

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

|               |                |          |                     |           |
|---------------|----------------|----------|---------------------|-----------|
| F.A.P. R.T.E. | SECTION        | COUNTY   | TOTAL SHEETS        | SHEET NO. |
| 870           | 2018-029-RS-SW | DUPAGE   | 51                  | 1         |
|               |                | ILLINOIS | CONTRACT NO.: 62G23 |           |

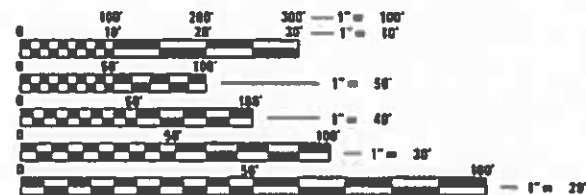
D-91-267-18



FOR INDEX OF SHEETS, SEE SHEET NO. 2

**PROPOSED HIGHWAY PLANS**  
**F.A.P. 870 /IL 53 (LINCOLN AVE.)**  
**I-88 (REAGAN MEMORIAL TOLLWAY) TO 59TH ST.**  
**RESURFACING (3P) AND**  
**ADA PEDESTRIAN RAMPS**  
**SECTION NO.: 2018-029-RS-SW**  
**PROJECT: NHPP-GRR3 (253)**  
**DUPAGE COUNTY**  
**C-91-164-18**

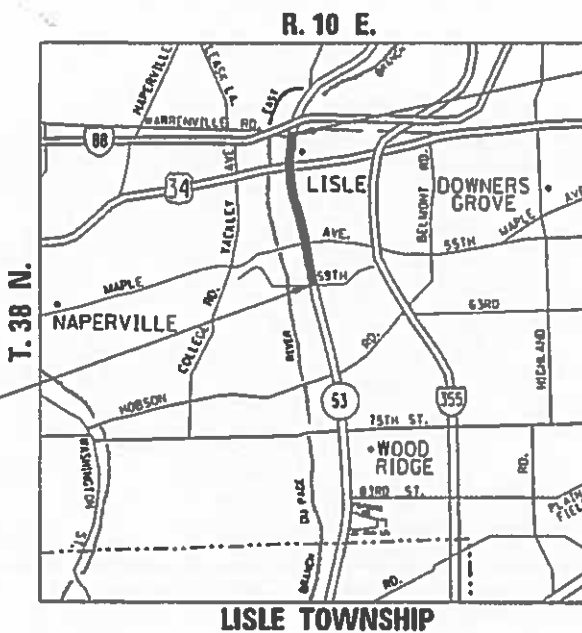
PROJECT LOCATED IN THE VILLAGES OF LISLE



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT BEGINS  
STA. 10 + 99



PROJECT ENDS  
STA. 132 + 06

**TRAFFIC DATA:**  
2017 ADT - 25700 TO 31100  
SPEED LIMIT - 35 TO 45 MPH

PROJECT ENGINEER: VESELIN V. VELICHKOV (847) 705-4432  
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

GROSS AND NET LENGTH OF PROJECT - 12107 FEET - 2.29 MILES

CONTRACT NO. 62G23

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED January 24, 2019

*Anthony J. D'Amico* / *MS*  
REGION ENGINEER

*March 23, 2019*  
E.A. ETK  
ENGINEER OF DESIGN AND ENVIRONMENT

*March 23, 2019*  
*David P. Chaf*  
DIRECTOR OF HIGHWAY PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

| INDEX OF SHEETS |   | STATE STANDARDS |   | GENERAL NOTES:   |  |
|-----------------|---|-----------------|---|--|--|
| SHEET NO.       | DESCRIPTION:  |                 |   |  |  |
| 1               | COVER SHEET   | 000001-07       | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS                                | BEFORE STARTING ANY EXCAVATION THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR "CUAN" (CHICAGO UTILITY ALERT NETWORK) AT 312-744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES, (48 HOURS NOTIFICATION IS REQUIRED).  |  |
| 2               | INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES                                 | 424001-11       | PERPENDICULAR CURB RAMPS FOR SIDEWALKS                                      | 10 FEET (3 METERS) TRANSITION SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.   |  |
| 3-5             | SUMMARY OF QUANTITIES   | 424011-04       | CORNER PARALLEL CURB RAMPS FOR SIDEWALKS                                    | THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE UTILITY COMPANIES AND THE VILLAGE OF LISLE.   |  |
| 6-14            | EXISTING AND PROPOSED TYPICAL SECTIONS  | 424016-05       | MIDBLOCK CURB RAMPS FOR SIDEWALKS   | THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.  |  |
| 15-19           | PROPOSED ROADWAY AND PAVEMENT MARKING PLANS   | 442201-03       | CLASS C AND D PATCHES   | ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.   |  |
| 20-32           | ADA SIDEWALK RAMP DETAILS   | 604001-04       | FRAME AND LIDS, TYPE 1  | ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.  |  |
| 33              | DETAILS FOR FRAMES AND LIDS ADJUSTMENTS WITH MILLING (BD-08)                        | 604006-05       | FRAME AND GRATE, TYPE 3   | ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.   |  |
| 34              | PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)                                 | 606001-07       | CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER               | LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT [OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)], WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.  |  |
| 35              | CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)                             | 701101-05       | OFF-ROAD OPERATION, MULTILANE, 15' (4.5m) TO 24" (600mm) FROM PAVEMENT EDGE | DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.   |  |
| 36              | BUTT JOINT AND HMA TAPER DETAILS (BD-32)  | 701106-02       | OFF-ROAD OPERATION, MULTILANE, MORE THAN 15' (4.5m) AWAY                    | IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND EXISTING CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.  |  |
| 37              | TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10) | 701601-09       | URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRANSVERSABLE MEDIAN        | FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENT SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT IN PART OF THIS CONTRACT.  |  |
| 38              | TYPICAL APPLICATION FOR RAISED REFLECTIVE PAVEMENT MARKERS (TC-11)                  | 701602-10       | URBAN LANE CLOSURE, MULTILANE, 2W WITH BI-DIRECTIONAL LEFT TURN LANE        | BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.  |  |
| 39              | DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)                                      | 701606-10       | URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN              | WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES (40 mm) WHERE THE SPEED LIMIT IS 40 MPH (80 km/ h) OR LESS AND 1 INCH (25 mm) WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH (80 km/ h). WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING MACHINE IS SLOPED A MINIMUM (1:3). |  |
| 40              | TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TC-14)                                 | 701701-10       | URBAN LANE CLOSURE, MULTILANE INTERSECTION                                  | THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL CENTER AT KALPANA,KANNAN-HOSADURGA @ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.   |  |
| 41              | SHORT-TREM PAVEMENT MARKING LETTERS AND SYMBOLS (TC-16)                             | 701801-06       | LANE CLOSURE, MULTILANE, 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE             | PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH THE "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL. (TC-13)   |  |
| 42              | DISTRICT ONE - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)   | 701901-08       | TRAFFIC CONTROL DEVICES   | THE RESIDENT ENGINEER SHALL CONTACT DON CHIARUGI, AREA TRAFFIC FIELD ENGINEER, AT DON.CHIARUGI@ILLINOIS.GOV, A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.   |  |
| 43-50           | DETECTOR LOOP LOCATION DETAILS  | 780001-05       | TYPICAL PAVEMENT MARKINGS   | THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.  |  |
| 51              | ARTERIAL ROAD INFORMATION SIGNING (TC-22)   | 886001-01       | DETECTOR LOOP INSTALLATIONS   | THE CONTRACTOR SHALL CONTACT THE ROADSIDE DEVELOPMENT UNIT AT (847) 705-4171 AT LEAST 2 WEEKS PRIOR TO BEGINNING ANY FORESTRY WORK FOR LAYOUT.   |  |
|                 |   | 886006-01       | TYPICAL LAYOUTS FOR DETECTION LOOPS   |  |  |

| SUMMARY OF QUANTITIES |  |       | URBAN<br>TOTAL<br>QUANTITIES | CONSTRUCTION TYPE CODE       |  |  |  |  | SUMMARY OF QUANTITIES |            |   | URBAN<br>TOTAL<br>QUANTITIES | CONSTRUCTION TYPE CODE |                              |  |  |  |  |
|-----------------------|--|-------|------------------------------|------------------------------|--|--|--|--|-----------------------|------------|---|------------------------------|------------------------|------------------------------|--|--|--|--|
| CODE NO               | ITEM   | UNIT  |                              | 80% FED<br>20% STATE<br>0005 |  |  |  |  |                       | CODE NO    | ITEM  |                              | UNIT                   | 80% FED<br>20% STATE<br>0005 |  |  |  |  |
| 20100110              | TREE REMOVAL (6 TO 15 UNITS DIAMETER)                          | UNIT  | 158                          | 158                          |  |  |  |  |                       | 42001300   | PROTECTIVE COAT                             | SO YD                        | 827                    | 827                          |  |  |  |  |
| 20100210              | TREE REMOVAL (OVER 15 UNITS DIAMETER)                          | UNIT  | 16                           | 16                           |  |  |  |  |                       | 42400200   | PORTLAND CEMENT CONCRETE SIDEWALK 5<br>INCH | SO FT                        | 3882                   | 3882                         |  |  |  |  |
| 20101350              | TREE PRUNING (OVER 10 INCH DIAMETER)                           | EACH  | 16                           | 16                           |  |  |  |  |                       | * 42400800 | DETECTABLE WARNINGS                         | SO FT                        | 425                    | 425                          |  |  |  |  |
| 20200100              | EARTH EXCAVATION   | CU YD | 396                          | 396                          |  |  |  |  |                       | 44000159   | HOT-MIX ASPHALT SURFACE REMOVAL, 2<br>1/2"  | SO YD                        | 88908                  | 88908                        |  |  |  |  |
| 21101615              | TOPSOIL FURNISH AND PLACE, 4"                                  | SO YD | 173                          | 173                          |  |  |  |  |                       | 44000600   | SIDEWALK REMOVAL                            | SO FT                        | 3651                   | 3651                         |  |  |  |  |
| 25000400              | NITROGEN FERTILIZER NUTRIENT                                   | POUND | 81                           | 81                           |  |  |  |  |                       | 44201811   | CLASS D PATCHES, TYPE I, 14 INCH            | SO YD                        | 267                    | 267                          |  |  |  |  |
| 25000500              | PHOSPHORUS FERTILIZER NUTRIENT                                 | POUND | 81                           | 81                           |  |  |  |  |                       | 44201815   | CLASS D PATCHES, TYPE II, 14 INCH           | SO YD                        | 2134                   | 2134                         |  |  |  |  |
| 25000600              | POTASSIUM FERTILIZER NUTRIENT                                  | POUND | 81                           | 81                           |  |  |  |  |                       | 44201819   | CLASS D PATCHES, TYPE III, 14 INCH          | SO YD                        | 1601                   | 1601                         |  |  |  |  |
| 25003210              | INTERSEEDING, CLASS 2A   | ACRE  | 0.86                         | 0.86                         |  |  |  |  |                       | 44201821   | CLASS D PATCHES, TYPE IV, 14 INCH           | SO YD                        | 1334                   | 1334                         |  |  |  |  |
| 25200110              | SODDING, SALT TOLERANT   | SO YD | 173                          | 173                          |  |  |  |  |                       | 60252800   | CATCH BASINS TO BE RECONSTRUCTED            | EACH                         | 3                      | 3                            |  |  |  |  |
| 40600290              | BITUMINOUS MATERIALS (TACK COAT)                               | POUND | 60013                        | 60013                        |  |  |  |  |                       | 60300105   | FRAMES AND GRATES TO BE ADJUSTED            | EACH                         | 8                      | 8                            |  |  |  |  |
| 40600400              | MIXTURE FOR CRACKS, JOINTS, AND<br>FLANGEWAYS                  | TON   | 134                          | 134                          |  |  |  |  |                       | 60300305   | FRAMES AND LIDS TO BE ADJUSTED              | EACH                         | 8                      | 8                            |  |  |  |  |
| 40600827              | POLYMERIZED LEVELING BINDER (MACHINE<br>METHOD), IL-4, 75, N50 | TON   | 3668                         | 3668                         |  |  |  |  |                       | *66900200  | NON-SPECIAL WASTE DISPOSAL                  | CU YD                        | 77                     | 77                           |  |  |  |  |
| 40600982              | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT<br>JOINT                | SO YD | 716                          | 716                          |  |  |  |  |                       | *66900530  | SOIL DISPOSAL ANALYSIS                      | EACH                         | 3                      | 3                            |  |  |  |  |

\* SPECIALTY ITEMS  
 NON-PARTICIPATING ITEMS

| SUMMARY OF QUANTITIES |   |        | URBAN            |                        | CONSTRUCTION TYPE CODE |  |  |  |  |
|-----------------------|---|--------|------------------|------------------------|------------------------|--|--|--|--|
| CODE NO               | ITEM  | UNIT   | TOTAL QUANTITIES | 80% FED 20% STATE 0005 |                        |  |  |  |  |
| *66901001             | REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN      | LSUM   | 1                | 1                      |                        |  |  |  |  |
| *66901002             | ON-SITE MONITORING OF REGULATED SUBSTANCES      | CAL DA | 15               | 15                     |                        |  |  |  |  |
| *66901003             | REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT  | LSUM   | 1                | 1                      |                        |  |  |  |  |
| 67000400              | ENGINEER'S FIELD OFFICE, TYPE A                 | CAL MO | 6                | 6                      |                        |  |  |  |  |
| 67100100              | MOBILIZATION                                    | L SUM  | 1                | 1                      |                        |  |  |  |  |
| 70102625              | TRAFFIC CONTROL AND PROTECTION, STANDARD 701606 | L SUM  | 1                | 1                      |                        |  |  |  |  |
| 70102632              | TRAFFIC CONTROL AND PROTECTION, STANDARD 701602 | L SUM  | 1                | 1                      |                        |  |  |  |  |
| 70102635              | TRAFFIC CONTROL AND PROTECTION, STANDARD 701701 | L SUM  | 1                | 1                      |                        |  |  |  |  |
| 70102640              | TRAFFIC CONTROL AND PROTECTION, STANDARD 701801 | L SUM  | 1                | 1                      |                        |  |  |  |  |
| 70300100              | SHORT TERM PAVEMENT MARKING                     | FOOT   | 10174            | 10174                  |                        |  |  |  |  |
| 70300150              | SHORT TERM PAVEMENT MARKING REMOVAL             | SO FT  | 1696             | 1696                   |                        |  |  |  |  |
| 70300210              | TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS  | SO FT  | 1649             | 1649                   |                        |  |  |  |  |

| SUMMARY OF QUANTITIES |  |       | URBAN            |                        | CONSTRUCTION TYPE CODE |  |  |  |  |
|-----------------------|--|-------|------------------|------------------------|------------------------|--|--|--|--|
| CODE NO               | ITEM   | UNIT  | TOTAL QUANTITIES | 80% FED 20% STATE 0005 |                        |  |  |  |  |
| 70300220              | TEMPORARY PAVEMENT MARKING - LINE 4"                 | FOOT  | 27469            | 27469                  |                        |  |  |  |  |
| 70300240              | TEMPORARY PAVEMENT MARKING - LINE 6"                 | FOOT  | 8404             | 8404                   |                        |  |  |  |  |
| 70300250              | TEMPORARY PAVEMENT MARKING - LINE 8"                 | FOOT  | 1655             | 1655                   |                        |  |  |  |  |
| 70300260              | TEMPORARY PAVEMENT MARKING - LINE 12"                | FOOT  | 1505             | 1505                   |                        |  |  |  |  |
| 70300280              | TEMPORARY PAVEMENT MARKING - LINE 24"                | FOOT  | 1308             | 1308                   |                        |  |  |  |  |
| 70300520              | PAVEMENT MARKING TAPE, TYPE III 4"                   | FOOT  | 5087             | 5087                   |                        |  |  |  |  |
| *78000100             | THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS | SO FT | 1649             | 1649                   |                        |  |  |  |  |
| *78000200             | THERMOPLASTIC PAVEMENT MARKING - LINE 4"             | FOOT  | 27469            | 27469                  |                        |  |  |  |  |
| *78000400             | THERMOPLASTIC PAVEMENT MARKING - LINE 6"             | FOOT  | 8404             | 8404                   |                        |  |  |  |  |
| *78000500             | THERMOPLASTIC PAVEMENT MARKING - LINE 8"             | FOOT  | 1655             | 1655                   |                        |  |  |  |  |
| *78000600             | THERMOPLASTIC PAVEMENT MARKING - LINE 12"            | FOOT  | 1505             | 1505                   |                        |  |  |  |  |
| *78000650             | THERMOPLASTIC PAVEMENT MARKING - LINE 24"            | FOOT  | 1308             | 1308                   |                        |  |  |  |  |
| *78100100             | RAISED REFLECTIVE PAVEMENT MARKER                    | EACH  | 1240             | 1240                   |                        |  |  |  |  |

|   |  |            |           |
|---|--|------------|-----------|
| FILE NAME =   | USER NAME = steedpa                                      | DESIGNED - | REVISED - |
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|   | PLOT SCALE = 100.0000' / 1in.                            | CHECKED -  | REVISED - |
|   | PLOT DATE = 2/11/2019                                    | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES  
FAP 870 /IL 53 (LINCOLN AVE.)—I-88 TO 59TH ST.**

|   |                |        |                    |           |
|---|----------------|--------|--------------------|-----------|
| F.A.P. RTE.                                     | SECTION        | COUNTY | TOTAL SHEETS       | SHEET NO. |
| 870   | 2018-029-RS-SW |        | 51                 | 4         |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |                |        | CONTRACT NO. 62G23 |           |

| SUMMARY OF QUANTITIES |  |        | URBAN            |                        | CONSTRUCTION TYPE CODE |  |  |  |  |
|-----------------------|--|--------|------------------|------------------------|------------------------|--|--|--|--|
| CODE NO               | ITEM   | UNIT   | TOTAL QUANTITIES | 80% FED 20% STATE 0005 |                        |  |  |  |  |
| 78300200              | RAISED REFLECTIVE PAVEMENT MARKER  | EACH   | 1116             | 1116                   |                        |  |  |  |  |
|                       | REMOVAL  |        |                  |                        |                        |  |  |  |  |
| *88600600             | DETECTOR LOOP REPLACEMENT  | FOOT   | 4106             | 4106                   |                        |  |  |  |  |
| * 89502376            | REBUILD EXISTING HANDHOLE  | EACH   | 8                | 8                      |                        |  |  |  |  |
| K0029618              | WEED CONTROL, BROADLEAF IN TURF  | GALLON | 2.5              | 2.5                    |                        |  |  |  |  |
| X0320050              | CONSTRUCTION LAYOUT (SPECIAL)  | L SUM  | 1                | 1                      |                        |  |  |  |  |
| X2010100              | TREE LIMB REMOVAL (4 TO 10 INCHES DIAMETER)                                | EACH   | 1                | 1                      |                        |  |  |  |  |
| X4060004              | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80 | TON    | 8713             | 8713                   |                        |  |  |  |  |
| X5537800              | STORM SEWERS TO BE CLEANED 12"   | FOOT   | 1816             | 1816                   |                        |  |  |  |  |
| X6030310              | FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)                                   | EACH   | 13               | 13                     |                        |  |  |  |  |
| X7030005              | TEMPORARY PAVEMENT MARKING REMOVAL   | SO FT  | 1696             | 1696                   |                        |  |  |  |  |
| Z0004562              | COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT               | FOOT   | 1087             | 1087                   |                        |  |  |  |  |
| Z0030850              | TEMPORARY INFORMATION SIGNING  | SO FT  | 51.4             | 51.4                   |                        |  |  |  |  |
| Z0033700              | LONGITUDINAL JOINT SEALANT   | FOOT   | 36607            | 36607                  |                        |  |  |  |  |

| SUMMARY OF QUANTITIES |   |       | URBAN            |                        | CONSTRUCTION TYPE CODE |  |  |  |  |
|-----------------------|---|-------|------------------|------------------------|------------------------|--|--|--|--|
| CODE NO               | ITEM                                    | UNIT  | TOTAL QUANTITIES | 80% FED 20% STATE 0005 |                        |  |  |  |  |
| Z0048665              | RAILROAD PROTECTIVE LIABILITY INSURANCE | L SUM | 1                | 1                      |                        |  |  |  |  |
| Z0064800              | SELECTIVE CLEARING                      | UNIT  | 37.7             | 37.7                   |                        |  |  |  |  |
| Z0076600              | TRAINEES                                | HOUR  | 500              | 500                    |                        |  |  |  |  |
| Z0076604              | TRAINEES TRAINING PROGRAM GRADUATE      | HOUR  | 500              | 500                    |                        |  |  |  |  |

|  |                              |            |           |
|--|------------------------------|------------|-----------|
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|  | PLOT DATE = 2/1/2019         |            |           |

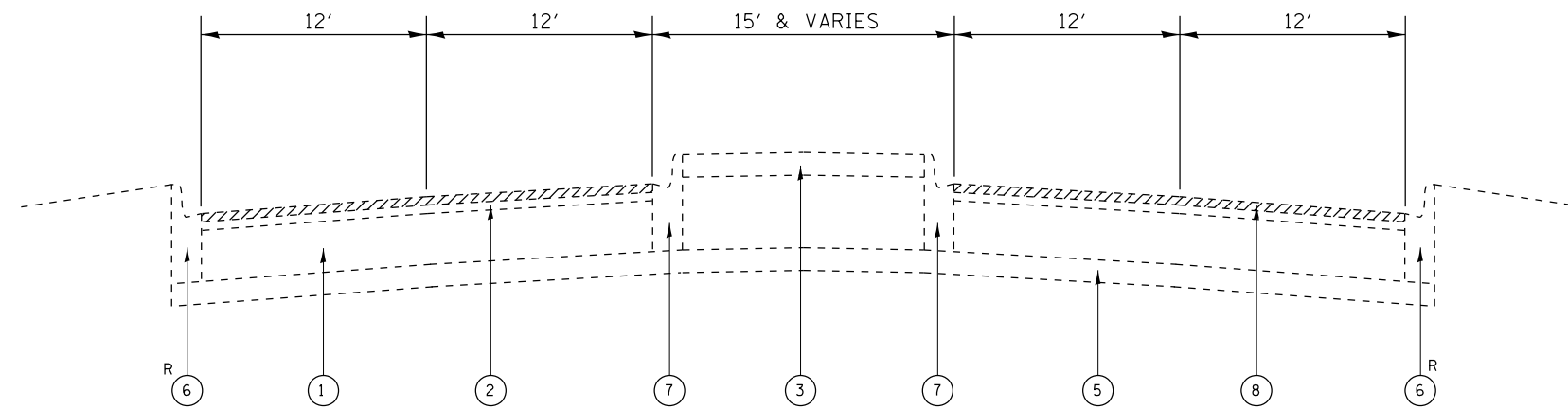
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**  
**FAP 870 /IL 53 (LINCOLN AVE.)—188 TO 59TH ST.**

|                           |                |         |              |           |
|---------------------------|----------------|---------|--------------|-----------|
| F.A.P. RTE.               | SECTION        | COUNTY  | TOTAL SHEETS | SHEET NO. |
| 870                       | 2018-029-RS-SW | DU PAGE | 51           | 5         |
| CONTRACT NO. 62G23        |                |         |              |           |
| ILLINOIS FED. AID PROJECT |                |         |              |           |

**LEGEND:**

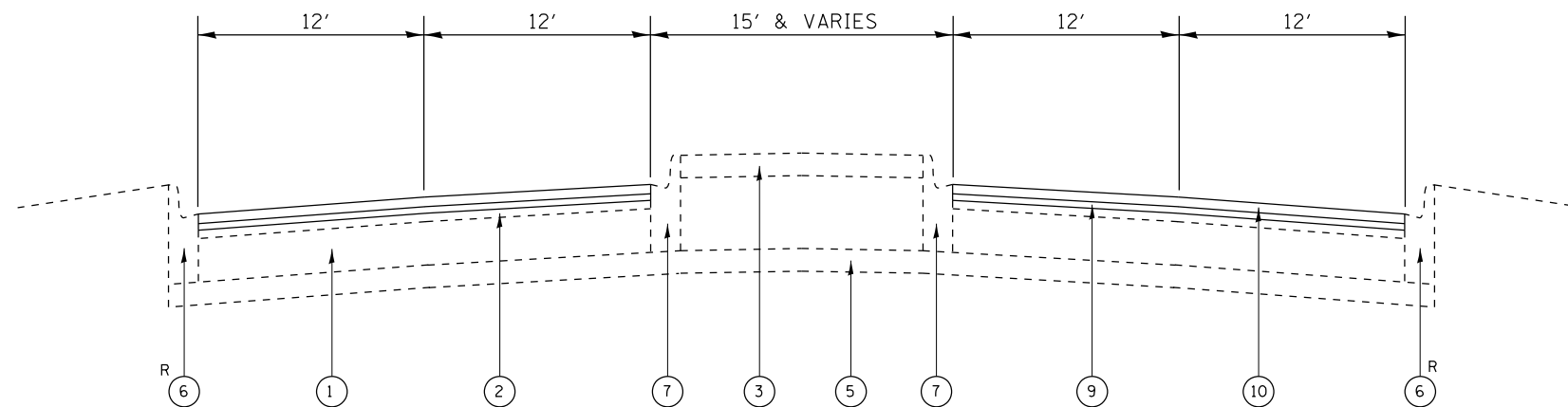
- ① EXISTING P.C.C. PAVEMENT ±10"
- ② EXISTING HOT-MIX ASPHALT SURFACE ±4" (AFTER MILLING)
- ③ EXISTING HOT-MIX ASPHALT MEDIAN
- ④ EXISTING P.C.C. BARRIER MEDIAN
- ⑤ EXISTING STABILIZED SUB-BASE
- ⑥ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑦ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
- ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
- ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 1¾"
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)



**EXISTING TYPICAL SECTION**  
 STA. 10+99 TO 11+64  
 STA. 18+93 TO 19+65  
 STA. 26+55 TO 29+62  
 STA. 35+57 TO 39+07  
 STA. 113+88 TO 114+99

**MIXTURE REQUIREMENTS**

| MIXTURE TYPE   | AIR VOIDS @ Ndes | OMP     |
|--|------------------|---------|
| <b>PATCHING</b>  |                  |         |
| CLASS "D" PATCHES (HMA BINDER COURSE, IL-19MM)   | 4% @ 70 GYR.     | OC / OA |
| <b>PAVEMENT RESURFACING</b>  |                  |         |
| POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80   | 3.5% @ 70 GYR.   | PFP     |
| POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50   | 3.5% @ 50 GYR.   | QCP     |
| OMP DESIGNATION: QUALITY CONTROL FOR PERFORMANCE (QCP); QUALITY CONTROL / QUALITY ASSURANCE (OC / OA); PAY FOR PERFORMANCE (PFP) |                  |         |



**PROPOSED TYPICAL SECTION**  
 STA. 10+99 TO 11+64  
 STA. 18+93 TO 19+65  
 STA. 26+55 TO 29+62  
 STA. 35+57 TO 39+07  
 STA. 113+88 TO 114+99

**NOTES:**

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE QUANTITIES IS 112 LBS./SQ. YD./ IN.

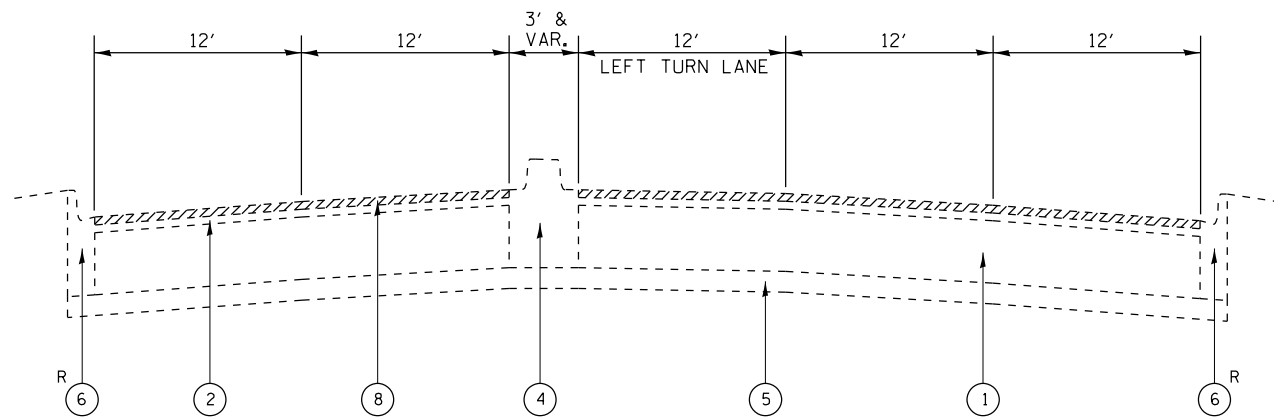
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

THE MILLING SHALL BE DONE PRIOR TO PATCHING

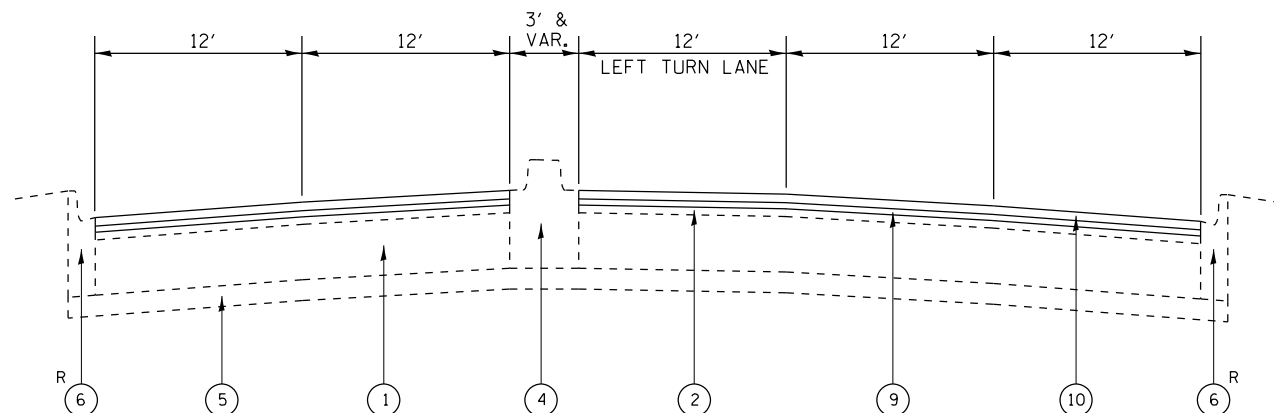
QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE

FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS

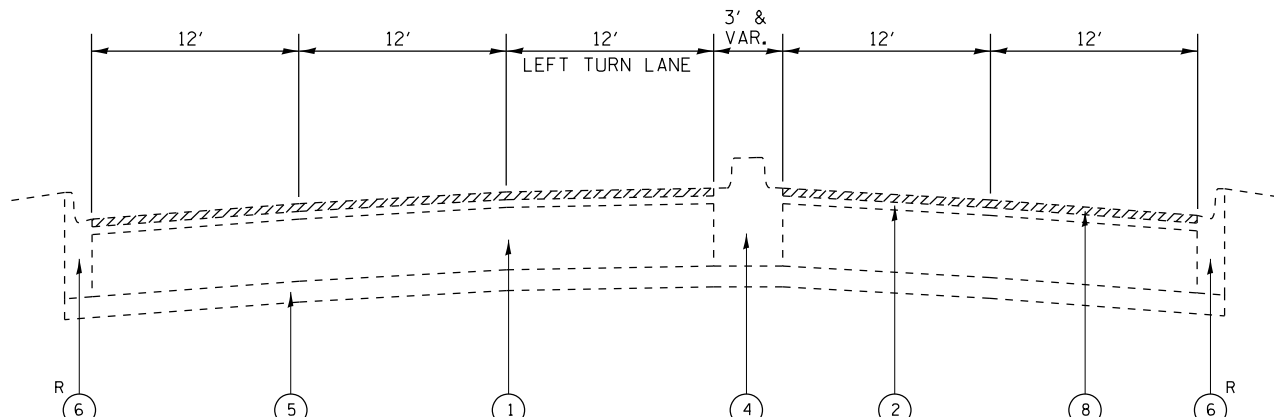
THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE POLYMERIZED LEVELING BINDER WHERE THE SURFACE JOINT WILL BE LOCATED



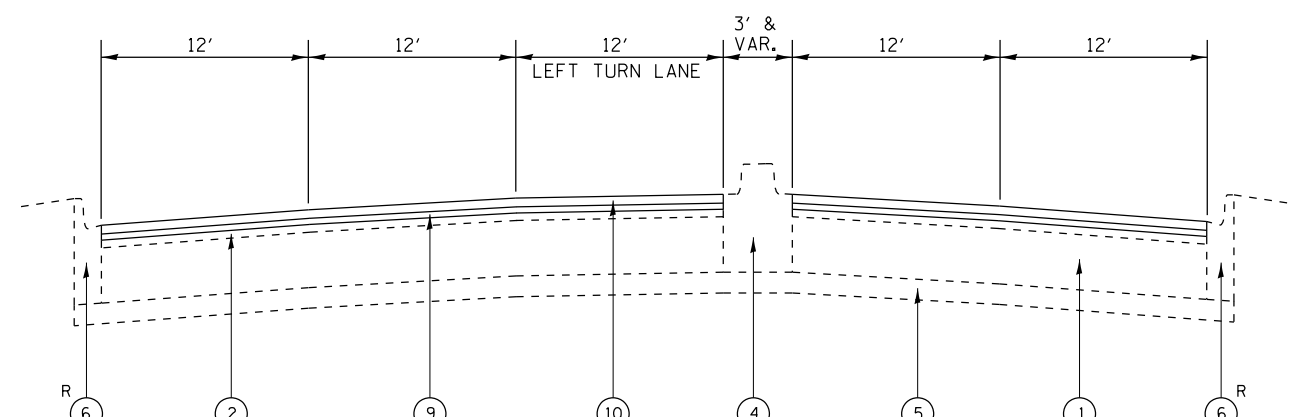
**EXISTING TYPICAL SECTION**  
 STA. 11+76 TO 14+47  
 STA. 21+20 TO 22+60  
 STA. 31+74 TO 33+43  
 STA. 39+07 TO 40+52  
 STA. 114+99 TO 118+02



**PROPOSED TYPICAL SECTION**  
 STA. 11+76 TO 14+47  
 STA. 21+20 TO 22+60  
 STA. 31+74 TO 33+43  
 STA. 39+07 TO 40+52  
 STA. 114+99 TO 118+02



**EXISTING TYPICAL SECTION**  
 STA. 15+35 TO 18+93  
 STA. 23+47 TO 26+55  
 STA. 34+12 TO 35+57



**PROPOSED TYPICAL SECTION**  
 STA. 15+35 TO 18+93  
 STA. 23+47 TO 26+55  
 STA. 34+12 TO 35+57

**LEGEND:**

- ① EXISTING P.C.C. PAVEMENT ±10"
- ② EXISTING HOT-MIX ASPHALT SURFACE ±4" (AFTER MILLING)
- ③ EXISTING HOT-MIX ASPHALT MEDIAN
- ④ EXISTING P.C.C. BARRIER MEDIAN
- ⑤ EXISTING STABILIZED SUB-BASE
- ⑥ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑦ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
- ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
- ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 1¾"
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)

