

019

FOR INDEX OF SHEETS, SEE SHEET NO. 2

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

04-24-2020 LETTING ITEM 019

FAI ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	1
ILLINOIS		CONTRACT NO. 62K34		

PROJECT LOCATED IN CITY OF JOLIET

TRAFFIC DATA I-80

2017 ADT = 85,400

POSTED SPEED = 55 MPH

# PROPOSED HIGHWAY PLANS

FAI ROUTE 80: I-80  
CENTER ST TO ROWELL AVE  
SECTION 2019-162-BR  
BRIDGE DECK REPAIRS  
WILL COUNTY  
C-91-118-20

HBM ENGINEERING GROUP, LLC  
ROBERT T. BORD, P.E.  
\*062-043749



DATE: 01/13/2020

SIGNATURE AND SEAL  
APPLY TO DRAWINGS:  
1-10 AND 13

EXPIRATION DATE: 11-30-2021

HBM ENGINEERING GROUP, LLC  
MOUSSA A. ISSA, PH.D., P.E., S.E.  
\*081-005738



DATE: 01/13/2020

SIGNATURE AND SEAL  
APPLY TO DRAWINGS:  
60-81

EXPIRATION DATE: 11-30-2020

ACCURATE GROUP, INC.  
JENNIFER MARIE TOBERGTE, P.E.  
\*062-063235



DATE: 01/13/2020

SIGNATURE AND SEAL  
APPLY TO DRAWINGS:  
12, 14-16 AND 46-59

EXPIRATION DATE: 11-30-2021

ACCURATE GROUP, INC.  
SNEHA PRIYANG SHAH, S.E.  
\*081-007062



DATE: 01/13/2020

SIGNATURE AND SEAL  
APPLY TO DRAWINGS:  
82-83

EXPIRATION DATE: 11-30-2021

MILLENNIA PROFESSIONAL SERVICES  
MATTHEW D ROEHLER  
\*062-067917



DATE: 01/13/2020

SIGNATURE AND SEAL  
APPLY TO DRAWINGS:  
11 AND 29-45

EXPIRATION DATE: 11-30-2021



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER PRAVEEN KAINI, PE (847) 705-4237  
PROJECT MANAGER FAWAD AQUEEL, PE, PTOE (847) 705-4247

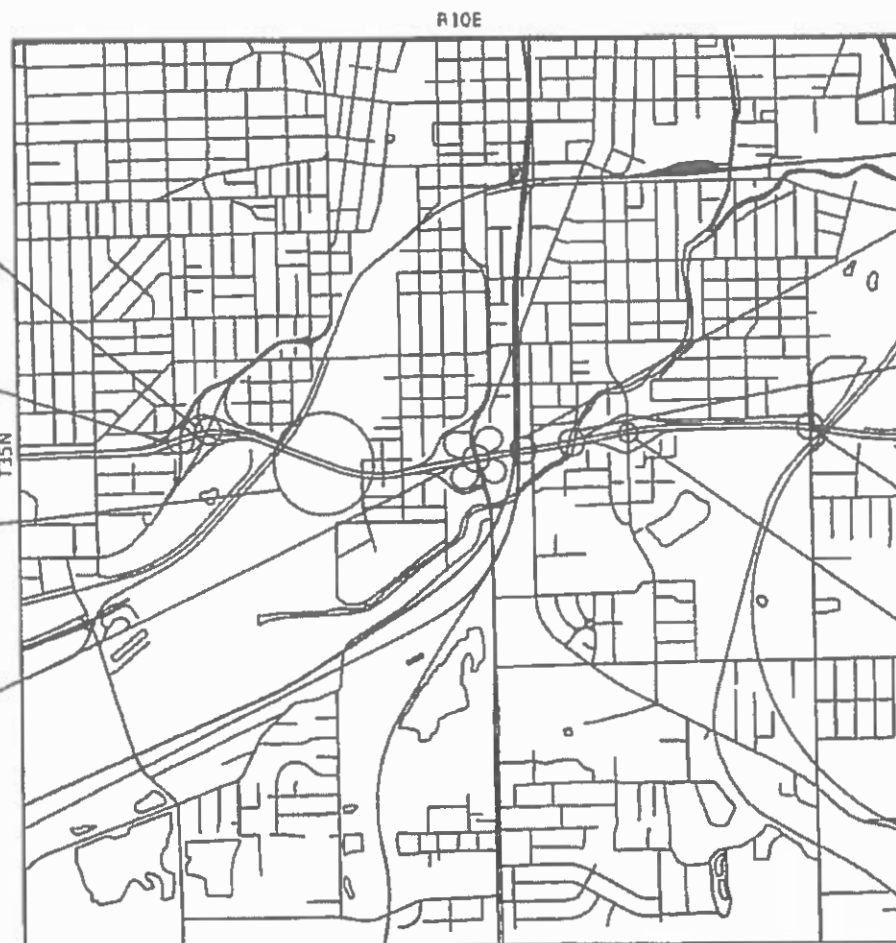
CONTRACT NO. 62K34

I-80 OVER CENTER ST  
SB TO I-80 EB RAMP  
SN 099-0054  
SN 099-0055

I-80 OVER CENTER ST  
SN 099-0052  
SN 099-0053

I-80 OVER US 6, RR &  
DES PLAINES RIVER  
SN 099-0056  
SN 099-0057

I-80 OVER IL 53  
SN 099-0058  
SN 099-0059

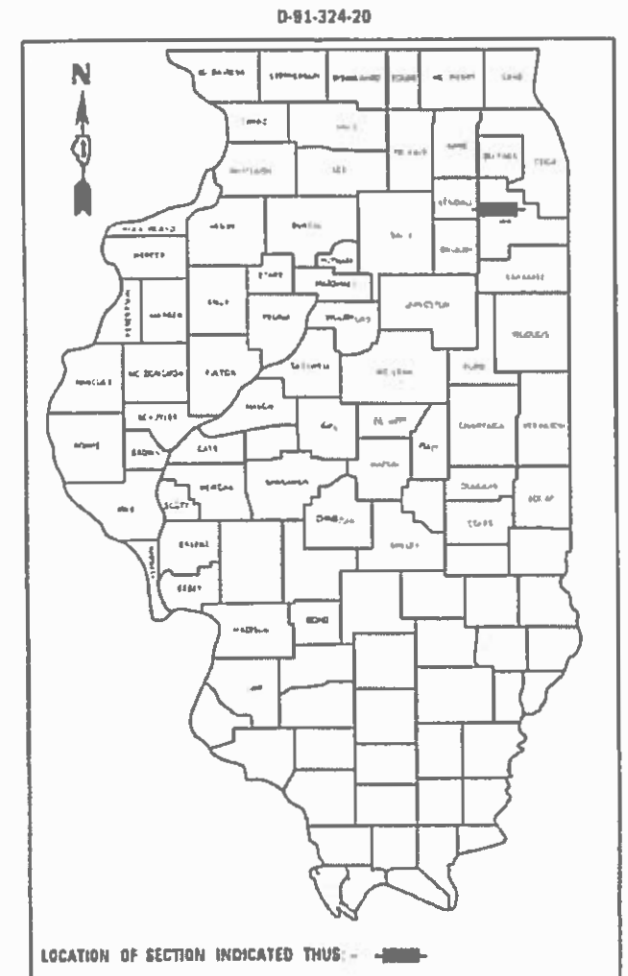


I-80 OVER  
GARDNER ST & RR  
SN 099-0060  
SN 099-0061

I-80 OVER HICKORY CR  
SN 099-0062  
SN 099-0063

I-80 OVER  
ROWELL AVE & RR  
SN 099-0066  
SN 099-0067

I-80 OVER  
RICHARDS ST  
SN 099-0064  
SN 099-0065



LOCATION OF SECTION INDICATED THUS: [Symbol]

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED January 24, 2020

Anthony A. D'Amico 1945  
REGIONAL ENGINEER

March 20, 2020

[Signature]  
ENGINEER OF DESIGN AND ENVIRONMENT

March 20, 2020

[Signature]  
DIRECTOR OF HIGHWAY PROJECT IMPLEMENTATION 13

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

LOCATION MAP NOT TO SCALE  
GROSS LENGTH = NET LENGTH = 11,500 FT = 2.18 MILE

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**LIST OF HIGHWAY STANDARDS**

STANDARD NO.	DESCRIPTION
701101	OFF-RD OPERATIONS, MULTILANE, 15' (4.5m) TO 24"(600mm) FROM PAVEMENT EDGE
701400	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401	LANE CLOSURE, FREEWAY/EXPRESSWAY
701411	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS ≥ 45 MPH
701427	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≤ 40 MPH
701428	TRAFFIC CONTROL SETUP AND REMOVAL FREEWAY/EXPRESSWAY
701446	TWO LANE CLOSURE, FREEWAY/EXPRESSWAY
701901	TRAFFIC CONTROL DEVICES

**COMMITMENTS**

NONE

**GENERAL NOTES**

1. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
2. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
3. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
4. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
5. ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
6. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT Kalpana.Kannan-Hosadurga@illinois.gov FOR ARTERIALS AND (847)705-4155 FOR EXPRESSWAYS A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
7. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE CITY OF JOLIET
8. THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM THE BUREAU OF MAINTENANCE BRIDGE INSPECTORS.
9. THE "ARTERIAL ROAD INFORMATION SIGN (TC-22)", HIGHWAY STANDARD 701101 AND 701427 ARE APPLICABLE ONLY TO ARTERIAL ROADS AND SHALL NOT BE APPLIED TO EXPRESSWAYS.

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES, INDEX OF SHEETS AND HIGHWAY STANDARDS  
I-80 FROM CENTER STREET TO ROWELL AVENUE**

SCALE: 100:0.0000 " = 1" / in. SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	2
			CONTRACT NO. 62K34	
		ILLINOIS	FED. AID PROJECT	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE							
				100% STATE							
				BRIDGE							
				0047							
SN 099-0052 & SN 099-0053	SN 099-0054 & SN 099-0055	SN 099-0056 & SN 099-0057	SN 099-0058 & SN 099-0059	SN 099-0060 & SN 099-0061	SN 099-0062 & SN 099-0063	SN 099-0064 & SN 099-0065	SN 099-0066 & SN 099-0067				
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	0.6	0.6	1.2	0.6	1.2	0.6	0.6	0.6
67100100	MOBILIZATION	L SUM	1	0.1	0.1	0.2	0.1	0.2	0.1	0.1	0.1
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	32	4	4	4	4	4	4	4	4
70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	0.1	0.1	0.2	0.1	0.2	0.1	0.1	0.1
78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	275	25	25	100	25	25	25	25	25
78008220	POLYUREA PAVEMENT MARKING TYPE I - LINE 5"	FOOT	275	25	25	100	25	25	25	25	25
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1		0.2	0.2	0.1	0.2	0.1	0.2	
X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	0.1	0.1	0.2	0.1	0.2	0.1	0.1	0.1
X7013820	TRAFFIC CONTROL SURVEILLANCE, EXPRESSWAYS	CAL DA	32	4	4	4	4	4	4	4	4
Z0004556	HOT-MIX ASPHALT SURFACE REMOVAL (DECK)	SQ YD	154	43	25		18	26	12	30	
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	22	6		4				8	4
Z0016200	DECK SLAB REPAIR (PARTIAL)	SQ YD	206	37	25	51	18	26	12	22	15
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	948.2		158	158.2	158	158	158	158	
* Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	6		1	1	1	1	1	1	

\* SPECIALTY ITEM

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES  
I-80 FROM CENTER STREET TO ROWELL AVENUE**

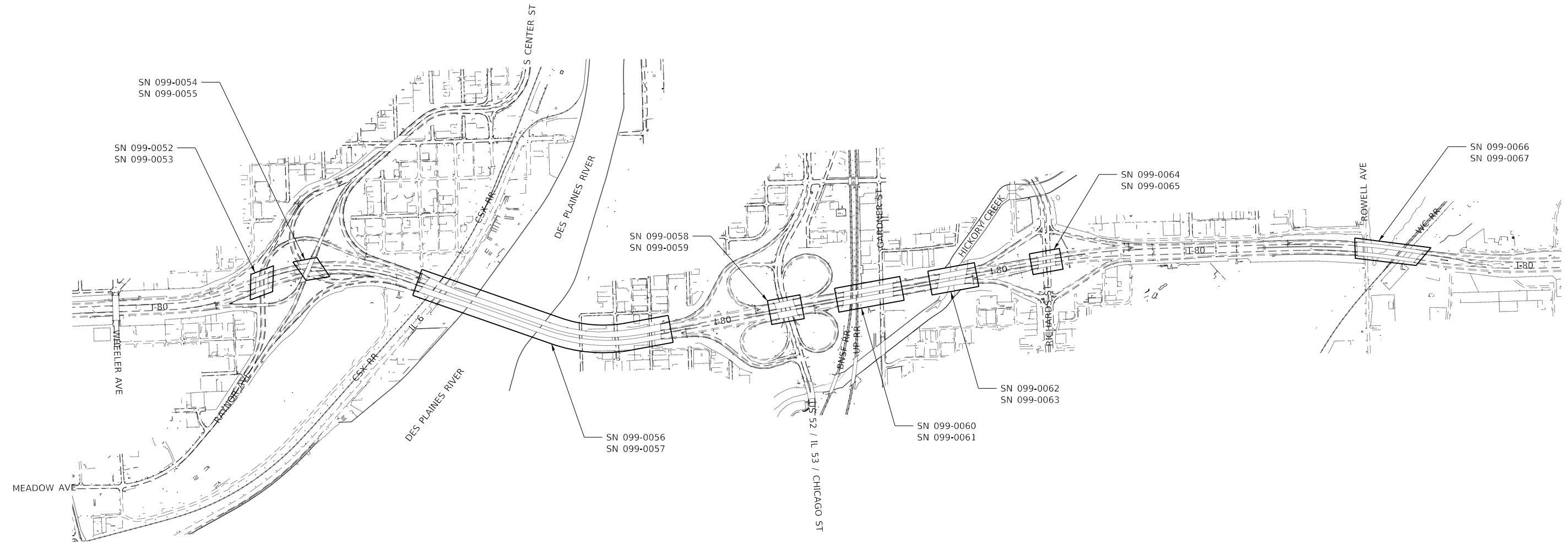
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	3
			CONTRACT NO. 62K34	
		ILLINOIS	FED. AID PROJECT	





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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

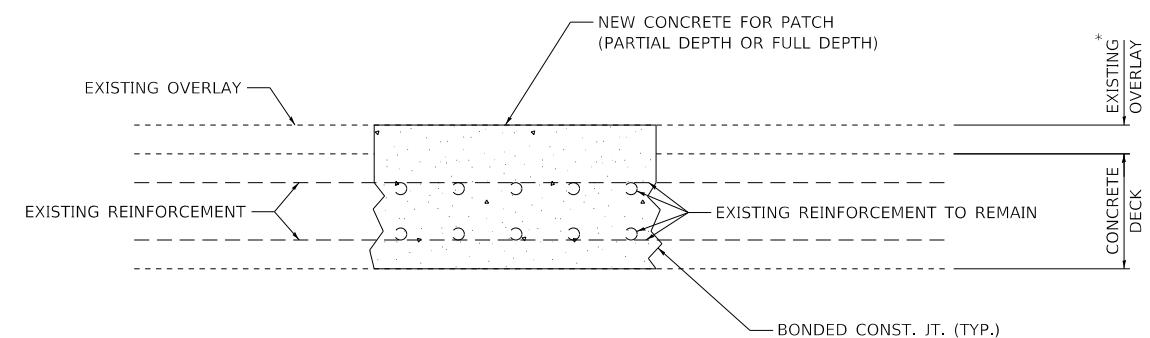
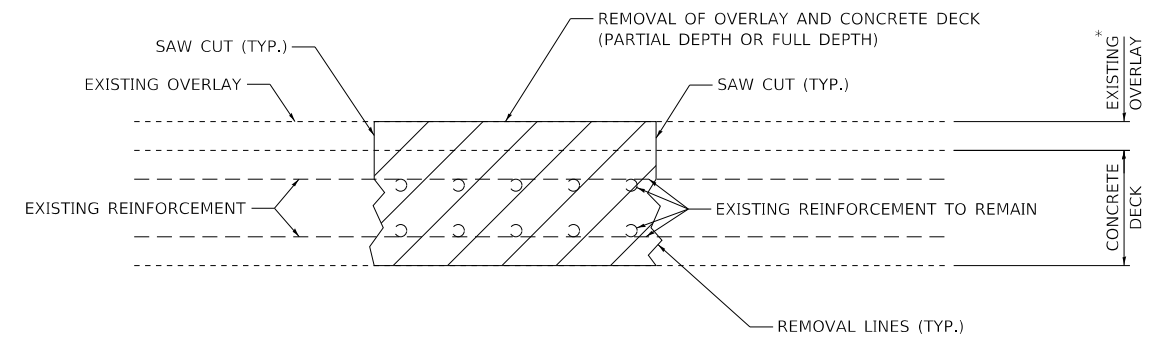
<b>KEY PLAN</b>			
<b>I-80 FROM CENTER STREET TO ROWELL AVENUE</b>			
SCALE: 1000:0.0000	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	5
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

**MOT GENERAL NOTES**

1. THE TRAFFIC CONTROL DEPICTED IN THE PLANS ARE THE MINIMUM REQUIREMENT. ADDITIONAL TRAFFIC CONTROL DEVICES AS SPECIFIED IN THE HIGHWAY STANDARDS AS SHOWN IN THE INDEX OF SHEETS AND THE SPECIAL PROVISIONS SHALL BE PLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER. ALL TRAFFIC CONTROL DEVICES SHALL BE CONSIDERED INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, (EXPRESSWAYS) UNLESS OTHERWISE INDICATED WITHIN THESE GENERAL NOTES, PLANS OR SPECIAL PROVISIONS.
2. TRAFFIC CONDITIONS, ACCIDENTS, AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY OR REMOVE LANE CLOSURES OR CHANNELIZATION SHOWN IN THE PLANS. THE CONTRACTOR SHALL PROMPTLY RESPOND AT THE TIME OF NOTIFICATION BY THE ENGINEER FOR THE MAINTENANCE OF TRAFFIC CONTROL DEVICES.
3. ALL "ROAD CONSTRUCTION AHEAD", "SHOULDER CLOSED AHEAD", AND "SPEED LIMIT AHEAD" SIGNS SHALL BE EQUIPPED WITH MONO-DIRECTIONAL TYPE A AMBER FLASHING LIGHTS.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING LABOR, SIGNS AND TRAFFIC CONTROL DEVICES NECESSARY FOR THE MAINTENANCE OF TRAFFIC UNLESS NOTED OTHERWISE IN THE SPECIAL PROVISIONS.
5. LANES AND RAMPS MAY ONLY BE CLOSED DURING THE ALLOWABLE HOURS LISTED IN THE KEEPING THE EXPRESSWAY OPEN TO TRAFFIC SPECIAL PROVISION. THIS CONTRACT WILL ONLY ALLOW WEEKEND CLOSURES FOR BRIDGE DECK PATCHING AS LISTED IN THE SPECIAL PROVISIONS.
6. THE CONTRACTOR SHALL BE REQUIRED TO MAINTAIN TRAFFIC IN ACCORDANCE WITH THE TRAFFIC CONTROL GENERAL NOTES, SPECIAL PROVISIONS, APPLICABLE STATE STANDARDS, AND AS DIRECTED BY THE ENGINEER. ANY CHANGES TO THE TRAFFIC CONTROL SHALL BE SUBMITTED TO THE ENGINEER 48 HOURS IN ADVANCE OF ANY PROPOSED CHANGE FOR APPROVAL PRIOR TO IMPLEMENTING ANY CHANGES.
7. THE CONTRACTOR SHALL PROVIDE ADEQUATE TEMPORARY DRAINAGE AND EROSION CONTROL PROTECTION DURING ALL PHASES OF CONSTRUCTION.
8. THE CONTRACTOR SHALL PLACE ONE (1) CHANGEABLE MESSAGE SIGN AT EACH END OF THE PROJECT AND/OR AS DIRECTED BY THE ENGINEER TO INFORM MOTORISTS OF UPCOMING CONSTRUCTION ACTIVITIES. THE MESSAGE SIGNS WITH APPROPRIATE INFORMATION SHALL BE PLACED SEVEN (7) DAYS BEFORE START OF CONSTRUCTION ACTIVITY. THIS WORK IS TO BE PAID FOR AT THE CONTRACT UNIT PRICE PER CALENDAR DAY, "CHANGEABLE MESSAGE SIGN". TWO (2) CHANGEABLE MESSAGE SIGNS WILL BE PLACED AT LOCATIONS WITH CLOSURES ON THE MAINLINE EXPRESSWAY AND ANY IMPACTED RAMPS.
9. IMMEDIATELY AFTER THE COMPLETION OF CONSTRUCTION, THE CONTRACTOR SHALL RESTORE ALL PERMANENT PAVEMENT MARKINGS, SIGNS, AND OTHER TRAFFIC CONTROL DEVICES THAT WERE COVERED, IF THEY WERE REMOVED, DAMAGED, OR OTHERWISE AFFECTED BY CONSTRUCTION. THE COST TO REPAIR ANY DAMAGES WILL BE BORNE BY THE CONTRACTOR AND NOT THE RESPONSIBILITY OF THE DEPARTMENT.
10. CONTRACTOR SHALL REQUEST AND GAIN THE APPROVAL FROM THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S EXPRESSWAY TRAFFIC OPERATIONS ENGINEER AT WWW.IDOTLCS.COM TWENTY-FOUR (24) HOURS IN ADVANCE OF ALL DAILY LANE, RAMP AND SHOULDER CLOSURES.
11. THE CONTRATOR SHALL COVER ALL EXISTING SIGNS THAT CONFLICT WITH THE REVISED TRAFFIC PATTERN, ACCORDING TO ARTICLE 701.04. WITH THE EXCEPTION THAT, THE MODIFICATION OF OVERHEAD SIGNS IS NOT REQUIRED FOR THIS CONTRACT.
12. IN ADDITION TO FIELD AND AERIAL SURVEYS, PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING FACILITIES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND EXISTING CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING MATERIALS. SUCH VARIATIONS SHALL NOT BE A CAUSE FOR ADDITIONAL COMPENSATION DUE TO A CHANGE IN THE SCOPE OF WORK.
13. NO MATERIAL OR EQUIPMENT SHALL BE LEFT ON OR NEAR THE ROADWAY DURING NON-WORKING HOURS.
14. LANE CLOSURES FOR ALL STAGES OF BRIDGE DECK PATCHING ARE ONLY PERMITTED DURING THE FOLLOWING TIME FRAME:  
FRIDAY: 10:00 PM (FRI.) TO 5:00 AM (MON.).
15. THE EXISTING PERMANENT PAVEMENT MARKING REMOVED IN THE AREA OF A DECK PATCH SHALL BE REPLACED WITH POLYUREA PAVEMENT MARKING TO MATCH THE WIDTH AND COLOR THAT WAS REMOVED.
16. THE ENGINEER SHALL CONTACT PATRICE HARRIS, AREA TRAFFIC FIELD TECHNICIAN, AT (847) 705-4153 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
17. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT Kalpana.Kannan-Hosadurga@illinois.gov AND THE EXPRESSWAY TRAFFIC CONTROL SUPERVISOR CLOSURES ON THE EXPRESSWAYS AT (847) 705-4155 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK. THE CONTRACTOR SHALL REGISTER ACCOUNT AT WWW.IDOTLCS.COM TO REQUEST.

**PATCHING DETAILS**



\* EXISTING OVERLAY:  
 2" POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT  
 AT 099-0052,-0053; 099-0054,-0055; 099-0058,-0059; 099-0061,-0061;  
 099-0062,-0063;099-0064,-0065  
 VARIABLE THICKNESS LATEX MODIFIED CONCRETE OVERLAY  
 AT 099-0056,-0057; 099-0066,-0067

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>MOT GENERAL NOTES</b>			
<b>I-80 FROM CENTER STREET TO ROWELL AVENUE</b>			
SCALE: 100:0.0000 " = 1"	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	6
			CONTRACT NO. 62K34	
		ILLINOIS	FED. AID PROJECT	

**SUGGESTED SEQUENCE OF CONSTRUCTION**

**STAGE 1**

INSTALL PORTABLE CHANGEABLE MESSAGE SIGNS AS DIRECTED BY THE ENGINEER.  
 CLOSURES WILL ONLY BE ALLOWED ON WEEKENDS DURING HOURS NOTED IN THE SPECIAL PROVISIONS.  
 UTILIZE IDOT HIGHWAY STANDARD 701400 FOR APPROACH TO LANE CLOSURES.  
 CLOSE EB I-80 LEFT LANE USING HIGHWAY STANDARDS 701400 AND 701401  
 PERFORM PARTIAL AND FULL DEPTH PATCHING ON STRUCTURES SN 099-0053, SN 099-0054, SN 099-0057, SN 099-0058, SN 099-0061, SN 099-0063, SN 099-0065 AND SN 099-0067

**STAGE 2**

INSTALL PORTABLE CHANGEABLE MESSAGE SIGNS AS DIRECTED BY THE ENGINEER.  
 CLOSURES WILL ONLY BE ALLOWED ON WEEKENDS DURING HOURS NOTED IN THE SPECIAL PROVISIONS.  
 UTILIZE IDOT HIGHWAY STANDARD 701400 FOR APPROACH TO LANE CLOSURES.  
 CLOSE WB I-80 LEFT LANE USING HIGHWAY STANDARDS 701400 AND 701401  
 PERFORM PARTIAL AND FULL DEPTH PATCHING ON STRUCTURES SN 099-0052, SN 099-0055, SN 099-0056, SN 099-0059, SN 099-0060, SN 099-0062, SN 099-0064 AND SN 099-0066

**STAGE 3A**

INSTALL PORTABLE CHANGEABLE MESSAGE SIGNS AS DIRECTED BY THE ENGINEER.  
 CLOSE OUTSIDE SHOULDER AND ADJACENT LANE(S) AND RAMPS AS INDICATED IN THE MOT PLANS. CLOSURES WILL ONLY BE ALLOWED ON WEEKENDS DURING HOURS NOTED IN THE SPECIAL PROVISIONS.  
 UTILIZE IDOT HIGHWAY STANDARD 701400 FOR APPROACH TO LANE CLOSURES.  
 KEEP EB I-80 LANE 1 OPEN  
 CLOSE EB I-80 LANE 2 ACCORDING TO MOT PLANS  
 KEEP RAMP EB I-80 TO NB CENTER STREET OPEN  
 PERFORM PARTIAL AND FULL DEPTH PATCHING ON STRUCTURES SN 099-0052 AND SN 099-0055

**STAGE 3B**

INSTALL PORTABLE CHANGEABLE MESSAGE SIGNS AS DIRECTED BY THE ENGINEER.  
 CLOSE OUTSIDE SHOULDER AND ADJACENT LANE(S) AND RAMPS AS INDICATED IN THE MOT PLANS. CLOSURES WILL ONLY BE ALLOWED ON WEEKENDS DURING HOURS NOTED IN THE SPECIAL PROVISIONS.  
 UTILIZE IDOT HIGHWAY STANDARD 701400 FOR APPROACH TO LANE CLOSURES.  
 KEEP EB I-80 LANE 1 OPEN  
 CLOSE EB I-80 LANE 2 AND AUXILIARY LANES ACCORDING TO MOT PLANS  
 KEEP RAMP EB I-80 TO NB CENTER STREET OPEN  
 KEEP RAMP NB RAYNOR AVE TO EB I-80 OPEN  
 KEEP RAMP EB I-80 TO SB CHICAGO STREET OPEN  
 CLOSE RAMP FROM CHICAGO STREET TO EB I-80 USING THE DISTRICT DETAIL "ENTRANCE AND EXIT RAMP CLOSURE DETAILS (TC-08)" AND UTILIZE DETOUR AS SHOWN IN PLANS.  
 KEEP RAMP EB I-80 TO NB CHICAGO STREET OPEN  
 PERFORM PARTIAL AND FULL DEPTH PATCHING ON STRUCTURES SN 099-0056 AND SN 099-0059

**STAGE 3C**

INSTALL PORTABLE CHANGEABLE MESSAGE SIGNS AS DIRECTED BY THE ENGINEER.  
 CLOSE OUTSIDE SHOULDER AND ADJACENT LANE(S) AND RAMPS AS INDICATED IN THE MOT PLANS. CLOSURES WILL ONLY BE ALLOWED ON WEEKENDS DURING HOURS NOTED IN THE SPECIAL PROVISIONS.  
 UTILIZE IDOT HIGHWAY STANDARD 701400 FOR APPROACH TO LANE CLOSURES.  
 CLOSE RAMP FROM CHICAGO STREET TO EB I-80 USING THE DISTRICT DETAIL "ENTRANCE AND EXIT RAMP CLOSURE DETAILS (TC-08)" AND UTILIZE DETOUR AS SHOWN IN PLANS.  
 CLOSE LANE 1 ON THE CENTER STREET RAMP, TO I-80 EASTBOUND, AS SHOWN IN THE STAGE 3C PLAN SHEETS.  
 SHIFT EASTBOUND I-80 TRAFFIC INTO THE MODIFIED TRAFFIC PATTERN, AS SHOWN IN THE STAGE 3C PLAN SHEETS.  
 PERFORM PARTIAL DEPTH PATCH WORK IN DESIGNATED WORK ZONES, AS SHOWN IN THE STAGE 3C PLAN SHEETS OR AS DIRECTED BY THE ENGINEER.

**STAGE 3D**

INSTALL PORTABLE CHANGEABLE MESSAGE SIGNS AS DIRECTED BY THE ENGINEER.  
 CLOSE OUTSIDE SHOULDER AND ADJACENT LANE(S) AND RAMPS AS INDICATED IN THE MOT PLANS. CLOSURES WILL ONLY BE ALLOWED ON WEEKENDS DURING HOURS NOTED IN THE SPECIAL PROVISIONS.  
 UTILIZE IDOT HIGHWAY STANDARD 701400 FOR APPROACH TO LANE CLOSURES.  
 CLOSE RAMP FROM CHICAGO STREET TO EB I-80 USING THE DISTRICT DETAIL "ENTRANCE AND EXIT RAMP CLOSURE DETAILS (TC-08)" AND UTILIZE DETOUR AS SHOWN IN PLANS.  
 CLOSE LANE 1 ON THE CENTER STREET RAMP, TO I-80 EASTBOUND, AS SHOWN IN THE STAGE 3D PLAN SHEETS.  
 SHIFT EASTBOUND I-80 TRAFFIC INTO THE MODIFIED TRAFFIC PATTERN, AS SHOWN IN THE STAGE 3D PLAN SHEETS.  
 PERFORM PARTIAL DEPTH PATCH WORK IN DESIGNATED WORK ZONES, AS SHOWN IN THE STAGE 3D PLAN SHEETS OR AS DIRECTED BY THE ENGINEER.

**STAGE 4A**

INSTALL PORTABLE CHANGEABLE MESSAGE SIGNS AS DIRECTED BY THE ENGINEER.  
 CLOSE OUTSIDE SHOULDER AND ADJACENT LANE(S) AND RAMPS AS INDICATED IN THE MOT PLANS. CLOSURES WILL ONLY BE ALLOWED ON WEEKENDS DURING HOURS NOTED IN THE SPECIAL PROVISIONS.  
 UTILIZE IDOT HIGHWAY STANDARD 701400 FOR APPROACH TO LANE CLOSURES.  
 CLOSE RAMP FROM RICHARDS STREET TO WESTBOUND I-80 USING THE DISTRICT DETAIL "ENTRANCE AND EXIT RAMP CLOSURE DETAILS (TC-08)" AND UTILIZE DETOUR AS SHOWN IN PLANS. ALL CHICAGO STREET RAMPS SHALL REMAIN OPEN DURING THIS STAGE.  
 PERFORM BRIDGE DECK PATCHING AS SHOWN IN THE PLANS OR AS DIRECTED BY THE ENGINEER.

**STAGE 4B**

INSTALL PORTABLE CHANGEABLE MESSAGE SIGNS AS DIRECTED BY THE ENGINEER.  
 CLOSE OUTSIDE SHOULDER AND ADJACENT LANE(S) AND RAMPS AS INDICATED IN THE MOT PLANS. CLOSURES WILL ONLY BE ALLOWED ON WEEKENDS DURING HOURS NOTED IN THE SPECIAL PROVISIONS.  
 UTILIZE IDOT HIGHWAY STANDARD 701400 FOR APPROACH TO LANE CLOSURES.  
 CLOSE RAMP FROM WESTBOUND I-80 TO US ROUTE 52 / IL ROUTE 53 (CHICAGO STREET) USING THE DISTRICT DETAIL "ENTRANCE AND EXIT RAMP CLOSURE DETAILS (TC-08)" AND UTILIZE DETOUR AS SHOWN IN THE PLANS. ALL RICHARDS STREET RAMPS SHALL REMAIN OPEN DURING THIS STAGE.  
 PERFORM BRIDGE DECK PATCHING AS SHOWN IN THE PLANS OR AS DIRECTED BY THE ENGINEER.

MODEL: D:\64bit... FILE NAME: P:\17105746 IDOT District 1\1700\_26\_I80 DR Patching\Sheet Files\ID162K34-SHT-Sequence-HBM.dgn



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	DRAWN - KJD	REVISED -
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PLOT DATE = 2/3/2020	DATE - 01/31/2020	REVISED -

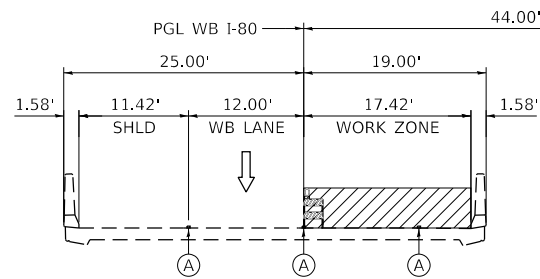
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED SEQUENCE OF CONSTRUCTION  
I-80 FROM CENTER STREET TO ROWELL AVENUE**

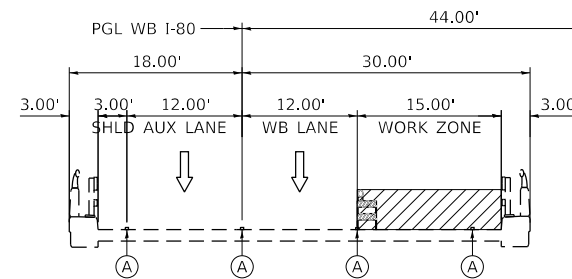
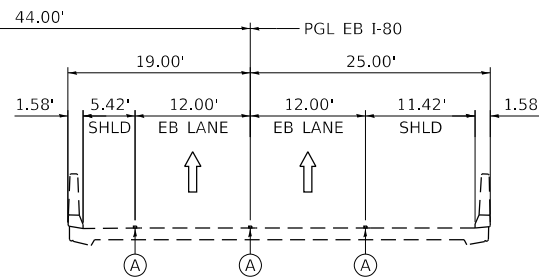
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	7
			CONTRACT NO. 62K34	
		ILLINOIS	FED. AID PROJECT	

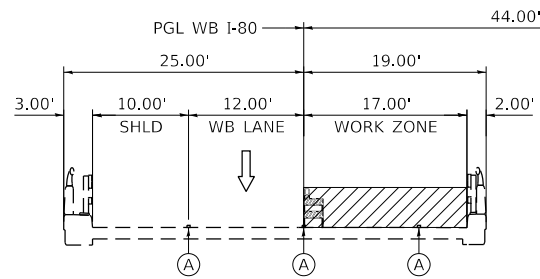
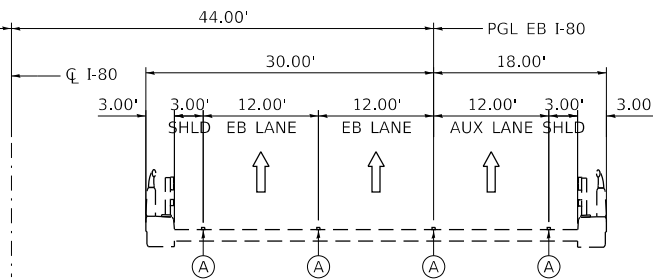




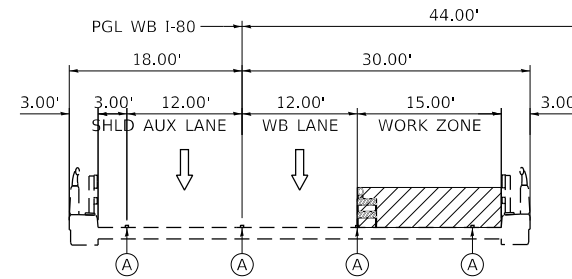
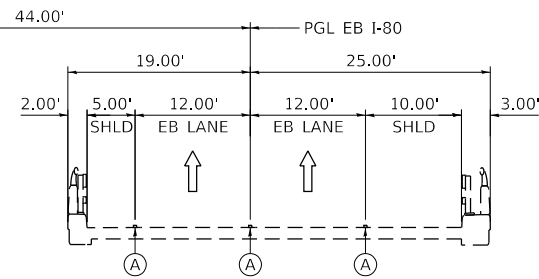
**STAGE 2**  
I-80 OVER RAYNOR AVE



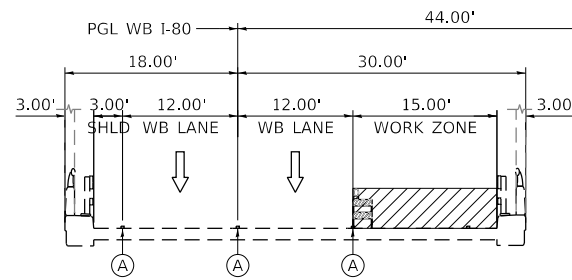
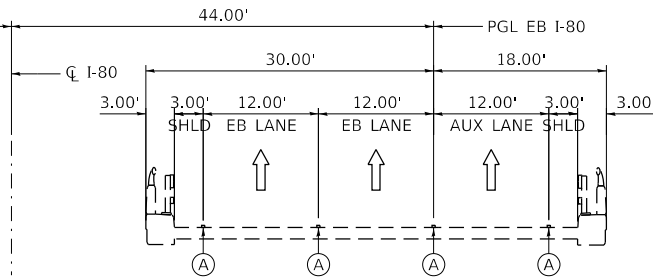
**STAGE 2**  
I-80 OVER RAILROAD/GARDNER STREET



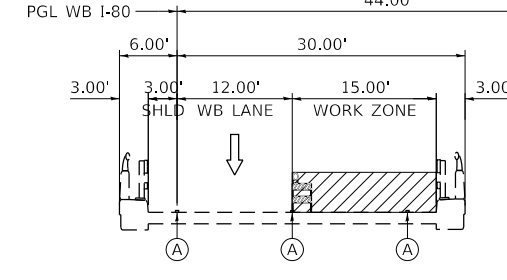
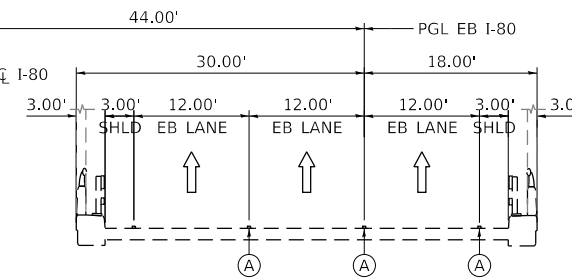
**STAGE 2**  
I-80 OVER CENTER STREET RAMP



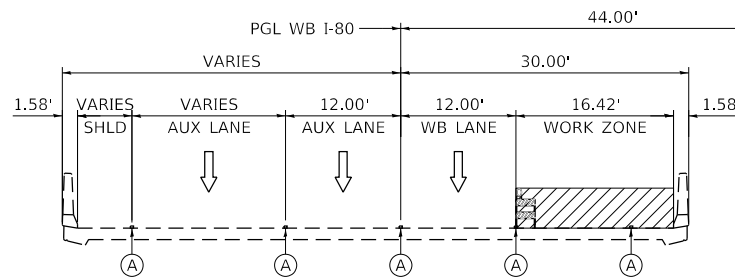
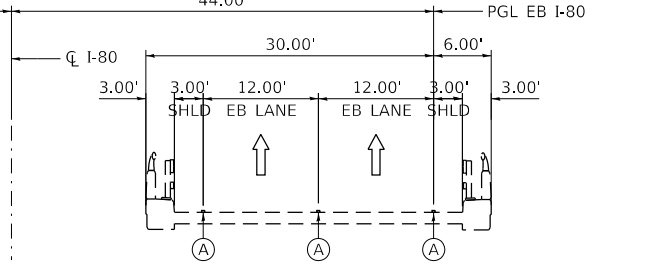
**STAGE 2**  
I-80 OVER HICKORY CREEK



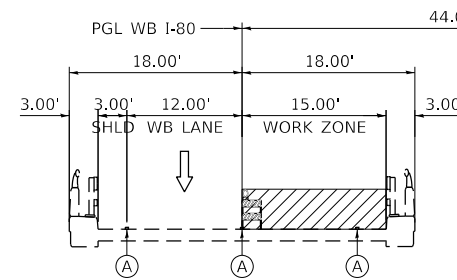
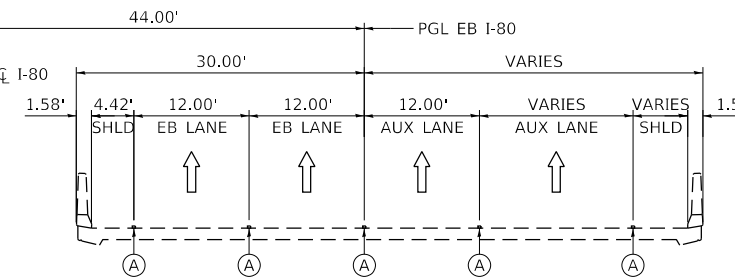
**STAGE 2**  
I-80 OVER DES PLAINES RIVER



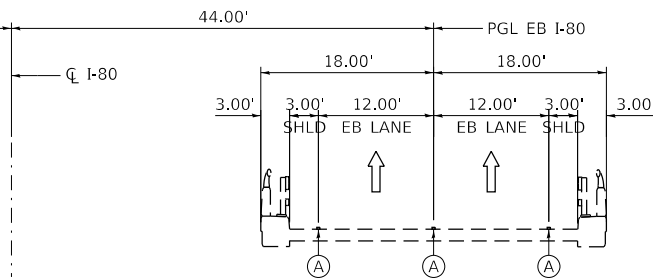
**STAGE 2**  
I-80 OVER RICHARDS STREET



**STAGE 2**  
I-80 OVER IL 53 (CHICAGO STREET)



**STAGE 2**  
I-80 OVER CNRR AND ROWELL AVE



**LEGEND**

- WORK ZONE
- EXISTING PAVEMENT MARKINGS
- TYPE II BARRICADES, DRUMS, OR VERTICAL BARRICADES

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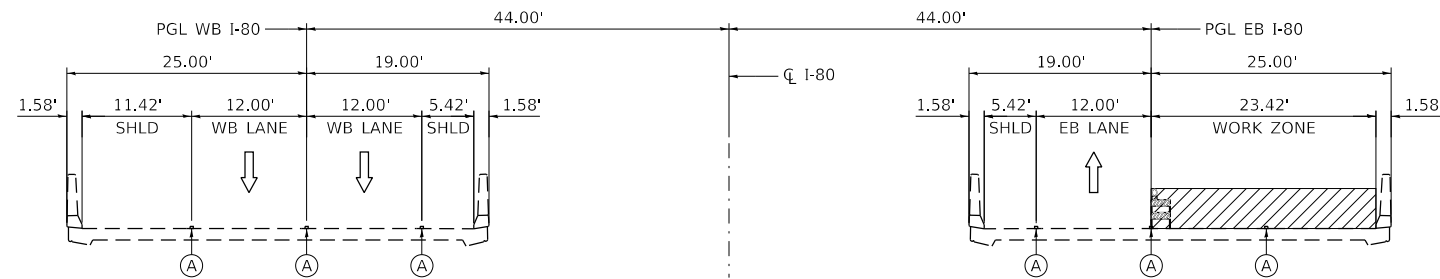
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

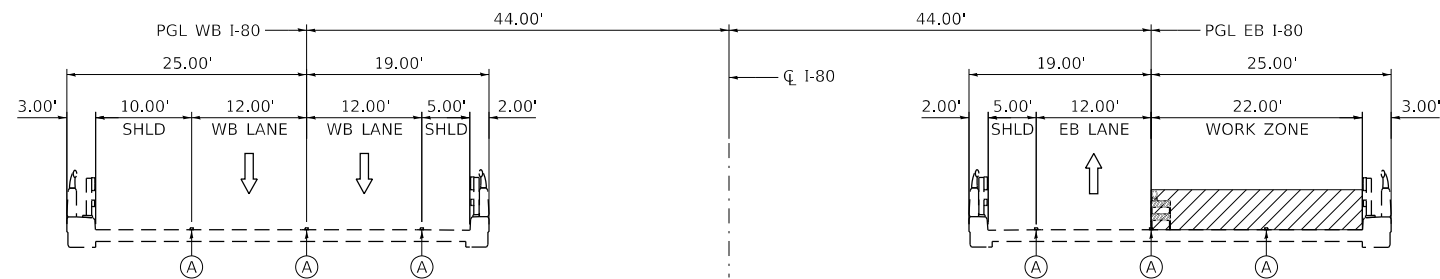
**MOT STAGE 2 TYPICAL SECTIONS  
I-80 FROM CENTER STREET TO ROWELL AVENUE**

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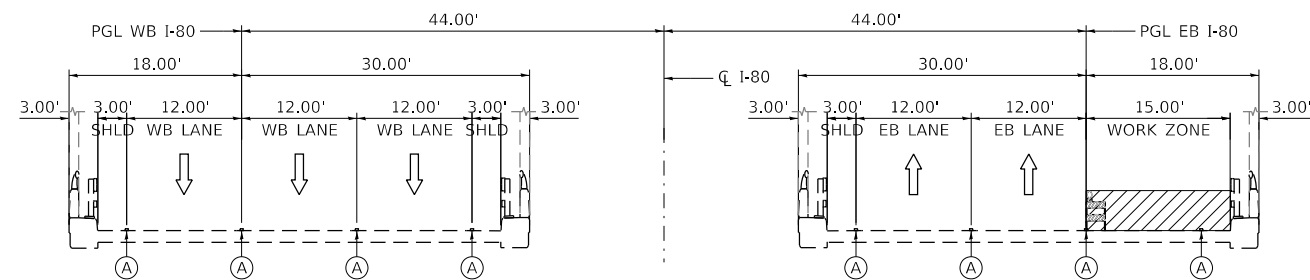
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	9
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				



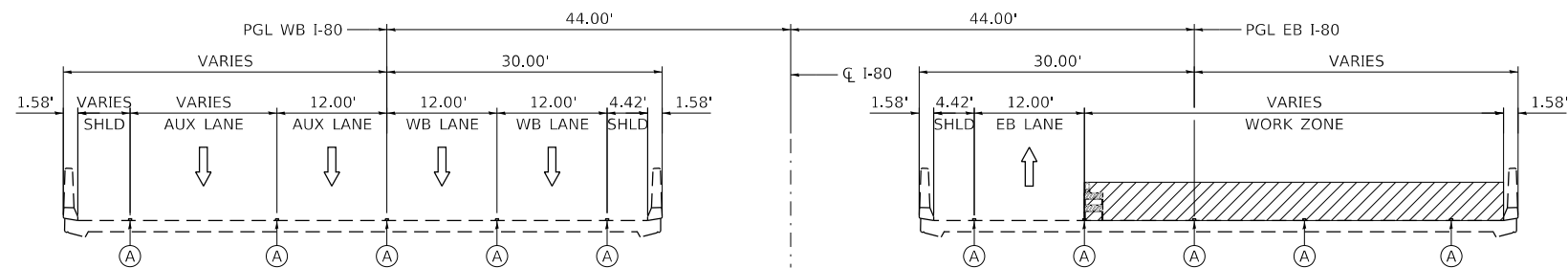
**STAGE 3A**  
I-80 OVER RAYNOR AVE



**STAGE 3A**  
I-80 OVER CENTER STREET RAMP






**STAGE 3B**  
I-80 OVER DES PLAINES RIVER



**STAGE 3B**  
I-80 OVER IL 53 (CHICAGO STREET)

**LEGEND**

-  WORK ZONE
-  EXISTING PAVEMENT MARKINGS
-  TYPE II BARRICADES, DRUMS, OR VERTICAL BARRICADES

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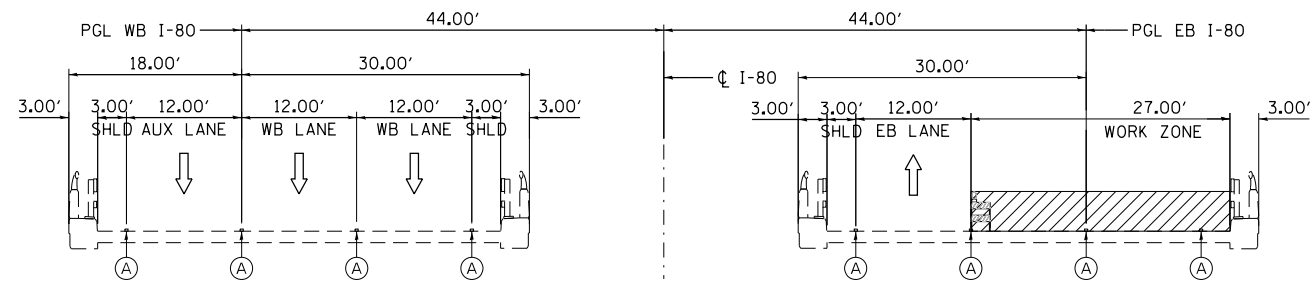
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

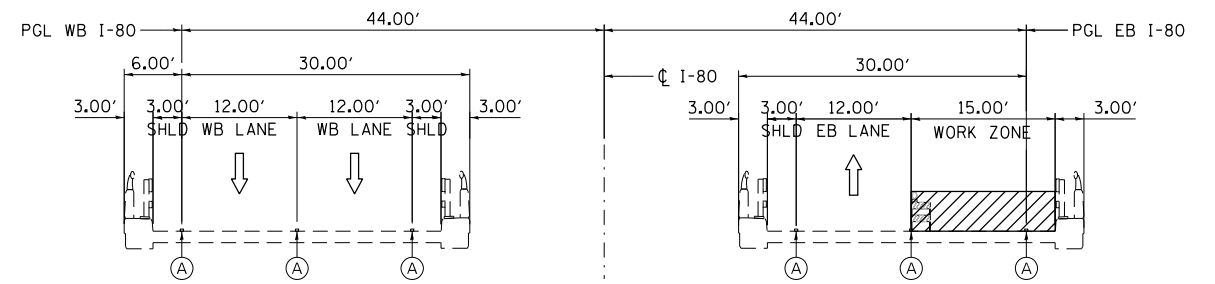
**MOT STAGE 3A AND 3B TYPICAL SECTIONS  
I-80 FROM CENTER STREET TO ROWELL AVENUE**

SCALE: 20:0,0000' : " SHEET 3 OF 5 SHEETS STA. TO STA.

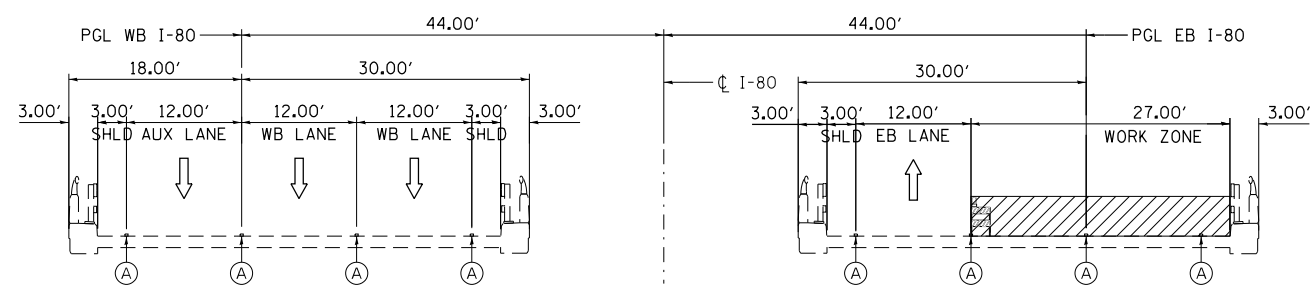
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	10
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				



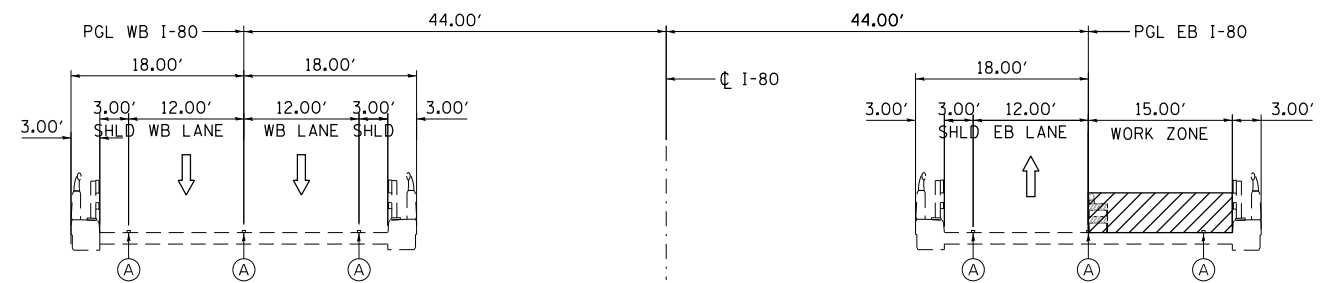
**STAGE 3C**  
I-80 OVER RAILROAD/GARDNER STREET



**STAGE 3D**  
I-80 OVER RICHARDS STREET



**STAGE 3C**  
I-80 OVER HICKORY CREEK



**STAGE 3D**  
I-80 OVER CNRR AND ROWELL AVE

**LEGEND**



WORK ZONE

(A) EXISTING PAVEMENT MARKINGS

TYPE II BARRICADES, DRUMS, OR VERTICAL BARRICADES

DIRECTION OF TRAFFIC

MODEL: \\MILLENNIA\PROF\DESIGN\FILES



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PLOT DATE = \$DATES	CHECKED - JP	REVISED -
	DATE - 01/31/2020	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**MOT STAGE 3C AND 3D TYPICAL SECTIONS  
I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**

SCALE: N/A SHEET 4 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	11
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				





**NOTES**

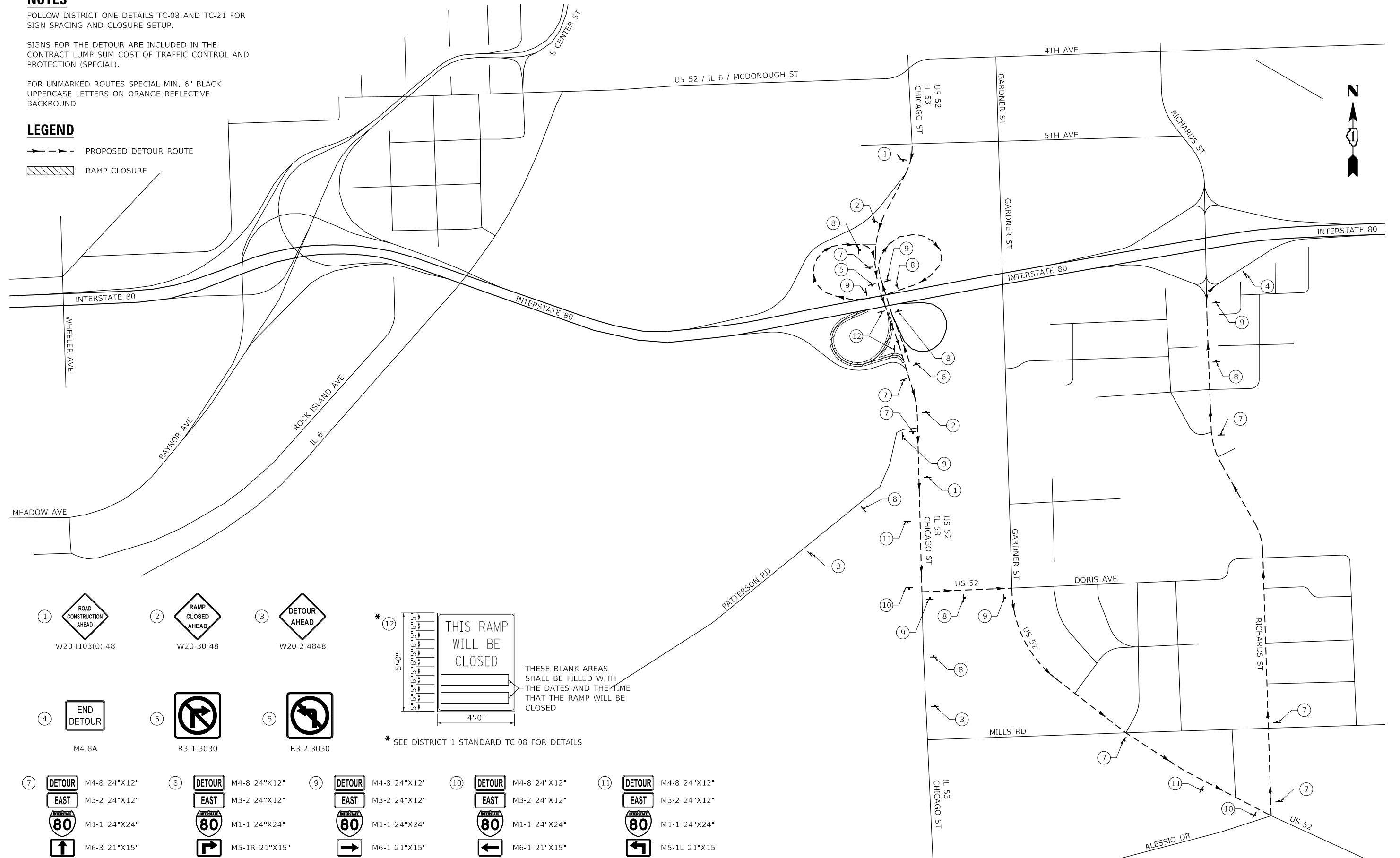
FOLLOW DISTRICT ONE DETAILS TC-08 AND TC-21 FOR SIGN SPACING AND CLOSURE SETUP.

SIGNS FOR THE DETOUR ARE INCLUDED IN THE CONTRACT LUMP SUM COST OF TRAFFIC CONTROL AND PROTECTION (SPECIAL).

