

CONSTRUCTION PLANS FOR INGERSOLL AIRPORT

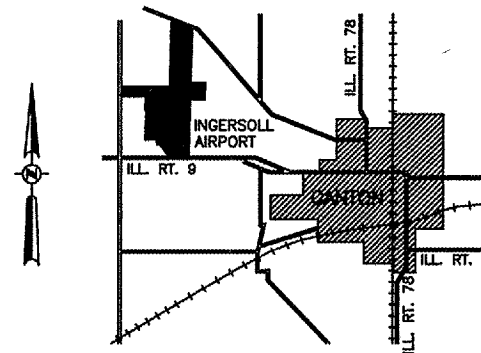
CANTON, FULTON COUNTY, ILLINOIS

APPLY POROUS FRICTION COURSE AND MARK RUNWAY 18-36

ILLINOIS PROJECT NO. CTK-3713
AIP PROJECT NO. 3-17-0008-B9

DECEMBER 7, 2007
REVISED: MARCH 10, 2008

SUMMARY OF QUANTITIES				
ITEM NO.	DESCRIPTION	UNIT	AS BID QUANTITY	AS BUILT QUANTITY
AR150530	TRAFFIC MAINTENANCE	L. SUM	1	
AR152480	SHOULDER ADJUSTMENT	SQ. YD.	864	
AR201660	BITUMINOUS CRACK REPAIR	LIN. FT.	15,050	
AR201666	BASE COURSE CRACK REPAIR	SQ. YD.	540	
AR401110	PAVEMENT PREPARATION	L. SUM	1	
AR401655	BUTT JOINT CONSTRUCTION	SQ. YD.	767	
AR402622	POROUS FRICTION COURSE, 0.10'	SQ. YD.	36,836	
AR603510	BITUMINOUS TACK COAT	GAL.	9,946	
AR620520	PAVEMENT MARKING -- WATERBORNE	SQ. FT.	20,416	
AR620525	PAVEMENT MARKING -- BLACK BORDER	SQ. FT.	1,090	



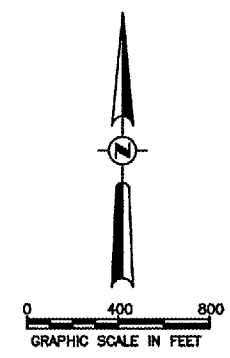
VICINITY MAP

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6	RUNWAY MARKING LAYOUT

	PLANS PREPARED BY :	CANTON PARK DISTRICT APPROVED <u>Walter B. Buckle</u> PRESIDENT DATE <u>11/20/07</u> APPROVED <u>Paul C. Hrus</u> SECRETARY DATE <u>11/20/07</u>
	HUTCHISON ENGINEERING, INC. JACKSONVILLE, ILLINOIS SUBMITTED <u>Dan L. Hutchison</u> DATE <u>11/19/07</u>	

CONSTRUCTION PROCEDURE NOTES

1. ALL CONTRACTOR EMPLOYEES WILL PARK THEIR PERSONAL VEHICLES IN THE AREA DESIGNATED BY THE AIRPORT MANAGER FOR USE AS A VEHICLE PARKING AREA. THE CONTRACTOR WILL BE RESPONSIBLE FOR TRANSPORTING EMPLOYEES TO THE PROPOSED CONSTRUCTION SITE. ONLY AUTHORIZED CONTRACTOR VEHICLES WILL BE ALLOWED ON THE AIR OPERATIONS AREA OF THE AIRPORT AND ON THE CONSTRUCTION SITE.
2. THE CONTRACTOR SHALL FURNISH ALL EMPLOYEES WITH SOME TYPE OF TAG OR GARMENT TO IDENTIFY THEM AS BEING PART OF THE CONSTRUCTION CREW.
3. FLAGS WILL BE REQUIRED ON ALL CONTRACTOR'S VEHICLES AND EQUIPMENT USED ON THE AIRPORT OPERATIONS AREA AND CONSTRUCTION SITE. THE FLAGS SHALL BE THREE (3) FOOT SQUARE CHECKERED FLAGS (INTERNATIONAL ORANGE AND WHITE) DISPLAYED IN FULL VIEW ABOVE THE VEHICLE.
4. THE CONTRACTOR WILL USE THE DESIGNATED HAUL ROUTE AND EQUIPMENT STORAGE AREA SHOWN ON THIS SHEET. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE DESIGNATED HAUL ROUTE AND EQUIPMENT STORAGE AREA FOR THE DURATION OF THE PROJECT. ANY DAMAGE TO EXISTING PAVEMENTS USED TO HAUL MATERIAL TO THE CONSTRUCTION SITE WILL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE TO THE SATISFACTION OF THE AIRPORT MANAGER AND THE RESIDENT ENGINEER.
5. THE CONTRACTOR SHALL FURNISH, MAINTAIN, AND REMOVE ALL BARRICADES REQUIRED TO DELINEATE THE WORK AREA AND KEEP AIRCRAFT FROM ENCRUCHING INTO SAID WORK AREA. THIS REQUIREMENT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
6. CONSTRUCTION ACTIVITY CONTROL AND OPERATION AREA PROTECTION SHALL BE ACCOMPLISHED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR NO. 150/5370-2E.
7. THE MAXIMUM CONSTRUCTION EQUIPMENT HEIGHT UTILIZED ON THE AIRPORT FOR THIS PROJECT SHALL BE 30'.
8. THIS PROJECT CONSISTS OF WORK TO APPLY POROUS FRICTION COURSE AND MARK RUNWAY 18-36.
9. THE CONTRACTOR SHALL COORDINATE A METHOD OF DISABLING THE RUNWAY 18-36 EDGE LIGHTS WITH THE AIRPORT MANAGEMENT SO AS TO KEEP THE LIGHTS INOPERABLE WHILE THE RUNWAY IS CLOSED TO AIR TRAFFIC. THE RUNWAY 9-27 EDGE LIGHTS SHALL ALSO BE DISABLED WHEN THAT RUNWAY IS CLOSED TO AIR TRAFFIC. THE COST OF DISABLING AND RE-ENERGIZING THE EDGE LIGHT CIRCUITS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
10. THE UNICOM FREQUENCY AT THE AIRPORT IS 122.8 MHZ. THE CONTRACTOR SHALL FURNISH A FLAGMAN WITH A RADIO IN HIS POSSESSION TO MONITOR AIR TRAFFIC ON THIS FREQUENCY AND DIRECT HAULING EQUIPMENT ACROSS RUNWAY 9-27 DURING CONSTRUCTION. NO VEHICLE, EQUIPMENT OR PERSON SHALL CROSS AN ACTIVE RUNWAY WITHOUT RECEIVING CLEARANCE TO CROSS FROM THE FLAGMAN. VEHICLE DRIVERS SHALL ALSO CONFIRM BY PERSONAL OBSERVATION THAT NO AIRCRAFT IS APPROACHING WHEN GIVEN CLEARANCE TO CROSS.
11. PRIOR TO THE OPERATION OF ANY CONSTRUCTION VEHICLES WITHIN THE AIR OPERATIONS AREA, IT WILL BE REQUIRED THAT THE CONTRACTOR RECEIVE TRAINING FROM THE AIRPORT MANAGEMENT REGARDING PROCEDURES FOR VEHICLE OPERATIONS TO ENSURE AIRCRAFT SAFETY DURING CONSTRUCTION. IT WILL BE REQUIRED THAT THE CONTRACTOR RECEIVE PERMISSION FROM THE AIRPORT MANAGEMENT IN ADVANCE OF ANY ACTIVITY SPECIFIC TO THE DAY, TIME AND EXACT PURPOSE OF ANY NEED TO ENTER THE AIRPORT OPERATIONS AREA.
12. THIS PROJECT WILL DISTURB LESS THAN 1 ACRE OF LAND.



