FEDERAL AID PROGRAM ENGINEER: CARMEN E. RAMOS, P.E., SCHAUMBURG, IL

FOR INDEX OF SHEETS, SEE SHEET NO. 2

04-25-2025 LETTING ITEM 016

FOR UNDERGROUND UTILITY LOCATIONS CALL
J.U.L.I.E.
CALL 811
Call Before You Dig | It's Smart. It's Free. It's The Law.
OR TOLL FREE (800) 892-0123
illinois1call.com

FUNCTIONAL CLASSIFICATION
MINOR COLLECTOR
2020 ADT = 1250
POSTED SPEED LIMIT: 35 MPH

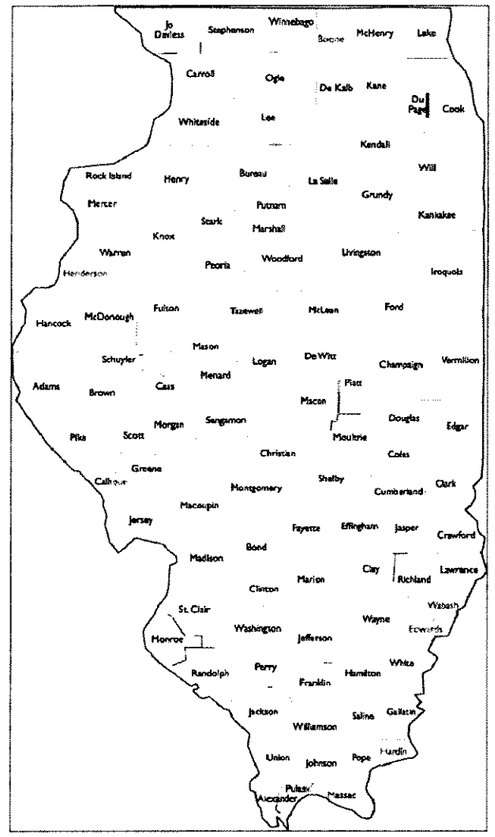
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

MUN ROUTE 1020 (FAIRFIELD WAY)
BLOOMINGDALE ROAD TO LAKE STREET
RESURFACING
SECTION NO. 20-00073-00-RS
PROJECT NO. KV4P(458)
VILLAGE OF BLOOMINGDALE
DUPAGE COUNTY
JOB NO. C-91-086-25

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1020	20-00073-00-RS	DUPAGE	16	1
	ILLINOIS	CONTRACT NO. 61L29		

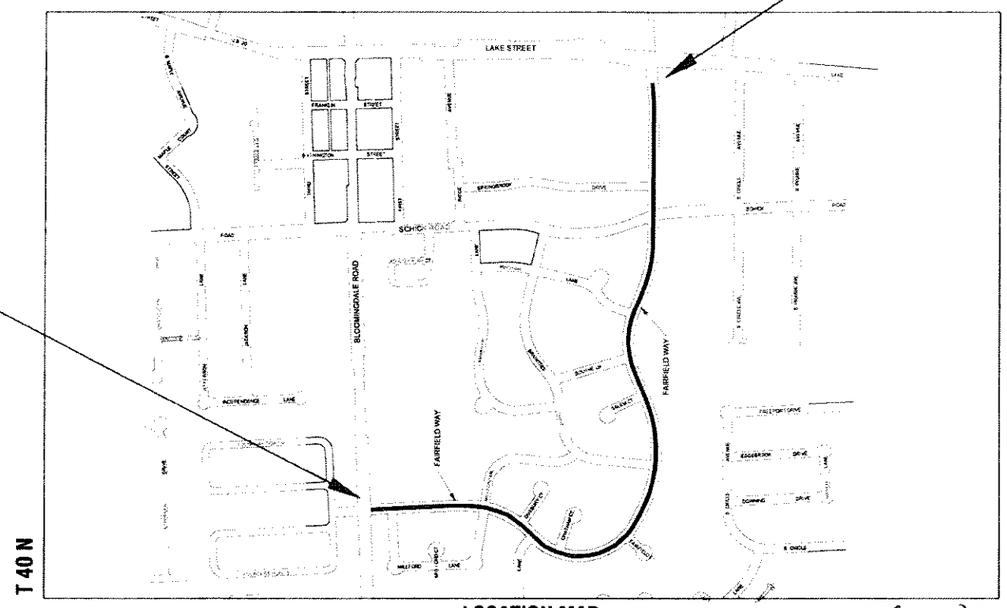
LOCATION OF SECTION



END IMPROVEMENTS
STA. 155+33

R 10 E of 3RD PM

BEGIN IMPROVEMENTS
STA. 100+00



LOCATION MAP
N.T.S.

N

0 50' 100' 1"=50'

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZE PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED SIZE PLANS, THE ABOVE SCALE MAY BE USED.

GROSS AND NET LENGTH = 5533 FT = 1.05 MILES

PROFESSIONAL ENGINEER'S SIGN & SEAL

Exp 11/25

BRIAN P. SISCO
62-57624
REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROVED 1/30 20 25
VILLAGE OF BLOOMINGDALE

PASSED FEB 13 20 25
DISTRICT ONE ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW Feb, 13th 20 25 IR
REGIONAL ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

PREPARED BY: VILLAGE OF BLOOMINGDALE

CONTRACT NO. 61L29

GENERAL NOTES

- ALL REFERENCES TO THE "VILLAGE" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN THE VILLAGE OF BLOOMINGDALE
- ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION ON JANUARY 1, 2022.
- PUBLIC OR PRIVATE UTILITIES - THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE VILLAGE AND ITS ENGINEER DO NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATION OF SUCH UTILITIES AND EXERCISE CARE DURING CONSTRUCTION OPERATIONS SO AS NOT TO DAMAGE THEM. IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND ARTICLE 105.07 OF THE "STANDARD SPECIFICATIONS" THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISITING UTILITIES SO THAT THEIR FACILITIES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATED WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE "STANDARD SPECIFICATIONS"
- ALL CURB & GUTTER, SIDEWALK, PCC DRIVEWAY AND CLASS D PATCH AREAS ARE TO BE DETERMINED IN THE FIELD BY THE ENGINEER.
- PROPOSED CURB SHALL BE DEPRESSED AT ALL SIDEWALK AND DRIVEWAY LOCATIONS AS DETERMINED BY THE ENGINEER
- RECYCLED RUBBER ADJUSTING RINGS - FOR WORK RELATED TO CATCH BASIN AND INLET INSTALLATION OR ADJUSTMENT, THE CONTRACTOR SHALL UTILIZE RECYCLED RUBBER ADJUSTING RINGS FOR THE TOP 2 INCHES OF ADJUSTMENT.
- CONTRACTOR SHALL PROVIDE CONCRETE WASHOUT FACILTIY THROUGHOUT CONSTRUCTION
- ACCESS TO ABUTTING PROPERTY - THE CONTRACTOR SHALL MAINTAIN ACCESS TO ABUTTING PROPERTY DURING THE CONSTRUCTION OF THIS PROJECT EXCEPT FOR PERIODS OF SHORT DURATION, AS APPROVED BY THE ENGINEER.
- EXISTING SIGNS - THE CONTRACTOR SHALL REMOVE EXISTING SIGNS IN CONFLICT WITH PROPOSED CONSTRUCTION, STORE THEM IN PROTECTED LOCATIONS AND REINSTALL THEM AFTER CONSTRUCTION AT THE DIRECTION OF THE ENGINEER AND AT NO ADDITIONAL COST TO THE CONTRACT. DAMAGE TO EXISTING SIGNS SHALL BE REPAIRED.
- SAWING ASPHALT OR CONCRETE FOR REMOVAL ITEMS - THE WORK SHALL CONSIST OF SAWING JOINTS IN THE EXISTING ROADWAY, HMA SURFACE, DRIVEWAY PAVEMENT, CURB AND GUTTER AND SIDEWALK IN ORDER TO SEPARATE THOSE PORTIONS TO BE REMOVED FROM THOSE WHICH WILL REMAIN IN PLACE. THIS WORK SHALL BE PERFORMED AT THE LOCATIONS SPECIFIED ON THE PLANS AND/OR AS OTHERWISE DESIGNATED BY THE ENGINEER. IN AREAS OF FULL DEPTH REMOVAL, THE SAW CUTS SHALL ALSO BE FULL DEPTH.
  THE CONTRACTOR WILL BE REQUIRED TO SAW VERTICAL CUTS SO AS TO FORM CLEAN VERTICAL JOINTS. SHOULD THE CONTRACTOR DEFACE ANY EDGE, A NEW SAWED JOINT SHALL BE PROVIDED.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE THICKNESS OF THE EXISTING PAVEMENT AND WHETHER OR NOT IT CONTAINS REINFORCEMENT.
- RESPONSIBILITY FOR VANDALISM - THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE DEFACEMENT OF ANY CONCRETE POURS BEFORE THEY HAVE SET UP. CONCRETE SIDEWALK, DRIVEWAY PAVEMENT OR CURB AND GUTTER THAT HAS BEEN DEFACED, IN THE OPINION OF THE ENGINEER, SHALL BE REPAIRED OR REMOVED AND REPLACED BY THE CONTRACTOR.
- THE CONTRACTOR'S ATTENTION IS CALLED TO THE FACT THAT THE PRESERVATION OF EXISTING TREES IS OF UTMOST IMPORTANCE TO THE VILLAGE OF BLOOMINGDALE. THE CONTRACTOR SHALL TAKE EXTRA CARE IN GRADING AND EXCAVATING NEAR TREES WHICH ARE NOT MARKED FOR REMOVAL SO AS NOT TO CAUSE INJURY TO THE ROOT SYSTEM OR TRUNKS.
- TREE ROOT PRUNING IS TO BE USED ON EXISTING TREES TO PREVENT THE RIPPING UP OF ROOTS WHEN TRENCHING OR EXCAVATION IS WITHIN THE ROOT ZONE OF ADJACENT TREES TO REMAIN. TREE ROOT PRUNING SHALL BE USED AS NECESSARY OR AS DIRECTED BY THE ENGINEER.
- THE GENERAL CONTRACTOR SHALL HIRE AN IDOT PRE-QUALIFIED ELECTRICAL CONTRACTOR TO INSTALL TRAFFIC SIGNAL DETECTOR LOOPS AT THE BLOOMINGDALE RD / FAIRFIELD WAY INTERSECTION.
- THE ELECTRICAL CONTRACTOR IS REQUIRED TO SUBMIT THE DETECTOR LOOP CATALOG CUTS FOR APPROVAL.
- TRAFFIC SIGNAL DETECTOR LOOPS SHALL BE INSTALLED IN THE HMA BINDER COURSE.
- PLEASE CONTACT MS. MARYANNE SIOSON, DUPAGE COUNTY PRINCIPAL ENGINEER AT 630 407-6908 (48 HRS. IN ADVANCE) TO APPROVE THE DETECTOR LOOP LAYOUT PRIOR TO COMMENCEMENT OF THE WORK

