

FEDERAL AID PROGRAM ENGINEER: FAWAD AQUEEL, P.E. 847-705-4021, SCHALMURBURG IL  
CONSULTING ENGINEER: ENGINEERING ENTERPRISES, INC. CONTACT: TIM WEIDNER 630-466-6700

FOR INDEX OF SHEETS AND  
HIGHWAY STANDARDS  
SEE SHEET NO. 2

STATE OF ILLINOIS 04-25-14 LETTING ITEM 021  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

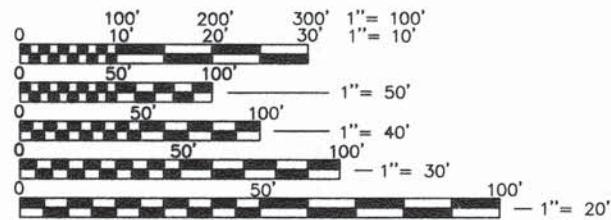
**PLANS FOR PROPOSED  
FEDERAL AID HIGHWAY**  
FAU 2312 (DUGAN ROAD)  
FAP 573 (U.S. 30) TO NORTH VILLAGE LIMITS  
RESURFACING  
SECTION: 13-00027-00-RS  
PROJECT NUMBER: M-4003(252)  
VILLAGE OF SUGAR GROVE  
KANE COUNTY  
C-91-109-14

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2312	13-00027-00-RS	KANE	14	1
CONTRACT NO. 61A17				

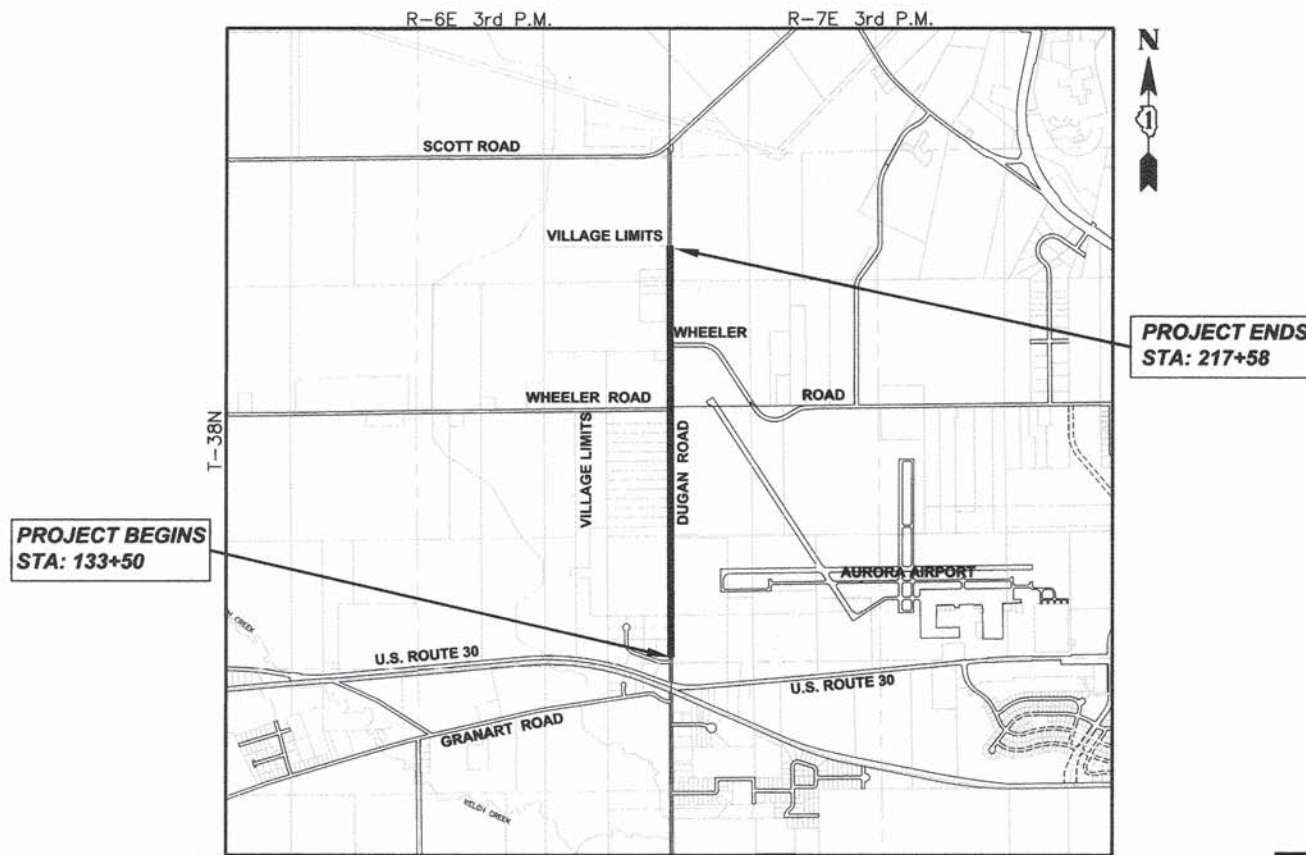


**DESIGN DESIGNATION**  
DUGAN ROAD MAJOR COLLECTOR  
DESIGN SPEED = 55 M.P.H.  
POSTED SPEED = NOT POSTED  
ADT (2013) = 1,300 VPD

PROJECT LOCATED  
IN THE VILLAGE OF  
SUGAR GROVE



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.



PROJECT BEGINS  
STA: 133+50

PROJECT ENDS  
STA: 217+58

E 1/2 SEC . 12, E 1/2 SEC . 13 AND AND NE 1/4 SEC . 24, T38N, R6E, 3RD PM, BIG ROCK TOWNSHIP AND  
W 1/2 SEC . 7, W 1/2 SEC . 18 AND NW 1/4 SEC . 19, T38N, R7E, 3RD PM, SUGAR GROVE TOWNSHIP

LOCATION MAP  
1" = 2,000'

GROSS LENGTH OF PROJECT = 8,408 FEET (1.592 MILES)  
NET LENGTH OF PROJECT = 8,408 FEET (1.592 MILES)

**JULIE JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION CALL 811**

Know what's below.  
Call before you dig.

CONTRACT NO. 61A17

**40 YEARS**  
Engineering Enterprises, Inc.  
Consulting Engineers  
52 Wheeler Road  
Sugar Grove, Illinois 60554  
630.466.6700 / www.eelweb.com

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

APPROVED 1/22/2014 2014  
*Ant Spicile*  
VILLAGE OF SUGAR GROVE, DIRECTOR OF PUBLIC WORKS

PASSED FEBRUARY 10, 2014 2014  
*C. J. Holt*  
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW February 10 2014  
*Timothy V. Weidner*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

DATE: 1/22/2014

BY: *Timothy V. Weidner*  
TIMOTHY V. WEIDNER

LICENSE EXPIRES: NOVEMBER 30, 2015

SEAL

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Plotted: January 22, 2014 @ 10:50 AM By: Larry Nolan - Tab: 01 Cover 22-34

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**GENERAL NOTES**

**SPECIFICATIONS, STANDARDS, AND SPECIAL PROVISIONS**

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, ADOPTED JANUARY 1, 2012 (HEREINAFTER REFERRED TO AS THE STANDARD SPECIFICATIONS), THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JANUARY 1, 2014, THE LATEST EDITION OF THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, THE STANDARD SPECIFICATIONS FOR WATER & SEWER MAIN CONSTRUCTION IN ILLINOIS, SIXTH EDITION, THE CODES AND ORDINANCES OF THE VILLAGE OF CAMPTON HILLS, ILLINOIS, THE DETAILS IN THE PLANS AND THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS.

NO SUBSTITUTIONS OR VARIANCES WILL BE PERMITTED TO ANY STANDARD NOTES OR ORDINANCES UNLESS APPROVED OTHERWISE IN WRITING PRIOR TO COMMENCING CONSTRUCTION ACTIVITY.

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH ARTICLE 107.14 OF THE STANDARD SPECIFICATIONS.

THE CONTRACTOR SHALL AT ALL TIMES PROVIDE PROTECTION FOR TRAFFIC AS CALLED FOR IN THE APPLICATION OF TRAFFIC CONTROL DEVICES, THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS AND THE PLANS

**UTILITIES**

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL EXISTING AND PROPOSED UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.

THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, FIELD TILES AND ANY OTHER PUBLIC OR PRIVATE UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND NOT NECESSARILY COMPLETE; THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE RESTORED TO A CONDITION EQUAL TO THAT EXISTING BEFORE THE DAMAGE OCCURRED. THIS WORK SHALL BE ARRANGED BY THE UTILITY COMPANY AND SHALL BE AT THE CONTRACTOR'S EXPENSE.

IF EXISTING UTILITY LINES OF ANY NATURE ARE ENCOUNTERED WHICH CONFLICT IN LOCATION WITH NEW CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE ENGINEER SO THAT THE CONFLICT MAY BE RESOLVED.

UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR OPERATE ANY VALVES OR HYDRANTS.

**STAKING**

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, THE OWNER'S AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

ALL OFFSET LOCATIONS GIVEN ON THE DETAILED PLANS ARE FROM THE CENTERLINE AS SHOWN ON THE PLANS.

**MISCELLANEOUS**

SAWING OF REMOVAL ITEMS AS NOTED ON THE PLANS, SPECIFIED IN THE STANDARD SPECIFICATIONS, OR AS REQUIRED BY THE ENGINEER SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

AT ALL MAINLINE AND SIDE ROAD BUTT JOINT LOCATIONS, THE EXISTING SURFACE SHALL BE CUT TO A MINIMUM THICKNESS OF ONE AND A HALF (1.5) INCHES AS INDICATED ON THE PLANS.

THE THICKNESS OF ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASES ON WHICH THE ASPHALT MIXTURES ARE TO BE PLACED.

