

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	2011-200-RS	COOK	24	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO. 60R42		

002

FOR INDEX OF SHEETS, SEE SHEET NO. 2

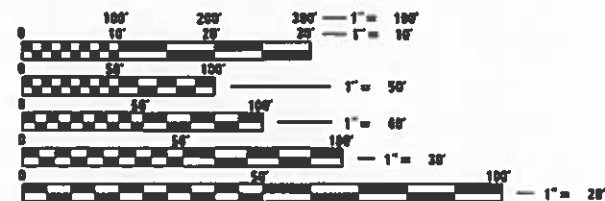
PROPOSED HIGHWAY PLANS

F.A.P. 330: U.S. ROUTE 45
NORTH OF ILL 83 (CALUMET SAG RD.) TO WEST CREEK ROAD
SECTION 2011-200-RS
PROJECT: NHPP - 6XWB(631)
RESURFACING (3P)
COOK COUNTY
C-91-089-12

THE PROJECT IS LOCATED WITHIN
THE VILLAGE OF PALOS PARK

TRAFFIC DATA

2017 ADT - 38,100
POSTED SPEED LIMIT - 45 MPH

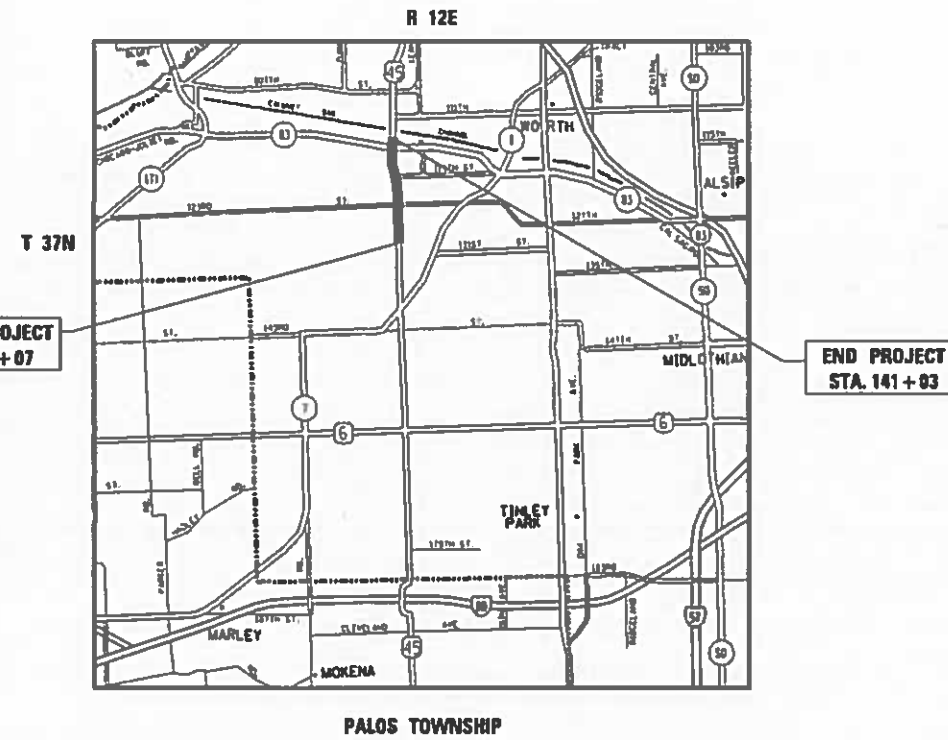


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: VESELIN VELICHKOV (847) 705-4432
PROJECT MANAGER: FAWAD AQUEEL (847) 702-4247

CONTRACT NO. 60R42



GROSS LENGTH OF PROJECT = 9,696 FT = 1.84 MI.
NET LENGTH OF PROJECT = 9,696 FT = 1.84 MI.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED January 24, 2019

Anthony J. Dunigan REGIONAL ENGINEER
March 22, 2019

EA. Elk
ENGINEER OF DESIGN AND ENVIRONMENT

March 29, 2019
David P. Chaf
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED).

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF PALOS PARK.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (45 KM/H) OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (45 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER. A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

THE RESIDENT ENGINEER SHALL CONTACT THE AREA TRAFFIC FIELD TECHNICIAN, PATRICE HARRIS, AT PATRICE.HARRIS@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

THE RESIDENT ENGINEER SHALL VERIFY ALL EXISTING PAVEMENT MARKINGS BEFORE MILLING.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIME DURING CONSTRUCTION.

PAVEMENT MARKING TAPE, TYIII SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.

THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS THAT WILL NOT BE REMOVED INCLUDING PREVIOUSLY SEEDED AREAS, ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE TO THE SATISFACTION OF THE ENGINEER.

CONTACT THE IDOT ROADSIDE DEVELOPMENT UNIT AT 847-705-4171 AT LEAST 2 WEEKS PRIOR TO BEGINNING WORK FOR LAYOUT.

BECAUSE OF THE PROXIMITY OF NORTHERN LONG-EARED BAT HABITAT, ALL TREE REMOVAL SHALL TAKE PLACE BETWEEN OCTOBER 1 TO APRIL 1.

THE CONTRACTOR WILL NEED TO SUBMIT TO THE RESIDENT ENGINEER AN APPROVED ACCESS PLAN FOR TREE REMOVAL WORK FROM STA. 102+00 TO STA. 122+00. THIS AREA CONSISTS OF STEEP SLOPES, LARGE RETAINING WALLS AND GUARDRAIL THROUGHOUT.

WEED CONTROL TEASEL ITEM SHALL BE PLACED WHERE TARGET WEEDS ARE FOUND OR AS DISGNATED BY THE ENGINEER.

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS, STANDARDS, AND GENERAL NOTES
3-5	SUMMARY OF QUANTITIES
6-8	EXISTING & PROPOSED TYPICAL SECTIONS
9-12	ROADWAY & PAVEMENT MARKINGS PLANS
13	DETECTOR LOOP PLANS
14	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-8)
15	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)
16	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
17	BUTT JOINT AND HMA TAPER DETAILS (BD-32)
18	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
19	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11)
20	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)
21	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)
22	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING (TC-16)
23	ARTERIAL ROAD INFORMATION SIGN (TC-22)
24	DISTRICT ONE DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)

LIST OF STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-07	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
420001-09	PAVEMENT JOINTS
442201-03	CLASS C AND D PATCHES
604001-04	FRAME AND LIDS, TYPE 1
606001-07	COMBINATION CONCRETE AND GUTTER
630001-12	STEEL PLATE BEAM GUARDRAIL
701101-05	OFF-ROAD OPERATIONS, MULTILANE, 15' TO 2' FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701411-09	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS > 45 MPH
701421-08	LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS > 45 MPH TO 55 MPH
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS > 45 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	LANE CLOSURE, MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
725001-01	OBJECT AND TERMINAL MARKERS
782006	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS
886001-01	DETECTOR LOOP INSTALLATION
886006-01	TYPICAL LAYOUT FOR DETECTOR LOOPS

FILE NAME =	USER NAME = qureshtye	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FAP 330 (US ROUTE 45) NORTH OF ILL 83 TO WEST CREEK RD. INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES	F.A.P. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\11\084EBIDINTEG\Illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\1089\Drawings\Design\108912-sh-t-plen.dgn	CHECKED -	REVISED -	330			2011-200-R5	COOK	24	2	
PLOT SCALE = 100.1299' / in.	CHECKED -	REVISED -	CONTRACT NO. 60R42							
PLOT DATE = 1/30/2019	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							
					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		

REV. - MS

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005 ROADWAY 80% FED 20% ST				
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	15	15				
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	30	30				
20101300	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH	15	15				
20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	15	15				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	164	164				
25003312	INTERSEEDING, CLASS 4A	ACRE	5.2	5.2				
25200110	SODDING, SALT TOLERANT	SO YD	164	164				
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	46169	46169				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	103	103				
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	2822	2822				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	228	228				
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	618	618				
42001300	PROTECTIVE COAT	SO YD	325	325				
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	68398	68398				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005 ROADWAY 80% FED 20% ST				
44003510	MEDIAN REMOVAL PARTIAL DEPTH	SO FT	49587	49587				
44201803	CLASS D PATCHES, TYPE II, 13 INCH	SO YD	848	848				
44201807	CLASS D PATCHES, TYPE III, 13 INCH	SO YD	377	377				
44201809	CLASS D PATCHES, TYPE IV, 13 INCH	SO YD	660	660				
60250200	CATCH BASINS TO BE ADJUSTED	EACH	7	7				
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	4	4				
* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	850	850				
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE I (SPECIAL) TANGENT	EACH	4	4				
63200310	GUARDRAIL REMOVAL	FOOT	850	850				
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6				
67100100	MOBILIZATION	L SUM	1	1				
70100310	TRAFFIC CONTROL AND PROTECTION, STANDARD 701421	L SUM	1	1				
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	1	1				

FILE NAME - c:\pw_work\p\dd\qureshiya\0287739-D108912-sht-plan.dgn	USER NAME - qureshiya	DESIGNED -	REVISOR -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S. ROUTE 45 SUMMARY OF QUANTITIES	F.A.P. RTE. 330	SECTION 2011-200-RS	COUNTY COOK	TOTAL SHEETS 24	SHEET NO. 3	
PLOT SCALE = 100/299' / 1" =	CHECKED -	REVISOR -	SCALE:			SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		CONTRACT NO. 60R42	
PLOT DATE = 1/31/2019	DATE -	REVISOR -									

URBAN

URBAN

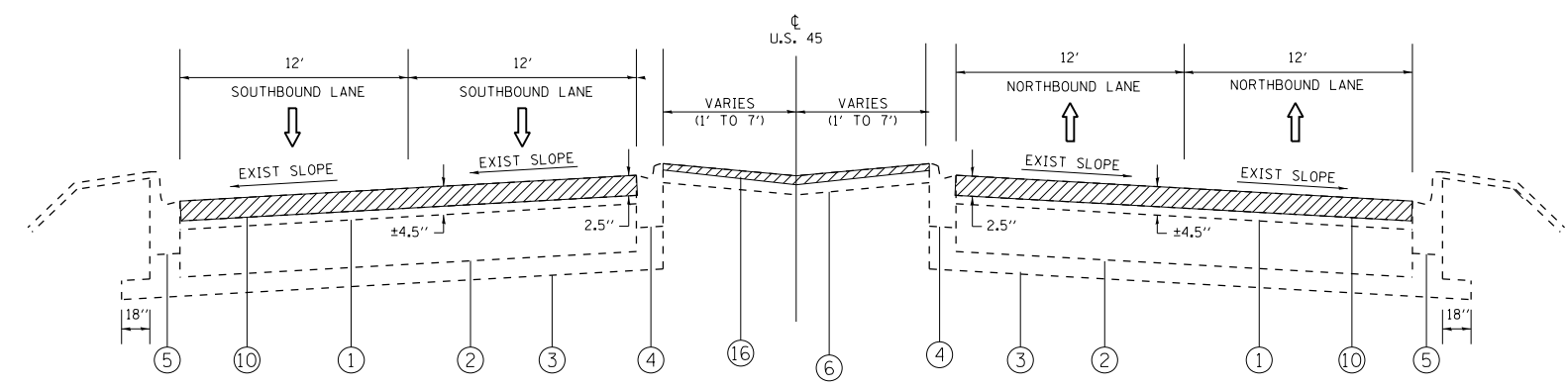
SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005 ROADWAY 80% FED 20% ST				
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1				
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1				
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1				
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1				
70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	1				
70300100	SHORT TERM PAVEMENT MARKING	FOOT	381	381				
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	381	381				
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	294	294				
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	25294	25294				
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1991	1991				
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	371	371				
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	114	114				
* 72501000	TERMINAL MARKER - DIRECT APPLIED	EACH	4	4				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005 ROADWAY 80% FED 20% ST				
*78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	294	294				
*78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	25294	25294				
*78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1991	1991				
*78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	371	371				
*78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	114	114				
*78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	310	310				
78200005	GUARDRAIL REFLECTORS, TYPE A	EACH	8	8				
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	186	186				
*88600600	DETECTOR LOOP REPLACEMENT	FOOT	549	549				
K0029624	WEED CONTROL, TEASEL	GALLON	5	5				
X0325222	WEED CONTROL, BASAL TREATMENT	GALLON	30	30				
X1900002	INTERSEEDING, CLASS 4 (MODIFIED)	ACRE	5	5				
X2010350	TREE REMOVAL, ACRES (SPECIAL)	ACRE	5	5				* SPECIALTY ITEM # NON PARTICIPATING ITEM