INDEX OF SHEETS

HIGHWAY STANDARDS

000001-08	STANDARD SYMBOLS, ABB. & PATTERNS
424001-12	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-06	DIAGONAL CURB RAMPS FOR SIDEWALKS
424016-06	MIDBLOCK CURB RAMPS FOR SIDEWALKS
424021-07	DEPRESSED CORNER FOR SIDEWALKS
604001-05	FRAME AND LIDS TYPE 1
604051-04	FRAME AND GRATE TYPE 11
606001-08	CONCRETE CURB AND COMBINATION CONC. CURB AND GUTTER
701006-05	OFF-ROAD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE - UNDIVIDED
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK CORNER OR CROSSWALK CLOSURE
701901-10	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS

DISTRICT ONE DETAILS

BD-08	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
BD-32	BUTT JOINT AND HMA TAPER DETAILS
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS
TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC-16	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS
TC-22	ARTERIAL ROAD INFORMATION SIGN
TS-07	DISTRICT ONE - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

VILLAGE OF BLOOMINGDALE
201 S. Bloomingdale Road
Bloomingdale, IL 60188
(630) 893-7000

USER NAME =	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE =	CHECKED -	REVISED -
PLOT DATE = 10/23/24	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, HIGHWAY STANDARDS & GENERAL NOTES
FAIRFIELD WAY RESURFACING

SCALE: | SHEET OF SHEETS | STA. | TO STA.

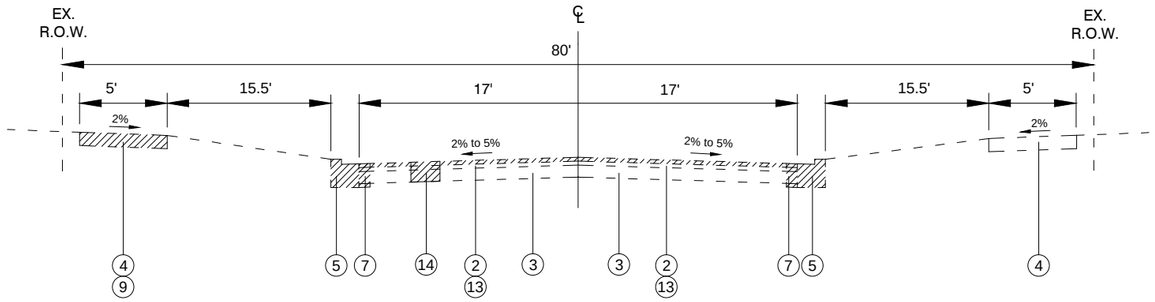
MUN RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1020	20-00073-00-RS	DUPAGE	16	2
		CONTRACT NO. 61L29		
	ILLINOIS	FED. AID PROJECT		

FUNDING SOURCE				STP
				50% FED 50% LOCAL
CONSTRUCTION TYPE CODE				OOO5
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY
20101000	TEMPORARY FENCE	FOOT	250	250
20101200	TREE ROOT PRUNING	EACH	25	25
25200200	SUPPLEMENTAL WATERING	UNIT	100	100
28000510	INLET FILTERS	EACH	45	45
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	15750	15750
40600400	MIXTURE FOR CRACKS, JOINTS AND FLANGEWAYS	TON	10	10
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	855	855
40602978	HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N50	TON	1995	1995
40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	1995	1995
42400800	DETECTABLE WARNINGS	SQ FT	520	520
44000160	HOT-MIX ASPHALT SURFACE REMOVAL - 2.75"	SQ YD	5683	5683
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	1500	1500
44000500	COMBINATION CONCRETE CURB AND GUTTER REMOVAL	FOOT	12000	12000
44000600	SIDEWALK REMOVAL	SQ FT	7400	7400
XX009709	CLASS D PATCHES, TYPE IV, 2.25 INCH	SQ YD	4500	4500
44201709	CLASS D PATCHES, TYPE III, 5 INCH	SQ YD	450	450
44201711	CLASS D PATCHES, TYPE IV, 5 INCH	SQ YD	600	600
60260100	INLETS TO BE ADJUSTED	EACH	8	8
60260300	INLETS TO BE ADJUSTED W/ NEW TYPE 1 FRAME & GRATE	EACH	7	7
60261300	INLETS TO BE ADJUSTED W/ NEW TYPE 11 FRAME & GRATE	EACH	30	30

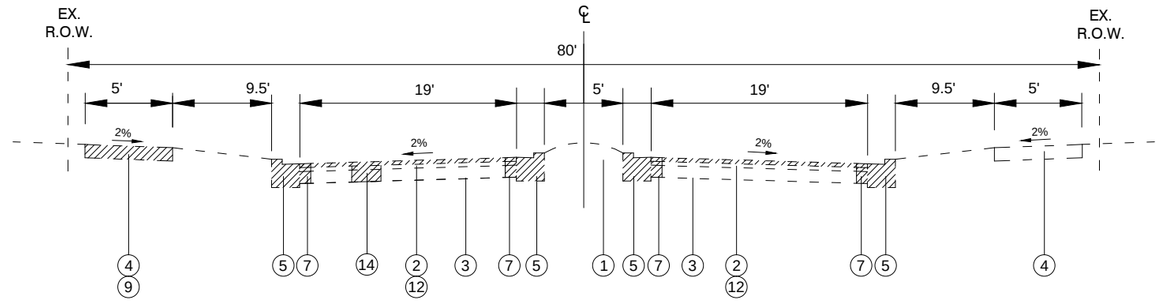
* SPECIALTY ITEM

	FUNDING SOURCE				STP 50% FED 50% LOCAL
	CONSTRUCTION TYPE CODE				OOO5
	67100100	MOBILIZATION	L SUM	1	1
	70102620	TRAFFIC CONTROL & PROTECTION, STANDARD 701501	L SUM	1	1
	70102635	TRAFFIC CONTROL & PROTECTION, STANDARD 701701	L SUM	1	1
	70102640	TRAFFIC CONTROL & PROTECTION, STANDARD 701801	L SUM	1	1
	70107025	CHANGEABLE MESSAGE SIGN	CAL DA	35	35
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	2100	2100
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	700	700
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS & SYMBOLS	SQ FT	125	125
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	3400	3400
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1100	1100
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	180	180
*	X8860105	DETECTOR LOOP REPLACEMENT	FOOT	200	200
	X0326806	WASHOUT BASIN	L SUM	1	1
	X2520700	SODDING (SPECIAL)	SQ YD	3700	3700
	X4230710	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH (SPECIAL)	SQ YD	1500	1500
	X4240430	PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH (SPECIAL)	SQ FT	7400	7400
	X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	17213	17213
	X6030310	FRAMES & LIDS TO BE ADJUSTED (SPECIAL)	EACH	3	3
	X6061700	COMBINATION CONCRETE CURB AND GUTTER, TYPE B (SPECIAL)	FOOT	12000	12000
*	XX008910	PAVEMENT MARKING (SPECIAL)	SQ FT	1600	1600
*	XX009710	PAVEMENT MARKING (SPECIAL 2)	SQ FT	275	275
	X4060280	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"	SQ YD	150	150
	X7200061	TEMPORARY INFORMATION SIGNING	SQ FT	52	52

