

PROPOSED HIGHWAY PLANS

**LOC. 1: FAU 3565: IL 171 (ARCHER AVE)
OVER US 45 SB RAMP TO I-294**

**LOC. 2: FAU 3593: MICHIGAN CITY RD
OVER I-94 (BISHOP FORD)**

**SECTION 2014-0741
COOK COUNTY
BRIDGE BEAM REPLACEMENT**

C-91-094-15

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
11 FAU 3565	21 FAU 3593	ILLINOIS	18	1
			CONTRACT NO. 62A17	

*18-1-17

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS IMPROVEMENT IS LOCATED IN THE VILLAGE OF JUSTICE AND THE VILLAGE OF DOLTON.

TRAFFIC DATA

IL 171

2013 ADT=7650
SPEED LIMIT=45 MPH

US 45 SB RAMP

2013 ADT=4900
SPEED LIMIT= 45 MPH

MICHIGAN CITY ROAD

2010 ADT=9300
SPEED LIMIT=35 MPH

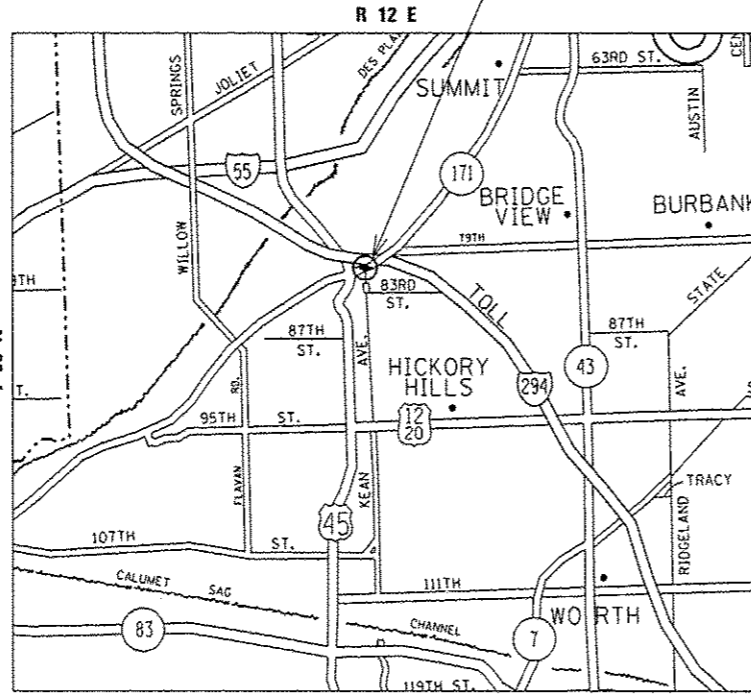
I-94

2013 ADT=122400
SPEED LIMIT=55 MPH



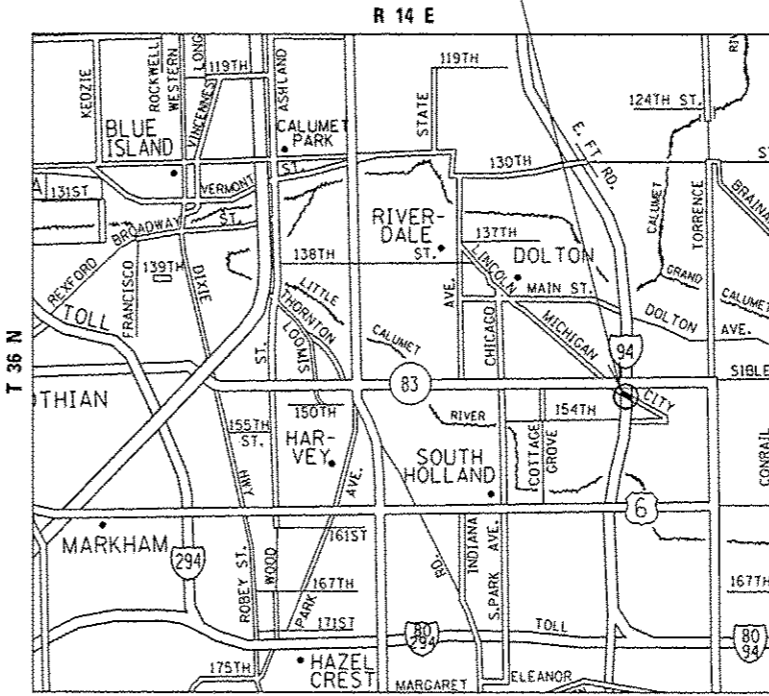
LOCATION OF SECTION INDICATED THUS: - [shaded box] -

LOCATION 1: SN 016-0209
IL 171 OVER US 45 SB RAMP TO I-294

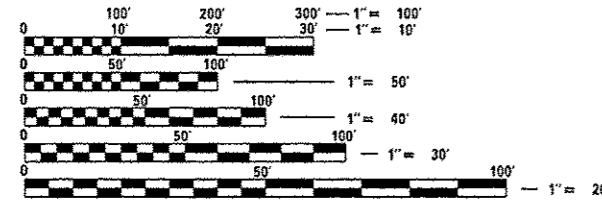


LYONS TOWNSHIP

LOCATION 2: SN 016-1068
MICHIGAN CITY RD OVER I-94



THORNTON TOWNSHIP



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: ALAIN MIDY (847) 221-3056
PROJECT MANAGER: ISSAM RAYAN (847) 705-4178

CONTRACT NO. 62A17

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED December 17, 2014

John F. [Signature]
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

Jan 30, 2015
John D. Baranzelli P.E.
ENGINEER OF DESIGN AND ENVIRONMENT

Jan 30, 2015
Orin Goman P.E.
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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* Deleted Sheet 10.

STATE STANDARDS

- 643001-02 SAND MODULE IMPACT ATTENUATORS
- 701400-08 APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
- 701401-09 LANE CLOSURE, FREEWAY/EXPRESSWAY
- 701423-08 LANE CLOSURE, MULTILANE WITH BARRIER, FOR SPEEDS ≥ 45 MPH TO 55 MPH
- 701428 TRAFFIC CONTROL SETUP AND REMOVAL FREEWAY/EXPRESSWAY
- 701446-06 TWO LANE CLOSURE FREEWAY/EXPRESSWAY
- 701601-09 URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
- 701901-04 TRAFFIC CONTROL DEVICES
- 704001-07 TEMPORARY CONCRETE BARRIER

GENERAL NOTES (BRIDGE)

THE CONTRACTOR SHALL TAKE NECESSARY PRECAUTIONS TO INSURE THAT NO DEBRIS WILL ENDANGER OR INTERFERE WITH TRAFFIC ON THE ROADWAY BENEATH THE BRIDGE ACCORDING TO ARTICLE 107.09 (or 107.12) OF THE STANDARD SPECIFICATIONS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE APPROPRIATE PAY ITEM INVOLVED AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

CONCRETE SUPERSTRUCTURE SHALL HAVE A SEVEN DAY MINIMUM CURE.

ALL VERTICAL CLEARANCE SIGNAGE, IF EXISTING, SHALL BE RESTORED TO THE CONDITION EXISTING PRIOR TO THE START OF WORK ASSOCIATED WITH THIS CONTRACT.

WHEN REMOVING THE EXISTING DAMAGED BEAM, THE CONTRACTOR SHALL TAKE APPROPRIATE PRECAUTIONS TO ENSURE THAT THE DECK AND THE BEAM ARE PROPERLY SUPPORTED DURING THE ENTIRE REMOVAL OPERATION SO AS TO PROTECT THE TRAFFIC AND PAVEMENT ON AND BELOW THE BRIDGE. THE CONTRACTOR SHALL SUBMIT THE REMOVAL SEQUENCE AND PROCEDURE TO THE ENGINEER FOR APPROVAL PRIOR TO REMOVAL OF THE EXISTING DECK, DAMAGED BEAM OR CARRIER BEAM. THIS COST IS INCLUDED IN THE COST OF "STRUCTURAL STEEL REMOVAL"

SLIPFORMING OF PARAPETS IS NOT ALLOWED.

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGES OF JUSTICE AND DOLTON.

THE CONTRACTOR WILL NOT BE ABLE TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED BASED AT THE UNIT PRICE BID FOR THE WORK.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847)705-4470 (FOR ARTERIALS) AND (847)705-4155 (FOR EXPRESSWAYS) A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

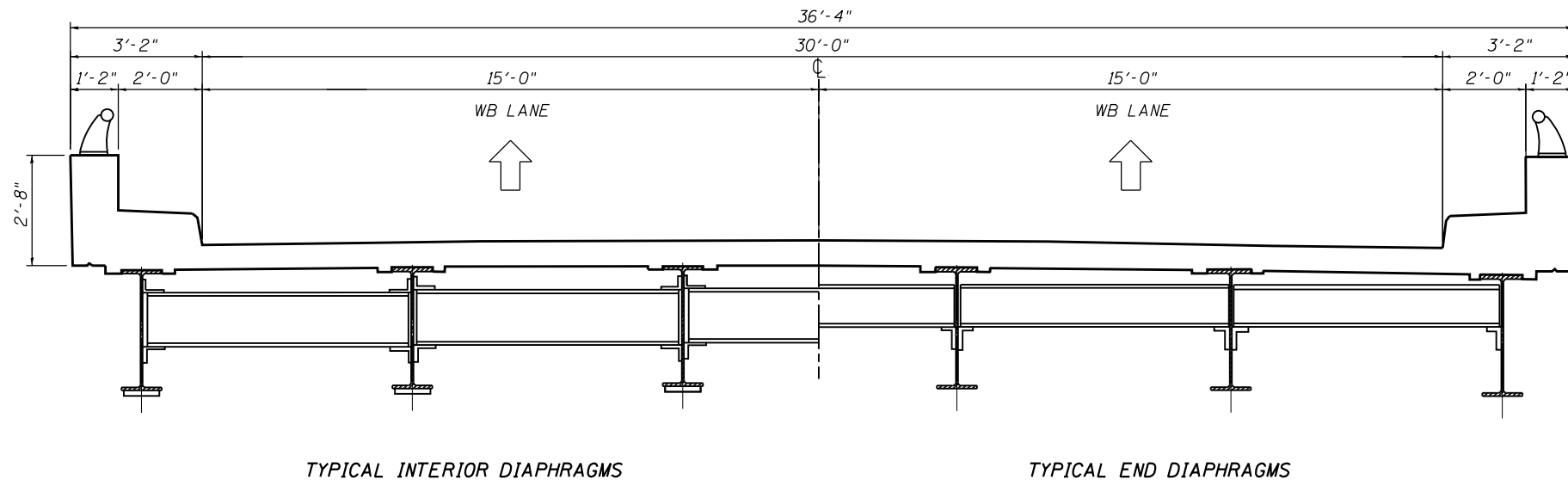
THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM THE BUREAU OF MAINTENANCE BRIDGE INSPECTORS.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

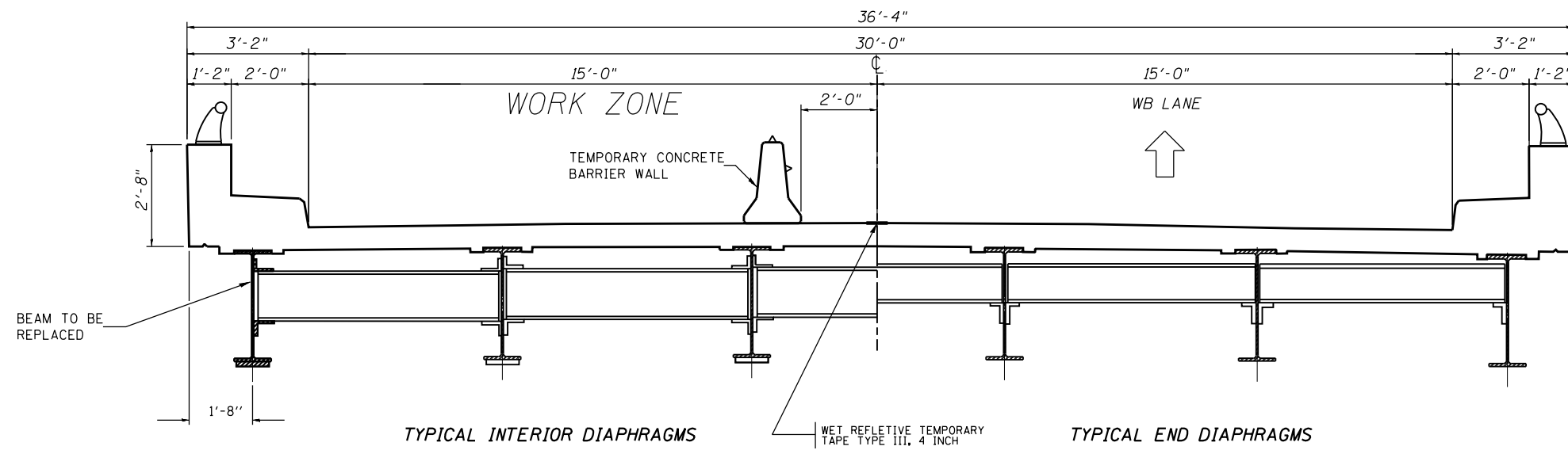
THE "ARTERIAL ROAD INFORMATION SIGN (TC-22)" IS APPLICABLE ONLY TO ARTERIAL ROADS AND SHALL NOT BE APPLIED TO EXPRESSWAYS/TOLLWAYS.

Rev. 3-12-15

FILE NAME :	USER NAME : raeqan	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, STATE STANDARDS, & GENERAL NOTES	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\p-work\pdoc\raeqan\0418085\010915-ent-gannote.dgn	DRAWN -	REVISED -	2014-0741			COOK	18	2		
PLOT SCALE * 100.0000 ' / in.	CHECKED -	REVISED -	1) FAU 3565			2) FAU 3593	CONTRACT NO. 62A17			
PLOT DATE * 12/22/2014	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							
Default				SCALE: N/A	SHEET 1 OF 1 SHEETS	STA. TO STA.				