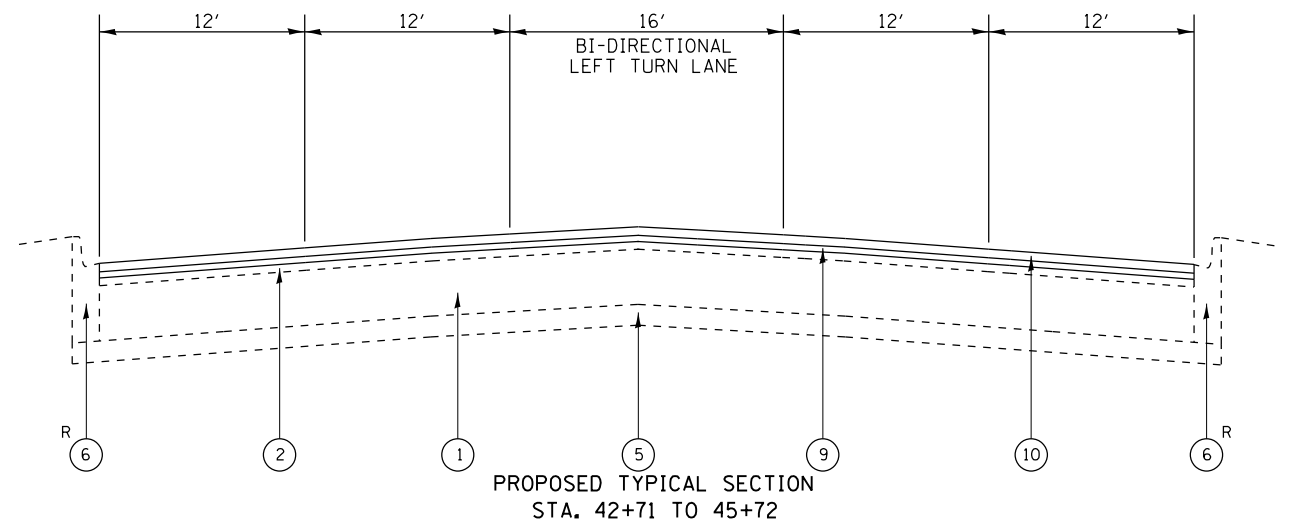
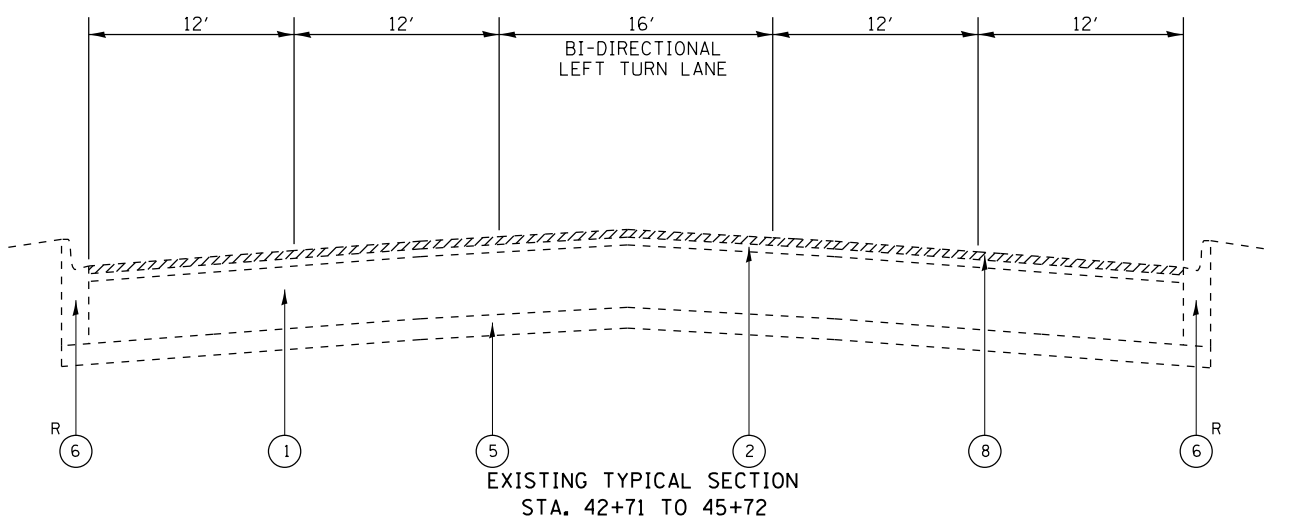
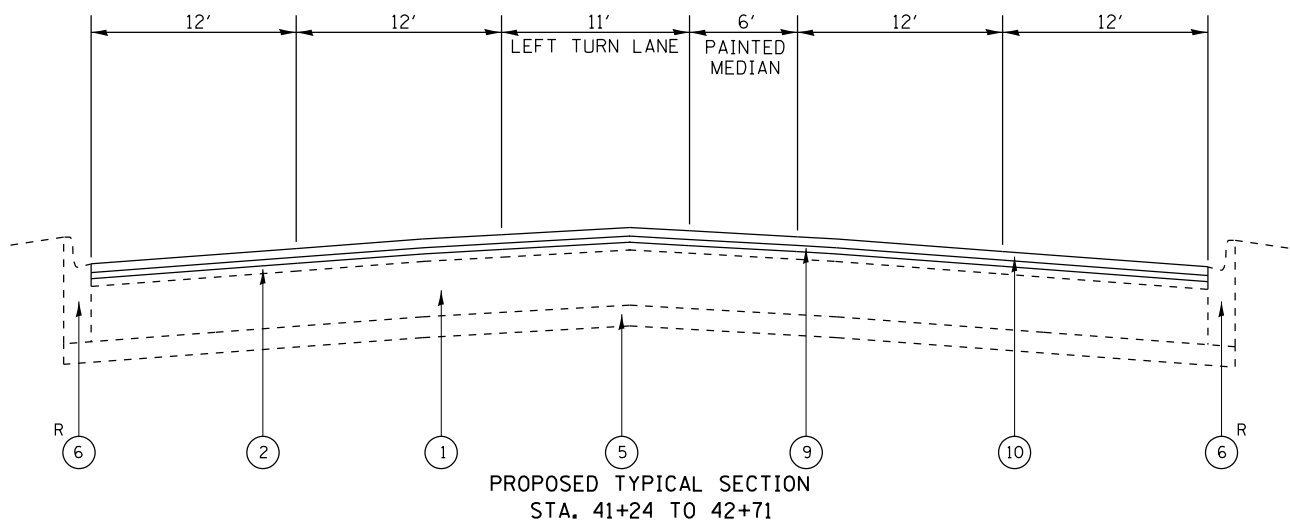
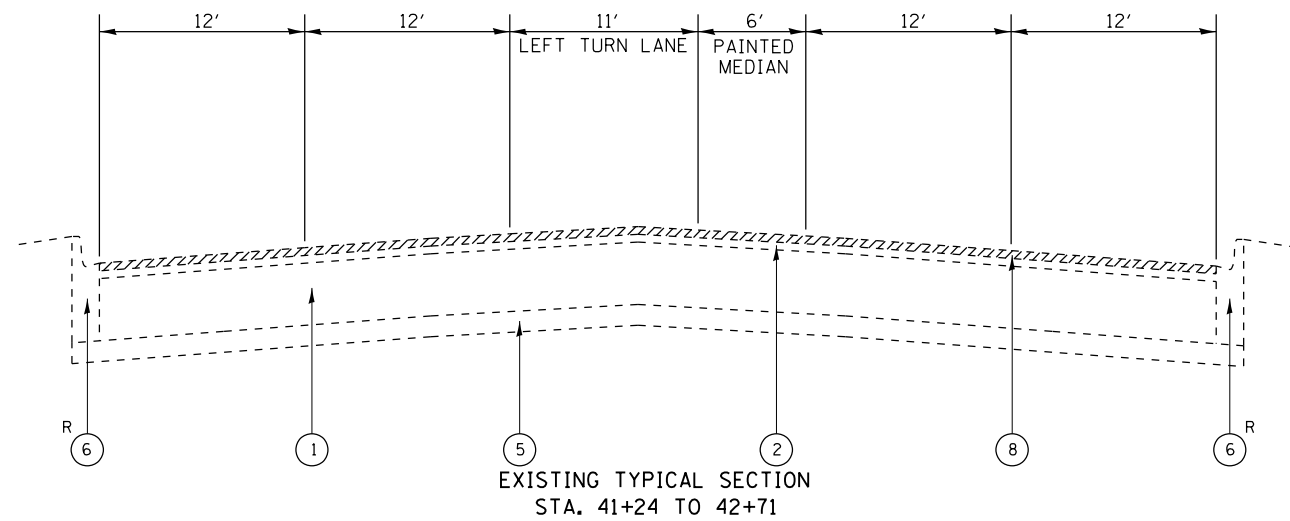
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| PLOT DATE = 2/1/2019  | DATE -             | REVISED -  | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED TYPICAL SECTIONS  
 FAP 870 /IL 53 (LINCOLN AVE.)—I-88 TO 59TH ST.**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

|                           |                |         |              |           |
|---------------------------|----------------|---------|--------------|-----------|
| F.A.P. RTE.               | SECTION        | COUNTY  | TOTAL SHEETS | SHEET NO. |
| 870                       | 2018-029-RS-SW | DU PAGE | 51           | 7         |
| CONTRACT NO. 62G23        |                |         |              |           |
| ILLINOIS FED. AID PROJECT |                |         |              |           |



**LEGEND:**

- ① EXISTING P.C.C. PAVEMENT ±10"
- ② EXISTING HOT-MIX ASPHALT SURFACE ±4" (AFTER MILLING)
- ③ EXISTING HOT-MIX ASPHALT MEDIAN
- ④ EXISTING P.C.C. BARRIER MEDIAN
- ⑤ EXISTING STABILIZED SUB-BASE
- ⑥ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑦ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
- ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
- ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 1¾"
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)

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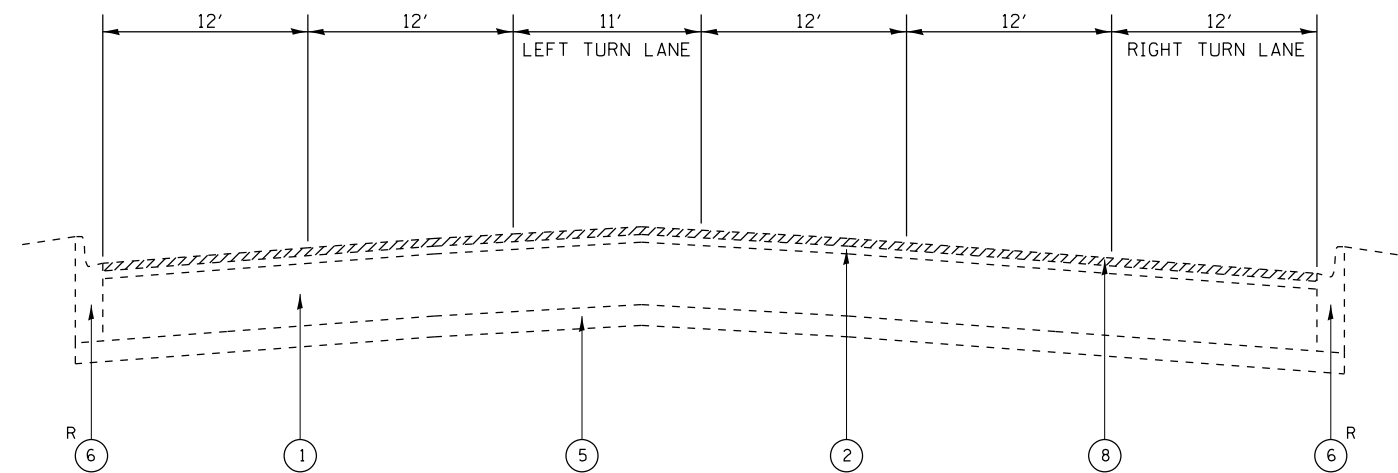
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED TYPICAL SECTIONS  
FAP 870 /IL 53 (LINCOLN AVE.)—I-88 TO 59TH ST.**

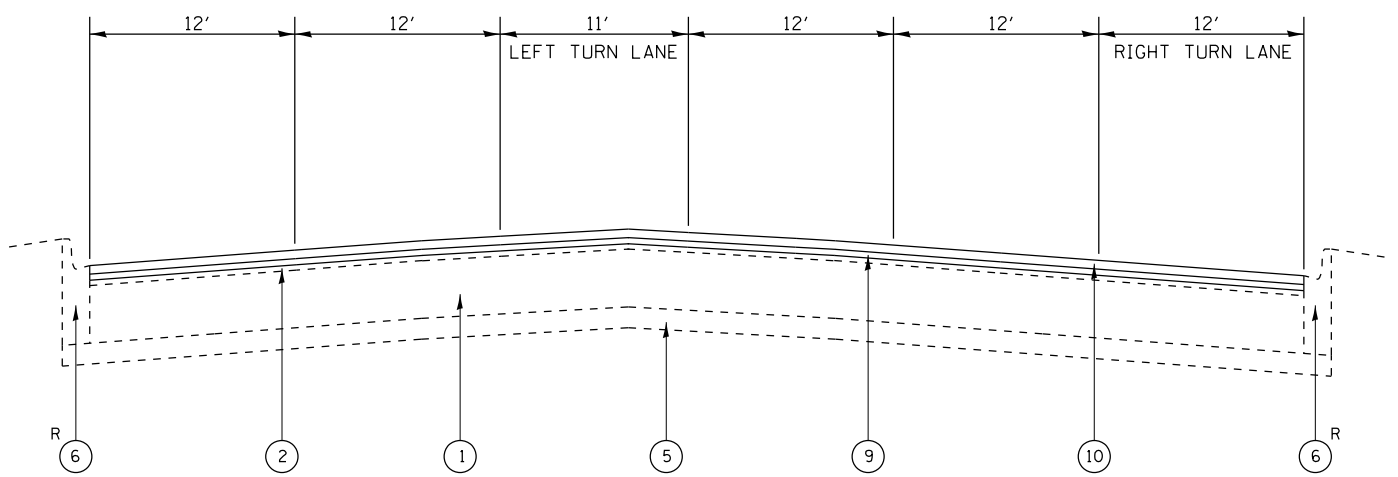
SCALE: SHEET NO. OF SHEETS STA. TO STA.

| F.A.P. RTE.               | SECTION        | COUNTY  | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|---------|--------------|-----------|
| 870                       | 2018-029-RS-SW | DU PAGE | 51           | 8         |
| CONTRACT NO. 62G23        |                |         |              |           |
| ILLINOIS FED. AID PROJECT |                |         |              |           |

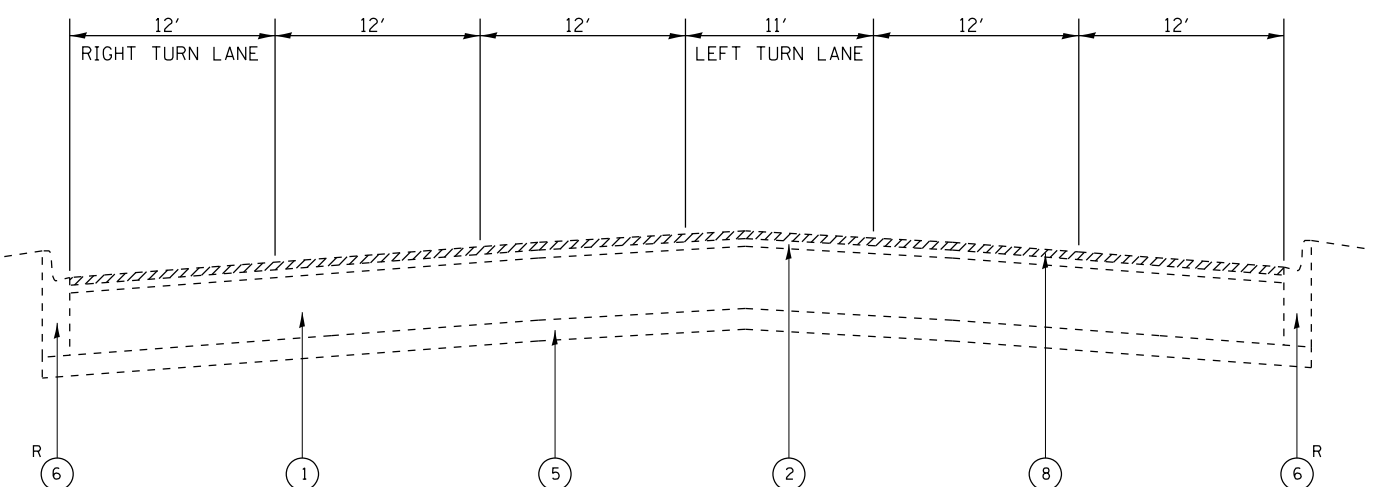




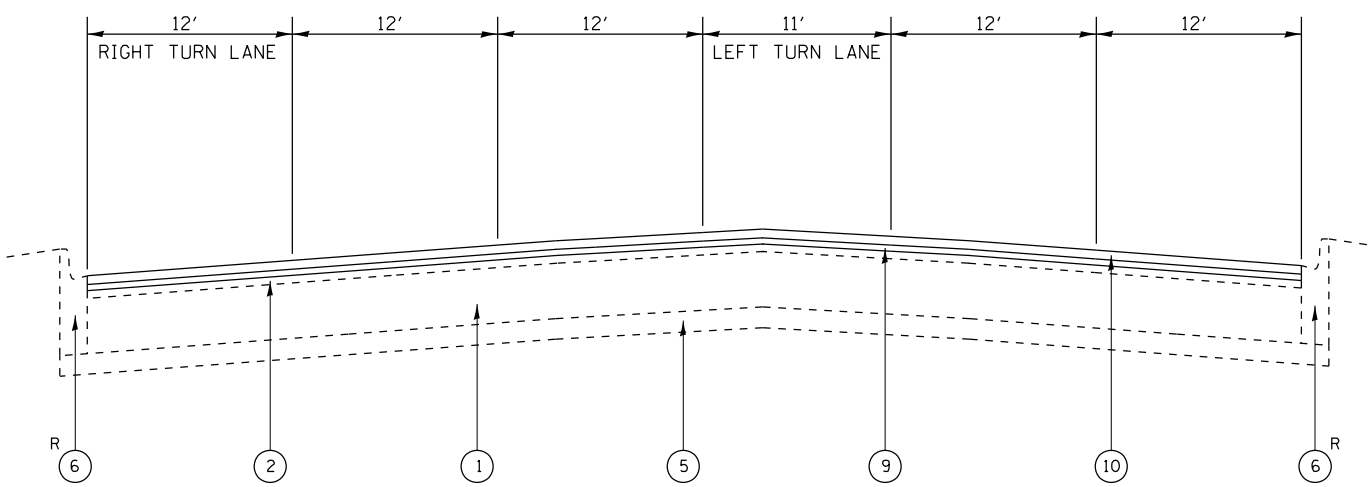
EXISTING TYPICAL SECTION  
STA. 45+72 TO 49+52



PROPOSED TYPICAL SECTION  
STA. 45+72 TO 49+52



EXISTING TYPICAL SECTION  
STA. 50+75 TO 55+00



PROPOSED TYPICAL SECTION  
STA. 50+75 TO 55+00

**LEGEND:**

- ① EXISTING P.C.C. PAVEMENT ±10"
- ② EXISTING HOT-MIX ASPHALT SURFACE ±4" (AFTER MILLING)
- ③ EXISTING HOT-MIX ASPHALT MEDIAN
- ④ EXISTING P.C.C. BARRIER MEDIAN
- ⑤ EXISTING STABILIZED SUB-BASE
- ⑥ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑦ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
- ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), 1L-4.75, N50, ¾"
- ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 1¾"
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)

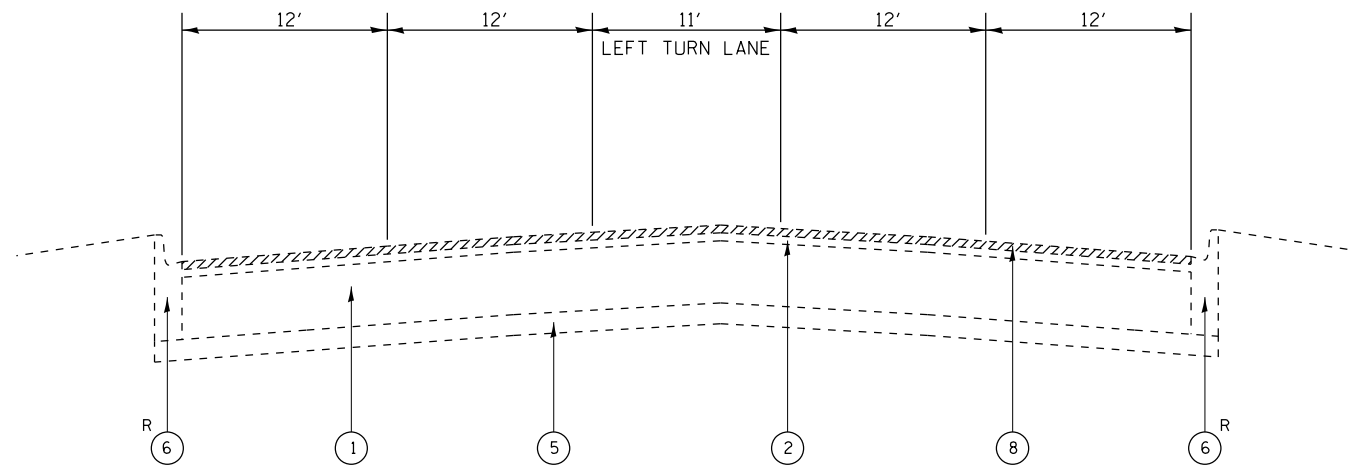
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

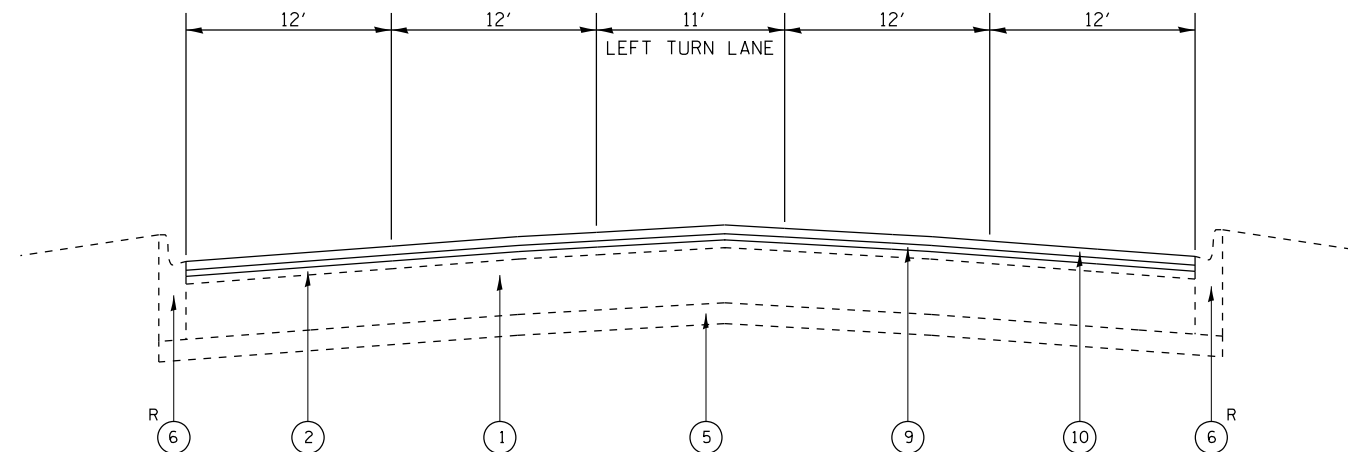
**EXISTING AND PROPOSED TYPICAL SECTIONS  
FAP 870 /IL 53 (LINCOLN AVE.)—I-88 TO 59TH ST.**

|                           |                |         |              |           |
|---------------------------|----------------|---------|--------------|-----------|
| F.A.P. RTE.               | SECTION        | COUNTY  | TOTAL SHEETS | SHEET NO. |
| 870                       | 2018-029-RS-SW | DU PAGE | 51           | 9         |
| CONTRACT NO. 62G23        |                |         |              |           |
| ILLINOIS FED. AID PROJECT |                |         |              |           |

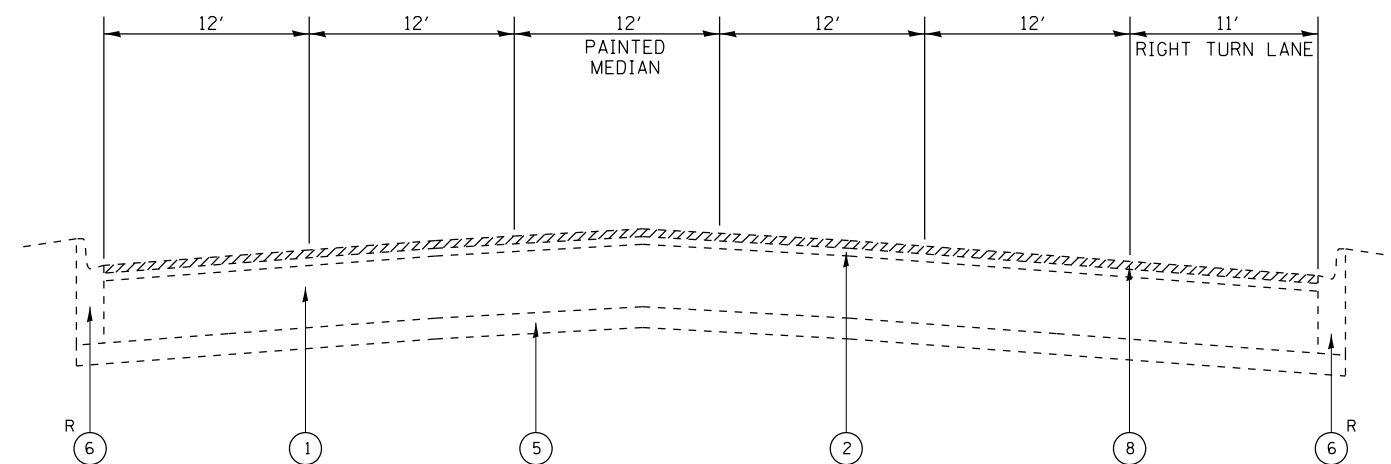
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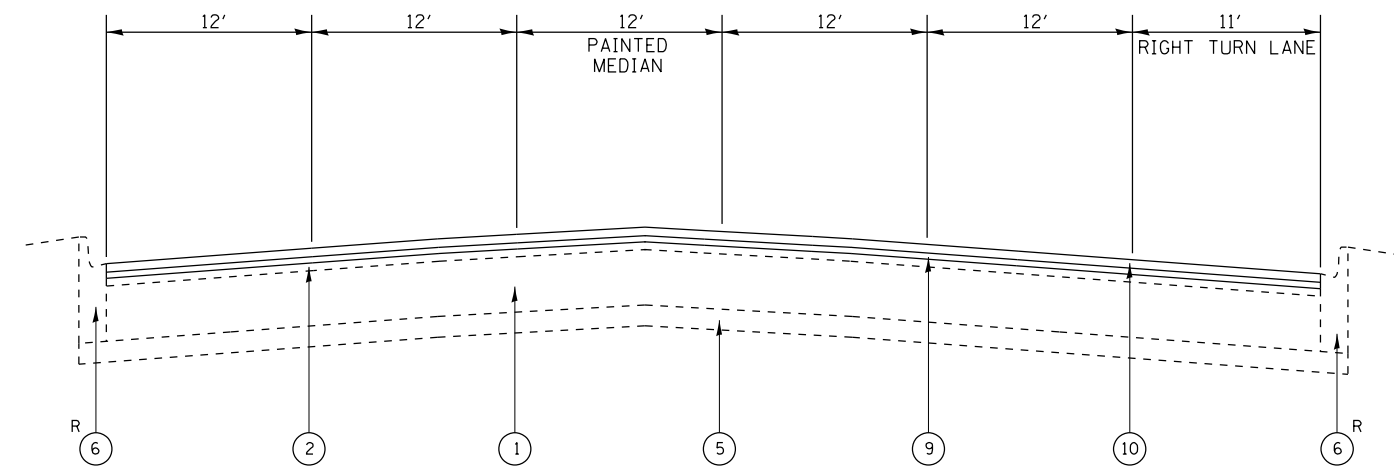
EXISTING TYPICAL SECTION  
 STA. 55+00 TO 57+25  
 STA. 62+21 TO 64+90  
 STA. 74+28 TO 78+93  
 STA. 92+07 TO 100+06  
 STA. 106+69 TO 112+00



PROPOSED TYPICAL SECTION  
 STA. 55+00 TO 57+25  
 STA. 62+21 TO 64+90  
 STA. 74+28 TO 78+93  
 STA. 92+07 TO 100+06  
 STA. 106+69 TO 112+00



EXISTING TYPICAL SECTION  
 STA. 58+37 TO 62+21



PROPOSED TYPICAL SECTION  
 STA. 58+37 TO 62+21

**LEGEND:**

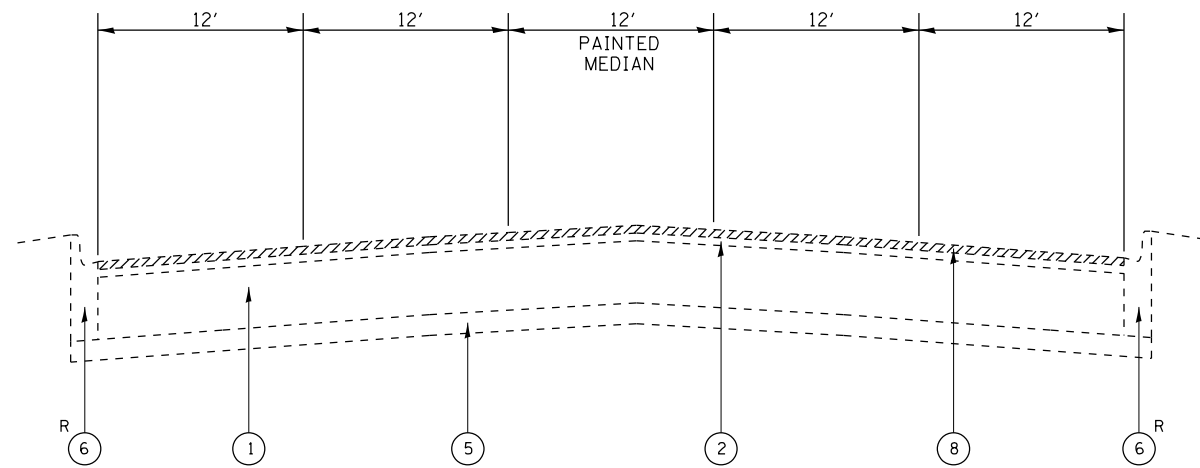
- ① EXISTING P.C.C. PAVEMENT ±10"
- ② EXISTING HOT-MIX ASPHALT SURFACE ±4" (AFTER MILLING)
- ③ EXISTING HOT-MIX ASPHALT MEDIAN
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- ⑤ EXISTING STABILIZED SUB-BASE
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- ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 1¾"
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)

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|   | PLOT DATE = 2/1/2019         | DATE -     | REVISED - |

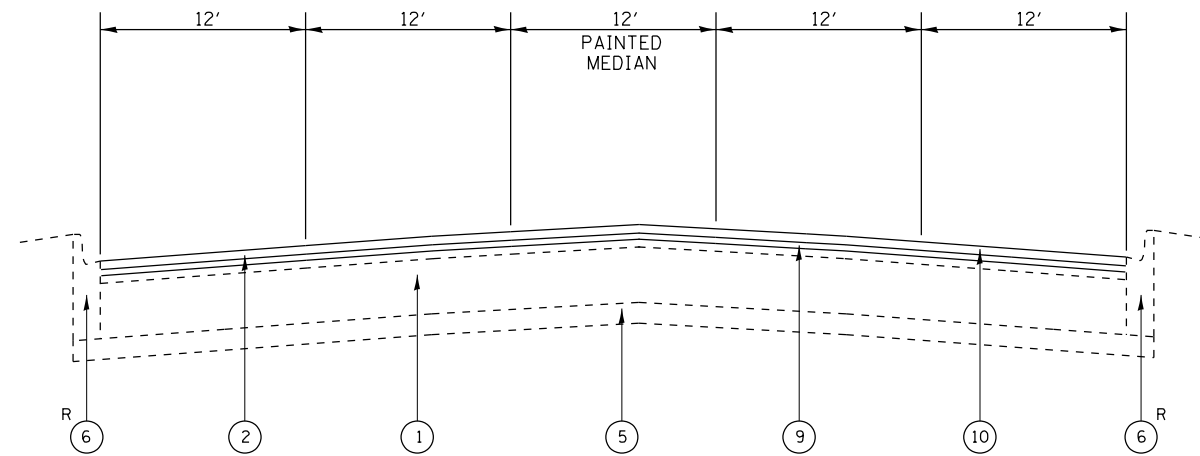
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

|   |           |           |              |
|---|-----------|-----------|--------------|
| <b>EXISTING AND PROPOSED TYPICAL SECTIONS</b>         |           |           |              |
| <b>FAP 870 /IL 53 (LINCOLN AVE.)—I-88 TO 59TH ST.</b> |           |           |              |
| SCALE:  | SHEET NO. | OF SHEETS | STA. TO STA. |

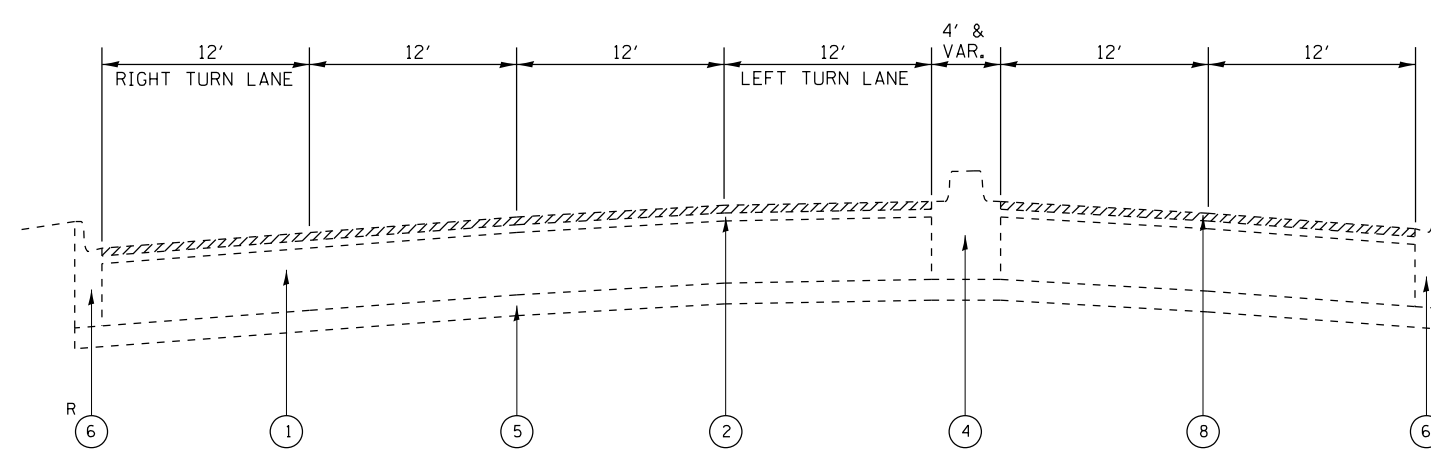
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| F.A.P. RTE.               | SECTION        | COUNTY  | TOTAL SHEETS | SHEET NO. |
| 870                       | 2018-029-RS-SW | DU PAGE | 51           | 10        |
| CONTRACT NO. 62G23        |                |         |              |           |
| ILLINOIS FED. AID PROJECT |                |         |              |           |



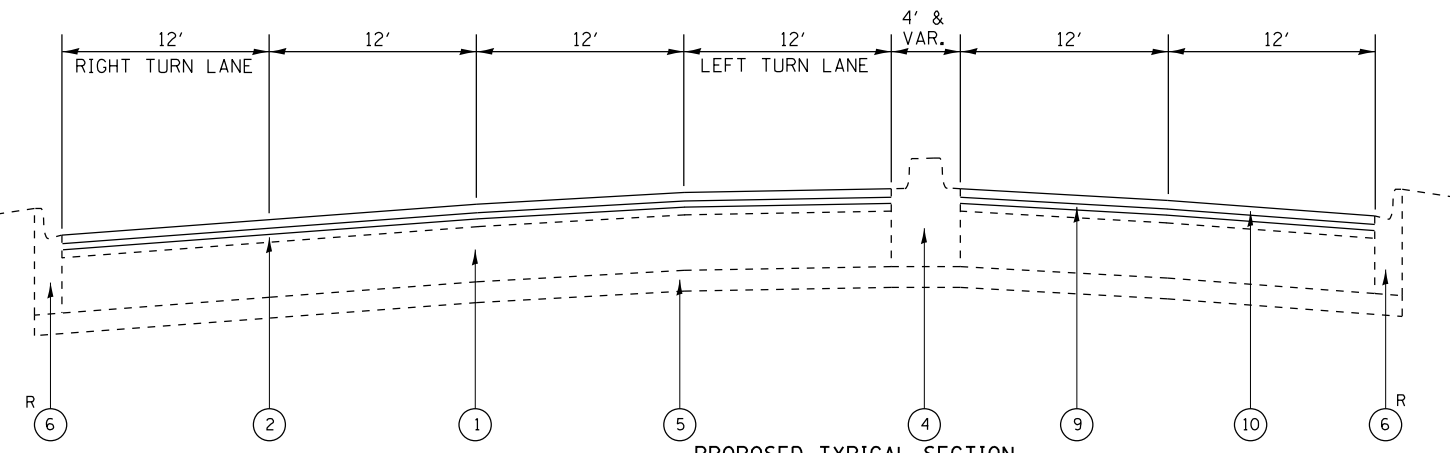
EXISTING TYPICAL SECTION  
 STA. 64+90 TO 74+28  
 STA. 78+93 TO 86+91  
 STA. 90+54 TO 92+07  
 STA. 100+06 TO 106+69



PROPOSED TYPICAL SECTION  
 STA. 64+90 TO 74+28  
 STA. 78+93 TO 86+91  
 STA. 90+54 TO 92+07  
 STA. 100+06 TO 106+69



EXISTING TYPICAL SECTION  
 STA. 118+02 TO 121+40



PROPOSED TYPICAL SECTION  
 STA. 118+02 TO 121+40

**LEGEND:**

- ① EXISTING P.C.C. PAVEMENT ±10"
- ② EXISTING HOT-MIX ASPHALT SURFACE ±4" (AFTER MILLING)
- ③ EXISTING HOT-MIX ASPHALT MEDIAN
- ④ EXISTING P.C.C. BARRIER MEDIAN
- ⑤ EXISTING STABILIZED SUB-BASE
- ⑥ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
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- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
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- ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 1¾"
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)

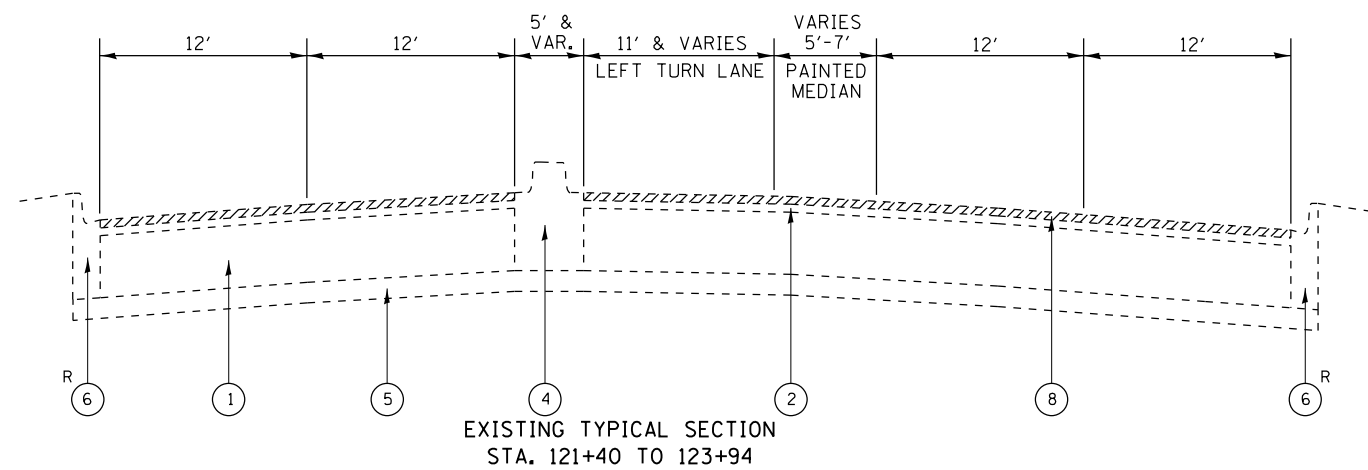
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

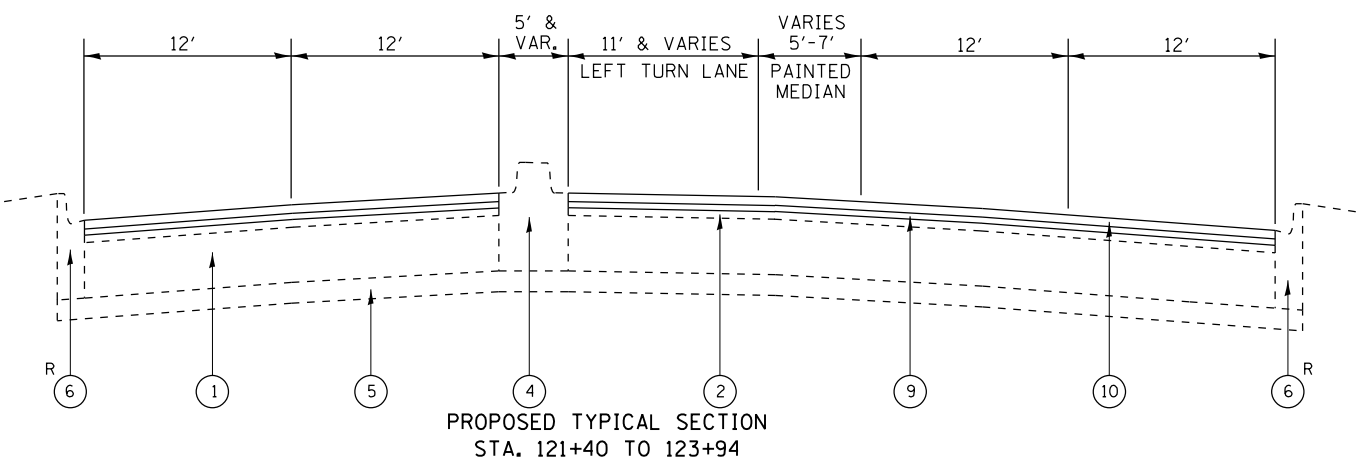
**EXISTING AND PROPOSED TYPICAL SECTIONS  
 FAP 870 /IL 53 (LINCOLN AVE.)—I-88 TO 59TH ST.**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

