ITEM NO. 10A

IDOT LETTING: APRIL 26, 2019

CONSTRUCTION PLANS - ISSUED MARCH 1, 2019

REPAIR EXISTING INLETS IN **MAIN APRON AREA**

MID-AMERICAN AIR CENTER LAWRENCEVILLE-VINCENNES INTERNATIONAL AIRPORT LAWRENCE COUNTY, ILLINOIS

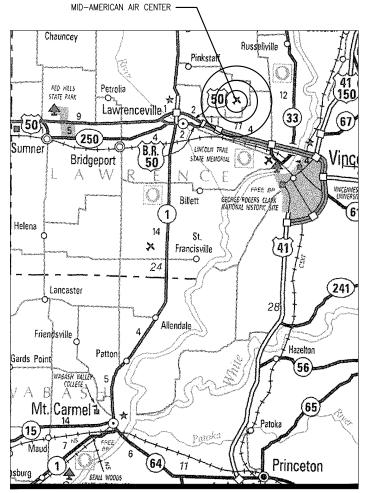
IDA PROJECT NO. LWV-4674 SBG PROJECT NO. 3-17-SBGP-144

SCOPE OF WORK

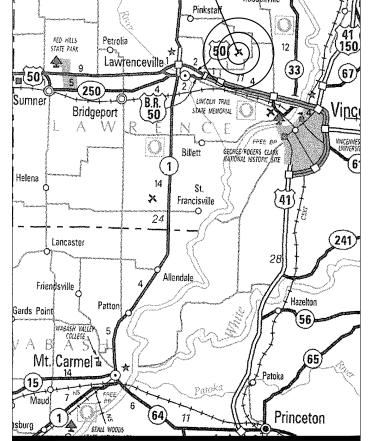
THIS PROJECT CONSISTS OF REPLACEMENT OF EXISTING STORM INLETS AND REPAIRS TO PAVEMENT AREAS LOCATED ACROSS THE MAIN AIRCRAFT APRON

NOTICE TO CONTRACTORS AND BIDDERS

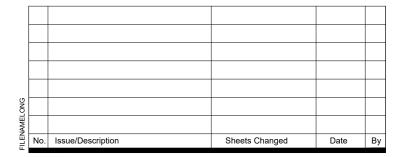
THESE CONSTRUCTION PLANS RELY UPON THE SPECIAL PROVISIONS AND THE SPECIFICATIONS TO PROVIDE FOR A COMPLETE DESCRIPTION OF THE WORK AND CONSTRUCTION REQUIREMENTS. THE PLANS SHALL ONLY BE USED IN COMBINATION WITH ALL CONTRACT DOCUMENTS.

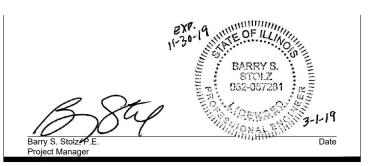






VICINITY MAP







BI-STATE AUTHORITY MID-AMERICAN AIR CENTER LAWRENCEVILLE -VINCENNES INTERNATIONAL AIRPORT Telephone: 618-943-5733



SUMMARY OF QUANTITIES							
ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	AS-BUILT QUANTITY			
AR150520	MOBILIZATION	L.S.	1				
AR209510	CRUSHED AGGREGATE BASE COURSE	TON	284				
AR401613	BIT. SURF. CSEMETHOD I, SUPERPAVE	TON	176				
AR602510	BITUMINOUS PRIME COAT	GAL.	267				
AR603510	BITUMINOUS TACK COAT	GAL.	115				
AR751920	REPLACE INLET	EA.	11				
AR800469	REMOVE BITUMINOUS & PCC PAVEMENT	S.Y.	764				
AR800597	CLEAN EXISTING DRAINAGE STRUCTURE	EA.	2				

GENERAL NOTES:

PAYMENT WILL BE MADE UNDER THE ITEM NUMBERS, DESCRIPTIONS AND UNITS NOTED IN THE ABOVE TABLE IN ACCORDANCE WITH THE BASIS OF PAYMENT FOR EACH RESPECTIVE WORK ITEM COMPLETED AND ACCEPTED BY THE ENGINEER.

CERITIFIED PAYROLLS
THE RESIDENT ENGINEER/TECHNICIAN CANNOT FORWARD CONSTRUCTION REPORTS TO THE ILLINOIS DIVISION OF AERONAUTICS FOR PROCESSING UNTIL ALL CERTIFIED PAYROLLS AND EEO STATEMENTS FOR THE PERIOD HAVE BEEN RECEIVED.

MATERIALS TO BE INCORPORATED INTO THE PROJECT CANNOT BE USED WITHOUT PRIOR APPROVAL.

ALL MATERIALS TO BE USED IN THE PROJECT MUST BE SUBMITTED TO THE RESIDENT ENGINEER FOR

APPROVAL. USE OF MATERIALS WITHOUT PRIOR APPROVAL AND ULTIMATELY DETERMINED TO BE UNACCEPTABLE BY THE ILLINOIS DIVISION OF AERONAUTICS ARE SUBJECT TO REMOVAL AND/OR

INDEX TO SHEETS					
SHEET NUMBER	R SHEET TITLE				
1	COVER SHEET				
2	SUMMARY OF QUANTITIES AND INDEX TO SHEETS				
3	PROPOSED SCOPE OF WORK				
4	PROPOSED SAFETY PLAN - NORTH WORK AREA				
5	PROPOSED SAFETY PLAN - SOUTH WORK AREA				
6	CONSTRUCTION SAFETY DETAILS AND NOTES				
7	PROPOSED CONSTRUCTION PLAN - NORTH 1				
8	PROPOSED CONSTRUCTION PLAN - NORTH 2				
9	PROPOSED CONSTRUCTION PLAN - SOUTH 1				
10	PROPOSED CONSTRUCTION PLAN - SOUTH 2				
11	TYPICAL CONSTRUCTION DETAILS 1				
12	TYPICAL CONSTRUCTION DETAILS 2				

UTILITY NOTE

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES.
PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY.

CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123. CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

J.U.L.I.E. INFORMATION

COUNTY__ LAWRENCE LAWRENCEVILLE TOWNSHIP. ALLISON SECTION NO._ _23 & 26 LAWRENCEVILLE-VINCENNES AIRPORT

13608 HANGAR ROAD LAWRENCEVILLE, ILLINOIS 62439

Offices Nationwide

Hanson Professional Services Inc. 1525 S. 6th Street Springfield, IL 62703 phone: 217-788-2450 fax: 217-788-2503

Illinois Licensed Professional Service Corporation #184-001084

MID-AMERICAN AIR CENTER LAWRENCEVILLE-VINCENNES

BI-STATE AUTHORITY LAWRENCEVILLE, ILLINOIS



REPAIR EXISTING INLETS IN MAIN APRON AREA

IDA No: LWV-4674

SBG No: 3-17-SBGP-144

	NO.	DATE	DES	CRIPT	ION	
	INO.	DATE	DES	DWN	REV	
	ISSUE:	MARCH	1, 201	9		
PROJECT NO: 18A0093						

CAD FILE: G-002_FLP.DWG

DESIGN BY: BSS 11/26/2018 DRAWN BY: CWS 11/26/2018

REVIEWED BY: BSS 03/01/2019

SHEET TITLE

SUMMARY OF **QUANTITIES AND INDEX TO SHEETS** THIS PROJECT CONSISTS OF REPLACEMENT OF EXISTING STORM INLETS AND REPAIRS TO PAVEMENT AREAS LOCATED AROUND THE MAIN AIRCRAFT APRON. THIS PROJECT INCLUDES PAVEMENT REMOVAL, INLET REPLACEMENT, PLACEMENT OF AGGREGATE BASE AND BITUMINOUS PAVEMENT, AND ASSOCIATED ITEMS.

THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIAL, EQUIPMENT, AND TRANSPORTATION NECESSARY TO CONSTRUCT ALL ELEMENTS OF THE PROJECT AS DESCRIBED IN THE CONSTRUCTION PLANS AND SPECIFICATIONS.

THE RULES, REGULATIONS, AND SPECIFICATIONS ENUMERATED HEREIN SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS. THEY SHALL NOT PROHIBIT THE CONTRACTOR FROM FURNISHING AND INSTALLING HIGHER GRADES OF MATERIAL THAN ARE SPECIFIED

THE CONTRACTOR IS NOT PERMITTED TO USE THE AIRPORT AUTO PARKING LOT FOR MATERIAL AND EQUIPMENT STORAGE. THE CONSTRUCTION ENTRANCE AS SHOWN ON THE PROPOSED SAFETY AND PHASING PLAN WILL BE THE PRIMARY ACCESS TO THE PROPOSED CONSTRUCTION SITE. ALL HAULING OF MATERIALS AND EQUIPMENT SHALL BE RESTRICTED TO THE DESIGNATED CONSTRUCTION ENTRANCE.

IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT, PRESERVE AND REPAIR THE EXISTING AIRFIELD AND ROADWAY PAVEMENTS AT ALL TIMES.

THE CONTRACTOR SHALL REPAIR ANY DAMAGE TO EXISTING ELECTRICAL, DRAINAGE, AND PAVEMENT STRUCTURES AT NO ADDITIONAL COST TO THE CONTRACT. ANY DAMAGE TO FAA CABLES, ACCESS ROADS OR TO FAA FACILITIES DURING CONSTRUCTION WILL REQUIRE THE CONTRACTOR TO REPLACE THE DAMAGED CABLES, ACCESS ROAD OR FAA FACILITIES TO FAA REQUIREMENTS AT THE CONTRACTOR'S EXPENSE. SPLICING OF CABLES IS NOT AN ACCEPTABLE FORM OF REPAIR.

NO EQUIPMENT SHALL BE PERMITTED TO CROSS OR USE ANY EXISTING PAVEMENT OUTSIDE THE CONSTRUCTION LIMITS, GENERAL PROJECT AREA OR HAUL ROUTE.

CONTRACTOR IS REQUIRED TO PROVIDE THEIR OWN RESTROOM FACILITIES.

UNLESS OTHERWISE NOTED, ALL DISTURBED AREAS OUTSIDE OF THE PROPOSED CONSTRUCTION LIMITS SHALL BE GRADED, SEEDED AND/OR HYDROMULCH SEEDED AT NO ADDITIONAL COST TO THE CONTRACT.

10. ALL WASTE MATERIAL SHALL BE HAULED FROM THE AIRPORT AND PROPERLY DISPOSED OF UNLESS OTHERWISE SPECIFIED HEREIN.

11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING PERMITS FOR HAULING ON PUBLIC ROADS, AS APPLICABLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ANY DAMAGES TO ANY PAVEMENTS (PUBLIC OR PRIVATE) CAUSED BY HIS/HER CONSTRUCTION EQUIPMENT OR PERSONNEL.

12. THE OWNER SHALL HAVE THE RIGHT OF FIRST REFUSAL FOR ALL SALVAGEABLE MATERIAL REMOVED ON THE PROJECT.

13. THE CONTRACTOR SHALL COORDINATE WITH THE RESIDENT ENGINEER/TECHNICIAN SO THEY MAY DEVELOP ONE SET OF REDLINED AS-BUILT DRAWINGS AT THE COMPLETION

14. CONTRACTOR SHALL NOTE THAT ALL AREAS WITHIN THE AIRPORT PROPERTY LINE AND OUTSIDE THE CONSTRUCTION LIMITS MAY BE USED FOR AGRICULTURAL PURPOSES. THE CONSTRUCTION LIMITS SHALL BE RESTRICTED TO AREAS THAT ARE ABSOLUTELY NECESSARY TO DISTURB TO COMPLETE THE REQUIRED WORK ITEMS. LIMITS SHALL BE COORDINATED WITH THE RESIDENT ENGINEER/TECHNICIAN PRIOR TO BEGINNING ANY WORK. ALL AREAS WHICH HAVE BEEN FARMED AND OR DESIGNATED TO BE FARMED AFTER THE PROJECT COMPLETION. AND HAVE BEEN DISTURBED BY CONSTRUCTION ACTIVITY, SHALL BE CHISEL PLOWED (36" MAX.) OR OTHERWISE SCARIFIED TO RETURN THE AREA TO A REASONABLE TILLABLE CONDITION (IF SO PERMITTED BY THE AIRPORT MANAGER.)

15. CONTRACTOR SHALL RESTORE TO ORIGINAL CONDITION ALL GRASS, STONE, OR PAVEMENT DISTURBED BY CONTRACTOR'S CONSTRUCTION OPERATIONS, STAGING, AND CONSTRUCTION ACCESS ROUTES. DISTURBED AREAS TO BE REPAIRED, GRADED, AND SEEDED AND MULCHED UNLESS OTHERWISE NOTED. STAGING AREA AND SITE ACCESS RESTORATION SHALL BE INCLUDED IN THE COST OF THE HAUL ROUTE.

16. THE PROJECT PAY ITEMS ARE INTENDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL INCIDENTAL WORK REQUIRED TO COMPLETE THE PROJECT TO THE SATISFACTION OF THE RESIDENT ENGINEER/TECHNICIAN IS TO BE INCLUDED IN THE COSTS OF PERFORMING THESE ITEMS.