FOR UNMARKED ROUTES SPECIAL MIN. 6" BLACK UPPERCASE LETTERS ON ORANGE REFLECTIVE BACKGROUND

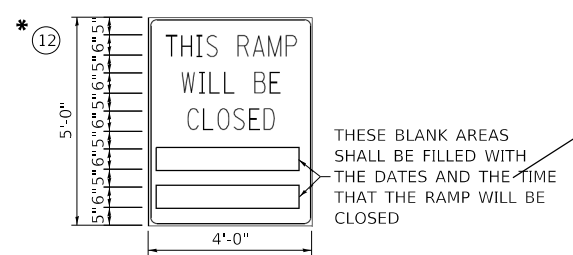
**LEGEND**

- PROPOSED DETOUR ROUTE
- RAMP CLOSURE



- ① W20-1103(0)-48
- ② W20-30-48
- ③ W20-2-4848

- ④ M4-8A
- ⑤ R3-1-3030
- ⑥ R3-2-3030



\* SEE DISTRICT 1 STANDARD TC-08 FOR DETAILS

- ⑦ M4-8 24"x12"  
 M3-2 24"x12"  
 M1-1 24"x24"  
 M6-3 21"x15"
- ⑧ M4-8 24"x12"  
 M3-2 24"x12"  
 M1-1 24"x24"  
 M5-1R 21"x15"
- ⑨ M4-8 24"x12"  
 M3-2 24"x12"  
 M1-1 24"x24"  
 M6-1 21"x15"
- ⑩ M4-8 24"x12"  
 M3-2 24"x12"  
 M1-1 24"x24"  
 M6-1 21"x15"
- ⑪ M4-8 24"x12"  
 M3-2 24"x12"  
 M1-1 24"x24"  
 M5-1L 21"x15"

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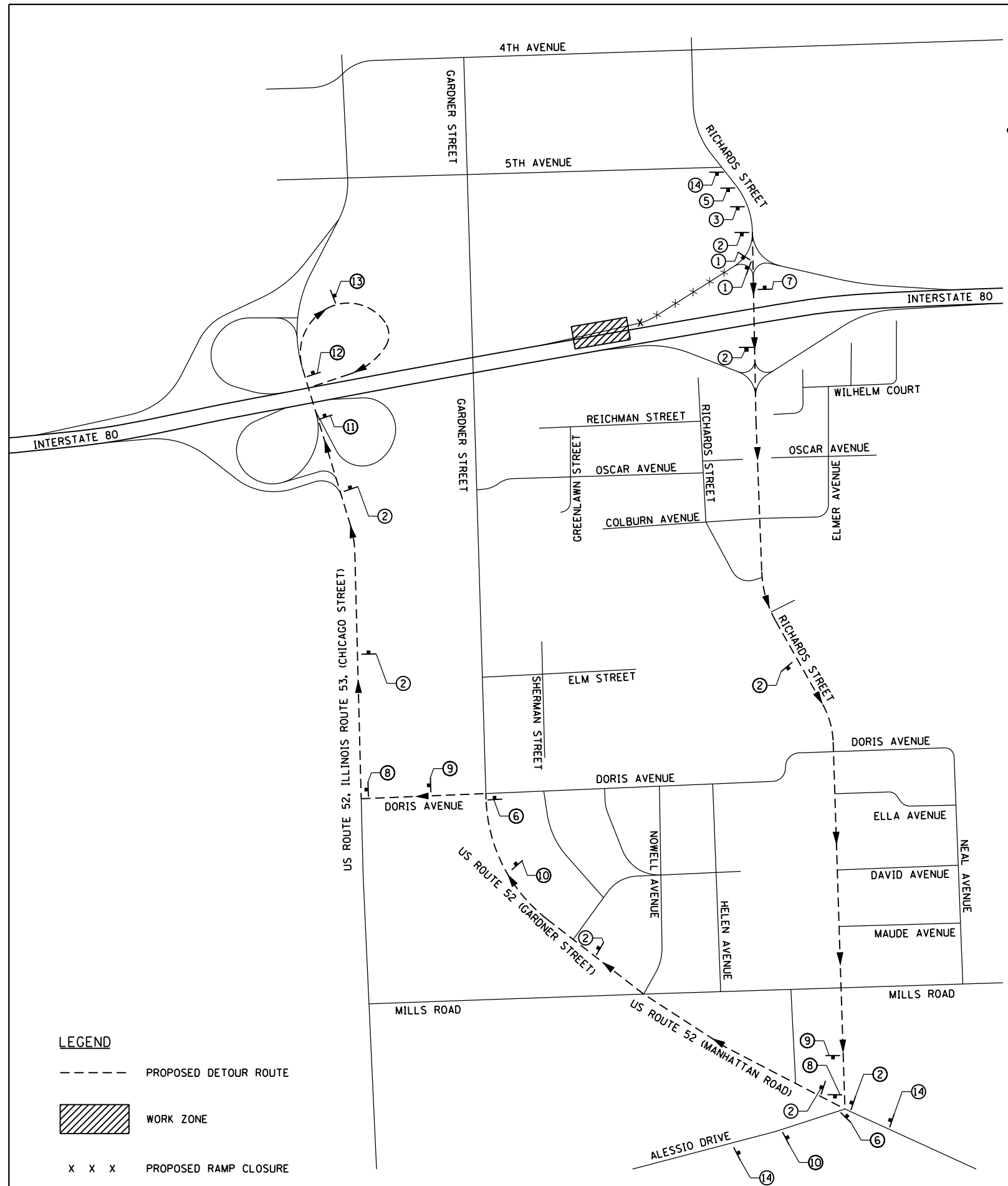
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	DATE - 01/31/2020	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETOUR PLAN - STAGE 3B, 3C AND 3D  
I-80 FROM CENTER STREET TO ROWELL AVENUE**

SCALE: 700:0.0000 '"/ SHEET 1 OF 4 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	13
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				



**SIGN LEGEND**

- |   |   |
|---|---|
| <p>①  11.2' x 11.8'</p> <p>③  W20-30(O)-48</p> <p>⑤  W20-1103(O)-48</p> <p>⑦  R3-2-3030</p> <p>⑨  M4-8 24''x12''<br/>  M3-4 24''x12''<br/>  M1-1 24''x 24''<br/>  M5-1R 21''x15''</p> <p>⑩  M4-8 24''x12''<br/>  M3-4 24''x12''<br/>  M1-1 24''x 24''<br/>  M5-1L 21''x15''</p> <p>⑫  M4-8 24''x12''<br/>  M3-4 24''x12''<br/>  M1-1 24''x 24''<br/>  M6-2R 21''x15''</p> | <p>②  M4-8 24''x12''<br/>  M3-4 24''x12''<br/>  M1-1 24''x 24''<br/>  M6-3 21''x15''</p> <p>④  (48''x30'')<br/>MOUNTED ON<br/>TYPE III<br/>BARRICADE</p> <p>⑥  M4-8 24''x12''<br/>  M3-4 24''x12''<br/>  M1-1 24''x 24''<br/>  M6-1L 21''x15''</p> <p>⑧  M4-8 24''x12''<br/>  M3-4 24''x12''<br/>  M1-1 24''x 24''<br/>  M6-1R 21''x15''</p> <p>⑪  M4-8 24''x12''<br/>  M3-4 24''x12''<br/>  M1-1 24''x 24''<br/>  M5-2R 21''x15''</p> <p>⑬  M4-8A 24''x18''</p> <p>⑭  W20-2-4848</p> |
|---|---|

SEE DISTRICT 1 STANDARD  
TC-08 FOR DETAILS

BLACK LEGEND ON  
WHITE BACKGROUND  
X (1/2) BORDER  
E 1000 FONT  
THESE BLANK AREAS SHALL BE  
FILLED WITH THE DATES AND THE  
TIME THAT THE RAMP WILL BE  
CLOSED.

**NOTE:**

1. THE RIGHT AND LEFT TURN LANES ACCESSING THE RAMP CONNECTING RICHARDS ST. TO WESTBOUND I-80 SHALL BE CLOSED TO TRAFFIC DURING STAGE 4A IN ACCORDANCE WITH DISTRICT DETAIL TC-08.
2. THE TEMPORARY DETOUR IS INCLUDED IN THE CONTRACT UNIT PRICE LUMP SUM FOR "TRAFFIC CONTROL AND PROTECTION (SPECIAL)."

**LEGEND**

- PROPOSED DETOUR ROUTE
- WORK ZONE
- x x x PROPOSED RAMP CLOSURE

MODEL: Default  
FILE NAME:



USER NAME = johnn	DESIGNED - AB	REVISED -
	DRAWN - AB	REVISED -
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

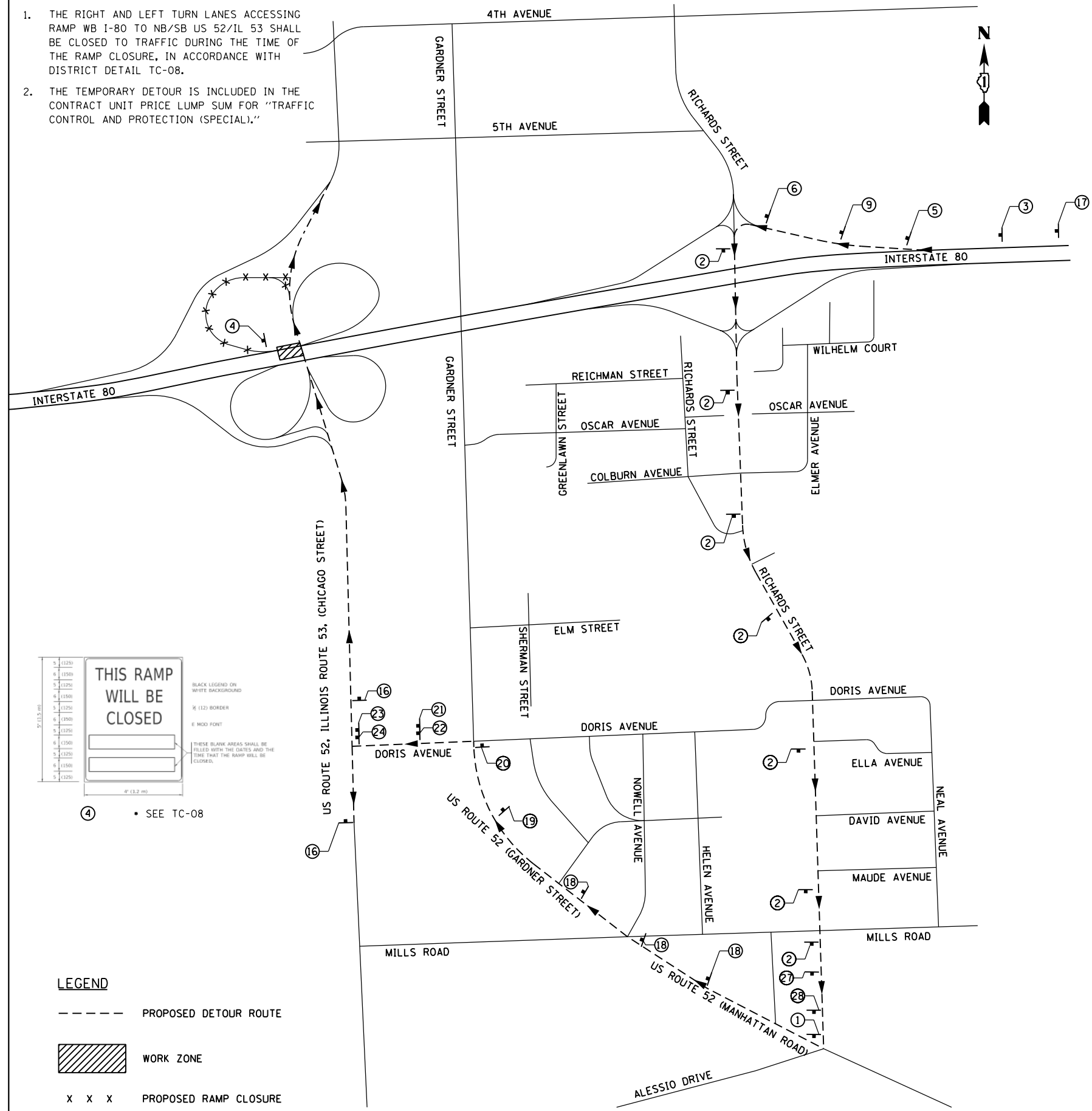
**DETOUR PLAN - STAGE 4A (SB RICHARDS ST. TO WB I-80)  
I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**

SCALE: 100.0000' / 1" SHEET 2 OF 4 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	14
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

NOTE:

1. THE RIGHT AND LEFT TURN LANES ACCESSING RAMP WB I-80 TO NB/SB US 52/IL 53 SHALL BE CLOSED TO TRAFFIC DURING THE TIME OF THE RAMP CLOSURE, IN ACCORDANCE WITH DISTRICT DETAIL TC-08.
2. THE TEMPORARY DETOUR IS INCLUDED IN THE CONTRACT UNIT PRICE LUMP SUM FOR "TRAFFIC CONTROL AND PROTECTION (SPECIAL)."



**THIS RAMP WILL BE CLOSED**

BLACK LEGEND ON WHITE BACKGROUND  
 1/2" BORDER  
 6 MOD FONT

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

④ • SEE TC-08

**LEGEND**

--- PROPOSED DETOUR ROUTE

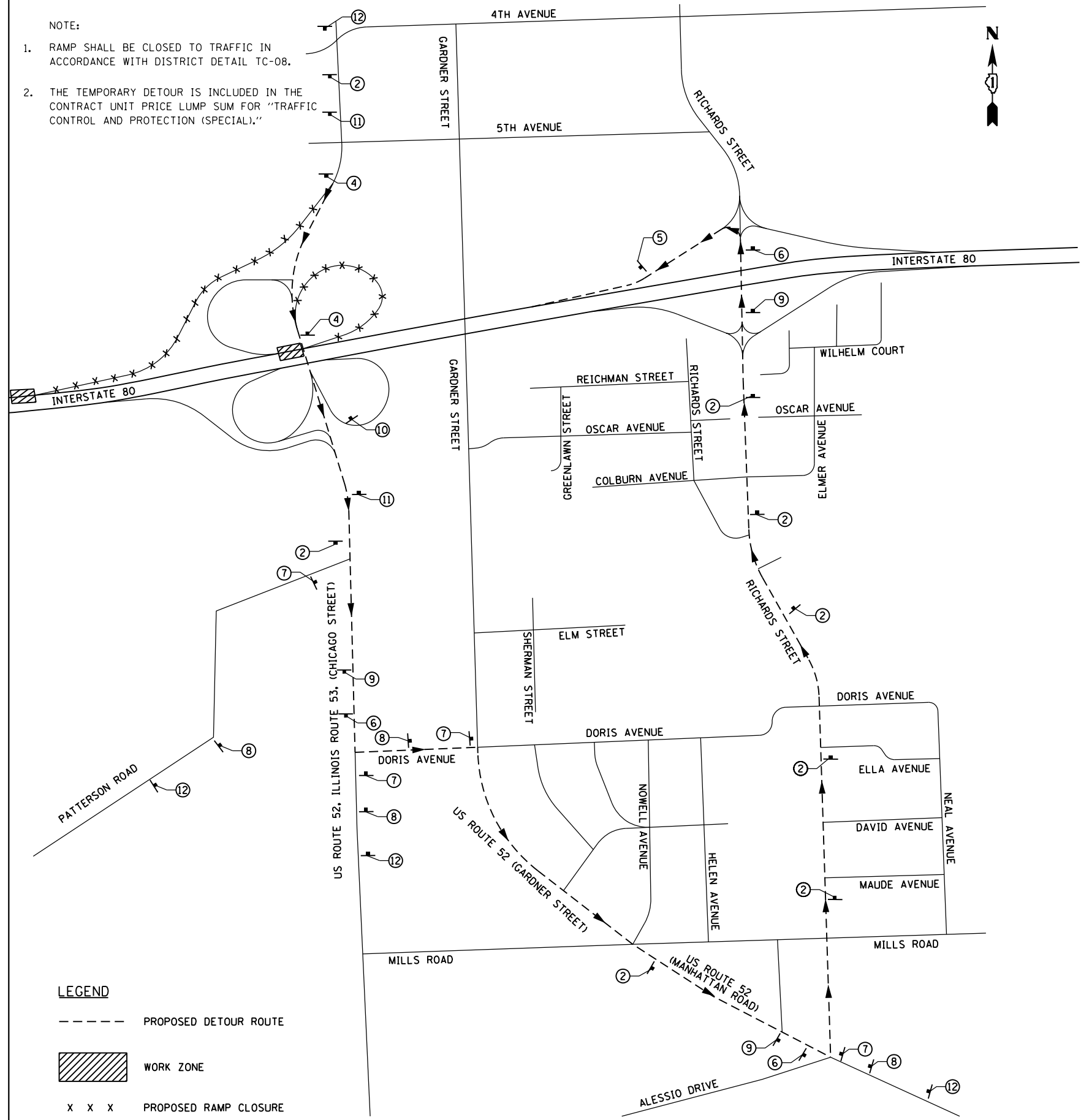
▨ WORK ZONE

X X X PROPOSED RAMP CLOSURE

**SIGN LEGEND**

- |   |  |  |   |  |  |   |  |  |
|---|--|--|---|--|--|---|--|--|
| ① | END DETOUR<br>52                         | M4-8A 24"x18"<br>M1-4 24"x24"  | ② | DETOUR<br>52<br>ILLINOIS 53<br>↑         | M4-8 24"x12"<br>M1-4 24"x24"<br>M1-1100 24"x24"<br>M6-3 21"x15"                                  | ③ | DETOUR<br>52<br>ILLINOIS 53<br>↗         | M4-8 24"x12"<br>M1-4 24"x24"<br>M1-I100 24"x24"<br>M5-2R 21"x15"                                 |
| ④ | • SEE TC-08                              |  | ⑤ | DETOUR<br>52<br>ILLINOIS 53<br>↘         | M4-8 24"x12"<br>M1-4 24"x24"<br>M1-1100 24"x24"<br>M6-2R 21"x15"                                 | ⑥ | DETOUR<br>52<br>ILLINOIS 53<br>←         | M4-8 24"x12"<br>M1-4 24"x24"<br>M1-1100 24"x24"<br>M6-1L 21"x15"                                 |
| ⑦ | DETOUR<br>WEST<br>52<br>NORTH<br>53<br>→ | M4-8 24"x12"<br>M3-4 24"x12"<br>M1-4 24"x24"<br>M3-1 24"x12"<br>M1-I100 24"x24"<br>M6-1R 21"x15" | ⑧ | DETOUR<br>WEST<br>52<br>NORTH<br>53<br>↗ | M4-8 24"x12"<br>M3-4 24"x12"<br>M1-4 24"x24"<br>M3-1 24"x12"<br>M1-1100 24"x24"<br>M5-1R 21"x15" | ⑨ | DETOUR<br>52<br>ILLINOIS 53<br>↖         | M4-8 24"x12"<br>M1-4 24"x24"<br>M1-I100 24"x24"<br>M5-1L 21"x15"                                 |
| ⑩ | DETOUR<br>52<br>ILLINOIS 53<br>↗         | M4-8 24"x12"<br>M1-4 24"x24"<br>M5-1R 21"x15"  | ⑪ | DETOUR<br>52<br>ILLINOIS 53<br>→         | M4-8 24"x12"<br>M1-4 24"x24"<br>M1-1100 24"x24"<br>M6-1R 21"x15"                                 | ⑫ | DETOUR<br>WEST<br>52<br>NORTH<br>53<br>↑ | M4-8 24"x12"<br>M3-4 24"x12"<br>M1-4 24"x24"<br>M3-1 24"x12"<br>M1-I100 24"x24"<br>M6-3 21"x15"  |
| ⑬ | DETOUR<br>EAST<br>52<br>SOUTH<br>53<br>↑ | M4-8 24"x12"<br>M3-2 24"x12"<br>M1-4 24"x24"<br>M3-3 24"x12"<br>M1-I100 24"x24"<br>M6-3 21"x15"  | ⑭ | DETOUR<br>EAST<br>52<br>SOUTH<br>53<br>← | M4-8 24"x12"<br>M3-2 24"x12"<br>M1-4 24"x24"<br>M3-3 24"x12"<br>M1-I100 24"x24"<br>M6-1L 21"x15" | ⑮ | DETOUR<br>EAST<br>52<br>SOUTH<br>53<br>↖ | M4-8 24"x12"<br>M3-2 24"x12"<br>M1-4 24"x24"<br>M3-3 24"x12"<br>M1-I100 24"x24"<br>M5-1L 21"x15" |
| ⑯ | END DETOUR                               | M4-8A 24"x18"  | ⑰ | DETOUR AHEAD                             | W20-2-4848   | ⑱ | DETOUR<br>53<br>↑                        | M4-8 24"x12"<br>M1-I100 24"x24"<br>M6-3 21"x15"  |
| ⑲ | DETOUR<br>53<br>↖                        | M4-8 24"x12"<br>M1-I100 24"x24"<br>M5-1L 21"x15"   | ⑳ | DETOUR<br>53<br>←                        | M4-8 24"x12"<br>M1-I100 24"x24"<br>M6-1L 21"x15"   | ㉑ | DETOUR<br>NORTH<br>53<br>↗               | M4-8 24"x12"<br>M3-1 24"x12"<br>M1-I100 24"x24"<br>M5-1R 21"x15"                                 |
| ㉒ | DETOUR<br>SOUTH<br>53<br>↖               | M4-8 24"x12"<br>M3-3 24"x12"<br>M1-I100 24"x24"<br>M5-1L 21"x15"                                 | ㉓ | DETOUR<br>NORTH<br>53<br>→               | M4-8 24"x12"<br>M3-1 24"x12"<br>M1-I100 24"x24"<br>M6-1R 21"x15"                                 | ㉔ | DETOUR<br>SOUTH<br>53<br>←               | M4-8 24"x12"<br>M3-3 24"x12"<br>M1-I100 24"x24"<br>M6-1L 21"x15"                                 |
| ㉕ | DETOUR<br>SOUTH<br>53<br>↑               | M4-8 24"x12"<br>M3-3 24"x12"<br>M1-I100 24"x24"<br>M6-3 21"x15"                                  | ㉖ | DETOUR<br>NORTH<br>53<br>↑               | M4-8 24"x12"<br>M3-1 24"x12"<br>M1-I100 24"x24"<br>M6-3 21"x15"                                  | ㉗ | DETOUR<br>53<br>↗                        | M4-8 24"x12"<br>M1-I100 24"x24"<br>M5-1R 21"x15"   |
| ㉘ | DETOUR<br>53<br>→                        | M4-8 24"x12"<br>M1-I100 24"x24"<br>M6-1R 21"x15"   |   |  |  |   |  |  |

NOTE:  
 1. RAMP SHALL BE CLOSED TO TRAFFIC IN ACCORDANCE WITH DISTRICT DETAIL TC-08.  
 2. THE TEMPORARY DETOUR IS INCLUDED IN THE CONTRACT UNIT PRICE LUMP SUM FOR "TRAFFIC CONTROL AND PROTECTION (SPECIAL)."



**LEGEND**  
 - - - - - PROPOSED DETOUR ROUTE  
 [Hatched Box] WORK ZONE  
 X X X PROPOSED RAMP CLOSURE

**SIGN LEGEND**

- ① M4-8 24"x12"  
M3-4 24"x12"  
M1-1 24"x 24"  
M6-2L 21"x15"
- ② M4-8 24"x12"  
M3-4 24"x12"  
M1-1 24"x24"  
M6-3 21"x15"
- ③ M4-8 24"x12"  
M3-4 24"x12"  
M1-1 24"x 24"  
M5-2L 21"x15"
- ④ SEE DISTRICT 1 STANDARD TC-08 FOR DETAILS
- ⑤ M4-8A 24"x18"
- ⑥ M4-8 24"x12"  
M3-4 24"x12"  
M1-1 24"x24"  
M6-1L 21"x15"
- ⑦ M4-8 24"x12"  
M3-4 24"x12"  
M1-1 24"x24"  
M6-1R 21"x15"
- ⑧ M4-8 24"x12"  
M3-4 24"x12"  
M1-1 24"x24"  
M5-1R 21"x15"
- ⑨ M4-8 24"x12"  
M3-4 24"x12"  
M1-1 24"x24"  
M5-1L 21"x15"
- ⑩ W20-30(0)-48
- ⑪ W20-1103(0)-48
- ⑫ W20-2-4848

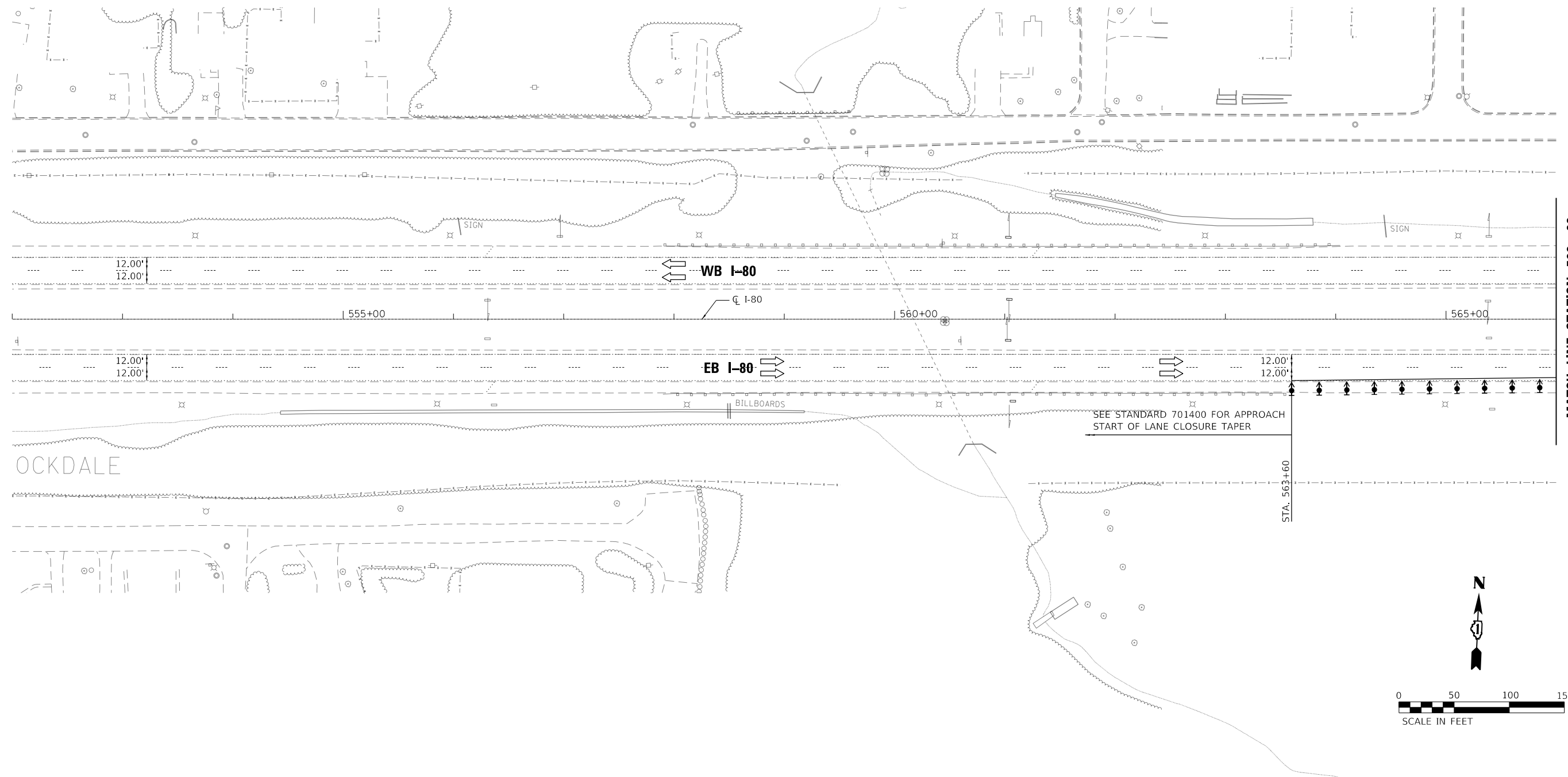
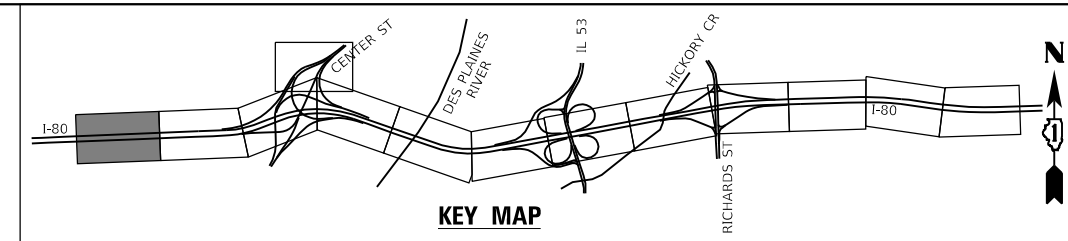


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PLOT SCALE = 100.0000' / in.	DRAWN - AB	REVISED -
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	DATE - 01/31/2020	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

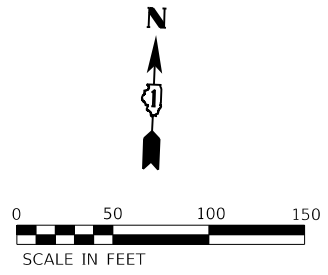
**DETOUR PLAN - STAGE 4B (WESTBOUND I-80)  
 I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**  
 SCALE: 100.0000' / in SHEET 4 OF 4 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	16
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				



ROCKDALE

MATCH LINE STATION 566 + 00



**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE

- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

- DIRECTION OF TRAFFIC
- ARROW BOARD

- CONSTRUCTION SIGN
- SPOTTER

- TYPE III BARRICADES WITH FLASHING LIGHTS



USER NAME = ken.drabant	DESIGNED - KJD, AMI	REVISED -
PLOT SCALE = 100:0.0000 " = 1" / in.	DRAWN - KJD, AMI	REVISED -
PLOT DATE = 2/3/2020	CHECKED - RTB	REVISED -
	DATE - 01/31/2020	REVISED -

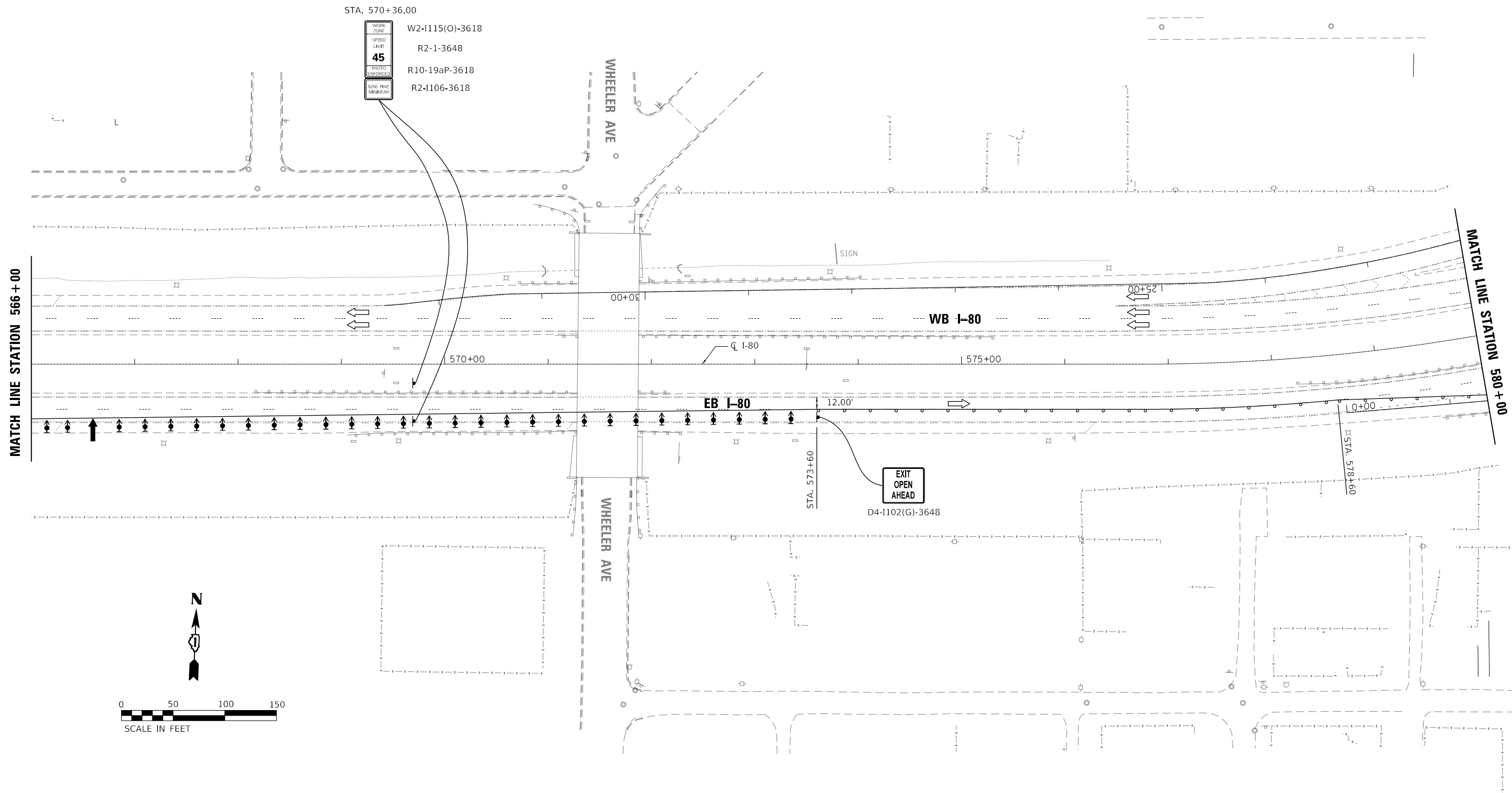
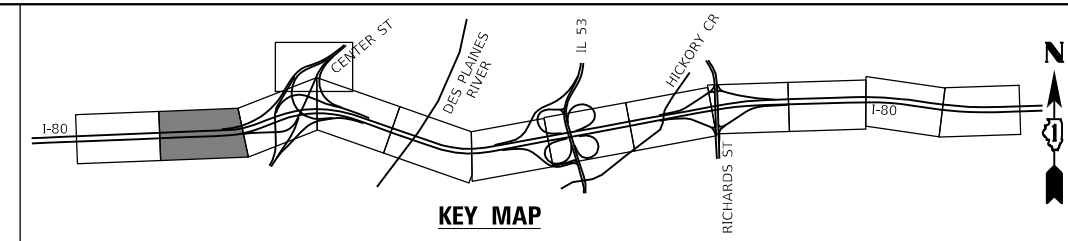
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC - STAGE 3A  
I-80 FROM CENTER STREET TO ROWELL AVENUE**

SCALE: 100:0.0000 " = 1" / in. SHEET 1 OF 4 SHEETS STA. 552+00 TO STA. 566+00

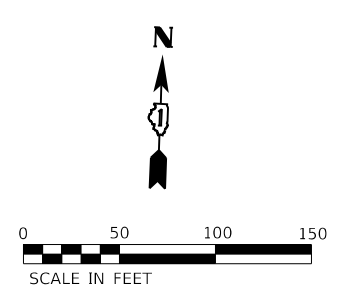
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	17
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

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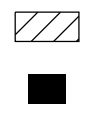


STA. 570+36.00  
 WORK ZONE W2-1115(O)-3618  
 SPEED LIMIT 45 R2-1-3648  
 PHOTO ENFORCEMENT R10-19aP-3618  
 8210 PINE WHEELING R2-1106-3618

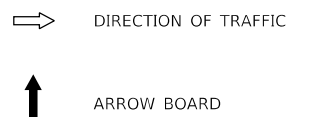
EXIT OPEN AHEAD  
 D4-1102(G)-3648



**MOT PLAN LEGEND**



↑ DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS  
 ○ TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)



USER NAME = ken.drabant  
 PLOT SCALE = 100:0.0000 " = 1" / in.  
 PLOT DATE = 2/3/2020

DESIGNED - KJD, AMI  
 DRAWN - KJD, AMI  
 CHECKED - RTB  
 DATE - 01/31/2020

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

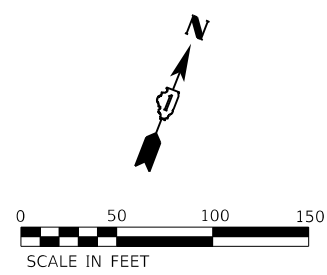
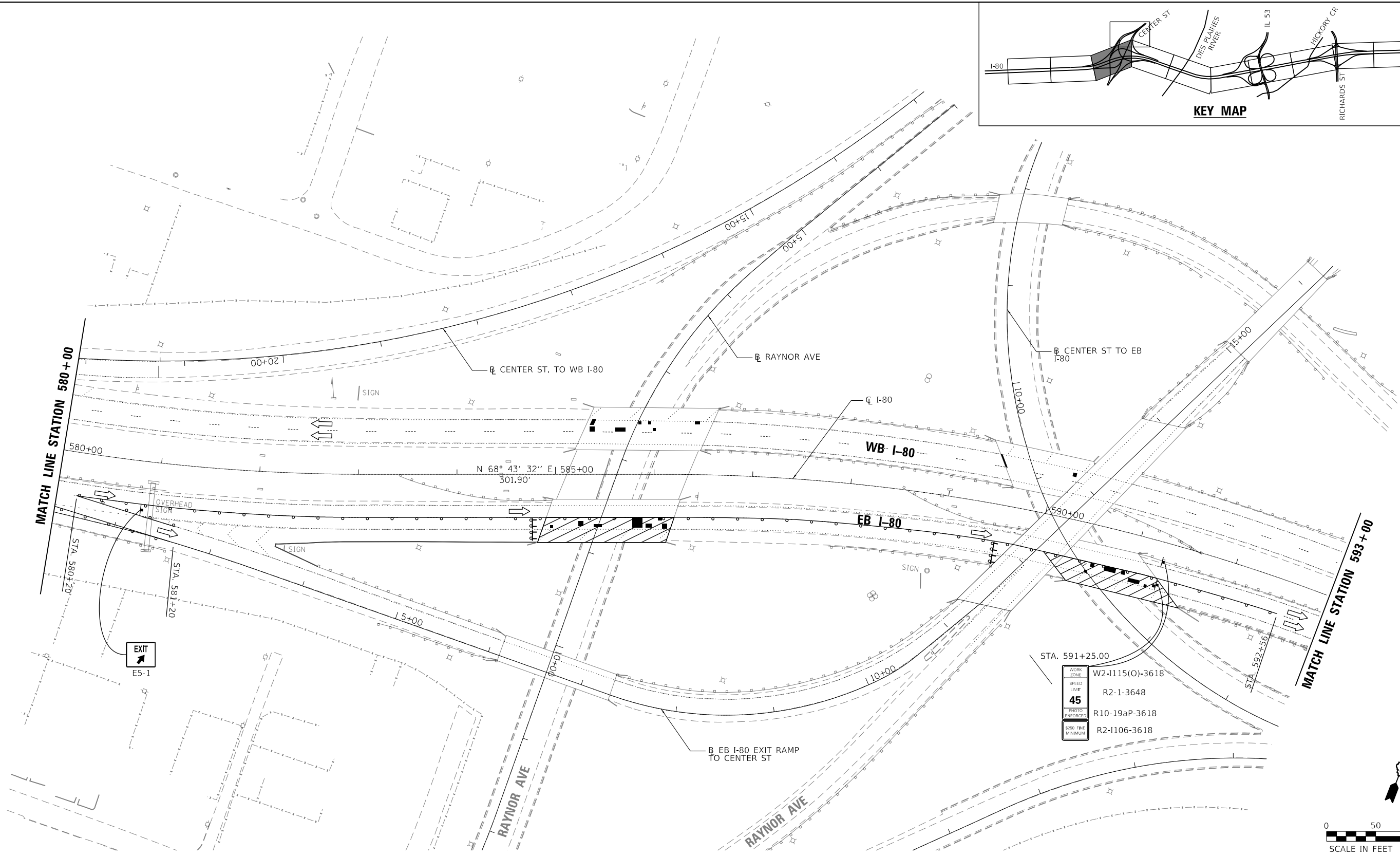
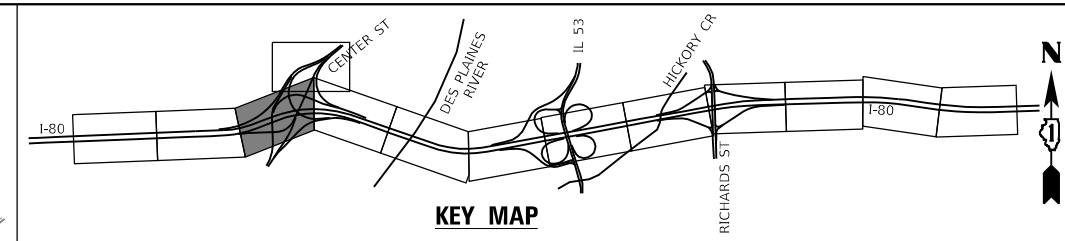
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC - STAGE 3A  
 I-80 FROM CENTER STREET TO ROWELL AVENUE

SCALE: 100:0.0000 " = 1" / in. SHEET 2 OF 4 SHEETS STA. 566+00 TO STA. 580+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	18
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

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**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE

- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

- DIRECTION OF TRAFFIC
- ARROW BOARD

- CONSTRUCTION SIGN
- SPOTTER

- TYPE III BARRICADES WITH FLASHING LIGHTS



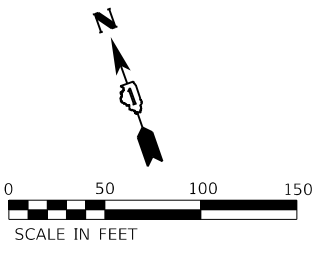
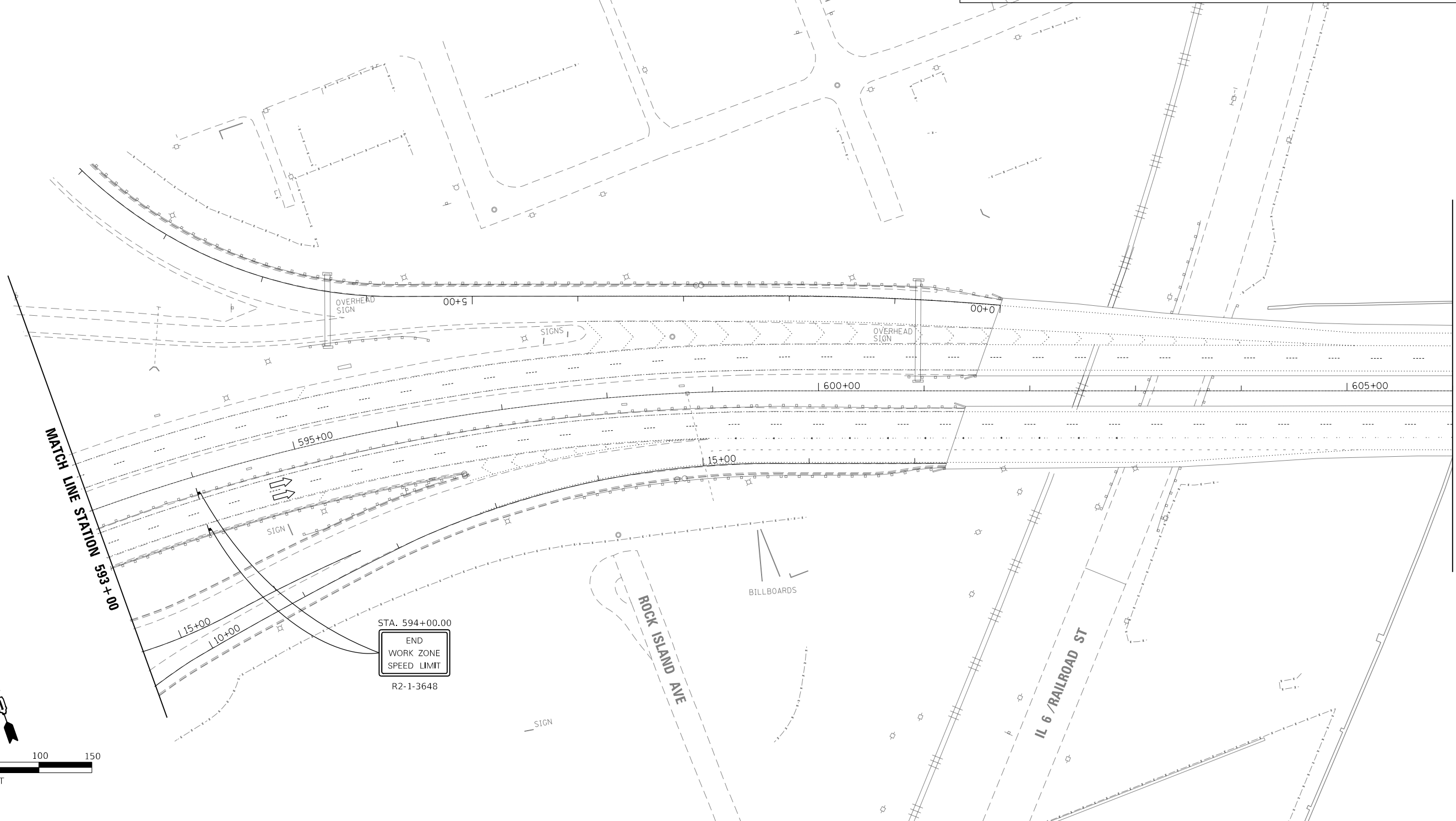
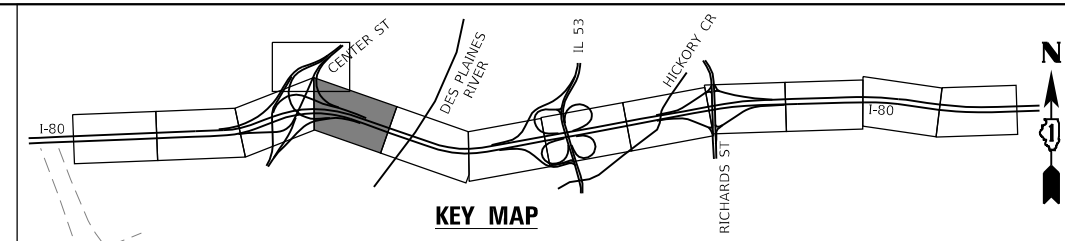
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PLOT DATE = 2/3/2020	DATE - 01/31/2020	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>MAINTENANCE OF TRAFFIC - STAGE 3A</b>			
<b>I-80 FROM CENTER STREET TO ROWELL AVENUE</b>			
SCALE: 100:0.0000 "	SHEET 3	OF 4 SHEETS	STA. 580+00 TO STA. 593+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	19
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

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**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE
- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)
- DIRECTION OF TRAFFIC
- ARROW BOARD
- CONSTRUCTION SIGN
- SPOTTER
- TYPE III BARRICADES WITH FLASHING LIGHTS

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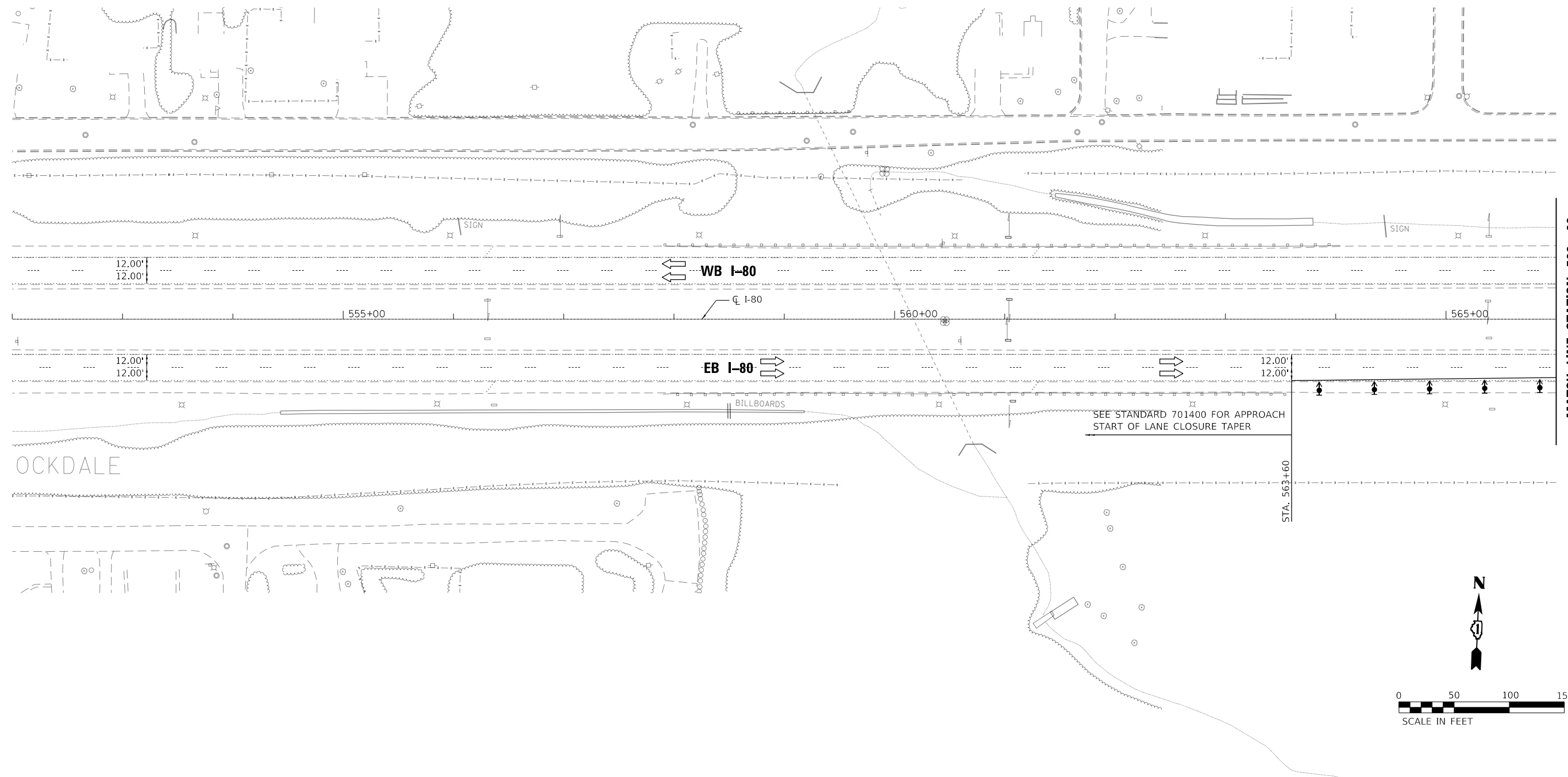
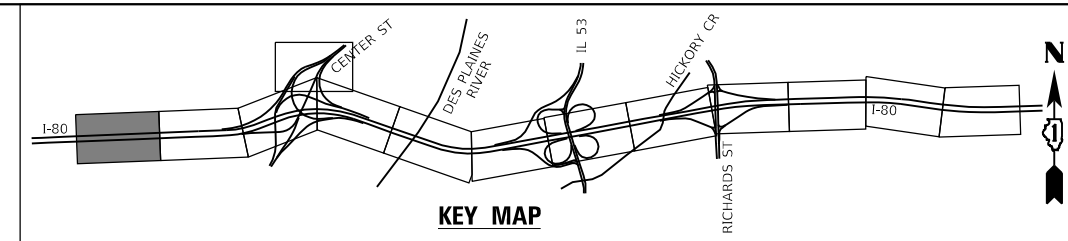
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PLOT DATE = 2/3/2020	DATE - 01/31/2020	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC - STAGE 3A**  
**I-80 FROM CENTER STREET TO ROWELL AVENUE**  
 SCALE: 100:0.0000 " = 1" / in. SHEET 4 OF 4 SHEETS STA. 593+00 TO STA. 606+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	20
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

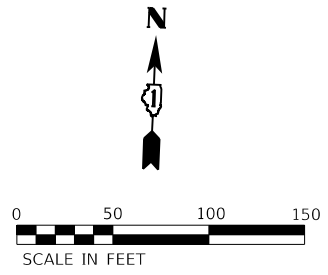




ROCKDALE

MATCH LINE STATION 566 + 00

STA. 563+60



**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE

- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

- DIRECTION OF TRAFFIC
- ARROW BOARD

- CONSTRUCTION SIGN
- SPOTTER

- TYPE III BARRICADES WITH FLASHING LIGHTS



USER NAME = ken.drabant	DESIGNED - KJD, AMI	REVISED -
PLOT SCALE = 100:0.0000 '"/>		
PLOT DATE = 2/3/2020	DATE - 01/31/2020	REVISED -

DRAWN - KJD, AMI	REVISED -
CHECKED - RTB	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

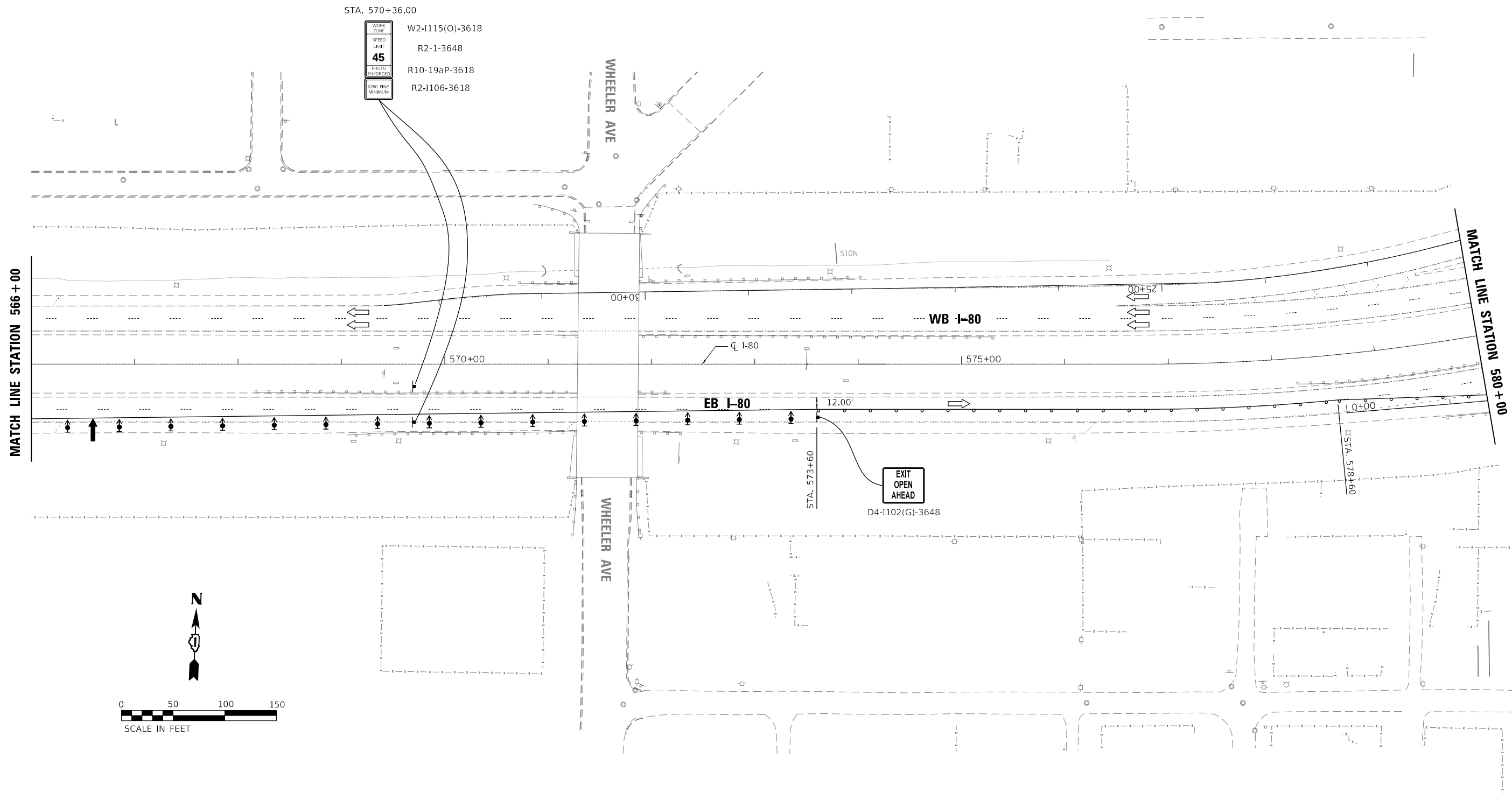
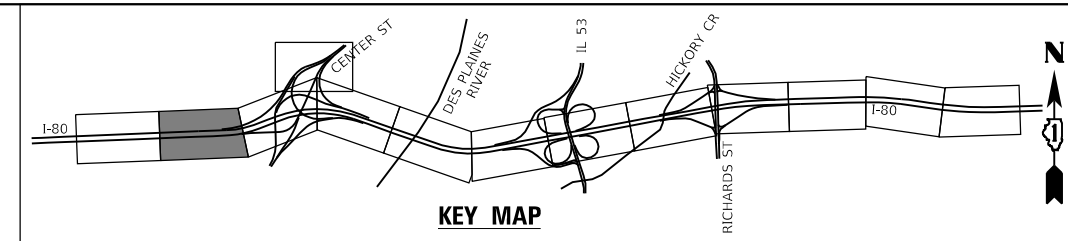
**MAINTENANCE OF TRAFFIC – STAGE 3B  
I-80 FROM CENTER STREET TO ROWELL AVENUE**

SCALE: 100:0.0000 '"/>

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62K34				

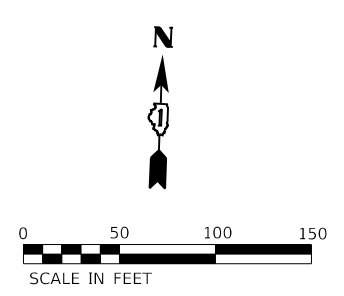
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ILLINOIS FED. AID PROJECT



STA. 570+36.00  
 WORK ZONE W2-1115(O)-3618  
 SPEED LIMIT 45 R2-1-3648  
 PHOTO ENFORCEMENT R10-19aP-3618  
 8210 PINE W/WORKING R2-1106-3618

EXIT OPEN AHEAD  
 D4-1102(G)-3648



**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE

- DIRECTION INDICATOR BARRICIDE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

