PROGRAM AND OFFICE ENGINEER CONTACT: CHARLES F. RIDDLE, P.E. 847-705-4406 SCHAUMBURG, IL
CONSULTING ENGINEER: ENGINEERING ENTERPRISES, INC. CONTACT: JAMES R. LENZINI 630-466-6700

04-26-13 LETTING ITEM 041

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	1

CONTRACT NO. 63760

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED
FEDERAL-AID HIGHWAY

CONTINENTAL DRIVE: JOHNSON ELEMENTARY TO KIWANIS PARK SHARED-USE PATH
SECTION: 11-00032-00-BT
PROJECT NUMBER: M-9003(841)
SHARED-USE PATH CONSTRUCTION/SIGNAL MODIFICATION
CITY OF WARRENVILLE
DUPAGE COUNTY
JOB NUMBER: C-91-610-11

INDEX OF SHEETS

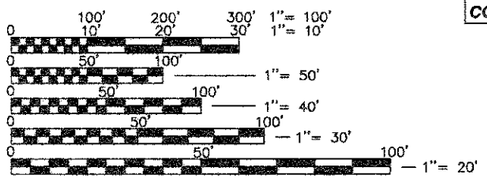
DESIGN DESIGNATION

CONTINENTAL DRIVE: MAJOR COLLECTOR
MEADOW AVENUE: LOCAL ROAD

TRAFFIC:
ADT: 500 (CONTINENTAL DR.)
ADT: 700 (MEADOW AVE.)

SPEED LIMIT:
POSTED SPEED: 25 MPH (CONTINENTAL DR.)
POSTED SPEED: 20 MPH (MEADOW AVE.)

SEE SHEET NO. 2 FOR HIGHWAY STANDARDS

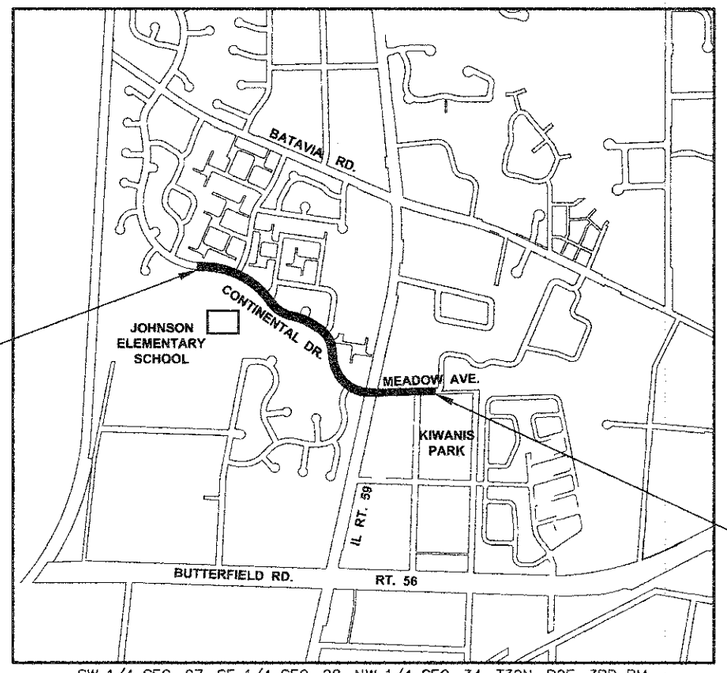


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

JULIE
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
CALL 811

811 Know what's below. Call before you dig.

CONTRACT NO. 63760



PROJECT BEGINS STA. 100+13.07 CONTINENTAL DRIVE

PROJECT ENDS STA. 132+32.00 MEADOW AVENUE

PROJECT LOCATED IN THE CITY OF WARRENVILLE

SW 1/4 SEC. 27, SE 1/4 SEC. 28, NW 1/4 SEC. 34, T39N, R9E, 3RD PM, WINFIELD TOWNSHIP

LOCATION MAP

SCALE: 1" = 1,000'

TOTAL GROSS & NET LENGTH OF PROJECT = 3,219 FEET (0.610 MILES)

LOCATION OF SECTION INDICATED THUS: ▪

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED 10/17/2012
Michael Smith
CITY OF WARRENVILLE, PUBLIC WORKS SUPERINTENDENT

PASSED December 31, 2012
C Holt C Holt
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW January 2, 2013
H. Cates
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

DATE: 10/12/12

BY: James R. Lenzini
JAMES R. LENZINI

LICENSE EXPIRES: NOVEMBER 30, 2013

JAMES R. LENZINI 062-043174 REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS

SEAL

Engineering Enterprises, Inc.
CONSULTING ENGINEERS
52 Wheeler Road
Sugar Grove, Illinois 60554
Phone: (630) 466-6700

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Plotted: October 10, 2012 @ 11:04 AM By: Jim Schmidt - Tab: 01 Cover 22x34
Path: \DS\PROJ\WW1008\DWG\DWG FINAL ENG\WW1008-CVR

GENERAL NOTES

SPECIFICATIONS, STANDARDS, AND SPECIAL PROVISIONS

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION," ADOPTED JANUARY 1, 2012 (HEREINAFTER REFERRED TO AS THE STANDARD SPECIFICATIONS), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS," ADOPTED JANUARY 1, 2013, THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", THE "STANDARD SPECIFICATIONS FOR WATER & SEWER MAIN CONSTRUCTION IN ILLINOIS", SIXTH EDITION, THE CODES AND ORDINANCES OF THE CITY OF WARRENVILLE, ILLINOIS, THE DETAILS IN THE PLANS AND THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS.

NO SUBSTITUTIONS OR VARIANCES WILL BE PERMITTED TO ANY STANDARD NOTES OR ORDINANCES UNLESS APPROVED OTHERWISE IN WRITING PRIOR TO COMMENCING CONSTRUCTION ACTIVITY.

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH ARTICLE 107.14 OF THE STANDARD SPECIFICATIONS.

THE CONTRACTOR SHALL AT ALL TIMES PROVIDE PROTECTION FOR TRAFFIC AS CALLED FOR IN THE APPLICATION OF TRAFFIC CONTROL DEVICES, THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS AND THE PLANS

UTILITIES

THE CONTRACTOR SHALL COOPERATE WITH THE OWNER IF ANY UTILITY IMPROVEMENTS ARE REQUIRED WITHIN THE DURATION OF THE CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL EXISTING AND PROPOSED UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.

THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, FIELD TILES AND ANY OTHER PUBLIC OR PRIVATE UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND NOT NECESSARILY COMPLETE; THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE RESTORED TO A CONDITION EQUAL TO THAT EXISTING BEFORE THE DAMAGE OCCURRED. THIS WORK SHALL BE ARRANGED BY THE UTILITY COMPANY AND SHALL BE AT THE CONTRACTOR'S EXPENSE.

IF EXISTING UTILITY LINES OF ANY NATURE ARE ENCOUNTERED WHICH CONFLICT IN LOCATION WITH NEW CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE ENGINEER SO THAT THE CONFLICT MAY BE RESOLVED.

STAKING

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, THE OWNER'S AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE EDGE OF PAVEMENT, UNLESS OTHERWISE NOTED. CURB AND GUTTER ELEVATIONS SHOWN AT POINTS OF CURVE, ETC., ARE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.

ELEVATIONS ARE NGVD 29 DATUM, ESTABLISHED FROM THE DUPAGE COUNTY GEODETIC SURVEY BENCHMARK NETWORK.

ALL OFFSET LOCATIONS GIVEN ON THE DETAILED PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC., ARE FROM THE BASELINE AS SHOWN ON THE PLANS.

SEWERS AND WATER MAINS

ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, IT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN IN AN OPERATING CONDITION TEMPORARY OUTLETS AND CONNECTIONS FOR ALL DRAINS, SEWERS, AND CATCH BASINS. THE CONTRACTOR SHALL PROVIDE FACILITIES WHICH HAVE THE CAPACITY TO RECEIVE AND DISCHARGE THE STORM WATER FLOW RATES NORMALLY ACCEPTED AND RELEASED BY EXISTING DRAINAGE FACILITIES. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

ALL FRAMES, GRATES, OR LIDS SCHEDULED TO BE REMOVED FROM EXISTING STRUCTURES SHALL REMAIN THE PROPERTY OF THE CITY OR STATE, AS APPLICABLE. ANY ITEMS DAMAGED DURING REMOVAL SHALL BE REPLACED BY THE CONTRACTOR AT THEIR OWN EXPENSE. THE COST OF SALVAGING EXISTING FRAMES, GRATES, OR LIDS AND/OR STOCKPILING THEM ON THE JOB SITE FOR PICKUP BY THE CITY OR STATE OR DELIVERY TO THE CITY OR STATE MAINTENANCE YARD SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

SIGNS

PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR, CITY AND ENGINEER SHALL INVENTORY THE LOCATION, SIZE, TYPE, AND CONDITION OF ALL EXISTING SIGNS. ANY SIGN DAMAGED DURING CONSTRUCTION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

THE CITY SHALL RELOCATE ALL EXISTING SIGN PANEL ASSEMBLIES AS REQUIRED. THE CITY SHALL REPLACE SIGNS AND FURNISH AND INSTALL NEW SIGNS AS INDICATED ON THE PLANS OTHER THAN THE R10-3E PEDESTRIAN TRAFFIC SIGNAL (COUNTDOWN) SIGN TO BE INCLUDED IN THE COST OF THE PUSHBUTTON.

MISCELLANEOUS

THE CONTRACTOR SHALL MAINTAIN EXISTING SIDE STREET, DRIVEWAY AND PEDESTRIAN ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING CONSTRUCTION OF THE PROJECT, UNLESS OTHERWISE NOTED IN THE PLANS OR DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE ITEM "AGGREGATE FOR TEMPORARY ACCESS".

SAWING OF REMOVAL ITEMS AS NOTED ON THE PLANS, SPECIFIED IN THE STANDARD SPECIFICATIONS, OR AS REQUIRED BY THE ENGINEER SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

THE THICKNESS OF ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASES ON WHICH THE ASPHALT MIXTURES ARE TO BE PLACED.

PROTECTIVE COAT SHALL BE APPLIED TO ALL GUTTER FLAGS, FACE AND TOP OF CURB, PCC SIDEWALK, PCC DRIVEWAY PAVEMENT, AND AS DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING FRESH CONCRETE FROM DAMAGE AND VANDALISM. ANY DAMAGED OR VANDALIZED CONCRETE SHALL BE REMOVED AND REPLACED AT THE CONTRACTOR'S EXPENSE.

THE CONTRACTOR SHALL BE REQUIRED TO MAKE ARRANGEMENTS FOR THE PROPER BRACING, SHORING AND OTHER REQUIRED PROTECTION OF ALL ROADWAYS, STRUCTURES, POLES, CABLES AND PIPE LINES, BEFORE CONSTRUCTION BEGINS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO THE STREETS OR ROADWAYS AND ASSOCIATED STRUCTURES AND SHALL MAKE REPAIRS AS NECESSARY TO THE SATISFACTION OF THE ENGINEER AND CITY AT THEIR OWN EXPENSE. ANY SHEETING AND/OR SHORING USED FOR THIS IMPROVEMENT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

THE CONTRACTOR SHALL PROTECT ALL EXISTING FACILITIES (E.G. CURB, DRIVEWAYS, PAVEMENT) THAT ARE NOT INDICATED TO BE REMOVED ON THE PLANS. ANY FACILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE RESTORED TO A CONDITION EQUAL TO THAT EXISTING BEFORE THE DAMAGE OCCURRED AT THE CONTRACTOR'S EXPENSE.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS BEFORE BIDDING ON THIS CONTRACT.

WHERE NEW WORK MEETS EXISTING FEATURES TO REMAIN, THE CONTRACTOR SHALL FIELD CHECK ALL DIMENSIONS AND ELEVATIONS BEFORE PROCEEDING WITH CONSTRUCTION. IMMEDIATELY NOTIFY THE ENGINEER OF ANY DISCREPANCIES.

THE CONTRACTOR WILL BE REQUIRED TO COMPLY WITH ALL STATE REGULATIONS REGARDING AIR, WATER AND NOISE POLLUTION. THE CONTRACTOR IS PROHIBITED FROM BURNING ANY MATERIAL WITHIN OR ADJACENT TO THE IMPROVEMENT.

THE CONTRACTOR SHALL PREPARE THE SUBGRADE IN ACCORDANCE WITH ARTICLE 301.03 OF THE STANDARD SPECIFICATIONS PRIOR TO THE REMOVAL OF ANY UNSTABLE MATERIALS.

ALL DISTURBED AREAS WITHIN THE PROJECT THAT ARE NOT OTHERWISE SURFACED SHALL BE CLEANED, LAYERED WITH TOPSOIL, AND SEEDED AS SHOWN IN THE PLANS. LIMITS SHOWN ON THE PLANS ARE THE MAXIMUM PAY WIDTHS FOR PAYMENT PURPOSES. ADDITIONAL AREAS DAMAGED BY MACHINERY, CONSTRUCTION EQUIPMENT, CONTRACTOR NEGLIGENCE OR OVER-EXCAVATION SHALL BE RESTORED TO A CONDITION EQUAL TO THAT EXISTING BEFORE THE DAMAGE OCCURRED AT THE COST OF THE CONTRACTOR.

THE CONTRACTOR SHALL DISPOSE OF AND REMOVE FROM THE SITE EACH DAY ALL CURB AND GUTTER, PAVEMENT AND ALL OTHER EXCAVATED MATERIAL NOT FOR SALVAGE. THE COST FOR HAULING AND TRUCKING TO DISPOSAL LOCATIONS WILL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

THE ENGINEER AND CITY ARE NOT RESPONSIBLE FOR THE CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OR PROCEDURES, TIME OF PERFORMANCE, PROGRAMS OR FOR ANY SAFETY PRECAUTIONS USED BY THE CONTRACTOR. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR EXECUTION OF THEIR WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS AND SPECIFICATIONS.

BITUMINOUS MATERIALS (PRIME COAT) SHALL BE APPLIED AT A RATE OF 0.1 GALLONS PER SQUARE YARD ON ASPHALT AND 0.5 GALLONS PER SQUARE YARD ON AGGREGATE. BITUMINOUS MATERIALS SHALL BE SS-1 ON ASPHALT AND MC-30 ON AGGREGATE.

DRIVEWAY PAVEMENT REMOVAL SHALL INCLUDE REMOVAL OF ALL EXISTING MATERIAL (WHETHER ASPHALT, CONCRETE, STONE, OR EARTH) TO A DEPTH OF 9 INCHES FROM PROPOSED DRIVEWAY GRADE FOR PCC DRIVEWAY PAVEMENT AND 8 INCHES FOR STABILIZED DRIVEWAYS.

THE ENGINEER SHALL CONTACT DON CHIARUGI, AREA TRAFFIC ENGINEER, AT (847) 741-9857 A MINIMUM OF TWO WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

PCC SIDEWALK AND PCC DRIVEWAY PAVEMENT SHALL HAVE FIBER MESH INSTALLED PER CITY REQUIREMENTS.

PAVEMENT PATCHING AREAS, ADJACENT TO PROPOSED CURB AND GUTTER, SHALL BE BACKFILLED WITH SUBBASE GRANULAR MATERIAL, TYPE B AND CLASS SI CONCRETE AND HAVE HMA SURFACE COURSE AS SHOWN IN THE SPECIAL DETAIL. THE SUBBASE GRANULAR MATERIAL, TYPE B, CLASS SI CONCRETE AND HMA SURFACE COURSE WILL BE CONSIDERED INCLUDED IN THE COST OF COMBINATION CONCRETE CURB AND GUTTER.

THE COST OF EARTH EXCAVATION REQUIRED FOR CONSTRUCTION OF THE SHARED-USE PATH, SIDEWALKS, DRIVEWAYS, CURB AND GUTTER, AND ALL ASSOCIATED ITEMS INCLUDING INSTALLATION OF TOPSOIL SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

DETECTABLE WARNINGS SHALL BE BRICK RED IN COLOR.

A NOTIFICATION SHALL BE GIVEN A MINIMUM OF SEVEN (7) WORKING DAYS PRIOR TO ANY SIGNAL MODIFICATIONS. THE CONTRACTOR SHALL NOTIFY THE FOLLOWING:
*TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER AT (847)705-4424
*IDOT ELECTRICAL MAINTENANCE CONTRACTOR AT (773)287-7600

THE CONTRACTOR SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOUR IN ADVANCE OF BEGINNING WORK.

IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT ANY RESIDENT OR BUSINESS OF ANY REMOVAL AND REPLACEMENT ACTIVITIES THAT WILL INHIBIT OR PROHIBIT ACCESS TO THEIR DRIVEWAY, IN WRITING, A MINIMUM OF 48 HOURS BUT NOT MORE THAN 72 HOURS, PRIOR TO THE COMMENCEMENT OF THESE ACTIVITIES. THE MORING OF THE WORK, THE CONTRACTOR SHALL AGAIN NOTIFY THE OWNER VERBALLY, TO ALLOW THE OWNER TIME TO MOVE THEIR VEHICLE SO AS NOT TO PROHIBIT THE VEHICLE FROM LEAVING THE DRIVEWAY UPON REMOVAL OF ANY MATERIAL. THE NOTICE GIVEN OUT BY THE CONTRACTOR SHALL PROVIDE INFORMATION REGARDING THE ANTICIPATED DATE THAT FULL ACCESS WILL BE RESTORED. COORDINATION BETWEEN ACTIVITIES SHOULD ALLOW ALL WORK TO BE DONE IN A TIMELY MATTER SO AS TO PERMIT ACCESS TO THE ROADWAY. ANY ADDITIONAL COST OF STAGING REQUIRED TO MAINTAIN ACCESS IS CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

IDOT HIGHWAY STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-07	PERPENDICULAR CURB RAMPS FOR SIDEWALK
424016-01	MID-BLOCK CURB RAMPS FOR SIDEWALKS
424021-01	DEPRESSED CORNER FOR SIDEWALKS
424026-01	ENTRANCE / ALLEY PEDESTRIAN CROSSINGS
606001-05	CONCRETE CURB TYPE B AND COMBINATION CURB AND GUTTER
701006-04	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5m) TO 24" (600 mm) FROM PAVEMENT EDGE
701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE 2L, 2W, UNDIVIDED
701701-08	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-02	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-03	SIGN PANEL ERECTION DETAILS
780001-03	TYPICAL PAVEMENT MARKINGS
814001-02	HANDHOLES
857001-01	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
873001-02	TRAFFIC SIGNAL GROUNDING & BONDING
876001-02	PEDESTRIAN PUSH BUTTON POST
878001-09	CONCRETE FOUNDATION DETAILS
886001-01	DETECTOR LOOP INSTALLATIONS

STRUCTURE ADJUSTMENT / REMOVAL NOTATION

"ADJ" FOR ADJUST — ADJ

SUPPLEMENTAL LEGEND

SEE IDOT HIGHWAY STANDARDS FOR ADDITIONAL INFORMATION

- EXISTING PCC SIDEWALK
- EXISTING CONCRETE CURB AND GUTTER, DRIVEWAY PAVEMENT AND SIDEWALK TO BE REMOVED
- PROPOSED HOT-MIX ASPHALT (SHARED-USE PATH OR STABILIZED DRIVEWAYS)
- PROPOSED PCC SIDEWALK
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- EXISTING CURB OR CURB & GUTTER
- PROPOSED CURB OR CURB & GUTTER

Engineering Enterprises, Inc.
CONSULTING ENGINEERS
52 Wheeler Road
Sugar Grove, Illinois 60554
630.466.6700 / www.eeiweb.com

USER NAME =	DESIGNED - SWM	REVISED -	
	DRAWN - CLN	REVISED -	
PLOT SCALE =	CHECKED - JRL	REVISED -	
PLOT DATE =	DATE - 07/20/12	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES, HIGHWAY STANDARDS, AND LEGEND

SCALE: N/A | SHEET NO. 1 OF 1 SHEETS | STA. N/A | TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	2
		CONTRACT NO. 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-9003(841)		

