENT AND AS DIRECTED BY THE ENGINEER. THE STATION SYMBOL STAMPS USED SHALL BE FURNISHED BY THE CONTRACTOR. THEY SHALL BE 5.5 IN. TALL, OF A DESIGN APPROVED BY THE ENGINEER, AND SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

EXISTING PIPE UNDERDRAIN OUTLETS IN THE FORESLOPES OR MEDIAN SLOPES SHALL BE PRESERVED AND PROTECTED DURING CONSTRUCTION. ANY DAMAGE TO AN UNDERDRAIN OUTLET RESULTING FROM THE CONSTRUCTION ACTIVITY SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

UNLESS OTHERWISE DIRECTED BY THE ENGINEER, BITUMINOUS RESURFACING SHALL BE PLACED IN A SEQUENCE THAT WILL MINIMIZE THE TIME THE CENTERLINE EDGE IS EXPOSED TO TRAFFIC. WHEN AT THE END OF A DAY'S OPERATION THE EXPOSED CENTERLINE EDGE IS GREATER THAN 2000 FT., THE CONTRACTOR SHALL BE REQUIRED TO PAVE IN THE ADJACENT LANE ON THE FOLLOWING WORK DAY. PRIOR TO WINTER SHUTDOWN, RESURFACING ON ADJACENT LANES IS TO BE BROUGHT UP TO THE SAME ELEVATION.

PRIOR TO PLACEMENT OF THE FINAL PAVEMENT MARKINGS THE RESIDENT ENGINEER SHALL CONTACT THE BUREAU OF OPERATIONS AND ARRANGE FOR INSPECTION AND APPROVAL OF THE PAVEMENT MARKING LAYOUT.

PATCHING QUANTITIES AND SCHEDULES ARE FOR PLANNING PURPOSES ONLY. THE FINAL LOCATIONS AND QUANTITIES WILL BE DETERMINED BY THE ENGINEER.

IN ADDITION TO THE REQUIREMENTS OF ARTICLE 107.16, THE CONTRACTOR SHALL PROTECT THE SURFACE OF ALL BRIDGE DECKS AND BRIDGE APPROACH PAVEMENTS IN A MANNER SATISFACTORY TO THE ENGINEER BEFORE ANY EQUIPMENT IS ALLOWED TO CROSS THE STRUCTURE. PROTECTION SHALL BE PROVIDED FOR ALL EQUIPMENT AS DEFINED IN ARTICLE 101.17 REGARDLESS IF TRACK MOUNTED OR WHEELED.

RECLAIMED ASPHALT PAVEMENT (RAP) WILL NOT BE ALLOWED FOR USE AS AGGREGATE IN AGGREGATE SHOULDERS, TYPE B.

ATTAINMENT OF PROPER CROWN OR SUPERELEVATION SHALL BE FULLY ACCOMPLISHED WITH THE HOT-MIX ASPHALT SURFACE REMOVAL OR HOT-MIX ASPHALT BINDER COURSE, WHEN SPECIFIED.

EXISTING PIPE UNDERDRAIN OUTLETS IN THE FORESLOPES OR MEDIAN SLOPES SHALL BE PRESERVED AND PROTECTED DURING CONSTRUCTION. ANY DAMAGE TO AN UNDERDRAIN OUTLET RESULTING FROM THE CONSTRUCTION ACTIVITY SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

COMMITMENTS

NO COMMITMENTS.

FILE NAME #	USER NAME # *USER*	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES AND COMMITMENTS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et\p4\work\pav\brandon\ja\0302650\78416-ahra.dgn	DRAWN -	REVISED -	57			(41-1)-DRS-1	JEFFERSON	53	4	
PLOT SCALE = 100.0000' / 1" =	CHECKED -	REVISED -	CONTRACT NO. 78416							
PLOT DATE = 2/2/2015	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							
SCALE:						SHEET	OF	SHEETS	STA.	TO STA.

MIXTURE REQUIREMENTS

Locations	Hot-Mix Asphalt Surface Course
Mixture Use(s):	Polymerized Hot-Mix Asphalt Surface Course, Mix E, N90
AC/PG:	SBS PG76-22
ABR % (Max):	See Special Provision
Design Air Voids:	4.0 %, 90 Gyration Design
Mixture Composition: (Gradation Mixture)	IL-9.5 mm
Friction Aggregate:	E Surface
Quality Management Program:	PPF

Locations:	Hot-Mix Asphalt Shoulders and Incidental HMA Surfacing
Mixture Use(s):	Hot-Mix Asphalt Surface Course, Mix C, N90
AC/PG:	PG64-22
ABR % (Max):	See Special Provision
Design Air Voids:	4.0 %, 90 Gyration Design
Mixture Composition: (Gradation Mixture)	IL-9.5 mm
Friction Aggregate:	C Surface
Quality Management Program:	OCP for HMA Shoulders; OC/QA for Incidental HMA Surfacing

Locations	Hot-Mix Asphalt Binder
Mixture Use(s):	Polymerized Hot-Mix Asphalt Binder, N90, IL-19.0mm
AC/PG:	SBS PG76-22
ABR % (Max):	See Special Provision
Design Air Voids:	4.0 %, 90 Gyration Design
Mixture Composition: (Gradation Mixture)	IL-19.0mm
Friction Aggregate:	None
Quality Management Program:	PPF

MTD CROSSING RESTRICTIONS TABLE

SN & CENTERLINE STATION ROUTE (FAI 57/64)	EXISTING FILL HEIGHT OVER BOX CULVERT	THICKNESS OF EXISTING PAVEMENT OVER BOX CULVERT	MTD CROSSING RESTRICTIONS
	FOOT	FOOT	
FAI 64			
SN 041-2009 (STA. 2663+00)	12.3	NA	•EMPTIED
SN 041-7007 (STA. 2636+20)	2.0	1.2	LOADED
FAI 57			
SN 041-7032 (STA. 424+20)	11.3	NA	LOADED
SN 041-7031 (STA. 396+65)	12.0	NA	LOADED
SN 041-2008 (STA. 378+75)	9.9	NA	••SPECIAL REQUIREMENTS APPLY
SN 041-7033 (STA. 537+62)	9.0	NA	LOADED
TR-379			
SN 041-0058 (STA. 527+90)	NA	NA	STRUCTURE CROSSES OVER I-57
FAI 57 NB			
SN 041-0003 (STA. 463+57)	NA	NA	•EMPTIED
SN 041-0005 (STA. 492+80)	NA	NA	•EMPTIED
SN 041-0007 (STA. 608B+89)	NA	NA	•EMPTIED
SN 041-7034 (STA. 615B+59)	12.0	NA	LOADED
FAI 57 SB			
SN 041-0004 (STA. 463+57)	NA	NA	•EMPTIED
SN 041-0006 (STA. 492+80)	NA	NA	•EMPTIED
SN 044-0059 (STA. 20E+32)	NA	NA	•EMPTIED

•THE CONTRACTOR SHALL REFER TO BDE SPECIAL PROVISION FOR MATERIAL TRANSFER DEVICE.

••THE EMPTIED MTD MUST BE TRANSPORTED OVER SN 041-2008 ON A TRANSPORT VEHICLE AND THAT INFORMATION DESCRIBING AXLE LOADS AND AXLE SPACING OF THE TRANSPORT VEHICLE MUST BE PROVIDED TO THE ENGINEER FOR REVIEW BY THE BUREAU OF BRIDGES AND STRUCTURES.

SUMMARY OF QUANTITIES - CONT

COUNTY:	JEFFERSON
ROUTE:	FAI 57, I-57
FUNDING:	90% FED 10% STATE
LOCATION:	RURAL

CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	RURAL	
				ROADWAY 0004	BRIDGE 0014
44213204	TIE BARS 3/4"	EACH	74	74	
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	1642	1642	
48203100	HOT-MIX ASPHALT SHOULDERS	TON	10436	10436	
50102400	CONCRETE REMOVAL	CU YD	1.3		1.3
50300255	CONCRETE SUPERSTRUCTURE	CU YD	0.9		0.9
50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	1560		1560
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	100		100
52100010	ELASTOMERIC BEARING ASSEMBLY, TYPE I	EACH	5		5
52100020	ELASTOMERIC BEARING ASSEMBLY, TYPE II	EACH	5		5
52100520	ANCHOR BOLTS, 1"	EACH	20		20
60600605	CONCRETE CURB, TYPE B	FOOT	90	90	
* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	3275	3275	
* 63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	1	1	
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	5	5	
	* SPECIALTY ITEMS				

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SUMMARY OF QUANTITIES - CONT

COUNTY:	JEFFERSON	
ROUTE:	FAI 57, I-57	
FUNDING:	90% FED 10% STATE	
LOCATION:	RURAL	RURAL
TOTAL QUANTITY	ROADWAY 0004	BRIDGE 0014

CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	ROADWAY 0004	BRIDGE 0014
* 63100089	TRAFFIC BARRIER TERMINAL, TYPE 6B	EACH	8	8	
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	13	13	
* 63100169	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) FLARED	EACH	4	4	
63200310	GUARDRAIL REMOVAL	FOOT	3634	3634	
63500105	DELINEATORS	EACH	211	211	
64200116	SHOULDER RUMBLE STRIPS, 16 INCH	FOOT	113975	113975	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	5	5	
67100100	MOBILIZATION	L SUM	1	1	
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	4	4	
70100700	TRAFFIC CONTROL AND PROTECTION, STANDARD 701406	L SUM	1	1	
70100800	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	L SUM	1	1	
70100820	TRAFFIC CONTROL AND PROTECTION, STANDARD 701451	L SUM	1	1	
70100825	TRAFFIC CONTROL AND PROTECTION, STANDARD 701456	L SUM	1	1	
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	10	10	
	* SPECIALTY ITEMS				

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SUMMARY OF QUANTITIES - CONT

COUNTY:	JEFFERSON	
ROUTE:	FAI 57, I-57	
FUNDING:	90% FED 10% STATE	
LOCATION:	RURAL	RURAL
TOTAL QUANTITY	ROADWAY 0004	BRIDGE 0014

CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	ROADWAY 0004	BRIDGE 0014
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	25	25	
70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	1	
70300100	SHORT TERM PAVEMENT MARKING	FOOT	20898	20814	84
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	6966	6937	29
* 78003110	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 4"	FOOT	115585	115105	480
* 78003130	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 6"	FOOT	14364	14304	60
* 78003140	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 8"	FOOT	4204	4204	
* 78003150	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 12"	FOOT	246	246	
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	927	927	
* 78200410	GUARDRAIL MARKERS, TYPE A	EACH	60	60	
* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	17	17	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	927	927	
X0322278	RODENT SHIELDS	EACH	149	149	
X2503100	MOWING	UNIT	459	459	
<input type="checkbox"/> NON-PART (100% STATE) * SPECIALTY ITEMS					

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SUMMARY OF QUANTITIES - CONT

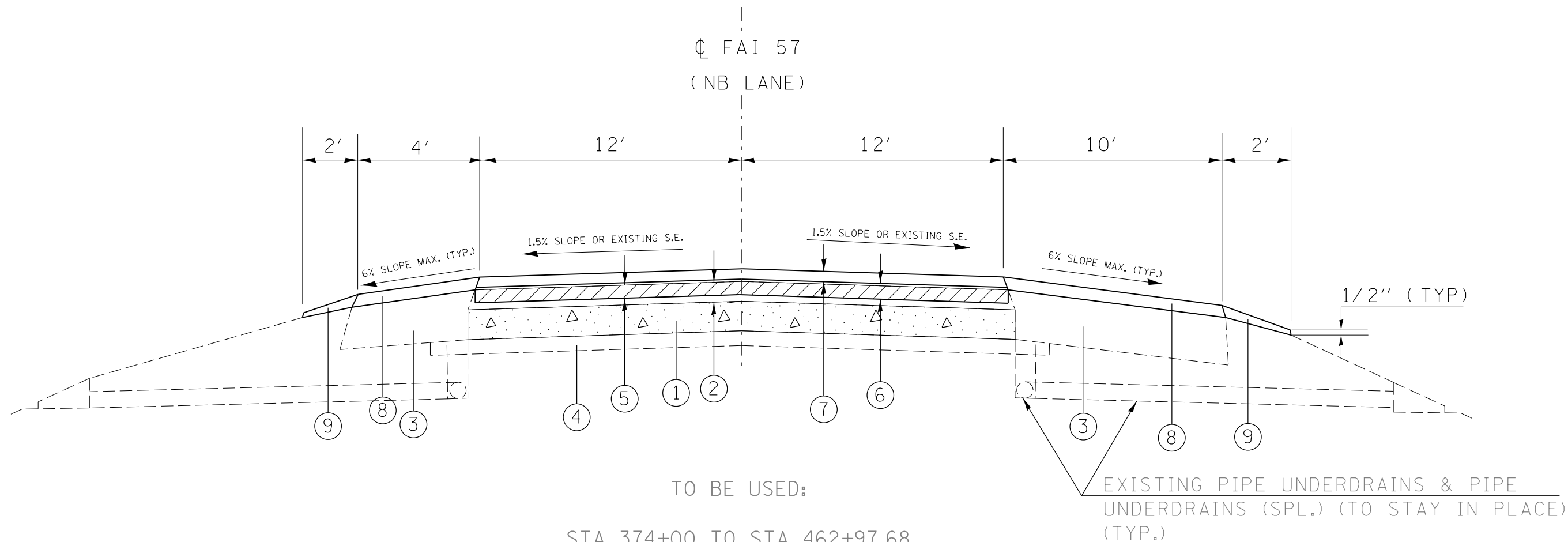
COUNTY:	JEFFERSON	
ROUTE:	FAI 57. I-57	
FUNDING:	90% FED 10% STATE	
LOCATION:	RURAL	RURAL
	ROADWAY	BRIDGE
	0004	0014

CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	ROADWAY	BRIDGE
* X6330900	VERTICAL ADJUSTMENT OF GUARDRAIL	FOOT	5837	5837	
X7010410	SPEED DISPLAY TRAILER	CAL MO	5	5	
Z0001600	APPROACH SLAB REPAIR	SO YD	71.4	71.4	
Z0001899	JACK AND REMOVE EXISTING BEARINGS	EACH	10		10
Z0004556	HOT-MIX ASPHALT SURFACE REMOVAL (DECK)	SO YD	614		614
Z0016001	DECK SLAB REPAIR (FULL DEPTH, TYPE 1)	SO YD	0.3		0.3
Z0021907	SILICONE JOINT SEALER, 1.75"	FOOT	164		164
Z0021912	SILICONE JOINT SEALER, 2.5"	FOOT	74		74
Z0034105	MATERIAL TRANSFER DEVICE	TON	33808	33808	
Z0041895	POLYMER CONCRETE	CU FT	16.7		16.7

10 * SPECIALTY ITEMS

TYPICAL SECTION #1
FAI 57 MAINLINE

(NOT TO SCALE)



TO BE USED:

STA 374+00 TO STA 462+97.68
STA 464+16.32 TO STA 490+97.19
STA 494+62.81 TO STA 551+50

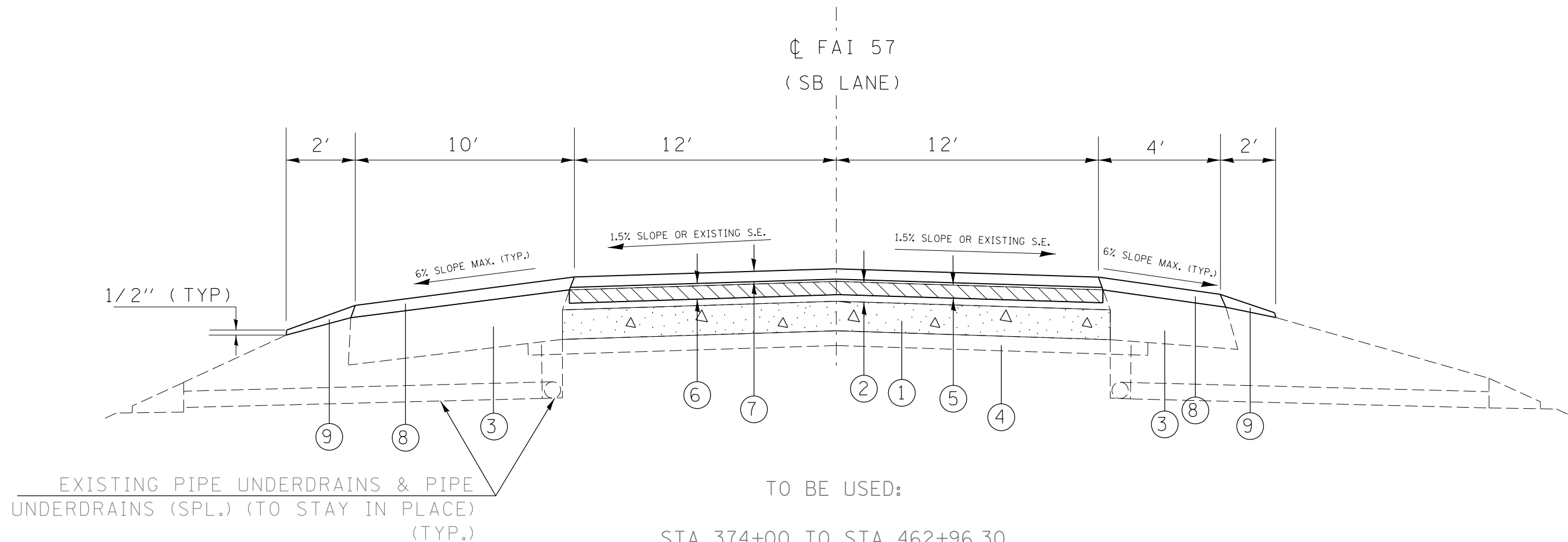
- | | |
|--|---|
| ① EXISTING CRC PAVEMENT 7" | ⑥ POLY. HMA BINDER COURSE, IL-19.0, N90, 2 1/4" |
| ② EXISTING BIT SURFACE AND BINDER (5 3/4") | ⑦ POLY. HMA SURFACE COURSE, MIX. E, N90, 1 1/2" |
| ③ EXISTING HMA SHOULDERS | ⑧ HMA SHOULDERS 1 3/4" |
| ④ EXISTING STABILIZED SUB-BASE (4") | ⑨ AGGREGATE WEDGE SHOULDERS, TY B |
| ⑤ HMA SURFACE REMOVAL, 2" | |

NOTE: MAXIMUM ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT SLOPE AND SHOULDER MAY NOT EXCEED 8%

FILE NAME =	USER NAME = \$USER\$	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTION #1			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\dot\brandonja\d0382650\78416-sh.ta.dgn		DRAWN -	REVISED -		57	(41-1)RS-1	JEFFERSON	53	11			
PLOT SCALE = 100.0000' / in.		CHECKED -	REVISED -		SCALE: SHEET 1 OF 5 SHEETS STA. TO STA.			CONTRACT NO. 78416				
PLOT DATE = 2/2/2015		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

TYPICAL SECTION #2
FAI 57 MAINLINE

(NOT TO SCALE)



TO BE USED:

STA 374+00 TO STA 462+96.30
STA 464+17.70 TO STA 490+93.86
STA 494+66.14 TO STA 551+50

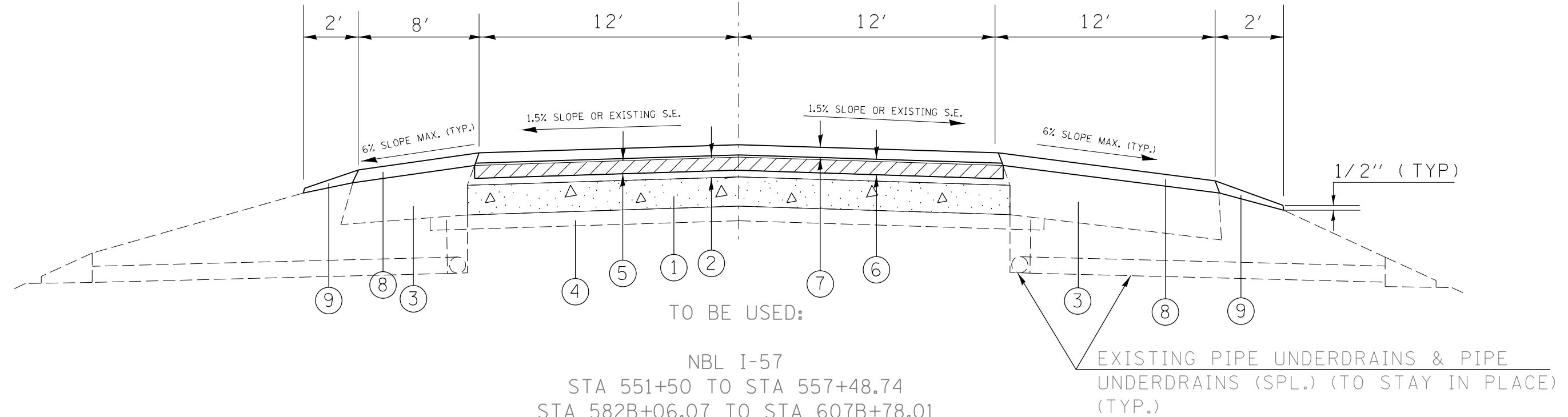
- | | |
|--|---|
| ① EXISTING CRC PAVEMENT 7" | ⑥ POLY. HMA BINDER COURSE, IL-19.0, N90, 2 1/4" |
| ② EXISTING BIT SURFACE AND BINDER (5 3/4") | ⑦ POLY. HMA SURFACE COURSE, MIX. E, N90, 1 1/2" |
| ③ EXISTING HMA SHOULDERS | ⑧ HMA SHOULDERS 1 3/4" |
| ④ EXISTING STABILIZED SUB-BASE (4") | ⑨ AGGREGATE WEDGE SHOULDERS, TY B |
| ⑤ HMA SURFACE REMOVAL, 2" | |

NOTE: MAXIMUM ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT
SLOPE AND SHOULDER MAY NOT EXCEED 8%

FILE NAME =	USER NAME = \$USER\$	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTION #2			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
et:\pwork\pwork\brandonja\0382650\78416-sh.ta.dgn		DRAWN -	REVISED -		SCALE:	SHEET 2	OF 5	SHEETS	STA.	TO STA.	57	(41-1)RS-1	JEFFERSON	53	12
		CHECKED -	REVISED -								CONTRACT NO. 78416				
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT										

TYPICAL SECTION #3
 FAI 57 MAINLINE &
 FAI 64 (RAMP D)
 (NOT TO SCALE)

CL FAI 57 (NBL) &
 FAI 64 (RAMP D)



TO BE USED:

NBL I-57
 STA 551+50 TO STA 557+48.74
 STA 582B+06.07 TO STA 607B+78.01
 STA 610B+15.41 TO STA. 624B+10.01
 STA 624B+10.00 TO STA 634B+10

RAMP D (SB I-57 TO EB I-64)
 STA 636+72.00 TO STA 626+72.00**
 STA 626+72.00 TO STA 623+65.82
 STA 2626+21.89 TO STA 2670+65

EXISTING PIPE UNDERDRAINS & PIPE
 UNDERDRAINS (SPL.) (TO STAY IN PLACE)
 (TYP.)

**VARIABLE LANE AND SHOULDER
 WIDTH WITHIN TRANSITION

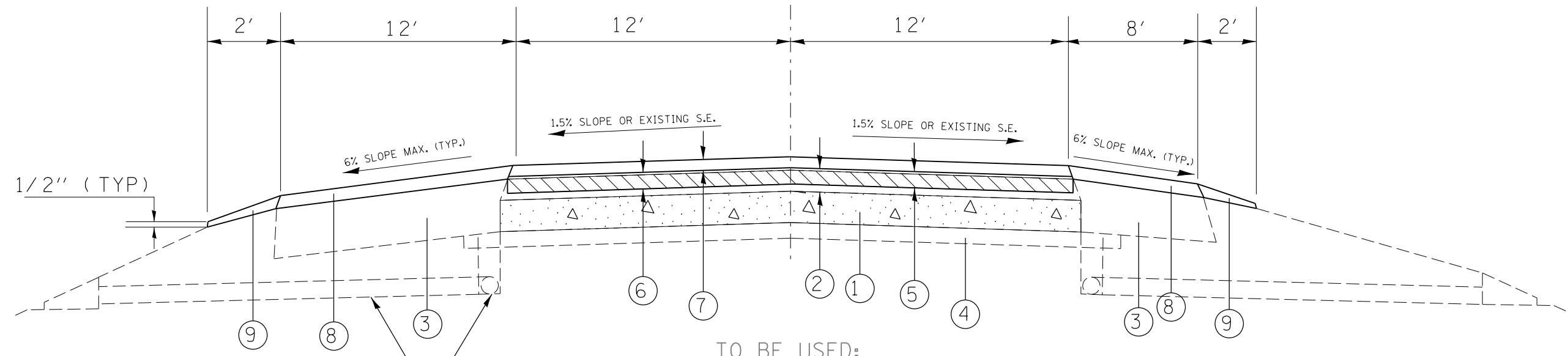
- ① EXISTING CRC PAVEMENT 8"
- ② EXISTING BIT SURFACE AND BINDER (5 3/4")
- ③ EXISTING HMA SHOULDERS
- ④ EXISTING STABILIZED SUB-BASE (4")
- ⑤ HMA SURFACE REMOVAL, 2"
- ⑥ POLY. HMA BINDER COURSE, IL-19.0, N90, 2 1/4"
- ⑦ POLY. HMA SURFACE COURSE, MIX. E, N90, 1 1/2"
- ⑧ HMA SHOULDERS 1 3/4"
- ⑨ AGGREGATE WEDGE SHOULDERS, TY B

NOTE: MAXIMUM ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT SLOPE AND SHOULDER MAY NOT EXCEED 8%

FILE NAME =	USER NAME = \$USER*	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTION #3			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pwork\pwork\brandonja\0382650\78416-sha.dgn		DRAWN -	REVISED -		57	(41-1)RS-1	JEFFERSON	53	13	CONTRACT NO. 78416		
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		SCALE:	SHEET 3	OF 5 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		
	PLOT DATE = 2/2/2015	DATE -	REVISED -									

TYPICAL SECTION #4
 FAI 57 MAINLINE &
 FAI 64 (RAMP C)
 (NOT TO SCALE)

CL FAI 57 (SBL) &
 FAI 64 (RAMP C)



EXISTING PIPE UNDERDRAINS & PIPE UNDERDRAINS (SPL.) (TO STAY IN PLACE) (TYP.)