**EXISTING
CROSS SECTION**



STAGING

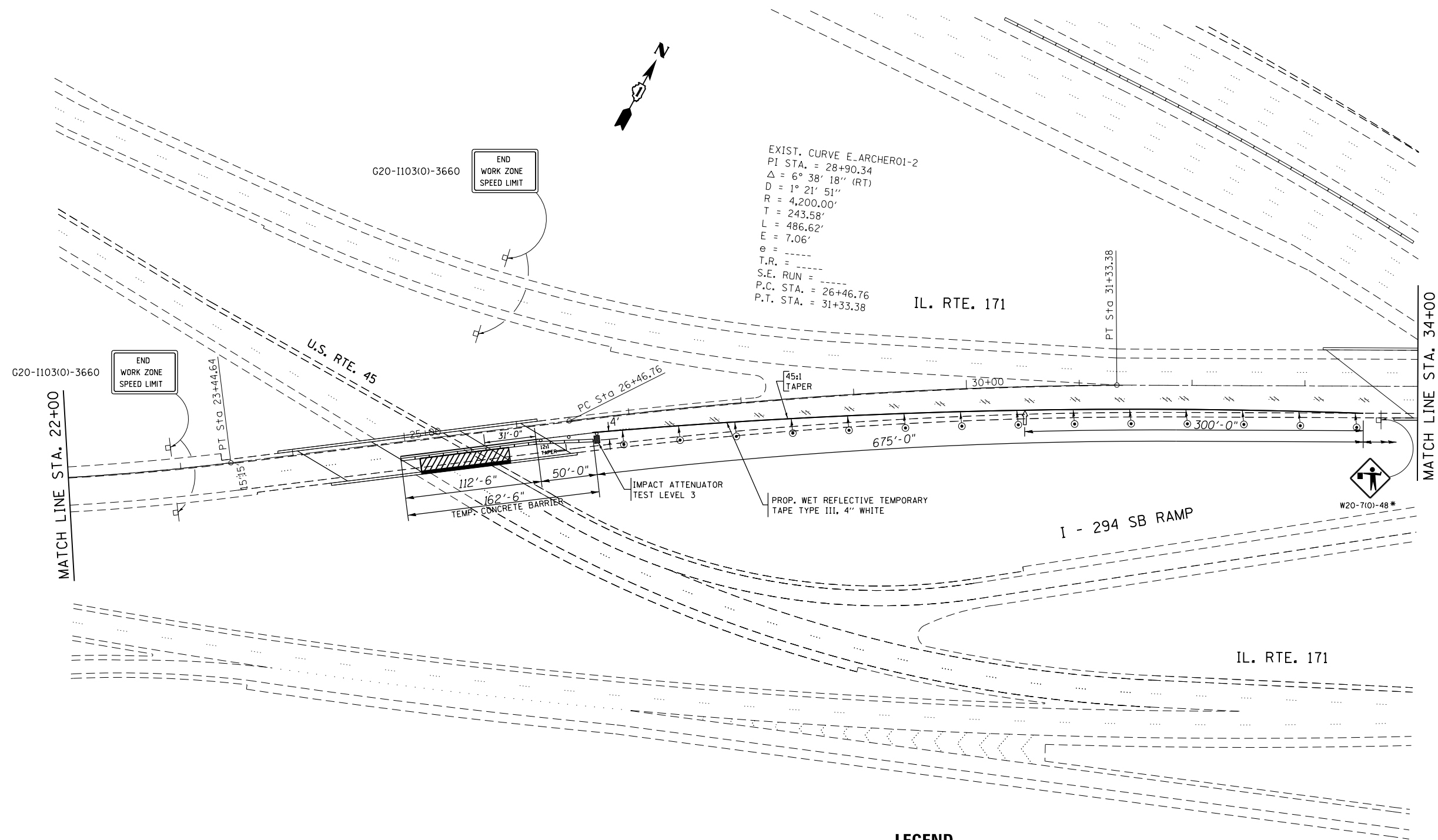
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	PLOT DATE = 12/18/2014	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**STAGING & TRAFFIC CONTROL PLAN - TYPICAL SECTIONS
IL 171 (ARCHER AVE) OVER US 45 SB RAMP TO I-294: SN 016-0209**

SCALE: NTS SHEET 1 OF 3 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	2014-074I	COOK	18	4
*1) FAU 3565 2) FAU 3593		CONTRACT NO. 62A17		
ILLINOIS FED. AID PROJECT				










EXIST. CURVE E_ARCHER01-2
 PI STA. = 28+90.34
 $\Delta = 6^\circ 38' 18''$ (RT)
 $D = 1^\circ 21' 51''$
 $R = 4,200.00'$
 $T = 243.58'$
 $L = 486.62'$
 $E = 7.06'$
 $e =$
 $T.R. =$
 $S.E. RUN =$
 $P.C. STA. = 26+46.76$
 $P.T. STA. = 31+33.38$

NOTES:

* REMOVE SIGN IF FLAGGER IS NOT PRESENT FOR MORE THAN ONE HOUR.

** WHEN CONSTRUCTION IS OVER, ALL EXISTING PAVEMENT MARKING REMOVED SHALL BE REPLACED WITH THERMOPLASTIC PAVEMENT MARKING (OVER HMA PAVEMENT) AND POLYUREA PAVEMENT MARKING TYPE I (OVER PCC PAVEMENT).

LEGEND

-  ARROW BOARD
-  DRUM OR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT @ 50' C-C
-  WORK ZONE
-  TEMP. CONCRETE BARRIER
-  TEMP. SIGN POST
-  PAVEMENT MARKING REMOVAL **
-  DIRECTION INDICATOR BARRICADES WITH STEADY BURN MONODIRECTIONAL LIGHTS @ 50' C-C

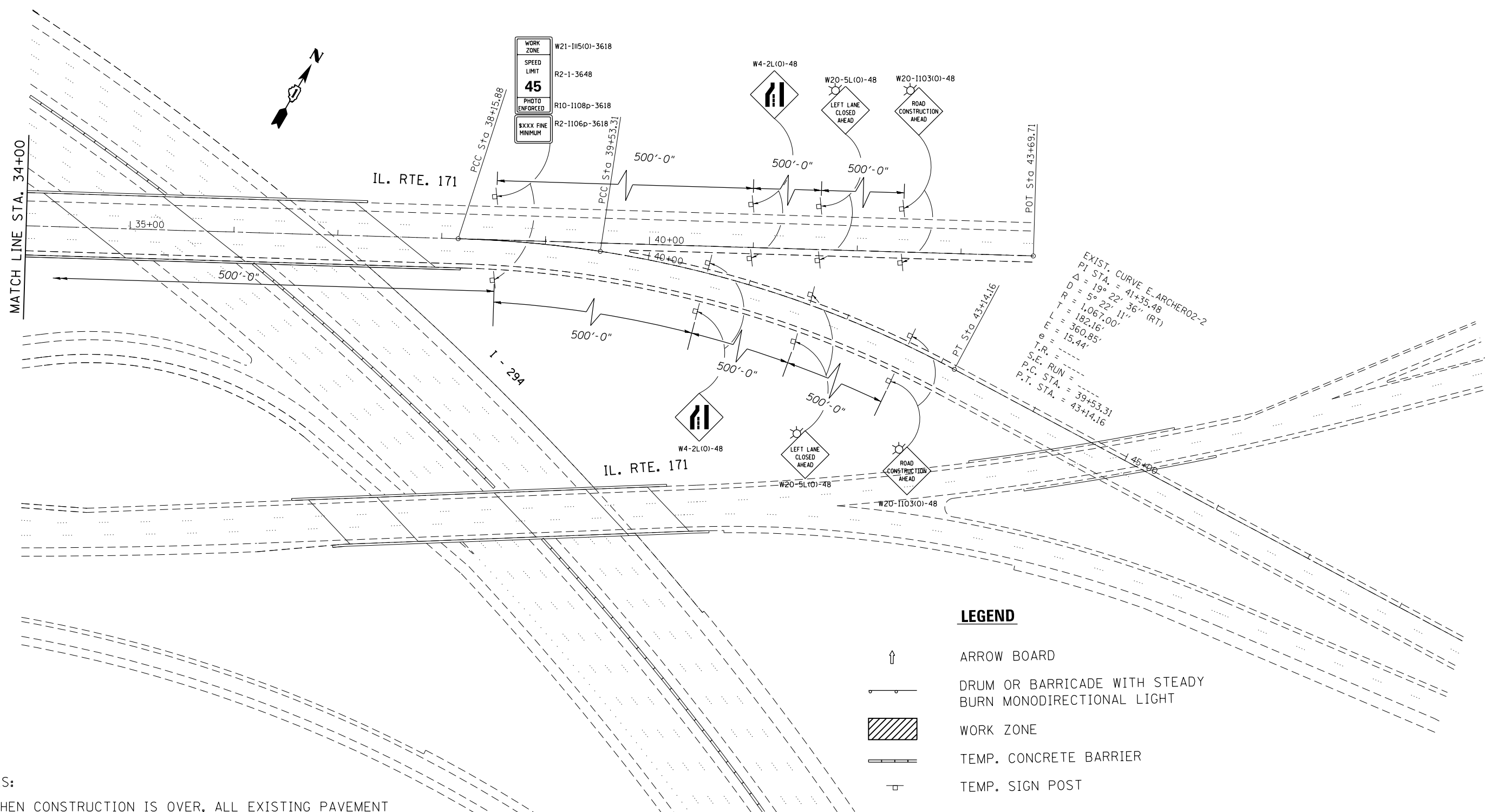
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	PLOT DATE = 12/19/2014	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

STAGING & TRAFFIC CONTROL PLAN
 IL 171 (ARCHER AVE) OVER US 45 SB RAMP TO I-294: SN 016-0209








SCALE: 1"=50' SHEET 2 OF 3 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	2014-0741	COOK	18	5
*1) FAU 3565		2) FAU 3593	CONTRACT NO. 62A17	
ILLINOIS FED. AID PROJECT				



EXIST. CURVE E-ARCHER02-2
 PI STA. = 41+35.48
 $\Delta = 19^\circ 22' 36''$ (RT)
 $D = 5^\circ 22' 11''$
 $R = 1,067.00'$
 $T = 182.16'$
 $L = 360.85'$
 $e = 15.44'$
 T.R. = ----
 S.E. RUN = ----
 P.C. STA. = 39+53.31
 P.T. STA. = 43+14.16

LEGEND

-  ARROW BOARD
-  DRUM OR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
-  WORK ZONE
-  TEMP. CONCRETE BARRIER
-  TEMP. SIGN POST
-  PAVEMENT MARKING REMOVAL **
-  DIRECTION INDICATOR BARRICADES WITH STEADY BURN MONODIRECTIONAL LIGHTS @ 50' C-C

NOTES:
 ** WHEN CONSTRUCTION IS OVER, ALL EXISTING PAVEMENT MARKING REMOVED SHALL BE REPLACED WITH THERMOPLASTIC PAVEMENT MARKING (OVER HMA PAVEMENT) AND POLYUREA PAVEMENT MARKING TYPE I (OVER PCC PAVEMENT).

FILE NAME =	USER NAME = razeqem	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGING & TRAFFIC CONTROL PLAN			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	Plot Scale = 100.0000' / in.	DRAWN -	REVISED -		IL 171 (ARCHER AVE) OVER US 45 SB RAMP TO I-294: SN 016-0209			*	2014-0741	COOK	18	6
	PLOT DATE = 12/18/2014	CHECKED -	REVISED -		SCALE: 1"=50'	SHEET 3 OF 3 SHEETS	STA. TO STA.	1) FAU 3565 2) FAU 3593 CONTRACT NO. 62A17				
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

GENERAL NOTES

All structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Cost of removal and re-installation of all members necessary to complete the work as detailed on the plans and as specified in the Special Provisions shall be included with Furnishing and Erecting Structural Steel.

The Inorganic Zinc Rich Primer / Acrylic / Acrylic Paint System shall be used for shop and field painting of new structural steel except where otherwise noted. The color of the final finish coat for all interior steel surfaces shall be gray, Munsell No. 5B 7/1. The color of the final finish coat for the exterior and bottom flange of the fascia beams shall be Reddish Brown, Munsell No. 2.5YR 3/4.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

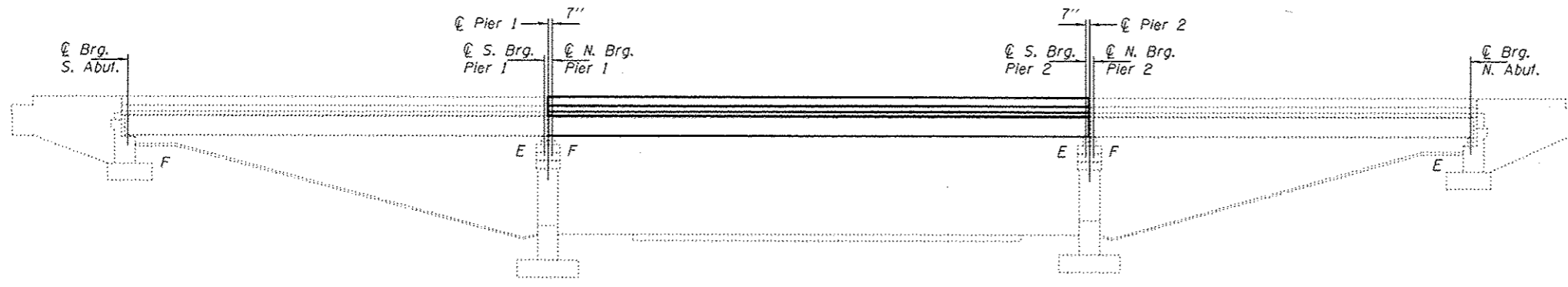
Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the GBSP "Cleaning and Painting Contact Surface Areas of Existing Steel Structures".

Fasteners shall be high strength bolts. Bolts 7/8"φ, open holes 15/16"φ, unless otherwise noted.

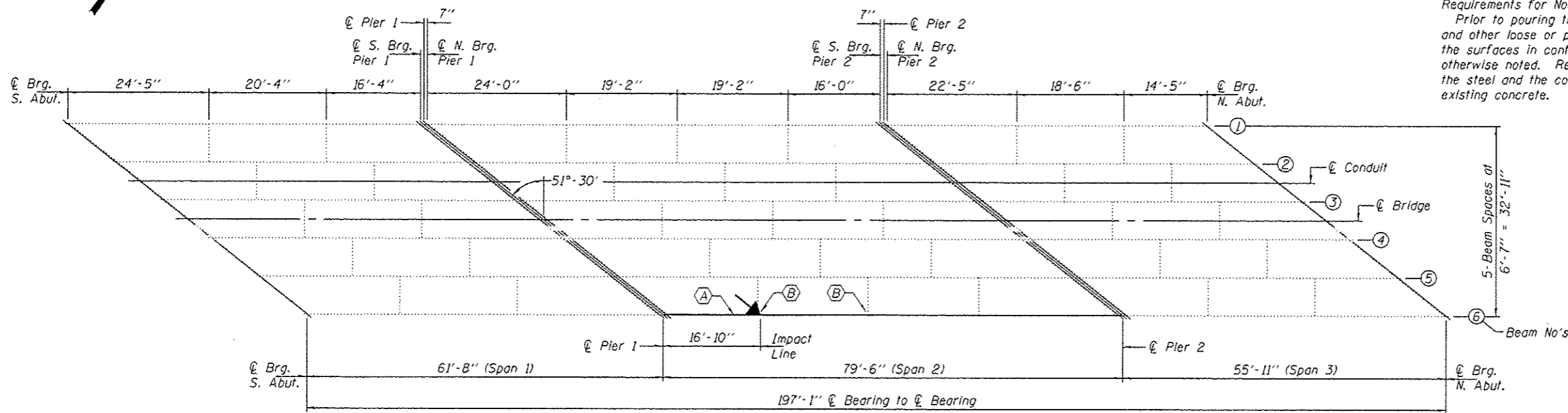
Diaphragm connection holes shall be 15/16"φ for 3/4"φ bolts. Two hardened washers shall be required at diaphragm connections.

Load carrying components designated "NTR" shall conform to the Supplemental Requirements for Notch Toughness, Zone 2.

Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.



