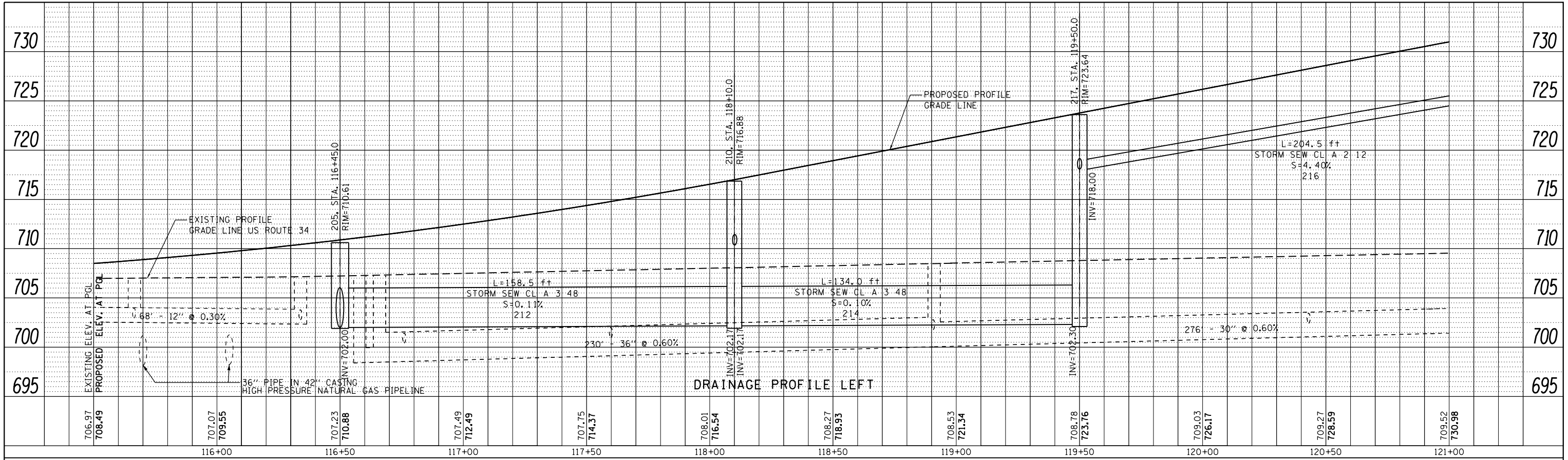
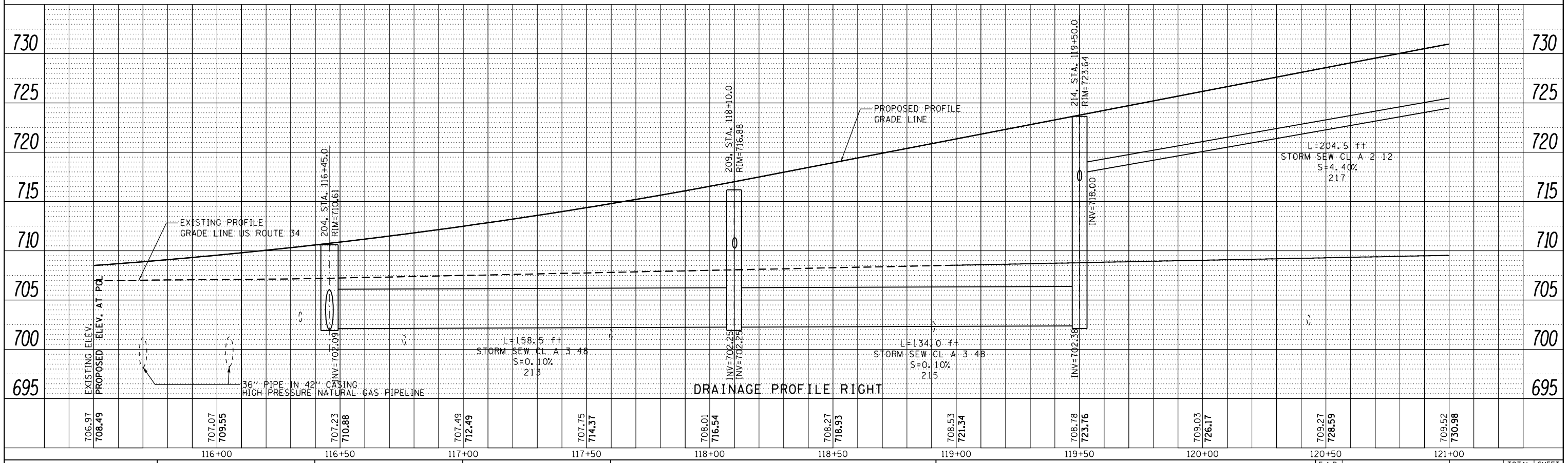




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	PLOTTED		
	GRADES CHECKED		
	ALIGNMENT CHECKED		
	STRUCTURE NOTATIONS OK'D		
	CARD FILE NAME		
	NO.		



PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	ALIGNMENT CHECKED		
	STRUCTURE NOTATIONS OK'D		
	CARD FILE NAME		
	NO.		



USER NAME = URS	DESIGNED - MB	REVISED -
PLOT SCALE = 40.0000' / in.	DRAWN - KJB	REVISED -
PLOT DATE = 02/28/2014	CHECKED - SPF	REVISED -
	DATE - 02/28/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SCALE: 20		SHEET NO. 3 OF 8 SHEETS		STA. 115+50 TO STA. 121+00	
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DRAINAGE AND UTILITY PROFILE  
US ROUTE 34

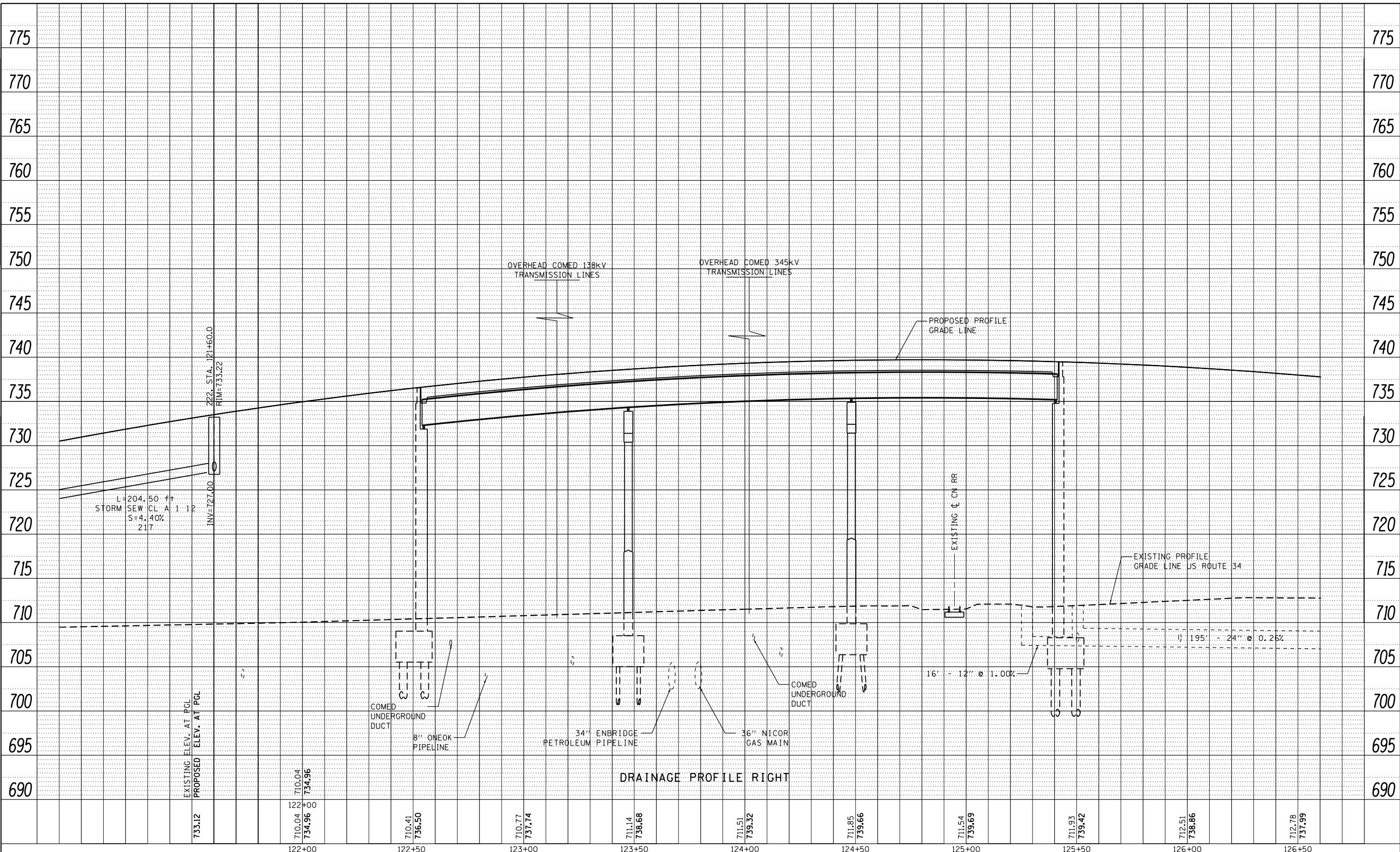
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	102
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



PLAN	SURVEYED	BY	DATE
	PLOTTED		
	NOTE BOOK		
	NO.		
	CARD FILE NAME		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOT AT THIS OFFICE		
	NO.		

FILE NAME = Q:\DOT\_25368367\_US\_34.ctb; C:\Programs\Roadway\Drawings\Utility\Drawings\Profile\Profile\_0714.dgn



USER NAME	= URS	DESIGNED	- MB	REVISED	-
		DRAWN	- KJB	REVISED	-
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PLOT DATE	= 02/28/2014	DATE	- 02/28/2014	REVISED	-

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

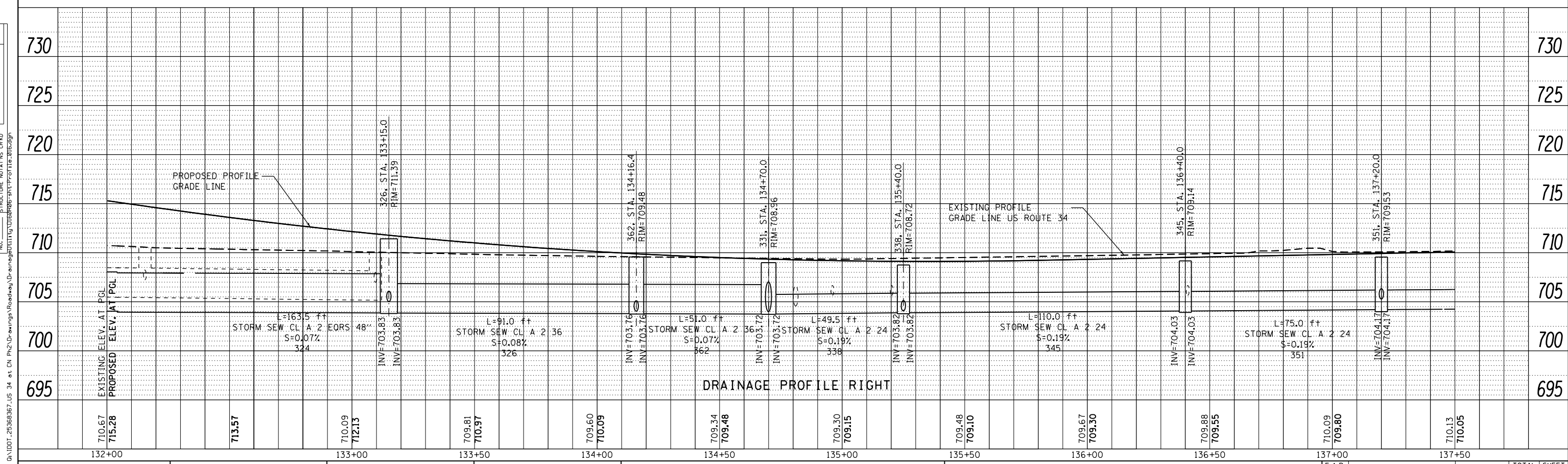
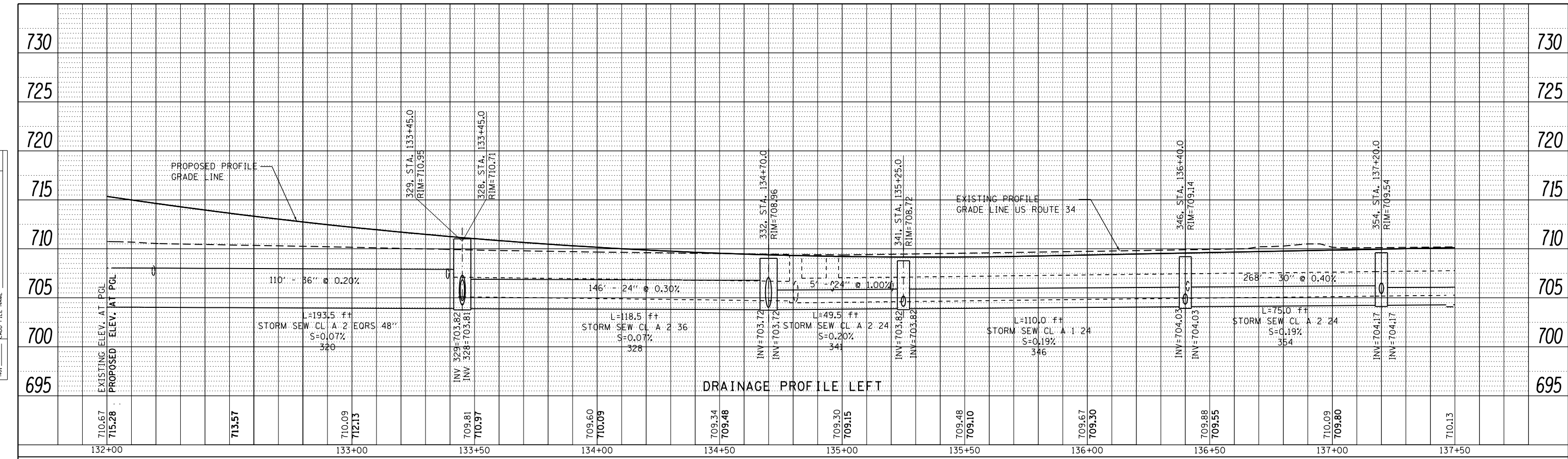
DRAINAGE AND UTILITY PROFILE US ROUTE 34			
SCALE:	20	SHEET NO. 5	OF 8 SHEETS
STA. 121+00		TO STA. 126+50	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	104
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



PLAN	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	ALIGNMENT CHECKED		
	STRUCTURE NOT AT THIS OFFICE		
	NO. _____		
	CARD FILE NAME		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	ALIGNMENT CHECKED		
	STRUCTURE NOT AT THIS OFFICE		
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	CARD FILE NAME		

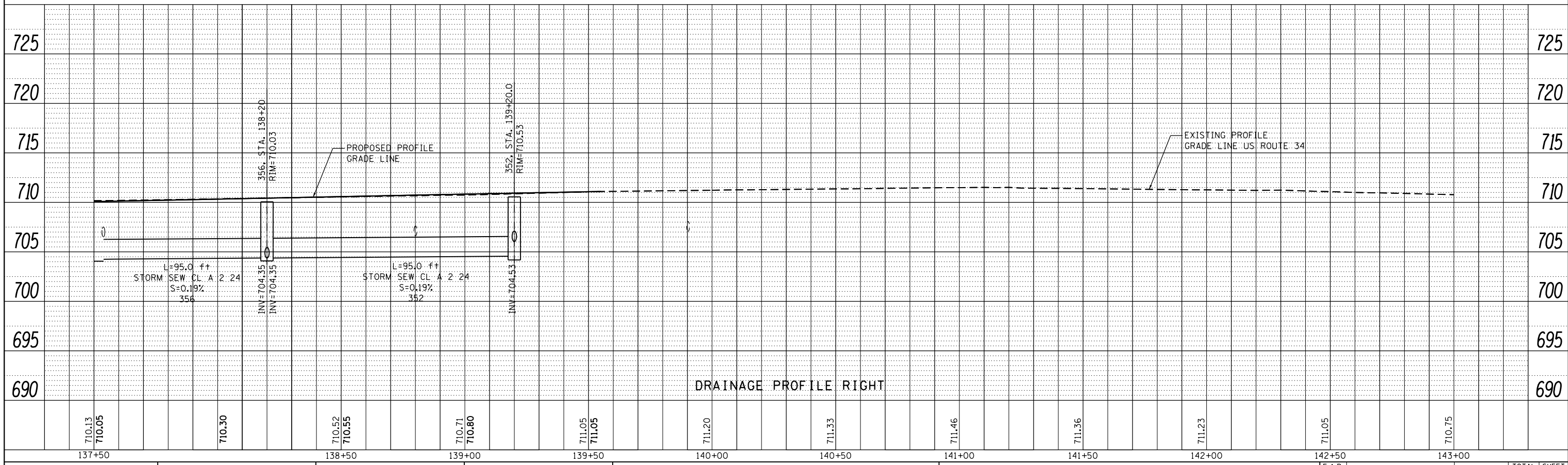


<b>URS</b>	USER NAME = URS	DESIGNED - MB	REVISED -	<b>STATE OF ILLINOIS</b>	<b>DRAINAGE AND UTILITY PROFILE</b>	SCALE: 20	SHEET NO. 7 OF 8 SHEETS	STA. 132+00 TO STA. 137+50	SECTION 652-A	COUNTY	TOTAL SHEETS 383	SHEET NO. 106
	PLOT SCALE = 48.0000' / in.	DRAWN - KJB	REVISED -									
	PLOT DATE = 02/28/2014	CHECKED - SPF	REVISED -	<b>DEPARTMENT OF TRANSPORTATION</b>	<b>US ROUTE 34</b>				FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS OK'D		
	NOTE BOOK		
	NO.		
	CARD FILE NAME		



PROFILE	SURVEYED	BY	DATE
	PLOTTED		
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	NO.		
	CARD FILE NAME		

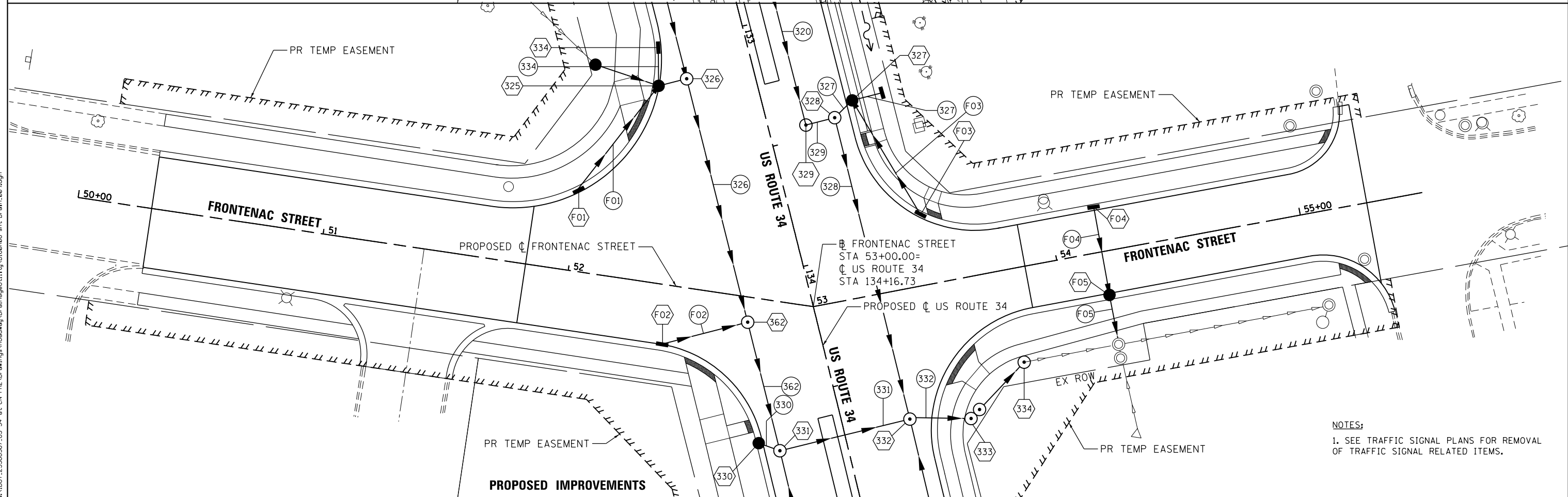
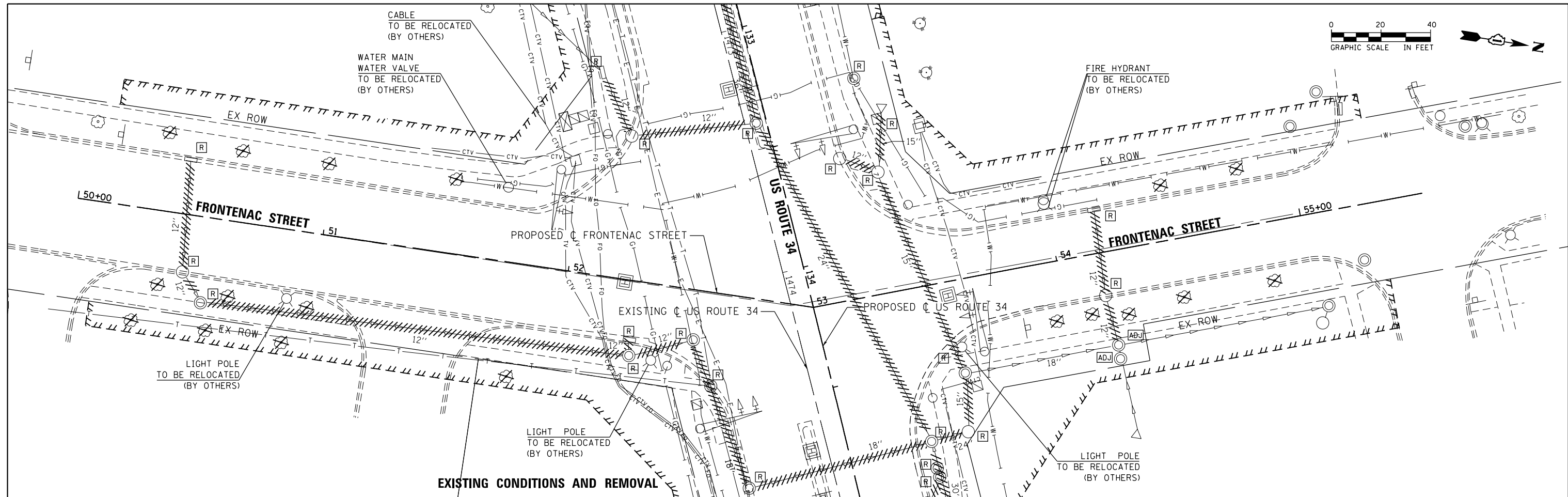
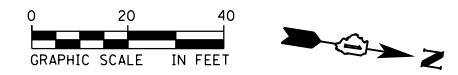


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PLOT DATE = 02/28/2014	CHECKED - SPF	REVISED -
	DATE - 02/28/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DRAINAGE AND UTILITY PROFILE US ROUTE 34</b>	
SCALE: 20	SHEET NO. 8 OF 8 SHEETS
STA. 137+50	TO STA. 143+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	107
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**  
 1. SEE TRAFFIC SIGNAL PLANS FOR REMOVAL OF TRAFFIC SIGNAL RELATED ITEMS.

FILE NAME = Q:\1\DOT\_25368367\_US\_34.ctb; CN Ph2\Drawings\Roadway\DrainageUtility\11616106-sht-Dr-am\_009.dgn



USER NAME = URS	DESIGNED - MB	REVISED -
PLOT SCALE = 40.0000' / in.	DRAWN - KJB	REVISED -
PLOT DATE = 02/28/2014	CHECKED - SPF	REVISED -
	DATE - 02/28/2014	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DRAINAGE AND UTILITY PLAN  
 FRONTENAC STREET**

SCALE: 20 SHEET NO. 1 OF 1 SHEETS STA. 50+00 TO STA. 55+00

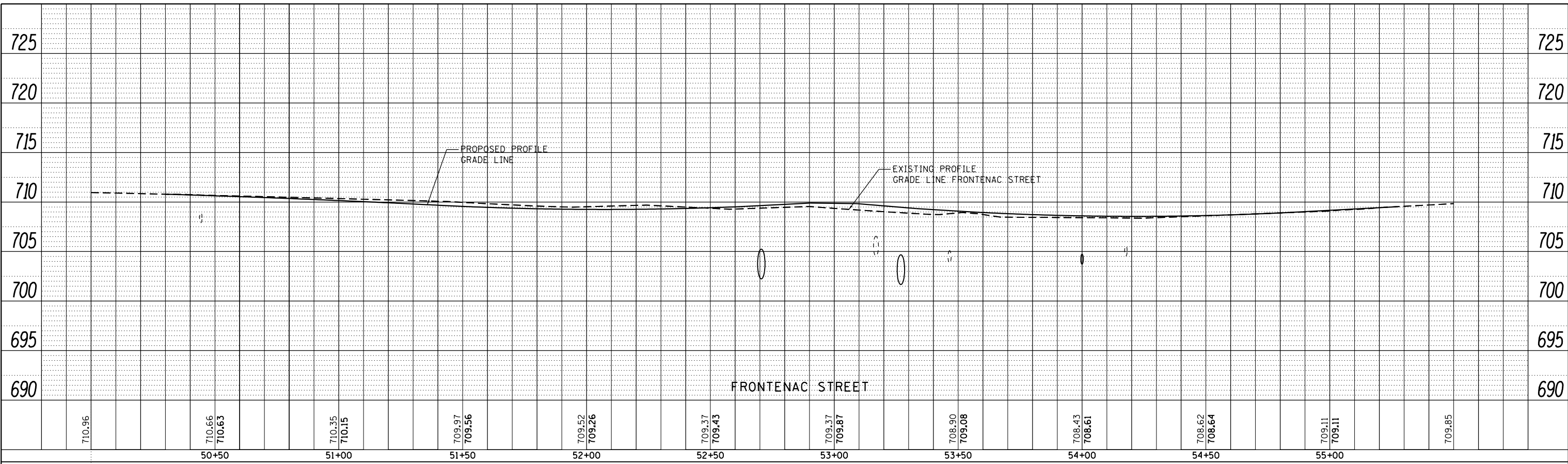
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	108
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



PLAN	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	ALIGNMENT CHECKED		
	GRADE FILE NAME		

PROFILE	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOT AT THIS CHKD		

FILE NAME = Q:\YDOT\_25368367\_US\_34.ctb; CN Pk2\Drawings\Roadway\Drainage\Utility\10122014\25368367-ant-profile-1012.dgn

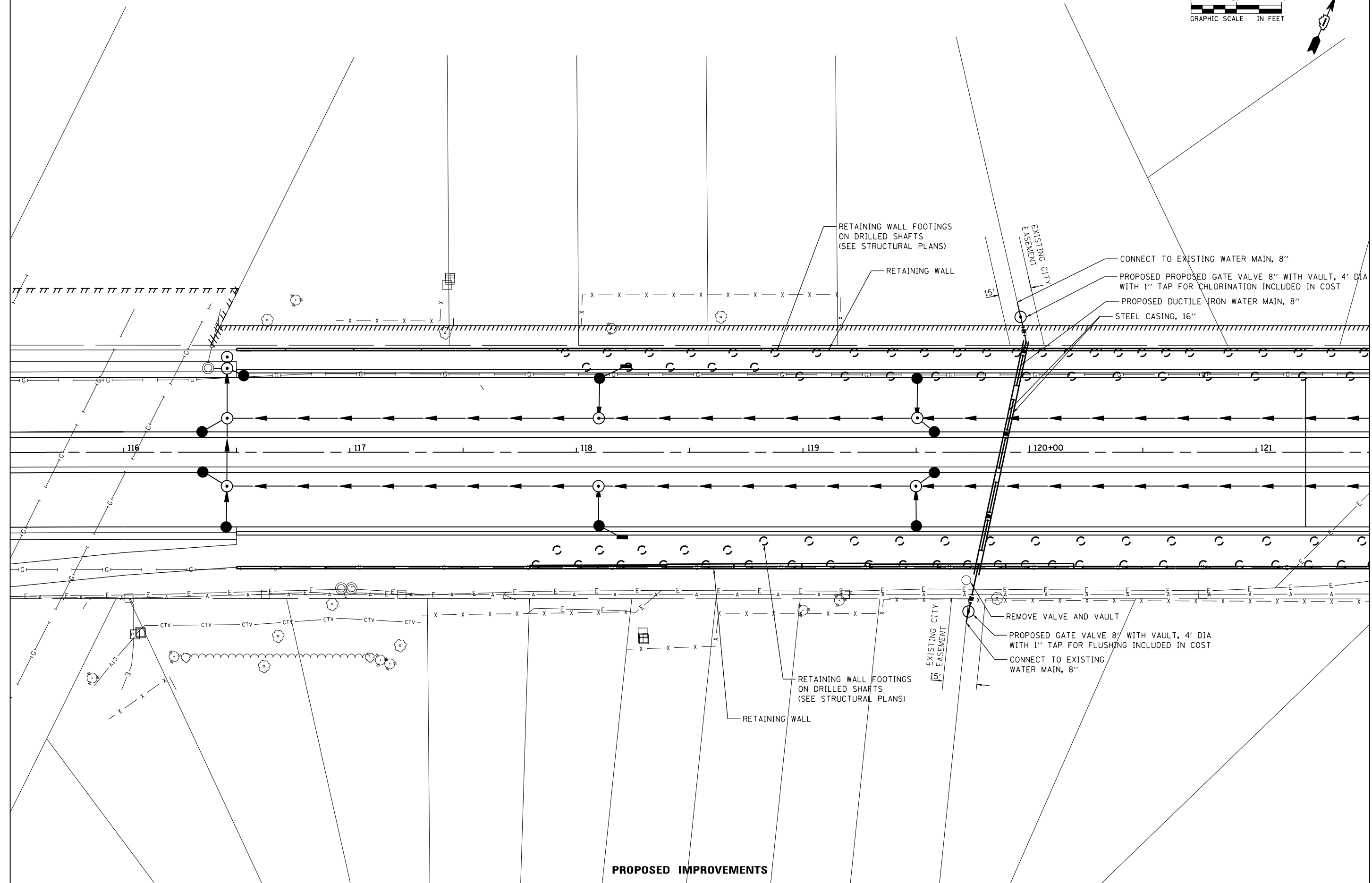
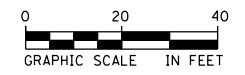


USER NAME = URS	DESIGNED - MB	REVISED -
PLOT SCALE = 40.0000' / in.	DRAWN - KJB	REVISED -
PLOT DATE = 02/28/2014	CHECKED - SPF	REVISED -
	DATE - 02/28/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DRAINAGE AND UTILITY PROFILE			
FRONTENAC STREET			
SCALE: 20	SHEET NO. 1 OF 1 SHEETS	STA. 50+00 TO STA. 55+50	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A		383	109
CONTRACT NO. 60R06			ILLINOIS FED. AID PROJECT	



**PROPOSED IMPROVEMENTS**

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROPOSED WATER MAIN PLAN**

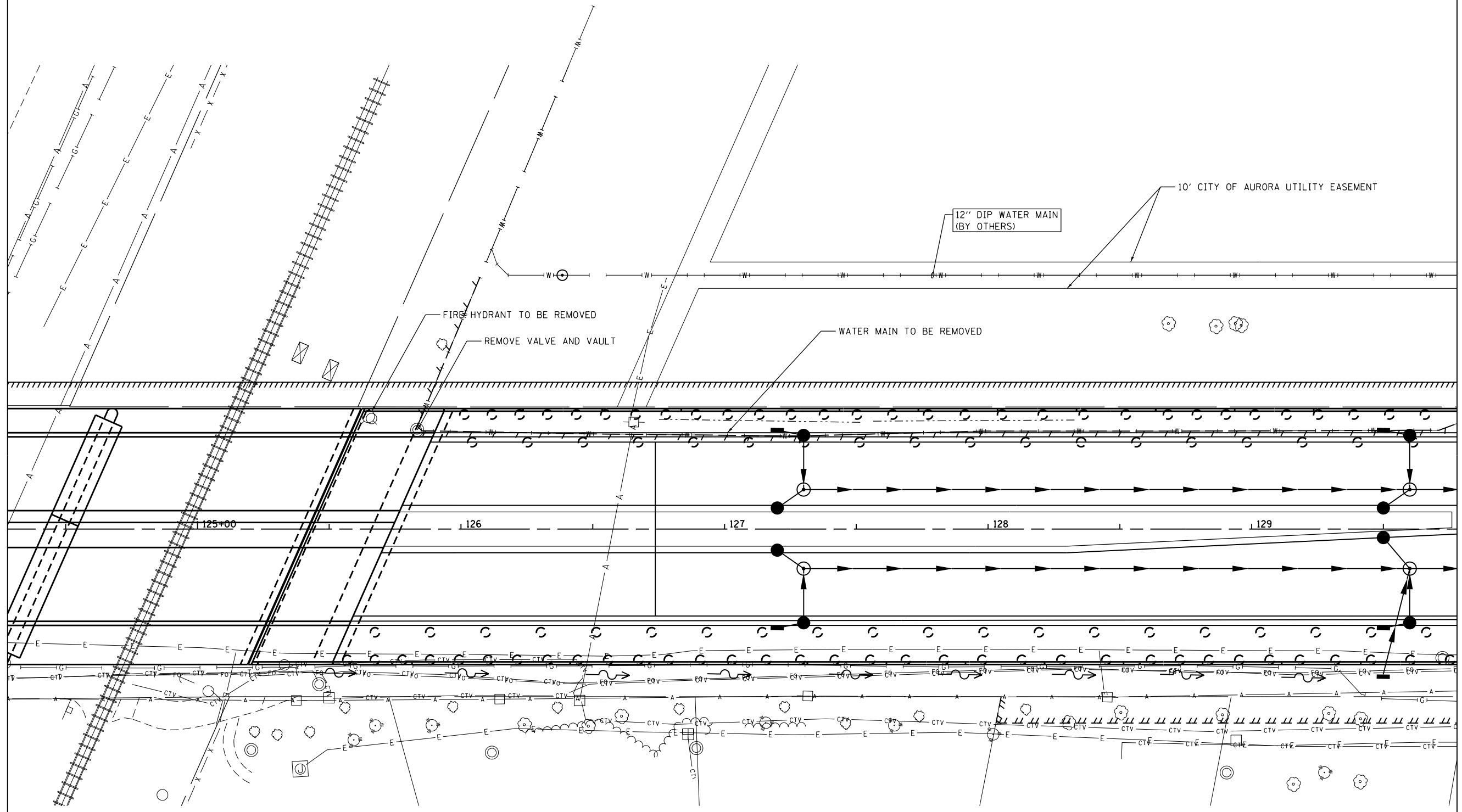
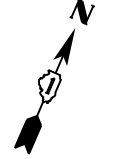
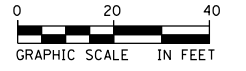
SCALE: 20 SHEET NO. 1 OF 7 SHEETS STA. TO STA.



USER NAME = URS	DESIGNED - MB	REVISED -
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PLOT SCALE = 40.0000' / in.	CHECKED - SPF	REVISED -
PLOT DATE = 02/28/2014	DATE - 02/28/2014	REVISED -

F.A.P. RTE. 311	SECTION	COUNTY DUPAGE	TOTAL SHEETS 383	SHEET NO. 110
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FILE NAME = Q:\Aurora\_351278\_US 34 Water Main Relocation\Roadway Drawings\Plan&Profile\Aurora\_WaterMain-sh1-Plan.dgn



MATCH LINE STA. 129 + 78

**PROPOSED IMPROVEMENTS**

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROPOSED WATER MAIN PLAN**

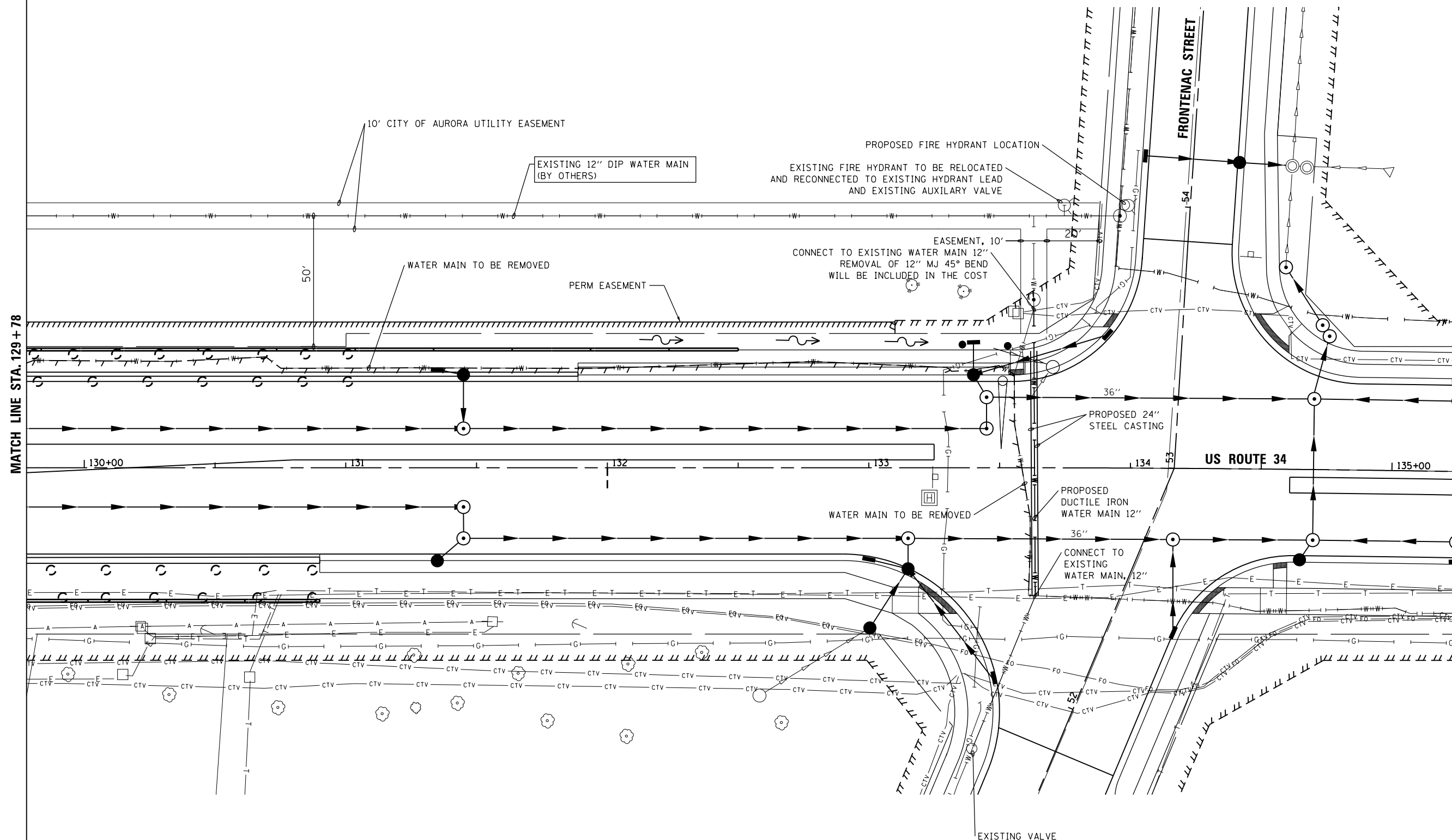
SCALE: 20 SHEET NO. 2 OF 7 SHEETS STA. TO STA.



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PLOT DATE = 02/28/2014	DATE - 02/28/2014	REVISED -

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311			383	111
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FILE NAME = Q:\Aurora\_351278\US 34 Water Main Relocation\Roadway\Drawings\Plan&Profile\Aurora\_WaterMain-sh1-Plan\_2.dgn



MATCH LINE STA. 129+78

**PROPOSED IMPROVEMENTS**

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROPOSED WATER MAIN PLAN**

SCALE: 20 SHEET NO. 3 OF 7 SHEETS STA. TO STA.

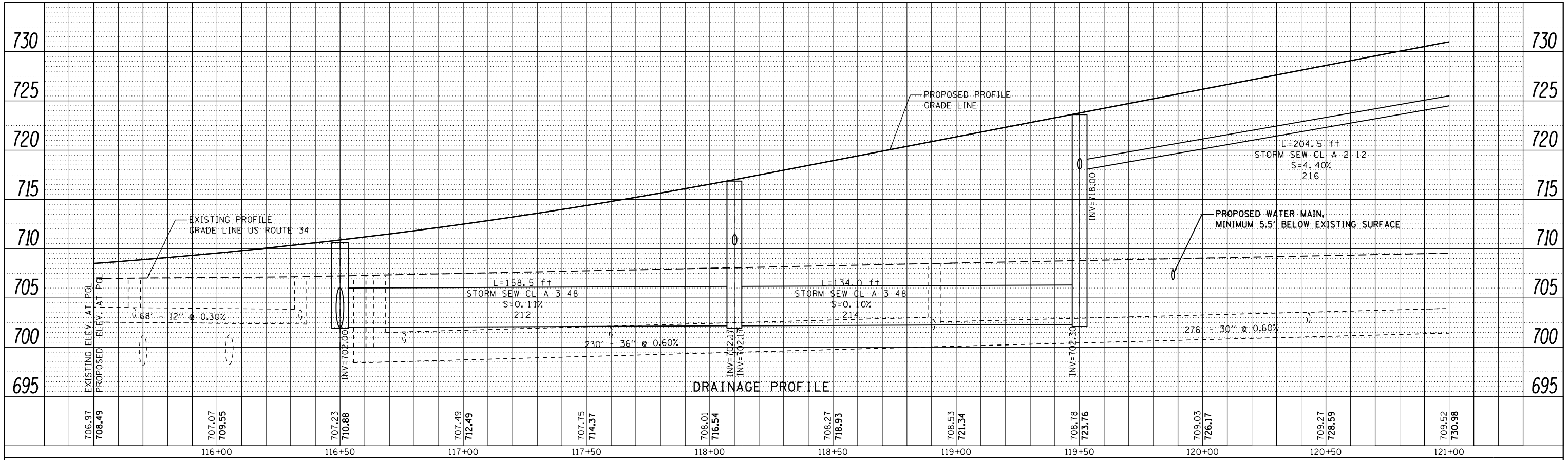


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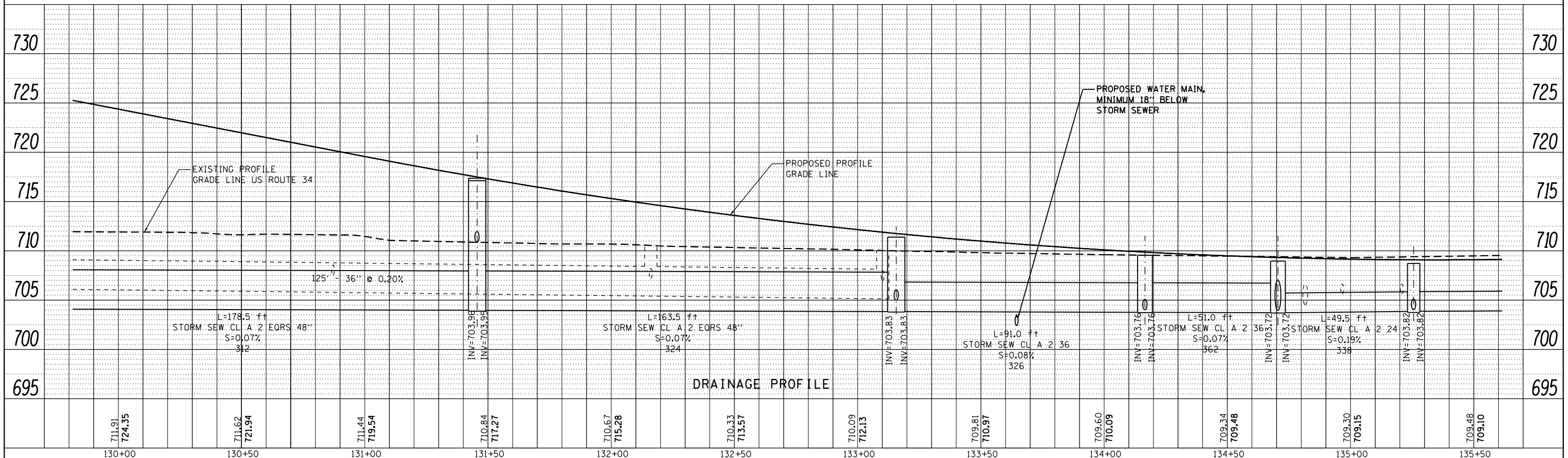
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311		DUPAGE	383	112
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FILE NAME = Q:\Aurora\351278\US 34 Water Main Relocation\Roadway\Drawings\Plan\Plan-3.dgn

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	NOTE BOOK		
	NO. _____		
	CARD FILE NAME		



PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	NOTE BOOK		
	NO. _____		
	CARD FILE NAME		



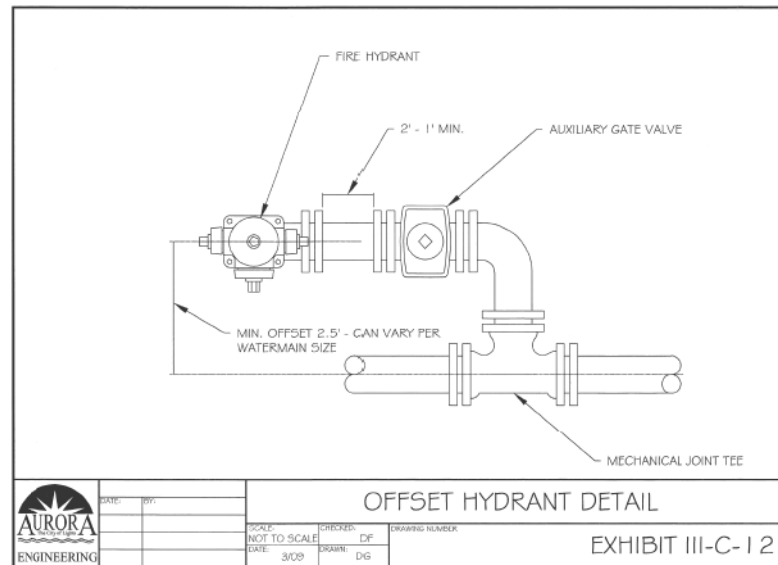
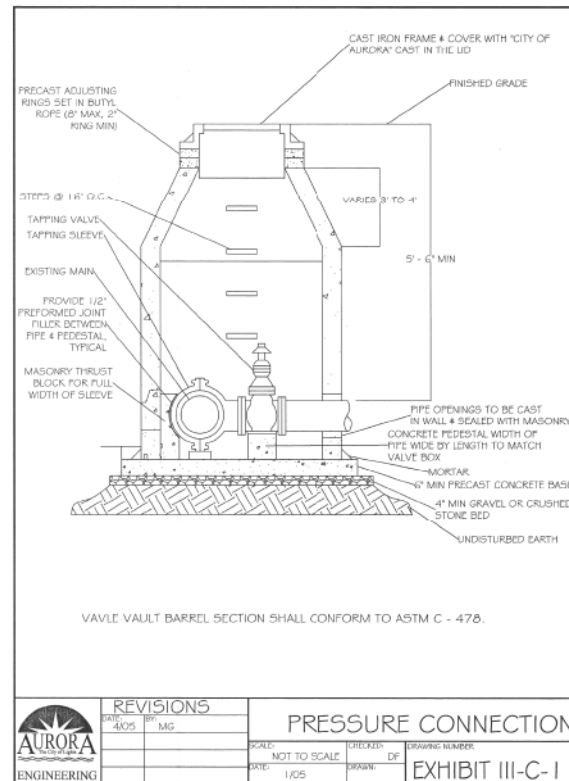
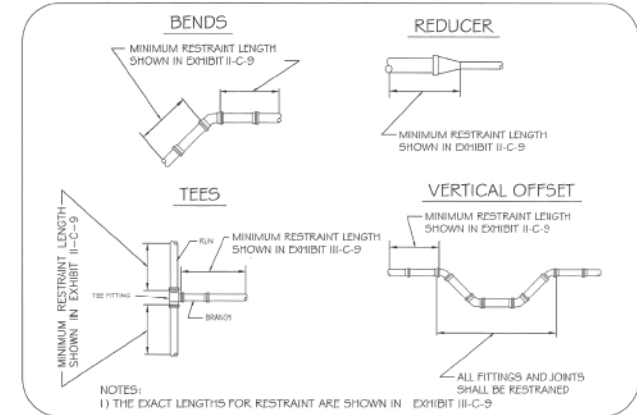
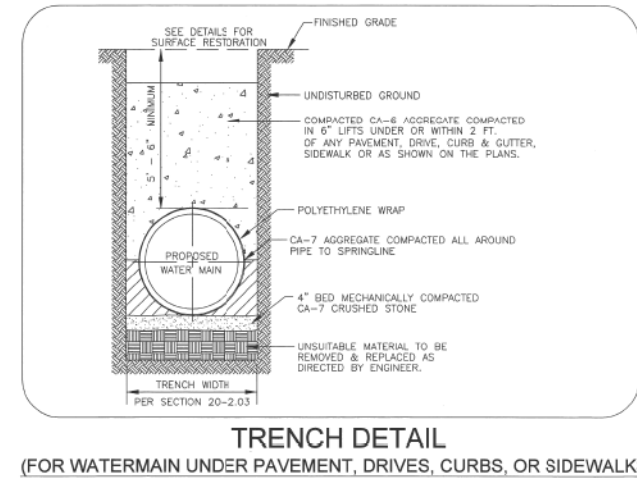
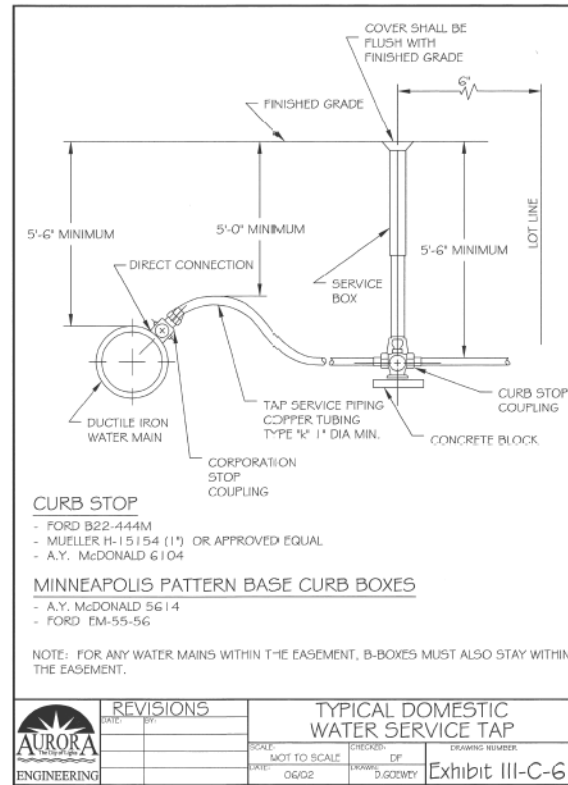
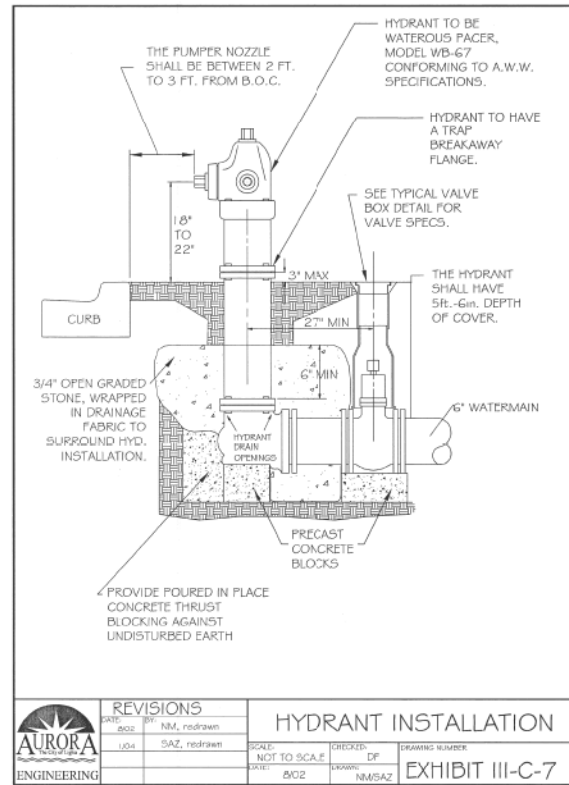
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	DATE - 02/28/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>PROPOSED WATER MAIN PROFILE</b>	
SCALE: 20	SHEET NO. 4 OF 7 SHEETS
STA. _____	TO STA. _____

F.A.P. RTE. 311	SECTION	COUNTY	TOTAL SHEETS 383	SHEET NO. 113
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

# WATER MAIN



**Minimum Restraint Length (ft) on both sides of the Fitting**

Fitting Type/Nominal Size	6"	8"	12"	16"
11 1/2" Bend	2	3	4	6
22 1/2" Bend	5	6	9	11
15" Bend	10	13	18	23
30" Bend	23	30	43	56
Dead End	31	40	57	74
Top Side of a Vertical Offset <sup>1</sup>	19	25	35	46
Tee Run x Branch <sup>2</sup>				
Tee Run x Branch <sup>2</sup>	6" BY	24		
Tee Run x Branch <sup>2</sup>	8" BY	22	34	
Tee Run x Branch <sup>2</sup>	12" BY	18	31	51
Tee Run x Branch <sup>2</sup>	16" BY	14	28	48
Reducer <sup>3</sup>	8" BY	17		
Reducer <sup>3</sup>	12" BY	42	30	
Reducer <sup>3</sup>	16" BY	62	54	31

<sup>1</sup> All joints within the lowered section of the watermain shall be restrained or shall be solid pipe. The above distances reflect the required restraint distance on the normally elevated watermain either side of the 45° fitting of the vertical offset (or lowering).

<sup>2</sup> Minimum of 2 ft of solid pipe is required on both sides of the fitting on the run side. Distance indicates the length of restraint on the branch side or the side perpendicular to the tee run watermain.

<sup>3</sup> Indicates the distance from the larger end of the reducer.

Notes:  
1) All runs and bells shall be shrouded steel.  
2) The brace restraint extends from the fitting to the minimum restraint distance must be supported by the City of Aurora prior to backfilling.

Watermain Restraint Length Table  
EXHIBIT III-C-9

**WATERMAIN RESTRAINT LENGTH TABLE**  
(EXHIBIT III-C-9)

FILE NAME = Q:\Aurora\351278\US 34 Water Main Relocation\Roadway Drawings\Plan&Profile\Aurora-WaterMain-shd-Details-1.dgn



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PLOT DATE = 02/28/2014	CHECKED - SPF	REVISED -
	DATE - 02/28/2014	REVISED -

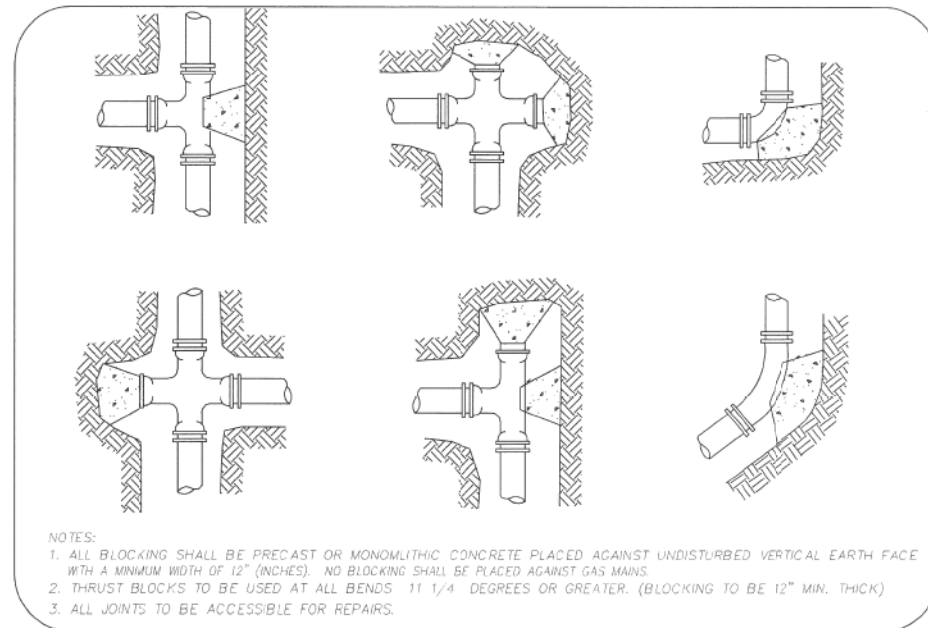
CITY OF AURORA

WATER MAIN PLAN DETAILS - 1

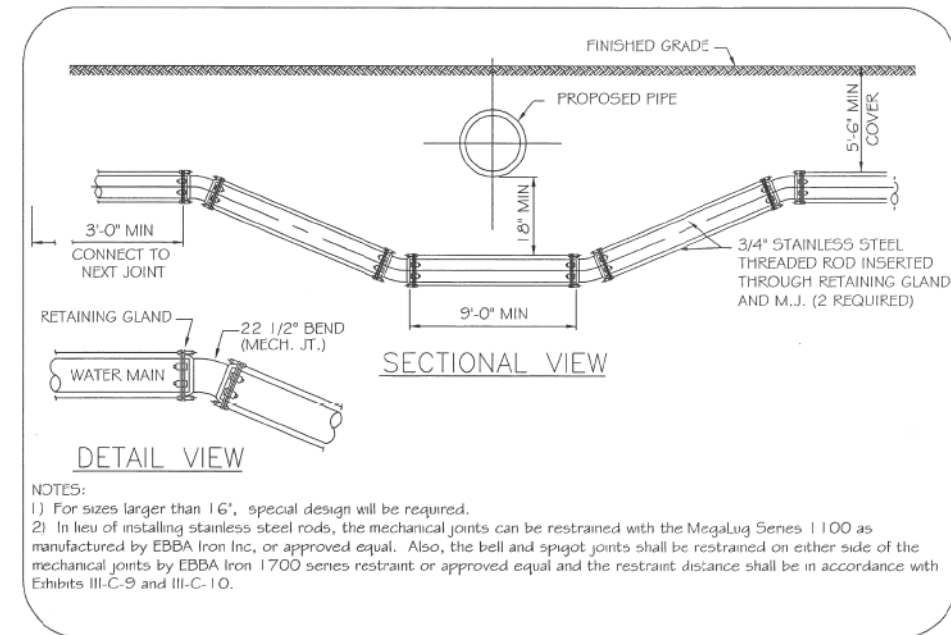
SCALE: 20 SHEET NO. 5 OF 7 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311		DUPAGE	383	114
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

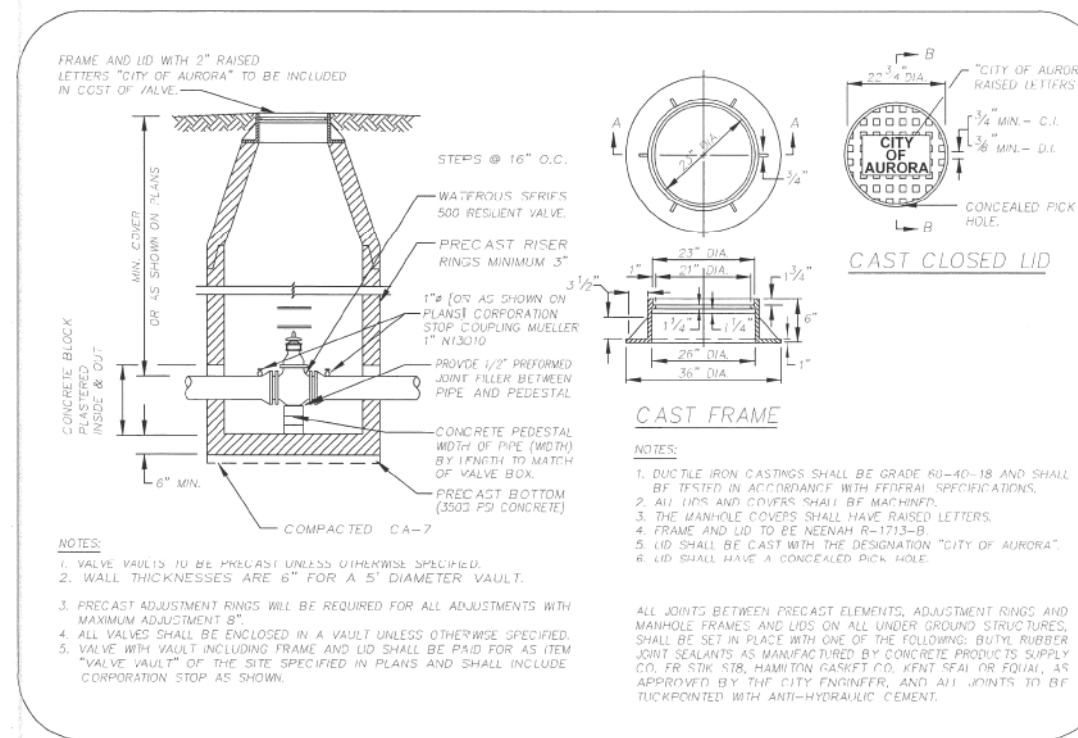
# WATER MAIN



**THRUST BLOCK DETAIL**



**WATERMAIN LOWERING DETAIL**



**VALVE VAULT AND LID DETAIL**

FILE NAME = Q:\Aurora\351278\US 34 Water Main Relocation\Roadway Drawings\Plan&Profile\Aurora WaterMain.sht, Details-2.dgn



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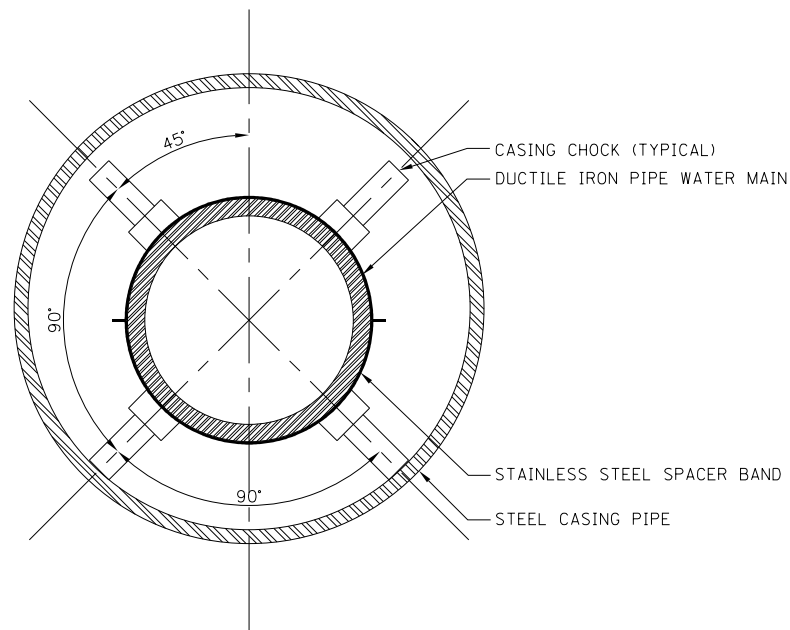
CITY OF AURORA

**WATER MAIN DETAILS - 2**

SCALE: 20 SHEET NO. 6 OF 7 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311		DUPAGE	383	115
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

# WATER MAIN



SECTION A-A

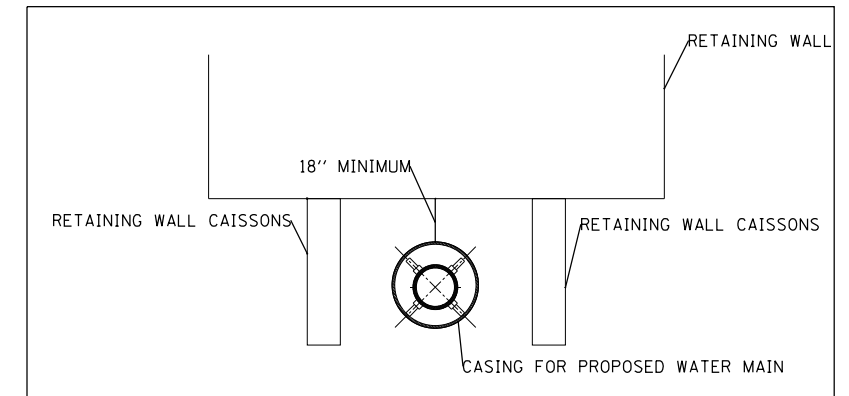
STEEL CASING PIPE SHALL MEET THE REQUIREMENTS OF ASTM A139 (ANSI B36.10); MINIMUM YIELD STRENGTH OF 35,000 P.S.I.; 0.344" MINIMUM WALL THICKNESS.

CASING PIPE SHALL BE WELDED IN ACCORDANCE WITH THE REQUIREMENTS OF AMERICAN WELDING SOCIETY (AWS) STANDARD D1.1.

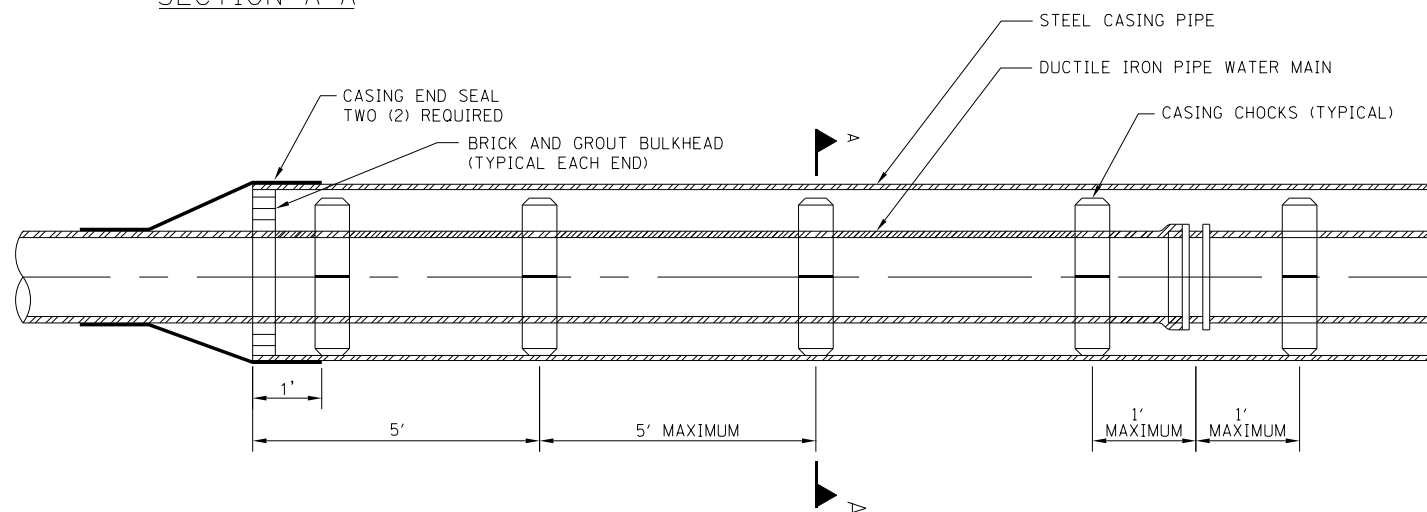
CASING PIPE SHALL BE BITUMINOUS COATED INSIDE AND OUTSIDE IN ACCORDANCE WITH THE REQUIREMENTS OF AASHTO M190.

CASING CHOCKS SHALL BE STAINLESS STEEL, RESTRAINED CHOCKS

CASING END SEALS SHALL BE SYNTHETIC RUBBER WITH TWO (2) STAINLESS STEEL BANDS AS MANUFACTURED BY CASCADE WATERWORKS MANUFACTURING CO., OR PIPELINE SEAL AND INSULATOR INC. END SEALS SHALL BE BONDED TO CASING PIPE AND DUCTILE IRON PIPE WATER MAIN WITH A BONDING CEMENT SUPPLIED BY THE END SEAL MANUFACTURER.



WATER MAIN SECTION AT RETAINING WALL



ELEVATION

TYPICAL CASING DETAIL

N.T.S.

FILE NAME = Q:\Aurora\351278\US 34 Water Main Relocation\Roadway\Drawings\Plan&Profile\Aurora WaterMain.sht.Details-3-Ldgn



USER NAME = URS	DESIGNED - MB	REVISED -
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PLOT SCALE = 40.0000' / in.	CHECKED - SPF	REVISED -
PLOT DATE = 02/28/2014	DATE - 02/28/2014	REVISED -

CITY OF AURORA

WATER MAIN DETAILS - 3

SCALE: 20 SHEET NO. 7 OF 7 SHEETS STA. TO STA.

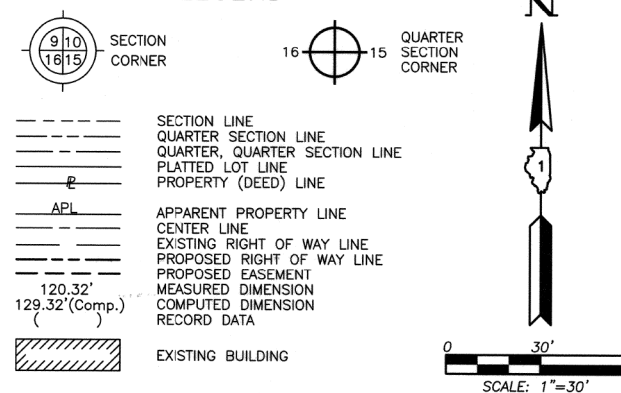
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311		DUPAGE	383	116
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



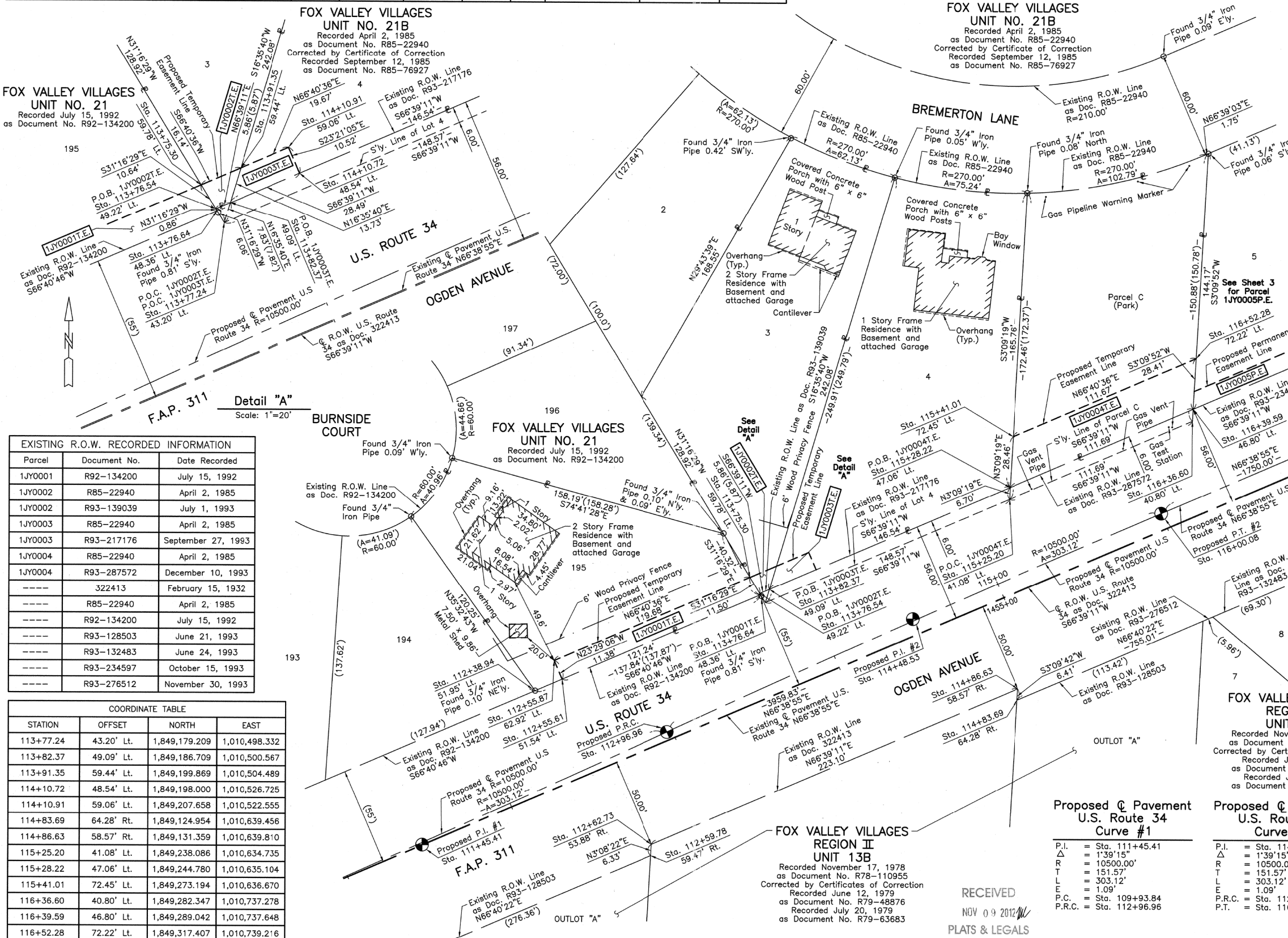
PART OF THE SW 1/4 OF SEC. 29, TWP. 38 N., R. 9 E. OF THE 3RD. P.M., IN DuPAGE COUNTY, ILLINOIS.

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT ACRES	SQUARE AREA FEET	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
1JY0001T.E.	Dean Edward Bisconti	0.320	N/A	N/A	0.320	0.031	N/A	Grading	07-29-314-044	
1JY0002T.E.	Jeffrey E. Easley and Shannon G. Easley, husband and wife, as Tenants by the Entirety	0.404	N/A	N/A	0.404	0.003	116	Grading	07-29-324-012	
1JY0003T.E.	Neal C. Adamski and Vicki J. Adamski, as tenants by the entirety	0.452	N/A	N/A	0.452	0.006	253	Grading	07-29-324-017	
1JY0004T.E.	Fox Valley Park District	0.348	N/A	N/A	0.348	0.065	N/A	Grading	07-29-324-018	

LEGEND



- Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.
- IRON PIPE OR ROD FOUND
  - ⊙ "MAG" NAIL SET
  - + CUT CROSS FOUND OR SET
  - 5/8" REBAR SET
  - T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - T2
  - T3
  - BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - BT2
  - BT3
  - STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
  - M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - ⊙ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
  - RIGHT OF WAY STAKING PROPOSED TO BE SET.



EXISTING R.O.W. RECORDED INFORMATION

Parcel	Document No.	Date Recorded
1JY0001	R92-134200	July 15, 1992
1JY0002	R85-22940	April 2, 1985
1JY0002	R93-139039	July 1, 1993
1JY0003	R85-22940	April 2, 1985
1JY0003	R93-217176	September 27, 1993
1JY0004	R85-22940	April 2, 1985
1JY0004	R93-287572	December 10, 1993
----	322413	February 15, 1932
----	R85-22940	April 2, 1985
----	R92-134200	July 15, 1992
----	R93-128503	June 21, 1993
----	R93-132483	June 24, 1993
----	R93-234597	October 15, 1993
----	R93-276512	November 30, 1993

COORDINATE TABLE

STATION	OFFSET	NORTH	EAST
113+77.24	43.20' Lt.	1,849,179.209	1,010,498.332
113+82.37	49.09' Lt.	1,849,186.709	1,010,500.567
113+91.35	59.44' Lt.	1,849,199.869	1,010,504.489
114+10.72	48.54' Lt.	1,849,198.000	1,010,526.725
114+10.91	59.06' Lt.	1,849,207.658	1,010,522.555
114+83.69	64.28' Rt.	1,849,124.954	1,010,639.456
114+86.63	58.57' Rt.	1,849,131.359	1,010,639.810
115+25.20	41.08' Lt.	1,849,238.086	1,010,634.735
115+28.22	47.06' Lt.	1,849,244.780	1,010,635.104
115+41.01	72.45' Lt.	1,849,273.194	1,010,636.670
116+36.60	40.80' Lt.	1,849,282.347	1,010,737.278
116+39.59	46.80' Lt.	1,849,289.042	1,010,737.648
116+52.28	72.22' Lt.	1,849,317.407	1,010,739.216

STATE OF ILLINOIS }  
 COUNTY OF LAKE } SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 29, TOWNSHIP 38N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, DUPAGE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS 28th DAY OF June 2012 A.D.

*Christian H. Jorgensen* PRESIDENT  
 2797 PROFESSIONAL LAND SURVEYOR NO. 35-2797  
 LICENSE EXPIRATION DATE: NOVEMBER 30, 2012  
 THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.  
 Note: Surface Coordinates are Shown.

COORDINATE TABLE

STATION	OFFSET	NORTH	EAST
112+38.94	51.95' Lt.	1,849,129.081	1,010,369.058
112+55.61	51.54' Lt.	1,849,135.651	1,010,384.299
112+55.87	62.92' Lt.	1,849,146.093	1,010,379.762
112+59.78	59.47' Rt.	1,849,036.626	1,010,434.628
112+62.73	53.88' Rt.	1,849,042.944	1,010,434.975
113+75.30	59.78' Lt.	1,849,193.478	1,010,489.665
113+76.54	49.22' Lt.	1,849,184.386	1,010,495.187
113+76.64	48.36' Lt.	1,849,183.648	1,010,495.636

JORGENSEN & ASSOCIATES, INC.  
 120 PARK AVENUE  
 LAKE VILLA, ILLINOIS 60046  
 (847) 356-3371

SHEET 1 IS A COVER SHEET AND IS NOT RECORDED.

**PLAT OF HIGHWAYS**  
 STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 F.A.P. 311 (U.S. ROUTE 34)

SECTION 652-A DuPAGE COUNTY  
 PROJECT JOB NO. R-91-014-11  
 STATION 111+00 TO STATION 117+00  
 SCALE: 1"=30' SHEET 2 OF 18

BUREAU OF LAND ACQUISITION  
 201 WEST CENTER COURT  
 SCHAMBURG, ILLINOIS 60196

SHEET 117 OF 383

PART OF THE SW 1/4 OF SEC. 29, TWP. 38 N., R. 9 E. OF THE 3RD. P.M., IN DuPAGE COUNTY, ILLINOIS.

Schedule of Ties		
Point Number	Tie to point	Tie Distance (feet)
1	T1	25.80
	T2	25.50
	T3	30.10

**FOX VALLEY VILLAGES  
UNIT NO. 21B**  
Recorded April 2, 1985  
as Document No. R85-22940  
Corrected by Certificate of Correction  
Recorded September 12, 1985  
as Document No. R85-76927



EXISTING R.O.W. RECORDED INFORMATION		
Parcel	Document No.	Date Recorded
1JY0005	R85-22940	April 2, 1985
1JY0005	R93-234597	October 15, 1993
1JY0006	R85-22940	April 2, 1985
1JY0006	R93-128501	June 21, 1993
1JY0007	R85-22940	April 2, 1985
1JY0007	R93-234599	October 15, 1993
1JY0008	R85-22940	April 2, 1985
1JY0008	R93-217178	September 27, 1993
1JY0009	R85-22940	April 2, 1985
1JY0009	93 ED 101	*March 7, 1994
---	322413	February 15, 1932
---	R85-22940	April 2, 1985
---	R93-132483	June 24, 1993
---	R93-132486	June 24, 1993
---	R93-132488	June 24, 1993
---	R93-150802	July 14, 1993
---	R93-192270	August 27, 1993
---	R93-192273	August 27, 1993
---	R93-217174	September 27, 1993
---	93 ED 287572	December 10, 1993
---	93 ED 70	*January 18, 1994
---	93 ED 53	*February 9, 1994
---	93 ED 113	*March 14, 1994

**LEGEND**

SECTION CORNER 16 15  
QUARTER SECTION CORNER

SECTION LINE  
QUARTER SECTION LINE  
QUARTER, QUARTER SECTION LINE  
PLATTED LOT LINE  
PROPERTY (DEED) LINE

APL  
APPARENT PROPERTY LINE  
CENTER LINE  
EXISTING RIGHT OF WAY LINE  
PROPOSED RIGHT OF WAY LINE  
PROPOSED EASEMENT  
MEASURED DIMENSION  
COMPUTED DIMENSION  
RECORD DATA

120.32'  
129.32'(Comp.)

EXISTING BUILDING

0 20' 40'  
SCALE: 1"=20'

Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.

○ IRON PIPE OR ROD FOUND      ⊕ "MAG" NAIL SET  
+ CUT CROSS FOUND OR SET      ● 5/8" REBAR SET

● T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.  
● T2  
● T3

● BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.  
● BT2  
● BT3

■ STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

■ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

⊕ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)

□ RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }  
COUNTY OF LAKE }SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 29, TOWNSHIP 38N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, DuPAGE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RE-TRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS 19th DAY OF October 2012 A.D.

*Christian H. Jorgensen* PRESIDENT  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797  
LICENSE EXPIRATION DATE: NOVEMBER 30, 2012  
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.  
Note: Surface Coordinates are Shown.



COORDINATE TABLE				
STATION	OFFSET	NORTH	EAST	
116+36.60	40.80' Lt.	1,849,282.347	1,010,737.278	
118+02.06	55.75' Lt.	1,849,361.652	1,010,883.260	
116+39.59	46.80' Lt.	1,849,289.042	1,010,737.648	
116+44.06	55.75' Lt.	1,849,299.024	1,010,738.200	
117+44.89	40.80' Lt.	1,849,325.262	1,010,836.699	
117+44.89	46.80' Lt.	1,849,330.774	1,010,834.329	
117+44.91	55.75' Lt.	1,849,339.000	1,010,830.792	
118+02.04	40.79' Lt.	1,849,347.909	1,010,889.169	
118+02.04	46.79' Lt.	1,849,353.422	1,010,886.799	
119+16.36	55.75' Lt.	1,849,406.956	1,010,988.197	
119+16.36	40.78' Lt.	1,849,393.205	1,010,994.108	
119+16.34	46.78' Lt.	1,849,398.718	1,010,991.739	
119+16.36	55.75' Lt.	1,849,406.956	1,010,988.197	
119+98.04	55.75' Lt.	1,849,439.332	1,011,063.187	
120+00.01	46.78' Lt.	1,849,431.875	1,011,068.557	
120+01.33	40.78' Lt.	1,849,426.890	1,011,072.147	

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
1JY0005P.E.	Frederick D. Johnson and Christine S. Clay-Johnson, his wife, as joint tenants	0.217	N/A	N/A	0.217	0.021	Highway Purposes	07-29-324-014	
1JY0006P.E.	Lenie Lim Cash and Gregory Cash, as Tenants by the Entirety	0.169	N/A	N/A	0.169	0.012	Highway Purposes	07-29-324-013	
1JY0007P.E.	Ken A. Lubang and Nelia A. Lubang, husband and wife, as Tenants by the Entirety	0.169	N/A	N/A	0.169	0.012	Highway Purposes	07-29-324-015	
1JY0008P.E.	Waseem Iqbal	0.169	N/A	N/A	0.169	0.012	Highway Purposes	07-29-324-016	
1JY0009P.E.	Ricardo D. Ramirez and Erica R. Ramirez, as Tenants by the Entirety	0.210	N/A	N/A	0.210	0.017	Highway Purposes	07-29-324-019	

**PLAT OF HIGHWAYS  
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
F.A.P. 311 (U.S. ROUTE 34)**

SECTION 652-A DuPAGE COUNTY  
PROJECT JOB NO. R-91-014-11  
STATION 116+00 TO STATION 121+00  
SCALE: 1"=20' SHEET 3 OF 18

**BUREAU OF LAND ACQUISITION**  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196

SHEET 118 OF 383

PART OF THE SOUTH 1/2 OF SEC. 29, TWP. 38 N., R. 9 E. OF THE 3RD. P.M., IN DuPAGE COUNTY, ILLINOIS.

EXISTING R.O.W. RECORDED INFORMATION		
Parcel	Document No.	Date Recorded
1JY0010	R85-22940	April 2, 1985
1JY0010	93 ED 53	*February 9, 1994
1JY0011	R85-22940	April 2, 1985
1JY0011	93 ED 71	*January 20, 1994

\* Date Filed

**FOX VALLEY VILLAGES  
UNIT NO. 21B**  
Recorded April 2, 1985  
as Document No. R85-22940  
Corrected by Certificate of Correction  
Recorded September 12, 1985  
as Document No. R85-76927

**BLAINE COURT EAST**

**FOX VALLEY VILLAGES  
REGION II  
UNIT 13C**  
Recorded November 17, 1978  
as Document No. R78-110956  
Corrected by Certificates of Correction  
Recorded June 12, 1979  
as Document No. R79-48877  
Recorded July 20, 1979  
as Document No. R79-63682

EXISTING R.O.W. RECORDED INFORMATION		
Parcel	Document No.	Date Recorded
1JY0012	R85-22940	April 2, 1985
1JY0012	R93-287576	December 10, 1993
1JY0013	R85-22940	April 2, 1985
1JY0013	R93-287578	December 10, 1993
1JY0014	R85-22940	April 2, 1985
---	217098	July 6, 1926
---	231201	March 9, 1927
---	322413	February 15, 1932
---	R85-22940	April 2, 1985
---	R93-150805	July 14, 1993
---	R93-192273	August 27, 1993
---	R93-208811	September 16, 1993
---	R93-217174	September 27, 1993
---	R93-287568	December 10, 1993
---	R94-019785	January 21, 1994
---	93 ED 101	*March 7, 1994

\* Date Filed

**LEGEND**

SECTION CORNER 910 1615  
QUARTER SECTION CORNER 16 15

SECTION LINE  
QUARTER SECTION LINE  
QUARTER, QUARTER SECTION LINE  
PLATTED LOT LINE  
PROPERTY (DEED) LINE

APL  
APPARENT PROPERTY LINE  
CENTER LINE  
EXISTING RIGHT OF WAY LINE  
PROPOSED RIGHT OF WAY LINE  
PROPOSED EASEMENT  
MEASURED DIMENSION  
COMPUTED DIMENSION  
RECORD DATA

EXISTING BUILDING

Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.

IRON PIPE OR ROD FOUND @ "MAG" NAIL SET  
CUT CROSS FOUND OR SET • 5/8" REBAR SET

T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.  
T2  
T3

BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.  
BT2  
BT3

STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)

RIGHT OF WAY STAKING PROPOSED TO BE SET.