TO BE USED:

SBL I-57
 STA 551+50 TO STA 574+71.08
 STA 600+00.00 TO STA 636+72

RAMP C (WB I-64 TO NB I-57)
 STA 634+10.00 TO STA 624+10.00**
 STA 2631+96.62 TO STA 2670+65

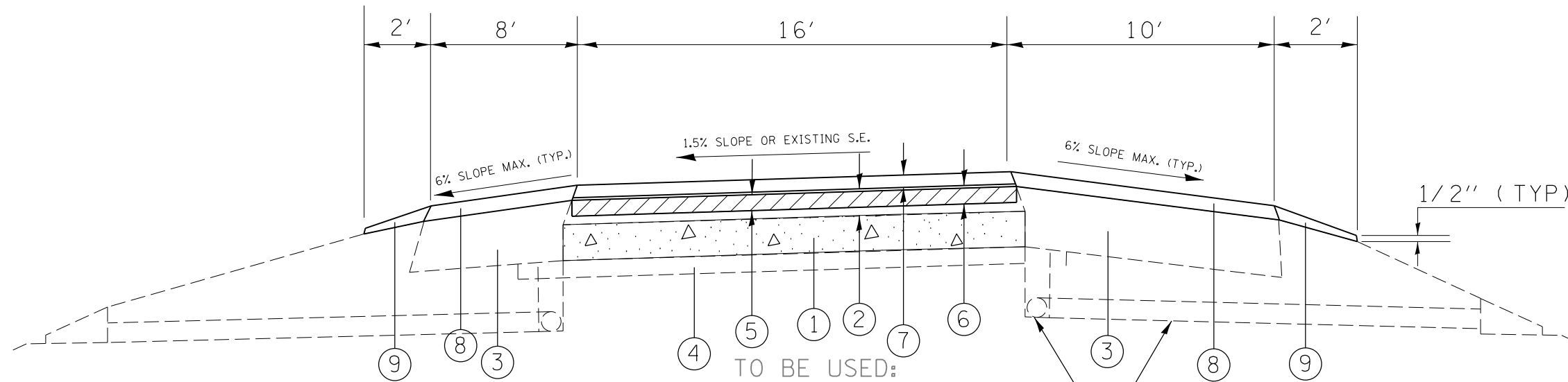
**VARIABLE LANE AND SHOULDER WIDTH WITHIN TRANSITION

- | | |
|--|---|
| ① EXISTING CRC PAVEMENT 8" | ⑥ POLY. HMA BINDER COURSE, IL-19.0, N90, 2 1/4" |
| ② EXISTING BIT SURFACE AND BINDER (5 3/4") | ⑦ POLY. HMA SURFACE COURSE, MIX. E, N90, 1 1/2" |
| ③ EXISTING HMA SHOULDERS | ⑧ HMA SHOULDERS 1 3/4" |
| ④ EXISTING STABILIZED SUB-BASE (4") | ⑨ AGGREGATE WEDGE SHOULDERS, TY B |
| ⑤ HMA SURFACE REMOVAL, 2" | |

NOTE: MAXIMUM ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT SLOPE AND SHOULDER MAY NOT EXCEED 8%

FILE NAME =	USER NAME = \$USER*	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTION #4			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
et:\pwork\pwork\brandonja\d0382650\78416-sh.ta.dgn		DRAWN -	REVISED -		SCALE:	SHEET 4	OF 5	SHEETS	STA.	TO STA.	57	(41-1-1)RS-1	JEFFERSON	53	14
		CHECKED -	REVISED -								CONTRACT NO. 78416				
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT										

TYPICAL SECTION #5
RAMP E & F
(NOT TO SCALE)



RAMP F (NB I-57 TO EB I-64)
STA 590B+08.37 TO STA 595B+38.55**
STA 595B+38.55 TO STA 596B+71.11
STA 11+18.00 TO STA 25+38.78
STA 25+38.78 TO STA 27+30.29**
STA 2654+80.61 TO STA 2664+26.52**

RAMP E (WB I-64 TO SB I-57)
STA 562+64.39 TO STA 574+62.77**
STA 8+59.00 TO STA 18+81.25
STA 21+96.41 TO STA 34+74.42
STA 2656+49.92 TO STA 2668+50.08**

EXISTING PIPE UNDERDRAINS & PIPE UNDERDRAINS (SPL.) (TO STAY IN PLACE) (TYP.)

**VARIABLE LANE AND SHOULDER WIDTH WITHIN TRANSITION

- ① EXISTING CRC PAVEMENT (8'')
- ② EXISTING BIT SURFACE AND BINDER (5 3/4'')
- ③ EXISTING HMA SHOULDERS
- ④ EXISTING STABILIZED SUB-BASE (4'')
- ⑤ HMA SURFACE REMOVAL, 2''
- ⑥ POLY. HMA BINDER COURSE, IL-19.0, N90, 2 1/4''
- ⑦ POLY. HMA SURFACE COURSE, MIX. E, N90, 1 1/2''
- ⑧ HMA SHOULDERS 1 3/4''
- ⑨ AGGREGATE WEDGE SHOULDERS, TY B

NOTE: MAXIMUM ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT SLOPE AND SHOULDER MAY NOT EXCEED 8%

FILE NAME =	USER NAME = \$USER*	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTION #5			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pwork\pwork\brandonja\d0382650\78416-sh.txd		DRAWN -	REVISED -		57	(41-1)RS-1	JEFFERSON	53	15			
PLOT SCALE = 100.0000' / in.		CHECKED -	REVISED -		SCALE: SHEET 5 OF 5 SHEETS STA. TO STA.			CONTRACT NO. 78416				
PLOT DATE = 2/2/2015		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

PATCHING SCHEDULE

SOUTHBOUND I-57											
LEFT OR RIGHT*	STATION	WIDTH	LENGTH	AREA	CLASS A PATCH, TY I, 14 IN	CLASS A PATCH, TY II, 14 IN	CLASS A PATCH, TY III, 14 IN	CLASS A PATCH, TY IV, 14 IN	SAW CUTS	TIE BARS	PATCHING REINFORCEMENT
		FOOT	FOOT	SQ YD	SQ YD	SQ YD	SQ YD	SQ YD	FOOT	EACH	SQ YD
LEFT	532+25	6	51	34.0	.	.	.	34.	126	50	34.
RIGHT	484+65	6	20	13.3	.	13.3	.	.	64	0	13.3
SOUTHBOUND TOTALS					0	13.3	0	34.0	190	50	47.3

NORTHBOUND I-57											
LEFT OR RIGHT*	STATION	WIDTH	LENGTH	AREA	CLASS A PATCH, TY I, 14 IN	CLASS A PATCH, TY II, 14 IN	CLASS A PATCH, TY III, 14 IN	CLASS A PATCH, TY IV, 14 IN	SAW CUTS	TIE BARS	PATCHING REINFORCEMENT
		FOOT	FOOT	SQ YD	SQ YD	SQ YD	SQ YD	SQ YD	FOOT	EACH	SQ YD
RIGHT	536+91	6	20	13.3	.	13.3	.	.	64	0	13.3
RIGHT	537+03	6	20	13.3	.	13.3	.	.	64	0	13.3
RIGHT	542+92	6	25	16.7	.	.	16.7	.	74	24	16.7
RIGHT	543+84	6	7	4.7	4.7	.	.	.	38	0	4.7
RIGHT	484+65	6	18	12.0	.	12.0	.	.	60	0	12.
NORTHBOUND TOTALS					4.7	38.6	16.7	0.0	300	24	60.0
PROJECT TOTALS					4.7	51.9	16.7	34.0	490	74	107.3

* RIGHT OR LEFT IS BASED ON DIRECTION OF TRAVEL

APPROACH SLAB REPAIR SCHEDULE

SN 041-0007 NORTHBOUND I-57					
LEFT OR RIGHT*	LOCATION SN 041-0007	WIDTH	LENGTH	AREA	APPROACH SLAB REPAIR
		FOOT	FOOT	SQ YD	SQ YD
SOUTH APPROACH					
LEFT		6	11	7.3	7.3
RIGHT		6	54	36.0	36.0
NORTH APPROACH					
LEFT		12	5	6.7	6.7
RIGHT		12	11	14.7	14.7
RIGHT		12	5	6.7	6.7
NORTHBOUND TOTALS					71.4
PROJECT TOTALS					71.4

* RIGHT OR LEFT IS BASED ON DIRECTION OF TRAVEL

PAVEMENT RESURFACING SCHEDULE FAI 57 (I-57) - NORTHBOUND

LOCATION STATION TO STATION			LENGTH (INFO ONLY)	EXIST. PAVEMENT WIDTH	TEMP. RAMPS	HMA SURFACE REMOVAL - 2"	POLY HMA SURFACE COURSE 1 1/2"	POLY HMA BINDER COURSE 2 1/4"	POLY. BIT. MAT' LS. PRIME COAT	
FAI 57 (I-57) NORTHBOUND			FEET	FEET	SO YD	SO YD	TON	TON	LBS	
374+00.00	TO	374+70.00	70.0	24.0	57	187	18	12	114	
374+70.00	TO	462+27.68	8757.7	24.0		23354	1972	2943	15764	
462+27.68	TO	462+97.68	70.0	24.0	57	187	18	12	114	
SN 041-0003 - Sta. 462+97.68 to Sta. 464+16.32			118.6							
464+16.32	TO	464+86.32	70.0	24.0	57	187	18	12	114	
464+86.32	TO	490+27.19	2540.9	24.0		6776	572	854	4574	
490+27.19	TO	490+97.19	70.0	24.0	57	187	18	12	114	
SN 041-0005 - Sta. 490+97.19 to Sta. 494+62.81			365.6							
494+62.81	TO	495+32.81	70.0	24.0	57	187	18	12	114	
495+32.81	TO	551+50.00	5617.2	24.0		14979	1265	1887	10111	
551+50.00	TO	557+48.74	598.7	24.0		1597	135	201	1078	
557+48.74	=	582B+06.07								
582B+06.07	TO	607B+08.01	2501.9	24.0		6672	563	841	4503	
607B+08.01	TO	607B+78.01	70.0	24.0	66	187	18	12	114	
SN 041-0007 - Sta. 607B+78.01 to Sta. 610B+15.41			237.4							
610B+15.41	TO	610B+85.41	70.0	24.0	66	187	18	12	114	
610B+85.41	TO	624B+10.01	1324.6	24.0		3532	298	445	2384	
624B+10.01	=	624B+10.00								
624B+10.00	TO	633B+40	930.0	24.0		2480	209	312	1674	
633B+40	TO	634B+10	70.0	24.0	66	187	18	12	114	
NORTHBOUND TOTALS						483	60883	5159	7580	41000

PAVEMENT RESURFACING SCHEDULE FAI 57 (I-57) - SOUTHBOUND

LOCATION STATION TO STATION			LENGTH (INFO ONLY)	EXIST. PAVEMENT WIDTH	TEMP. RAMPS	HMA SURFACE REMOVAL - 2"	POLY HMA SURFACE COURSE 1 1/2"	POLY HMA BINDER COURSE 2 1/4"	POLY. BIT. MAT' LS. PRIME COAT
FAI 57 (I-57) SOUTHBOUND			FEET	FEET	SQ YD	SQ YD	TON	TON	LBS
374+00.00	TO	374+70.00	70.0	24.0	57	187	18	12	114
374+70.00	TO	462+26.30	8756.3	24.0		23350	1961	2942	15761
462+26.30	TO	462+96.30	70.0	24.0	57	187	18	12	114
SN 041-0004 - Sta. 462+96.30 to Sta. 464+17.70			121.4						
464+17.70	TO	464+87.70	70.0	24.0	57	187	18	12	114
464+87.70	TO	490+23.86	2536.2	24.0		6763	568	852	4565
490+23.86	TO	490+93.86	70.0	24.0	57	187	18	12	114
SN 041-0006 - Sta. 490+93.86 to Sta. 494+66.14			292.3						
494+66.14	TO	495+36.14	70.0	24.0	57	187	18	12	114
495+36.14	TO	551+50.00	5613.9	24.0		14970	1258	1886	10105
551+50.00	TO	574+71.08	2321.1	24.0		6190	520	780	4178
574+71.08	=	600+00.00							
600+00.00	TO	636+02.00	3602.0	24.0		9605	807	1210	6484
636+02.00	TO	636+72.00	70.0	24.0	66	187	18	12	114
SOUTHBOUND TOTALS					351	61998	5222	7743	41777

MEDIAN CROSSOVER SCHEDULE

LOCATION STATION	AREA	INCIDENTAL HMA SURFACING	BITUMINOUS MATERIALS (PRIME COAT)
	SQ FT	TON	LBS
546+50	1643	18	82

PAVEMENT RESURFACING SCHEDULE - RAMPS E&F

LOCATION STATION TO STATION			LENGTH (INFO ONLY)	EXIST. PAVEMENT WIDTH	TEMP. RAMPS	HMA SURFACE REMOVAL - 2"	POLY HMA SURFACE COURSE 1 1/2"	POLY HMA BINDER COURSE 2 1/4"	POLY. BIT. MAT' LS. PRIME COAT	
RAMP F (NB I-57 TO EB I-64)			FEET	FEET	SQ YD	SQ YD	TON	TON	LBS	
590+08.37	TO	595+38.55	530.2	VAR.		698	59	88	471	
595+38.55	TO	596+71.11	132.6	16.0		236	20	30	159	
596+71.11	=	11+18.00								
11+18.00	TO	27+30.29	1612.3	16.0		2866	243	361	1935	
27+30.29	=	2654+80.61								
2654+80.61	TO	2664+26.52	945.9	VAR.		980	82	124	662	
RAMP E (WB I-64 TO SB I-57)										
562+64.39	TO	574+62.77	1198.4	VAR.		1198	101	151	809	
574+62.77	=	8+59.00								
8+59.00	TO	10+32.84	173.8	16.0		309	26	39	209	
10+32.84	TO	18+15.55	782.7	16.0		1391	118	175	939	
18+15.55	TO	18+85.55	70.0	16.0	51	124	12	8	76	
SN 041-0059 - Sta. 18+85.55 to Sta. 22+00.71										
22+00.71	TO	22+70.71	70.0	16.0	51	124	12	8	76	
22+70.71	TO	32+89.34	1018.6	16.0		1811	153	228	1222	
32+89.34	TO	34+74.42	185.1	16.0		329	28	41	222	
34+74.42	=	2656+49.92								
2656+49.92	TO	2668+50.08	1200.2	VAR.		1428	120	180	964	
RAMP F & E TOTALS						102	11495	973	1433	7743

PAVEMENT RESURFACING SCHEDULE - RAMPS C&D

LOCATION STATION TO STATION			LENGTH (INFO ONLY)	EXIST. PAVEMENT WIDTH	TEMP. RAMPS	HMA SURFACE REMOVAL - 2"	POLY HMA SURFACE COURSE 1 1/2"	POLY HMA BINDER COURSE 2 1/4"	POLY. BIT. MAT' LS. PRIME COAT	REMARKS	
RAMP C (WB I-64 TO NB I-57)			FEET	FEET	SQ YD	SQ YD	TON	TON	LBS		
634+10.00	TO	624+10.00	1000.0	VAR.		2108	177	266	1423		
624+10.00	=	2631+96.62									
2631+96.62	TO	2634+99.82	303.2	24.0		809	68	102	546		
2634+99.82	TO	2656+50.08	2150.3	24.0		5734	484	722	3870		
2656+50.08	TO	2660+00.00	349.9	24.0		933	79	118	630		
2660+00.00	TO	2669+95.00	995.0	24.0		2653	224	334	1791		
2669+95.00	TO	2670+65.00	70.0	24.0	66	187	18	12	114		
RAMP D (SB I-57 TO EB I-64)											
636+72.00	TO	626+72.00	1000.0	VAR.		2100	176	265	1418		
626+72.00	TO	624+00.00	272.0	24.0		725	61	91	490		
624+00.00	TO	623+65.82	34.2	24.0		91	8	11	62		
623+65.82	=	2626+21.89									
2626+21.89	TO	2640+10.00	1388.1	24.0		3702	313	466	2499		
2640+10.00	TO	2640+80.00	70.0	24.0	30	342	16	24	126	*	
2640+80.00	TO	2642+80.00	200.0	24.0		978	45	67	360	*	
2642+80.00	TO	2643+50.00	70.0	24.0	30	342	16	24	126	*	
2643+50.00	TO	2652+83.50	933.5	24.0		2489	210	314	1680		
2652+83.50	=	2652+90.64									
2652+90.64	TO	2669+95.00	1704.4	24.0		4545	384	573	3068		
2669+95.00	TO	2670+65.00	70.0	24.0	66	187	18	12	114		
RAMP C & D TOTALS						192	27925	2297	3401	18316	
PROJECT TOTALS						1128	162301	13651	20157	108835	

- HMA SURFACE REMOVAL 2" TO 3 3/4"

SHOULDER RESURFACING SCHEDULE

LOCATION STATION TO STATION	LENGTH (INFO ONLY)	HMA SURFACE REMOVAL - BUTT JOINT	SHOULDER						BIT. MAT'LS. PRIME COAT	SHOULDER RUMBLE STRIP		REMARKS	
			HOT-MIX ASPHALT				AGGREGATE			FOOT	FOOT		
			10' OUTSIDE (RT.)	4' INSIDE (LT.)	12' OUTSIDE (RT.)	8' INSIDE (LT.)	2' WEDGE OUTSIDE	2' WEDGE INSIDE					
TON	TON	TON	TON	TON	TON	LBS							
FAI 57 (I-57) NORTHBOUND			FEET	SQ YD									
374+00.00 TO 374+80.00	80.0	125	9	4				1	1	56	80	80	
374+80.00 TO 462+17.68	8737.7		951	381				124	124	6116	8738	8738	
462+17.68 TO 462+97.68	80.0	125	9	4				1	1	56	80	80	
SN 041-0003 - Sta. 462+97.68 to Sta. 464+16.32													
464+16.32 TO 464+96.32	80.0	125	9	4				1	1	56	80	80	
464+96.32 TO 490+17.19	2520.9		274	110				36	36	1765	2521	2521	
490+17.19 TO 490+97.19	80.0	125	9	4				1	1	56	80	80	
SN 041-0005 - Sta. 490+97.19 to Sta. 494+62.81													
494+62.81 TO 495+42.81	80.0	125	9	4				1	1	56	80	80	
495+42.81 TO 551+50.00	5607.2		611	244				80	80	3925	5557	5557	
551+50.00 TO 557+48.74	598.7				78	52		9	9	599	599	599	
557+48.74 = 582B+06.07													
582B+06.07 TO 606B+98.01	2491.9				326	217		35	35	2492	2492	2492	
606B+98.01 TO 607B+78.01	80.0	178			11	7		1	1	80	80	80	
SN 041-0007 - Sta. 607B+78.01 to Sta. 610B+15.41													
610B+15.41 TO 610B+95.41	80.0	178			11	7		1	1	80	80	80	
610B+95.41 TO 624B+10.01	1314.6				172	115		19	19	1315	1315	1315	
624B+10.01 = 624B+10.00													
624B+10.00 TO 633B+30	920.0					80		13	13	920	920		
624B+10.00 TO 631B+70.00	760.0				99							760	
631B+70.00 TO 632B+50.00	80.0	107			11							80	
632B+50.00 TO 633B+30	80.0	72				7		1	1	80	80		
NORTHBOUND TOTALS			1160		1883	753	709	486	326	326	17,651	22,781	22,621

• THE HMA SURFACE REMOVAL - BUTT JOINT QUANTITY INCLUDES SURFACE REMOVAL OF PCC SHOULDERS

SHOULDER RESURFACING SCHEDULE

LOCATION STATION TO STATION	LENGTH (INFO ONLY)	HMA SURFACE REMOVAL - BUTT JOINT	SHOULDERS								BIT. MAT'LS. PRIME COAT	SHOULDER RUMBLE STRIP		REMARKS	
			HOT-MIX ASPHALT				AGGREGATE					FOOT	FOOT		
			10' OUTSIDE (RT.)	4' INSIDE (LT.)	12' OUTSIDE (RT.)	8' INSIDE (LT.)	2' WEDGE OUTSIDE	2' WEDGE INSIDE	TON	TON					
FEET	SQ YD	TON	TON	TON	TON	TON	TON	LBS	FOOT	FOOT					
FAI 57 (I-57) SOUTHBOUND															
374+00.00	TO	374+80.00	80.0	125	9	4				1	1	56	80	80	
374+80.00	TO	462+16.30	8736.3		951	381				124	124	6115	8736	8736	
462+16.30	TO	462+96.30	80.0	125	9	4				1	1	56	80	80	*
SN 041-0004 - Sta. 462+96.30 to Sta. 464+17.70															
464+17.70	TO	464+97.70	80.0	125	9	4				1	1	56	80	80	*
464+97.70	TO	490+13.86	2516.2		274	110				36	36	1761	2516	2516	
490+13.86	TO	490+93.86	80.0	125	9	4				1	1	56	80	80	*
SN 041-0006 - Sta. 490+93.86 to Sta. 494+66.14															
494+66.14	TO	495+46.14	80.0	125	9	4				1	1	56	80	80	*
495+46.14	TO	551+50.00	5603.9		610	244				80	80	3923	5604	5604	
551+50.00	TO	574+71.08	2321.1				303	202		33	33	2321	2321	2321	
574+71.08	=	600+00.00													
600+00.00	TO	635+92.00	3592.0				469			51	51	3592	3592		
600+00.00		633+90.00	3390.0					295						3390	
633+90.00		634+70.00	80.0	72			11						80		
635+92.00	TO	636+72.00	80.0	107				7		1	1	80		80	
SOUTHBOUND TOTALS				804	1882	753	784	505	331	331	18,073	23,249	23,047		

• THE HMA SURFACE REMOVAL - BUTT JOINT QUANTITY INCLUDES SURFACE REMOVAL OF PCC SHOULDERS

SHOULDER RESURFACING SCHEDULE

LOCATION STATION TO STATION	LENGTH (INFO ONLY)	HMA SURFACE REMOVAL - BUTT JOINT	SHOULDER						BIT. MAT' LS. PRIME COAT	SHOULDER RUMBLE STRIP		
			HOT-MIX ASPHALT				AGGREGATE					
			10' OUTSIDE (RT.)	8' INSIDE (LT.)	12' OUTSIDE (RT.)	8' INSIDE (LT.)	2' WEDGE OUTSIDE	2' WEDGE INSIDE				
	FEET	SQ YD	TON	TON	TON	TON	TON	TON	LBS	FOOT	FOOT	
RAMP C (WB I-64 TO NB I-57)												
626+34.55	TO	624+10.00	224.6				8			35		
624+10.00	=	2631+96.62										
2631+96.62	TO	2634+76.27	279.7				37			171		
2634+76.27	TO	2654+79.58	2003.3			262	175	29	29	2003	1923	1923
2654+79.58	TO	2661+43.97	664.4			87	101	9	9	664	664	664
2661+43.97	TO	2669+85.00	841.0			110	73	12	12	841	841	841
2669+85.00	TO	2670+65.00	80.0	178		11	7	1	1	80	80	80
RAMP D (SB I-57 TO EB I-64)												
629+56.52	TO	623+65.82	590.7			68				313		
623+65.82	=	2626+21.89										
2626+21.89	TO	2640+10.00	1388.1			181	121	20	20	1388	1388	1388
2640+10.00	TO	2640+80.00	70.0			15	10	1	1	70	70	70
2640+80.00	TO	2642+80.00	200.0			30	20	3	3	200	200	200
2642+80.00	TO	2643+50.00	70.0			15	10	1	1	70	70	70
2643+50.00	TO	2652+83.50	933.5			122	81	13	13	934	934	934
2652+83.50	=	2652+90.64										
2652+90.64	TO	2654+80.61	190.0			20	17	3	3	190	190	190
2654+80.61	TO	2669+85.00	1504.4			197	131	21	21	1504	1504	1504
2669+85.00	TO	2670+65.00	80.0	178		11	7	1	1	80	80	80
RAMP E (WB I-64 TO SB I-57)												
573+31.33	TO	574+71.08	139.8		20					90		
574+71.08	=	600+00.00										
600+00.00	TO	601+63.08	163.1		4					17		
601+63.08	=	10+31.50										
10+31.50	TO	18+05.55	774.1		84	67		11	11	697	759	759
18+05.55	TO	18+85.55	80.0	125	9	7		1	1	72	80	80
SN 041-0059 - Sta. 18+85.55 to Sta. 22+00.71												
22+00.71	TO	22+80.71	80.0	125	9	7		1	1	72	80	80
22+80.71	TO	33+00.47	1019.8		111	89		15	15	918	927	927
33+00.47	=	2654+79.58										
RAMP F (NB I-57 TO EB I-64)												
590+08.37	TO	592+89.76	281.4									
592+89.76	TO	596+71.11	381.3		45					205		
596+71.11	=	11+18.00										
11+18.00	TO	25+51.60	1433.6		156	125		20	20	1290	1347	1347
25+51.60	TO	27+30.29	178.7		19					89		
27+30.29	=	2654+80.61										
2654+80.61	TO	2664+26.52	945.9									
RAMP TOTALS			606	390	364	1129	799	164	164	11,994	11,138	11,138
PROJECT TOTALS			2570	10436			1642		47,800	113,975		