LEGEND

- ① EXISTING HMA SURFACING, ± 4.50"
- ② EXISTING HMA BASE COURSE, ± 11"
- ③ EXISTING SUB-BASE GRANULAR MATERIAL TYPE A, 4"
- ④ EXISTING COMB. CONC. CURB & GUTTER, TYPE M-2.12
- ⑤ EXISTING COMB. CONC. CURB & GUTTER, TYPE B-6.24
- ⑥ EXISTING STABILIZED MEDIAN SURFACE, 12"
- ⑦ EXISTING HMA SHOULDER, 8"
- ⑧ EXISTING CONCRETE WALL (DOWELS EMBEDDED 8" MIN.)
- ⑨ EXISTING PGE SPECIAL, 12"
- ⑩ PROPOSED HMA SURFACE REMOVAL, 2-1/2"
- ⑪ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑫ PROPOSED POLY. HMA SURFACE COURSE, SMA, 9.5, N80, 1-3/4"
- *⑬ PROPOSED COMBINATION CURB AND GUTTER REMOVAL & REPLACEMENT
- ⑭ PROPOSED TOPSOIL FURNISH AND PLACE, 4"
- ⑮ PROPOSED SODDING, SALT TOLERANT
- ⑯ PROPOSED HMA MEDIAN SURFACE REMOVAL, 2" (PAID AS MEDIAN REMOVAL PARTIAL DEPTH)
- ⑰ PROPOSED HMA SURFACE COURSE, MIX "D", 9.5, N50, 2"

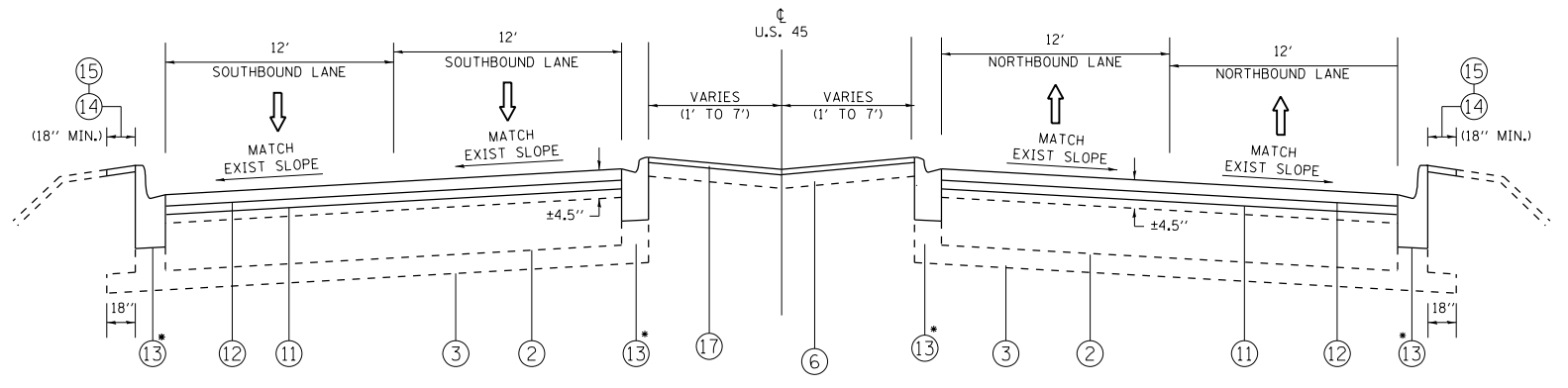
NOTE
THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING



US 45 (LA GRANGE RD.)
EXISTING TYPICAL SECTION
STA. 44+07 TO STA. 103+85

SOUTHBOUND LEFT TURN LANE:
 ● BLACK WALNUT TRAIL (STA. 46+44 TO STA. 47+37)
 ● 123RD/McCARTHY RD. (STA. 73+26 TO STA. 74+86)
 ● 119TH ST. (STA. 99+73 TO STA. 102+00)

NORTHBOUND LEFT TURN LANE:
 ● WILD CHERRY LANE (STA. 48+59 TO STA. 49+60)
 ● POWELL RD. (STA. 59+04 TO STA. 60+71)
 ● 123RD/McCARTHY RD. (STA. 70+82 TO STA. 72+36)
 ● SWALLOW CLIFF WOOD ENTRANCE (STA. 95+99 TO 97+66)



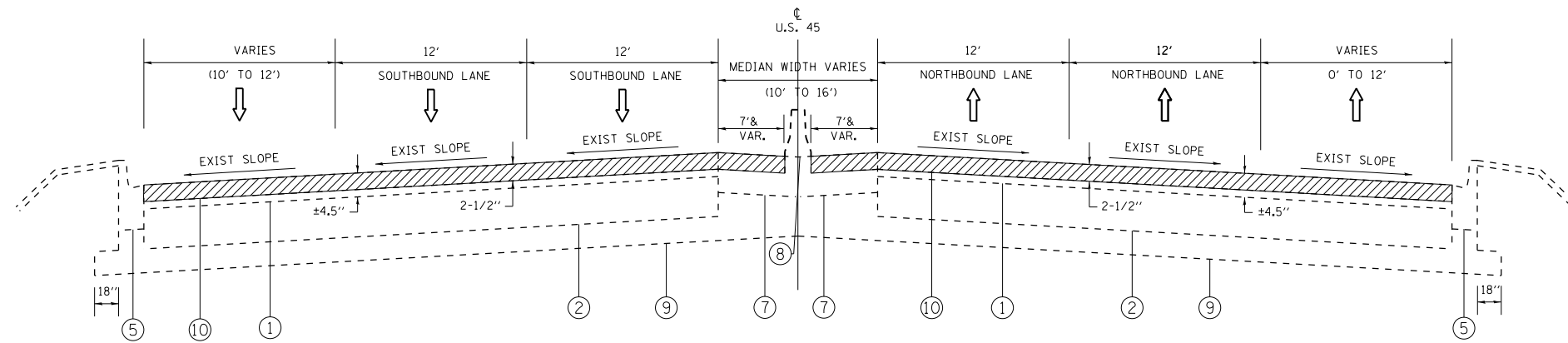
US 45 (LA GRANGE RD.)
PROPOSED TYPICAL SECTION
STA. 44+07 TO STA. 103+85

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR VOIDS(%) @ Ndes	
PAVEMENT RESURFACING		
POLY. HMA SURFACE COURSE, SMA, 9.5, N80	3.5% @ 80 GYR.	QCP
POLY. LEVELING BINDER (MM), IL-4.75, N50	3.5% @ 50 GYR.	QCP
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR.	QC/QA
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR.	QC/QA
MEDIAN RESURFACING		
HMA SURFACE COURSE, MIX "D", 9.5, N50	4% @ 50 GYR.	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP)		

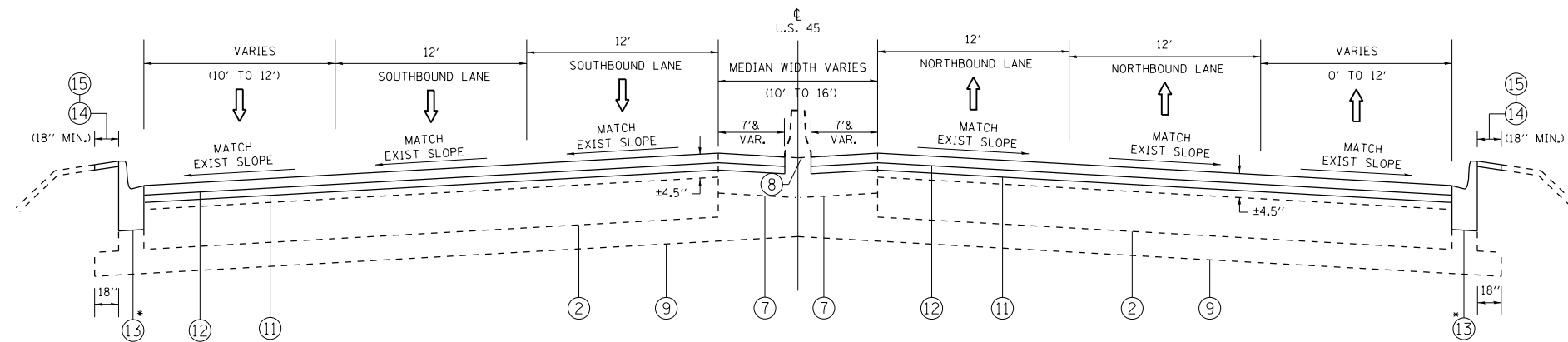
NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
 NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
 FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS.
 QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE

LEGEND

- ① EXISTING HMA SURFACING, ± 4.50"
- ② EXISTING HMA BASE COURSE, ± 11"
- ③ EXISTING SUB-BASE GRANULAR MATERIAL TYPE A, 4"
- ④ EXISTING COMB. CONC. CURB & GUTTER, TYPE M-2.12
- ⑤ EXISTING COMB. CONC. CURB & GUTTER, TYPE B-6.24
- ⑥ EXISTING STABILIZED MEDIAN SURFACE, 12"
- ⑦ EXISTING HMA SHOULDER, 8"
- ⑧ EXISTING CONCRETE WALL (DOWELS EMBEDDED 8" MIN.)
- ⑨ EXISTING PGE SPECIAL, 12"
- ⑩ PROPOSED HMA SURFACE REMOVAL, 2-1/2"
- ⑪ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑫ PROPOSED POLY. HMA SURFACE COURSE, SMA, 9.5, N80, 1-3/4"
- ⑬ PROPOSED COMBINATION CURB AND GUTTER REMOVAL & REPLACEMENT
- ⑭ PROPOSED TOPSOIL FURNISH AND PLACE, 4"
- ⑮ PROPOSED SODDING, SALT TOLERANT
- ⑯ PROPOSED HMA MEDIAN SURFACE REMOVAL, 2" (PAID AS MEDIAN REMOVAL PARTIAL DEPTH)
- ⑰ PROPOSED HMA SURFACE COURSE, MIX "D", 9.5, N50, 2"



US 45 (LA GRANGE RD.)
EXISTING TYPICAL SECTION
STA. 103+85 TO STA. 136+31

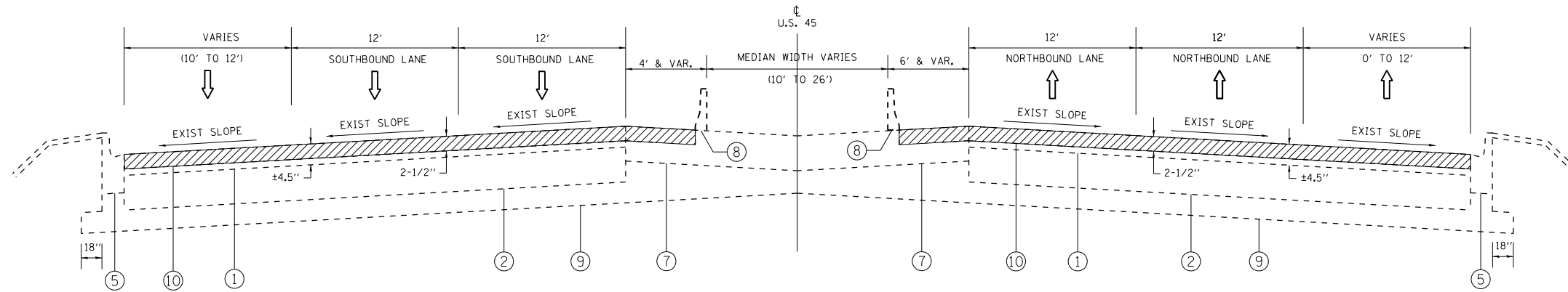


US 45 (LA GRANGE RD.)
PROPOSED TYPICAL SECTION
STA. 103+85 TO STA. 136+31

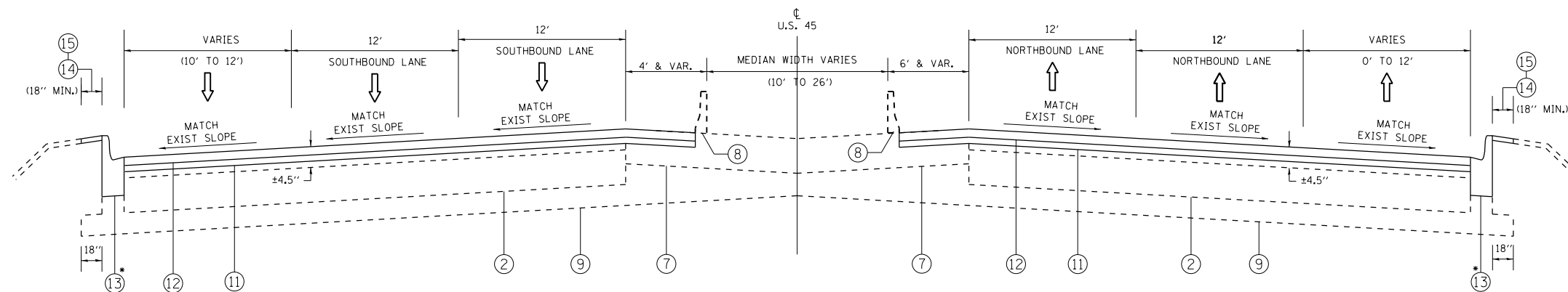
FILE NAME =	USER NAME = qureshiya	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S. ROUTE 45 EXISTING & PROPOSED TYPICAL SECTIONS		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\1\1084EBIDINTEG\Illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\1084EBIDINTEG\Design\1084EBIDINTEG\12-sht-plan.dgn	DRAWN	REVISED -	REVISED -				330	2011-200-RS	COOK	24	7
PLOT SCALE = 100.1299' / 1" =	CHECKED -	REVISED -	REVISED -		SHEET NO. 1 OF 1 SHEETS			STA.	TO STA.	CONTRACT NO. 60R42	
PLOT DATE = 1/30/2019	DATE -	REVISED -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						