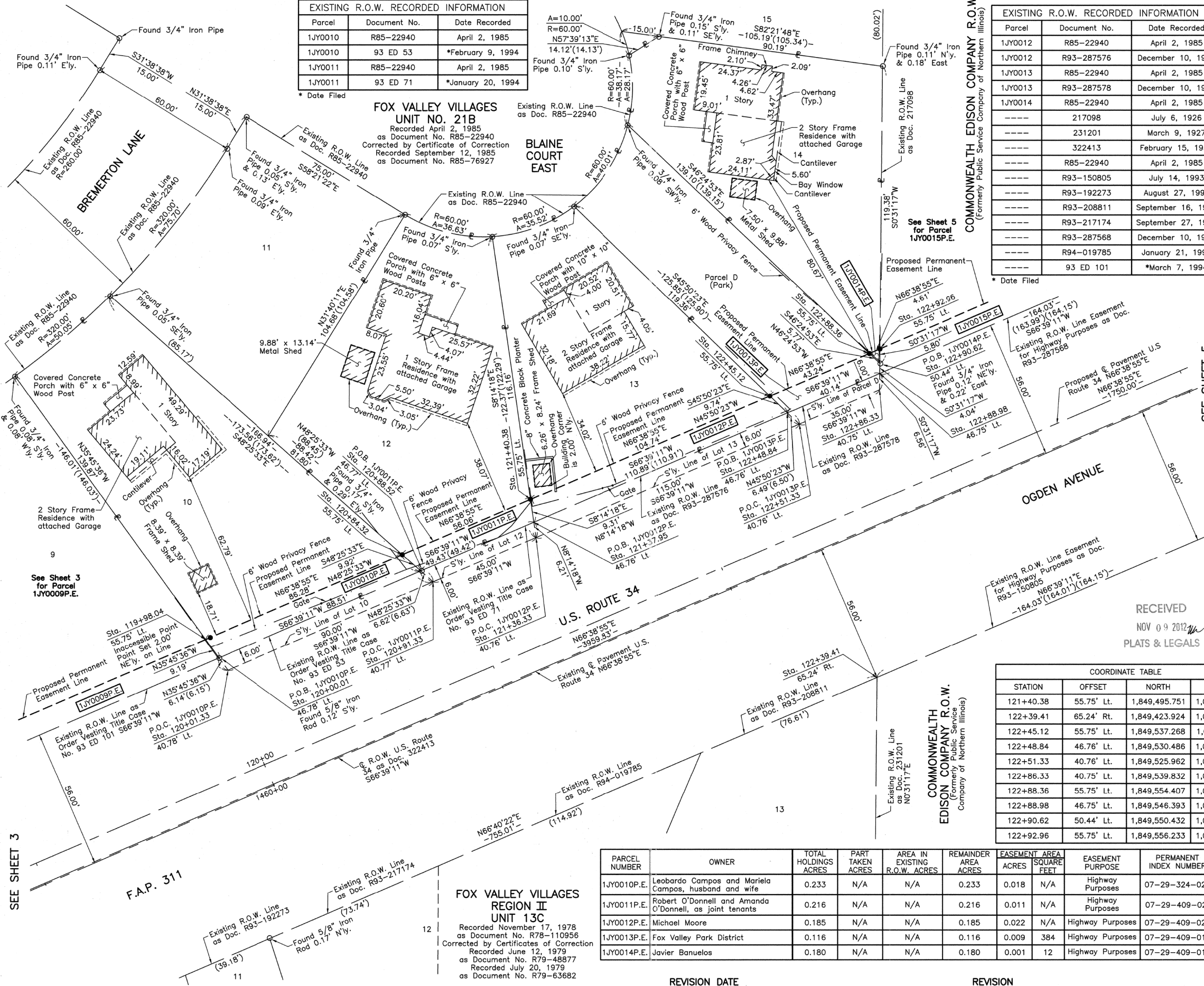
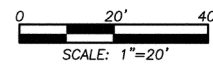
STATE OF ILLINOIS } SS  
COUNTY OF LAKE } SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 29, TOWNSHIP 38N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, DuPAGE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RE-TRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS 19th DAY OF October 20, 2012 A.D.

RECEIVED  
NOV 09 2012  
PLATS & LEGALS

CHRISTIAN H. JORGENSEN  
2797 PROFESSIONAL LAND SURVEYOR STATE OF ILLINOIS  
LAKE VILLA, ILLINOIS



SEE SHEET 5

SEE SHEET 3

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
119+98.04	55.75' Lt.	1,849,439.332	1,011,063.187
120+00.01	46.78' Lt.	1,849,431.875	1,011,068.557
120+01.33	40.78' Lt.	1,849,426.890	1,011,072.147
120+84.32	55.75' Lt.	1,849,473.532	1,011,142.402
120+88.52	46.77' Lt.	1,849,466.952	1,011,149.821
120+91.33	40.77' Lt.	1,849,462.556	1,011,154.777
121+36.33	40.76' Lt.	1,849,480.389	1,011,196.091
121+37.95	46.76' Lt.	1,849,486.540	1,011,195.201
121+40.38	55.75' Lt.	1,849,495.751	1,011,193.867
122+39.41	65.24' Rt.	1,849,423.924	1,011,332.752
122+45.12	55.75' Lt.	1,849,537.268	1,011,290.031
122+48.84	46.76' Lt.	1,849,530.486	1,011,297.014
122+51.33	40.76' Lt.	1,849,525.962	1,011,301.673
122+86.33	40.75' Lt.	1,849,539.832	1,011,333.807
122+88.36	55.75' Lt.	1,849,554.407	1,011,329.727
122+88.98	46.75' Lt.	1,849,546.393	1,011,333.867
122+90.62	50.44' Lt.	1,849,550.432	1,011,333.903
122+92.96	55.75' Lt.	1,849,556.233	1,011,333.956

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT ACRES	AREA SQUARE FEET	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
1JY0010P.E.	Leobardo Campos and Mariela Campos, husband and wife	0.233	N/A	N/A	0.233	0.018	N/A	Highway Purposes	07-29-324-020	
1JY0011P.E.	Robert O'Donnell and Amanda O'Donnell, as joint tenants	0.216	N/A	N/A	0.216	0.011	N/A	Highway Purposes	07-29-409-021	
1JY0012P.E.	Michael Moore	0.185	N/A	N/A	0.185	0.022	N/A	Highway Purposes	07-29-409-020	
1JY0013P.E.	Fox Valley Park District	0.116	N/A	N/A	0.116	0.009	384	Highway Purposes	07-29-409-019	
1JY0014P.E.	Javier Banuelos	0.180	N/A	N/A	0.180	0.001	12	Highway Purposes	07-29-409-018	

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
119+98.04	55.75' Lt.	1,849,439.332	1,011,063.187
120+00.01	46.78' Lt.	1,849,431.875	1,011,068.557
120+01.33	40.78' Lt.	1,849,426.890	1,011,072.147
120+84.32	55.75' Lt.	1,849,473.532	1,011,142.402
120+88.52	46.77' Lt.	1,849,466.952	1,011,149.821
120+91.33	40.77' Lt.	1,849,462.556	1,011,154.777
121+36.33	40.76' Lt.	1,849,480.389	1,011,196.091
121+37.95	46.76' Lt.	1,849,486.540	1,011,195.201

JORGENSEN & ASSOCIATES, INC.  
120 PARK AVENUE  
LAKE VILLA, ILLINOIS 60046  
(847) 356-3371

**PLAT OF HIGHWAYS  
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
F.A.P. 311 (U.S. ROUTE 34)**

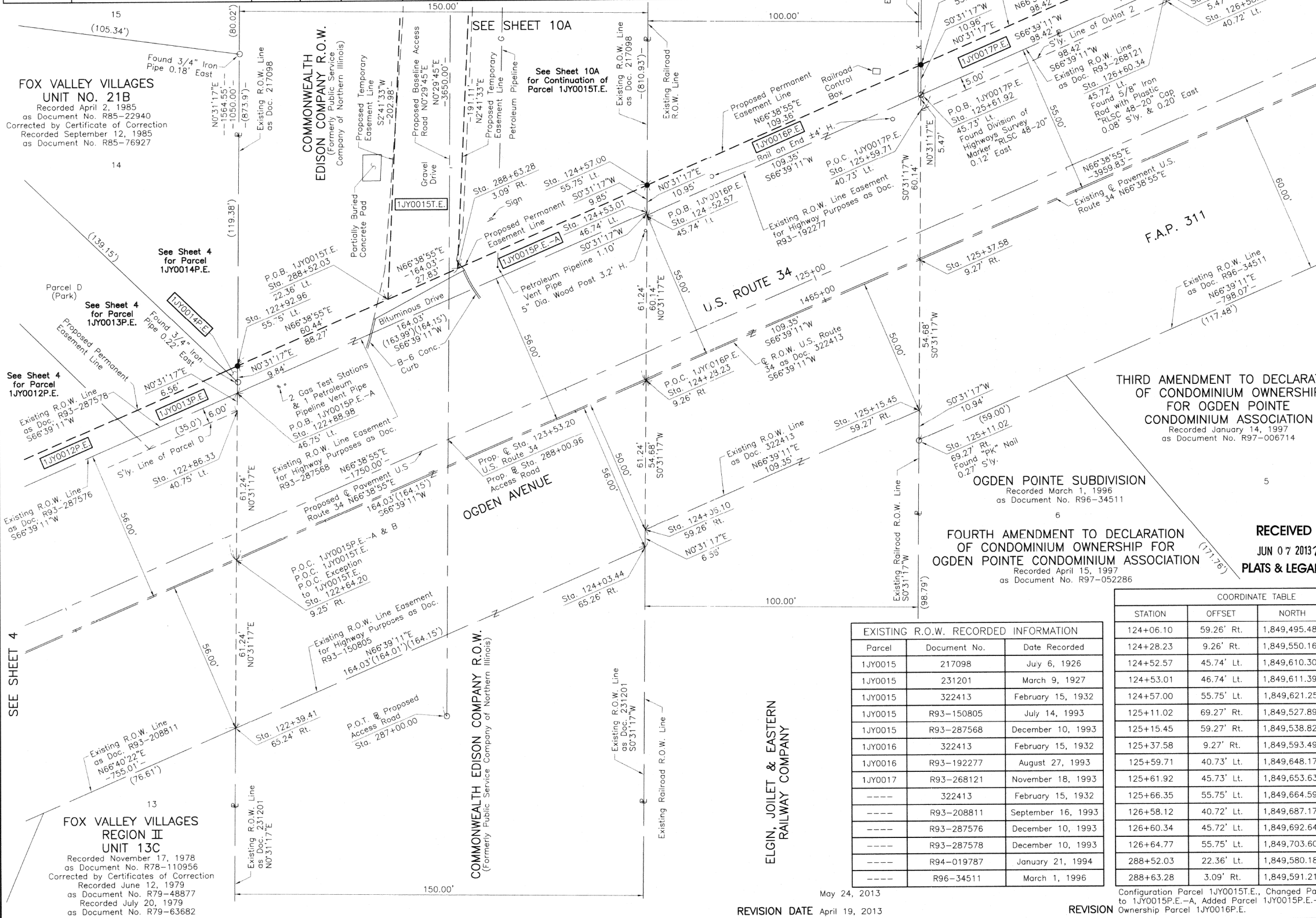
SECTION 652-A DuPAGE COUNTY  
PROJECT JOB NO. R-91-014-11  
STATION 119+00 TO STATION 124+00  
SCALE: 1"=20' SHEET 4 OF 18

**BUREAU OF LAND ACQUISITION  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196**

SHEET 119 OF 383

PART OF THE SE 1/4 OF SEC. 29, TWP. 38 N., R. 9 E. OF THE 3RD. P.M., IN DuPAGE COUNTY, ILLINOIS.

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
1JY0015P.E.-A 1JY0015P.E.-B 1JY0015T.E.	Commonwealth Edison Company formerly known as Public Service Company of Northern Illinois	N/A	N/A	N/A	N/A	P.E.-A=0.034 P.E.-B=1.032 T.E.=3.216	Highway Purposes Access & Bridge Maintenance Construction Purposes	07-20-413-0C2 07-29-200-0C2 07-29-200-0C4 07-29-200-0C6 07-29-400-0C2 07-29-402-0C2	
1JY0016P.E.	Wisconsin Central Ltd.	N/A	N/A	N/A	N/A	0.025	Highway Purposes	07-29-506-0C2	
1JY0017P.E. 1JY0017T.E.	Fox Valley Park District	22.608	N/A	N/A	22.608	P.E.=0.023 T.E.=1.116	Highway Purposes Construction Purposes	07-29-203-0C1 07-29-203-0C2 07-29-203-0C3 07-29-203-0C4 07-29-203-0C5 07-29-203-1C4	



**FOX VALLEY VILLAGES UNIT NO. 21B**  
Recorded April 2, 1985  
as Document No. R85-22940  
Corrected by Certificate of Correction  
Recorded September 12, 1985  
as Document No. R85-76927

**See Sheet 4 for Parcel 1JY0014P.E.**  
**See Sheet 4 for Parcel 1JY0013P.E.**  
**See Sheet 4 for Parcel 1JY0012P.E.**

**See Sheet 4 for Parcel 1JY0012P.E.**

**FOX VALLEY VILLAGES REGION II UNIT 13C**  
Recorded November 17, 1978  
as Document No. R78-110956  
Corrected by Certificates of Correction  
Recorded June 12, 1979  
as Document No. R79-48877  
Recorded July 20, 1979  
as Document No. R79-63682

**SEE SHEET 12 WILLOW SHORES**  
Recorded August 14, 1979  
as Document No. R79-72361  
Outlot 2  
**See Sheets 13 & 14 for Total Holdings Parcel 1JY0017P.E. & 1JY0017T.E.**  
**See Sheet 12 for Parcel 1JY0017T.E.**

**VILLAGE BAPTIST CHURCH RESUBDIVISION**  
Recorded May 31, 1990  
as Document No. R90-065512  
LOT 1  
**See Sheet 6 for Parcel 1JY0018P.E. & 1JY0018T.E.**

**LEGEND**

SECTION LINE  
QUARTER SECTION LINE  
QUARTER, QUARTER SECTION LINE  
PLATTED LOT LINE  
PROPERTY (DEED) LINE

APL  
APPARENT PROPERTY LINE  
CENTER LINE  
EXISTING RIGHT OF WAY LINE  
PROPOSED RIGHT OF WAY LINE  
PROPOSED EASEMENT  
MEASURED DIMENSION  
COMPUTED DIMENSION  
RECORD DATA

EXISTING BUILDING

Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.

○ IRON PIPE OR ROD FOUND      ⊕ "MAG" NAIL SET  
+ CUT CROSS FOUND OR SET      ● 5/8" REBAR SET

● T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION.  
● T2 IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.  
● BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.  
● BT2  
● BT3

■ STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.  
■ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

● PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)  
□ RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS } SS  
COUNTY OF LAKE

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 29, TOWNSHIP 38N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, DuPAGE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.  
DATED AT LAKE VILLA, ILLINOIS THIS 19TH DAY OF October 20 2012 A.D.

**RECEIVED**  
JUN 07 2013  
PLATS & LEGALS

**CHRISTIAN H. JORGENSEN**  
2797 PROFESSIONAL LAND SURVEYOR STATE OF ILLINOIS  
LAKE VILLA, ILLINOIS

**OGDEN AVENUE**  
U.S. ROUTE 34  
F.A.P. 311  
OGDEN POINTE SUBDIVISION  
Recorded March 1, 1996  
as Document No. R96-34511  
6  
**FOURTH AMENDMENT TO DECLARATION OF CONDOMINIUM OWNERSHIP FOR OGDEN POINTE CONDOMINIUM ASSOCIATION**  
Recorded April 15, 1997  
as Document No. R97-052286

**EXISTING R.O.W. RECORDED INFORMATION**

Parcel	Document No.	Date Recorded
1JY0015	217098	July 6, 1926
1JY0015	231201	March 9, 1927
1JY0015	322413	February 15, 1932
1JY0015	R93-150805	July 14, 1993
1JY0015	R93-287568	December 10, 1993
1JY0016	322413	February 15, 1932
1JY0016	R93-192277	August 27, 1993
1JY0017	R93-268121	November 18, 1993
----	322413	February 15, 1932
----	R93-208811	September 16, 1993
----	R93-287576	December 10, 1993
----	R93-287578	December 10, 1993
----	R94-019787	January 21, 1994
----	R96-34511	March 1, 1996

**COORDINATE TABLE**

STATION	OFFSET	NORTH	EAST
124+06.10	59.26' Rt.	1,849,495.489	1,011,483.410
124+28.23	9.26' Rt.	1,849,550.162	1,011,483.907
124+52.57	45.74' Lt.	1,849,610.303	1,011,484.454
124+53.01	46.74' Lt.	1,849,611.397	1,011,484.464
124+57.00	55.75' Lt.	1,849,621.251	1,011,484.554
125+11.02	69.27' Rt.	1,849,527.890	1,011,583.709
125+15.45	59.27' Rt.	1,849,538.824	1,011,583.808
125+37.58	9.27' Rt.	1,849,593.498	1,011,584.306
125+59.71	40.73' Lt.	1,849,648.172	1,011,584.803
125+61.92	45.73' Lt.	1,849,653.639	1,011,584.853
125+66.35	55.75' Lt.	1,849,664.596	1,011,584.953
126+58.12	40.72' Lt.	1,849,687.174	1,011,675.162
126+60.34	45.72' Lt.	1,849,692.642	1,011,675.212
126+64.77	55.75' Lt.	1,849,703.607	1,011,675.311
288+52.03	22.36' Lt.	1,849,580.188	1,011,389.443
288+63.28	3.09' Rt.	1,849,591.218	1,011,414.990

**COORDINATE TABLE**

STATION	OFFSET	NORTH	EAST
122+39.41	65.24' Rt.	1,849,423.924	1,011,332.752
122+64.20	9.25' Rt.	1,849,485.159	1,011,333.309
122+86.33	40.75' Lt.	1,849,539.832	1,011,333.807
122+88.98	46.75' Lt.	1,849,546.393	1,011,333.867
122+92.96	55.75' Lt.	1,849,556.233	1,011,333.956
123+53.20	⊕	1,849,528.927	1,011,411.360
124+03.44	65.26' Rt.	1,849,488.928	1,011,483.350

**JORGENSEN & ASSOCIATES, INC.**  
120 PARK AVENUE  
LAKE VILLA, ILLINOIS 60046  
(847) 356-3371

SHEET 1 IS A COVER SHEET AND IS NOT RECORDED.

**PLAT OF HIGHWAYS STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**  
F.A.P. 311 (U.S. ROUTE 34)

SECTION 652-A      DuPAGE COUNTY  
PROJECT      JOB NO. R-91-014-11  
STATION 122+00      TO STATION 127+00  
STATION 287+00.00      TO STATION 290+00  
SCALE: 1"=20'      SHEET 5 OF 18

**BUREAU OF LAND ACQUISITION**  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196

REVISION DATE April 19, 2013  
REVISION  
MADE BY

Configuration Parcel 1JY0015T.E., Changed Parcel 1JY0015P.E. to 1JY0015P.E.-A, Added Parcel 1JY0015P.E.-B Ownership Parcel 1JY0016P.E.

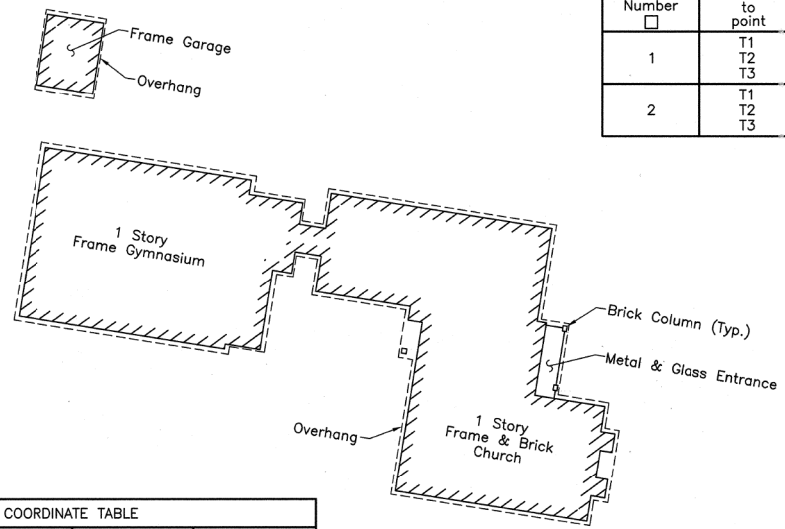
AS DOCUMENT NO.

PART OF THE SE 1/4 OF SEC. 29, TWP. 38 N., R. 9 E. OF THE 3RD. P.M., IN DuPAGE COUNTY, ILLINOIS.

EXISTING R.O.W. RECORDED INFORMATION		
Parcel	Document No.	Date Recorded
1JY0018	R79-72361	August 14, 1979
1JY0018	R94-019787	January 21, 1994
----	322413	February 15, 1932
----	R79-72361	August 14, 1979
----	R80-32592	June 6, 1980
----	R88-61176	June 13, 1988
----	R93-65154	April 6, 1993
----	93 ED 40	*November 2, 1993
----	R93-268121	November 18, 1993
----	93 ED 79	*January 31, 1994
----	R96-34511	March 1, 1996

\* Date Filed

Schedule of Ties		
Point Number	Tie to point	Tie Distance (feet)
1	T1	24.56
	T2	14.83
	T3	24.28
2	T1	21.63
	T2	9.75
	T3	21.82



COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
55+31.28	32.99' Lt.	1,850,157.084	1,012,279.002
55+31.31	44.14' Lt.	1,850,153.387	1,012,268.477
126+58.12	40.72' Lt.	1,849,687.174	1,011,675.162
126+60.34	45.72' Lt.	1,849,692.642	1,011,675.212
126+64.77	55.75' Lt.	1,849,703.607	1,011,675.311
131+01.19	45.69' Lt.	1,849,867.352	1,012,079.974
131+01.19	50.69' Lt.	1,849,871.943	1,012,077.992
133+09.09	69.33' Rt.	1,849,844.163	1,012,316.435
133+10.00	50.67' Lt.	1,849,954.692	1,012,269.702
133+10.00	55.75' Lt.	1,849,959.356	1,012,267.688
133+10.00	56.34' Lt.	1,849,959.898	1,012,267.454
133+44.93	56.34' Lt.	1,849,973.741	1,012,299.520
133+63.80	40.67' Lt.	1,849,966.852	1,012,323.106
133+66.15	50.68' Lt.	1,849,976.973	1,012,321.321
134+35.49	64.00' Rt.	1,849,898.636	1,012,430.020
134+86.29	46.54' Lt.	1,850,020.203	1,012,434.092

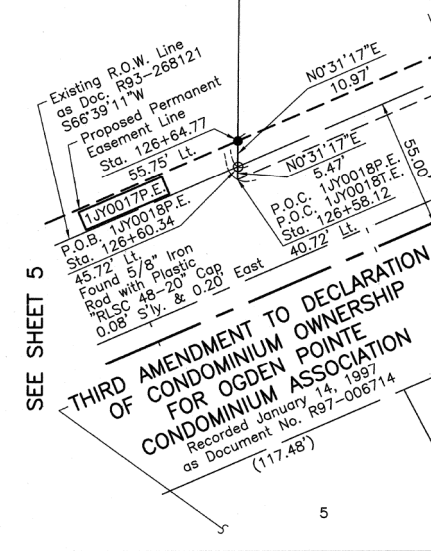
VILLAGE BAPTIST CHURCH RESUBDIVISION  
Recorded May 31, 1990  
as Document No. R90-065512

See Sheet 13 for Total Holdings Parcel 1JY0018P.E. & 1JY0018T.E.

WILLOW SHORES  
Recorded August 14, 1979  
as Document No. R79-72361

Outlot 2

See Sheet 5 for Parcel 1JY0017P.E.



SECOND AMENDMENT TO DECLARATION OF CONDOMINIUM OWNERSHIP FOR OGDEN POINTE CONDOMINIUM ASSOCIATION  
Recorded November 1, 1996  
as Document No. R96-179671

FIFTEENTH AMENDMENT TO DECLARATION OF CONDOMINIUM OWNERSHIP FOR OGDEN POINTE CONDOMINIUM ASSOCIATION  
Recorded April 1, 1999  
as Document No. R99-076137

EIGHTH AMENDMENT TO DECLARATION OF CONDOMINIUM OWNERSHIP FOR OGDEN POINTE CONDOMINIUM ASSOCIATION  
Recorded October 9, 1997  
as Document No. R97-153900

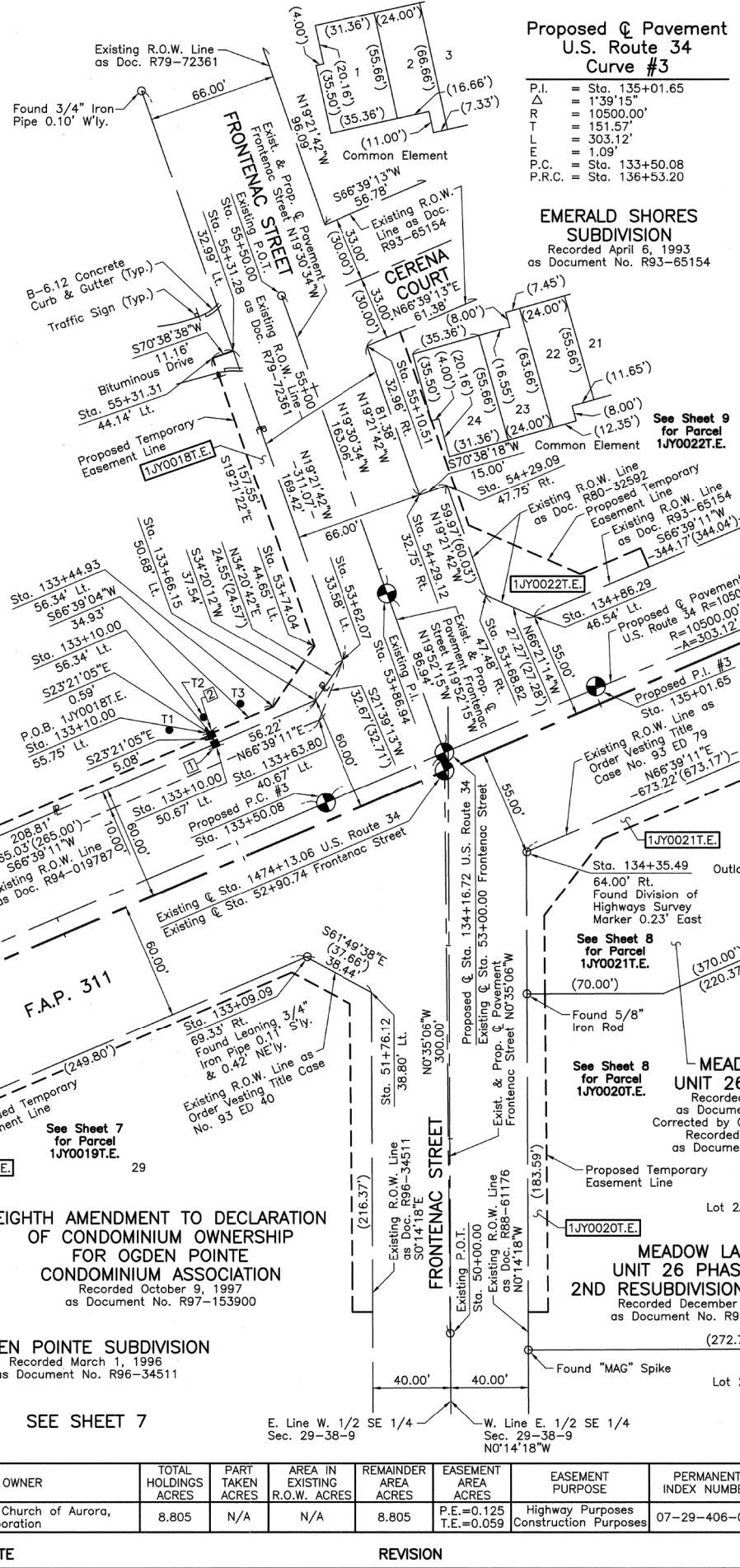
OGDEN POINTE SUBDIVISION  
Recorded March 1, 1996  
as Document No. R96-34511

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
1JY0018P.E.	Village Baptist Church of Aurora, an Illinois Corporation	8.805	N/A	N/A	8.805	P.E.=0.125 T.E.=0.059	Highway Purposes Construction Purposes	07-29-406-009	

REVISION DATE

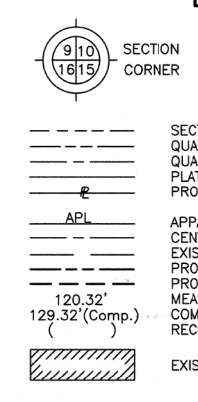
REVISION

MADE BY

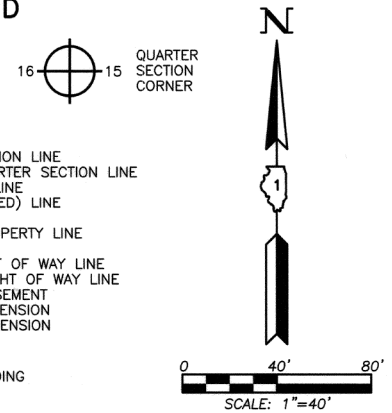


Proposed Pavement U.S. Route 34 Curve #3  
P.I. = Sta. 135+01.65  
Δ = 1'39"15"  
R = 10500.00'  
T = 151.57'  
E = 303.12'  
L = 1.09'  
P.C. = Sta. 133+50.08  
P.R.C. = Sta. 136+53.20

EMERALD SHORES SUBDIVISION  
Recorded April 6, 1993  
as Document No. R93-65154



LEGEND



SEE SHEET 9  
SEE SHEET 8

- Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.
- IRON PIPE OR ROD FOUND
- CUT CROSS FOUND OR SET
- THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }  
COUNTY OF LAKE }  
THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 29, TOWNSHIP 38N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, DuPAGE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.  
DATED AT LAKE VILLA, ILLINOIS THIS 23rd DAY OF October 2012 A.D.

CHRISTIAN H. JORGENSEN  
2797 PROFESSIONAL LAND SURVEYOR  
STATE OF ILLINOIS  
LAKE VILLA, ILLINOIS

Christina J. Jorgensen PRESIDENT  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797  
LICENSE EXPIRATION DATE: NOVEMBER 30, 2012  
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.  
Note: Surface Coordinates are Shown.

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
51+76.12	38.80' Lt.	1,849,826.014	1,012,350.322
53+62.07	33.58' Lt.	1,849,997.242	1,012,335.171
53+68.82	47.48' Rt.	1,850,031.139	1,012,409.115
53+74.04	44.65' Lt.	1,850,004.740	1,012,320.696
54+29.09	47.75' Rt.	1,850,087.718	1,012,389.233
54+29.12	32.75' Rt.	1,850,082.745	1,012,375.082
55+10.51	32.96' Rt.	1,850,159.527	1,012,348.100

MEADOW LAKES UNIT 26 PHASE ONE  
Recorded June 13, 1988  
as Document No. R88-61176  
Corrected by Certificate of Correction  
Recorded January 3, 1990  
as Document No. R90-000594

MEADOW LAKES UNIT 26 PHASE ONE 2ND RESUBDIVISION OF LOT 1  
Recorded December 2, 1997  
as Document No. R97-185065

RECEIVED  
NOV 09 2012  
PLATS & LEGALS

PLAT OF HIGHWAYS  
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
F.A.P. 311 (U.S. ROUTE 34)  
SECTION 652-A DuPAGE COUNTY  
PROJECT JOB NO. R-91-014-11  
STATION 50+00.00 TO STATION 55+50.00  
STATION 126+00 TO STATION 136+00  
SCALE: 1"=40' SHEET 6 OF 18

BUREAU OF LAND ACQUISITION  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196

SHEET 121 OF 383

PART OF THE SE 1/4 OF SEC. 29, TWP. 38 N., R. 9 E. OF THE 3RD. P.M., IN DuPAGE COUNTY, ILLINOIS.

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
1JY0019T.E.	Ogden Point Condominium Association, an Illinois not-for-profit corporation	*2.227	N/A	N/A	*2.227	0.098	Construction Purposes	07-29-419-001 thru 07-29-419-130	

\* Area based on Lots 1, 2, 3 & 29 in Ogden Point Subdivision Only.

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
128+02.17	73.63' Rt.	1,849,639.282	1,011,852.733
128+02.76	69.29' Rt.	1,849,643.504	1,011,851.558
131+01.19	45.69' Lt.	1,849,867.352	1,012,079.974
131+01.19	50.69' Lt.	1,849,871.943	1,012,077.992
132+99.19	73.65' Rt.	1,849,836.269	1,012,309.050
133+09.09	69.33' Rt.	1,849,844.163	1,012,316.435
133+66.15	50.68' Lt.	1,849,976.973	1,012,321.321
134+35.49	64.00' Rt.	1,849,898.636	1,012,430.020
134+86.29	46.54' Lt.	1,850,020.203	1,012,434.092

EXISTING R.O.W. RECORDED INFORMATION		
Parcel	Document No.	Date Recorded
1JY0019	93 ED 40	*November 2, 1993
1JY0019	R96-34511	March 1, 1996
----	322413	February 15, 1932
----	R79-72361	August 14, 1979
----	R80-32592	June 6, 1980
----	R88-61176	June 13, 1988
----	R94-019787	January 21, 1994
----	93 ED 79	*January 31, 1994
----	R93-65154	April 6, 1993
----	R96-34511	March 1, 1996

\* Date Filed

VILLAGE BAPTIST CHURCH RESUBDIVISION  
Recorded May 31, 1990  
as Document No. R90-065512

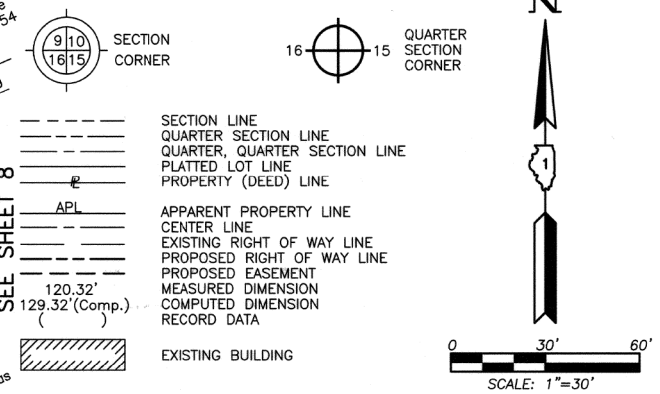
LOT 1

Proposed  $\phi$  Pavement  
U.S. Route 34  
Curve #3

P.I. = Sta. 135+01.65  
 $\Delta$  = 1°39'15"  
R = 10500.00'  
T = 151.57'  
L = 303.12'  
E = 1.09'  
P.C. = Sta. 133+50.08  
P.R.C. = Sta. 136+53.20

See Sheet 6 for Parcel 1JY0018P.E. & 1JY0018T.E.

LEGEND



Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.

- IRON PIPE OR ROD FOUND
- ⊗ "MAG" NAIL SET
- + CUT CROSS FOUND OR SET
- 5/8" REBAR SET
- T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- T2
- T3
- BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- BT2
- BT3
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }  
COUNTY OF LAKE }  
THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 29, TOWNSHIP 38N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, DuPAGE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE REPRODUCED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.  
DATED AT LAKE VILLA, ILLINOIS THIS 28th DAY OF June 2012 A.D.

MEADOW LAKES UNIT 26 PHASE ONE 2ND RESUBDIVISION OF LOT 1  
Recorded December 2, 1997  
as Document No. R97-185065

MEADOW LAKES UNIT 26 PHASE ONE 2ND RESUBDIVISION OF LOT 1  
Recorded December 2, 1997  
as Document No. R97-185065

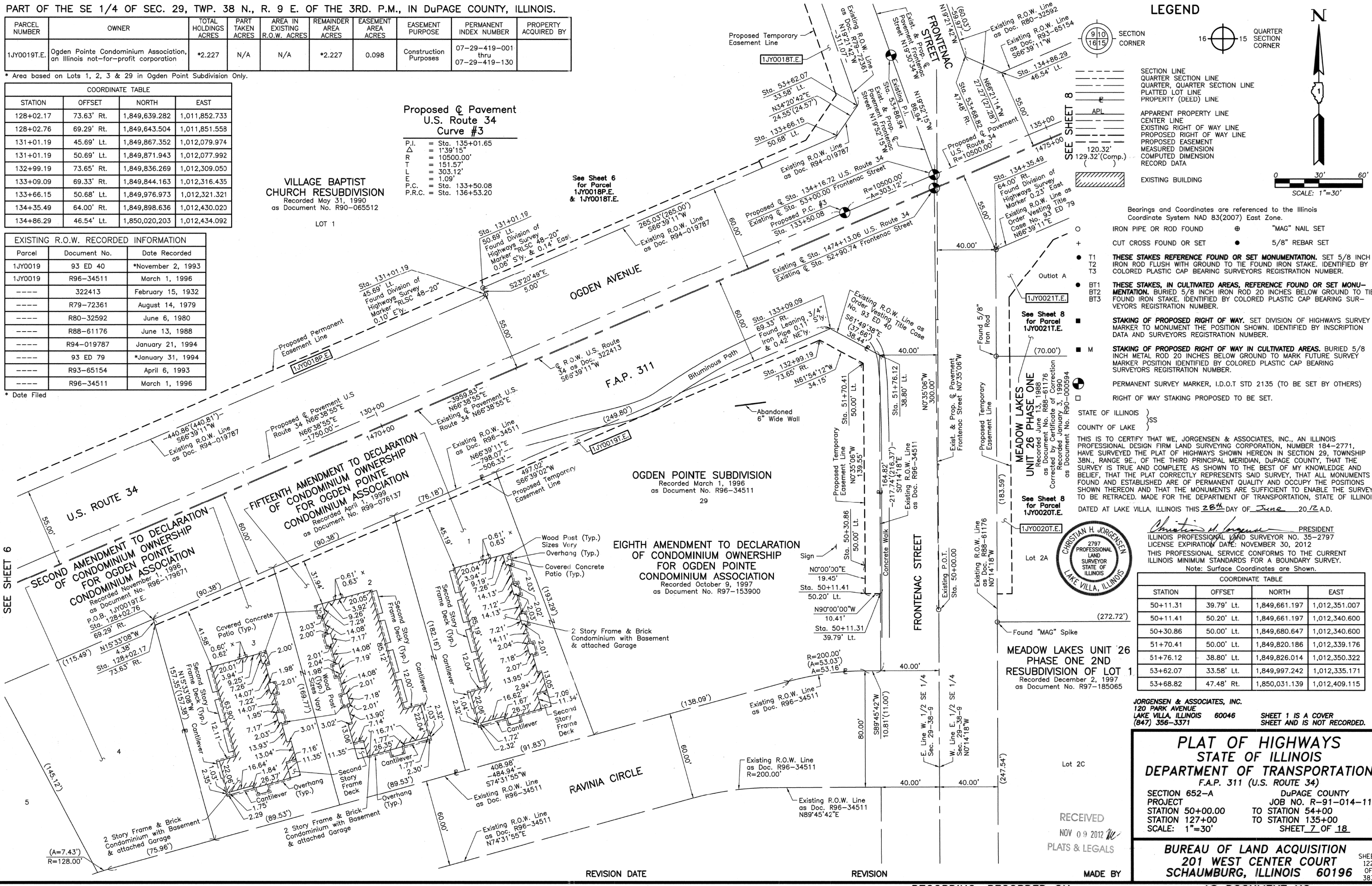
MEADOW LAKES UNIT 26 PHASE ONE 2ND RESUBDIVISION OF LOT 1  
Recorded December 2, 1997  
as Document No. R97-185065

MEADOW LAKES UNIT 26 PHASE ONE 2ND RESUBDIVISION OF LOT 1  
Recorded December 2, 1997  
as Document No. R97-185065

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
50+11.31	39.79' Lt.	1,849,661.197	1,012,351.007
50+11.41	50.20' Lt.	1,849,661.197	1,012,340.600
50+30.86	50.00' Lt.	1,849,680.647	1,012,340.600
51+70.41	50.00' Lt.	1,849,820.186	1,012,339.176
51+76.12	38.80' Lt.	1,849,826.014	1,012,350.322
53+62.07	33.58' Lt.	1,849,997.242	1,012,335.171
53+68.82	47.48' Rt.	1,850,031.139	1,012,409.115

JORGENSEN & ASSOCIATES, INC.  
120 PARK AVENUE  
LAKE VILLA, ILLINOIS 60046  
(847) 356-3371

PLAT OF HIGHWAYS  
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
F.A.P. 311 (U.S. ROUTE 34)  
SECTION 652-A DuPAGE COUNTY  
PROJECT JOB NO. R-91-014-11  
STATION 50+00.00 TO STATION 54+00  
STATION 127+00 TO STATION 135+00  
SCALE: 1"=30' SHEET 7 OF 18  
BUREAU OF LAND ACQUISITION  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196



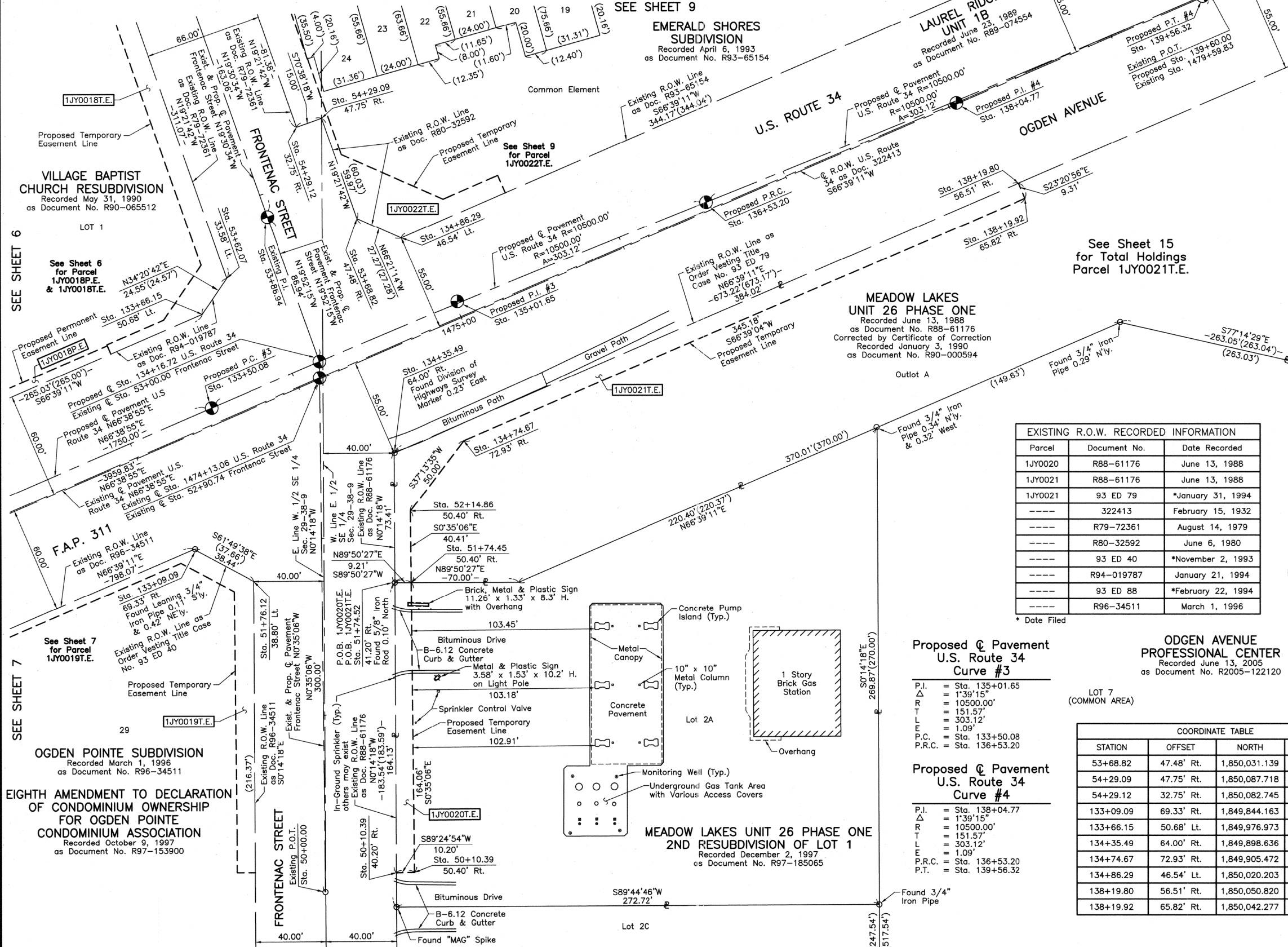
RECEIVED  
NOV 09 2012  
PLATS & LEGALS

MADE BY

SHEET 122 OF 383

PART OF THE SE 1/4 OF SEC. 29, TWP. 38 N., R. 9 E. OF THE 3RD. P.M., IN DuPAGE COUNTY, ILLINOIS.

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
1JY0020T.E.	JRL 3 Development Corp	1.350	N/A	N/A	1.350	0.037	Construction Purposes	07-29-410-007	
1JY0021T.E.	Meadow Lakes Owners Association, Inc.	2.200	N/A	N/A	2.200	0.105	Construction Purposes	07-29-410-004	



**LEGEND**

SECTION CORNER 9 10 16 15  
QUARTER SECTION CORNER

SECTION LINE  
QUARTER SECTION LINE  
QUARTER QUARTER SECTION LINE  
PLATTED LOT LINE  
PROPERTY (DEED) LINE

APL  
APPARENT PROPERTY LINE  
CENTER LINE  
EXISTING RIGHT OF WAY LINE  
PROPOSED RIGHT OF WAY LINE  
PROPOSED EASEMENT  
MEASURED DIMENSION  
COMPUTED DIMENSION  
RECORD DATA

EXISTING BUILDING

Scale: 1"=30'

- Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.
- IRON PIPE OR ROD FOUND
  - ⊕ "MAG" NAIL SET
  - + CUT CROSS FOUND OR SET
  - 5/8" REBAR SET
  - T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - T2
  - T3
  - BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - BT2
  - BT3
  - STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
  - M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - ⊙ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
  - RIGHT OF WAY STAKING PROPOSED TO BE SET.

**EXISTING R.O.W. RECORDED INFORMATION**

Parcel	Document No.	Date Recorded
1JY0020	R88-61176	June 13, 1988
1JY0021	R88-61176	June 13, 1988
1JY0021	93 ED 79	*January 31, 1994
---	322413	February 15, 1932
---	R79-72361	August 14, 1979
---	R80-32592	June 6, 1980
---	93 ED 40	*November 2, 1993
---	R94-019787	January 21, 1994
---	93 ED 88	*February 22, 1994
---	R96-34511	March 1, 1996

\* Date Filed



THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 29, TOWNSHIP 38N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, DuPAGE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RE-Traced. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS 26th DAY OF June 2012 A.D.

*Christian H. Jorgensen* PRESIDENT  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797  
LICENSE EXPIRATION DATE: NOVEMBER 30, 2012  
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.  
Note: Surface Coordinates are Shown.

**OGDEN AVENUE PROFESSIONAL CENTER**  
Recorded June 13, 2005  
as Document No. R2005-122120

RECEIVED  
APR 01 2013  
PLATS & LEGALS

**COORDINATE TABLE**

STATION	OFFSET	NORTH	EAST
53+68.82	47.48' Rt.	1,850,031.139	1,012,409.115
54+29.09	47.75' Rt.	1,850,087.718	1,012,389.233
54+29.12	32.75' Rt.	1,850,082.745	1,012,375.082
133+09.09	69.33' Rt.	1,849,844.163	1,012,316.435
133+66.15	50.68' Lt.	1,849,976.973	1,012,321.321
134+35.49	64.00' Rt.	1,849,898.636	1,012,430.020
134+74.67	72.93' Rt.	1,849,905.472	1,012,469.371
134+86.29	46.54' Lt.	1,850,020.203	1,012,434.092
138+19.80	56.51' Rt.	1,850,050.820	1,012,782.594
138+19.92	65.82' Rt.	1,850,042.277	1,012,786.282

**Proposed Pavement U.S. Route 34 Curve #3**

P.I. = Sta. 135+01.65  
Δ = 1°39'15"  
R = 10500.00'  
T = 151.57'  
L = 303.12'  
E = 1.09'  
P.C. = Sta. 133+50.08  
P.R.C. = Sta. 136+53.20

**Proposed Pavement U.S. Route 34 Curve #4**

P.I. = Sta. 138+04.77  
Δ = 1°39'15"  
R = 10500.00'  
T = 151.57'  
L = 303.12'  
E = 1.09'  
P.R.C. = Sta. 136+53.20  
P.T. = Sta. 139+56.32

**COORDINATE TABLE**

STATION	OFFSET	NORTH	EAST
50+10.39	40.20' Rt.	1,849,661.095	1,012,431.008
50+10.39	50.40' Rt.	1,849,661.199	1,012,441.210
51+74.45	50.40' Rt.	1,849,825.252	1,012,439.535
51+74.52	41.20' Rt.	1,849,825.226	1,012,430.326
51+76.12	38.80' Lt.	1,849,826.014	1,012,350.322
52+14.86	50.40' Rt.	1,849,865.660	1,012,439.122
53+62.07	33.58' Lt.	1,849,997.242	1,012,335.171

JORGENSEN & ASSOCIATES, INC.  
120 PARK AVENUE  
LAKE VILLA, ILLINOIS 60046  
(847) 356-3371

SHEET 1 IS A COVER SHEET AND IS NOT RECORDED.

**PLAT OF HIGHWAYS STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION F.A.P. 311 (U.S. ROUTE 34)**

SECTION 652-A DuPAGE COUNTY  
PROJECT JOB NO. R-91-014-11  
STATION 50+00.00 TO STATION 55+00  
STATION 132+00 TO STATION 139+60.00  
SCALE: 1"=30' SHEET 8 OF 18

**BUREAU OF LAND ACQUISITION**  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196

SHEET 123 OF 383

PART OF THE SE 1/4 OF SEC. 29, TWP. 38 N., R. 9 E. OF THE 3RD. P.M., IN DuPAGE COUNTY, ILLINOIS.

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
1JY0022T.E.	Oakview Townhomes Association, an Illinois Not For Profit Corporation	1.281	N/A	N/A	1.281	0.081	Grading	07-29-401-034	

EXISTING R.O.W. RECORDED INFORMATION		
Parcel	Document No.	Date Recorded
1JY0022	R79-72361	August 14, 1979
1JY0022	R80-32592	June 6, 1980
1JY0022	R93-65154	April 6, 1993
----	322413	February 15, 1932
----	R79-72361	August 14, 1979
----	R88-61176	June 13, 1988
----	93 ED 40	*November 2, 1993
----	R94-019787	January 21, 1994
----	93 ED 79	*January 31, 1994
----	R96-34511	March 1, 1996

Proposed  $\phi$  Pavement  
U.S. Route 34  
Curve #3

P.I. = Sta. 135+01.65  
 $\Delta$  = 1°39'15"  
 R.T. = 10500.00'  
 L = 151.57'  
 T.L. = 303.12'  
 L = 1.09'  
 P.C. = Sta. 133+50.08  
 P.R.C. = Sta. 136+53.20

VILLAGE BAPTIST  
CHURCH RESUBDIVISION  
Recorded May 31, 1990  
as Document No. R90-065512

LOT 1

### LEGEND

SECTION CORNER 910 1615  
 QUARTER SECTION CORNER 16 15

SECTION LINE  
 QUARTER SECTION LINE  
 QUARTER, QUARTER SECTION LINE  
 PLATTED LOT LINE  
 PROPERTY (DEED) LINE

APL  
 APPARENT PROPERTY LINE  
 CENTER LINE  
 EXISTING RIGHT OF WAY LINE  
 PROPOSED RIGHT OF WAY LINE  
 PROPOSED EASEMENT  
 MEASURED DIMENSION  
 COMPUTED DIMENSION  
 RECORD DATA

120.32'  
 129.32'(Comp.)

EXISTING BUILDING

Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.

○ IRON PIPE OR ROD FOUND      ⊕ "MAG" NAIL SET  
 + CUT CROSS FOUND OR SET      ● 5/8" REBAR SET

● T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.  
 ● T2  
 ● T3

● BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.  
 ● BT2  
 ● BT3

■ STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

■ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

⊕ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)

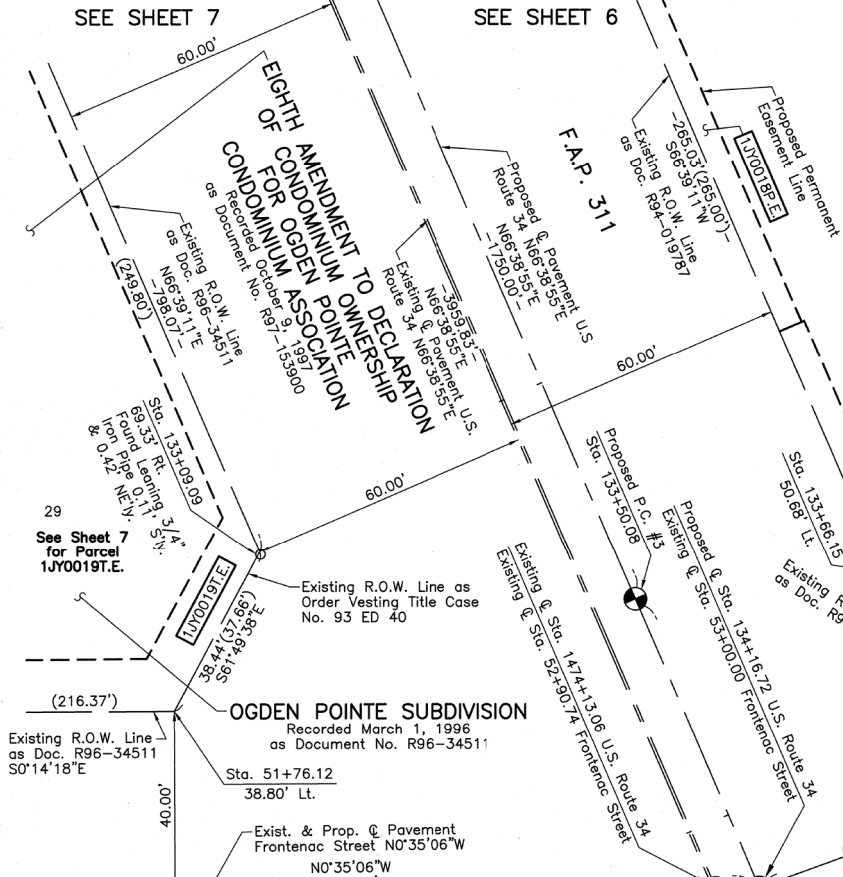
□ RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS } SS  
 COUNTY OF LAKE }

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 29, TOWNSHIP 38N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, DuPAGE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

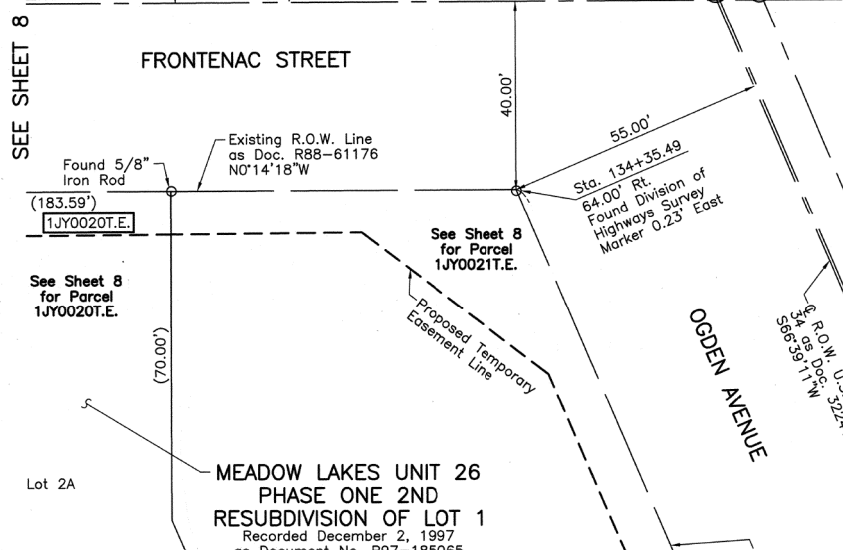
DATED AT LAKE VILLA, ILLINOIS THIS 26<sup>th</sup> DAY OF June 2012 A.D.

RECEIVED  
 NOV 09 2012  
 PLATS & LEGALS



See Sheet 6 for Parcel 1JY0018P.E. & 1JY0018T.E.

Proposed Temporary Easement Line



Exist. & Prop.  $\phi$  Pavement Frontenac Street N19°52'15"W Existing P.I. Sta. 53+66.94

Exist. & Prop.  $\phi$  Pavement Frontenac Street N19°30'34"W Existing P.I. Sta. 53+62.07

STATION	OFFSET	NORTH	EAST
54+63.38	51.35' Rt.	1,850,121.246	1,012,381.178
55+10.51	32.96' Rt.	1,850,159.527	1,012,348.100
55+11.75	51.48' Rt.	1,850,166.884	1,012,365.145
133+09.09	69.33' Rt.	1,849,844.163	1,012,316.435
133+66.15	50.68' Lt.	1,849,976.973	1,012,321.321
134+35.49	64.00' Rt.	1,849,898.636	1,012,430.020
134+86.29	46.54' Lt.	1,850,020.203	1,012,434.092
135+16.37	57.66' Lt.	1,850,042.056	1,012,457.756
135+49.86	58.25' Lt.	1,850,055.403	1,012,488.677
135+50.06	47.56' Lt.	1,850,045.592	1,012,492.912
138+30.50	53.63' Lt.	1,850,156.597	1,012,750.082

MEADOW LAKES UNIT 26 PHASE ONE  
Recorded June 13, 1988  
as Document No. R88-61176  
Corrected by Certificate of Correction  
Recorded January 3, 1990  
as Document No. R90-000594

See Sheet 16 for Total Holdings Parcel 1JY0022T.E.

EMERALD SHORES SUBDIVISION  
Recorded April 6, 1993  
as Document No. R93-65154



STATION	OFFSET	NORTH	EAST
51+76.12	38.80' Lt.	1,849,826.014	1,012,350.322
53+62.07	33.58' Lt.	1,849,997.242	1,012,335.171
53+68.82	47.48' Lt.	1,850,031.139	1,012,409.115
54+06.06	51.35' Rt.	1,850,067.212	1,012,400.316
54+29.09	47.75' Rt.	1,850,087.718	1,012,389.233
54+29.12	32.75' Rt.	1,850,082.745	1,012,375.082
54+63.37	51.50' Rt.	1,850,121.286	1,012,381.320

JORGENSEN & ASSOCIATES, INC.  
120 PARK AVENUE  
LAKE VILLA, ILLINOIS 60046  
(847) 356-3371

PLAT OF HIGHWAYS  
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
F.A.P. 311 (U.S. ROUTE 34)

SECTION 652-A DuPAGE COUNTY  
PROJECT JOB NO. R-91-014-11  
STATION 51+00 TO STATION 55+50.00  
STATION 132+00 TO STATION 137+00  
SCALE: 1"=20' SHEET 9 OF 18

BUREAU OF LAND ACQUISITION  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196



EXISTING R.O.W. RECORDED INFORMATION		
Parcel	Document No.	Date Recorded
1JY0015	217098	July 6, 1926
----	R79-72361	August 14, 1979
----	R85-22940	April 2, 1985
----	R93-268121	November 18, 1993
----	R94-019787	January 21, 1994

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
299+10.03	55.46' Lt.	1,850,638.439	1,136,502
300+00.36	13.64' Lt.	1,850,728.405	1,140,810
300+00.45	3.25' Lt.	1,850,728.404	1,141,483
300+00.70	25.76' Rt.	1,850,728.402	1,144,750
300+00.79	36.36' Rt.	1,850,728.401	1,145,810
300+50.00	68.64' Lt.	1,850,778.518	1,135,539
300+50.00	13.64' Lt.	1,850,778.042	1,140,837
301+06.51	77.07' Lt.	1,850,835.103	1,134,594

**LEGEND**

SECTION CORNER  
QUARTER SECTION CORNER

SECTION LINE  
QUARTER SECTION LINE  
QUARTER, QUARTER SECTION LINE  
PLATTED LOT LINE  
PROPERTY (DEED) LINE

APL  
APPARENT PROPERTY LINE  
CENTER LINE  
EXISTING RIGHT OF WAY LINE  
PROPOSED RIGHT OF WAY LINE  
PROPOSED EASEMENT  
MEASURED DIMENSION  
COMPUTED DIMENSION  
RECORD DATA

EXISTING BUILDING

Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.

○ IRON PIPE OR ROD FOUND  
⊕ "MAG" NAIL SET  
+ CUT CROSS FOUND OR SET  
● 5/8" REBAR SET

● T1  
● T2  
● T3  
● BT1  
● BT2  
● BT3

● THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

● THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

● STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

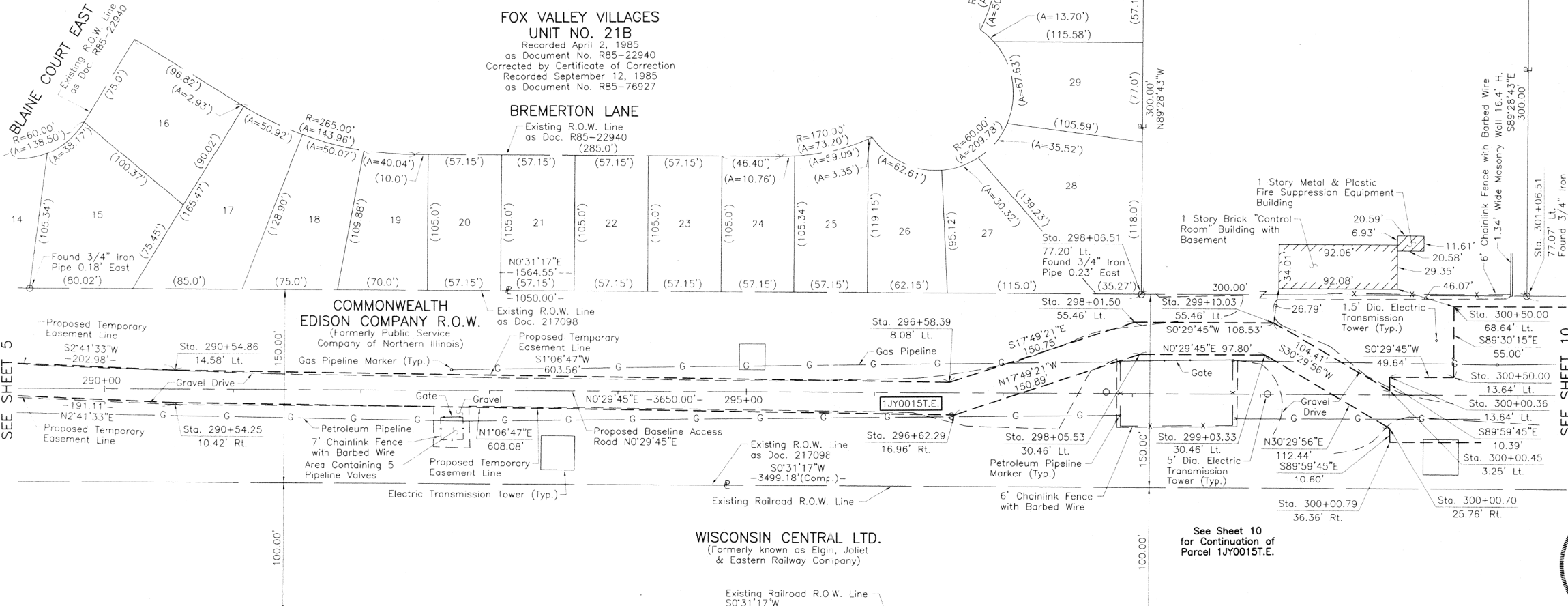
● STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

● PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)

□ RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }  
COUNTY OF LAKE }SS

SCALE: 1"=50'



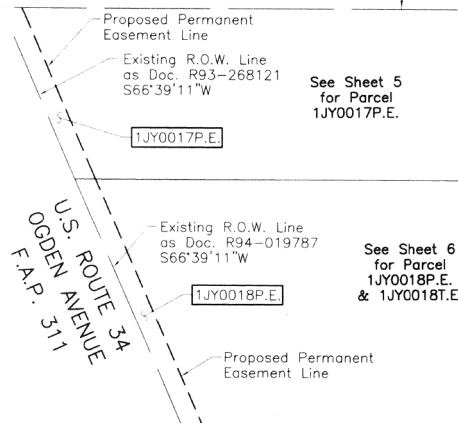
THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 29, TOWNSHIP 38N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, DuPAGE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS 7th DAY OF June 2013 A.D.

CHRISTIAN H. JORGENSEN  
2797 PROFESSIONAL LAND SURVEYOR  
STATE OF ILLINOIS  
LAKE VILLA, ILLINOIS

PRESIDENT  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797  
LICENSE EXPIRATION DATE: NOVEMBER 30, 2014  
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.  
Note: Surface Coordinates are Shown.

STATION	OFFSET	NORTH	EAST
290+54.25	10.42' Rt.	1,849,782.117	1,011,423.967
290+54.86	14.58' Lt.	1,849,782.947	1,011,398.979
296+58.39	8.08' Lt.	1,850,386.398	1,011,410.702
296+62.29	16.96' Rt.	1,850,390.081	1,011,435.778
298+01.50	55.46' Lt.	1,850,529.913	1,011,364.563
298+05.53	30.46' Lt.	1,850,533.728	1,011,389.597
298+06.51	77.20' Lt.	1,850,535.115	1,011,342.864
299+03.33	30.46' Lt.	1,850,631.524	1,011,390.443



VILLAGE BAPTIST CHURCH RESUBDIVISION  
Recorded May 31, 1990  
as Document No. R90-065512

WILLOW SHORES  
Recorded August 14, 1979  
as Document No. R79-72361

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
1JY0015P.E.-A 1JY0015P.E.-B 1JY0015T.E.	Commonwealth Edison Company formerly known as Public Service Company of Northern Illinois	N/A	N/A	N/A	N/A	P.E.-A=0.034 P.E.-B=1.032 T.E.=3.216	Highway Purposes Access & Bridge Maintenance Construction Purposes	07-20-41;-002 07-29-20;-002 07-29-20;-004 07-29-20;-006 07-29-20;-008 07-29-40;-002 07-29-40;-002	

RECEIVED  
JUN 07 2013  
PLATS & LEGALS

JORGENSEN & ASSOCIATES, INC.  
120 PARK AVENUE  
LAKE VILLA, ILLINOIS 60046  
(847) 356-3371

PLAT OF HIGHWAYS  
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
F.A.P. 311 (U.S. ROUTE 34)  
SECTION 652-A  
PROJECT STATION 289+00 TO STATION 302+00  
SCALE: 1"=50'  
SHEET 10A OF 18

BUREAU OF LAND ACQUISITION  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196

PART OF THE EAST 1/2 OF SEC. 29, TWP. 38 N., R. 9 E. OF THE 3RD. P.M., IN DuPAGE COUNTY, ILLINOIS

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
1JY0015P.E.-A 1JY0015P.E.-B 1JY0015T.E.	Commonwealth Edison Company formerly known as Public Service Company of Northern Illinois	N/A	N/A	N/A	N/A	P.E.-A=0.034 P.E.-B=1.032 T.E.=3.216	Highway Purposes Access & Bridge Maintenance Construction Purposes	07-20-413-002 07-29-200-002 07-29-200-004 07-29-200-006 07-29-200-008 07-29-400-002 07-29-402-002	

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
301+06.51	77.07' Lt.	1,850,835.103	1,011,345.594
303+21.39	38.84' Lt.	1,851,049.640	1,011,385.682
303+21.60	14.64' Lt.	1,851,049.641	1,011,409.883
303+45.91	39.06' Lt.	1,851,074.161	1,011,385.674
303+46.12	14.86' Lt.	1,851,074.162	1,011,409.875
307+71.79	68.64' Lt.	1,851,500.281	1,011,359.786
307+71.79	13.64' Lt.	1,851,499.805	1,011,414.784
308+24.96	36.36' Rt.	1,851,552.540	1,011,465.238
308+24.97	26.36' Rt.	1,851,552.637	1,011,455.238

**LEGEND**

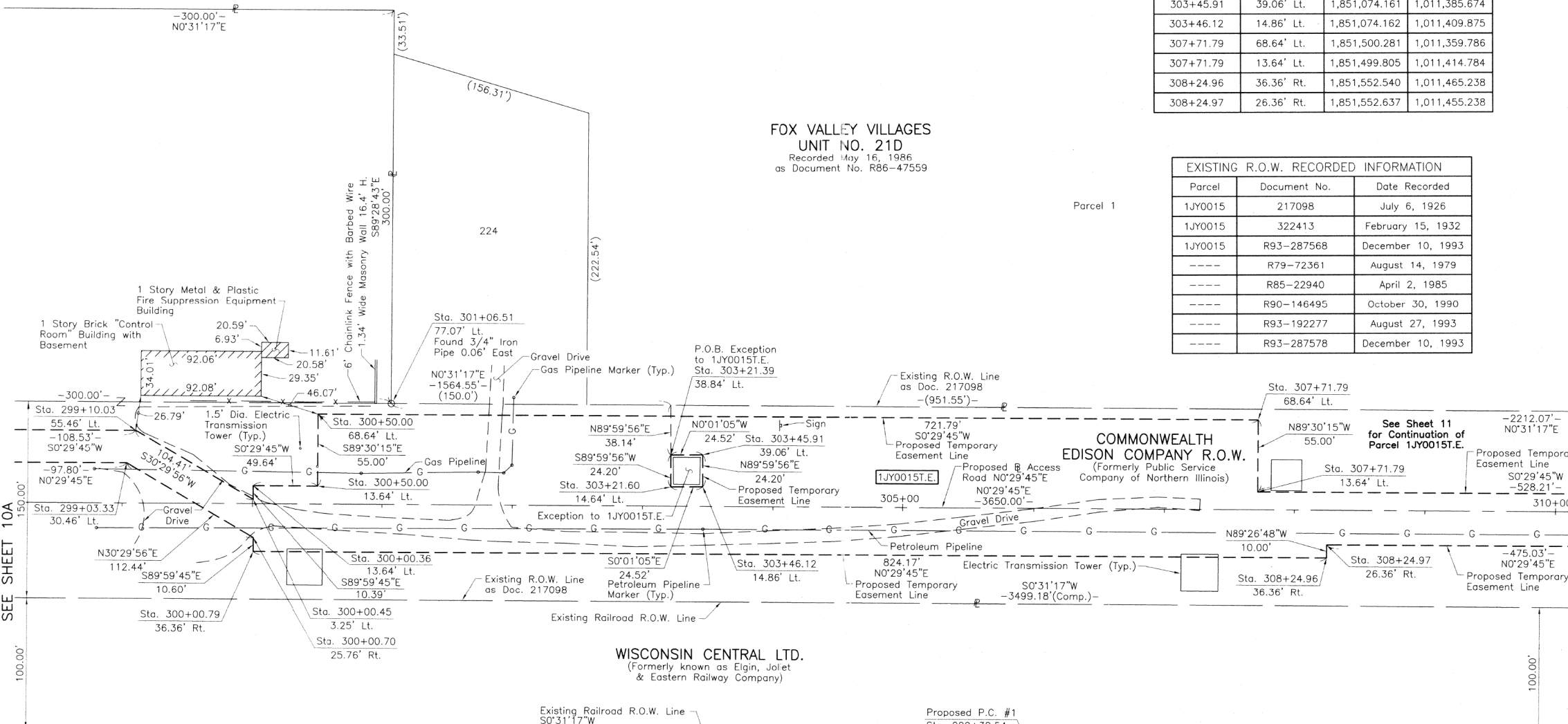
SECTION CORNER  
QUARTER SECTION CORNER

SECTION LINE  
QUARTER SECTION LINE  
QUARTER SECTION LINE  
PLATTED LOT LINE  
PROPERTY (DEED) LINE

APL  
APPARENT PROPERTY LINE  
CENTER LINE  
EXISTING RIGHT OF WAY LINE  
PROPOSED RIGHT OF WAY LINE  
PROPOSED EASEMENT  
MEASURED DIMENSION  
COMPUTED DIMENSION  
RECORD DATA

EXISTING BUILDING

SCALE: 1"=50'



SEE SHEET 10A

WILLOW SHORES  
Recorded August 14, 1979 as Document No. R79-72361  
Outlot 2

WILLOW SHORES  
Recorded August 14, 1979 as Document No. R79-72361

WILLOW SHORES CONDOMINIUM  
Recorded September 23, 1980 as Document No. R80-37120

DIAMOND BAY  
PLANNED UNIT DEVELOPMENT  
UNITS I AND II  
Recorded October 30, 1990 as Document No. R90-146495

EXISTING R.O.W. RECORDED INFORMATION		
Parcel	Document No.	Date Recorded
1JY0015	217098	July 6, 1926
1JY0015	322413	February 15, 1932
1JY0015	R93-287568	December 10, 1993
----	R79-72361	August 14, 1979
----	R85-22940	April 2, 1985
----	R90-146495	October 30, 1990
----	R93-192277	August 27, 1993
----	R93-287578	December 10, 1993

Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.

○ IRON PIPE OR ROD FOUND    ⊕ "MAG" NAIL SET  
+ CUT CROSS FOUND OR SET    ● 5/8" REBAR SET

● T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.  
● T2  
● T3

● BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.  
● BT2  
● BT3

■ STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

■ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

● PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)

□ RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }  
COUNTY OF LAKE }SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 29, TOWNSHIP 38N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, DU PAGE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS 26<sup>th</sup> DAY OF June 20 2012 A.D.



Christian H. Jorgensen  
PRESIDENT  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797  
LICENSE EXPIRATION DATE: NOVEMBER 30, 2012  
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.  
Note: Surface Coordinates are Shown.

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
299+03.33	30.46' Lt.	1,850,631.524	1,011,390.443
299+10.03	55.46' Lt.	1,850,638.439	1,011,365.502
300+00.36	13.64' Lt.	1,850,728.405	1,011,408.107
300+00.45	3.25' Lt.	1,850,728.404	1,011,418.493
300+00.70	25.76' Rt.	1,850,728.402	1,011,447.506
300+00.79	36.36' Rt.	1,850,728.401	1,011,458.104
300+50.00	68.64' Lt.	1,850,778.518	1,011,353.539
300+50.00	13.64' Lt.	1,850,778.042	1,011,408.537

RECEIVED  
JUN 07 2012  
PLATS & LEGALS

JORGENSEN & ASSOCIATES, INC.  
120 PARK AVENUE  
LAKE VILLA, ILLINOIS 60046  
(847) 356-3371

SHEET 1 IS A COVER SHEET AND IS NOT RECORDED.

**PLAT OF HIGHWAYS**  
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
F.A.P. 311 (U.S. ROUTE 34)  
SECTION 652-A DuPAGE COUNTY  
PROJECT JOB NO. R-91-014-11  
STATION 298+00 TO STATION 310+00  
SCALE: 1"=50' SHEET 10 OF 18

BUREAU OF LAND ACQUISITION  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196

SHEET 126 OF 383

REVISION DATE April 19, 2013

REVISION MADE BY

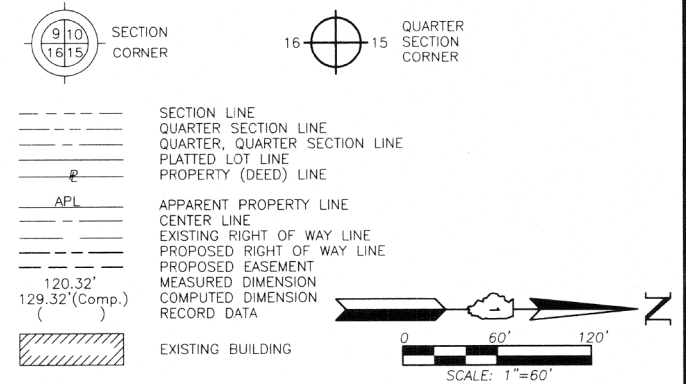
PART OF THE SE 1/4 OF SEC. 20 AND PART OF THE NE 1/4 OF SEC. 29, TWP. 38 N., R. 9 E. OF THE 3RD. P.M., IN DuPAGE COUNTY, ILLINOIS.

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
1JY0015P.E.-A 1JY0015P.E.-B 1JY0015T.E.	Commonwealth Edison Company formerly known as Public Service Company of Northern Illinois	N/A	N/A	N/A	N/A	P.E.-A=0.034 P.E.-B=1.032 T.E.=3.216	Highway Purposes Access & Bridge Maintenance Construction Purposes	07-20-413-002 07-29-210-002 07-29-210-004 07-29-210-006 07-29-210-008 07-29-410-002	

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
317+50.00	13.64' Lt.	1,852,477.978	1,011,423.246
317+50.00	26.36' Rt.	1,852,477.632	1,011,463.245
317+50.00	29.15' Rt.	1,852,477.608	1,011,466.034
323+18.58	76.09' Lt.	1,853,047.082	1,011,365.724
323+20.00	13.64' Lt.	1,853,047.961	1,011,428.180
323+20.91	26.36' Rt.	1,853,048.524	1,011,468.186
323+22.00	73.92' Rt.	1,853,049.194	1,011,515.749

EXISTING R.O.W. RECORDED INFORMATION		
Parcel	Document No.	Date Recorded
1JY0015	214148	May 24, 1926
1JY0015	217098	July 6, 1926
---	R75-02469	January 16, 1975
---	R90-112342	August 29, 1990
---	R90-146495	October 30, 1990

LEGEND



Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.

- IRON PIPE OR ROD FOUND      ⊕ "MAG" NAIL SET
- + CUT CROSS FOUND OR SET      ● 5/8" REBAR SET
- THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- BT1 BT2 BT3 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ⊙ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }  
COUNTY OF LAKE }SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM AND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON BETWEEN SECTION 20, TOWNSHIP 38N., RANGE 9E. AND SECTION 29, TOWNSHIP 38N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, DUPAGE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS 28th DAY OF June, 2013 A.D.



*Christian H. Jorgensen* PRESIDENT  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797  
LICENSE EXPIRATION DATE: NOVEMBER 30, 2012  
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

Note: Surface Coordinates are Shown.

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
313+00.00	45.85' Lt.	1,852,028.274	1,011,387.142
313+00.00	13.64' Lt.	1,852,027.995	1,011,419.356
313+00.00	26.36' Rt.	1,852,027.649	1,011,459.350
313+00.00	29.15' Rt.	1,852,027.625	1,011,462.140
314+54.90	45.85' Lt.	1,852,183.168	1,011,388.483
314+54.90	29.15' Lt.	1,852,182.519	1,011,463.480
317+50.00	45.85' Lt.	1,852,478.257	1,011,391.037

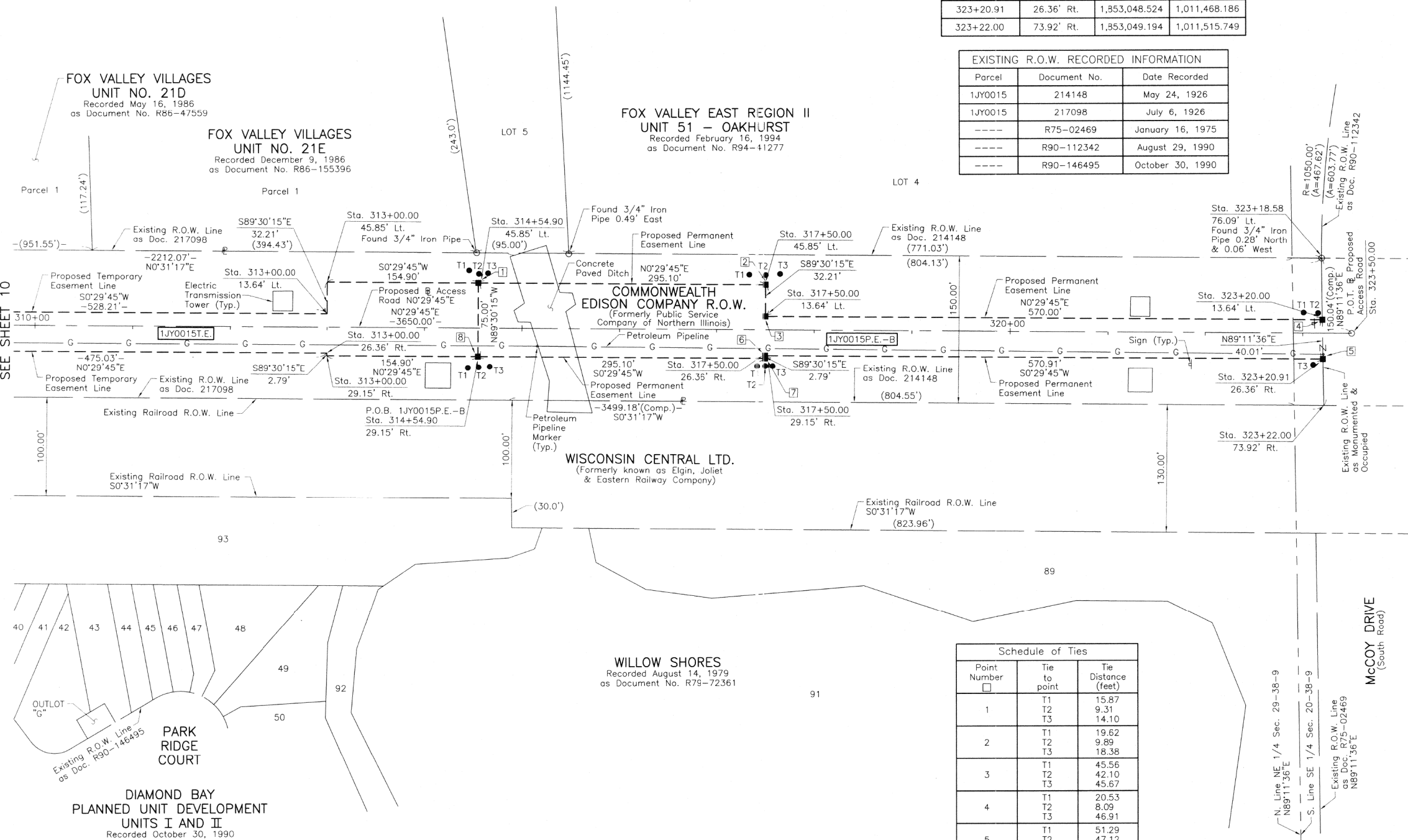
JORGENSEN & ASSOCIATES, INC.  
120 PARK AVENUE  
LAKE VILLA, ILLINOIS 60046 SHEET 1 IS A COVER SHEET AND IS NOT RECORDED.  
(847) 356-3371

**PLAT OF HIGHWAYS**  
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
F.A.P. 311 (U.S. ROUTE 34)

SECTION 652-A DuPAGE COUNTY  
PROJECT JOB NO. R-91-014-11  
STATION 310+00 TO STATION 323+50.00  
SCALE: 1"=60' SHEET 11 OF 18

BUREAU OF LAND ACQUISITION  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196  
SHEET 127 OF 383

Schedule of Ties		
Point Number	Tie to point	Tie Distance (feet)
1	T1	15.87
	T2	9.31
	T3	14.10
2	T1	19.62
	T2	9.89
	T3	18.38
3	T1	45.56
	T2	42.10
	T3	45.67
4	T1	20.53
	T2	8.09
	T3	46.91
5	T1	51.29
	T2	47.12
	T3	11.70
6	T1	13.50
	T2	10.64
	T3	13.09
7	T1	11.41
	T2	7.85
	T3	10.94
8	T1	15.37
	T2	10.92
	T3	15.88



SEE SHEET 10

ROUTE F.A.P. 311 (U.S. ROUTE 34)

SECTION 652-A

COUNTY DuPAGE

JOB NO. R-91-014-11

RECORDING: RECORDED ON

AS DOCUMENT NO.

May 24, 2013

REVISION DATE April 19, 2013

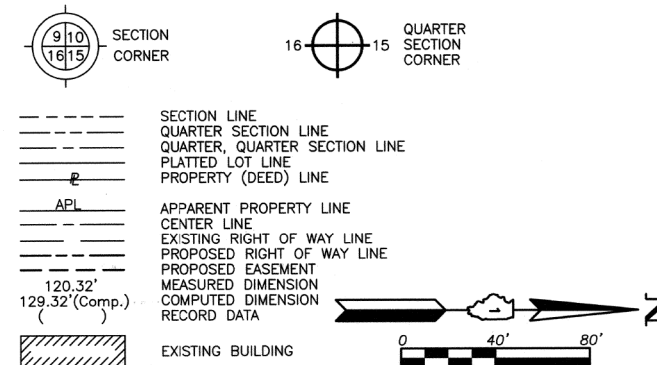
REVISION

Configuration Parcel 1JY0015T.E., Changed Parcel 1JY0015P.E. to 1JY0015P.E.-A, Added Parcel 1JY0015P.E.-B Revised Railroad Name

MADE BY

RECEIVED  
JUN 07 2013  
PLATS & LEGALS

LEGEND



Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.

- IRON PIPE OR ROD FOUND
- ⊕ "MAG" NAIL SET
- + CUT CROSS FOUND OR SET
- 5/8" REBAR SET
- T1, T2, T3 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- BT1, BT2, BT3 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ⊙ PERMANENT SURVEY MARKER, I.D.O.T. STD 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }  
 COUNTY OF LAKE }SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 29, TOWNSHIP 38N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, DuPAGE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS 22<sup>ND</sup> DAY OF APRIL 2013 A.D.



RECEIVED  
 APR 22 2013  
 PLATS & LEGALS

COORDINATE TABLE

STATION	OFFSET	NORTH	EAST
*199+98.70	19.95' Rt.	1,851,387.416	1,011,633.617
200+01.41	17.50' Lt.	1,851,387.091	1,011,671.157
200+35.50	55.70' Rt.	1,851,348.422	1,011,600.276
200+37.90	22.78' Rt.	1,851,348.121	1,011,633.277
200+41.81	34.07' Lt.	1,851,347.602	1,011,690.272
205+00.31	15.14' Rt.	1,850,887.514	1,011,629.290
205+00.32	25.00' Rt.	1,850,887.599	1,011,619.431
206+36.24	25.17' Lt.	1,850,751.440	1,011,669.508

\* Forward Tangent Extended Back  
 JORGENSEN & ASSOCIATES, INC.  
 120 PARK AVENUE  
 LAKE VILLA, ILLINOIS 60046 SHEET 1 IS A COVER  
 (847) 356-3371 SHEET AND IS NOT RECORDED.

**PLAT OF HIGHWAYS**  
 STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 F.A.P. 311 (U.S. ROUTE 34)

SECTION 652-A DuPAGE COUNTY  
 PROJECT JOB NO. R-91-014-11  
 STATION 200+00.00 TO STATION 212+05.00  
 SCALE: 1"=40' SHEET 12 OF 18

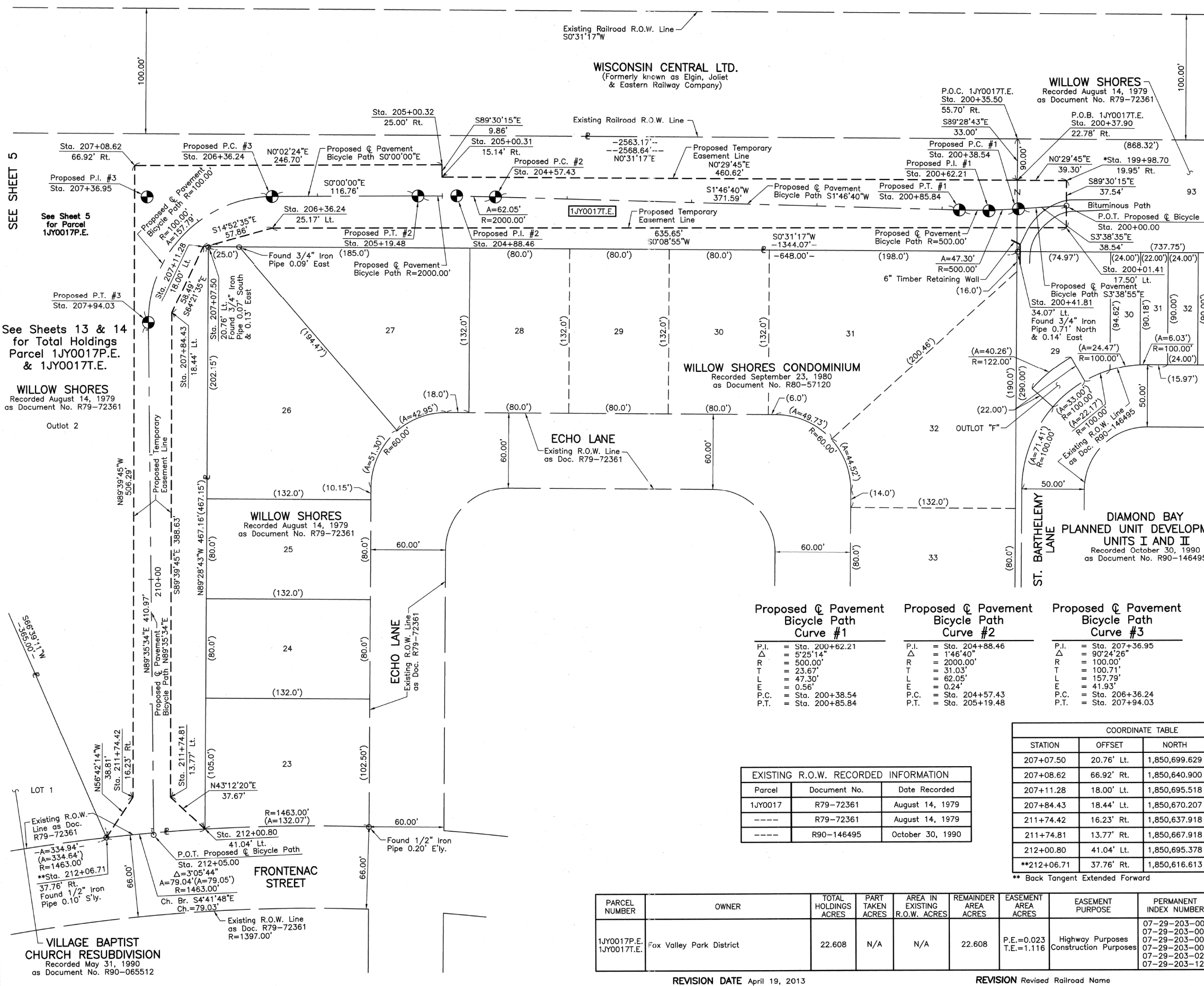
**BUREAU OF LAND ACQUISITION**  
 201 WEST CENTER COURT  
 SCHAUMBURG, ILLINOIS 60196

EXISTING R.O.W. RECORDED INFORMATION

Parcel	Document No.	Date Recorded
1JY0017	R79-72361	August 14, 1979
----	R79-72361	August 14, 1979
----	R90-146495	October 30, 1990

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
1JY0017P.E. 1JY0017T.E.	Fox Valley Park District	22.608	N/A	N/A	22.608	P.E.=0.023 T.E.=1.116	Highway Purposes Construction Purposes	07-29-203-001 07-29-203-002 07-29-203-003 07-29-203-004 07-29-203-025 07-29-203-124	

REVISION DATE April 19, 2013 REVISION Revised Railroad Name MADE BY



SEE SHEET 5

See Sheet 5 for Parcel 1JY0017P.E.

