

FEDERAL AID PROGRAM ENGINEER: FAWAD AQUEEL, P.E. 847-705-4021 SCHAUMBURG, IL
CONSULTANT ENGINEER: REID T. MAGNER, P.E. CIVILTECH ENGINEERING, INC.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

FAU RTE 2586 (SWIFT ROAD)
ARMY TRAIL ROAD TO LAKE STREET (US ROUTE 20)
RESURFACING IMPROVEMENTS
SECTION 13-00102-00-RS
PROJECT NO. M-4003(212)
VILLAGE OF ADDISON
DUPAGE COUNTY
C-91-389-13

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2586	13-00102-00-RS	DUPAGE	40	1
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 61A14	



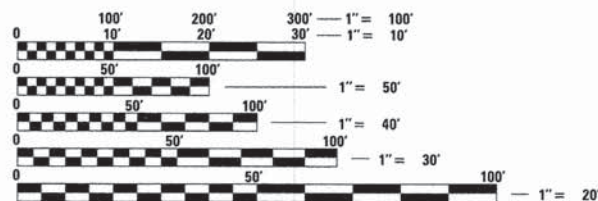
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- 701427-02 LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS < 40 MPH
- 701601-09 URBAN LANE CLOSURE MULTILANE, 1 W OR 2W WITH NONTRAVERSABLE MEDIAN
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- 886006-01 TYPICAL LAYOUTS FOR DETECTION LOOPS

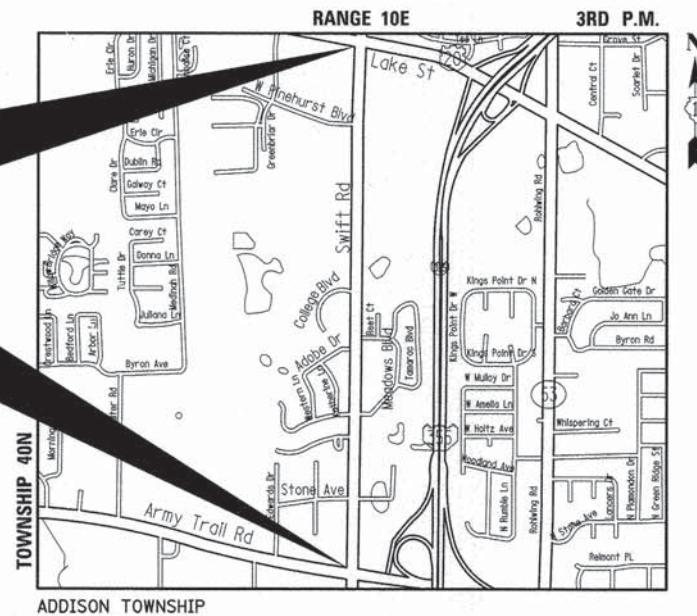
DESIGN DESIGNATION = MINOR ARTERIAL URBAN
POSTED SPEED = 35 MPH
SWIFT ROAD - 2012 ADT = 11,900 VPD



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.

JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1.800.892.0123 OR 811



LOCATION MAP
NOT TO SCALE

PROJECT LENGTH:

SWIFT ROAD (STA 67+00.0 TO STA 139+75.8)
PROJECT TOTAL - 7,275.8 FT (1.38 MILE) (GROSS & NET)

CONTRACT NO. 61A14

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

Approved: 1-29-2014
[Signature]
Rodolfo M. Espedillo, P.E., Village of Addison, Village Engineer

Passed: FEBRUARY 10, 2014
[Signature]
District One Engineer of Local Roads & Streets

Releasing for Bid Based on Limited Review: February 10, 2014
[Signature]
Deputy Director of Highways, Region One Engineer

APPLIES TO SHEETS 1 - 40



[Signature]
REGISTERED P.E., STATE OF ILLINOIS EXPIRES 11/30/2015

APPLIES TO SHEETS 17 - 19



[Signature]
REGISTERED P.E., STATE OF ILLINOIS EXPIRES 11/30/2015



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GENERAL

- 1. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS, AND THE LATEST EDITION OF THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO AS THE "STANDARD SPECIFICATIONS"), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", THE "MANUAL OF TEST PROCEDURES FOR MATERIALS" AND THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS". ANY REFERENCE TO "STANDARDS" THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST I.D.O.T. STANDARD.
2. ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.
3. THE THICKNESSES OF HMA MIXTURES SHOWN ON THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE COURSE UPON WHICH THE HMA MATERIALS ARE PLACED.
4. THE CONTRACTOR SHALL NOTIFY THE ENGINEER, THE VILLAGE OF ADDISON (630) 620-2020, DUPAGE COUNTY DIVISION OF TRANSPORTATION (630) 407-6980, UTILITY COMPANIES, SCHOOL DISTRICTS, AND LOCAL POLICE AND FIRE DEPARTMENTS, AND SHALL SUBMIT A TRAFFIC CONTROL AUTHORIZATION REQUEST TO THE ILLINOIS DEPARTMENT OF TRANSPORTATION 72 HOURS PRIOR TO THE COMMENCEMENT OF WORK.
5. THE CONTRACTOR SHALL LIMIT ALL DROP-OFFS BETWEEN LANES TO 2" DURING ALL OVERNIGHT PERIODS.
6. NO WORK SHALL COMMENCE UNTIL TRAFFIC CONTROL REQUIREMENTS ARE MET TO THE SATISFACTION OF THE ENGINEER.
7. PROPOSED LINES AND GRADES SHOWN ON THE CONSTRUCTION PLANS REPRESENT FINISHED GRADE ELEVATIONS, UNLESS OTHERWISE NOTED.
8. THE CONTRACTOR SHALL SAW CUT PAVEMENT AND CURB & GUTTER TO SEPARATE THE EXISTING MATERIAL TO BE REMOVED BY MEANS OF AN APPROVED SAW TO A DEPTH AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE REMOVAL ITEM INVOLVED. THE COST OF ALL MATERIALS REQUIRED AND ALL LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE COST OF THE ITEMS BEING REMOVED, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
9. ALL DIMENSIONS, INCLUDING RADII, ARE GIVEN TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
10. WHEN DIRECTED BY THE ENGINEER, SUPPLEMENTAL WATERING SHALL BE APPLIED TO ALL SODDED AREAS PRIOR TO FINAL ACCEPTANCE AT A RATE SPECIFIED BY THE ENGINEER AND IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS" AND SPECIAL PROVISIONS. SHOULD THE CONTRACTOR DESIRE TO OBTAIN WATER FOR CONSTRUCTION PURPOSES FROM THE LOCAL AGENCY, THE CONTRACTOR WILL BE RESPONSIBLE FOR MAKING ARRANGEMENTS THROUGH THE LOCAL AGENCY. THE LOCAL AGENCY WILL INSTRUCT THE CONTRACTOR WHERE A POTABLE WATER SUPPLY FROM A HYDRANT NEAR THE WORK SITE IS LOCATED. THE AGENCY SHALL METER THE POTABLE WATER USED BY THE CONTRACTOR AND THE CONTRACTOR WILL BE CHARGED FOR THE WATER USED AT THE AGENCY RATES. THE CONTRACTOR IS RESPONSIBLE FOR THE TRANSPORTATION OF THE WATER TO THE SITE WHERE NEEDED. THE COST OF ALL MATERIALS REQUIRED AND ALL LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE COST OF MOBILIZATION, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
11. TEMPORARY TOILET - THE CONTRACTOR SHALL PROVIDE A TEMPORARY TOILET FACILITY FOR THE USE OF ALL CONTRACTORS' PERSONNEL EMPLOYED ON THE WORK SITE, AND SHALL MAINTAIN SAME IN PROPER SANITARY CONDITION. AT COMPLETION, THE FACILITY SHALL BE REMOVED AND THE PREMISES LEFT CLEAN. THE ENGINEER SHALL APPROVE THE LOCATION OF THE TEMPORARY TOILET. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF MOBILIZATION AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
12. WHEN REMOVING CURB AND GUTTER, PAVEMENT OR ANY OTHER STRUCTURE, THE CONTRACTOR SHALL TAKE PRECAUTIONS NECESSARY TO AVOID DAMAGE TO UNDERGROUND PUBLIC OR PRIVATE UTILITIES. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL CONCRETE BREAKER BE ALLOWED.
13. CONCRETE WASHOUT FACILITY SHOULD BE PROVIDED IN THE FIELD BY THE CONTRACTOR AT LOCATION OR LOCATIONS APPROVED BY THE ENGINEER. WASHING CONCRETE AT CURB AND GUTTER IS STRICTLY PROHIBITED. THE COST OF CONCRETE WASHOUT FACILITY SHALL BE INCLUDED IN THE COST OF THE CONCRETE WORK ITEMS INCLUDED IN THE CONTRACT. THE COST OF ALL MATERIALS REQUIRED AND ALL LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE COST OF THE ASSOCIATED CONCRETE ITEMS, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
14. THE CONTRACTOR SHALL PAY SPECIAL ATTENTION TO ARTICLE 201.01(g) OF THE STANDARD SPECIFICATIONS. REMOVAL OF ALL OBSTRUCTIONS IN THE RIGHT-OF-WAY, THAT ARE NOT INCLUDED IN A SPECIFIC REMOVAL ITEM, SHALL BE CONSIDERED CLEARING AND INCLUDED IN THE COST OF EARTH EXCAVATION. THIS SHALL INCLUDE, BUT NOT LIMITED TO, FENCES, WALLS, FOUNDATIONS, BUILDINGS, ACCUMULATIONS OF RUBBISH OF WHATEVER NATURE, OLD TYPE III BARRICADES, OLD WATER HEATERS, OLD CMP PIPE, RUBBER TIRES, CONCRETE BLOCKS, UTILITY ANCHORS, METAL PARTS, ABANDONED WOODEN POWER POLES, GATES, AND ALL VEGETATION, TREES, SHRUBS, ETC. LESS THAN 6" IN DIAMETER.
15. THE TRANSITION FROM THE PROPOSED CURB AND GUTTER SECTION TO A FLAT OR DEPRESSED SECTION (WHERE THERE IS NO CONNECTION TO EXISTING CURB AND GUTTER) SHALL BE ACCOMPLISHED IN APPROXIMATELY FIVE FEET, AND WILL BE PAID FOR AS "COMBINATION CONCRETE CURB AND GUTTER" OF THE TYPE BEING CONSTRUCTED. THE COST OF ALL MATERIALS REQUIRED AND ALL LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE COST OF THE ASSOCIATED CURB AND GUTTER ITEMS, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
16. THE CONTRACTOR SHALL OBTAIN THE DUPAGE COUNTY HIGHWAY PERMIT PRIOR TO START OF CONSTRUCTION AND PROVIDE THE REQUIRED CONTACT INFORMATION, \$2,500.00 BOND, CERTIFICATES OF INSURANCE, AND A COPY OF THE EXECUTED CONTRACT DOCUMENTS. THE COST OF ALL MATERIALS REQUIRED AND ALL LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE COST OF MOBILIZATION, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

GENERAL (CONTINUED)

- 17. THE CONTRACTOR SHALL PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION, EXCEPT FOR PERIODS OF SHORT INTERRUPTION. THE CONTRACTOR SHALL NOTIFY THE PROPERTY OWNER NO LESS THAN 24 HOURS IN ADVANCE OF THE SHORT INTERRUPTION OF ACCESS AND/OR SERVICES AND SHALL NOTIFY THE OWNER OF THE TIME AND DURATION OF THE INTERRUPTION. THE COST TO PROVIDE ACCESS SHALL BE PAID FOR AND INCLUDED IN THE ITEMS FOR TEMPORARY RAMPS.
18. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
19. CURB AND GUTTER JOINTS SHALL BE PLACED AS PER STANDARD 606001 AND IN ADDITION ONE INCH EXPANSION JOINTS SHALL BE PLACED EVERY 150 FT.
20. PRIOR TO PLACING HOT-MIX ASPHALT CONCRETE MIX ADJACENT TO EXISTING PAVEMENT TO REMAIN, THE EXPOSED EDGE SHALL BE CLEANED OF LOOSE MATERIAL TO THE SATISFACTION OF THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE HOT-MIX ASPHALT CONCRETE BEING PLACED.
21. REMOVAL AND DISPOSAL OF DEBRIS, STONES AND LANDSCAPING ITEMS TO CONSTRUCT CURB AND GUTTER, SIDE ROAD APPROACHES AND PLACING OF TOPSOIL AND SODDING, SHALL BE CONSIDERED INCLUDED IN THE COST OF "EARTH EXCAVATION."
22. IF NECESSARY, PAVEMENT PATCHING ADJACENT TO CURB REMOVAL AND REPLACEMENT SHALL INCLUDE THE INSTALLATION OF A FULL DEPTH CONCRETE POUR TO FILL ANY VOID AREAS BETWEEN THE NEW CURB AND FIRST SAW CUT TO REMOVE EXISTING CURB. SHOULD IT BE NECESSARY, A SECOND SAW CUT SHALL BE MADE TO PROVIDE A CLEAN SHARP BUTT JOINT OVER THE ENTIRE LENGTHS OF NEW CURB. THE COST OF ALL MATERIALS REQUIRED AND ALL LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE COST OF THE ASSOCIATED CURB AND GUTTER ITEMS, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
STORM SEWERS, STRUCTURES, AND UTILITIES
23. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION IS REQUIRED).
24. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES. THE LOCATION OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE ENGINEER DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATION OF SUCH UTILITIES AND EXERCISE CARE DURING HIS CONSTRUCTION OPERATIONS SO AS NOT TO DAMAGE THEM IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND APPLICABLE ARTICLES INCLUDED IN THE "STANDARD SPECIFICATIONS" INCLUDING, BUT NOT LIMITED TO, ARTICLES 105.07 AND 107.31. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITIES SO THAT THEIR FACILITIES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF THE CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL RECEIVE NO ADDITIONAL COMPENSATION FOR CONSTRUCTION STAGING NECESSARY TO ACCOMMODATE UTILITY RELOCATION OR ADJUSTMENT AND/OR FOR DELAYS CAUSED BY UTILITY RELOCATION OR ADJUSTMENT.
25. ALL EXISTING FRAMES, GRATES, AND LIDS THAT ARE BEING REPLACED SHALL REMAIN THE PROPERTY OF THE VILLAGE OF ADDISON. THE CONTRACTOR SHALL DELIVER ALL REPLACED FRAMES, GRATES AND LIDS TO THE RESPECTIVE PUBLIC WORKS FACILITY, OR AS DIRECTED BY THE ENGINEER. THE COST OF ALL MATERIALS REQUIRED AND ALL LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE COST OF THE ASSOCIATED FRAME AND GRATE ITEMS, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
26. ONLY PRECAST CONCRETE ADJUSTMENT RINGS, MAXIMUM OF 12" IN HEIGHT, WILL BE ALLOWED IN THE ADJUSTMENT OR RECONSTRUCTION OF CATCH BASIN, MANHOLE, INLET AND VALVE VAULT STRUCTURES, UNLESS INDICATED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE ENGINEER. COMMON BRICK WILL NOT BE ALLOWED. ALL TYPE 8 GRATES ON RESTRICTED DEPTH DRAINAGE STRUCTURES SHALL BE ADJUSTED TO PLAN GRADE WITH 4" MINIMUM CONCRETE ADJUSTMENT RINGS.
27. THE COST OF MAKING STORM SEWER CONNECTIONS TO EXISTING OR PROPOSED SEWER OR DRAINAGE STRUCTURES SHALL BE INCLUDED IN THE COST OF THE STORM SEWER BEING CONNECTED. ALL JOINTS IN CONCRETE SEWER PIPE SHALL BE SEALED WITH RUBBER GASKETS, PREFORMED JOINT SEALANTS, OR EXTERNAL SEALING BANDS. NO MASTIC JOINT SEALER WILL BE ALLOWED. THE COST OF ALL MATERIALS REQUIRED AND ALL LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE COST OF THE ASSOCIATED STORM SEWER ITEMS, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
28. THE CONTRACTOR SHALL MAINTAIN FLOWS THROUGH SEWER SYSTEMS AT ALL TIMES. THE EXISTING STRUCTURES SHALL BE INSPECTED BEFORE CONSTRUCTION STARTS. AS DIRECTED BY THE ENGINEER, ANY ACCUMULATION OF MATERIAL IN THE STRUCTURE DUE TO CONSTRUCTION OPERATIONS SHALL BE REMOVED BY THE CONTRACTOR AT HIS EXPENSE. THE COST OF ALL MATERIALS REQUIRED AND ALL LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE COST OF MOBILIZATION, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
29. ALL FRAMES WITH CLOSED LIDS TO BE FURNISHED AS PART OF THIS CONTRACT FOR CONSTRUCTION, ADJUSTMENT OR RECONSTRUCTION OF ANY MANHOLES, CATCH BASIN, INLET, VALVE VAULT, OR METER VAULT SHALL HAVE CAST INTO THE LID ONE OF THE FOLLOWING WORDS: ALL LIDS TO BE USED ON STORM SEWER STRUCTURES SHALL BEAR THE WORD "STORM". ALL LIDS TO BE USED ON SANITARY SEWER STRUCTURES SHALL BEAR THE WORD "SANITARY". ALL LIDS TO BE USED ON WATER SYSTEM STRUCTURES SHALL BEAR THE WORD "WATER". ALL OPEN GRATES SHALL INCLUDE THE WORDING "DUMP NO WASTE, DRAINS TO WATERWAYS". THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE STRUCTURE BEING CONSTRUCTED, ADJUSTED OR RECONSTRUCTED. THE COST OF ALL MATERIALS REQUIRED AND ALL LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE COST OF THE ASSOCIATED FRAME AND GRATE ITEMS, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
30. ALL STRUCTURES TO BE ADJUSTED SHOULD HAVE A CHIMNEY SEAL INSTALLED. ALL SANITARY MANHOLES SHALL HAVE EXTERIOR CHIMNEY SEALS AND HAVE NO MORE THAN THREE ADJUSTMENT RINGS FOR A TOTAL OF 12 INCHES. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE STRUCTURE BEING ADJUSTED.

DRIVEWAYS AND ENTRANCES

- 31. ALL HMA DRIVEWAYS SHALL BE REPLACED WITH HMA AND ALL PCC DRIVEWAYS SHALL BE REPLACED WITH PCC UNLESS OTHERWISE SHOWN ON THE PLANS OR DIRECTED BY THE ENGINEER.
32. ALL EXCAVATION REQUIRED TO CONSTRUCT NEW DRIVEWAYS SHALL BE PAID FOR AS "DRIVEWAY PAVEMENT REMOVAL".
33. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LAYOUT OF EACH DRIVEWAY BASED UPON STANDARD DETAILS.
34. WHEN THE PCC SIDEWALK EXTENDS THROUGH DRIVEWAYS AND FIELD ENTRANCES, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS (MIN 6"). SIDEWALK WILL BE PAID FOR AS PCC SIDEWALK 5" REGARDLESS OF THE THICKNESS. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAX OF 1:50.
35. ALL DRIVEWAYS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE IDOT DRIVEWAY DETAILS AND STANDARD SPECIFICATIONS FOR DESIGN AND CONSTRUCTION, WITH THE ADDITION OF A 6"x6" NO.6 WIRE MESH AND 4" COMPACTED CA-6 BASE. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF PCC DRIVEWAYS.
SIGNING, STRIPING, AND LANDSCAPING
36. THE CONTRACTOR WILL BE REQUIRED TO RELOCATE OR REMOVE AND REPLACE SIGNS WHICH INTERFERE WITH HIS CONSTRUCTION OPERATIONS, AND TO TEMPORARILY RESET ALL SUCH SIGNS DURING CONSTRUCTION OPERATIONS. THIS WORK WILL BE INCLUDED IN THE COST OF THE ASSOCIATED TRAFFIC CONTROL AND PROTECTION ITEMS.
37. ANY SIGNS WHICH ARE DAMAGED BEYOND REPAIR DURING CONSTRUCTION OPERATIONS SHALL BE REPLACED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST TO THE CONTRACT.
38. THE CONTRACTOR SHALL ADHERE TO LIMITS OF RESTORATION SHOWN, INCLUDING CONCRETE WASHOUT FACILITIES. AREAS OUTSIDE THESE LIMITS THAT ARE DAMAGED OR DISTURBED BY THE CONTRACTOR, SHALL BE RESTORED BY THE CONTRACTOR AT HIS EXPENSE, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
39. PHOSPHORUS FERTILIZER NUTRIENT SHALL NOT BE USED.
40. ALL EXISTING SIGNS SHALL CONFORM TO THE CURRENT EDITION OF "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS & HIGHWAYS." THOSE EXISTING SIGNS THAT DO NOT CONFORM SHALL BE REPLACED AS DIRECTED BY THE ENGINEER, AND SHALL BE PAID FOR AS "SIGN PANEL - TYPE 1".

SURVEY

- 41. VERTICAL CONTROL REFERENCED TO NAVD 88.

BENCHMARK TABLE

Table with 7 columns: BM#, NORTHING, EASTING, ELEVATION, DESCRIPTION, STATION, OFFSET. Rows include BM#1, BM#3, and BM#6 with their respective coordinates and descriptions.

- 42. BEARINGS ARE REFERENCED TO THE ILLINOIS STATE PLANE COORDINATE SYSTEM NAD83 (2007) EAST ZONE. GRID COORDINATES ARE SHOWN.

POINTS OF CONTACT

VILLAGE OF ADDISON: MR. RUDOLFO ESPEDIDO, P.E., VILLAGE ENGINEER, ONE FRIENDSHIP PLAZA, ADDISON, IL 60101, Respedido@addison-il.org (630) 693-7533
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AT&T CORPORATION: LEGAL MANDATE ENGINEERING, 1000 COMMERCE DRIVE, FLOOR 2, OAK BROOK, IL 60523, ag1852@att.com (630) 573-5703
VILLAGE OF ADDISON POLICE DEPARTMENT: 3 FRIENDSHIP PLAZA, ADDISON, IL 60101, (630) 543-3080
VILLAGE OF ADDISON FIRE DEPARTMENT: 10 S. ADDISON ROAD, ADDISON, IL 60101, (630) 628-3100
DUPAGE COUNTY DIVISION OF TRANSPORTATION: MR. ROBERT J. KOLAR, HIGHWAY PERMITTING DEPARTMENT - MANAGER, 421 N. COUNTY FARM ROAD, WHEATON, IL 60187-2553, (630) 407-6900
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VILLAGE OF ADDISON: 222 N. KENNEDY DRIVE, ADDISON, IL 60101-2497, (630) 458-2500
DUPAGE HIGH SCHOOL DISTRICT 88: 2 FRIENDSHIP PLAZA, ADDISON, IL 60101, (630) 530-3981
DUPAGE WATER COMMISSION: MR. MICHAEL SCHWEIZER, 600 EAST BUTTERFIELD ROAD, ELMHURST, IL 60126, (630) 834-0100

GENERAL NOTES

SHEET NO. 1 OF 1 SHEETS

CIVILTECH logo and contact information: 450 E Devon Ave, Suite 300, Itasca, Illinois 60143, Tel: 630.773.3900 Fax: 630.773.3975, www.civiltechinc.com

Table with 2 columns: ACTION (DESIGNED, DRAWN, CHECKED, DATE) and STATUS (RTM, REVISED).

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

Table with 4 columns: F.A.U. RTE., SECTION, COUNTY, TOTAL SHEETS, SHEET NO. Values: 2586, 13-00102-00-RS, DUPAGE, 40, 2. CONTRACT NO. 61A14. FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT.

SUMMARY OF QUANTITIES

CODED PAY ITEM NO.	PAY ITEM	UNIT	TOTAL QUANTITY	0005 ROADWAY
20200100	EARTH EXCAVATION	CU YD	17	17
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	350	350
21101685	TOPSOIL FURNISH AND PLACE, 24"	SQ YD	108	108
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	5	5
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	5	5
25200110	SODDING, SALT TOLERANT	SQ YD	350	350
25200200	SUPPLEMENTAL WATERING	UNIT	5	5
28000510	INLET FILTERS	EACH	64	64
35101500	AGGREGATE BASE COURSE, TYPE B	CU YD	33	33
35800200	AGGREGATE BASE REPAIR	TON	322	322
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	15	15
40600895	CONSTRUCTING TEST STRIP	EACH	2	2
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	270	270
40600990	TEMPORARY RAMP	SQ YD	172	172
40603240	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90	TON	6,071	6,071
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX 'D', N50	TON	5	5
40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX 'F', N90	TON	4,722	4,722
42001300	PROTECTIVE COAT	SQ YD	1,281	1,281
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	115	115
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	4,241	4,241
42400800	DETECTABLE WARNINGS	SQ FT	200	200
44000100	PAVEMENT REMOVAL	SQ YD	108	108
44000165	HOT-MIX ASPHALT SURFACE REMOVAL, 4"	SQ YD	48,179	48,179
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	78	78
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	100	100
44000600	SIDEWALK REMOVAL	SQ FT	2,350	2,350
44201749	CLASS D PATCHES, TYPE I, 9 INCH	SQ YD	240	240
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	964	964
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SQ YD	2,891	2,891
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SQ YD	723	723
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	14,550	14,550
56400300	FIRE HYDRANTS TO BE ADJUSTED	EACH	1	1
60404800	FRAMES AND GRATES, TYPE 11	EACH	1	1
60404950	FRAMES AND GRATES, TYPE 24	EACH	19	19
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	4	4
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	18	18

^ DENOTES SPECIALTY ITEM
 ^^ DENOTES CONSTRUCTION TYPE CODE 0042
 * DENOTES SPECIAL PROVISION

CODED PAY ITEM NO.	PAY ITEM	UNIT	TOTAL QUANTITY	0005 ROADWAY
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	153	153
67100100	MOBILIZATION	L SUM	1	1
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	6	6
70300100	SHORT TERM PAVEMENT MARKING	FOOT	5,808	5,808
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	1,035	1,035
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	25,126	25,126
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	3,135	3,135
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	115	115
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1,790	1,790
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	315	315
72000100	SIGN PANEL - TYPE 1	SQ FT	50	50
72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	2	2
78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	1,035	1,035
78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	25,126	25,126
78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3,135	3,135
78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	115	115
78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1,790	1,790
78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	315	315
78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	15	15
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	9	9
88600600	DETECTOR LOOP REPLACEMENT	FOOT	717	717
89502376	REBUILD EXISTING HANDHOLE	EACH	3	3
X4060110	BITUMINOUS MATERIALS (PRIME COAT)	POUND	32,520	32,520
X4400220	CURB REMOVAL AND REPLACEMENT	FOOT	2,800	2,800
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	23	23
X7800100	PAINT PAVEMENT MARKING - RAISED MEDIAN	SQ FT	786	786
XX003668	PRECONSTRUCTION VIDEO TAPING	L SUM	1	1
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1
Z0017400	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED	EACH	15	15
Z0017700	DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED	EACH	5	5
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	201	201
Z0076600	TRAINEES	HOUR	500	500
Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOUR	500	500

PLAN SURVEYED BY DATE
 PLOTTED BY DATE
 NOTE BOOK NO. OF WAY CHECKED
 NO. ROAD FILE NAME

PROFILE SURVEYED BY DATE
 PLOTTED BY DATE
 NOTE BOOK NO. OF WAY CHECKED
 NO. STRUCTURE NOTATIONS CHECKED



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 DRAWN - JRR
 CHECKED - RTM
 DATE - 2/03/2014

REVISED -
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 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

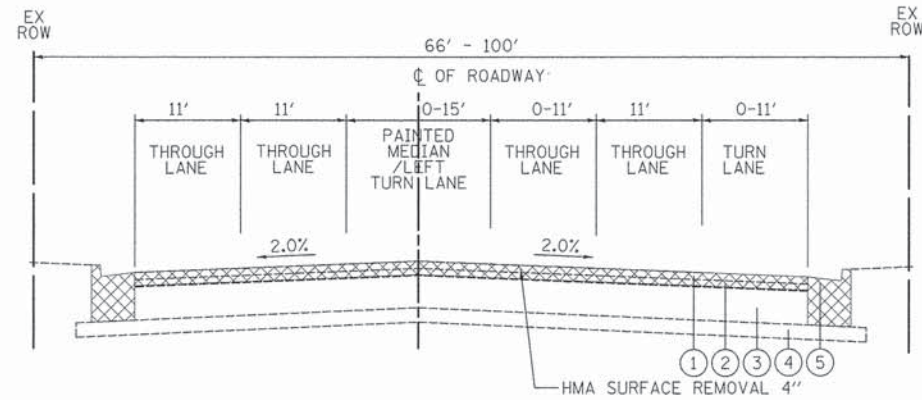
SUMMARY OF QUANTITIES

SHEET NO. 1 OF 1 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2586	13-00102-00-RS		40	3
DUPAGE			CONTRACT NO. 61A14	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

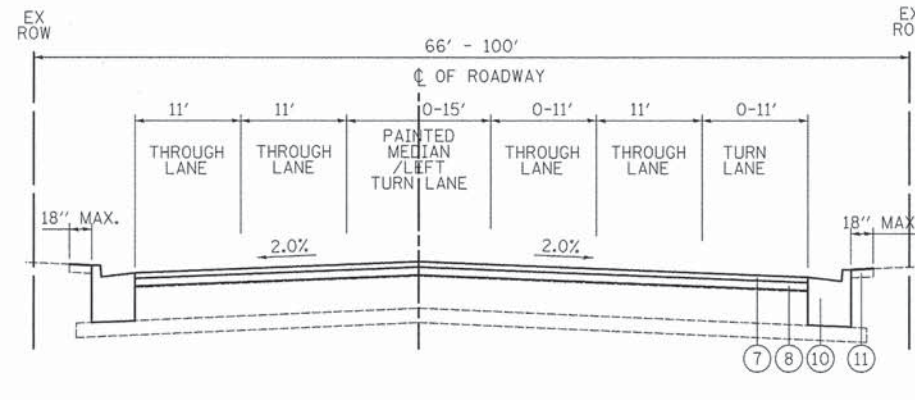
LEGEND

- ① EXISTING BITUMINOUS CONCRETE SURFACE COURSE, 1" TO 2" (REFERENCE ROADWAY SOIL INVESTIGATION REPORTS PREPARED BY: CHICAGO TESTING LABORATORY, DATED JUNE 21, 2013 AND TESTING SERVICE CORPORATION, DATED JULY 24, 2013)
- ② EXISTING BITUMINOUS CONCRETE BINDER COURSE, 2" (REFERENCE ROADWAY SOIL INVESTIGATION REPORTS PREPARED BY: CHICAGO TESTING LABORATORY, DATED JUNE 21, 2013 AND TESTING SERVICE CORPORATION, DATED JULY 24, 2013)
- ③ EXISTING BITUMINOUS BASE COURSE, 9" (REFERENCE ROADWAY SOIL INVESTIGATION REPORTS PREPARED BY: CHICAGO TESTING LABORATORY, DATED JUNE 21, 2013 AND TESTING SERVICE CORPORATION, DATED JULY 24, 2013)
- ④ EXISTING GRANULAR SUBBASE TYPE B, 4" TO 6" (REFERENCE ROADWAY SOIL INVESTIGATION REPORTS PREPARED BY: CHICAGO TESTING LABORATORY, DATED JUNE 21, 2013 AND TESTING SERVICE CORPORATION, DATED JULY 24, 2013)
- ⑤ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- ⑥ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑦ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1-3/4"
- ⑧ PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90, 2-1/4"
- ⑨ PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- ⑩ PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑪ PROPOSED SODDING, SALT TOLERANT
PROPOSED TOPSOIL FURNISH AND PLACE, 4"