THE CONTRACTOR SHALL BE REQUIRED TO MAKE ARRANGEMENTS FOR THE PROPER BRACING, SHORING AND OTHER REQUIRED PROTECTION OF ALL ROADWAYS, STRUCTURES, POLES, CABLES AND PIPE LINES, BEFORE CONSTRUCTION BEGINS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO THE STREETS OR ROADWAYS AND ASSOCIATED STRUCTURES AND SHALL MAKE REPAIRS AS NECESSARY TO THE SATISFACTION OF THE ENGINEER AT THEIR OWN EXPENSE. ANY SHEETING AND/OR SHORING USED FOR THIS IMPROVEMENT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

THE CONTRACTOR SHALL PROTECT ALL EXISTING FACILITIES (E.G. CURB, DRIVEWAYS, PAVEMENT) THAT ARE NOT INDICATED TO BE REMOVED ON THE PLANS. ANY FACILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE RESTORED TO A CONDITION EQUAL TO THAT EXISTING BEFORE THE DAMAGE OCCURRED AT THE CONTRACTOR'S EXPENSE.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS BEFORE BIDDING ON THIS CONTRACT.

EXISTING PAVEMENT THICKNESSES SHOWN ON THE PLANS ARE APPROXIMATE, BASED ON AVAILABLE INFORMATION AT THE TIME OF DESIGN. ANY ADDITIONAL COSTS REQUIRED BY THE CONTRACTOR DUE TO THICKNESSES OTHER THAN THOSE SHOWN ON THE PLANS WILL BE INCLUDED IN THE COST OF THE CONTRACT.

WHERE NEW WORK MEETS EXISTING FEATURES TO REMAIN, THE CONTRACTOR SHALL FIELD CHECK ALL DIMENSIONS AND ELEVATIONS BEFORE PROCEEDING WITH CONSTRUCTION. IMMEDIATELY NOTIFY THE ENGINEER OF ANY DISCREPANCIES.

THE CONTRACTOR WILL BE REQUIRED TO COMPLY WITH ALL STATE REGULATIONS REGARDING AIR, WATER AND NOISE POLLUTION. THE CONTRACTOR IS PROHIBITED FROM BURNING ANY MATERIAL WITHIN OR ADJACENT TO THE IMPROVEMENT.

ALL DISTURBED AREAS WITHIN THE PROJECT THAT ARE NOT OTHERWISE SURFACED SHALL BE CLEANED, LAYERED WITH TOPSOIL, AND SEED AS SHOWN IN THE PLANS. LIMITS SHOWN ON THE PLANS ARE THE MAXIMUM PAY WIDTHS FOR PAYMENT PURPOSES. ADDITIONAL AREAS DAMAGED BY MACHINERY, CONSTRUCTION EQUIPMENT, CONTRACTOR NEGLIGENCE OR OVER-EXCAVATION SHALL BE RESTORED TO A CONDITION EQUAL TO THAT EXISTING BEFORE THE DAMAGE OCCURRED AT THE COST OF THE CONTRACTOR.

THE CONTRACTOR SHALL DISPOSE OF AND REMOVE FROM THE SITE EACH DAY ALL CURB AND GUTTER, PAVEMENT AND ALL OTHER EXCAVATED MATERIAL NOT FOR SALVAGE. THE COST FOR HAULING AND TRUCKING TO DISPOSAL LOCATIONS WILL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

THE ENGINEER IS NOT RESPONSIBLE FOR THE CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OR PROCEDURES, TIME OF PERFORMANCE, PROGRAMS OR FOR ANY SAFETY PRECAUTIONS USED BY THE CONTRACTOR. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR EXECUTION OF THEIR WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS AND SPECIFICATIONS.

PATCHING LOCATIONS SHALL BE VERIFIED BY THE ENGINEER.

THE NEED FOR REMOVAL OF UNSTABLE MATERIAL AND REPLACEMENT WITH AGGREGATE SUBGRADE IMPROVEMENT AT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER (BY USE OF CONE PENETROMETER IN CONJUNCTION WITH THE IDOT SUBGRADE STABILITY MANUAL). IF UNSTABLE MATERIAL IS ENCOUNTERED, THE MATERIAL SHALL BE REMOVED AND REPLACED WITH AGGREGATE SUBGRADE IMPROVEMENT. IF UNSTABLE MATERIAL IS NOT ENCOUNTERED, THEN THE QUANTITY WILL BE DEDUCTED FROM THE CONTRACT WITH NO ADDITIONAL COMPENSATION PAID TO THE CONTRACTOR. GEOTECHNICAL FABRIC FOR GROUND STABILIZATION SHALL BE PLACED AT ALL LOCATIONS REQUIRING REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL AND AGGREGATE SUBGRADE IMPROVEMENT AS APPROVED BY THE ENGINEER.

TOPSOIL, SEEDING, FERTILIZER AND EROSION CONTROL BLANKET SHALL BE PLACED ADJACENT TO THE AGGREGATE SHOULDER AS DIRECTED BY THE ENGINEER. THE ENGINEER SHALL REVIEW THE SITE CONDITIONS WITH THE CONTRACTOR AFTER SHOULDER CONSTRUCTION.

DUGAN ROAD SHALL BE OPEN TO TRAFFIC AT ALL TIMES. WHEN IT IS NECESSARY TO CLOSE ONE LANE OF TRAFFIC DUE TO CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN A MINIMUM OF ONE-WAY TRAFFIC DURING CONSTRUCTION HOURS WITH THE USE OF TRAFFIC CONTROL DEVICES, SIGNS AND FLAGGERS AS APPLICABLE IN THE TRAFFIC CONTROL STANDARDS.

THE CONTRACTOR SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT ANY RESIDENT OR BUSINESS OF ANY REMOVAL AND REPLACEMENT ACTIVITIES THAT WILL INHIBIT OR PROHIBIT ACCESS TO THEIR DRIVEWAY, IN WRITING, A MINIMUM OF 48 HOURS BUT NOT MORE THAN 72 HOURS, PRIOR TO THE COMMENCEMENT OF THESE ACTIVITIES. THE MORNING OF THE WORK, THE CONTRACTOR SHALL AGAIN NOTIFY THE OWNER VERBALLY, TO ALLOW THE OWNER TIME TO MOVE THEIR VEHICLE SO AS NOT TO PROHIBIT THE VEHICLE FROM LEAVING THE DRIVEWAY UPON REMOVAL OF ANY MATERIAL. THE NOTICE GIVEN OUT BY THE CONTRACTOR SHALL PROVIDE INFORMATION REGARDING THE ANTICIPATED DATE THAT FULL ACCESS WILL BY RESTORED. COORDINATION BETWEEN ACTIVITIES SHOULD ALLOW ALL WORK TO BE DONE IN A TIMELY MANNER SO AS TO PERMIT ACCESS TO THE ROADWAY. ANY ADDITIONAL COST OF STAGING REQUIRED TO MAINTAIN ACCESS IS CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

**INDEX OF SHEETS**




SHEET NO.	SHEET DESCRIPTION
1.	COVER SHEET AND LOCATION MAP
2.	GENERAL NOTES, HIGHWAY STANDARDS, SUPPLEMENTAL LEGEND, AND INDEX OF SHEETS
3.	SUMMARY OF QUANTITIES
4.-5.	TYPICAL SECTIONS
6.-9.	GENERAL PLAN
<b>DISTRICT ONE DETAILS</b>	
10.	(TC-10) TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
11.	(TC-11) TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
12.	(TC-13) DISTRICT ONE - TYPICAL PAVEMENT MARKINGS
13.	(BD-22) PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
14.	(BD-32) BUTT JOINT AND HMA TAPER DETAILS

**HIGHWAY STANDARDS**

STD. NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
442201-03	CLASS C AND D PATCHES
701011-04	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701201-04	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS ≥ 45 MPH
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-03	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS ≥ 45 MPH
701311-03	LANE CLOSURE, 2L, 2W MOVING OPERATIONS - DAY ONLY
701336-06	LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES, FOR SPEEDS ≥ 45 MPH
701901-03	TRAFFIC CONTROL DEVICES
780001-04	TYPICAL PAVEMENT MARKINGS

**SUPPLEMENTAL LEGEND**

SEE IDOT HIGHWAY STANDARDS FOR ADDITIONAL INFORMATION

-  HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
-  CLASS D PATCHES
-  HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"

Plotted: February 7, 2014 @ 7:54 AM By: Kris Pung - Tab: 02 Index 22x34

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	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES, HIGHWAY STANDARDS,  
SUPPLEMENTAL LEGEND AND INDEX OF SHEETS**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2312	13-00027-00-RS	KANE	14	2
CONTRACT NO. 61A17				
SCALE: N.T.S.	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT - STP	

PLOT: 13-00027-00-RS-02-01.DWG FINAL ENG: SGT 302-CVR



SUMMARY OF QUANTITIES

SPECIALTY ITEM	CODE NO.	ITEM DESCRIPTION	UNIT	URBAN TOTAL QUANTITY	FEDERAL = 75%
					VILLAGE = 25%
					ROADWAY
					0005
	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	825	825
	21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	2,475	2,475
	25000210	SEEDING, CLASS 2A	ACRE	1.5	1.5
	25000400	NITROGEN FERTILIZER NUTRIENT	POUND	135	135
	25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	135	135
	25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	135	135
	25100630	EROSION CONTROL BLANKET	SQ YD	7,260	7,260
	30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	825	825
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	5	5
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	55	55
	40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	2,834	2,834
	40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1,890	1,890
	44000152	HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"	SQ YD	22,480	22,480
	44201690	CLASS D PATCHES, TYPE I, 4 INCH	SQ YD	4	4
	44201692	CLASS D PATCHES, TYPE II, 4 INCH	SQ YD	60	60
	44201694	CLASS D PATCHES, TYPE III, 4 INCH	SQ YD	54	54
	44201696	CLASS D PATCHES, TYPE IV, 4 INCH	SQ YD	2,357	2,357
	48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	872	872
	67100100	MOBILIZATION	L SUM	1	1
	70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1
	70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1
	70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	L SUM	1	1
	70106800	CHANGEABLE MESSAGE SIGN	CAL MO	4	4
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	2,130	2,130
	70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	710	710
	* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	23,704	23,704
	* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	54	54
	* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	139	139
	X2110100	TOPSOIL FURNISH AND PLACE, SPECIAL	CU YD	810	810
	X4060110	BITUMINOUS MATERIALS (PRIME COAT)	POUND	15,180	15,180
	Z0004510	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"	SQ YD	816	816