PAVEMENT MARKING SCHEDULE

LOCATION STATION TO STATION	LENGTH (INFORMATION ONLY)	PREFORMED PLASTIC PAVMT MARKING, TY B - LINE 4"		PREFORMED PLASTIC PAVMT MARKING, TY B - LINE 6"	PREFORMED PLASTIC PAVMT MARKING, TY B - LINE 8"	PREFORMED PLASTIC PAVMT MARKING, TY B - LINE 12"	SHORT-TERM PAVT MRK - LINE 4"			PAVEMENT MARKING REMOVAL		
		SOLID YELLOW	SOLID WHITE	WHITE (SKIP-DASH)	SOLID WHITE	SOLID WHITE	SOLID YELLOW	SOLID WHITE	MAINLINE SKIP-DASH			
		LT. EDGE OF PAVT.	RT. EDGE OF PAVT.				LT. SHLD. DIAG. STRIPE	RT. SHLD. DIAG. STRIPE				
	FEET	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	SO FT	
FAI 57 (I-57) NORTHBOUND												
374+00.00	TO	462+97.68	8898	8898	8898	2224	0	0	712	712	1780	1068
SN 041-0003-Sta. 462+97.68 to Sta. 464+16.32												
464+16.32	TO	490+97.19	2681	2681	2681	670	0	0	214	214	536	322
SN 041-0005-Sta. 490+97.19 to Sta. 494+62.81												
494+62.81	TO	557+48.74	6286	6286	6286	1571	0	0	503	503	1257	754
557+48.74	=	582B+06.07										
582B+06.07	TO	593B+04.60	1099	1099	1099	275	0	0	88	88	220	132
593B+04.60	TO	597B+29.57	425	425	425	106	425	0	34	68	85	62
597B+29.57	TO	607B+78.01	1048	1048	1048	262	0	0	84	84	210	126
SN 041-0007-Sta. 607B+78.01 to Sta. 610B+15.41												
610B+15.41	TO	620B+79.20	1064	1064	1064	266	0	0	85	85	213	128
620B+79.20	TO	624B+10.01	331	331	0	83	331	0	26	53	66	49
624B+10.01	=	624B+10.00										
624B+10.00	TO	626B+20.53	211	211	211	53	211	0	17	34	42	31
626B+20.53	TO	634B+10	790	790	790	198	0	0	63	63	158	95
FAI 57 (I-57) SOUTHBOUND												
374+00.00	TO	462+96.30	8896	8896	8896	2224	0	0	712	712	1779	1068
SN 041-0004-Sta. 462+96.30 to Sta. 464+17.70												
464+17.70	TO	490+93.86	2676	2676	2676	669	0	0	214	214	535	321
SN 041-0006-Sta. 490+93.86 to Sta. 494+66.14												
494+66.14	TO	574+71.08	8005	8005	8005	2001	0	0	640	640	1601	961
572+73.88	TO	574+71.08	197	0	0	50	0	0	0	0	39	13
574+71.08	=	600+00.00										
600+00.00	TO	600+51.43	51	0	0	20	0	0	0	0	10	3
600+00.00	TO	600+51.43	51	51	51	13	0	0	4	4	10	6
600+51.43	TO	601+63.06	112	0	112	28	0	0	0	9	22	10
601+63.06	TO	625+27.27	2364	2364	2364	591	0	0	189	189	473	284
625+27.27	TO	629+28.05	401	0	401	100	401	0	0	64	80	48
629+28.05	TO	636+72.00	744	744	744	372	0	0	60	60	149	89
TOTALS			46571	45809	45991	11837	1368	0	3665	3815	9314	5598

PAVEMENT MARKING SCHEDULE

LOCATION STATION TO STATION	LENGTH (INFORMATION ONLY)	PREFORMED PLASTIC PAVMT MARKING, TY B - LINE 4"		PREFORMED PLASTIC PAVMT MARKING, TY B - LINE 6"	PREFORMED PLASTIC PAVMT MARKING, TY B - LINE 8"	PREFORMED PLASTIC PAVMT MARKING, TY B - LINE 12"	SHORT-TERM PAVT MRK - LINE 4"			PAVEMENT MARKING REMOVAL SQ FT	
		SOLID YELLOW	SOLID WHITE	WHITE (SKIP-DASH)	SOLID WHITE	SOLID WHITE	SOLID YELLOW	SOLID WHITE	MAINLINE SKIP-DASH		
		LT. EDGE OF PAVT.	RT. EDGE OF PAVT.	WHITE (SKIP-DASH)	SOLID WHITE	SOLID WHITE	LT. SHLD. DIAG. STRIPE	RT. SHLD. DIAG. STRIPE			
		FEET	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT		FOOT
RAMP C (WB I-64 TO NB I-57)											
634B+10 TO 626B+20.54	789	0	0	197	0	0	0	0	158	53	
626B+20.54 TO 624B+10.00	211	0	211	53	211	0	0	34	42	25	
624B+10.00 = 2631+96.62											
2631+96.62 TO 2635+25.71	329	0	329	82	329	0	0	53	66	39	
2635+25.71 TO 2654+17.15	1891	1891	1891	473	0	0	151	151	378	227	
2654+17.15 TO 2659+92.15	575	0	575	144	575	0	0	92	115	69	
2659+92.15 TO 2670+65.00	1073	1073	1073	268	0	0	86	86	215	129	
RAMP D (SB I-57 TO EB I-64)											
629+28.05 TO 625+27.50	401	401	0	100	401	246	32	32	80	48	
625+27.50 TO 623+65.82	162	162	162	40	0	0	13	13	32	19	
623+65.82 = 2626+21.89											
2626+21.89 TO 2652+98.54	2677	2677	2677	669	0	0	214	214	535	321	
2652+98.54 TO 2654+12.56	114	114	0	29	0	0	9	0	23	11	
2654+12.56 TO 2670+65.00	1652	1652	1652	472	0	0	132	132	330	198	
RAMP E (WB I-64 TO SB I-57)											
574+62.77 = 8+59.00											
9+18.45 TO 10+77.16	159	159	0	0	159	0	13	13	0	8	
10+77.16 TO 18+81.25	804	804	804	0	0	0	64	64	0	43	
SN 041-0059-Sta. 18+81.25 to Sta. 21+96.41											
21+96.41 TO 32+38.86	1042	1042	1042	0	0	0	83	83	0	56	
32+38.86 TO 34+74.42	236	236	0	0	236	0	19	19	0	13	
34+74.42 = 2656+49.92											
2656+49.92 TO 2659+92.22	342	342	0	0	342	0	27	27	0	18	
RAMP F (NB I-57 TO EB I-64)											
593B+04.60 TO 596B+71.11	367	0	0	0	367	0	0	29	0	10	
596B+71.11 = 11+18.00											
11+18.00 TO 11+73.16	55	0	0	0	55	0	0	4	0	1	
11+73.16 TO 25+00.00	1327	1327	1327	0	0	0	106	106	0	71	
25+00.00 TO 26+61.80	162	0	162	0	162	0	0	26	0	9	
27+30.29 = 2654+80.61											
TOTALS	14367	11880	11905	2527	2836	246	950	1179	1975	1368	
PROJECT TOTALS		115585		14364	4204	246		20898		6966	

DELINEATOR AND RRPM SCHEDULE

LOCATION STATION TO STATION	LENGTH (INFORMATION ONLY)	DELINEATORS (OUTSIDE SHOULDERS ONLY)		RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	RAISED REFLECTIVE PAVEMENT MARKERS	RAISED REFLECTIVE PAVEMENT MARKERS
		SINGLE	DOUBLE		WHITE	YELLOW
		FEET	EACH	EACH	EACH	EACH
FAI 57 (I-57) NORTHBOUND						
374+00.00 TO 462+97.68	8898	23		112	112	
SN 041-0003-Sta. 462+97.68 to Sta. 464+16.32						
464+16.32 TO 491+37.19	2721	8		35	35	
SN 041-0005-Sta. 490+97.19 to Sta. 494+62.81						
494+62.81 TO 557+48.74	6286	17		80	80	
557+48.74 = 582B+06.07						
582B+06.07 TO 607B+78.01	2572	11	2	33	33	
SN 041-0007-Sta. 607B+78.01 to Sta. 610B+15.41						
610B+15.41 TO 624B+10.01	1395	4		18	18	
624B+10.01 = 624B+10.00						
624B+10.00 TO 626B+20.53	211	2		4	4	
626B+20.53 TO 634B+10	789	3		11	11	
FAI 57 (I-57) SOUTHBOUND						
374+00.00 TO 462+96.30	8896	23		112	112	
SN 041-0004-Sta. 462+96.30 to Sta. 464+17.70						
464+17.70 TO 490+93.86	2676	8		34	34	
SN 041-0006-Sta. 490+93.86 to Sta. 494+66.14						
494+66.14 TO 574+71.08	8004.94	21	3	101	101	
574+71.08 = 600+00.00						
600+00.00 TO 636+72.00	3672.00	10		47	47	
TOTALS		130	5	587	587	

DELINEATOR AND RRPM SCHEDULE

LOCATION STATION TO STATION	LENGTH (INFORMATION ONLY)	DELINEATORS (OUTSIDE SHOULDERS ONLY)		RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	RAISED REFLECTIVE PAVEMENT MARKERS	RAISED REFLECTIVE PAVEMENT MARKERS	
		SINGLE	DOUBLE				
		FEET	EACH	EACH	EACH	WHITE EACH	YELLOW EACH
RAMP C (WB I-64 TO NB I-57)							
624+10.00 = 2631+96.62							
2631+96.62 TO 2670+65.00	3868.38	16	4	79	79		
RAMP D (SB I-57 TO EB I-64)							
626+72.00 TO 623+65.82	306.18	2		5	5		
623+65.82 = 2626+21.89							
2626+21.89 TO 2670+65.00	4443.11	21	2	89	89		
RAMP E (WB I-64 TO SB I-57)							
574+62.77 = 8+59.00							
8+59.00 TO 18+81.25	1022.25	8		51	15	36	
SN 041-0059-Sta. 18+81.25 to Sta. 21+96.41							
21+96.41 TO 34+74.42	1278.01	9		54	17	37	
34+74.42 = 2656+49.92							
RAMP F (NB I-57 TO EB I-64)							
595+38.55 TO 596+71.11	132.56	1		6	6		
596+71.11 = 11+18.00							
11+18.00 TO 27+30.29	1612.29	13		56	43	13	
27+30.29 = 2654+80.61							
TOTALS			70	6	340	254	86
PROJECT TOTALS			211		927	927	

GUARDRAIL SCHEDULE

SOUTHBOUND I-57

LEFT OR RIGHT*	LOCATION STATION TO STATION			GUARDRAIL REMOVAL	SPBGR TYPE A	TRAFFIC BARRIER TERMINAL					CONC. CURB TY B 6"	GUARDRAIL MARKERS	** VERTICAL ADJUSTMENT	TERMINAL MARKERS DIRECT APPLIED	COMMENTS
						TY 1, SPL (FLARED)	TY 1, SPL (TAN)	TYPE 2	TYPE 6	TYPE 6B					
						FOOT	FOOT	EACH	EACH	EACH					
	STA.	TO	STA.												
LT EOP	464+13.00	TO	467+09.00	152.0	200.0	1				1			4	1	SN #041-0004, NE CORNER, PARAPET
RT EOP	464+14.00	TO	468+48.00	417.0	337.5		1			1			15	1	SN #041-0004, NW CORNER, PARAPET
LT EOP	494+66.00	TO	497+63.00	146.0	200.0	1				1			4	1	SN #041-0006, NE CORNER, PERMANENT BARRIER
RT EOP	494+40.00	TO	496+99.00	246.0	162.5		1			1			4	1	SN #041-0006, NW CORNER, PARAPET
LT EOP	521+00.00	TO	525+30.00									430			SIGN TRUSS
RT EOP	528+30.00	TO	532+14.00	376.0	287.5		1			1			15	1	SN 041-0054, APPROACH, PIER
LT EOP	549+42.00	TO	552+84.00									342			SIGN TRUSS
LT EOP	629+16.00	TO	632+83.00									367			SIGN TRUSS
RT EOP	629+05.00	TO	632+10.00									305			SIGN TRUSS
TOTAL SB LANES				1,337	1,188	2	3	0	2	3	30	17	1,444	5	

NORTHBOUND I-57

LEFT OR RIGHT*	LOCATION STATION TO STATION			GUARDRAIL REMOVAL	SPBGR TYPE A	TRAFFIC BARRIER TERMINAL					CONC. CURB TY B 6"	GUARDRAIL MARKERS	** VERTICAL ADJUSTMENT	TERMINAL MARKERS DIRECT APPLIED	COMMENTS
						TY 1, SPL (FLARED)	TY 1, SPL (TAN)	TYPE 2	TYPE 6	TYPE 6B					
						FOOT	FOOT	EACH	EACH	EACH					
	STA.	TO	STA.												
LT EOP	459+99.00	TO	462+95.00	152.0	200.0	1				1			4	1	SN #041-0003, SW CORNER, PARAPET
RT EOP	458+88.00	TO	462+97.00	27.0			1						15	1	SN #041-0003, SE CORNER, PARAPET
LT EOP	488+02.00	TO	490+98.00	146.0	200.0	1				1			4	1	SN #041-0005, SW CORNER, PERMANENT BARRIER
RT EOP	488+67.00	TO	491+25.00	246.0	162.5		1			1			4	1	SN #041-0005, SE CORNER, PARAPET
LT EOP	516+96.00	TO	521+00.00									404			SIGN TRUSS
RT EOP	517+62.00	TO	521+04.00									342			SIGN TRUSS
RT EOP	523+63.00	TO	527+46.00	376.0	287.5		1			1			15	1	SN 041-0054, APPROACH, PIER
LT EOP	546+88.00	TO	549+56.00									268			SIGN TRUSS
RT EOP	546+07.00	TO	549+50.00									343			SIGN TRUSS
LT EOP	589+48B	TO	593+15B									367			SIGN TRUSS
RT EOP	589+74B	TO	593+16B									342			SIGN TRUSS
LT EOP	605+03B	TO	607+74B	211.0	175.0		1			1			15	1	SN #041-0007, SW CORNER, WING WALL
RT EOP	605+10B	TO	607+44B	211.0	137.5		1			1			15	1	SN #041-0007, SE CORNER, WING WALL
TOTAL NB LANES				1,369	1,163	2	5	0	3	3	60	25	2,425	7	

EASTBOUND I-64

LEFT OR RIGHT*	LOCATION STATION TO STATION			GUARDRAIL REMOVAL	SPBGR TYPE A	TRAFFIC BARRIER TERMINAL					CONC. CURB TY B 6"	GUARDRAIL MARKERS	** VERTICAL ADJUSTMENT	TERMINAL MARKERS DIRECT APPLIED	COMMENTS
						TY 1, SPL (FLARED)	TY 1, SPL (TAN)	TYPE 2	TYPE 6	TYPE 6B					
						FOOT	FOOT	EACH	EACH	EACH					
	STA.	TO	STA.												
LEFT	256+71.00	TO	2660+14.00		275		1		1				5	1	SIGN TRUSS
TOTAL EB LANES				0	275	0	1	1	0	0	0	5	0	1	

* RIGHT OR LEFT IS BASED ON DIRECTION OF TRAVEL

** VERTICAL ADJUST EXISTING GUARDRAIL TO MATCH HMA OVERLAY

GUARDRAIL SCHEDULE

RAMP D (SOUTHBOUND I-57 TO EASTBOUND I-64)

LEFT OR RIGHT*	LOCATION STATION TO STATION STA. TO STA.			GUARDRAIL REMOVAL FOOT	SPBGR TYPE A FOOT	TRAFFIC BARRIER TERMINAL					CONC. CURB TY B 6" FOOT	GUARDRAIL MARKERS EACH	•• VERTICAL ADJUSTMENT FOOT	TERMINAL MARKERS DIRECT APPLIED EACH	COMMENTS
						TY 1, SPL (FLARED)	TY 1, SPL (TAN)	TYPE 2	TYPE 6	TYPE 6B					
						EACH	EACH	EACH	EACH	EACH					
LEFT	2639+51.08	TO	2640+04.21	53		1						277	1	SN 041-0007 & 041-0059 PIERS	
RIGHT	2637+35.61	TO	2637+88.74	53		1						508	1	SN 041-0007 & 041-0059 PIERS	
TOTAL RAMP D				106	0	0	2	0	0	0	0	2	785	2	

RAMP E (WESTBOUND I-64 TO SOUTHBOUND I-57)

LEFT OR RIGHT*	LOCATION STATION TO STATION STA. TO STA.			GUARDRAIL REMOVAL FOOT	SPBGR TYPE A FOOT	TRAFFIC BARRIER TERMINAL					CONC. CURB TY B 6" FOOT	GUARDRAIL MARKERS EACH	•• VERTICAL ADJUSTMENT FOOT	TERMINAL MARKERS DIRECT APPLIED EACH	COMMENTS
						TY 1, SPL (FLARED)	TY 1, SPL (TAN)	TYPE 2	TYPE 6	TYPE 6B					
						EACH	EACH	EACH	EACH	EACH					
RIGHT	21+81.00	TO	27+02.00	511.0	425	1			1			7	1	SN #041-0059, NE CORNER, APPROACH	
LEFT	22+05.00	TO	25+26.00	311.0	225	1			1			4	1	SN #041-0059, SE CORNER, APPROACH	
RIGHT	14+41.11	TO	18+80.38									448		SN 041-0059, NW CORNER, APPROACH	
RIGHT	2660+10.00	TO	2263+78.00									368		SIGN TRUSS	
LEFT	2660+15.00	TO	2663+82.00									367		SIGN TRUSS	
TOTAL RAMP E & WB I-64				822	650	0	2	0	0	2	0	11	1,183	2	
PROJECT TOTALS				3,634	3,275	4	13	1	5	8	90	60	5,837	17	

* RIGHT OR LEFT IS BASED ON DIRECTION OF TRAVEL

•• VERTICAL ADJUST EXISTING GUARDRAIL TO MATCH HMA OVERLAY

RODENT SHEILD SCHEDULE

LOCATION				RODENT SHEILDS	
	STA	TO	STA	EACH	
FAI 57 (I-57) NORTHBOUND				MEDIAN SHLDR	OUTSIDE SHLDR
STA	374+00.00	TO	STA 462+97.68	16	14
STA	464+16.32	TO	STA 490+97.19	8	0
STA	494+62.81	TO	STA 557+48.74	11	6
STA	582B+06.07	TO	STA 607B+78.01	2	2
STA	610B+15.41	TO	STA 624B+10.01	2	2
STA	624B+10.00	TO	STA 634B+10.00	0	0
FAI (I-57) SOUTHBOUND					
STA	374+00.00	TO	STA 462+96.30	16	14
STA	464+17.70	TO	STA 490+93.86	2	1
STA	494+66.14	TO	STA 574+71.08	16	8
STA	600+00.00	TO	STA 636+72.00	4	6
RAMPS					
C STA	2631+96.62	TO	STA 2642+00.00	1	4
E STA	2626+21.89	TO	STA 2670+00.00	5	4
F STA	8+59.00	TO	STA 34+74.42	1	3
	11+18.00	TO	STA 27+30.29	1	0
PROJECT TOTALS				149	



EXIST. CURVE 57NB-RAMPB3
 PI STA. = 644+11.65
 $\Delta = 11^\circ 45' 00''$ (RT)
 $D = 1^\circ 15' 00''$
 $R = 4,583.66'$
 $T = 471.65'$
 $L = 940.00'$
 $E = 24.20'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 639+40.00$
 $P.T. STA. = 648+80.00$

EXIST. CURVE 64EB-RAMP1
 PI STA. = 11+02.14
 $\Delta = 19^\circ 16' 03''$ (RT)
 $D = 4^\circ 00' 00''$
 $R = 1,432.40'$
 $T = 243.14'$
 $L = 481.69'$
 $E = 20.49'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 8+59.00$
 $P.T. STA. = 13+40.69$

EXIST. CURVE 64EB-RAMP2
 PI STA. = 21+90.81
 $\Delta = 96^\circ 06' 45''$ (RT)
 $D = 7^\circ 30' 00''$
 $R = 763.94'$
 $T = 850.12'$
 $L = 1,281.50'$
 $E = 379.00'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 13+40.69$
 $P.T. STA. = 26+22.19$

EXIST. CURVE 64EB-RAMPD1
 PI STA. = 2628+90.17
 $\Delta = 10^\circ 42' 00''$ (LT)
 $D = 2^\circ 00' 00''$
 $R = 2,864.79'$
 $T = 268.28'$
 $L = 535.00'$
 $E = 12.53'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 2626+21.89$
 $P.T. STA. = 2631+56.89$

EXIST. CURVE 57NB-RAMPB2
 PI STA. = 613+05.86
 $\Delta = 26^\circ 35' 31''$ (LT)
 $D = 1^\circ 30' 00''$
 $R = 3,819.73'$
 $T = 902.66'$
 $L = 1,772.80'$
 $E = 105.21'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 604+03.20$
 $P.T. STA. = 621+76.00$

EXIST. CURVE 64EB-RAMPD2
 PI STA. = 2643+45.77
 $\Delta = 63^\circ 49' 43''$ (LT)
 $D = 3^\circ 00' 05''$
 $R = 1,908.96'$
 $T = 1,188.88'$
 $L = 2,126.61'$
 $E = 339.94'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 2631+56.89$
 $P.T. STA. = 2652+83.50$

EXIST. CURVE 64WB-RAMP C1
 PI STA. = 2645+69.82
 $\Delta = 49^\circ 23' 04''$ (LT)
 $D = 2^\circ 07' 59''$
 $R = 2,686.09'$
 $T = 1,235.02'$
 $L = 2,315.20'$
 $E = 270.32'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 2633+34.80$
 $P.T. STA. = 2656+50.00$

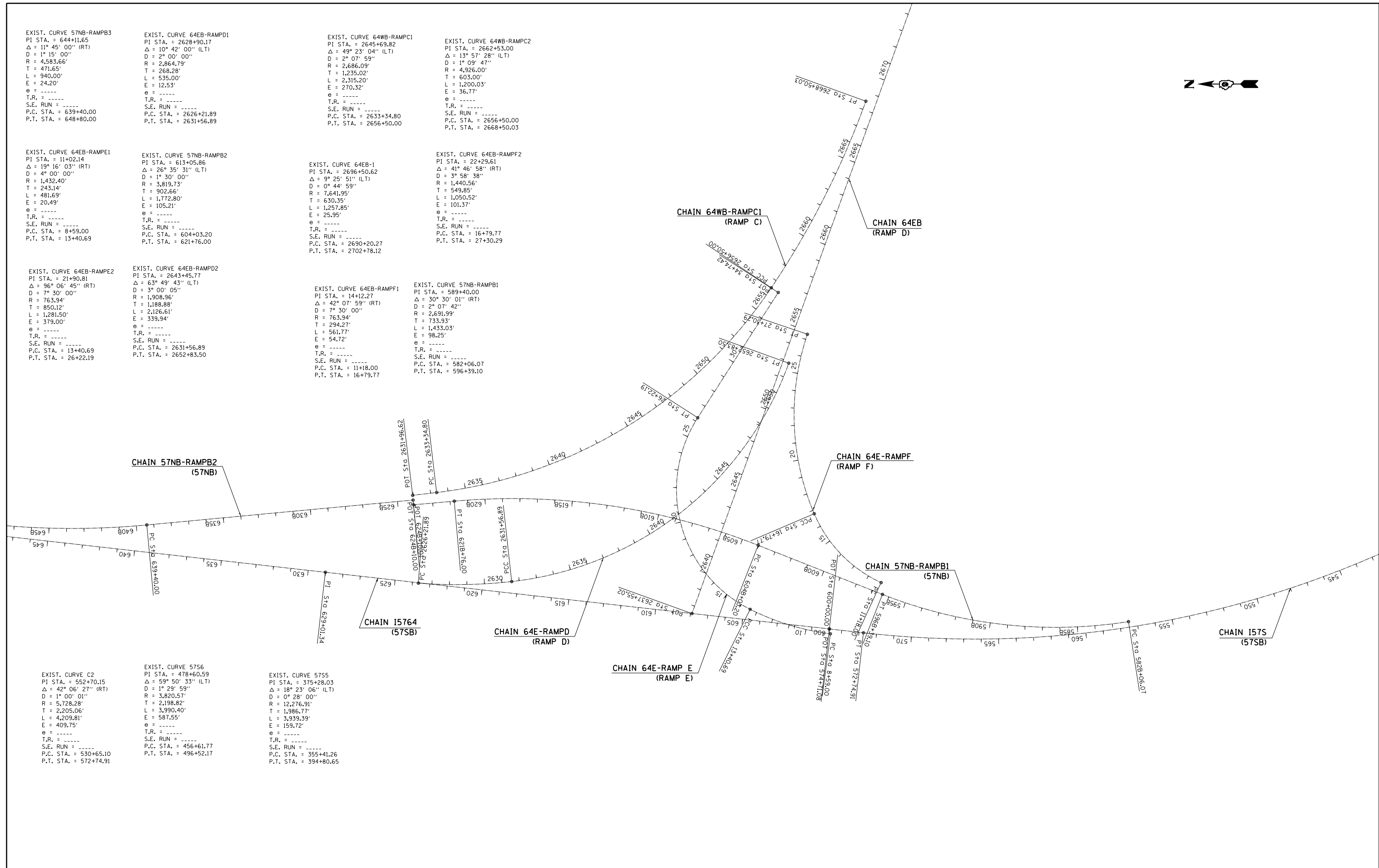
EXIST. CURVE 64EB-1
 PI STA. = 2696+50.62
 $\Delta = 9^\circ 25' 51''$ (LT)
 $D = 0^\circ 44' 59''$
 $R = 7,641.95'$
 $T = 630.35'$
 $L = 1,257.85'$
 $E = 25.95'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 2690+20.27$
 $P.T. STA. = 2702+78.12$

EXIST. CURVE 64EB-RAMPF1
 PI STA. = 14+12.27
 $\Delta = 42^\circ 07' 59''$ (RT)
 $D = 7^\circ 30' 00''$
 $R = 763.94'$
 $T = 294.27'$
 $L = 561.77'$
 $E = 54.72'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 11+18.00$
 $P.T. STA. = 16+79.77$

EXIST. CURVE 64WB-RAMP C2
 PI STA. = 2662+53.00
 $\Delta = 13^\circ 57' 28''$ (LT)
 $D = 1^\circ 09' 47''$
 $R = 4,926.00'$
 $T = 603.00'$
 $L = 1,200.03'$
 $E = 36.77'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 2656+50.00$
 $P.T. STA. = 2668+50.03$

