

04-25-14 LETTING ITEM 182

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

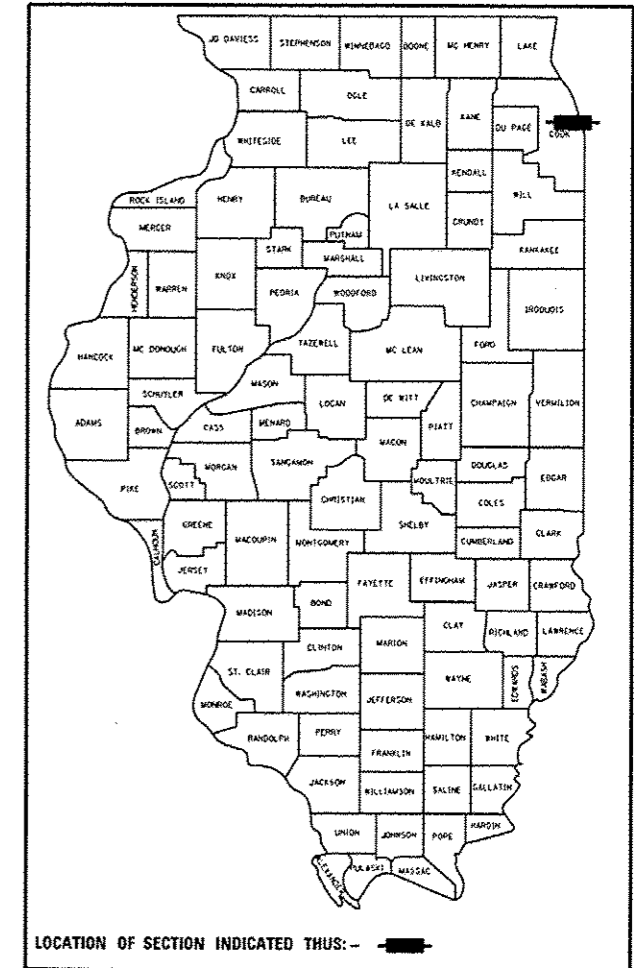
**PROPOSED
HIGHWAY PLANS**

**FAI 55 ROUTE I-55 (STEVENSON EXPRESSWAY)
AT US-41 (LAKE SHORE DR.)
SECTION: 2014-018BR
BRIDGE DECK REPAIRS
COOK COUNTY**

C-91-264-14

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|------------|--------|--------------------|-----------|
| 55 | 2014-018BR | COOK | 37 | 1 |
| ILLINOIS | | | CONTRACT NO. 60X91 | |

D-91-264-14



FOR INDEX OF SHEETS, SEE SHEET NO. 2

THE IMPROVEMENT IS LOCATED
IN THE CITY OF CHICAGO

TRAFFIC DATA

2013 ADT = 123,400

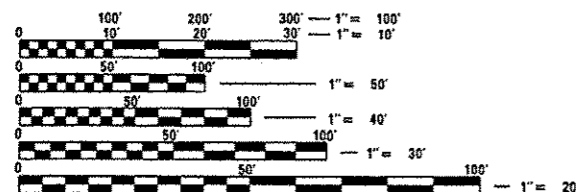
SPEED LIMIT = 30 - 50 MPH

LOCATION OF WORK:
F.A.I. 55 AT U.S. 41
SN 016-0036, SN 016-1045
SN 016-1048, SN 016-1052
SN 016-1055, SN 016-1075



SOUTH TOWNSHIP

GROSS & NET LENGTH = 7,796 FT. = 1.478 MILES



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

C.U.A.N.
CHICAGO UTILITY ALERT NETWORK
1-312-744-7000

PROJECT ENGINEER - J. ALAIN MIDY (847) 221-3056
PROJECT MANAGER - ISSAM RAYYAN (847) 705-4178

CONTRACT NO. 60X91

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED March 20 20 14

John D. Baranzoli
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

March 21 20 14
John D. Baranzoli P.E.
ENGINEER OF DESIGN AND ENVIRONMENT

March 21 20 14
Chris Osman P.E.
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

INDEX OF SHEETS

SHEET NO. DESCRIPTION

1. TITLE SHEET
2. INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
3. SUMMARY OF QUANTITIES
4. SITE PLAN
5. SUGGESTED STAGE IA CONSTRUCTION
6. SUGGESTED STAGE IB CONSTRUCTION
7. SUGGESTED STAGE II CONSTRUCTION
8. SUGGESTED STAGE IIIA CONSTRUCTION
9. SUGGESTED STAGE IIIB CONSTRUCTION
10. SUGGESTED STAGING FOR SHORING AND CRIBBING
11. DETOUR PLAN - STAGE IIIB
12. NIGHT TIME DETOUR PLAN - STAGE II & STAGE IIIB
13. STATE STREET RAMP DETOUR

S.N. 016-0036

14. GENERAL PLAN AND ELEVATION
15. DECK CONCRETE PATCHING
16. PIER E5 REPAIRS

SN 016-1045

17. GENERAL PLAN AND ELEVATION
18. DECK CONCRETE PATCHING

SN 016-1048

19. GENERAL PLAN AND ELEVATION
20. DECK CONCRETE PATCHING

SN 016-1052

21. GENERAL PLAN AND ELEVATION
22. DECK CONCRETE PATCHING

SN 016-1055

23. GENERAL PLAN AND ELEVATION
24. DECK CONCRETE PATCHING

SN 016-1075

25. GENERAL PLAN AND ELEVATION
26. DECK CONCRETE PATCHING
27. PIER E24 REPAIRS
28. ENTRANCE AND EXIT RAMP CLOSURE DETAILS (TC-08)
29. TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE & MULTI-LANE WEAVE (TC-09)
- 30 - 31. MULTI-LANE FREEWAY PAVEMENT MARKIGN DETAILS (TC-12)
32. TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES (TC-17)
- 33 - 37. DETECTOR LOOP DETAILS

LIST OF STATE STANDARDS

| STANDARD NO. | DESCRIPTION |
|--------------|--|
| 701400-07 | APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY |
| 701401-08 | LANE CLOSURE, FREEWAY/EXPRESSWAY |
| 701411-08 | LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS ≥ 45 MPH |
| 701422-06 | LANE CLOSURE, MULTILANE, FOR SPEEDS ≥ 45 MPH TO 55 MPH |
| 701428 | TRAFFIC CONTROL SETUP AND REMOVAL FREEWAY/EXPRESSWAY |
| 701446-05 | TWO LANE CLOSURE FREEWAY/EXPRESSWAY |
| 701901-03 | TRAFFIC CONTROL DEVICES |
| 704001-07 | TEMPORARY CONCRETE BARRIER |
| 780001-04 | TYPICAL PAVEMENT MARKINGS |

GENERAL NOTES

THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM I.D.O.T. FIELD MAINTENANCE ENGINEERS.

THE CONTRACTOR MUST USE EXTREME CAUTION DURING FULL DEPTH DECK SLAB REPAIR AND MUST NOT NICK, CUT OR DAMAGE, IN ANY WAY, ANY OF THE STEEL BEAMS. THE EXISTING REINFORCEMENT BARS SHALL BE CLEANED. IF ANY OF THESE REINFORCEMENT BARS ARE DAMAGED THEY MUST BE REPLACED WITH AN APPROVED BAR SPLICER OR ANCHORAGE SYSTEM. THE COST OF THIS WORK IS INCLUDED WITH DECK SLAB REPAIR (FULL DEPTH, TYPE II).

THE CONTRACTOR SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4151 AT LEAST 72 HOURS PRIOR TO INSTALLATION OF THE TEMPORARY TRAFFIC CONTROL DEVICES.

THESE ARE ELECTRICAL CONDUITS AND/OR FIBERGLASS DRAIN PIPES ATTACHED TO THE BOTTOM OF THE BRIDGE DECK. THE CONTRACTOR MAY MODIFY THE FORMING AND MUST EXERCISE EXTREME CAUTION DURING FULL DEPTH REPAIR BREAK OUT AND SHALL TEMPORARILY SUPPORT THE CONDUITS AND/OR DRAIN PIPES IF NEED BE, AT NO ADDITIONAL COST TO THE DEPARTMENT.

WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR MUST EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC.

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO ROUTINE VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF THE WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED BASED UPON THE UNIT PRICE BID FOR THE WORK.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL C.U.A.N. AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES (48 HOURS NOTIFICATION IS REQUIRED).

THE CONTRACTOR WILL NOT BE ALLOWED TO SETUP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT

TO COORDINATE ACCESS TO THE McCORMICK PLACE PARKING LOT CALL DARREN PETER AT (312) 296-5984.

DURING ANY WORK WITHIN THE McCORMICK PLACE PARKING LOT, THE CONTRACTOR MUST COORDINATE WITH THE McCORMICK PLACE PARKING LOT AUTHORITY AS TO AVOID ANY AND ALL CONFLICTS DURING THIS CONTRACT.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

ANY DAMAGES TO THE CONDUITS OR PIPES CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPLACED OR REPAIRED BY THE CONTRACTOR AT HIS/HER EXPENSE.

ALL PROPOSED RAISED REFLECTIVE PAVEMENT MARKERS (BRIDGE) SHALL BE LOW PROFILE.

THE CONTRACTOR SHALL COORDINATE AND/OR MODIFY THE WORK SCHEDULE AS DIRECTED BY THE DEPARTMENT TO ACCOMODATE SPECIAL EVENTS.

THE CONTRACTOR SHALL PLACE TEMPORARY CONCRETE BARRIER ADJACENT TO TEMPORARY SHORING AND CRIBBING TO PROVIDE A BARRIER FROM TRAFFIC. AN ESTIMATED THREE (3) SECTIONS OF TEMPORARY CONCRETE BARRIER WILL BE NEEDED.

THE CONTRACTOR SHALL USE PP3 CONCRETE FOR ALL DECK PATCHES.

AS PART OF THIS PROJECT, ALL BRIDGE DECKS SHALL BE RE-STRIPED AND RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE REMOVED AND REPLACED.

THE CONTRACTOR WILL BE ALLOWED ADDITIONAL LANE CLOSURES TO MEASURE AND SAWCUT PATCHING AREAS AND TO LAYOUT WEAVE LOCATIONS.

TO COORDINATE TRAFFIC CONTROL WITH McCORMICK PLACE CALL EUGENE HARTISON AT (312) 791-6081.

| | | | | | | | | | | | |
|---|----------------------------|-------------|------------|---|--|---------------------------|------------|--------|--------------|-----------|----|
| FILE NAME: c:\pwworkspace\pds\dot\asbar\enp\1d382528\016414-ent-cover.dgn | USER NAME: asbar.enp | DESIGNED: - | REVISED: - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES I-55 (STEVENSON EXPRESSWAY) AT US-41 (LAKE SHORT DRIVE) | F.A.I. R.T.E.: | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| Default | PLUT SCALE: 1/8" = 1' - 0" | DRAWN: - | REVISED: - | | | 55 | 2014-0188R | COOK | 33 | 2 | |
| | PLUT DATE: 4/17/2014 | CHECKED: - | REVISED: - | | | CONTRACT NO. 60X91 | | | | | |
| | | DATE: - | REVISED: - | | | SCALE: | SHEET | OF | SHEETS | STA. | TO |

URBAN
100%
STATE

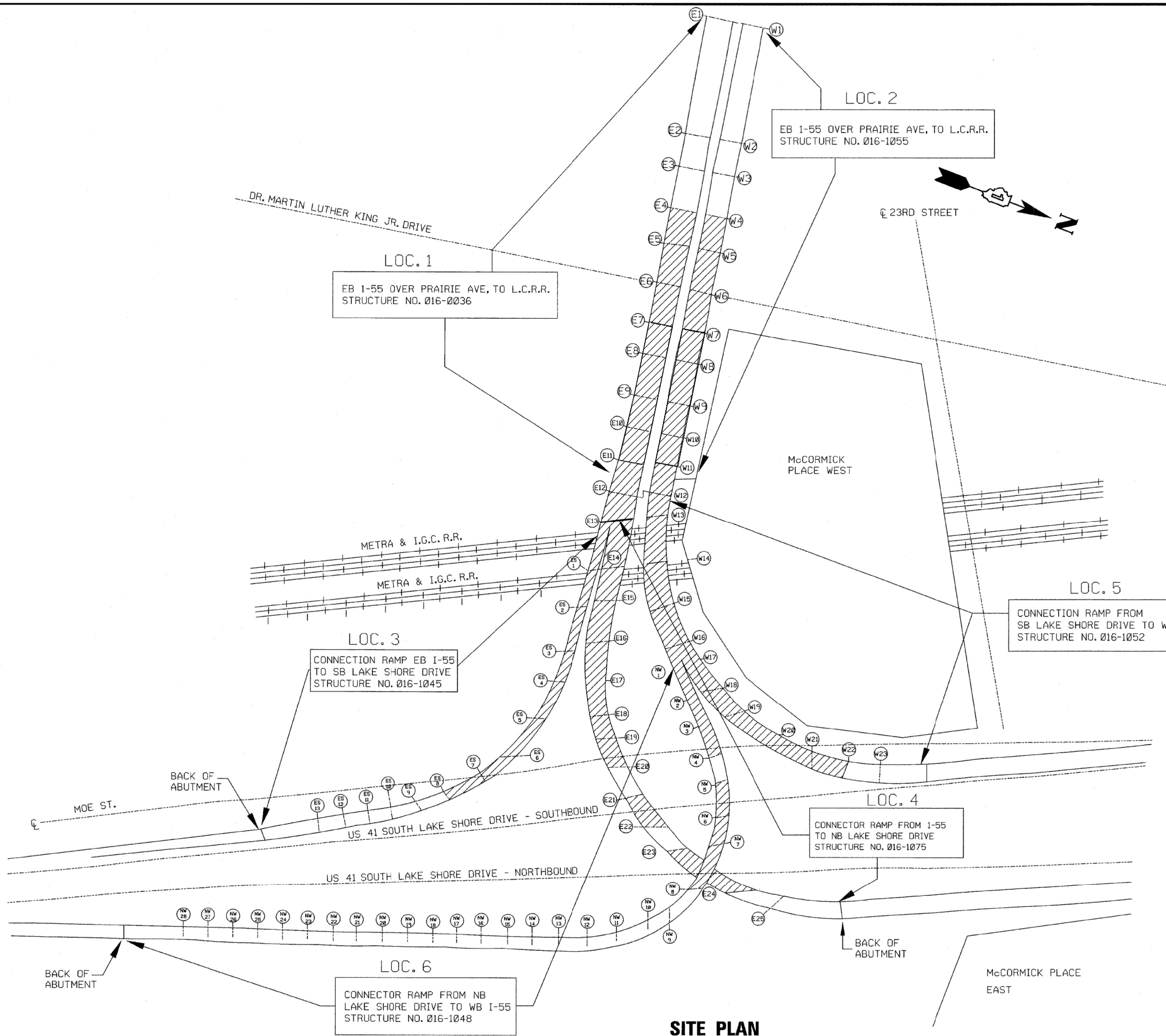
URBAN
100%
STATE

| SUMMARY OF QUANTITIES | | | CONSTRUCTION TYPE CODE | | | | | | | SUMMARY OF QUANTITIES | | | CONSTRUCTION TYPE CODE | | | | | | |
|-----------------------|---|--------|------------------------|---------------------|--------------------|--------------------|--------------------|--------------------|--------------------|-----------------------|--|---------|------------------------|--------------------|--------------------|--------------------|--------------------|--------------------|--------------------|
| CODE NO | ITEM | UNIT | TOTAL QUANTITIES | S.N. 0016-0036 0014 | S.N. 016-1045 0014 | S.N. 016-1048 0014 | S.N. 016-1052 0014 | S.N. 016-1055 0014 | S.N. 016-1075 0014 | CODE NO | ITEM | UNIT | TOTAL QUANTITIES | S.N. 016-0036 0014 | S.N. 016-1045 0014 | S.N. 016-1048 0014 | S.N. 016-1052 0014 | S.N. 016-1055 0014 | S.N. 016-1075 0014 |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 6 | 1 | 1 | 1 | 1 | 1 | 1 | Z0016002 | DECK SLAB REPAIR (FULL DEPTH, TYPE II) | SO YD | 52.2 | 4 | 24.4 | 7.9 | 0.7 | 12 | 3.2 |
| 67100100 | MOBILIZATION | L SUM | 1 | 0.17 | 0.17 | 0.17 | 0.17 | 0.16 | 0.16 | Z0016200 | DECK SLAB REPAIR (PARTIAL) | SO YD | 408.1 | 40.3 | 130.7 | 78.3 | 6.8 | 120.3 | 31.7 |
| 70106800 | CHANGEABLE MESSAGE SIGN | CAL MO | 6 | 1 | 1 | 1 | 1 | 1 | 1 | Z0073200 | TEMPORARY SHORING AND CRIBBING | EACH | 3 | 2 | | | | | 1 |
| 70200100 | NIGHTTIME WORK ZONE LIGHTING | L SUM | 1 | 0.17 | 0.17 | 0.17 | 0.17 | 0.16 | 0.16 | X7013820 | TRAFFIC CONTROL SURVEILLANCE (EXPRESSWAYS) | CAL DAY | 20 | 4 | 4 | 3 | 3 | 3 | 3 |
| 70400100 | TEMPORARY CONCRETE BARRIER | FOOT | 62.5 | 62.5 | | | | | | X8850102 | INDUCTION LOOP | FOOT | 120 | 20 | 20 | 20 | 20 | 20 | 20 |
| 70600240 | IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 2 | EACH | 1 | 1 | | | | | | | | | | | | | | | |
| * 78008210 | POLYUREA PAVEMENT MARKING TYPE I - LINE 4" | FOOT | 15320 | 1820 | 1940 | 4310 | 2350 | 2500 | 2400 | | | | | | | | | | |
| * 78008220 | POLYUREA PAVEMENT MARKING TYPE I - LINE 5" | FOOT | 1870 | 540 | | | 380 | 630 | 320 | | | | | | | | | | |
| * 78008240 | POLYUREA PAVEMENT MARKING TYPE I - LINE 8" | FOOT | 2270 | 1610 | | | 300 | | 360 | | | | | | | | | | |
| * 78008250 | POLYUREA PAVEMENT MARKING TYPE I - LINE 12" | FOOT | 1810 | 280 | | | 570 | 170 | 790 | | | | | | | | | | |
| * 78100300 | REPLACEMENT REFLECTOR | EACH | 218 | 86 | | | 38 | 62 | 32 | | | | | | | | | | |
| 78300100 | PAVEMENT MARKING REMOVAL | SO FT | 200 | 60 | | | 140 | | | | | | | | | | | | |
| X7011015 | TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS) | L SUM | 1 | 0.17 | 0.17 | 0.17 | 0.17 | 0.16 | 0.16 | | | | | | | | | | |
| X7030030 | WET REFLECTIVE TEMPORARY TAPE TYPE III, 4 INCH | FOOT | 9500 | 1300 | | | 8200 | | | | | | | | | | | | |
| X7030045 | WET REFLECTIVE TEMPORARY TAPE TYPE III, 8 INCH | FOOT | 2500 | 2050 | | | 450 | | | | | | | | | | | | |
| Z0012755 | STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES) | SO FT | 174 | 81 | | | | | 93 | | | | | | | | | | |

Rev.

| | | | | | | | | | | | | | | | | |
|-----------|-----------|----------|---------|---|--|--|--|-----------------------|--|--|--|---|------------|--------|--------------|-----------|
| FILE NAME | USER NAME | DESIGNED | REVISED | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | | | | SUMMARY OF QUANTITIES | | | | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | DRAWN | REVISED | | | | | | | | | 55 | 2014-018BR | COOK | 33 | 3 |
| | | CHECKED | REVISED | | | | | | | | | CONTRACT NO. 60X91 | | | | |
| | | DATE | REVISED | | | | | | | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

*Specialty Items



LEGEND:
 EXISTING PROTECTIVE SHIELDING

SITE PLAN

| | | | | | | | | | | | | |
|--|----------------------------|------------|-----------|---|---|--|---------------------------|-------------|------------|--------|--------------|-----------|
| FILE NAME = c:\pwork\work\pwork\osbornnp\d0382528\106414-shit-cover.dgn | USER NAME = osbornnp | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SITE PLAN | | | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 2.0000' / in. | CHECKED - | REVISED - | | I-55 (STEVENS ON EXPRESSWAY) AT US-41 (LAKE SHORE DRIVE) | | | 55 | 2014-018BR | COOK | 33 | 4 |
| Default | PLOT DATE = 3/20/2014 | DATE - | REVISED - | SCALE: SHEET OF SHEETS STA. TO STA. | | | CONTRACT NO. 60X91 | | | | | |
| ILLINOIS FED. AID PROJECT | | | | | | | | | | | | |

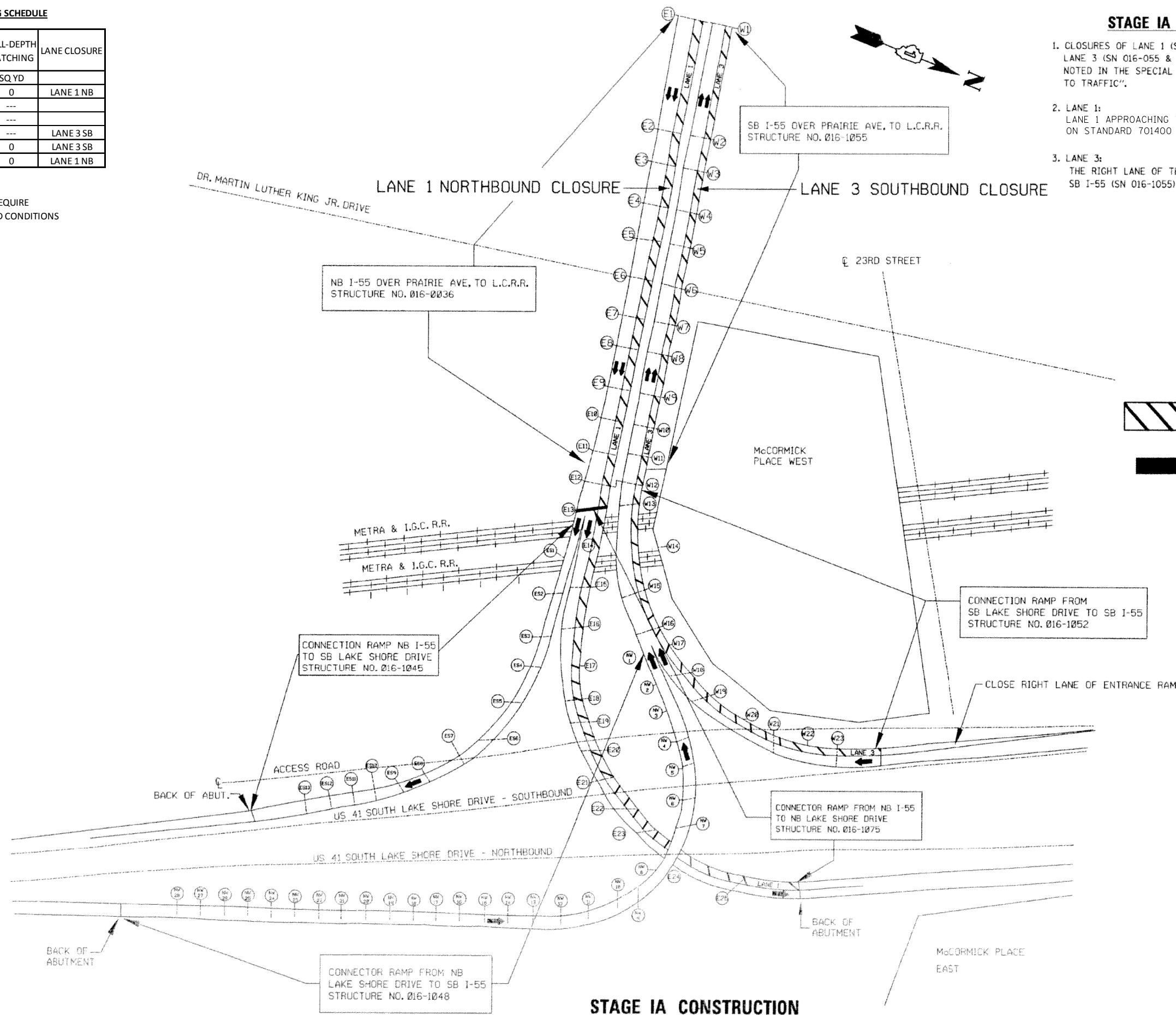
STAGE IA DECK PATCHING SCHEDULE

| STRUCTURE NO. | LANE NO. | PARTIAL-DEPTH PATCHING SQ.YD. | FULL-DEPTH PATCHING SQ.YD. | LANE CLOSURE |
|---------------|----------|-------------------------------|----------------------------|--------------|
| 016-0036 | 1 | 3 | 0 | LANE 1 NB |
| 016-1045 | --- | --- | --- | |
| 016-1048 | --- | --- | --- | |
| 016-1052 | 3 | 2.8 | --- | LANE 3 SB |
| 016-1055 | 3 | 187 | 0 | LANE 3 SB |
| 016-1075 | 1 | 4.8 | 0 | LANE 1 NB |

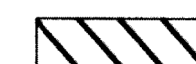

NOTE:
SOME PARTIAL-DEPTH PATCHES MAY REQUIRE FULL-DEPTH PATCHING BASED ON FIELD CONDITIONS

STAGE IA TRAFFIC CONTROL NOTES

- CLOSURES OF LANE 1 (SN 016-0036 & SN 016-1075) NB I-55 AND LANE 3 (SN 016-055 & SN 016-0052) SB I-55 WILL BE ALLOWED AS NOTED IN THE SPECIAL PROVISION "KEEPING THE EXPRESSWAY OPEN TO TRAFFIC".
- LANE 1:
LANE 1 APPROACHING THE WORK ZONE SHALL BE CLOSED AS SHOWN ON STANDARD 701400 & 701401
- LANE 3:
THE RIGHT LANE OF THE TWO LANE RAMP (SN 016-1052) AND LANE 3 SB I-55 (SN 016-1055) SHALL BE CLOSED AS SHOWN ON STANDARD 701422.