\* SPECIALTY ITEMS

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

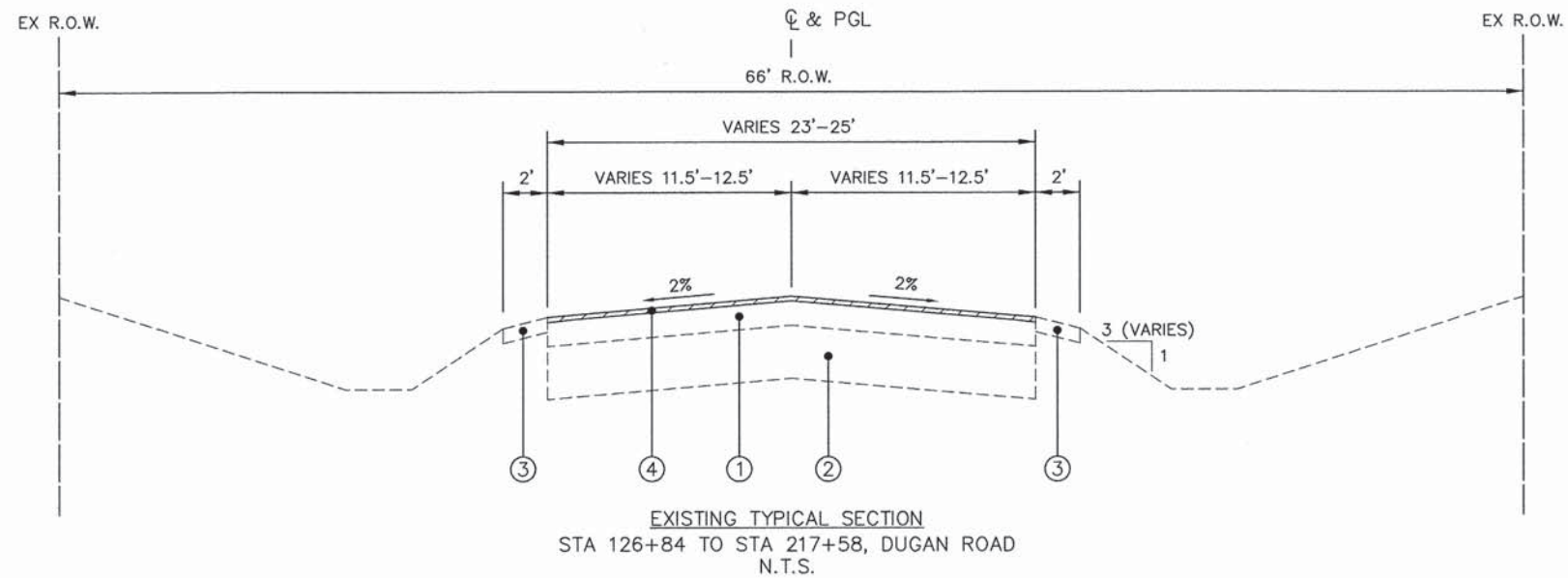
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2312	13-00027-00-RS	KANE	14	3
CONTRACT NO. 61A17			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP	

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**LEGEND**

- ① EXISTING 2" - 5.25" ASPHALT PAVEMENT
- ② EXISTING 6" - 20" AGGREGATE BASE
- ③ EXISTING 2' AGGREGATE SHOULDER
- ④ HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"
- ⑤ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1.5"
- ⑥ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2.25"
- ⑦ BITUMINOUS MATERIALS (PRIME COAT)
- ⑧ AGGREGATE WEDGE SHOULDER, TYPE B
- ⑨ TOPSOIL, SEEDING CLASS 2A, FERTILIZER AND EROSION CONTROL BLANKET (LOCATIONS AS DIRECTED BY THE ENGINEER)

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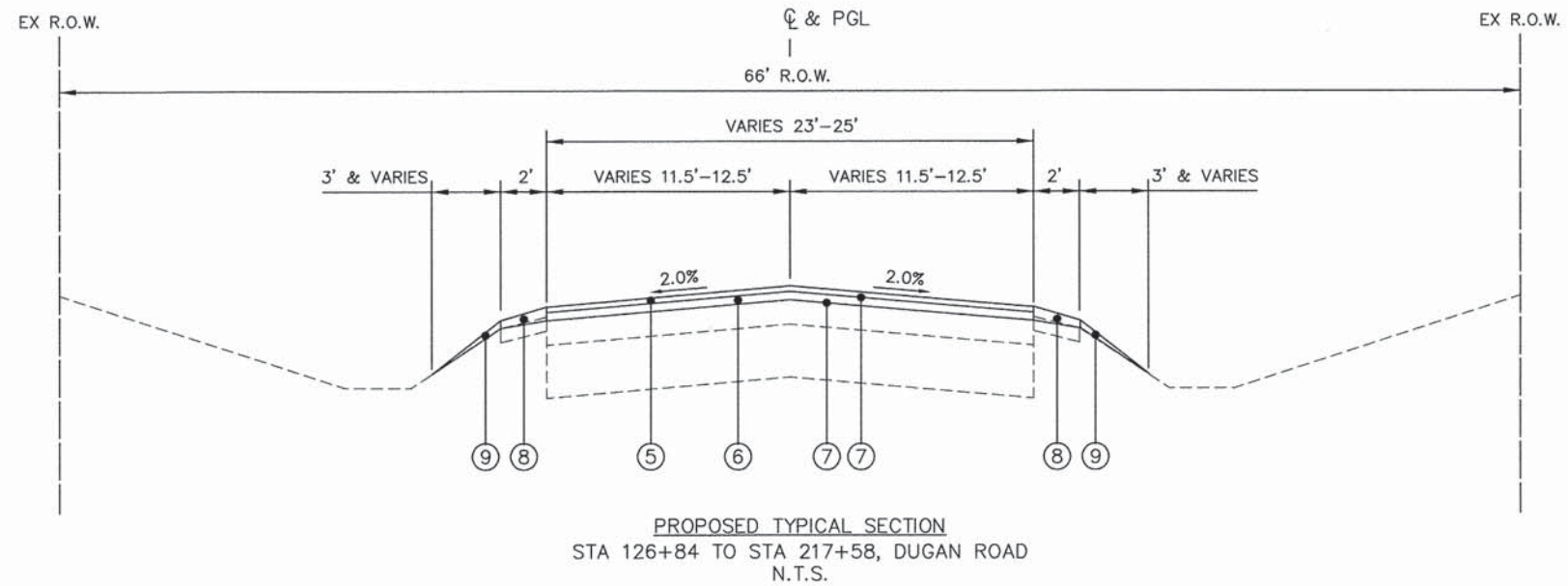
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS**

SCALE: N.T.S. SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2312	13-00027-00-RS	KANE	14	4
CONTRACT NO. 61A17				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP				

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PATCHING SHALL BE PERFORMED AFTER MILLING

**HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

OPERATION	MIXTURE TYPE	AIR VOIDS @ N <sub>des</sub>	QUALITY MANAGEMENT PROGRAM (QMP)
DUGAN ROAD RESURFACING	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2.25"	4% @ 50 Gyr.	QCP
	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 mm), 1.5"	4% @ 50 Gyr.	QCP
DRIVEWAYS	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"		QC/QA
	HMA SURFACE COURSE, MIX "D", N50 (IL-9.5 mm), 3"	4% @ 50 Gyr.	QC/QA
PATCHING	CLASS D PATCHES, 4 INCH		QC/QA
	HMA BINDER COURSE, IL-19.0, N70, 4" (IN 1 LIFT)	4% @ 70 Gyr.	QC/QA

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/INCH.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS. QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE

LEGEND	
①	EXISTING 2" - 5.25" ASPHALT PAVEMENT
②	EXISTING 6" - 20" AGGREGATE BASE
③	EXISTING 2' AGGREGATE SHOULDER
④	HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"
⑤	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1.5"
⑥	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2.25"
⑦	BITUMINOUS MATERIALS (PRIME COAT)
⑧	AGGREGATE WEDGE SHOULDER, TYPE B
⑨	TOPSOIL, SEEDING CLASS 2A, FERTILIZER AND EROSION CONTROL BLANKET (LOCATIONS AS DIRECTED BY THE ENGINEER)