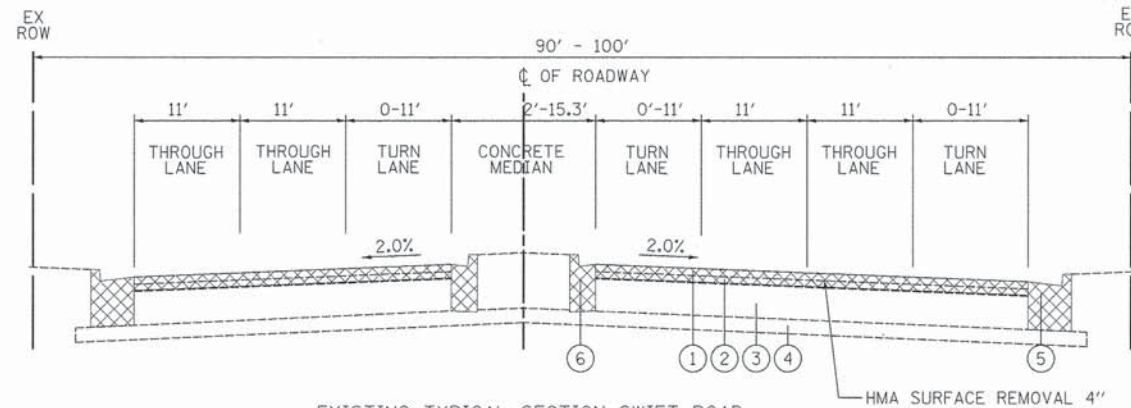
EXISTING TYPICAL SECTION SWIFT ROAD

STA 70+18 TO STA 91+24
 STA 99+06 TO STA 108+24
 STA 111+49 TO STA 116+49
 STA 131+50 TO STA 139+75.8



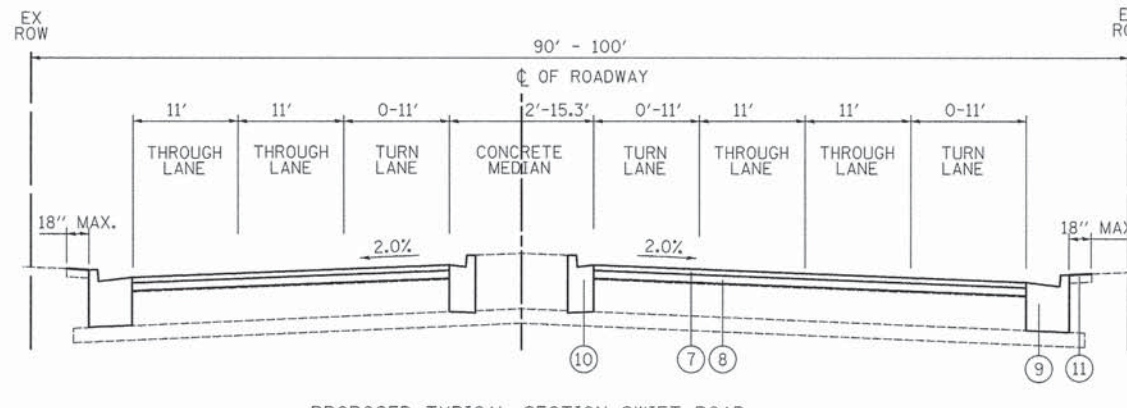
PROPOSED TYPICAL SECTION SWIFT ROAD

STA 70+18 TO STA 91+24
 STA 99+06 TO STA 108+24
 STA 111+49 TO STA 116+49
 STA 131+50 TO STA 139+75.8



EXISTING TYPICAL SECTION SWIFT ROAD

STA 67+00 TO STA 70+18
 STA 124+00 TO STA 131+50

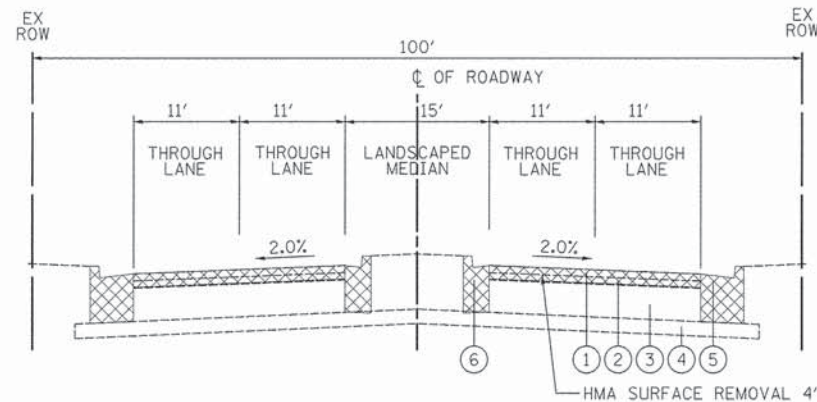


PROPOSED TYPICAL SECTION SWIFT ROAD

STA 67+00 TO STA 70+18
 STA 124+00 TO STA 131+50

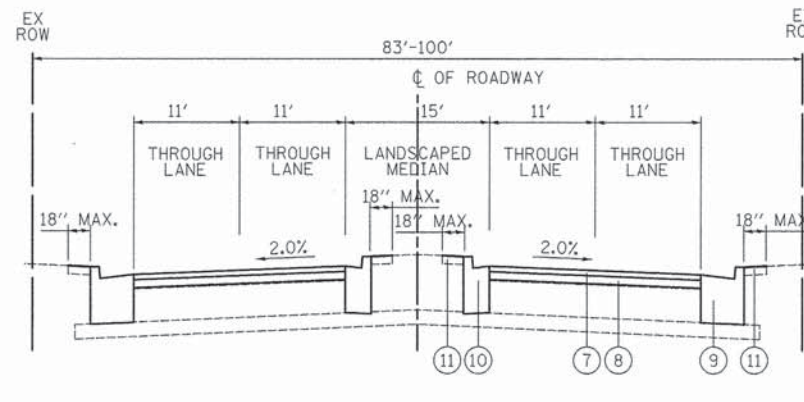
NOTES:

- 1. COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT LIMITS WILL BE DETERMINED BY THE FIELD ENGINEER. ALL CURB AND GUTTER TO BE REMOVED WILL BE REPLACED IN KIND (REGARDLESS OF TYPE) AND PAID FOR AT THE SAME UNIT BID PRICE FOR "CURB REMOVAL AND REPLACEMENT". (SEE PLANS FOR SPOT REPAIRS)
- 2. CONTRACTOR SHALL MILL FIRST BEFORE PATCHING.



EXISTING TYPICAL SECTION SWIFT ROAD

STA 91+24 TO STA 99+06
 STA 108+24 TO STA 111+49
 STA 116+49 TO STA 124+00



PROPOSED TYPICAL SECTION SWIFT ROAD

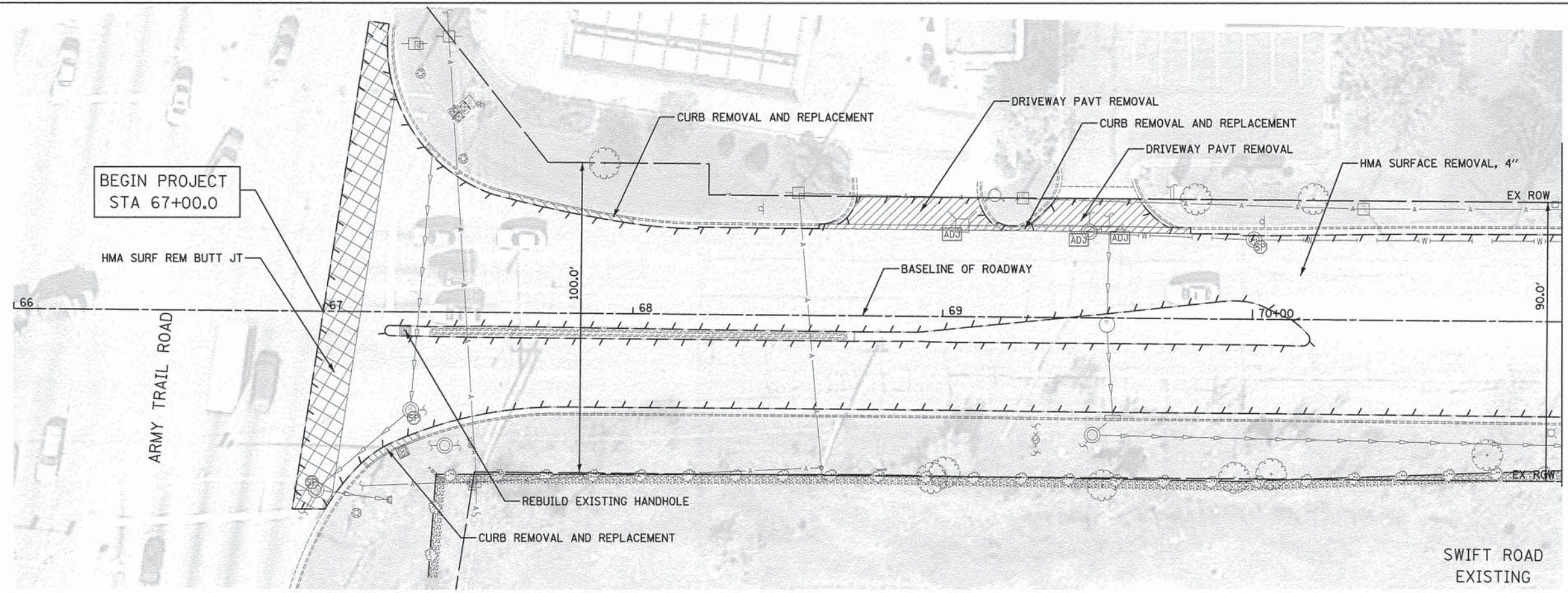
STA 91+24 TO STA 99+06
 STA 108+24 TO STA 111+49
 STA 116+49 TO STA 124+00

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AIR VOIDS @ Ndes	QUALITY MANAGEMENT PROGRAM (QMP)
PAVEMENT RESURFACING		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL 9.5 mm); 1-3/4" (IN 1 LIFT)	4% @ 90 Gyr.	QCP
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90; 2-1/4" (IN 1 LIFT)	4% @ 90 Gyr.	QCP
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 mm); 9" (IN 3 LIFTS)	4% @ 70 Gyr.	QCP
DRIVEWAY		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-19 mm); 2" (IN 1 LIFT)	4% @ 50 Gyr.	Qc/Qa
QMP Options: Quality Control of Quality Assurance (QC/QA); Quality Control for Performance (QCP); Pay for Performance (PFP)		

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
 THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
 FOR HMA FULL DEPTH "AC TYPE" SEE SPECIAL PROVISIONS.
 FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
 QUALITY MANAGEMENT PROGRAM (CMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

PLAN	SURVEYED	DATE
	ALIGNED	
	CHECKED	
	BY	
	NO. OF DAY CHECKED	
	DATE	

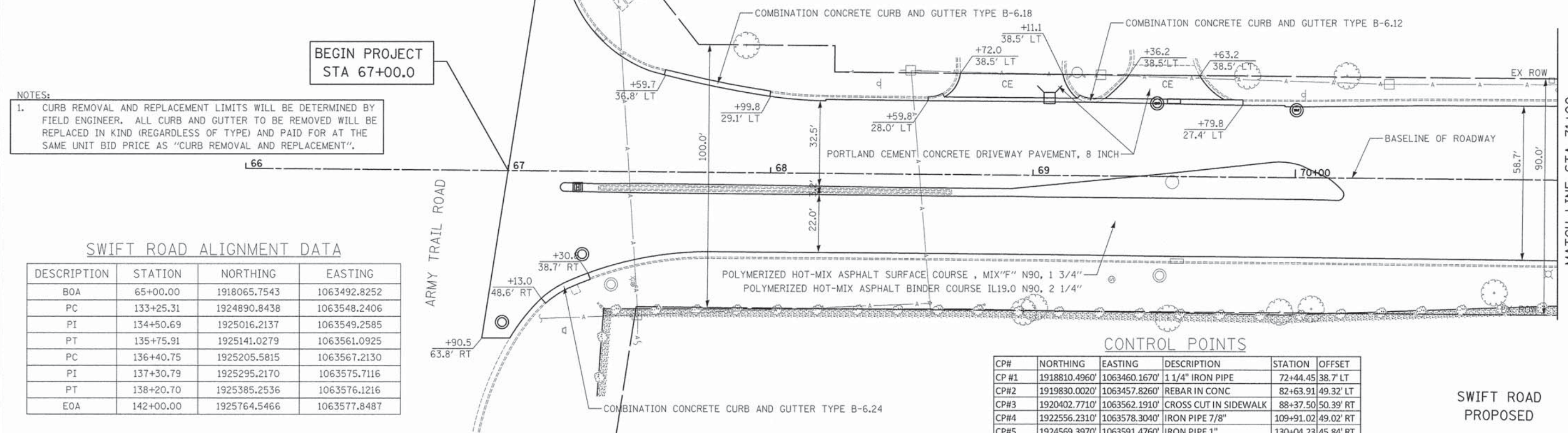
PROFILE	SURVEYED	DATE
	UPGRADES CHECKED	
	BY	
	NO. OF DAY CHECKED	
	DATE	



- LEGEND:**
- HMA SURFACE REMOVAL, 4"
 - HMA SURFACE REMOVAL BUTT JOINT
 - REMOVAL
 - DETECTABLE WARNING AND DEPRESSED CURB LOCATION (REFERENCE THE LATEST IDOT STANDARD 4240001 FOR PLACEMENT)
 - FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
 - DRAINAGE AND UTILITY STRUCTURES TO BE ADJUSTED
 - WATER METER
 - FIRE HYDRANT
 - HAND HOLE
 - CATCH BASIN
 - MANHOLE
 - INLET

BENCHMARKS

BM#	NORTHING	EASTING	ELEVATION	DESCRIPTION	STATION	OFFSET
BM#1	1918157.1600'	1063425.4880'	735.48'	Arrow bolt FH at SW Quadrant of Army Trail Road and Swift Road	65+90.86	68.08' LT
BM#3	1920568.2200'	1063476.8460'	742.85'	NW BB FH at NW Quadrant of Cimarron Way and Swift Road	90+02.25	36.3' LT
BM#6	1924544.5470'	1063461.0610'	735.96'	NE BB FH at NW Quadrant Pinehurst Blvd and Swift Road	129+78.32	84.37' LT



NOTES:
 1. CURB REMOVAL AND REPLACEMENT LIMITS WILL BE DETERMINED BY FIELD ENGINEER. ALL CURB AND GUTTER TO BE REMOVED WILL BE REPLACED IN KIND (REGARDLESS OF TYPE) AND PAID FOR AT THE SAME UNIT BID PRICE AS "CURB REMOVAL AND REPLACEMENT".

SWIFT ROAD ALIGNMENT DATA

DESCRIPTION	STATION	NORTHING	EASTING
BOA	65+00.00	1918065.7543	1063492.8252
PC	133+25.31	1924890.8438	1063548.2406
PI	134+50.69	1925016.2137	1063549.2585
PT	135+75.91	1925141.0279	1063561.0925
PC	136+40.75	1925205.5815	1063567.2130
PI	137+30.79	1925295.2170	1063575.7116
PT	138+20.70	1925385.2536	1063576.1216
EOA	142+00.00	1925764.5466	1063577.8487

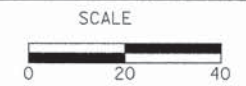
CONTROL POINTS

CP#	NORTHING	EASTING	DESCRIPTION	STATION	OFFSET
CP #1	1918810.4960'	1063460.1670'	1 1/4" IRON PIPE	72+44.45	38.7' LT
CP#2	1919830.0020'	1063457.8260'	REBAR IN CONC	82+63.91	49.32' LT
CP#3	1920402.7710'	1063562.1910'	CROSS CUT IN SIDEWALK	88+37.50	50.39' RT
CP#4	1922556.2310'	1063578.3040'	IRON PIPE 7/8"	109+91.02	49.02' RT
CP#5	1924569.3970'	1063591.4760'	IRON PIPE 1"	130+04.23	45.84' RT



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CHECKED - RTM	REVISED -
DATE - 12/05/2013	REVISED -

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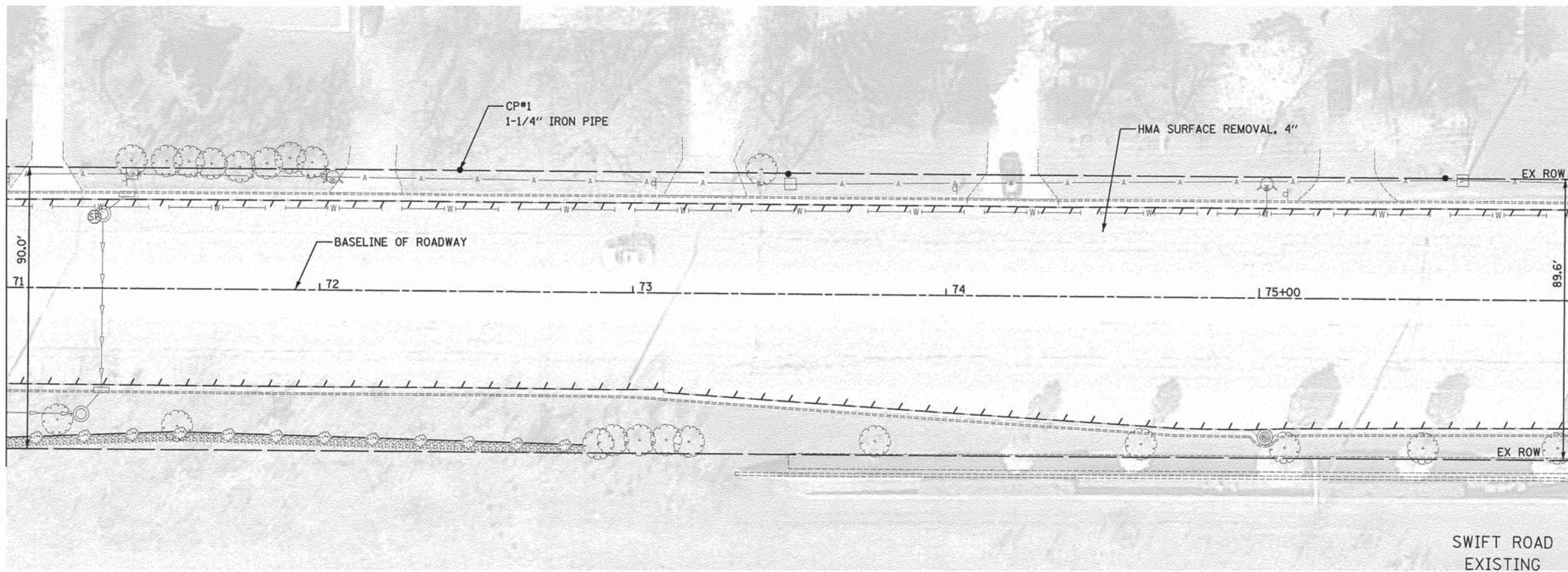
ROADWAY PLAN

SHEET NO. 1 OF 15 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2586	13-00102-00-RS	DUPAGE	40	5
CONTRACT NO. 61A14			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



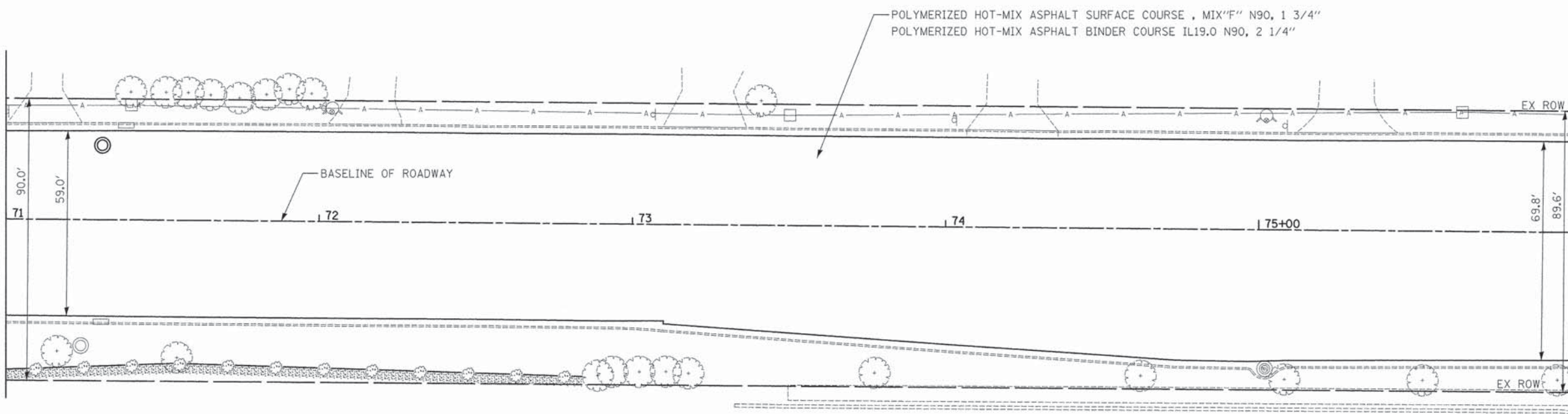
MATCH LINE STA 71+00
SEE SHEET NO. 5



MATCH LINE STA 76+00
SEE SHEET NO. 7

SWIFT ROAD
EXISTING

MATCH LINE STA 71+00
SEE SHEET NO. 5



MATCH LINE STA 76+00
SEE SHEET NO. 7

SWIFT ROAD
PROPOSED

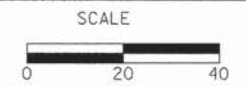
PLAN	REVISION	DATE
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NO.	NO.	
NO.	NO.	
NO.	NO.	
NO.	NO.	
NO.	NO.	
NO.	NO.	
NO.	NO.	
NO.	NO.	

PROFILE	REVISION	DATE
NO.	NO.	
NO.	NO.	
NO.	NO.	
NO.	NO.	
NO.	NO.	
NO.	NO.	
NO.	NO.	
NO.	NO.	
NO.	NO.	

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DRAWN - JRR	REVISED -
CHECKED - RTM	REVISED -
DATE - 12/05/2013	REVISED -

**STATE OF ILLINOIS
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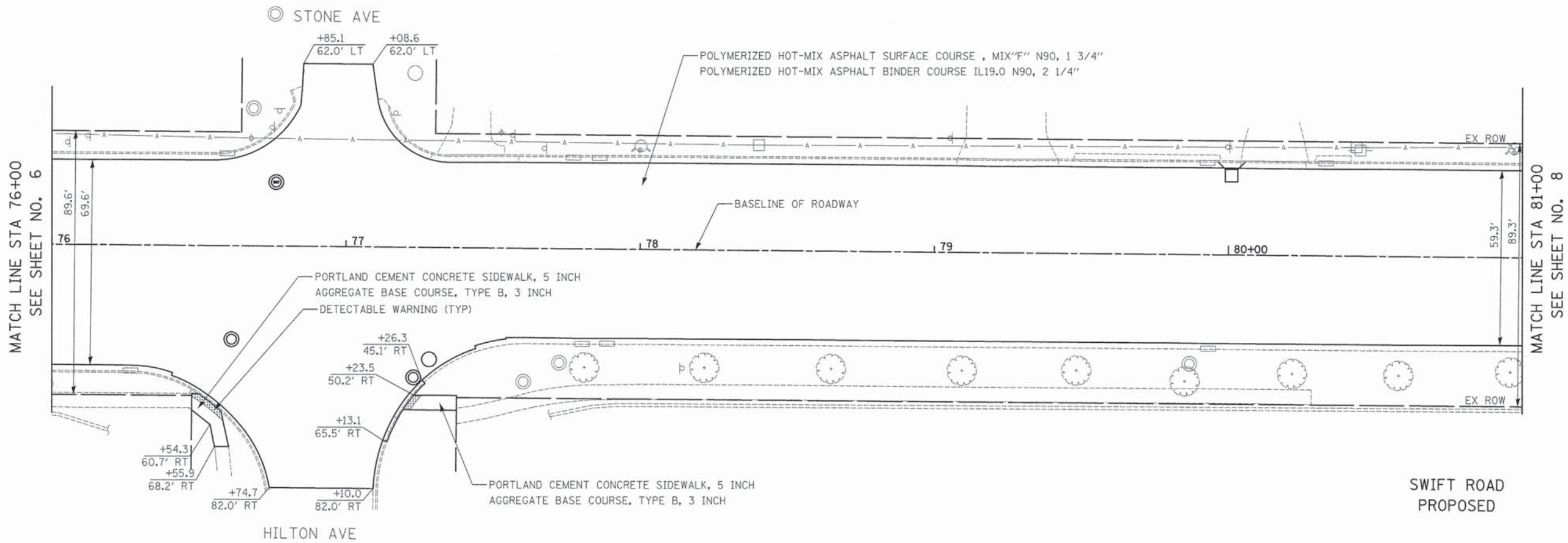
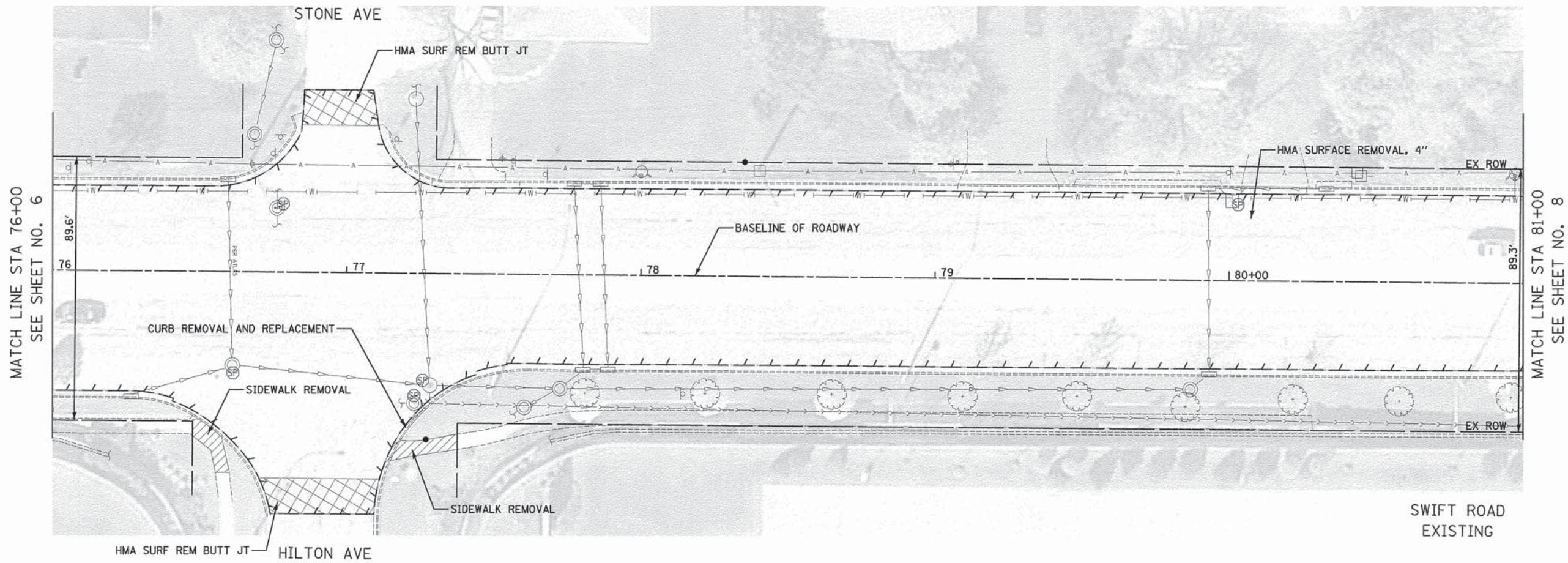
ROADWAY PLAN

SHEET NO. 2 OF 15 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2586	13-00102-00-RS	DUPAGE	40	6
				CONTRACT NO. 61A14
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PLAN	REVISED	DATE
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	NOTED	
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	NOTED	
	NOTED	
	NOTED	
	NOTED	

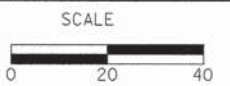
PROFILE	REVISED	DATE
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	NOTED	
	NOTED	
	NOTED	
	NOTED	
	NOTED	
	NOTED	



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DRAWN	- JRR	REVISED	-
CHECKED	- RTM	REVISED	-
DATE	- 12/05/2013	REVISED	-

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**



ROADWAY PLAN

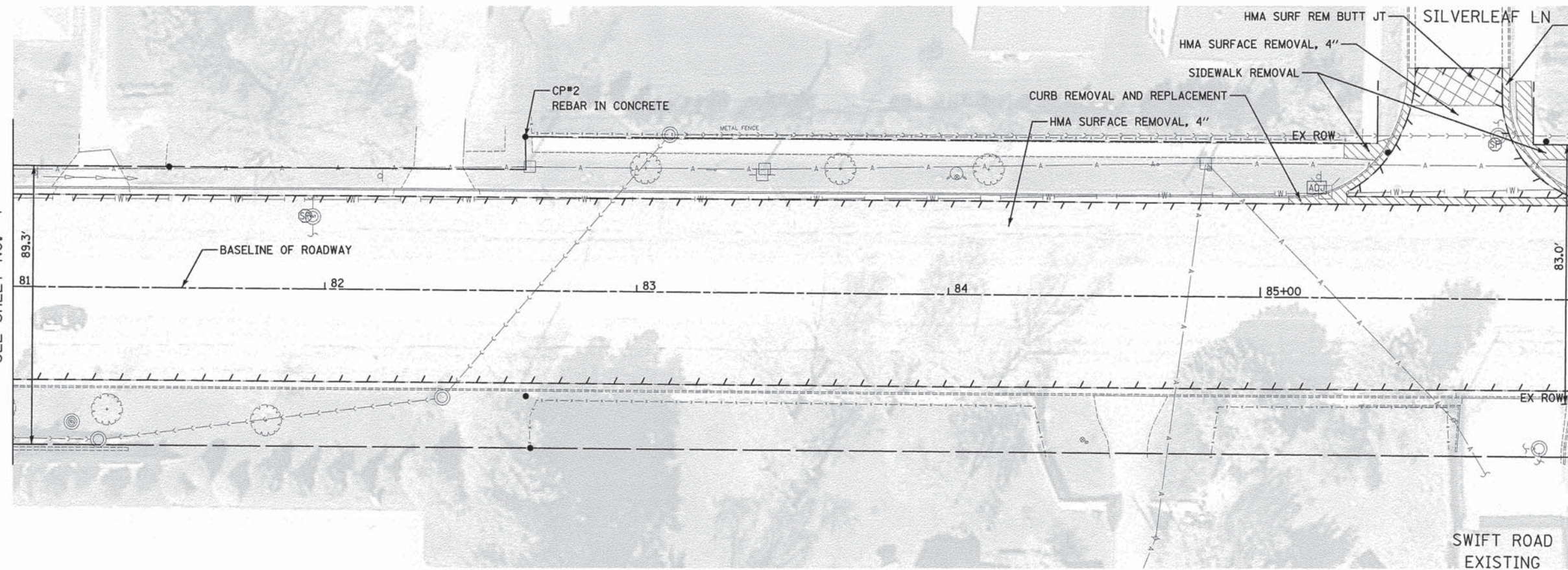
SHEET NO. 3 OF 15 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2586	13-00102-00-RS	DUPAGE	40	7
CONTRACT NO. 61A14				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PLAN	DATE
SUBMITTED	BY
NOTED	DATE
ALIGNMENT CHECKED	
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DATE	
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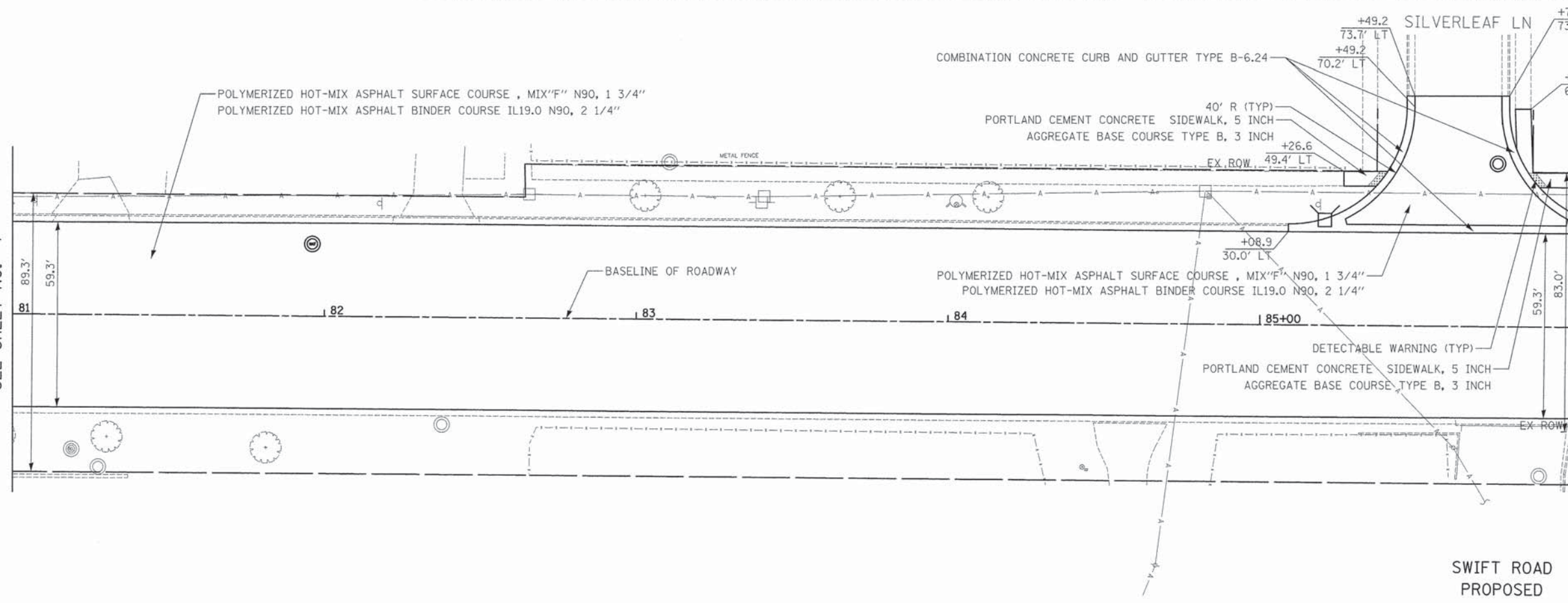
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SUBMITTED	BY
NOTED	DATE
GRADES CHECKED	
BY	
DATE	
NO.	

