

DEPARTMENT OF TRANSPORTATION

DIVISION OF HIGHWAYS

**PROPOSED  
HIGHWAY PLANS**

FAP ROUTE 573 (US 30)  
SECTION (116VB-1-1BR)I

**BRIDGE REPAIRS  
DEKALB COUNTY**

C-93-024-13

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(116VB-1-1BR)I	DEKALB	21	1
		ILLINOIS	CONTRACT NO. 66C65	

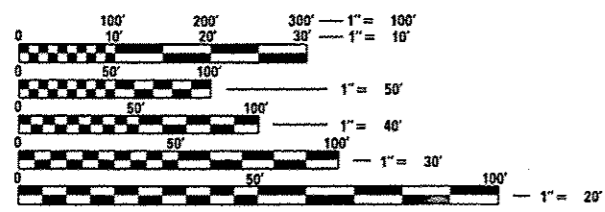
1. COVER SHEET
2. GENERAL NOTES
- 3-4. SUMMARY OF QUANTITIES
5. TYPICAL SECTIONS
6. STAGING DETAILS FOR STRUCTURE NO. 019-0032
- 7 - 21. STRUCTURE PLANS

HIGHWAY STANDARDS

- 000001-06 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 001001-02 AREAS OF REINFORCEMENT BARS
- 001006 DECIMAL OF AN INCH AND OF A FOOT
- 482006-03 HMA SHOULDER ADJACENT TO RIGID PAVEMENT
- 701001-02 OFF-ROAD OPERATIONS 2L, 2W, MORE THAN 15' (4.5 m) AWAY
- 701006-04 OFF-ROAD OPERATIONS 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
- 701011-03 OFF-ROAD MOVING OPERATIONS 2L, 2W, DAY ONLY
- 701201-04 LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS ≥ 45 MPH
- 701206-03 LANE CLOSURE, 2L, 2W, NIGHT ONLY, FOR SPEEDS ≥ 45 MPH
- 701301-04 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
- 701306-03 LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS ≥ 45 MPH
- 701311-03 LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
- 701321-13 LANE CLOSURE, 2L, 2W, BRIDGE REPAIR WITH BARRIER
- 701901-02 TRAFFIC CONTROL DEVICES
- 704001-07 TEMPORARY CONCRETE BARRIER



RURAL	
MINOR ARTERIAL	
FAP 573 (US 30)	
2011	
ADT	4500
P.V.	81.11%
S.U.	5.00%
M.U.	13.89%



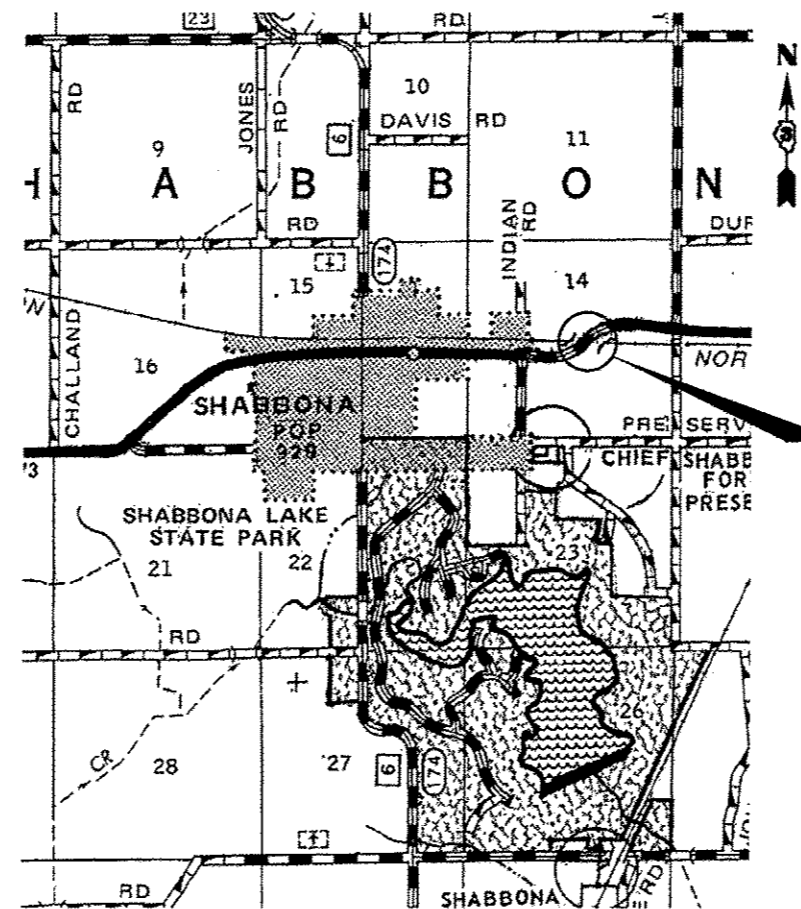
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

DISTRICT NO. 3 815-434-6131

PROJECT ENGINEER: JOE KANNEL P.E.  
UNIT CHIEF: RON WOODSHANK  
TOWNSHIP: SHABBONA

CONTRACT NO. 66C65



**PROJECT LOCATION**  
STRUCTURE NO. 019-0032  
STA. 357 + 63.72  
OVER BNSF RAILROAD  
0.50 MILES EAST OF SHABBONA

GROSS LENGTH = 300 FT. = 0.057 MILE  
NET LENGTH = 300 FT. = 0.057 MILE

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED 2/23 2013  
*Paul A. Larson*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

March 22 2013  
*John D. Baranzelli P.E. Jr.*  
ENGINEER OF DESIGN AND ENVIRONMENT

March 22 2013  
*Omer Osman P.E. Jr.*  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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OF THE STATE OF ILLINOIS**

**GENERAL NOTES - ROADWAY**

THE THICKNESS OF HMA SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA IS PLACED.

THE ENGINEER WILL BE THE SOLE JUDGE CONCERNING CURING TIME FOR THE VARIOUS HMA LIFTS.

FOR STABILIZATION, ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR SAND BAGS PER BARRICADE.

ABANDONED UNDERGROUND UTILITIES THAT CONFLICT WITH CONSTRUCTION SHALL BE DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT OF WAY ACCORDING TO ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF EARTH EXCAVATION.

ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER SHOWN IN THE LIST OF STANDARDS OR THE COPY INCLUDED IN THESE PLANS.

THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN CALCULATING PLAN QUANTITIES:

GRANULAR MATERIALS (RIPRAP)	0.6	TONS / CU YD
HMA SHOULDERS	112	LBS / SQ YD / IN

BITUMINOUS MATERIALS (PRIME COAT) RATES		
SURFACE TYPE	ESTIMATED TRUCK APPLICATION RATE	RESIDUAL RATE
AGGREGATE BASES	0.375 GAL / SQ YD	N/A
MILLED HMA OR PCC PAVEMENT	0.08 GAL / SQ YD	0.04 GAL / SQ YD
EXISTING PAVEMENT	0.05 GAL / SQ YD	0.025 GAL / SQ YD
FOG COAT (BETWEEN ADDITIONAL HMA LIFTS)	0.05 GAL / SQ YD	0.025 GAL / SQ YD

ESTIMATED TRUCK APPLICATION RATE USED FOR CALCULATING PLAN QUANTITIES

THE CONTRACTOR SHALL CONTACT JULIE AT LEAST 48 HOURS PRIOR TO EXCAVATION TO DETERMINE WHICH UTILITIES ARE IN THE AREA.

**COMMITMENTS**

DISPOSAL OF EXCAVATED MATERIALS. ALL SUITABLE OR UNSUITABLE MATERIALS EXCAVATED DUE TO CONSTRUCTION OPERATIONS SHALL BE DISPOSED OF WITHIN THE LIMITS OF EXISTING RIGHT OF WAY. DISPOSAL OF EXCAVATED MATERIALS OFF THE RIGHT OF WAY SHALL NOT BE PERMITTED.

EXCAVATED MATERIALS SHALL BE DISPOSED OF AT LOCATIONS DIRECTED BY THE ENGINEER. ANY SUCH DISPOSAL SHALL NOT CREATE AND UNSIGHTLY OR OBJECTIONABLE APPEARANCE OR DETRACT FROM THE NATURAL TOPOGRAPHIC FEATURES WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.

DATE: 2-13-13

PREPARED BY: Tom Zornick  
DISTRICT STUDIES & PLANS ENGINEER

EXAMINED BY: Herbert D. [Signature]  
DISTRICT CONSTRUCTION ENGINEER

Wayne L. Phillips  
DISTRICT MATERIALS ENGINEER

Bruce A. [Signature]  
DISTRICT OPERATIONS ENGINEER

FILE NAME =	USER NAME = woodshankr-l	DESIGNED - RW	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>GENERAL NOTES</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pe_work\psidot\woodshankr-l\0320292\	0366C65-ahf-details.dgn	DRAWN - RW	REVISED -			573	(116VB-1-1BR)	DEKALB	21	2	
	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -			CONTRACT NO. 66C65		ILLINOIS FED. AID PROJECT			
	PLOT DATE = 2/7/2013	DATE -	REVISED -			SCALE:	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		

CODE NO.	ITEM	UNIT	CONSTR. CODE
			STATE FUNDS 100% STATE ROADWAY 0014 RURAL
20200500	EARTH EXCAVATION (WIDENING)	CU YD	29
42001500	P.C. CONCRETE BRIDGE APPROACH SHOULDER PAVEMENT	SQ YD	45
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	128
50102400	CONCRETE REMOVAL	CU YD	75.4
50300255	CONCRETE SUPERSTRUCTURE	CU YD	81
50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	1382
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	9200
50800515	BAR SPLICERS	EACH	36
52000110	PREFORMED JOINT STRIP SEAL	FOOT	128
52100010	ELASTOMERIC BEARING ASSEMBLY, TYPE I	EACH	5
52100020	ELASTOMERIC BEARING ASSEMBLY, TYPE II	EACH	5
52100520	ANCHOR BOLTS, 1"	EACH	20
59000200	EPOXY CRACK INJECTION	FOOT	290
63300575	REMOVE AND REERECT RAIL ELEMENT OF EXISTING GUARDRAIL	FOOT	100
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	4
67100100	MOBILIZATION	L SUM	1
70100405	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321	EACH	1
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1

FILE NAME =	USER NAME = woodshenkr1	DESIGNED - RW	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pvc\work\pvc\dot\woodshenkr1\d8328292\	666C65.eht-details.dgn	DRAWN - RW	REVISED -			573	(116VB-1-1BR)	DEKALB	21	3
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -	REVISED -			CONTRACT NO. 66C65				
PLOT DATE = 2/14/2013	DATE -	REVISED -	REVISED -			SCALE:	SHEET NO. 1 OF 2 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT

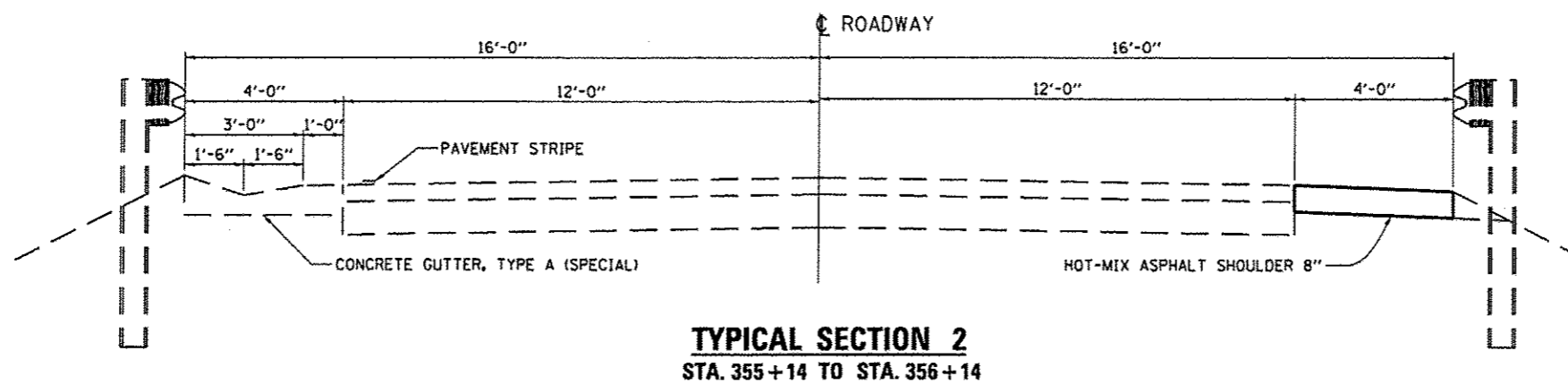
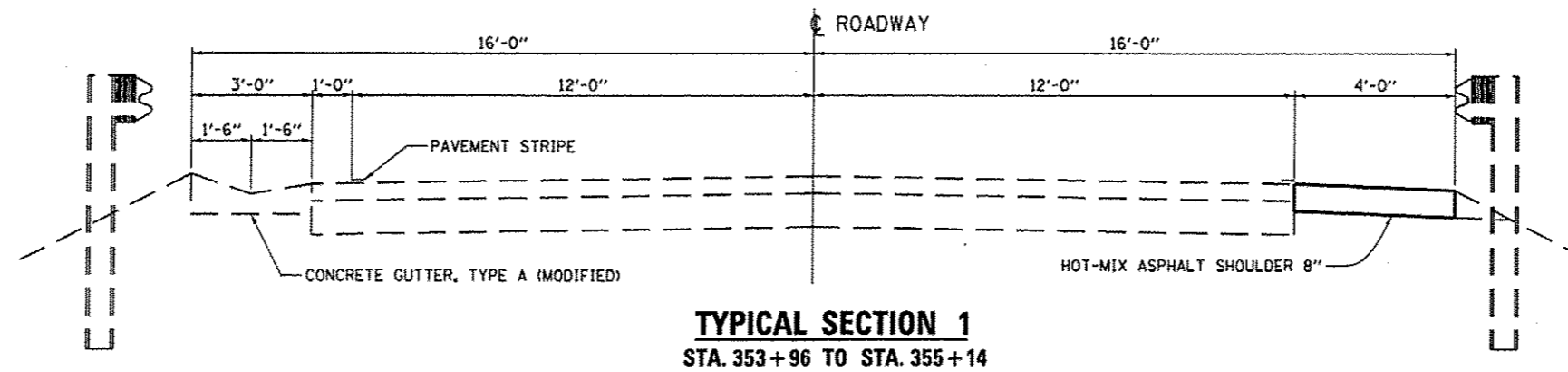
Rev.

CODE NO.	ITEM	UNIT	CONSTR. CODE
			STATE FUNDS 100% STATE ROADWAY 0014 RURAL
70100455	TRAFFIC CONTROL AND PROTECTION, STANDARD 701206	L SUM	1
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1
70106700	TEMPORARY RUMBLE STRIPS	EACH	6
70400100	TEMPORARY CONCRETE BARRIER	FOOT	440
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	440
70600250	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2
70600350	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2
* 78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	1568
* 78001130	PAINT PAVEMENT MARKING - LINE 6"	FOOT	196
78100300	REPLACEMENT REFLECTOR	EACH	10
78300100	PAVEMENT MARKING REMOVAL	SQ FT	615
X0550300	SLOPE WALL BREAKING	SQ YD	253
X2810108	STONE RIPRAP, CLASS A4 (SPECIAL)	SQ YD	253
Z0001495	BRIDGE APPROACH SHOULDER REMOVAL	SQ YD	45
Z0001800	APPROACH SLAB REPAIR (PARTIAL DEPTH)	SQ YD	20
Z0001899	JACK AND REMOVE EXISTING BEARINGS	EACH	10
Z0001905	STRUCTURAL STEEL REPAIR	POUND	180

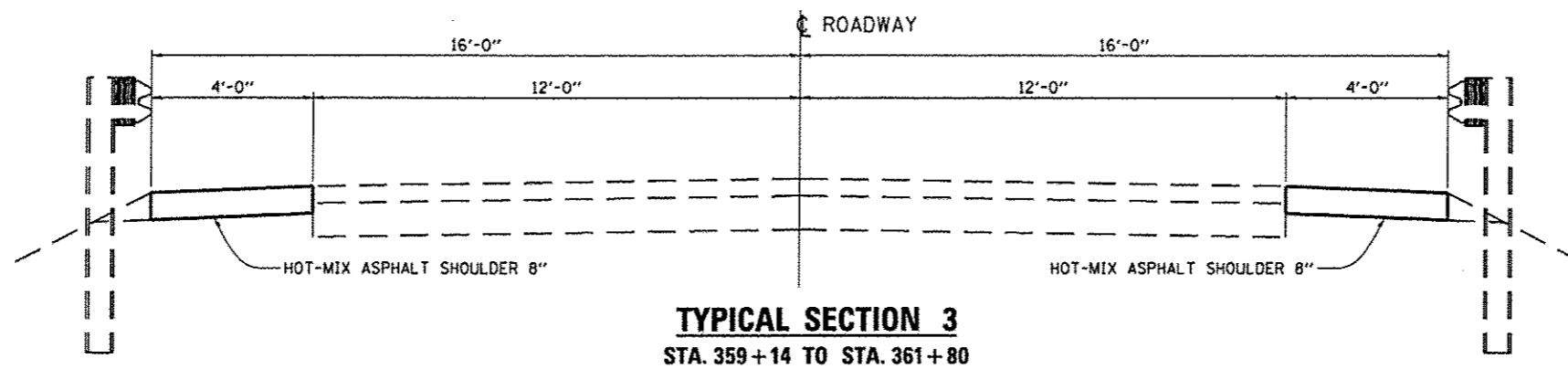
\* SPECIALITY ITEM

FILE NAME =	USER NAME = woodshankr1	DESIGNED - RW	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
of\pe_work\pedit\woodshankr1\08328292\	366C65-ehd-details.dgn	DRAWN - RW	REVISED -					573	(116VB-1-1BR)	DEKALB	21	4
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	PLOT DATE = 2/14/2013	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

Rev.