LEGEND

- ① EXISTING HMA SURFACING, ± 4.50"
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- ⑭ PROPOSED TOPSOIL FURNISH AND PLACE, 4"
- ⑮ PROPOSED SODDING, SALT TOLERANT
- ⑯ PROPOSED HMA MEDIAN SURFACE REMOVAL, 2" (PAID AS MEDIAN REMOVAL PARTIAL DEPTH)
- ⑰ PROPOSED HMA SURFACE COURSE, MIX "D", 9.5, N50, 2"



US 45 (LA GRANGE RD.)
EXISTING TYPICAL SECTION
STA. 136+31 TO STA. 141+03



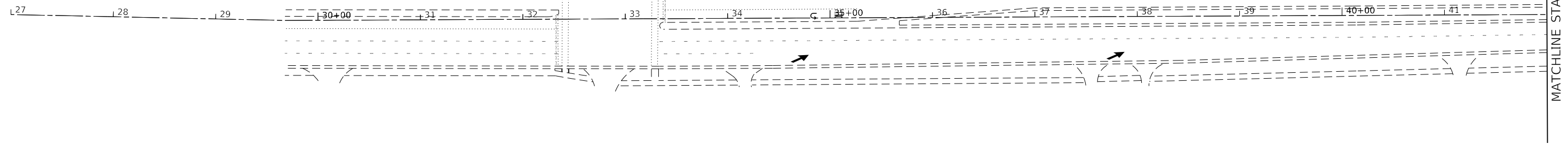
US 45 (LA GRANGE RD.)
PROPOSED TYPICAL SECTION
STA. 136+31 TO STA. 141+03

FILE NAME =	USER NAME = qureshiya	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S. ROUTE 45 EXISTING & PROPOSED TYPICAL SECTIONS	F.A.P. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
p:\jll084EBIDINTEG\Illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\DI08912\Design\DI08912-sht-plan.dgn						330	2011-200-R5	COOK	24	8	
PLOT SCALE = 100.2598' / in.						CONTRACT NO. 60R42					
PLOT DATE = 1/30/2019						SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



U.S. ROUTE 45
(LA GRANGE RD.)

W. CREEK RD.



MATCHLINE STA. 42+00.00

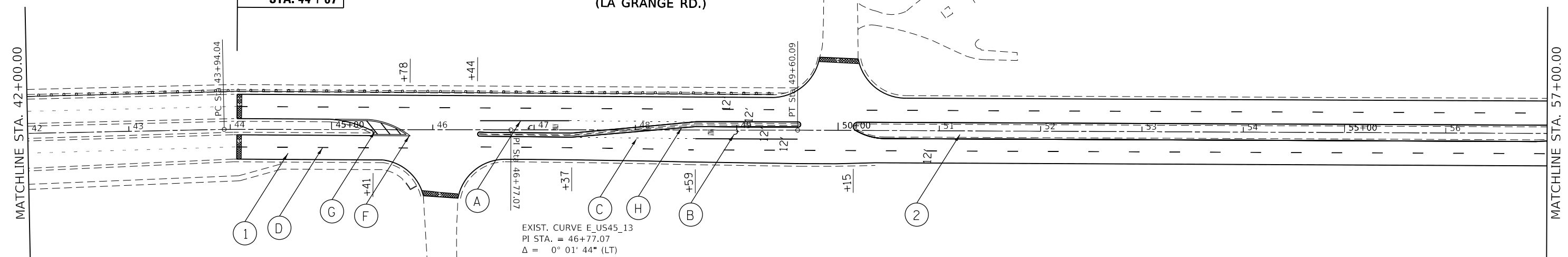
- ② PROP. MEDIAN SURFACE REMOVAL, 2" (PAID AS MEDIAN REMOVAL PARTIAL DEPTH)
PROP. HMA SURFACE COURSE, MIX "D", 9.5, N50, 2"
- ① PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2 "
PROP. POLY. HMA SURFACE COURSE, SMA, 9.5, N80, 1 3/4 "
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U.S. ROUTE 45
(LA GRANGE RD.)

WILD CHERRY LN.

PROJECT BEGINS
STA. 44+07



MATCHLINE STA. 57+00.00

EXIST. CURVE E_US45_13
 PI STA. = 46+77.07
 $\Delta = 0^\circ 01' 44''$ (LT)
 $D = 0^\circ 00' 18''$
 $R = 1,126,079.05'$
 $T = 283.02'$
 $L = 566.05'$
 $E = 0.04'$
 $e = ______$
 $T.R. = ______$
 $S.E. RUN = ______$
 $P.C. STA. = 43+94.04$
 $P.T. STA. = 49+60.09$

HMA SURFACE REMOVAL
 BUTT JOINT, 4.5'

NOTE: PAVEMENT MARKINGS SHALL BE INSTALLED ACCORDING TO IDOT'S D1 DETAIL TC-13.
 RAISED REFLECTIVE PAVEMENT PAVEMENT MARKERS SHALL BE INSTALLED ACCORDING TO IDOT'S D1 DETAIL TC-11.

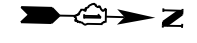
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USER NAME = qureshya	DESIGNED -	REVISED -
	DRAWN -	REVISED -
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PLOT DATE = 1/30/2019	DATE -	REVISED -

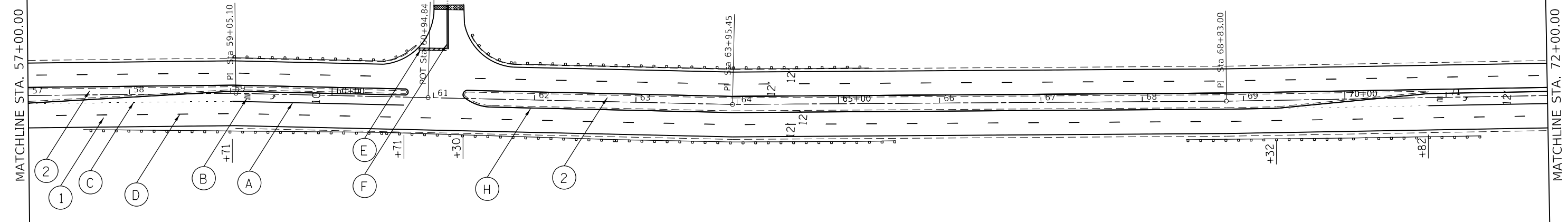
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN			
U.S. ROUTE 45 (NORTH OF IL. ROUTE 83 TO WEST CREEK ROAD)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	2011-200-R5	COOK	24	9
CONTRACT NO. 60R42				
ILLINOIS FED. AID PROJECT				



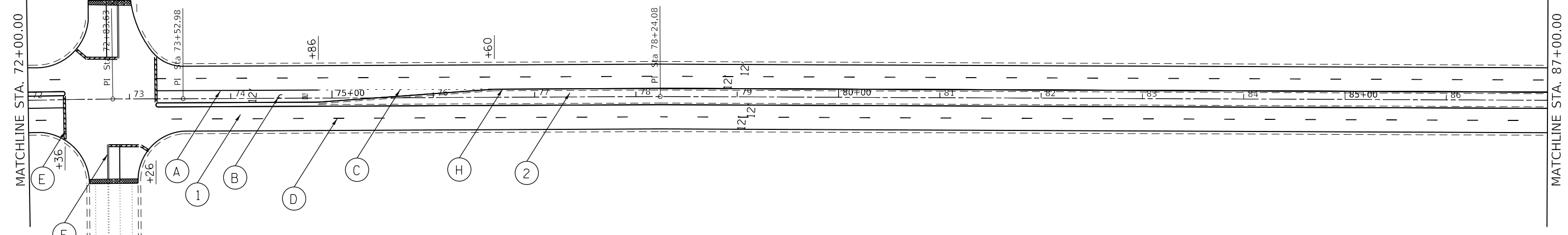
U.S. ROUTE 45
(LA GRANGE RD.)



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U.S. ROUTE 45
(LA GRANGE RD.)



HMA SURFACE REMOVAL BUTT JOINT, 4.5'

NOTE: PAVEMENT MARKINGS SHALL BE INSTALLED ACCORDING TO IDOT'S D1 DETAIL TC-13.
RAISED REFLECTIVE PAVEMENT PAVEMENT MARKERS SHALL BE INSTALLED ACCORDING TO IDOT'S D1 DETAIL TC-11.

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USER NAME = qureshiya	DESIGNED -	REVISED -
PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 1/30/2019	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN
U.S. ROUTE 45 (NORTH OF IL. ROUTE 83 TO WEST CREEK ROAD)
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	2011-200-R5	COOK	24	10
CONTRACT NO. 60R42				
ILLINOIS FED. AID PROJECT				

U.S. ROUTE 45
(LA GRANGE RD.)

FOREST PRESERVE ENT.

BIKE PATH UNDERPASS

119TH ST.

MATCHLINE STA. 87+00.00

MATCHLINE STA. 102+00.00

- ② PROP. MEDIAN SURFACE REMOVAL, 2" (PAID AS MEDIAN REMOVAL PARTIAL DEPTH)
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$R = 2,453.33'$

U.S. ROUTE 45
(LA GRANGE RD.)

MATCHLINE STA. 102+00.00

MATCHLINE STA. 117+00.00

HMA SURFACE REMOVAL BUTT JOINT, 4.5'

NOTE: PAVEMENT MARKINGS SHALL BE INSTALLED ACCORDING TO IDOT'S D1 DETAIL TC-13.
RAISED REFLECTIVE PAVEMENT PAVEMENT MARKERS SHALL BE INSTALLED ACCORDING TO IDOT'S D1 DETAIL TC-11.

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PLOT DATE = 1/30/2019	CHECKED -	REVISED -
	DATE -	REVISED -

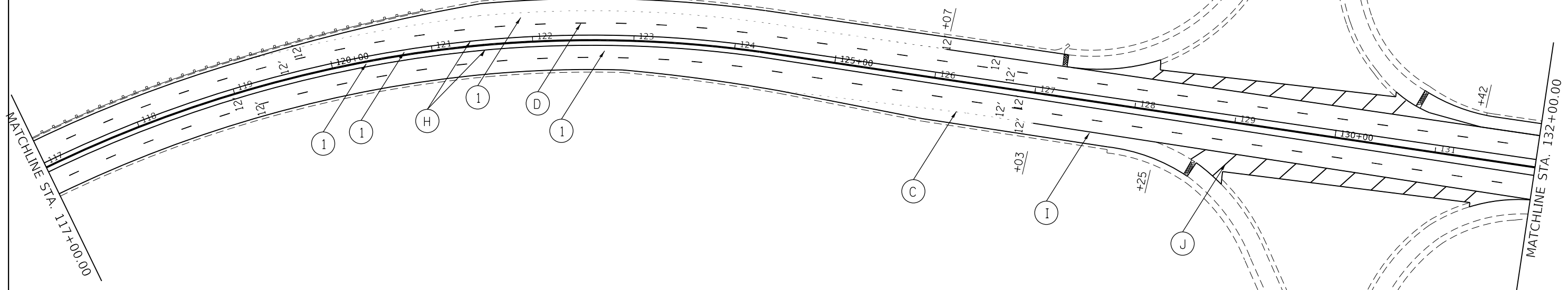
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN
U.S. ROUTE 45 (NORTH OF IL. ROUTE 83 TO WEST CREEK ROAD)

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	2011-200-R5	COOK	24	11
CONTRACT NO. 60R42				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.

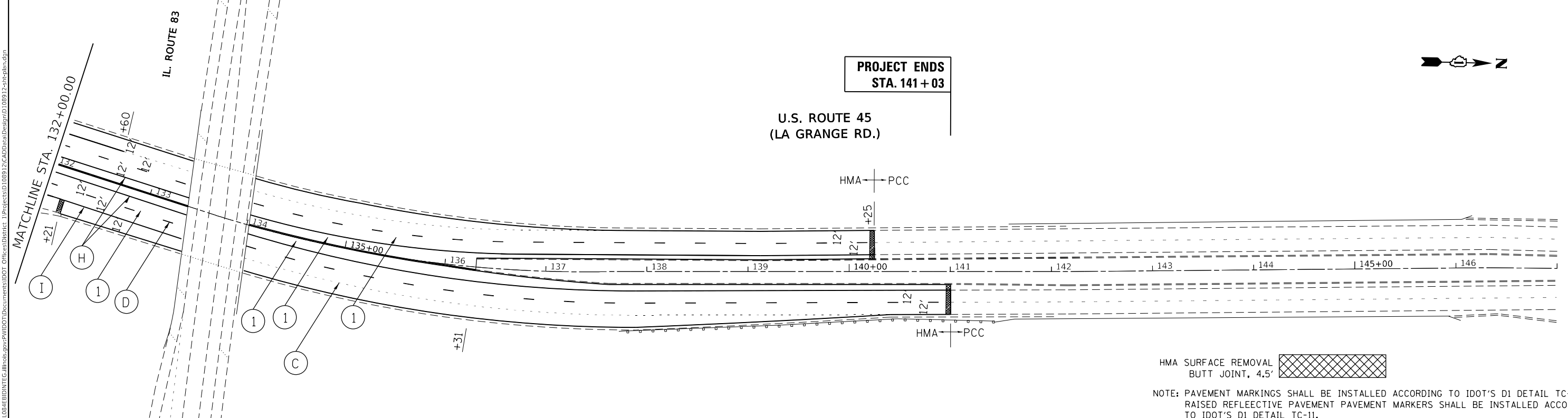
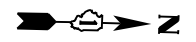
U.S. ROUTE 45
(LA GRANGE RD.)