MATCH LINE STA 81+00
SEE SHEET NO. 7



MATCH LINE STA 86+00
SEE SHEET NO. 9

MATCH LINE STA 81+00
SEE SHEET NO. 7



MATCH LINE STA 86+00
SEE SHEET NO. 9

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DRAWN - JRR	REVISED -
CHECKED - RTM	REVISED -
DATE - 12/05/2013	REVISED -

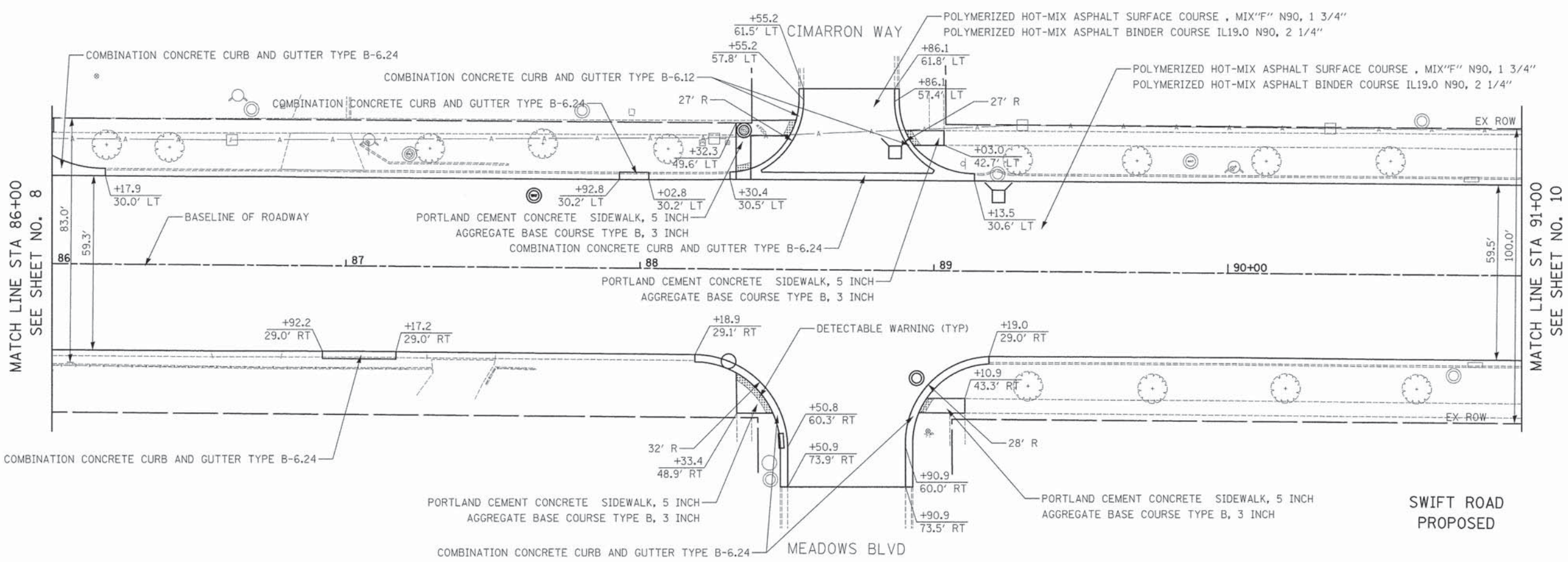
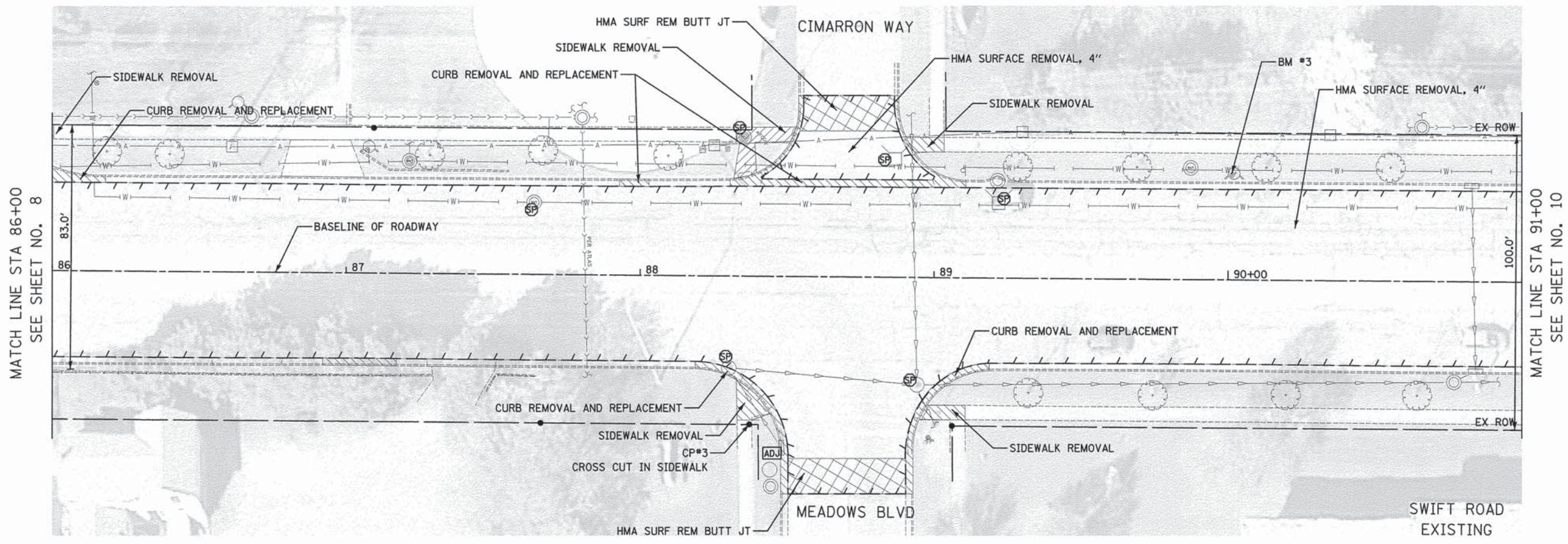
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



ROADWAY PLAN

SHEET NO. 4 OF 15 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2586	13-00102-00-RS	DUPAGE	40	8
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 61A14	



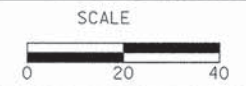
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DATE	_____
BY	_____
DESIGNED	_____
DRAWN	_____
CHECKED	_____
IN CHARGE	_____

PROFILE	NO. _____
DATE	_____
BY	_____
DESIGNED	_____
DRAWN	_____
CHECKED	_____
IN CHARGE	_____

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DRAWN - JRR	REVISED -
CHECKED - RTM	REVISED -
DATE - 12/05/2013	REVISED -

**STATE OF ILLINOIS
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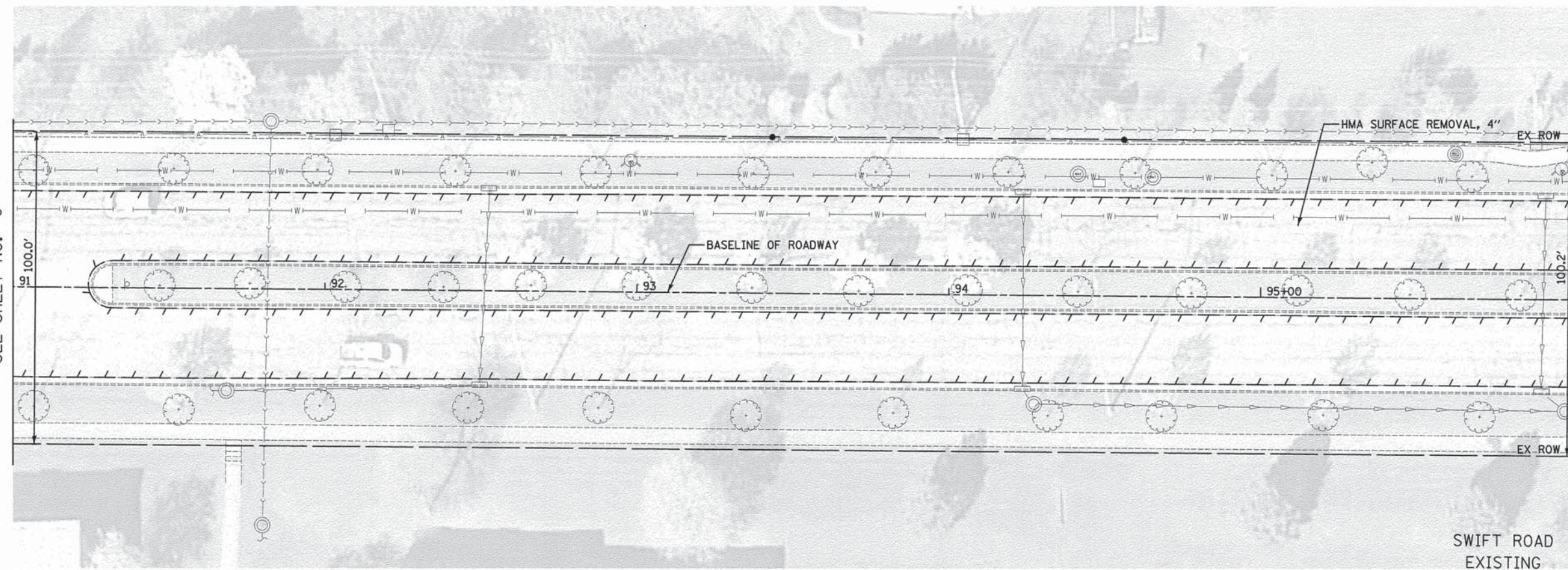
ROADWAY PLAN

SHEET NO. 5 OF 15 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 61A14	



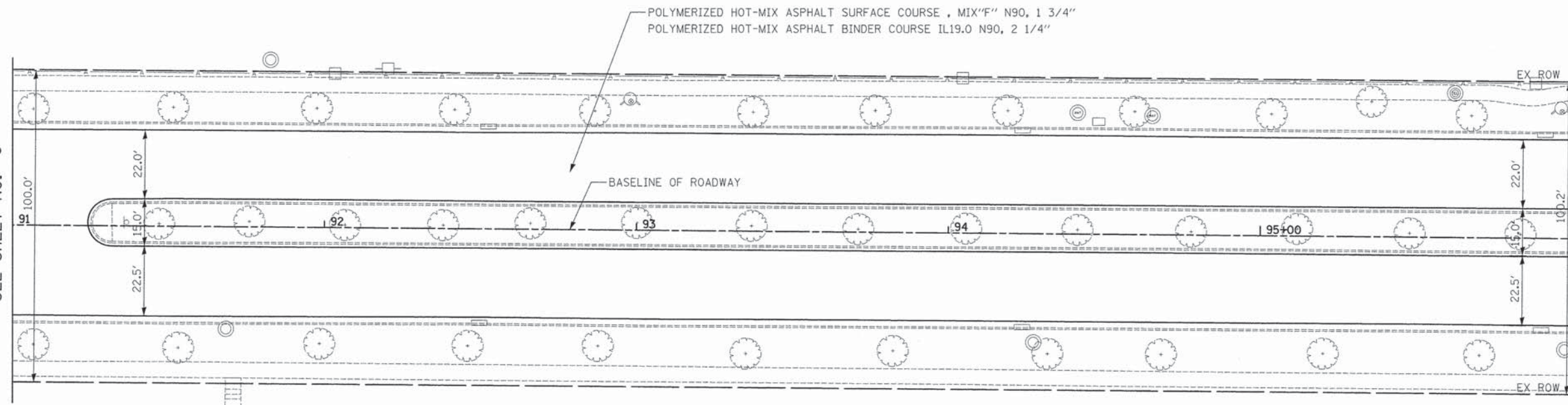
MATCH LINE STA 91+00
SEE SHEET NO. 9



MATCH LINE STA 96+00
SEE SHEET NO. 11

SWIFT ROAD
EXISTING

MATCH LINE STA 91+00
SEE SHEET NO. 9



MATCH LINE STA 96+00
SEE SHEET NO. 11

SWIFT ROAD
PROPOSED

PLAN	REVISIONS	DATE
NO.	NO.	
BY	BY	
DATE	DATE	
NO.	NO.	
BY	BY	
DATE	DATE	
NO.	NO.	
BY	BY	
DATE	DATE	
NO.	NO.	

PROFILE	REVISIONS	DATE
NO.	NO.	
BY	BY	
DATE	DATE	
NO.	NO.	
BY	BY	
DATE	DATE	
NO.	NO.	
BY	BY	
DATE	DATE	
NO.	NO.	

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DRAWN -	JRR	REVISED -	
CHECKED -	RTM	REVISED -	
DATE -	12/05/2013	REVISED -	

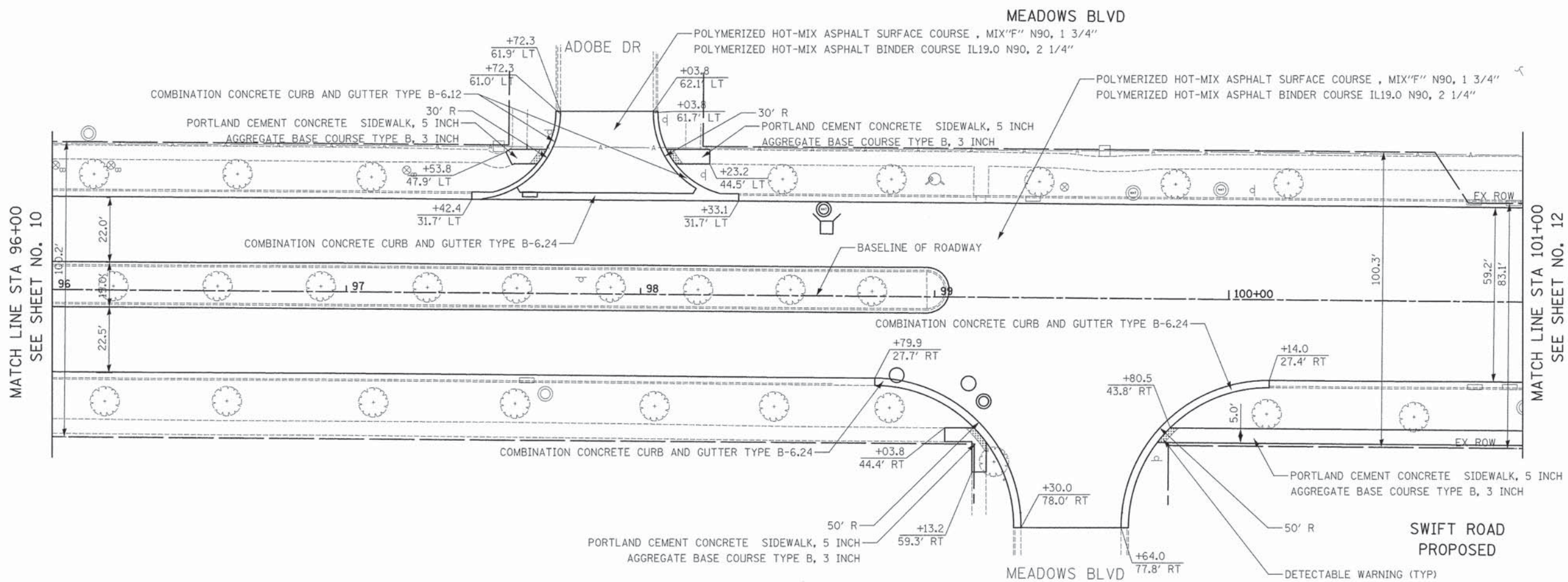
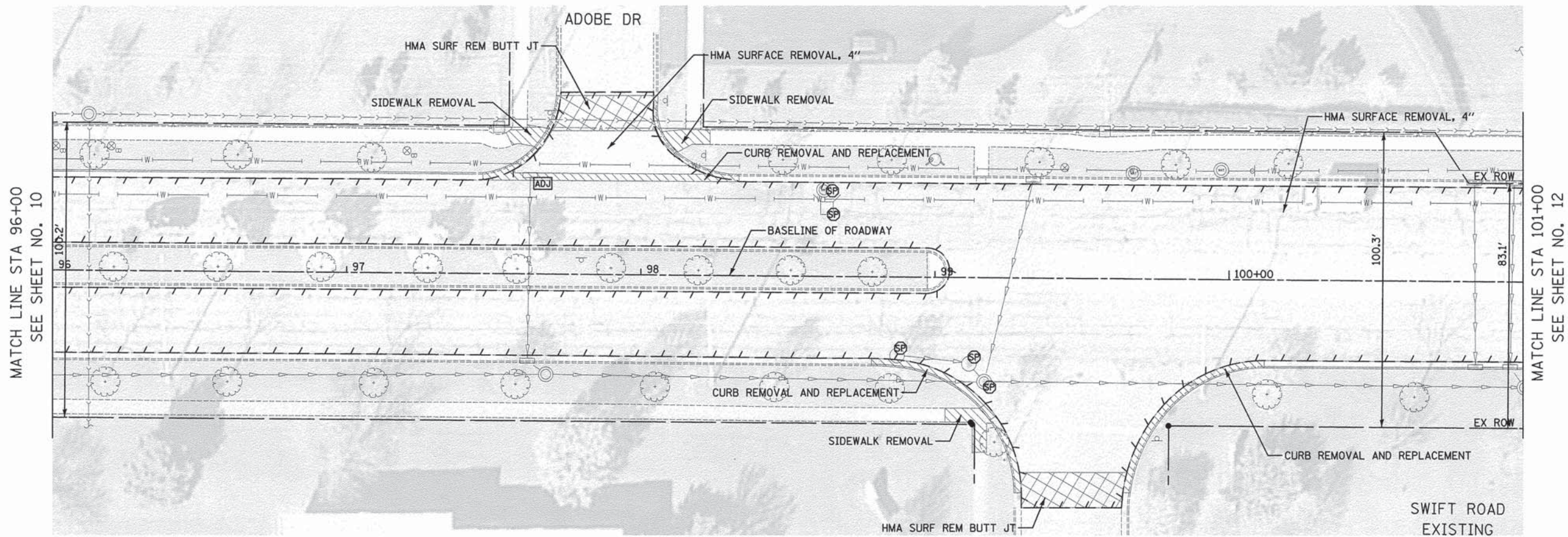
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



ROADWAY PLAN

SHEET NO. 6 OF 15 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2586	13-00102-00-RS	DUPAGE	40	10
CONTRACT NO. 61A14				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



PLAN	DATE
BY	
CHECKED	
DESIGNED	
DRAWN	
NOTED	
NO.	

PROFILE	DATE
BY	
CHECKED	
DESIGNED	
DRAWN	
NOTED	
NO.	

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DRAWN - JRR	REVISED -
CHECKED - RTM	REVISED -
DATE - 12/05/2013	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**



ROADWAY PLAN

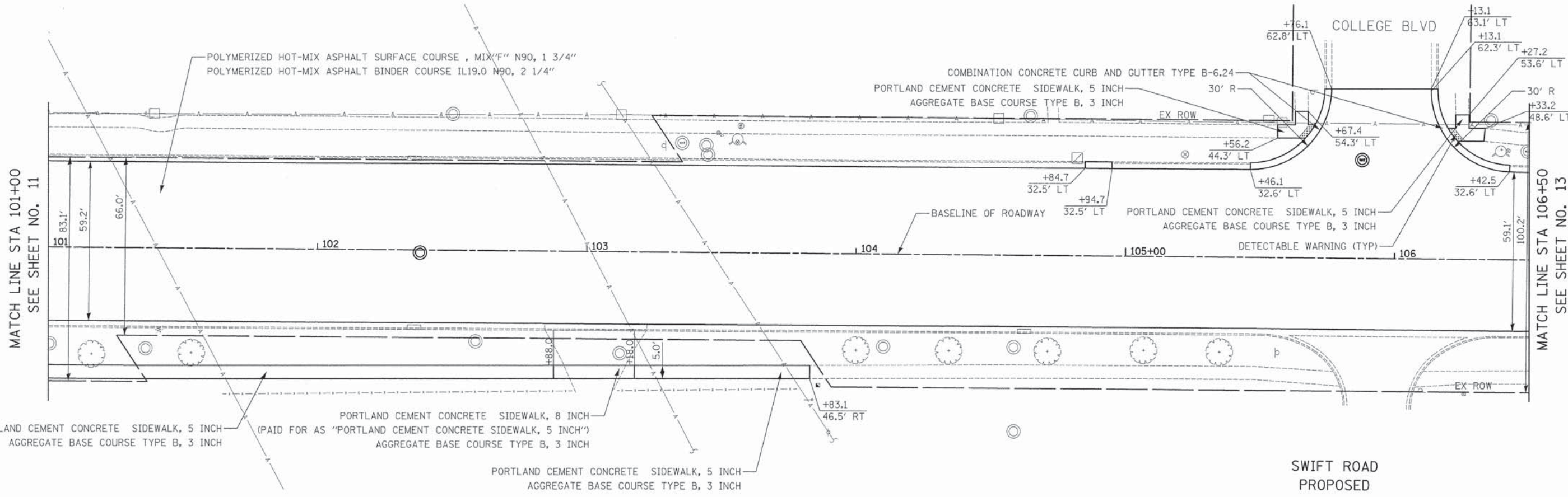
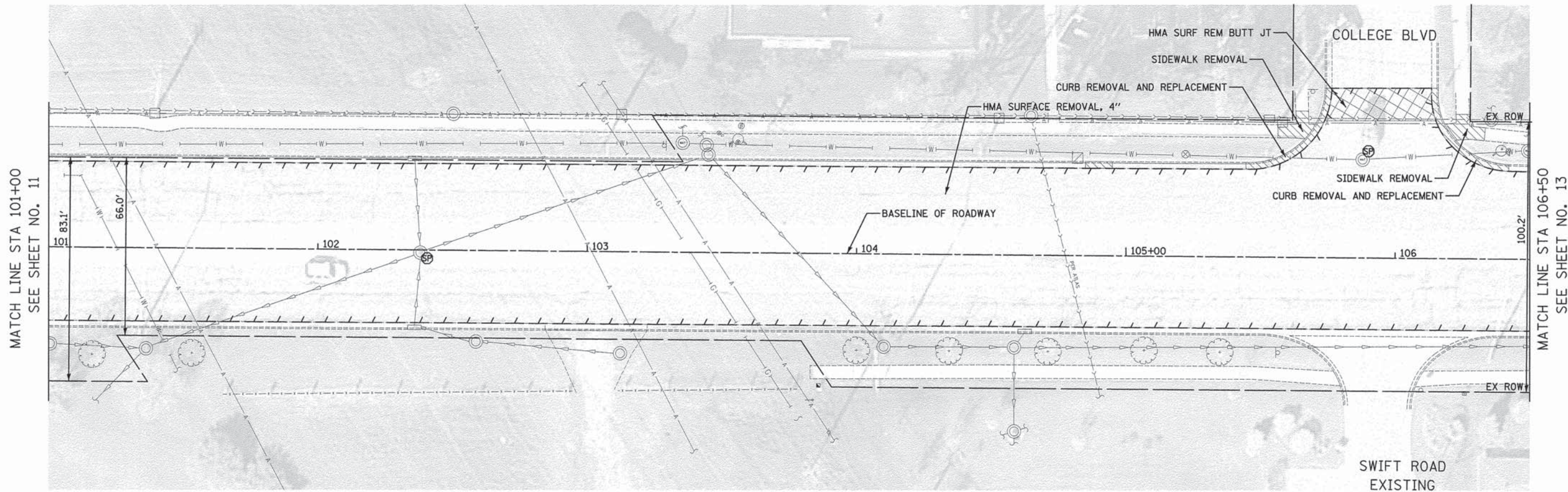
SHEET NO. 7 OF 15 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2586	13-00102-00-RS	DUPAGE	40	11
CONTRACT NO. 61A14				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



PLAN	SURVEYED	BY	DATE
	NOTED		
	ALIGNED		
	CHECKED		
	BY		
	DATE		

PROFILE	SURVEYED	BY	DATE
	NOTED		
	ALIGNED		
	CHECKED		
	BY		
	DATE		



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DRAWN -	JRR	REVISED -	
CHECKED -	RTM	REVISED -	
DATE -	12/05/2013	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



ROADWAY PLAN

SHEET NO. 8 OF 15 SHEETS STA. TO STA.