Plotted: October 11, 2012 @ 9:29 AM By: Jim Schmidt - Tcs: 02 Index 22x34

Path: H:\939PROJ\WW1006\DWG\DWG FINAL ENG\WW1006-GVR

SUMMARY OF QUANTITIES

SPECIAL PROVISION	SPECIALTY ITEM	CODE NO	ITEM DESCRIPTION	UNIT	URBAN TOTAL QUANTITY	FEDERAL = 75% CITY = 25% TRAFFIC SIGNAL 0021	FEDERAL = 75% CITY = 25% SHARED-USE PATH 0028
		20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	52		52
	*	20101200	TREE ROOT PRUNING	EACH	2		2
		21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	2,267		2,267
	*	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	1,040		1,040
		21301072	EXPLORATION TRENCH 72" DEPTH	FOOT	50		50
	*	25000210	SEEDING, CLASS 2A	ACRE	0.25		0.25
	*	25000400	NITROGEN FERTILIZER NUTRIENT	POUND	19		19
	*	25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	19		19
	*	25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	19		19
Δ	*	25100630	EROSION CONTROL BLANKET	SQ YD	1,040		1,040
		35102000	AGGREGATE BASE COURSE, TYPE B 8"	SQ YD	2,267		2,267
Δ		40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	50		50
		40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	668		668
		40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	282		282
		40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	169		169
		42001300	PROTECTIVE COAT	SQ YD	1,040		1,040
Δ		42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	21		21
		42400299	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	8,485		8,485
		42400800	DETECTABLE WARNINGS	SQ FT	528		528
		44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	122		122
		44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	457		457
		44000600	SIDEWALK REMOVAL	SQ FT	14,696		14,696
	*	56400100	FIRE HYDRANTS TO BE MOVED	EACH	1		1
		60255500	MANHOLES TO BE ADJUSTED	EACH	4		4
Δ		60256600	VALVE BOXES TO BE ADJUSTED	EACH	1		1
		60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	171		171
		60609200	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.12	FOOT	286		286
		67100100	MOBILIZATION	L SUM	1		1
Δ		70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1		1
Δ		70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1		1
Δ		70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701901	L SUM	1		1
	*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1,544		1,544
	*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	2,110		2,110
	*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	122		122
		78300100	PAVEMENT MARKING REMOVAL	SQ FT	513		513
	*	81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	94	94	
	*	81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	193	193	
	*	81400100	HANDHOLE	EACH	1	1	
Δ	*	83600200	LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	6		6
	*	84200804	REMOVAL OF POLE FOUNDATION	EACH	2		2
Δ	*	84400105	RELOCATE EXISTING LIGHTING UNIT	EACH	2		2
	*	85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1	1	
	*	87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	925	925	
	*	87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	951	951	
	*	87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	76	76	
	*	87502440	TRAFFIC SIGNAL POST, GALVANIZED STEEL 10 FT.	EACH	4	4	
	*	87800100	CONCRETE FOUNDATION, TYPE A	FOOT	16	16	
	*	87900200	DRILL EXISTING HANDHOLE	EACH	9	9	
	*	88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	8	8	
	*	88600100	DETECTOR LOOP, TYPE I	FOOT	105	105	
	*	88800100	PEDESTRIAN PUSH-BUTTON	EACH	8	8	
	*	89502200	MODIFY EXISTING CONTROLLER	EACH	1	1	
	*	89502350	REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	266	266	
Δ	*	89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1	1	
	*	B2000216	TREE, ACER GINNALA (AMUR MAPLE), 2" CALIPER, TREE FORM, BALLED AND BURLAPPED	EACH	3		3
Δ		X6026050	SANITARY MANHOLES TO BE ADJUSTED	EACH	4		4
Δ	*	X8360110	LIGHT POLE FOUNDATION, SPECIAL	FOOT	5		5
Δ		Z0066700	STABILIZED DRIVEWAYS 10"	SQ YD	71		71

Δ SEE SPECIAL PROVISIONS

* SPECIALTY ITEMS

Engineering Enterprises, Inc.
CONSULTING ENGINEERS
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Sugar Grove, Illinois 60554
630.466.6700 / www.eeiweb.com

USER NAME =	DESIGNED - SWM	REVISED - TVW/JPS 10/11/12
	DRAWN - CLN	REVISED - TVW/JPS 11/30/12
PLOT SCALE =	CHECKED - JRL	REVISED -
PLOT DATE =	DATE - 07/20/12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: N/A | SHEET NO. 1 OF 1 SHEETS | STA. N/A | TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	3
		CONTRACT NO. 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-9003(841)		

Plotted: January 18, 2013 @ 1:54 PM By: Kris Pung – Tab: 03 SDQ 22x34

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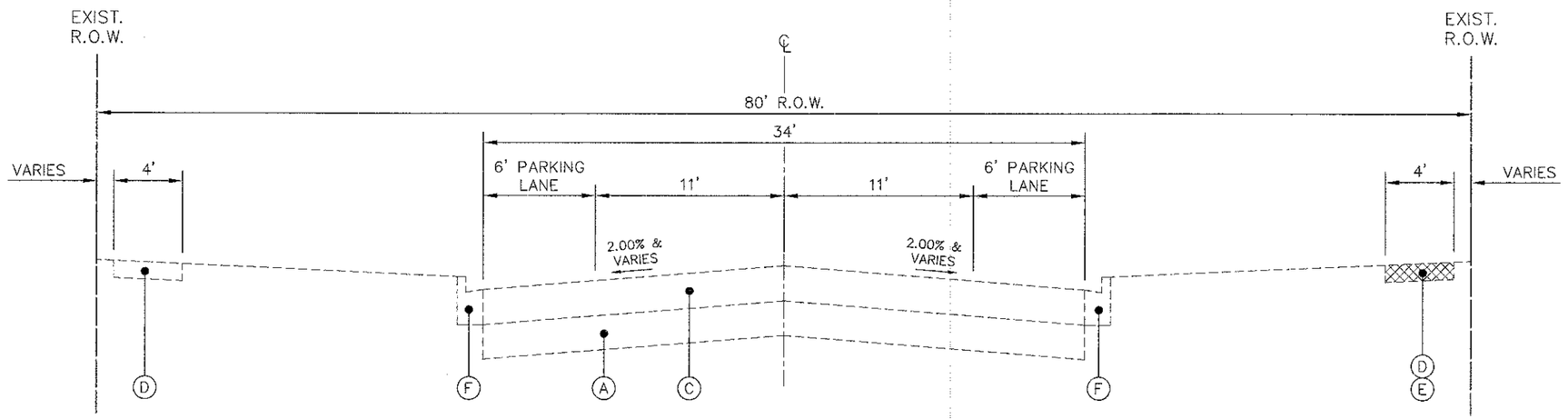
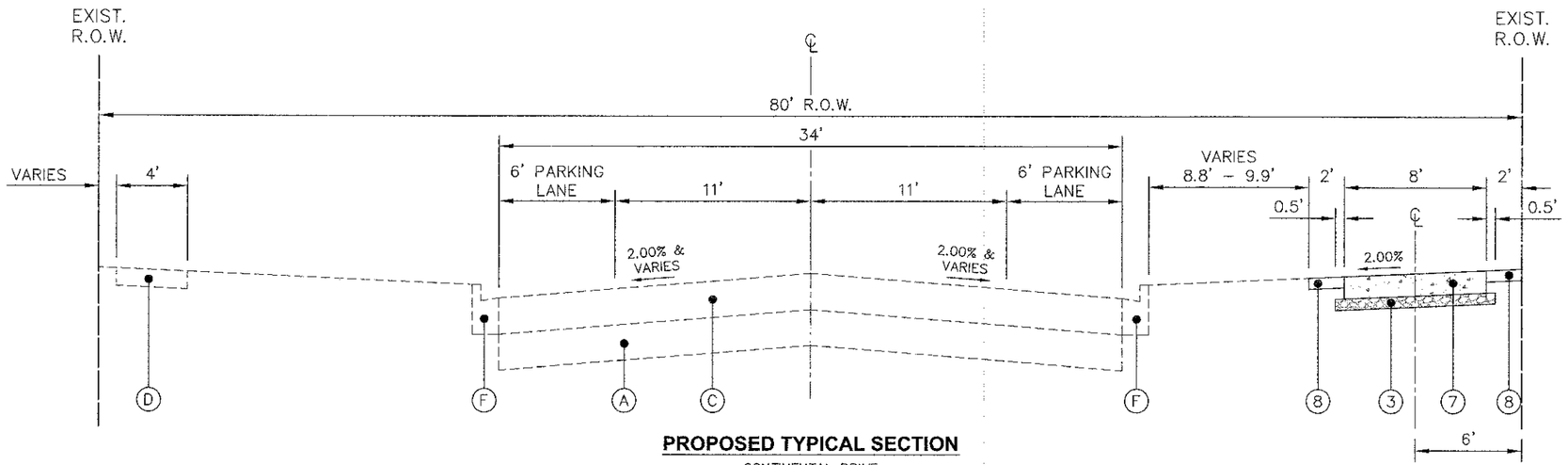


EXISTING TYPICAL SECTION

CONTINENTAL DRIVE
STA 100+13.07 TO STA 104+50
N.T.S.



PROPOSED TYPICAL SECTION

CONTINENTAL DRIVE
STA 100+13.07 TO STA 104+50

N.T.S.

LEGEND

- (A) EXISTING BASE
- (B) EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (C) EXISTING HOT-MIX ASPHALT PAVEMENT
- (D) EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
- (E) SIDEWALK REMOVAL
- (F) EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.12
- (1) GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- (2) AGGREGATE BASE COURSE, TYPE B, 8"
- (3) AGGREGATE BASE COURSE, TYPE B, 2" (INCLUDED IN COST OF PCC SIDEWALK)
- (4) BITUMINOUS MATERIALS (PRIME COAT)
- (5) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2.5"
- (6) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1.5"
- (7) PORTLAND CEMENT CONCRETE SIDEWALK, 5" W/ FIBER MESH
- (8) TOPSOIL 4", CLASS 2A SEED, AND EROSION CONTROL BLANKET

Engineering Enterprises, Inc.
CONSULTING ENGINEERS
52 Wheeler Road
Sugar Grove, Illinois 60554
630.466.6700 / www.eeiweb.com

USER NAME =	DESIGNED - SWM	REVISED -
	DRAWN - CLN	REVISED -
PLOT SCALE =	CHECKED - JRL	REVISED -
PLOT DATE =	DATE - 07/20/12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

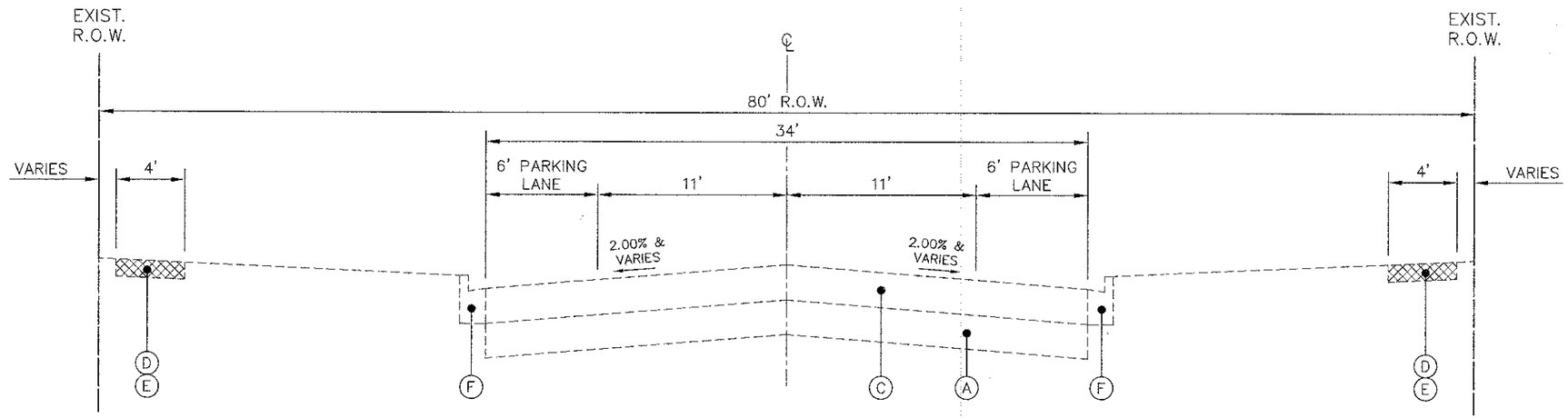
TYPICAL SECTION

SCALE: N/A | SHEET NO. 1 OF 4 SHEETS | STA. N/A | TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	4
		CONTRACT NO. 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-9003(841)		

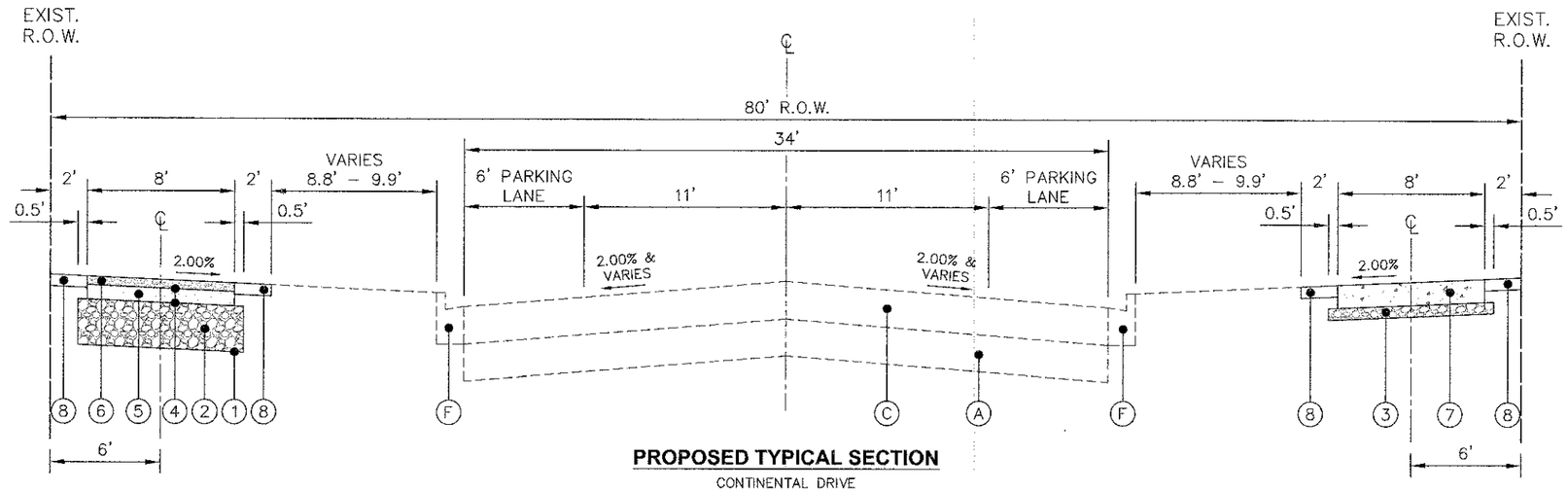
Plotted: October 11, 2012 @ 9:48 AM By: Jim Schmidt - Tab: 04 Section 22x34

Path: H:\SYS\PROJ\WW1006\DWG\DWG FINAL ENG\WW1006-EVR



EXISTING TYPICAL SECTION

CONTINENTAL DRIVE
STA 104+50 TO STA 105+13
N.T.S.



PROPOSED TYPICAL SECTION

CONTINENTAL DRIVE
STA 104+50 TO STA 105+13
N.T.S.

LEGEND

- (A) EXISTING BASE
- (B) EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (C) EXISTING HOT-MIX ASPHALT PAVEMENT
- (D) EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
- (E) SIDEWALK REMOVAL
- (F) EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.12
- (1) GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- (2) AGGREGATE BASE COURSE, TYPE B, 8"
- (3) AGGREGATE BASE COURSE, TYPE B, 2" (INCLUDED IN COST OF PCC SIDEWALK)
- (4) BITUMINOUS MATERIALS (PRIME COAT)
- (5) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2.5"
- (6) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1.5"
- (7) PORTLAND CEMENT CONCRETE SIDEWALK, 5" W/ FIBER MESH
- (8) TOPSOIL 4", CLASS 2A SEED, AND EROSION CONTROL BLANKET

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

OPERATION	MIXTURE TYPE	AIR VOIDS @ N_{des}
SHARED-USE PATH	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"	4% @ 50 Gyr.
	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 mm), 1 1/2"	4% @ 50 Gyr.
DRIVEWAY RECONSTRUCTION	STABILIZED DRIVEWAYS, 10" HMA SURFACE COURSE, MIX "D", N50 (IL-9.5 MM), 2"	4% @ 50 Gyr.
PATCHING	PAVEMENT PATCHING (INCLUDED IN COST OF CURB & GUTTER) HMA SURFACE COURSE, MIX "D", N50 (IL-9.5 MM), 2"	4% @ 50 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/INCH.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
FOR "PERCENT OF RAP" SEE SPECIAL PROVISIONS

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	DRAWN - CLN	REVISED - --- ----------
PLOT SCALE =	CHECKED - JRL	REVISED - --- ----------
PLOT DATE =	DATE - 07/20/12	REVISED - --- ----------

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

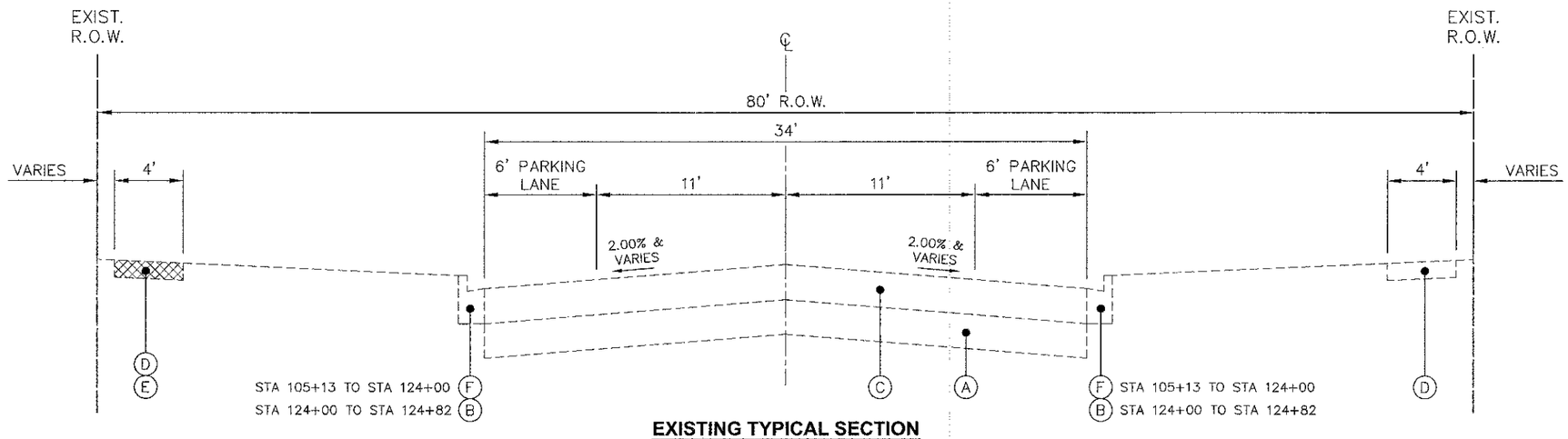
TYPICAL SECTION

SCALE: N/A | SHEET NO. 2 OF 4 SHEETS | STA. N/A | TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	5
		CONTRACT NO. 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-9003(841)		

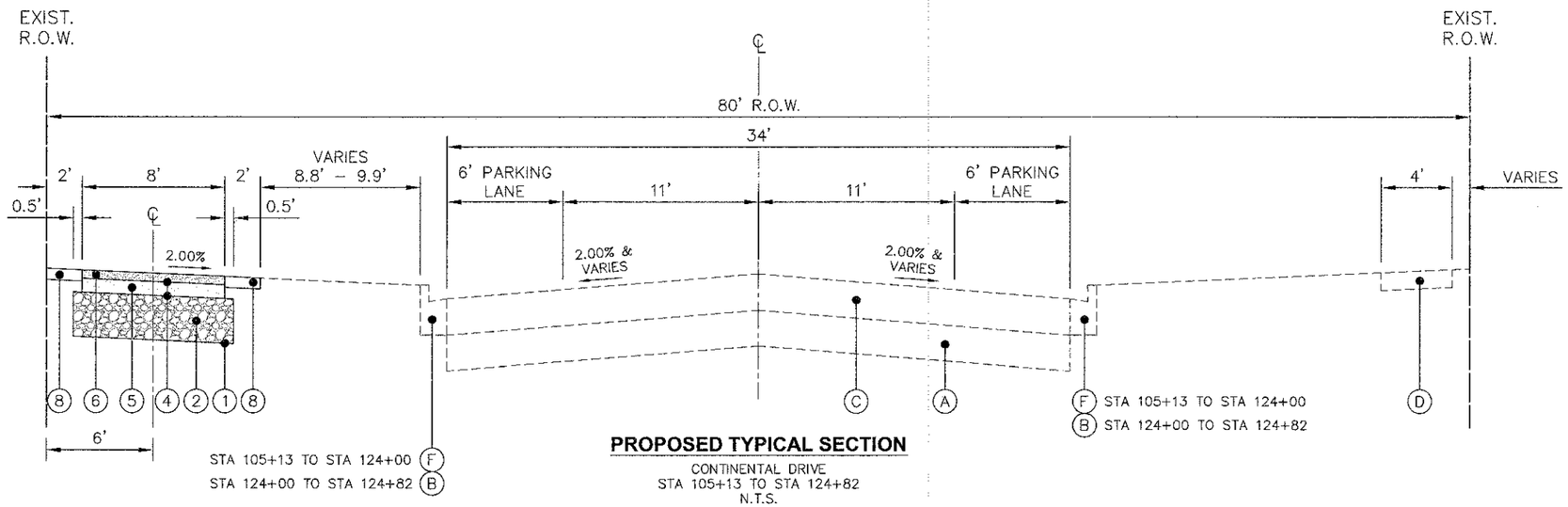
Plotted: October 11, 2012 @ 9:49 AM By: Jim Schmidt - Tab: 05 Section 22x34

Path: H:\SDSK\PROJ\WV1096\DWG\DWG FINAL ENG WV1096-CVR



EXISTING TYPICAL SECTION

CONTINENTAL DRIVE
STA 105+13 TO STA 124+82
N.T.S.