- DIRECTION OF TRAFFIC
- ARROW BOARD

- CONSTRUCTION SIGN
- SPOTTER

- TYPE III BARRICADES WITH FLASHING LIGHTS



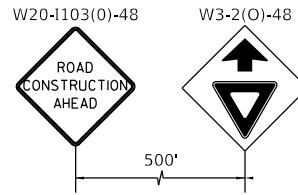
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

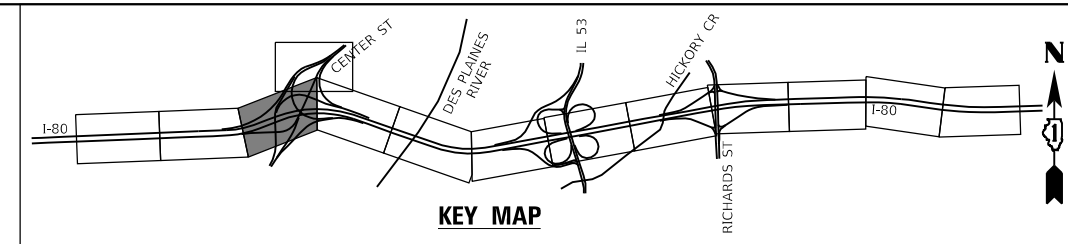
<b>MAINTENANCE OF TRAFFIC – STAGE 3B</b>			
<b>I-80 FROM CENTER STREET TO ROWELL AVENUE</b>			
SCALE: 100:0.0000 " = 1"	SHEET 2	OF 8 SHEETS	STA. 566+00 TO STA. 580+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	22
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

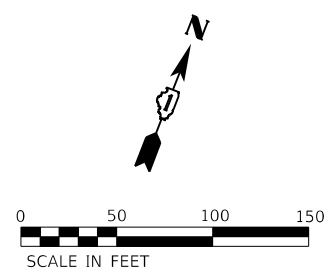
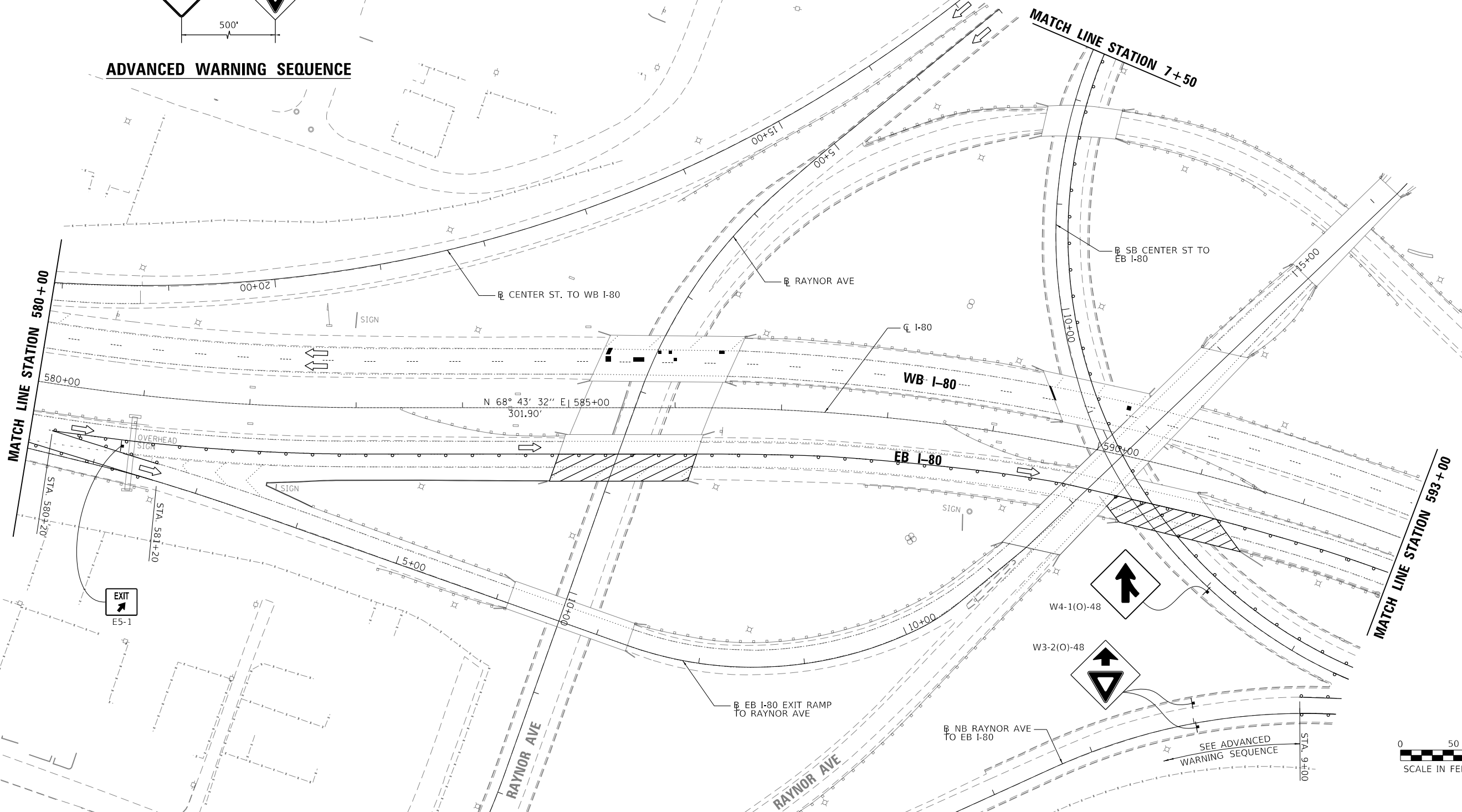
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**ADVANCED WARNING SEQUENCE**



**KEY MAP**



**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE

- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

- DIRECTION OF TRAFFIC
- ARROW BOARD

- CONSTRUCTION SIGN
- SPOTTER

- TYPE III BARRICADES WITH FLASHING LIGHTS



USER NAME	= ken.drabant
PLOT SCALE	= 100:0.0000 " = 1" / in.
PLOT DATE	= 2/3/2020

DESIGNED	- KJD, AMI	REVISED	-
DRAWN	- KJD, AMI	REVISED	-
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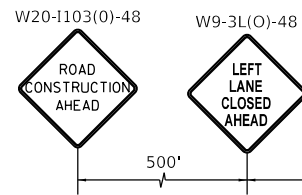
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC – STAGE 3B  
I-80 FROM CENTER STREET TO ROWELL AVENUE**

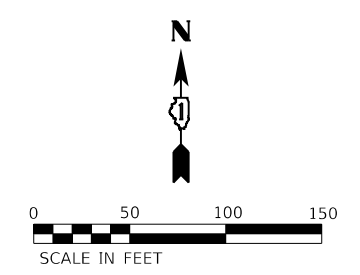
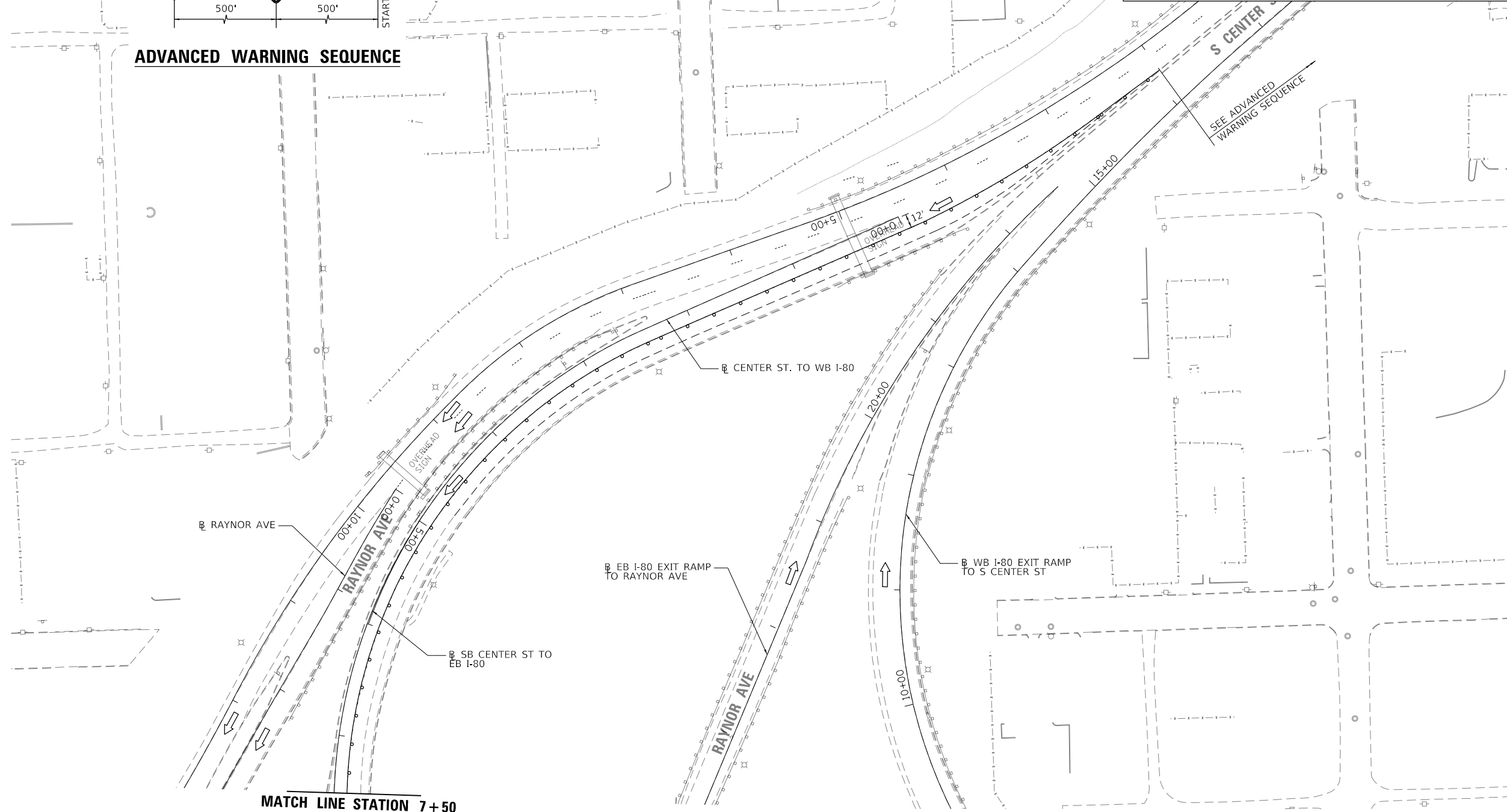
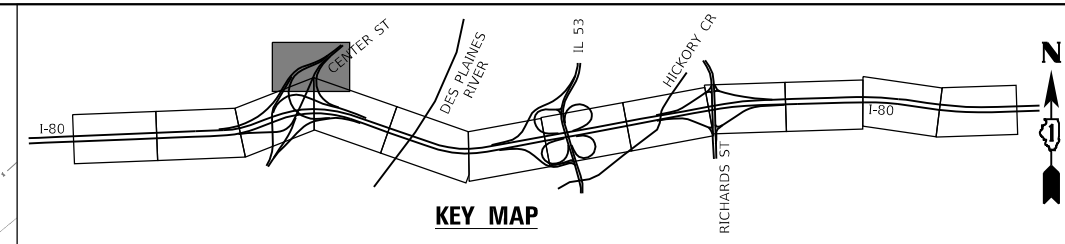
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	23
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

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**ADVANCED WARNING SEQUENCE**



**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE

- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

- DIRECTION OF TRAFFIC
- ARROW BOARD

- CONSTRUCTION SIGN
- SPOTTER

- TYPE III BARRICADES WITH FLASHING LIGHTS

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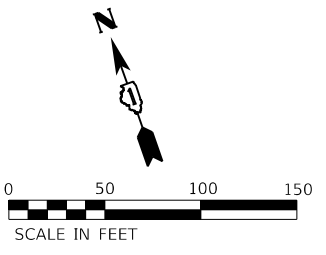
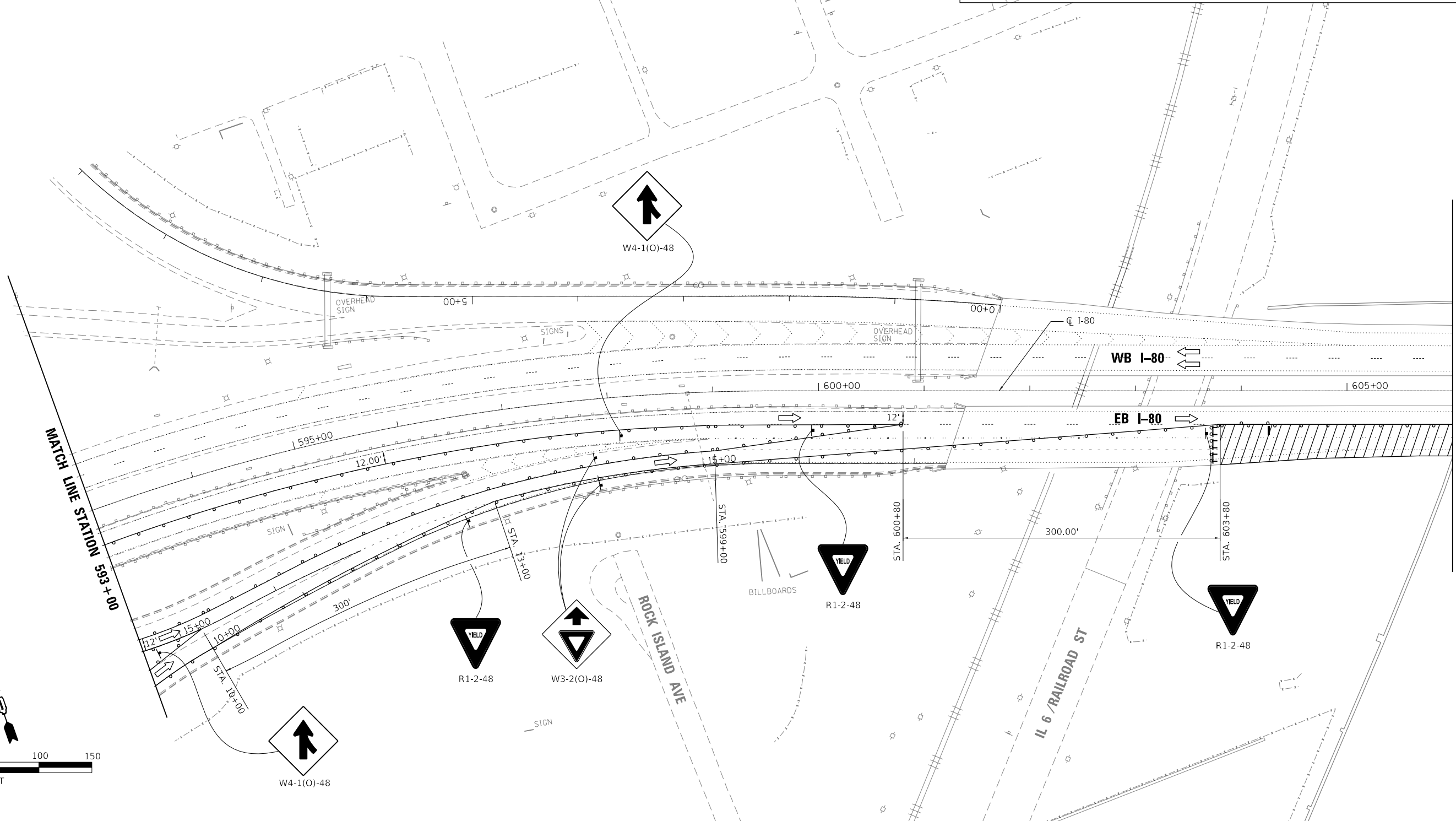
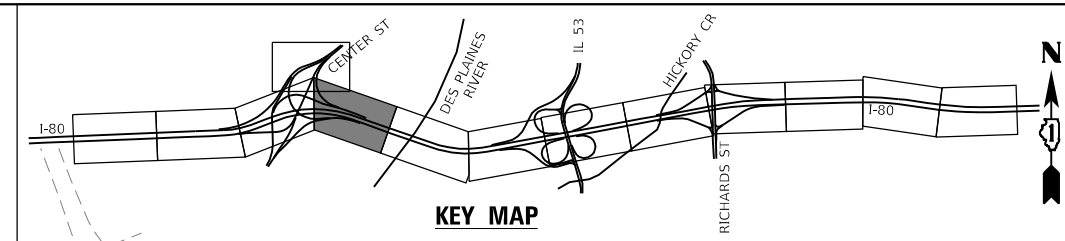
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PLOT DATE = 2/3/2020	DATE - 01/31/2020	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC – STAGE 3B  
I-80 FROM CENTER STREET TO ROWELL AVENUE**

SCALE: 100:0.0000 ' = 1" SHEET 4 OF 8 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	24
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				



**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE
- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)
- DIRECTION OF TRAFFIC
- ARROW BOARD
- CONSTRUCTION SIGN
- SPOTTER
- TYPE III BARRICADES WITH FLASHING LIGHTS



USER NAME = ken.drabant	DESIGNED - KJD, AMI	REVISED -
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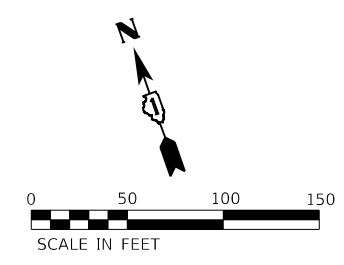
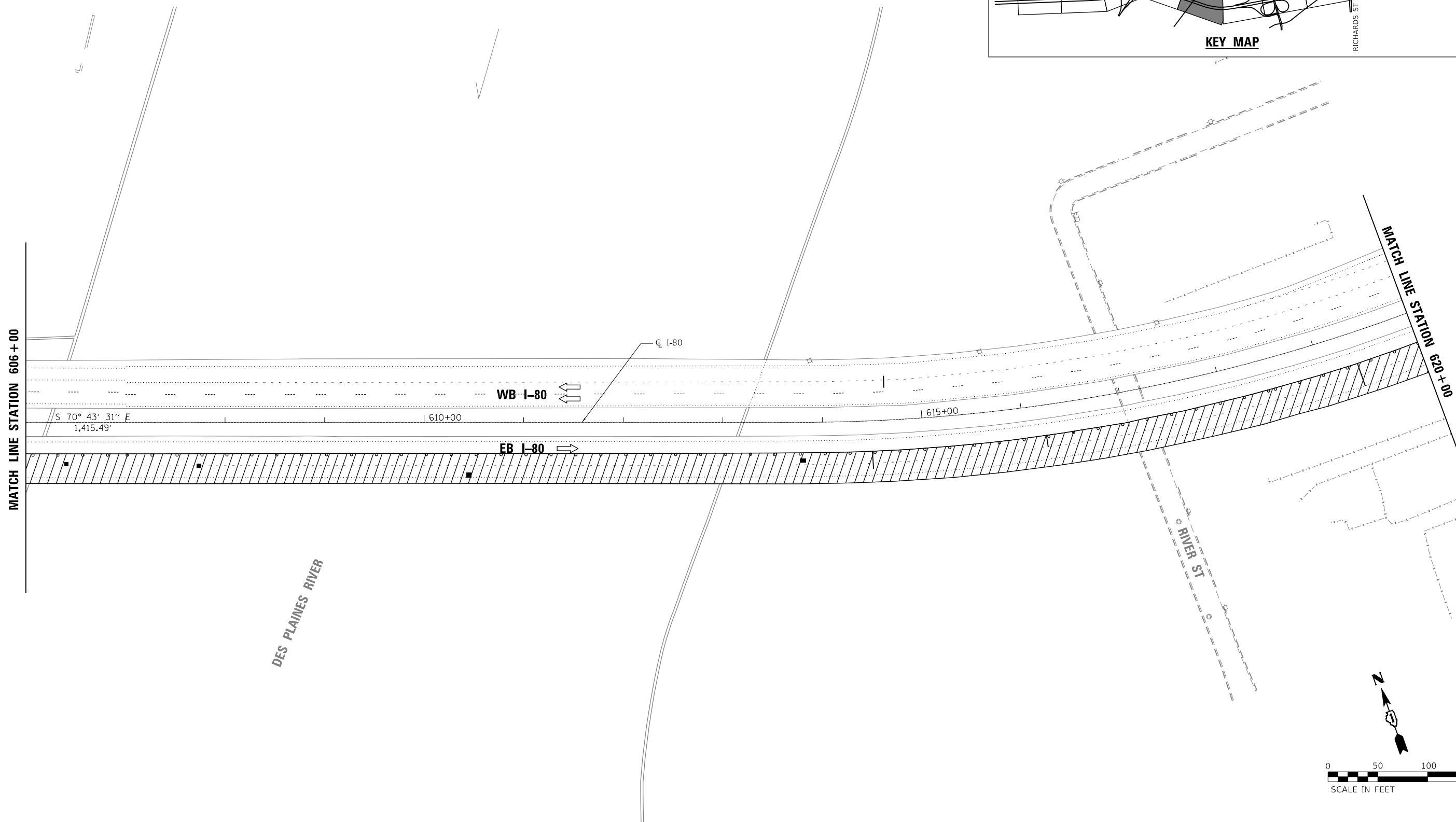
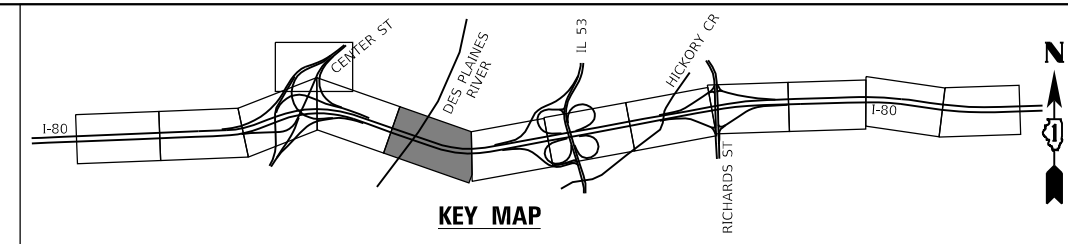
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC – STAGE 3B  
I-80 FROM CENTER STREET TO ROWELL AVENUE**

SCALE: 100:0.0000 " = 1" / in. SHEET 5 OF 8 SHEETS STA. 593+00 TO STA. 606+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	25
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

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**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE

- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

- DIRECTION OF TRAFFIC
- ARROW BOARD

- CONSTRUCTION SIGN
- SPOTTER

- TYPE III BARRICADES WITH FLASHING LIGHTS



USER NAME = ken.drabant	DESIGNED - KJD, AMI	REVISED -
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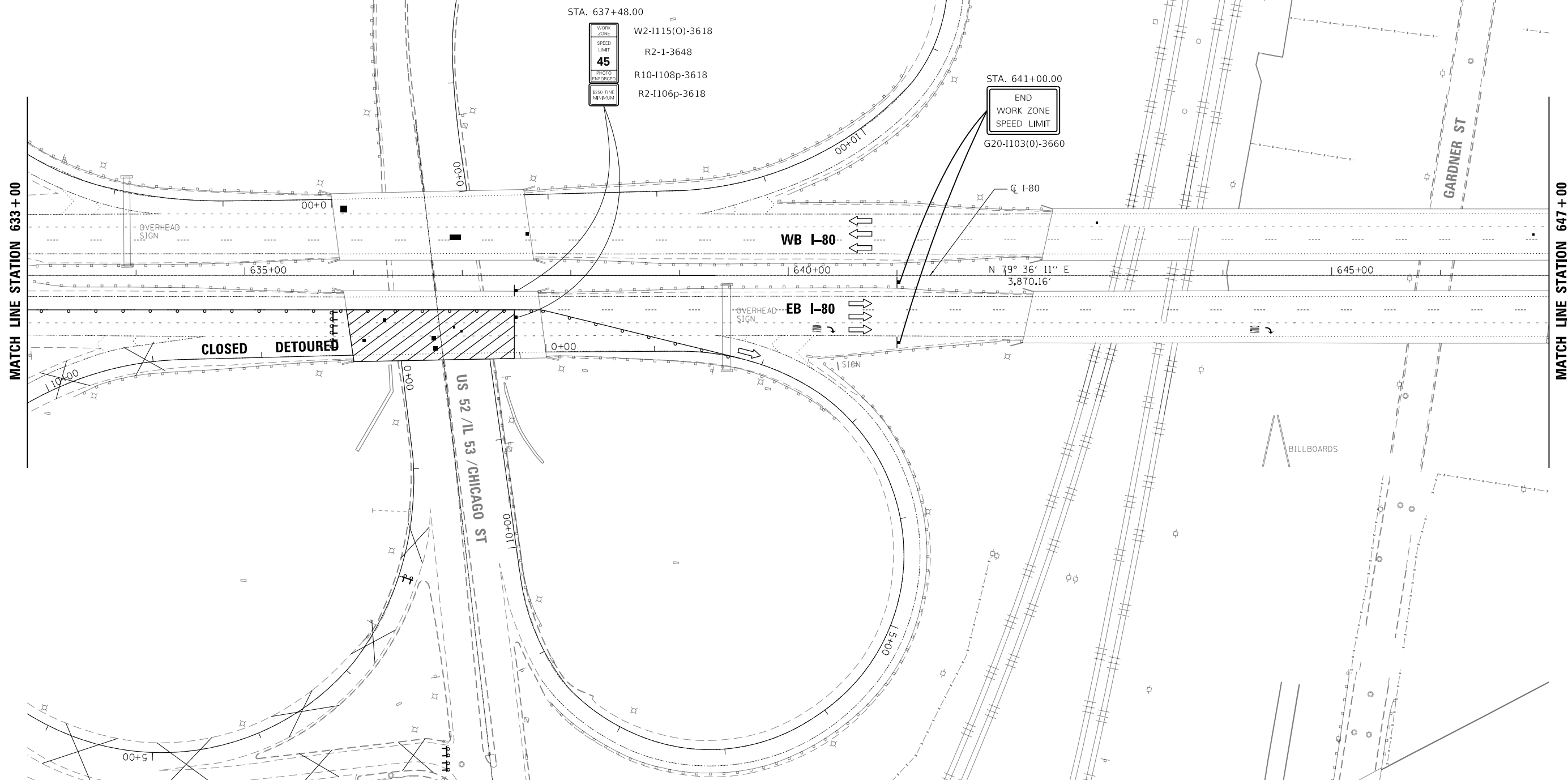
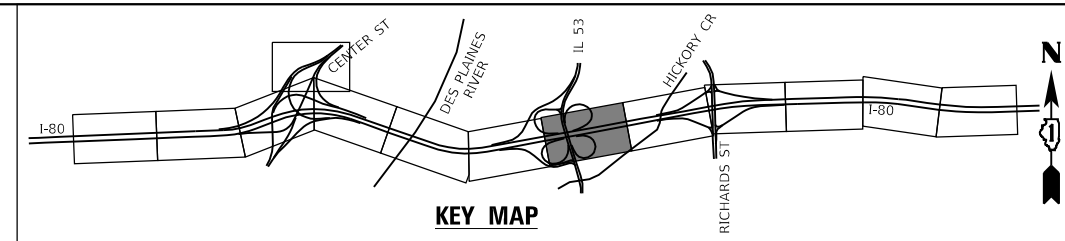
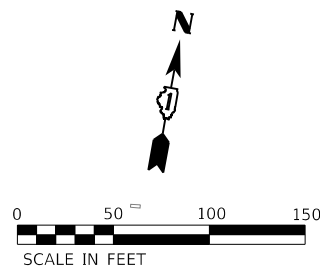
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>MAINTENANCE OF TRAFFIC – STAGE 3B</b>			
<b>I-80 FROM CENTER STREET TO ROWELL AVENUE</b>			
SCALE: 100:0.0000 " = 1"	SHEET 6	OF 8 SHEETS	STA. 606+00 TO STA. 620+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	26
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

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**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE

- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

- DIRECTION OF TRAFFIC
- ARROW BOARD

- CONSTRUCTION SIGN
- SPOTTER

- TYPE III BARRICADES WITH FLASHING LIGHTS



USER NAME	= ken.drabant
DESIGNED	- KJD, AMI
DRAWN	- KJD, AMI
CHECKED	- RTB
DATE	- 01/31/2020

DESIGNED	- KJD, AMI	REVISED	-
DRAWN	- KJD, AMI	REVISED	-
CHECKED	- RTB	REVISED	-
DATE	- 01/31/2020	REVISED	-

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC – STAGE 3B  
I-80 FROM CENTER STREET TO ROWELL AVENUE**

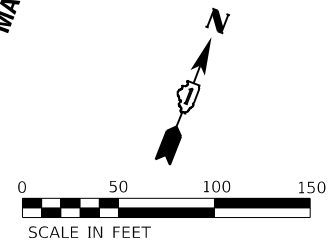
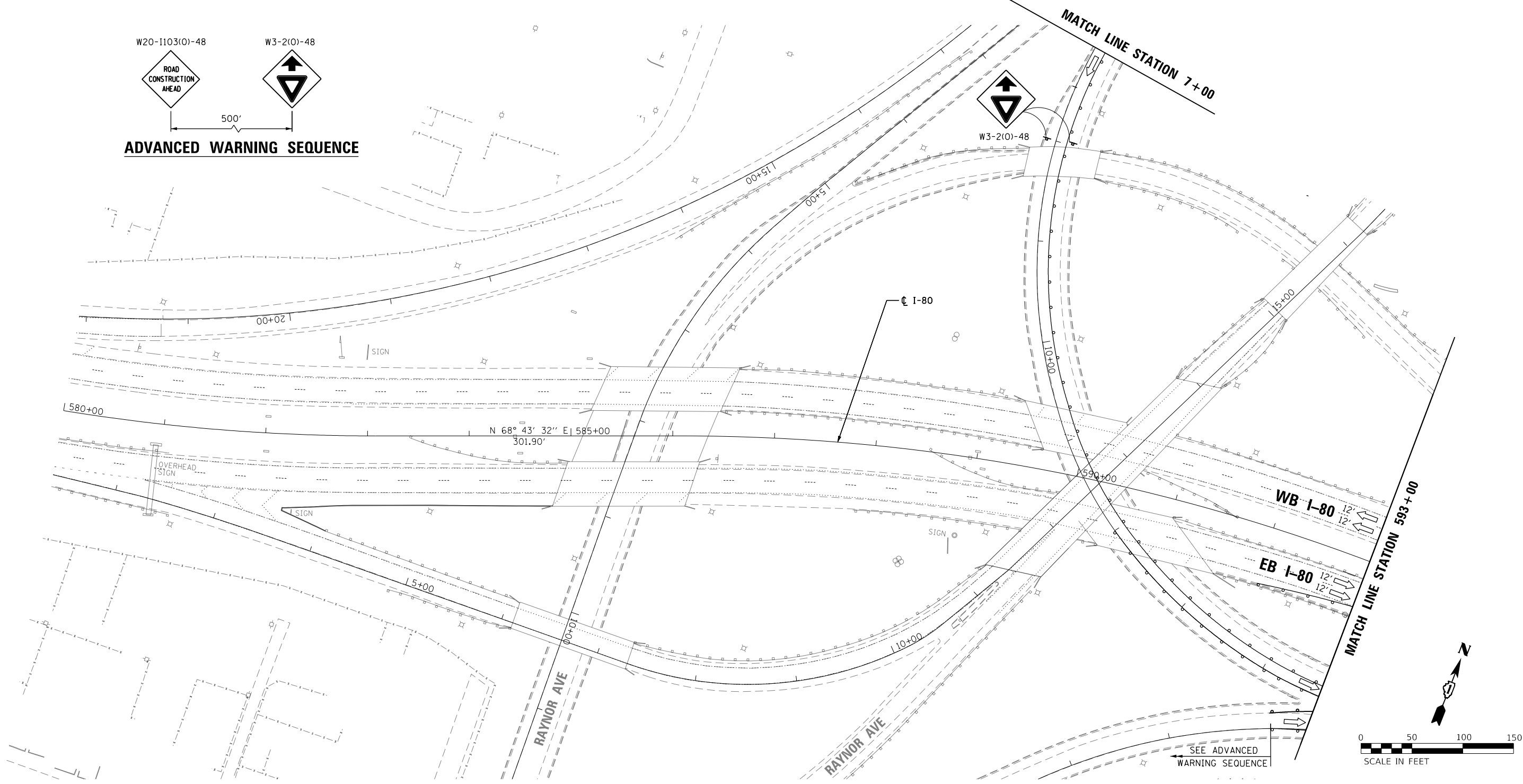
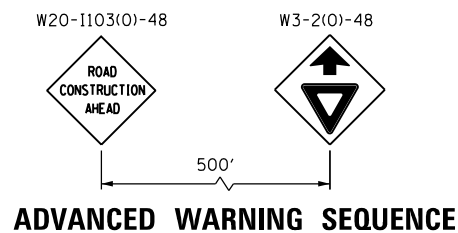
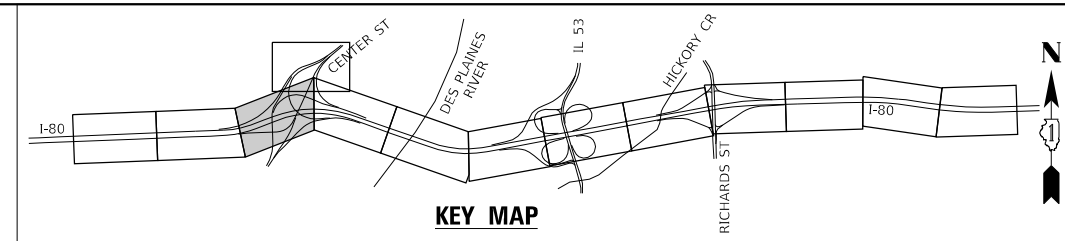
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	28
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

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**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE
- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)
- DIRECTION OF TRAFFIC
- ARROW BOARD
- CONSTRUCTION SIGN
- SPOTTER
- TYPE III BARRICADES WITH FLASHING LIGHTS

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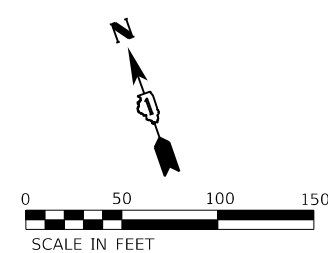
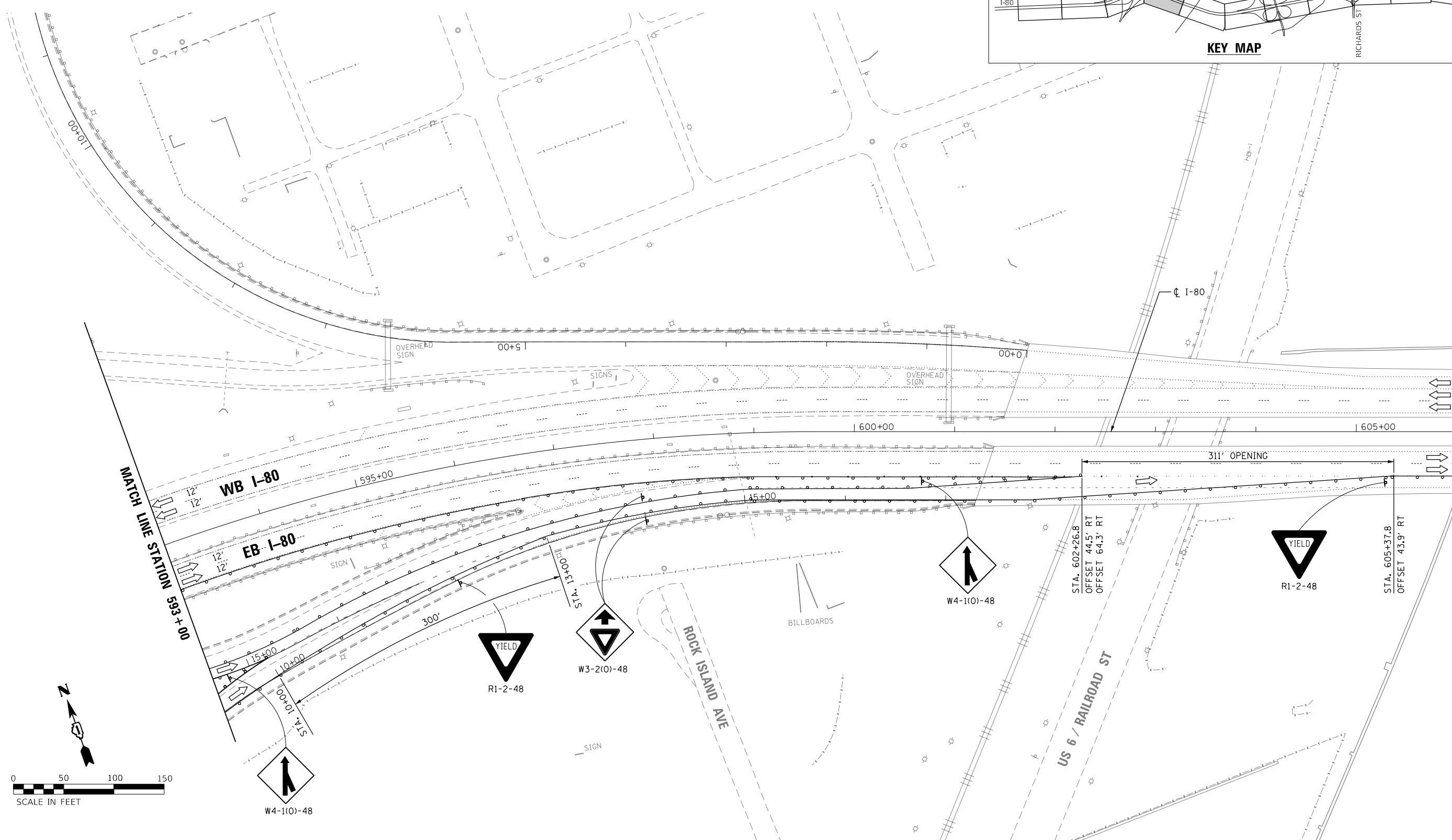
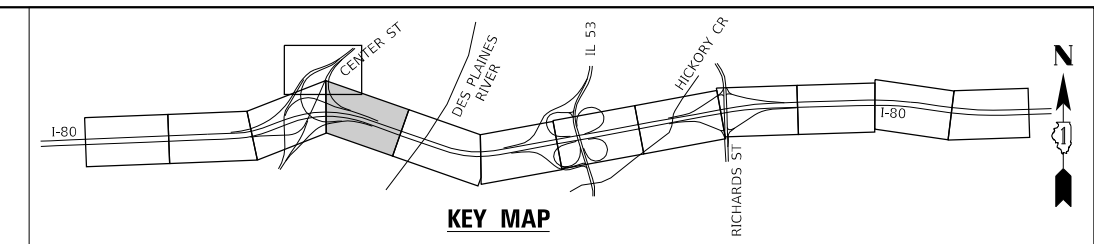


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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>MAINTENANCE OF TRAFFIC - STAGE 3C</b>			
<b>I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE</b>			
SCALE: 100:0.0000	SHEET 2	OF 7 SHEETS	STA. 580+00 TO STA. 593+00

F.A.I. RTE. 80	SECTION 2019-162-BR	COUNTY WILL	TOTAL SHEETS 93	SHEET NO. 30
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				



**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE

- DIRECTION INDICATOR BARRICIDE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

- DIRECTION OF TRAFFIC
- ARROW BOARD

- CONSTRUCTION SIGN
- SPOTTER

- TYPE III BARRICADES WITH FLASHING LIGHTS



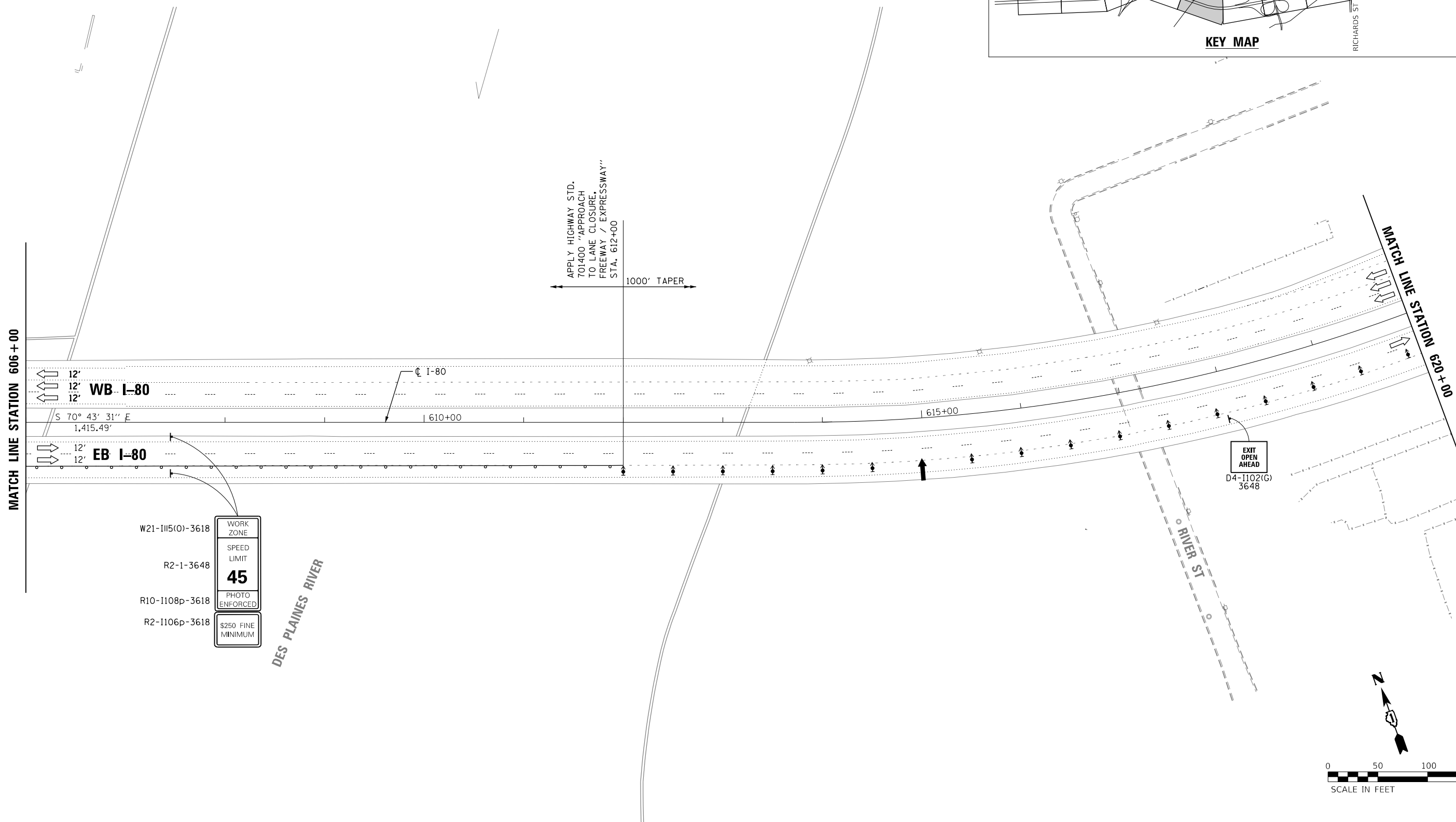
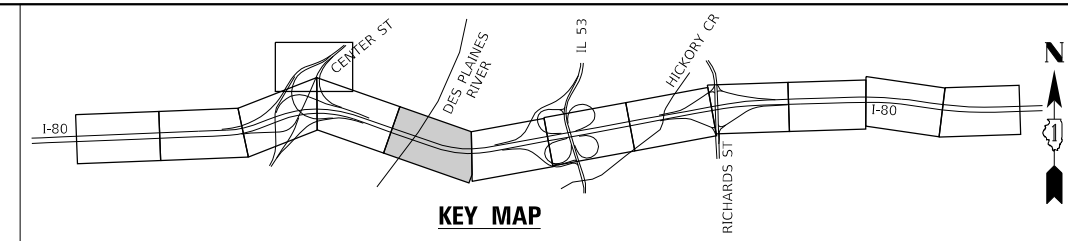
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PLOT DATE = 1/31/2020	DATE - 01/31/2020	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC - STAGE 3C**  
**I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**  
 SCALE: 100:0.0000 SHEET 3 OF 7 SHEETS STA. 593+00 TO STA. 606+00

F.A.I. RTE. 80	SECTION 2019-162-BR	COUNTY WILL	TOTAL SHEETS 93	SHEET NO. 31
CONTRACT NO. 62K34			ILLINOIS FED. AID PROJECT	

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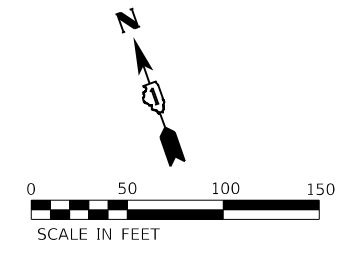
APPLY HIGHWAY STD.  
701400 "APPROACH  
TO LANE CLOSURE,  
FREEWAY / EXPRESSWAY"  
STA. 612+00  
1000' TAPER

- W21-II15(O)-3618
- R2-1-3648
- R10-II08p-3618
- R2-II06p-3618

WORK ZONE  
SPEED LIMIT  
**45**  
PHOTO ENFORCED  
\$250 FINE MINIMUM

DES PLAINES RIVER

EXIT OPEN AHEAD  
D4-II02(G)  
3648



**MOT PLAN LEGEND**

- WORK AREA
- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- DIRECTION OF TRAFFIC
- CONSTRUCTION SIGN
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)
- ARROW BOARD
- TYPE III BARRICADES WITH FLASHING LIGHTS
- DECK PATCHES TO BE COMPLETED IN THIS STAGE
- SPOTTER

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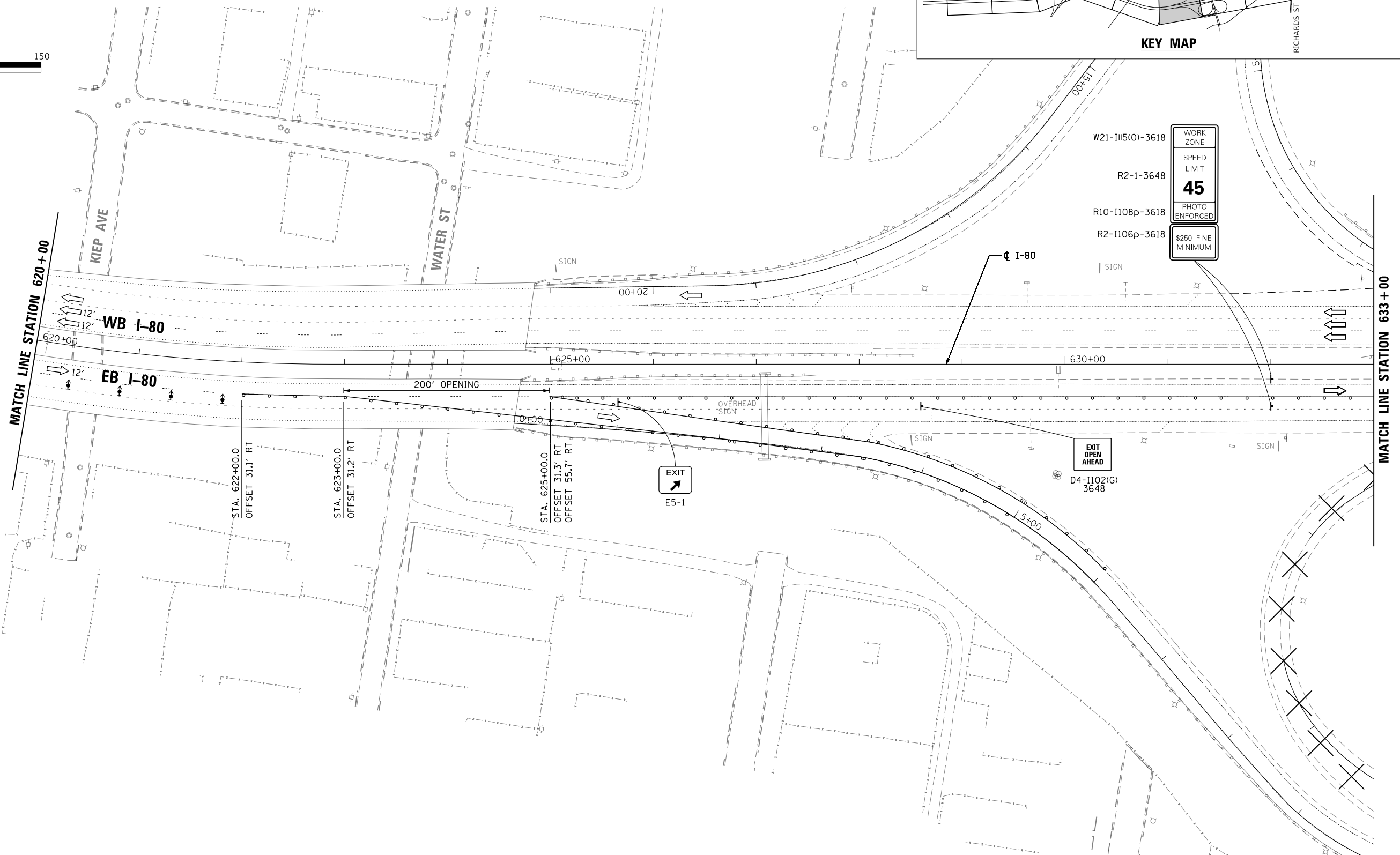
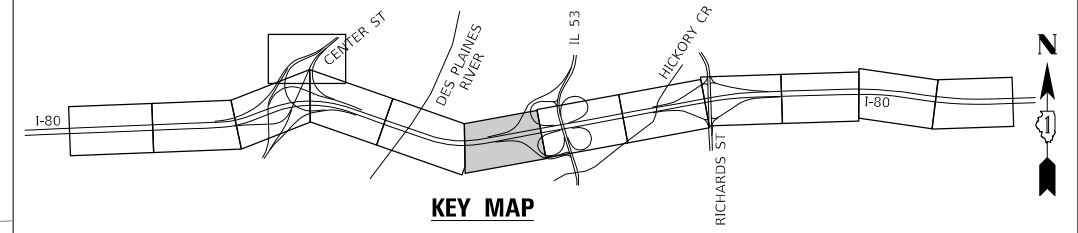
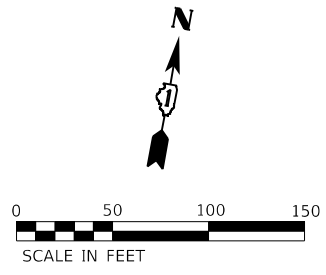
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC - STAGE 3C  
I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**

SCALE: 100:0.0000 SHEET 4 OF 7 SHEETS STA. 606+00 TO STA. 620+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	32
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				



WORK ZONE
SPEED LIMIT
<b>45</b>
PHOTO ENFORCED
\$250 FINE MINIMUM

W21-1115(O)-3618  
R2-1-3648  
R10-1108p-3618  
R2-1106p-3618

EXIT  
D4-1102(G)  
3648

**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE

- DIRECTION INDICATOR BARRICIDE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

- DIRECTION OF TRAFFIC
- ARROW BOARD

- CONSTRUCTION SIGN
- SPOTTER

- TYPE III BARRICADES WITH FLASHING LIGHTS

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC - STAGE 3C  
I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	33
CONTRACT NO. 62K34				

SCALE: 100:0.0000 SHEET 5 OF 7 SHEETS STA. 620+00 TO STA. 633+00

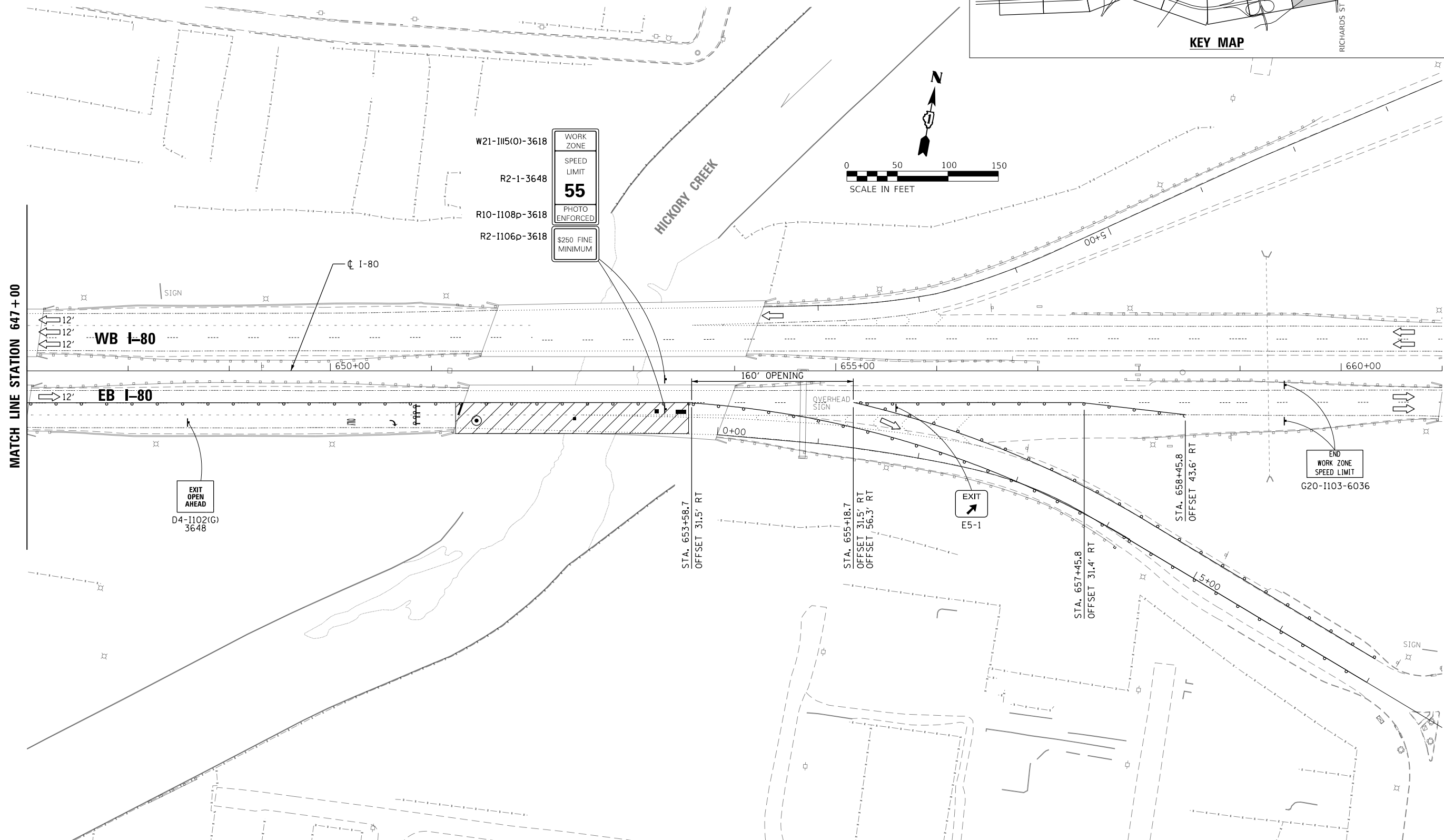
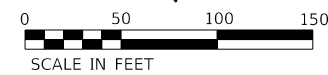
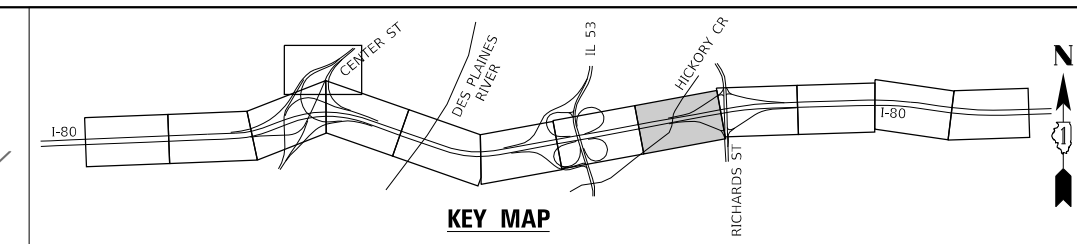
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PLOT DATE = 1/31/2020	CHECKED - JP	REVISED -
	DATE - 01/31/2020	REVISED -

ILLINOIS FED. AID PROJECT





W21-1115(O)-3618  
R2-1-3648  
R10-1108p-3618  
R2-1106p-3618

WORK ZONE  
SPEED LIMIT  
**55**  
PHOTO ENFORCED  
\$250 FINE MINIMUM

EXIT OPEN AHEAD  
D4-1102(G)  
3648

END WORK ZONE SPEED LIMIT  
G20-1103-6036

**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE
- DIRECTION INDICATOR BARRICIDE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)
- DIRECTION OF TRAFFIC
- ARROW BOARD
- CONSTRUCTION SIGN
- SPOTTER
- TYPE III BARRICADES WITH FLASHING LIGHTS