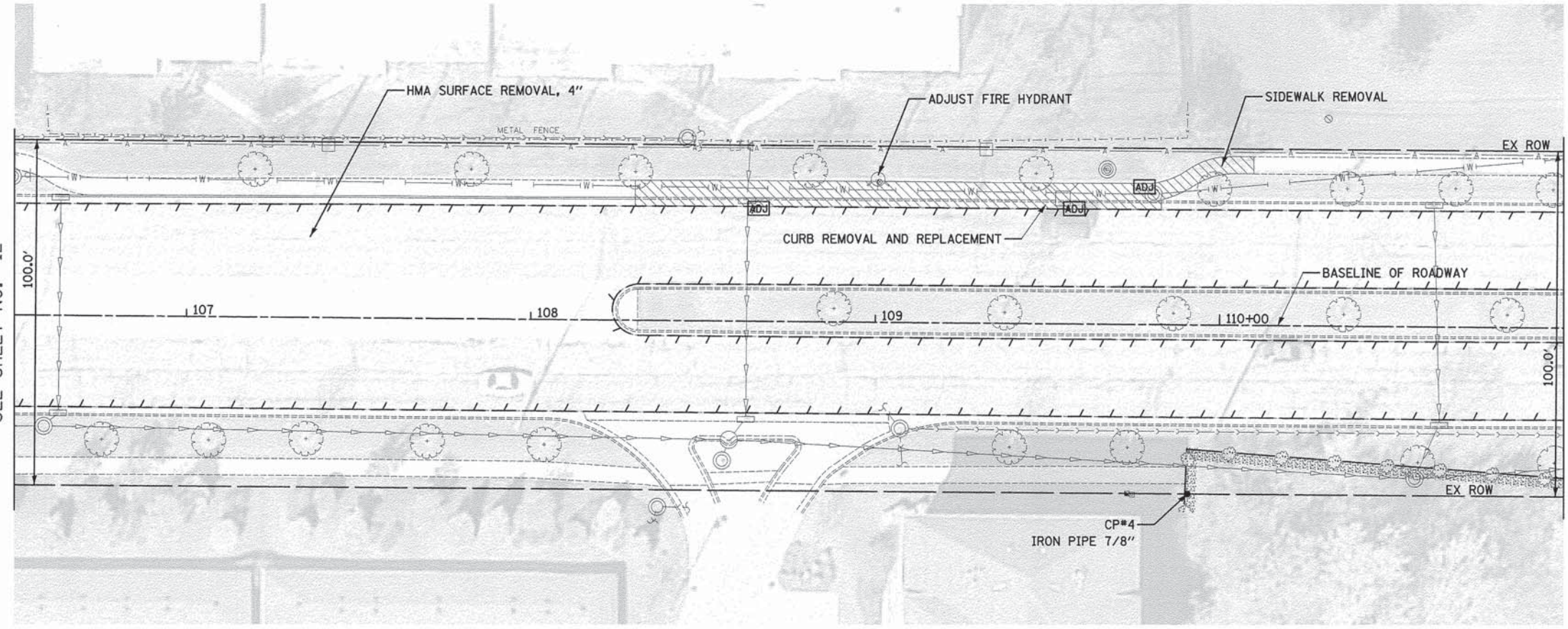
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2586	13-00102-00-RS	DUPAGE	40	12
CONTRACT NO. 61A14				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



PLAN	NO. _____
REVISIONS	
DATE	
BY	
CHECKED	
DESIGNED	
APPROVED	

PROFILE	NO. _____
REVISIONS	
DATE	
BY	
CHECKED	
DESIGNED	
APPROVED	

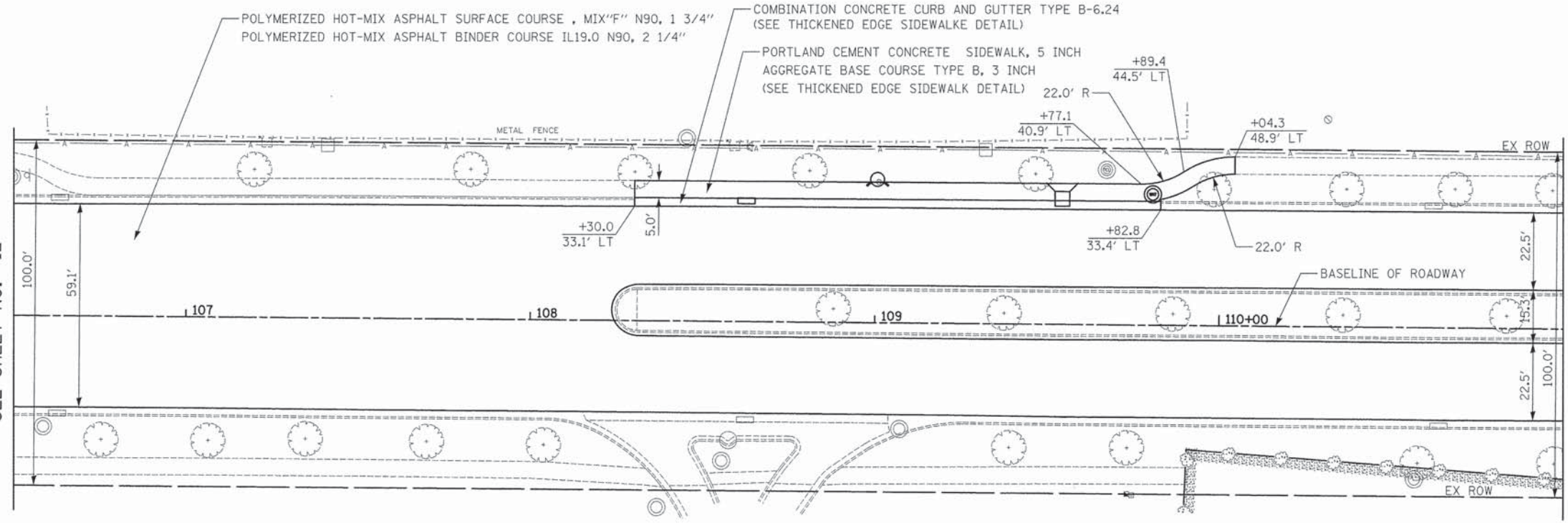
MATCH LINE STA 106+50
SEE SHEET NO. 12



MATCH LINE STA 111+00
SEE SHEET NO. 14

SWIFT ROAD
EXISTING

MATCH LINE STA 106+50
SEE SHEET NO. 12



MATCH LINE STA 111+00
SEE SHEET NO. 14

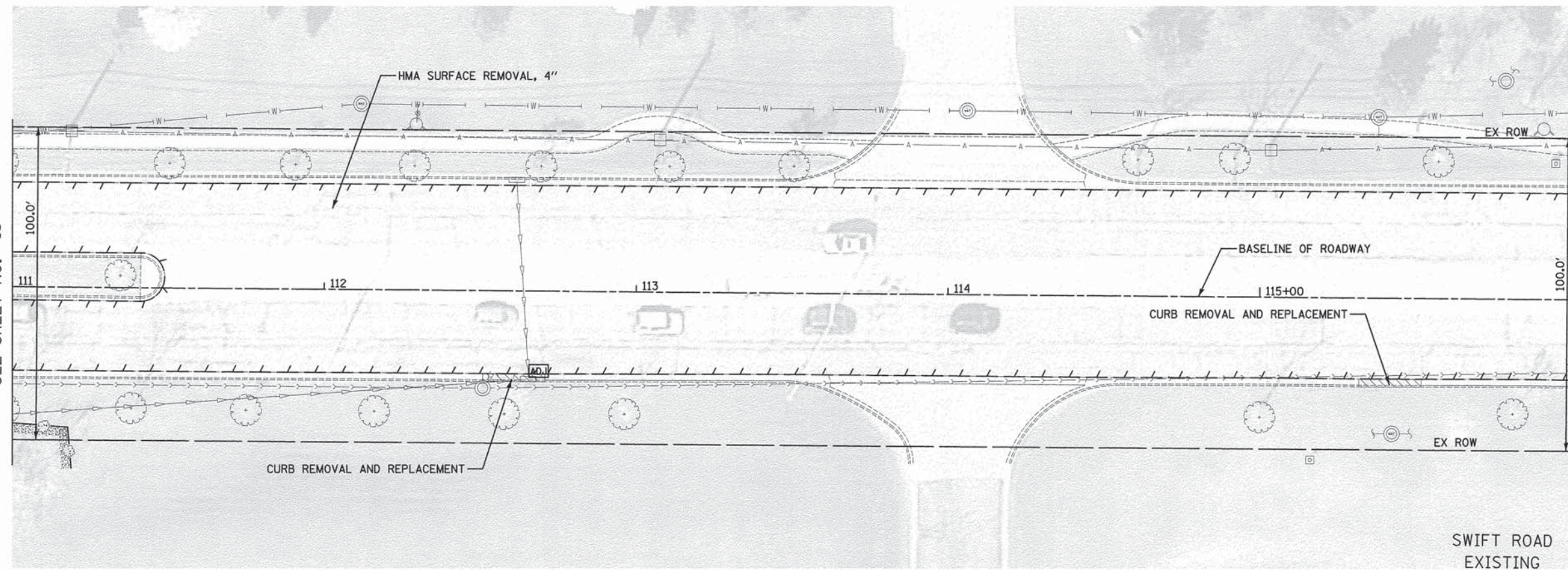
SWIFT ROAD
PROPOSED

① COMB CONC C&G, TY B-6.24
 ② AGGREGATE BASE REPAIR
 ③ PCC SIDEWALK, 5 INCH
 AGG BSE CRSE. TY B, 3 INCH
 TOPSOIL, FURNISH AND PLACE, 4"
 ④ SODDING

THICKENED EDGE
SIDEWALK DETAIL

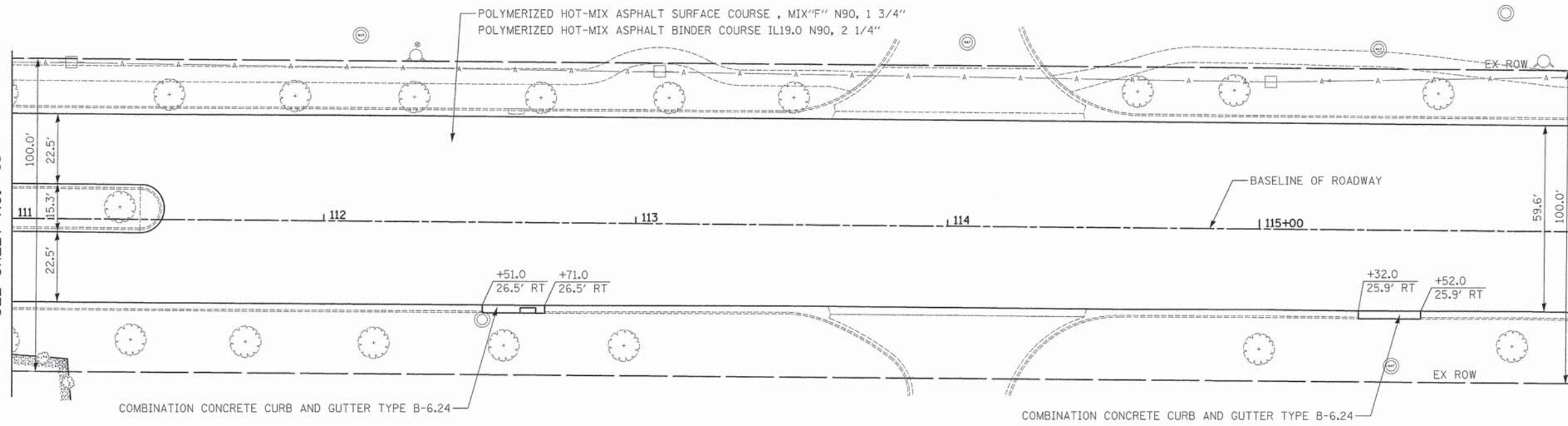


MATCH LINE STA 111+00
SEE SHEET NO. 13



MATCH LINE STA 116+00
SEE SHEET NO. 15

MATCH LINE STA 111+00
SEE SHEET NO. 13



MATCH LINE STA 116+00
SEE SHEET NO. 15

PLAN	DESIGNED	BY	DATE
	PLOTTED		
	ALIGNMENT CHECKED		
	NOTE BOOK NO.		
	DRAWN		
	CHECKED		
	DATE		

PROFILE	DESIGNED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	NOTE BOOK NO.		
	DRAWN		
	CHECKED		
	DATE		

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CHECKED - RTM	REVISED -
DATE - 12/05/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



ROADWAY PLAN

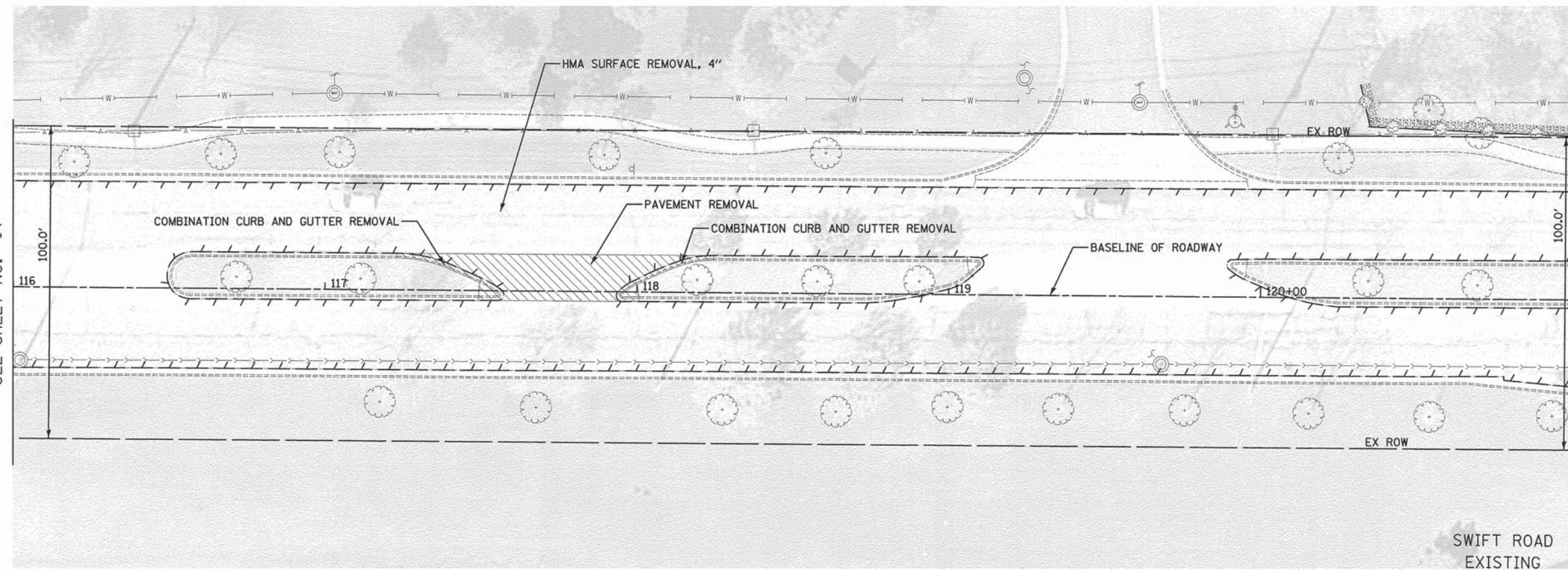
SHEET NO. 10 OF 15 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2586	13-00102-00-RS	DUPAGE	40	14
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

CONTRACT NO. 61A14



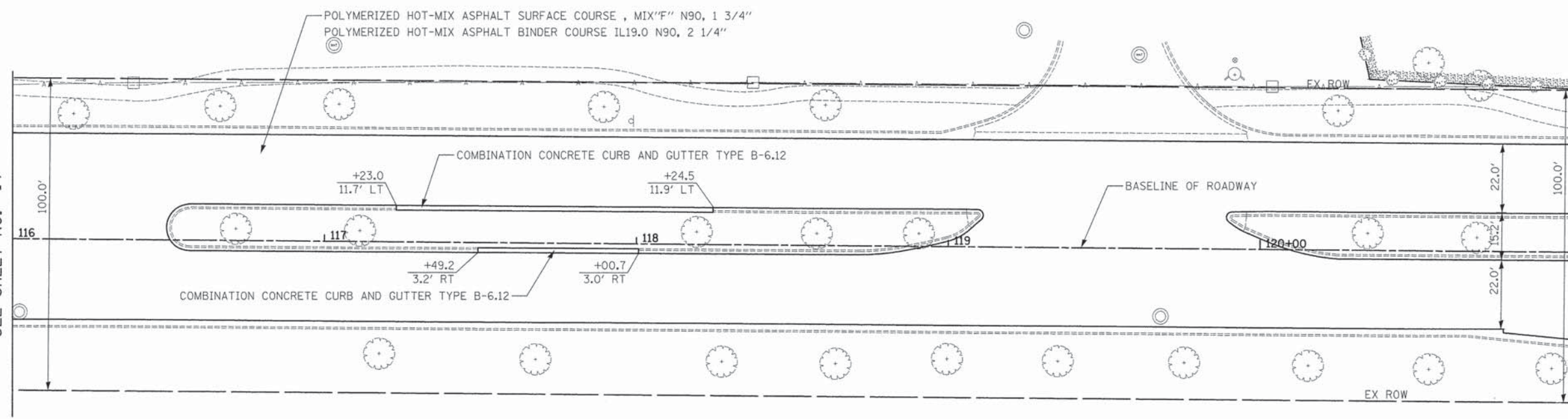
MATCH LINE STA 116+00
SEE SHEET NO. 14



MATCH LINE STA 121+00
SEE SHEET NO. 16

SWIFT ROAD
EXISTING

MATCH LINE STA 116+00
SEE SHEET NO. 14



MATCH LINE STA 121+00
SEE SHEET NO. 16

SWIFT ROAD
PROPOSED

PLAN	REVISION	DATE
NO.	BY	
	DATE	
	BY	
	DATE	
	BY	
	DATE	

PROFILE	REVISION	DATE
NO.	BY	
	DATE	
	BY	
	DATE	
	BY	
	DATE	

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DATE - 12/05/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**



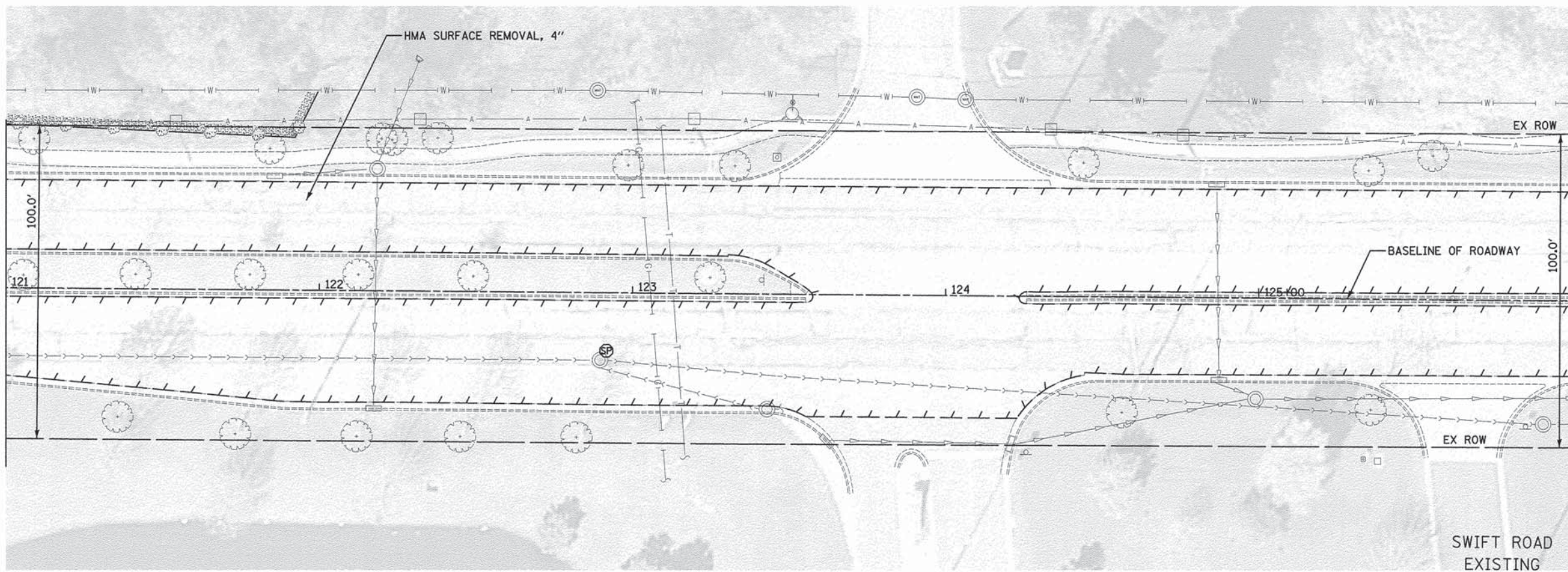
ROADWAY PLAN

SHEET NO. 11 OF 15 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2586	13-00102-00-RS	DUPAGE	40	15
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 61A14	



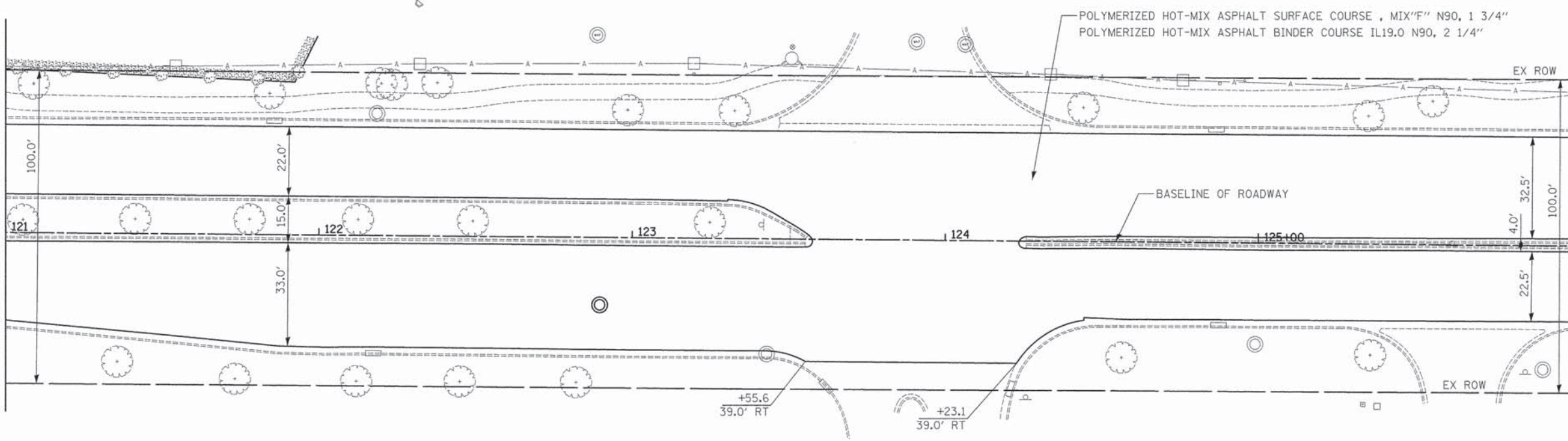
MATCH LINE STA 121+00
SEE SHEET NO. 15



MATCH LINE STA 126+00
SEE SHEET NO. 17

SWIFT ROAD
EXISTING

MATCH LINE STA 121+00
SEE SHEET NO. 15



MATCH LINE STA 126+00
SEE SHEET NO. 17

SWIFT ROAD
PROPOSED

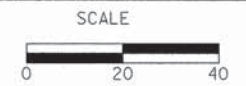
PLAN	REVISIONS	DATE
NO.	BY	
	DATE	
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	DATE	
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PROFILE	REVISIONS	DATE
NO.	BY	
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CHECKED - RTM	REVISED -
DATE - 12/05/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**



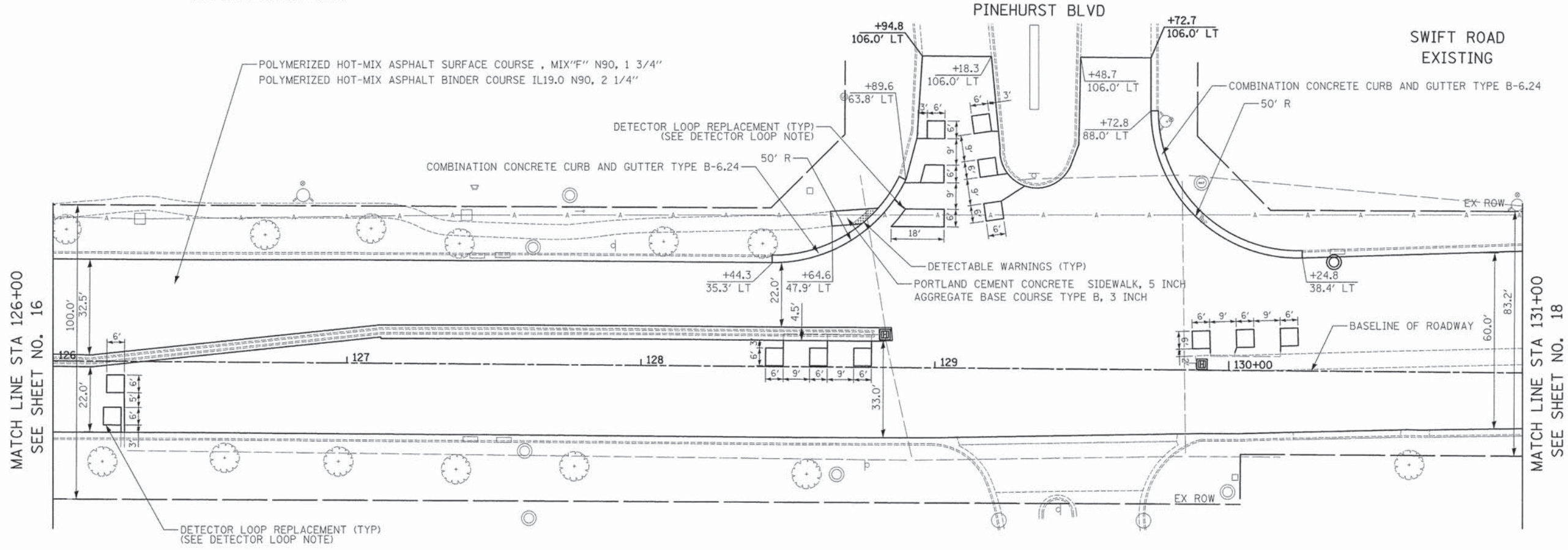
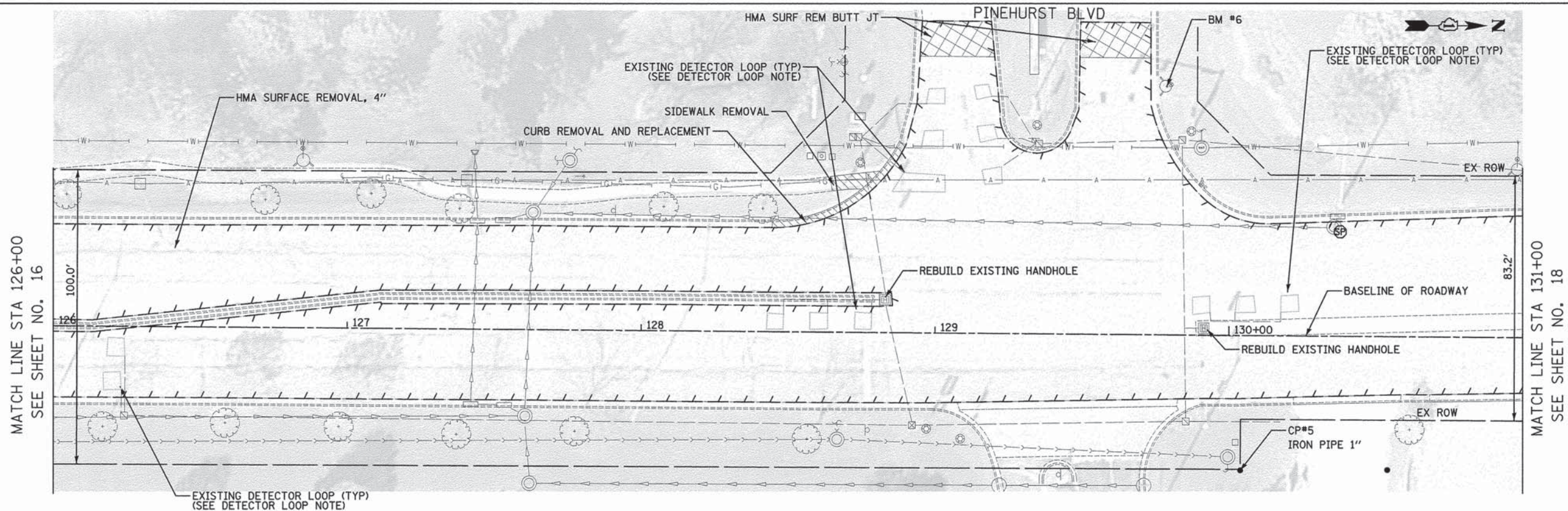
ROADWAY PLAN

SHEET NO. 12 OF 15 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2586	13-00102-00-RS	DUPAGE	40	16
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 61A14	

PLAN	SURVEYED	BY	DATE
	NOTE BOOK		
	ALIGNED		
	CHECKED		
	PT. OF WAY		
	CHECKED		
	NO.		
	DATE		

PROFILE	SURVEYED	BY	DATE
	NOTE BOOK		
	GRADES		
	CHECKED		
	NO.		
	DATE		



DETECTOR LOOP NOTE:
 EXISTING DETECTOR LOOP LOCATIONS ARE APPROXIMATE AND NEED TO BE FIELD VERIFIED.
 EXISTING DETECTOR LOOPS WILL BE REMOVED AND REPLACED AND PAID FOR AS "DETECTOR LOOP REPLACEMENT."

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DRAWN -	JRR	REVISED -	
CHECKED -	RTM	REVISED -	
DATE -	12/05/2013	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



ROADWAY PLAN

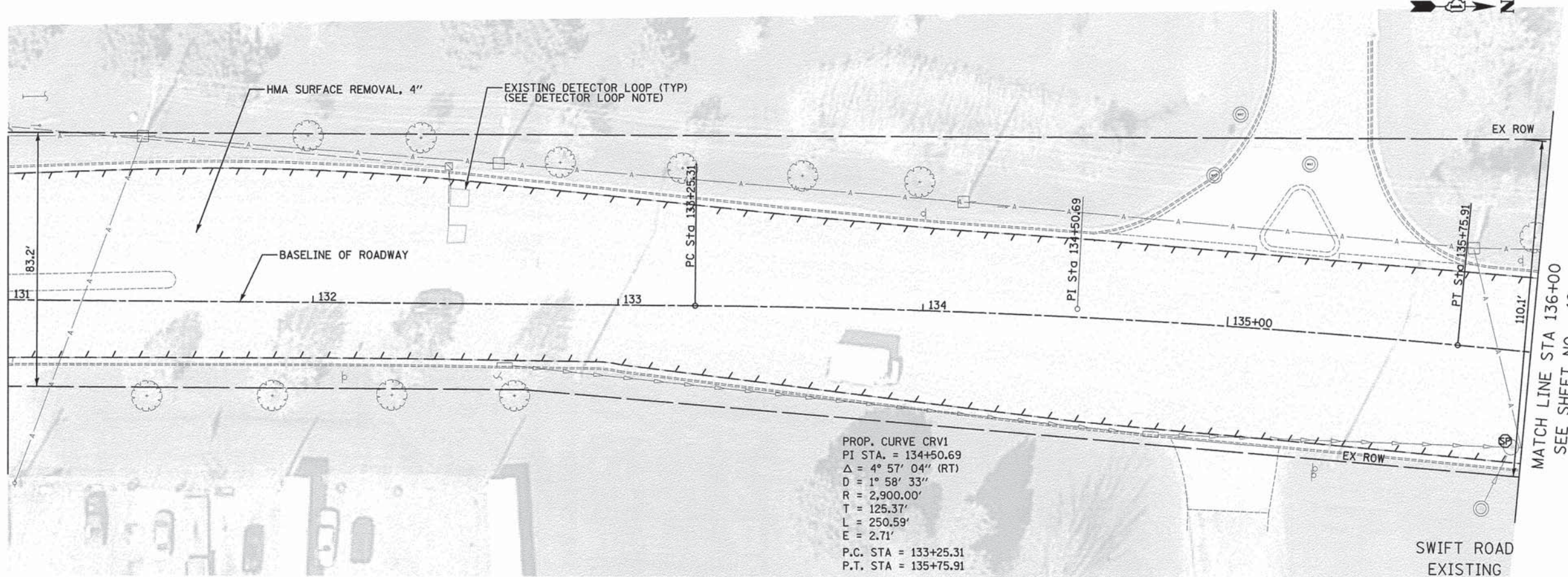
SHEET NO. 13 OF 15 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2586	13-00102-00-RS	DUPAGE	40	17
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 61A14	

PLAN	DATE
BY	
SURVEYED	
ALIGNMENT CHECKED	
PT. OF WAY CHECKED	
NOTE BOOK NO.	
DATE FILED	

PROFILE	DATE
BY	
SURVEYED	
GRADES CHECKED	
BLM. NOTED	
STRUCTURE NOTATIONS CHECKED	
NOTE BOOK NO.	
DATE FILED	

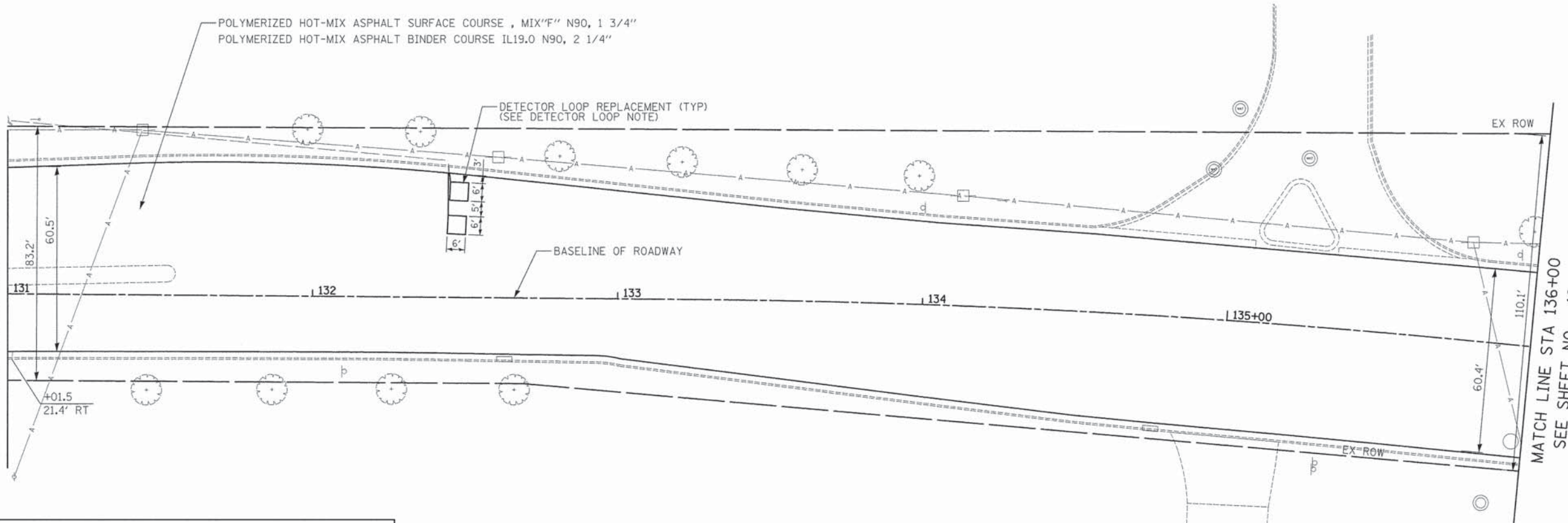
MATCH LINE STA 131+00
SEE SHEET NO. 17



PROP. CURVE CRV1
 PI STA. = 134+50.69
 $\Delta = 4^\circ 57' 04''$ (RT)
 $D = 1^\circ 58' 33''$
 $R = 2,900.00'$
 $T = 125.37'$
 $L = 250.59'$
 $E = 2.71'$
 P.C. STA = 133+25.31
 P.T. STA = 135+75.91

SWIFT ROAD
EXISTING

MATCH LINE STA 131+00
SEE SHEET NO. 17



SWIFT ROAD
PROPOSED

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DATE - 12/05/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



ROADWAY PLAN

SHEET NO. 14 OF 15 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2586	13-00102-00-RS	DUPAGE	40	18
CONTRACT NO. 61A14				

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



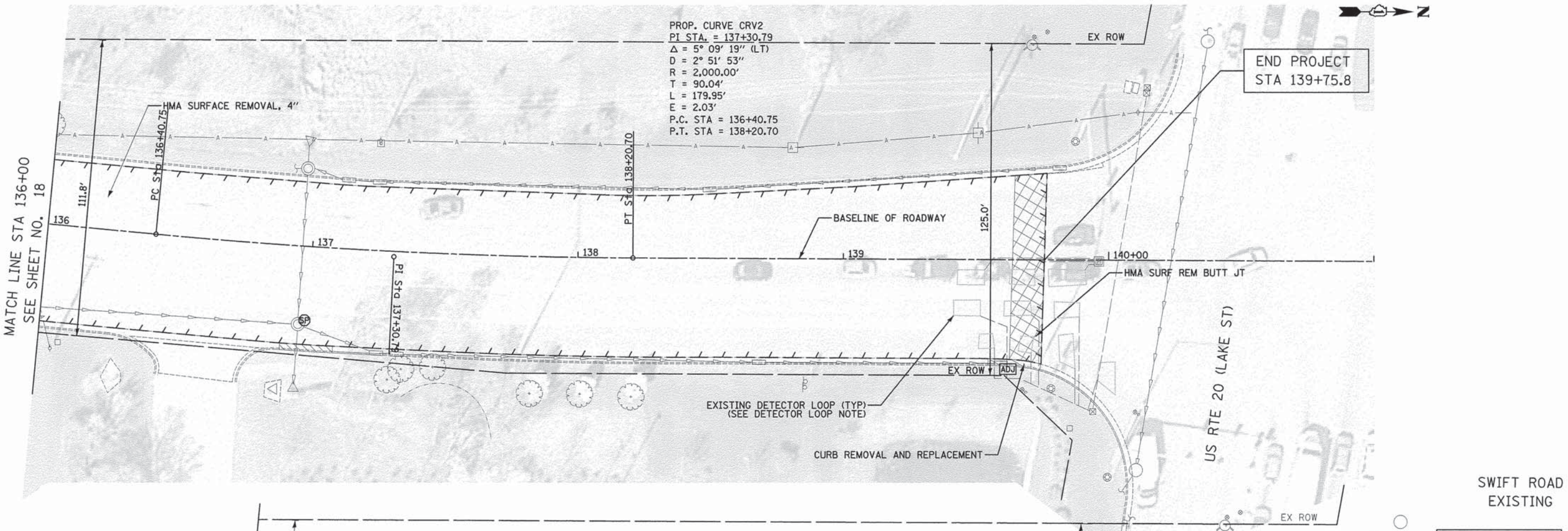
PROP. CURVE CRV2
 PI STA. = 137+30.79
 $\Delta = 5^\circ 09' 19''$ (LT)
 $D = 2^\circ 51' 53''$
 $R = 2,000.00'$
 $T = 90.04'$
 $L = 179.95'$
 $E = 2.03'$
 P.C. STA = 136+40.75
 P.T. STA = 138+20.70

END PROJECT
 STA 139+75.8

MATCH LINE STA 136+00
 SEE SHEET NO. 18

DATE	
BY	
CHECKED	
DESIGNED	
PLANNED	
NOTED	
NO.	