PROPOSED TYPICAL SECTION

CONTINENTAL DRIVE
STA 105+13 TO STA 124+82
N.T.S.

LEGEND

- (A) EXISTING BASE
- (B) EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (C) EXISTING HOT-MIX ASPHALT PAVEMENT
- (D) EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
- (E) SIDEWALK REMOVAL
- (F) EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.12
- (1) GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- (2) AGGREGATE BASE COURSE, TYPE B, 8"
- (3) AGGREGATE BASE COURSE, TYPE B, 2" (INCLUDED IN COST OF PCC SIDEWALK)
- (4) BITUMINOUS MATERIALS (PRIME COAT)
- (5) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2.5"
- (6) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1.5"
- (7) PORTLAND CEMENT CONCRETE SIDEWALK, 5" W/ FIBER MESH
- (8) TOPSOIL 4", CLASS 2A SEED, AND EROSION CONTROL BLANKET

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	DRAWN - CLN	REVISED -	---
PLOT SCALE =	CHECKED - JRL	REVISED -	---
PLOT DATE =	DATE - 07/20/12	REVISED -	---

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

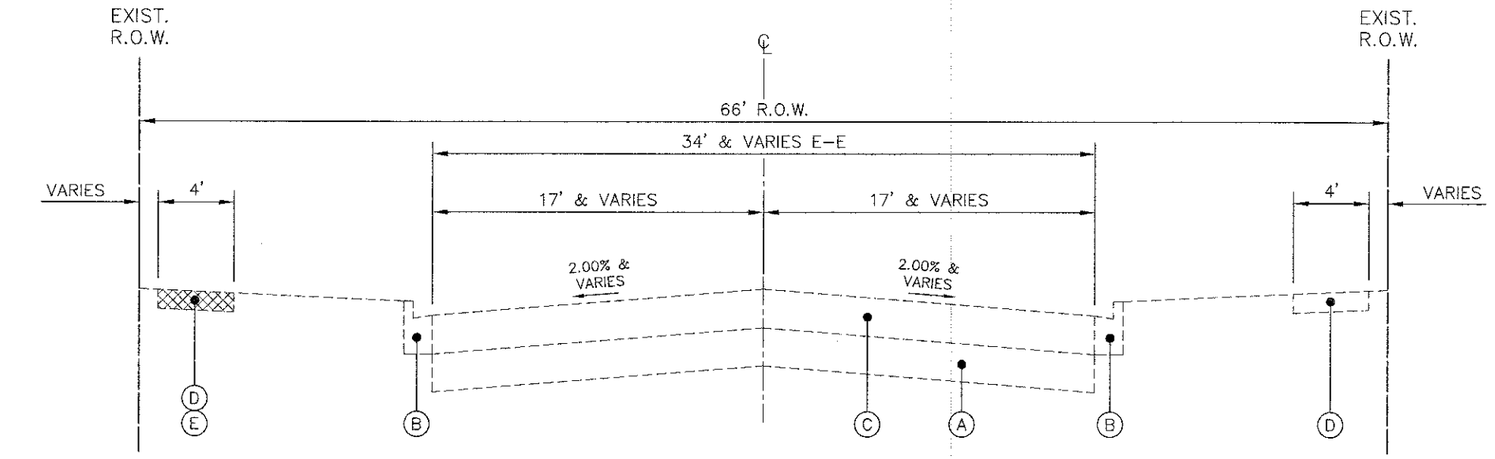
TYPICAL SECTION

SCALE: N/A | SHEET NO. 3 OF 4 SHEETS | STA. N/A | TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	6
		CONTRACT NO. 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-9003(841)		

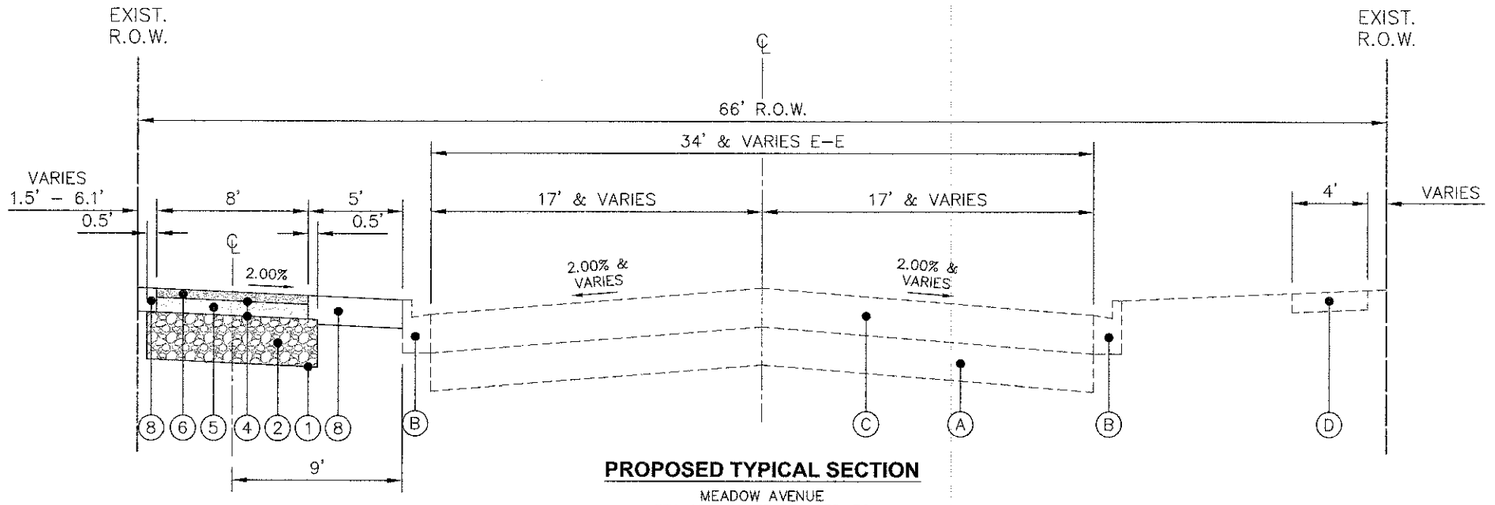
Plotted: October 11, 2012 @ 9:50 AM By: Jim Schmidt - Tab: 06 Section 22x34

Path: H:\SDS\PROJ\WY\006\DWG\ENG FINAL ENG\WY1006-CVR



EXISTING TYPICAL SECTION

MEADOW AVENUE
STA 124+82 TO STA 132+32
N.T.S.



PROPOSED TYPICAL SECTION

MEADOW AVENUE
STA 124+82 TO STA 132+32
N.T.S.

LEGEND

- (A) EXISTING BASE
- (B) EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (C) EXISTING HOT-MIX ASPHALT PAVEMENT
- (D) EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
- (E) SIDEWALK REMOVAL
- (F) EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.12
- (1) GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- (2) AGGREGATE BASE COURSE, TYPE B, 8"
- (3) AGGREGATE BASE COURSE, TYPE B, 2" (INCLUDED IN COST OF PCC SIDEWALK)
- (4) BITUMINOUS MATERIALS (PRIME COAT)
- (5) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2.5"
- (6) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1.5"
- (7) PORTLAND CEMENT CONCRETE SIDEWALK, 5" W/ FIBER MESH
- (8) TOPSOIL 4", CLASS 2A SEED, AND EROSION CONTROL BLANKET

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Sugar Grove, Illinois 60554
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PLOT SCALE =	CHECKED - JRL	REVISED -	---
PLOT DATE =	DATE - 07/20/12	REVISED -	---

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTION

SCALE: N/A | SHEET NO. 4 OF 4 SHEETS | STA. N/A | TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	7
		CONTRACT NO. 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-9003(841)		

Plotted: October 11, 2012 @ 9:57 AM By: Jim Schmidt - Tab: 07 Section 22x34

Path: H:\SDSRPROJ\WY1006\DWG\DWG FINAL E-W\WY1006-CVR

DETECTABLE WARNING SCHEDULE

STATION	OFFSET	LT/RT	LENGTH	WIDTH	SQ FT
CONTINENTAL DRIVE					
100+34	66.0	RT	2.0	7.0	14.0
100+78	66.0	RT	2.0	7.0	14.0
103+90	52.8	RT	2.0	7.0	14.0
103+95	12.5	RT	2.0	12.0	24.0
104+05	0.0		2.0	7.0	14.0
104+36	0.0		2.0	7.0	14.0
104+48	14.5	RT	2.0	3.0	6.0
104+48	52.7	RT	2.0	4.0	8.0
105+12	67.1	RT	2.0	7.0	14.0
105+59	67.2	RT	2.0	7.0	14.0
108+06	0.0		2.0	7.0	14.0
108+37	0.0		2.0	7.0	14.0
108+49	13.2	RT	2.0	3.0	6.0
108+53	53.2	RT	2.0	4.0	8.0
112+62	0.0		2.0	7.0	14.0
113+33	0.0		2.0	7.0	14.0
115+77	0.0		2.0	7.0	14.0
116+07	0.0		2.0	7.0	14.0
117+16	0.0		2.0	7.0	14.0
117+45	0.0		2.0	20.0	40.0
117+52	56.4	RT	2.0	4.0	8.0
124+27	13.3	RT	2.0	3.0	6.0
124+26	54.1	RT	2.0	3.0	6.0
124+42	67.6	RT	2.0	3.0	6.0
124+42	0.0		2.0	7.0	14.0
MEADOW AVENUE					
125+05	78.3	RT	2.0	4.0	8
125+32	48.0	RT	2.0	3.0	6.0
125+41	0.0		2.0	20.0	40.0
127+13	0.0		2.0	7.0	14.0
127+42	0.0		2.0	7.0	14.0
128+30	0.0		2.0	7.0	14.0
128+58	0.0		2.0	7.0	14.0
129+53	9.0	RT	2.0	7.0	14.0
129+57	51.8	RT	2.0	20.0	40.0
129+95	55.4	RT	2.0	7.0	14.0
132+00	0.0		2.0	7.0	14.0
132+32	16.7	RT	2.0	4.0	8.0
TOTAL					**528**

SIDEWALK SCHEDULE

STATION	STATION	SIDEWALK REM	PC CONC SIDEWALK 5	PROTECTIVE COAT
		SQ FT	SQ FT	SQ YD
CONTINENTAL DRIVE				
100+00	110+50	6,188.0	6,516.2	724.0
110+50	121+00	4,112.0	668.0	74.2
121+00	124+82	1,508.0	240.0	26.7
MEADOW AVENUE				
124+82	132+32	2,888.0	1,060.6	117.8
TOTAL		**14,696**	**8,485**	**943**

SHARED-USE PATH QUANTITIES

STATION	STATION	AGG BASE CSE B 8	HMA BC IL-19.0 N50	HMA SC "D" N50	GEOTECH FAB F/GR STAB	BIT MATLS PR CT
		SQ YD	TON	TON	SQ YD	GALLON
CONTINENTAL DRIVE						
100+00	110+50	548.0	68.3	41.0	548.0	161.4
110+50	121+00	851.0	105.8	63.5	851.0	250.6
121+00	124+82	326.0	40.6	24.4	326.0	96.0
MEADOW AVENUE						
124+82	132+32	542.0	67.5	40.5	542.0	159.6
TOTAL		**2,267**	**282**	**169**	**2,267**	**668**

DRIVEWAY SCHEDULE

STATION	LT/RT	TYPE	PE/CE	DRIVE PAVEMENT REM	PCC DRIVEWAY PAVT 7	STAB DRIVEWAYS 10	PROTECTIVE COAT
				SQ YD	SQ YD	SQ YD	SQ YD
CONTINENTAL DRIVE							
112+12	LT	HMA	PE	31.7		22.2	
114+21	LT	HMA	PE	32.7		23.0	
MEADOW AVENUE							
131+31	LT	HMA	PE	44.4		26.0	
132+24	RT	PCC	PE	13.1	21.1		21.1
TOTAL				**122**	**21**	**71**	**21**

CONCRETE CURB & GUTTER SCHEDULE

STATION	OFFSET	LT/RT	COMB CC&G TB6.12	COMB CC&G TM6.12	COMB CURB GUTTER REM	PROTECTIVE COAT
			FOOT	FOOT	FOOT	SQ YD
CONTINENTAL DRIVE						
100+34	68.0	RT		12.0	12.0	2.0
100+78	68.0	RT		12.0	12.0	2.0
103+90	52.8	RT		12.0	12.0	2.0
104+02	6.1	RT		37.0	37.0	6.2
104+36	0.0			12.0	12.0	2.0
105+12	67.1	RT		12.0	12.0	2.0
105+59	67.2	RT		12.0	12.0	2.0
108+06	0.0			12.0	12.0	2.0
108+37	0.0			12.0	12.0	2.0
112+62	0.0			29.0	29.0	4.8
113+33	0.0			29.0	29.0	4.8
115+77	0.0			22.0	22.0	3.7
116+07	0.0			30.0	30.0	5.0
117+16	0.0			12.0	12.0	2.0
117+45	0.0			21.0	21.0	3.5
122+16	53.5	RT		5.0	5.0	0.8
122+65	53.1	RT		5.0	5.0	0.8
124+26	54.1	RT	5.0		5.0	0.8
124+27	13.3	RT	5.0		5.0	0.8
124+42	0.0		12.0		12.0	2.0
MEADOW AVENUE						
125+41	0.0		23.0		23.0	3.8
127+13	0.0		12.0		12.0	2.0
127+42	0.0		12.0		12.0	2.0
128+30	0.0		12.0		12.0	2.0
128+58	0.0		12.0		12.0	2.0
129+19	9.1	RT	5.0		5.0	0.8
129+19	46.1	RT	5.0		5.0	0.8
129+53	9.0	RT	12.0		12.0	2.0
129+57	51.8	RT	26.0		26.0	4.3
129+95	55.4	RT	16.0		16.0	2.7
132+00	0.0		14.0		14.0	2.3
TOTAL			**171**	**286**	**457**	**76**

PAVEMENT MARKING

STATION	STATION	6" WHITE	12" WHITE	24" WHITE	PAVT MARKING REMOVAL
		FOOT	FOOT	FOOT	SQ FT
100+00	110+50	408	663		211
110+50	121+00	338	488		
121+00	124+84	408	375	86	108
124+82	132+32	390	584	36	194
TOTALS		**1,544**	**2,110**	**122**	**513**

LANDSCAPING

STATION	STATION	TOPSOIL F & P 4	NITROGEN FERT NUTR	PHOSPHORUS FERT NUTR	POTASSIUM FERT NUTR	SEEDING CL 2A	EROSION CONTR BLANKET
		SQ YD	POUND	POUND	POUND	ACRE	SQ YD
CONTINENTAL DRIVE							
100+00	110+50	291.8	5.4	5.4	5.4	0.06	291.8
110+50	121+00	227.8	4.2	4.2	4.2	0.05	227.8
121+00	124+82	97.3	1.8	1.8	1.8	0.02	97.3
MEADOW AVENUE							
124+82	132+32	422.8	7.9	7.9	7.9	0.09	422.8
TOTAL		**1,040**	**19**	**19**	**19**	**0.21 ***	**1,040**

* 0.25 MIN. PER BDE

STRUCTURE ADJUSTMENT SCHEDULE

STATION	OFFSET	LT/RT	MAN ADJUST	FIRE HYDNTS TO BE MVD	VALVE BOXES TO BE ADJUSTED	SANITARY MANHOLES TO BE ADJUSTED
			EACH	EACH	EACH	EACH
CONTINENTAL DRIVE						
100+14	63.0	RT			1	
100+17	63.7	RT		1		
103+86	7.3	RT	1			
106+72	4.3	RT				1
111+47	0.7	LT	1			
111+77	2.5	RT				1
113+36	3.5	RT				1
114+50	4.1	RT	1			
MEADOW AVENUE						
129+10	4.0	RT	1			
129+51	56.9	RT				1
130+26	2.9	RT	1			
TOTAL			**4**	**1**	**1**	**4**

LIGHT POLE REMOVAL AND RELOCATION

EXISTING		PROPOSED		RELOC EX LT UNIT	REM POLE FDN	LIGHT POLE FDN 24D	LIGHT POLE FDN SPL
STATION	OFFSET	STATION	OFFSET	EACH	EACH	FOOT	FOOT
100+12	248.7 RT	100+08	244 RT	1	1	6	
132+02	4.4 LT	132+07	14.4 LT	1	1		5
TOTALS				**2**	**2**	**6**	**5**

SIGNS TO BE RELOCATED (FOR CITY REFERENCE)

SIGN TYPE	SIGN DESIGNATION	EXISTING		PROPOSED		QTY
		STATION	OFFSET	STATION	OFFSET	EACH
BUS STOP SIGN		104+58	4.0 RT	104+59	8.9 RT	1
DO NOT ENTER/STOP SIGN	R5-1/R1-1	105+09	64.0 RT	105+01	60.4 RT	1
DO NOT ENTER/STOP SIGN	R5-1/R1-1	105+63	66.0 RT	105+72	60.5 RT	1
STOP SIGN	R1-1	129+96	65.0 RT	129+96	67.0 RT	1
TOTALS						**4**

SIGNS INSTALLATION SCHEDULE (FOR CITY REFERENCE)

SIGN TYPE	SIGN DESIGNATION	PROPOSED		QTY
		STATION	OFFSET	EACH
BIKE ROUTE	D11-1	100+06	67' RT	1
BICYCLE ROUTE ARROW SIGNS	M6-1L	100+06	67' RT	1
BIKE ROUTE	D11-1	100+12	59' RT	1
BICYCLE ROUTE ARROW SIGNS	M6-1R	100+12	59' RT	1
BIKE ROUTE	D11-1	103+81	73' RT	1
BICYCLE ROUTE ARROW SIGNS	M6-1L	103+81	73' RT	1
BIKE ROUTE	D11-1	103+86	1' RT	1
BICYCLE ROUTE ARROW SIGNS	M6-1L	103+86	1' RT	1
BIKE ROUTE	D11-1	104+54	5' LT	1
BICYCLE ROUTE ARROW SIGNS	M5-1L	104+54	5' LT	1
BIKE ROUTE	D11-1	115+00	6' RT	2
COMBINATION BIKE AND PED CROSSING	W11-15	130+61	45' RT	1
TRAIL CROSSING (PLAQUE)	W11-15p	130+61	45' RT	1
AHEAD (PLAQUE)	W16-9p	130+61	45' RT	1
STOP SIGN	R1-1	131+73	6' RT	1
COMBINATION BIKE AND PED CROSSING	W11-15	132+15	100' LT	1
TRAIL CROSSING (PLAQUE)	W11-15p	132+15	100' LT	1
AHEAD (PLAQUE)	W16-9p	132+15	100' LT	1
TOTALS				**19**

TREE REMOVAL SCHEDULE

STATION	OFFSET	LT/RT	TREE REMOVAL 6 - 15
			UNIT
100+08	185.35	RT	14.0
113+96 (MULTI-STEM)	6.18	RT	6.0
			6.0
			6.0
			6.0
123+26	4.02	RT	14.0
TOTAL			**52**