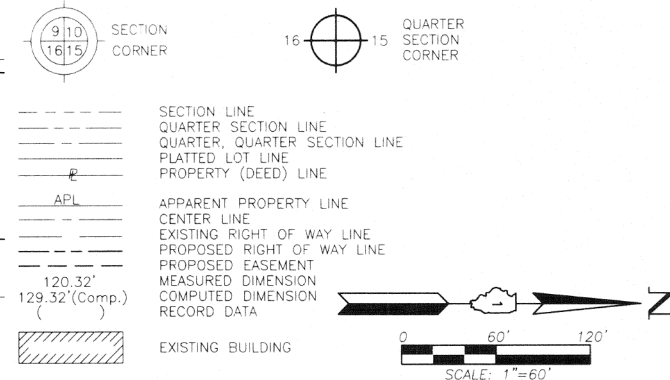
See Sheets 13 & 14 for Total Holdings Parcel 1JY0017P.E. & 1JY0017T.E.

WILLOW SHORES  
 Recorded August 14, 1979  
 as Document No. R79-72361

WILLOW SHORES  
 Recorded August 14, 1979  
 as Document No. R79-72361

VILLAGE BAPTIST CHURCH RESUBDIVISION  
 Recorded May 31, 1980  
 as Document No. R90-065512

LEGEND



- Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.
- IRON PIPE OR ROD FOUND
  - ⊕ "MAG" NAIL SET
  - + CUT CROSS FOUND OR SET
  - 5/8" REBAR SET
  - T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8" INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - T2
  - T3
  - BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8" INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - BT2
  - BT3
  - STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
  - M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8" INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
  - RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS } SS  
 COUNTY OF LAKE } SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREIN IN SECTION 29, TOWNSHIP 38N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, DuPAGE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS 27<sup>th</sup> DAY OF JUNE, 2013 A.D.

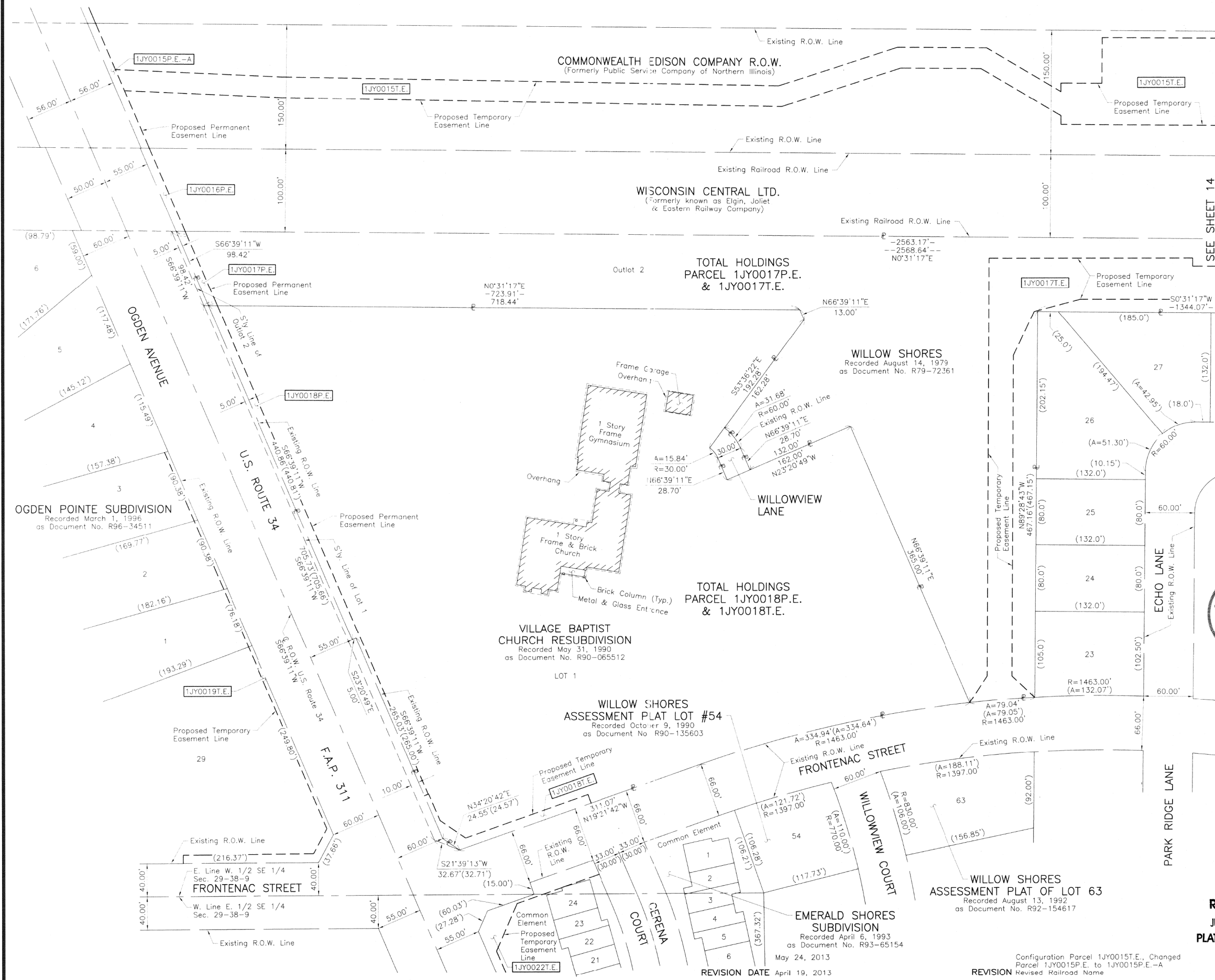


*Christian H. Jorgensen* PRESIDENT  
 ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797  
 LICENSE EXPIRATION DATE: NOVEMBER 30, 2012  
 THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

JORGENSEN & ASSOCIATES, INC.  
 120 PARK AVENUE  
 LAKE VILLA, ILLINOIS 60046 SHEET 1 IS A COVER  
 (847) 356-3371 SHEET AND IS NOT RECORDED.

**PLAT OF HIGHWAYS**  
 STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 F.A.P. 311 (U.S. ROUTE 34)  
 SECTION 652-A DuPAGE COUNTY  
 PROJECT JOB NO. R-91-014-11  
 STATION NONE TO STATION  
 SCALE: 1"=60' SHEET 13 OF 18

**BUREAU OF LAND ACQUISITION**  
 201 WEST CENTER COURT  
 SCHAUMBURG, ILLINOIS 60196 SHEET 129 OF 383



SEE SHEET 14

PARK RIDGE LANE

RECEIVED  
 JUN 07 2013  
 PLATS & LEGALS

MADE BY

Configuration Parcel 1JY0015T.E., Changed Parcel 1JY0015P.E. to 1JY0015P.E.-A  
 REVISION Revised Railroad Name

REVISION DATE April 19, 2013

EMERALD SHORES SUBDIVISION  
 Recorded August 6, 1993  
 as Document No. R93-65154  
 May 24, 2013

WILLOW SHORES ASSESSMENT PLAT OF LOT 63  
 Recorded August 13, 1992  
 as Document No. R92-154617

WILLOW SHORES ASSESSMENT PLAT LOT #54  
 Recorded October 9, 1990  
 as Document No. R90-135603

VILLAGE BAPTIST CHURCH RESUBDIVISION  
 Recorded May 31, 1990  
 as Document No. R90-065512

TOTAL HOLDINGS PARCEL 1JY0018P.E. & 1JY0018T.E.

WILLOWVIEW LANE

WILLOW SHORES  
 Recorded August 14, 1979  
 as Document No. R79-72361

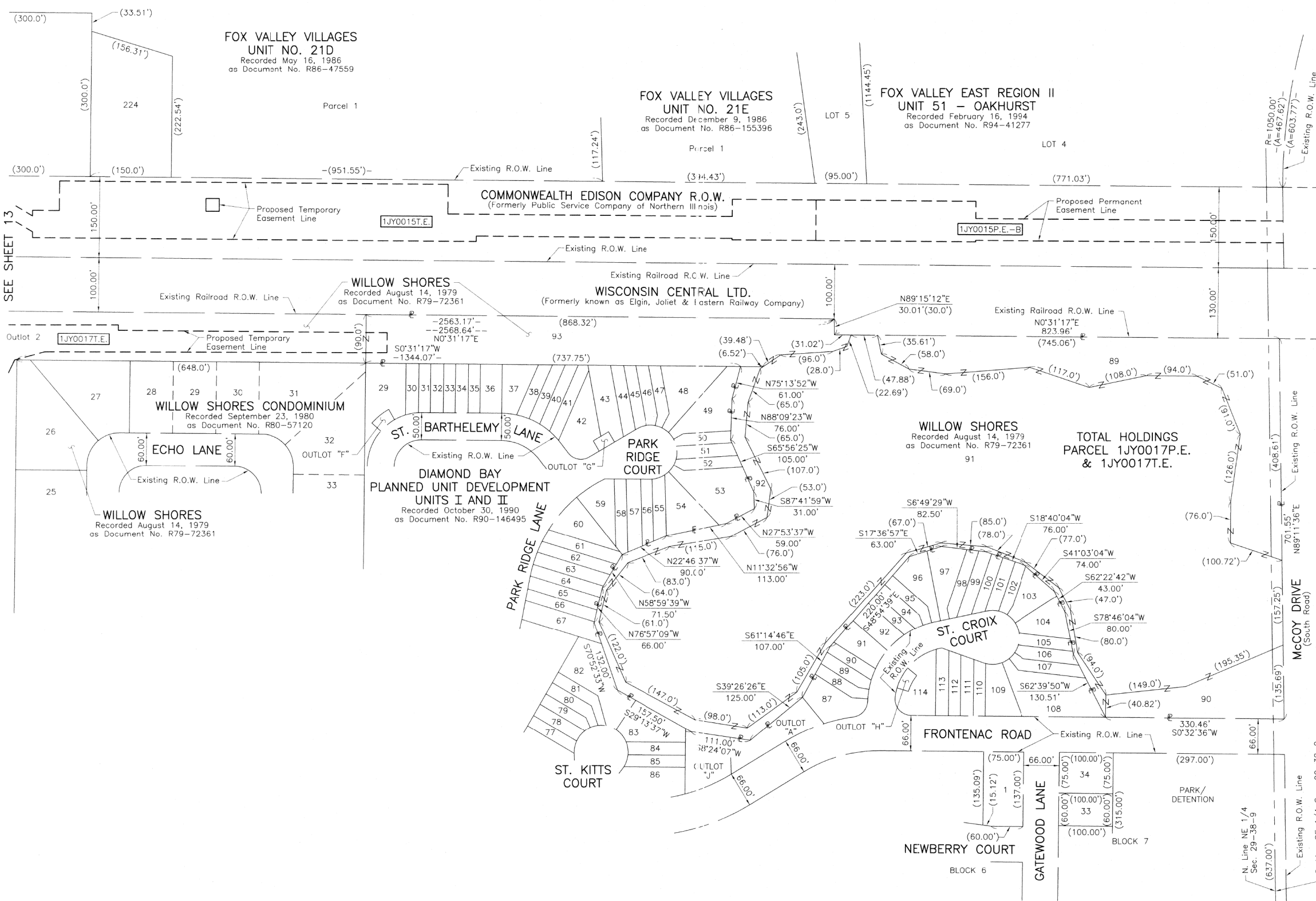
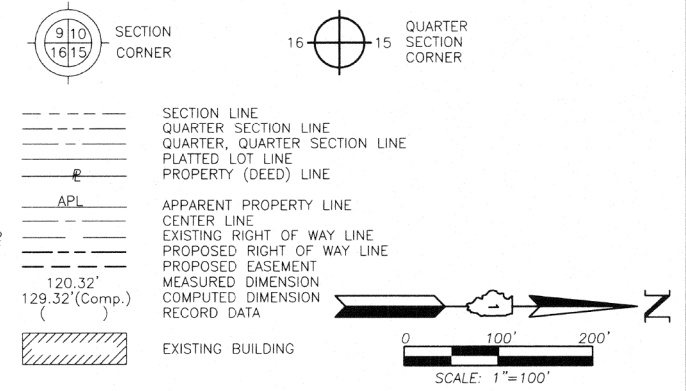
TOTAL HOLDINGS PARCEL 1JY0017P.E. & 1JY0017T.E.

WISCONSIN CENTRAL LTD.  
 (Formerly known as Elgin, Joliet & Eastern Railway Company)

COMMONWEALTH EDISON COMPANY R.O.W.  
 (Formerly Public Service Company of Northern Illinois)

PART OF THE SE 1/4 OF SEC. 20 AND PART OF THE NE 1/4 OF SEC. 29, TWP. 38 N., R. 9 E. OF THE 3RD. P.M., IN DuPAGE COUNTY, ILLINOIS.

**LEGEND**



- Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.
- IRON PIPE OR ROD FOUND
  - ⊕ "MAG" NAIL SET
  - + CUT CROSS FOUND OR SET
  - 5/8" REBAR SET
  - T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - T2
  - T3
  - BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - BT2
  - BT3
  - STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
  - M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
  - ⊙ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
  - RIGHT OF WAY STAKING PROPOSED TO BE SET.

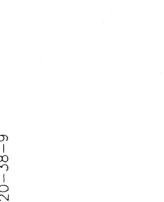
STATE OF ILLINOIS }  
 COUNTY OF LAKE }SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, AVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON BETWEEN SECTION 20, TOWNSHIP 38N., RANGE 9E. AND SECTION 29, TOWNSHIP 38N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, DuPAGE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS 28<sup>th</sup> DAY OF June 20 2013 A.D.



*Christian H. Jorgensen* PRESIDENT  
 ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797  
 LICENSE EXPIRATION DATE: NOVEMBER 30, 2012  
 THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.



JORGENSEN & ASSOCIATES, INC.  
 120 PARK AVENUE  
 LAKE VILLA, ILLINOIS 60046 SHEET 1 IS A COVER SHEET AND IS NOT RECORDED.  
 (847) 356-3371

**PLAT OF HIGHWAYS**  
 STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 F.A.P. 311 (U.S. ROUTE 34)

SECTION 652-A DuPAGE COUNTY  
 PROJECT JOB NO. R-91-014-11  
 STATION NONE TO STATION  
 SCALE: 1"=100' SHEET 14 OF 18

**BUREAU OF LAND ACQUISITION**  
 201 WEST CENTER COURT  
 SCHAUMBURG, ILLINOIS 60196

RECEIVED JUN 07 2013 PLATS & LEGALS MADE BY

SHEET 130 OF 383

May 24, 2013  
 REVISION DATE April 19, 2013

Configuration Parcel 1JY0015T.E.,  
 Added Parcel 1JY0015P.E.-B  
 Revised Railroad Name

PART OF THE SE 1/4 OF SEC. 29, TWP. 38 N., R. 9 E. OF THE 3RD. P.M., IN DuPAGE COUNTY, ILLINOIS.

LAUREL RIDGE  
UNIT 1B  
Recorded June 23, 1989  
as Document No. R89-074554

LEGEND

SECTION CORNER  
QUARTER SECTION CORNER

SECTION LINE  
QUARTER SECTION LINE  
QUARTER, QUARTER SECTION LINE  
PLATTED LOT LINE  
PROPERTY (DEED) LINE

APL  
APPARENT PROPERTY LINE  
CENTER LINE  
EXISTING RIGHT OF WAY LINE  
PROPOSED RIGHT OF WAY LINE  
PROPOSED EASEMENT  
MEASURED DIMENSION  
COMPUTED DIMENSION  
RECORD DATA

EXISTING BUILDING

Scale: 1"=40'

Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.

○ IRON PIPE OR ROD FOUND  
+ CUT CROSS FOUND OR SET

● T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.  
● BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.  
● BT2  
● BT3

■ STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.  
■ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

⊕ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)  
□ RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }  
COUNTY OF LAKE }SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 29, TOWNSHIP 38N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, DuPAGE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS 28th DAY OF June 2012 A.D.



Christian H. Jorgensen PRESIDENT  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797  
LICENSE EXPIRATION DATE: NOVEMBER 30, 2012  
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

JORGENSEN & ASSOCIATES, INC.  
120 PARK AVENUE  
LAKE VILLA, ILLINOIS 60046  
(847) 356-3371

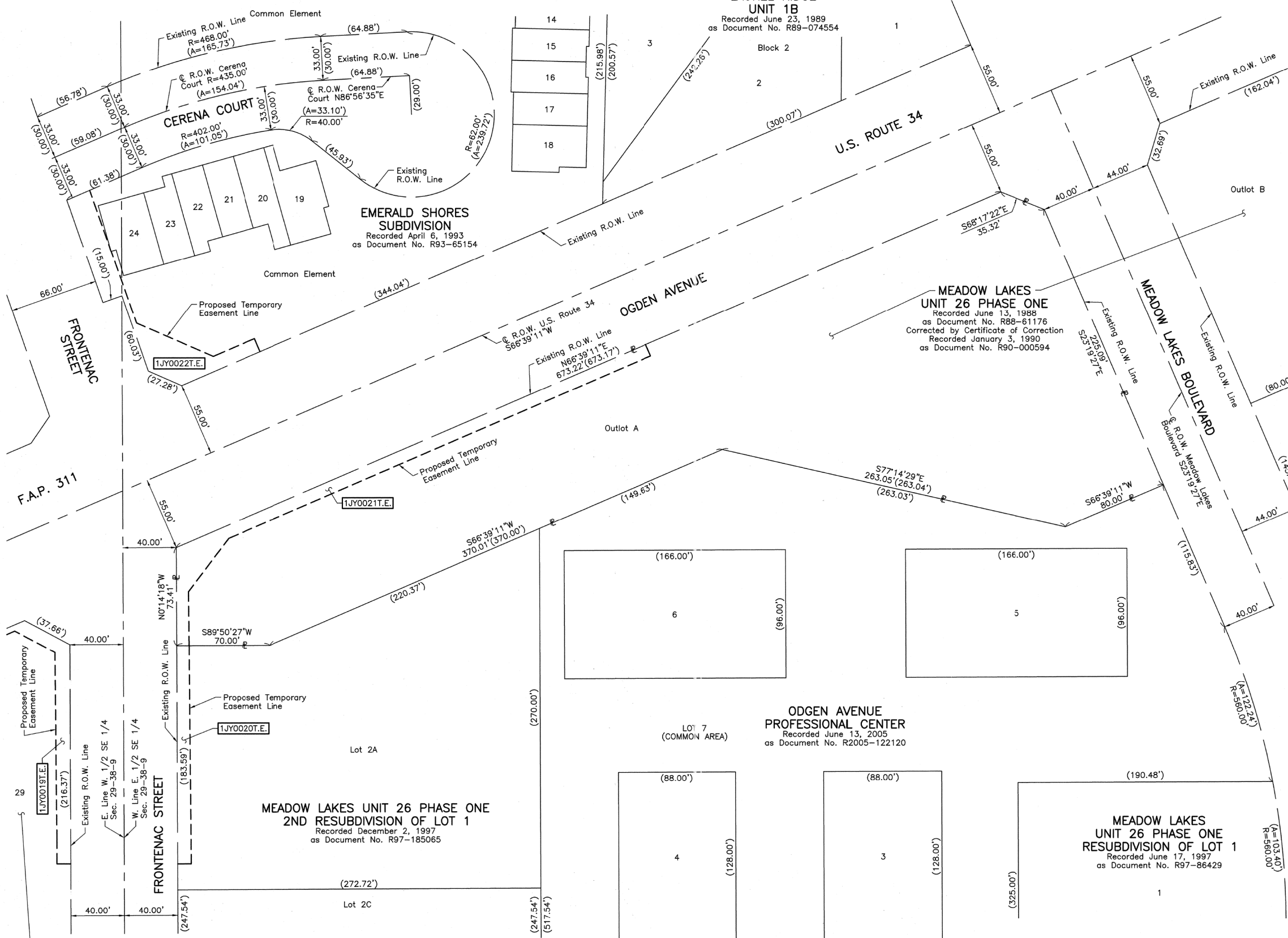
SHEET 1 IS A COVER SHEET AND IS NOT RECORDED.

PLAT OF HIGHWAYS  
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
F.A.P. 311 (U.S. ROUTE 34)

SECTION 652-A DuPAGE COUNTY  
PROJECT JOB NO. R-91-014-11  
STATION NONE TO STATION  
SCALE: 1"=40' SHEET 15 OF 18

BUREAU OF LAND ACQUISITION  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196

SHEET 131 OF 383



PART OF THE EAST 1/2 OF SEC. 29, TWP. 38 N., R. 9 E. OF THE 3RD. P.M., IN DuPAGE COUNTY, ILLINOIS.

VILLAGE BAPTIST CHURCH RESUBDIVISION

OGDEN POINTE SUBDIVISION

29

Proposed Temporary Easement Line

Existing R.O.W. Line

E. Line W. 1/2 SE 1/4 Sec. 29-38-9

W. Line E. 1/2 SE 1/4 Sec. 29-38-9

FRONTENAC STREET

Proposed Temporary Easement Line

Existing R.O.W. Line

(183.59')

Proposed Temporary Easement Line

Existing R.O.W. Line

Lot 2A

Proposed Temporary Easement Line

MEADOW LAKES UNIT 26 PHASE ONE 2ND RESUBDIVISION OF LOT 1

Recorded December 2, 1997 as Document No. R97-185065

Proposed Temporary Easement Line

Existing R.O.W. Line

MEADOW LAKES UNIT 26 PHASE ONE

Recorded June 13, 1988 as Document No. R88-61176

Corrected by Certificate of Correction Recorded January 3, 1990 as Document No. R90-000594

Outlot A

Proposed Temporary Easement Line

Existing R.O.W. Line

MEADOW LAKES UNIT 26 PHASE ONE

Recorded June 13, 1988 as Document No. R88-61176

Corrected by Certificate of Correction Recorded January 3, 1990 as Document No. R90-000594

Outlot A

Proposed Temporary Easement Line

Existing R.O.W. Line

LOT 7 (COMMON AREA)

ODGEN AVENUE PROFESSIONAL CENTER

Recorded June 13, 2005 as Document No. R2005-122120

ROUTE F.A.P. 311 (U.S. ROUTE 34)

SECTION 652-A

COUNTY DuPAGE

JOB NO. R-91-014-11

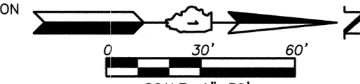
RECORDING: RECORDED ON

AS DOCUMENT NO.

LEGEND



- SECTION LINE
QUARTER SECTION LINE
QUARTER, QUARTER SECTION LINE
PLATTED LOT LINE
PROPERTY (DEED) LINE
APL
APPARENT PROPERTY LINE
CENTER LINE
EXISTING RIGHT OF WAY LINE
PROPOSED RIGHT OF WAY LINE
PROPOSED EASEMENT
MEASURED DIMENSION
COMPUTED DIMENSION
RECORD DATA
EXISTING BUILDING



- Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.
IRON PIPE OR ROD FOUND
CUT CROSS FOUND OR SET
THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
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PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS
COUNTY OF LAKE
THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 29, TOWNSHIP 38N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, DuPAGE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RE-TRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.
DATED AT LAKE VILLA, ILLINOIS THIS 28th DAY OF June 20, 2012 A.D.



CHRISTIAN H. JORGENSEN
2787 PROFESSIONAL LAND SURVEYOR STATE OF ILLINOIS
LAKE VILLA, ILLINOIS
PRESIDENT
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797
LICENSE EXPIRATION DATE: NOVEMBER 30, 2012
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

WILLOW SHORES ASSESSMENT PLAT OF LOT #56
Recorded June 19, 1991 as Document No. R91-074945

WILLOW SHORES ASSESSMENT PLAT OF LOT #57
Recorded December 20, 1991 as Document No. R91-171508

JORGENSEN & ASSOCIATES, INC.
120 PARK AVENUE
LAKE VILLA, ILLINOIS 60046
(847) 356-3371
SHEET 1 IS A COVER SHEET AND IS NOT RECORDED.

PLAT OF HIGHWAYS STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.P. 311 (U.S. ROUTE 34)
SECTION 652-A DuPAGE COUNTY
PROJECT JOB NO. R-91-014-11
STATION NONE TO STATION
SCALE: 1"=30' SHEET 16 OF 18

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAMBURG, ILLINOIS 60196
RECEIVED NOV 09 2012 PLATS & LEGALS
MADE BY

REVISION DATE

REVISION

MADE BY

REVISION DATE

REVISION

MADE BY

REVISION DATE

REVISION

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REVISION DATE

REVISION

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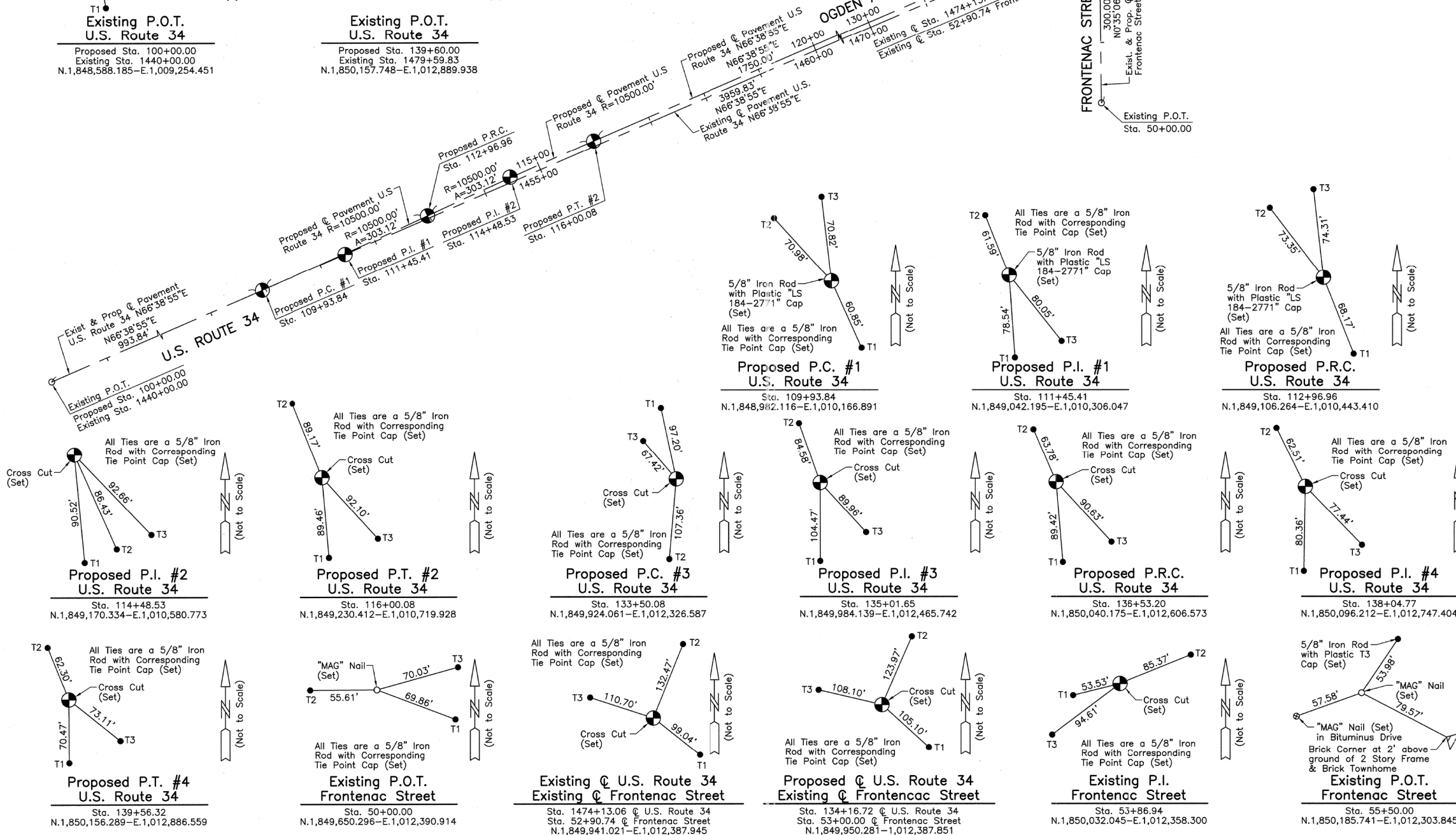
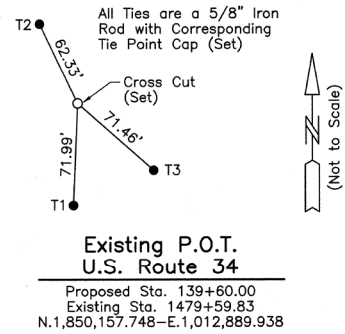
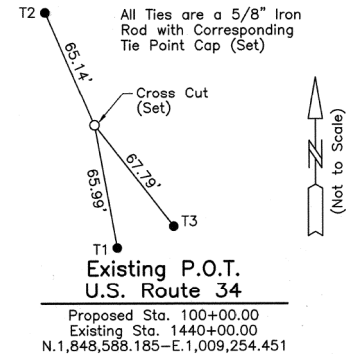
REVISION DATE

REVISION

MADE BY



Proposed Pavement U.S. Route 34 Curve #1	Proposed Pavement U.S. Route 34 Curve #2	Proposed Pavement U.S. Route 34 Curve #3	Proposed Pavement U.S. Route 34 Curve #4
P.I. = Sta. 111+45.41	P.I. = Sta. 114+48.53	P.I. = Sta. 135+01.65	P.I. = Sta. 138+04.77
Δ = 1°39'15"	Δ = 1°39'15"	Δ = 1°39'15"	Δ = 1°39'15"
R = 10500.00'	R = 10500.00'	R = 10500.00'	R = 10500.00'
T = 151.57'	T = 151.57'	T = 151.57'	T = 151.57'
L = 303.12'	L = 303.12'	L = 303.12'	L = 303.12'
E = 1.09'	E = 1.09'	E = 1.09'	E = 1.09'
P.C. = Sta. 109+93.84	P.R.C. = Sta. 112+96.96	P.C. = Sta. 133+50.08	P.R.C. = Sta. 136+53.20
P.R.C. = Sta. 112+96.96	P.T. = Sta. 116+00.08	P.R.C. = Sta. 136+53.20	P.T. = Sta. 139+56.32



### LEGEND

SECTION CORNER: 910/1615

QUARTER SECTION CORNER: 16/15

SECTION LINE

QUARTER SECTION LINE

QUARTER, QUARTER SECTION LINE

PLATTED LOT LINE

PROPERTY (DEED) LINE

APL

APPARENT PROPERTY LINE

CENTER LINE

EXISTING RIGHT OF WAY LINE

PROPOSED RIGHT OF WAY LINE

PROPOSED EASEMENT

MEASURED DIMENSION

COMPUTED DIMENSION

RECORD DATA

EXISTING BUILDING

Scale: 1"=100'

Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.

○ IRON PIPE OR ROD FOUND      ⊕ "MAG" NAIL SET

+ CUT CROSS FOUND OR SET      ● 5/8" REBAR SET

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⊕ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)

□ RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS }  
COUNTY OF LAKE }SS

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM, LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 29, TOWNSHIP 38N., RANGE 9E., OF THE THIRD PRINCIPAL MERIDIAN, DuPAGE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RE-TRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS 23<sup>RD</sup> DAY OF October 2012 A.D.

CHRISTIAN H. JORGENSEN  
2797 PROFESSIONAL LAND SURVEYOR  
STATE OF ILLINOIS  
LAKE VILLA, ILLINOIS

Christian H. Jorgensen PRESIDENT  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797  
LICENSE EXPIRATION DATE: NOVEMBER 30, 2012  
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.  
Note: Surface Coordinates are Shown.

RECEIVED  
NOV 09 2012  
PLATS & LEGALS

JORGENSEN & ASSOCIATES, INC.  
120 PARK AVENUE  
LAKE VILLA, ILLINOIS 60046  
(847) 356-3371

SHEET 1 IS A COVER SHEET AND IS NOT RECORDED.

### PLAT OF HIGHWAYS

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
F.A.P. 311 (U.S. ROUTE 34)

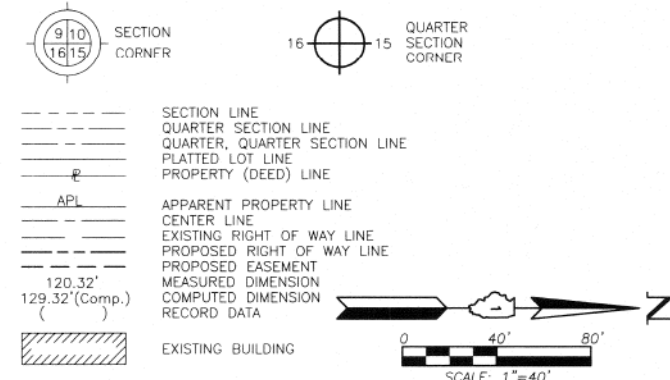
SECTION 652-A DuPAGE COUNTY  
PROJECT JOB NO. R-91-014-11  
STATION 50+00.00 TO STATION 55+50.00  
STATION 100+00.00 TO STATION 139+60.00  
STATION 1440+00.00 TO STATION 1479+59.83  
SCALE: 1"=100' SHEET 17 OF 18

BUREAU OF LAND ACQUISITION  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196

REVISION DATE REVISION MADE BY

PART OF THE SE 1/4 OF SEC. 20 AND PART OF THE NE 1/4 OF SEC. 29, TWP. 38 N., R. 9 E. OF THE 3RD. P.M., IN DuPAGE COUNTY, ILLINOIS.

LEGEND



Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.

- IRON PIPE OR ROD FOUND
- ⊕ "MAG" NAIL SET
- + CUT CROSS FOUND OR SET
- 5/8" REBAR SET
- T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- T2
- T3
- BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
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STATE OF ILLINOIS }  
COUNTY OF LAKE }SS

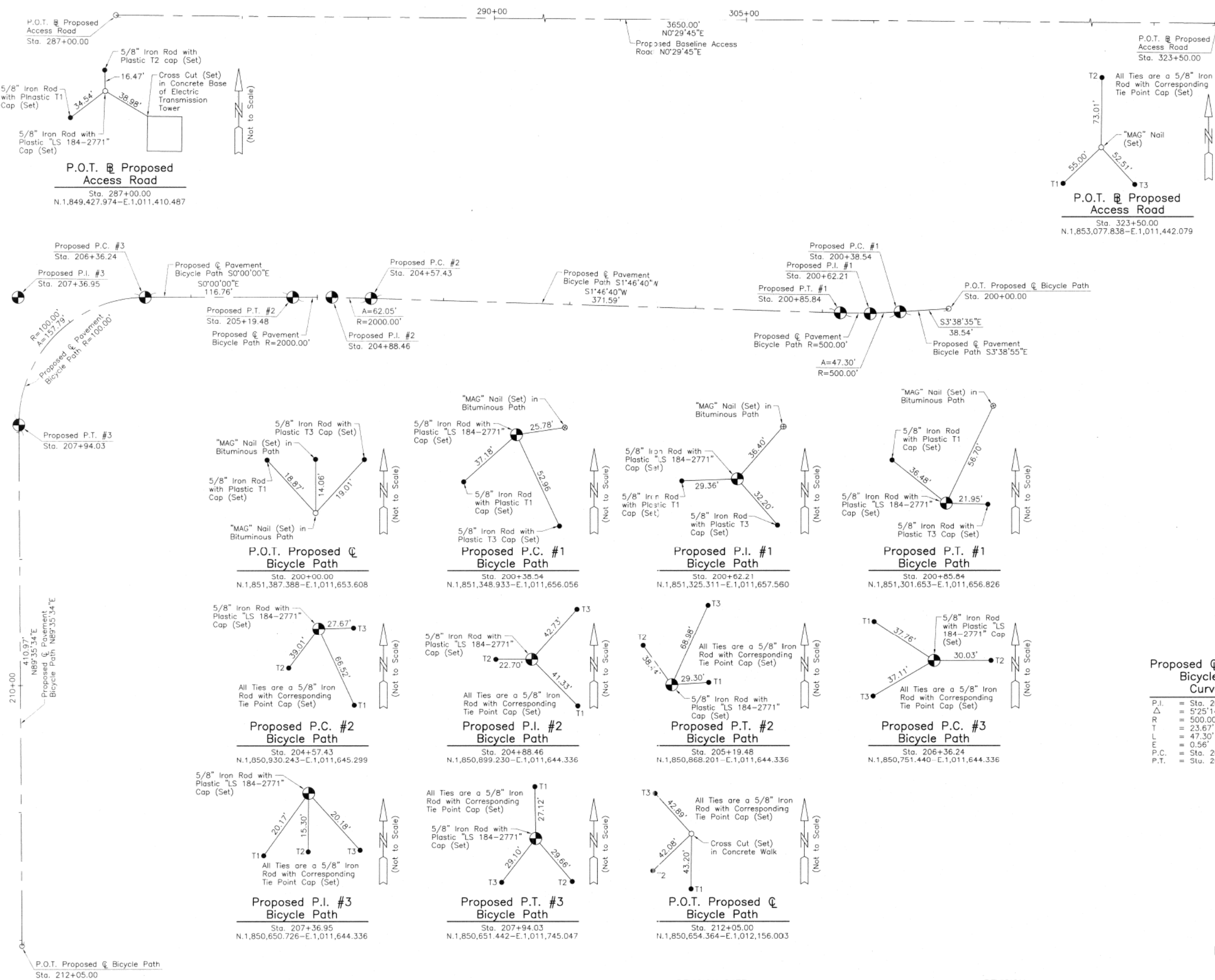
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DATED AT LAKE VILLA, ILLINOIS THIS 25<sup>TH</sup> DAY OF October 2013 A.D.



CHRISTIAN H. JORGENSEN  
PRESIDENT  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797  
LICENSE EXPIRATION DATE: NOVEMBER 30, 2012  
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.  
Note: Surface Coordinates are Shown.

Proposed Pavement Bicycle Path Curve #1	Proposed Pavement Bicycle Path Curve #2	Proposed Pavement Bicycle Path Curve #3
P.I. = Sta. 200+62.21	P.I. = Sta. 204+88.46	P.I. = Sta. 207+36.95
Δ = 5°25'14"	Δ = 1°46'40"	Δ = 90°24'26"
R = 500.00'	R = 2000.00'	R = 100.00'
T = 23.67'	T = 31.03'	T = 100.71'
L = 47.30'	L = 62.05'	L = 157.79'
E = 0.56'	E = 0.24'	E = 41.93'
P.C. = Sta. 200+38.54	P.C. = Sta. 204+57.43	P.C. = Sta. 206+36.24
P.T. = Sta. 200+85.84	P.T. = Sta. 205+19.48	P.T. = Sta. 207+94.03



RECEIVED  
JUN 07 2013  
PLATS & LEGALS

MADE BY

JORGENSEN & ASSOCIATES, INC.  
120 PARK AVENUE  
LAKE VILLA, ILLINOIS 60046  
(847) 356-3371

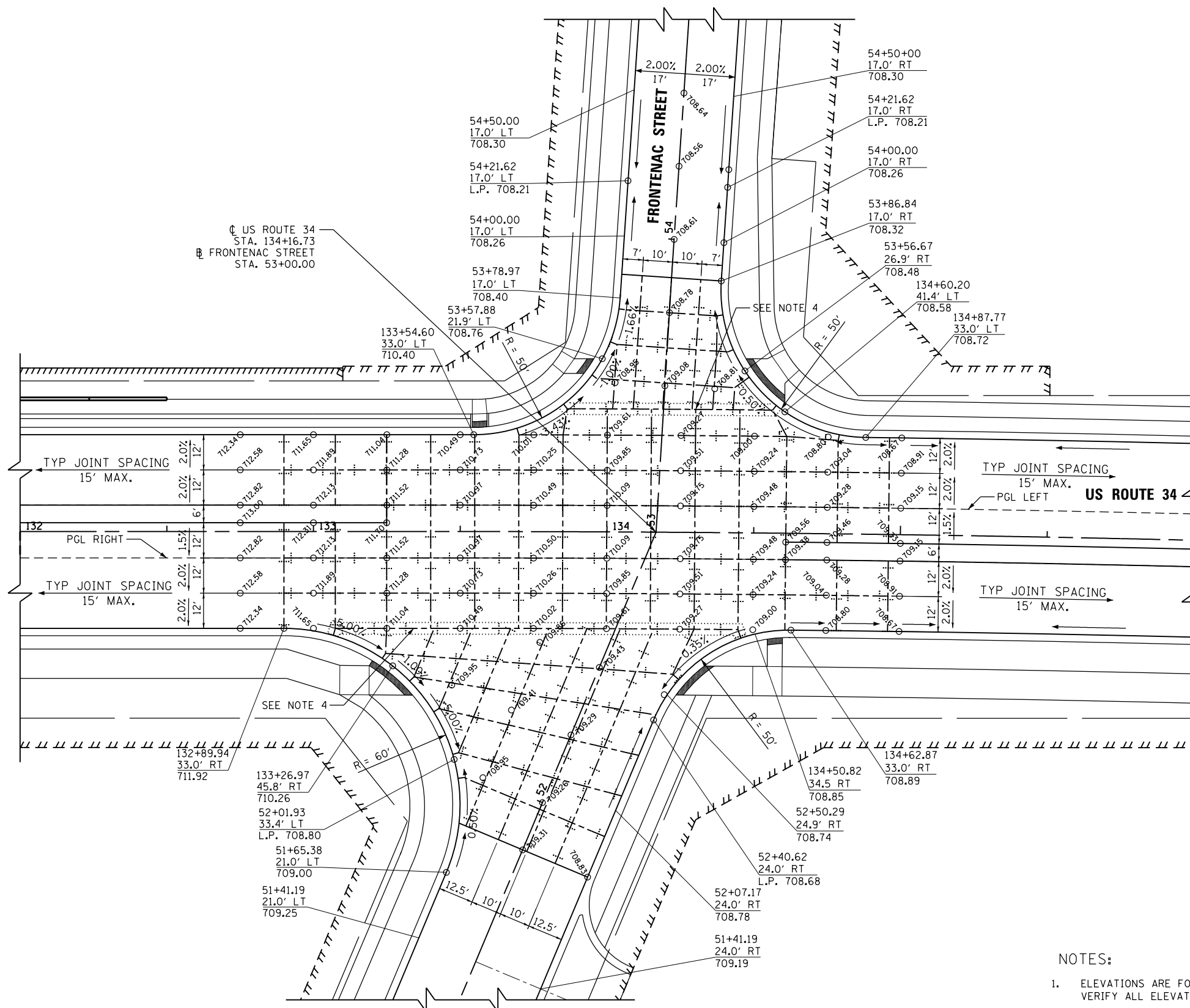
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SHEET AND IS NOT RECORDED.

**PLAT OF HIGHWAYS**  
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
F.A.P. 311 (U.S. ROUTE 34)

SECTION 652-A DuPAGE COUNTY  
PROJECT JOB NO. R-91-014-11  
STATION 200+00.00 TO STATION 212+05.00  
STATION 287+00.00 TO STATION 323+50.00  
SCALE: 1"=40' SHEET 18 OF 18

BUREAU OF LAND ACQUISITION  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196

REVISION DATE May 24, 2013  
REVISION Changed Access Road to



- NOTES:**
- ELEVATIONS ARE FOR INFORMATION ONLY. CONTRACTOR SHALL VERIFY ALL ELEVATIONS PRIOR TO CONSTRUCTION.
  - REFER TO STANDARDS 420001, 420101, 420106 FOR JOINT DETAILS AND SPACING.
  - FOR CURB RAMPS FOR SIDEWALK REFER TO STANDARDS 424001, 424006, 424011, 424021, 424026 AND/OR AS DIRECTED BY THE ENGINEER.
  - PAVEMENT SEPERATION JOINT IN ACCORDANCE WITH BD52.

FILE NAME = Q:\1\DOT\_25368367\_US\_34.ctb CN Ph2\Drawings\Roadway\Intersection\168706E-sht-Intersec.dgn



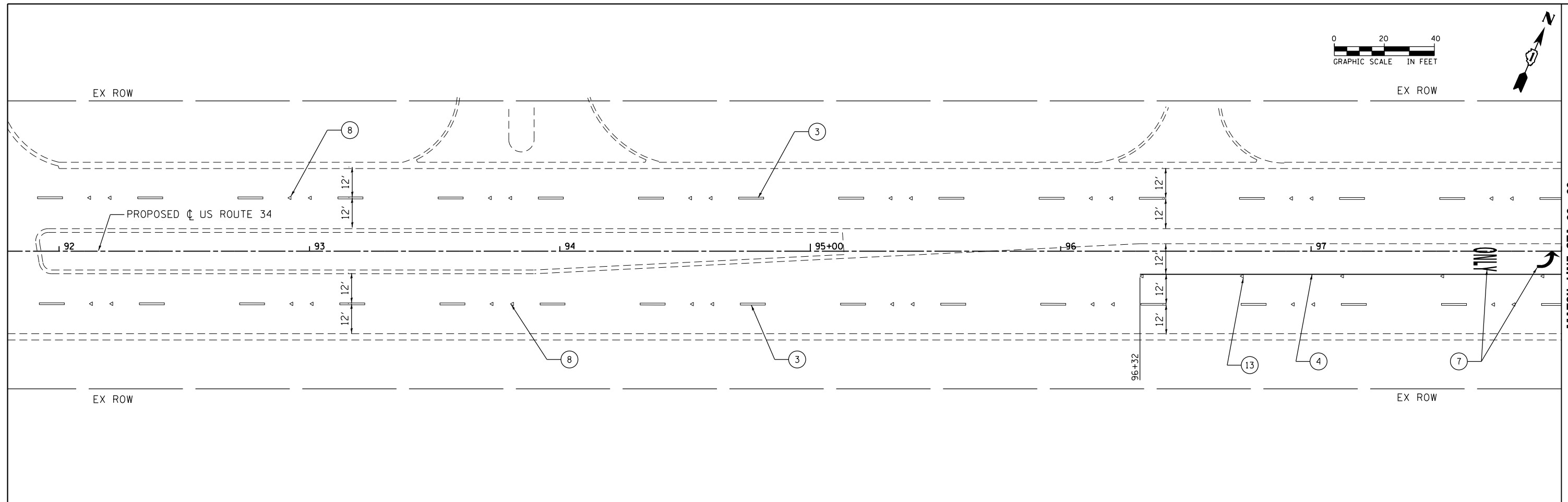
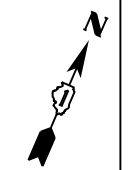
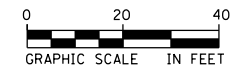
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**FRONTENAC STREET  
INTERSECTION DETAIL**

SCALE: 20 SHEET NO. 1 OF 1 SHEETS STA. 131+00 TO STA. 137+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	135
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**PAVEMENT MARKING LEGEND**

- ① POLYUREA PAVEMENT MARKING TYPE 1- LINE 4" (SOLID YELLOW)
- ② POLYUREA PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW, 11" C-C)
- ③ POLYUREA PAVEMENT MARKING - LINE 4" (WHITE SKIP DASH, 10' LINE 30' SPACE)
- ④ POLYUREA PAVEMENT MARKING - LINE 6" (SOLID WHITE)
- ⑤ POLYUREA PAVEMENT MARKING TYPE 1 - LINE 4" (SOLID WHITE)
- ⑥ POLYUREA PAVEMENT MARKING - LINE 24" (SOLID WHITE STOP BAR)
- ⑦ POLYUREA PAVEMENT MARKING - LETTERS AND SYMBOLS (WHITE)
- ⑧ RAISED REFLECTIVE PAVEMENT MARKER OR RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE) (CRYSTAL 1-WAY 2 EACH @ 80' C-C)
- ⑨ THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW, 11" C-C)
- ⑩ THERMOPLASTIC PAVEMENT MARKING - LINE 6" (SOLID WHITE)
- ⑪ THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE 2' SKIP, 6' DASH)
- ⑫ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (WHITE)
- ⑬ RAISED REFLECTIVE PAVEMENT MARKER (CRYSTAL 1-WAY @ 40' C-C)

**PAVEMENT MARKING NOTES**

1. FOR PCC PAVEMENT: POLYUREA PAVEMENT MARKING TYPE 1 SHALL BE USED FOR FINAL PAVEMENT MARKINGS OF THE SIZE AND COLOR SPECIFIED.
2. FOR HOT MIX ASPHALT PAVEMENT: THERMOPLASTIC PAVEMENT MARKING SHALL BE USED FOR FINAL PAVEMENT MARKING OF THE SIZE AND COLOR SPECIFIED.
3. FOR RAISED REFLECTIVE PAVEMENT MARKER PLACEMENT, SPACING AND COLOR SEE DISTRICT 1 STANDARD TC-11.

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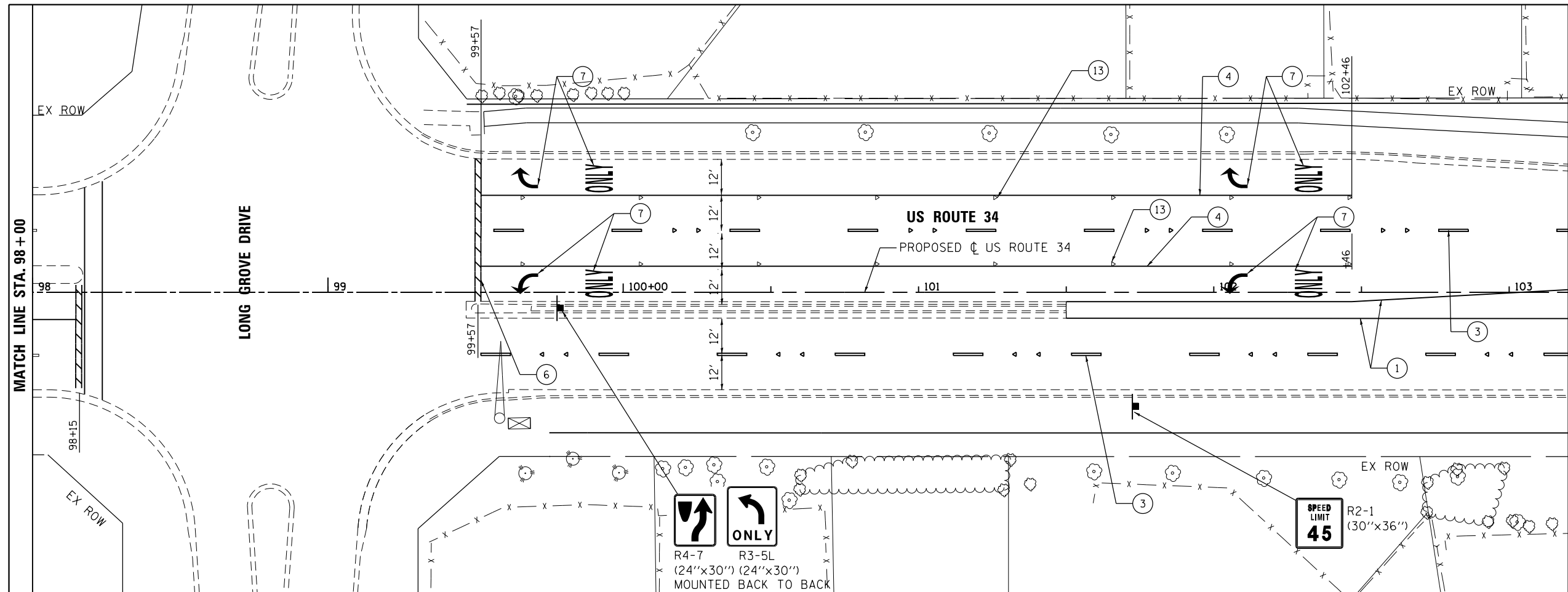
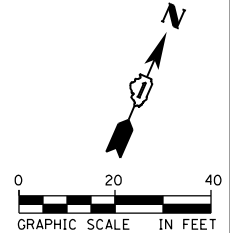
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

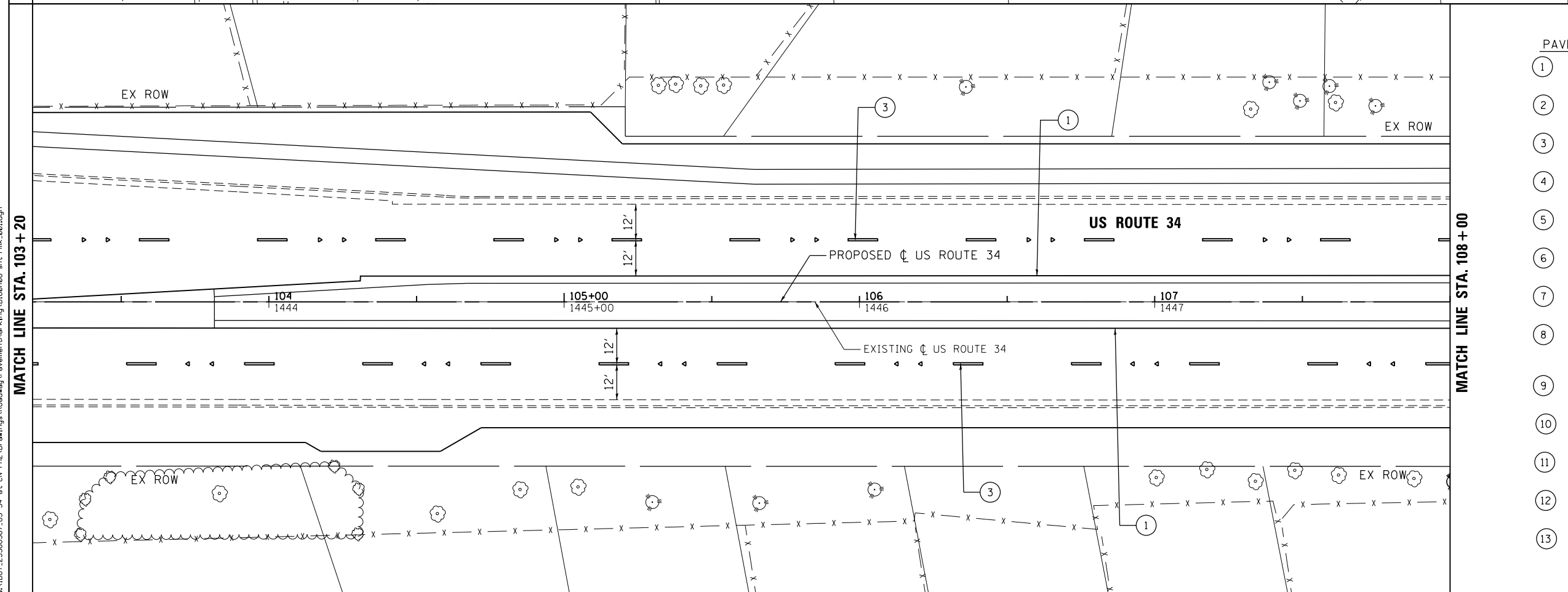
**PAVEMENT MARKING PLAN  
US ROUTE 34**

SCALE: 20 SHEET NO. 1 OF 7 SHEETS STA. 92+00 TO STA. 98+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	136
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- PAVEMENT MARKING NOTES**
- FOR PCC PAVEMENT:  
POLYUREA PAVEMENT MARKING TYPE 1 SHALL BE USED FOR FINAL PAVEMENT MARKINGS OF THE SIZE AND COLOR SPECIFIED.
  - FOR HOT MIX ASPHALT PAVEMENT:  
THERMOPLASTIC PAVEMENT MARKING SHALL BE USED FOR FINAL PAVEMENT MARKING OF THE SIZE AND COLOR SPECIFIED.
  - FOR RAISED REFLECTIVE PAVEMENT MARKER PLACEMENT, SPACING AND COLOR SEE DISTRICT 1 STANDARD TC-11.



- PAVEMENT MARKING LEGEND**
- POLYUREA PAVEMENT MARKING TYPE 1- LINE 4" (SOLID YELLOW)
  - POLYUREA PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW, 11" C-C)
  - POLYUREA PAVEMENT MARKING - LINE 4" (WHITE SKIP DASH, 10' LINE 30' SPACE)
  - POLYUREA PAVEMENT MARKING - LINE 6" (SOLID WHITE)
  - POLYUREA PAVEMENT MARKING TYPE 1 - LINE 4" (SOLID WHITE)
  - POLYUREA PAVEMENT MARKING - LINE 24" (SOLID WHITE STOP BAR)
  - POLYUREA PAVEMENT MARKING - LETTERS AND SYMBOLS (WHITE)
  - RAISED REFLECTIVE PAVEMENT MARKER OR RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE) (CRYSTAL 1-WAY 2 EACH @ 80' C-C)
  - THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW, 11" C-C)
  - THERMOPLASTIC PAVEMENT MARKING - LINE 6" (SOLID WHITE)
  - THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE 2' SKIP, 6' DASH)
  - THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (WHITE)
  - RAISED REFLECTIVE PAVEMENT MARKER (CRYSTAL 1-WAY @ 40' C-C)

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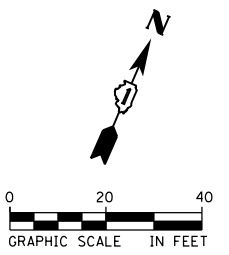
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING PLAN  
US ROUTE 34**

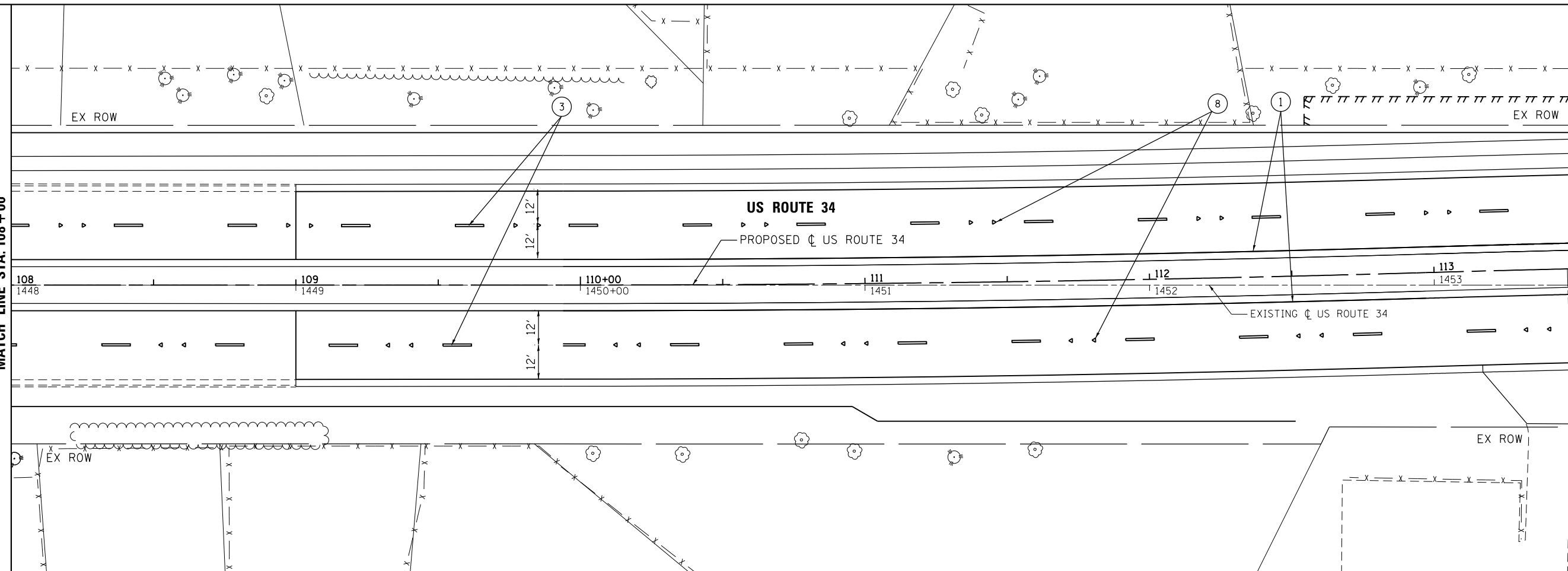
SCALE: 20    SHEET NO. 2 OF 7 SHEETS    STA. 98+00 TO STA. 108+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	137
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



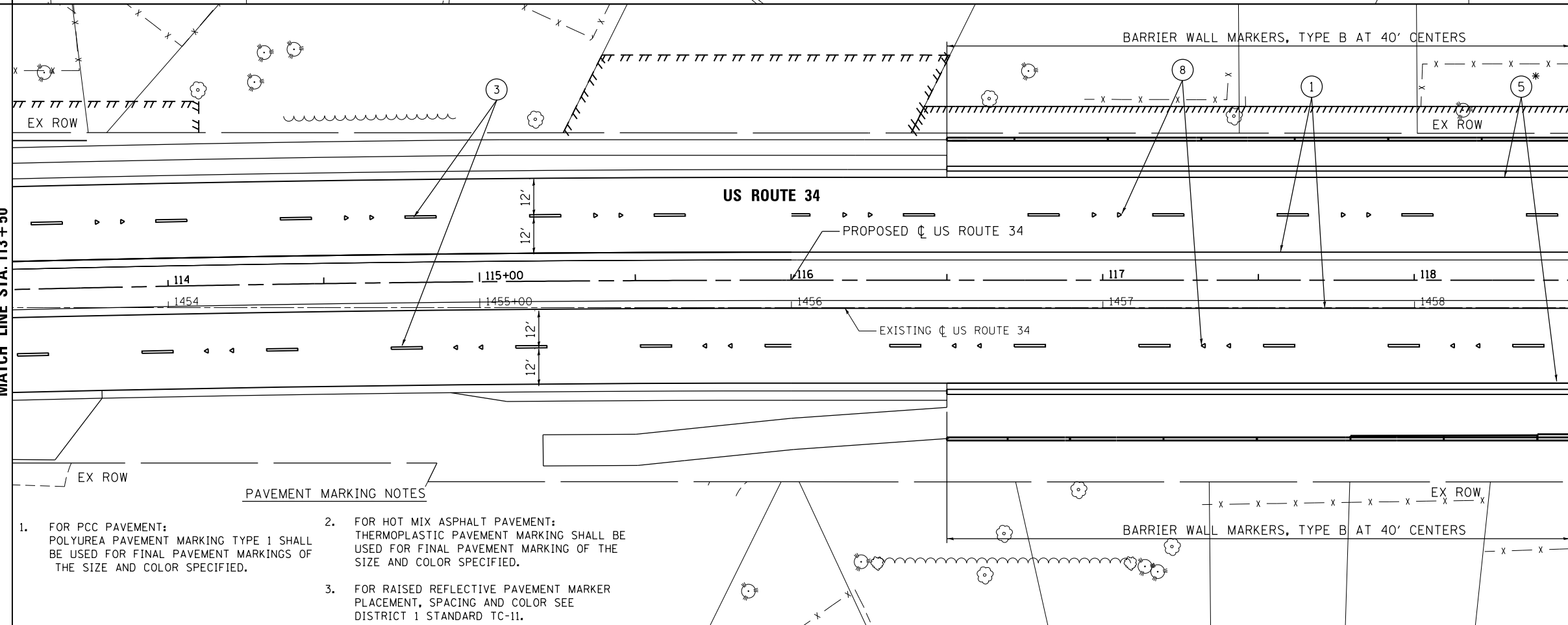
MATCH LINE STA. 108+00

MATCH LINE STA. 113+50



MATCH LINE STA. 113+50

MATCH LINE STA. 118+50



- PAVEMENT MARKING LEGEND**
- ① POLYUREA PAVEMENT MARKING TYPE 1- LINE 4" (SOLID YELLOW)
  - ② POLYUREA PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW, 11" C-C)
  - ③ POLYUREA PAVEMENT MARKING - LINE 4" (WHITE SKIP DASH, 10' LINE 30' SPACE)
  - ④ POLYUREA PAVEMENT MARKING - LINE 6" (SOLID WHITE)
  - ⑤ POLYUREA PAVEMENT MARKING TYPE 1 - LINE 4" (SOLID WHITE)
  - ⑥ POLYUREA PAVEMENT MARKING - LINE 24" (SOLID WHITE STOP BAR)
  - ⑦ POLYUREA PAVEMENT MARKING - LETTERS AND SYMBOLS (WHITE)
  - ⑧ RAISED REFLECTIVE PAVEMENT MARKER OR RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE) (CRYSTAL 1-WAY 2 EACH @ 80' C-C)
  - ⑨ THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW, 11" C-C)
  - ⑩ THERMOPLASTIC PAVEMENT MARKING - LINE 6" (SOLID WHITE)
  - ⑪ THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE 2' SKIP, 6' DASH)
  - ⑫ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (WHITE)
  - ⑬ RAISED REFLECTIVE PAVEMENT MARKER (CRYSTAL 1-WAY @ 40' C-C)

**PAVEMENT MARKING NOTES**

1. FOR PCC PAVEMENT: POLYUREA PAVEMENT MARKING TYPE 1 SHALL BE USED FOR FINAL PAVEMENT MARKINGS OF THE SIZE AND COLOR SPECIFIED.
2. FOR HOT MIX ASPHALT PAVEMENT: THERMOPLASTIC PAVEMENT MARKING SHALL BE USED FOR FINAL PAVEMENT MARKING OF THE SIZE AND COLOR SPECIFIED.
3. FOR RAISED REFLECTIVE PAVEMENT MARKER PLACEMENT, SPACING AND COLOR SEE DISTRICT 1 STANDARD TC-11.



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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

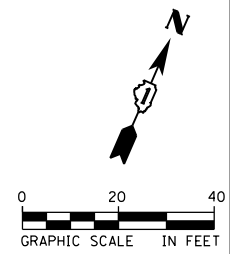
**PAVEMENT MARKING PLAN  
US ROUTE 34**

SCALE: 20    SHEET NO. 3 OF 7 SHEETS    STA. 108+00 TO STA. 118+50

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	138
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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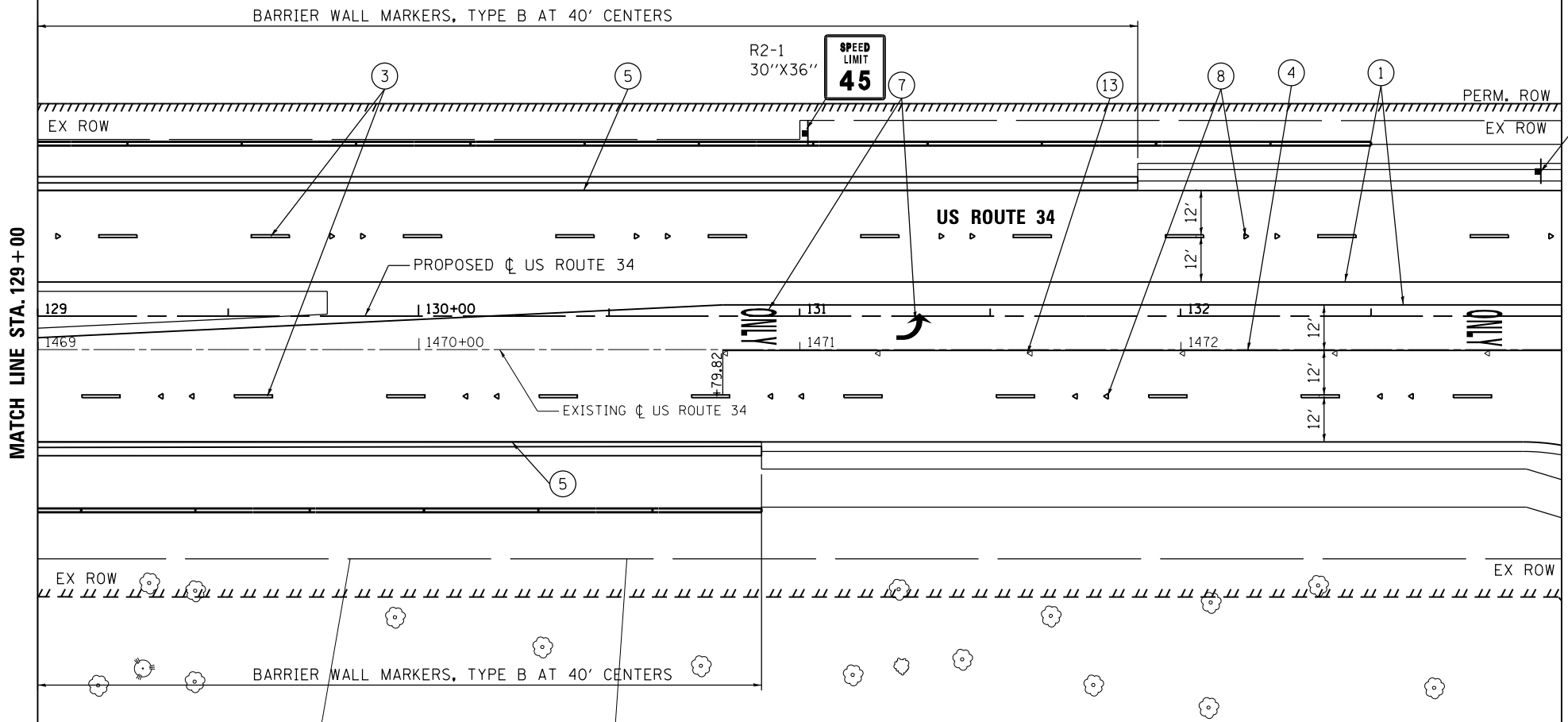




- PAVEMENT MARKING LEGEND**
- ① POLYUREA PAVEMENT MARKING TYPE 1- LINE 4" (SOLID YELLOW)
  - ② POLYUREA PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW, 11" C-C)
  - ③ POLYUREA PAVEMENT MARKING - LINE 4" (WHITE SKIP DASH, 10' LINE 30' SPACE)
  - ④ POLYUREA PAVEMENT MARKING - LINE 6" (SOLID WHITE)
  - ⑤ POLYUREA PAVEMENT MARKING TYPE 1 - LINE 4" (SOLID WHITE)
  - ⑥ POLYUREA PAVEMENT MARKING - LINE 24" (SOLID WHITE STOP BAR)
  - ⑦ POLYUREA PAVEMENT MARKING - LETTERS AND SYMBOLS (WHITE)
  - ⑧ RAISED REFLECTIVE PAVEMENT MARKER OR RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE) (CRYSTAL 1-WAY 2 EACH @ 80' C-C)
  - ⑨ THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW, 11" C-C)
  - ⑩ THERMOPLASTIC PAVEMENT MARKING - LINE 6" (SOLID WHITE)
  - ⑪ THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE 2' SKIP, 6' DASH)
  - ⑫ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (WHITE)
  - ⑬ RAISED REFLECTIVE PAVEMENT MARKER (CRYSTAL 1-WAY @ 40' C-C)

M1-4  
24"x24"  
M3-4  
24"x12"

MATCH LINE STA. 133+00



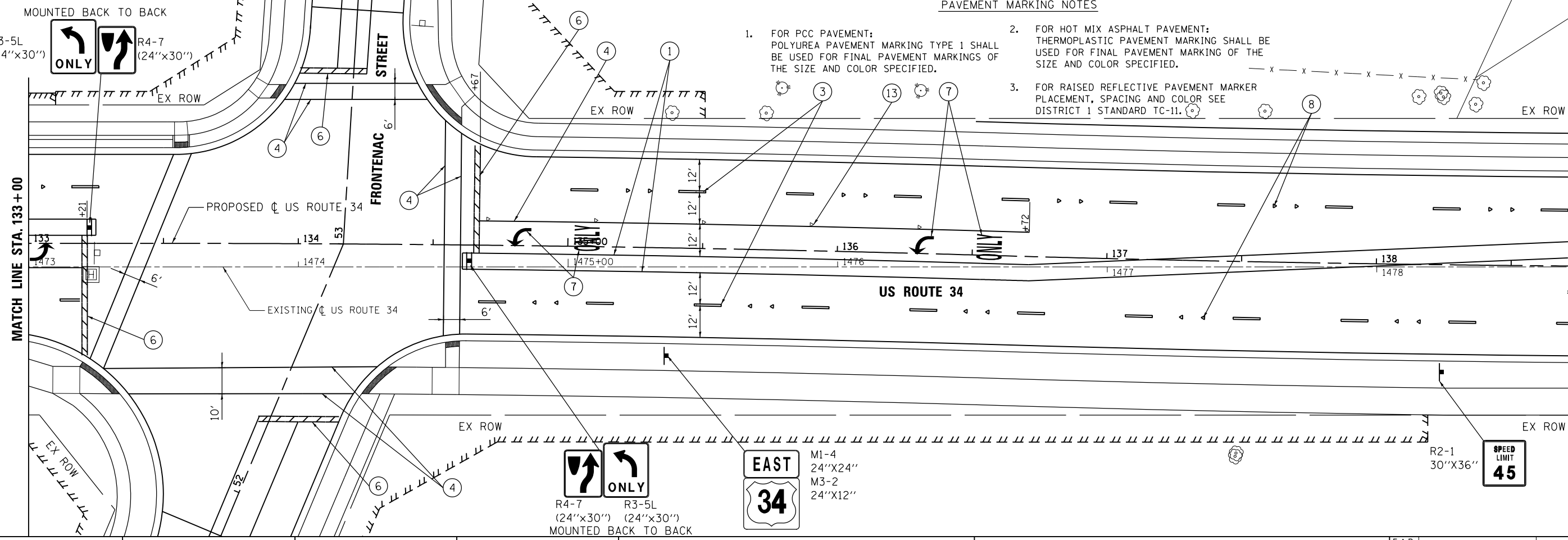
MATCH LINE STA. 129+00

**PAVEMENT MARKING NOTES**

1. FOR PCC PAVEMENT: POLYUREA PAVEMENT MARKING TYPE 1 SHALL BE USED FOR FINAL PAVEMENT MARKINGS OF THE SIZE AND COLOR SPECIFIED.
2. FOR HOT MIX ASPHALT PAVEMENT: THERMOPLASTIC PAVEMENT MARKING SHALL BE USED FOR FINAL PAVEMENT MARKING OF THE SIZE AND COLOR SPECIFIED.
3. FOR RAISED REFLECTIVE PAVEMENT MARKER PLACEMENT, SPACING AND COLOR SEE DISTRICT 1 STANDARD TC-11.

M1-4  
24"x24"  
M3-2  
24"x12"

MATCH LINE STA. 138+75



MATCH LINE STA. 133+00



USER NAME = URS	DESIGNED - MB	REVISED -
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING PLAN  
US ROUTE 34**

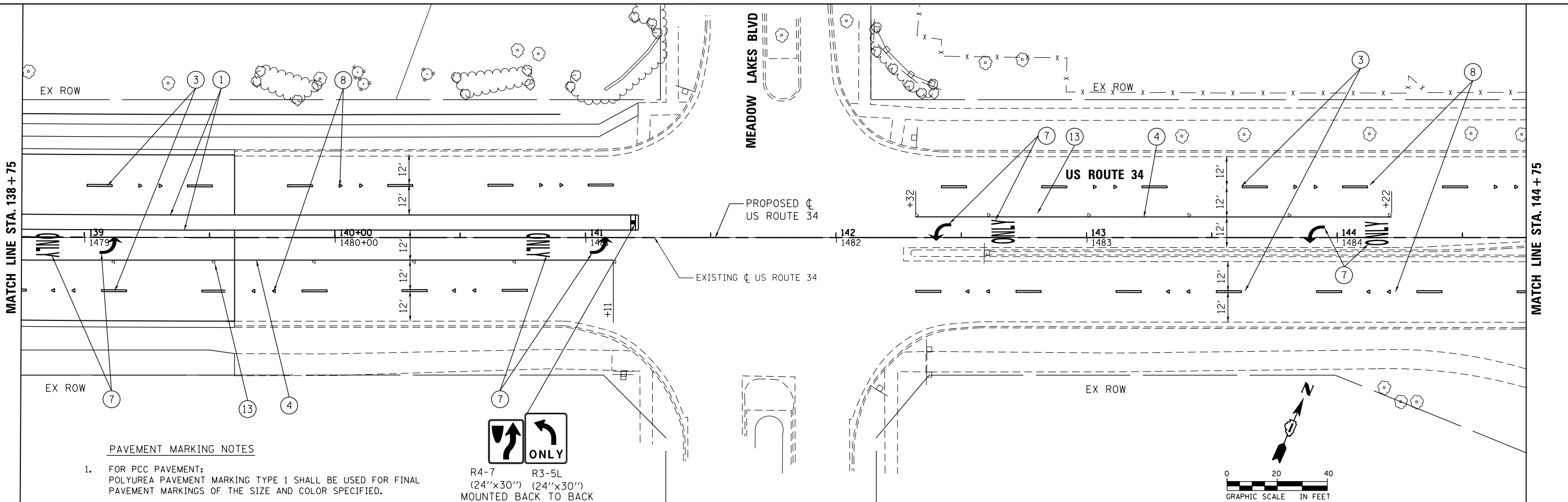
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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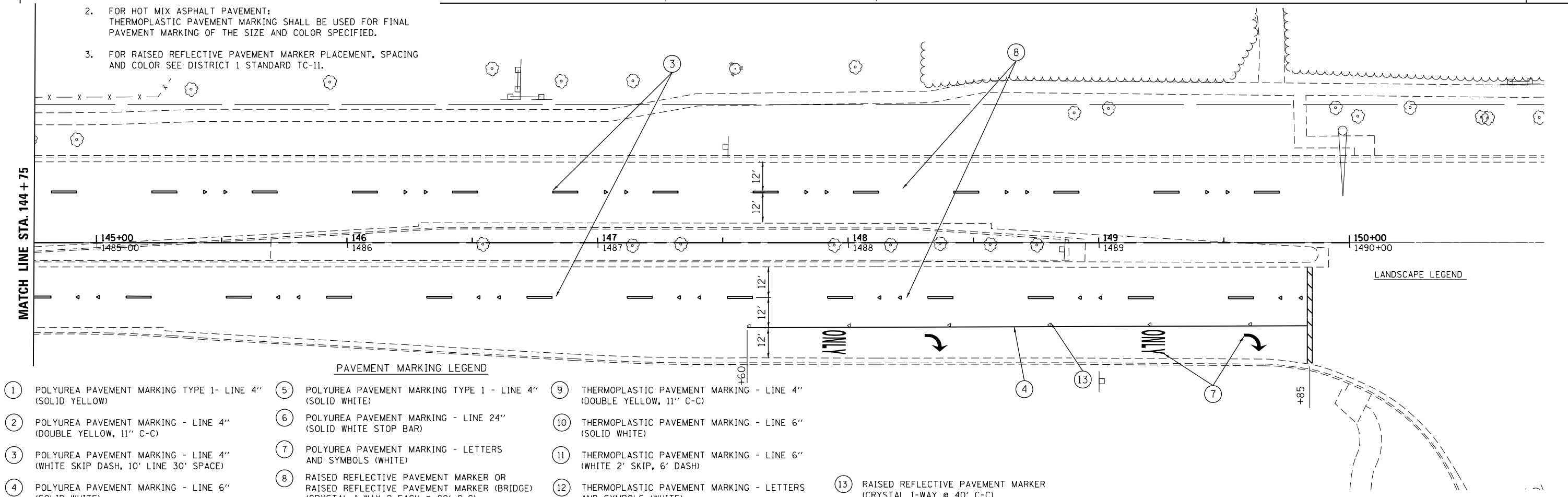
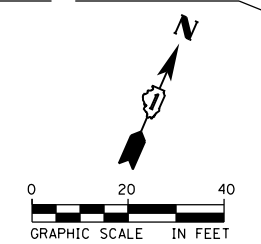


**PAVEMENT MARKING NOTES**

- FOR PCC PAVEMENT:  
POLYUREA PAVEMENT MARKING TYPE 1 SHALL BE USED FOR FINAL PAVEMENT MARKINGS OF THE SIZE AND COLOR SPECIFIED.
- FOR HOT MIX ASPHALT PAVEMENT:  
THERMOPLASTIC PAVEMENT MARKING SHALL BE USED FOR FINAL PAVEMENT MARKING OF THE SIZE AND COLOR SPECIFIED.
- FOR RAISED REFLECTIVE PAVEMENT MARKER PLACEMENT, SPACING AND COLOR SEE DISTRICT 1 STANDARD TC-11.



R4-7 R3-5L  
(24"x30") (24"x30")  
MOUNTED BACK TO BACK



**PAVEMENT MARKING LEGEND**

- |   |  |  |
|---|--|--|
| 1 POLYUREA PAVEMENT MARKING TYPE 1- LINE 4" (SOLID WHITE)                   | 5 POLYUREA PAVEMENT MARKING TYPE 1 - LINE 4" (SOLID WHITE)   | 9 THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW, 11" C-C)  |
| 2 POLYUREA PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW, 11" C-C)              | 6 POLYUREA PAVEMENT MARKING - LINE 24" (SOLID WHITE STOP BAR)  | 10 THERMOPLASTIC PAVEMENT MARKING - LINE 6" (SOLID WHITE)            |
| 3 POLYUREA PAVEMENT MARKING - LINE 4" (WHITE SKIP DASH, 10' LINE 30' SPACE) | 7 POLYUREA PAVEMENT MARKING - LETTERS AND SYMBOLS (WHITE)  | 11 THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE 2' SKIP, 6' DASH) |
| 4 POLYUREA PAVEMENT MARKING - LINE 6" (SOLID WHITE)                         | 8 RAISED REFLECTIVE PAVEMENT MARKER OR RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE) (CRYSTAL 1-WAY 2 EACH @ 80' C-C) | 12 THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (WHITE)      |
|   |  | 13 RAISED REFLECTIVE PAVEMENT MARKER (CRYSTAL 1-WAY @ 40' C-C)       |



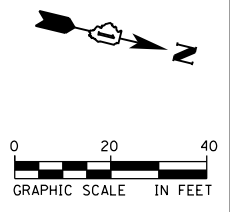
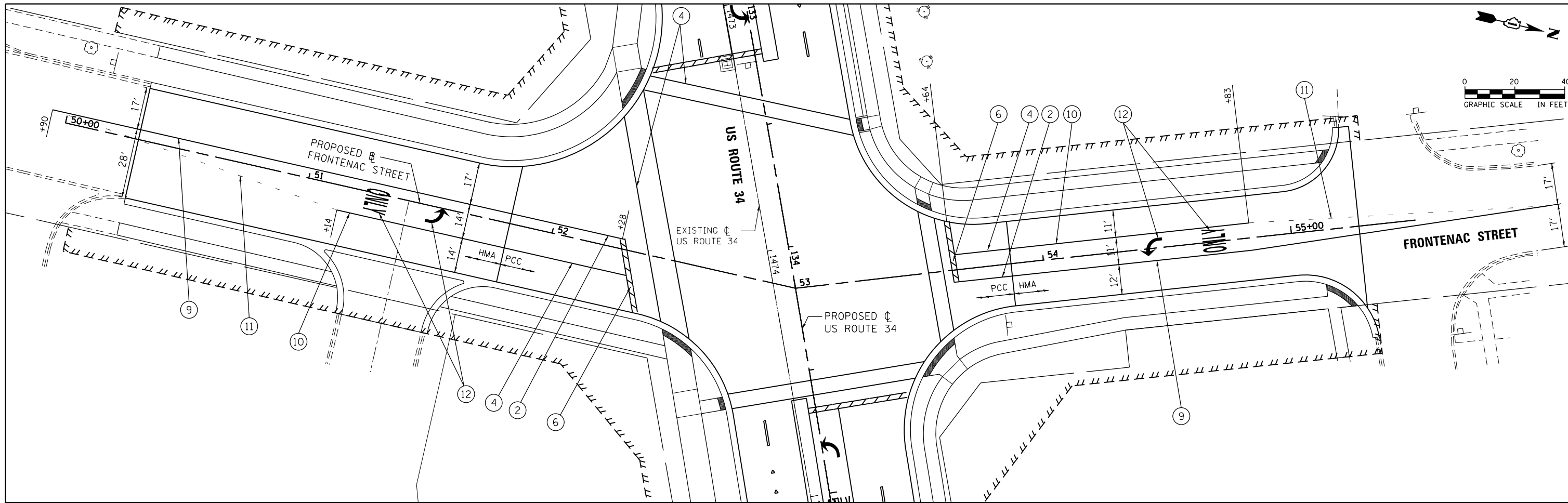
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING PLAN  
US ROUTE 34**

SCALE: 20 SHEET NO. 6 OF 7 SHEETS STA. 138+75 TO STA. 150+00

F.A.P. RTE. 311	SECTION 652-A	COUNTY DUPAGE	TOTAL SHEETS 383	SHEET NO. 141
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**PAVEMENT MARKING LEGEND**

- ① POLYUREA PAVEMENT MARKING TYPE 1- LINE 4" (SOLID YELLOW)
- ② POLYUREA PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW, 11" C-C)
- ③ POLYUREA PAVEMENT MARKING - LINE 4" (WHITE SKIP DASH, 10' LINE 30' SPACE)
- ④ POLYUREA PAVEMENT MARKING - LINE 6" (SOLID WHITE)
- ⑤ POLYUREA PAVEMENT MARKING TYPE 1 - LINE 4" (SOLID WHITE)
- ⑥ POLYUREA PAVEMENT MARKING - LINE 24" (SOLID WHITE STOP BAR)
- ⑦ POLYUREA PAVEMENT MARKING - LETTERS AND SYMBOLS (WHITE)
- ⑧ RAISED REFLECTIVE PAVEMENT MARKER OR RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE) (CRYSTAL 1-WAY 2 EACH @ 80' C-C)
- ⑨ THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW, 11" C-C)
- ⑩ THERMOPLASTIC PAVEMENT MARKING - LINE 6" (SOLID WHITE)
- ⑪ THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE 2' SKIP, 6' DASH)
- ⑫ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS (WHITE)
- ⑬ RAISED REFLECTIVE PAVEMENT MARKER (CRYSTAL 1-WAY @ 40' C-C)

**PAVEMENT MARKING NOTES**

1. FOR PCC PAVEMENT: POLYUREA PAVEMENT MARKING TYPE 1 SHALL BE USED FOR FINAL PAVEMENT MARKINGS OF THE SIZE AND COLOR SPECIFIED.
2. FOR HOT MIX ASPHALT PAVEMENT: THERMOPLASTIC PAVEMENT MARKING SHALL BE USED FOR FINAL PAVEMENT MARKING OF THE SIZE AND COLOR SPECIFIED.
3. FOR RAISED REFLECTIVE PAVEMENT MARKER PLACEMENT, SPACING AND COLOR SEE DISTRICT 1 STANDARD TC-11.