LEGEND:

-  WORK ZONE OR LANE CLOSURE
-  TRAFFIC DIRECTION

STAGE IA CONSTRUCTION

| | | | | | | | | | | |
|---|----------------------------|------------|-----------|---|--|---------------------------|------------|--------|--------------|-----------|
| FILE NAME = | USER NAME = osbornnp | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SUGGESTED STAGE IA CONSTRUCTION I-55 (STEVENSON EXPRESSWAY) AT US-41 (LAKE SHORE DRIVE) | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| et:\pw\work\p\idot\osbornnp\d0382528\DI06414-sh-t-cover.dgn | PLOT SCALE = 2.0000' / in. | DRAWN - | REVISED - | | | 55 | 2014-018BR | COOK | 33 | 5 |
| Default | PLOT DATE = 4/1/2014 | CHECKED - | REVISED - | | | CONTRACT NO. 60X91 | | | | |
| | | DATE - | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | |

STAGE IB DECK PATCHING SCHEDULE

| STRUCTURE NO. | LANE NO. | PARTIAL-DEPTH PATCHING SQ YD | FULL-DEPTH PATCHING SQ YD | LANE CLOSURE |
|---------------|----------|------------------------------|---------------------------|--------------|
| 016-0036 | --- | --- | --- | |
| 016-1045 | --- | --- | --- | |
| 016-1048 | --- | --- | --- | |
| 016-1052 | 2 | 2.2 | 0 | LANE 2 SB |
| 016-1055 | --- | --- | --- | |
| 016-1075 | 2 | 26.9 | 0 | LANE 2 NB |

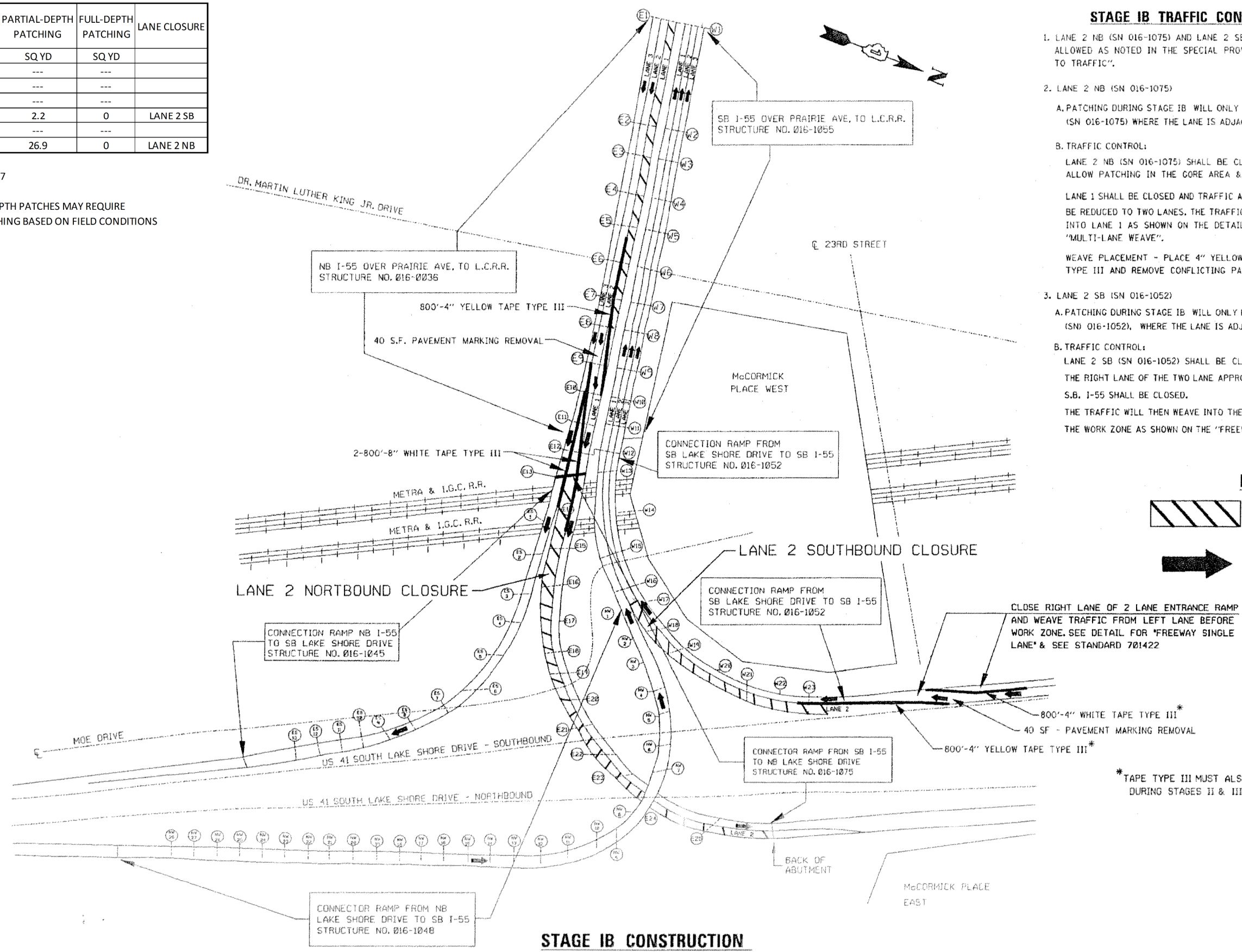
* SPAN SP 1 TO SP 7

NOTE:



SOME PARTIAL-DEPTH PATCHES MAY REQUIRE FULL-DEPTH PATCHING BASED ON FIELD CONDITIONS

STAGE IB TRAFFIC CONTROL NOTES

- LANE 2 NB (SN 016-1075) AND LANE 2 SB (SN 016-1052) CLOSURES WILL BE ALLOWED AS NOTED IN THE SPECIAL PROVISION "KEEPING EXPRESSWAY OPEN TO TRAFFIC".
- LANE 2 NB (SN 016-1075)
 - PATCHING DURING STAGE IB WILL ONLY BE ALLOWED IN THE RIGHT LANE OF (SN 016-1075) WHERE THE LANE IS ADJACENT TO THE PARAPET WALL.
 - TRAFFIC CONTROL:
 - LANE 2 NB (SN 016-1075) SHALL BE CLOSED AS SHOWN. LOCATE WEAVE TO ALLOW PATCHING IN THE GORE AREA & LANE 2 ADJACENT TO THE GORE AREA.
 - LANE 1 SHALL BE CLOSED AND TRAFFIC APPROACHING THE WORK ZONE SHALL BE REDUCED TO TWO LANES. THE TRAFFIC IN LANE 2 WILL THEN BE WEAVED INTO LANE 1 AS SHOWN ON THE DETAIL FOR "FREEWAY SINGLE LANE" & "MULTI-LANE WEAVE".
 - WEAVE PLACEMENT - PLACE 4" YELLOW AND 8" WHITE PAVEMENT MARKING TAPE, TYPE III AND REMOVE CONFLICTING PAVEMENT MARKINGS.
- LANE 2 SB (SN 016-1052)
 - PATCHING DURING STAGE IB WILL ONLY BE ALLOWED IN THE LEFT LANE OF (SN 016-1052), WHERE THE LANE IS ADJACENT TO THE PARAPET WALL.
 - TRAFFIC CONTROL:
 - LANE 2 SB (SN 016-1052) SHALL BE CLOSED AS SHOWN.
 - THE RIGHT LANE OF THE TWO LANE APPROACH TO RAMP FROM S.B. U.S. 41 TO S.B. I-55 SHALL BE CLOSED.
 - THE TRAFFIC WILL THEN WEAVE INTO THE RIGHT LANE (LANE 3) OF THE RAMP BEFORE THE WORK ZONE AS SHOWN ON THE "FREEWAY SINGLE LANE WEAVE" DETAIL.



LEGEND:

-  WORK ZONE OR LANE CLOSURE
-  TRAFFIC DIRECTION

* TAPE TYPE III MUST ALSO BE PLACED DURING STAGES II & IIIA

STAGE IB CONSTRUCTION

STAGE IIIB DECK PATCHING SCHEDULE

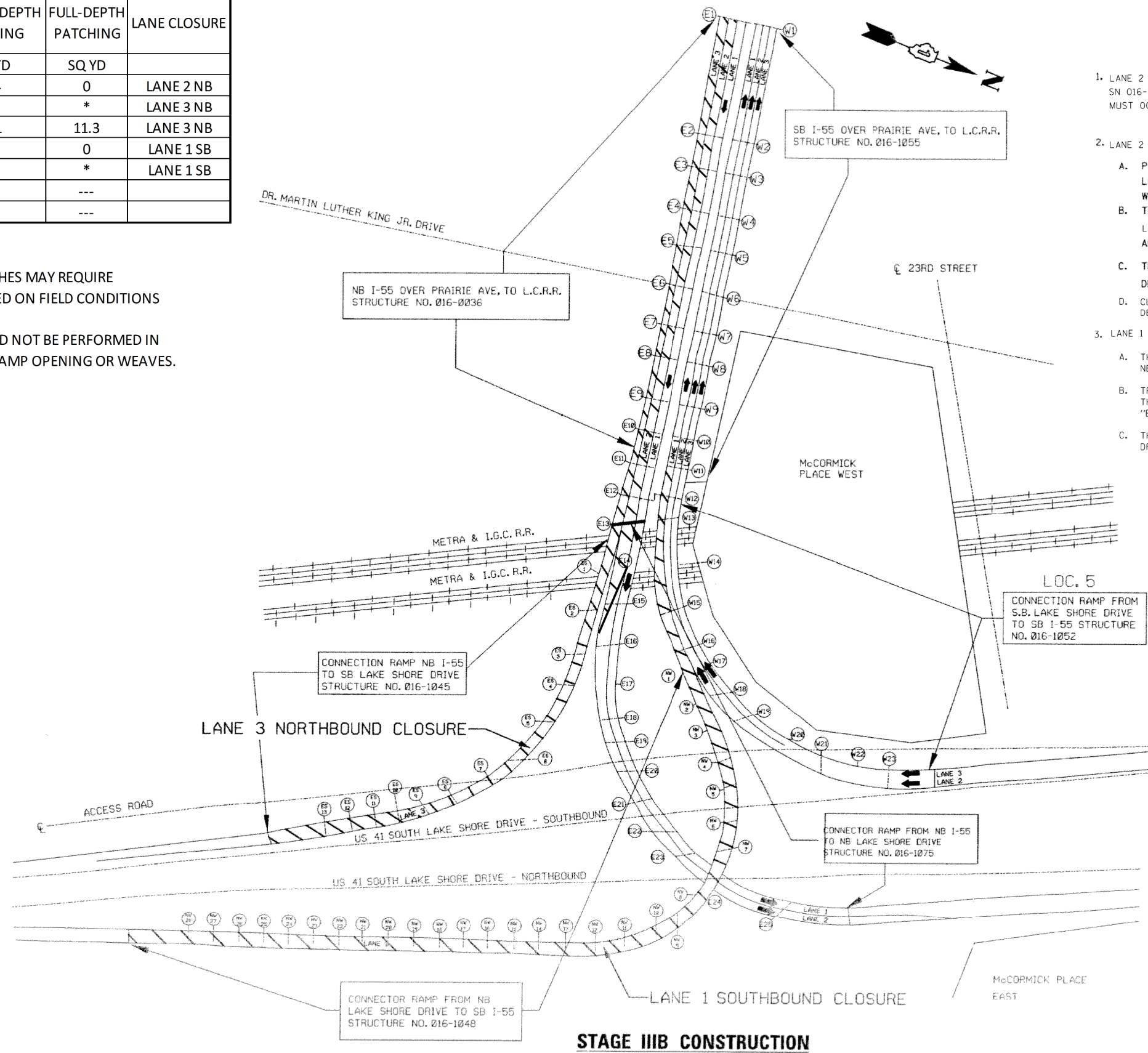
| STRUCTURE NO. | LANE NO. | PARTIAL-DEPTH PATCHING | FULL-DEPTH PATCHING | LANE CLOSURE |
|---------------|----------|------------------------|---------------------|--------------|
| | | SQ YD | SQ YD | |
| 016-0036 | 2 | 9.4 | 0 | LANE 2 NB |
| 016-0036 | 3 | * | * | LANE 3 NB |
| 016-1045 | RAMP | 131 | 11.3 | LANE 3 NB |
| 016-1048 | RAMP | 78 | 0 | LANE 1 SB |
| 016-1052 | 1 | * | * | LANE 1 SB |
| 016-1055 | --- | --- | --- | |
| 016-1075 | --- | --- | --- | |

NOTE:
SOME PARTIAL-DEPTH PATCHES MAY REQUIRE FULL-DEPTH PATCHING BASED ON FIELD CONDITIONS


*ANY PATCHING THAT COULD NOT BE PERFORMED IN PREVIOUS STAGES DUE TO RAMP OPENING OR WEAVES.


STAGE IIIB TRAFFIC CONTROL NOTES

- LANE 2 NB & LANE 3 NB (SN 016-0036 & SN 016-1045) AND LANE 1 SB (SN 016-1048 & SN 016-1052) CLOSURES WILL BE ALLOWED, HOWEVER, THESE LANE CLOSURES MUST OCCUR ON SEPARATE WEEKENDS.
- LANE 2 & LANE 3 NB (SN 016-0036) & RAMP NB TO SB U.S. 41 (SN 016-1045):
 - PATCHING DURING THIS STAGE WILL BE DONE ON THE RAMP AND ANY REMAINING LOCATIONS IN LANE 3. ALSO DURING THIS STAGE THE JACKS FOR THE PIER REPAIRS WILL BE REMOVED.
 - TRAFFIC CONTROL:
LANE 2 & LANE 3 WILL BE CLOSED USING STANDARDS 701400 & 701401. IN ADDITION, THE APPROPRIATE EXIT RAMP CLOSED SIGN SHALL BE INSTALLED.
 - THE RAMP TRAFFIC SHALL BE DETOURED AS SHOWN ON THE "DETOUR PLAN - STAGE IIIB" DRAWING.
 - CLOSE ENTRANCE RAMP FROM STATE ST. TO NB I-55. SEE ENTRANCE RAMP CLOSURE DETAIL AND DETOUR
- LANE 1 CLOSURE SB I-55 & NB U.S. 41 RAMP:
 - THIS WORK SHALL CONSIST OF THE CLOSURE OF LANE 1 SB AND THE RAMP FROM NB U.S. 41 (SN 016-1048).
 - TRAFFIC CONTROL:
THE ENTRANCE OF THE NB U.S. 41 RAMP SHALL BE CLOSED AS SHOWN ON THE "ENTRANCE AND EXIT RAMP CLOSURE DETAIL".
 - THE RAMP TRAFFIC SHALL BE DETOURED AS SHOWN ON THE "DETOUR PLAN - STAGE IIIB" DRAWING



LEGEND:

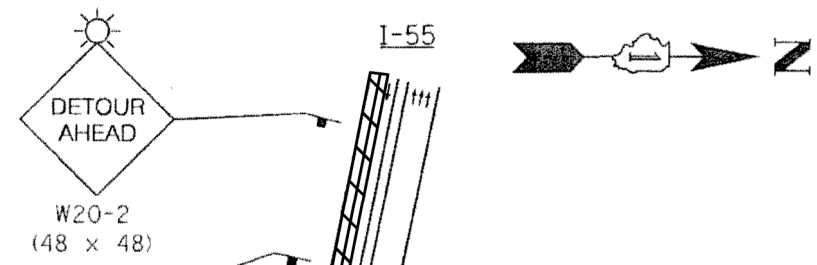
 WORK ZONE OR LANE CLOSURE

 TRAFFIC DIRECTION

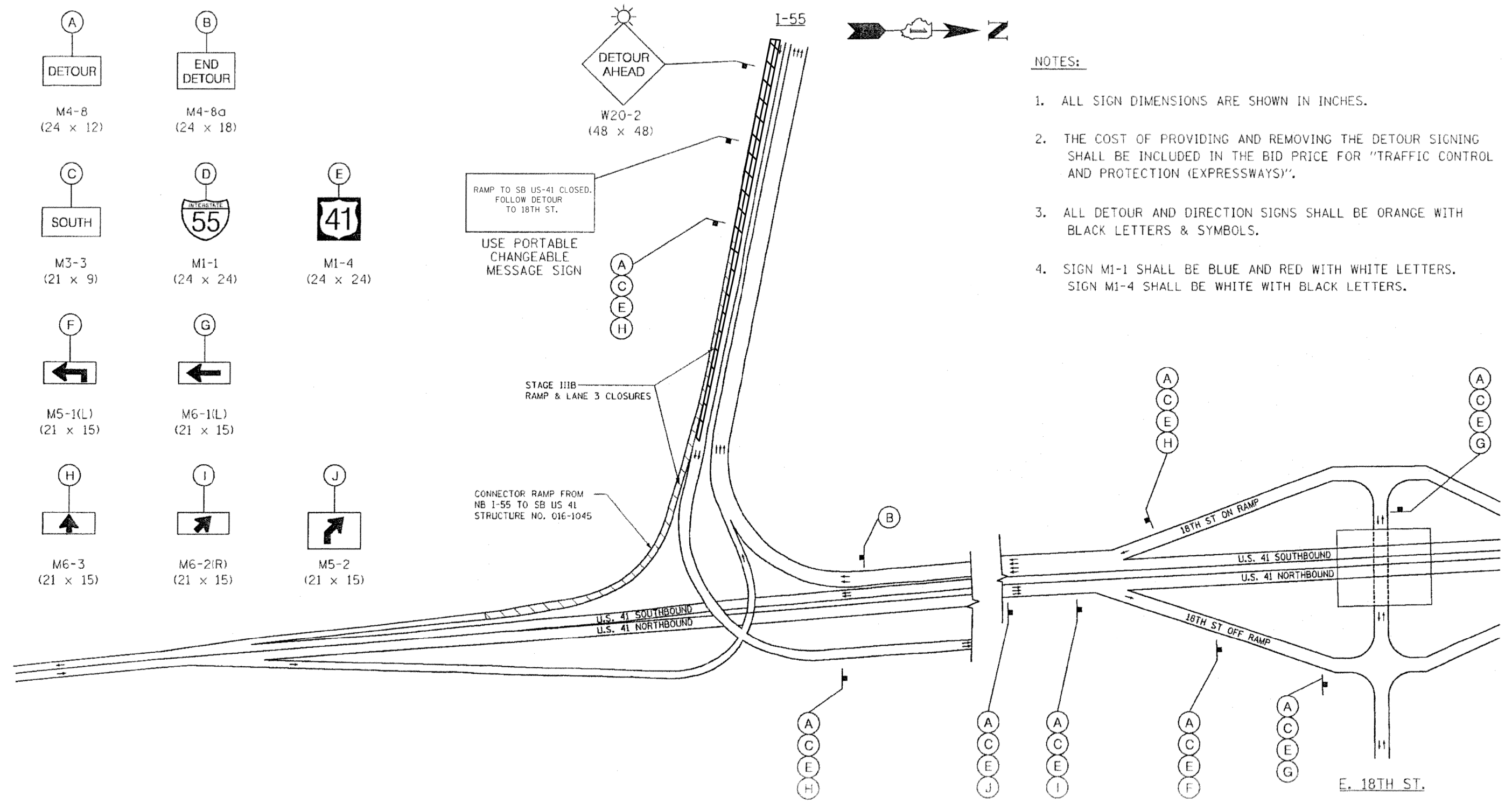
STAGE IIIB CONSTRUCTION

| | | | | | | | | | | | | | |
|---|-----------------------------|------------|-----------|---|--|------------|------|----|--------------------|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = osbornep | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SUGGESTED STAGE IIIB CONSTRUCTION I-55 (STEVENSON EXPRESSWAY) AT US-41 (LAKE SHORE DRIVE) | | | | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ci:\pw_work\pwidat\osbornep\d0382528\016414-sht-cover.dgn | PLOT SCALE = 2.0000' / 1in. | DRAWN - | REVISED - | | 55 | 2014-018BR | COOK | 33 | 9 | | | | |
| Default | PLOT DATE = 4/1/2014 | CHECKED - | REVISED - | | SCALE: SHEET OF SHEETS STA. TO STA. | | | | CONTRACT NO. 60X91 | | | | |
| | | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | | |

- A**
DETOUR
M4-8
(24 x 12)
- B**
END
DETOUR
M4-8a
(24 x 18)
- C**
SOUTH
M3-3
(21 x 9)
- D**
INTERSTATE
55
M1-1
(24 x 24)
- E**
41
M1-4
(24 x 24)
- F**
←
M5-1(L)
(21 x 15)
- G**
←
M6-1(L)
(21 x 15)
- H**
↑
M6-3
(21 x 15)
- I**
↗
M6-2(R)
(21 x 15)
- J**
↗
M5-2
(21 x 15)

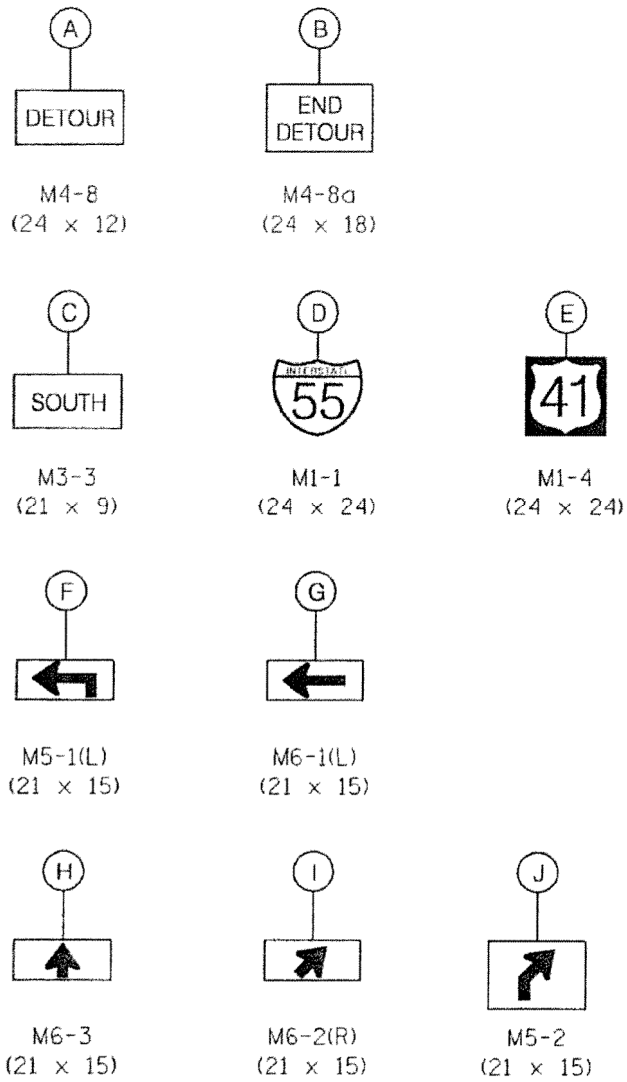


- NOTES:**
- ALL SIGN DIMENSIONS ARE SHOWN IN INCHES.
 - THE COST OF PROVIDING AND REMOVING THE DETOUR SIGNING SHALL BE INCLUDED IN THE BID PRICE FOR "TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)".
 - ALL DETOUR AND DIRECTION SIGNS SHALL BE ORANGE WITH BLACK LETTERS & SYMBOLS.
 - SIGN M1-1 SHALL BE BLUE AND RED WITH WHITE LETTERS. SIGN M1-4 SHALL BE WHITE WITH BLACK LETTERS.