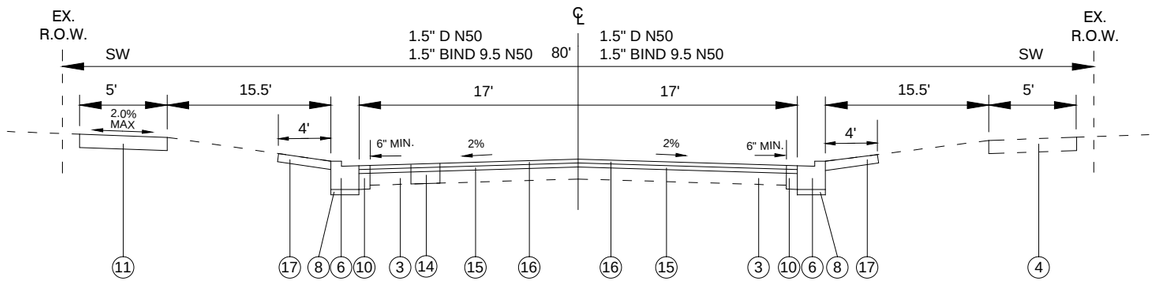
* SPECIALTY ITEM



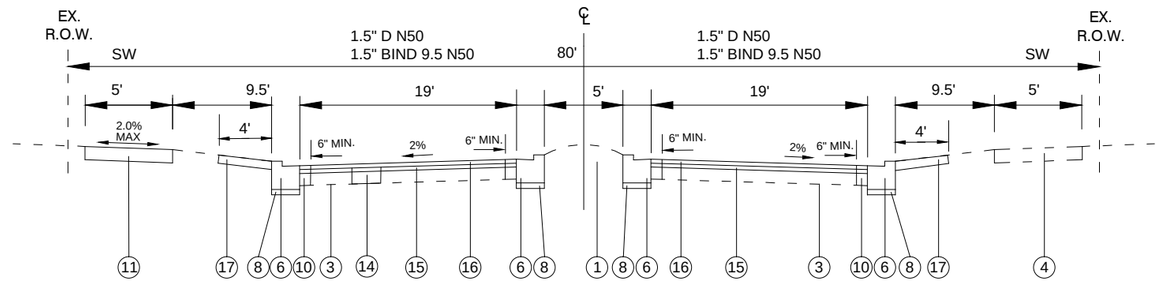
EXISTING TYPICAL SECTION
FAIRFIELD WAY
STA 100+00 TO 145+50



EXISTING TYPICAL SECTION
FAIRFIELD WAY
STA 145+50 TO 155+53



PROPOSED TYPICAL SECTION
FAIRFIELD WAY
STA 100+00 TO 145+50



PROPOSED TYPICAL SECTION
FAIRFIELD WAY
STA 145+50 TO 155+53

LEGEND

1. EXISTING LANDSCAPED MEDIAN
2. EXISTING HMA SURFACE & BINDER, 9" (approx.)
3. EXISTING AGGREGATE BASE, 6" (approx.)
4. EXISTING PCC SIDEWALK
5. EXISTING CONCRETE CURB AND GUTTER, (TYPE M3.12)
6. COMBINATION CONCRETE CURB AND GUTTER, TYPE B (SPECIAL)
7. EXISTING PAVEMENT REMOVAL (INCLUDED IN THE COST OF COMB. CONCRETE CURB AND GUTTER, TYPE B (SPECIAL)
8. SUBBASE GRANULAR MATERIAL, TYPE B, 4" (INCLUDED IN THE COST OF COMB. CONCRETE CURB AND GUTTER, TYPE B (SPECIAL)
9. SIDEWALK REMOVAL
10. PORTLAND CEMENT CONCRETE BASE COURSE (INCLUDED IN THE COST OF COMB. CONCRETE CURB AND GUTTER, TYPE B (SPECIAL)
11. PORTLAND CEMENT CONCRETE SIDEWALK, 5" (SPECIAL)
12. HMA SURFACE REMOVAL - 2.75"
13. HMA SURFACE REMOVAL - VARIABLE DEPTH (2.75" AT EDGE 7" AT CENTER)
14. CLASS D PATCH - 5"
15. HMA BINDER COURSE, IL-9.5, N50 - 1.5"
16. HMA SURFACE COURSE, IL-9.5, MIX "D", N50 - 1.5"
17. SODDING (SPECIAL)

REMOVAL ITEMS

NOTE:
EXISTING PAVING FABRIC MAY BE ENCOUNTERED DURING HMA REMOVAL OPERATIONS. NO ADDITIONAL COMPENSATION WILL BE ALLOWED DUE TO THE PRESENCE OF PAVING FABRIC IN THE MILLED ASPHALT LAYERS.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QMP
MIXTURE TYPE	AIR VOIDS @ Ndes	
PAVEMENT RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50; 1.5"	4% @ 50 Gyr.	LR 1030-2
HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N50; 1.5"	4% @ 50 Gyr.	LR 1030-2
HMA DRIVEWAY PAVEMENT 3"		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50; 3" (IN 2 LIFTS)	4% @ 50 Gyr.	LR 1030-2
PATCHING		
CLASS D PATCH (HMA BINDER, IL-19.0)	4% @ 50 Gyr.	LR 1030-2
QMP DESIGNATION: QUALITY CONTROL / QUALITY ASSURANCE (QC/QA) PER LR 1030-2		

THE UNIT WEIGHT USED TO CALCULATED ALL HOT-MIX SURFACE QUANTITIES IS 112 LBS./SQ.YD./IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA, THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

VILLAGE OF BLOOMINGDALE
201 S. Bloomingdale Road
Bloomingdale, IL 60188
(630) 893-7000

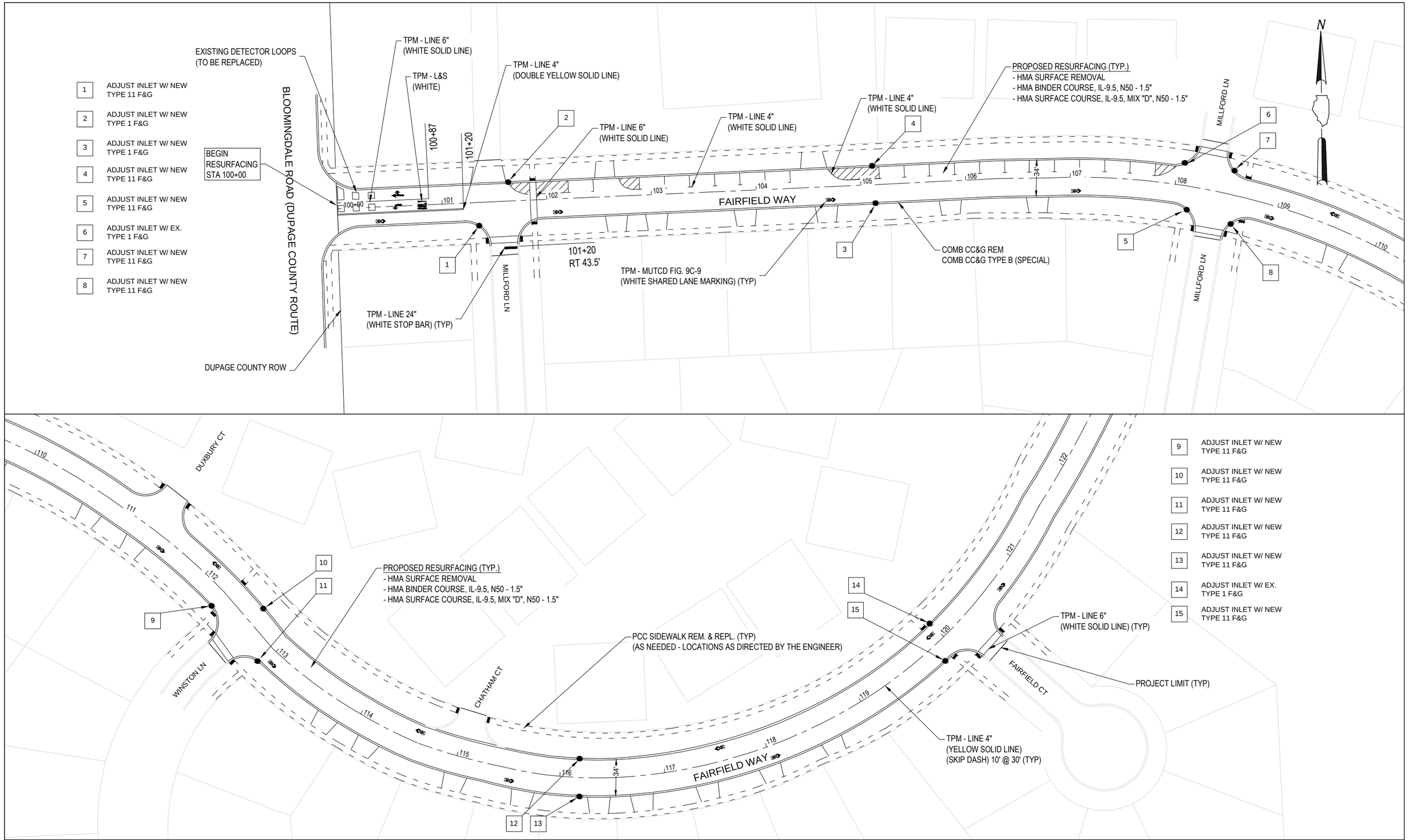
USER NAME =	DESIGNED -	REVISED - 12/6/24
	DRAWN -	REVISED -
PLOT SCALE =	CHECKED -	REVISED -
PLOT DATE = 5/21/24	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAIRFIELD WAY RESURFACING
TYPICAL SECTIONS

SCALE: | SHEET OF SHEETS | STA. TO STA.