ELEVATION



FRAMING PLAN

Impact Line →

- (A) - Remove & Replace Existing Beam
- (B) - Remove & Replace Bottom Clip Angles

TOTAL BILL OF MATERIAL

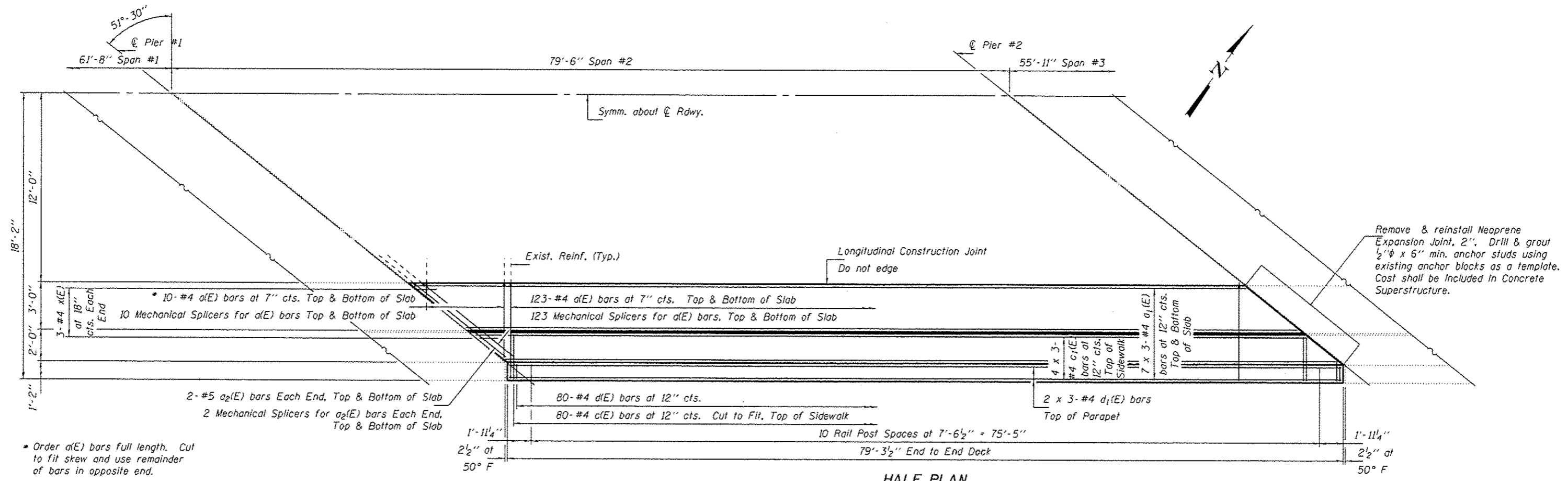
ITEM	UNIT	QUANTITY
Structural Steel Removal	Pound	16,200
Furnishing & Erecting Structural Steel	Pound	16,200
Stud Shear Connectors	Each	459
Concrete Superstructure	Cu. Yd.	25.2
Concrete Removal	Cu. Yd.	27.0
Reinforcement Bars, Epoxy Coated	Pound	1910
* Protective Coat	Sq. Yd.	45.7
Mechanical Splicers	Each	270

* On new concrete only.

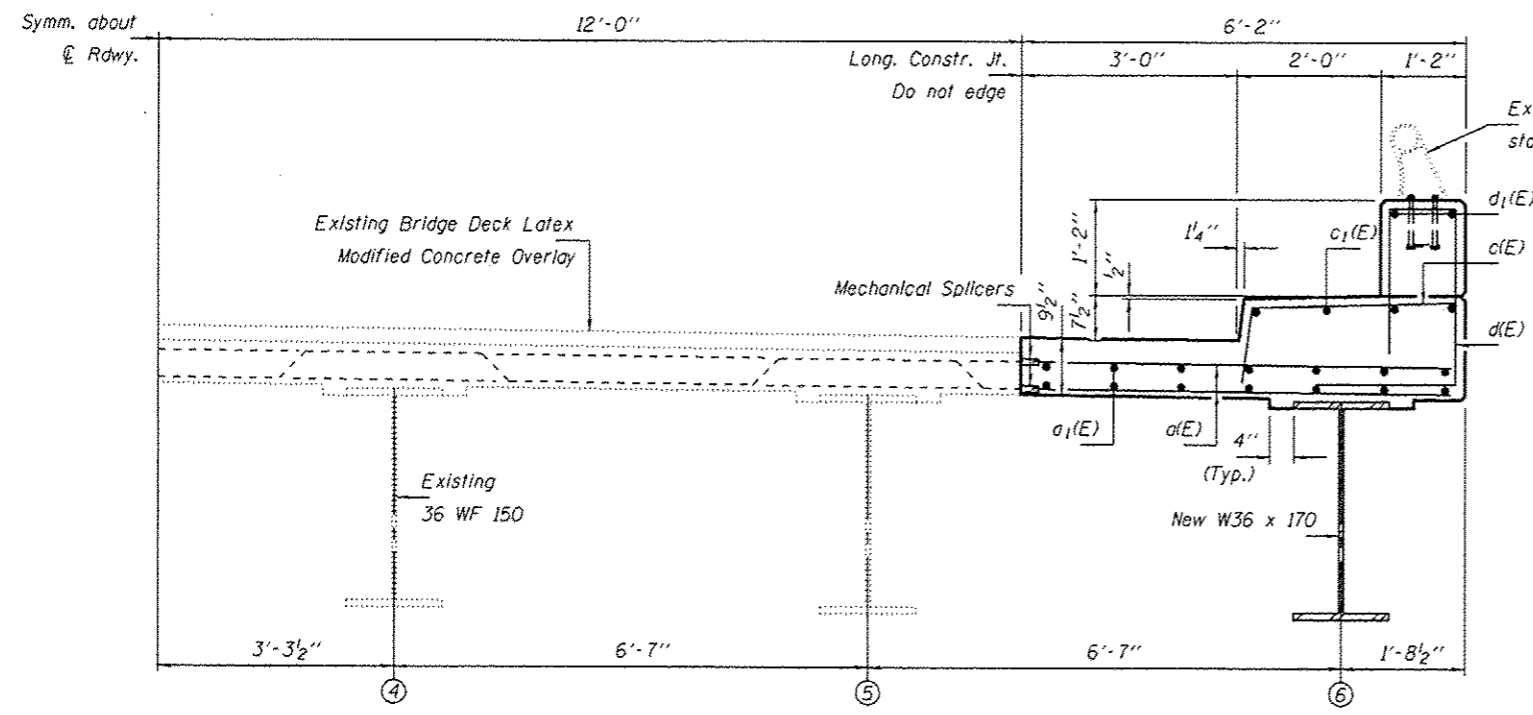


Expires: November 30, 2016

DESIGNED - <i>Tom F. Day</i>	EXAMINED - <i>Tom F. Day</i>	DATE - JANUARY 20, 2015	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL PLAN & ELEVATION ILLINOIS ROUTE 171 - ARCHER AVENUE (WB) OVER RAMP A SN 016-0209		F.A.I. RTE. 94	SECTION 2014-0741	COUNTY COOK	TOTAL SHEETS 18	SHEET NO. 7		
CHECKED - <i>J. Schneller</i>	PASSED - <i>Tom F. Day</i>	REVISED		SHEET NO. 1 OF 6 SHEETS		CONTRACT NO. 62A17		ILLINOIS FED. AID PROJECT				
DRAWN - <i>J. Schneller</i>		REVISED										
CHECKED - <i>TLC</i>		REVISED										

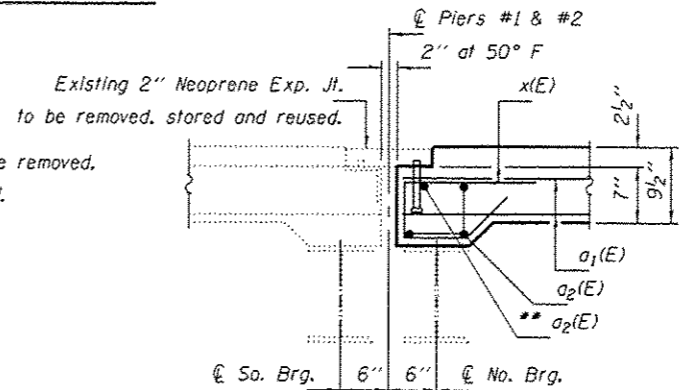
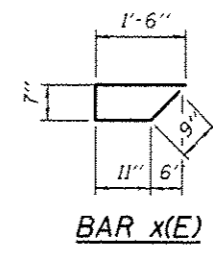
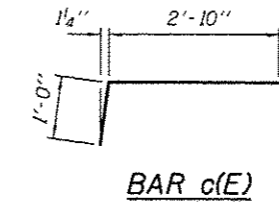
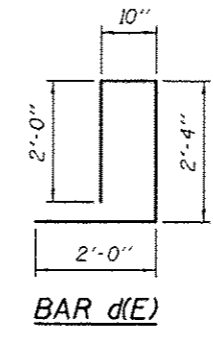


HALF PLAN



HALF CROSS SECTION
(Looking North)

BAR LAP
#4 bars - 2'-0"



SECTION THRU PIER
(Pier #1 shown, Pier #2 similar by rotation)

** Place a2(E) bars in back of Anchor Bolts as shown if required to maintain min. cl. Anchor Bolt should be tied to a2(E) bars.

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a1(E)	133	#4	5'-11"	—
a2(E)	42	#4	27'-9"	—
a2(E)	8	#5	7'-0"	—
c(E)	80	#4	3'-10"	┌
c1(E)	12	#4	27'-9"	—
d(E)	80	#4	7'-2"	└
d1(E)	6	#4	27'-9"	—
x(E)	6	#4	3'-9"	□
Concrete Removal			Cu. Yd.	27.0
Concrete Superstructure			Cu. Yd.	25.2
Reinforcement Bars, Epoxy Coated			Lbs.	1910

Bars indicated thus 7 x 3-#4 etc. Indicates 7 line of bars with 3 lengths per line.

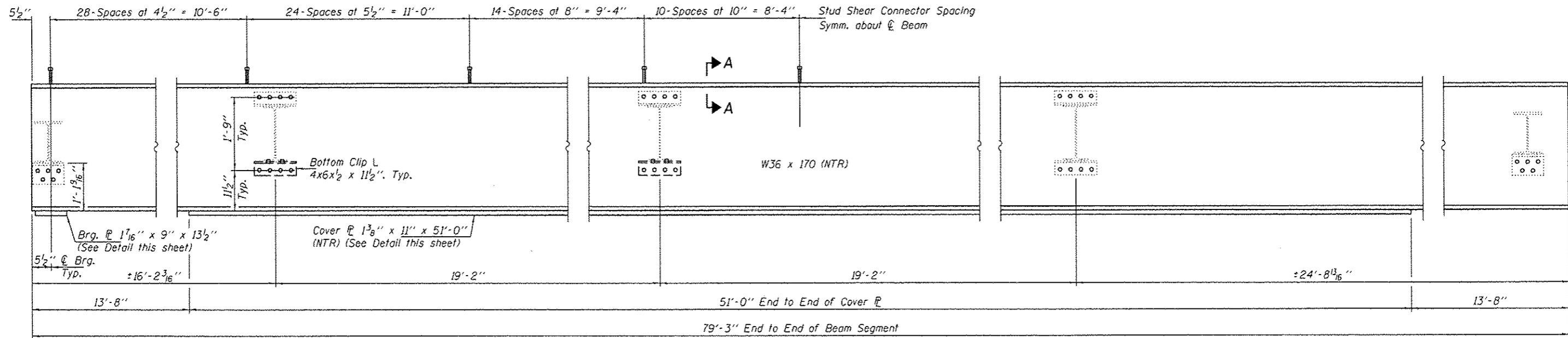
DESIGNED - GGE	EXAMINED - <i>Timothy A. Ballew</i>	DATE - JANUARY 20, 2015
CHECKED - TLC	ACTING ENGINEER OF STRUCTURAL SERVICES	
DRAWN - J. Schneller	PASSED - <i>J. Carl Perry</i>	REVISED
CHECKED - GGE, TLC	ACTING ENGINEER OF BRIDGES AND STRUCTURES	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

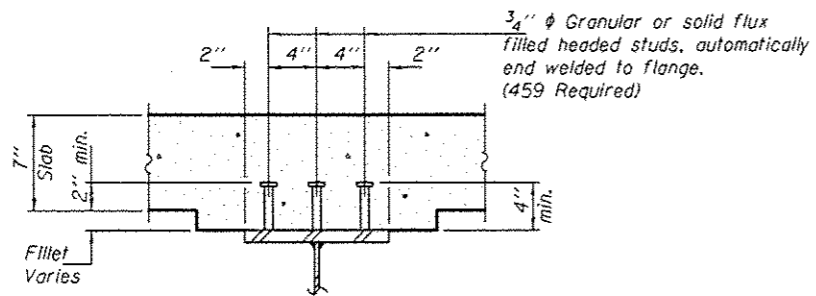
REPAIR DETAILS
SN 016-0209

SHEET NO. 2 OF 6 SHEETS

F.A.I. RTE. 94	SECTION 2014-0741	COUNTY COOK	TOTAL SHEETS 18	SHEET NO. 8
CONTRACT NO. 62A17			[ILLINOIS] FED. AID PROJECT	

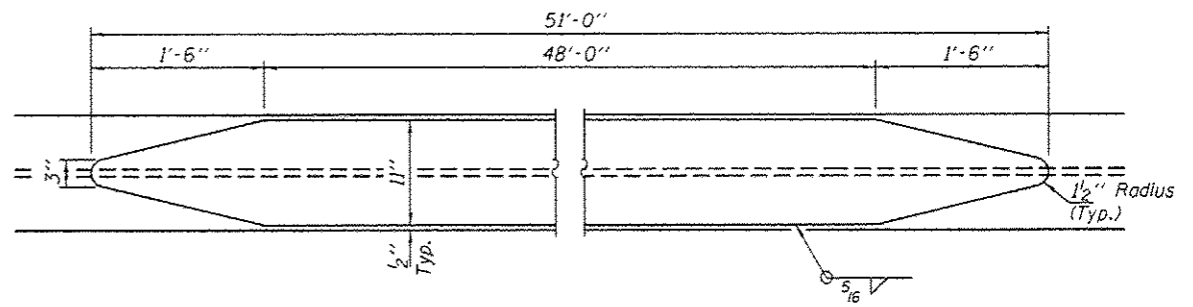


PARTIAL ELEVATION - BEAM 6

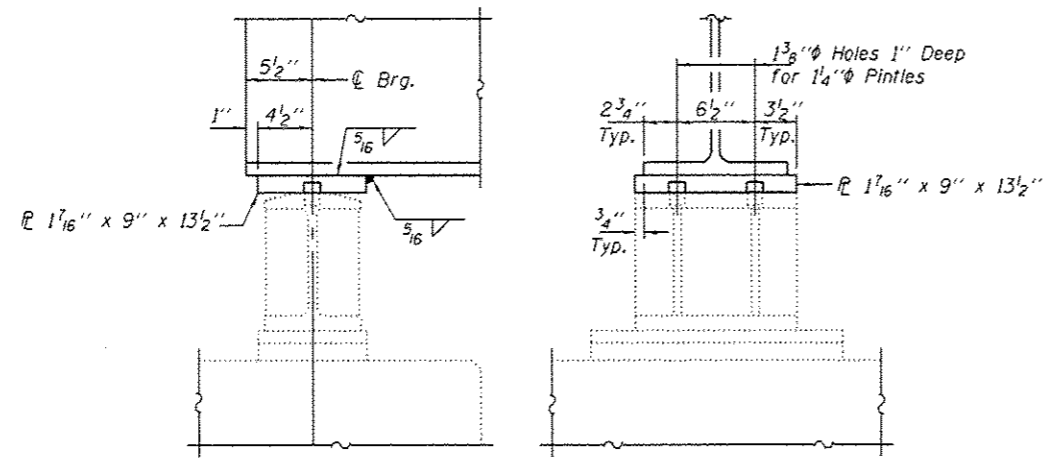


SECTION A-A

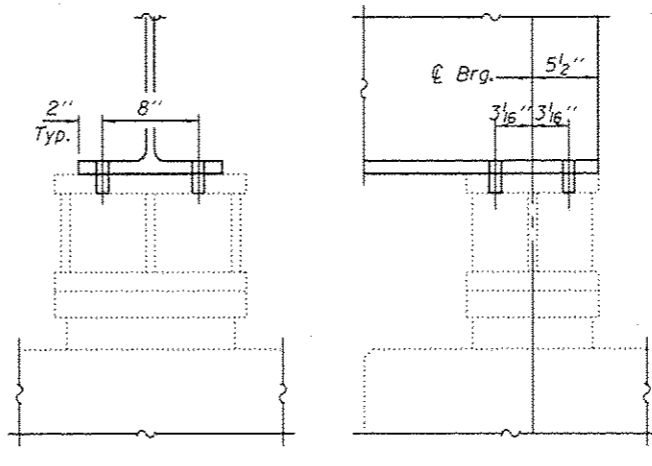
BOLT LEGEND
 ○ Field Drill holes in new steel using holes in existing steel as template.