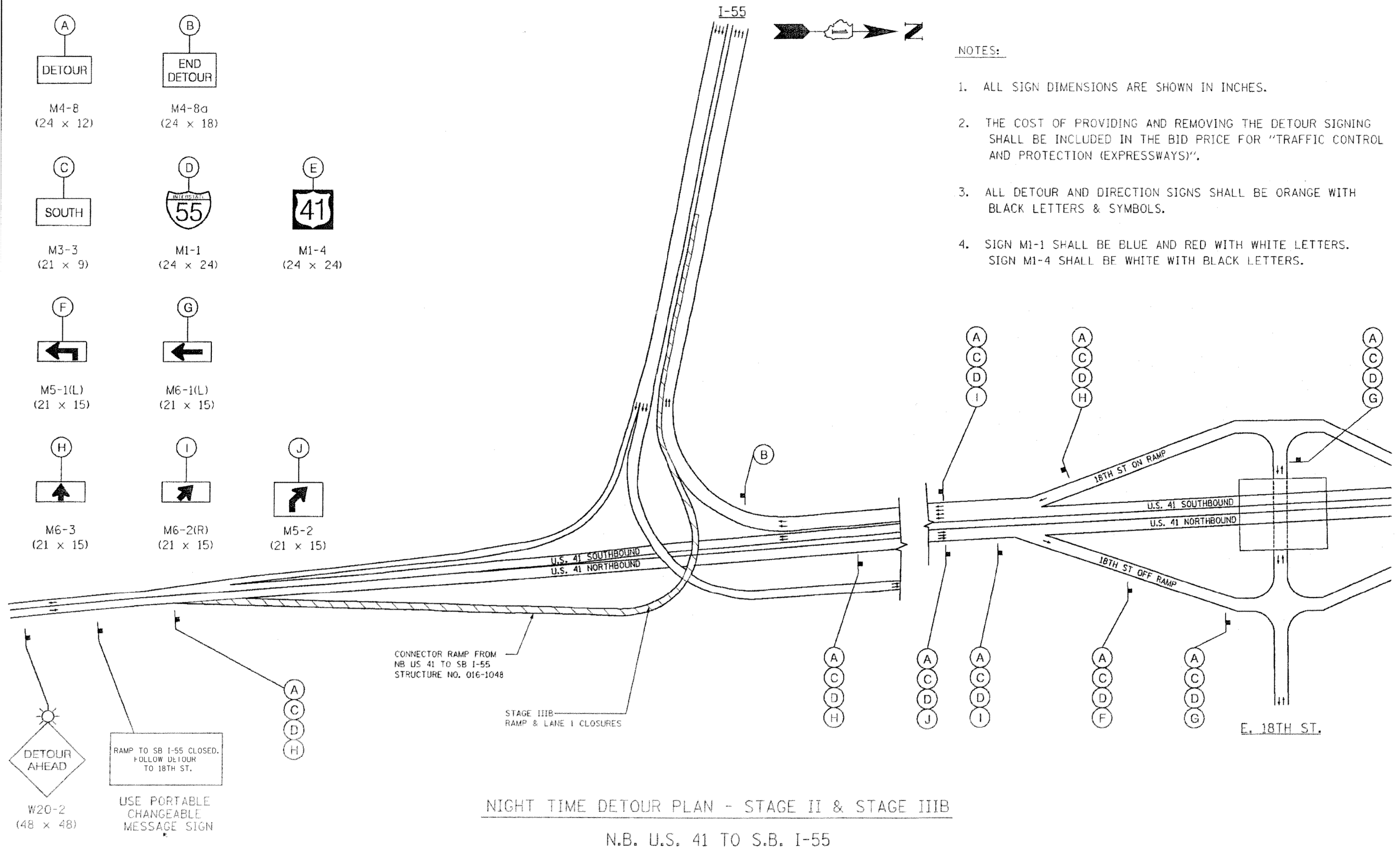
DETOUR STAGE IIIIB
N.B. I-55 TO S.B. U.S. 41

| | | | | | | | | | | | | |
|---|----------------------|------------|-----------|---|--|-------|-----------|-------------|------------|---------------------------|--------------|-----------|
| FILE NAME = | USER NAME = osbornnp | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | DETOUR PLAN - STAGE IIIIB | | | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ci:\p\work\p\idot\osbornnp\d0382528\016414-sh.t:cover.dgn | | DRAWN - | REVISED - | | I-55 (STEVENSON EXPRESSWAY) AT US-41 (LAKE SHORT DRIVE) | | | 55 | 2014-018BR | COOK | 33 | 11 |
| Default | | CHECKED - | REVISED - | | SCALE: | SHEET | OF SHEETS | STA. | TO STA. | CONTRACT NO. 60X91 | | |
| | PLOT DATE = 4/1/2014 | DATE - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | |

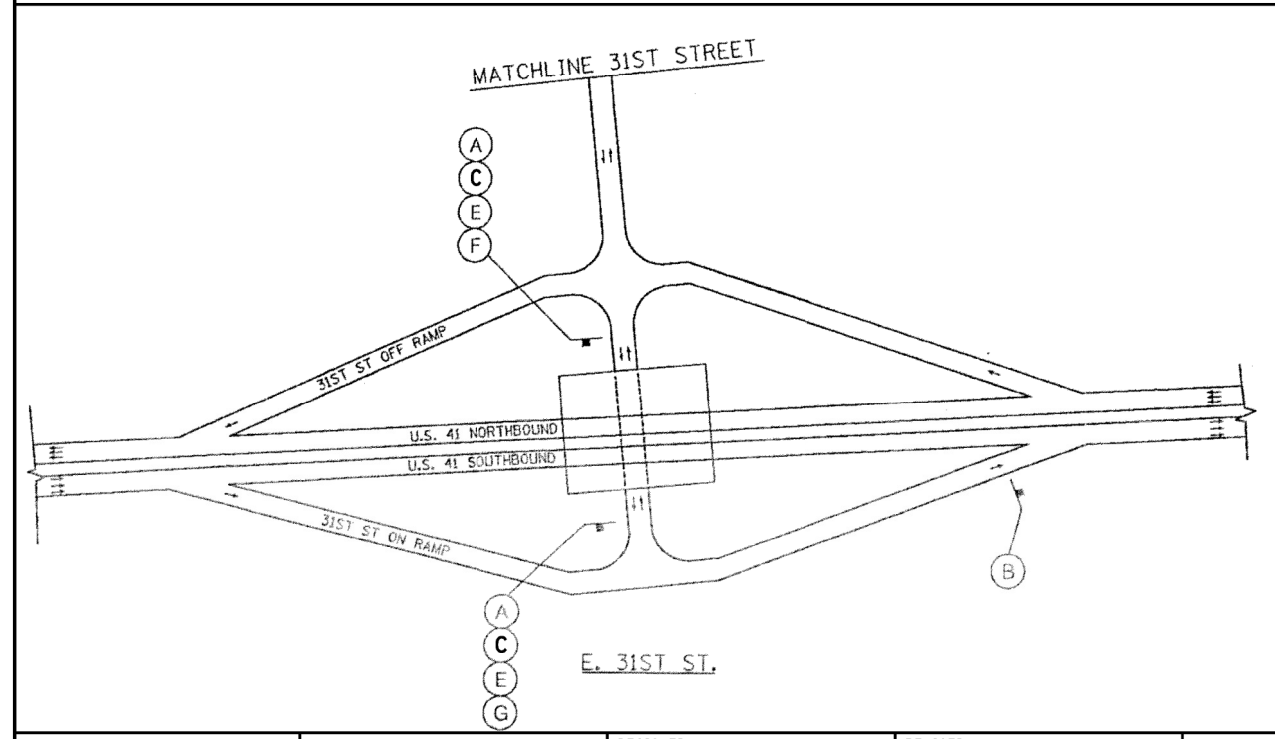
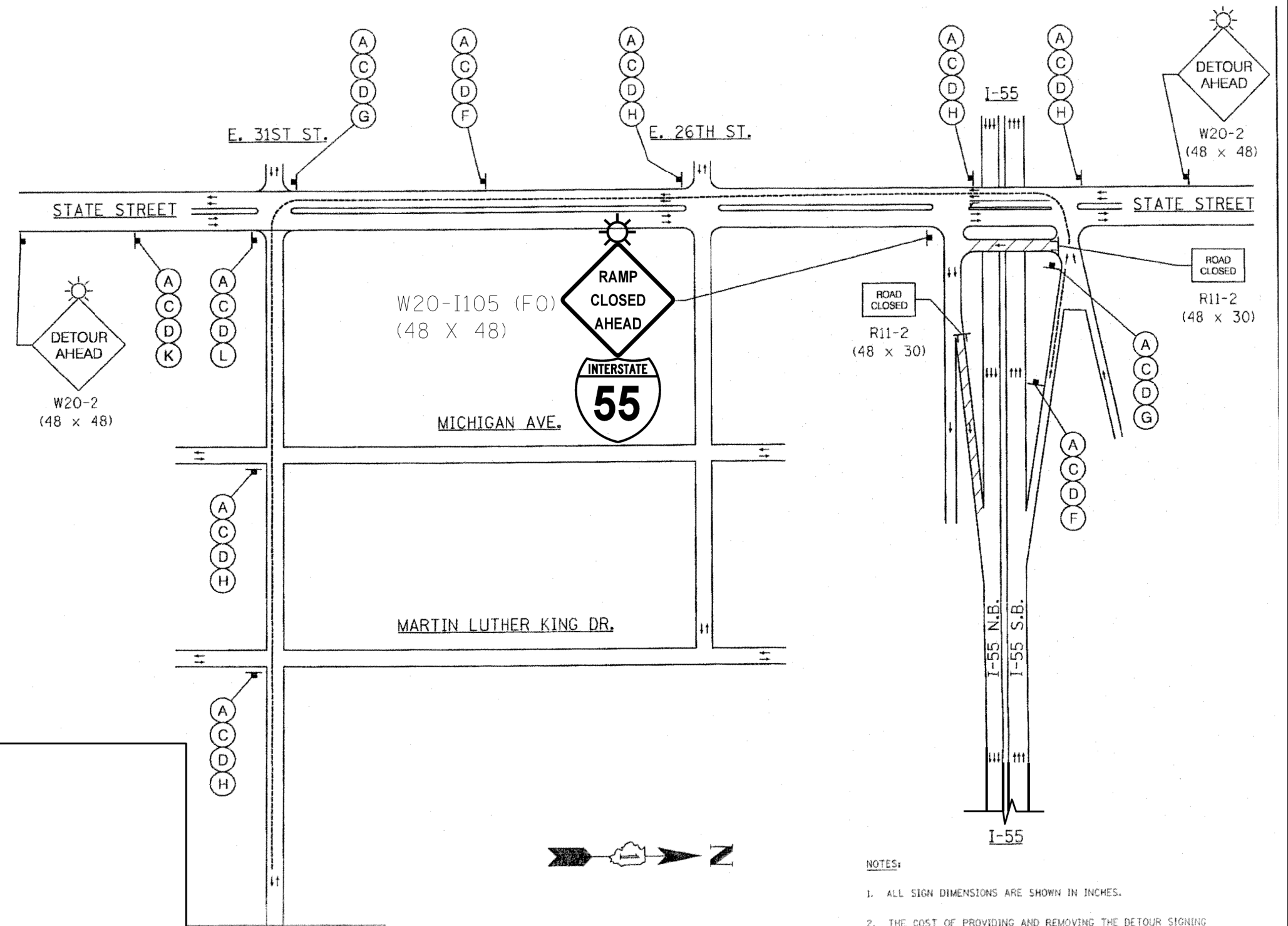
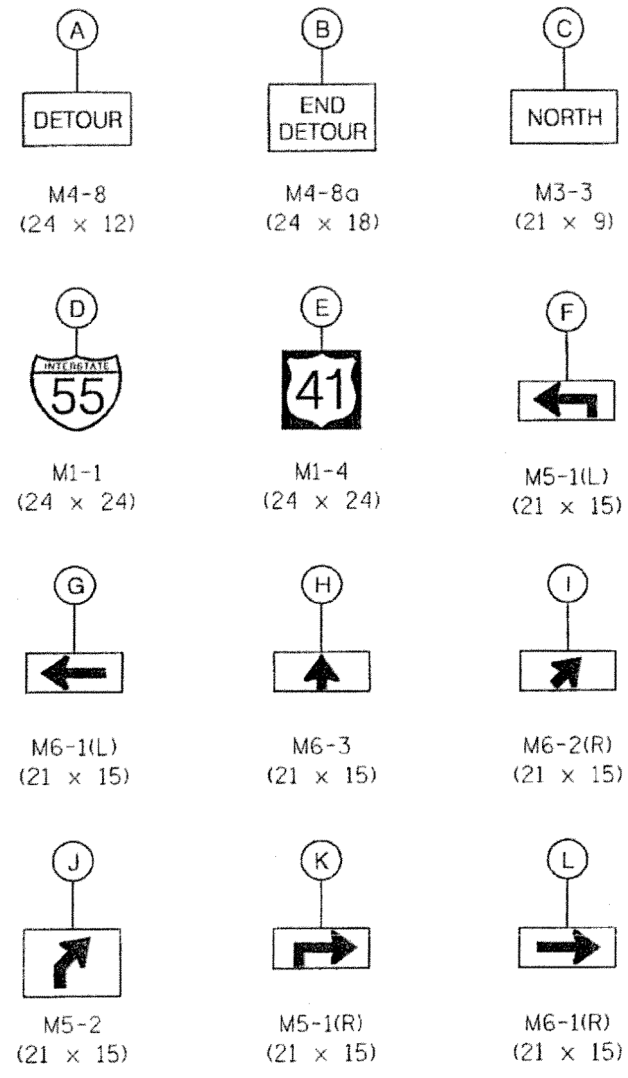


NOTES:

1. ALL SIGN DIMENSIONS ARE SHOWN IN INCHES.
2. THE COST OF PROVIDING AND REMOVING THE DETOUR SIGNING SHALL BE INCLUDED IN THE BID PRICE FOR "TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)".
3. ALL DETOUR AND DIRECTION SIGNS SHALL BE ORANGE WITH BLACK LETTERS & SYMBOLS.
4. SIGN M1-1 SHALL BE BLUE AND RED WITH WHITE LETTERS. SIGN M1-4 SHALL BE WHITE WITH BLACK LETTERS.



| | | | | | | | | | | | |
|--|----------------------------|------------|-----------|---|--|---------------------------|------------|--------|--------------|-----------|--|
| FILE NAME = | USER NAME = osbornnp | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | NIGHT TIME DETOUR PLAN - STAGE II & STAGE IIIB I-55 (STEVENS ON EXPRESSWAY) AT US-41 (LAKE SHORT DRIVE) | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| ci:\p\work\p\idot\osbornnp\d0382528\016414-shr-cover.dgn | PLOT SCALE = 2.0000' / in. | DRAWN - | REVISED - | | | 55 | 2014-018BR | COOK | 33 | 12 | |
| Default | PLOT DATE = 4/1/2014 | CHECKED - | REVISED - | | | CONTRACT NO. 60X91 | | | | | |
| | | DATE - | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | | |



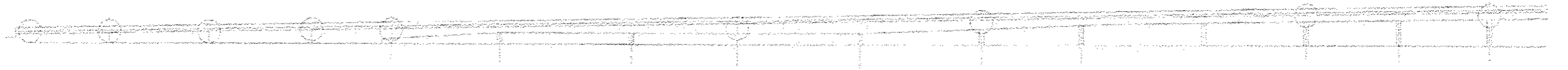
STATE STREET RAMP DETOUR
FOR STAGE II & IIIB
N.B. I-55 TO N.B. U.S. 41

- NOTES:
1. ALL SIGN DIMENSIONS ARE SHOWN IN INCHES.
 2. THE COST OF PROVIDING AND REMOVING THE DETOUR SIGNING SHALL BE INCLUDED IN THE BID PRICE FOR "TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)".
 3. ALL DETOUR AND DIRECTION SIGNS SHALL BE ORANGE WITH BLACK LETTERS & SYMBOLS.
 4. SIGN M1-1 SHALL BE BLUE AND RED WITH WHITE LETTERS. SIGN M1-4 SHALL BE WHITE WITH BLACK LETTERS.

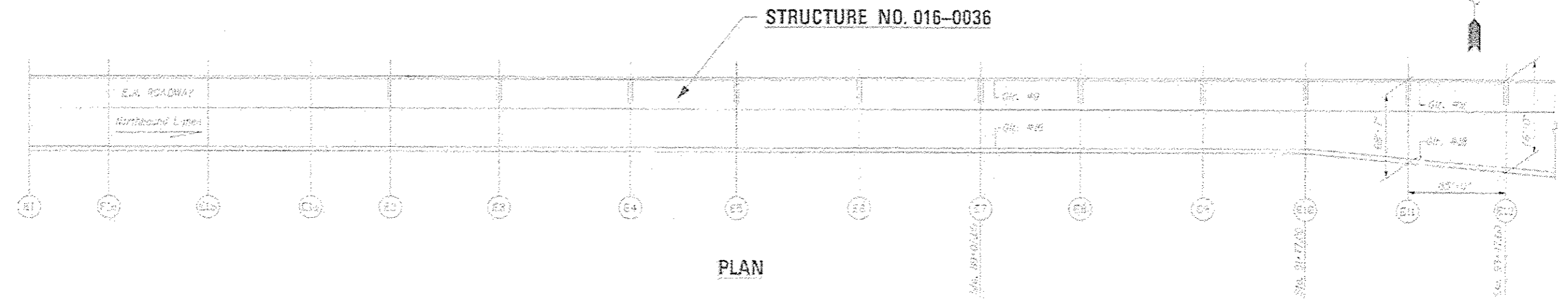
| | | | | | | | | | | | | | |
|-------------|---|------------|-----------|---|---|-------|----|--------|------------------|----------------------|---------------|---------------------------|----------------|
| FILE NAME = | USER NAME = osbornenp | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | STATE STREET RAMP DETOUR I-55 (STEVENSON EXPRESSWAY) AT US-41 (LAKE SHORT DRIVE) | | | | F.A.I. RTE. = 55 | SECTION = 2014-018BR | COUNTY = COOK | TOTAL SHEETS = 33 | SHEET NO. = 13 |
| Default | ci:\p\work\p\idot\osbornenp\d0382528\0186414-shit-cover.dgn | DRAWN - | REVISED - | | SCALE: | SHEET | OF | SHEETS | STA. | TO | STA. | CONTRACT NO. 60X91 | |
| | PLOT SCALE = 2.0000" / in. | CHECKED - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | | |
| | PLOT DATE = 4/1/2014 | DATE - | REVISED - | | | | | | | | | | |

SN 016-0036 (N.B.)
SN 016-0037 (N.B.)

SN 016-0036 (N.B.)



ELEVATION - E.N. ROADWAY



STRUCTURE NO. 016-0036

PLAN

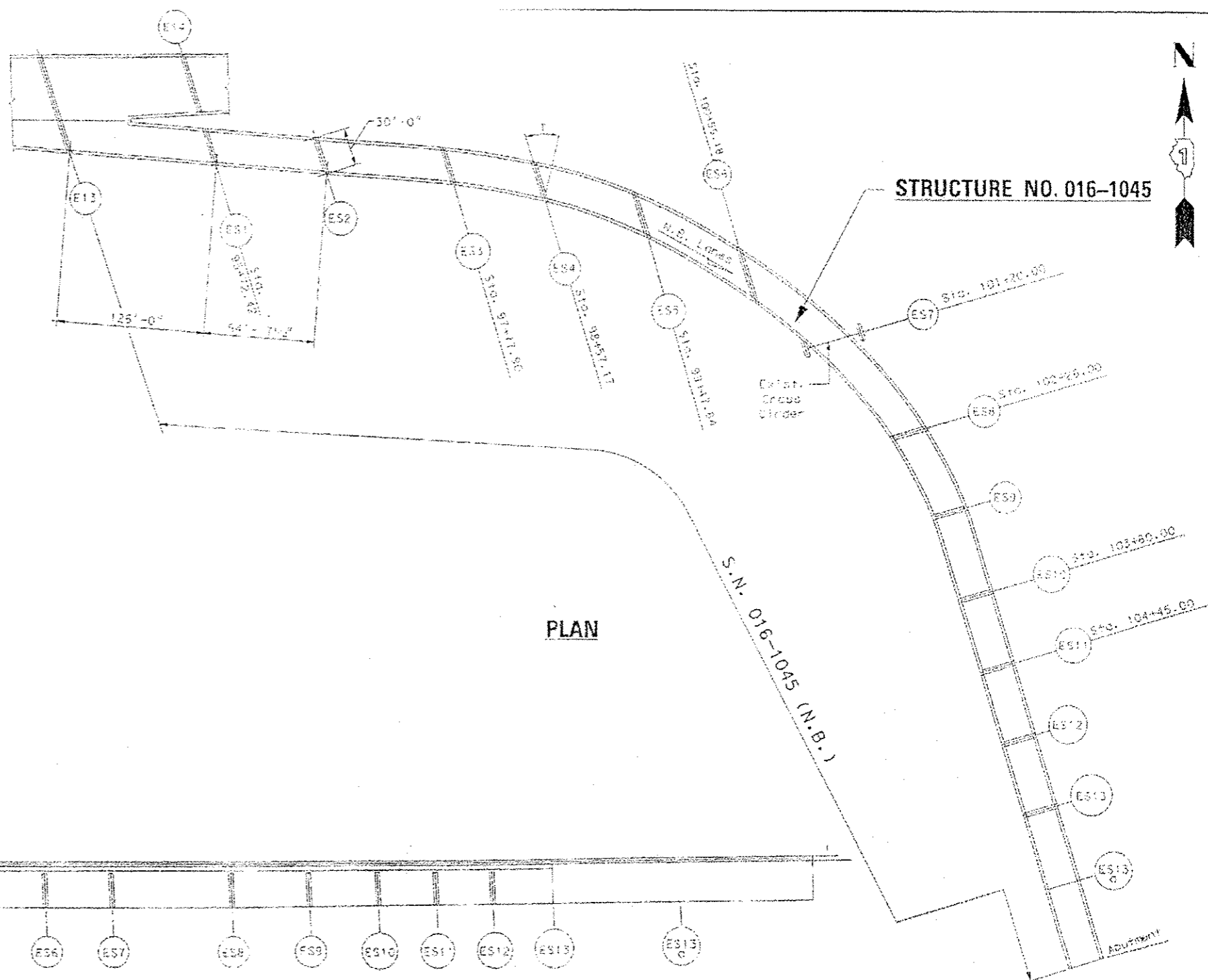
FOR INFORMATION ONLY



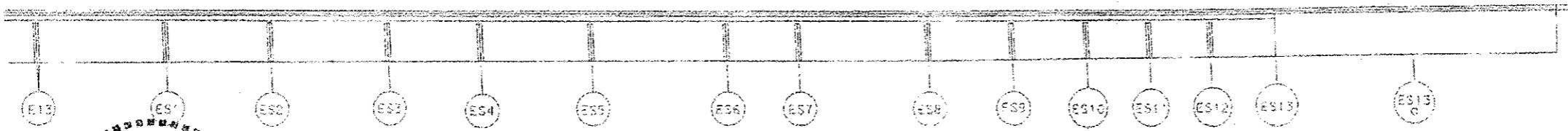
David Carl Puzey 3/27/14
Expires 11/30/14

GENERAL PLAN & ELEVATION
E.A.I. RTE. I-55 OVER
U.S. 41/LAKE SHORE DRIVE
COOK COUNTY
STRUCTURE NO. 016-0036

| | | | | | | | | | | | | | | | | |
|-----------|-----------|----------|---------|---|---|--------|-------|----|--------|------|---------|---------------------------|------------|--------|--------------|-----------|
| FILE NAME | USER NAME | DESIGNED | REVISED | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | GENERAL PLAN & ELEVATION SN 016-0036 I-55 (STEVENSON EXPRESSWAY) AT US-41 (LAKE SHORT DRIVE) | SCALE: | SHEET | OF | SHEETS | STA. | TO STA. | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | DRAWN | REVISED | | | | | | | | | 55 | 2014-0188R | COOK | 33 | 14 |
| | | CHECKED | REVISED | | | | | | | | | CONTRACT NO. 60X91 | | | | |
| | | DATE | REVISED | | | | | | | | | ILLINOIS FED. AID PROJECT | | | | |



PLAN



ELEVATION-RAMP E.S.