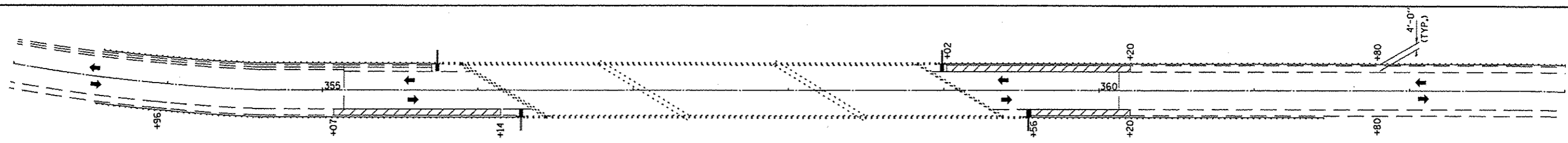


\* STRUCTURE NO. 019-0032  
STA. 356+14 TO STA. 359+14  
\*\*STATIONING MEASURED ALONG CENTERLINE OF ROADWAY



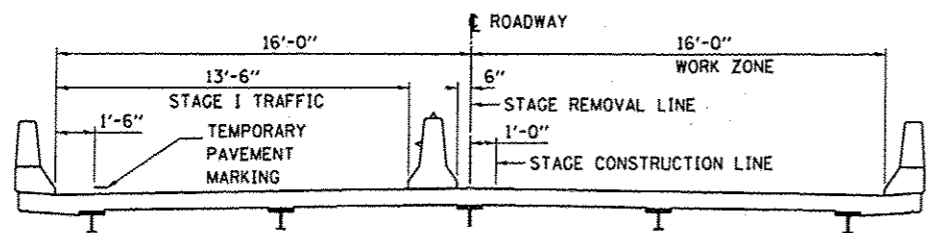
	HMA SHOULDER
PG GRADE	PG64-22
DESIGN AIR VOIDS	4% @ N50
MIXTURE COMPOSITION	IL 19.0
DENSITY CONTROL METHOD	CORES

FILE NAME =	USER NAME = woodshankr1	DESIGNED - RW	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL SECTIONS</b>		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwwork\pwwork\woodshankr1\d9328292	366C65-shr-details.dgn	DRAWN - RW	REVISED -		573	(116VB-1-1BR)	DEKALB	21	5		
PLOT SCALE = 100.0000 1 / in.	CHECKED -	REVISED -	REVISED -		SCALE: SHEET NO. 1 OF 1 SHEETS		STA.	TO STA.	CONTRACT NO. 66C65		
PLOT DATE = 2/14/2013	DATE -	REVISED -	REVISED -		ILLINOIS FED. AID PROJECT						

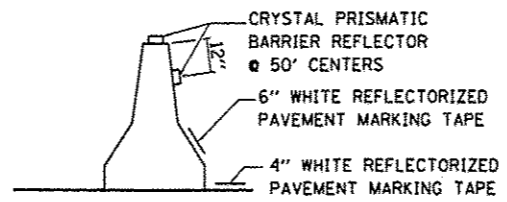


PRE-STAGE CONSTRUCTION

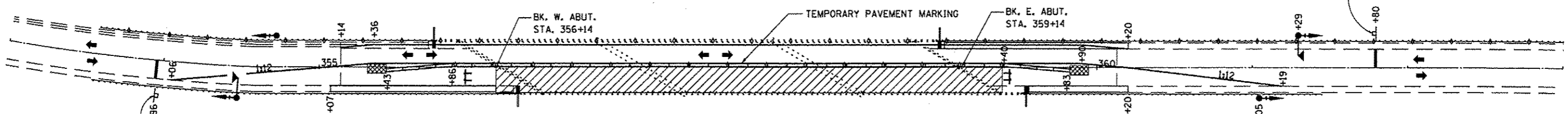
 EARTH EXCAVATION (WIDENING)  
HOT-MIX ASPHALT SHOULDER 8"



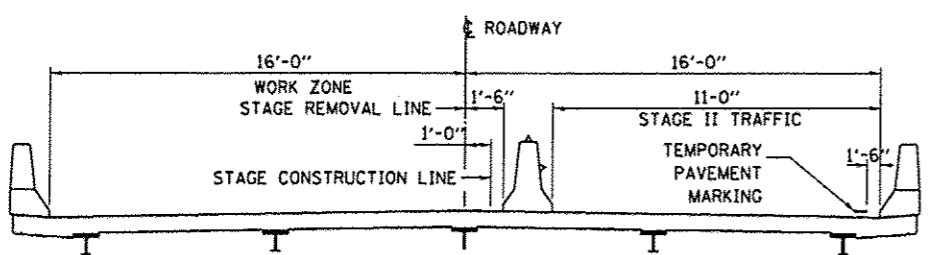
CROSS SECTION  
STAGE I  
LOOKING EAST



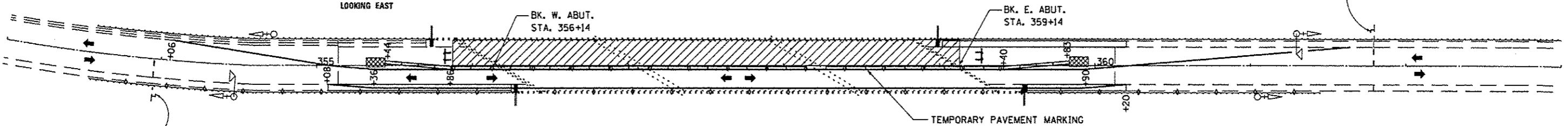
TEMPORARY CONCRETE BARRIER WALL DETAILS



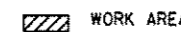

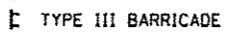

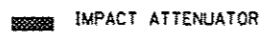
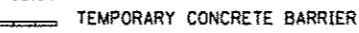
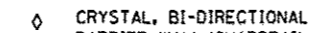
STAGE I CONSTRUCTION



CROSS SECTION  
STAGE II  
LOOKING EAST



STAGE II CONSTRUCTION

- LEGEND**
-  WORK AREA
  -  SIGN
  -  TYPE III BARRICADE
  -  TRAFFIC SIGNAL WITH BACKPLATE
  -  IMPACT ATTENUATOR
  -  TEMPORARY CONCRETE BARRIER
  -  CRYSTAL, BI-DIRECTIONAL BARRIER WALL/GUARDRAIL MARKER

GENERAL NOTES

TRAFFIC CONTROL AND PROTECTION FOR PRE-STAGE CONSTRUCTION OPERATIONS SHALL BE ACCORDING TO STANDARD 701201.

IN ADDITION TO THE DETAILS SHOWN, TRAFFIC CONTROL AND PROTECTION FOR STAGE I AND STAGE II CONSTRUCTION OPERATIONS SHALL BE ACCORDING TO STANDARD 701321 AND 704001.

WHEN TEMPORARY RUMBLE STRIPS ARE SHOWN ON THE TRAFFIC CONTROL AND PROTECTION STANDARD 701321, THE RUMBLE STRIPS SHALL BE CONSTRUCTED OF THERMOPLASTIC PAVEMENT MARKINGS TO A THICKNESS OF ONE-HALF INCH (1/2").

FILE NAME =	USER NAME = woodshenkr1	DESIGNED - RW	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STAGING DETAILS FOR STRUCTURE NO. 019-0032</b>			F.A.P. RTE. 573	SECTION (I16VB-1-IBR1)	COUNTY DEKALB	TOTAL SHEETS 21	SHEET NO. 6
o:\pw\work\p1dat\woodshenkr1\03220292\0366C65-shr-details.dgn		DRAWN - RW	REVISED -		SCALE: 1" = 40'	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT			
PLOT SCALE = 1/8" = 1' / in.		CHECKED -	REVISED -									
PLOT DATE = 2/14/2013		DATE -	REVISED -									

**GENERAL NOTES**

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF WORK; HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.

ALL STRUCTURAL STEEL SHALL CONFORM TO AASHTO M 270 GRADE 36, UNLESS OTHERWISE NOTED.

REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED. PRIOR TO POURING THE NEW CONCRETE DECK, ALL HEAVY OR LOOSE RUST, LOOSE MILL SCALE, AND OTHER LOOSE OR POTENTIALLY DETRIMENTAL FOREIGN MATERIAL SHALL BE REMOVED FROM THE SURFACES IN CONTACT WITH CONCRETE. TIGHTLY ADHERED PAINT MAY REMAIN UNLESS OTHERWISE NOTED. REMOVAL SHALL BE ACCOMPLISHED BY METHODS THAT WILL NOT DAMAGE THE STEEL AND THE COST WILL BE INCLUDED IN CONCRETE REMOVAL.

CROSS FRAMES OR DIAPHRAGMS BETWEEN BEAMS OR GIRDERS AT BEARINGS MAY BE TEMPORARILY DISCONNECTED TO INSTALL BEARING ANCHOR RODS.

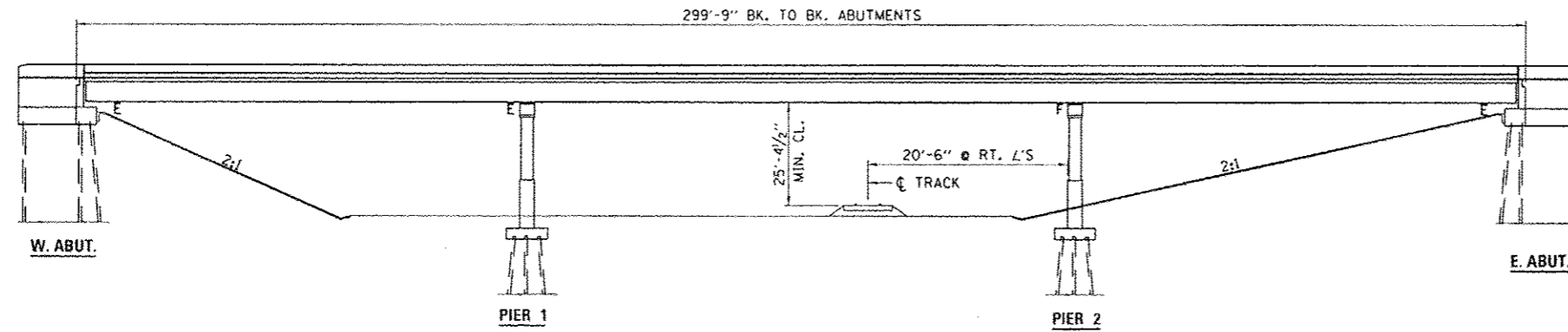
TWO 1/8 IN. ADJUSTING SHIMS SHALL BE PROVIDED FOR EACH BEARING IN ADDITION TO ALL OTHER PLATES OR SHIMS AND PLACED AS SHOWN ON BEARING DETAILS.

ALL (EMBEDDED AND SEPARATE) BEARING PLATES, STEEL EXTENSIONS SIDE RETAINERS, ANCHOR BOLTS, NUTS, WASHERS AND PINTLES SHALL BE GALVANIZED ACCORDING TO AASHTO M111 OR M232 AS APPLICABLE.

H.S. BOLTS IN BEARING ASSEMBLY SHALL BE GALVANIZED ACCORDING TO AASHTO M298 CLASS 50.

COST OF REMOVAL AND RE-INSTALLATION OF ALL MEMBERS NECESSARY TO COMPLETE THE WORK AS DETAILED ON THE PLANS AND AS SPECIFIED IN THE SPECIAL PROVISIONS SHALL BE INCLUDED WITH FURNISHING AND ERECTING STRUCTURAL STEEL.

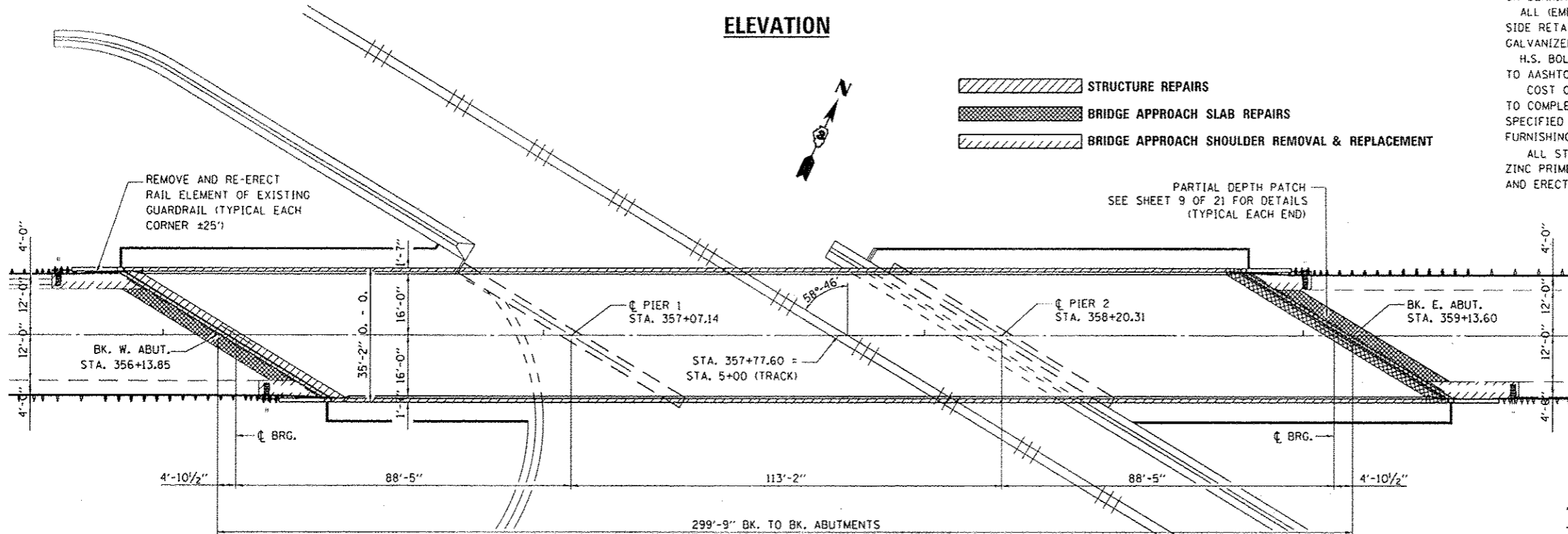
ALL STRUCTURAL STEEL SHALL BE SHOP PAINTED WITH THE INORGANIC ZINC PRIMER PER AASHTO M300, TYPE 1, COST INCLUDED WITH FURNISHING AND ERECTING STRUCTURAL STEEL.