EXIST. CURVE 64EB-RAMPF2
 PI STA. = 22+29.61
 $\Delta = 41^\circ 46' 58''$ (RT)
 $D = 3^\circ 58' 38''$
 $R = 1,440.56'$
 $T = 549.85'$
 $L = 1,050.52'$
 $E = 101.37'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 16+79.77$
 $P.T. STA. = 27+30.29$

EXIST. CURVE 57NB-RAMPB1
 PI STA. = 589+40.00
 $\Delta = 30^\circ 30' 01''$ (RT)
 $D = 2^\circ 07' 42''$
 $R = 2,691.99'$
 $T = 733.93'$
 $L = 1,433.03'$
 $E = 98.25'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 582+06.07$
 $P.T. STA. = 596+39.10$



EXIST. CURVE C2
 PI STA. = 552+70.15
 $\Delta = 42^\circ 06' 27''$ (RT)
 $D = 1^\circ 00' 01''$
 $R = 5,728.28'$
 $T = 2,205.06'$
 $L = 4,209.81'$
 $E = 409.75'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 530+65.10$
 $P.T. STA. = 572+74.91$

EXIST. CURVE 5756
 PI STA. = 478+60.59
 $\Delta = 59^\circ 50' 33''$ (LT)
 $D = 1^\circ 29' 59''$
 $R = 3,820.57'$
 $T = 2,198.82'$
 $L = 3,990.40'$
 $E = 587.55'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 456+61.77$
 $P.T. STA. = 496+52.17$

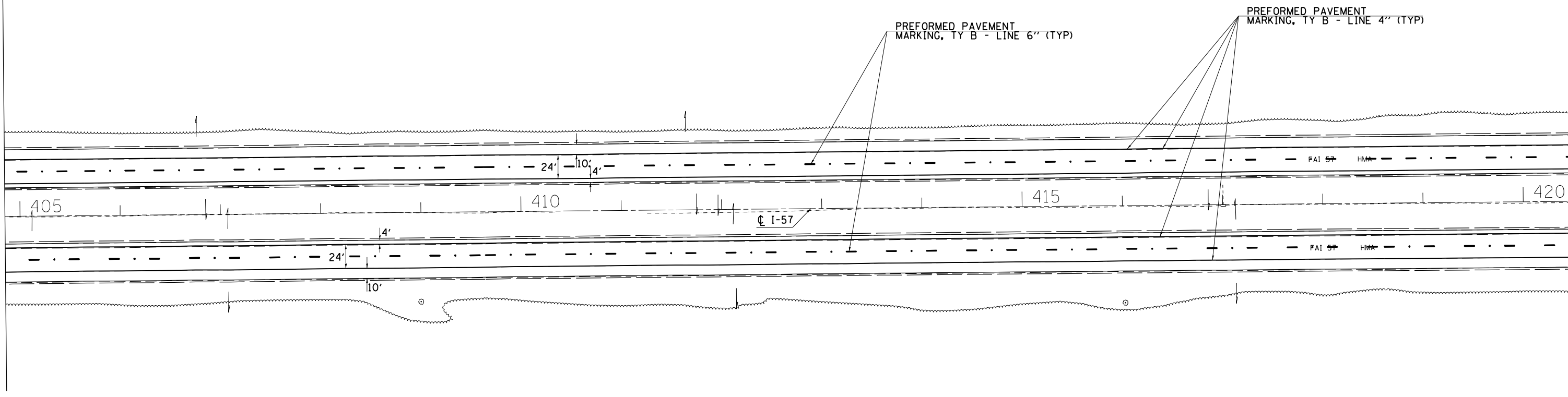
EXIST. CURVE 5755
 PI STA. = 375+28.03
 $\Delta = 18^\circ 23' 06''$ (LT)
 $D = 0^\circ 28' 00''$
 $R = 12,276.91'$
 $T = 1,986.77'$
 $L = 3,939.39'$
 $E = 159.72'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 355+41.26$
 $P.T. STA. = 394+80.65$

FILE NAME =	USER NAME = \$USER\$	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GEOPAK ELEMENTS IDENTIFICATION SHEET				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 100.0000' / 1" =		CHECKED -	REVISED -		CONTRACT NO. 78416								
PLOT DATE = 2/2/2015		DATE -	REVISED -		ILLINOIS FED. AID PROJECT								
SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.							



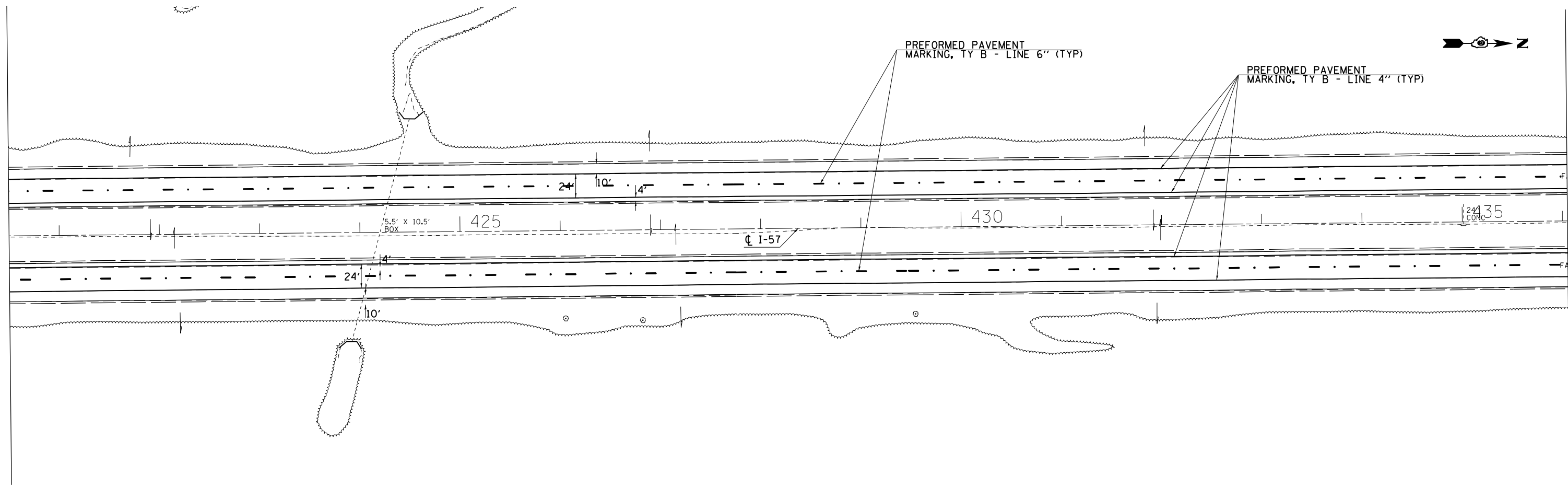
PREFORMED PAVEMENT MARKING, TY B - LINE 6" (TYP)

PREFORMED PAVEMENT MARKING, TY B - LINE 4" (TYP)



PREFORMED PAVEMENT MARKING, TY B - LINE 6" (TYP)

PREFORMED PAVEMENT MARKING, TY B - LINE 4" (TYP)



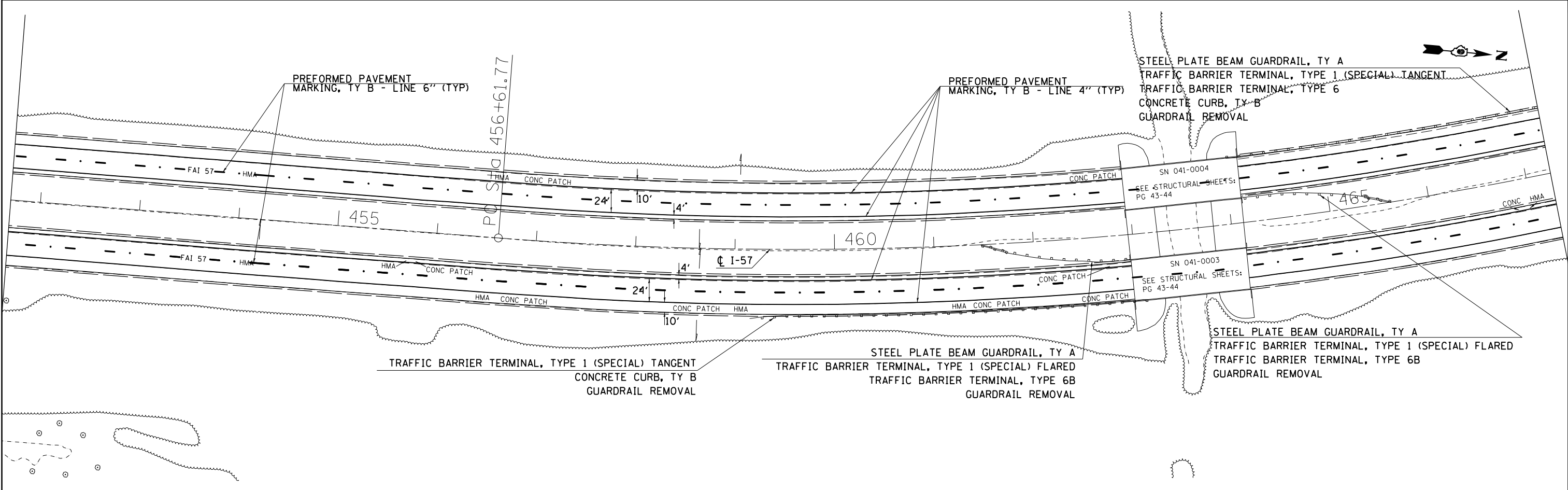
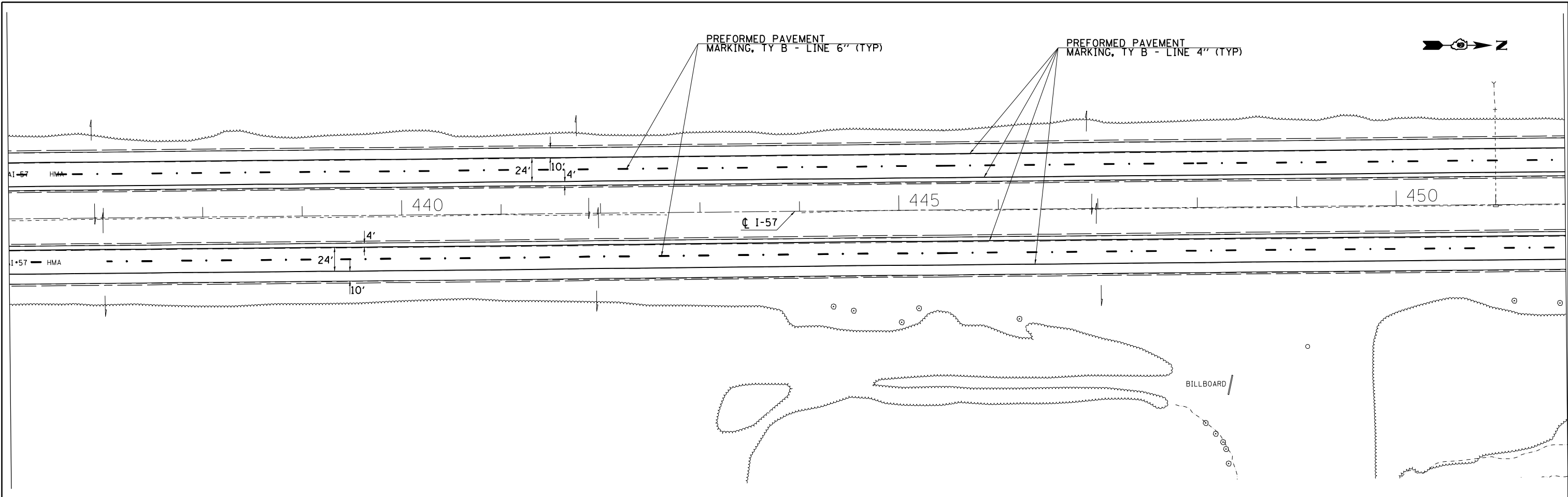
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MODELNAME	PLOT DATE = 2/2/2015	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN ALONG I-57

SCALE: SHEET 2 OF 12 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(41-1)RS-1	JEFFERSON	53	32
CONTRACT NO. 78416				
ILLINOIS FED. AID PROJECT				



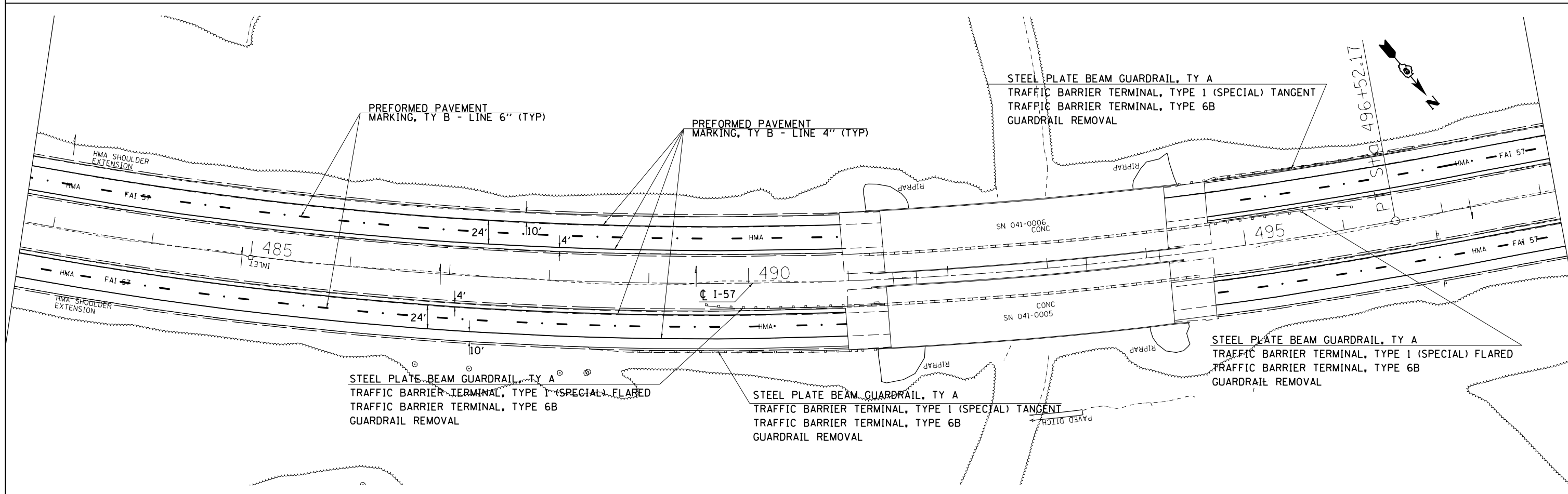
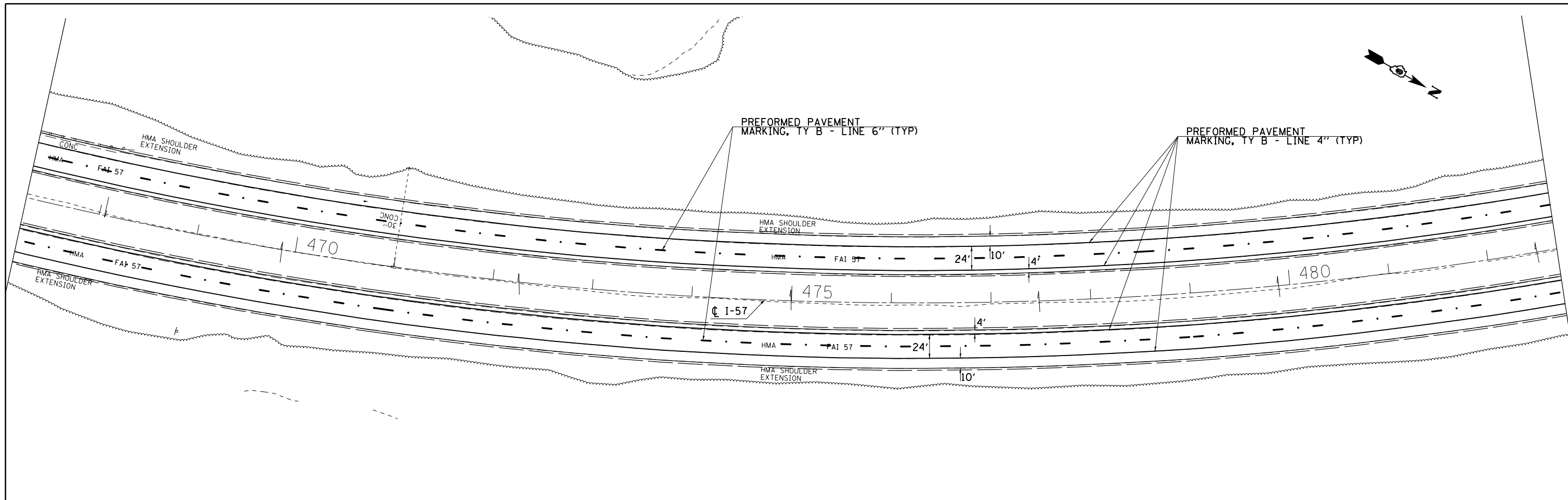
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MODELNAME	PLOT DATE = 2/2/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

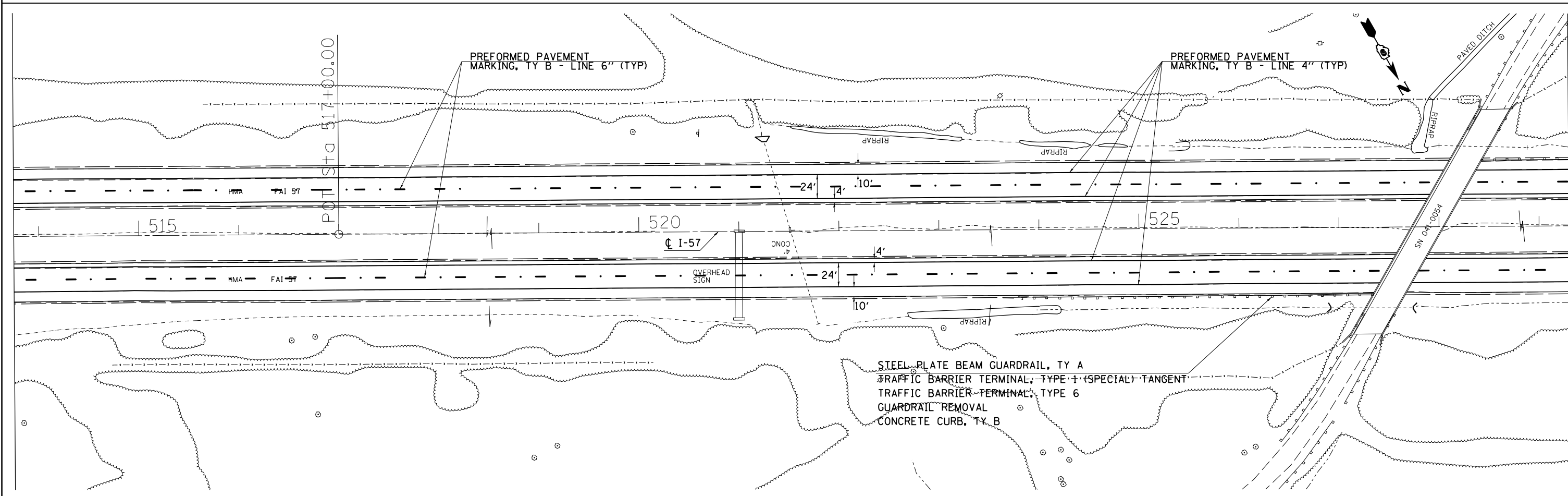
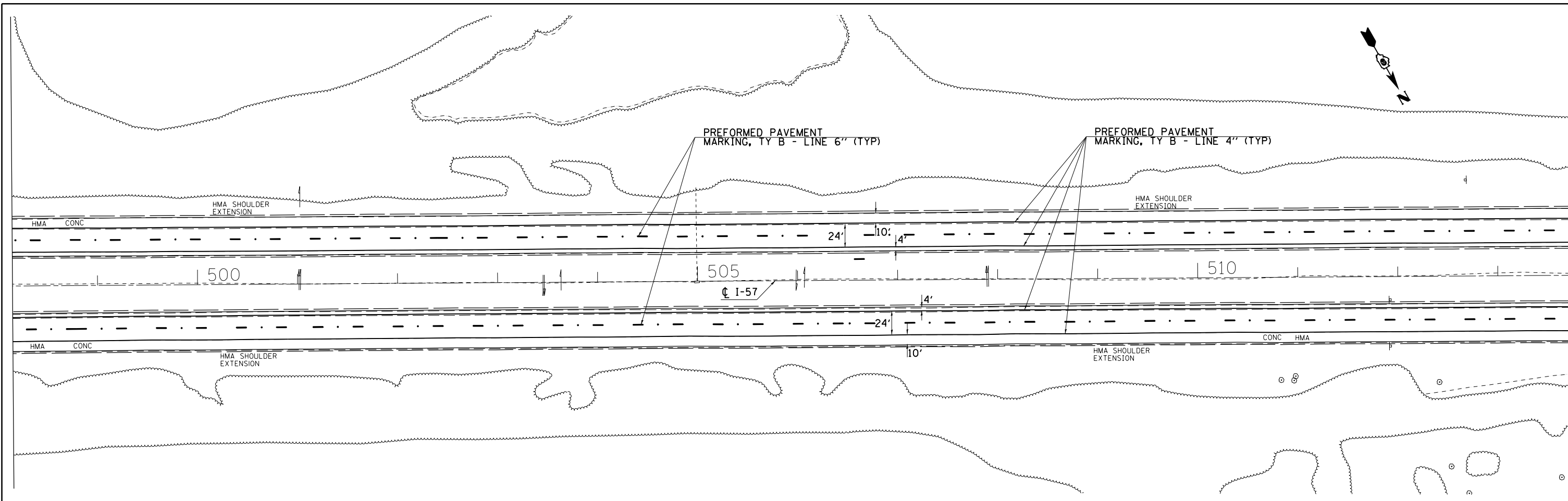
PLAN ALONG I-57

SCALE: SHEET 3 OF 12 SHEETS STA. TO STA.

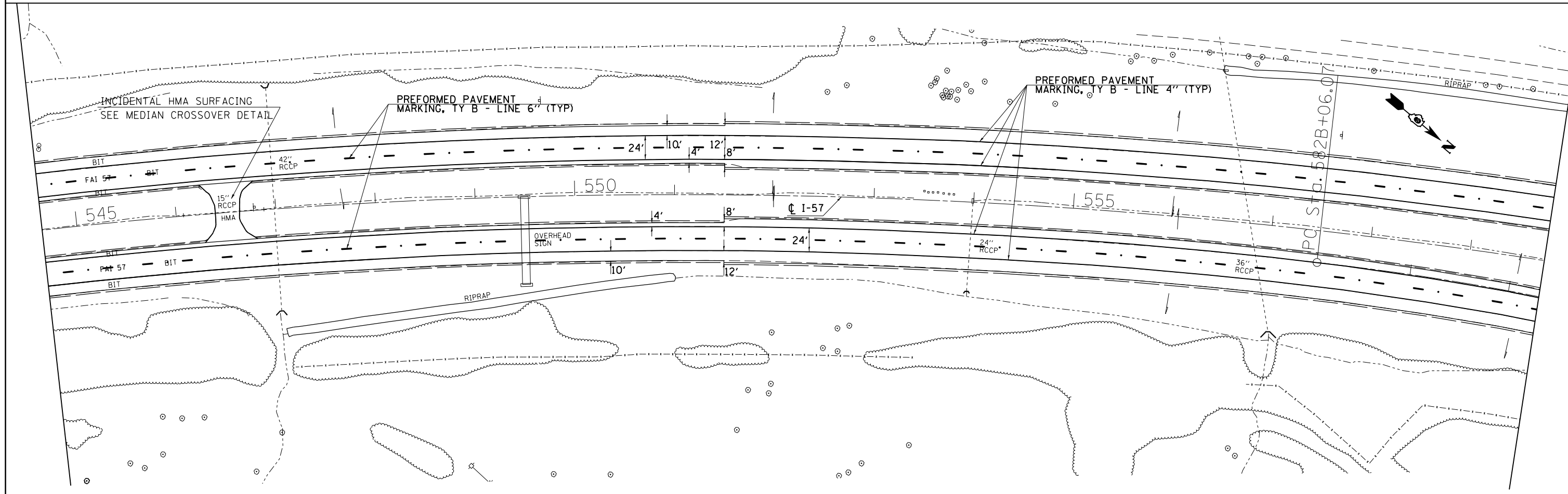
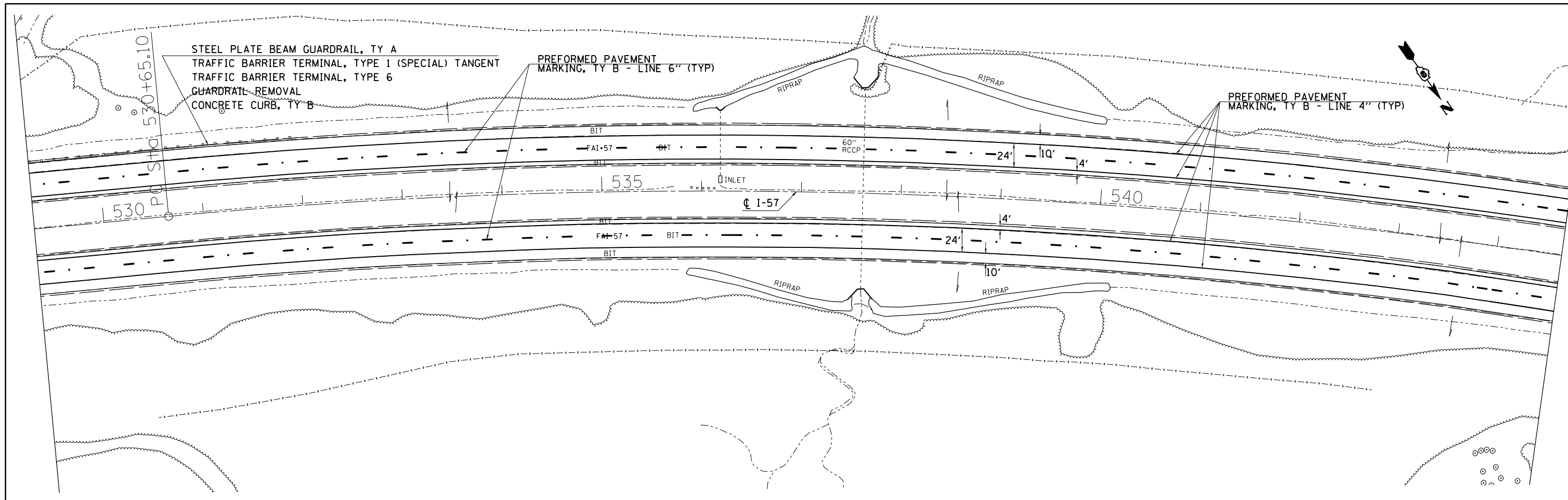
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(41-1)RS-1	JEFFERSON	53	33
CONTRACT NO. 78416				
ILLINOIS FED. AID PROJECT				