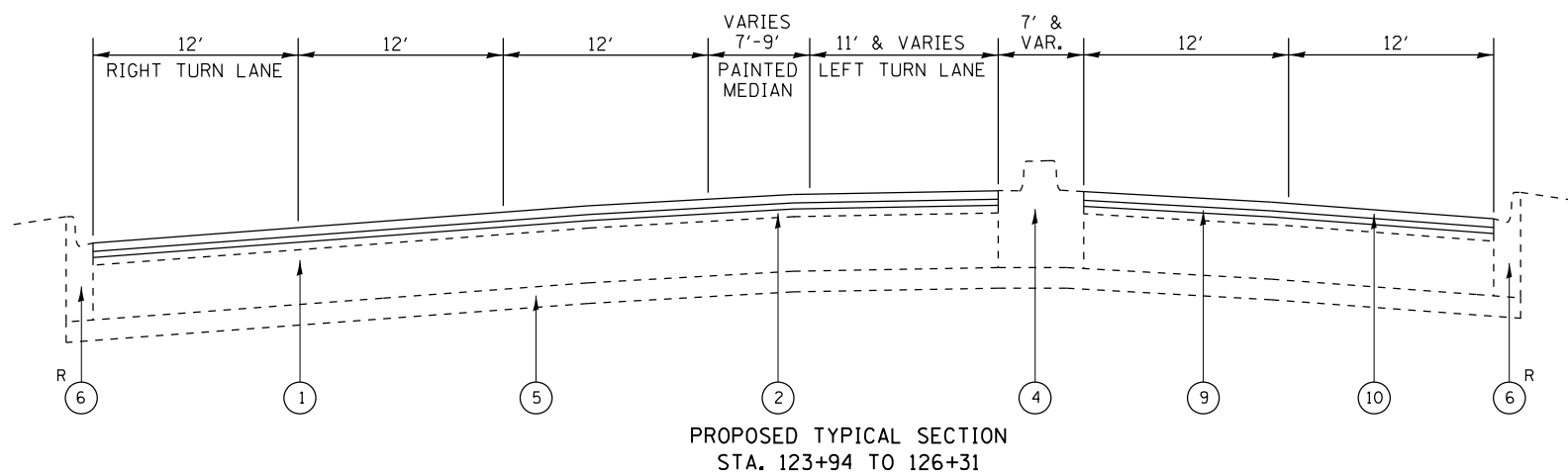
| F.A.P. RTE.               | SECTION        | COUNTY  | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|---------|--------------|-----------|
| 870                       | 2018-029-RS-SW | DU PAGE | 51           | 11        |
| CONTRACT NO. 62G23        |                |         |              |           |
| ILLINOIS FED. AID PROJECT |                |         |              |           |



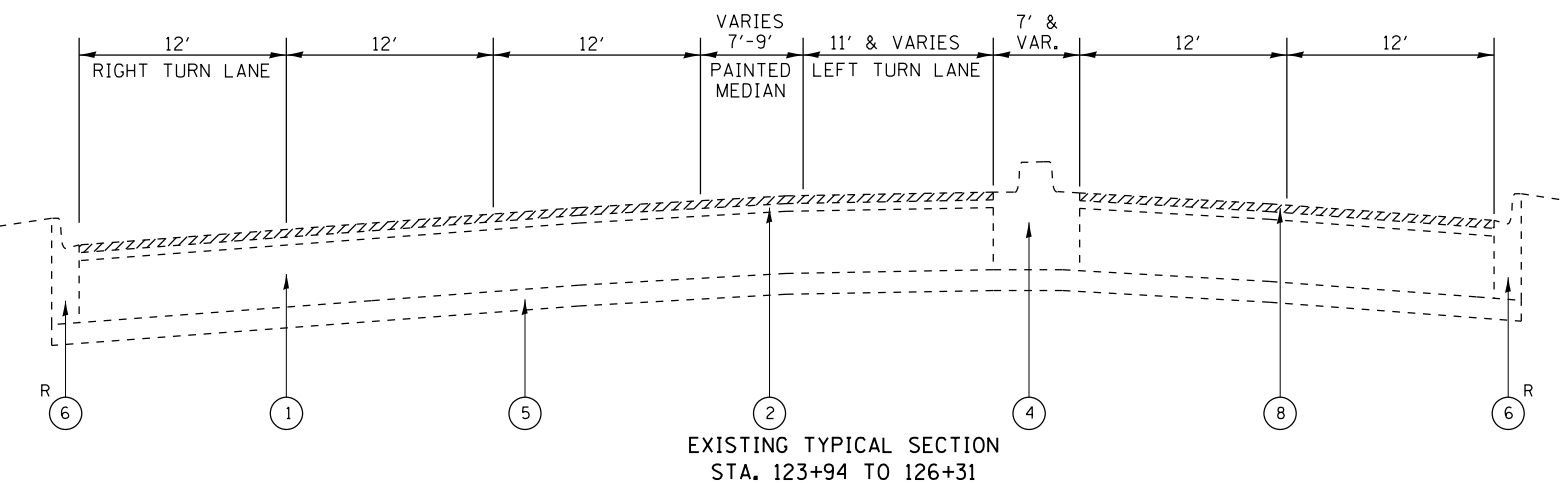
EXISTING TYPICAL SECTION  
STA. 121+40 TO 123+94



PROPOSED TYPICAL SECTION  
STA. 121+40 TO 123+94



PROPOSED TYPICAL SECTION  
STA. 123+94 TO 126+31



EXISTING TYPICAL SECTION  
STA. 123+94 TO 126+31

**LEGEND:**

- ① EXISTING P.C.C. PAVEMENT ±10"
- ② EXISTING HOT-MIX ASPHALT SURFACE ±4" (AFTER MILLING)
- ③ EXISTING HOT-MIX ASPHALT MEDIAN
- ④ EXISTING P.C.C. BARRIER MEDIAN
- ⑤ EXISTING STABILIZED SUB-BASE
- ⑥ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
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- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
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- ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 1¾"
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)

|   |                              |            |           |
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|   | PLOT SCALE = 100.0000' / in. | CHECKED -  | REVISED - |
|   | PLOT DATE = 2/1/2019         | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

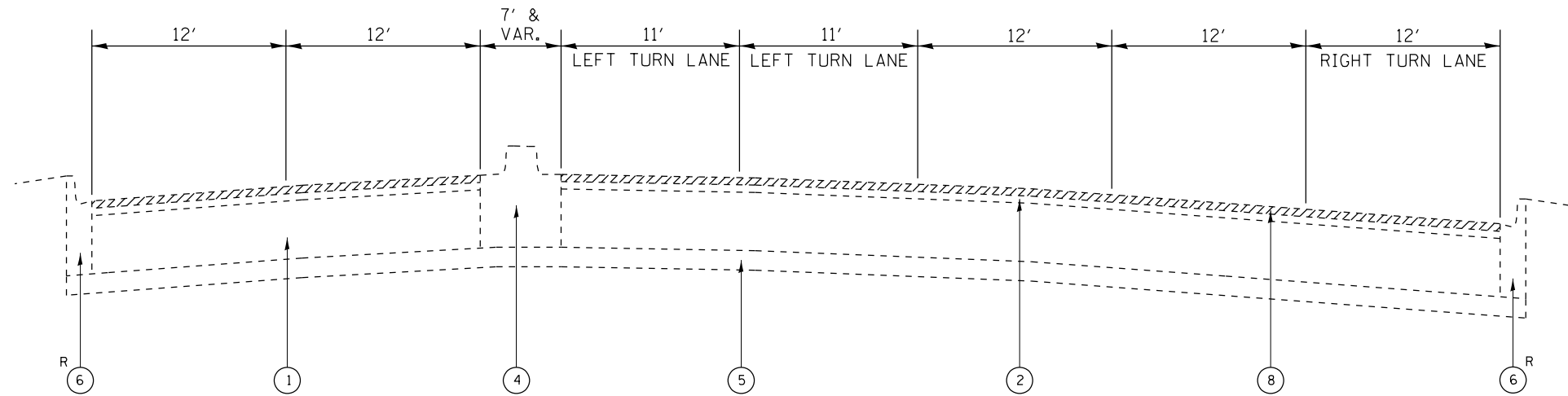
**EXISTING AND PROPOSED TYPICAL SECTIONS  
FAP 870 /IL 53 (LINCOLN AVE.)—I-88 TO 59TH ST.**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

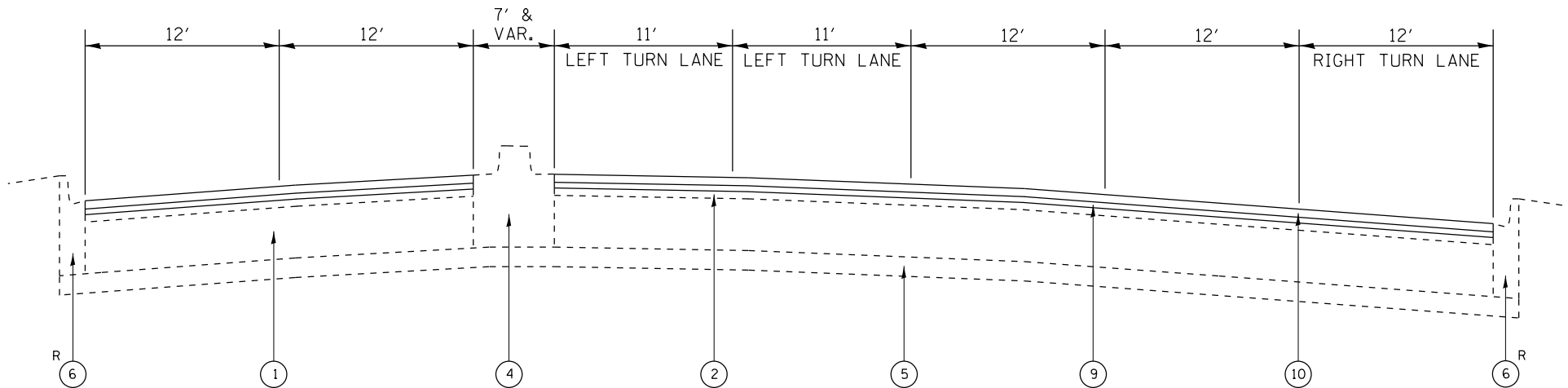
| F.A.P. RTE.               | SECTION        | COUNTY | TOTAL SHEETS       | SHEET NO. |
|---------------------------|----------------|--------|--------------------|-----------|
| 870                       | 2018-029-RS-SW | DJ     | 51                 | 12        |
|                           |                |        | CONTRACT NO. 62G23 |           |
| ILLINOIS FED. AID PROJECT |                |        |                    |           |

**LEGEND:**

- ① EXISTING P.C.C. PAVEMENT ±10"
- ② EXISTING HOT-MIX ASPHALT SURFACE ±4" (AFTER MILLING)
- ③ EXISTING HOT-MIX ASPHALT MEDIAN
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- ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 1¾"
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)



EXISTING TYPICAL SECTION  
STA. 126+31 TO 130+50

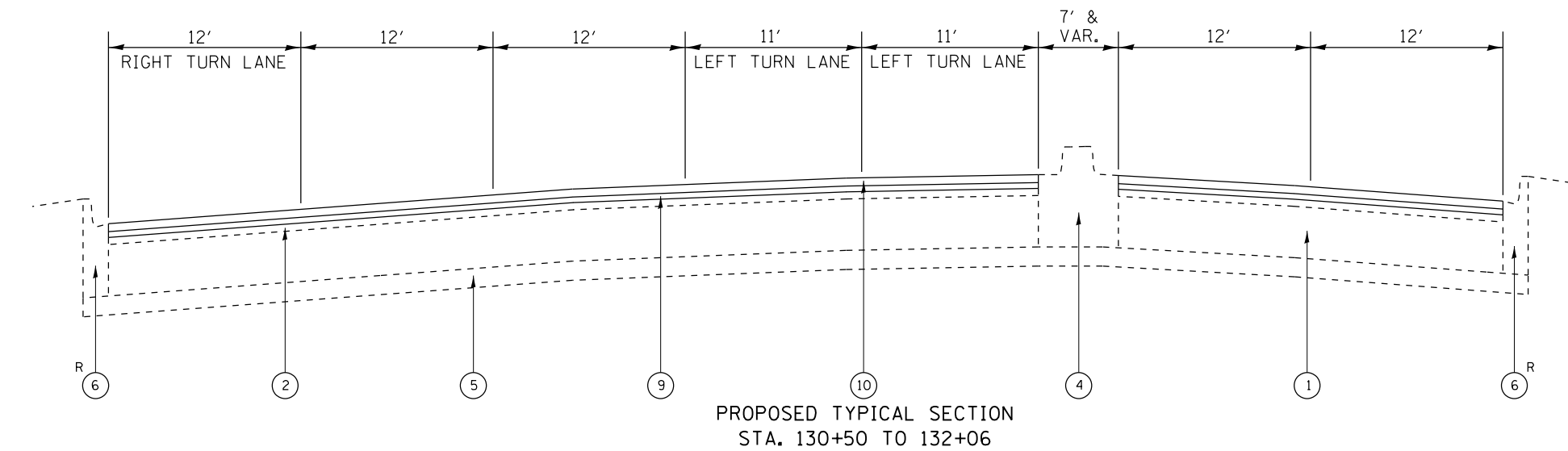
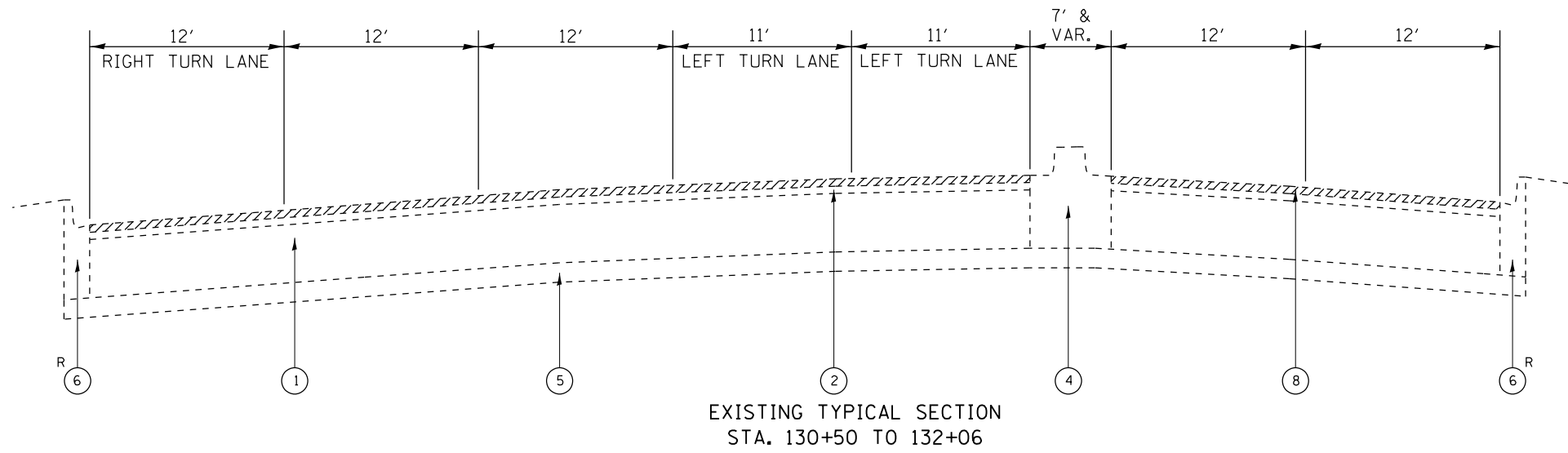


PROPOSED TYPICAL SECTION  
STA. 126+31 TO 130+50

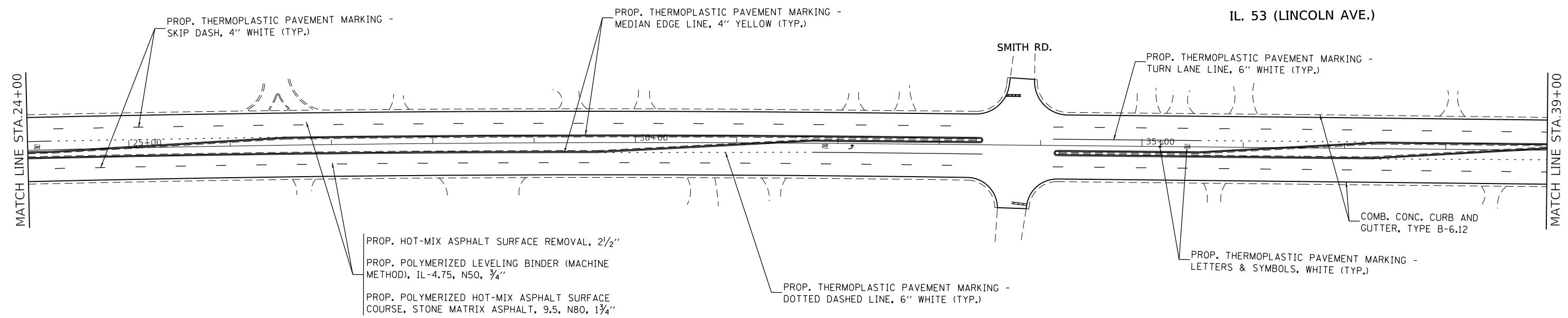
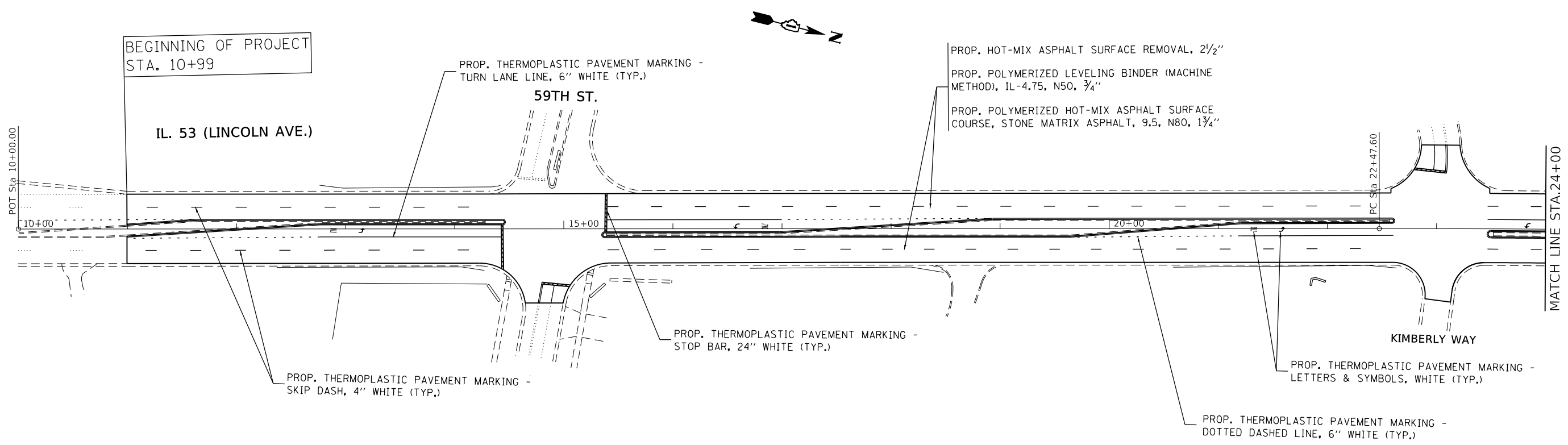
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| FILE NAME =  | USER NAME = steedp | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>EXISTING AND PROPOSED TYPICAL SECTIONS<br/>FAP 870 /IL 53 (LINCOLN AVE.)—I-88 TO 59TH ST.</b> | F.A.P. RTE.               | SECTION        | COUNTY                           | TOTAL SHEETS | SHEET NO. |  |
|  |                    |            |           |   |  | 870                       | 2018-029-RS-SW | DU PAGE                          | 51           | 13        |  |
|  |                    |            |           |   |  | CONTRACT NO. 62G23        |                |                                  |              |           |  |
|  |                    |            |           |   |  | ILLINOIS FED. AID PROJECT |                |                                  |              |           |  |
| PLOT SCALE = 100.0000' / in.<br>PLOT DATE = 2/1/2019 |                    |            |           | CHECKED -<br>DATE -                                       |  | SCALE:                    |                | SHEET NO. OF SHEETS STA. TO STA. |              |           |  |

**LEGEND:**

- ① EXISTING P.C.C. PAVEMENT ±10"
- ② EXISTING HOT-MIX ASPHALT SURFACE ±4" (AFTER MILLING)
- ③ EXISTING HOT-MIX ASPHALT MEDIAN
- ④ EXISTING P.C.C. BARRIER MEDIAN
- ⑤ EXISTING STABILIZED SUB-BASE
- ⑥ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑦ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
- ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
- ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 1¾"
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)



|   |                    |            |           |   |  |             |                |                           |              |           |  |
|---|--------------------|------------|-----------|---|--|-------------|----------------|---------------------------|--------------|-----------|--|
| FILE NAME =   | USER NAME = steedp | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>EXISTING AND PROPOSED TYPICAL SECTIONS<br/>FAP 870 /IL 53 (LINCOLN AVE.)—I-88 TO 59TH ST.</b> | F.A.P. RTE. | SECTION        | COUNTY                    | TOTAL SHEETS | SHEET NO. |  |
| pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI32218\Drawings\Design\DI32218-sh-t-plan.dgn |                    |            |           |   |  | FAP 870     | 2018-029-RS-SW | DU PAGE                   | 51           | 14        |  |
| PLOT SCALE = 100.0000' / in.  |                    |            |           |   |  | CHECKED -   | REVISED -      | CONTRACT NO. 62G23        |              |           |  |
| PLOT DATE = 2/1/2019  |                    |            |           |   |  | DATE -      | REVISED -      | ILLINOIS FED. AID PROJECT |              |           |  |
| SCALE:  |                    |            |           |   |  | SHEET NO.   | OF SHEETS      | STA.                      | TO STA.      |           |  |



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|------------------------------|------------|-----------|
| USER NAME = steedpa          | DESIGNED - | REVISED - |
| PLOT SCALE = 100,0000' / in. | DRAWN -    | REVISED - |
| PLOT DATE = 2/1/2019         | CHECKED -  | REVISED - |
|                              | DATE -     | REVISED - |

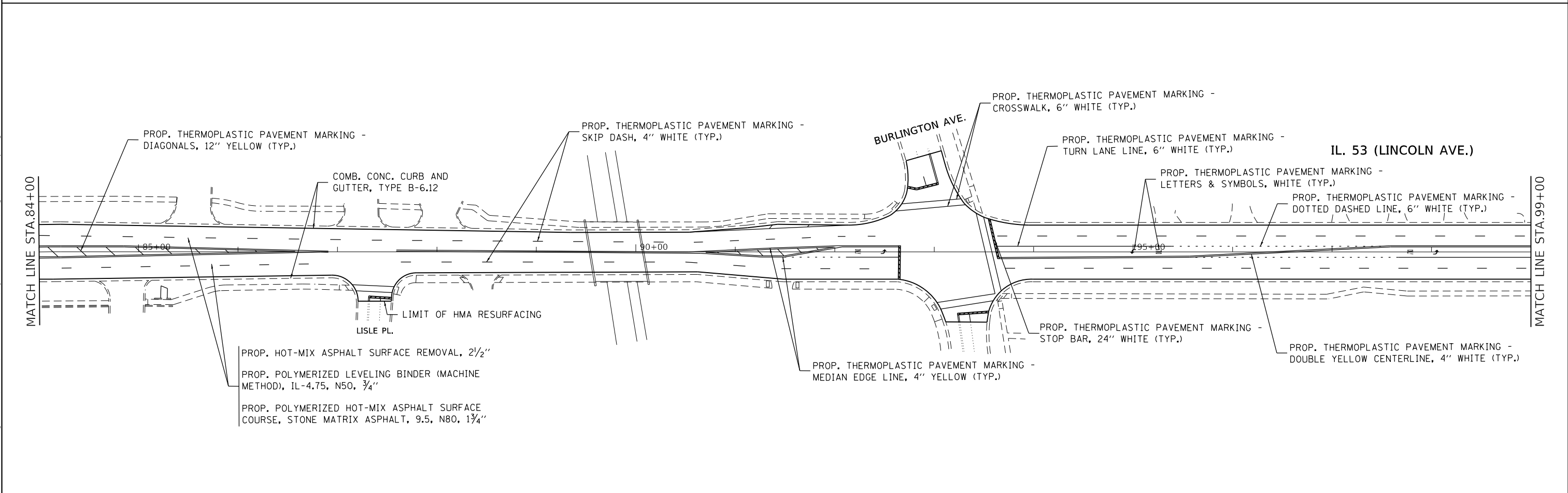
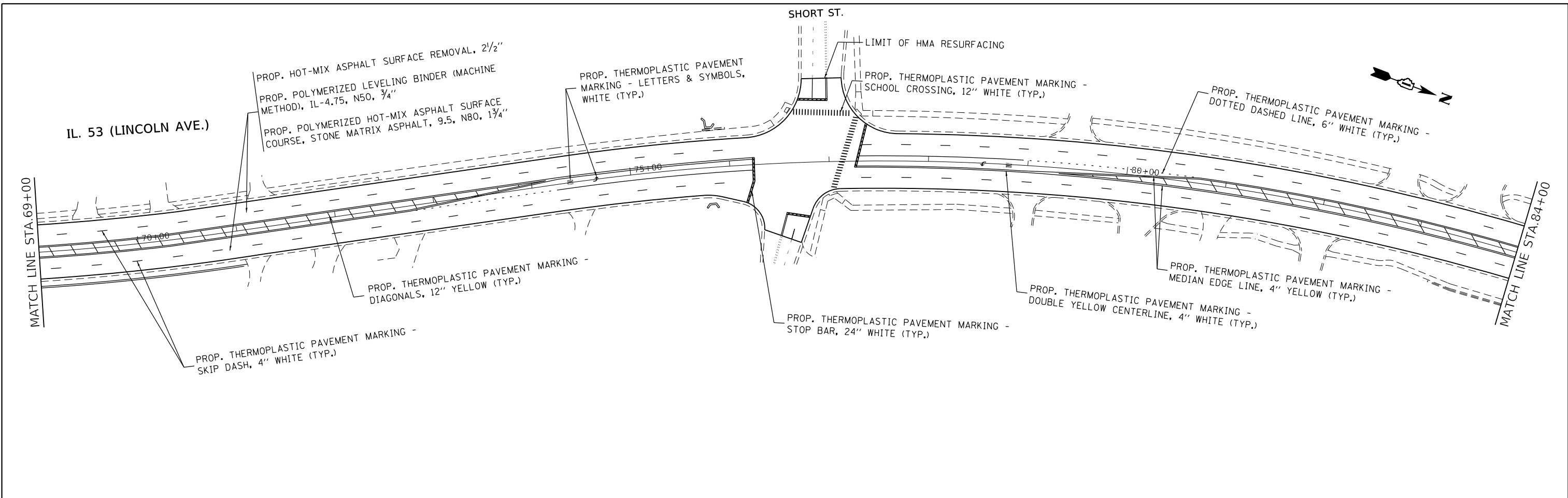
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN**  
**IL. ROUTE 53 (LINCOLN HWY.) (59TH ST. TO INTERSTATE 88)**  
 SCALE: 1" = 50'    SHEET    OF    SHEETS    STA.    TO    STA.

| F.A.P. RTE.        | SECTION        | COUNTY | TOTAL SHEETS              | SHEET NO. |
|--------------------|----------------|--------|---------------------------|-----------|
| 870                | 2018-029-R5-SW | DUPAGE | 51                        | 15        |
| CONTRACT NO. 62G23 |                |        | ILLINOIS FED. AID PROJECT |           |







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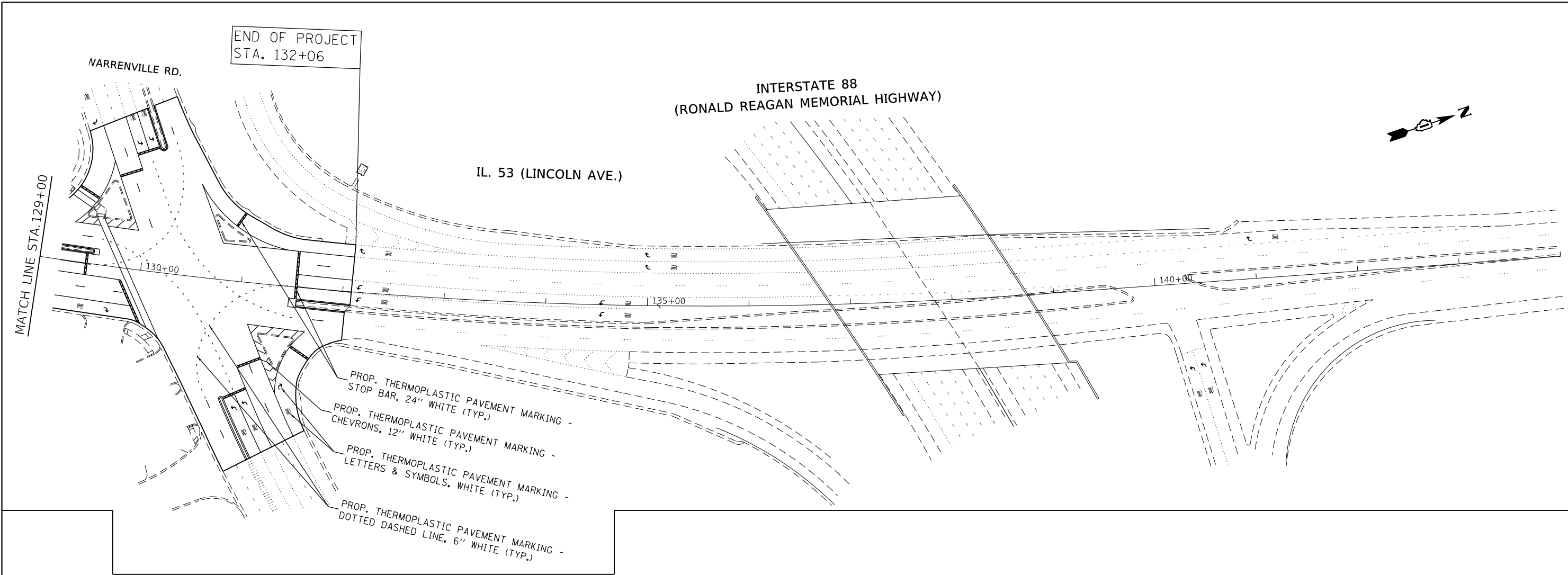
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|------------------------------|------------|-----------|
| USER NAME = steedpa          | DESIGNED - | REVISED - |
|                              | DRAWN -    | REVISED - |
| PLOT SCALE = 100.0000' / in. | CHECKED -  | REVISED - |
| PLOT DATE = 2/1/2019         | DATE -     | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN**  
**IL. ROUTE 53 (LINCOLN HWY.) (59TH ST. TO INTERSTATE 88)**  
 SCALE: 1" = 50'    SHEET    OF    SHEETS    STA.    TO    STA.

| F.A.P. RTE.               | SECTION        | COUNTY | TOTAL SHEETS       | SHEET NO. |
|---------------------------|----------------|--------|--------------------|-----------|
| 870                       | 2018-029-R5-SW | DUPAGE | 51                 | 17        |
|                           |                |        | CONTRACT NO. 62G23 |           |
| ILLINOIS FED. AID PROJECT |                |        |                    |           |





- PROP. THERMOPLASTIC PAVEMENT MARKING - STOP BAR, 24" WHITE (TYP.)
- PROP. THERMOPLASTIC PAVEMENT MARKING - CHEVRONS, 12" WHITE (TYP.)
- PROP. THERMOPLASTIC PAVEMENT MARKING - LETTERS & SYMBOLS, WHITE (TYP.)
- PROP. THERMOPLASTIC PAVEMENT MARKING - DOTTED DASHED LINE, 6" WHITE (TYP.)

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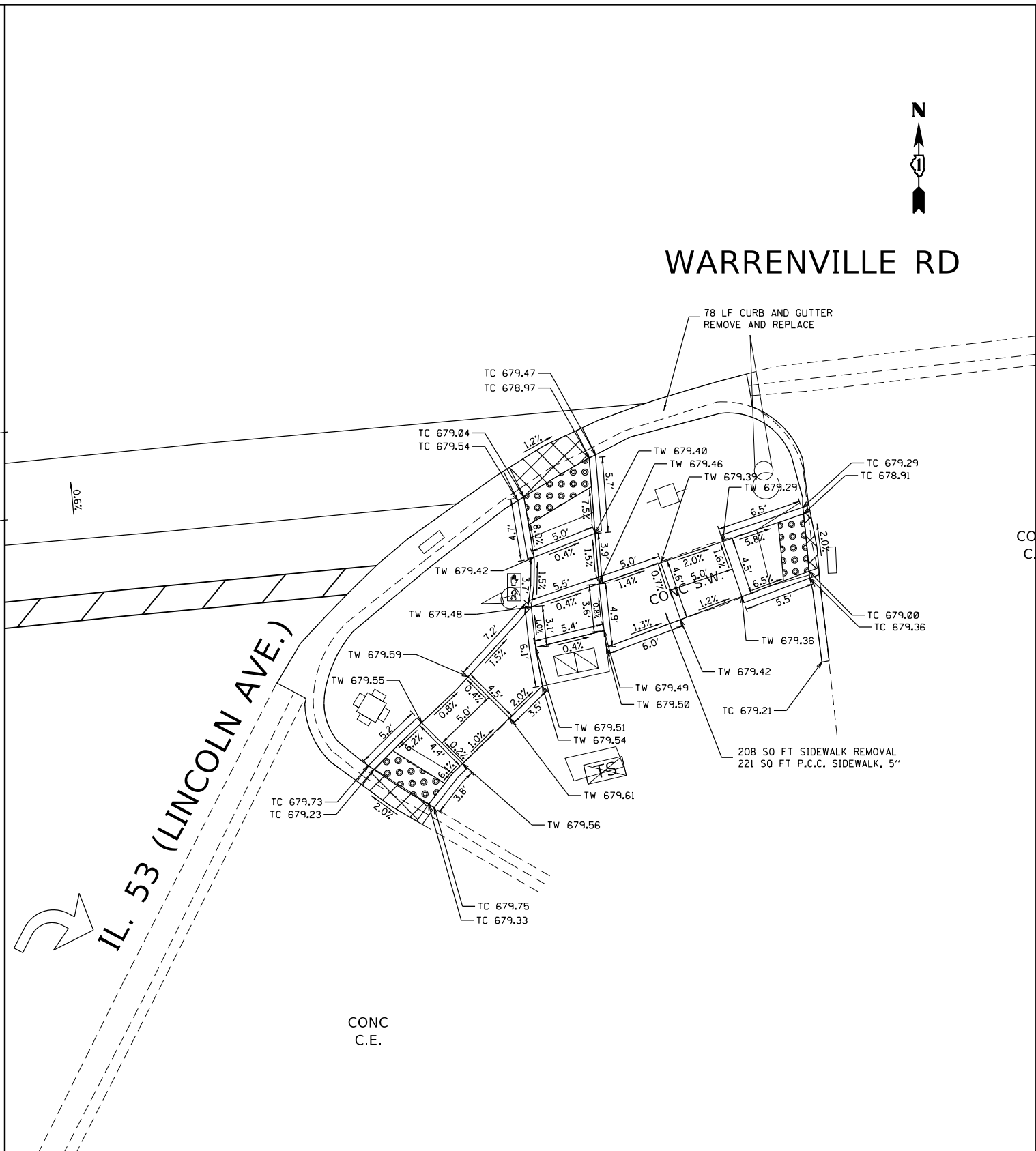
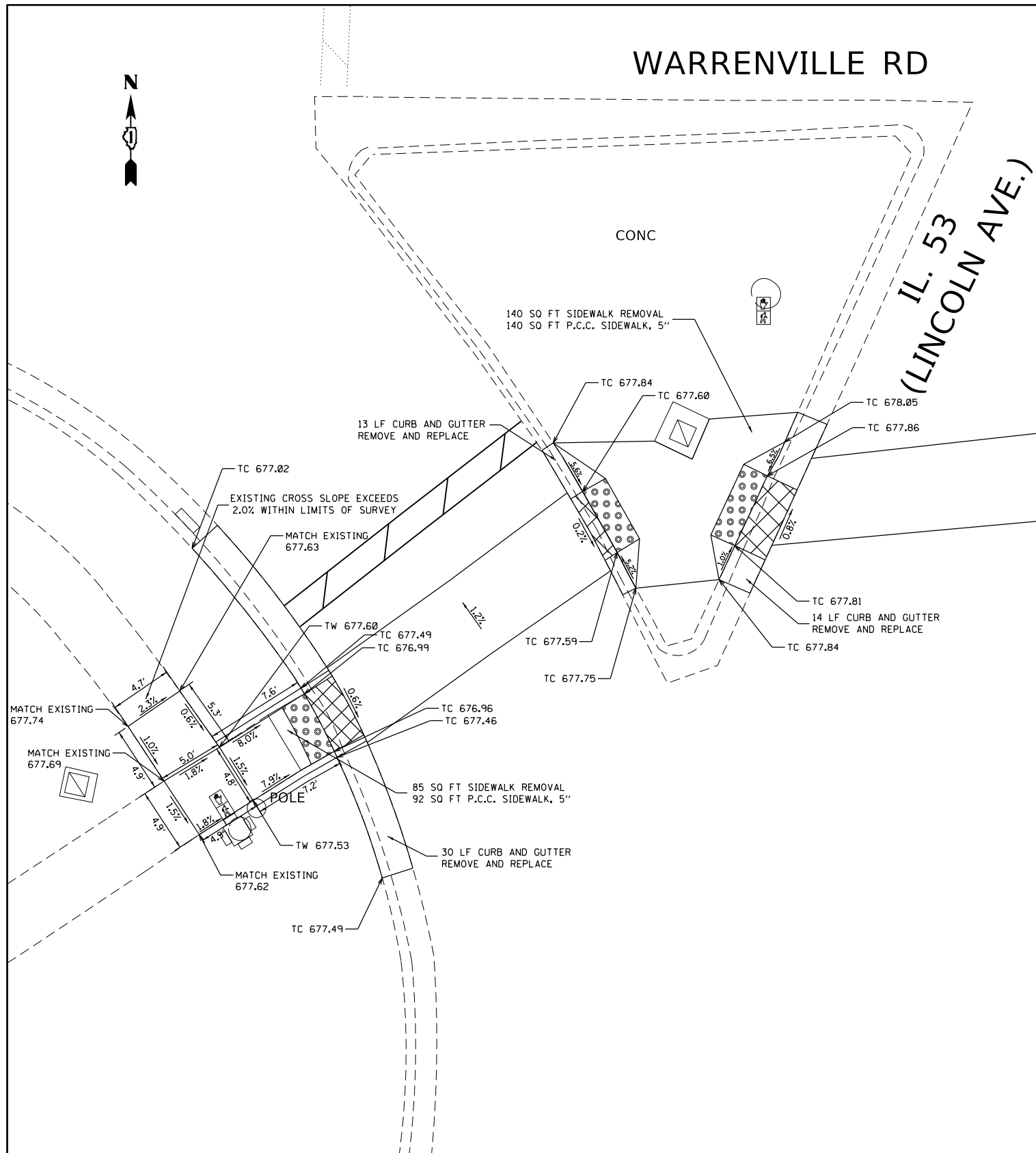
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| PLOT SCALE = 100,0000' / in. | DRAWN -    | REVISED - |
| PLOT DATE = 2/1/2019         | CHECKED -  | REVISED - |
|                              | DATE -     | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN**  
**IL. ROUTE 53 (LINCOLN HWY.) (59TH ST. TO INTERSTATE 88)**

|                    |                |                  |              |           |
|--------------------|----------------|------------------|--------------|-----------|
| F.A.P. RTE.        | SECTION        | COUNTY           | TOTAL SHEETS | SHEET NO. |
| 870                | 2018-029-R5-SW | DUPAGE           | 51           | 19        |
| CONTRACT NO. 62G23 |                |                  |              |           |
| ILLINOIS           |                | FED. AID PROJECT |              |           |

SCALE: 1" = 50'      SHEET      OF      SHEETS      STA.      TO STA.