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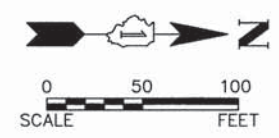
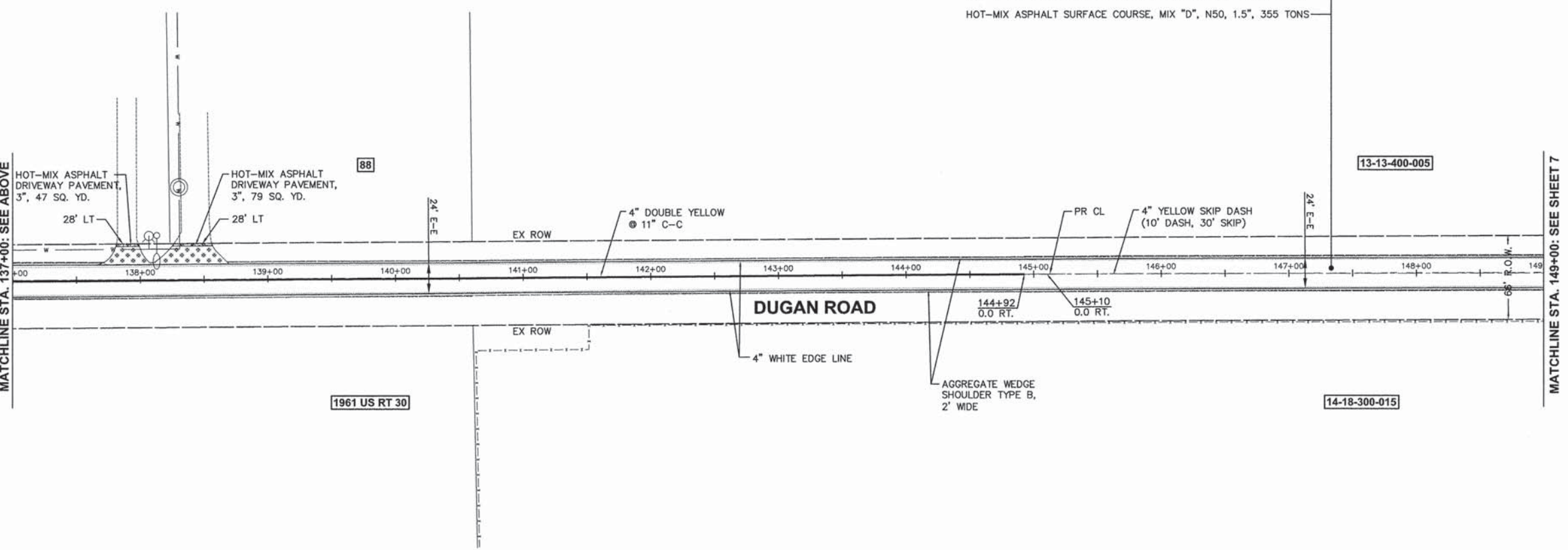
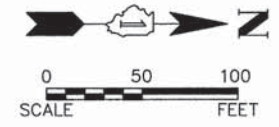
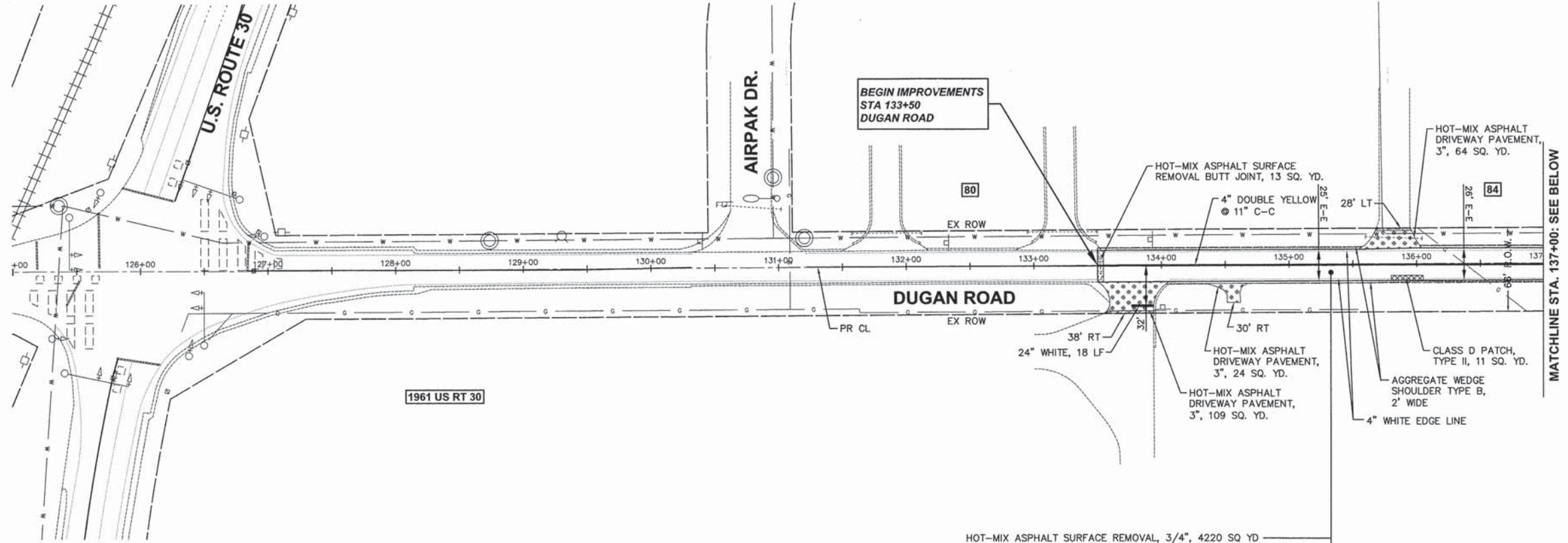
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS	
SCALE: N.T.S.	SHEET NO. 2 OF 2 SHEETS
STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2312	13-00027-00-RS	KANE	14	5
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT - STP			CONTRACT NO. 61A17	

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HOT-MIX ASPHALT SURFACE REMOVAL, 3/4", 4220 SQ YD  
 BITUMINOUS MATERIALS (PRIME COAT), 2850 POUNDS  
 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2.25", 532 TONS  
 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1.5", 355 TONS

- NOTES:**
1. ALL CLASS D PATCHES ARE 4" IN DEPTH (TYP.).
  2. ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC.
  3. RAISED REFLECTIVE PAVEMENT MARKINGS TO BE INSTALLED THROUGHOUT PROJECT LIMITS ACCORDING TO TC-11.

Plotted: February 7, 2014 @ 8:23 AM By: Kris Pung - Tab: 06 Plan 1 22x34

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USER NAME =	DESIGNED -	REVISED -
PLLOT SCALE =	DRAWN -	REVISED -
PLLOT DATE =	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

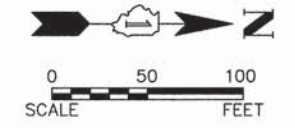
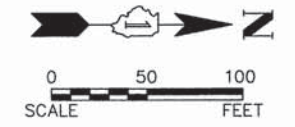
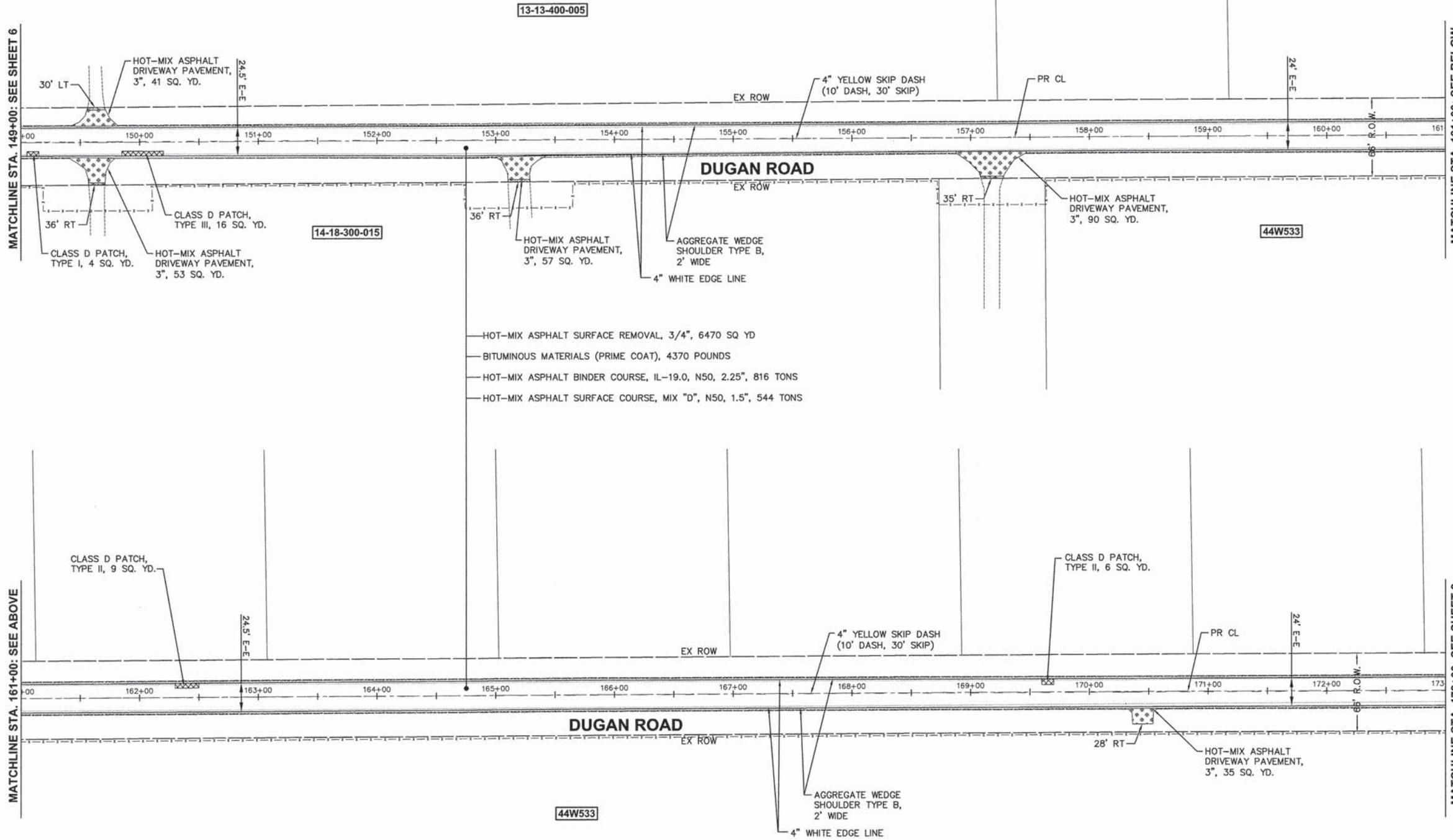
<b>DUGAN ROAD</b>	
<b>STA 126+00 TO STA 149+00</b>	
SCALE: N.T.S.	SHEET NO. 1 OF 4 SHEETS STA. 126+00 TO STA. 149+00

F.A.U. RTE. 2312	SECTION 13-00027-00-RS	COUNTY KANE	TOTAL SHEETS 14	SHEET NO. 6
CONTRACT NO. 61A17			FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT - STP	

Path: \\S:\S\PROJ\13-002\DWG\DWG\_FINAL\_ENG\01102.PLAN

MATCHLINE STA. 149+00: SEE SHEET 6

MATCHLINE STA. 161+00: SEE ABOVE



HOT-MIX ASPHALT SURFACE REMOVAL, 3/4", 6470 SQ YD  
 BITUMINOUS MATERIALS (PRIME COAT), 4370 POUNDS  
 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2.25", 816 TONS  
 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1.5", 544 TONS

- NOTES:**
1. ALL CLASS D PATCHES ARE 4" IN DEPTH (TYP.).
  2. ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC.
  3. RAISED REFLECTIVE PAVEMENT MARKINGS TO BE INSTALLED THROUGHOUT PROJECT LIMITS ACCORDING TO TC-11.