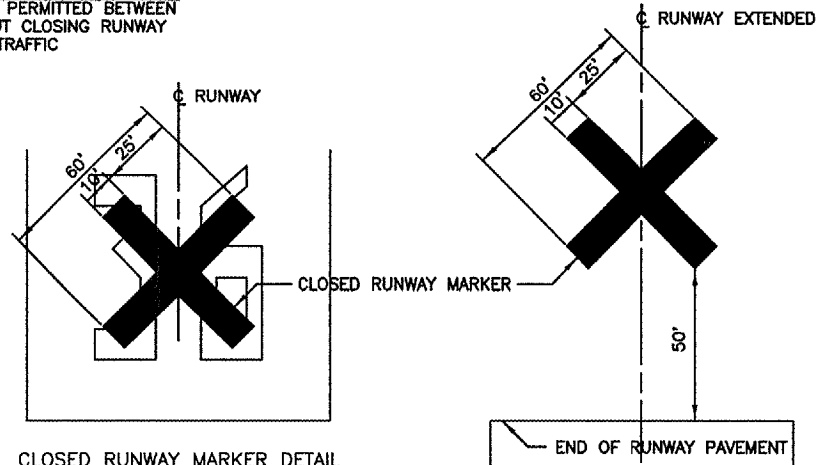
LEGEND

- CONTRACTOR'S ACCESS/HAUL ROUTE
- APPLY POROUS FRICTION COURSE AND MARK RUNWAY 18-36
- LOCATION OF LIGHTED BARRICADES (USING FLASHING RED LIGHTS)

GENERAL NOTES

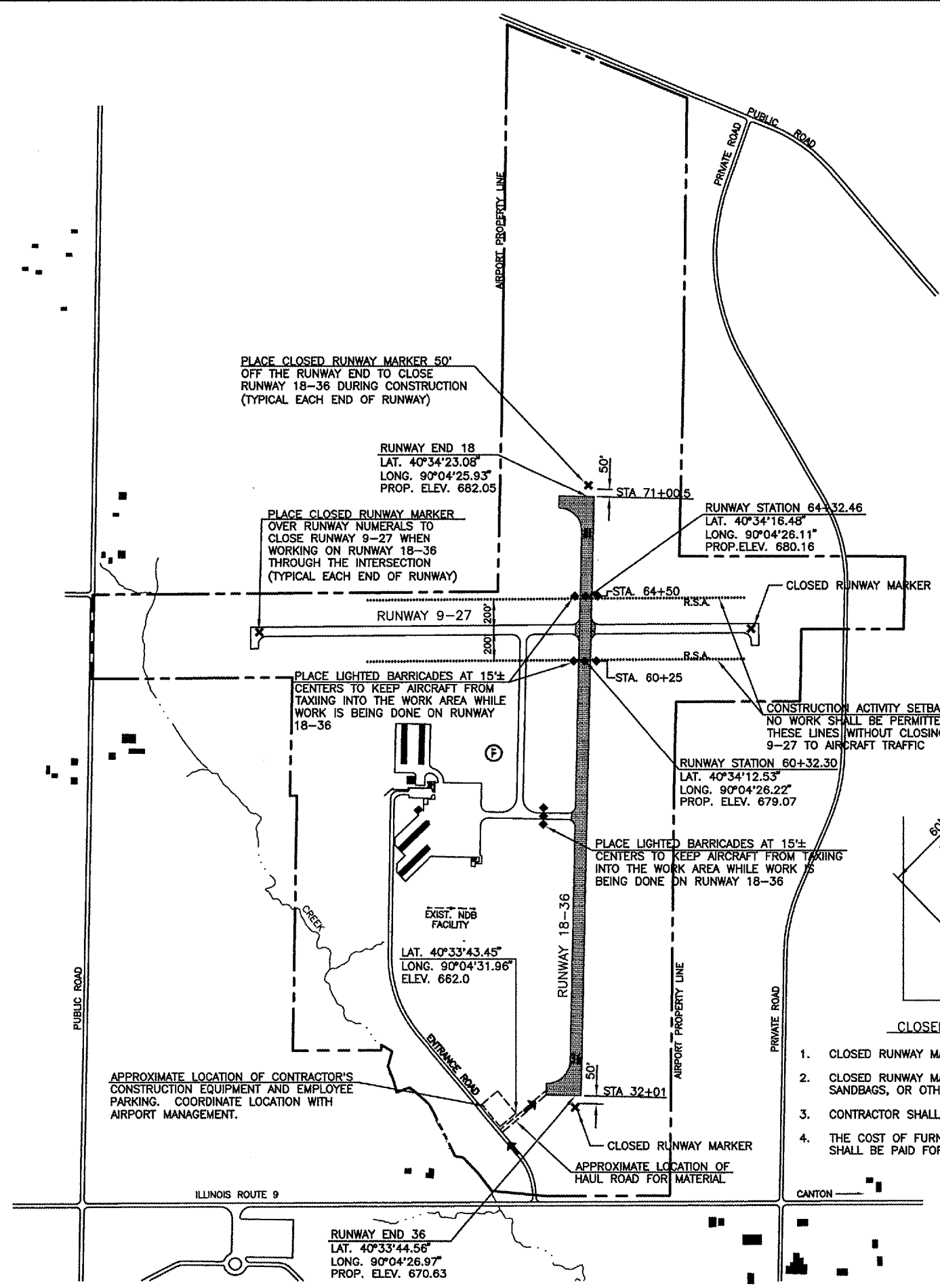
1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS. PRIOR TO STARTING WORK, THE CONTRACTOR SHALL CONTACT J.U.L.I.E. (JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS) PHONE 1-800-892-0123 TO ARRANGE FOR LOCATION OF UTILITIES IN THE WORK AREA. A MINIMUM OF FORTY-EIGHT HOURS ADVANCE NOTICE IS REQUIRED FOR NON-EMERGENCY WORK.

