

ITEM 21A
04-26-2013 LETTING

CONSTRUCTION PLANS FOR CENTRAL ILLINOIS REGIONAL AIRPORT

BL069
TOTAL SHEETS: 55

BLOOMINGTON-NORMAL AIRPORT AUTHORITY
BLOOMINGTON, ILLINOIS

ILLINOIS PROJECT BMI - 4288
A. I. P. PROJECT 3-17-0006-XX

EXPAND AIR CARGO APRON

MARCH 19, 2013



COMMON GROUND ALLIANCE
www.call811.com or
Phone: 811

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.

DESIGN INFORMATION

GEOMETRIC CRITERIA

TAXIWAY DESIGN GROUP IV
AIRPLANE DESIGN GROUP IV
AIRCRAFT APPROACH CATEGORY D

PAVEMENT DESIGN CRITERIA

BOEING B-757
DEPARTURE WEIGHT 256,000
130 ANNUAL DEPARTURES

PAVEMENT DESIGN CRITERIA

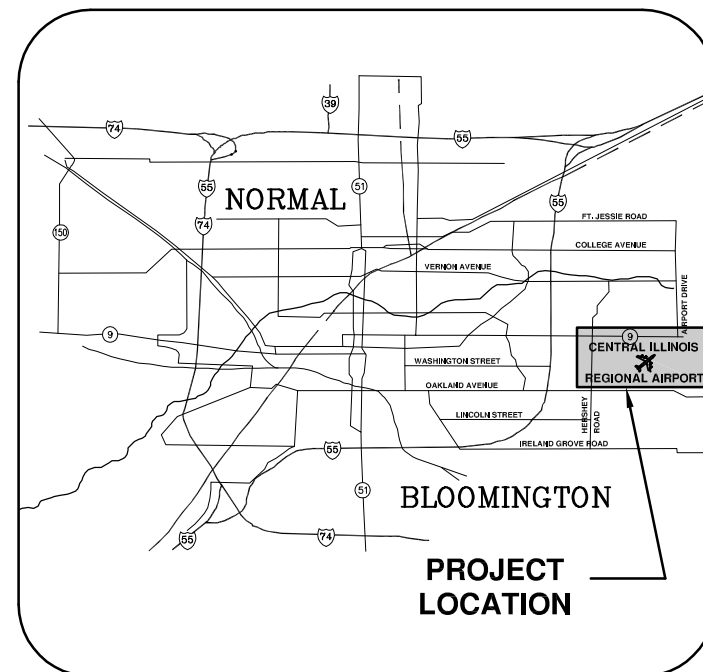
BOEING B-767
DEPARTURE WEIGHT 361,000
130 ANNUAL DEPARTURES

ROADWAY DESIGN CRITERIA

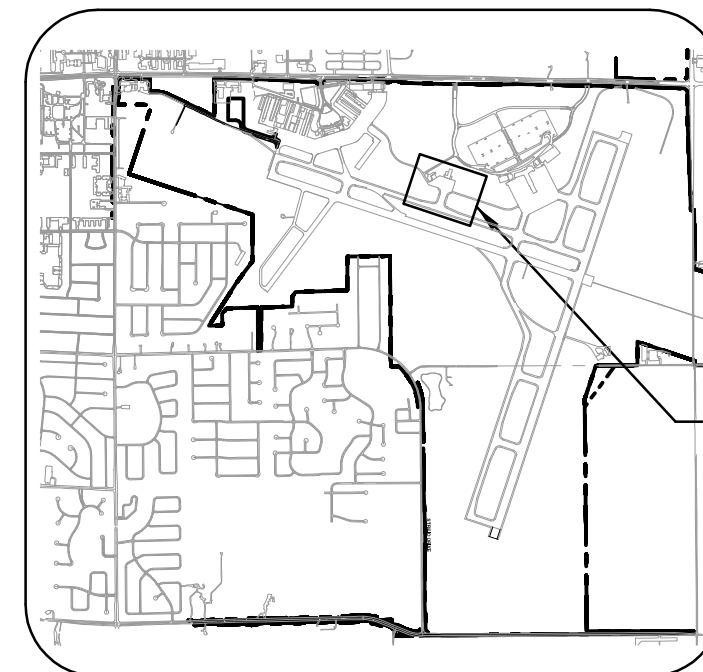
LOW VOLUME LOCAL STREET <20 MPH
ALIGNMENT CURVE RADIUS 500'
MINIMUM K VALUE = 50
DESIGN VEHICLE - 60,000 LBS 2 AXLE FUEL TRUCK

CALL J.U.L.I.E.
BEFORE EXCAVATING
1-800-892-0123

TOWNSHIP: 23 NORTH
RANGE: 3 EAST OF THE 4TH P.M.
SECTION: 6
COUNTY: McLEAN
CIVIL TOWNSHIP: BLOOMINGTON CITY
& OLD TOWN



LOCATION MAP



SITE PLAN

Exp. 11-30-2013



Chris B. Groth
3-28-2013

BLOOMINGTON-NORMAL AIRPORT AUTHORITY
CENTRAL ILLINOIS REGIONAL AIRPORT
BLOOMINGTON, ILLINOIS

APPROVED *[Signature]*
EXECUTIVE DIRECTOR OF AVIATION

DATE 3/28/13



CRAWFORD MURPHY & TILLY, INC.
CONSULTING ENGINEERS
SPRINGFIELD, IL ■ AURORA, IL ■ ST. LOUIS, MO

SUBMITTED BY *Chris B. Groth*

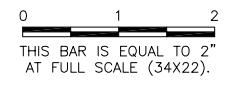
DATE 3-28-2013 2013

CMT JOB NUMBER 12085-05-00

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MODIFIED: 3/19/13 10:20
PLOT # 1
LAST UPDATE:

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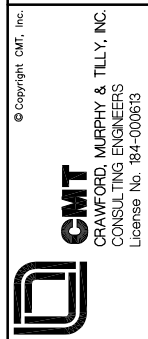
REVISIONS		
NUMBER	BY	DATE



SHEET LIST TABLE	
Sheet Number	Sheet Title
01	COVER SHEET
02	INDEX TO SHEETS & SUMMARY OF QUANTITIES
03	SITE PLAN
04	CONSTRUCTION ACTIVITY PLAN INDEX
05	CONSTRUCTION ACTIVITY PLAN 1
06	CONSTRUCTION ACTIVITY PLAN 2
07	CONSTRUCTION SAFETY PHASING PLAN NOTES
08	EXISTING CONDITIONS AND REMOVALS
09	EXISTING PAVEMENT STRUCTURE
10	EXISTING PAVEMENT REPAIR PLAN
11	PROPOSED IMPROVEMENTS
12	PERIMETER ROAD PLAN AND PROFILE
13	EAST EDGE AND PERIMETER ROAD CULVERT PLAN AND PROFILE
14	SOUTH PAVED DITCH PLAN AND PROFILE
15	TYPICAL SECTIONS 1
16	TYPICAL SECTIONS 2
17	JOINTING PLAN 1
18	JOINTING PLAN 2
19	JOINTING DETAILS
20	STAKING PLAN
21	PAVING DETAILS 1
22	NOSE TETHER DETAIL
23	SITE GRADING
24	DRAINAGE IMPROVEMENTS
25	DRAINAGE DETAILS 1
26	DRAINAGE DETAILS 2
27	UNDERDRAIN DETAILS
28	MARKING PLAN
29	MARKING DETAILS
30	ROADWAY SIGNAGE DETAILS
31	TAXIWAY LIGHTING PLAN
32	ELECTRICAL DETAILS 1
33	ELECTRICAL DETAILS 2
34	ELECTRICAL DETAILS 3
35	APRON LIGHTING PLAN
36	APRON LIGHTING DETAILS
37	ELECTRIC GATE DETAILS 1
38	ELECTRIC GATE DETAILS 2
39	FENCE DETAILS 1
40	FENCE DETAILS 2
41	INDEX TO CROSS SECTIONS & EROSION CONTROL
42	EROSION CONTROL DETAILS
43	EARTHWORK SUMMARY
44	EAST APRON CROSS SECTION SHEET 1
45	EAST APRON CROSS SECTION SHEET 2
46	EAST APRON CROSS SECTION SHEET 3
47	SOUTH APRON CROSS SECTION SHEET 1
48	SOUTH APRON CROSS SECTION SHEET 2
49	SOUTH APRON CROSS SECTION SHEET 3
50	SOUTH APRON CROSS SECTION SHEET 4
51	SOUTH APRON CROSS SECTION SHEET 5
52	PERIMETER ROAD CROSS SECTION SHEET 1
53	PERIMETER ROAD CROSS SECTION SHEET 2
54	PERIMETER ROAD CROSS SECTION SHEET 3
55	PAVED DITCH CROSS SECTION SHEET 1

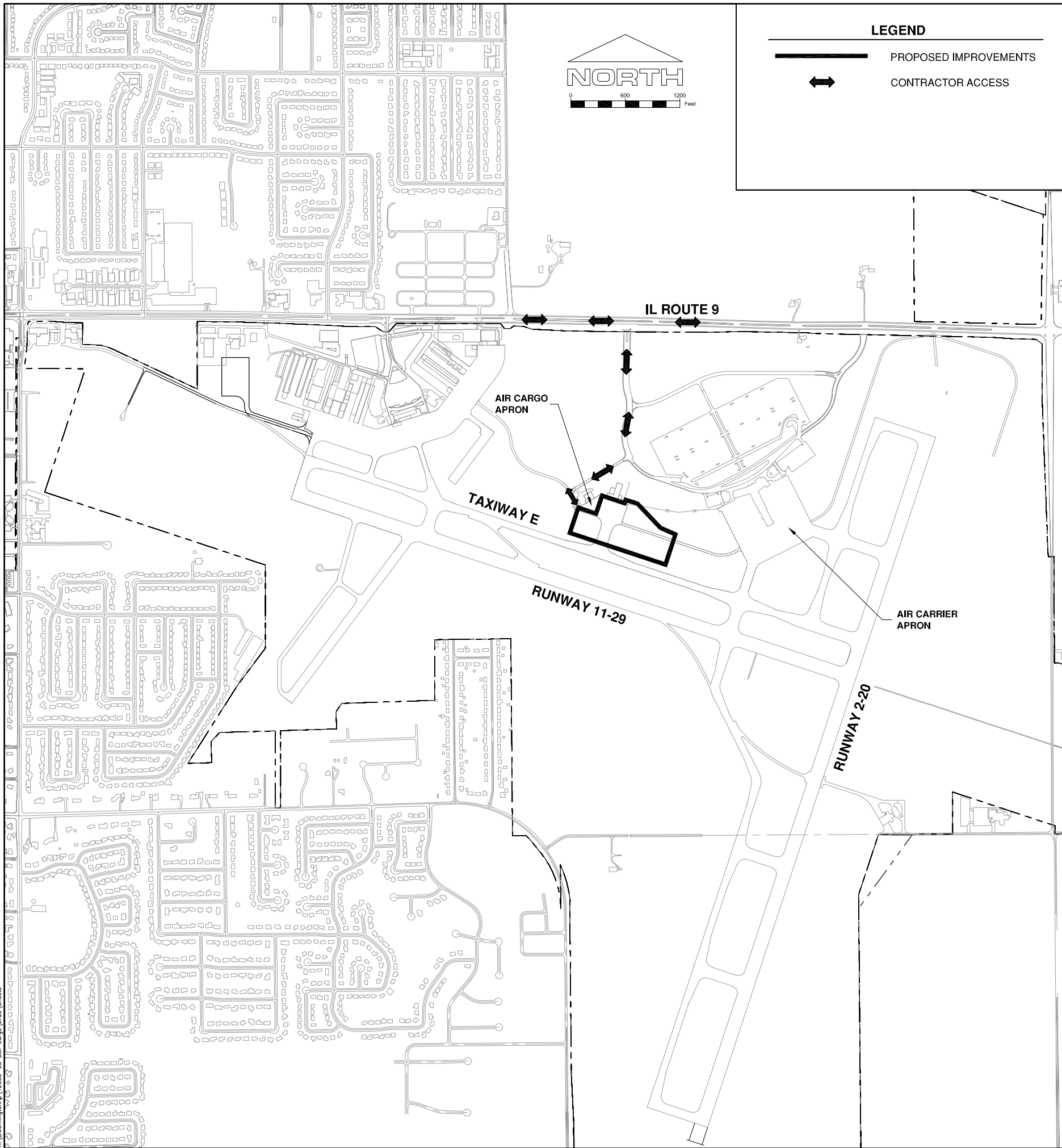
SUMMARY OF QUANTITIES			
ITEM NO.	Description	UNIT	QUANTITY
AR106502	APRON LIGHT POLE W/DOUBLE FIXTURE	EA	2
AR106503	APRON LIGHT POLE W/TRIPLE FIXTURE	EA	2
AR106905	REMOVE LIGHT POLE & FIXTURE	EA	1
AR108000	CABLE TRENCH	LF	212
AR108052	POWER CABLE IN CONDUIT	LF	400
AR108108	1/C #8 5 KV UG CABLE	LF	1,650
AR110011	1" DIRECTIONAL BORE	LF	350
AR110504	4-WAY CONCRETE ENCASED DUCT	LF	45
AR110610	ELECTRICAL HANDHOLE	EA	1
AR800312	2" HDPE, DIRECT BURY	LF	780
AR125415	MITL - BASE MOUNTED	EA	9
AR125416	MITL - BASE MOUNTED IN SHOULDER	EA	4
AR125565	SPLICE CAN	EA	2
AR125902	REMOVE BASE MOUNTED LIGHT	EA	17
AR150510	ENGINEER'S FIELD OFFICE	LS	1
AR152410	UNCLASSIFIED EXCAVATION	CY	7,200
AR156500	TEMPORARY EROSION CONTROL	LS	1
AR162510	CLASS E FENCE 10'	LF	155
AR162900	REMOVE CLASS E FENCE	LF	137
AR162964	RELOCATE GATE	EA	1
AR162969	RELOCATE GATE OPERATOR	EA	1
AR209606	CRUSHED AGG. BASE COURSE - 6"	SY	9,775
AR401900	REMOVE BITUMINOUS PAVEMENT	SY	875
AR501509	9" PCC PAVEMENT	SY	1,775
AR501515	15" PCC PAVEMENT	SY	7,900
AR501530	PCC TEST BATCH	EA	1
AR501900	REMOVE PCC PAVEMENT	SY	2,350
AR510520	NOSE TETHER	EA	2
AR605540	CLEAN & SEAL JOINTS	LF	21,000
AR605541	CLEAN & SEAL CRACKS	LF	500
AR620520	PAVEMENT MARKING-WATERBORNE	SF	3,000
AR620525	PAVEMENT MARKING-BLACK BORDER	SF	3,000
AR620900	PAVEMENT MARKING REMOVAL	SF	2,500
AR620595	TEMPORARY MARKING & REMOVAL	SF	1,000
AR701524	24" RCP, CLASS IV	LF	150
AR701900	REMOVE PIPE	LF	170
AR705524	4" PERFORATED UNDERDRAIN W/SOCK	LF	1,100
AR705615	UNDERDRAIN OUTFALL	EA	1
AR705635	UNDERDRAIN COLLECTION STRUCTURE	EA	2
AR705640	UNDERDRAIN CLEANOUT	EA	2
AR705645	UNDERDRAIN CONNECTION	EA	6
AR705900	REMOVE UNDERDRAIN	LF	710
AR705904	REMOVE UNDERDRAIN CLEANOUT	EA	2
AR705905	REMOVE COLLECTION STRUCTURE	EA	2
AR752424	PRECAST REINFORCED CONC. FES 24"	EA	4
AR752900	REMOVE END SECTION	EA	4
AR754610	PAVED DITCH	LF	750
AR754910	REMOVE PAVED DITCH	LF	130
AR901510	SEEDING	AC	4.5
AR904510	SODDING	SY	850
AR908510	MULCHING	AC	4.5
AR908520	EXCELSIOR BLANKET	SY	500
AR908525	KNITTED STRAW MAT	SY	1,505
AR910200	ROADWAY SIGN	EA	3
AR910915	REMOVE ROADWAY SIGN	EA	2

BLOOMINGTON-NORMAL AIRPORT AUTHORITY
CENTRAL ILLINOIS REGIONAL AIRPORT
BLOOMINGTON, ILLINOIS
EXPAND AIR CARGO APRON
INDEX TO SHEETS & SUMMARY OF QUANTITIES



DESIGN BY:	CBG
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MARCH 19, 2013
JOB No:	12085-05-00
IL PROJ. NO.	BMI-4288
AIP PROJ. NO.	3-17-0006-XX
SHEET	02 OF 55 SHEETS

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LEGEND

- PROPOSED IMPROVEMENTS
- CONTRACTOR ACCESS

GENERAL NOTES

1. ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
2. THE CONTRACTOR SHALL REMAIN WITHIN THE DESIGNATED WORK AREA AT ALL TIMES DURING CONSTRUCTION. AT NO TIME SHOULD THE CONTRACTOR OR HIS PERSONNEL ACCESS ANY ACTIVE AIRFIELD PAVEMENTS. PRIOR TO BEGINNING CONSTRUCTION THE CONTRACTOR'S SUPERINTENDENT SHALL BE REQUIRED TO ATTEND TRAINING COURSES AT THE AIRPORT TO BECOME AWARE OF POLICIES AND PROCEDURES ASSOCIATED WITH WORKING WITHIN THE AIRPORT SECURITY FENCE.
3. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
4. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS AND STOCKPILES SHALL BE 25' ABOVE GROUND ELEVATION.
5. BROKEN CONCRETE, BITUMINOUS PAVEMENT AND OTHER DEBRIS SHALL BE DISPOSED OF BY THE CONTRACTOR OFF AIRPORT PROPERTY.
6. VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 130' OF THE CENTERLINE OF ACTIVE TAXIWAYS OR 250' FROM THE CENTERLINE OF ACTIVE RUNWAYS.
7. ALL PAVEMENTS, DRIVES, OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
8. EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE AIRPORT.
9. THE CONTRACTOR SHALL CONTINUOUSLY CLEAN ALL CONSTRUCTION AREAS WHICH ARE OR WILL BE OPENED TO AIR TRAFFIC.
10. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER, THE AIRPORT AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
11. THE CONTRACTOR SHALL MAINTAIN A SECURE AIRFIELD PERIMETER THROUGHOUT THE DURATION OF THE PROJECT.
12. CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - A. THE CONTRACTOR SHALL COMPLETE A TEN YEAR CRIMINAL AND EMPLOYMENT BACKGROUND CHECK AND A SECURITY FORM FOR THE EMPLOYEES THAT HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE AIRPORT BY THE CONTRACTOR.
 - B. THE CONTRACTOR SHALL USE AN EXISTING GATE(S) FOR ACCESS TO THE AIRFIELD. THE CONTRACTOR SHALL INSTALL AND MAINTAIN A HEAVY-DUTY PADLOCK ON THE ACCESS GATE. HE SHALL PROVIDE KEYS FOR THIS PADLOCK TO THE RESIDENT ENGINEER AND THE AIRPORT. NO ADDITIONAL KEYS ARE TO BE DISTRIBUTED UNLESS AUTHORIZED BY THE AIRPORT.
 - C. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS. IF THE CONTRACTOR CHOOSES TO LEAVE THE GATE(S) OPEN, THEN HE SHALL POST A COMPETENT SECURITY GUARD TO PREVENT UNAUTHORIZED ENTRIES. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS IF SO DIRECTED.
 - D. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T..
 - E. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
 - F. THE CONTRACTOR WILL CLOSE AND LOCK THE ACCESS GATE(S) UPON LEAVING THE SITE.
 - G. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.

GROUND CONTROL FREQUENCY - 121.65
 AIR CONTROL FREQUENCY - 124.60
 MAXIMUM HEIGHT OF TYPICAL EQUIPMENT - 25 FT.

FILE: 1208505-GC001.dwg
 UPDATE BY: Chris Groth
 PLOT DATE: 4/4/2013 12:47 PM

1208505-C-SPAL
 1208505-V-VF3D
 BMI-V-AERL
 BMI-V-SPXP

BL069

REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

**EXPAND AIR CARGO APRON
 SITE PLAN**

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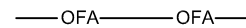



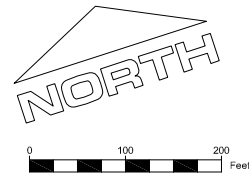
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DRAWN BY:	CMT
CHECKED BY:	RLV
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DATE:	MARCH 19, 2013
JOB No:	12085-05-00
IL PROJ. NO.	BMI-4288
AIP PROJ. NO.	3-17-0006-XX
SHEET	03 OF 55 SHEETS

GENERAL PHASING NOTES

- PROJECT WILL BE SPLIT INTO 2 PHASES WITH PHASE 2 HAVING 2 SUB-PARTS.
- PHASE 1 SHALL BE CONSIDERED THE PRIORITY WORK AREA AS IT WILL REQUIRE THE AIRPORT TO REROUTE FUEL TRUCKS AROUND THE WORK AREA. THE CONTRACTOR WILL HAVE 60 DAYS TO COMPLETE PHASE 1.
- PHASE 2 SHALL BE CONSIDERED A SECONDARY WORK AREA. CONSTRUCTION ACTIVITIES WITHIN PHASE 2A MAY BE CONSTRUCTED CONCURRENT WITH PHASE 1.
- PHASE 2B SHALL NOT BE CONSTRUCTED CONCURRENT WITH PHASE 1.
- CONSTRUCTION ACTIVITIES WILL BE PHASED TO LIMIT INTERRUPTING ACCESS TO AND FROM THE GROWMARK HANGAR.
- ALL CONSTRUCTION ACTIVITIES WILL REMAIN OUTSIDE OF THE TAXIWAY E SAFETY AREA SHOWN.
- THE CONTRACTOR SHALL PROVIDE BADGED PERSONNEL TO ACT AS A GATE GUARD ON DAYS WHEN DELIVERY TRUCKS ARE IN USE.
- THE TOPSOIL SHALL BE STRIPPED AND STOCKPILED TO COVER THE EXCESS EXCAVATION STOCKPILE AFTER PLACING. CUT STOCKPILE SHALL BE COMPACTED IN ACCORDANCE WITH ITEM 152.

LEGEND

-  PROPOSED IMPROVEMENTS
-  OFA — OFA — OBJECT FREE AREA
-  CONTRACTOR ACCESS



CRITICAL POINT TABLE			
POINT	LATITUDE	LONGITUDE	ELEVATION
1	N40° 28' 57.66"	W88° 55' 15.79"	858.20
2	N40° 28' 54.91"	W88° 55' 16.95"	861.13
3	N40° 28' 53.95"	W88° 55' 13.05"	863.55
4	N40° 28' 53.79"	W88° 55' 12.43"	863.64
5	N40° 28' 53.64"	W88° 55' 11.81"	863.59
6	N40° 28' 51.54"	W88° 55' 03.30"	865.00
7	N40° 28' 54.83"	W88° 55' 01.90"	861.00
8	N40° 28' 57.90"	W88° 55' 18.56"	858.20
9	N40° 28' 58.36"	W88° 55' 17.37"	858.20
10	N40° 28' 59.48"	W88° 55' 19.59"	858.20
11	N40° 28' 59.94"	W88° 55' 18.40"	858.20
12	N40° 29' 02.06"	W88° 55' 21.25"	858.20
13	N40° 29' 01.70"	W88° 55' 19.80"	858.20


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1208505-C-SPAL
 1208505-V-VF3D
 BMI-V-AERL
 BMI-V-SPXP

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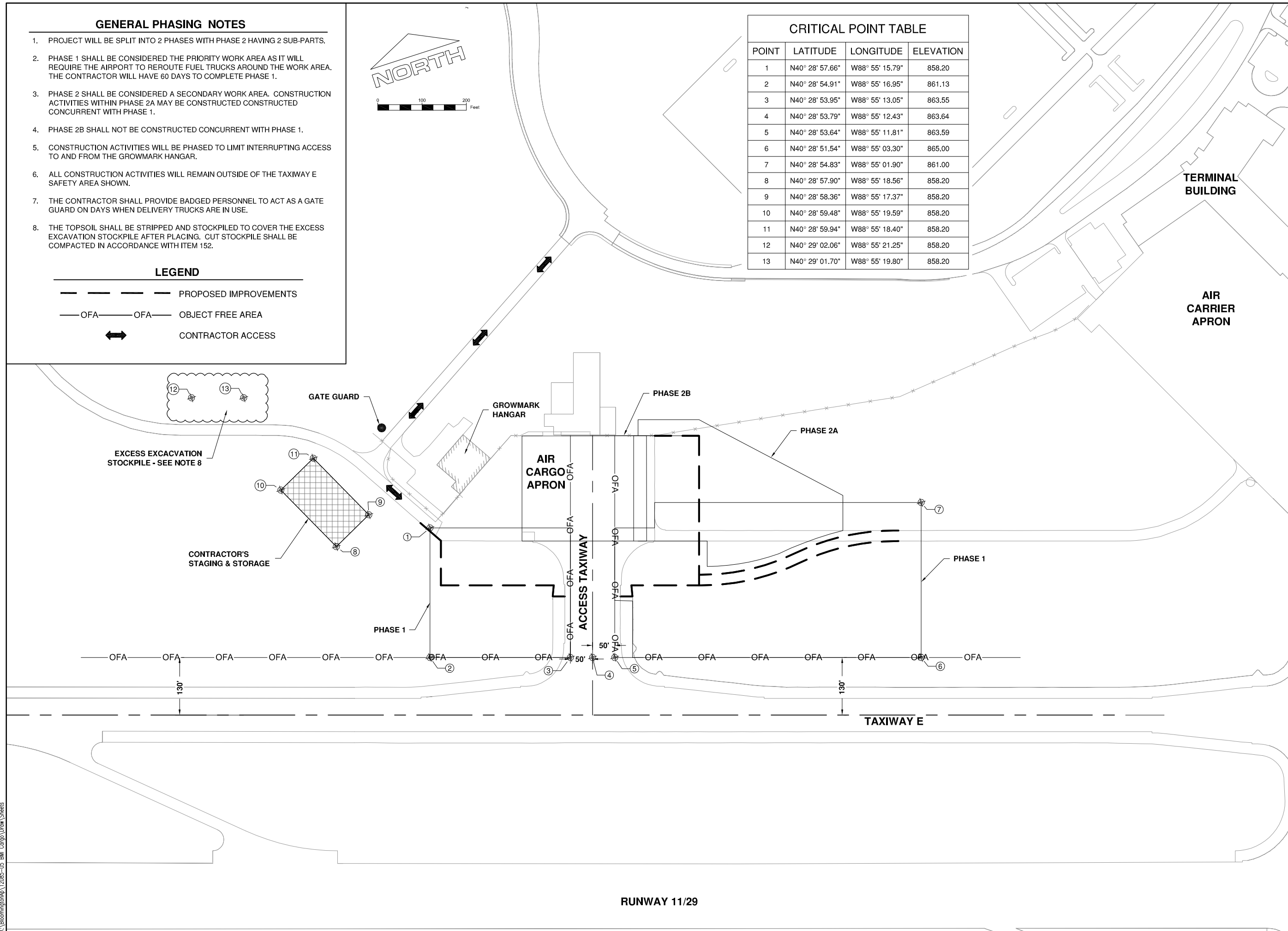

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BLOOMINGTON-NORMAL AIRPORT AUTHORITY
CENTRAL ILLINOIS REGIONAL AIRPORT
BLOOMINGTON, ILLINOIS
EXPAND AIR CARGO APRON
CONSTRUCTION ACTIVITY PLAN INDEX

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IL PROJ. NO. BMI-4288 AIP PROJ. NO. 3-17-0006-XX	
SHEET 04 OF 55 SHEETS	

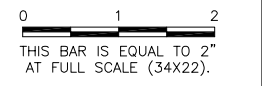


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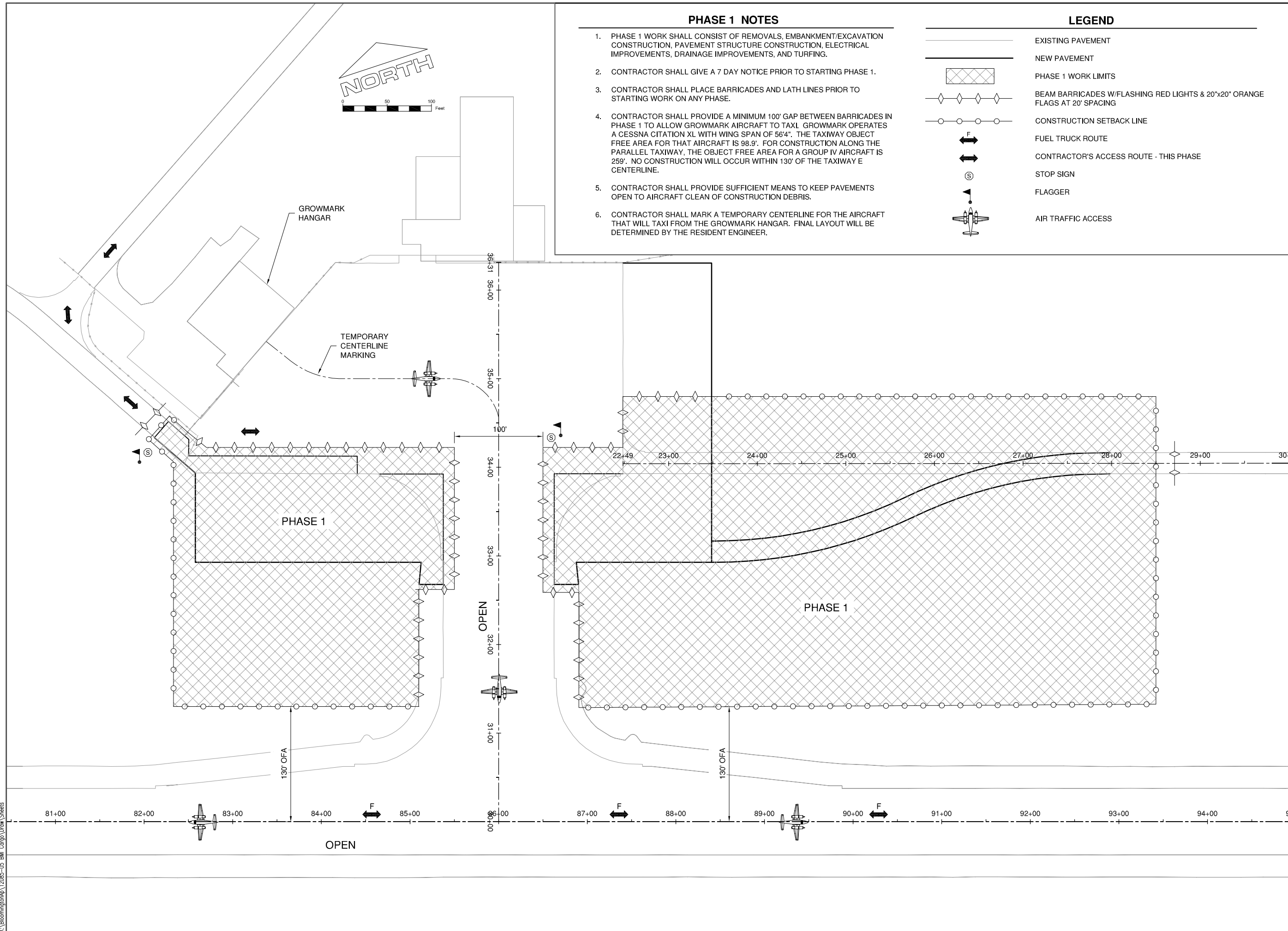
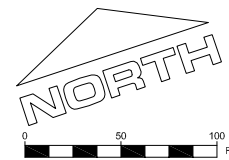


PHASE 1 NOTES

1. PHASE 1 WORK SHALL CONSIST OF REMOVALS, EMBANKMENT/EXCAVATION CONSTRUCTION, PAVEMENT STRUCTURE CONSTRUCTION, ELECTRICAL IMPROVEMENTS, DRAINAGE IMPROVEMENTS, AND TURFING.
2. CONTRACTOR SHALL GIVE A 7 DAY NOTICE PRIOR TO STARTING PHASE 1.
3. CONTRACTOR SHALL PLACE BARRICADES AND LATH LINES PRIOR TO STARTING WORK ON ANY PHASE.
4. CONTRACTOR SHALL PROVIDE A MINIMUM 100' GAP BETWEEN BARRICADES IN PHASE 1 TO ALLOW GROWMARK AIRCRAFT TO TAXI. GROWMARK OPERATES A CESSNA CITATION XL WITH WING SPAN OF 56'4". THE TAXIWAY OBJECT FREE AREA FOR THAT AIRCRAFT IS 98.9'. FOR CONSTRUCTION ALONG THE PARALLEL TAXIWAY, THE OBJECT FREE AREA FOR A GROUP IV AIRCRAFT IS 259'. NO CONSTRUCTION WILL OCCUR WITHIN 130' OF THE TAXIWAY E CENTERLINE.
5. CONTRACTOR SHALL PROVIDE SUFFICIENT MEANS TO KEEP PAVEMENTS OPEN TO AIRCRAFT CLEAN OF CONSTRUCTION DEBRIS.
6. CONTRACTOR SHALL MARK A TEMPORARY CENTERLINE FOR THE AIRCRAFT THAT WILL TAXI FROM THE GROWMARK HANGAR. FINAL LAYOUT WILL BE DETERMINED BY THE RESIDENT ENGINEER.

LEGEND

- EXISTING PAVEMENT
- NEW PAVEMENT
- PHASE 1 WORK LIMITS
- BEAM BARRICADES W/FLASHING RED LIGHTS & 20"x20" ORANGE FLAGS AT 20' SPACING
- CONSTRUCTION SETBACK LINE
- FUEL TRUCK ROUTE
- CONTRACTOR'S ACCESS ROUTE - THIS PHASE
- STOP SIGN
- FLAGGER
- AIR TRAFFIC ACCESS

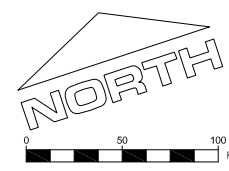


BLOOMINGTON-NORMAL AIRPORT AUTHORITY
CENTRAL ILLINOIS REGIONAL AIRPORT
BLOOMINGTON, ILLINOIS

EXPAND AIR CARGO APRON
CONSTRUCTION ACTIVITY PLAN 1

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DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MARCH 19, 2013
JOB No:	12085-05-00
IL PROJ. NO. BMI-4288 AIP PROJ. NO. 3-17-0006-XX	
SHEET 05 OF 55 SHEETS	



PHASE 2 NOTES

1. PHASE 2 WORK SHALL CONSIST OF REMOVALS, EMBANKMENT CONSTRUCTION, PAVEMENT STRUCTURE CONSTRUCTION, ELECTRICAL IMPROVEMENTS, DRAINAGE IMPROVEMENTS, AND TURFING.
2. CONTRACTOR SHALL GIVE A 7 DAY NOTICE PRIOR TO STARTING PHASE 2.
3. CONTRACTOR SHALL PLACE BARRICADES AND LATH LINES PRIOR TO STARTING WORK ON ANY PHASE.
4. CONTRACTOR SHALL PROVIDE SUFFICIENT MEANS TO KEEP PAVEMENTS OPEN TO AIRCRAFT CLEAN OF CONSTRUCTION DEBRIS.
5. PHASE 2B SHALL INCLUDE REPAIRS TO INDIVIDUAL PANELS ON THE EXISTING APRON.
6. THE TEMPORARY FENCE SHALL BE PLACED, SECURED AND APPROVED BY THE RESIDENT ENGINEER PRIOR TO REMOVAL OF THE PERIMETER FENCE. SEE SECTION 50-20 OF THE SPECIAL PROVISIONS FOR TEMPORARY FENCE REQUIREMENTS. COST FOR THE TEMPORARY FENCE SHALL BE INCIDENTAL.

LEGEND

- EXISTING PAVEMENT
- NEW PAVEMENT
- PHASE 2 WORK LIMITS
- COMPLETED PHASE 1
- BEAM BARRICADES W/FLASHING RED LIGHTS & 20"x20" ORANGE FLAGS AT 20' SPACING
- CONSTRUCTION SETBACK LINE
- TEMPORARY FENCE
- FUEL TRUCK ROUTE
- CONTRACTOR'S ACCESS ROUTE - THIS PHASE
- STOP SIGN
- FLAGGER
- AIR TRAFFIC ACCESS

FILE: 1208505-GC003.dwg
 UPDATE BY: Chris Groth
 PLOT DATE: 4/4/2013 12:50 PM

1208505-C-SPAL
 1208505-V-VF3D
 BMI-V-AERL
 BMI-V-SPXP

BL069

REVISIONS

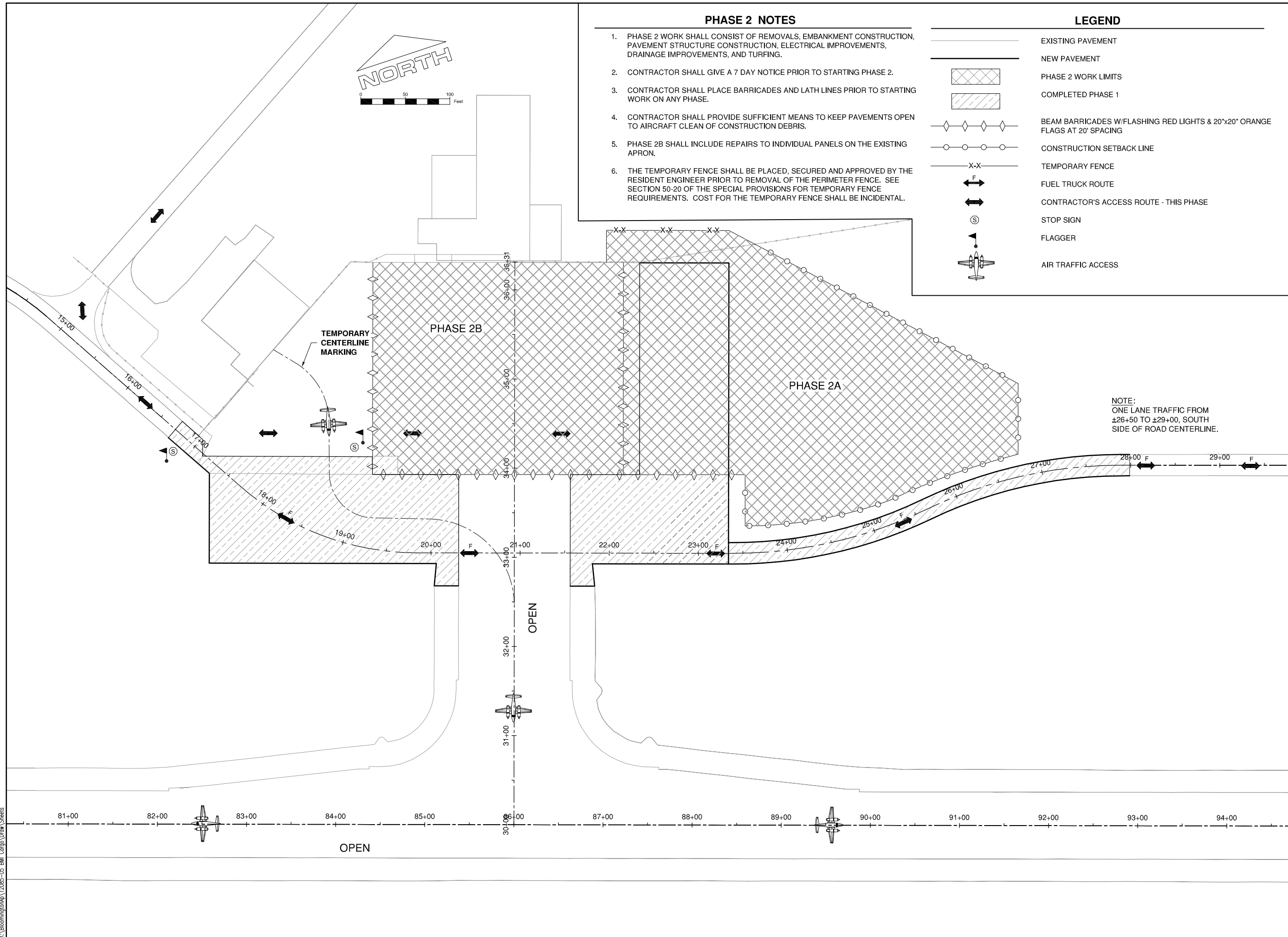
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

**EXPAND AIR CARGO APRON
 CONSTRUCTION ACTIVITY PLAN 2**

NOTE:
 ONE LANE TRAFFIC FROM
 ±26+50 TO ±29+00, SOUTH
 SIDE OF ROAD CENTERLINE.



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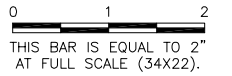
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DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MARCH 19, 2013
JOB No:	12085-05-00
IL PROJ. NO.	BMI-4288
AIP PROJ. NO.	3-17-0006-XX
SHEET	06 OF 55 SHEETS

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GENERAL

- THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2F, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2F. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
- THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
- A MINIMUM OF 10 DAYS PRIOR TO THE PRE-CONSTRUCTION MEETING THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE STORM WATER POLLUTION PREVENTION PROGRAM (SWPPP) CERTIFICATION STATEMENT.
- ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

1. COORDINATION

- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRE-CONSTRUCTION CONFERENCE WITH THE AIRPORT, ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRE-CONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
- ON OR BEFORE THE PRE-CONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON AN AS NEEDED BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
- DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A COORDINATION MEETINGS WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

2. PHASING

- TOTAL CONTRACT TIME SHALL BE 78 CALENDAR DAYS.
- PHASING SHALL BE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

- THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES AT ALL TIMES.

5. CONTRACTOR ACCESS

- CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS.
- THE CONTRACTOR IS TO ACCESS THE SITE USING THE ENTRANCE SHOWN.
- ALL CONTRACTOR EMPLOYEES WHO ARE DESIGNATED AS DRIVERS FOR THE CONTRACTOR AND DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, CONCRETE, ETC.) SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE.
- THE CONTRACTOR'S STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE SITE PLAN.
- THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
- WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
- THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
- ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL NOTIFY THE AIRPORT IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

6. WILDLIFE MANAGEMENT

- THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
- CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED AT ALL TIMES UNLESS GATE GUARD IS PRESENT.
- THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

- THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
- THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

- THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
- THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTICE TO AIRMEN (NOTAMS) MAY BE ISSUED BY THE AIRPORT.
- FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
- IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.

10. INSPECTION REQUIREMENTS

- THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2F MAY BE USED TO AID IN THE INSPECTIONS.
- THE CONTRACTOR SHALL ATTEND A FINAL INSPECTION OF EACH PHASE WORK AREA PRIOR TO OPENING THE AREA TO AIRPORT OPERATIONS.

11. UNDERGROUND UTILITIES

- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION.
- BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

12. PENALTIES

- NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

13. SPECIAL CONDITIONS

- ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. RUNWAY AND TAXIWAY VISUAL AIDS

- NO RUNWAY OR TAXIWAY CLOSURES ARE REQUIRED FOR THIS PROJECT. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOW THE REQUIREMENTS OF FAA AC 150/5370-2F.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET.

15. MARKING AND SIGNS FOR ACCESS ROUTES

- BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED ON THIS SHEET AND THE CONSTRUCTION ACTIVITY PLAN SHEET.

16. HAZARD MARKING AND LIGHTING

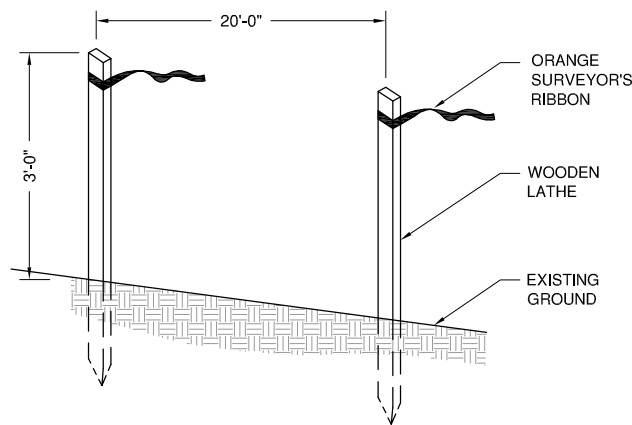
- THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
- ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2F AND 150/5210-5C AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET OR AS DIRECTED BY THE RESIDENT ENGINEER.
- THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.

17. PROTECTION

- ALL WORK REQUIRED INSIDE OF THE RUNWAY SAFETY AREAS, WHICH EXTENDS 250' FROM THE RUNWAY CENTERLINE, WILL REQUIRE THE RUNWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.
- ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY SAFETY AREA, WHICH EXTENDS 85.5' FROM THE TAXIWAY CENTERLINE ALONG TAXIWAY E AND 40' ALONG THE ACCESS TAXIWAY (OR AS SPECIFIED ON THE CONSTRUCTION ACTIVITY PLANS) WILL REQUIRE THE TAXIWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.

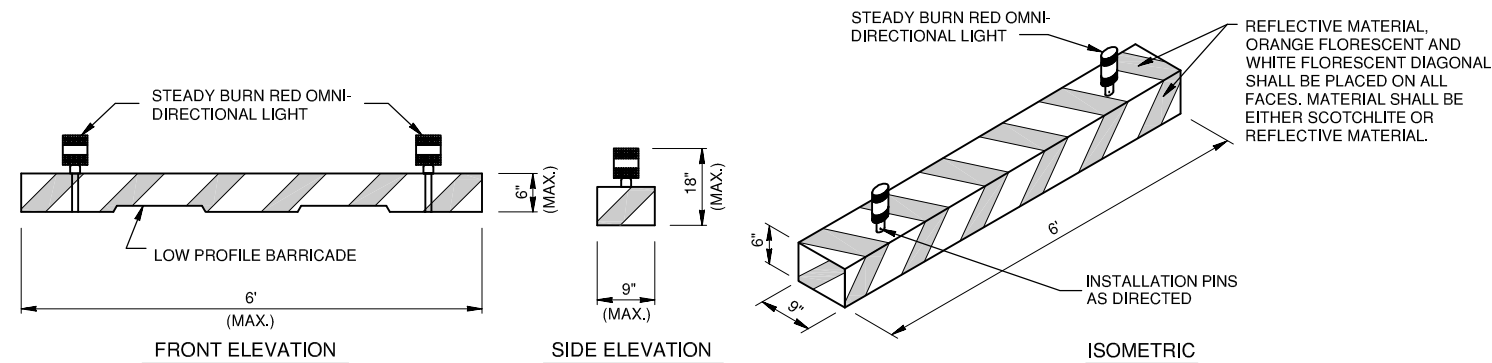
18. OTHER LIMITATIONS ON CONSTRUCTION

- IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
- BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.



CONSTRUCTION SETBACK LINE DETAIL

N.T.S.



BEAM BARRICADE DETAILS

N.T.S.

NOTES

- BARRICADE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY JET OR PROP BLAST.
- BARRICADE SHALL BE EASILY COLLAPSIBLE UPON CONTACT WITH AIRCRAFT.
- PLACE AT 10' INTERVALS.
- NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.

BLOOMINGTON-NORMAL AIRPORT AUTHORITY
CENTRAL ILLINOIS REGIONAL AIRPORT
BLOOMINGTON, ILLINOIS

EXPAND AIR CARGO APRON
CONSTRUCTION SAFETY PHASING
PLAN NOTES

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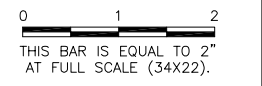
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DATE:	MARCH 19, 2013
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**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

**EXPAND AIR CARGO APRON
 EXISTING CONDITIONS AND REMOVALS**

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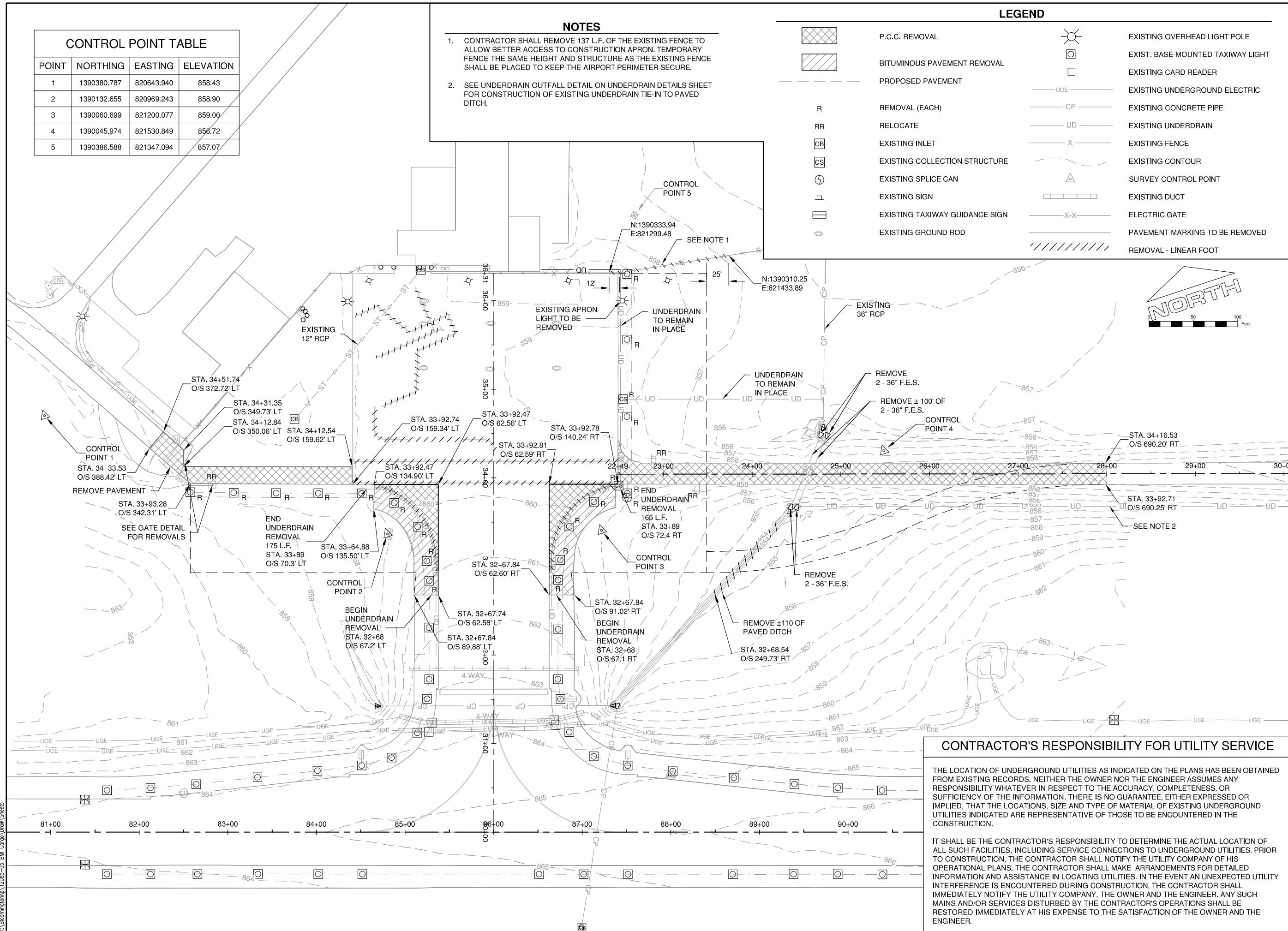
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SHEET	08 OF 55 SHEETS

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3	1390060.699	821200.077	859.00
4	1390045.974	821530.849	856.72
5	1390386.588	821347.094	857.07

- NOTES**
- CONTRACTOR SHALL REMOVE 137 L.F. OF THE EXISTING FENCE TO ALLOW BETTER ACCESS TO CONSTRUCTION APRON. TEMPORARY FENCE THE SAME HEIGHT AND STRUCTURE AS THE EXISTING FENCE SHALL BE PLACED TO KEEP THE AIRPORT PERIMETER SECURE.
 - SEE UNDERDRAIN OUTFALL DETAIL ON UNDERDRAIN DETAILS SHEET FOR CONSTRUCTION OF EXISTING UNDERDRAIN TIE-IN TO PAVED DITCH.