FILE NAME =	USER NAME = \$USER\$	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN ALONG I-57			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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MODELNAME	PLOT DATE = 2/2/2015	DATE -	REVISED -		SCALE:	SHEET 4	OF 12 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		



FILE NAME =	USER NAME = USER*	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN ALONG I-57		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
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MODELNAME	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 78416									
	PLOT DATE = 2/2/2015	DATE -	REVISED -		ILLINOIS FED. AID PROJECT									



FILE NAME =	USER NAME = \$USER\$	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN ALONG I-57			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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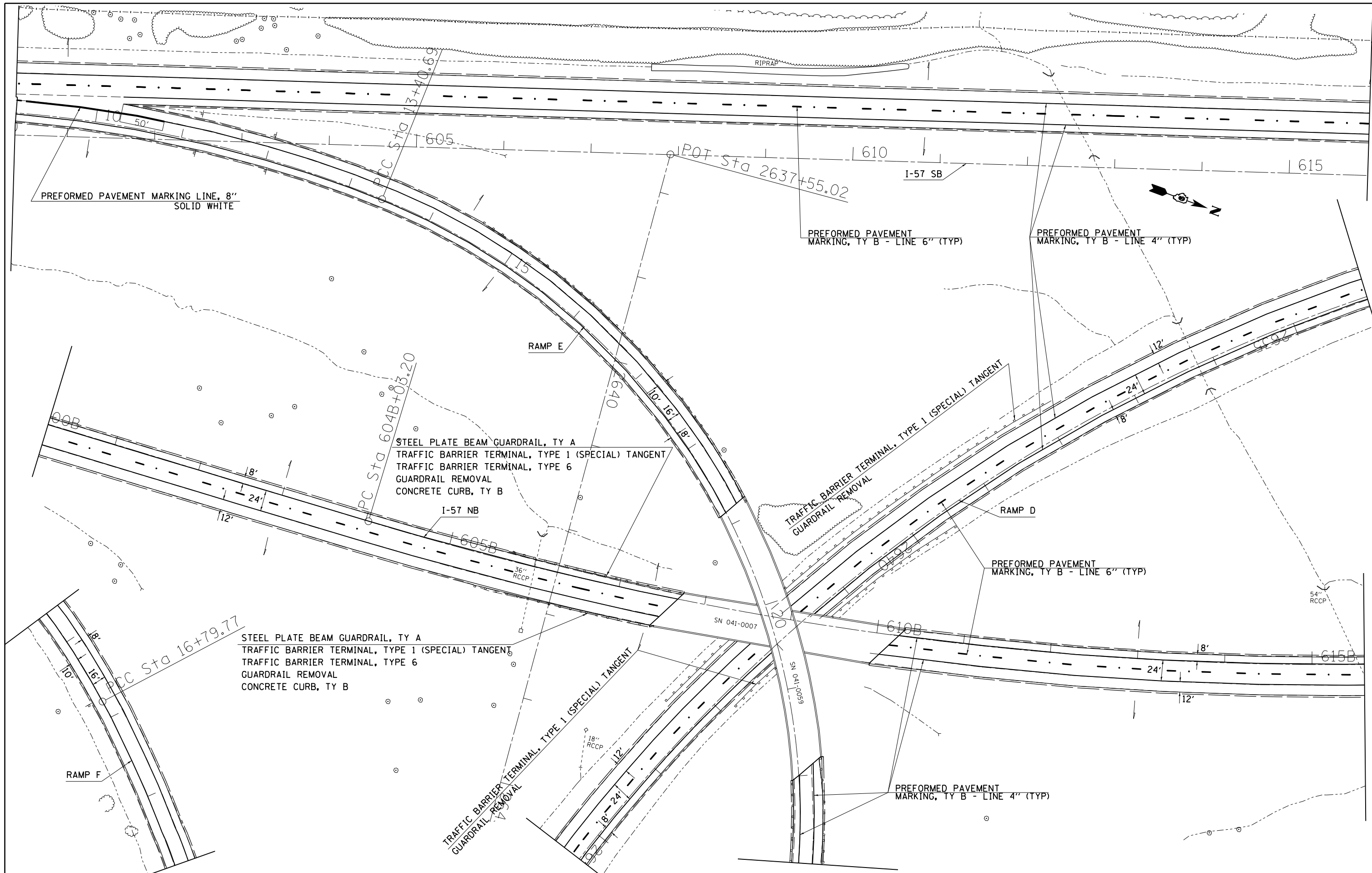
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\$MODELNAME\$	PLOT DATE = 2/2/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN ALONG I-57

SCALE: SHEET 7 OF 12 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(41-1)RS-1	78416	53	37
CONTRACT NO. 78416				
ILLINOIS FED. AID PROJECT				



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USER NAME = \$USER\$
 PLOT SCALE = 100.0000' / 1" / 1" / 1"
 PLOT DATE = 2/2/2015

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PLAN ALONG I-57 & I-64 RAMPS

SCALE: SHEET 8 OF 12 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(41-1)RS-1	JEFFERSON	53	38
CONTRACT NO. 78416				
ILLINOIS FED. AID PROJECT				



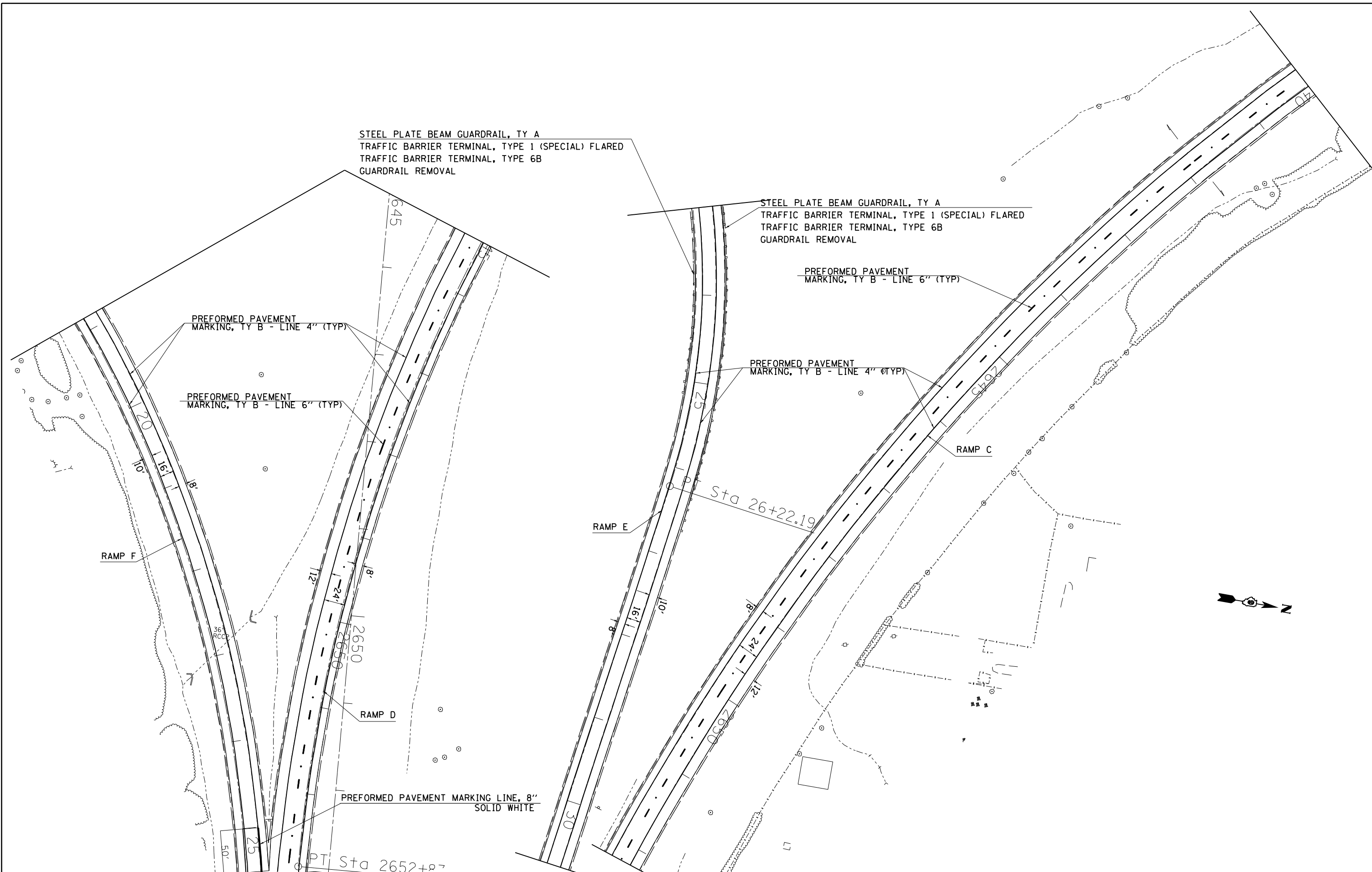
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	PLOT DATE = 2/2/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN ALONG I-57

SCALE: SHEET 9 OF 12 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(41-1)RS-1	JEFFERSON	53	39
CONTRACT NO. 78416				
ILLINOIS FED. AID PROJECT				



STEEL PLATE BEAM GUARDRAIL, TY A
 TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) FLARED
 TRAFFIC BARRIER TERMINAL, TYPE 6B
 GUARDRAIL REMOVAL

STEEL PLATE BEAM GUARDRAIL, TY A
 TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) FLARED
 TRAFFIC BARRIER TERMINAL, TYPE 6B
 GUARDRAIL REMOVAL

PREFORMED PAVEMENT
 MARKING, TY B - LINE 4" (TYP)

PREFORMED PAVEMENT
 MARKING, TY B - LINE 6" (TYP)

PREFORMED PAVEMENT
 MARKING, TY B - LINE 6" (TYP)

PREFORMED PAVEMENT
 MARKING, TY B - LINE 4" (TYP)

RAMP F

RAMP C

RAMP E

RAMP D

PREFORMED PAVEMENT MARKING LINE, 8"
 SOLID WHITE

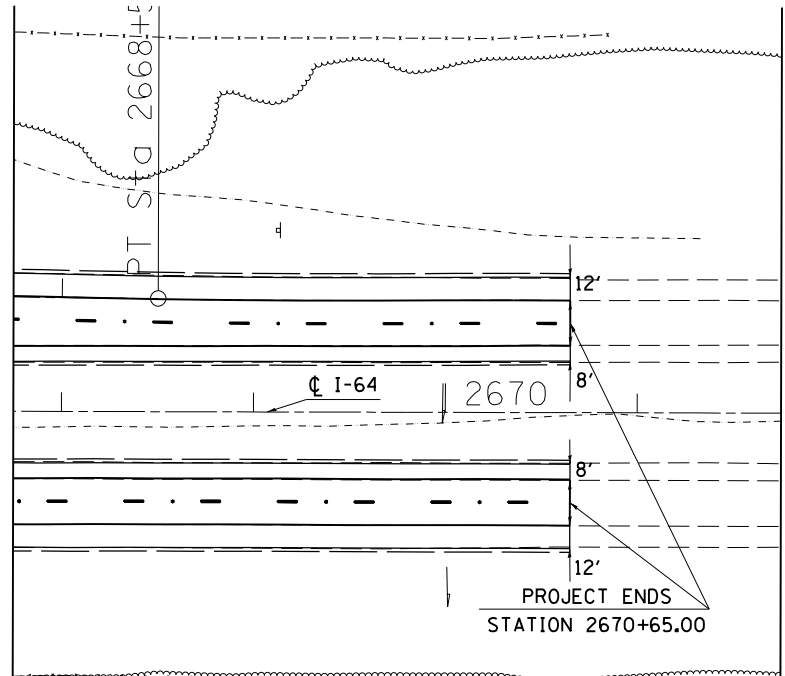
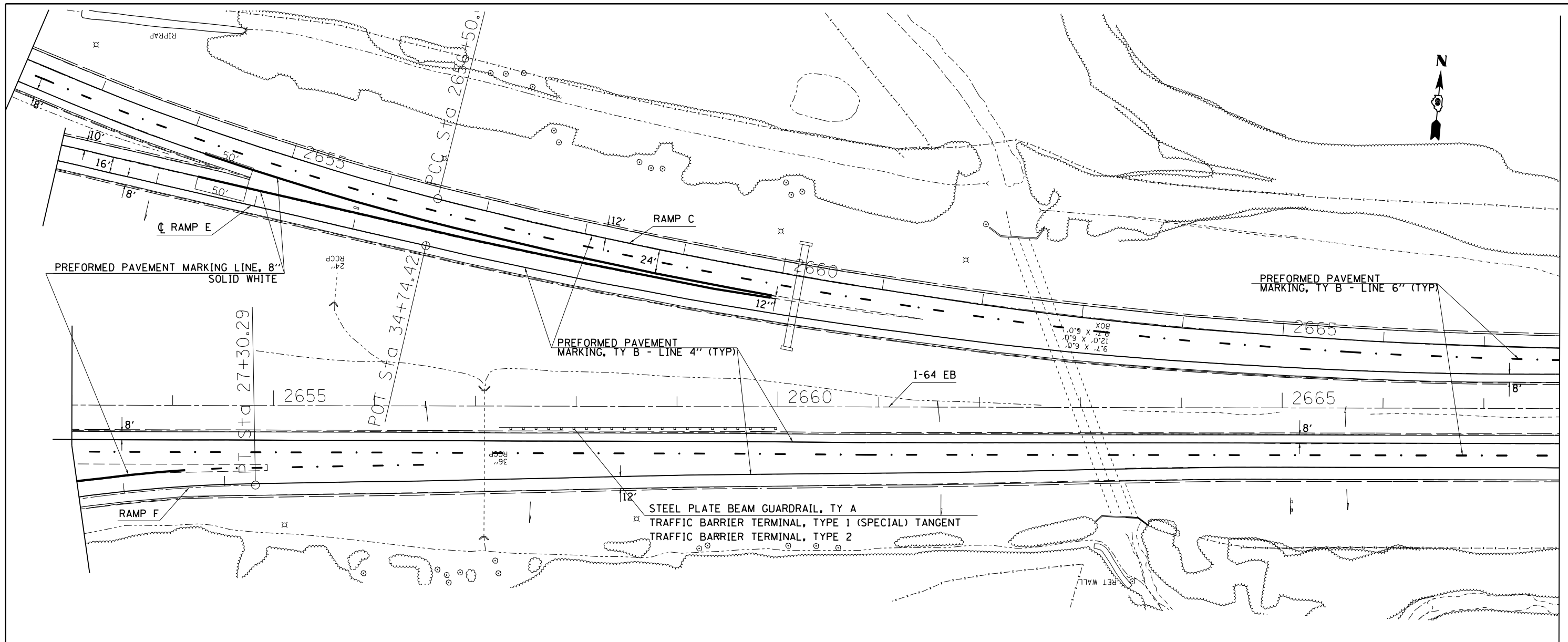
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\$MODELNAME\$	PLOT DATE = 2/2/2015	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PLAN ALONG I-64 / RAMP

SCALE: SHEET 10 OF 12 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(41-1)RS-1	JEFFERSON	53	40
CONTRACT NO. 78416				
ILLINOIS FED. AID PROJECT				



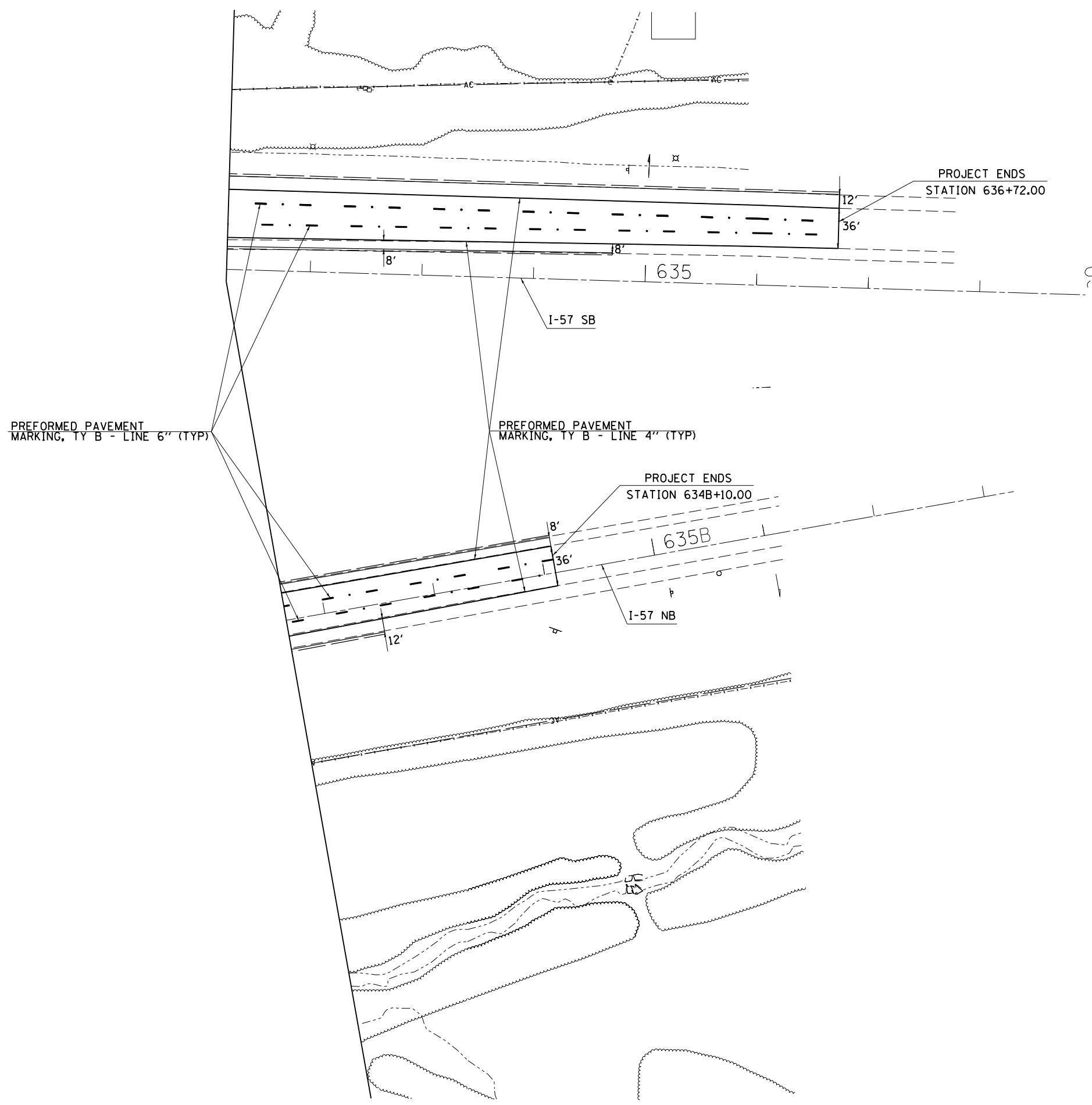
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\$MODELNAME\$	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 2/2/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN ALONG I-64

SCALE: SHEET 11 OF 12 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(41-1)RS-1	JEFFERSON	53	41
CONTRACT NO. 78416				
ILLINOIS FED. AID PROJECT				



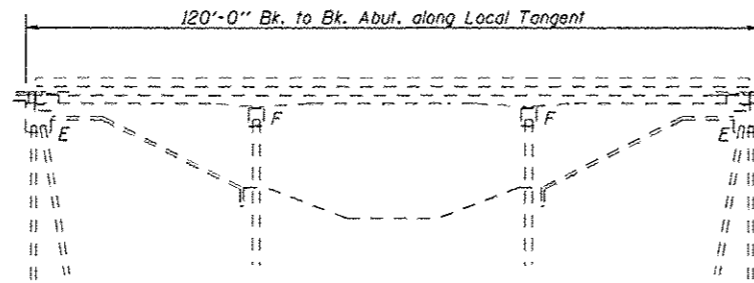
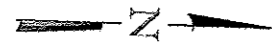
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

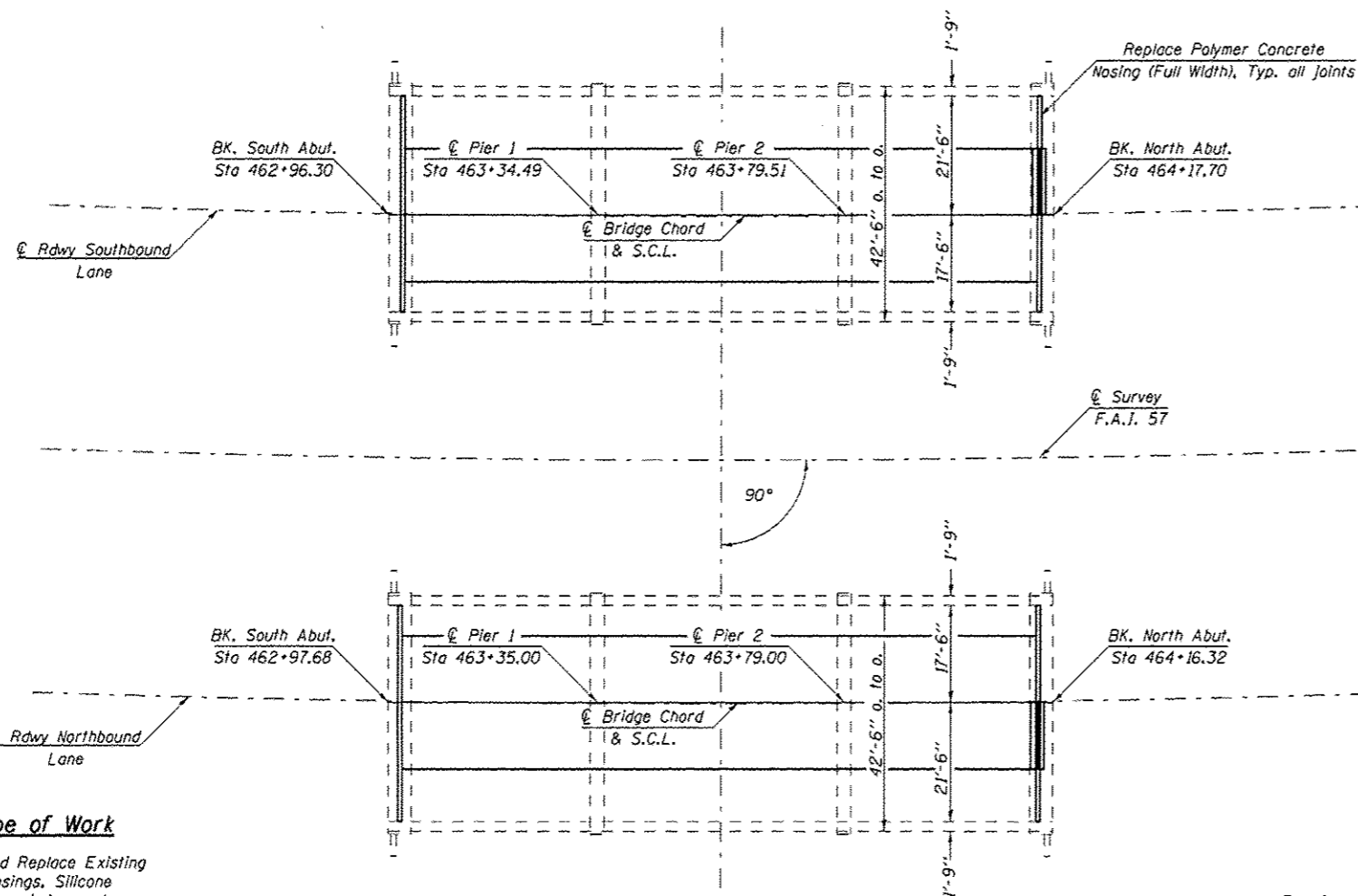
PLAN ALONG I-57

SCALE: SHEET 12 OF 12 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(41-1)RS-1	JEFFERSON	53	42
			CONTRACT NO. 78416	
ILLINOIS FED. AID PROJECT				



ELEVATION



Scope of Work

- Remove and Replace Existing Polymer Nosings, Silicone Joint Seal, and damaged concrete.
- Mill and Resurface (24'-0", driving and passing lanes.)

GENERAL NOTES

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Staging should match that of the adjacent resurfacing.
Reinforcement bars designated (E) shall be epoxy coated.

* Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50°F.

The class of the portland cement concrete, curing, and mix design criteria for Concrete Superstructure shall follow Article 1020.04 of the Standard Specifications for Class PP (Bridge Deck Patching).