**ELEVATION**



- STRUCTURE REPAIRS
- BRIDGE APPROACH SLAB REPAIRS
- BRIDGE APPROACH SHOULDER REMOVAL & REPLACEMENT



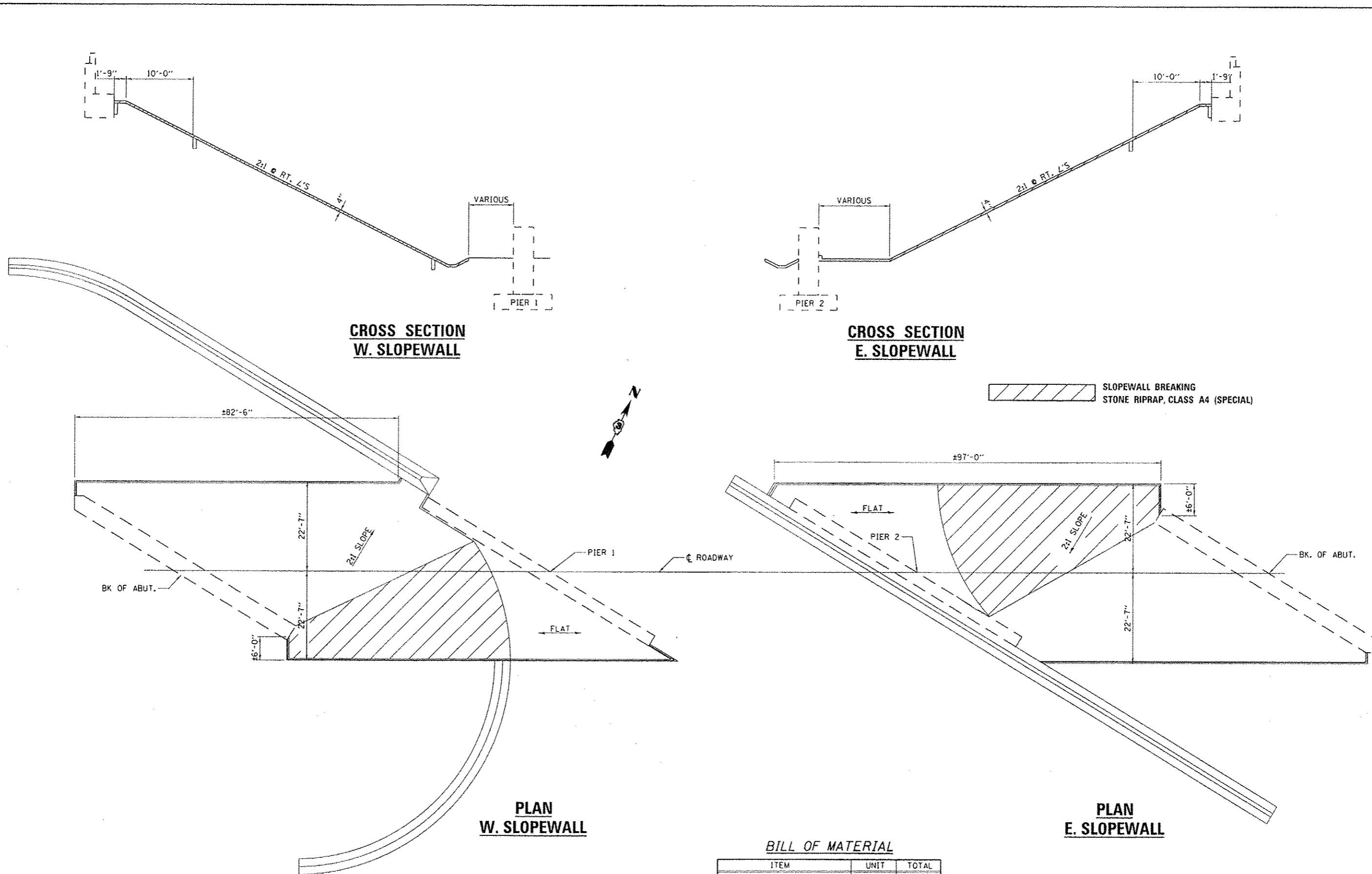
**PLAN  
STRUCTURE NO. 019-0032**

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	SUPER	SUB	TOTAL
CONCRETE REMOVAL	CU YD	75.4		75.4
CONCRETE SUPERSTRUCTURE	CU YD	81		81
F & E STRUCTURAL STEEL	POUND	1382		1382
PREFORMED JOINT STRIP SEAL	FOOT	128		128
EPOXY CRACK INJECTION	FOOT		290	290
JACK AND REMOVE EXISTING BEARINGS	EACH	10		10
ANCHOR BOLTS 1" Ø		20		20
ELASTOMERIC BEARING ASSEMBLY, TYPE I	EACH	5		5
ELASTOMERIC BEARING ASSEMBLY, TYPE II	EACH	5		5
REINFORCEMENT BARS, EPOXY COATED	POUND	8930		8930
STONE RIPRAP, CLASS A4 (SPECIAL)	SO YD		253	253
SLOPE WALL BREAKING	SO YD		253	253
BRIDGE APPROACH SHOULDER REMOVAL	SO YD	45		45
PCC BRIDGE APPROACH SHOULDER PAVEMENT	SO YD	45		45
APPROACH SLAB REPAIR (PARTIAL)	SO YD	20		20
BAR SPLICERS	EACH	36		36
STRUCTURAL STEEL REPAIR	POUND	220		220



*David Carl Puzey* 3/23/13  
Expires 11/30/14



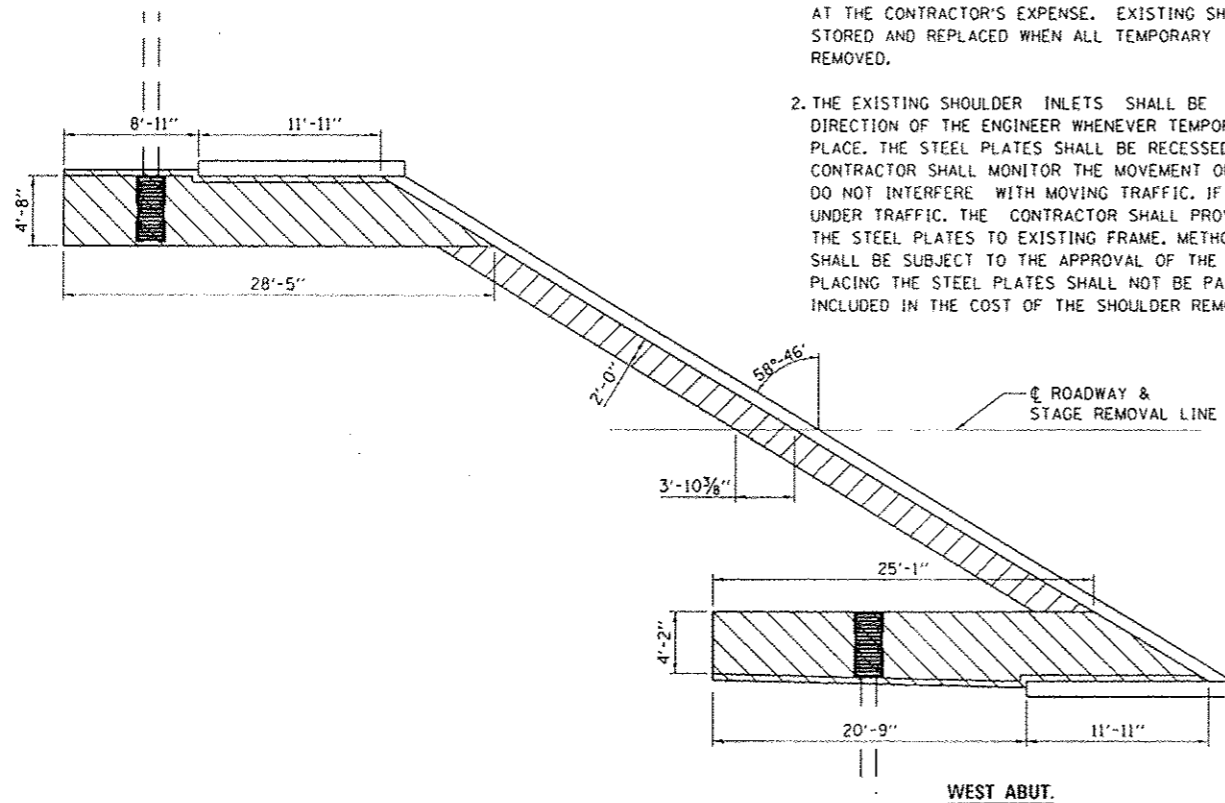
**BILL OF MATERIAL**

ITEM	UNIT	TOTAL
SLOPE WALL BREAKING	SO. YD.	253
STONE RIPRAP, CLASS A4 (SPECIAL)	SO. YD.	253

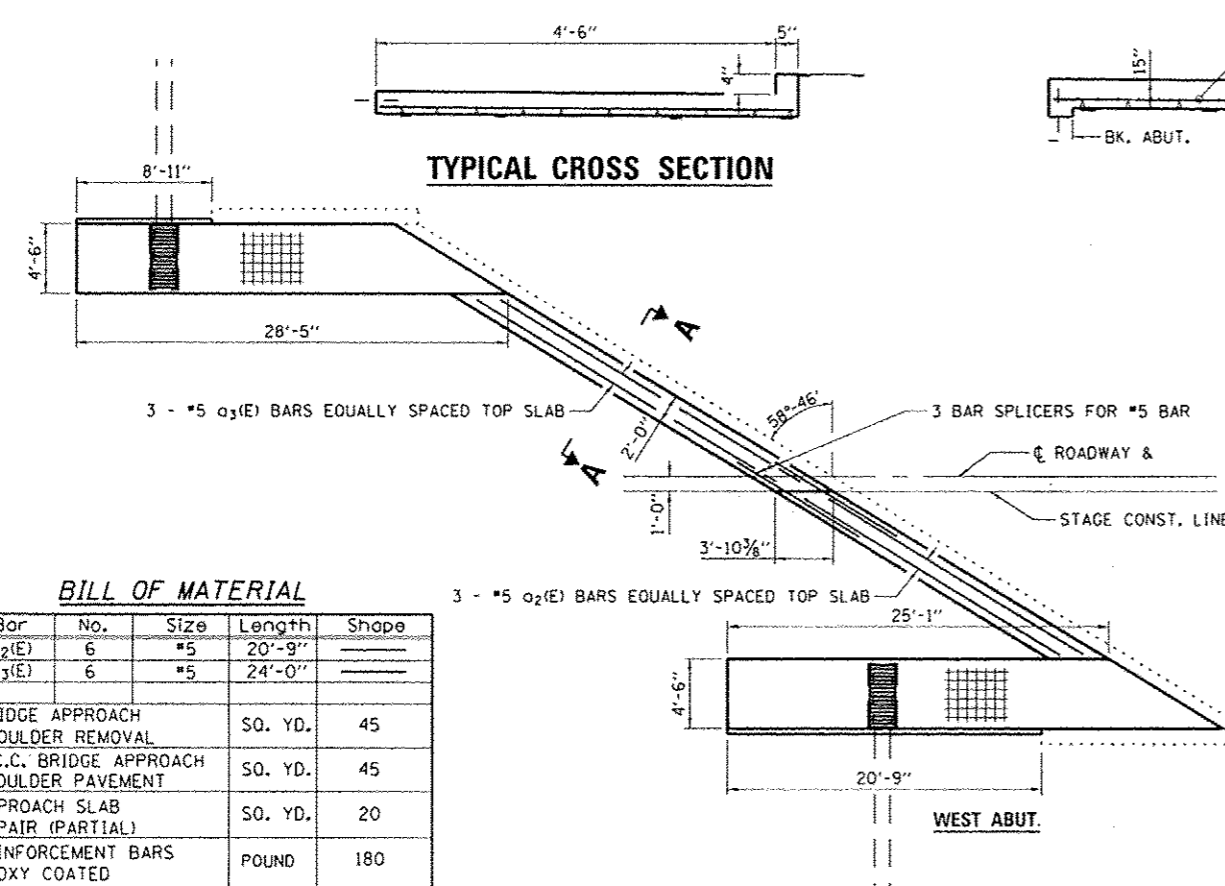


**NOTES:**

- EXISTING SHOULDER INLETS AND FRAMES SHALL REMAIN IN PLACE AND INCORPORATED INTO THE NEW CONSTRUCTION. IF THE CONTRACTOR'S CONSTRUCTION OPERATIONS CAUSE DAMAGE TO THE INLET OR FRAME, THE DAMAGED ITEM SHALL BE REMOVED AND REPLACED AT THE CONTRACTOR'S EXPENSE. EXISTING SHOULDER GRATES SHALL BE REMOVED, STORED AND REPLACED WHEN ALL TEMPORARY TRAFFIC CONTROL DEVICES HAVE BEEN REMOVED.
- THE EXISTING SHOULDER INLETS SHALL BE COVERED WITH STEEL PLATE(S) AT THE DIRECTION OF THE ENGINEER WHENEVER TEMPORARY TRAFFIC CONTROL DEVICES ARE IN PLACE. THE STEEL PLATES SHALL BE RECESSED WITHIN THE LIMITS OF THE INLET. THE CONTRACTOR SHALL MONITOR THE MOVEMENT OF THE PLATED INLETS SO THAT THEY DO NOT INTERFERE WITH MOVING TRAFFIC. IF THE PLATES SHOW SIGNS OF MOVEMENT UNDER TRAFFIC, THE CONTRACTOR SHALL PROVIDE POSITIVE MEANS OF ANCHORING THE STEEL PLATES TO EXISTING FRAME. METHOD OF ANCHORING THE STEEL PLATES SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER. COST OF FURNISHING AND PLACING THE STEEL PLATES SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE SHOULDER REMOVAL AND REPLACEMENT ITEMS.



**EXISTING PLAN**



**PROPOSED PLAN**

**TYPICAL CROSS SECTION**

**TYPICAL LONGITUDINAL SECTION**

**NOTE:**

TRANSVERSE REINFORCEMENT BARS FOR STAGE I BRIDGE APPROACH SHOULDER PAVEMENT SHALL BE ORDERED FULL LENGTH, CUT TO FIT SKEW AND REMAINDER USED IN STAGE II SHOULDER PAVEMENT. TYPICAL AT EACH ABUTMENT.