MUN RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1020	20-00073-00-RS	DUPAGE	16	5
		CONTRACT NO. 61L29		
	ILLINOIS	FED. AID PROJECT		



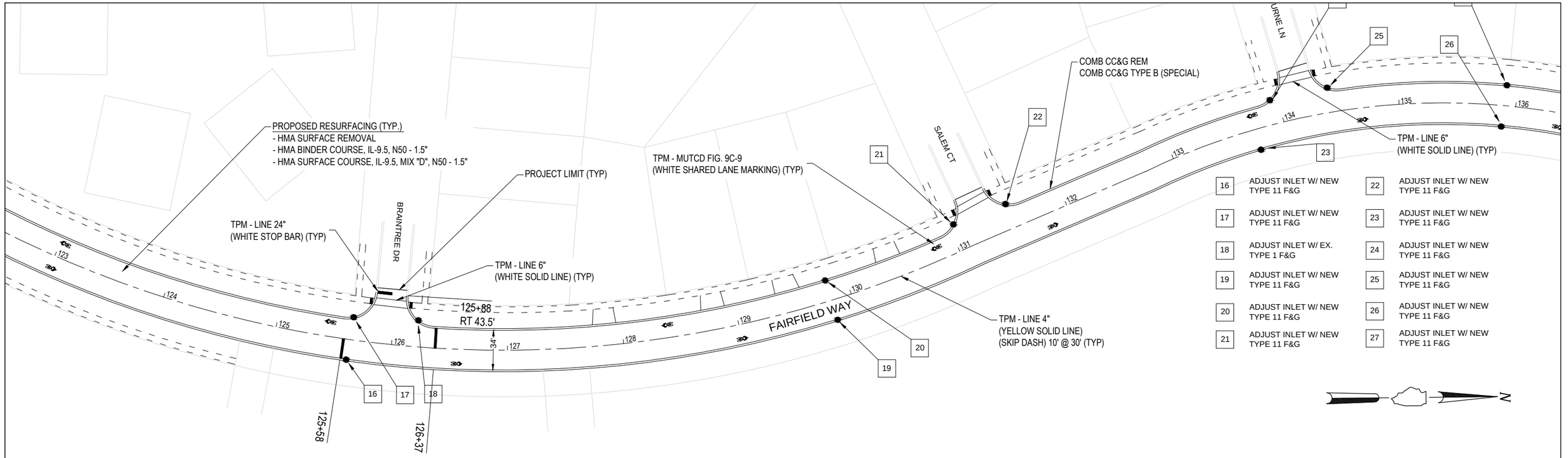
VILLAGE OF BLOOMINGDALE 201 S. Bloomingdale Road Bloomingdale, IL 60188 (630) 893-7000	USER NAME =	DESIGNED -	REVISED - 12/6/24
		DRAWN -	REVISED -
	PLOT SCALE 1" = 40'	CHECKED -	REVISED -
	PLOT DATE = 10/23/24	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAIRFIELD WAY RESURFACING
PROPOSED ROADWAY & PAVEMENT MARKING PLAN

SCALE: 1"=40' | SHEET OF SHEETS | STA. 100+00 TO STA. 122+50

MUN RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1020	20-00073-00-RS	DUPAGE	16	6
		CONTRACT NO. 61L29		
	ILLINOIS	FED. AID PROJECT		



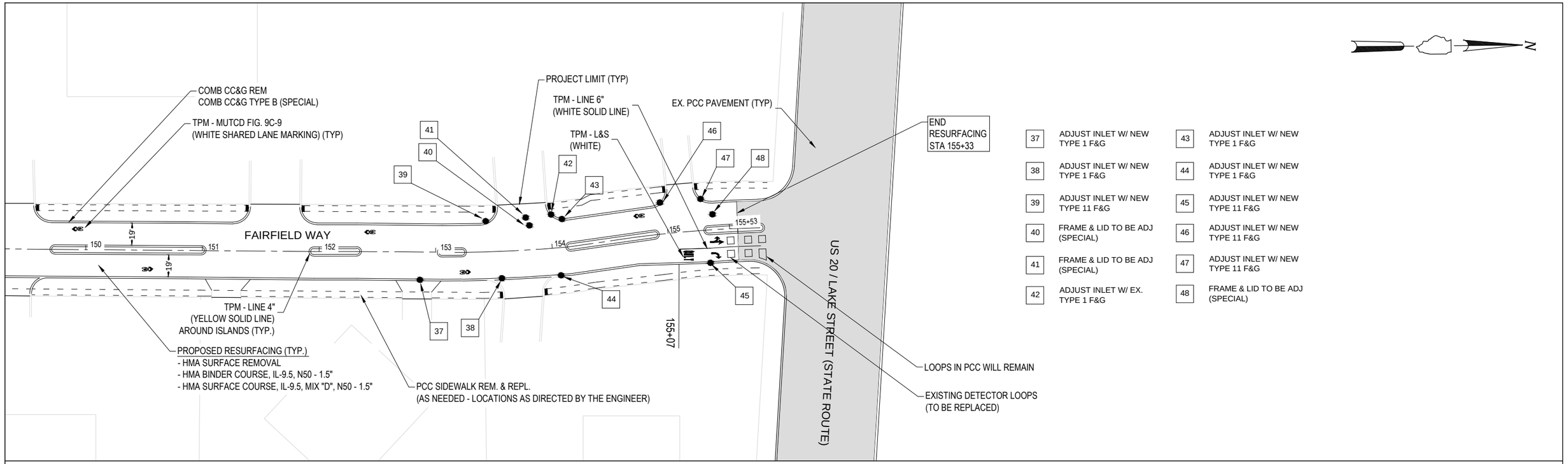
VILLAGE OF BLOOMINGDALE 201 S. Bloomingdale Road Bloomingdale, IL 60188 (630) 893-7000	USER NAME =	DESIGNED -	REVISED - 5/21/24
		DRAWN -	REVISED - 12/6/24
	PLOT SCALE 1" = 40'	CHECKED -	REVISED -
	PLOT DATE = 5/21/24	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