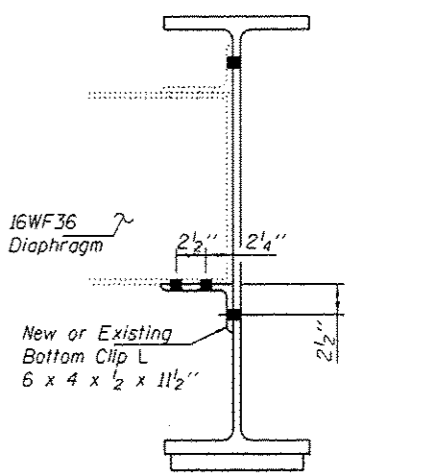
COVER PLATE DETAILS
 1 3/8" x 11" x 51'-0" (NTR) (1-Req'd)



FIXED BEARING AT PIER 1

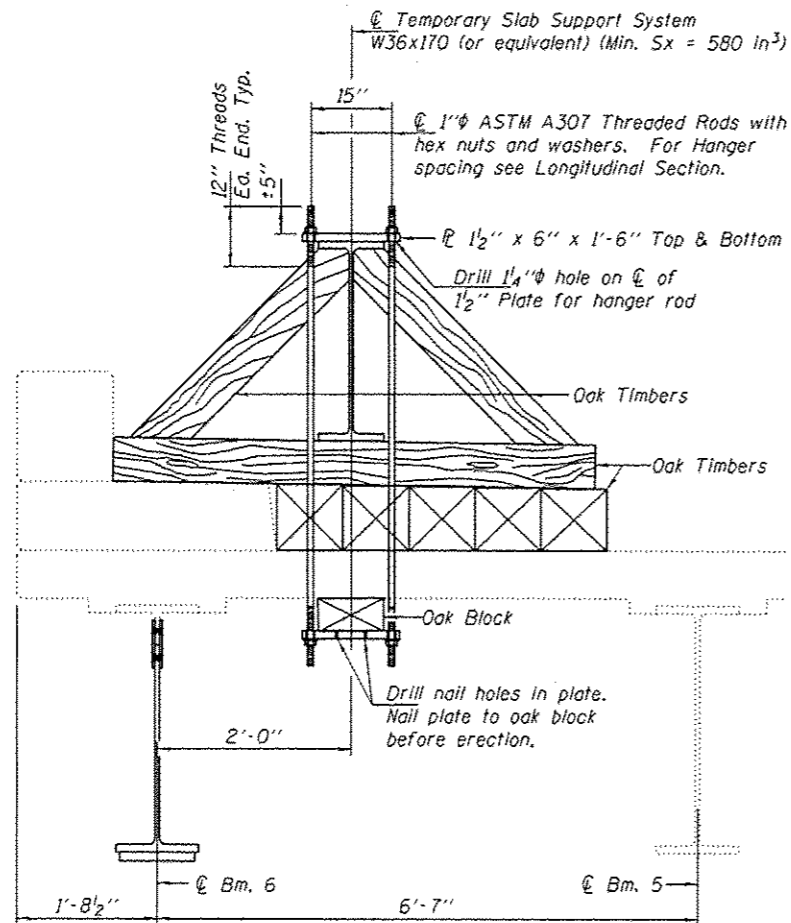


EXPANSION BEARING AT PIER 2

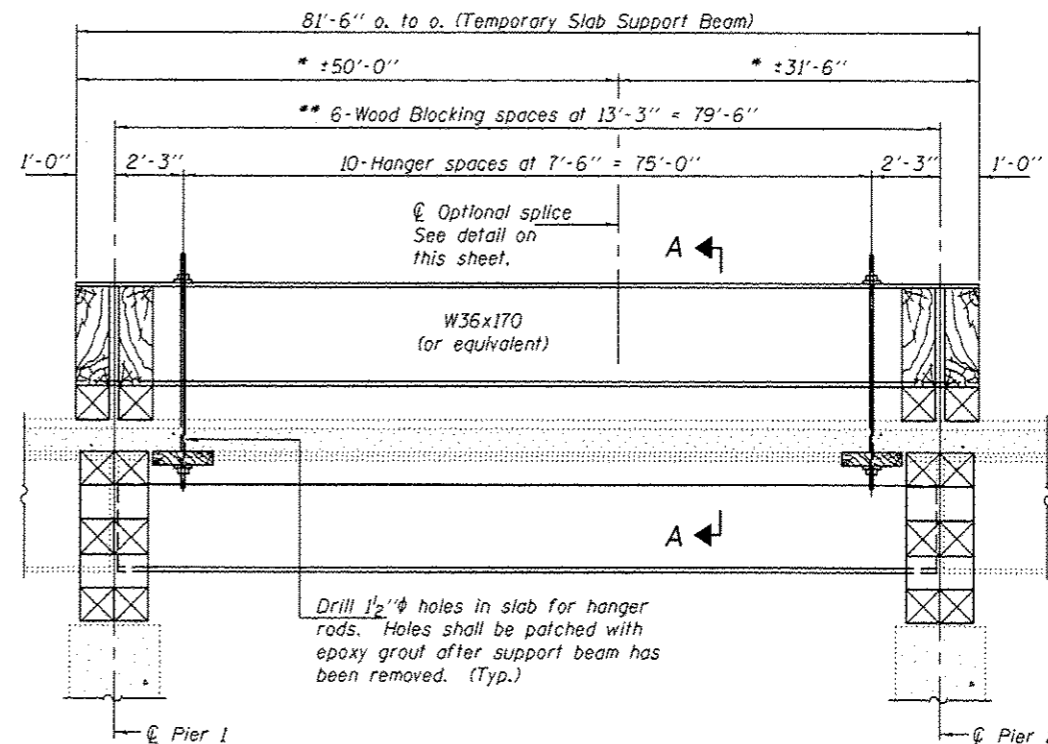


TYPICAL SECTION AT INTERIOR DIAPHRAGMS

DESIGNED - GGE	EXAMINED - <i>Timothy A. Daulton</i> ACTING ENGINEER OF STRUCTURAL SERVICES	DATE - JANUARY 20, 2015	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	NEW BEAM DETAILS SN 016-0209	F.A.I. RTE. 94	SECTION 2014-0741	COUNTY COOK	TOTAL SHEETS 18	SHEET NO. 9
CHECKED - TLC	PASSED - <i>J. Schneller</i> ACTING ENGINEER OF BRIDGES AND STRUCTURES	REVISED			CONTRACT NO. 62A17				
DRAWN - J. Schneller	REVISED	SHEET NO. 3 OF 6 SHEETS			ILLINOIS FED. AID PROJECT				
CHECKED - GGE TLC									

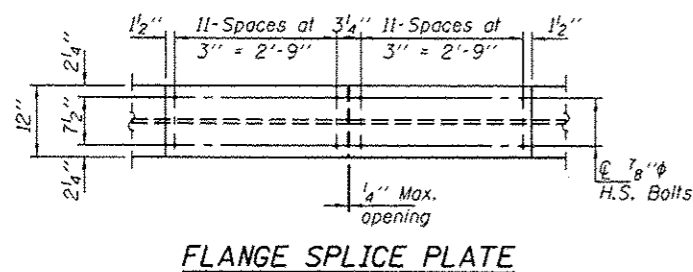


SECTION A-A

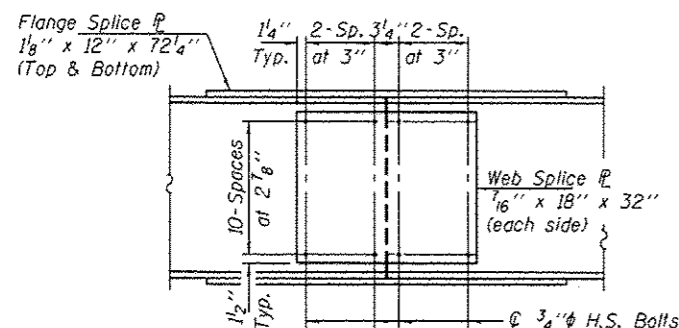


LONGITUDINAL SECTION
SUGGESTED TEMPORARY SLAB SUPPORT SYSTEM

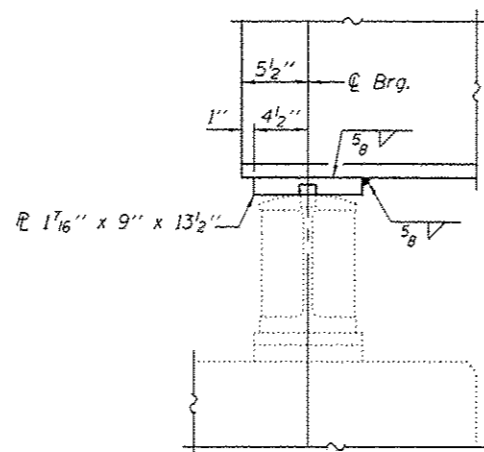
* These dimensions may vary for available beams in stock.
** Wood Blocking between supports to be placed after support beam deflects under its own weight.



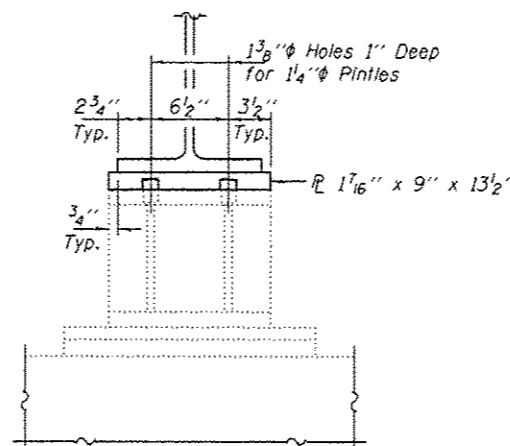
FLANGE SPLICE PLATE



WEB SPLICE PLATE



FIXED BEARING AT PIER 1



EXPANSION BEARING AT PIER 2

DESIGNED - GGE	EXAMINED - <i>Timothy A. Daulton</i>	DATE - JANUARY 20, 2015	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		TEMPORARY SLAB SUPPORT SYSTEM & BEARING DETAILS SN 016-0209		F.A.I. RTE. 94	SECTION 2014-0741	COUNTY COOK	TOTAL SHEETS 18	SHEET NO. 10
CHECKED - TLC	PASSED - <i>Carl P. ...</i>	REVISED	SHEET NO. 4 OF 6 SHEETS		ILLINOIS FED. AID PROJECT		CONTRACT NO. 62A17				

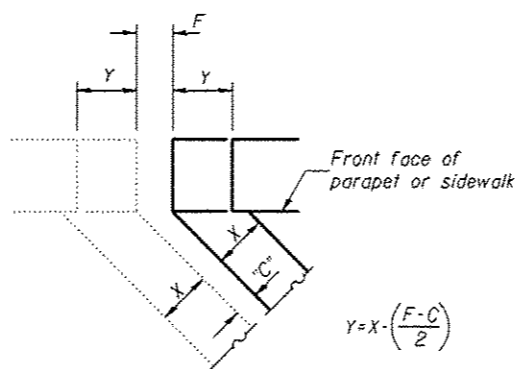
INSTALLATION NOTES

- ① Reinstall continuous seal in roadway, parapet, curb, and sidewalk.
- ② Reinstall anchor blocks as indicated.

NOTE A: Maximum spacing of anchor bolts shall be 12" centers.

SKREW LIMITATIONS

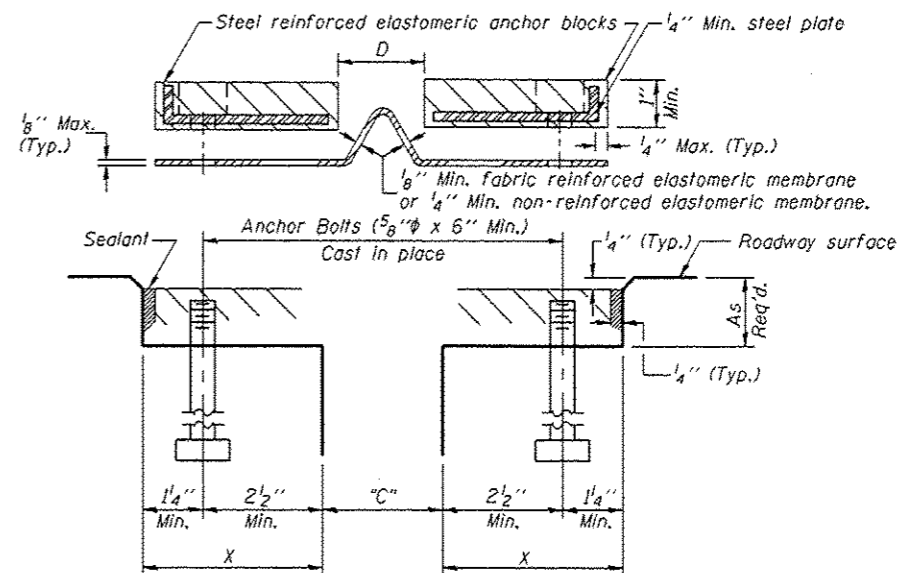
The details of the anchor blocks and the elastomeric membrane in the parapet, as shown, are for up to 50° skews. For skews greater than 50°, the anchor blocks and the elastomeric membrane, installed according to dimension "D", might require modifications to insure a minimum clearance of 1/2" from centerline of anchor studs to edge of parapet opening. The anchor blocks and the elastomeric membrane shall also be installed to the top of the parapet with the anchor studs spaced at ±12" cts.