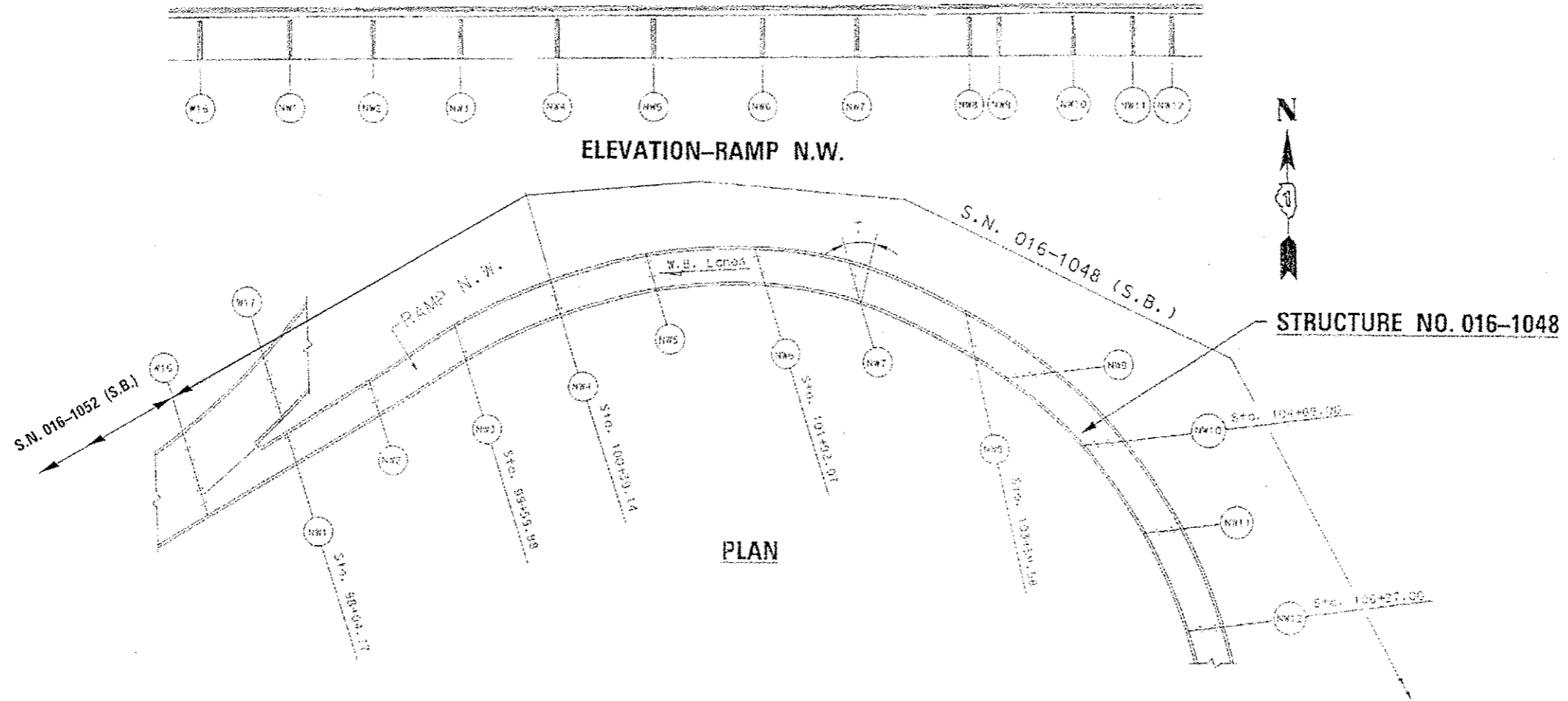
FOR INFORMATION ONLY

GENERAL PLAN & ELEVATION
 F.A.I. RTE. I-55 OVER
 U.S. 41/LAKE SHORE DRIVE
 COOK COUNTY
 STRUCTURE NO. 016-1045

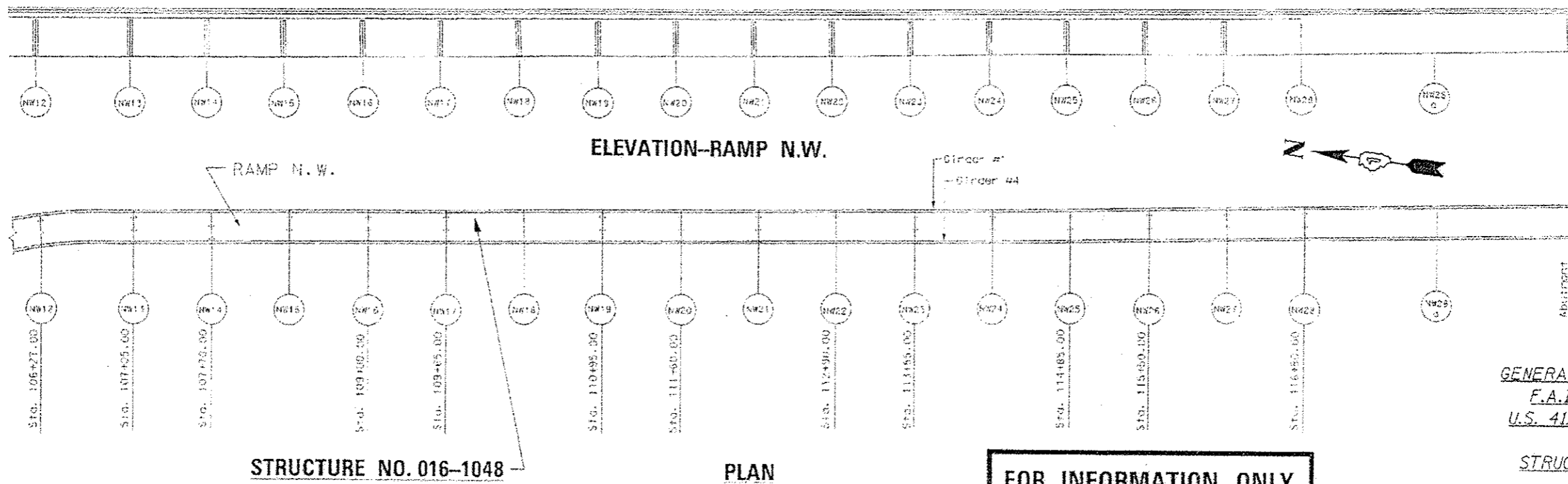


David Carl Puzey 3/27/14
 Expires 11/30/14

| | | | | | | | | | | | |
|-----------|--------------------------|-----------------------|-----------|---|---|---------------------------|------------|--------|--------------|-----------|--|
| FILE NAME | USER NAME - osbornenp | DESIGNED - <i>JAM</i> | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | GENERAL PLAN AND ELEVATION - S.N. 016-1045 I-55 (STEVENSON EXPRESSWAY) AT US-41 (LAKE SHORT DRIVE) | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| | | DRAWN - <i>ER</i> | REVISED - | | | 55 | 2014-018BR | COOK | 33 | 17 | |
| | PLT SCALE - 2.0000' / 1" | CHECKED - <i>CR</i> | REVISED - | | | CONTRACT NO. 60X91 | | | | | |
| | PLT DATE - 3/28/2014 | DATE - | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | | |



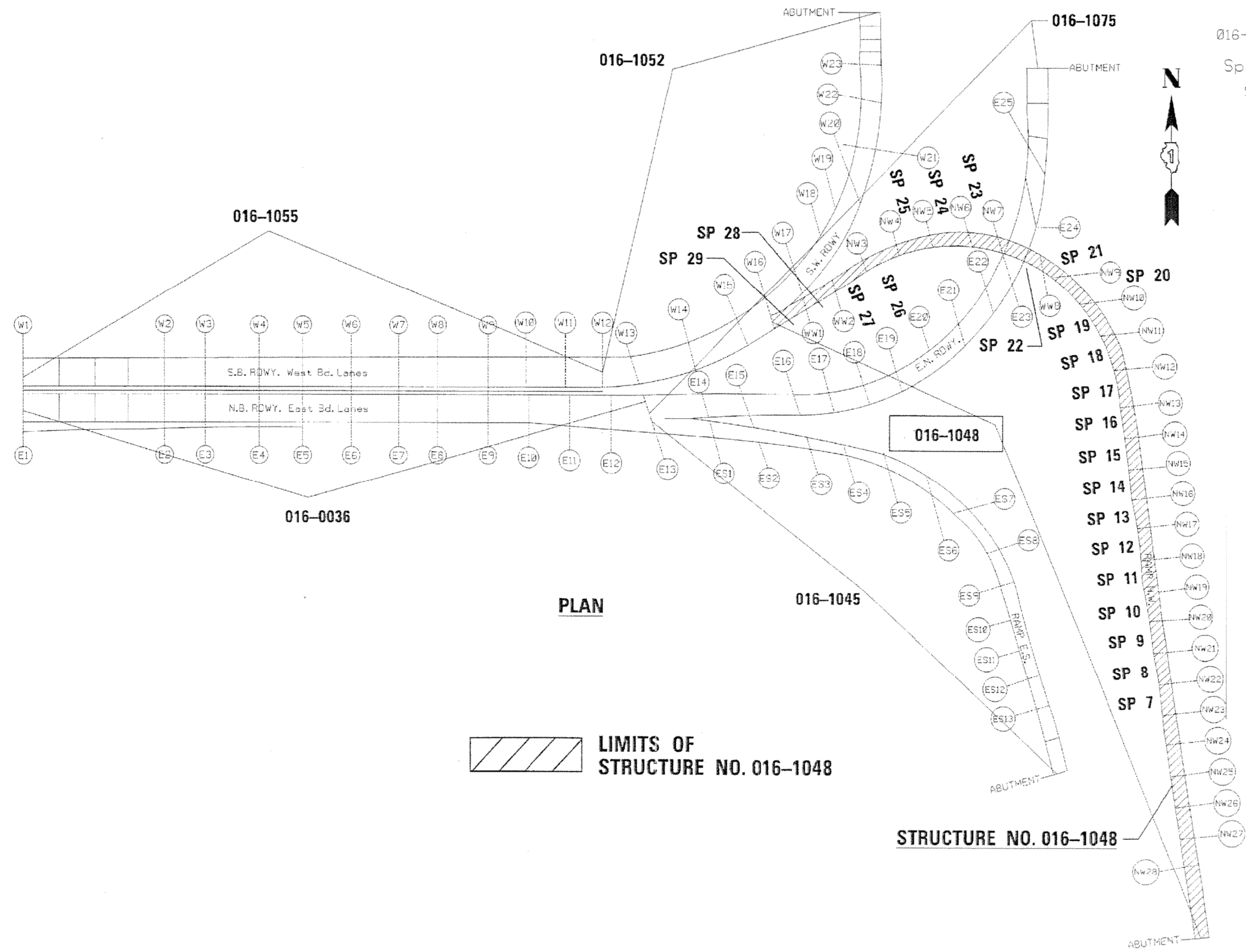
David Carl Puzey 3/27/14
Expires 11/30/14



FOR INFORMATION ONLY

GENERAL PLAN & ELEVATION
 F.A.I. RTE. I-55 OVER
 U.S. 41/LAKE SHORE DRIVE
 COOK COUNTY
 STRUCTURE NO. 016-1048

| | | | | | | | | | | | | |
|--|--|-----------------------|-----------|---|---|-------|-------------------|-----------------------|----------------|--------------------|-----------------|--------------------|
| FILE NAME : c:\p-work\p106\ashorncorp\0302528\0302528.dgn | USER NAME : ashorncorp 6414-ashl.covm.dgn | DESIGNED - JAN | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | GENERAL PLAN AND ELEVATION - S.N. 016-1048 I-55 (STEVENSON EXPRESSWAY) AT US-41 (LAKE SHORT DRIVE) | | F.A.I. RTE. 55 | SECTION 2014-018BR | COUNTY COOK | TOTAL SHEETS 33 | SHEET NO. 19 | |
| Default | PLOT SCALE = 2.0000 1/2 in. | CHECKED - JA | REVISED - | | SCALE: | SHEET | OF | SHEETS | STA. | TO | STA. | CONTRACT NO. 60X91 |
| | PLOT DATE = 3/28/2014 | DATE | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | |
| | | | REVISED - | | | | | | | | | |



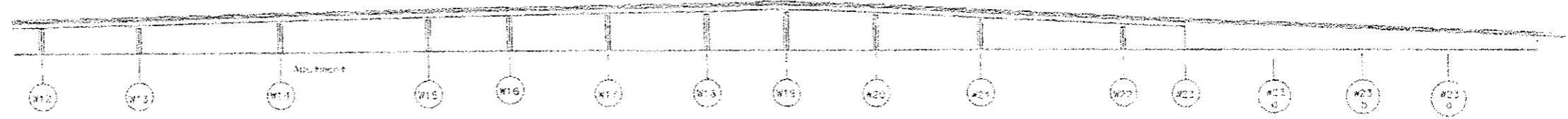
016-1048 US 41 N.B. Ramp to I-55 S.B.
 Spans are numbered from the South
 Span 7 between Pier NW23 & NW22

FAI 55 (STEVENSON EXPRESSWAY)
STRUCTURE NO.: 016-1048
BRIDGE DECK REPAIRS

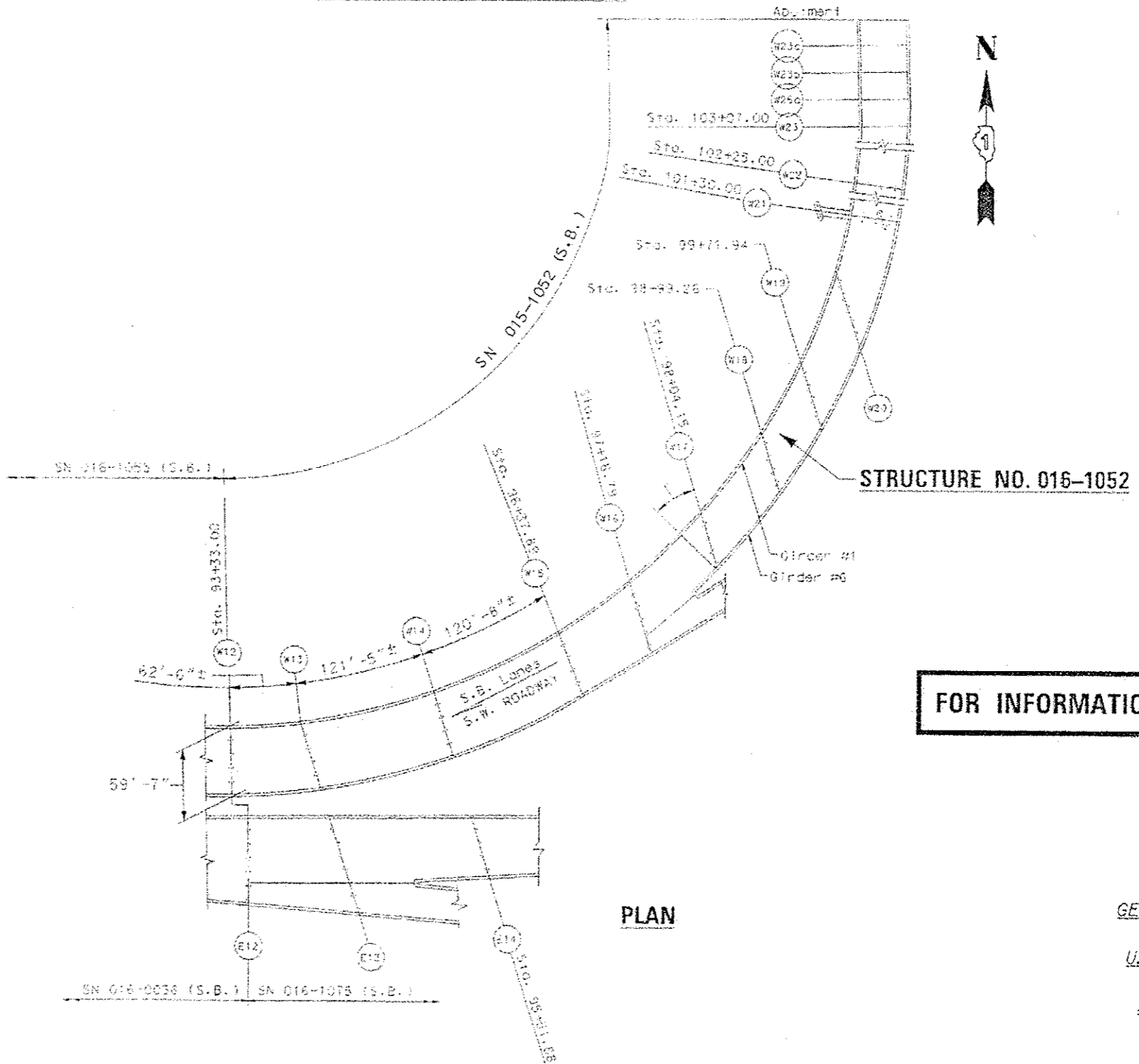
| Span # | Ramp Lane | |
|--------------|----------------------------|------|
| | Partial | Full |
| 1 | 0 | |
| 2 | 0 | |
| 3 | 0 | |
| 4 | 0 | |
| 5 | 3x3, 10x10 = 109 sf | |
| 6 | 8x8, 3x4 = 76 sf | |
| 7 | 0 | |
| 8 | 0 | |
| 9 | 0 | |
| 10 | 0 | |
| 11 | 4x6, 6x6 = 60sf | |
| 12 | 3x4, 2x3 = 18 sf | |
| 13 | 0 | |
| 14 | 0 | |
| 15 | 3x3, 5x5 = 34 sf | |
| 16 | 2x3, 10x10 = 106 sf | |
| 17 | 0 | |
| 18 | 2 @ 2x2 = 8 sf | |
| 19 | 5x5 = 25 sf | |
| 20 | 5x5, 4x4, 6x5 = 71 | |
| 21 | 4x4 = 16 sf | |
| 22 | 4x6, 4x4, 2 @ 6x6 = 112 sf | |
| 23 | 3x3, 2x2, 8x6 = 61 sf | |
| 24 | 3x3 = 9 sf | |
| 25 | 0 | |
| 26 | 0 | |
| 27 | 0 | |
| 28 | 0 | |
| 29 | 0 | |
| TOTAL | 705 SF | |

PARTIAL DEPTH GRAND TOTAL 705 SF = 78 SY
FULL DEPTH GRAND TOTAL 71 SF = 7.9 SY

I-55 @ Lake Shore Drive



ELEVATION - S.B. ROADWAY



STRUCTURE NO. 016-1052

FOR INFORMATION ONLY

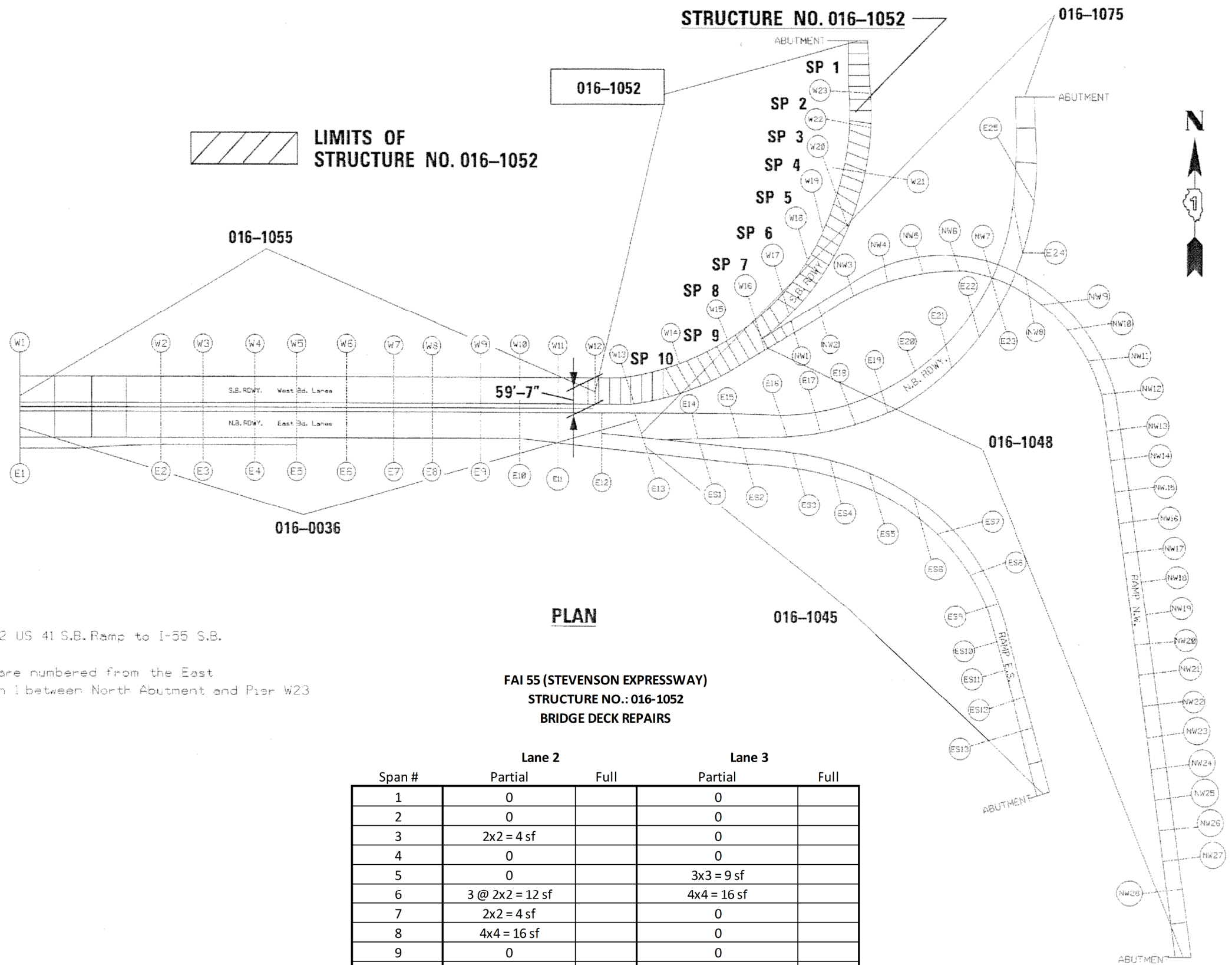
PLAN



David Carl Puzey 3/27/14
Expires 11/30/14

GENERAL PLAN & ELEVATION
E.A.I. RTE. I-55 OVER
U.S. 41/LAKE SHORE DRIVE
COOK COUNTY
STRUCTURE NO. 016-1052

| | | | | | | | | | | | | |
|-------------|------------------------------|----------------|-----------|---|---|-------------------|----------------|---------------------------|-----------|----------|----------------|-------------|
| FILE NAME : | USER NAME : asbarnep | DESIGNED : JAM | REVISED : | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | GENERAL PLAN AND ELEVATION - S.N. 016-1052 | | | F.A.I. RTE. : | SECTION : | COUNTY : | TOTAL SHEETS : | SHEET NO. : |
| Default | PROJECT : 2014-0188R | DRAWN : IR | REVISED : | | 55 | 2014-0188R | COOK | 33 | 21 | | | |
| | PL01 SCALE : 2.0000 1" = 20' | CHECKED : | REVISED : | | I-55 (STEVENSON EXPRESSWAY) AT US-41 (LAKE SHORT DRIVE) | | | CONTRACT NO. 60X91 | | | | |
| | PL01 DATE : 3/27/2014 | DATE : | REVISED : | | SCALE : | SHEET OF SHEETS : | STA. TO STA. : | ILLINOIS FED. AID PROJECT | | | | |



016-1052 US 41 S.B. Ramp to I-55 S.B.