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**PROJECT ENDS
STA. 141 + 03**

U.S. ROUTE 45
(LA GRANGE RD.)



HMA SURFACE REMOVAL
BUTT JOINT, 4.5'

NOTE: PAVEMENT MARKINGS SHALL BE INSTALLED ACCORDING TO IDOT'S D1 DETAIL TC-13.
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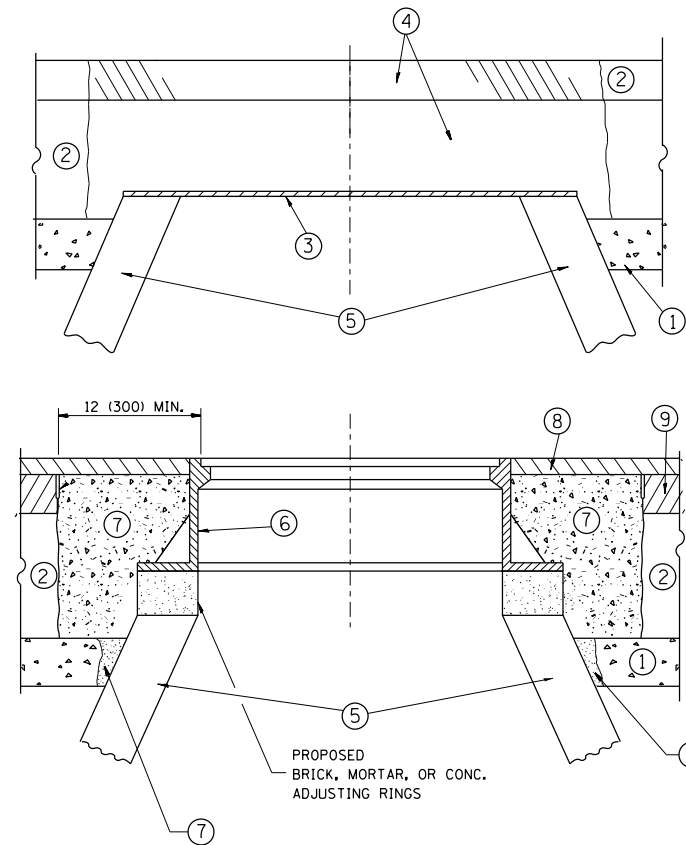
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USER NAME = qureshiya	DESIGNED -	REVISED -
DRAWN -	REVISED -	
PLOT SCALE = 100.1299' / in.	CHECKED -	REVISED -
PLOT DATE = 1/30/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY PLAN
U.S. ROUTE 45 (NORTH OF IL. ROUTE 83 TO WEST CREEK ROAD)
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	2011-200-RS	COOK	24	12
CONTRACT NO. 60R42				
ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

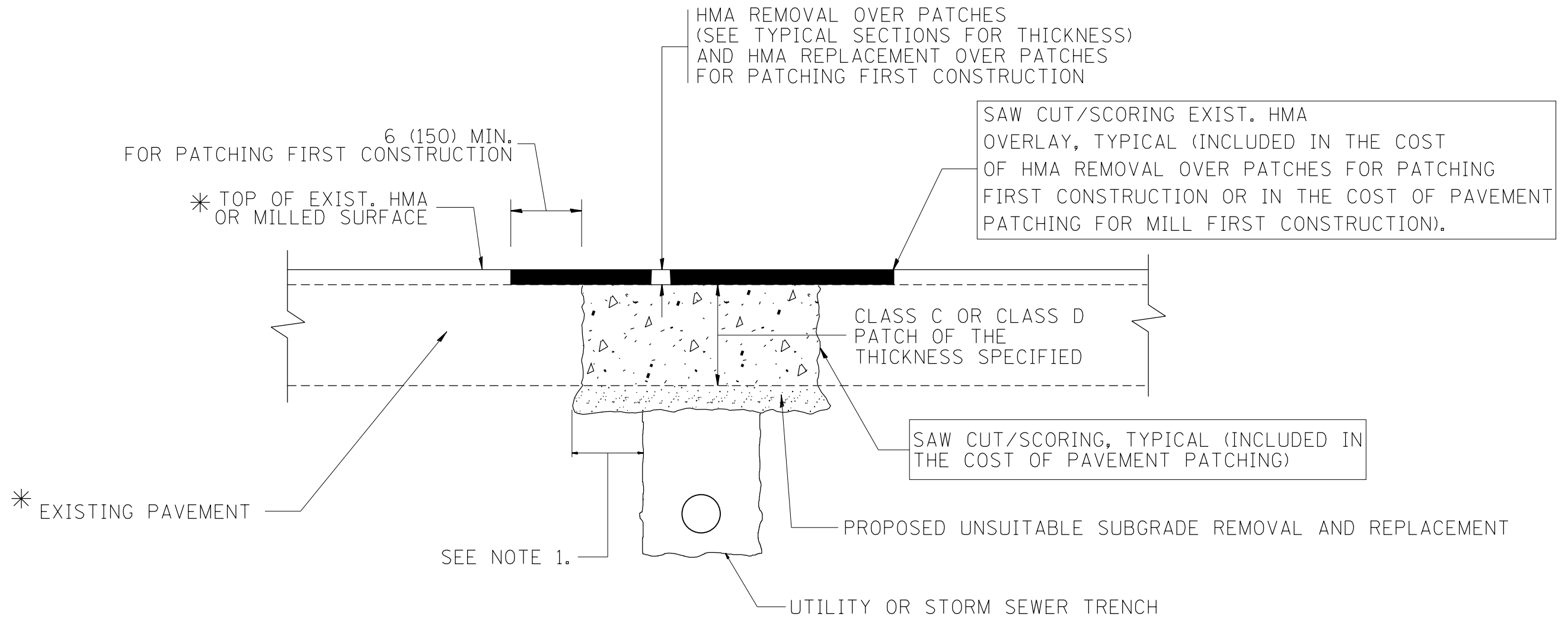
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = qureshiye	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
pw\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI084EBIDINTEG\Design\DistStd.dgn			REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 1/30/2019	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	2011-200-RS	COOK	24	14
BD600-03 (BD-8)		CONTRACT NO. 60R42		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = qureshiye	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI084EBIDINTEG\Design\DistStd.dgn	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 01-01-07					330	2011-200-RS	COOK	24	15
	PLOT DATE = 1/30/2019	DATE - 10-25-94	REVISED - R. BORO 09-04-07		BD400-04 (BD-22)			CONTRACT NO. 60R42				
			REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

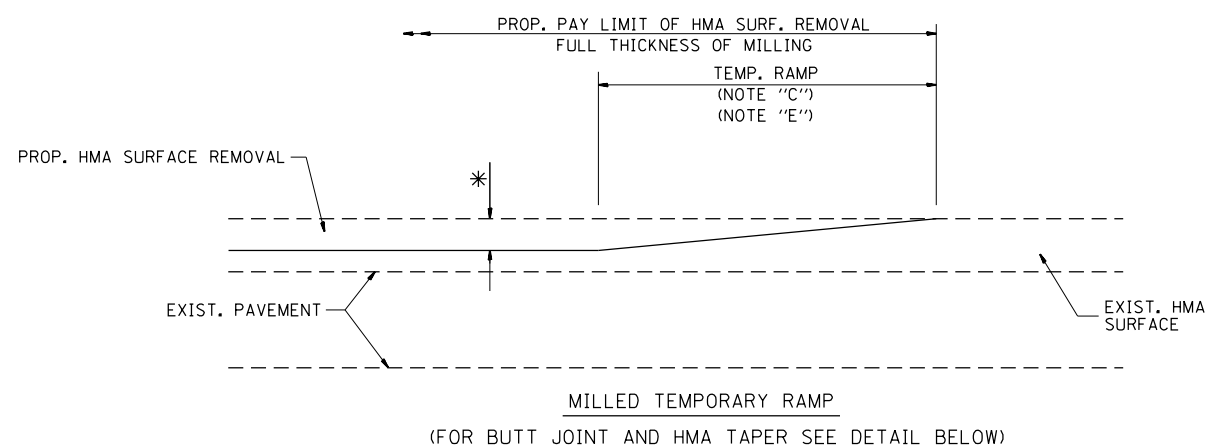
PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

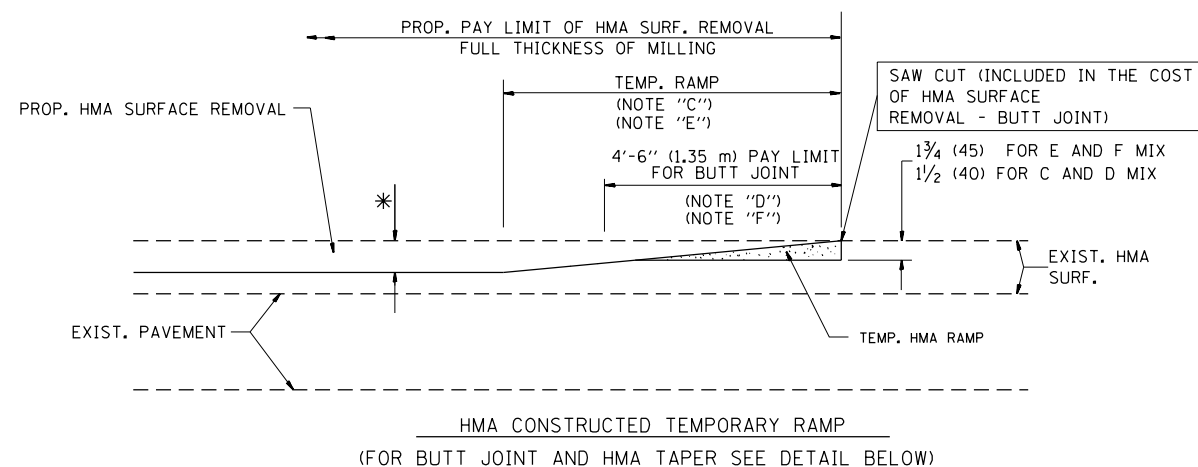
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = qureshiye	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI084EBID\Design\DistStd.dgn		REVISED - A. ABBAS 03-21-97	REVISED - M. GOMEZ 01-22-01			330	2011-200-RS	COOK	24	16
PLOT SCALE = 100.0000' / 1".	CHECKED -	REVISED - R. BORO 12-15-09				BD600-06 (BD-24)		CONTRACT NO. 60R42		
PLOT DATE = 1/30/2019	DATE - 03-11-94					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