**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a <sub>2</sub> (E)	6	#5	20'-9"	—
a <sub>3</sub> (E)	6	#5	24'-0"	—
BRIDGE APPROACH SHOULDER REMOVAL			SQ. YD.	45
P.C.C. BRIDGE APPROACH SHOULDER PAVEMENT			SQ. YD.	45
APPROACH SLAB REPAIR (PARTIAL)			SQ. YD.	20
REINFORCEMENT BARS EPOXY COATED			POUND	180

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**APPROACH SLAB DETAILS  
FOR STRUCTURE NO. 019-0032**

F.A.P. RTE. 573	SECTION (116VB-1-1BR1)	COUNTY DEKALB	TOTAL SHEETS 21	SHEET NO. 9
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SCALE: SHEET NO. 3 OF 15 SHEETS STA. 356+13.85 TO STA. 359+13.60

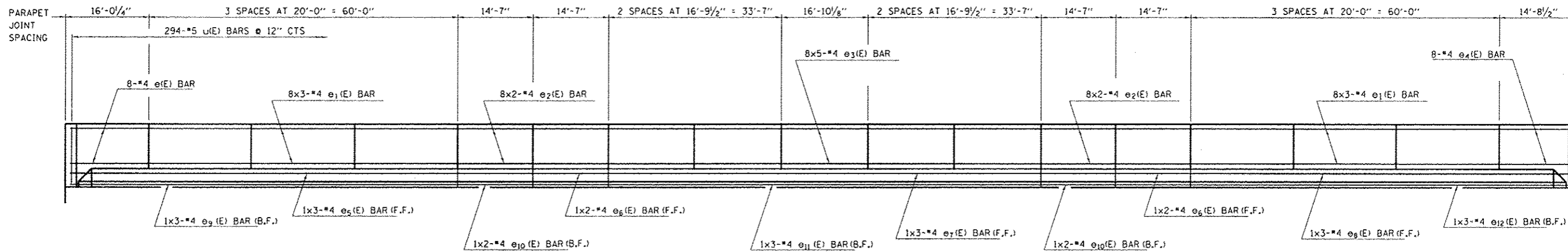
ILLINOISIFIED A10 PROJECT

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DESIGNED: RW  
DRAWN: RW  
CHECKED: ARS  
DATE: 3/22/2013  
PLOT SCALE: 1/8"=1'-0"  
PLOT DATE: 3/22/2013

DESIGNED: RW  
DRAWN: RW  
CHECKED: ARS  
DATE: 3/22/2013

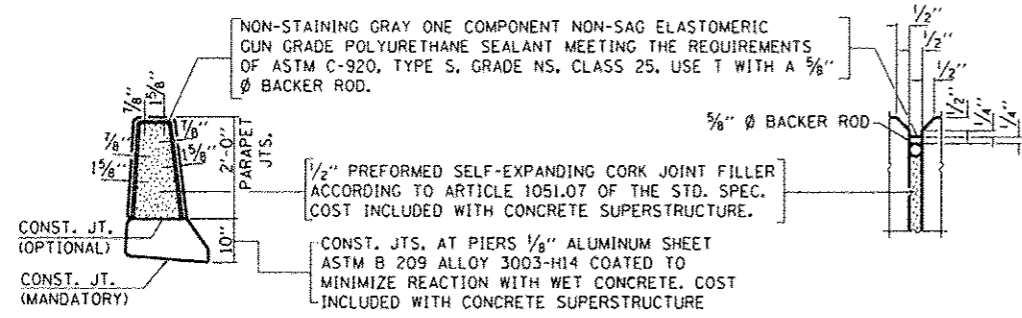
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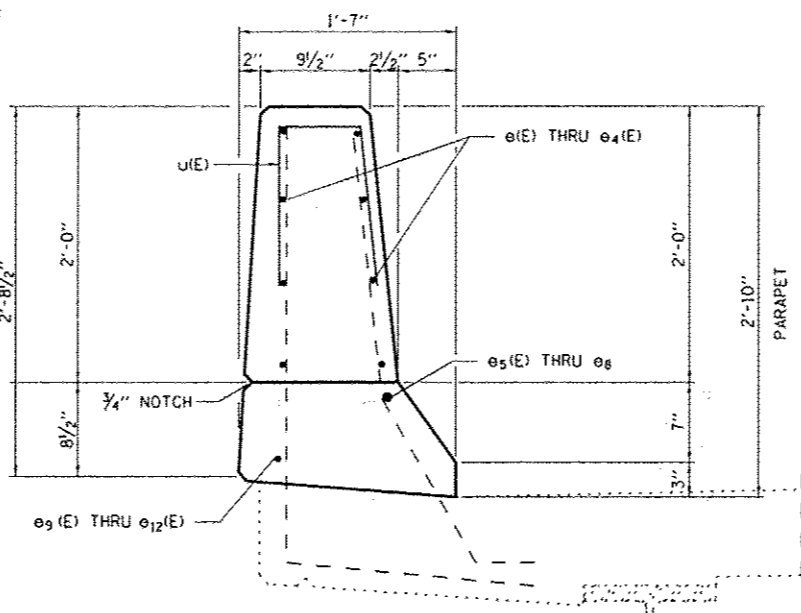


**ELEVATION**

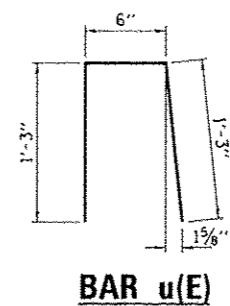
NORTH PARAPET SHOWN, SOUTH PARAPET SIMILAR THRU 180° ROTATION ABOUT C ROADWAY



**PARAPET JOINT DETAILS**



**SECTION THRU PARAPET**



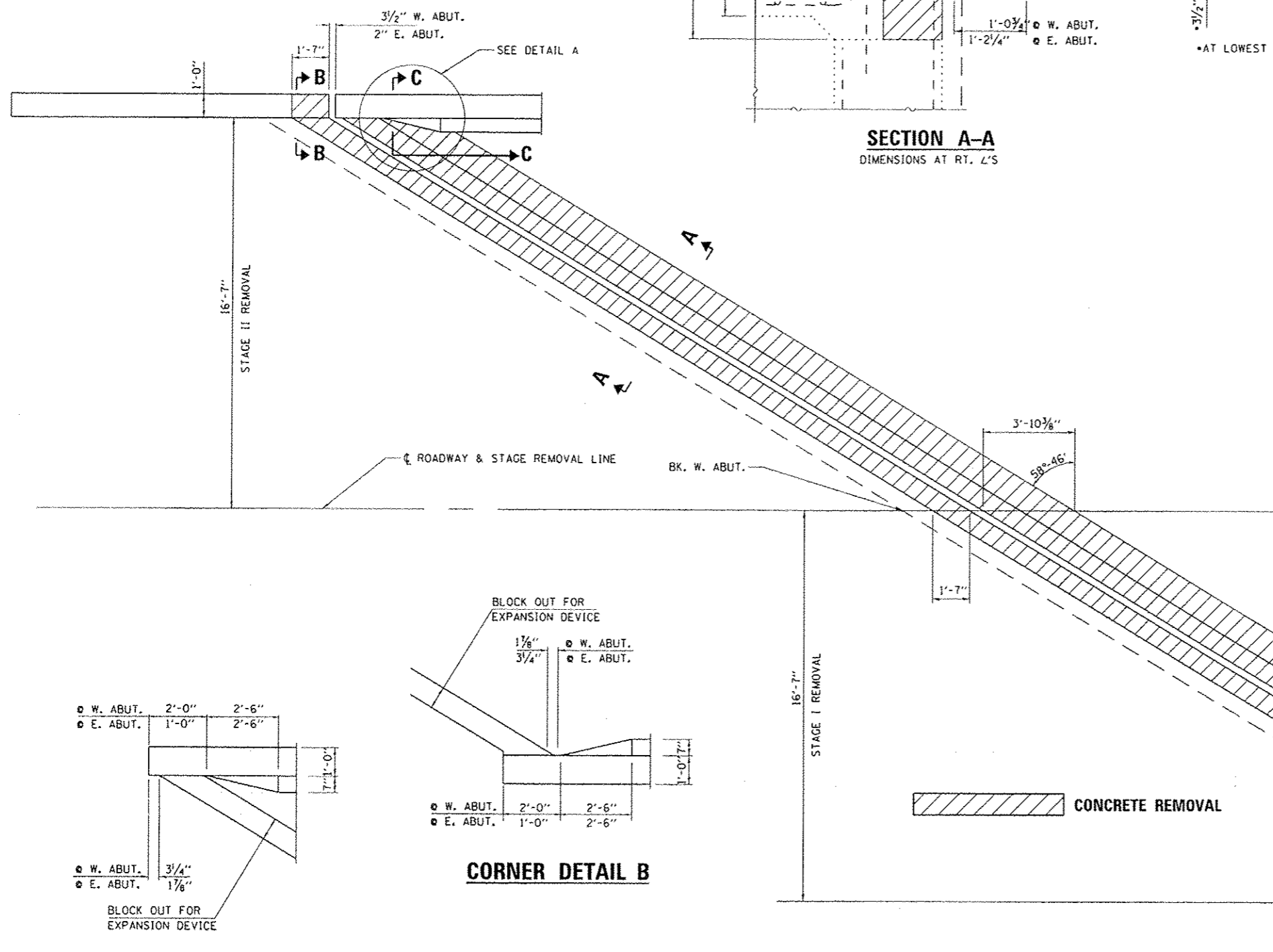
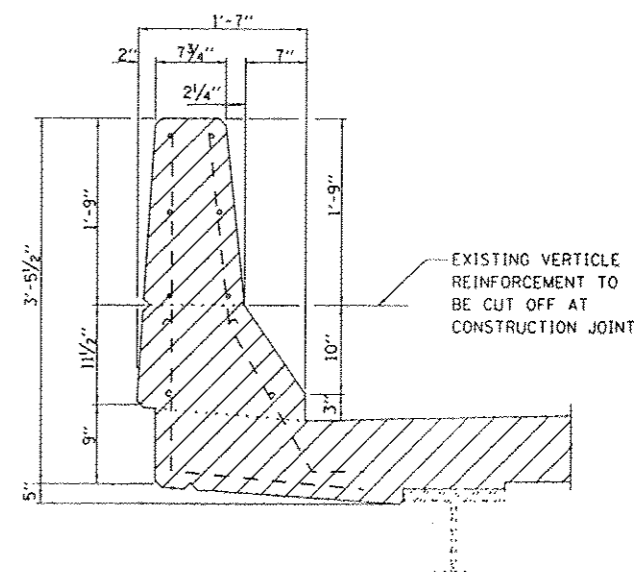
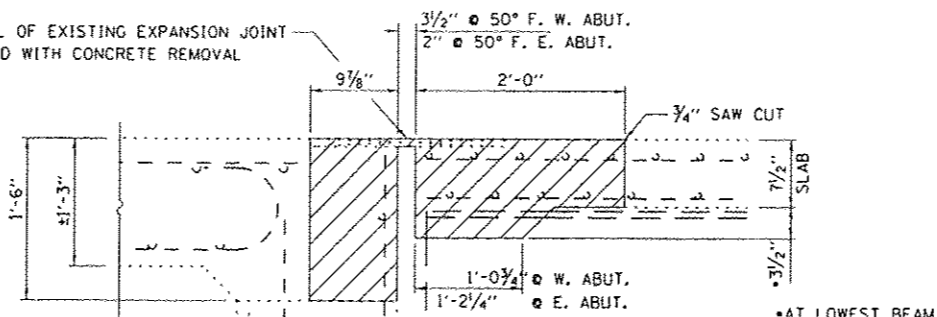
**BAR u(E)**

**BILL OF MATERIAL FOR TWO PARAPETS**

BAR	NO.	SIZE	LENGTH	SHAPE
e1(E)	16	#4	15'-9"	=====
e1(E)	96	#4	19'-9"	=====
e2(E)	64	#4	14'-4"	=====
e3(E)	80	#4	16'-6"	=====
e4(E)	16	#4	14'-5"	=====
e5(E)	6	#8	28'-11"	=====
e6(E)	8	#8	14'-4"	=====
e7(E)	6	#8	25'-0"	=====
e8(E)	6	#8	28'-6"	=====
e9(E)	6	#4	26'-9"	=====
e10(E)	8	#4	14'-3"	=====
e11(E)	6	#4	22'-6"	=====
e12(E)	6	#4	26'-3"	=====
u(E)	294	#5	3'-0"	=====
CONCRETE REMOVAL			CU YD	59.4
CONCRETE SUPERSTRUCTURE			CU YD	64.4
REINFORCEMENT BARS EPOXY COATED			POUND	6000

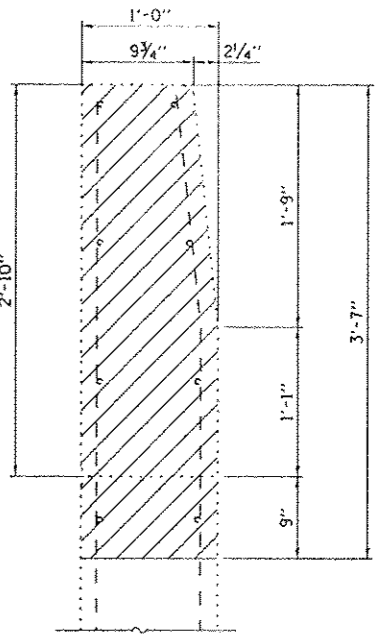


REMOVAL OF EXISTING EXPANSION JOINT INCLUDED WITH CONCRETE REMOVAL



**SECTION A-A**  
DIMENSIONS AT RT. L'S

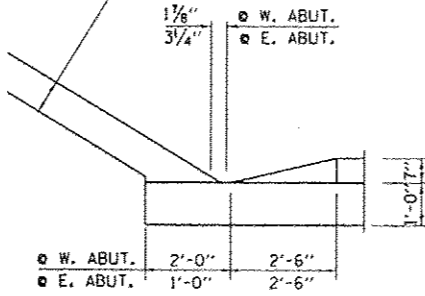
**SECTION C-C**  
EXISTING (TYP.)



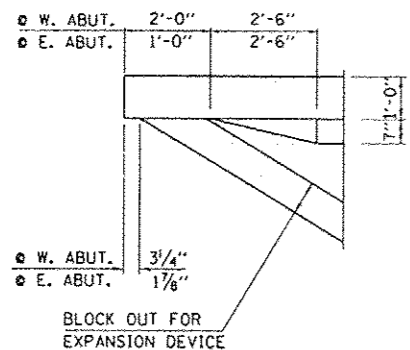
**SECTION B-B**

BLOCK OUT FOR EXPANSION DEVICE

1 1/8" W. ABUT.  
3 1/4" E. ABUT.



**CORNER DETAIL B**



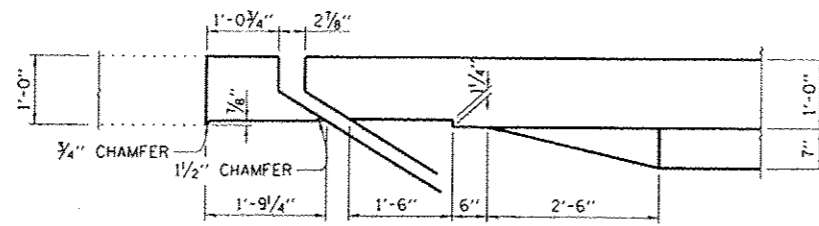
**CORNER DETAIL A**

CONCRETE REMOVAL

**PLAN**

WEST ABUTMENT SHOWN, EAST ABUTMENT SIMILAR THRU 180° ROTATION ABOUT CL ROADWAY

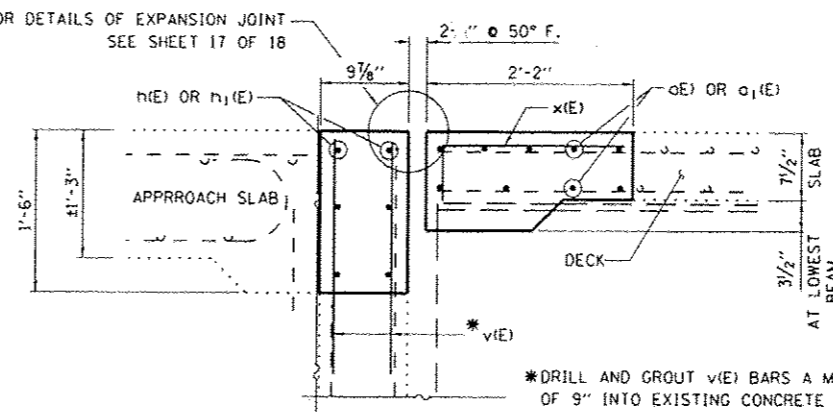
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PLOT SCALE = 1/8" = 1'-0"	PLOT DATE = 3/22/2013	DRAWN - RW	REVISOR -			SCALE:	SHEET NO. 6 OF 15 SHEETS	STA. 356+13.85 TO STA. 359+13.60	CONTRACT NO. 66C65		ILLINOIS FED. AID PROJECT
		CHECKED - ARS	REVISOR -								
		DATE -	REVISOR -								



**DETAIL A**

4 - #4 d(E) BARS @ 12" CTS. (B.F.)  
 1 - #5 d<sub>1</sub>(E) BARS @ 11" CTS. (F.F.)  
 3 - #5 d<sub>2</sub>(E) BARS @ 11" CTS. (F.F.)

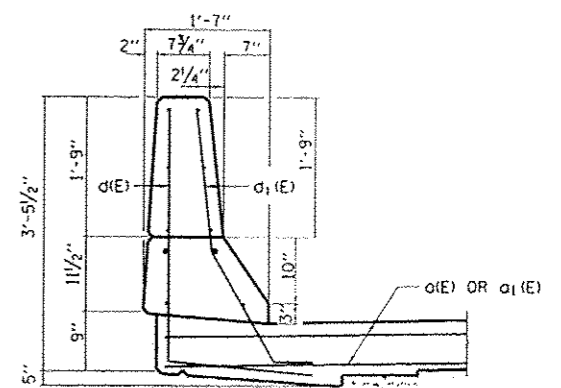
FOR DETAILS OF EXPANSION JOINT  
 SEE SHEET 17 OF 18



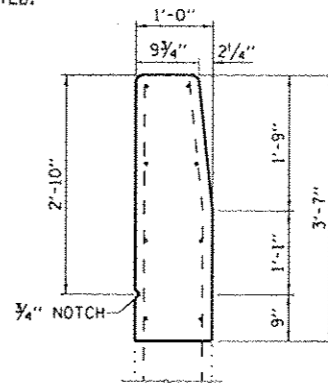
**SECTION B-B**

DIMENSIONS AT RT. L'S

\*DRILL AND GROUT v(E) BARS A MIN. OF 9" INTO EXISTING CONCRETE IN ACCORDANCE WITH ARTICLE 584 OF THE STANDARD SPECIFICATIONS. COST INCLUDED WITH REINFORCEMENT BARS EPOXY COATED.