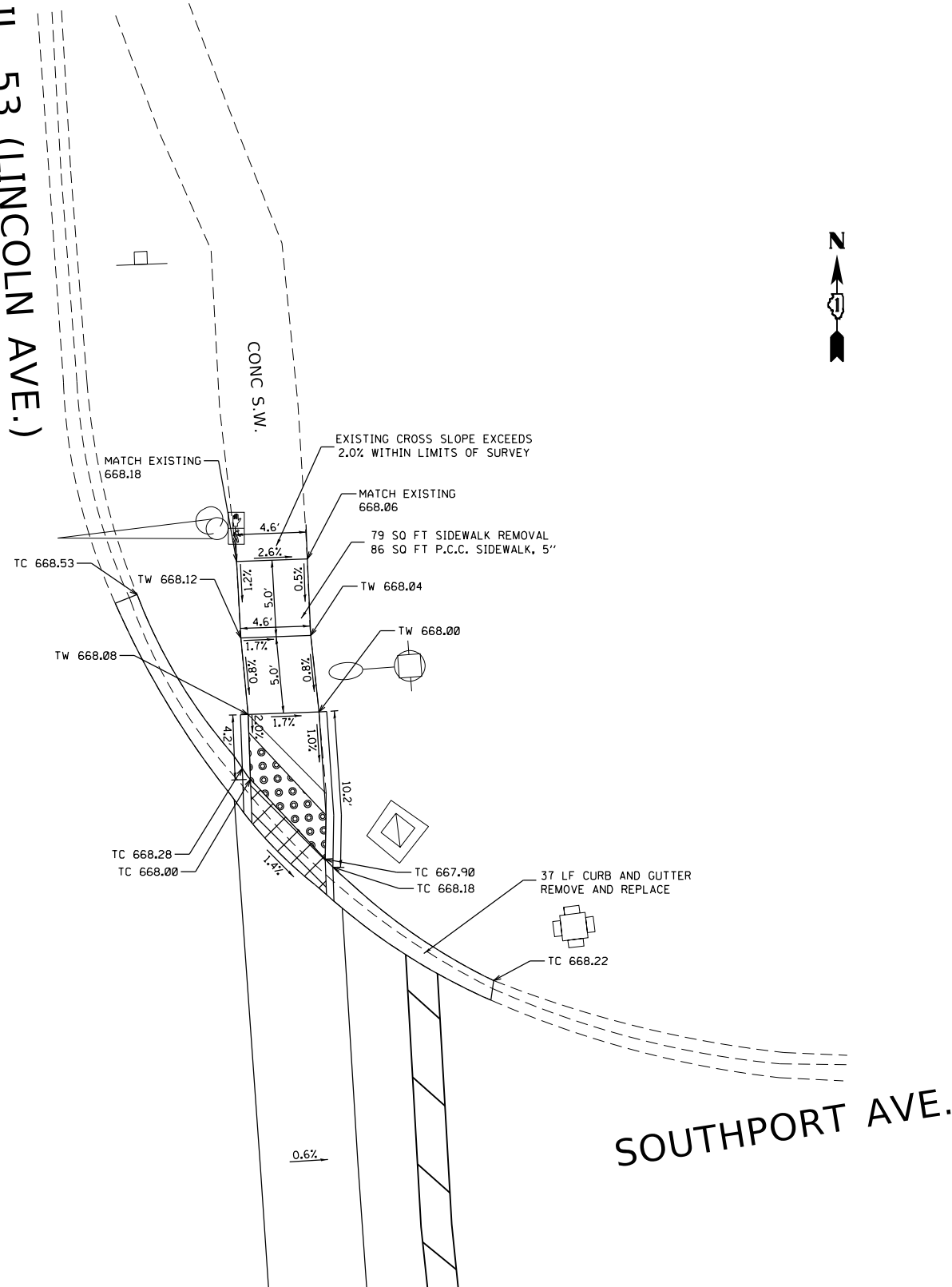
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 BENCHMARK : SET X ON LIGHT POLE  
 LOCATION : BASE AT ISLAND AT SOUTHWEST CORNER OF IL 53 (LINCOLN AVE.)  
 AT WARRENVILLE RD.

**LEGEND**

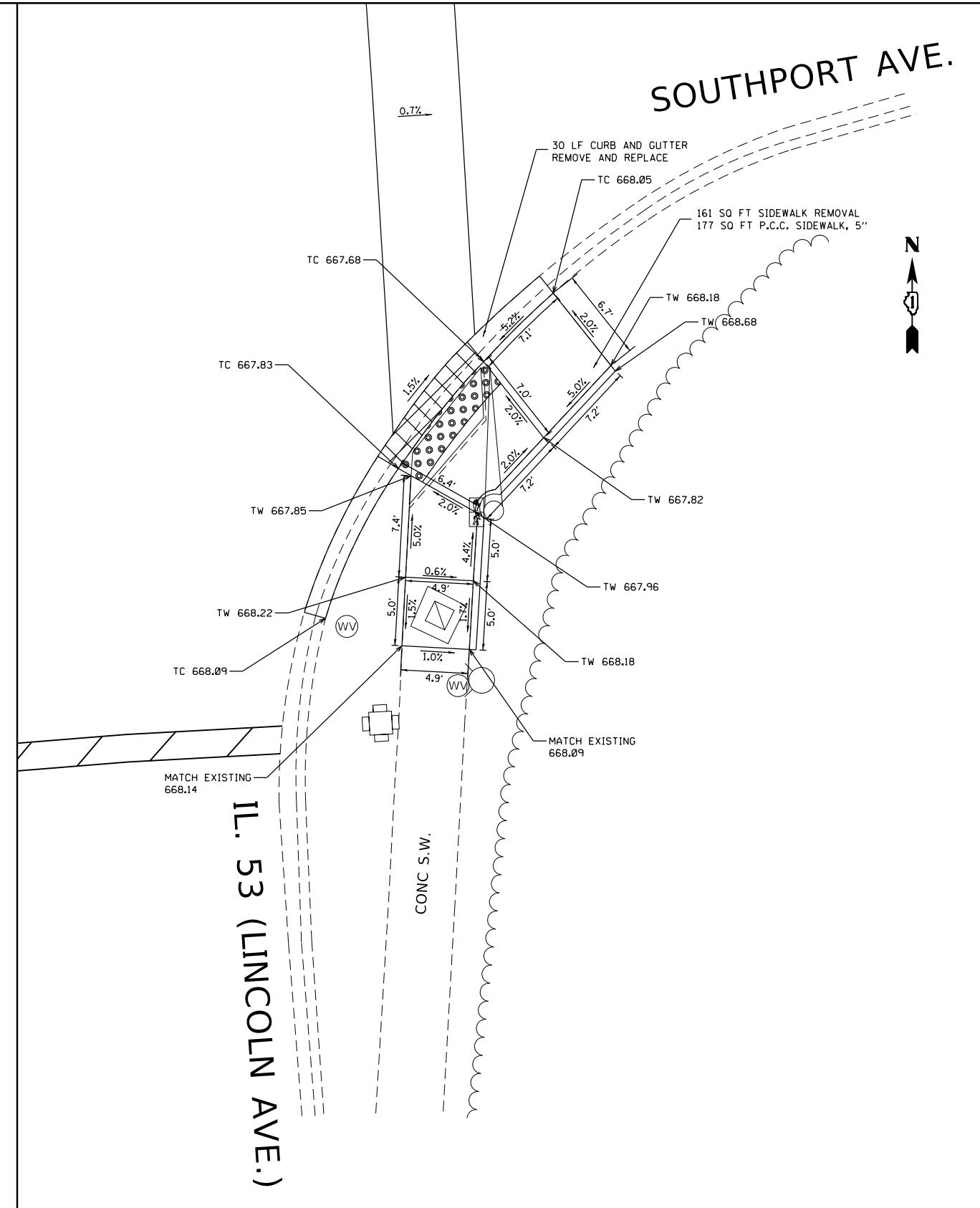
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|--|--------------------------|--|-------------------|
| <span style="border: 1px solid black; padding: 2px;">xx.xx'</span> | EXISTING LENGTH          |  | PROPOSED SIDEWALK |
|  | PROPOSED SIDE CURB       |  | DEPRESSIBLE CURB  |
| ( )  | EXISTING ELEVATION/SLOPE |  |                   |

REFERENCE BENCH MARK ELEV 678.29  
 BENCHMARK : SET X ON LIGHT POLE  
 LOCATION : BASE AT ISLAND AT SOUTHWEST CORNER OF IL 53 (LINCOLN AVE.)  
 AT WARRENVILLE RD.

IL. 53 (LINCOLN AVE.)



SOUTHPORT AVE.



REFERENCE BENCH MARK ELEV 668.61  
 BENCHMARK : SET "□" ON NORTHEAST CORNER OF TRAFFIC SIGNAL BOX  
 LOCATION : BASE AT NORTHWEST CORNER OF IL 53 (LINCOLN AVE.)  
 AT THE EXIT RAMP TO US 34 (OGDEN AVE.)

| LEGEND |                          |
|--------|--------------------------|
| xx.xx' | EXISTING LENGTH          |
| ==     | PROPOSED SIDE CURB       |
| ( )    | EXISTING ELEVATION/SLOPE |
| □      | PROPOSED SIDEWALK        |
| •••••  | DETECTABLE WARNINGS      |
| ▣      | DEPRESSED CURB           |

REFERENCE BENCH MARK ELEV 668.61  
 BENCHMARK : SET "□" ON NORTHEAST CORNER OF TRAFFIC SIGNAL BOX  
 LOCATION : BASE AT NORTHWEST CORNER OF IL 53 (LINCOLN AVE.)  
 AT THE EXIT RAMP TO US 34 (OGDEN AVE.)

|   |                             |            |           |
|---|-----------------------------|------------|-----------|
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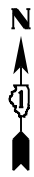
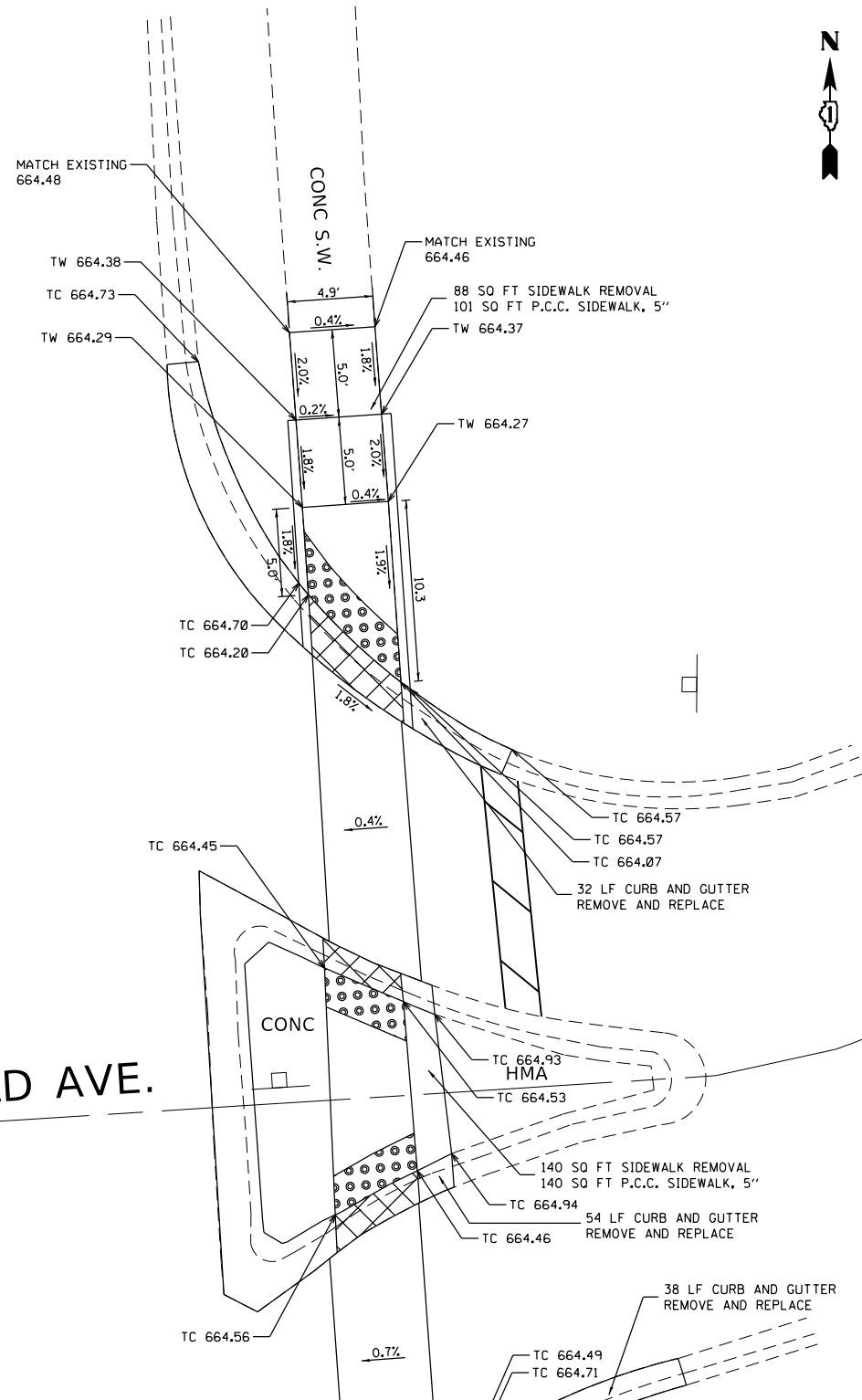
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

ADA DETAILS (AT SOUTHPORT AVE.-NE & SE CORNER)  
 FAP 870 IL 53 (LINCOLN AVE.)-I-88 TO 59TH ST.

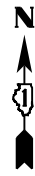
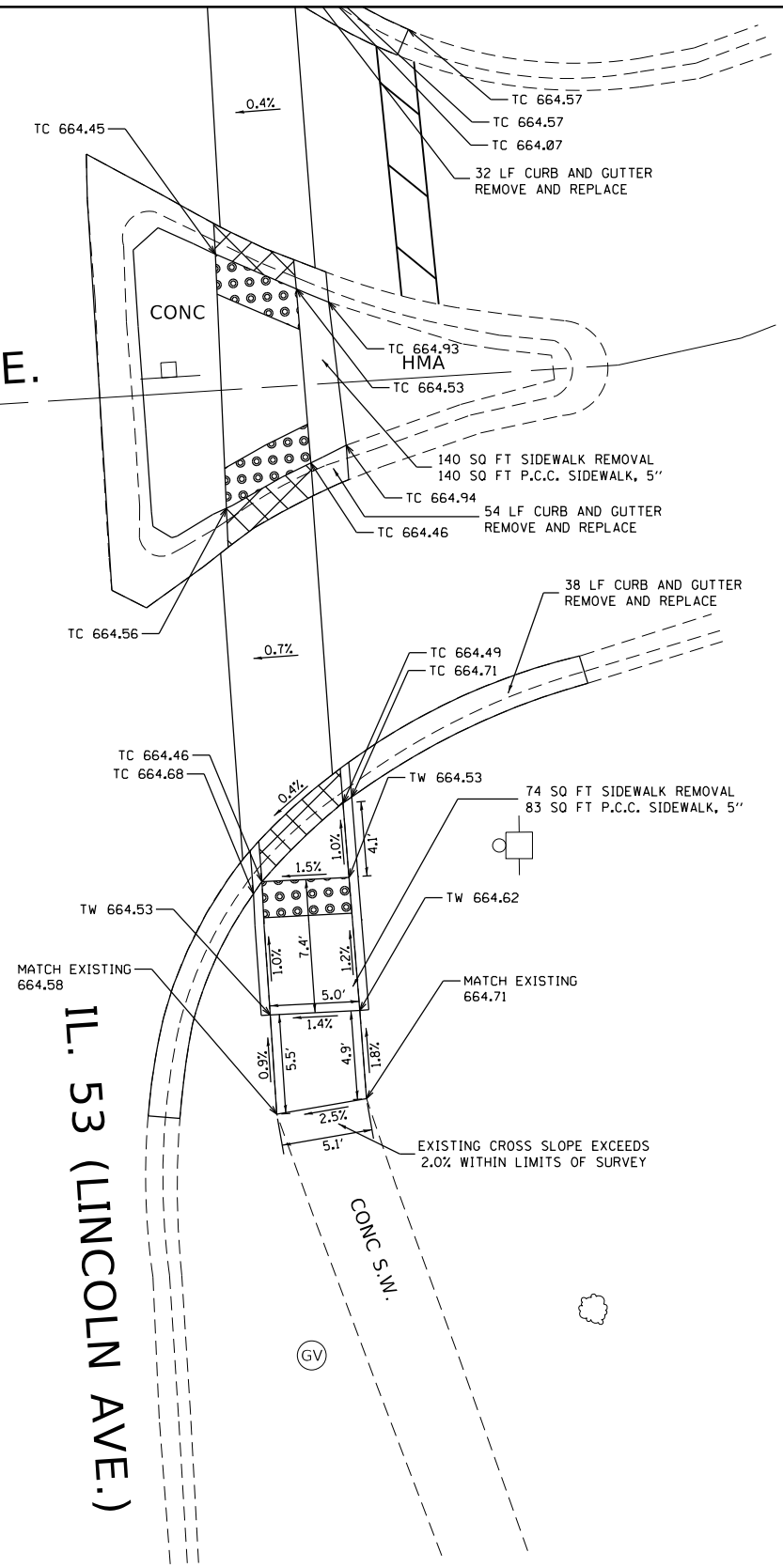
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|---------------------------|----------------|--------|--------------|-----------|
| F.A.P. RTE.               | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| 870                       | 2018-029-RS-SW | DUPAGE | 51           | 21        |
| CONTRACT NO. 62G23        |                |        |              |           |
| ILLINOIS FED. AID PROJECT |                |        |              |           |

SCALE: SHEET OF SHEETS STA. TO STA.

IL. 53 (LINCOLN AVE.)



GARFIELD AVE.



IL. 53 (LINCOLN AVE.)

REFERENCE BENCH MARK ELEV 666.22  
 BENCHMARK : SET X ON RAILROAD SPIKE INSIDE LIGHT POLE  
 LOCATION : LIGHT POLE AT SOUTHEAST CORNER OF IL 53 (LINCOLN AVE.)  
 AT GARFIELD AVE.

**LEGEND**

- xx.xx' EXISTING LENGTH
- PROPOSED SIDE CURB
- EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- DEPRESSED CURB

REFERENCE BENCH MARK ELEV 666.22  
 BENCHMARK : SET X ON RAILROAD SPIKE INSIDE LIGHT POLE  
 LOCATION : LIGHT POLE AT SOUTHEAST CORNER OF IL 53 (LINCOLN AVE.)  
 AT GARFIELD AVE.

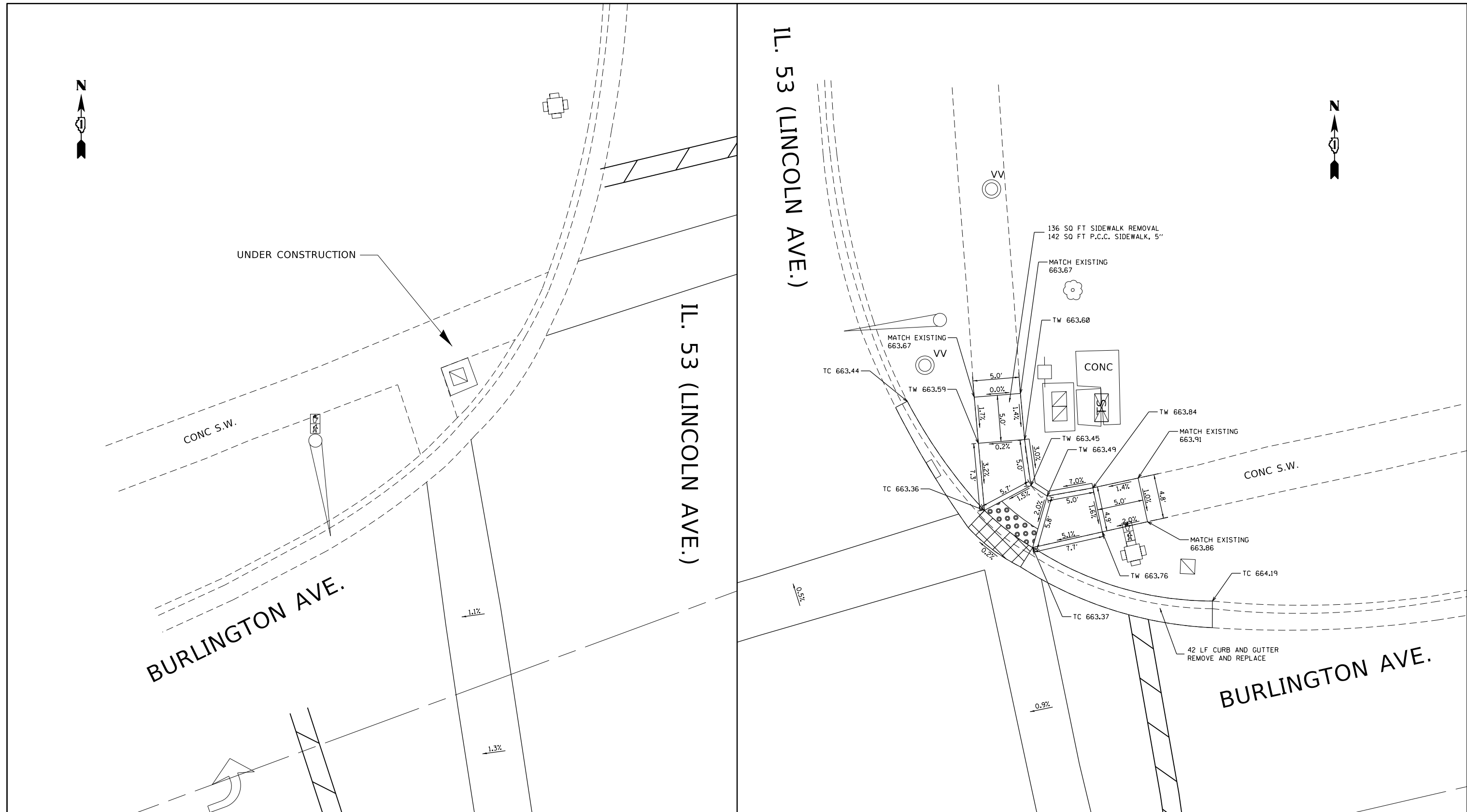
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| Default   | PLOT SCALE = 10.0000' / in. | CHECKED -  | REVISED - |
|   | PLOT DATE = 2/1/2019        | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ADA DETAILS (AT GARFIELD AVE.-NE & SE CORNER)  
 FAP 870 IL 53 (LINCOLN AVE.)-I-88 TO 59TH ST.**

SCALE: SHEET OF SHEETS STA. TO STA.

|                           |                |        |              |           |
|---------------------------|----------------|--------|--------------|-----------|
| F.A.P. RTE.               | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| 870                       | 2018-029-RS-SW | DUPAGE | 51           | 22        |
| CONTRACT NO. 62G23        |                |        |              |           |
| ILLINOIS FED. AID PROJECT |                |        |              |           |



REFERENCE BENCH MARK ELEV 664.40  
 BENCHMARK : SET "□" ON SOUTHWEST CORNER OF TRAFFIC SIGNAL BOX  
 LOCATION : BASE AT NORTHEAST CORNER OF IL 53 (LINCOLN AVE.)  
 AT BURLINGTON AVE.

**LEGEND**

|        |                          |  |                     |
|--------|--------------------------|--|---------------------|
| xx.xx' | EXISTING LENGTH          |  | PROPOSED SIDEWALK   |
|        | PROPOSED SIDE CURB       |  | DETECTABLE WARNINGS |
| ( )    | EXISTING ELEVATION/SLOPE |  | DEPRESSED CURB      |

REFERENCE BENCH MARK ELEV 664.40  
 BENCHMARK : SET "□" ON SOUTHWEST CORNER OF TRAFFIC SIGNAL BOX  
 LOCATION : BASE AT NORTHEAST CORNER OF IL 53 (LINCOLN AVE.)  
 AT BURLINGTON AVE.

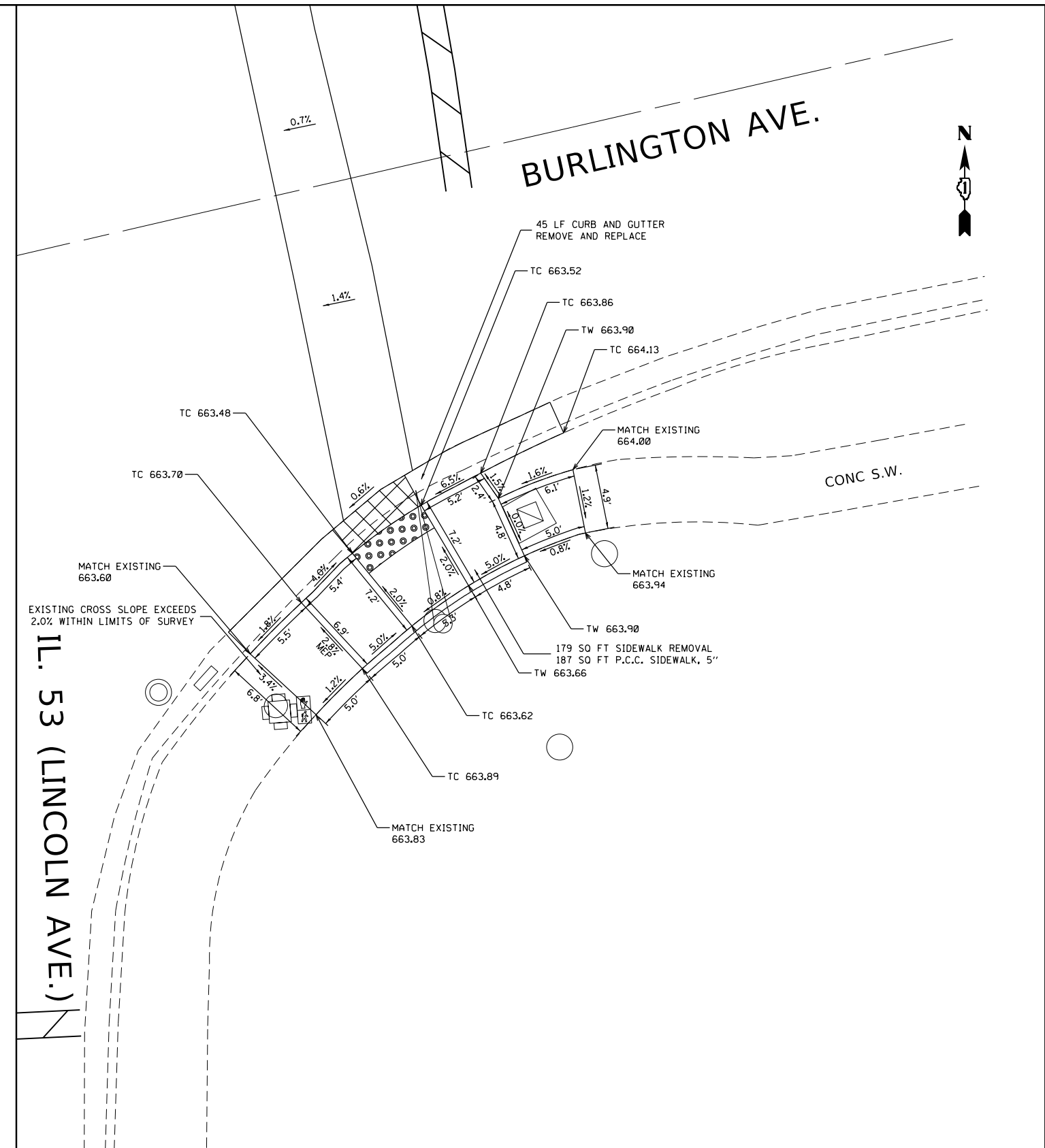
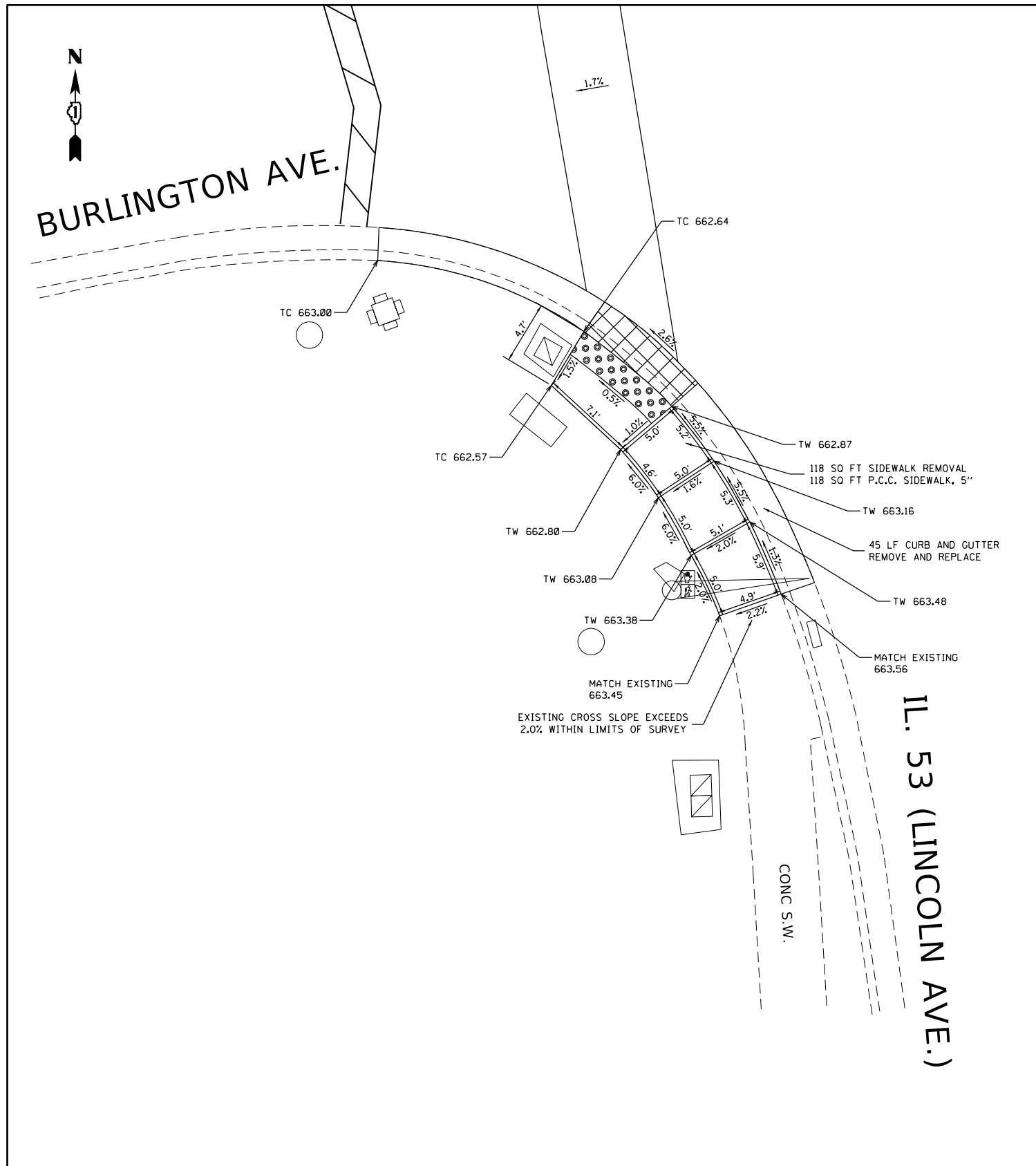
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|   | PLOT DATE = 3/8/2019        | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ADA DETAILS (AT BURLINGTON AVE.-NW & NE CORNERS)  
 FAP 870 IL 53 (LINCOLN AVE.)-1-88 TO 59TH ST.**

SCALE:      SHEET      OF      SHEETS      STA.      TO      STA.

|                           |                |        |              |           |
|---------------------------|----------------|--------|--------------|-----------|
| F.A.P. RTE.               | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| 870                       | 2018-029-RS-SW | DUPAGE | 51           | 23        |
| CONTRACT NO. 62G23        |                |        |              |           |
| ILLINOIS FED. AID PROJECT |                |        |              |           |



REFERENCE BENCH MARK ELEV 664.40  
 BENCHMARK : SET "□" ON SOUTHWEST CORNER OF TRAFFIC SIGNAL BOX  
 LOCATION : BASE AT NORTHEAST CORNER OF IL 53 (LINCOLN AVE.)  
 AT BURLINGTON AVE.

**LEGEND**

|        |                          |  |                     |
|--------|--------------------------|--|---------------------|
| xx.xx' | EXISTING LENGTH          |  | PROPOSED SIDEWALK   |
| ====   | PROPOSED SIDE CURB       |  | DETECTABLE WARNINGS |
| ( )    | EXISTING ELEVATION/SLOPE |  | DEPRESSED CURB      |

REFERENCE BENCH MARK ELEV 664.40  
 BENCHMARK : SET "□" ON SOUTHWEST CORNER OF TRAFFIC SIGNAL BOX  
 LOCATION : BASE AT NORTHEAST CORNER OF IL 53 (LINCOLN AVE.)  
 AT BURLINGTON AVE.

|  |                             |            |           |
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|  | PLOT DATE = 2/1/2019        | DATE -     | REVISED - |

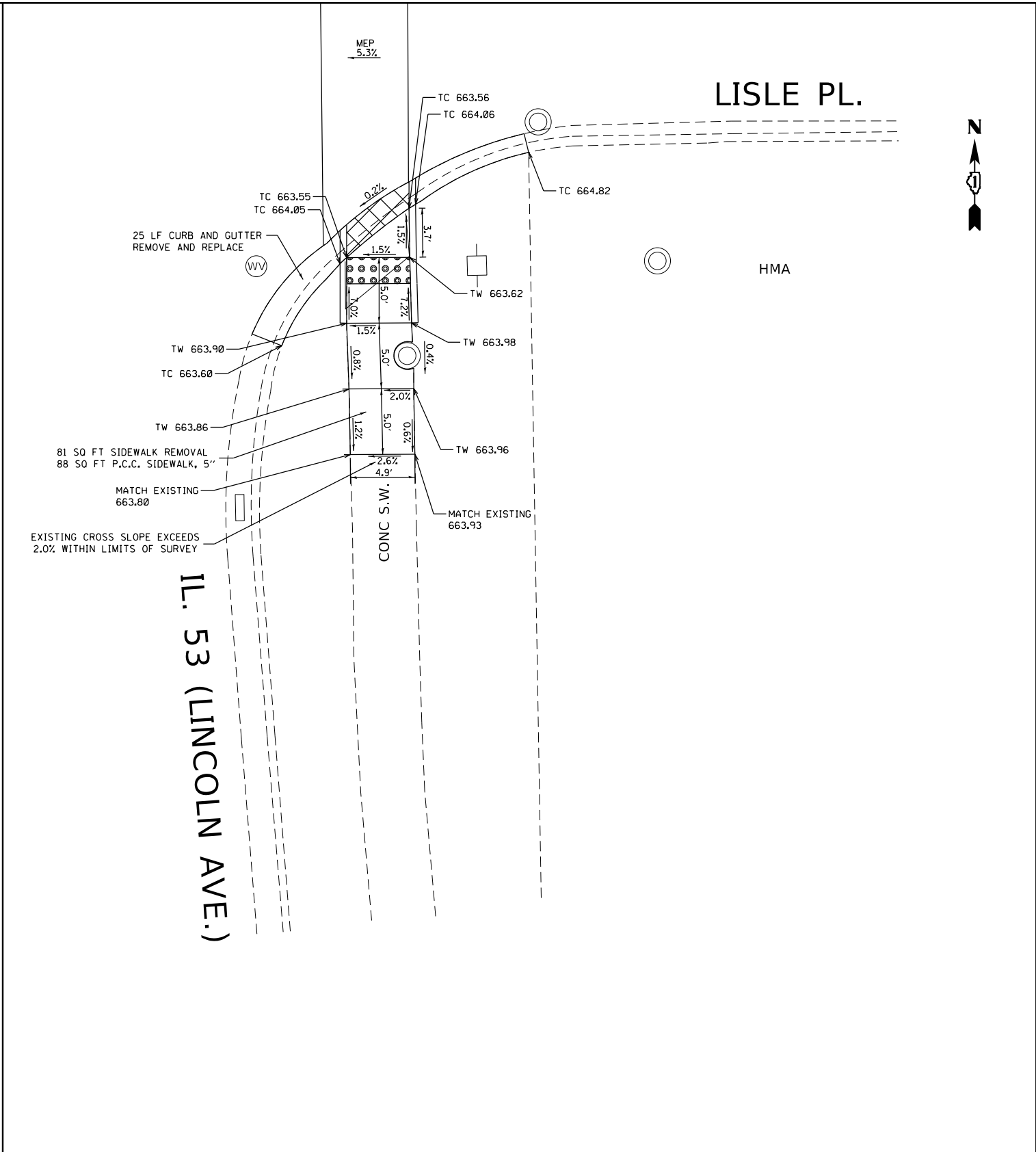
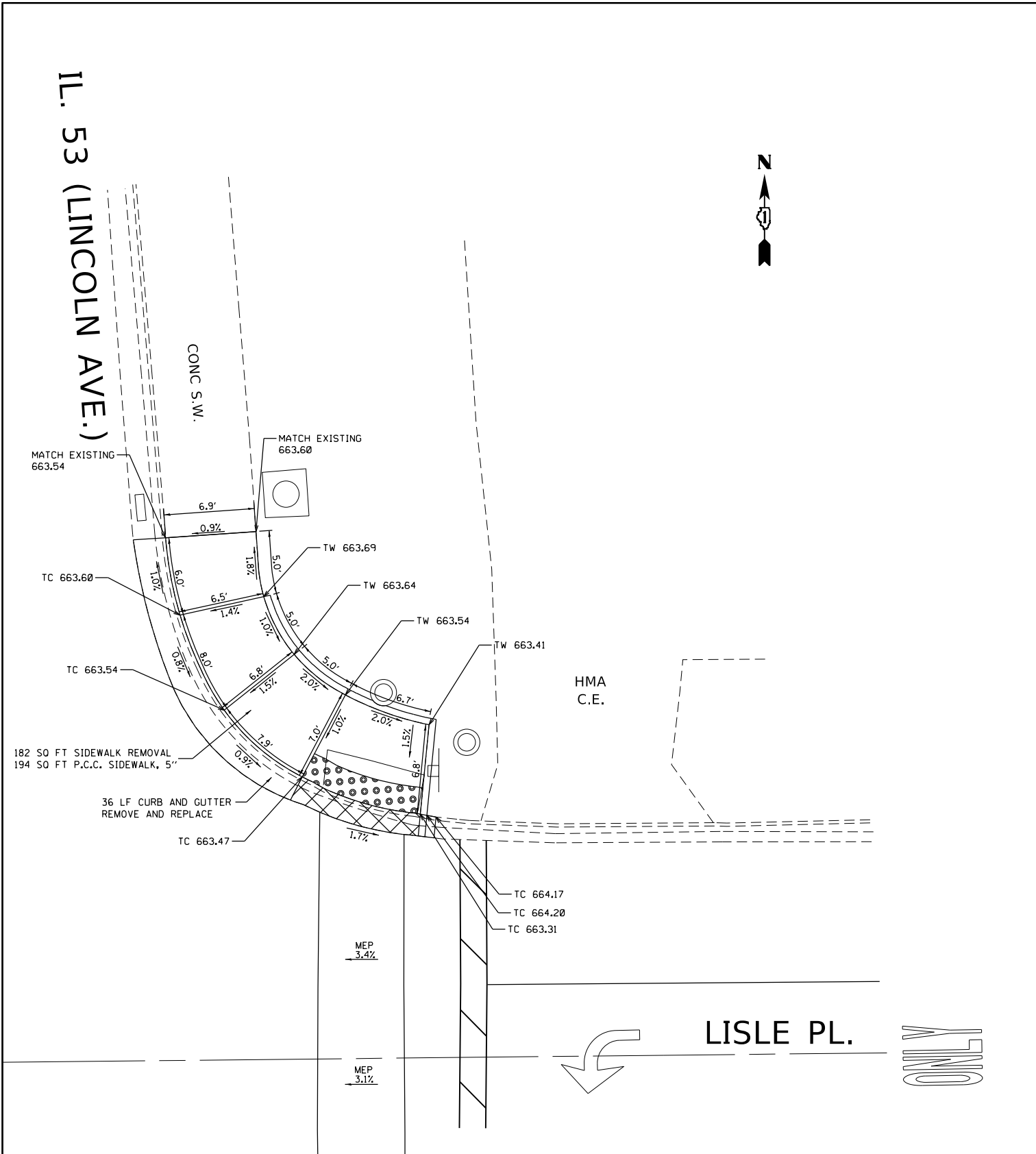
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ADA DETAILS (AT BURLINGTON AVE.-SW & SE CORNERS)  
 FAP 870 /IL 53 (LINCOLN AVE.)-I-88 TO 59TH ST.**

| F.A.P. RTE.               | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------|--------------|-----------|
| 870                       | 2018-029-RS-SW | DUPAGE | 51           | 24        |
| CONTRACT NO. 62G23        |                |        |              |           |
| ILLINOIS FED. AID PROJECT |                |        |              |           |

SCALE: SHEET OF SHEETS STA. TO STA.