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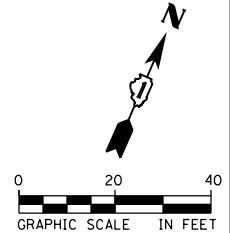
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

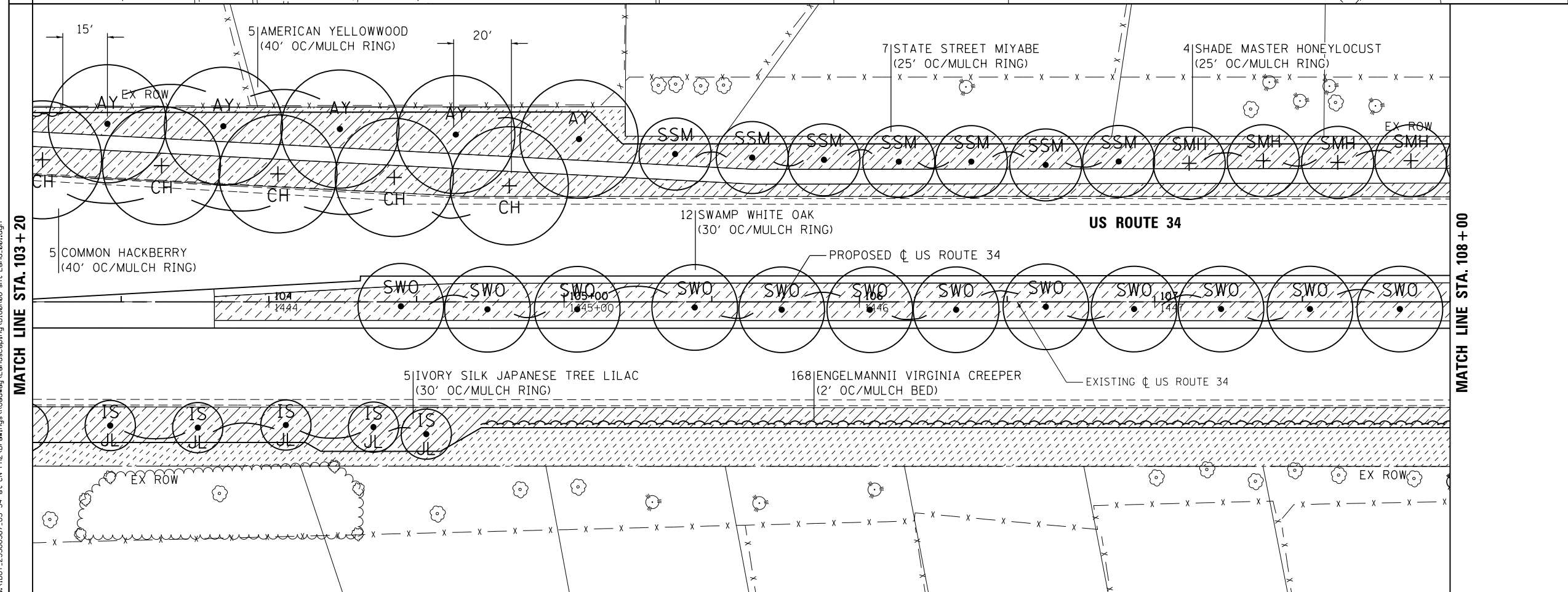
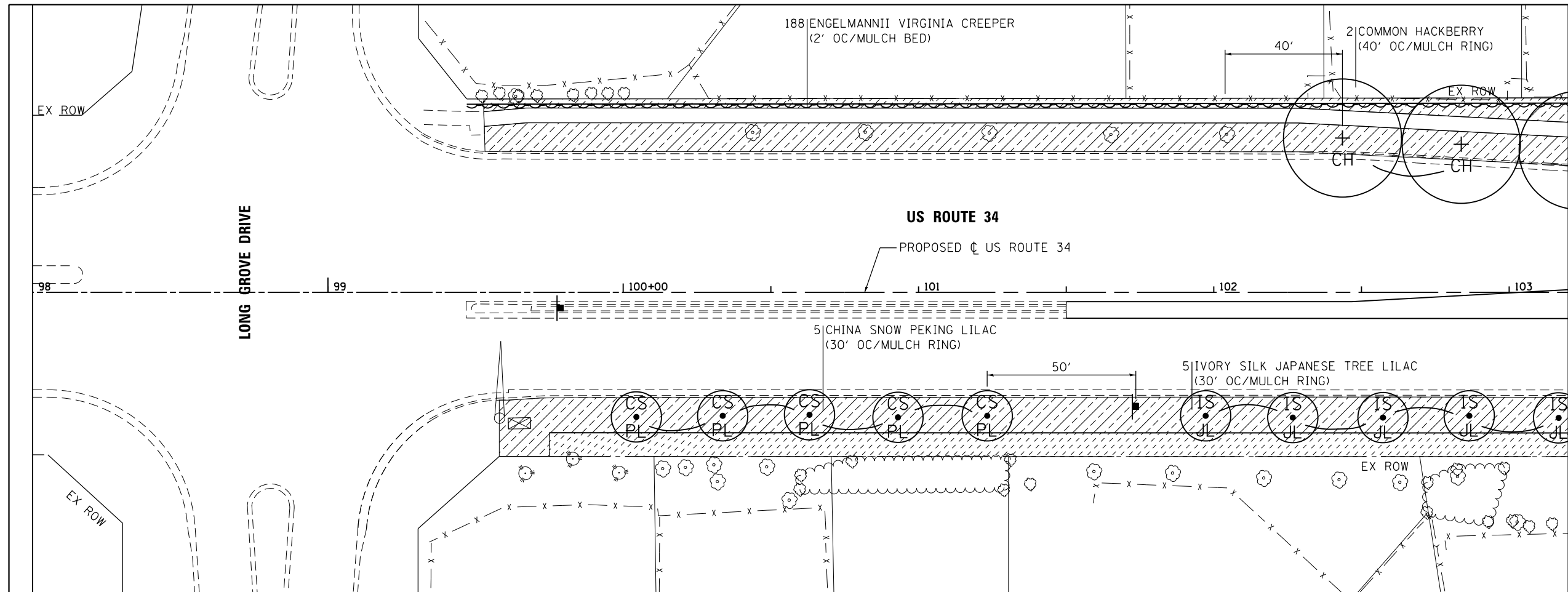
**PAVEMENT MARKING PLAN  
FRONTENAC STREET**

SCALE: 20 SHEET NO. 7 OF 7 SHEETS STA. 50+00 TO STA. 55+50

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	142
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- LANDSCAPE LEGEND**
- TOPSOIL, FURNISH AND PLACE, 24" SODDING, SALT TOLLERANT
  - TOPSOIL, FURNISH AND PLACE, 4" SODDING, SALT TOLLERANT
  - TOPSOIL, FURNISH AND PLACE, 4" SEEDING CLASS 1 EROSION CONTROL BLANKET
- NOTES**  
 THE ENGINEER WILL CONTACT FABIOLA QUIROZ OF THE ROADSIDE DEVELOPMENT UNIT AT (847) 705-4171, AT LEAST 72 HOURS PRIOR TO PLANTING FOR LAYOUT APPROVAL OF THE TREES.
- SEE SHEET 297 FOR SCHEDULE OF LANDSCAPE ITEMS



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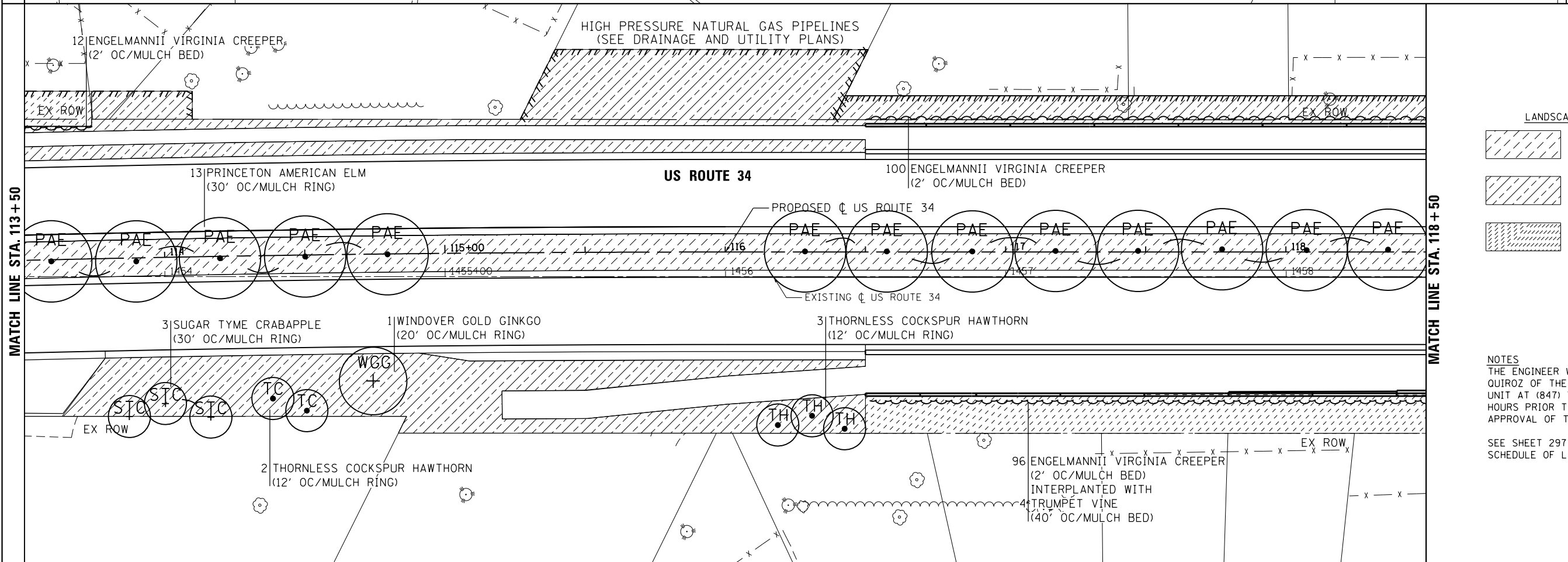
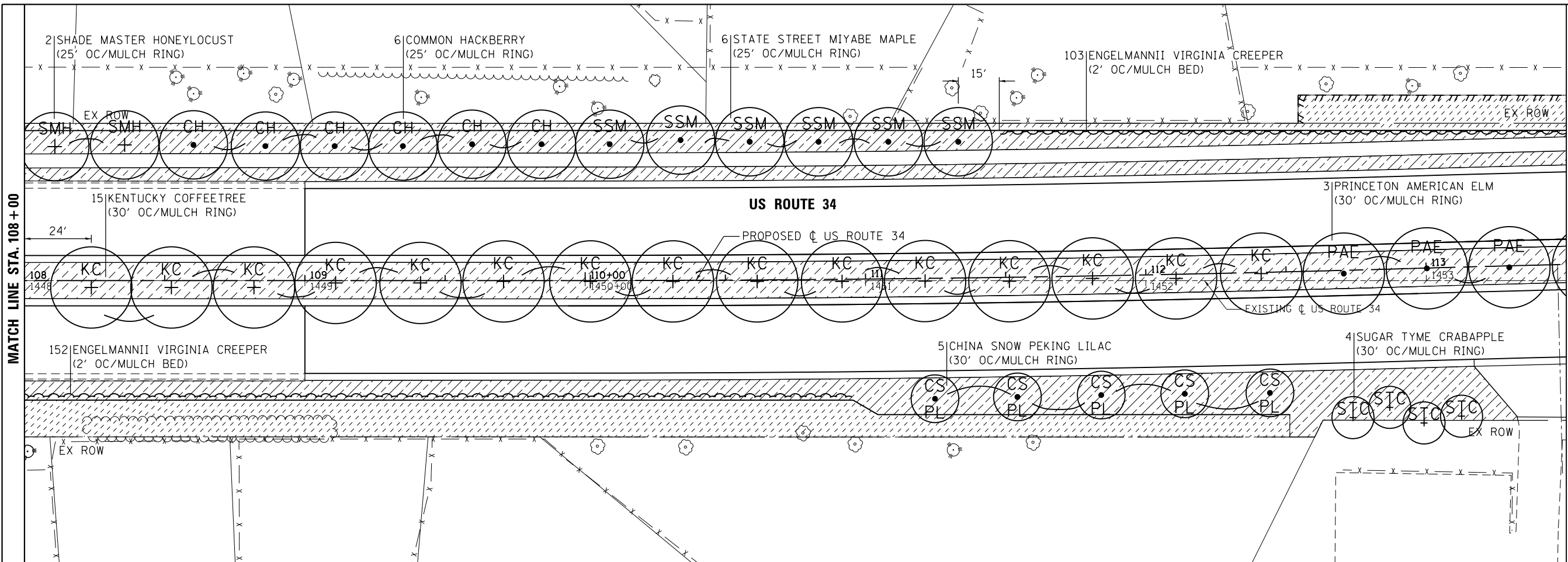
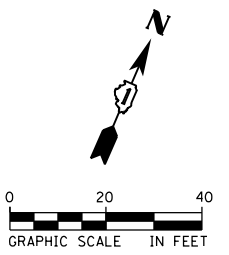
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PLOT DATE = 02/28/2014	DATE - 02/28/2014	REVISOR -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**LANDSCAPE PLAN  
US ROUTE 34**

SCALE: 20    SHEET NO. 1 OF 5 SHEETS    STA. 98+00 TO STA. 108+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	143
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**LANDSCAPE LEGEND**

	TOPSOIL, FURNISH AND PLACE, 24" SODDING, SALT TOLLERANT
	TOPSOIL, FURNISH AND PLACE, 4" SODDING, SALT TOLLERANT
	TOPSOIL, FURNISH AND PLACE, 4" SEEDING CLASS 1 EROSION CONTROL BLANKET

**NOTES**  
 THE ENGINEER WILL CONTACT FABIOLA QUIROZ OF THE ROADSIDE DEVELOPMENT UNIT AT (847) 705-4171, AT LEAST 72 HOURS PRIOR TO PLANTING FOR LAYOUT APPROVAL OF THE TREES.  
 SEE SHEET 297 FOR SCHEDULE OF LANDSCAPE ITEMS

FILE NAME = Q:\1001\_25366367\_US 34.ctb; CN: Ph2:Drawings\Roadway\Landscaping\0160R06-ant-Land\_002.dgn



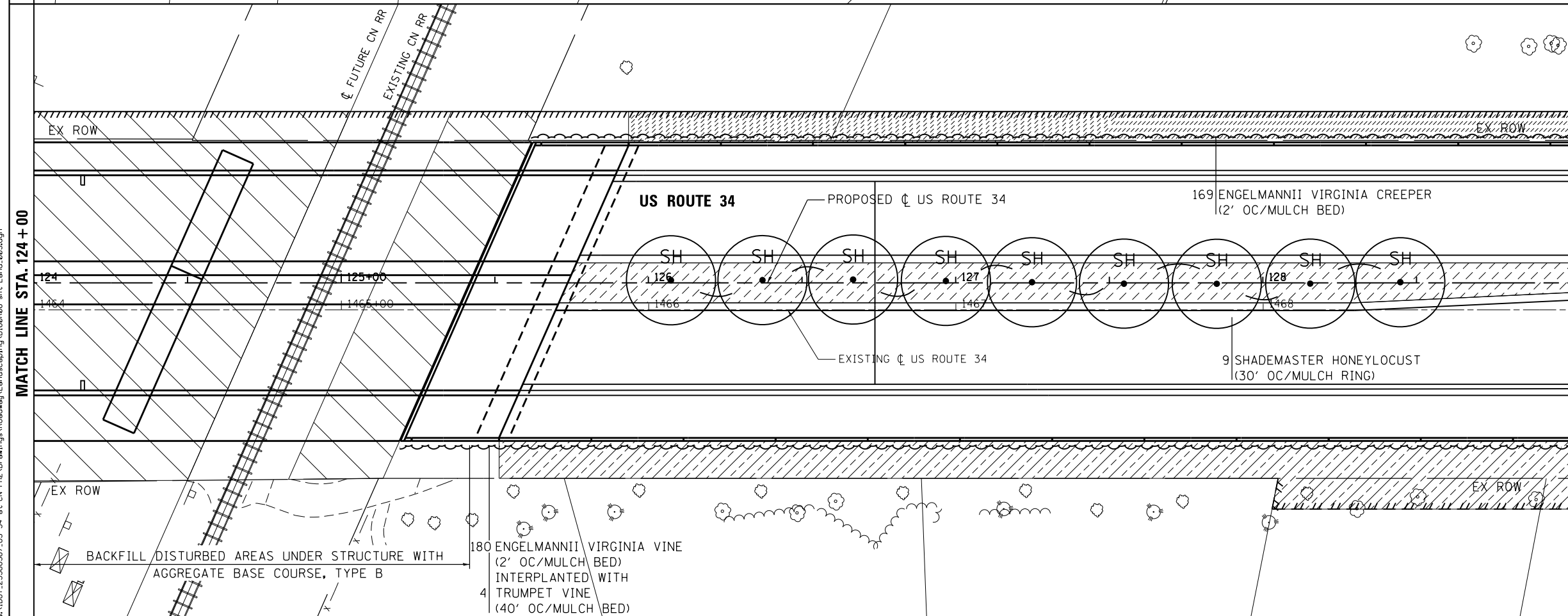
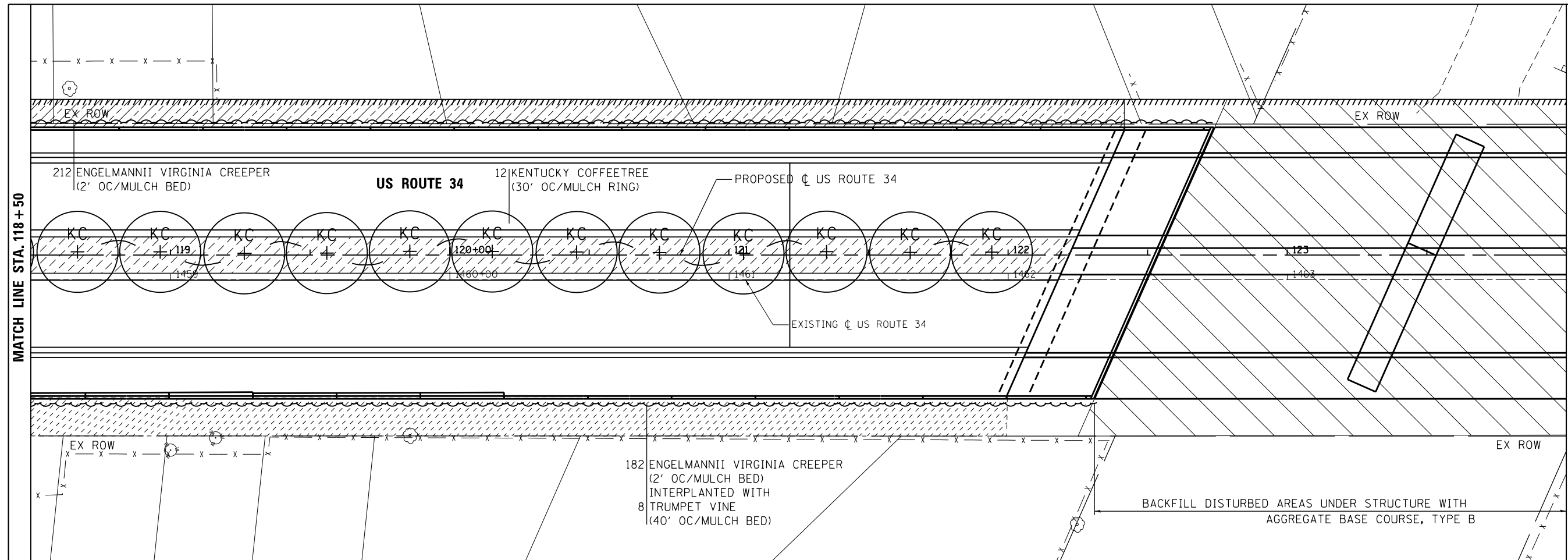
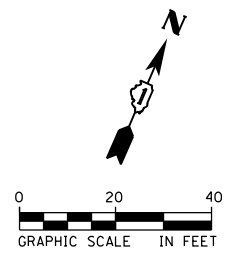
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PLOT DATE = 02/28/2014	DATE - 02/28/2014	REVISOR -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**LANDSCAPE PLAN  
 US ROUTE 34**

SCALE: 20 SHEET NO. 2 OF 5 SHEETS STA. 108+00 TO STA. 118+50

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	144
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**LANDSCAPE LEGEND**

	TOPSOIL, FURNISH AND PLACE, 24" SODDING, SALT TOLLERANT
	TOPSOIL, FURNISH AND PLACE, 4" SODDING, SALT TOLLERANT
	TOPSOIL, FURNISH AND PLACE, 4" SEEDING CLASS 1 EROSION CONTROL BLANKET
	AGGREGATE BASE COURSE, TYPE B

**NOTES**  
 THE ENGINEER WILL CONTACT FABIOLA QUIROZ OF THE ROADSIDE DEVELOPMENT UNIT AT (847) 705-4171, AT LEAST 72 HOURS PRIOR TO PLANTING FOR LAYOUT APPROVAL OF THE TREES.

SEE SHEET 297 FOR SCHEDULE OF LANDSCAPE ITEMS

FILE NAME = Q:\1001\_25368367\_US 34.ctb; CN: P:\2\Drawings\Roadway\Landscaping\0160R06-ant-Land\_003.dgn



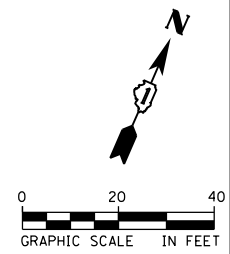
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PLOT DATE = 02/28/2014	DATE - 02/28/2014	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**LANDSCAPE PLAN  
 US ROUTE 34**

SCALE: 20 SHEET NO. 3 OF 5 SHEETS STA. 118+50 TO STA. 129+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	145
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



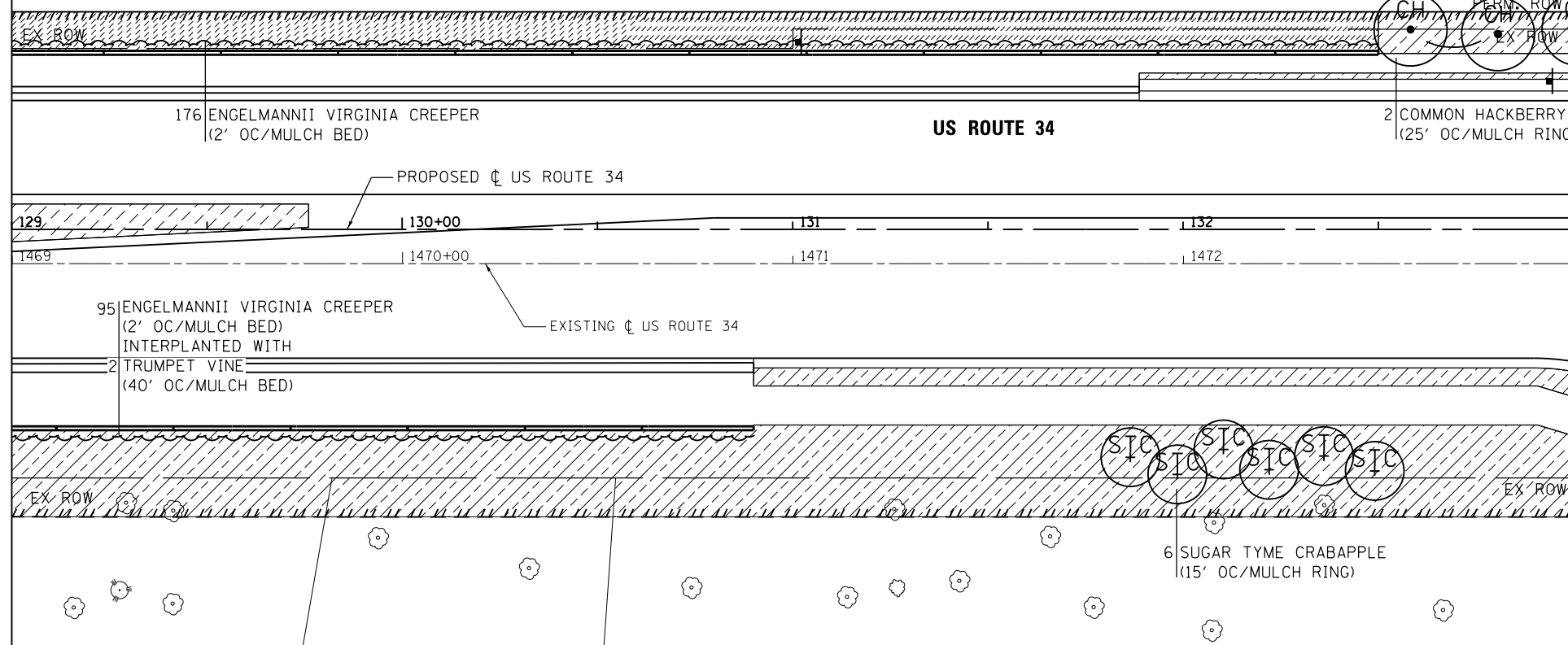
**LANDSCAPE LEGEND**

- TOPSOIL, FURNISH AND PLACE, 24" SODDING, SALT TOLLERANT
- TOPSOIL, FURNISH AND PLACE, 4" SODDING, SALT TOLLERANT
- TOPSOIL, FURNISH AND PLACE, 4" SEEDING CLASS 1 EROSION CONTROL BLANKET

**NOTES**  
 THE ENGINEER WILL CONTACT FABIOLA QUIROZ OF THE ROADSIDE DEVELOPMENT UNIT AT (847) 705-4171, AT LEAST 72 HOURS PRIOR TO PLANTING FOR LAYOUT APPROVAL OF THE TREES.  
  
 SEE SHEET 297 FOR SCHEDULE OF LANDSCAPE ITEMS

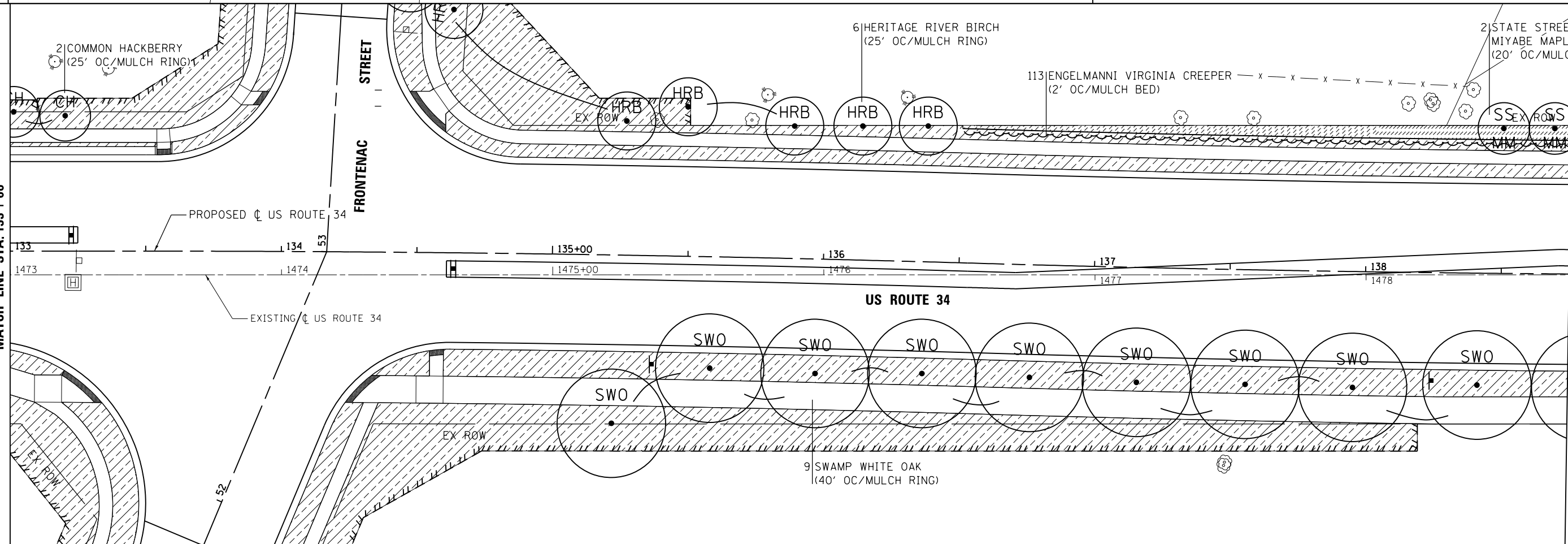
MATCH LINE STA. 129+00

MATCH LINE STA. 133+00



MATCH LINE STA. 133+00

MATCH LINE STA. 138+75



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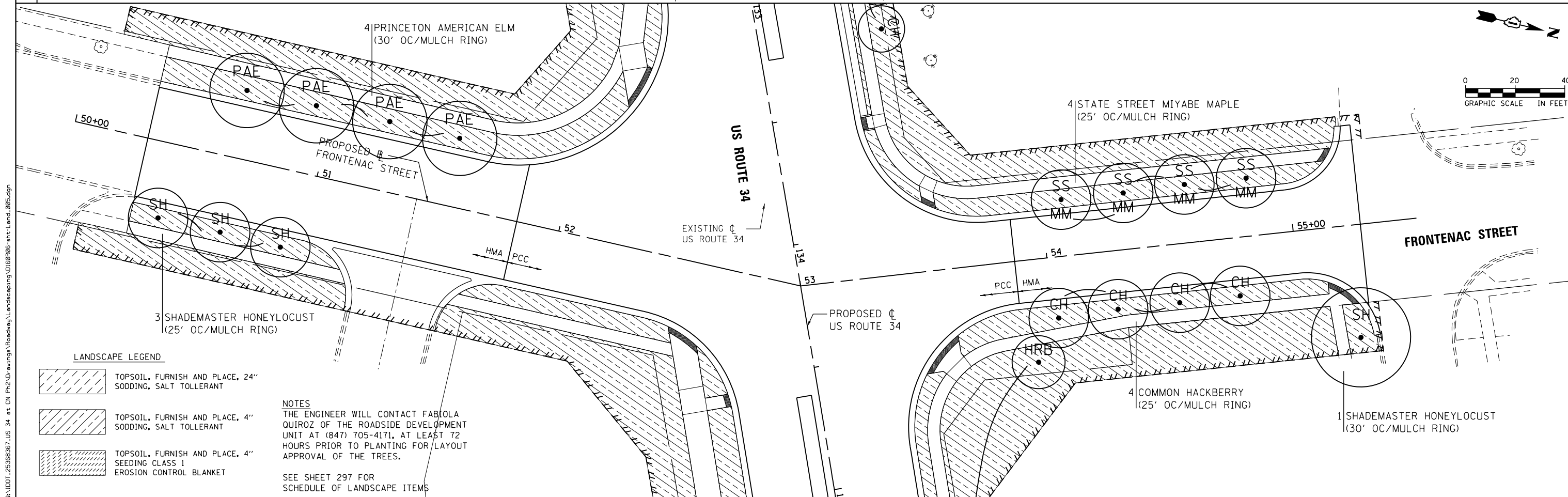
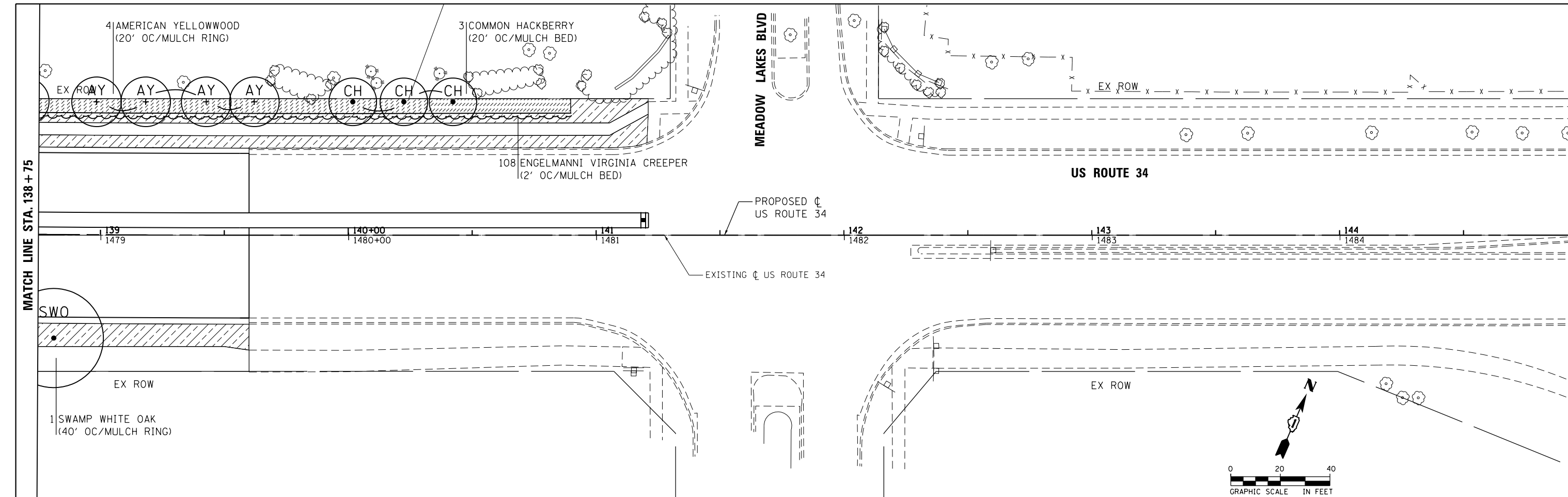
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PLOT DATE = 02/28/2014	DATE - 02/28/2014	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**LANDSCAPE PLAN  
 US ROUTE 34**

SCALE: 20 SHEET NO. 4 OF 5 SHEETS STA. 129+00 TO STA. 138+75

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	146
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**LANDSCAPE LEGEND**

	TOPSOIL, FURNISH AND PLACE, 24" SODDING, SALT TOLLERANT
	TOPSOIL, FURNISH AND PLACE, 4" SODDING, SALT TOLLERANT
	TOPSOIL, FURNISH AND PLACE, 4" SEEDING CLASS 1 EROSION CONTROL BLANKET

**NOTES**  
 THE ENGINEER WILL CONTACT FABOLA QUIROZ OF THE ROADSIDE DEVELOPMENT UNIT AT (847) 705-4171, AT LEAST 72 HOURS PRIOR TO PLANTING FOR LAYOUT APPROVAL OF THE TREES.

SEE SHEET 297 FOR SCHEDULE OF LANDSCAPE ITEMS

FILE NAME = Q:\1001\_25368367\_US\_34.ctb; CN: Ph2; Drawings; Roadway; Landscape; 0160R06-ah-Land\_006.dgn



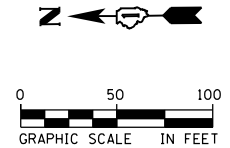
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PLOT DATE = 02/28/2014	CHECKED - SPF	REVISED -
	DATE - 02/28/2014	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**LANDSCAPE PLAN  
 US ROUTE 34 AND FRONTENAC ST**

SCALE: 20 SHEET NO. 5 OF 5 SHEETS STA. 138+75 TO STA. 150+00

F.A.P. RTE. 311	SECTION 652-A	COUNTY DUPAGE	TOTAL SHEETS 383	SHEET NO. 147
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



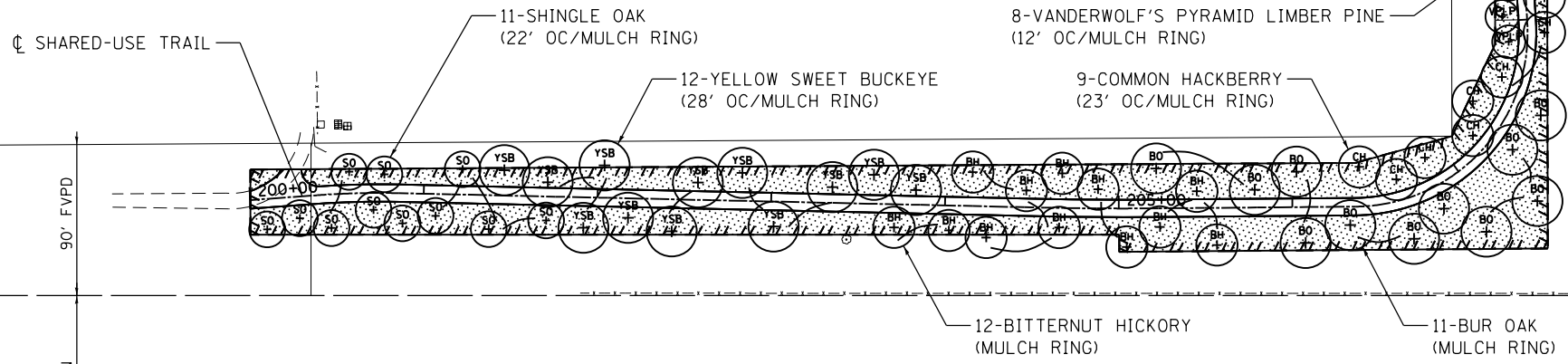
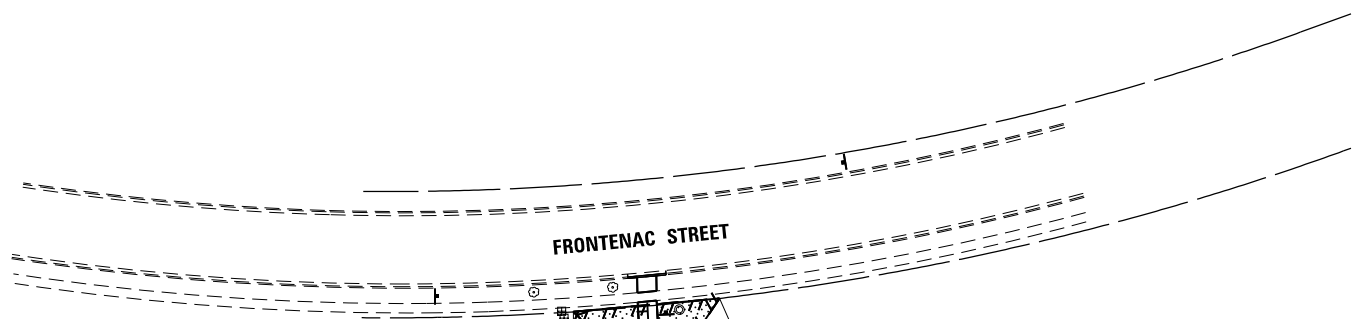
TREE, AESCULUS FLAVEA (YELLOW SWEET BUCKEYE), 2" CALIPER, BALLED AND BURLAPPED	12 TOTAL
TREE, CARYA CORDIFORMIS (BITTERNUT HICKORY), 2" CALIPER, BALLED AND BURLAPPED	12 TOTAL
TREE, CELTIS OCCIDENTALIS (COMMON HACKBERRY), 2" CALIPER, BALLED AND BURLAPPED	9 TOTAL
TREE, QUERCUS IMBRICARIA (SHINGLE OAK), 2" CALIPER, BALLED AND BURLAPPED	11 TOTAL
TREE, QUERCUS MACROCARPA (BUR OAK), 2" CALIPER, BALLED AND BURLAPPED	11 TOTAL
TREE, ULMUS AMERICANA PRINCETON (PRINCETON AMERICAN ELM), 2" CALIPER, BALLED AND BURLAPPED	8 TOTAL
EVERGREEN, PINUS FLEXILIS VANDERWOLF'S PYRAMID (VANDERWOLF'S PYRAMID LIMBER PINE), 6' HEIGHT, BALLED AND BURLAPPED	15 TOTAL

**LANDSCAPING LEGEND**

- TOPSOIL FURNISH AND PLACE, 4" SEEDING CLASS 3 EROSION CONTROL BLANKET (SPECIAL)
- SHINGLE OAK
- YELLOW SWEET BUCKEYE
- BITTERNUT HICKORY
- BUR OAK
- COMMON HACKBERRY
- VANDERWOLF'S PYRAMID LIMBER PINE
- PRINCETON AMERICAN ELM

**NOTES**  
 THE ENGINEER WILL CONTACT FABIOLA QUIROZ OF THE ROADSIDE DEVELOPMENT UNIT AT (847) 705-4171, AT LEAST 72 HOURS PRIOR TO PLANTING FOR LAYOUT APPROVAL OF THE TREES.

SEE SHEET 297 FOR SCHEDULE OF LANDSCAPE ITEMS



FILE NAME = Q:\100T\_25368367\_US\_34.ctb CN Ph2\Drawings\Roadway\Landscaping\0160R06-ah-Land-Trail\_001.dgn



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PLOT DATE = 02/28/2014	DATE - 02/28/2014	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**LANDSCAPING PLAN  
 SHARED-USE TRAIL**

SCALE: 50 SHEET NO. 1 OF 1 SHEETS STA. 200+00 TO STA. 212+05

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	148
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60R06	



RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

**NOTES FOR EXISTING TRAFFIC SIGNAL EQUIPMENT REMOVAL**

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR, SHALL REMAIN THE PROPERTY OF I.D.O.T. AND SHALL BE STORED AS STATED IN THE SPECIAL PROVISIONS FOR PICK UP BY I.D.O.T. FORCES AS DESIGNATED BY THE ENGINEER.

1 EACH CONTROLLER AND CABINET, COMPLETE

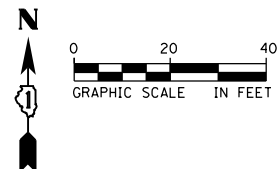
THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND DISPOSED OF BY HIM AT HIS EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 4 EACH ALUMINUM MAST ARM ASSEMBLY AND POLE
- 4 EACH TRAFFIC SIGNAL POST
- 8 EACH TRAFFIC SIGNAL BACKPLATE
- 4 EACH SIGNAL HEAD, 1 - FACE, 3 - SECTION
- 6 EACH SIGNAL HEAD, 1 - FACE, 5 - SECTION
- 2 EACH SIGNAL HEAD, 2 - FACE, 1-3 SECTION, 1-5 SECTION
- 1 EACH PEDESTRIAN HEAD, 1 - FACE
- 3 EACH PEDESTRIAN HEAD, 2-FACE
- 5 EACH PEDESTRIAN PUSHBUTTON
- 1 EACH SERVICE INSTALLATION

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE AGENCY LISTED BELOW. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR THE PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE LISTED AGENCY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

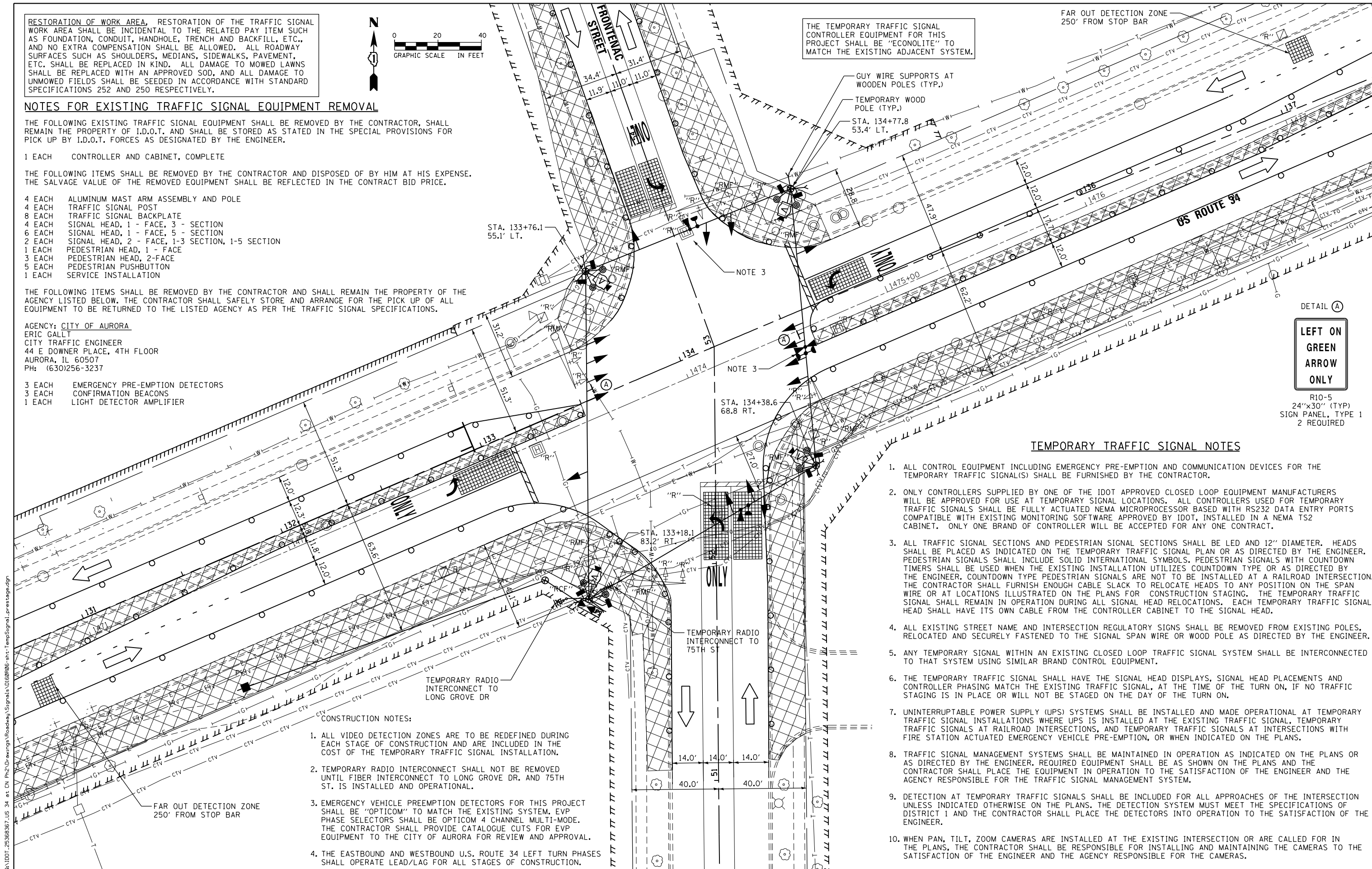
AGENCY: CITY OF AURORA  
 ERIC GALLT  
 CITY TRAFFIC ENGINEER  
 44 E DOWNER PLACE, 4TH FLOOR  
 AURORA, IL 60507  
 PH: (630)256-3237

- 3 EACH EMERGENCY PRE-EMPTION DETECTORS
- 3 EACH CONFIRMATION BEACONS
- 1 EACH LIGHT DETECTOR AMPLIFIER



THE TEMPORARY TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

FAR OUT DETECTION ZONE  
 250' FROM STOP BAR



DETAIL (A)  
**LEFT ON GREEN ARROW ONLY**  
 R10-5  
 24"x30" (TYP)  
 SIGN PANEL, TYPE 1  
 2 REQUIRED

**TEMPORARY TRAFFIC SIGNAL NOTES**

1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE IDOT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT, INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE LED AND 12" DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT A RAILROAD INTERSECTION. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
7. UNINTERRUPTABLE POWER SUPPLY (UPS) SYSTEMS SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL, TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
8. TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
9. DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
10. WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.

**CONSTRUCTION NOTES:**

1. ALL VIDEO DETECTION ZONES ARE TO BE REDEFINED DURING EACH STAGE OF CONSTRUCTION AND ARE INCLUDED IN THE COST OF THE TEMPORARY TRAFFIC SIGNAL INSTALLATION.
2. TEMPORARY RADIO INTERCONNECT SHALL NOT BE REMOVED UNTIL FIBER INTERCONNECT TO LONG GROVE DR. AND 75TH ST. IS INSTALLED AND OPERATIONAL.
3. EMERGENCY VEHICLE PREEMPTION DETECTORS FOR THIS PROJECT SHALL BE "OPTICOM" TO MATCH THE EXISTING SYSTEM. EVP PHASE SELECTORS SHALL BE OPTICOM 4 CHANNEL MULTI-MODE. THE CONTRACTOR SHALL PROVIDE CATALOGUE CUTS FOR EVP EQUIPMENT TO THE CITY OF AURORA FOR REVIEW AND APPROVAL.
4. THE EASTBOUND AND WESTBOUND U.S. ROUTE 34 LEFT TURN PHASES SHALL OPERATE LEAD/LAG FOR ALL STAGES OF CONSTRUCTION.

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	DRAWN - KJB	REVISED -
PLOT SCALE = 40.0000' / in.	CHECKED - SPF	REVISED -
PLOT DATE = 02/28/2014	DATE - 02/28/2014	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

**TEMPORARY TRAFFIC SIGNAL INSTALLATION AND REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN - PRE-STAGE US 34 (OGDEN AVENUE) AND FRONTENAC STREET**

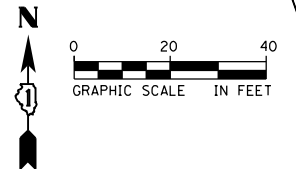
SCALE: 1"=20' SHEET NO. 1 OF 6 SHEETS STA. 131+00 TO STA. 137+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	149
				CONTRACT NO. 60R06
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

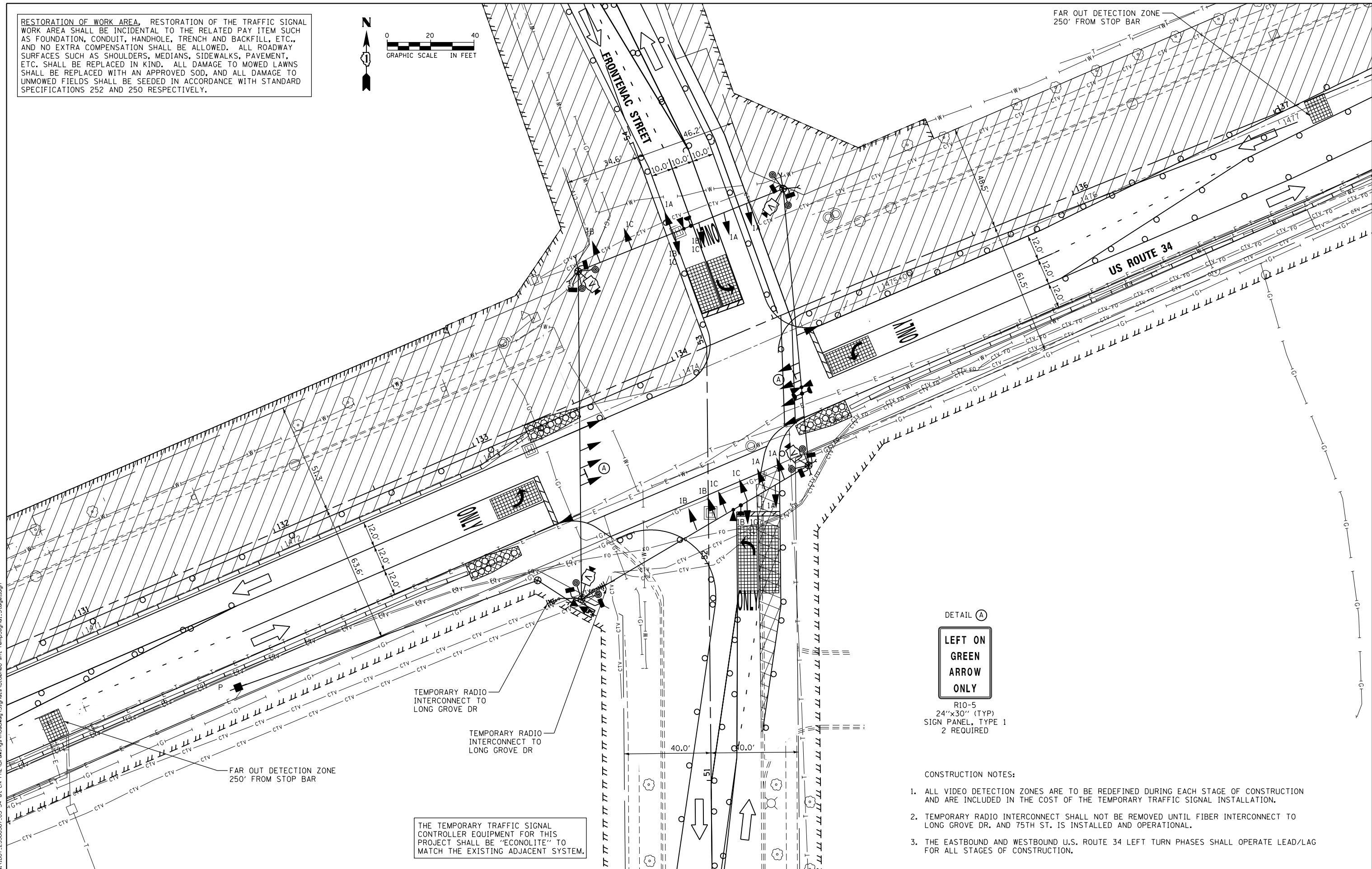


FILE NAME = Q:\1007\_25368367\_US 34 at CN Ph2 Drawings\Roadway\Signals\1618006-shr-TempSignal\_prestage.dgn

RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



FAR OUT DETECTION ZONE  
250' FROM STOP BAR



DETAIL (A)  
**LEFT ON GREEN ARROW ONLY**

R10-5  
24"x30" (TYP)  
SIGN PANEL, TYPE 1  
2 REQUIRED

CONSTRUCTION NOTES:

1. ALL VIDEO DETECTION ZONES ARE TO BE REDEFINED DURING EACH STAGE OF CONSTRUCTION AND ARE INCLUDED IN THE COST OF THE TEMPORARY TRAFFIC SIGNAL INSTALLATION.
2. TEMPORARY RADIO INTERCONNECT SHALL NOT BE REMOVED UNTIL FIBER INTERCONNECT TO LONG GROVE DR. AND 75TH ST. IS INSTALLED AND OPERATIONAL.
3. THE EASTBOUND AND WESTBOUND U.S. ROUTE 34 LEFT TURN PHASES SHALL OPERATE LEAD/LAG FOR ALL STAGES OF CONSTRUCTION.

THE TEMPORARY TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

TEMPORARY RADIO INTERCONNECT TO LONG GROVE DR

TEMPORARY RADIO INTERCONNECT TO LONG GROVE DR

FAR OUT DETECTION ZONE  
250' FROM STOP BAR

FILE NAME = Q:\1001\_25368367\_US 34 at CN Ph2 Drawings\Roadway\Signals\0168706-sht-TempSignal\_Stage1.dgn



USER NAME = URS	DESIGNED - MB	REVISED -
DRAWN - KJB	REVISED -	
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PLOT DATE = 02/28/2014	DATE - 02/28/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

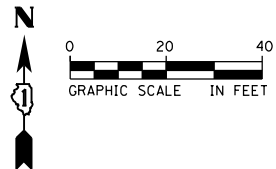
TEMPORARY TRAFFIC SIGNAL INSTALLATION - STAGE 1  
US 34 (OGDEN AVENUE) AND FRONTENAC STREET

SCALE: 1"=20' SHEET NO. 2 OF 6 SHEETS STA. 131+00 TO STA. 137+00

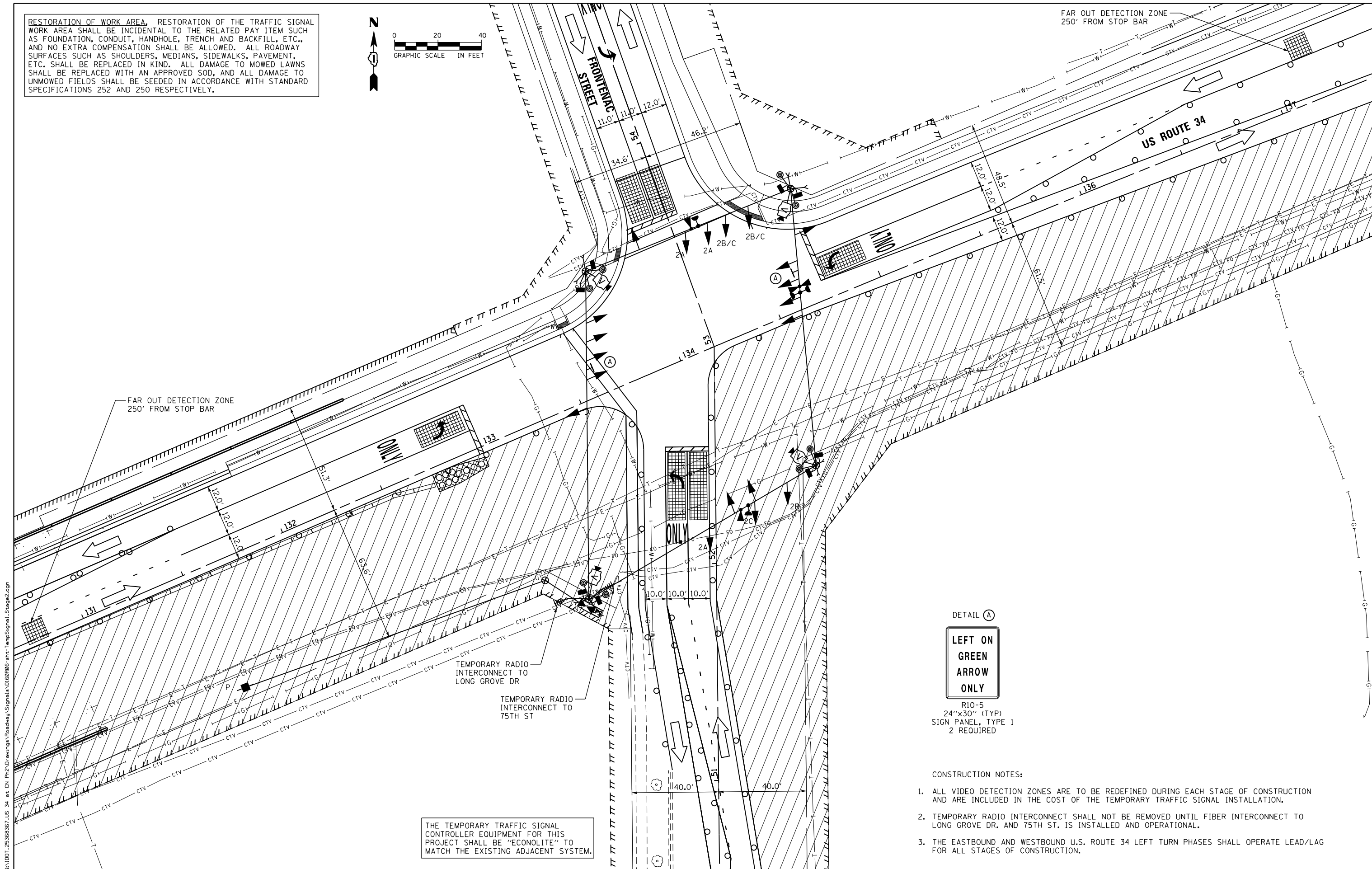
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	150
CONTRACT NO. 60R06				

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



FAR OUT DETECTION ZONE  
250' FROM STOP BAR



DETAIL (A)  
**LEFT ON GREEN ARROW ONLY**  
R10-5  
24"x30" (TYP)  
SIGN PANEL, TYPE 1  
2 REQUIRED

- CONSTRUCTION NOTES:
1. ALL VIDEO DETECTION ZONES ARE TO BE REDEFINED DURING EACH STAGE OF CONSTRUCTION AND ARE INCLUDED IN THE COST OF THE TEMPORARY TRAFFIC SIGNAL INSTALLATION.
  2. TEMPORARY RADIO INTERCONNECT SHALL NOT BE REMOVED UNTIL FIBER INTERCONNECT TO LONG GROVE DR. AND 75TH ST. IS INSTALLED AND OPERATIONAL.
  3. THE EASTBOUND AND WESTBOUND U.S. ROUTE 34 LEFT TURN PHASES SHALL OPERATE LEAD/LAG FOR ALL STAGES OF CONSTRUCTION.

THE TEMPORARY TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

FILE NAME = Q:\11001\_25368367\_US\_34.ctb; CN P:\2\Drawings\Roadway\Signals\11687006-sht-TempSignal\_Stage2.dgn



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DRAWN - KJB	REVISIONS -	
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PLOT DATE = 02/28/2014	DATE - 02/28/2014	REVISIONS -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

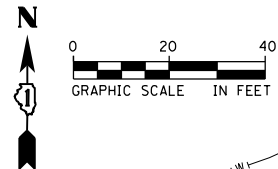
**TEMPORARY TRAFFIC SIGNAL INSTALLATION - STAGE 2  
US 34 (OGDEN AVENUE) AND FRONTENAC STREET**

SCALE: 1"=20' SHEET NO. 4 OF 6 SHEETS STA. 131+00 TO STA. 137+00

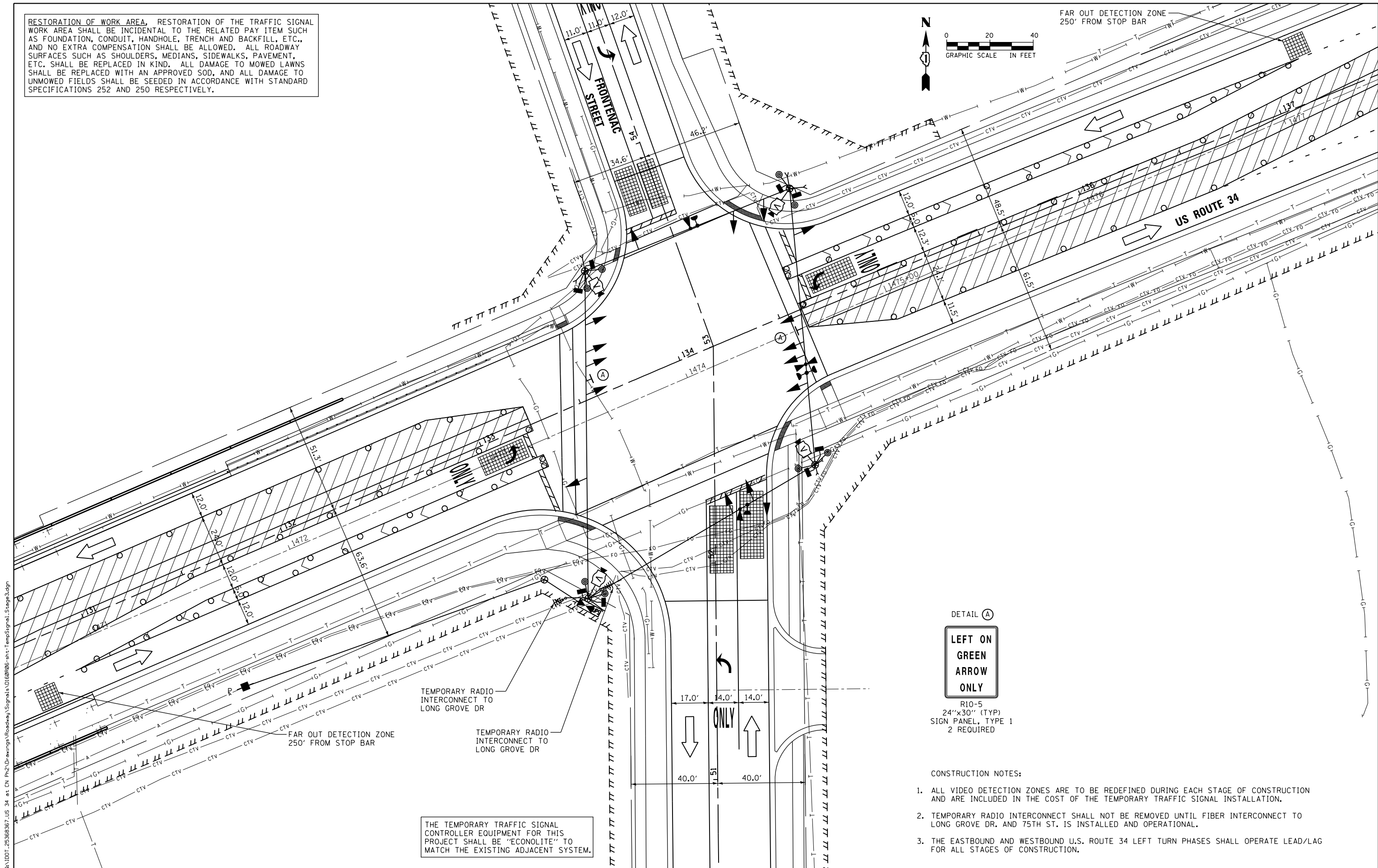
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	151
CONTRACT NO. 60R06				

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



FAR OUT DETECTION ZONE  
250' FROM STOP BAR



DETAIL (A)



R10-5  
24"x30" (TYP)  
SIGN PANEL, TYPE 1  
2 REQUIRED

CONSTRUCTION NOTES:

1. ALL VIDEO DETECTION ZONES ARE TO BE REDEFINED DURING EACH STAGE OF CONSTRUCTION AND ARE INCLUDED IN THE COST OF THE TEMPORARY TRAFFIC SIGNAL INSTALLATION.
2. TEMPORARY RADIO INTERCONNECT SHALL NOT BE REMOVED UNTIL FIBER INTERCONNECT TO LONG GROVE DR. AND 75TH ST. IS INSTALLED AND OPERATIONAL.
3. THE EASTBOUND AND WESTBOUND U.S. ROUTE 34 LEFT TURN PHASES SHALL OPERATE LEAD/LAG FOR ALL STAGES OF CONSTRUCTION.

THE TEMPORARY TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

FILE NAME = Q:\11001\_25368367\_US 34 at CN Ph2 Drawings\Roadway\Signals\1168006-sht-TempSignal\_Stage3.dgn



USER NAME = URS	DESIGNED - MB	REVISED -
PLOT SCALE = 48.0000' / in.	DRAWN - KJB	REVISED -
PLOT DATE = 02/28/2014	CHECKED - SPF	REVISED -
	DATE - 02/28/2014	REVISED -

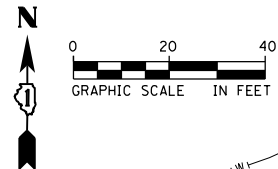
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TEMPORARY TRAFFIC SIGNAL INSTALLATION - STAGE 3  
US 34 (OGDEN AVENUE) AND FRONTENAC STREET

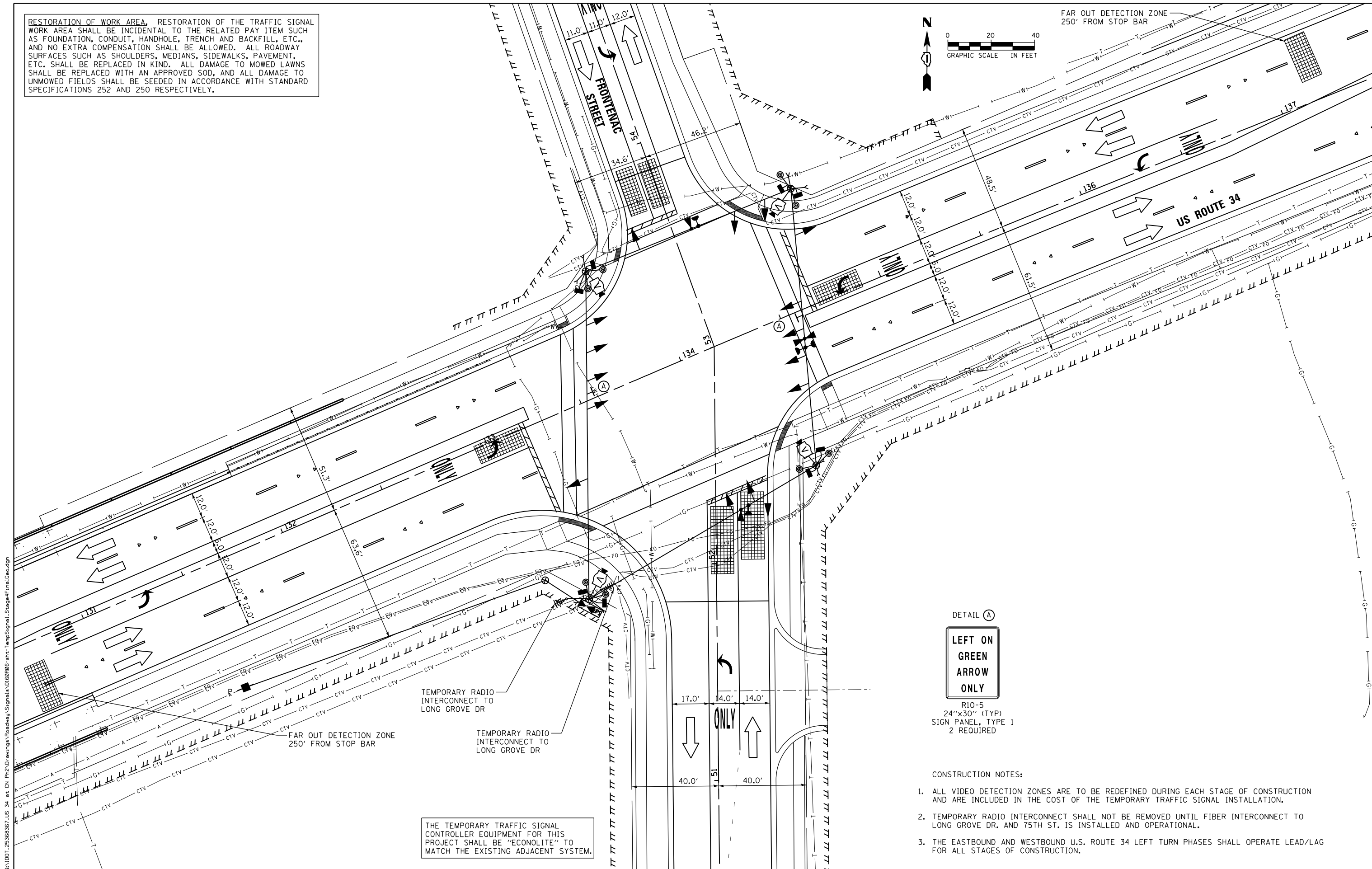
SCALE: 1"=20' SHEET NO. 5 OF 6 SHEETS STA. 131+00 TO STA. 137+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	152
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



FAR OUT DETECTION ZONE  
250' FROM STOP BAR



R10-5  
24"x30" (TYP)  
SIGN PANEL, TYPE 1  
2 REQUIRED

CONSTRUCTION NOTES:

1. ALL VIDEO DETECTION ZONES ARE TO BE REDEFINED DURING EACH STAGE OF CONSTRUCTION AND ARE INCLUDED IN THE COST OF THE TEMPORARY TRAFFIC SIGNAL INSTALLATION.
2. TEMPORARY RADIO INTERCONNECT SHALL NOT BE REMOVED UNTIL FIBER INTERCONNECT TO LONG GROVE DR. AND 75TH ST. IS INSTALLED AND OPERATIONAL.
3. THE EASTBOUND AND WESTBOUND U.S. ROUTE 34 LEFT TURN PHASES SHALL OPERATE LEAD/LAG FOR ALL STAGES OF CONSTRUCTION.

THE TEMPORARY TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

TEMPORARY RADIO INTERCONNECT TO LONG GROVE DR

FAR OUT DETECTION ZONE  
250' FROM STOP BAR

FILE NAME = Q:\1001\_25368367\_US 34 at CN Ph2 Drawings\Roadway\Signals\01687006-sht-TempSignal\_Stage4Final.dgn



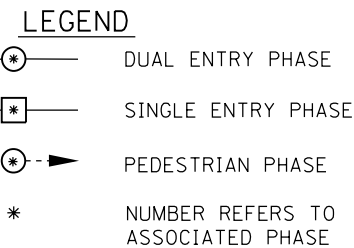
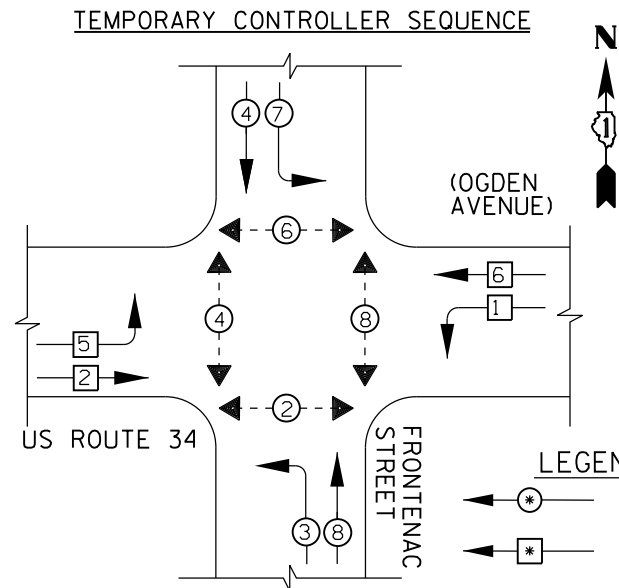
USER NAME = URS	DESIGNED - MB	REVISED -
PLOT SCALE = 48.0000' / in.	DRAWN - KJB	REVISED -
PLOT DATE = 02/28/2014	CHECKED - SPF	REVISED -
	DATE - 02/28/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TEMPORARY TRAFFIC SIGNAL INSTALLATION  
FINAL GEOMETRICS  
US 34 (OGDEN AVENUE) AND FRONTENAC STREET

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	153
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: 1"=20' SHEET NO. 6 OF 6 SHEETS STA. 131+00 TO STA. 137+00



**TEMPORARY PHASE DESIGNATION DIAGRAM**  
PRE-STAGE, FINAL GEOMETRICS

**CONSTRUCTION NOTES:**

1. THE EASTBOUND AND WESTBOUND U.S. ROUTE 34 LEFT TURN PHASES SHALL OPERATE LEAD/LAG FOR ALL STAGES OF CONSTRUCTION.

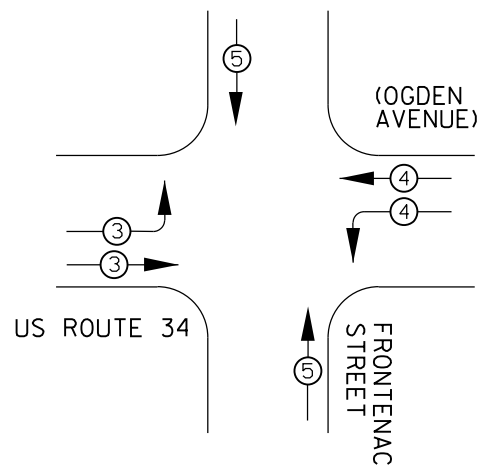
RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

FILE NAME = Q:\1100T\_25368367\_US 34 at CN Ph2\Drawings\Roadway\Signals\1169706-sht-Temp-CablePlan.dgn

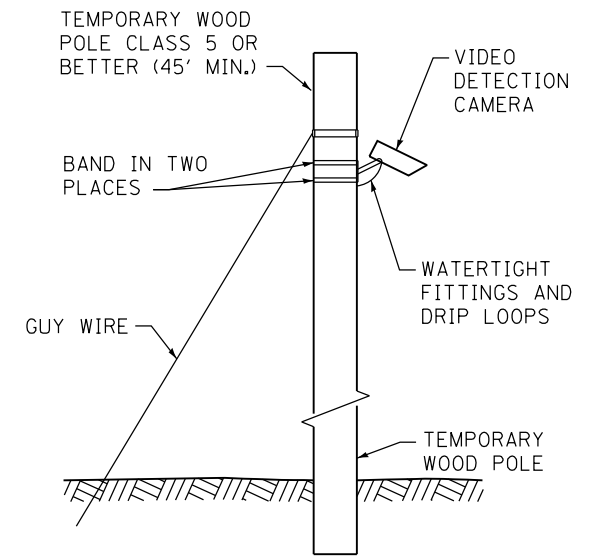
I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	LED	%OPERATION	
SIGNAL (RED)	16		17	0.50	136.0
(YELLOW)	16		25	0.25	100.0
(GREEN)	16		15	0.25	60.0
ARROW	8		12	0.10	9.6
PED. SIGNAL	8		25	1.00	200.0
CONTROLLER	1		100	1.00	100.0
VIDEO SYSTEM	1	150		1.00	150.0
ENERGY COSTS TO: TOTAL =					755.6

ILLINOIS DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAY/DISTRICT 1  
201 W CENTER COURT/SCHAUMBURG, ILLINOIS 60196-1096  
ENERGY SUPPLY CONTACT: KAREN RICHMOND  
PHONE: (630) 723-2128  
COMPANY: ComED

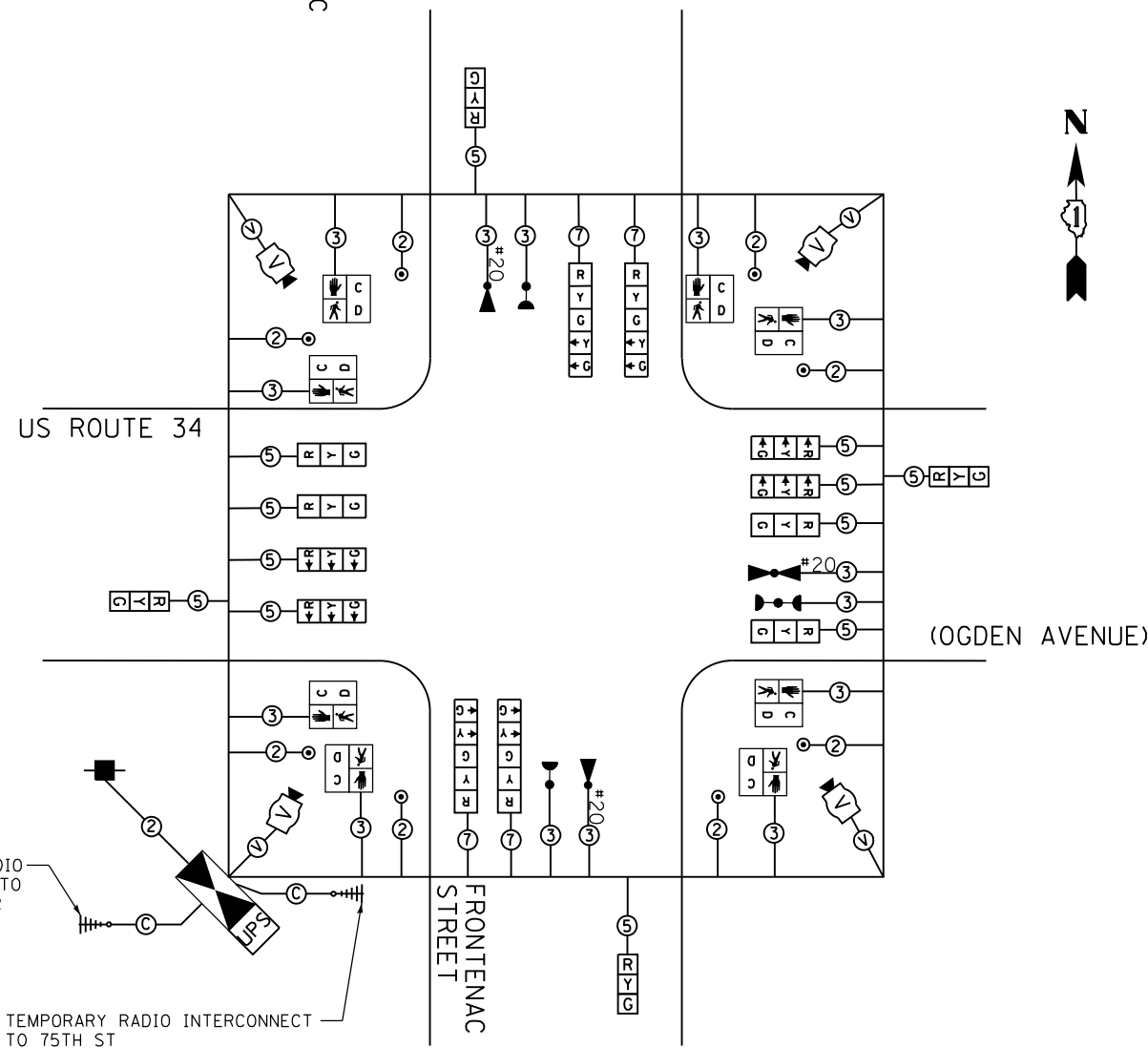
**TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE**



TEMPORARY EMERGENCY VEHICLE PREEMPTORS			
EMERGENCY VEHICLE PREEMPTOR	3	4	5
MOVEMENT			



**TEMPORARY VIDEO DETECTION MOUNTING DETAIL**  
(NOT TO SCALE)



**TEMPORARY CABLE PLAN**  
PRE-STAGE, FINAL GEOMETRICS

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

THE RESIDENT ENGINEER SHALL DETERMINE ON LOCATION DURING CONSTRUCTION IF THE PEDESTRIAN SIGNAL CAN BE MAINTAINED.



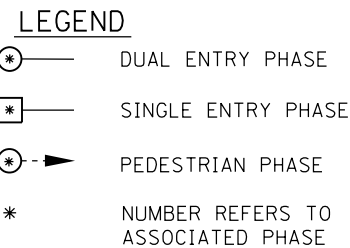
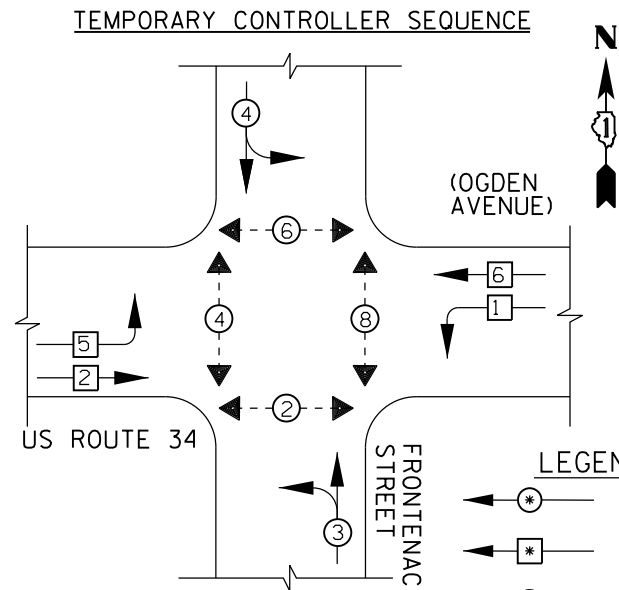
USER NAME = URS	DESIGNED - MB	REVISED -
	DRAWN - KJB	REVISED -
PLOT SCALE = 48.0000' / in.	CHECKED - SPF	REVISED -
PLOT DATE = 02/28/2014	DATE - 02/28/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**TEMPORARY SIGNAL CABLE PLAN**  
PRE-STAGE, FINAL GEOMETRICS  
US 34 (OGDEN AVENUE) AND FRONTENAC STREET

SCALE: NTS SHEET NO. X OF X SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	154
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**TEMPORARY PHASE DESIGNATION DIAGRAM**  
STAGE 1A, 1B, 1C, 2A, 2B, 2C, 3

**CONSTRUCTION NOTES:**

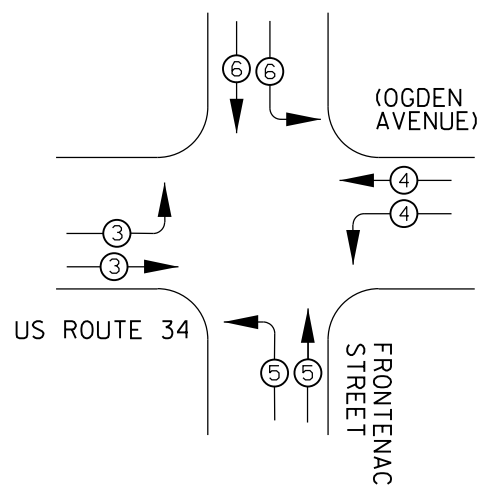
1. THE EASTBOUND AND WESTBOUND U.S. ROUTE 34 LEFT TURN PHASES SHALL OPERATE LEAD/LAG FOR ALL STAGES OF CONSTRUCTION.

RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

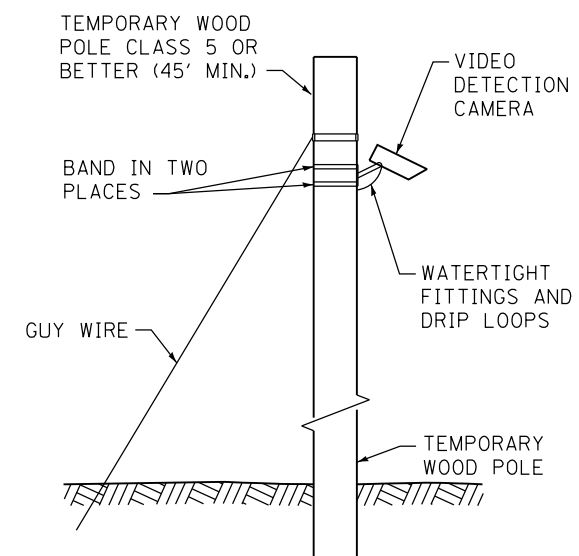
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I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	LED	%OPERATION	
SIGNAL (RED)	20		17	0.50	120.0
(YELLOW)	20		25	0.25	125.0
(GREEN)	24		15	0.25	75.0
ARROW	0		12	0.10	0
PED. SIGNAL	8		25	1.00	200.0
CONTROLLER	1		100	1.00	100.0
VIDEO SYSTEM	1	150		1.00	150.0
<b>ENERGY COSTS TO:</b>					<b>TOTAL = 720.0</b>
ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAY/DISTRICT 1 201 W CENTER COURT/SCHAUMBURG, ILLINOIS 60196-1096 ENERGY SUPPLY CONTACT: KAREN RICHMOND PHONE: (630) 723-2128 COMPANY: ComED					

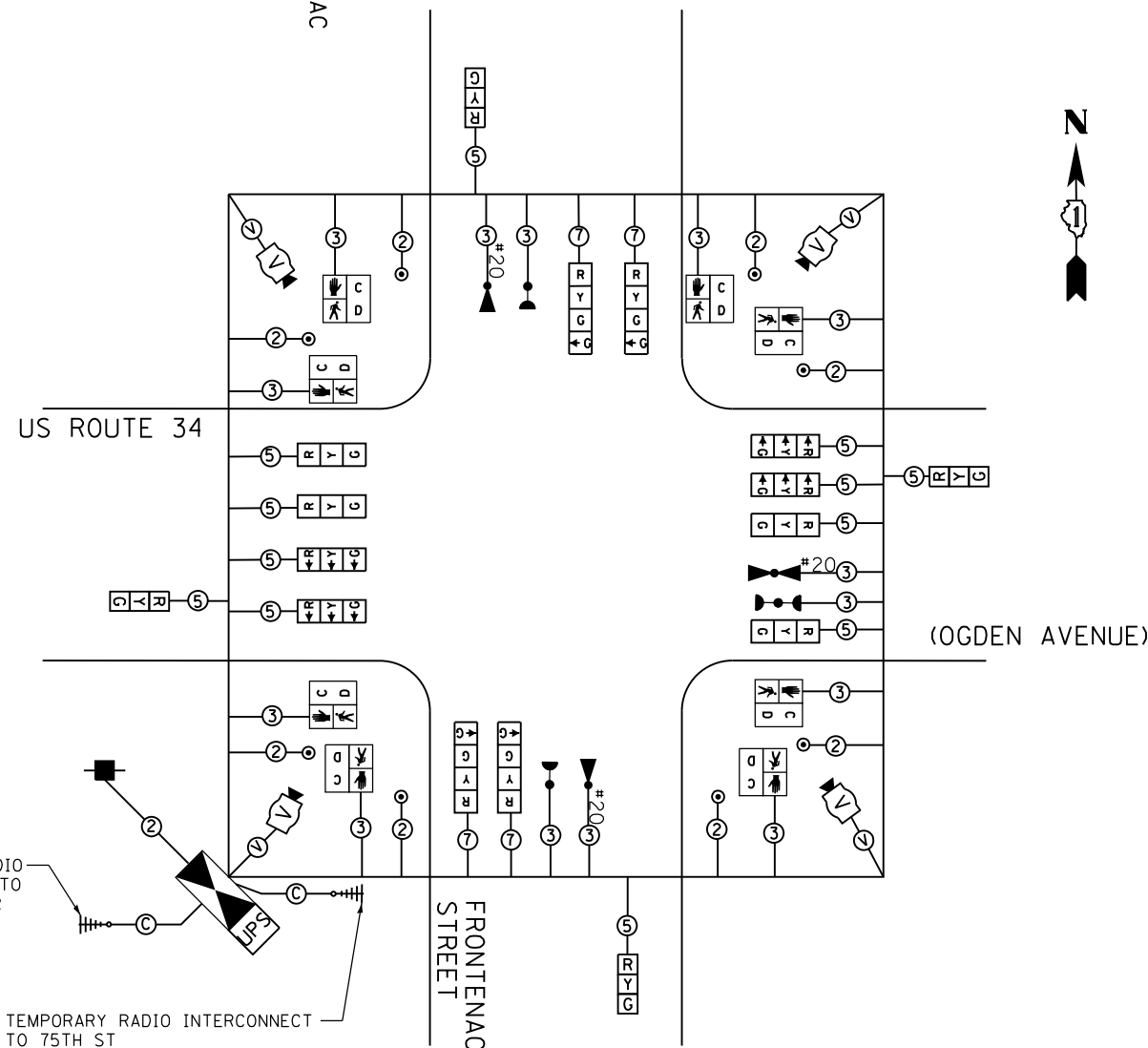
**TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE**



TEMPORARY EMERGENCY VEHICLE PREEMPTORS				
EMERGENCY VEHICLE PREEMPTOR	3	4	5	6
MOVEMENT				



**TEMPORARY VIDEO DETECTION MOUNTING DETAIL**  
(NOT TO SCALE)



**TEMPORARY CABLE PLAN**  
STAGE 1, 2, 3

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

THE RESIDENT ENGINEER SHALL DETERMINE ON LOCATION DURING CONSTRUCTION IF THE PEDESTRIAN SIGNAL CAN BE MAINTAINED.