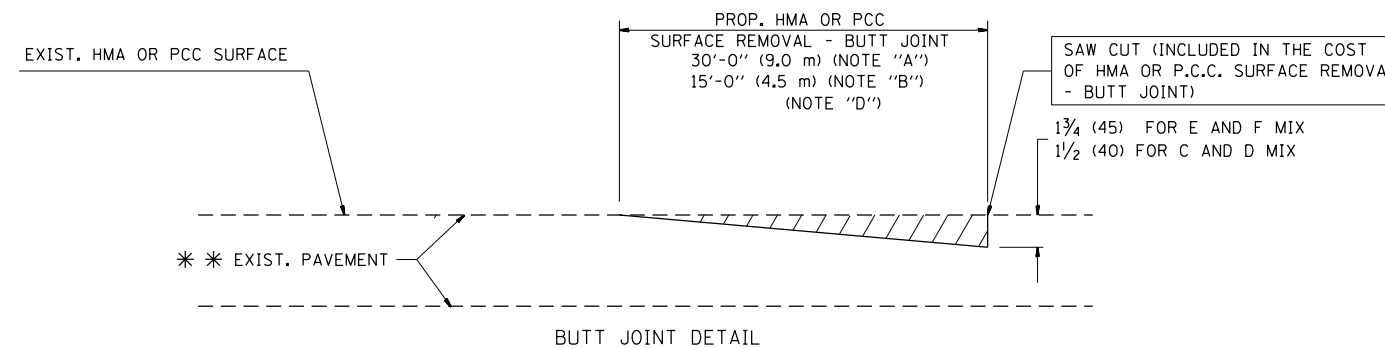


OPTION 1

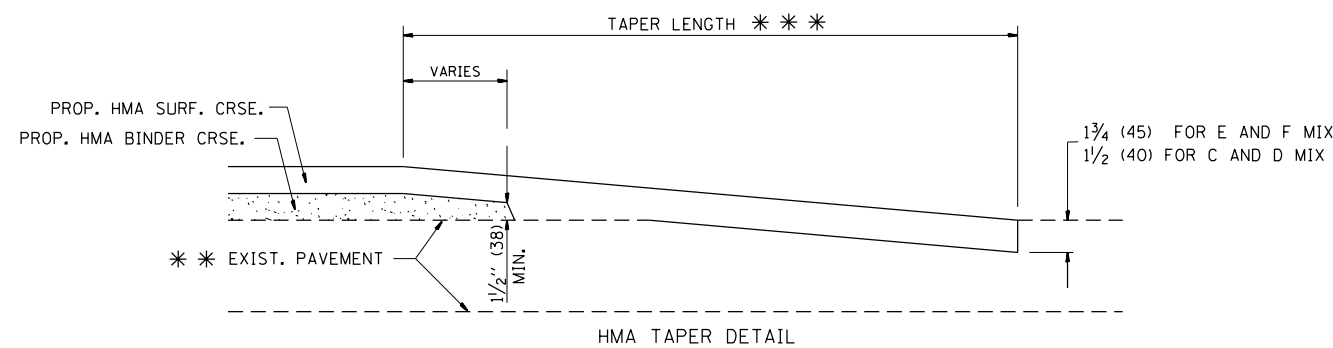


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

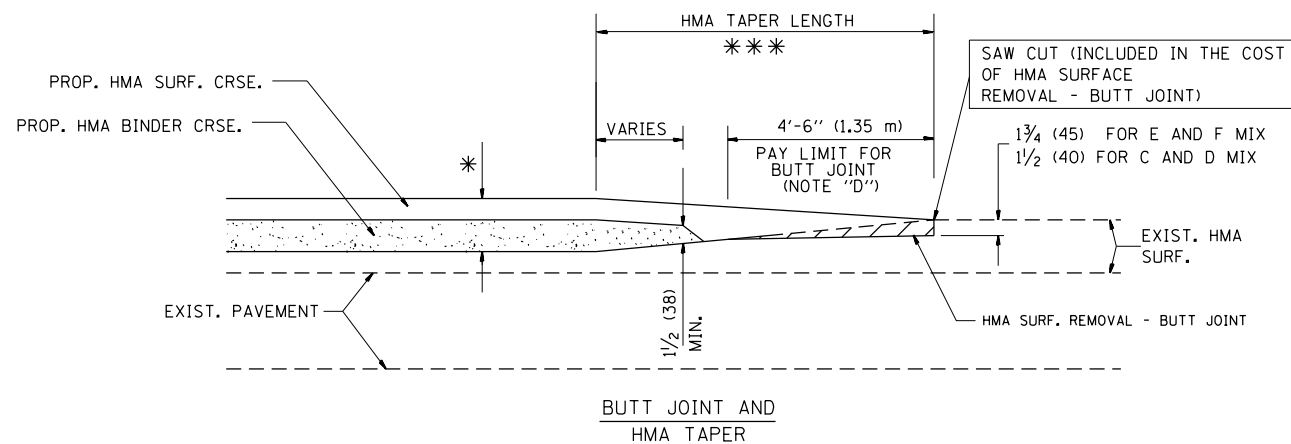
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- * * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

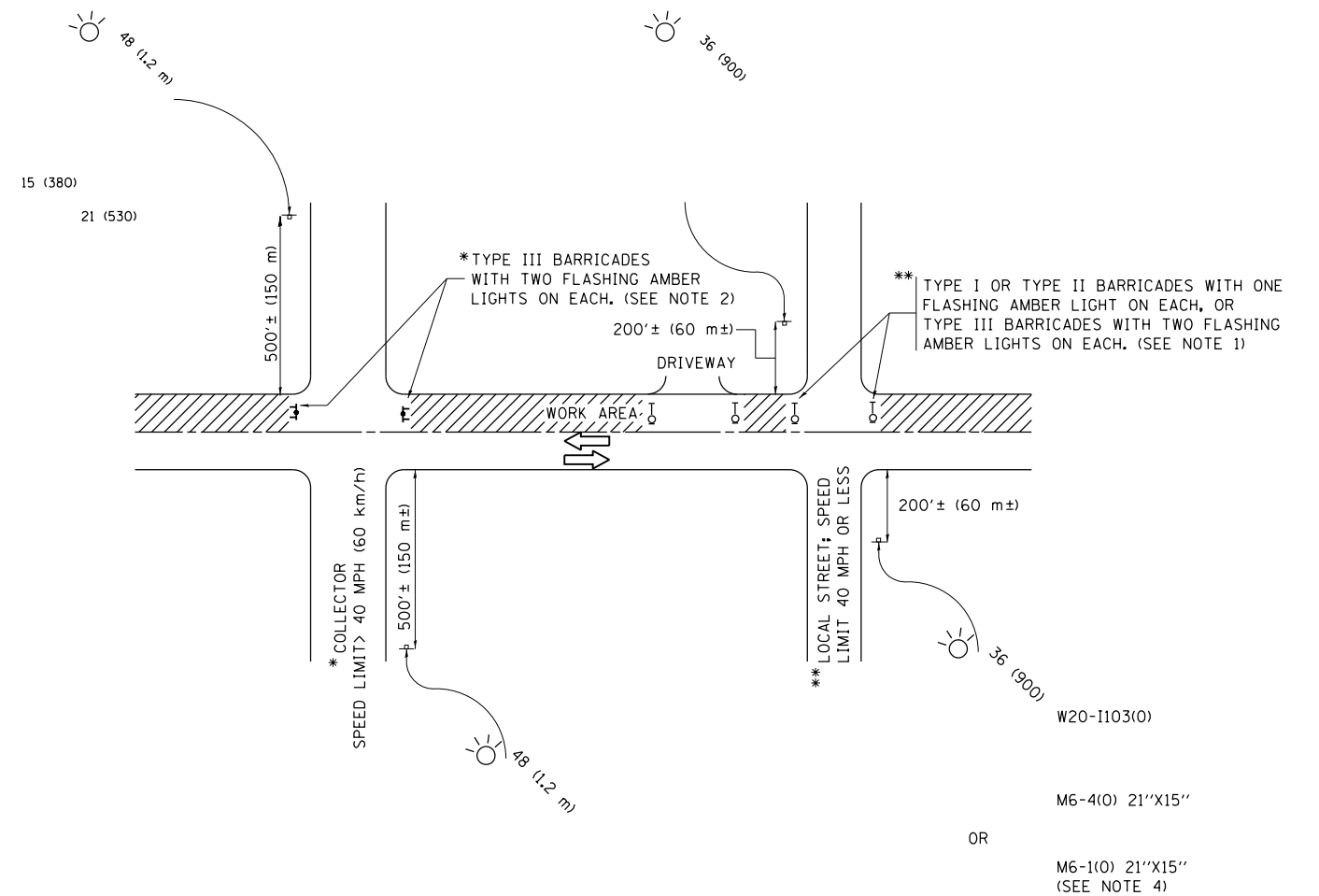
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p:\11084EBID\INTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI089\Drawings\Design\DistStd.dgn			REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/30/2019	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	2011-200-RS	COOK	24	17
BD400-05 BD32		CONTRACT NO. 60R42		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

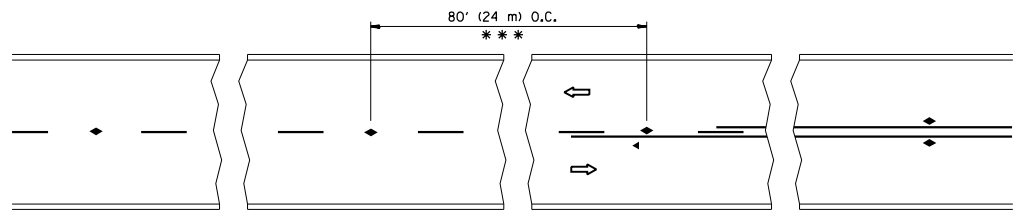
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pw:\IL\084EBIDINTEG\Illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\DI089\084EBIDINTEG\DistStd.dgn			REVISED - T. RAMMACHER 01-06-00
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 1/30/2019	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

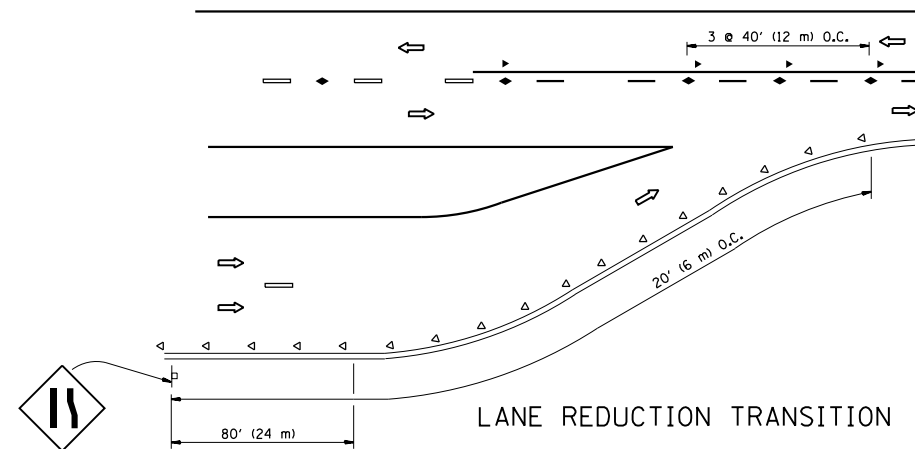
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 60R42	
ILLINOIS FED. AID PROJECT				

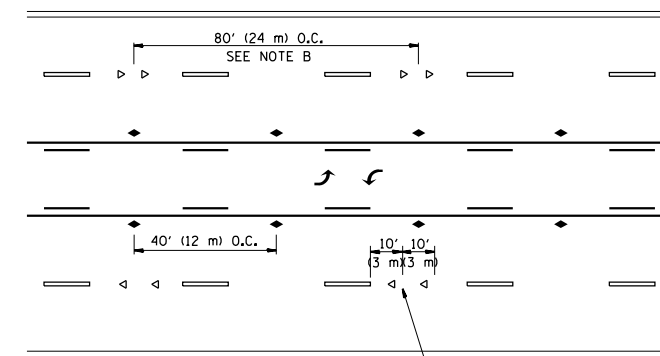


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

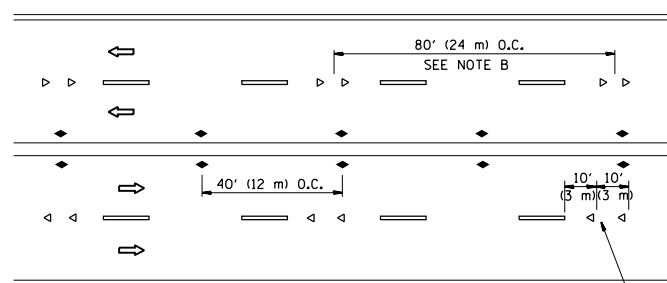
TWO-LANE/TWO-WAY



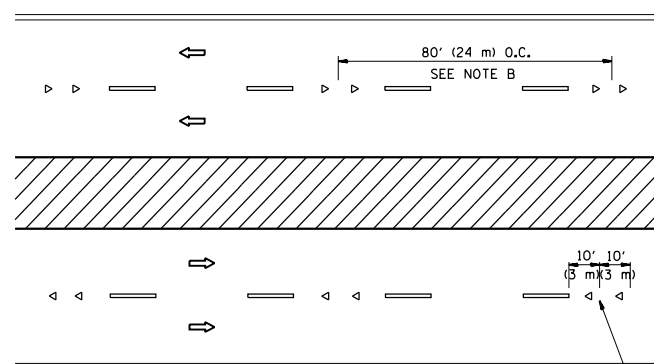
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

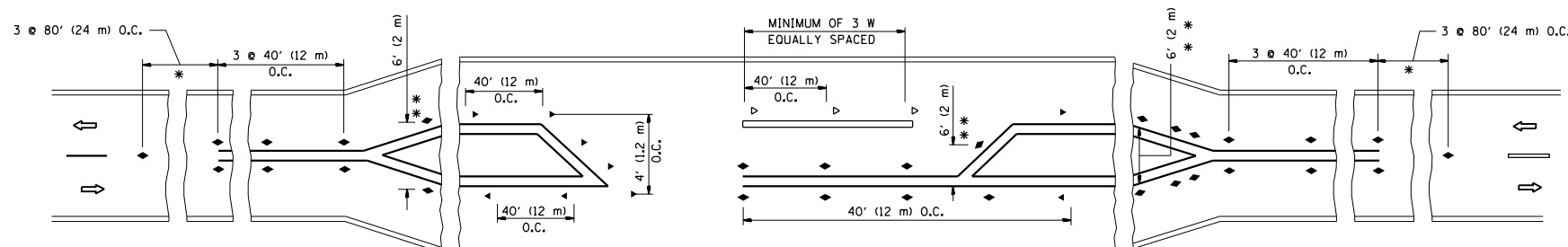
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

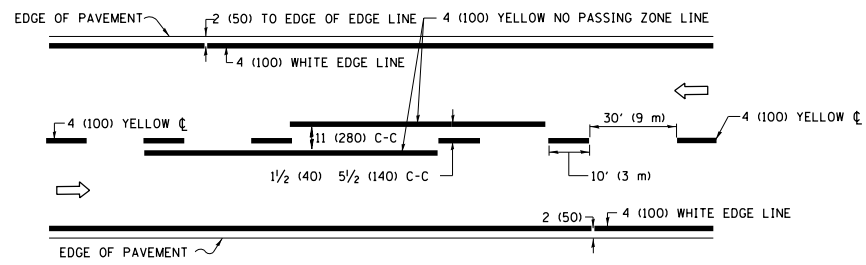
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = qureshiye	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
pw:\IL\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI084EBIDINTEG\Design\DistStd.dgn		CHECKED -	REVISED - T. RAMMACHER 03-12-99
		DATE -	REVISED - T. RAMMACHER 01-06-00
			REVISED - C. JUCIUS 09-09-09