Plotted: February 7, 2014 @ 8:24 AM By: Kris Pung - Tab: 07 Plan 2, 22x34

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PLOT SCALE =	DRAWN -	REVISED -
PLOT DATE =	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DUGAN ROAD</b>	
<b>STA 149+00 TO STA 173+00</b>	
SCALE: N.T.S.	SHEET NO. 2 OF 4 SHEETS
STA. 149+00	TO STA. 173+00

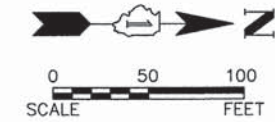
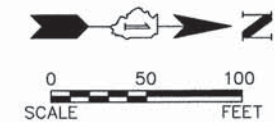
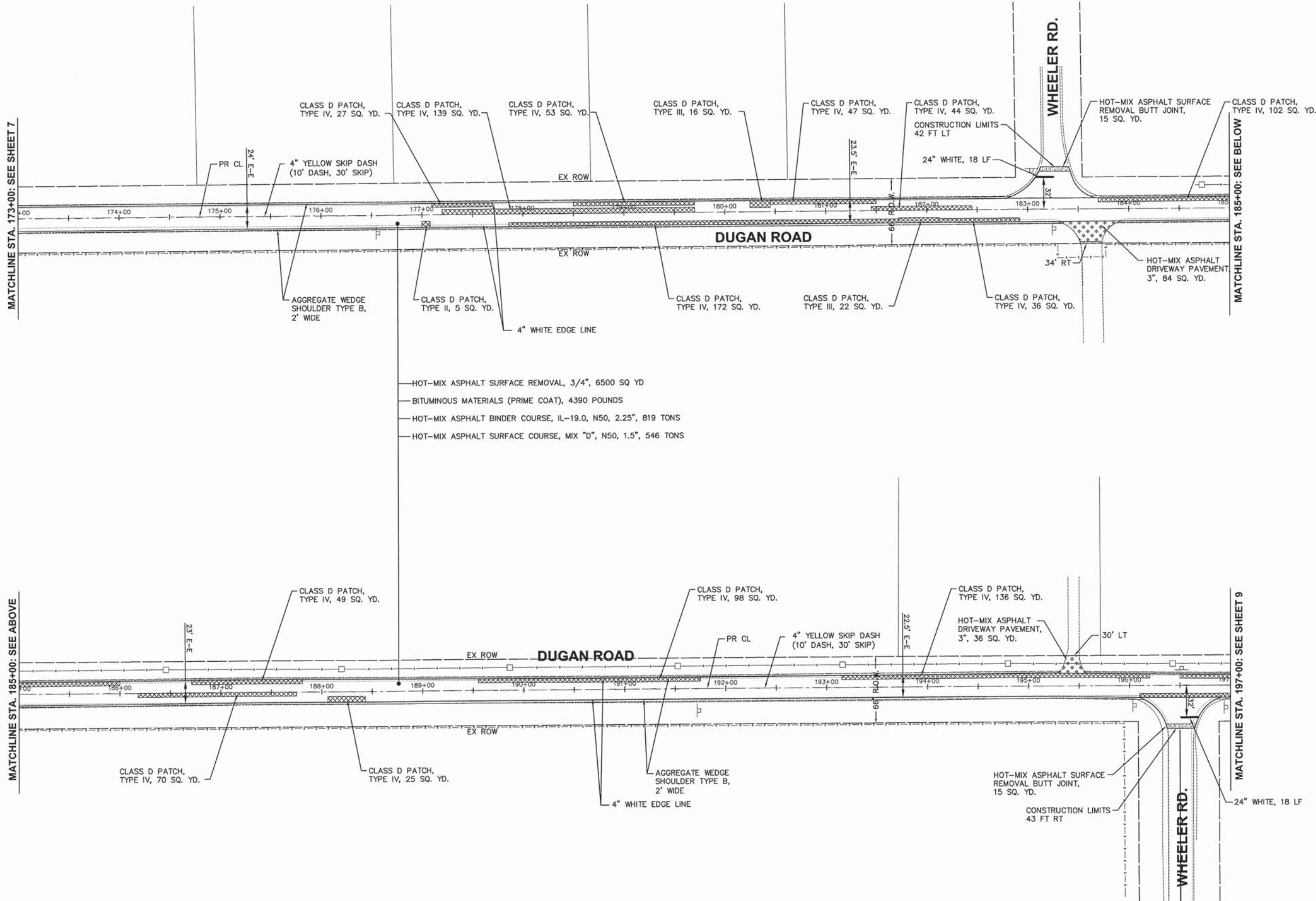
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2312	13-00027-00-RS	KANE	14	7
CONTRACT NO. 61A17			FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT - STP	

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MATCHLINE STA. 173+00: SEE SHEET 7

MATCHLINE STA. 185+00: SEE ABOVE



- NOTES:**
1. ALL CLASS D PATCHES ARE 4" IN DEPTH (TYP.).
  2. ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC.
  3. RAISED REFLECTIVE PAVEMENT MARKINGS TO BE INSTALLED THROUGHOUT PROJECT LIMITS ACCORDING TO TC-11.

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PLOT SCALE =	CHECKED -	REVISED -
PLOT DATE =	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DUGAN ROAD  
STA 173+00 TO STA 197+00**

SCALE: N.T.S. SHEET NO. 3 OF 4 SHEETS STA. 173+00 TO STA. 197+00

F.A.I. RTE. 2312	SECTION 13-00027-00-RS	COUNTY KANE	TOTAL SHEETS 14	SHEET NO. 8
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT - STP			CONTRACT NO. 61A17	

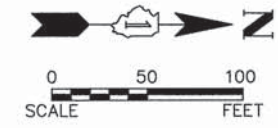
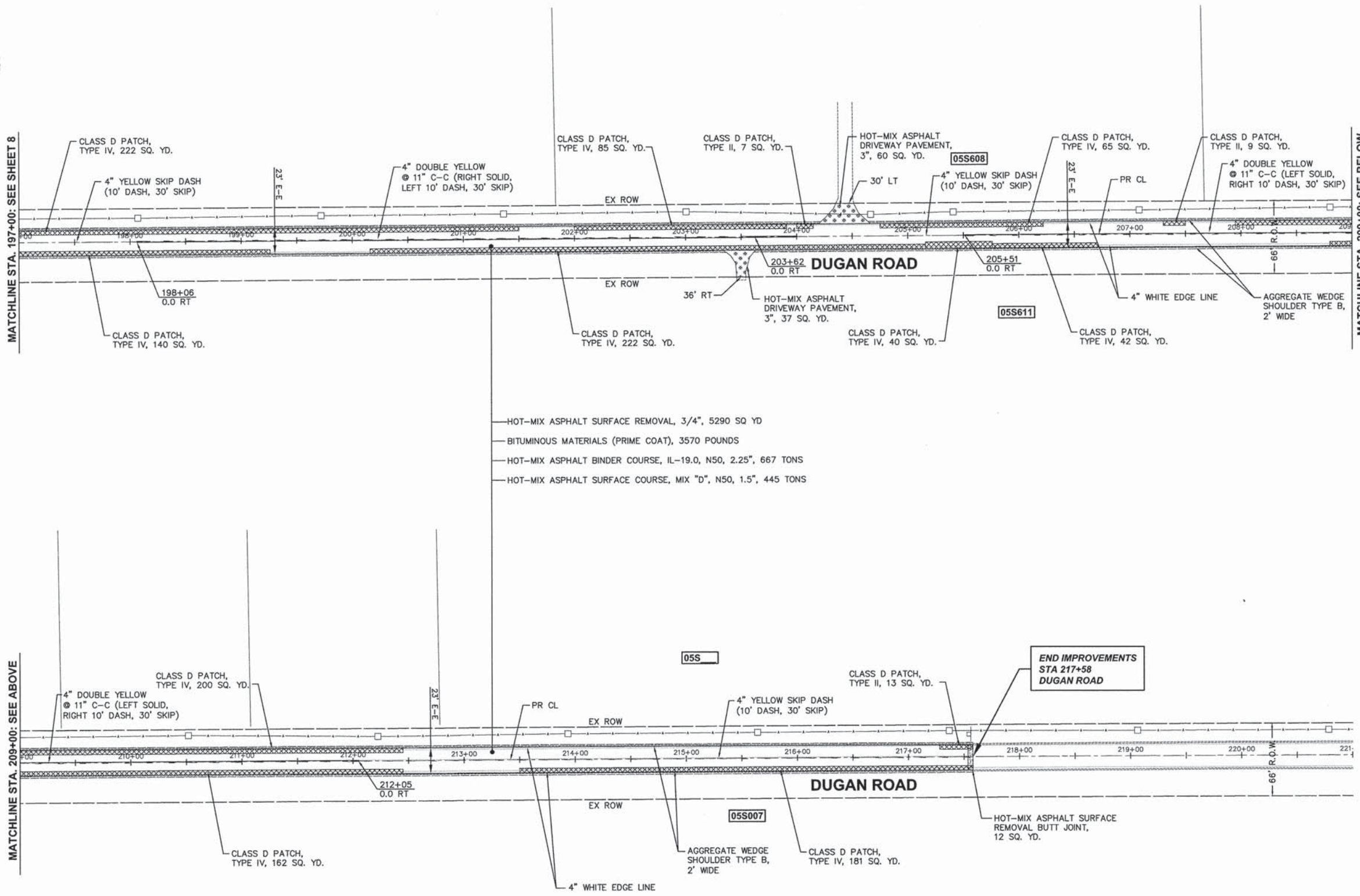
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MATCHLINE STA. 197+00: SEE SHEET 8

MATCHLINE STA. 209+00: SEE ABOVE



- HOT-MIX ASPHALT SURFACE REMOVAL, 3/4", 5290 SQ YD
- BITUMINOUS MATERIALS (PRIME COAT), 3570 POUNDS
- HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2.25", 667 TONS
- HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1.5", 445 TONS

- NOTES:**
1. ALL CLASS D PATCHES ARE 4" IN DEPTH (TYP.).
  2. ALL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC.
  3. RAISED REFLECTIVE PAVEMENT MARKINGS TO BE INSTALLED THROUGHOUT PROJECT LIMITS ACCORDING TO TC-11.