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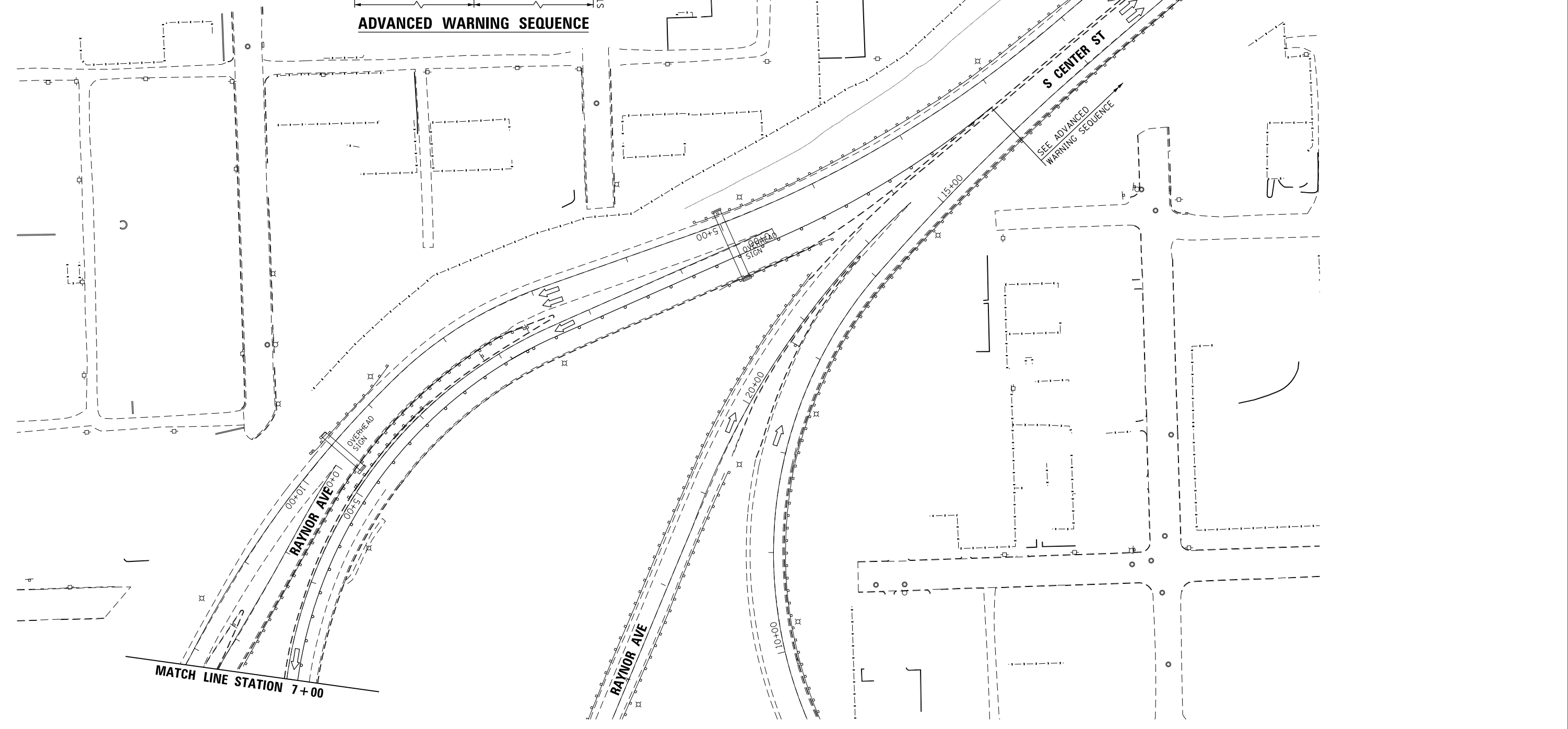
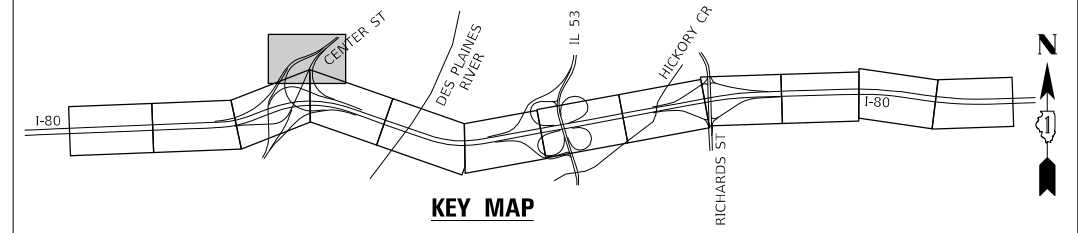
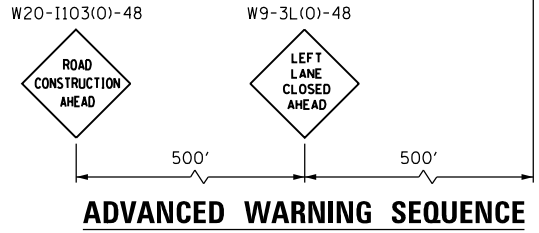
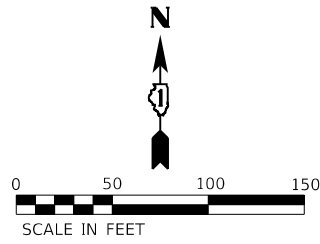
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PLOT DATE = 1/31/2020	DATE - 01/31/2020	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC - STAGE 3C  
I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**

SCALE: 100:0.0000    SHEET 7 OF 7 SHEETS    STA. 647+00 TO STA. 661+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	35
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				



**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE
- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)
- DIRECTION OF TRAFFIC
- ARROW BOARD
- CONSTRUCTION SIGN
- SPOTTER
- TYPE III BARRICADES WITH FLASHING LIGHTS

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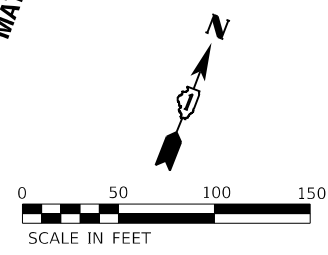
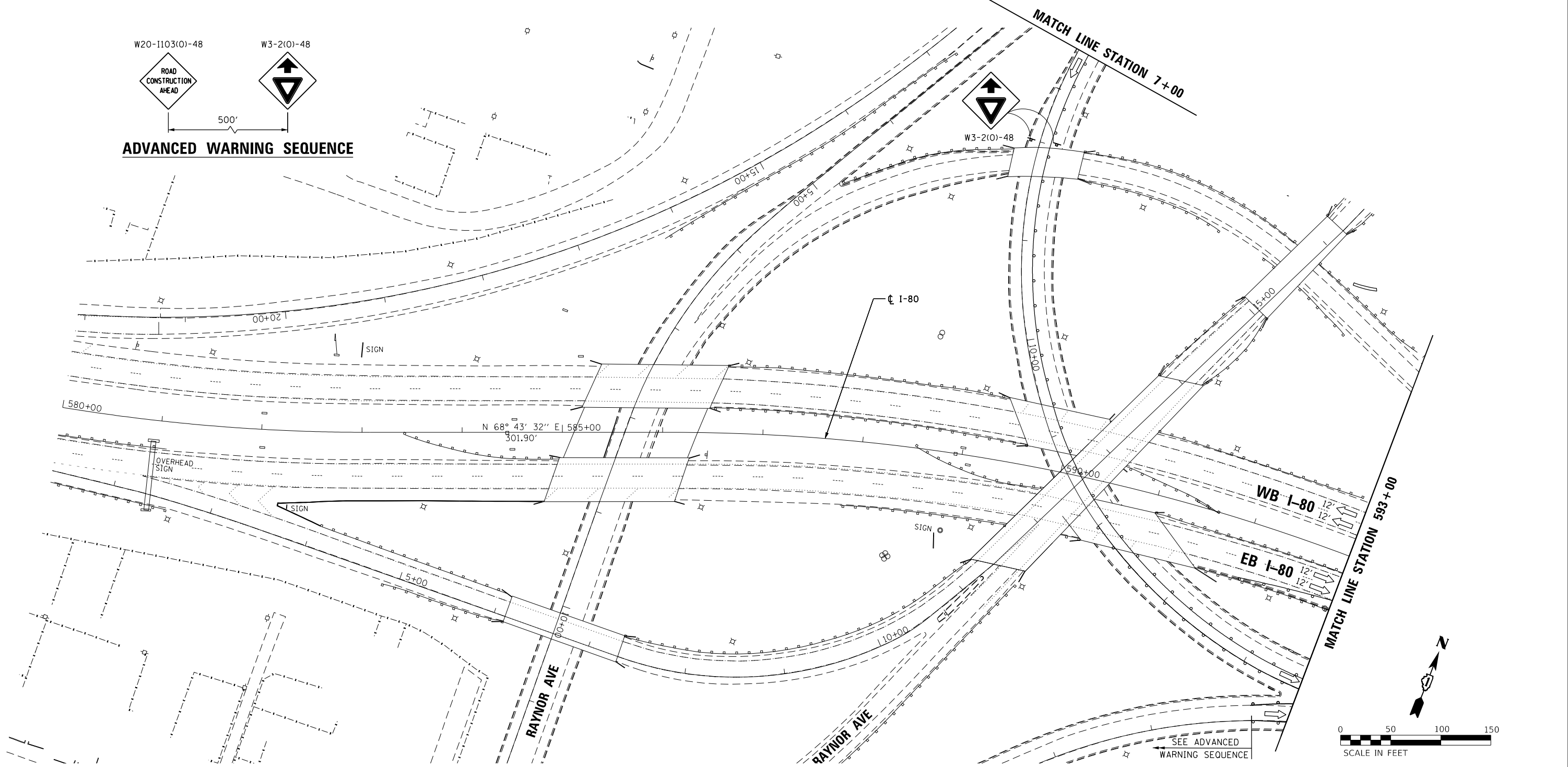
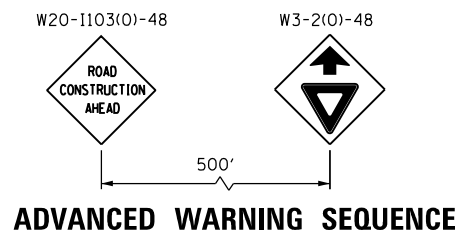
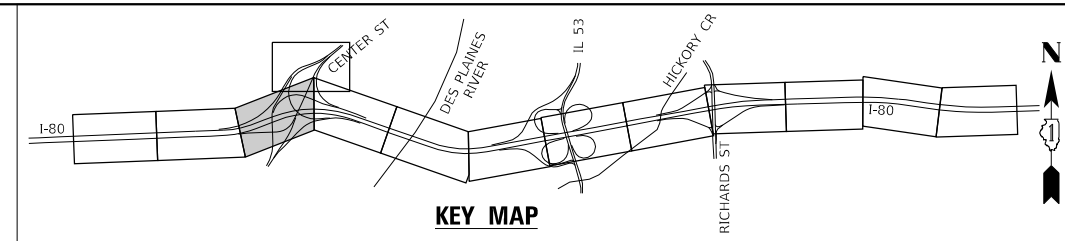
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC – STAGE 3D**  
**I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**

SCALE: 100:0.0000    SHEET 1    OF 10 SHEETS    STA.    TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	36
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				





**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE

- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

- DIRECTION OF TRAFFIC
- ARROW BOARD
- CONSTRUCTION SIGN
- SPOTTER
- TYPE III BARRICADES WITH FLASHING LIGHTS



USER NAME = mkoehler  
 DESIGNED - MDK  
 DRAWN - MDK  
 CHECKED - JP  
 DATE - 01/31/2020

DESIGNED - MDK  
 DRAWN - MDK  
 CHECKED - JP  
 DATE - 01/31/2020

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

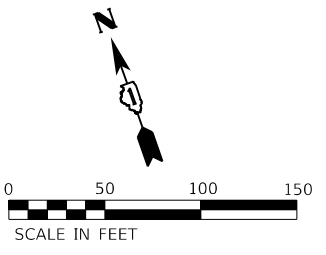
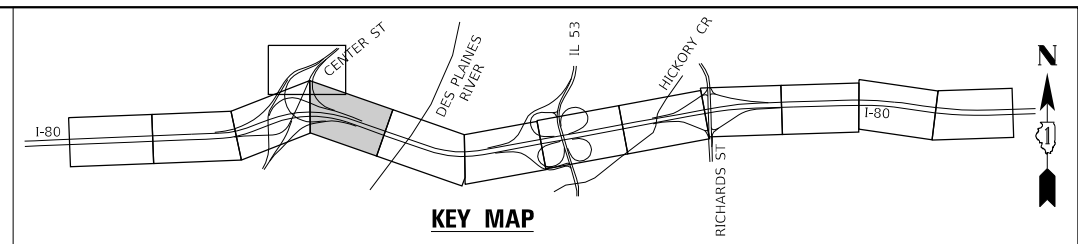
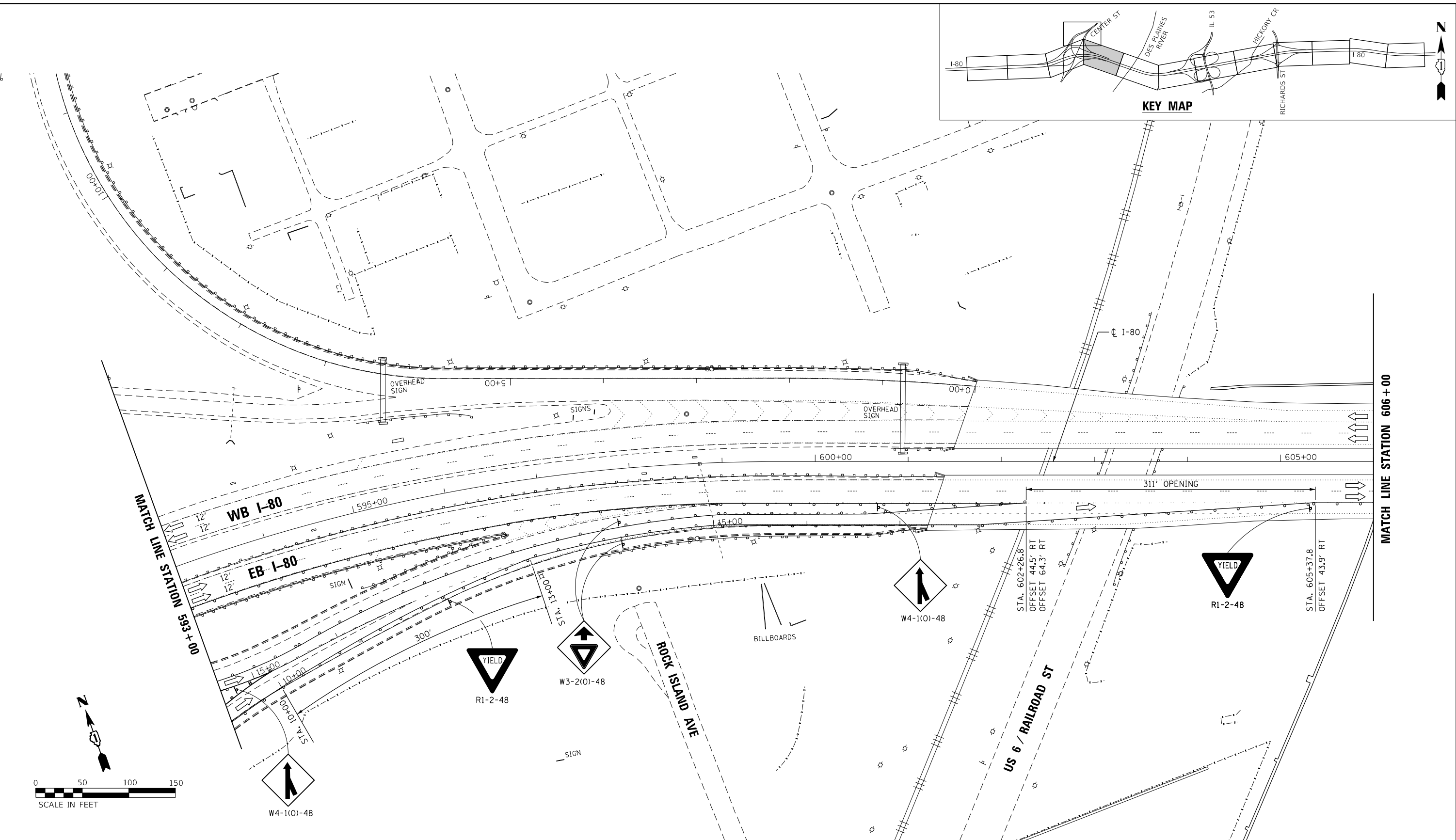
**MAINTENANCE OF TRAFFIC - STAGE 3D  
 I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	37
CONTRACT NO. 62K34				

SCALE: 100:0.0000 SHEET 2 OF 10 SHEETS STA. 580+00 TO STA. 593+00

ILLINOIS FED. AID PROJECT

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**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE

- DIRECTION INDICATOR BARRICIDE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

- DIRECTION OF TRAFFIC
- ARROW BOARD

- CONSTRUCTION SIGN
- SPOTTER

- TYPE III BARRICADES WITH FLASHING LIGHTS



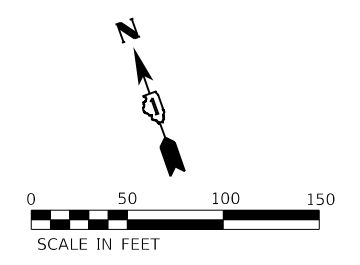
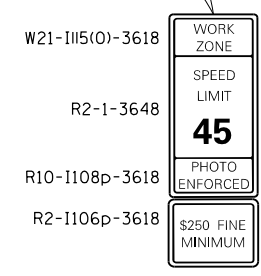
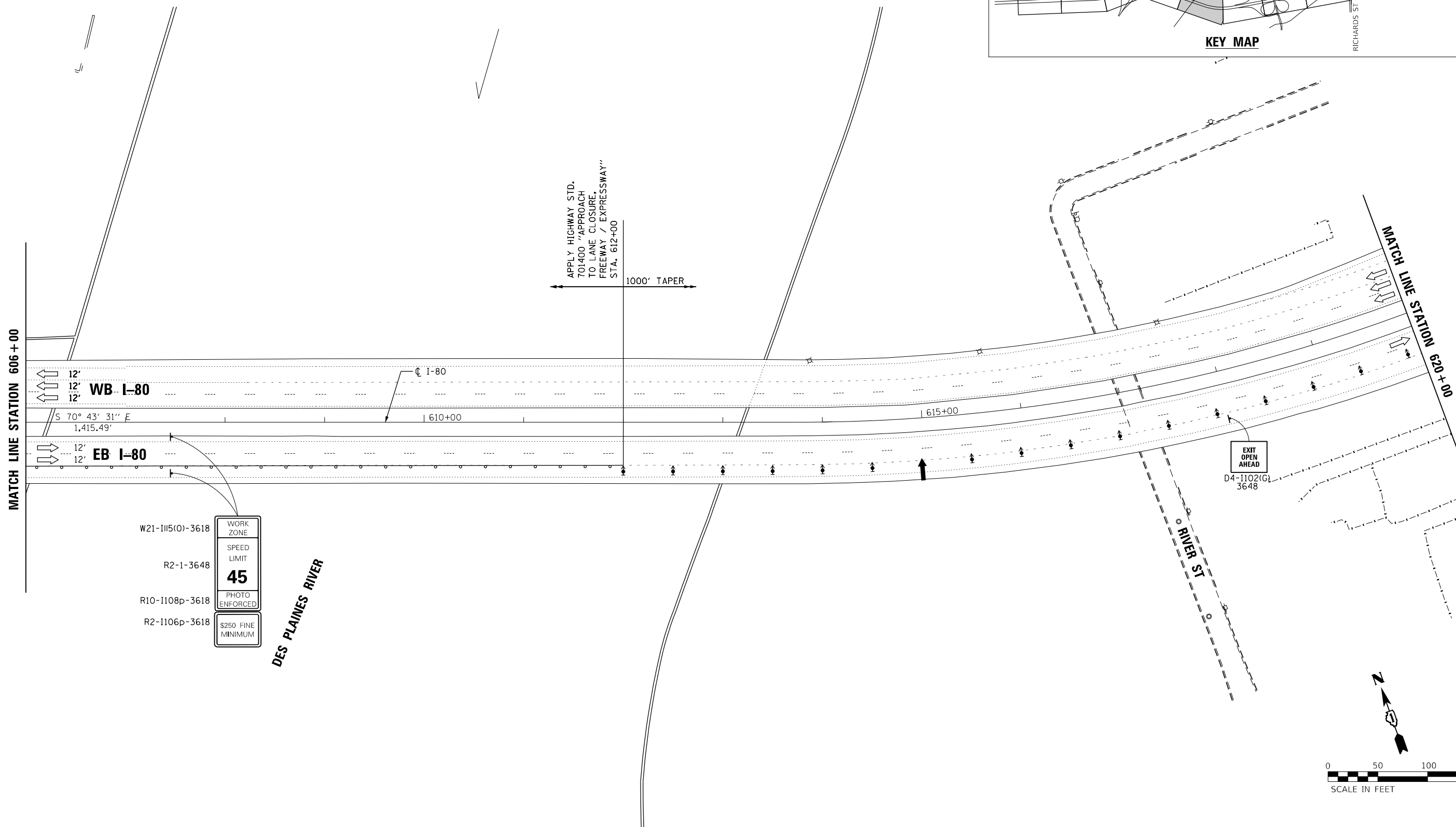
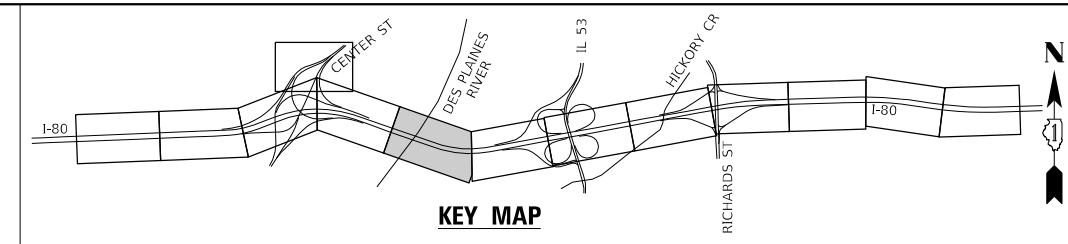
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PLOT DATE = 1/31/2020	CHECKED - JP	REVISED -
	DATE - 01/31/2020	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC - STAGE 3D**  
**I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**  
 SCALE: 100:0.0000 SHEET 3 OF 10 SHEETS STA. 593+00 TO STA. 606+00

F.A.I. RTE. 80	SECTION 2019-162-BR	COUNTY WILL	TOTAL SHEETS 93	SHEET NO. 38
CONTRACT NO. 62K34			ILLINOIS FED. AID PROJECT	

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**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE
- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)
- DIRECTION OF TRAFFIC
- ARROW BOARD
- CONSTRUCTION SIGN
- SPOTTER
- TYPE III BARRICADES WITH FLASHING LIGHTS

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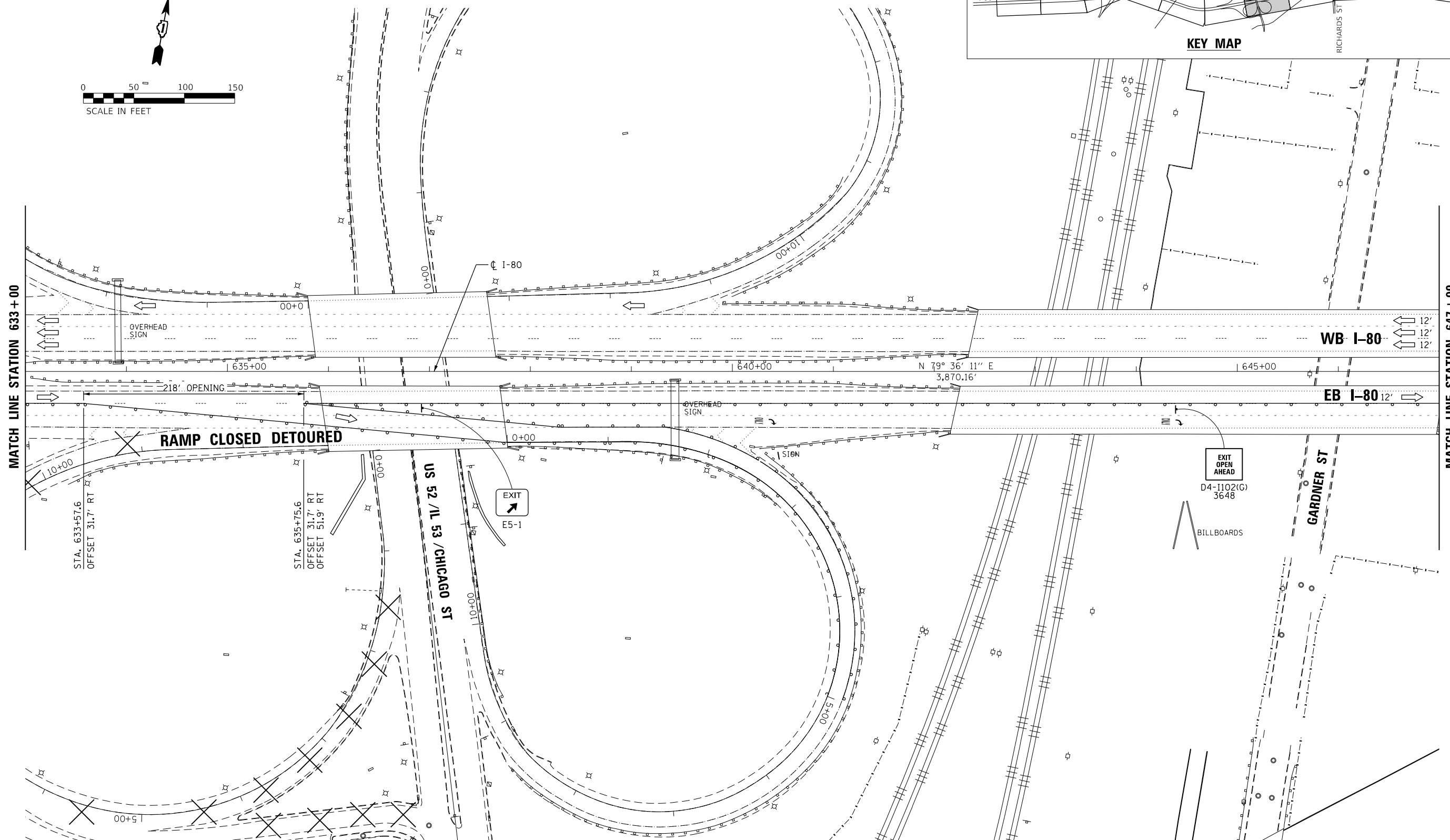
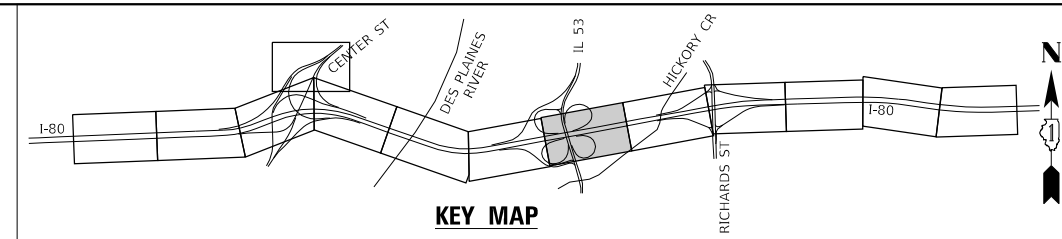
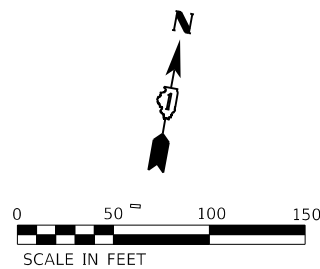
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC - STAGE 3D**  
**I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**

SCALE: 100:0.0000    SHEET 4 OF 10 SHEETS    STA. 606+00 TO STA. 620+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	39
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				





**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE
- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)
- DIRECTION OF TRAFFIC
- ARROW BOARD
- CONSTRUCTION SIGN
- SPOTTER
- TYPE III BARRICADES WITH FLASHING LIGHTS

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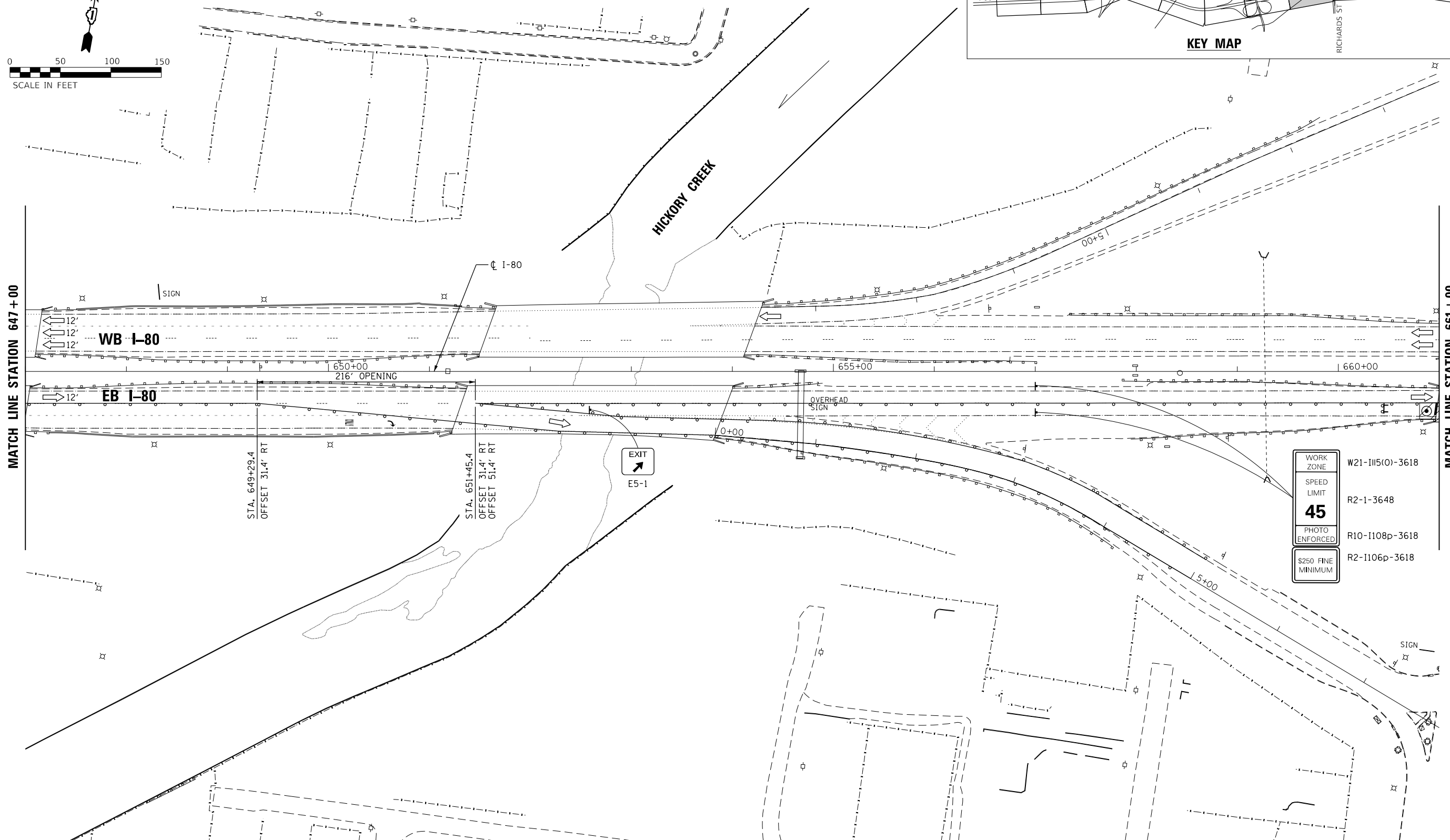
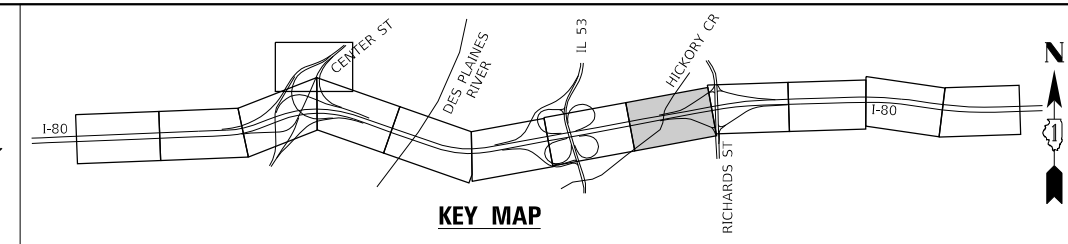
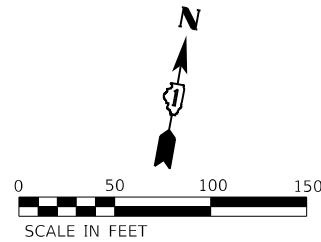


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PLOT DATE = 1/31/2020	CHECKED - JP	REVISED -
	DATE - 01/31/2020	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC - STAGE 3D**  
**I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**  
 SCALE: 100:0.0000 SHEET 6 OF 10 SHEETS STA. 633+00 TO STA. 647+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	41
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				



WORK ZONE	W21-II15(0)-3618
SPEED LIMIT	R2-1-3648
PHOTO ENFORCED	R10-II108p-3618
\$250 FINE MINIMUM	R2-II106p-3618

**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE

- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

- DIRECTION OF TRAFFIC
- ARROW BOARD

- CONSTRUCTION SIGN
- SPOTTER

- TYPE III BARRICADES WITH FLASHING LIGHTS



USER NAME = mkoehler	DESIGNED - MDK	REVISED -
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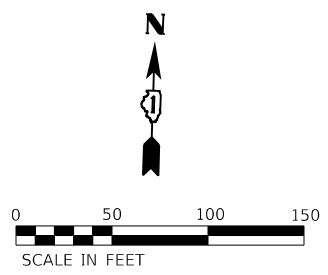
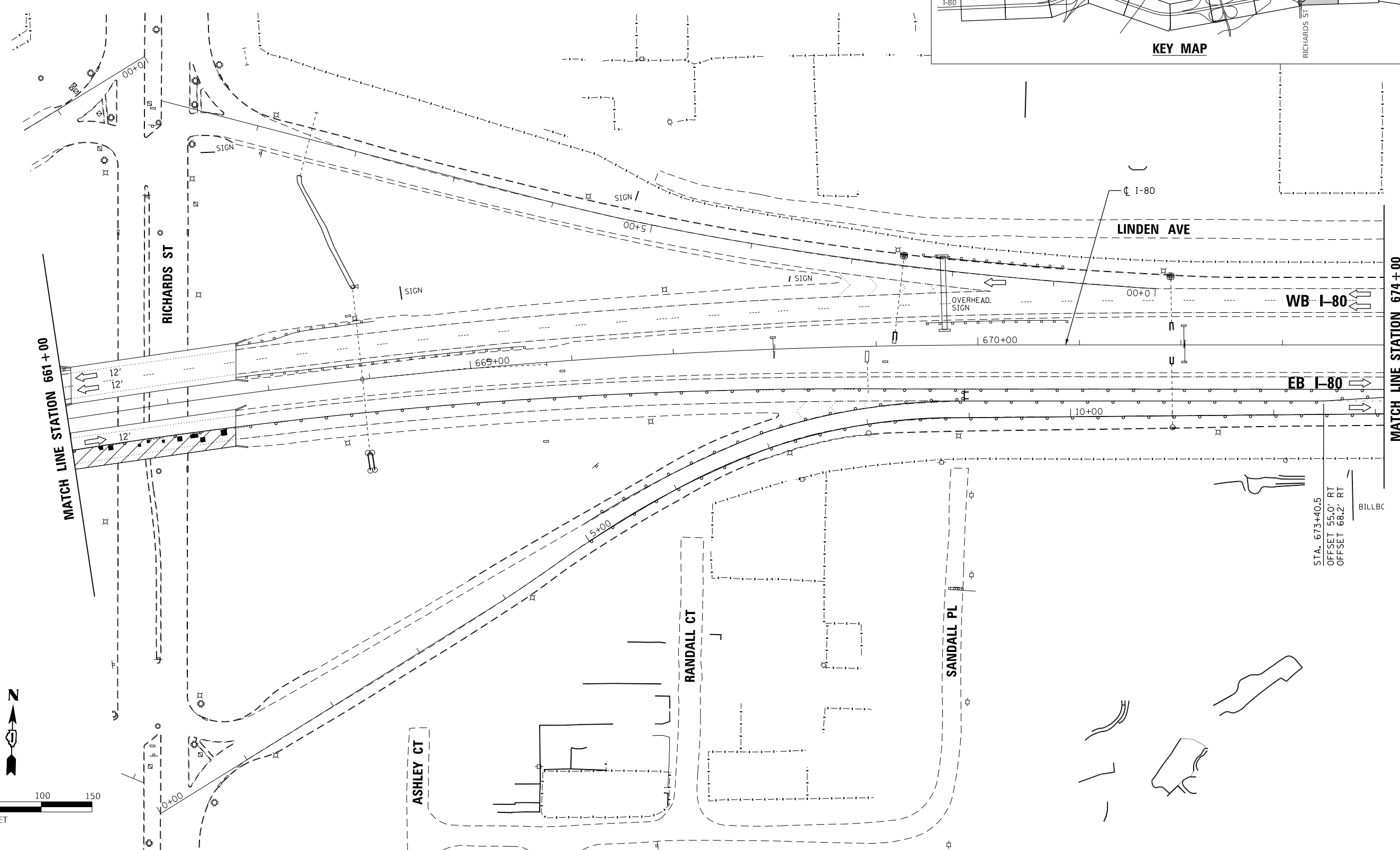
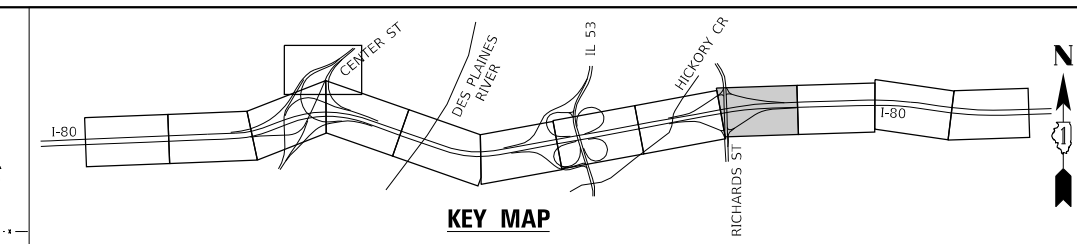
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC - STAGE 3D  
I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	42
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

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SCALE: 100:0.0000 SHEET 7 OF 10 SHEETS STA. 647+00 TO STA. 661+00



**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE

- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

- DIRECTION OF TRAFFIC
- ARROW BOARD

- CONSTRUCTION SIGN
- SPOTTER

- TYPE III BARRICADES WITH FLASHING LIGHTS

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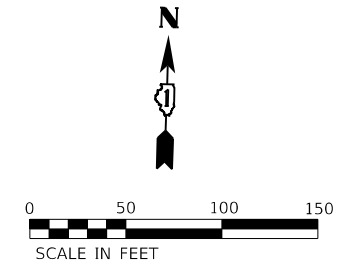
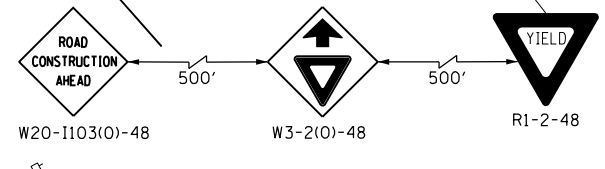
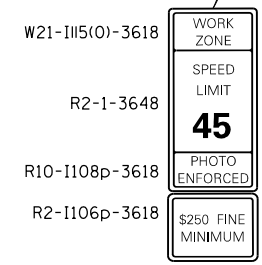
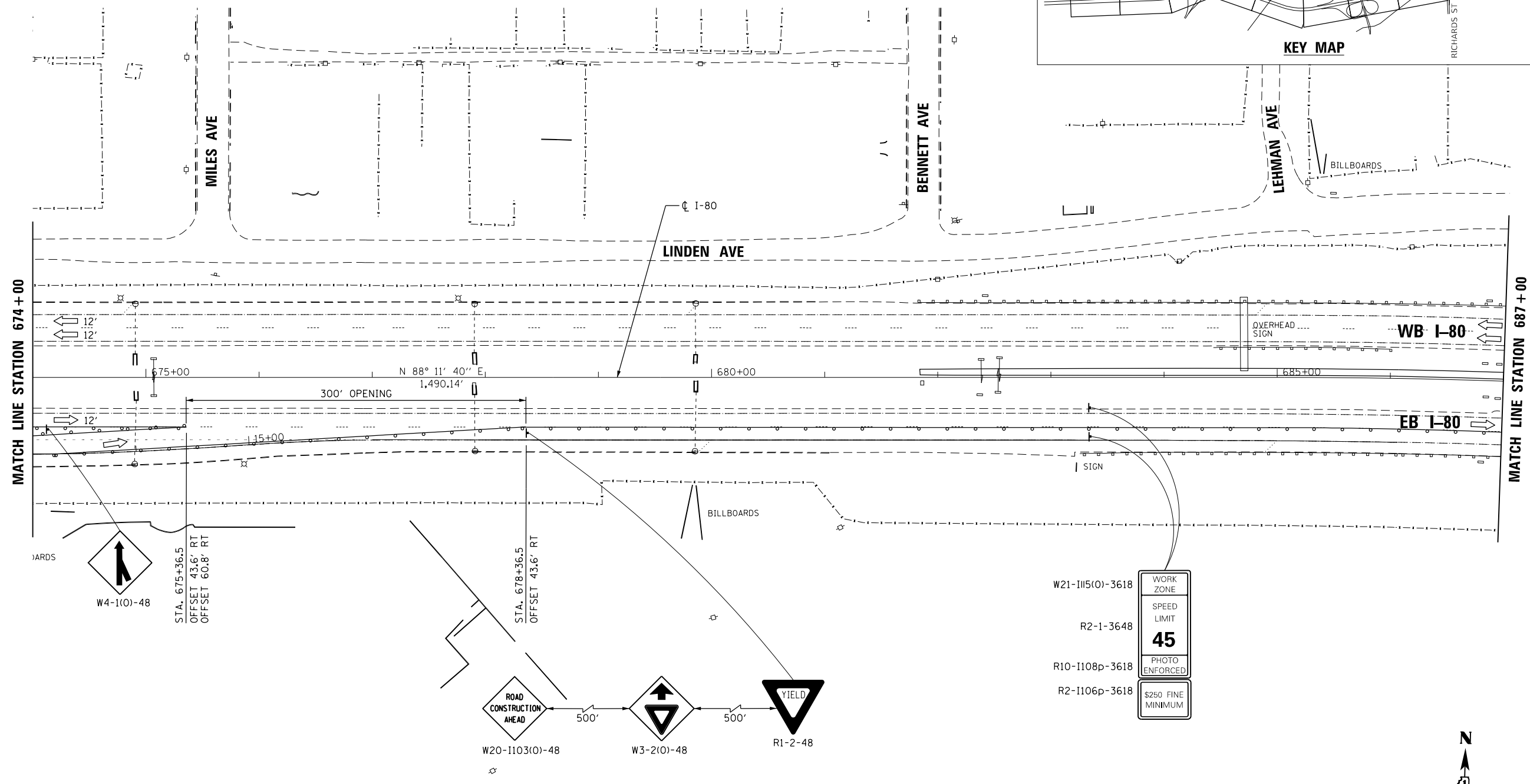
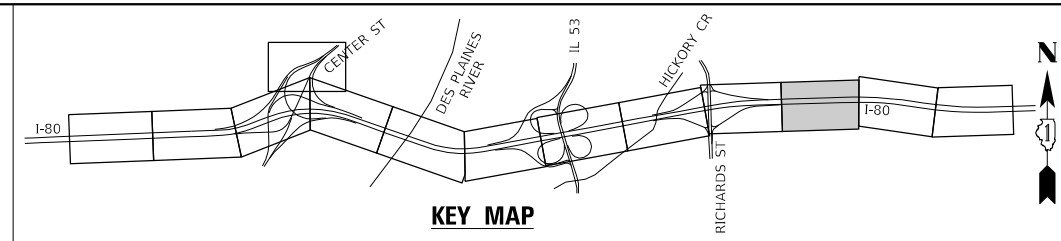


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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC - STAGE 3D**  
**I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**  
 SCALE: 100:0.0000 SHEET 8 OF 10 SHEETS STA. 661+00 TO STA. 674+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	43
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				



**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE
- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

- DIRECTION OF TRAFFIC
- ARROW BOARD
- CONSTRUCTION SIGN
- SPOTTER
- TYPE III BARRICADES WITH FLASHING LIGHTS

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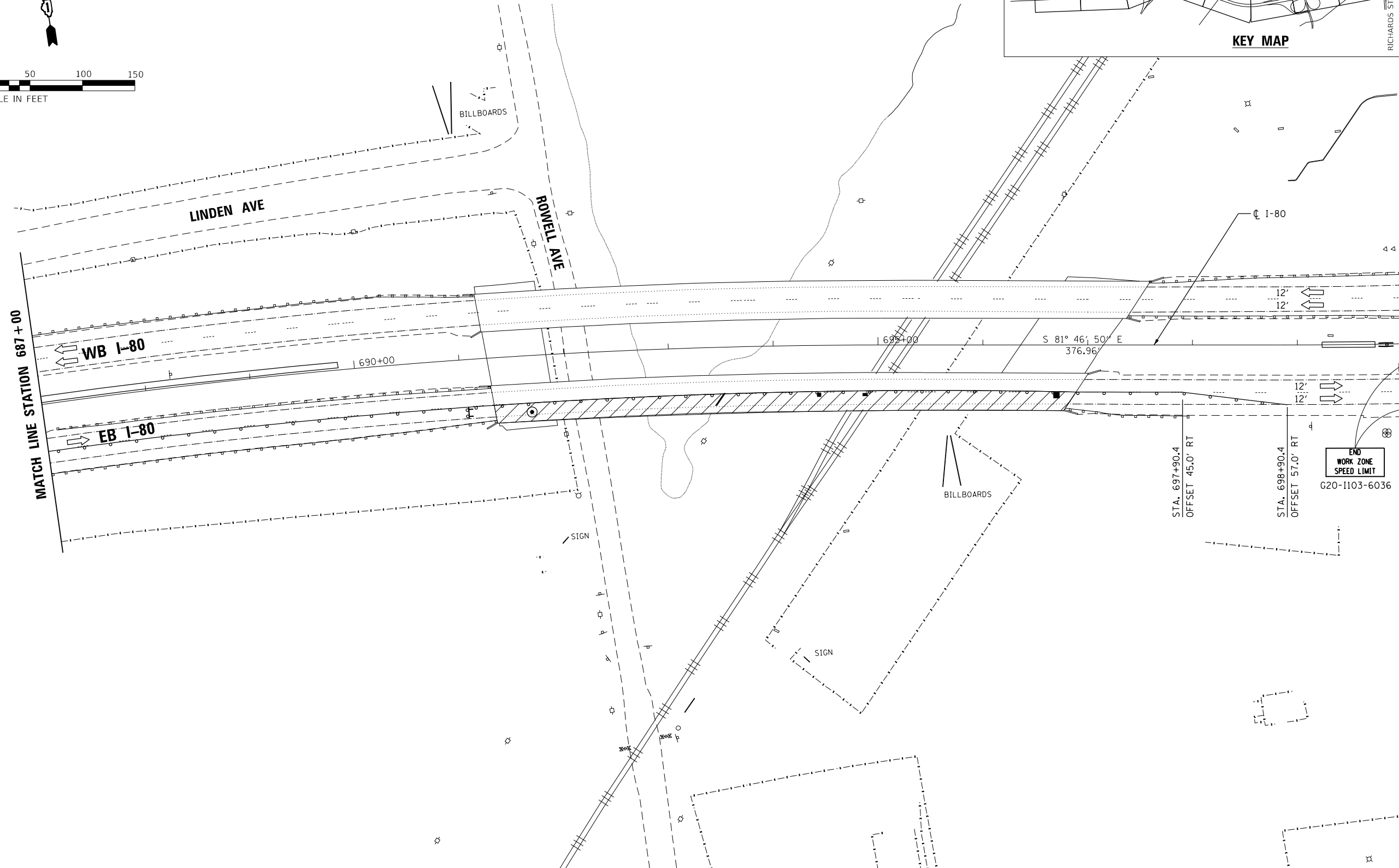
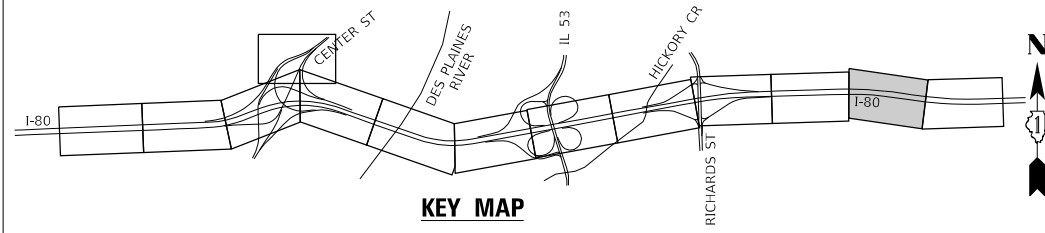
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC - STAGE 3D  
I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**

SCALE: 100:0.0000    SHEET 9 OF 10 SHEETS    STA. 674+00 TO STA. 687+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	44
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				





**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE

- DIRECTION INDICATOR BARRICIDE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

- DIRECTION OF TRAFFIC
- ARROW BOARD

- CONSTRUCTION SIGN
- SPOTTER

- TYPE III BARRICADES WITH FLASHING LIGHTS

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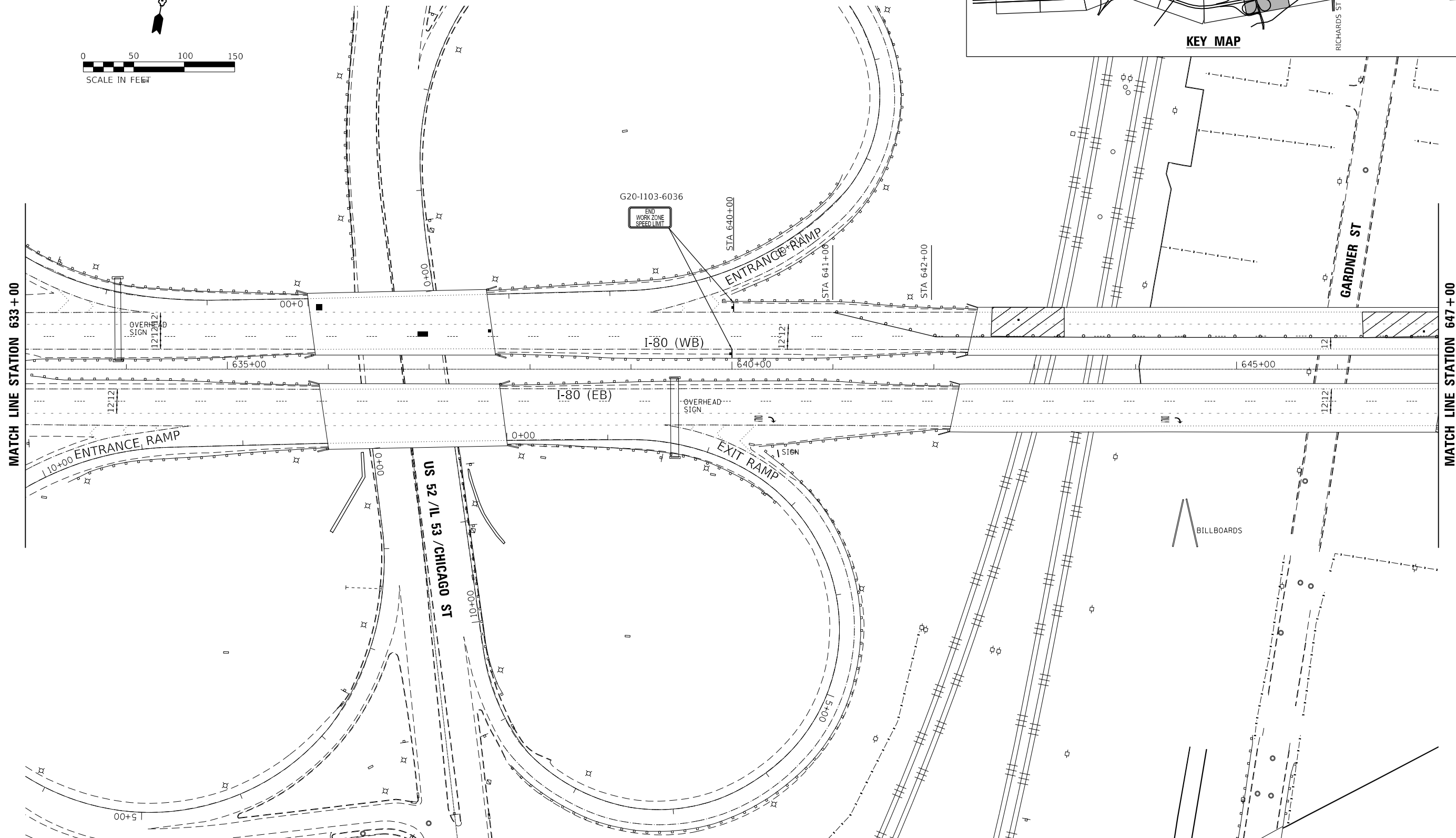
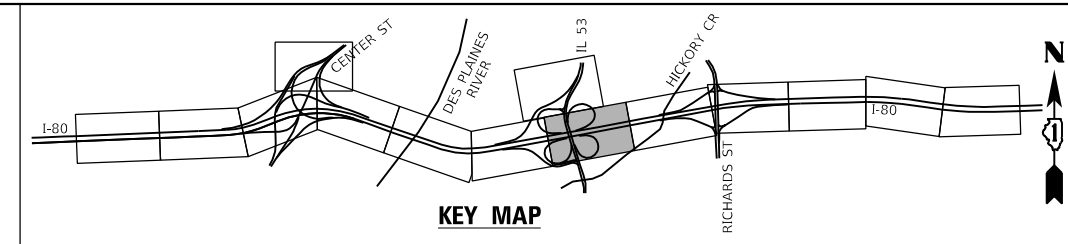
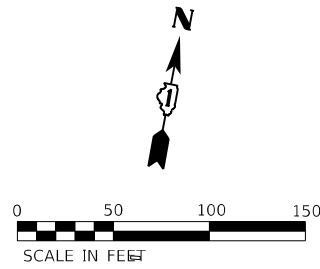
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PLOT DATE = 1/31/2020	DATE - 01/31/2020	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC - STAGE 3D**  
**I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**

SCALE: 100:0.0000    SHEET 10 OF 10 SHEETS    STA. 687+00 TO STA. 700+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	45
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				



**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE

- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

- DIRECTION OF TRAFFIC
- ARROW BOARD

- CONSTRUCTION SIGN
- SPOTTER
- TYPE III BARRICADES WITH FLASHING LIGHTS

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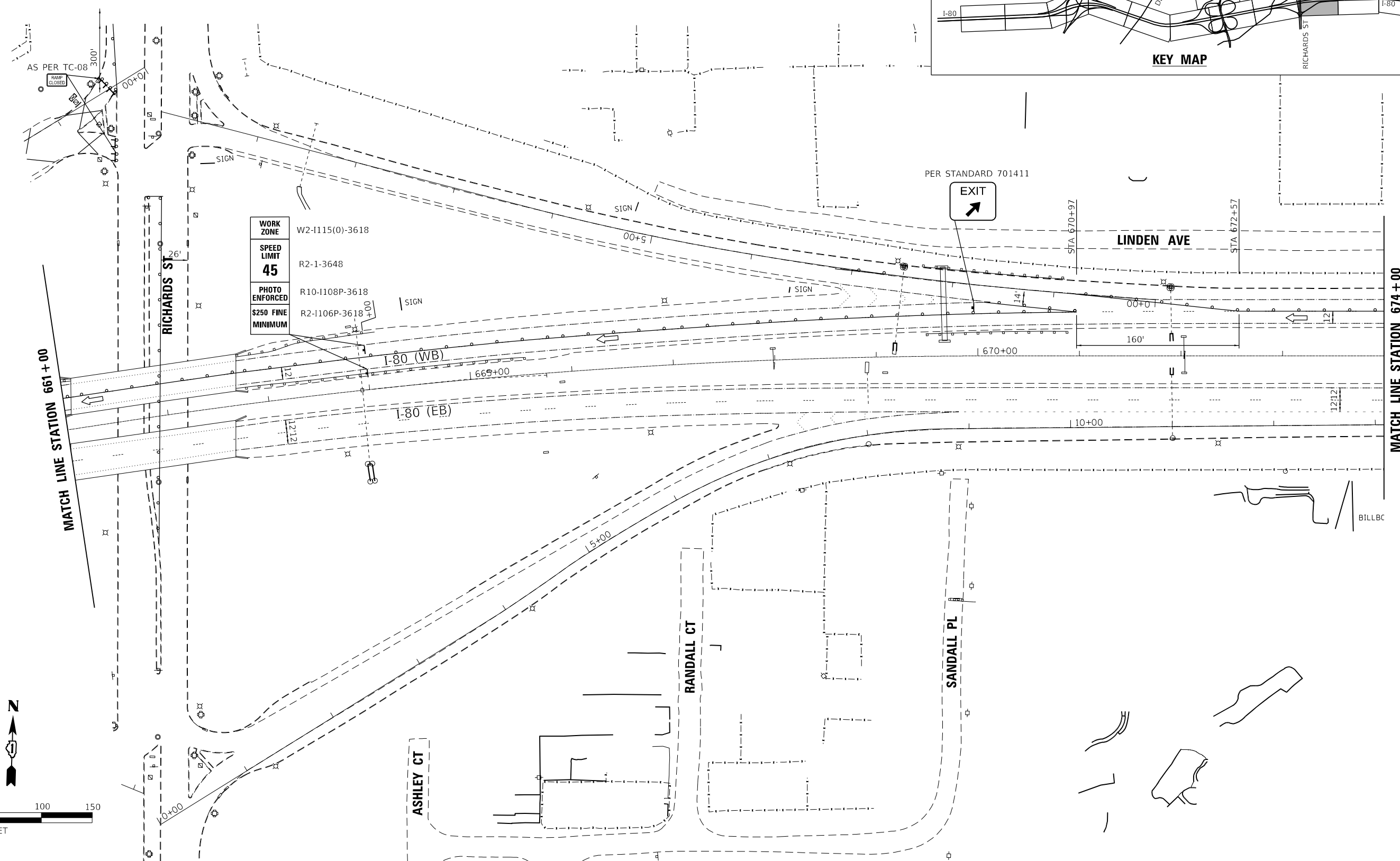
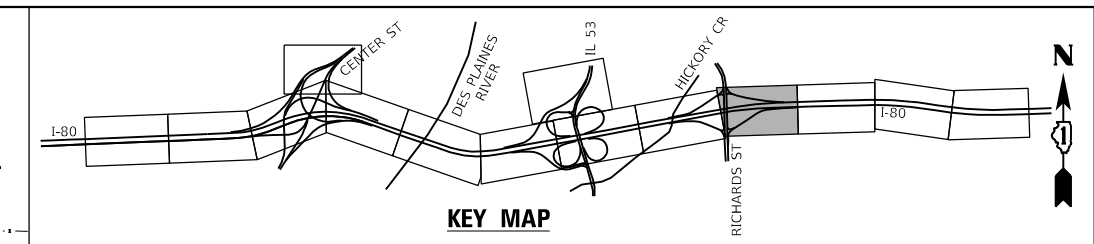
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	DATE - 01/31/2020	REvised -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC - STAGE 4A**  
**I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**  
 SCALE: 100:0.0000 " = 1" / in. SHEET 1 OF 6 SHEETS STA. 633+00 TO STA. 647+00