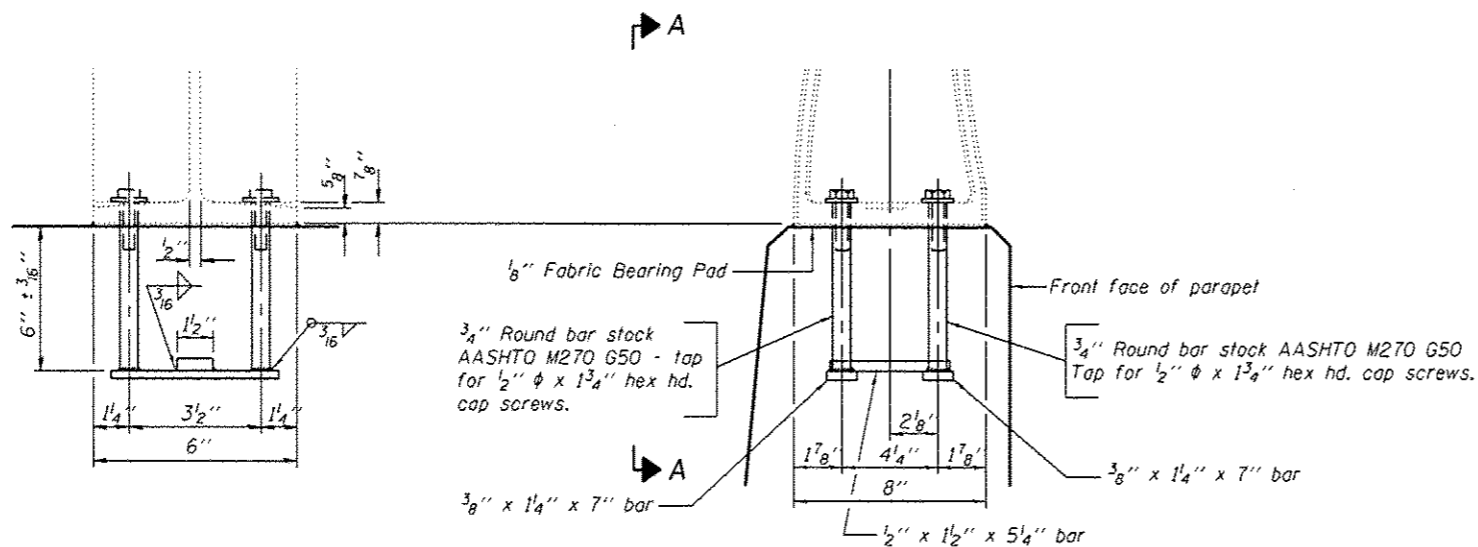
FORMING BLOCKOUT SKETCH

$$Y = X \cdot \left(\frac{F - C}{2} \right)$$

For dimension "F" see sheet #



CROSS SECTION

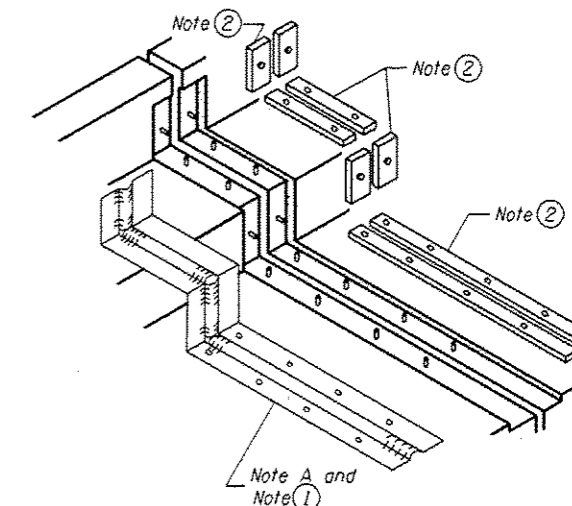


VIEW A-A

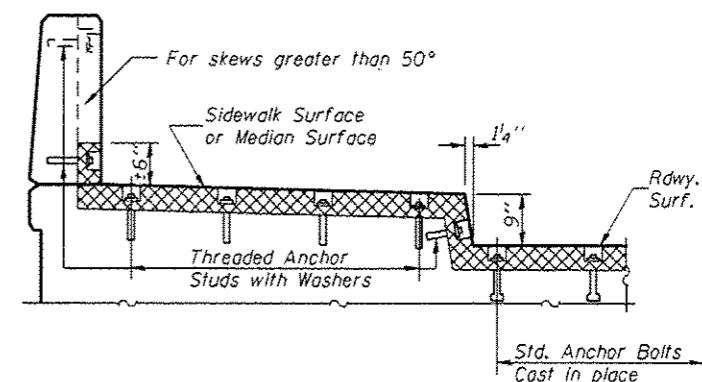
RAIL POST DETAILS

SECTION A-A

Notes:
 All Posts shall be normal to parapet.
 All joints in rail shall be spliced per detail.
 All exposed rail ends shall be capped per detail.
 Provide 1-1/8" and 2-1/16" Aluminum Shims for 25% of the Posts. Rail elements shall be parallel to Grade-high spots will be ground and low spots shimmed.
 See sheet of for rail post spacing.

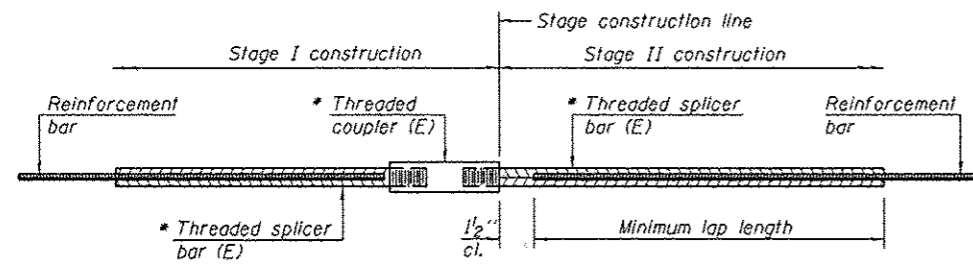


AT SIDEWALK OR MEDIAN



AT SIDEWALK OR MEDIAN TYPICAL END TREATMENTS

DESIGNED - GGE	EXAMINED	DATE - JANUARY 20, 2015	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	RAIL POST AND EXPANSION JOINT DETAILS		F.A.I. RTE. 94	SECTION 2014-0741	COUNTY COOK	TOTAL SHEETS 18	SHEET NO. 11
CHECKED - TLC	PASSED	REVISED		SN 016-0209				CONTRACT NO. 62A17		
DRAWN - J Schneller		REVISED		SHEET NO. 5 OF 6 SHEETS		ILLINOIS FED. AID PROJECT				
CHECKED - GGE TLC										



STANDARD BAR SPLICER ASSEMBLY

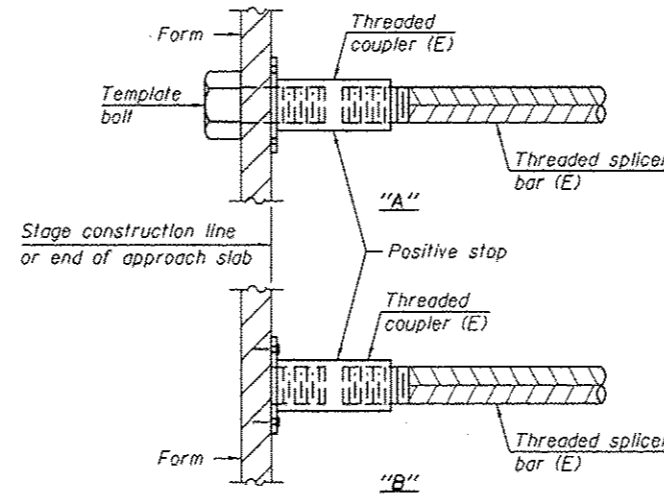
Bar size to be spliced	Minimum Lap Lengths					
	Table 1	Table 2	Table 3	Table 4	Table 5	Table 6
3, 4	1'-5"	1'-11"	2'-1"	2'-4"	2'-7"	2'-11"
5	1'-9"	2'-5"	2'-7"	2'-11"	3'-3"	3'-8"
6	2'-1"	2'-11"	3'-1"	3'-6"	3'-10"	4'-5"
7	2'-9"	3'-10"	4'-2"	4'-8"	5'-2"	5'-10"
8	3'-8"	5'-1"	5'-5"	6'-2"	6'-9"	7'-8"
9	4'-7"	6'-5"	6'-10"	7'-9"	8'-7"	9'-8"

- Table 1: Black bar, 0.8 Class C
- Table 2: Black bar, Top bar lap, 0.8 Class C
- Table 3: Epoxy bar, 0.8 Class C
- Table 4: Epoxy bar, Top bar lap, 0.8 Class C
- Table 5: Epoxy bar, Class C
- Table 6: Epoxy bar, Top bar top, Class C

Threaded splicer bar length = min. lap length + 1/2" + thread length

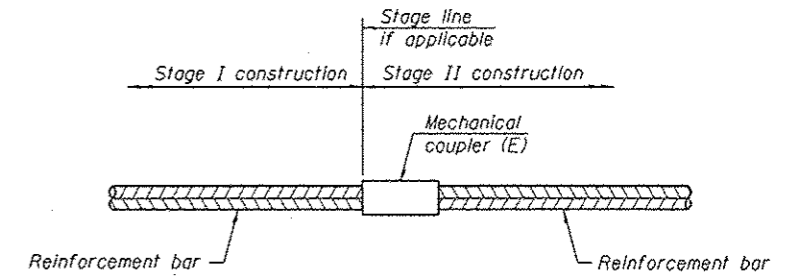
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length



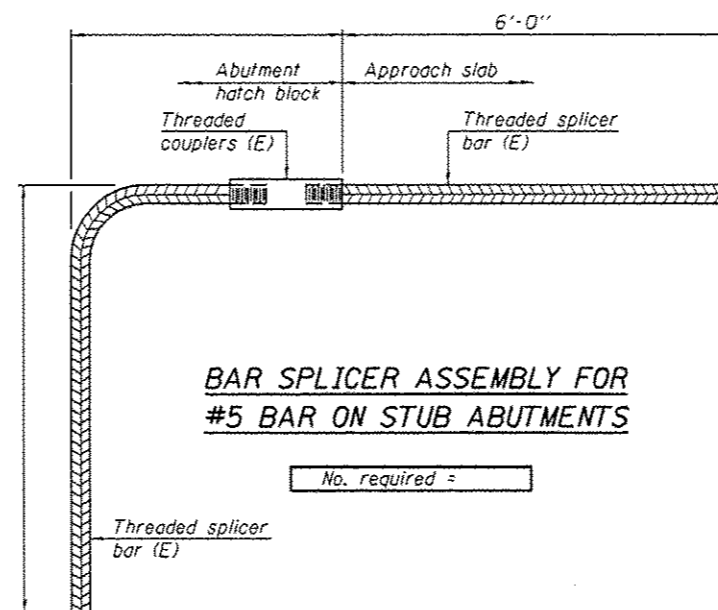
INSTALLATION AND SETTING METHODS

- "A" : Set bar splicer assembly by means of a template bolt.
- "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
- (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required
Slab	#4	270



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

- Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
- All reinforcement shall be lapped and tied to the splicer bars.
- Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 50B of the Standard Specifications.
- See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1 8-31-12

DESIGNED - CGE	EXAMINED	DATE - JANUARY 20, 2015	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS SN 016-0209	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
CHECKED - TLC	PASSED	REVISED			94	2014-0741	COOK	18	12	
DRAWN - J Schneller	ACTING ENGINEER OF BRIDGES AND STRUCTURES	REVISED			CONTRACT NO. 62A17					
CHECKED - CGE TLC	ACTING ENGINEER OF STRUCTURAL SERVICES	REVISED			ILLINOIS FED. AID PROJECT					

NOTES

All structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

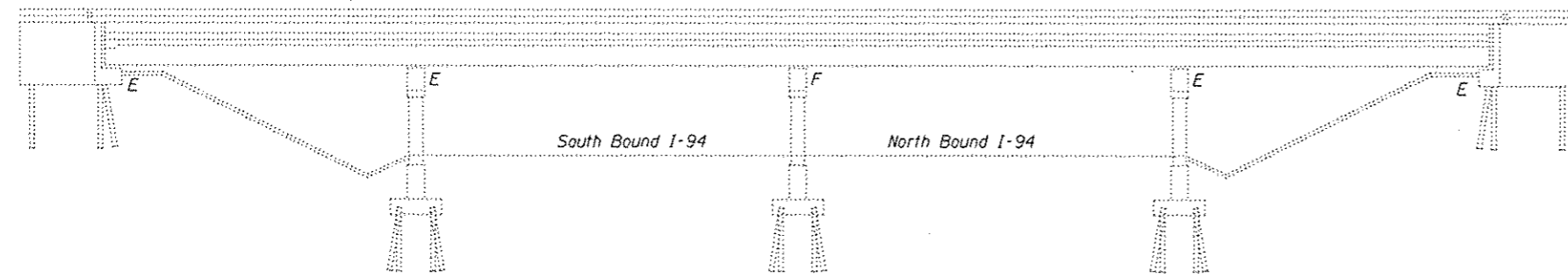
The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the Special Provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures".

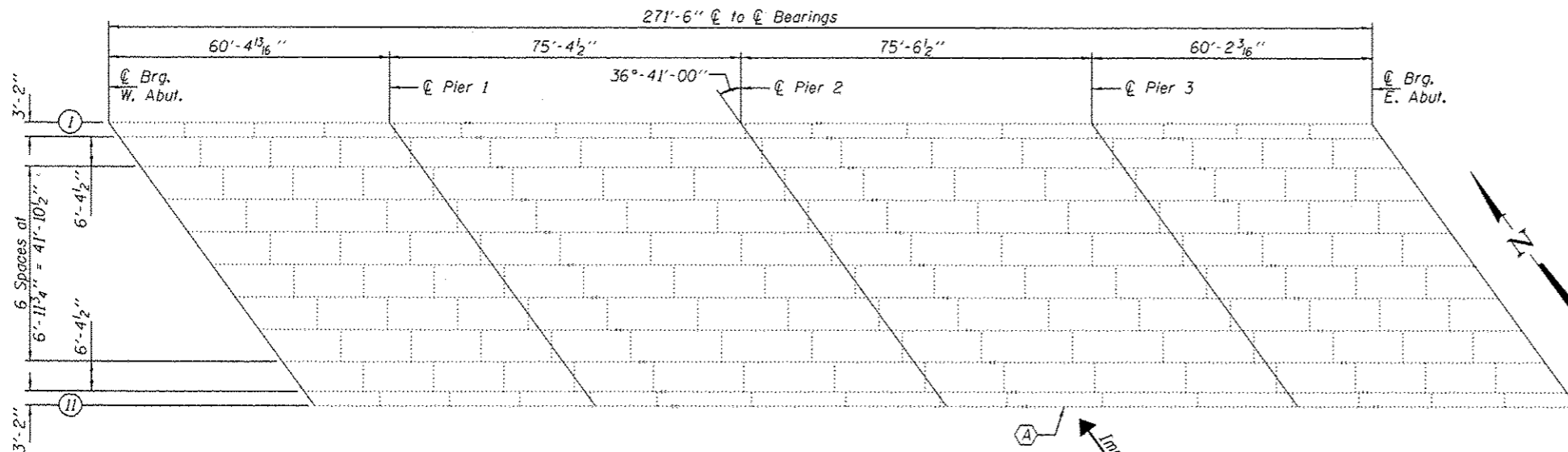
All structural steel shall be shop painted with the inorganic zinc rich primer per AASHTO M300, Type 1. Cost included with Structural Steel Repair.

Cost of removal and/or re-Installation of all members necessary to complete the work as detailed on the plans and as specified in the Special Provisions shall be included in the cost of Structural Steel Repair.

Fasteners shall be high strength bolts. Bolts $\frac{7}{8}$ " ϕ , open holes $\frac{15}{16}$ " ϕ , unless otherwise noted.