USER NAME = URS	DESIGNED - MB	REVISED -
	DRAWN - KJB	REVISED -
PLOT SCALE = 48.0000' / in.	CHECKED - SPF	REVISED -
PLOT DATE = 02/28/2014	DATE - 02/28/2014	REVISED -

**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

**TEMPORARY SIGNAL CABLE PLAN**  
STAGE 1, 2, 3  
**US 34 (OGDEN AVENUE) AND FRONTENAC STREET**

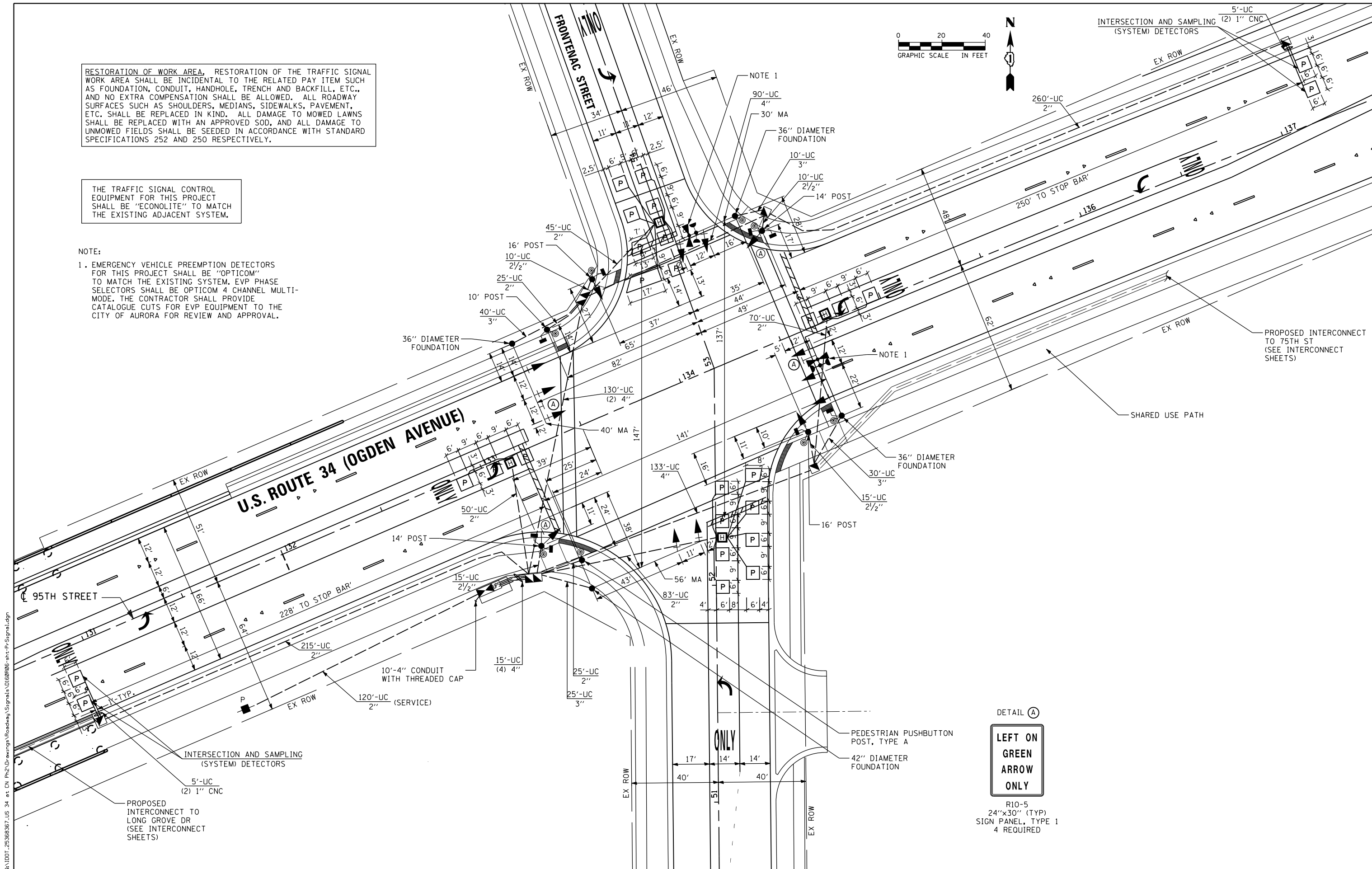
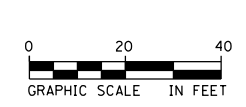
SCALE: NTS SHEET NO. X OF X SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	155
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

NOTE:  
 1. EMERGENCY VEHICLE PREEMPTION DETECTORS FOR THIS PROJECT SHALL BE "OPTICOM" TO MATCH THE EXISTING SYSTEM. EVP PHASE SELECTORS SHALL BE OPTICOM 4 CHANNEL MULTI-MODE. THE CONTRACTOR SHALL PROVIDE CATALOGUE CUTS FOR EVP EQUIPMENT TO THE CITY OF AURORA FOR REVIEW AND APPROVAL.



DETAIL (A)  
**LEFT ON GREEN ARROW ONLY**  
 R10-5  
 24"x30" (TYP)  
 SIGN PANEL, TYPE 1  
 4 REQUIRED

FILE NAME = Q:\100T\_25368367\_US 34.ctb; CN Ph2; Drawings; Roadway; Signals; 0168706; sht-Fr-Signal.dgn



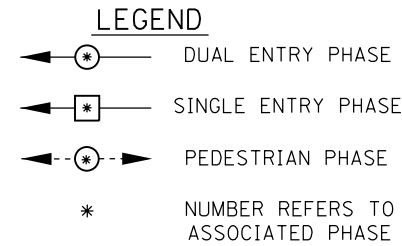
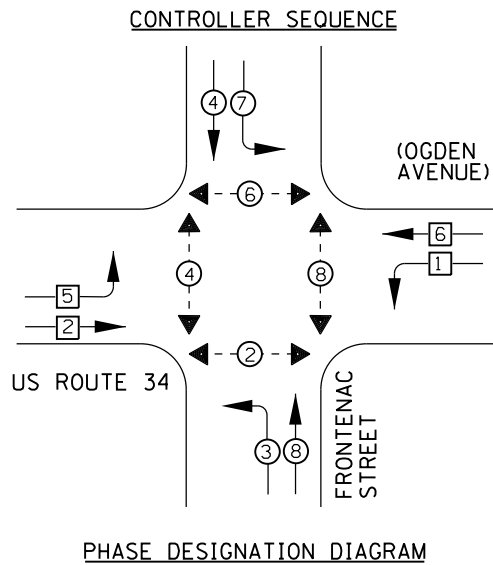
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DRAWN - KJB	REVISED -	
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PLOT DATE = 02/28/2014	DATE - 02/28/2014	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

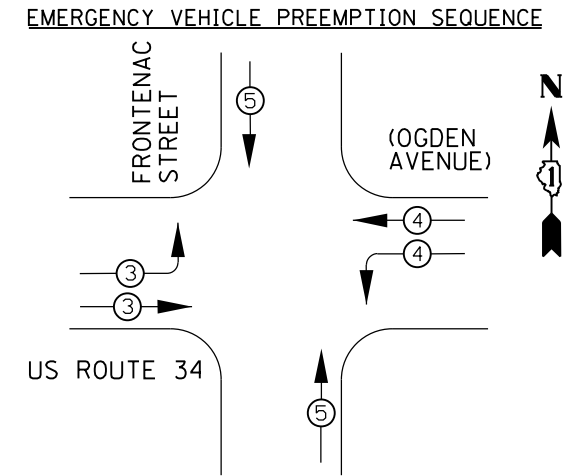
TRAFFIC SIGNAL INSTALLATION PLAN  
 US 34 (OGDEN AVENUE) AND FRONTENAC STREET  
 SCALE: 1"=20' SHEET NO. 1 OF 1 SHEETS STA. 131+50 TO STA. 137+50

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	156
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



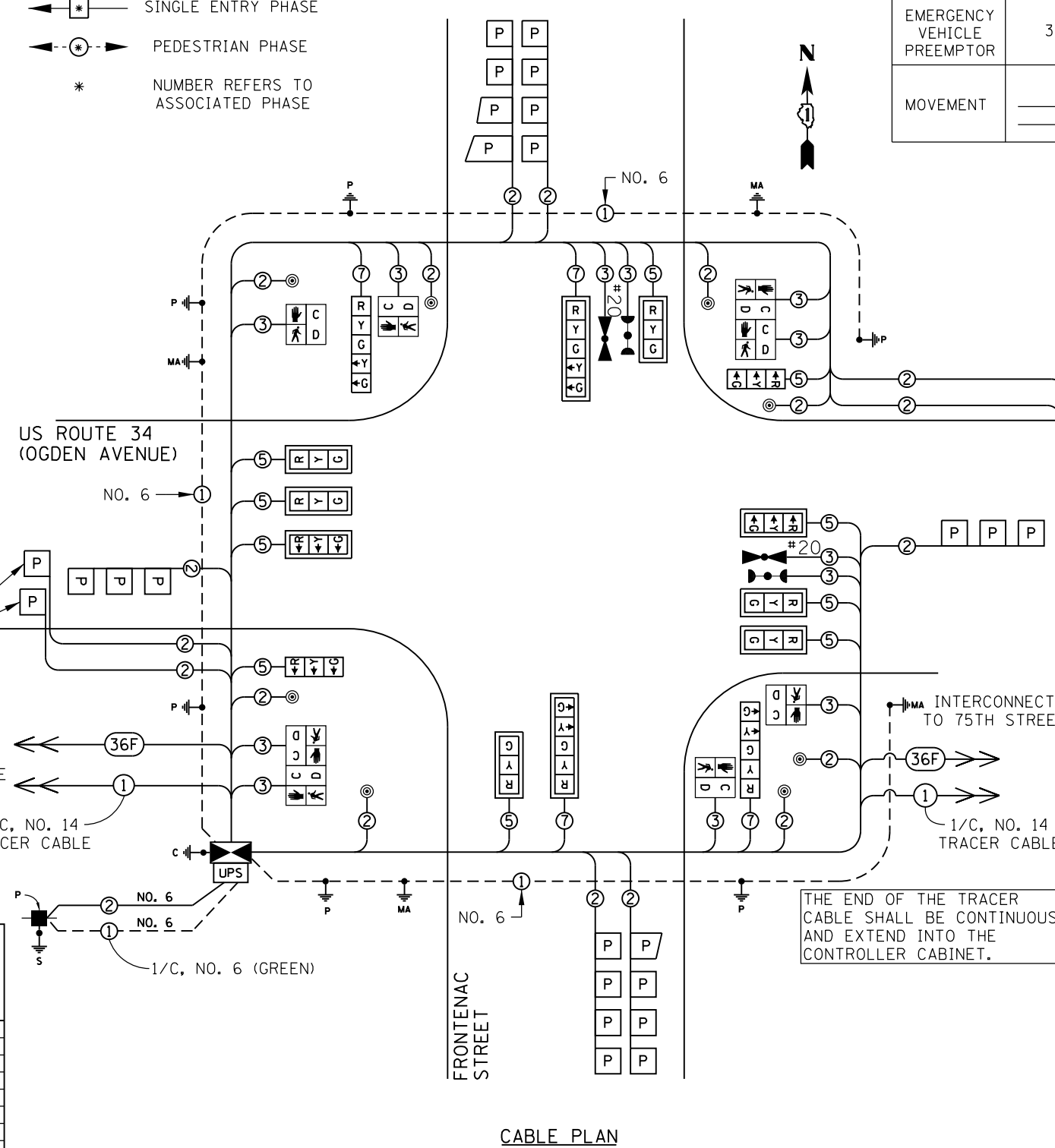


PROPOSED EMERGENCY VEHICLE PREEMPTORS			
EMERGENCY VEHICLE PREEMPTOR	3	4	5
MOVEMENT			



SCHEDULE OF QUANTITIES

PAY ITEM	UNITS	QNTY.
SERVICE INSTALLATION - POLE MOUNTED	EACH	1
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	1388
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	50
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	105
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	553
HANDHOLE	EACH	8
HEAVY-DUTY HANDHOLE	EACH	4
DOUBLE HANDHOLE	EACH	2
FULL-ACTUATED CONTROLLER AND TYPE IV CABINET (SPECIAL)	EACH	1
UNINTERRUPTIBLE POWER SUPPLY, EXTENDED	EACH	1
TRANSCIVER - FIBER OPTIC	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 2C	FOOT	1,532
ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 3C	FOOT	2,170
ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 5C	FOOT	2,360
ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 7C	FOOT	880
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	2,591
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C	FOOT	153
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	777
TRAFFIC SIGNAL POST, GALVANIZED STEEL 10 FT.	EACH	1
TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.	EACH	2
TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	2
PEDESTRIAN PUSHBUTTON POST, TYPE A	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 30 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 36 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 40 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 56 FT.	EACH	1
CONCRETE FOUNDATION, TYPE A	FOOT	24
CONCRETE FOUNDATION, TYPE C	FOOT	4
CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	35
CONCRETE FOUNDATION, TYPE E 42-INCH DIAMETER	FOOT	21
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	8
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	2
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	2
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	2
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4
PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	2
TRAFFIC SIGNAL BACKPLATE, LOUVERED	EACH	10
INDUCTIVE LOOP DETECTOR	EACH	10
PREFORMED DETECTOR LOOP	FOOT	942
LIGHT DETECTOR	EACH	2
LIGHT DETECTOR AMPLIFIER	EACH	1
PEDESTRIAN PUSH-BUTTON	EACH	8
TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING HANDHOLE	EACH	10
REMOVE EXISTING DOUBLE HANDHOLE	EACH	1
REMOVE EXISTING CONCRETE FOUNDATION	EACH	9
ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED	FOOT	592
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 2	EACH	1
TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1



I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	WATTAGE LED	% OPERATION	
SIGNAL (RED)	10		17	0.50	85.0
(YELLOW)	10		25	0.25	62.5
(GREEN)	10		15	0.25	37.5
ARROW	20		12	0.10	24.0
PED. SIGNAL	8		25	1.00	200.0
CONTROLLER	1		100	1.00	100.0
ILLUM. SIGN	-		25	0.50	-
<b>TOTAL =</b>					<b>509.0</b>

ENERGY COSTS TO:  
ILLINOIS DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAY/DISTRICT 1  
201 W CENTER COURT/SCHAUMBURG, ILLINOIS 60196-1096  
ENERGY SUPPLY CONTACT: KAREN RICHMOND  
PHONE: (630) 723-2128  
COMPANY: ComED



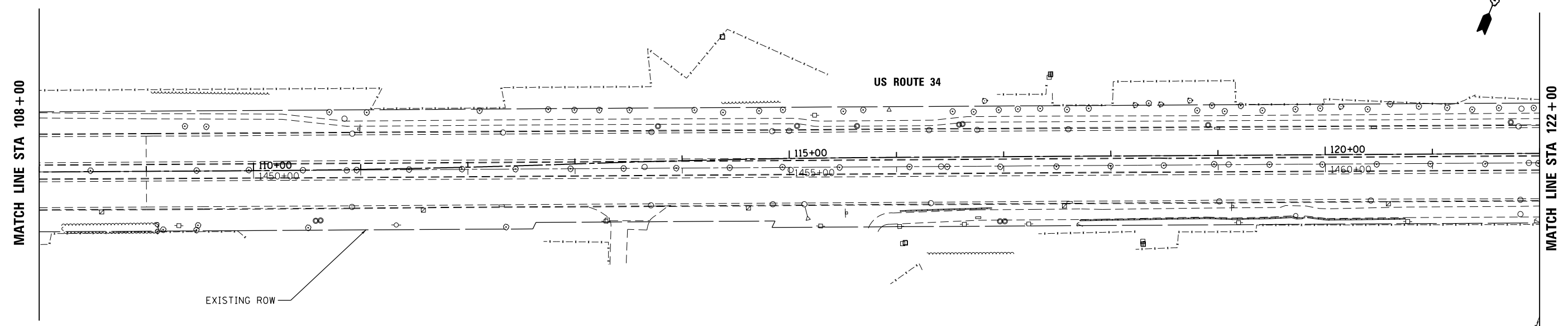
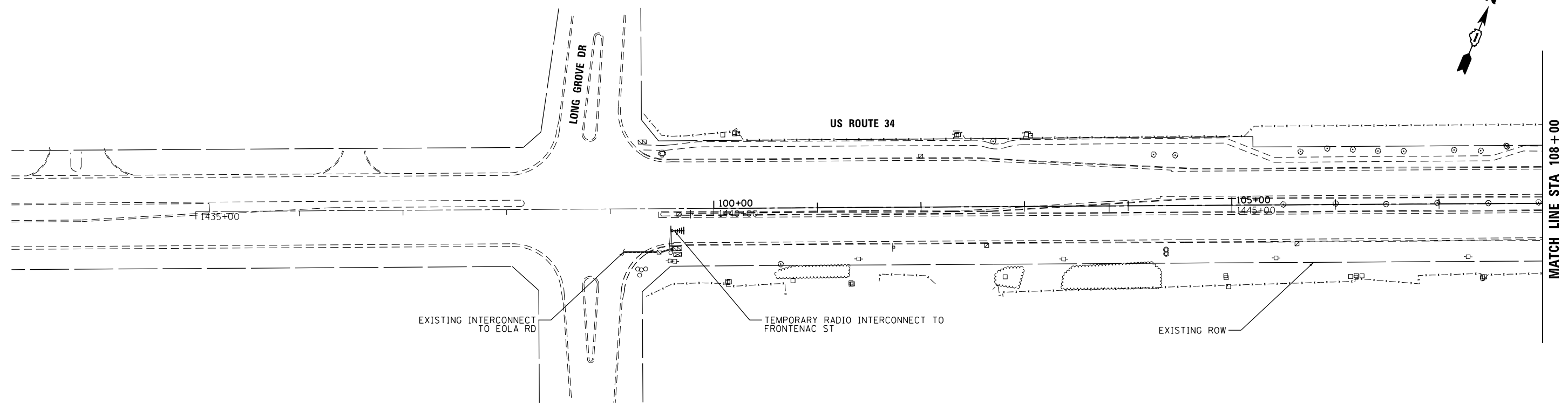
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DESIGNED - MB	DRAWN - KJB	REVISED -
PLOT SCALE = 48.0000' / in.	CHECKED - SPF	REVISED -
PLOT DATE = 02/28/2014	DATE - 02/28/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SIGNAL SCHEDULE OF QUANTITIES AND CABLE PLAN			
US 34 (OGDEN AVENUE) AND FRONTENAC STREET			
SCALE: NTS	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	157
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FILE NAME = Q:\1\DOT\_25368367\_US 34 at CN Ph2\Drawings\Roadway\Signals\0169706-sht-Fr-CablePlan.dgn



FILE NAME = Q:\DOT\_25368367\_US\_34.ctb CN Ph2\Drawings\Roadway\Signals\168706-Sht-Interconnect\_Temp-Plan.dgn



USER NAME = URS	DESIGNED - MB	REVISED -
DRAWN - KJB	REVISOR -	
PLOT SCALE = 100.0000' / in.	CHECKED - SPF	REVISOR -
PLOT DATE = 02/28/2014	DATE - 02/28/2014	REVISOR -

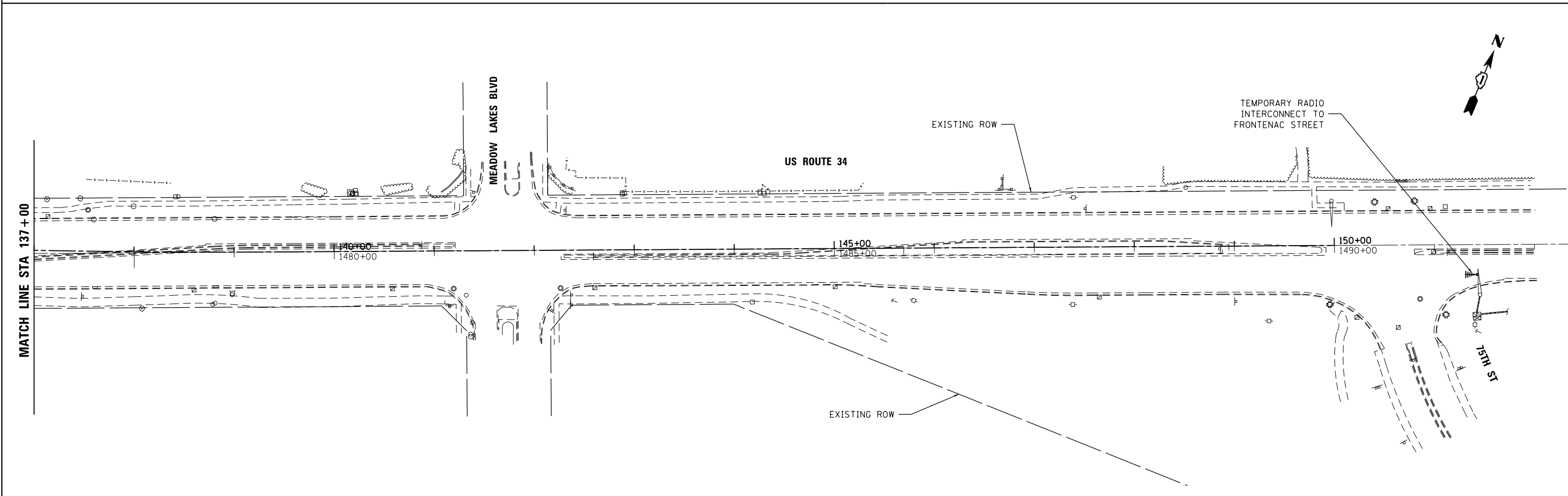
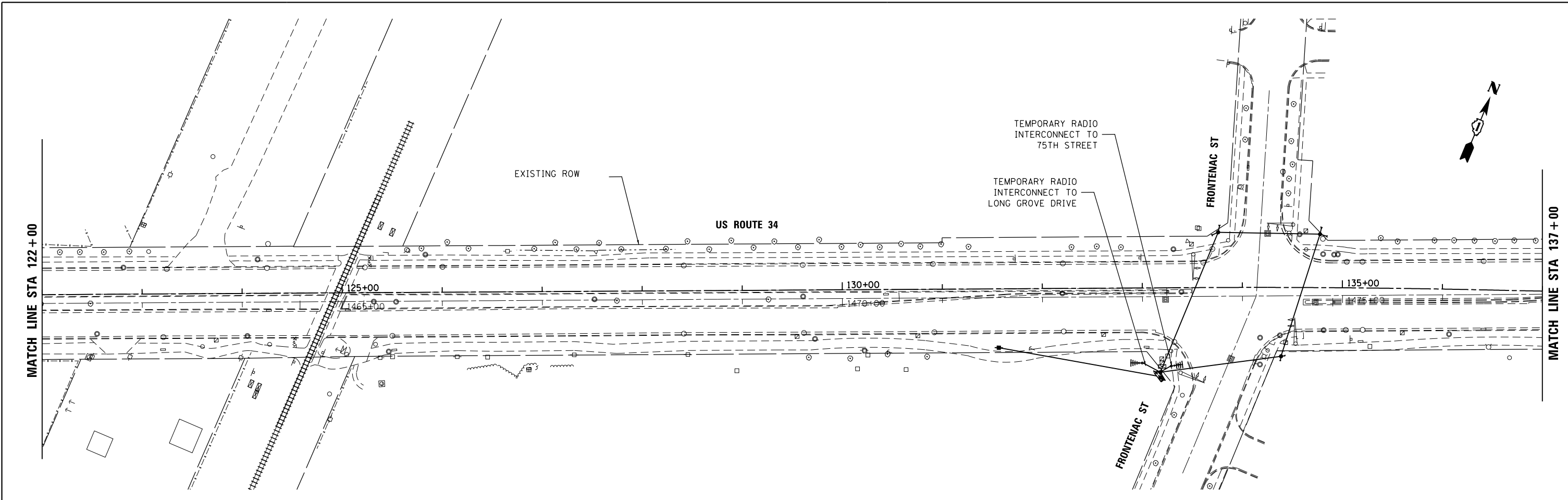
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY INTERCONNECT PLAN  
US ROUTE 34 (OGDEN AVENUE)  
FROM LONG GROVE ROAD TO 75TH STREET**

SCALE: 1"=50'    SHEET NO. 1 OF 2 SHEETS    STA. 100+00 TO STA. 122+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	158
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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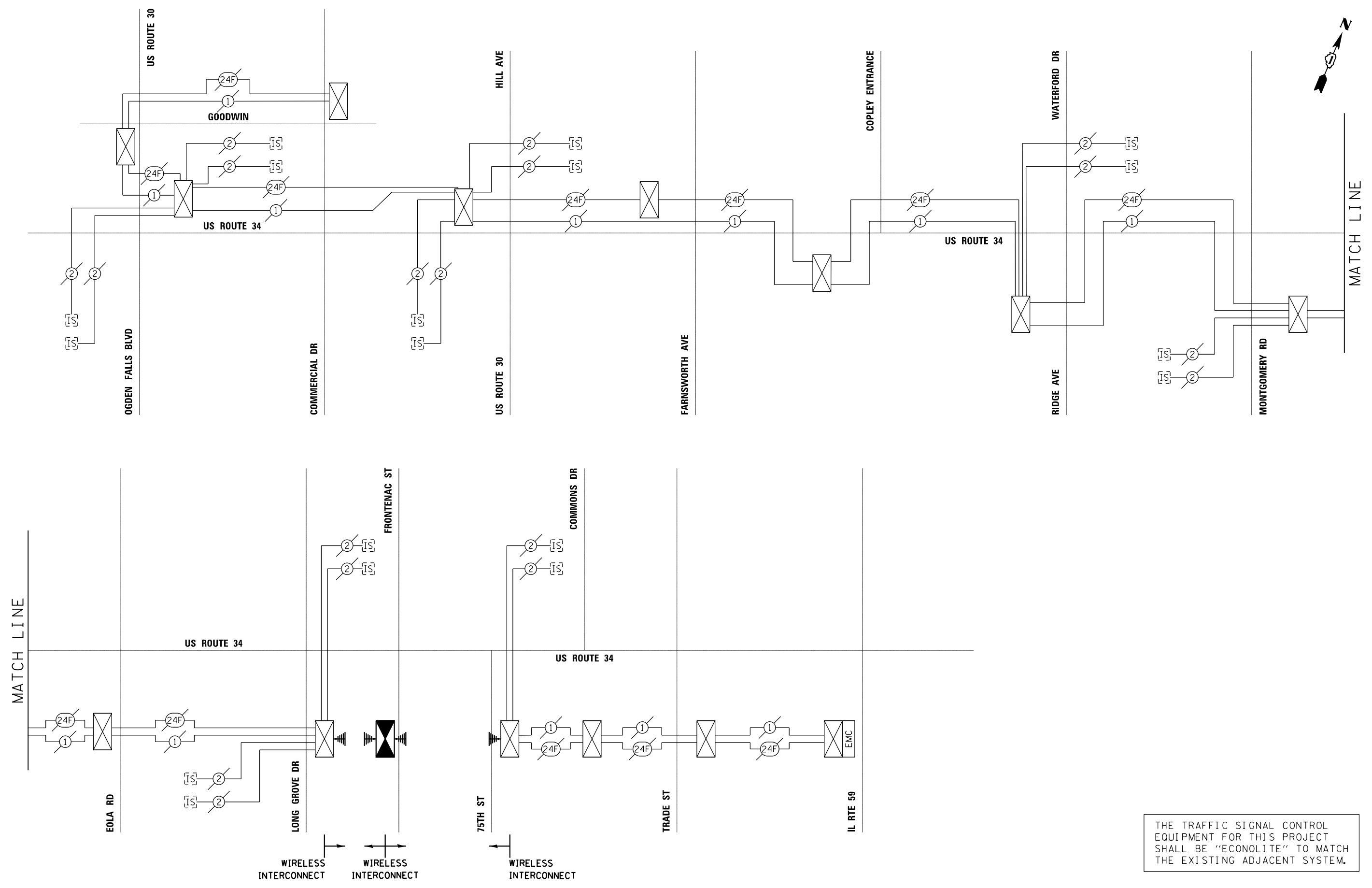
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DRAWN - KJB	REVISED -	
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PLOT DATE = 02/28/2014	DATE - 02/28/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY INTERCONNECT PLAN  
US ROUTE 34 (OGDEN AVENUE)  
FROM LONG GROVE ROAD TO 75TH STREET**

SCALE: 1"=50'    SHEET NO. 2 OF 2 SHEETS    STA. 122+00 TO STA. 150+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	159
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

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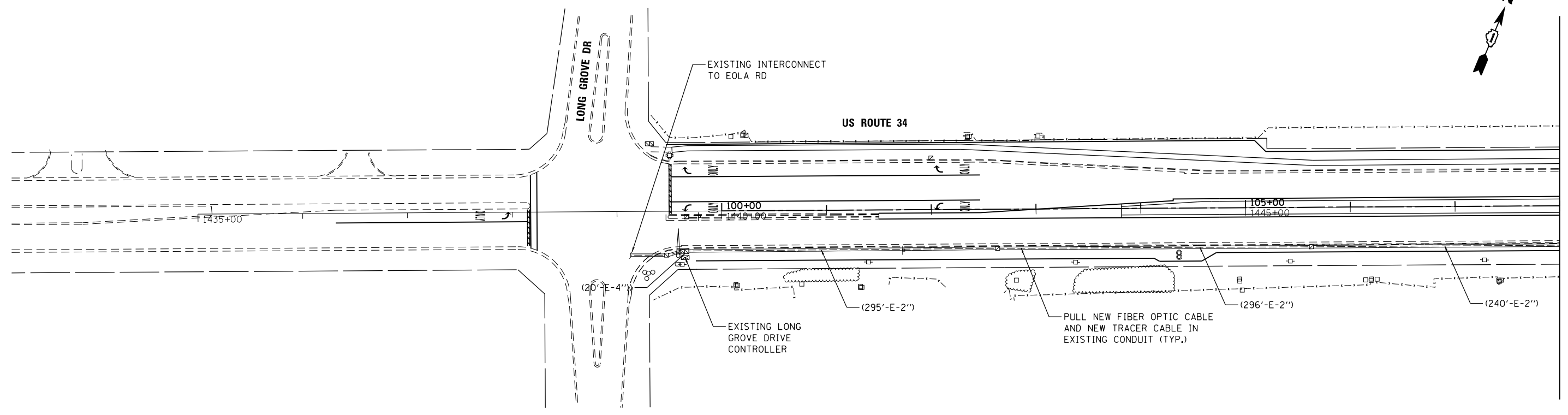
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

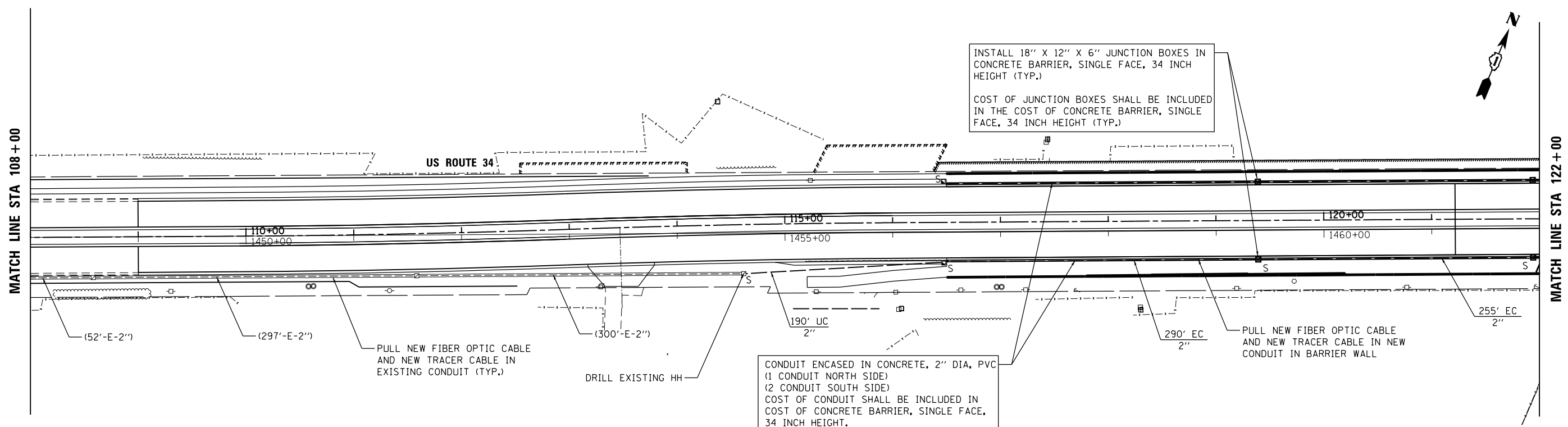
**TEMPORARY INTERCONNECT SCHEMATIC  
US ROUTE 34 FROM OGDEN FALLS BLVD / US ROUTE 30  
TO IL ROUTE 59**

SCALE: NTS SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	160
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



MATCH LINE STA 108+00



MATCH LINE STA 108+00

MATCH LINE STA 122+00

FILE NAME = Q:\DOT\_25368367\_US\_34.ctb CN Ph2\Drawings\Roadway\Signals\168706-Sht-Interconnect\_Prop-Plan.dgn



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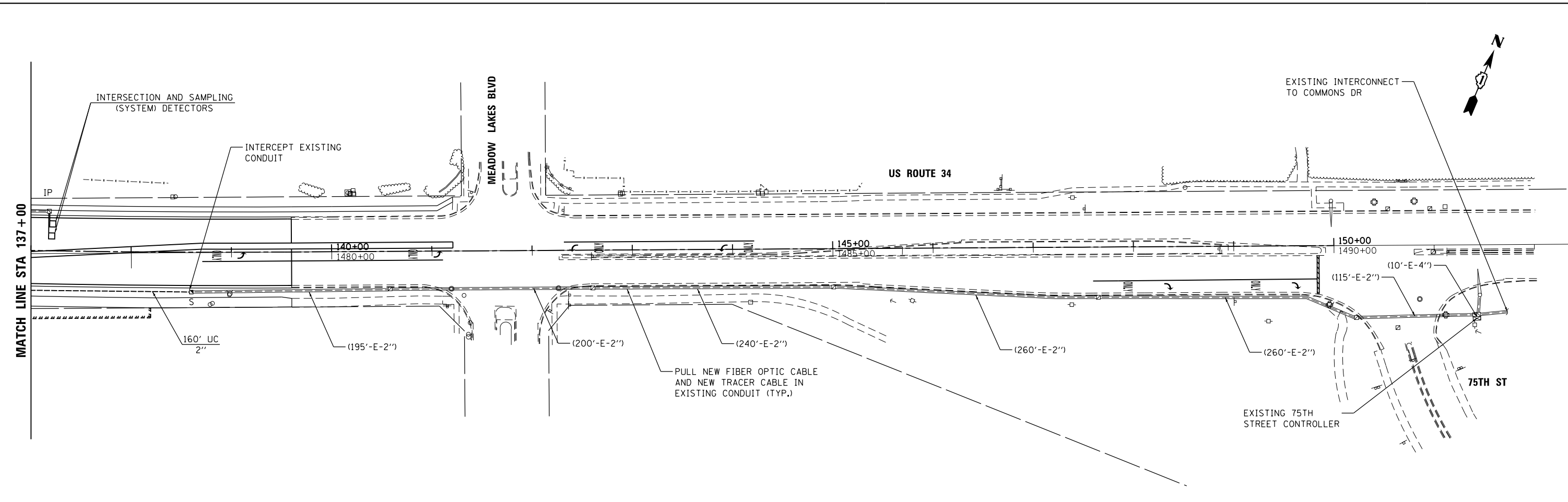
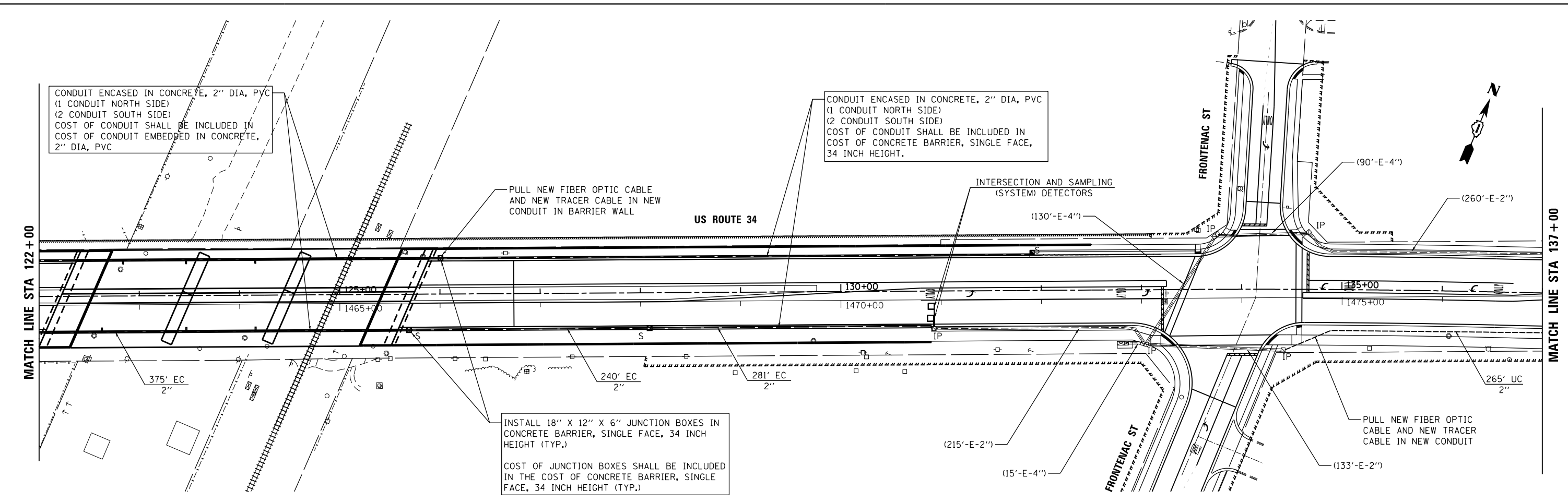
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROPOSED INTERCONNECT PLAN  
US ROUTE 34 (OGDEN AVENUE)  
FROM LONG GROVE ROAD TO 75TH STREET**

SCALE: 1"=50' SHEET NO. 1 OF 2 SHEETS STA. 100+00 TO STA. 122+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	161
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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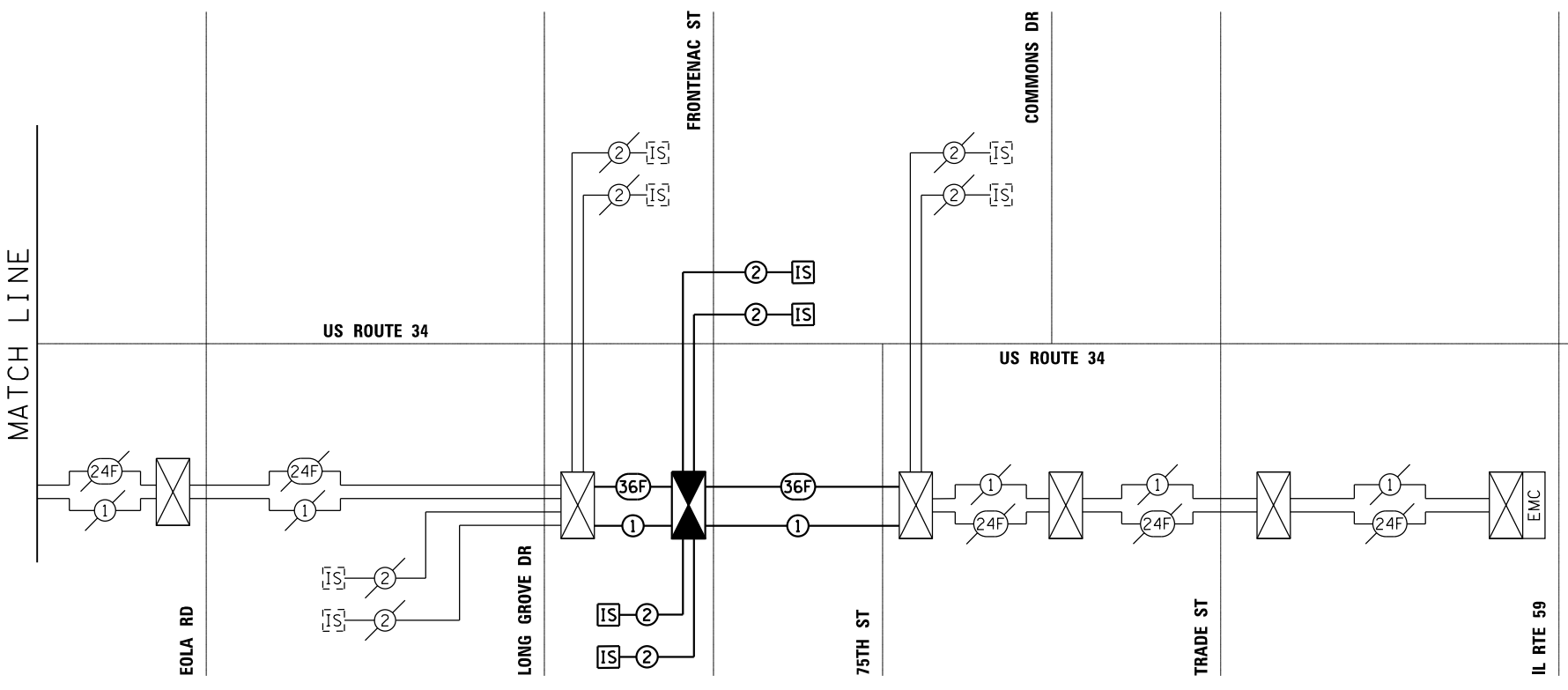
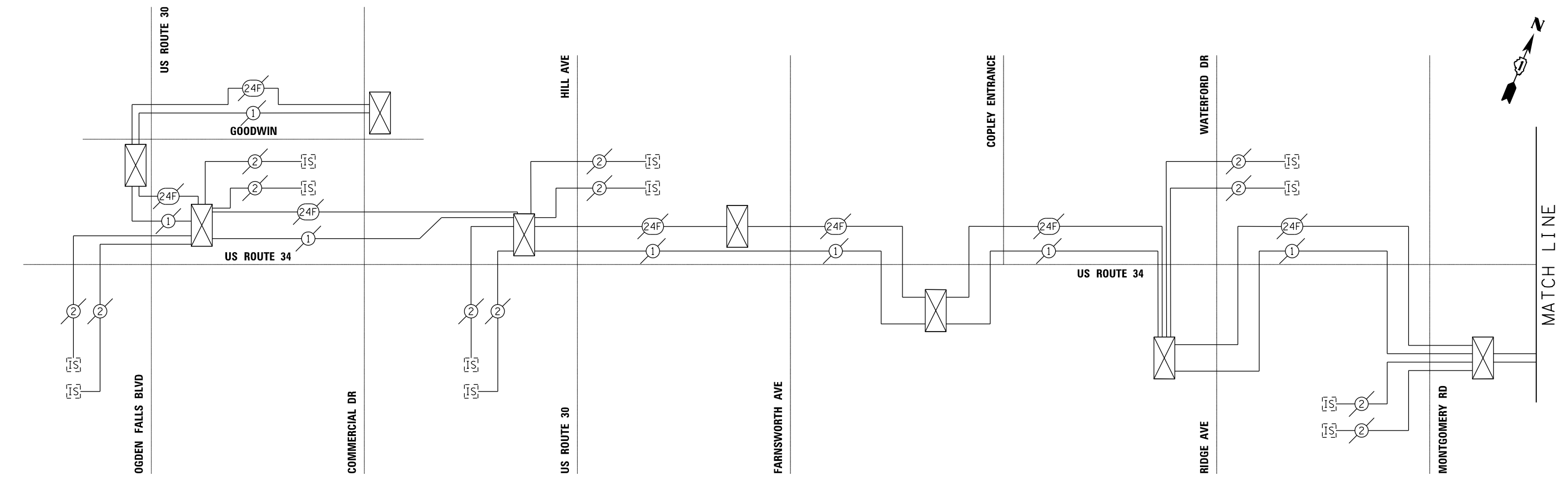
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DRAWN - KJB	REVISED -	
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PLOT DATE = 02/28/2014	DATE - 02/28/2014	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROPOSED INTERCONNECT PLAN  
US ROUTE 34 (OGDEN AVENUE)  
FROM LONG GROVE ROAD TO 75TH STREET**

SCALE: 1"=50'    SHEET NO. 2 OF 2 SHEETS    STA. 122+00 TO STA. 150+00

F.A.P. RTE. 311	SECTION 652-A	COUNTY DUPAGE	TOTAL SHEETS 383	SHEET NO. 162
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

SCHEDULE OF QUANTITIES

PAY ITEM	UNITS	QNTY.
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	615
HANDHOLE	EACH	4
FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F	FOOT	5,447
ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	5,447
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	5,447
CONDUIT ENCASED IN CONCRETE, 2" DIA, PVC	FOOT	780

(CABLE LENGTHS INCLUDE SLACKS OF 6.5' IN HH, AND 13' IN DHH AND CABINETS)

FILE NAME = Q:\DOT\_25368367\_US\_34.ctb\Drawings\Roadway\Signals\161806-Sht-Inter-connect\_Prop-Schem.dgn



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	DRAWN - KJB	REVISED -
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PLOT DATE = 02/28/2014	DATE - 02/28/2014	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**INTERCONNECT SCHEMATIC**  
**US ROUTE 34 FROM OGDEN FALLS BLVD / US ROUTE 30**  
**TO IL ROUTE 59**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	163
CONTRACT NO. 60R06				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: NTS SHEET NO. 1 OF 1 SHEETS STA. TO STA.

Benchmark: CP#3 - P.K. Rod in ground at driveway next to Nicor Gas pressure regulation station. Sta. 113+67.17, 50.22' Rt. Elev. 706.54.

Existing Structure: None. One lane of traffic in both directions to be maintained during construction utilizing stage construction and Temporary MSE Walls.

Salvage: None.

STATION 124+94.76  
BUILT 20\_\_ BY  
STATE OF ILLINOIS  
F.A. RT. 311 SEC. 652-A  
LOADING HL-93  
STR. NO. 022-0512

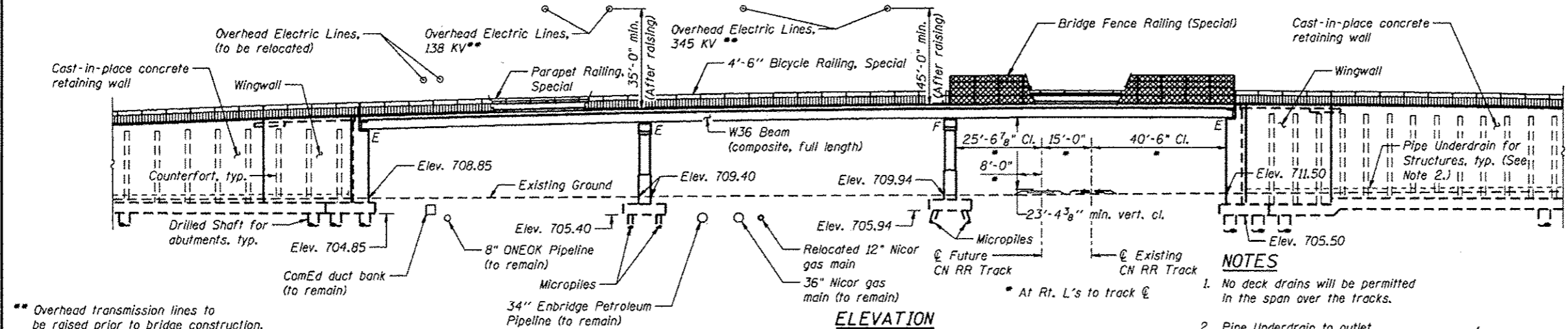
**NAME PLATE**  
See Std. 515001

**LOADING HL-93**  
Allow 50#/sq. ft. for future wearing surface.

**DESIGN SPECIFICATIONS**  
2012 AASHTO LRFD Bridge Design Specifications, 6th Edition

**DESIGN STRESSES**  
**FIELD UNITS**  
f'c = 3,500 psi  
fy = 60,000 psi (Reinforcement)  
fy = 50,000 psi (M270 Grade 50)

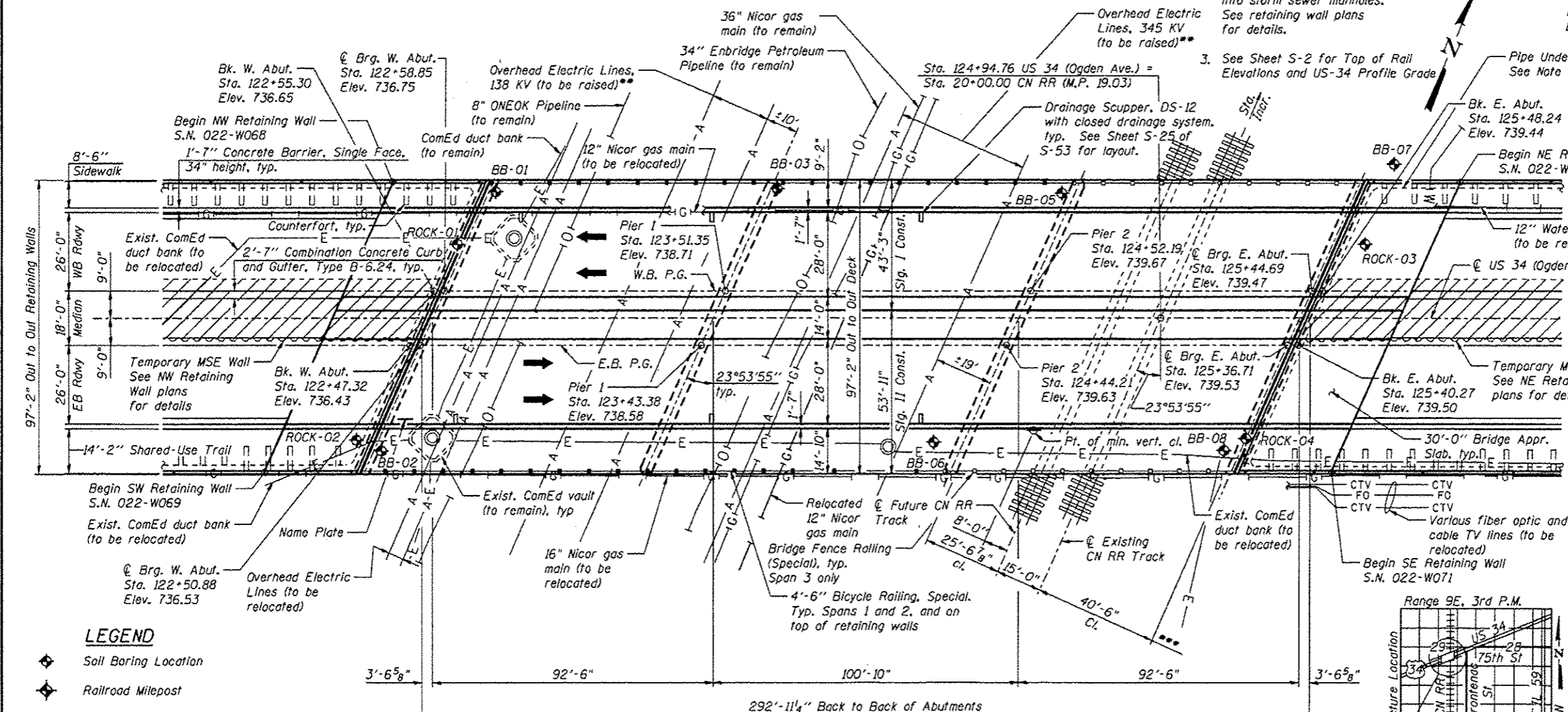
**SEISMIC DATA**  
Seismic Performance Zone (SPZ) = 1  
Design Spectral Acceleration at 1.0 sec. (Sp1) = 0.065  
Design Spectral Acceleration at 0.2 sec. (Sp5) = 0.123  
Soil Site Class = C



\*\* Overhead transmission lines to be raised prior to bridge construction.

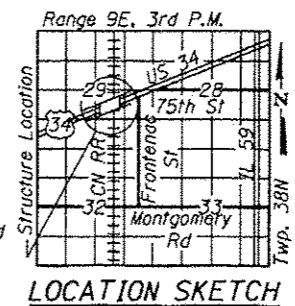
**NOTES**

- No deck drains will be permitted in the span over the tracks.
- Pipe Underdrain to outlet into storm sewer manholes. See retaining wall plans for details.
- See Sheet S-2 for Top of Rail Elevations and US-34 Profile Grade



**PLAN**

M.P. 19 Sta. 18+50 \*\*\* Existing and future CN tracks may be shifted ±7'-6" to the west after the project is built to accommodate two future metra tracks of 15'-0" spacing to east of CN RR tracks.



*Arsalan M. Khan*  
ARSALAN M. KHAN, S.E.  
LICENSE NO.: 081-006258  
EXPIRES: NOVEMBER 30, 2014  
DATE: 02-26-2014

**APPROVED**  
For Structural Adequacy Only  
*A. Carl Ferguson*  
Engineer of Bridges & Structures

**GENERAL PLAN & ELEVATION**  
**US 34 (OGDEN AVE.) OVER CN RR**  
F.A.P. RTE. 311 - SECTION 652-A  
DUPAGE COUNTY  
STATION 124+94.76  
STRUCTURE NO. 022-0512



DESIGNED - PMV/KMP/BAR	REVISED
CHECKED - AMK	REVISED
DRAWN - PMV	REVISED
CHECKED - AMK	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SHEET NO. S-1 OF S-53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	154
			CONTRACT NO. 60R06	

ILLINOIS FED. AID PROJECT



**GENERAL NOTES**

- Fasteners shall be ASTM A325 Type 1, mechanically galvanized bolts. Bolts  $\frac{7}{8}$  in.  $\phi$ , holes  $\frac{15}{16}$  in.  $\phi$ , unless otherwise noted.
- Calculated weight of Structural Steel =  
 AASHTO M270 Grade 50 = 985,119 lbs  
 AASHTO M270 Grade 36 = 83,866 lbs
- No field welding is permitted except as specified in the contract documents.
- Reinforcement bars designated (E) shall be epoxy coated.
- Bearing seat surfaces shall be constructed or adjusted to the designated elevations within a tolerance of  $\frac{1}{8}$  in. (0.01 ft.). Adjustment shall be made either by grinding the surface or by shimming the bearings.
- Concrete Sealer shall be applied to the designated areas of the abutments.
- All Structural Steel shall be galvanized according to the Special Provision "Hot Dip Galvanizing of Structural Steel." Cost included with Furnishing Structural Steel.
- The Contractor shall design, furnish and install protective armoring systems before operating any equipment over any underground pipelines or gas mains. Armoring system designs are to be submitted for review and approval by each utility owner prior to installation.
- There are existing 138 kV and 345 kV lines within the project limits, the contractor needs to be aware that no outage or protection can be provided to these facilities. The plans include pay items for micropiles for this reason. Per ComEd and OSHA requirements, without the on-site presence of a representative of ComEd, all contract activities must stay 30 feet in any direction from the 345 kV lines and any ComEd conductors and 20 feet from the 138 kV lines. With on-site ComEd supervision, all contract activities must stay 20 feet in any direction from the 345 kV lines and any ComEd conductors and 13 feet from the 138 kV lines.
- The maximum height above the proposed top of bridge deck that equipment will be able to operate without de-energizing overhead ComEd transmission lines and without the onsite presence of a ComEd representative is estimated to be 15 feet.
- Form liner textured surface shall be applied to the wingwalls, retaining walls and noise abatement walls. The finish shall consist of a rolled Ashlar Stone finish and shall have a minimum 0.75 in impression.
- The pattern produced on the exposed surfaces of the concrete wingwalls, retaining walls and the noise abatement walls shall be identical. The concrete stain applied to the surfaces of all of those items shall also be identical. The Contractor shall provide all necessary coordination and direction with its vendor manufacturing the noise abatement walls to assure the same concrete form liners and stain are used for them as those for the cast-in-place wingwalls and retaining walls.

**FOR INFORMATION ONLY**

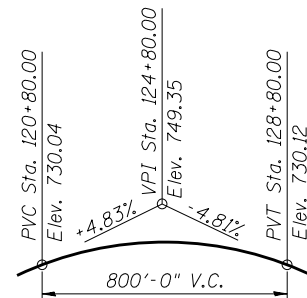
**INDEX OF SHEETS**

- S-1 General Plan and Elevation
- S-2 General Notes, Index of Sheets and Bill of Material
- S-3 Footing Layout
- S-4 Stage Construction Details
- S-5 Temporary Concrete Barrier for Stage Construction
- S-6 Top of Slab Elevations 1
- S-7 Top of Slab Elevations 2
- S-8 Top of Slab Elevations 3
- S-9 Top of Slab Elevations 4
- S-10 Top of Slab Elevations 5
- S-11 Top of Slab Elevations 6
- S-12 Top of Slab Elevations 7
- S-13 Top of Slab Elevations 8
- S-14 Top of West Approach Slab Elevations
- S-15 Top of East Approach Slab Elevations
- S-16 Superstructure Plan and Cross Section
- S-17 Parapet Elevations and Details
- S-18 Deck Details
- S-19 Bridge Approach Slab Details 1
- S-20 Bridge Approach Slab Details 2
- S-21 Concrete Parapet Slipforming Option
- S-22 Bridge Fence Railing (Special)
- S-23 Bicycle Railing, Special
- S-24 Drainage Scupper, DS-12
- S-25 Drainage System
- S-26 Preformed Joint Strip Seal
- S-27 Framing Plan and Beam Elevation
- S-28 Steel Details
- S-29 Bearing Details 1
- S-30 Bearing Details 2
- S-31 West Abutment 1
- S-32 West Abutment 2
- S-33 West Abutment Details
- S-34 East Abutment 1
- S-35 East Abutment 2
- S-36 East Abutment Details
- S-37 Drilled Shafts and Miscellaneous Details
- S-38 Pier 1
- S-39 Pier 2
- S-40 Pier Details
- S-41 Micropile Details
- S-42 Bar Splicer Assembly and Mechanical Splicer Details
- S-43 Soil Boring Logs 1
- S-44 Soil Boring Logs 2
- S-45 Soil Boring Logs 3
- S-46 Soil Boring Logs 4
- S-47 Soil Boring Logs 5
- S-48 Soil Boring Logs 6
- S-49 Soil Boring Logs 7
- S-50 Soil Boring Logs 8
- S-51 Soil Boring Logs 9
- S-52 Soil Boring Logs 10
- S-53 Soil Boring Logs 11

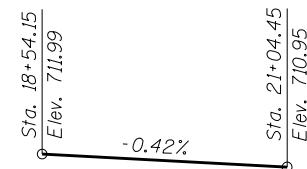
**TOTAL BILL OF MATERIAL**

Item	Unit	Total
Structure Excavation	Cu. Yd.	1,448
Concrete Structures	Cu. Yd.	2,340.3
Concrete Superstructure	Cu. Yd.	1,228.7
Bridge Deck Grooving	Sq. Yd.	2,033
Form Liner Textured Surface	Sq. Ft.	3,159
Protective Coat	Sq. Yd.	4,915
Erecting Structural Steel	L. Sum	1
Stud Shear Connectors	Each	19,008
Reinforcement Bars	Pound	197,600
Reinforcement Bars, Epoxy Coated	Pound	507,780
Bar Splicers	Each	1,350
Bicycle Railing, Special	Foot	516
Bridge Fence Railing (Special)	Foot	188
Parapet Railing, Special	Foot	702
Name Plates	Each	1
Drilled Shaft in Soil	Cu. Yd.	1,092
Drilled Shaft in Rock	Cu. Yd.	59
Preformed Joint Strip Seal	Foot	213
Erecting Elastomeric Bearing Assembly, Type I	Each	32
Erecting Elastomeric Bearing Assembly, Type II	Each	16
Anchor Bolts, 1"	Each	64
Anchor Bolts, 1 1/2"	Each	64
Concrete Sealer	Sq. Ft.	7,429
Geocomposite Wall Drain	Sq. Yd.	993
Micro-piles	Each	92
Micropile Proof Load Test	Each	2
Granular Backfill for Structures	Cu. Yd.	1,324
Drainage Scuppers, DS-12	Each	6
Drainage System	L. Sum	1
Pipe Underdrains for Structures 4"	Foot	329
Staining Concrete Structures	Sq. Ft.	3,159

Note: Bill of Material shown for Bridge quantities only. For Retaining Wall Bills of Material, See Sheets NW-2, SW-2, NE-2 and SE-2.



**PROFILE GRADE - US 34**  
Along E.B. and W.B. P.G.'s



**TOP OF RAIL ELEVATIONS**  
CN RR



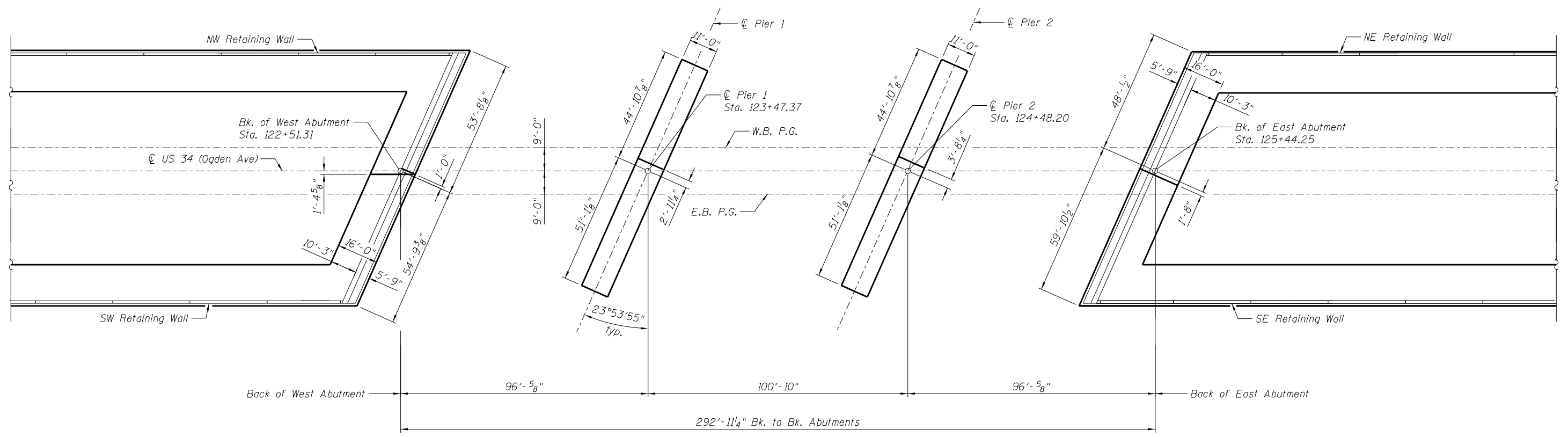
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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES, BILL OF MATERIALS, AND INDEX OF SHEETS**  
**STRUCTURE NO. 022-0512**

SHEET NO. S-2 OF S-53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A		383	165
			CONTRACT NO. 60R06	
ILLINOIS FED. AID PROJECT				



**FOOTING LAYOUT**

**NOTES**

1. See pier footing plans for micropile layout.
2. See abutment footing plans for drilled shaft layout.



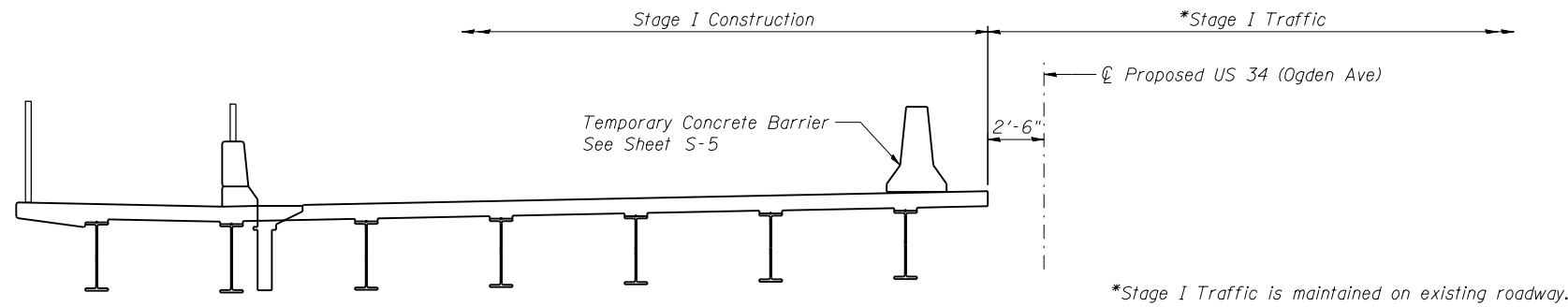
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

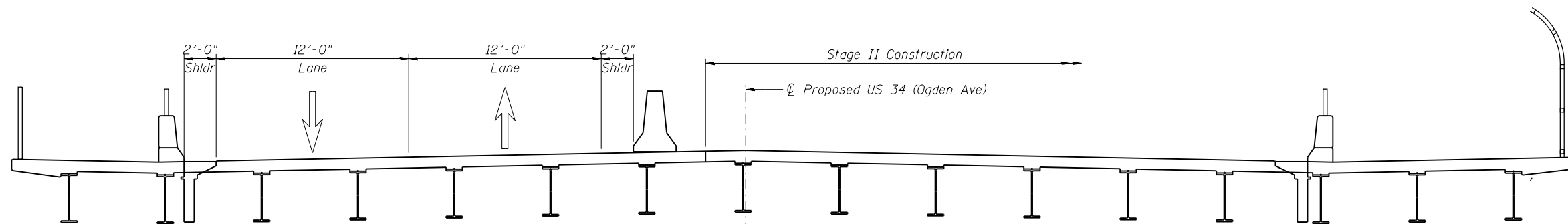
**FOOTING LAYOUT  
STRUCTURE NO. 022-0512**

SHEET NO. S-3 OF S-53 SHEETS

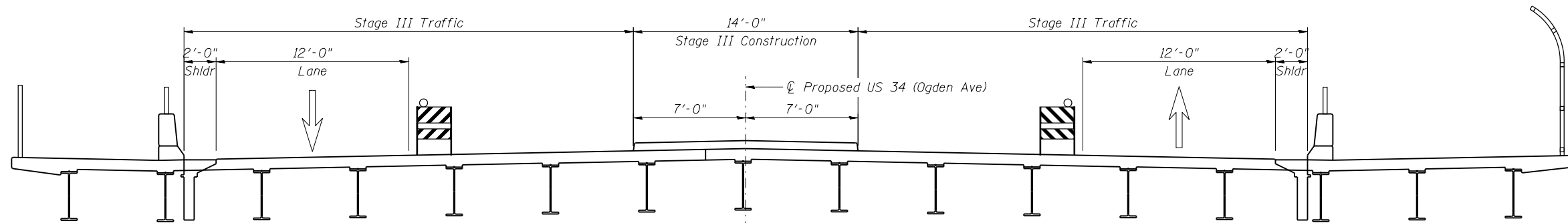
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DuPAGE	383	166
CONTRACT NO. 60R06			ILLINOIS FED. AID PROJECT	



**STAGE I CONSTRUCTION**



**STAGE II CONSTRUCTION**



**STAGE III CONSTRUCTION**

**NOTES**

1. All staging cross sections are looking East.
2. For quantity of Temporary Concrete Barrier, see roadway plans.



USER NAME =	DESIGNED - BAR	REVISED
	CHECKED - STB	REVISED
PLOT SCALE =	DRAWN - BAR	REVISED
PLOT DATE = 02/28/2014	CHECKED - STB	REVISED

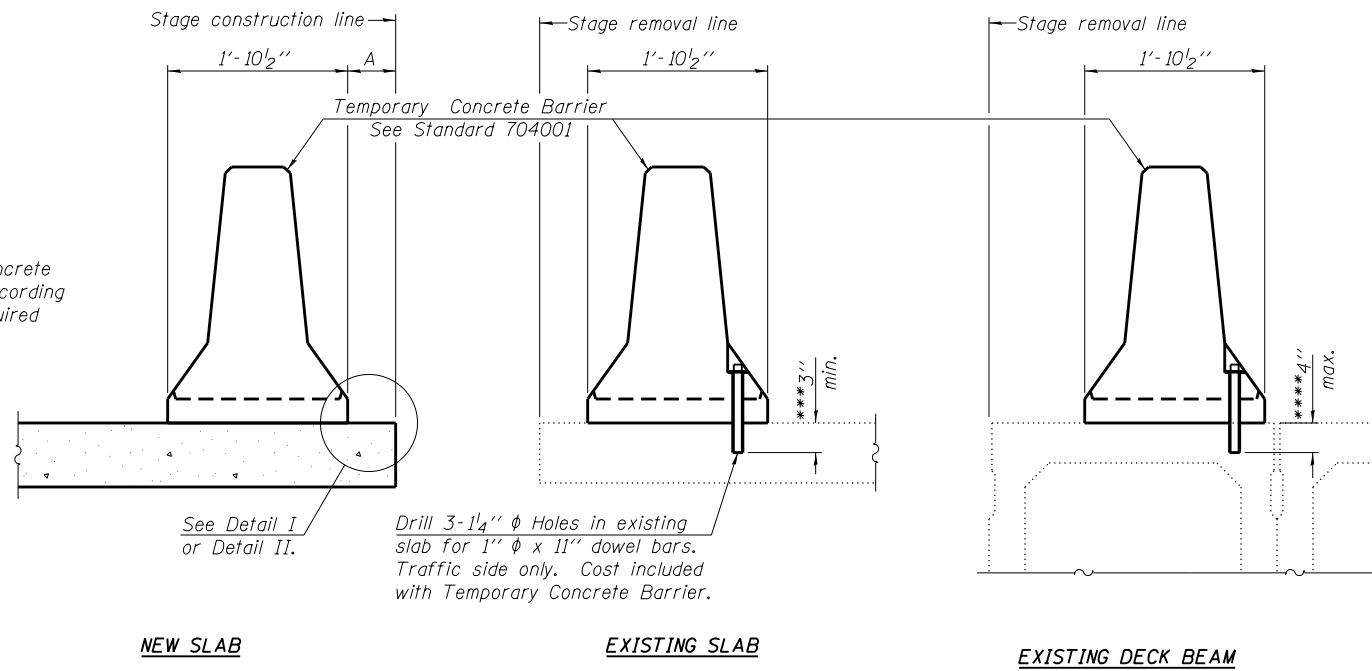
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STAGE CONSTRUCTION DETAILS  
STRUCTURE NO. 022-0512**

SHEET NO. S-4 OF S-53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DuPAGE	383	167
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60R06	

When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".



Drill 3-1/4"  $\phi$  Holes in existing slab for 1"  $\phi$  x 11" dowel bars. Traffic side only. Cost included with Temporary Concrete Barrier.

**NOTES**

Detail I - With Bar Splicer or Couplers:  
Connect one (1) 1" x 7" x "W" steel  $\bar{L}$  to the top layer of couplers with 2-5/8"  $\phi$  bolts screwed to coupler at approximate  $\bar{C}$  of each barrier panel.

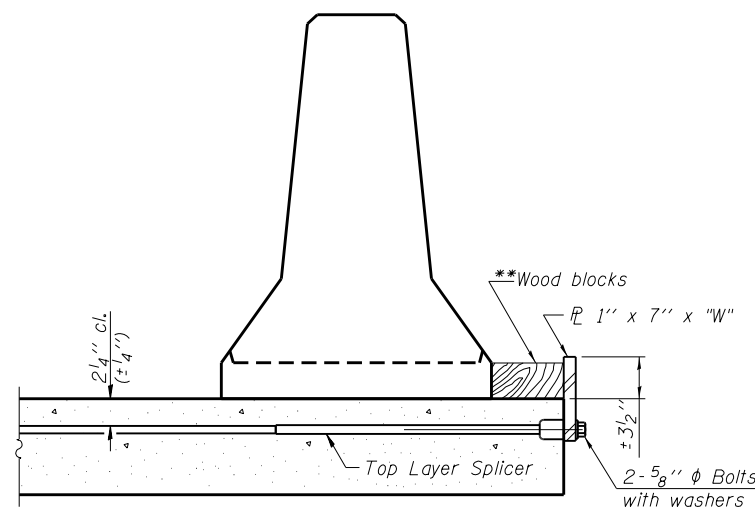
Detail II - With Extended Reinforcement Bars:  
Connect one (1) 1" x 7" x "W" steel  $\bar{L}$  to the concrete slab or concrete wearing surface with 2-5/8"  $\phi$  Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate  $\bar{C}$  of each barrier panel.

Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x "W" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

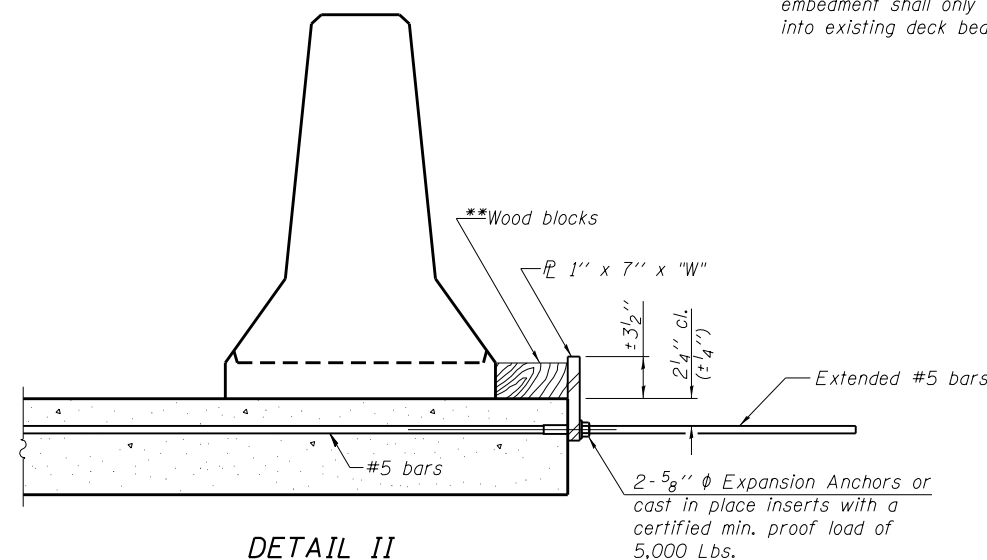
**SECTIONS THRU SLAB OR DECK BEAM**

\*\*\* Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

\*\*\*\* If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



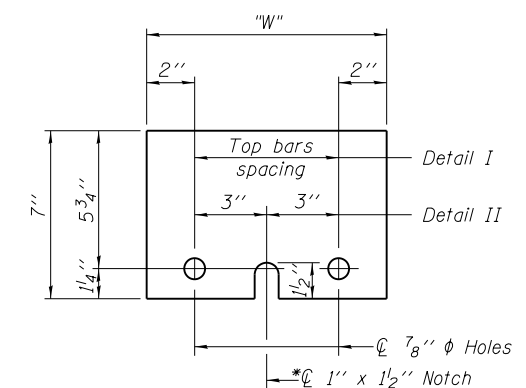
**DETAIL I**



**DETAIL II**

\*\* Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

"W" = Top bars spacing + 4"



**STEEL RETAINER  $\bar{L}$  1" x 7" x "W"**

\* Required only with Detail II

R-27

7-1-10



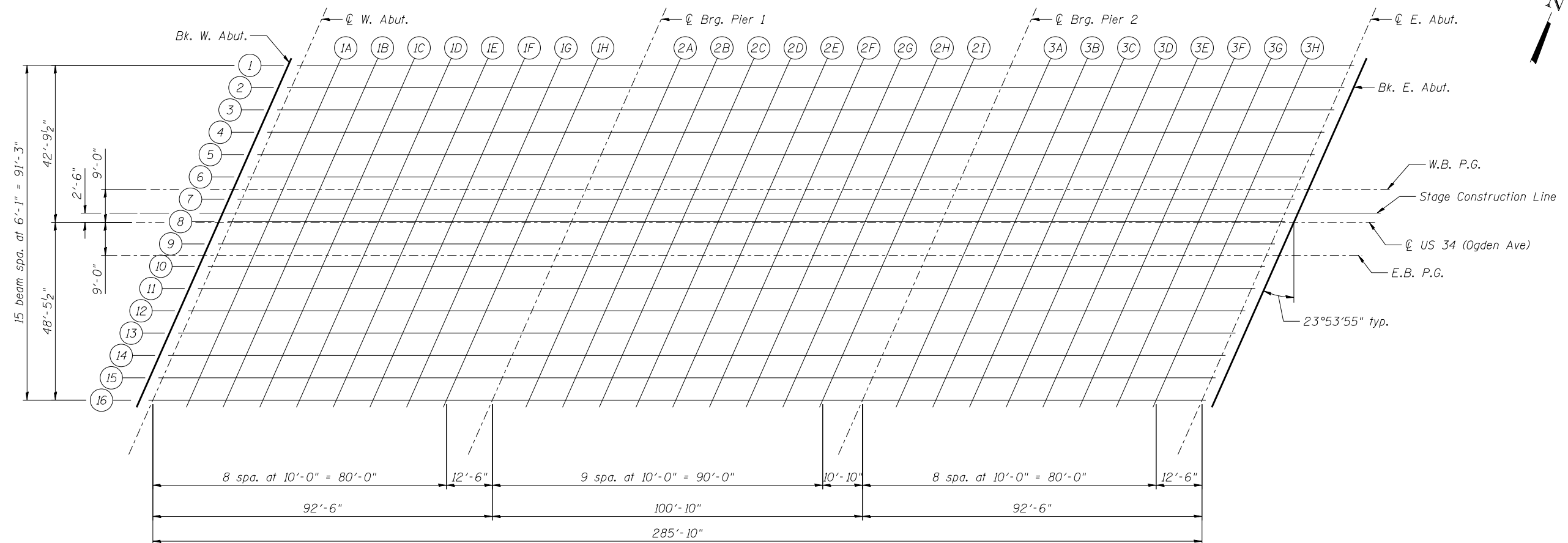
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

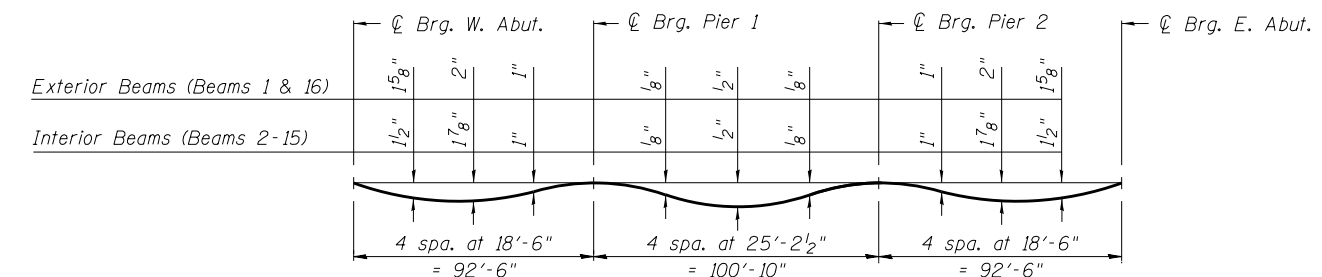
TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION  
STRUCTURE NO. 022-0512

SHEET NO. S-5 OF S-53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A		383	168
			CONTRACT NO. 60R06	
ILLINOIS FED. AID PROJECT				



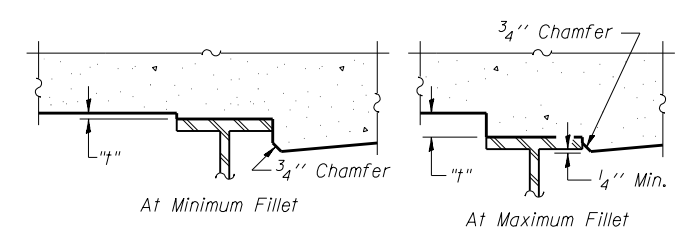
PLAN



DEAD LOAD DEFLECTION DIAGRAM

(Includes weight of concrete only.)

Note:  
The above deflections are not to be used in the field if the engineer is working from the "Theoretical Elevations Adjusted for Dead Load Deflections" shown on sheets S-7 thru S-13 of S-53.



To determine "t": After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown above. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown on sheets S-7 thru S-13 of S-53, minus slab thickness, equals the fillet heights "t" above top flange of beams.

FILLET HEIGHTS



USER NAME =	DESIGNED - BAR	REVISED
	CHECKED - NPP	REVISED
PLOT SCALE =	DRAWN - BAR	REVISED
PLOT DATE = 02/28/2014	CHECKED - KJZ	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS 1  
STRUCTURE NO. 022-0512

SHEET NO. S-6 OF S-53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DuPAGE	383	169
CONTRACT NO. 60R06				
ILLINOIS FED. AID PROJECT				

**BEAM 1**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	122+70.27	42.79' Lt.	736.54	736.54
☉ Brg. W. Abut.	122+73.83	42.79' Lt.	736.62	736.62
1A	122+83.83	42.79' Lt.	736.87	736.94
1B	122+93.83	42.79' Lt.	737.10	737.22
1C	123+03.83	42.79' Lt.	737.32	737.48
1D	123+13.83	42.79' Lt.	737.53	737.70
1E	123+23.83	42.79' Lt.	737.72	737.88
1F	123+33.83	42.79' Lt.	737.90	738.03
1G	123+43.83	42.79' Lt.	738.07	738.16
1H	123+53.83	42.79' Lt.	738.23	738.27
☉ Pier 1	123+66.33	42.79' Lt.	738.42	738.42
2A	123+76.33	42.79' Lt.	738.55	738.54
2B	123+86.33	42.79' Lt.	738.67	738.67
2C	123+96.33	42.79' Lt.	738.78	738.80
2D	124+06.33	42.79' Lt.	738.87	738.91
2E	124+16.33	42.79' Lt.	738.95	738.99
2F	124+26.33	42.79' Lt.	739.03	739.06
2G	124+36.33	42.79' Lt.	739.09	739.11
2H	124+46.33	42.79' Lt.	739.13	739.14
2I	124+56.33	42.79' Lt.	739.17	739.16
☉ Pier 2	124+67.16	42.79' Lt.	739.19	739.19
3A	124+77.16	42.79' Lt.	739.20	739.23
3B	124+87.16	42.79' Lt.	739.20	739.28
3C	124+97.16	42.79' Lt.	739.19	739.31
3D	125+07.16	42.79' Lt.	739.16	739.32
3E	125+17.16	42.79' Lt.	739.13	739.30
3F	125+27.16	42.79' Lt.	739.08	739.24
3G	125+37.16	42.79' Lt.	739.01	739.15
3H	125+47.16	42.79' Lt.	738.94	739.02
☉ E. Abut.	125+59.66	42.79' Lt.	738.83	738.83
Bk. E. Abut.	125+63.21	42.79' Lt.	738.80	738.80

**BEAM 2**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	122+67.58	36.71' Lt.	736.40	736.40
☉ Brg. W. Abut.	122+71.13	36.71' Lt.	736.49	736.49
1A	122+81.13	36.71' Lt.	736.74	736.80
1B	122+91.13	36.71' Lt.	736.97	737.09
1C	123+01.13	36.71' Lt.	737.20	737.34
1D	123+11.13	36.71' Lt.	737.41	737.56
1E	123+21.13	36.71' Lt.	737.61	737.75
1F	123+31.13	36.71' Lt.	737.79	737.91
1G	123+41.13	36.71' Lt.	737.97	738.04
1H	123+51.13	36.71' Lt.	738.13	738.17
☉ Pier 1	123+63.63	36.71' Lt.	738.31	738.31
2A	123+73.63	36.71' Lt.	738.45	738.44
2B	123+83.63	36.71' Lt.	738.57	738.58
2C	123+93.63	36.71' Lt.	738.68	738.70
2D	124+03.63	36.71' Lt.	738.78	738.81
2E	124+13.63	36.71' Lt.	738.87	738.91
2F	124+23.63	36.71' Lt.	738.94	738.98
2G	124+33.63	36.71' Lt.	739.01	739.03
2H	124+43.63	36.71' Lt.	739.06	739.06
2I	124+53.63	36.71' Lt.	739.10	739.09
☉ Pier 2	124+64.46	36.71' Lt.	739.13	739.13
3A	124+74.46	36.71' Lt.	739.14	739.17
3B	124+84.46	36.71' Lt.	739.14	739.21
3C	124+94.46	36.71' Lt.	739.13	739.24
3D	125+04.46	36.71' Lt.	739.11	739.25
3E	125+14.46	36.71' Lt.	739.07	739.23
3F	125+24.46	36.71' Lt.	739.03	739.18
3G	125+34.46	36.71' Lt.	738.97	739.09
3H	125+44.46	36.71' Lt.	738.90	738.97
☉ E. Abut.	125+56.96	36.71' Lt.	738.79	738.79
Bk. E. Abut.	125+60.52	36.71' Lt.	738.76	738.76

**BEAM 3**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	122+64.88	30.63' Lt.	736.46	736.46
☉ Brg. W. Abut.	122+68.44	30.63' Lt.	736.55	736.55
1A	122+78.44	30.63' Lt.	736.80	736.86
1B	122+88.44	30.63' Lt.	737.04	737.15
1C	122+98.44	30.63' Lt.	737.26	737.41
1D	123+08.44	30.63' Lt.	737.47	737.63
1E	123+18.44	30.63' Lt.	737.68	737.82
1F	123+28.44	30.63' Lt.	737.87	737.98
1G	123+38.44	30.63' Lt.	738.04	738.12
1H	123+48.44	30.63' Lt.	738.21	738.25
☉ Pier 1	123+60.94	30.63' Lt.	738.40	738.40
2A	123+70.94	30.63' Lt.	738.54	738.53
2B	123+80.94	30.63' Lt.	738.66	738.67
2C	123+90.94	30.63' Lt.	738.78	738.80
2D	124+00.94	30.63' Lt.	738.88	738.91
2E	124+10.94	30.63' Lt.	738.97	739.01
2F	124+20.94	30.63' Lt.	739.05	739.08
2G	124+30.94	30.63' Lt.	739.11	739.13
2H	124+40.94	30.63' Lt.	739.17	739.17
2I	124+50.94	30.63' Lt.	739.21	739.21
☉ Pier 2	124+61.77	30.63' Lt.	739.24	739.24
3A	124+71.77	30.63' Lt.	739.26	739.29
3B	124+81.77	30.63' Lt.	739.26	739.33
3C	124+91.77	30.63' Lt.	739.26	739.36
3D	125+01.77	30.63' Lt.	739.24	739.38
3E	125+11.77	30.63' Lt.	739.21	739.36
3F	125+21.77	30.63' Lt.	739.16	739.31
3G	125+31.77	30.63' Lt.	739.11	739.23
3H	125+41.77	30.63' Lt.	739.04	739.12
☉ E. Abut.	125+54.27	30.63' Lt.	738.94	738.94
Bk. E. Abut.	125+57.82	30.63' Lt.	738.91	738.91

Note:  
Work this with Sheet S-6.