Plotted: February 7, 2014 @ 8:26 AM By: Kira Pung - Tab: 09 Plan 4 22x34

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	DATE -	REVISED -

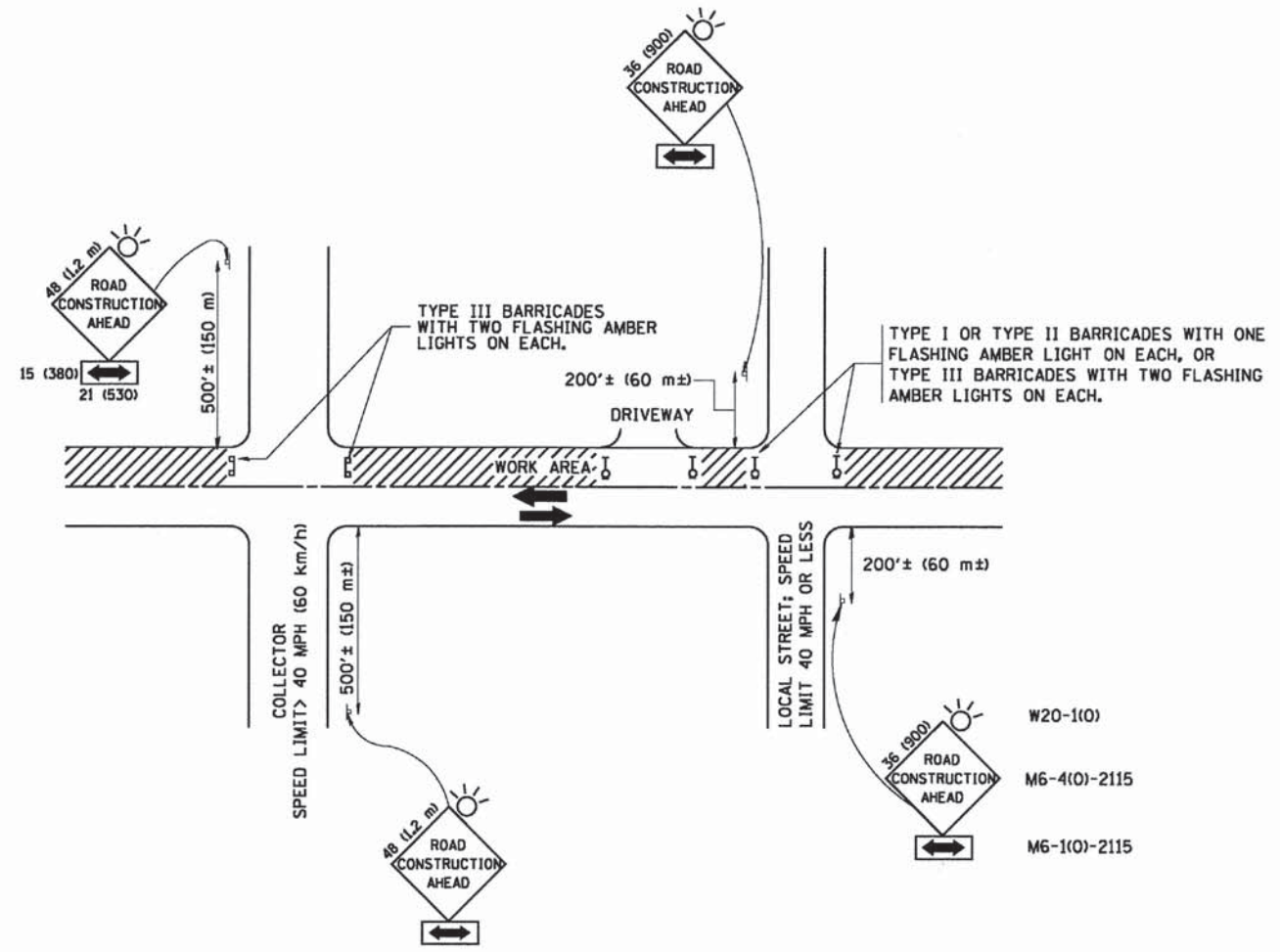
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DUGAN ROAD</b>	
<b>STA 197+00 TO STA 221+00</b>	
SCALE: N.T.S.	SHEET NO. 4 OF 4 SHEETS
STA. 197+00	TO STA. 221+00

F.A.U. RTE. 2312	SECTION 13-00027-00-RS	COUNTY KANE	TOTAL SHEETS 14	SHEET NO. 9
FED. ROAD DIST. NO. 1   ILLINOIS			CONTRACT NO. 61A17	
FED. AID PROJECT - STP				

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TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

Plotted: January 20, 2014 @ 9:28 AM By: Kris Pung - Tab: 10 TC-10 22-34

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PLOT DATE = 1/4/2008	DATE = 06-89

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DRAWN -	REVISED - A. HOUSEH 03-06-96
CHECKED -	REVISED - A. HOUSEH 10-15-96
DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

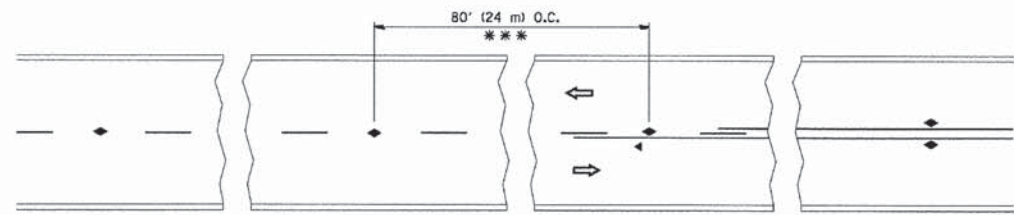
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS		
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2312	13-00027-00-RS	KANE	14	10
TC-10			CONTRACT NO. 61A17	
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT - STP				

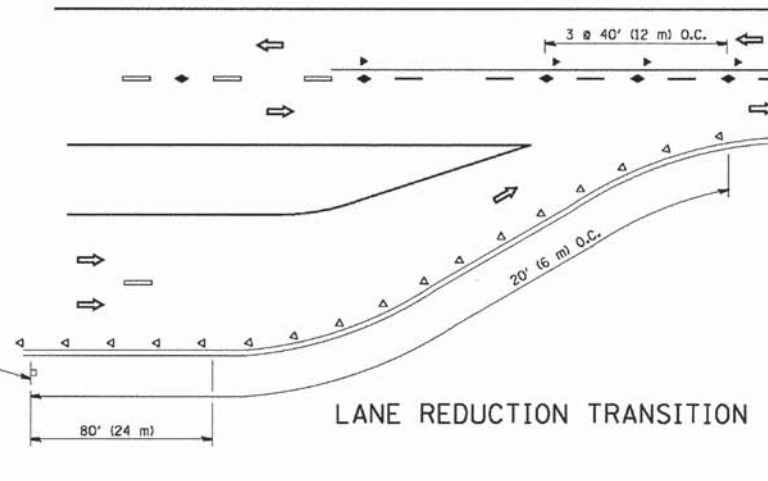
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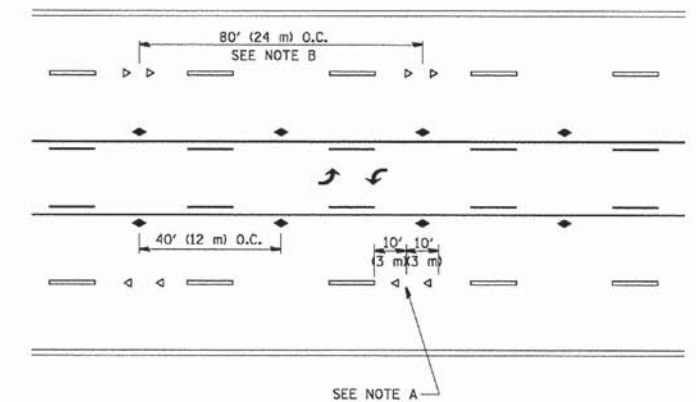


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

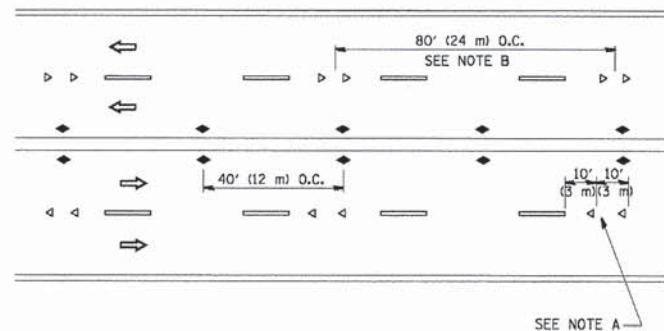
TWO-LANE/TWO-WAY



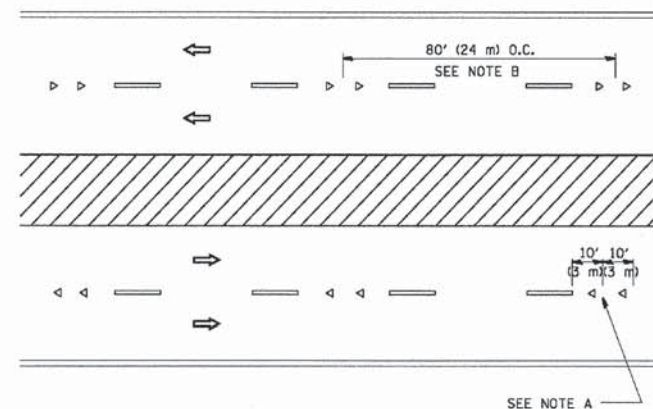
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

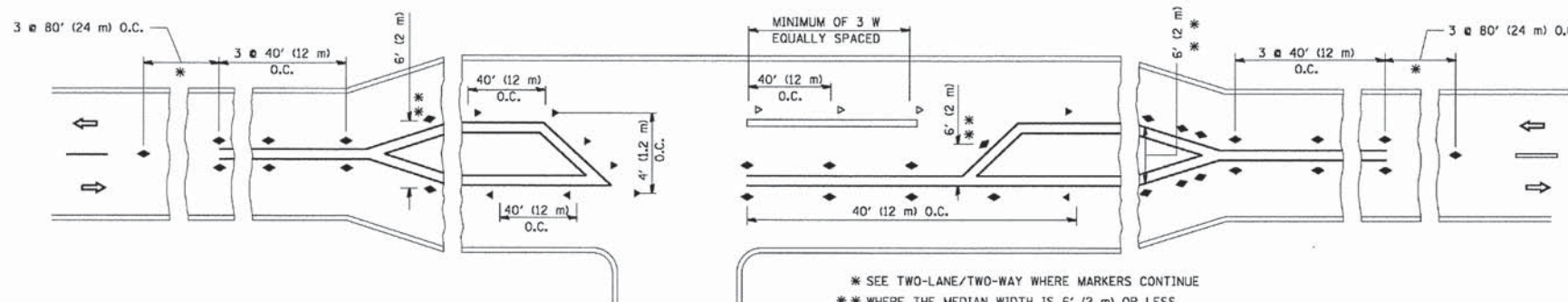
- YELLOW STRIPE
- WHITE STRIPE
- ◄ ONE-WAY AMBER MARKER
- ◄ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (16 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