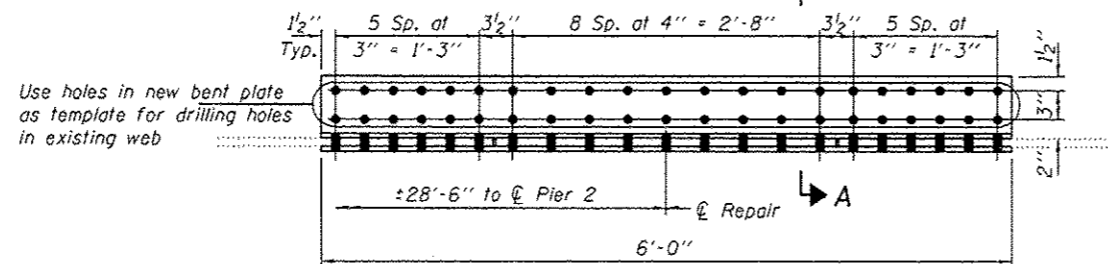


ELEVATION



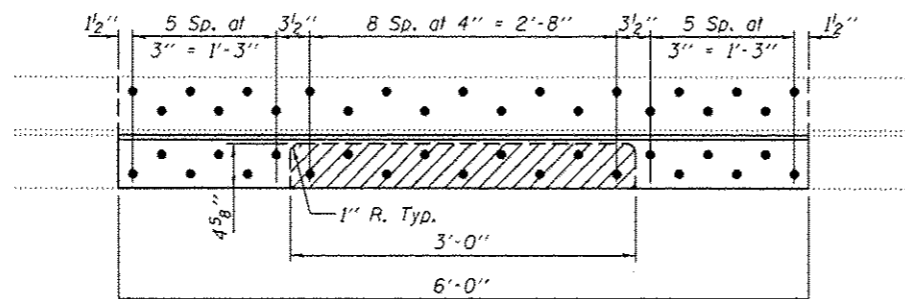
FRAMING PLAN

(A) - Existing Beam to be Straightened & Strengthened.



ELEVATION

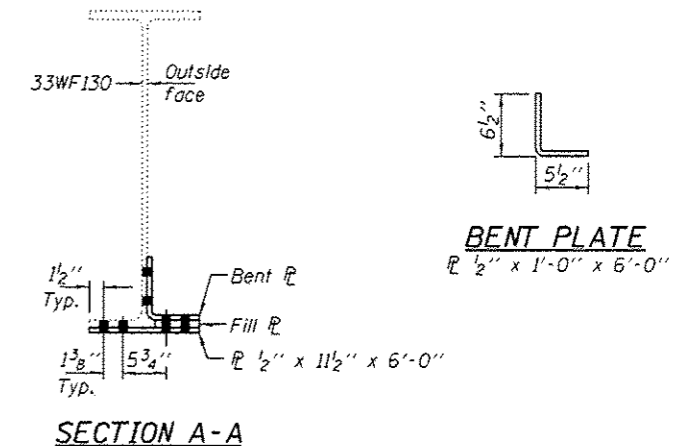
(Looking North)



PLAN

Hatched area indicates removal. Cost included with Structural Steel Repair.

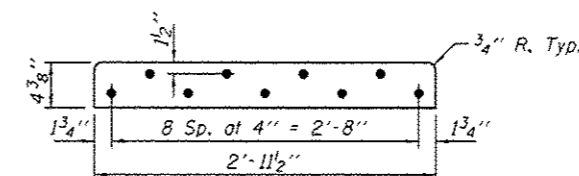
Use holes in new bottom flange plate as template for drilling holes in existing bottom flange



BENT PLATE

$L \frac{1}{2}'' \times 11 \frac{1}{2}'' \times 6'-0''$

SECTION A-A



FILL PLATE

$L \frac{7}{8}'' \times 4 \frac{3}{8}'' \times 2'-11 \frac{1}{2}''$

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Structural Steel Repair	Pound	330
Beam Straightening	L. Sum	1



EXPIRES 11-30-2016

DESIGNED: *John Clark*
 CHECKED: *Adrian T. Holloway*
 DRAWN: *boliva*
 CHECKED: *TLC ATH*

EXAMINED: *Timothy A. ...*
 PASSED: *David Carl Puze*

DATE: JANUARY 20, 2015

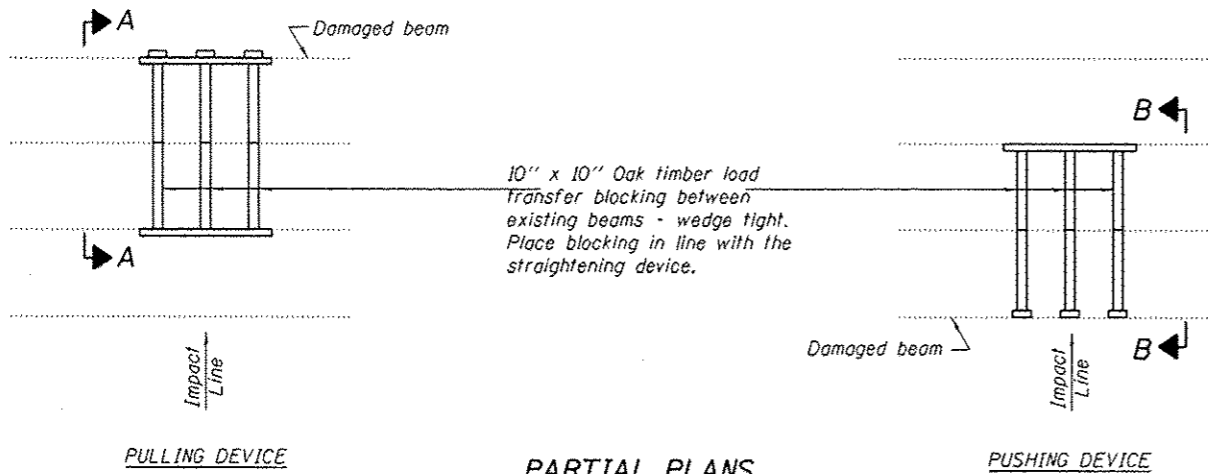
REVISED: _____
 REVISED: _____

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

FLANGE REPAIR DETAILS
 SN 016-1068

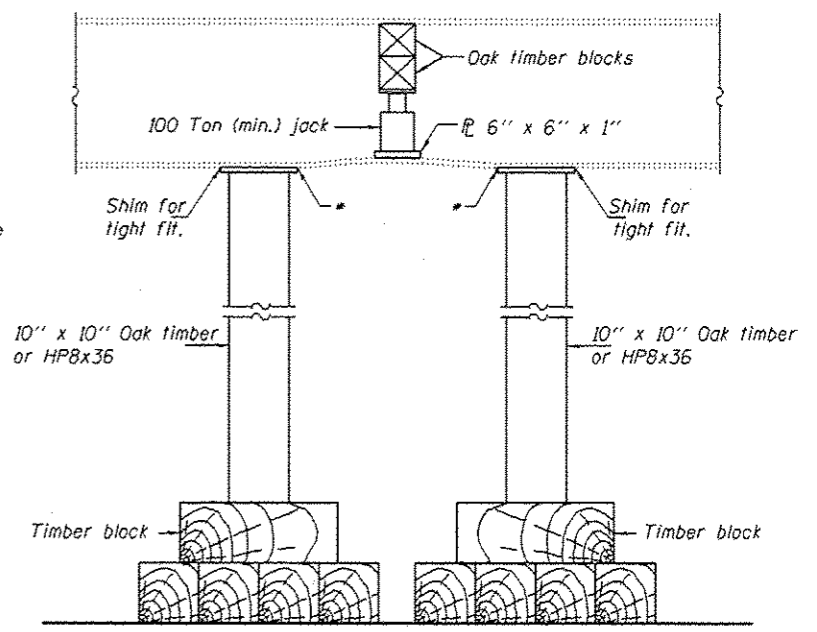
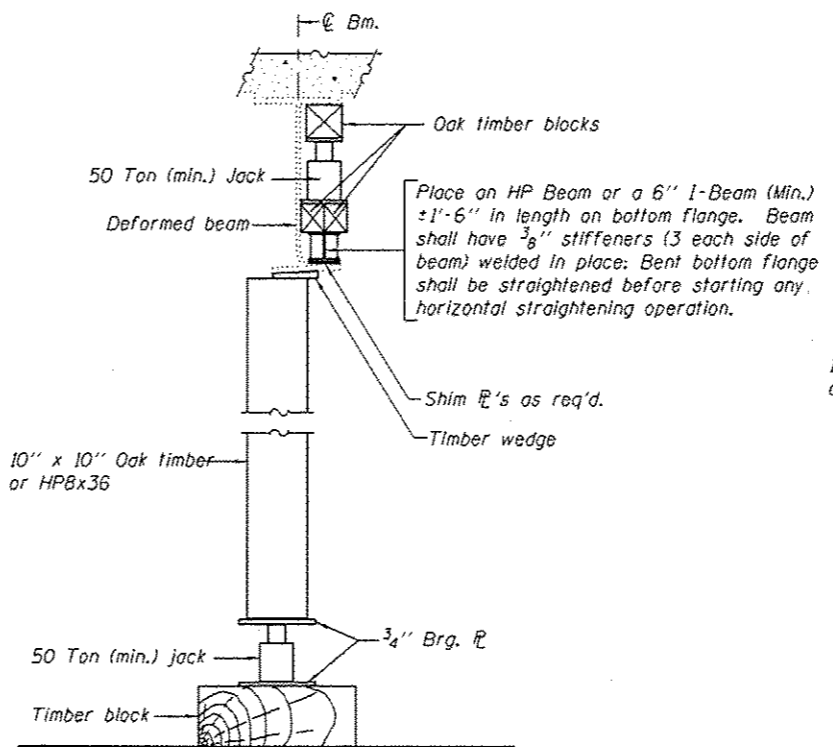
SHEET NO. 1 OF 2 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	2014-0741	COOK	18	13
CONTRACT NO. 62A17				
ILLINOIS FED. AID PROJECT				



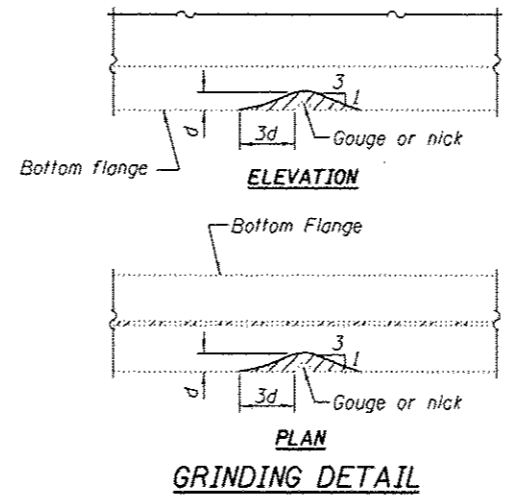
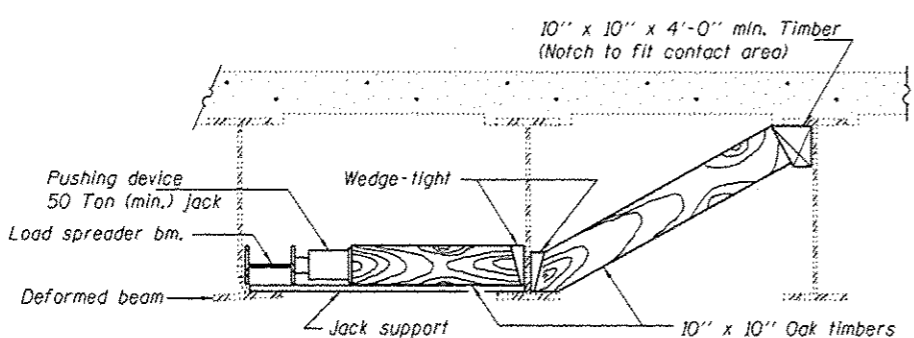
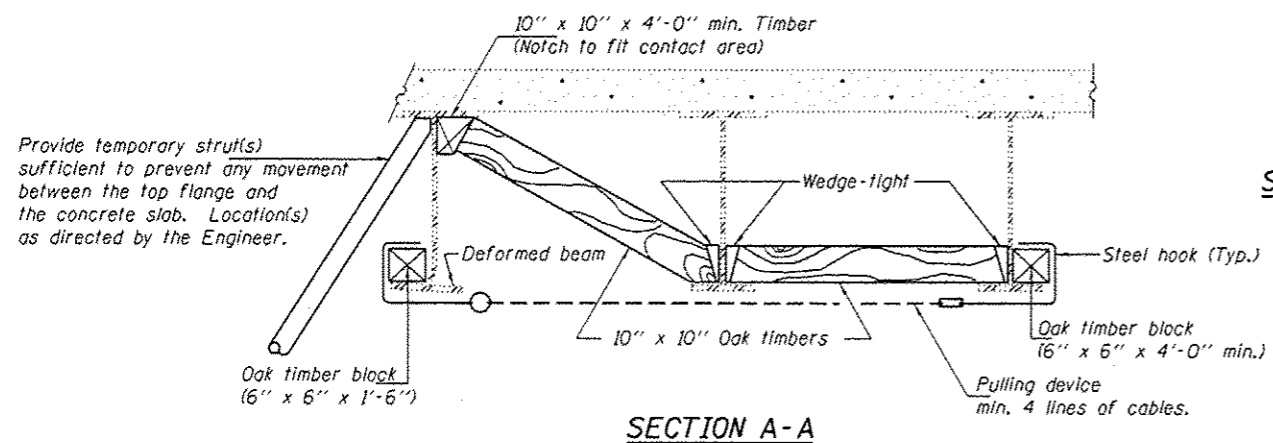
PARTIAL PLANS
SUGGESTED BEAM STRAIGHTENING METHODS

Straightening force shall be maintained on all load transfer blocking during beam straightening.



* Edge of plate shall line up with edge of deformation.

Note:
Braces and jack assembly shall be placed on same side of web.
Bent bottom flange shall be straightened before starting any horizontal straightening operations.



Grind existing nicks, gouges and shallow cracks in the damaged beams as detailed. Ground surfaces shall be inspected for cracks using magnetic particle testing prior to initiating any beam straightening operations. Any cracks that cannot be removed by grinding approximately 1/4'' deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. Ground surfaces shall be spot cleaned and painted with an aluminum epoxy mastic primer followed by a finish coat to match the color of the existing beam. Cost of grinding, testing and spot painting included with Beam Straightening.

EXISTING DEFORMATION TO BE STRAIGHTENED
(Looking East)
(Approximate max. deflections)
Deflected length of beam to be straightened is approximately 6'.