REFERENCE BENCH MARK ELEV 665.85  
 BENCHMARK : SET X ON TOP SPINDAL OF FIRE HYDRANT  
 LOCATION : WEST SIDE OF IL 53 (LINCOLN AVE.) ±150' SOUTH OF LISLE PL. AND LISLE LANES BOWL.

**LEGEND**

|  |                          |   |                     |
|--|--------------------------|---|---------------------|
| <span style="border: 1px solid black; padding: 2px;">xx.xx'</span>                         | EXISTING LENGTH          | <span style="border: 1px solid black; width: 20px; height: 10px; display: inline-block;"></span>  | PROPOSED SIDEWALK   |
| <span style="border-bottom: 3px double black; width: 20px; display: inline-block;"></span> | PROPOSED SIDE CURB       | <span style="display: inline-block; width: 15px; height: 15px; background: repeating-linear-gradient(45deg, transparent, transparent 2px, black 2px, black 4px); border: 1px solid black;"></span>  | DETECTABLE WARNINGS |
| ( )  | EXISTING ELEVATION/SLOPE | <span style="display: inline-block; width: 15px; height: 15px; background: repeating-linear-gradient(-45deg, transparent, transparent 2px, black 2px, black 4px); border: 1px solid black;"></span> | DEPRESSED CURB      |

REFERENCE BENCH MARK ELEV 665.85  
 BENCHMARK : SET X ON TOP SPINDAL OF FIRE HYDRANT  
 LOCATION : WEST SIDE OF IL 53 (LINCOLN AVE.) ±150' SOUTH OF LISLE PL. AND LISLE LANES BOWL.

|  |                             |            |           |
|--|-----------------------------|------------|-----------|
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|  | PLOT DATE = 2/1/2019        | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

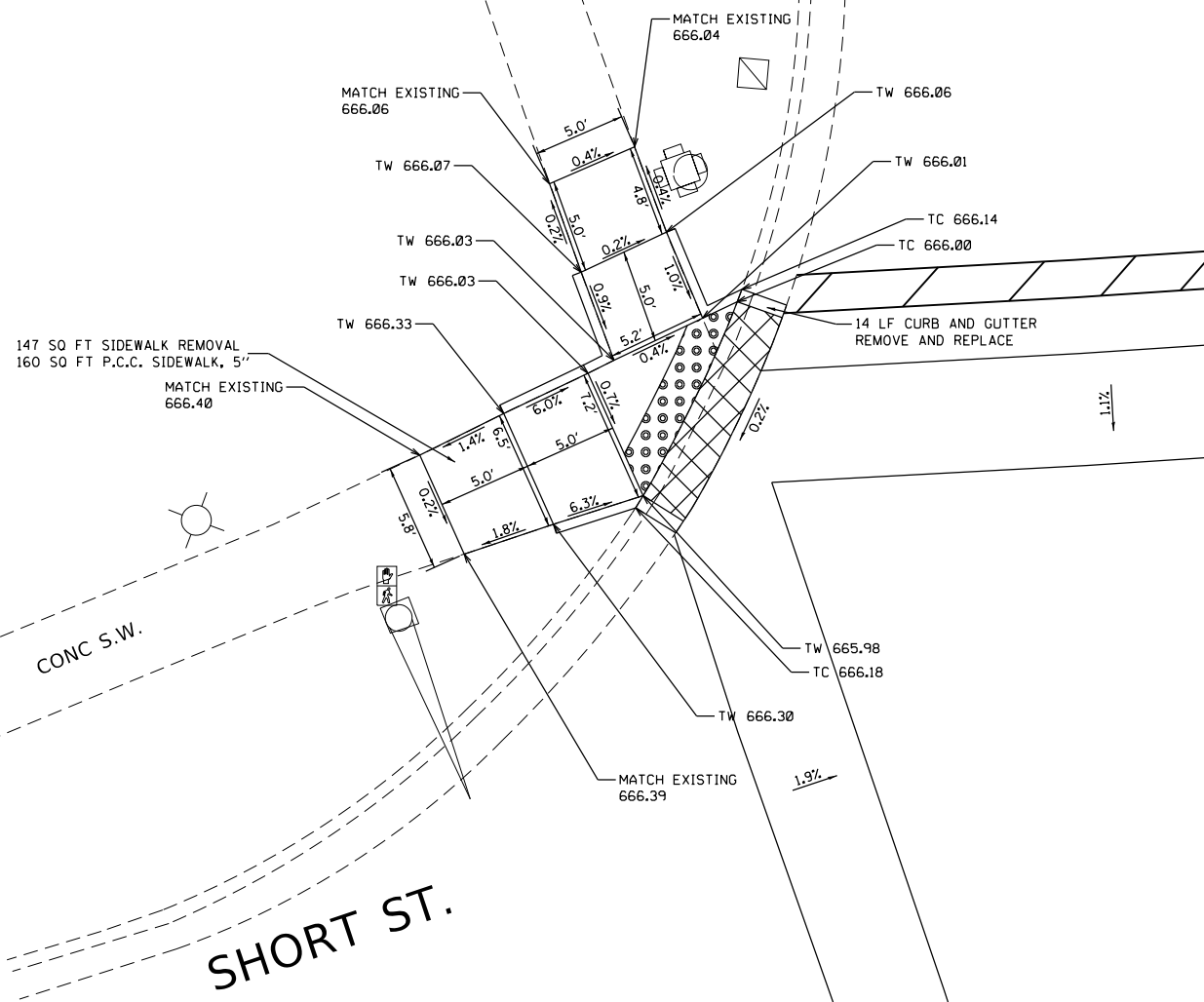
**ADA DETAILS (AT LISLE PL.-NE & SE CORNERS)  
 FAP 870 /IL 53 (LINCOLN AVE.)-I-88 TO 59TH ST.**

|                           |                |        |              |           |
|---------------------------|----------------|--------|--------------|-----------|
| F.A.P. RTE.               | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| 870                       | 2018-029-RS-SW | DUPAGE | 51           | 25        |
| CONTRACT NO. 62G23        |                |        |              |           |
| ILLINOIS FED. AID PROJECT |                |        |              |           |

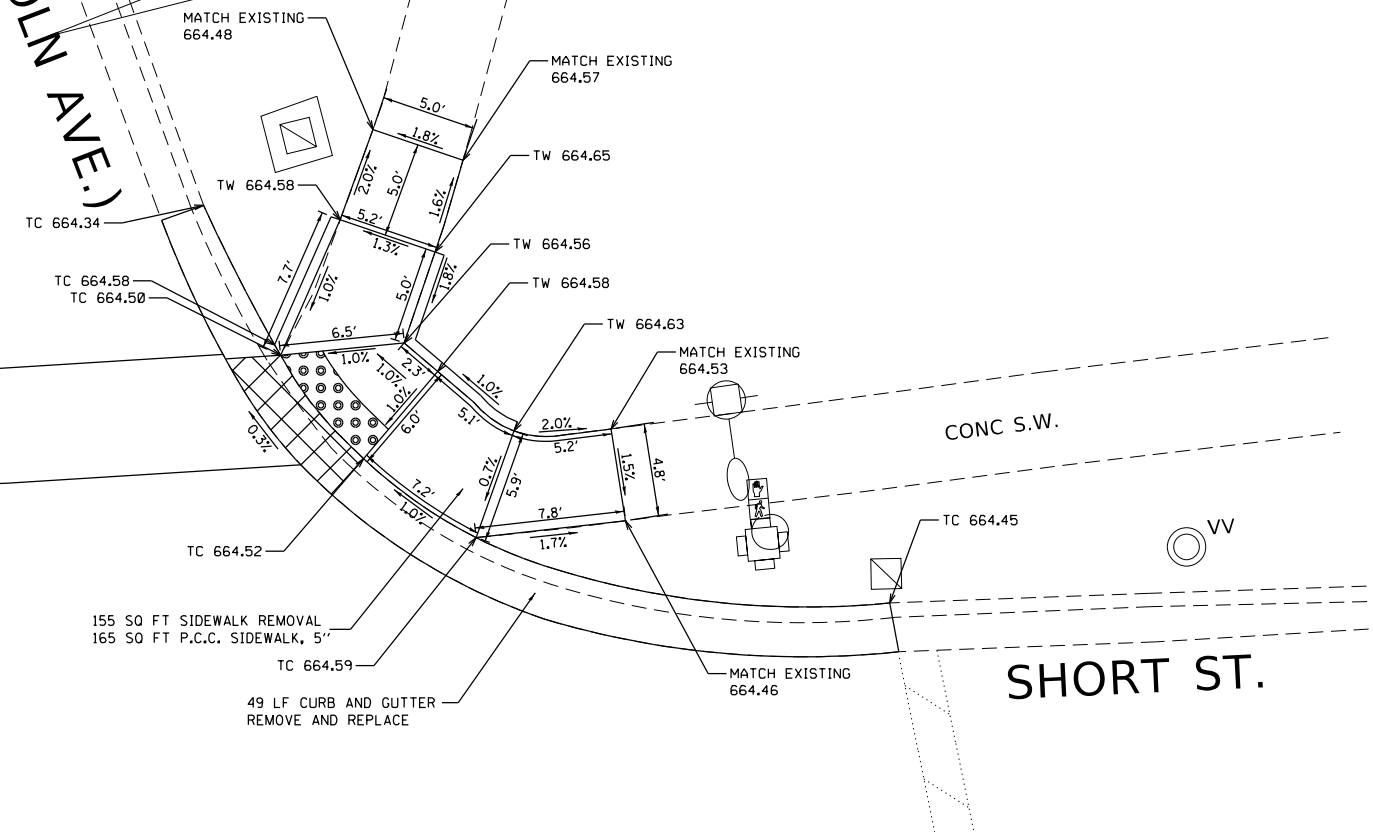
|        |       |    |        |      |    |      |
|--------|-------|----|--------|------|----|------|
| SCALE: | SHEET | OF | SHEETS | STA. | TO | STA. |
|--------|-------|----|--------|------|----|------|



IL. 53 (LINCOLN AVE.)



IL. 53 (LINCOLN AVE.)



REFERENCE BENCH MARK ELEV 665.05  
 BENCHMARK : SET "□" ON NORTHWEST CORNER OF TRAFFIC SIGNAL BOX  
 LOCATION : SOUTHEAST CORNER OF IL 53 (LINCOLN AVE.) AND SHORT ST.

**LEGEND**

- xx.xx' EXISTING LENGTH
- PROPOSED SIDE CURB
- ( ) EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- DEPRESSED CURB

REFERENCE BENCH MARK ELEV 665.05  
 BENCHMARK : SET "□" ON NORTHWEST CORNER OF TRAFFIC SIGNAL BOX  
 LOCATION : SOUTHEAST CORNER OF IL 53 (LINCOLN AVE.) AND SHORT ST.

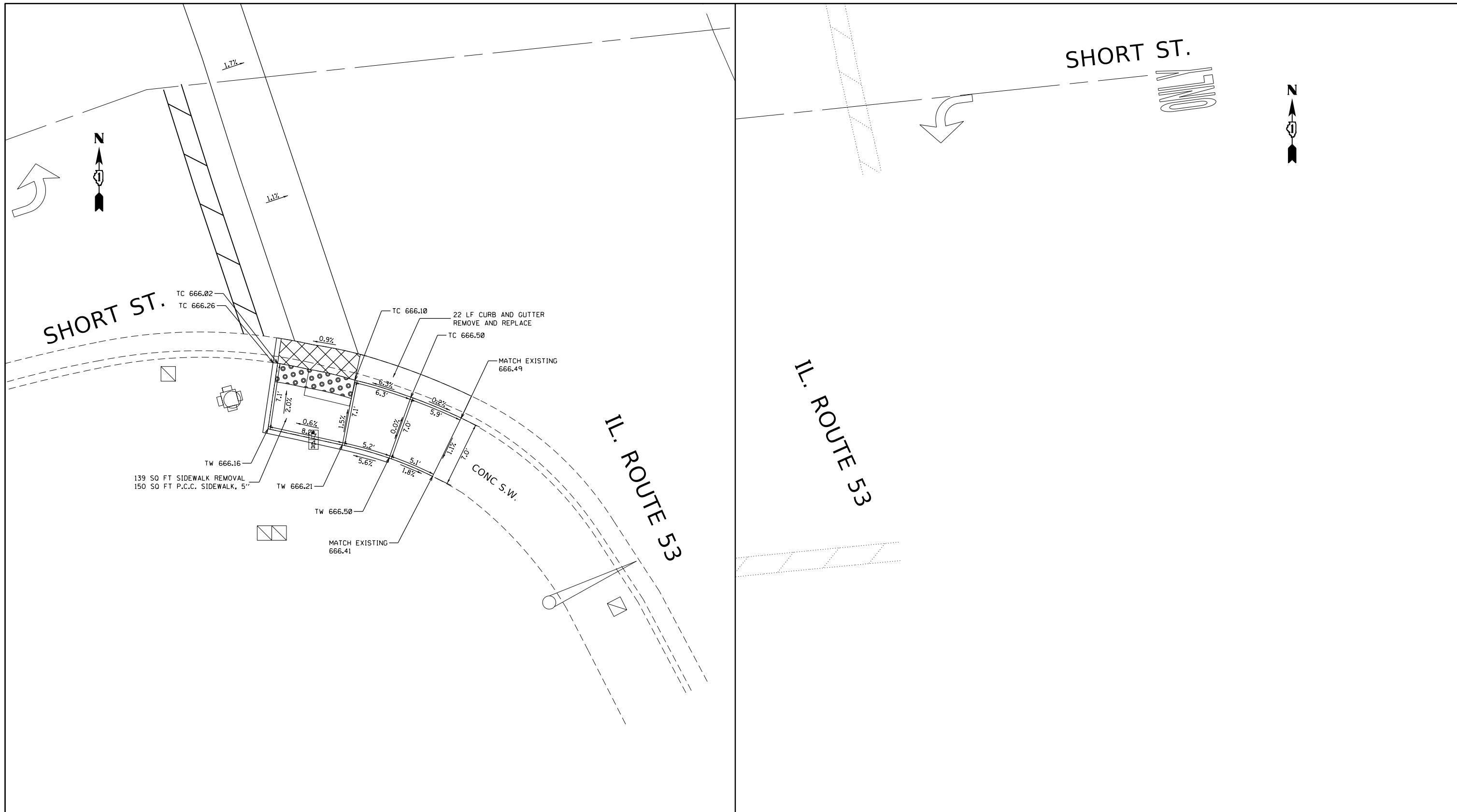
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| Default  | PLOT DATE = 3/6/2019 | CHECKED -  | REVISED - |
|  |                      | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ADA DETAILS (AT SHORT ST.-NE & NW CORNERS)  
 FAP 870 /IL 53 (LINCOLN AVE.)—I-88 TO 59TH ST.**

SCALE: SHEET OF SHEETS STA. TO STA.

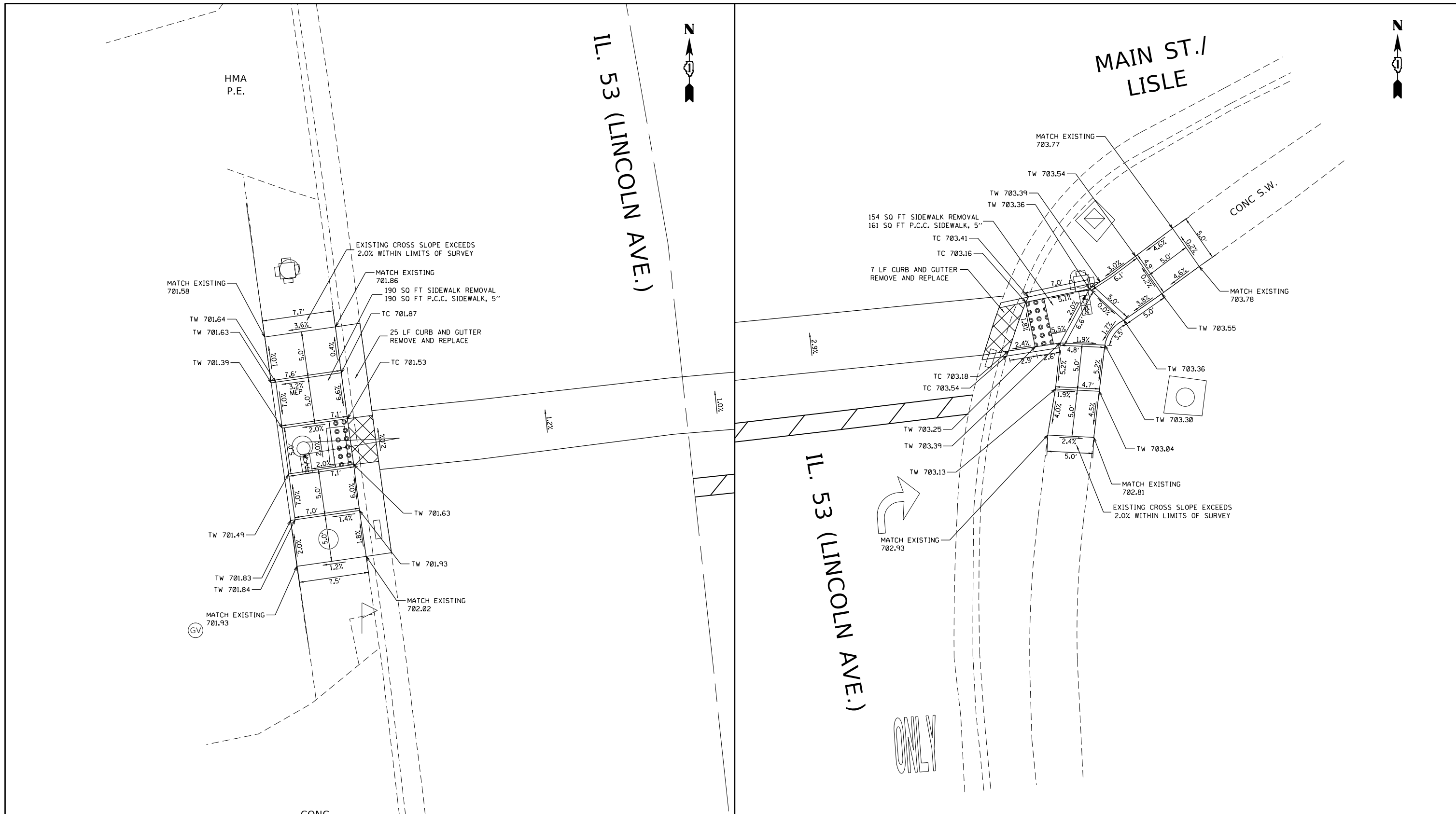
| F.A.P. RTE.               | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------|--------------|-----------|
| 870                       | 2018-029-RS-SW | DUPAGE | 51           | 26        |
| CONTRACT NO. 62G23        |                |        |              |           |
| ILLINOIS FED. AID PROJECT |                |        |              |           |



REFERENCE BENCH MARK ELEV 665.05  
 BENCHMARK : SET "□" ON NORTHWEST CORNER OF TRAFFIC SIGNAL BOX  
 LOCATION : SOUTHEAST CORNER OF IL 53 (LINCOLN AVE.) AND SHORT ST.

| LEGEND |                          |
|--------|--------------------------|
| xx.xx' | EXISTING LENGTH          |
| ==     | PROPOSED SIDE CURB       |
| ( )    | EXISTING ELEVATION/SLOPE |
| □      | PROPOSED SIDEWALK        |
| •••••  | DETECTABLE WARNINGS      |
| ▣      | DEPRESSED CURB           |

REFERENCE BENCH MARK ELEV 665.05  
 BENCHMARK : SET "□" ON NORTHWEST CORNER OF TRAFFIC SIGNAL BOX  
 LOCATION : SOUTHEAST CORNER OF IL 53 (LINCOLN AVE.) AND SHORT ST.



REFERENCE BENCH MARK ELEV 703.54  
 BENCHMARK : SET "□" ON NORTHWEST CORNER OF TRAFFIC SIGNAL BOX  
 LOCATION : NORTHEAST CORNER OF IL 53 (LINCOLN AVE.) AND MAIN ST.

| LEGEND |                          |
|--------|--------------------------|
| xx.xx' | EXISTING LENGTH          |
| ==     | PROPOSED SIDE CURB       |
| ( )    | EXISTING ELEVATION/SLOPE |
| □      | PROPOSED SIDEWALK        |
| •••••  | DETECTABLE WARNINGS      |
| ▣      | DEPRESSED CURB           |

REFERENCE BENCH MARK ELEV 703.54  
 BENCHMARK : SET "□" ON NORTHWEST CORNER OF TRAFFIC SIGNAL BOX  
 LOCATION : NORTHEAST CORNER OF IL 53 (LINCOLN AVE.) AND MAIN ST.

|   |                             |            |           |
|---|-----------------------------|------------|-----------|
| FILE NAME =   | USER NAME = steedp          | DESIGNED - | REVISED - |
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|   | PLOT DATE = 2/1/2019        | DATE -     | REVISED - |

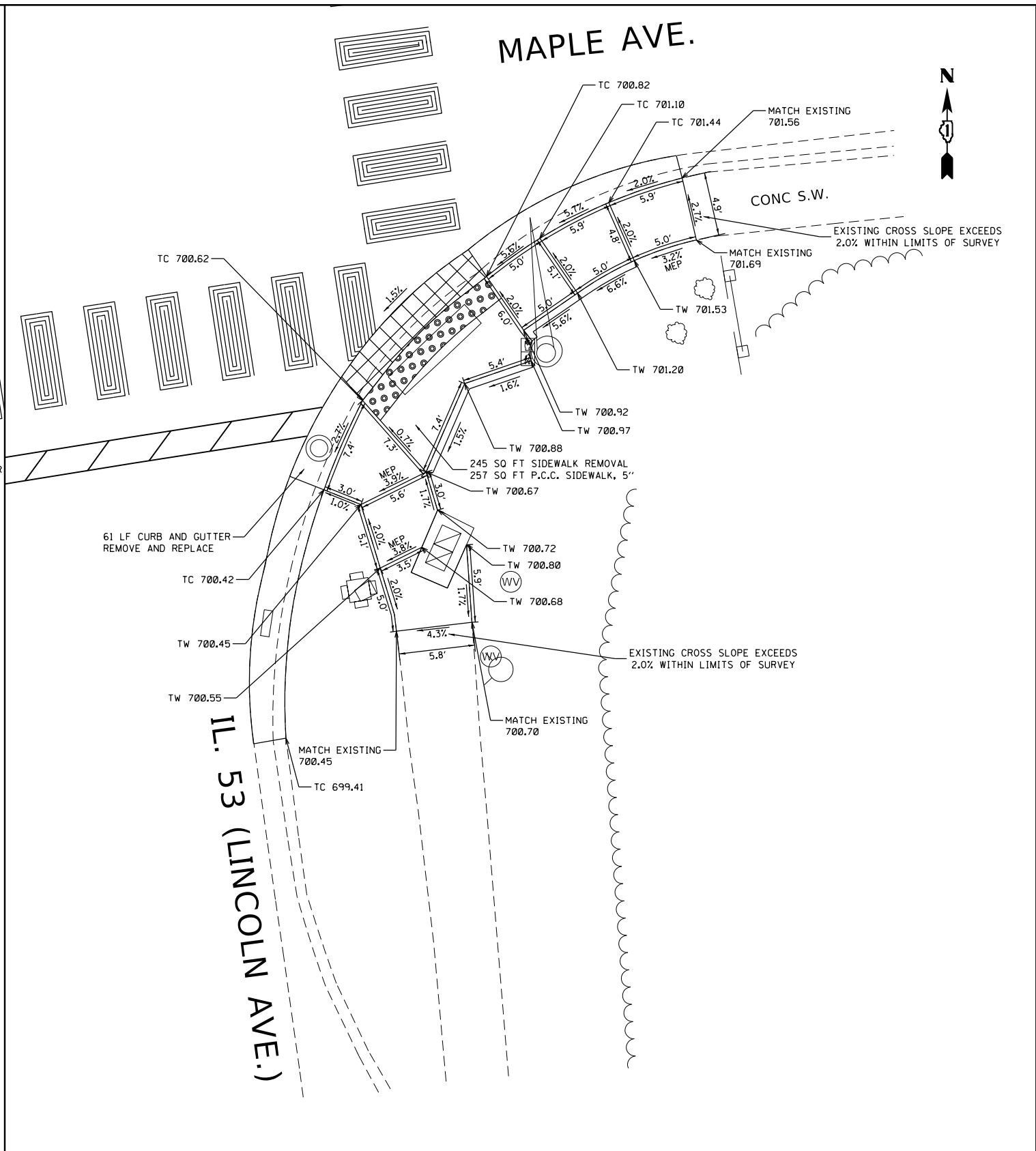
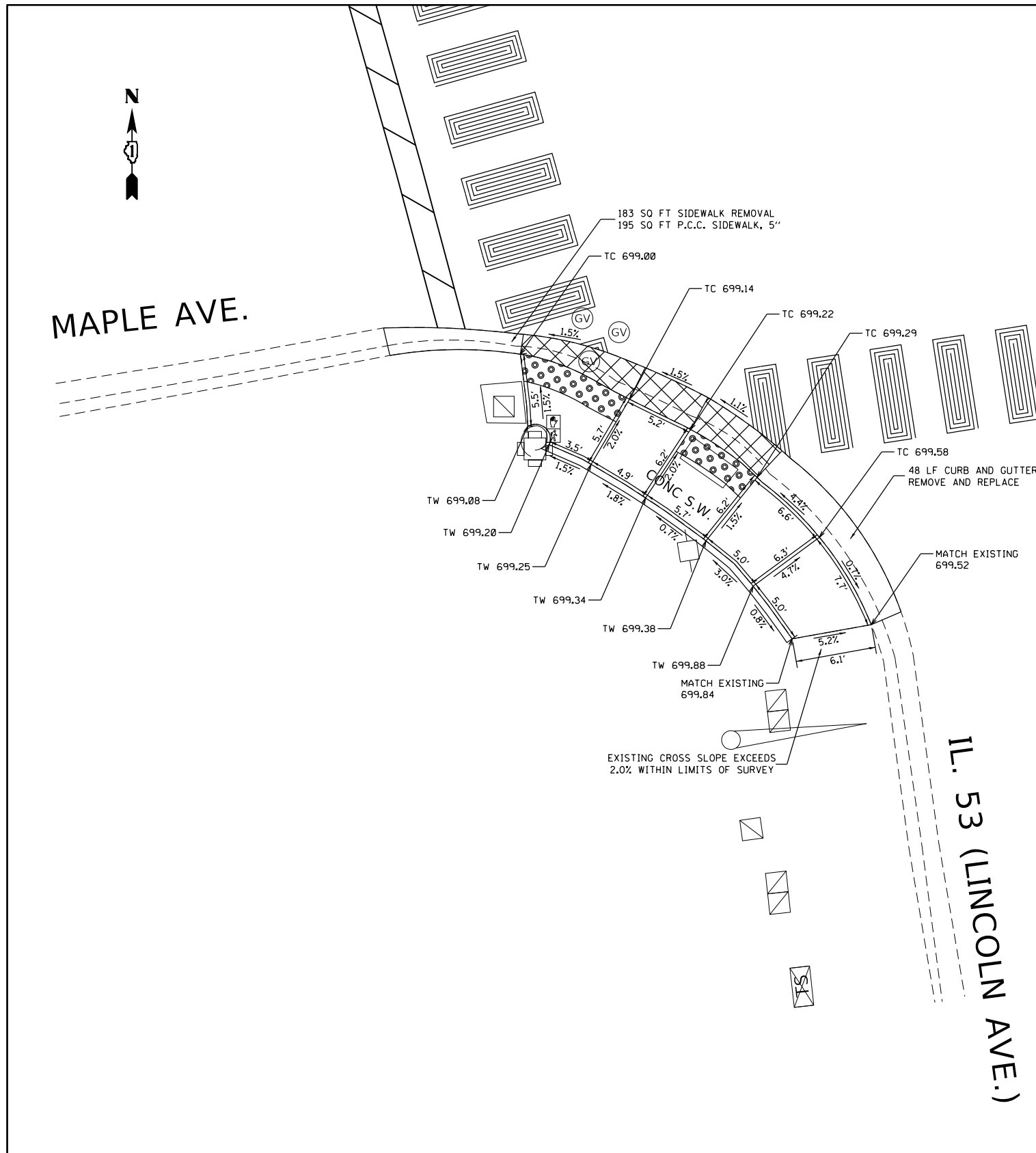
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ADA DETAILS (AT MAIN ST./LISLE-SW & SE CORNERS)  
 FAP 870 /IL 53 (LINCOLN AVE.)—I-88 TO 59TH ST.**

|                           |                |        |              |           |
|---------------------------|----------------|--------|--------------|-----------|
| F.A.P. RTE.               | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| 870                       | 2018-029-RS-SW | DUPAGE | 51           | 28        |
| CONTRACT NO. 62G23        |                |        |              |           |
| ILLINOIS FED. AID PROJECT |                |        |              |           |

SCALE: SHEET OF SHEETS STA. TO STA.





REFERENCE BENCH MARK ELEV 703.56  
 BENCHMARK : SET X ON TOP SPINDAL OF FIRE HYDRANT  
 LOCATION : SOUTHEAST CORNER OF MAPLE AVE. AND IL 53 (LINCOLN AVE.)

**LEGEND**

|  |                          |  |                     |
|--|--------------------------|--|---------------------|
|  | EXISTING LENGTH          |  | PROPOSED SIDEWALK   |
|  | PROPOSED SIDE CURB       |  | DETECTABLE WARNINGS |
|  | EXISTING ELEVATION/SLOPE |  | DEPRESSED CURB      |

REFERENCE BENCH MARK ELEV 703.56  
 BENCHMARK : SET X ON TOP SPINDAL OF FIRE HYDRANT  
 LOCATION : SOUTHEAST CORNER OF MAPLE AVE. AND IL 53 (LINCOLN AVE.)

|  |                      |            |           |
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| FILE NAME =  | USER NAME = steedpo  | DESIGNED - | REVISED - |
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| Default  | PLOT DATE = 3/8/2019 | CHECKED -  | REVISED - |
|  |                      | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

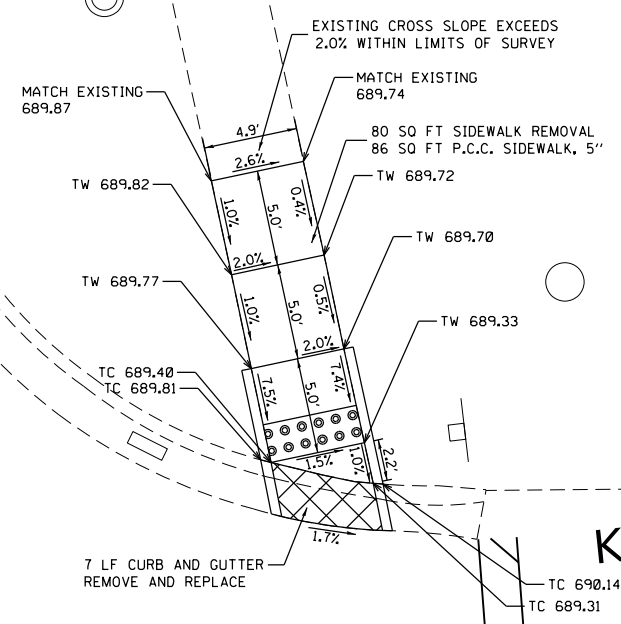
**ADA DETAILS (AT MAPLE AVE.-SW & SE CORNERS)  
 FAP 870 /IL 53 (LINCOLN AVE.)—1-88 TO 59TH ST.**

|                           |                |        |              |           |
|---------------------------|----------------|--------|--------------|-----------|
| F.A.P. RTE.               | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| 870                       | 2018-029-RS-SW | DUPAGE | 51           | 30        |
| CONTRACT NO. 62G23        |                |        |              |           |
| ILLINOIS FED. AID PROJECT |                |        |              |           |

SCALE: SHEET OF SHEETS STA. TO STA.

IL. 53 (LINCOLN AVE.)

CONC S.W.



KOHLEY RD.

REFERENCE BENCH MARK ELEV 690.00  
 BENCHMARK : SET X ON TOP SPINDAL OF FIRE HYDRANT  
 LOCATION : SOUTHEAST CORNER OF IL 53 (LINCOLN AVE.) AND KOHLEY RD.