Spans are numbered from the East
Span 1 between North Abutment and Pier W23

PLAN

**FAI 55 (STEVENSON EXPRESSWAY)
STRUCTURE NO.: 016-1052
BRIDGE DECK REPAIRS**

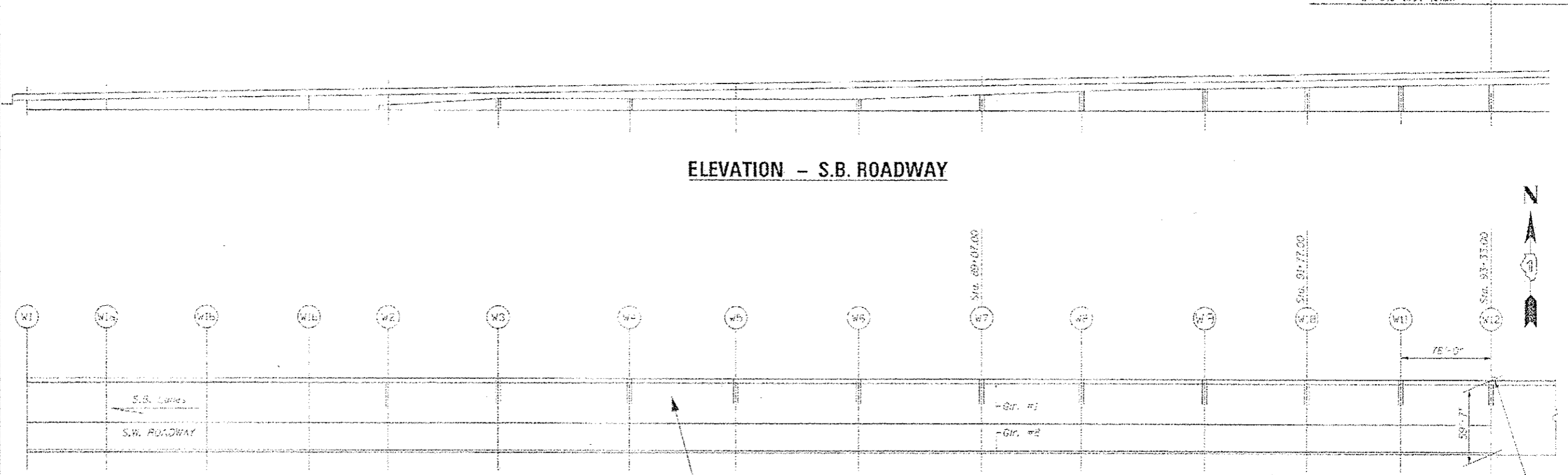
| Span # | Lane 2 | | Lane 3 | |
|--------------|-----------------|------|--------------|------|
| | Partial | Full | Partial | Full |
| 1 | 0 | | 0 | |
| 2 | 0 | | 0 | |
| 3 | 2x2 = 4 sf | | 0 | |
| 4 | 0 | | 0 | |
| 5 | 0 | | 3x3 = 9 sf | |
| 6 | 3 @ 2x2 = 12 sf | | 4x4 = 16 sf | |
| 7 | 2x2 = 4 sf | | 0 | |
| 8 | 4x4 = 16 sf | | 0 | |
| 9 | 0 | | 0 | |
| 10 | 0 | | 0 | |
| TOTAL | 36 SF | | 25 SF | |

PARTIAL DEPTH TOTAL 61 SF = 6.8 SY
FULL DEPTH TOTAL 6 SF = 0.7 SY

SN 016-1045 (S.B.)
SN 016-1052 (S.B.)

SN 016-1055 (S.B.)

ELEVATION - S.B. ROADWAY



STRUCTURE NO. 016-1055

PLAN

FOR INFORMATION ONLY

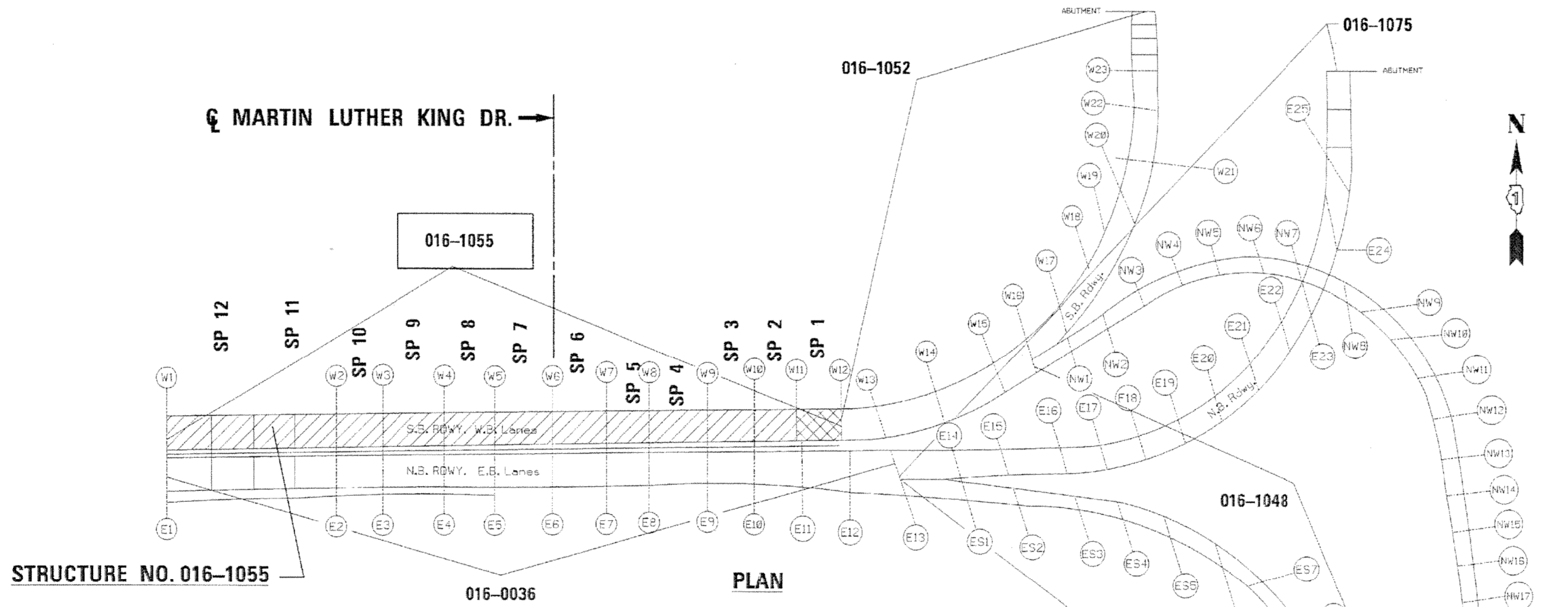
Sign Truss support
Typical at East Side of
Joints of Piers E10 & E12



David Carl Puzey 3/27/14
Expires 11/30/14

GENERAL PLAN & ELEVATION
F.A.I. RTE. I-55 OVER
U.S. 41/LAKE SHORE DRIVE
COOK COUNTY
STRUCTURE NO. 016-1055

| | | | | | | | | | | | | |
|--|-----------------------|----------------|-----------|---|---|-------|--------------------|--------------------|---------------------------|-----------------|--------------|--|
| FILE NAME | USER NAME = osbornemp | DESIGNED - JAM | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | GENERAL PLAN AND ELEVATION - S.N. 016-1055 | | F.A.I. RTE. 55 | SECTION 2014-018BR | COUNTY COOK | TOTAL SHEETS 33 | SHEET NO. 23 | |
| C:\p\work\sp\dot\osbornemp\08382528\016-1055-101.dwg | | DRAWN - ER | REVISED - | | I-55 (STEVENSON EXPRESSWAY) AT US-41 (LAKE SHORT DRIVE) | | CONTRACT NO. 60X91 | | ILLINOIS FED. AID PROJECT | | | |
| PLOT SCALE = 2.0000 1/2" = 1' | | CHECKED - ER | REVISED - | | SCALE: | SHEET | OF | SHEETS | STA. | TO | STA. | |
| PLOT DATE = 3/28/2014 | | DATE | REVISED | | | | | | | | | |



STRUCTURE NO. 016-1055

PLAN

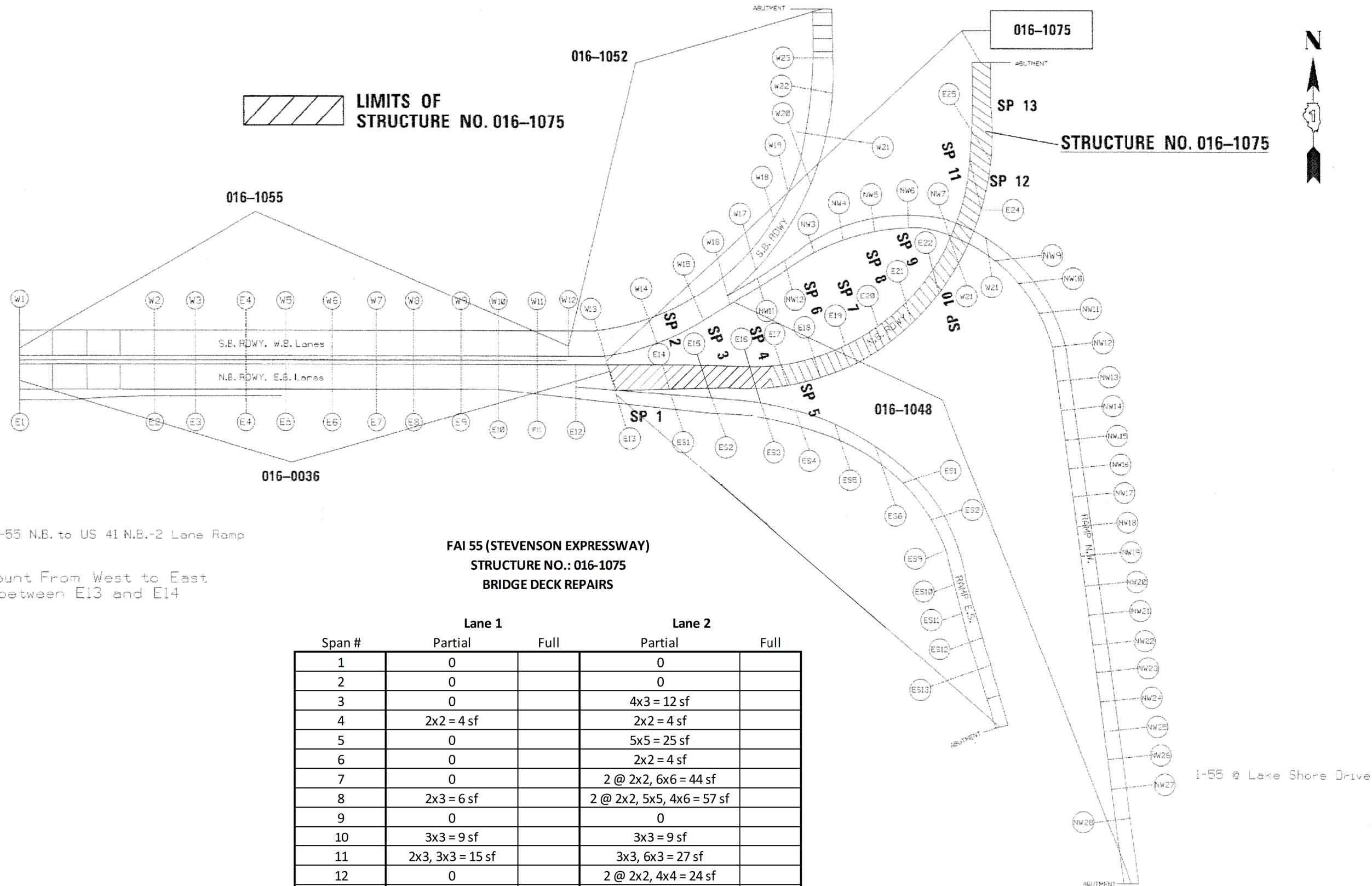
 LIMITS OF STRUCTURE NO. 016-1055

016-1055-I-55 S.B. over ML King etc.
Spans are numbered from the East
Span 1 between W12 & W11

FAI 55 (STEVENSON EXPRESSWAY)
STRUCTURE NO.: 016-1055
BRIDGE DECK REPAIRS

| Span # | Lane 1 | | Lane 2 | | Lane 3 | |
|--------------|---------------------------------|------|---------------------------|------|------------------------------------|------|
| | Partial | Full | Partial | Full | Partial | Full |
| 1 | 0 | | 3x3 = 9 sf | | 4x4 = 16 sf | |
| 2 | 4x4, 4x6, 2x6, 2x2, 3x3 = 65 sf | | 2x2, 4x4 = 20 sf | | 3 @ 3x3, 2 @ 2x2, 4x6 = 59 sf | |
| 3 | 0 | | 4x6, 2 @ 2x2 = 32 sf | | 0 | |
| 4 | 0 | | 3x3, 2x2 = 13 sf | | 3 @ 3x3 = 27 sf | |
| 5 | 2 @ 2x2 = 8 sf | | 3 @ 2x2, 4x4 = 28 sf | | 2x2, 3x3 = 13 sf | |
| 6 | 4x4 = 16 | | 2x3, 3 @ 3x4, 4x4 = 58 sf | | 3 @ 2x2, 3 @ 2x3, 3x3, 3x6 = 57 sf | |
| 7 | 3x3 = 9 sf | | 4 @ 3x3, 3x2, 4x4 = 56 sf | | 12x5, 7x4 = 88 sf | |
| 8 | 2x4 = 8 sf | | 8x10 = 80 sf | | 2x4, 6x8 = 56 sf | |
| 9 | 0 | | 12x6, 4x4 = 88 sf | | 3x3, 7x4 = 37 sf | |
| 10 | 5x4 = 20 sf | | 0 | | 4x4 = 16 sf | |
| 11 | 0 | | 3x4 = 12 sf | | 2 @ 4x6, 12x12 = 192 sf | |
| 12 | 0 | | 0 | | 0 | |
| TOTAL | 126 SF | | 396 SF | | 561 SF | |

PARTIAL DEPTH GRAND TOTAL 1083 SF = 120.3 SY
FULL DEPTH GRAND TOTAL 108 SF = 12 SY



016-1075-I-55 N.B. to US 41 N.B.-2 Lane Ramp

Span Count From West to East
Span 1 between E13 and E14

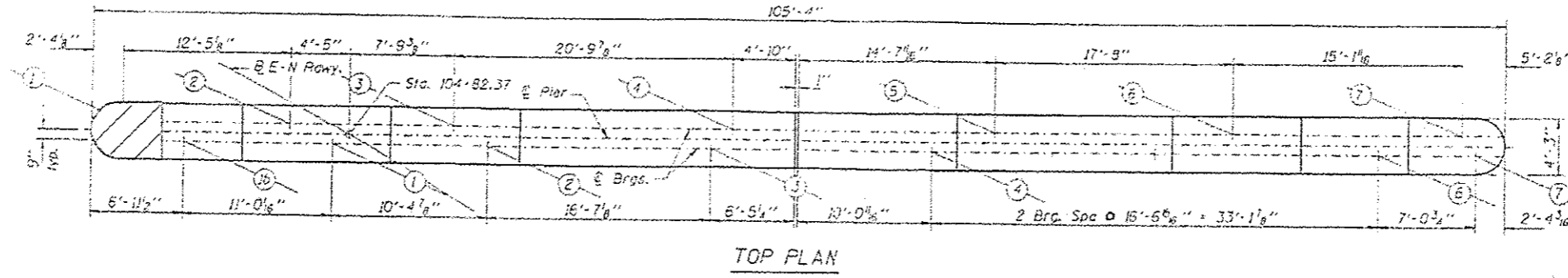
**FAI 55 (STEVENSON EXPRESSWAY)
STRUCTURE NO.: 016-1075
BRIDGE DECK REPAIRS**

| Span # | Lane 1 | | Lane 2 | |
|--------------------|------------------|------|---------------------------|------|
| | Partial | Full | Partial | Full |
| 1 | 0 | | 0 | |
| 2 | 0 | | 0 | |
| 3 | 0 | | 4x3 = 12 sf | |
| 4 | 2x2 = 4 sf | | 2x2 = 4 sf | |
| 5 | 0 | | 5x5 = 25 sf | |
| 6 | 0 | | 2x2 = 4 sf | |
| 7 | 0 | | 2 @ 2x2, 6x6 = 44 sf | |
| 8 | 2x3 = 6 sf | | 2 @ 2x2, 5x5, 4x6 = 57 sf | |
| 9 | 0 | | 0 | |
| 10 | 3x3 = 9 sf | | 3x3 = 9 sf | |
| 11 | 2x3, 3x3 = 15 sf | | 3x3, 6x3 = 27 sf | |
| 12 | 0 | | 2 @ 2x2, 4x4 = 24 sf | |
| 13 Vaulted Span | 3x3 = 9 sf | | 2 @ 4x4, 2x2 = 36 sf | |
| TOTAL | 43 SF | | 242 SF | |

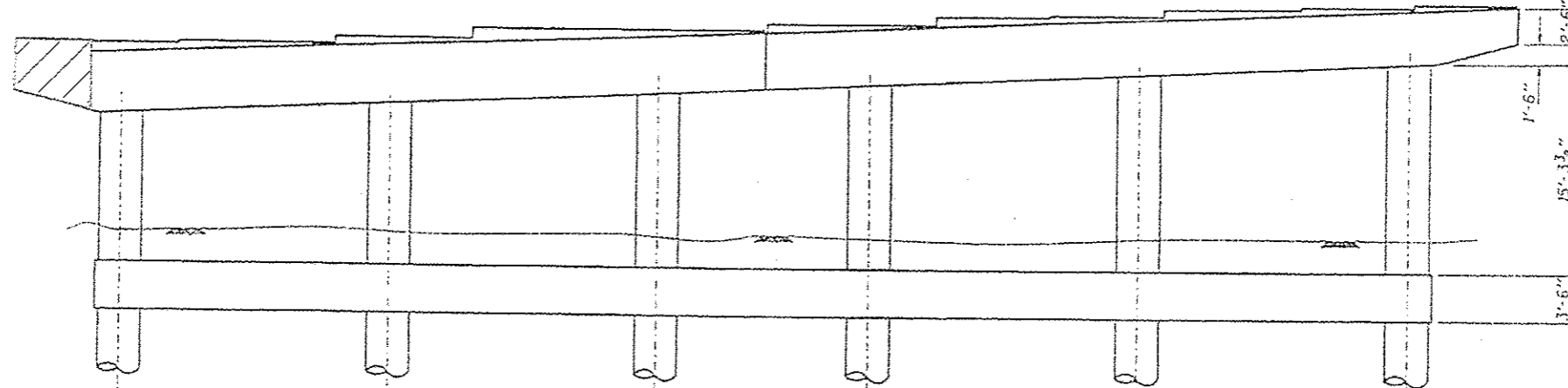
PARTIAL DEPTH GRAND TOTAL 285 SF = 31.7 SY

FULL DEPTH GRAND TOTAL 29 SF = 3.2 SY

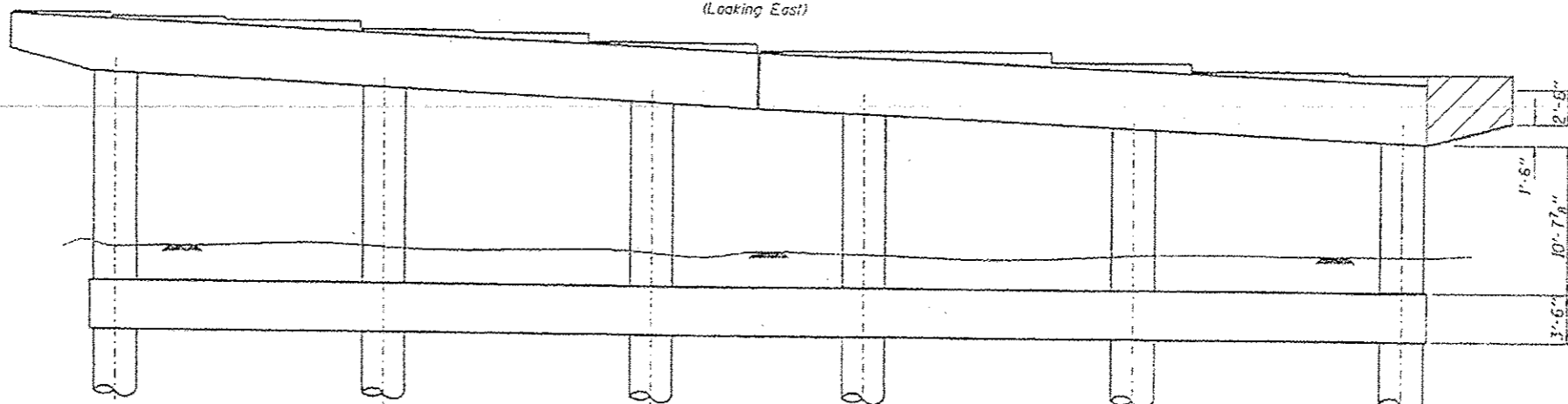
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



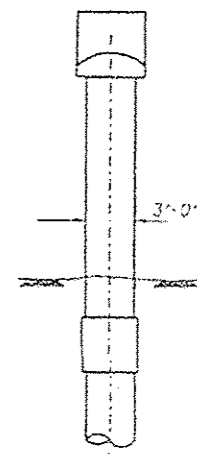
TOP PLAN



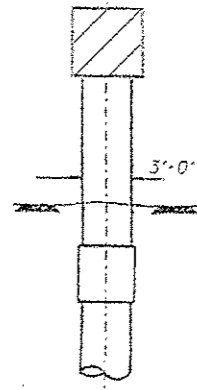
ELEVATION
(Looking East)



ELEVATION
(Looking West)



END VIEW
(Looking North)

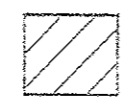


END VIEW
(Looking South)