REP-11-M-2005

DESIGNED <i>TLC</i>	EXAMINED	DATE
CHECKED <i>ATH</i>	<i>Timothy A. ...</i>	JANUARY 20, 2015
DRAWN <i>baliva</i>	PASSED	REVISED
CHECKED <i>TLC ATH</i>	<i>Carl ...</i>	REVISED

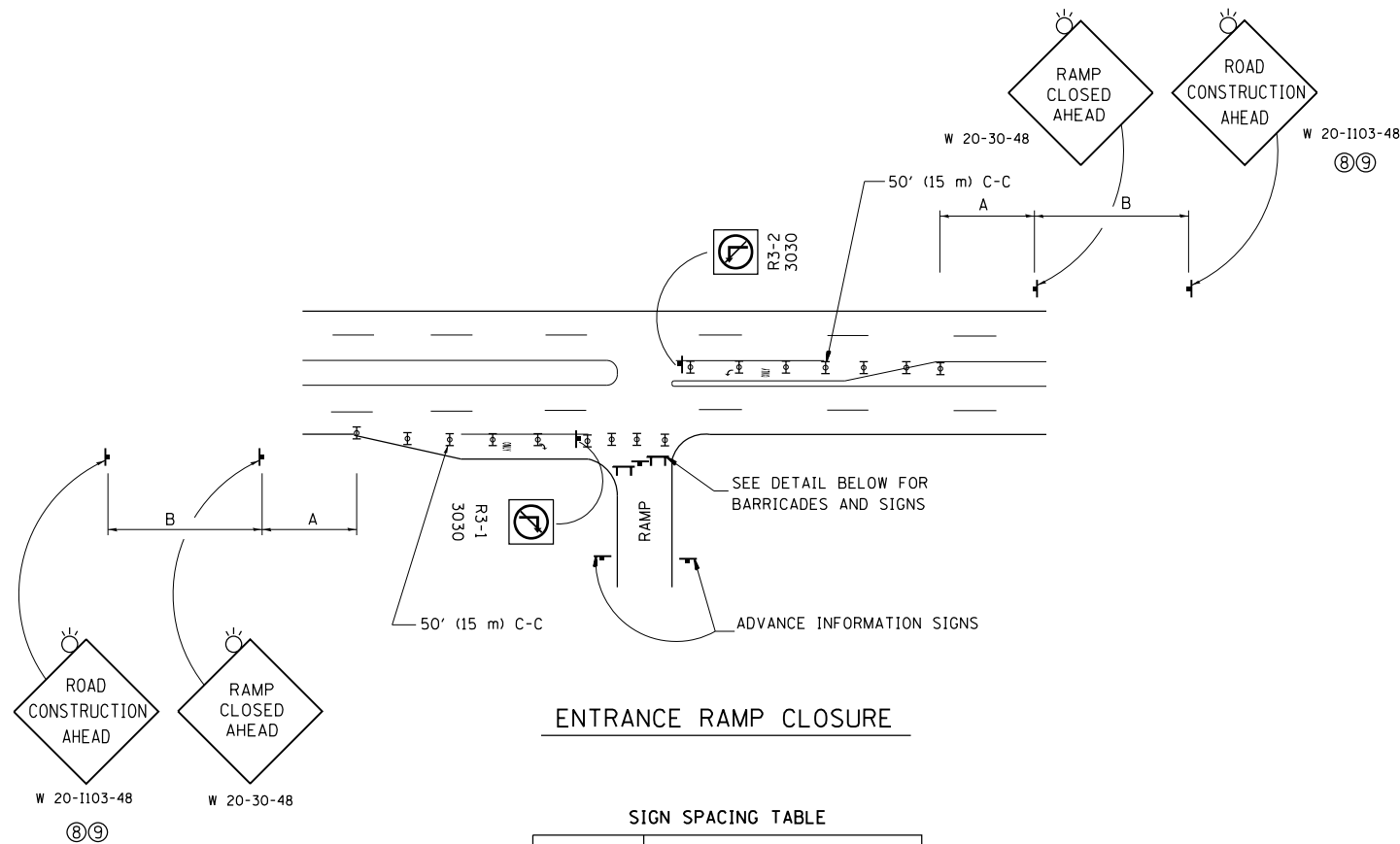
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BEAM STRAIGHTENING DETAILS
SN 016-1068

SHEET NO. 2 OF 2 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	2014-0741	COOK	18	14
CONTRACT NO. 62A17				

ILLINOIS FED. AID PROJECT

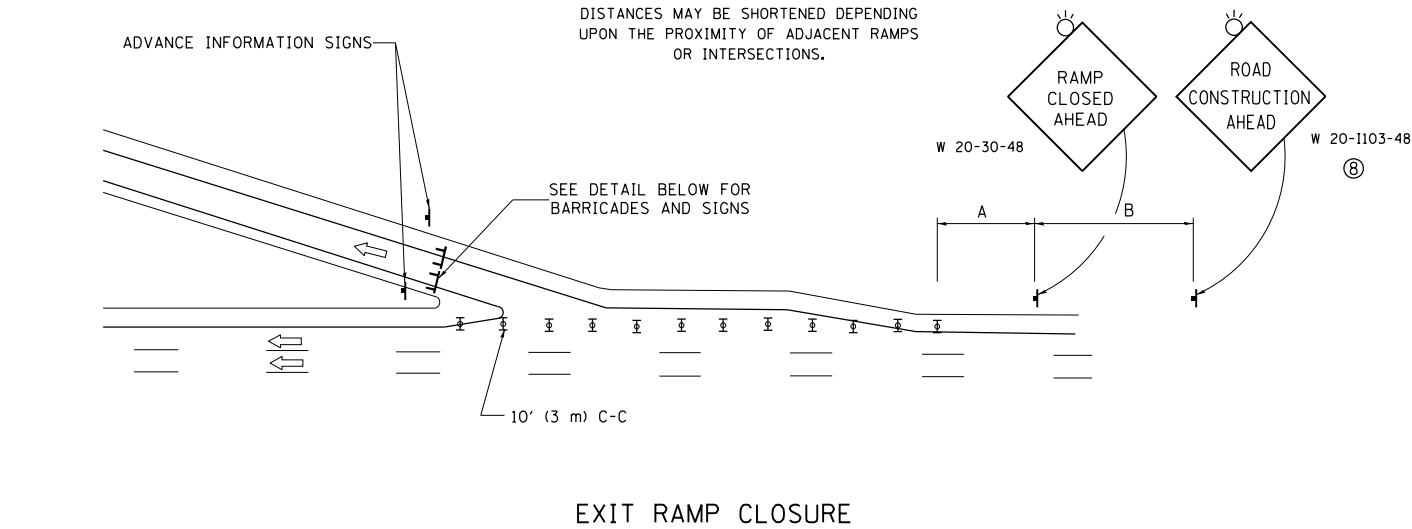


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY <24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

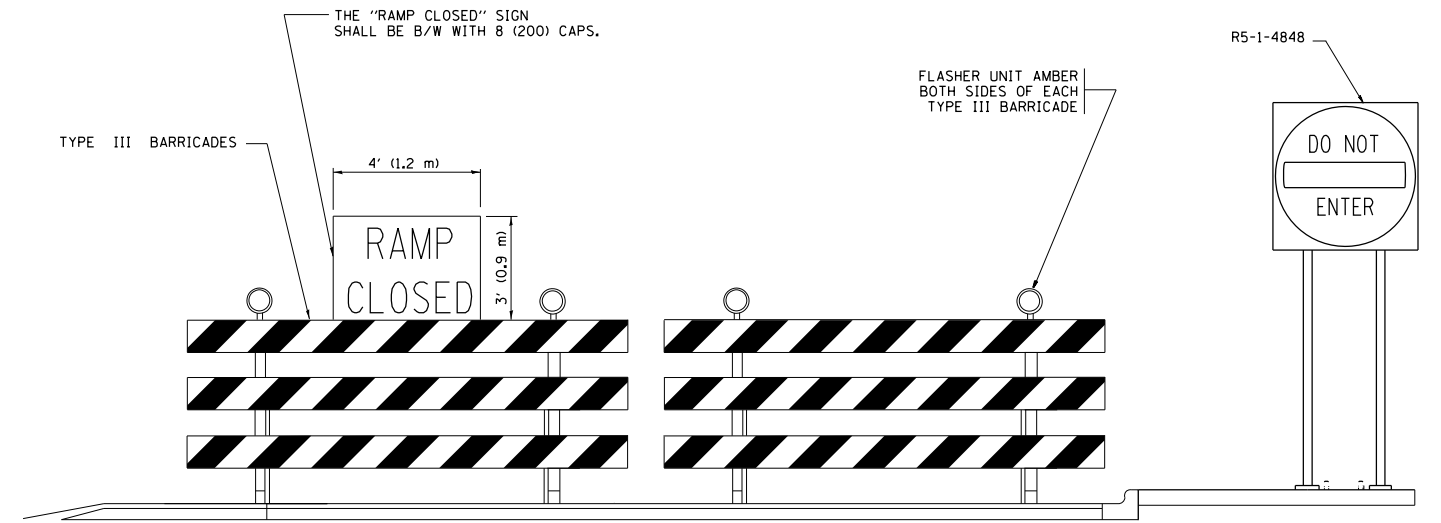
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



EXIT RAMP CLOSURE

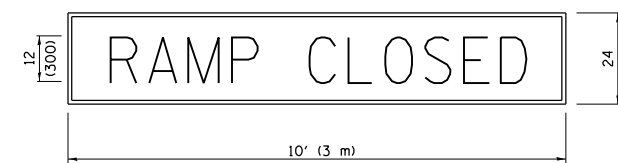
SYMBOLS

- ⊥ TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- ⊥ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



DETAIL FOR REQUIRED BARRICADES & SIGNS

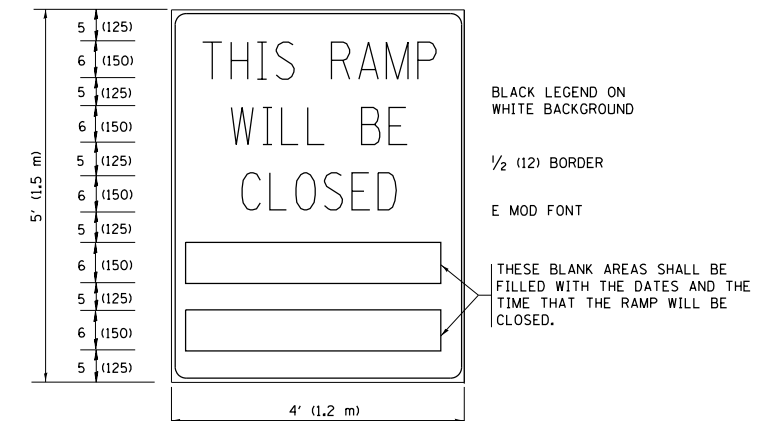
RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY
E MOD FONT
1 (25) BORDER

THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

RAMP CLOSURE ADVANCE INFORMATION SIGN



BLACK LEGEND ON WHITE BACKGROUND
1/2 (12) BORDER
E MOD FONT

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

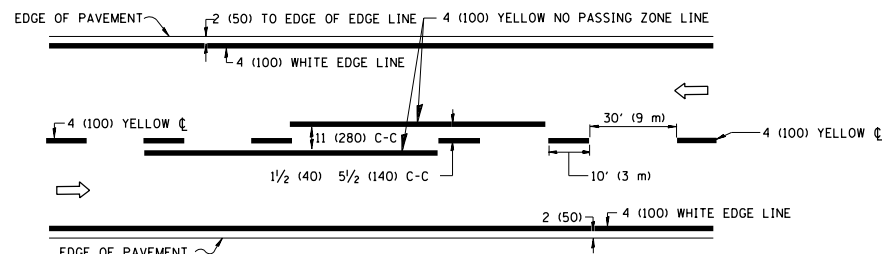
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ei:\pw\work\p\dot\razeqem\d0418085\DI09#15-shr-plan.dgn		DRAWN -	REVISED - S.P.B. 01-07
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - S.P.B. 12-09
	PLOT DATE = 12/18/2014	DATE - 02-83	REVISED - M.D. 06-13

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

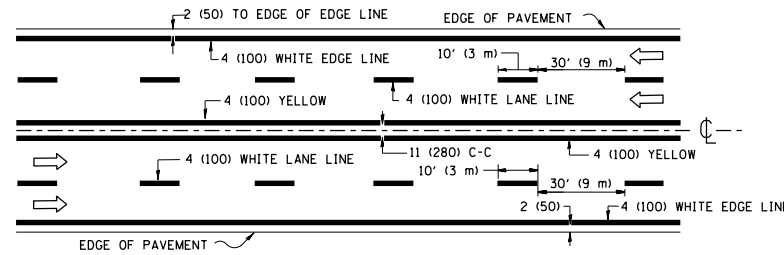
**ENTRANCE AND EXIT RAMP
CLOSURE DETAILS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

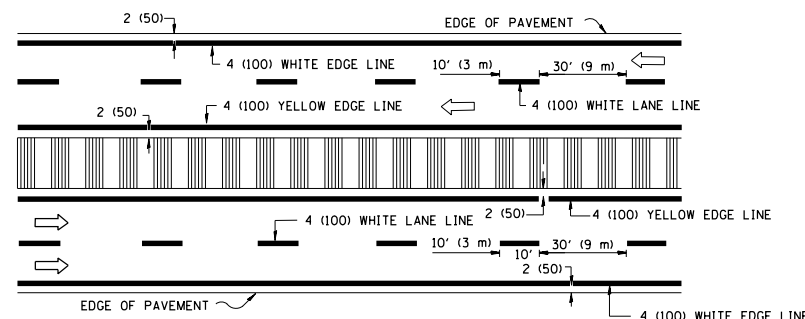
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2014-0741	COOK	18	15
TC-08			CONTRACT NO. 62A17	
IFAU 3565 2IFAU 3593 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY



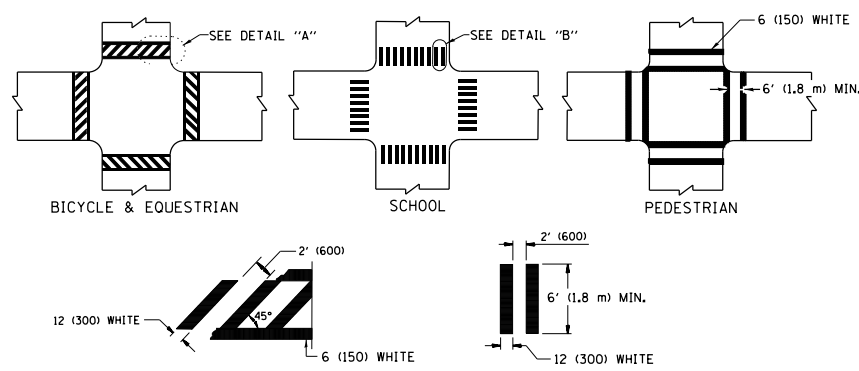
MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

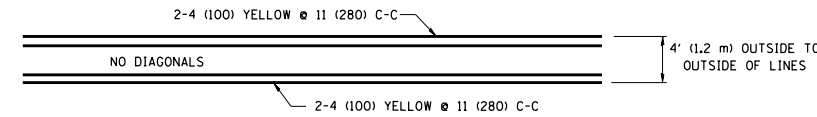


DETAIL "A"

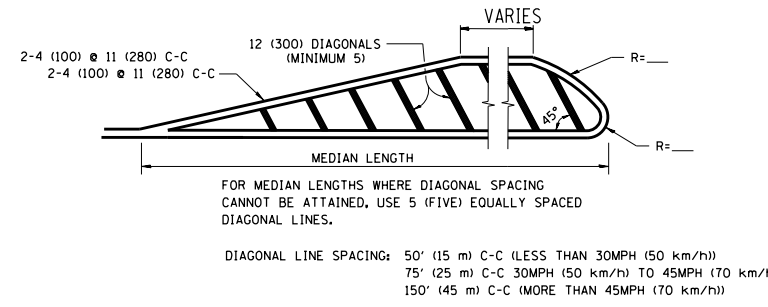
DETAIL "B"

TYPICAL CROSSWALK MARKING

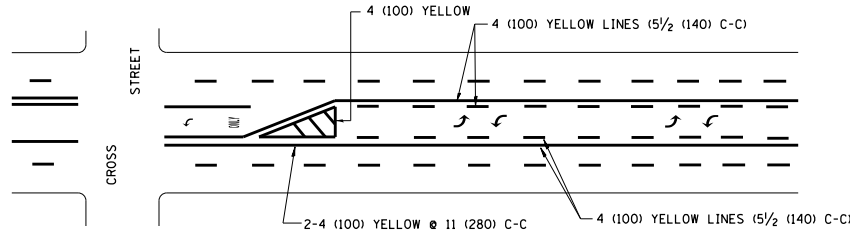
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