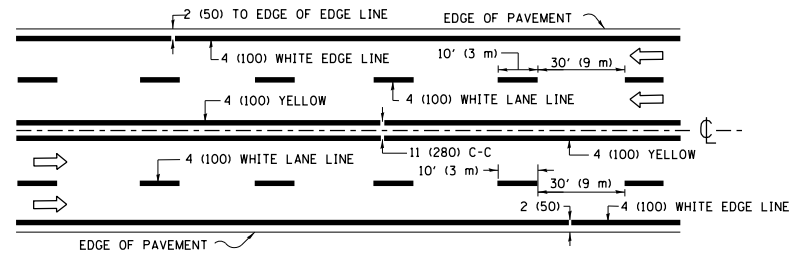
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

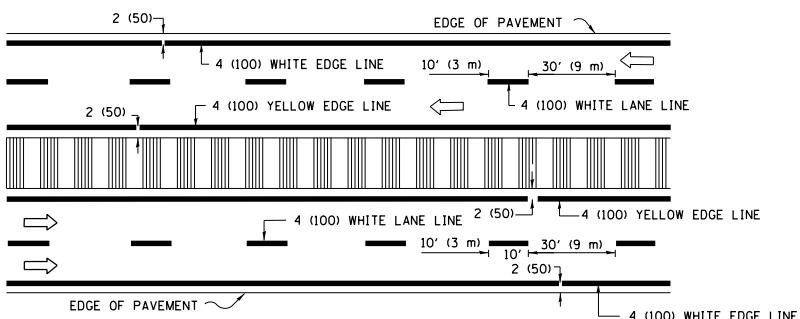
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	2011-200-RS	COOK	24	19
TC-11			CONTRACT NO. 60R42	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

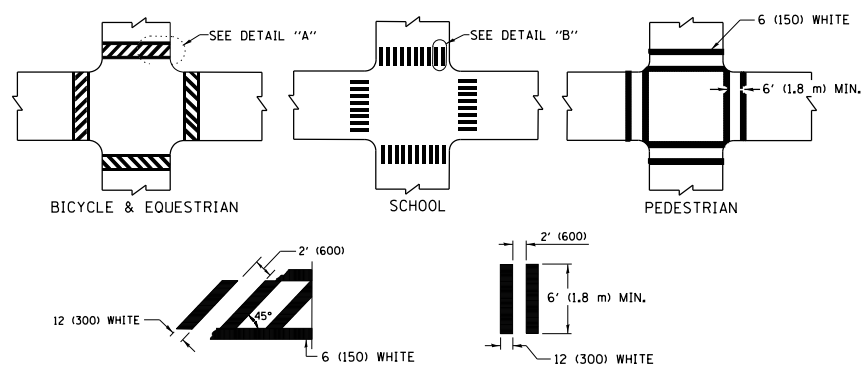


MULTI-LANE UNDIVIDED



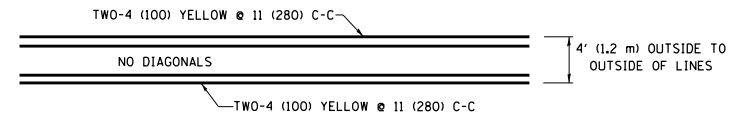
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

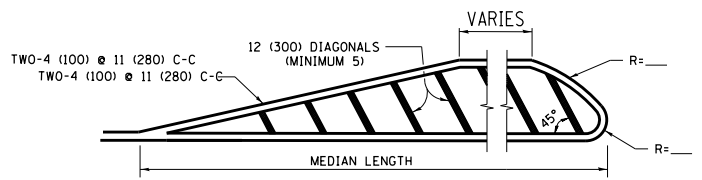


TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

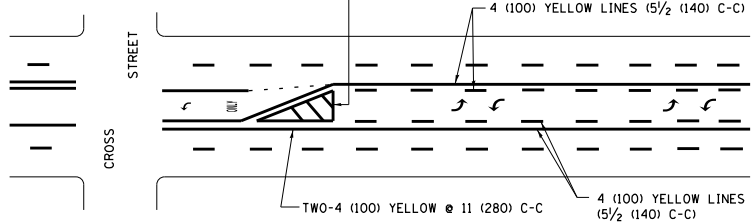


4' (1.2 m) WIDE MEDIANS ONLY

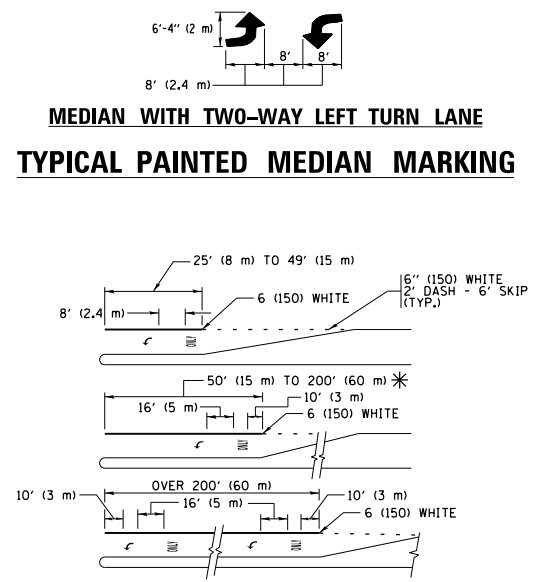


MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

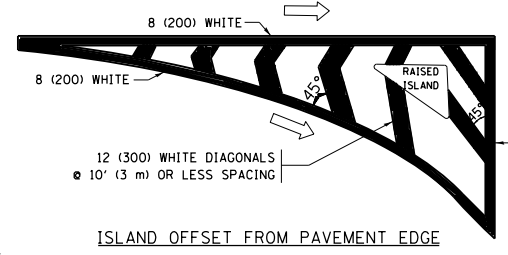


**MEDIAN WITH TWO-WAY LEFT TURN LANE
TYPICAL PAINTED MEDIAN MARKING**

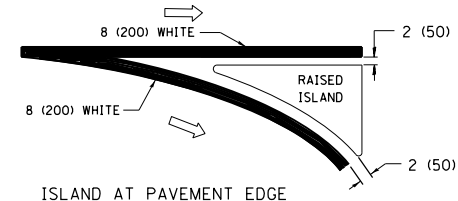


FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

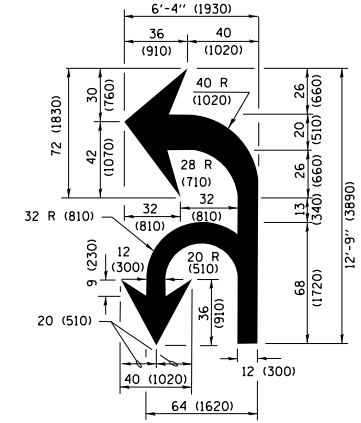
**TYPICAL LEFT (OR RIGHT) TURN LANE
TYPICAL TURN LANE MARKING**



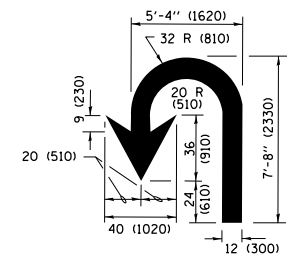
ISLAND OFFSET FROM PAVEMENT EDGE



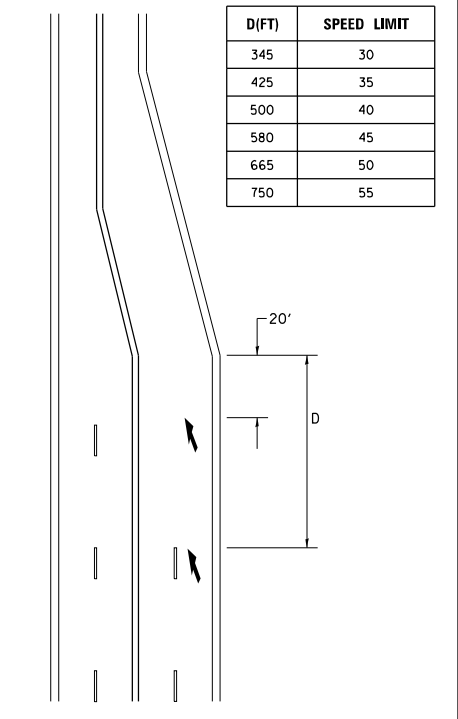
**ISLAND AT PAVEMENT EDGE
TYPICAL ISLAND MARKING**



COMBINATION LEFT AND U-TURN



U-TURN



LANE REDUCTION TRANSITION
* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = qureshiye	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
pw\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\01084EBIDINTEG\Design\DistStd.dgn		CHECKED -	REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 100.0000' / in.	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 1/30/2019		REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	2011-200-RS	COOK	24	20
TC-13		CONTRACT NO.	60R42	
ILLINOIS FED. AID PROJECT				

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

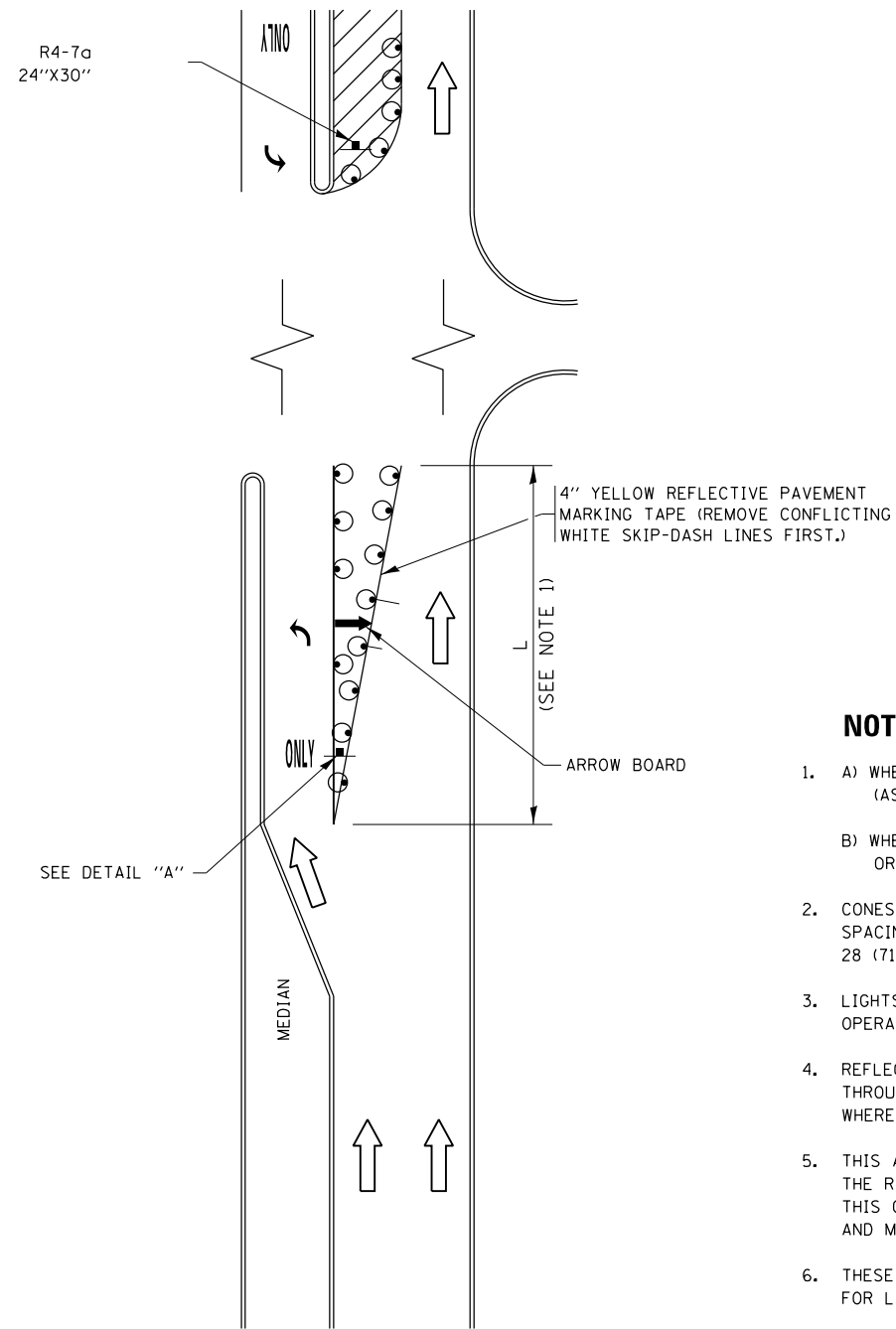
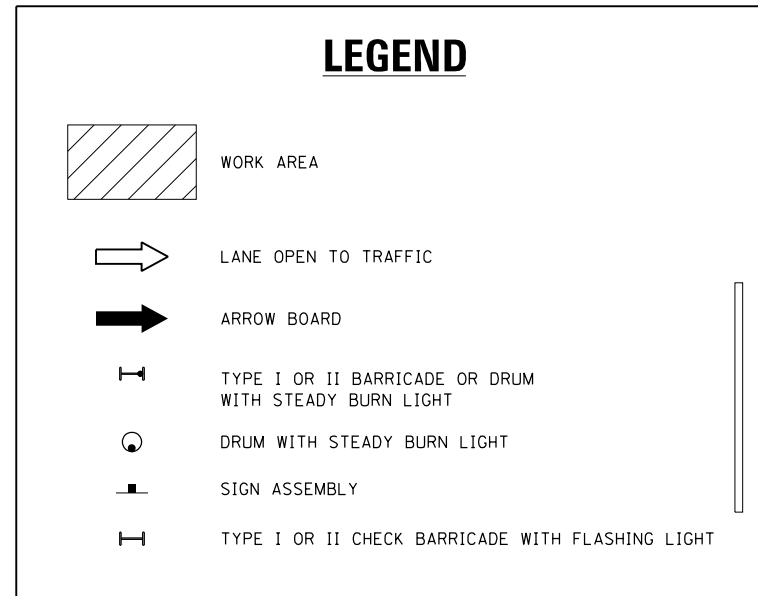