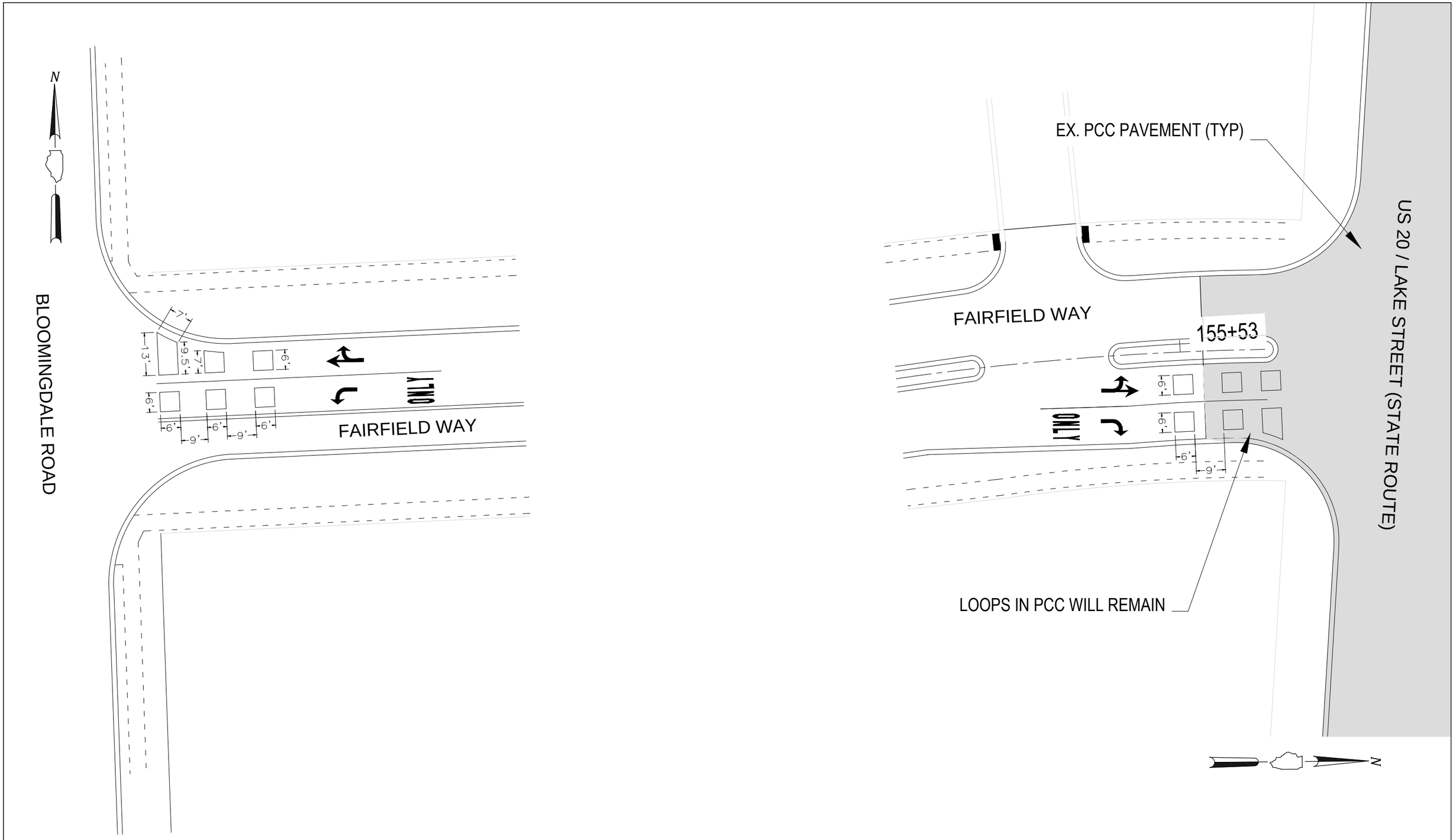
FAIRFIELD WAY RESURFACING
PROPOSED ROADWAY & PAVEMENT MARKING PLAN

SCALE: 1"=40' | SHEET OF SHEETS | STA. 122+50 TO STA. 149+50

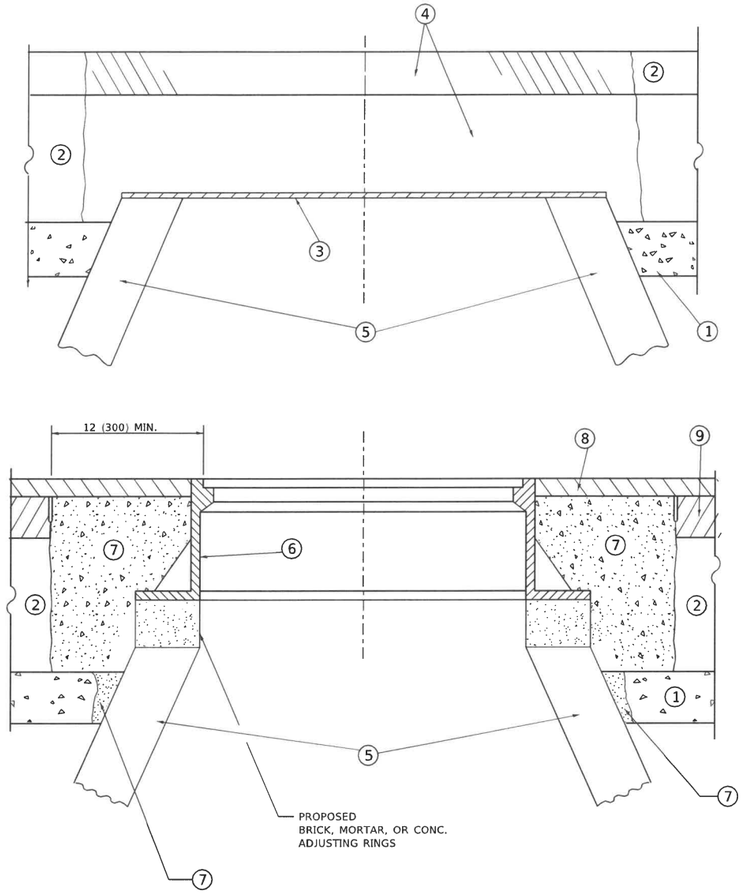
MUN RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1020	20-00073-00-RS	DUPAGE	16	7
		CONTRACT NO. 61L29		
	ILLINOIS	FED. AID PROJECT		



VILLAGE OF BLOOMINGDALE 201 S. Bloomingdale Road Bloomingdale, IL 60188 (630) 893-7000	USER NAME =	DESIGNED -	REVISED - 5/21/24	**STATE OF ILLINOIS** **DEPARTMENT OF TRANSPORTATION**	**FAIRFIELD WAY RESURFACING** **PROPOSED ROADWAY & PAVEMENT MARKING PLAN**			MUN RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - 12/6/24					1020	20-00073-00-RS	DUPAGE	16	8
	PLOT SCALE 1" = 40'	CHECKED -	REVISED -							CONTRACT NO. 61L29		
	PLOT DATE = 5/21/24	DATE -	REVISED -		SCALE: 1"=40'	SHEET OF SHEETS	STA. 149+50 TO STA. 155+33		ILLINOIS	FED. AID PROJECT		



VILLAGE OF BLOOMINGDALE 201 S. Bloomingdale Road Bloomingdale, IL 60188 (630) 893-7000	USER NAME =	DESIGNED -	REVISED - 5/21/24	**STATE OF ILLINOIS** **DEPARTMENT OF TRANSPORTATION**	**FAIRFIELD WAY RESURFACING** **DETECTOR LOOP REPLACEMENT DETAIL**			MUN RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - 12/6/24					1020	20-00073-00-RS	DUPAGE	16	9
	PLOT SCALE 1" = 40'	CHECKED -	REVISED -							CONTRACT NO. 61L29		
	PLOT DATE = 5/21/24	DATE -	REVISED -		SCALE: 1"=40'	SHEET OF SHEETS	STA. 149+50 TO STA. 155+33		ILLINOIS	FED. AID PROJECT		



DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

NOTES

1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
3. CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
4. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
5. THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.

B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.

C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.

D) BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).

STAGE 2 (AFTER PAVEMENT MILLING)

A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.

B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.

C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

1. SUB-BASE GRANULAR MATERIAL
2. EXISTING PAVEMENT
3. 36 (900) DIAMETER METAL PLATE
4. PROPOSED CRUSHED STONE AND HMA SURFACE MIX
5. EXISTING STRUCTURE
6. FRAME AND LID (SEE NOTES)
7. CLASS PP-2* CONCRETE
8. PROPOSED HMA SURFACE COURSE
9. PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

1. REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
2. THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
3. NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
4. WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

MODEL: Default
FILE NAME: W:\dist\std\22x34\bd08.dgn

USER NAME = Lawrence.DeManche	DESIGNED - R. SHAH	REVISED - R. BORO 03-09-11	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
	DRAWN -	REVISED - R. BORO 12-06-11	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - K. SMITH 11-18-22	
PLOT DATE = 9/15/2023	DATE - 10-25-94	REVISED - K. SMITH 09-15-23	

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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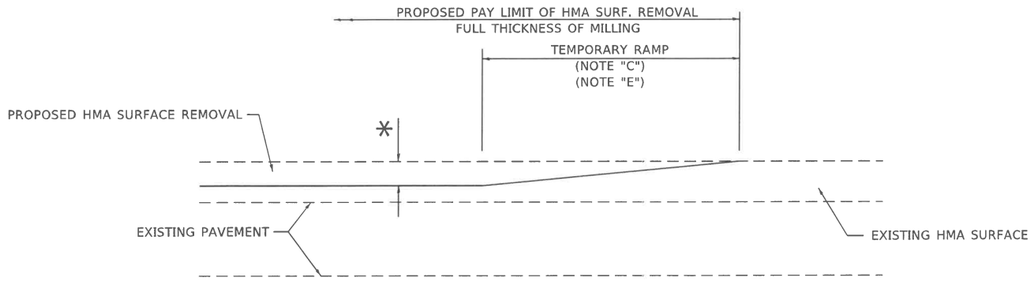
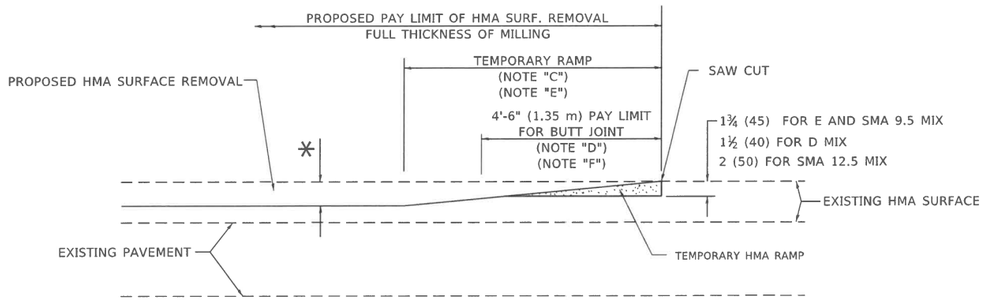
	BD600-03 (BD-08)	CONTRACT NO.		
	ILLINOIS FED. AID PROJECT			