LEGEND

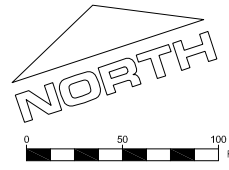
	P.C.C. REMOVAL		EXISTING OVERHEAD LIGHT POLE
	BITUMINOUS PAVEMENT REMOVAL		EXIST. BASE MOUNTED TAXIWAY LIGHT
	PROPOSED PAVEMENT		EXISTING CARD READER
R	REMOVAL (EACH)		EXISTING UNDERGROUND ELECTRIC
RR	RELOCATE		EXISTING CONCRETE PIPE
	EXISTING INLET		EXISTING UNDERDRAIN
	EXISTING COLLECTION STRUCTURE		EXISTING FENCE
	EXISTING SPLICE CAN		EXISTING CONTOUR
	EXISTING SIGN		SURVEY CONTROL POINT
	EXISTING TAXIWAY GUIDANCE SIGN		EXISTING DUCT
	EXISTING GROUND ROD		ELECTRIC GATE
			PAVEMENT MARKING TO BE REMOVED
			REMOVAL - LINEAR FOOT




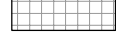


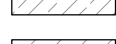
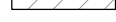
CONTRACTOR'S RESPONSIBILITY FOR UTILITY SERVICE

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.



EXISTING PAVEMENT STRUCTURE LEGEND

-  12" P.C.C. PAVEMENT (501)
4" BITUMINOUS BASE COURSE (403)
-  12" P.C.C. PAVEMENT (501)
4" CRUSHED AGGREGATE BASE COURSE (209)
-  6" P.C.C. PAVEMENT (501)
4" AGGREGATE BASE COURSE (209)
-  6" P.C.C. PAVEMENT (501)
12" LIME TREATED SUBGRADE (155)
-  4" BITUMINOUS PAVEMENT (403)
8" AGGREGATE BASE COURSE (208)
-  11" P.C.C. PAVEMENT (501)
4" CRUSHED AGGREGATE BASE COURSE (209)

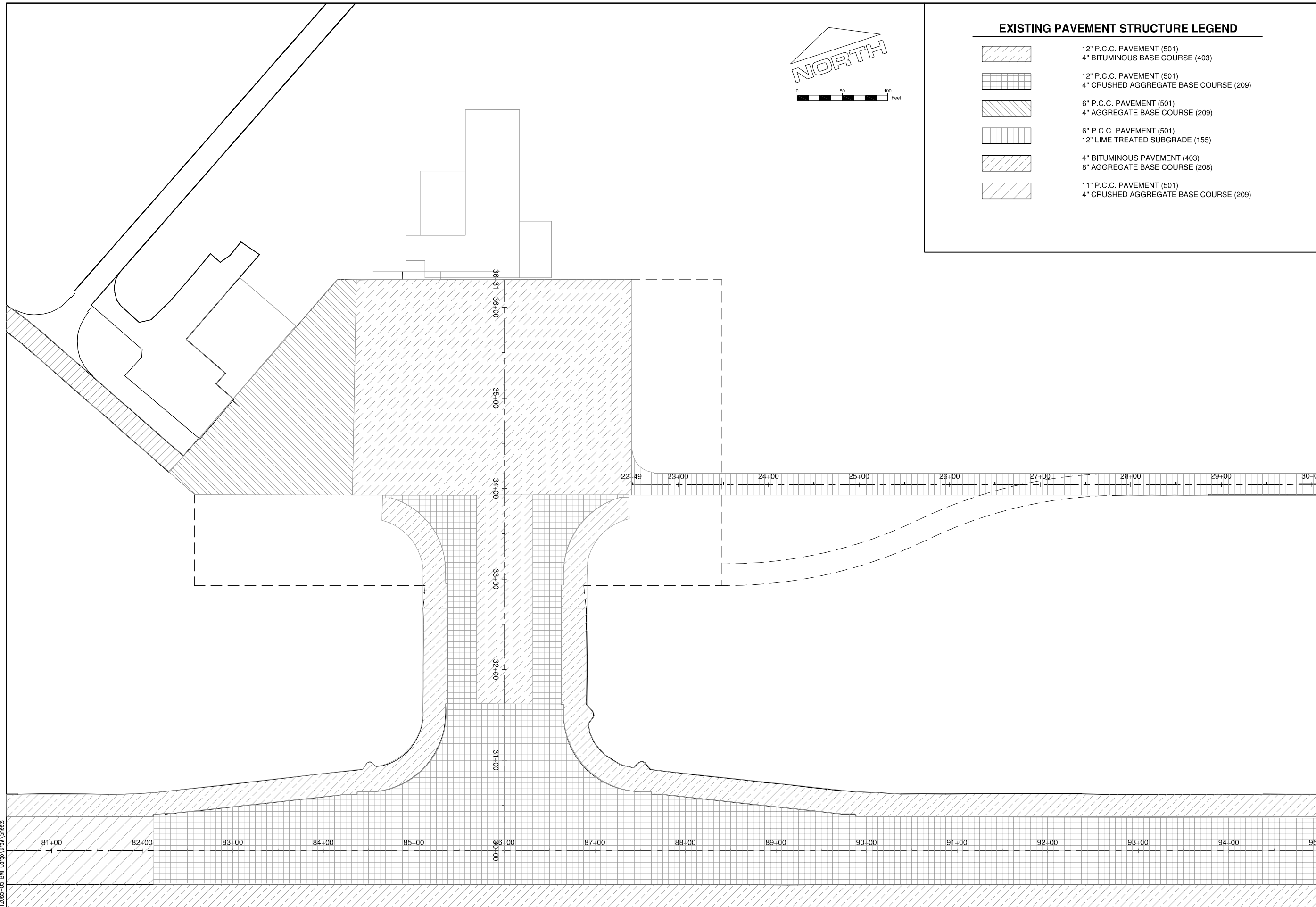
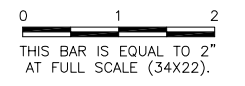
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 1208505-V-VF3D
 BMI-V-AERL
 BMI-V-SPXP

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BLOOMINGTON-NORMAL AIRPORT AUTHORITY
CENTRAL ILLINOIS REGIONAL AIRPORT
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EXPAND AIR CARGO APRON
EXISTING PAVEMENT STRUCTURE

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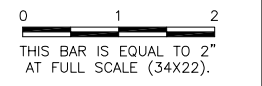
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NUMBER	BY	DATE



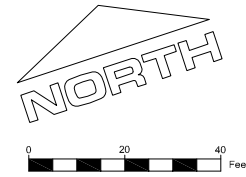
BLOOMINGTON-NORMAL AIRPORT AUTHORITY
CENTRAL ILLINOIS REGIONAL AIRPORT
BLOOMINGTON, ILLINOIS
EXPAND AIR CARGO APRON
EXISTING PAVEMENT REPAIR PLAN

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AIP PROJ. NO.	3-17-0006-XX
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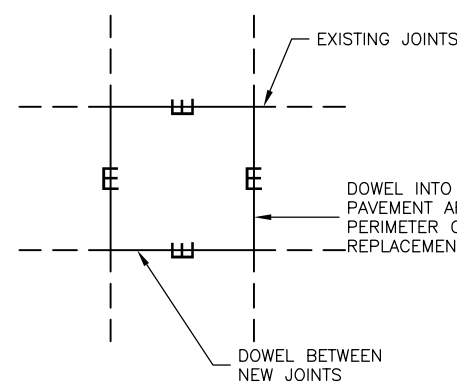
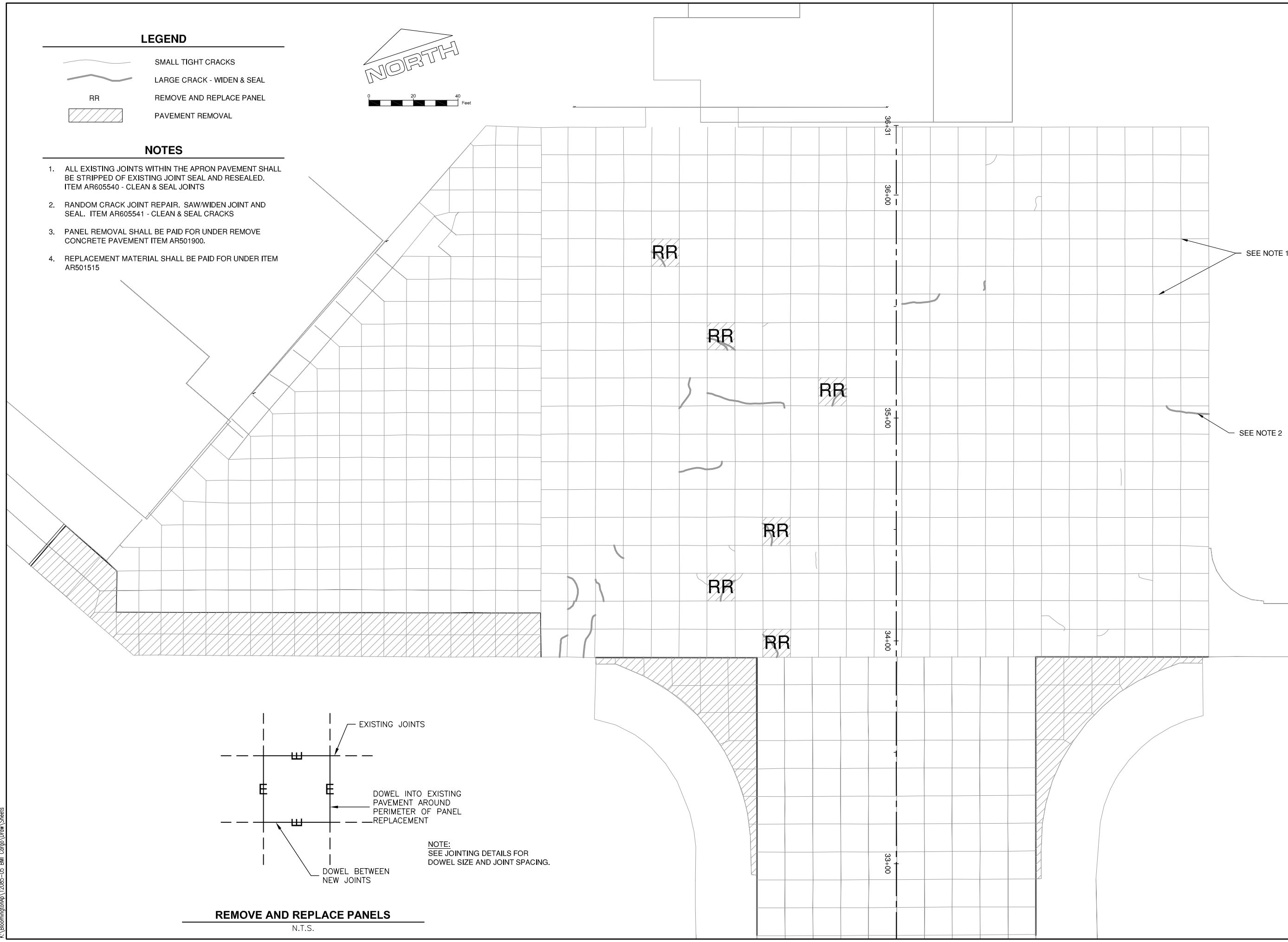
LEGEND

- SMALL TIGHT CRACKS
- LARGE CRACK - WIDEN & SEAL
- RR** REMOVE AND REPLACE PANEL
- PAVEMENT REMOVAL



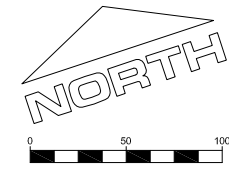
NOTES

1. ALL EXISTING JOINTS WITHIN THE APRON PAVEMENT SHALL BE STRIPPED OF EXISTING JOINT SEAL AND RESEALED. ITEM AR605540 - CLEAN & SEAL JOINTS
2. RANDOM CRACK JOINT REPAIR. SAW/WIDEN JOINT AND SEAL. ITEM AR605541 - CLEAN & SEAL CRACKS
3. PANEL REMOVAL SHALL BE PAID FOR UNDER REMOVE CONCRETE PAVEMENT ITEM AR501900.
4. REPLACEMENT MATERIAL SHALL BE PAID FOR UNDER ITEM AR501515



NOTE:
 SEE JOINTING DETAILS FOR
 DOWEL SIZE AND JOINT SPACING.

REMOVE AND REPLACE PANELS
 N.T.S.



LEGEND

	15" P.C.C. PAVEMENT (501) 6" CRUSHED AGGREGATE BASE COURSE (209)
	9" P.C.C. PAVEMENT (501) 6" CRUSHED AGGREGATE BASE COURSE (209)
	NEW 10' CHAIN LINK FENCE

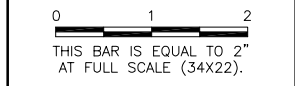
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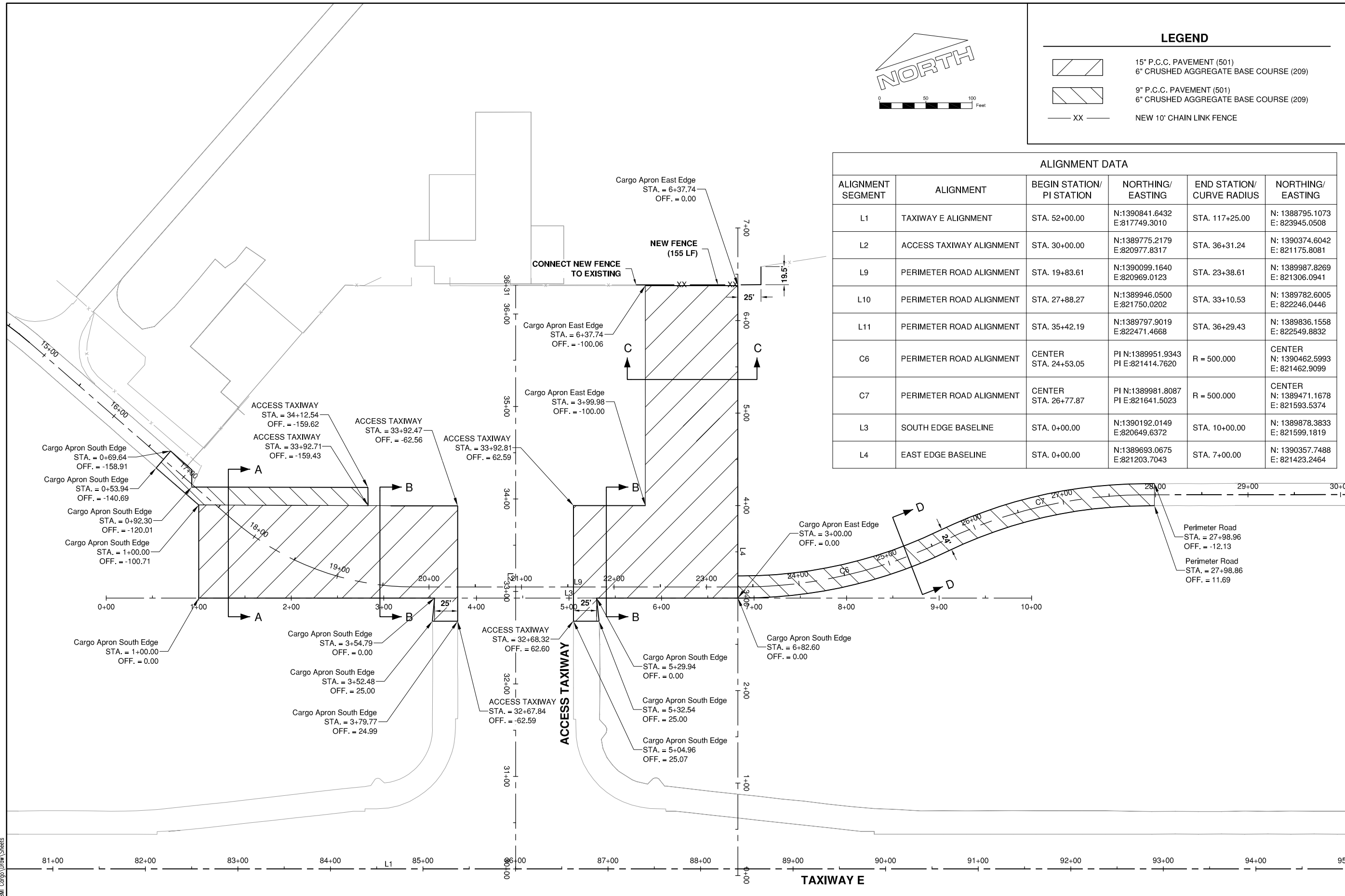
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ALIGNMENT DATA

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L2	ACCESS TAXIWAY ALIGNMENT	STA. 30+00.00	N:1389775.2179 E:820977.8317	STA. 36+31.24	N: 1390374.6042 E: 821175.8081
L9	PERIMETER ROAD ALIGNMENT	STA. 19+83.61	N:1390099.1640 E:820969.0123	STA. 23+38.61	N: 1389987.8269 E: 821306.0941
L10	PERIMETER ROAD ALIGNMENT	STA. 27+88.27	N:1389946.0500 E:821750.0202	STA. 33+10.53	N: 1389782.6005 E: 822246.0446
L11	PERIMETER ROAD ALIGNMENT	STA. 35+42.19	N:1389797.9019 E:822471.4668	STA. 36+29.43	N: 1389836.1558 E: 822549.8832
C6	PERIMETER ROAD ALIGNMENT	CENTER STA. 24+53.05	PI N:1389951.9343 PI E:821414.7620	R = 500.000	CENTER N: 1390462.5993 E: 821462.9099
C7	PERIMETER ROAD ALIGNMENT	CENTER STA. 26+77.87	PI N:1389981.8087 PI E:821641.5023	R = 500.000	CENTER N: 1389471.1678 E: 821593.5374
L3	SOUTH EDGE BASELINE	STA. 0+00.00	N:1390192.0149 E:820649.6372	STA. 10+00.00	N: 1389878.3833 E: 821599.1819
L4	EAST EDGE BASELINE	STA. 0+00.00	N:1389693.0675 E:821203.7043	STA. 7+00.00	N: 1390357.7488 E: 821423.2464



BLOOMINGTON-NORMAL AIRPORT AUTHORITY
CENTRAL ILLINOIS REGIONAL AIRPORT
BLOOMINGTON, ILLINOIS

EXPAND AIR CARGO APRON
PROPOSED IMPROVEMENTS

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CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MARCH 19, 2013
JOB No:	12085-05-00

IL PROJ. NO. BMI-4288
 AIP PROJ. NO. 3-17-0006-XX
 SHEET 11 OF 55 SHEETS

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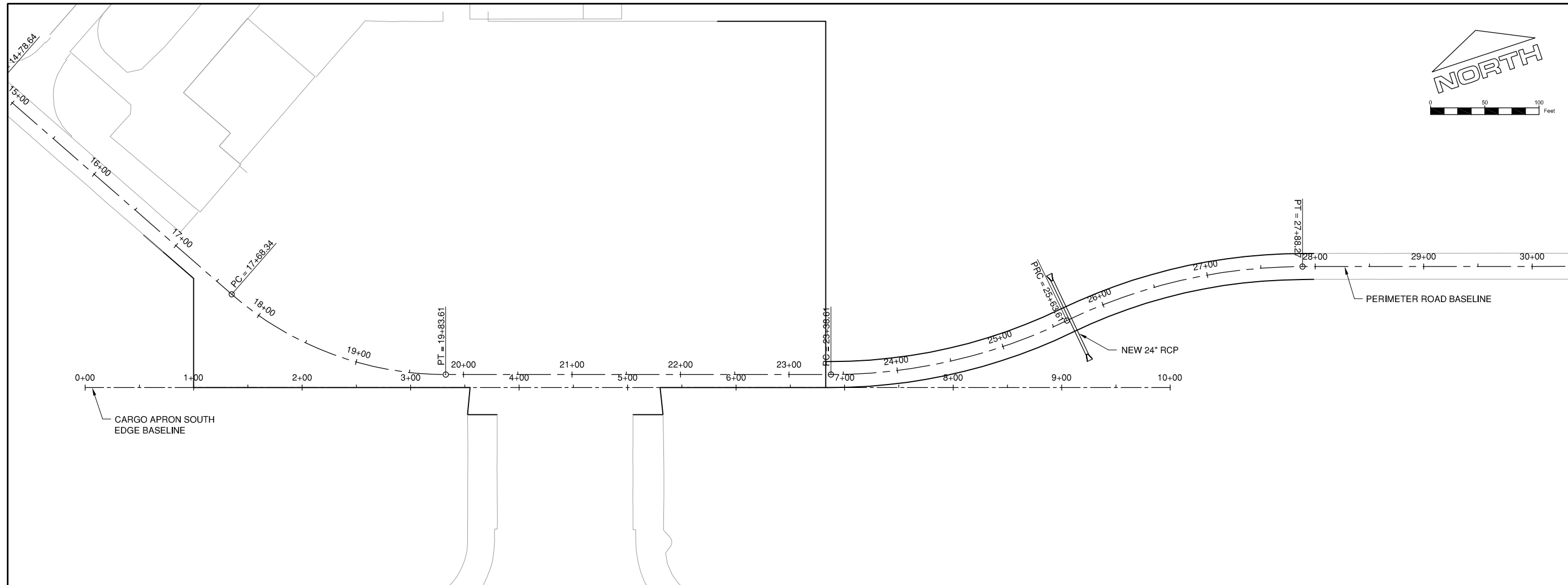


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 PLOT DATE: 4/4/2013 12:55 PM
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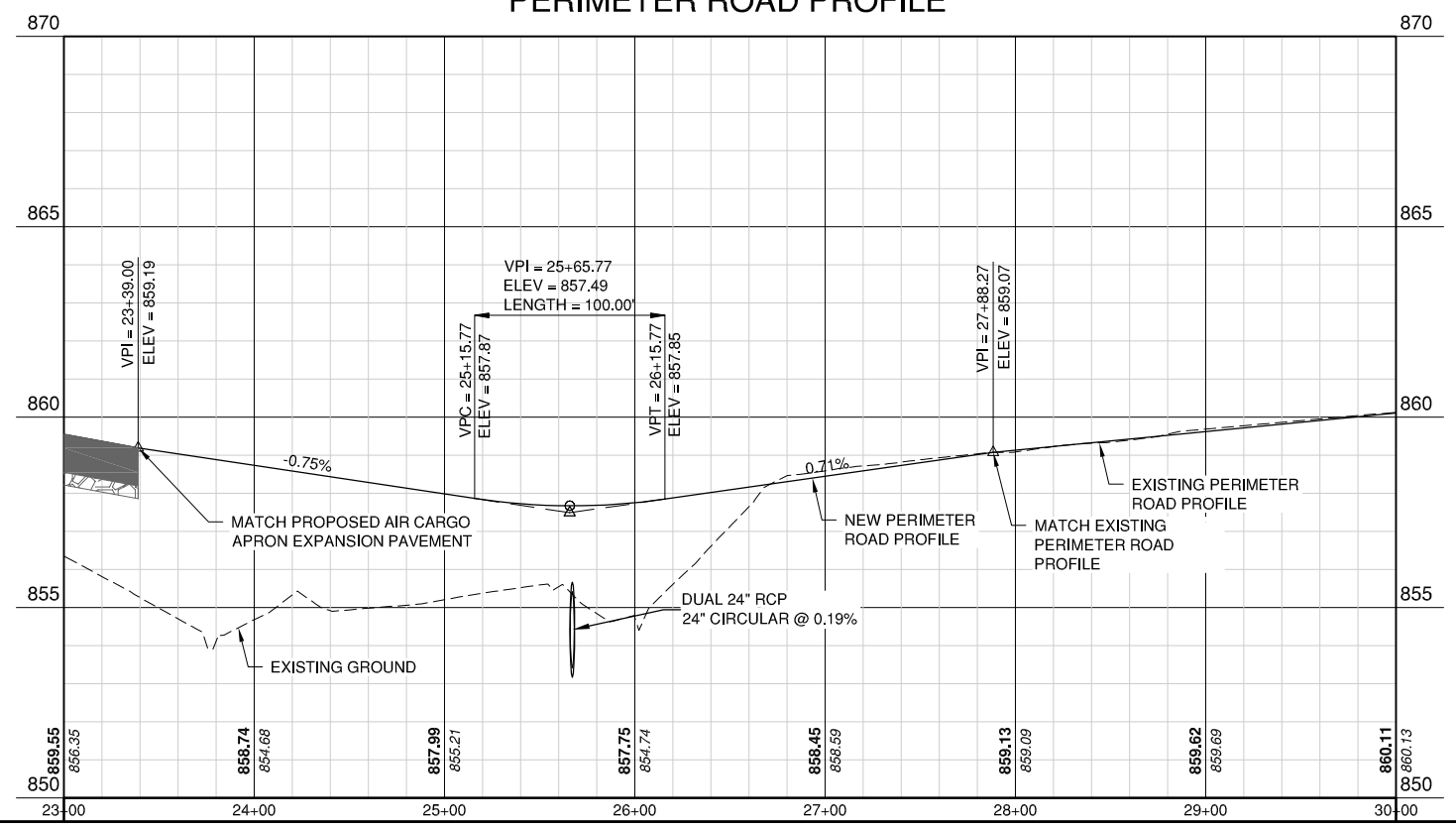
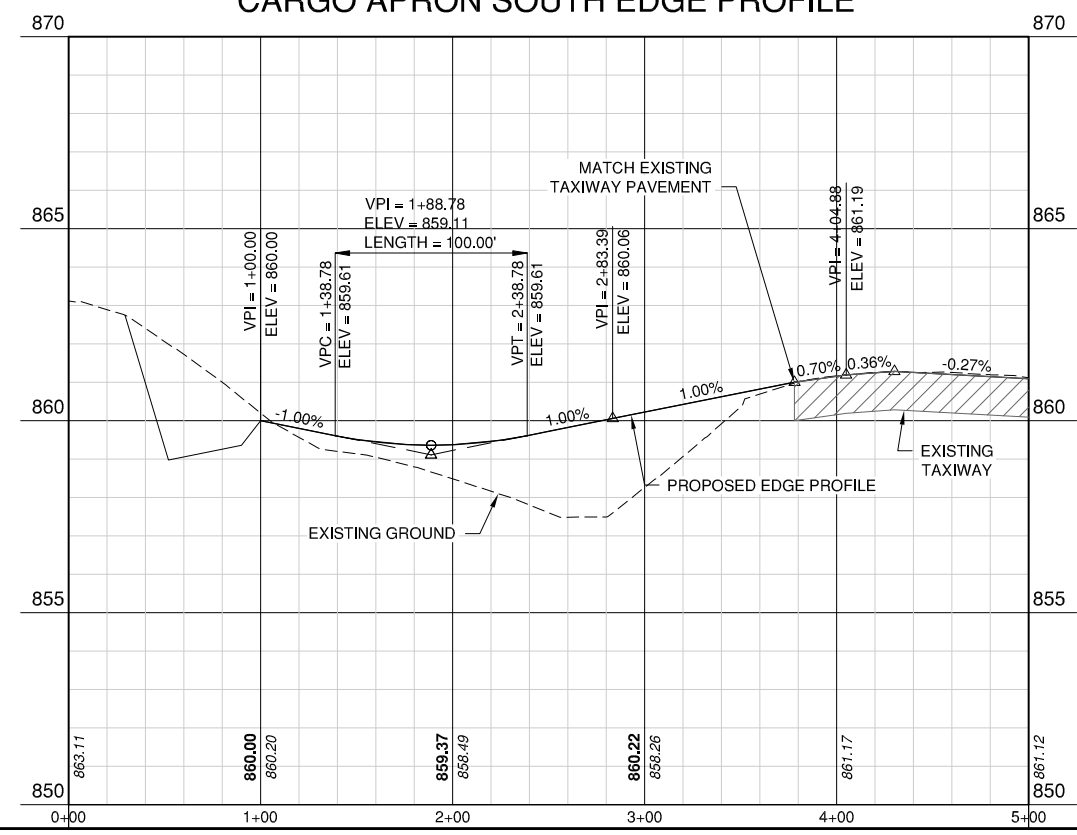
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0 1 2
 THIS BAR IS EQUAL TO 2"
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CARGO APRON SOUTH EDGE PROFILE

PERIMETER ROAD PROFILE



**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

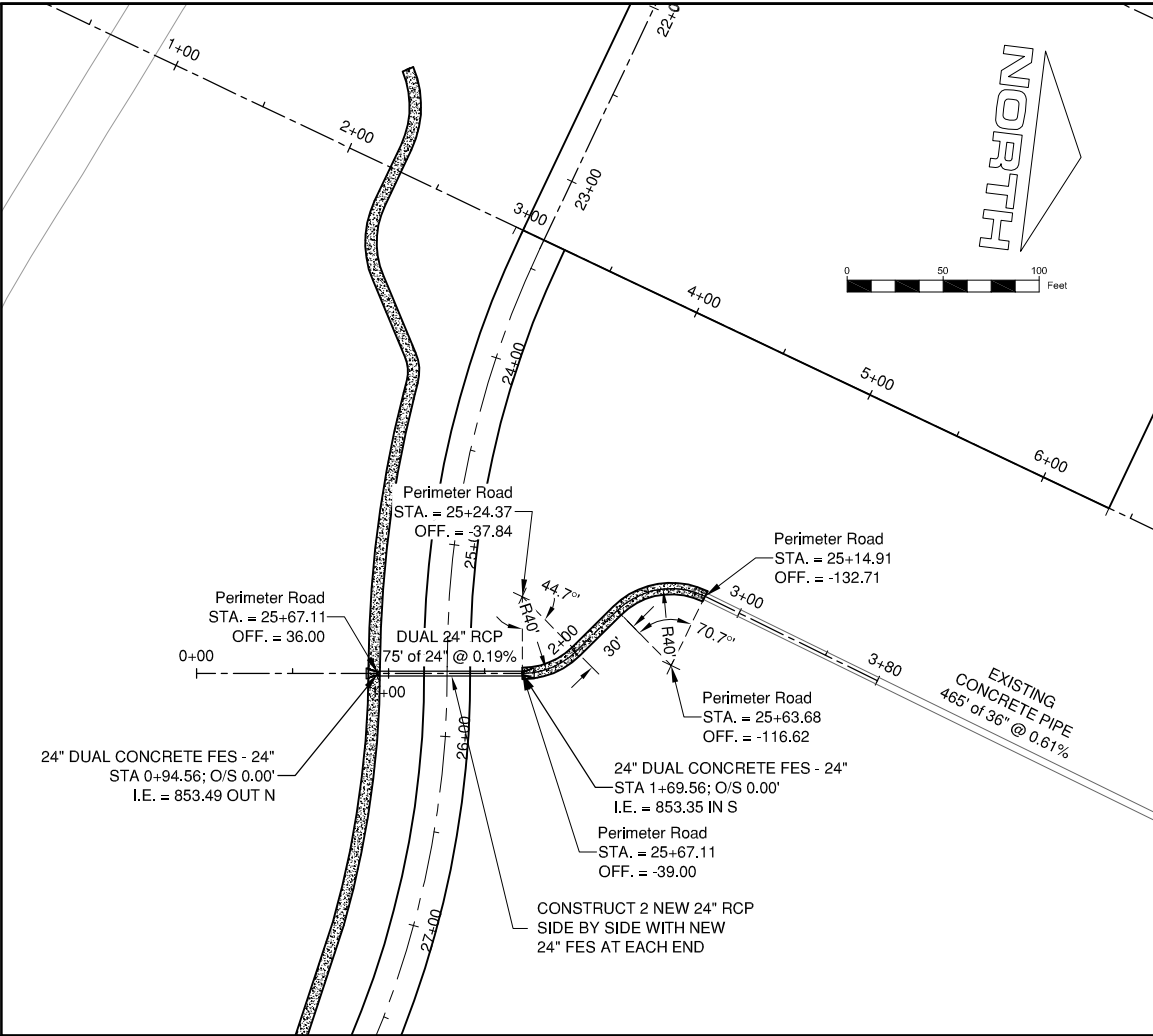
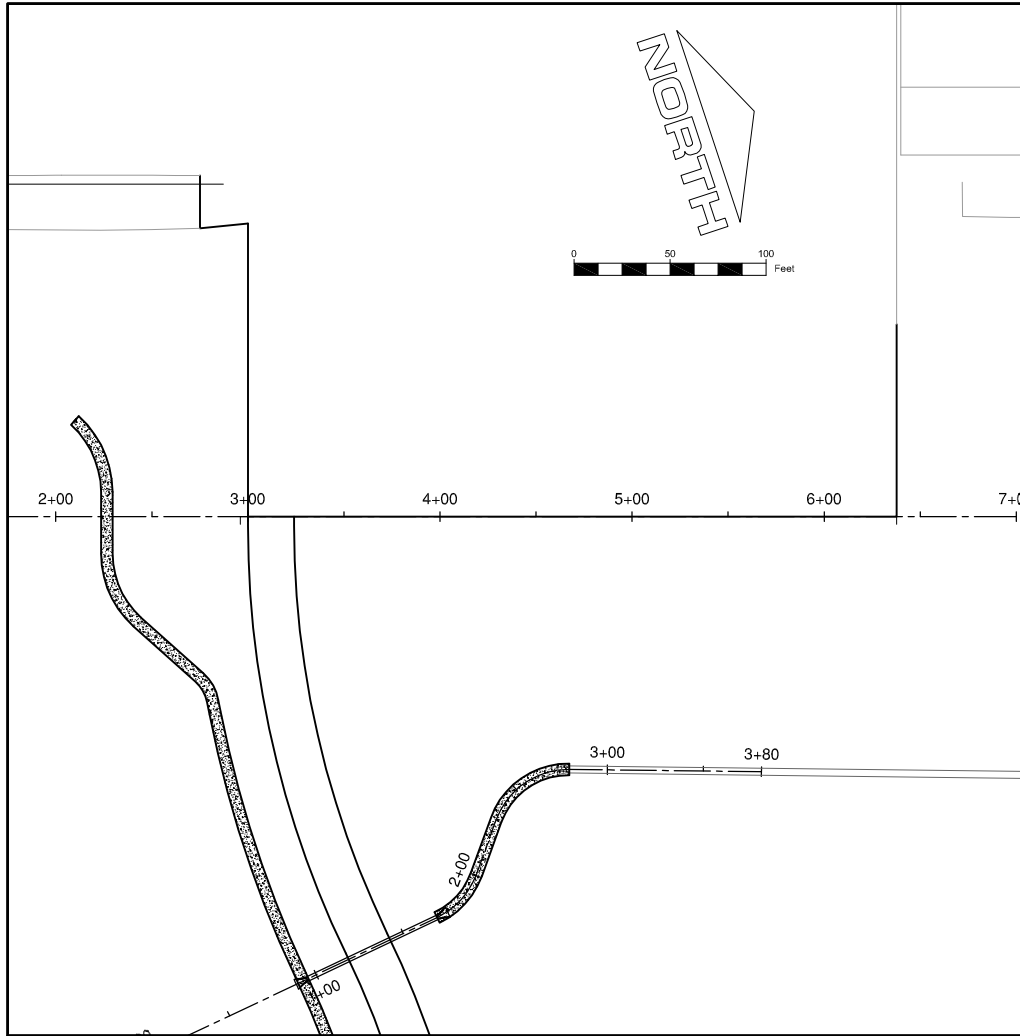
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 PERIMETER ROAD PLAN AND PROFILE**

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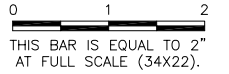


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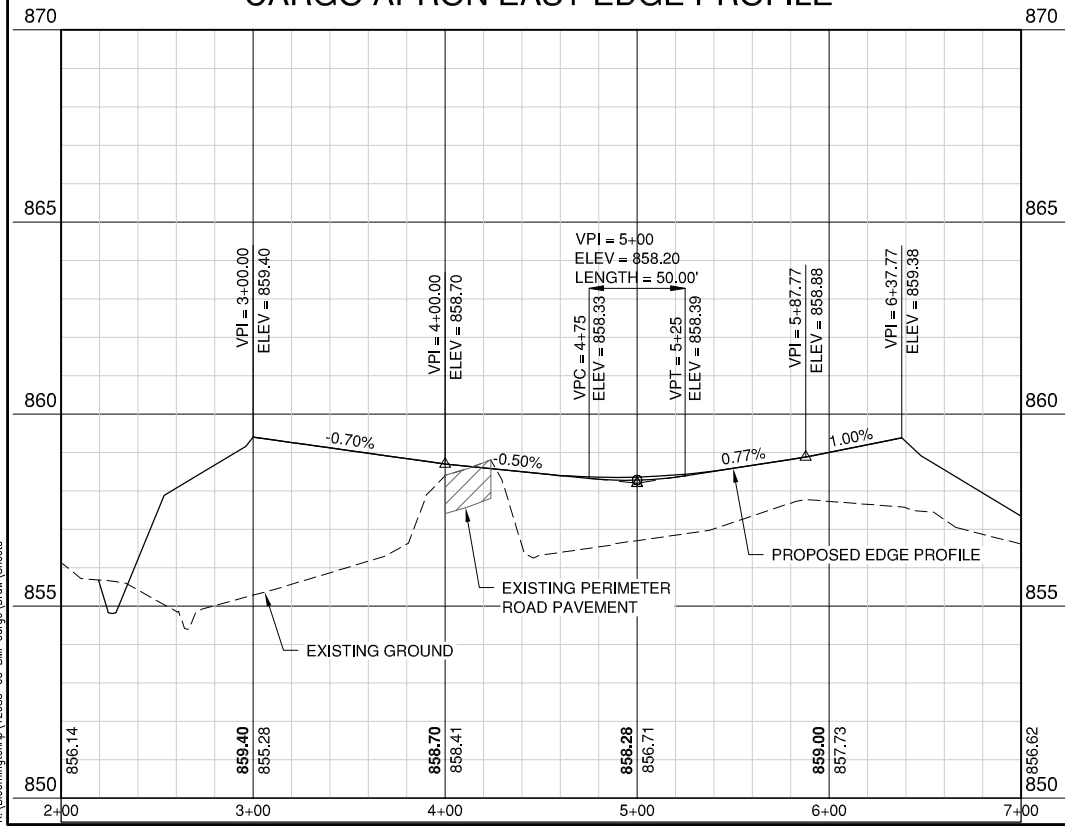
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 BMI-V-SPYP
 1208505-C-UP

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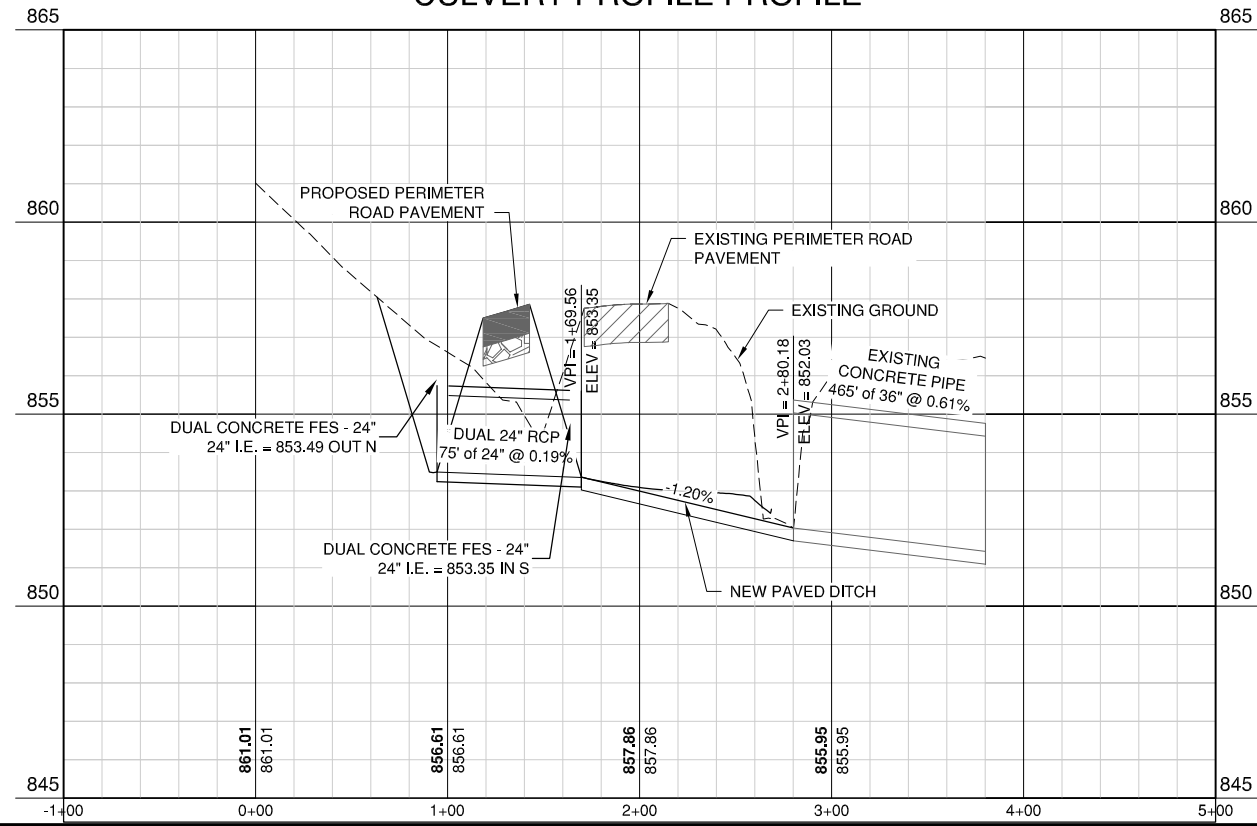
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CARGO APRON EAST EDGE PROFILE



CULVERT PROFILE PROFILE



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BLOOMINGTON, ILLINOIS
EXPAND AIR CARGO APRON
EAST EDGE AND PERIMETER ROAD
CULVERT PLAN AND PROFILE

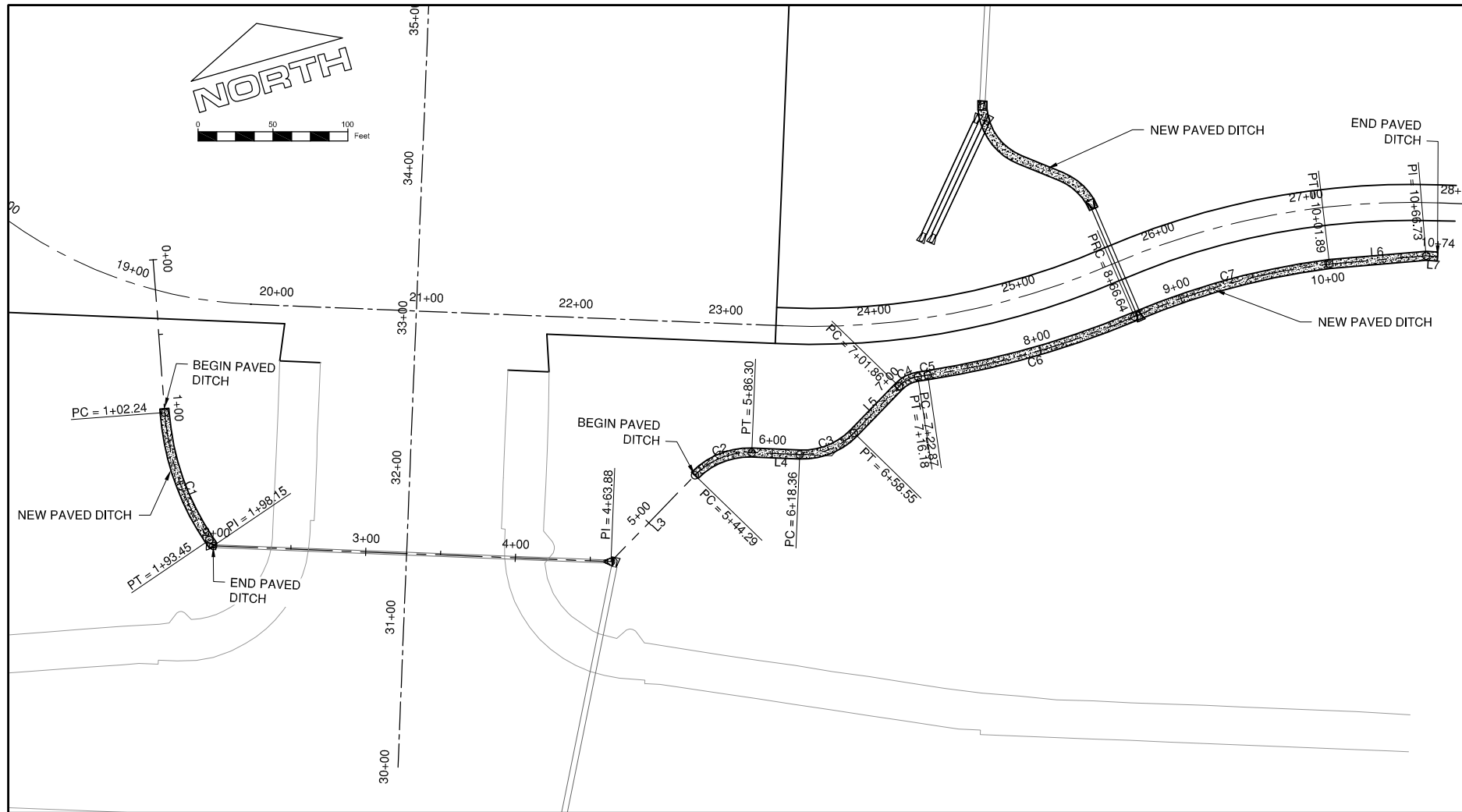
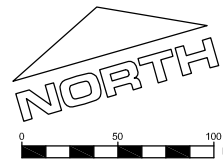
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SHEET 13 OF 55 SHEETS	

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ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
C1	PAVED DITCH	CENTER STA. 1+48.90	PI N:1390000.0044 PI E:820883.8627	R = 175.000	CENTER N: 1390010.3342 E: 821064.6827
L3	PAVED DITCH	STA. 4+63.88	N:1389868.0435 E:821152.2905	STA. 5+44.29	N: 1389908.0883 E: 821222.0136
C2	PAVED DITCH	CENTER STA. 5+66.62	PI N:1389919.2142 PI E:821241.3851	R = 50.000	CENTER N: 1389864.7307 E: 821246.9156
L4	PAVED DITCH	STA. 5+86.30	N:1389912.2079 E:821262.5973	STA. 6+18.36	N: 1389902.1526 E: 821293.0402
C3	PAVED DITCH	CENTER STA. 6+39.73	PI N:1389895.4516 PI E:821313.3280	R = 47.820	CENTER N: 1389947.5597 E: 821308.0381
L5	PAVED DITCH	STA. 6+58.55	N:1389906.0931 E:821331.8553	STA. 7+01.86	N: 1389927.6661 E: 821369.4146
C4	PAVED DITCH	CENTER STA. 7+09.28	PI N:1389931.3602 PI E:821375.8462	R = 22.100	CENTER N: 1389908.5018 E: 821380.4220
C5	PAVED DITCH	CENTER STA. 7+95.18	PI N:1389919.8172 PI E:821461.4923	R = 540.999	CENTER N: 1390465.6249 E: 821462.9178
C6	PAVED DITCH	CENTER STA. 7+95.18	PI N:1389919.8172 PI E:821461.4923	R = 540.999	CENTER N: 1390465.6249 E: 821462.9178
C7	PAVED DITCH	CENTER STA. 9+34.75	PI N:1389938.1050 PI E:821600.7185	R = 462.108	CENTER N: 1389471.0587 E: 821593.5394
L6	PAVED DITCH	STA. 10+01.89	N:1389927.1378 E:821667.9442	STA. 10+66.73	N: 1389914.6250 E: 821731.5693
L7	PAVED DITCH	STA. 10+66.73	N:1389914.6250 E:821731.5693	STA. 10+74.42	N: 1389912.0642 E: 821738.8133