COUNTY - FULTON
 CITY - CANTON
 TOWNSHIP - T. 7 N., R. 4 E. 4th P.M. (CANTON)
 SECTION NO. - 20 & 29



CLOSED RUNWAY MARKER DETAIL

1. CLOSED RUNWAY MARKER SHALL BE YELLOW IN COLOR.
2. CLOSED RUNWAY MARKER SHALL BE PAINTED PLYWOOD, PLASTIC SHEETING WEIGHTED DOWN WITH SANDBAGS, OR OTHER APPROVED MATERIALS ACCEPTABLE TO THE ENGINEER.
3. CONTRACTOR SHALL MAINTAIN MARKERS AS DIRECTED BY THE RESIDENT ENGINEER.
4. THE COST OF FURNISHING, PLACING, MAINTAINING, AND REMOVING CLOSED RUNWAY MARKERS SHALL BE PAID FOR UNDER ITEM AR150530 TRAFFIC MAINTENANCE.



RECOMMENDED CONSTRUCTION SEQUENCE

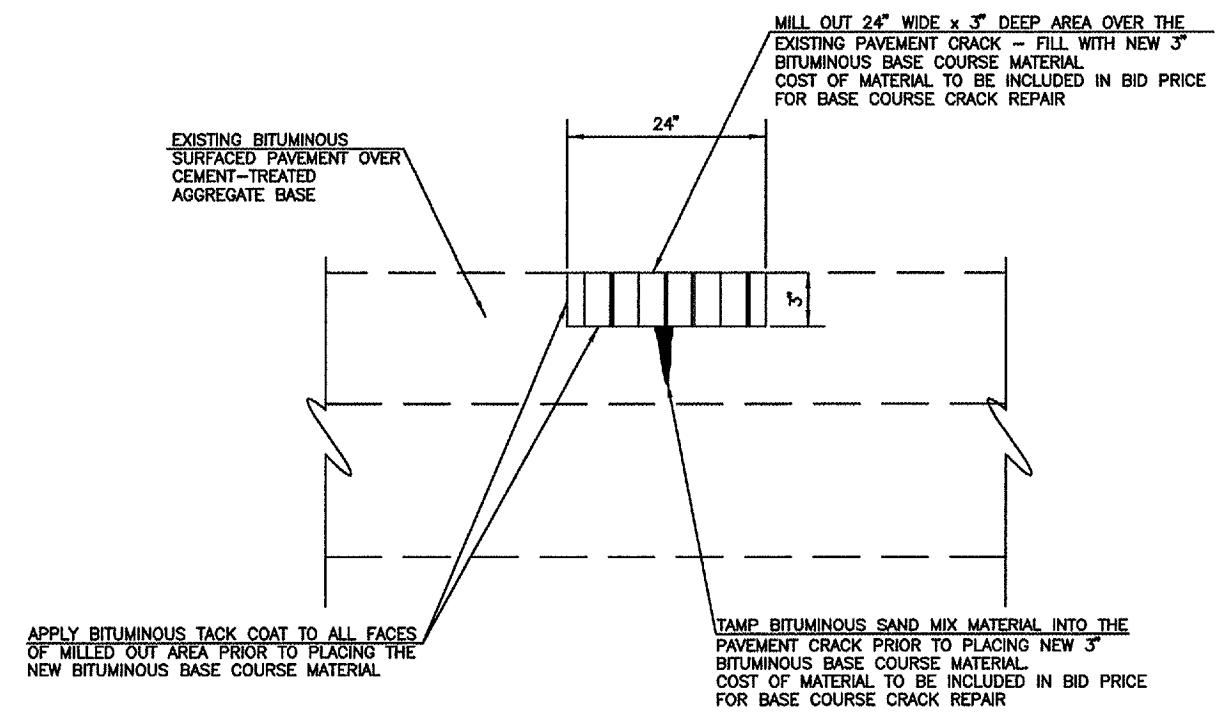
1. INSTALL CLOSED RUNWAY MARKERS AT THE ENDS OF RUNWAY 18-36 TO CLOSE THE RUNWAY DURING THE CONSTRUCTION PERIOD. DISABLE THE RUNWAY 18-36 EDGE LIGHTING CIRCUIT FOR THE DURATION OF THE CONSTRUCTION PERIOD. COORDINATE WITH THE AIRPORT MANAGEMENT TO ARRANGE FOR SHUTDOWN OF RUNWAY 18-36 AND TO ALLOW THE AIRPORT MANAGEMENT TO ISSUE A NOTAMS FOR THE CONSTRUCTION TO BE ACCOMPLISHED.
2. CRACK REPAIR AND PAVEMENT PREPARATION CAN BE ACCOMPLISHED ON RUNWAY 18-36 UP TO A LOCATION 200' EACH SIDE OF THE RUNWAY 18-36 CENTERLINE WHILE KEEPING RUNWAY 9-27 OPEN TO AIR TRAFFIC.
3. RUNWAY 9-27 SHALL THEN BE CLOSED TO AIR TRAFFIC WHICH WILL CLOSE THE AIRPORT. PREPARATORY WORK SHALL BE ACCOMPLISHED ON RUNWAY 18-36 BETWEEN STA. 60+25 AND STA. 64+50. WHILE RUNWAY 9-27 IS STILL CLOSED, PERFORM THE PAVEMENT SURFACING WORK ON RUNWAY 18-36 FROM STA. 60+25 TO STA. 71+00.5 (NORTH END OF RUNWAY). ALL WORK IN THIS AREA SHALL BE DONE AS EXPEDITIOUSLY AS POSSIBLE IN ORDER TO KEEP THE AIRPORT CLOSED FOR AS SHORT A TIME AS IS NECESSARY. ENTIRE AIRPORT CLOSURE SHALL NOT EXCEED FIVE CONSECUTIVE DAYS. UPON COMPLETION OF WORK AND PAVEMENT CURING IN THIS AREA, REMOVE THE CLOSED RUNWAY MARKERS FROM THE RUNWAY 9-27 NUMERALS AND RE-OPEN RUNWAY 9-27 TO AIR TRAFFIC.
4. PAVEMENT SURFACING WORK SHALL THEN BE ACCOMPLISHED ON RUNWAY 18-36 FROM STA. 32+01 (SOUTH END OF THE RUNWAY) TO STA. 60+25 WHILE AIR TRAFFIC USES RUNWAY 9-27. UPON COMPLETION OF SHOULDER RESTORATION AND PAVEMENT MARKING ON RUNWAY 18-36, REMOVE THE CLOSED RUNWAY MARKERS FROM RUNWAY 18-36 AND RE-OPEN THE RUNWAY TO AIR TRAFFIC.
5. WORK SHALL BE SEQUENCED SO AS NOT TO ALLOW THE CONTRACTOR'S HAULING EQUIPMENT TO DRIVE ON THE FINISHED PAVEMENT. CONTRACTOR SHALL MAKE EVERY EFFORT TO REFRAIN FROM TRACKING BITUMINOUS TACK COAT MATERIAL ONTO EXISTING PAVEMENT SURFACES OF THE AIRPORT WITH HIS HAULING EQUIPMENT.