Plotted: January 20, 2014 @ 9:28 AM By: Kris Pung - Tab: 11 TC-11 22-34

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 PLOT DATE = 3/2/2011

DESIGNED -  
 DRAWN -  
 CHECKED -  
 DATE -

REVISED - T. RAMMACHER 09-19-94  
 REVISED - T. RAMMACHER 03-12-99  
 REVISED - T. RAMMACHER 01-06-00  
 REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

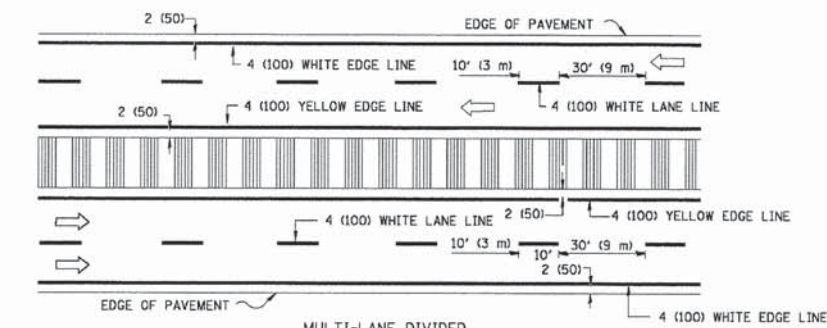
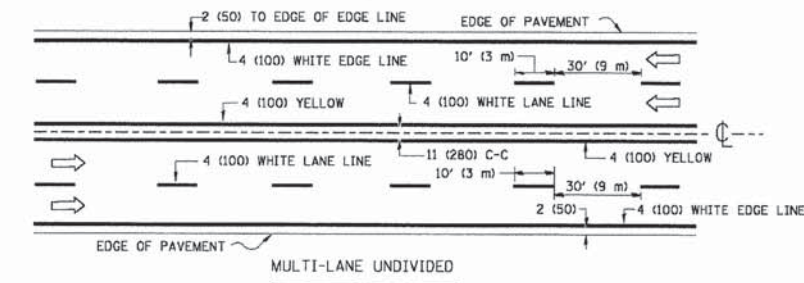
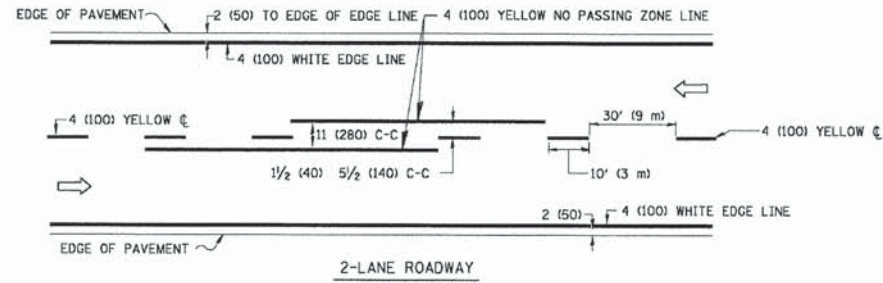
TYPICAL APPLICATIONS  
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2312	13-00027-00-RS	KANE	14	11
<b>TC-11</b>			<b>CONTRACT NO. 61A17</b>	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP				

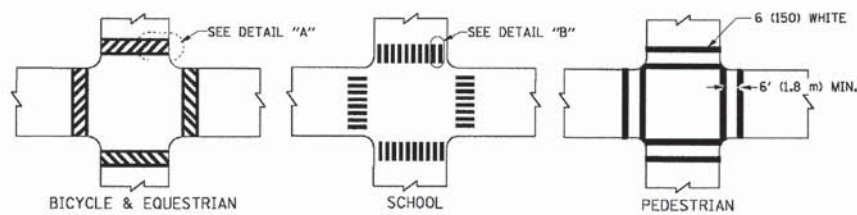
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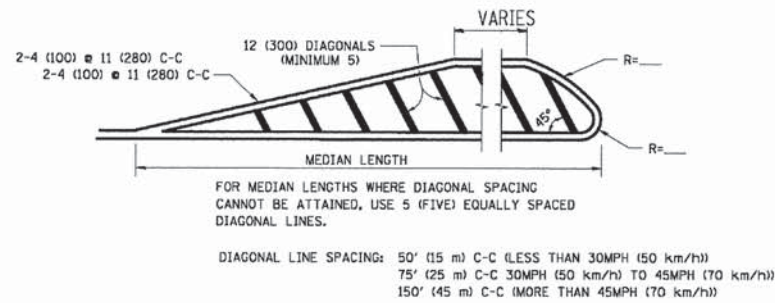
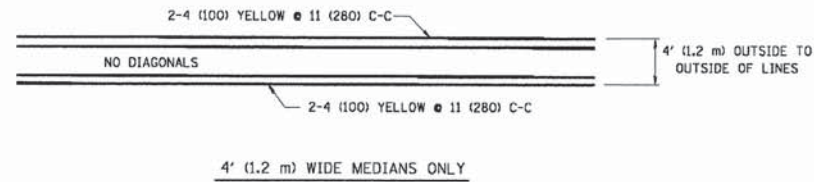


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

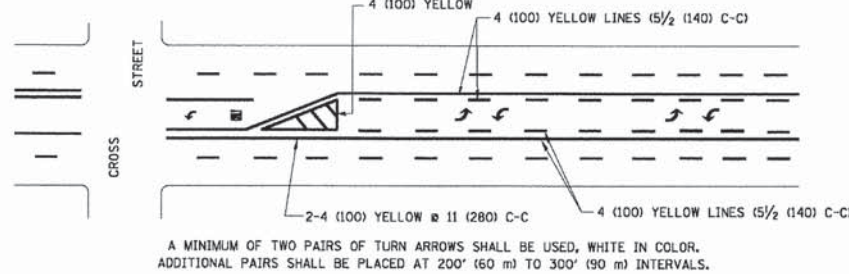
### TYPICAL LANE AND EDGE LINE MARKING



### TYPICAL CROSSWALK MARKING

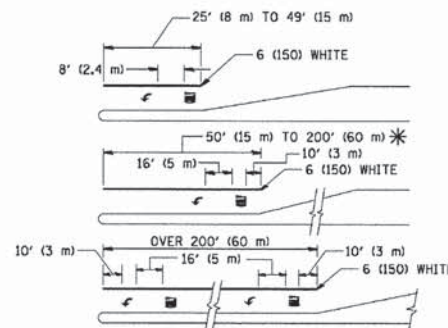


### MEDIANS OVER 4' (1.2 m) WIDE



### MEDIAN WITH TWO-WAY LEFT TURN LANE

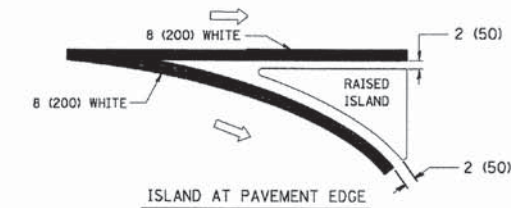
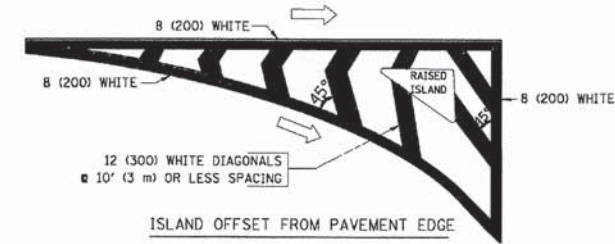
### TYPICAL PAINTED MEDIAN MARKING



\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

### TYPICAL LEFT (OR RIGHT) TURN LANE

### TYPICAL TURN LANE MARKING



### TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2' (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2' (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

Plotted: January 20, 2014 @ 9:29 AM By: Kris Pung - Tab: 12 TC-13 22x34

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## STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

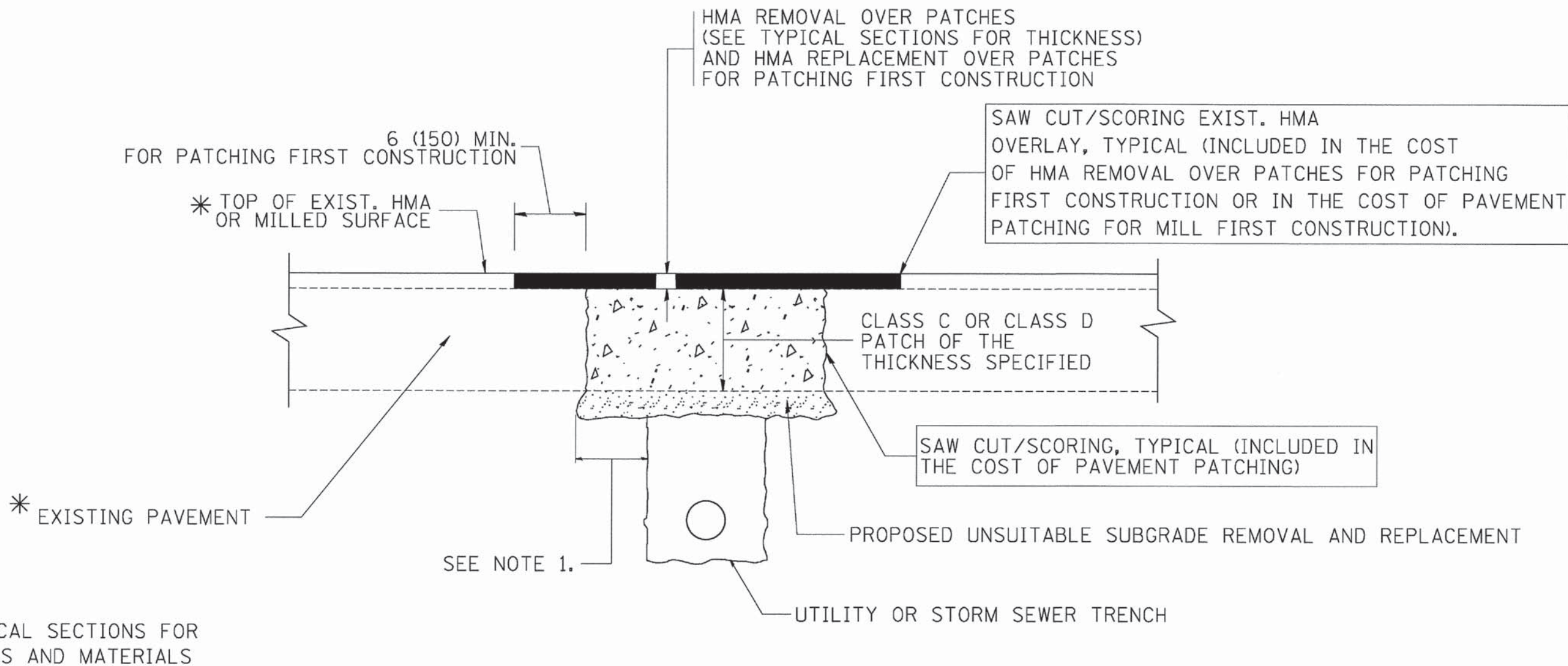
### DISTRICT ONE TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2312	13-00027-00-RS	KANE	14	12
TC-13			CONTRACT NO. 61A17	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP				