FILE: 1208505-CP201.dwg
 UPDATE BY: Chris Groth
 PLOT DATE: 4/4/2013 12:57 PM

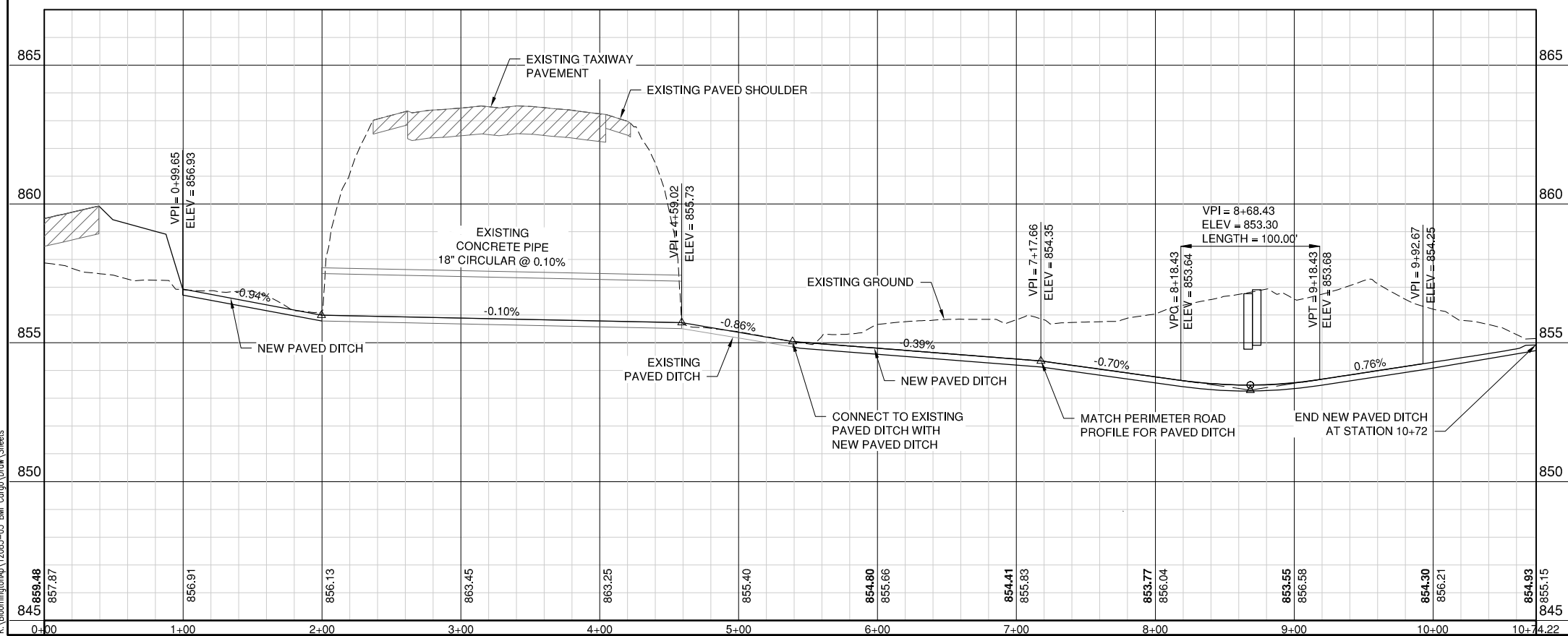
1208505-C-SPAL
 BMI-V-SPXP
 BMI-V-AERL
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0 1 2
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SOUTH PAVED DITCH PROFILE



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EXPAND AIR CARGO APRON
SOUTH PAVED DITCH PLAN AND
PROFILE



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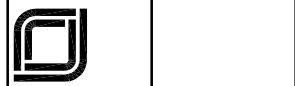
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NUMBER	BY	DATE

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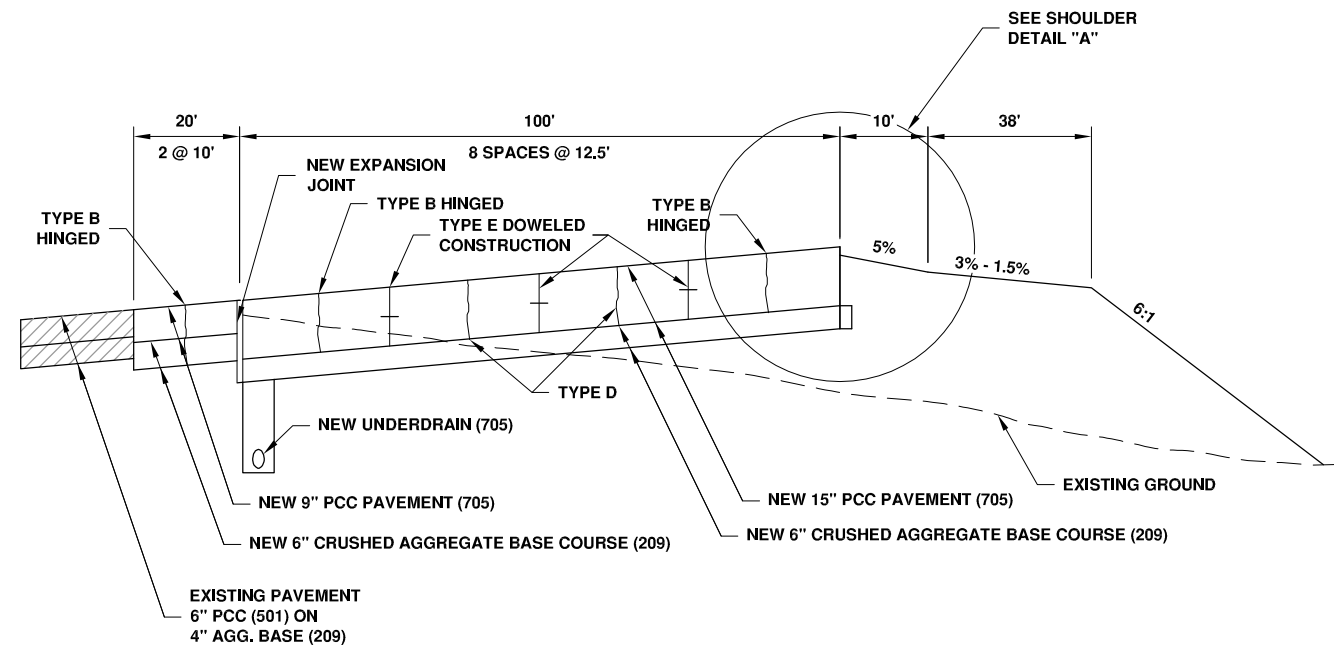
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**EXPAND AIR CARGO APRON
 TYPICAL SECTIONS 1**

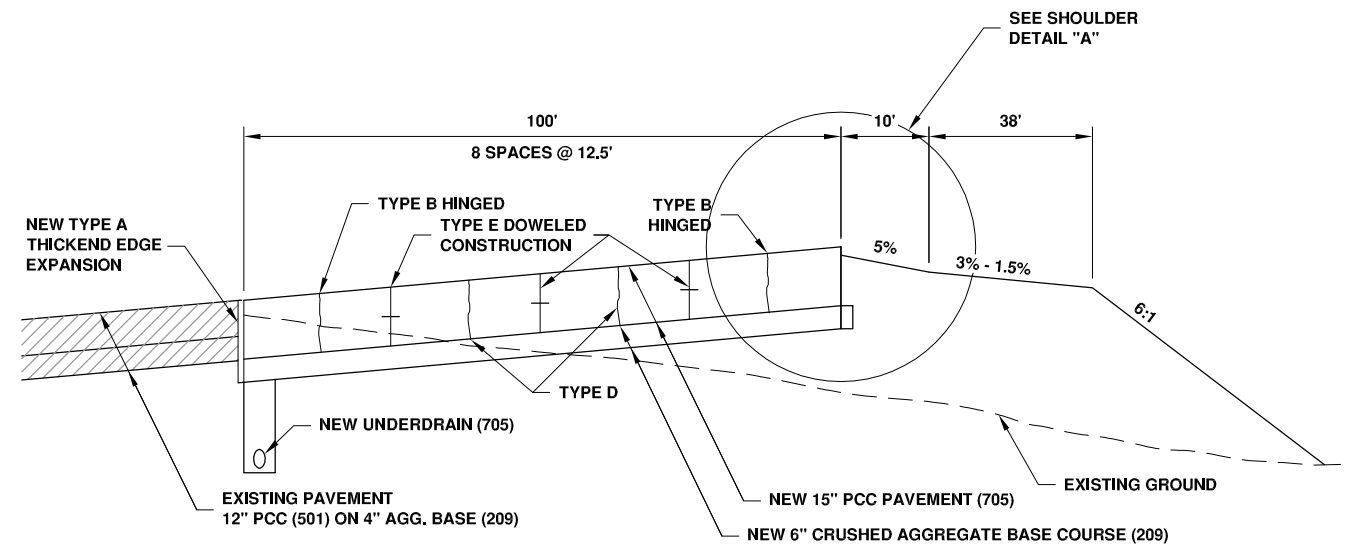
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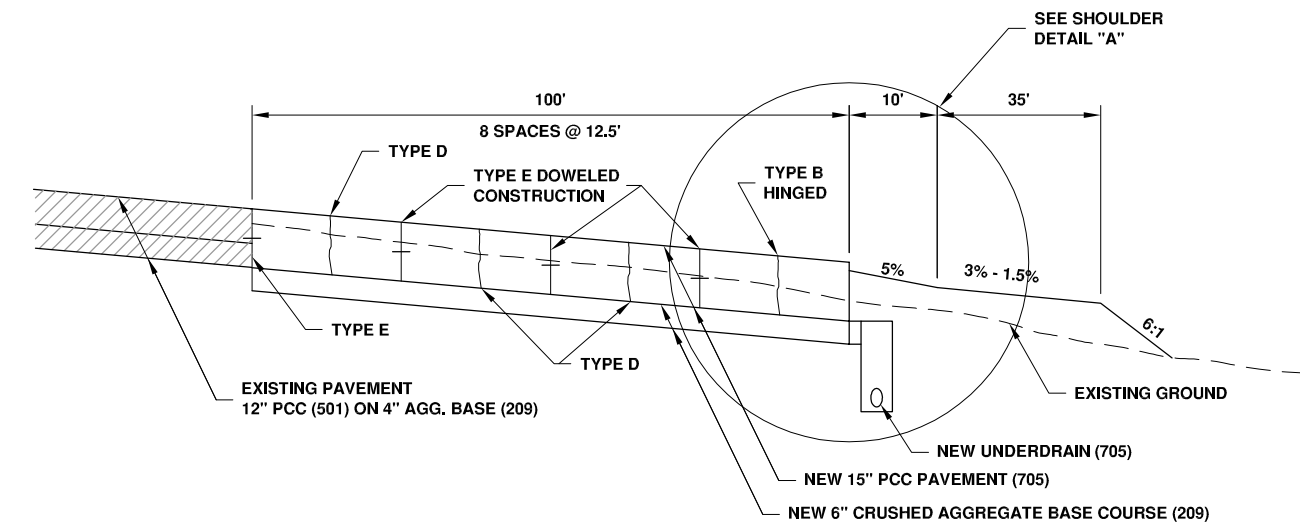
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CHECKED BY:	RLV
APPROVED BY:	RLV
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IL PROJ. NO.	BMI-4288
AIP PROJ. NO.	3-17-0006-XX
SHEET	15 OF 55 SHEETS



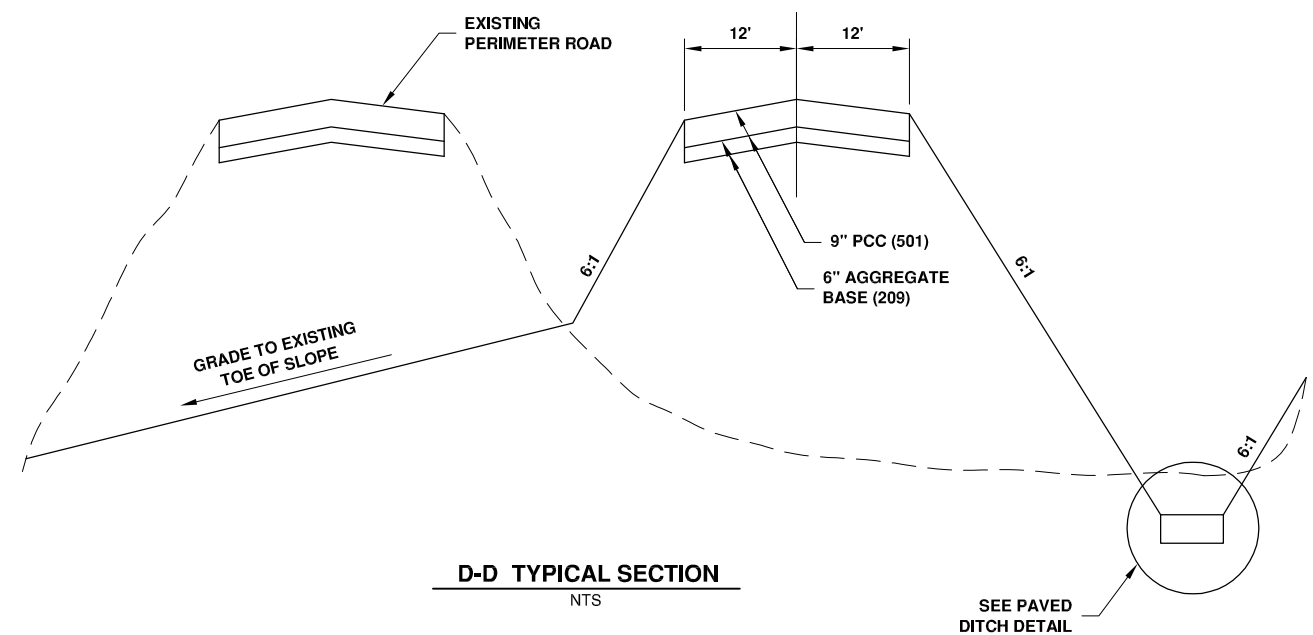
A-A TYPICAL SECTION
 NTS



B-B TYPICAL SECTION
 NTS



C-C TYPICAL SECTION
 NTS



D-D TYPICAL SECTION
 NTS

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NUMBER	BY	DATE

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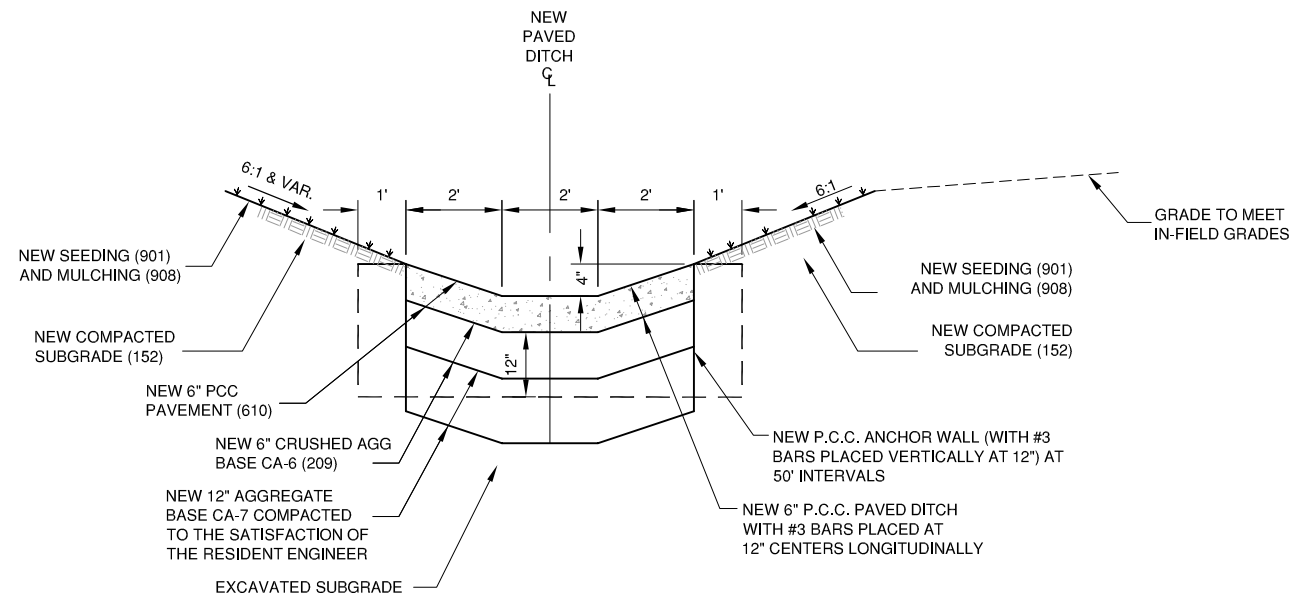
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**EXPAND AIR CARGO APRON
 TYPICAL SECTIONS 2**

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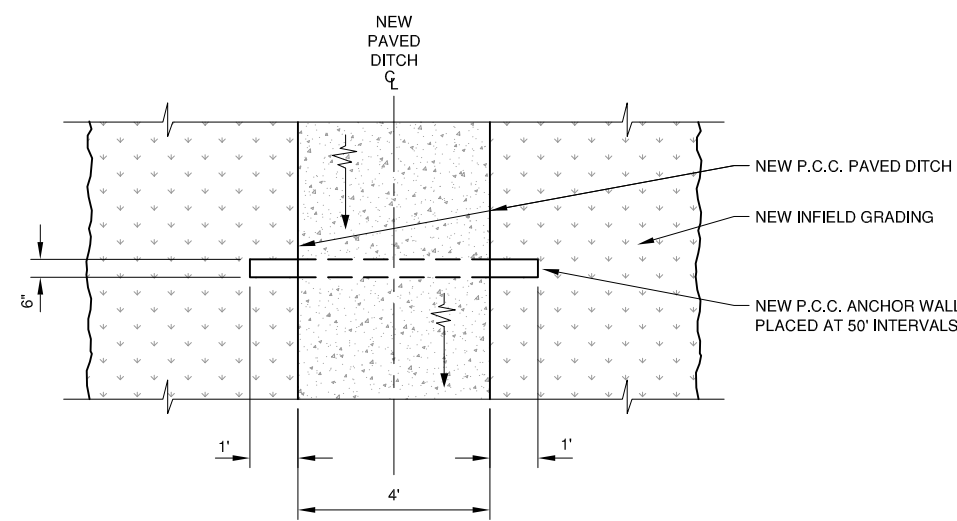


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SHEET	16 OF 55 SHEETS



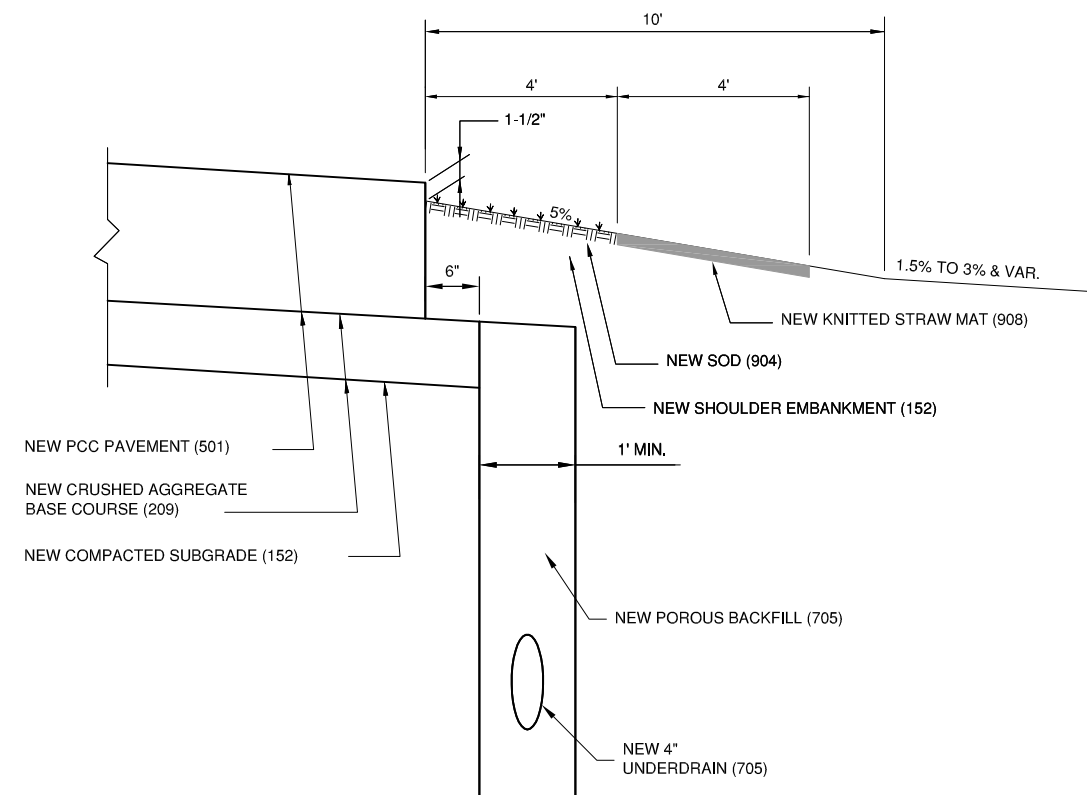
PAVED DITCH AND GRASS WATERWAY TYPICAL SECTION

N.T.S.



PAVED DITCH AND GRASS WATERWAY PLAN VIEW

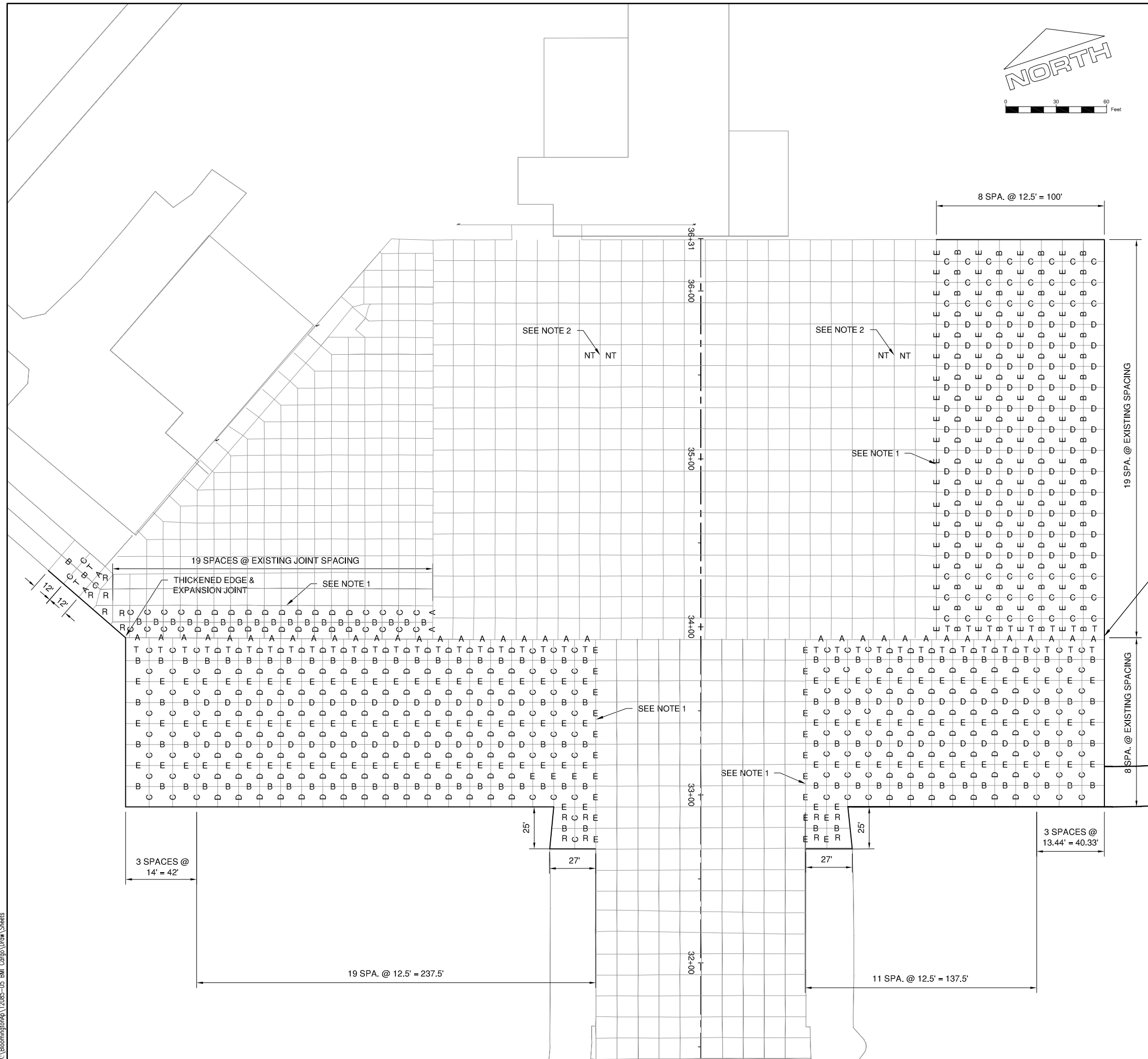
N.T.S.



NEW SHOULDER DETAIL "A"

N.T.S.

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LEGEND

- A — TYPE A ISOLATION JOINT (3/4")
- B — TYPE B HINGED (TIED) CONTRACTION JOINT
- C — TYPE C DOWELED CONTRACTION JOINT
- D — TYPE D DUMMY CONTRACTION JOINT
- E — TYPE E DOWELED CONSTRUCTION JOINT
- R REINFORCED PANEL
- T THICKENED EDGE
- NT NOSE TETHER

NOTES

1. MATCH EXISTING JOINT SPACING.
2. SEE PAVING DETAILS 2 FOR NOSE TETHER DETAIL.

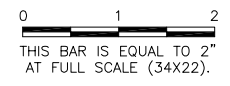
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 PLOT DATE: 4/4/2013 12:58 PM

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 BMI-V-AERL
 BMI-V-SPXP

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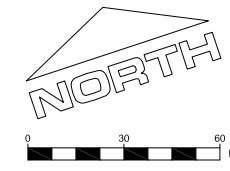


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EXPAND AIR CARGO APRON
JOINTING PLAN 1

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IL PROJ. NO.	BMI-4288
AIP PROJ. NO.	3-17-0006-XX
SHEET	17 OF 55 SHEETS



LEGEND

- A — TYPE A ISOLATION JOINT (3/4")
- B — TYPE B HINGED (TIED) CONTRACTION JOINT
- C — TYPE C DOWELED CONTRACTION JOINT
- D — TYPE D DUMMY CONTRACTION JOINT
- E — TYPE E DOWELED CONSTRUCTION JOINT
- R REINFORCED PANEL
- T THICKENED EDGE

NOTES

1. REMOVE TO CLOSEST EXISTING PANEL.

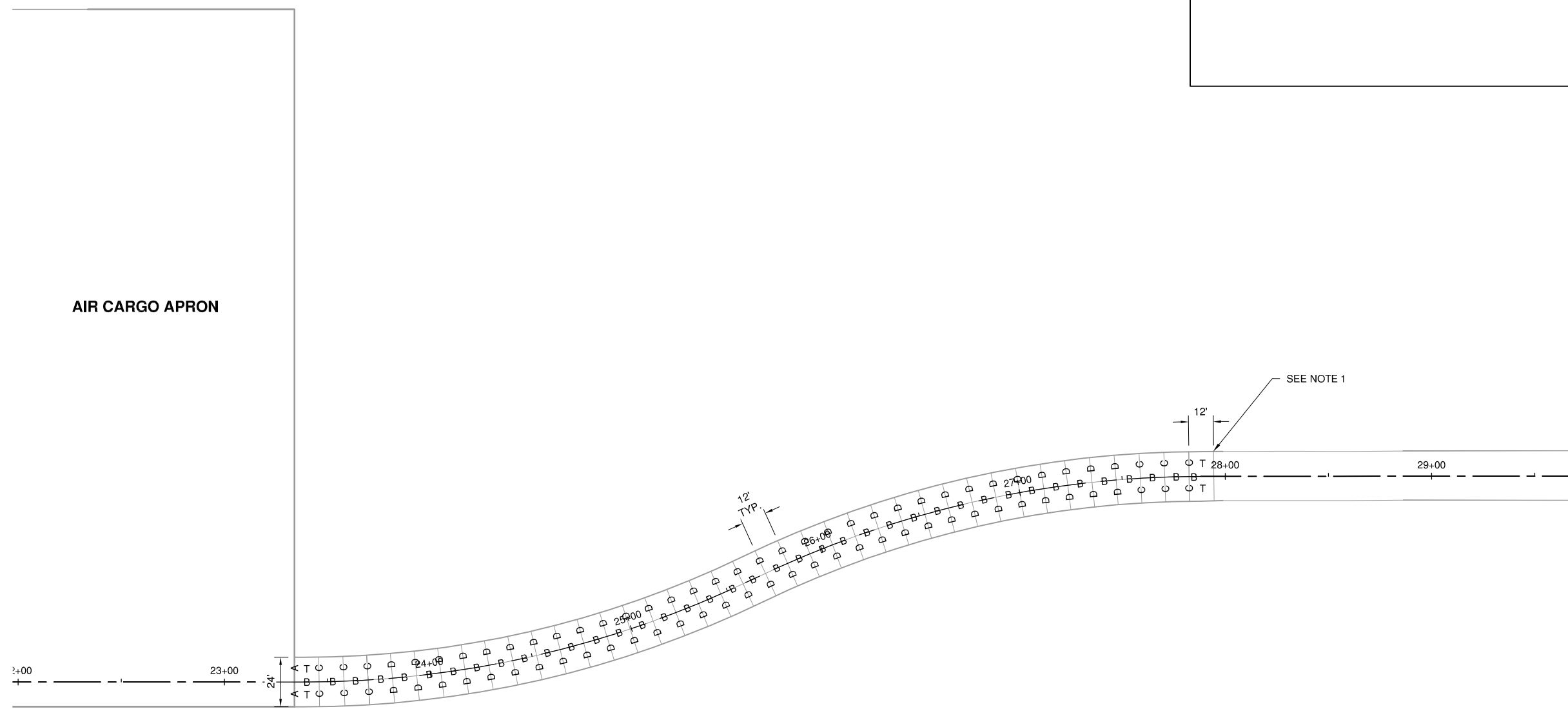
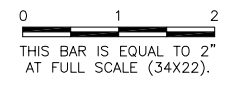
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 PLOT DATE: 4/4/2013 12:59 PM

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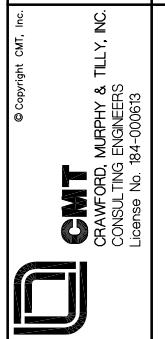
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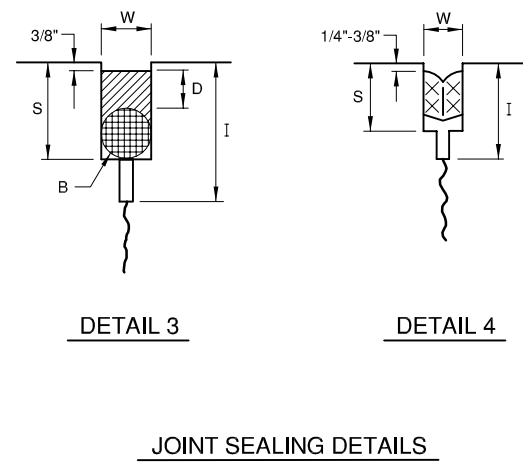
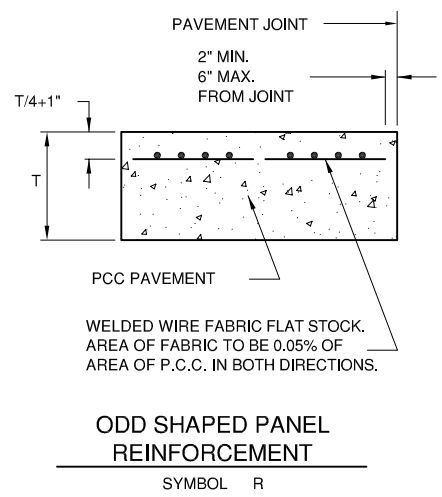
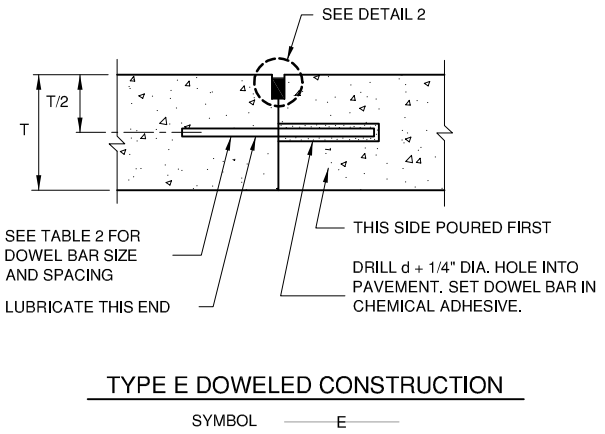
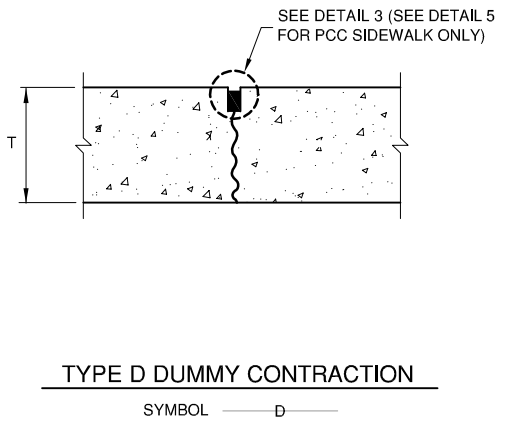
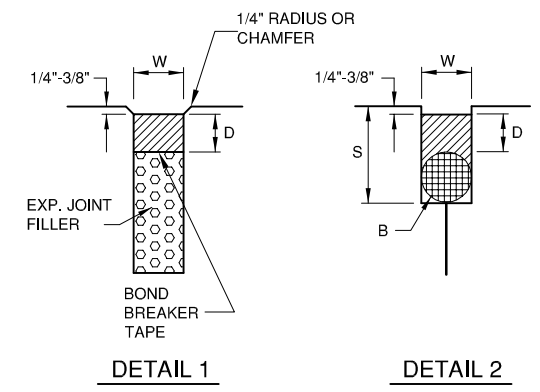
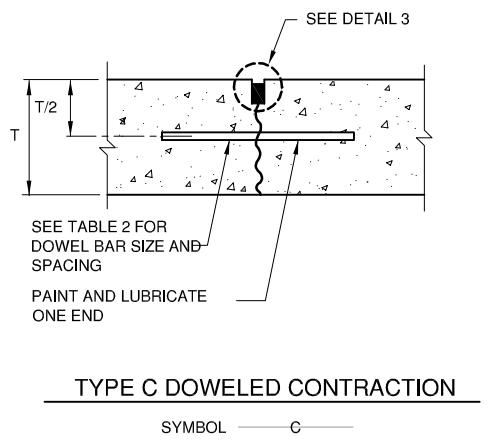
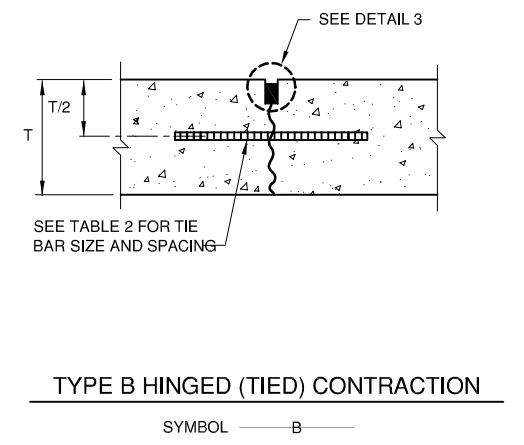
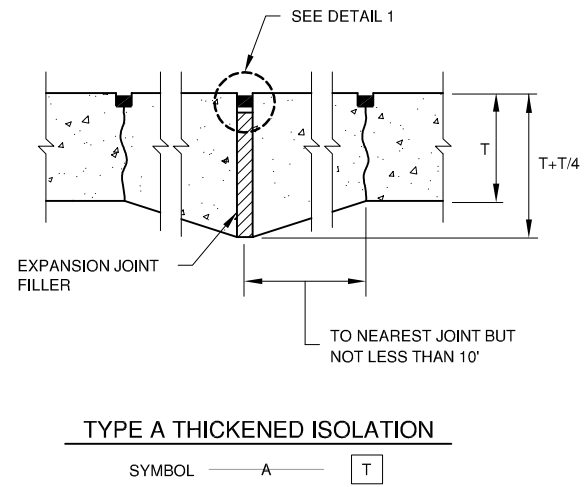
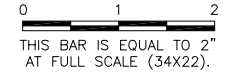
EXPAND AIR CARGO APRON
JOINTING PLAN 2



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APPROVED BY:	RLV
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REVISIONS		
NUMBER	BY	DATE



PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT I, INCHES $I = (T/3) \pm 1/4"$
5	1.67"
6	2.00"
7	2.33"
8	2.67"
9	3.00"
10	3.33"
11	3.67"
12	4.00"
13	4.33"
14	4.67"
15	5.00"
16	5.33"
17	5.67"
18	6.00"

PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA.	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
5	5/8"	12"	12"	#4	24"	30"
6	3/4"	18"	12"	#5	30"	30"
7	3/4"	18"	12"	#5	30"	30"
8	1"	19"	12"	#5	30"	30"
9	1"	19"	12"	#5	30"	30"
10	1"	19"	12"	#5	30"	30"
11	1"	19"	12"	#5	30"	30"
12	1"	19"	12"	#5	30"	30"
13	1 - 1/4"	20"	15"	#5	30"	30"
14	1 - 1/4"	20"	15"	#5	30"	30"
15	1 - 1/4"	20"	15"	#5	30"	30"
16	1 - 1/4"	20"	15"	#5	30"	30"
17	1 - 1/2"	20"	18"	#5	30"	30"
18	1 - 1/2"	20"	18"	#5	30"	30"

	DETAIL 1	DETAIL 2	DETAIL 3	DETAIL 4	DETAIL 5
	HOT/ COLD POUR	HOT/ COLD POUR	SILICONE	PRE FORMED	HOT/ COLD POUR
W=WIDTH OF SEALANT RESERVOIR (IN.)	3/4	1/2	1/2	3/8" COMPRESSED	3/16"
D=DEPTH OF SEALANT RESERVOIR (IN.)	3/4	1/2	1/4	N/A	I
B=BACKER ROD DIAMETER (IN.)	N/A	5/8	5/8	N/A	N/A
S=SECOND SAWCUT DEPTH (IN.) MINIMUM	N/A	1-3/8	1-3/8	1-1/2	N/A

- JOINT NOTES**
- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
 - THE INITIAL SAWCUT FOR ALL LONGITUDINAL & TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
 - ALL TIE BARS & MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING & AFTER CONCRETE PLACEMENT.
 - TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH THE SPECIFICATIONS.
 - THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAWCUT WILL NOT BE ALLOWED.

BLOOMINGTON-NORMAL AIRPORT AUTHORITY
CENTRAL ILLINOIS REGIONAL AIRPORT
BLOOMINGTON, ILLINOIS
EXPAND AIR CARGO APRON
JOINTING DETAILS

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 IL PROJ. NO. BMI-4288
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 SHEET 19 OF 55 SHEETS

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LEGEND

- PROPOSED ELEVATION
- EXISTING ELEVATION
- PROPOSED CONTOURS
- EXISTING CONTOURS

NOTES

1. SPOT ELEVATIONS ARE SHOWN AT JOINT INTERSECTIONS.

FILE: 1208505-CG101.dwg
 UPDATE BY: Chris Groth
 PLOT DATE: 4/4/2013 1:00 PM

1208505-C-SPAL
 1208505-V-VF3D
 BMI-V-AERL
 BMI-V-SPXP
 1208505-CP102
 1208505-CP103

BL069

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

**EXPAND AIR CARGO APRON
 STAKING PLAN**

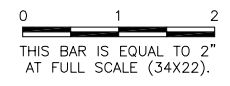
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APPROVED BY:	RLV
DATE:	MARCH 19, 2013
JOB No:	12085-05-00
IL PROJ. NO.	BMI-4288
AIP PROJ. NO.	3-17-0006-XX
SHEET	20 OF 55 SHEETS

BL069

REVISIONS		
NUMBER	BY	DATE



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 CENTRAL ILLINOIS REGIONAL AIRPORT
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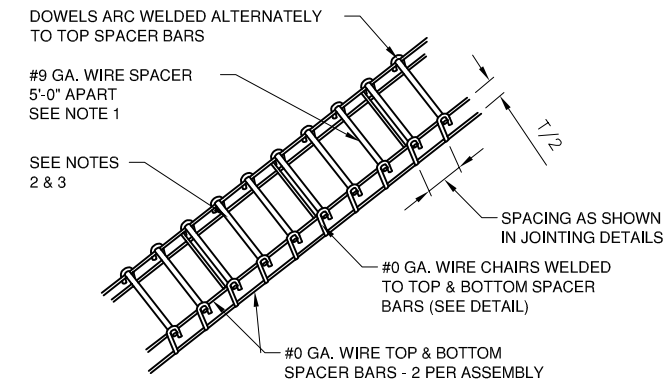
**EXPAND AIR CARGO APRON
 PAVING DETAILS 1**

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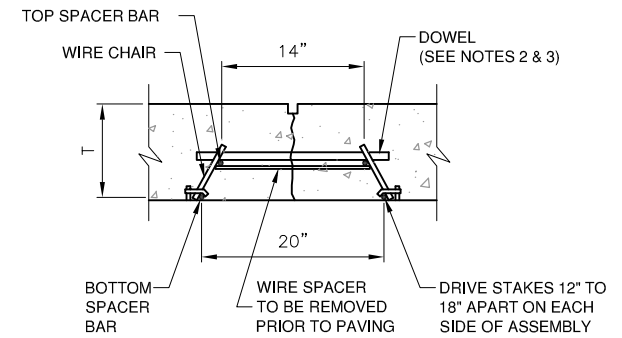
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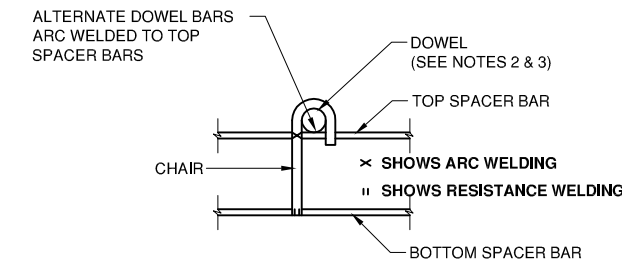
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APPROVED BY:	RLV
DATE:	MARCH 19, 2013
JOB No:	12085-05-00
IL PROJ. NO. BMI-4288	
AIP PROJ. NO. 3-17-0006-XX	
SHEET	21 OF 55 SHEETS



DOWEL BASKET ASSEMBLY DETAIL



DOWEL BAR INSTALLATION DETAIL

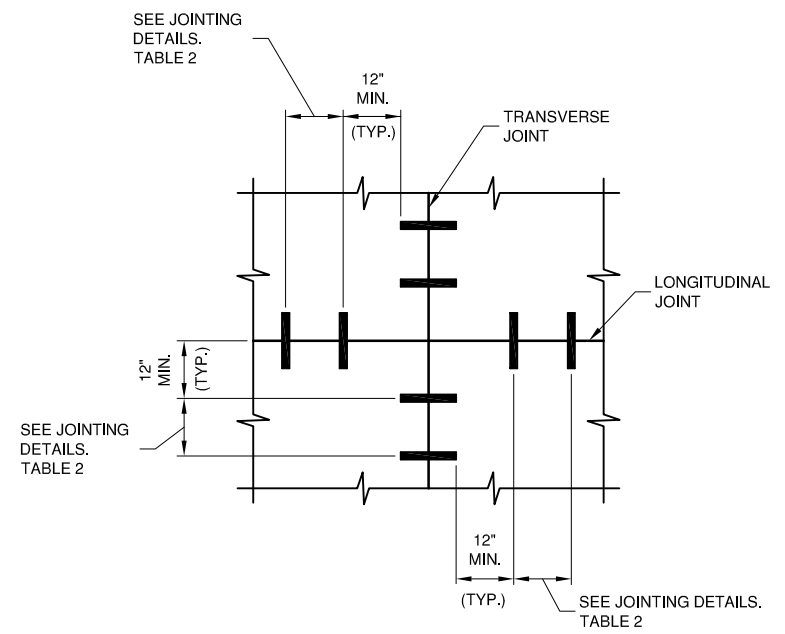


**TYPICAL DOWEL BASKET ELEVATION
 DETAIL SHOWING CHAIR**

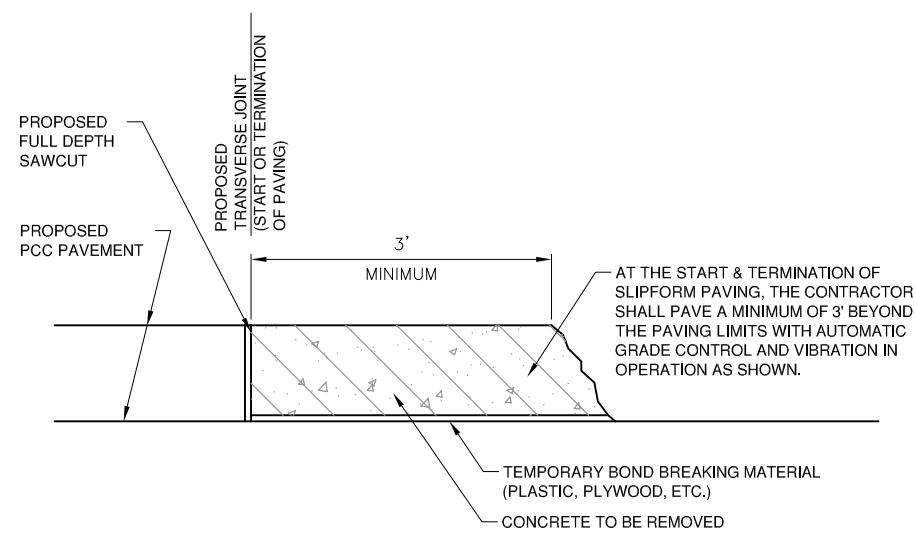
**DOWEL BASKET DETAILS
 N.T.S.**

DOWEL BASKET NOTES

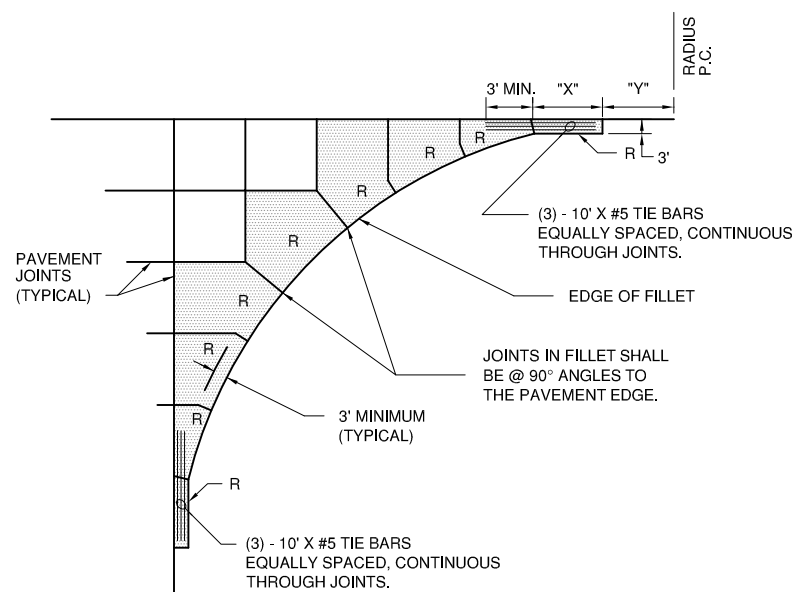
- #9 GA. WIRE SPACER BAR ARC WELDED TO THE BOTTOM OF TOP SPACER BAR. (MAY BE MECHANICALLY ATTACHED IN LIEU OF WELDING) 3 REQUIRED PER UNIT. THIS WIRE MUST BE CUT OR REMOVED PRIOR TO PAVING.
- DOWEL BAR DIAMETER, LENGTH & SPACING SHALL BE AS SHOWN IN TABLE 2.
- DOWELS SHALL BE EPOXY COATED FULL LENGTH OF DOWEL. IMMEDIATELY PRIOR TO PAVING, THE FREE END OF EACH DOWEL SHALL BE LUBRICATED OR OILED, FOR HALF THE LENGTH OF THE DOWEL.