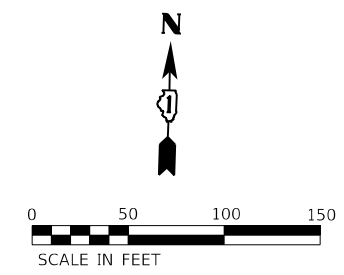
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	46
			CONTRACT NO. 62K34	
		ILLINOIS FED. AID PROJECT		





WORK ZONE	W2-1115(0)-3618
SPEED LIMIT	R2-1-3648
PHOTO ENFORCED	R10-1108P-3618
\$250 FINE MINIMUM	R2-1106P-3618

PER STANDARD 701411



**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE

- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

- DIRECTION OF TRAFFIC
- ARROW BOARD

- CONSTRUCTION SIGN
- SPOTTER

- TYPE III BARRICADES WITH FLASHING LIGHTS



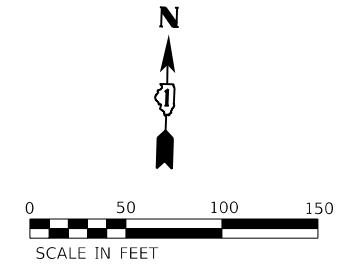
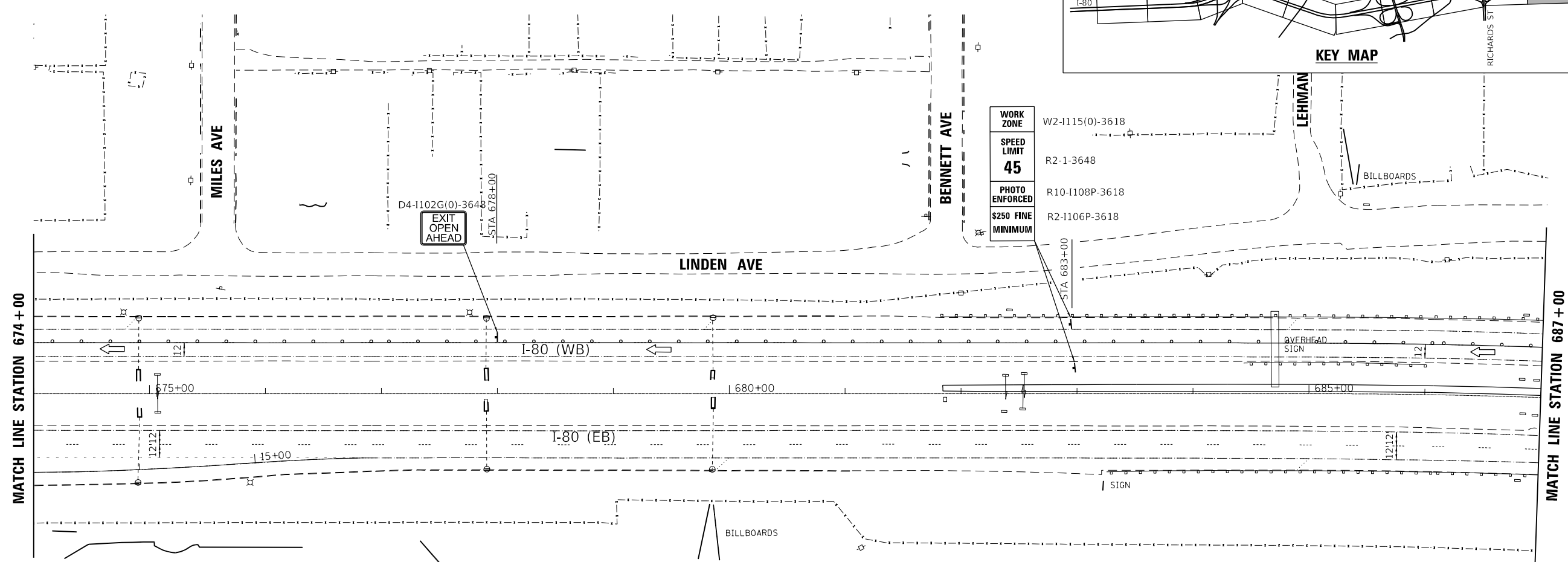
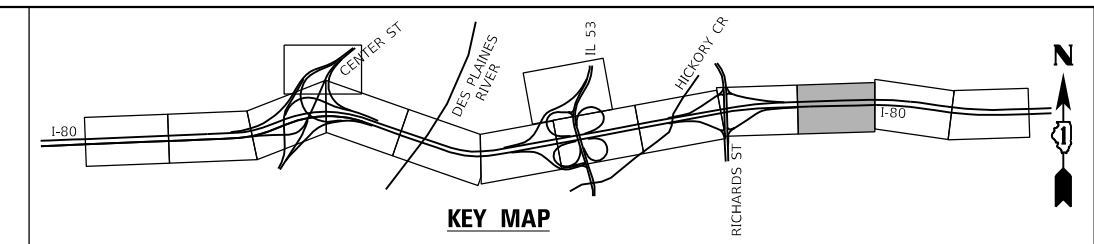
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DESIGNED	- AB
DRAWN	- AB
CHECKED	- LRC
DATE	- 01/31/2020
REVISIONS	
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REVISIONS	
REVISIONS	

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC - STAGE 4A**  
**I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**  
 SCALE: 100:0.0000 '"/> SHEET 3 OF 6 SHEETS STA. 661+00 TO STA. 674+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	48
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

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**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE
- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)
- DIRECTION OF TRAFFIC
- ARROW BOARD
- CONSTRUCTION SIGN
- SPOTTER
- TYPE III BARRICADES WITH FLASHING LIGHTS



USER NAME = johnn	DESIGNED - AB	REVISED -
	DRAWN - AB	REVISED -
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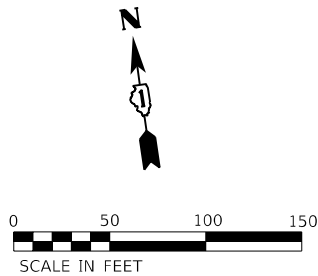
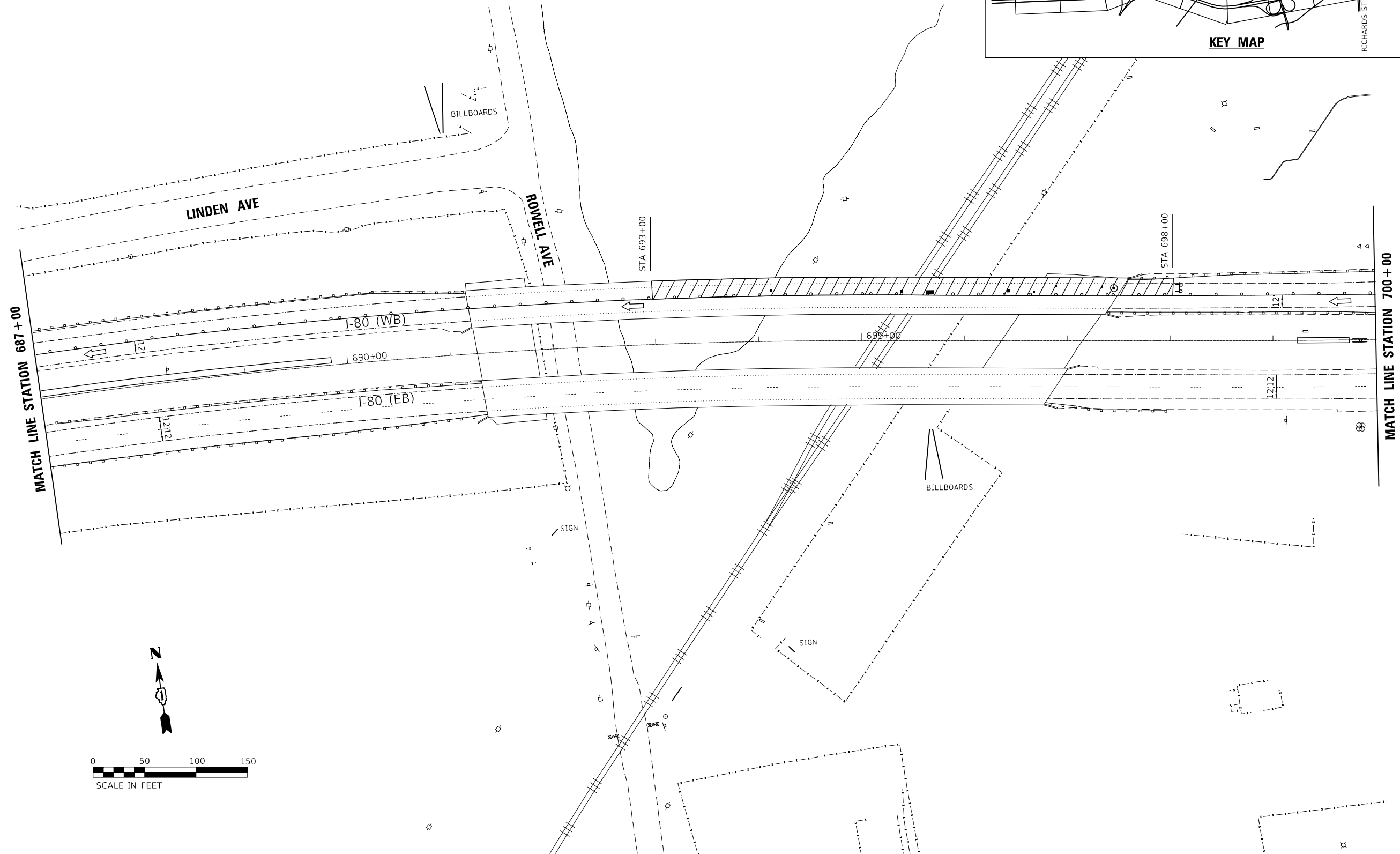
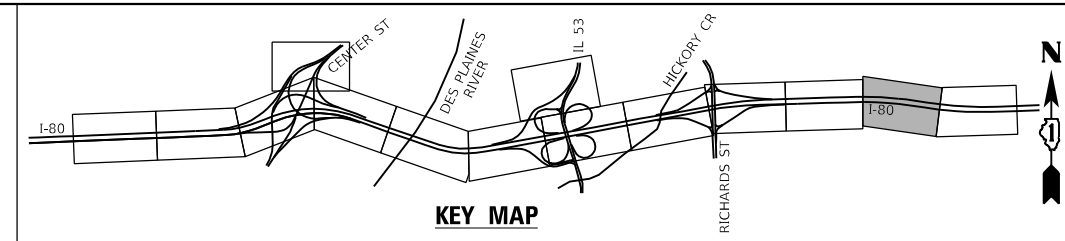
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC - STAGE 4A  
I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**

SCALE: 100:0.0000 '"/>

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	49
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

MODEL: D:\m\p\... FILE: MainE... 26\CADD\CADD\_Sheets\CHRD\62K34-51T-MOT-4A-04-AGL.dgn



**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE

- DIRECTION INDICATOR BARRICIDE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

- DIRECTION OF TRAFFIC
- ARROW BOARD

- CONSTRUCTION SIGN
- SPOTTER

- TYPE III BARRICADES WITH FLASHING LIGHTS

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

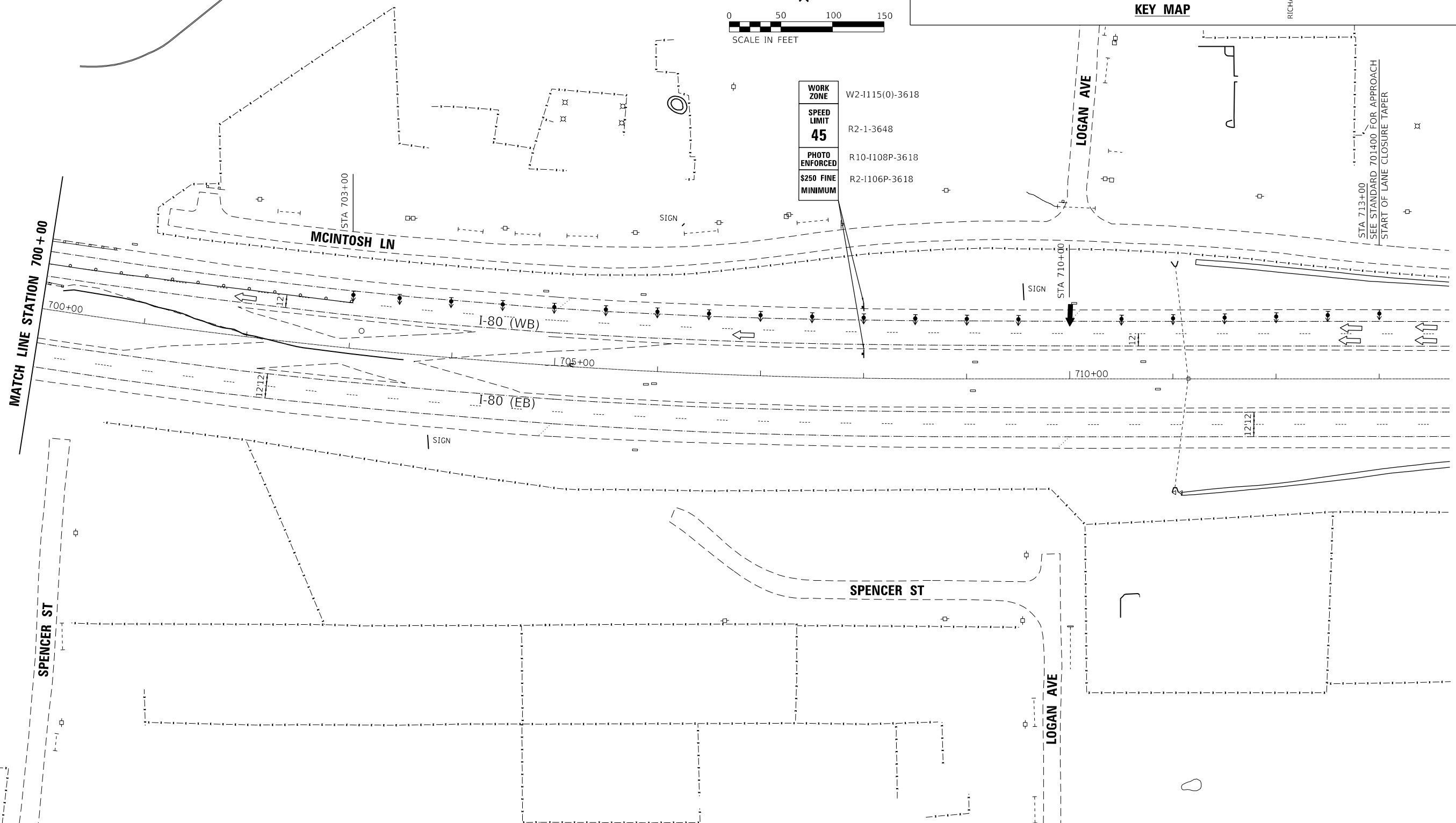
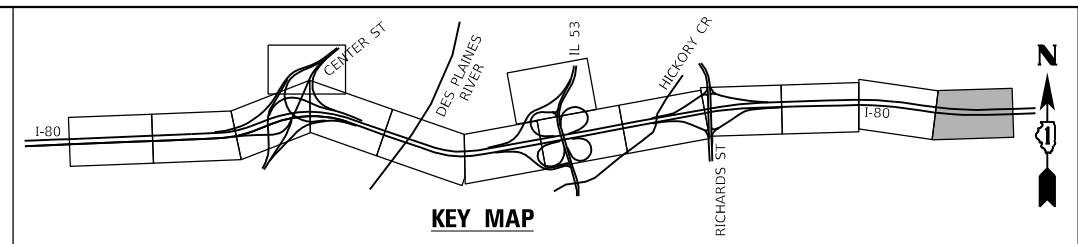
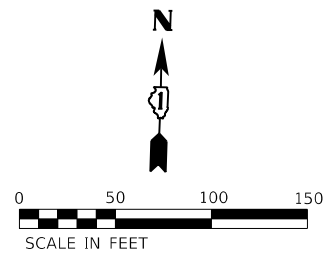
**SUGGESTED MAINTENANCE OF TRAFFIC - STAGE 4A  
I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	50
			CONTRACT NO. 62K34	
			ILLINOIS FED. AID PROJECT	



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	DRAWN - AB	REVISED -
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WORK ZONE	W2-1115(0)-3618
SPEED LIMIT	R2-1-3648
PHOTO ENFORCED	R10-1108P-3618
\$250 FINE MINIMUM	R2-1106P-3618

STA 713+00  
SEE STANDARD 701400 FOR APPROACH  
START OF LANE CLOSURE TAPER

**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE

- DIRECTION INDICATOR BARRICIDE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

- DIRECTION OF TRAFFIC
- ARROW BOARD

- CONSTRUCTION SIGN
- SPOTTER

- TYPE III BARRICADES WITH FLASHING LIGHTS



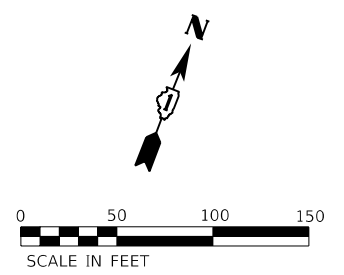
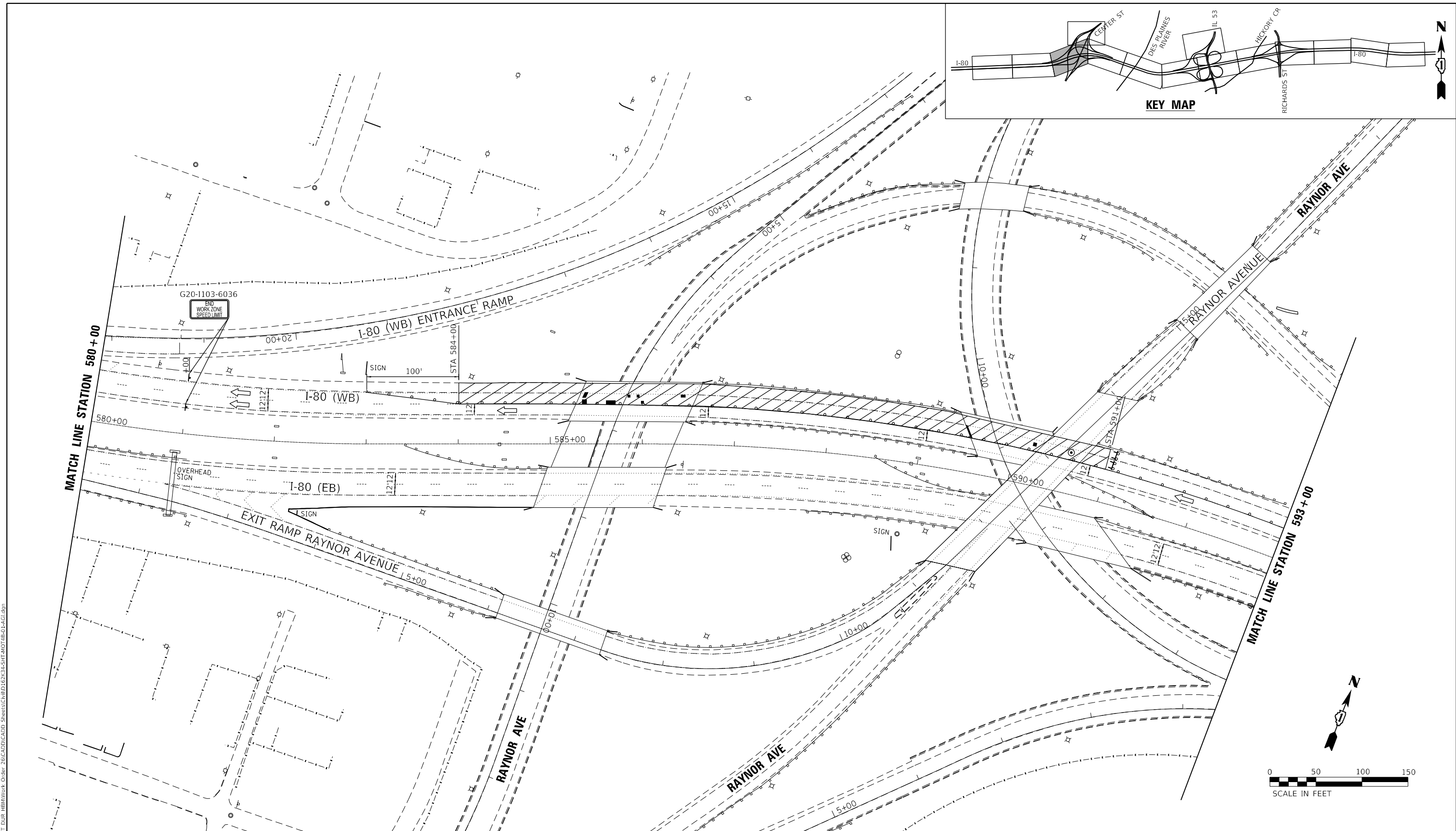
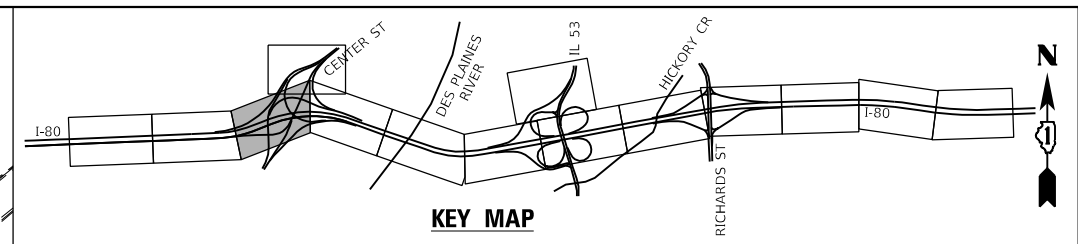
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PLOT DATE = 2/3/2020	CHECKED - LRC	REVISED -
	DATE - 01/31/2020	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC - STAGE 4A**  
**I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**  
SCALE: 100:0.0000 '"/>

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	51
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

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 PROJECT: 13040...  
 ORDER: 26...  
 ORDER: 26...



**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE

- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

- DIRECTION OF TRAFFIC
- ARROW BOARD

- CONSTRUCTION SIGN
- SPOTTER

- TYPE III BARRICADES WITH FLASHING LIGHTS



USER NAME = johnn	DESIGNED - AB	REVISED -
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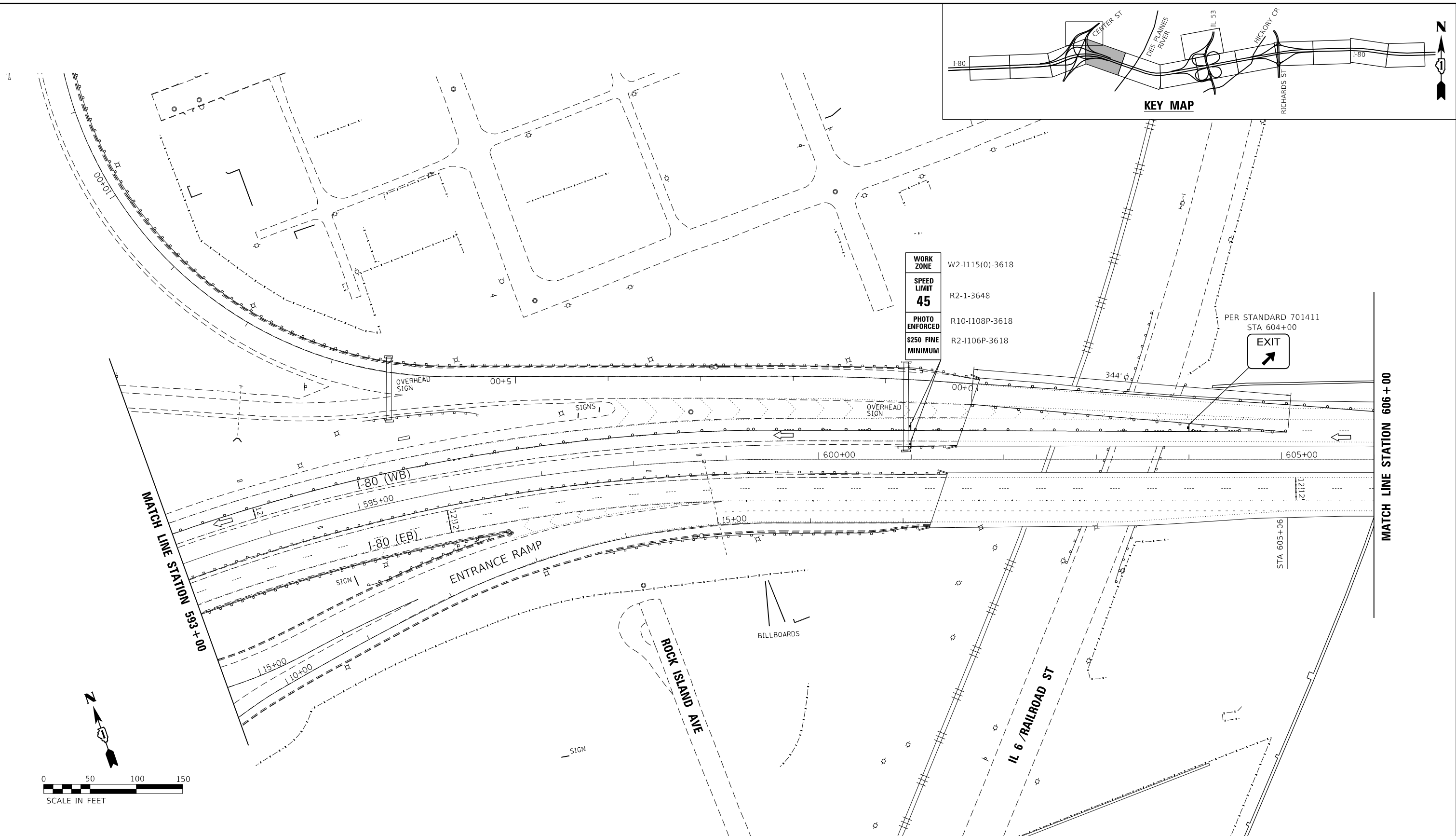
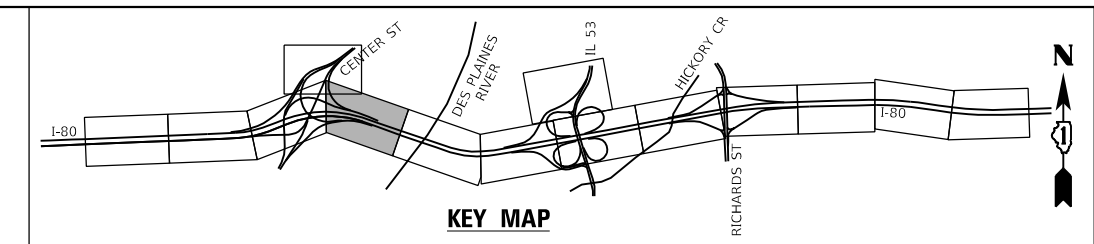
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC - STAGE 4B**  
**I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**  
 SCALE: 100:0.0000 '"/>

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62K34				

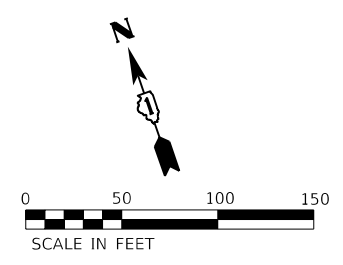
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WORK ZONE	W2-1115(0)-3618
SPEED LIMIT	R2-1-3648
PHOTO ENFORCED	R10-1108P-3618
\$250 FINE MINIMUM	R2-1106P-3618

PER STANDARD 701411  
STA 604+00



**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE

- DIRECTION INDICATOR BARRICIDE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

- DIRECTION OF TRAFFIC
- ARROW BOARD

- CONSTRUCTION SIGN
- SPOTTER

- TYPE III BARRICADES WITH FLASHING LIGHTS



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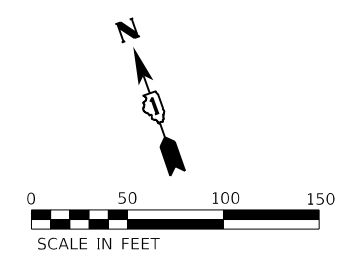
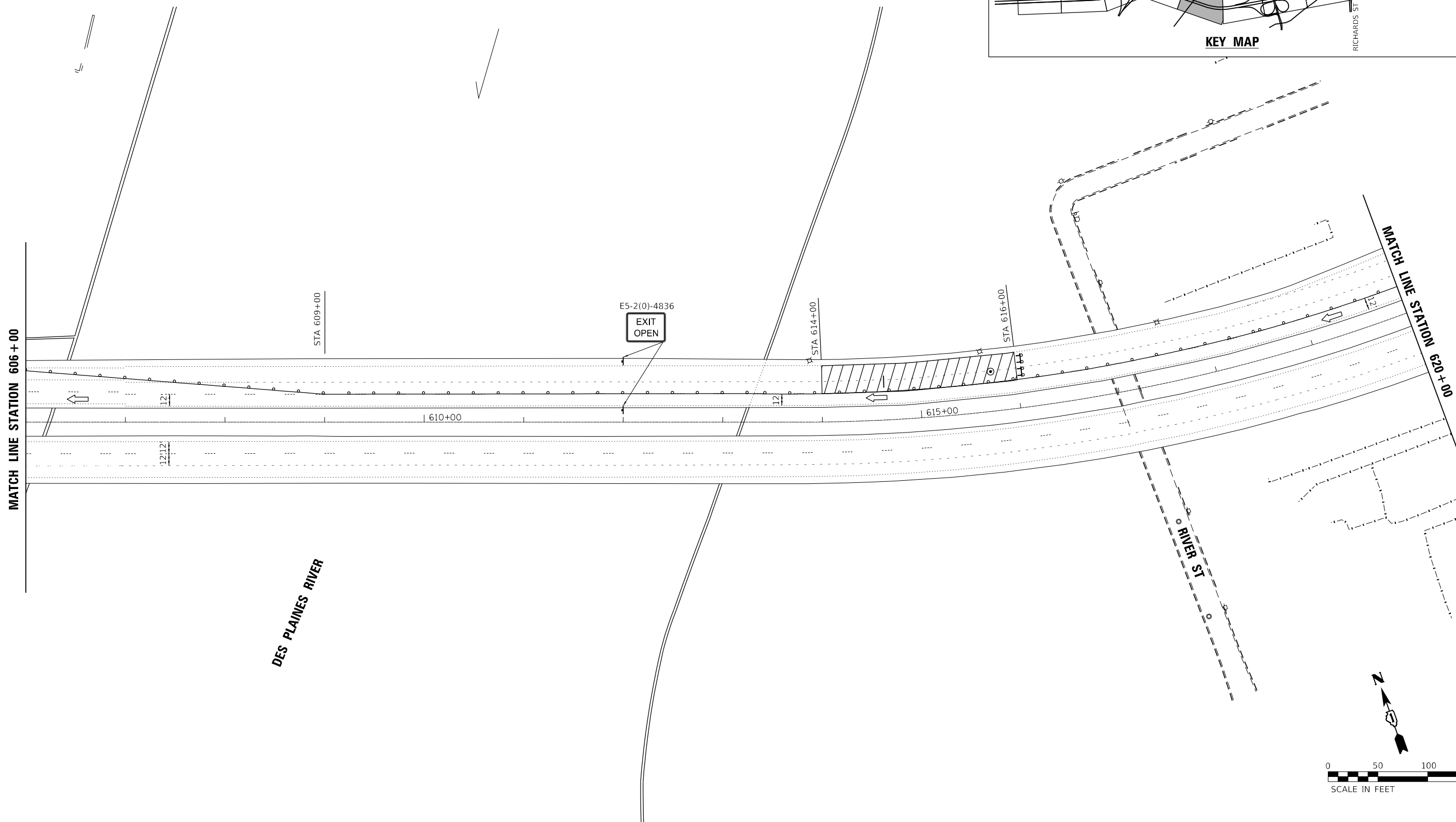
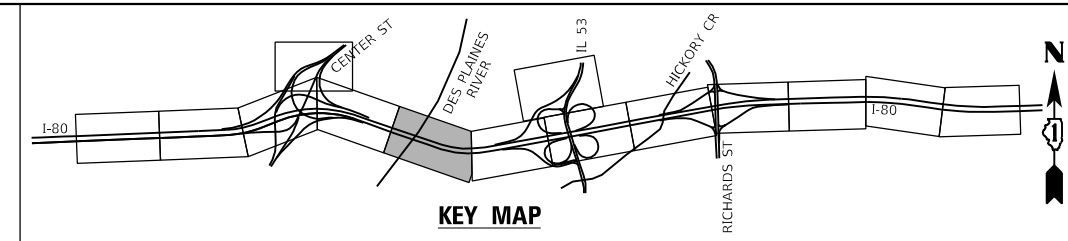
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DATE - 01/31/2020	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC - STAGE 4B**  
**I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**  
SCALE: 100:0.0000 '"/>

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

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**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE
- DIRECTION INDICATOR BARRICIDE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)
- DIRECTION OF TRAFFIC
- ARROW BOARD
- CONSTRUCTION SIGN
- SPOTTER
- TYPE III BARRICADES WITH FLASHING LIGHTS

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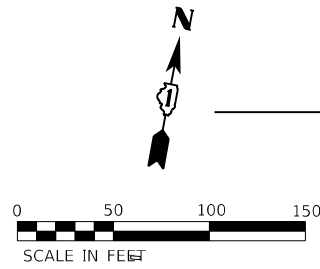
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC - STAGE 4B  
I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**

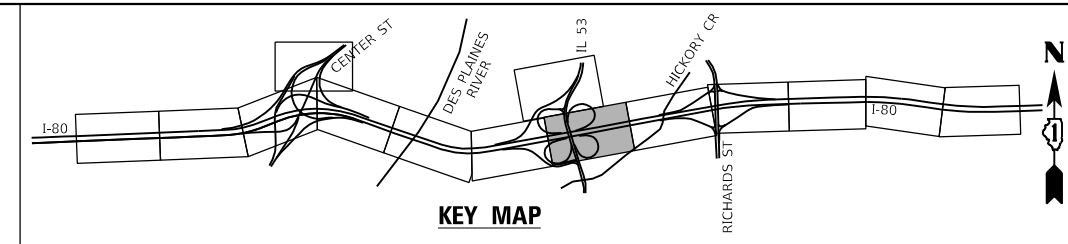
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	54
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				





MATCH LINE SEE SHEET 06



<b>WORK ZONE</b>	W2-1115(0)-3618
<b>SPEED LIMIT</b>	R2-1-3648
<b>45</b>	
<b>PHOTO ENFORCED</b>	R10-1108P-3618
<b>\$250 FINE MINIMUM</b>	R2-1106P-3618

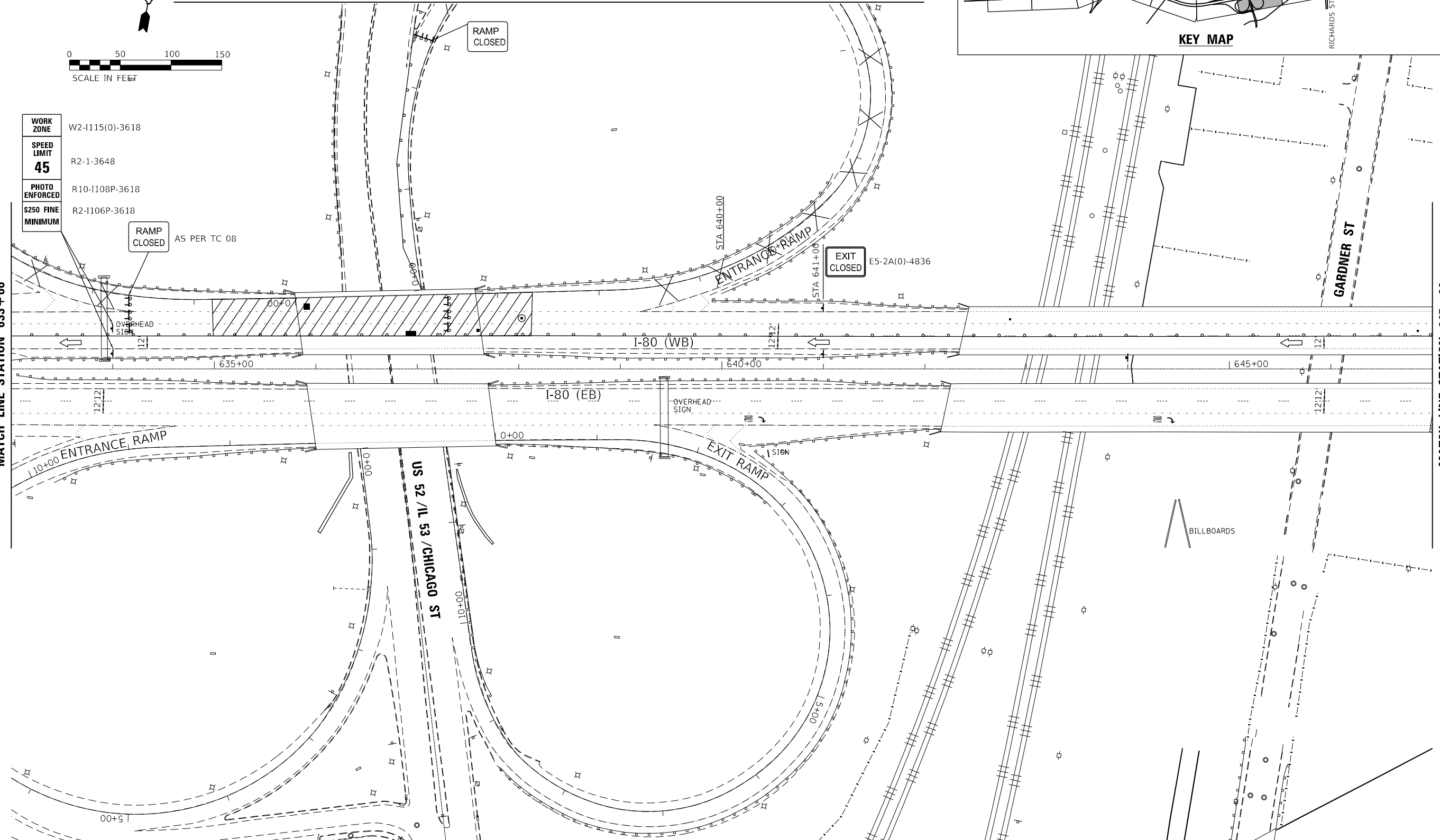
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RAMP CLOSED

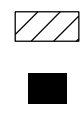
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MATCH LINE STATION 633 + 00

MATCH LINE STATION 647 + 00



**MOT PLAN LEGEND**



WORK AREA  
DECK PATCHES TO BE COMPLETED IN THIS STAGE

DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS  
TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

DIRECTION OF TRAFFIC  
ARROW BOARD

CONSTRUCTION SIGN  
SPOTTER

TYPE III BARRICADES WITH FLASHING LIGHTS



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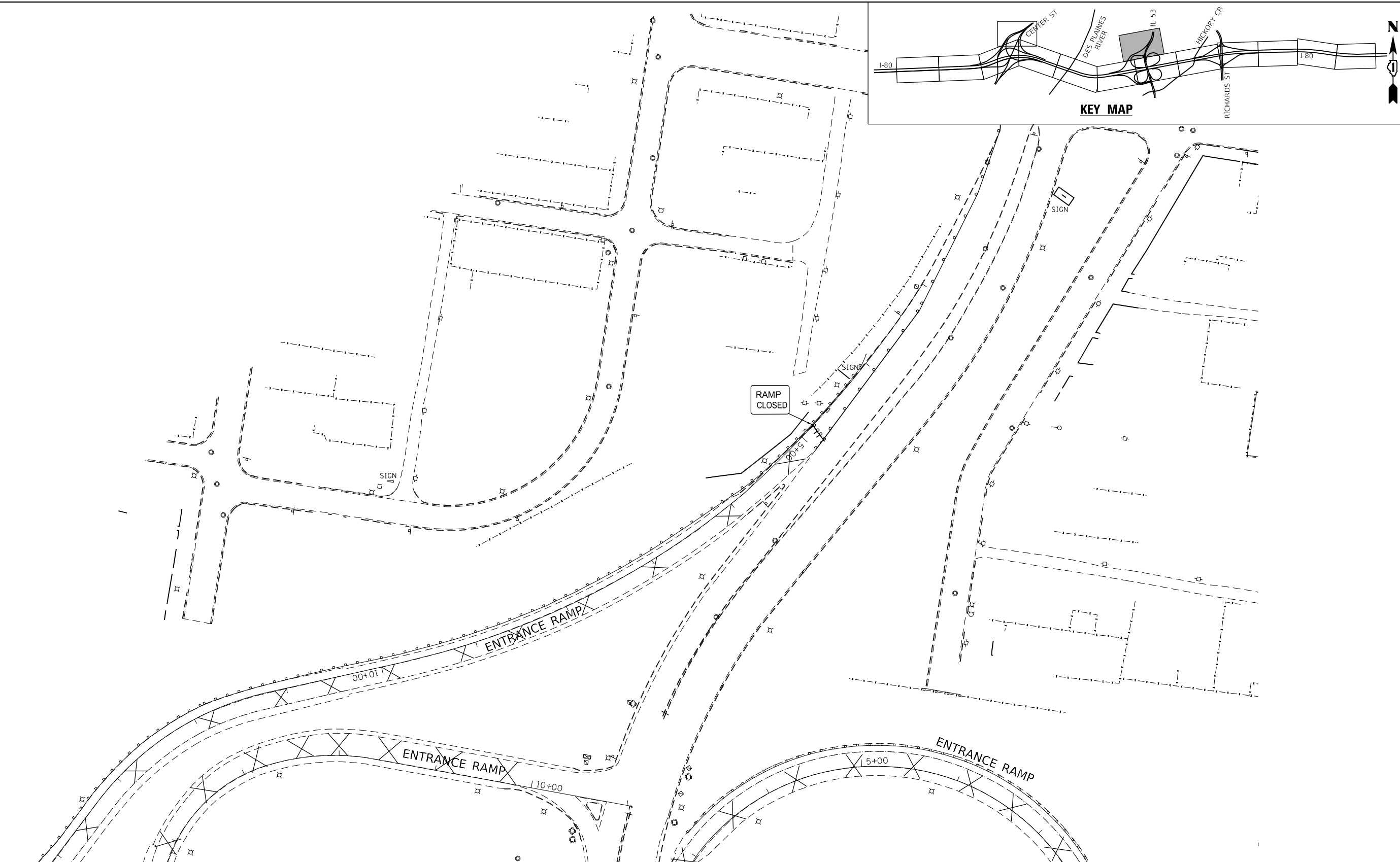
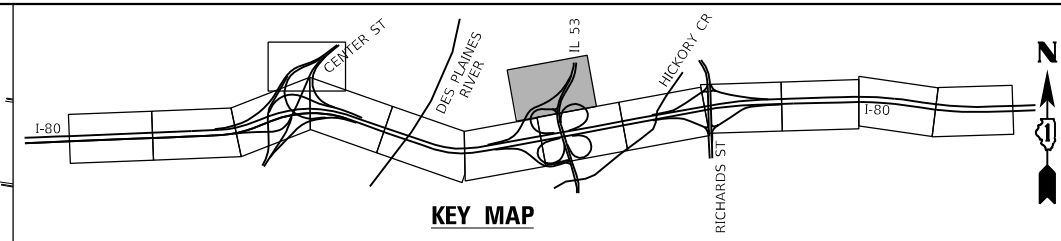
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DRAWN	- AB	REVISED	-
CHECKED	- LRC	REVISED	-
DATE	- 01/31/2020	REVISED	-

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC - STAGE 4B**  
**I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**  
SCALE: 100:0.0000 " = 1" / in. SHEET 5 OF 8 SHEETS STA. 633+00 TO STA. 647+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	56
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				



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



MATCH LINE SEE SHEET STAGE 4B- 04 AND 05

**MOT PLAN LEGEND**

-  WORK AREA
-  DECK PATCHES TO BE COMPLETED IN THIS STAGE

-  DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
-  TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

-  DIRECTION OF TRAFFIC
-  ARROW BOARD

-  CONSTRUCTION SIGN
-  SPOTTER
-  TYPE III BARRICADES WITH FLASHING LIGHTS

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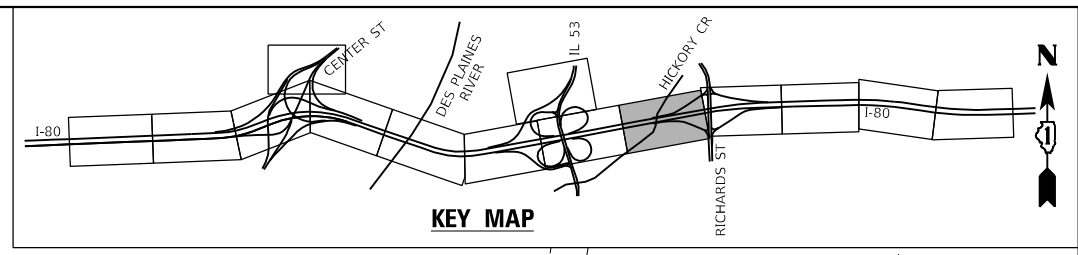
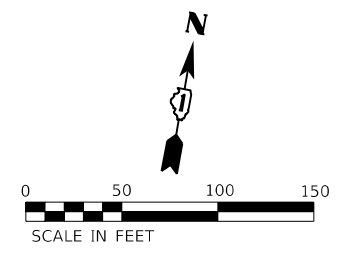
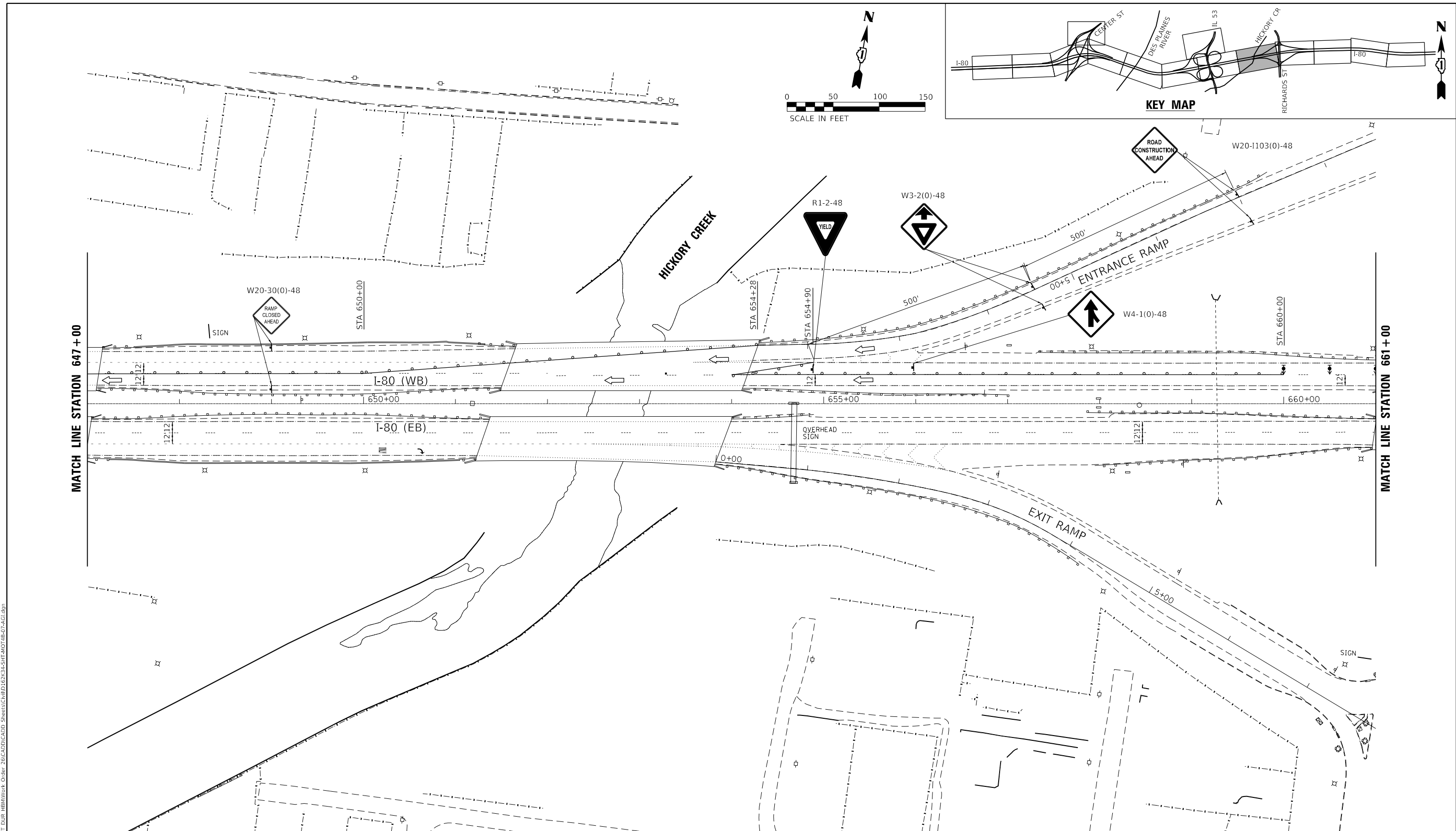
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC - STAGE 4B  
I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**

SCALE: 100:0.0000 '"/>

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	57
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

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**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE
- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)
- DIRECTION OF TRAFFIC
- ARROW BOARD
- CONSTRUCTION SIGN
- SPOTTER
- TYPE III BARRICADES WITH FLASHING LIGHTS

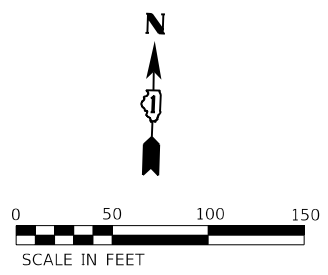
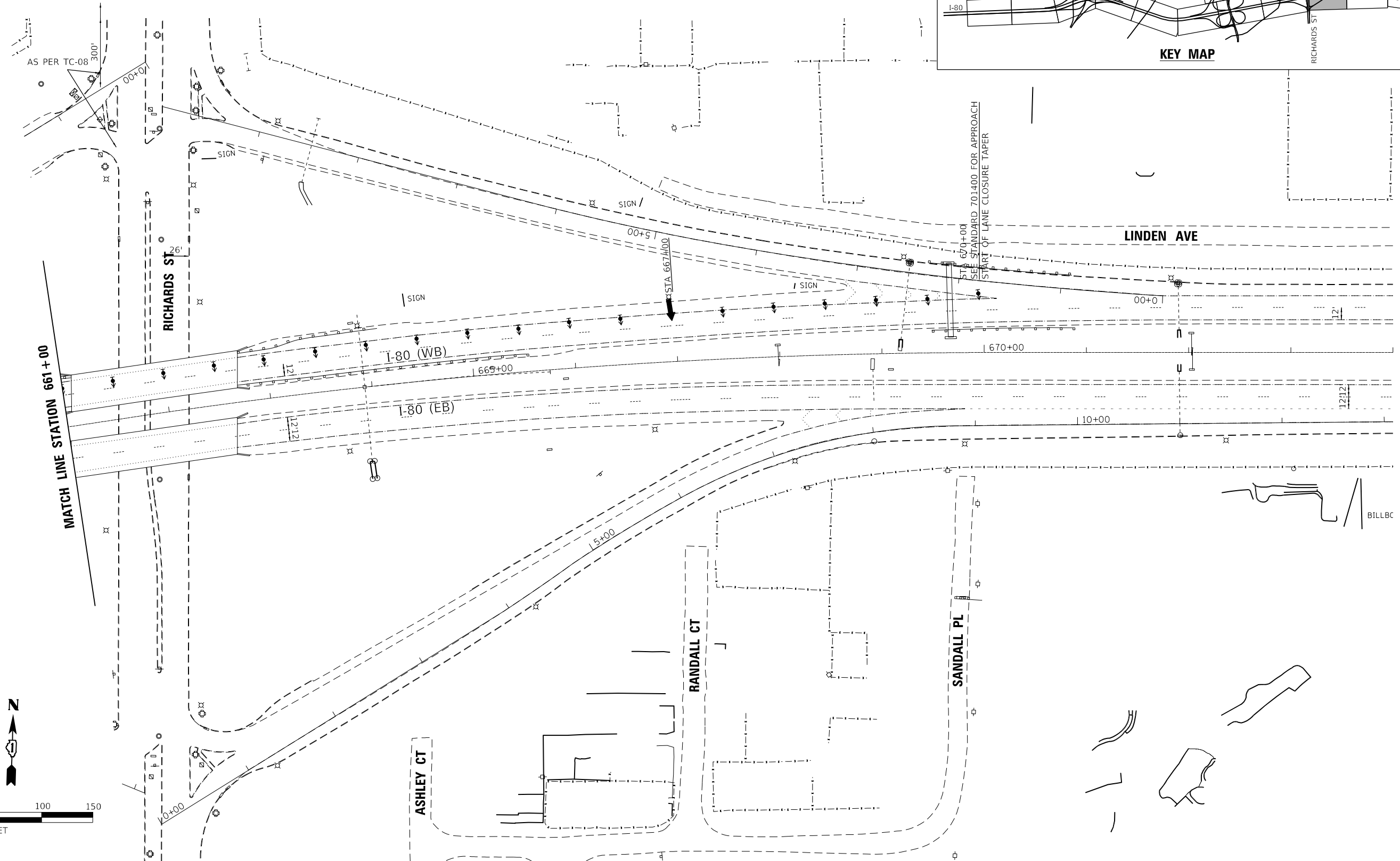
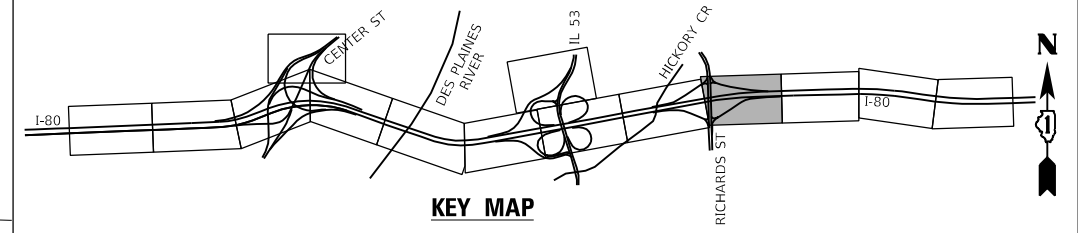


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	DRAWN - AB	REVISED -
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PLOT DATE = 2/3/2020	DATE - 01/31/2020	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC - STAGE 4B**  
**I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**  
 SCALE: 100:0.0000 '"/>

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	58
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				



**MOT PLAN LEGEND**

- WORK AREA
- DECK PATCHES TO BE COMPLETED IN THIS STAGE

- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONO-DIRECTIONAL LIGHT AT 50' CENTERS
- TYPE II BARRICADES OR DRUMS (WITH STEADY BURNING MONO-DIRECTIONAL LIGHT IN WEAVES, TAPERS AND GORES) (NO LIGHTS IN TANGENTS) (25' CENTERS ALONG TAPERS AND RAMPS)

- DIRECTION OF TRAFFIC
- ARROW BOARD

- CONSTRUCTION SIGN
- SPOTTER

- TYPE III BARRICADES WITH FLASHING LIGHTS



USER NAME = johnn	DESIGNED - AB	REVISED -
	DRAWN - AB	REVISED -
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC - STAGE 4B**  
**I-80 DECK PATCHING FROM CENTER STREET TO ROWELL AVENUE**  
 SCALE: 100:0.0000 '"/>

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	59
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

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Existing Structure:  
 S.N. 099-0052 carrying I-80 Eastbound over Center Street Ramp was originally constructed in 1964 as F.A.I. Route 80, Section 99-3HB-1. The structure consists of a three-span continuous wide flange beam and reinforced concrete deck superstructure supported by concrete stub abutments and multi column piers. The skew is 19°29'28" forward left. The parapets and joints were replaced in 1982, and the deck and joints repaired in 1992, 1998, 2001, and 2011.

Stage construction shall be utilized to maintain traffic during construction.

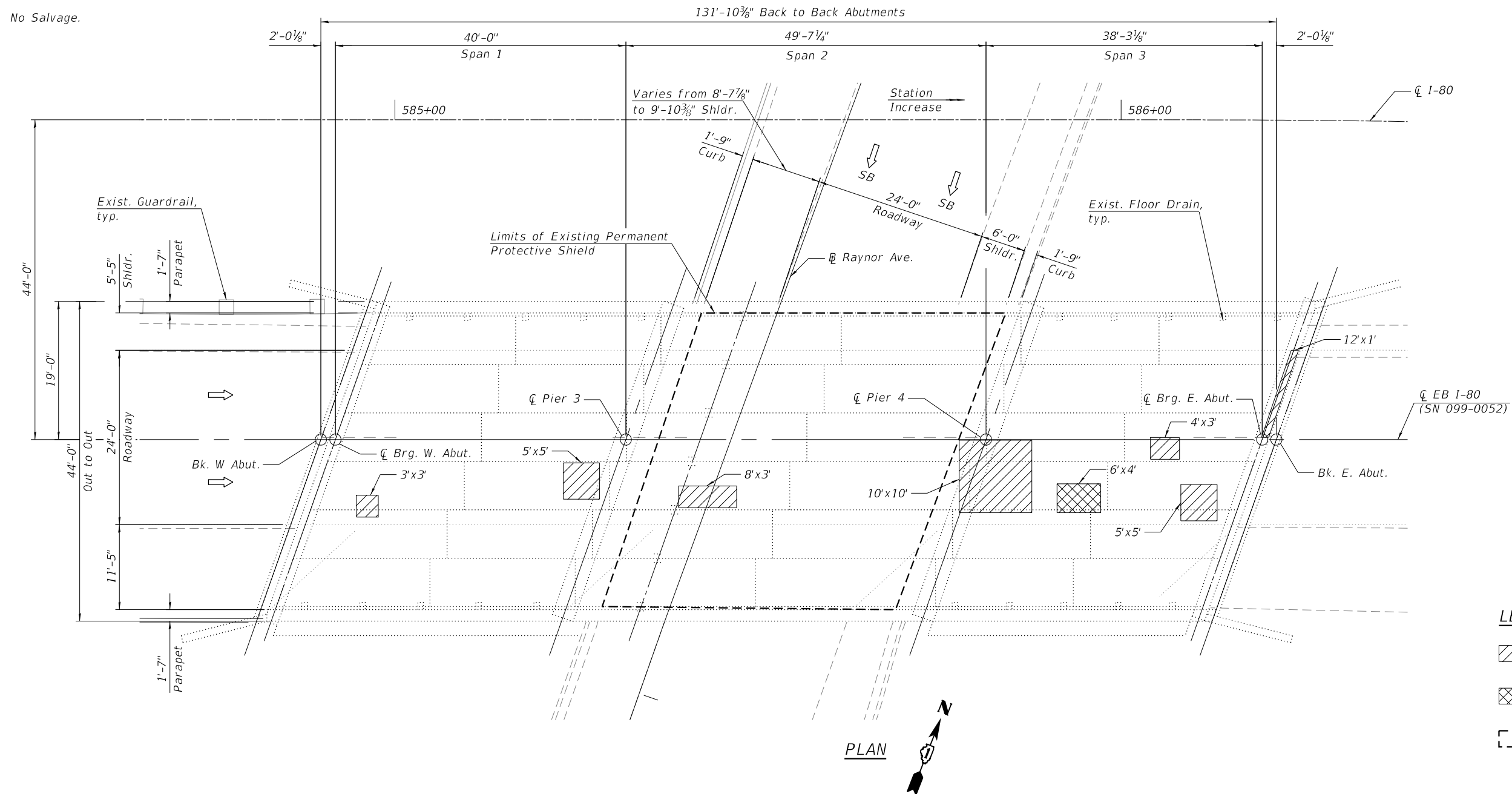
No Salvage.