FIGURE 1



NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
 B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

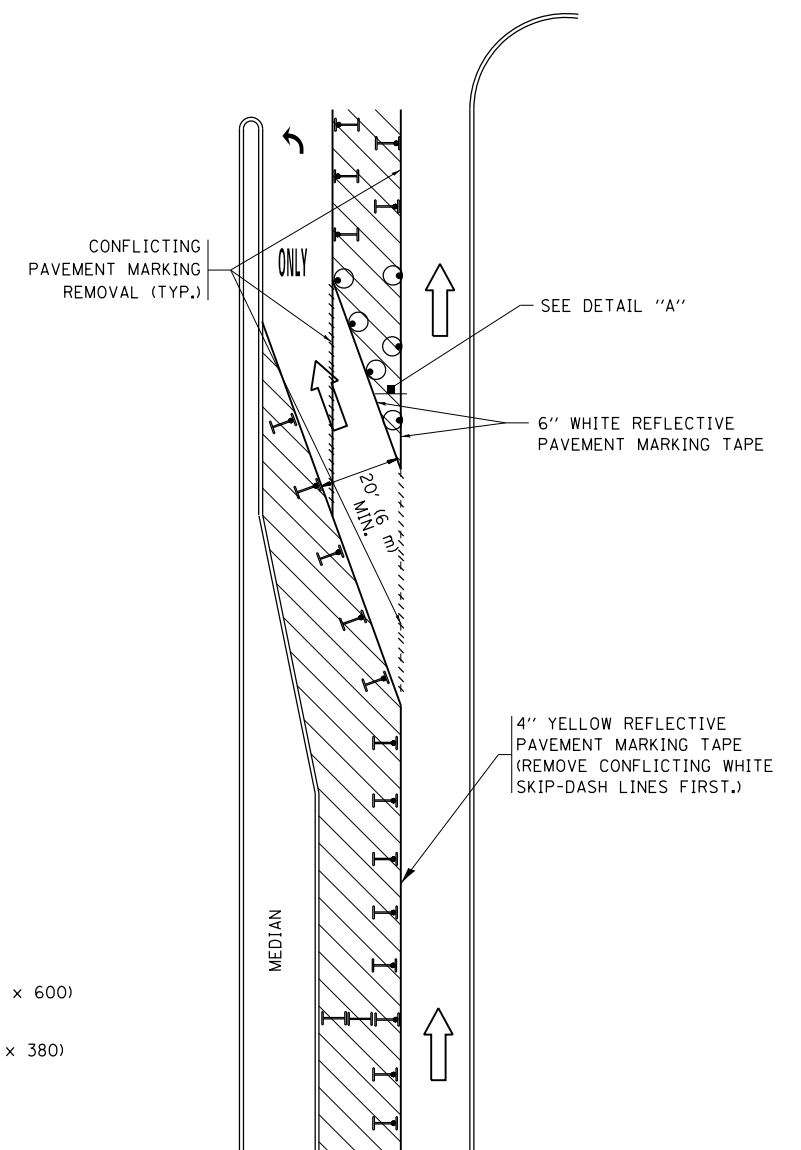
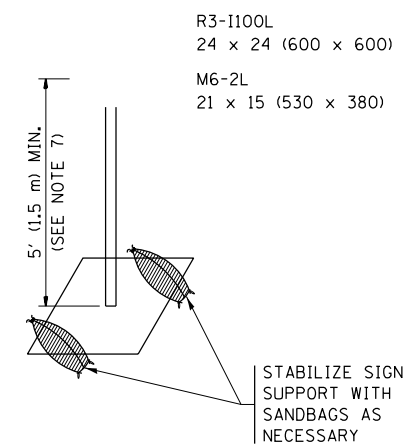


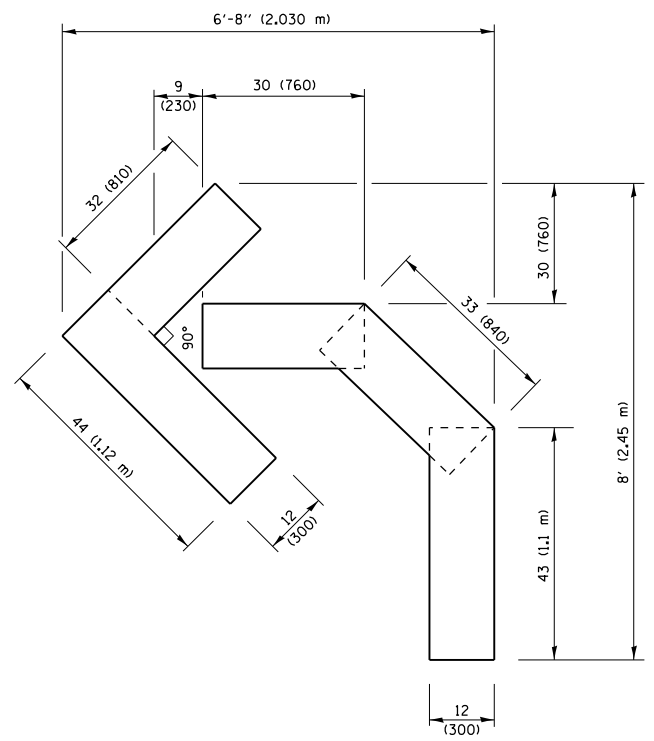
FIGURE 2



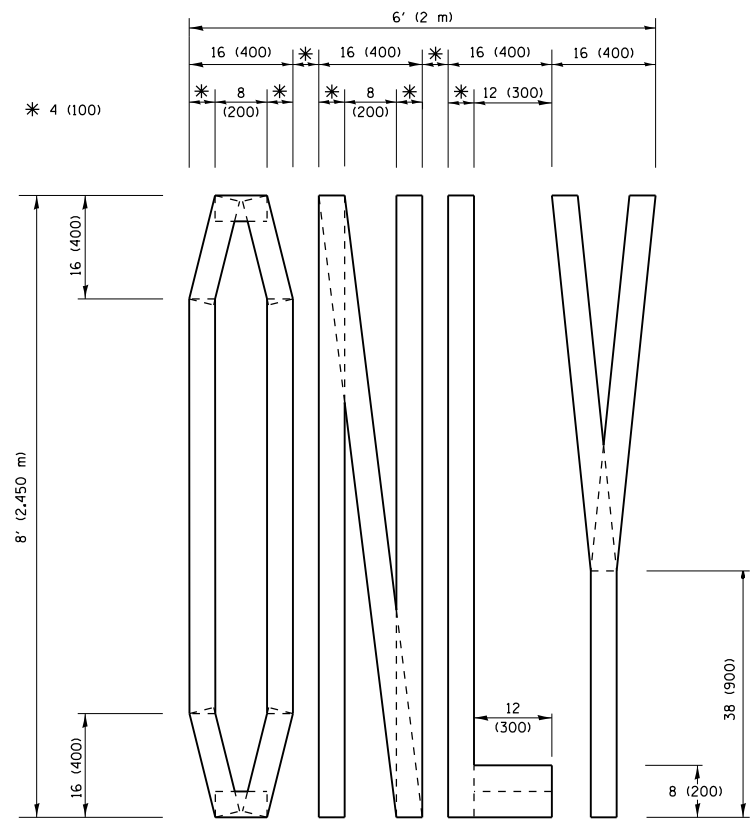
DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

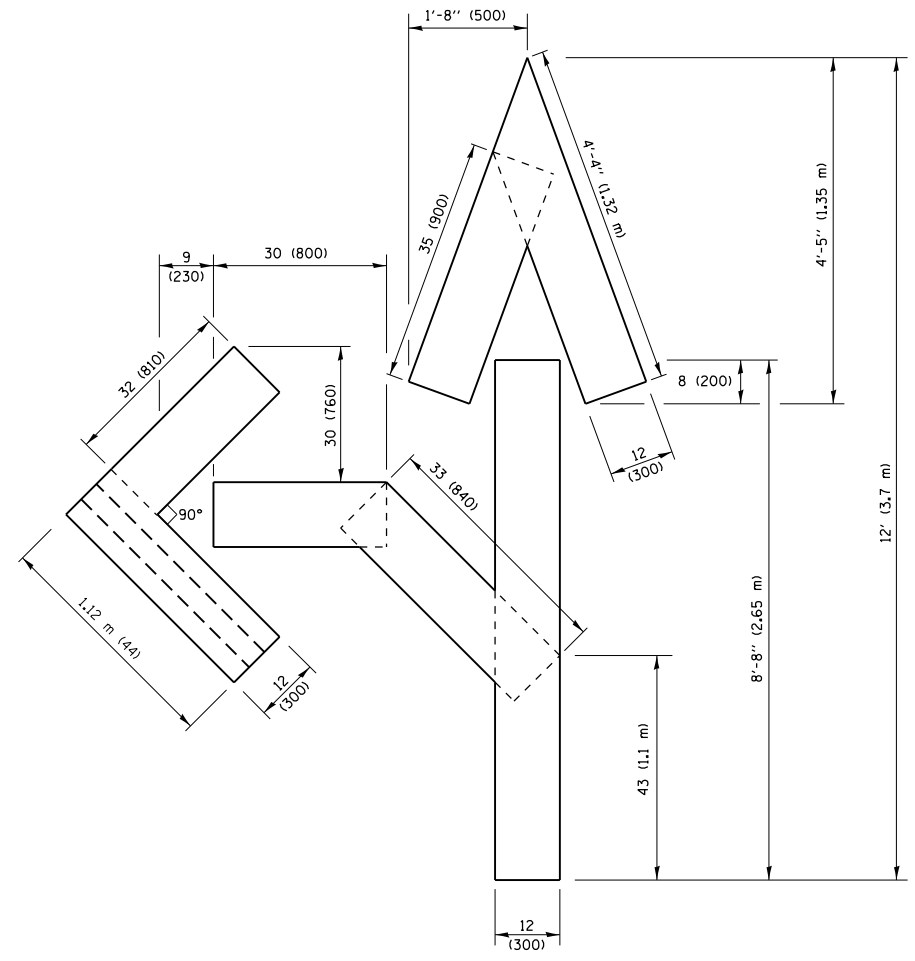
FILE NAME =	USER NAME = qureshiye	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)			F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default		REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13					330	2011-200-RS	COOK	24	21
	PLOT SCALE = 100.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16		TC-14			CONTRACT NO. 60R42				
	PLOT DATE = 1/30/2019	REVISED - T. RAMMACHER 01-06-00	REVISED -		SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

