

OPERATIONAL SAFETY NOTES

1. FLAGPERSONS AND/OR ESCORTS WITH RADIOS SHALL BE REQUIRED TO CONTROL VEHICLE TRAFFIC ACROSS ACTIVE AIRFIELD PAVEMENTS. NO CONSTRUCTION PERSONNEL/EQUIPMENT ALLOWED WITHIN THE RUNWAY SAFETY AREA (RSA) OR TAXIWAY SAFETY AREA (TSA) WHEN PAVEMENTS ARE OPEN TO AIRCRAFT TRAFFIC. PAVEMENTS ARE TO BE KEPT FREE OF DEBRIS AT ALL TIMES. ANY DAMAGE TO PAVEMENTS BY THE CONTRACTOR'S FORCES SHALL BE REPAIRED AT NO ADDITIONAL COST TO THE CONTRACT.
2. CONSTRUCTION PERSONNEL WILL BE REQUIRED TO ATTEND AIRFIELD DRIVER SAFETY TRAINING (APPROXIMATELY 1 HOUR) AT THE AIRPORT PRIOR TO BEGINNING CONSTRUCTION ACTIVITIES.
3. DURING WORK IN THIS PHASE, THE OWNER WILL ISSUE A NOTAM TO TEMPORARILY REDUCE THE WIDTH OF THE RUNWAY 12R/30L SAFETY AREA (RSA) FROM 500' WIDE TO 300' WIDE (B-III AIRCRAFT, $\geq 3/4$ MI. VIS.) ANY WORK REQUIRED WITHIN 150' OF THE RUNWAY 12R/30L CENTERLINE WILL REQUIRE THE RUNWAY TO BE SHUTDOWN TO AIRCRAFT TRAFFIC. WORK REQUIRED WITHIN THE REDUCED RUNWAY SAFETY AREA MAY BE REQUIRED TO BE SCHEDULED AROUND CERTAIN PEAK TRAFFIC TIMES, AND MUST BE COORDINATED WITH THE OWNER IN ADVANCE.
4. THE CONTRACTOR SHALL LOCATE THE REDUCED RSA LIMITS FOR THIS WORK AREA AT THE START OF CONSTRUCTION AND WILL PLACE FLAGGED LATHE EVERY 150' ALONG IT WITHIN THE WORK AREA LIMITS. THIS LINE WILL BE THE LIMITS THAT ALL CONTRACTOR PERSONNEL MAY VENTURE WHEN THE RUNWAY IS NOT CLOSED. THE CONTRACTOR WILL MAINTAIN THESE LATHE LINES DURING CONSTRUCTION.
5. THE OWNER RESERVES THE RIGHT TO ALLOW SPECIFIC CRITICAL AIRCRAFT (C-II) OPERATIONS DURING CONSTRUCTION THAT WILL REQUIRE THE STANDARD RSA WIDTH OF 500' TO BE TEMPORARILY REINSTATED. IN THESE INSTANCES, THE CONTRACTOR WILL BE REQUIRED TO TEMPORARILY MOVE THEIR PERSONNEL AND EQUIPMENT OUTSIDE OF THE STANDARD RSA LIMITS UNTIL THE AIRCRAFT OPERATION IS COMPLETED (ESTIMATED MAXIMUM 30 MINUTES). THE CONTRACTOR WILL BE GIVEN AS MUCH ADVANCE NOTICE AS POSSIBLE FOR EACH OCCURRENCE. ANTICIPATED FREQUENCY OF THIS OCCURRENCE IS 5 PER WEEK, ON AVERAGE, HOWEVER THE OWNER WILL ATTEMPT TO SCHEDULE CRITICAL AIRCRAFT OPERATIONS OUTSIDE OF THE NORMAL DAILY CONSTRUCTION SCHEDULE AS BEST AS POSSIBLE TO AVOID INTERRUPTIONS TO CONSTRUCTION PROGRESS.
6. IF WEATHER AND ATMOSPHERIC CONDITIONS RESULT IN A LESS THAN $3/4$ -MILE VISIBILITY, THE OWNER RESERVES THE RIGHT TO MODIFY THE TEMPORARY RUNWAY SAFETY AREA TO A 400' WIDTH IN ORDER TO ALLOW CONTINUED AIRCRAFT OPERATIONS ON THE RUNWAY (B-III AIRCRAFT, $< 3/4$ MI. VIS.) IF THIS SCENARIO OCCURS, ANY WORK REQUIRED WITHIN 200' OF THE RUNWAY 12R/30L CENTERLINE WILL REQUIRE THE RUNWAY TO BE SHUTDOWN TO AIRCRAFT TRAFFIC. WORK REQUIRED WITHIN THE REDUCED RUNWAY SAFETY AREA MAY BE REQUIRED TO BE SCHEDULED AROUND CERTAIN PEAK TRAFFIC TIMES, AND MUST BE COORDINATED WITH THE OWNER IN ADVANCE.