**SCOPE OF WORK**

1. Perform full depth and partial depth deck slab repair.

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	QUANTITY
Hot-Mix Asphalt Surface Removal (Deck)	Sq. Yd.	26
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	3
Deck Slab Repair (Partial)	Sq. Yd.	23



**NOTES**

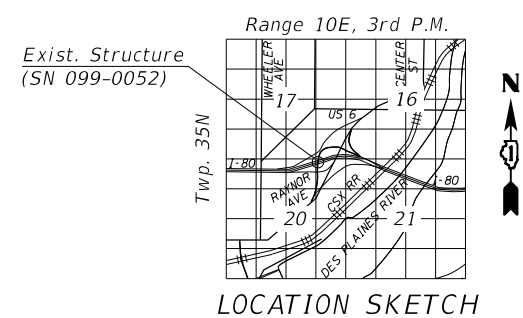
1. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
2. Contractor shall not scale dimensions from the contract plans for construction purposes. Scales shown are for information only.
3. Deck slab repair areas are based upon inspection data provided by IDOT D1 Maintenance at the time of plan preparation. Actual areas to be repaired shall be determined by the Engineer in the field at the time of construction and shown on As-built plans.
4. Deck slab repair concrete shall be placed up to top of existing asphalt overlay. Cost included with Deck Slab Repair, of the type specified. See Patching Detail on Sheet 4.

**LEGEND**

- Deck Slab Repair (Partial)
- Deck Slab Repair (Full Depth, Type II)
- Limits of Existing Permanent Protective Shield
- EB Eastbound
- SB Southbound



Signed Moussa A. Issa  
 Date 01/31/2020



MODEL: Default  
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DECK REPAIRS  
 EB I-80 OVER RAYNOR AVE (S.N. 099-0052)**

SHEET S1-1 OF S1-1 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	60

CONTRACT NO. 62K34  
 ILLINOIS FED. AID PROJECT



Existing Structure:  
 S.N. 099-0053 carrying I-80 Westbound over Center Street Ramp was originally constructed in 1964 as F.A.I. Route 80, Section 99-3HB-1. The structure consists of a three-span continuous wide flange beam and reinforced concrete deck superstructure supported by concrete stub abutments and multi column piers. The skew is 24°36'0" forward left. The parapets and joints were replaced in 1982, and the deck and joints repaired in 1992, 1998, 2001, and 2011.

Stage construction shall be utilized to maintain traffic during construction.

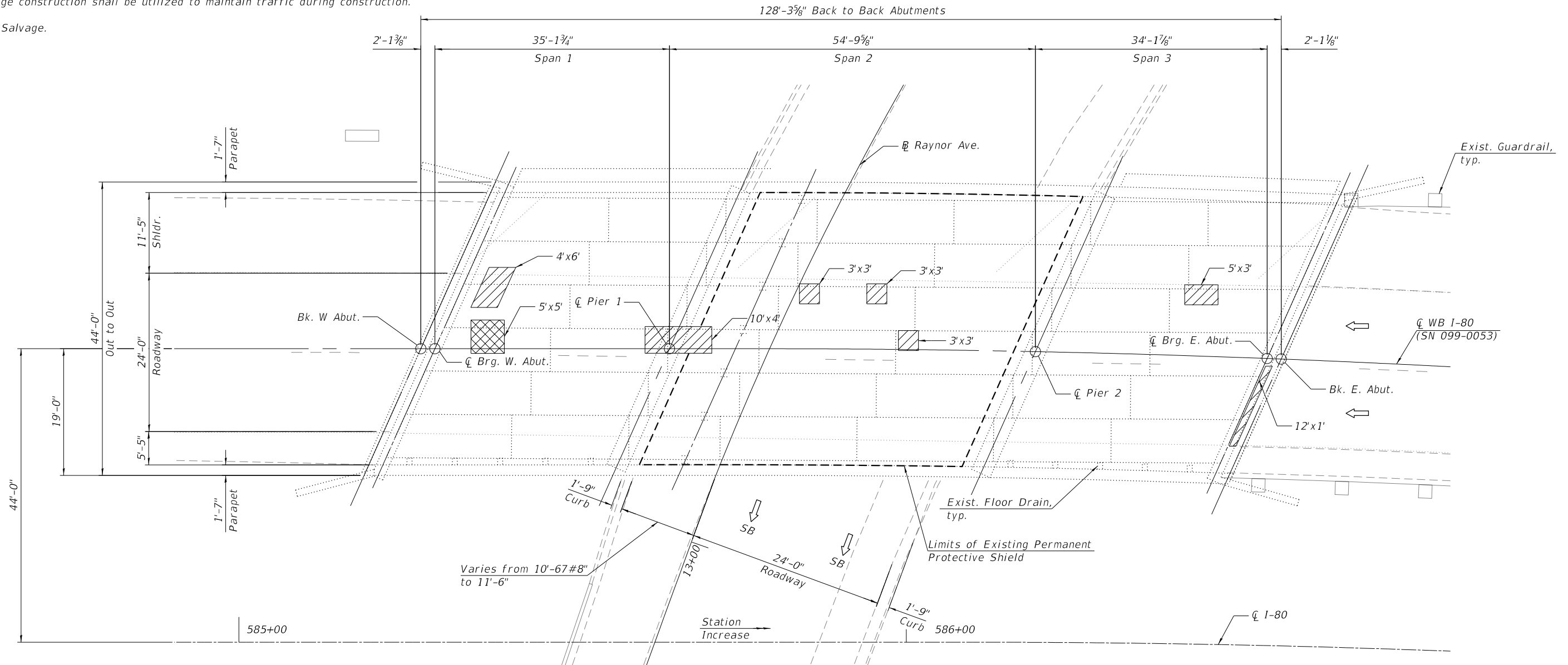
No Salvage.

**SCOPE OF WORK**

1. Perform full depth and partial depth deck slab repair.

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	QUANTITY
Hot-Mix Asphalt Surface Removal (Deck)	Sq. Yd.	17
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	3
Deck Slab Repair (Partial)	Sq. Yd.	14



**NOTES**

1. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
2. Contractor shall not scale dimensions from the contract plans for construction purposes. Scales shown are for information only.
3. Deck slab repair areas are based upon inspection data provided by IDOT D1 Maintenance at the time of plan preparation. Actual areas to be repaired shall be determined by the Engineer in the field at the time of construction and shown on As-built plans.
4. Deck slab repair concrete shall be placed up to top of existing asphalt overlay. Cost included with Deck Slab Repair, of the type specified. See Patching Detail on Sheet 4.

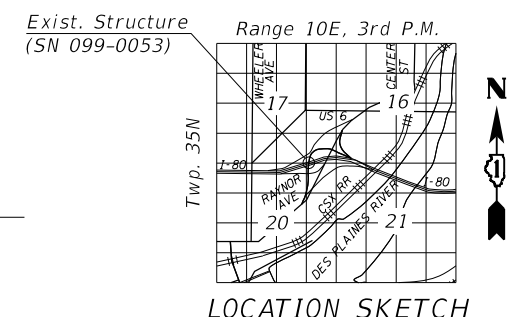
**PLAN**

**LEGEND**

- Deck Slab Repair (Partial)
- Deck Slab Repair (Full Depth, Type II)
- Limits of Existing Permanent Protective Shield
- WB Westbound
- SB Southbound



Signed: *Moussa A. Issa*  
 Date: 01/31/2020



MODEL: Default  
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DECK REPAIRS  
 WB I-80 OVER RAYNOR AVENUE (S.N. 099-0053)**

SHEET S2-1 OF S2-1 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	61
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

Existing Structure:  
 S.N. 099-0055 carrying I-80 Eastbound over Southbound Center Street to Eastbound I-80 Ramp was originally constructed in 1964 as F.A.I. Route 80, Section 99-3HB-2. The structure consists of a single span wide flange beam and reinforced concrete deck superstructure supported by stub abutments. The skew is 49°26'38" forward right tangent to C I-80 at Sta. 590+22.78. The deck was repaired in 1992, 2001 and 2011.

**SCOPE OF WORK**

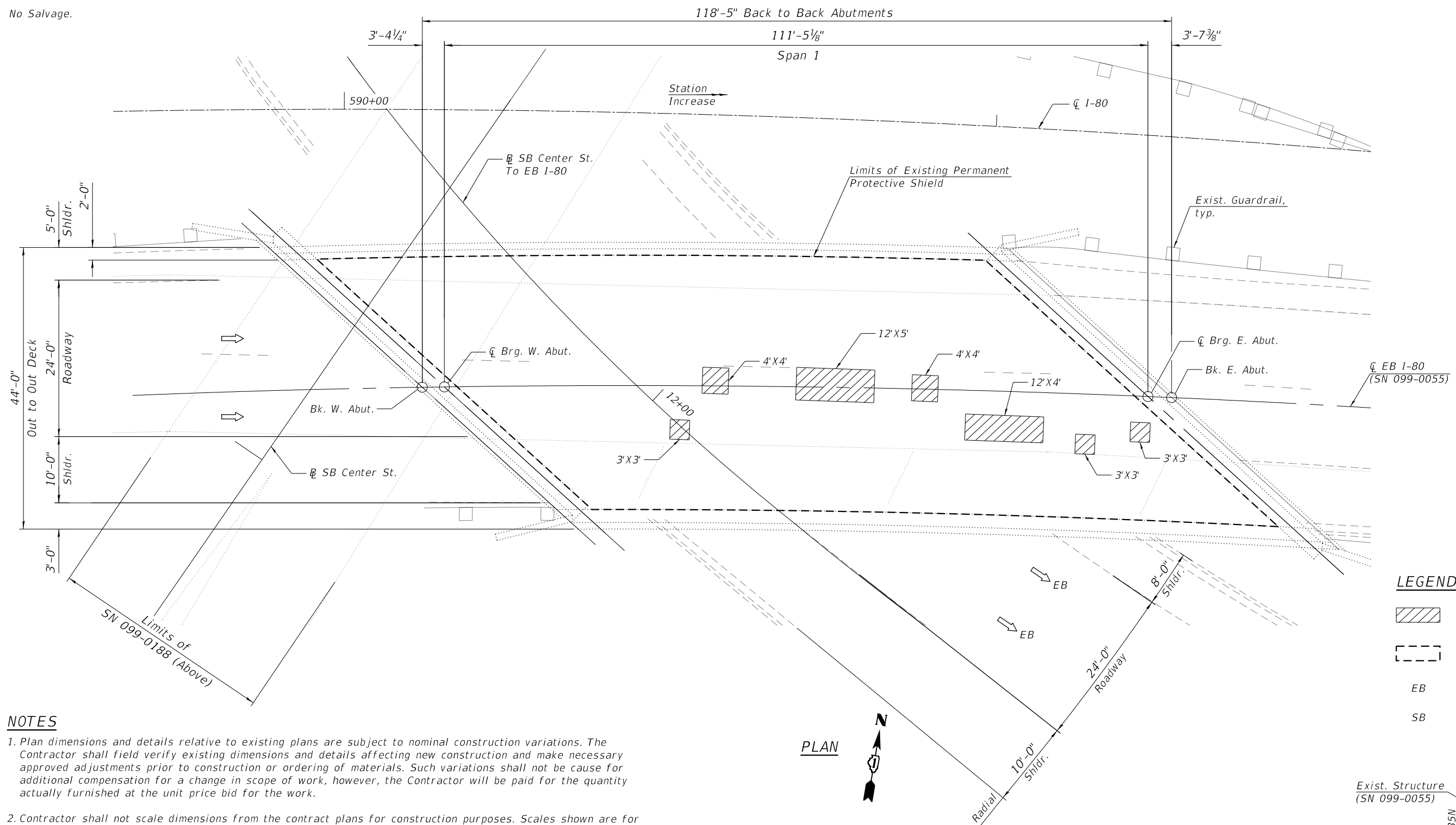
1. Perform partial depth deck slab repair.

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	QUANTITY
Hot-Mix Asphalt Surface Removal (Deck)	Sq. Yd.	19
Deck Slab Repair (Partial)	Sq. Yd.	19

Stage construction shall be utilized to maintain traffic during construction.

No Salvage.



**LEGEND**

- Deck Slab Repair (Partial)
- Limits of Existing Permanent Protective Shield
- EB Eastbound
- SB Southbound

**NOTES**

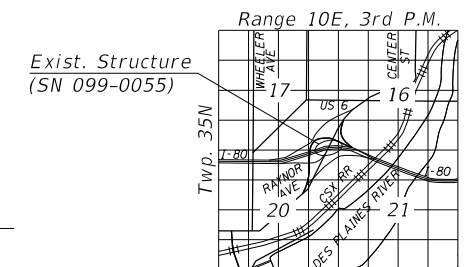
1. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
2. Contractor shall not scale dimensions from the contract plans for construction purposes. Scales shown are for information only.
3. Deck slab repair areas are based upon inspection data provided by IDOT D1 Maintenance at the time of plan preparation. Actual areas to be repaired shall be determined by the Engineer in the field at the time of construction and shown on As-built plans.
4. Deck slab repair concrete shall be placed up to top of existing asphalt overlay. Cost included with Deck Slab Repair (Partial). See Patching Detail on Sheet 4.

PLAN



Signed *Moussa A. Issa*

Date 01/31/2020



LOCATION SKETCH

MODEL: Default  
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

DECK REPAIRS  
 EB I-80 OVER SB CENTER ST TO EB I-80 RAMP (S.N. 099-0055)

SHEET S3-1 OF S3-1 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	62
			CONTRACT NO. 62K34	
ILLINOIS FED. AID PROJECT				

Existing Structure:  
 S.N. 099-0054 carrying I-80 Westbound over Southbound Center Street to Eastbound I-80 Ramp was originally constructed in 1964 as F.A.I. Route 80, Section 99-3HB-2. The structure consists of a single-span wide flange beam and reinforced concrete deck superstructure supported by stub abutments. The skew is 34°38'07" forward right tangent to  $\bar{C}$  I-80 at Sta. 590+22.78. The deck was repaired in 1992, 2001, and 2011.

**SCOPE OF WORK**

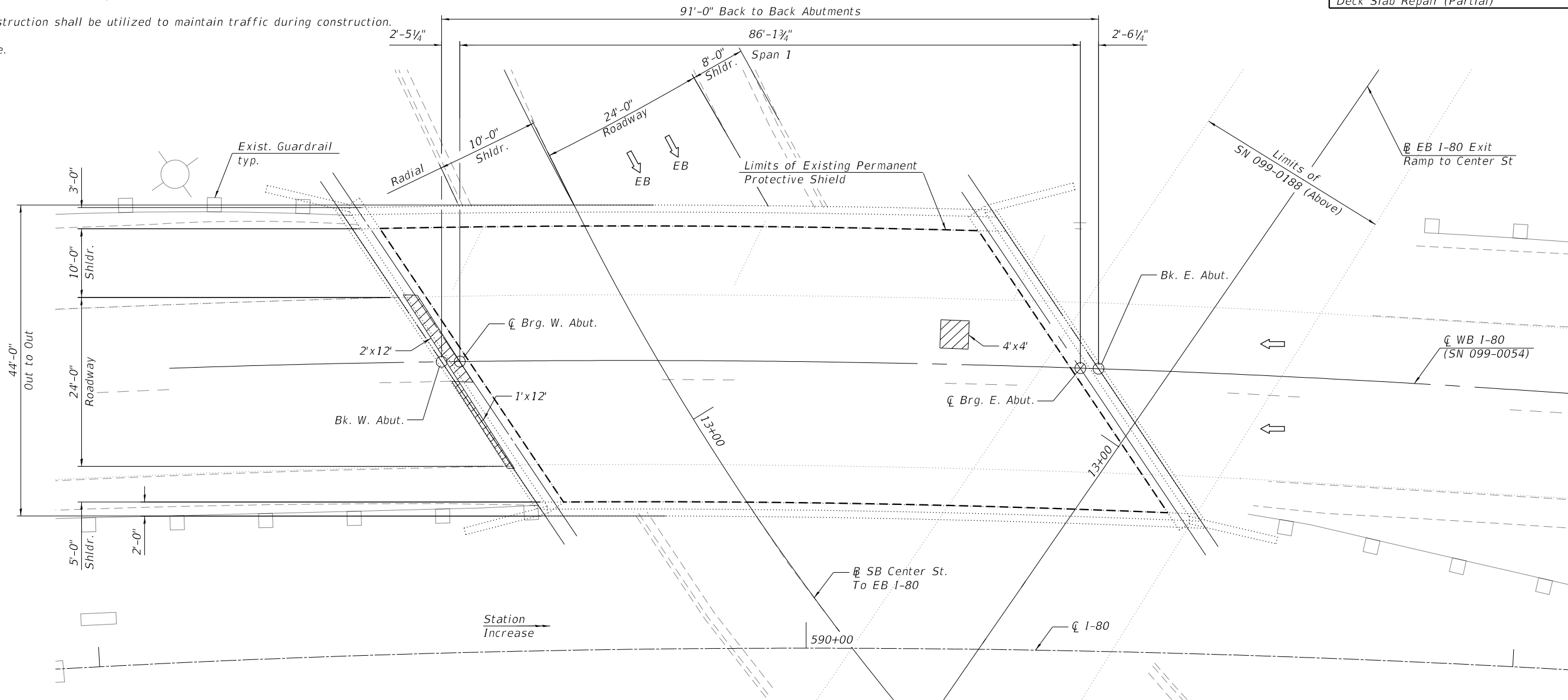
1. Perform partial depth deck slab repair.

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	QUANTITY
Hot-Mix Asphalt Surface Removal (Deck)	Sq. Yd.	6
Deck Slab Repair (Partial)	Sq. Yd.	6

Stage construction shall be utilized to maintain traffic during construction.

No Salvage.



PLAN

**NOTES**

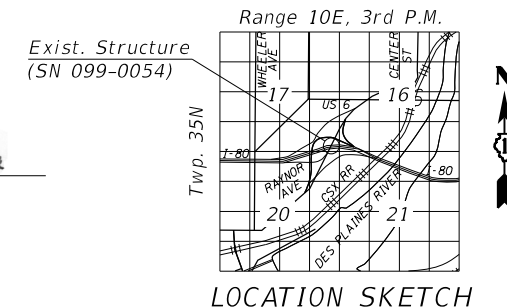
1. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
2. Contractor shall not scale dimensions from the contract plans for construction purposes. Scales shown are for information only.
3. Deck slab repair areas are based upon inspection data provided by IDOT D1 Maintenance at the time of plan preparation. Actual areas to be repaired shall be determined by the Engineer in the field at the time of construction and shown on As-built plans.
4. Deck slab repair concrete shall be placed up to top of existing asphalt overlay. Cost included with Deck Slab Repair (Partial). See Patching Detail on Sheet 4.

**LEGEND**

- Deck Slab Repair (Partial)
- Limits of Existing Permanent Protective Shield
- WB Westbound
- EB Eastbound
- SB Southbound



Signed *Moussa A. Issa*  
 Date 01/31/2020



LOCATION SKETCH

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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

DECK REPAIRS  
 WB I-80 OVER CENTER ST SB TO I-80 EB RAMP (SN 099-0054)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	63
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

SHEET S4-1 OF S4-1 SHEETS

**Existing Structure:**

S.N. 099-0056 was originally constructed in 1962 as F.A.I. 80, Section 99-3D-E&F-P. The existing structure is a 27-span bridge consisting of a 3-span truss over the river and a 6-span west approach and 18-span east approach. The approach spans are composite and non-composite wide flange steel beams. The truss over the river is a Warren truss with verticals. The beams for the approach spans and the floor system for the truss spans support a 7" thick slab. The west approach spans and the truss spans have a 2 3/4" latex concrete overlay. The east approach spans have a Class BD concrete wearing surface with welded wire fabric. The approach slabs have a variable depth polymerized bituminous concrete binder course and surface course. The substructure consists of reinforced concrete stub abutments founded on steel piles and multi-column piers founded on spread footings. The structure was rehabilitated in 1998, 1999, 2001 and 2011.

Stage construction shall be utilized to maintain traffic during construction.

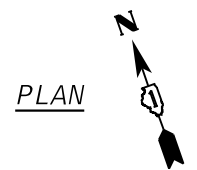
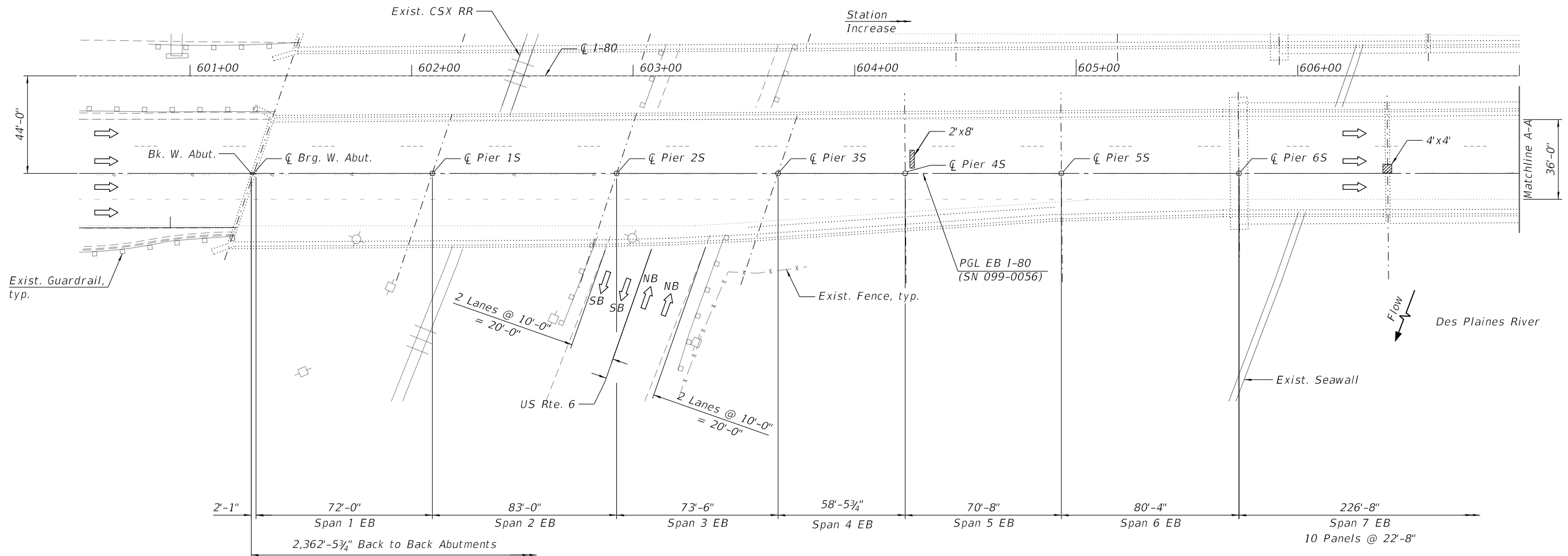
No Salvage.

**SCOPE OF WORK**

1. Perform full depth and partial depth deck slab repair.

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	QUANTITY
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	4
Deck Slab Repair (Partial)	Sq. Yd.	36



**NOTES**

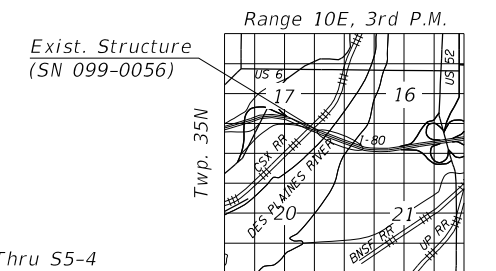
1. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
2. Contractor shall not scale dimensions from the contract plans for construction purposes. Scales shown are for information only.
3. Deck slab repair areas are based upon inspection data provided by IDOT D1 Maintenance at the time of plan preparation. Actual areas to be repaired shall be determined by the Engineer in the field at the time of construction and shown on As-built plans.

**LEGEND**

- Deck Slab Repair (Partial)
- Deck Slab Repair (Full Depth, Type II)
- EB Eastbound
- SB Southbound
- NB Northbound



Signed Moussa A. Issa  
 Date 01/31/2020 For Sheets S5-1 Thru S5-4  
 (Total of 4 Sheets)



**LOCATION SKETCH**

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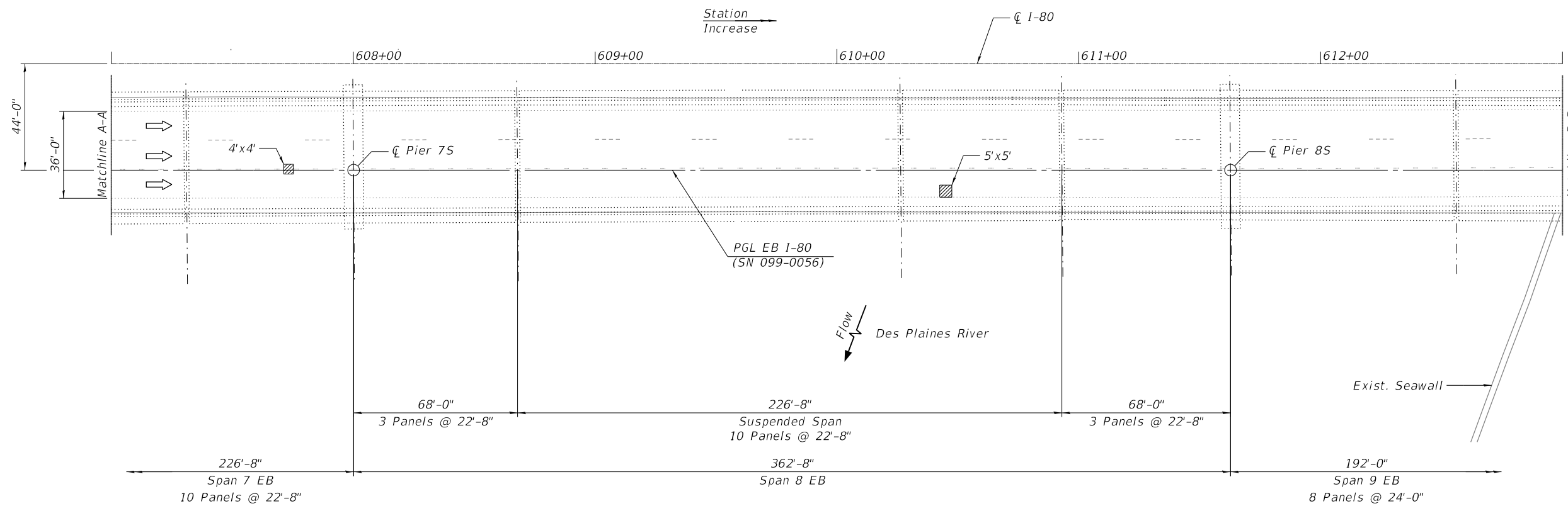
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DECK REPAIRS  
 EB I-80 OVER US 6, CSX RR & DES PLAINES RIVER (S.N. 099-0056)**

SHEET S5-1 OF S5-4 SHEETS

F.A.I. RTE. 80	SECTION 2019-162-BR	COUNTY WILL	TOTAL SHEETS 93	SHEET NO. 64
			CONTRACT NO. 62K34	
		ILLINOIS FED. AID PROJECT		



**NOTE**

1. For Notes, Legend, Scope of Work, and Total Bill of Material, See Sheet S5-1.

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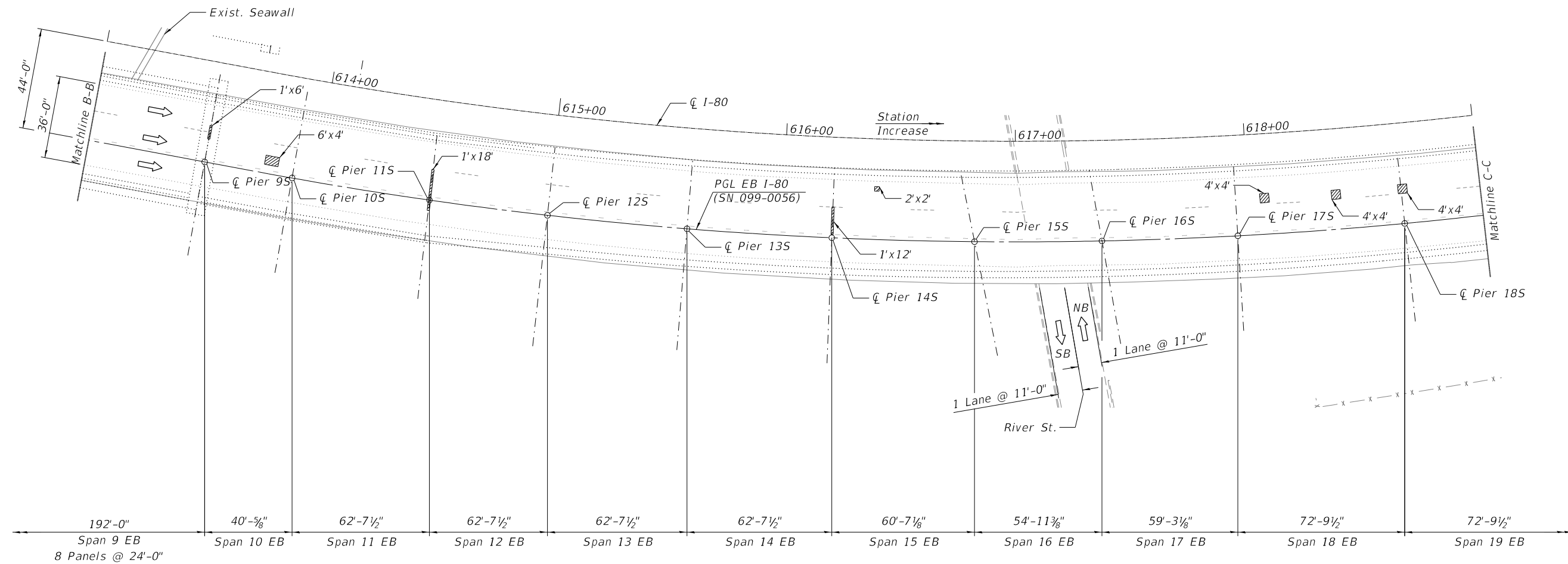
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DECK REPAIRS  
EB I-80 OVER US 6, CSX RR & DES PLAINES RIVER (S.N. 099-0056)

SHEET S5-2 OF S5-4 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	65
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62K34	



**NOTE**

1. For Notes, Legend, Scope of Work, and Total Bill of Material, See Sheet S5-1.

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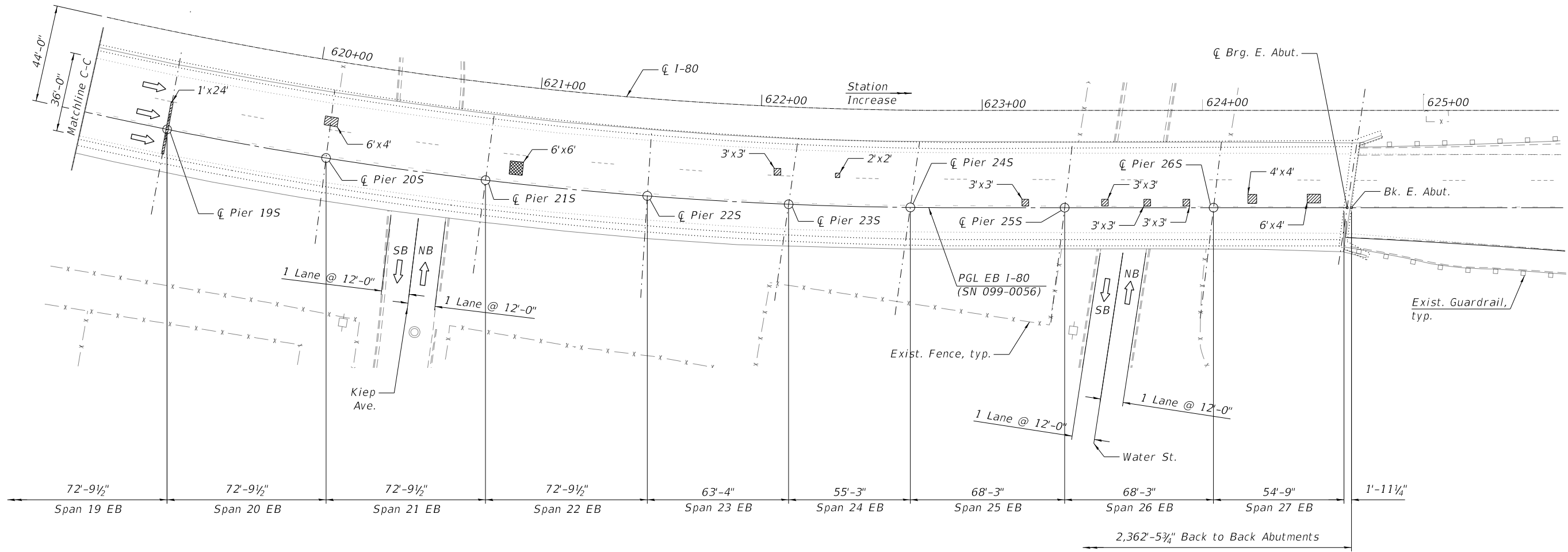


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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DECK REPAIRS**  
**EB I-80 OVER US 6, CSX RR & DES PLAINES RIVER (S.N. 099-0056)**  
 SHEET S5-3 OF S5-4 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	66
			CONTRACT NO. 62K34	
		ILLINOIS FED. AID PROJECT		



PLAN



**NOTE**

1. For Notes, Legend, Scope of Work, and Total Bill of Material, See Sheet S5-1.

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DECK REPAIRS  
EB I-80 OVER US 6, CSX RR & DES PLAINES RIVER (S.N. 099-0056)**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	67
			CONTRACT NO. 62K34	
		ILLINOIS FED. AID PROJECT		

SHEET S5-4 OF S5-4 SHEETS

**Existing Structure:**  
 S.N. 099-0057 was originally constructed in 1962 as F.A.I. 80, Section 99-3D-E&F-P. The existing structure is a 27-span bridge consisting of a 3-span truss over the river and a 6-span west approach and 18-span east approach. The approach spans are composite and non-composite wide flange steel beams. The truss over the river is a Warren truss with verticals. The beams for the approach spans and the floor system for the truss spans support a 7" thick slab. The west approach spans and the truss spans have a 2 3/4" latex concrete overlay. The east approach spans have a Class BD concrete wearing surface with welded wire fabric. The approach slabs have a variable depth polymerized bituminous concrete binder course and surface course. The substructure consists of reinforced concrete stub abutments founded on steel piles and multi-column piers founded on spread footings. The structure was rehabilitated in 1998, 1999, 2001, 2011.

Stage construction shall be utilized to maintain traffic during construction.

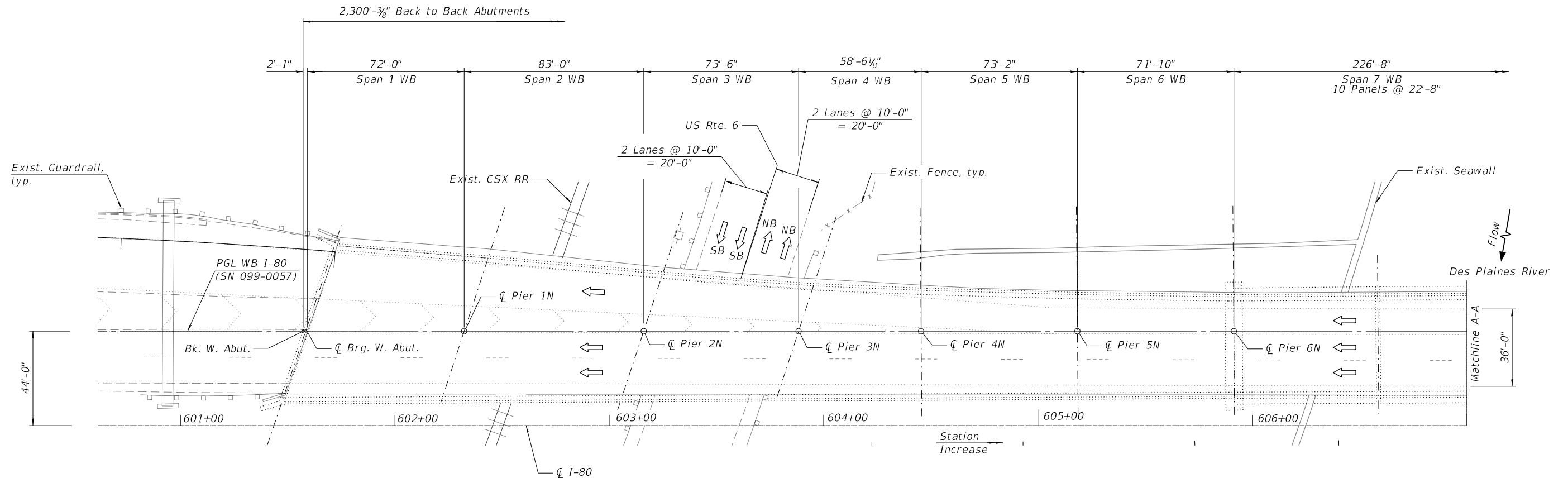
No Salvage.

**SCOPE OF WORK**

1. Perform partial depth deck slab repair.

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	QUANTITY
Deck Slab Repair (Partial)	Sq. Yd.	15



PLAN

**NOTES**

- Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
- Contractor shall not scale dimensions from the contract plans for construction purposes. Scales shown are for information only.
- Deck slab repair areas are based upon inspection data provided by IDOT D1 Maintenance at the time of plan preparation. Actual areas to be repaired shall be determined by the Engineer in the field at the time of construction and shown on As-built plans.

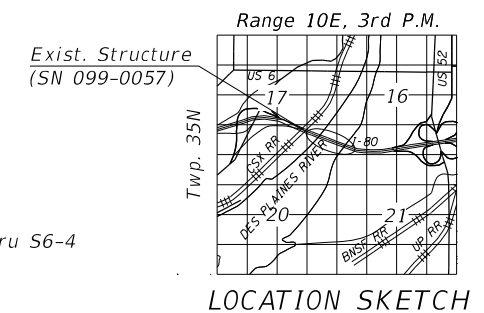
**LEGEND**

- Deck Slab Repair (Partial)
- NB Northbound
- SB Southbound
- WB Westbound



Signed Moussa A. Issa

Date 01/31/2020 For Sheets S6-1 Thru S6-4 (Total of 4 Sheets)



LOCATION SKETCH

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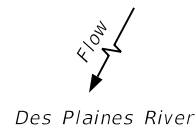
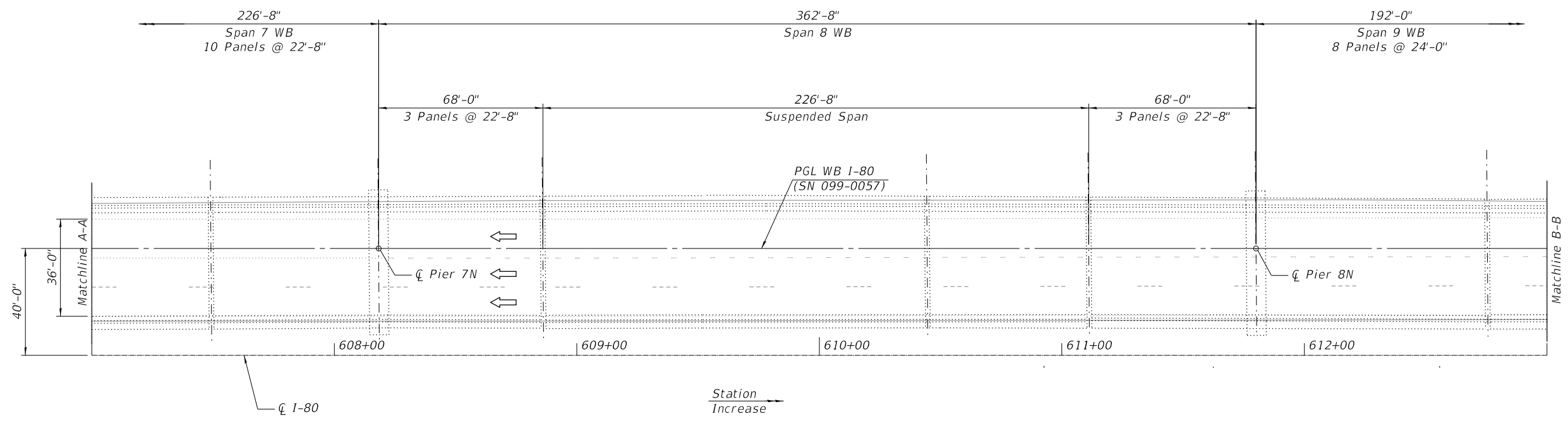
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

DECK REPAIRS  
 WB I-80 OVER US 6, CSX RR & DES PLAINES RIVER (S.N. 099-0057)

F.A.I. RTE. 80	SECTION 2019-162-BR	COUNTY WILL	TOTAL SHEETS 93	SHEET NO. 68
			CONTRACT NO. 62K34	
ILLINOIS FED. AID PROJECT				

SHEET S6-1 OF S6-4 SHEETS





**NOTE**

1. For Notes, Legend, Scope of Work and Total Bill of Material, See Sheet S6-1.

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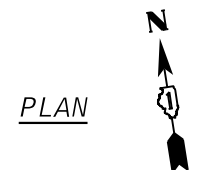
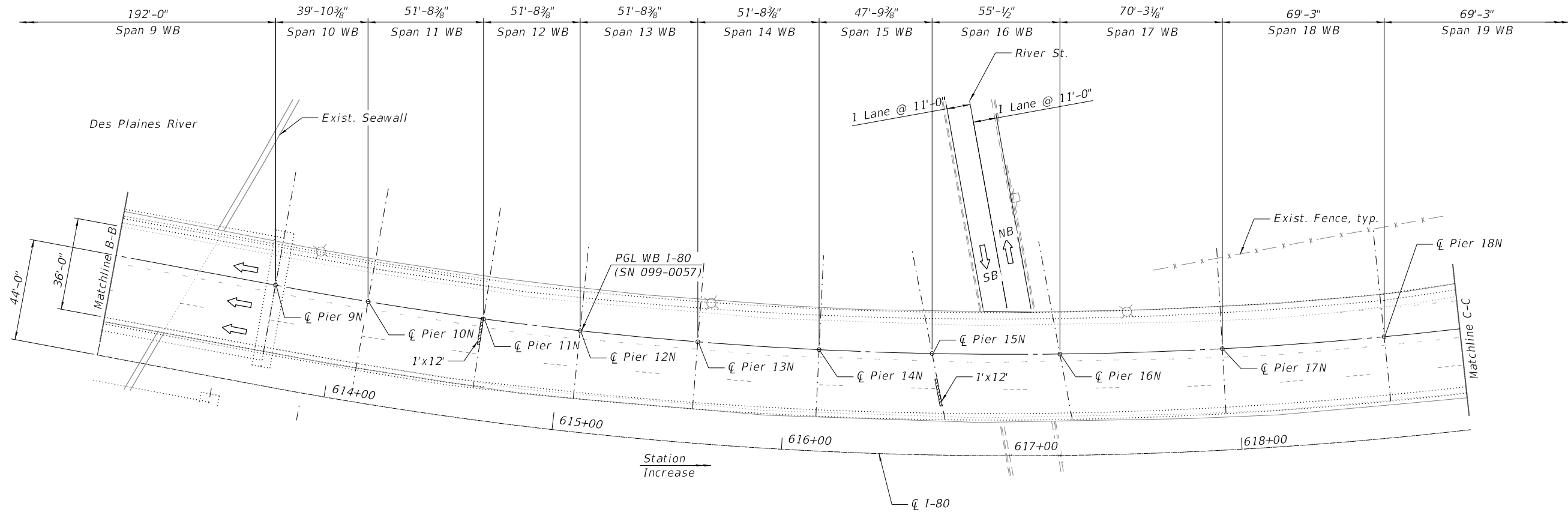


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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DECK REPAIRS  
WB I-80 OVER US 6, CSX RR & DES PLAINES RIVER (S.N. 099-0057)  
SHEET S6-2 OF S6-4 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	69
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62K34	



**NOTE**

1. For Notes, Legend, Scope of Work and Total Bill of Material, see sheet S6-1.

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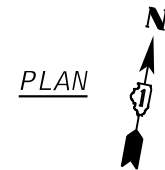
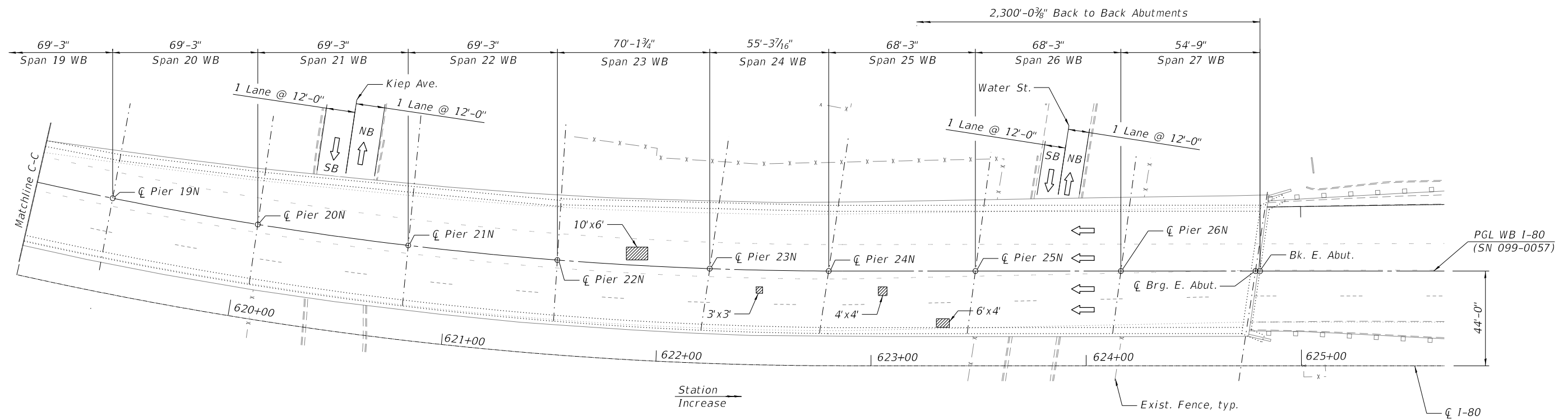
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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**DECK REPAIRS**  
**WB I-80 OVER US 6, CSX RR & DES PLAINES RIVER (S.N. 099-0057)**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	70
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62K34	

SHEET S6-3 OF S6-4 SHEETS



**NOTE**

1. For Notes, Legend, Scope of Work and Total Bill of Material, see sheet S6-1.

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DECK REPAIRS  
WB I-80 OVER US 6, CSX RR & DES PLAINES RIVER (S.N. 099-0057)**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	71
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62K34	

SHEET S6-4 OF S6-4 SHEETS

**Existing Structure:**  
 Bridge over IL Rte 53 Chicago Street, SN 099-0059 carrying I-80 Eastbound, was originally constructed in 1962 as a part of F.A.I. 80 Project, I-80-4(31)134, Section 99-4.99-4(B.HB). The superstructure consists of 3 simple span steel multi-girder units supported on concrete abutments and piers. The existing bridge decks consist of 7-inch reinforced concrete composite slab with 2" bituminous overlay and waterproofing membrane. The transverse deck joints are PJS type with vertical armor plates. In 1971 the longitudinal deck joint was eliminated. In 1990 and 1998 repairs were made to the decks, abutments, piers, deck joints, rail and drainage system. In 2001, the bituminous overlay and waterproofing membrane were replaced. The structure was fully painted in 1985; the facias and beam ends under joints were repainted in 2003. The deck, joints, abutments and the piers were repaired in 2011.

Stage construction shall be utilized to maintain traffic during construction.

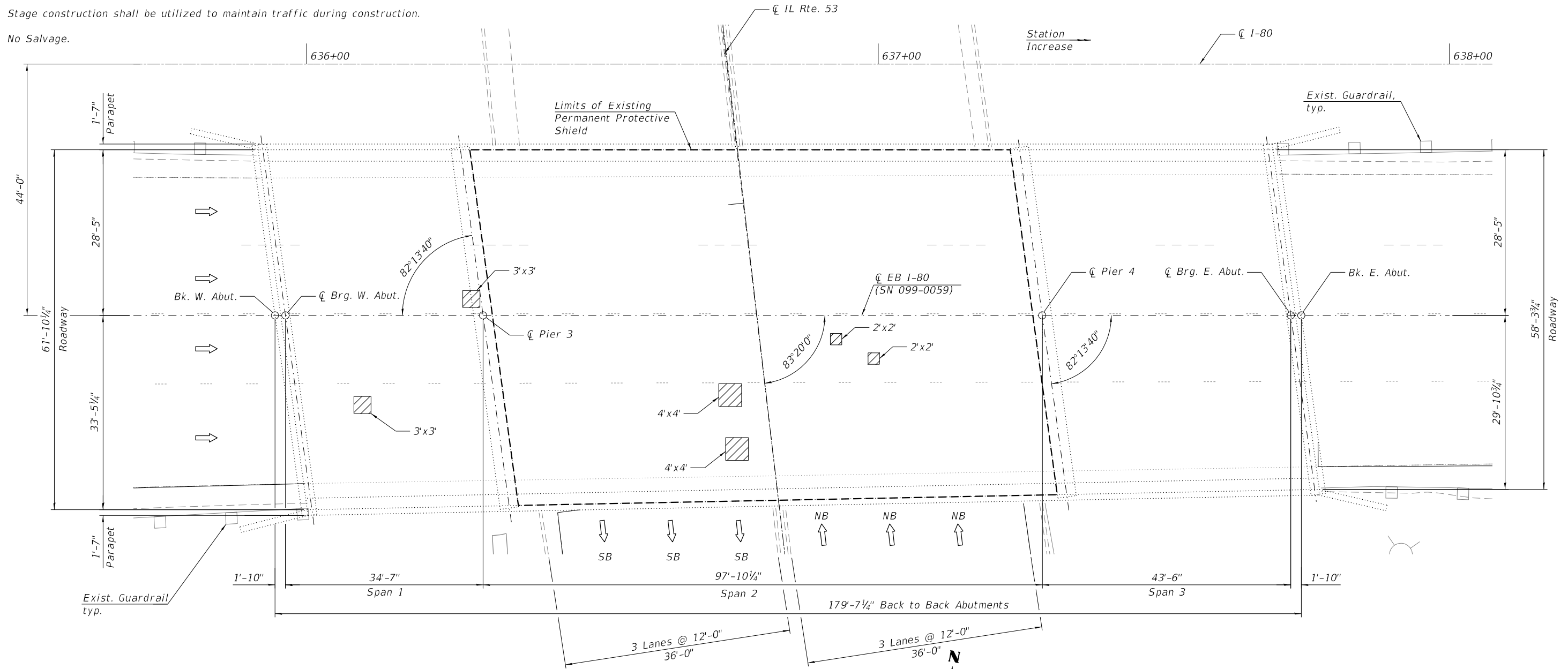
No Salvage.

**SCOPE OF WORK**

1. Perform partial depth deck slab repair.

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	QUANTITY
Hot-Mix Asphalt Surface Removal (Deck)	Sq. Yd.	7
Deck Slab Repair (Partial)	Sq. Yd.	7



**NOTES:**

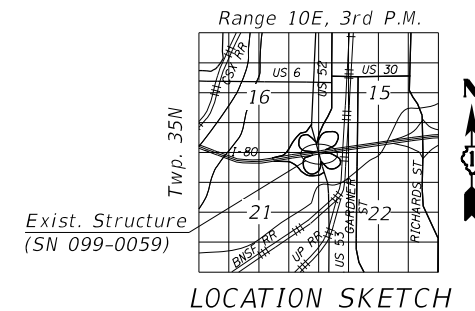
1. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
2. Contractor shall not scale dimensions from the contract plans for construction purposes. Scales shown are for information only.
3. Deck slab repair areas are based upon inspection data provided by IDOT D1 Maintenance at the time of plan preparation. Actual areas to be repaired shall be determined by the Engineer in the field at the time of construction and shown on As-built plans.
4. Deck slab repair concrete shall be placed up to top of existing asphalt overlay. Cost included with Deck Slab Repair (Partial). See Patching Detail on Sheet 4.

**LEGEND**

- Deck Slab Repair (Partial)
- Limits of Existing Permanent Protective Shield
- EB Eastbound
- SB Southbound
- NB Northbound



Signed Moussa A. Issa  
 Date 01/31/2020



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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DECK REPAIRS  
 EB I-80 OVER IL RTE 53 (S.N. 099-0059)**

SHEET S7-1 OF S7-1 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	72
CONTRACT NO. 62K34				
		ILLINOIS	FED. AID PROJECT	

**Existing Structure:**

Bridge over IL Rte 53 Chicago Street, SN 099-0058 carrying I-80 Westbound, was originally constructed in 1962 as a part of F.A.I. 80 Project, I-80-4(31)134, Section 99-4.99-4(B.HB). The superstructure consists of 3 simple span steel multi-girder units supported on concrete abutments and piers. The existing bridge decks consist of 7-inch reinforced concrete composite slab with 2" bituminous overlay and waterproofing membrane. The transverse deck joints are PJS type with vertical armor plates. In 1971 the longitudinal deck joint was eliminated. In 1990 and 1998 repairs were made to the decks, abutments, piers, deck joints, rail and drainage system. In 2001, the bituminous overlay and waterproofing membrane were replaced. The structure was fully painted in 1985; the facias and beam ends under joints were repainted in 2003. In 2011, repairs were made to the deck, abutments and piers, and the deck joints and a portion of the slopewall was removed and replaced.

Stage construction shall be utilized to maintain traffic during construction.

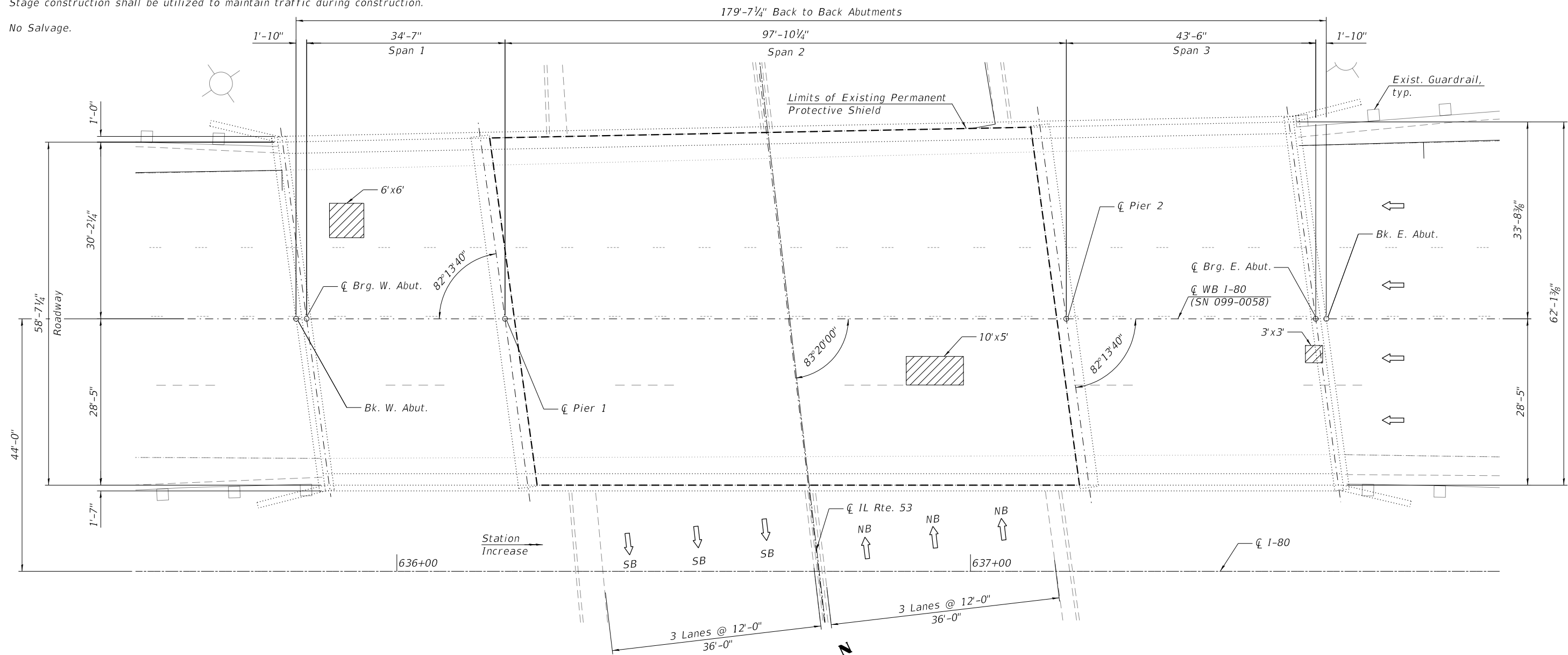
No Salvage.