USER NAME =	DESIGNED - BAR	REVISED
	CHECKED - NPP	REVISED
PLOT SCALE =	DRAWN - BAR	REVISED
PLOT DATE = 02/28/2014	CHECKED - KJZ	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS 2  
STRUCTURE NO. 022-0512

SHEET NO. S-7 OF S-53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DuPAGE	383	170
CONTRACT NO. 60R06			ILLINOIS FED. AID PROJECT	

**BEAM 4**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	122+62.18	24.54' Lt.	736.51	736.51
☉ Brg. W. Abut.	122+65.74	24.54' Lt.	736.61	736.61
1A	122+75.74	24.54' Lt.	736.86	736.92
1B	122+85.74	24.54' Lt.	737.10	737.21
1C	122+95.74	24.54' Lt.	737.33	737.47
1D	123+05.74	24.54' Lt.	737.55	737.70
1E	123+15.74	24.54' Lt.	737.75	737.90
1F	123+25.74	24.54' Lt.	737.94	738.06
1G	123+35.74	24.54' Lt.	738.12	738.20
1H	123+45.74	24.54' Lt.	738.29	738.33
☉ Pier 1	123+58.24	24.54' Lt.	738.49	738.49
2A	123+68.24	24.54' Lt.	738.63	738.62
2B	123+78.24	24.54' Lt.	738.76	738.76
2C	123+88.24	24.54' Lt.	738.87	738.89
2D	123+98.24	24.54' Lt.	738.98	739.01
2E	124+08.24	24.54' Lt.	739.07	739.11
2F	124+18.24	24.54' Lt.	739.16	739.19
2G	124+28.24	24.54' Lt.	739.22	739.24
2H	124+38.24	24.54' Lt.	739.28	739.29
2I	124+48.24	24.54' Lt.	739.33	739.32
☉ Pier 2	124+59.07	24.54' Lt.	739.36	739.36
3A	124+69.07	24.54' Lt.	739.38	739.41
3B	124+79.07	24.54' Lt.	739.39	739.46
3C	124+89.07	24.54' Lt.	739.39	739.49
3D	124+99.07	24.54' Lt.	739.37	739.51
3E	125+09.07	24.54' Lt.	739.34	739.50
3F	125+19.07	24.54' Lt.	739.30	739.45
3G	125+29.07	24.54' Lt.	739.25	739.37
3H	125+39.07	24.54' Lt.	739.19	739.26
☉ E. Abut.	125+51.57	24.54' Lt.	739.09	739.09
Bk. E. Abut.	125+55.13	24.54' Lt.	739.06	739.06

**BEAM 5**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	122+59.49	18.46' Lt.	736.57	736.57
☉ Brg. W. Abut.	122+63.04	18.46' Lt.	736.66	736.66
1A	122+73.04	18.46' Lt.	736.92	736.98
1B	122+83.04	18.46' Lt.	737.16	737.28
1C	122+93.04	18.46' Lt.	737.39	737.54
1D	123+03.04	18.46' Lt.	737.61	737.77
1E	123+13.04	18.46' Lt.	737.82	737.97
1F	123+23.04	18.46' Lt.	738.02	738.13
1G	123+33.04	18.46' Lt.	738.20	738.28
1H	123+43.04	18.46' Lt.	738.37	738.41
☉ Pier 1	123+55.54	18.46' Lt.	738.57	738.57
2A	123+65.54	18.46' Lt.	738.72	738.71
2B	123+75.54	18.46' Lt.	738.85	738.85
2C	123+85.54	18.46' Lt.	738.97	738.99
2D	123+95.54	18.46' Lt.	739.08	739.11
2E	124+05.54	18.46' Lt.	739.18	739.21
2F	124+15.54	18.46' Lt.	739.26	739.29
2G	124+25.54	18.46' Lt.	739.33	739.35
2H	124+35.54	18.46' Lt.	739.39	739.40
2I	124+45.54	18.46' Lt.	739.44	739.44
☉ Pier 2	124+56.38	18.46' Lt.	739.48	739.48
3A	124+66.38	18.46' Lt.	739.51	739.53
3B	124+76.38	18.46' Lt.	739.52	739.58
3C	124+86.38	18.46' Lt.	739.52	739.62
3D	124+96.38	18.46' Lt.	739.50	739.64
3E	125+06.38	18.46' Lt.	739.48	739.63
3F	125+16.38	18.46' Lt.	739.44	739.59
3G	125+26.38	18.46' Lt.	739.39	739.52
3H	125+36.38	18.46' Lt.	739.33	739.41
☉ E. Abut.	125+48.88	18.46' Lt.	739.24	739.24
Bk. E. Abut.	125+52.43	18.46' Lt.	739.21	739.21

**BEAM 6**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	122+56.79	12.38' Lt.	736.62	736.62
☉ Brg. W. Abut.	122+60.35	12.38' Lt.	736.72	736.72
1A	122+70.35	12.38' Lt.	736.98	737.04
1B	122+80.35	12.38' Lt.	737.22	737.34
1C	122+90.35	12.38' Lt.	737.46	737.61
1D	123+00.35	12.38' Lt.	737.68	737.84
1E	123+10.35	12.38' Lt.	737.89	738.04
1F	123+20.35	12.38' Lt.	738.09	738.21
1G	123+30.35	12.38' Lt.	738.28	738.36
1H	123+40.35	12.38' Lt.	738.46	738.49
☉ Pier 1	123+52.85	12.38' Lt.	738.66	738.66
2A	123+62.85	12.38' Lt.	738.81	738.80
2B	123+72.85	12.38' Lt.	738.94	738.95
2C	123+82.85	12.38' Lt.	739.07	739.09
2D	123+92.85	12.38' Lt.	739.18	739.21
2E	124+02.85	12.38' Lt.	739.28	739.32
2F	124+12.85	12.38' Lt.	739.37	739.40
2G	124+22.85	12.38' Lt.	739.44	739.46
2H	124+32.85	12.38' Lt.	739.51	739.51
2I	124+42.85	12.38' Lt.	739.56	739.55
☉ Pier 2	124+53.68	12.38' Lt.	739.60	739.60
3A	124+63.68	12.38' Lt.	739.63	739.65
3B	124+73.68	12.38' Lt.	739.64	739.71
3C	124+83.68	12.38' Lt.	739.64	739.75
3D	124+93.68	12.38' Lt.	739.63	739.77
3E	125+03.68	12.38' Lt.	739.61	739.77
3F	125+13.68	12.38' Lt.	739.58	739.73
3G	125+23.68	12.38' Lt.	739.53	739.66
3H	125+33.68	12.38' Lt.	739.48	739.55
☉ E. Abut.	125+46.18	12.38' Lt.	739.39	739.39
Bk. E. Abut.	125+49.74	12.38' Lt.	739.36	739.36

Note:  
Work this with Sheet S-6.



USER NAME =	DESIGNED - BAR	REVISED
	CHECKED - NPP	REVISED
PLOT SCALE =	DRAWN - BAR	REVISED
PLOT DATE = 02/28/2014	CHECKED - KJZ	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS 3  
STRUCTURE NO. 022-0512

SHEET NO. S-8 OF S-53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DuPAGE	383	171
CONTRACT NO. 60R06			ILLINOIS FED. AID PROJECT	

**W.B. P.G.**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	122+55.30	9' Lt.	736.65	736.65
☉ Brg. W. Abut.	122+58.85	9' Lt.	736.75	736.75
1A	122+68.85	9' Lt.	737.01	737.07
1B	122+78.85	9' Lt.	737.26	737.37
1C	122+88.85	9' Lt.	737.50	737.64
1D	122+98.85	9' Lt.	737.72	737.88
1E	123+08.85	9' Lt.	737.93	738.08
1F	123+18.85	9' Lt.	738.14	738.25
1G	123+28.85	9' Lt.	738.32	738.40
1H	123+38.85	9' Lt.	738.50	738.54
☉ Pier 1	123+51.35	9' Lt.	738.71	738.71
2A	123+61.35	9' Lt.	738.86	738.85
2B	123+71.35	9' Lt.	738.99	739.00
2C	123+81.35	9' Lt.	739.12	739.14
2D	123+91.35	9' Lt.	739.23	739.26
2E	124+01.35	9' Lt.	739.33	739.37
2F	124+11.35	9' Lt.	739.42	739.46
2G	124+21.35	9' Lt.	739.50	739.52
2H	124+31.35	9' Lt.	739.57	739.57
2I	124+41.35	9' Lt.	739.62	739.62
☉ Pier 2	124+52.19	9' Lt.	739.67	739.67
3A	124+62.19	9' Lt.	739.69	739.72
3B	124+72.19	9' Lt.	739.71	739.78
3C	124+82.19	9' Lt.	739.71	739.82
3D	124+92.19	9' Lt.	739.71	739.85
3E	125+02.19	9' Lt.	739.69	739.84
3F	125+12.19	9' Lt.	739.66	739.81
3G	125+22.19	9' Lt.	739.61	739.73
3H	125+32.19	9' Lt.	739.56	739.63
☉ E. Abut.	125+44.69	9' Lt.	739.47	739.47
Bk. E. Abut.	125+48.24		739.44	739.44

**BEAM 7**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	122+54.10	6.29' Lt.	736.68	736.68
☉ Brg. W. Abut.	122+57.65	6.29' Lt.	736.77	736.77
1A	122+67.65	6.29' Lt.	737.03	737.10
1B	122+77.65	6.29' Lt.	737.29	737.40
1C	122+87.65	6.29' Lt.	737.52	737.67
1D	122+97.65	6.29' Lt.	737.75	737.91
1E	123+07.65	6.29' Lt.	737.97	738.11
1F	123+17.65	6.29' Lt.	738.17	738.28
1G	123+27.65	6.29' Lt.	738.36	738.44
1H	123+37.65	6.29' Lt.	738.54	738.57
☉ Pier 1	123+50.15	6.29' Lt.	738.74	738.74
2A	123+60.15	6.29' Lt.	738.89	738.89
2B	123+70.15	6.29' Lt.	739.03	739.04
2C	123+80.15	6.29' Lt.	739.16	739.18
2D	123+90.15	6.29' Lt.	739.28	739.31
2E	124+00.15	6.29' Lt.	739.38	739.42
2F	124+10.15	6.29' Lt.	739.47	739.50
2G	124+20.15	6.29' Lt.	739.55	739.57
2H	124+30.15	6.29' Lt.	739.62	739.62
2I	124+40.15	6.29' Lt.	739.67	739.67
☉ Pier 2	124+50.99	6.29' Lt.	739.72	739.72
3A	124+60.99	6.29' Lt.	739.75	739.77
3B	124+70.99	6.29' Lt.	739.77	739.83
3C	124+80.99	6.29' Lt.	739.77	739.88
3D	124+90.99	6.29' Lt.	739.77	739.90
3E	125+00.99	6.29' Lt.	739.75	739.90
3F	125+10.99	6.29' Lt.	739.72	739.87
3G	125+20.99	6.29' Lt.	739.67	739.80
3H	125+30.99	6.29' Lt.	739.62	739.70
☉ E. Abut.	125+43.49	6.29' Lt.	739.54	739.54
Bk. E. Abut.	125+47.04	6.29' Lt.	739.51	739.51

**STAGE CONSTRUCTION LINE**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	122+52.42	2.5' Lt.	736.71	736.71
☉ Brg. W. Abut.	122+55.97	2.5' Lt.	736.81	736.81
1A	122+65.97	2.5' Lt.	737.07	737.13
1B	122+75.97	2.5' Lt.	737.32	737.44
1C	122+85.97	2.5' Lt.	737.56	737.71
1D	122+95.97	2.5' Lt.	737.79	737.95
1E	123+05.97	2.5' Lt.	738.01	738.15
1F	123+15.97	2.5' Lt.	738.21	738.33
1G	123+25.97	2.5' Lt.	738.41	738.48
1H	123+35.97	2.5' Lt.	738.59	738.62
☉ Pier 1	123+48.47	2.5' Lt.	738.80	738.80
2A	123+58.47	2.5' Lt.	738.95	738.94
2B	123+68.47	2.5' Lt.	739.09	739.09
2C	123+78.47	2.5' Lt.	739.22	739.24
2D	123+88.47	2.5' Lt.	739.34	739.37
2E	123+98.47	2.5' Lt.	739.44	739.48
2F	124+08.47	2.5' Lt.	739.54	739.57
2G	124+18.47	2.5' Lt.	739.62	739.64
2H	124+28.47	2.5' Lt.	739.69	739.69
2I	124+38.47	2.5' Lt.	739.74	739.74
☉ Pier 2	124+49.31	2.5' Lt.	739.79	739.79
3A	124+59.31	2.5' Lt.	739.82	739.85
3B	124+69.31	2.5' Lt.	739.84	739.91
3C	124+79.31	2.5' Lt.	739.85	739.96
3D	124+89.31	2.5' Lt.	739.85	739.99
3E	124+99.31	2.5' Lt.	739.83	739.99
3F	125+09.31	2.5' Lt.	739.80	739.95
3G	125+19.31	2.5' Lt.	739.76	739.88
3H	125+29.31	2.5' Lt.	739.71	739.78
☉ E. Abut.	125+41.81	2.5' Lt.	739.63	739.63
Bk. E. Abut.	125+45.36	2.5' Lt.	739.60	739.60

Note:  
Work this with Sheet S-6.



USER NAME =	DESIGNED - BAR	REVISED
	CHECKED - NPP	REVISED
PLOT SCALE =	DRAWN - BAR	REVISED
PLOT DATE = 02/28/2014	CHECKED - KJZ	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS 4  
STRUCTURE NO. 022-0512

SHEET NO. S-9 OF S-53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DuPAGE	383	172
CONTRACT NO. 60R06			ILLINOIS FED. AID PROJECT	



**BEAM 8**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	122+51.40	0.21' Lt.	736.73	736.73
☉ Brg. W. Abut.	122+54.96	0.21' Lt.	736.83	736.83
1A	122+64.96	0.21' Lt.	737.09	737.15
1B	122+74.96	0.21' Lt.	737.35	737.46
1C	122+84.96	0.21' Lt.	737.59	737.73
1D	122+94.96	0.21' Lt.	737.82	737.97
1E	123+04.96	0.21' Lt.	738.04	738.18
1F	123+14.96	0.21' Lt.	738.24	738.36
1G	123+24.96	0.21' Lt.	738.44	738.51
1H	123+34.96	0.21' Lt.	738.62	738.65
☉ Pier 1	123+47.46	0.21' Lt.	738.83	738.83
2A	123+57.46	0.21' Lt.	738.98	738.98
2B	123+67.46	0.21' Lt.	739.12	739.13
2C	123+77.46	0.21' Lt.	739.25	739.27
2D	123+87.46	0.21' Lt.	739.37	739.40
2E	123+97.46	0.21' Lt.	739.48	739.52
2F	124+07.46	0.21' Lt.	739.57	739.61
2G	124+17.46	0.21' Lt.	739.66	739.68
2H	124+27.46	0.21' Lt.	739.73	739.73
2I	124+37.46	0.21' Lt.	739.78	739.78
☉ Pier 2	124+48.29	0.21' Lt.	739.83	739.83
3A	124+58.29	0.21' Lt.	739.87	739.89
3B	124+68.29	0.21' Lt.	739.89	739.96
3C	124+78.29	0.21' Lt.	739.90	740.00
3D	124+88.29	0.21' Lt.	739.89	740.03
3E	124+98.29	0.21' Lt.	739.88	740.04
3F	125+08.29	0.21' Lt.	739.85	740.00
3G	125+18.29	0.21' Lt.	739.81	739.94
3H	125+28.29	0.21' Lt.	739.76	739.84
☉ E. Abut.	125+40.79	0.21' Lt.	739.68	739.68
Bk. E. Abut.	125+44.35	0.21' Lt.	739.66	739.66

**☉ US 34 (OGDEN AVE)**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	122+51.31	0' Lt.	736.73	736.73
☉ Brg. W. Abut.	122+54.87	0' Lt.	736.83	736.83
1A	122+64.87	0' Lt.	737.09	737.16
1B	122+74.87	0' Lt.	737.35	737.46
1C	122+84.87	0' Lt.	737.59	737.74
1D	122+94.87	0' Lt.	737.82	737.98
1E	123+04.87	0' Lt.	738.04	738.18
1F	123+14.87	0' Lt.	738.24	738.36
1G	123+24.87	0' Lt.	738.44	738.51
1H	123+34.87	0' Lt.	738.62	738.66
☉ Pier 1	123+47.37	0' Lt.	738.83	738.83
2A	123+57.37	0' Lt.	738.98	738.98
2B	123+67.37	0' Lt.	739.13	739.13
2C	123+77.37	0' Lt.	739.26	739.28
2D	123+87.37	0' Lt.	739.38	739.41
2E	123+97.37	0' Lt.	739.48	739.52
2F	124+07.37	0' Lt.	739.58	739.61
2G	124+17.37	0' Lt.	739.66	739.68
2H	124+27.37	0' Lt.	739.73	739.74
2I	124+37.37	0' Lt.	739.79	739.78
☉ Pier 2	124+48.20	0' Lt.	739.84	739.84
3A	124+58.20	0' Lt.	739.87	739.90
3B	124+68.20	0' Lt.	739.89	739.96
3C	124+78.20	0' Lt.	739.90	740.01
3D	124+88.20	0' Lt.	739.90	740.04
3E	124+98.20	0' Lt.	739.88	740.04
3F	125+08.20	0' Lt.	739.86	740.01
3G	125+18.20	0' Lt.	739.82	739.94
3H	125+28.20	0' Lt.	739.77	739.84
☉ E. Abut.	125+40.70	0' Lt.	739.69	739.69
Bk. E. Abut.	125+44.25	0' Lt.	739.66	739.66

**BEAM 9**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	122+48.71	5.88' Rt.	736.54	736.54
☉ Brg. W. Abut.	122+52.26	5.88' Rt.	736.63	736.63
1A	122+62.26	5.88' Rt.	736.90	736.97
1B	122+72.26	5.88' Rt.	737.16	737.27
1C	122+82.26	5.88' Rt.	737.41	737.55
1D	122+92.26	5.88' Rt.	737.64	737.80
1E	123+02.26	5.88' Rt.	737.86	738.01
1F	123+12.26	5.88' Rt.	738.07	738.19
1G	123+22.26	5.88' Rt.	738.27	738.34
1H	123+32.26	5.88' Rt.	738.45	738.49
☉ Pier 1	123+44.76	5.88' Rt.	738.67	738.67
2A	123+54.76	5.88' Rt.	738.82	738.82
2B	123+64.76	5.88' Rt.	738.97	738.97
2C	123+74.76	5.88' Rt.	739.10	739.12
2D	123+84.76	5.88' Rt.	739.22	739.26
2E	123+94.76	5.88' Rt.	739.33	739.37
2F	124+04.76	5.88' Rt.	739.43	739.46
2G	124+14.76	5.88' Rt.	739.52	739.54
2H	124+24.76	5.88' Rt.	739.59	739.60
2I	124+34.76	5.88' Rt.	739.65	739.65
☉ Pier 2	124+45.60	5.88' Rt.	739.71	739.71
3A	124+55.60	5.88' Rt.	739.74	739.77
3B	124+65.60	5.88' Rt.	739.77	739.83
3C	124+75.60	5.88' Rt.	739.78	739.89
3D	124+85.60	5.88' Rt.	739.78	739.92
3E	124+95.60	5.88' Rt.	739.77	739.92
3F	125+05.60	5.88' Rt.	739.74	739.89
3G	125+15.60	5.88' Rt.	739.71	739.83
3H	125+25.60	5.88' Rt.	739.66	739.73
☉ E. Abut.	125+38.10	5.88' Rt.	739.58	739.58
Bk. E. Abut.	125+41.65	5.88' Rt.	739.56	739.56

Note:  
Work this with Sheet S-6.



USER NAME =	DESIGNED - BAR	REVISED
	CHECKED - NPP	REVISED
PLOT SCALE =	DRAWN - BAR	REVISED
PLOT DATE = 02/28/2014	CHECKED - KJZ	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS 5  
STRUCTURE NO. 022-0512

SHEET NO. S-10 OF S-53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A		383	173
CONTRACT NO. 60R06			ILLINOIS FED. AID PROJECT	

**E.B. P.G.**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	122+47.32	9' Rt.	736.43	736.43
☉ Brg. W. Abut.	122+50.88	9' Rt.	736.53	736.53
1A	122+60.88	9' Rt.	736.80	736.86
1B	122+70.88	9' Rt.	737.06	737.17
1C	122+80.88	9' Rt.	737.31	737.45
1D	122+90.88	9' Rt.	737.54	737.70
1E	123+00.88	9' Rt.	737.76	737.91
1F	123+10.88	9' Rt.	737.98	738.09
1G	123+20.88	9' Rt.	738.17	738.25
1H	123+30.88	9' Rt.	738.36	738.40
☉ Pier 1	123+43.38	9' Rt.	738.58	738.58
2A	123+53.38	9' Rt.	738.74	738.73
2B	123+63.38	9' Rt.	738.88	738.89
2C	123+73.38	9' Rt.	739.02	739.04
2D	123+83.38	9' Rt.	739.14	739.17
2E	123+93.38	9' Rt.	739.25	739.29
2F	124+03.38	9' Rt.	739.35	739.39
2G	124+13.38	9' Rt.	739.44	739.46
2H	124+23.38	9' Rt.	739.52	739.52
2I	124+33.38	9' Rt.	739.58	739.58
☉ Pier 2	124+44.21	9' Rt.	739.63	739.63
3A	124+54.21	9' Rt.	739.67	739.70
3B	124+64.21	9' Rt.	739.70	739.76
3C	124+74.21	9' Rt.	739.71	739.82
3D	124+84.21	9' Rt.	739.71	739.85
3E	124+94.21	9' Rt.	739.70	739.86
3F	125+04.21	9' Rt.	739.68	739.83
3G	125+14.21	9' Rt.	739.65	739.77
3H	125+24.21	9' Rt.	739.60	739.68
☉ E. Abut.	125+36.71	9' Rt.	739.53	739.53
Bk. E. Abut.	125+40.27	9' Rt.	739.50	739.50

**BEAM 10**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	122+46.01	11.96' Rt.	736.33	736.33
☉ Brg. W. Abut.	122+49.57	11.96' Rt.	736.43	736.43
1A	122+59.57	11.96' Rt.	736.71	736.77
1B	122+69.57	11.96' Rt.	736.97	737.08
1C	122+79.57	11.96' Rt.	737.21	737.36
1D	122+89.57	11.96' Rt.	737.45	737.61
1E	122+99.57	11.96' Rt.	737.67	737.82
1F	123+09.57	11.96' Rt.	737.89	738.00
1G	123+19.57	11.96' Rt.	738.09	738.16
1H	123+29.57	11.96' Rt.	738.28	738.31
☉ Pier 1	123+42.07	11.96' Rt.	738.49	738.49
2A	123+52.07	11.96' Rt.	738.65	738.65
2B	123+62.07	11.96' Rt.	738.80	738.81
2C	123+72.07	11.96' Rt.	738.94	738.96
2D	123+82.07	11.96' Rt.	739.07	739.10
2E	123+92.07	11.96' Rt.	739.18	739.22
2F	124+02.07	11.96' Rt.	739.28	739.31
2G	124+12.07	11.96' Rt.	739.37	739.39
2H	124+22.07	11.96' Rt.	739.45	739.45
2I	124+32.07	11.96' Rt.	739.51	739.51
☉ Pier 2	124+42.90	11.96' Rt.	739.57	739.57
3A	124+52.90	11.96' Rt.	739.61	739.63
3B	124+62.90	11.96' Rt.	739.63	739.70
3C	124+72.90	11.96' Rt.	739.65	739.76
3D	124+82.90	11.96' Rt.	739.65	739.79
3E	124+92.90	11.96' Rt.	739.64	739.80
3F	125+02.90	11.96' Rt.	739.62	739.77
3G	125+12.90	11.96' Rt.	739.59	739.71
3H	125+22.90	11.96' Rt.	739.55	739.62
☉ E. Abut.	125+35.40	11.96' Rt.	739.47	739.47
Bk. E. Abut.	125+38.95	11.96' Rt.	739.45	739.45

**BEAM 11**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	122+43.32	18.04' Rt.	736.13	736.13
☉ Brg. W. Abut.	122+46.87	18.04' Rt.	736.23	736.23
1A	122+56.87	18.04' Rt.	736.51	736.57
1B	122+66.87	18.04' Rt.	736.77	736.88
1C	122+76.87	18.04' Rt.	737.02	737.17
1D	122+86.87	18.04' Rt.	737.26	737.42
1E	122+96.87	18.04' Rt.	737.49	737.63
1F	123+06.87	18.04' Rt.	737.70	737.82
1G	123+16.87	18.04' Rt.	737.91	737.98
1H	123+26.87	18.04' Rt.	738.10	738.14
☉ Pier 1	123+39.37	18.04' Rt.	738.32	738.32
2A	123+49.37	18.04' Rt.	738.49	738.48
2B	123+59.37	18.04' Rt.	738.64	738.64
2C	123+69.37	18.04' Rt.	738.78	738.80
2D	123+79.37	18.04' Rt.	738.91	738.94
2E	123+89.37	18.04' Rt.	739.02	739.06
2F	123+99.37	18.04' Rt.	739.13	739.16
2G	124+09.37	18.04' Rt.	739.22	739.24
2H	124+19.37	18.04' Rt.	739.30	739.30
2I	124+29.37	18.04' Rt.	739.37	739.36
☉ Pier 2	124+40.20	18.04' Rt.	739.43	739.43
3A	124+50.20	18.04' Rt.	739.47	739.50
3B	124+60.20	18.04' Rt.	739.50	739.57
3C	124+70.20	18.04' Rt.	739.52	739.63
3D	124+80.20	18.04' Rt.	739.53	739.67
3E	124+90.20	18.04' Rt.	739.52	739.68
3F	125+00.20	18.04' Rt.	739.50	739.65
3G	125+10.20	18.04' Rt.	739.47	739.60
3H	125+20.20	18.04' Rt.	739.43	739.51
☉ E. Abut.	125+32.70	18.04' Rt.	739.36	739.36
Bk. E. Abut.	125+36.26	18.04' Rt.	739.34	739.34

Note:  
Work this with Sheet S-6.



USER NAME =	DESIGNED - BAR	REVISED
	CHECKED - NPP	REVISED
PLOT SCALE =	DRAWN - BAR	REVISED
PLOT DATE = 02/28/2014	CHECKED - KJZ	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS 6  
STRUCTURE NO. 022-0512

SHEET NO. S-11 OF S-53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DuPAGE	383	174
CONTRACT NO. 60R06			ILLINOIS FED. AID PROJECT	

**BEAM 12**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	122+40.62	24.13' Rt.	735.93	735.93
☉ Brg. W. Abut.	122+44.17	24.13' Rt.	736.03	736.03
1A	122+54.17	24.13' Rt.	736.31	736.37
1B	122+64.17	24.13' Rt.	736.57	736.69
1C	122+74.17	24.13' Rt.	736.83	736.97
1D	122+84.17	24.13' Rt.	737.07	737.23
1E	122+94.17	24.13' Rt.	737.30	737.45
1F	123+04.17	24.13' Rt.	737.52	737.64
1G	123+14.17	24.13' Rt.	737.73	737.80
1H	123+24.17	24.13' Rt.	737.92	737.96
☉ Pier 1	123+36.67	24.13' Rt.	738.15	738.15
2A	123+46.67	24.13' Rt.	738.32	738.31
2B	123+56.67	24.13' Rt.	738.47	738.48
2C	123+66.67	24.13' Rt.	738.62	738.63
2D	123+76.67	24.13' Rt.	738.75	738.78
2E	123+86.67	24.13' Rt.	738.87	738.90
2F	123+96.67	24.13' Rt.	738.97	739.01
2G	124+06.67	24.13' Rt.	739.07	739.09
2H	124+16.67	24.13' Rt.	739.15	739.16
2I	124+26.67	24.13' Rt.	739.22	739.22
☉ Pier 2	124+37.51	24.13' Rt.	739.29	739.29
3A	124+47.51	24.13' Rt.	739.33	739.36
3B	124+57.51	24.13' Rt.	739.37	739.43
3C	124+67.51	24.13' Rt.	739.39	739.50
3D	124+77.51	24.13' Rt.	739.40	739.54
3E	124+87.51	24.13' Rt.	739.40	739.55
3F	124+97.51	24.13' Rt.	739.38	739.53
3G	125+07.51	24.13' Rt.	739.36	739.48
3H	125+17.51	24.13' Rt.	739.32	739.39
☉ E. Abut.	125+30.01	24.13' Rt.	739.25	739.25
Bk. E. Abut.	125+33.56	24.13' Rt.	739.23	739.23

**BEAM 13**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	122+37.92	30.21' Rt.	735.72	735.72
☉ Brg. W. Abut.	122+41.48	30.21' Rt.	735.82	735.82
1A	122+51.48	30.21' Rt.	736.11	736.17
1B	122+61.48	30.21' Rt.	736.38	736.49
1C	122+71.48	30.21' Rt.	736.63	736.78
1D	122+81.48	30.21' Rt.	736.88	737.04
1E	122+91.48	30.21' Rt.	737.11	737.26
1F	123+01.48	30.21' Rt.	737.34	737.45
1G	123+11.48	30.21' Rt.	737.55	737.62
1H	123+21.48	30.21' Rt.	737.74	737.78
☉ Pier 1	123+33.98	30.21' Rt.	737.97	737.97
2A	123+43.98	30.21' Rt.	738.15	738.14
2B	123+53.98	30.21' Rt.	738.30	738.31
2C	123+63.98	30.21' Rt.	738.45	738.47
2D	123+73.98	30.21' Rt.	738.59	738.62
2E	123+83.98	30.21' Rt.	738.71	738.75
2F	123+93.98	30.21' Rt.	738.82	738.85
2G	124+03.98	30.21' Rt.	738.92	738.94
2H	124+13.98	30.21' Rt.	739.00	739.01
2I	124+23.98	30.21' Rt.	739.08	739.07
☉ Pier 2	124+34.81	30.21' Rt.	739.15	739.15
3A	124+44.81	30.21' Rt.	739.20	739.22
3B	124+54.81	30.21' Rt.	739.23	739.30
3C	124+64.81	30.21' Rt.	739.26	739.36
3D	124+74.81	30.21' Rt.	739.27	739.41
3E	124+84.81	30.21' Rt.	739.27	739.43
3F	124+94.81	30.21' Rt.	739.26	739.41
3G	125+04.81	30.21' Rt.	739.24	739.36
3H	125+14.81	30.21' Rt.	739.20	739.28
☉ E. Abut.	125+27.31	30.21' Rt.	739.14	739.14
Bk. E. Abut.	125+30.87	30.21' Rt.	739.12	739.12

**BEAM 14**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	122+35.23	36.29' Rt.	735.51	735.51
☉ Brg. W. Abut.	122+38.78	36.29' Rt.	735.62	735.62
1A	122+48.78	36.29' Rt.	735.90	735.97
1B	122+58.78	36.29' Rt.	736.18	736.29
1C	122+68.78	36.29' Rt.	736.44	736.58
1D	122+78.78	36.29' Rt.	736.69	736.84
1E	122+88.78	36.29' Rt.	736.93	737.07
1F	122+98.78	36.29' Rt.	737.15	737.27
1G	123+08.78	36.29' Rt.	737.36	737.44
1H	123+18.78	36.29' Rt.	737.57	737.60
☉ Pier 1	123+31.28	36.29' Rt.	737.80	737.80
2A	123+41.28	36.29' Rt.	737.97	737.97
2B	123+51.28	36.29' Rt.	738.14	738.14
2C	123+61.28	36.29' Rt.	738.29	738.30
2D	123+71.28	36.29' Rt.	738.42	738.46
2E	123+81.28	36.29' Rt.	738.55	738.59
2F	123+91.28	36.29' Rt.	738.66	738.70
2G	124+01.28	36.29' Rt.	738.77	738.79
2H	124+11.28	36.29' Rt.	738.86	738.86
2I	124+21.28	36.29' Rt.	738.93	738.93
☉ Pier 2	124+32.12	36.29' Rt.	739.00	739.00
3A	124+42.12	36.29' Rt.	739.06	739.08
3B	124+52.12	36.29' Rt.	739.10	739.16
3C	124+62.12	36.29' Rt.	739.13	739.23
3D	124+72.12	36.29' Rt.	739.14	739.28
3E	124+82.12	36.29' Rt.	739.15	739.30
3F	124+92.12	36.29' Rt.	739.14	739.29
3G	125+02.12	36.29' Rt.	739.12	739.24
3H	125+12.12	36.29' Rt.	739.09	739.16
☉ E. Abut.	125+24.62	36.29' Rt.	739.03	739.03
Bk. E. Abut.	125+28.17	36.29' Rt.	739.01	739.01

Note:  
Work this with Sheet S-6.



USER NAME =	DESIGNED - BAR	REVISED
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS 7  
STRUCTURE NO. 022-0512

SHEET NO. S-12 OF S-53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DuPAGE	383	175
CONTRACT NO. 60R06			ILLINOIS FED. AID PROJECT	

**BEAM 15**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	122+32.53	42.38' Rt.	735.49	735.49
⊕ Brg. W. Abut.	122+36.09	42.38' Rt.	735.59	735.59
1A	122+46.09	42.38' Rt.	735.88	735.94
1B	122+56.09	42.38' Rt.	736.16	736.27
1C	122+66.09	42.38' Rt.	736.42	736.57
1D	122+76.09	42.38' Rt.	736.68	736.83
1E	122+86.09	42.38' Rt.	736.92	737.06
1F	122+96.09	42.38' Rt.	737.15	737.26
1G	123+06.09	42.38' Rt.	737.36	737.44
1H	123+16.09	42.38' Rt.	737.57	737.60
⊕ Pier 1	123+28.59	42.38' Rt.	737.80	737.80
2A	123+38.59	42.38' Rt.	737.98	737.98
2B	123+48.59	42.38' Rt.	738.15	738.15
2C	123+58.59	42.38' Rt.	738.30	738.32
2D	123+68.59	42.38' Rt.	738.44	738.47
2E	123+78.59	42.38' Rt.	738.57	738.61
2F	123+88.59	42.38' Rt.	738.69	738.72
2G	123+98.59	42.38' Rt.	738.79	738.81
2H	124+08.59	42.38' Rt.	738.89	738.89
2I	124+18.59	42.38' Rt.	738.97	738.96
⊕ Pier 2	124+29.42	42.38' Rt.	739.04	739.04
3A	124+39.42	42.38' Rt.	739.10	739.12
3B	124+49.42	42.38' Rt.	739.14	739.21
3C	124+59.42	42.38' Rt.	739.17	739.28
3D	124+69.42	42.38' Rt.	739.19	739.33
3E	124+79.42	42.38' Rt.	739.20	739.36
3F	124+89.42	42.38' Rt.	739.20	739.35
3G	124+99.42	42.38' Rt.	739.18	739.30
3H	125+09.42	42.38' Rt.	739.15	739.23
⊕ E. Abut.	125+21.92	42.38' Rt.	739.10	739.10
Bk. E. Abut.	125+25.48	42.38' Rt.	739.08	739.08

**BEAM 16**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. W. Abut.	122+29.84	48.46' Rt.	735.47	735.47
⊕ Brg. W. Abut.	122+33.39	48.46' Rt.	735.58	735.58
1A	122+43.39	48.46' Rt.	735.87	735.94
1B	122+53.39	48.46' Rt.	736.15	736.27
1C	122+63.39	48.46' Rt.	736.42	736.58
1D	122+73.39	48.46' Rt.	736.67	736.85
1E	122+83.39	48.46' Rt.	736.92	737.08
1F	122+93.39	48.46' Rt.	737.15	737.28
1G	123+03.39	48.46' Rt.	737.37	737.45
1H	123+13.39	48.46' Rt.	737.58	737.62
⊕ Pier 1	123+25.89	48.46' Rt.	737.82	737.82
2A	123+35.89	48.46' Rt.	738.00	737.99
2B	123+45.89	48.46' Rt.	738.17	738.17
2C	123+55.89	48.46' Rt.	738.32	738.35
2D	123+65.89	48.46' Rt.	738.47	738.50
2E	123+75.89	48.46' Rt.	738.60	738.64
2F	123+85.89	48.46' Rt.	738.72	738.76
2G	123+95.89	48.46' Rt.	738.83	738.85
2H	124+05.89	48.46' Rt.	738.93	738.93
2I	124+15.89	48.46' Rt.	739.01	739.01
⊕ Pier 2	124+26.73	48.46' Rt.	739.09	739.09
3A	124+36.73	48.46' Rt.	739.15	739.18
3B	124+46.73	48.46' Rt.	739.19	739.27
3C	124+56.73	48.46' Rt.	739.23	739.35
3D	124+66.73	48.46' Rt.	739.25	739.41
3E	124+76.73	48.46' Rt.	739.26	739.43
3F	124+86.73	48.46' Rt.	739.26	739.43
3G	124+96.73	48.46' Rt.	739.25	739.38
3H	125+06.73	48.46' Rt.	739.22	739.31
⊕ E. Abut.	125+19.23	48.46' Rt.	739.18	739.18
Bk. E. Abut.	125+22.78	48.46' Rt.	739.16	739.16

Note:  
Work this with Sheet S-6.



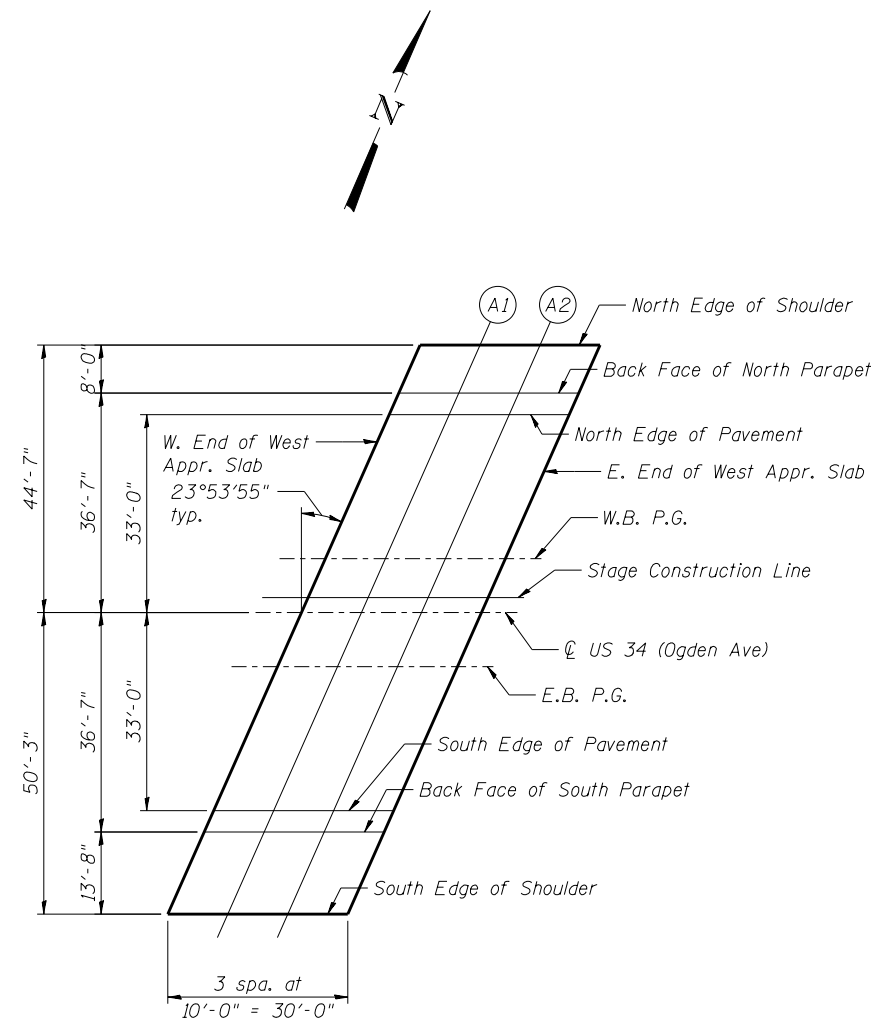
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PLOT DATE = 02/28/2014	CHECKED - KJZ	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TOP OF SLAB ELEVATIONS 8  
STRUCTURE NO. 022-0512

SHEET NO. S-13 OF S-53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DuPAGE	383	176
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60R06	



**PLAN**  
West Approach

**NORTH EDGE OF SHOULDER**

Location	Station	Offset	Theoretical Grade Elevations
W. End of West Appr. Slab	122+41.62	44.58' Lt.	735.78
A1	122+51.62	44.58' Lt.	736.06
A2	122+61.62	44.58' Lt.	736.33
E. End of West Appr. Slab	122+71.62	44.58' Lt.	736.59

**BACK FACE OF NORTH PARAPET**

Location	Station	Offset	Theoretical Grade Elevations
W. End of West Appr. Slab	122+38.07	36.58' Lt.	735.59
A1	122+48.07	36.58' Lt.	735.88
A2	122+58.07	36.58' Lt.	736.15
E. End of West Appr. Slab	122+68.07	36.58' Lt.	736.41

**NORTH EDGE OF PAVEMENT**

Location	Station	Offset	Theoretical Grade Elevations
W. End of West Appr. Slab	122+36.48	33' Lt.	735.62
A1	122+46.48	33' Lt.	735.91
A2	122+56.48	33' Lt.	736.18
E. End of West Appr. Slab	122+66.48	33' Lt.	736.45

**WEST BOUND PROFILE GRADE**

Location	Station	Offset	Theoretical Grade Elevations
W. End of West Appr. Slab	122+25.85	9.00' Lt.	735.80
A1	122+35.85	9.00' Lt.	736.10
A2	122+45.85	9.00' Lt.	736.39
E. End of West Appr. Slab	122+55.85	9.00' Lt.	736.67

**STAGE CONSTRUCTION LINE**

Location	Station	Offset	Theoretical Grade Elevations
W. End of West Appr. Slab	122+22.97	2.50' Lt.	735.85
A1	122+32.97	2.50' Lt.	736.15
A2	122+42.97	2.50' Lt.	736.44
E. End of West Appr. Slab	122+52.97	2.50' Lt.	736.72

**CL US 34 (OGDEN AVE)**

Location	Station	Offset	Theoretical Grade Elevations
W. End of West Appr. Slab	122+21.86	0.00'	735.86
A1	122+31.86	0.00'	736.17
A2	122+41.86	0.00'	736.46
E. End of West Appr. Slab	122+51.86	0.00'	736.75

**EAST BOUND PROFILE GRADE**

Location	Station	Offset	Theoretical Grade Elevations
W. End of West Appr. Slab	122+17.87	9.00' Rt.	735.55
A1	122+27.87	9.00' Rt.	735.86
A2	122+37.87	9.00' Rt.	736.16
E. End of West Appr. Slab	122+47.87	9.00' Rt.	736.45

**SOUTH EDGE OF PAVEMENT**

Location	Station	Offset	Theoretical Grade Elevations
W. End of West Appr. Slab	122+07.24	33' Rt.	734.71
A1	122+17.24	33' Rt.	735.03
A2	122+27.24	33' Rt.	735.34
E. End of West Appr. Slab	122+37.24	33' Rt.	735.64

**BACK FACE OF SOUTH PARAPET**

Location	Station	Offset	Theoretical Grade Elevations
W. End of West Appr. Slab	122+05.65	36.58' Rt.	734.58
A1	122+15.65	36.58' Rt.	734.91
A2	122+25.65	36.58' Rt.	735.22
E. End of West Appr. Slab	122+35.65	36.58' Rt.	735.52

**SOUTH EDGE OF SHOULDER**

Location	Station	Offset	Theoretical Grade Elevations
W. End of West Appr. Slab	121+99.59	50.25' Rt.	734.52
A1	122+09.59	50.25' Rt.	734.85
A2	122+19.59	50.25' Rt.	735.17
E. End of West Appr. Slab	122+29.59	50.25' Rt.	735.48



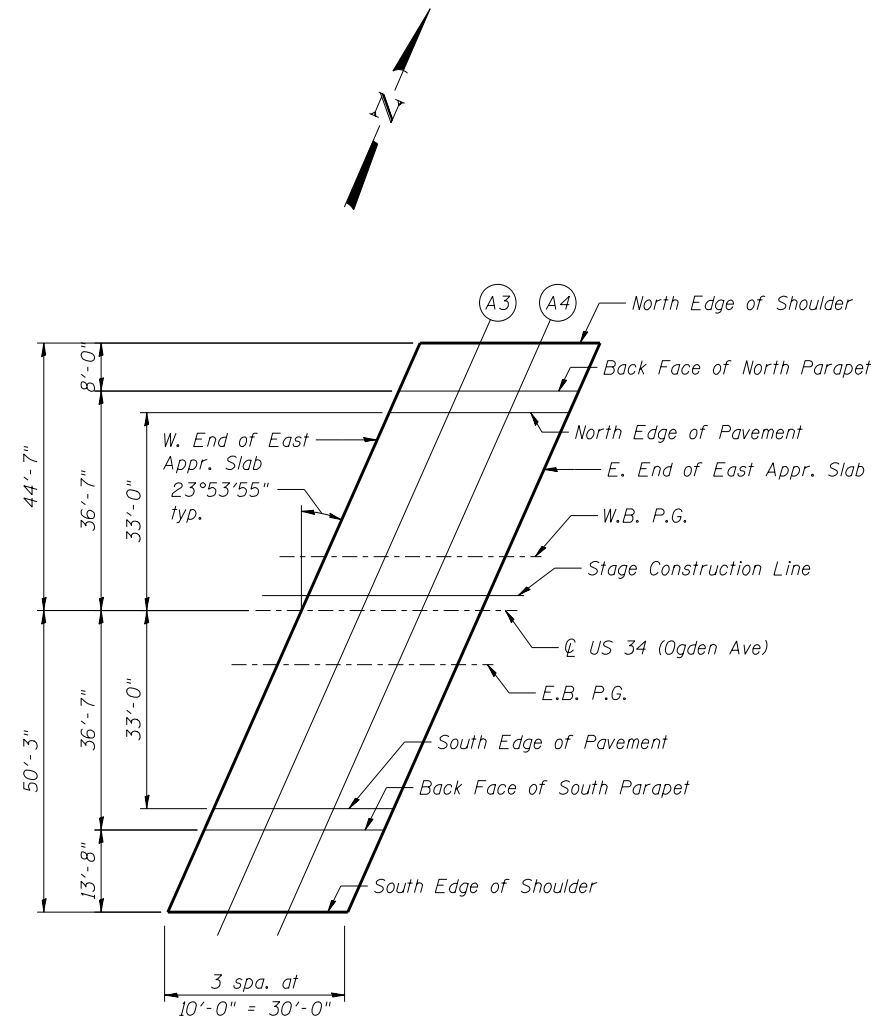
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PLOT DATE = 02/28/2014	CHECKED - KJZ	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TOP OF WEST APPROACH SLAB ELEVATIONS  
STRUCTURE NO. 022-0512

SHEET NO. S-14 OF S-53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DuPAGE	383	177
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60R06	



**PLAN**  
East Approach

**NORTH EDGE OF SHOULDER**

Location	Station	Offset	Theoretical Grade Elevations
W. End of East Apr. Slab	125+63.46	44.58' Lt.	738.81
A3	125+73.46	44.58' Lt.	738.71
A4	125+83.46	44.58' Lt.	738.59
E. End of East Apr. Slab	125+93.46	44.58' Lt.	738.46

**BACK FACE OF NORTH PARAPET**

Location	Station	Offset	Theoretical Grade Elevations
W. End of East Apr. Slab	125+59.92	36.58' Lt.	738.76
A3	125+69.92	36.58' Lt.	738.66
A4	125+79.92	36.58' Lt.	738.55
E. End of East Apr. Slab	125+89.92	36.58' Lt.	738.42

**NORTH EDGE OF PAVEMENT**

Location	Station	Offset	Theoretical Grade Elevations
W. End of East Apr. Slab	125+58.33	33' Lt.	738.85
A3	125+68.33	33' Lt.	738.75
A4	125+78.33	33' Lt.	738.64
E. End of East Apr. Slab	125+88.33	33' Lt.	738.52

**WEST BOUND PROFILE GRADE**

Location	Station	Offset	Theoretical Grade Elevations
W. End of East Apr. Slab	125+47.69	9.00' Lt.	739.45
A3	125+57.69	9.00' Lt.	739.36
A4	125+67.69	9.00' Lt.	739.26
E. End of East Apr. Slab	125+77.69	9.00' Lt.	739.15

**STAGE CONSTRUCTION LINE**

Location	Station	Offset	Theoretical Grade Elevations
W. End of East Apr. Slab	125+44.81	2.50' Lt.	739.60
A3	125+54.81	2.50' Lt.	739.52
A4	125+64.81	2.50' Lt.	739.43
E. End of East Apr. Slab	125+74.81	2.50' Lt.	739.32

**CL US 34 (OGDEN AVE)**

Location	Station	Offset	Theoretical Grade Elevations
W. End of East Apr. Slab	125+43.71	0.00'	739.66
A3	125+53.71	0.00'	739.58
A4	125+63.71	0.00'	739.49
E. End of East Apr. Slab	125+73.71	0.00'	739.38

**EAST BOUND PROFILE GRADE**

Location	Station	Offset	Theoretical Grade Elevations
W. End of East Apr. Slab	125+39.72	9.00' Rt.	739.51
A3	125+49.72	9.00' Rt.	739.43
A4	125+59.72	9.00' Rt.	739.34
E. End of East Apr. Slab	125+69.72	9.00' Rt.	739.24

**SOUTH EDGE OF PAVEMENT**

Location	Station	Offset	Theoretical Grade Elevations
W. End of East Apr. Slab	125+29.08	33' Rt.	739.07
A3	125+39.08	33' Rt.	739.01
A4	125+49.08	33' Rt.	738.93
E. End of East Apr. Slab	125+59.08	33' Rt.	738.85

**BACK FACE OF SOUTH PARAPET**

Location	Station	Offset	Theoretical Grade Elevations
W. End of East Apr. Slab	125+27.50	36.58' Rt.	739.01
A3	125+37.50	36.58' Rt.	738.95
A4	125+47.50	36.58' Rt.	738.87
E. End of East Apr. Slab	125+57.50	36.58' Rt.	738.79

**SOUTH EDGE OF SHOULDER**

Location	Station	Offset	Theoretical Grade Elevations
W. End of East Apr. Slab	125+21.44	50.25' Rt.	739.18
A3	125+31.44	50.25' Rt.	739.13
A4	125+41.44	50.25' Rt.	739.06
E. End of East Apr. Slab	125+51.44	50.25' Rt.	738.98



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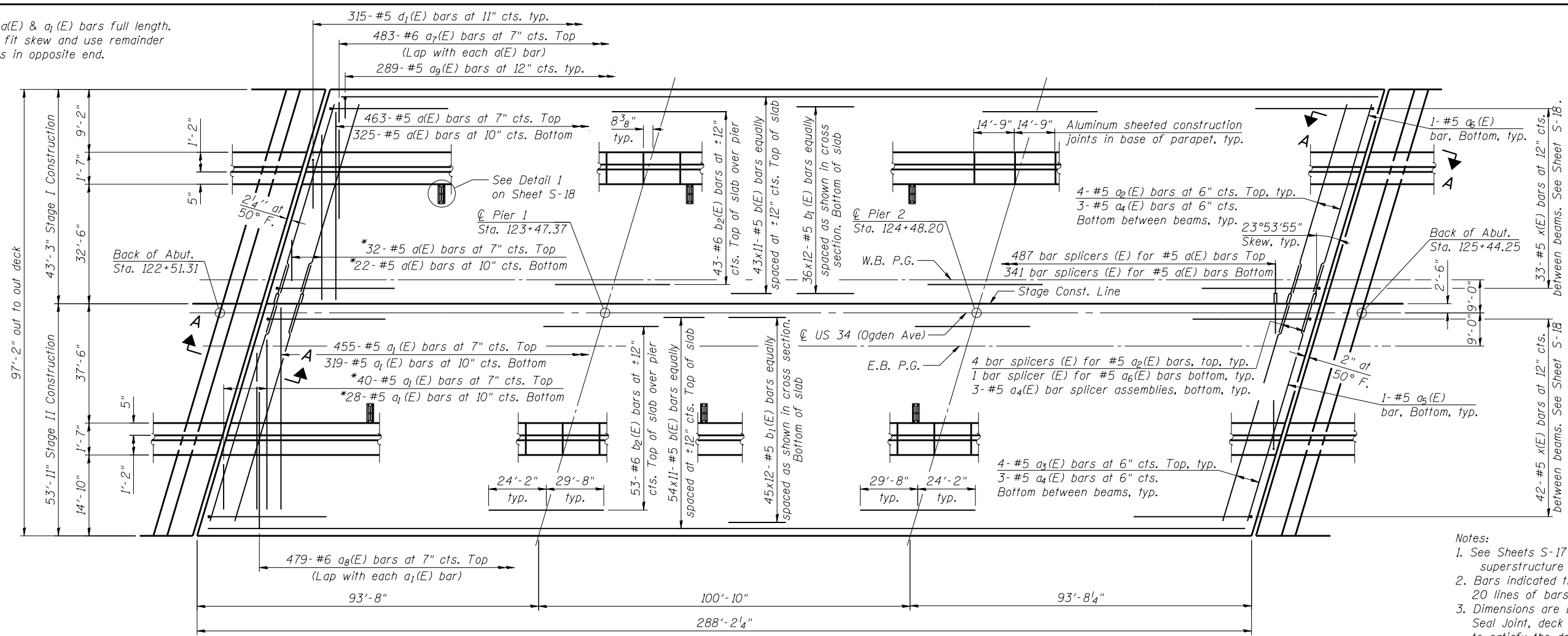
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TOP OF EAST APPROACH SLAB ELEVATIONS  
STRUCTURE NO. 022-0512**

SHEET NO. S-15 OF S-53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DuPAGE	383	178
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60R06	

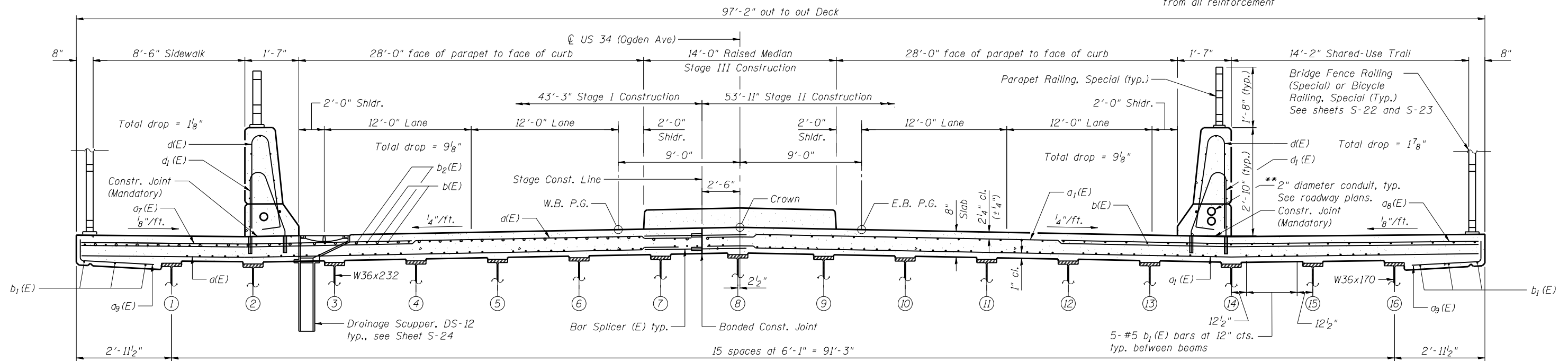
\*Order a(E) & a<sub>1</sub>(E) bars full length.  
Cut to fit skew and use remainder  
of bars in opposite end.



**PLAN**

- Notes:
1. See Sheets S-17 and S-18 for parapet, median, superstructure details and Bill of Material.
  2. Bars indicated thus 20 x 3-#5 etc. indicates 20 lines of bars with 3 lengths per line.
  3. Dimensions are based on a Rolled Rail Strip Seal Joint, deck dimensions may require adjustments to satisfy the details Sheet S-26.
  4. See Sheet S-18 for Section A-A.

\*\* Maintain 1/2" min. cl. from all reinforcement



**CROSS SECTION**  
(Looking East)

**MINIMUM BAR LAP**  
#5 Bars = 3'-3"



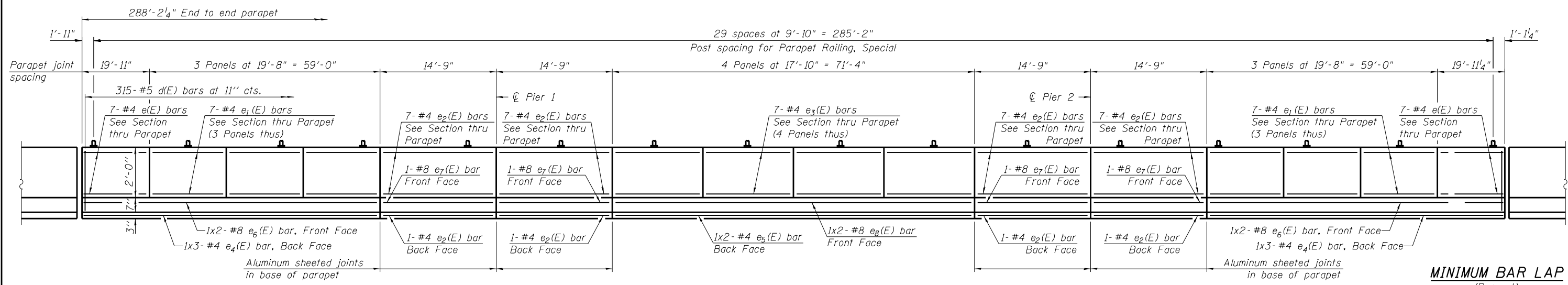
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PLOT DATE = 02/28/2014	CHECKED - AMK	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE PLAN AND CROSS SECTION  
STRUCTURE NO. 022-0512

SHEET NO. S-16 OF S-53 SHEETS

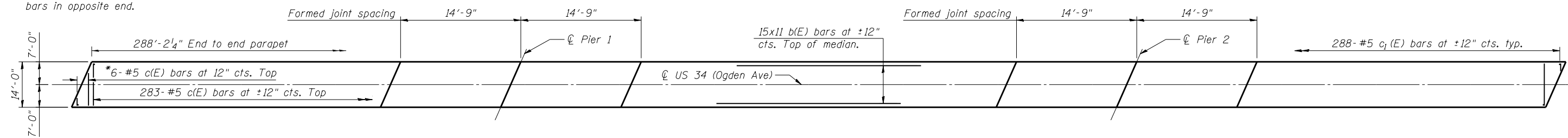
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311	652-A	DuPAGE	383	179
CONTRACT NO. 60R06			ILLINOIS FED. AID PROJECT	



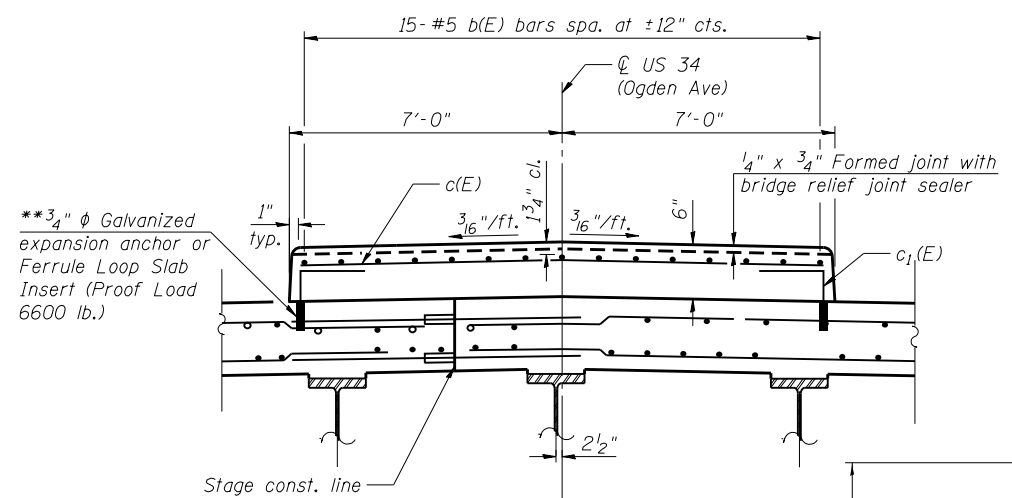
**INSIDE ELEVATION OF PARAPET**  
(All spacings are relative to front face of parapet)

**MINIMUM BAR LAP**  
(Parapet)  
#4 bar = 2'-0"  
#8 bar = 5'-2"

\*Order c(E) bars full length. Cut to fit skew and use remainder of bars in opposite end.

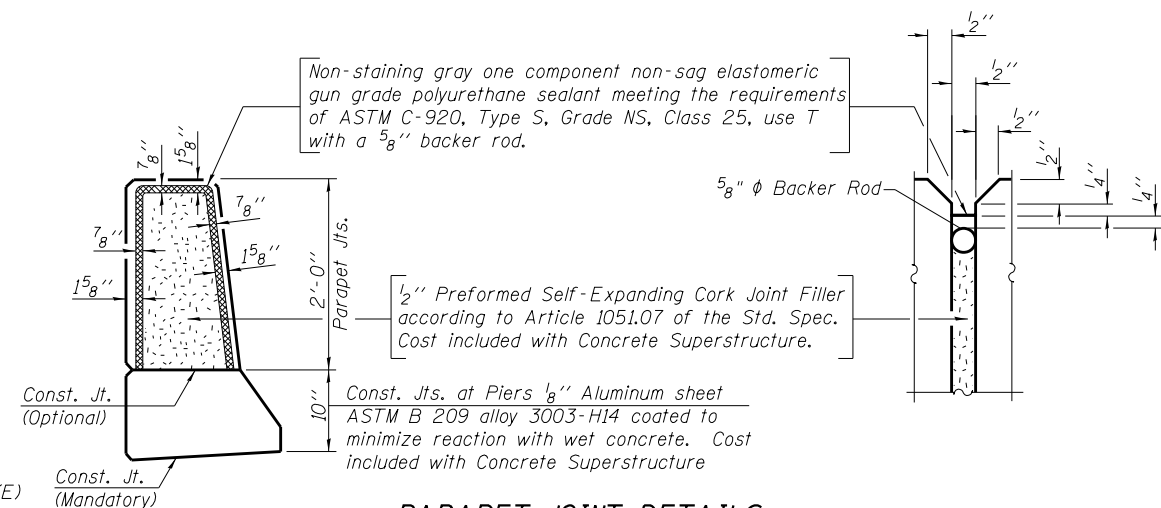


**RAISED CURB MEDIAN PLAN**

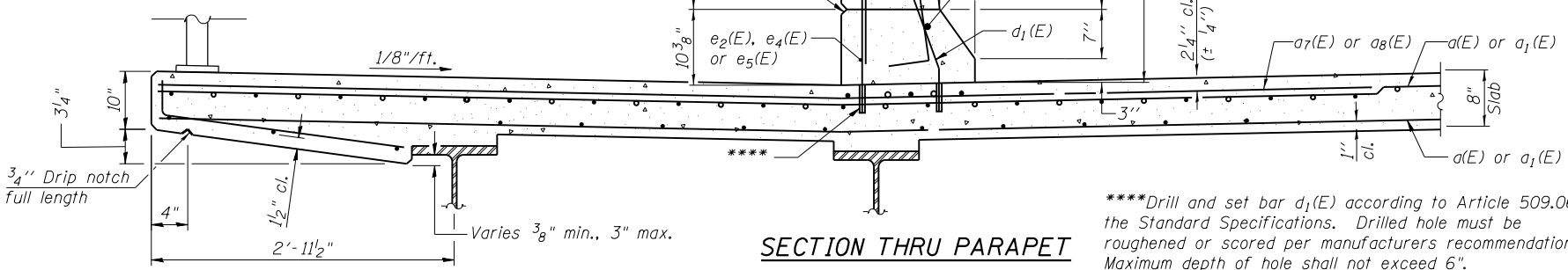


**RAISED CURB MEDIAN SECTION**

\*\*Cost of expansion anchors/inserts is included in the cost of Reinforcement Bars. Epoxy Coated



**PARAPET JOINT DETAILS**



**SECTION THRU PARAPET**

\*\*\*\*Drill and set bar d<sub>1</sub>(E) according to Article 509.06 of the Standard Specifications. Drilled hole must be roughened or scored per manufacturers recommendations. Maximum depth of hole shall not exceed 6\".

note: See Sheet S-18 for Bill of Material and bar bending diagrams.



USER NAME =	DESIGNED - BAR	REVISED
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PLOT SCALE =	DRAWN - BAR	REVISED
PLOT DATE = 03/19/2014	CHECKED - NPP	REVISED

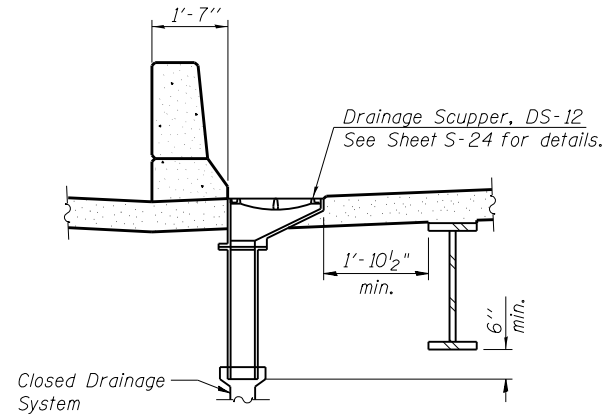
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PARAPET ELEVATIONS AND DETAILS  
STRUCTURE NO. 022-0512**

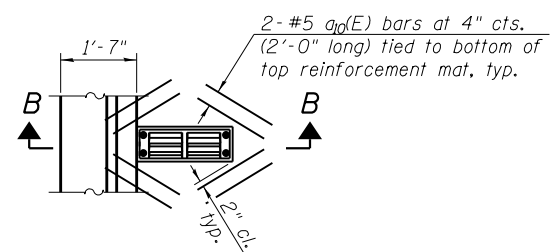
SHEET NO. S-17 OF S-53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 60R06				
ILLINOIS FED. AID PROJECT				



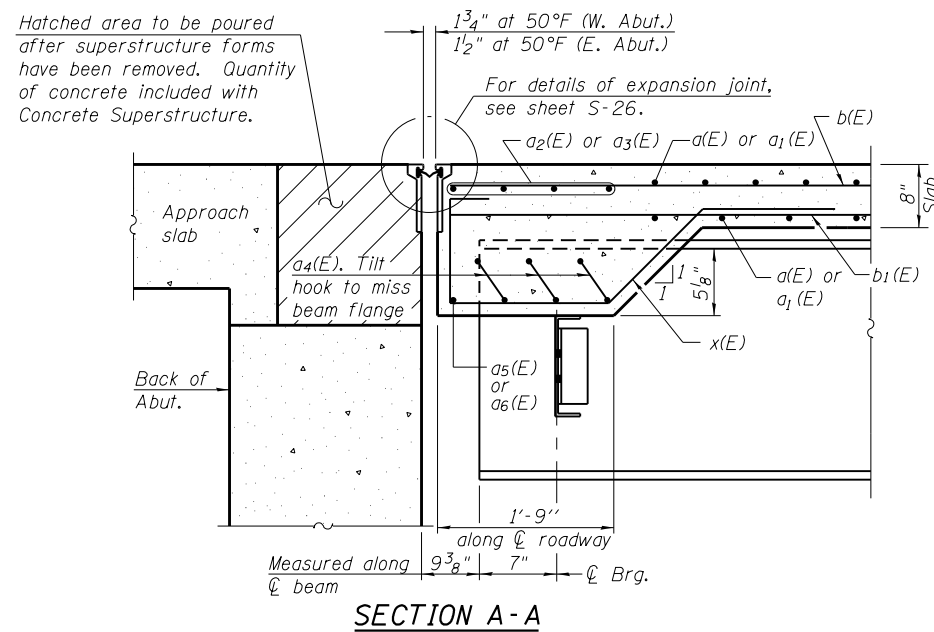


**SECTION B-B**

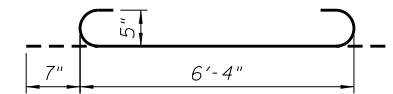


**DETAIL 1 - PLAN**

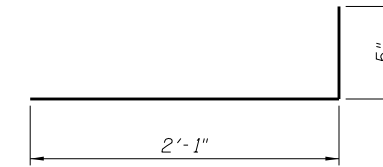
Note:  
Cut longitudinal reinforcement to  
clear drainage scuppers.



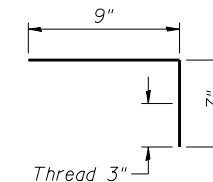
**SECTION A-A**



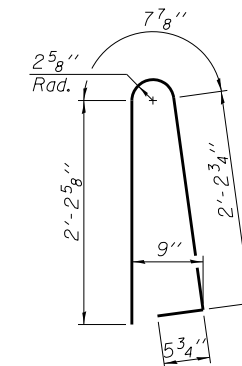
**BAR a<sub>4</sub>(E)**



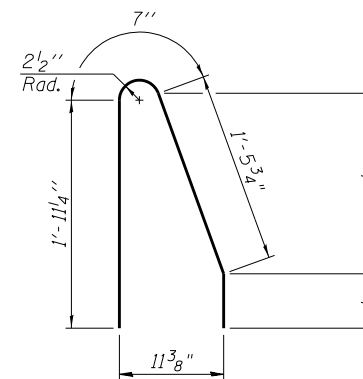
**BAR a<sub>9</sub>(E)**



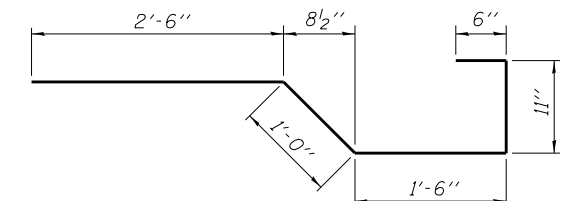
**BAR c<sub>1</sub>(E)**



**BAR d(E)**



**BAR d<sub>1</sub>(E)**



**BAR x(E)**

**SUPERSTRUCTURE  
BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a(E)	842	#5	42'-10"	—
a <sub>1</sub> (E)	842	#5	53'-6"	—
a <sub>2</sub> (E)	8	#5	46'-11"	—
a <sub>3</sub> (E)	8	#5	58'-7"	—
a <sub>4</sub> (E)	84	#5	7'-6"	U
a <sub>5</sub> (E)	2	#5	55'-7"	—
a <sub>6</sub> (E)	2	#5	43'-5"	—
a <sub>7</sub> (E)	483	#6	15'-9"	—
a <sub>8</sub> (E)	479	#6	21'-5"	—
a <sub>9</sub> (E)	578	#5	2'-6"	U
a <sub>10</sub> (E)	48	#5	2'-0"	—
b(E)	1232	#5	29'-2"	—
b <sub>1</sub> (E)	972	#5	27'-0"	—
b <sub>2</sub> (E)	190	#6	53'-10"	—
c(E)	289	#5	13'-6"	—
c <sub>1</sub> (E)	576	#5	1'-4"	L
d(E)	630	#5	5'-7"	U
d <sub>1</sub> (E)	630	#5	4'-7 1/2"	U
e(E)	28	#4	19'-7"	—
e <sub>1</sub> (E)	84	#4	19'-4"	—
e <sub>2</sub> (E)	64	#4	14'-5"	—
e <sub>3</sub> (E)	56	#4	17'-6"	—
e <sub>4</sub> (E)	12	#4	27'-7"	—
e <sub>5</sub> (E)	4	#4	36'-6"	—
e <sub>6</sub> (E)	8	#8	41'-11"	—
e <sub>7</sub> (E)	8	#8	14'-5"	—
e <sub>8</sub> (E)	4	#8	38'-1"	—
x(E)	150	#5	6'-5"	U
Reinforcement Bars, Epoxy Coated			Pound	212,240
Concrete Superstructure			Cu. Yds.	918.8

Bars indicated thus 1 x 7 - #8 etc. indicates  
1 line of bars with 7 lengths per line.



USER NAME =	DESIGNED - BAR	REVISED
	CHECKED - AMK	REVISED
PLOT SCALE =	DRAWN - BAR	REVISED
PLOT DATE = 02/28/2014	CHECKED - AMK	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

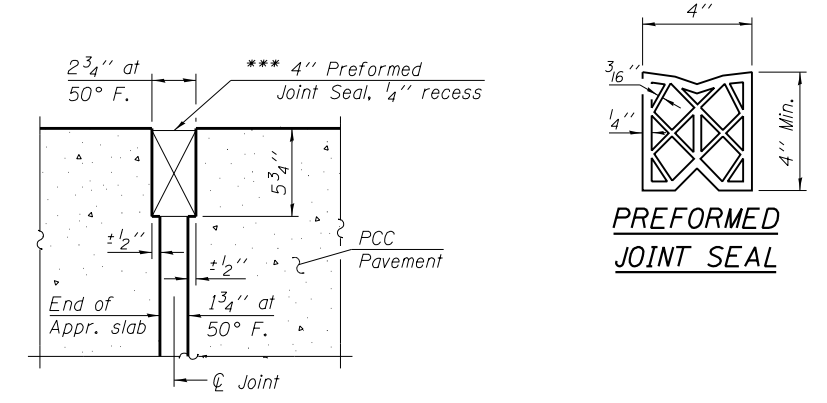
DECK DETAILS  
STRUCTURE NO. 022-0512

SHEET NO. S-18 OF S-53 SHEETS

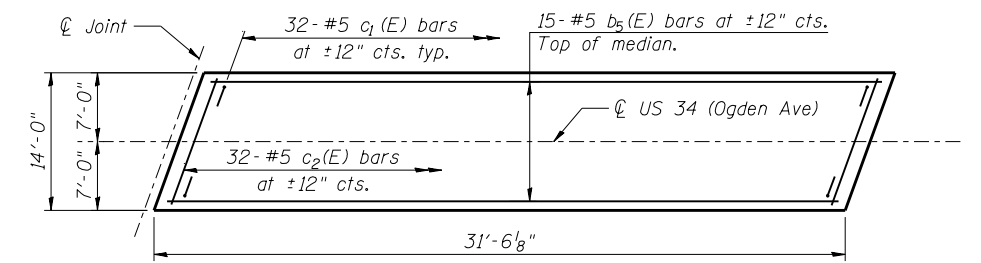
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DuPAGE	383	181
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60R06	

Notes:  
 See sheet S-20 of S-53 for Sections C-C & D-D and View E-E.  
 $a_{11}(E)$  thru  $a_{14}(E)$  bar spacings measured along  $\text{C} \perp$  Rdwy.  
 The joint opening shall be determined per Article 520.04. The minimum dimension shall be 1 1/2" for installation purposes.

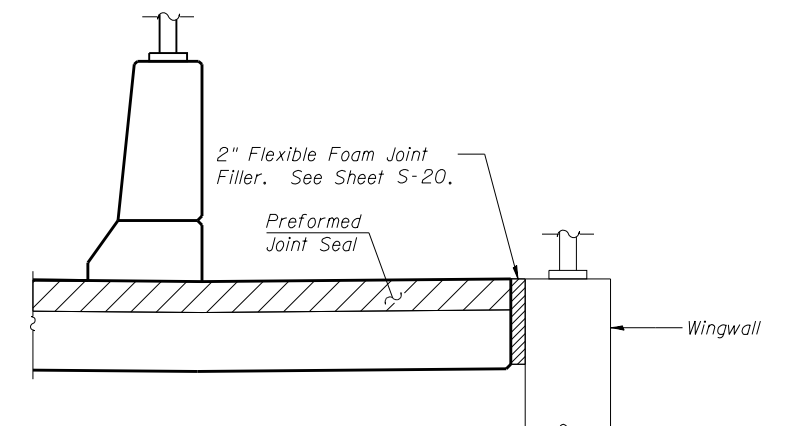
\*\*\* Cost included with Concrete Superstructure.



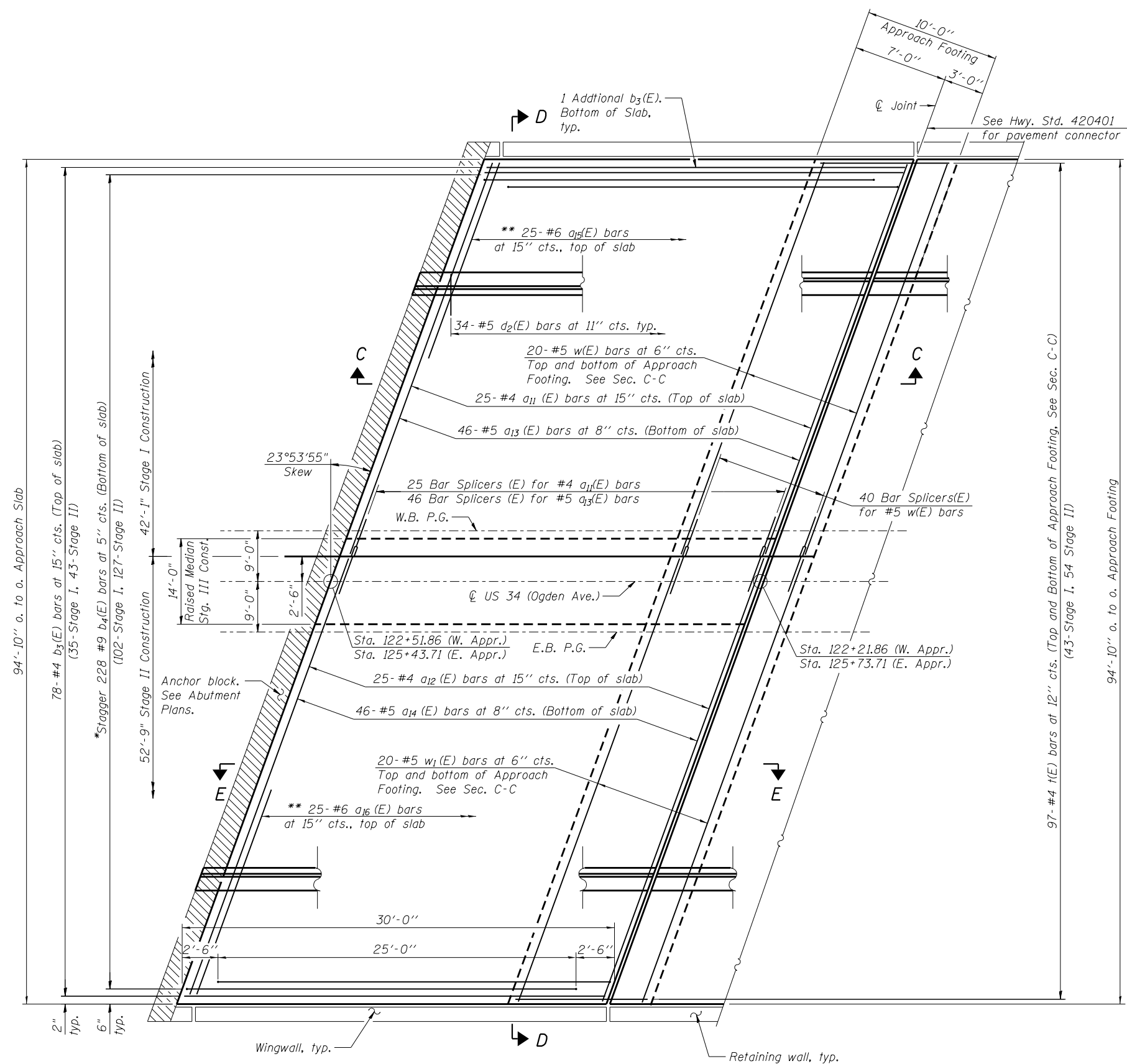
**DETAIL A**



**APPROACH SLAB MEDIAN PLAN**



**VIEW B-B**



**PLAN**

East Approach Shown  
 (West Approach Similar, Opposite Hand)

\* Tilt #9  $b_4(E)$  bars as required to maintain clearance.  
 \*\* Space between  $a_{11}(E)$  or  $a_{12}(E)$  bars, typ. each parapet.



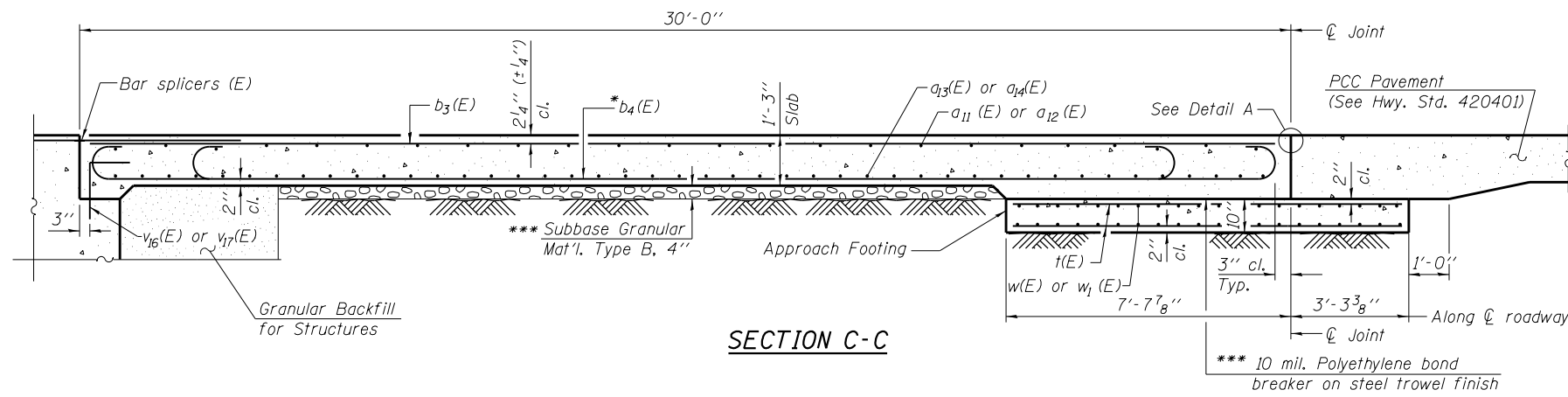
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	CHECKED - AMK	REVISED
PLOT SCALE =	DRAWN - BAR	REVISED
PLOT DATE = 02/28/2014	CHECKED - AMK	REVISED

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

BRIDGE APPROACH SLAB DETAILS 1  
 STRUCTURE NO. 022-0512

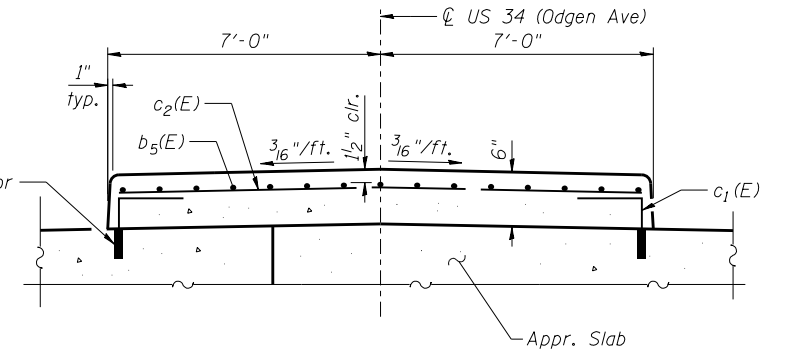
SHEET NO. S-19 OF S-53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DuPAGE	383	182
CONTRACT NO. 60R06				
ILLINOIS FED. AID PROJECT				

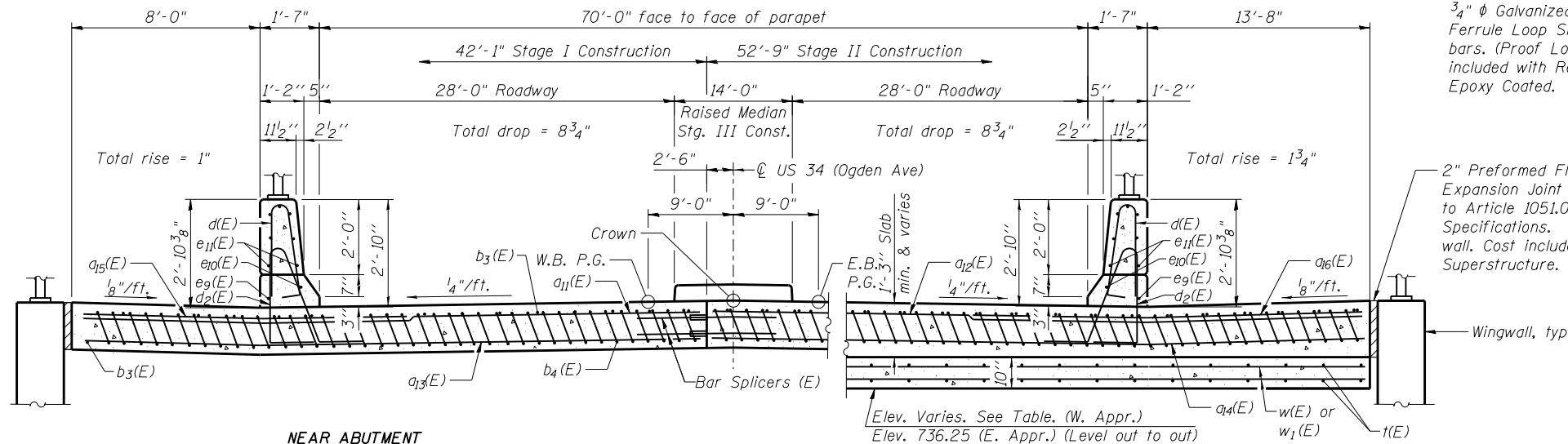


**SECTION C-C**

Notes:  
 See sheet S-19 of S-53 for Detail A and View B-B.  
 Approach slab and parapet concrete shall be paid for as Concrete Superstructure.  
 Approach footing concrete shall be paid for as Concrete Structures.  
 Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.  
 For  $v_{16}(E)$  or  $v_{17}(E)$  bar details, see sheets S-33 or S-36 of S-53.  
 The approach footing maximum applied service bearing pressure ( $Q_{max}$ ) = 2.0 ksf.  
 For bar splicer details, see sheet S-42 of S-53.  
 Cost of excavation for approach footing included with Concrete Structures.  
 For Granular Backfill for Structures and drainage treatment details, see sheet S-37 of S-53.  
 For additional parapet details, see sheet S-17 of S-53.



**SECTION THROUGH APPR. SLAB MEDIAN**



**NEAR ABUTMENT**

**SECTION D-D**

**AT APPROACH FOOTING**

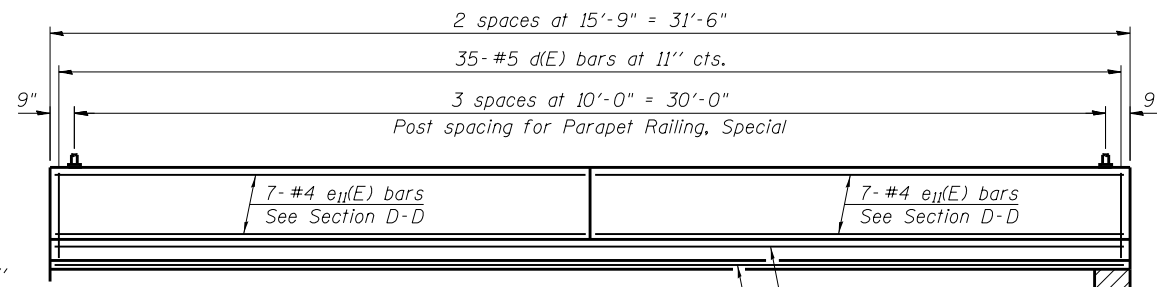
\* Tilt #9  $b_4(E)$  bars as required to maintain clearance.