- 500' STANDARD RSA (C-II AIRCRAFT)
- 400' MODIFIED RSA (B-III AIRCRAFT, $< 3/4$ MI. VIS.)
- 300' MODIFIED RSA (B-III AIRCRAFT, $\geq 3/4$ MI. VIS.)

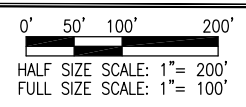
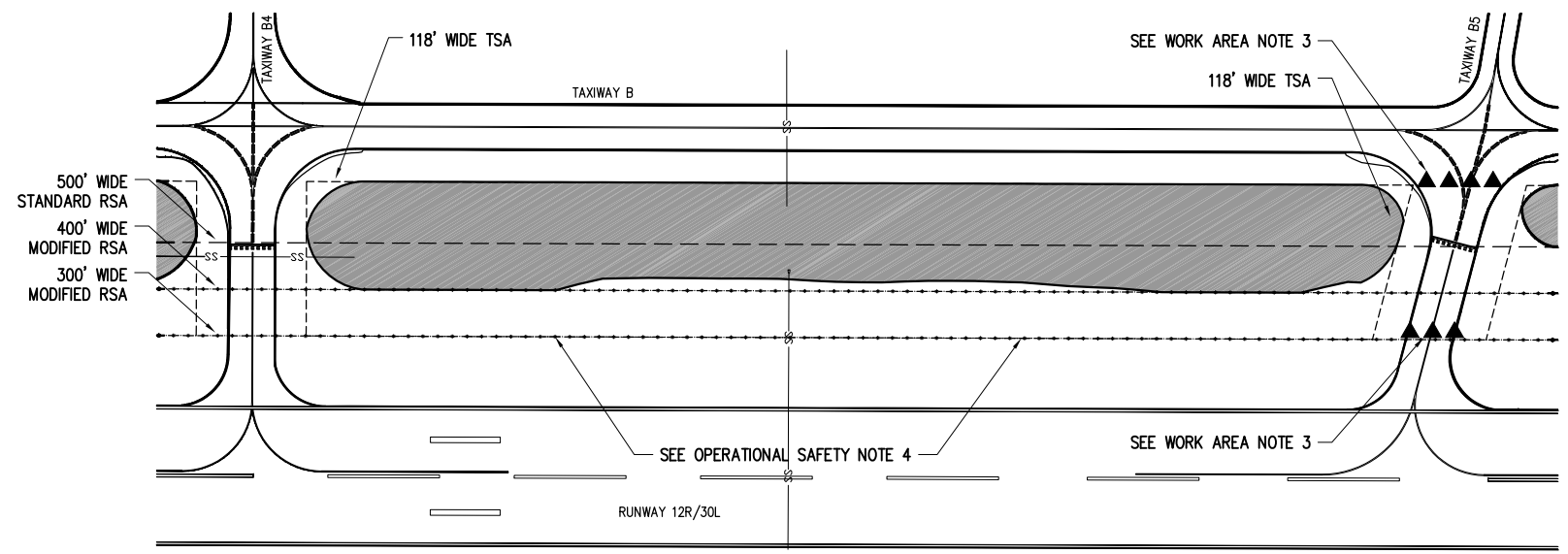
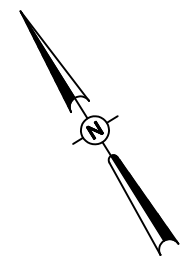
OVERALL PROJECT VIEW