**LEGEND**

- xx.xx' EXISTING LENGTH
- PROPOSED SIDE CURB
- ( ) EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- DEPRESSED CURB

REFERENCE BENCH MARK ELEV 690.00  
 BENCHMARK : SET X ON TOP SPINDAL OF FIRE HYDRANT  
 LOCATION : SOUTHEAST CORNER OF IL 53 (LINCOLN AVE.) AND KOHLEY RD.

|  |                             |            |           |
|--|-----------------------------|------------|-----------|
| FILE NAME =  | USER NAME = steedpa         | DESIGNED - | REVISED - |
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| Default  | PLOT SCALE = 10.0000' / in. | CHECKED -  | REVISED - |
|  | PLOT DATE = 2/1/2019        | DATE -     | REVISED - |

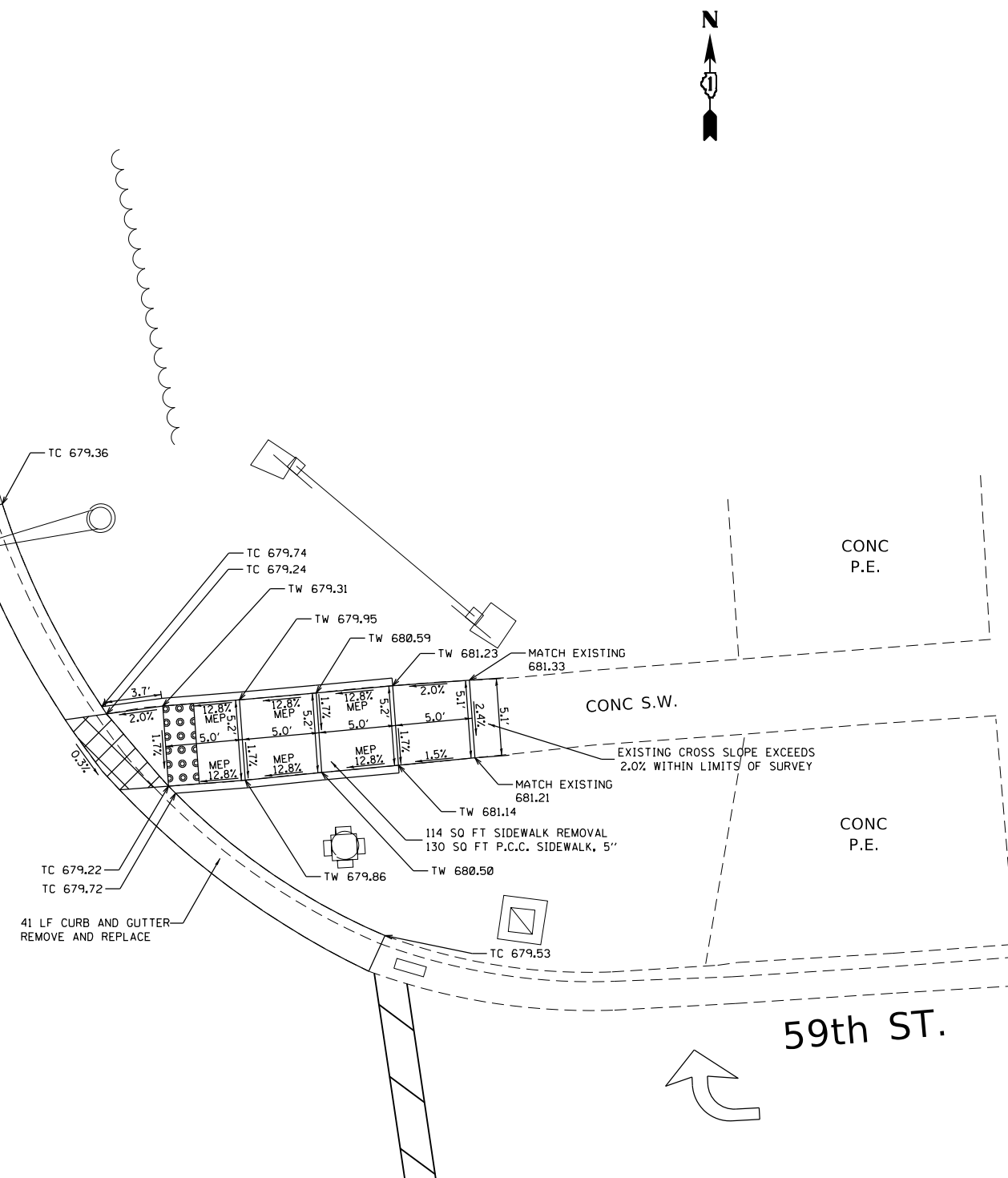
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ADA DETAILS (AT KOHLEY RD.-NE CORNER)  
 FAP 870 /IL 53 (LINCOLN AVE.)-1-88 TO 59TH ST.**

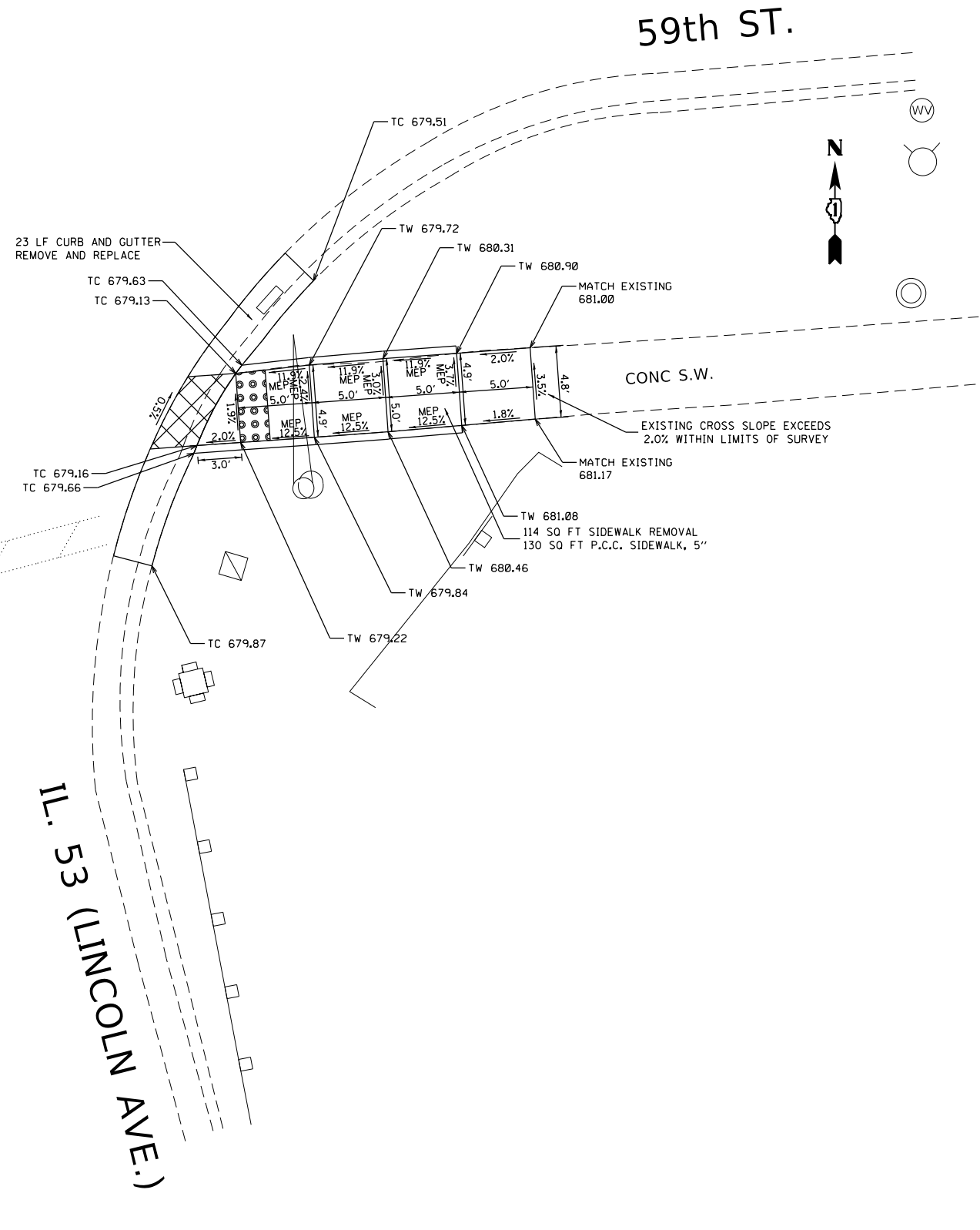
SCALE: SHEET OF SHEETS STA. TO STA.

| F.A.P. RTE.               | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------|--------|--------------|-----------|
| 870                       | 2018-029-RS-SW | DUPAGE | 51           | 31        |
| CONTRACT NO. 62G23        |                |        |              |           |
| ILLINOIS FED. AID PROJECT |                |        |              |           |

IL. 53 (LINCOLN AVE.)



IL. 53 (LINCOLN AVE.)



REFERENCE BENCH MARK ELEV 679.22  
 BENCHMARK : SET "□" ON NORTHWEST CORNER OF TRAFFIC SIGNAL BOX  
 LOCATION : SOUTHWEST CORNER OF IL 53 (LINCOLN AVE.) AND 59TH ST.

**LEGEND**

- xx.xx' EXISTING LENGTH
- PROPOSED SIDE CURB
- ( ) EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- DEPRESSED CURB

REFERENCE BENCH MARK ELEV 679.22  
 BENCHMARK : SET "□" ON NORTHWEST CORNER OF TRAFFIC SIGNAL BOX  
 LOCATION : SOUTHWEST CORNER OF IL 53 (LINCOLN AVE.) AND 59TH ST.

|  |                             |            |           |
|--|-----------------------------|------------|-----------|
| FILE NAME =  | USER NAME = steedpo         | DESIGNED - | REVISED - |
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| Default  | PLOT SCALE = 10.0000' / in. | CHECKED -  | REVISED - |
|  | PLOT DATE = 2/1/2019        | DATE -     | REVISED - |

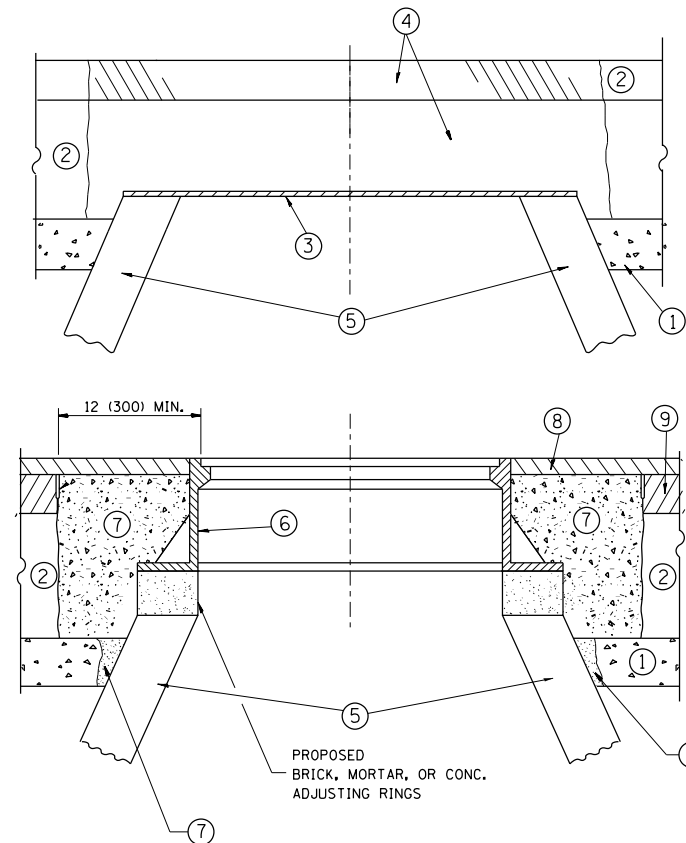
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ADA DETAILS (AT 59TH ST.-NE & SE CORNERS)  
 FAP 870 IL 53 (LINCOLN AVE.)-I-88 TO 59TH ST.**

SCALE: SHEET OF SHEETS STA. TO STA.

|                           |                |        |              |           |
|---------------------------|----------------|--------|--------------|-----------|
| F.A.P. RTE.               | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| 870                       | 2018-029-RS-SW | DUPAGE | 51           | 32        |
| CONTRACT NO. 62G23        |                |        |              |           |
| ILLINOIS FED. AID PROJECT |                |        |              |           |





**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

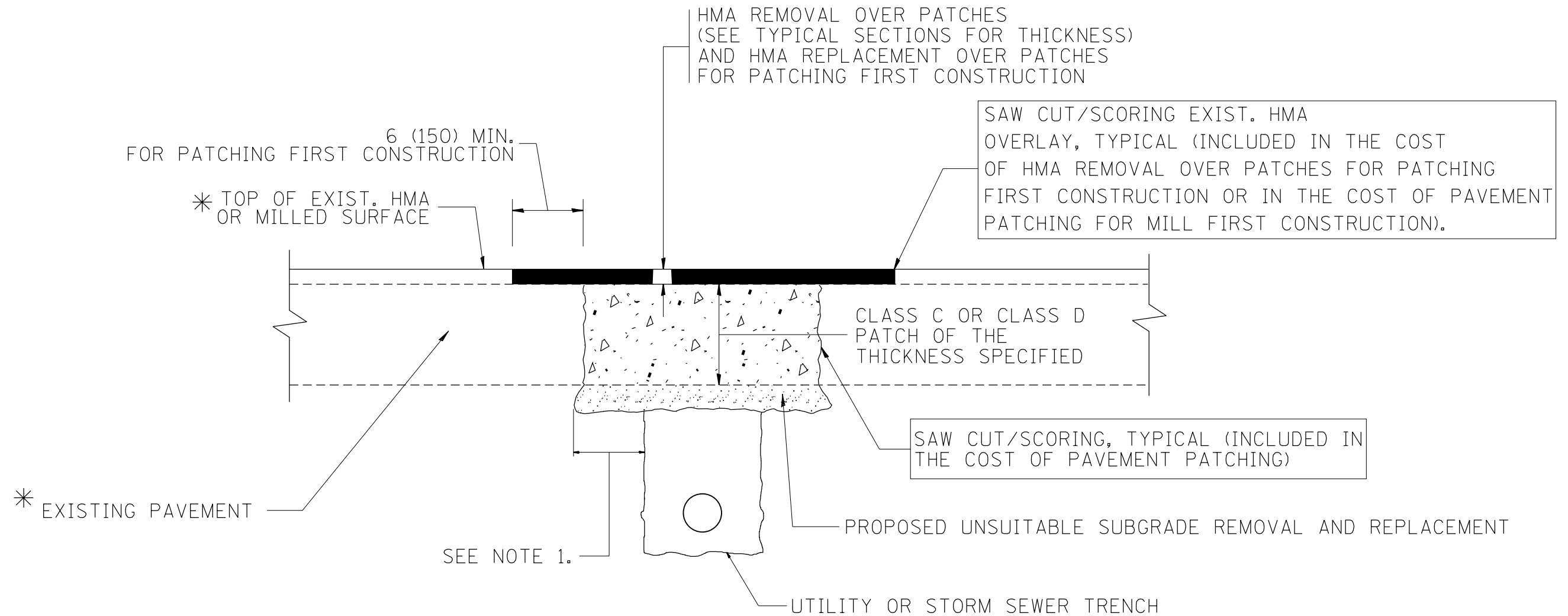
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

|   |                               |                    |                                |
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| FILE NAME =   | USER NAME = steedpo           | DESIGNED - R. SHAH | REVISED - R. WIEDEMAN 05-14-04 |
| pw\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI322\Drawings\Design\DistStd.dgn |                               | CHECKED -          | REVISED - R. BORO 01-01-07     |
|   | PLOT SCALE = 100.0000' / 1in. | DATE - 10-25-94    | REVISED - R. BORO 03-09-11     |
|   | PLOT DATE = 2/1/2019          |                    | REVISED - R. BORO 12-06-11     |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|  |                         |      |         |
|--|-------------------------|------|---------|
| <b>DETAILS FOR<br/>FRAMES AND LIDS ADJUSTMENT WITH MILLING</b> |                         |      |         |
| SCALE: NONE  | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. |

|   |                |                           |              |           |
|---|----------------|---------------------------|--------------|-----------|
| F.A.P. RTE.                                     | SECTION        | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| 870   | 2018-029-RS-SW | DJ                        | 51           | 33        |
| <b>BD600-03 (BD-8)</b>                          |                | <b>CONTRACT NO. 62G23</b> |              |           |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |                |                           |              |           |



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

|   |                               |                            |                             |   |  |                         |                         |                           |              |           |
|---|-------------------------------|----------------------------|-----------------------------|---|--|-------------------------|-------------------------|---------------------------|--------------|-----------|
| FILE NAME =   | USER NAME = steedpo           | DESIGNED - R. SHAH         | REVISED - A. ABBAS 04-27-98 | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>PAVEMENT PATCHING FOR<br/>HMA SURFACED PAVEMENT</b> | F.A.P. RTE.             | SECTION                 | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| pw\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI322\BROWNS\Design\DistStd.dgn | PLOT SCALE = 100.0000' / 1in. | CHECKED -                  | REVISED - R. BORO 01-01-07  |   |  | 870                     | 2018-029-RS-SW          | DJ PAGE                   | 51           | 34        |
| PLOT DATE = 2/1/2019  | DATE - 10-25-94               | REVISED - R. BORO 09-04-07 | REVISED - K. ENG 10-27-08   |   |  | <b>BD400-04 (BD-22)</b> |                         | <b>CONTRACT NO. 62G23</b> |              |           |
|   |                               |                            |                             |   |  | SCALE: NONE             | SHEET NO. 1 OF 1 SHEETS | STA.                      | TO STA.      |           |

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001  
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) \*\*

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

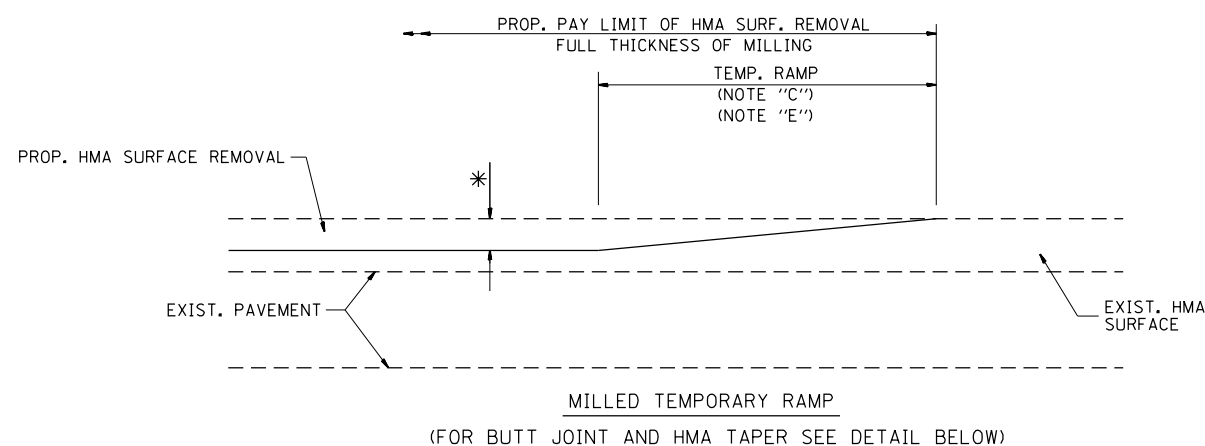
PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:  
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

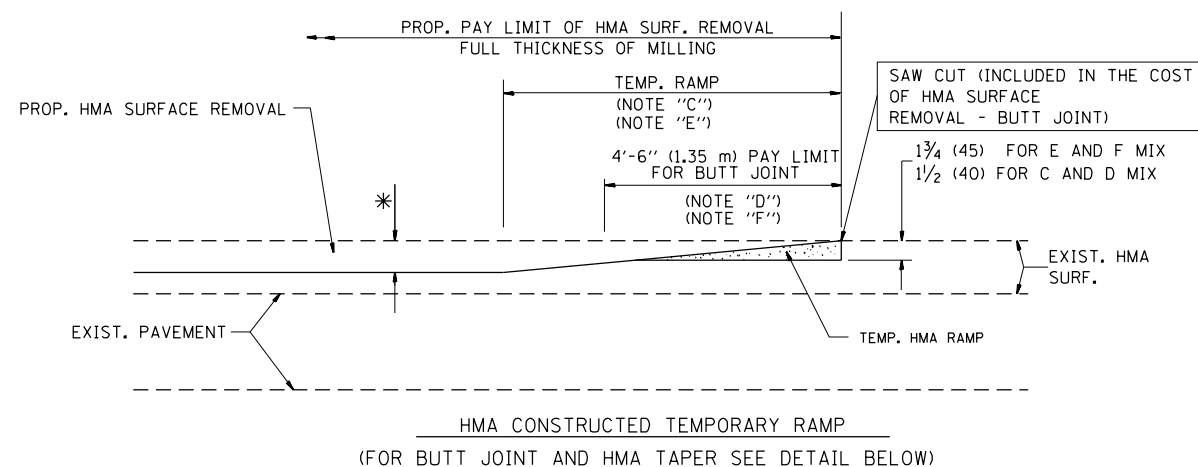
# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

|  |                     |                             |                             |   |  |                         |                         |                           |              |   |
|--|---------------------|-----------------------------|-----------------------------|---|--|-------------------------|-------------------------|---------------------------|--------------|---|
| FILE NAME =  | USER NAME = steedpo | DESIGNED - A. HOUSEH        | REVISED - R. SHAH 10-03-96  | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>CURB OR CURB AND GUTTER<br/>REMOVAL AND REPLACEMENT</b> | F.A.P. R.T.E.           | SECTION                 | COUNTY                    | TOTAL SHEETS | SHEET NO.   |
| pw\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI322\BROWN\Meta\Design\DistStd.dgn |                     | REVISED - A. ABBAS 03-21-97 | REVISED - M. GOMEZ 01-22-01 |   |  | 870                     | 2018-029-RS-SW          | DU PAGE                   | 51           | 35  |
| PLOT SCALE = 100.0000' / 1in.  | CHECKED -           | REVISED - R. BORO 12-15-09  |                             |   |  | <b>BD600-06 (BD-24)</b> |                         | <b>CONTRACT NO. 62G23</b> |              |   |
| PLOT DATE = 2/1/2019   | DATE - 03-11-94     |                             |                             |   |  | SCALE: NONE             | SHEET NO. 1 OF 1 SHEETS | STA.                      | TO STA.      | FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT |

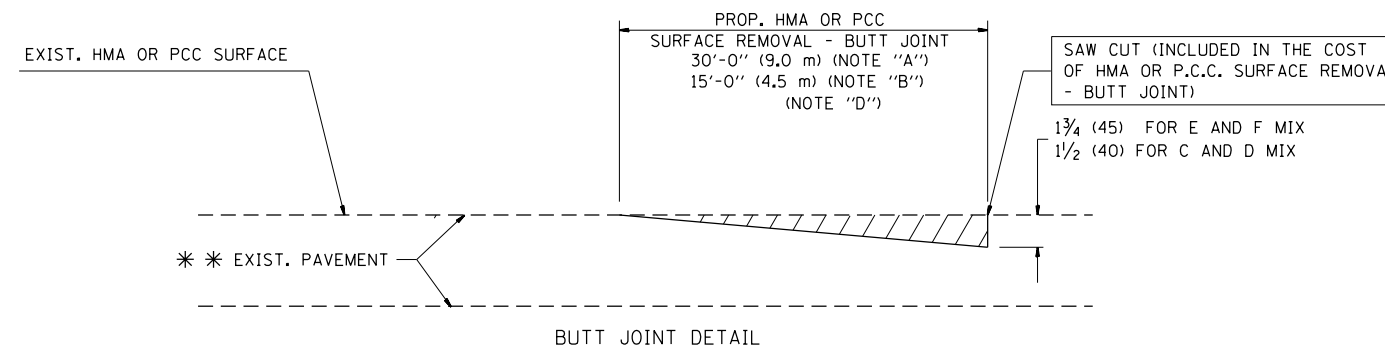


**OPTION 1**

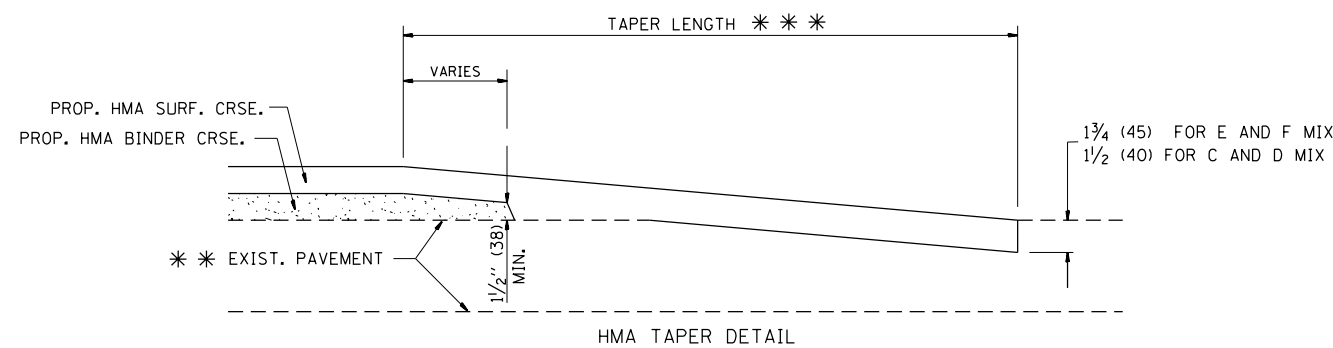


**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\* \* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

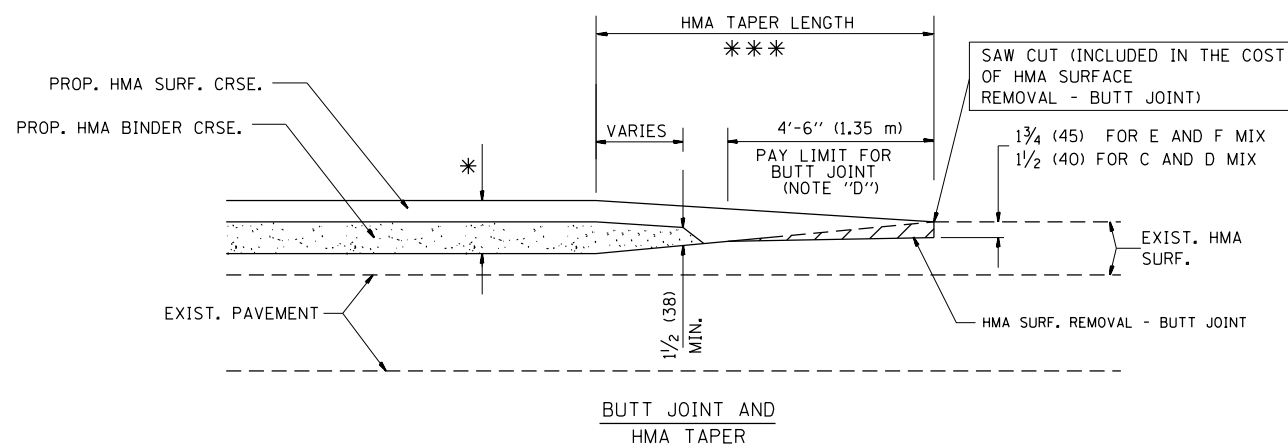
**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \* \* \* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

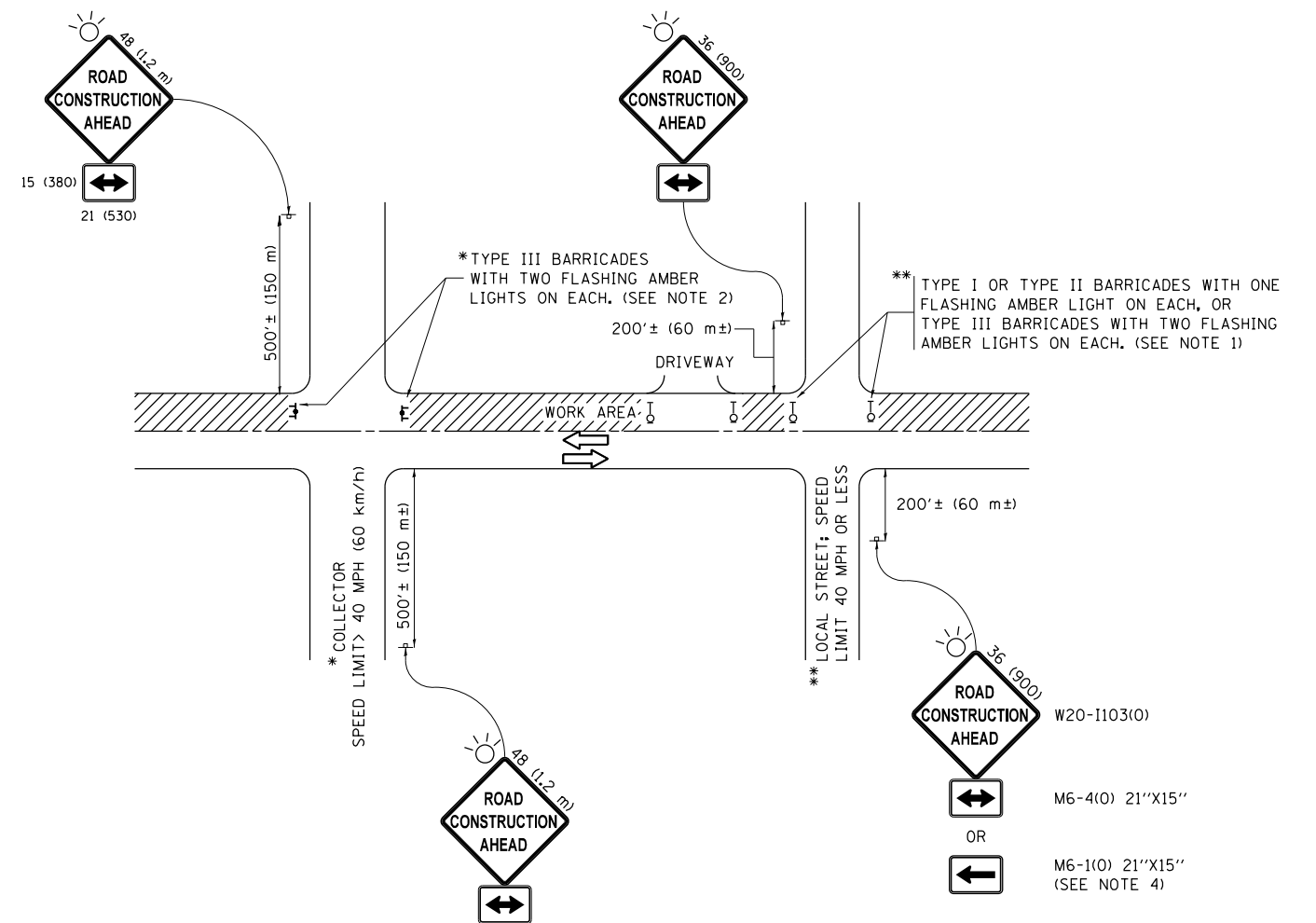
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| p:\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI322\Drawings\Design\DistStd.dgn |                               | REVISED - A. ABBAS 03-21-97 |                             |
|  | PLOT SCALE = 100.0000' / 1in. | CHECKED -                   | REVISED - M. GOMEZ 04-06-01 |
|  | PLOT DATE = 2/1/2019          | DATE - 06-13-90             | REVISED - R. BORO 01-01-07  |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND  
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

|   |                |                    |              |           |
|---|----------------|--------------------|--------------|-----------|
| F.A.P. RTE.                                     | SECTION        | COUNTY             | TOTAL SHEETS | SHEET NO. |
| 870   | 2018-029-RS-SW | DJ                 | 51           | 36        |
| BD400-05 BD32                                   |                | CONTRACT NO. 62G23 |              |           |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |                |                    |              |           |



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

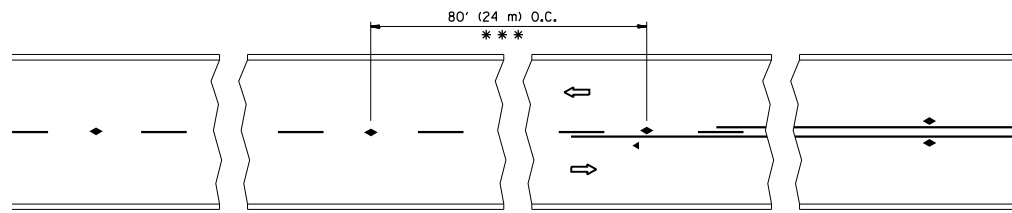
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| Default   | PLOT SCALE = 100.0000' / in. | CHECKED -         | REVISED - A. SCHUETZE 07-01-13  |
|   | PLOT DATE = 2/1/2019         | DATE - 06-89      | REVISED - A. SCHUETZE 09-15-16  |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

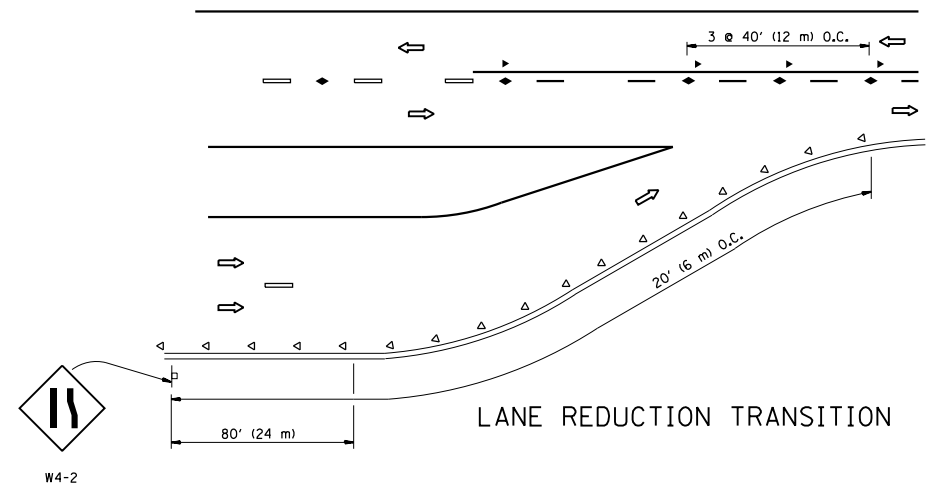
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

| F.A.P. RTE.               | SECTION        | COUNTY | TOTAL SHEETS              | SHEET NO. |
|---------------------------|----------------|--------|---------------------------|-----------|
| 870                       | 2018-029-RS-SW | DJ     | 51                        | 37        |
| <b>TC-10</b>              |                |        | <b>CONTRACT NO. 62G23</b> |           |
| ILLINOIS FED. AID PROJECT |                |        |                           |           |

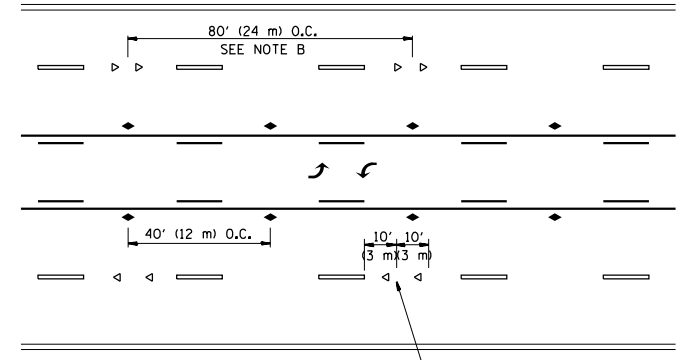


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

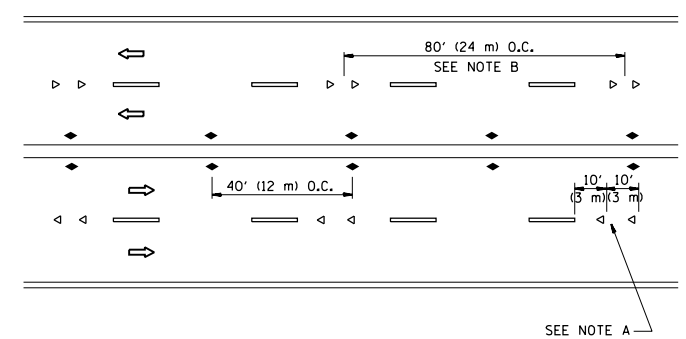
TWO-LANE/TWO-WAY



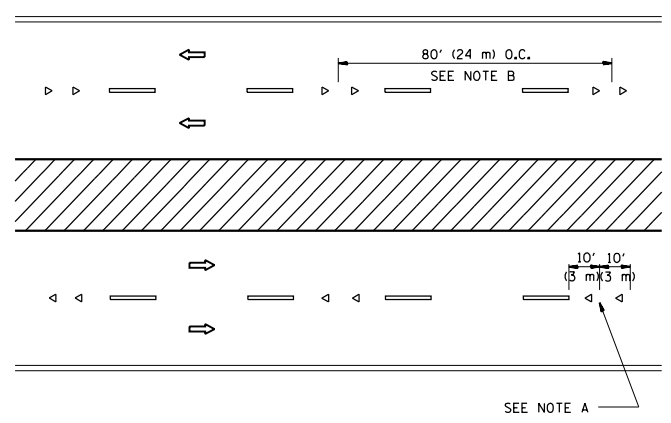
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

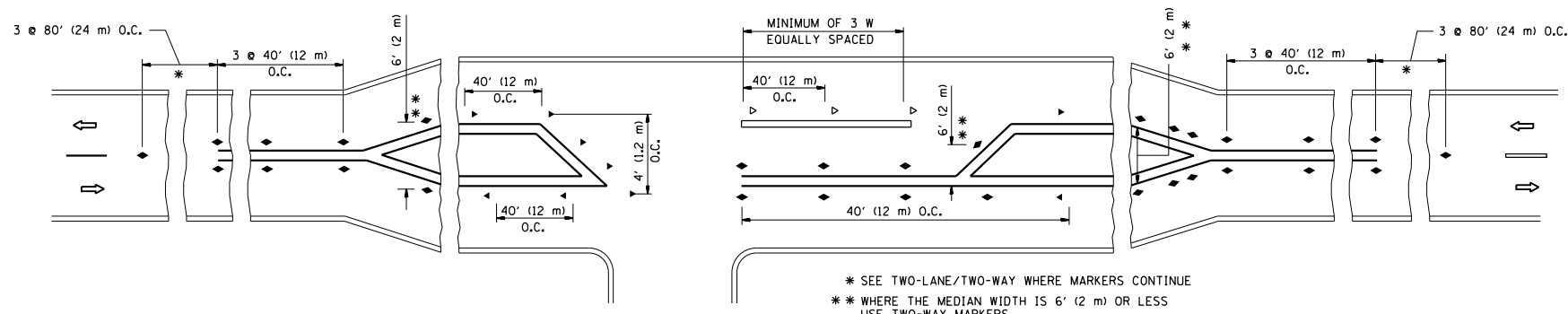
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

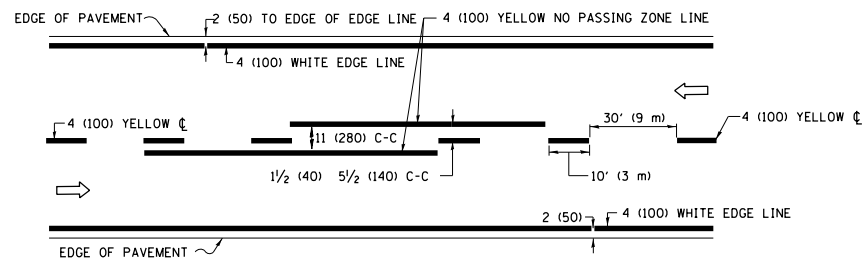


LEFT TURN

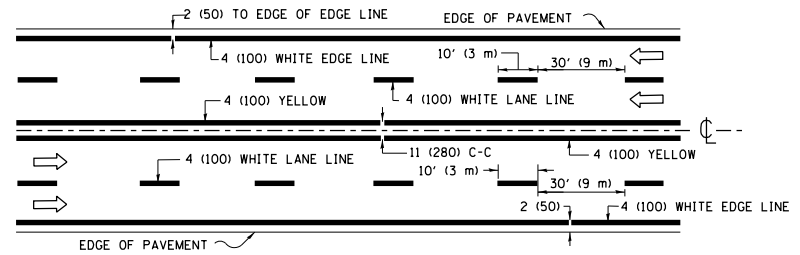
\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

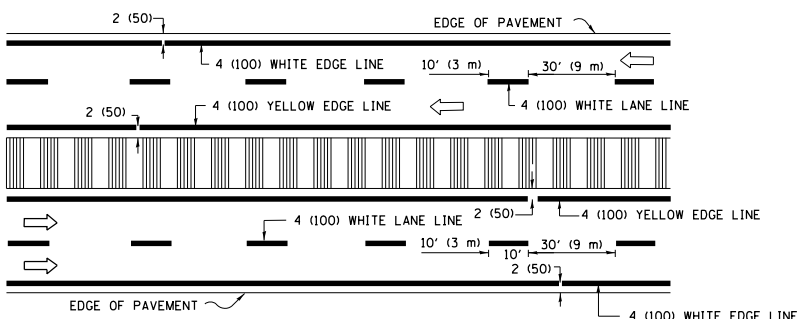
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| FILE NAME =   | USER NAME = steedp           | DESIGNED -                      | REVISED - T. RAMMACHER 09-19-94 | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>TYPICAL APPLICATIONS<br/>RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)</b> |  |  | F.A.P. RTE.               | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| pw\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI322\Drawings\Design\DistStd.dgn | PLOT SCALE = 100.0000' / in. | CHECKED -                       | REVISED - T. RAMMACHER 03-12-99 |   |  |  |  | 870                       | 2018-029-RS-SW | DJ     | PAGE 51      | 38        |
| PLOT DATE = 2/1/2019  | DATE -                       | REVISED - T. RAMMACHER 01-06-00 | REVISED - C. JUCIUS 09-09-09    |   | <b>TC-11</b>   |  |  | <b>CONTRACT NO. 62G23</b> |                |        |              |           |
|   |                              |                                 |                                 |   | SCALE: NONE  |  |  | SHEET NO. 1 OF 1 SHEETS   |                |        | STA. TO STA. |           |



**2-LANE ROADWAY**

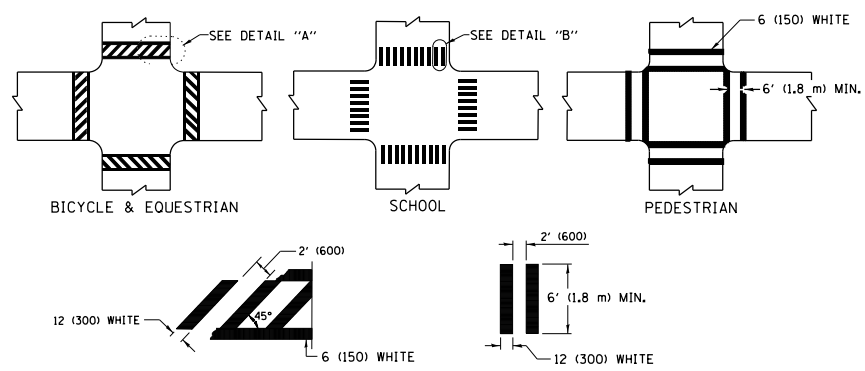


**MULTI-LANE UNDIVIDED**



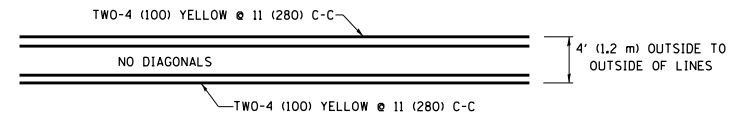
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