David Carl Puzey 3/9/15
Expires 11/30/16

TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Polymerized HMA Surface Course, Mix "E", N90	Ton	43
Concrete Removal	Cu. Yd.	1.3
Concrete Superstructure	Cu. Yd.	0.9
HMA Surface Removal (Deck)	Sq. Yd.	614
Silicone Joint Sealer, 1 3/4"	Foot	164
Polymer Concrete	Cu. Ft.	11.4
Reinforcement Bars, Epoxy Coated	Pound	100

Design Stresses

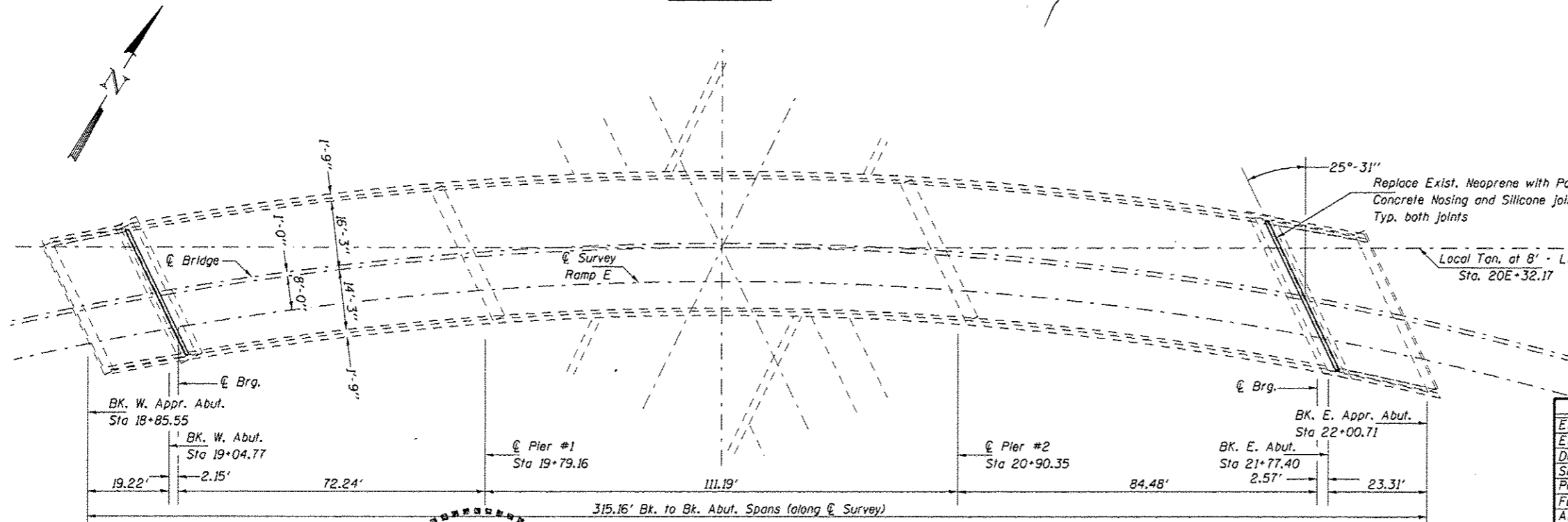
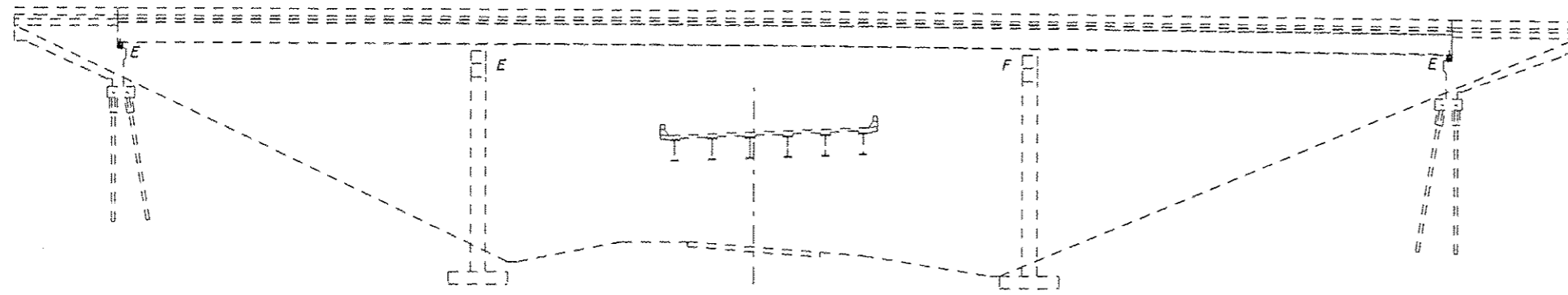
FIELD UNITS (Existing Construction)
 $f_c = 1,400$ psi (super & sub.)
 $f_s = 20,000$ psi Struct.(A-36)
 $f_s = 20,000$ psi (reinforcement)
 $V_c = 75$ psi (Figs.)
 $n = 10$

**BRIDGE REPAIR
 FAI RT 57 OVER
 DODDS CREEK
 JEFFERSON COUNTY
 SECTION (41-1-1)RS-1
 STA. 463+57
 SN 041-0003 (NB) SN 041-0004 (SB)**

FILE NAME -	USER NAME - #USER#	DESIGNED - <i>TWH</i>	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL PLAN & ELEVATION				F.A.I. RTE. -	SECTION -	COUNTY -	TOTAL SHEETS -	SHEET NO. -
0:\pwwork\pwwork\brandonjo\202026501\041-0003-0004-sheet.dgn		DRAWN -	REVISED -		57	(41-1-1)RS-1	JEFFERSON	53	43				
PLOT SCALE = 3/8"=1'-0"		CHECKED - <i>MAS</i>	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.				CONTRACT NO. 78416				
PLOT DATE = 2/2/2015		DATE -	REVISED -		ILLINOIS FED. AID PROJECT								

GENERAL NOTES

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
Staging should match that of the adjacent resurfacing.



25°-31'
Replace Exist. Neoprene with Polymer
Concrete Nosing and Silicone Joint Sealer (Full Width),
Typ. both joints

Local Tan. at 8' - L1.
Sta. 20E+32.17

TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Elastomeric Bearing Assembly, Type I	Each	5
Elastomeric Bearing Assembly, Type II	Each	5
Deck Slab Repair (Full Depth, Type I)	Sq. Yd.	0.3
Silicone Joint Sealer, 2 1/2"	Foot	74
Polymer Concrete	Cu. Ft.	5.3
Furnishing and Erecting Structural Steel	Pound	1560
Anchor Bolts, 1"	Each	20
Jack and Remove Existing Bearings	Each	10

Design Stresses

FIELD UNITS (New Construction)
 $f_y = 36,000$ psi (AASHTO M270 Gr. 36)
FIELD UNITS (Existing Construction)
 $f_c = 1,400$ psi (super & sub.)
 $f_s = 20,000$ psi (structural steel)
 $f_s = 20,000$ psi (reinforcement)
 $V_c = 75$ psi (Figs.)
 $n = 10$

BRIDGE REPAIR
 FAI RT 64 WB OVER
 FAI RT 57 NBL
 JEFFERSON COUNTY
 SECTION (41-1-1)RS-1
 STA. 20E+32.17 (RAMP E)
 SN 041-0059

Scope of Work

- Remove existing Neoprene Joints and replace with Polymer Nosings and Silicone.
- Removal and Replacement of the abutment Bearings.
- Deck Slab Repair (Full Depth).

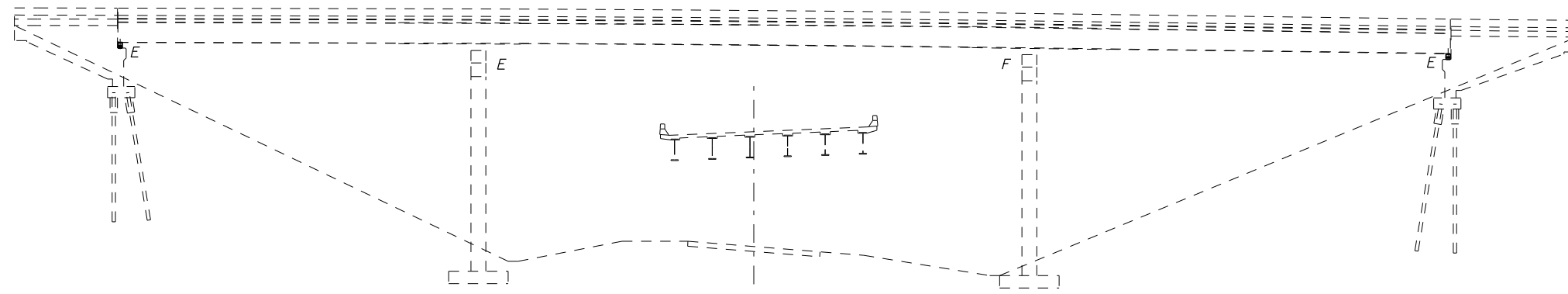


David Carl Puzey 3/9/15
 Expires 11/30/16

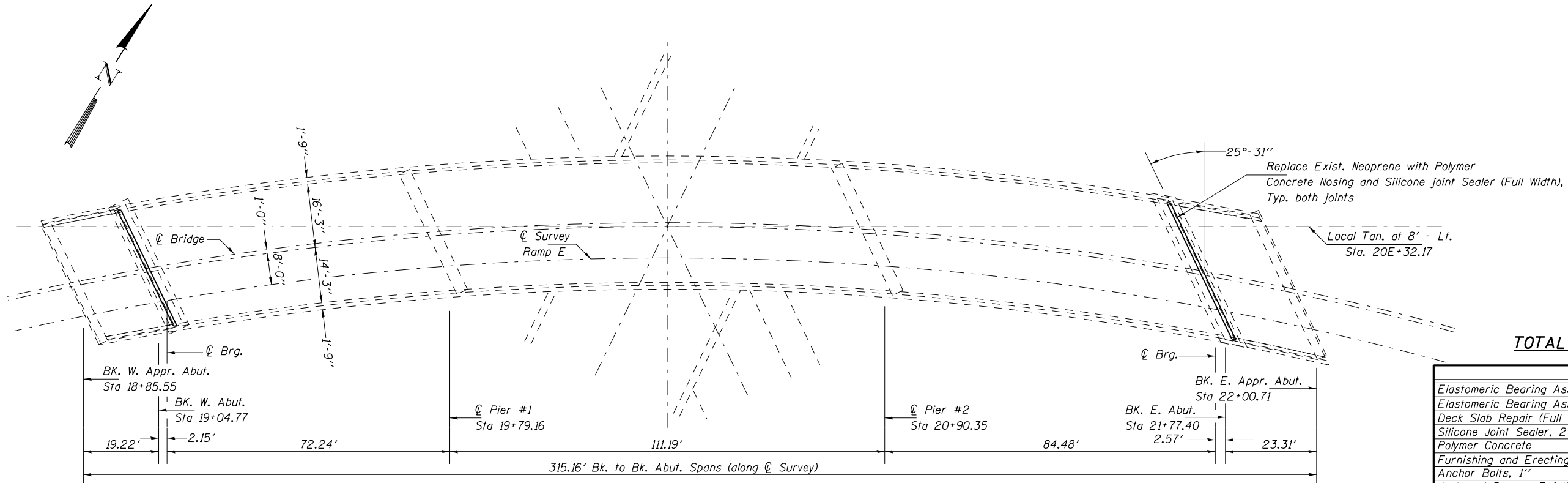
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PLDT SCALE: 30.0000' / in.	CHECKED: MHS	REVISIONS:	DATE:		SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	CONTRACT NO. 78416				
PLDT DATE: 2/2/2015	DATE:	REVISIONS:	DATE:		ILLINOIS FED. AID PROJECT							

GENERAL NOTES

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
Staging should match that of the adjacent resurfacing.



ELEVATION



PLAN

TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Elastomeric Bearing Assembly, Type I	Each	5
Elastomeric Bearing Assembly, Type II	Each	5
Deck Slab Repair (Full Depth, Type I)	Sq. Yd.	0.3
Silicone Joint Sealer, 2 1/2"	Foot	74
Polymer Concrete	Cu. Ft.	5.3
Furnishing and Erecting Structural Steel	Pound	1560
Anchor Bolts, 1"	Each	20
Jack and Remove Existing Bearings	Each	10

Scope of Work

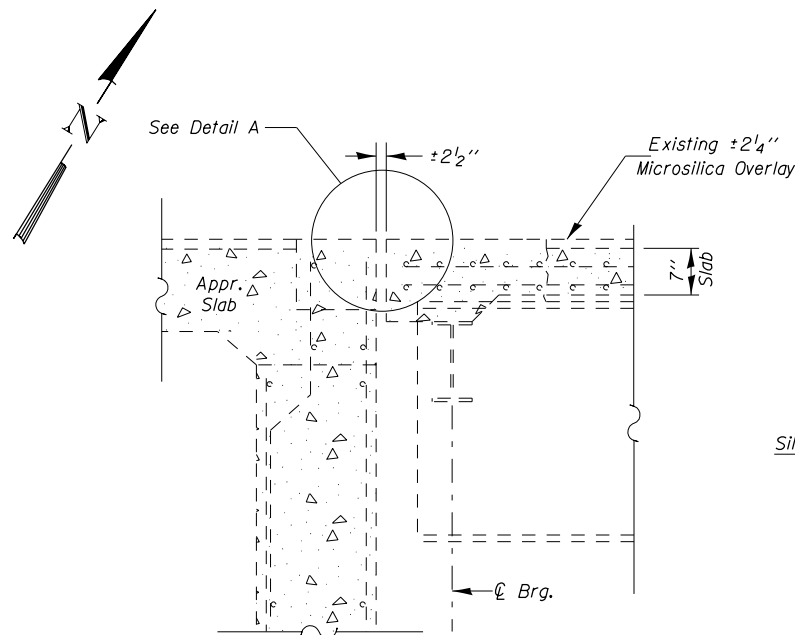
- Remove existing Neoprene Joints and replace with Polymer Nosings and Silicone.
- Removal and Replacement of the abutment Bearings.
- Deck Slab Repair (Full Depth).

Design Stresses

FIELD UNITS (New Construction)
 $f_y = 36,000$ psi (AASHTO M270 Gr. 36)
 FIELD UNITS (Existing Construction)
 $f_c = 1,400$ psi (super & sub.)
 $f_s = 20,000$ psi (structural steel)
 $f_s = 20,000$ psi (reinforcement)
 $V_c = 75$ psi (Figs.)
 $n = 10$

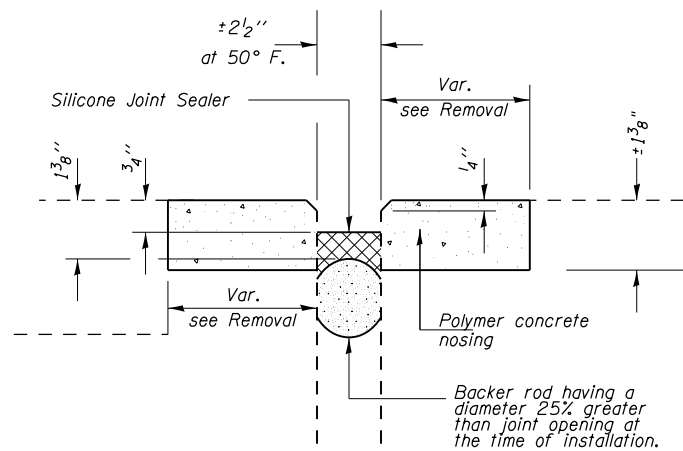
BRIDGE REPAIR
FAI RT 64 WB OVER
FAI RT 57 NBL
JEFFERSON COUNTY
SECTION (41-1-1)RS-1
STA. 20E+32.17 (RAMP E)
SN 041-0059

FILE NAME =	USER NAME = \$USER\$	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL PLAN & ELEVATION				F.A.I. RT.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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		CHECKED -	REVISED -						CONTRACT NO. 78416					
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT									

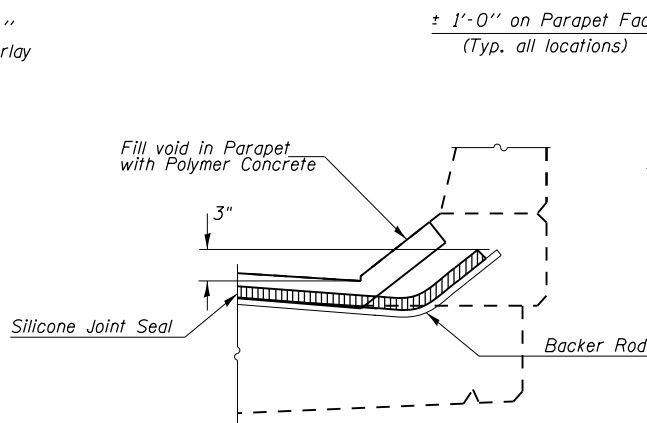


SECTION A-A

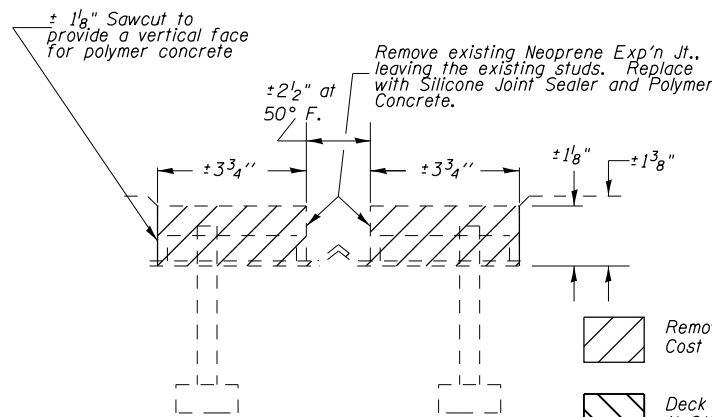
Dimensions measured @ Right Angles



SILICONE JOINT SEALER DETAIL

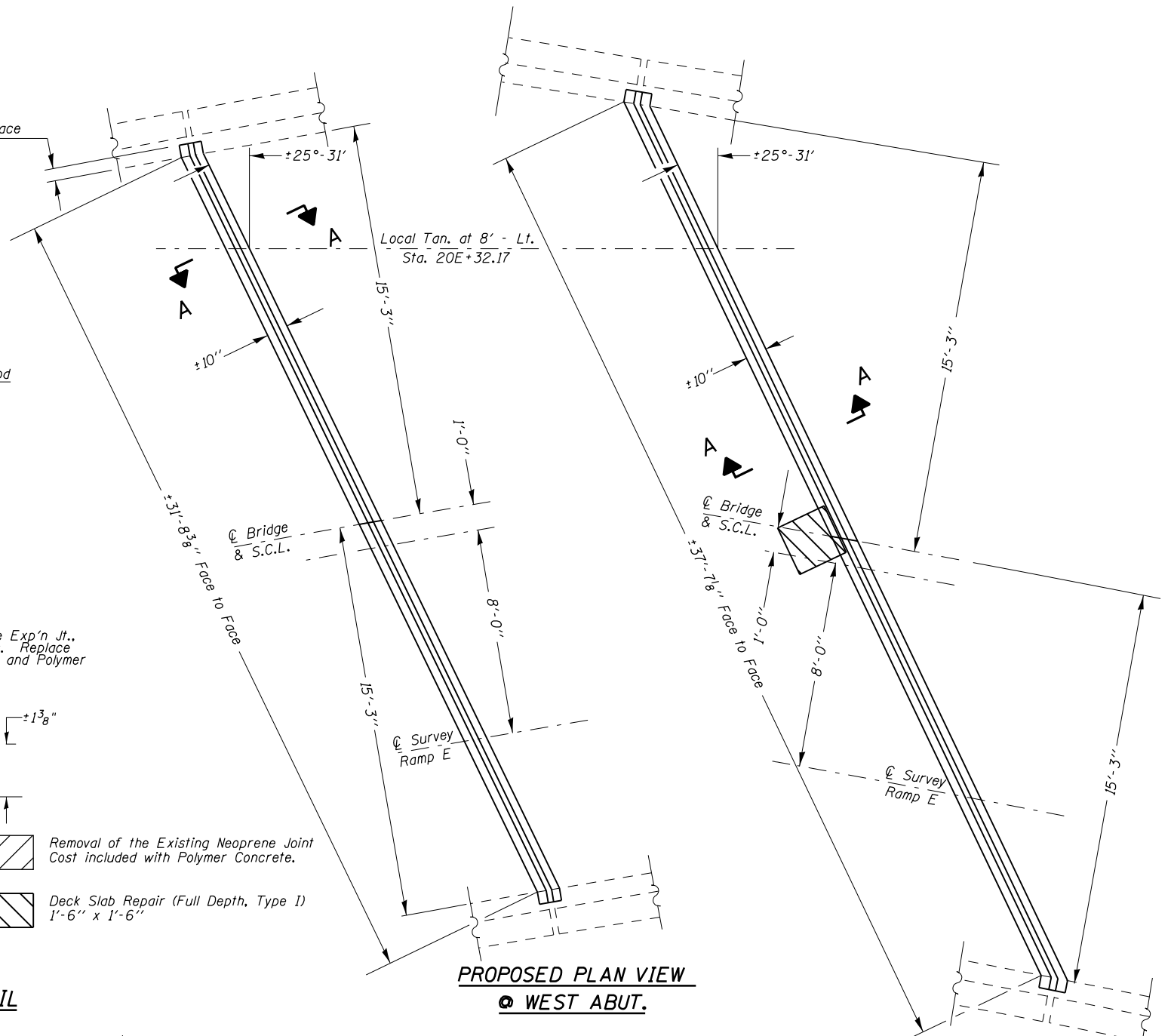


TYPICAL END OF SEAL TREATMENT AT EXPANSION JOINT



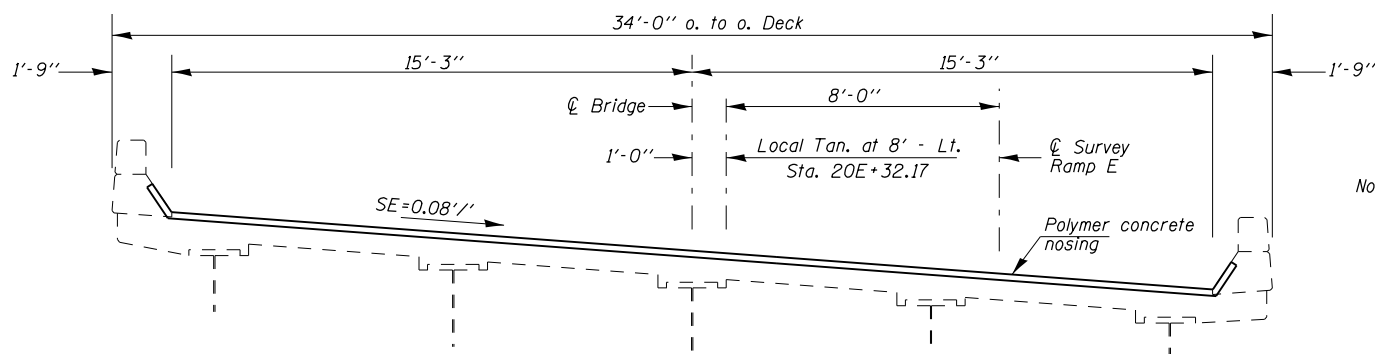
**DETAIL A
EXISTING 2 1/2" NEOPRENE JOINT DETAIL**

- Removal of the Existing Neoprene Joint Cost included with Polymer Concrete.
- Deck Slab Repair (Full Depth, Type I) 1'-6" x 1'-6"



**PROPOSED PLAN VIEW
WEST ABUT.**

**PROPOSED PLAN VIEW
EAST ABUT.**



**ELEVATION VIEW SHOWING
NEW POLYMER CONCRETE NOSINGS
Looking East**

Notes:
Staging should match that of the adjacent resurfacing. The Full Depth Patch shall be finished to conform with the adjacent deck. Once the curing is complete the Polymer Concrete Nosing will be installed continuously over the section as shown in the Silicone Joint Sealer Detail. A metal plate may be used to cover the open hole or during curing for the Full Depth Patch. The metal plate shall be capable of carrying the full weight of all vehicles, as approved by Engineer. Cost to be included with Deck Slab Repair (Full Depth, Type I).

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Deck Slab Repair (Full Depth, Type I)	Sq. Yd.	0.3
Silicone Joint Sealer, 2 1/2"	Foot	74
Polymer Concrete	Cu. Ft.	5.3

**JOINT DETAILS
JEFFERSON COUNTY
SN 041-0059**

FILE NAME =	USER NAME = \$USER\$	DESIGNED - TWH	REVISED -
et:\pwork\pwork\brandonja\0382650\04-0003.0004-sheet.dgn		DRAWN - TWH	REVISED -
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	PLOT DATE = 2/2/2015	DATE - 2/17/14	REVISED -

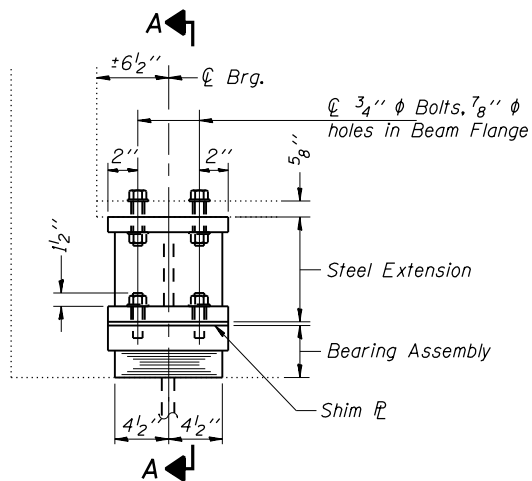
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

JOINTS AND OVERLAY DETAILS

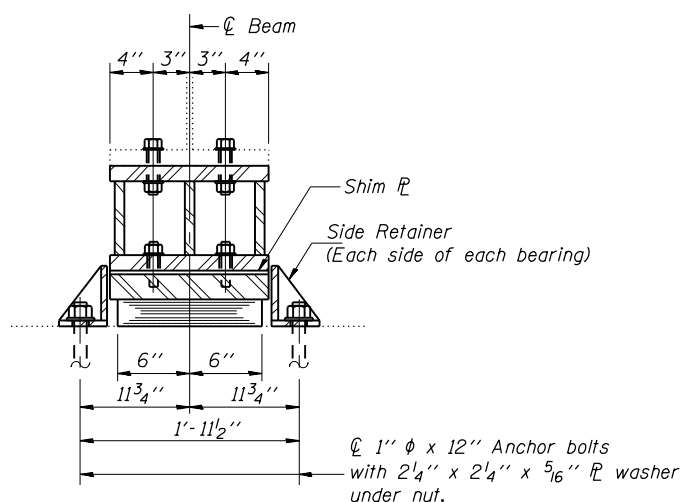
SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(41-1)RS-1	JEFFERSON	46	53
CONTRACT NO. 78416				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



ELEVATION AT EAST ABUT.