TREE REPLACEMENT SCHEDULE

STATION	OFFSET	LT/RT	T-ACER GINN TF 2
			EACH
100+57	7.2	RT	1.0
101+75	7.4	RT	1.0
114+11	59.3	RT	1.0
TOTAL			**3**

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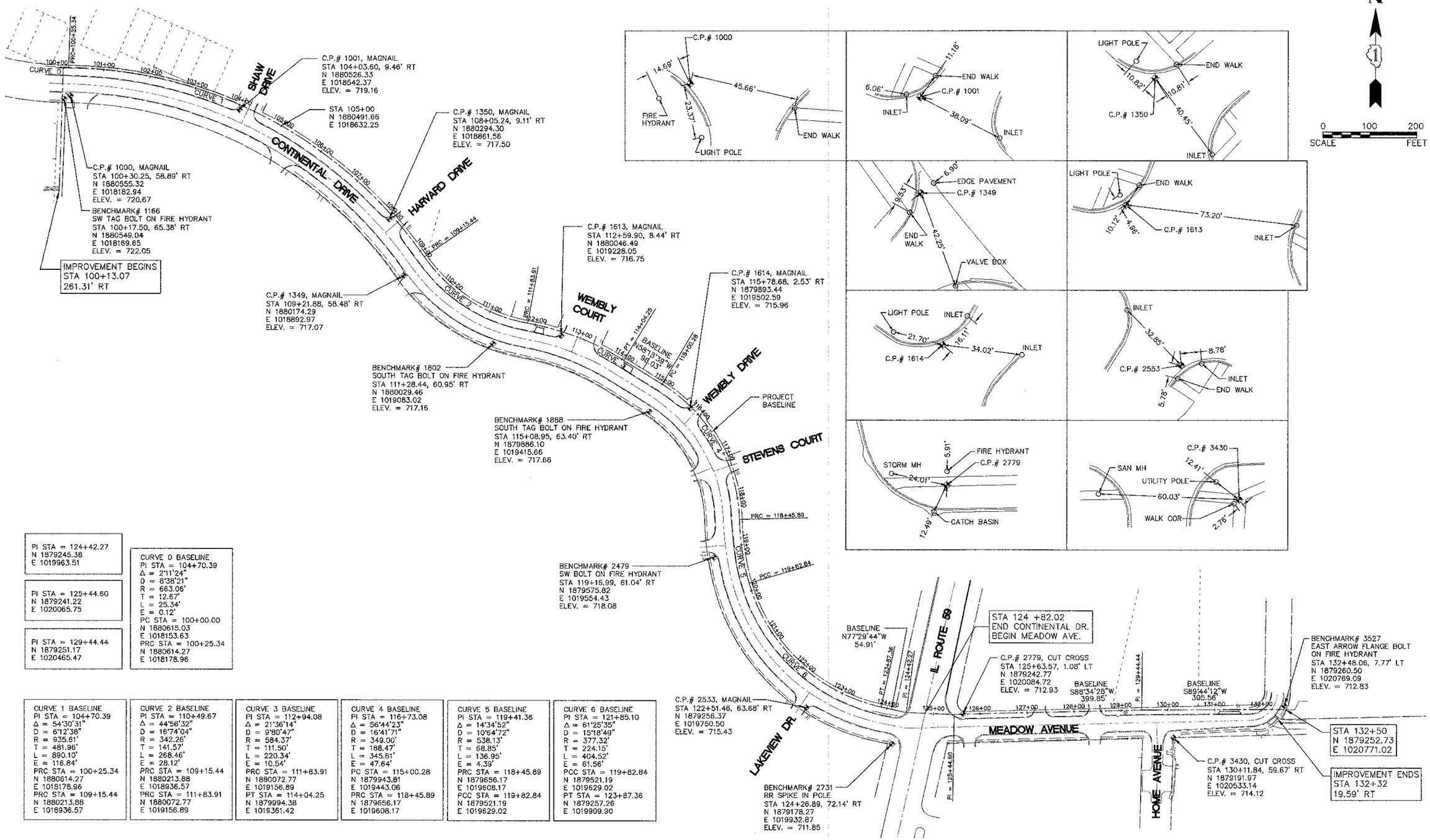
PLOT SCALE = | DRAWN - CLN | REVISED - --- ----------
PLOT DATE = | CHECKED - JRL | REVISED - --- ----------
| DATE - 07/20/12 | REVISED - --- ----------

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

SCALE: N/A | SHEET NO. 1 OF 1 SHEETS | STA. N/A | TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	8
		CONTRACT NO. 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-9003(841)		



PI STA = 124+42.27
N 1879245.38
E 1019963.51

PI STA = 125+44.60
N 1879241.22
E 1020065.75

PI STA = 129+44.44
N 1879251.17
E 1020465.47

CURVE 0 BASELINE
PI STA = 104+70.39
Δ = 2°11'24"
D = 8°38'21"
R = 663.06'
T = 12.67'
L = 25.34'
E = 0.12'
PC STA = 100+00.00
N 1880615.03
E 1018153.63
PRC STA = 100+25.34
N 1880614.27
E 1018178.96

CURVE 1 BASELINE
PI STA = 104+70.39
Δ = 54°30'31"
D = 6°12'38"
R = 935.61'
T = 481.96'
L = 890.10'
E = 116.84'
PRC STA = 100+25.34
N 1880614.27
E 1018178.96
PRC STA = 109+15.44
N 1880213.88
E 1018936.57

CURVE 2 BASELINE
PI STA = 110+49.67
Δ = 44°56'32"
D = 16°74'04"
R = 342.26'
T = 141.57'
L = 268.46'
E = 28.12'
PRC STA = 109+15.44
N 1880213.88
E 1018936.57
PRC STA = 111+83.91
N 1880072.77
E 1019156.89

CURVE 3 BASELINE
PI STA = 112+94.08
Δ = 21°36'14"
D = 9°80'47"
R = 584.37'
T = 111.50'
L = 220.34'
E = 10.54'
PRC STA = 111+83.91
N 1880072.77
E 1019156.89
PT STA = 114+04.25
N 1879994.38
E 1019361.42

CURVE 4 BASELINE
PI STA = 116+73.08
Δ = 56°44'23"
D = 16°41'71"
R = 343.00'
T = 188.47'
L = 345.61'
E = 47.64'
PC STA = 115+00.28
N 1879943.81
E 1019443.06
PRC STA = 118+45.89
N 1879656.17
E 1019608.17

CURVE 5 BASELINE
PI STA = 119+41.36
Δ = 14°34'52"
D = 10°64'72"
R = 538.13'
T = 68.85'
L = 136.95'
E = 4.39'
PRC STA = 118+45.89
N 1879656.17
E 1019608.17
PCC STA = 119+82.84
N 1879521.19
E 1019629.02

CURVE 6 BASELINE
PI STA = 121+85.10
Δ = 61°25'35"
D = 15°18'49"
R = 377.32'
T = 224.15'
L = 404.52'
E = 61.56'
PCC STA = 119+82.84
N 1879521.19
E 1019629.02
PT STA = 123+87.36
N 1879257.26
E 1019909.90

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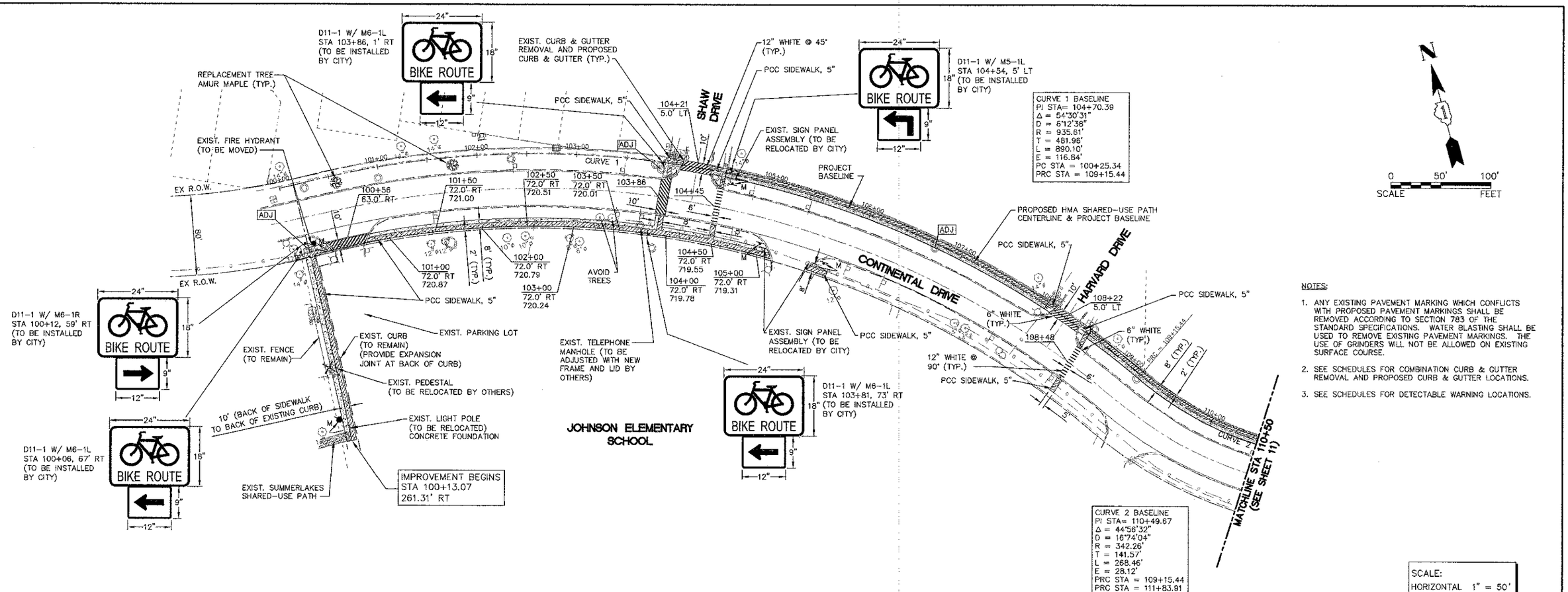
PLOT DATE =	DATE - 07/20/12	REVISED - ---	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES AND BENCHMARKS

SCALE: 1"=100' | SHEET NO. 1 OF 1 SHEETS | STA. 100+00 TO STA. 132+32

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	9
		CONTRACT NO. 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-9003(841)		



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	DRAWN - CLN	REVISED -	---
PLOT SCALE =	CHECKED - JRL	REVISED -	---
PLOT DATE =	DATE - 07/20/12	REVISED -	---

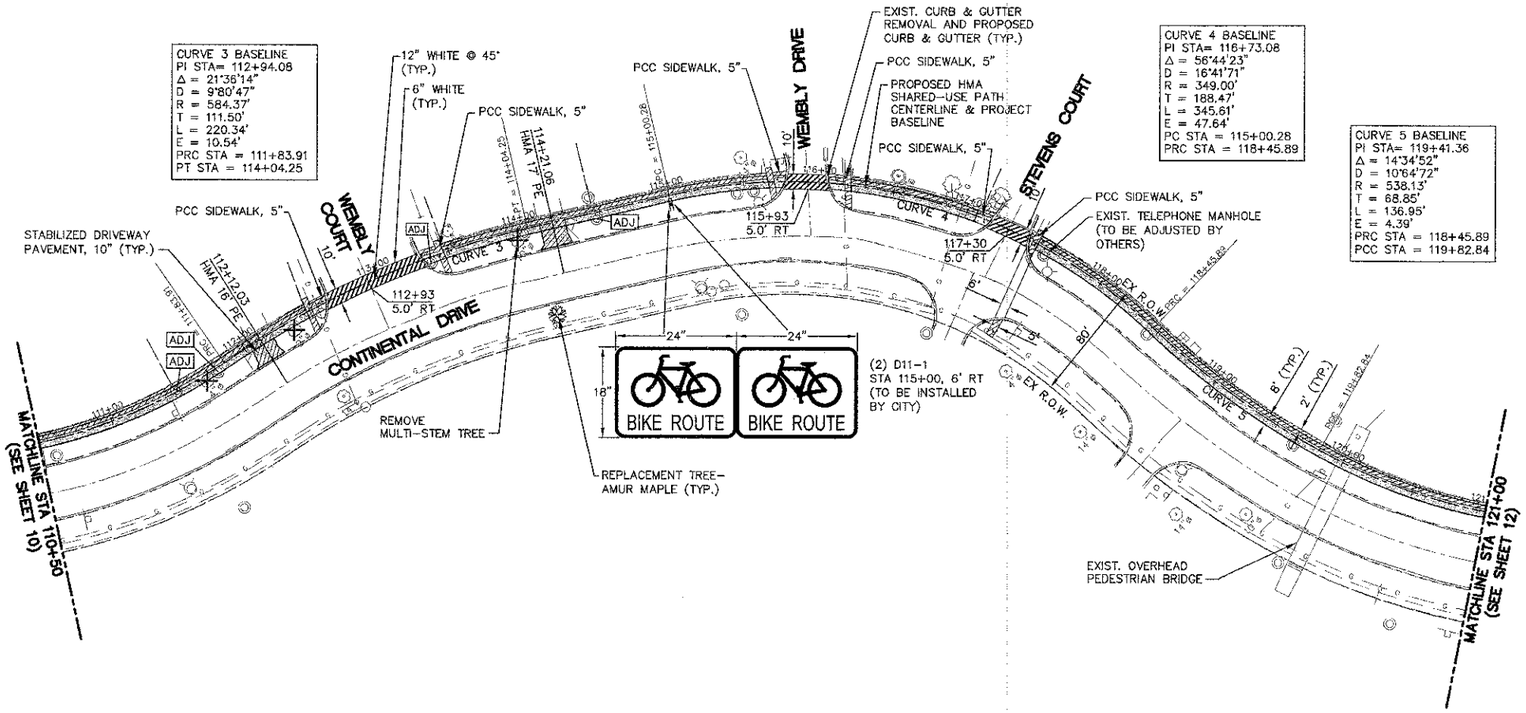
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE
CONTINENTAL DRIVE

SCALE: 1"=50' | SHEET NO. 1 OF 2 SHEETS | STA. 100+00 TO STA. 110+50

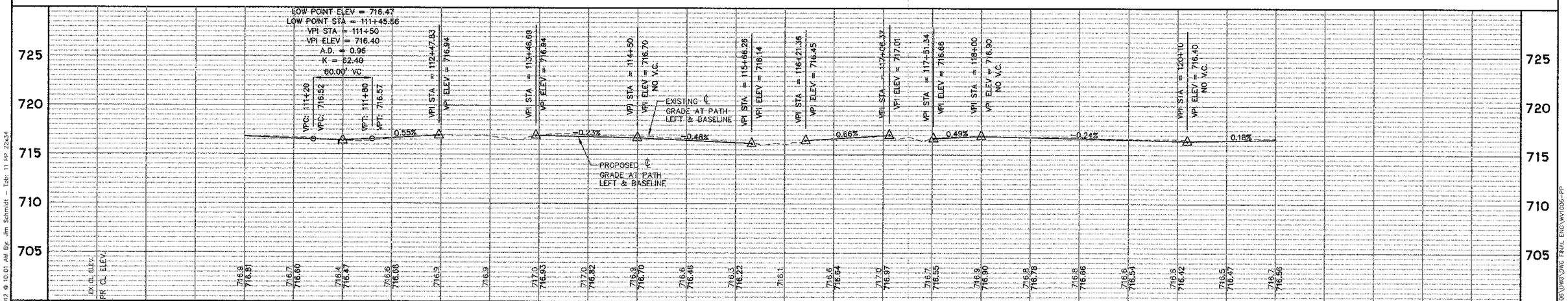
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	10
		CONTRACT NO. 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-9003(841)		

Plotted: October 11, 2012 @ 10:00 AM By Jim Schmidt - Tab: 10 PP 22x34
Path: H:\SDSKPROJ\WV1006\DWG\DWG FINAL ENG\WV1006-PP



NOTES:

1. ANY EXISTING PAVEMENT MARKING WHICH CONFLICTS WITH PROPOSED PAVEMENT MARKINGS SHALL BE REMOVED ACCORDING TO SECTION 783 OF THE STANDARD SPECIFICATIONS. WATER BLASTING SHALL BE USED TO REMOVE EXISTING PAVEMENT MARKINGS. THE USE OF GRINDERS WILL NOT BE ALLOWED ON EXISTING SURFACE COURSE.
2. SEE SCHEDULES FOR COMBINATION CURB & GUTTER REMOVAL AND PROPOSED CURB & GUTTER LOCATIONS.
3. SEE SCHEDULES FOR DETECTABLE WARNING LOCATIONS.



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PLOT SCALE =	CHECKED - JRL	REVISED -	
PLOT DATE =	DATE - 07/20/12	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE
CONTINENTAL DRIVE

SCALE: 1"=50' | SHEET NO. 2 OF 2 SHEETS | STA. 110+50 | TO STA. 121+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	11
		CONTRACT NO. 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-9003(841)		

Plotted: October 11, 2012 @ 10:01 AM By: Jim Schmidt - Tab: 11 HP 22x34
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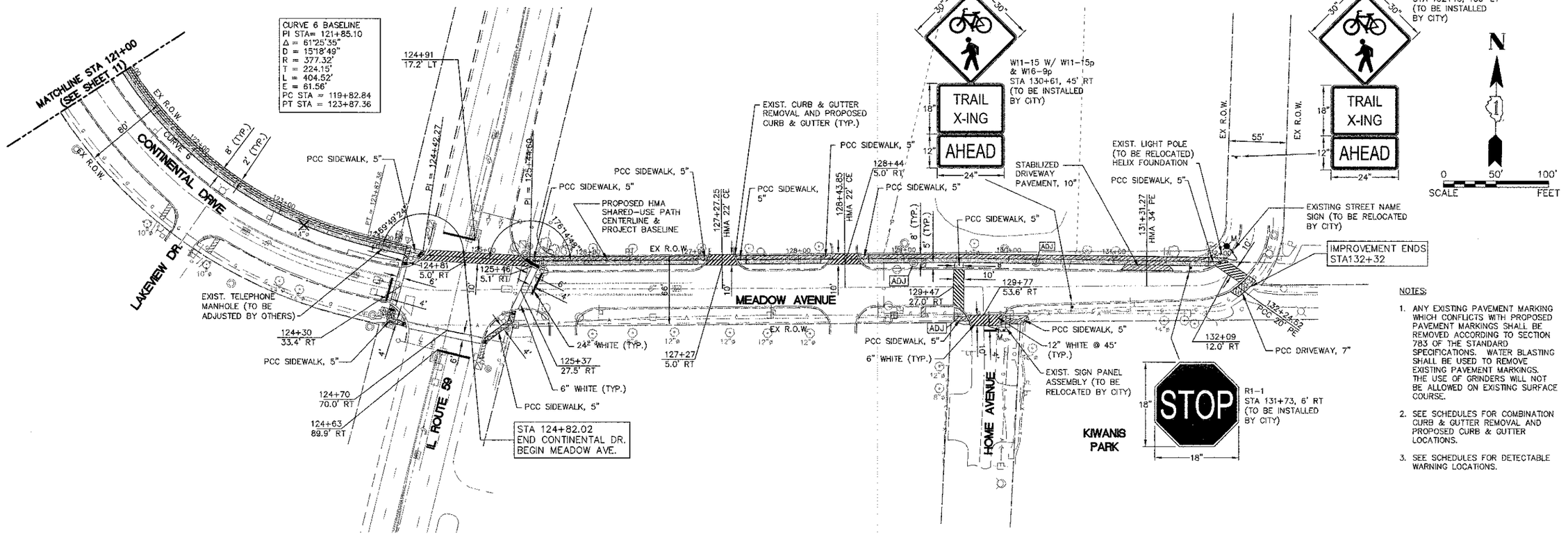
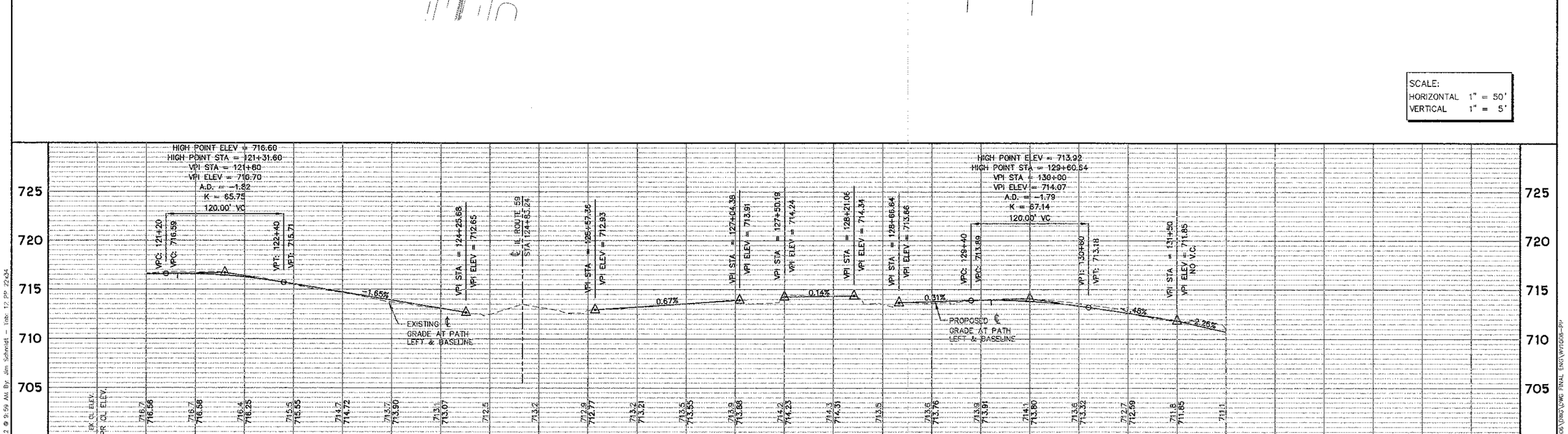


NOTES:

1. ANY EXISTING PAVEMENT MARKING WHICH CONFLICTS WITH PROPOSED PAVEMENT MARKINGS SHALL BE REMOVED ACCORDING TO SECTION 783 OF THE STANDARD SPECIFICATIONS. WATER BLASTING SHALL BE USED TO REMOVE EXISTING PAVEMENT MARKINGS. THE USE OF GRINDERS WILL NOT BE ALLOWED ON EXISTING SURFACE COURSE.
2. SEE SCHEDULES FOR COMBINATION CURB & GUTTER REMOVAL AND PROPOSED CURB & GUTTER LOCATIONS.
3. SEE SCHEDULES FOR DETECTABLE WARNING LOCATIONS.