NORTH WORK AREA

ALL WORK AREAS

JULY 07, 2019

JULY 21, 2019

AUGUST 18, 2019

OCTOBER 06, 2019

OCTOBER 13, 2019

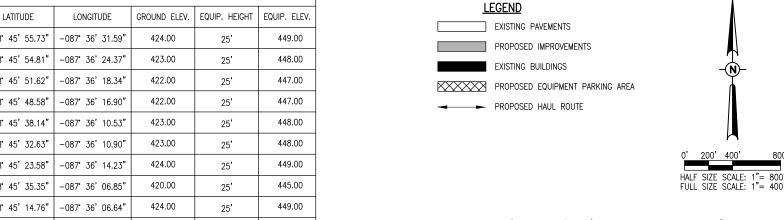
SEPTEMBER 06-13, 2019

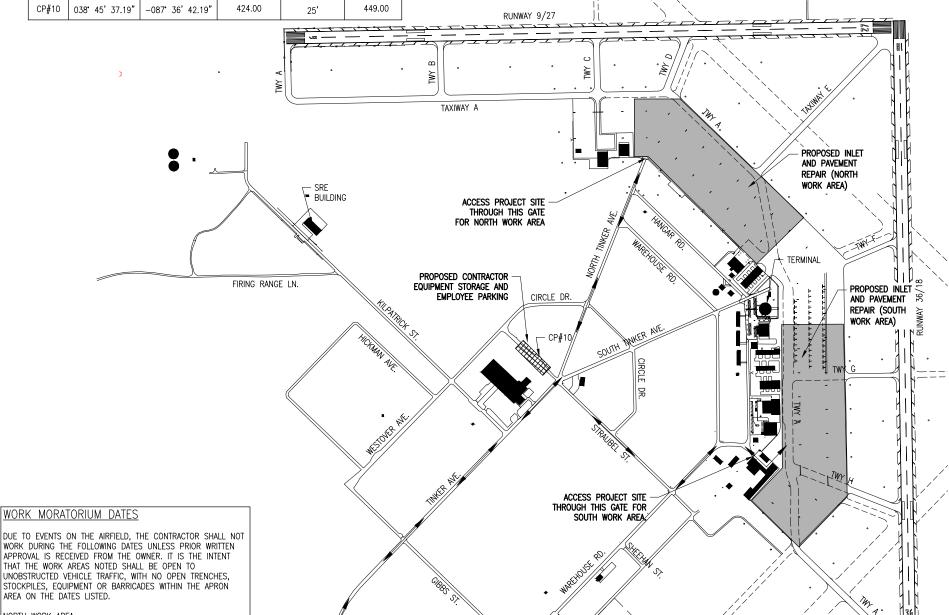
SEPTEMBER 15, 2019

APPROXIMATE LOCATIONS OF UNDERGROUND UTILITIES ARE SHOWN THROUGHOUT THESE PLANS. THE CONTRACTOR SHALL DETERMINE EXACT LOCATIONS AND PROTECT THESE UTILITIES DURING CONSTRUCTION. ANY UTILITIES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL COORDINATE WITH THE PROPER PERSONS FOR THE PURPOSE OF LOCATING AND PROTECTING EXISTING UNDERGROUND UTILITIES.

18. THE CONTRACTOR MUST AT ALL TIMES MAINTAIN PROPER DRAINAGE FOR ALL AREAS AFFECTED BY HIS WORK.

CRITICAL POINTS TABLE					
POINT #	LATITUDE	LONGITUDE	GROUND ELEV.	EQUIP. HEIGHT	EQUIP. ELEV.
CP#1	038* 45' 55.73"	-087* 36' 31.59"	424.00	25'	449.00
CP#2	038* 45' 54.81"	-087* 36' 24.37"	423.00	25'	448.00
CP#3	038* 45' 51.62"	-087* 36' 18.34"	422.00	25'	447.00
CP#4	038* 45' 48.58"	-087* 36' 16.90"	422.00	25'	447.00
CP#5	038* 45' 38.14"	-087* 36' 10.53"	423.00	25'	448.00
CP#6	038* 45' 32.63"	-087* 36' 10.90"	423.00	25'	448.00
CP#7	038* 45' 23.58"	-087* 36' 14.23"	424.00	25'	449.00
CP#8	038* 45' 35.35"	-087* 36' 06.85"	420.00	25'	445.00
CP#9	038* 45' 14.76"	-087* 36' 06.64"	424.00	25'	449.00
CP#10	038* 45' 37.19"	-087* 36' 42.19"	424.00	25'	449.00





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BI-STATE AUTHORITY LAWRENCEVILLE, ILLINOIS



REPAIR EXISTING INLETS IN MAIN APRON AREA

IDA No: LWV-4674

SBG No: 3-17-SBGP-144

	NO.	DATE	DES	CRIPT	ION
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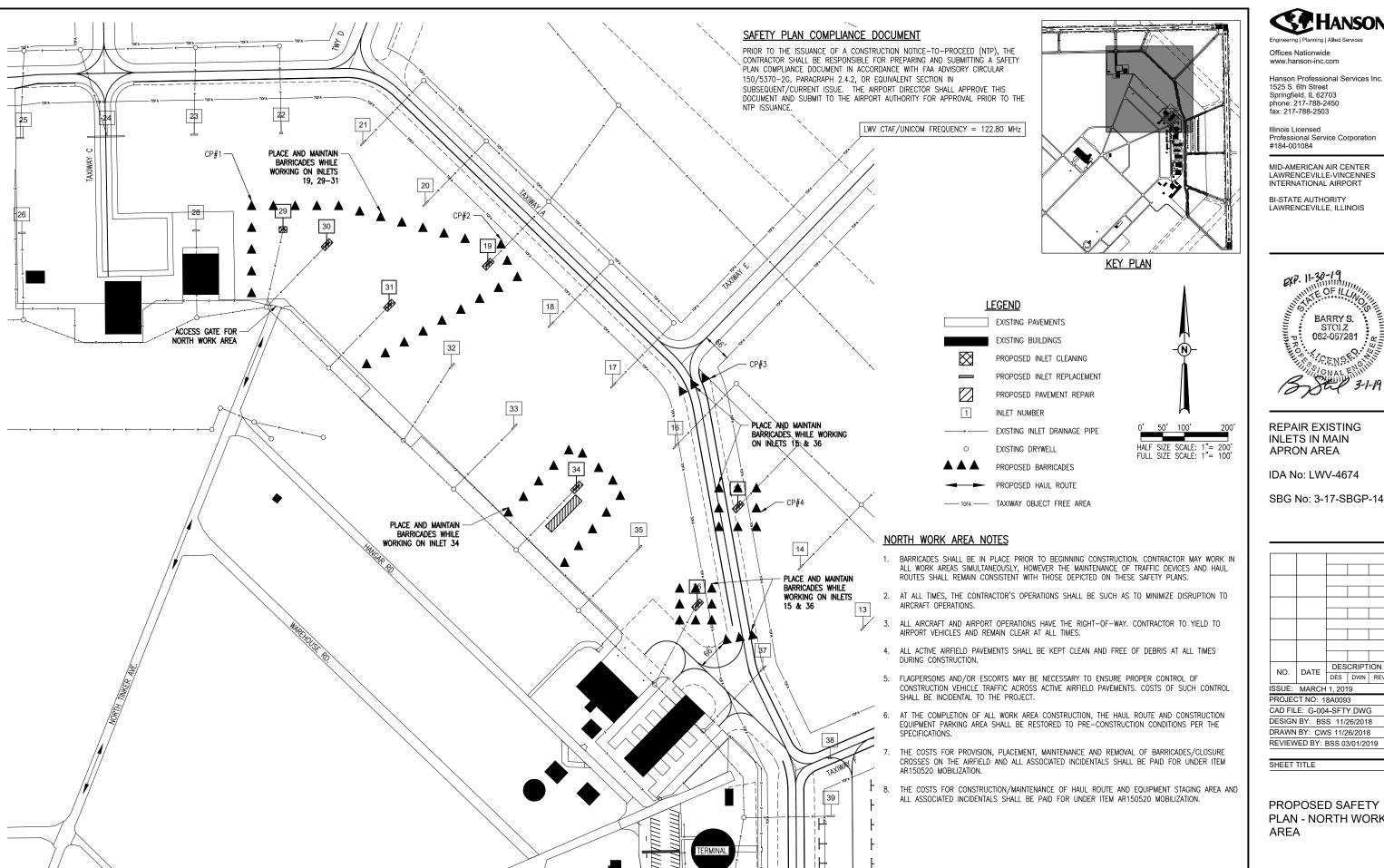
CAD FILE: G-003-SOW.DWG DESIGN BY: BSS 11/26/2018