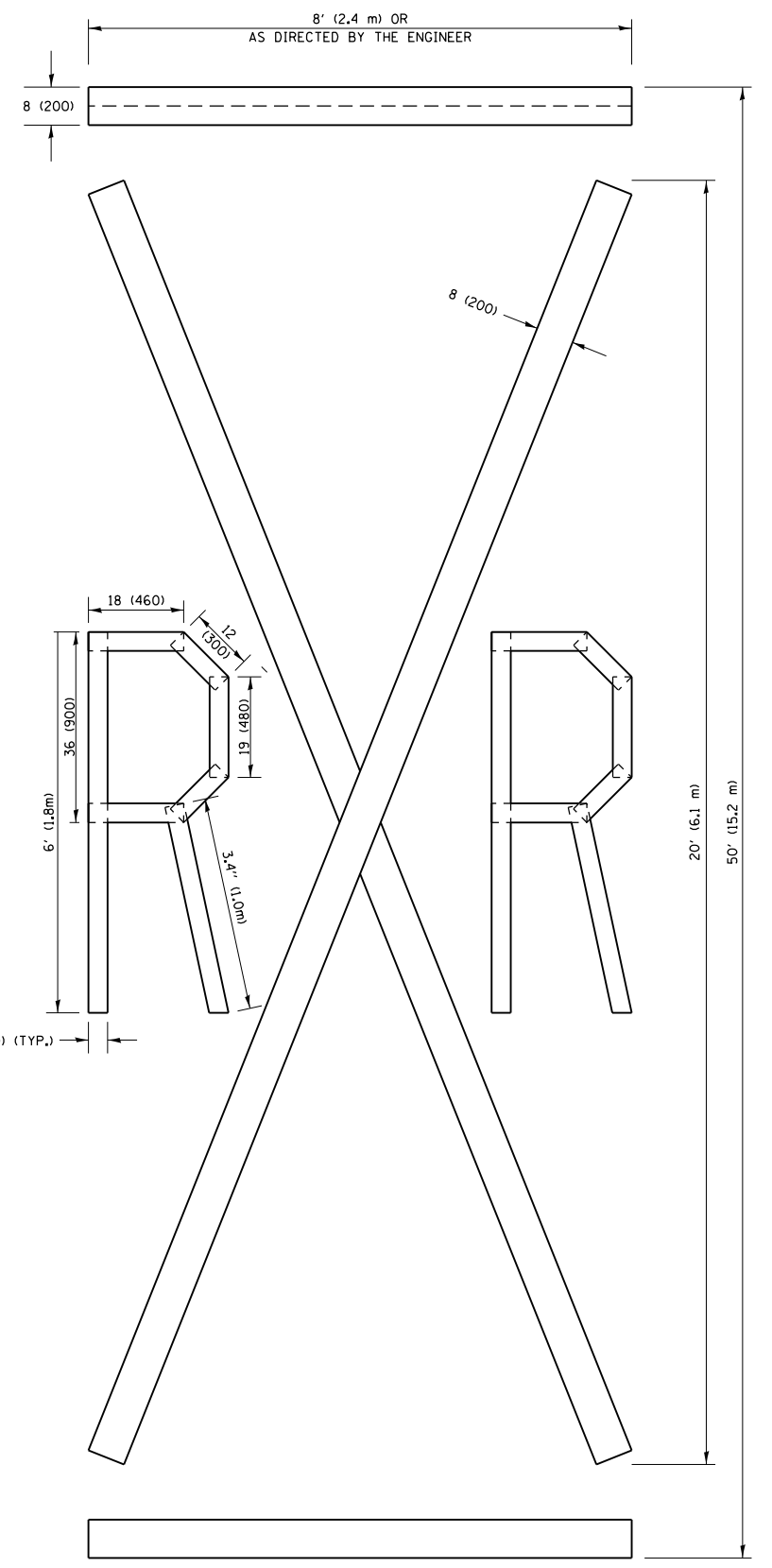


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

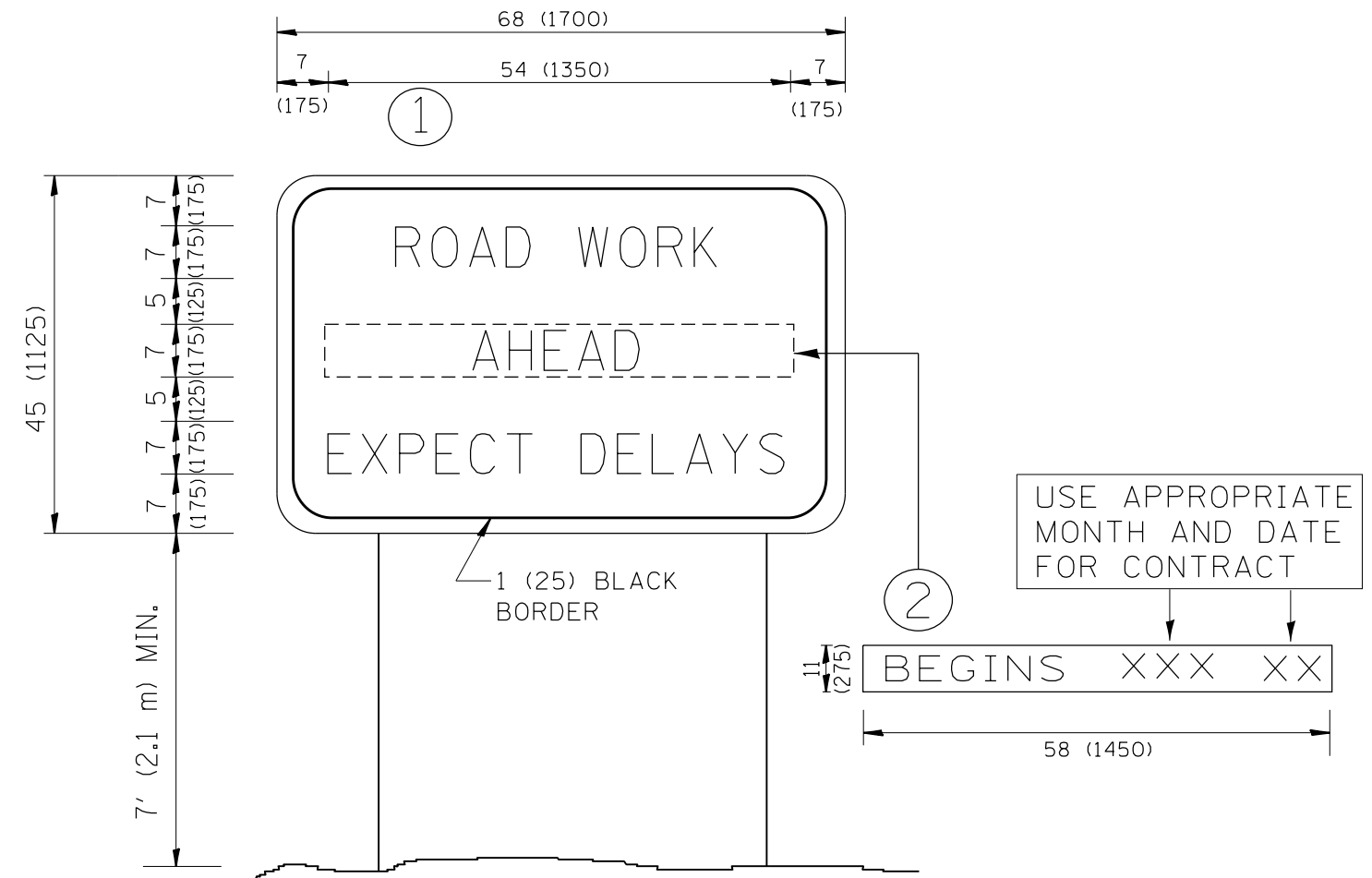
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = qureshiya	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
pw\11084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI189\Drawings\Design\DistStd.dgn		CHECKED -	REVISED - E. GOMEZ 08-28-00
		DATE -	REVISED - E. GOMEZ 08-28-00
			REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	2011-200-RS	COOK	24	22
TC-16		CONTRACT NO. 60R42		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

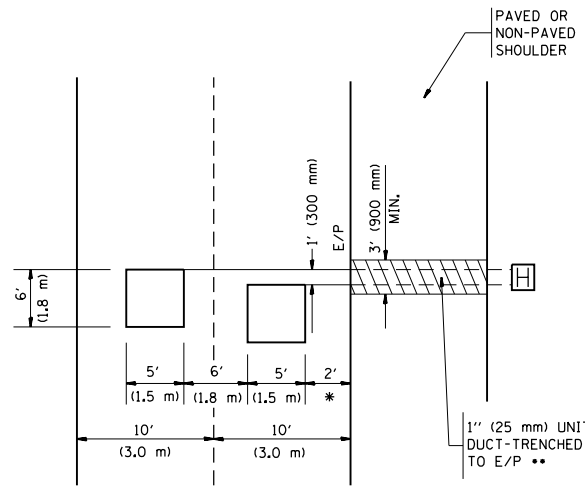
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = qureshiye	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI084EBIDINTEG\Design\DistStd.dgn	DRAWN	REVISED - R. MIRS 12-11-97	330			2011-200-RS	COOK	24	23	
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99	TC-22			CONTRACT NO. 60R42				
PLOT DATE = 1/30/2019	DATE -	REVISED - C. JUCIUS 01-31-07	SCALE: NONE			SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



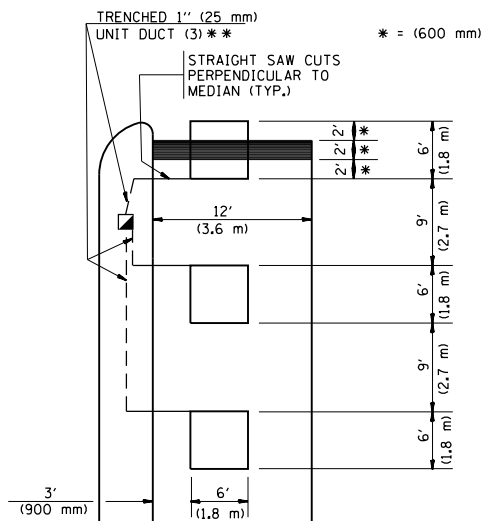
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

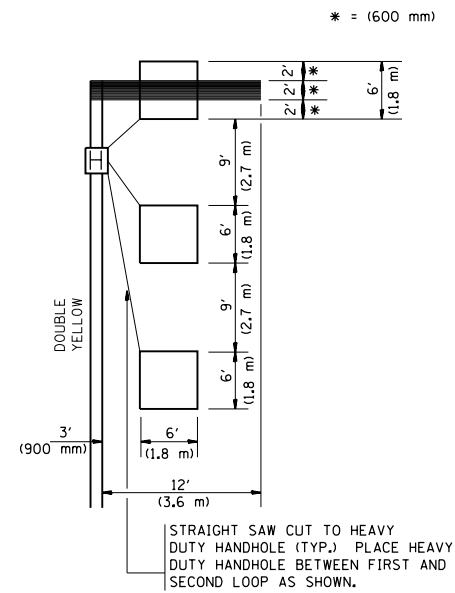


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

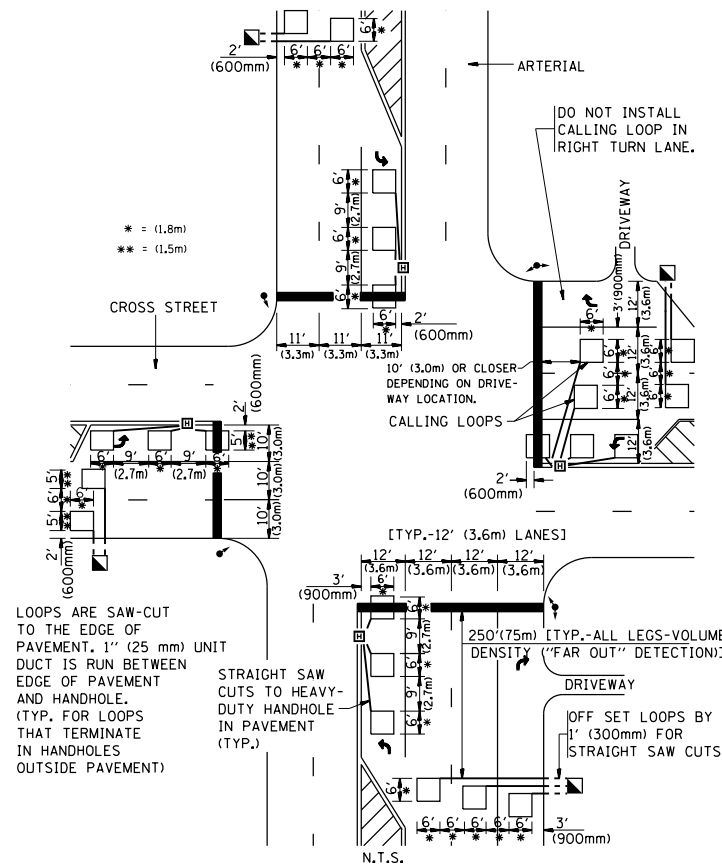
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

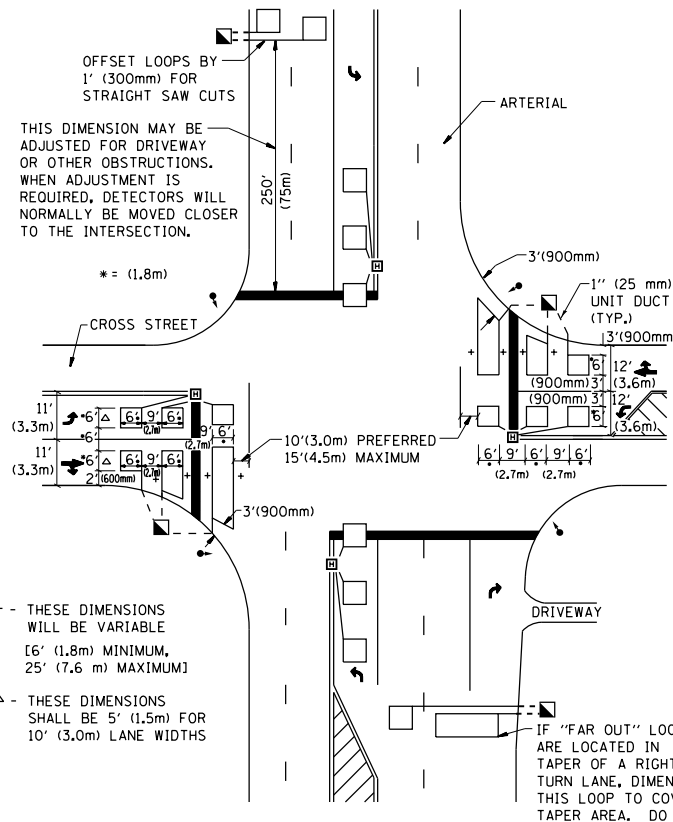
THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2
N.T.S.**

FILE NAME =	USER NAME = qureshiye	DESIGNED -	REVISED -
pw:\IL\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI084EBID\Design\DistStd.dgn		CHECKED - R.K.F.	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	2011-200-RS	COOK	24	24
TS-07		CONTRACT NO. 60R42		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				