INGERSOLL AIRPORT
 CANTON, ILLINOIS

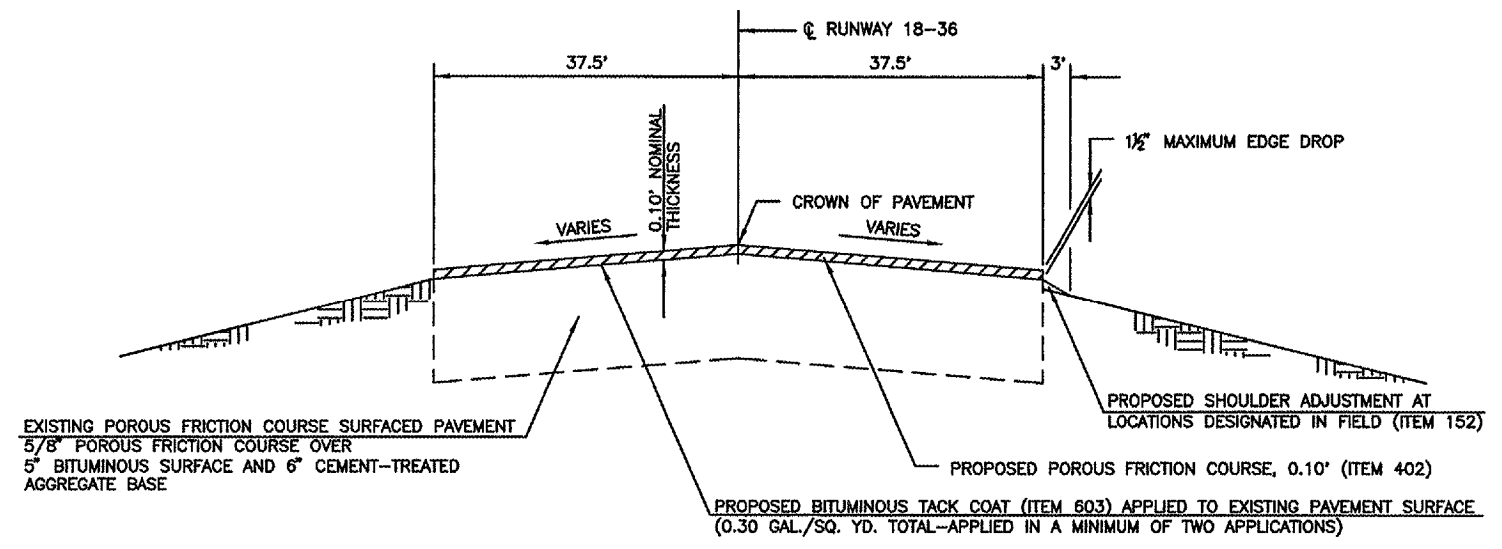
IMPROVEMENT AND SAFETY PLAN

HUTCHISON ENGINEERING, INC.
 JACKSONVILLE, ILLINOIS

DRAWN BY: T.J.D. ILL. PROJ. NO. CTR-3713
 DATE: NOVEMBER, 2007 IAP PROJ. NO. 3-17-0008-89



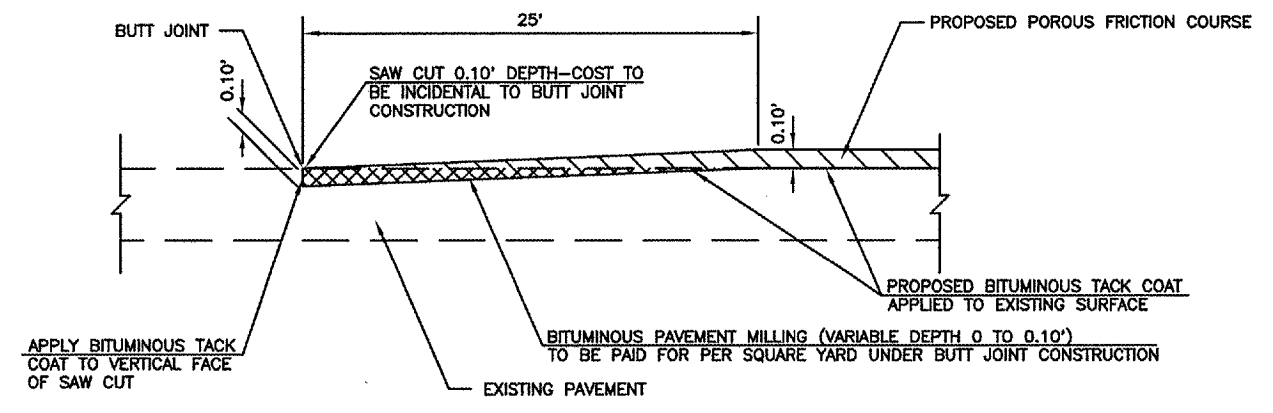
BASE COURSE CRACK REPAIR DETAIL
NOT TO SCALE