Reaction Table for Temporary
Shoring and Cribbing at Pier E24

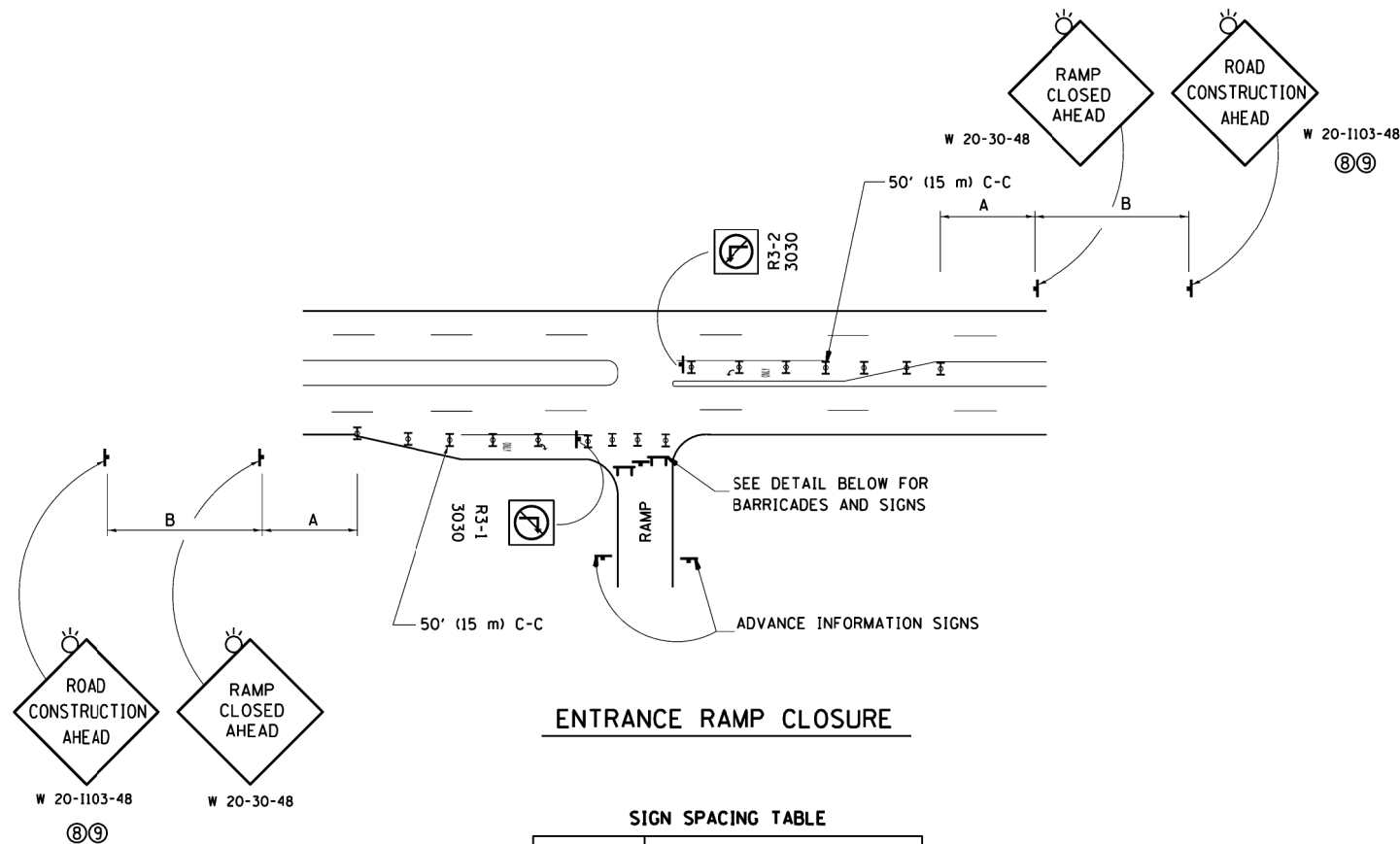
| Span E24 | |
|-----------|------------|
| R DL = | 58.5 kips |
| R LL = | 48.2 kips |
| R Imp. = | 11.2 kips |
| R Total = | 117.9 kips |

LEGEND:



Structural Repair of Concrete (Depth Greater than 5 in)
Area (Pier Cap north nose) = 93 sf
Temporary Shoring and Cribbing = 1 Each

PIER E24 DETAILS
STR. NO. 016-1075
F.A.I. RT. 55

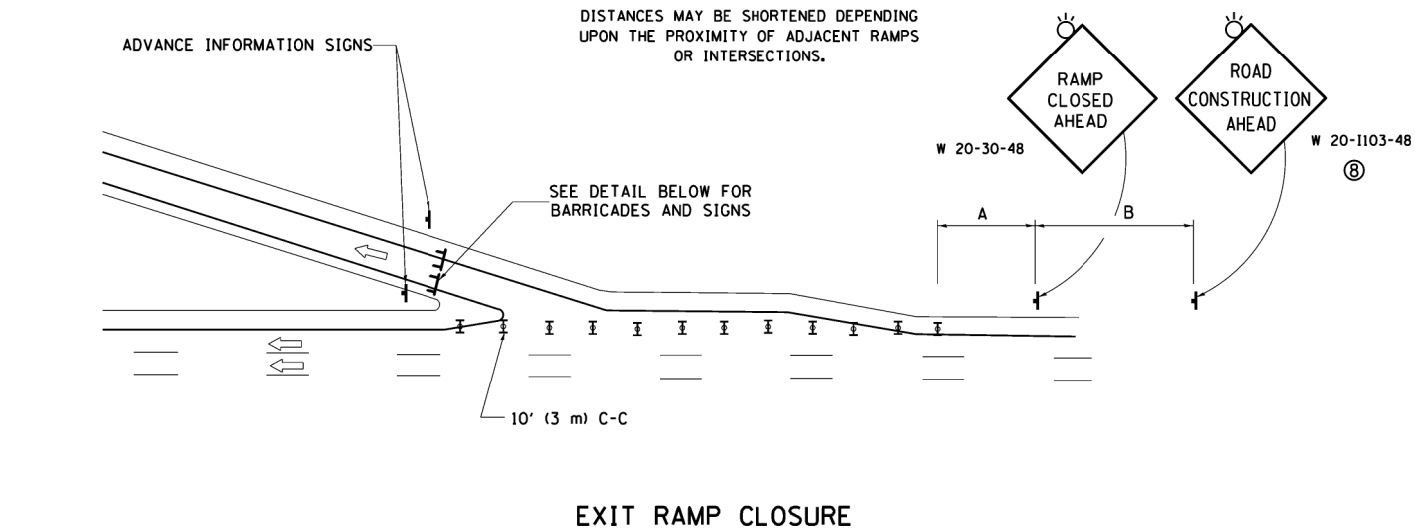


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

| FACILITY | DISTANCE BETWEEN SIGNS | |
|-------------------------|------------------------|------------------|
| | A | B |
| EXPRESSWAY >24 HOURS | 1000' (300 m) | 1500' (450 m) |
| EXPRESSWAY <24 HOURS | 500' (150 m) | 500' (150 m) |
| ARTERIAL 55 MPH | 500' (150 m) | 500' (150 m) |
| ARTERIAL 50-45 MPH | 350' (100 m) | 350' (100 m) |
| ARTERIAL <45 MPH | 200' (60 m) | 200' (60 m) |

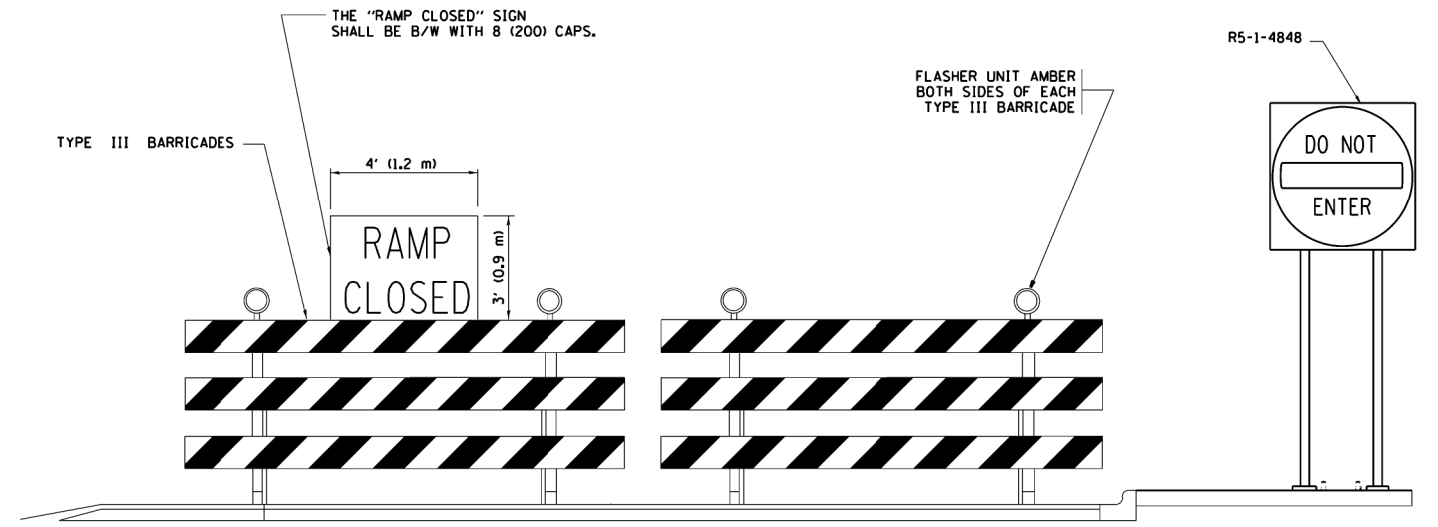
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



EXIT RAMP CLOSURE

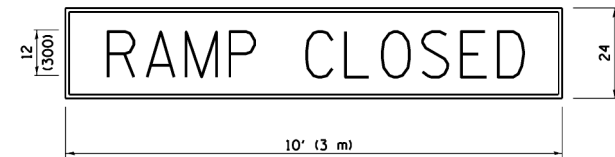
SYMBOLS

- ⊥ TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- ⊓ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



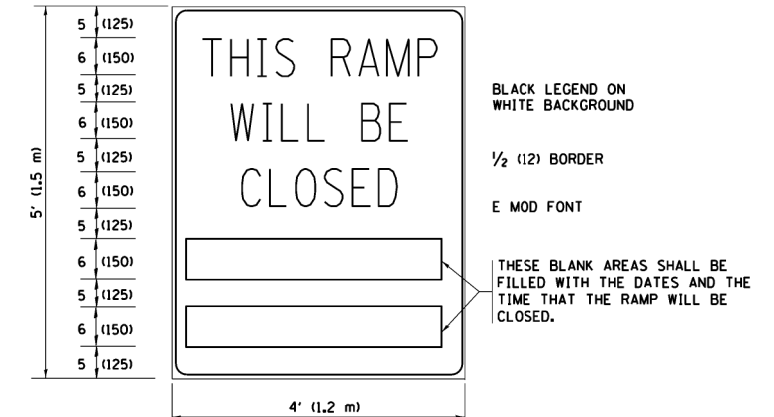
DETAIL FOR REQUIRED BARRICADES & SIGNS

RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY
E MOD FONT
1 (25) BORDER
THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

RAMP CLOSURE ADVANCE INFORMATION SIGN



BLACK LEGEND ON WHITE BACKGROUND
1/2 (12) BORDER
E MOD FONT

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

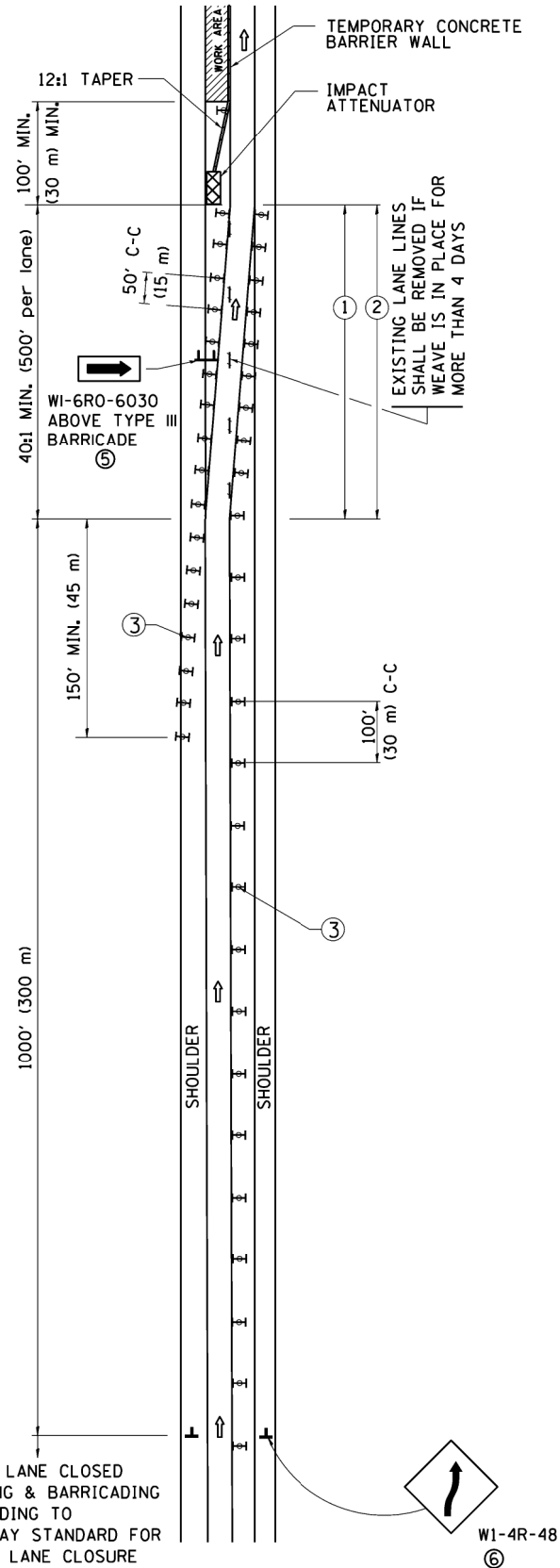
THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

GENERAL NOTES:

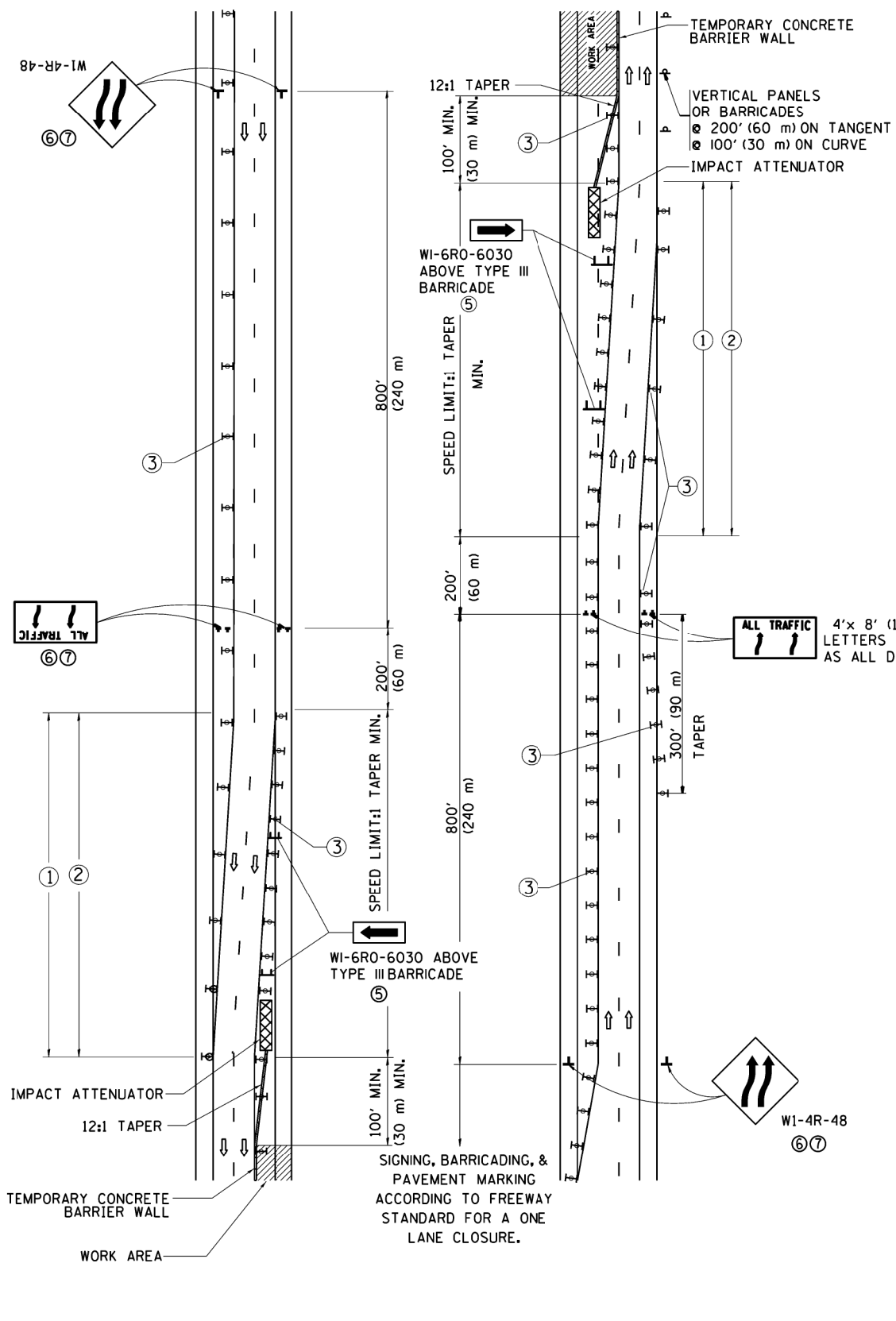
- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

SINGLE LANE WEAVE



MULTI-LANE WEAVE



GENERAL NOTES

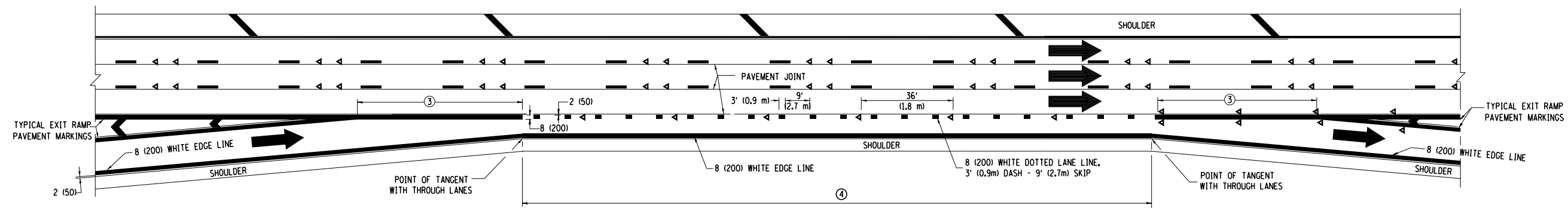
- ① EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 4 DAYS IN DURATION.
- ② CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- ③ PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ④ ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ⑤ TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- ⑥ WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- ⑦ THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

SYMBOLS

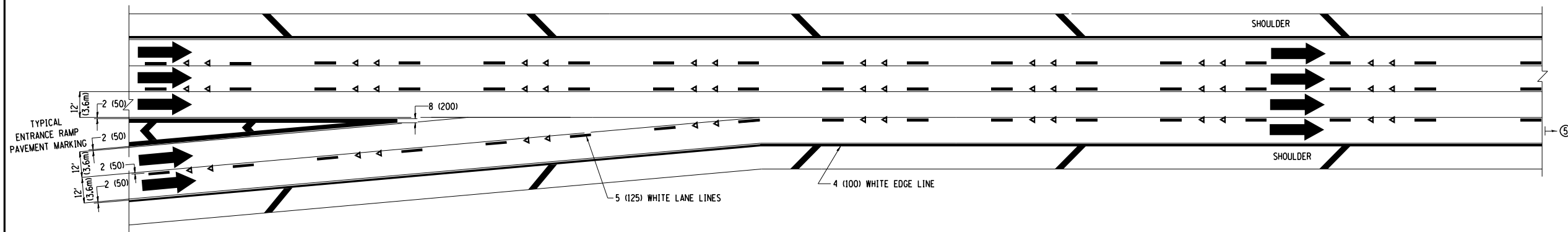
- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- TEMPORARY CONCRETE BARRIER WALL
- IMPACT ATTENUATOR
- W1-4R-48
- W24-1-48

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

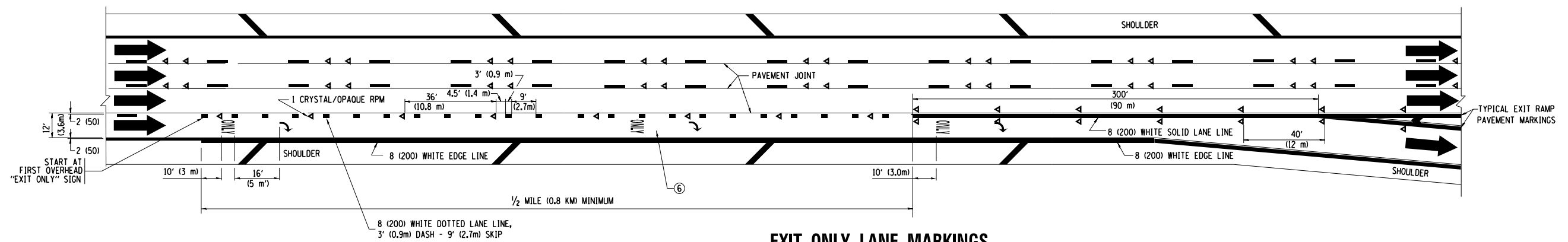
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|---|----------------------|----------------|---------------------|---|--|-------------------------|------|-----------|---------------------------|--------|--------------|-----------|--|
| FILE NAME = | USER NAME = osbornnp | DESIGNED - DWS | REVISED - JAF 02-06 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE & MULTI-LANE WEAVE | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| c:\p\work\p\idot\osbornnp\d0382528\d0382528.dgn | | DRAWN - | REVISED - SPB 01-07 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | 2014-018BR | COOK | 33 | 29 | |
| PLOT SCALE = 1/8" = 1'-0" | | CHECKED - | REVISED - SPB 12-09 | | | | | | TC-09 | | | | |
| PLOT DATE = 3/18/2014 | | DATE - 02-87 | REVISED - MD 06-13 | | | | | | CONTRACT NO. 60X91 | | | | |



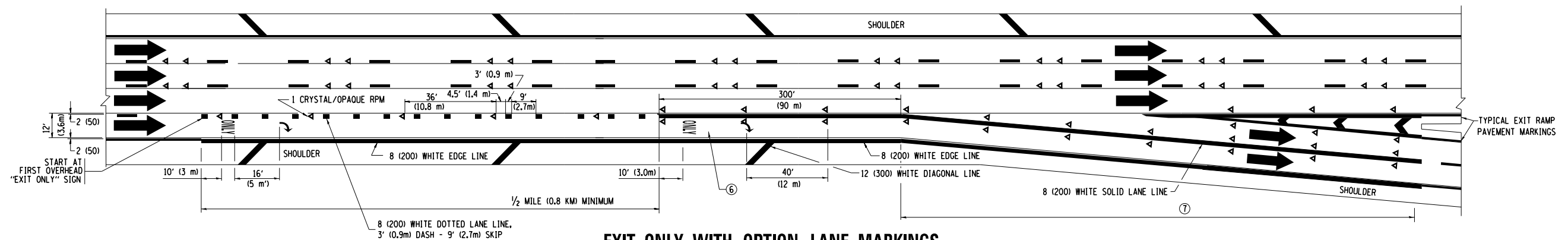
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



EXIT ONLY LANE MARKINGS

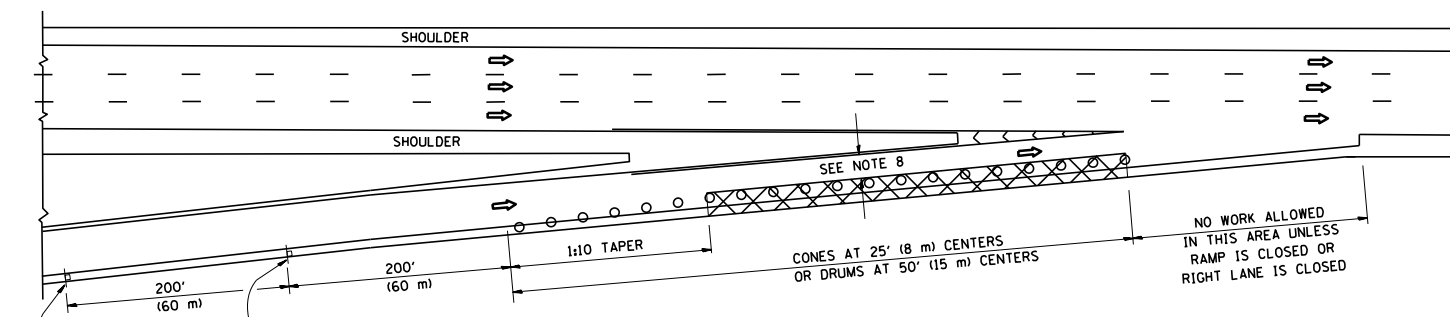


EXIT ONLY WITH OPTION LANE MARKINGS

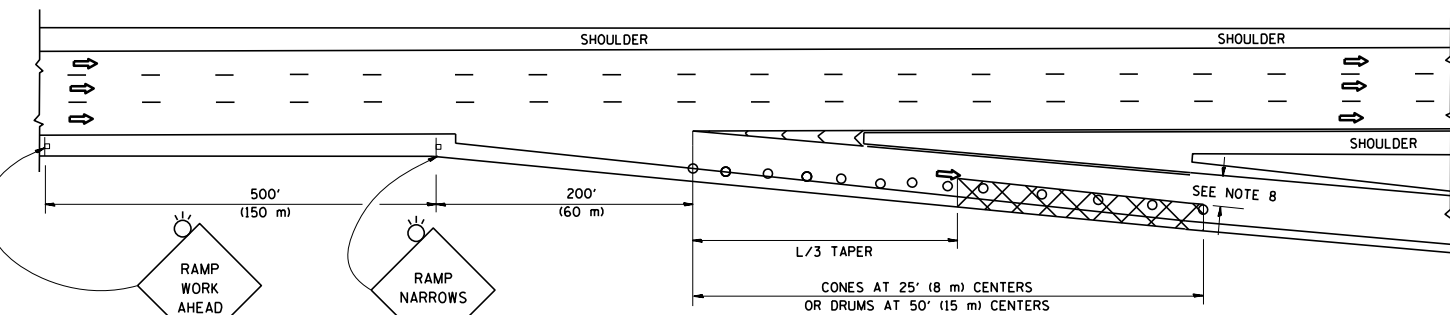
- NOTES**
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
 - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
 - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
 - ⑥ ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
 - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.