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PLOT DATE =	DATE - 07/20/12	REVISED -	---

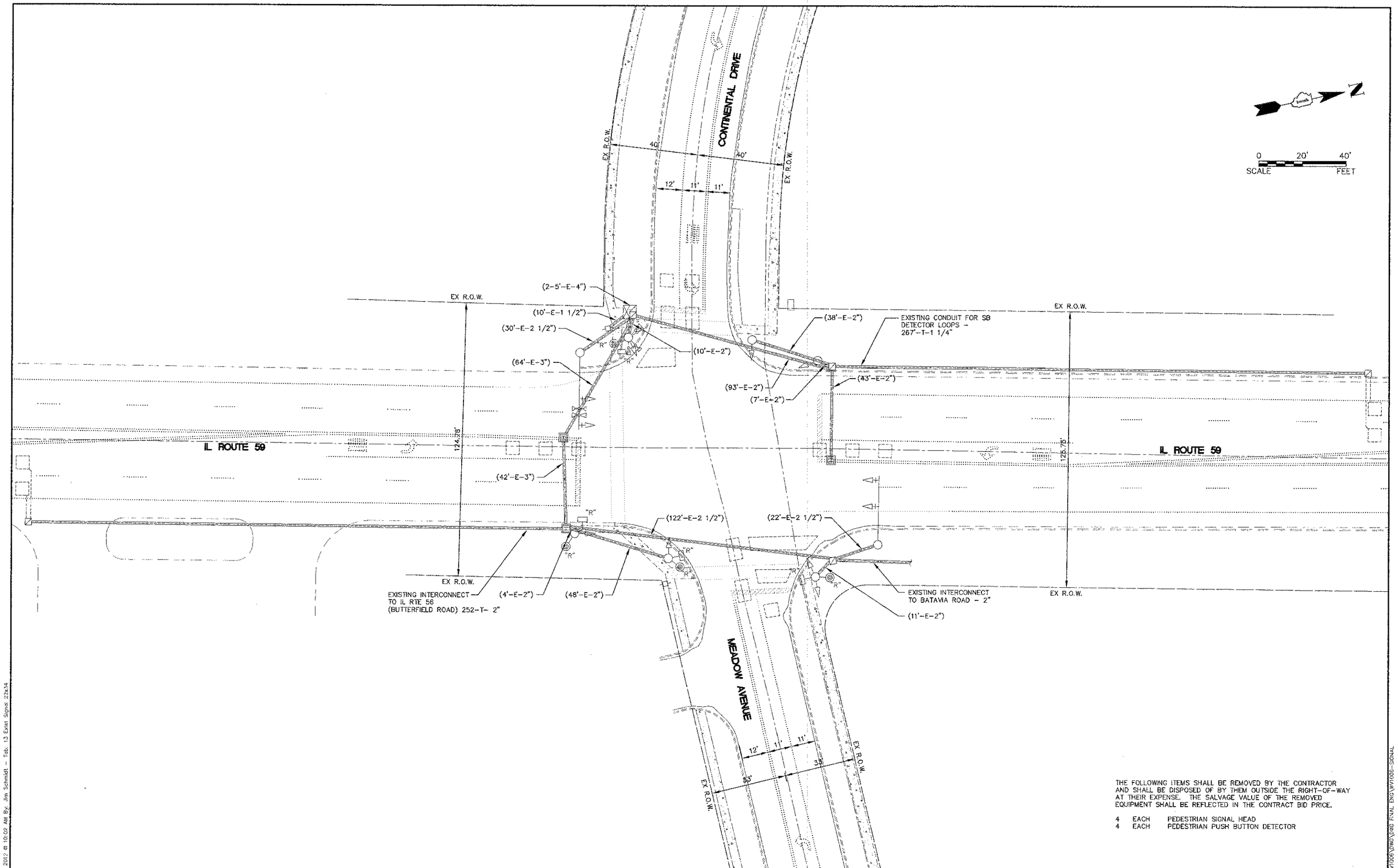
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE
MEADOW AVENUE

SCALE: 1"=50' | SHEET NO. 1 OF 1 SHEETS | STA. N/A | TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	12
		CONTRACT NO. 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-9003(841)		

Plotted: October 11, 2012 @ 9:59 AM By: Jim Schmidt - Tab: 12 PP: 27x34
Path: H:\SDSKPROJ\VV1006\DWG\DWG FINAL ENGR\VV1006-PP



THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

4 EACH PEDESTRIAN SIGNAL HEAD
4 EACH PEDESTRIAN PUSH BUTTON DETECTOR

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PLOT DATE =	DATE - 07/20/12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

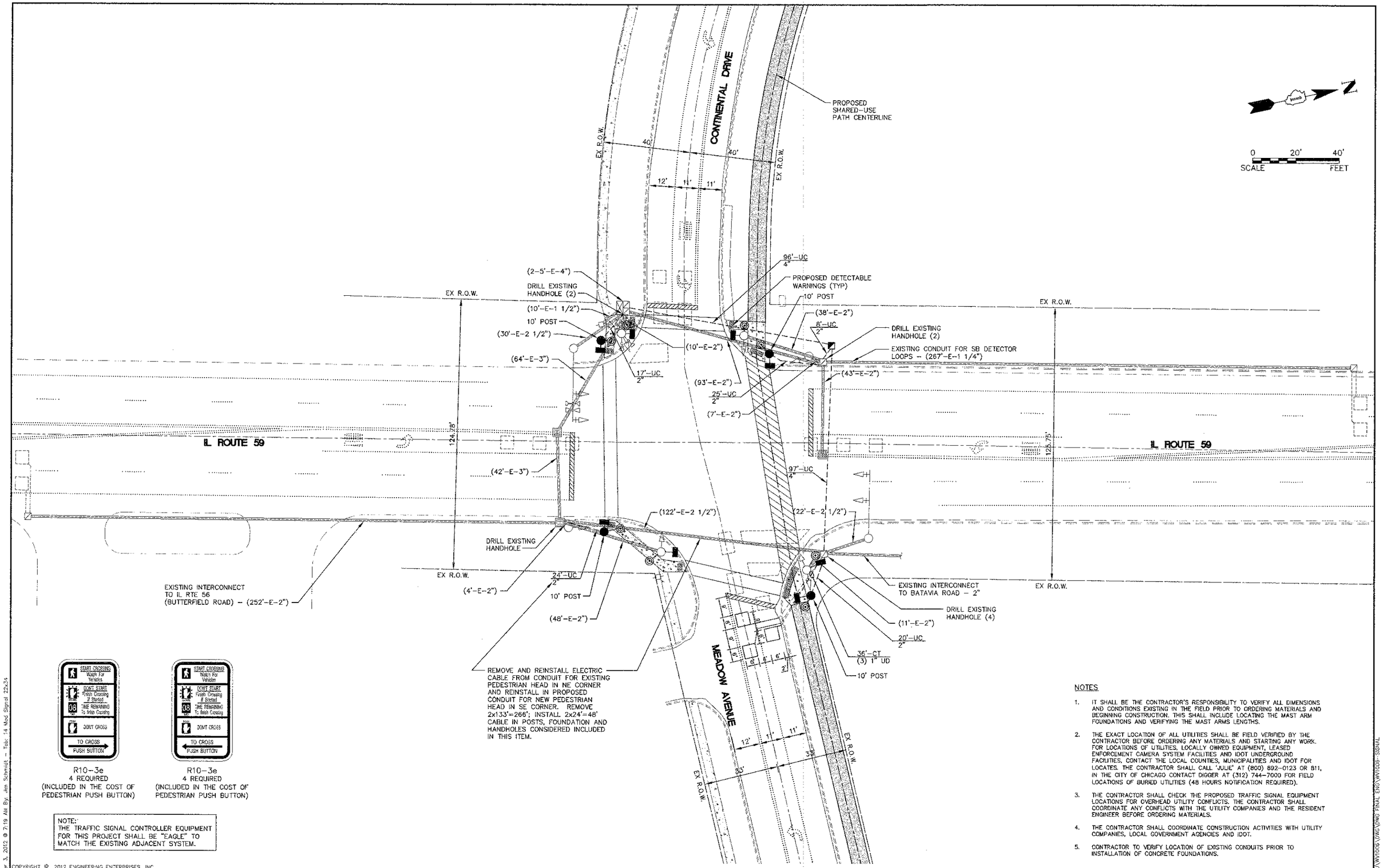
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN
IL ROUTE 59 AT CONTINENTAL DRIVE / MEADOW AVENUE

SCALE: 1"=20' | SHEET NO. 1 OF 3 SHEETS | STA. N/A | TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	13
		CONTRACT NO. 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-9003(841)		

Plotted: October 11, 2012 @ 10:09 AM By: Jim Schmidt - Tab: 13 Exist Signal 22x34

Path: H:\SDS\PROJ\WV1006\DWG\DWG FINAL ENG\WV1006-SIGNAL



R10-3e
4 REQUIRED
(INCLUDED IN THE COST OF PEDESTRIAN PUSH BUTTON)

R10-3e
4 REQUIRED
(INCLUDED IN THE COST OF PEDESTRIAN PUSH BUTTON)

NOTE:
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "EAGLE" TO MATCH THE EXISTING ADJACENT SYSTEM.

NOTES

1. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARMS LENGTHS.
2. THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES. THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811, IN THE CITY OF CHICAGO CONTACT DIGGER AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION REQUIRED).
3. THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.
4. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LOCAL GOVERNMENT AGENCIES AND IDOT.
5. CONTRACTOR TO VERIFY LOCATION OF EXISTING CONDUITS PRIOR TO INSTALLATION OF CONCRETE FOUNDATIONS.

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PLOT SCALE =	CHECKED - JRL	REVISED -	
PLOT DATE =	DATE - 07/20/12	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL MODIFICATION PLAN
IL ROUTE 59 AT CONTINENTAL DRIVE / MEADOW AVENUE

SCALE: 1"=20' | SHEET NO. 2 OF 3 SHEETS | STA. N/A | TO STA. N/A

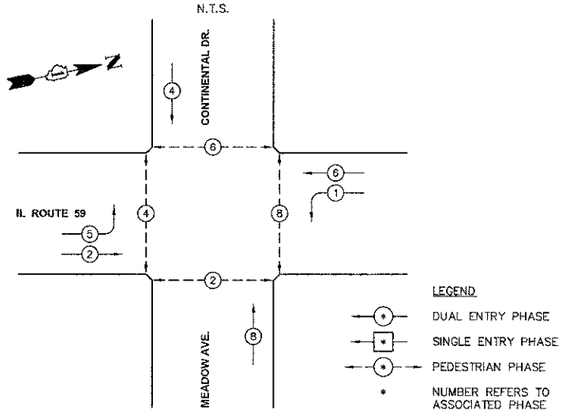
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	14
		CONTRACT NO. 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-9003(841)		

Plotted: December 3, 2012 @ 7:19 AM By: Jim Schmidt - Tab: 14 Mod Signal 22x34

PATH: I:\SSS\PROJ\WY1006\DWG\DWG FINAL ENG\WY1006-SIGNAL

CONTROLLER SEQUENCE

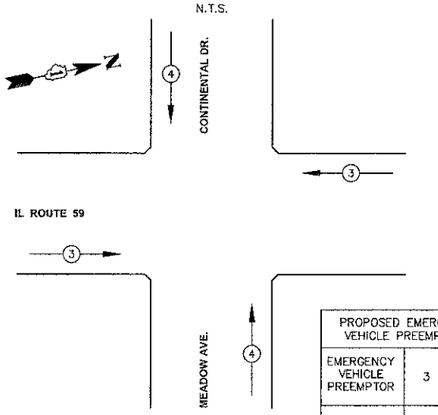
N.T.S.



PHASE DESIGNATION DIAGRAM

EMERGENCY VEHICLE PREEMPTION SEQUENCE

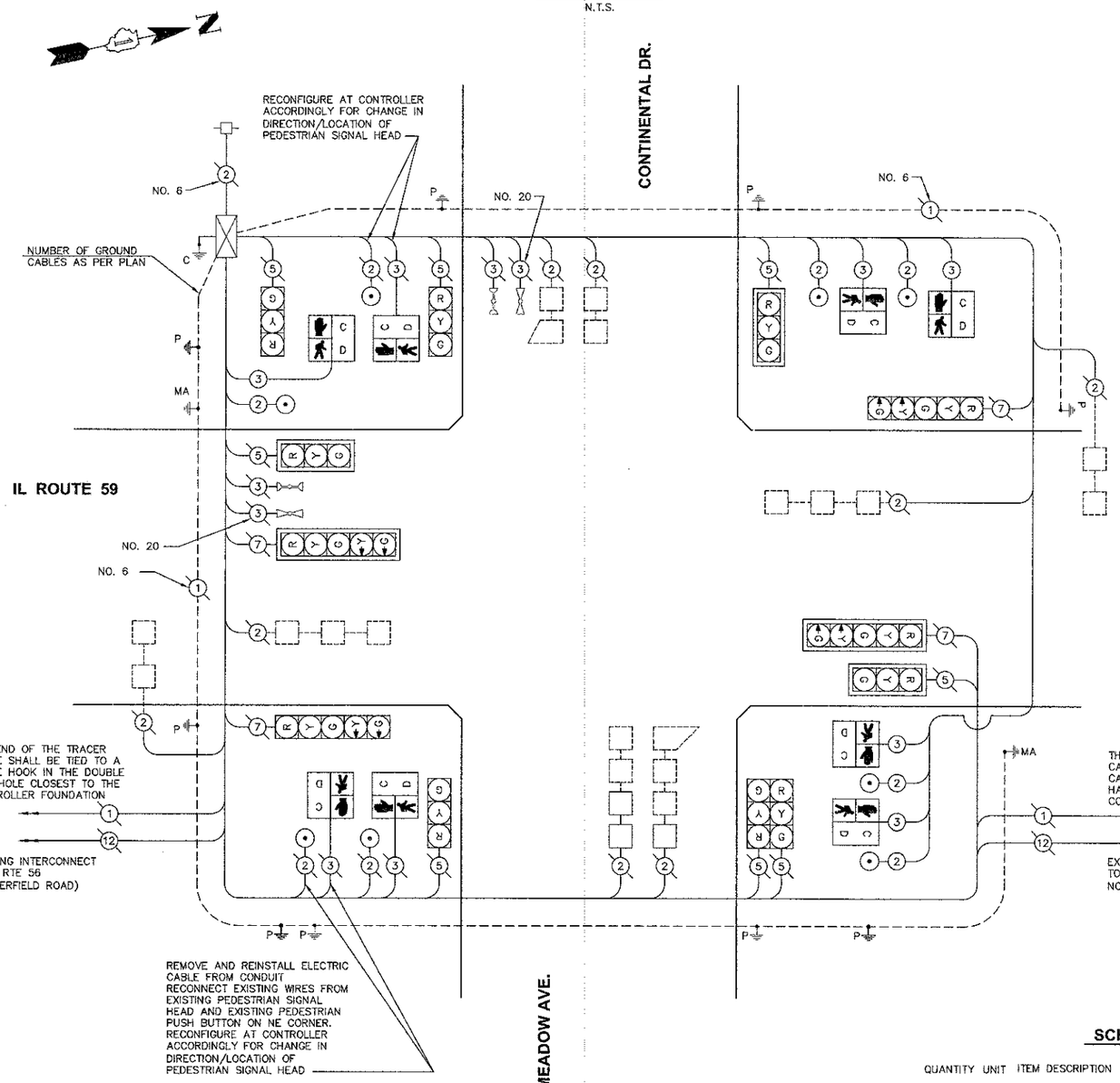
N.T.S.



PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT		

CABLE PLAN

N.T.S.



SCHEDULE OF QUANTITIES

QUANTITY	UNIT	ITEM DESCRIPTION
94	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.
193	FOOT	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.
1	EACH	HANDHOLE
1	EACH	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION
925	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
951	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
76	FOOT	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C
4	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 10 FT.
16	FOOT	CONCRETE FOUNDATION, TYPE A
9	EACH	DRILL EXISTING HANDHOLE
8	EACH	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
105	FOOT	DETECTOR LOOP, TYPE I
8	EACH	PEDESTRIAN PUSH-BUTTON
1	EACH	MODIFY EXISTING CONTROLLER
266	FOOT	REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT

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	DRAWN - CLN	REVISED - TVW/JPS 11/30/12	
PLOT SCALE =	CHECKED - JRL	REVISED -	
PLOT DATE =	DATE - 07/20/12	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES, CABLE PLAN, PHASE DESIGNATION DIAGRAM, AND EMERGENCY VEHICLE PREEMPTION SEQUENCE
IL ROUTE 59 AT CONTINENTAL DRIVE / MEADOW AVENUE

SCALE: 1"=20' | SHEET NO. 3 OF 3 SHEETS | STA. N/A | TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	15
		CONTRACT NO. 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-9003(841)		

Plotted: January 15, 2013 @ 1:10 PM By: Kris Pung — Tab: 15 Cable Plan 22x34

Path: I:\SIGNAL\PROJ\WHEATON\DWG\DWG FINAL ENG\WHTO08-SIGNAL

LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.

2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.

3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.

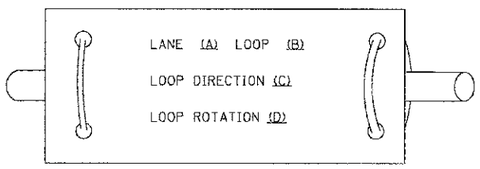
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.

5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.

6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.

7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

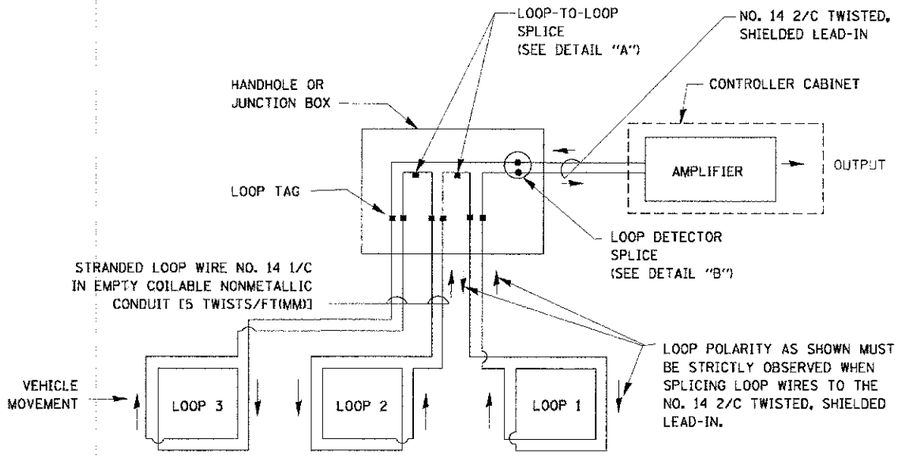


A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY

B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.

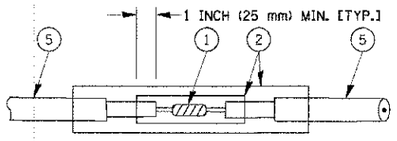
C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".