WORK AREA 2 NOTES

1. THE WORK ITEMS TO BE COMPLETED IN THIS AREA INCLUDE UNCLASSIFIED EXCAVATION, MISCELLANEOUS DRAINAGE ITEMS, REROUTING OF UTILITY LINES, SEEDING/MULCHING AND EROSION CONTROL.
2. THE CONTRACTOR SHALL HAVE A MAXIMUM OF 14 CONSECUTIVE CALENDAR DAYS TO COMPLETE THE WORK IN THIS AREA.
3. WORK AREAS 2, 3 AND 4 MAY BE PERFORMED SIMULTANEOUSLY, HOWEVER CONTRACT TIME PROVISIONS SHALL STILL BE ENFORCED FOR EACH WORK AREA. TAXIWAYS "B5" AND "B6" MUST BE PROPERLY BARRICADED AND MAINTAINED DURING WORK HOURS, AND ALL PAVEMENTS MUST BE CLEANED AND BARRICADES REMOVED BEFORE LEAVING THE SITE EACH DAY.
4. CLOSURE CROSSES AND BARRICADES SHALL BE IN PLACE PRIOR TO BEGINNING CONSTRUCTION.
5. AT ALL TIMES, THE CONTRACTOR'S OPERATIONS SHALL BE SUCH AS TO MINIMIZE CLOSURES.
6. AT THE COMPLETION OF ALL WORK AREA CONSTRUCTION, THE HAUL ROAD, EQUIPMENT PARKING AREA, AND GATE ARE TO BE LEFT IN PLACE IN THEIR PRE-CONSTRUCTION CONDITION.
7. THE CONTRACTOR SHALL MAINTAIN NATURAL WATER FLOW THROUGHOUT CONSTRUCTION TO ALLOW FOR COMPLETE REALIGNMENT OF THE DITCH. CONTRACTOR SHALL SUBMIT A WATER/SITE MAINTENANCE PLAN TO THE OWNER FOR REVIEW AND APPROVAL PRIOR TO BEGINNING CONSTRUCTION.
8. THE COSTS FOR ALL ITEMS ASSOCIATED WITH THIS SAFETY PLAN ARE TO BE INCLUDED IN THE COST OF OTHER WORK ITEMS, UNLESS OTHERWISE NOTED WITHIN THE PLAN SET.

LEGEND

- [White Box] EXISTING IMPROVEMENTS
- [Black Box] WORK AREA LIMITS
- [Cross-hatched Box] PROPOSED EQUIPMENT PARKING AREA
- [Diagonal-hatched Box] PROPOSED EARTHWORK STOCKPILE AREA
- [Line with 'x'] EXISTING FENCE
- [Triangle] PROPOSED BARRICADES
- [Arrow] PROPOSED HAUL ROUTE
- [T-shaped symbol] CONSTRUCTION SIGN:
 - [Diamond with 'A'] "CONSTRUCTION TRAFFIC →"
 - [Diamond with 'B'] "← CONSTRUCTION TRAFFIC"



WORK AREA 2 VIEW

REVISION	DATE

SAINT LOUIS DOWNTOWN AIRPORT
A Division of Bi-State Development Agency
BLOCK GRANT PROJ.: 3-17-0039-B29
IL PROJ.: CPS-4210

Hanson Project No.	11A0190	LAYOUT	BSS	01/31/13
Filename	C-006-SFY.dwg	DRAWN	MLH	02/01/13
Scale	AS SHOWN	REVIEWED	BSS	03/08/13
Date	03/08/13			

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GRADE DITCH PARALLEL TO MAIN RUNWAY

PROPOSED CONSTRUCTION SAFETY PLAN - WORK AREA 2

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