4' (1.2 m) WIDE MEDIANS ONLY



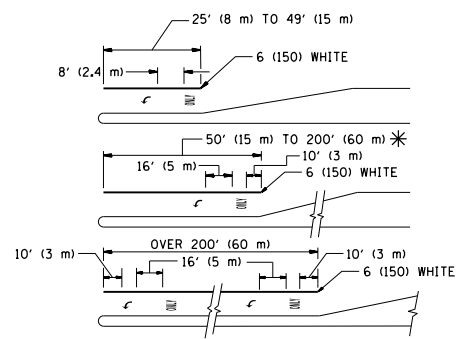
MEDIANS OVER 4' (1.2 m) WIDE



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

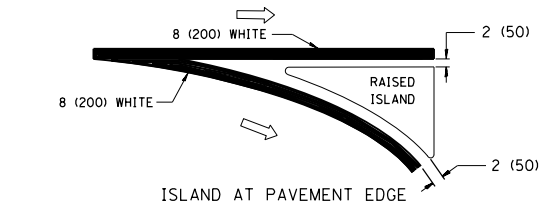
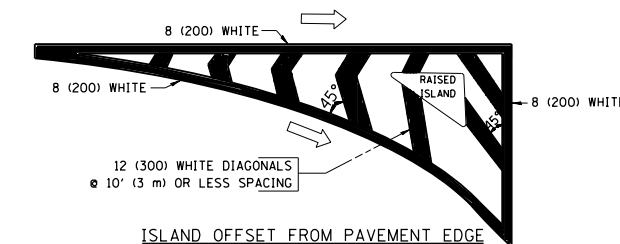


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = razeqem	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
Default	Default	DRAWN -	REVISED - C. JUCIUS 09-09-09
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - C. JUCIUS 07-01-13
	PLOT DATE = 12/18/2014	DATE - 03-19-90	REVISED -

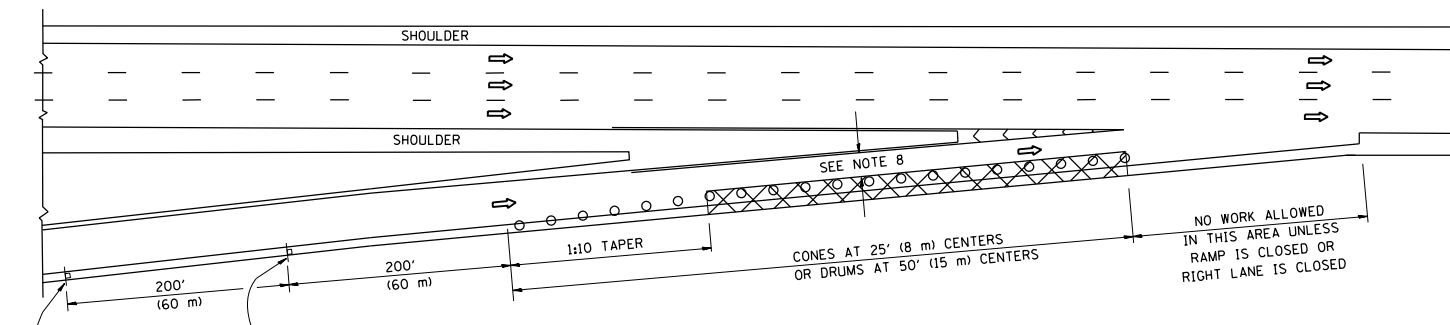
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
TYPICAL PAVEMENT MARKINGS**

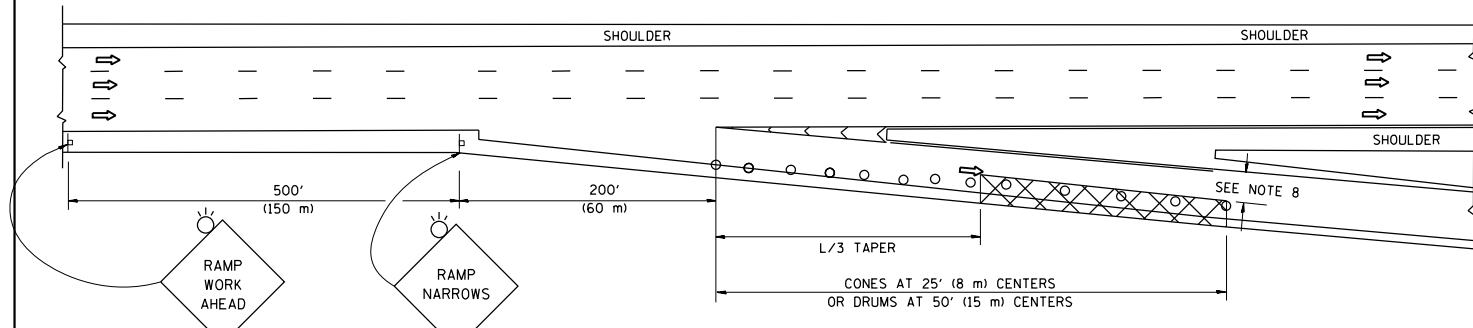
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2014-0741	COOK	18	16
TC-13		CONTRACT NO. 62A17		
JFAU 3565 2FAU 3593 ILLINOIS FED. AID PROJECT				

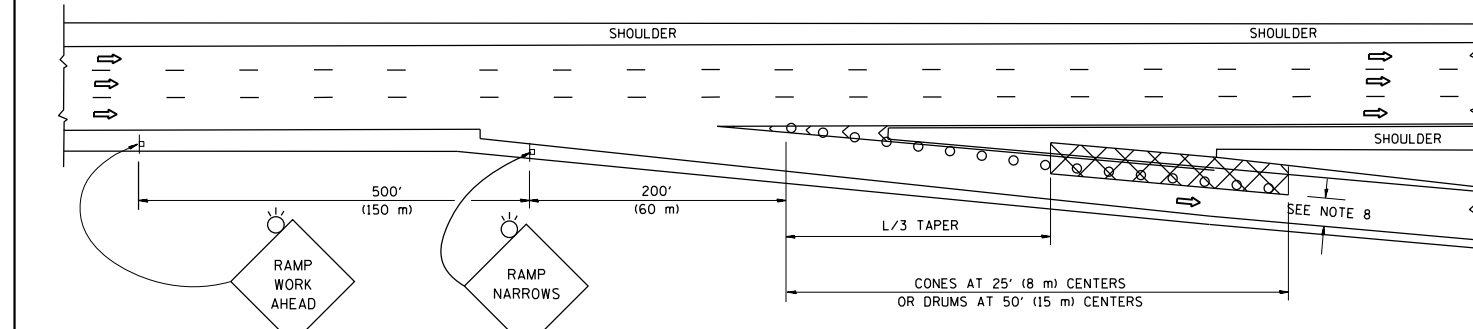
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

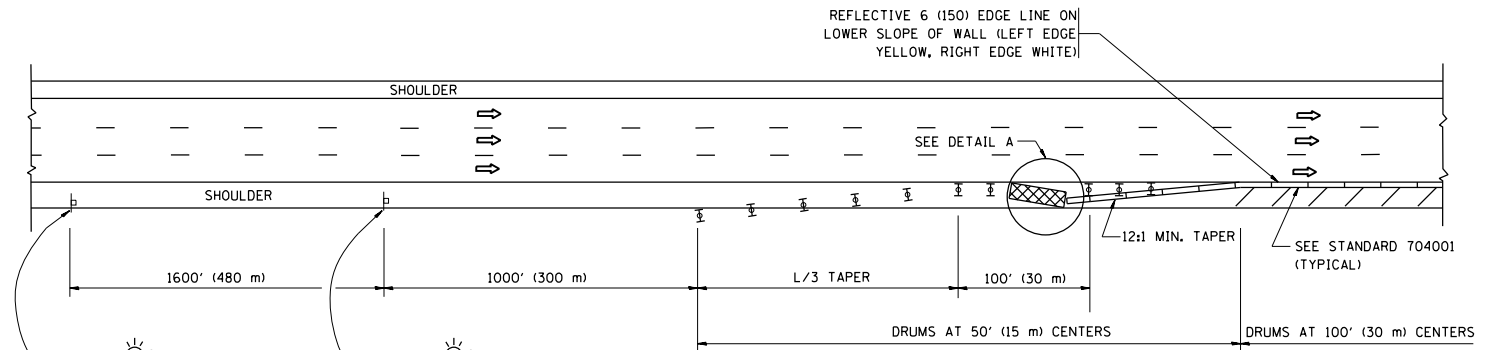
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

GENERAL NOTES

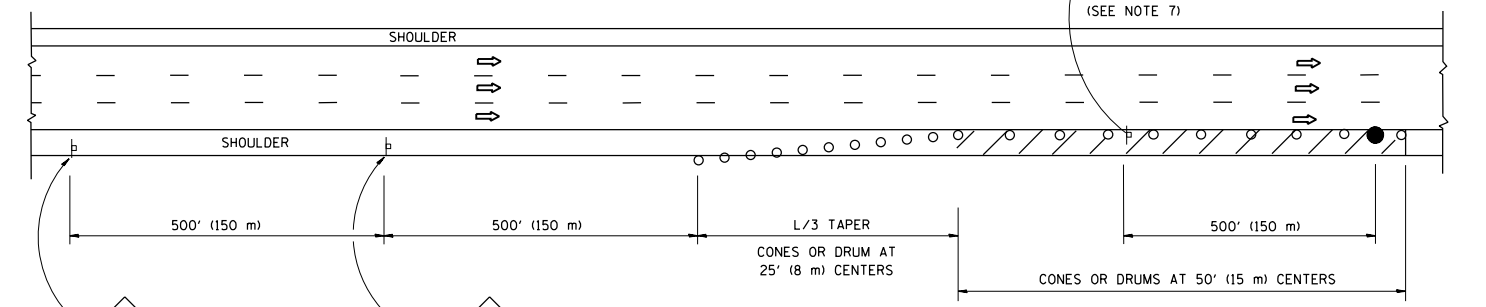
1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC ENGLISH L=0.65(W)(S) L=(W)(S)
W = WIDTH OF OFFSET IN FEET (METERS) S = NORMAL POSTED SPEED MPH (KM/H)	
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.
5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - b. THE WORK AVTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12' MIN. WIDTH TANGENT SECTION
16' MIN. WIDTH CURVE SECTION.

SHOULDER CLOSURE DETAILS

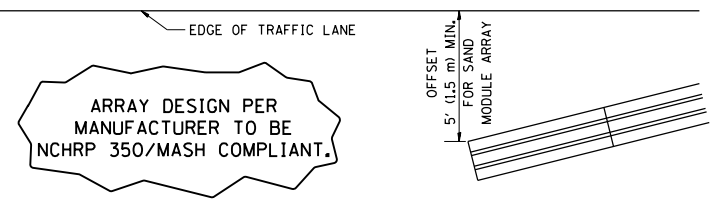


PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

- THIS DETAIL IS USED WHERE:
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCROACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

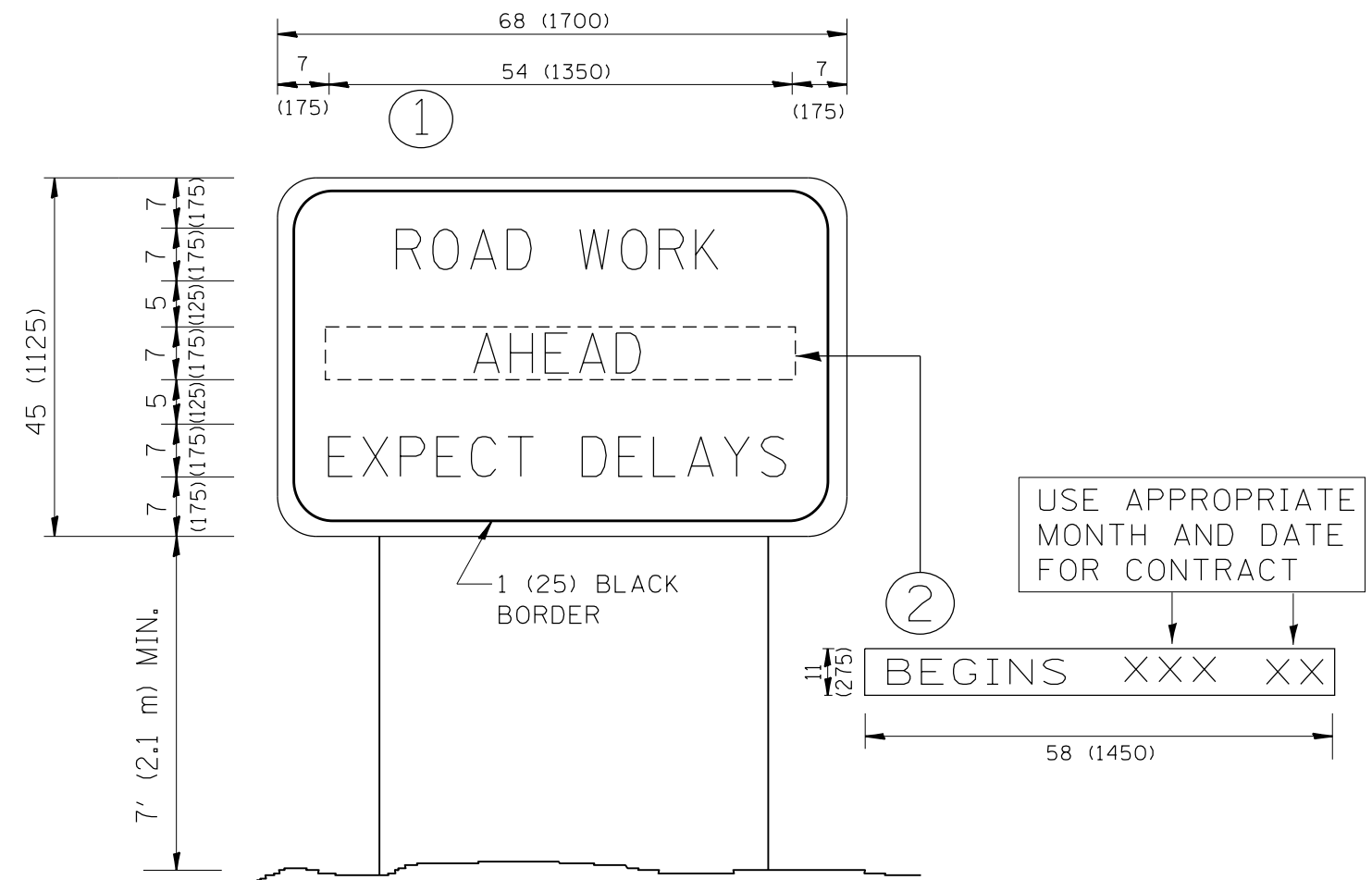
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ei:\pw\work\p\rdot\razeqem\d0418085\DI09\15-shr-plan.dgn		DRAWN - D.W.S.	REVISED - S.P.B. 01-07
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - S.P.B. 12-09
	PLOT DATE = 12/18/2014	DATE - 11-96	REVISED - M.D. 06-13

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY
SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2014-0741	COOK	18	17
TC-17		CONTRACT NO. 62A17		
I/FAU 3565 2/FAU 3593 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = razeqem	DESIGNED -	REVISED - R. MIRS 09-15-97
ct:\pw\work\p1dot\razeqem\d0418085\DI09#15-sh1-plan.dgn		DRAWN -	REVISED - R. MIRS 12-11-97
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 12/18/2014	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	2014-074I	COOK	18	18
TC-22			CONTRACT NO. 62A17	
I/FAU 3565 2/FAU 3593 ILLINOIS FED. AID PROJECT				