D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

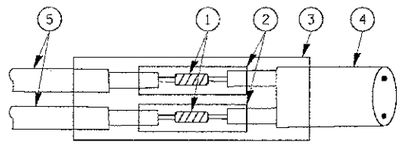


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

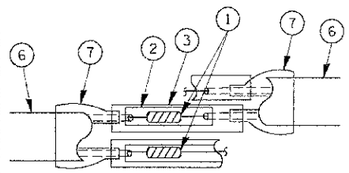


DETAIL "A"
LOOP-TO-LOOP SPLICE



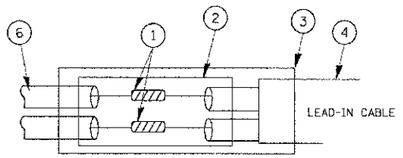
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE

PRE-FORMED LOOP



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

(1) WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.

(2) WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.

(3) WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.

(4) NO. 14 2/C TWISTED, SHIELDED CABLE.

(5) LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

(6) PRE-FORMED LOOP

(7) XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

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		DRAWN - BCK	REVISED -
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - DAD	REVISED -
	PLOT DATE = 11/4/2009	DATE - 10-28-09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET NO. 1 OF 6 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	16
	TS-05	CONTRACT NO. 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-9003(841)		

Plotted: October 11, 2012 @ 10:06 AM By: Jim Schmidt - Top: 16 (TS-05A) 22x34

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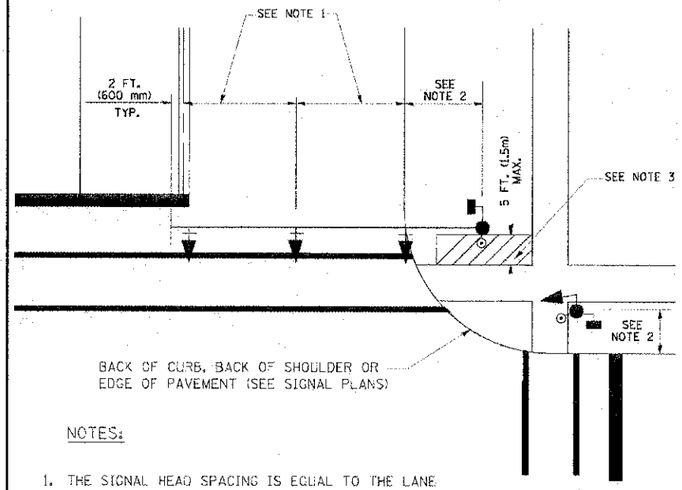
TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

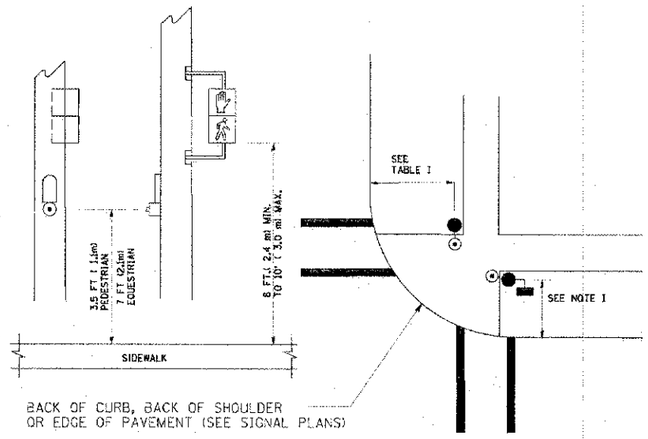
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

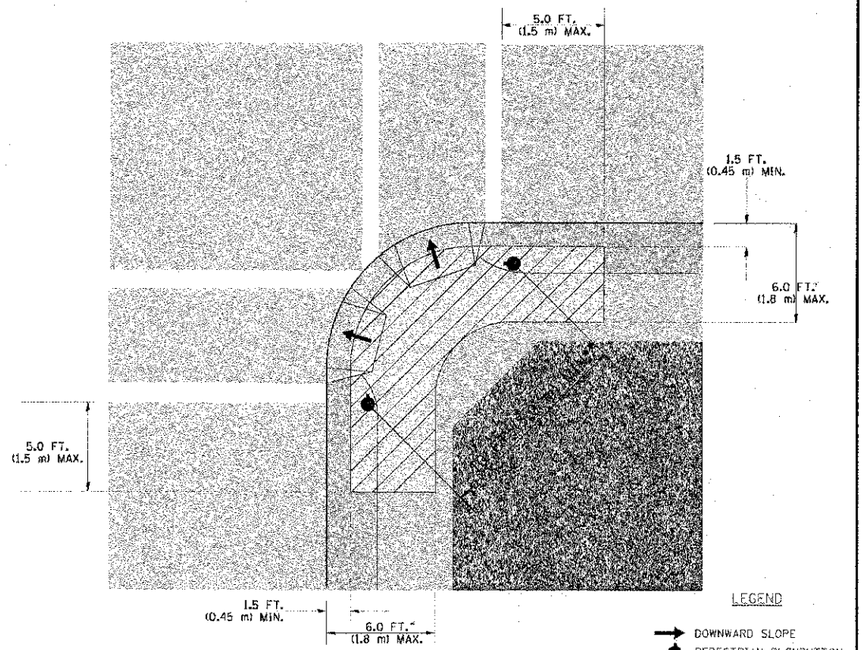
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



* WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.

** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

FILE NAME =	USER NAME =	DESIGNED - DAD	REVISED -
		DRAWN - BCK	REVISED -
	PLOT SCALE =	CHECKED - DAD	REVISED -
	PLOT DATE =	DATE - 10-26-09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET NO. 2 OF 6 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	17
	TS-05	CONTRACT NO. 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-9003(841)		

Plotted: October 11, 2012 @ 10:07 AM By: Jim Schmidt - Tab: 17 (TS-05B) 22x34

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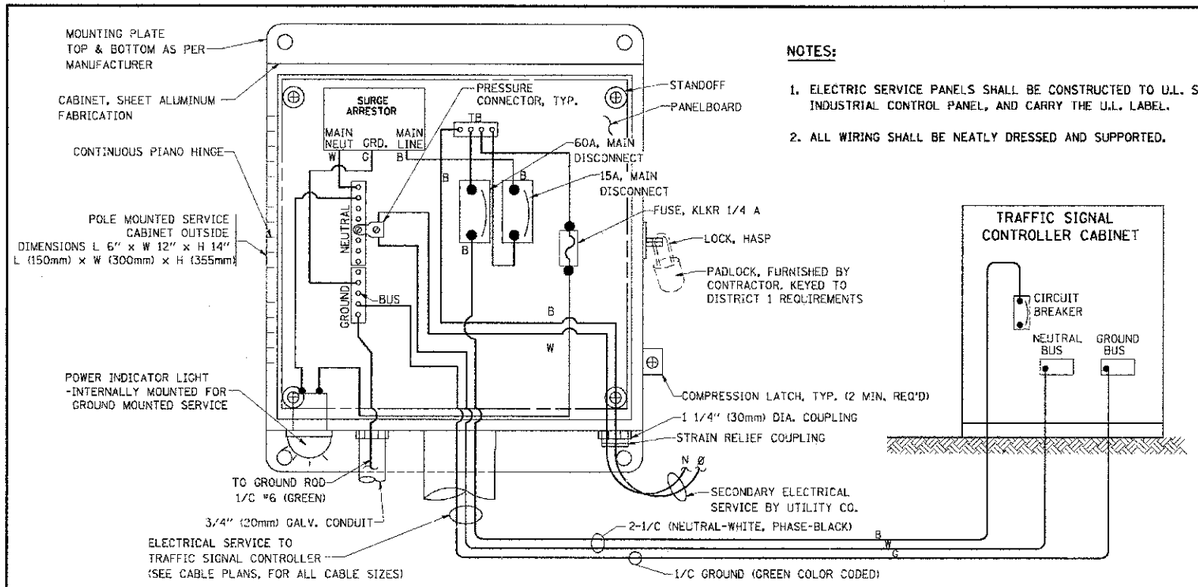


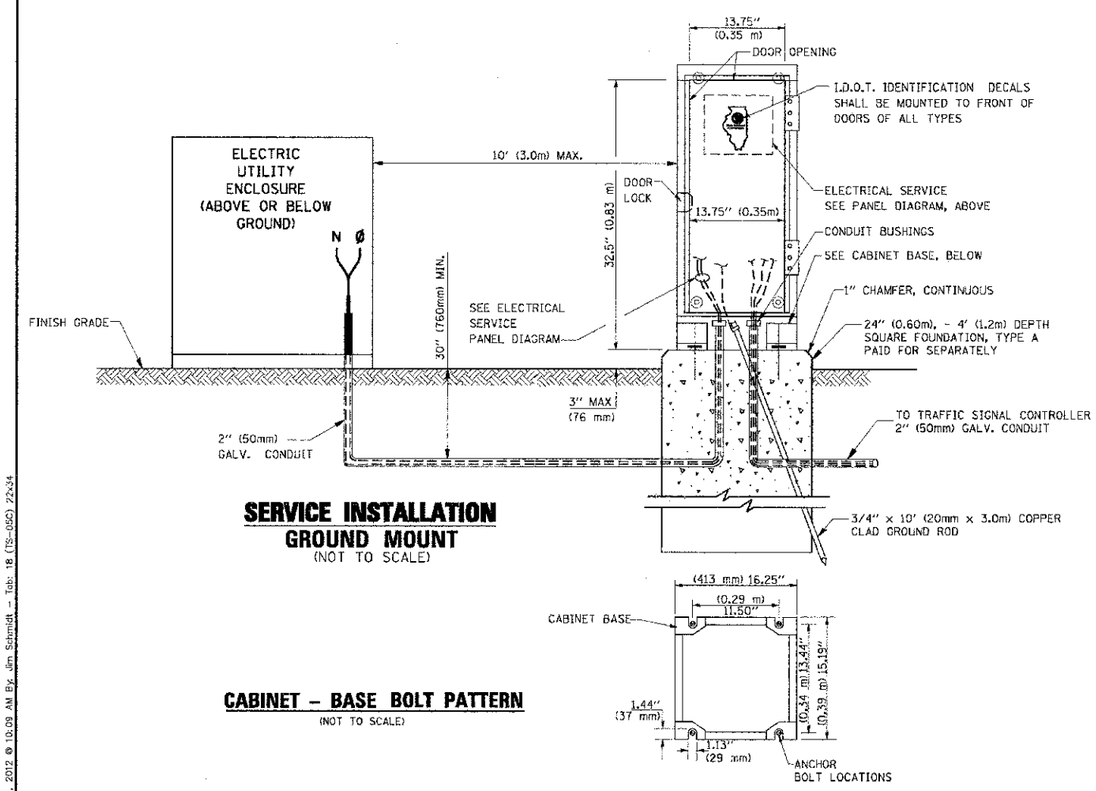
NOTES:

1. ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508, INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL.
2. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.

ELECTRICAL SERVICE – PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)

SERVICE INSTALLATION POLE MOUNT (SHOWN)

(NOT TO SCALE)

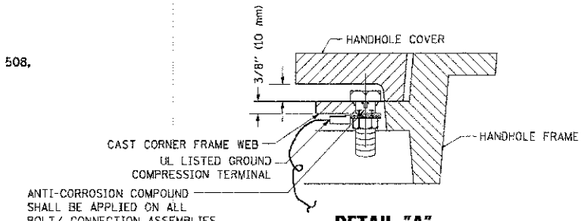


SERVICE INSTALLATION GROUND MOUNT

(NOT TO SCALE)

CABINET – BASE BOLT PATTERN

(NOT TO SCALE)



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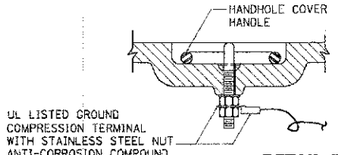

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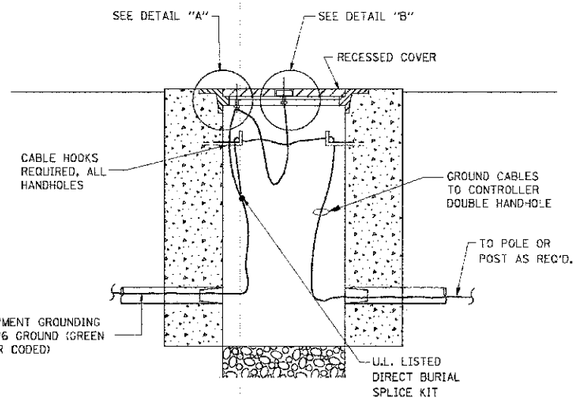
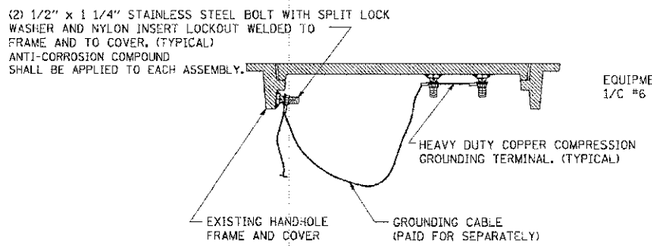


HANDHOLE COVER & FRAME – GROUNDING DETAIL

(NOT TO SCALE)



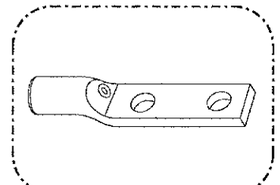
EXISTING HANDHOLE COVER & FRAME – GROUNDING DETAIL

(NOT TO SCALE)

NOTES:

GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.

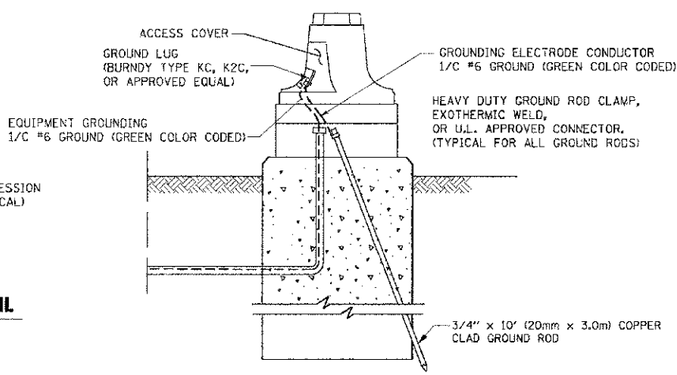


HEAVY-DUTY COMPRESSION TERMINAL (BURNDY TYPE YGHA OR APPROVED EQUAL)

¾" (20mm) HEAVY-DUTY GROUND ROD CLAMP (BURNDY TYPE GRC OR APPROVED EQUAL)

NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES
  6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES
  13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES.
  5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



MAST ARM POLE / POST-GROUNDING DETAIL

(NOT TO SCALE)

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		DRAWN - BCK	REVISED -
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - DAD	REVISED -
	PLOT DATE = 11/4/2009	DATE - 10-28-09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET NO. 3 OF 6 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	18
	TS-05	CONTRACT NO. 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-9003(841)		

Plotted: October 11, 2012 @ 10:09 AM By: Jim Schmidt - Tab: 18 (TS-05C) 22x34

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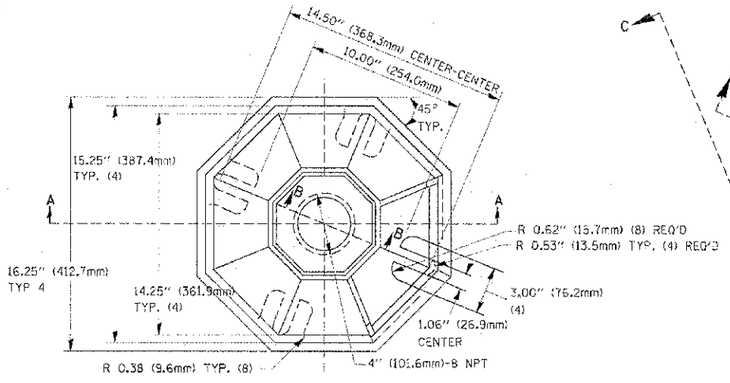
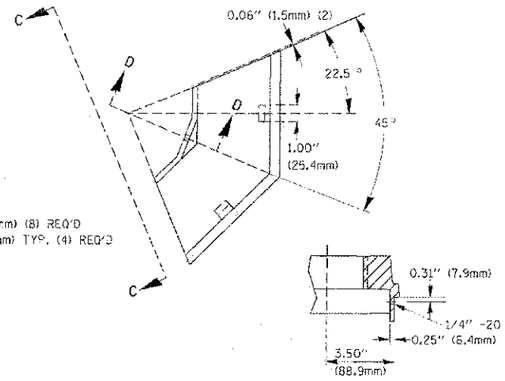
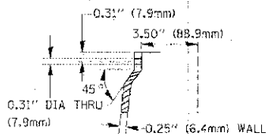


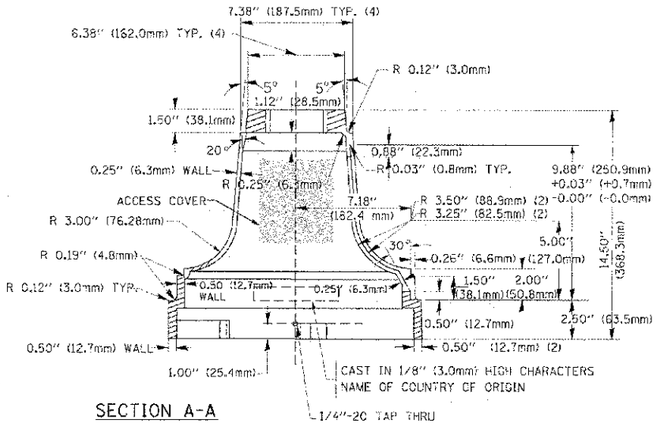
TOP VIEW



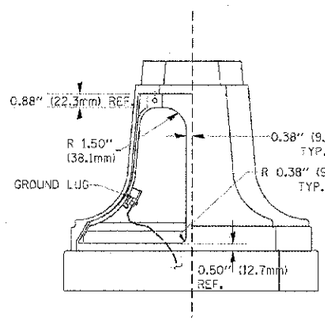
SECTION B-B



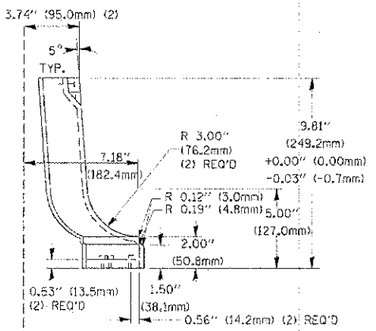
SECTION D-D



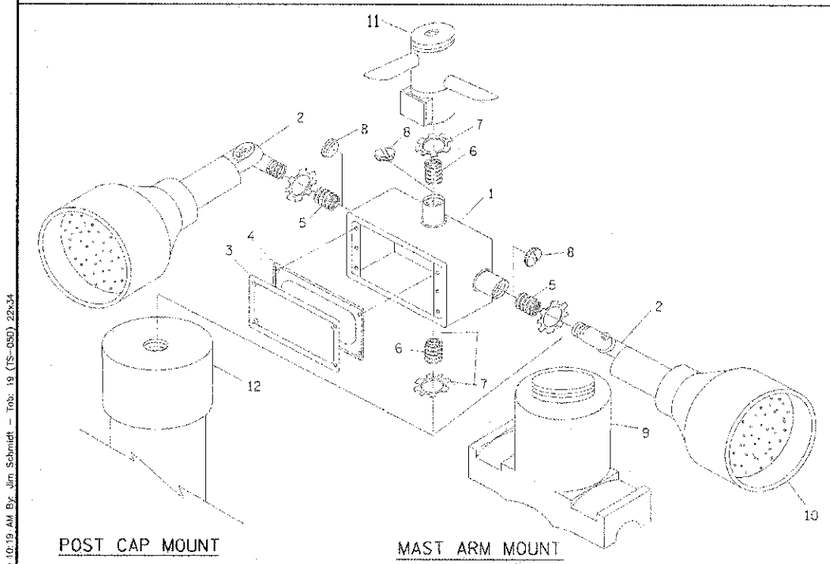
SECTION A-A



VIEW C-C



TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A



POST CAP MOUNT

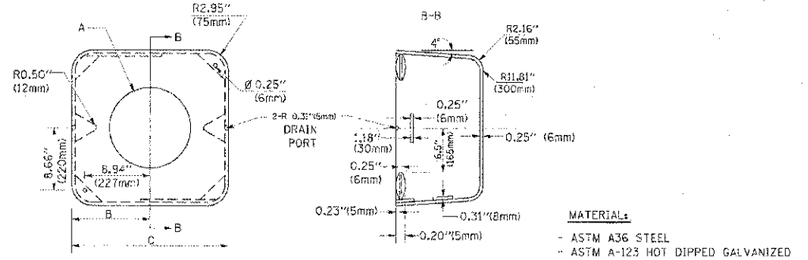
MAST ARM MOUNT

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	¾"(19 mm) CLOSE NIPPLE
7	¾"(19 mm) LOCKNUT
8	¾"(19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP (18 FT. (5.4 m) POST MIN.)