**SCOPE OF WORK**

1. Perform partial depth deck slab repair.

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	QUANTITY
Hot-Mix Asphalt Surface Removal (Deck)	Sq. Yd.	11
Deck Slab Repair (Partial)	Sq. Yd.	11



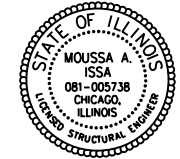
**PLAN**

**NOTES**

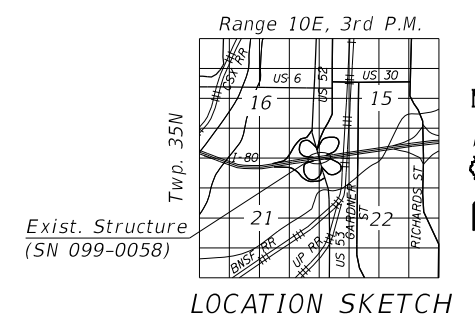
1. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
2. Contractor shall not scale dimensions from the contract plans for construction purposes. Scales shown are for information only.
3. Deck slab repair areas are based upon inspection data provided by IDOT D1 Maintenance at the time of plan preparation. Actual areas to be repaired shall be determined by the Engineer in the field at the time of construction and shown on As-built plans.
4. Deck slab repair concrete shall be placed up to top of existing asphalt overlay. Cost included with Deck Slab Repair (Partial). See Patching Detail on Sheet 4.

**LEGEND**

- Deck Slab Repair (Partial)
- Limits of Existing Permanent Protective Shield
- WB Westbound
- SB Southbound
- NB Northbound



Signed Moussa A. Issa  
Date 01/31/2020



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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DECK REPAIRS  
WB I-80 OVER IL ROUTE 53 (S.N. 099-0058)**

SHEET S8-1 OF S8-1 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	73
CONTRACT NO. 62K34				

ILLINOIS FED. AID PROJECT

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**Existing Structure:**

The existing structure is a seven span, three unit bridge. Spans 8 thru 10 and 12 thru 14 are three span continuous non-composite units and Span 11 is a single span composite unit. All units are steel wide flange beams. The beams support a 7" reinforced concrete slab and a 2" thick waterproof membrane system and polymerized bituminous concrete surface course. The substructure consists of reinforced concrete stub abutments and multi-column piers all founded on steel piles. The structure was originally constructed in 1964 as F.A.I. Route 80 Section 99-4VB and rehabilitated in 1990, 1998, 2001 and 2011.

Stage construction shall be utilized to maintain traffic during construction.

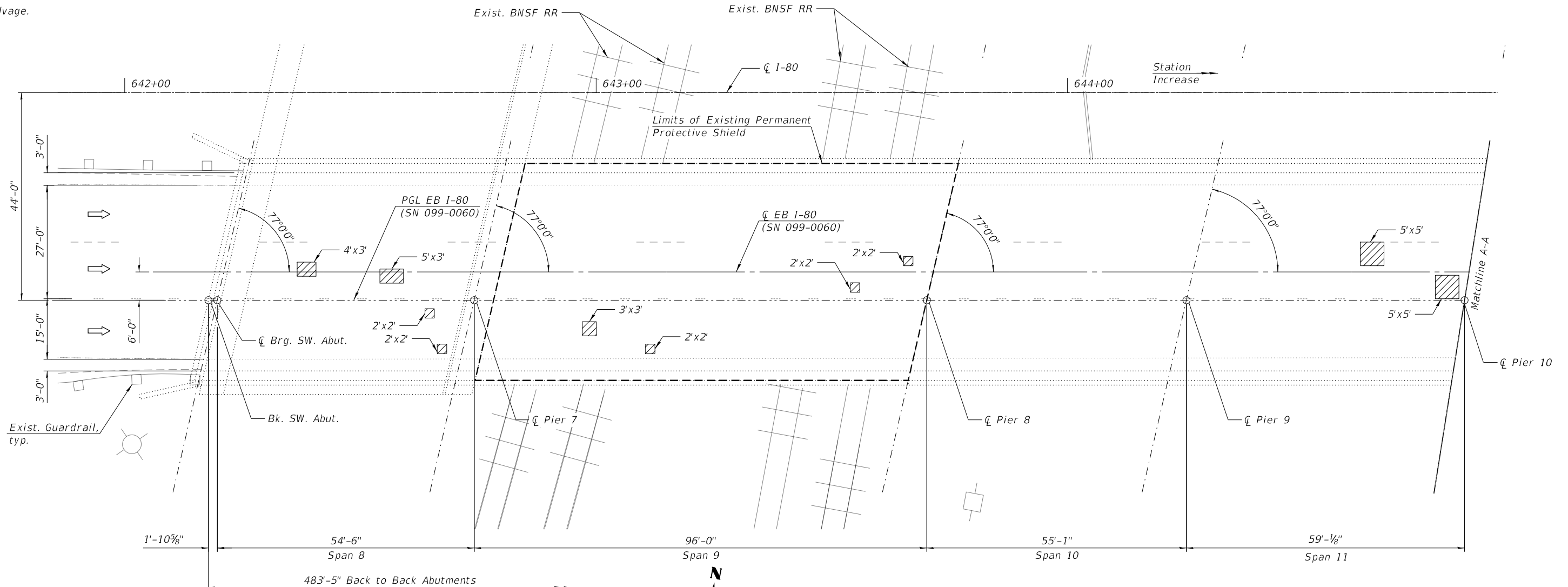
No Salvage.

**SCOPE OF WORK**

1. Perform partial depth deck slab repair.

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	QUANTITY
Hot-Mix Asphalt Surface Removal (Deck)	Sq. Yd.	26
Deck Slab Repair (Partial)	Sq. Yd.	26



PLAN



**NOTES**

1. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
2. Contractor shall not scale dimensions from the contract plans for construction purposes. Scales shown are for information only.
3. Deck slab repair areas are based upon inspection data provided by IDOT D1 Maintenance at the time of plan preparation. Actual areas to be repaired shall be determined by the Engineer in the field at the time of construction and shown on As-built plans.
4. Deck slab repair concrete shall be placed up to top of existing asphalt overlay. Cost included with Deck Slab Repair (Partial). See Patching Detail on Sheet 4.

**LEGEND**

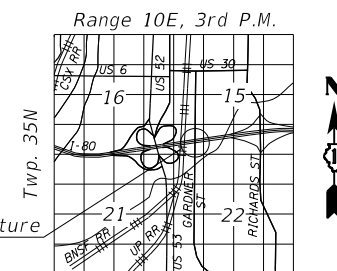
- Deck Slab Repair (Partial)
- Limits of Existing Permanent Protective Shield
- EB Eastbound
- NB Northbound
- SB Southbound



Signed Moussa A. Issa

Date 01/31/2020 For Sheets S9-1 Thru S9-2 (Total of 2 Sheets)

Exist. Structure (099-0060)



**LOCATION SKETCH**

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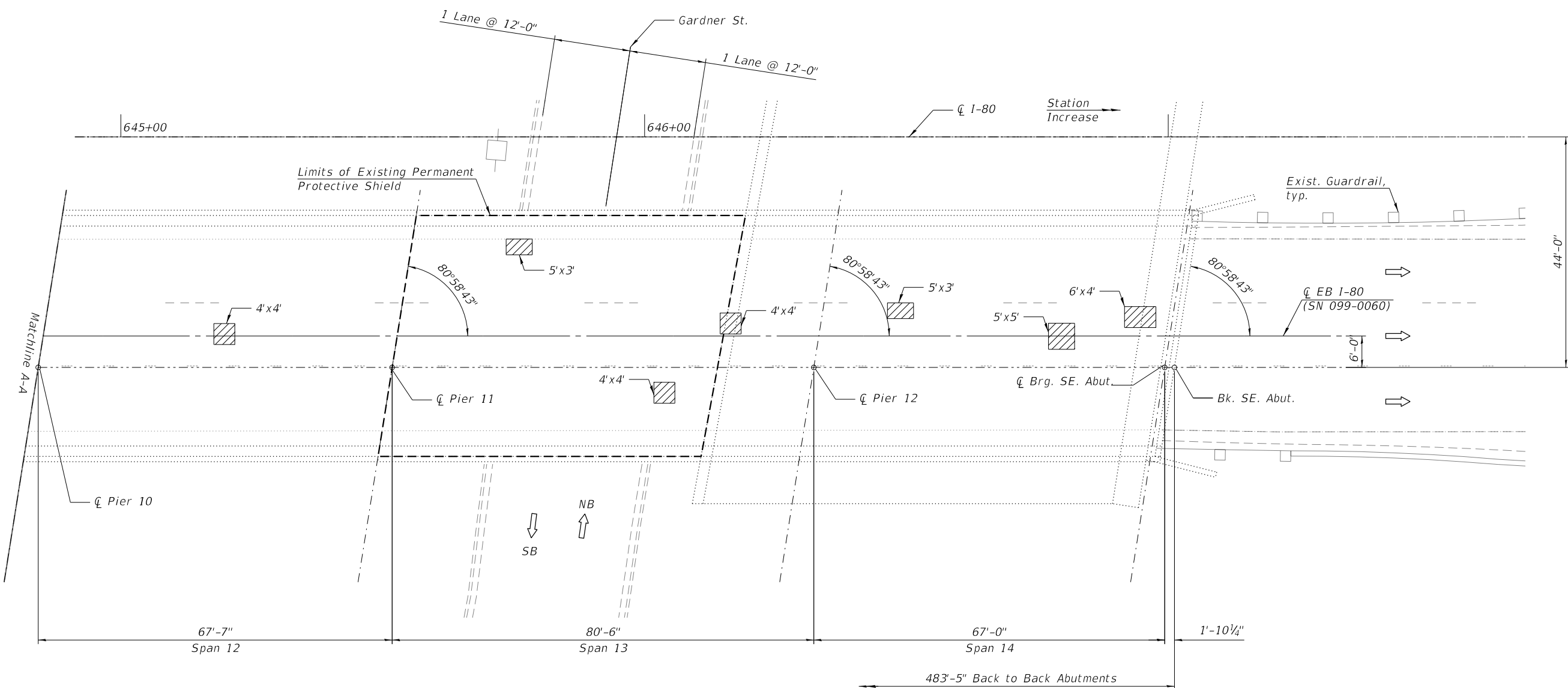
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DECK REPAIRS  
EB I-80 OVER GARDNER ST, BNSF RR & UP RR (S.N. 099-0060)**

SHEET S9-1 OF S9-2 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	74
			CONTRACT NO. 62K34	
ILLINOIS FED. AID PROJECT				



**NOTE**

1. For Notes, Legend, Scope of Work and Total Bill of Material, See Sheet S9-1.

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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**DECK REPAIRS**  
**EB I-80 OVER GARDNER ST, BNSF RR & UP RR (S.N. 099-0060)**

SHEET S9-2 OF S9-2 SHEETS

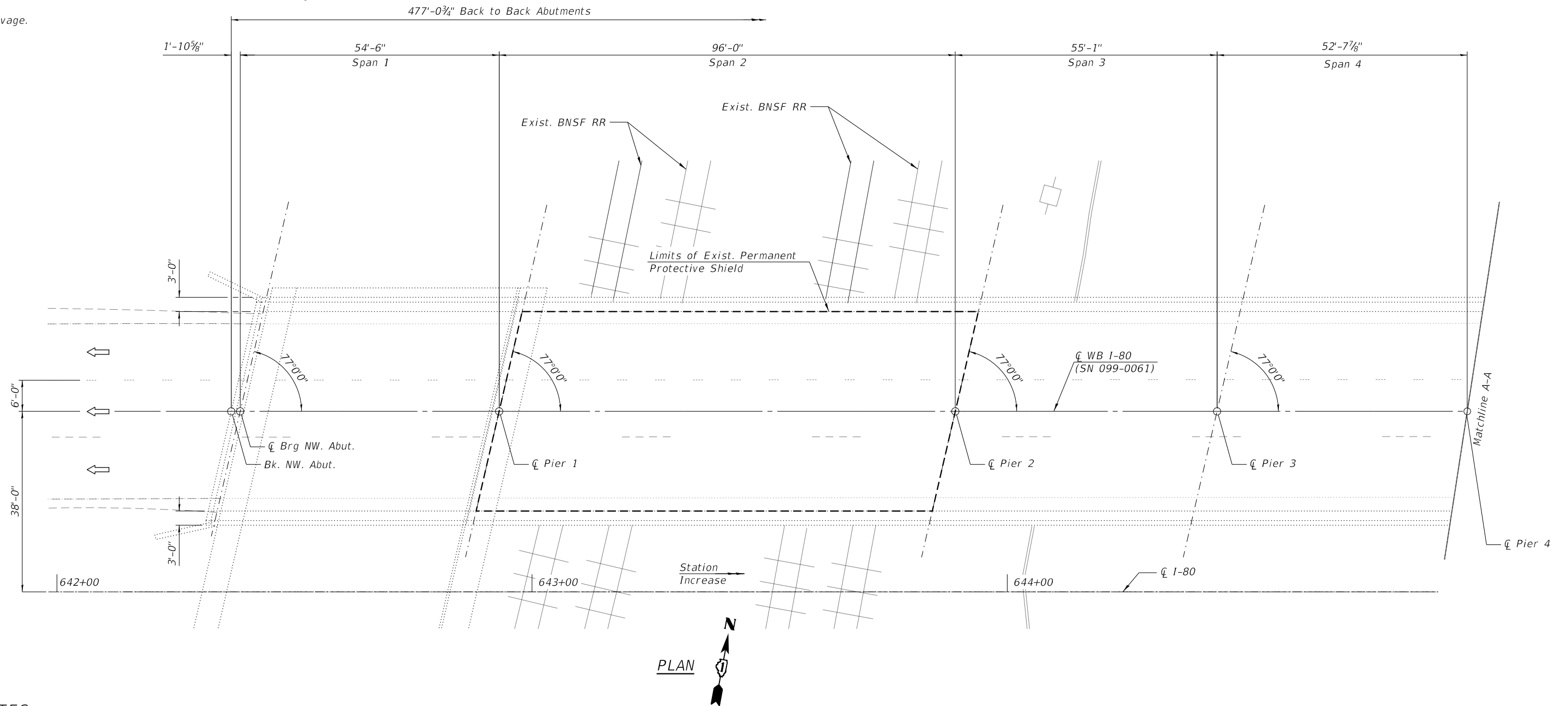
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	75
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62K34	

**Existing Structure:**

The existing structure is a seven span, three unit bridge. Spans 1 thru 3 and 5 thru 7 are three span continuous non-composite units and Span 4 is a single span composite unit. All units are steel wide flange beams. The beams support a 7" reinforced concrete slab and a 2" thick waterproof membrane system and polymerized bituminous concrete surface course. The substructure consists of reinforced concrete stub abutments and multi-column piers all founded on steel piles. The structure was originally constructed in 1964 as F.A.I. Route 80 Section 99-4VB and rehabilitated in 1990, 1998, 2001 and 2011.

Stage construction shall be utilized to maintain traffic during construction.

No Salvage.



**NOTES**

1. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
2. Contractor shall not scale dimensions from the contract plans for construction purposes. Scales shown are for information only.
3. Deck slab repair areas are based upon inspection data provided by IDOT D1 Maintenance at the time of plan preparation. Actual areas to be repaired shall be determined by the Engineer in the field at the time of construction and shown on As-built plans.
4. Deck slab repair concrete shall be placed up to top of existing asphalt overlay. Cost included with Deck Slab Repair, of the type specified. See Patching Detail on Sheet 4.

**LEGEND**

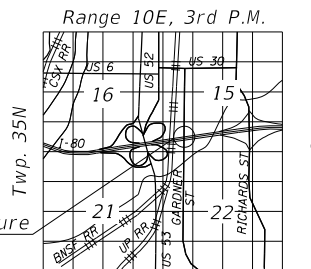
- Limits of Existing Permanent Protective Shield
- WB Westbound
- SB Southbound
- NB Northbound



Signed *Moussa A. Issa*

Date 01/31/2020 For Sheets S10-1 Thru S10-2 (Total of 2 Sheets)

Exist. Structure (099-0061)



**LOCATION SKETCH**

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

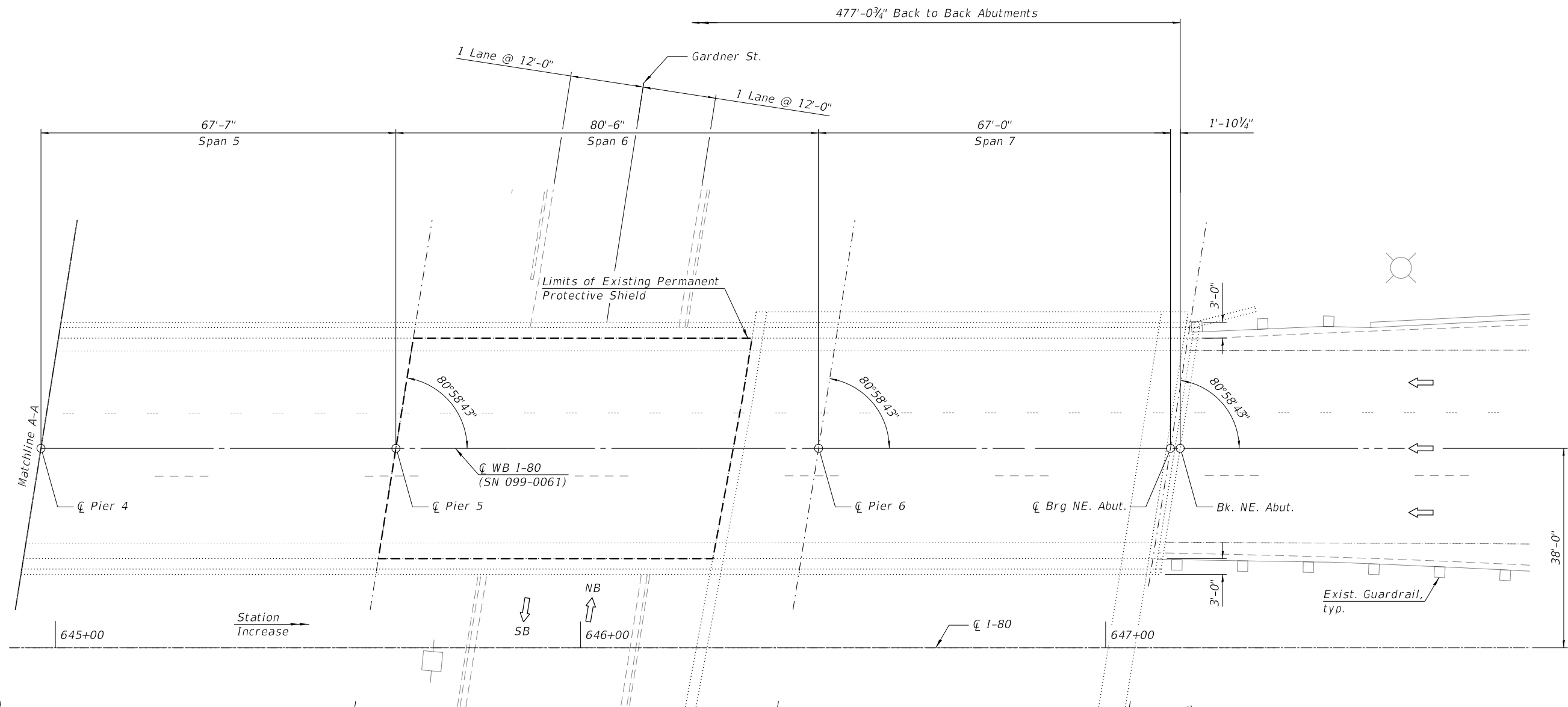
**DECK REPAIRS  
WB I-80 OVER GARDNER ST, BNSF RR & UP RR (S.N. 099-0061)**

SHEET S10-1 OF S10-2 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	76
CONTRACT NO. 62K34				

ILLINOIS FED. AID PROJECT





**NOTE**

1. For Notes, Legend, Scope of work, and Total Bill of Material, see Sheet S10-1.

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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

DECK REPAIRS  
 WB I-80 OVER GARDNER ST, BNSF RR & UP RR (S.N. 099-0061)

SHEET S10-2 OF S10-2 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	77
			CONTRACT NO. 62K34	

ILLINOIS FED. AID PROJECT

**Existing Structure:**  
 S.N. 099-0062 was originally constructed in 1964 as F.A.I. Route 80, Section 99-4B-1. The existing structure is a three span composite steel wide flange beam bridge. The beams support a 7" reinforced concrete slab and a 2" thick waterproof membrane system and polymerized bituminous surface course. The substructure consists of reinforced concrete stub abutments founded on steel piles and multi-column piers founded on spread footings. The structure was rehabilitated in 1990, 1998, 2001, and 2011.

Stage construction shall be utilized to maintain traffic during construction.

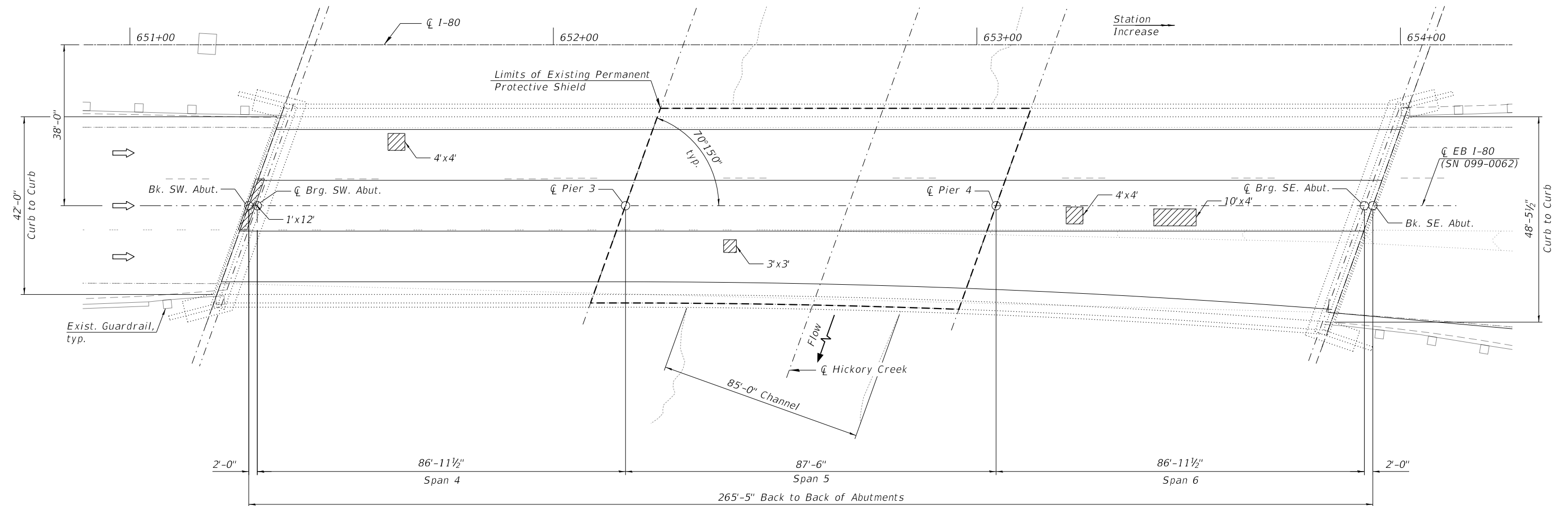
No Salvage.

**SCOPE OF WORK**

1. Perform partial depth deck slab repair.

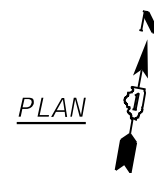
**TOTAL BILL OF MATERIAL**

ITEM	UNIT	QUANTITY
Hot-Mix Asphalt Surface Removal (Deck)	Sq. Yd.	11
Deck Slab Repair (Partial)	Sq. Yd.	11



**NOTES**

1. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
2. Contractor shall not scale dimensions from the contract plans for construction purposes. Scales shown are for information only.
3. Deck slab repair areas are based upon inspection data provided by IDOT D1 Maintenance at the time of plan preparation. Actual areas to be repaired shall be determined by the Engineer in the field at the time of construction and shown on As-built plans.
4. Deck slab repair concrete shall be placed up to top of existing asphalt overlay. Cost included with Deck Slab Repair (Partial). See Patching Detail on Sheet 4.

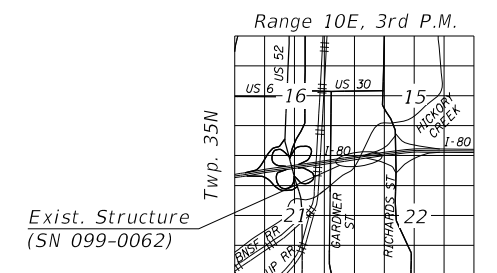


**LEGEND**

- Deck Slab Repair (Partial)
- Limits of Existing Permanent Protective Shield
- EB Eastbound



Signed Moussa A. Issa  
 Date 01/31/2020



**LOCATION SKETCH**

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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DECK REPAIRS  
 EB I-80 OVER HICKORY CREEK (S.N. 099-0062)**

SHEET S11-1 OF S11-1 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	78
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

**Existing Structure:**  
 S.N. 099-0063 was originally constructed in 1964 as F.A.I. Route 80, Section 99-4B-1. The existing structure is a three span composite steel wide flange beam bridge. The beams support a 7" reinforced concrete slab and a 2" thick waterproof membrane system and polymerized bituminous surface course. The substructure consists of reinforced concrete stub abutments founded on steel piles and multi-column piers founded on spread footings. The structure was rehabilitated in 1990, 1998, 2001 and 2011.

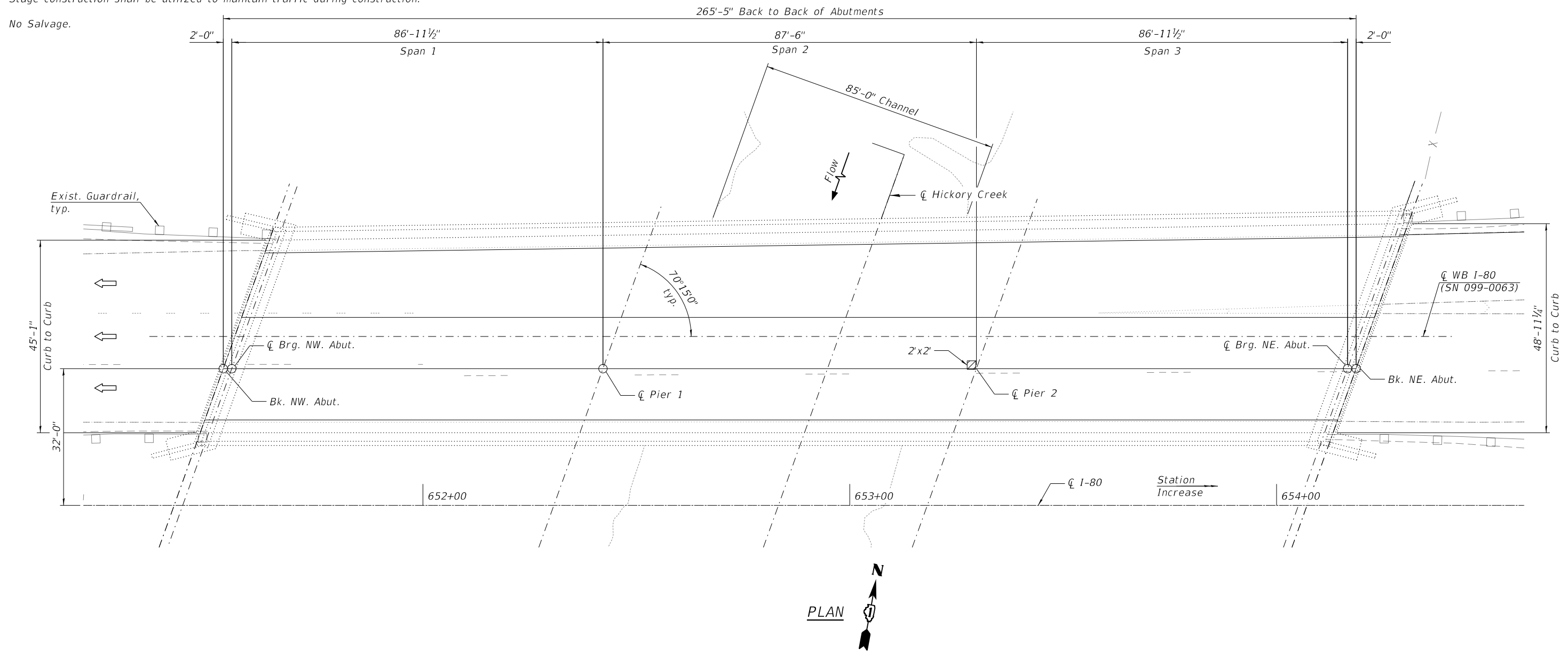
Stage construction shall be utilized to maintain traffic during construction.

**SCOPE OF WORK**

1. Perform partial depth deck slab repair.

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	QUANTITY
Hot-Mix Asphalt Surface Removal (Deck)	Sq. Yd.	1
Deck Slab Repair (Partial)	Sq. Yd.	1



**NOTES**

1. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
2. Contractor shall not scale dimensions from the contract plans for construction purposes. Scales shown are for information only.
3. Deck slab repair areas are based upon inspection data provided by IDOT D1 Maintenance at the time of plan preparation. Actual areas to be repaired shall be determined by the Engineer in the field at the time of construction and shown on As-built plans.
4. Deck slab repair concrete shall be placed up to top of existing asphalt overlay. Cost included with Deck Slab Repair (Partial). See Patching Detail on Sheet 4.

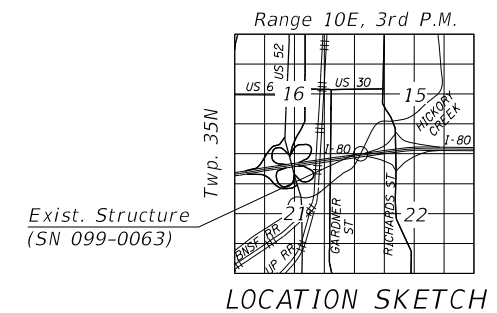
**LEGEND**

- Deck Slab Repair (Partial)
- WB Westbound



Signed Moussa A. Issa

Date 01/31/2020



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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DECK REPAIRS  
 WB I-80 OVER HICKORY CREEK (S.N. 099-0063)**

SHEET S12-1 OF S12-1 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	79
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

**Existing Structure:**  
 S.N. 099-0064 carrying I-80 Eastbound over Richards Street, was originally constructed in 1961 as F.A.I. Route 80, I-80-4(38)134, Section 99-4HB-1. The superstructures consist of 3 simple spans of steel wide flange beam units. The 7-inch thick deck is supported on reinforced concrete piers and abutments with footings that extend a minimum of one foot into solid rock. In 1990 and 1998 repairs were made to the decks, abutments, piers, deck joints, rail and drainage system. In 2001, repairs were made to deck and expansion joints; a new 2" polymerized bituminous concrete overlay with waterproofing membrane was installed. In 2011, repairs were made to the decks, joints, abutments and piers.

Stage construction shall be utilized to maintain traffic during construction.

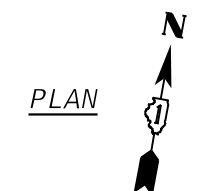
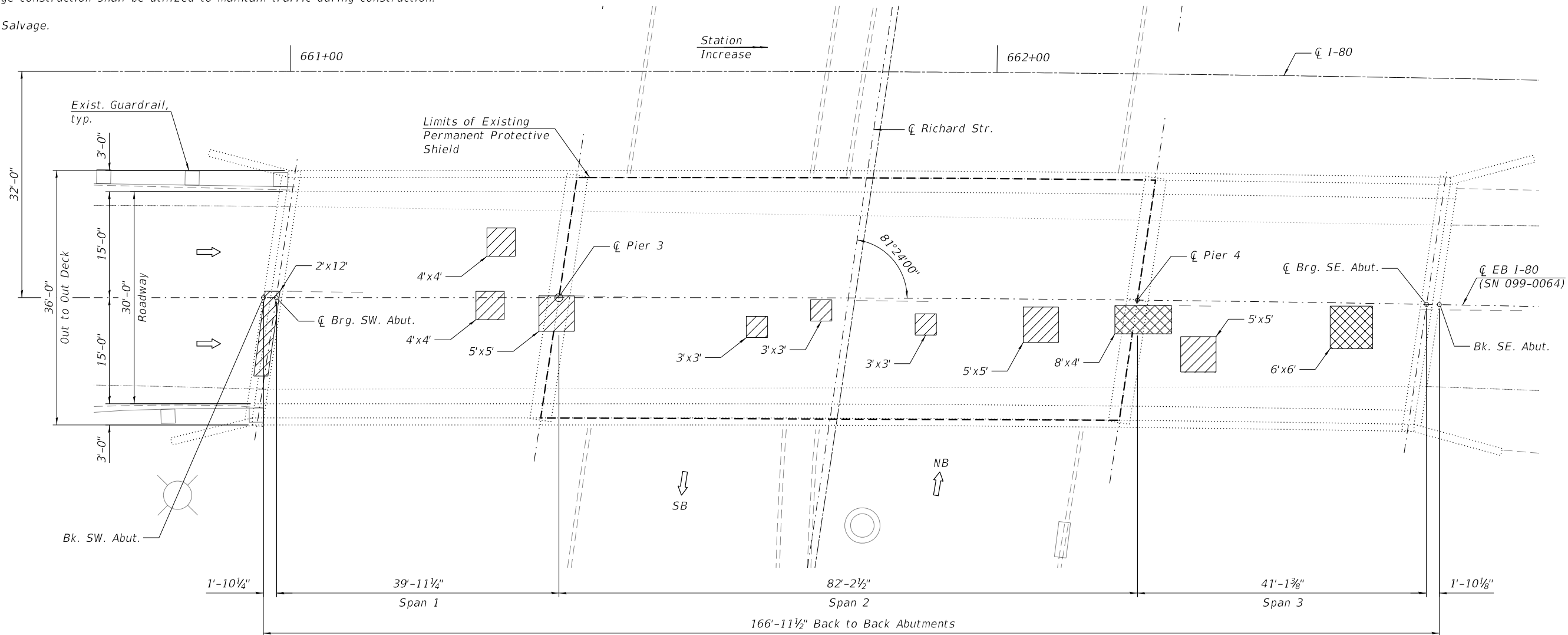
No Salvage.

**SCOPE OF WORK**

1. Perform full depth and partial depth deck slab repair.

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	QUANTITY
Hot-Mix Asphalt Surface Removal (Deck)	Sq. Yd.	26
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	8
Deck Slab Repair (Partial)	Sq. Yd.	18



**NOTES:**

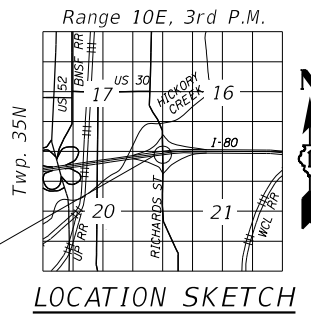
1. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
2. Contractor shall not scale dimensions from the contract plans for construction purposes. Scales shown are for information only.
3. Deck slab repair areas are based upon inspection data provided by IDOT D1 Maintenance at the time of plan preparation. Actual areas to be repaired shall be determined by the Engineer in the field at the time of construction and shown on As-built plans.
4. Deck slab repair concrete shall be placed up to top of existing asphalt overlay. Cost included with Deck Slab Repair, of the type specified. See Patching Detail on Sheet 4.

**LEGEND**

- Deck Slab Repair (Partial)
- Deck Slab Repair (Full Depth, Type II)
- Limits of Existing Permanent Protective Shield
- EB Eastbound
- NB Northbound
- SB Southbound



Signed Moussa A. Issa  
 Date 01/31/2020



MODEL: Default  
 FILE NAME: P:\1710-746 IDOT District 1\WO 26 I-80 DK Patching\Sheet Files\162K34-SHT-STRS13-HBM.dgn  
 2/3/2020 10:29:51 AM



USER NAME =	DESIGNED - KJD, CP	REVISED -
	CHECKED - MI, RTB	REVISED -
PLOT SCALE =	DRAWN - KJD, CP	REVISED -
PLOT DATE = 01/31/2020	CHECKED - MI, RTB	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DECK REPAIRS  
 EB I-80 OVER RICHARDS STREET (S.N. 099-0064)**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	80
CONTRACT NO. 62K34				

SHEET S13-1 OF S13-1 SHEETS

ILLINOIS FED. AID PROJECT

**Existing Structure:**  
 S.N. 099-0065 carrying I-80 Westbound over Richards Street, was originally constructed in 1961 as F.A.I. Route 80, I-80-4(38)134, Section 99-4HB-1. The superstructures consist of 3 simple spans of steel wide flange beam units. The 7-inch thick deck is supported on reinforced concrete piers and abutments with footings that extend a minimum of one foot into solid rock. In 1990 and 1998 repairs were made to the decks, abutments, piers, deck joints, rail and drainage system. In 2001, repairs were made to deck and expansion joints; a new 2" polymerized bituminous concrete overlay with waterproofing membrane was installed. In 2011, repairs were made to the decks, joints, abutments, and piers.

**SCOPE OF WORK**

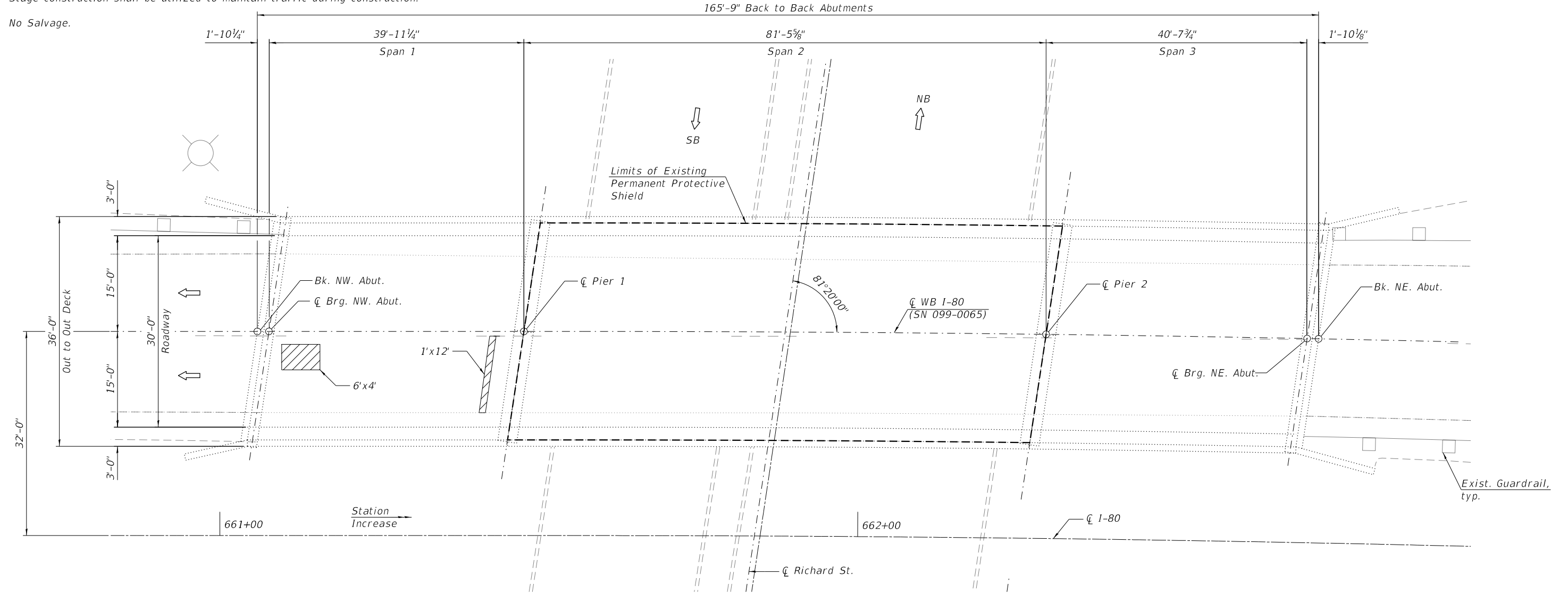
1. Perform partial depth deck slab repair.

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	QUANTITY
Hot-Mix Asphalt Surface Removal (Deck)	Sq. Yd.	4
Deck Slab Repair (Partial)	Sq. Yd.	4

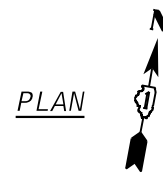
Stage construction shall be utilized to maintain traffic during construction.

No Salvage.



**NOTES:**

1. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
2. Contractor shall not scale dimensions from the contract plans for construction purposes. Scales shown are for information only.
3. Deck slab repair areas are based upon inspection data provided by IDOT D1 Maintenance at the time of plan preparation. Actual areas to be repaired shall be determined by the Engineer in the field at the time of construction and shown on As-built plans.
4. Deck slab repair concrete shall be placed up to top of existing asphalt overlay. Cost included with Deck Slab Repair (Partial). See Patching Detail on Sheet 4.

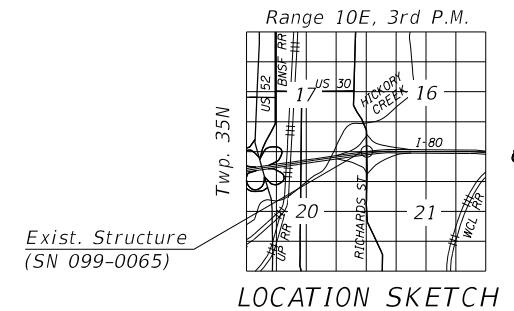


**LEGEND**

- Deck Slab Repair (Partial)
- Limits of Existing Permanent Protective Shield
- EB Eastbound
- NB Northbound
- SB Southbound



Signed Moussa A. Issa  
 Date 01/31/2020



MODEL: Default  
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PLOT DATE = 01/31/2020	CHECKED - MI, RTB	REVISED -

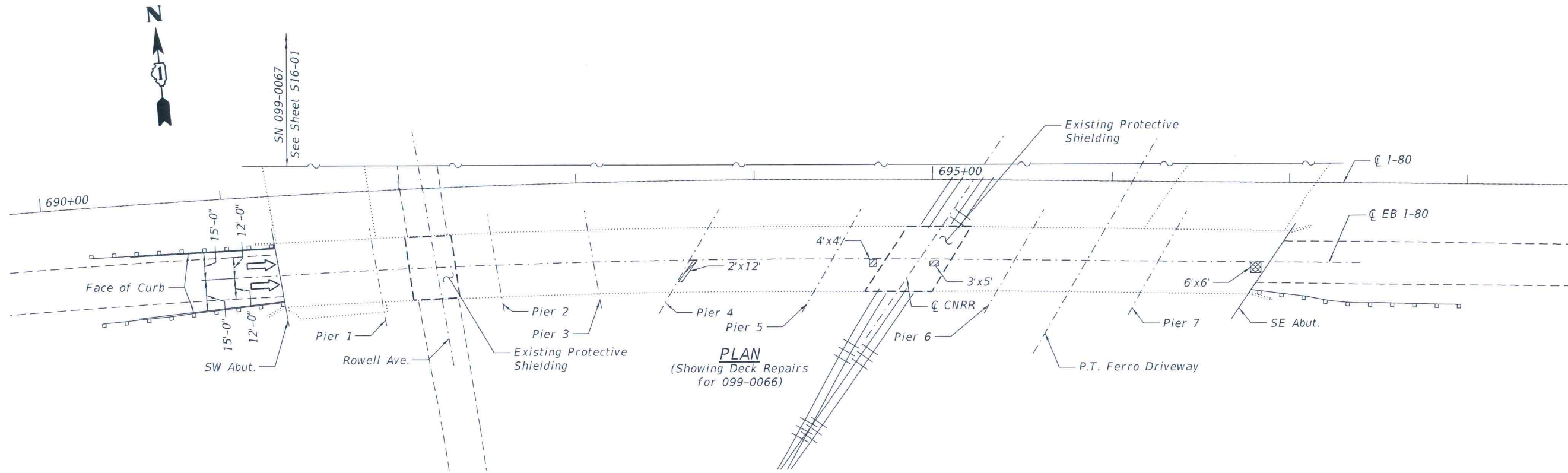
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DECK REPAIRS  
 WB I-80 OVER RICHARDS STREET (S.N. 099-0065)**

SHEET S14-1 OF S14-1 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	81
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

Existing Structure:  
 Dual bridges over CNRR and Rowell Ave. SN 099-0066 carrying I-80 Eastbound and SN 099-0067 carrying I-80 Westbound, were originally constructed in 1962 as part of F.A.I. 80 Project, I-1G-80-4(41)135, Section 99-4-IVB. The EB and WB superstructure consists of 8 and 9 continuous span steel multi-girder units supported on concrete abutments and piers respectively. The existing bridge decks consist of 6¼" reinforced concrete composite slab with 2¾" latex concrete overlay.



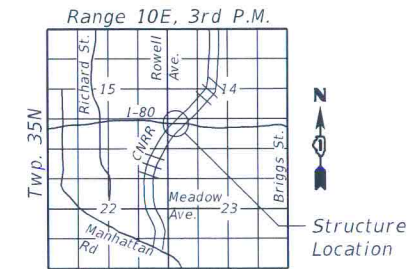
**PLAN**  
 (Showing Deck Repairs for 099-0066)

**NOTES:**

1. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
2. Contractor shall not scale dimensions from the contract plans for construction purposes. Scales shown are for information only.
3. Deck slab repair areas are based upon inspection data provided by IDOT D1 Maintenance at the time of plan preparation. Actual areas to be repaired shall be determined by the Engineer in the field at the time of construction and shown on As-built plans.



DATE SIGNED: 02/23/2020  
 EXP. DATE: 11/30/2020



**LOCATION SKETCH**

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	QUANTITY
Deck Slab Repair (Partial)	Sq. Yd.	7
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	4

**LEGEND**

- Deck Slab Repair (Partial)
- Deck Slab Repair (Full Depth, Type II)
- Limits of Existing Protective Shielding

MODEL: Default  
 FILE NAME:



USER NAME =	DESIGNED - SAT	REVISED -
PLOT SCALE =	CHECKED - SS	REVISED -
PLOT DATE =	DRAWN - JN	REVISED -
	CHECKED - SAT	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

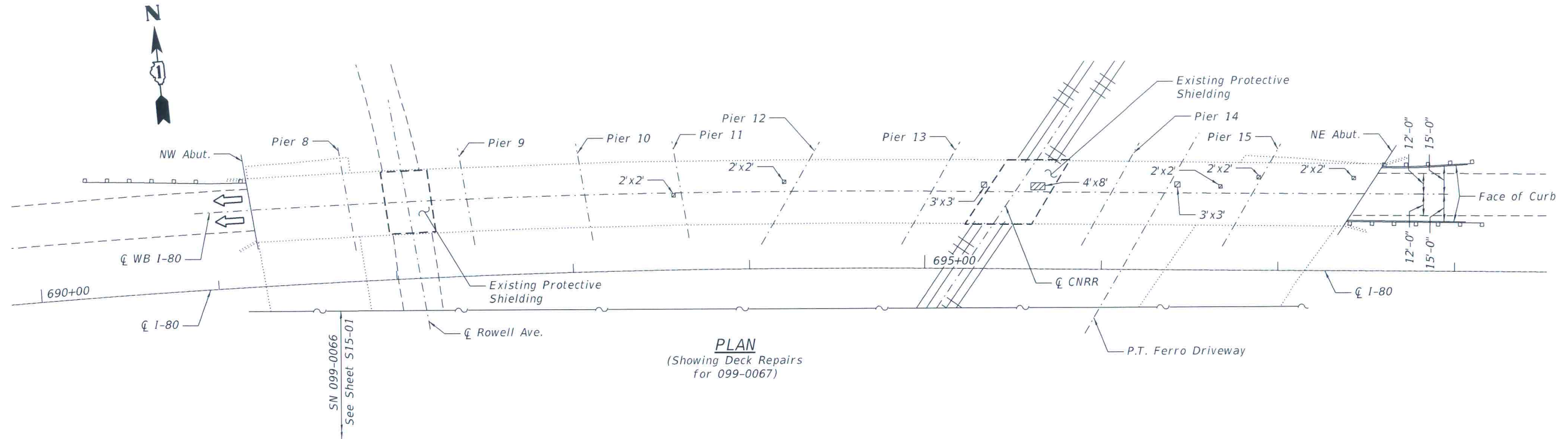
**DECK REPAIRS**  
**EB I-80 OVER ROWELL AVE & RR (SN 099 - 0066)**

F.A.I. RTE. 80	SECTION 2019-162-BR	COUNTY WILL	TOTAL SHEETS 93	SHEET NO. 82
CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

SHEET S15-01 OF S16-01 SHEETS



Existing Structure:  
 Dual bridges over CNRR and Rowell Ave. SN 099-0066 carrying I-80 Eastbound and SN 099-0067 carrying I-80 Westbound, were originally constructed in 1962 as part of F.A.I. 80 Project, I-G-80-4(41)135, Section 99-4-IVB. The EB and WB superstructure consists of 8 and 9 continuous span steel multi-girder units supported on concrete abutments and piers respectively. The existing bridge decks consist of 6 1/4" reinforced concrete composite slab with 2 3/4" latex concrete overlay.



**NOTES:**

1. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
2. Contractor shall not scale dimensions from the contract plans for construction purposes. Scales shown are for information only.
3. Deck slab repair areas are based upon inspection data provided by IDOT D1 Maintenance at the time of plan preparation. Actual areas to be repaired shall be determined by the Engineer in the field at the time of construction and shown on As-built plans.

**TOTAL BILL OF MATERIAL**

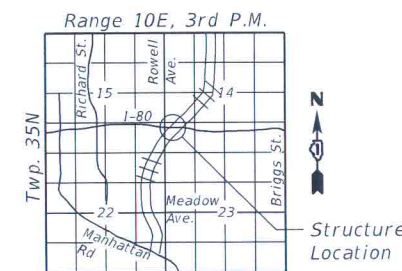
ITEM	UNIT	QUANTITY
Deck Slab Repair (Partial)	Sq. Yd.	8

**LEGEND**

- Deck Slab Repair (Partial)
- Limits of Existing Protective Shielding



DATE SIGNED: 02/03/2020  
 EXP. DATE: 11/30/2020

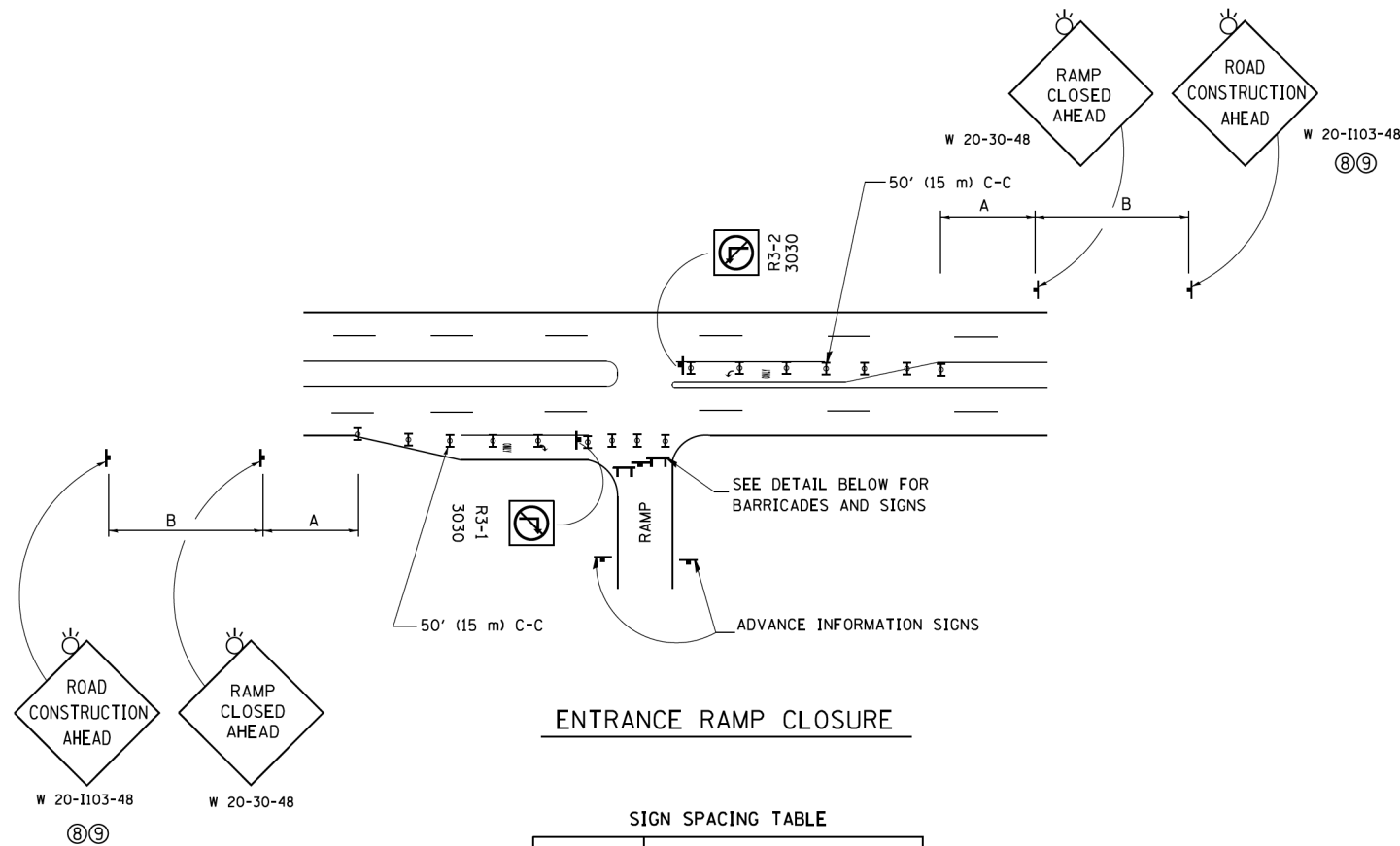


STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

DECK REPAIRS  
 WB I-80 OVER ROWELL AVE & RR (SN 099 - 0067)

SHEET S16-01 OF S16-01 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62K34				
ILLINOIS FED. AID PROJECT				

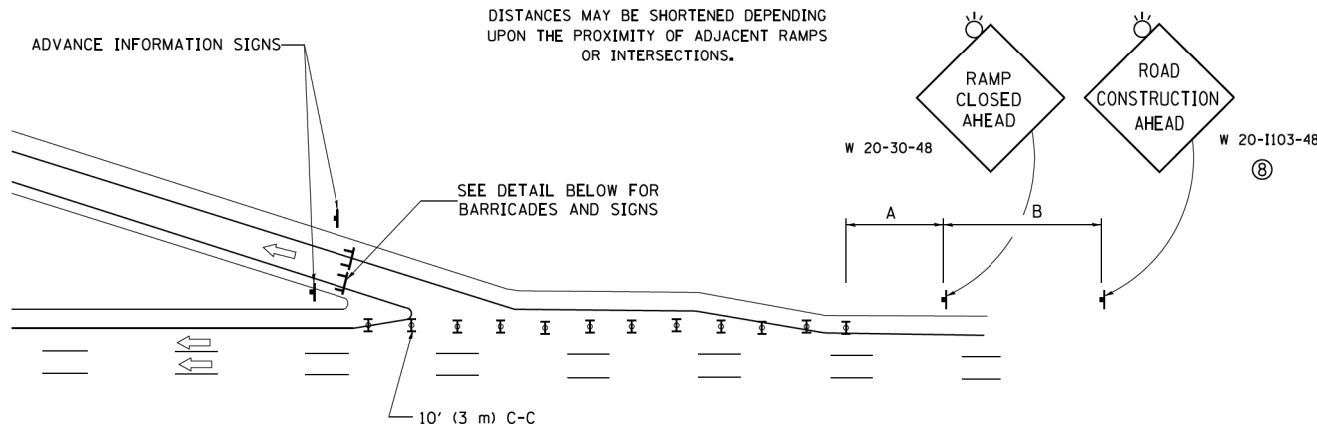


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY ≤24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

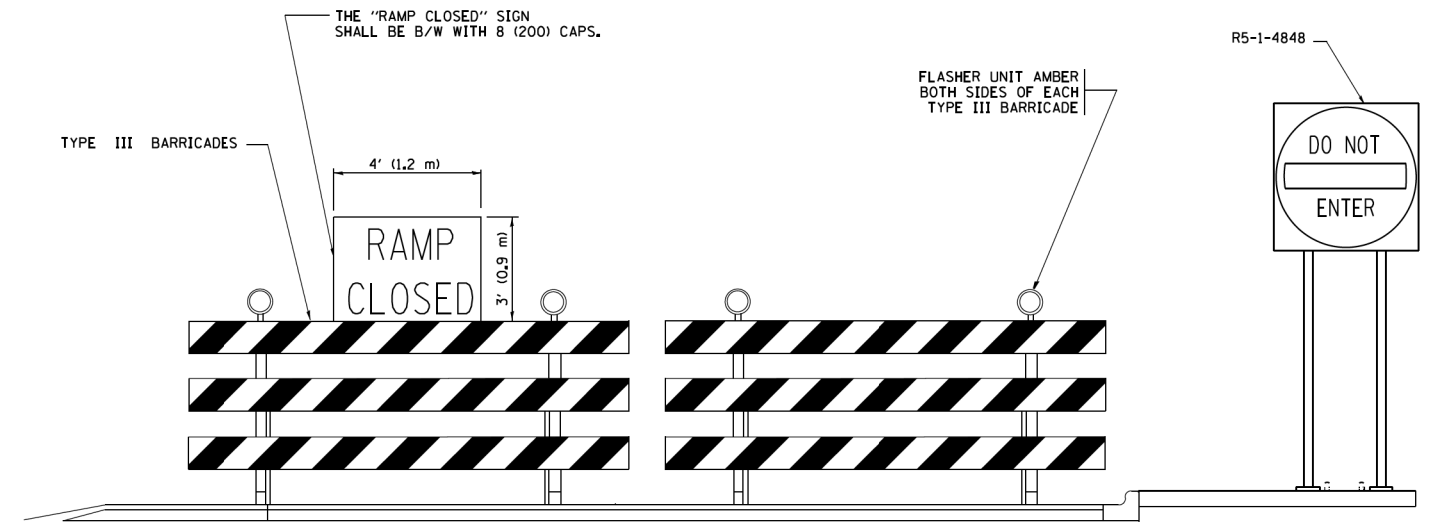
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



EXIT RAMP CLOSURE

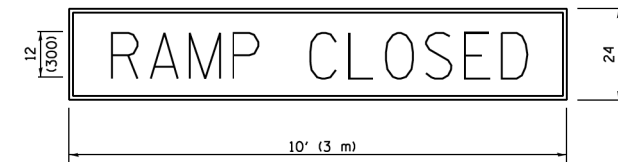
**SYMBOLS**

- ⌚ TYPE II BARRICADE OR DRUM
- ▬ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