DRAWN BY: CWS 11/26/2018 REVIEWED BY: BSS 03/01/2019

SHEET TITLE

CP#9

PROPOSED SCOPE OF WORK



Offices Nationwide

Hanson Professional Services Inc. 1525 S. 6th Street Springfield, IL 62703

Illinois Licensed Professional Service Corporation #184-001084

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BI-STATE AUTHORITY LAWRENCEVILLE, ILLINOIS



REPAIR EXISTING INLETS IN MAIN APRON AREA

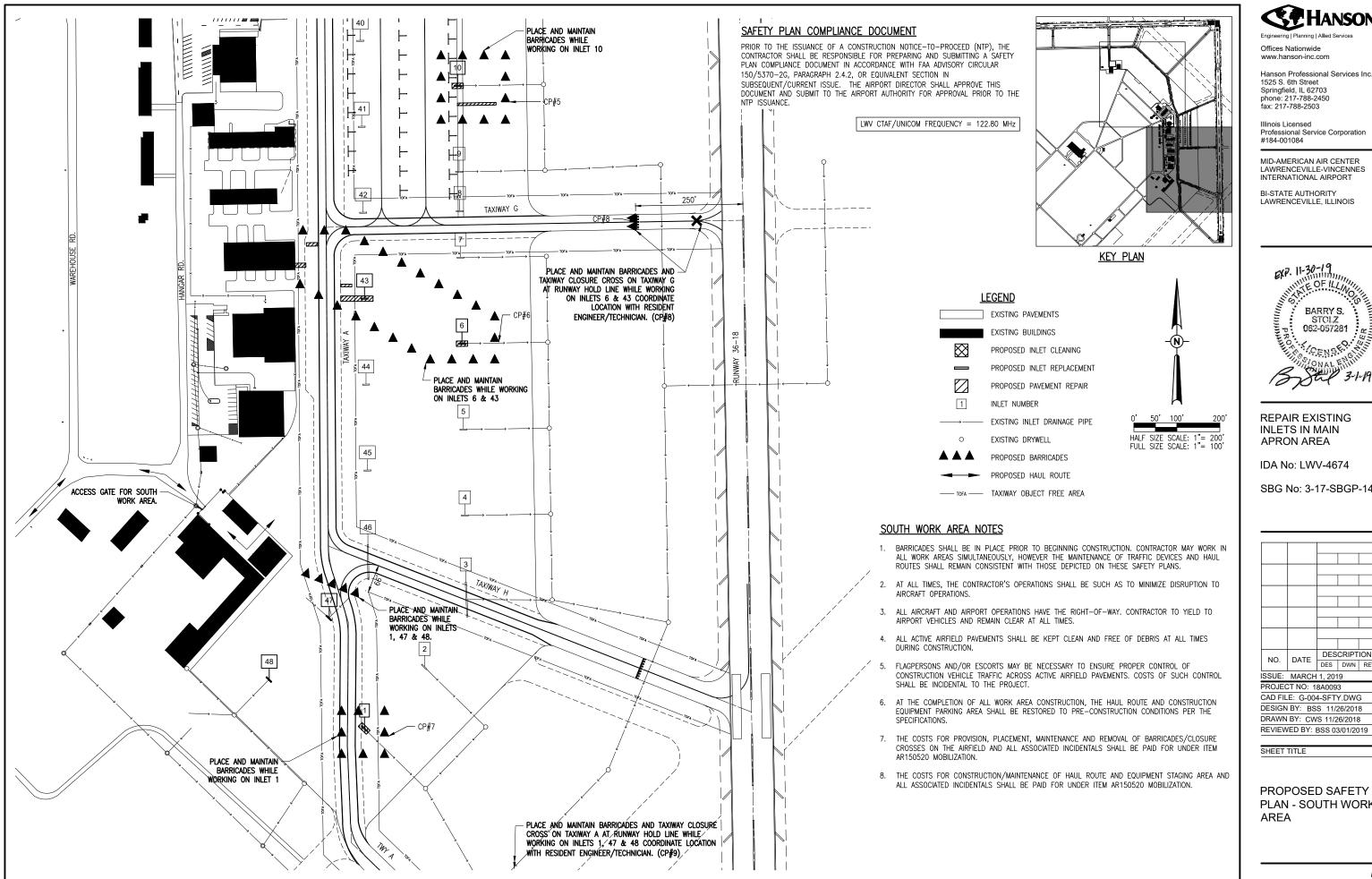
IDA No: LWV-4674

SBG No: 3-17-SBGP-144

DESCRIPTION NO. DATE DES DWN REV ISSUE: MARCH 1, 2019 PROJECT NO: 18A0093 CAD FILE: G-004-SFTY.DWG DESIGN BY: BSS 11/26/2018

SHEET TITLE

PROPOSED SAFETY PLAN - NORTH WORK AREA



Offices Nationwide

Hanson Professional Services Inc. 1525 S. 6th Street Springfield, IL 62703 phone: 217-788-2450 fax: 217-788-2503

Illinois Licensed Professional Service Corporation #184-001084

MID-AMERICAN AIR CENTER LAWRENCEVILLE-VINCENNES

BI-STATE AUTHORITY LAWRENCEVILLE, ILLINOIS



REPAIR EXISTING INLETS IN MAIN APRON AREA

IDA No: LWV-4674

SBG No: 3-17-SBGP-144

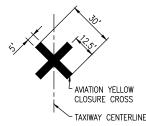
DESCRIPTION NO. DATE DES DWN REV ISSUE: MARCH 1, 2019 PROJECT NO: 18A0093 CAD FILE: G-004-SFTY.DWG DESIGN BY: BSS 11/26/2018