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

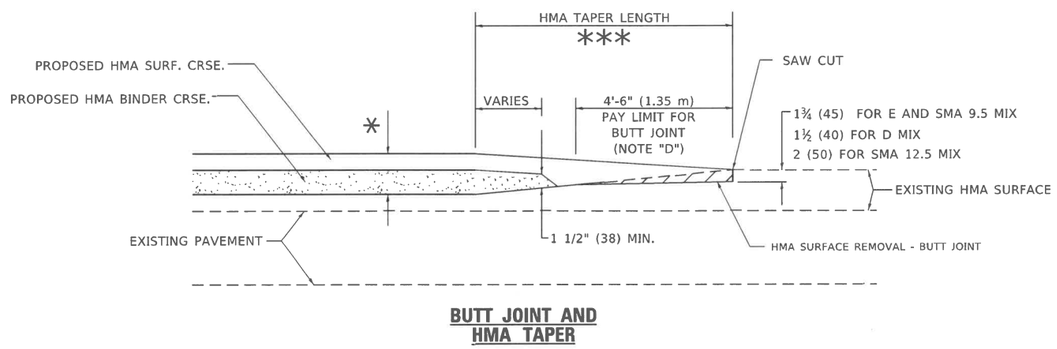


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

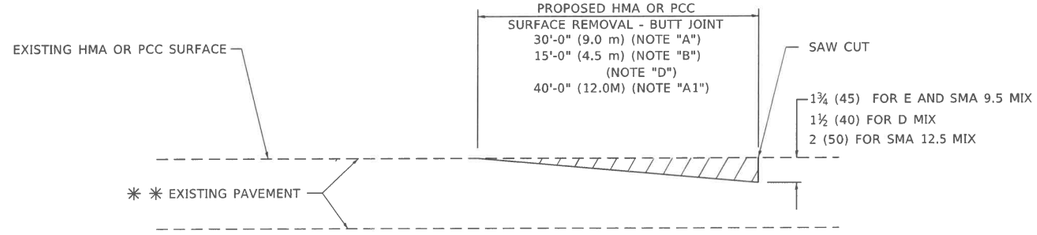
OPTION 2

TYPICAL TEMPORARY RAMP

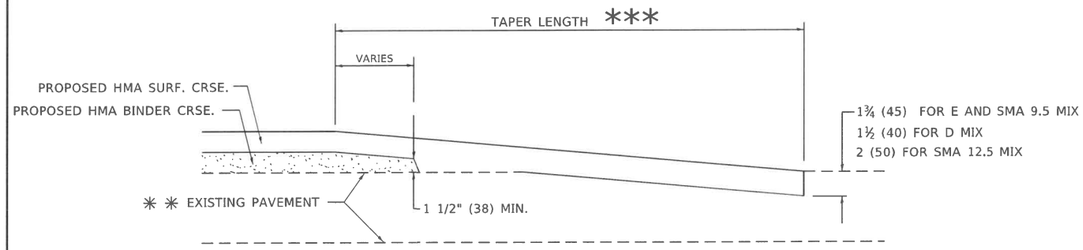


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

GENERAL NOTES

A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.

A1. INTERSTATES

B. MINOR SIDE ROADS.

C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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USER NAME = Lawrence.DeManche	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97	
	DRAWN -	REVISED - M. GOMEZ 04-06-01	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - R. BORO 01-01-07	
PLOT DATE = 11/18/2022	DATE - 06-13-90	REVISED - K. SMITH 11-18-22	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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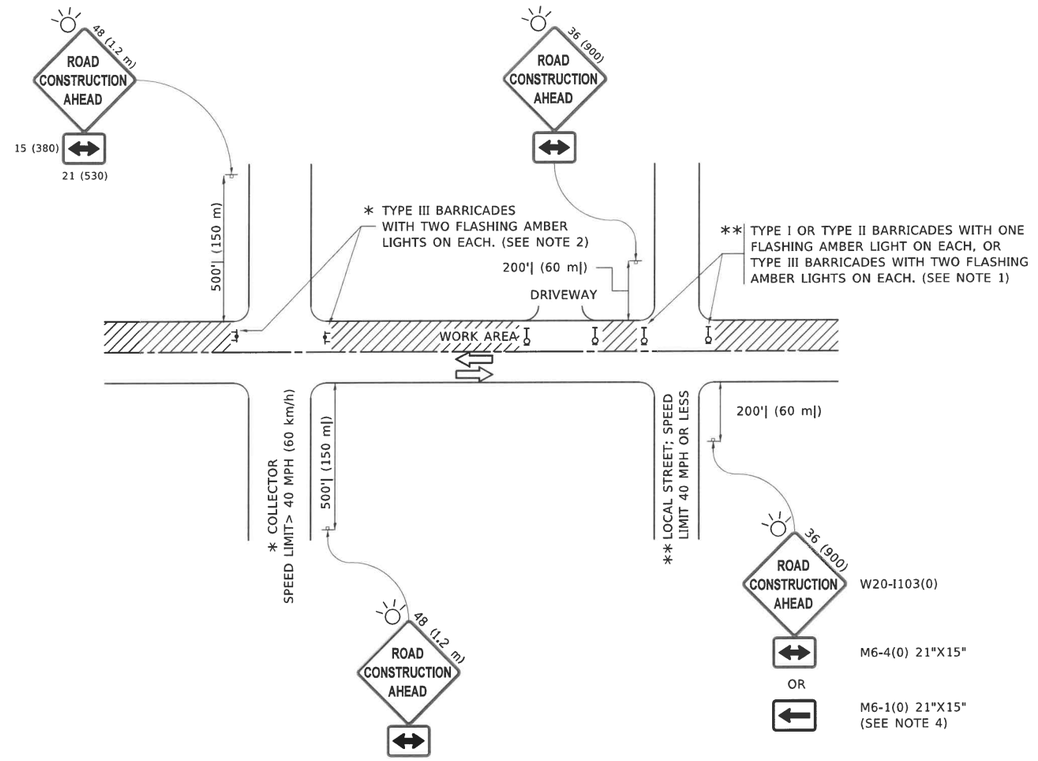
	ILLINOIS	FED. AID PROJECT		



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = footemj	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96	
	DRAWN -	REVISED - T. RAMMACHER 01-06-00	
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13	
PLOT DATE = 3/4/2019	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	ILLINOIS FED. AID PROJECT			

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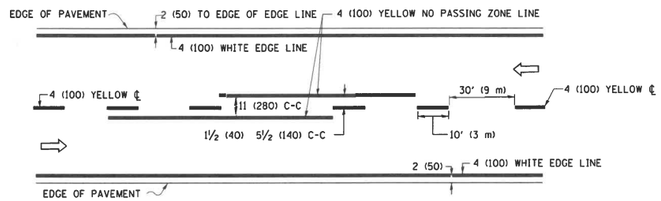
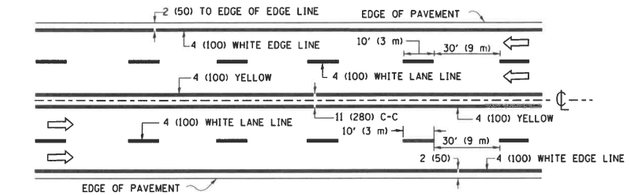
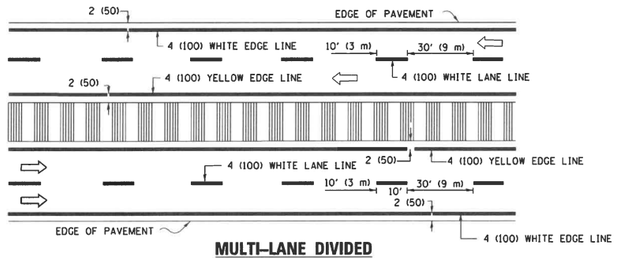


2–LANE ROADWAY

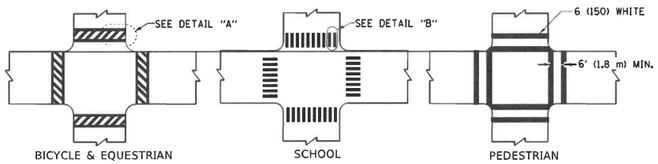


MULTI–LANE UNDIVIDED



MULTI–LANE DIVIDED
WITH MEDIAN

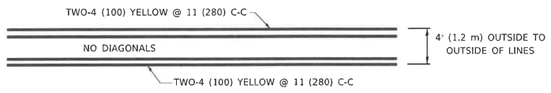
TYPICAL LANE AND EDGE LINE MARKING



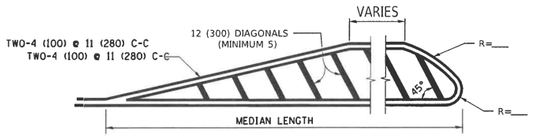
DETAIL "A" **DETAIL "B"**

TYPICAL CROSSWALK MARKING

✱ MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



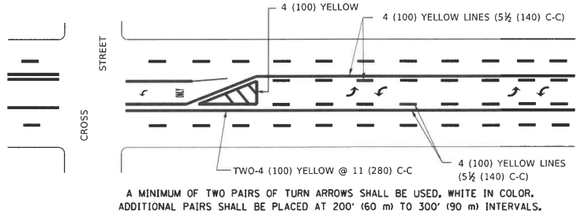
4' (1.2 m) WIDE MEDIANS ONLY



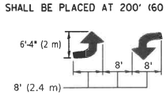
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