| | | | | | | | | | | | | | |
|--|--------------------------|----------------------|---------------------------|---|--|-------------|-------------|--------------|---------|---------------------------|--------------|-----------|----|
| FILE NAME = e:\pwwork\pwwork\osbornen\10382528\10382528.dgn | USER NAME = osbornenp | DESIGNED - D.W.S. | REVISED - D.W.S. 07-96 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| PLOT SCALE = 1/8" = 1'-0" | CHECKED - | DATE - | REVISED - | | SCALE: NONE | SHEET NO. 2 | OF 2 SHEETS | STA. | TO STA. | 2014-018BR | COOK | 33 | 31 |
| PLOT DATE = 3/18/2014 | DATE - | REVISED - | REVISED - | | | | | TC-12 | | CONTRACT NO. 60X91 | | | |
| | | | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | | | | | |

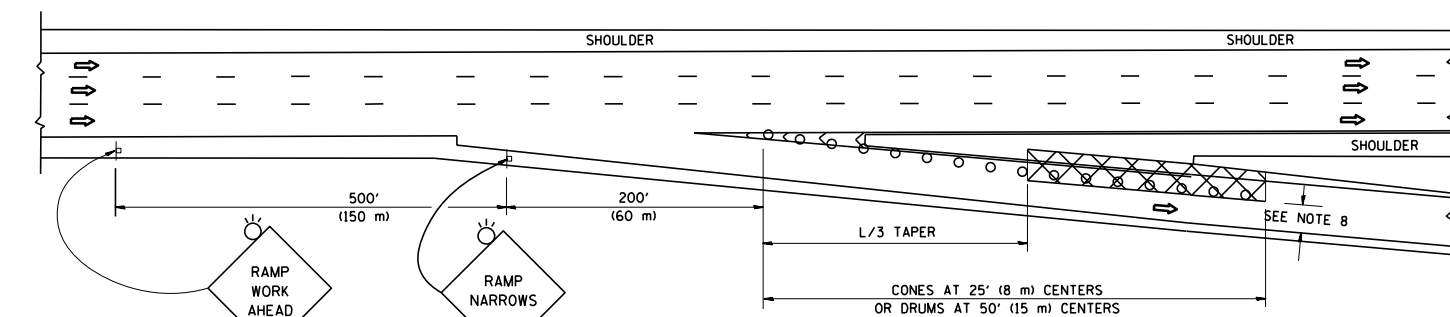
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

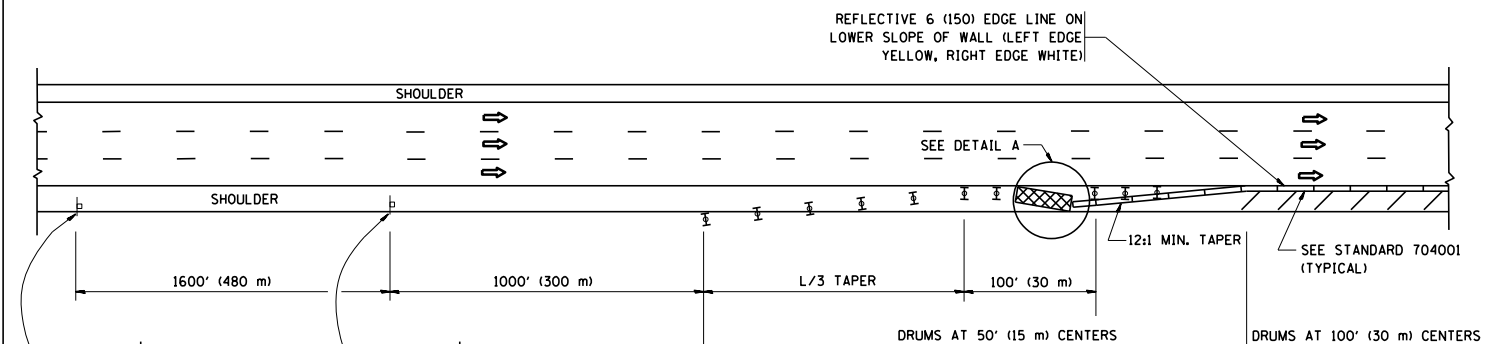
SYMBOLS

- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

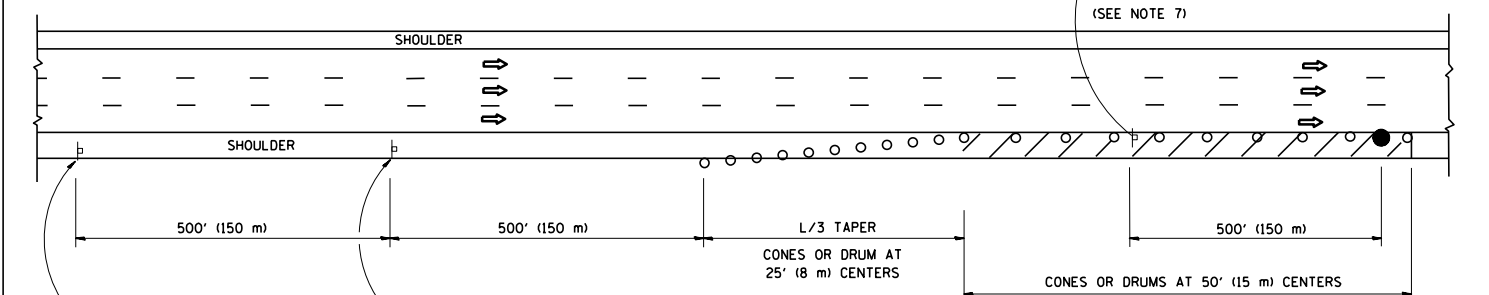
GENERAL NOTES

1. THE "L" DISTANCE EQUALS:
SPEED LIMIT FORMULAS
45 mph (80 km/h) METRIC ENGLISH
OR GREATER: $L=0.65(W)(S)$ $L=(W)(S)$
W = WIDTH OF OFFSET IN FEET (METERS)
S = NORMAL POSTED SPEED MPH (KM/H)
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

SHOULDER CLOSURE DETAILS



PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCR OACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.

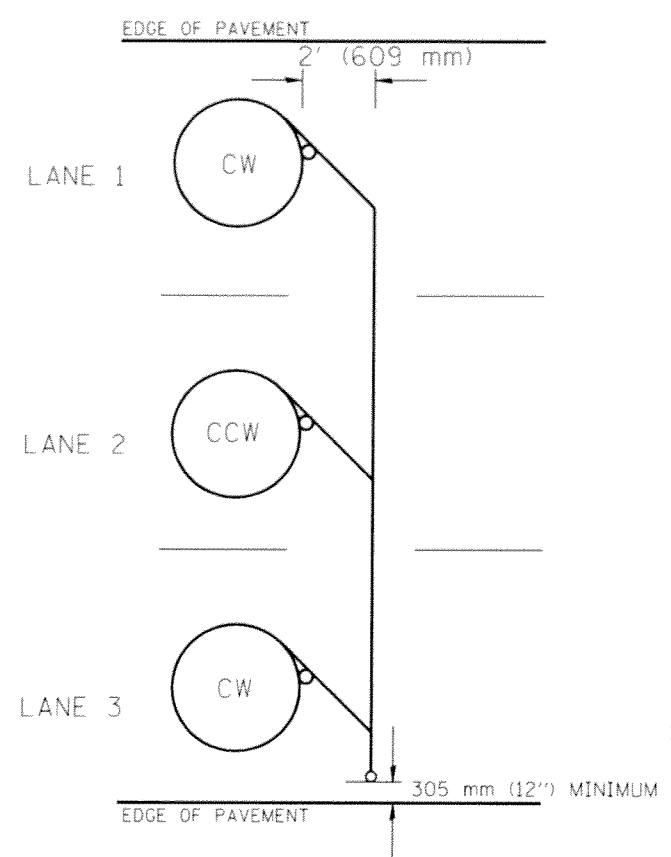
ARRAY DESIGN PER MANUFACTURER TO BE NCHRP 350/MASH COMPLIANT.

DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)

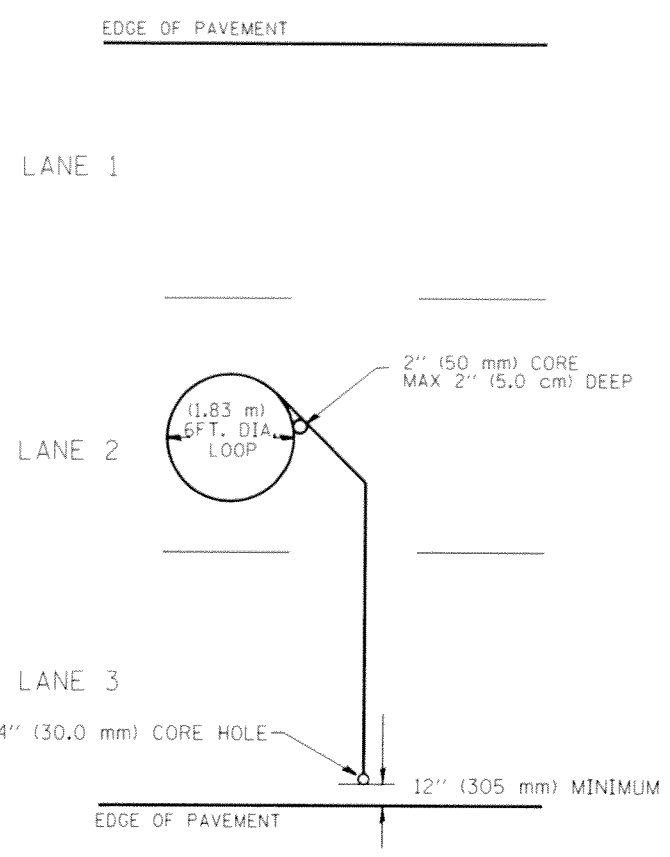
5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - b. THE WORK ACTIVITY REQUIRES FREQUENT ENCR OACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12" MIN. WIDTH TANGENT SECTION
16" MIN. WIDTH CURVE SECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

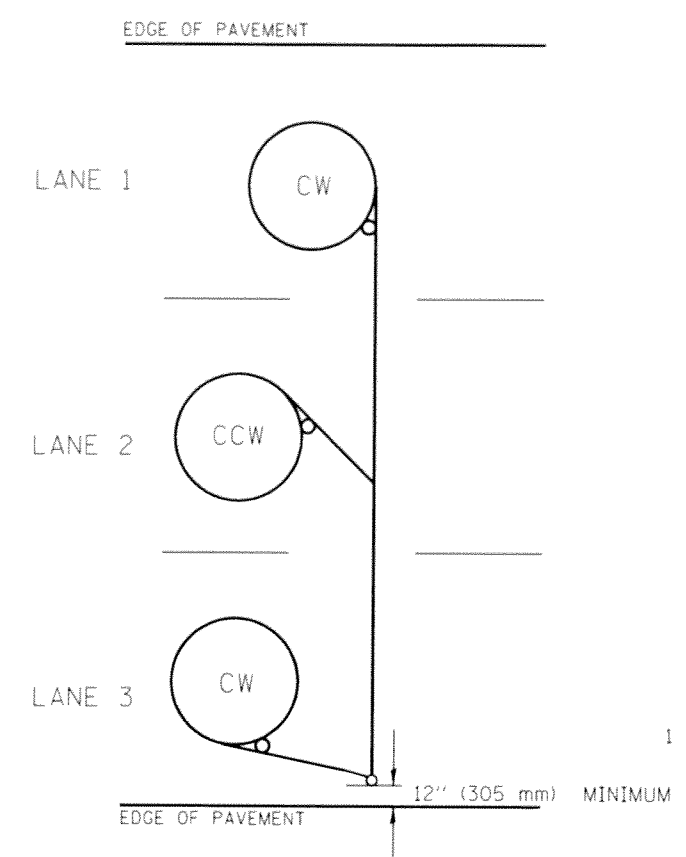
| | | | | | | | | | | | | | |
|--|----------------------|----------------|------------------------|---|--|-------------------------|------|-----------|---|--------|--------------|-----------|--------------------|
| FILE NAME = | USER NAME = osbornnp | DESIGNED - | REVISED - J.A.F. 12-06 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| c:\pwork\pwork\osbornnp\d0382528\1115std.dgn | | DRAWN - D.W.S. | REVISED - S.P.B. 01-07 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | 2014-018BR | COOK | 33 | 32 | |
| | | CHECKED - | REVISED - S.P.B. 12-09 | | | | | | TC-17 | | | | CONTRACT NO. 60X91 |
| | | DATE - 11-96 | REVISED - M.D. 06-13 | | | | | | FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT | | | | |



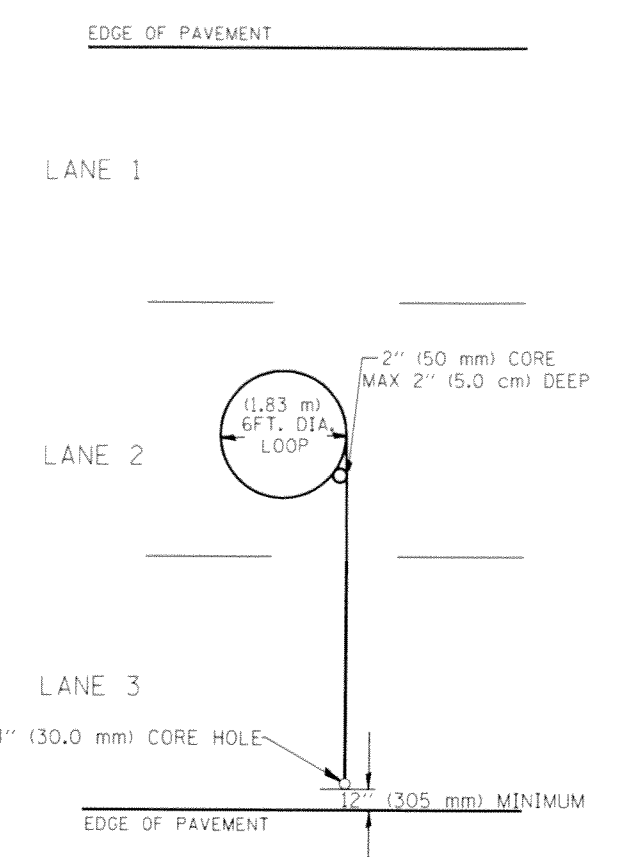
TYPICAL 6' (1.83 m) DIA. INDUCTION LOOP CORE DRILL
LAYOUT FOR MULTIPLE LANE ROADWAY



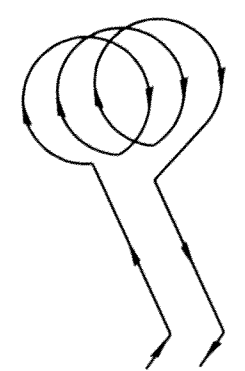
TYPICAL 6' (1.83 m) DIA. INDUCTION LOOP
LAYOUT FOR LANE 2



TYPICAL 6' (1.83 m) DIA. INDUCTION LOOP CORE DRILL
LAYOUT FOR MULTIPLE LANE ROADWAY



TYPICAL 6' (1.83 m) DIA. INDUCTION LOOP
LAYOUT FOR LANE 2

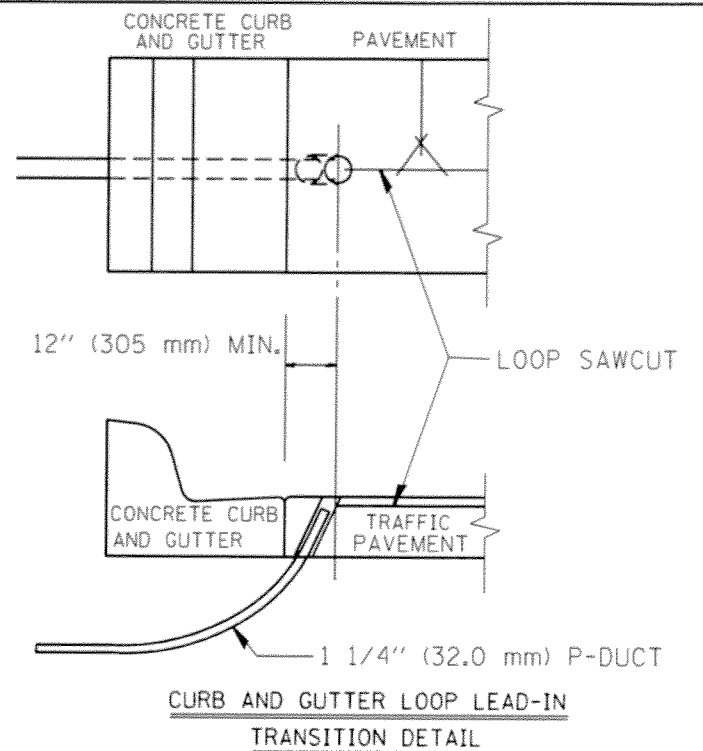
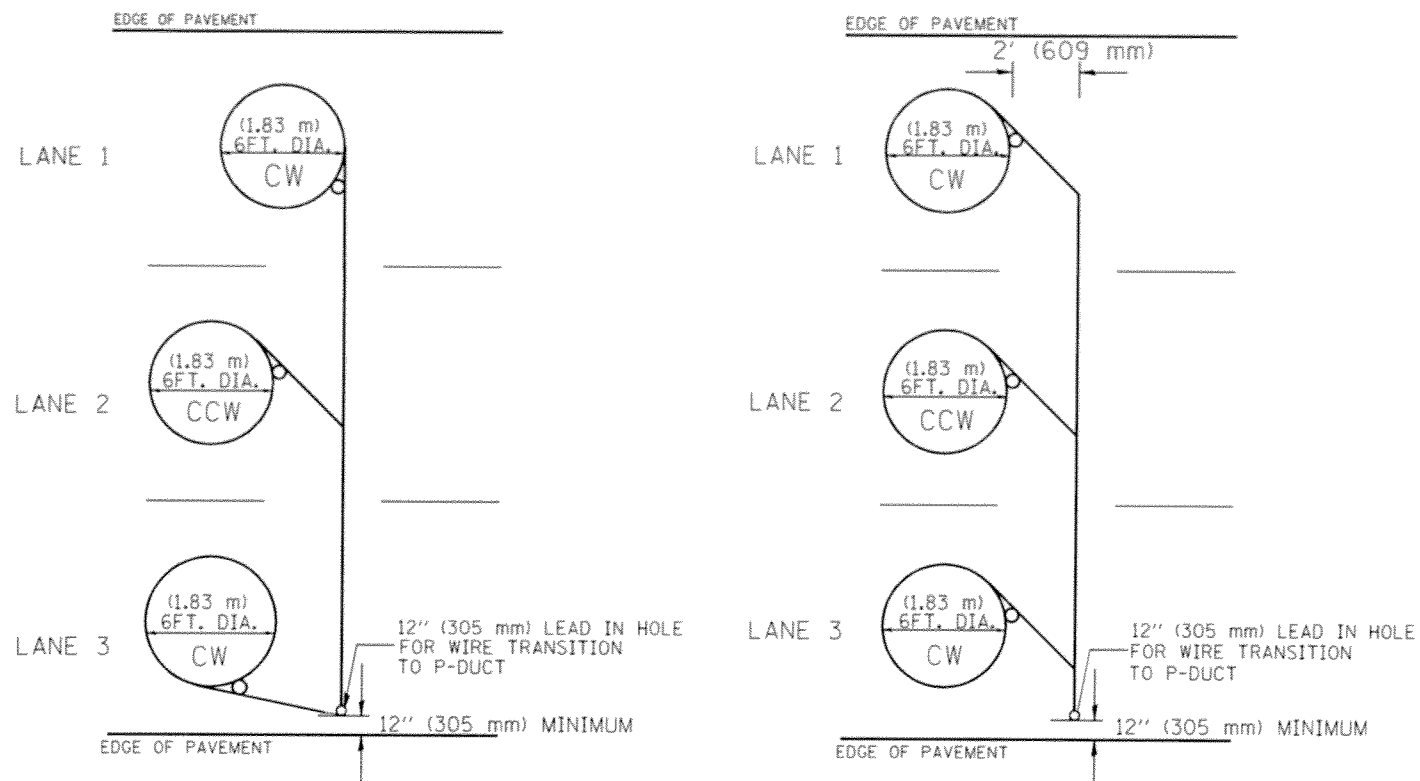


WIRING DETAILS

NOTES

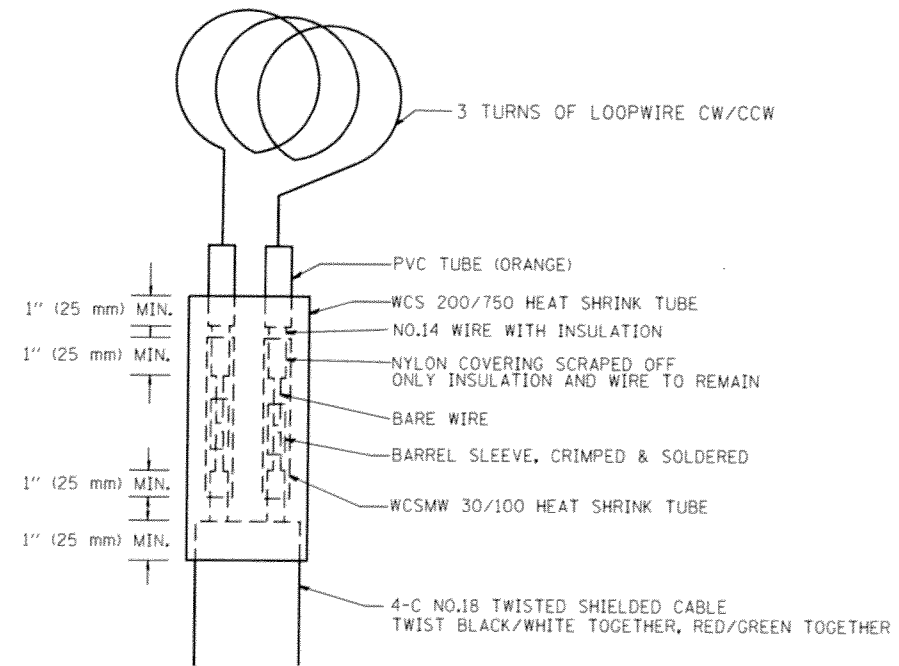
1. EACH LOOP SHALL BE SPLICED TO A 4-C NO.18 TWISTED SHIELDED LEAD IN WHEN 150' (45 m) OR MORE FROM CABINET.
2. LOOPS SHALL BE SPLICED IN HANDHOLES ONLY, OTHERWISE WRITTEN PERMISSION SHALL BE OBTAINED FROM TSC ENGINEER.
3. LOOPS SHALL NOT BE SPLICED IN SERIES.
4. EACH LOOP LEAD IN SHALL BE IDENTIFIED AND PERMANENTLY COLOR CODED IN THE COREHOLE, HANDHOLE & CABINETS THRU WHICH THEY ENTER OR PASS AND TAGGED WITH THE CORRECT NOMENCLATURES.