SHEET TITLE

PROPOSED SAFETY PLAN - SOUTH WORK AREA

TAXIWAY OBJECT FREE AREA NOTES

- ACCORDING TO THE FAA ADVISORY CIRCULAR 150/5370-2G (CURRENT EDITION), "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION" CONSTRUCTION ACTIVITY INCLUDING OPEN EXCAVATIONS. MAY BE ACCOMPLISHED WITHIN AN ACTIVE TAXIWAY OBJECT FREE AREA. SUBJECT TO RESTRICTIONS. THIS IS APPLICABLE FOR THIS PROJECT, AND THEREFORE THE FOLLOWING ITEMS APPLY TO THIS CONTRACT, AND THE CONTRACTOR SHALL CONSIDER THESE RESTRICTIONS THROUGHOUT THE DURATION OF BIDDING AND CONSTRUCTION.
- AIRCRAFT TAXIING SPEED SHALL BE LIMITED TO 10 MPH. (THIS IS THE OWNER'S RESPONSIBILITY FOR THIS PROJECT.)
- NOTAMS SHALL BE ISSUED ADVISING TAXIING PILOTS OF HAZARD AND RECOMMENDING REDUCED TAXIING SPEEDS ON THE TAXIWAY. (THIS IS THE OWNER'S RESPONSIBILITY FOR THIS
- LOW-PROFILE BARRICADES SHALL BE IN PLACE AS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN, PRIOR TO CONSTRUCTION OF EACH PHASE, IN ACCORDANCE WITH THE DETAIL AND NOTES ON THIS SHEET. (THIS IS THE CONTRACTOR'S RESPONSIBILITY FOR THIS
- 1.4. A FIVE (5) FOOT CLEARANCE SHALL BE MAINTAINED BETWEEN EQUIPMENT AND MATERIALS AND ANY PART OF AN AIRCRAFT (INCLUDES WINGTIP OVERHANG). IF NECESSARY TO FACILITATE THIS CLEARANCE IT MAY BE NECESSARY TO TEMPORARILY MOVE PERSONNEL AND FOLIPMENT FOR THE PASSAGE OF THAT AIRCRAFT. (THIS IS THE CONTRACTOR'S RESPONSIBILITY FOR THIS
- FLAGGERS FURNISHED BY THE CONTRACTOR SHALL BE USED TO DIRECT AND CONTROL CONSTRUCTION EQUIPMENT AND PERSONNEL TO A PRE-ESTABLISHED SETBACK DISTANCE FOR SAFE PASSAGE OF AIRCRAFT, AND SHALL BE USED TO DIRECT TAXIING AIRCRAFT. (THIS IS THE CONTRACTOR'S RESPONSIBILITY FOR THIS PROJECT.)
- 1.6. THE CONTRACTOR SHALL PROVIDE THE FLAGGER PERSONNEL WITH AN AIRCRAFT BAND RADIO WHICH SHALL BE USED TO MONITOR AIRCRAFT TRAFFIC VIA THE UNICOM FREQUENCY (122.80 MHZ) (THIS IS THE CONTRACTOR'S RESPONSIBILITY FOR THIS PROJECT.)
- WHEN WORK IS NOT BEING PERFORMED WITHIN AN ACTIVE TAXIWAY OBJECT FREE AREA AND DURING ALL NIGHT-TIME HOURS, ALL EQUIPMENT, MATERIALS AND PERSONNEL SHALL REMAIN CLEAR OF THE TAXIWAY OBJECT FREE AREA.
- WORK WITHIN THE TAXIWAY OBJECT FREE AREA AS SHOWN SHOULD BE EXPEDITED TO THE BEST EXTENT POSSIBLE IN ORDER TO AVOID PROLONGED DISRUPTION TO NORMAL AIRCRAFT OPERATIONS.
- ALL AIRCRAFT AND AIRPORT OPERATIONS HAVE THE RIGHT-OF-WAY. CONTRACTOR TO YIELD TO VEHICLES AND REMAIN CLEAR AT ALL TIMES.



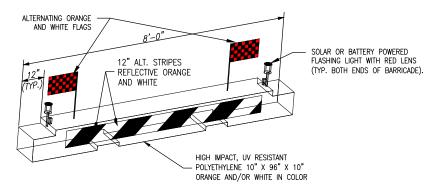
TAXIWAY CLOSURE CROSS MARKER DETAIL

NOT TO SCALE

- TEMPORARY CLOSURE CROSS MARKINGS SHALL BE CONSTRUCTED OF PLYWOOD, SNOW FENCE OR APPROVED FABRIC AND SHALL BE SECURED TO PAVEMENT BY APPROVED METHOD.
- 2. COST FOR PROVIDING, PLACING, MAINTAINING, RELOCATING AND REMOVING CLOSURE CROSSES SHALL BE INCLUDED IN THE COST OF THE MOBILIZATION ITEM.

BARRICADE NOTES

- 1. ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SLIPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE, THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN.
- 2. BARRICADES SHALL BE "LOW-PROFILE" WITH A MAXIMUM HEIGHT OF 18" ABOVE GROUND, EXCLUSIVE OF ASSOCIATED WARNING LIGHTS AND FLAGS.
- 3. BARRICADES SHALL BE SPACED END TO END THE WIDTH OF THE PAVEMENT, WITH GAPS BETWEEN BARRICADES NOT TO EXCEED 4' WIDE. BARRICADES ARE TO BE SET BACK 66' FROM THE ACTIVE TAXIWAY CENTERLINE OR AS SHOWN ON THE
- 4. CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE, LENS DIRECTED. ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY MAY BE USED IN EITHER A STEADY BURN (TYPE C) OR LOW INTENSITY FLASHING MODE (TYPE A) UNLESS NOTED OTHERWISE.
- 5. THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR BEFORE SUNSET AND 1/2 HOUR AFTER SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION.
- 6. BARRICADES SHALL BE SECURED TO THE GROUND BY APPROVED METHODS TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS.
- 7. THE ONLY COLOR COMBINATION ON BARRICADES IS ORANGE AND WHITE. THE ORANGE STRIPES SHALL BE ENCAPSULATED LENS REFLECTIVE SHEETING. THE WHITE STRIPES SHALL BE EITHER ENCAPSULATED OR ENCLOSED LENS REFLECTIVE SHEETING AND MUST BE IN ACCEPTABLE CONDITION.
- 8. COST FOR PROVIDING, PLACING, MAINTAINING, AND REMOVING BARRICADES SHALL BE INCLUDED IN THE COST OF THE MOBILIZATION ITEM.



LOW-PROFILE BARRICADE DETAIL

DETAIL ABOVE REPRESENTS ONE OPTION FOR LOW-PROFILE BARRICADES. OTHER OPTIONS MAY BE UTILIZED AS LONG AS THEY MEET THE REQUIREMENTS OF THE PROJECT, INCLUDING BARRICADE NOTE 1.