**DOWEL PLACEMENT DETAIL
 N.T.S.**



**REQUIREMENTS AT START & TERMINATION OF SLIPFORM PAVING
 N.T.S.**



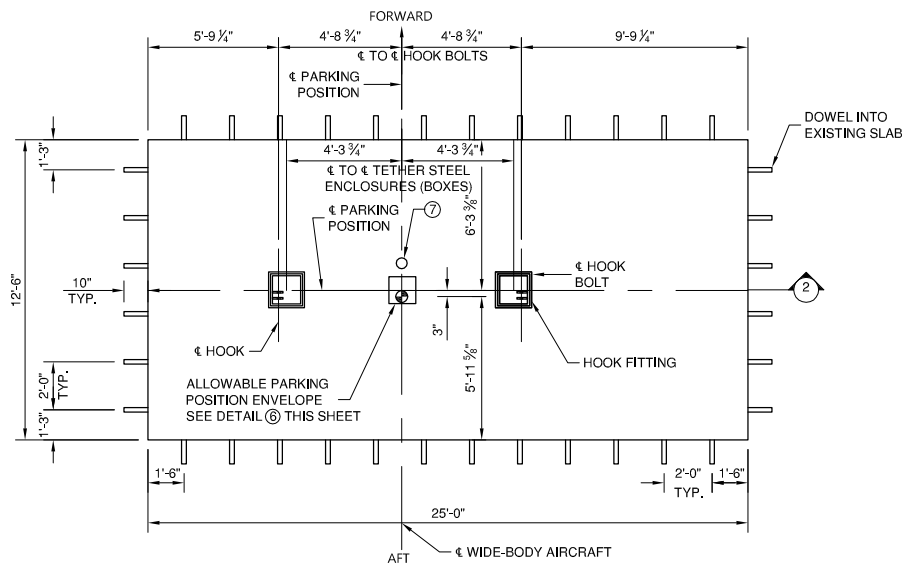
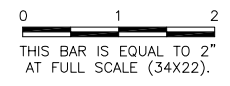
**FILLET DETAIL & FILLET REINFORCING LAYOUT
 N.T.S.**

DENOTES ODD SHAPED REINFORCED PANELS TO BE REINFORCED WITH WELDED WIRE FABRIC AS SHOWN ON JOINTING DETAILS SHEET. ALL NON RECTANGULAR SHAPED PANELS SHALL BE REINFORCED. (REINFORCEMENT NOT SHOWN)

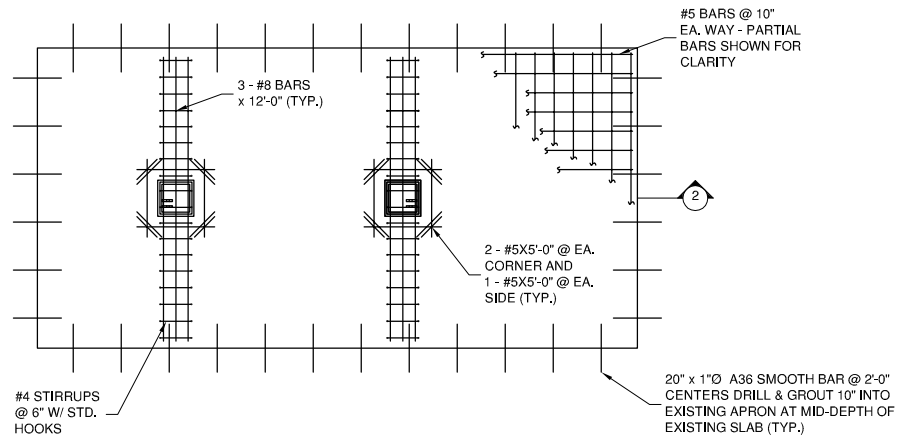
FILLET RADIUS	"X" (IN FEET)	"Y" (IN FEET)
20	4.30	6.24
25	4.88	7.00
30	5.40	7.68
50	7.11	9.95
75	8.79	12.21
85	9.38	13.00
100	10.21	14.11
125	11.44	15.78
150	12.56	17.29
175	13.58	18.68
200	14.53	19.98

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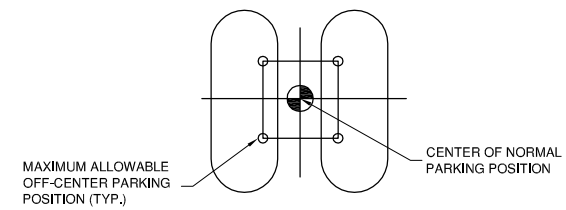
REVISIONS		
NUMBER	BY	DATE



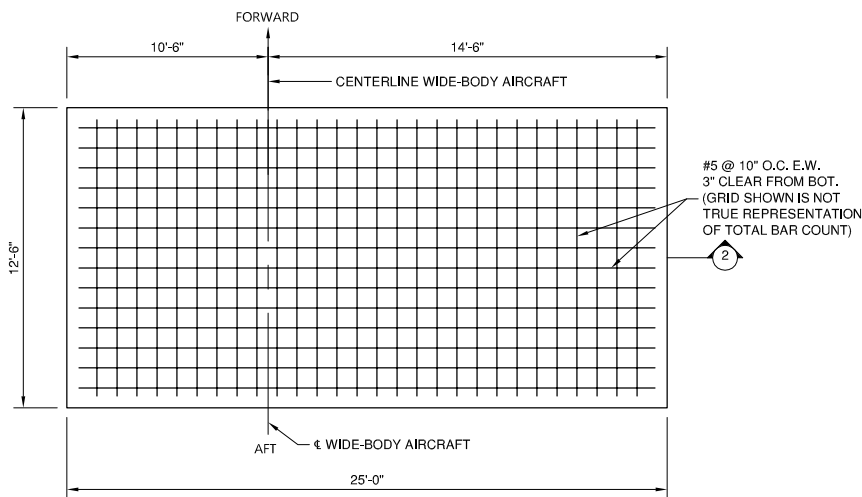
1 PLAN
N.T.S.



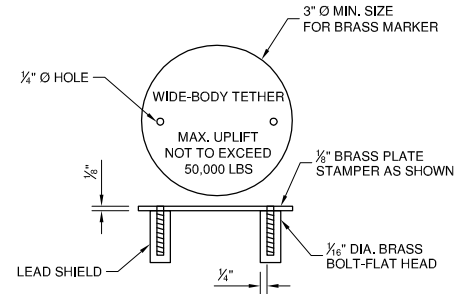
2 REINFORCING PLAN (HIGH)
N.T.S.



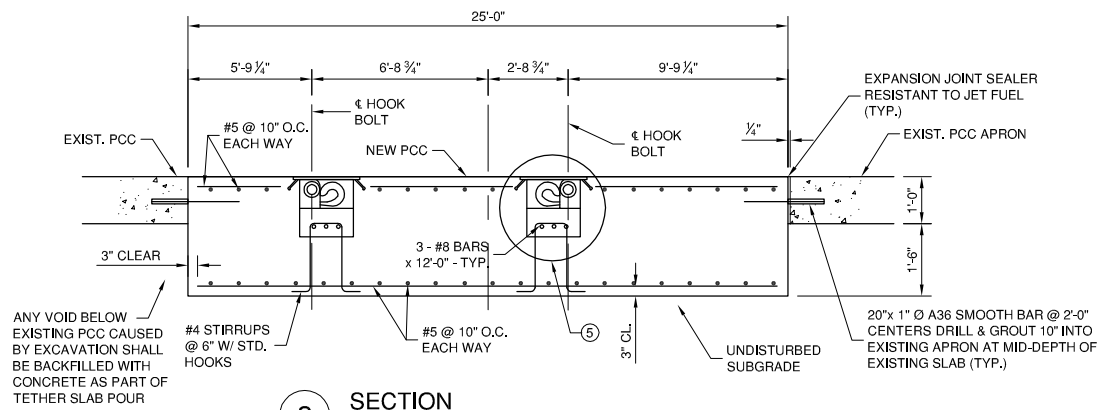
3 ALLOWABLE PARKING POSITION ENVELOPE
N.T.S.



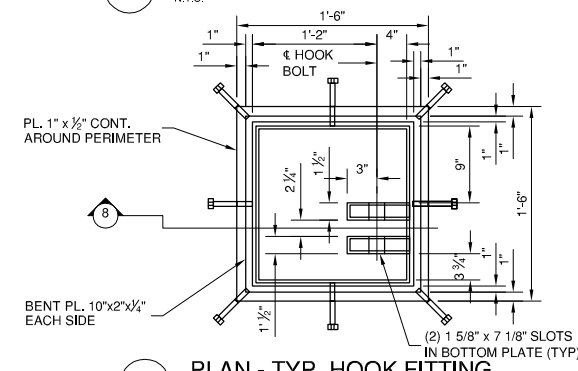
4 REINFORCING PLAN (LOW)
N.T.S.



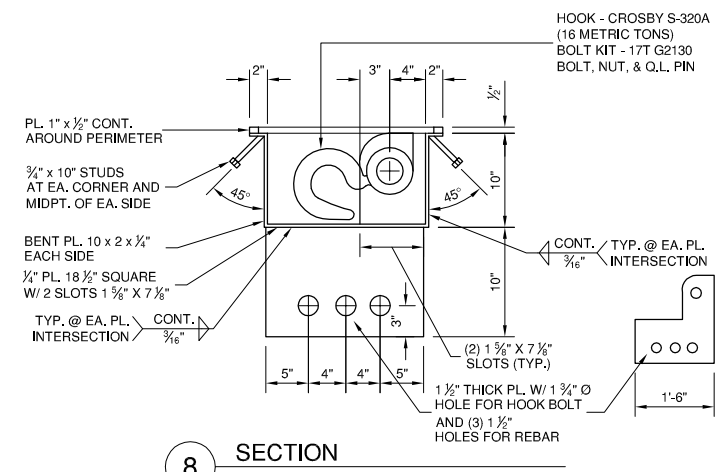
5 LOAD PLATE DETAIL
N.T.S.



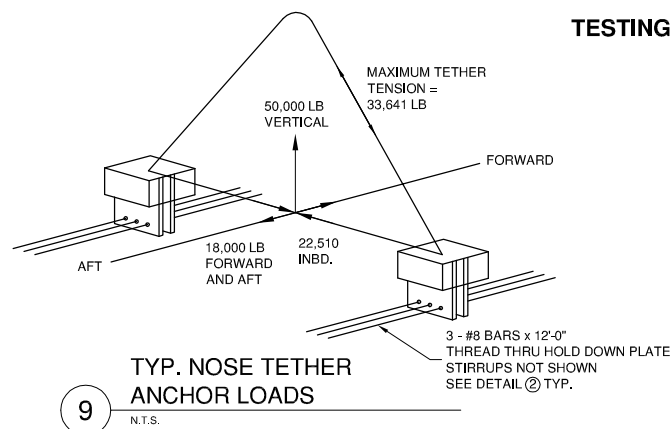
6 SECTION
N.T.S.



7 PLAN - TYP. HOOK FITTING
N.T.S.

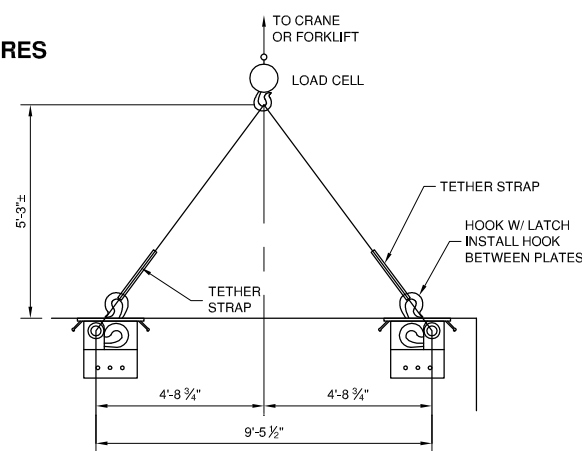


8 SECTION
N.T.S.



9 TYP. NOSE TETHER ANCHOR LOADS
N.T.S.

TESTING PROCEDURES



K:\Bloomington\12085-05_BMI Corp\Draw\Sheets



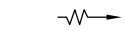

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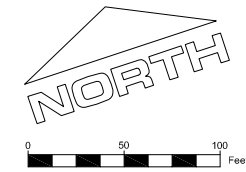
EXPAND AIR CARGO APRON
 NOSE TETHER DETAIL

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CHECKED BY:	JJF
APPROVED BY:	RLV
DATE:	MARCH 19, 2013
JOB No:	12085-05-00
IL PROJ. NO.	BMI-4288
AIP PROJ. NO.	3-17-0006-XX
SHEET	22 OF 55 SHEETS

LEGEND

-  PROPOSED CONTOUR
-  EXISTING CONTOUR
-  DRAINAGE PATH
-  NEW PAVED DITCH



FILE: 1208505-CG102.dwg
 UPDATE BY: Chris Groth
 PLOT DATE: 4/4/2013 1:48 PM

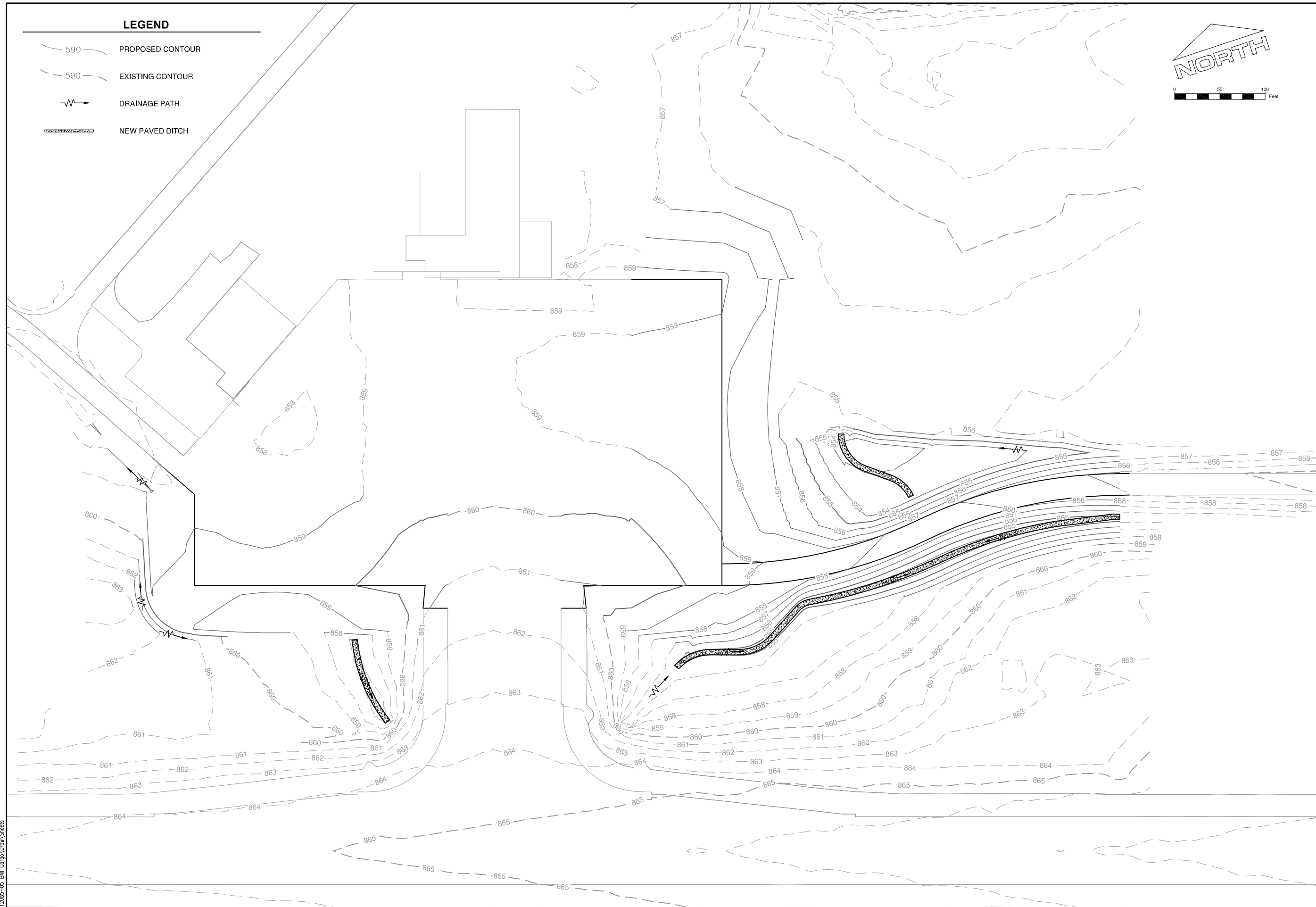
1208505-C-SPAL
 1208505-V-VF3D
 BMI-V-AERL
 BMI-V-SPXP
 1208505-C-UP

BL069

REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



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BLOOMINGTON, ILLINOIS

EXPAND AIR CARGO APRON
SITE GRADING

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BL069

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EXPAND AIR CARGO APRON
 DRAINAGE IMPROVEMENTS

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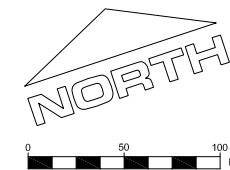
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DATE: MARCH 19, 2013

JOB No: 12085-05-00

IL PROJ. NO. BMI-4288
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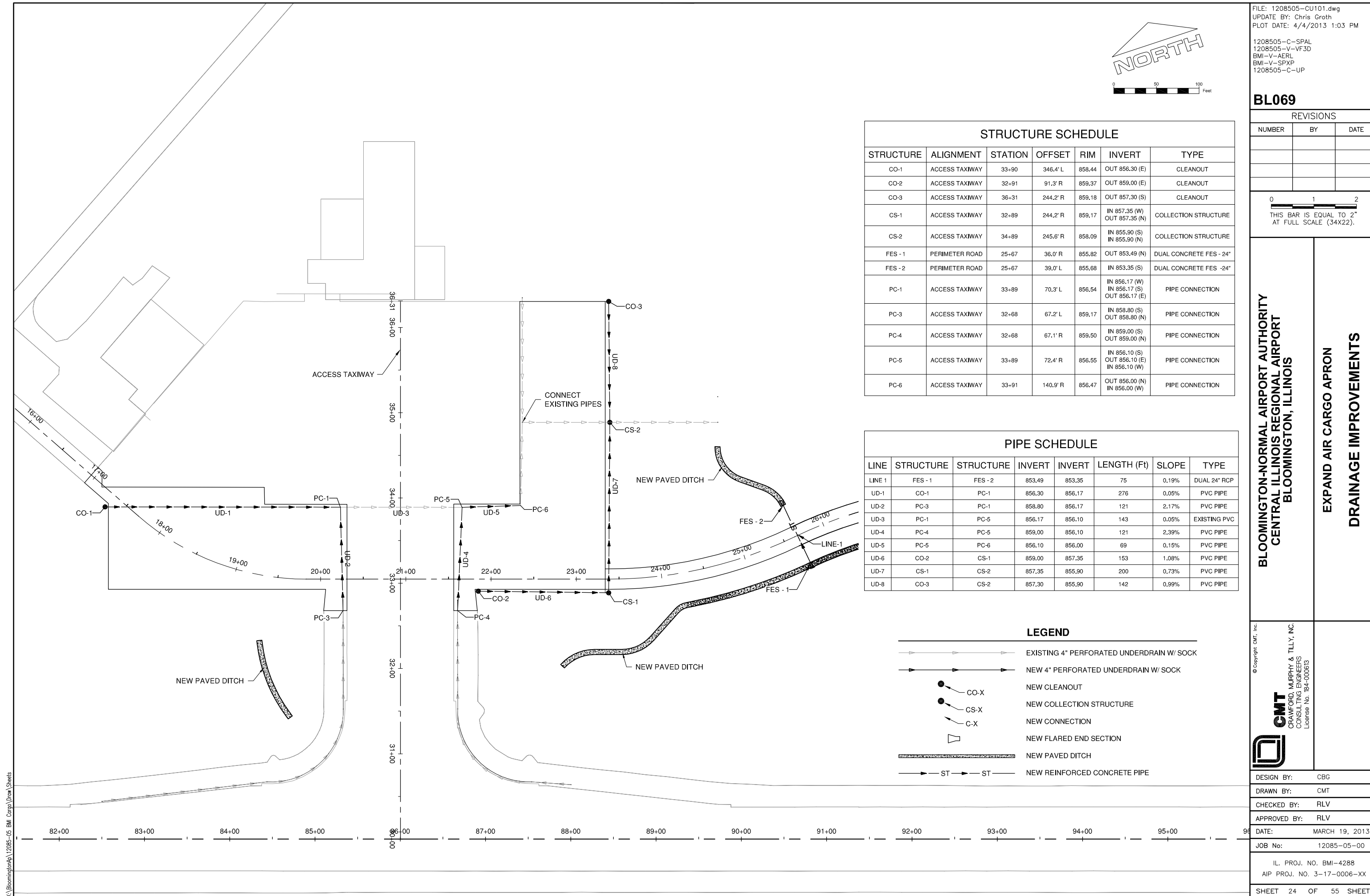
SHEET 24 OF 55 SHEETS



STRUCTURE SCHEDULE						
STRUCTURE	ALIGNMENT	STATION	OFFSET	RIM	INVERT	TYPE
CO-1	ACCESS TAXIWAY	33+90	346.4' L	858.44	OUT 856.30 (E)	CLEANOUT
CO-2	ACCESS TAXIWAY	32+91	91.3' R	859.37	OUT 859.00 (E)	CLEANOUT
CO-3	ACCESS TAXIWAY	36+31	244.2' R	859.18	OUT 857.30 (S)	CLEANOUT
CS-1	ACCESS TAXIWAY	32+89	244.2' R	859.17	IN 857.35 (W) OUT 857.35 (N)	COLLECTION STRUCTURE
CS-2	ACCESS TAXIWAY	34+89	245.6' R	858.09	IN 855.90 (S) IN 855.90 (N)	COLLECTION STRUCTURE
FES - 1	PERIMETER ROAD	25+67	36.0' R	855.82	OUT 853.49 (N)	DUAL CONCRETE FES - 24"
FES - 2	PERIMETER ROAD	25+67	39.0' L	855.68	IN 853.35 (S)	DUAL CONCRETE FES - 24"
PC-1	ACCESS TAXIWAY	33+89	70.3' L	856.54	IN 856.17 (W) IN 856.17 (S) OUT 856.17 (E)	PIPE CONNECTION
PC-3	ACCESS TAXIWAY	32+68	67.2' L	859.17	IN 858.80 (S) OUT 858.80 (N)	PIPE CONNECTION
PC-4	ACCESS TAXIWAY	32+68	67.1' R	859.50	IN 859.00 (S) OUT 859.00 (N)	PIPE CONNECTION
PC-5	ACCESS TAXIWAY	33+89	72.4' R	856.55	IN 856.10 (S) OUT 856.10 (E) IN 856.10 (W)	PIPE CONNECTION
PC-6	ACCESS TAXIWAY	33+91	140.9' R	856.47	OUT 856.00 (N) IN 856.00 (W)	PIPE CONNECTION

PIPE SCHEDULE							
LINE	STRUCTURE	STRUCTURE	INVERT	INVERT	LENGTH (Ft)	SLOPE	TYPE
LINE 1	FES - 1	FES - 2	853.49	853.35	75	0.19%	DUAL 24" RCP
UD-1	CO-1	PC-1	856.30	856.17	276	0.05%	PVC PIPE
UD-2	PC-3	PC-1	858.80	856.17	121	2.17%	PVC PIPE
UD-3	PC-1	PC-5	856.17	856.10	143	0.05%	EXISTING PVC
UD-4	PC-4	PC-5	859.00	856.10	121	2.39%	PVC PIPE
UD-5	PC-5	PC-6	856.10	856.00	69	0.15%	PVC PIPE
UD-6	CO-2	CS-1	859.00	857.35	153	1.08%	PVC PIPE
UD-7	CS-1	CS-2	857.35	855.90	200	0.73%	PVC PIPE
UD-8	CO-3	CS-2	857.30	855.90	142	0.99%	PVC PIPE

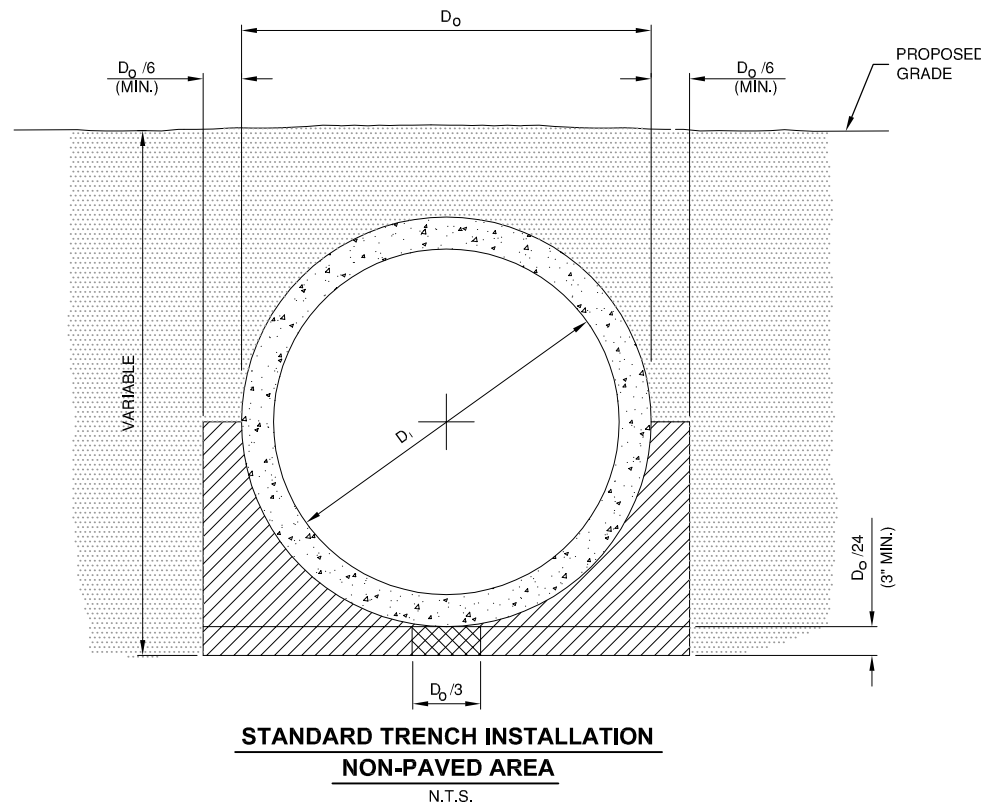
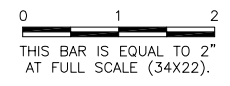
LEGEND	
	EXISTING 4" PERFORATED UNDERDRAIN W/ SOCK
	NEW 4" PERFORATED UNDERDRAIN W/ SOCK
	NEW CLEANOUT
	NEW COLLECTION STRUCTURE
	NEW CONNECTION
	NEW FLARED END SECTION
	NEW PAVED DITCH
	NEW REINFORCED CONCRETE PIPE



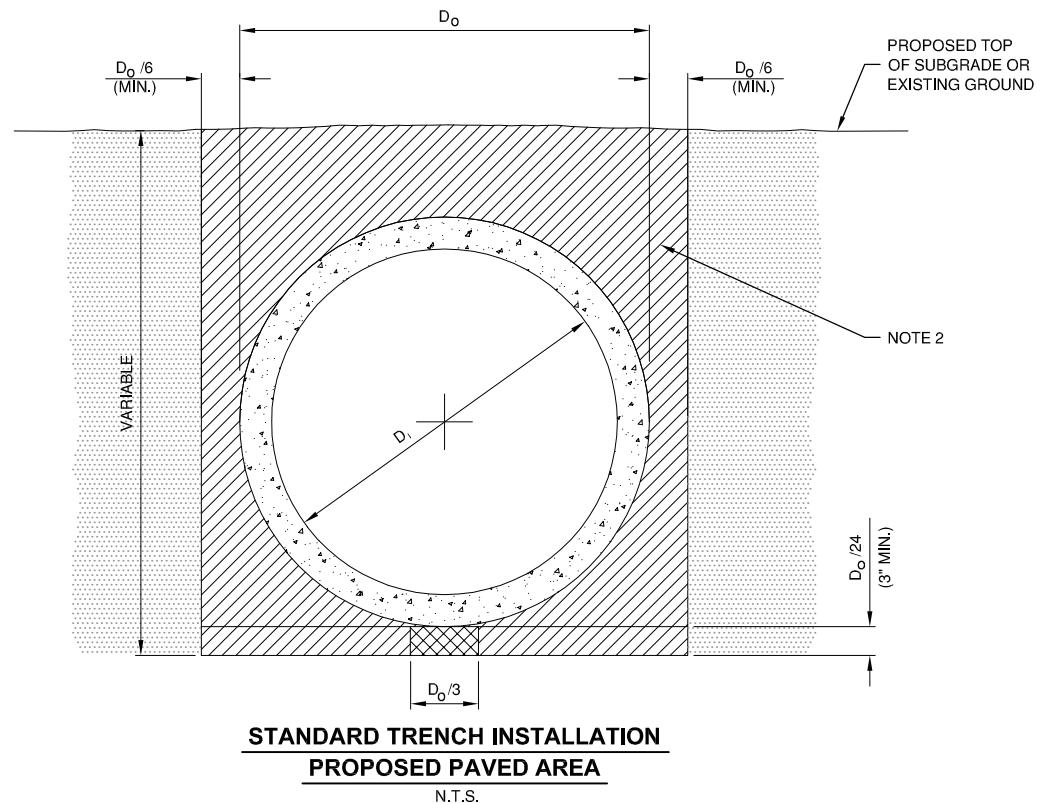
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REVISIONS		
NUMBER	BY	DATE



**STANDARD TRENCH INSTALLATION
 NON-PAVED AREA**
 N.T.S.



**STANDARD TRENCH INSTALLATION
 PROPOSED PAVED AREA**
 N.T.S.

LEGEND

- DRAINAGE CONDUIT MATERIAL-CONCRETE
- MIDDLE BEDDING LOOSELY PLACED UNCOMPACTED BEDDING
- HAUNCH AND OUTER BEDDING COMPACTION- 95% STANDARD PROCTOR
- LOWER SIDE AND OVERFILL COMPACTION- SAME AS EMBANKMENT REQUIREMENTS

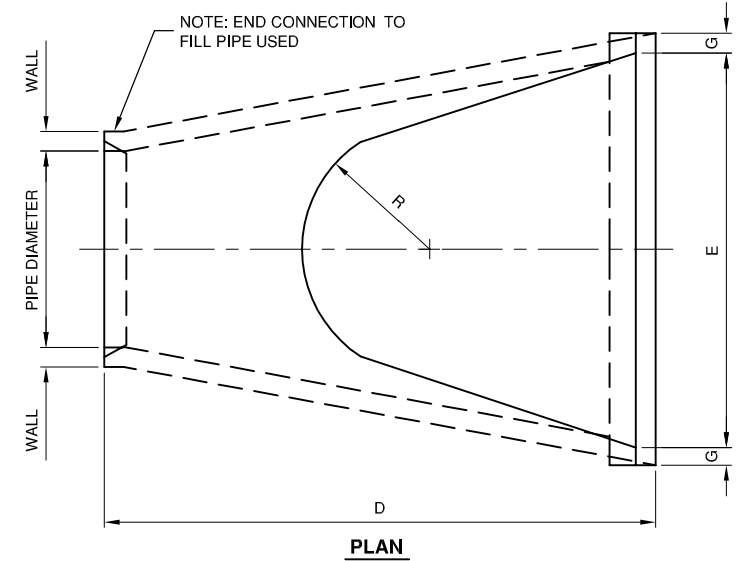
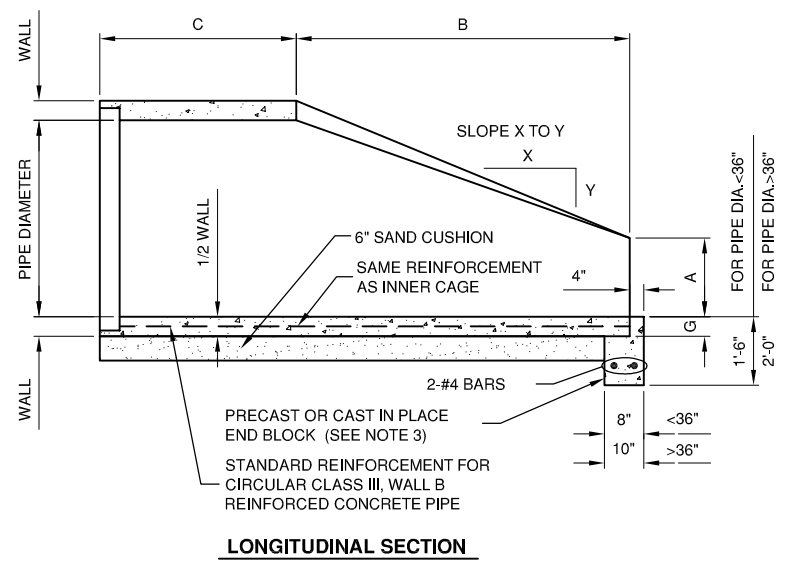
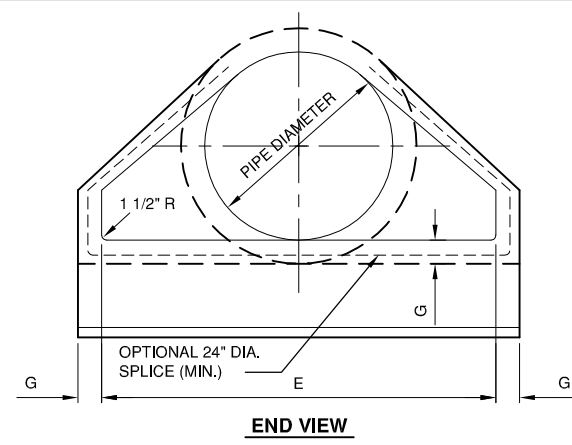
D_0 PIPE OUTSIDE DIAMETER
 D_1 PIPE INSIDE DIAMETER

- NOTES**
- BEDDING SHOWN IS IN ACCORDANCE WITH "STANDARD EMBANKMENT INSTALLATIONS", STANDARD INSTALLATION & BEDDING FACTORS FOR THE INDIRECT DESIGN METHOD (DESIGN DATA 40), AMERICAN CONCRETE PIPE ASSOCIATION.
 - BACKFILL TO EXTEND 3' BEYOND EDGES OF PROPOSED PAVEMENT.

DIMENSIONS - TABLE 1

PIPE DIA.	APPROX WT.(lbs.)	WALL	A	B	C	D	E	G	R	SLOPE
12"	530	2"	4"	2'-0"	4'-0 7/8"	6'-0 7/8"	2'-0"	2"	9"	3:1
15"	740	2 1/4"	6"	2'-3"	3'-10"	6'-1"	2'-6"	2 1/4"	11"	3:1
18"	990	2 1/2"	9"	2'-3"	3'-10"	6'-1"	3'-0"	2 1/2"	12"	3:1
21"	1280	2 3/4"	9"	2'-11"	3'-2"	6'-1"	3'-6"	2 3/4"	13"	3:1
24"	1520	3"	9 1/2"	3'-7 1/2"	2'-6"	6'-1 1/2"	4'-0"	3"	14"	3:1
27"	1930	3 1/4"	10 1/2"	4'-0"	2'-1 1/2"	6'-1 1/2"	4'-6"	3 1/4"	14 1/2"	3:1
30"	2190	3 1/2"	1'-0"	4'-6"	1'-7 3/4"	6'-1 3/4"	5'-0"	3 1/2"	15"	3:1
33"	3200	3 3/4"	1'-1 1/2"	4'-10 1/2"	3'-3 1/4"	8'-1 3/4"	5'-6"	3 3/4"	17 1/2"	3:1
36"	4100	4"	1'-3"	5'-3"	2'-10 3/4"	8'-1 3/4"	6'-0"	4"	20"	3:1
42"	5380	4 1/2"	1'-9"	5'-3"	2'-11"	8'-2"	6'-6"	4 1/2"	22"	3:1
48"	6550	5"	2'-0"	6'-0"	2'-2"	8'-2"	7'-0"	5"	22"	3:1
54"	8240	5 1/2"	2'-3"	5'-5"	2'-11"	8'-4"	7'-6"	5 1/2"	24"	2.4:1
60"	8730	6"	2'-11"	5'-0"	3'-3"	8'-3"	8'-0"	5"	*	2:1
66"	10710	6 1/2"	2'-6"	6'-0"	2'-3"	8'-3"	8'-6"	5 1/2"	*	2:1
72"	12520	7"	3'-0"	6'-6"	1'-9"	8'-3"	9'-0"	6"	*	1.86:1
78"	14770	7 1/2"	3'-0"	7'-6"	1'-9"	9'-3"	9'-6"	6 1/2"	*	1.82:1
84"	18160	8"	3'-0"	7'-6 1/2"	1'-9"	9'-3 1/2"	10'-0"	6 1/2"	*	1.5:1

* RADIUS AS FURNISHED BY MANUFACTURER.



PRECAST REINFORCED CONCRETE FLARED END SECTION
 (I.D.O.T. STD. NO. 542301)

N.T.S.

- NOTES**
- PRECAST CONCRETE FLARED END SECTIONS SHALL CONFORM TO THE APPLICABLE REQUIREMENTS OF AASHTO M-170 CLASS III, WALL B REINFORCED CONCRETE PIPE.
 - PRECAST CONCRETE FLARED END SECTION FOR PIPE DIAMETER REQUIRED SHALL BE AS INDICATED ON DETAIL PLAN FOR EACH INDIVIDUAL INSTALLATION.
 - THE END BLOCK SHALL BE PLACED PRIOR TO THE INSTALLATION OF THE FLARED END SECTION. THE END BLOCK SHALL BE BACKFILLED IN ACCORDANCE WITH ITEM 701.

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 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

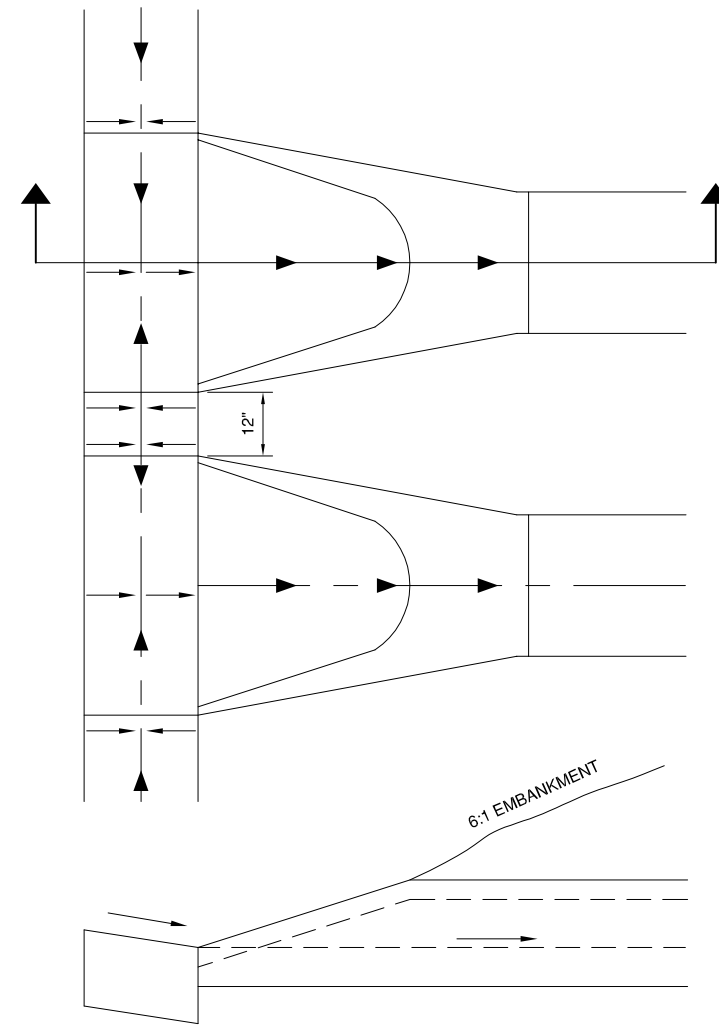
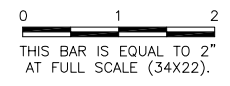
**EXPAND AIR CARGO APRON
 DRAINAGE DETAILS 1**

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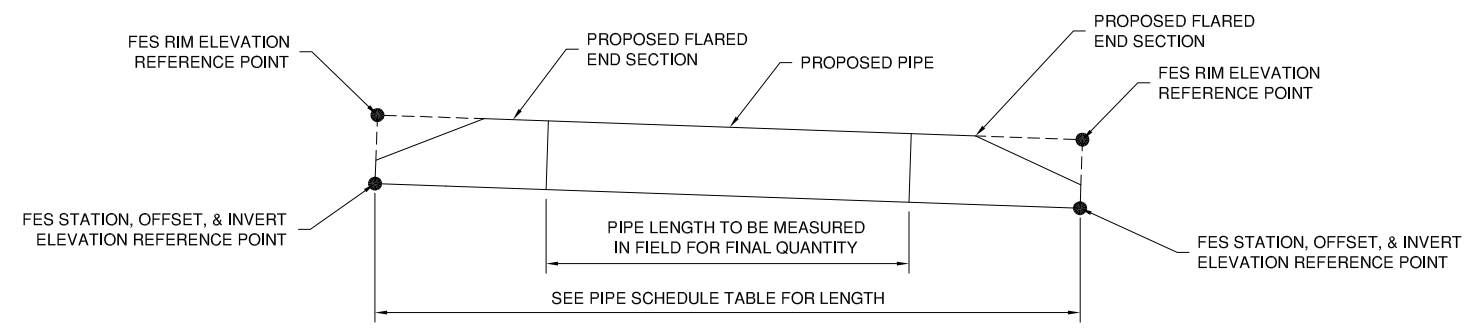
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JOB No:	12085-05-00
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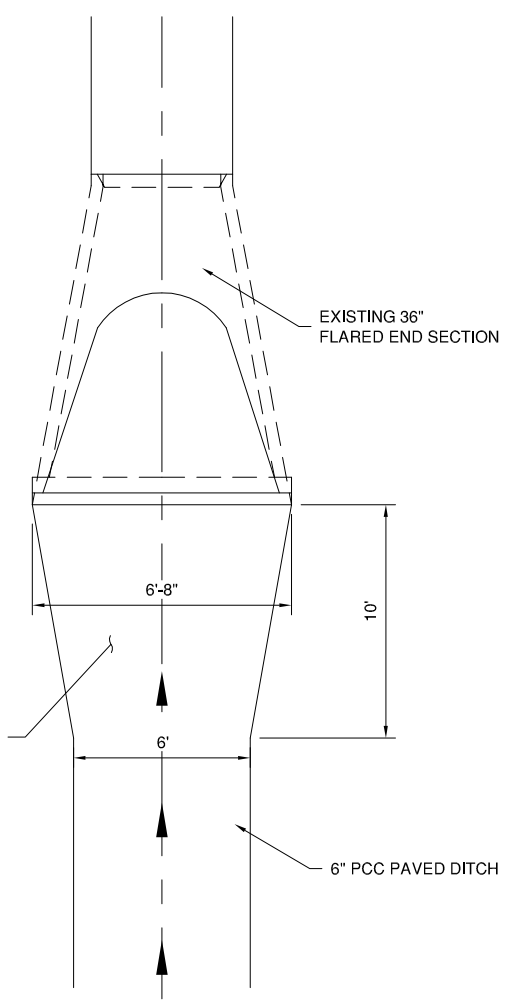
REVISIONS		
NUMBER	BY	DATE



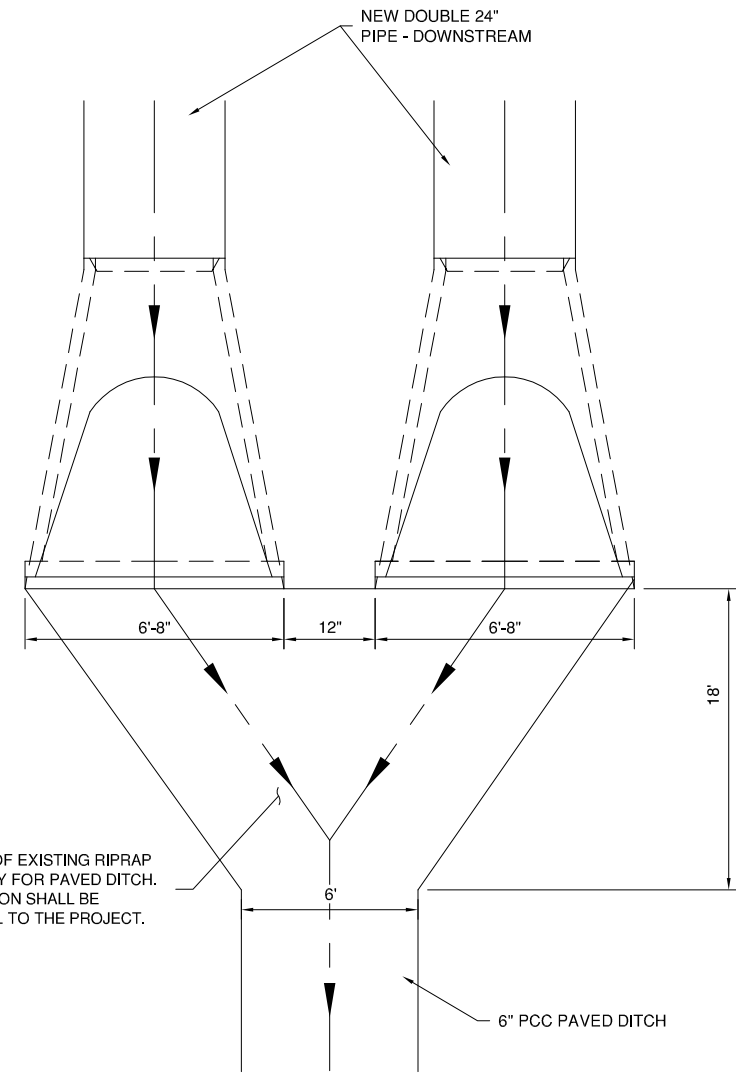
PAVED DITCH @ NEW DOUBLE 24" PIPE UPSTREAM
 N.T.S.



DRAINAGE SCHEDULE REFERENCE DETAIL
 N.T.S.



36" FES AND PAVED DITCH DETAIL
 N.T.S.



NEW DOUBLE 24" F.E.S. AND PAVED DITCH DETAIL
 N.T.S.