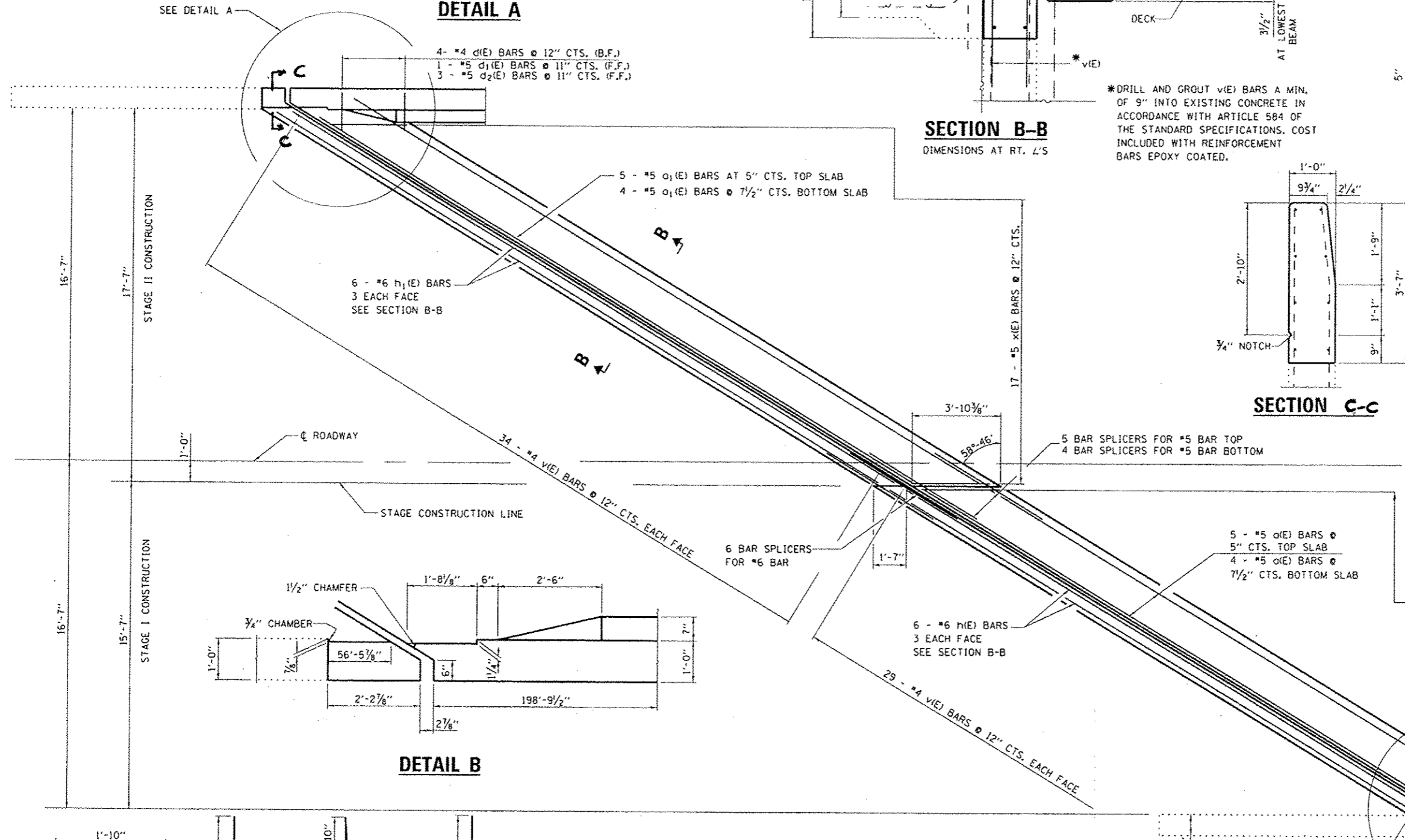
**SECTION C-C**



**SECTION C-C**

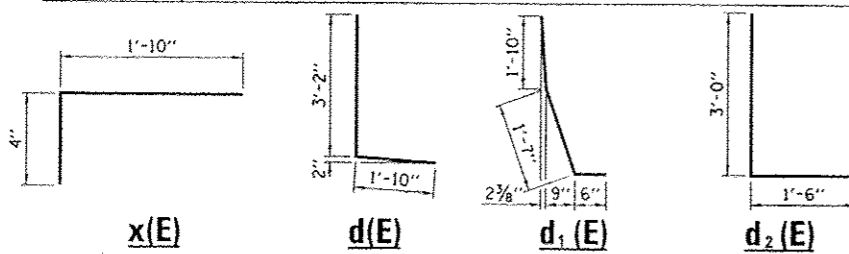
**BILL OF MATERIAL FOR TWO ABUTS.**

Bar	No.	Size	Length	Shape	
d(E)	18	#5	31'-1"	—	
d <sub>1</sub> (E)	18	#5	34'-3"	—	
d(E)	14	#4	5'-0"	—	
d <sub>1</sub> (E)	2	#5	3'-9"	—	
d <sub>2</sub> (E)	12	#5	4'-6"	—	
h(E)	12	#6	29'-10"	—	
h <sub>1</sub> (E)	12	#6	33'-1"	—	
v(E)	252	#4	2'-11"	—	
x(E)	64	#5	2'-2"	—	
CONCRETE REMOVAL				CU. YD.	16.0
CONCRETE SUPERSTRUCTURE				CU. YD.	16.6
REINFORCEMENT BARS EPOXY COATED				POUND	3100

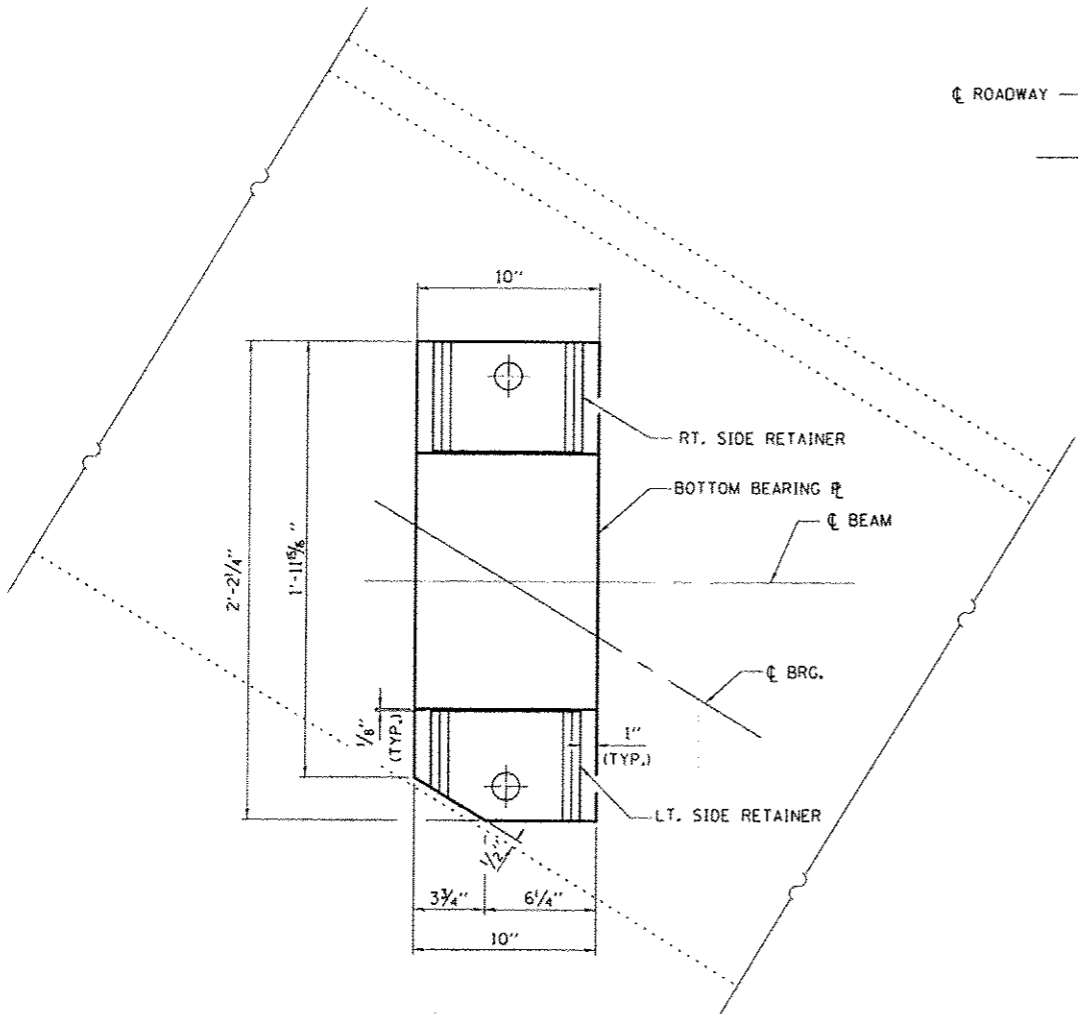
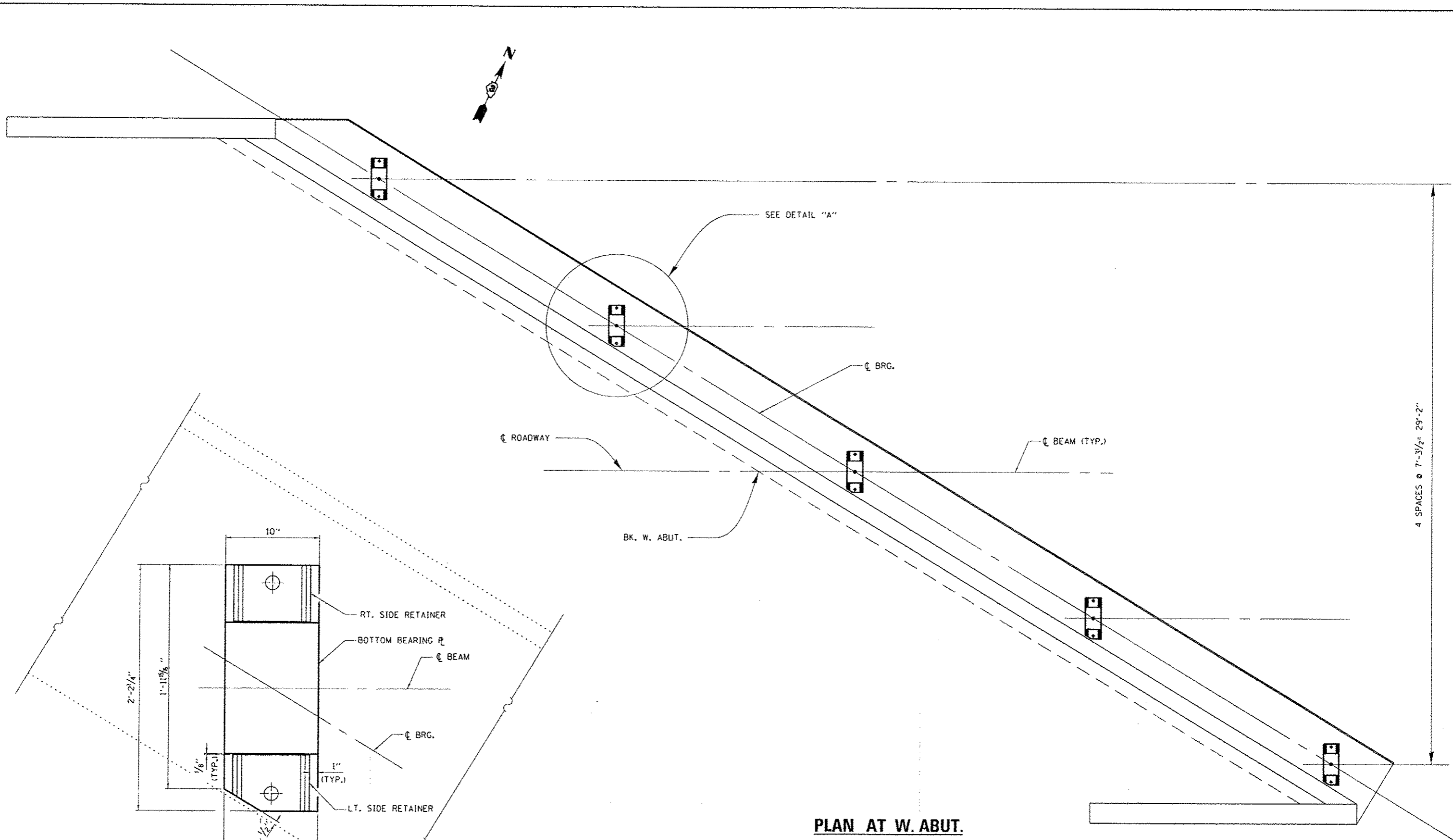


**PLAN**

WEST ABUTMENT SHOWN, EAST ABUTMENT SIMILAR THRU 180° ROTATION ABOUT C ROADWAY



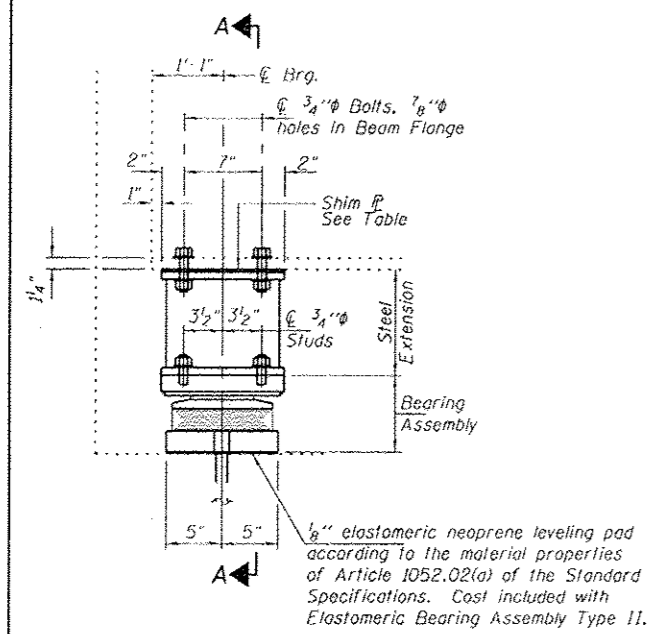
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J06C65-shc-details.dgn		RW	-			CONTRACT NO. 66C65		ILLINOIS FED. AID PROJECT		
PLT SCALE = 100,000 / in.		ARS	-			SCALE:	SHEET NO. 7 OF 15 SHEETS	STA. 356+13.85 TO STA. 359+13.60		
PLT DATE = 3/22/2013			-							



**DETAIL A**

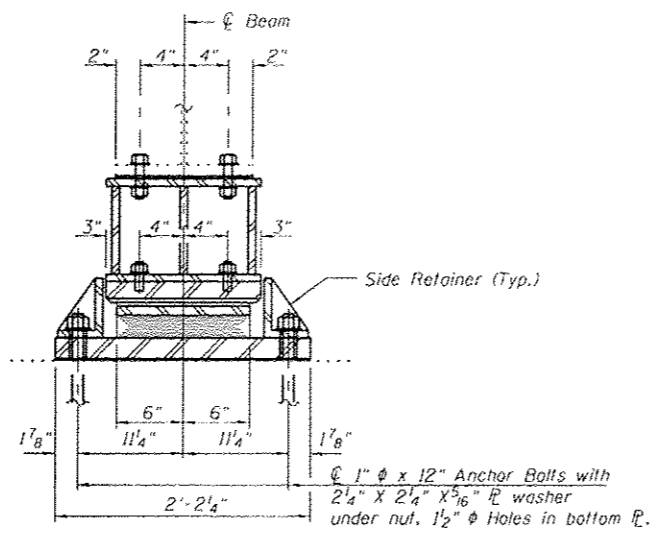
**PLAN AT W. ABUT.**

FILE NAME :	USER NAME :	DESIGNED :	REVISED :	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BEARING DETAILS AT WEST ABUTMENT FOR STRUCTURE NO. 019-0032	F.A.P. RTE. :	SECTION :	COUNTY :	TOTAL SHEETS :	SHEET NO. :	
c:\p-work\p\dott\oodshankr\1\d8328242-	066C65-shr-detail.dgn	DRAWN :	REVISED :			573	(116VB-1-1BR)	DEKALB	21	14	
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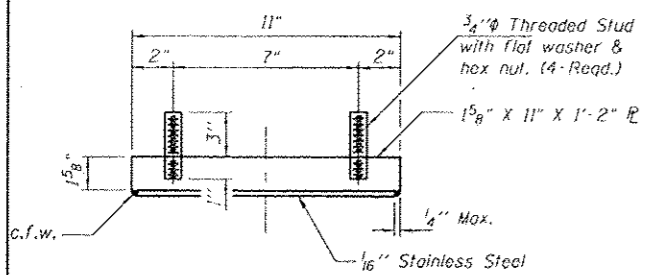


ELEVATION AT ABUTMENT

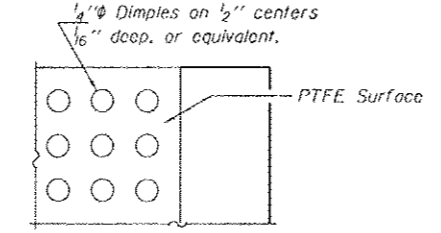
TYPE II TFE ELASTOMERIC EXP. BRG.