SAFETY NOTES

- 1. FOLLOWING ARE THE CONSTRUCTION SAFETY PROCEDURES THAT THE CONTRACTOR SHALL FOLLOW THROUGHOUT THIS PROJECT. ADDITIONAL REQUIREMENTS ARE SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET AND THIS
- 2. ALL PROVISIONS OF THE LATEST EDITION OF FAA ADVISORY CIRCULAR AC 150/5370-2 (CURRENT EDITION). "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", APPLY TO THIS CONTRACT, EXCEPT AS MODIFIED BY THIS SAFETY PLAN, OR AS MODIFIED BY THE OWNER THROUGH THE RESIDENT ENGINEER/TECHNICIAN AT THE PRECONSTRUCTION CONFERENCE, OR DURING THE COURSE OF THE CONTRACT.
- 3. THE CONTRACTORS SHALL MINIMIZE DISRUPTION OF STANDARD OPERATING PROCEDURES FOR AERONAUTICAL ACTIVITY BY REMAINING WITHIN THE PRESCRIBED STAGING, CONSTRUCTION, AND PHASING AREAS PRESENTED ON THE CONSTRUCTION
- 4. NO UNAUTHORIZED PERSONNEL SHALL ENTER ANY AREA OF THE AIRPORT THAT COULD POTENTIALLY BE HAZARDOUS. THE AIRPORT MANAGER RESERVES THE RIGHT TO SUSPEND OPERATIONS IN ORDER TO MAINTAIN SAFETY AT THE AIRPORT.
- CONTRACTOR EQUIPMENT, VEHICLES, AND PROJECT MATERIALS SHALL BE STORED IN THE EQUIPMENT PARKING/STAGING AREA SHOWN ON THE PLAN VIEW, EXCEPT AS OTHERWISE PROVIDED FOR AT THE PRECONSTRUCTION CONFERENCE.
- 6. ALL CONSTRUCTION EQUIPMENT OPERATING IN THE PRESCRIBED CONSTRUCTION AREA IS REQUIRED TO DISPLAY A CHECKERBOARD FLAG PROPERLY LOCATED OR A ROTATING BEACON (STROBE) AS SPECIFIED IN AC 150/5210-5. "PAINTING. MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT" LATEST EDITION
- 7. NO CONSTRUCTION MATERIAL STOCKPILES SHALL BE LOCATED WITHIN 250' OF ANY ACTIVE RUNWAY, WITHIN 66' OF ANY OTHER ACTIVE AIRPORT OPERATIONS AREA, OR PENETRATE A PART 77 IMAGINARY SURFACE (PROVIDED BY THE RESIDENT ENGINEER/TECHNICIAN) EXTENDING OUT AND UPWARDS FROM ALL SIDES OF AN ACTIVE RUNWAY
- 8. CLOSED AIRFIELD PHASING AREAS, OPEN TRENCHES, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH LIGHTED BARRICADES WITH STEADY BURNING OR FLASHING RED LIGHTS AS SPECIFIED IN 150/5370-2, "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION, LATEST EDITION, LIGHTED BARRICADES MUST BE NO TALLER THAN 18" (EXCLUSIVE OF SUPPLEMENTARY LIGHTS AND FLAGS) ON THE TAXIWAYS AND COMPLY WITH ADVISORY CIRCULAR 150/5370-2. LATEST EDITION, CONTRACTOR SHALL NIGHT CHECK BARRICADES DAILY FOR PROPER OPERATION.
- NO OPEN TRENCHES WITHIN 250' OF AN ACTIVE RUNWAY CENTERLINE OR WITHIN 66' OF ANY AIRPORT OPERATIONS AREA WILL BE PERMITTED UNLESS PROPERLY MARKED. OTHER TRENCHES SHALL BE MAINTAINED SAFE, I.E., BARRICADED OR COVERED WITH STEEL PLATES IN ALL OTHER AREAS.
- 10. OPEN TRENCHES, EXCAVATIONS, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHOULD BE PROMINENTLY MARKED WITH ORANGE FLAGS AND LIGHTED WITH FLASHING RED LIGHTS DURING HOURS OF RESTRICTED VISIBILITY AND/OR
- 11. NO CONSTRUCTION EQUIPMENT GREATER THAN 25' TALL WILL BE PERMITTED ON THE AIRPORT. HOWEVER OTHER EQUIPMENT TALLER THAN 25' MAY BE PERMITTED WITH THE APPROVAL OF THE AIRPORT MANAGER AND AIRSPACE APPROVAL BY THE
- 12. NO OPEN FLAME WELDING OR TORCH CUTTING OPERATION IS PERMITTED UNLESS ADEQUATE FIRE AND SAFETY PRECAUTIONS ARE PROVIDED AND HAVE BEEN APPROVED BY THE AIRPORT MANAGER NO FLARE POTS ARE ALLOWED ON THE PROJECT.
- 13. SOIL, DEBRIS, AND LOOSE MATERIAL DROPPED OR TRACKED ONTO AIRPORT ROADS, TAXIWAYS, AND SOD SURFACES, OR WHICH CAN BE BLOWN ONTO SUCH SURFACES, SHALL BE IMMEDIATELY SWEPT, PICKED UP AND REMOVED, OR PLACED INTO CLOSED CONTAINERS. ANY DAMAGE TO AIRPORT PROPERTY SHALL BE REPAIRED IMMEDIATELY AT NO COST TO THE OWNER
- 14. EACH CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND MAINTAINING AIRPORT LIGHTING AND NAVIGATIONAL ELECTRICAL SYSTEMS DURING CONSTRUCTION. A CONTACT PERSON AND TELEPHONE NUMBER FOR 24 HOUR EMERGENCY IMMEDIATE REPAIR SHALL BE SUBMITTED TO THE AIRPORT MANAGER AND RESIDENT ENGINEER/TECHNICIAN. HAUL ROUTES CROSSING PAVEMENT, DRAINAGE, MISCELLANEOUS. STRUCTURES AND/OR AIRFIELD CABLES SHALL BE PROTECTED FROM
- 15. ALL AIRCRAFT AND AIRPORT OPERATIONS HAVE THE RIGHT-OF-WAY. CONTRACTOR TO YIELD TO VEHICLES AND REMAIN CLEAR AT ALL TIMES.
- 16. CONTRACTOR SHALL PLACE, SECURE, AND MAINTAIN LIGHTED BARRICADES AND CLOSURE CROSSES WHEN A RUNWAY/TAXIWAY/APRON IS CLOSED OR AS REQUIRED BY THE PLANS AND DESIGNATED BY THE RESIDENT FNGINFER/TECHNICIAN
- 17. CONTRACTOR SHALL MARK HAZARDOUS AREA WITH STEADY-BURNING OR FLASHING RED LIGHTS DURING PERIODS OF LOW
- 18. THE CONTRACTOR SHALL PERIODICALLY PERFORM ONSITE INSPECTIONS THROUGHOUT THE DURATION OF THE PROJECT WITH THE IMMEDIATE REMEDY OF ANY DIFFERENCES, WHETHER CAUSED BY NEGLIGENCE, OVERSIGHT, OR PROJECT SCOPE
- 19. CONTRACTOR SHALL MOVE MAINTENANCE OF TRAFFIC COMPONENTS AT THE WRITTEN DIRECTION OF THE RESIDENT ENGINEER/TECHNICIAN AT NO ADDITIONAL COST.
- 20. CONTRACTOR SHALL NOT REMOVE THE BARRICADES WITHOUT THE APPROVAL BY THE RESIDENT ENGINEER/TECHNICIAN.
- 21. CONTRACTOR SHALL MAINTAIN FLASHERS, SIGNS AND/OR BARRICADES AS REQUIRED BY THE PLANS, CITY OR COUNTY REGULATIONS OR CONTRACTOR ACTIVITIES. CONTRACTOR SHALL OBTAIN ANY AND ALL REQUIRED LOCAL PERMITS UNLESS SPECIFIED OTHERWISE.
- 22. THE CONTRACTOR SHALL UTILIZE WATER AND/OR CHEMICALS APPROVED BY THE RESIDENT ENGINEER/TECHNICIAN AS NECESSARY TO CONTROL DUST
- 23. NO CONSTRUCTION VEHICLES SHALL BE DRIVEN ACROSS ANY ACTIVE RUNWAY, INCLUDING TURF RUNWAYS. CONSTRUCTION EQUIPMENT OR CONSTRUCTION ACTIVITY WILL NOT BE PERMITTED WITHIN 250' OF ANY ACTIVE RUNWAY CENTERLINE OR WITHIN 66' OF ANY OTHER ACTIVE AIRPORT TAXIWAY OR APRON. HOWEVER, CONSTRUCTION MAY BE PERMITTED IN THESE AREAS IF THE CONTRACTOR HAS GAINED APPROVAL FROM THE AIRPORT MANAGER AT LEAST 7 DAYS IN ADVANCE OF THE SCHEDULED CONSTRUCTION PERIOD AND THE OPERATIONAL AREA IS CLOSED TO TRAFFIC AND PROPER NOTAMS ARE ISSUED
- 24. UNLESS SPECIFIED OTHERWISE, COST FOR THE ABOVE IS TO BE CONSIDERED INCIDENTAL TO THE PROJECT. SEPARATE PAYMENT SHALL NOT BE MADE.