DATE	
BY	
CHECKED	
DESIGNED	
PLANNED	
NOTED	
NO.	

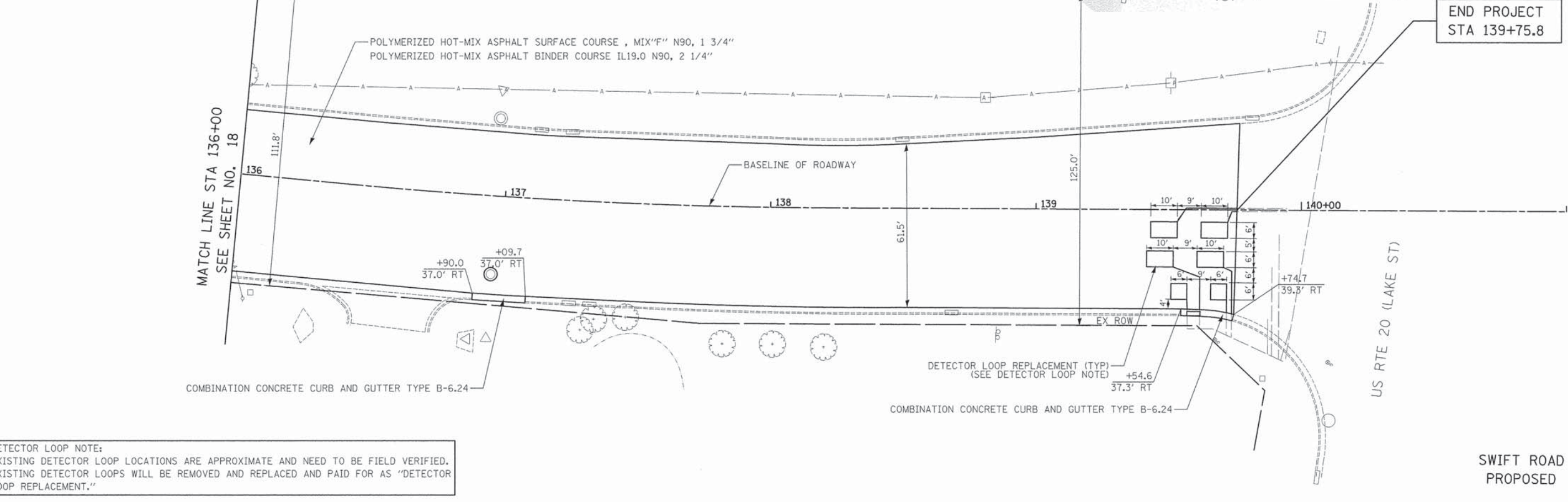


SWIFT ROAD
 EXISTING

END PROJECT
 STA 139+75.8

MATCH LINE STA 136+00
 SEE SHEET NO. 18

POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F" N90, 1 3/4"
 POLYMERIZED HOT-MIX ASPHALT BINDER COURSE IL19.0 N90, 2 1/4"



SWIFT ROAD
 PROPOSED

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DATE - 12/05/2013	REVISED -

STATE OF ILLINOIS
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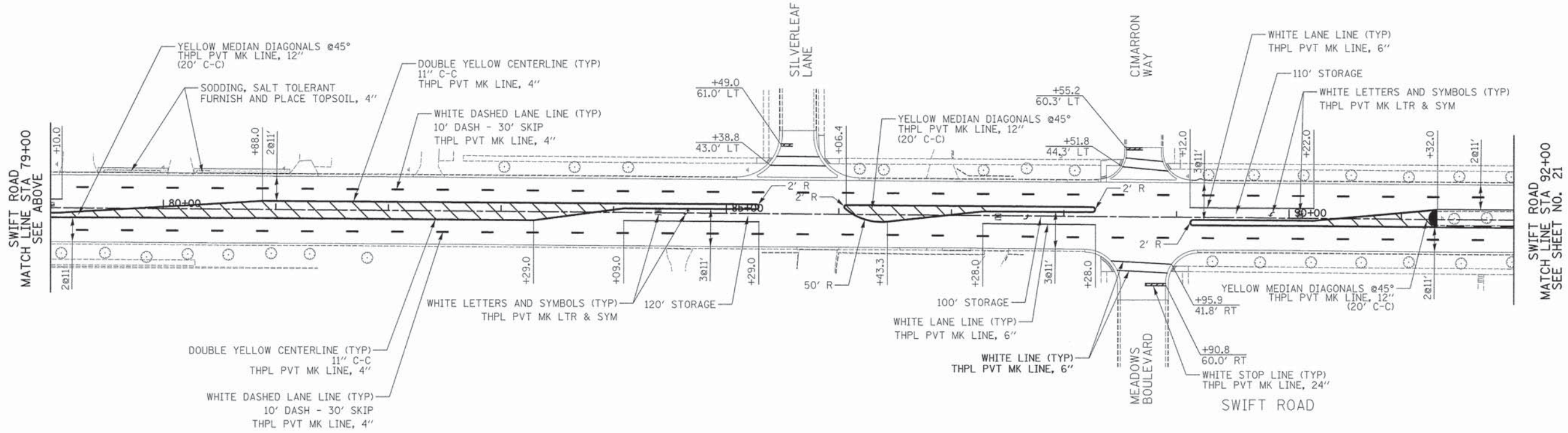
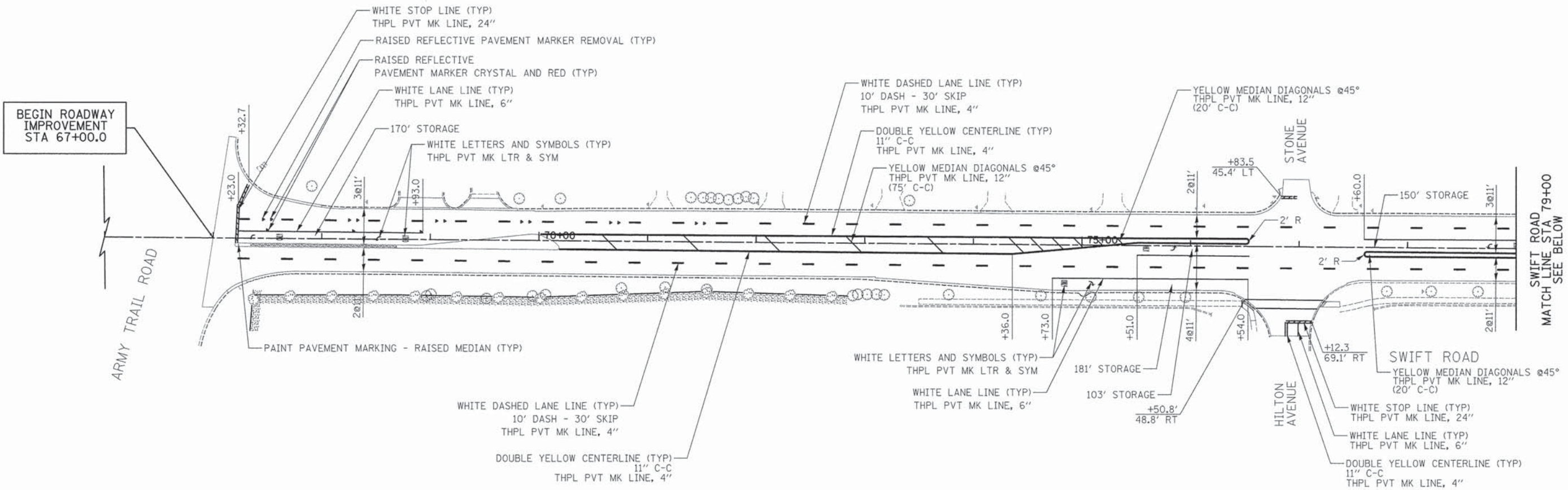
ROADWAY PLAN

SHEET NO. 15 OF 15 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2586	13-00102-00-RS	DUPAGE	40	19
				CONTRACT NO. 61A14
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

DATE	
BY	
REVISION	
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PLAN	
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DATE	
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REVISION	
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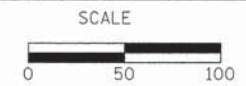
DATE	
BY	
REVISION	
NO.	
PROFILE	
NO.	
DATE	
BY	
REVISION	
NO.	



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DRAWN -	JRR	REVISED -	00143
CHECKED -	RTM	REVISED -	
DATE -	2/03/2014	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



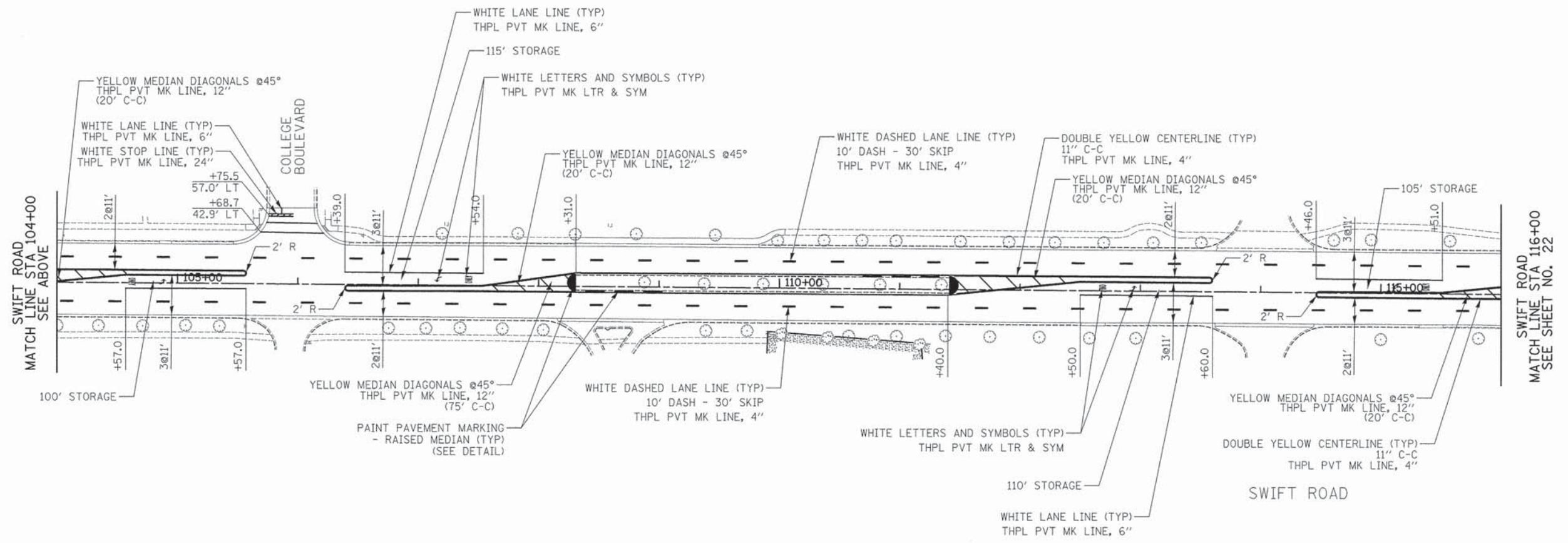
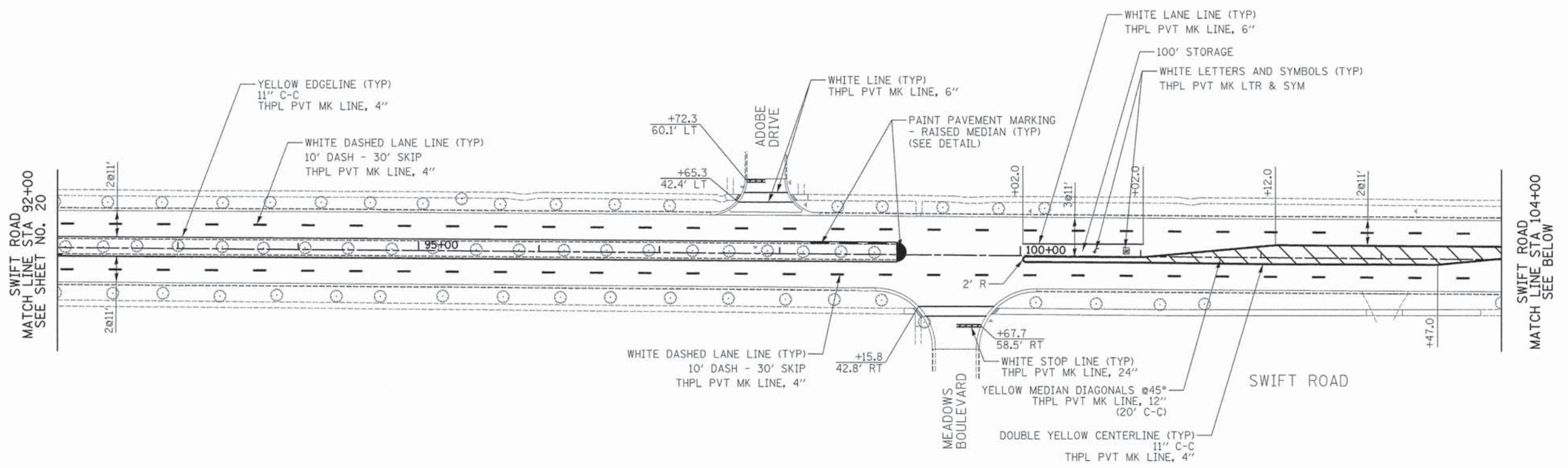
Pavement Marking

SHEET NO. 1 OF 3 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2586	13-00102-00-RS	DUPAGE	40	20
CONTRACT NO. 61A14				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PLAN	REVISIONS	DATE
NO.	BY	
NO.	BY	
NO.	BY	
NO.	BY	
NO.	BY	
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NO.	BY	

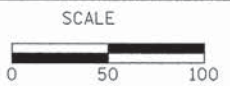
PROFILE	REVISIONS	DATE
NO.	BY	
NO.	BY	
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NO.	BY	
NO.	BY	
NO.	BY	
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NO.	BY	
NO.	BY	



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DRAWN - JRR	REVISED -
CHECKED - RTM	REVISED -
DATE - 12/05/2013	REVISED -

**STATE OF ILLINOIS
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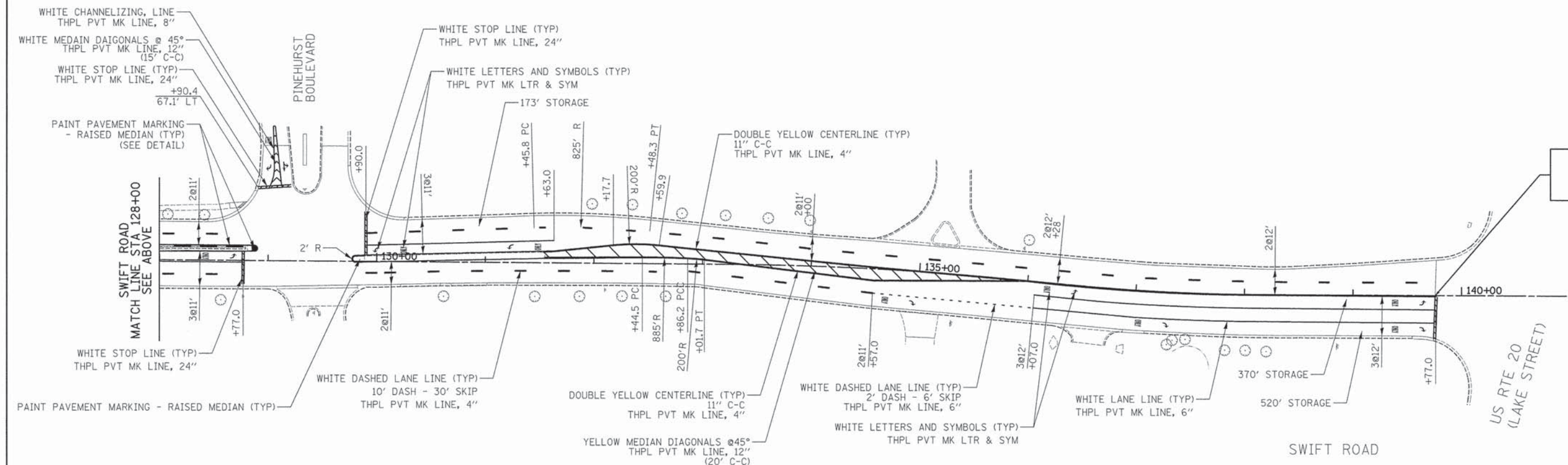
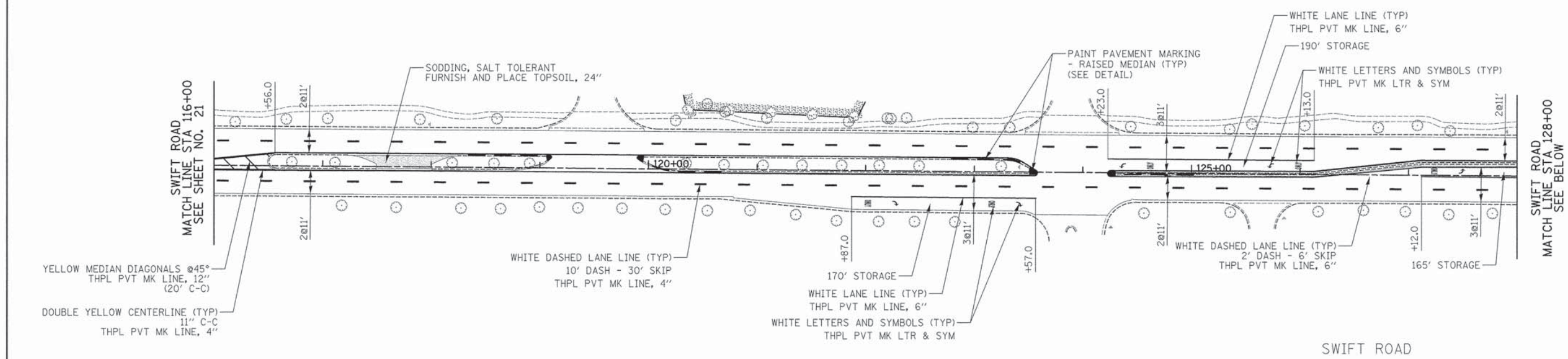
Pavement Marking

SHEET NO. 2 OF 3 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2586	13-00102-00-RS	DUPAGE	40	21
CONTRACT NO. 61A14				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

DATE	
BY	
REVISION	
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DATE	
BY	
REVISION	
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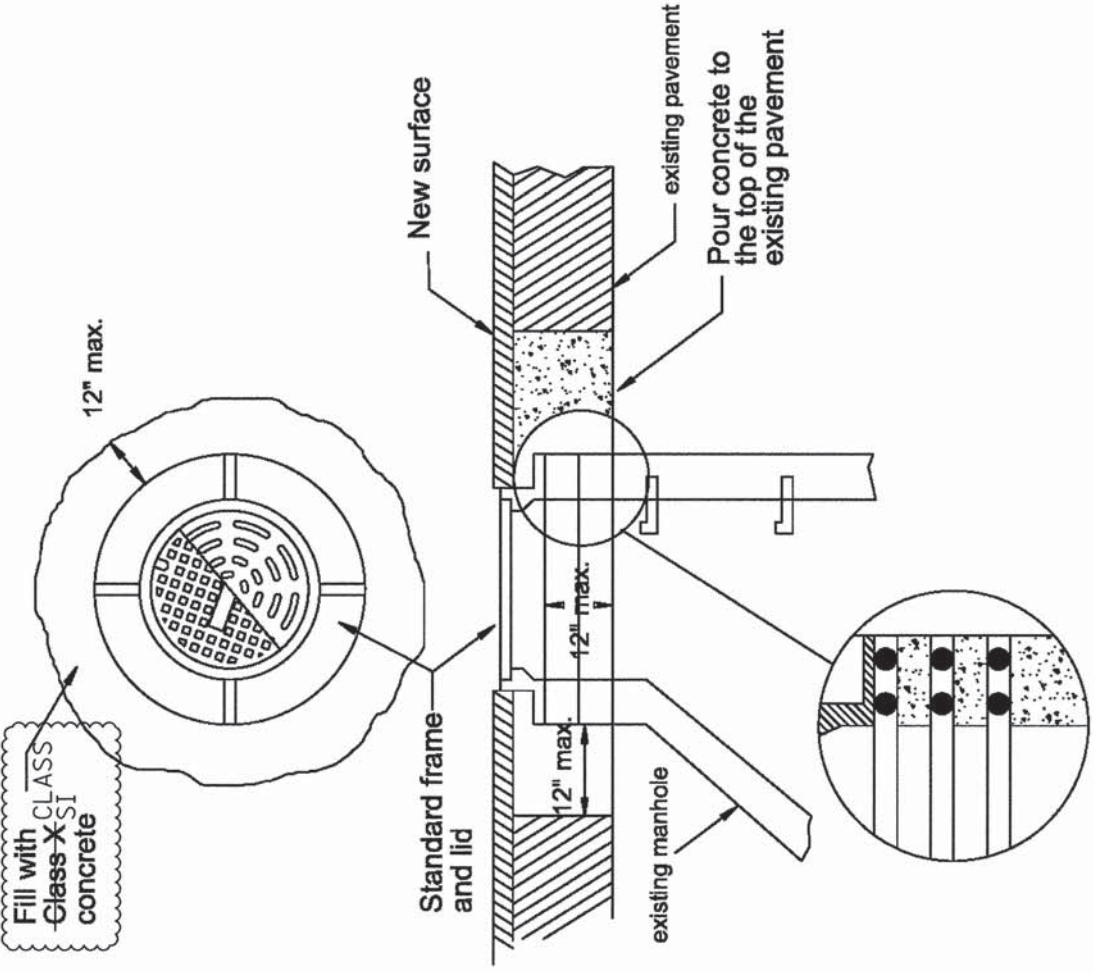


STD. 208.10

NOT TO SCALE

Fill with **CLASS X-SI** concrete

- NOTES:
- Frames located in paved areas shall have the pavement removed for a distance not to exceed 12" outside of the base of the frame. The frame shall then be adjusted to finished surface grade, and the area around the frame filled with **CLASS X** concrete to the surface of the adjacent pavement.
 - Frames located in paved areas and in the curb and gutter may require the use of tapered adjusting rings or metal shims with non-shrink grout.
 - Adjustments shall be made with a maximum of 2 precast concrete adjusting rings. No more than 1 ring shall be 2" in size. If the total height of the adjustment exceeds 12", the adjustment shall be made by adding or removing complete barrel sections. The cone, concrete adjusting rings, and frame shall be set in a full bed of bituminous mastic, or 2 continuous rows of pre-formed, bituminous mastic material (~~approved equal~~).
 - All common brick adjusting courses shall be removed and replaced with precast concrete adjusting rings.



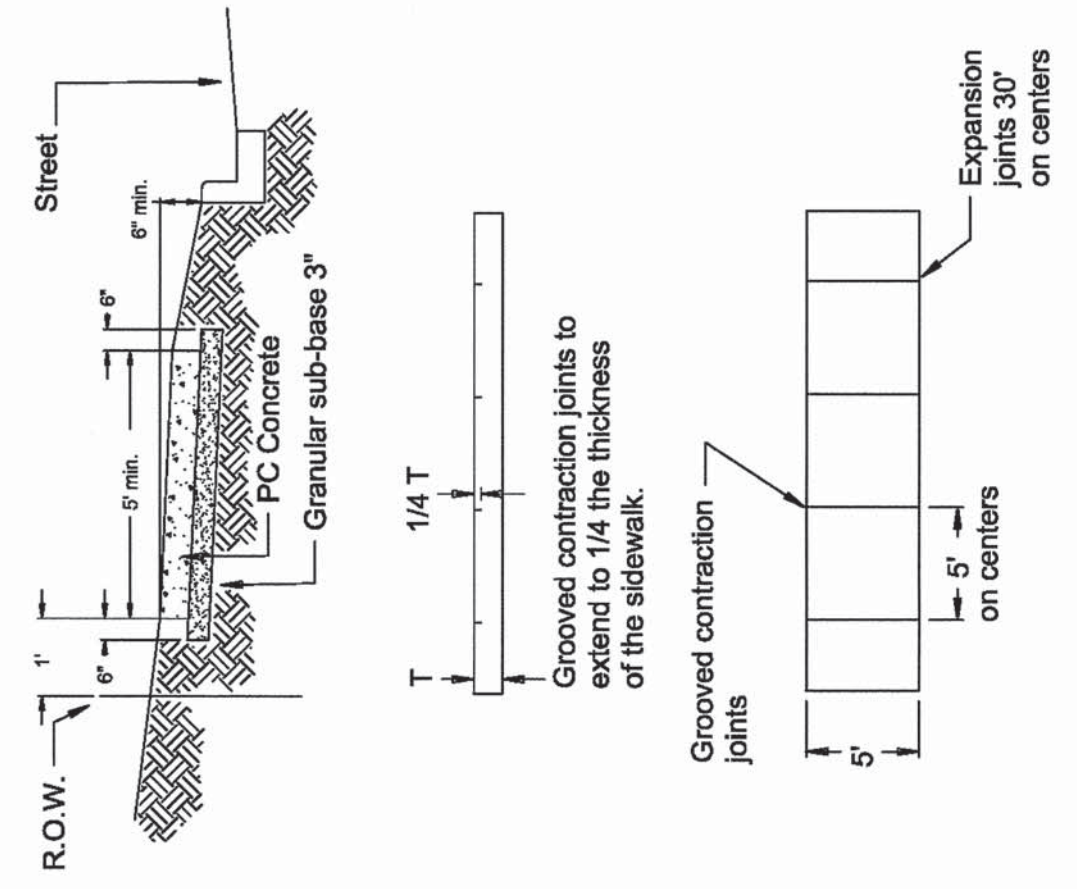
FRAME ADJUSTMENT

REVISIONS	VILLAGE OF ADDISON
5/98	APPROVED: 2/2/94
8/03	<i>[Signature]</i>
	R. ESPEDIDO, P.E., VILLAGE ENGINEER

STD. 501A

NOT TO SCALE

- NOTES:
- Concrete shall be **CLASS X-SI** bag mix, with a compressive strength of not less than 3,500 PSI.
 - Sidewalks shall have a minimum thickness of 5".
 - Sidewalks across driveways shall have a minimum thickness of 6".
 - Sidewalks shall be placed on 3" of compacted sub-base.
 - 3/4" expansion joints at 30' on centers.
 - Grooved contraction joints at 5' on centers.
 - Slope sidewalk 1/4" per foot in the direction of drainage.
 - Public sidewalks shall be a minimum of 5' wide.
 - Private residential service walks shall be a minimum of 3' wide.