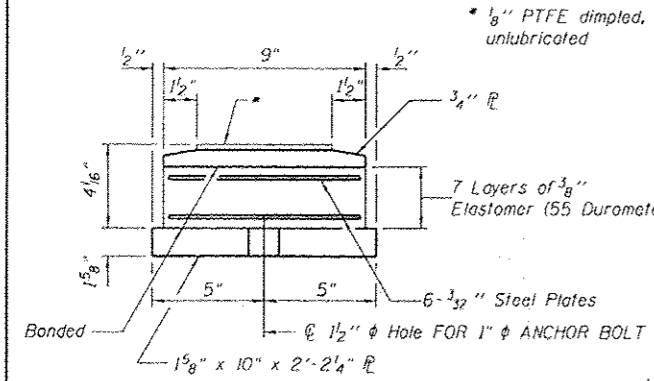
SECTION A-A



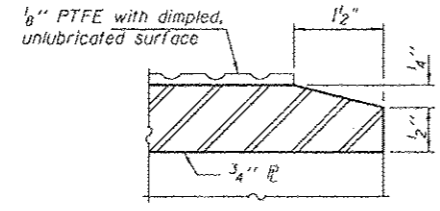
TOP BEARING ASSEMBLY



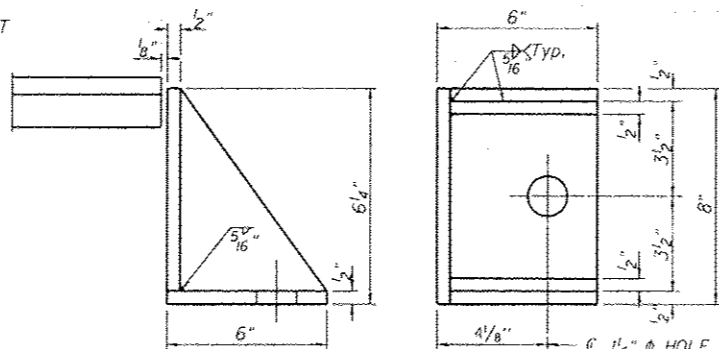
PLAN-PTFE SURFACE



BOTTOM BEARING ASSEMBLY



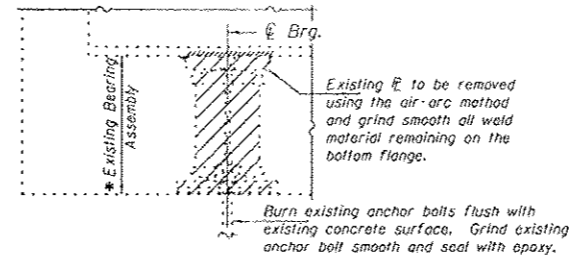
SECTION THRU PTFE



RT. SIDE RETAINER

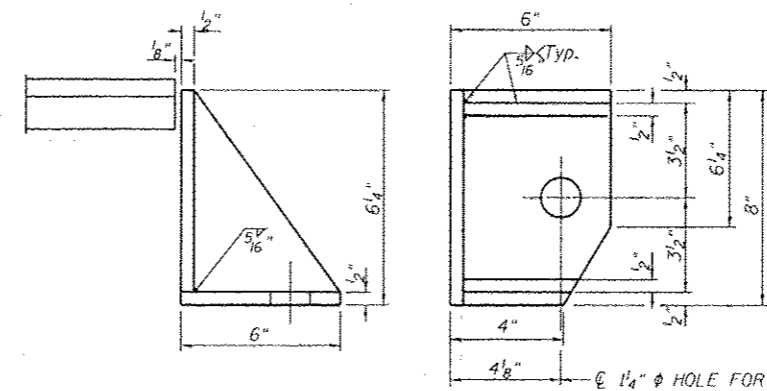
Notes:  
 Diaphragm removal and reinstallation may be required to facilitate drilling holes. Cost included with Furnishing and Erecting Structural Steel.  
 New steel extensions, shim plates and connection bolts are included with Furnishing and Erecting Structural Steel. Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions. Min. jack capacity = 50 Tons.  
 Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.  
 Anchor bolts for Type II bearings shall be placed in holes drilled through the bottom bearing plate after members are in place. Side retainers shall be placed after bolts are installed.  
 Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications. Side retainers shall be included in the cost of Elastomeric Bearing Assembly, Type II.  
 The 1/8" PTFE sheet shall be bonded directly to the top steel plate with a two-component, medium viscosity epoxy resin, conforming to the requirements of the Federal Specification MMM-A-134, Type I. The bond agent shall be applied on the full area of the contact surfaces.  
 Bonding of 1/8" PTFE sheet during vulcanizing process will be permitted provided the process and method of adjusting assembly height is approved by the Engineer.

\*Vertical dimensions between top of concrete bearing seat and bottom of steel girder have been taken from existing field conditions. The Contractor shall verify such dimensions prior to ordering the fabrication of steel extensions.



EXISTING BEARING REMOVAL DETAIL

Cost included with Jack and Remove Existing Bearings.



LT. SIDE RETAINER

SIDE RETAINER

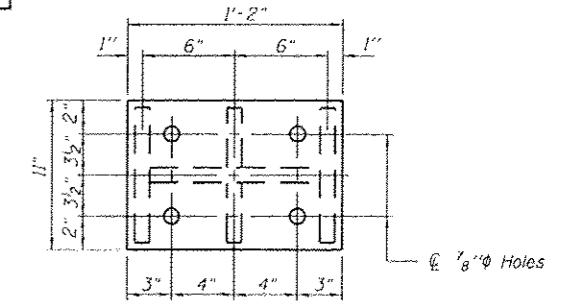
Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

BEAM REACTIONS

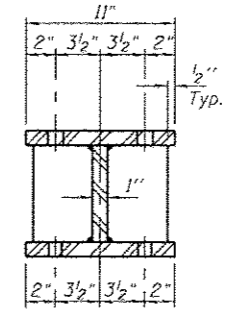
RP	(K)	10.5
RL	(K)	41.7
Imp.	(K)	9.8
R (Total)	(K)	92.0

STEEL EXTENSION W. ABUT.

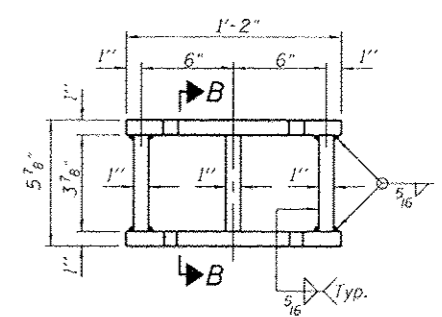
BEAM 1	5/8" P
BEAM 2	3/4" P
BEAM 3	3/4" P
BEAM 4	3/4" P
BEAM 5	0 P



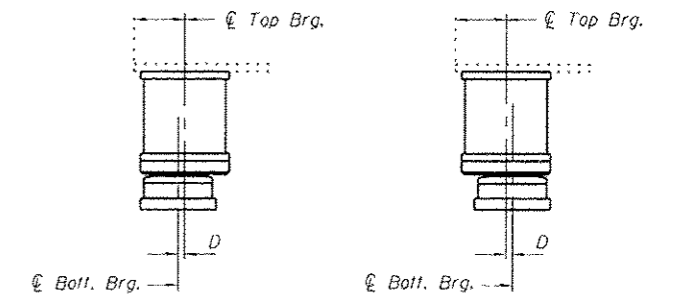
PLAN TOP AND BOTTOM PLATE



SECTION B-B



STEEL EXTENSION DETAIL



BELOW 50° F. (Move bott. brg. away from fixed brg.) ABOVE 50° F. (Move bott. brg. toward fixed brg.)

SETTING ANCHOR BOLTS AT EXP. BRG.

D = 1/8" per each 100" of expansion for every 15° temp. change from the normal temp. of 50°F.

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type II	Each	5
Jack and Remove Existing Bearings	Each	5
Furnishing and Erecting Structural Steel	Pound	657
Anchor Bolts 1"φ	Each	10

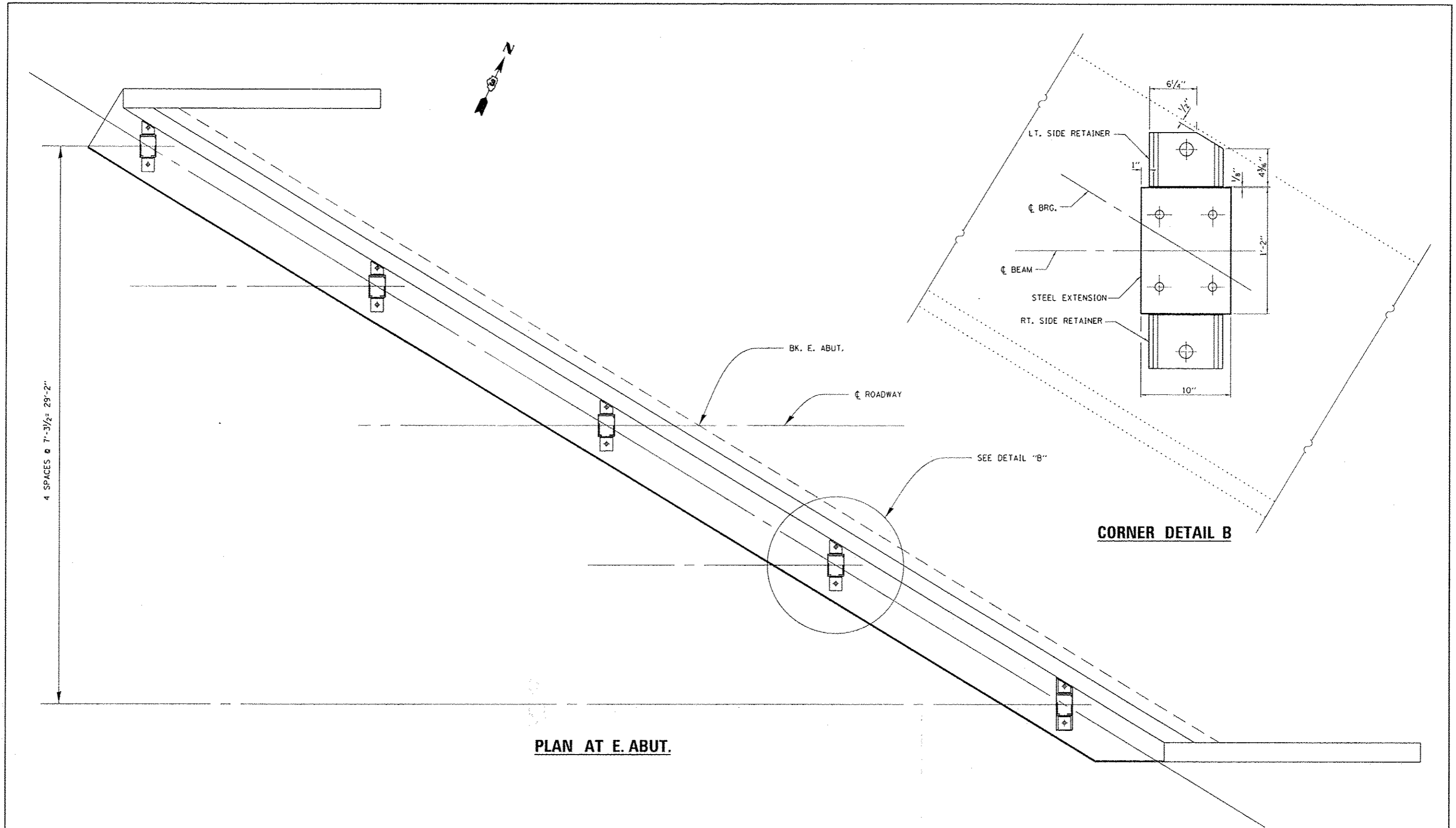
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		ARS	-
		DATE :	REVISED :
			-

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

ELASTOMERIC BEARING ASSEMBLY DETAILS AT  
 WEST ABUTMENT FOR STRUCTURE NO. 019-0032

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	(116VB-1-1BR)	DEKALB	21	15
			CONTRACT NO. 66C65	
ILLINOIS FED. AID PROJECT				

SCALE: SHEET NO. 9 OF 15 SHEETS STA. 356+13.85 TO STA. 359+13.60



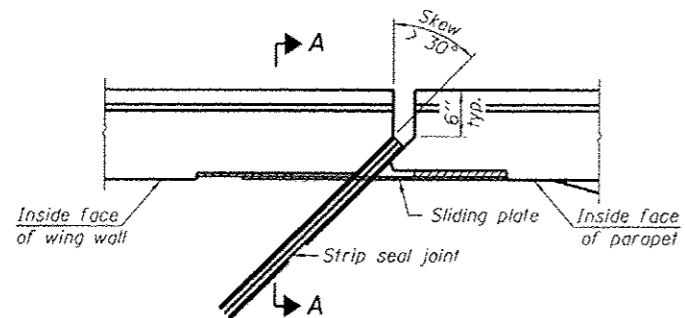
**PLAN AT E. ABUT.**

**CORNER DETAIL B**

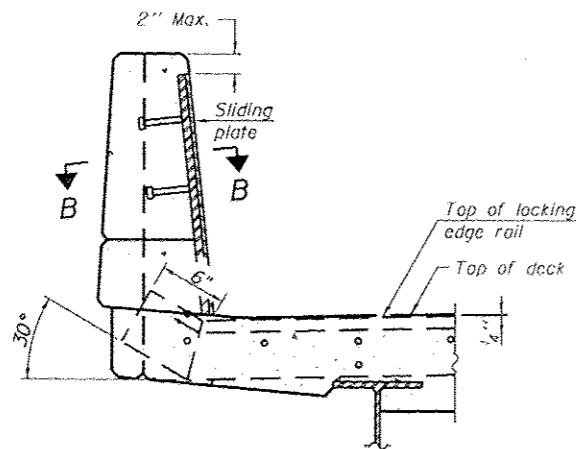
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	PLT SCALE : 100.000 / in.	CHECKED - <i>ARS</i>	REVISED -			573	(116VB-1-IBR)	DEKALB	21	16
PLT DATE : 3/22/2013	DATE -	REVISED -	SCALE: SHEET NO. 10 OF 15 SHEETS STA. 356+13.85 TO STA. 359+13.60			ILLINOIS FED. AID PROJECT				