SECTION A-A

TYPE I ELASTOMERIC EXP. BRG.

Notes:

Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.

Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.

Side retainers and other steel members required for the bearing assembly shall be included in the cost of Elastomeric Bearing Assembly, Type I.

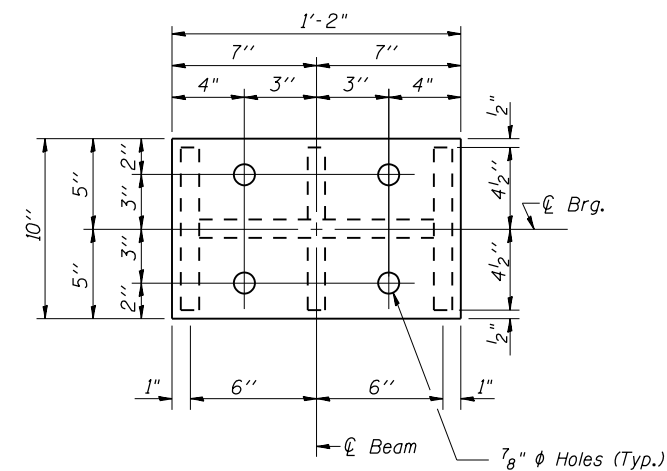
The minimum jack capacity required is 55 Tons.

Existing diaphragm removal and reinstallation may be required to facilitate drilling holes, cost to be included with "Jack and Remove Existing Bearings".

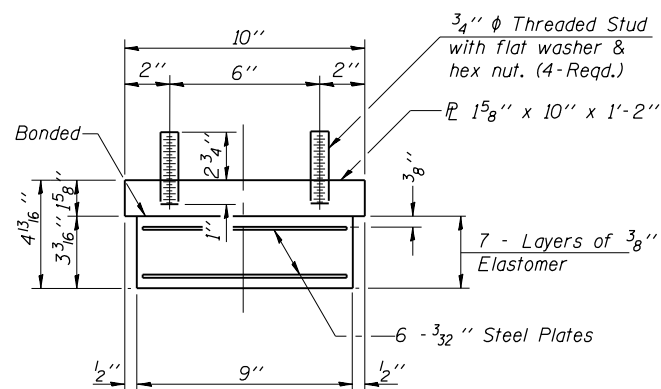
Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions.

Two 1/8 in. adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details.

New steel extensions, connection bolts, Fill P's and Shim P's are included in "Furnishing and Erecting Structural Steel".



PLAN-TOP & BOTTOM PLATE



BEARING ASSEMBLY

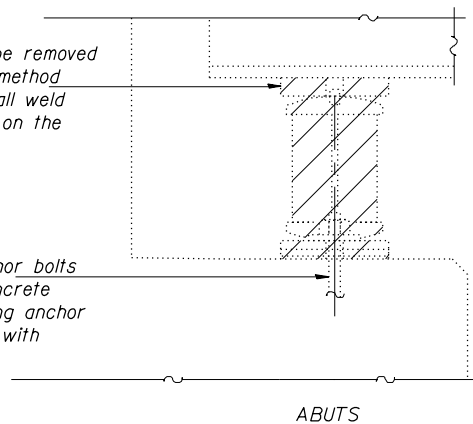
Note: Shim plates shall not be placed under Bearing Assembly.

GIRDER REACTIONS

R _P	(K)	34.1
R _L	(K)	45.2
Imp.	(K)	9.4
R (Total)	(K)	88.7

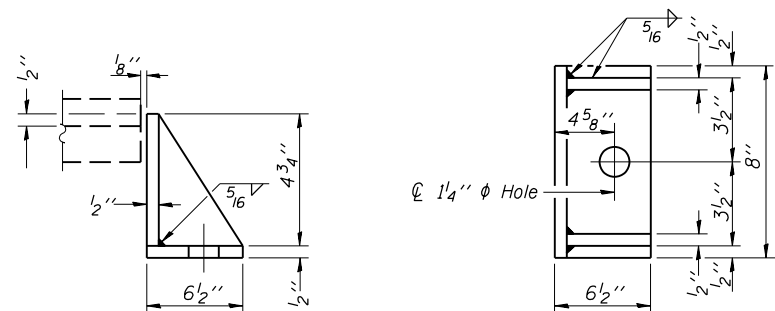
Existing Plate to be removed using the air-arc method and grind smooth all weld material remaining on the bottom flange.

Burn the existing anchor bolts flush with existing concrete surface. Grind existing anchor bolts smooth and seal with epoxy.



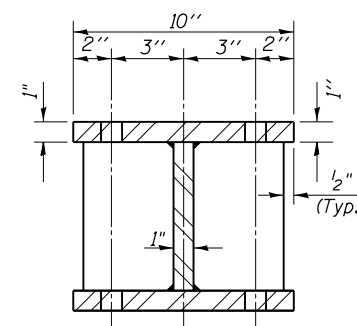
EXISTING BEARING REMOVAL DETAIL

Cost is included with Jack and Remove Existing Bearings

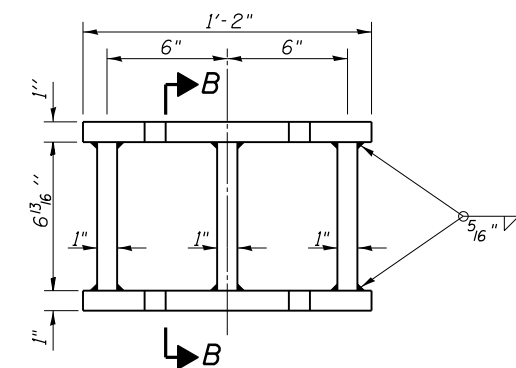


SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



SECTION B-B



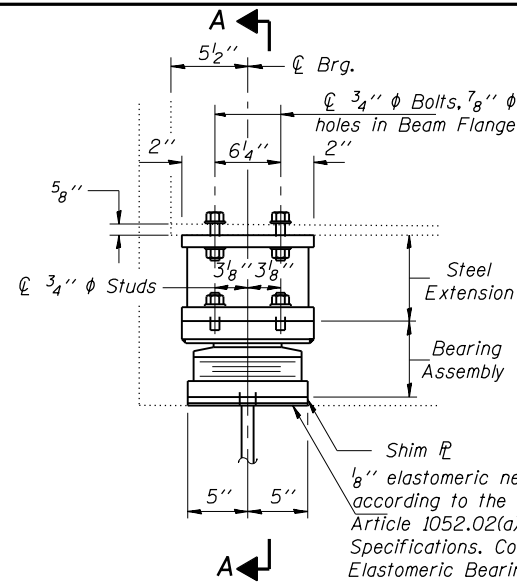
STEEL EXTENSION DETAIL

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly Type I	Each	5
Anchor Bolts, 1"	Each	10
Jack and Remove Existing Bearings	Each	5
Furnishing and Erecting Structural Steel	Pound	810

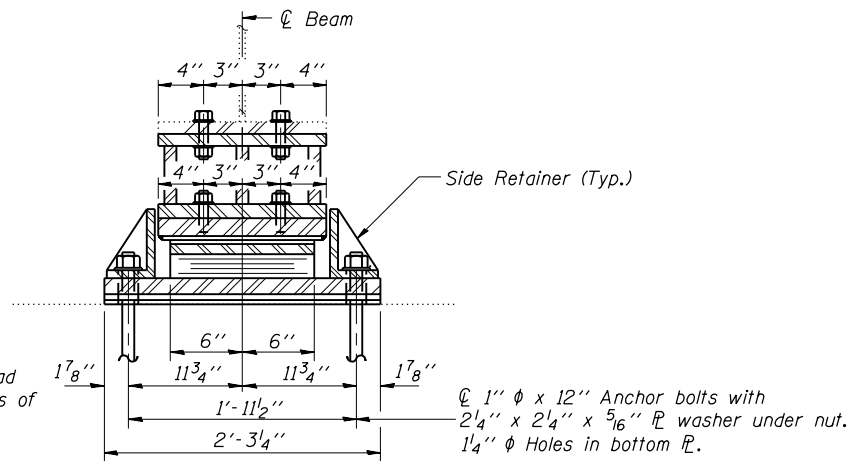
EAST ABUTMENT
BEARING DETAILS
STRUCTURE NO. 041-0059

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



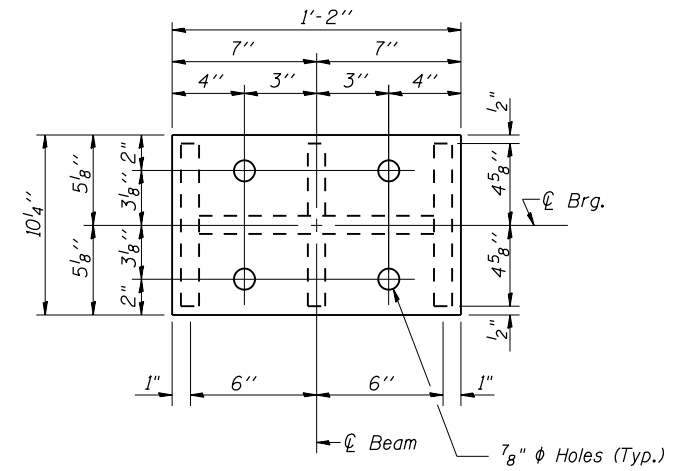
ELEVATION AT WEST ABUT.

TYPE II ELASTOMERIC EXP. BRG.

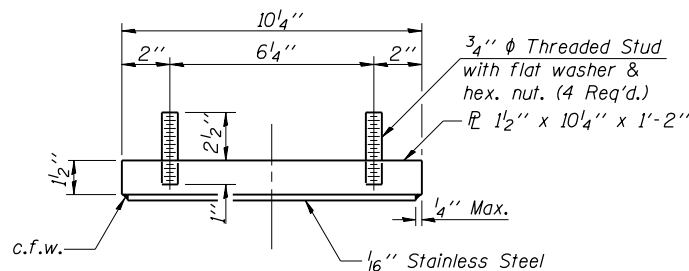


SECTION A-A

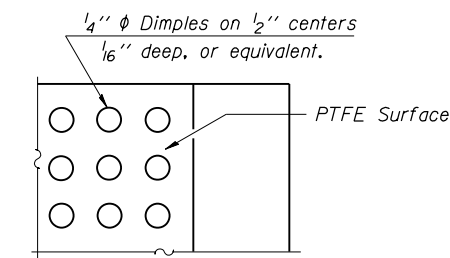
Notes:
Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
Anchor bolts for Type II bearings shall be placed in holes drilled in the concrete through holes in the bottom bearing plate after members are in place. Side retainers shall be placed after bolts are installed.
Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.
Side retainers and other steel members required for the bearing assembly shall be included in the cost of Elastomeric Bearing Assembly, Type II.
The 1/8" PTFE sheet shall be bonded directly to the top steel plate with a two-component, medium viscosity epoxy resin, conforming to the requirements of the Federal Specification MMM-A-134, Type I. The bond agent shall be applied on the full area of the contact surfaces.
Bonding of 1/8" PTFE sheet during vulcanizing process will be permitted provided the process and method of adjusting assembly height is approved by the Engineer.
The minimum jack capacity required is 55 Tons.
Existing diaphragm removal and reinstallation may be required to facilitate drilling holes, cost to be included with "Jack and Remove Existing Bearings".
Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions.
Two 1/2 in. adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details.
New steel extensions, connection bolts, Fill P's and Shim P's are included in "Furnishing and Erecting Structural Steel".



PLAN-TOP & BOTTOM PLATE



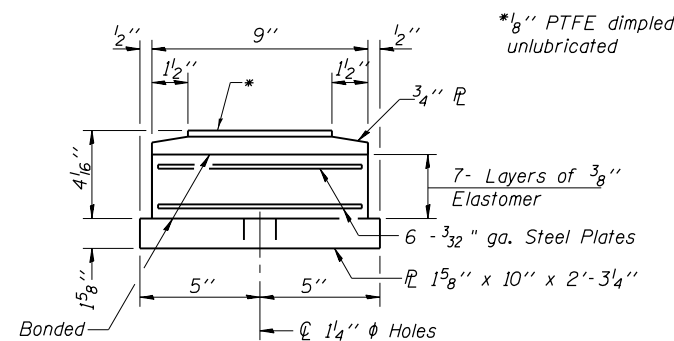
TOP BEARING ASSEMBLY



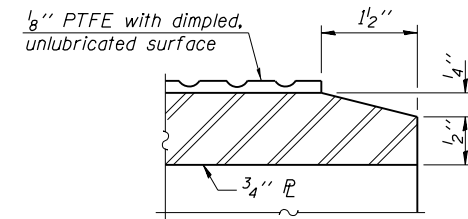
PLAN-PTFE SURFACE

GIRDER REACTIONS

R _L	(K)	27.2
R _R	(K)	44.0
Imp.	(K)	9.7
R (Total)	(K)	80.9



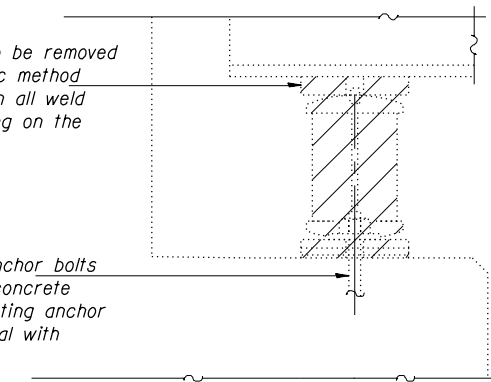
BOTTOM BEARING ASSEMBLY



SECTION THRU PTFE

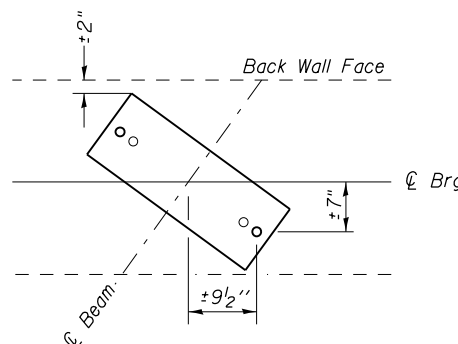
Existing Plate to be removed using the air-arc method and grind smooth all weld material remaining on the bottom flange.

Burn the existing anchor bolts flush with existing concrete surface. Grind existing anchor bolts smooth and seal with epoxy.

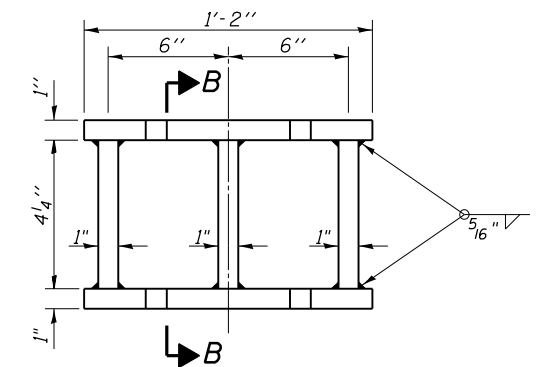


EXISTING BEARING REMOVAL DETAIL

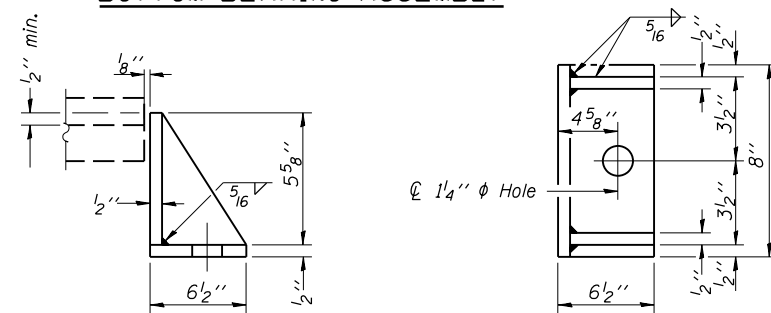
Cost is included with Jack and Remove Existing Bearings



ANCHOR BOLT LAYOUT

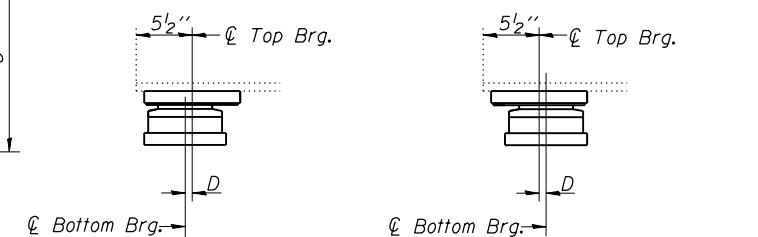


STEEL EXTENSION DETAIL



SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



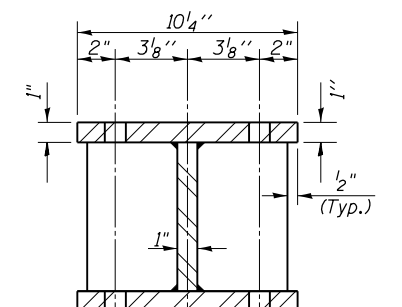
BELOW 50°F.

ABOVE 50°F.

(Move bottom. brg. away from fixed brg.)(Move bottom. brg. toward fixed brg.)

SETTING ANCHOR BOLTS AT EXP. BRG.

D=1/8" per each 100' of expansion for every 15° temp. change from the normal temp. of 50°F.



SECTION B-B

BILL OF MATERIAL

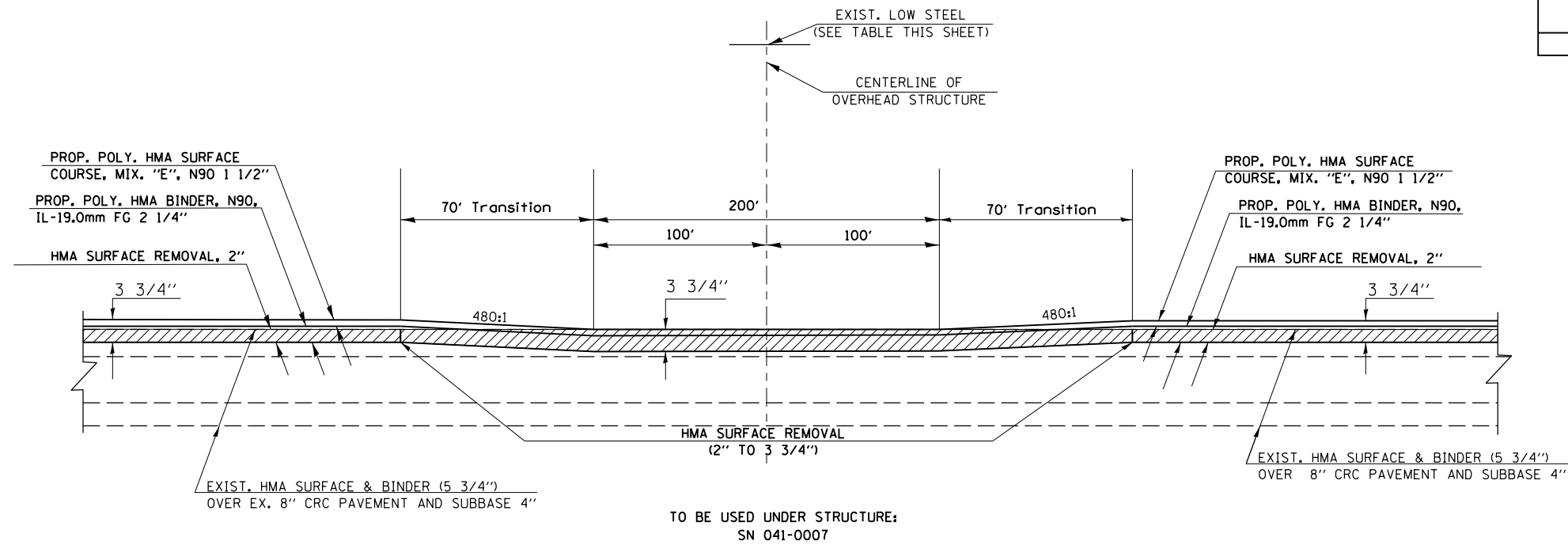
Item	Unit	Total
Elastomeric Bearing Assembly Type II	Each	5
Anchor Bolts, 1"	Each	10
Jack and Remove Existing Bearings	Each	5
Furnishing and Erecting Structural Steel	Pound	750

WEST ABUTMENT
BEARING DETAILS
STRUCTURE NO. 041-0059

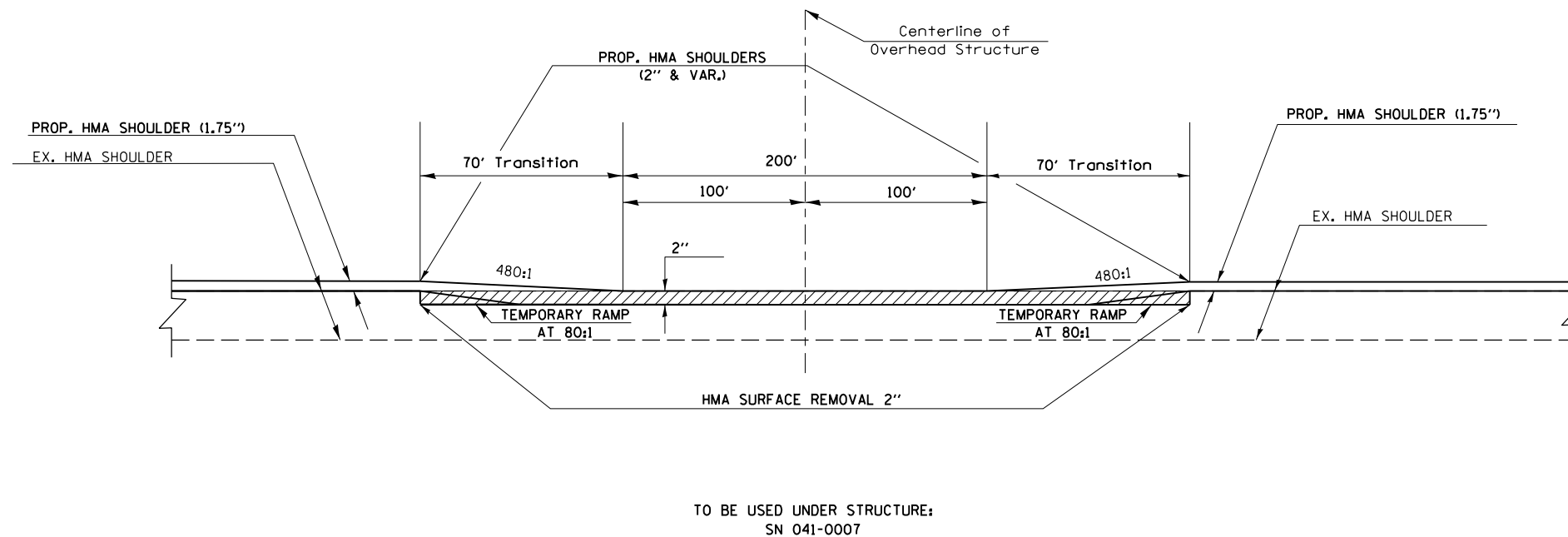
**DETAIL FOR HMA TRANSITION
UNDER OVERPASS
(TO MAINTAIN VERTICAL CLEARANCE)**

**EXIST. OVERPASS STRUCTURE
CLEARANCE HEIGHTS**

STRUCTURE NO.	EX. CLEARANCE HEIGHT (LOOKING EAST)		
	EOP	CL	EOP
041-0007	15.96'	16.58'	17.28'



**DETAIL OF PROPOSED SHOULDER
TREATMENT UNDER OVERPASS**



FILE NAME =	USER NAME = \$USER\$	DESIGNED -	REVISED -
ei:\pwork\pwork\pwork\brandonja\0382650\78416-shd-detailed.dgn		DRAWN -	REVISED -
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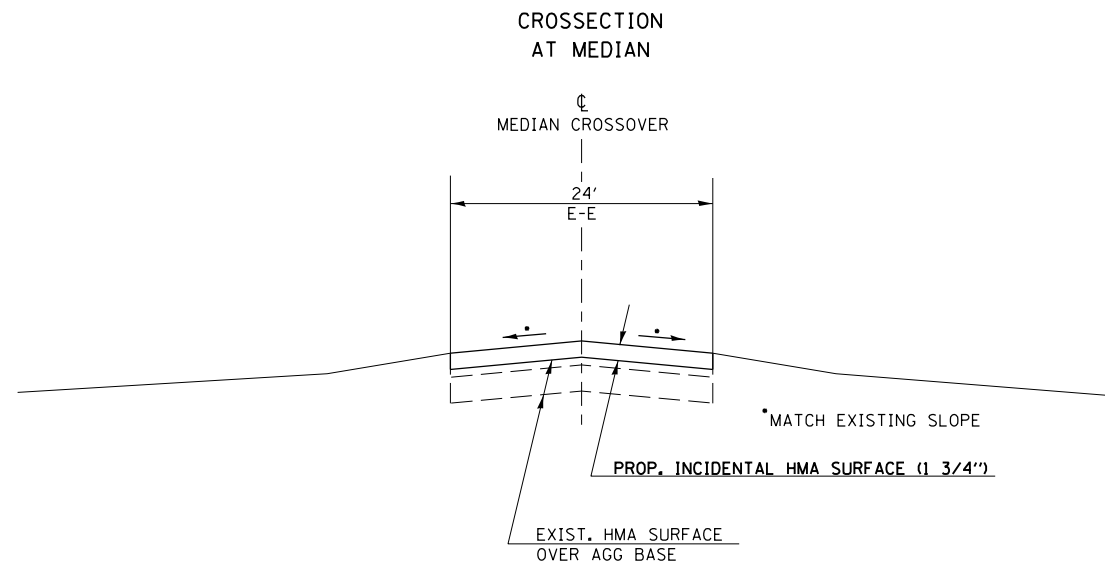
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**HMA TRANSITION DETAILS
UNDER STRUCTURE**