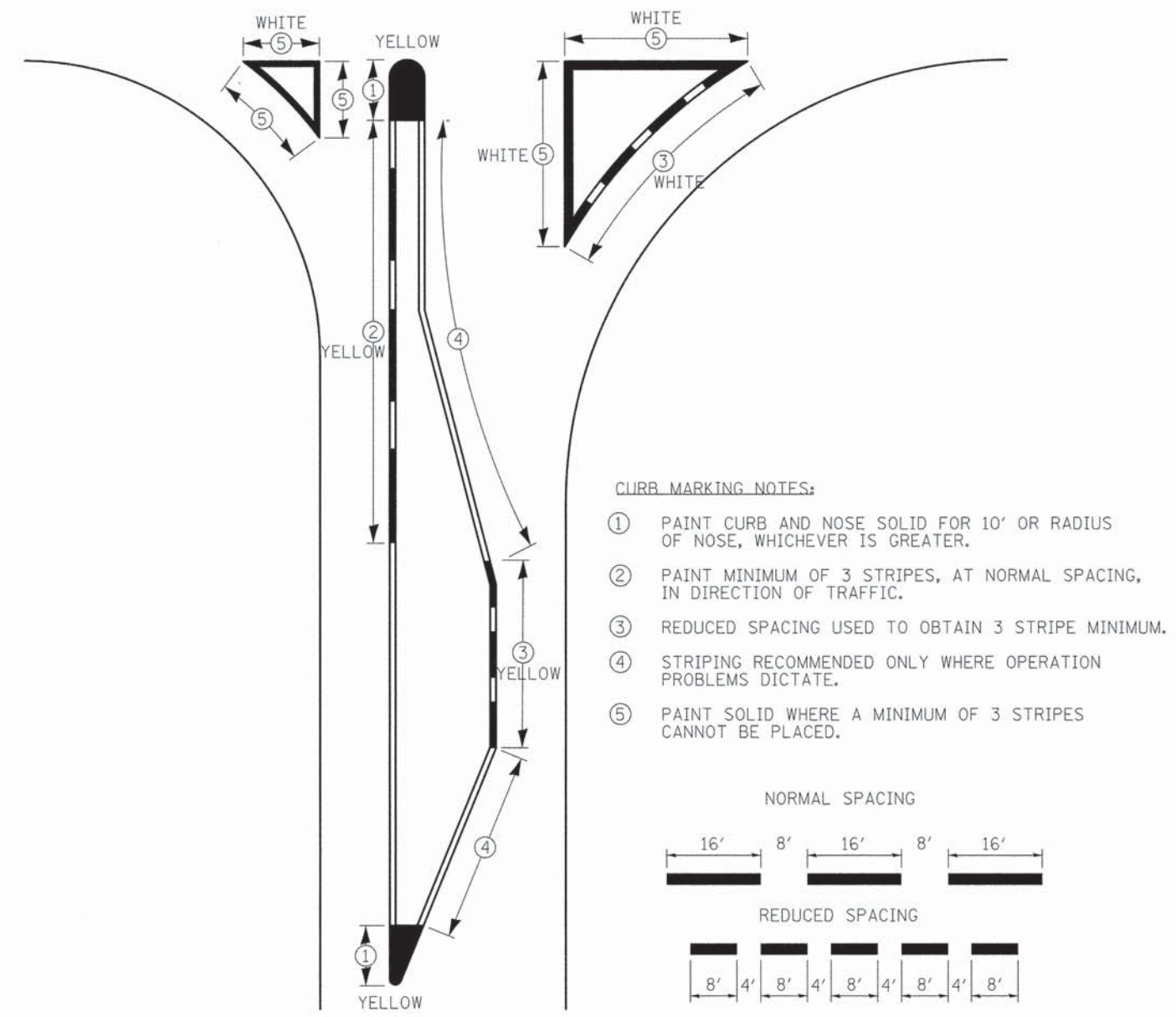
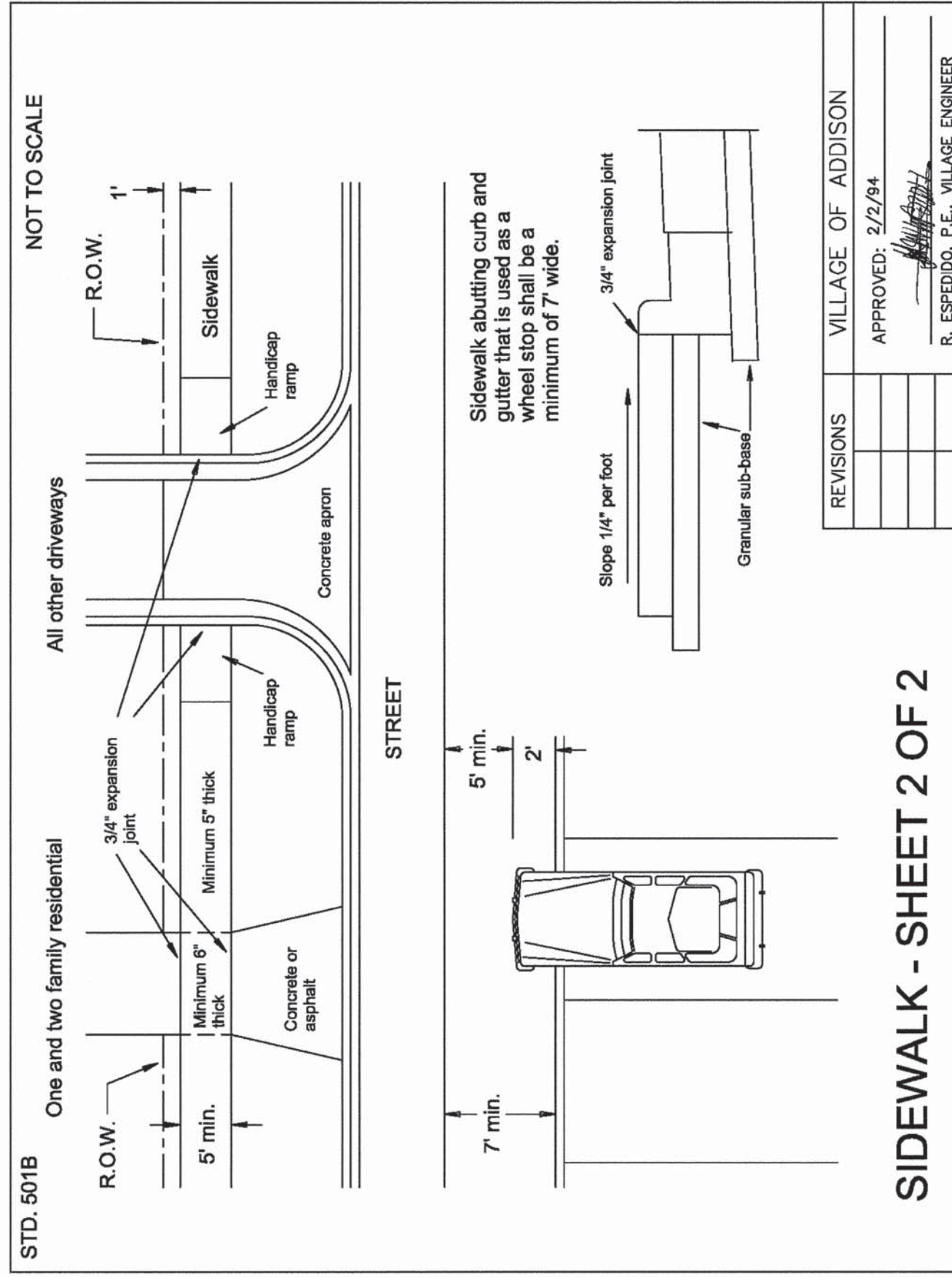


SIDEWALK - SHEET 1 OF 2

REVISIONS	VILLAGE OF ADDISON
8/03	APPROVED: 2/2/94
	<i>[Signature]</i>
	R. ESPEDIDO, P.E., VILLAGE ENGINEER

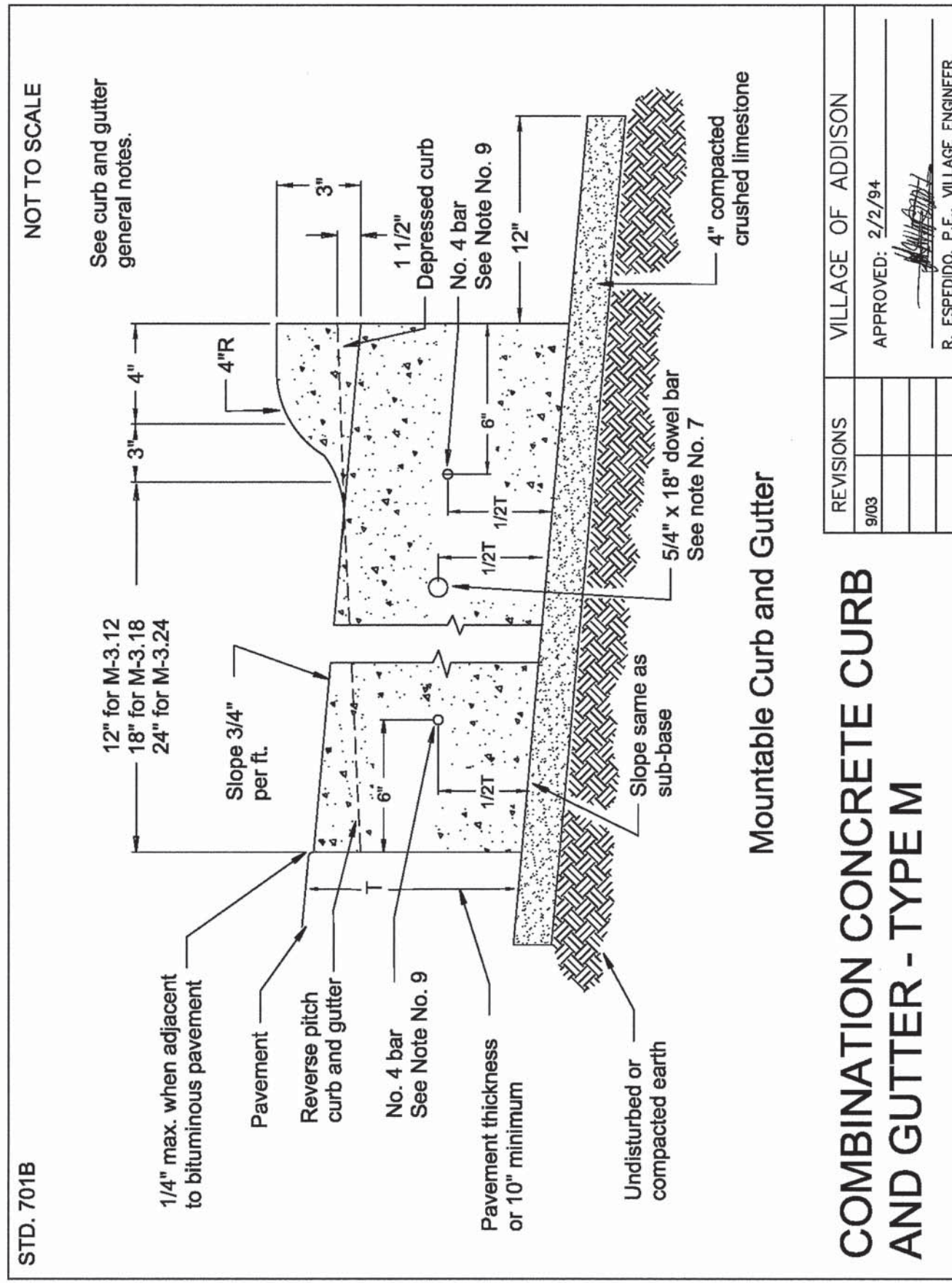
PLAN	SURVEYED	BY	DATE
NOTE BOOK	ALIGNED CHECKED		
NO.	PT. OF WAY CHECKED		
	FIELD FILE NAME		

PROFILE	SURVEYED	BY	DATE
NOTE BOOK	GRADES CHECKED		
NO.	BLK. NOTED		
	STRUCTURE NOTATIONS CHECKED		



PLAN	REVISIONS	BY	DATE
NO.			
	DESIGNED		
	PLOTTED		
	GRADES CHECKED		
	BY		
	NOTED		
	STRUCTURE DETAILERS CHECKED		

PROFILE	REVISIONS	BY	DATE
NO.			
	DESIGNED		
	PLOTTED		
	GRADES CHECKED		
	BY		
	NOTED		
	STRUCTURE DETAILERS CHECKED		



STD. 701D

CURB AND GUTTER GENERAL NOTES

1. Thickness: "T" = thickness of the curb or gutter
2. Depressed curbs: The top of a curb shall be depressed where it is constructed across a sidewalk, driveway, or where directed by the Director of Community Development. The transition from full height curb to depressed curb shall be at the rate of 3" per foot.
3. Sub-grade: Improved sub-grade shall extend 1' outside of the back of curb.
4. Sub-base: The sub-base shall consist of 4" of compacted crushed limestone.
5. Concrete: Shall be a 6 bag mix, attaining a strength of not less than 3500 P.S.I. in 28 days.
6. Contraction joints: Maximum of 25' intervals, grooved or sawcut a minimum of 1/8" wide, and a minimum of 2" deep. Sawing shall be completed within 24 hours of the pour.
7. Expansion joints: 1" thick preformed joint filler with a single 5/4"x 18" epoxy coated smooth dowel bar, fitted with a pinch cap, installed 5' on either side of any structure within the curb line, at the PT and PC of a radius, and at the end of each concrete pour. Maximum spacing of 75'.
8. Construction joints: Use whenever the placing of concrete is held up for 30 minutes or more. Construction joints shall be installed in the same manner as an expansion joint.
9. Steel reinforcement: 2 continuous No. 4 bars centered in the curb over all trench crossings so that the bars extend 5' beyond the trench on all sides.

REVISIONS	VILLAGE OF ADDISON
	APPROVED: 2/2/94
	R. ESPEDIDO, P.E., VILLAGE ENGINEER

DESIGNED - RTM	REVISED -
DRAWN - JRR	REVISED -
CHECKED - RTM	REVISED -
DATE - 12/05/2013	REVISED -

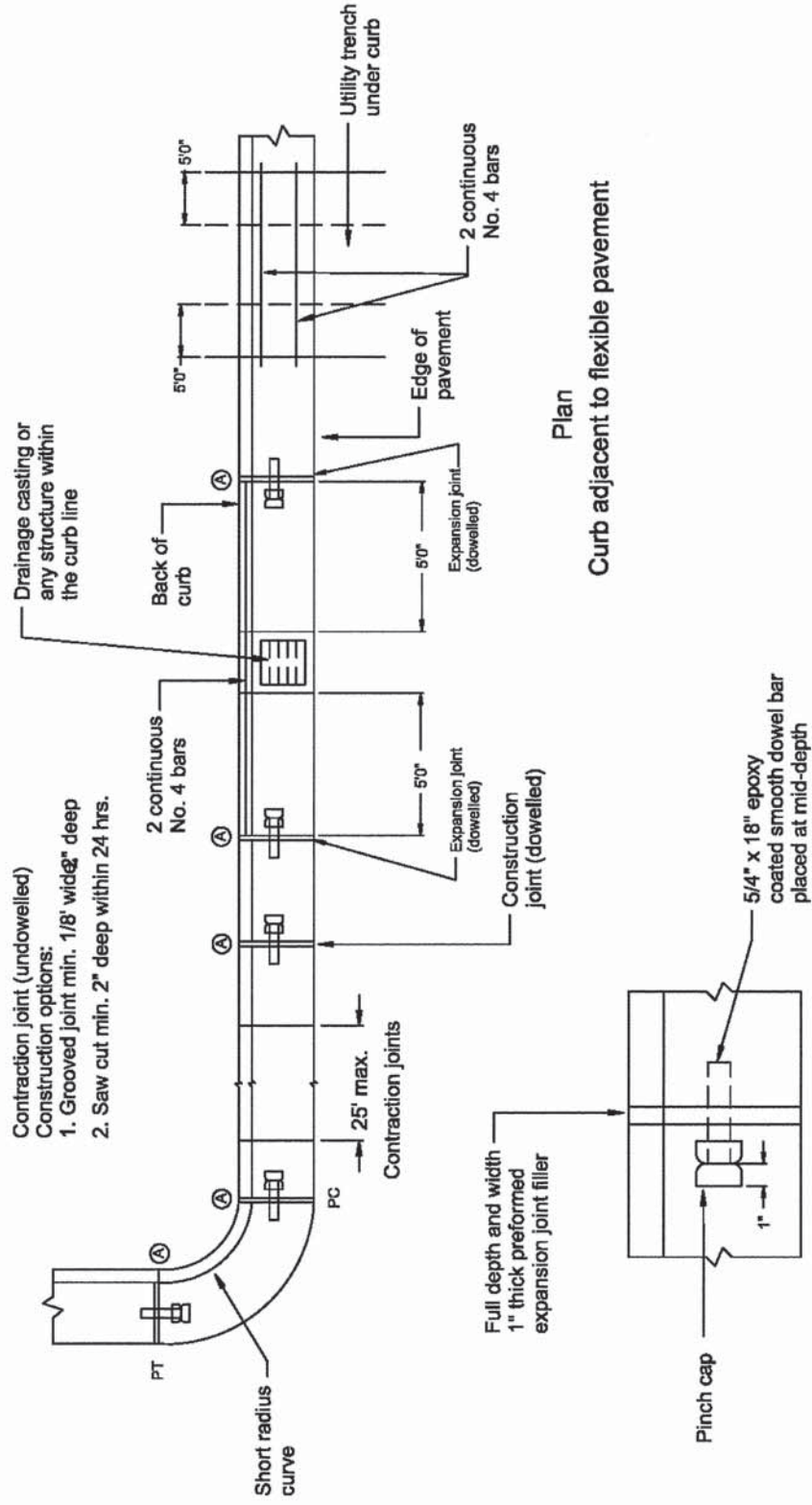
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2586	13-00102-00-RS	DUPAGE	40	25
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 61A14	

PLAN	DESIGNED	BY	DATE
NO.	PLOTTED		
	ALIGNMENT CHECKED		
	NOTE BOOK		
	NO.		
	DATE		
	NO.		
	DATE		

PROFILE	DESIGNED	BY	DATE
NO.	PLOTTED		
	GRADES CHECKED		
	NOTE BOOK		
	NO.		
	DATE		
	NO.		
	DATE		

STD. 704

NOT TO SCALE



Detail A
Expansion joint

CURB AND GUTTER JOINTS

REVISIONS

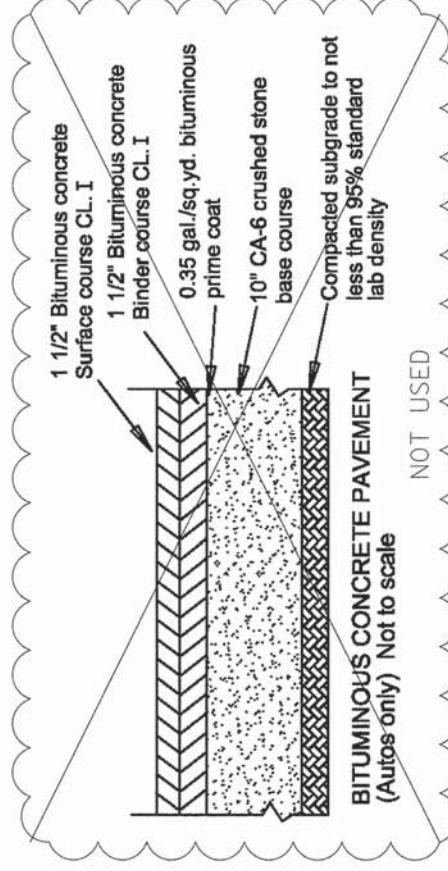
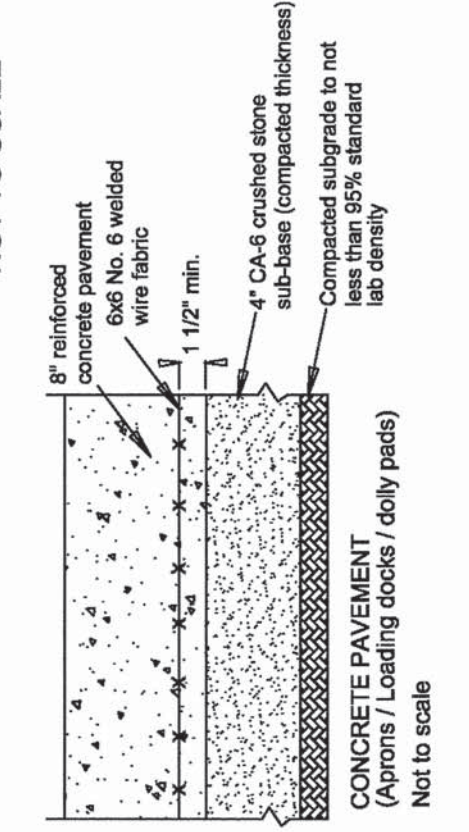
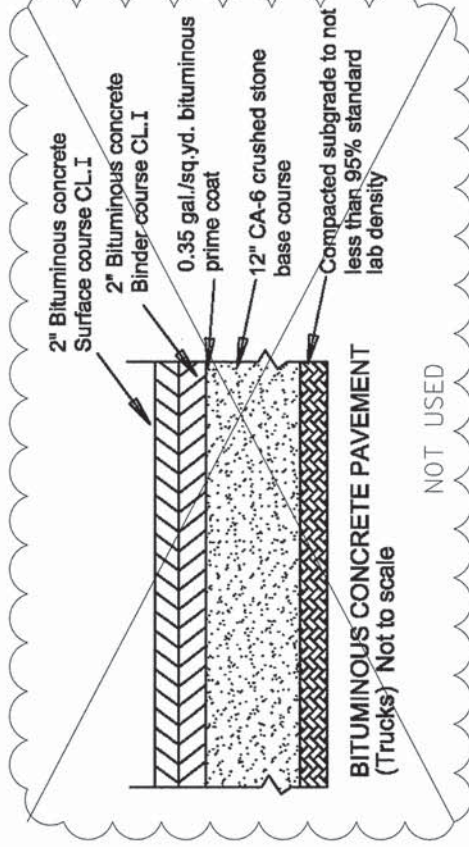
VILLAGE OF ADDISON

APPROVED: 2/2/94

R. ESPEDIDO, P.E., VILLAGE ENGINEER

STD. 1006

NOT TO SCALE



PARKING LOT & DRIVEWAY PAVEMENT

(Industrial / Commercial)

REVISIONS

VILLAGE OF ADDISON

APPROVED: 2/2/94

R. ESPEDIDO, P.E., VILLAGE ENGINEER



450 E Devon Ave, Suite 300
Itasca, Illinois 60143
Tel: 630.773.3900 Fax: 630.773.3975
www.civiltechinc.com

DESIGNED - RTM	REVISED -
DRAWN - JRR	REVISED -
CHECKED - RTM	REVISED -
DATE - 12/05/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

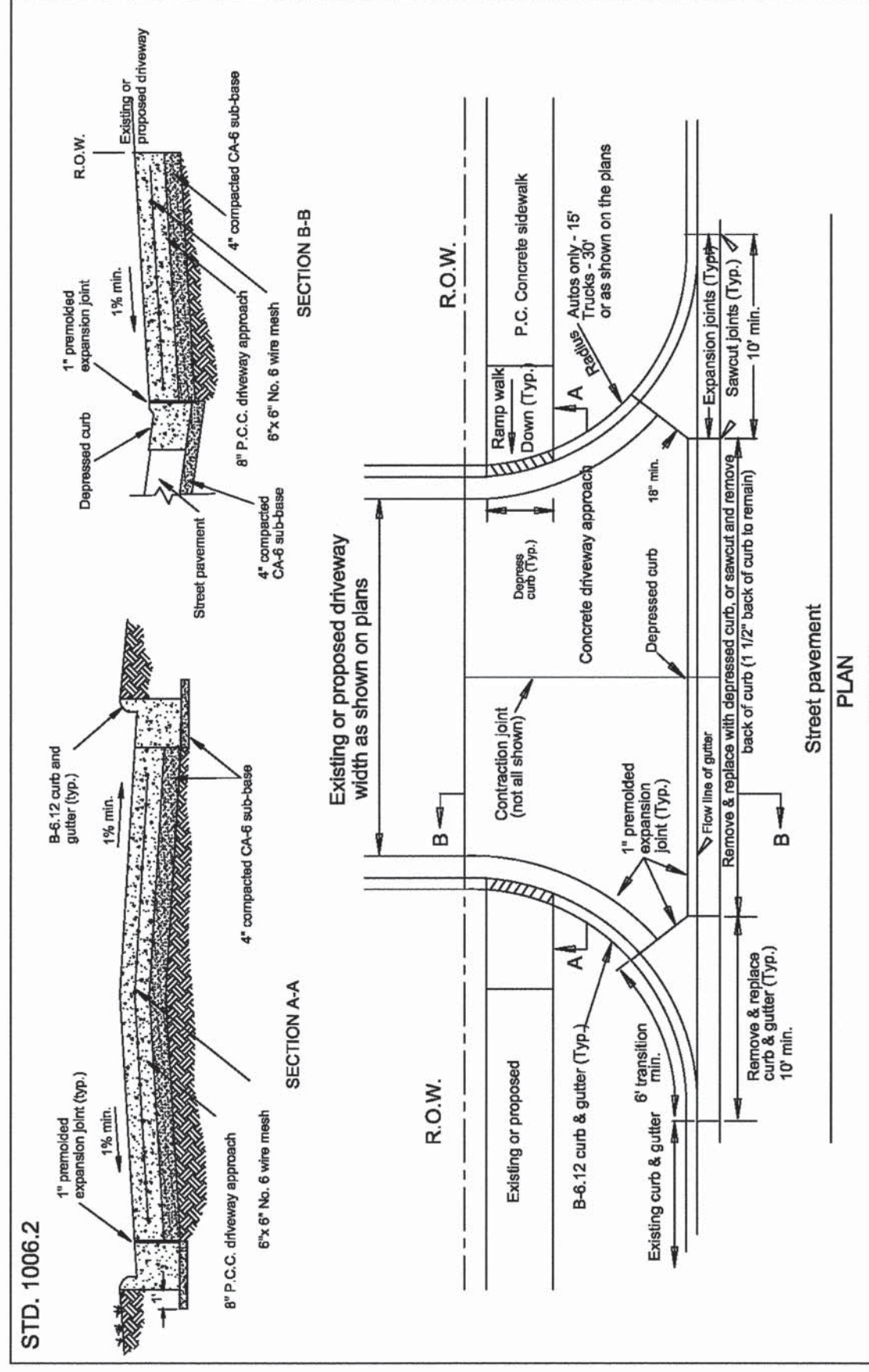
CONSTRUCTION DETAILS - SWIFT ROAD

SHEET NO. 4 OF 5 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 61A14				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PLAN	DESIGNED	BY	DATE
NO.	NOTE BOOK		
	ALIGNMENT CHECKED		
	PT. OF WAY CHECKED		
	MAP FILE NAME		

PROFILE	DESIGNED	BY	DATE
NO.	NOTE BOOK		
	GRADES CHECKED		
	B.M. NOTED		
	STRUCTURE DISTANCES CHECKED		



P.C. CONCRETE DRIVEWAY APPROACH
(INDUSTRIAL/COMMERCIAL)

PLAN
Not to scale

REVISIONS	VILLAGE OF ADDISON
8/03	APPROVED: 2/2/94
	R. ESPEDIDO, P.E., VILLAGE ENGINEER

CIVILTECH
450 E Devon Ave, Suite 300
Itasca, Illinois 60143
Tel: 630.773.3900 Fax: 630.773.3975
www.civiltechinc.com

DESIGNED - RTM	REVISED -
DRAWN - JRR	REVISED -
CHECKED - RTM	REVISED -
DATE - 12/05/2013	REVISED -

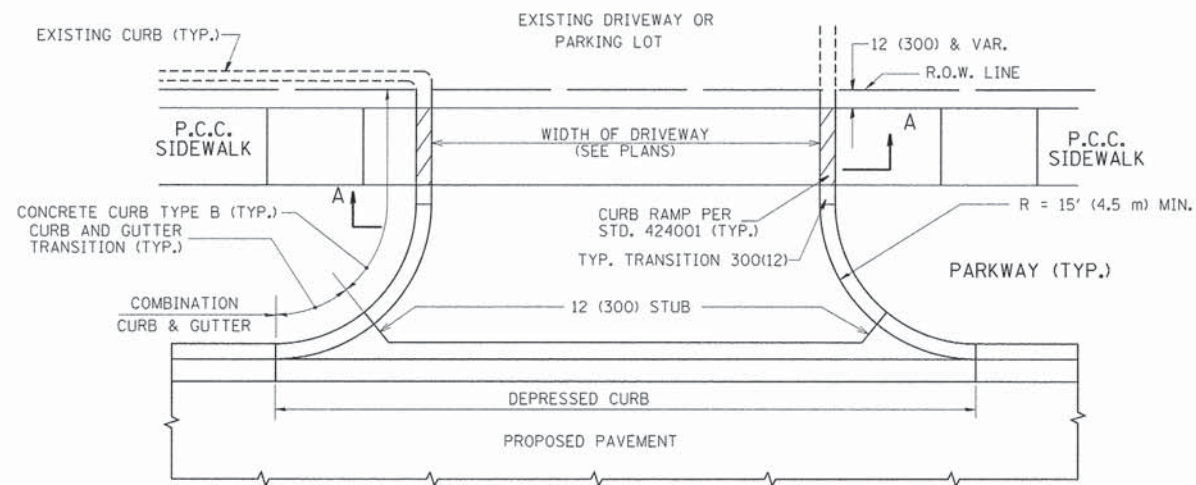
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS - SWIFT ROAD

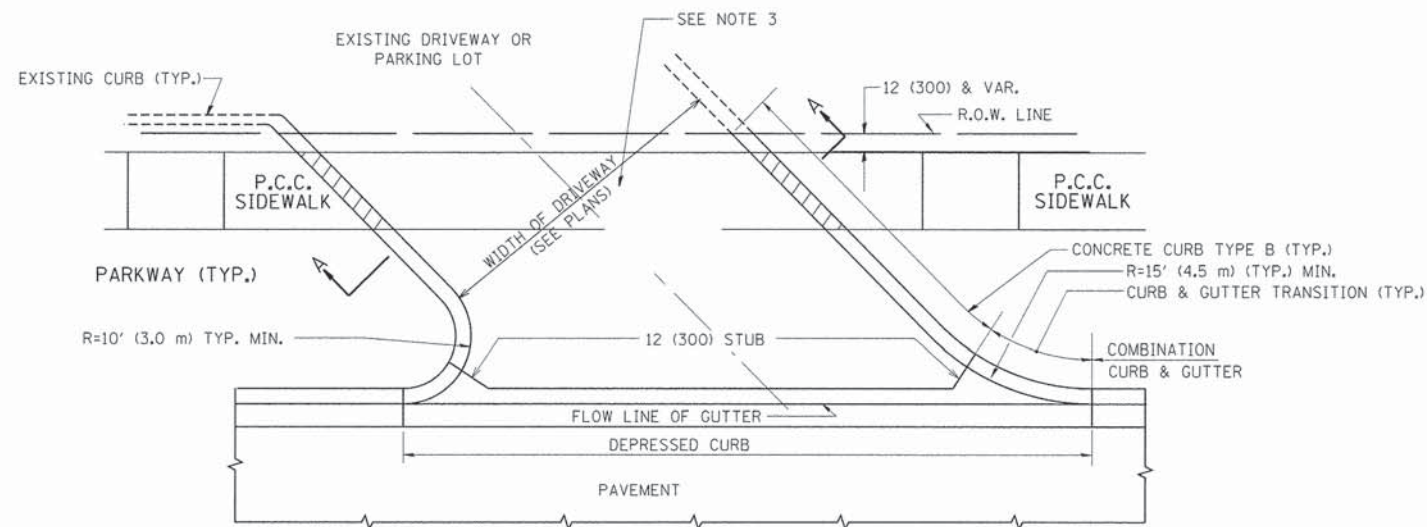
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DUPAGE			CONTRACT NO. 61A14	