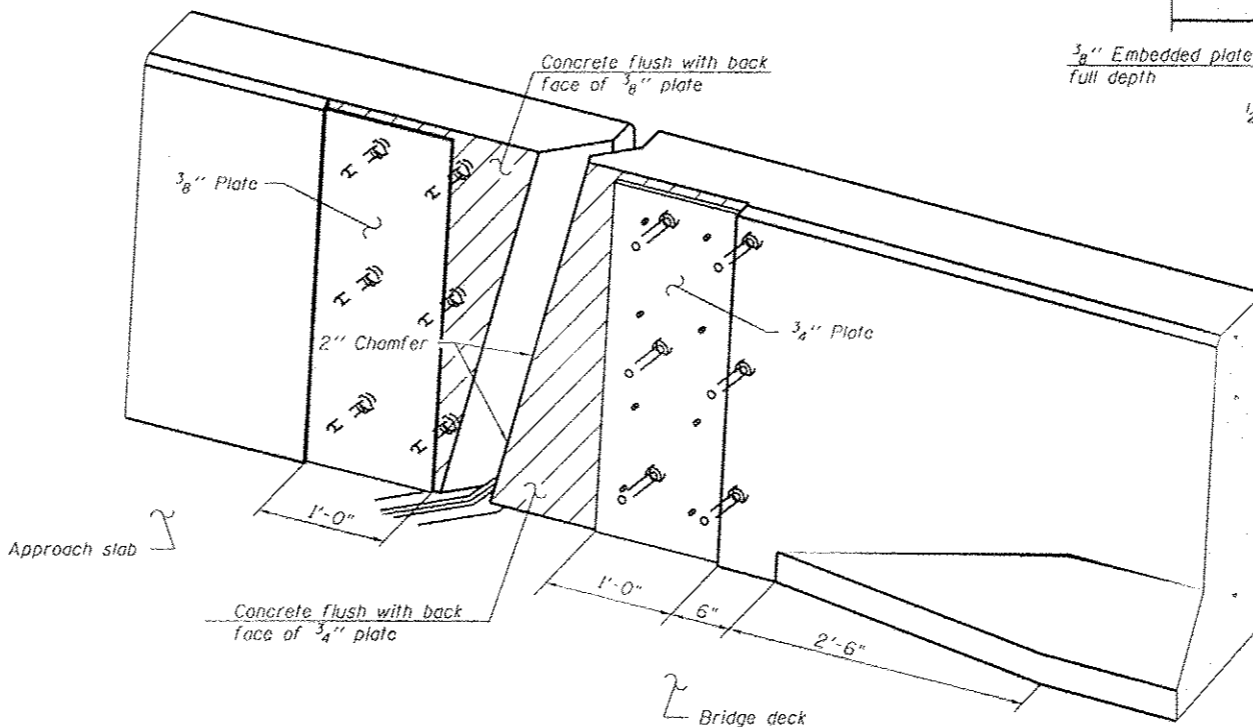




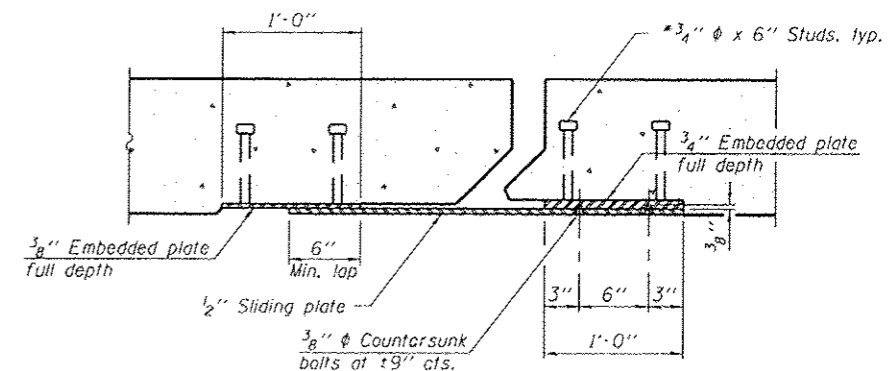
**PLAN**  
(For skews > 30°)  
Showing point block



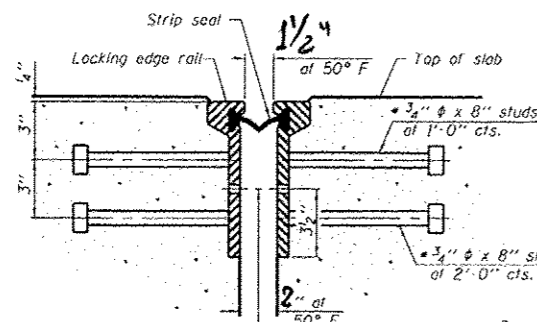
**SECTION A-A**



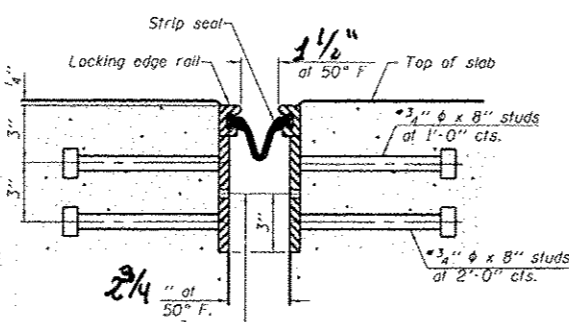
**TRIMETRIC VIEW**  
(Showing back plates only)



**SECTION B-B**



**SECTION THRU ROLLED RAIL JOINT**



**SECTION THRU WELDED RAIL JOINT**

**ROLLED EXTRUDED RAIL**

**WELDED RAIL**

**LOCKING EDGE RAIL SPLICE**

The inside of the locking edge rail groove shall be free of weld residue.  
Rolled rail shown, welded rail similar.

**LOCKING EDGE RAILS**

**Notes:**

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.

The manufacturer's recommended installation methods shall be followed.

The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

Maximum space between rail segments shall be 3/16", sealed with a suitable sealant. Joints in rails within 10 ft. of curbs shall be welded.

Parapet plates and anchorage studs for skews > 30° included in the cost of Preformed Joint Strip Seal.

7/16" diameter holes at 4'-0" cts. for 3/8" diameter bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

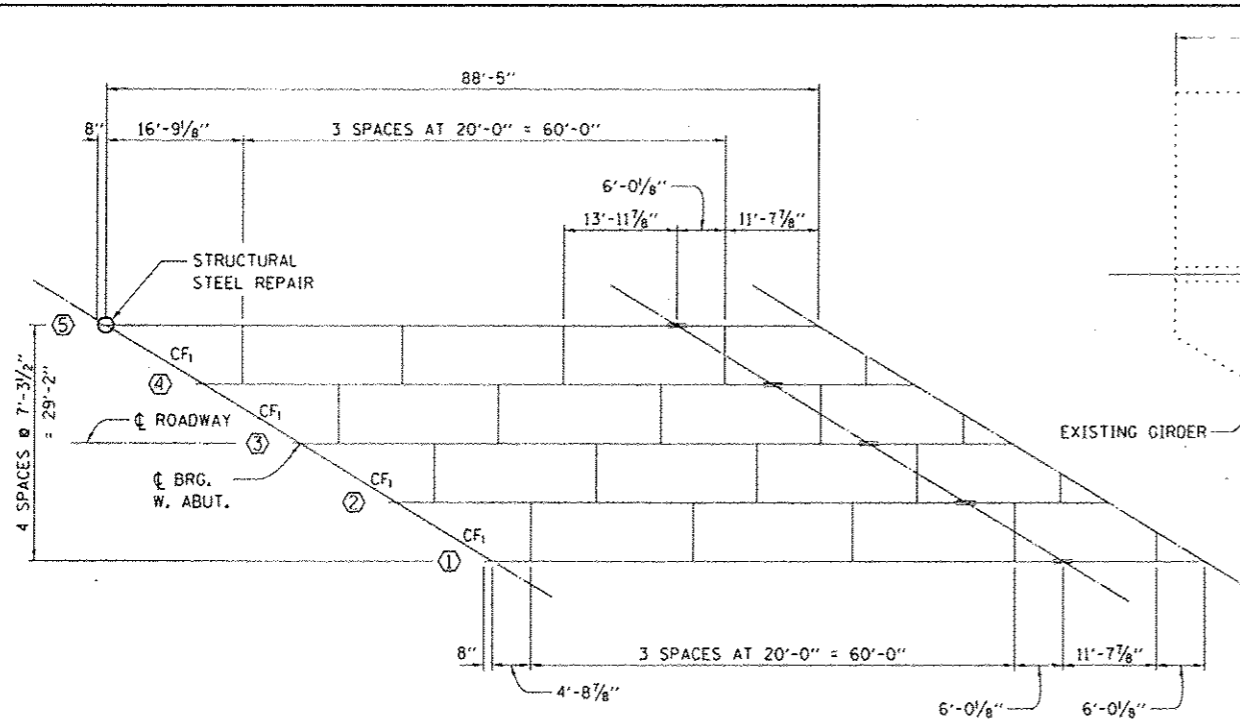
7/16" diameter holes at 4'-0" cts. for 3/8" diameter bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

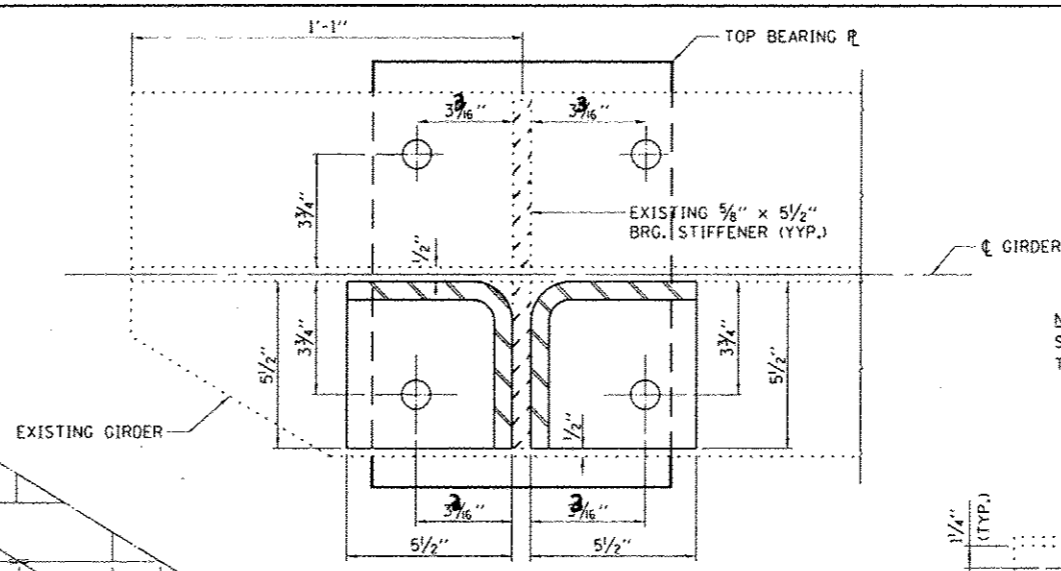
**BILL OF MATERIAL**

Item	Unit	Total
Preformed Joint Strip Seal	Foot	128

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PLOT SCALE: 1/8" = 1'-0"	CHECKED: ARS	REVISED: -	SCALE: SHEET NO. 12 OF 15 SHEETS			STA. 356+13.85 TO STA. 359+13.60	CONTRACT NO. 66C65	ILLINOIS FED. AID PROJECT		
PLOT DATE: 3/22/2013	DATE: -	REVISED: -								

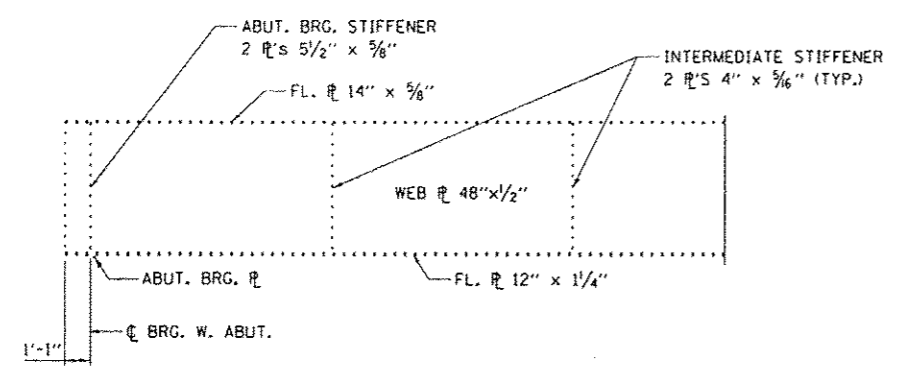


**PARTIAL PLAN**

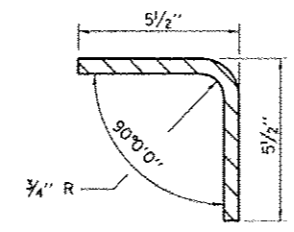


**PLAN AT FASCIA BEAM  
INTERIOR BRG. STIFFENER**

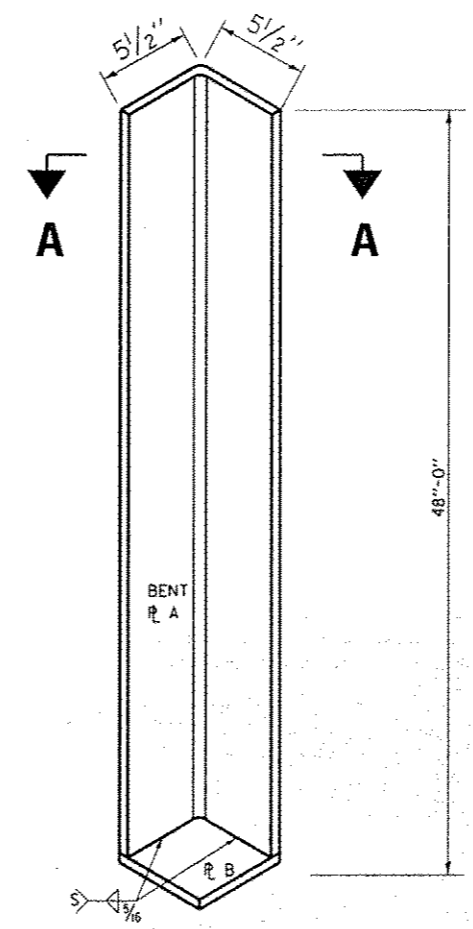
**NOTE:**  
STRUCTURAL STEEL REPAIR MEMBERS DESIGNATED (NTR) SHALL MEET THE CHARPY V-NOTCH IMPACT REQUIREMENTS FOR TEMPERATURE ZONE 2.