(See Plan for dimensions not shown)

\*\*\* Cost included with Concrete Superstructure.

**WEST APPROACH SLAB FOOTING ELEVATIONS**

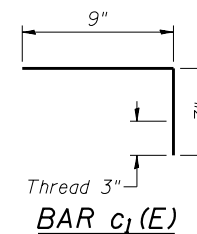
Location	Elevation
North Edge	733.37
Stage Construction Line	733.68
US 34 / Crown	733.70
South Edge	731.93



**VIEW E-E**

**BAR d(E)**

**BAR d2(E)**

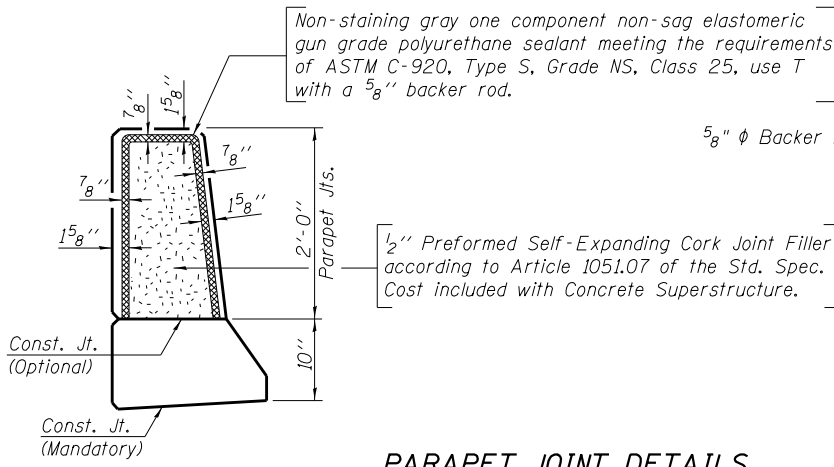


**BAR c1(E)**

**BAR d2(E)**

**TWO APPROACHES BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
$a_{11}(E)$	50	#4	45'-8"	—
$a_{12}(E)$	50	#4	57'-4"	—
$a_{13}(E)$	92	#5	45'-8"	—
$a_{14}(E)$	92	#5	57'-4"	—
$a_{15}(E)$	50	#6	15'-3"	—
$a_{16}(E)$	50	#6	21'-6"	—
$b_3(E)$	160	#4	29'-7"	—
$b_4(E)$	456	#9	29'-9"	—
$b_5(E)$	30	#5	31'-2"	—
$c_1(E)$	128	#5	1'-4"	—
$c_2(E)$	64	#5	14'-11"	—
$d(E)$	140	#5	5'-7"	—
$d_2(E)$	136	#5	7'-11"	—
$e_9(E)$	4	#4	31'-2"	—
$e_{10}(E)$	4	#8	31'-2"	—
$e_{11}(E)$	56	#4	15'-5"	—
$t(E)$	388	#4	10'-7"	—
$w(E)$	80	#5	45'-8"	—
$w_1(E)$	80	#5	57'-4"	—
Concrete Superstructure		Cu. Yd.	309.9	
Concrete Structures		Cu. Yd.	64.1	
Reinforcement Bars, Epoxy Coated		Pound	81,790	



**PARAPET JOINT DETAILS**



USER NAME =	DESIGNED - BAR	REVISED
	CHECKED - AMK	REVISED
PLOT SCALE =	DRAWN - BAR	REVISED
PLOT DATE = 02/28/2014	CHECKED - AMK	REVISED

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

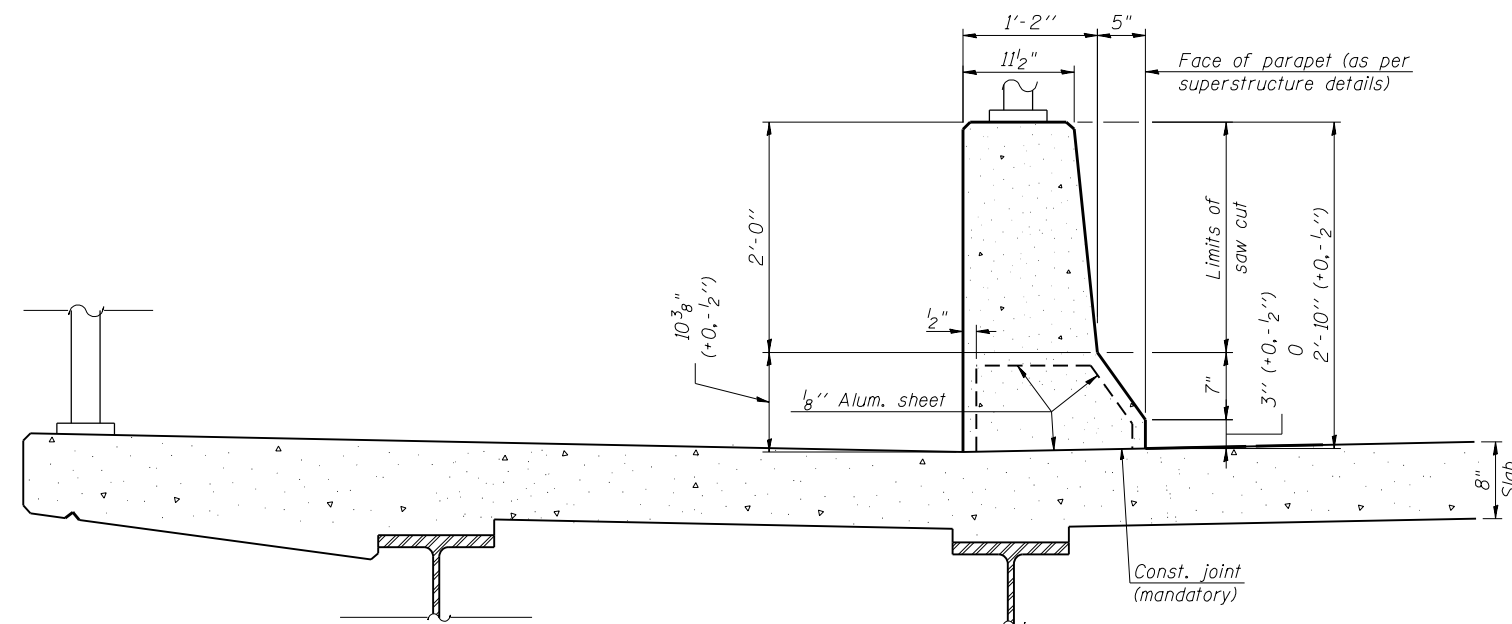
BRIDGE APPROACH SLAB DETAILS 2  
 STRUCTURE NO. 022-0512

SHEET NO. S-20 OF S-53 SHEETS

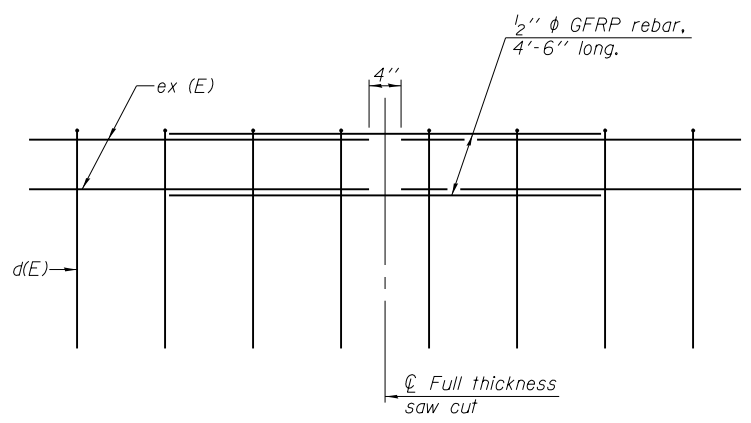
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DuPAGE	383	183
CONTRACT NO. 60R06			ILLINOIS FED. AID PROJECT	

**GENERAL NOTES**

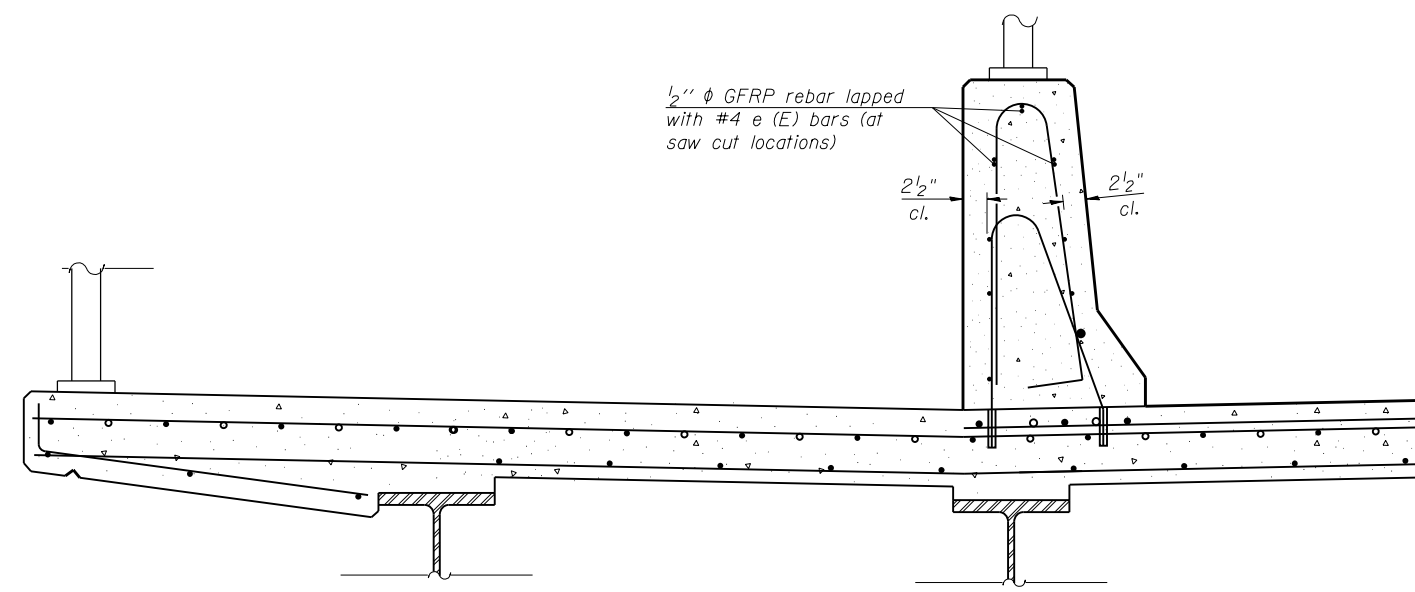
All dimensions shall remain the same as shown on superstructure details.  
Place aluminum sheet in curb portion at and near piers. Full thickness saw cut at all joint locations in lieu of cork joint filler.



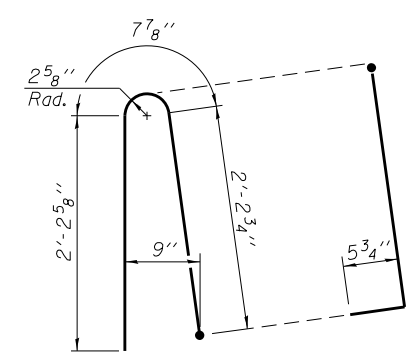
**SECTION**  
(Showing dimensions)



**GFRP REBAR STIFFENING DETAIL**  
(Place as shown in parapet section at each parapet joint location.)



**SECTION**  
(Showing reinforcement clearances for slip forming and additional reinforcement bars)



**ALTERNATE BAR d(E)**  
(For 34" parapet when conduit is present)



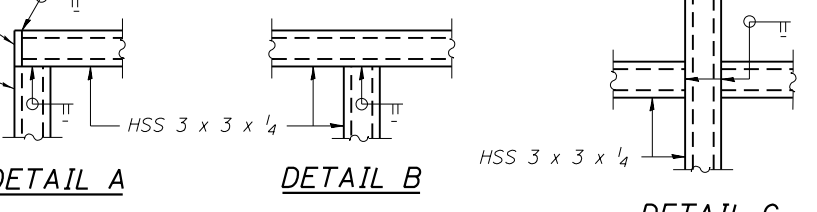
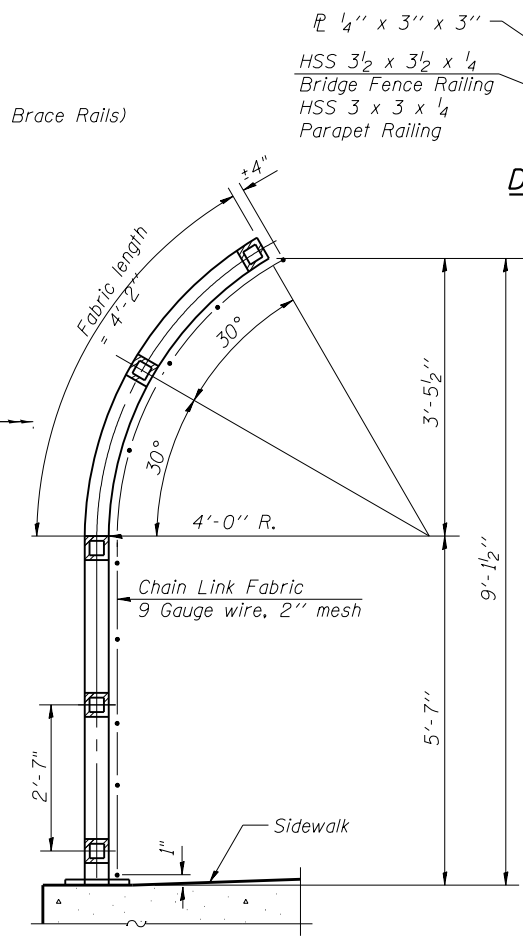
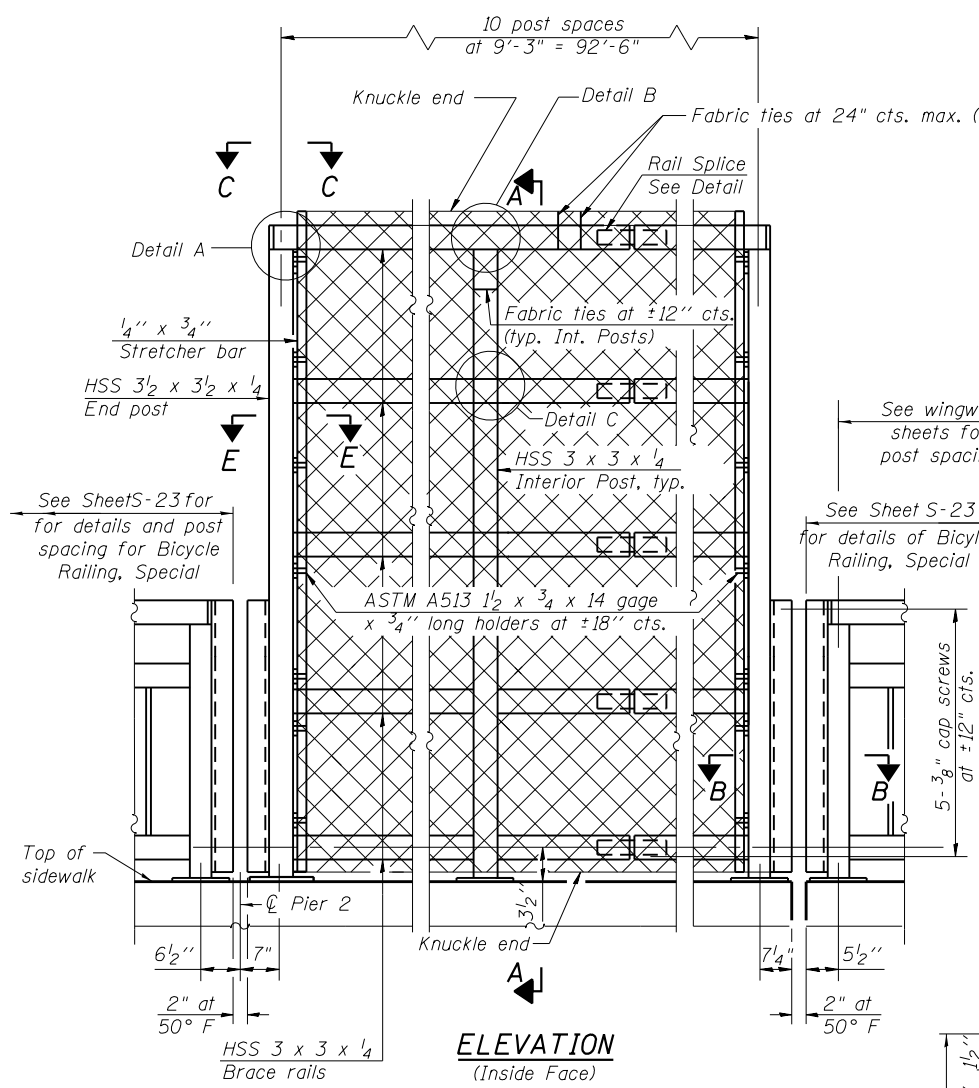
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	CHECKED - NPP	REVISED
PLOT SCALE =	DRAWN - BAR	REVISED
PLOT DATE = 02/28/2014	CHECKED - NPP	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CONCRETE PARAPET SLIPFORMING OPTION  
STRUCTURE NO. 022-0512

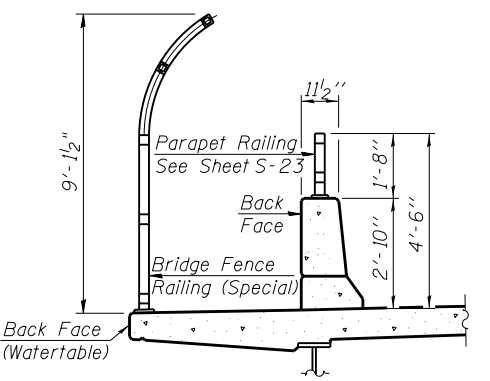
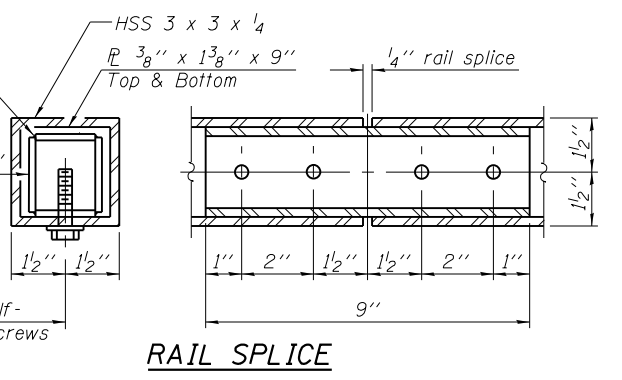
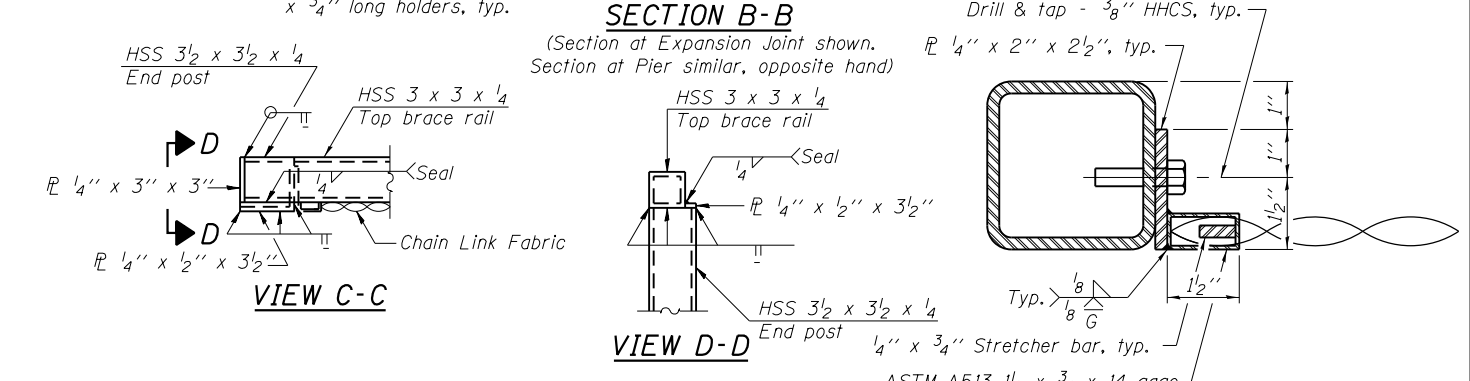
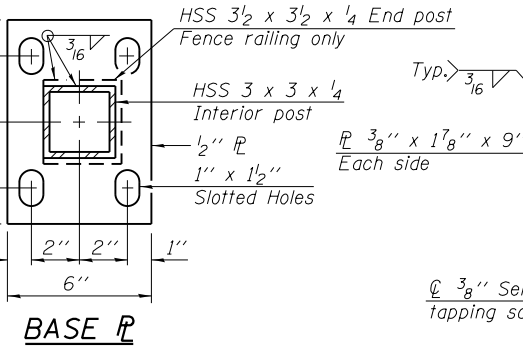
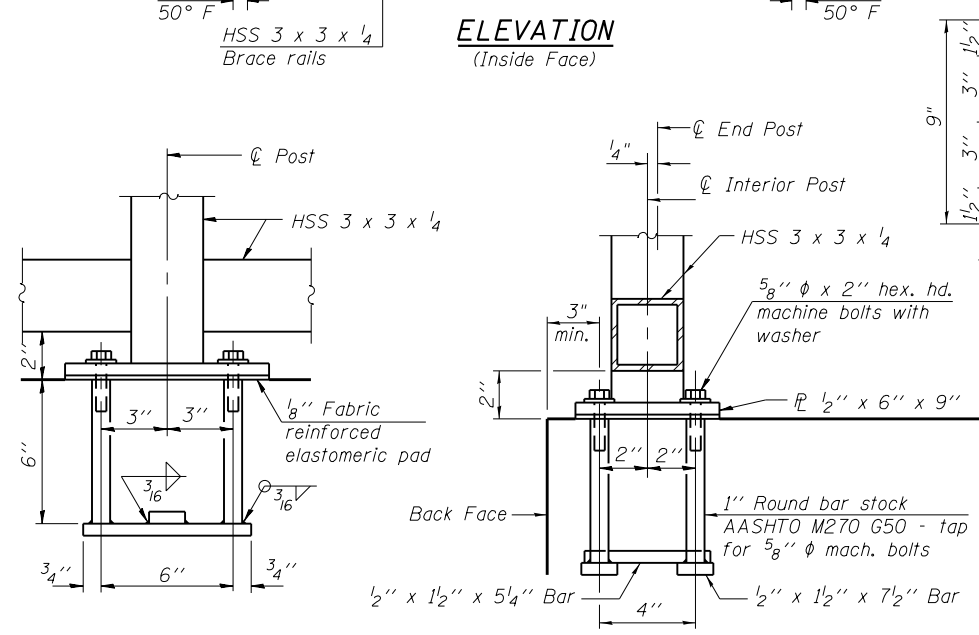
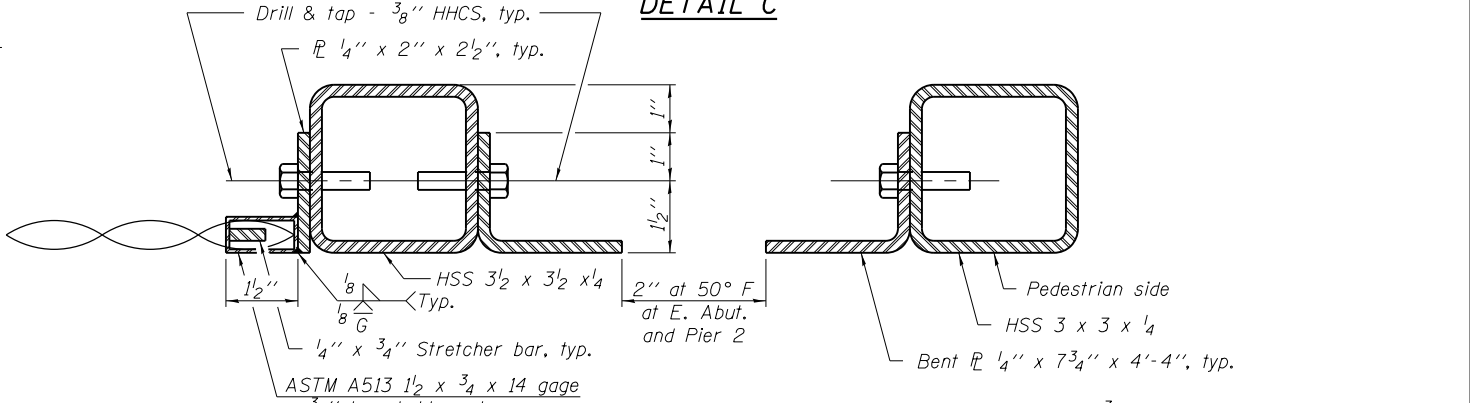
SHEET NO. S-21 OF S-53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DuPAGE	383	184
CONTRACT NO. 60R06				
ILLINOIS FED. AID PROJECT				



All steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.

All steel rail elements shall be finished with Black Polyester Powder Coating over galvanized steel. See Special Provisions.



**ANCHOR BOLT DETAILS**

In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting 5/8" phi anchor rods according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications.

**BILL OF MATERIAL**

Item	Unit	Quantity
Bridge Fence Railing (Special)	Foot	188

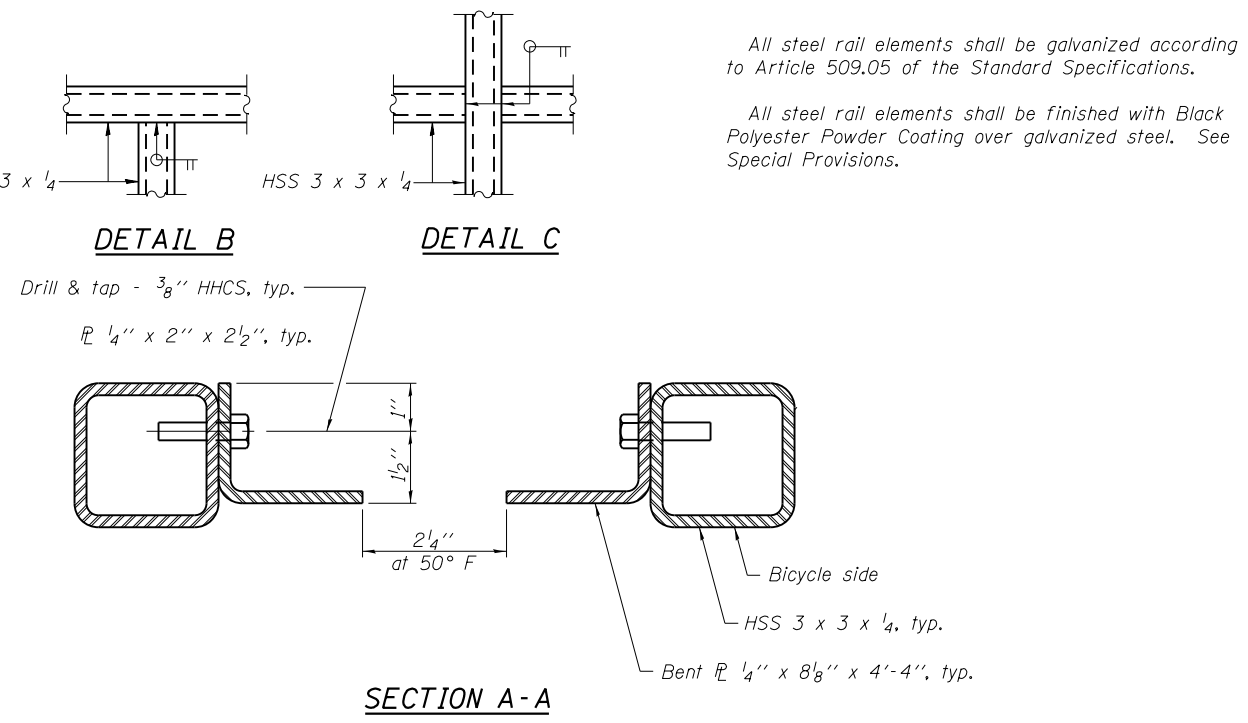
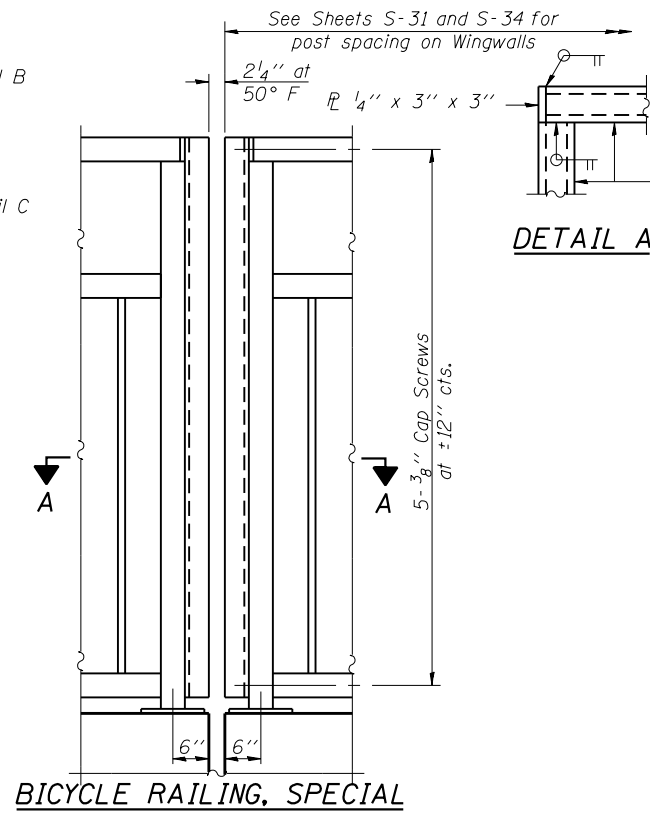
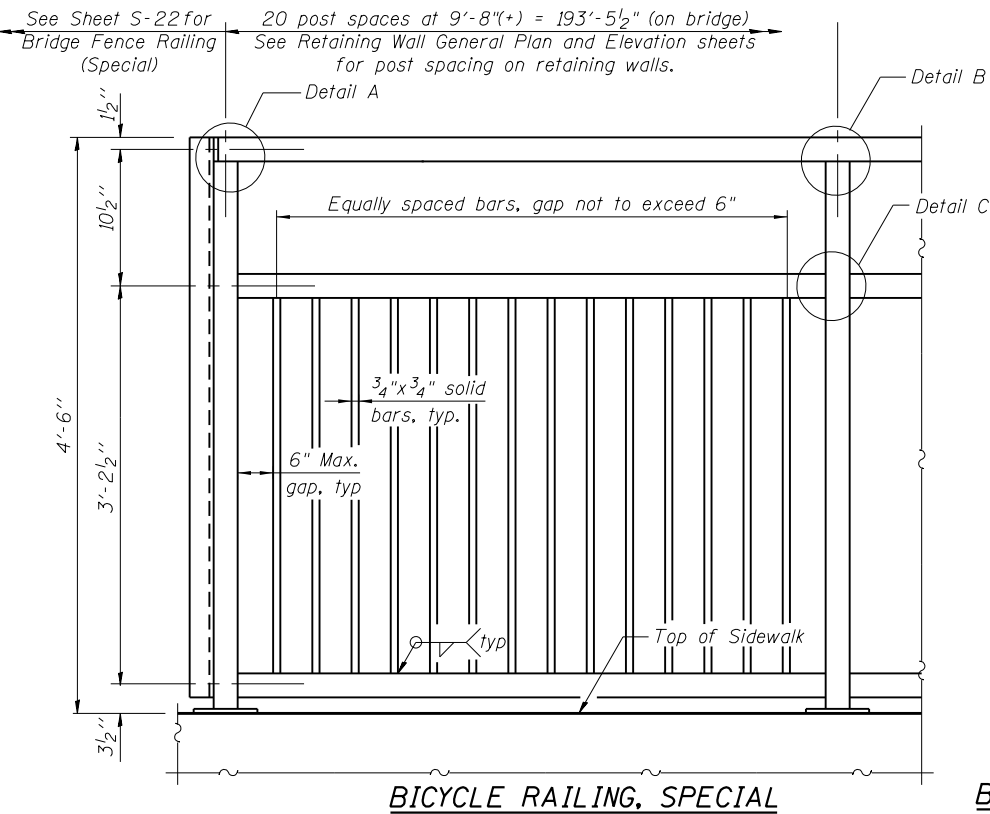


USER NAME =	DESIGNED - BAR	REVISED
	CHECKED - AMK	REVISED
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PLOT DATE = 02/28/2014	CHECKED - AMK	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

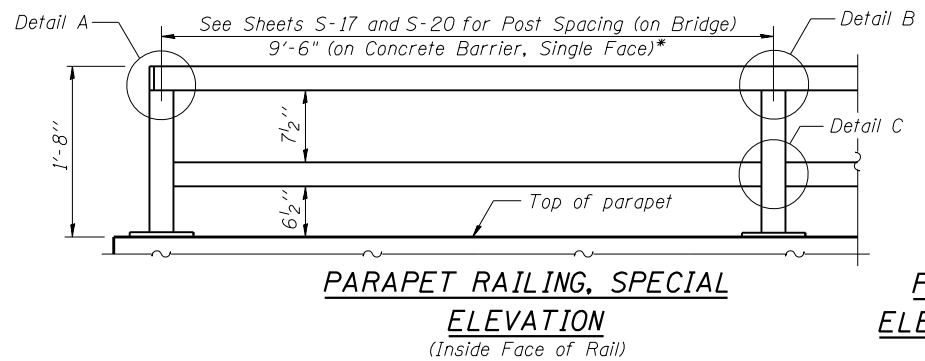
BRIDGE FENCE RAILING (SPECIAL)  
STRUCTURE NO. 022-0512  
SHEET NO. S-22 OF S-53 SHEETS

F.A.P. RTE. 311	SECTION 652-A	COUNTY DuPAGE	TOTAL SHEETS 383	SHEET NO. 185
CONTRACT NO. 60R06			ILLINOIS FED. AID PROJECT	



All steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.

All steel rail elements shall be finished with Black Polyester Powder Coating over galvanized steel. See Special Provisions.



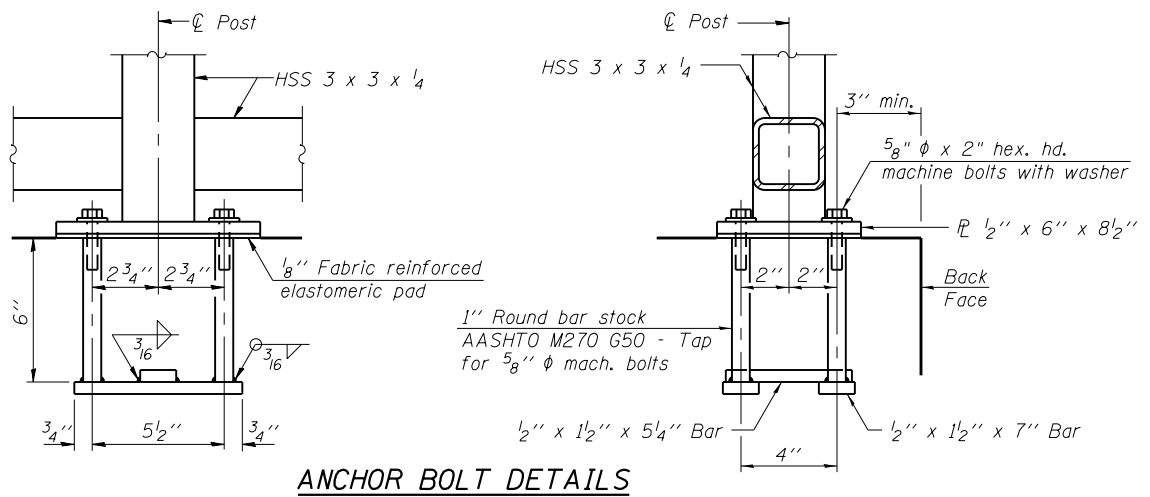
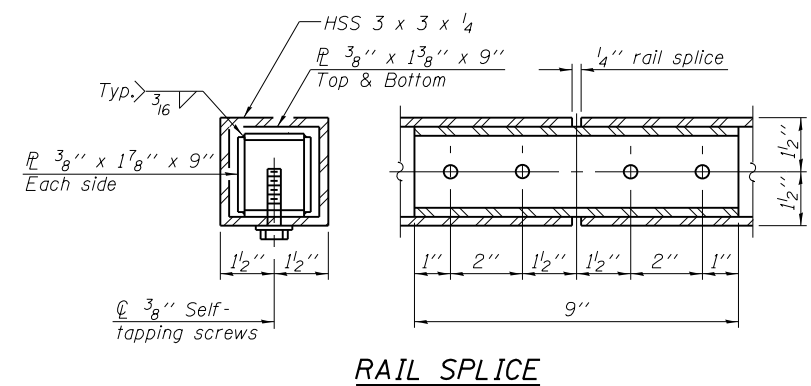
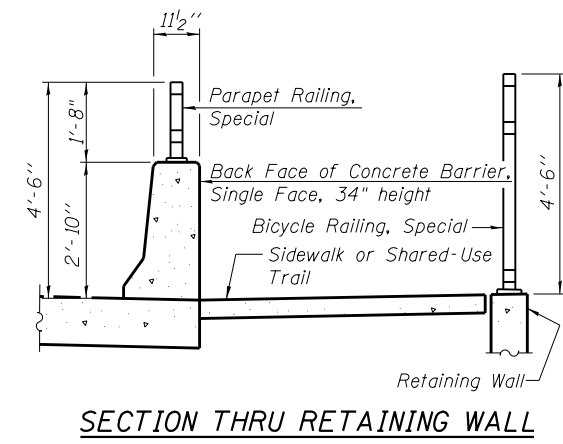
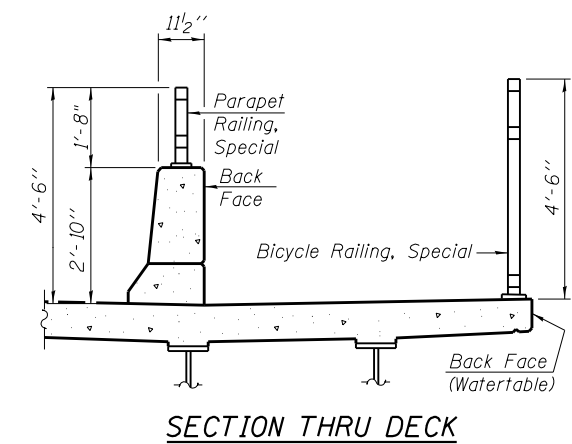
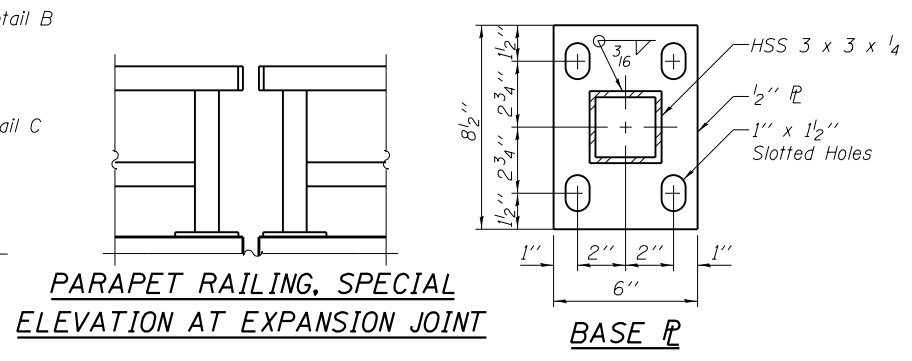
\*End Panel Length

7'-1" (NW)

3'-7" (SW)

8'-9" (NE)

8'-6" (SE)



In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting 5/8" φ anchor rods according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications.

**BILL OF MATERIAL**

Item	Unit	Quantity
Bicycle Railing, Special	Foot	2,854
Parapet Railing, Special	Foot	2,979



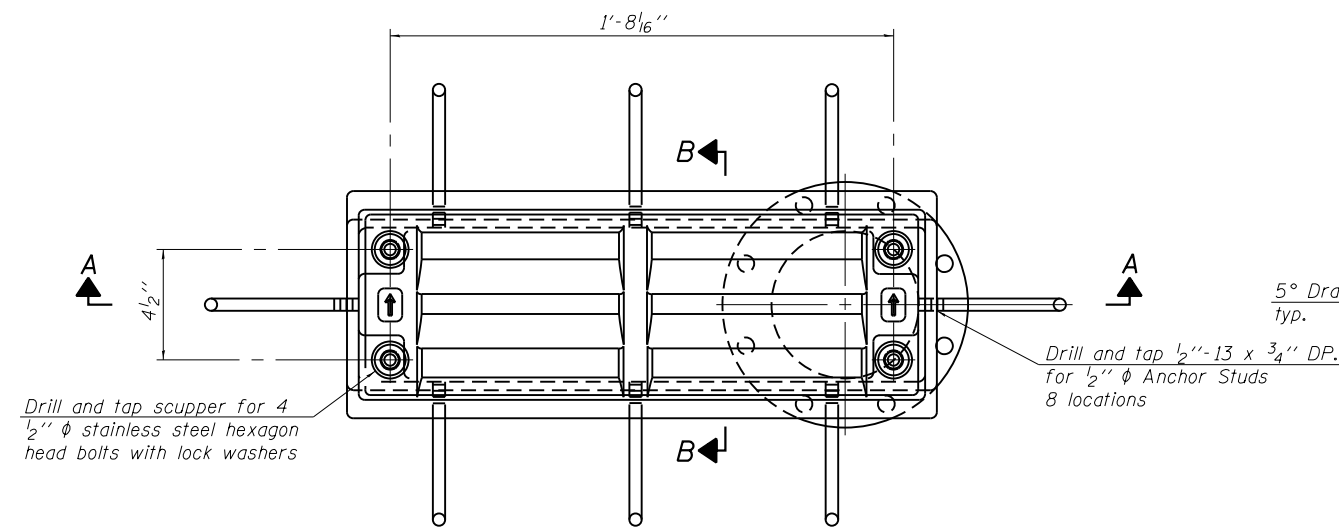
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PLOT DATE = 03/19/2014	CHECKED - AMK	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

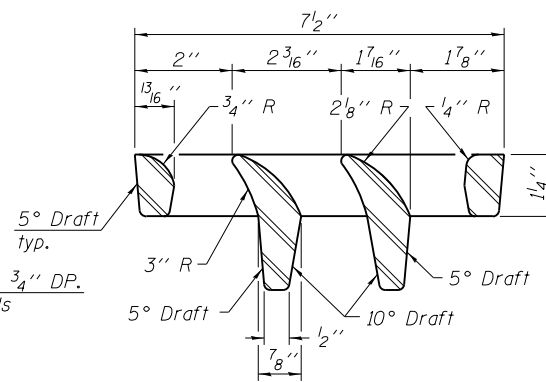
BICYCLE RAILING  
STRUCTURE NO. 022-0512

SHEET NO. S-23 OF S-53 SHEETS

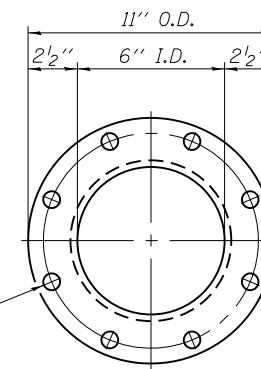
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DuPAGE	383	186
CONTRACT NO. 60R06				
ILLINOIS FED. AID PROJECT				



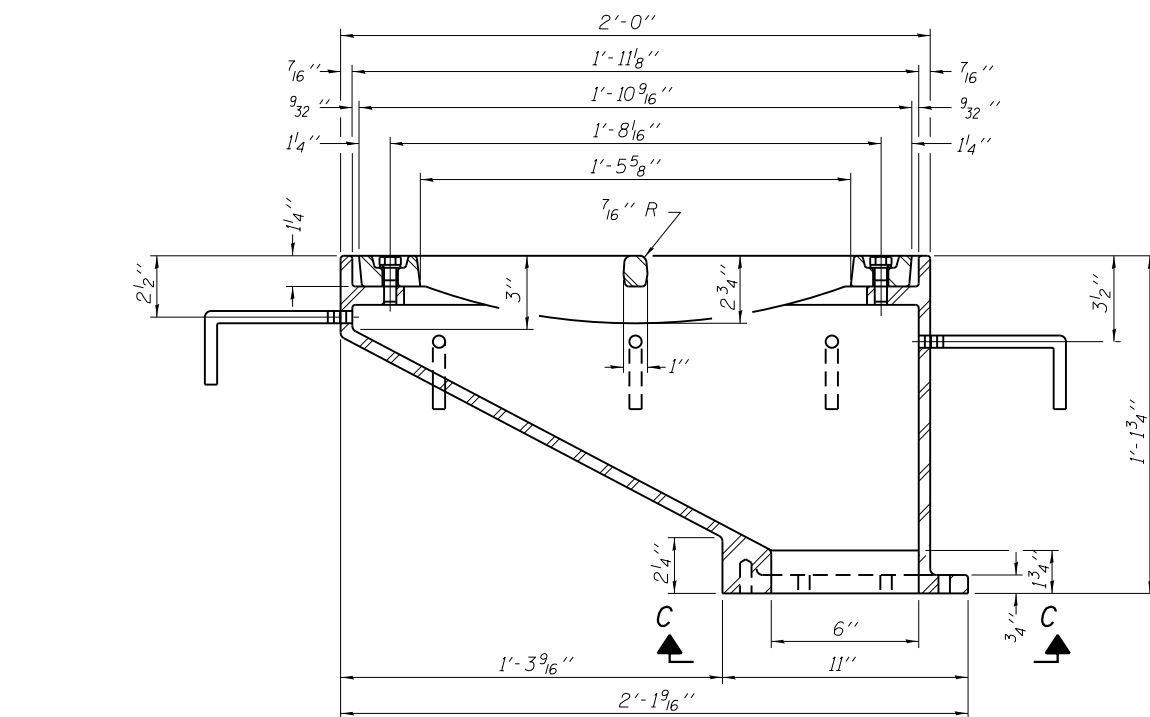
PLAN



VANE GRATE DETAIL

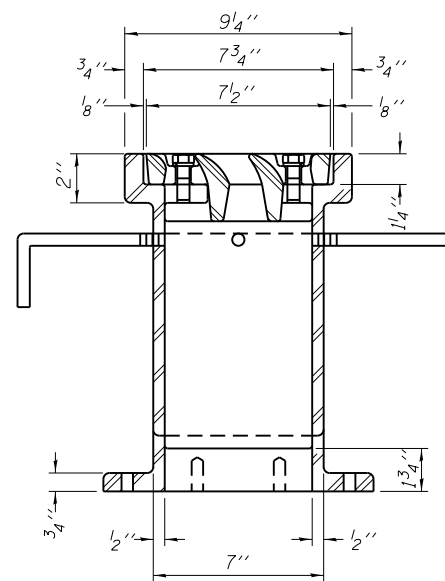


VIEW C-C

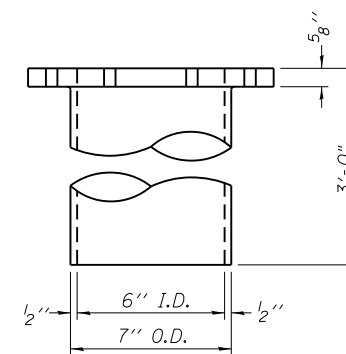


SECTION A-A

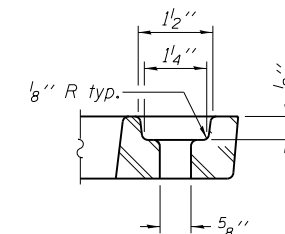
See Sheet S-25 for scupper location relative to parapet.



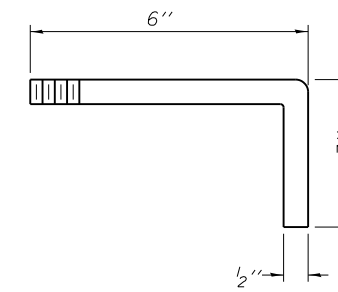
SECTION B-B



DOWNSPOUT



BOLT HOLE DETAIL



ANCHOR STUD DETAIL

Drill and tap 8 holes for 1/2"-13 bolts on a 9 1/2" φ bolt circle. (2 blind holes are 1/4" deep, 6 thru holes)

Notes:

All cast iron parts shall be gray iron conforming to the requirements of AASHTO M 105, Class 35B.

Bolts, anchor studs, washers and nuts shall conform to the requirements of ASTM A 307 and shall be galvanized according to AASHTO M 232.

Downspouts located on the exterior side of a painted steel fascia beam shall be painted with the finish coat specified for the exterior side of the fascia beam.

As an alternate, bolts, anchor studs, washers and nuts may be stainless steel according to Article 1006.29(d) of the Standard Specifications.

Structural steel weldments of equal sections and of the same configuration may be substituted for the cast iron scupper frame. Fillet or full penetration welds shall be used for the weldments. Details shall be submitted to the Engineer for approval. Structural steel weldments shall not be substituted for the cast iron scupper grate. Structural steel frames and downspouts shall be galvanized according to AASHTO M111.

The Contractor shall take appropriate measures to assure that Protective Coat is not applied to the scupper.

Cost of the Grate, Frame, Downspout, Anchor Studs, Bolts, Washers and Nuts including complete installation of the scupper shall be paid for at the contract unit price each for Drainage Scupper, DS-12.

Alternate fiberglass downspout conforming to ASTM D 2996 with a short-time rupture strength hoop tensile stress of 30,000 psi min. may be used in lieu of the cast iron or steel equivalent.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Drainage Scupper, DS-12	Each	6

DS-12

7-1-10



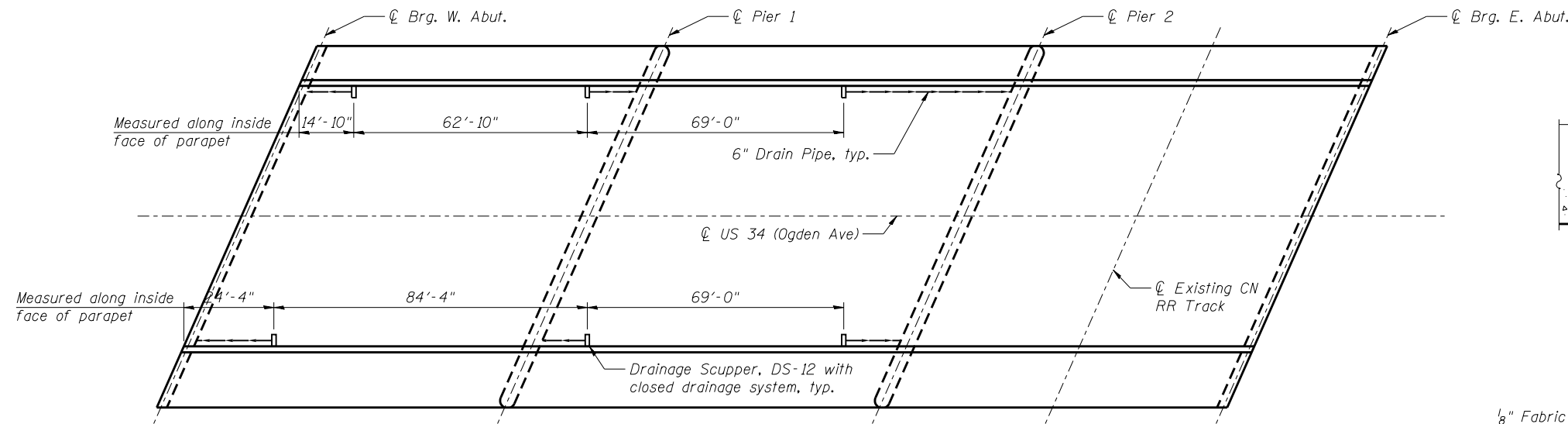
USER NAME =	DESIGNED - KMP	REVISED
	CHECKED - STB	REVISED
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PLOT DATE = 02/28/2014	CHECKED - STB	REVISED

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DEPARTMENT OF TRANSPORTATION

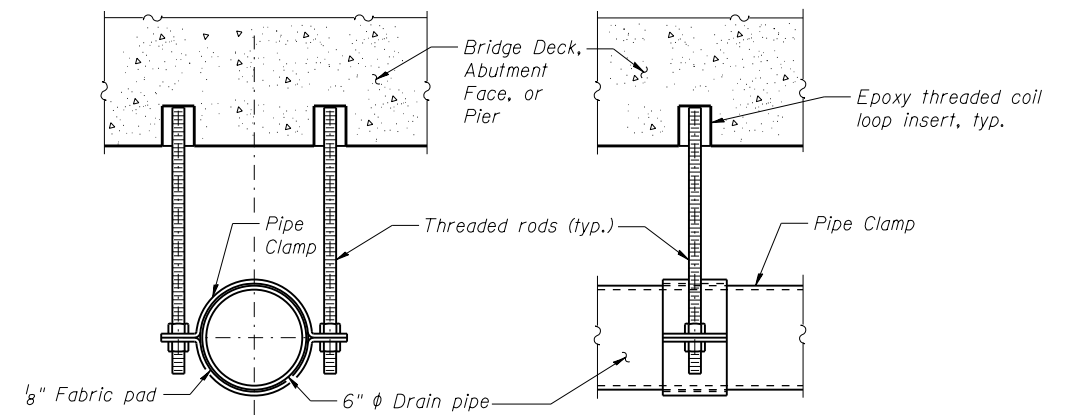
DRAINAGE SCUPPER, DS-12  
STRUCTURE NO. 022-0512

SHEET NO. S-24 OF S-53 SHEETS

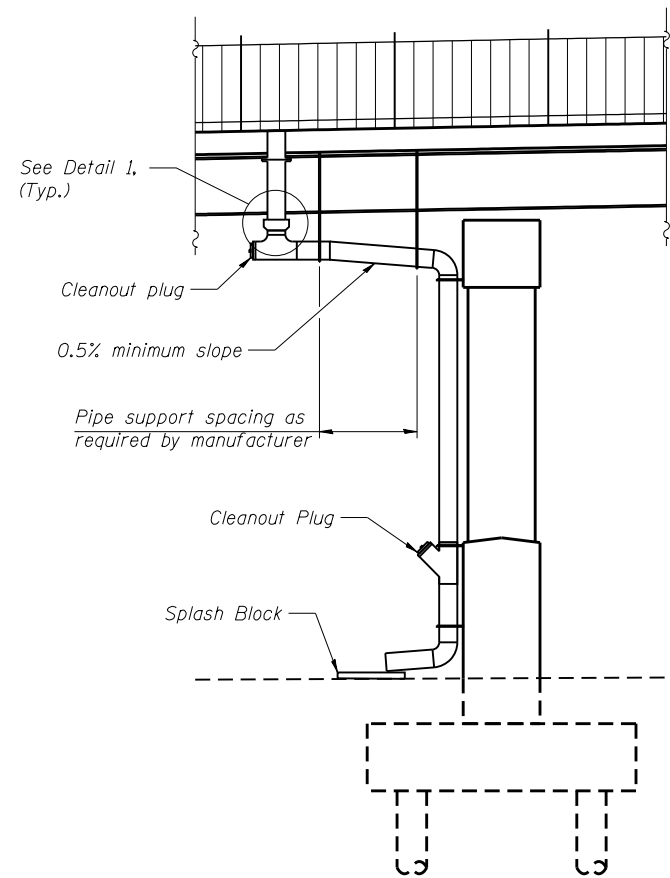
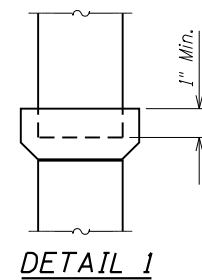
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DuPAGE	383	187
CONTRACT NO. 60R06			ILLINOIS FED. AID PROJECT	



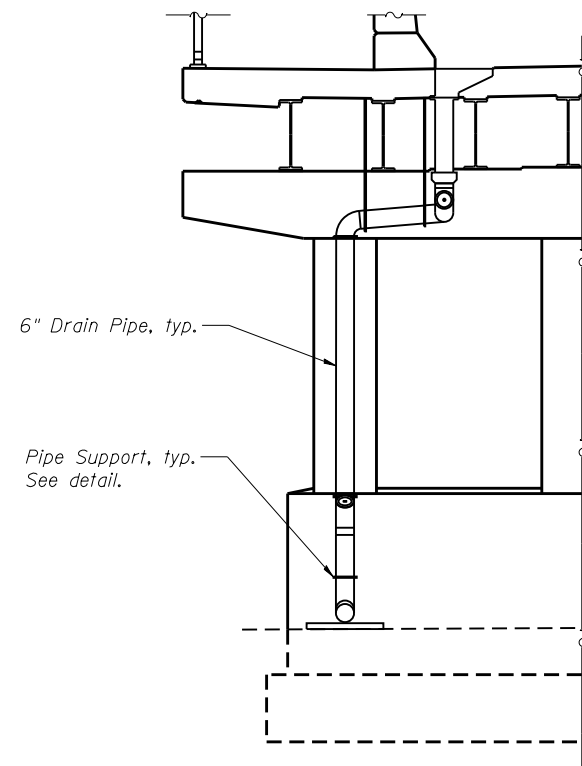
**DECK DRAINAGE PLAN**



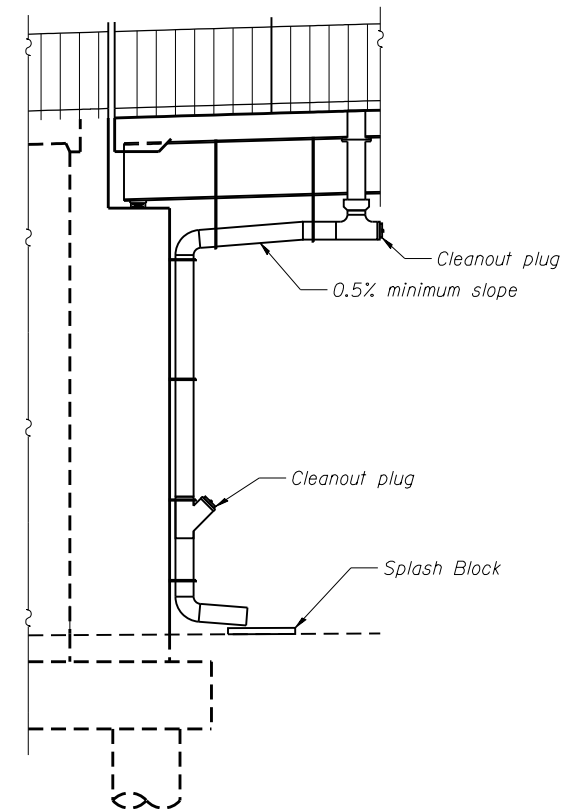
**PIPE SUPPORT DETAIL**



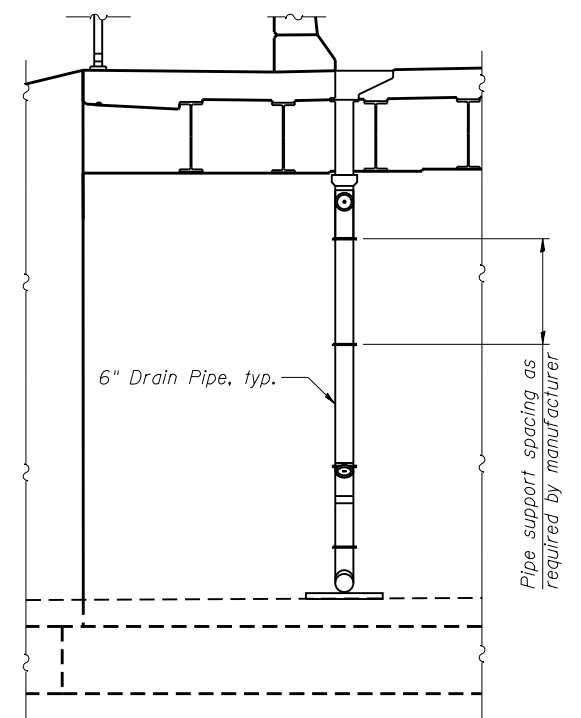
**TYPICAL ELEVATION AT PIER**  
(Looking North or South)



**TYPICAL ELEVATION AT PIER**  
(Looking East or West)



**TYPICAL ELEVATION AT ABUTMENT**  
(Looking North)



**TYPICAL ELEVATION AT ABUTMENT**  
(Looking West)



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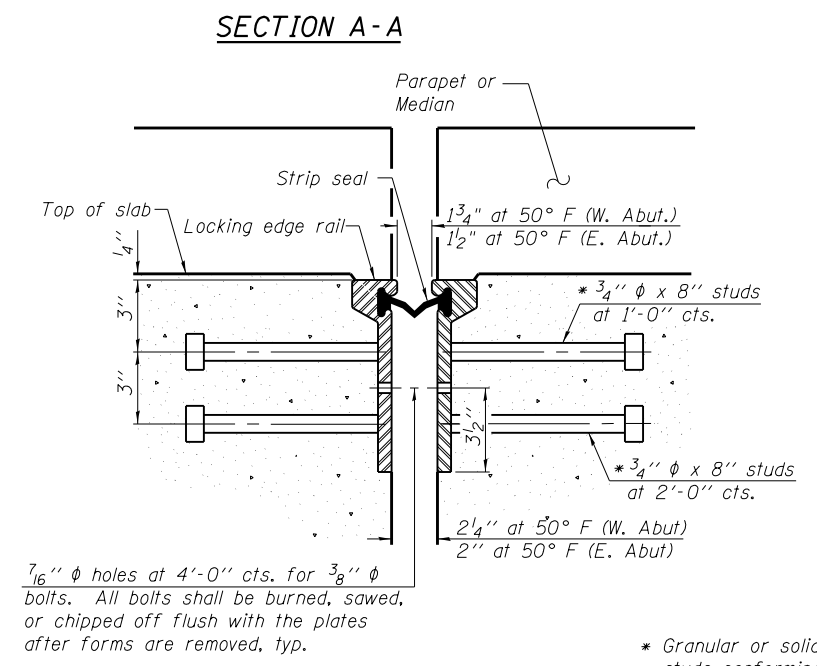
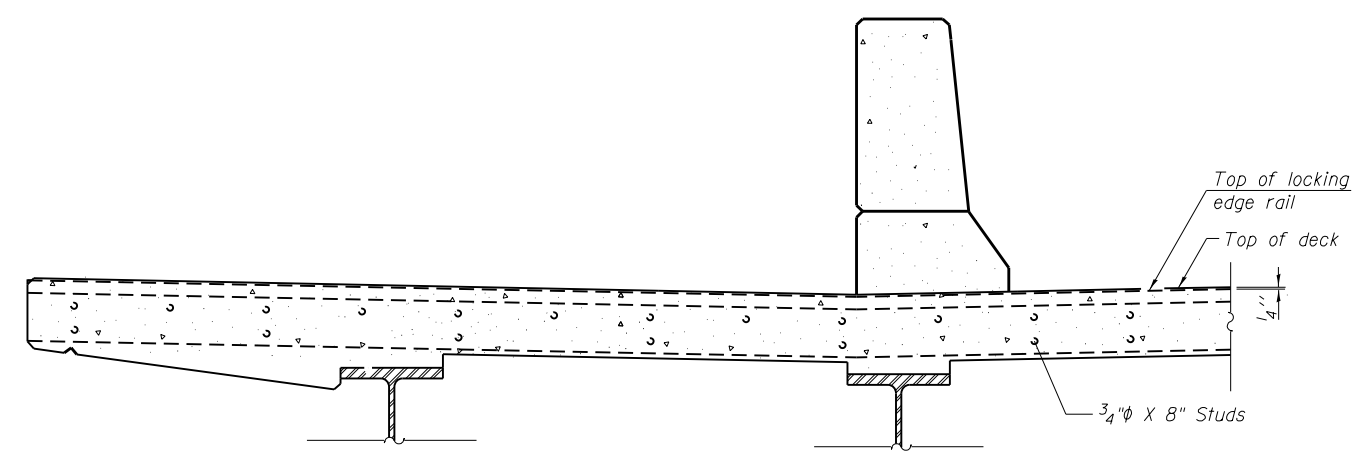
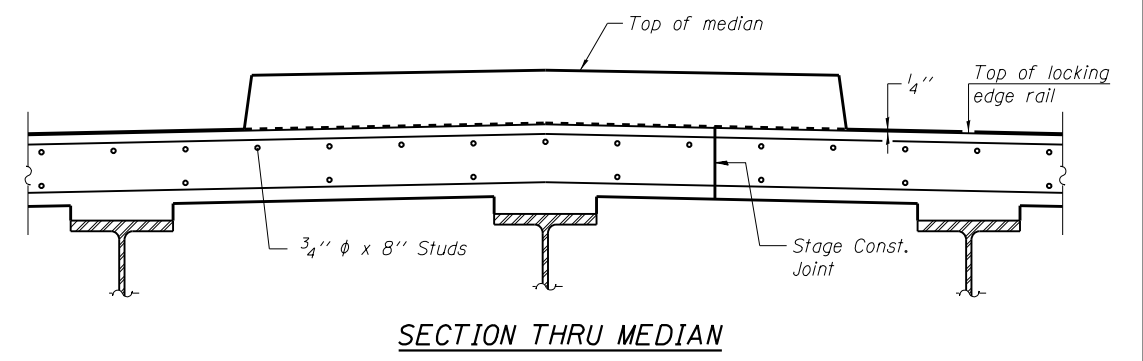
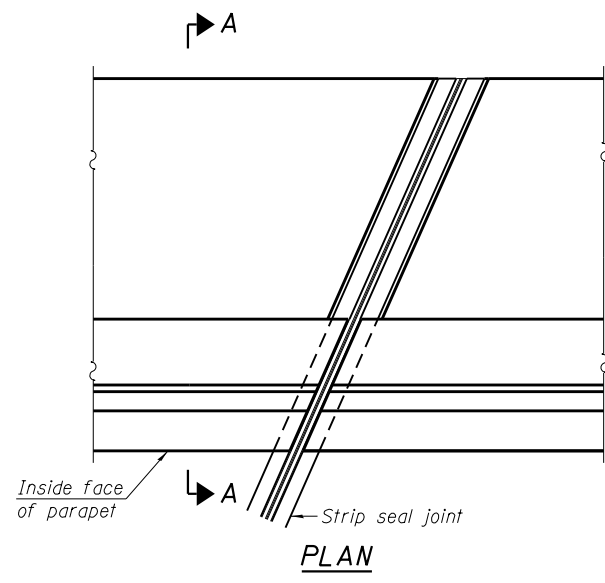
**DRAINAGE SYSTEM  
STRUCTURE NO. 022-0512**

SHEET NO. S-25 OF S-53 SHEETS

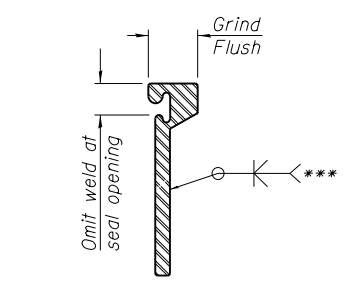
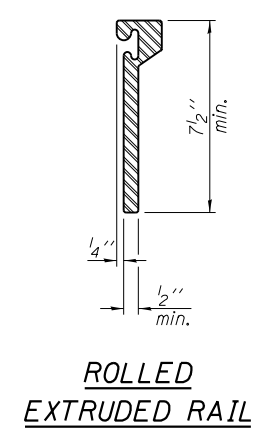
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 60R06				

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\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



\*\*\* Back gouge not required if complete joint penetration is verified by mock-up.

The inside of the locking edge rail groove shall be free of weld residue.  
Rolled rail shown, welded rail similar.

Notes:  
The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.

The manufacturer's recommended installation methods shall be followed.

The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

Maximum space between rail segments shall be 3/16", sealed with a suitable sealant. Joints in rails within 10 ft. of curbs shall be welded.

**BILL OF MATERIAL**

Item	Unit	Total
Preformed Joint Strip Seal	Foot	213



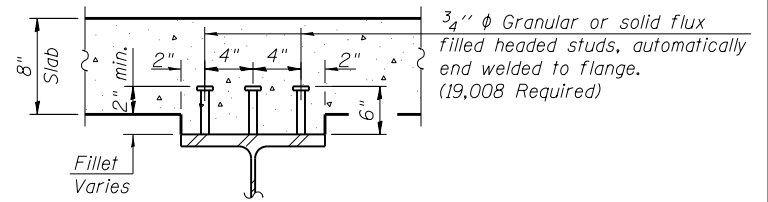
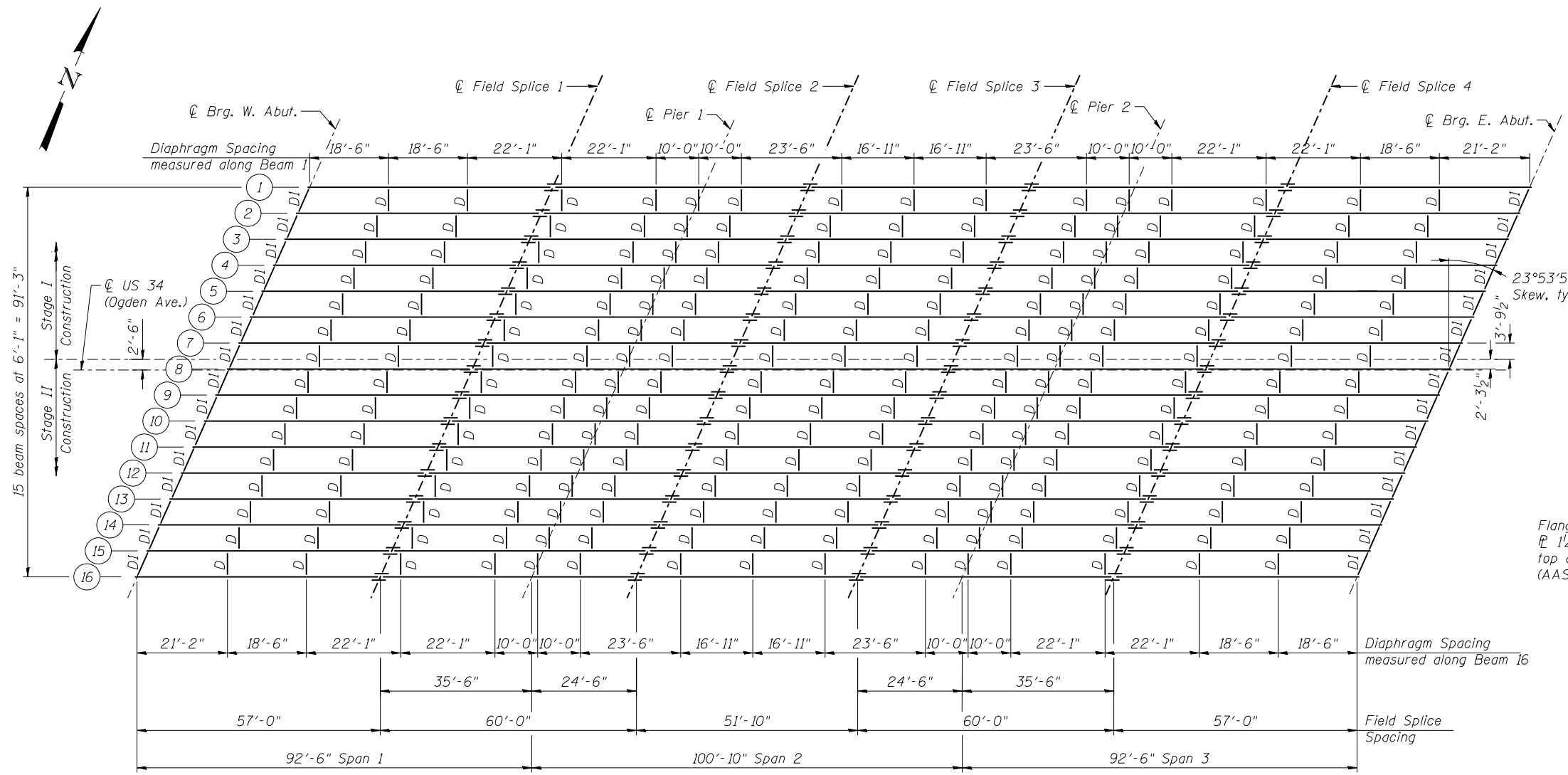
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PREFORMED JOINT STRIP SEAL  
STRUCTURE NO. 022-0512

SHEET NO. S-26 OF S-53 SHEETS

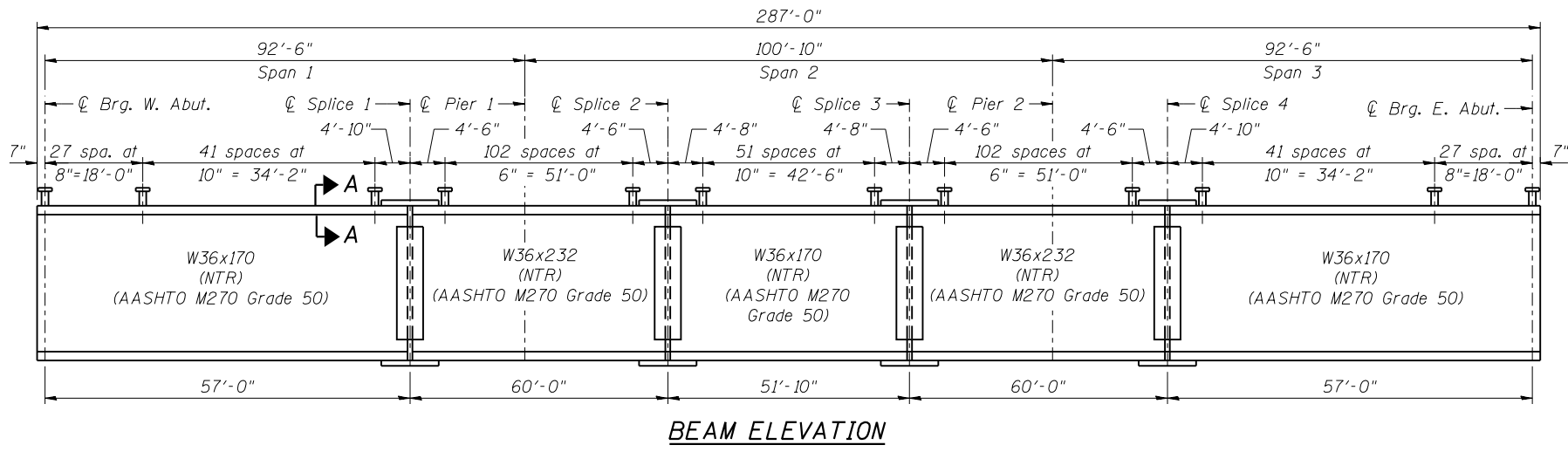
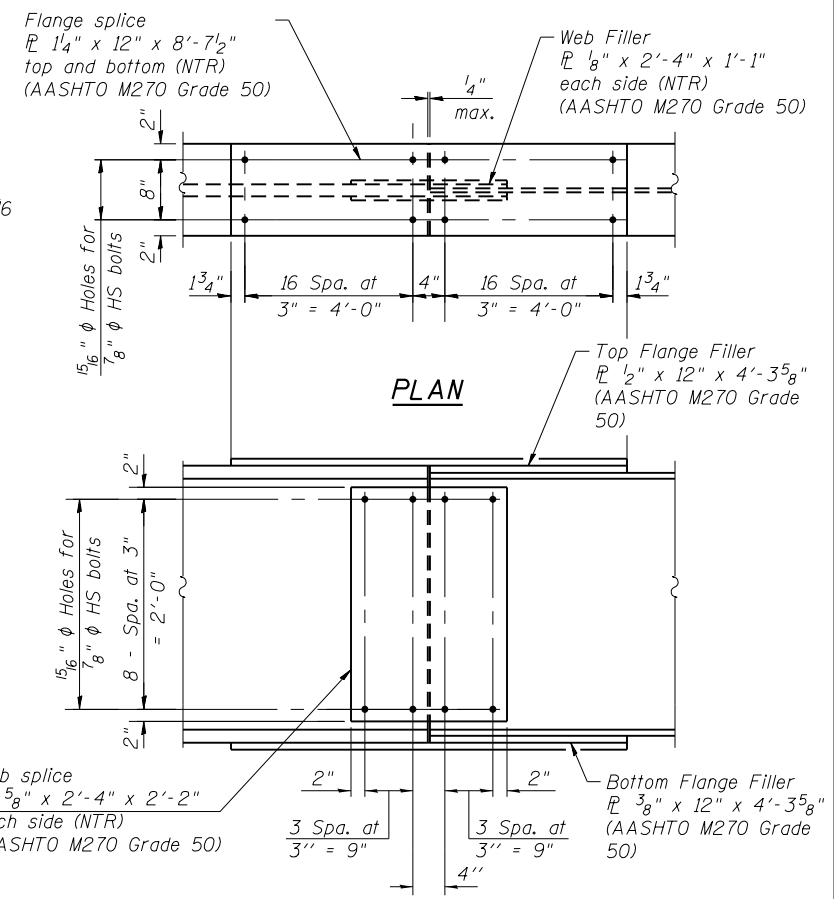
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DuPAGE	383	189
CONTRACT NO. 60R06				
ILLINOIS FED. AID PROJECT				



SECTION A-A

NOTES

1. See Sheet S-28 for diaphragm details, top of beam elevations, and moment and reaction tables.
2. For Bearing Details, see Sheets S-29 and S-30.
3. All cross frames or diaphragms shall be installed as steel is erected and secured with erection pins and bolts except as otherwise noted. Individual cross frames or diaphragms at supports may be temporarily disconnected to install bearing anchor rods.
4. Load carrying components designated "NTR" shall conform to the Impact Testing Requirement, Zone 2.



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FRAMING PLAN AND BEAM ELEVATION  
STRUCTURE NO. 022-0512

SHEET NO. S-27 OF S-53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DuPAGE	383	190
CONTRACT NO. 60R06			ILLINOIS FED. AID PROJECT	

INTERIOR BEAM MOMENT TABLE				
		0.4 Sp. 1 or 0.6 Sp. 3	Pier 1 or 2	0.5 Sp. 2
$I_s$	(in <sup>4</sup> )	10,500	15,000	10,500
$I_c(n)$	(in <sup>4</sup> )	25,334	33,211	25,334
$I_c(3n)$	(in <sup>4</sup> )	18,571	24,174	18,571
$I_c(cr)$	(in <sup>4</sup> )	-	17,920	-
$S_s$	(in <sup>3</sup> )	581	809	581
$S_c(n)$	(in <sup>3</sup> )	813	1,101	813
$S_c(3n)$	(in <sup>3</sup> )	735	989	735
$S_c(cr)$	(in <sup>3</sup> )	-	875	-
DC1	(k/')	0.824	0.885	0.824
$M_{DC1}$	(k)	480	-835	232
DC2	(k/')	0.25	0.25	0.25
$M_{DC2}$	(k)	145	-246	72
DW	(k/')	0.234	0.234	0.234
$M_{DW}$	(k)	135	-230	68
$M_L + IM$	(k)	1,243	-1,399	1,054
$M_u$ (Strength I)	(k)	3,159	-4,145	2,327
$\phi_r M_n$	(k)	4,010	4,503	4,010
$f_s$ DC1	(ksi)	9.9	-12.4	4.8
$f_s$ DC2	(ksi)	2.4	-3.4	1.2
$f_s$ DW	(ksi)	2.2	-3.2	1.1
$f_s$ ( $\phi + IM$ )	(ksi)	18.3	-19.2	15.6
$f_s$ (Service II)	(ksi)	39.3	-44.8	28.1
$0.95R_n F_y f$	(ksi)	47.5	47.5	47.5
$f_s$ (Total)(Strength I)	(ksi)	-	-	-
$\phi_r F_n$	(ksi)	-	-	-
$V_r$	(k)	27.5	29.3	28.6

INTERIOR BEAM REACTION TABLE					
	W. Abut.	Pier 1	Pier 2	E. Abut.	
$R_{DC1}$	(k)	30.20	91.84	91.84	30.20
$R_{DC2}$	(k)	8.91	26.82	26.82	8.91
$R_{DW}$	(k)	8.34	25.11	25.11	8.34
$R_L + IM$	(k)	90.01	169.28	169.28	90.01
$R_{Total}$	(k)	137.46	313.05	313.05	137.46

TOP OF BEAM ELEVATIONS (FOR FABRICATION ONLY)								
Beam No.	℄ Brg. W. Abut.	℄ Splice 1	℄ Pier 1	℄ Splice 2	℄ Splice 3	℄ Pier 2	℄ Splice 4	℄ Brg. E. Abut.
1	735.89	737.19	737.60	737.89	738.29	738.38	738.51	738.10
2	735.76	737.06	737.50	737.80	738.22	738.31	738.44	738.06
3	735.81	737.13	737.58	737.89	738.33	738.43	738.57	738.21
4	735.87	737.21	737.67	737.99	738.44	738.55	738.70	738.36
5	735.93	737.28	737.76	738.08	738.55	738.66	738.83	738.50
6	735.98	737.36	737.84	738.17	738.66	738.78	738.96	738.65
7	736.04	737.43	737.93	738.27	738.77	738.90	739.09	738.80
8	736.09	737.50	738.01	738.36	738.88	739.02	739.22	738.95
9	735.90	737.33	737.85	738.21	738.74	738.89	739.10	738.85
10	735.70	737.15	737.68	738.04	738.59	738.75	738.98	738.74
11	735.50	736.96	737.50	737.88	738.45	738.61	738.85	738.63
12	735.29	736.78	737.33	737.71	738.30	738.47	738.72	738.52
13	735.09	736.59	737.16	737.55	738.15	738.33	738.59	738.41
14	734.88	736.41	736.98	737.38	738.00	738.19	738.46	738.30
15	734.86	736.40	736.99	737.39	738.03	738.22	738.51	738.36
16	734.84	736.42	737.01	737.42	738.07	738.28	738.58	738.44

$I_s, S_s$ : Non-composite moment of inertia and section modulus of the steel section used for computing  $f_s$  (Total-Strength I, and Service II) due to non-composite dead loads (in<sup>4</sup> and in<sup>3</sup>).

$I_c(n), S_c(n)$ : Composite moment of inertia and section modulus of the steel and deck based upon the modular ratio, "n", used for computing  $f_s$  (Total-Strength I, and Service II) in uncracked sections due to short-term composite live loads (in<sup>4</sup> and in<sup>3</sup>).

$I_c(3n), S_c(3n)$ : Composite moment of inertia and section modulus of the steel and deck based upon 3 times the modular ratio, "3n", used for computing  $f_s$  (Total-Strength I, and Service II) in uncracked sections, due to long-term composite (superimposed) dead loads (in<sup>4</sup> and in<sup>3</sup>).

$I_c(cr), S_c(cr)$ : Composite moment of inertia and section modulus of the steel and longitudinal deck reinforcement, used for computing  $f_s$  (Total-Strength I and Service II) in cracked sections, due to both short-term composite live loads and long-term composite (superimposed) dead loads (in<sup>4</sup> and in<sup>3</sup>).

DC1: Un-factored non-composite dead load (kips/ft.).

$M_{DC1}$ : Un-factored moment due to non-composite dead load (kip-ft.).

DC2: Un-factored long-term composite (superimposed excluding future wearing surface) dead load (kips/ft.).

$M_{DC2}$ : Un-factored moment due to long-term composite (superimposed excluding future wearing surface) dead load (kip-ft.).

DW: Un-factored long-term composite (superimposed future wearing surface only) dead load (kips/ft.).

$M_{DW}$ : Un-factored moment due to long-term composite (superimposed future wearing surface only) dead load (kip-ft.).

$M_L + IM$ : Un-factored live load moment plus dynamic load allowance (impact) (kip-ft.).

$M_u$  (Strength I): Factored design moment (kip-ft.).

$1.25 (M_{DC1} + M_{DC2}) + 1.5 M_{DW} + 1.75 M_L + IM$

$\phi_r M_n$ : Compact composite positive moment capacity computed according to Article 6.10.7.1 or non-slender negative moment capacity according to Article A6.1.1 or A6.1.2 (kip-ft.).

$f_s$  DC1: Un-factored stress at edge of flange for controlling steel flange due to vertical non-composite dead loads as calculated below (ksi).

$M_{DC1} / S_{nc}$

$f_s$  DC2: Un-factored stress at edge of flange for controlling steel flange due to vertical composite dead loads as calculated below (ksi).

$M_{DC2} / S_c(3n)$  or  $M_{DC2} / S_c(cr)$  as applicable.

$f_s$  DW: Un-factored stress at edge of flange for controlling steel flange due to vertical composite future wearing surface loads as calculated below (ksi).

$M_{DW} / S_c(3n)$  or  $M_{DW} / S_c(cr)$  as applicable.

$f_s$  ( $\phi + IM$ ): Un-factored stress at edge of flange for controlling steel flange due to vertical composite live load plus impact loads as calculated below (ksi).

$M_L + IM / S_c(n)$  or  $M_{DW} / S_c(cr)$  as applicable.

$f_s$  (Service II): Sum of stresses as computed below (ksi).

$f_{sDC1} + f_{sDC2} + f_{sDW} + 1.3 f_s (\phi + IM)$

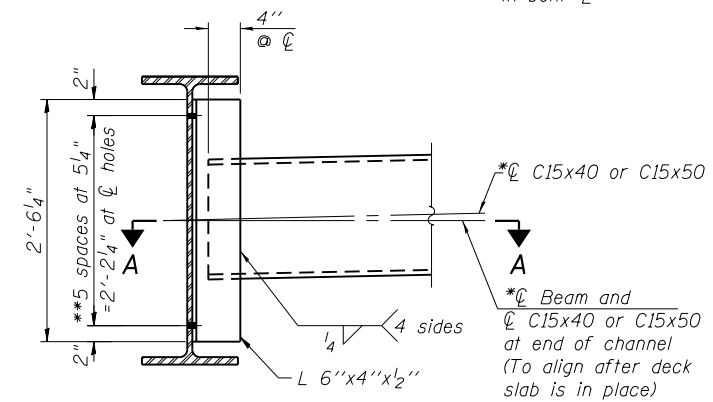
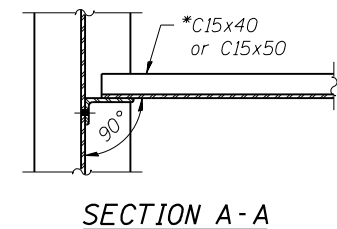
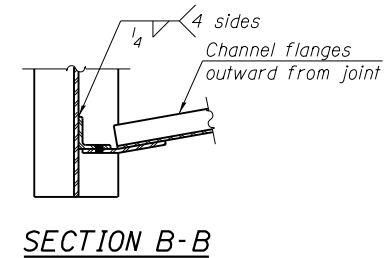
$0.95R_n F_y f$ : Composite stress capacity for Service II loading according to Article 6.10.4.2 (ksi).

$f_s$  (Total)(Strength I): Sum of stresses as computed below on non-compact section (ksi).

$1.25 (f_{sDC1} + f_{sDC2}) + 1.5 f_{sDW} + 1.75 f_s (\phi + IM)$

$\phi_r F_n$ : Non-Compact composite positive or negative stress capacity for Strength I loading according to Article 6.10.7 or 6.10.8 (ksi).