SHEET NO. 5 OF 5 SHEETS

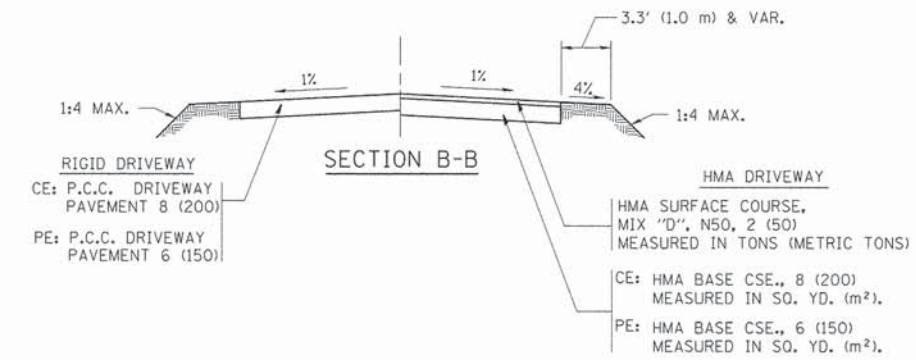
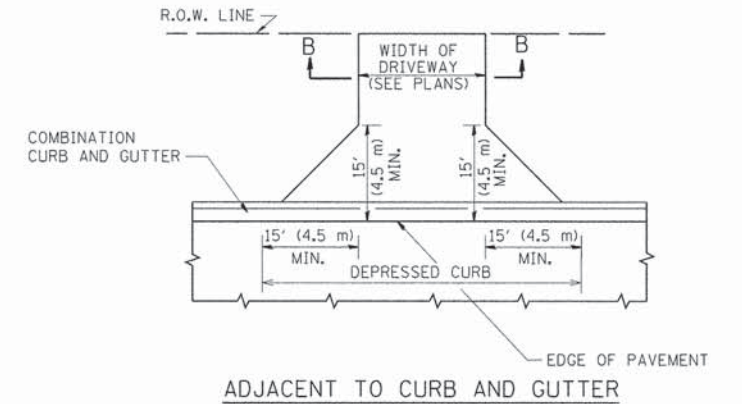
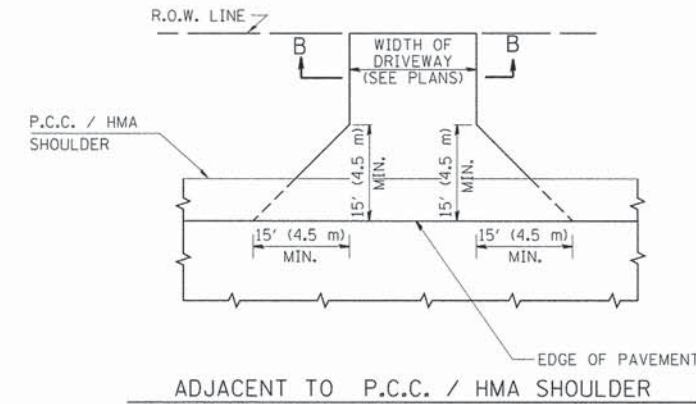
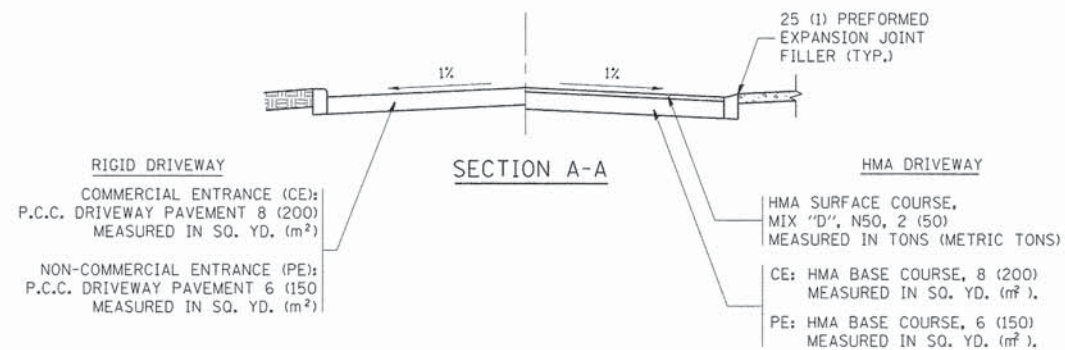
FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT



WITH CONCRETE CURB, TYPE B



WITH CONCRETE CURB, TYPE B



GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

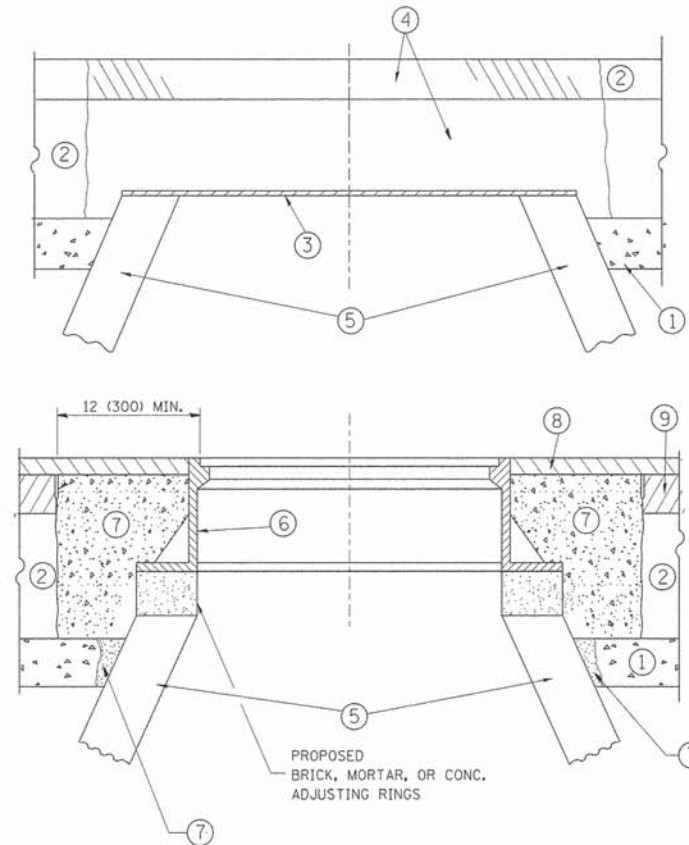
THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

FILE NAME =	USER NAME = leyso	DESIGNED - R. SHAH	REVISED - P. LoFLUER 04-15-03	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
cr\pw_work\pwsdot\leyso\bd015\bd01.dgn		DRAWN -	REVISED - R. BORO 01-01-07			2586	13-00102-00-RS	DUPAGE	40	28	
PLOT SCALE = 50.0000' / 1"		CHECKED -	REVISED - R. BORO 06-11-08			BD0156-07 (BD-01)					
PLOT DATE = 9/6/2011		DATE - 11-04-95	REVISED - R. BORO 09-06-11			CONTRACT NO.					
					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

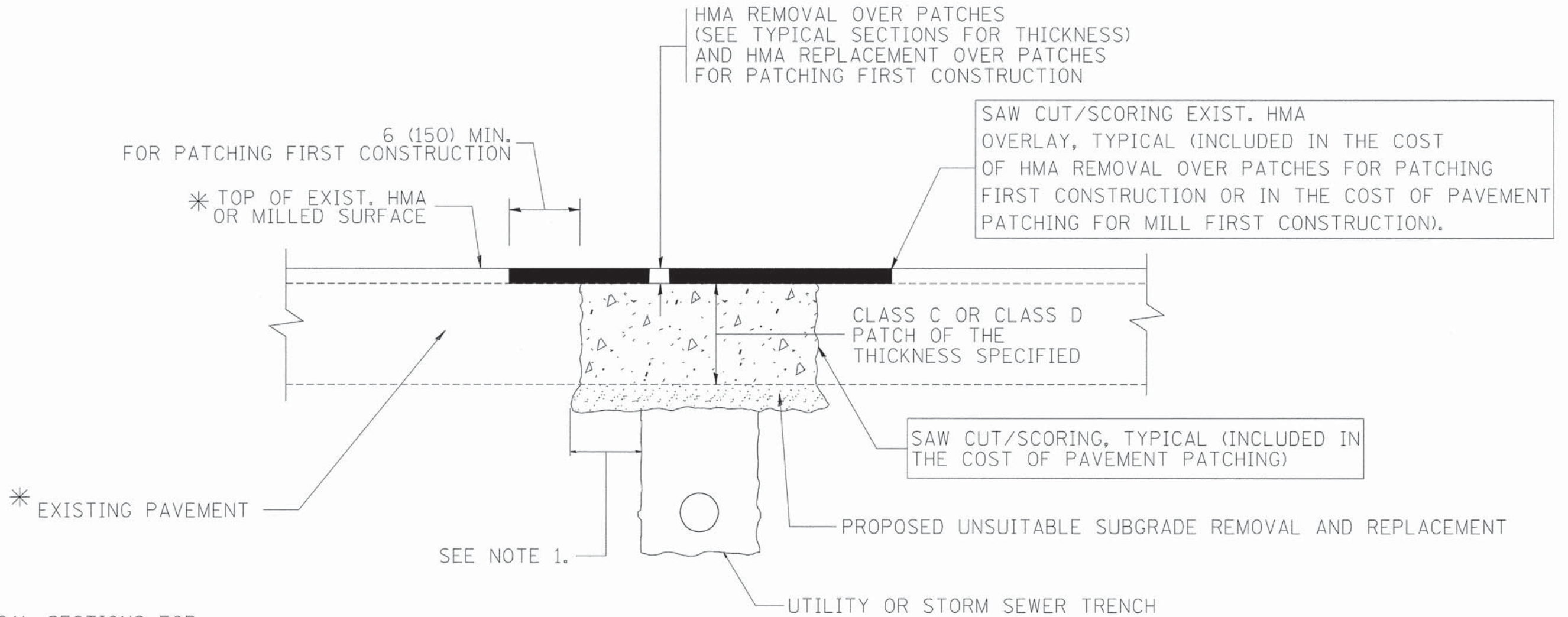
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		CHECKED -	REVISED - R. BORO 03-09-11
		DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2586	13-00102-00-RS	DUPAGE	40	29
BD600-03 (BD-8)			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	F.A. RTE. = 2586	SECTION = 13-00102-00-RS	COUNTY = DUPAGE	TOTAL SHEETS = 40	SHEET NO. = 30	
PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - R. BORO 01-01-07	REVISED - R. BORO 09-04-07			BD400-04 (BD-22)		CONTRACT NO.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	
PLOT DATE = 10/27/2008	DATE = 10-25-94	REVISED - K. ENG 10-27-08	SCALE: NONE			SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.			

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

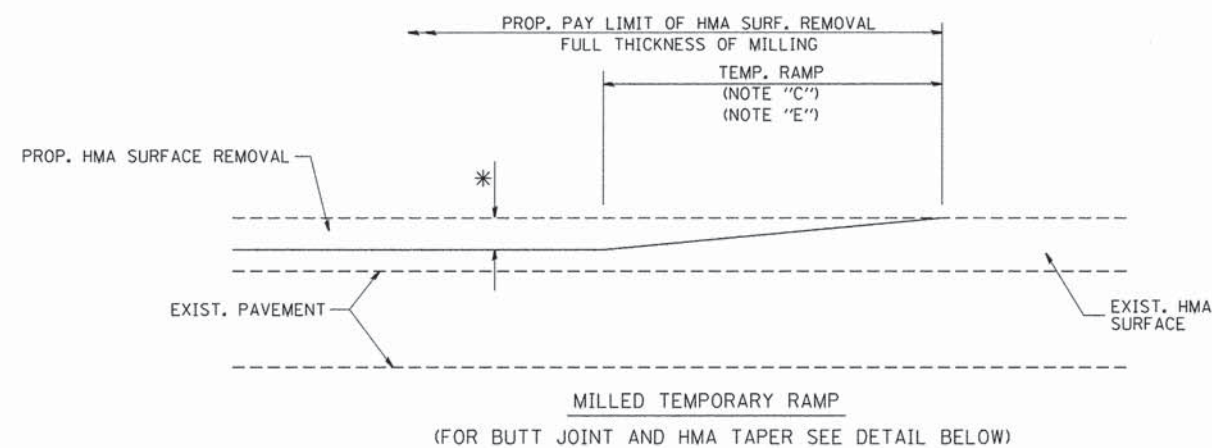
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

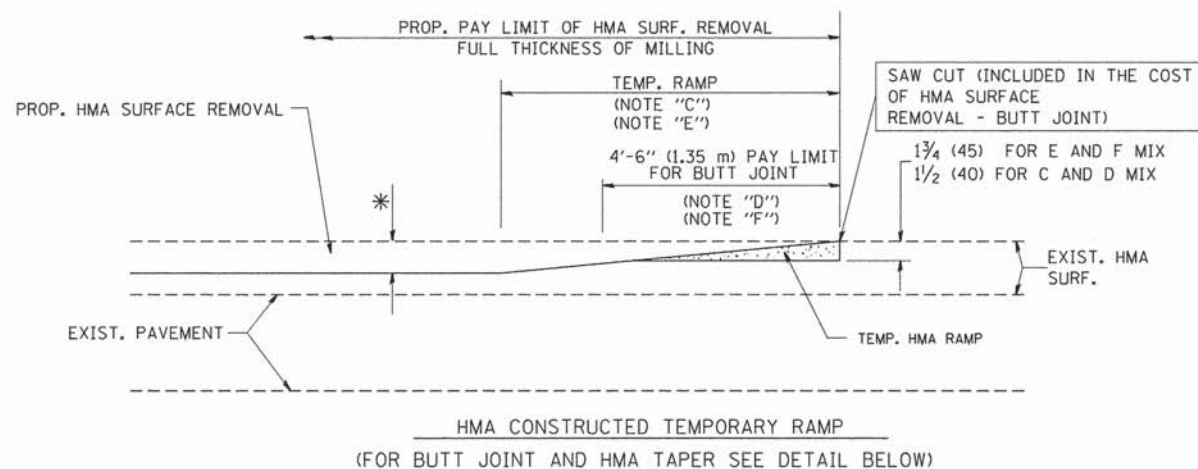
**CURB OR CURB AND GUTTER
REMOVAL AND REPLACEMENT**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2586	13-00102-00-RS	DUPAGE	40	31
BD600-06 (BD-24)			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

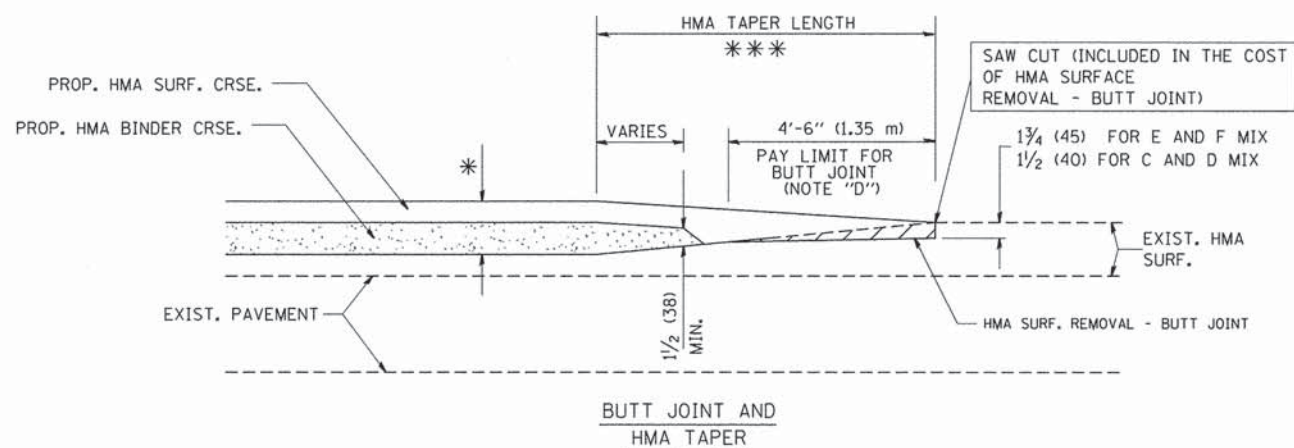


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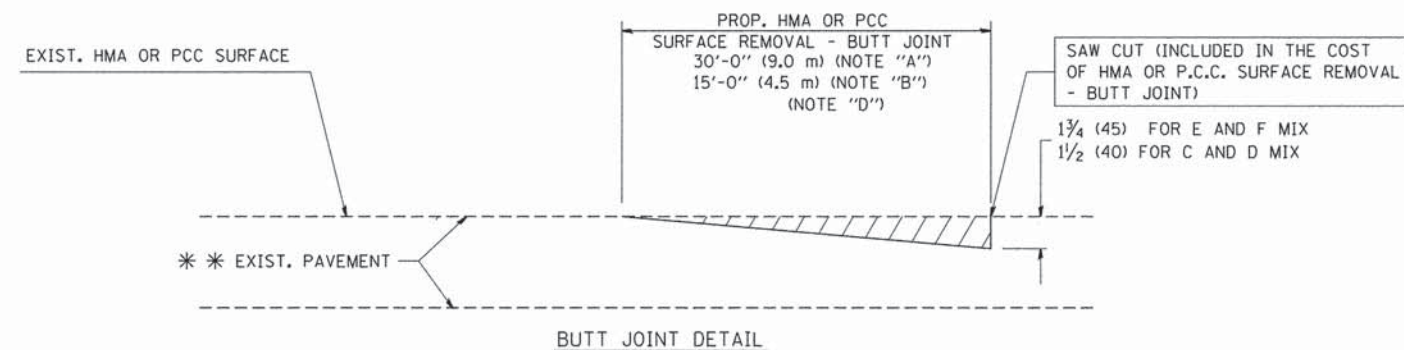


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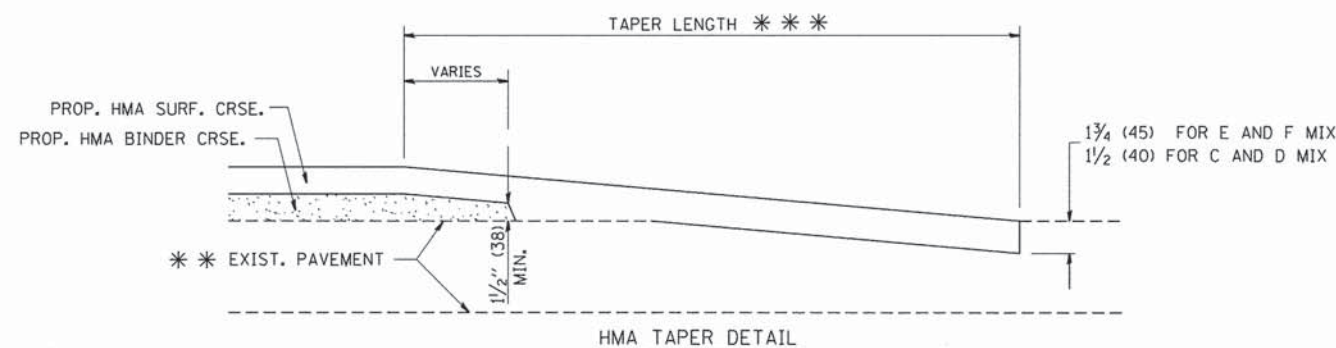
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\dietsd\22x34\bd32.dgn

USER NAME = goglianobt
 PLOT SCALE = 50.0000" / IN.
 PLOT DATE = 1/4/2008

DESIGNED - M. DE YONG
 DRAWN -
 CHECKED -
 DATE - 06-13-90

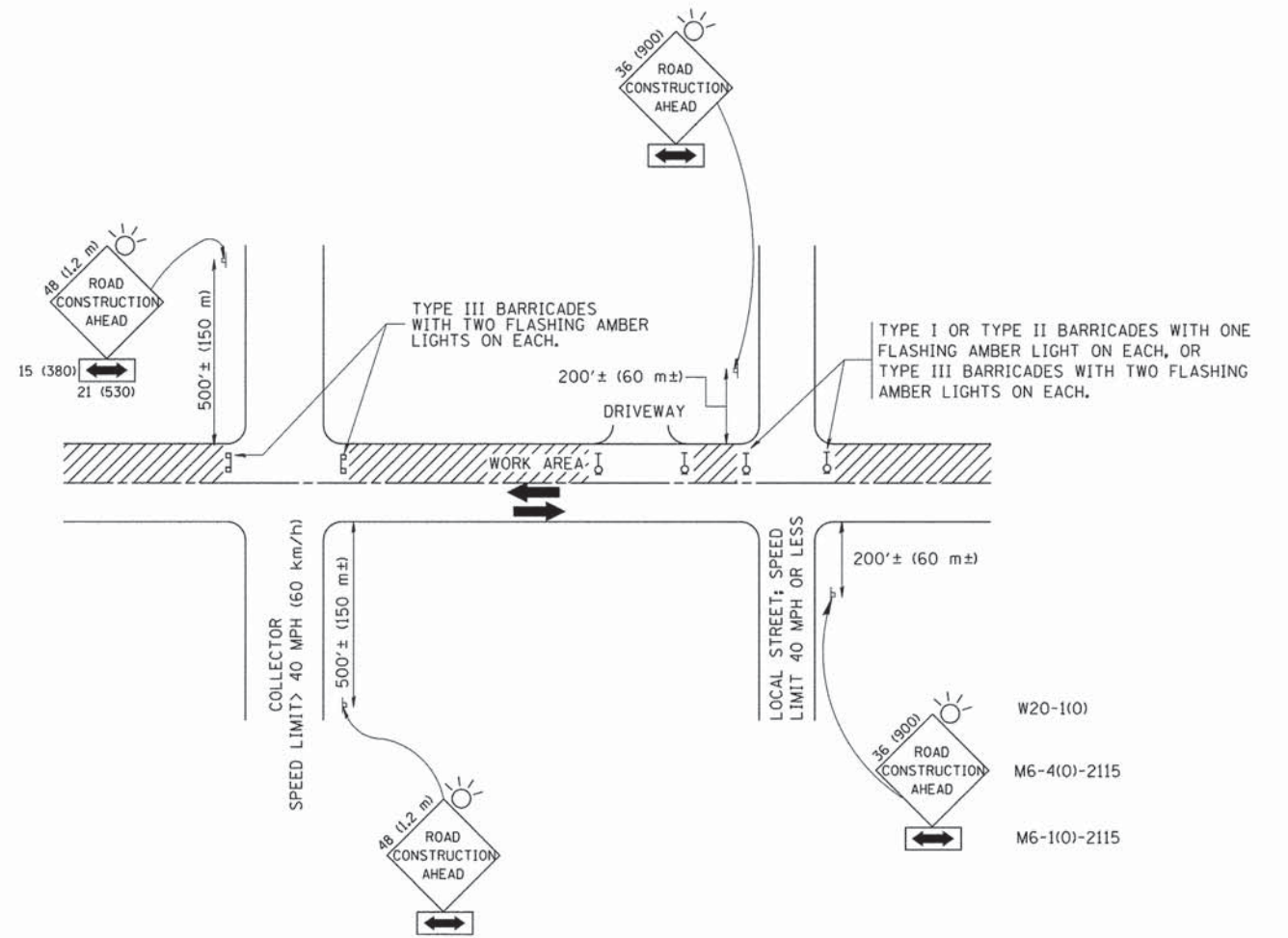
REVISED - R. SHAH 10-25-94
 REVISED - A. ABBAS 03-21-97
 REVISED - M. GOMEZ 04-06-01
 REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
 HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2586	13-00102-00-RS	DUPAGE	40	32
BD400-05 BD32			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

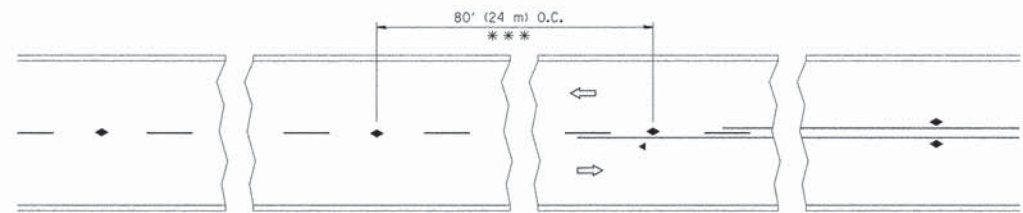
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	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

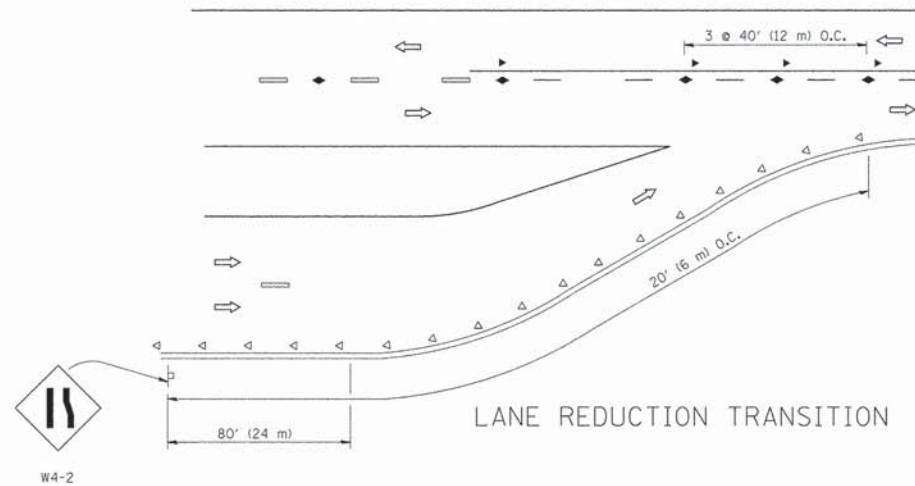
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

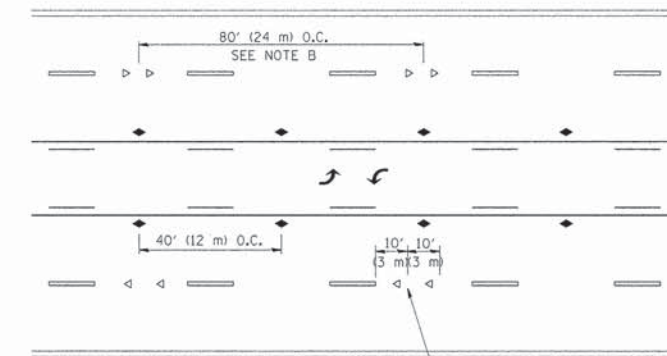


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

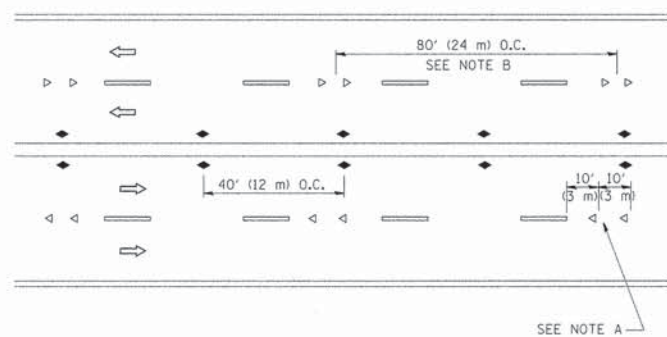
TWO-LANE/TWO-WAY



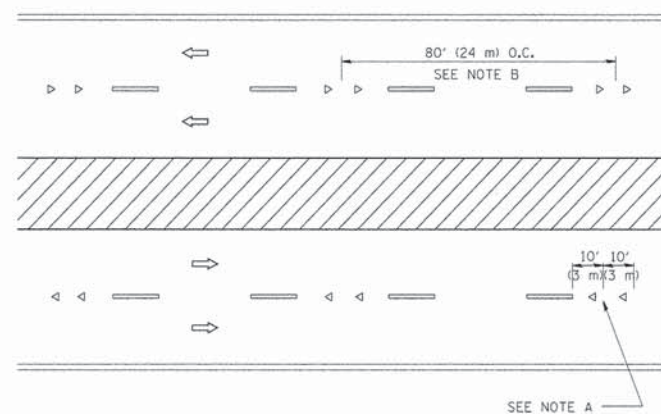
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

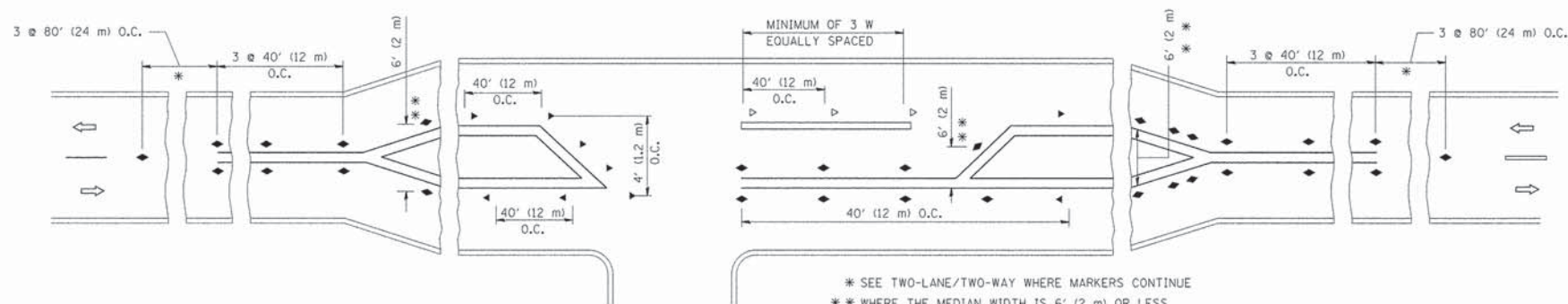
- YELLOW STRIPE
- WHITE STRIPE
- ◄ ONE-WAY AMBER MARKER
- ◄ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (16 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

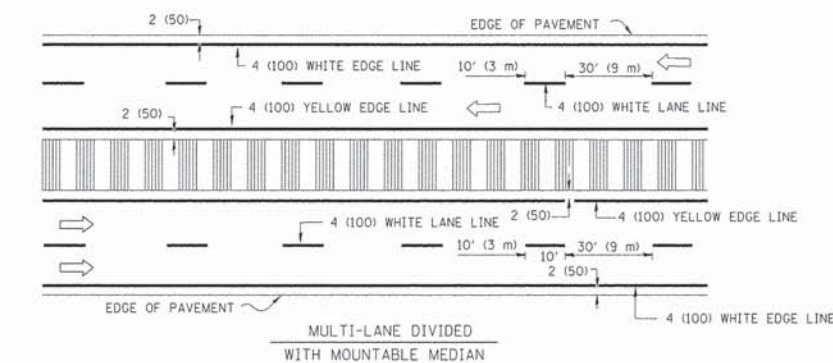
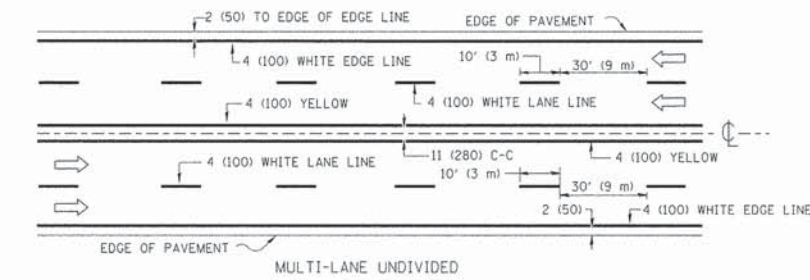
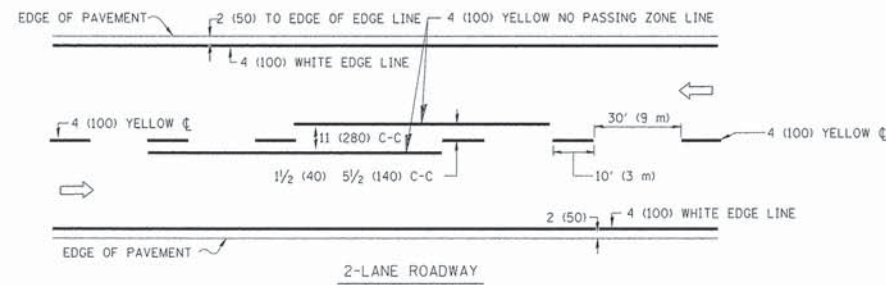
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	PLOT DATE = 9/9/2009	DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