**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

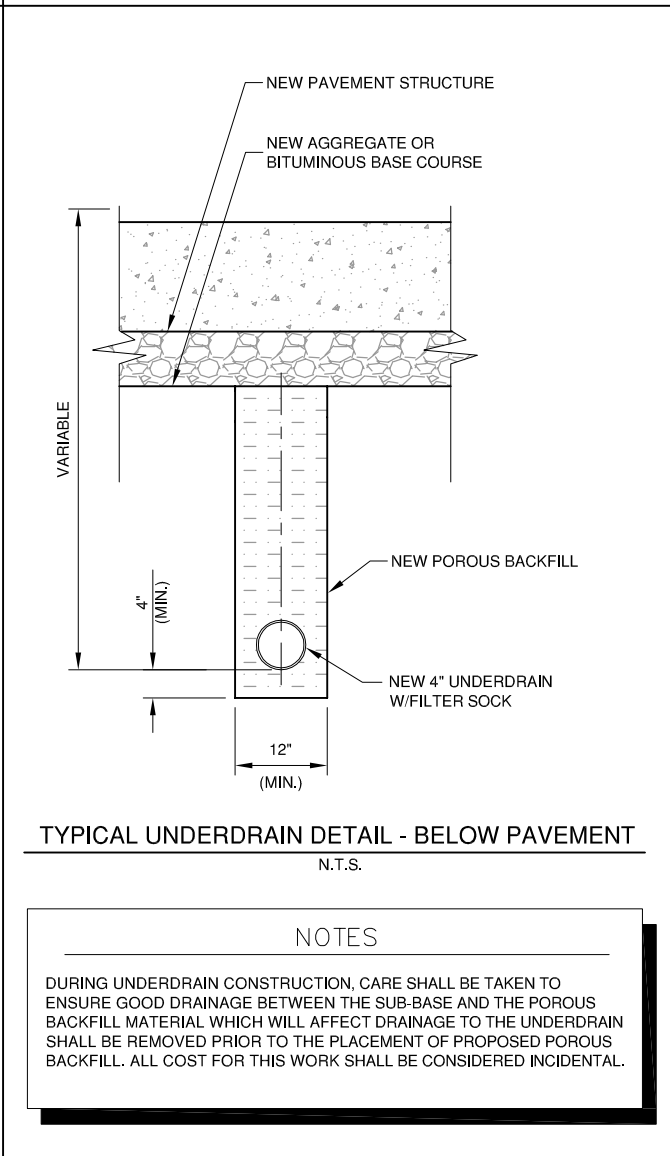
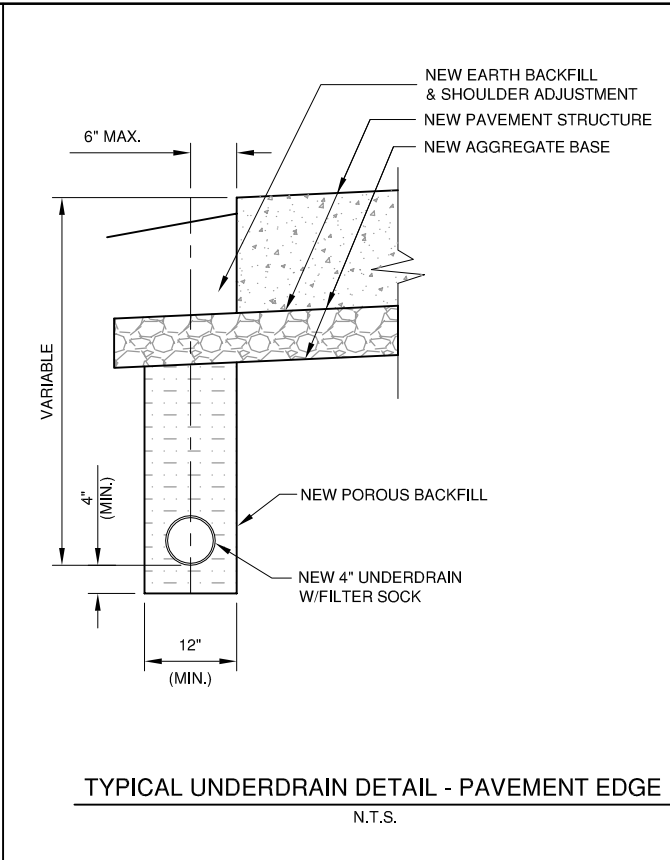
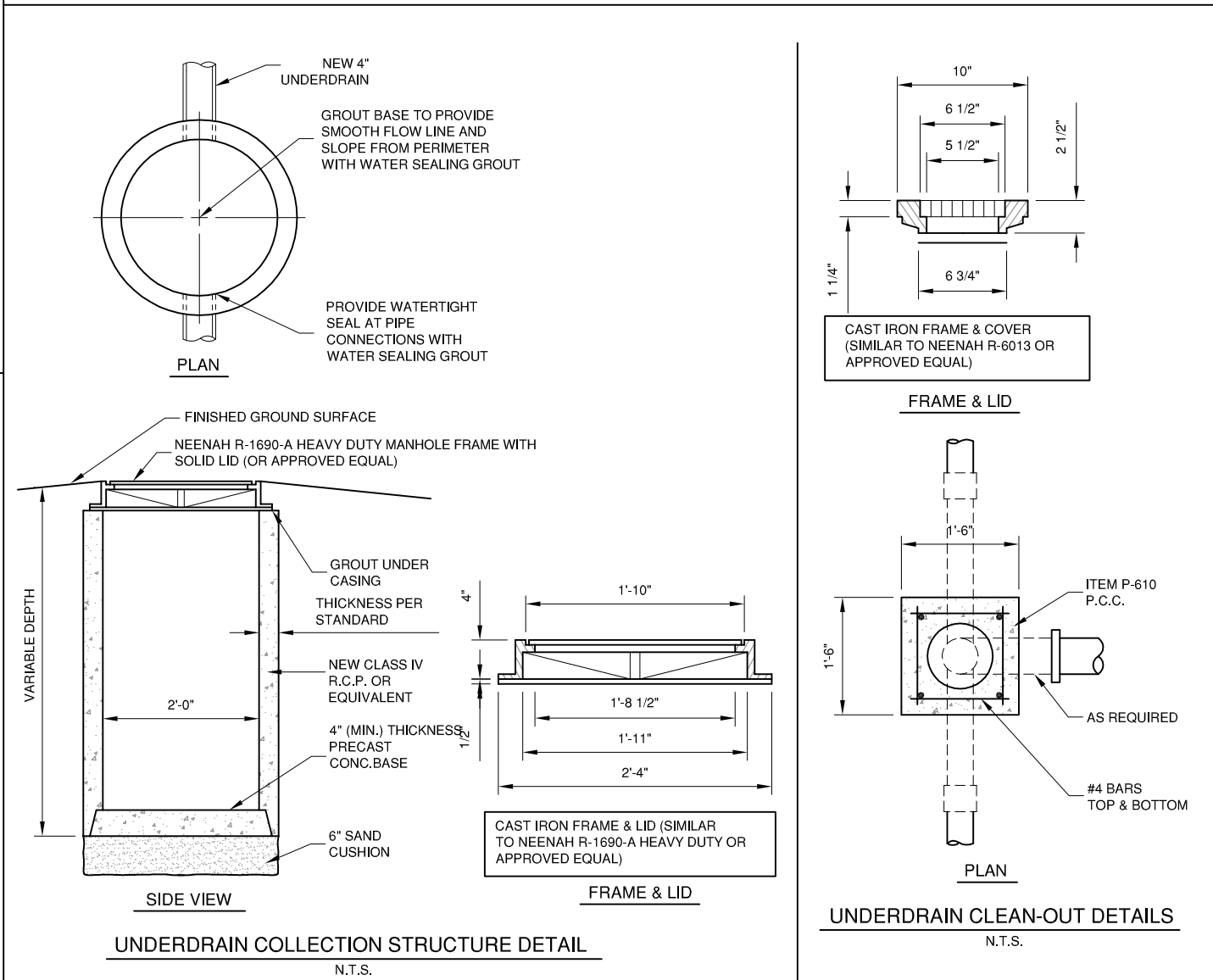
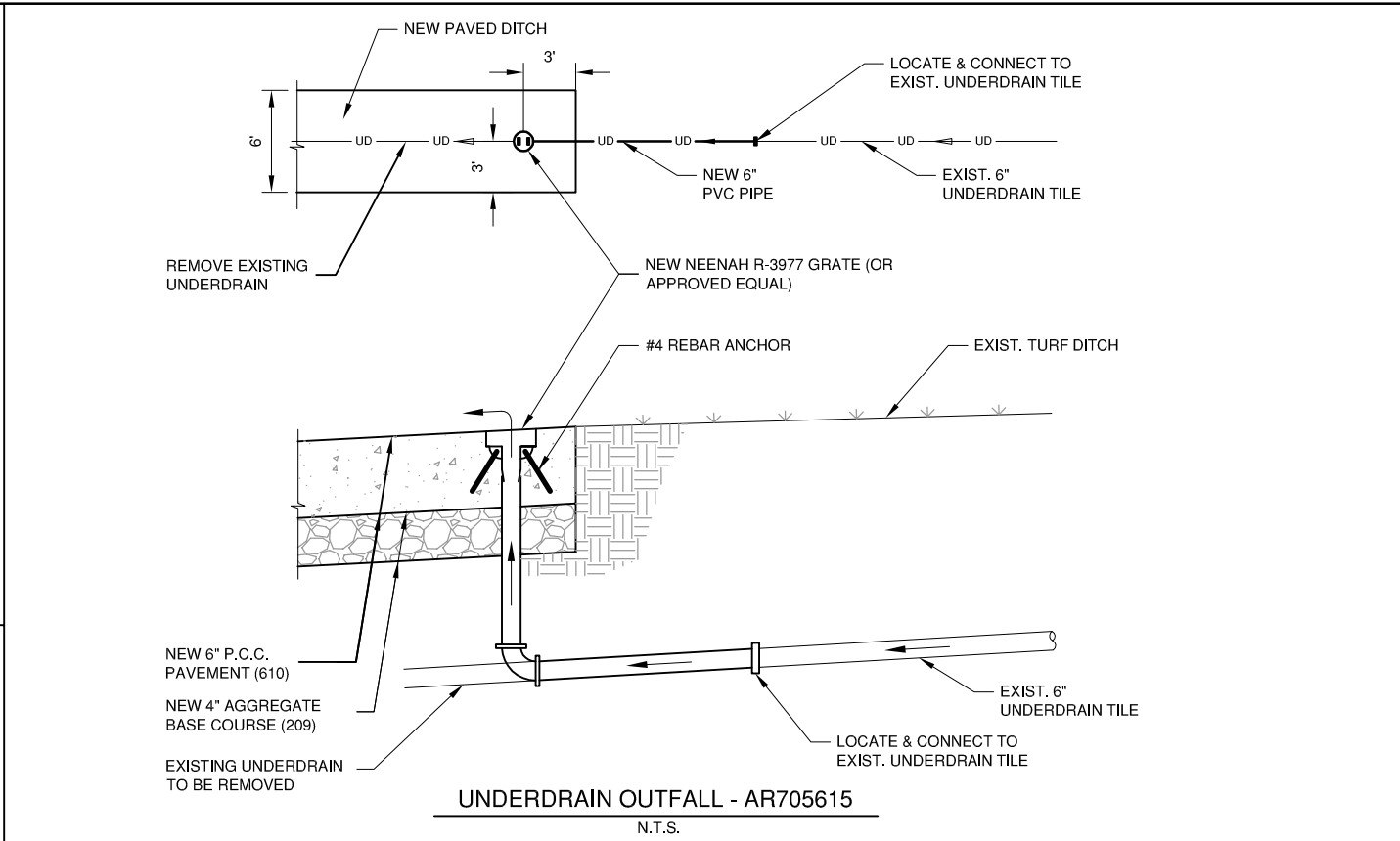
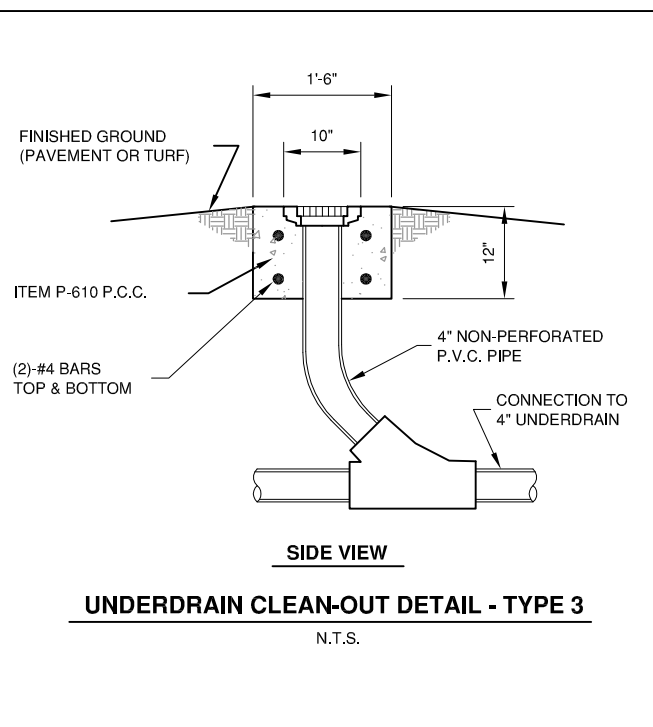
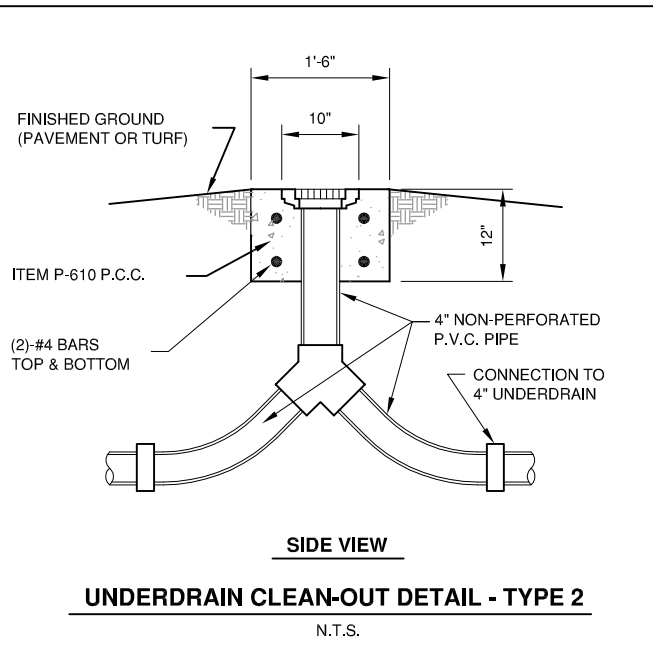
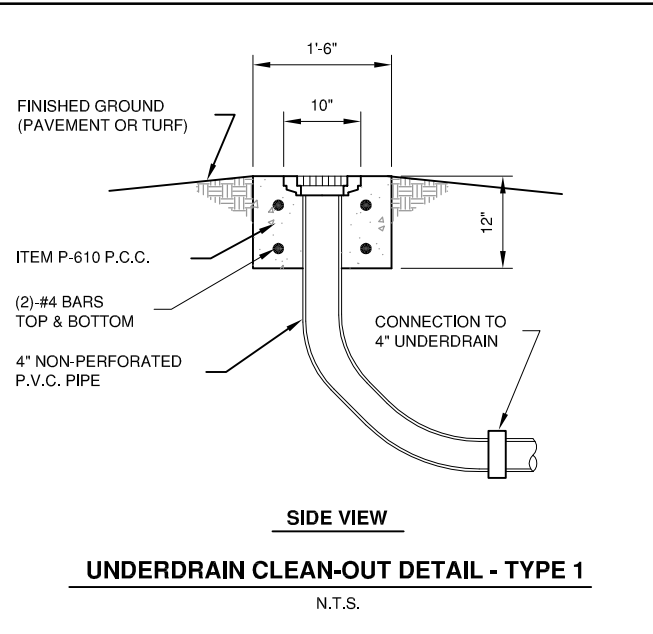
**EXPAND AIR CARGO APRON
 DRAINAGE DETAILS 2**

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CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	MARCH 19, 2013
JOB No:	12085-05-00

IL PROJ. NO. BMI-4288
 AIP PROJ. NO. 3-17-0006-XX

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NOTES

DURING UNDERDRAIN CONSTRUCTION, CARE SHALL BE TAKEN TO ENSURE GOOD DRAINAGE BETWEEN THE SUB-BASE AND THE POROUS BACKFILL MATERIAL WHICH WILL AFFECT DRAINAGE TO THE UNDERDRAIN SHALL BE REMOVED PRIOR TO THE PLACEMENT OF PROPOSED POROUS BACKFILL. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL.

FILE: 1208505-CU503.dwg
UPDATE BY: Chris Groth
PLOT DATE: 4/4/2013 1:04 PM

BL069

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0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

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BLOOMINGTON, ILLINOIS**

**EXPAND AIR CARGO APRON
UNDERDRAIN DETAILS**

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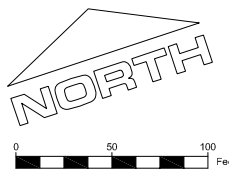
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SHEET 27 OF 55 SHEETS

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FILE: 1208505-CM101.dwg
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 PLOT DATE: 4/4/2013 1:05 PM
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BL069

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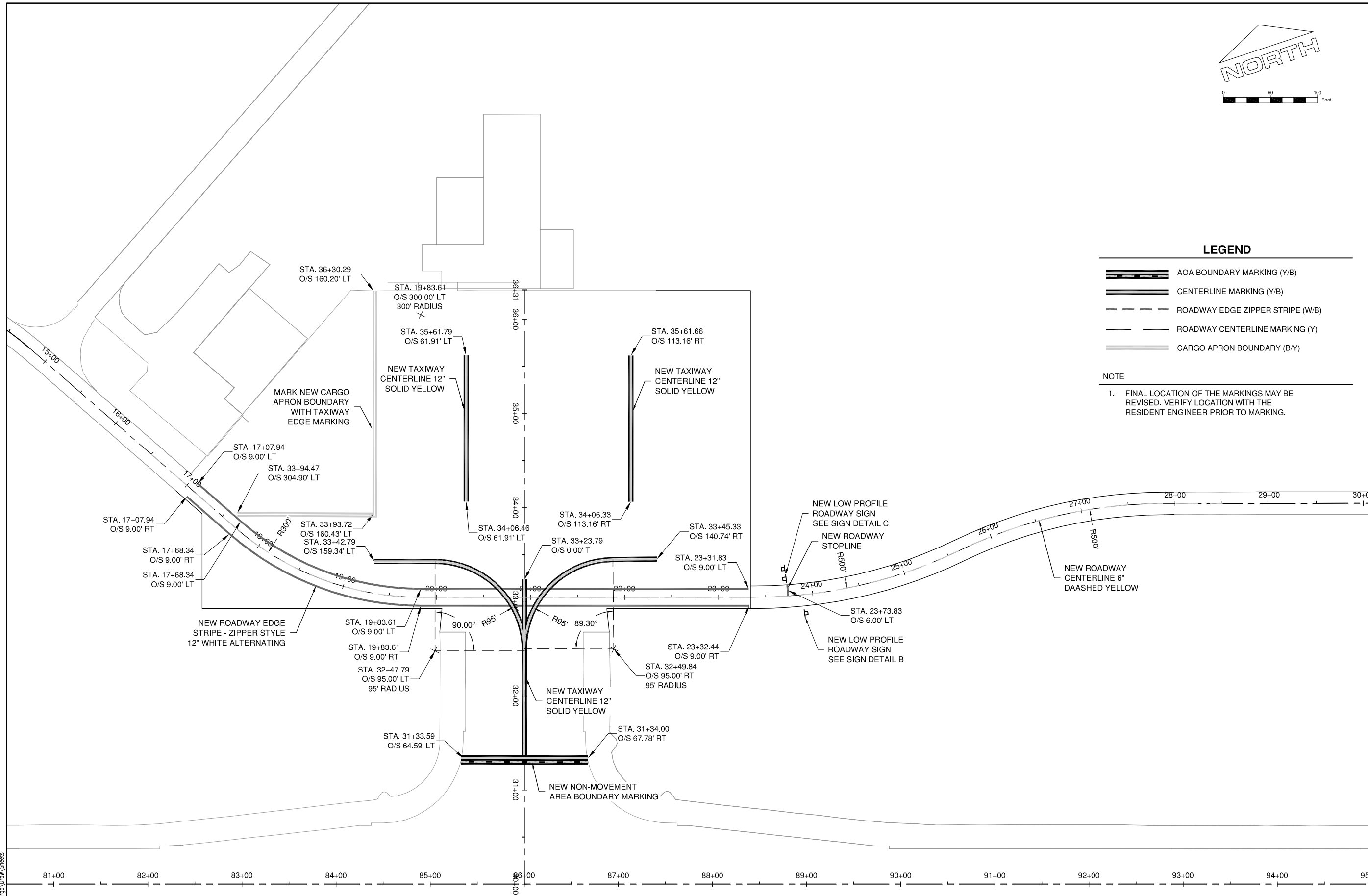
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 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

LEGEND

- AOA BOUNDARY MARKING (Y/B)
- CENTERLINE MARKING (Y/B)
- ROADWAY EDGE ZIPPER STRIPE (W/B)
- ROADWAY CENTERLINE MARKING (Y)
- CARGO APRON BOUNDARY (B/Y)

NOTE

1. FINAL LOCATION OF THE MARKINGS MAY BE REVISED. VERIFY LOCATION WITH THE RESIDENT ENGINEER PRIOR TO MARKING.



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**EXPAND AIR CARGO APRON
 MARKING PLAN**

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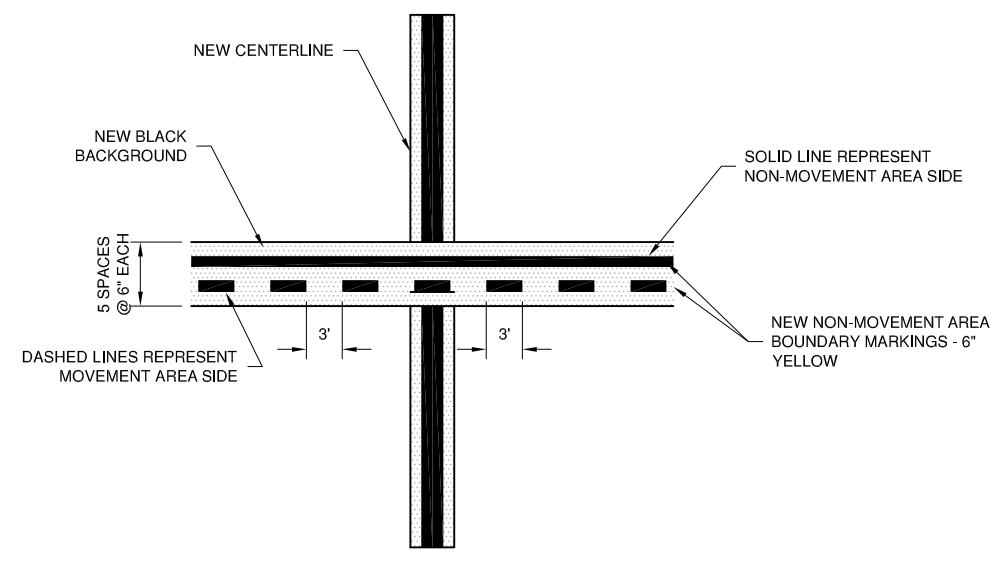
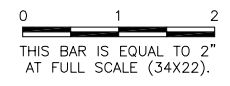
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SHEET 28 OF 55 SHEETS	

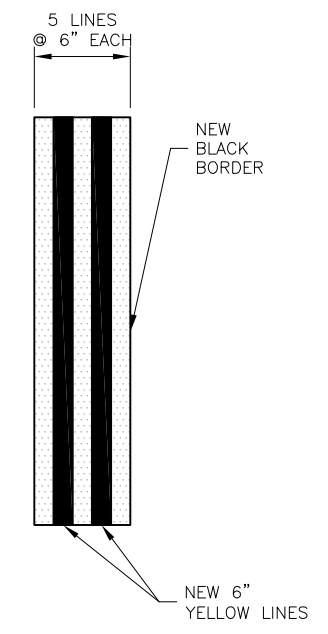
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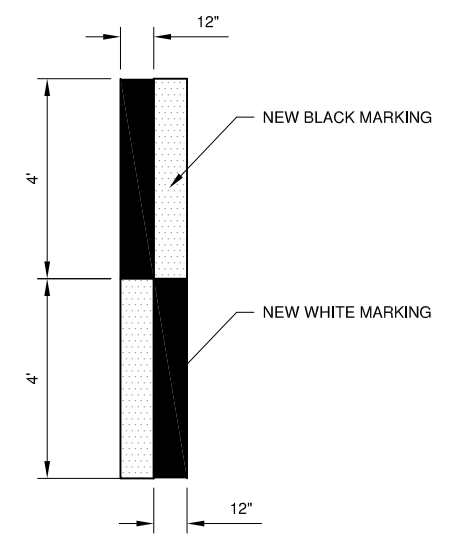
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NUMBER	BY	DATE



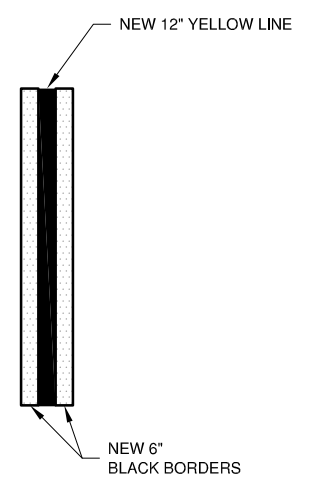
NON-MOVEMENT AREA BOUNDARY MARKING DETAIL
 N.T.S.



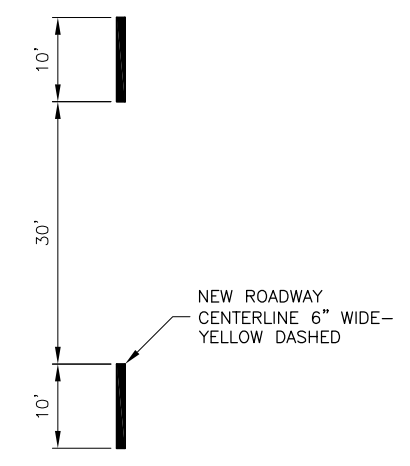
TAXIWAY EDGE MARKING CONTINUOUS
 N.T.S.



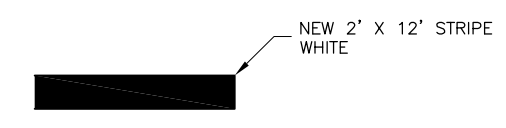
ROADWAY EDGE STRIPE DETAIL - ZIPPER STYLE
 N.T.S.



TAXIWAY CENTERLINE MARKING DETAIL
 N.T.S.




ROADWAY CENTERLINE - DASHED
 N.T.S.



NEW ROADWAY STOPLINE
 N.T.S.

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BLOOMINGTON, ILLINOIS

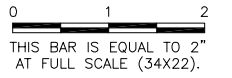
EXPAND AIR CARGO APRON MARKING DETAILS

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AIP PROJ. NO. 3-17-0006-XX	
SHEET	29 OF 55 SHEETS

BL069

REVISIONS		
NUMBER	BY	DATE

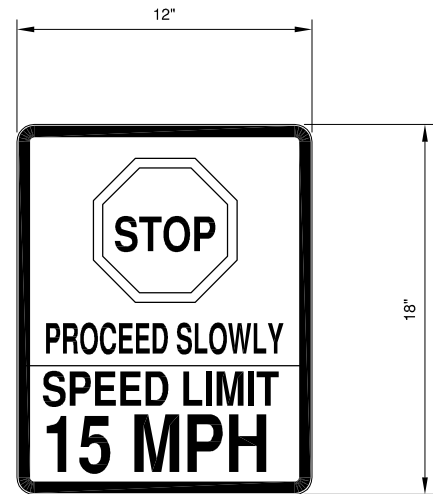


BLOOMINGTON-NORMAL AIRPORT AUTHORITY
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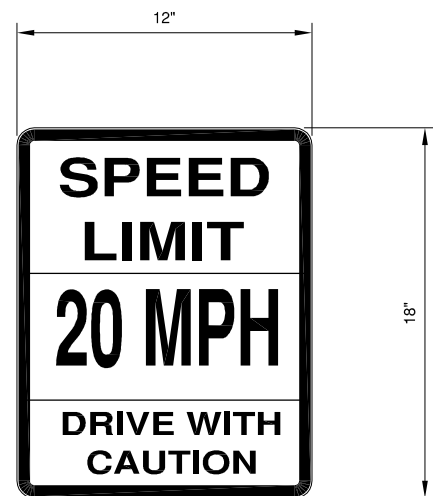
EXPAND AIR CARGO APRON
 ROADWAY SIGNAGE DETAILS

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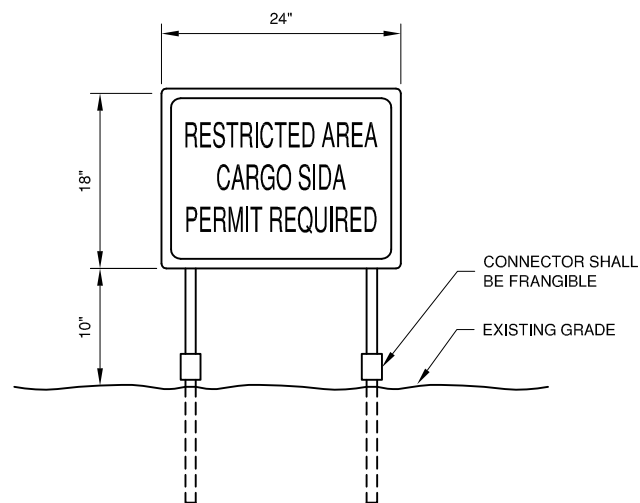
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SHEET	30 OF 55 SHEETS



A ROADWAY EDGELINES ADJACENT TO APRON - CONTINUOUS
 N.T.S.



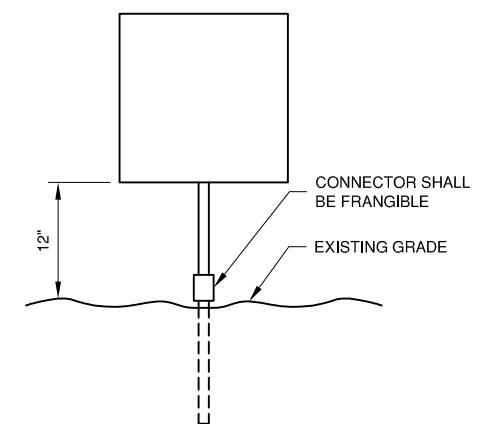
B ROADWAY EDGELINES ADJACENT TO APRON
 N.T.S.



C INFORMATION SIGN LEGEND
 N.T.S.

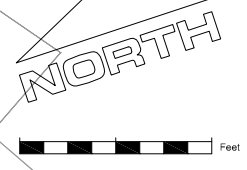
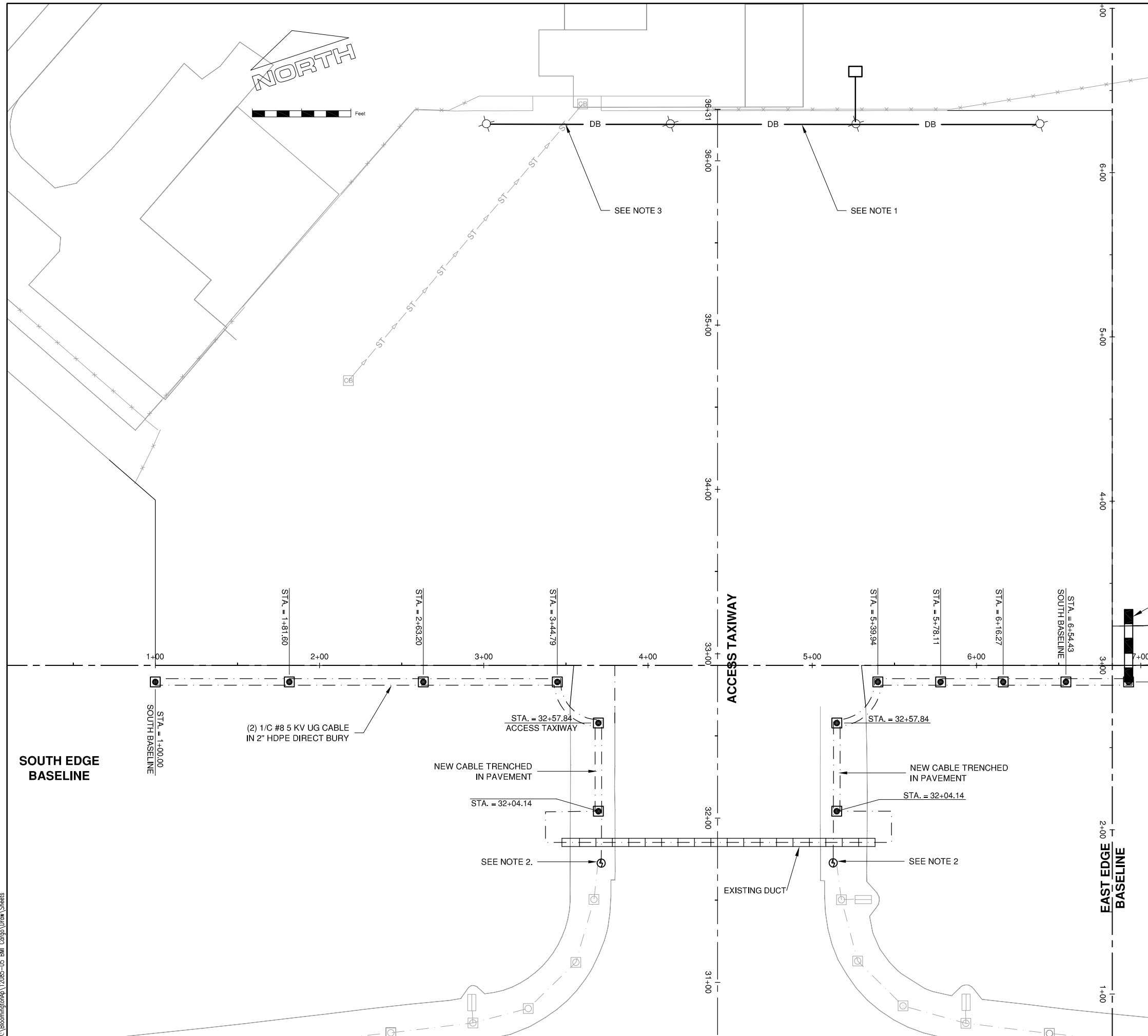
INFORMATION SIGN NOTE

1. FINAL SIGN FACE LEGEND SHALL BE DETERMINED BY THE AIRPORT.
2. SIGN LEGEND SHALL BE RED LETTERING ON WHITE BACKGROUND.



D SIGN POST DETAIL
 N.T.S.

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LEGEND

- NEW BASE LIGHT
- NEW APRON LIGHT
- SPLICE CAN
- NEW UNDERGROUND ELECTRICAL
- NEW 4-WAY DUCT
- EXISTING DUCT
- TAXIWAY E CIRCUIT
- DIRECTIONAL BORE

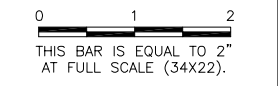
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 UPDATE BY: Chris Groth
 PLOT DATE: 4/4/2013 2:00 PM

1208505-C-SPAL
 1208505-V-VF3D
 BMI-V-AERL
 BMI-V-SPXP
 1208505-E-AL

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- NOTES:**
- SEE APRON LIGHTING PLAN FOR ADDITIONAL INFORMATION.
 - REMOVE EXISTING TAXIWAY LIGHT EQUIPMENT AND CONNECT TO TAXIWAY CIRCUIT. REPLACE LIGHT WITH SOLID BASE PLATE.
 - CONTRACTOR SHALL VERIFY THE SIZE AND LOCATION OF PIPE PRIOR TO CONSTRUCTING BORE.



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**EXPAND AIR CARGO APRON
 TAXIWAY LIGHTING PLAN**

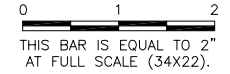
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JOB No:	12085-05-00
IL PROJ. NO.	BMI-4288
AIP PROJ. NO.	3-17-0006-XX
SHEET	31 OF 55 SHEETS

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NUMBER	BY	DATE

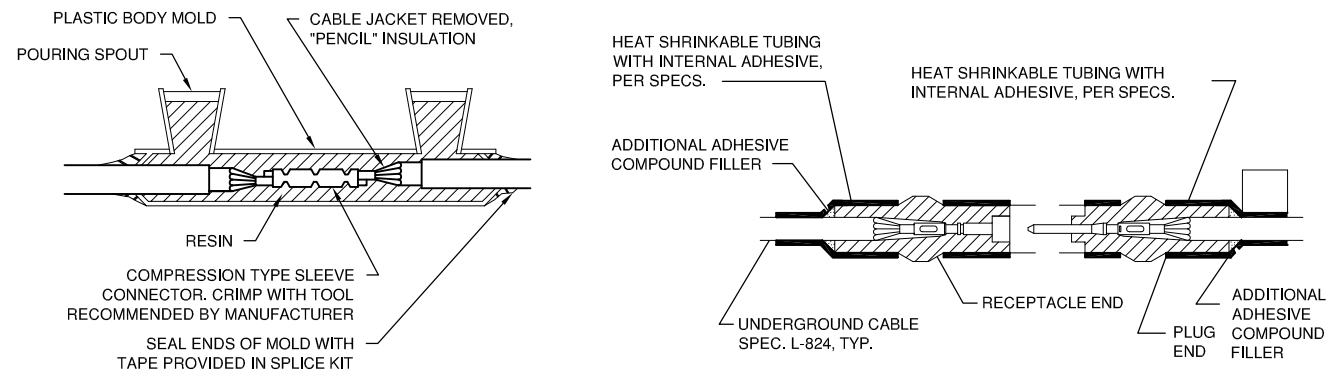


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BLOOMINGTON, ILLINOIS

EXPAND AIR CARGO APRON
ELECTRICAL DETAILS 1

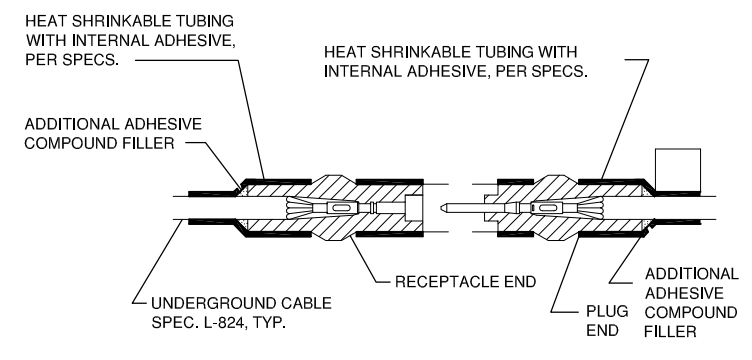
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IL PROJ. NO.	BMI-4288
AIP PROJ. NO.	3-17-0006-XX
SHEET	32 OF 55 SHEETS



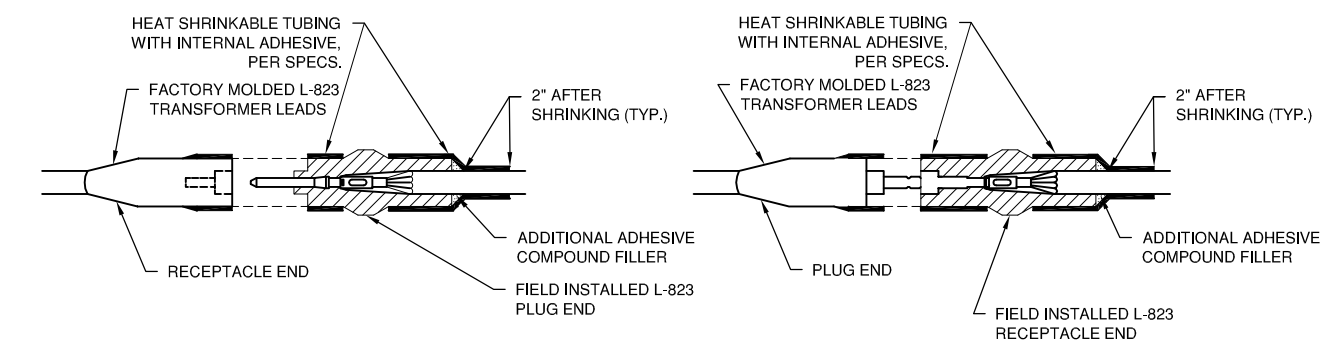
FOR IN-LINE CONNECTIONS OF EXISTING CABLES CUT DURING CONSTRUCTION.

TYPE A



NOT TO BE USED IN THIS PROJECT UNLESS OTHERWISE DIRECTED BY ENGINEER.

TYPE B



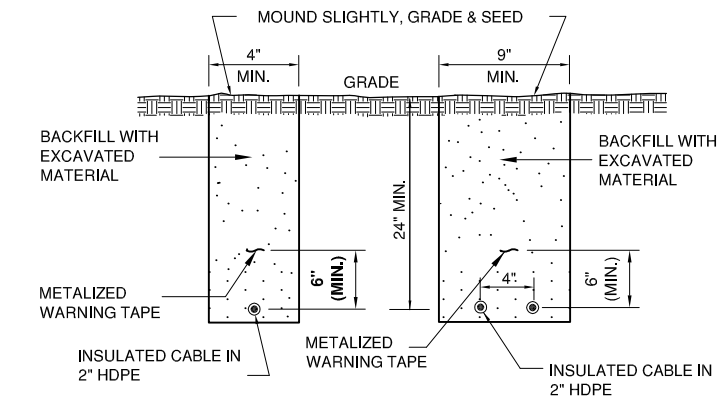
FOR SPLICES AT ISOLATION TRANSFORMERS.

TYPE C & D

CABLE SPLICES
N.T.S.

NOTES

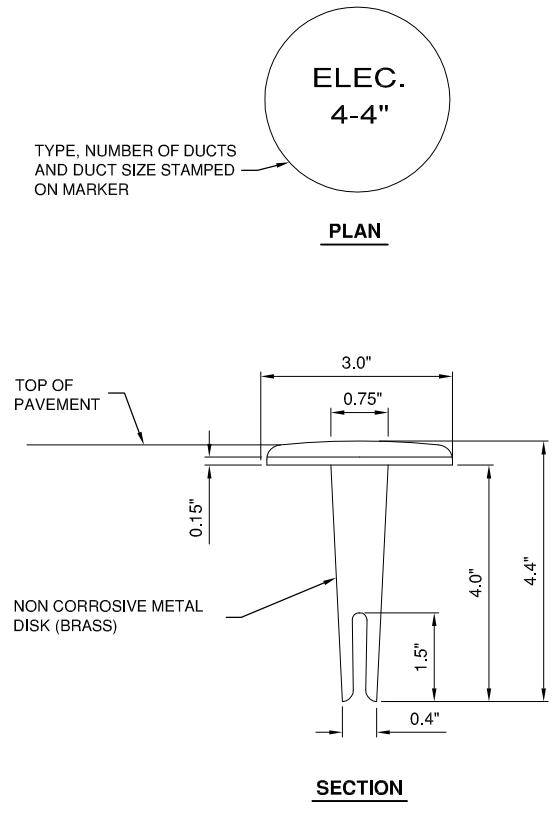
- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
- THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- THE CONTRACTOR SHALL HAVE A MINIMUM OF TWO (2) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.
- CONNECTION OF CONDUCTORS MUST BE MADE BY USING CRIMP CONNECTORS AND A CRIMPING TOOL APPROVED BY THE CONNECTOR/LUG MANUFACTURER. THE TOOL MUST PRODUCE A COMPLETE CRIMP BEFORE IT CAN BE REMOVED. THE CRIMPING TOOL USED MUST BE LISTED BY THE L-823 KIT MANUFACTURER. MAKE THE NUMBER AND TYPE OF CRIMPS PER THE KIT MANUFACTURER'S INSTRUCTIONS.



TRENCH DETAIL
N.T.S.

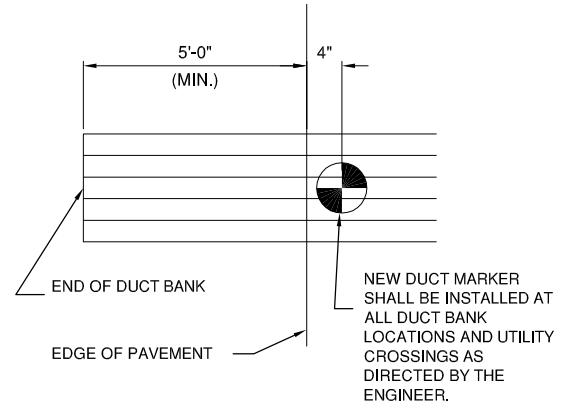
NOTES

- TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
- DEPTH OF TRENCHES SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS.
- SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
- ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO ITEM 108.



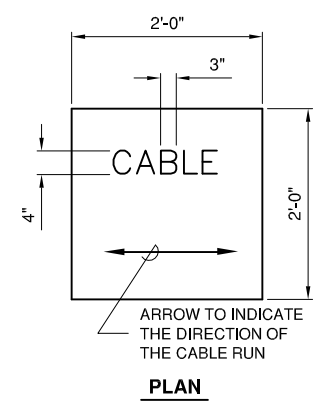
DUCT MARKER DETAILS
N.T.S.

NEW DUCT MARKERS SHALL BE INSTALLED AT BOTH EDGES OF PAVEMENT AT ALL NEW DIRECTIONAL BORE, SPLIT DUCT, AND CONCRETE ENCASED DUCT LOCATIONS. AR110 105-DUCT MARKER-IN PAVEMENT

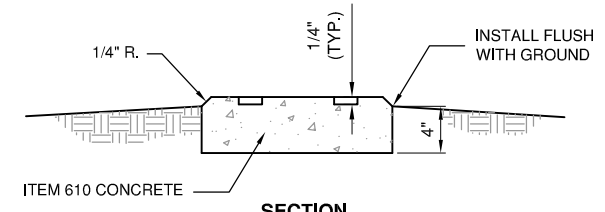


DUCT MARKERS SHALL BE RECESSED AND GROUTED INTO THE PAVEMENTS.

MARKER PLACEMENT



PLAN



SECTION

TURF CABLE / SPLICE MARKER DETAILS
N.T.S.

NOTES

- CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE HOMERUN.
- ITEM 610 CONCRETE SHALL BE USED.
- ALL EXPOSED EDGES SHALL BE EDGED WITH A 1/4" RADIUS TOOL.
- THE COST OF FURNISHING AND INSTALLING NEW MARKERS SHALL BE INCIDENTAL TO THE ASSOCIATED ITEMS.
- 0.049 CU. YD. CONCRETE PER MARKER.
- A MARKER CONFORMING TO THIS DETAIL MARKED "SPLICE" SHALL BE INSTALLED AT ALL SPLICE LOCATIONS NOT IN LIGHT CANS OR MANHOLES.

BL069

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

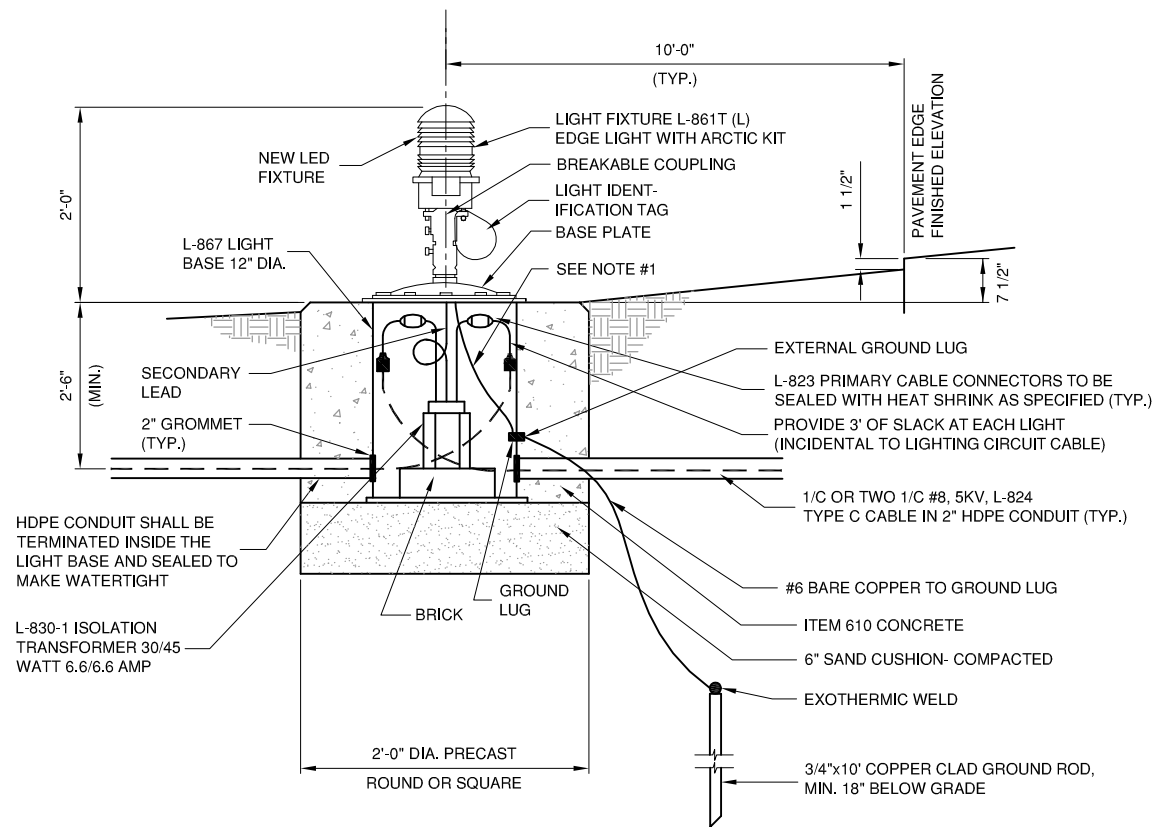
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 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

**EXPAND AIR CARGO APRON
 ELECTRICAL DETAILS 2**

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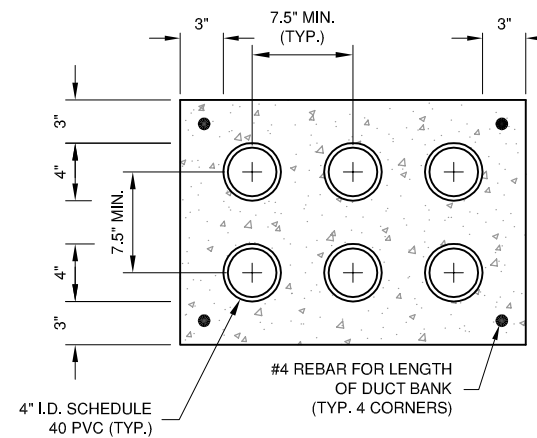
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APPROVED BY:	RLV
DATE:	MARCH 19, 2013
JOB No:	12085-05-00
IL PROJ. NO.	BMI-4288
AIP PROJ. NO.	3-17-0006-XX
SHEET	33 OF 55 SHEETS



BASE MOUNTED EDGE LIGHT
 N.T.S.

NOTES

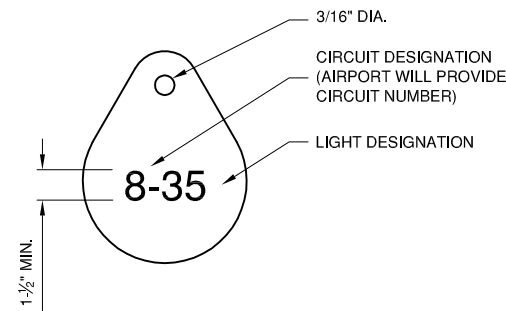
1. THE LIGHT FIXTURE SHALL BE BONDED TO THE LIGHT BASE INTERNAL GROUND LUG VIA A #6 AWG STRANDED COPPER WIRE RATED FOR 600 VOLTS WITH GREEN XHHW INSULATION. THE GROUND WIRE LENGTH SHALL BE SUFFICIENT TO ALLOW THE REMOVAL OF THE LIGHT FIXTURE FROM THE LIGHT BASE FOR ROUTINE MAINTENANCE. SEE THE LIGHT FIXTURE MANUFACTURER'S INSTRUCTIONS FOR PROPER METHODS OF ATTACHING THIS BONDING WIRE.



DUCT BANK DETAIL
 N.T.S.

NOTES

1. DIMENSIONS SHOWN ARE MINIMUM.
2. TOP OF CONCRETE ENCASEMENT TO BE NOT LESS THAN 18" BELOW FINISHED SUBGRADE.
3. DUCT CONCRETE SHALL BE ITEM 610 STRUCTURAL P.C.C. CONCRETE.
4. ALL DUCT SHALL BE 4" INSIDE DIAMETER.
5. LOCATIONS SHOWN ARE APPROXIMATE. DUCT BANKS SHALL BE INSTALLED AT LOCATIONS DESIGNATED BY THE ENGINEER.
6. CONTRACTOR SHALL INSTALL DUCT BANKS AT A DEPTH WHICH WILL NOT CONFLICT WITH ELEVATION SENSITIVE UTILITIES.
7. A PULL WIRE SHALL BE PROVIDED IN EACH UNUSED CONDUIT. UNUSED DUCT SHALL BE SEALED WITH PVC PLUGS/CAPS TO THE SATISFACTION OF THE ENGINEER.
8. DUCT BANKS SHALL EXTEND A MINIMUM OF 5' BEYOND THE NEW EDGE OF PAVEMENT.



LIGHT IDENTIFICATION DETAIL
 N.T.S.

NOTES

1. INSTALL A NONCORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH SET SCREW.
2. LEGENDS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. CONTRACTOR TO COORDINATE LEGEND WITH AIRPORT.
3. THE CONTRACTOR SHALL NUMBER THE EXISTING/ PROPOSED LIGHTS AND SIGNS IN EACH CIRCUIT STARTING AT THE HOMERUN CONTINUING AROUND THE ENTIRE CIRCUIT BACK TO THE HOMERUN.
4. AIRFIELD SIGNS SHALL BE TAGGED & NUMBERED.

BL069

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NUMBER	BY	DATE

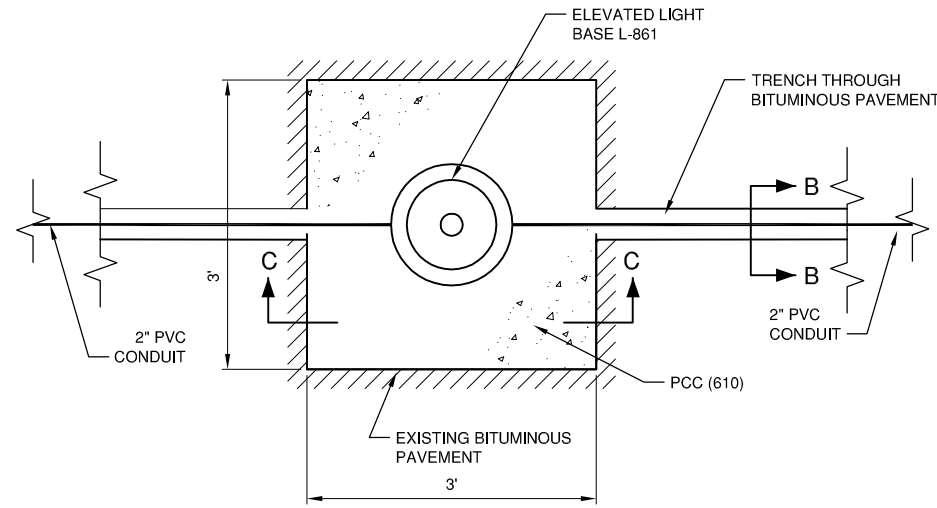
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 AT FULL SCALE (34X22).