DETAIL FOR REQUIRED BARRICADES & SIGNS

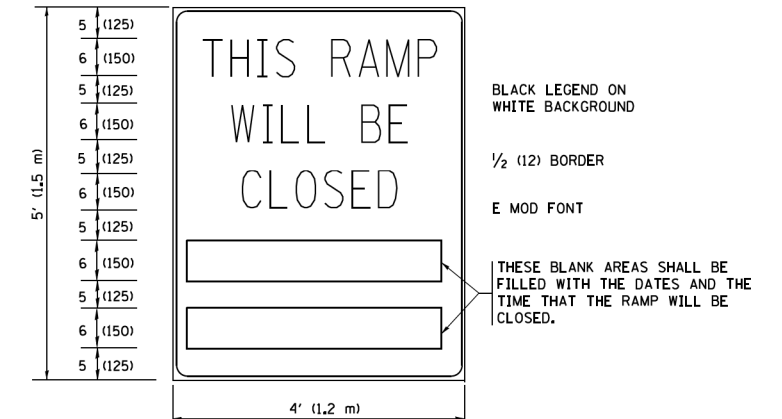
RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY  
E MOD FONT  
1 (25) BORDER

THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

RAMP CLOSURE ADVANCE INFORMATION SIGN



BLACK LEGEND ON WHITE BACKGROUND

1/2 (12) BORDER

E MOD FONT

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

**GENERAL NOTES:**

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② VERTICAL BARRICADES SHALL NOT BE USED FOR RAMP CLOSURES.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = footemj	DESIGNED - D.W.S.	REVISED - S.P.B. 01-07
pw\1\084EBID\INTEG\illinois.gov\FWIDOT\Documents\DOT Offices\District 1\Projects\Dist		DRAWN - CADData\CADsheets\to08.dgn	REVISED - S.P.B. 12-09
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

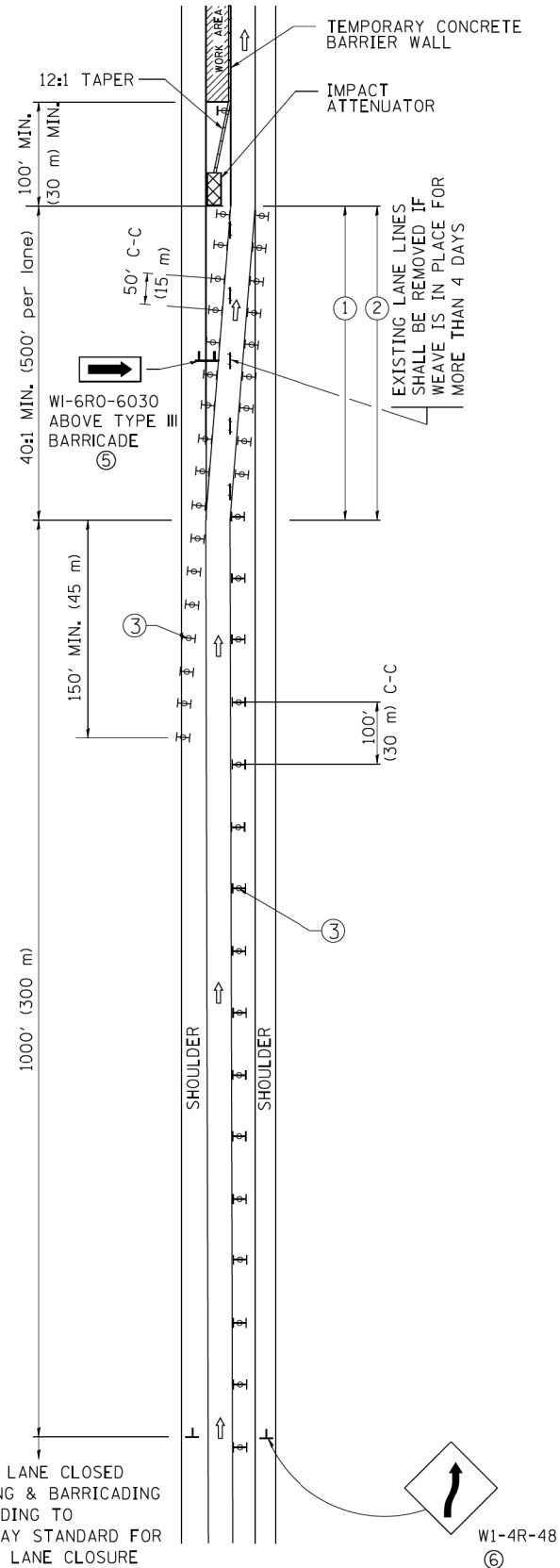
ENTRANCE AND EXIT RAMP  
CLOSURE DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

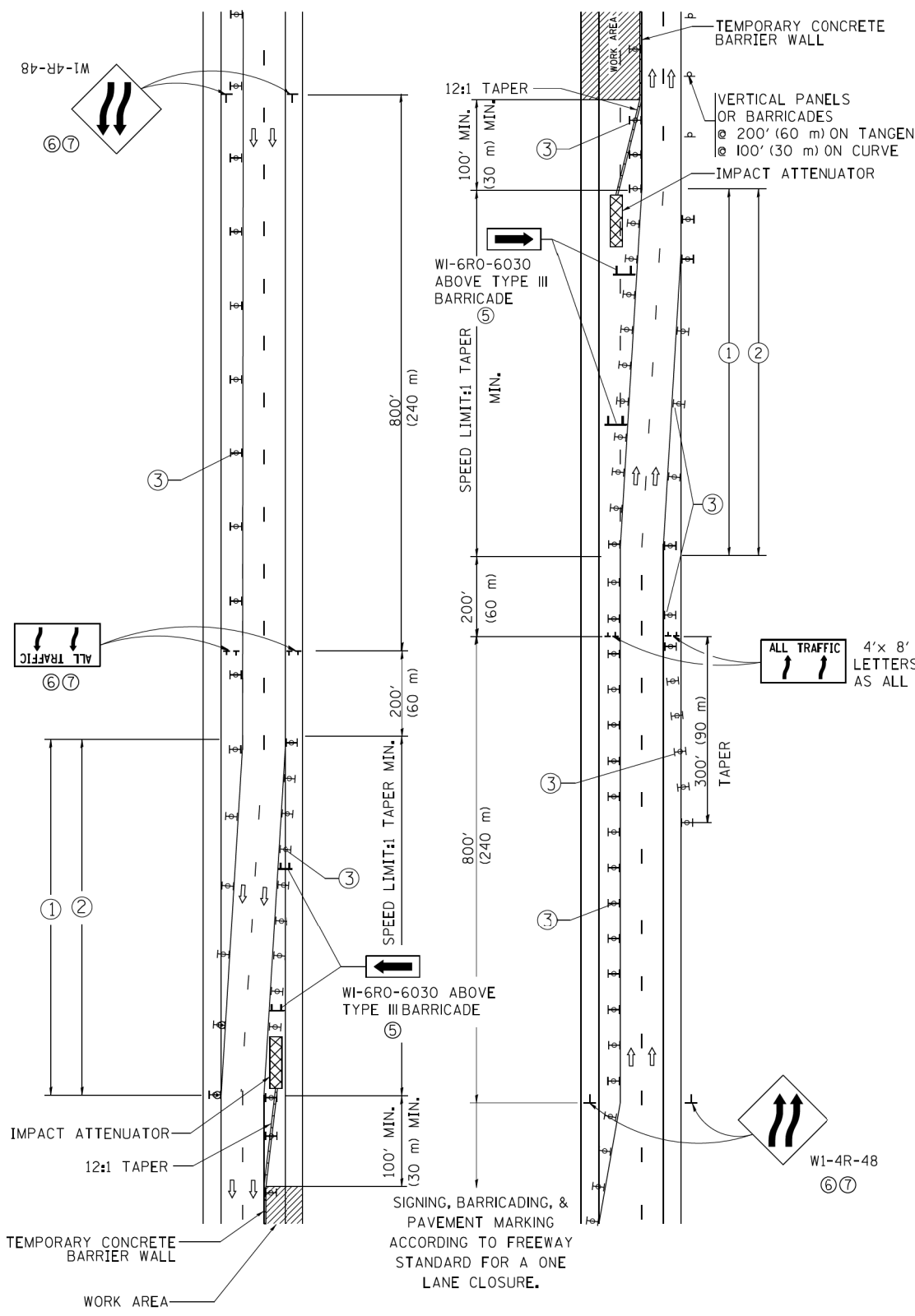
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	84
TC-08		CONTRACT NO. 62K34		
ILLINOIS FED. AID PROJECT				



# SINGLE LANE WEAVE



# MULTI-LANE WEAVE



### GENERAL NOTES

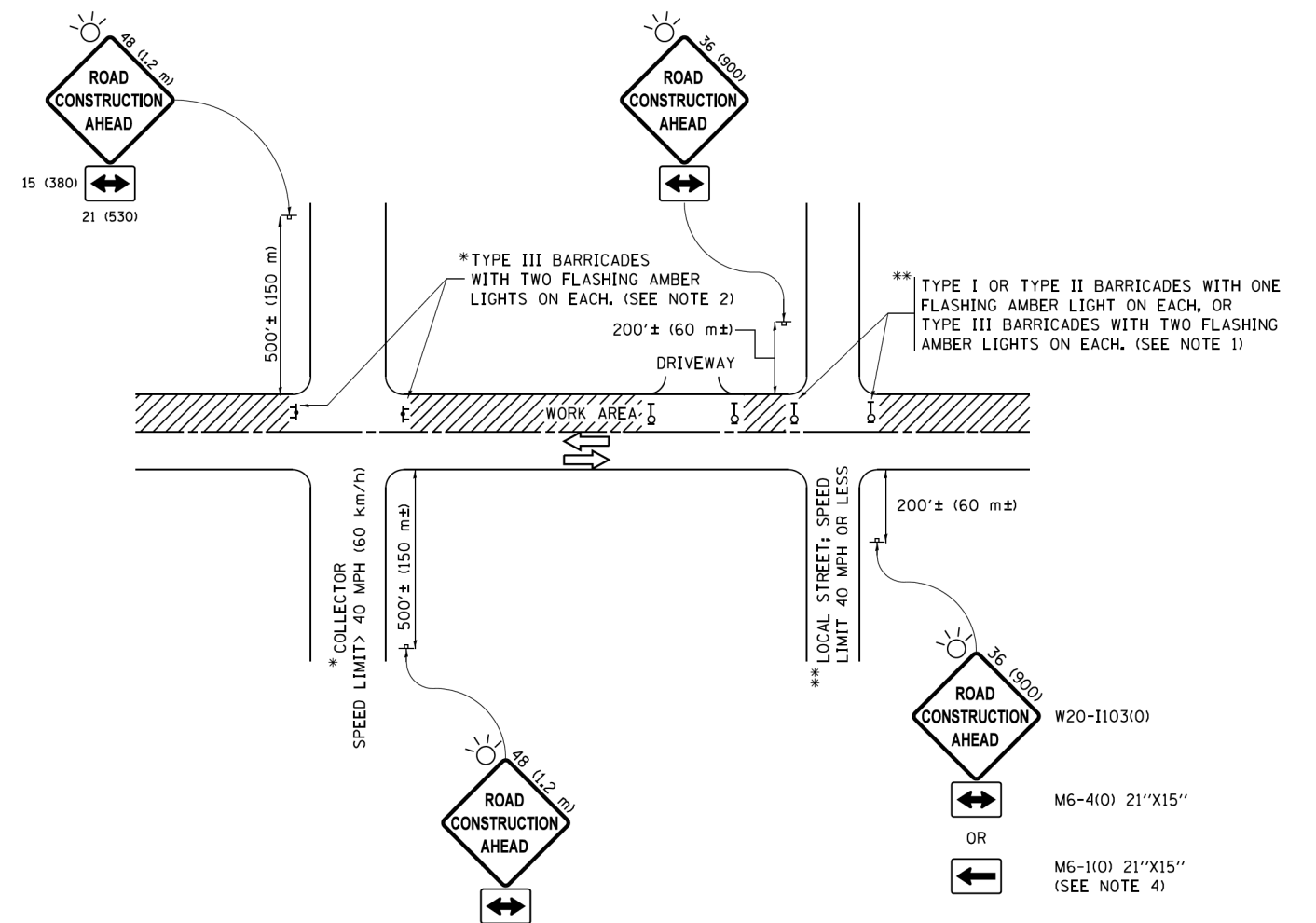
- ① EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 4 DAYS IN DURATION.
- ② CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- ③ PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ④ ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ⑤ TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- ⑥ WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- ⑦ THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

### SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- TEMPORARY CONCRETE BARRIER WALL
- IMPACT ATTENUATOR
- W1-4R-48
- W24-1-48

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = footemj	DESIGNED - DWS	REVISED - JAF 02-06	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE &amp; MULTI-LANE WEAVE</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\p\work\p\dot\footemj\d0108315\td09\fign	DRAWN -	REVISED - SPB 01-07	80					2019-162-BR	WILL	93	85	
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PLOT DATE = 7/1/2013	DATE - 02-87	REVISED - MD 06-13	SCALE: NONE		SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

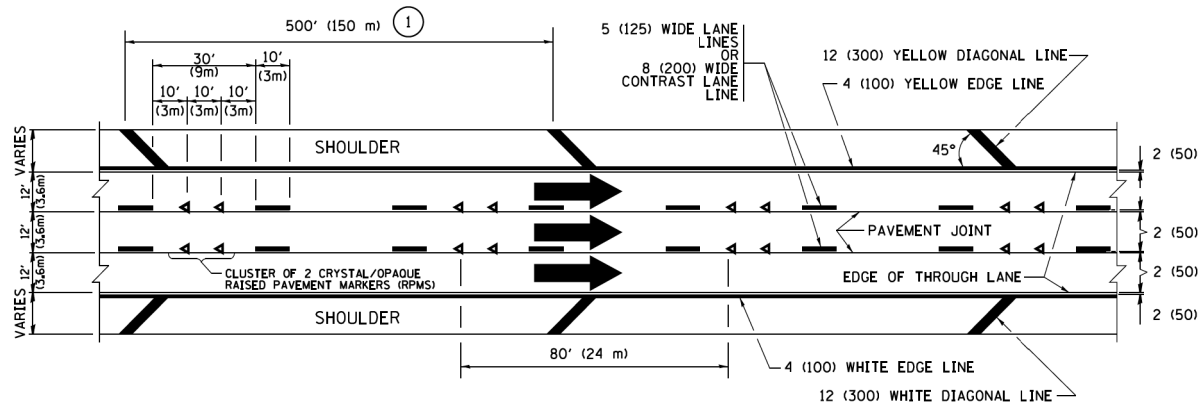
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	PLOT DATE = 9/15/2016	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

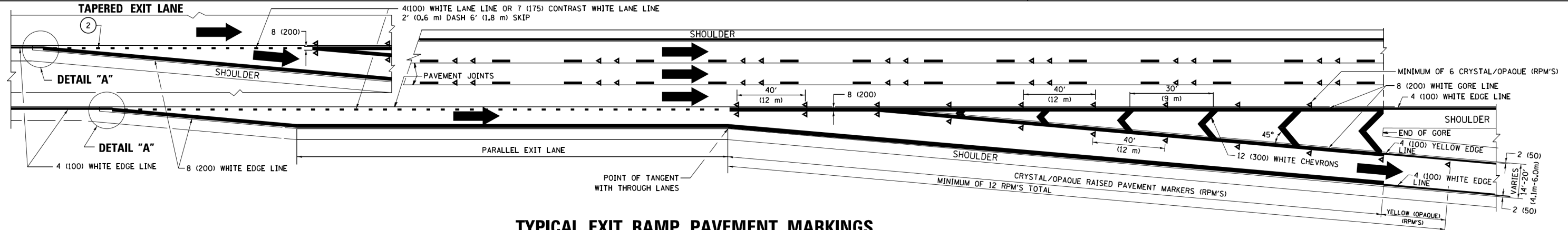
F.A.1 RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	86
<b>TC-10</b>			<b>CONTRACT NO. 62K34</b>	
ILLINOIS FED. AID PROJECT				



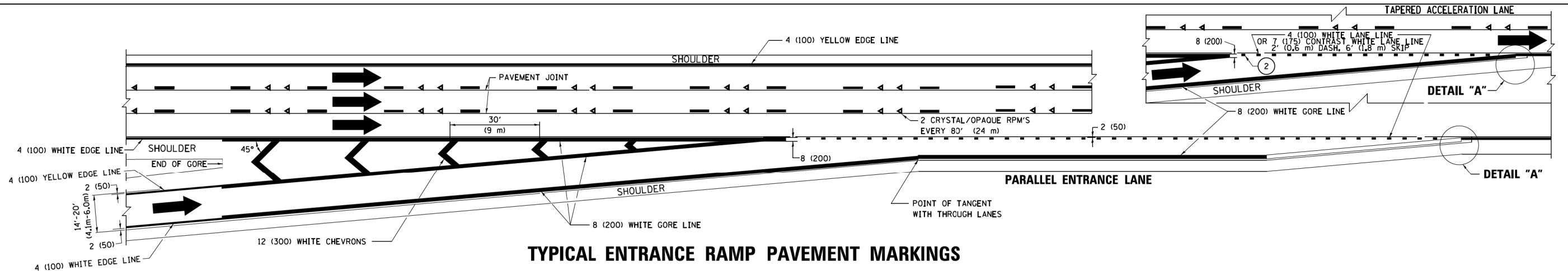
**TYPICAL EDGE LINES & LANE LINES**

**PAVEMENT MARKING MATERIALS**

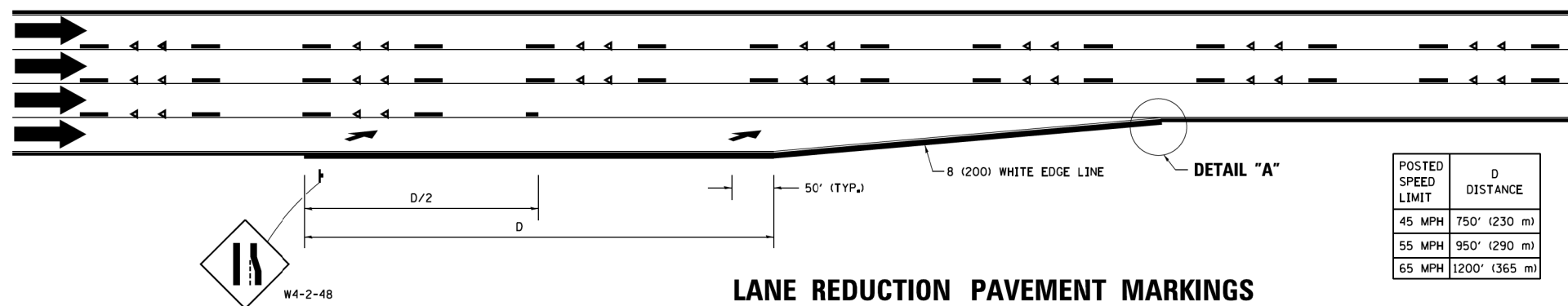
1. THERMOPLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR ALL EDGE LINES, GORE LINES, AND DIAGONAL LINES ON HMA PAVEMENTS.
2. POLYUREA OR MODIFIED URETHANE PAVEMENT MARKING LINE SHALL BE USED FOR ALL EDGE LINES, GORE LINES, AND DIAGONAL LINES ON PCC PAVEMENTS.
3. PREFORMED PLASTIC PAVEMENT MARKING LINE TYPE B, INLAID OR GROOVE IN, SHALL BE USED FOR ALL LANE LINES ON HMA PAVEMENTS.
4. CONTRAST PREFORMED PLASTIC PAVEMENT MARKING LINE TYPE B, GROOVE IN, SHALL BE USED FOR ALL LANE LINES ON PCC PAVEMENT.



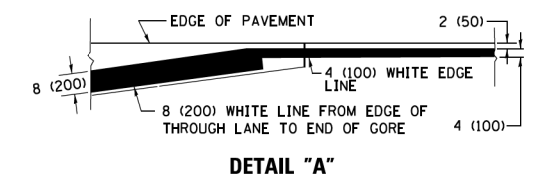
**TYPICAL EXIT RAMP PAVEMENT MARKINGS**



**TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS**



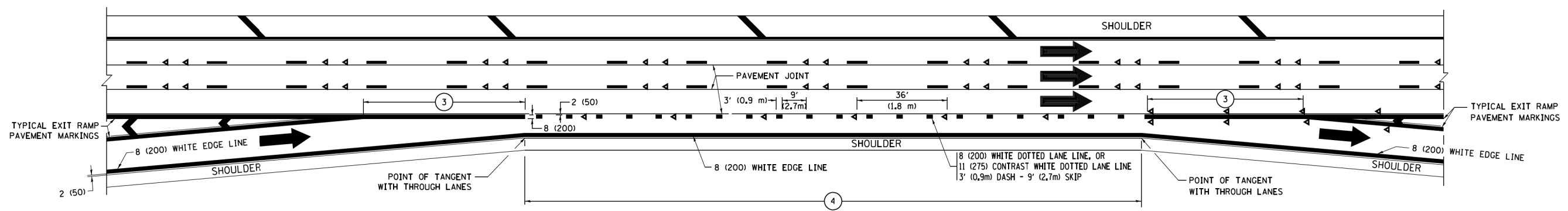
**LANE REDUCTION PAVEMENT MARKINGS**



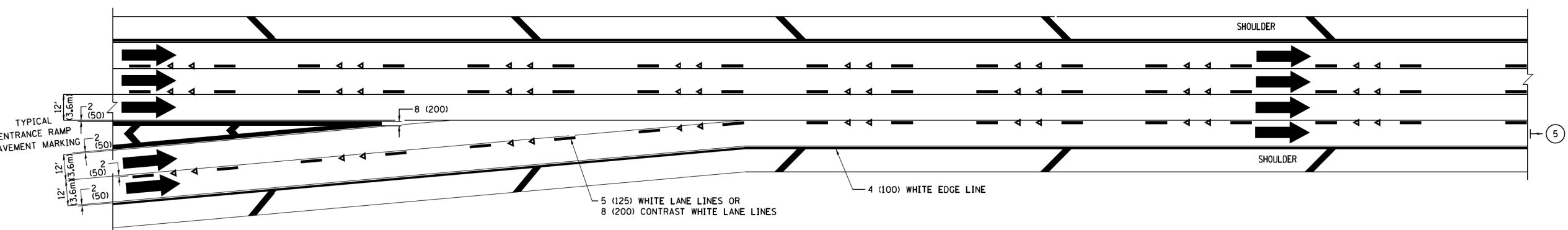
**NOTES:**

- ① THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
- ② 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.

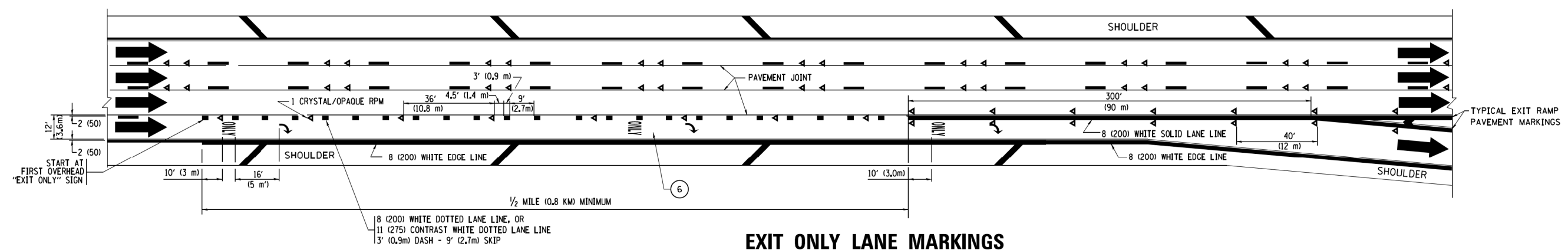
POSTED SPEED LIMIT	D DISTANCE
45 MPH	750' (230 m)
55 MPH	950' (290 m)
65 MPH	1200' (365 m)



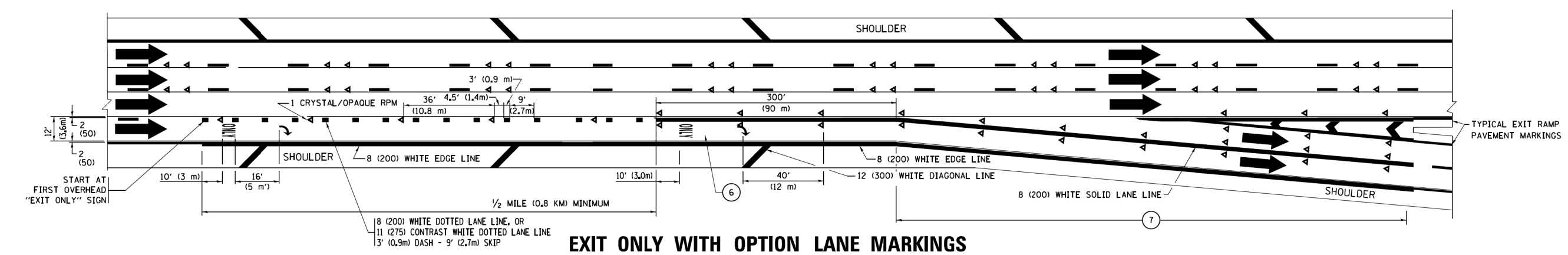
**AUXILIARY LANE MARKINGS**



**TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS**



**EXIT ONLY LANE MARKINGS**

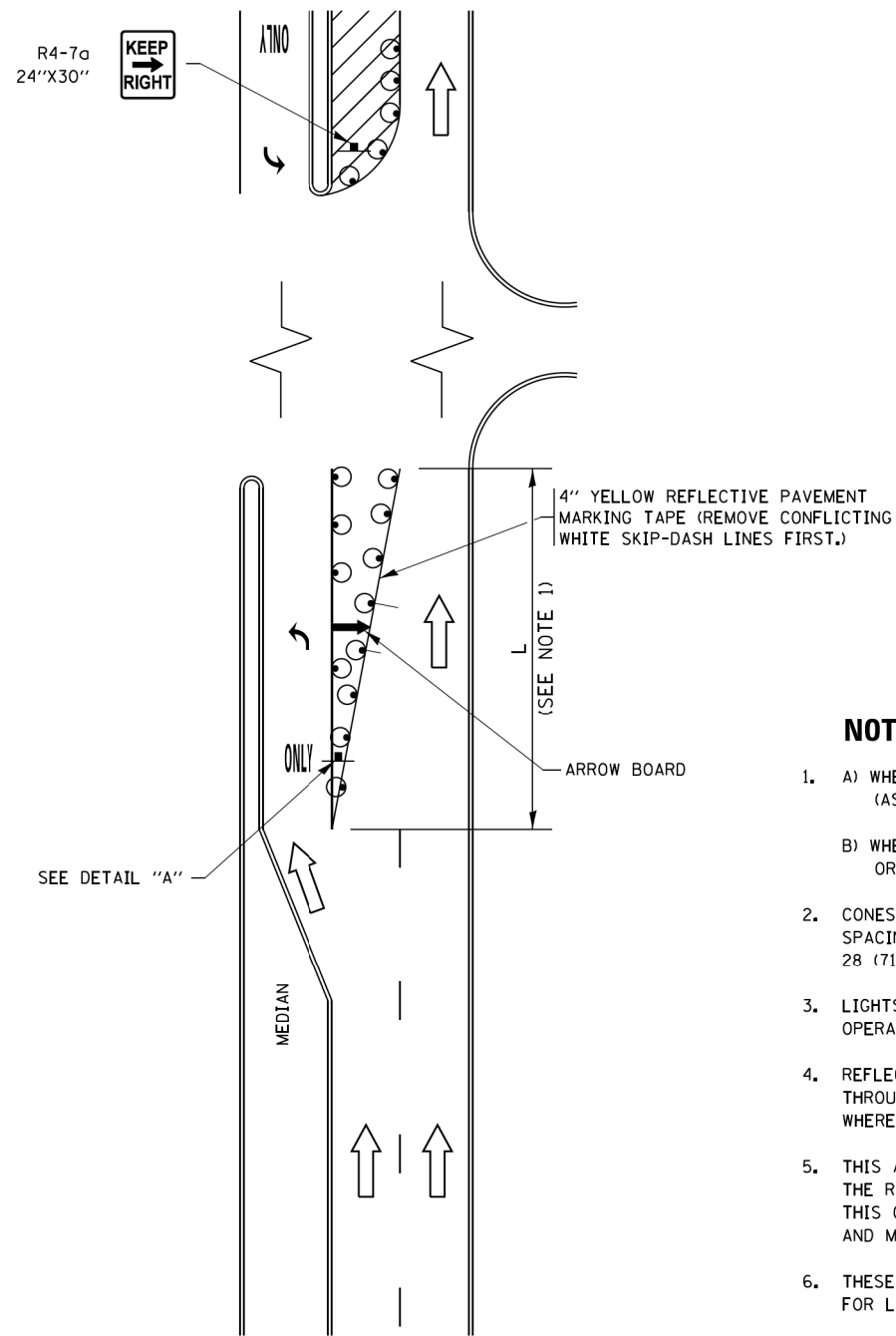


**EXIT ONLY WITH OPTION LANE MARKINGS**

- NOTES:**
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
  - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
  - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
  - ⑥ ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
  - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.

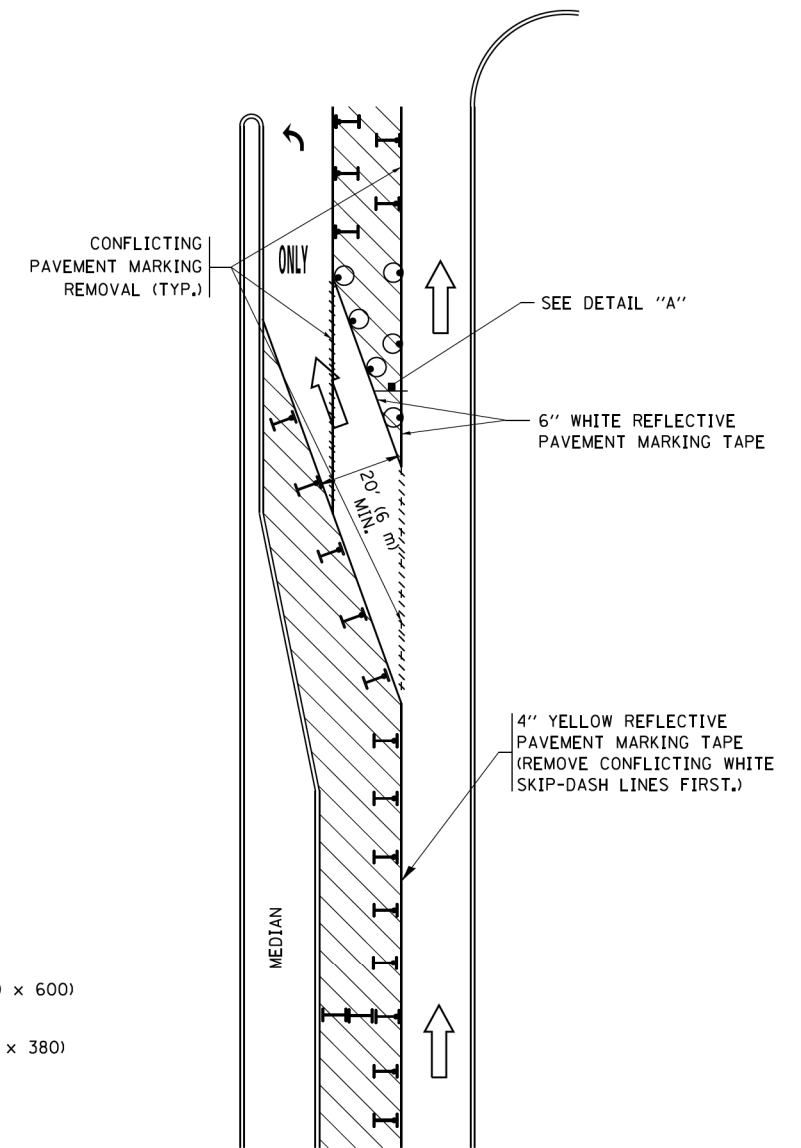
FILE NAME =	USER NAME = footemj	DESIGNED - D.W.S.	REVISED - J.A.F. 02-06	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
pw\1\084EBID\INTEG\illinois.gov\FWIDOT\Documents\DOT Offices\District 1\Projects\Dist 1\084EBID\CADData\CADsheets\tc12.dgn	DRAWN	CHECKED -	REVISED - S.P.B. 01-07		SCALE: NONE	SHEET 2	OF 2 SHEETS	STA.	TO STA.	80	2019-162-BR	WILL	93	88
Default	PLOT SCALE = 50,000' / in.	DATE - 01-90	REVISED - S.P.B. 01-10					<b>TC-12</b>		<b>CONTRACT NO. 62K34</b>				
	PLOT DATE = 9/6/2017		REVISED - M.D. 09-17					<small>ILLINOIS FED. AID PROJECT</small>						

# TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER



**FIGURE 1**

# TURN BAY ENTRANCE WITHIN A LANE CLOSURE



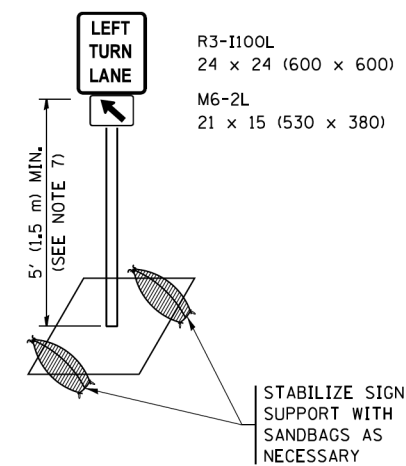
**FIGURE 2**

## LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

## NOTES:

1. A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

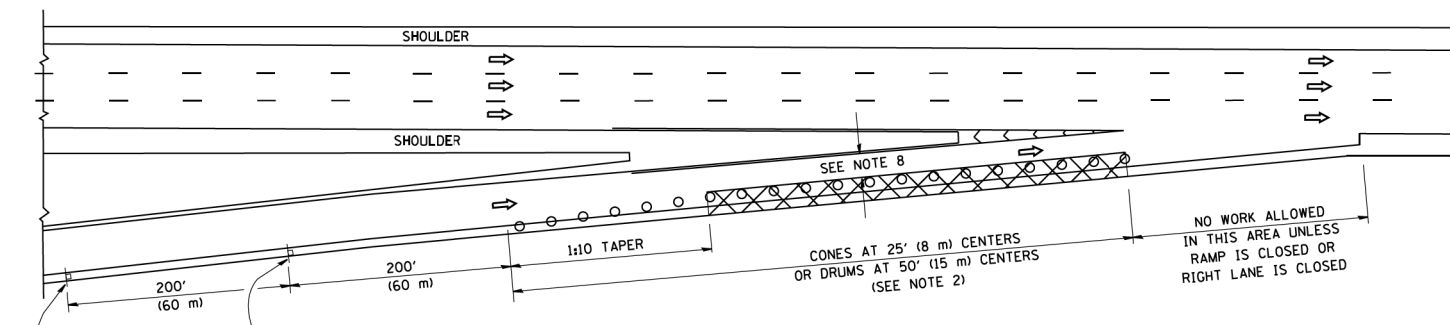


**DETAIL A**

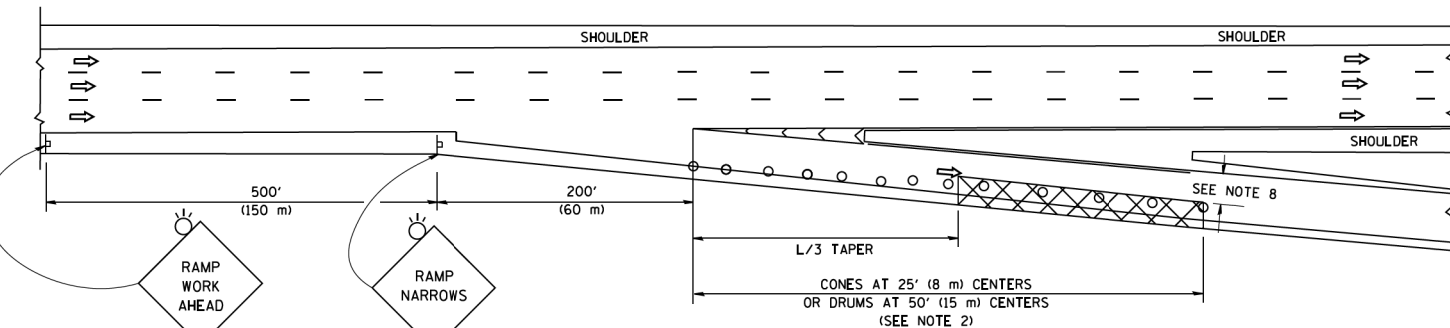
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = footemj	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)</b>				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\1\084EBID\INTEG\illinois.gov\FWIDOT\Documents\DOT Offices\District 1\Projects\Dist 1\ADDData\CAHOUSEH\10-07-95		REVISED - A. HOUSEH 10-07-95	REVISED - A. SCHUETZE 07-01-13		80	2019-162-BR	WILL	93	89				
PLOT SCALE = 50,0000' / in.		REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16		<b>TC-14</b>		<b>CONTRACT NO. 62K34</b>						
Default	PLOT DATE = 9/15/2016	REVISED - T. RAMMACHER 01-06-00	REVISED -		SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT				

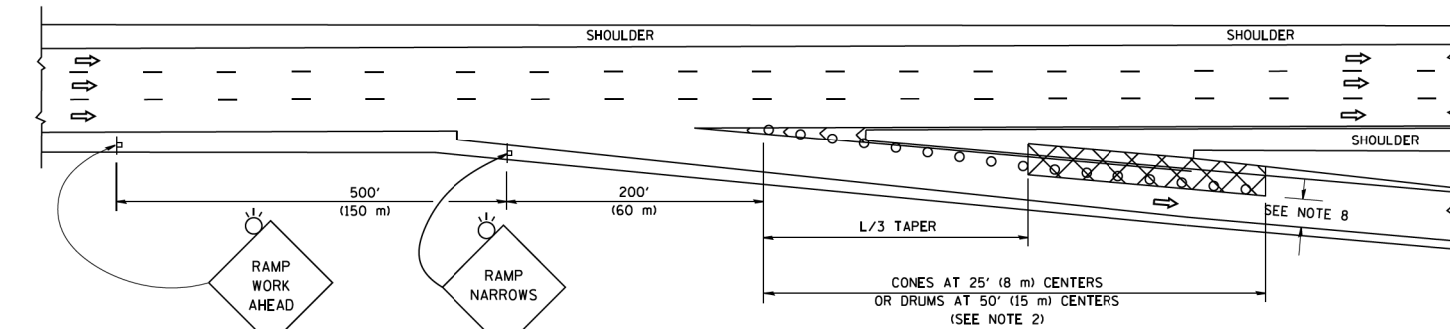
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

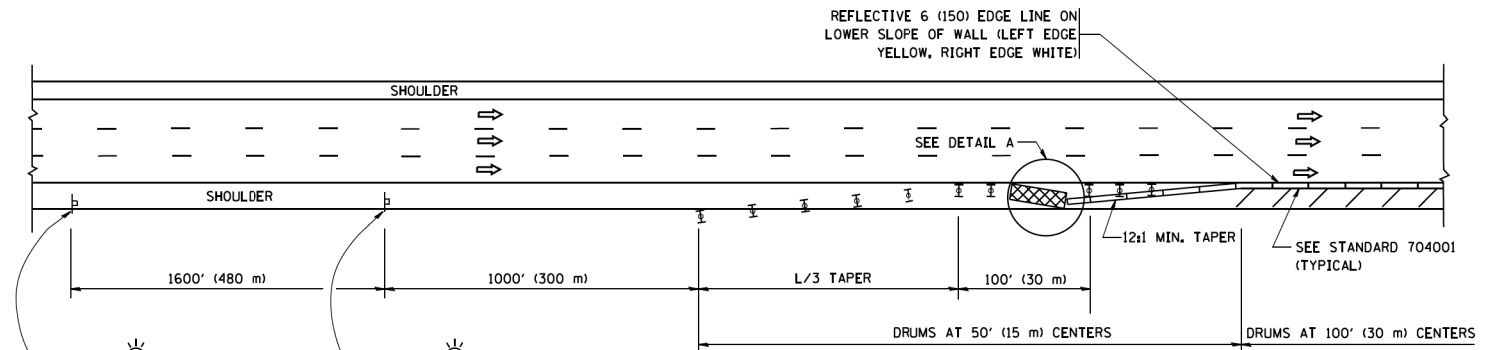
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

GENERAL NOTES

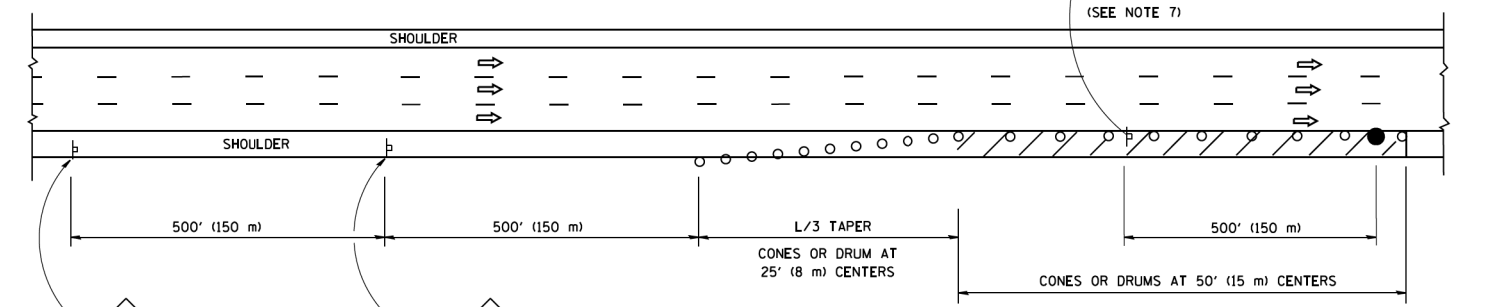
1. THE "L" DISTANCE EQUALS:
 

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER	METRIC ENGLISH L=0.65(W)(S) L=(W)(S)
W = WIDTH OF OFFSET IN FEET (METERS) S = NORMAL POSTED SPEED MPH (KM/H)	
2. TYPE II BARRICADES OR DRUMS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES. TYPE II BARRICADES OR DRUMS WITH MONODIRECTIONAL STEADY BURN LIGHTS ARE REQUIRED FOR DELINEATING OBSTACLES, EXCAVATIONS, OR HAZARDS EXCEEDING 100 FT (30m) IN LENGTH AT NIGHT.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.
5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
  - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
  - b. THE WORK ACTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12' MIN. WIDTH TANGENT SECTION  
16' MIN. WIDTH CURVE SECTION.

SHOULDER CLOSURE DETAILS

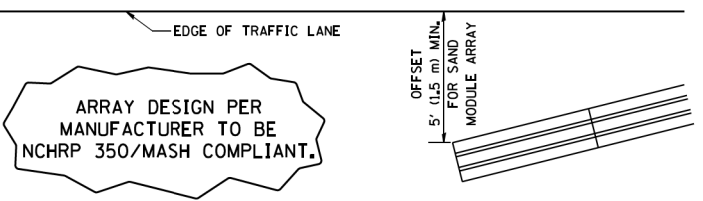


PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:  
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCROACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



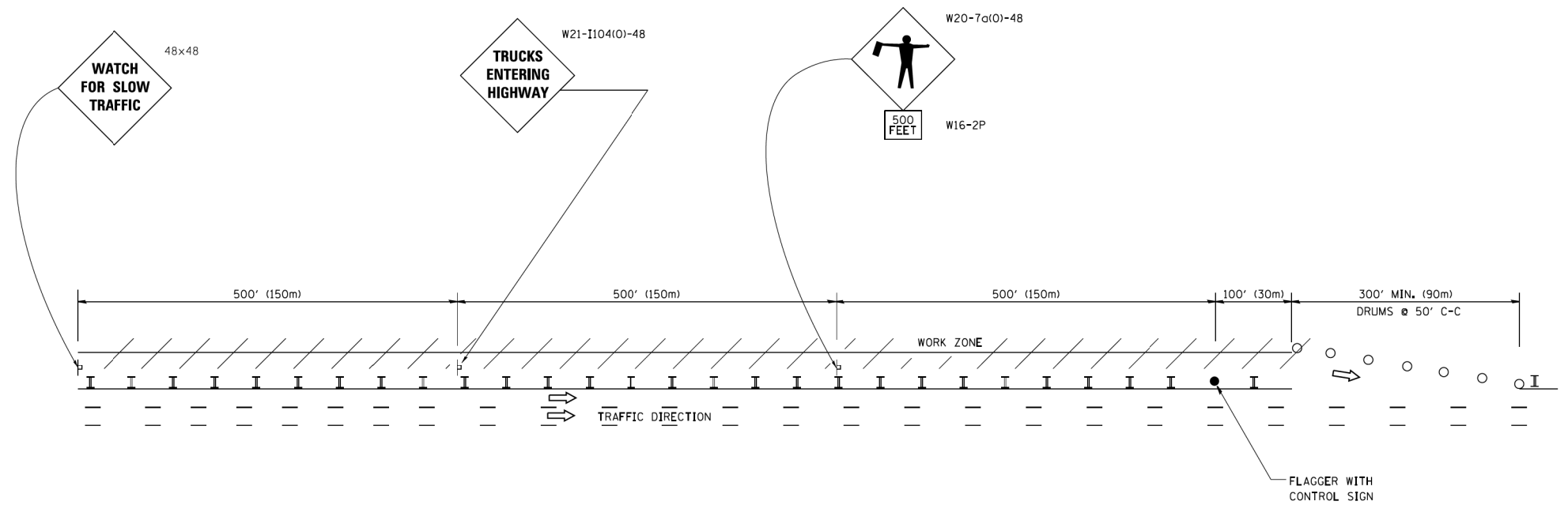
DETAIL "A"  
IMPACT ATTENUATOR, TEMPORARY  
(SEE NOTE 5)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

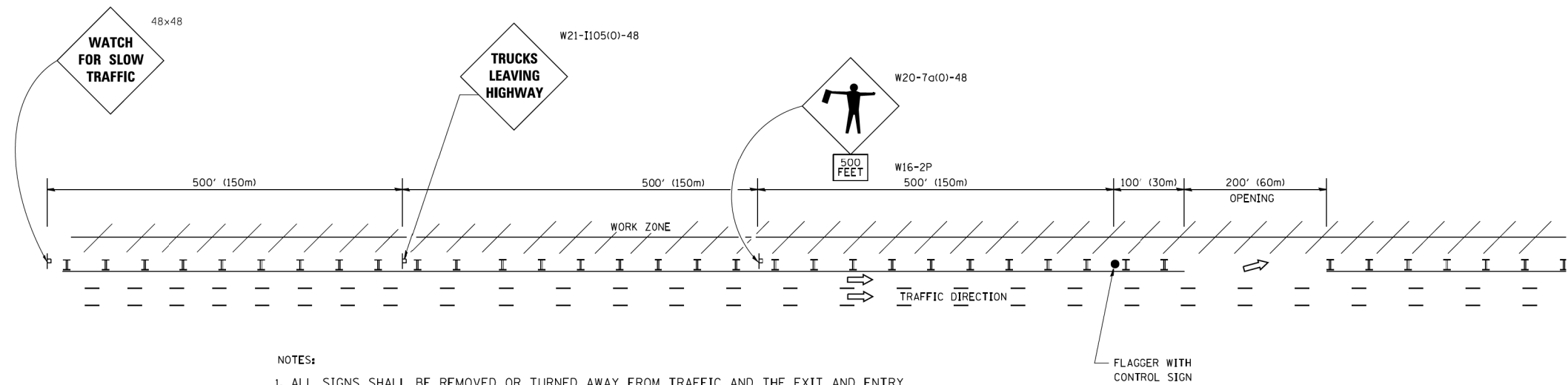
FILE NAME =	USER NAME = footemj	DESIGNED -	REVISED - S.P.B. 01-07	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES</b>	F.A.1 RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED - S.P.B. 12-09			80	2019-162-BR	WILL	93	90	
		CHECKED -	REVISED - M.D. 06-13			<b>TC-17</b>		CONTRACT NO. 62K34			
		DATE - 11-96	REVISED - M.D. 01-18			SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.		ILLINOIS FED. AID PROJECT			

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING




NOTES:


1. ALL SIGNS SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
2. WORK ZONE OPENINGS SHALL BE A MINIMUM OF ONE HALF MILE APART AND A MINIMUM OF ONE QUARTER MILE FROM ALL ENTRANCE AND EXIT RAMP.
3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS
5. FLAGGERS SHALL NOT STOP TRAFFIC OR DIRECT TRAFFIC INTO AN ADJACENT LANE.


ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = footemj	DESIGNED -	REVISED - J.A.F. 02-06	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>FREEWAY/EXPRESSWAY SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS ON FREEWAYS/EXPRESSWAYS</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca:\pwork\pwork\footemj\d0108315\tcl18.dgn		DRAWN -	REVISED - S.P.B. 01-07			80	2019-162-BR	WILL	93	91	
		CHECKED -	REVISED - S.P.B. 12-09			<b>TC-18</b>		CONTRACT NO. 62K34			
		DATE -	REVISED - M.D. 06-13			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	






**ROUTE MARKERS**

 FOR U.S. ROUTES  
M1-40-2424






 FOR ILLINOIS ROUTES  
M1-50-2424

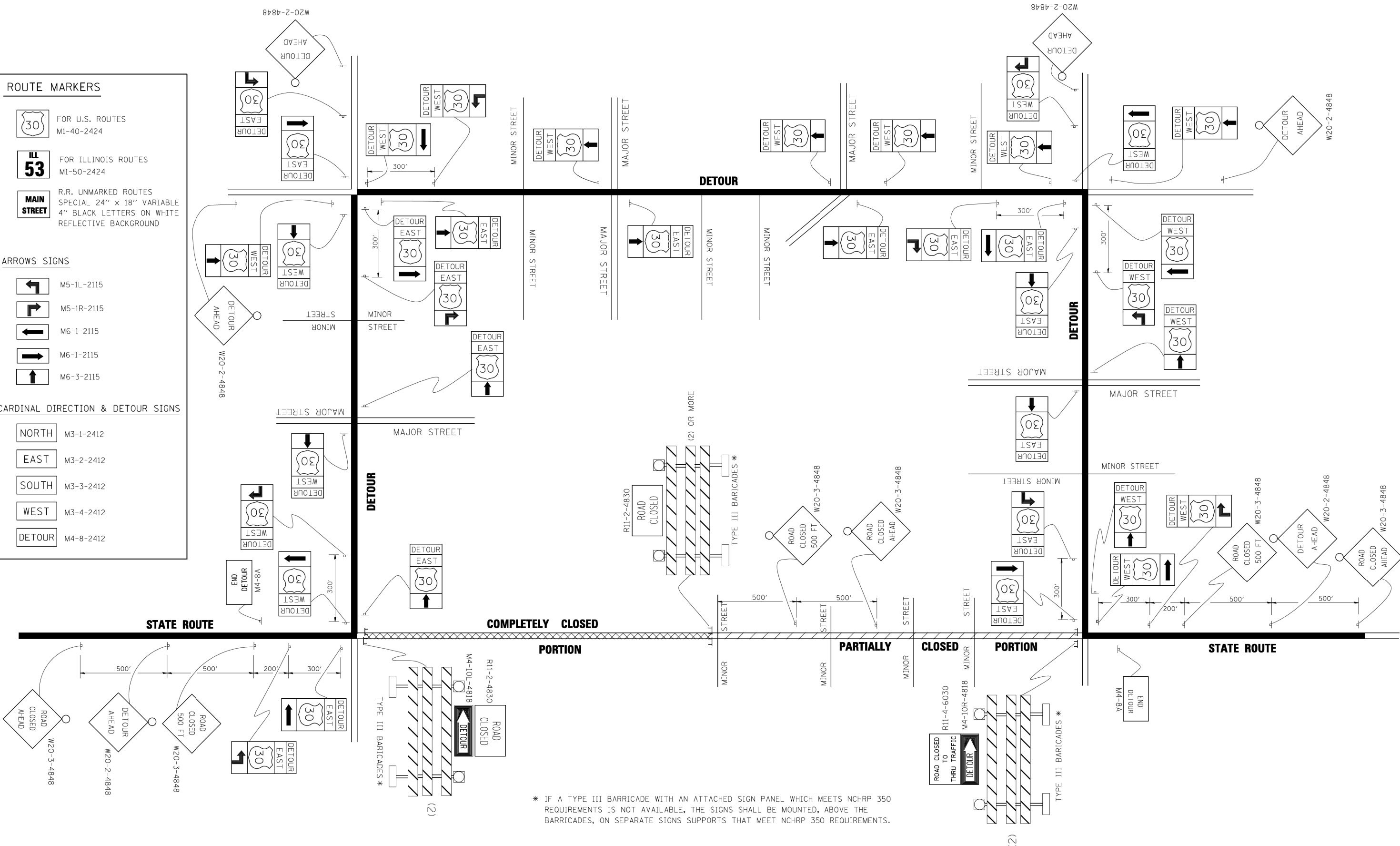
 R.R. UNMARKED ROUTES  
SPECIAL 24" x 18" VARIABLE  
4" BLACK LETTERS ON WHITE  
REFLECTIVE BACKGROUND

**ARROWS SIGNS**

-  M5-1L-2115
-  M5-1R-2115
-  M6-1-2115
-  M6-1-2115
-  M6-3-2115

**CARDINAL DIRECTION & DETOUR SIGNS**

-  NORTH M3-1-2412
-  EAST M3-2-2412
-  SOUTH M3-3-2412
-  WEST M3-4-2412
-  DETOUR M4-8-2412



\* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

FILE NAME =	USER NAME = d-ivakosgn	DESIGNED -	REVISED - 10-18-02
c:\p\work\PWID01\DRIVAKOSGN\0108315\1221.dgn		DRAWN -	REVISED - R. BORO 09-14-09
		PLOT SCALE = 49.9999' / IN.	REVISED -
		CHECKED -	REVISED -
		DATE - 9/14/2009	REVISED -

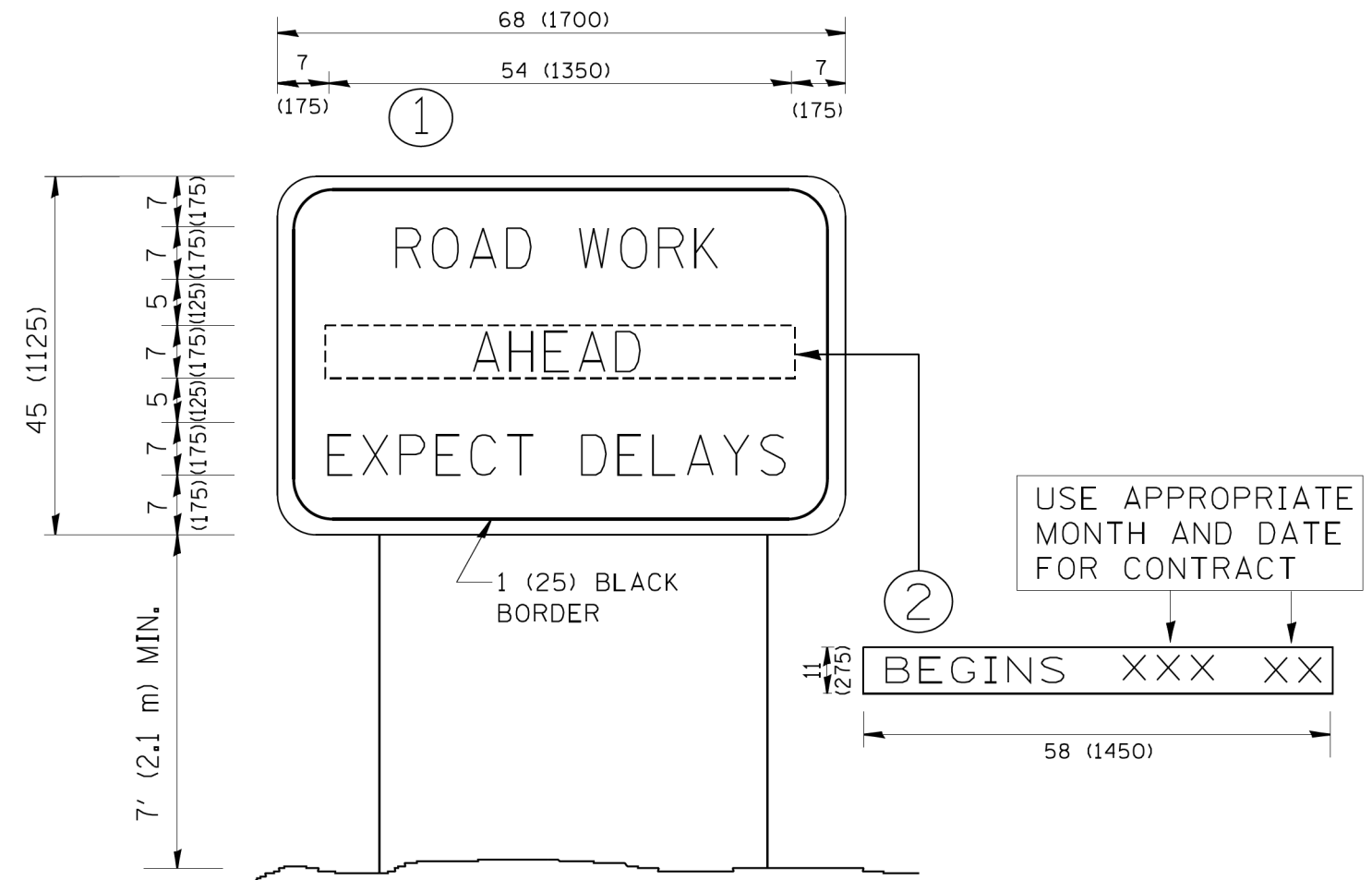
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETOUR SIGNING  
FOR CLOSING STATE HIGHWAYS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
80	2019-162-BR	WILL	93	92
<b>TC-21</b>		CONTRACT NO. 62K34		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\tc22.dgn	USER NAME = gegl1enobt	DESIGNED - DRAWN -	REVISOR - R. MIRS 09-15-97 REVISOR - R. MIRS 12-11-97	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARTERIAL ROAD INFORMATION SIGN</b>		F.A.I. RTE. 80	SECTION 2019-162-BR	COUNTY WILL	TOTAL SHEETS 93	SHEET NO. 93	
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISOR - T. RAMMACHER 02-02-99		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	<b>TC-22</b>		CONTRACT NO. 62K34	
	PLOT DATE = 1/4/2008	DATE -	REVISOR - C. JUCIUS 01-31-07		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							