Offices Nationwide

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MID-AMERICAN AIR CENTER LAWRENCEVILLE-VINCENNES

BI-STATE AUTHORITY LAWRENCEVILLE. ILLINOIS



REPAIR EXISTING INLETS IN MAIN APRON AREA

IDA No: LWV-4674

SBG No: 3-17-SBGP-144

DESCRIPTION NO. DATE DES DWN REV ISSUE: MARCH 1, 2019

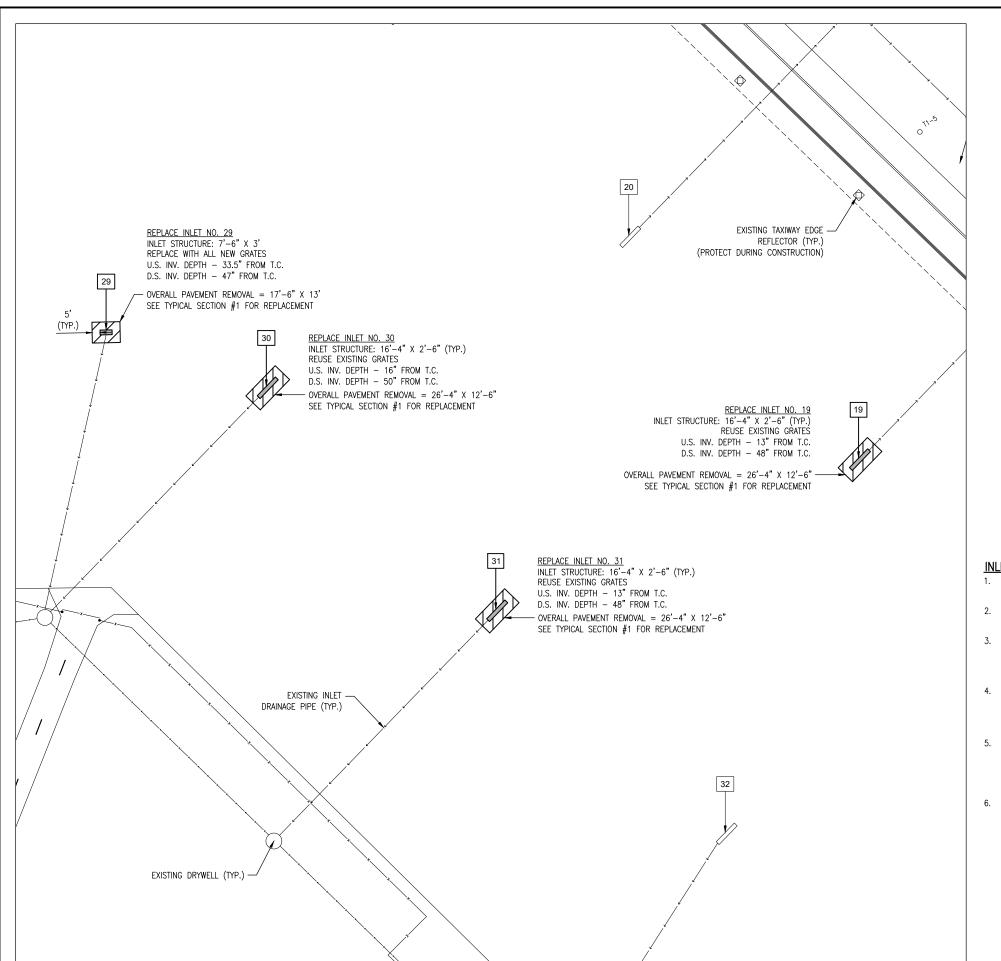
PROJECT NO: 18A0093

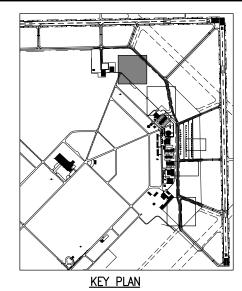
CAD FILE: G-501-SFY.DWG DESIGN BY: BSS 02/07/2019 DRAWN BY: CWS 02/07/2019

REVIEWED BY: BSS 03/01/2019

SHEET TITLE

CONSTRUCTION SAFETY DETAILS AND NOTES





LEGEND EXISTING PAVEMENTS EXISTING BUILDINGS PROPOSED INLET CLEANING PROPOSED INLET REPLACEMENT PROPOSED PAVEMENT REPAIR EXISTING INLET 1 INLET NUMBER HALF SIZE SCALE: 1"= 60' FULL SIZE SCALE: 1"= 30' — EXISTING INLET DRAINAGE PIPE EXISTING DRYWELL

INLET REPLACEMENT NOTES:

THE CONTRACTOR SHALL REPLACE INLETS AT LOCATIONS SHOWN ON THIS SHEET AND IN ACCORDANCE WITH DETAILS AND SPECIFICATIONS.