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 BLOOMINGTON, ILLINOIS**

**EXPAND AIR CARGO APRON
 ELECTRICAL DETAILS 3**

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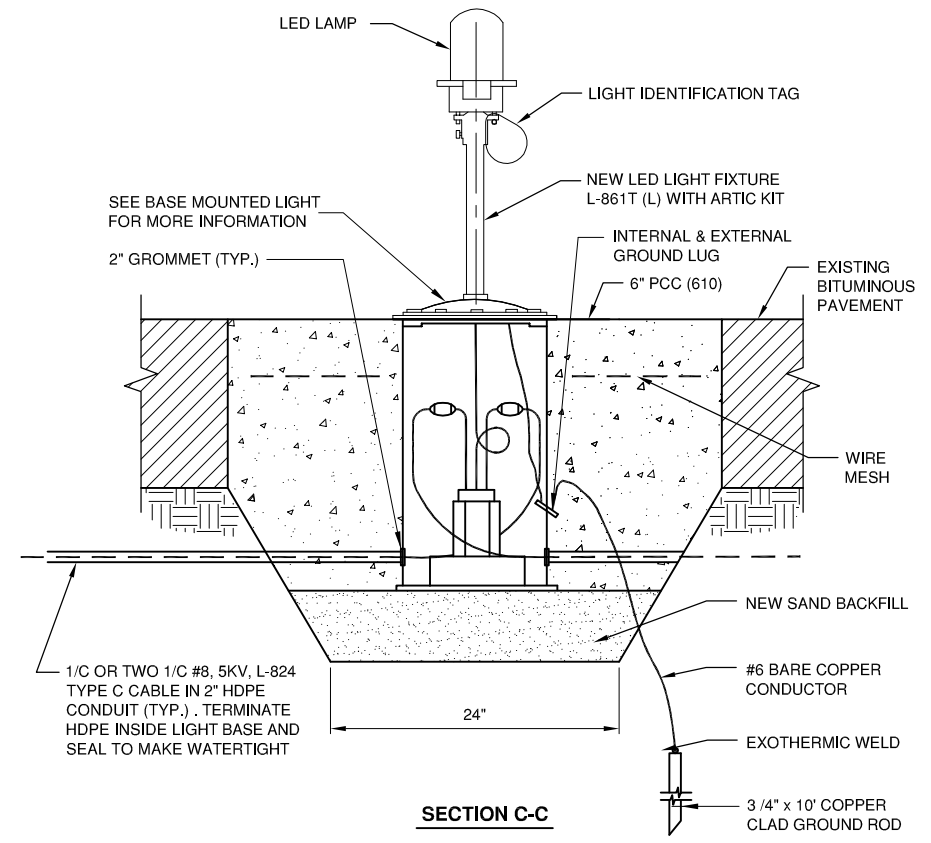
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DRAWN BY:	CMT
CHECKED BY:	WDP
APPROVED BY:	RLV
DATE:	MARCH 19, 2013
JOB No:	12085-05-00
IL PROJ. NO.	BMI-4288
AIP PROJ. NO.	3-17-0006-XX
SHEET	34 OF 55 SHEETS



**PLAN
 EXTENDED PAD**

**ELEVATED BASE MOUNTED LIGHT
 IN BITUMINOUS PAVEMENT**

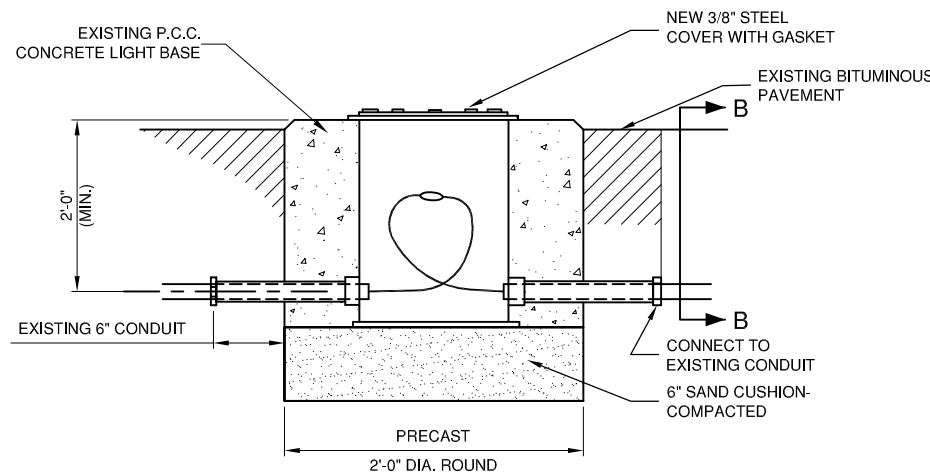
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SECTION C-C

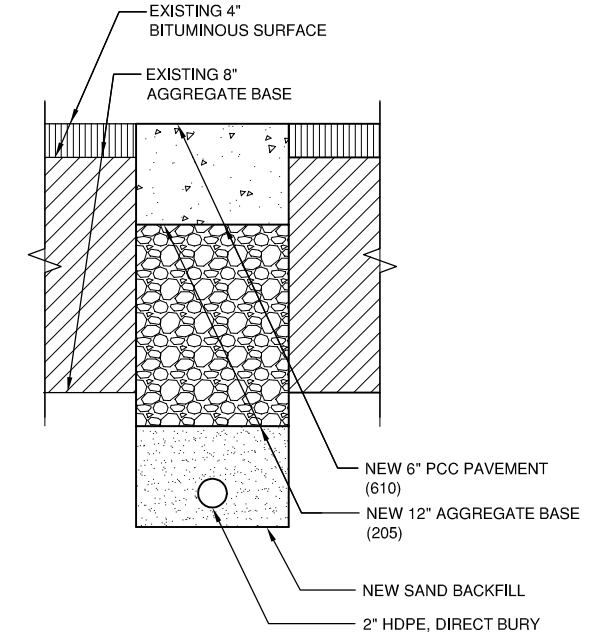
**ELEVATED LIGHT BASE IN
 EXISTING PAVEMENT DETAIL**

N.T.S.



**EXISTING LIGHT BASE CONVERTED
 TO SPLICE CAN DETAIL**

N.T.S.



SECTION B-B

CABLE TRENCH DETAIL IN PAVEMENT

N.T.S.

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NUMBER	BY	DATE

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 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

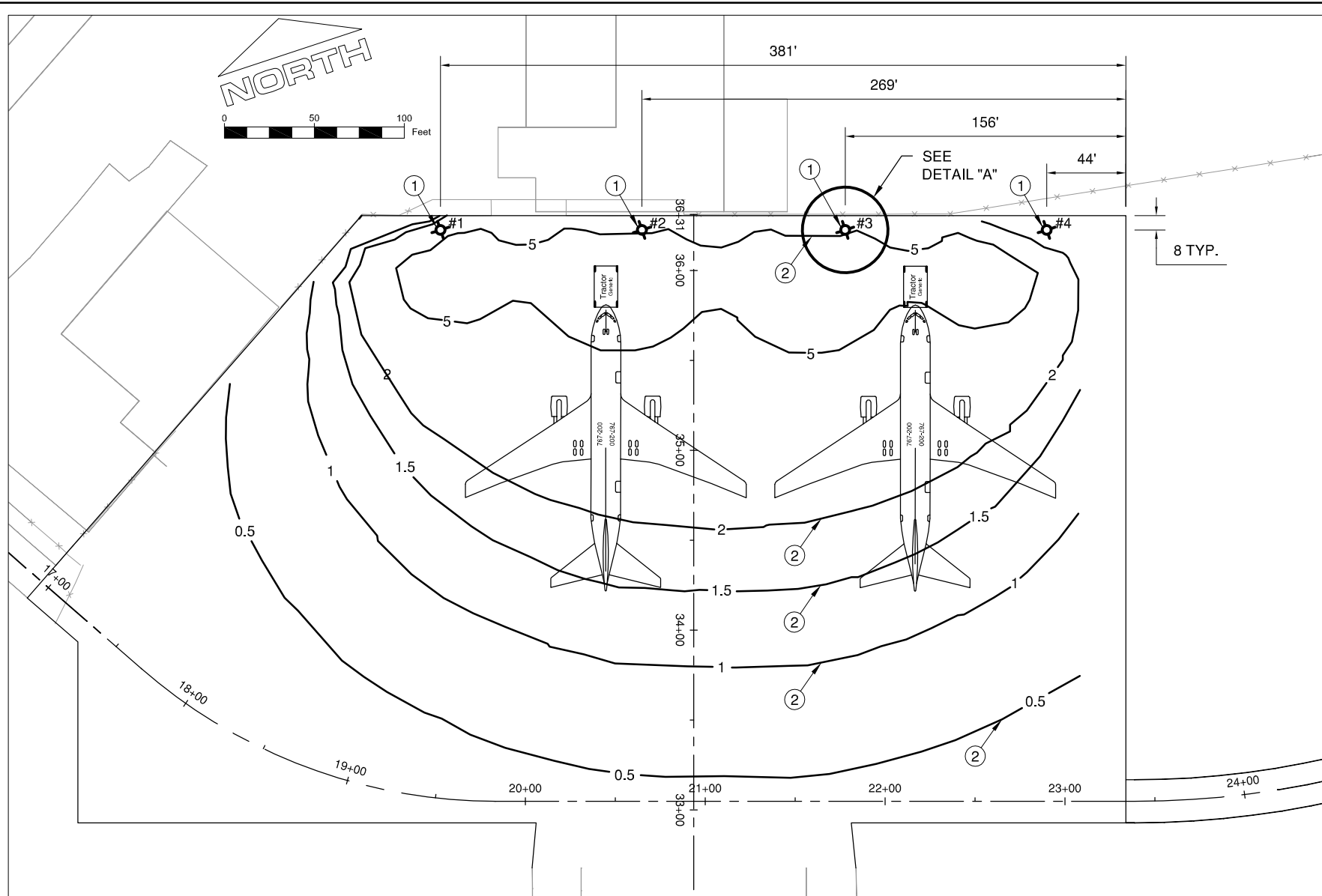
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BLOOMINGTON, ILLINOIS

EXPAND AIR CARGO APRON
APRON LIGHTING PLAN

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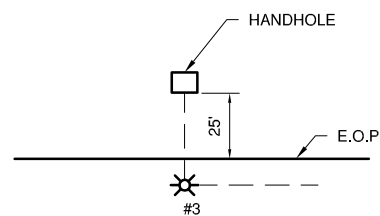
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DRAWN BY:	CMT
CHECKED BY:	CBG
APPROVED BY:	RLV
DATE:	MARCH 19, 2013
JOB No:	12085-05-00
IL PROJ. NO.	BMI-4288
AIP PROJ. NO.	3-17-0006-XX
SHEET	35 OF 55 SHEETS



PLAN VIEW

PLAN VIEW NOTES

- APRON LIGHT FIXTURE. LOCATE WHERE INDICATED OR AS DIRECTED BY THE RESIDENT ENGINEER IN THE FIELD. FIXTURE LOCATED 8' FROM EDGE OF APRON.
- APRON LIGHTING PHOTOMETRIC ILLUMINATION CONTOURS, IN FOOTCANDLES. CONTRACTOR SHALL ADJUST EACH LUMINAIRE AIMING POINT AS NECESSARY TO ATTAIN THESE VALUES AS CLOSELY AS POSSIBLE AS MEASURED ON THE APRON PAVEMENT. SEE APRON LIGHT HORIZONTAL LIGHTING DETAIL FOR ADDITIONAL INFORMATION.



DETAIL "A"
 N.T.S.

LIGHT FIXTURE SCHEDULE

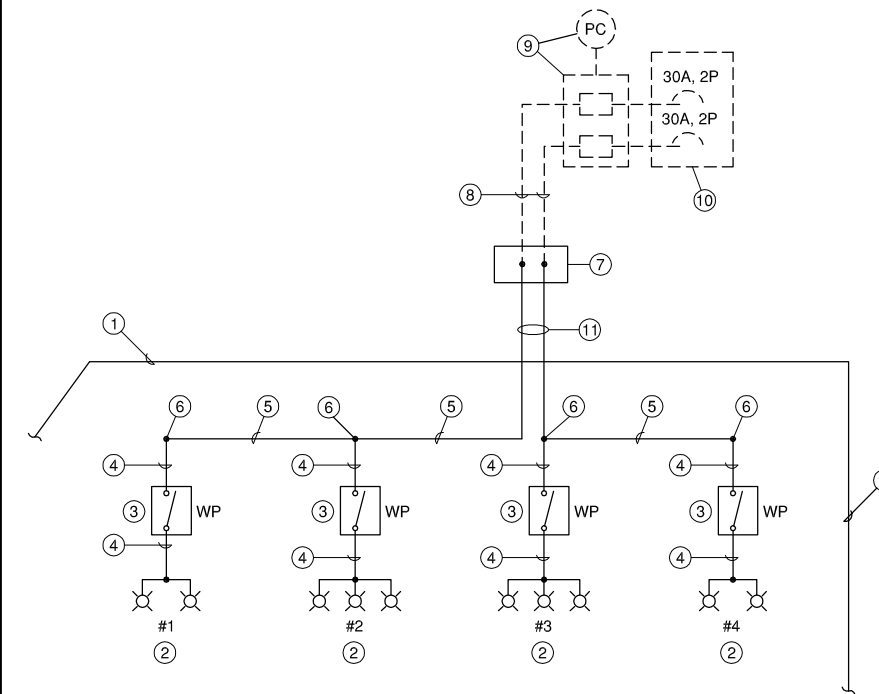
FIXTURE NO.	MANUFACTURER OR EQUIVALENT	MAKE OR MODEL #	LAMP
#1 & #4	STERNER	876/1000MH/4L/240/C/YK/120/SF/AV/N/BZ (TWO LUMINAIRES REQUIRED)	1000W METAL HALIDE
#2 & #3	STERNER	876/1000MH/4L/240/C/YK/130/SF/AV/N/BZ (THREE LUMINAIRES REQUIRED)	1000W METAL HALIDE

LIGHT FIXTURES ARE SHIPPED "PRE-AIMED". CONTRACTOR SHALL INITIALLY ADJUST AIMING NOMINALLY AN ADDITIONAL 10 DEGREES "UP". CONTRACTOR SHALL ADJUST LIGHT FIXTURES TO OBTAIN PHOTOMETRIC CONTOURS AS SHOWN ON THE PLANS, OR AS DIRECTED BY RESIDENT ENGINEER IN THE FIELD.

POLE SCHEDULE

POLE HEIGHT	MANUFACTURER OR EQUIVALENT	MAKE OR MODEL #	TYPE
39'	VALMONT	R/380860108T4/P4/DDB (INCLUDES VIBRATION DAMPER)	ROUND TAPERED ALUMINUM

NOTES: POLES SHALL BE CERTIFIED FOR 90 MPH WIND SPEED WITH 1.3 GUST FACTOR AND A TOTAL LUMINAIRE EPA OF 14.9 SQ. FT.

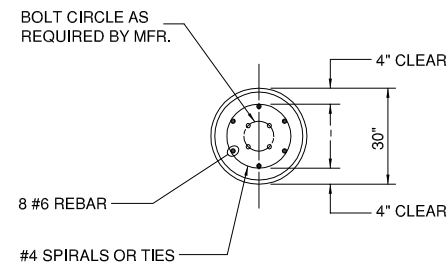


APRON LIGHTING WIRING

N.T.S.

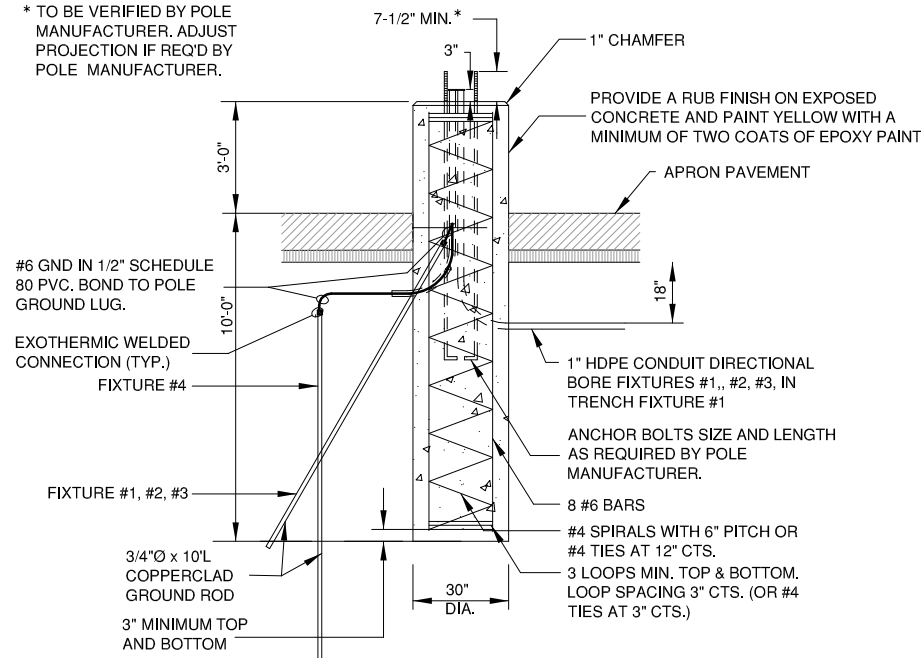
APRON LIGHTING WIRING NOTES

- EDGE OF EXISTING AND PROPOSED APRON.
- APRON LIGHT FIXTURE WITH NUMBER OF LUMINAIRES AS SHOWN.
- WEATHERPROOF TOGGLE SWITCH, 20A, 2-POLE, MOUNTED ON LIGHT POLE HANDHOLE COVER.
- #12 THWN WIRING INSIDE LIGHT POLE. SEE FIXTURE WIRING DETAIL FOR ADDITIONAL INFORMATION.
- TWO #6 USE, ONE #10 GROUND IN 1" HDPE CONDUIT. INSTALL FROM LIGHT FIXTURE TO LIGHT FIXTURE VIA DIRECTIONAL BORE UNDERNEATH EXISTING PAVEMENT AND IN TRENCH FROM EDGE OF EXISTING PAVEMENT TO LIGHT FIXTURE #4.
- SPLICE INSIDE LIGHT POLE. SEE FIXTURE WIRING DETAIL FOR ADDITIONAL INFORMATION.
- NEW POLYMER CONCRETE HANDHOLE, OPEN-BOTTOM DESIGN, QUAZITE/HUBBELL #PG-2436-BA-42 WITH #PG-2436-HH-00-17 COVER, LABELED "ELECTRICAL", OR EQUIVALENT.
- FUTURE WIRING BY OTHERS (N.I.C.).
- FUTURE LIGHTING CONTROLLER AND PHOTO CELL BY OTHERS (N.I.C.).
- FUTURE CIRCUIT BREAKERS IN PANELBOARD BY OTHERS (N.I.C.).
- TWO #6 USE, ONE #10 GROUND (FIXTURES #1 & #2 POWER), TWO #6 USE, ONE #10 GROUND (FIXTURES #3 & #4 POWER), IN 1" PVC CONDUIT FROM FIXTURE #3 TO HANDHOLE. INSTALL CONDUIT IN TRENCH FROM HANDHOLE TO EDGE OF EXISTING PAVEMENT AND PUSH CONDUIT FROM EDGE OF EXISTING PAVEMENT TO LIGHT FIXTURE #3.



TOP VIEW

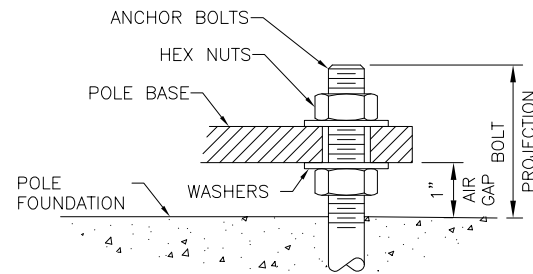
* TO BE VERIFIED BY POLE MANUFACTURER. ADJUST PROJECTION IF REQ'D BY POLE MANUFACTURER.



ELEVATION

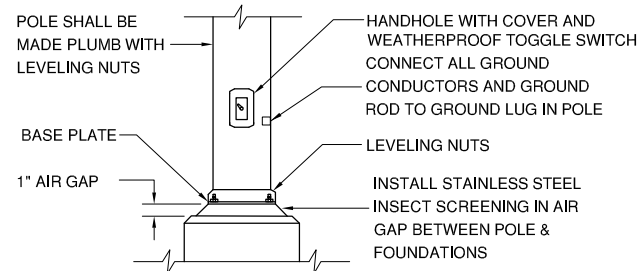
APRON LIGHT POLE FOUNDATION DETAIL (FIXTURE #1 SHOWN)

N.T.S.



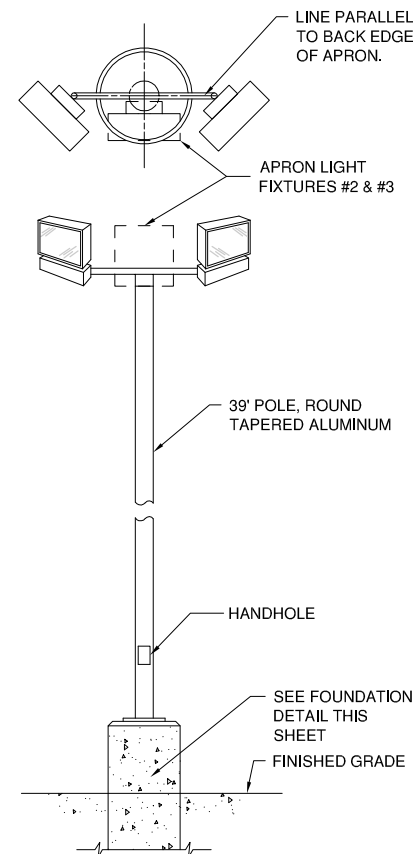
POLE BASE AIR GAP

N.T.S.



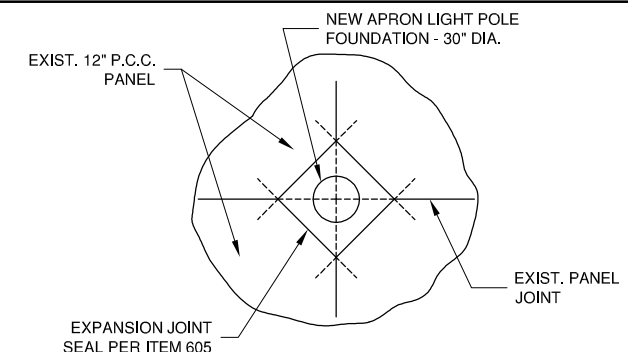
APRON LIGHT POLE DETAIL

N.T.S.

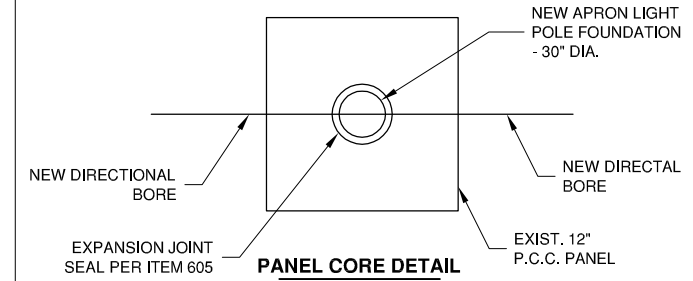


APRON LIGHT STANDARD DETAIL

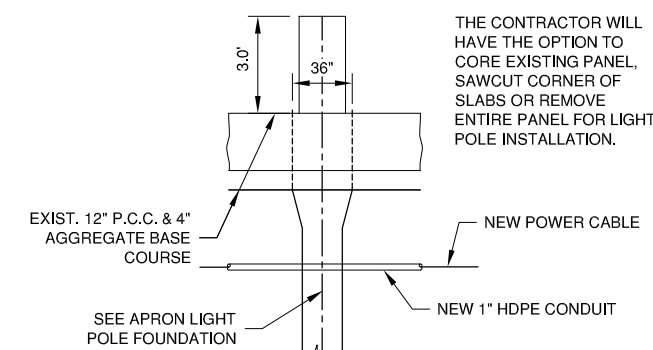
N.T.S.



CORNER SLAB DETAIL LIGHT



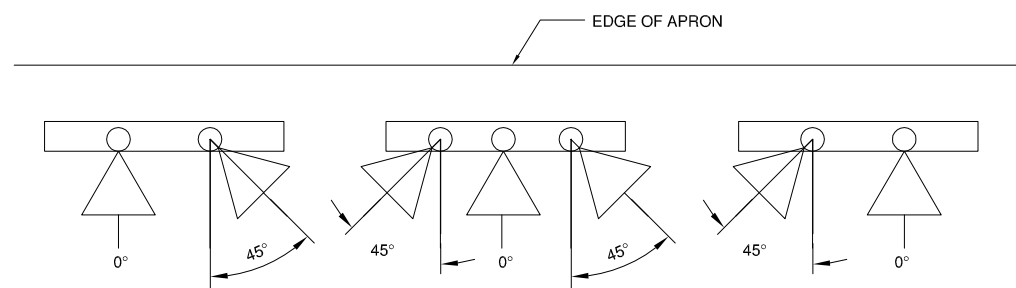
PANEL CORE DETAIL



ELEVATION VIEW

APRON LIGHT INSTALL IN EXISTING PAVEMENT DETAIL

N.T.S.



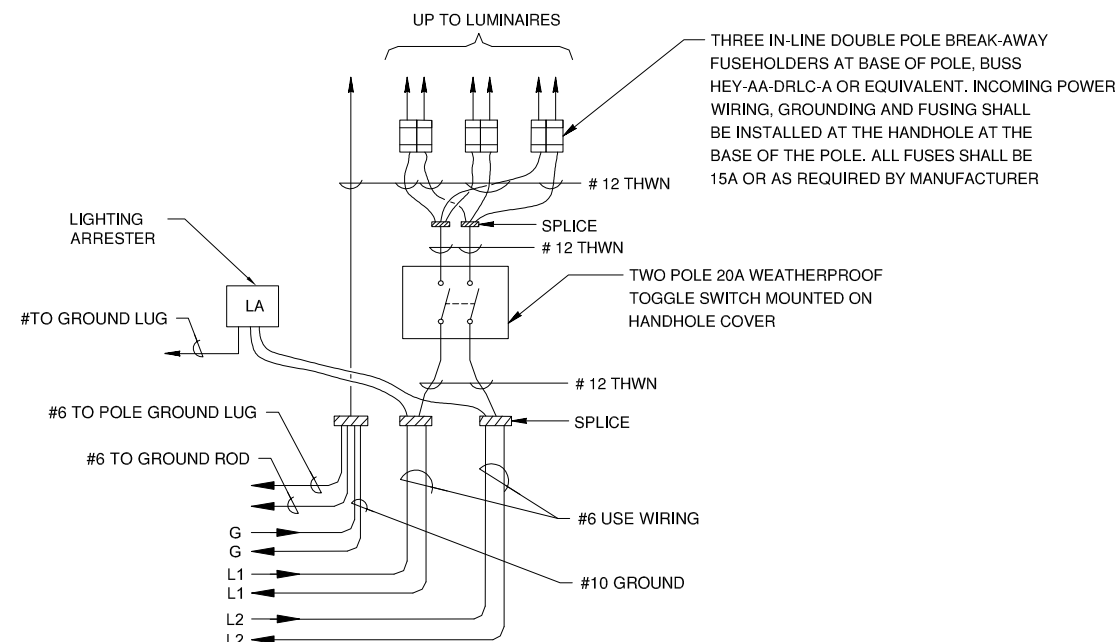
FIXTURE #1

FIXTURE #2 & #3

FIXTURE #4

APRON LIGHT DETAILS (NOMINAL, TO BE ADJUSTED AS NEEDED)

N.T.S.

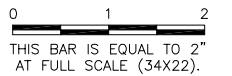


FIXTURE WIRING DETAIL (TRIPLE LUMINAIRE SHOWN)

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BLOOMINGTON-NORMAL AIRPORT AUTHORITY
CENTRAL ILLINOIS REGIONAL AIRPORT
BLOOMINGTON, ILLINOIS

EXPAND AIR CARGO APRON
APRON LIGHTING DETAILS

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CHECKED BY: CBG

APPROVED BY: RLV

DATE: MARCH 19, 2013

JOB No: 12085-05-00

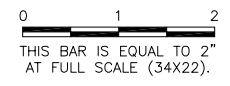
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AIP PROJ. NO. 3-17-0006-XX

SHEET 36 OF 55 SHEETS

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NUMBER	BY	DATE



**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
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 BLOOMINGTON, ILLINOIS**

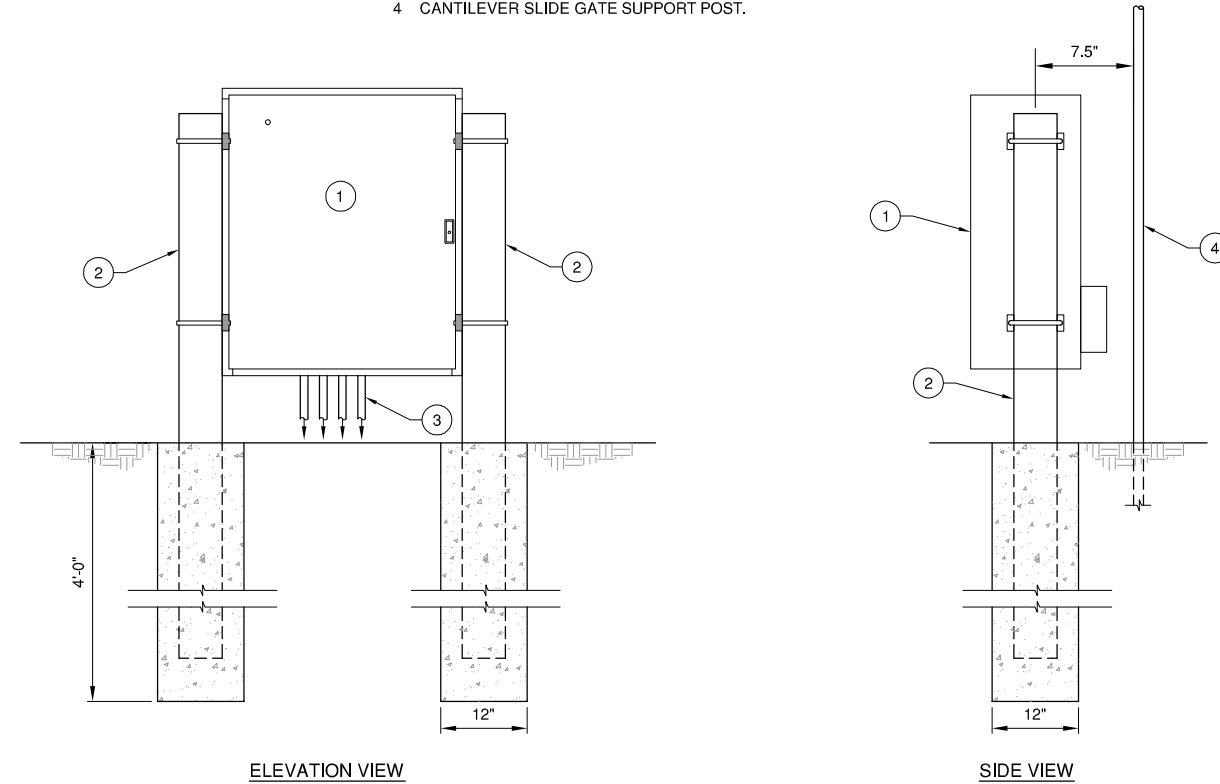
**EXPAND AIR CARGO APRON
 ELECTRIC GATE DETAILS 1**

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JOB No:	12085-05-00
IL PROJ. NO.	BMI-4288
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SHEET	37 OF 55 SHEETS

RELOCATED GATE OPERATOR KEYED NOTES

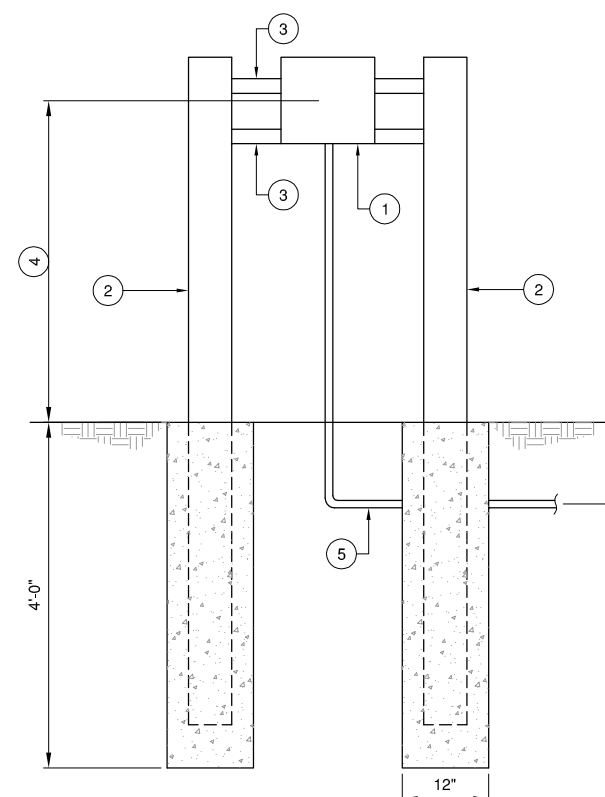
- 1 RELOCATED GATE OPERATOR.
- 2 GALVANIZED STEEL SUPPORT POSTS EMBEDDED IN 12" DIAMETER X 48" DEEP CONCRETE FOUNDATIONS. PAINT POSTS TO MATCH OPERATOR WITH MINIMUM OF TWO COATS EPOXY PAINT.
- 3 CONDUITS AND WIRING AS NEEDED TO MATCH EXISTING.
- 4 CANTILEVER SLIDE GATE SUPPORT POST.



RELOCATED GATE OPERATOR
 N.T.S.

RELOCATED CARD READER NOTES

- 1 RELOCATED INDALA WEATHERPROOF LONG RANGE CARD READER.
- 2 6" DIAMETER, CONCRETE FILLED, GALVANIZED STEEL SUPPORT POST, PAINTED YELLOW, EMBEDDED IN 12" DIAMETER X 48" DEEP CONCRETE FOUNDATION.
- 3 2" X 1" STEEL C-CHANNEL WELDED TO 6" PIPE AND PAINTED YELLOW.
- 4 MATCH HEIGHT OF EXISTING INSTALLATION.
- 5 RELOCATED 6-CONDUCTOR #18 SHIELDED CABLE IN NEW 1" CONDUIT.



RELOCATED CARD READER INSTALLATION DETAIL
 N.T.S.

GATE RELOCATION PLAN KEYED NOTES

- 1 EXISTING GATE OPERATOR TO BE RELOCATED.
- 2 EXISTING CARD READER TO BE RELOCATED.
- 3 EXISTING HANDHOLE.
- 4 EXISTING CARD READER TO REMAIN.
- 5 RELOCATED GATE OPERATOR. MOUNT TO STEEL POSTS EMBEDDED 4'-0" IN CONCRETE FOUNDATION. INSTALLATION TO MATCH EXISTING.
- 6 RE-ROUTE EXISTING TWO #6 USE (GATE POWER) ONE #10 GROUND AND 2/C #14 SHIELDED CABLE (GATE OPEN CONTROL) IN NEW 1" CONDUIT FROM EXISTING HANDHOLE TO RELOCATED GATE OPERATOR.
- 7 RELOCATED CARD READER. SEE DETAIL THIS SHEET.
- 8 INTERCEPT EXISTING CONDUIT AND RE-ROUTE EXISTING 6/C SHIELDED CABLE IN NEW 1" CONDUIT TO RELOCATED CARD READER.
- 9 NEW DETECTOR LOOPS, SEE DETAILS AND SPECIFICATIONS.
- 10 EXISTING AREA LIGHT TO REMAIN.
- 11 RELOCATED CANTILEVER SLIDE GATE.

RELOCATED GATE INSTALLATION SHALL PROJECT FROM EXISTING FENCELINE AND BE PERPENDICULAR TO THE PERIMETER ROAD

PERIMETER ROADWAY CENTERLINE

EXISTING GATE RELOCATION PLAN
 SCALE 1" = 10'

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NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
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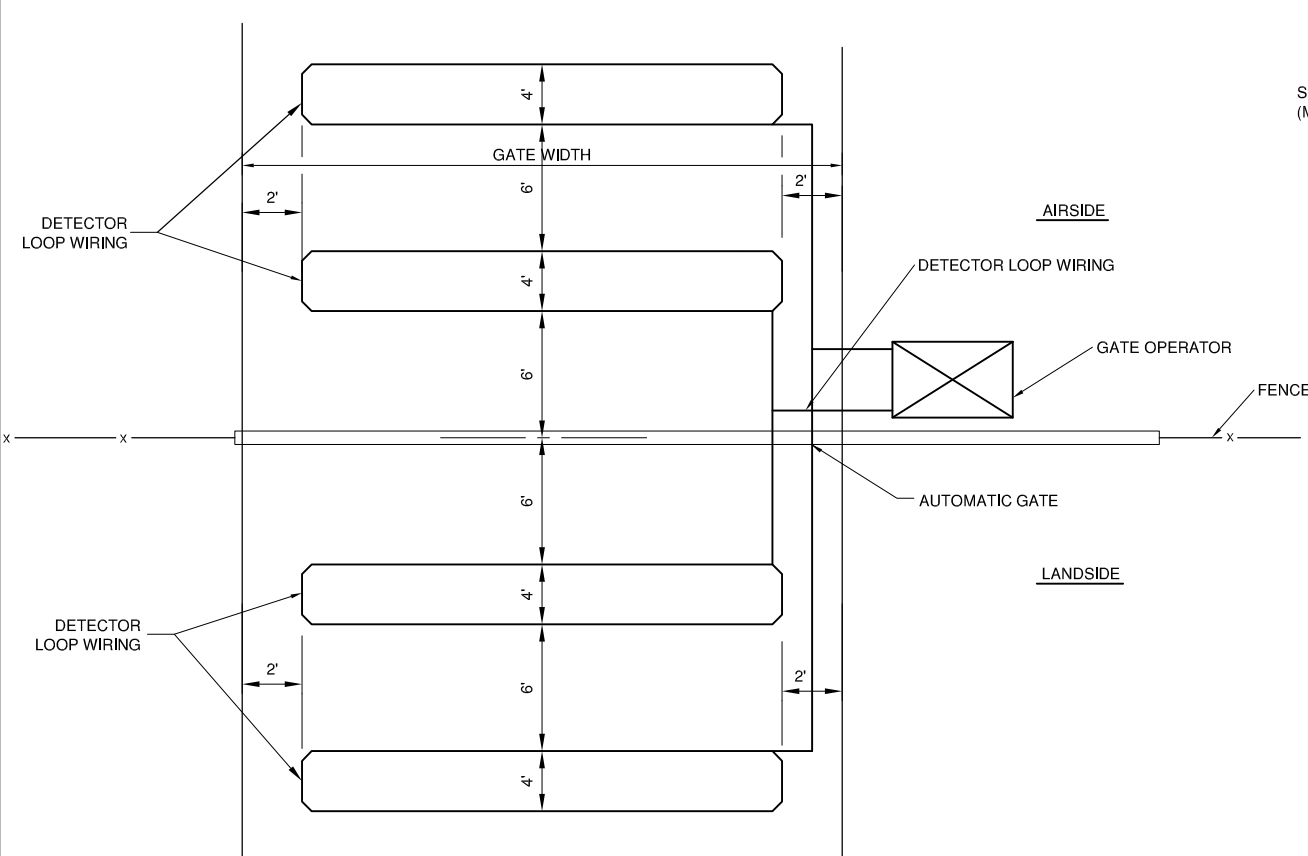
**EXPAND AIR CARGO APRON
 ELECTRIC GATE DETAILS 2**

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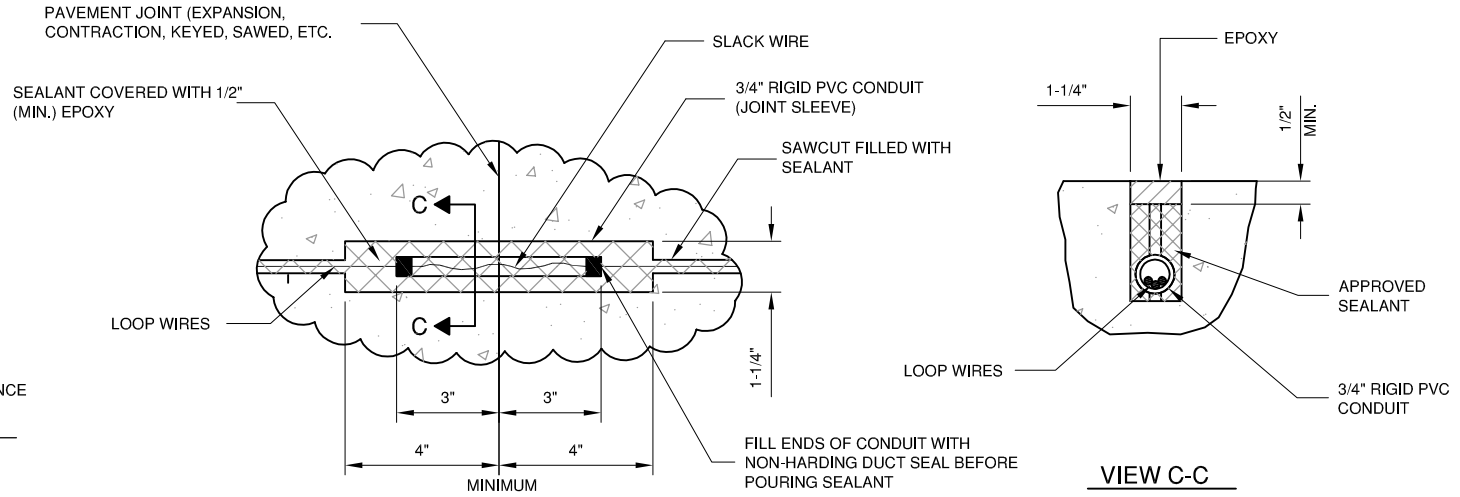
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JOB No:	12085-05-00
IL PROJ. NO.	BMI-4288
AIP PROJ. NO.	3-17-0006-XX
SHEET	38 OF 55 SHEETS



TYP. GATE PLAN
 N.T.S.



JOINT CROSSING DETAIL
 N.T.S.

NOTES

1. LOOP LEADS ARE LIMITED TO 100 FEET.
2. LOOP LEADS MUST HAVE FOUR (4) TWISTS PER FOOT.
3. LOOP AND LOOP LEADS MUST BE LOCATED, AT LEAST, 18" FROM ANY ELECTRICAL POWER SERVICE OR STEEL REINFORCEMENT.
4. LOOP LEADS MUST BE IN SEPARATE CONDUIT BETWEEN LOOP AND DETECTOR. THEY MUST NOT SHARE CONDUIT WITH OTHER WIRING OR LEADS FROM OTHER LOOPS.
5. WIRE SHALL BE #16 XHHW SINGLE CONDUCTOR STRANDED WIRE.
6. ALL WIRE SHALL BE CONTINUOUS WITHOUT SPLICING.

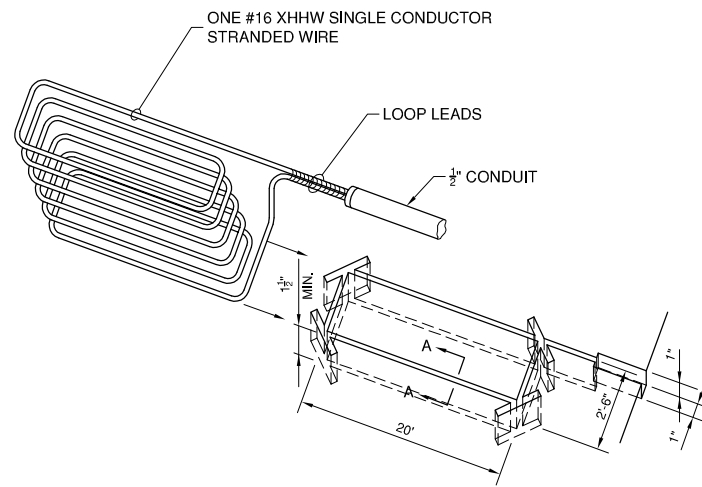
CAUTION:

- DO NOT SPLICE WIRE.
- DO NOT FRACTURE WIRE INSULATION. LOOPS SHORTED TO GROUND WILL CAUSE DETECTOR MALFUNCTION. WHEN PLACING WIRE IN THE SLOT, DO NOT USE SCREWDRIVER OR OTHER SHARP TOOLS.

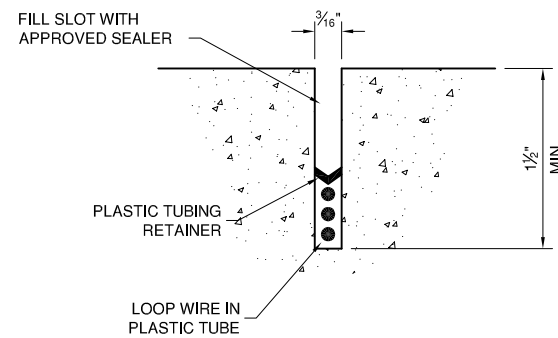
TYPICAL LAYOUT FOR LOOP:

- SAW SLOT 3/16" WIDE x 1-1/2" DEEP. MAKE RECTANGULAR SHAPE TO SPECIFIED LOOP DIMENSIONS PLUS SLOT FOR LEAD CONDUIT.
- GROUT WITH NO. 202 WEATHERBAN SEALANT (A PRODUCT OF 3M CO.) OR APPROVED EQUIVALENT ... (EXAMPLE: DE WITTS NO. 99 BLACK MASTIC CAULK).

LOCATIONS, DETAILS AND CHARACTER OF EQUIPMENT SHOWN ON THIS SHEET ARE GENERIC. EQUIPMENT LOCATION SHALL BE AS RECOMMENDED BY THE EQUIPMENT MANUFACTURER.



PERSPECTIVE VIEW OF DETECTOR LOOP SAW SLOT

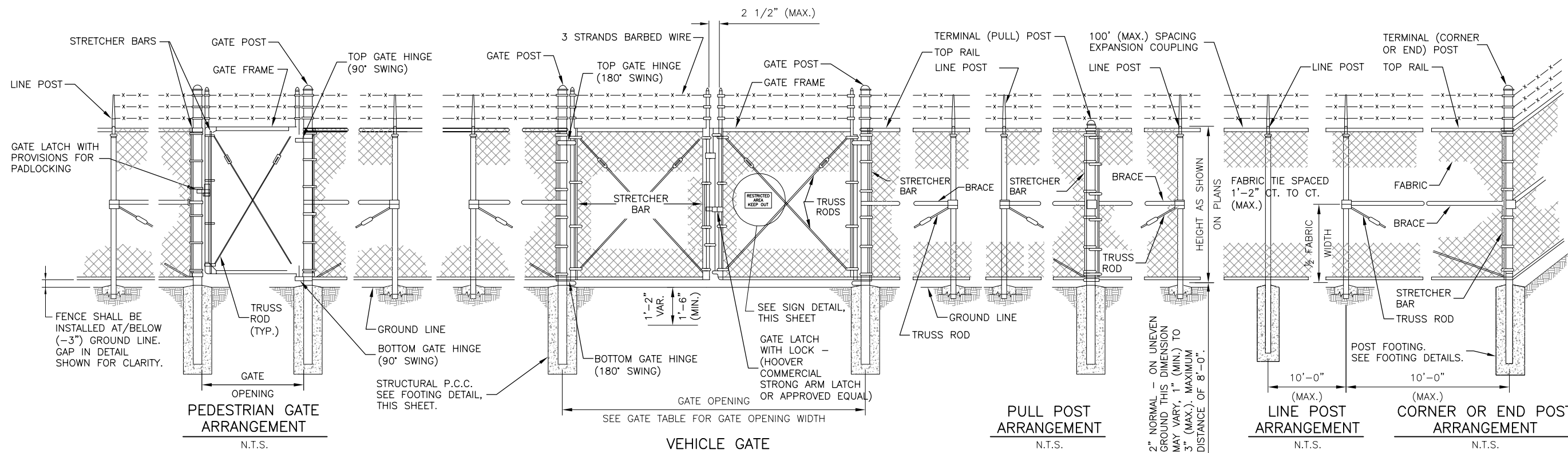
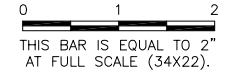


SECTION A-A

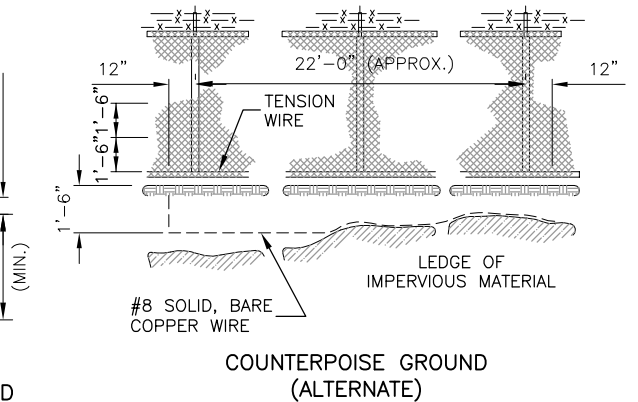
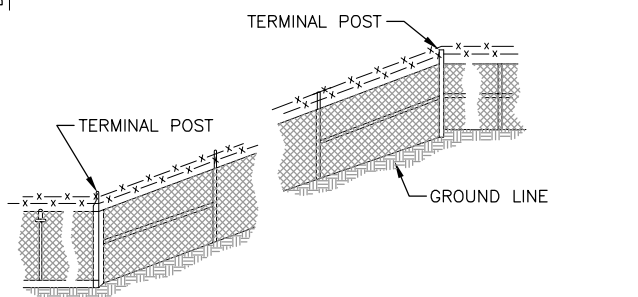
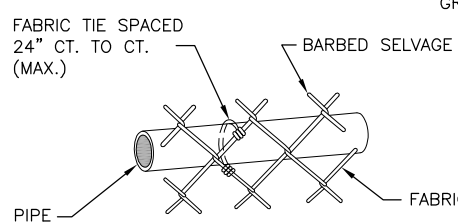
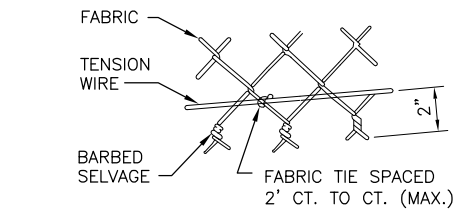
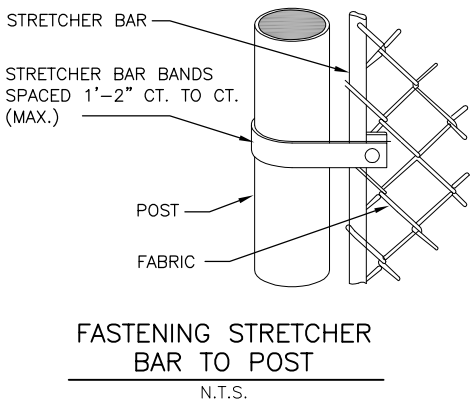
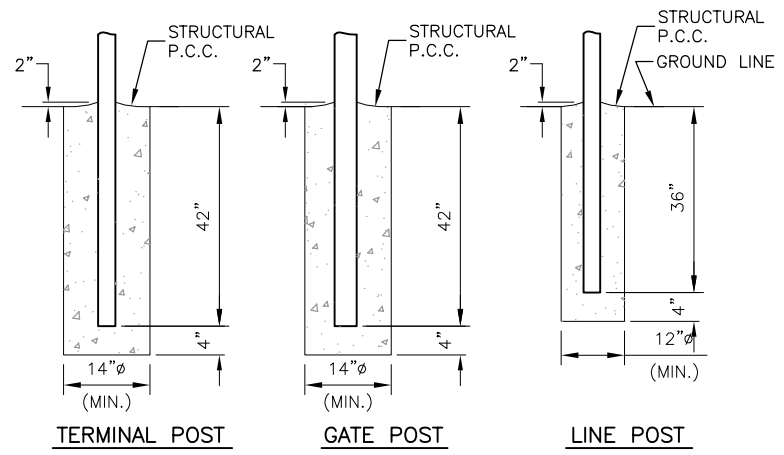
DETECTOR LOOP DETAILS
 N.T.S.