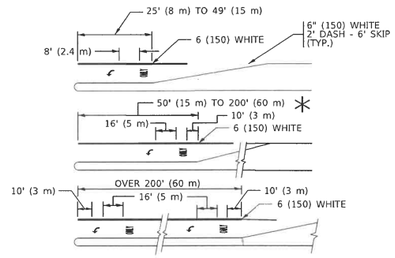


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO–WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

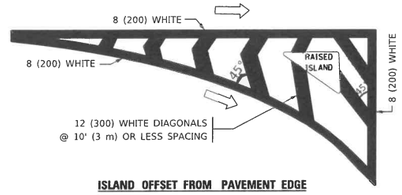


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) AREA = 20.8 SQ. FT. (1.9 m²)

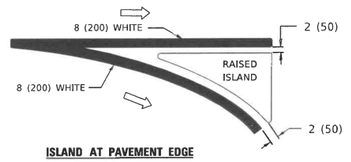
✱ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

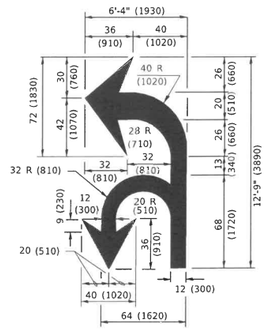


ISLAND OFFSET FROM PAVEMENT EDGE

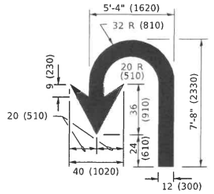


ISLAND AT PAVEMENT EDGE

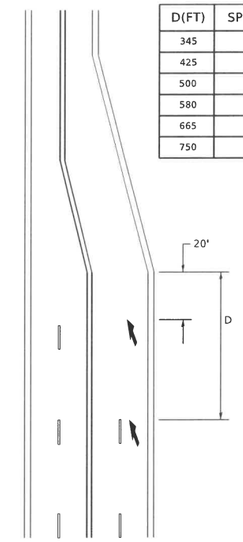
TYPICAL ISLAND MARKING



COMBINATION LEFT AND U–TURN



U–TURN



D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

LANE REDUCTION TRANSITION

✱ LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m² EACH "X"=54.0 SQ. FT. (5.0 m²
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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USER NAME = footemj	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09	**STATE OF ILLINOIS** **DEPARTMENT OF TRANSPORTATION**	**DISTRICT ONE** **TYPICAL PAVEMENT MARKINGS**
	DRAWN -	REVISED - C. JUCIUS 07-01-13		
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15		SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.
PLOT DATE = 3/4/2019	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16		

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1020	20-00073-00-RS	DUPAGE	16	13
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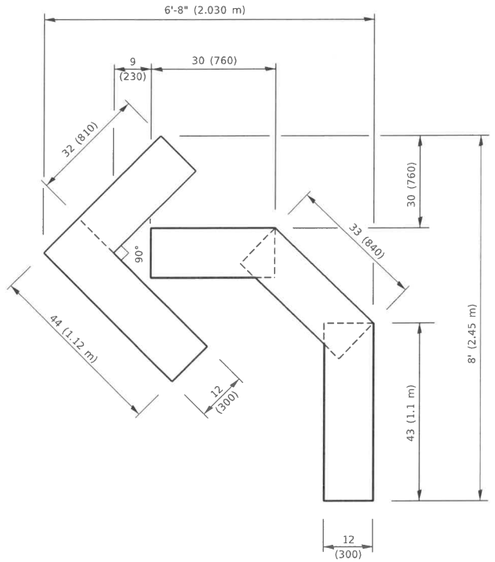
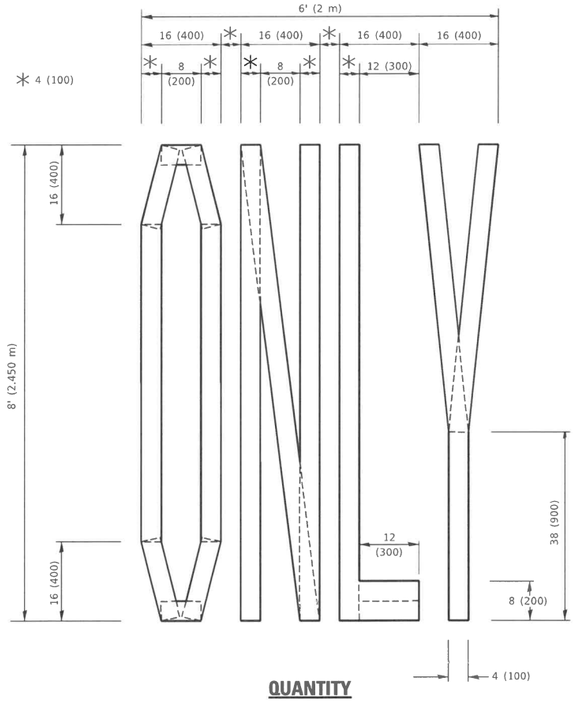


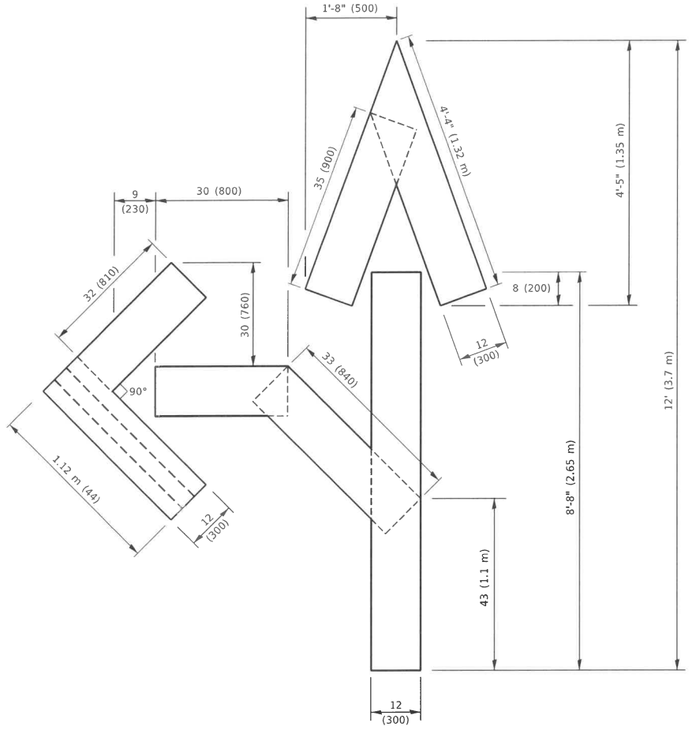
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

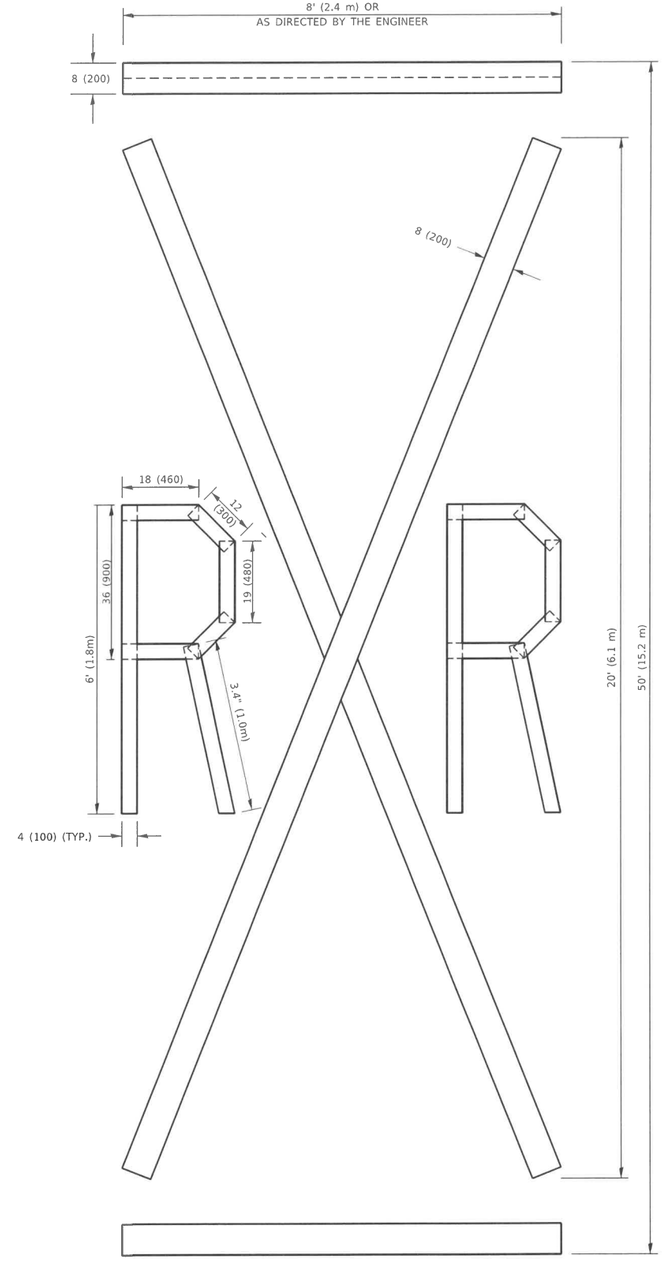


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default
FILE NAME: pw:\\IL084EBIDINTEG.Illinois.gov:PWIDOTDocuments\IDOT Offices\District 1\Projects\Dist\Std 22x34\CADData\CADsheets\tc16.dgn