RUNWAY 18-36 TYPICAL SECTION
NOT TO SCALE

GENERAL NOTES

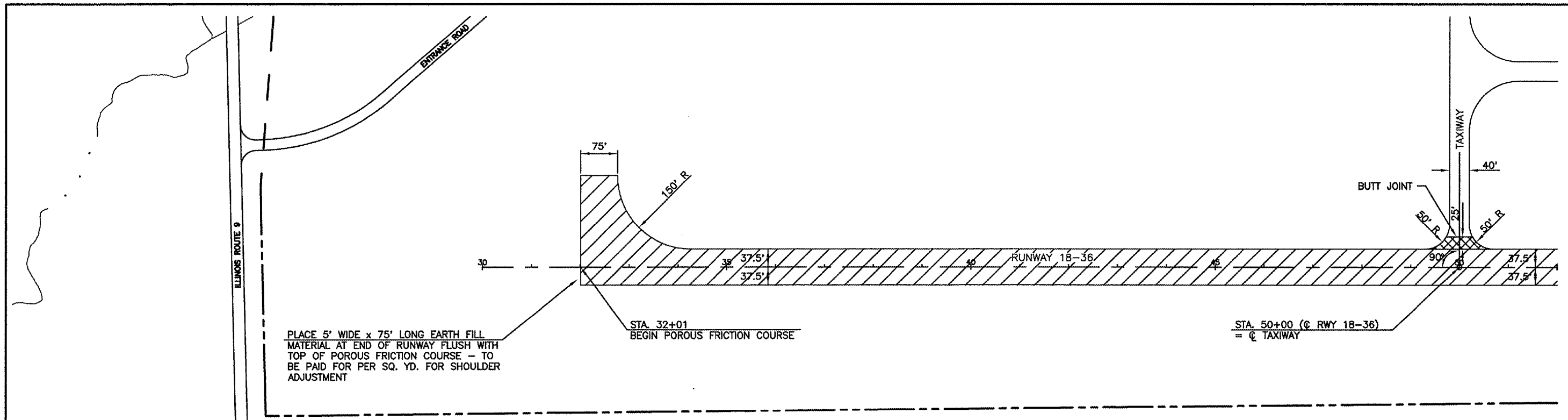
1. OPEN PAVING JOINTS AND/OR CRACKS IN THE EXISTING SURFACE OF THE PAVEMENT OVER 3/8" IN WIDTH SHALL BE DESIGNATED BY THE PROJECT ENGINEER IN THE FIELD TO BE REPAIRED PRIOR TO PLACING THE POROUS FRICTION COURSE. THIS WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE SPECIAL PROVISIONS FOR THE DESIGNATED TYPE OF CRACK REPAIR.
2. THE QUANTITIES FOR BITUMINOUS CRACK REPAIR HAVE BEEN ESTIMATED BY MEASURING CRACK REPAIR IN SELECTED REPRESENTATIVE LOCATIONS ON THE PAVEMENT. THESE QUANTITIES MAY BE INCREASED OR REDUCED AT THE TIME OF CONSTRUCTION BASED UPON THE CRACK WORK THAT IS MARKED OUT BY THE RESIDENT ENGINEER AFTER FIELD INSPECTION OF THE PAVEMENT CRACKS.
3. ANY SOD WHICH HAS ENCRACHED UPON THE PAVEMENT EDGE SHALL BE BLADED BACK PRIOR TO THE PROPOSED POROUS FRICTION COURSE APPLICATION. THE ENTIRE PAVEMENT SURFACE SHALL BE SWEEPED TO REMOVE ALL LOOSE MATERIALS BEFORE THE APPLICATION OF BITUMINOUS TACK COAT. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR PAVEMENT PREPARATION.
4. ANY DAMAGE TO THE EXISTING SOD BEYOND THE EDGE OF PAVEMENT RESULTING FROM THE BLADING OF THE SOD OFF THE PAVEMENT SURFACE SHALL BE FILLED WITH DIRT, GRADED, FERTILIZED AND SEED TO RESTORE THE SOD SHOULDERS TO THEIR ORIGINAL CONDITION. ANY LOCALLY AVAILABLE 12-12-12 FERTILIZER MAY BE USED APPLIED AT A RATE OF 1 POUND OF FERTILIZER PER EACH 160 SQUARE FEET OF DISTURBED OR RESEEDING AREA. SEED USED SHALL BE ALTA FESCUE OR KENTUCKY 31, APPLIED AT THE RATE OF 1 POUND OF PURE LIVE SEED PER EACH 436 SQUARE FEET OF RESEEDING AREA. THIS WORK WILL NOT BE PAID FOR SEPARATELY AND SHALL BE CONSIDERED INCIDENTAL TO PAVEMENT PREPARATION.
5. A BITUMINOUS TACK COAT SHALL BE APPLIED TO THE EXISTING PAVEMENT PRIOR TO PLACING THE POROUS FRICTION COURSE. THE APPLICATION RATE SHOWN ON THE TYPICAL SECTION IS THE TOTAL RATE OF DILUTED TACK COAT. THE PLAN QUANTITY FOR BITUMINOUS TACK COAT HAS BEEN DETERMINED USING A RATE OF 0.27 GAL./SQ. YD OF UNDILUTED EMULSION. THE EXACT AMOUNT OF DILUTED BITUMINOUS TACK COAT TO BE USED SHALL BE DETERMINED BY THE PROJECT ENGINEER AT THE TIME OF CONSTRUCTION. THE QUANTITY OF BITUMINOUS TACK COAT TO BE PAID FOR SHALL BE THE NUMBER OF GALLONS OF UNDILUTED EMULSION USED AND ACCEPTED.
6. THE POROUS FRICTION COURSE SHALL BE ABUTTED TO THE EXISTING PAVEMENT AT RUNWAY 9-27 AND THE INTERSECTING TAXIWAY AS SHOWN ON THE BITUMINOUS BUTT JOINT DETAIL. THE EXISTING PAVEMENT CONFINED WITHIN THE BITUMINOUS PAVEMENT MILLING AREA WILL BE REMOVED IN ACCORDANCE WITH THE DETAIL FROM 0 TO 0.10' DEPTH AT THE BUTT JOINT LOCATION. FILLING OF THE MILLED AREA WILL BE MADE USING POROUS FRICTION COURSE MIX AND WILL BE PAID FOR PER SQUARE YARD FOR POROUS FRICTION COURSE.
7. AT LOCATIONS WHERE THE EDGE DROP FROM THE FINISHED SURFACE TO THE EXISTING TURF SHOULDER EXCEEDS 1/2", THE SHOULDER SHALL BE ADJUSTED AS SHOWN IN THE TYPICAL SECTION USING SOIL OBTAINED OFF SITE THAT IS FREE OF HEAVY SOD, DECAYED VEGETATIVE MATTER, RUBBISH, AGGREGATE AND OTHER UNSUITABLE MATERIAL. THE SHOULDER ADJUSTMENT AREAS SHALL BE FERTILIZED AND SEED AND THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR SHOULDER ADJUSTMENT.
8. ALL AREAS DISTURBED BY THE CONTRACTOR INCLUDING THE CONTRACTOR'S PARKING AREA, HAUL ROADS, ETC. SHALL BE REGRADED AND RESEED AND/OR RESTORED TO THEIR ORIGINAL CONDITION. THIS RESTORATION SHALL BE ACCOMPLISHED BY THE CONTRACTOR AT HIS OWN EXPENSE UPON COMPLETION OF THIS IMPROVEMENT PROJECT.



BITUMINOUS BUTT JOINT CONSTRUCTION DETAIL
NOT TO SCALE

INGERSOLL AIRPORT CANTON, ILLINOIS	
TYPICAL SECTION AND SPECIAL DETAILS	
HUTCHISON ENGINEERING, INC. JACKSONVILLE, ILLINOIS	
DRAWN BY: T.J.D. DATE: NOVEMBER, 2007	ILL. PROJ. NO. CRK-3713 AIP PROJ. NO. 3-17-0008-B9

REV. MAR. 10, 2008



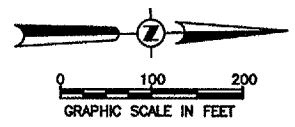
PLACE 5' WIDE x 75' LONG EARTH FILL MATERIAL AT END OF RUNWAY FLUSH WITH TOP OF POROUS FRICTION COURSE - TO BE PAID FOR PER SQ. YD. FOR SHOULDER ADJUSTMENT