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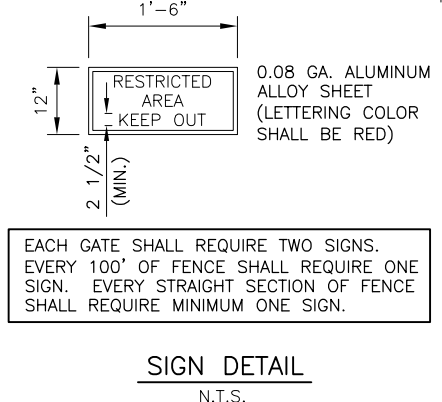
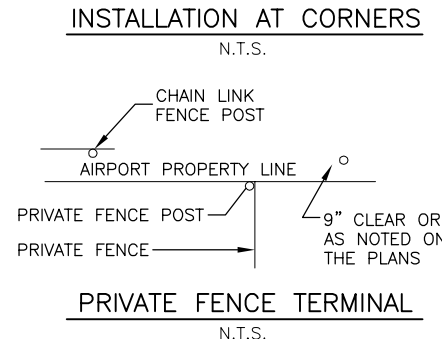
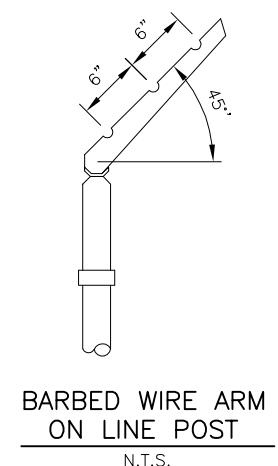


PULL POSTS SHALL BE PLACED AT LOCATIONS DETERMINED BY THE ENGINEER. THEY SHALL BE PLACED AT 660 FT. INTERVALS BETWEEN POSTS TO WHICH THE ENDS OF THE FABRIC ARE CLAMPED OR MIDWAY BETWEEN SUCH POSTS WHEN THE DISTANCE IS LESS THAN 1320 FT. AND GREATER THAN 660 FT.



PROTECTIVE ELECTRICAL GROUND DETAILS
N.T.S.

WHERE FENCE LINE HAS A CHANGE IN DIRECTION OF 15° OR MORE, A TERMINAL POST SHALL BE PLACED AS SHOWN ABOVE. WHERE ANGLE IS LESS THAN 15° AND EXISTING CONDITIONS REQUIRE A TERMINAL POST, THEY SHALL BE PLACED AS DIRECTED BY THE ENGINEER.



- NOTES**
- CONTINUOUS FENCE SHALL BE GROUNDED AT INTERVALS NOT EXCEEDING 1000 FT. EXCEPT THERE SHALL BE A GROUND NOT EXCEEDING 100 FT. FROM GATE IN EACH SECTION OF THE FENCE ADJACENT TO THE GATE.
 - FENCE UNDER POWER LINES SHALL BE GROUNDED BY 3 GROUNDS, ONE DIRECTLY UNDER THE CROSSING AND ONE ON EACH SIDE 25 TO 50 FT. AWAY. A SINGLE GROUND SHALL BE LOCATED DIRECTLY UNDER EACH TELEPHONE WIRE OR CABLE CROSSINGS.
 - THE COUNTERPOISE SHALL BE USED ONLY WHERE IT IS IMPOSSIBLE TO DRIVE A GROUND ROD BECAUSE OF IMPERVIOUS EARTH STRUCTURES.
 - THE GROUND WIRE SHALL BE CONNECTED TO THE FABRIC AND THE GROUND ROD BY A MECHANICAL CLAMP OF CAST BRONZE BODY AND BRONZE OR STAINLESS STEEL BOLTS AND WASHERS. WHEN A TENSION WIRE IS REQUIRED, THE BOTTOM CONNECTION OF THE GROUND WIRE SHALL BE MADE TO THE TENSION WIRE.
 - ALL PROPOSED CLASS E FENCE SHALL MEET THE REQUIREMENTS OF IDOT STANDARD 664001-01.

BLOOMINGTON-NORMAL AIRPORT AUTHORITY
CENTRAL ILLINOIS REGIONAL AIRPORT
BLOOMINGTON, ILLINOIS
EXPAND AIR CARGO APRON
FENCE DETAILS 1

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 APPROVED BY: RLV
 DATE: MARCH 19, 2013
 JOB No: 12085-05-00
 I.L. PROJ. NO. BMI-4288
 AIP PROJ. NO. 3-17-0006-XX
 SHEET 39 OF 55 SHEETS

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NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

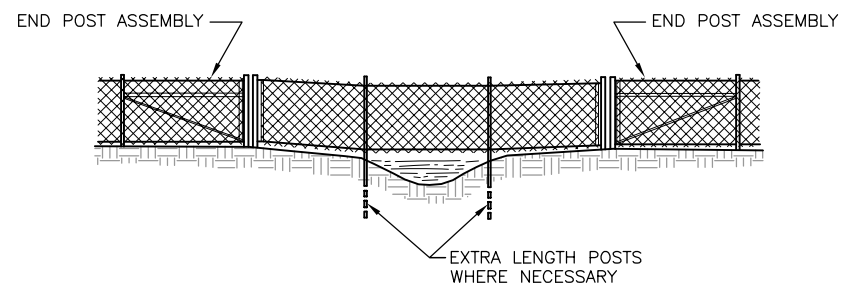
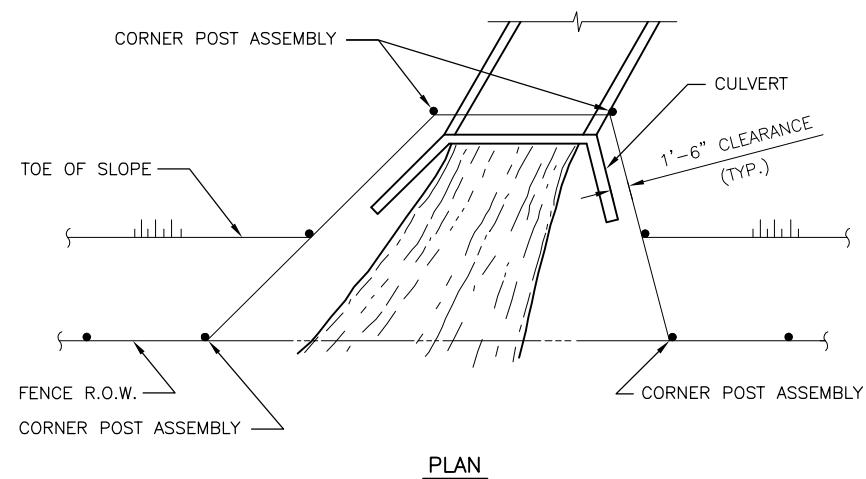
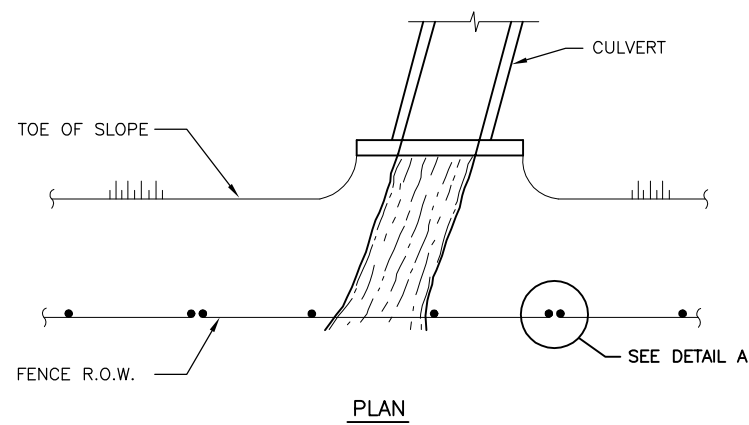
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**EXPAND AIR CARGO APRON
 FENCE DETAILS 2**

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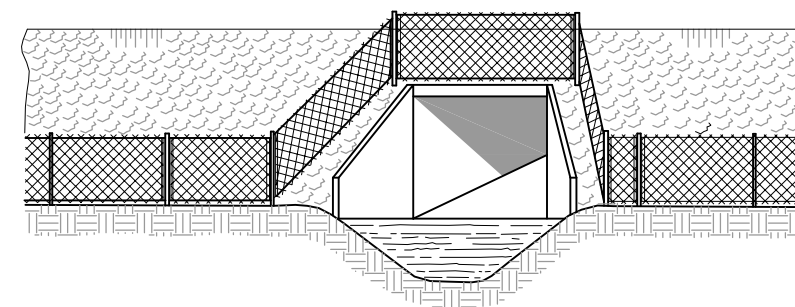


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SHEET	40 OF 55 SHEETS



THE CHAIN LINK FABRIC SHALL BE REPLACED BY BARBED WIRE STRANDS AT 12" MAXIMUM CENTERS BETWEEN THE DOUBLE POSTS SHOWN ON DETAIL A WHEN SHOWN ON THE PLANS.

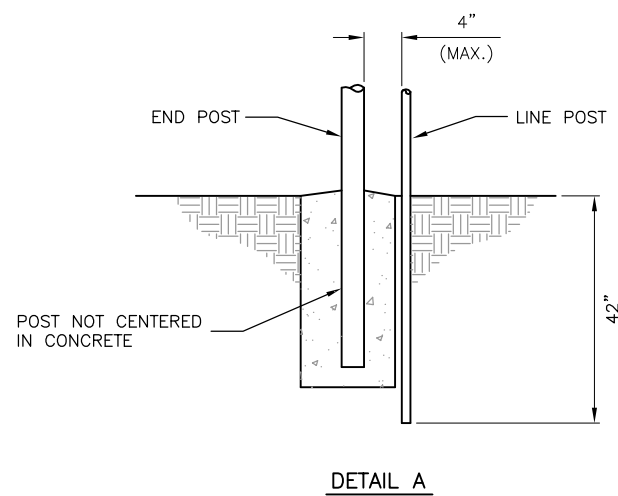
ELEVATION



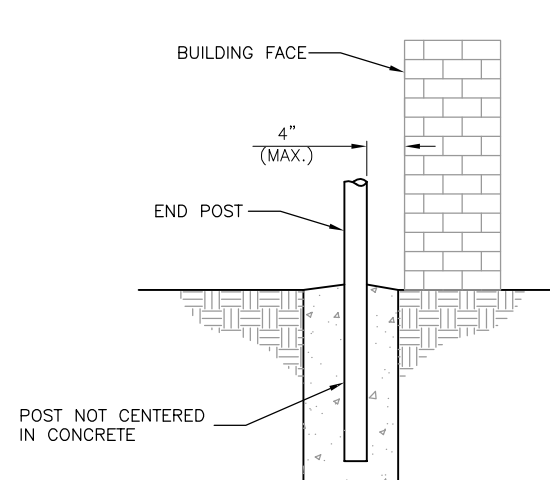
WHEN THE WIDTH OF THE CULVERT MAKES IT NECESSARY TO ANCHOR A POST TO THE TOP OF THE CULVERT, A CAST IRON SHOE OR OTHER DEVICE APPROVED BY THE ENGINEER SHALL BE USED.

ELEVATION

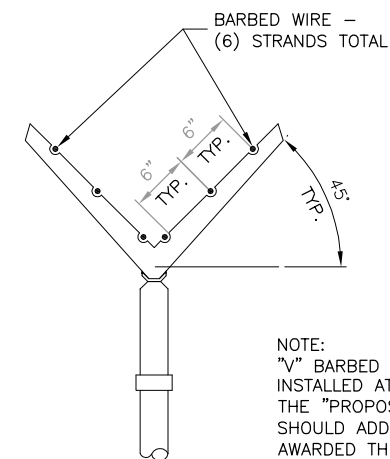
FENCE INSTALLATION AROUND HEADWALL DETAILS
 N.T.S.



FENCE INSTALLATION OVER STREAM DETAILS
 N.T.S.



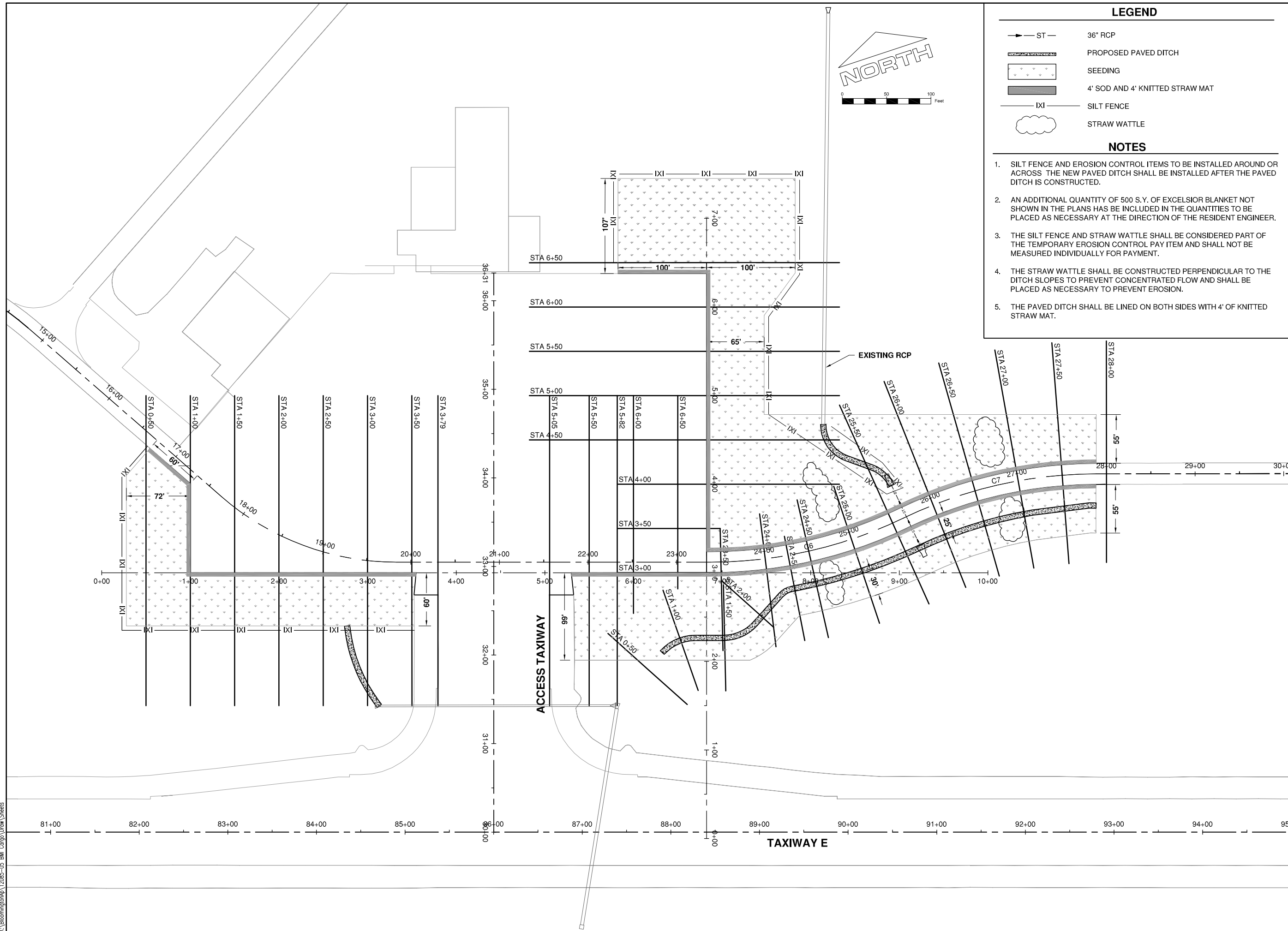
FENCE INSTALLATION AT BUILDING FACE
 N.T.S.



NOTE:
 "V" BARBED WIRE ARMS SHALL BE INSTALLED AT THE LOCATIONS SHOWN ON THE "PROPOSED IMPROVEMENTS" SHEETS. SHOULD ADDITIVE ALTERNATE #1 BE AWARDED THE "V" BARBED WIRE ARMS SHALL BE BLACK VINYL COATED.

"V" BARBED WIRE ARM ON LINE POST
 N.T.S.

K:\Bloomington\12085-05_BMI_Corp\Draw\Sheets



LEGEND

- ST — 36" RCP
- PROPOSED PAVED DITCH
- SEEDING
- 4' SOD AND 4' KNITTED STRAW MAT
- IXI — SILT FENCE
- STRAW WATTLE

NOTES

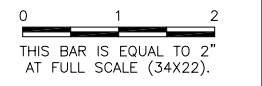
1. SILT FENCE AND EROSION CONTROL ITEMS TO BE INSTALLED AROUND OR ACROSS THE NEW PAVED DITCH SHALL BE INSTALLED AFTER THE PAVED DITCH IS CONSTRUCTED.
2. AN ADDITIONAL QUANTITY OF 500 S.Y. OF EXCELSIOR BLANKET NOT SHOWN IN THE PLANS HAS BE INCLUDED IN THE QUANTITIES TO BE PLACED AS NECESSARY AT THE DIRECTION OF THE RESIDENT ENGINEER.
3. THE SILT FENCE AND STRAW WATTLE SHALL BE CONSIDERED PART OF THE TEMPORARY EROSION CONTROL PAY ITEM AND SHALL NOT BE MEASURED INDIVIDUALLY FOR PAYMENT.
4. THE STRAW WATTLE SHALL BE CONSTRUCTED PERPENDICULAR TO THE DITCH SLOPES TO PREVENT CONCENTRATED FLOW AND SHALL BE PLACED AS NECESSARY TO PREVENT EROSION.
5. THE PAVED DITCH SHALL BE LINED ON BOTH SIDES WITH 4' OF KNITTED STRAW MAT.

FILE: 1208505-CG104.dwg
 UPDATE BY: Chris Groth
 PLOT DATE: 4/4/2013 2:09 PM
 1208505-C-SPAL
 1208505-V-VF3D
 BMI-V-AERL
 BMI-V-SPXP
 1208505-CP701
 1208505-C-UP

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**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

**EXPAND AIR CARGO APRON
 INDEX TO CROSS SECTIONS &
 EROSION CONTROL**

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APPROVED BY:	RLV
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JOB No:	12085-05-00
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SHEET	41 OF 55 SHEETS

BL069

REVISIONS		
NUMBER	BY	DATE

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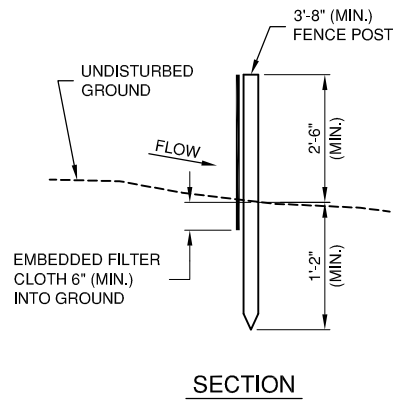
BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS

EXPAND AIR CARGO APRON
 EROSION CONTROL DETAILS

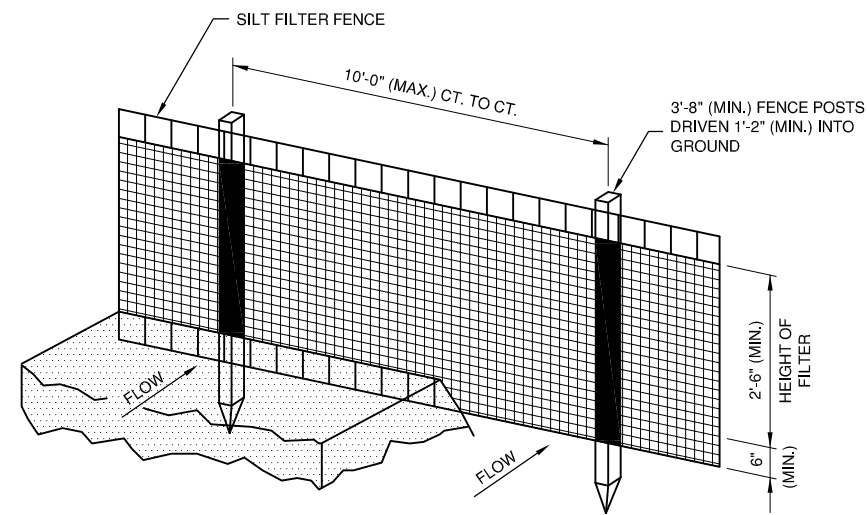
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SHEET	42 OF 55 SHEETS



SECTION

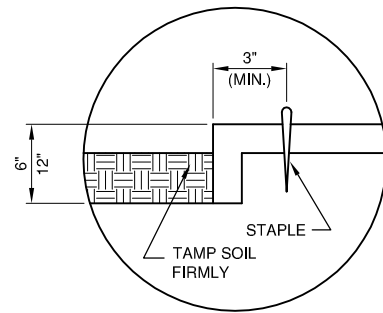


PERSPECTIVE VIEW

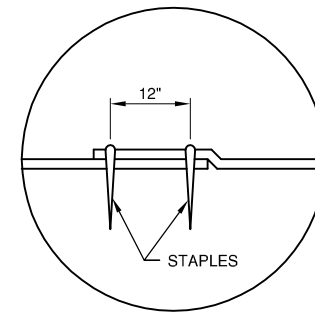
EROSION CONTROL FABRIC FENCE DETAILS
 N.T.S.

EROSION CONTROL FABRIC FENCE NOTES

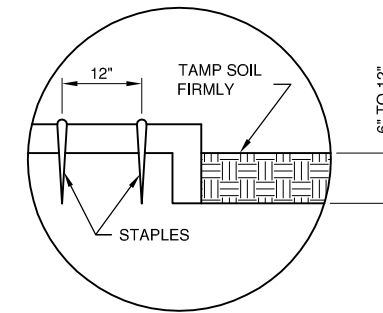
1. WOVEN WIRE FENCE TO BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES.
2. FILTER CLOTH TO BE FASTENED SECURELY TO WOVEN WIRE FENCE WITH TIES SPACED EVERY 2'-0" AT TOP AND MID SECTION.
3. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED BY 6" MINIMUM AND FOLDED.
4. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.



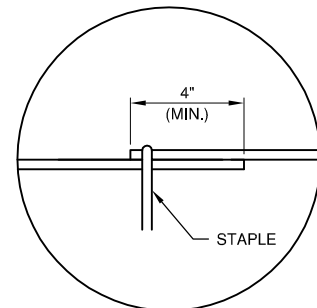
DETAIL 1 - TERMINAL FOLD



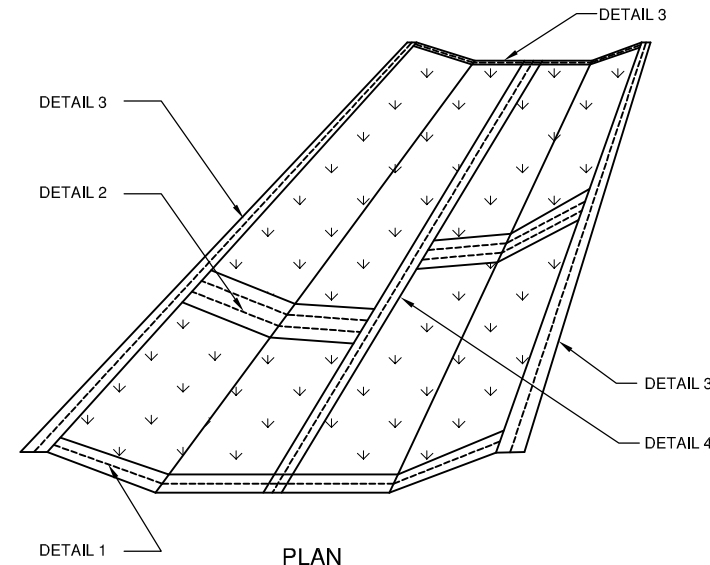
DETAIL 2 - JUNCTION SLOT



DETAIL 3 - ANCHOR SLOT



DETAIL 4 - LAP JOINT

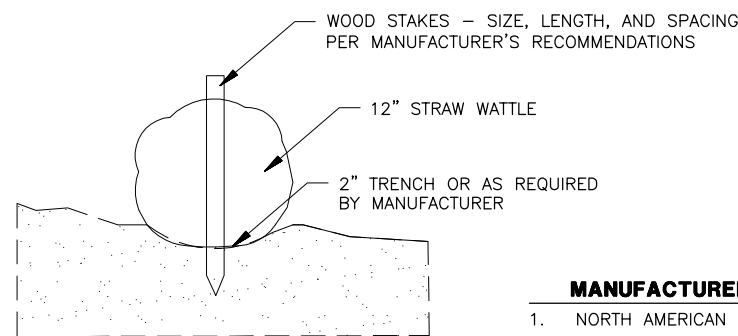


PLAN

EXCELSIOR BLANKET DETAILS
 N.T.S.

EXCELSIOR BLANKET NOTES

1. STAPLES TO BE PLACED ALTERNATELY, IN COLUMNS APPROXIMATELY 2' APART AND IN ROWS APPROXIMATELY 3' APART.
2. EROSION CONTROL MATERIAL SHALL BE PLACED LOOSELY OVER GROUND SURFACE. DO NOT STRETCH.
3. ALL TERMINAL ENDS AND TRANSVERSE LAPS SHALL BE STAPLED AT APPROXIMATELY 12" INTERVALS.

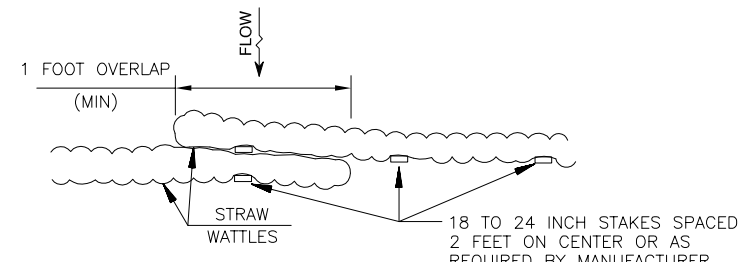


SECTION

MANUFACTURERS:

1. NORTH AMERICAN GREEN
2. WESTERN EXCELSIOR
3. APPROVED EQUAL

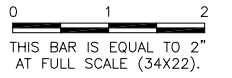
STRAW WATTLES
 N.T.S.



PLAN

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EARTHWORK SUMMARY		
STATION	EXCAVATE END AREA (S.F.)	EMBANK END AREA (S.F.)
APRON EAST EDGE		
3+00	0.00	223.70
3+50	0.00	98.05
4+00	378.82	1.03
4+50	135.92	148.80
5+00	96.63	64.95
5+50	100.33	58.40
6+00	103.01	58.88
6+50	0.00	173.67
TOTAL	814.7	827.5

EARTHWORK SUMMARY		
STATION	EXCAVATE END AREA (S.F.)	EMBANK END AREA (S.F.)
APRON SOUTH EDGE		
0+50	203.82	0.00
1+00	247.13	0.00
1+50	170.58	1.08
2+00	106.22	7.90
2+50	50.25	76.41
3+00	68.61	68.30
3+50	149.34	2.73
3+79	212.49	0.00
TOTAL	1208.4	156.4

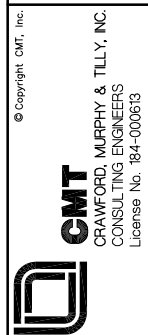
EARTHWORK SUMMARY		
STATION	EXCAVATE END AREA (S.F.)	EMBANK END AREA (S.F.)
PERIMETER ROAD		
23+50	11.71	338.40
24+00	19.74	285.16
24+50	20.99	186.03
25+00	75.61	102.61
25+50	363.15	66.06
26+00	301.87	46.79
26+50	219.50	25.74
27+00	126.84	13.91
27+50	63.95	1.03
28+00	0.00	0.00
TOTAL	1203.4	1065.7

EARTHWORK SUMMARY		
STATION	EXCAVATE END AREA (S.F.)	EMBANK END AREA (S.F.)
PAVED DITCH		
0+50	0.00	0.00
1+00	2.22	105.20
1+50	12.20	116.23
TOTAL	14.4	221.4

EARTHWORK SUMMARY		
STATION	EXCAVATE END AREA (S.F.)	EMBANK END AREA (S.F.)
APRON SOUTH EDGE		
5+05	217.28	0.00
5+50	103.60	18.36
5+81	45.24	95.90
6+00	0.00	92.17
6+50	0.00	60.92
TOTAL	366.1	267.3

BLOOMINGTON-NORMAL AIRPORT AUTHORITY
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BLOOMINGTON, ILLINOIS

EXPAND AIR CARGO APRON
EARTHWORK SUMMARY



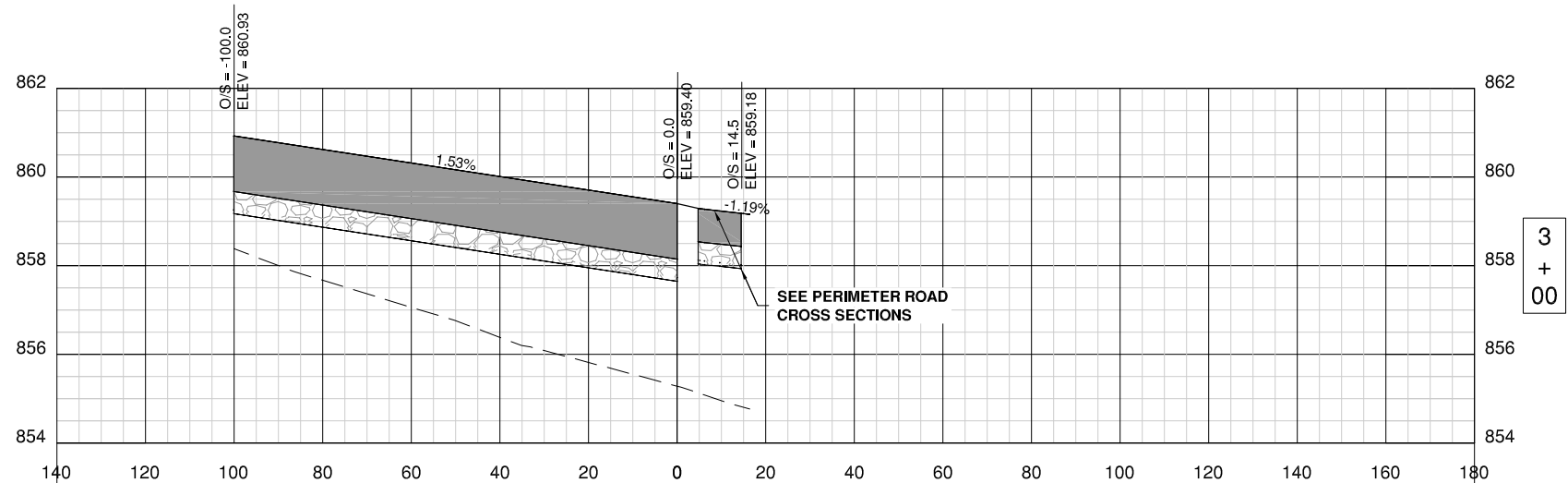
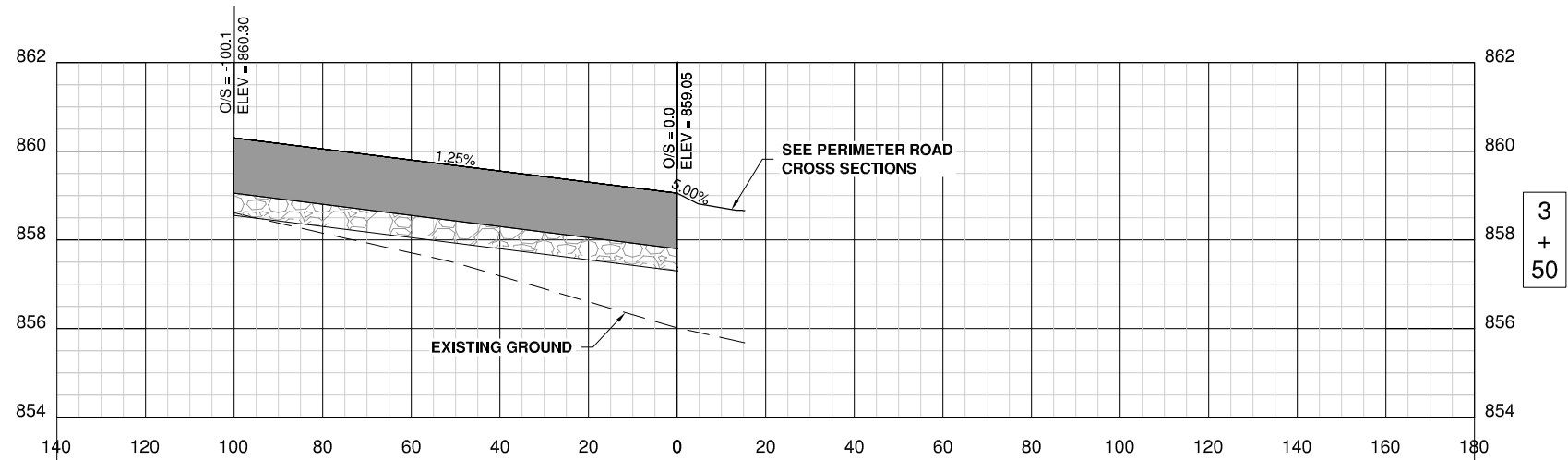
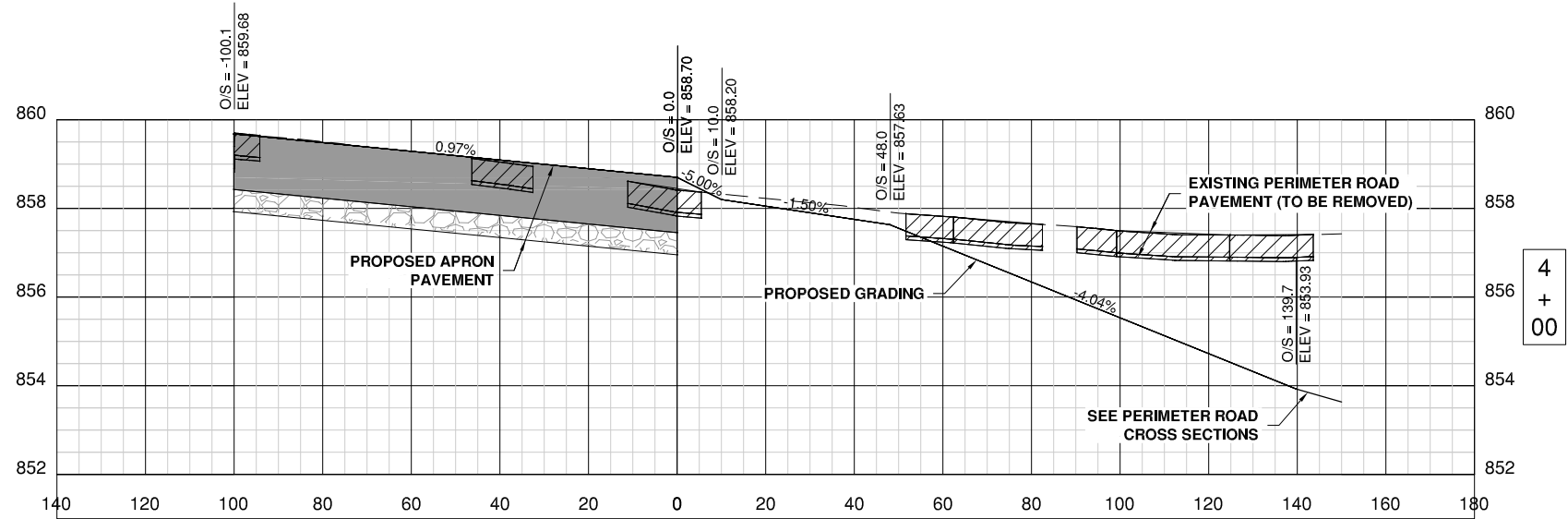
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CHECKED BY:	CBG
APPROVED BY:	RLV
DATE:	MARCH 19, 2013
JOB No:	12085-05-00
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SHEET 43 OF 55 SHEETS	

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EXPAND AIR CARGO APRON
 EAST APRON CROSS SECTION SHEET 1

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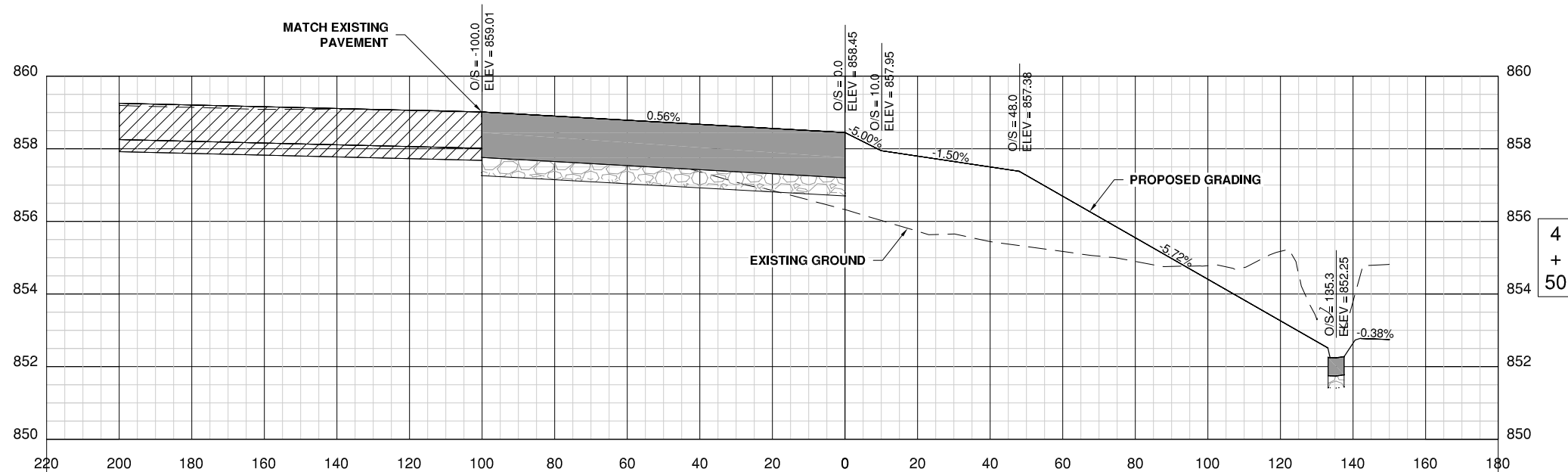
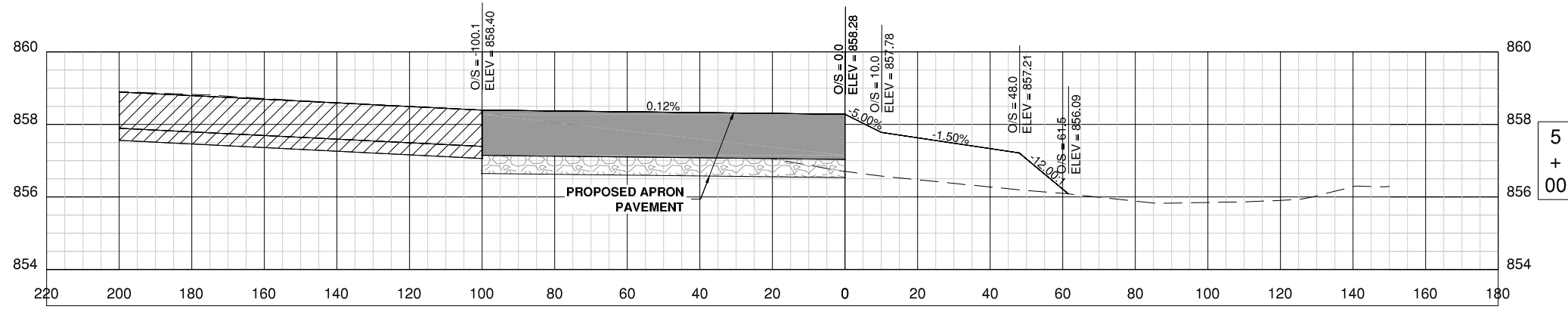
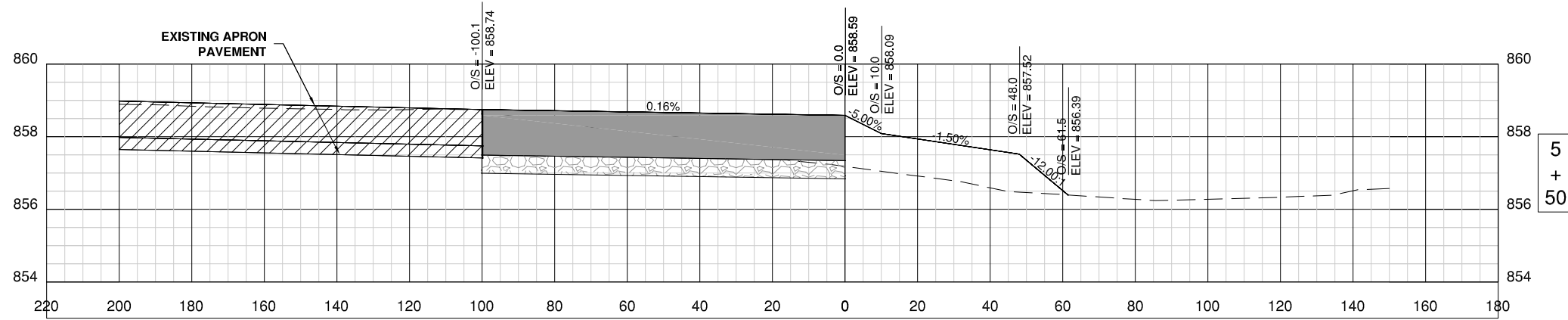


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AIP PROJ. NO.	3-17-0006-XX
SHEET	44 OF 55 SHEETS

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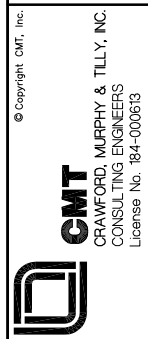
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 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

**EXPAND AIR CARGO APRON
 EAST APRON CROSS SECTION SHEET 2**

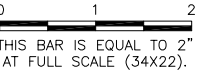


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SHEET	45 OF 55 SHEETS

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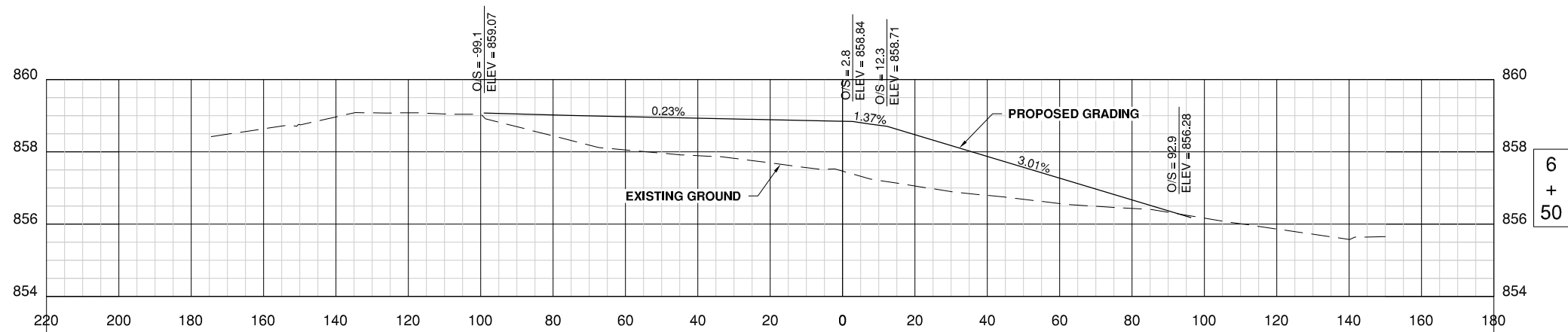
EXPAND AIR CARGO APRON
 EAST APRON CROSS SECTION SHEET 3

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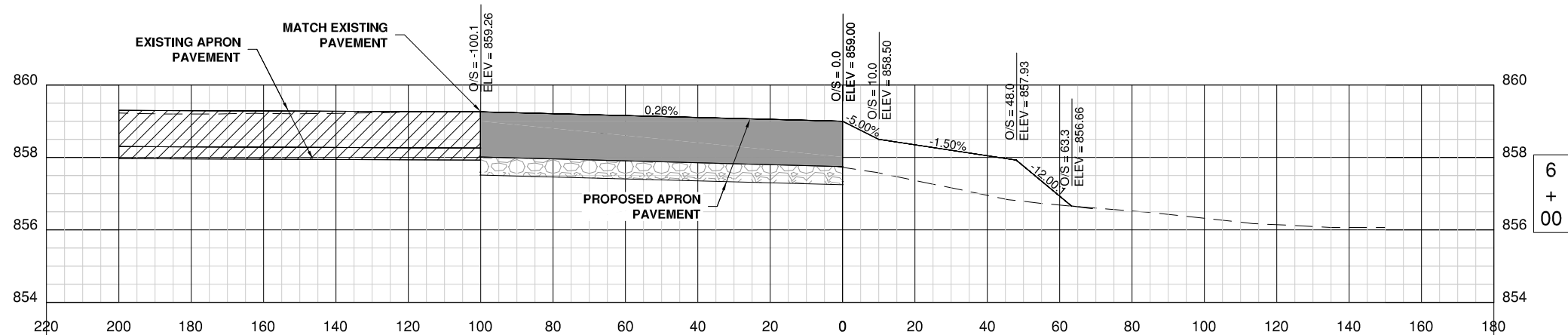


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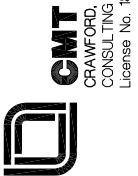
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 BLOOMINGTON, ILLINOIS**

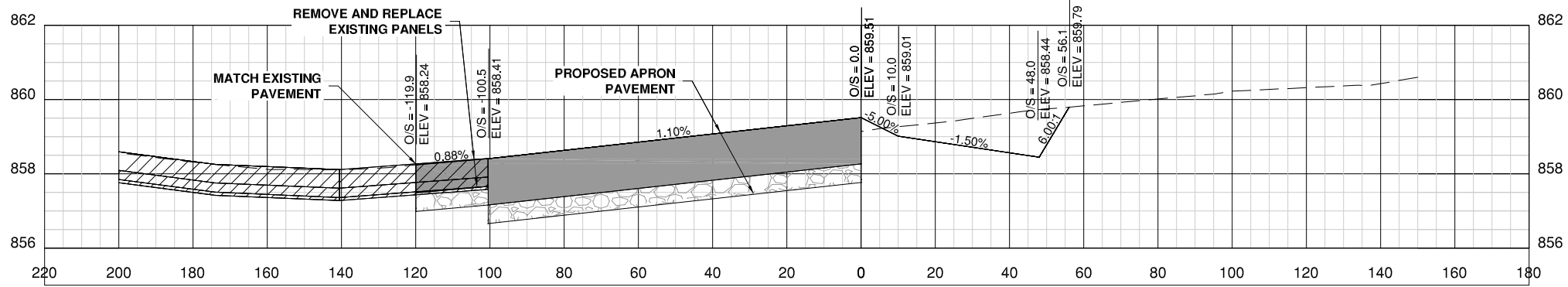
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 SOUTH APRON CROSS SECTION SHEET 1**

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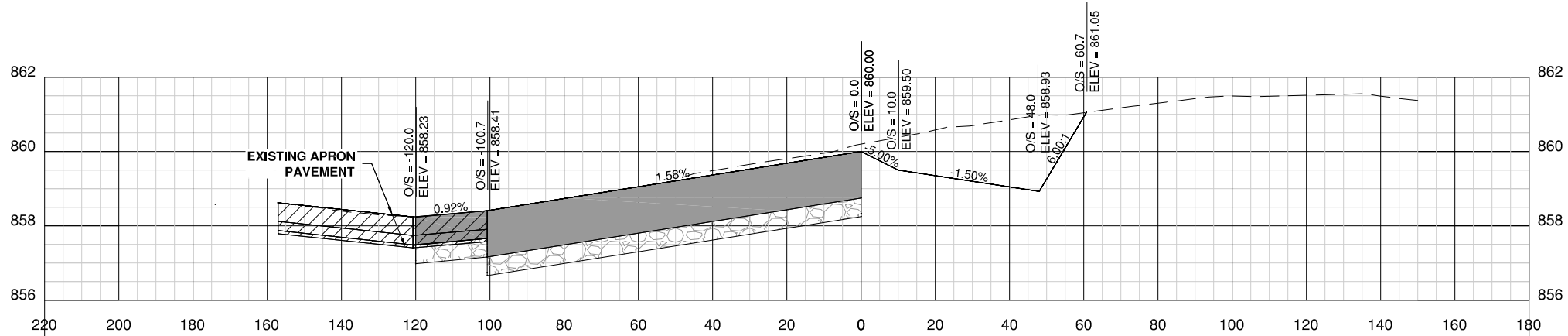