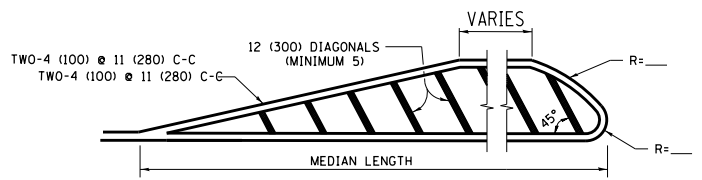


**TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

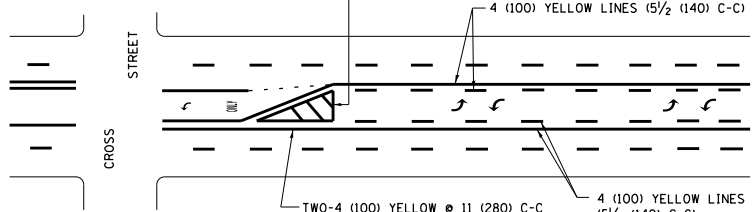


**4' (1.2 m) WIDE MEDIANS ONLY**



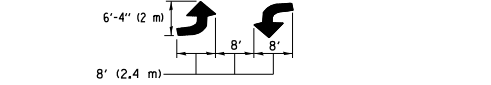
**MEDIANS OVER 4' (1.2 m) WIDE**

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



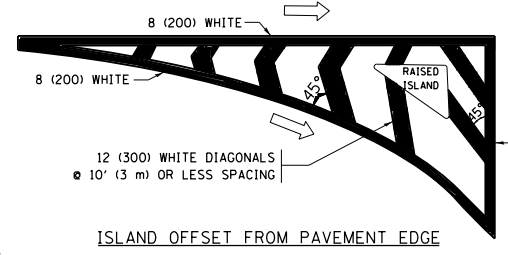
**MEDIAN WITH TWO-WAY LEFT TURN LANE  
TYPICAL PAINTED MEDIAN MARKING**

A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

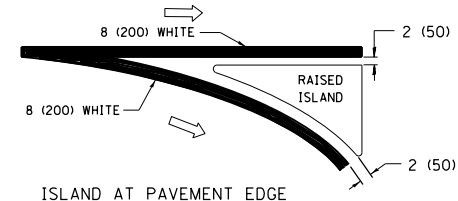


**TYPICAL LEFT (OR RIGHT) TURN LANE  
TYPICAL TURN LANE MARKING**

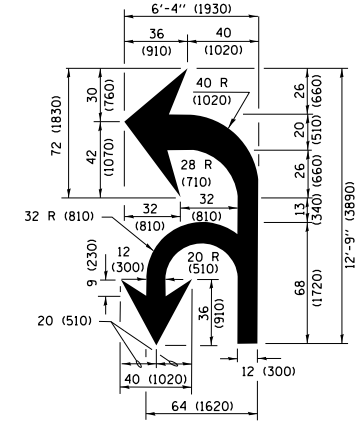
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



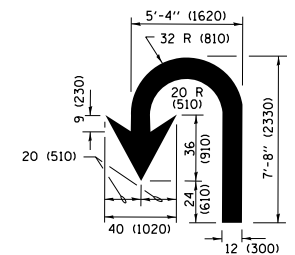
**ISLAND OFFSET FROM PAVEMENT EDGE**



**ISLAND AT PAVEMENT EDGE  
TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

**LANE REDUCTION TRANSITION**

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

| D(FT) | SPEED LIMIT |
|-------|-------------|
| 345   | 30          |
| 425   | 35          |
| 500   | 40          |
| 580   | 45          |
| 665   | 50          |
| 750   | 55          |

| TYPE OF MARKING   | WIDTH OF LINE  | PATTERN                      | COLOR   | SPACING /REMARKS   |
|---|--|------------------------------|---|--|
| CENTERLINE ON 2 LANE PAVEMENT   | 4 (100)  | SKIP-DASH                    | YELLOW  | 10' (3 m) LINE WITH 30' (9 m) SPACE  |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT   | 2 @ 4 (100)  | SOLID                        | YELLOW  | 11 (280) C-C   |
| NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS                                       | 4 (100)<br>2 @ 4 (100)   | SOLID<br>SOLID               | YELLOW<br>YELLOW  | 5/2 (140) C-C FROM SKIP-DASH CENTERLINE<br>11 (280) C-C<br>OMIT SKIP-DASH CENTERLINE BETWEEN   |
| LANE LINES  | 4 (100)<br>5 (125) ON FREEWAYS   | SKIP-DASH<br>SKIP-DASH       | WHITE<br>WHITE  | 10' (3 m) LINE WITH 30' (9 m) SPACE  |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)                                   | SAME AS LINE BEING EXTENDED  | SKIP-DASH                    | SAME AS LINE BEING EXTENDED                             | 2' (600) LINE WITH 6' (1.8 m) SPACE  |
| EDGE LINES  | 4 (100)  | SOLID                        | YELLOW-LEFT<br>WHITE-RIGHT                              | OUTLINE MEDIANS IN YELLOW  |
| TURN LANE MARKINGS  | 6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))                                      | SOLID                        | WHITE   | SEE TYPICAL TURN LANE MARKING DETAIL   |
| TWO WAY LEFT TURN MARKING   | 2 @ 4 (100) EACH DIRECTION<br>8' (2.4m) LEFT ARROW   | SKIP-DASH AND SOLID IN PAIRS | YELLOW<br>WHITE   | 10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE<br>SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL             |
| CROSSWALK LINES (PEDESTRIAN)<br>A. DIAGONALS (BIKE & EQUESTRIAN)<br>B. LONGITUDINAL BARS (SCHOOL) | 2 @ 6 (150)<br>12 (300) @ 45°<br>12 (300) @ 90°  | SOLID<br>SOLID<br>SOLID      | WHITE<br>WHITE<br>WHITE                                 | NOT LESS THAN 6' (1.8 m) APART<br>2' (600) APART<br>2' (600) APART<br>SEE TYPICAL CROSSWALK MARKING DETAILS.   |
| STOP LINES  | 24 (600)   | SOLID                        | WHITE   | PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS   | 2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°<br>NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS | SOLID                        | YELLOW;<br>TWO WAY TRAFFIC<br>WHITE;<br>ONE WAY TRAFFIC | 11 (280) C-C FOR THE DOUBLE LINE<br>SEE TYPICAL PAINTED MEDIAN MARKING.  |
| GORE MARKING AND CHANNELIZING LINES   | 8 (200) WITH 12 (300) DIAGONALS @ 45°  | SOLID                        | WHITE   | DIAGONALS:<br>15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h))<br>20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)<br>30' (9 m) C-C (OVER 45MPH (70 km/h))              |
| RAILROAD CROSSING   | 24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"               | SOLID                        | WHITE   | SEE STATE STANDARD 780001<br>AREA OF:<br>"R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH<br>"X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )                                     |
| SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')  | 12 (300) @ 45°   | SOLID                        | WHITE - RIGHT<br>YELLOW - LEFT                          | 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))<br>75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h))<br>150' (45 m) C-C (OVER 45MPH (70 km/h))                       |
| U TURN ARROW  | SEE DETAIL   | SOLID                        | WHITE   | 16.3 SF  |
| 2 ARROW COMBINATION LEFT AND U TURN   | SEE DETAIL   | SOLID                        | WHITE   | 30.4 SF  |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

|  |                              |                  |                              |
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| FILE NAME =  | USER NAME = steedp           | DESIGNED - EVERS | REVISED - C. JUCIUS 09-09-09 |
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| Default  | PLOT SCALE = 100.0000' / in. | DATE - 03-19-90  | REVISED - C. JUCIUS 12-21-15 |
|  | PLOT DATE = 2/1/2019         |                  | REVISED - C. JUCIUS 04-12-16 |

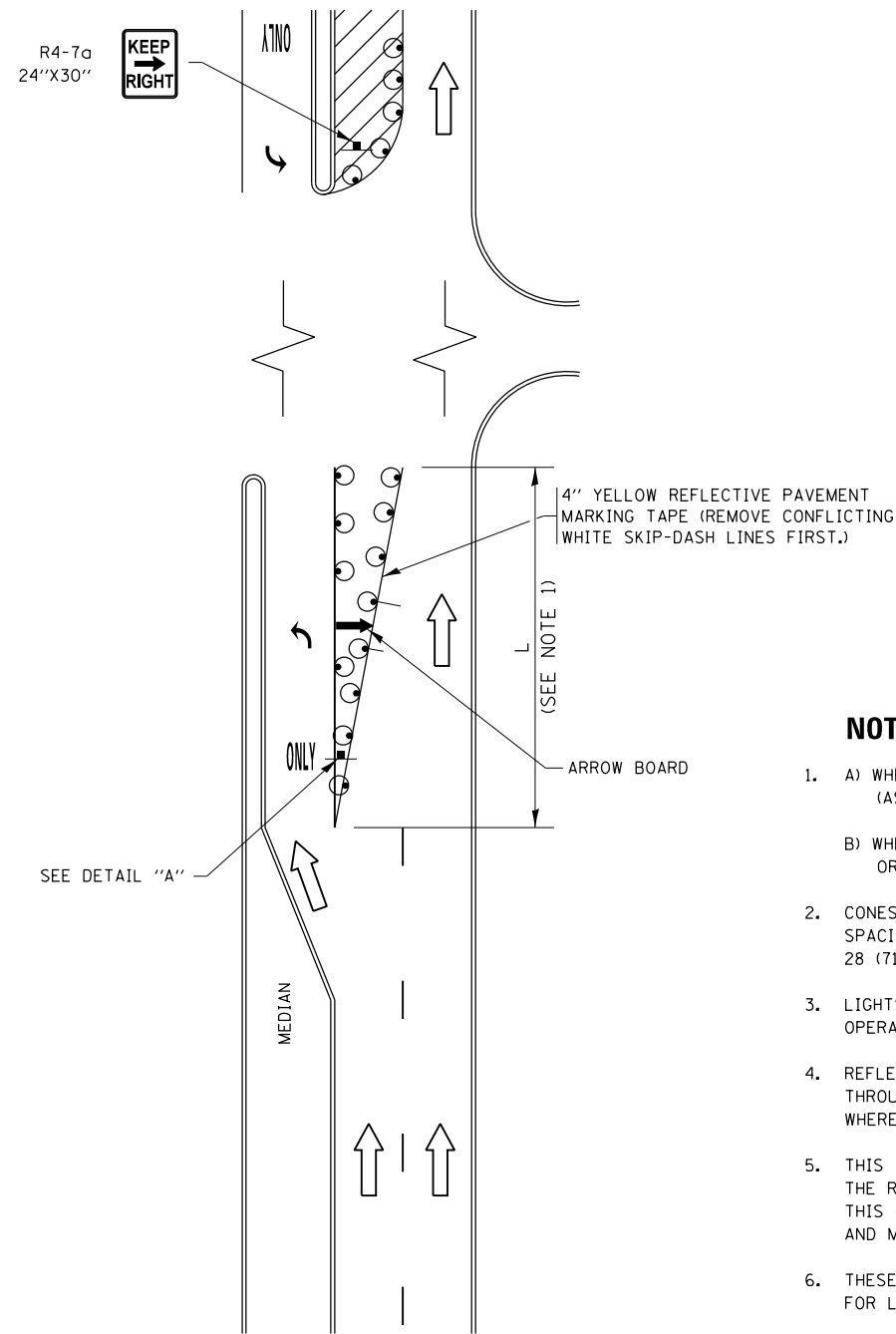
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

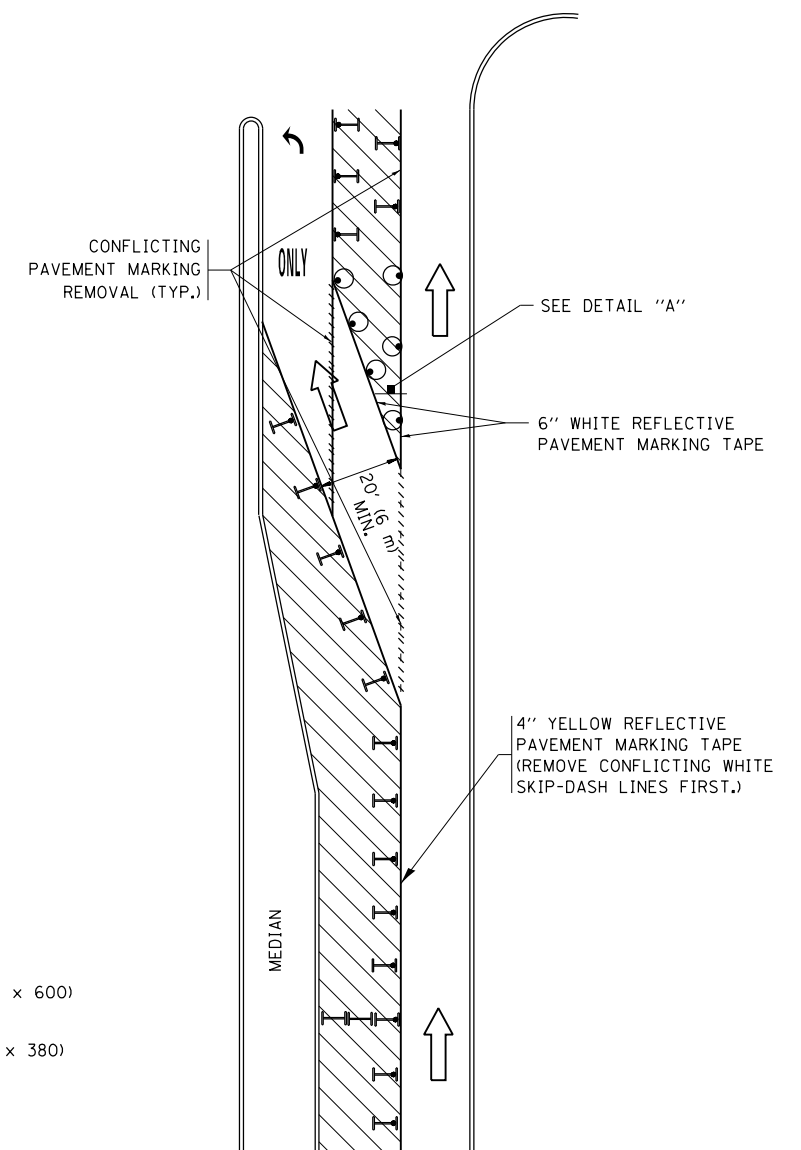
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| F.A.P. RTE.               | SECTION        | COUNTY              | TOTAL SHEETS | SHEET NO. |
| 870                       | 2018-029-RS-SW | DU PAGE             | 51           | 39        |
| <b>TC-13</b>              |                | <b>CONTRACT NO.</b> | <b>62G23</b> |           |
| ILLINOIS FED. AID PROJECT |                |                     |              |           |

# TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

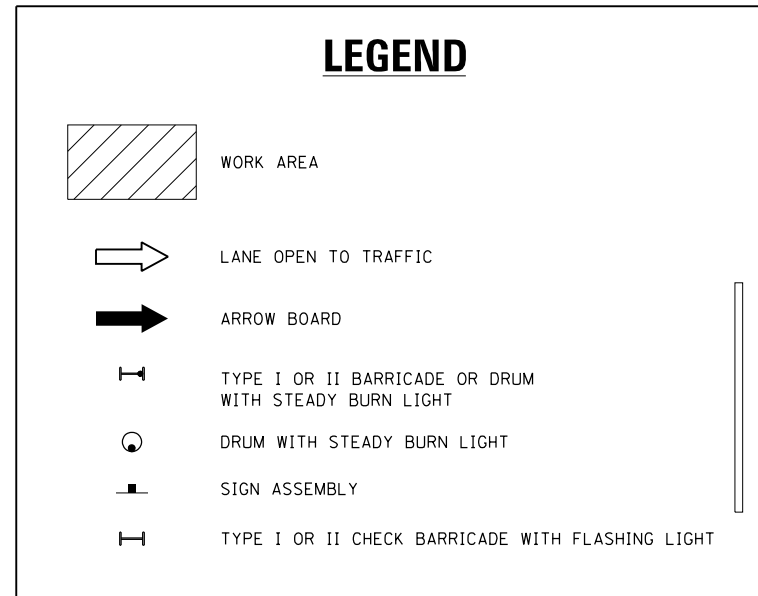


**FIGURE 1**

# TURN BAY ENTRANCE WITHIN A LANE CLOSURE

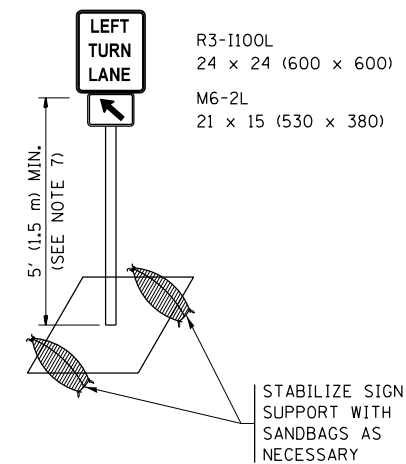


**FIGURE 2**



### NOTES:

1. A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

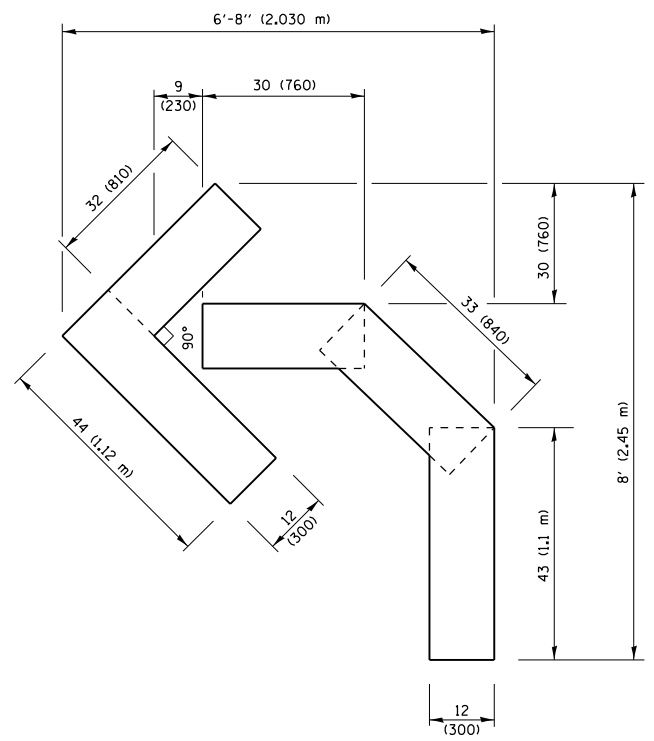


**DETAIL A**

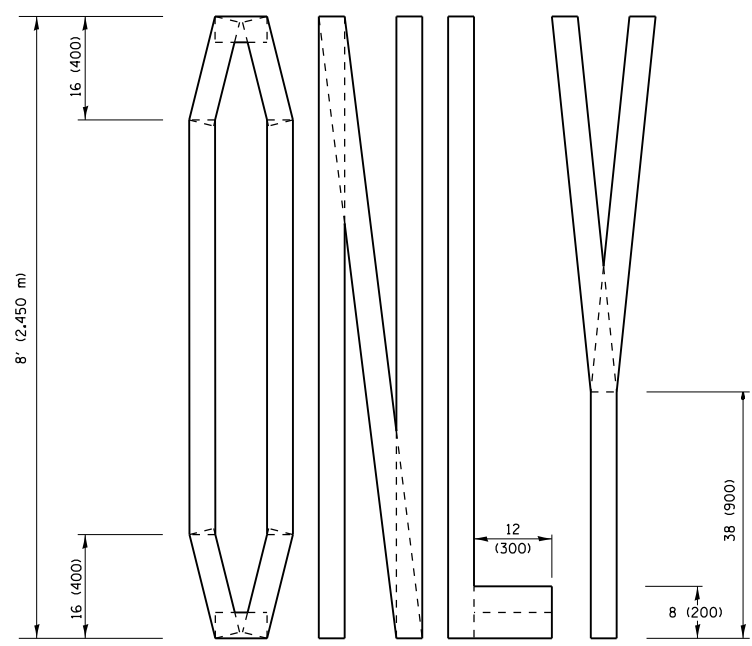
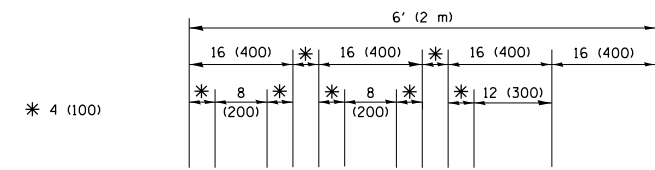
All dimensions are in inches (millimeters) unless otherwise shown.

|  |                              |                                 |                                |   |  |                           |                |                           |              |           |
|--|------------------------------|---------------------------------|--------------------------------|---|--|---------------------------|----------------|---------------------------|--------------|-----------|
| FILE NAME =  | USER NAME = steedp           | REVISED - T. RAMMACHER 09-08-94 | REVISED - R. BORO 09-14-09     | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>TRAFFIC CONTROL AND PROTECTION AT TURN BAYS<br/>(TO REMAIN OPEN TO TRAFFIC)</b> | F.A.P. R.E.               | SECTION        | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| pw:\IL\084EBIDINTEG\illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\DI322\REVISED Design\HOUSEH 11-07-95 | REVISED - A. HOUSEH 11-07-95 | REVISED - A. SCHUETZE 07-01-13  | REVISED - A. SCHUETZE 07-01-13 |   |  | 870                       | 2018-029-RS-SW | DU PAGE                   | 51           | 40        |
| Default  | PLOT SCALE = 100.0000' / in. | REVISED - A. HOUSEH 10-12-96    | REVISED - A. SCHUETZE 09-15-16 |   |  | <b>TC-14</b>              |                | <b>CONTRACT NO. 62G23</b> |              |           |
|  | PLOT DATE = 2/1/2019         | REVISED - T. RAMMACHER 01-06-00 | REVISED -                      |   |  | ILLINOIS FED. AID PROJECT |                |                           |              |           |
|  |                              |                                 |                                |   | SCALE: NONE  | SHEET 1                   | OF 1 SHEETS    | STA.                      | TO STA.      |           |

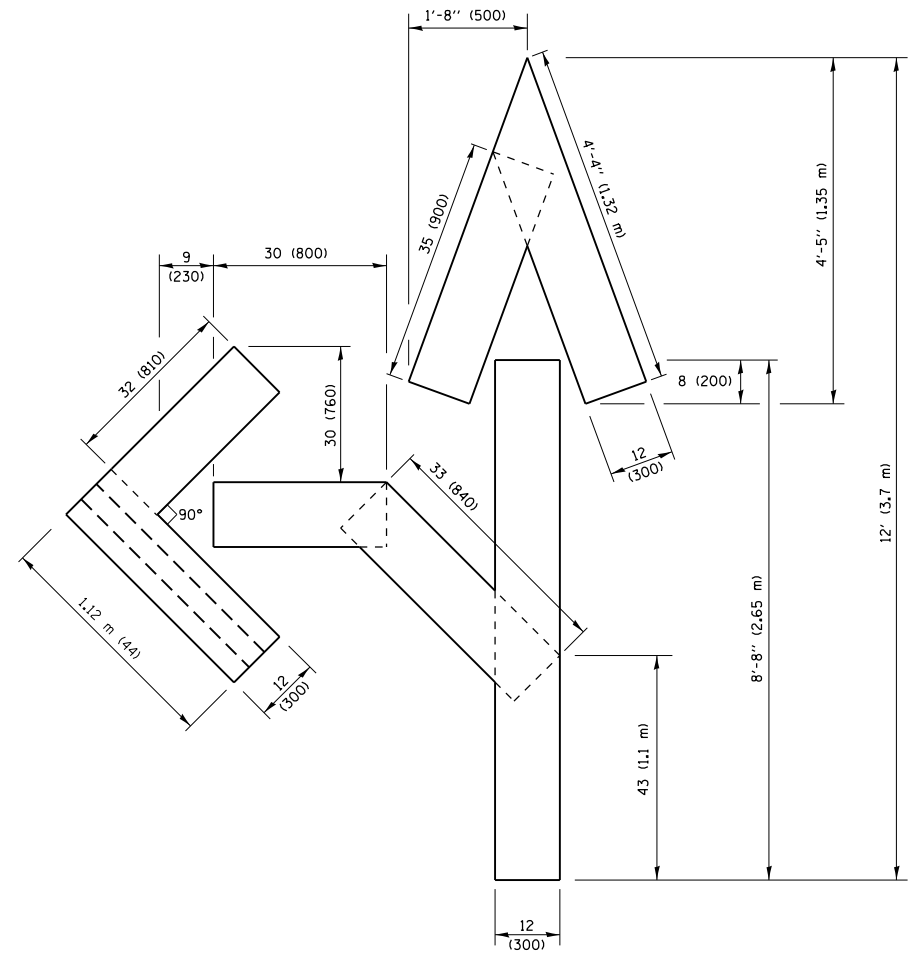




**QUANTITY**  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.41 sq. m)

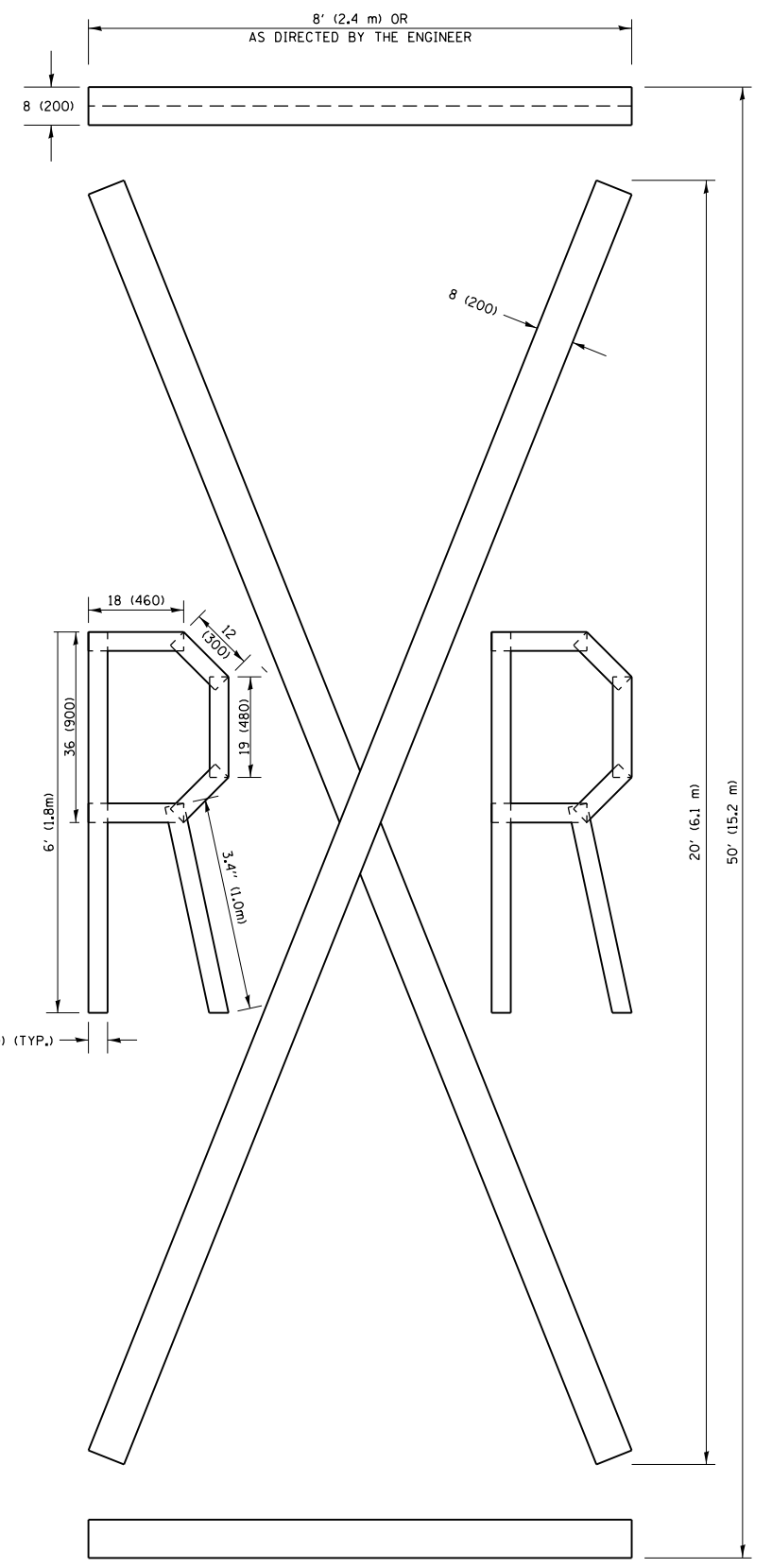


**QUANTITY**  
 4 (100) LINE = 64.1 ft. (19.5 m)  
 21.4 sq. ft. (1.99 sq. m)



**QUANTITY**  
 4 (100) LINE = 82.5 ft. (25.1 m)  
 27.5 sq. ft. (2.53 sq. m)

**NOTE:**  
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**  
 4 (100) LINE = 225.9 ft. (68.9 m)  
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

|   |                    |                 |                                 |
|---|--------------------|-----------------|---------------------------------|
| FILE NAME =   | USER NAME = steepp | DESIGNED -      | REVISED - T. RAMMACHER 03-02-98 |
| pw\11084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\01322\Drawings\Design\DistStd.dgn |                    | CHECKED -       | REVISED - E. GOMEZ 08-28-00     |
|   |                    | DATE - 09-18-94 | REVISED - E. GOMEZ 08-28-00     |
|   |                    |                 | REVISED - A. SCHUETZE 09-15-16  |

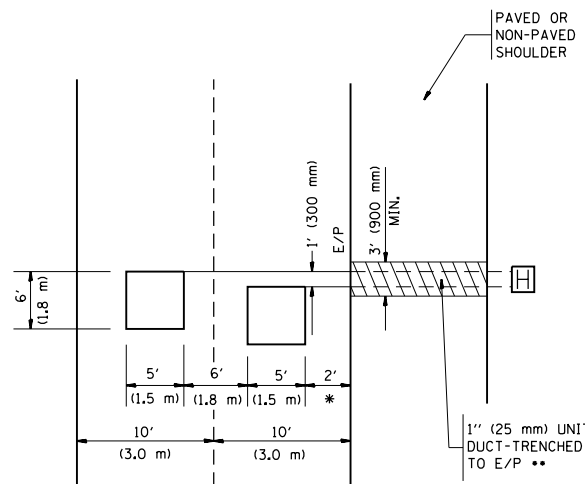
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

|  |                         |      |         |
|--|-------------------------|------|---------|
| <b>SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS</b> |                         |      |         |
| SCALE: NONE  | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. |

|   |                |                           |              |           |
|---|----------------|---------------------------|--------------|-----------|
| F.A.P. RTE.                                     | SECTION        | COUNTY                    | TOTAL SHEETS | SHEET NO. |
| 870   | 2018-029-R5-SW | DU PAGE                   | 51           | 41        |
| <b>TC-16</b>                                    |                | <b>CONTRACT NO. 62G23</b> |              |           |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |                |                           |              |           |

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



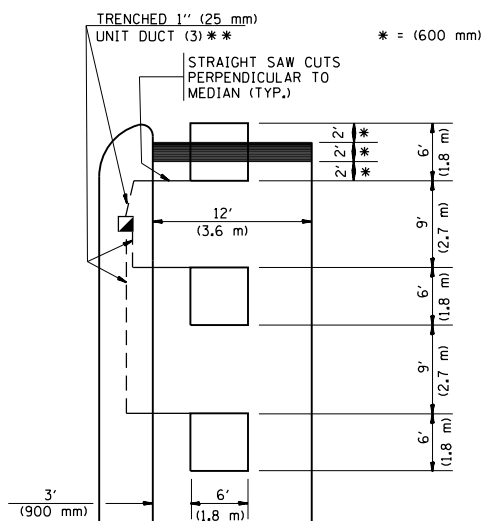
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

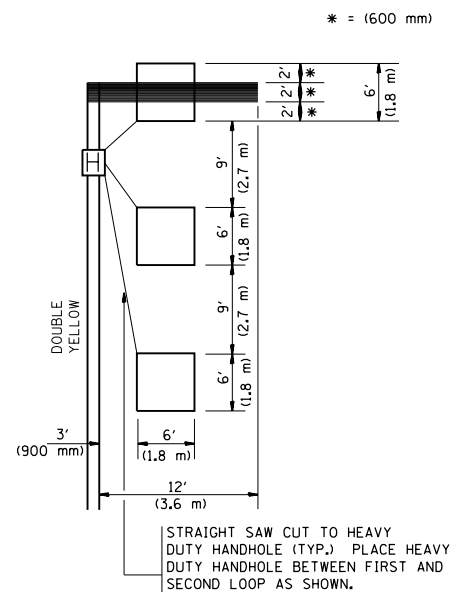


\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

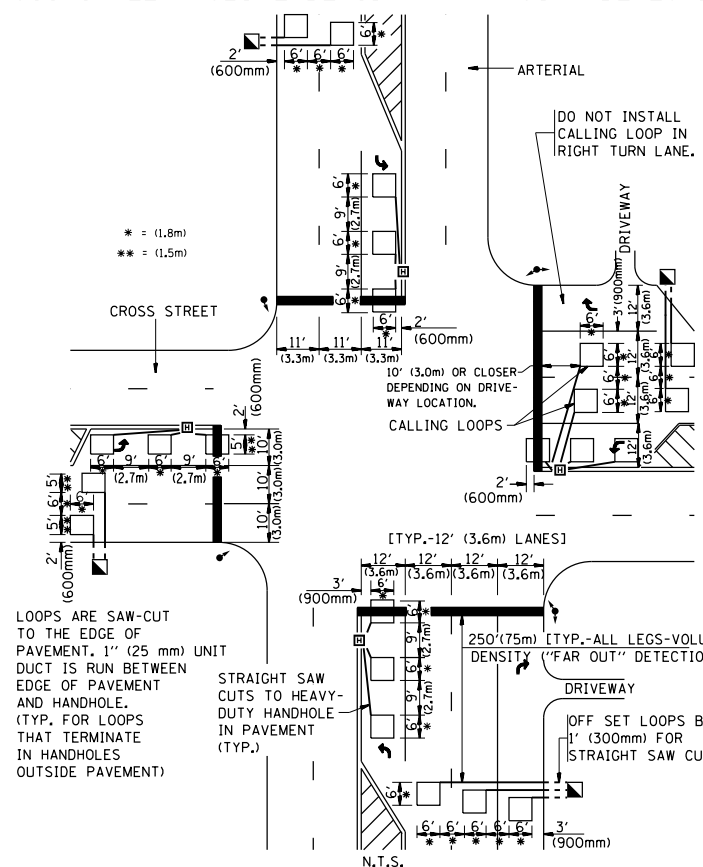
LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)



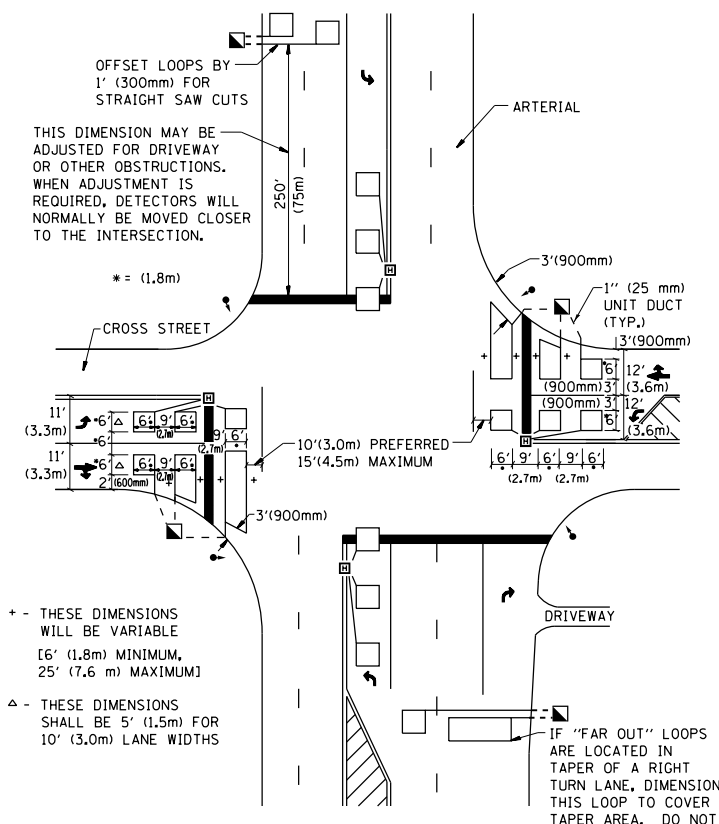
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1  
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2  
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