STA. 32+01 BEGIN POROUS FRICTION COURSE

STA. 50+00 (C RWY 18-36) = C TAXIWAY

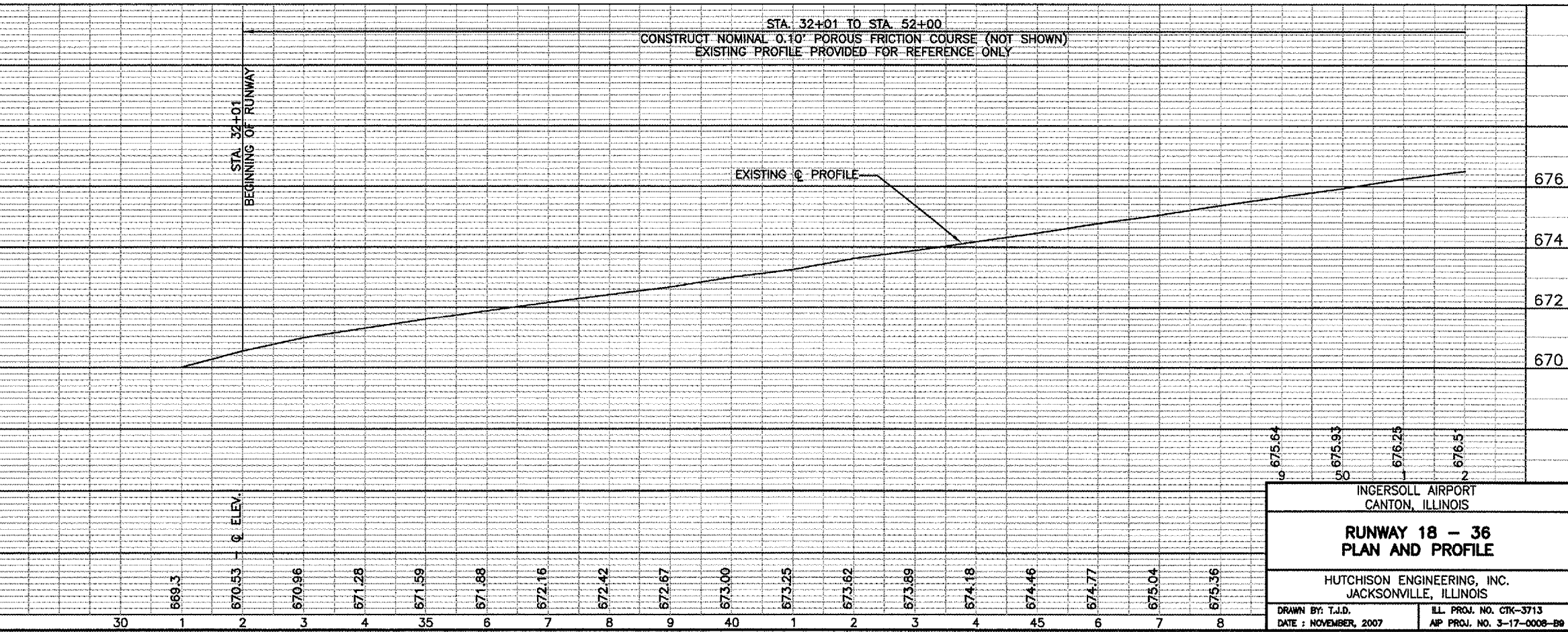
LEGEND

- POROUS FRICTION COURSE
- BITUMINOUS PAVEMENT MILLING FOR BUTT JOINT CONSTRUCTION



BASE COURSE CRACK REPAIR

TURNAROUND 85' LT	STA. 32+01 TO STA. 33+10	= 25.0 SQ. YD.
12.5' LT TO 37.5' LT	STA. 32+80	= 5.8 SQ. YD.
C TO 14' LT	STA. 32+89	= 3.1 SQ. YD.
C TO 37.5' LT	STA. 33+69	= 8.3 SQ. YD.
C TO 20' LT	STA. 34+03	= 4.4 SQ. YD.
27.5' RT TO 37.5' RT	STA. 34+23	= 2.2 SQ. YD.
C TO 28' LT	STA. 34+68	= 6.2 SQ. YD.
25' RT TO 37.5' RT	STA. 34+68	= 2.8 SQ. YD.
12' RT TO 37.5' RT	STA. 35+34	= 5.7 SQ. YD.
30' RT TO 37.5' RT	STA. 35+79	= 1.7 SQ. YD.
25' RT TO 37.5' RT	STA. 36+09	= 2.8 SQ. YD.
12.5' RT TO 37.5' RT	STA. 36+59	= 5.6 SQ. YD.
C TO 20' LT	STA. 36+89	= 4.4 SQ. YD.
10' RT TO 37.5' RT	STA. 36+89	= 6.1 SQ. YD.
C TO 25' LT	STA. 37+31	= 5.6 SQ. YD.
C TO 37.5' RT	STA. 38+56	= 8.3 SQ. YD.
12.5' RT TO 37.5' RT	STA. 38+86	= 5.6 SQ. YD.
C TO 20' RT	STA. 39+09	= 4.4 SQ. YD.
12.5' LT TO 25' LT	STA. 39+78	= 2.8 SQ. YD.
C TO 37.5' RT	STA. 39+78	= 8.3 SQ. YD.
12.5' LT TO 25' LT	STA. 40+24	= 2.8 SQ. YD.
10' RT TO 37.5' RT	STA. 40+44	= 6.1 SQ. YD.
10' LT TO 37.5' LT	STA. 41+12	= 6.1 SQ. YD.
10' RT TO 37.5' RT	STA. 41+12	= 6.1 SQ. YD.
C TO 12.5' RT	STA. 41+79	= 2.8 SQ. YD.
12.5' LT TO 37.5' LT	STA. 42+11	= 5.6 SQ. YD.
12.5' RT TO 37.5' RT	STA. 42+11	= 5.6 SQ. YD.
10' RT TO 37.5' RT	STA. 42+63	= 6.1 SQ. YD.
10' RT TO 37.5' RT	STA. 42+70	= 6.1 SQ. YD.
37.5' LT TO 37.5' RT	STA. 43+16	= 16.7 SQ. YD.
12.5' LT TO 37.5' LT	STA. 43+69	= 5.6 SQ. YD.
12.5' RT TO 37.5' RT	STA. 43+69	= 5.6 SQ. YD.
25' RT TO 37.5' RT	STA. 44+79	= 2.8 SQ. YD.
12.5' LT TO 37.5' LT	STA. 45+15	= 5.6 SQ. YD.
12.5' RT TO 27.5' RT	STA. 45+15	= 3.3 SQ. YD.
7' LT TO 37.5' LT	STA. 45+82	= 6.8 SQ. YD.
12.5' RT TO 37.5' RT	STA. 45+82	= 5.6 SQ. YD.
C TO 10' RT	STA. 46+27	= 2.2 SQ. YD.
12.5' RT TO 37.5' RT	STA. 46+32	= 5.6 SQ. YD.
15' LT TO 37.5' RT	STA. 47+16	= 11.7 SQ. YD.
C TO 25' LT	STA. 47+84	= 5.6 SQ. YD.
7' RT TO 15' RT	STA. 48+14	= 1.8 SQ. YD.
10' LT TO 37.5' LT	STA. 48+30	= 6.1 SQ. YD.
37.5' LT TO 37.5' RT	STA. 48+93	= 16.7 SQ. YD.
10' LT TO 10' RT	STA. 49+33	= 4.4 SQ. YD.
25' LT TO 37.5' LT	STA. 50+68	= 2.8 SQ. YD.
25' LT TO 37.5' LT	STA. 51+03	= 2.8 SQ. YD.
TOTAL THIS SHEET		= 277.9 SQ. YD.