| | | | | | | | | | | | | |
|--|------------------------------|-----------------|----------------|--|---|---------------------|--------------|---------------------|---------------------------|--------------------|-----------------|--------------|
| FILE NAME = C:\Project\TSC\TYPICAL\TSC\TYP08.dgn | USER NAME = mezag | DESIGNED - R.L. | REVISED - 6/94 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC SYSTEMS CENTER | EXISTING ROUND LOOP INSTALLATION | | | F.A. RTE. 55 | SECTION 2014-0186R | COUNTY Cook | TOTAL SHEETS 33 | SHEET NO. 33 |
| | PLOT SCALE = 100.0000 1/ IN. | DRAWN - G.M. | REVISED - 9/96 | | SCALE: NONE | SHEET NO. OF SHEETS | STA. TO STA. | FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | CONTRACT NO. 60X91 | | |
| | PLOT DATE = 3/3/2011 | CHECKED - R.L. | REVISED - | | | | | | | | | |
| | | DATE - 6-22-94 | REVISED - | | | | | | | | | |



TYPICAL 6FT. (1.83 m) DIA. INDUCTION LOOP CORE DRILL LAYOUT FOR MULTIPLE LANE ROADWAY

TYPICAL 6FT. (1.83 m) DIA. INDUCTION LOOP CORE DRILL LAYOUT FOR MULTIPLE LANE ROADWAY



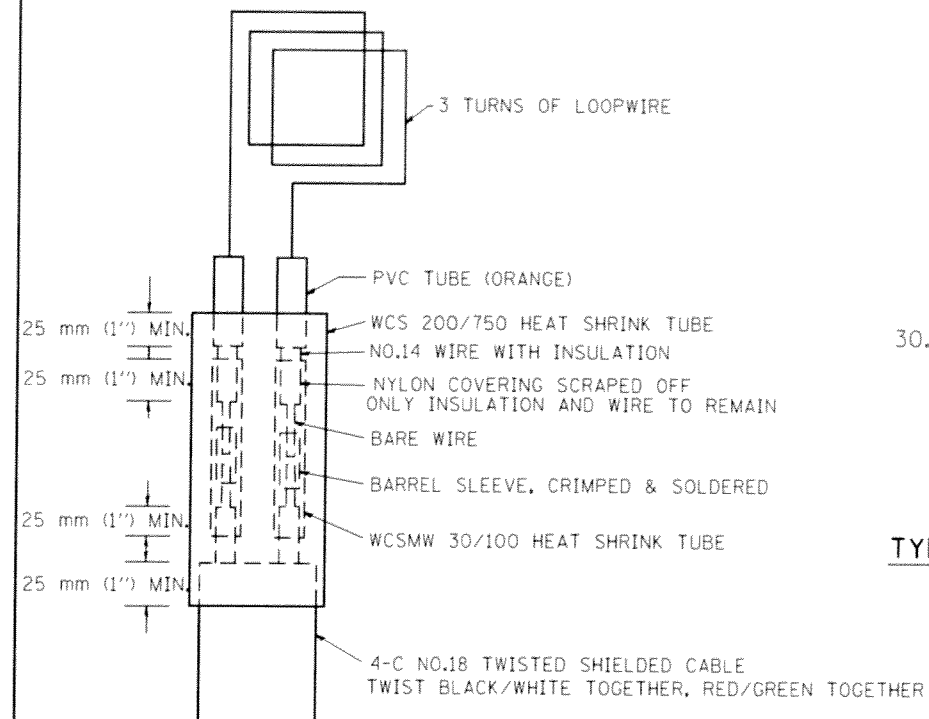
MINIMUM 1" (25 mm) HEAT SHRINK TUBING OVERLAP ON WIRE, PVC & SHIELDED CABLE TO FORM WATER TIGHT SEAL

LOOP SPLICING REQUIREMENTS

NOTES

1. EACH LOOP SHALL BE SPLICED TO A 4-C NO.18 TWISTED SHIELDED LEAD IN WHEN 150FT. (45 m) OR MORE FROM CABINET.
2. LOOPS SHALL BE SPLICED IN HANDHOLES ONLY, OTHERWISE WRITTEN PERMISSION SHALL BE OBTAINED FROM TSC ENGINEER.
3. LOOPS SHALL NOT BE SPLICED IN SERIES.
4. EACH LOOP LEAD IN SHALL BE IDENTIFIED AND PERMANENTLY COLOR CODED IN THE COREHOLE, HANDHOLE & CABINETS THRU WHICH THEY ENTER OR PASS AND TAGGED WITH THE CORRECT NOMENCLATURES.

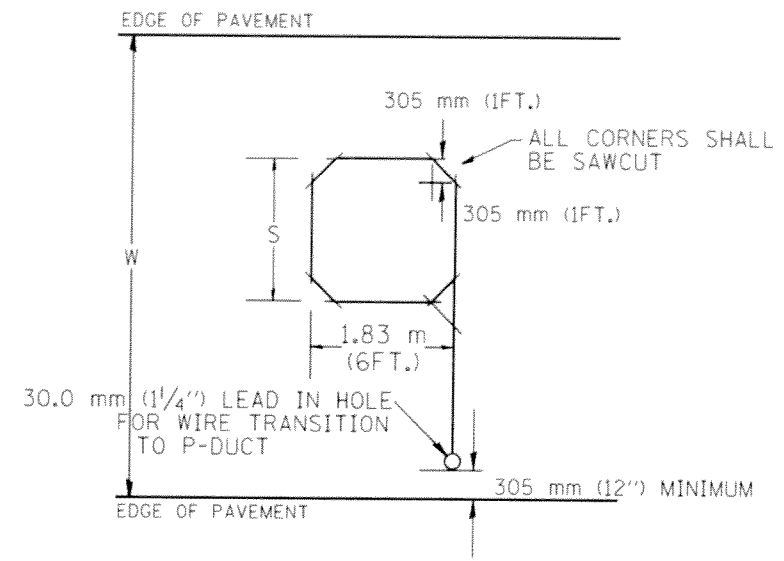
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| FILE NAME = | USER NAME = m29g | DESIGNED - R.L. | REVISED - 6/94 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC SYSTEMS CENTER | EXISTING ROUND INDUCTION LOOP TYPICALS | F.A.E. RTE. = | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| C:\Projects\TSC\TYPICALS\TSC\TYP08.dgn | | DRAWN - G.M. | REVISED - 10/96 | | | 55 | 2014-018BR | COOK | 34 | 34 |
| PLOT SCALE = 100.0000' / IN. | | CHECKED - R.L. | REVISED - | | | CONTRACT NO. 60X91 | | | | |
| PLOT DATE = 12/2/2010 | | DATE - 6-22-94 | REVISED - | | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |



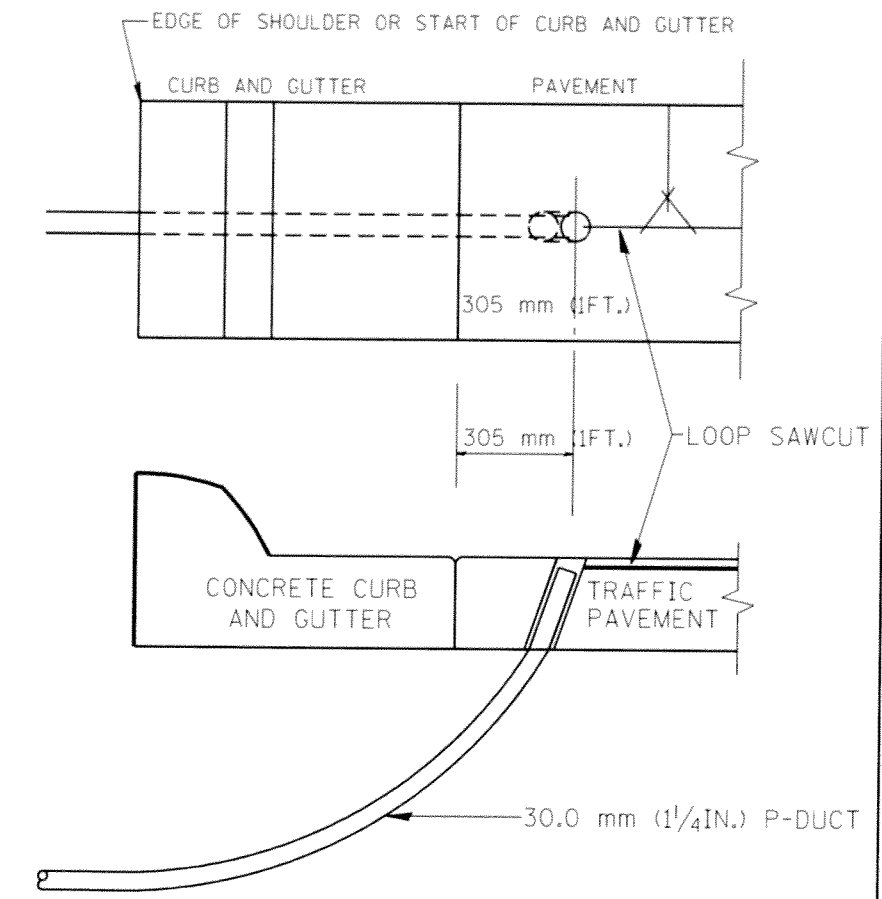
MINIMUM 25 mm (1") HEAT SHRINK TUBING OVERLAP ON WIRE, PVC & SHIELDED CABLE TO FORM WATER TIGHT SEAL

LOOP SPLICING REQUIREMENTS

| WIDTH (W) | WIDTH (S) |
|-------------|-------------|
| 3.7 m (12') | 2.5 m (8') |
| 4.0 m (13') | 2.8 m (9') |
| 4.3 m (14') | 3.1 m (10') |
| 4.6 m (15') | 3.4 m (11') |
| 4.9 m (16') | 3.7 m (12') |
| 5.2 m (17') | 4.0 m (13') |
| 5.5 m (18') | 4.3 m (14') |
| 5.8 m (19') | 4.6 m (15') |
| 6.1 m (20') | 4.9 m (16') |
| 6.4 m (21') | 5.2 m (17') |
| 6.7 m (22') | 5.5 m (18') |
| 7.0 m (23') | 5.8 m (19') |
| 7.3 m (24') | 6.1 m (20') |
| 7.6 m (25') | 6.4 m (21') |



TYPICAL "S" FT. BY 1.83 m (6FT.) INDUCTION LOOP SAWCUT LAYOUT FOR RAMPS



CURB AND GUTTER LOOP LEAD-IN TRANSITION DETAIL

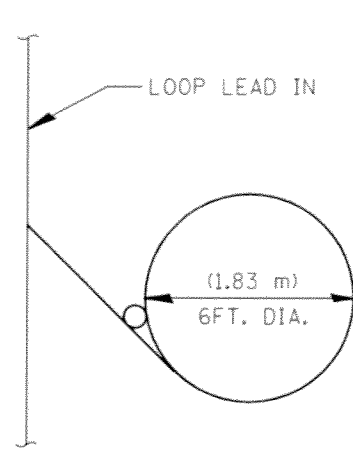
NOTES

1. EACH LOOP SHALL BE SPLICED TO A 4-C NO.18 TWISTED SHIELDED LEAD IN WHEN 45 m (150FT.) OR MORE FROM CABINET.
2. LOOPS SHALL BE SPLICED IN HANDHOLES ONLY, OTHERWISE WRITTEN PERMISSION SHALL BE OBTAINED FROM TSC ENGINEER.
3. LOOPS SHALL NOT BE SPLICED IN SERIES.
4. EACH LOOP LEAD IN SHALL BE IDENTIFIED AND PERMANENTLY COLOR CODED IN THE COREHOLE, HANDHOLE & CABINETS THRU WHICH THEY ENTER OR PASS AND TAGGED WITH THE CORRECT NOMENCLATURES.

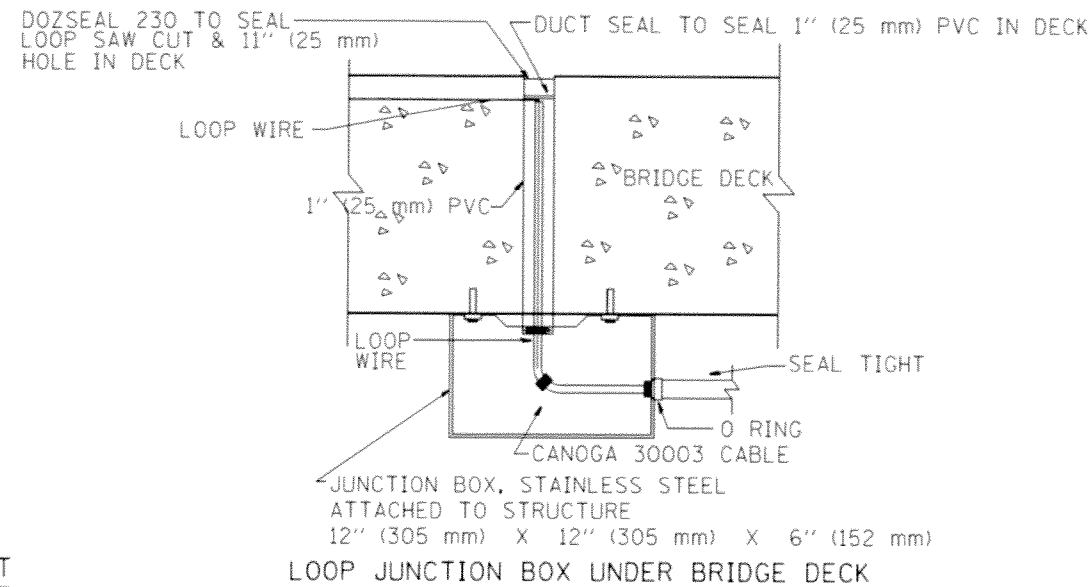
| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC SYSTEMS CENTER RECTANGULAR INDUCTION LOOP TYPICAL |
|-----------|-------|--|
| NAME | DATE | |
| R.L. | 6/94 | SCALE: VERT. NONE HORIZ. DATE 6-22-94 DRAWN BY: G.M. CHECKED BY: R.L. |
| T.C. | 11/95 | |
| R.L. | 05/96 | |
| T.C. | 10/96 | |

RR-TYPMET.DGN
RR=

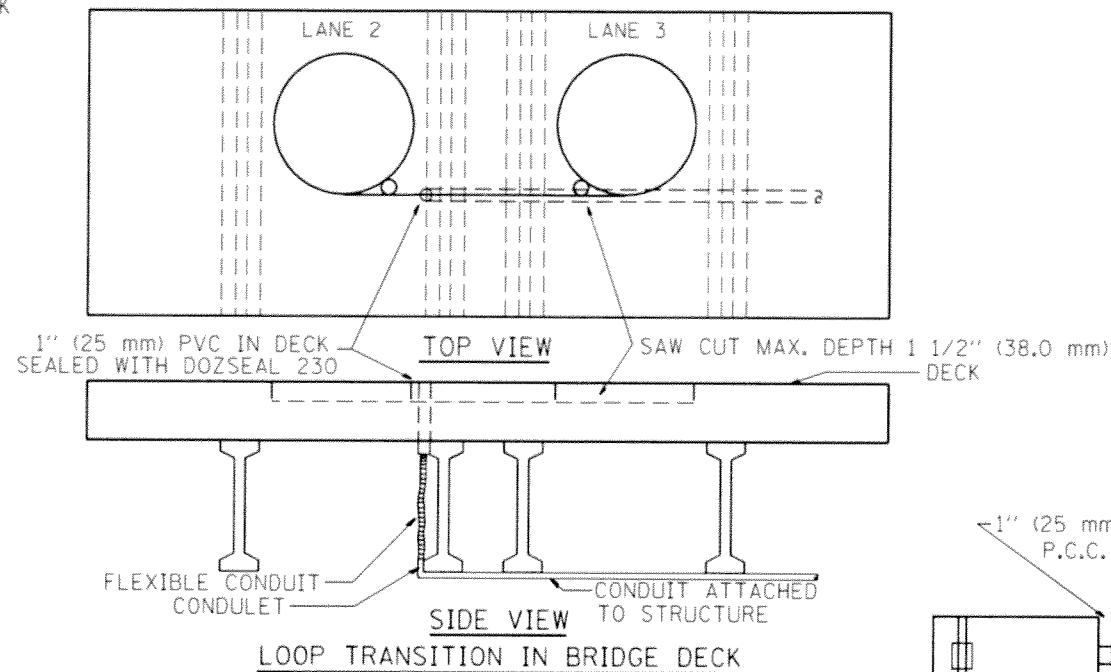
VI=4182



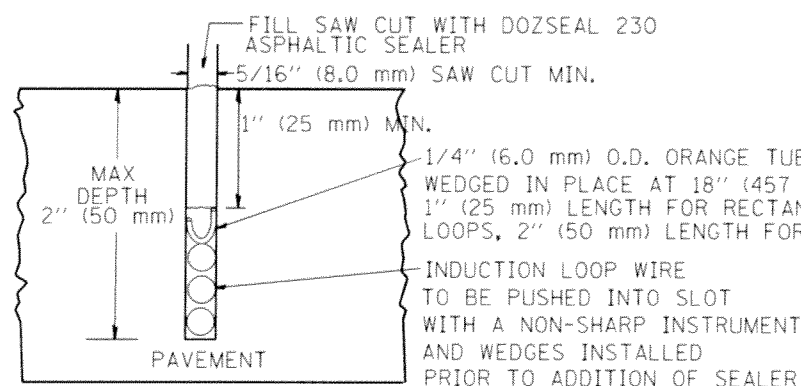
TYPICAL LOOP SAWCUT LAYOUT



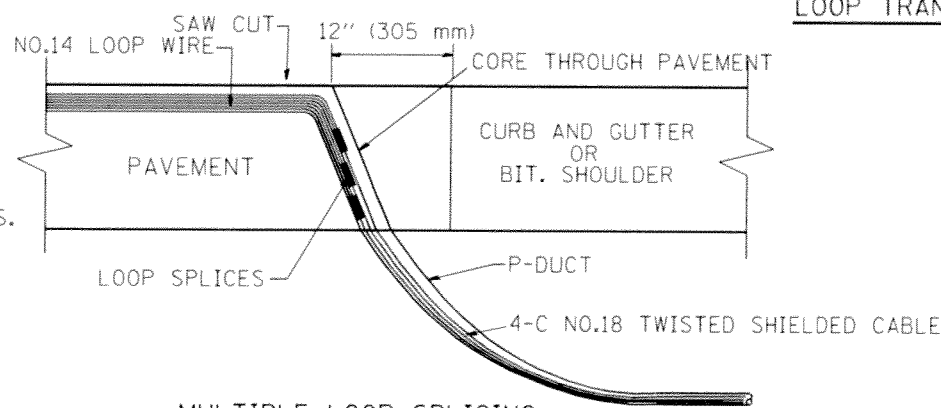
LOOP JUNCTION BOX UNDER BRIDGE DECK



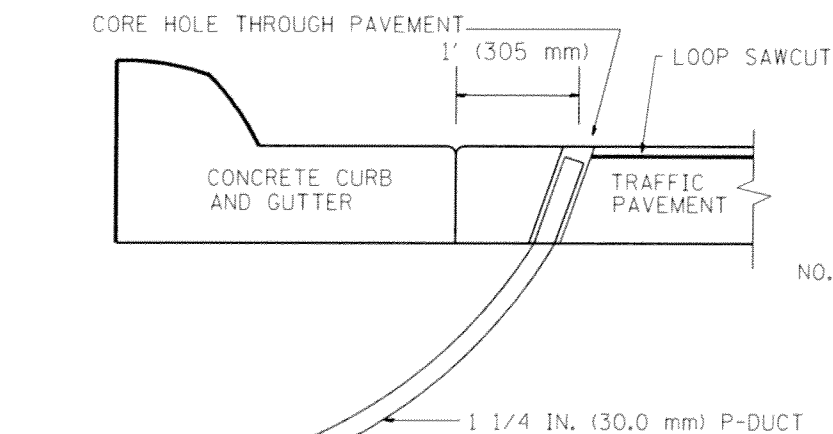
LOOP TRANSITION IN BRIDGE DECK



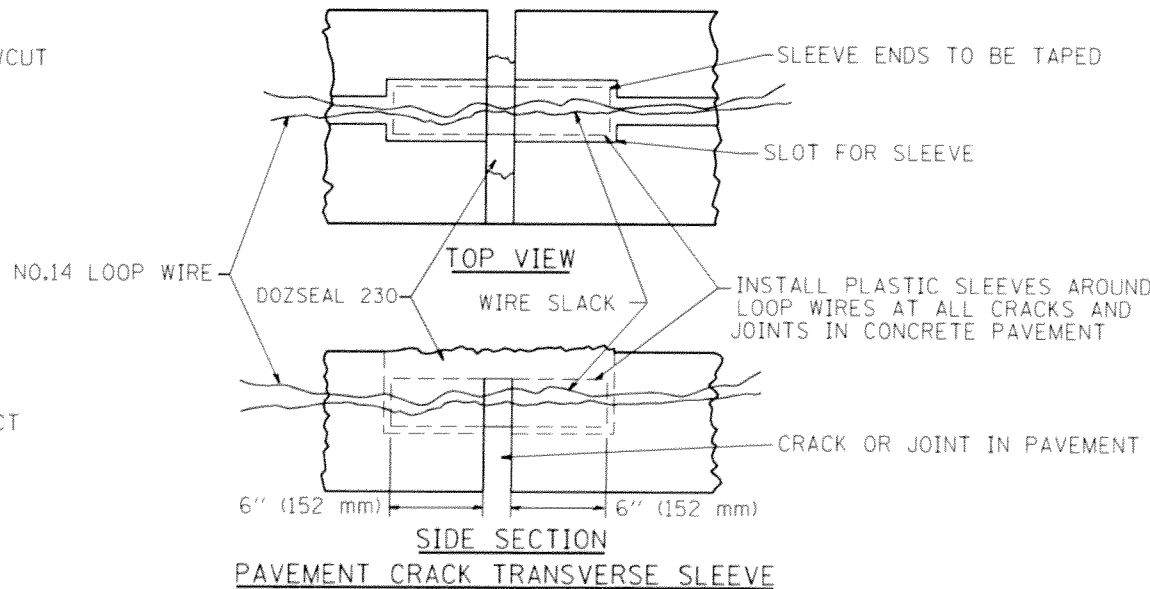
LOOP CROSS SECTION IN PAVEMENT



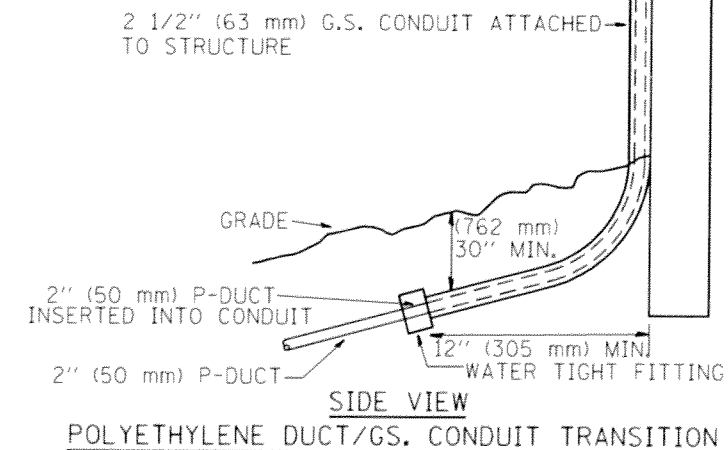
MULTIPLE LOOP SPLICING



SIDE SECTION LOOP LEAD-IN TRANSITION DETAIL



SIDE SECTION PAVEMENT CRACK TRANSVERSE SLEEVE



POLYETHYLENE DUCT/GS. CONDUIT TRANSITION

| | | | | | | | | | | | |
|---|-----------------------------|-----------------|-----------------|--|--|---------------------|-------------------|------------------------|---------------------------|--------------------|-----------------|
| FILE NAME * C:\Projects\TSC\TYPICALS\TSC1P88.dgn | USER NAME * mezag | DESIGNED - R.L. | REVISED - 06/94 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC SYSTEMS CENTER | LOOP, CONDUIT & DUCT INSTALLATION DETAILS | | F.A. # RTE. 55 | SECTION 2014-018 BR | COUNTY Cook | TOTAL SHEETS 36 | SHEET NO. 36 |
| | PLOT SCALE = 100.0000 / IN. | DRAWN - G.M. | REVISED - 03/95 | | SCALE: NONE | SHEET NO. OF SHEETS | STA. TO STA. | FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | CONTRACT NO. 60X91 | |
| | PLOT DATE = 12/2/2010 | CHECKED - R.L. | REVISED - 11/96 | | | | | | | | |
| | | DATE - 06/22/94 | REVISED - 18/96 | | | | | | | | |