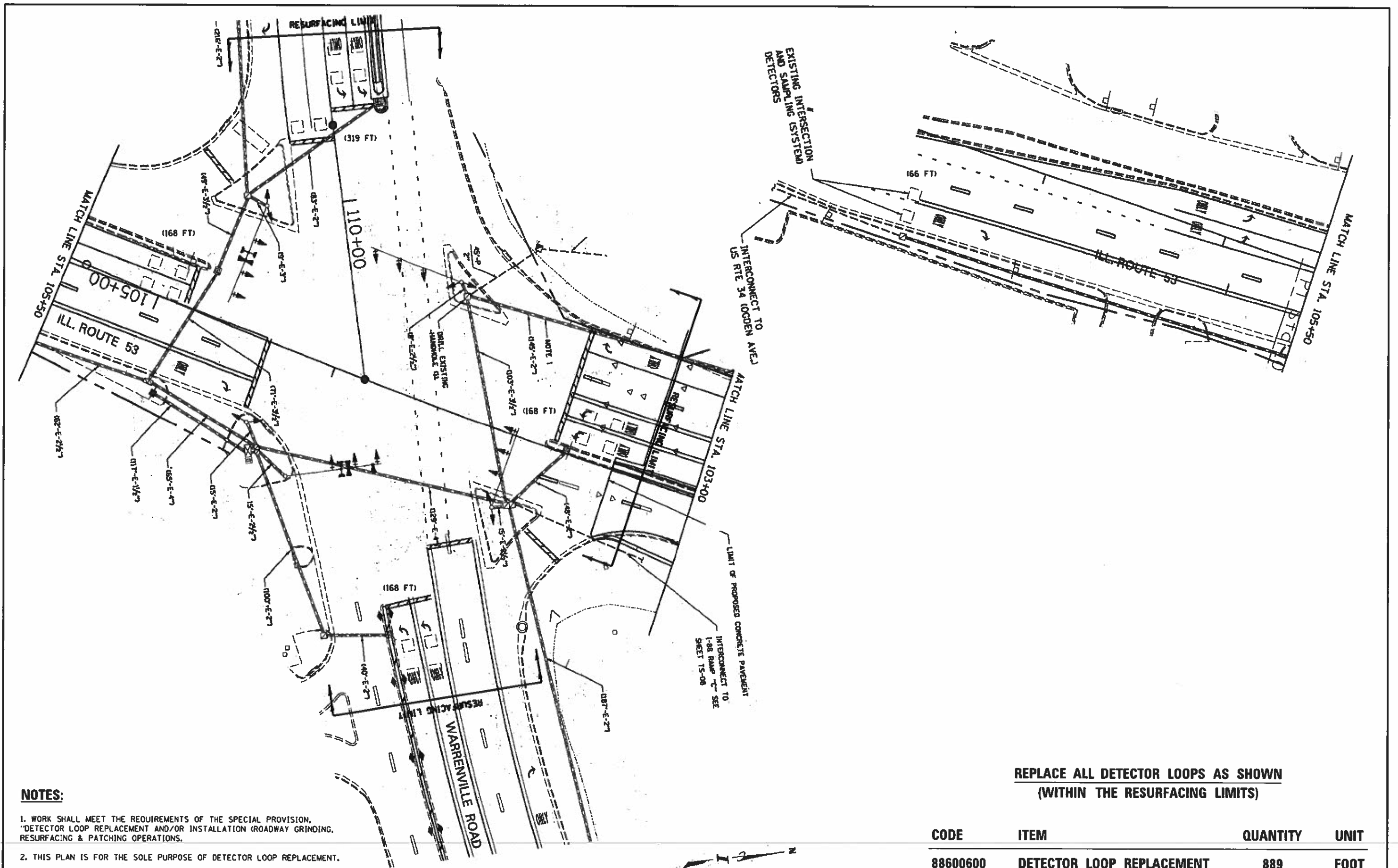
LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:  
ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

|  |                     |                  |           |   |  |                         |      |               |   |                |                           |           |    |
|--|---------------------|------------------|-----------|---|--|-------------------------|------|---------------|---|----------------|---------------------------|-----------|----|
| FILE NAME =  | USER NAME = steedpo | DESIGNED -       | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>DISTRICT 1 - DETECTOR LOOP INSTALLATION<br/>DETAILS FOR ROADWAY RESURFACING</b> |                         |      | F.A.P. R.T.E. | SECTION   | COUNTY         | TOTAL SHEETS              | SHEET NO. |    |
| p:\11\084EBID\INTEG\11inois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI322\Drawings\Design\DistStd.dgn |                     | CHECKED - R.K.F. | REVISED - |   | SCALE: NONE  | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.       | 870   | 2018-029-RS-SW | DJ PAGE                   | 51        | 42 |
| PLOT SCALE = 100.0000' / 1in.  |                     | DATE -           | REVISED - |   |  |                         |      |               | <b>TS-07</b>                                    |                | <b>CONTRACT NO. 62G23</b> |           |    |
| PLOT DATE = 2/1/2019   |                     |                  |           |   |  |                         |      |               | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |                |                           |           |    |



**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, 'DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

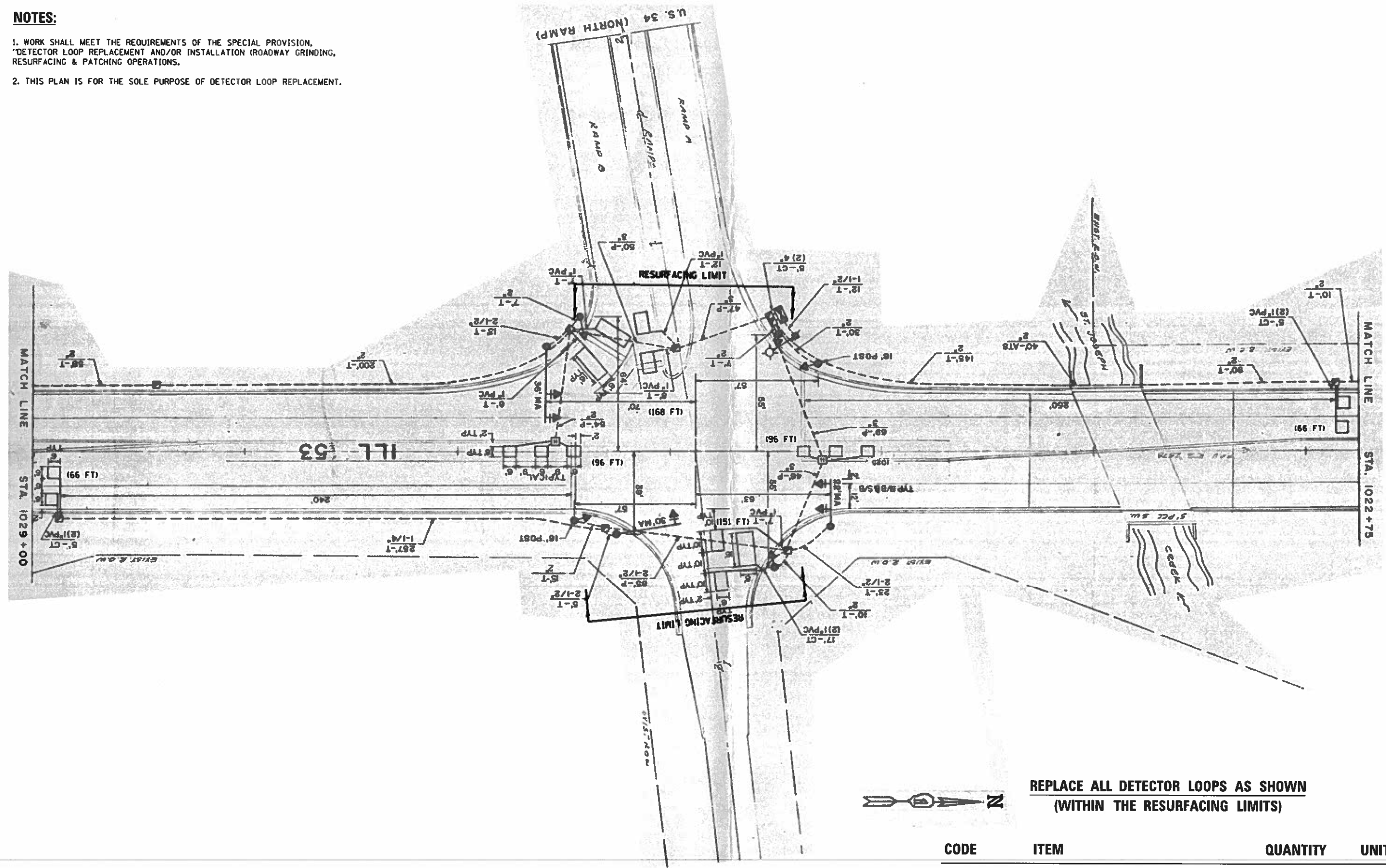
**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

| CODE     | ITEM                      | QUANTITY | UNIT |
|----------|---------------------------|----------|------|
| 88600600 | DETECTOR LOOP REPLACEMENT | 889      | FOOT |

|   |                   |              |                           |   |   |                  |                         |                |                  |               |
|---|-------------------|--------------|---------------------------|---|---|------------------|-------------------------|----------------|------------------|---------------|
| FILE NAME: S:\WP\Design\Shl.B. of Design - Detector | USER NAME: nguyen | DESIGNED: SN | REVISED:                  | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>DETECTOR LOOP REPLACEMENT PLAN<br/>ILL.53 (LINCOLN AVE.) AT WARRENVILLE RD</b> | F.A.P. RTE.: 870 | SECTION: 2018-029-R5-SW | COUNTY: DUPAGE | TOTAL SHEETS: 51 | SHEET NO.: 43 |
| PILOT SCALE: 40,0000 1/2 in.                        | CHECKED: SN       | REVISED:     | CONTRACT NO.: 62G23       |   |   |                  |                         |                |                  |               |
| PILOT DATE: 11/8/2018                               | DATE: 11/07/2018  | REVISED:     | ILLINOIS FED. AID PROJECT |   |   |                  |                         |                |                  |               |

**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



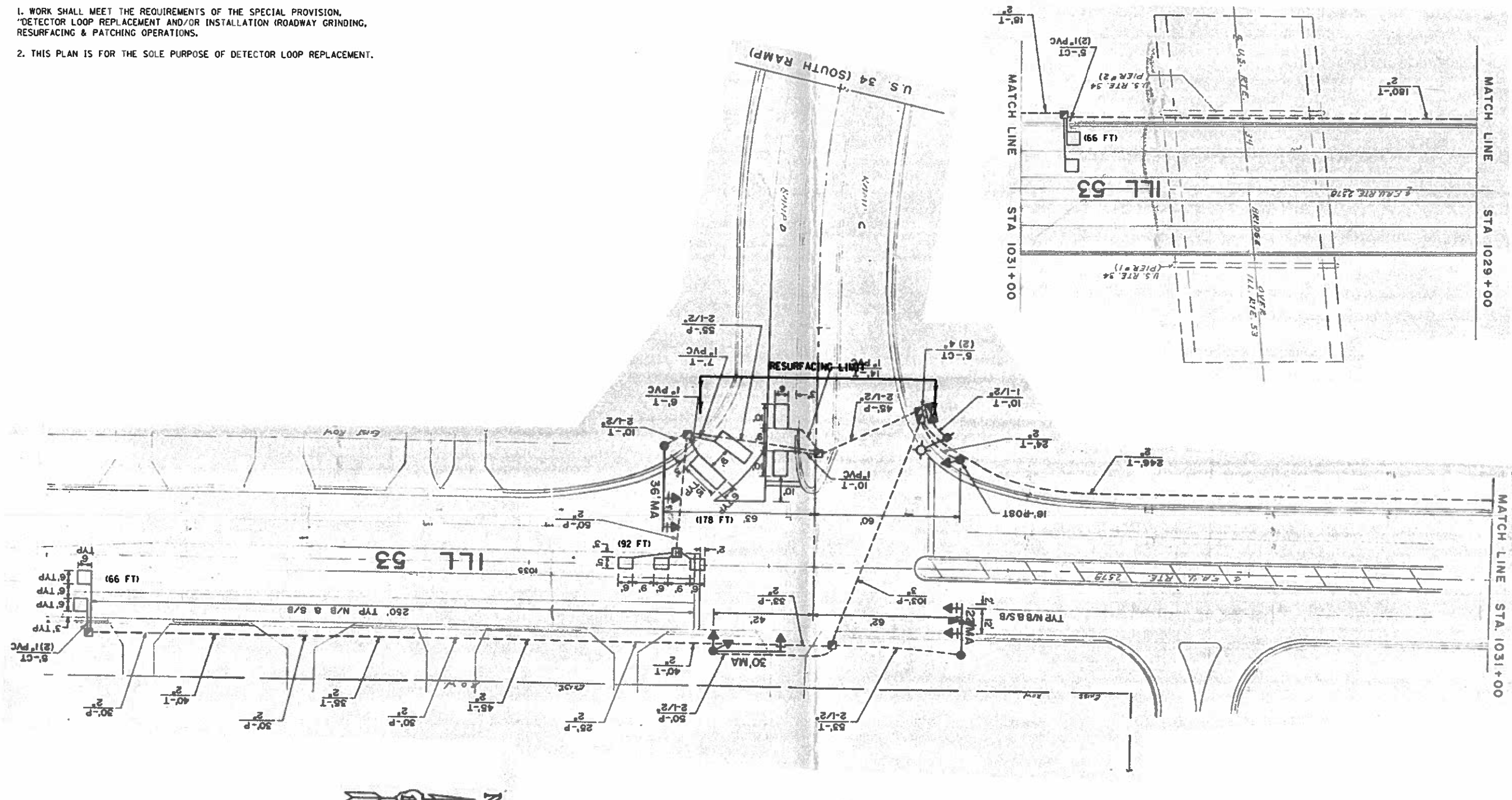
**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

| CODE     | ITEM                      | QUANTITY | UNIT |
|----------|---------------------------|----------|------|
| 88600600 | DETECTOR LOOP REPLACEMENT | 643      | FOOT |

|  |   |                  |   |   |   |                             |                |         |               |            |  |
|--|---|------------------|---|---|---|-----------------------------|----------------|---------|---------------|------------|--|
| FILE NAME: S:\WP\Design\SH\B. of Design - Detector | USER NAME: nguyenan                     | DESIGNED: SN     | REVISED:                                    | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>DETECTOR LOOP REPLACEMENT PLAN<br/>IL53 (LINCOLN AVE.) NORTH RAMP AT US34 (OGDEN AVE.)</b> | F.A.P. RTE.:                | SECTION:       | COUNTY: | TOTAL SHEETS: | SHEET NO.: |  |
| Default  | Loops Replacement - Resurfacing project | DRAWN: ADH       | REVISOR: US 34 Ogden Ave @ IL 53 North Ramp |   |   | 870                         | 2018-029-RS-5W | DUPAGE  | 51            | 44         |  |
|  | PLOT SCALE: 40,000 1"=40'               | CHECKED: SN      | REVISOR:                                    |   |   | CONTRACT NO. 62G23          |                |         |               |            |  |
|  | PLOT DATE: 11/8/2018                    | DATE: 11/07/2018 | REVISOR:                                    |   |   | (ILLINOIS) FED. AID PROJECT |                |         |               |            |  |

**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



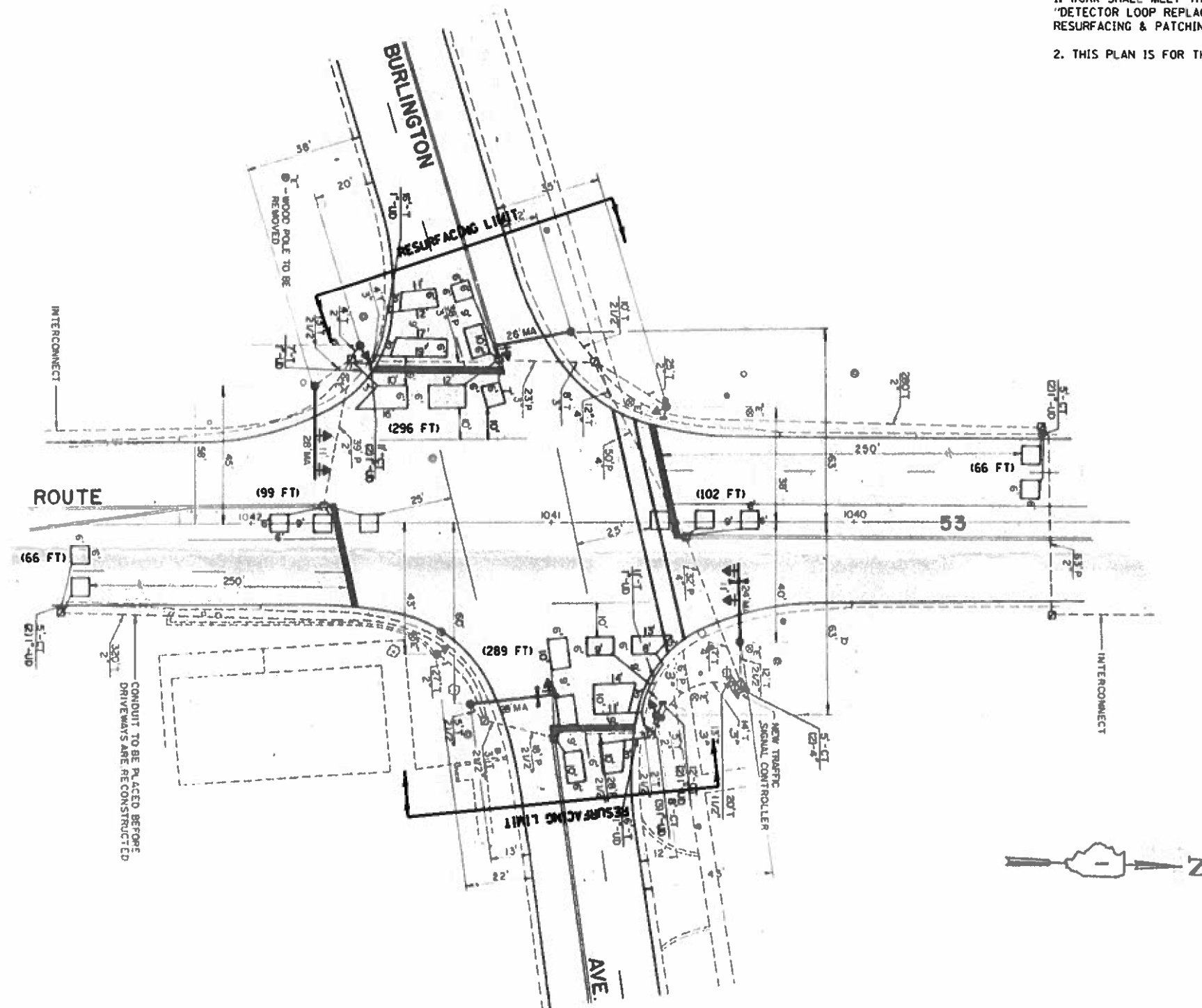
**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

| CODE     | ITEM                      | QUANTITY | UNIT |
|----------|---------------------------|----------|------|
| 88600600 | DETECTOR LOOP REPLACEMENT | 402      | FOOT |

|   |                                       |              |          |   |  |                           |                         |                |                  |               |
|---|---------------------------------------|--------------|----------|---|--|---------------------------|-------------------------|----------------|------------------|---------------|
| FILE NAME: S:\MP\Design\SN\B.of Design - Detector | USER NAME: nguyenam                   | DESIGNED: SN | REVISED: | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>DETECTOR LOOP REPLACEMENT PLAN<br/>ILL 53 (LINCOLN AVE.) SOUTH RAMP AT US 34 (OGDEN AVE.)</b> | F.A.P. RTE.: 870          | SECTION: 2018-029-RS-SW | COUNTY: DUPAGE | TOTAL SHEETS: 51 | SHEET NO.: 45 |
| PLT SCALE: 40,0000 1/2 in.                        | PROGRAM: ADR1.DSN\gn\2018\62061\CAD00 | CHECKED: SN  | REVISED: |   |  | CONTRACT NO.: 62G23       |                         |                |                  |               |
| PLDT DATE: 11/8/2018                              | DATE: 11/07/2018                      | REVISED:     | REVISED: |   |  | ILLINOIS FED. AID PROJECT |                         |                |                  |               |
| SCALE:  | SHEET OF SHEETS:                      | STA. TO STA. |          |   |  |                           |                         |                |                  |               |

**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

| CODE     | ITEM                      | QUANTITY | UNIT |
|----------|---------------------------|----------|------|
| 88600600 | DETECTOR LOOP REPLACEMENT | 918      | FOOT |

|   |   |                   |  |
|---|---|-------------------|--|
| FILE NAME -                               | USER NAME - nguyensm                    | DESIGNED - SN     | REVISED -                              |
| S:\WP\Design\5711.8.dwg Design - Detector | Loops Replacement - Resurfacing project | DRAWN - ADAN      | REVISIONS - IL 53 @ Burlington Ave.dgn |
| Default                                   | PLOT SCALE = 40.0000 1/2" = 1'          | CHECKED - SN      | REVISED -                              |
|   | PLOT DATE = 11/8/2018                   | DATE - 11/07/2018 | REVISED -                              |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

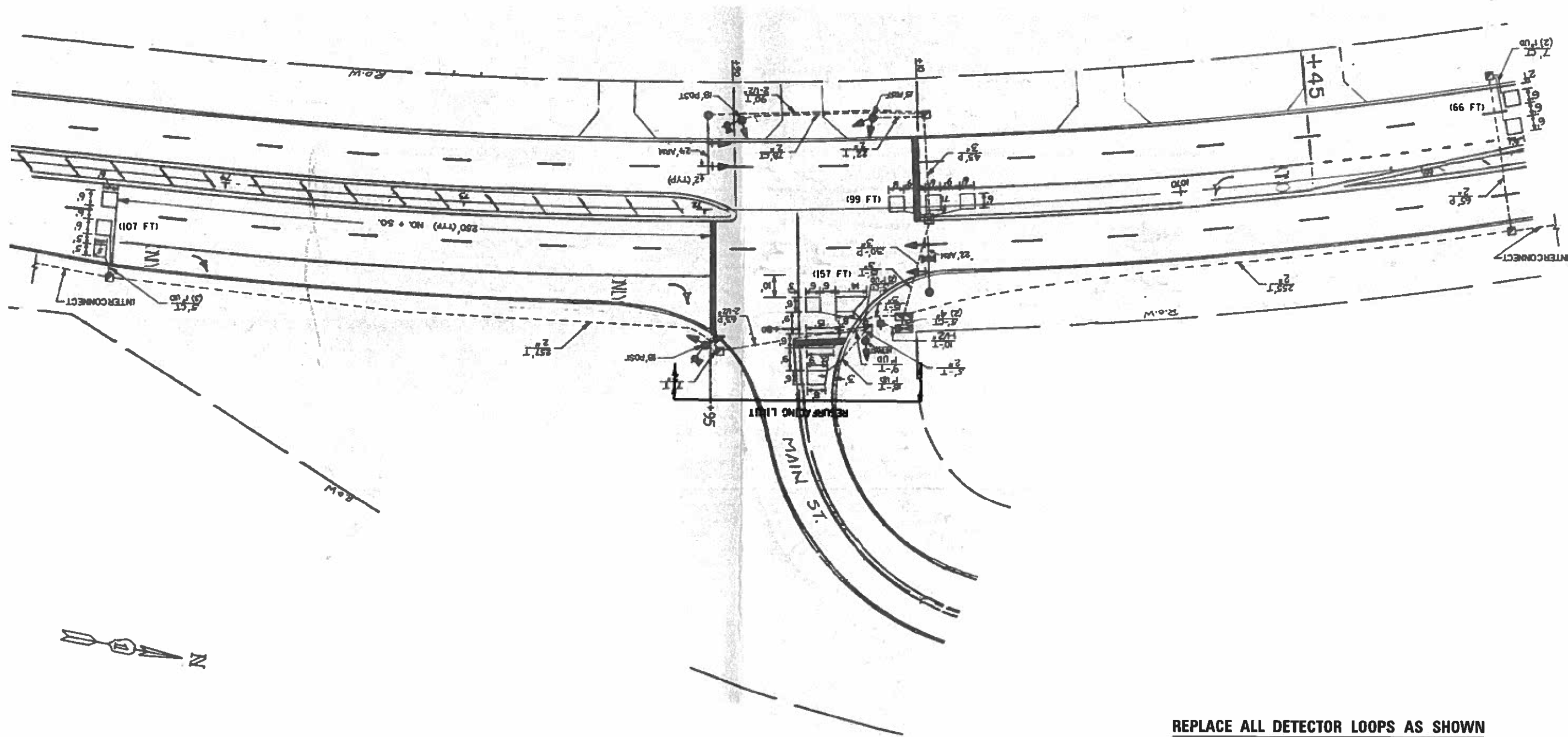
**DETECTOR LOOP REPLACEMENT PLAN  
IL53 (LINCOLN AVE.) AT BURLINGTON AVE.**

|                           |                |        |              |                    |
|---------------------------|----------------|--------|--------------|--------------------|
| F.A.P. RTE.               | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO.          |
| 870                       | 2018-029-RS-SW | DUPAGE | 51           | 46                 |
| SCALE:                    |                |        |              | CONTRACT NO. 62G23 |
| SHEET                     | OF             | SHEETS | STA.         | TO STA.            |
| ILLINOIS FED. AID PROJECT |                |        |              |                    |



**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

| CODE     | ITEM                      | QUANTITY | UNIT |
|----------|---------------------------|----------|------|
| 88600600 | DETECTOR LOOP REPLACEMENT | 429      | FOOT |

|   |   |                   |                                 |
|---|---|-------------------|---------------------------------|
| FILE NAME *                             | USER NAME * ngyensm                         | DESIGNED * SN     | REVISED *                       |
| S:\WP\Design\SN\3. of Design - Detector | Loops Replacement - Resurfacing project - S | DRAWN * ADA.DSN   | REVISIONS - IL 53 @ Main St.dgn |
| Default                                 | PLOT SCALE = 40,0000 1/2 in.                | CHECKED * SN      | REVISED *                       |
|   | PLOT DATE = 11/17/2018                      | DATE = 11/07/2018 | REVISED *                       |

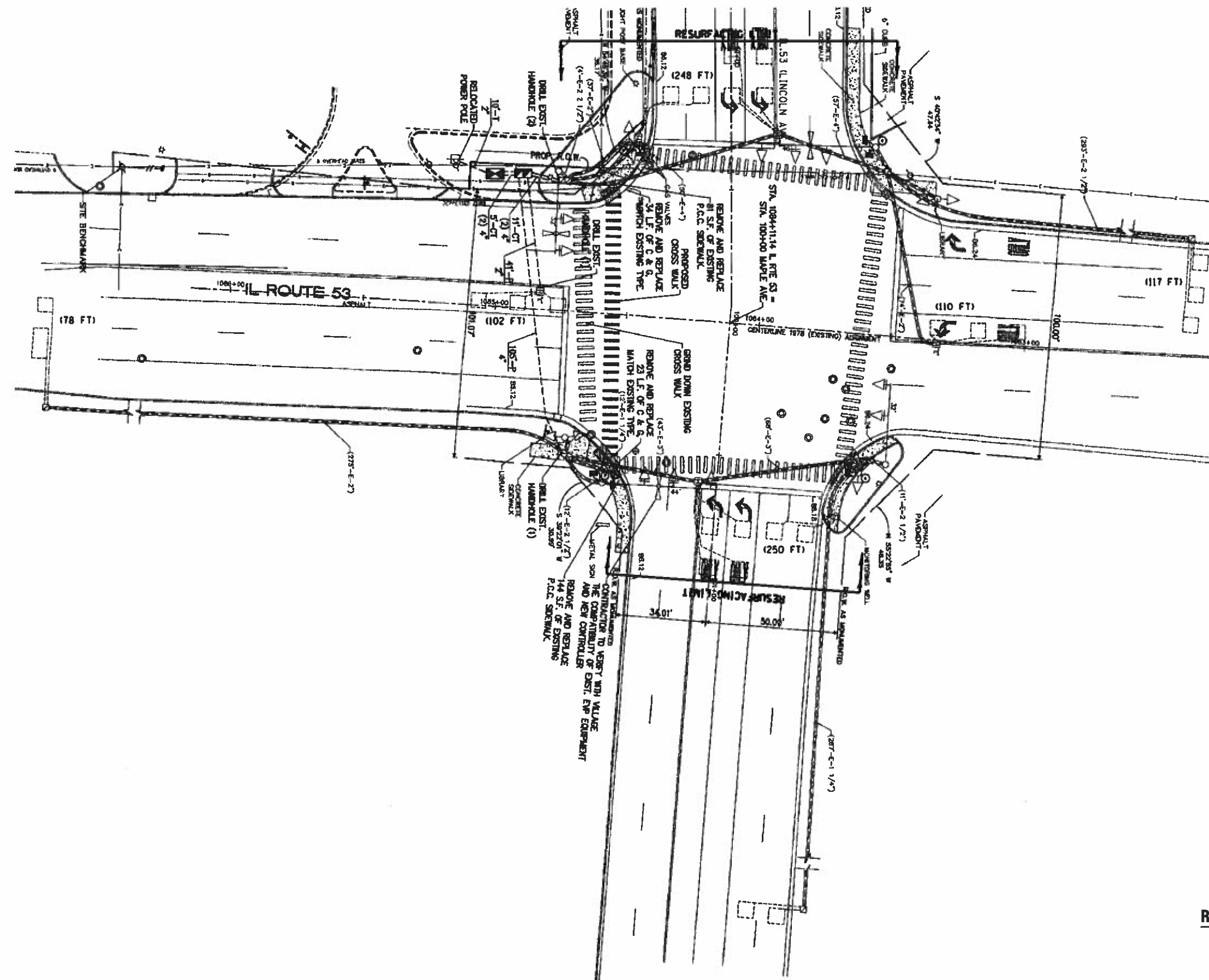
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN  
IL.53 (LINCOLN AVE.) AT MAIN STREET**

|                           |                |        |              |           |
|---------------------------|----------------|--------|--------------|-----------|
| F.A.P. RTE.               | SECTION        | COUNTY | TOTAL SHEETS | SHEET NO. |
| 870                       | 2018-029-R5-SW | DUPAGE | 51           | 48        |
| CONTRACT NO.              |                |        |              | 62G23     |
| ILLINOIS FED. AID PROJECT |                |        |              |           |

SCALE: SHEET OF SHEETS STA. TO STA.





**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

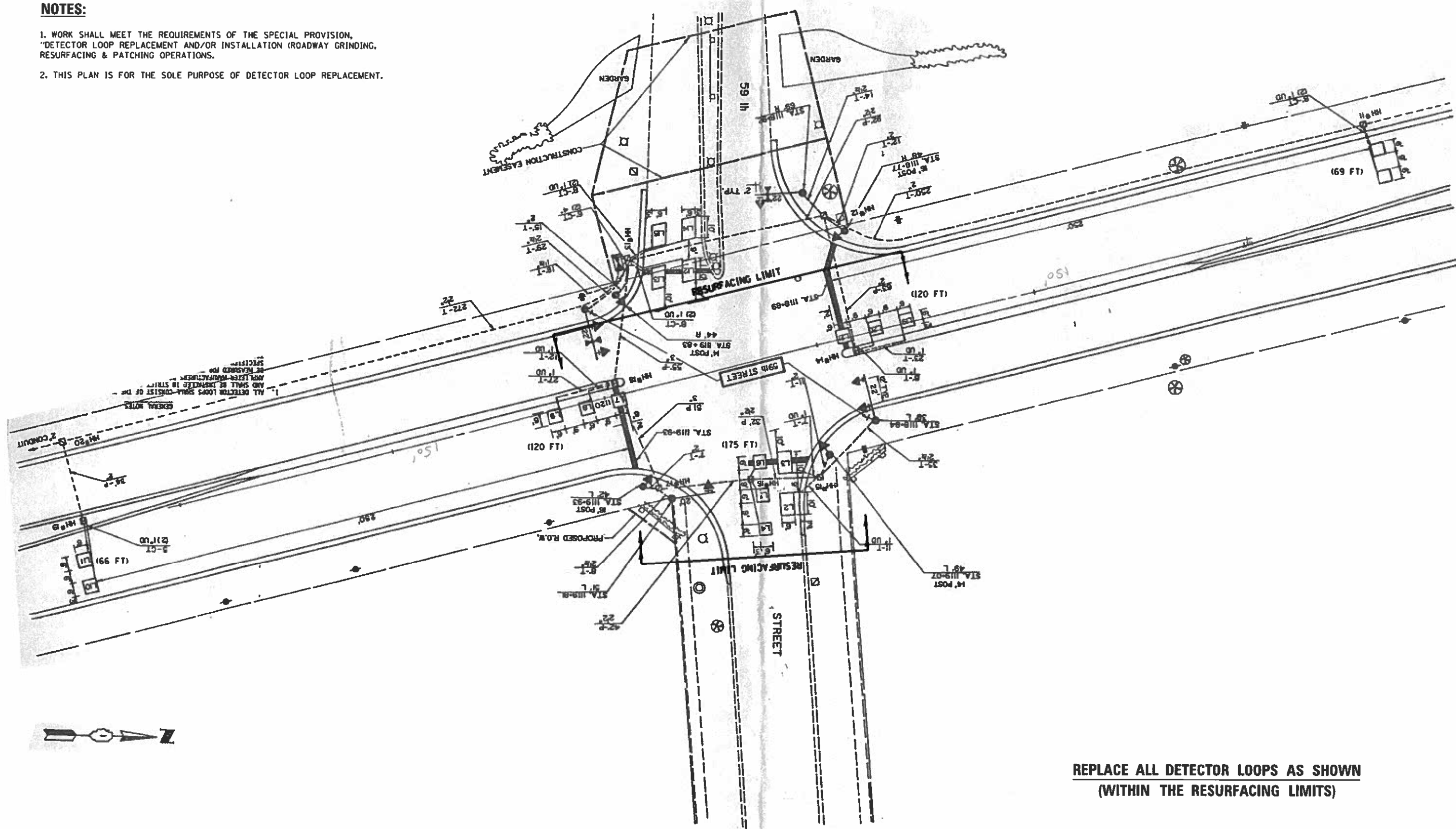
**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

| COD | ITEM                      | QUANTITY | UNIT |
|-----|---------------------------|----------|------|
| 886 | DETECTOR LOOP REPLACEMENT | 905      | FOOT |

|   |                     |               |                                     |   |  |                             |                          |                 |                   |                |
|---|---------------------|---------------|-------------------------------------|---|--|-----------------------------|--------------------------|-----------------|-------------------|----------------|
| FILE NAME = S:\WP\Design\SH.B. of Design - Detector | USER NAME = nguyenh | DESIGNED = SN | REVISED =                           | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>DETECTOR LOOP REPLACEMENT PLAN<br/>IL.53 (LINCOLN AVE.) AT MAPLE AVE.</b> | F.A.P. RTE. = 870           | SECTION = 2018-029-R5-SW | COUNTY = DUPAGE | TOTAL SHEETS = 51 | SHEET NO. = 49 |
| Plot Scale = 40,0000 1/4 in.                        | Checked = SN        | Revised =     | Scale: SHEET OF SHEETS STA. TO STA. |   |  | CONTRACT NO. 62G23          |                          |                 |                   |                |
| Plot Date = 11/7/2018                               | Date = 11/07/2018   | Revised =     |                                     |   |  | (ILLINOIS) FED. AID PROJECT |                          |                 |                   |                |
|   |                     |               |                                     |   |  |                             |                          |                 |                   |                |

**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

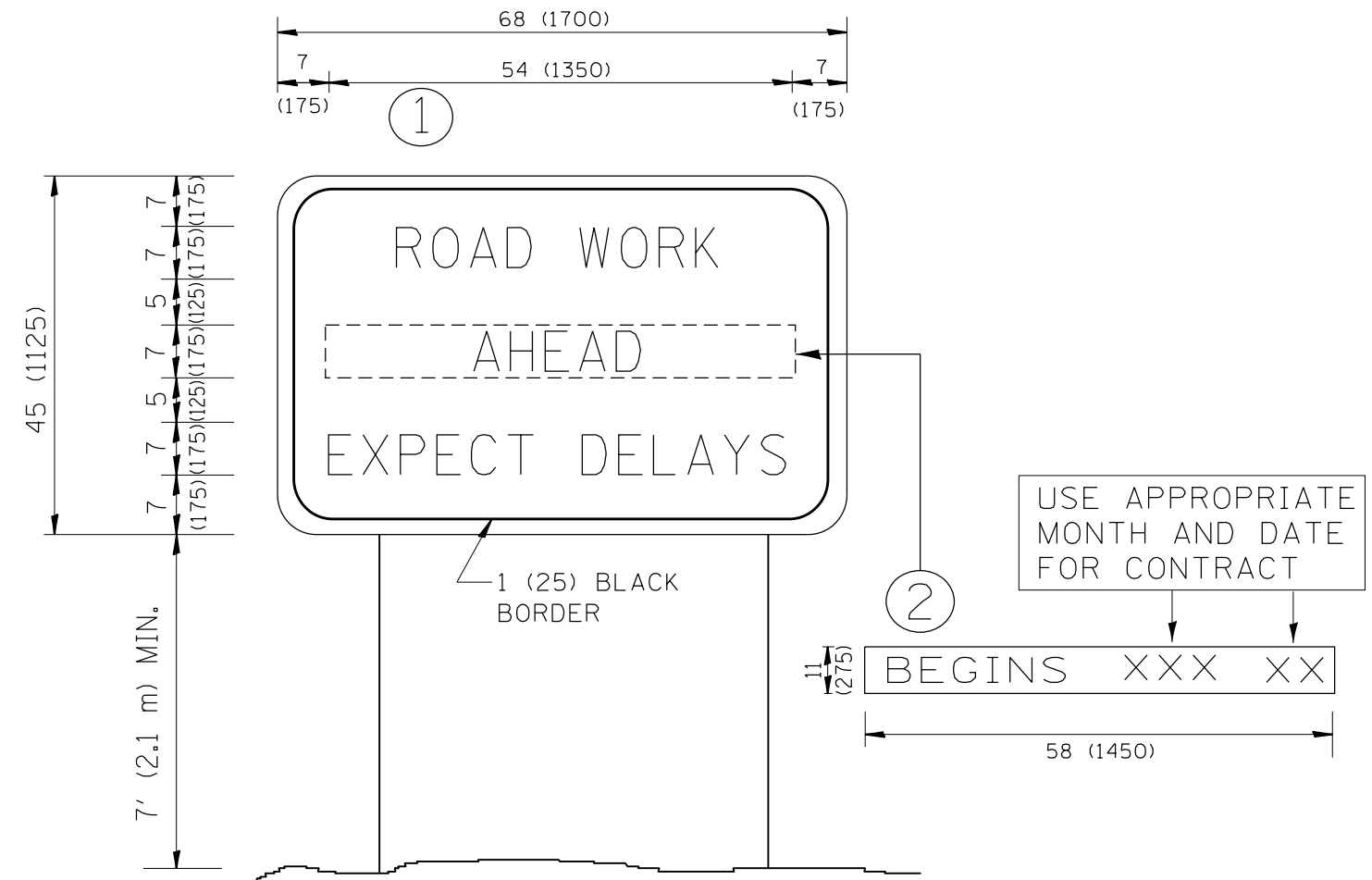
| CODE     | ITEM                      | QUANTITY | UNIT |
|----------|---------------------------|----------|------|
| 88600600 | DETECTOR LOOP REPLACEMENT | 550      | FOOT |

|  |   |                                       |  |
|--|---|---------------------------------------|--|
| FILE NAME =                              | USER NAME = nguyennam                       | DESIGNED = SN                         | REVISED =                                |
| SCRIP:Design\SHI.B. of Design - Detector | Loops Replacement - Resurfacing project - S | DRAWN BY = AOH\OSM gm\2018-62061\CA00 | ORGANIZED = opReplacementSheet,Blank.dgn |
| Default                                  | PLOT SCALE = 40.0000 1/4" = 1'              | CHECKED = SN                          | REVISED =                                |
|  | PLOT DATE = 11/7/2018                       | DATE = 11/07/2018                     | REVISED =                                |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN  
IL.53 (LINCOLN AVE.) AT 59TH STREET**

|                           |                 |              |                 |                        |               |                    |              |
|---------------------------|-----------------|--------------|-----------------|------------------------|---------------|--------------------|--------------|
| SCALE:                    | SHEET OF SHEETS | STA. TO STA. | F.A.P. RTE. 870 | SECTION 2018-029-RS-SW | COUNTY DUPAGE | TOTAL SHEETS 51    | SHEET NO. 50 |
|                           |                 |              |                 |                        |               | CONTRACT NO. 62G23 |              |
| ILLINOIS FED. AID PROJECT |                 |              |                 |                        |               |                    |              |



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

|  |                     |                               |                                 |
|--|---------------------|-------------------------------|---------------------------------|
| FILE NAME =  | USER NAME = steedpa | DESIGNED -                    | REVISED - R. MIRS 09-15-97      |
| p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI3220\Drawings\Design\DistStd.dgn |                     | CHECKED -                     | REVISED - R. MIRS 12-11-97      |
|  |                     | PLOT SCALE = 100.0000' / 1in. | REVISED - T. RAMMACHER 02-02-99 |
|  |                     | DATE -                        | REVISED - C. JUCIUS 01-31-07    |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

|   |                |                    |              |           |
|---|----------------|--------------------|--------------|-----------|
| F.A.P. RTE.                                       | SECTION        | COUNTY             | TOTAL SHEETS | SHEET NO. |
| 870   | 2018-029-RS-SW | DU PAGE            | 51           | 51        |
| TC-22   |                | CONTRACT NO. 62G23 |              |           |
| FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT |                |                    |              |           |