NOTES:

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
   ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
   ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A ¾"(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.

EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL



MATERIAL:
- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

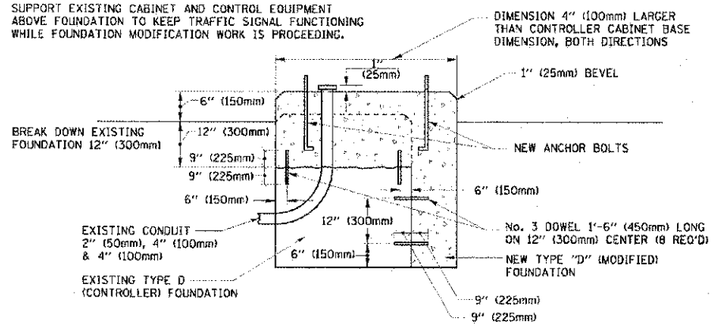
A	B	C	HEIGHT	WEIGHT
VARIES	9.5"(241mm)	19"(483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75"(273mm)	21.5"(546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0"(330mm)	26"(660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	16.5"(470mm)	37"(940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

SHROUD

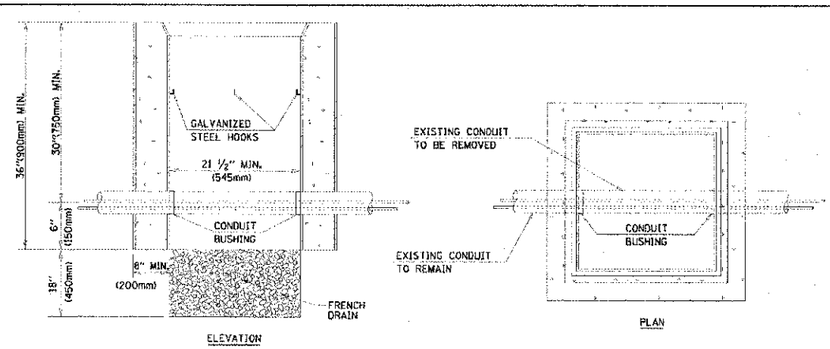
NOTES:

1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:
SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



NOTES:

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCIDENTAL TO THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

FILE NAME =	USER NAME =	DESIGNED - DAD	REVISED -
		DRAWN - BCK	REVISED -
	PLOT SCALE =	CHECKED - DAD	REVISED -
	PLOT DATE =	DATE - 10-26-09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

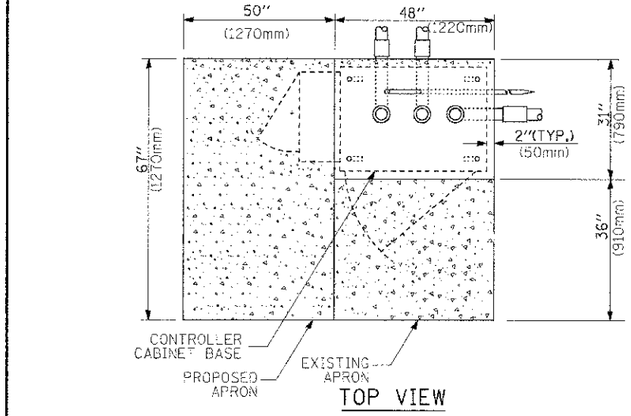
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET NO. 4 OF 6 SHEETS | STA. | TO STA.

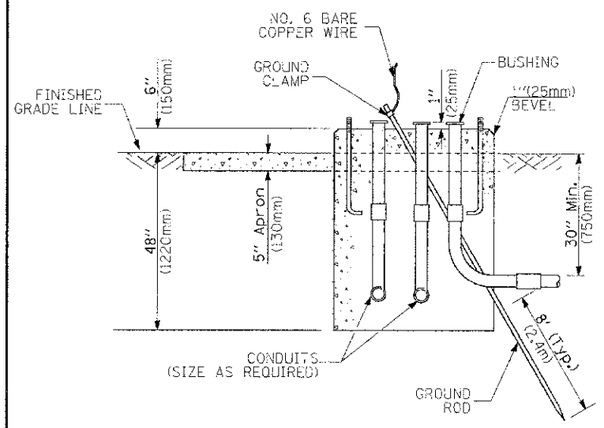
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-ST	DUPAGE	26	19
	TS-05	CONTRACT NO. 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-9003(841)		

Plotted: October 11, 2012 @ 10:19 AM By: Jim Schmidt - Tab: 19 (TS-050) 22x34

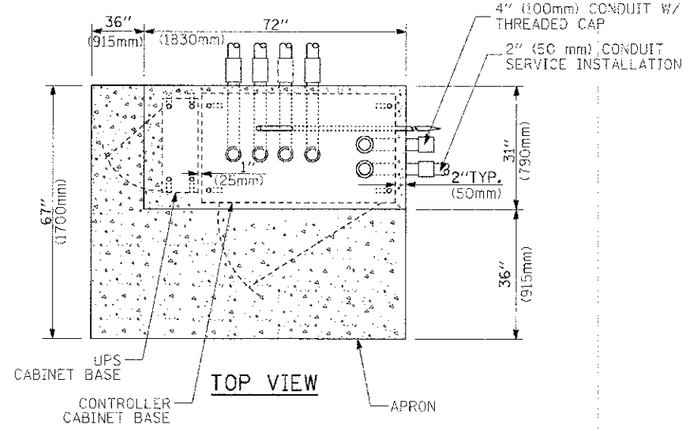
Path: H:\SSKPROJ\VP1008\DWG\DWG FINAL ENG\VP1008-DETAILS



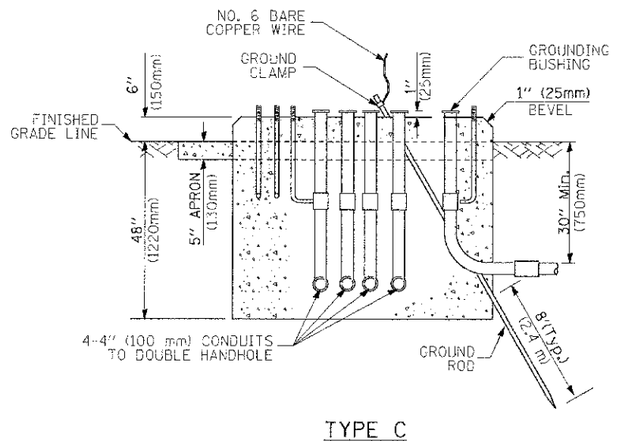
TOP VIEW



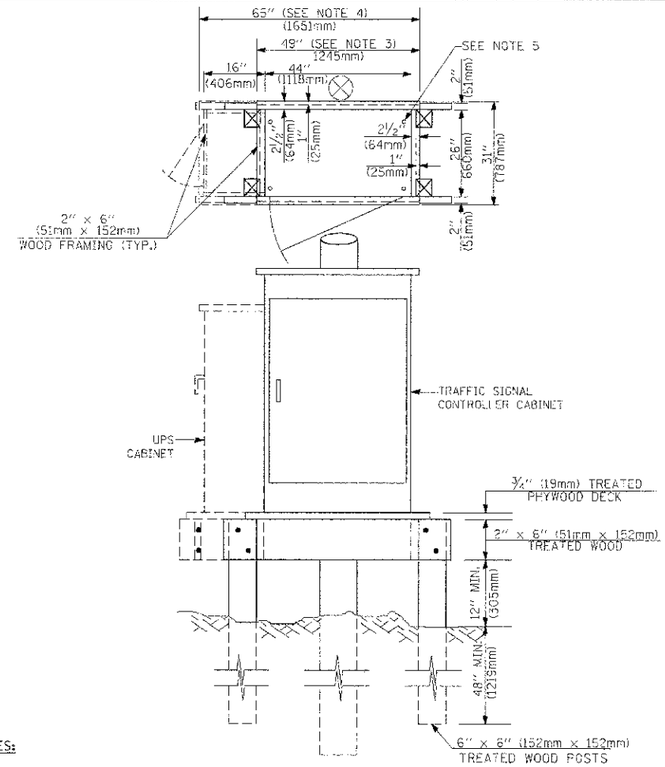
TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET



TOP VIEW



TYPE C
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001.

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

FILE NAME =	USER NAME = bauerdl	DESIGNED - DAG	REVISED -
c:\pw_work\PWIDOT\BAUERDL\d0108315\ts05.dgn		DRAWN - BCK	REVISED -
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - DAD	REVISED -
	PLOT DATE = 11/4/2009	DATE - 10-28-09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET NO. 5 OF 6 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	20
	TS-05	CONTRACT NO. 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-9003(841)		

Plotted: October 11, 2012 @ 10:21 AM By: Jim Schmidt - Tab: 20 (TS-05D) 22x34

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TRAFFIC SIGNAL LEGEND

ITEM	REMOVAL	EXISTING	PROPOSED
CONTROLLER CABINET	R		
RAILROAD CONTROL CABINET			
COMMUNICATIONS CABINET	R CC	CC	CC
MASTER CONTROLLER		EMC	MC
MASTER MASTER CONTROLLER		EMMC	MMC
UNINTERRUPTIBLE POWER SUPPLY	R UPS	UPS	UPS
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT	R	P	P
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT	R T	P T	P T
STEEL MAST ARM ASSEMBLY AND POLE	R		
ALUMINUM MAST ARM ASSEMBLY AND POLE	R		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE	R		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA	R PTZ	PTZ	PTZ
SIGNAL POST	R		
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM	R		
GUY WIRE	R		
SIGNAL HEAD	R		
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)			2
SIGNAL HEAD WITH BACKPLATE	R		
SIGNAL HEAD OPTICALLY PROGRAMMED	R "P"	"P"	"P"
FLASHER INSTALLATION (S DENOTES SOLAR POWER)	R "F"	"F"	"F"
PEDESTRIAN SIGNAL HEAD	P		
PEDESTRIAN PUSHBUTTON DETECTOR	R		
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR	R APS	APS	APS
ILLUMINATED SIGN "NO LEFT TURN"	R		
ILLUMINATED SIGN "NO RIGHT TURN"	R		
DETECTOR LOOP, TYPE I			
PREFORMED DETECTOR LOOP		P	P
MICROWAVE VEHICLE SENSOR	R M	M	M
VIDEO DETECTION CAMERA	R V	V	V
VIDEO DETECTION ZONE			
PAN, TILT, ZOOM CAMERA	R PTZ	PTZ	PTZ
WIRELESS DETECTOR SENSOR	R W	W	W
WIRELESS ACCESS POINT	R		

ITEM	REMOVAL	EXISTING	PROPOSED
EMERGENCY VEHICLE LIGHT DETECTOR	R		
CONFIRMATION BEACON	R		
HANDHOLE	R		
HEAVY DUTY HANDHOLE	R H	H	H
DOUBLE HANDHOLE	R		
JUNCTION BOX	R		
GALVANIZED STEEL CONDUIT IN TRENCH (T) OR PUSHED (P)			
TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE	R		
COMMON TRENCH			CT
COILABLE NONMETALLIC CONDUIT (EMPTY)			CNC
SYSTEM ITEM		S	S
INTERSECTION ITEM		I	IP
REMOVE ITEM	R		
RELOCATE ITEM	RL		
ABANDON ITEM	A		
12" (300mm) TRAFFIC SIGNAL SECTION		R	R
12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE		R Y G	
SIGNAL FACE		R Y G Y G	R Y G Y G
SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD		R Y G Y G "P"	R Y G Y G "P"
12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL		DW W	
12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED			
12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID			
PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER		C D	C D
RADIO INTERCONNECT	R		
RADIO REPEATER	R ERR	ERR	RR
DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED		5	5
GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)		1	1

ITEM	REMOVAL	EXISTING	PROPOSED
ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE		1	1
COAXIAL CABLE		C	C
VENDOR CABLE FOR CAMERA		V	V
COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED		6	6
FIBER OPTIC CABLE NO. 62.5/125, MM12F		12F	
FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F		24F	24F
FIBER OPTIC CABLE NO. 62.5/125, (NUMBER OF FIBERS & TYPE TO BE NOTED ON PLANS)			
GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE		G	G
CONTROLLER CABINET AND FOUNDATION TO BE REMOVED	RCF		
STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED	RMF		
ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED	RMF		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED	RMF		
SIGNAL POST AND FOUNDATION TO BE REMOVED	RMF		
INTERSECTION & SAMPLING (SYSTEM) DETECTOR		IS	IS
SAMPLING (SYSTEM) DETECTOR		S	S
EXISTING INTERSECTION LOOP DETECTOR PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR		P	
EXISTING PREFORMED INTERSECTION LOOP DETECTOR PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR		PP	
PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR		PIS	PIS
PREFORMED SAMPLING (SYSTEM) DETECTOR		PS	PS

RAILROAD SYMBOLS

	EXISTING	PROPOSED
RAILROAD CONTROL CABINET		
RAILROAD CANTILEVER MAST ARM		
FLASHING SIGNAL		
CROSSING GATE		
CROSSBUCK		

FILE NAME = c:\pw_work\PWIDOT\DAUER5L\d0800315\ts05.dgn	USER NAME = beuerdl	DESIGNED - DAG/BCK	REVISED -
		DRAWN - BCK	REVISED -
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - DAD	REVISED -
	PLOT DATE = 11/4/2009	DATE - 10-28-09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

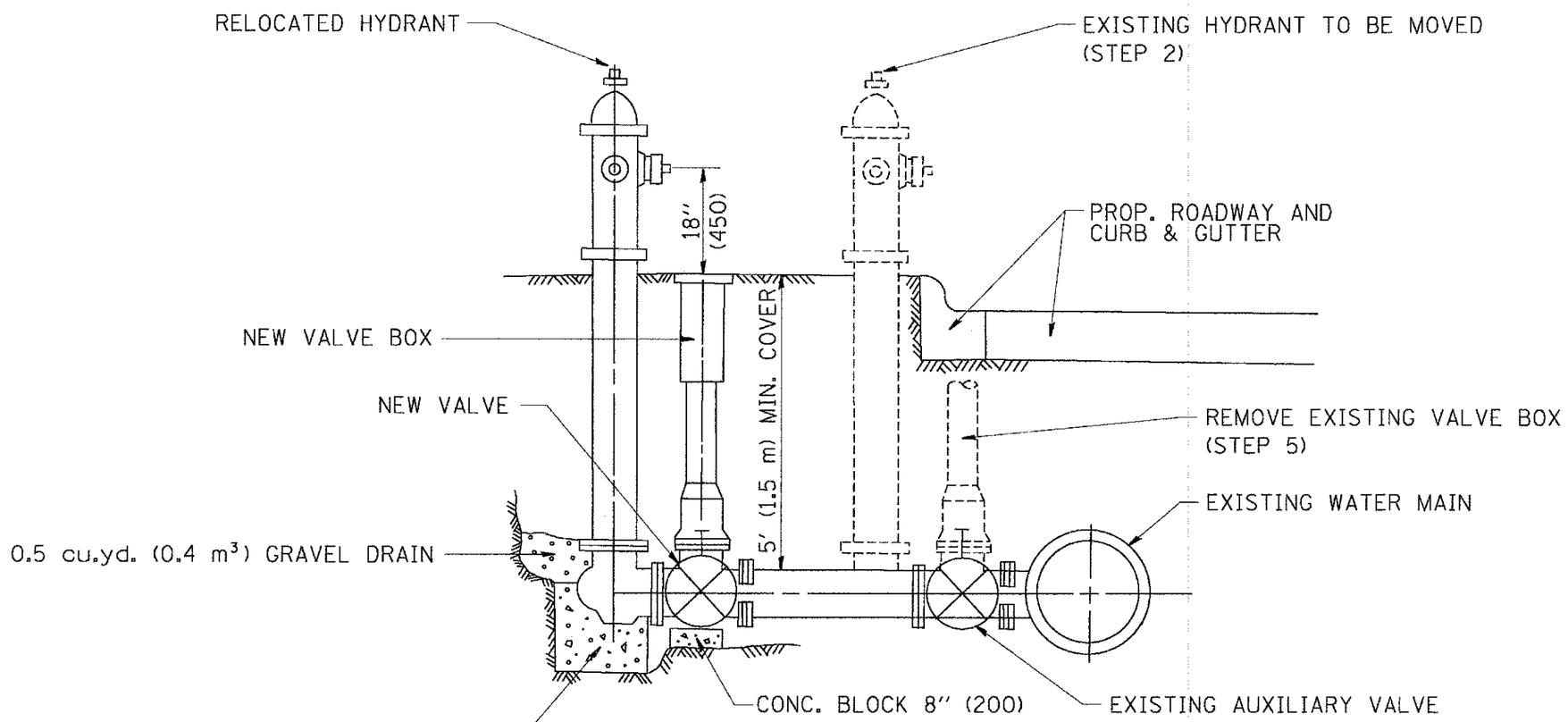
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET NO. 6 OF 6 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	21
	TS-05	CONTRACT NO. 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-9003(841)		

Plotted: October 11, 2012 @ 10:22 AM By: Jim Schmidt - Tab: 21 (TS-05F) 22x34

Path: P:\SIGPROJ\WV1005\DWG\PNG FINAL ENG\WV1008-DETAILS



SEQUENCE OF CONSTRUCTION:

1. CLOSE EXISTING VALVE.
2. REMOVE EXISTING HYDRANT.
3. INSTALL HYDRANT EXTENSION AND NEW VALVE.
4. RELOCATE EXISTING HYDRANT.
5. OPEN EXISTING VALVE, REMOVE BOX.
6. BACKFILL.
7. FLUSH AND TEST FOR CHLORIDE RESIDUAL AND PROVIDE TEST.

ALL WORK TO BE DONE IN ACCORDANCE WITH ARTICLE 564 OF THE STANDARD SPECIFICATIONS. NEW VALVE AND BOX SHALL BE SAME MAKE AND MODEL AS EXISTING.