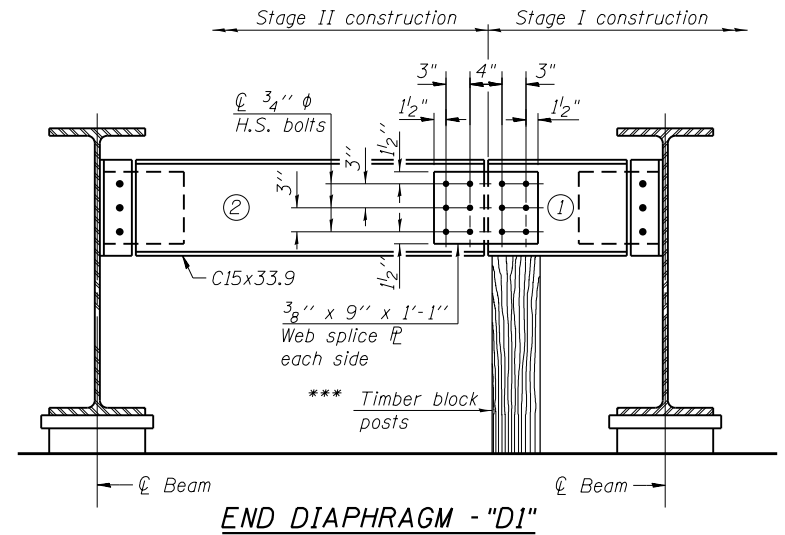
$V_r$ : Maximum factored shear range in span computed according to Article 6.10.10.



**INTERIOR DIAPHRAGM - "D"**

Note:  
Two hardened washers required for each set of oversized holes.  
Bolts for slotted holes shall be finger tightened prior to the deck slab pouring and then fully tightened after completion of the pour.  
Slotted holes shall be positioned so that bolts start at one end under no concrete load and finish near the opposite end under deck load.  
\*Alternate channels are permitted to facilitate material acquisition. Calculated weight of structural steel is based on the lighter section.  
The alternate, if utilized, shall be provided at no additional cost to the Department.  
\*\* $3/4$ "  $\phi$  HS bolts,  $15/16$ "  $\phi$  holes, u.n.o.

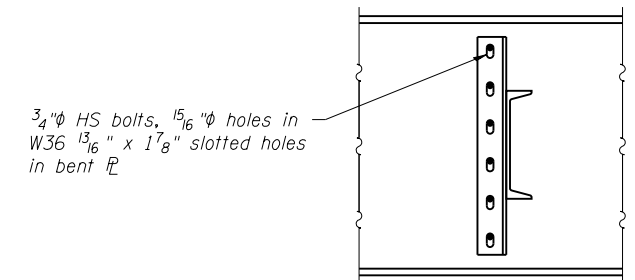
\*\*\* Cost of Timber Block Posts is included with Erecting Structural Steel.



**END DIAPHRAGM - "D1"**

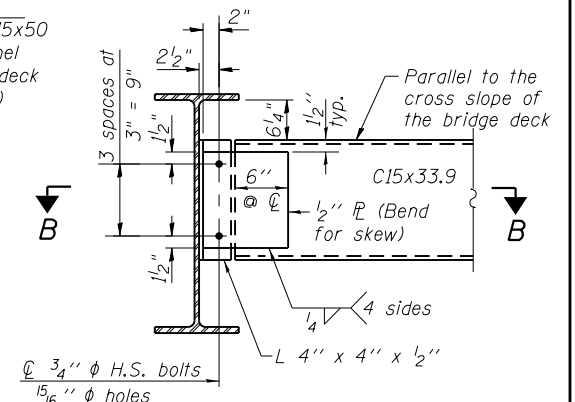
**END DIAPHRAGM STAGE CONSTRUCTION SEQUENCE**

- 1.) Order diaphragm in two sections.
- 2.) Attach section ① of diaphragm to beam 7.
- 3.) Place timber block posts between section ① of diaphragm and abutment bearing section.
- 4.) Attach section ② of diaphragm to both beam 8 and section ① of diaphragm during stage II construction with splice plates.
- 5.) Remove timber block posts.



**INTERIOR DIAPHRAGM AT STAGE CONSTRUCTION LINES**

At Beams 7 & 8  
(Not Required for diaphragms at ℄ Piers)  
 $5/16$ " plate washer required for each slotted hole



**END DIAPHRAGM - "D1"**

Note:  
Two hardened washers required for each set of oversized holes.



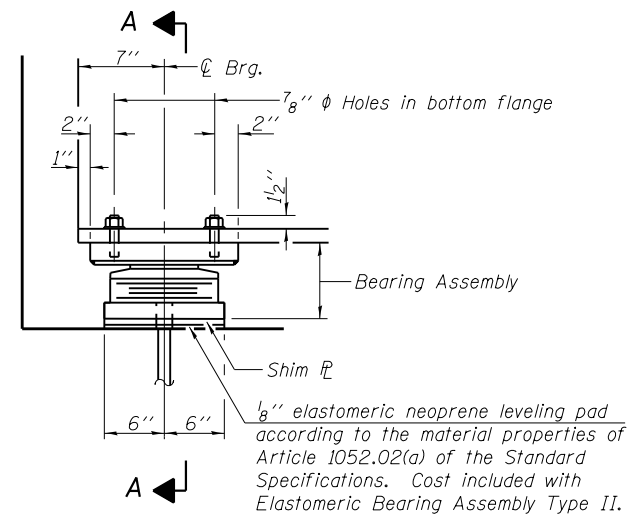
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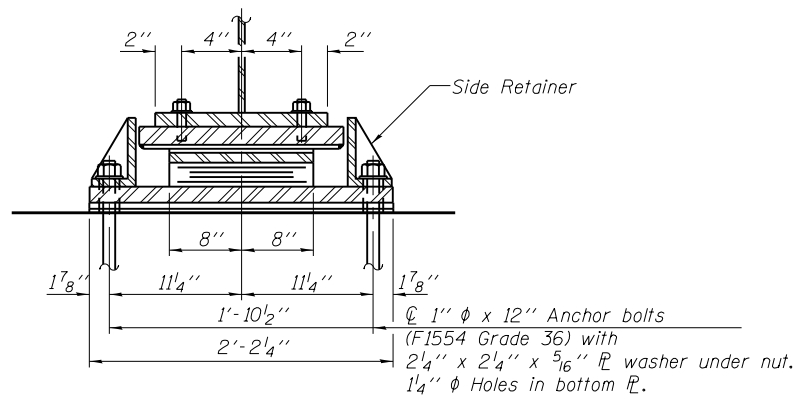
STEEL DETAILS  
STRUCTURE NO. 022-0512

SHEET NO. S-28 OF S-53 SHEETS

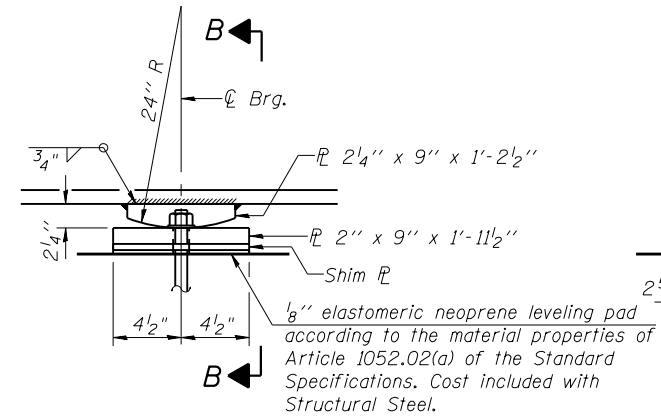
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311	652-A	DuPAGE	383	191
CONTRACT NO. 60R06			ILLINOIS FED. AID PROJECT	



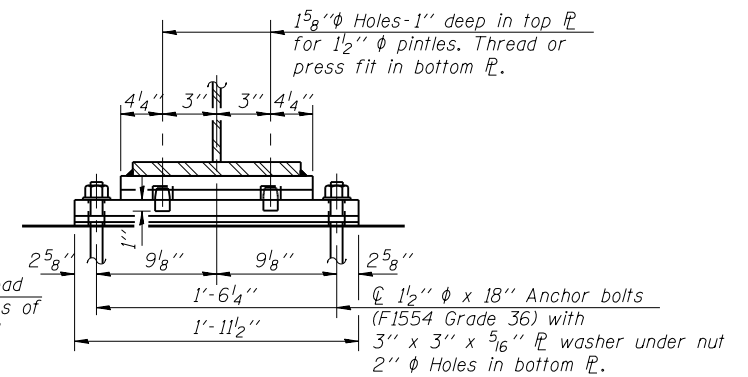
ELEVATION AT WEST ABUT.



SECTION A-A



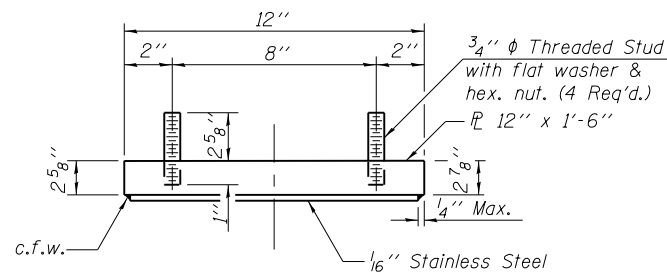
ELEVATION AT PIER 2



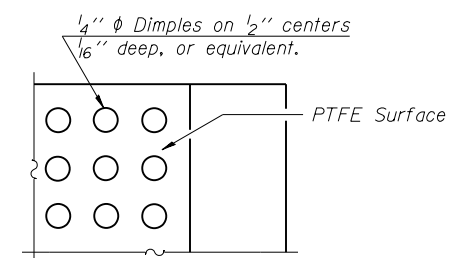
SECTION B-B

TYPE II ELASTOMERIC EXP. BRG.

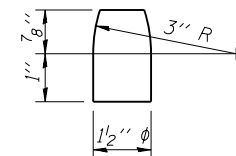
FIXED BEARING AT PIER 2



TOP BEARING ASSEMBLY

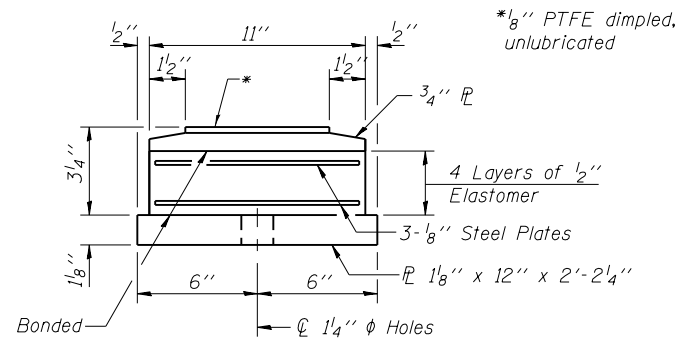


PLAN-PTFE SURFACE

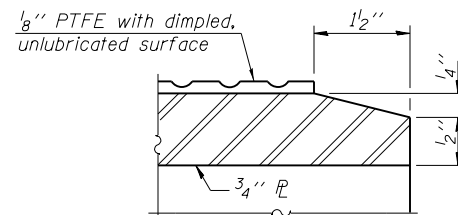


PINTLE

Notes:  
 Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.  
 Anchor bolts at fixed bearings may be either cast in place or installed in holes drilled after the supported member is in place.  
 Anchor bolts for Type II bearings shall be placed in holes drilled in the concrete through holes in the bottom bearing plate after members are in place. Side retainers shall be placed after bolts are installed.  
 Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.  
 Side retainers and other steel members required for the elastomeric bearing assembly shall be included in the cost of Elastomeric Bearing Assembly, Type II.  
 The 1/8" PTFE sheet shall be bonded directly to the top steel plate with a two-component, medium viscosity epoxy resin, conforming to the requirements of the Federal Specification MMM-A-134, Type I. The bond agent shall be applied on the full area of the contact surfaces.  
 Bonding of 1/8" PTFE sheet during vulcanizing process will be permitted provided the process and method of adjusting assembly height is approved by the Engineer.  
 Two 1/8 in. adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details.



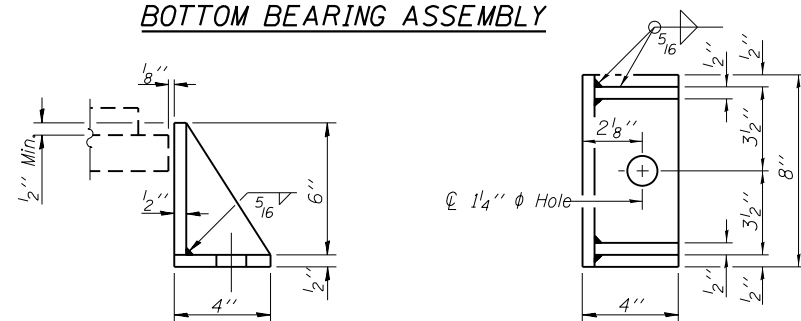
BOTTOM BEARING ASSEMBLY



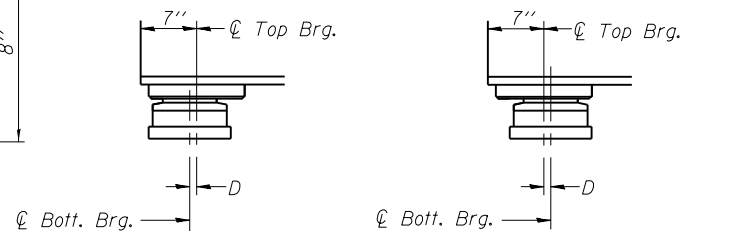
SECTION THRU PTFE

SHIM PLATE SCHEDULE

Beam	W. Abut.	Pier 2
1	1 1/2"	1"
2	-	-
3	3/4"	-
4	-	-
5	3/4"	-
6	-	-
7	5/8"	-
8	-	-
9	-	-
10	-	-
11	-	-
12	-	-
13	-	-
14	1/2"	-
15	1/8"	1/2"
16	-	1/8"



SIDE RETAINER  
 Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



BELOW 50°F. ABOVE 50°F.  
 (Move bott. brg. away from fixed brg.) (Move bott. brg. toward fixed brg.)

SETTING ANCHOR BOLTS AT EXP. BRG.

D=1/8" per each 100' of expansion for every 15° temp. change from the normal temp. of 50°F.

I-2E-2

1-27-12



USER NAME =	DESIGNED - KMP	REVISED
PLOT SCALE =	CHECKED - AMK	REVISED
PLOT DATE = 02/28/2014	DRAWN - BAR	REVISED
	CHECKED - KJZ	REVISED

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

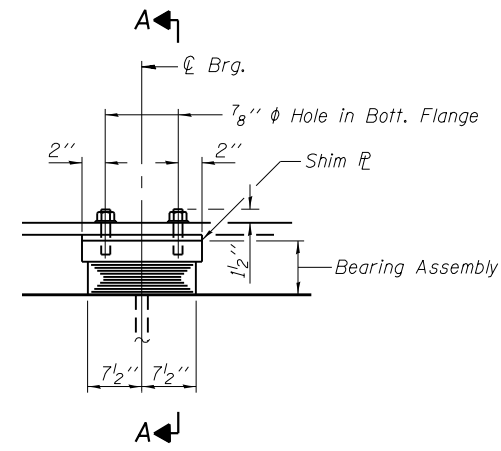
BEARING DETAILS 1  
 STRUCTURE NO. 022-0512

SHEET NO. S-29 OF S-53 SHEETS

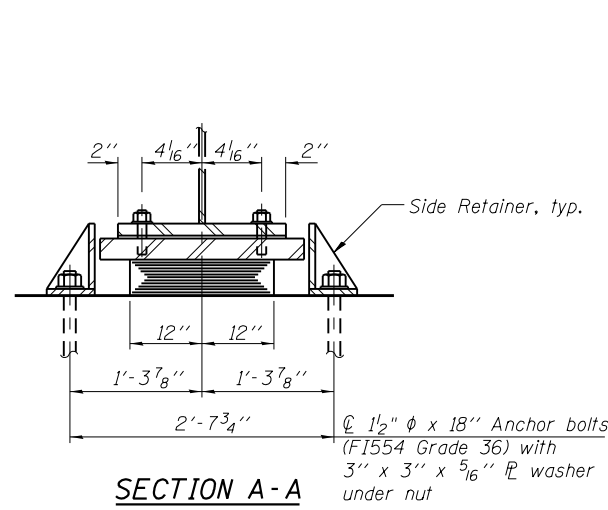
BILL OF MATERIAL

Item	Unit	Total
Erecting Elastomeric Bearing Assembly Type II	Each	16
Anchor Bolts, 1"	Each	32
Anchor Bolts, 1 1/2"	Each	32

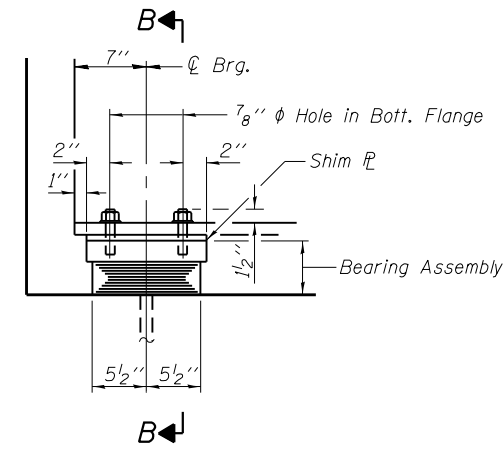
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	192
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60R06	



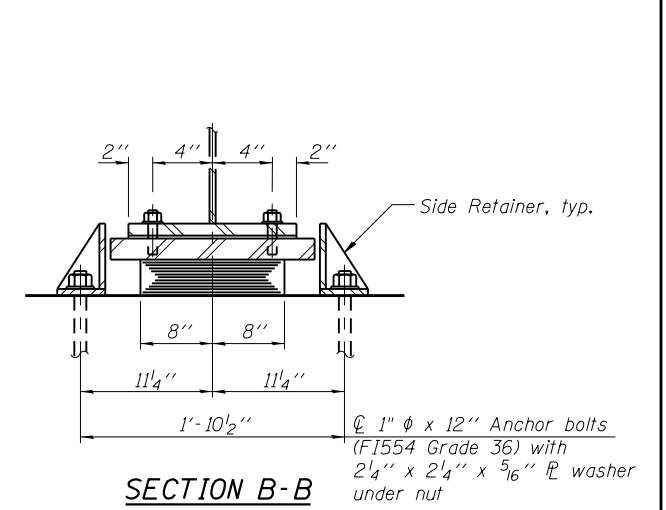
ELEVATION AT PIER 1



SECTION A-A



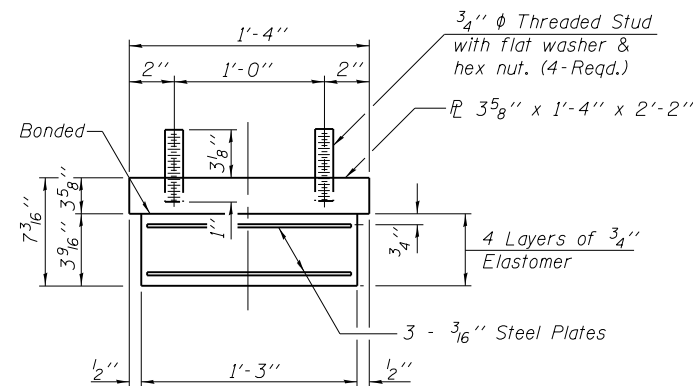
ELEVATION AT EAST ABUT.



SECTION B-B

TYPE I ELASTOMERIC EXP. BRG

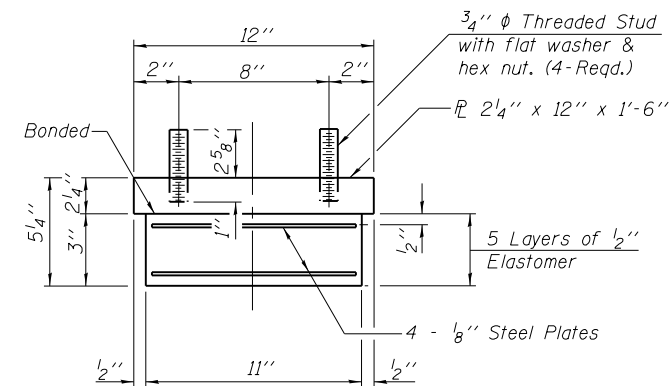
TYPE I ELASTOMERIC EXP. BRG.



BEARING ASSEMBLY

Note:  
Shim plates shall not be placed under Bearing Assembly.

Notes:  
Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.  
Anchor bolts for side retainers may be cast in place or installed in holes drilled before or after members are in place.  
Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.  
Side retainers and other steel members required for the elastomeric bearing assembly shall be included in the cost of Elastomeric Bearing Assembly, Type I.  
Two 1/8 inch adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details.

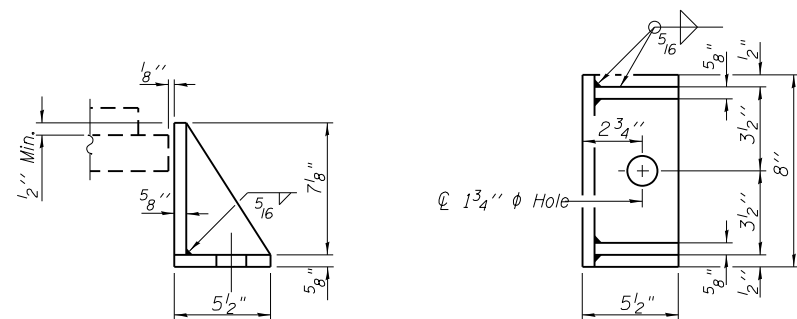


BEARING ASSEMBLY

Note:  
Shim plates shall not be placed under Bearing Assembly.

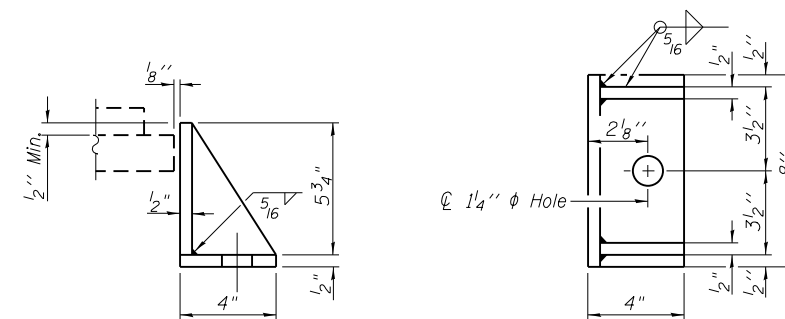
SHIM PLATE SCHEDULE

Beam	Pier 1	E. Abut.
1	1 1/4"	1/2"
2	-	-
3	-	-
4	-	-
5	-	-
6	-	-
7	-	-
8	-	-
9	-	-
10	-	-
11	-	-
12	-	-
13	-	-
14	-	-
15	1/8"	7/8"
16	3/8"	1 3/4"



SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

BILL OF MATERIAL

Item	Unit	Total
Erecting Elastomeric Bearing Assembly Type I	Each	32
Anchor Bolts, 1"	Each	32
Anchor Bolts, 1 1/2"	Each	32

I-2E-1

1-27-12



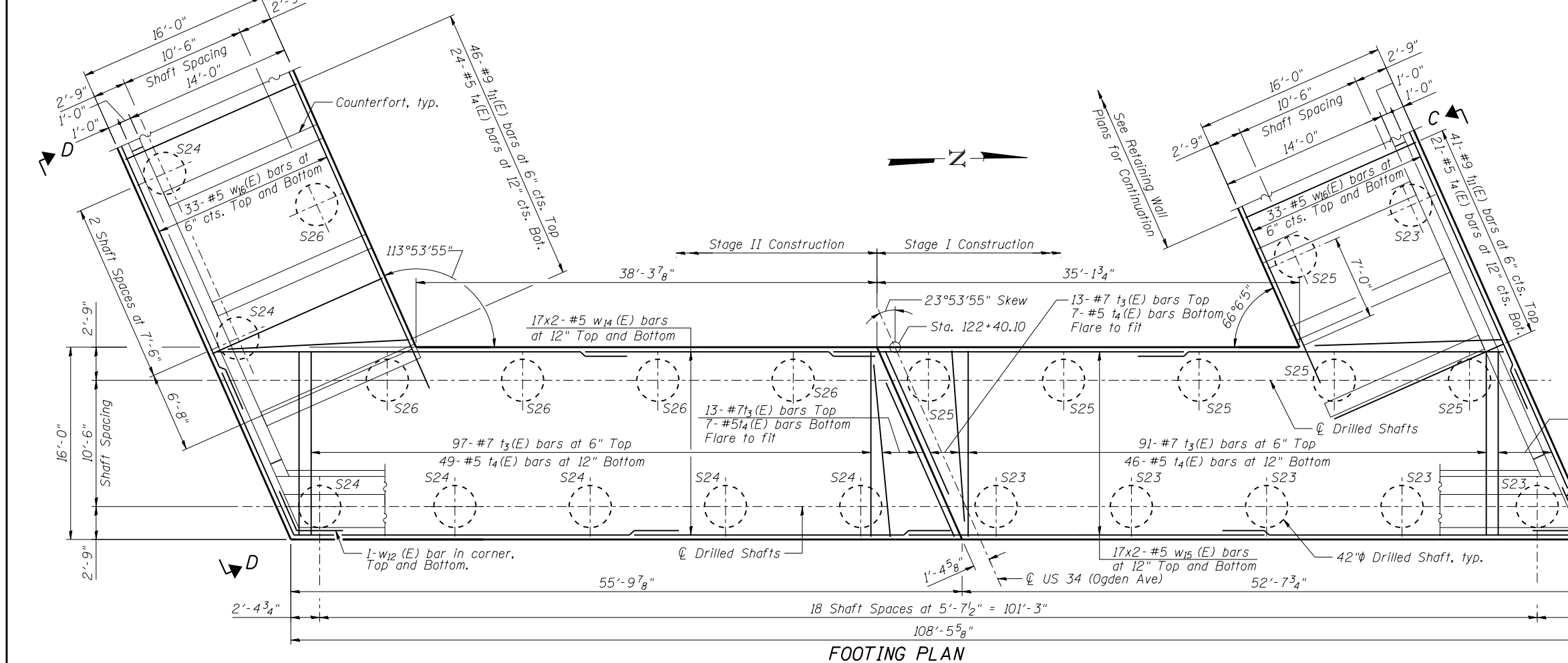
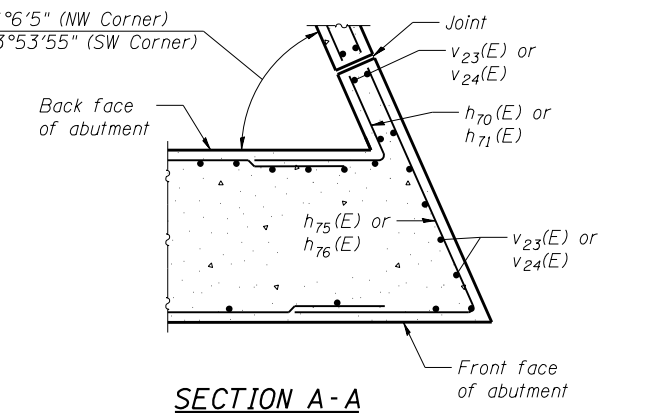
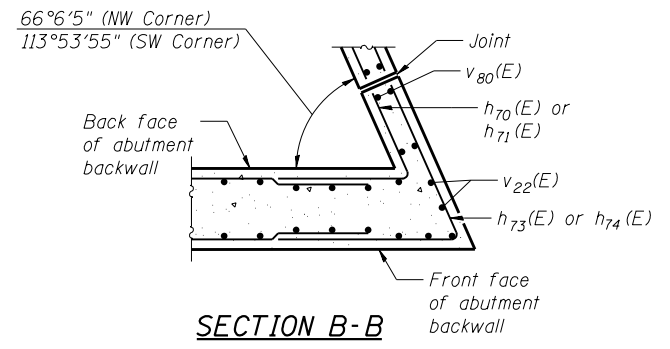
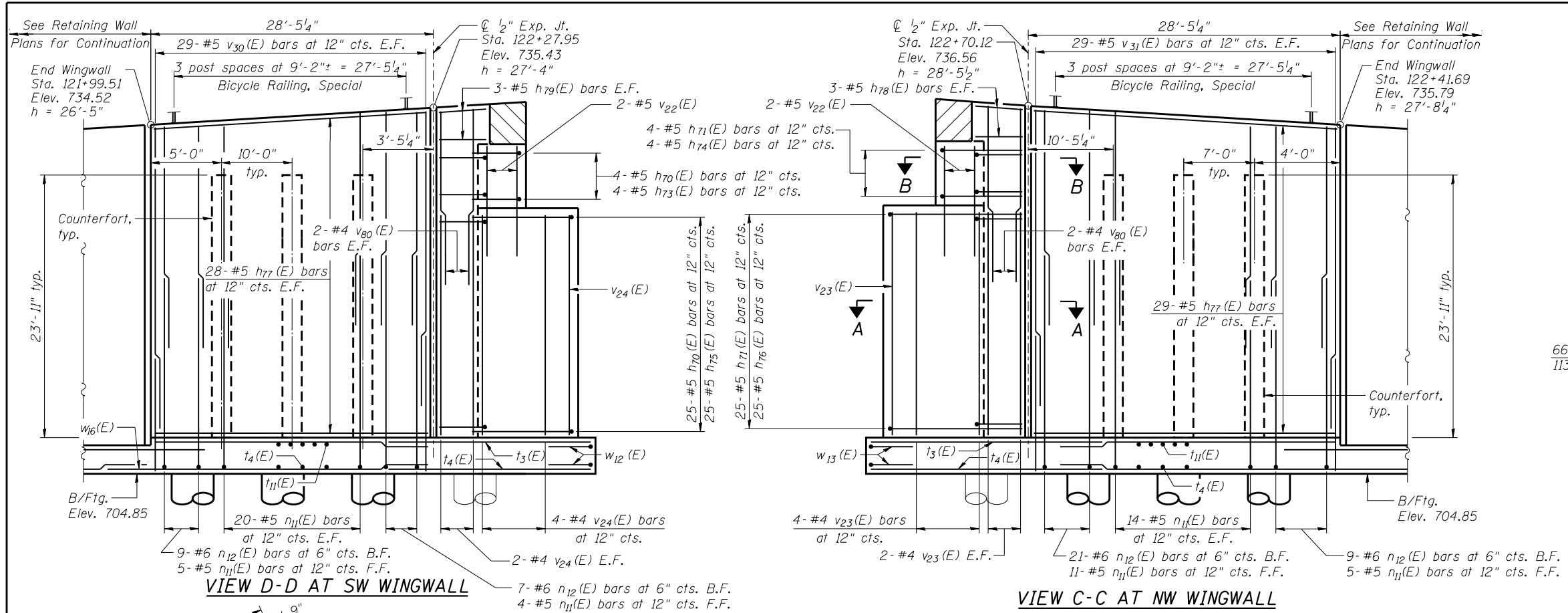
USER NAME =	DESIGNED - KMP	REVISED
PLOT SCALE =	CHECKED - AMK	REVISED
PLOT DATE = 03/19/2014	DRAWN - BAR	REVISED
	CHECKED - KJZ	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BEARING DETAILS 2  
STRUCTURE NO. 022-0512

SHEET NO. S-30 OF S-53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	193
			CONTRACT NO. 60R06	
ILLINOIS FED. AID PROJECT				



**MINIMUM BAR LAP**  
 #5 Bar = 2'-11" (Top Bar)

**NOTES**

1. Bars indicated 17x2- #5 etc. indicates 17 lines of bars with 2 lengths per line.
2. F.F. denotes Front Face. B.F. denotes Back Face. E.F. denotes Each Face.
3. For Bar List and Bill of Materials, see Sheet S-33.
4. For Section through wingwall see Sheet S-33.
5. Concrete Sealer shall be applied to the backwall, bearing seat, and front face of abutment.



USER NAME =	DESIGNED - KMP	REVISED
	CHECKED - STB	REVISED
PLOT SCALE =	DRAWN - KMP	REVISED
PLOT DATE = 02/28/2014	CHECKED - STB	REVISED

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

WEST ABUTMENT 1  
 STRUCTURE NO. 022-0512

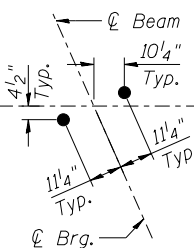
SHEET NO. S-31 OF S-53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DuPAGE	383	194
CONTRACT NO. 60R06				
ILLINOIS FED. AID PROJECT				

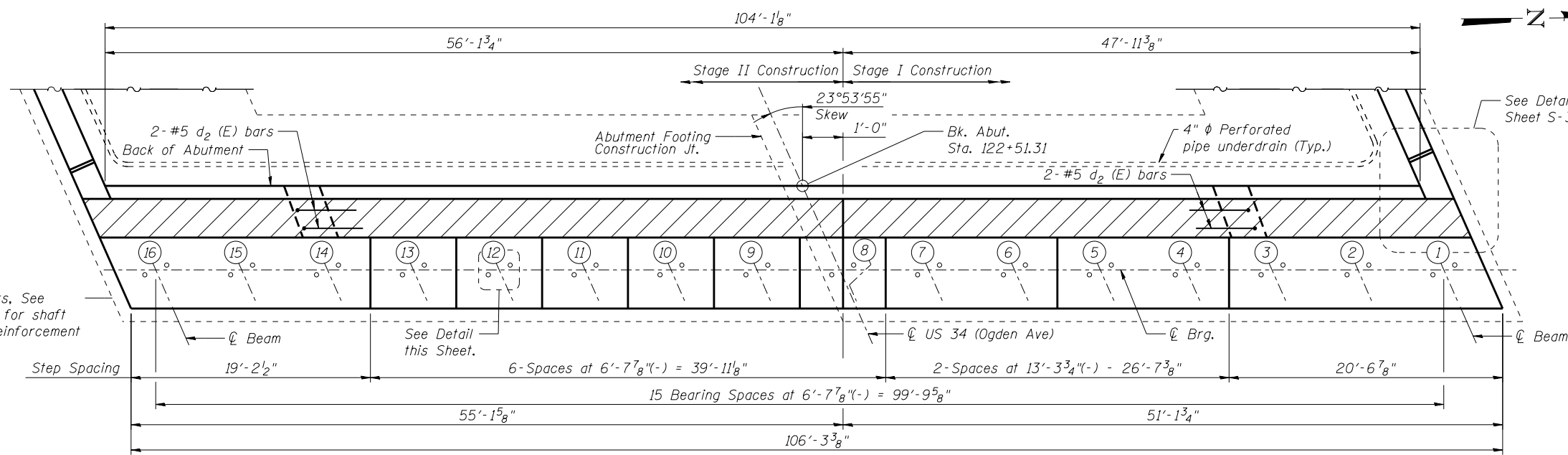
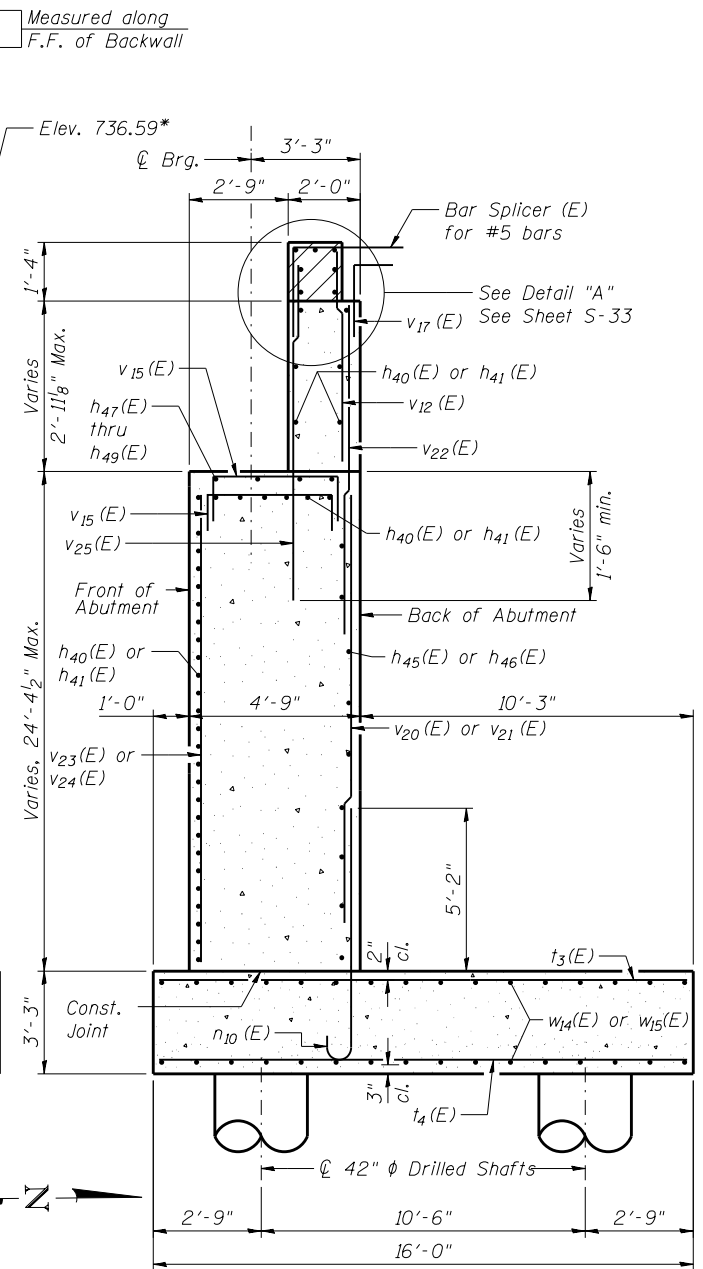
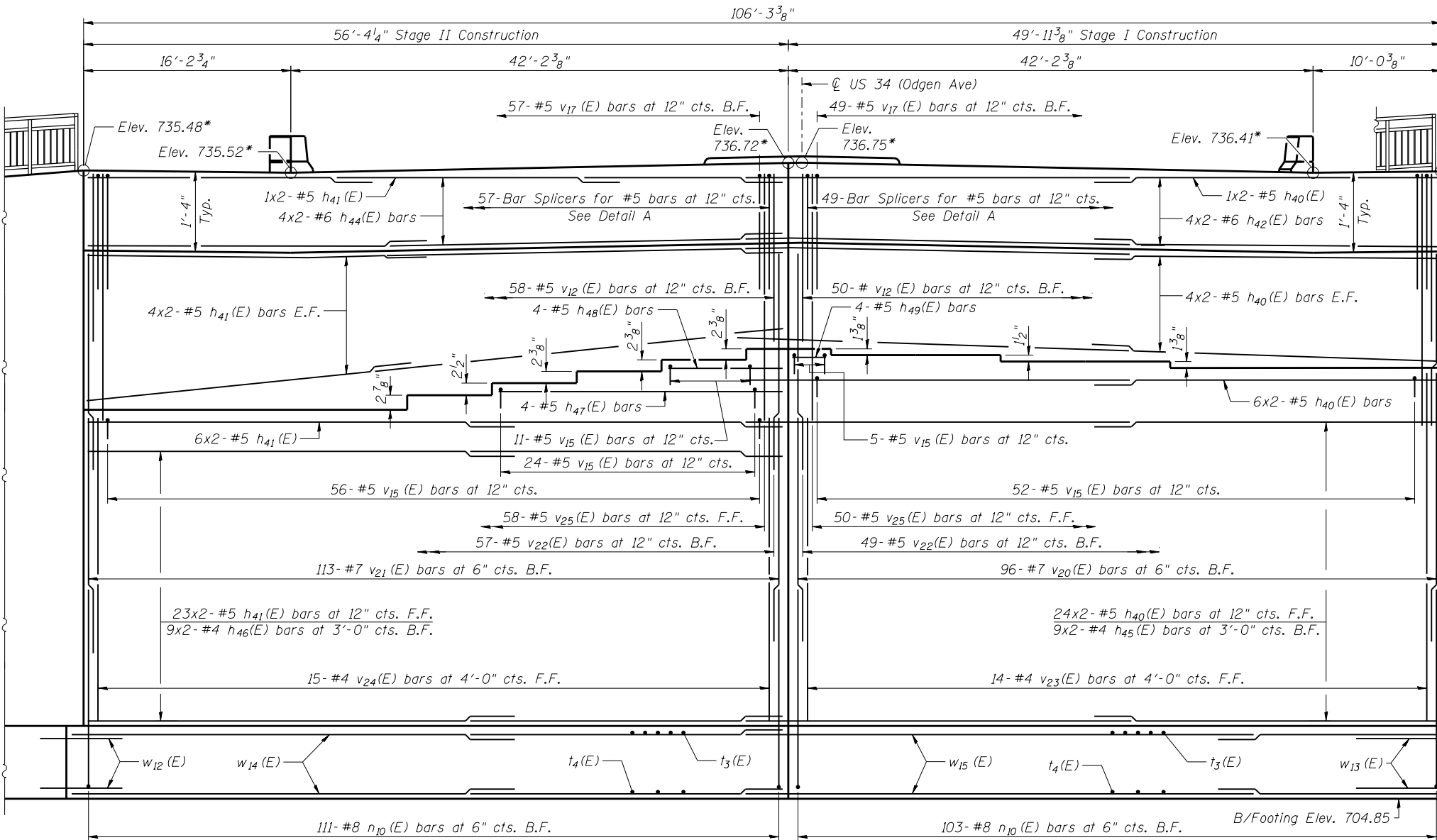
**SEAT ELEVATIONS**

Beam Line	Seat Elevation
1	732.13
2	732.13
3	732.13
4	732.24
5	732.24
6	732.36
7	732.36
8	732.47
9	732.27
10	732.07
11	731.87
12	731.67
13	731.46
14	731.22
15	731.22
16	731.22

\* Elevations along back of abutment



**ANCHOR BOLT LAYOUT**



USER NAME =  
 PLOT SCALE =  
 PLOT DATE = 02/28/2014

DESIGNED - KMP  
 CHECKED - AMK  
 DRAWN - KMP  
 CHECKED - AMK

REVISED  
 REVISED  
 REVISED  
 REVISED

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

WEST ABUTMENT 2  
 STRUCTURE NO. 022-0512

SHEET NO. S-32 OF S-53 SHEETS

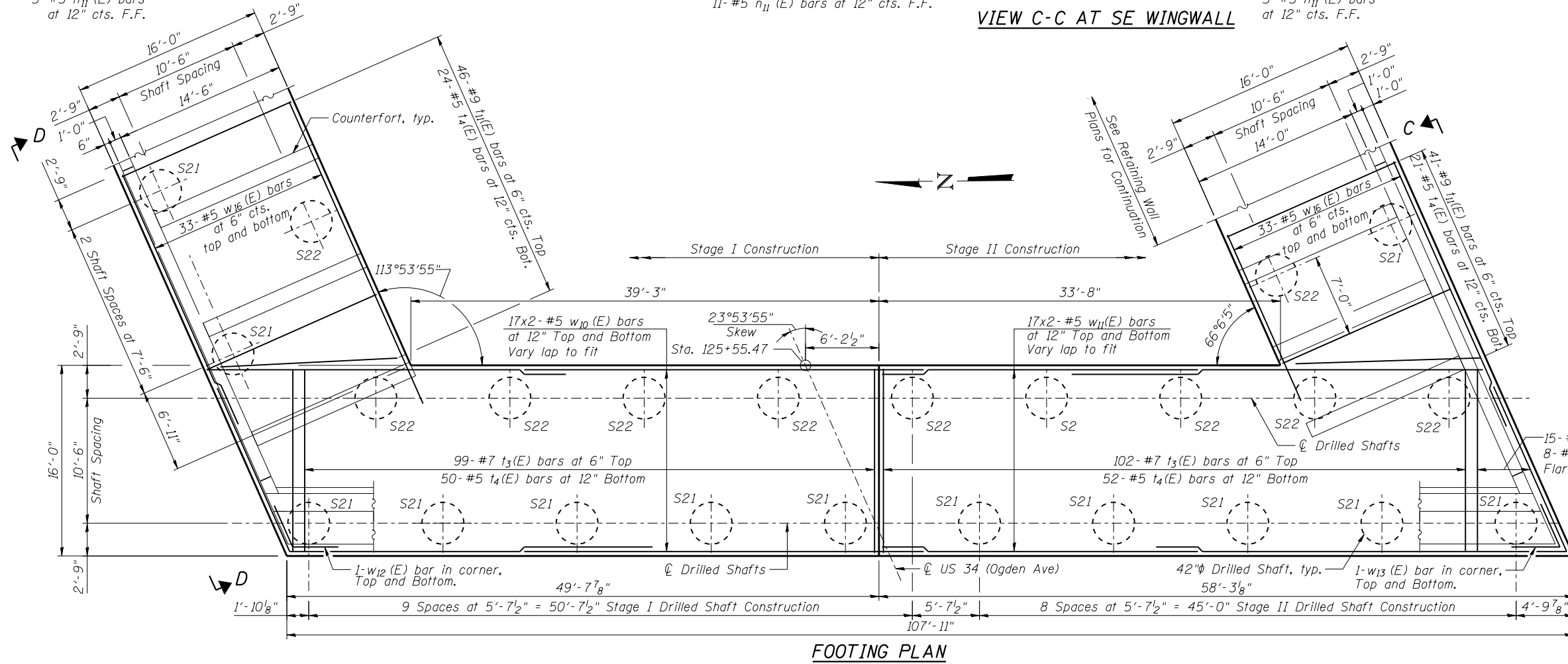
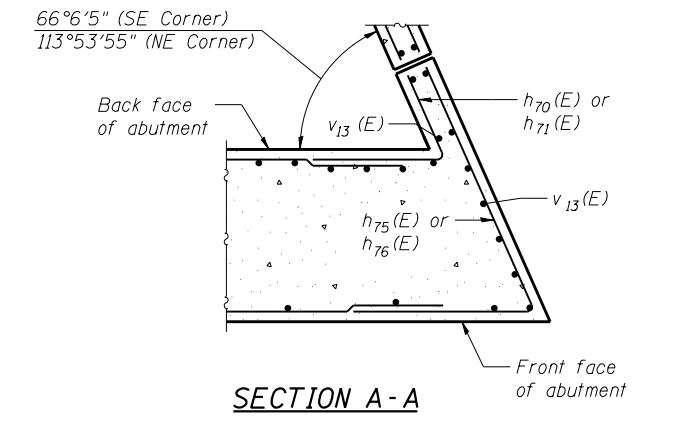
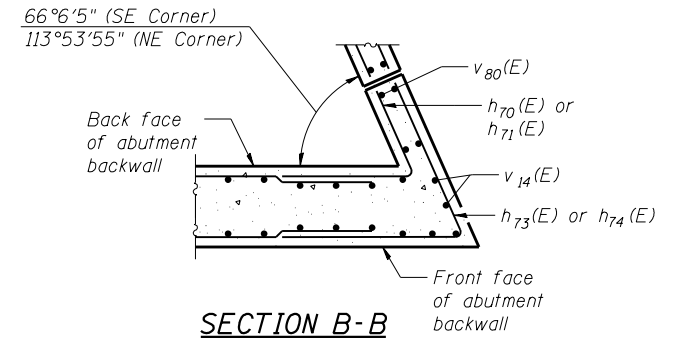
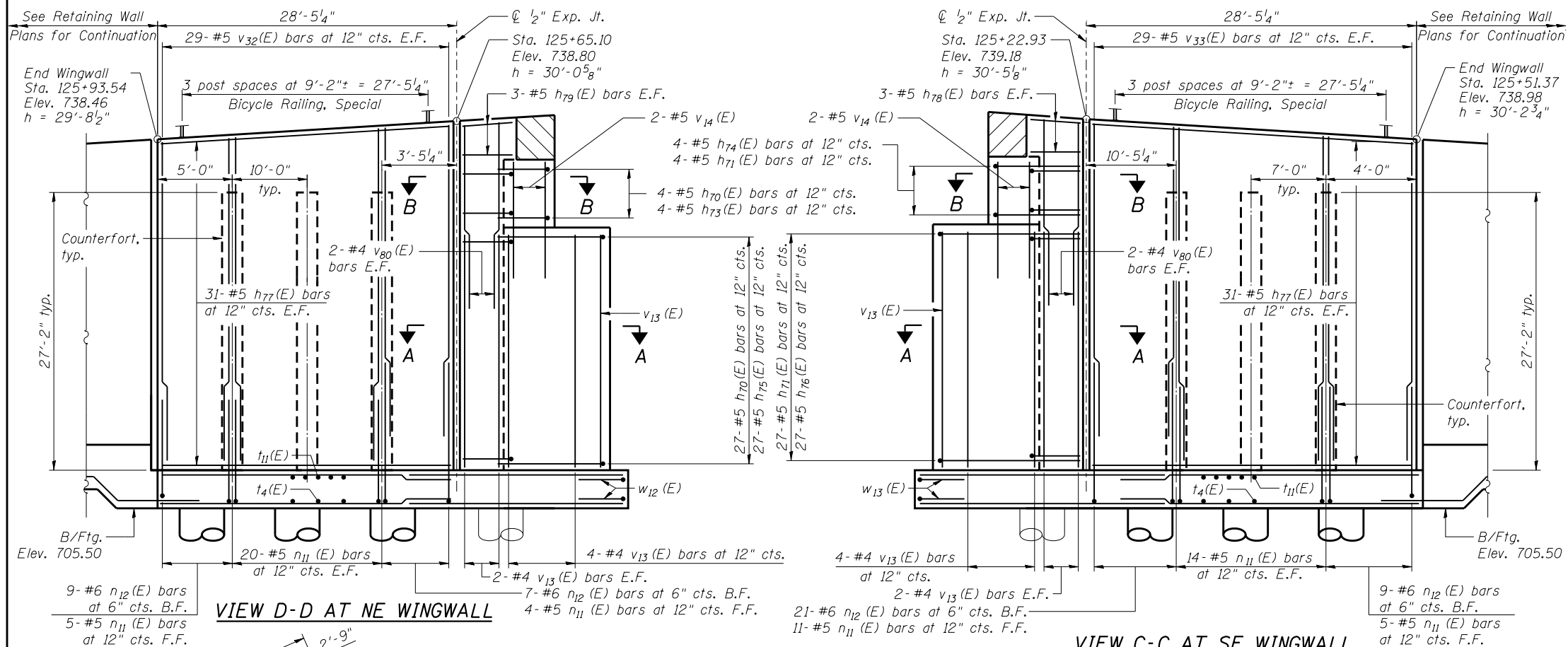
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DuPAGE	383	195

CONTRACT NO. 60R06

ILLINOIS FED. AID PROJECT







**MINIMUM BAR LAP**  
#5 Bar = 2'-11" (Top Bar)

- NOTES**
1. Bars indicated 16x3-#5 etc. indicates 16 lines of bars with 3 lengths per line.
  2. F.F. denotes Front Face. B.F. denotes Back Face. E.F. denotes Each Face.
  3. For Bar List and Bill of Materials, see Sheet S-36.
  4. For Section through wingwall see Sheet S-36.
  5. Concrete Sealer shall be applied to the backwall, bearing seat, and front face of abutment.



USER NAME =	DESIGNED - KMP	REVISED
PLOT SCALE =	CHECKED - STB	REVISED
PLOT DATE = 02/28/2014	DRAWN - KMP	REVISED
	CHECKED - STB	REVISED

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EAST ABUTMENT 1  
STRUCTURE NO. 022-0512**

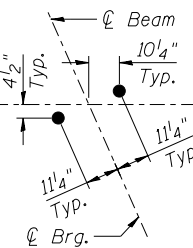
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DuPAGE	383	197
CONTRACT NO. 60R06			ILLINOIS FED. AID PROJECT	

SHEET NO. S-34 OF S-53 SHEETS

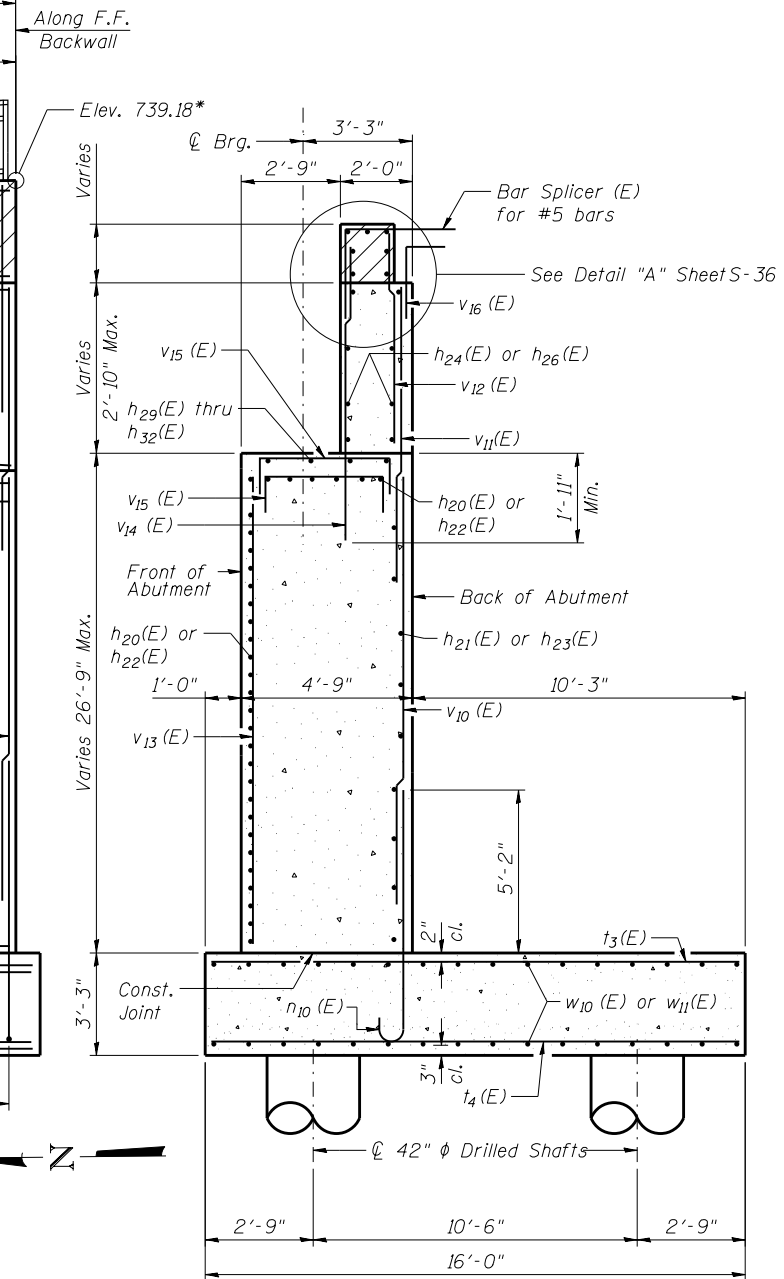
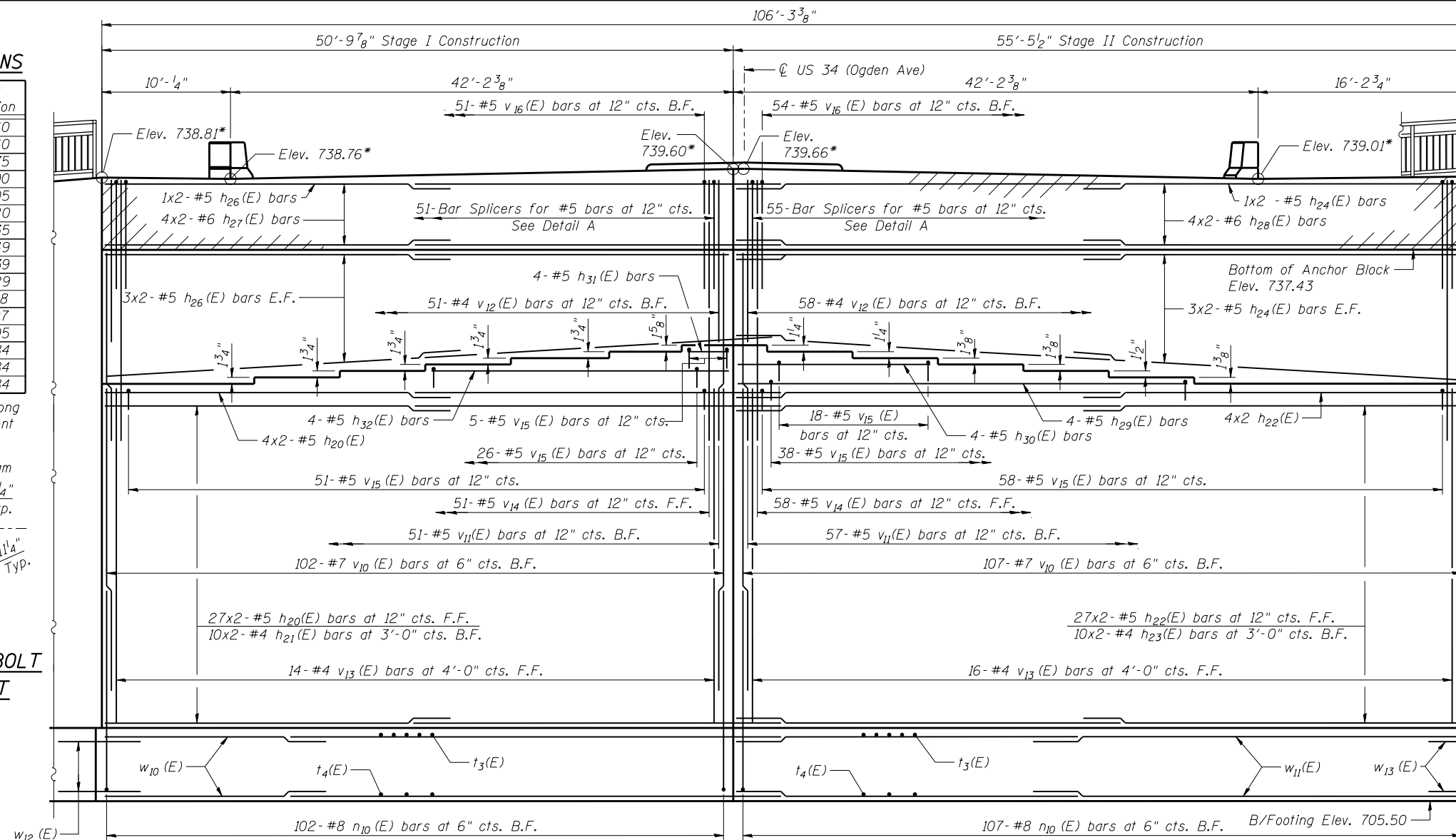
**SEAT ELEVATIONS**

Beam Line	Seat Elevation
1	734.60
2	734.60
3	734.75
4	734.90
5	735.05
6	735.20
7	735.35
8	735.49
9	735.39
10	735.29
11	735.18
12	735.07
13	734.95
14	734.84
15	734.84
16	734.84

\* Elevations along back of abutment



**ANCHOR BOLT LAYOUT**

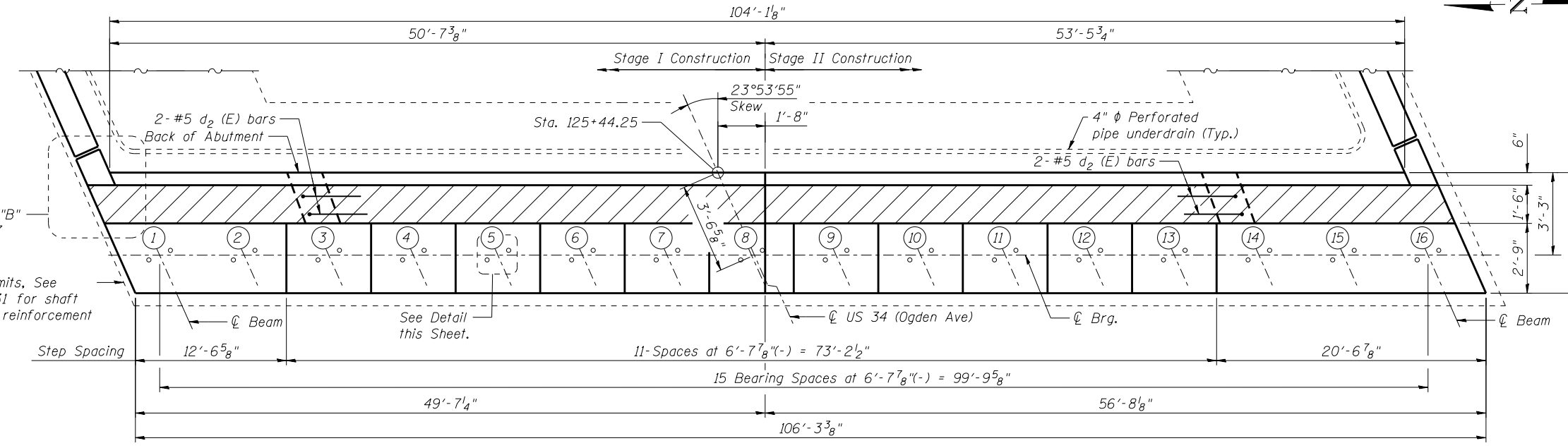


**MINIMUM BAR LAP**

- #4 Bar = 2'-4"
- #5 Bar = 2'-11"
- #6 Bar = 3'-6"
- #7 Bar = 4'-2"

**NOTES**

- Hatched area to be poured after superstructure forms have been removed. Quantity of concrete to be included with Concrete Superstructure.
- Space Reinforcement in cap to miss anchor bolts.
- Bars indicated 10x2-#5 etc. indicates 2 lines of bars with 3 lengths per line.
- Pour steps monolithically with stem.
- F.F. denotes Front Face. B.F. denotes Back Face. E.F. denotes Each Face.
- For Bar List and Bill of Materials, see Sheet S-36



USER NAME =  
DESIGNED - KMP  
CHECKED - STB  
PLOT SCALE =  
DRAWN - KMP  
PLOT DATE = 02/28/2014

DESIGNED - KMP  
CHECKED - STB  
DRAWN - KMP  
CHECKED - AMK

REVISED  
REVISED  
REVISED  
REVISED

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EAST ABUTMENT 2  
STRUCTURE NO. 022-0512**

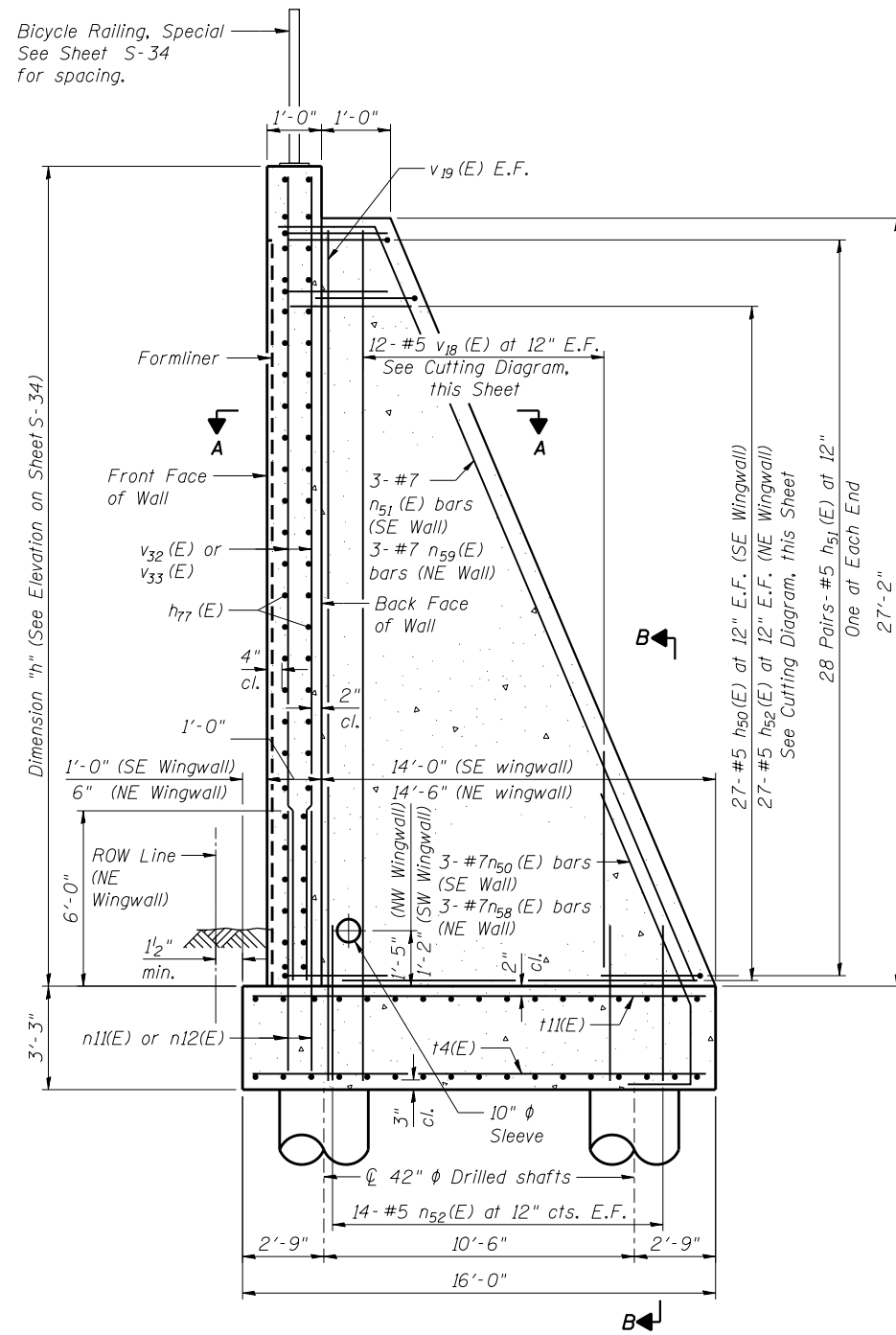
SHEET NO. S-35 OF S-53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DUPAGE	383	198

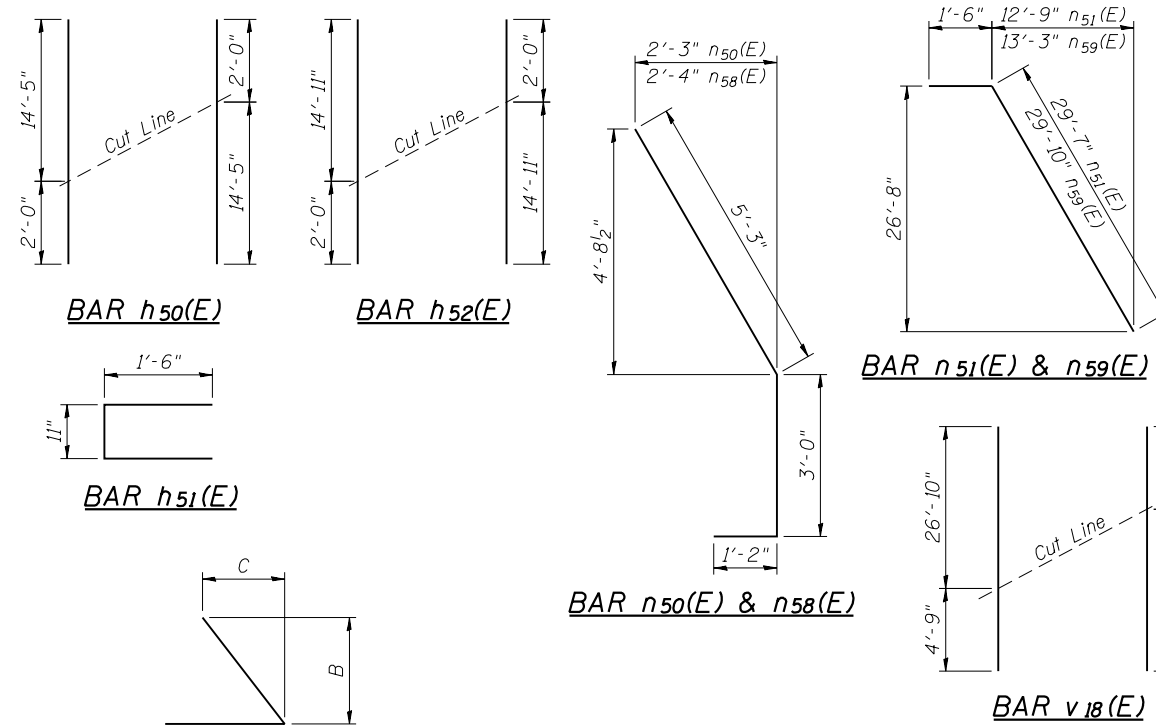
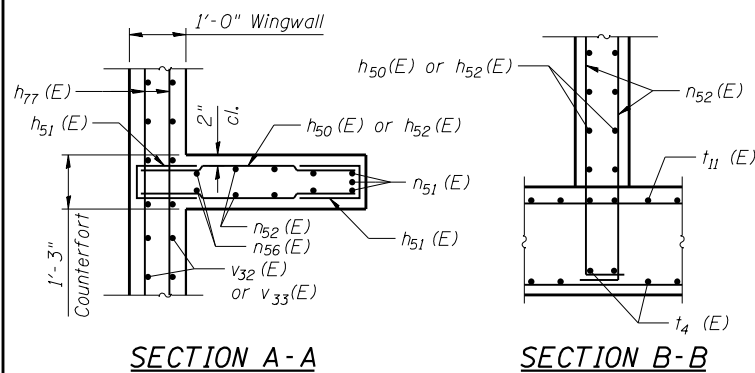
CONTRACT NO. 60R06

ILLINOIS FED. AID PROJECT

Bicycle Railing, Special  
See Sheet S-34  
for spacing.



WINGWALL SECTION

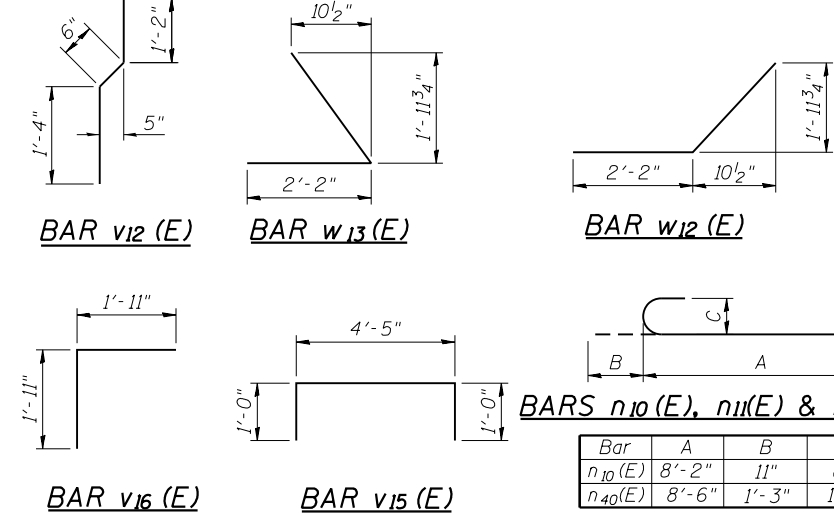


BAR h<sub>71</sub>(E), h<sub>74</sub>(E), h<sub>76</sub>(E)

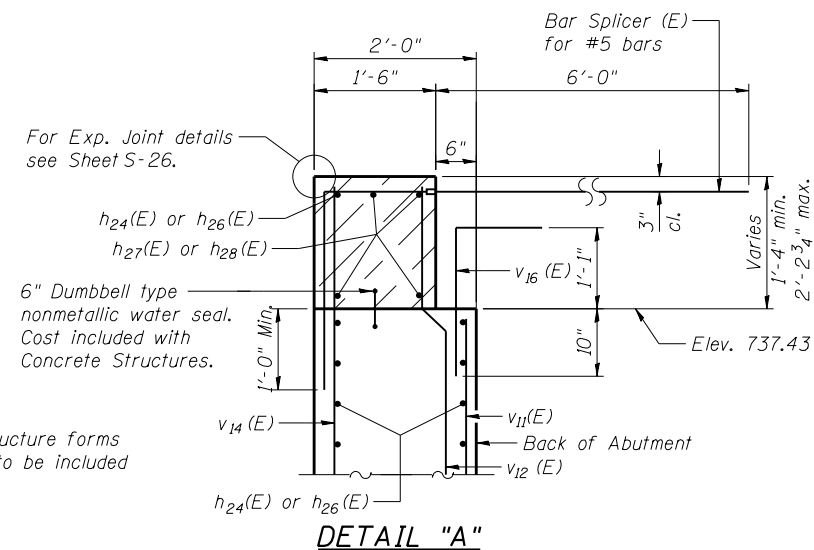
Bar	A	B	C
h <sub>71</sub> (E)	3'-3"	1'-0"	5 1/2"
h <sub>74</sub> (E)	2'-7"	1'-3 3/4"	2'-11 5/8"
h <sub>76</sub> (E)	2'-7"	5'-8 1/4"	2'-6 1/4"

BAR h<sub>70</sub>(E), h<sub>73</sub>(E), h<sub>75</sub>(E)

Bar	A	B	C
h <sub>70</sub> (E)	2'-7"	7 1/4"	1'-4 1/2"
h <sub>73</sub> (E)	2'-7"	2'-11 5/8"	1'-3 3/4"
h <sub>75</sub> (E)	2'-7"	2'-6 3/4"	5'-9 1/2"



Bar	A	B	C
n <sub>10</sub> (E)	8'-2"	11"	8"
n <sub>40</sub> (E)	8'-6"	1'-3"	11 3/4"



NOTES

- Hatched area to be poured after superstructure forms have been removed. Quantity of concrete to be included with "Concrete Superstructure".
- E.F. denotes Each Face See Sheet S-20 for d<sub>2</sub>(E) bar details.

BILL OF MATERIALS - EAST ABUTMENT

Bar	No.	Size	Length	Shape
d <sub>2</sub> (E)	4	#5	7'-11"	—
h <sub>20</sub> (E)	62	#5	27'-9"	—
h <sub>21</sub> (E)	20	#4	28'-1"	—
h <sub>22</sub> (E)	62	#5	29'-8"	—
h <sub>23</sub> (E)	20	#4	28'-3"	—
h <sub>24</sub> (E)	14	#5	28'-5"	—
h <sub>26</sub> (E)	14	#5	27'-2"	—
h <sub>27</sub> (E)	8	#6	27'-3"	—
h <sub>28</sub> (E)	8	#6	29'-4"	—
h <sub>29</sub> (E)	4	#5	35'-9"	—
h <sub>30</sub> (E)	4	#5	15'-10"	—
h <sub>31</sub> (E)	4	#5	3'-5"	—
h <sub>32</sub> (E)	4	#5	23'-5"	—
h <sub>50</sub> (E)	81	#5	16'-5"	—
h <sub>51</sub> (E)	336	#5	3'-11"	—
h <sub>52</sub> (E)	81	#5	16'-11"	—
h <sub>70</sub> (E)	31	#5	3'-10"	—
h <sub>71</sub> (E)	31	#5	4'-4"	—
h <sub>73</sub> (E)	4	#5	5'-1"	—
h <sub>74</sub> (E)	4	#5	5'-10"	—
h <sub>75</sub> (E)	27	#5	8'-1"	—
h <sub>76</sub> (E)	27	#5	8'-11"	—
h <sub>77</sub> (E)	124	#5	28'-1"	—
h <sub>78</sub> (E)	6	#5	0'-9"	—
h <sub>79</sub> (E)	6	#5	1'-3"	—
n <sub>10</sub> (E)	209	#8	9'-1"	—
n <sub>11</sub> (E)	118	#5	9'-0"	—
n <sub>12</sub> (E)	46	#5	9'-0"	—
n <sub>40</sub> (E)	312	#9	9'-9"	—
n <sub>50</sub> (E)	9	#7	9'-5"	—
n <sub>51</sub> (E)	9	#7	31'-1"	—
n <sub>52</sub> (E)	84	#5	5'-6"	—
n <sub>56</sub> (E)	12	#5	29'-8"	—
n <sub>58</sub> (E)	9	#5	9'-5"	—
n <sub>59</sub> (E)	9	#5	31'-4"	—
SP40	39	#4	27'-1"	—
SP41	22	#4	25'-2"	—
t <sub>3</sub> (E)	231	#7	15'-8"	—
t <sub>4</sub> (E)	163	#5	15'-8"	—
t <sub>11</sub> (E)	87	#9	15'-8"	—
v <sub>10</sub> (E)	209	#7	25'-0"	—
v <sub>11</sub> (E)	108	#5	5'-9"	—
v <sub>12</sub> (E)	109	#4	3'-0"	—
v <sub>13</sub> (E)	46	#4	25'-6"	—
v <sub>14</sub> (E)	113	#5	5'-10"	—
v <sub>15</sub> (E)	196	#5	6'-5"	—
v <sub>16</sub> (E)	105	#5	3'-10"	—
v <sub>18</sub> (E)	72	#5	31'-7"	—
v <sub>19</sub> (E)	12	#5	26'-10"	—
v <sub>32</sub> (E)	58	#5	29'-4"	—
v <sub>33</sub> (E)	58	#5	29'-8"	—
v <sub>40</sub>	507	#9	30'-0"	—
v <sub>41</sub>	286	#9	27'-3"	—
v <sub>80</sub> (E)	8	#4	6'-11"	—
w <sub>10</sub> (E)	68	#5	29'-8"	—
w <sub>11</sub> (E)	68	#5	30'-5"	—
w <sub>12</sub> (E)	2	#5	4'-4"	—
w <sub>13</sub> (E)	2	#5	4'-4"	—
w <sub>16</sub> (E)	124	#5	25'-0"	—
Structure Excavation	Cu. Yd.		573	
Concrete Structures	Cu. Yd.		821.4	
Reinforcement Bars	Pounds		96,610	
Reinforcement Bars, Epoxy Coated	Pounds		77,150	

\*\* Length is height of spiral.  
Minimum lap for spirals is 2'-0"



USER NAME =	DESIGNED - KMP	REVISED
	CHECKED - STB	REVISED
PLOT SCALE =	DRAWN - KMP	REVISED
PLOT DATE = 03/19/2014	CHECKED - STB	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EAST ABUTMENT DETAILS  
STRUCTURE NO. 022-0512

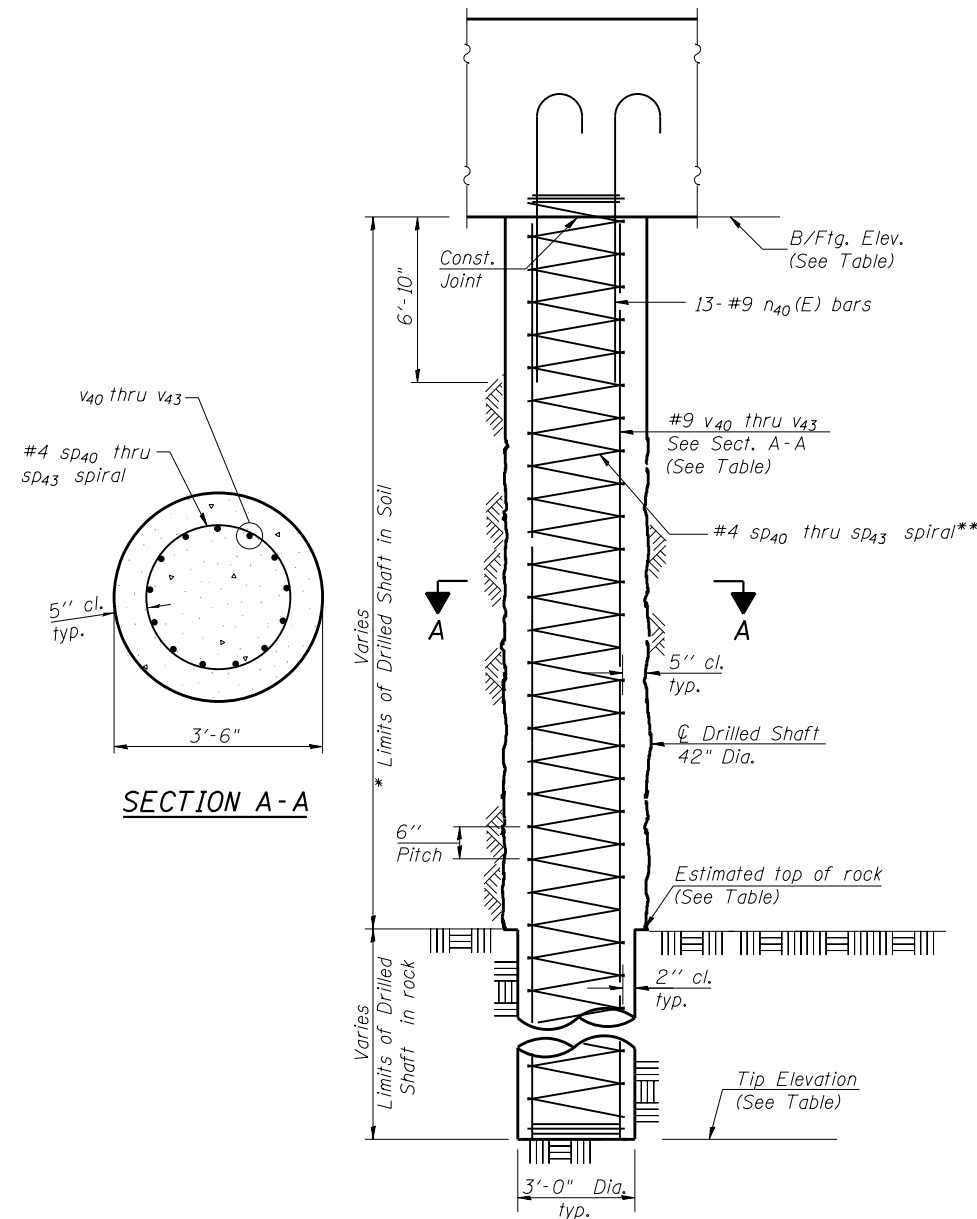
SHEET NO. S-36 OF S-53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DuPAGE	383	199
			CONTRACT NO. 60R06	
ILLINOIS FED. AID PROJECT				

**DRILLED SHAFT SCHEDULE**

Shaft Designation	Location	No. Shafts	Bottom of Footing	Tip Elevation	Est. Top of Rock	Vertical Reinforcement	Spiral Reinforcement	Total Length	Rock Socket Length
S21	E. Abut.	13	705.50	624.50	633.80	13x3- #9 v40	1x3 #4 sp40	81.0'	9.30'
S22	E. Abut.	11	705.50	655.50	633.80	13x2- #9 v41	1x2 #4 sp41	50.0'	-
S23	W. Abut.	6	704.85	608.50	616.30	13x3- #9 v42	1x3 #4 sp42	96.4'	7.80'
S24	W. Abut.	7	704.85	626.00	633.80	13x3- #9 v43	1x3 #4 sp43	78.9'	7.80'
S25	W. Abut.	6	704.85	654.85	616.30	13x2- #9 v41	1x2 #4 sp41	50.0'	-
S26	W. Abut.	5	704.85	654.85	633.80	13x2- #9 v41	1x2 #4 sp41	50.0'	-

Notes:  
See sheets S-31 and S-34 for Drilled Shaft Locations  
See sheet S-36 for bar bend details and bill of bars.

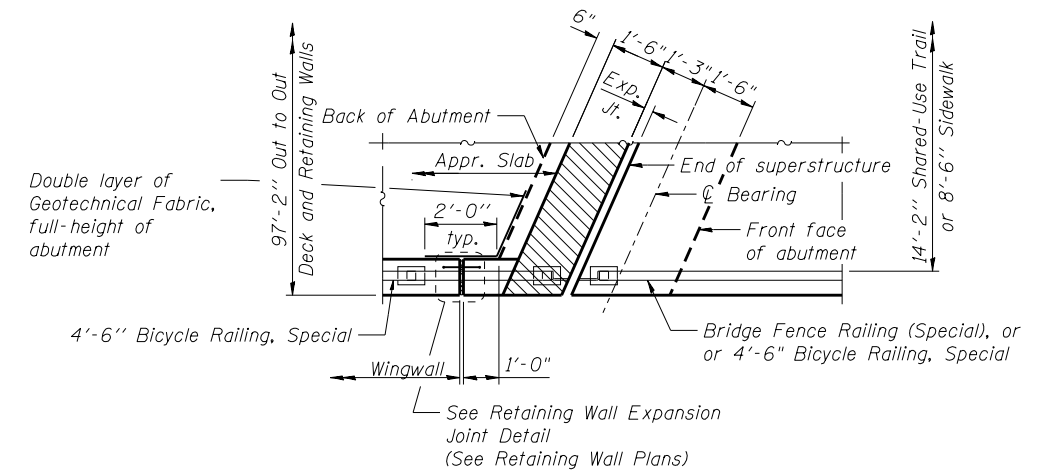
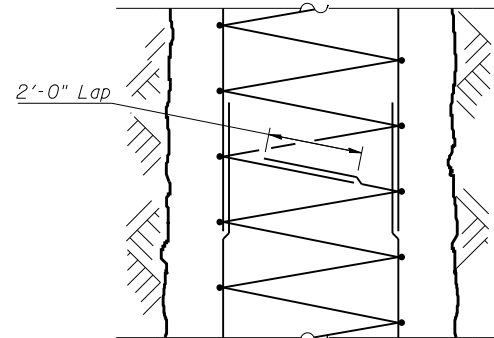


**DRILLED SHAFT DETAILS**

- \* The quantities and detailing are based on the estimated elevations shown on the plans. The actual elevations may differ at each shaft and corresponding adjustments shall be made to the drilled shaft and reinforcement quantities and payment limits.
- \*\* Provide 1/2 extra turns top and bottom of each drilled shaft. Extend spiral 2" into abutment or wingwall footing. Provide min. 4- #4 spacers or equivalent.

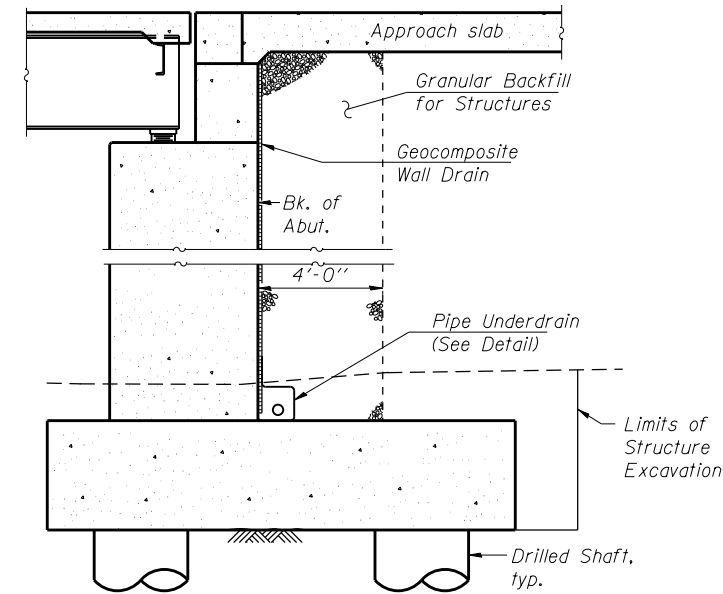
**MINIMUM BAR LAP**

#9 v40 thru v43 bar = 4'-7"



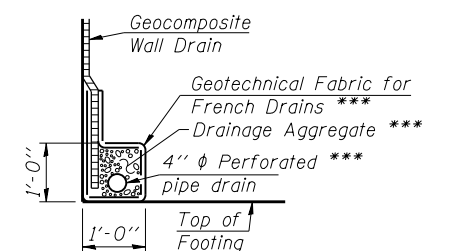
**DETAIL B**

NE Corner Shown, Other 3 Quadrants Similar.



**SECTION THRU ABUTMENT**

(Horiz. dim. at Rt. L's)



**PIPE UNDERDRAIN DETAIL**

\*\*\* Included in the cost of Pipe Underdrains for Structures. Connect abutment underdrains to Retaining Wall underdrains, and outlet to drainage structures. See Retaining Wall plans for outlet locations.



USER NAME =	DESIGNED - KMP	REVISED
	CHECKED - STB	REVISED
PLOT SCALE =	DRAWN - KMP	REVISED
PLOT DATE = 02/28/2014	CHECKED - STB	REVISED

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DRILLED SHAFT AND MISCELLANEOUS DETAILS  
STRUCTURE NO. 022-0512

SHEET NO. S-37 OF S-53 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	652-A	DuPAGE	383	200
CONTRACT NO. 60R06			ILLINOIS FED. AID PROJECT	