INGERSOLL AIRPORT
CANTON, ILLINOIS

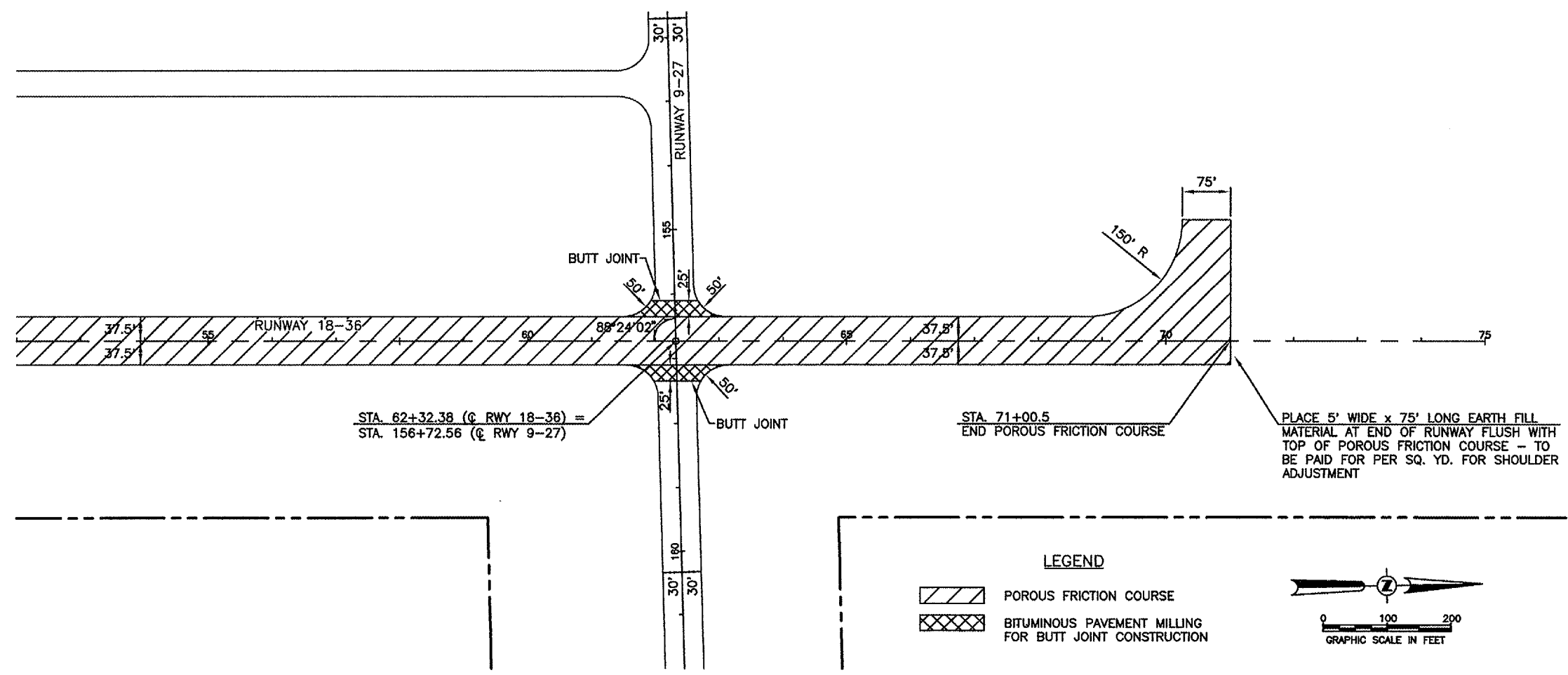
**RUNWAY 18 - 36
PLAN AND PROFILE**

HUTCHISON ENGINEERING, INC.
JACKSONVILLE, ILLINOIS

DRAWN BY: T.J.D. ILL. PROJ. NO. CTK-3713
DATE: NOVEMBER, 2007 AIP PROJ. NO. 3-17-0008-B9

BASE COURSE CRACK REPAIR

30' RT TO 37.5' RT	STA. 52+61 =	1.7 SQ. YD.
37.5' LT TO 37.5' RT	STA. 53+10 =	16.7 SQ. YD.
25' LT TO 37.5' LT	STA. 53+62 =	2.8 SQ. YD.
@ TO 12.5' LT	STA. 53+67 =	2.8 SQ. YD.
12.5' RT TO 37.5' RT	STA. 54+17 =	5.6 SQ. YD.
@ TO 12.5' LT	STA. 54+26 =	2.8 SQ. YD.
37.5' LT TO 37.5' RT	STA. 54+62 =	16.7 SQ. YD.
@ TO 12.5' LT	STA. 55+18 =	2.8 SQ. YD.
@ TO 37.5' LT	STA. 55+58 =	8.3 SQ. YD.
7' RT TO 37.5' RT	STA. 55+58 =	6.8 SQ. YD.
10' LT TO 37.5' LT	STA. 56+67 =	6.1 SQ. YD.
12.5' RT TO 37.5' RT	STA. 56+67 =	5.6 SQ. YD.
25' LT TO 37.5' LT	STA. 57+15 =	2.8 SQ. YD.
25' LT TO 37.5' LT	STA. 57+37 =	2.8 SQ. YD.
25' LT TO 37.5' LT	STA. 58+58 =	2.8 SQ. YD.
25' LT TO 37.5' LT	STA. 58+96 =	2.8 SQ. YD.
25' LT TO 37.5' LT	STA. 59+25 =	2.8 SQ. YD.
37.5' LT TO 25' RT	STA. 59+76 =	13.9 SQ. YD.
25' LT TO 37.5' LT	STA. 60+06 =	2.8 SQ. YD.
37.5' LT TO 25' RT	STA. 60+31 =	13.9 SQ. YD.
37.5' LT TO 12.5' RT	STA. 61+12 =	11.1 SQ. YD.
@ TO 37.5' RT	STA. 63+81 =	8.3 SQ. YD.
25' RT TO 37.5' RT	STA. 64+53 =	2.8 SQ. YD.
@ TO 12.5' RT	STA. 65+09 =	2.8 SQ. YD.
@ TO 37.5' RT	STA. 65+63 =	8.3 SQ. YD.
@ TO 37.5' LT	STA. 65+68 =	8.3 SQ. YD.
27' LT TO 25' RT	STA. 66+16 =	11.6 SQ. YD.
30' LT TO 37.5' RT	STA. 66+65 =	15.0 SQ. YD.
25' RT TO 37.5' RT	STA. 67+25 =	2.8 SQ. YD.
27' LT TO 15' RT	STA. 67+44 =	9.3 SQ. YD.
37.5' LT TO 37.5' RT	STA. 68+21 =	16.7 SQ. YD.
@ TO 37.5' RT	STA. 68+64 =	8.3 SQ. YD.
37.5' LT TO 25' RT	STA. 69+15 =	13.9 SQ. YD.
10' LT TO 10' RT	STA. 69+54 =	4.4 SQ. YD.
12.5' LT TO 25' LT	STA. 70+10 =	2.8 SQ. YD.
TURNAROUND 85' LT	STA. 70+44.5 TO	
	STA. 71+00.5 =	12.4 SQ. YD.
TOTAL THIS SHEET =		262.1 SQ. YD.