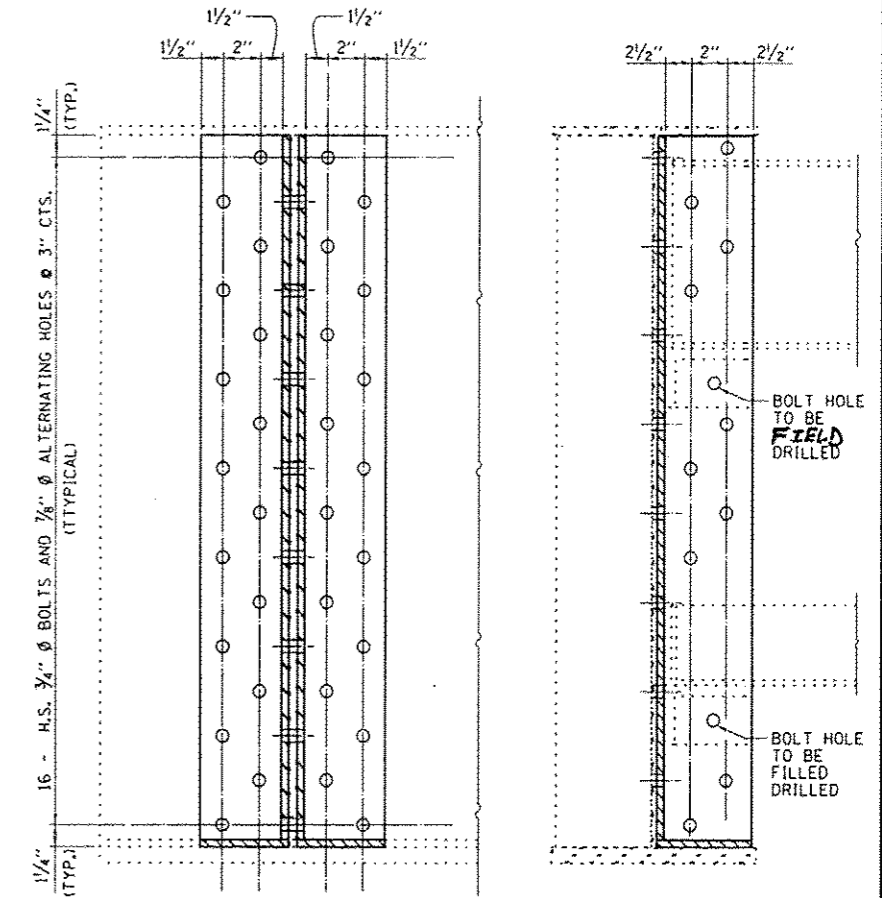
**GIRDER ELEVATION  
INTERIOR FACE**



**SECTION A-A  
12" BENT PLATE (NTR)**

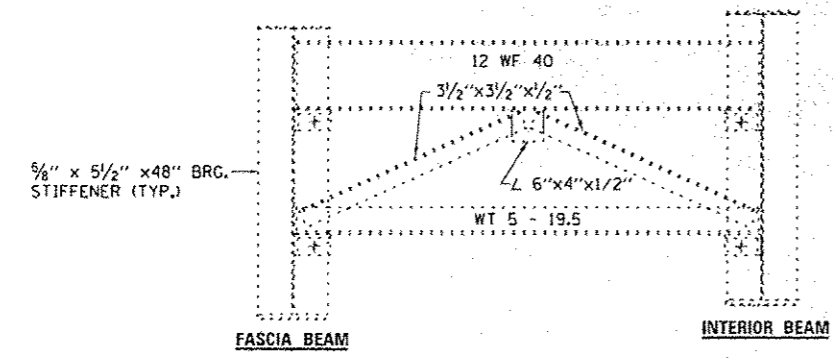


**PLATE ASSEMBLY DETAIL**

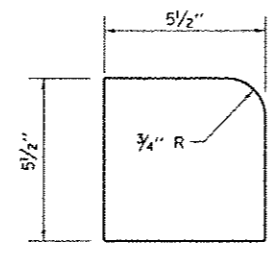


**ELEVATION**

**END ELEVATION**



**CROSS FRAME ELEVATION  
AT W. ABUT.**



**PLATE "B"  
12" PLATE**

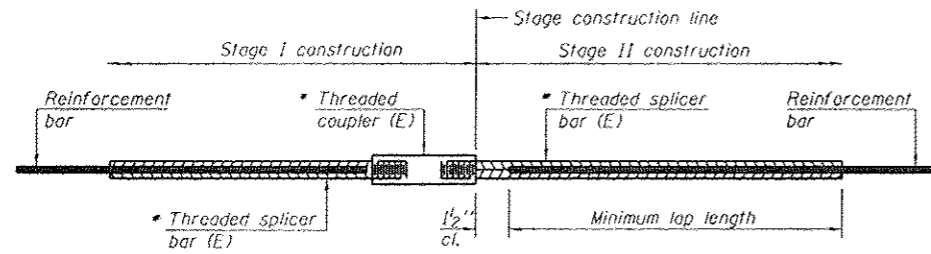
**BILL OF MATERIAL**

Item	Unit	Total
STRUCTURAL STEEL REPAIR	POUND	180

**NOTES**  
WHEN EXISTING CROSS FRAMES AND SUPPORT ANGLES REMOVED FOR THE INSTALLATION OF THE STRUCTURAL STEEL REPAIR, THE CROSS FRAMES AND ANGLE SHALL BE REINSTALLED WITH NEW HIGH STRENGTH BOLTS, NUTS AND WASHERS.

EXISTING CROSS FRAMES AND SUPPORT ANGLES SHALL BE USED AS TEMPLATES TO ESTABLISH LOCATIONS FOR NEW BOLT HOLES. COST OF REMOVING, REINSTALLING THE CROSS FRAMES AND ANGLES WHEN REQUIRED AND DRILLING OF NEW BOLT HOLES SHALL BE INCLUDED IN THE COST OF "STRUCTURAL STEEL REPAIR".

THE CONTRACTOR SHALL FURNISH ADDITIONAL 1/2" SHIM PLATES TO COMPENSATE FOR THE DIFFERENTIAL IN DIMENSIONS OF THE EXISTING SUPPORT ANGLE WHEN REINSTALLED. THE EXISTING ANGLE SHALL MAINTAIN FULL BEARING ACROSS THE CONTACT AREA OF THE ANGLE. COST OF FURNISHING AND INSTALLING THE SHIM PLATES SHALL BE INCLUDED IN THE COST OF "STRUCTURAL STEEL REPAIR".



**STANDARD BAR SPLICER ASSEMBLY**

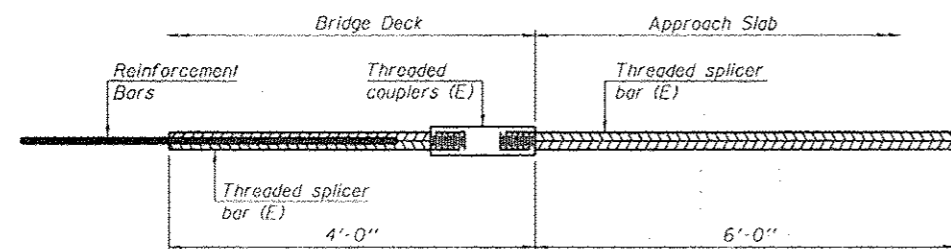
Bar size to be spliced	Minimum Lap Lengths					
	Table 1	Table 2	Table 3	Table 4	Table 5	Table 6
3, 4	1'-5"	1'-11"	2'-1"	2'-4"	2'-7"	2'-11"
5	1'-9"	2'-5"	2'-7"	2'-11"	3'-3"	3'-8"
6	2'-1"	2'-11"	3'-1"	3'-6"	3'-10"	4'-5"
7	2'-9"	3'-10"	4'-2"	4'-8"	5'-2"	5'-10"
8	3'-8"	5'-1"	5'-5"	6'-2"	6'-9"	7'-8"
9	4'-7"	6'-5"	6'-10"	7'-9"	8'-7"	9'-8"

- Table 1: Black bar, 0.8 Class C
- Table 2: Black bar, Top bar lap, 0.8 Class C
- Table 3: Epoxy bar, 0.8 Class C
- Table 4: Epoxy bar, Top bar lap, 0.8 Class C
- Table 5: Epoxy bar, Class C
- Table 6: Epoxy bar, Top bar lap, Class C

Threaded splicer bar length = min. lap length + 1/2" + thread length

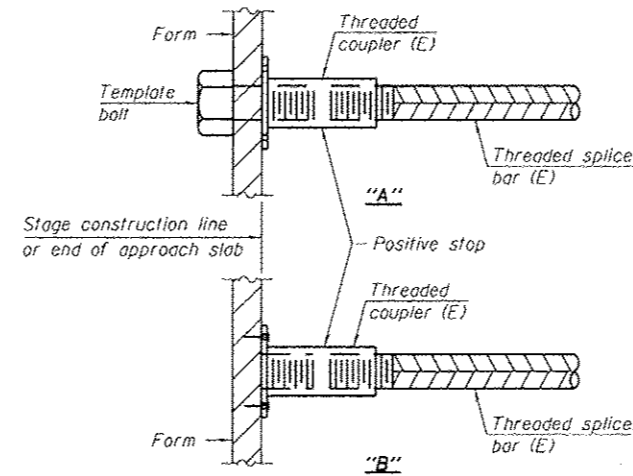
\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length
W. ABUT - DECK	#5	9	3
W. ABUT. HATCHBLOCK	#6	6	3
W. ABUT. - APPROACH	#5	3	3
E. ABUT - DECK	#5	9	3
E. ABUT. HATCHBLOCK	#6	6	3
E. ABUT. - APPROACH	#5	3	3



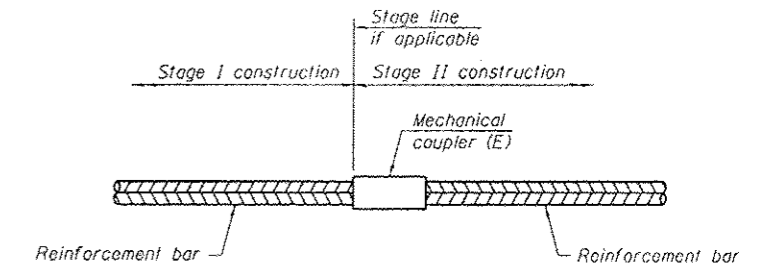
**BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS**

No. required =



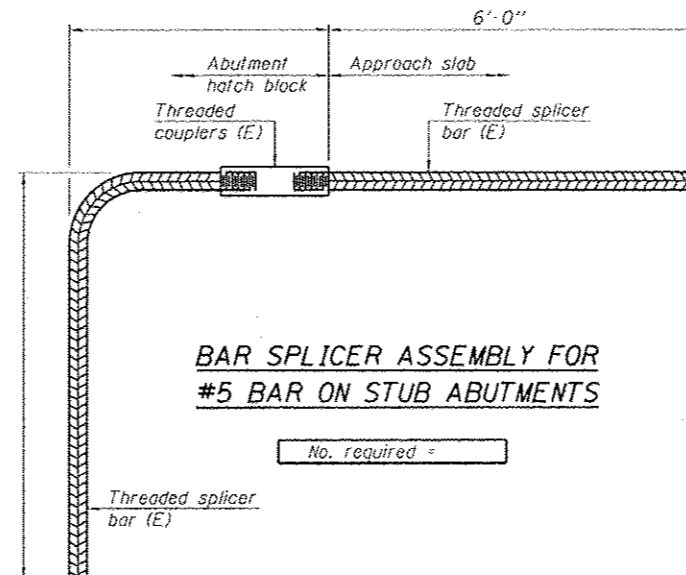
**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.  
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
 (E) : Indicates epoxy coating.



**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required



**BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS**

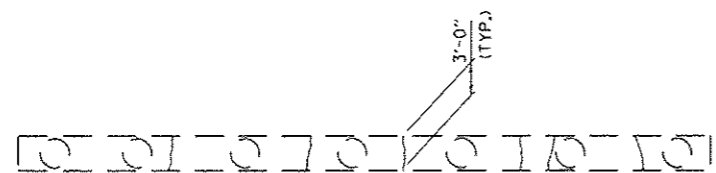
No. required =

**NOTES**

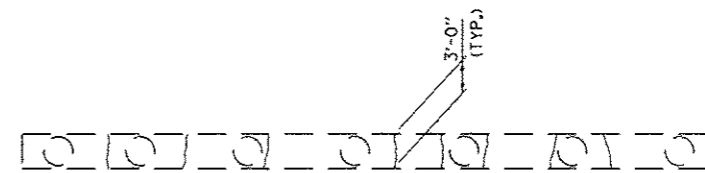
- Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
- All reinforcement shall be lapped and tied to the splicer bars.
- Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
- See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

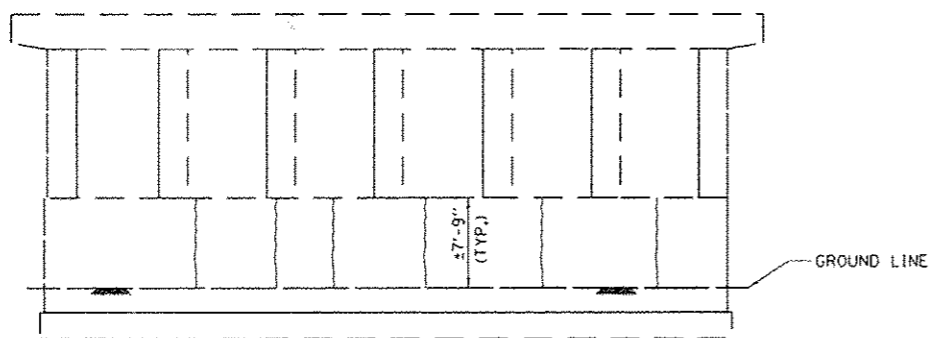
1-27-12



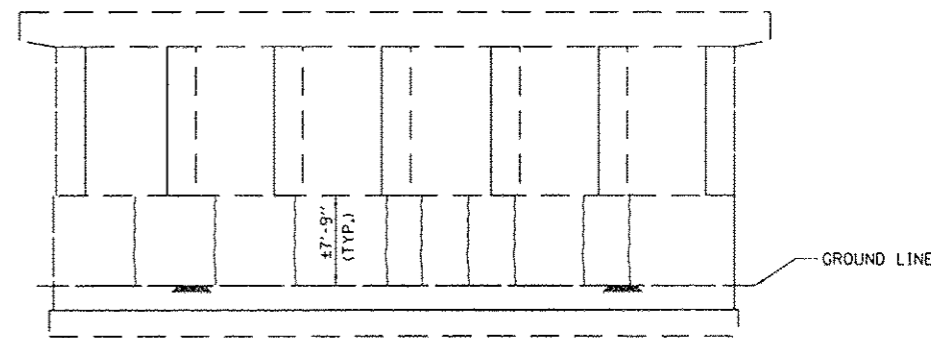
**PLAN**



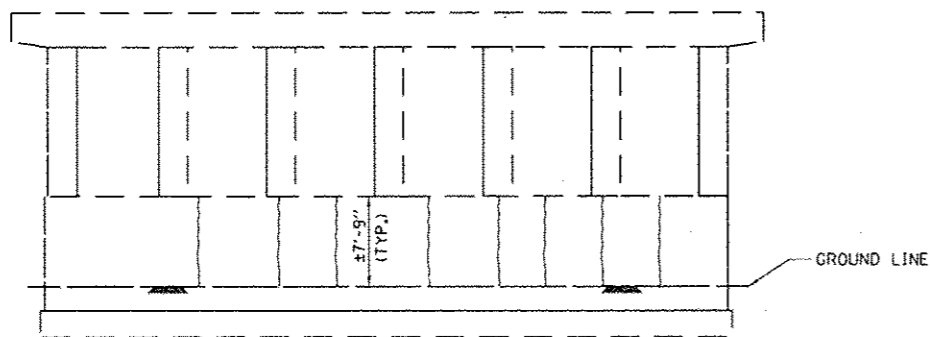
**PLAN**



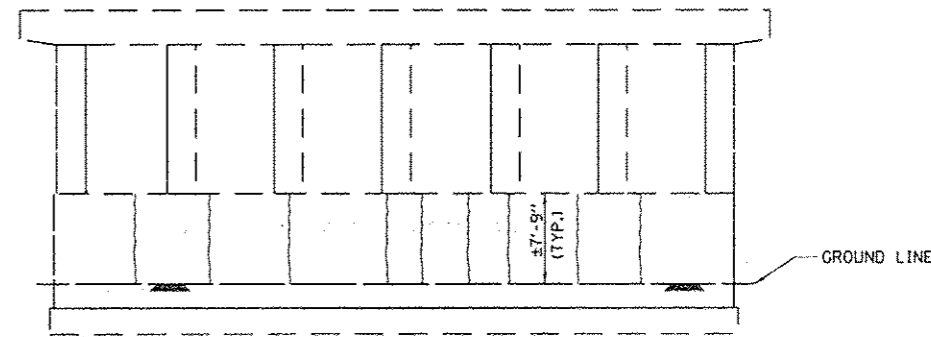
**PIER 1  
LOOKING EAST**



**PIER 2  
LOOKING EAST**



**PIER 1  
LOOKING WEST**



**PIER 2  
LOOKING WEST**

**BILL OF MATERIAL**

Item	Unit	Total
EPOXY CRACK INJECTION	Foot	290

FILE NAME : c:\p\work\p\p\dot\woodshankr1\d0320292\0366C65-shr\detailed.dgn	USER NAME : woodshankr1	DESIGNED - RW	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	EPOXY CRACK INJECTION DETAILS FOR STRUCTURE NO. 019-0032	F.A.P. RTE. 573	SECTION (I16VB-1-IBR)	COUNTY DEKALB	TOTAL SHEETS 21	SHEET NO. 21	
PLOT SCALE = 1/8" = 1'-0"	CHECKED - ARS	DATE -	REVISED -			SCALE:	SHEET NO. 15 OF 15 SHEETS	STA. 356+13.85 TO STA. 359+13.60	ILLINOIS FED. AID PROJECT		
PLOT DATE = 3/22/2013	DATE -	REVISED -	REVISED -								