USER NAME = footemj	DESIGNED -	REVISED - T. RAMMACHER 03-02-98	
	DRAWN -	REVISED - E. GOMEZ 08-28-00	
PLOT SCALE = 50.0068 ' / In.	CHECKED -	REVISED - E. GOMEZ 08-28-00	
PLOT DATE = 3/4/2019	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16	

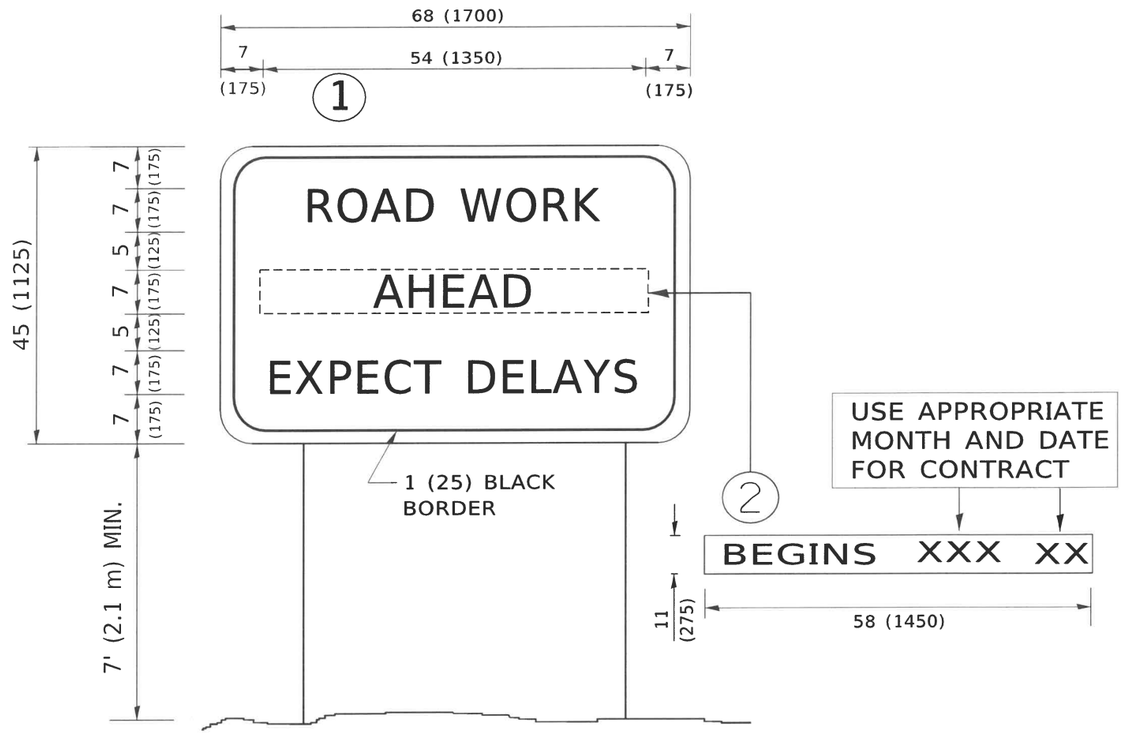
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1020	20-00073-00-RS	DUPAGE	16	14
	TC-16	CONTRACT NO.		
	ILLINOIS	FED. AID PROJECT		

tc16.dgn 3/4/2019 10:37:55 AM User=footemj



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default
FILE NAME: pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\DistStd22x34\CADData\CADsheets\tc22.dgn

USER NAME = footemj	DESIGNED -	REVISED -	R. MIRS 09-15-97
	DRAWN -	REVISED -	R. MIRS 12-11-97
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED -	T. RAMMACHER 02-02-99
PLOT DATE = 3/4/2019	DATE -	REVISED -	C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN

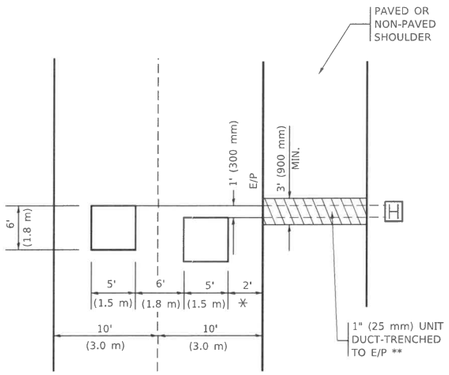
SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1020	20-00073-00-RS	DUPAGE	16	15
	TC-22	CONTRACT NO.		
	ILLINOIS FED. AID PROJECT			

tc22.dgn 3/6/2019 10:48:10 AM User=footemj

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



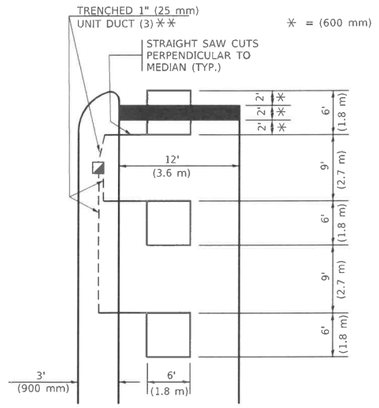
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS

VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

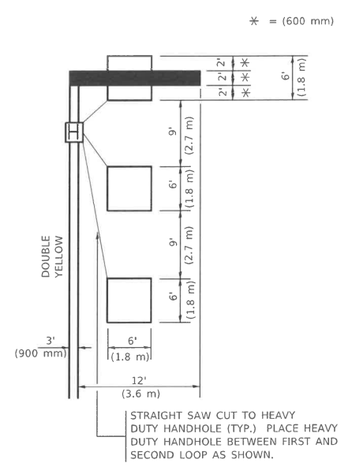


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS

VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING)



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

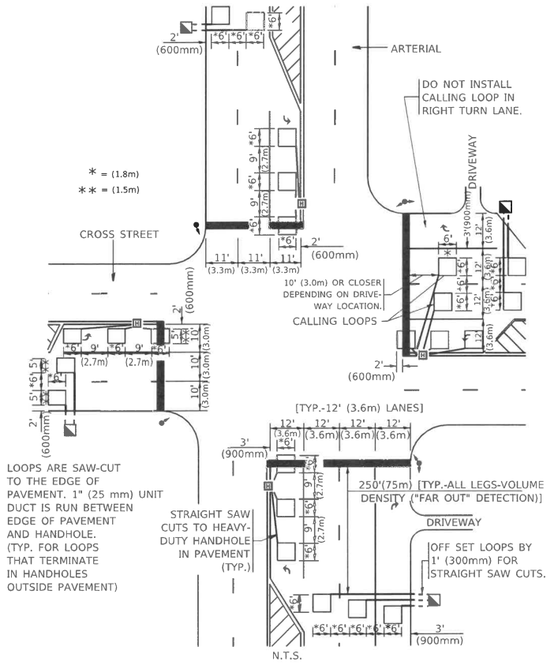
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

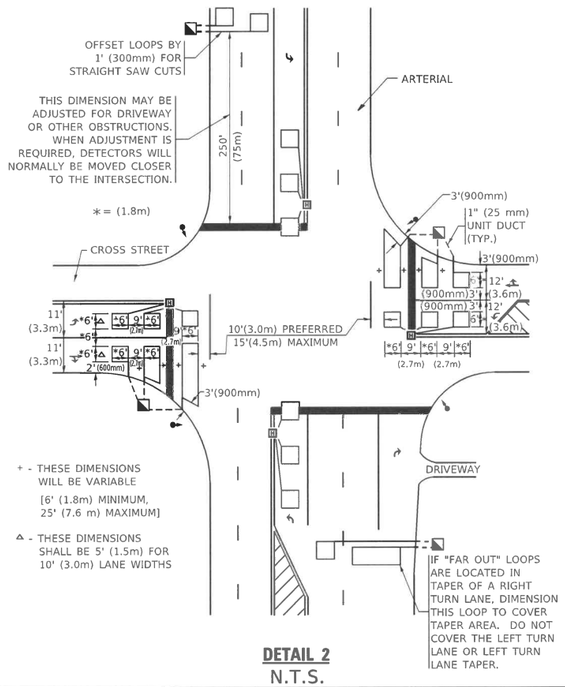
THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

USER NAME = footemj	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
	DRAWN -	REVISED -	
PLOT SCALE = 50.0000 ' / in.	CHECKED - R.K.F.	REVISED -	
PLOT DATE = 3/4/2019	DATE -	REVISED -	

DISTRICT 1 – DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1020	20-00073-00-RS	DUPAGE	16	16
	TS-07	CONTRACT NO.		
	ILLINOIS	FED. AID PROJECT		

MODEL: Default
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sts07.dgn 3/4/2019 12:50:18 PM User=footemj