FIRE HYDRANT TO BE MOVED

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\dist std\22x34\bd36.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED - R. SHAH 09-09-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FIRE HYDRANT TO BE MOVED
		DRAWN -	REVISED - R. SHAH 10-25-94		
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED -		SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.
	PLOT DATE = 1/4/2008	DATE -	REVISED -		

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	22
	BD-36	CONTRACT NO. 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-9003(841)		

Plotted: October 11, 2012 @ 10:23 AM By: Jim Schmidt — Tab: 22 (BD-36) 22x34

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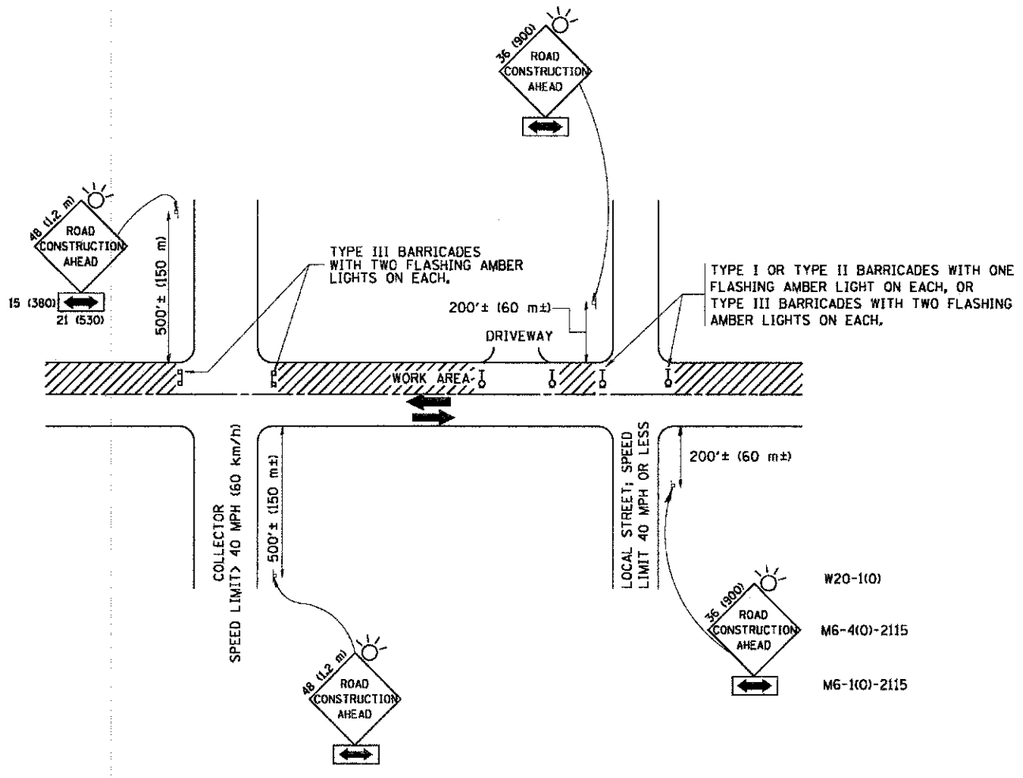

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = W:\d:stctd\22x34\tc10.dgn	USER NAME = gaglianobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.800 ' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED -T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	23
	TC-10	**CONTRACT NO.** 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-3003(841)		

Plotted: October 11, 2012 @ 10:24 AM By: Jim Schmidt - Tab: 23 (TC-10) 22x34

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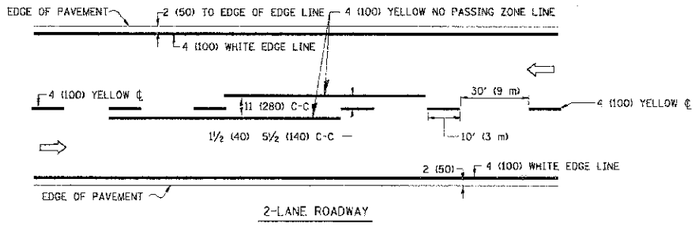
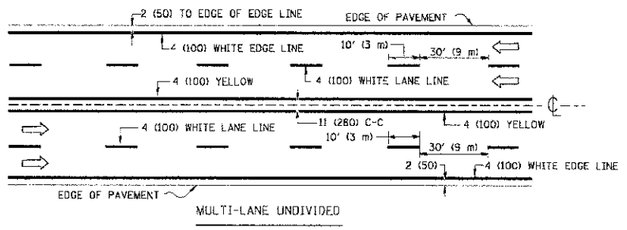
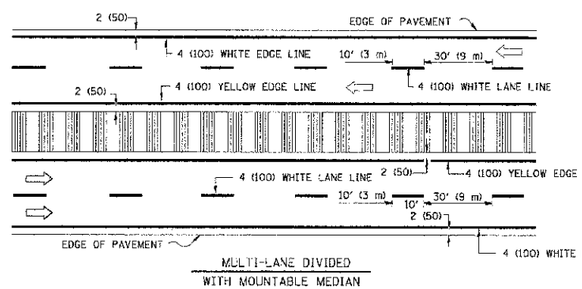


2-LANE ROADWAY



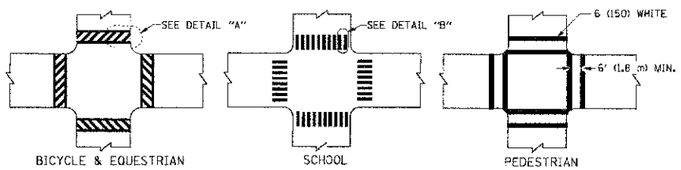
MULTI-LANE UNDIVIDED



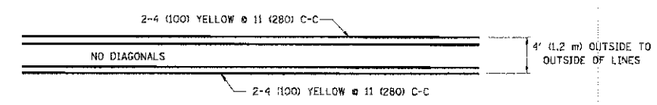
MULTI-LANE DIVIDED
WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

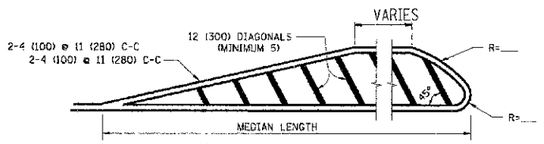
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



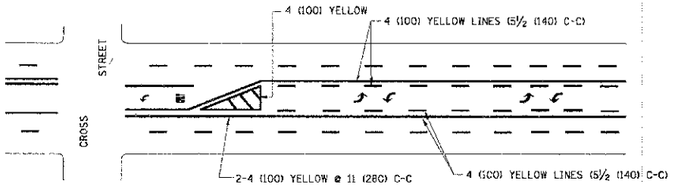
4' (1.2 m) WIDE MEDIANS ONLY



FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

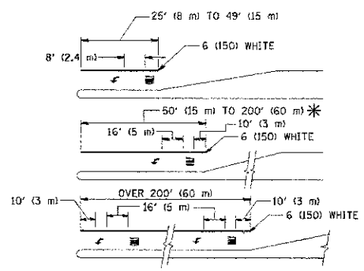
MEDIANS OVER 4' (1.2 m) WIDE



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED. WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

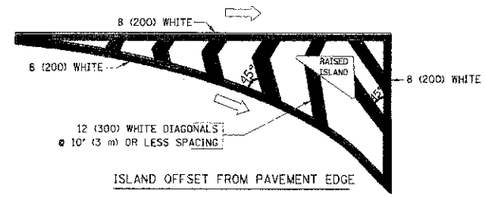


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

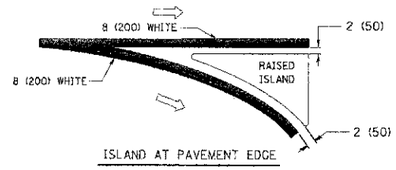
✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = c:\pw_work\pwidot\drivevaegn\d0108315\tc13.dgn	USER NAME = drivevaegn	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
		DRAWN -	REVISED - C. JUCIUS 09-09-09
	PLOT SCALE = 50.002 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

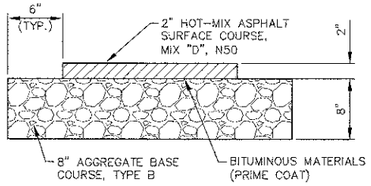
DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

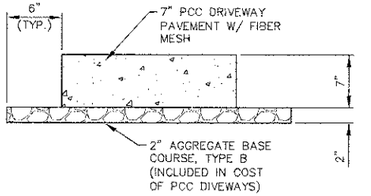
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	24
	TC-13	CONTRACT NO. 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-9003(841)		

Plotted: October 11, 2012 @ 10:26 AM By: Jim Schmidt — Tab: 24 (TC-13) 22x34

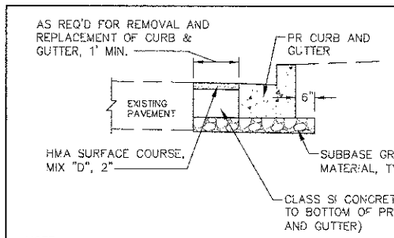
Path: H:\SDSRPROJ\WY1008\DWG\PWS FINAL ENG\WY1008-DETAILS



STABILIZED DRIVEWAYS, 10" (PE)
(N.T.S.)

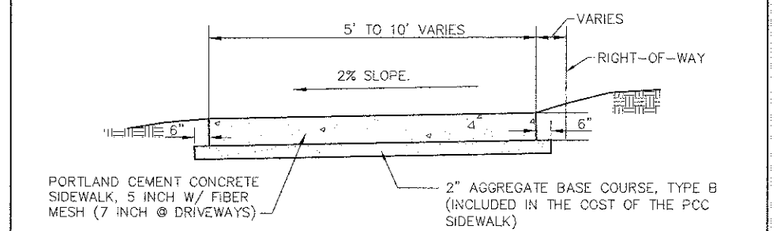


PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH
(N.T.S.)

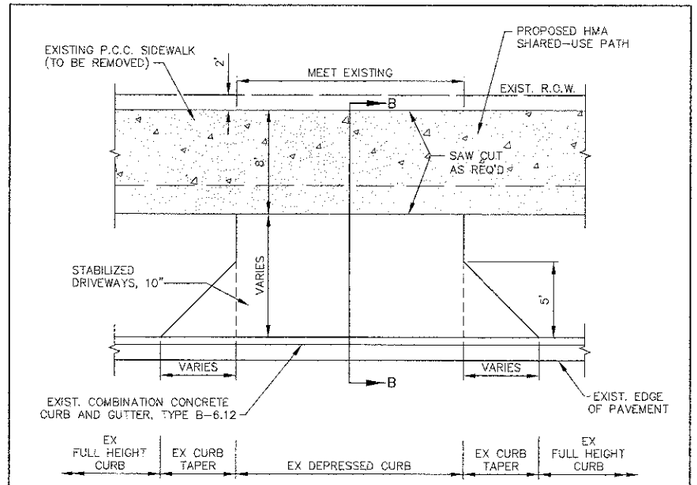


NOTE:
THE COST OF THE CLASS SI CONCRETE, SUBBASE GRANULAR MATERIAL, TYPE B, 4", AND HMA SURFACE COURSE, MIX "D", 2", SHALL BE CONSIDERED INCLUDED IN THE COST OF COMBINATION CONCRETE CURB & GUTTER, OF THE TYPE AND SIZE SPECIFIED

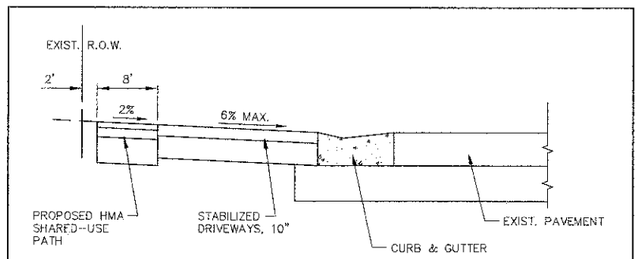
CONCRETE BACKFILL ADJACENT TO PROPOSED CURB AND GUTTER
(N.T.S.)



TYPICAL SIDEWALK SECTION
(N.T.S.)

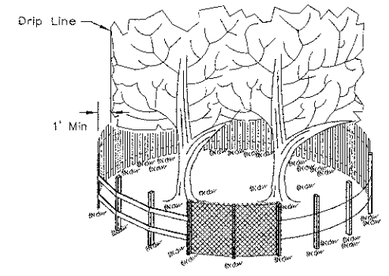


TYPICAL PRIVATE ENTRANCE (PE)
(N.T.S.)

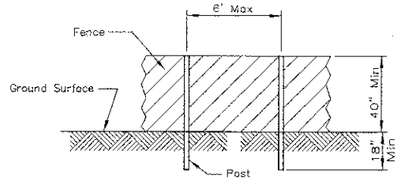


SECTION B – B
(N.T.S.)

TREE PROTECTION – FENCING

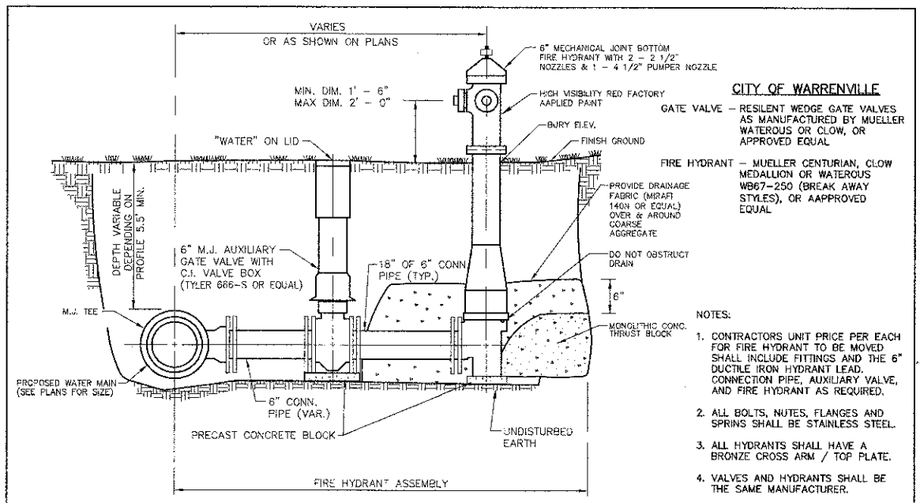


SIDE VIEW



POST AND FENCE DETAIL

NOTES:
1. The fence shall be located a minimum of 1 foot outside the drip line of the tree to be saved and in no case closer than 5 feet to the trunk of any tree.
2. Fence posts shall be either standard steel posts or wood posts with a minumum cross sectional area of 3.0 sq. in.
3. The fence may be either 40" high snow fence, 40" plastic web fencing or any other material as approved by the engineer/inspector.



CITY OF WARRENVILLE

GATE VALVE – RESILENT WEDGE GATE VALVES AS MANUFACTURED BY MUELLER WATEROUS OR CLOW, OR APPROVED EQUAL

FIRE HYDRANT – MUELLER CENTURIAN, CLOW MEDALLION OR WATEROUS WB67-250 (BREAK AWAY STYLES), OR AAPPROVED EQUAL

NOTES:
1. CONTRACTORS UNIT PRICE PER EACH FOR FIRE HYDRANT TO BE MOVED SHALL INCLUDE FITTINGS AND THE 6" DUCTILE IRON HYDRANT LEAD, CONNECTION PIPE, AUXILIARY VALVE, AND FIRE HYDRANT AS REQUIRED.
2. ALL BOLTS, NUTES, FLANGES AND SPRINS SHALL BE STAINLESS STEEL.
3. ALL HYDRANTS SHALL HAVE A BRONZE CROSS ARM / TOP PLATE.
4. VALVES AND HYDRANTS SHALL BE THE SAME MANUFACTURER.

FIRE HYDRANT ASSEMBLY INSTALLATION

Engineering Enterprises, Inc. 52 Wheeler Road Sugar Grove, Illinois 60554 Phone: (630) 466-9350	SCALE: N.T.S.	FOLDER: EE-WA	DRAWING NUMBER: HY-06	DRAWN BY: WCP
	DATE: 9/05/07			REVISED:

Engineering Enterprises, Inc.
CONSULTING ENGINEERS
52 Wheeler Road
Sugar Grove, Illinois 60554
630.466.6700 / www.eeiweb.com

USER NAME =	DESIGNED - SWM	REVISED -	---
	DRAWN - CLN	REVISED -	---
PLOT SCALE =	CHECKED - JRL	REVISED -	---
PLOT DATE =	DATE - 07/20/12	REVISED -	---

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SPECIAL DETAILS

SCALE: N/A | SHEET NO. 1 OF 2 SHEETS | STA. N/A | TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	25
		CONTRACT NO. 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-9003(841)		

Plotted: October 11, 2012 @ 10:26 AM By: Jim Schmidt - Tab: 25 Sp Oct 22x34

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GENERAL NOTES

1. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE FOLLOWING SPECIFICATIONS, WHICH ARE HEREBY MADE A PART HEREOF:

   A. STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, ILLINOIS DEPARTMENT OF TRANSPORTATION.

   B. THE NATIONAL ELECTRICAL CODE.

   C. LOCAL CODES AND ORDINANCES.

2. THE CONTRACTOR SHALL SUBMIT SPECIFICATIONS, SHOP DRAWINGS, AND CATALOG CUTS FOR ALL LIGHTING ITEMS TO THE ENGINEER FOR REVIEW BEFORE ORDERING ANY MATERIALS.

3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING TOP OF CONCRETE FOUNDATION HEIGHTS WITH FINISHED GRADE ELEVATIONS.

4. ALL ELECTRIC CABLE SHALL BE FULLY PIGMENTED COLOR CODED AND TAGGED.

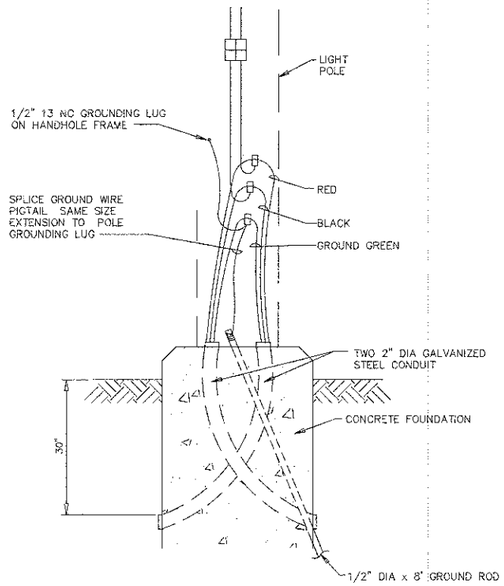
5. ALL ELECTRICAL EQUIPMENT AND MATERIAL SHALL BE UL LISTED AND LABELED.

6. POLE FUSES AND ASSOCIATED HARDWARE/MATERIAL AND INSTALLATION SHALL BE INCLUDED IN THE RELOCATE EXISTING LIGHTING UNIT PAY ITEM.

7. THE CONTRACTOR SHALL PERFORM ELECTRICAL TESTING IN THE PRESENCE OF THE ENGINEER. THE ELECTRICAL TESTING SHALL BE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.

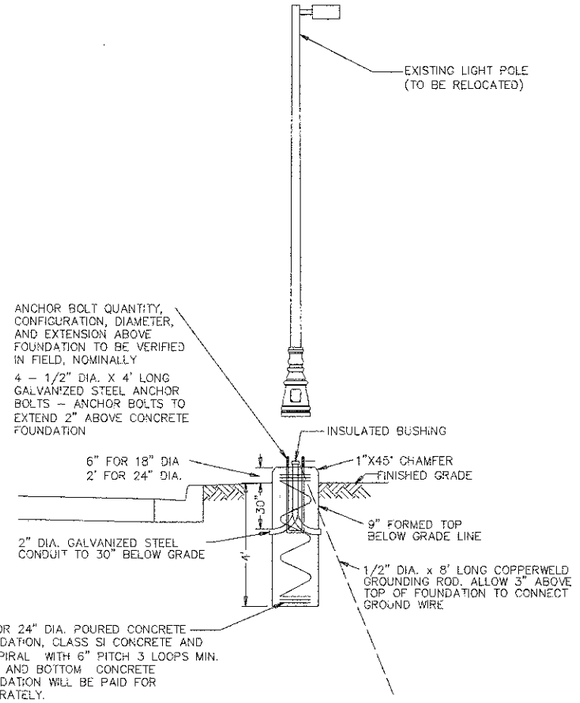
8. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO MARK THE PROPOSED LOCATIONS OF ALL LIGHTING POLES. THE EXACT LOCATION OF ALL SHALL BE CONFIRMED WITH THE ENGINEER PRIOR TO START OF WORK.

9. GROUND RODS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE ITEM IT IS GROUNDING.



LIGHT POLE WIRING DETAIL

N.T.S.



RELOCATE EXISTING LIGHTING UNIT DETAIL

NOTES:

VERIFY ALL POLE RELATED DIMENSIONS AND INFORMATION WITH EXISTING POLE TO BE RELOCATED PRIOR TO FABRICATION OF FOUNDATION.

MINIMUM COMPRESSIVE STRENGTH FOR CONCRETE SHALL BE 3500 PSI AT THE END OF 28 DAYS.

EXCAVATION OF THE POLE FOUNDATION SHALL BE MADE WITH AN AUGER 18" OR 24" IN DIAMETER.

THE ANCHOR BOLTS SHALL BE A TACK WELDED TYPE BOLT OR HOOK TYPE BOLT. COLD BENDING OF THE HOOK BOLT WILL NOT BE ALLOWED.

THE ANCHOR BOLTS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED IN THE FORM.

THE ENTIRE LENGTH OF THE ANCHOR BOLTS AS WELL AS THE NUTS AND WASHERS SHALL BE HOT DIP GALVANIZED IN ACCORDANCE WITH THE REQUIREMENTS OF ASTM DESIGNATION A153.

RACEWAYS SHALL PROJECT 1" ABOVE THE TOP OF THE FOUNDATION.

CONCRETE SHALL BE CLASS "SI". THE CONCRETE FOUNDATION MUST BE CURED FOR 10 DAYS BEFORE THE LIGHT STANDARD IS ERECTED.

THE CABLE TRENCH SHALL BE BACKFILLED AND FIRMLY COMPACTED BEFORE THE LIGHT IS ERECTED.

Engineering Enterprises, Inc.
CONSULTING ENGINEERS
52 Wheeler Road
Sugar Grove, Illinois 60554
630.466.6700 / www.eeiweb.com

USER NAME =	DESIGNED - SWM	REVISED - ---
	DRAWN - CLN	REVISED - ---
PLOT SCALE =	CHECKED - JRL	REVISED - ---
PLOT DATE =	DATE - 07/20/12	REVISED - ---

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SPECIAL DETAILS

SCALE: N/A | SHEET NO. 2 OF 2 SHEETS | STA. N/A | TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N/A	11-00032-00-BT	DUPAGE	26	26
		CONTRACT NO. 63760		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-9003(841)		