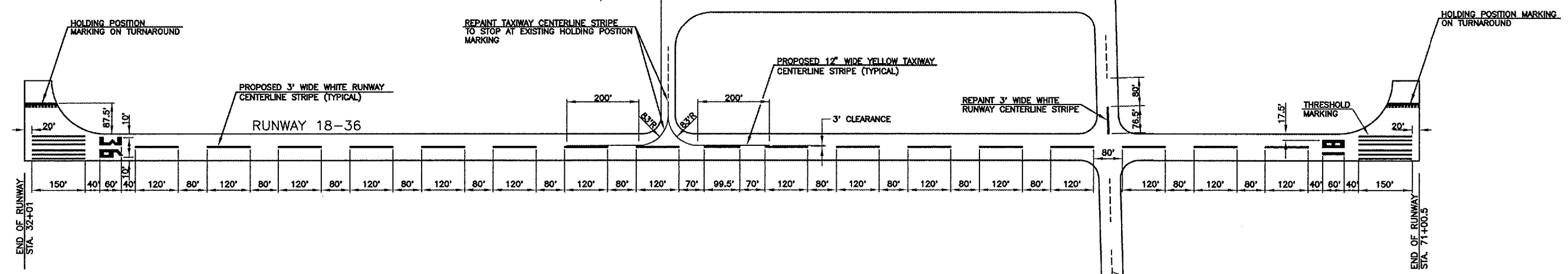
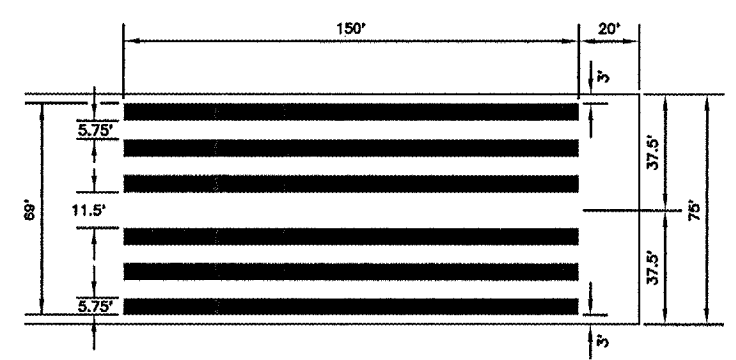
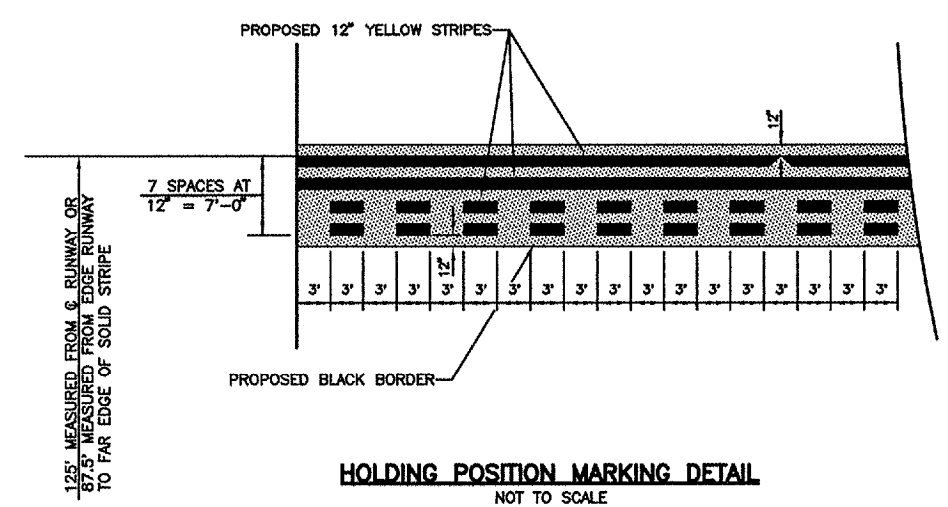


INGERSOLL AIRPORT
CANTON, ILLINOIS

**RUNWAY 18 - 36
PLAN AND PROFILE**

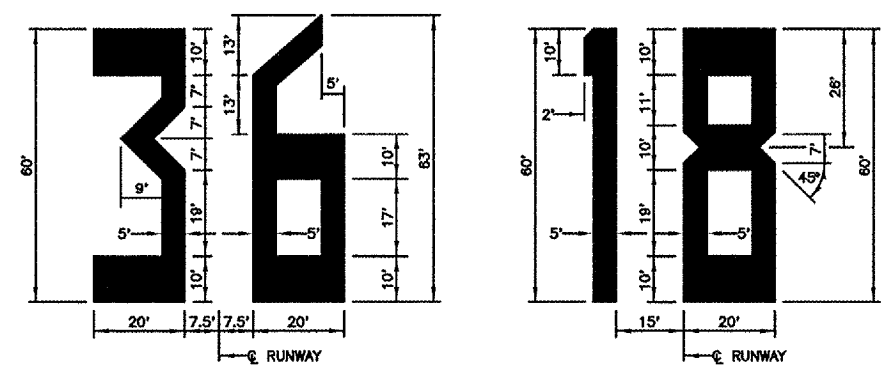
HUTCHISON ENGINEERING, INC.
JACKSONVILLE, ILLINOIS

DRAWN BY: T.J.D. ILL. PROJ. NO. CRK-3713
DATE: NOVEMBER, 2007 AIP PROJ. NO. 3-17-0008-89

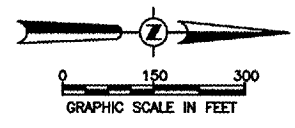


MARKING SUMMARY

DESCRIPTION	UNIT AREA SQ. FT.	NUMBER REQ'D	WHITE PAINT SQ. FT.	YELLOW PAINT SQ. FT.	BLACK PAINT SQ. FT.
NUMERAL "3"	633		633		
NUMERAL "6"	712		712		
NUMERAL "4"	318		318		
NUMERAL "8"	876		876		
THRESHOLD MARKING	5,175	2	10,350		
RUNWAY CENTERLINE STRIPES	360	16	5,760		
RUNWAY CENTERLINE STRIPE	299	1	299		
RUNWAY CENTERLINE STRIPE (RWY 9-27)	229	1	229		
TAXIWAY CENTERLINE STRIPES	701			701	
HOLDING POSITION MARKING (TURNAROUND)	269-YELLOW 545-BLACK	2		538	1,090
TOTALS			19,177	1,239	1,090



- NOTES**
1. ALL RUNWAY MARKING SHALL BE SOLID MARKING AND WHITE IN COLOR.
 2. ALL TAXIWAY MARKING SHALL BE SOLID MARKING AND YELLOW IN COLOR.
 3. ALL PAVEMENT MARKING SHALL CONSIST OF TWO APPLICATIONS OF WATERBORNE PAINT AND ONE APPLICATION OF TYPE B GLASS BEADS (REFLECTIVE MEDIA). PAINT SHALL BE APPLIED AT THE RATE OF 115 SQ. FT./GAL. MAXIMUM PER APPLICATION. GLASS BEADS SHALL BE APPLIED AT THE RATE OF 7 LBS./GAL. OF PAINT MINIMUM. GLASS BEADS WILL NOT BE REQUIRED ON THE BLACK PAINT.



INGERSOLL AIRPORT
CANTON, ILLINOIS

MARKING LAYOUT

HUTCHISON ENGINEERING, INC.
JACKSONVILLE, ILLINOIS

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