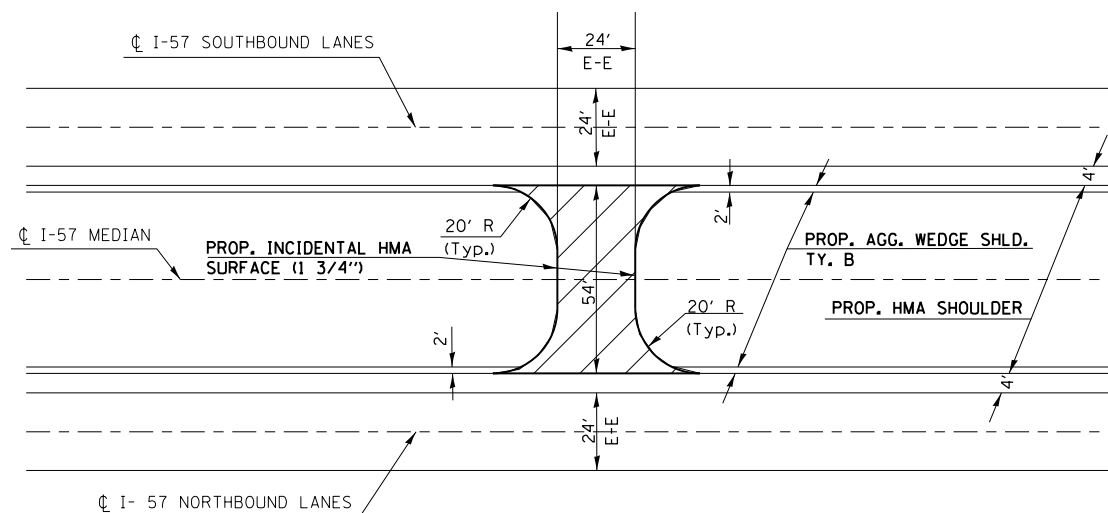
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-57	(41-1)RS-1	JEFFERSON	53	49
				CONTRACT NO. 78416
ILLINOIS FED. AID PROJECT				

DETAILS FOR
MEDIAN CROSSOVER

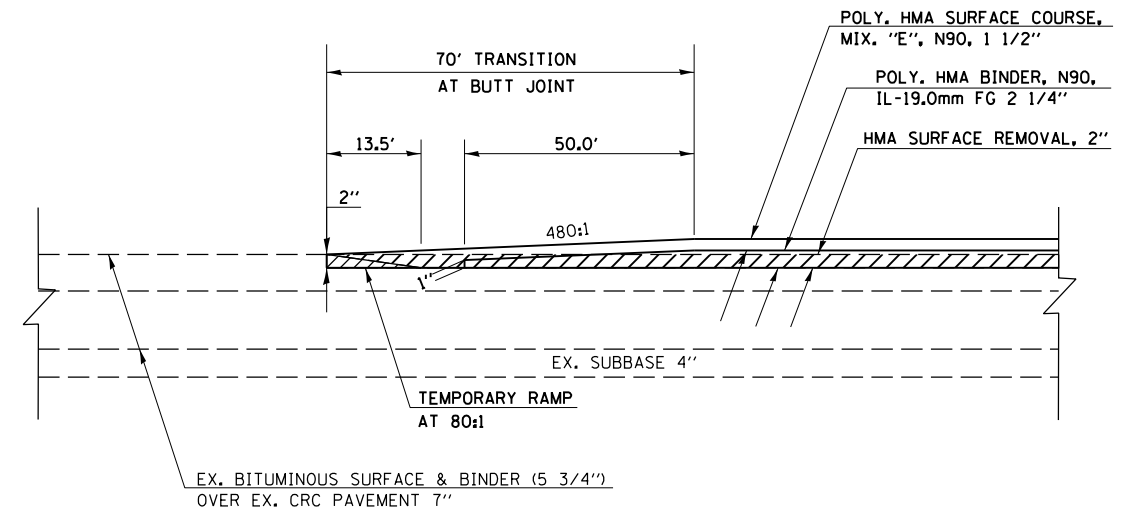


PLAN VIEW AT
MEDIAN CROSSOVER



TO BE USED:
I-57 STA. 546+52

DETAIL FOR TRANSITIONS AT
BEGINNING AND END OF PROJECT - MAINLINE ROADWAY



TO BE USED:

I-57 NB STA. 374+00.00 TO STA. 374+70.00

I-57 NB STA. 633B+40 TO STA. 634B+10

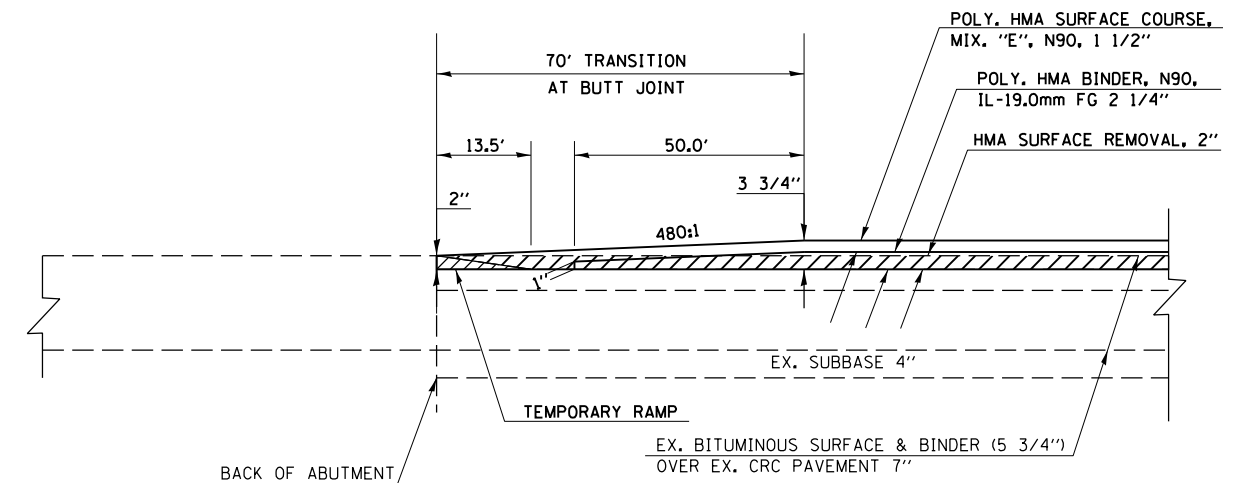
I-57 SB STA. 374+00.00 TO STA. 374+70.00

I-57 SB STA. 636+02.00 TO STA. 636+72.00

RAMP D (I-64 EB) STA. 2669+95.00 TO STA. 2670+65.00

RAMP C (I-64 WB) STA. 2669+95.00 TO STA. 2670+65.00

DETAIL FOR TRANSITIONS AT
STRUCTURES - MAINLINE & RAMPS ROADWAY



TO BE USED:

SN 041-0003 STA. 462+97.68 AND STA. 464+16.32

SN 041-0004 STA. 462+96.30 AND STA. 464+17.70

• SN 041-0005 STA. 490+97.19 AND STA. 494+62.81

• SN 041-0006 STA. 490+93.86 AND STA. 494+66.14

SN 041-0007 STA. 607B+78.01 AND STA. 610B+15.41

•• SN 041-0059 STA. 18+81.25 AND STA. 21+96.41

• BACK OF PCC CONNECTOR

•• BACK OF APPROACH PAVEMENT

FILE NAME =	USER NAME = \$USER\$	DESIGNED -	REVISED -
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\$MODELNAME\$	PLOT DATE = 2/2/2015	DATE -	REVISED -

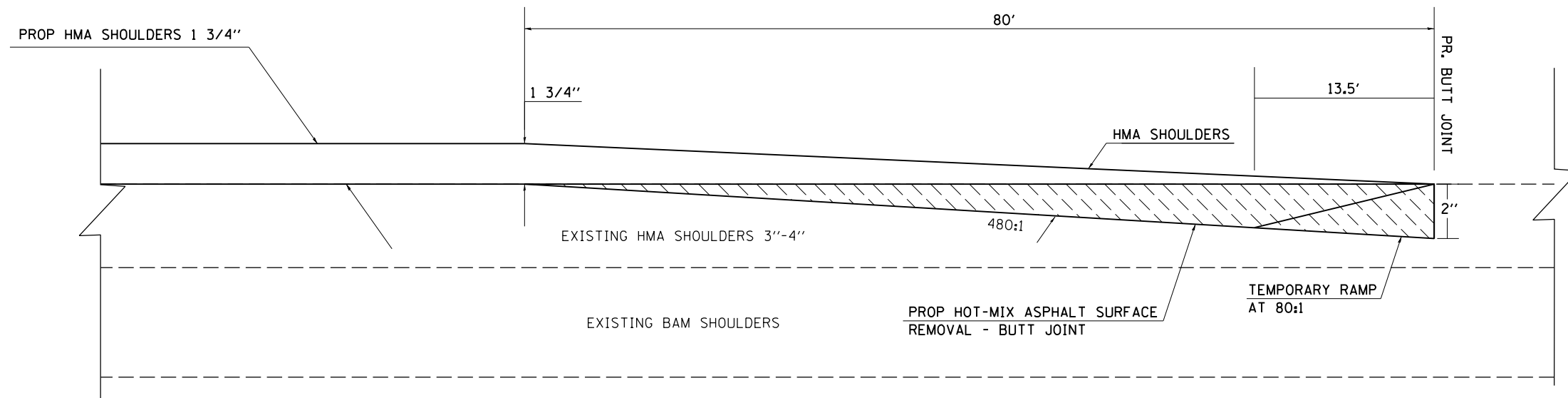
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

RESURFACING TRANSITIONS AND
MEDIAN CROSSOVER DETAILS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(41-1)RS-1	JEFFERSON	53	50
				CONTRACT NO. 78416
ILLINOIS FED. AID PROJECT				

**DETAIL FOR BUTT JOINT
TRANSITIONS OF SHOULDERS**



TO BE USED:

- I-57 NB STA. 374+00.00 TO STA. 374+80.00
- I-57 NB STA. 633B+40 TO STA. 634B+20
- I-57 SB STA. 374+00.00 TO STA. 374+80.00
- I-57 SB STA. 635+92.00 TO STA. 636+72.00
- RAMP D (I-64 EB) STA. 2669+85.00 TO STA. 2670+65.00
- RAMP C (I-64 WB) STA. 2669+85.00 TO STA. 2670+65.00
- SN 041-0003 STA. 462+97.68 AND STA. 464+16.32
- SN 041-0004 STA. 462+96.30 AND STA. 464+17.70
- * SN 041-0005 STA. 490+97.19 AND STA. 494+62.81
- * SN 041-0006 STA. 490+93.86 AND STA. 494+66.14
- SN 041-0007 STA. 607B+78.01 AND STA. 610B+15.41
- ** SN 041-0059 STA. 18+81.25 AND STA. 21+96.41

* BACK OF PCC CONNECTOR

** BACK OF APPROACH PAVEMENT

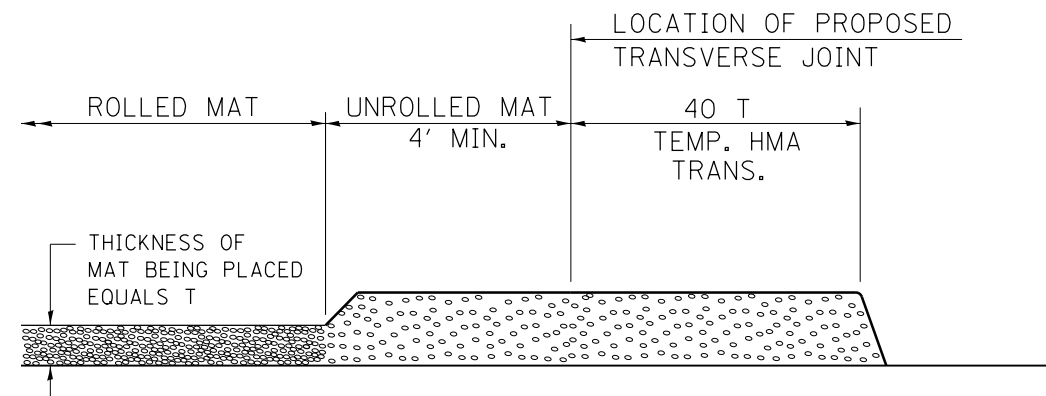
NOTE: THE FOLLOWING LOCATIONS INVOLVE REMOVING MATERIAL THAT HAVE A MIX OF PCC AND HMA. THIS WILL NOT BE PAID FOR SEPARATELY BUT INCLUDED IN THE COST OF HMA SURFACE REMOVAL - BUTT JOINT.

LOCATIONS:

- SN 041-0003 & SN 041-0004
- SN 041-0005 & SN 041-0006

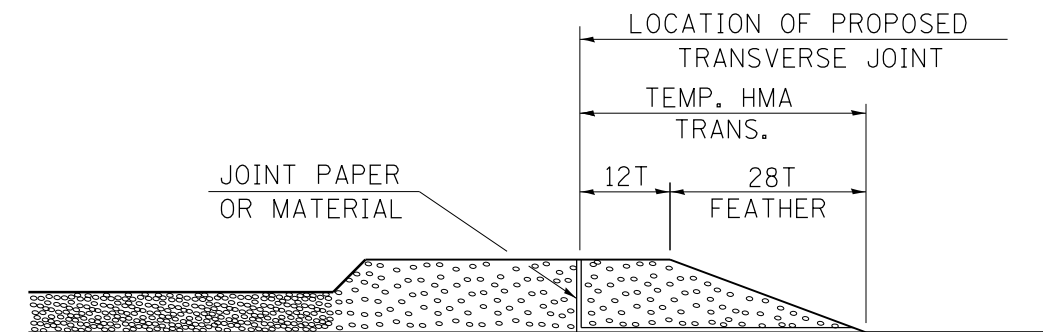
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et:\pwork\pwork\brandonja\d0382650\78416-shld-detail.dgn		DRAWN -	REVISED -					57	(41-1)RS-1	JEFFERSON	53	51
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	PLOT DATE = 2/2/2015	DATE -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.			

TEMPORARY HOT-MIX ASPHALT TRANSITIONS



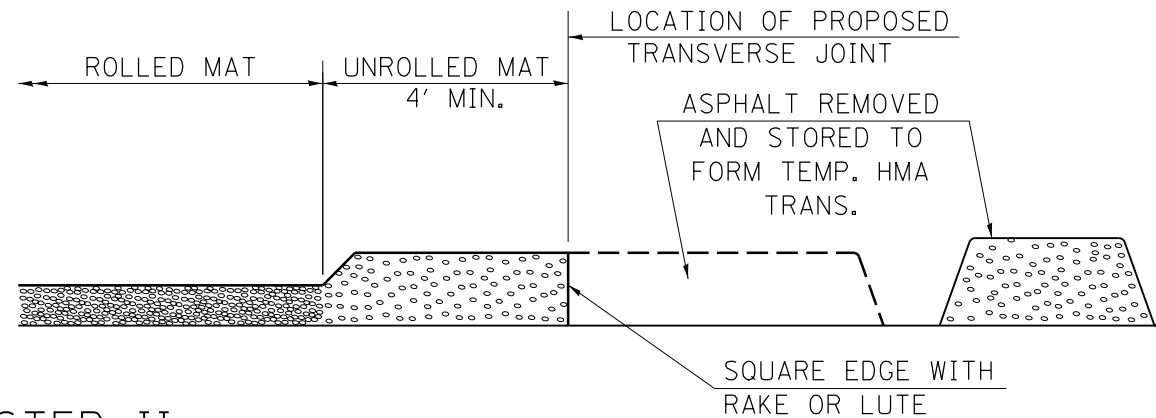
STEP I

1. PLACE HOT-MIX ASPHALT MAT, LENGTH 40 TIMES THE THICKNESS OF THE MAT BEING PLACED PAST THE PROPOSED TRANSVERSE JOINT LOCATION USING NORMAL OPERATING PROCEDURES.
2. EXTREME CARE SHOULD BE TAKEN TO MAINTAIN ENOUGH MATERIAL IN FRONT OF THE SCREED TO MAINTAIN REQUIRED PAVING DEPTH.



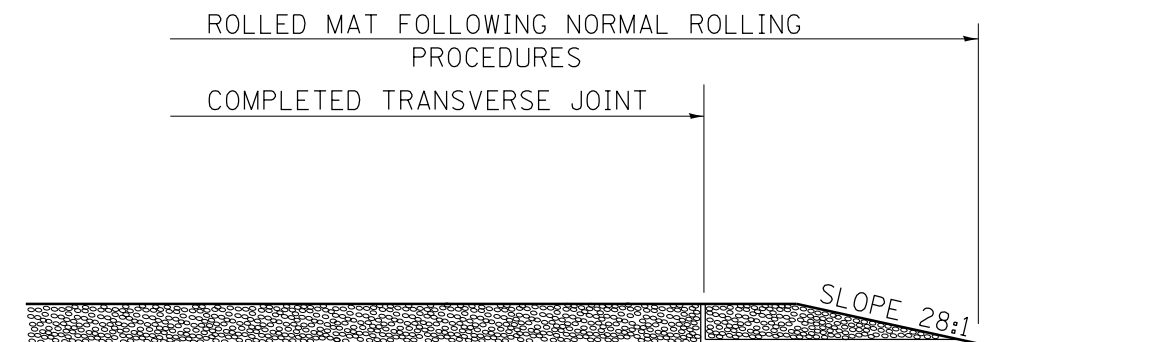
STEP III

1. JOINT PAPER OR OTHER PRESELECTED JOINT MATERIAL IS THEN PLACED IN THE CLEARED AREA AND THE EXCESS ASPHALT USED TO HAND FORM A TRANSITION TO THE DIMENSIONS SHOWN ABOVE.
2. NOTE THAT IN CONSTRUCTING THE TRANSITION, THE MAT DEPTH IS CONTINUED AS PART OF THE TRANSITION BEFORE FORMING THE FEATHER.



STEP II

1. MOVE THE PAVER OUT OF THE WAY AND REMOVE THE ASPHALT FROM THE AREA OF THE PROPOSED TEMPORARY HOT-MIX ASPHALT TRANSITION.
2. SQUARE UP THE END OF THE MAT WITH A RAKE OR LUTE.
3. NOTE THAT THE MAT WITHIN 4' OF THE END OF JOINT IS NOT TO BE ROLLED AT THIS TIME.



STEP IV

1. COMPLETE TEMPORARY TRANSITION BY ROLLING.
2. TO RESUME PAVING, AT THE JOINT, REMOVE TEMPORARY TRANSITION AND DISPOSE OF THE MATERIAL ACCORDING TO ART. 202.03 OF THE STD. SPECS. (COST INCLUDED IN THE CONTRACT).
3. CONSTRUCTING THE TEMPORARY TRANSITIONS WILL NOT BE PAID FOR SEPARATELY IN ACCORDANCE WITH ARTICLE 406.14 OF THE STANDARD SPECIFICATIONS.

REVISIONS	
REDRAWN	2-15-89
REVISED	8-16-94
REVISED	01-09-07
RESIZED	05-8-08
REVISED	05-16-13

STD. 9-26

FILE NAME =	USER NAME = \$USER\$	DESIGNED -	REVISED -
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	PLOT DATE = 2/2/2015	DATE -	REVISED -

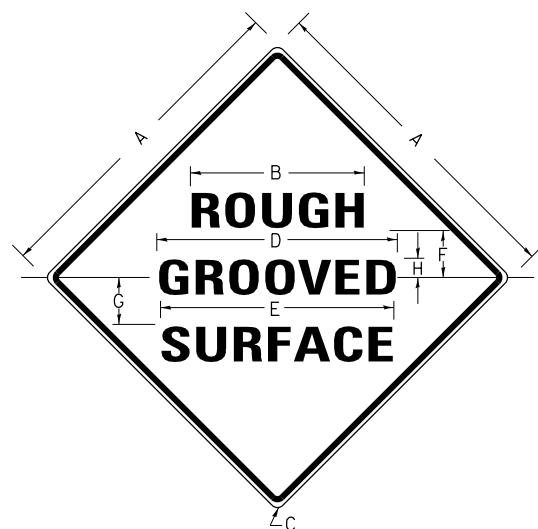
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TEMPORARY HOT-MIX ASPHALT TRANSITIONS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(41-1)RS-1	JEFFERSON	53	52
CONTRACT NO. 78416				
ILLINOIS FED. AID PROJECT				

ILLINOIS STANDARD



COLORS:

LEGEND AND BORDER- BLACK NON-REFLECTORIZED
BACKGROUND- ORANGE REFLECTORIZED

SIGN SIZE	DIMENSIONS							
	A	B	C	D	E	F	G	H
48X48	48.0	24.1	3.0	34.0	33.0	6.0	13.0	3.5

SIGN SIZE	SERIES LINES			MAR-GIN	BOR-DER	BLANK STD.
	1	2	3			
48X48	7C	7C	7C	0.8	1.2	B4-48D

ALL DIMENSIONS IN INCHES

NOTES:

PRIOR TO ALLOWING TRAFFIC ON ANY PORTION OF THE ROADWAY THAT HAS BEEN COLDMILLED, THE CONTRACTOR SHALL HAVE ERECTED "ROUGH GROOVED SURFACE" SIGNS THAT CONFORM TO THE ABOVE DETAILS. A MINIMUM OF ONE SIGN AT EACH END OF THE IMPROVEMENT WILL BE REQUIRED. THE CONTRACTOR SHALL MAINTAIN THE "ROUGH GROOVED SURFACE" SIGNS UNTIL THE COLDMILLED SURFACE IS COVERED WITH LEVELING BINDER OR SURFACE COURSE.

IF AT ANY TIME THE SIGNS ARE IN PLACE BUT NOT APPLICABLE, THEY SHALL BE TURNED FROM THE VIEW OF MOTORISTS OR COVERED AS DIRECTED BY THE ENGINEER.

THE COST OF FURNISHING, ERECTING, MAINTAINING, AND REMOVING THE REQUIRED SIGNS SHALL BE INCLUDED IN THE CONTRACT.

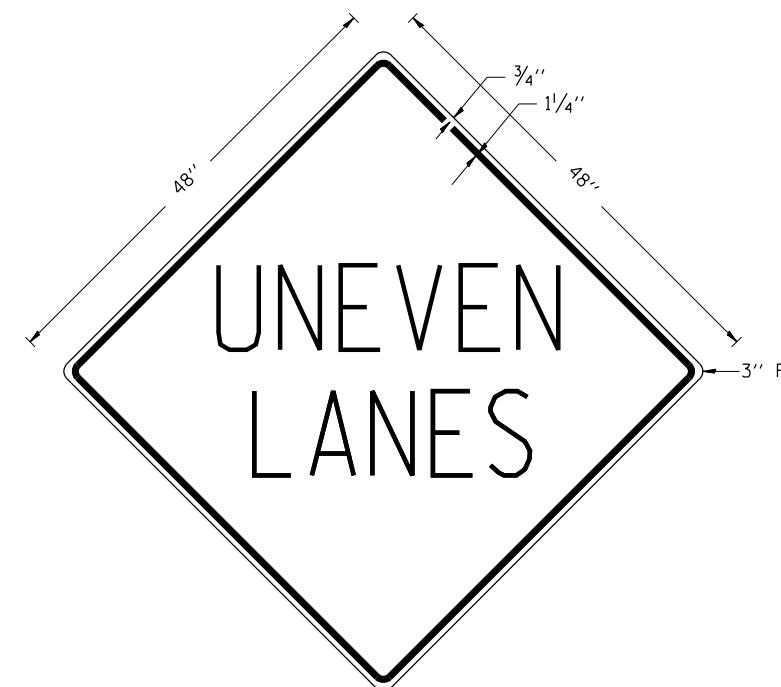
REVISIONS

REDRAWN	2-15-89
REVISED	4-6-93
REVISED	3-27-08
REVIEWED	5-17-13

STD. 9-39

UNEVEN LANES SIGN

W8-11 (48" x 48")



COLORS:

LEGEND AND BORDER - BLACK NON-REFLECTORIZED
BACKGROUND - ORANGE REFLECTORIZED

NOTE: PRIOR TO ALLOWING TRAFFIC ON ANY PORTION OF THE ROADWAY THAT HAS BEEN COLDMILLED OR BEFORE RESURFACING OPERATIONS BEGIN, THE CONTRACTOR SHALL HAVE ERECTED "UNEVEN PAVEMENT" SIGNS THAT CONFORM TO THE ABOVE DETAILS. A MINIMUM OF ONE SIGN AT EACH END OF THE IMPROVEMENT WILL BE REQUIRED. THE CONTRACTOR SHALL MAINTAIN THE "UNEVEN PAVEMENT" SIGNS UNTIL THE RESURFACING OPERATIONS ARE COMPLETED.

IF AT ANY TIME THE SIGNS ARE IN PLACE BUT NOT APPLICABLE, THEY SHALL BE TURNED FROM THE VIEW OF MOTORISTS OR COVERED AS DIRECTED BY THE ENGINEER.

THE COST OF FURNISHING, ERECTING, MAINTAINING, AND REMOVING THE REQUIRED SIGNS SHALL BE INCLUDED IN THE CONTRACT.

REVISIONS

DRAWN	2-15-89
REVISED	4-06-93
REDESIGNED	
RESIZED	
REVIEWED	5-17-13

STD. 9-41

FILE NAME =	USER NAME = \$USER\$	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROUGH GROOVED SURFACE AND UNEVEN LANE SIGN DETAILS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
et:\pw\work\p\dot\brandonja\d0382650\78416-shd-d-details.dgn		DRAWN -	REVISED -			57	(41-1)RS-1	JEFFERSON	53	53	
\$MODELNAME\$		CHECKED -	REVISED -			CONTRACT NO. 78416					
	PLOT DATE = 2/2/2015	DATE -	REVISED -			SCALE:	SHEET	OF	SHEETS	STA.	TO

ILLINOIS FED. AID PROJECT