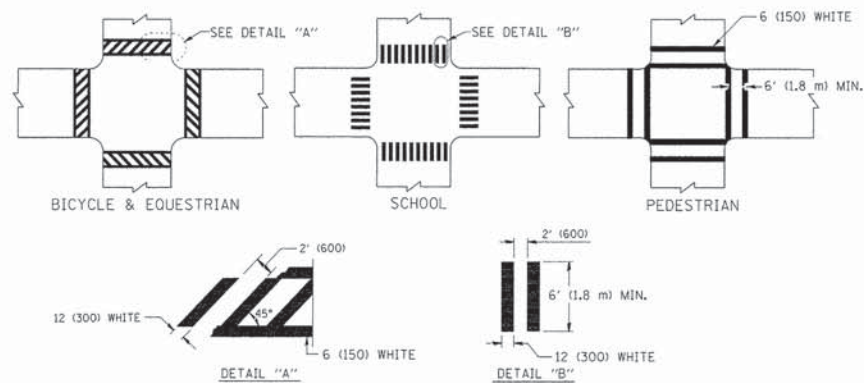
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-11			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

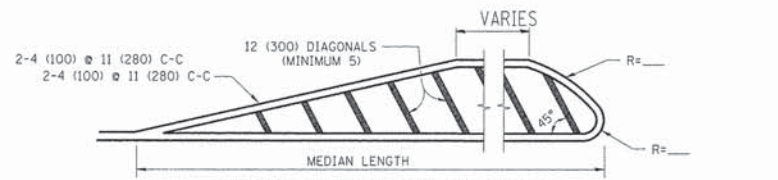
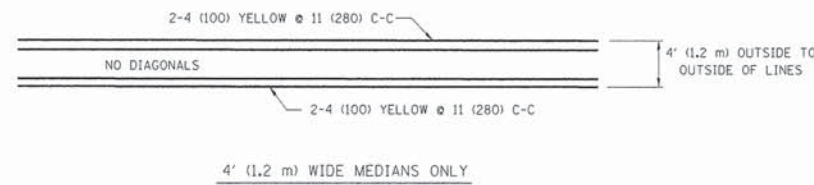


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

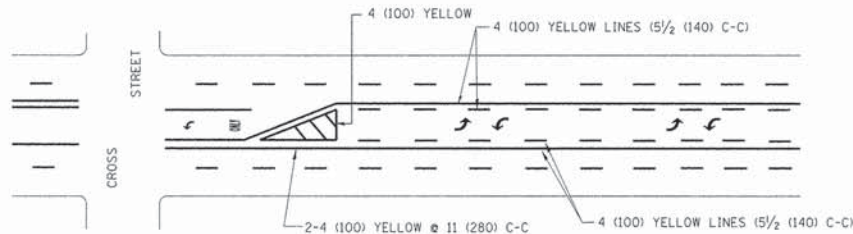


TYPICAL CROSSWALK MARKING

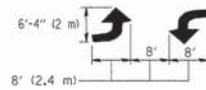


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

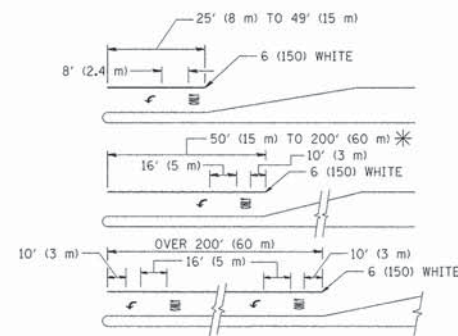


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

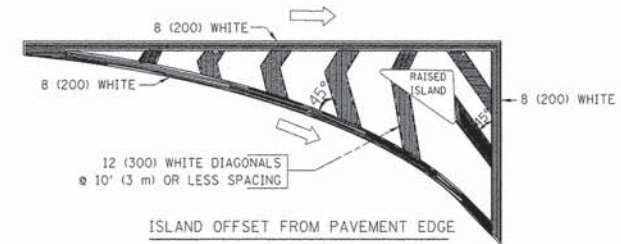


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

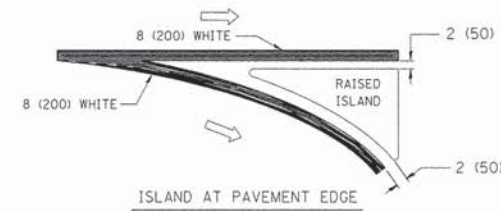
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

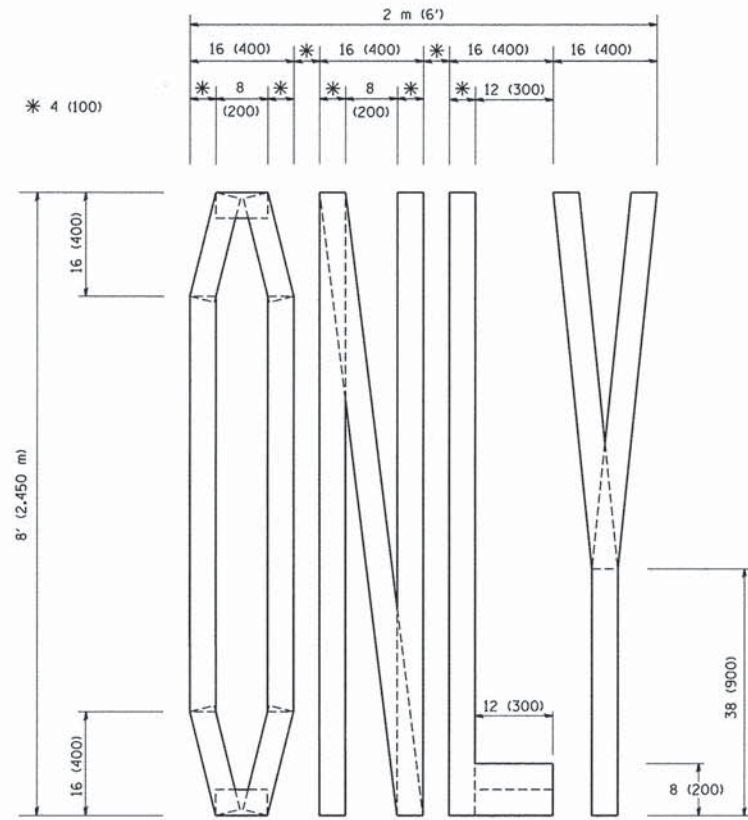
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

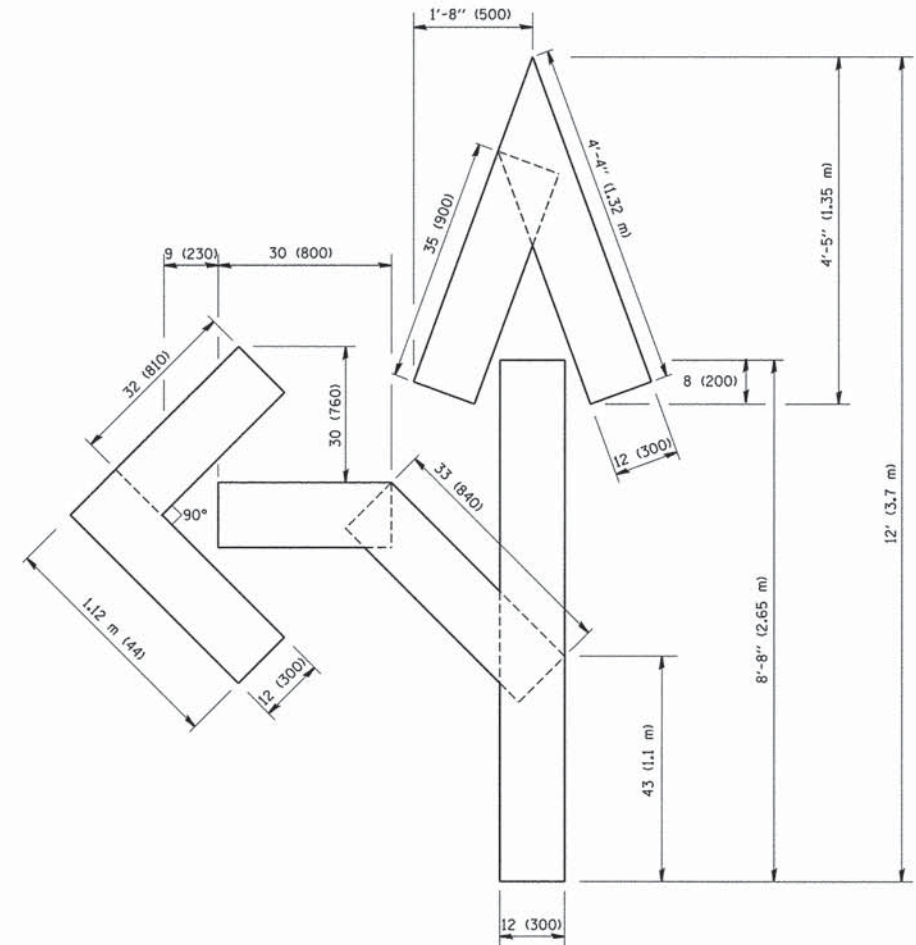
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

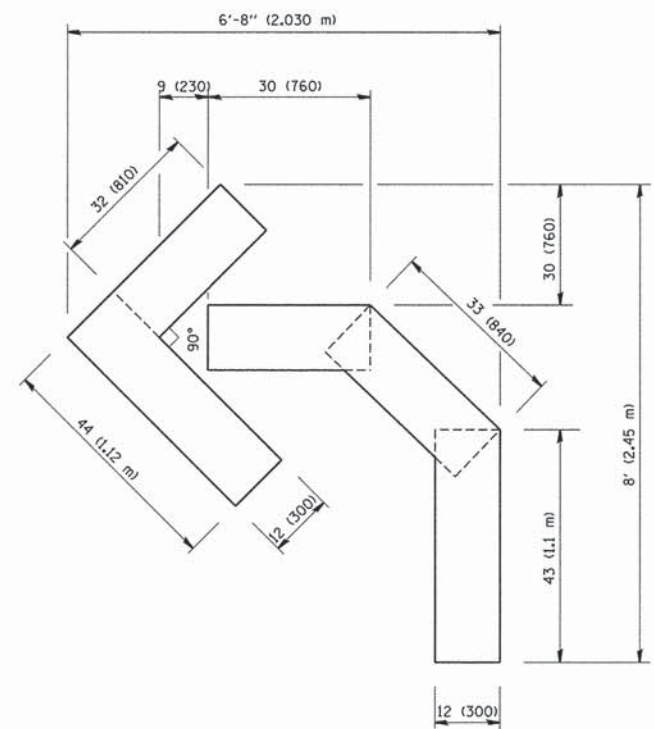
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TYPICAL PAVEMENT MARKINGS		2586	13-00102-00-RS	DUPAGE	40	35
SCALE: NONE		TC-13		CONTRACT NO.		
SHEET NO. 1 OF 1 SHEETS		STA.	TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



QUANTITY
4 (100) LINE = 64.1 ft. (19.7 m)
21.1 sq. ft. (1.97 sq. m)



QUANTITY
4 (100) LINE = 82.5 ft. (25.3 m)
27.5 sq. ft. (2.53 sq. m)



QUANTITY
4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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PLOT DATE = 1/4/2008	DATE = 09-18-94	REVISED -E. GOMEZ 08-28-00	

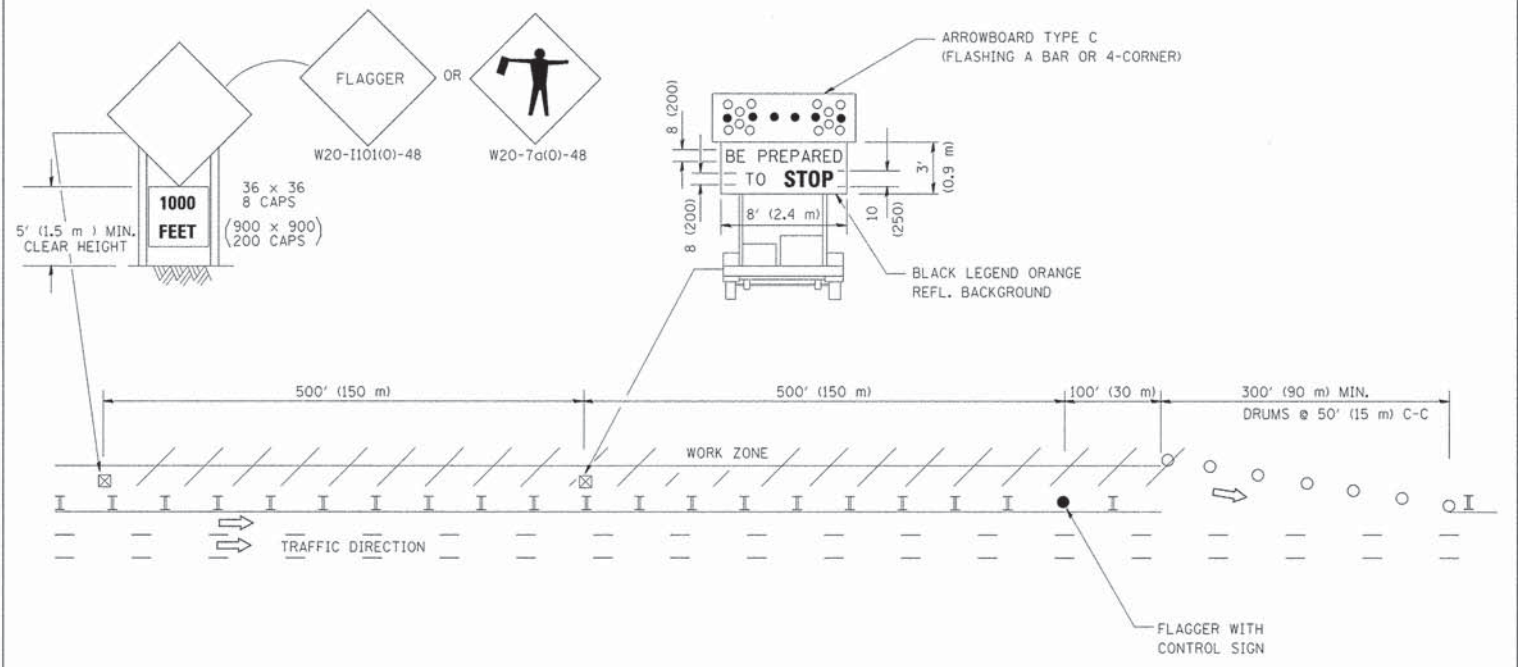
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

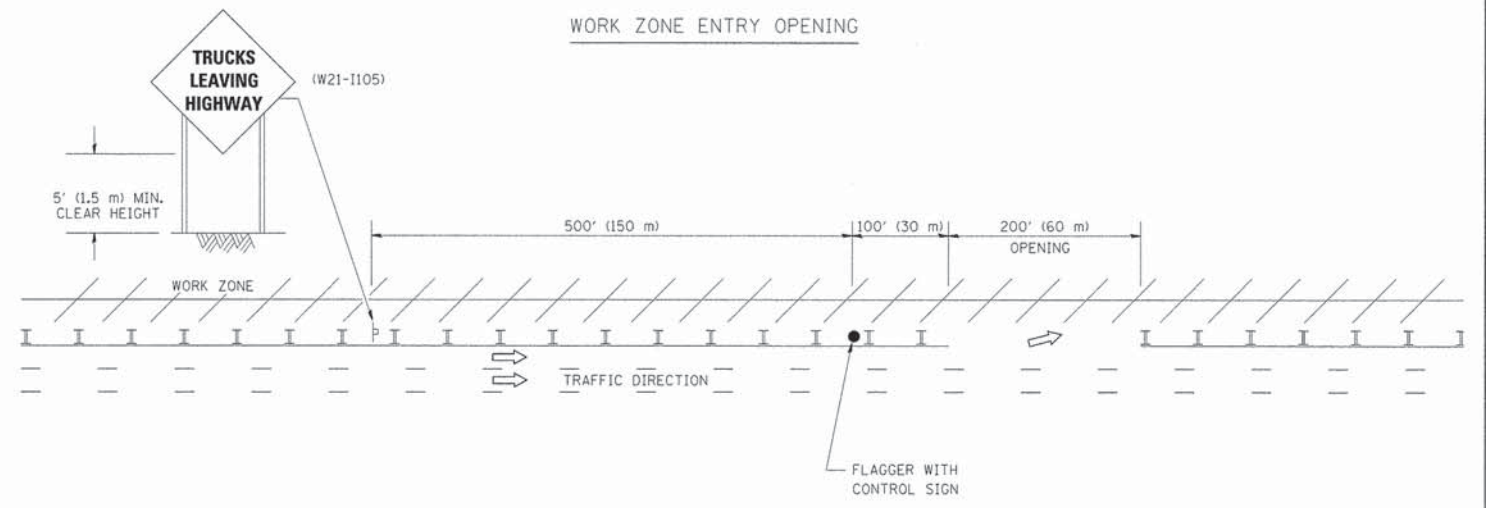
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2586	13-00102-00-RS	DUPAGE	40	36
TC-16			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. THE ARROWBOARD, THE FLAGGER AHEAD SIGN AND THE TRUCKS LEAVING HIGHWAY SIGN SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
2. WORK ZONE EXIT OPENINGS SHOULD BE A MINIMUM OF ONE HALF MILE APART.
3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

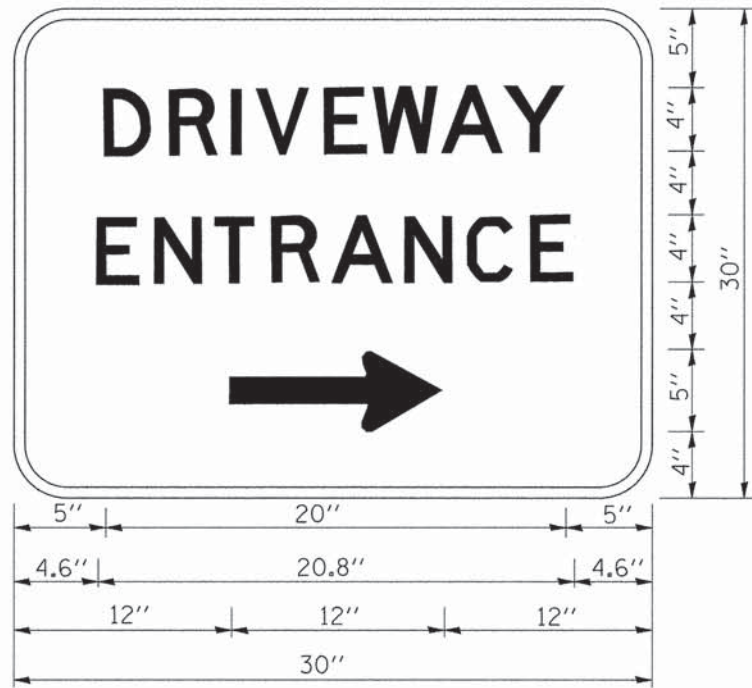
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	PLOT DATE = 1/26/2010		REVISED - S.P.B. 12-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIGNING FOR FLAGGING OPERATIONS
AT WORK ZONE OPENINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2586	13-00102-00-RS	DUPAGE	40	37
TC-18			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME = W:\diststd\22x34\to26.dgn

USER NAME = goglianobt

DESIGNED -

REVISED - C. JUCIUS 02-15-07

DRAWN -

REVISED -

PLOT SCALE = 50.000 ' / IN.

CHECKED -

REVISED -

PLOT DATE = 1/4/2008

DATE -

REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DRIVEWAY ENTRANCE SIGNING

SCALE: NONE

SHEET NO. 1 OF 1 SHEETS

STA.

TO STA.

F.A.
RTE.

2586

SECTION

13-00102-00-RS

COUNTY

DUPAGE

TOTAL SHEETS

40

SHEET NO.

38

TC-26

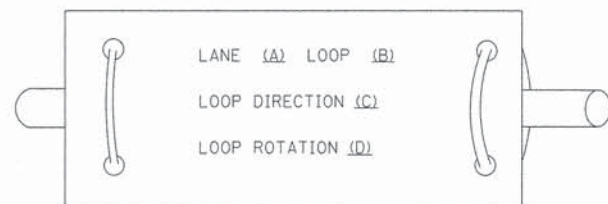
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FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT

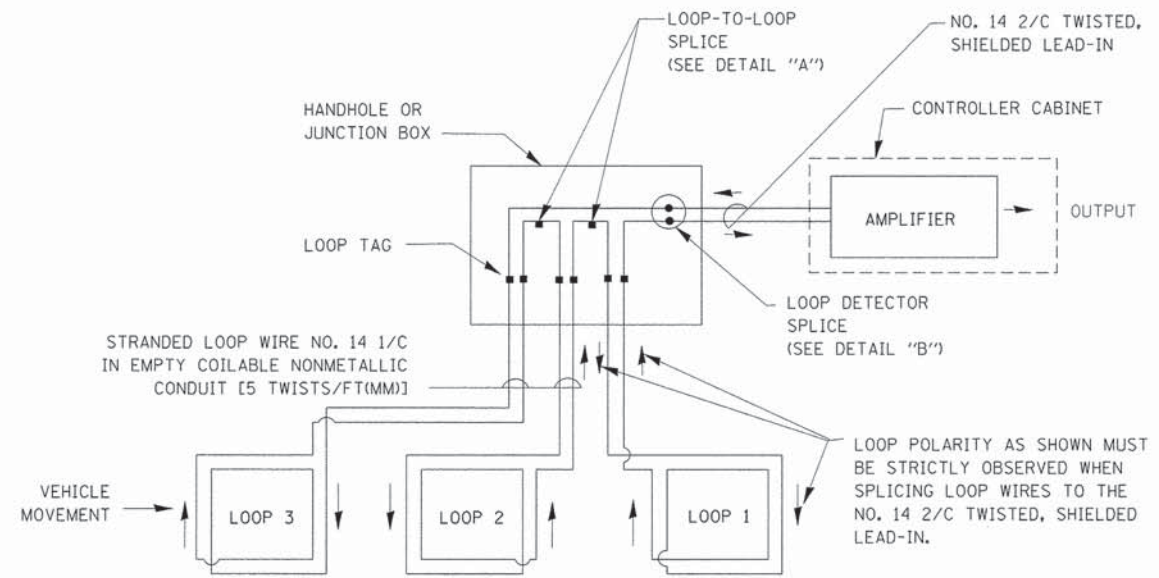
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

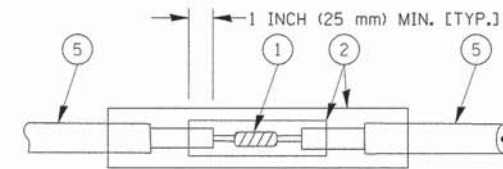


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

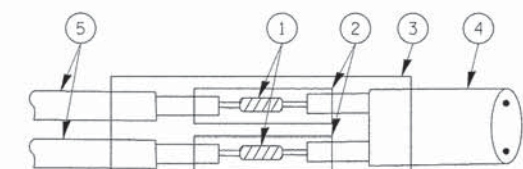


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

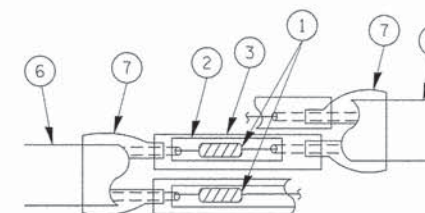


**DETAIL "A"
LOOP-TO-LOOP SPLICE**

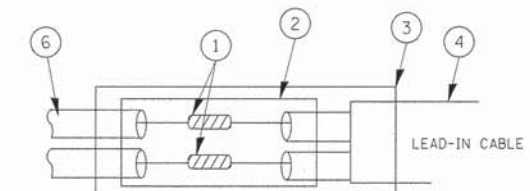


**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

TYPE I LOOP



**DETAIL "A"
LOOP-TO-LOOP SPLICE**



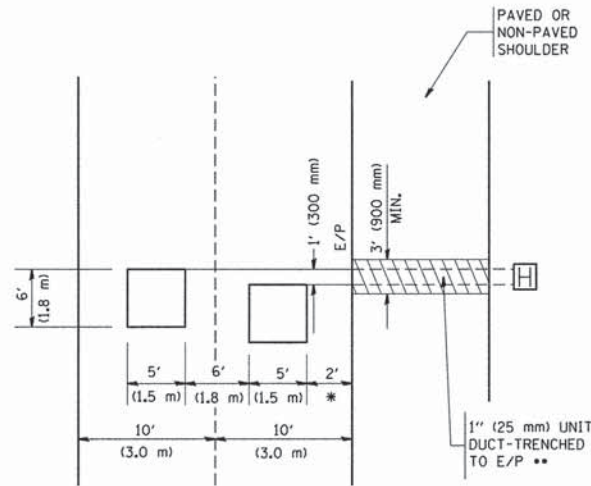
**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

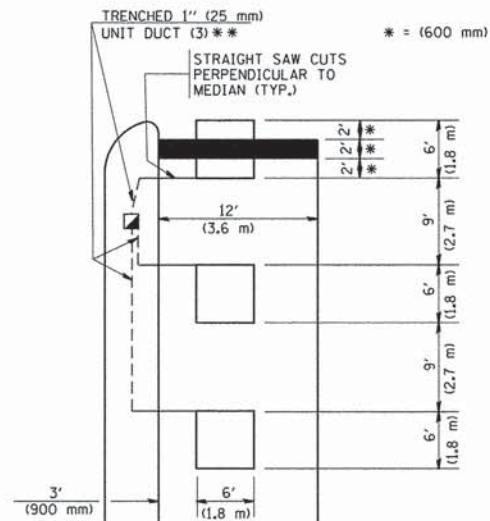


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

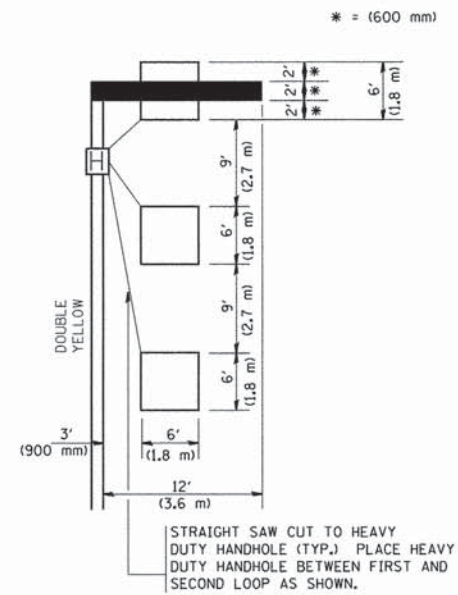


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

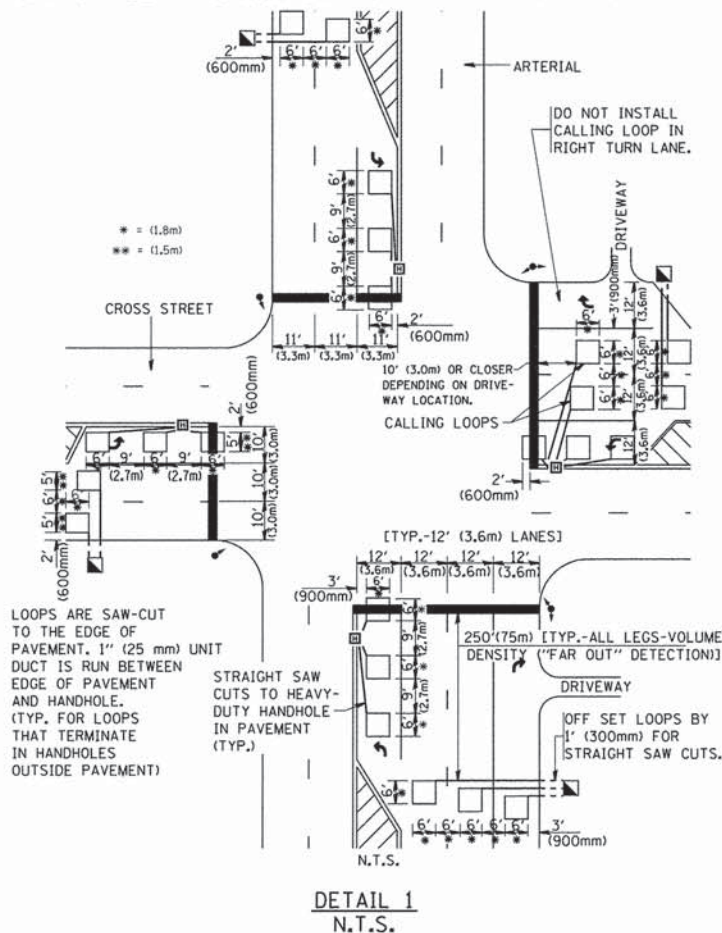
LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

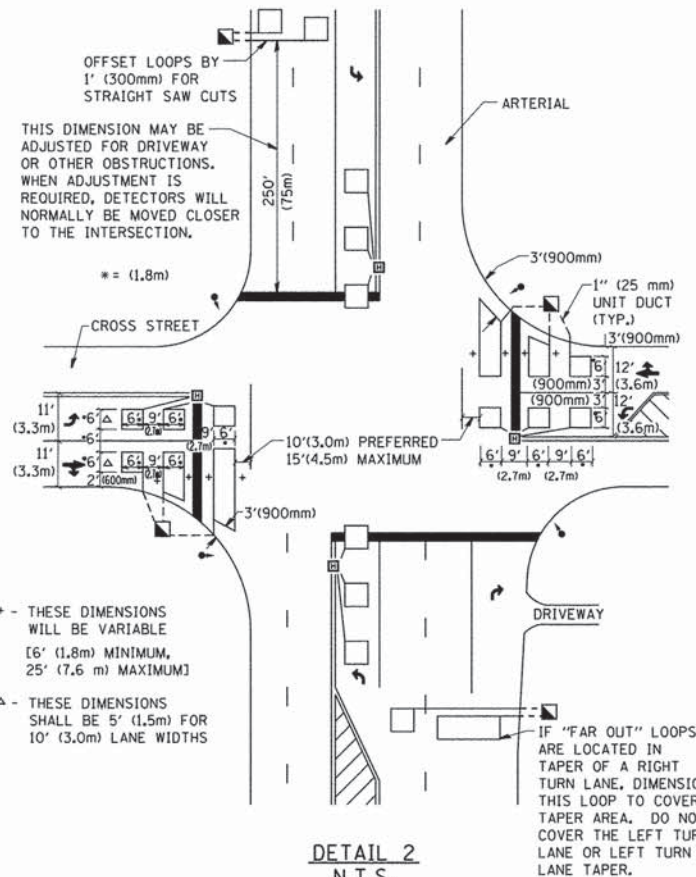
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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USER NAME = gaglianob
PLOT SCALE = 50.0000" / IN.
PLOT DATE = 1/4/2008

DESIGNED -
DRAWN -
CHECKED - R.K.F.
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2586	13-00102-00-RS	DUPAGE	40	40
TS-07			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				