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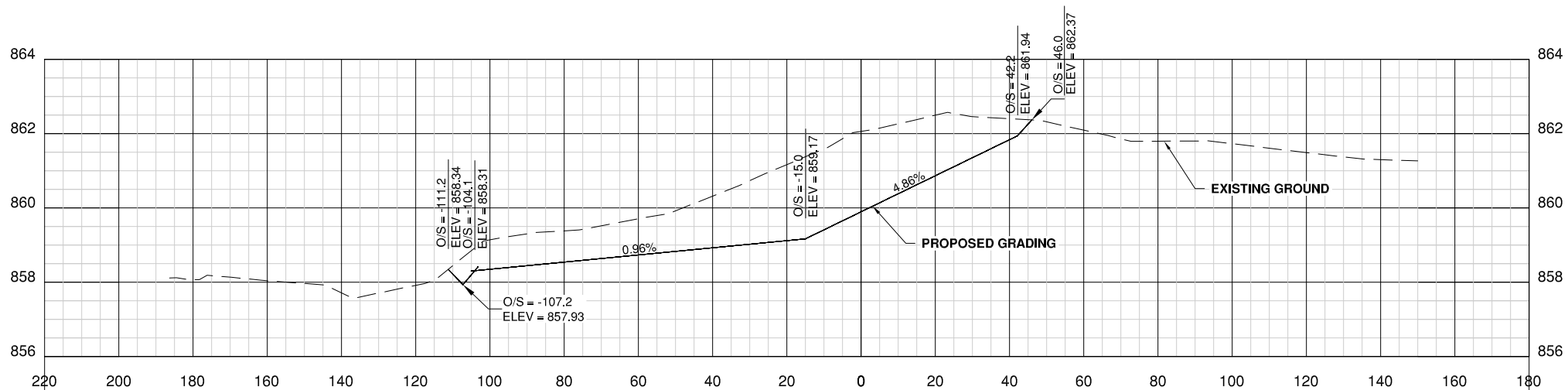
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 SOUTH APRON CROSS SECTION SHEET 2**

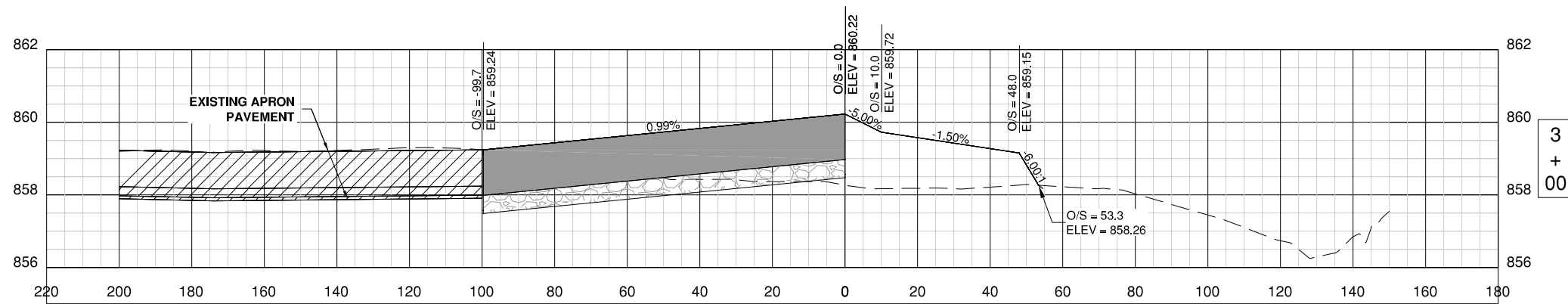
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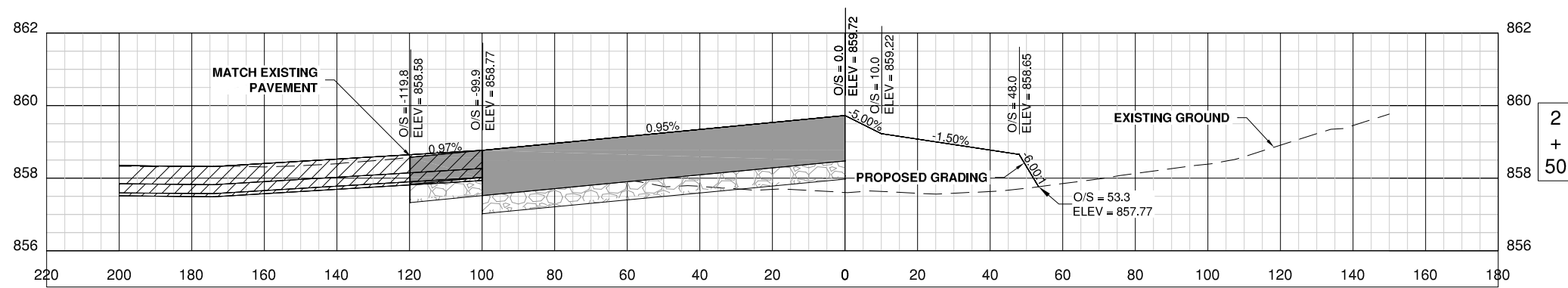


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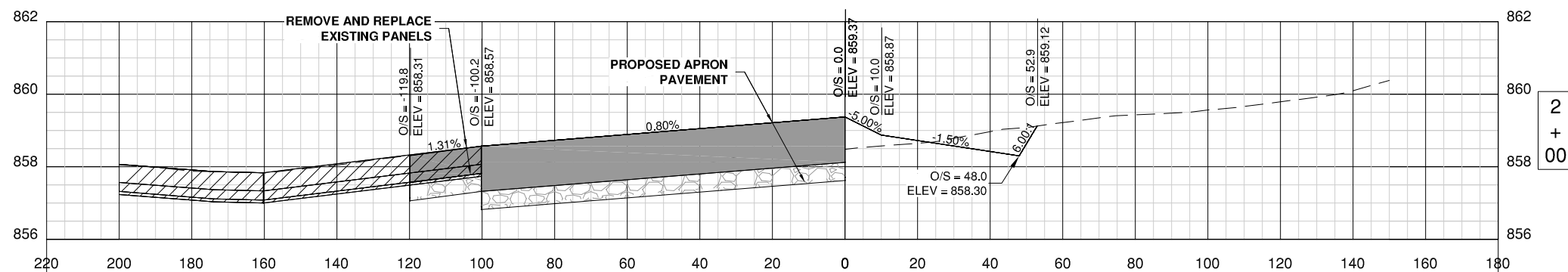
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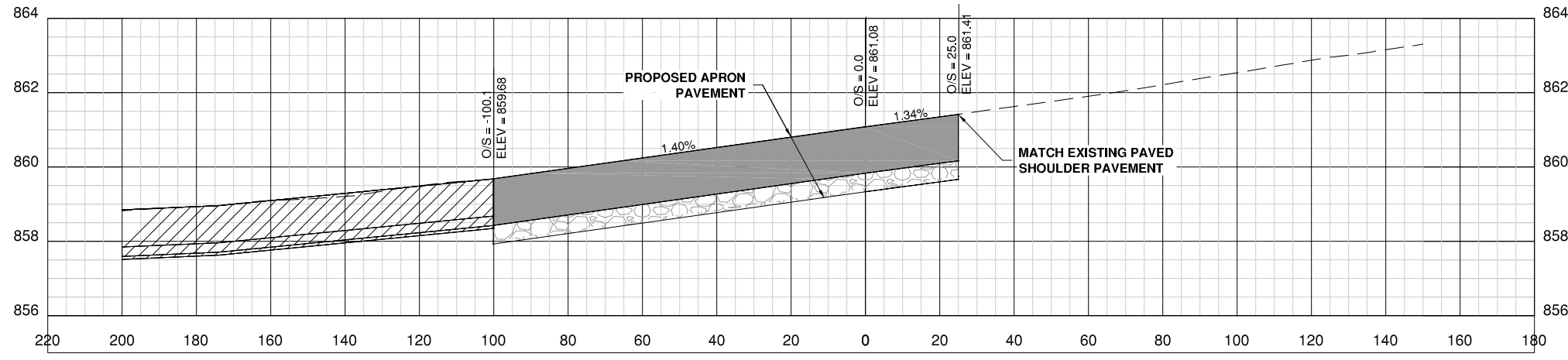
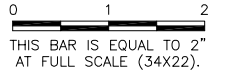
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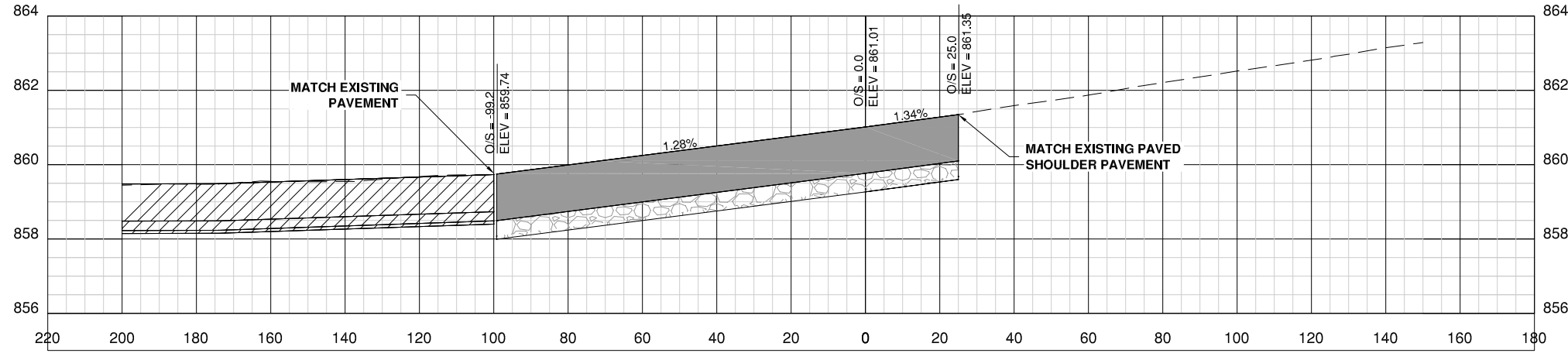
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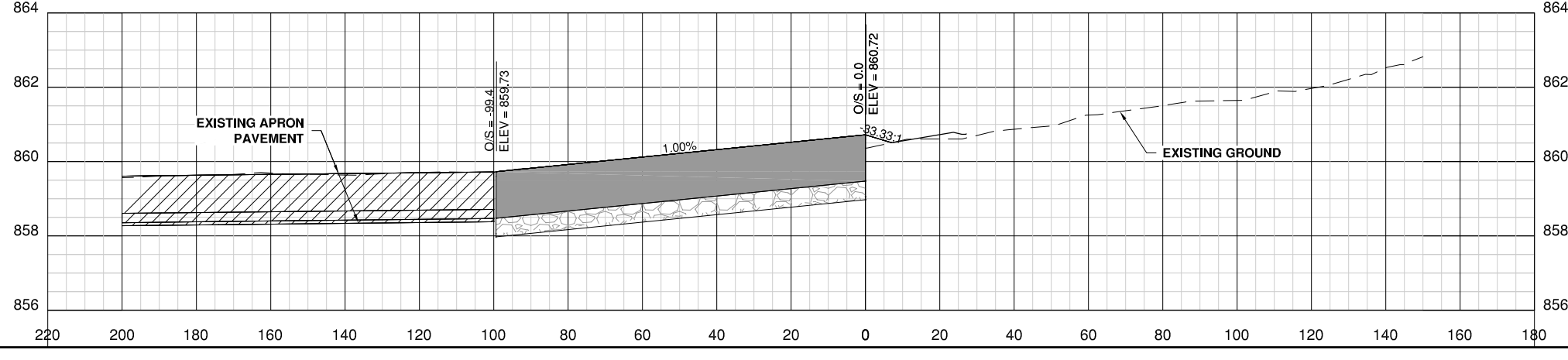
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 SOUTH APRON CROSS SECTION SHEET 3



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EXPAND AIR CARGO APRON
 SOUTH APRON CROSS SECTION SHEET 4

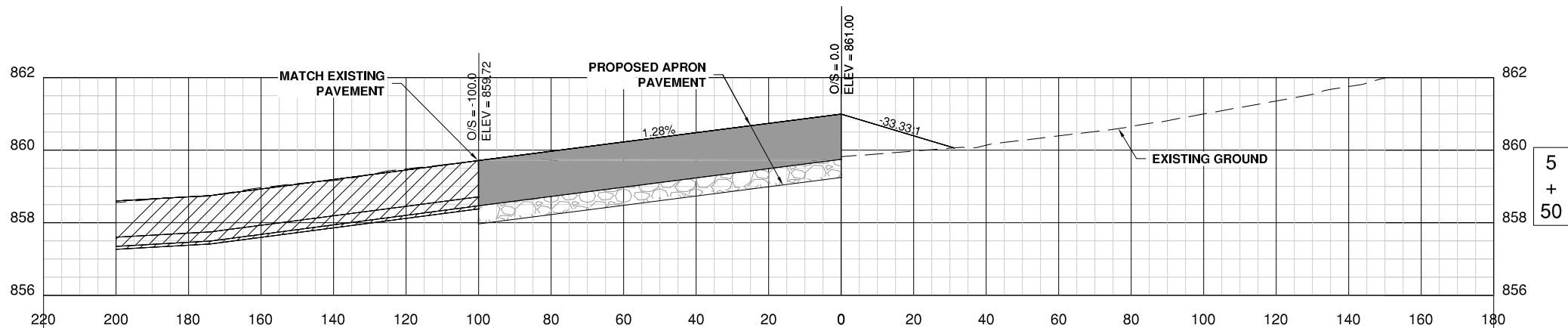
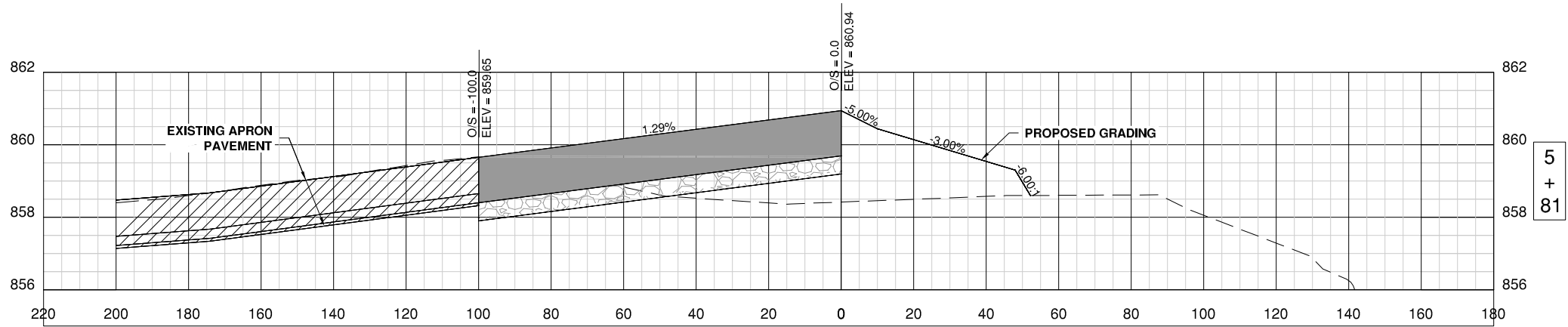
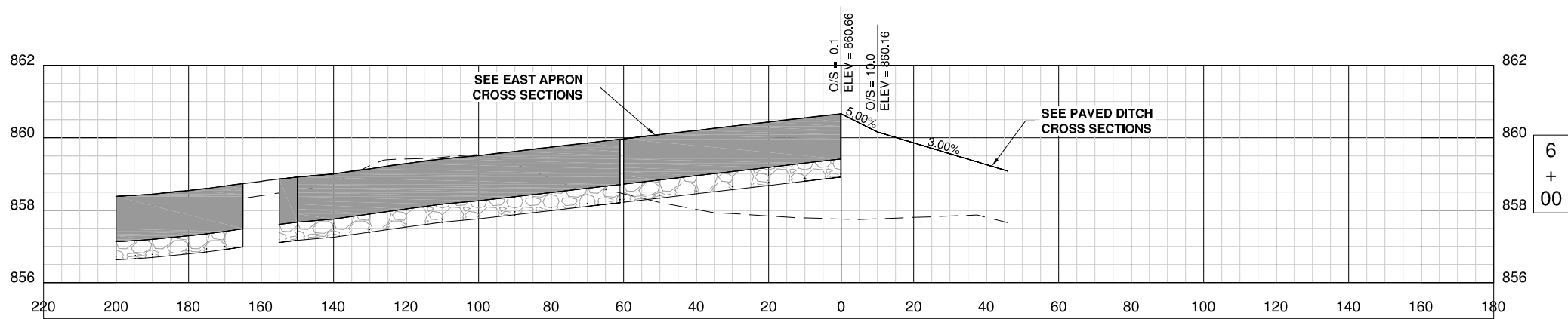
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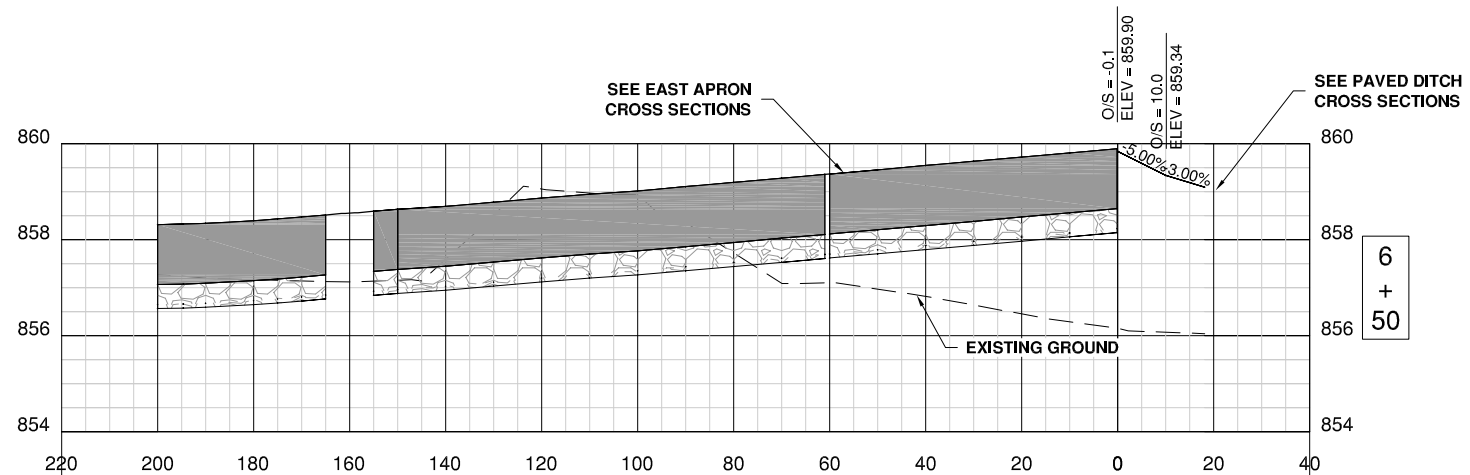
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AIP PROJ. NO.	3-17-0006-XX
SHEET	51 OF 55 SHEETS

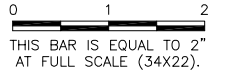


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 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

EXPAND AIR CARGO APRON

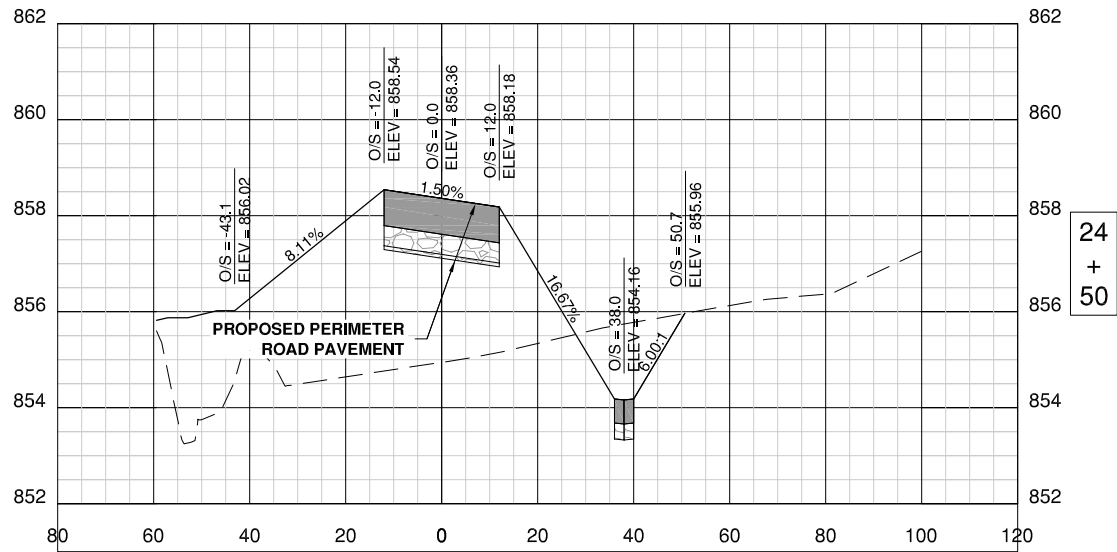
PERIMETER ROAD CROSS SECTION SHEET 1

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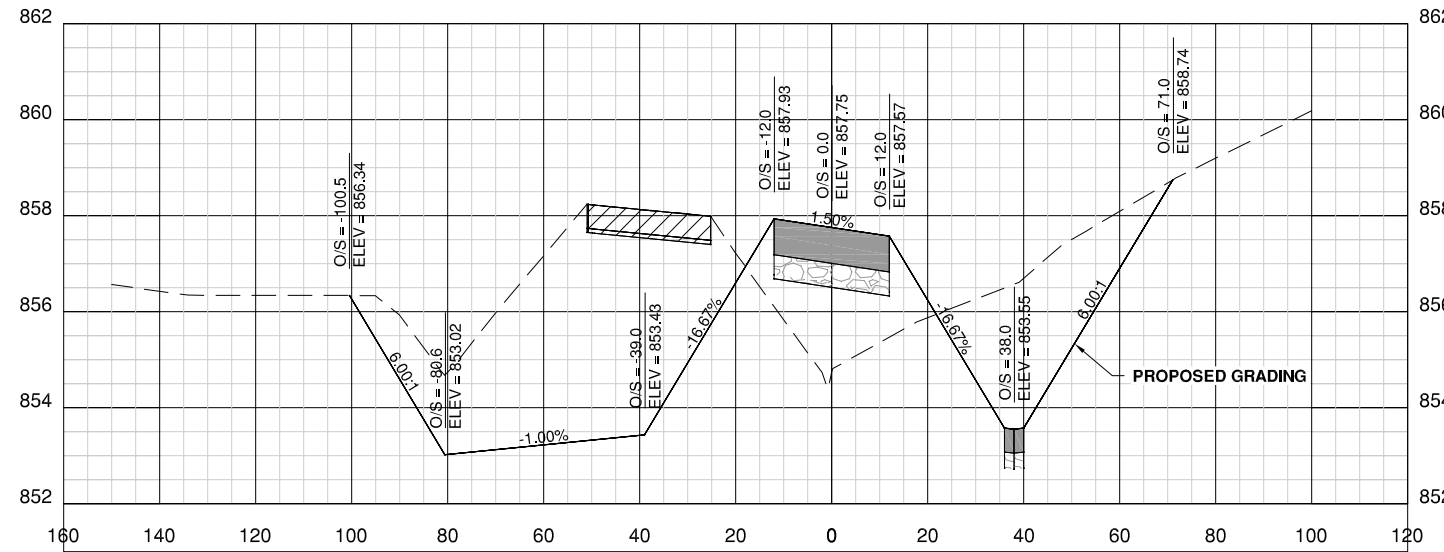
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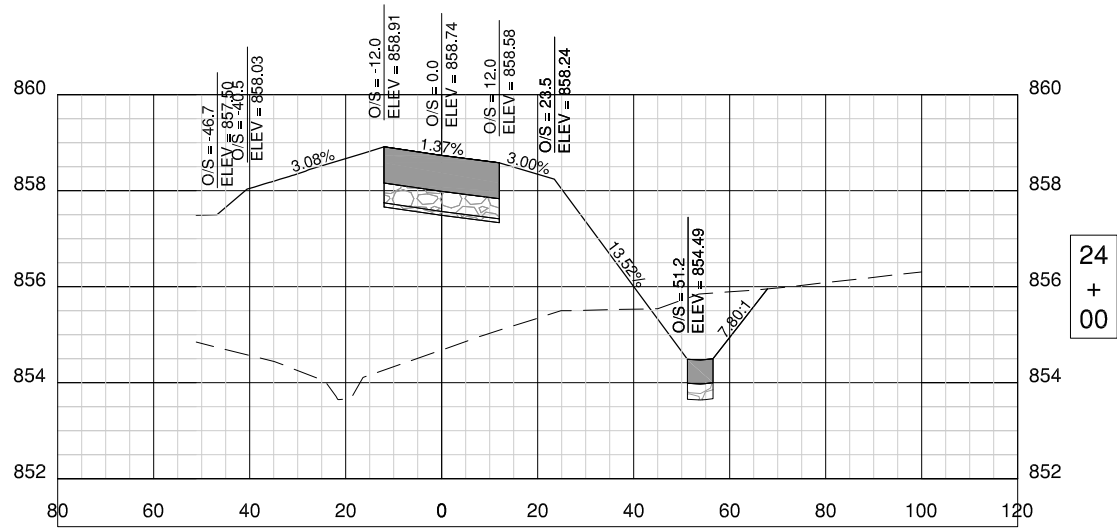
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AIP PROJ. NO.	3-17-0006-XX
SHEET	52 OF 55 SHEETS



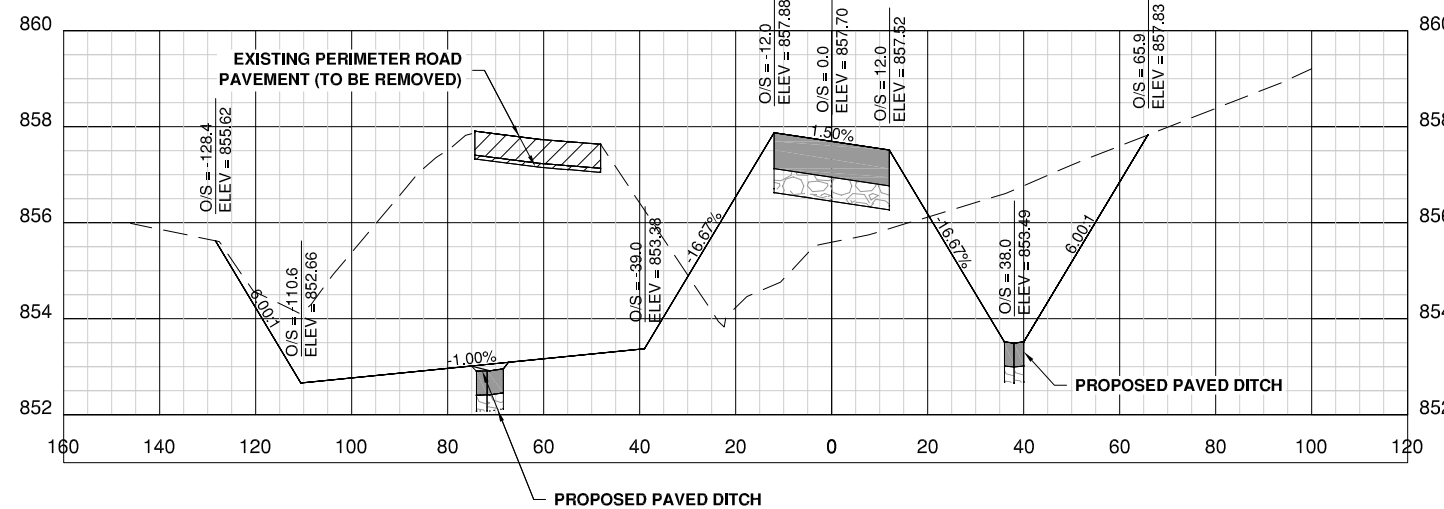
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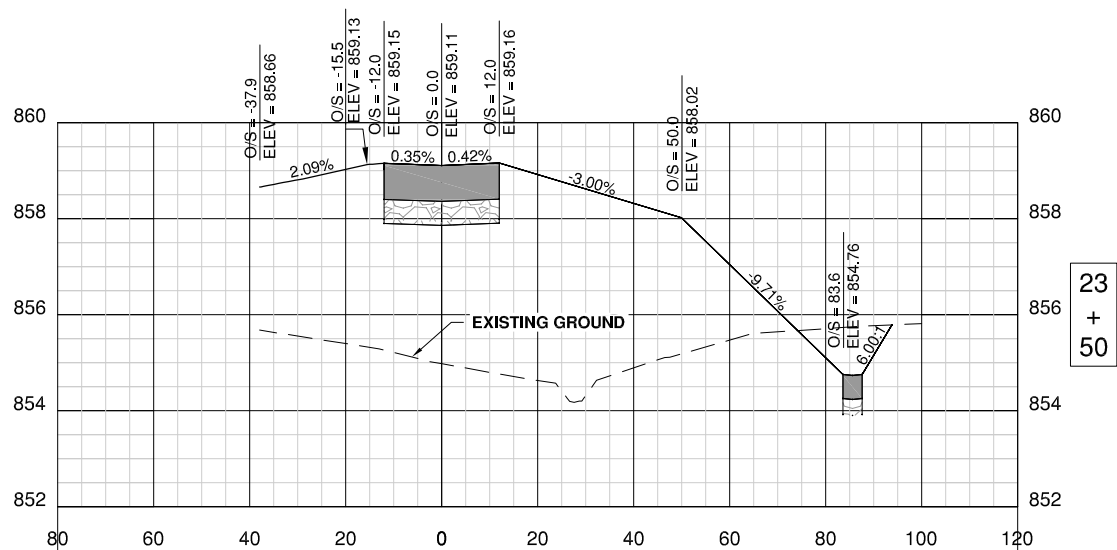
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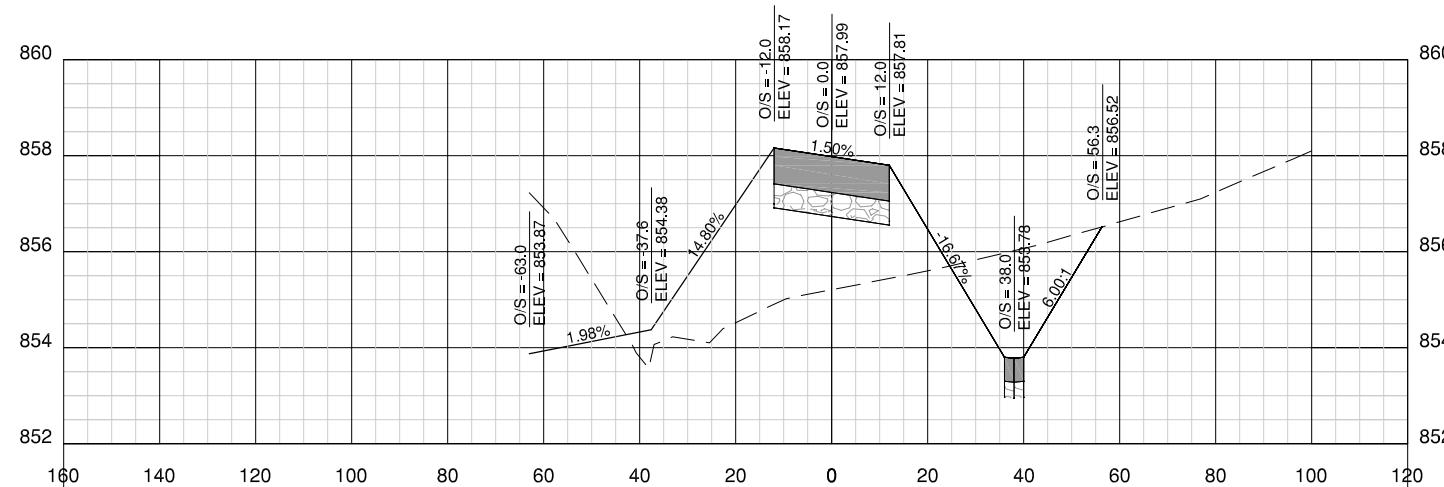
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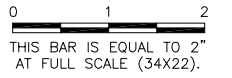
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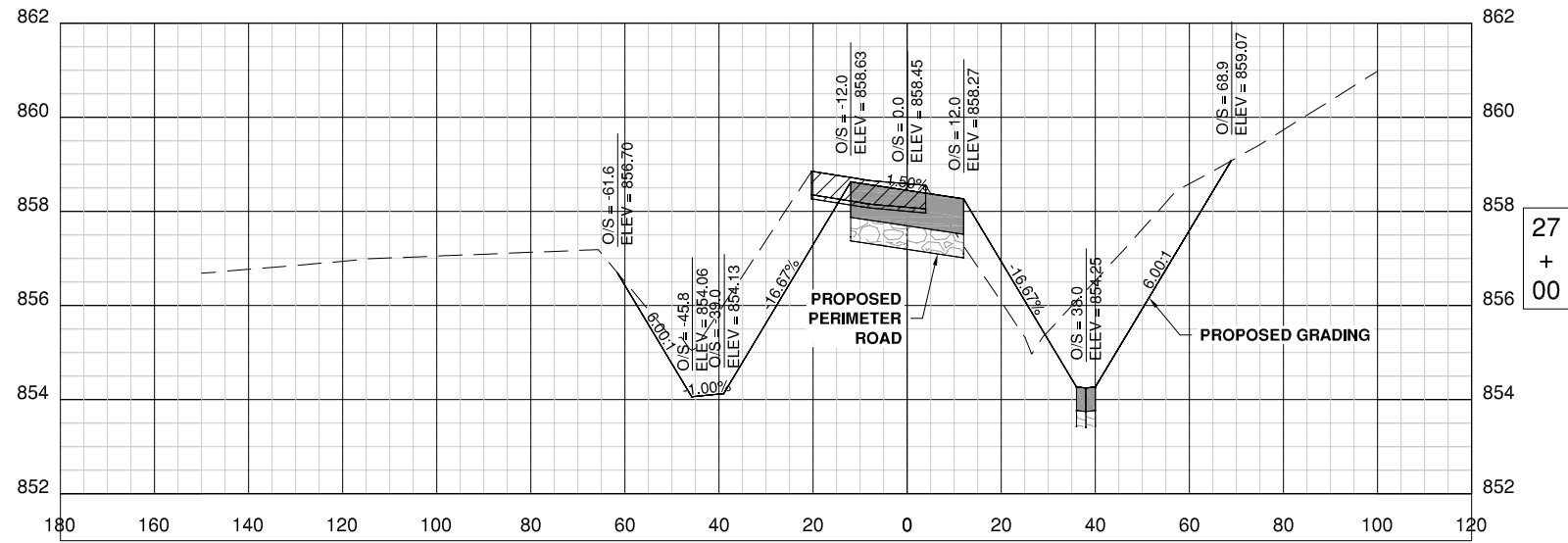
**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

**EXPAND AIR CARGO APRON
 PERIMETER ROAD CROSS SECTION SHEET 2**

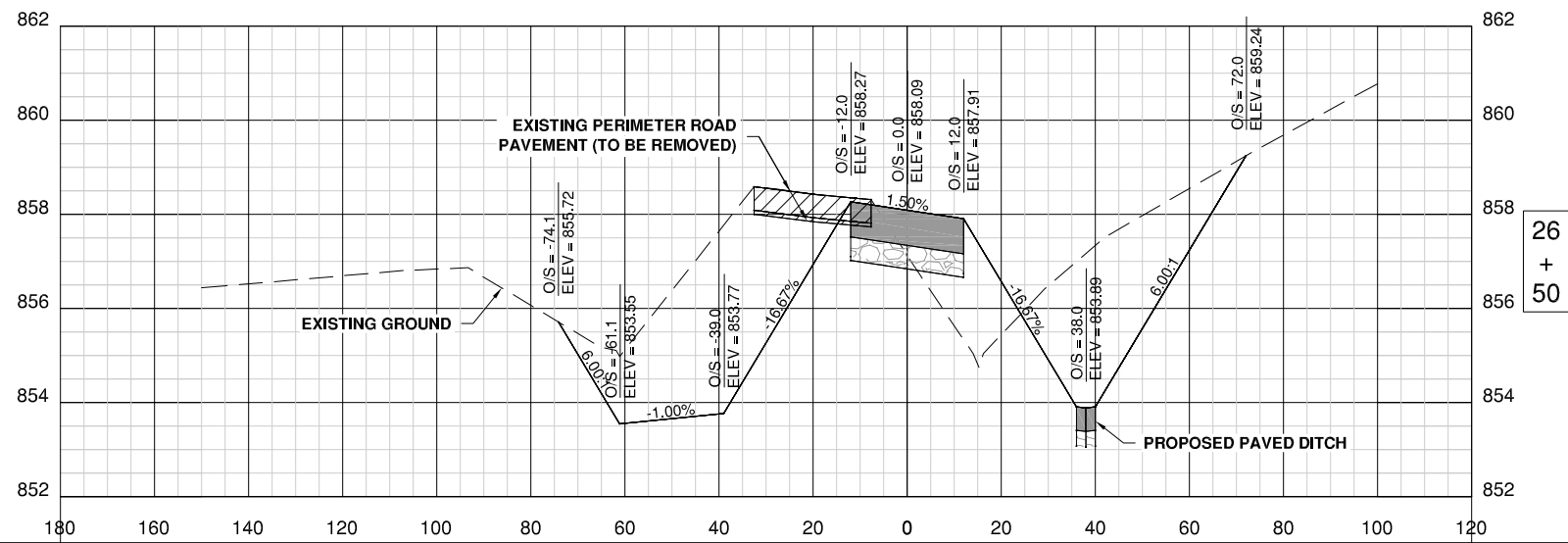
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DRAWN BY:	CMT
CHECKED BY:	CBG
APPROVED BY:	RLV
DATE:	MARCH 19, 2013
JOB No:	12085-05-00
IL. PROJ. NO. BMI-4288 AIP PROJ. NO. 3-17-0006-XX	
SHEET	53 OF 55 SHEETS



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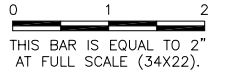


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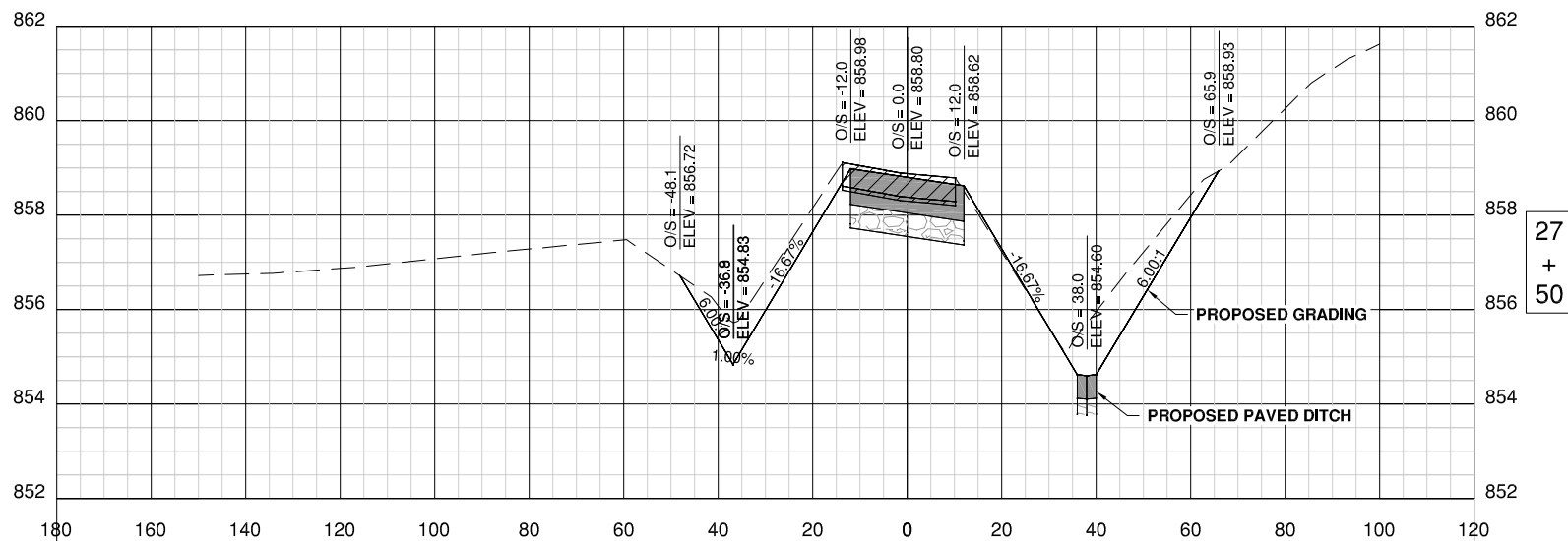
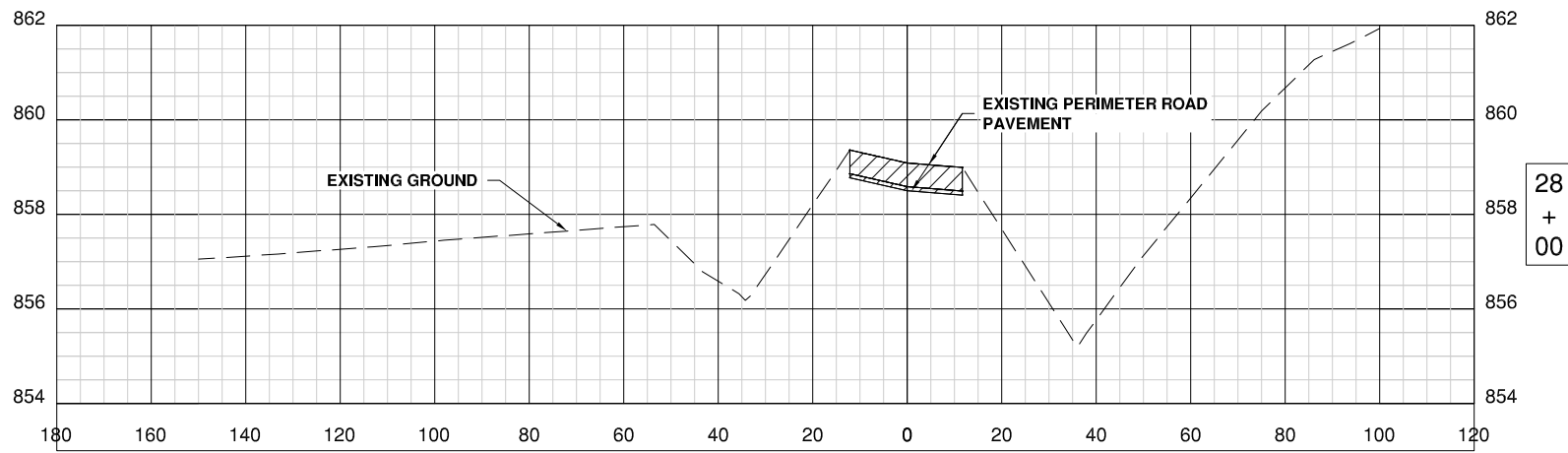
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SHEET	54 OF 55 SHEETS



BL069

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

**BLOOMINGTON-NORMAL AIRPORT AUTHORITY
 CENTRAL ILLINOIS REGIONAL AIRPORT
 BLOOMINGTON, ILLINOIS**

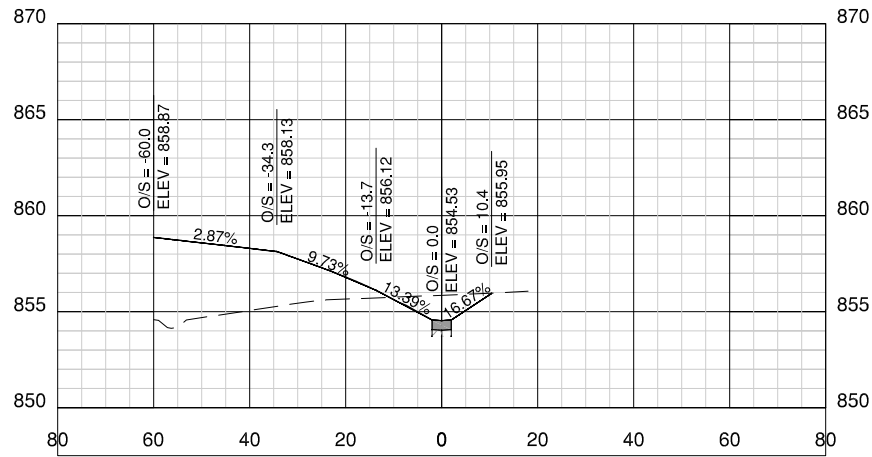
**EXPAND AIR CARGO APRON
 PAVED DITCH CROSS SECTION SHEET 1**

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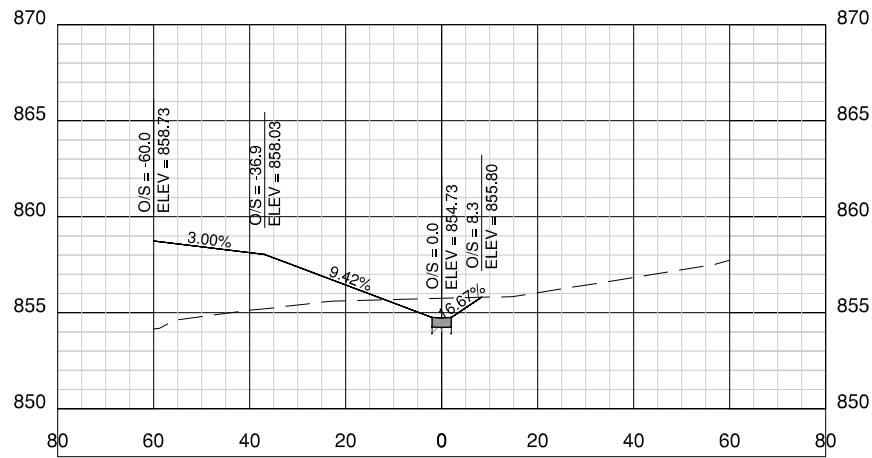


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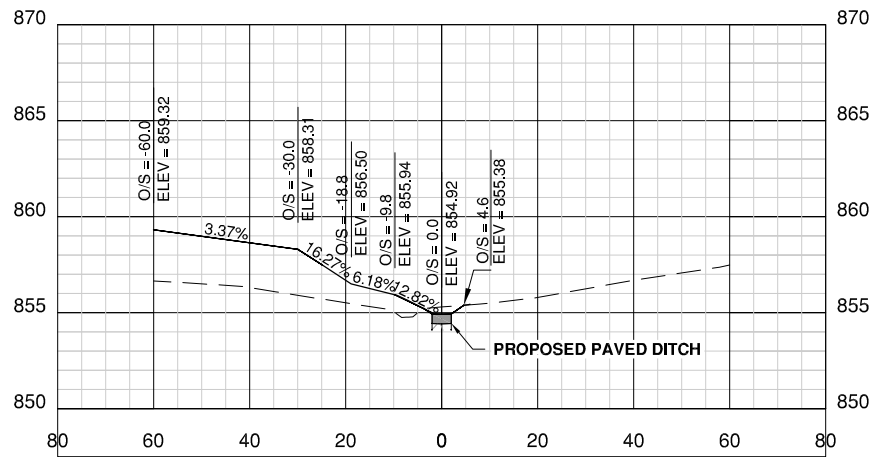
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JOB No:	12085-05-00
IL PROJ. NO.	BMI-4288
AIP PROJ. NO.	3-17-0006-XX
SHEET	55 OF 55 SHEETS



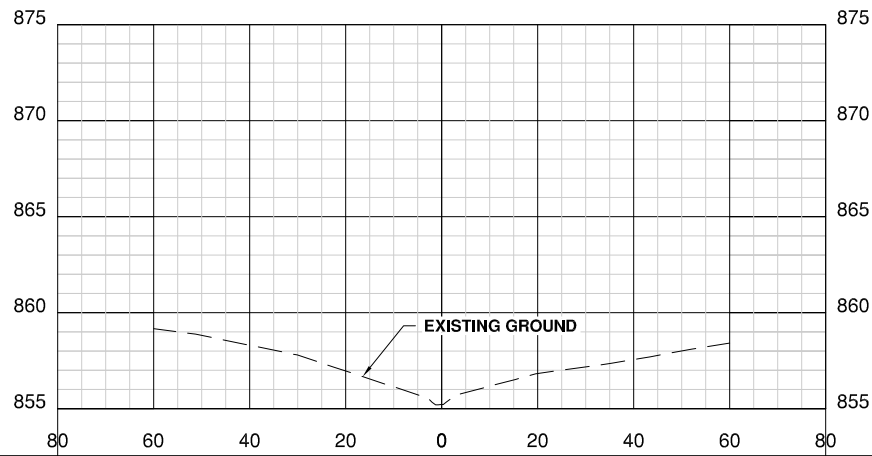
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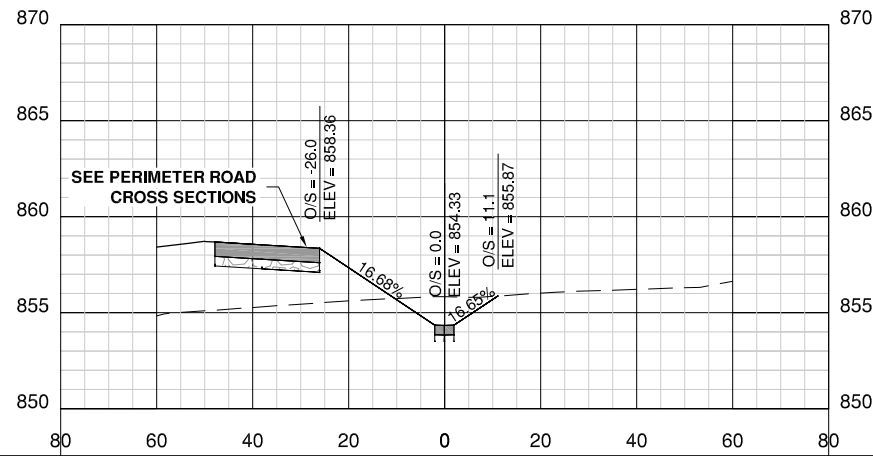
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