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\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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FILE NAME = c:\projects\dist22x34\bd22.dgn	USER NAME = beverd1	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98
PLOT SCALE = 50.000' / IN.	CHECKED -	DRAWN -	REVISED - R. BORO 01-01-07
PLOT DATE = 10/27/2008	DATE - 10-25-94		REVISED - R. BORO 09-04-07
			REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

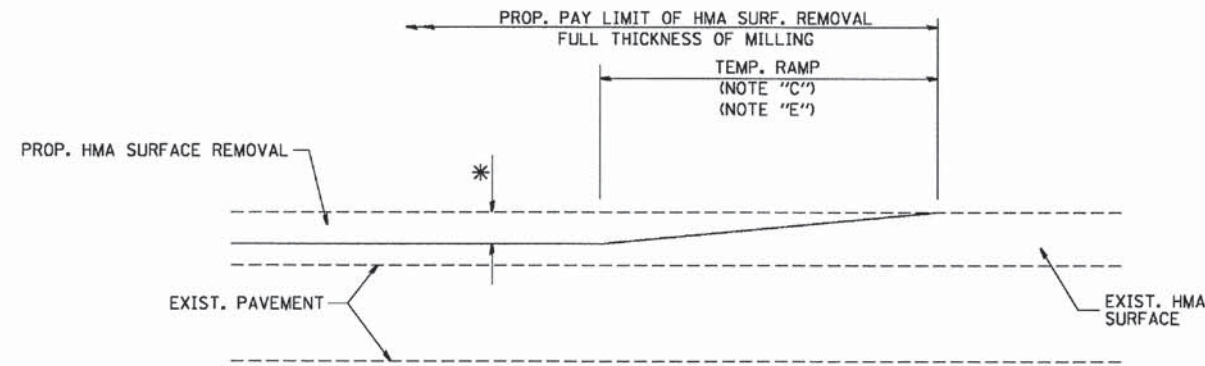
<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2312	13-00027-00-RS	KANE	14	13
BD400-04 (BD-22)			CONTRACT NO. 61A17	
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT - STP				

Plotted: January 20, 2014 @ 9:32 AM By: Kris Pung - Tab: 13 BD-22 22x34

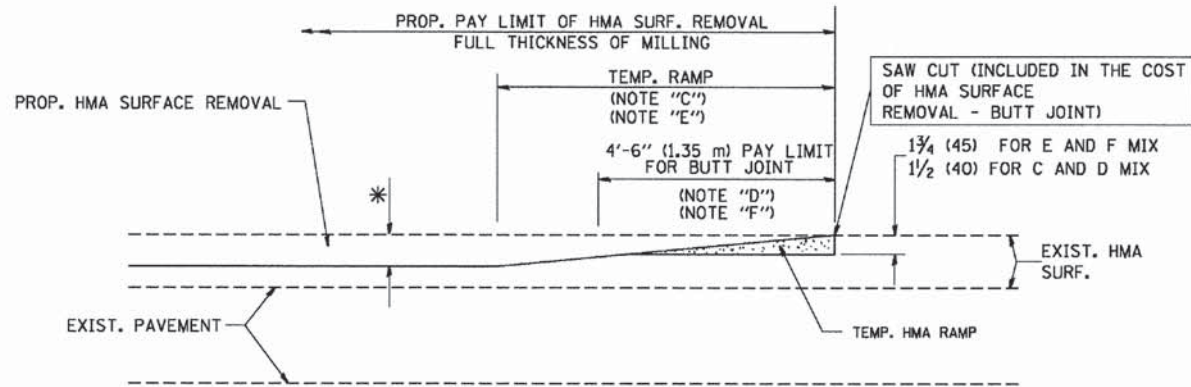
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MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

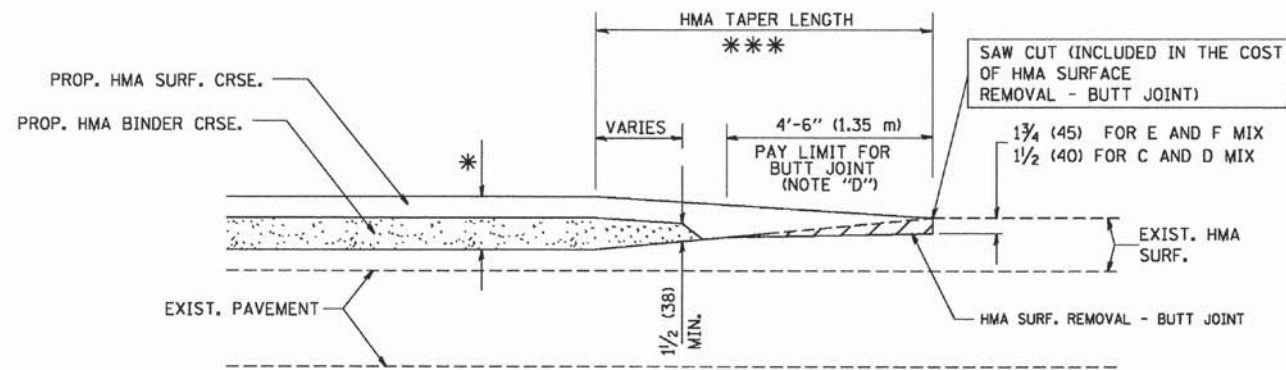
**OPTION 1**



HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

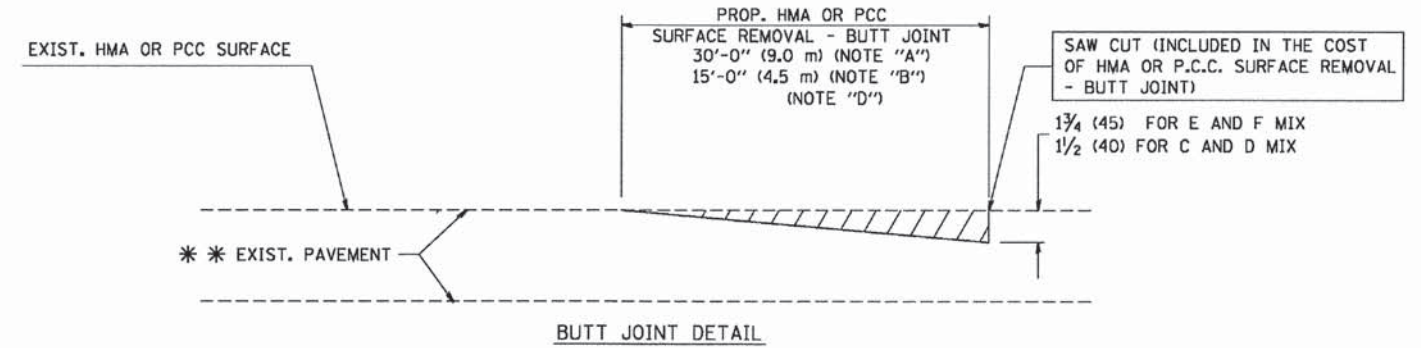
**OPTION 2**

**TYPICAL TEMPORARY RAMP**

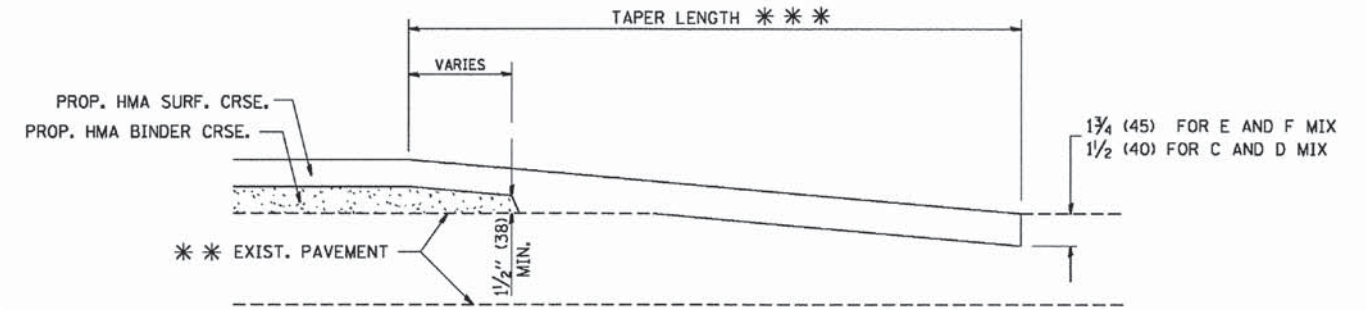


BUTT JOINT AND  
HMA TAPER

**TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



**TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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PLOT SCALE = 50.0000' / IN.  
PLOT DATE = 1/4/2009

DESIGNED - M. DE YONG  
DRAWN -  
CHECKED -  
DATE - 06-13-90

REVISED - R. SHAH 10-25-94  
REVISED - A. ABBAS 03-21-97  
REVISED - M. GOMEZ 04-06-01  
REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND  
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2312	13-00027-00-RS	KANE	14	14
BD400-05 BD32			CONTRACT NO.	61A17
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT - STP				

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