○ ☑ EXISTING TAXIWAY LIGHT/REFLECTOR

- 2. ALL INVERT DEPTH MEASUREMENTS ARE APPROXIMATE. CONTRACTOR SHALL FIELD VERIFY PRIOR TO REPLACEMENT OF INLETS.
- UPSTREAM INVERT DEPTH MEASUREMENTS SHOWN REPRESENT APPROXIMATE EXISTING DEPTHS. THE CONTRACTOR MAY CHOOSE TO ALTER THE PROPOSED UPSTREAM INVERT DEPTHS IN ORDER TO ALLOW FOR EASIER CONSTRUCTION OF NEW INLET STRUCTURES, PROVIDING THE NEW INLET HAS POSITIVE DRAINAGE TO THE DOWNSTREAM INVERT CONNECTION TO THE OUTLET STORM PIPE.
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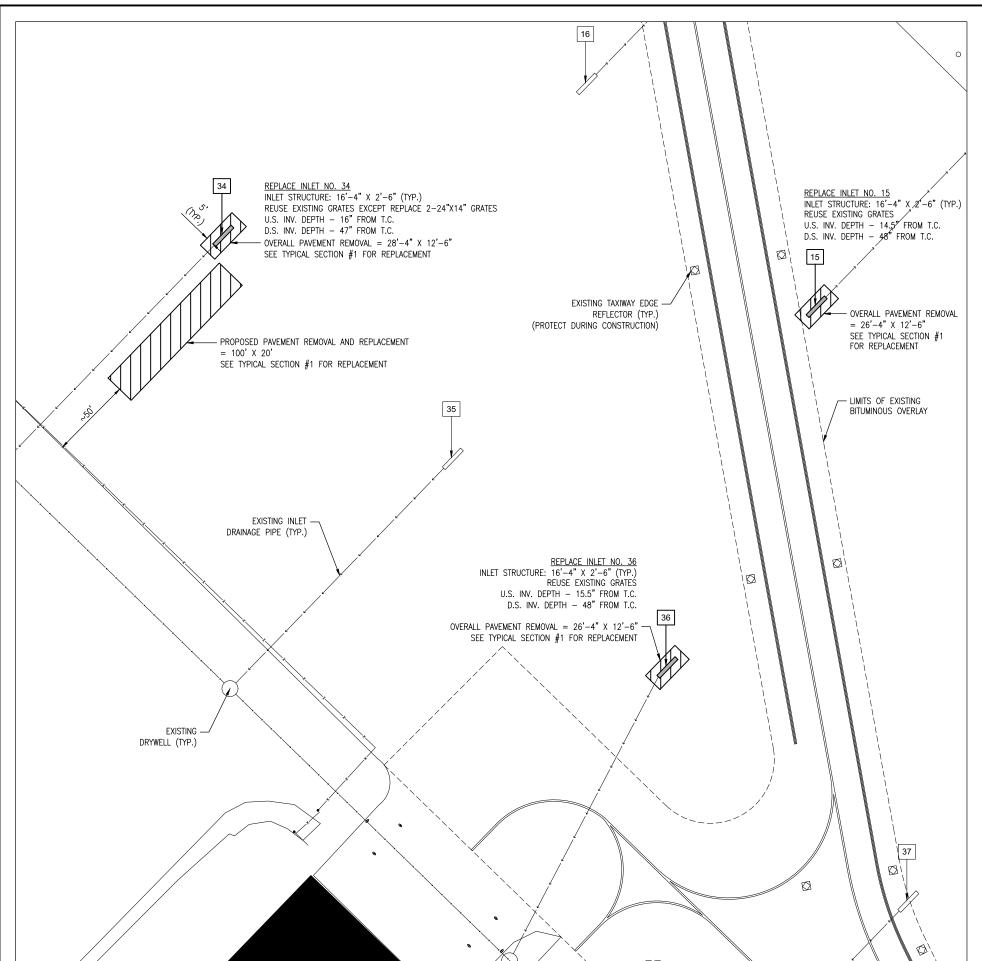
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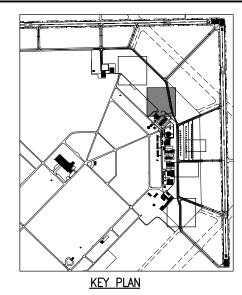
DESIGN BY: JRH 02/06/2019 DRAWN BY: JRH 02/06/2019

REVIEWED BY: BSS 03/01/2019

SHEET TITLE

PROPOSED CONSTRUCTION PLAN - NORTH 1





LEGEND

EXISTING PAVEMENTS

EXISTING BUILDINGS

PROPOSED INLET CLEANING

PROPOSED INLET REPLACEMENT

PROPOSED PAVEMENT REPAIR

EXISTING INLET

INLET NUMBER

EXISTING INLET DRAINAGE PIPE

EXISTING INLET DRAINAGE PIPE

EXISTING DRYWELL

EXISTING TAXIWAY LIGHT/REFLECTOR

INLET REPLACEMENT NOTES:

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BI-STATE AUTHORITY LAWRENCEVILLE, ILLINOIS



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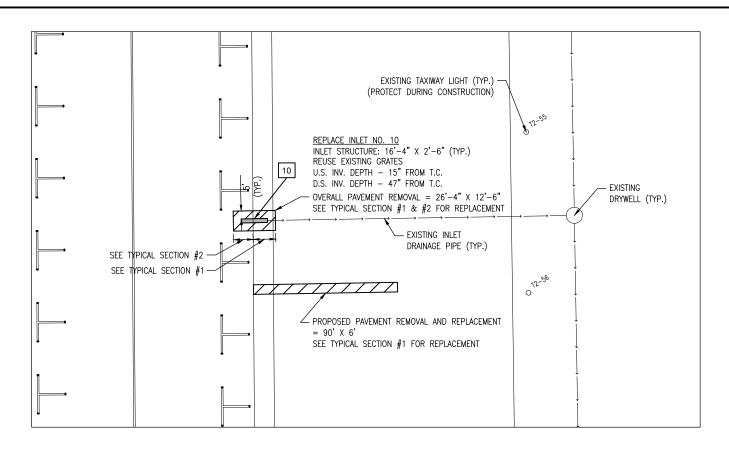
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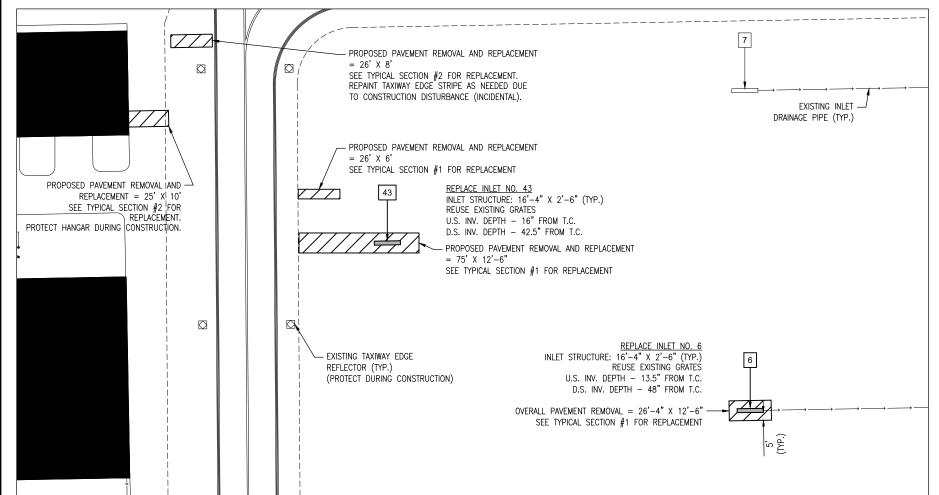
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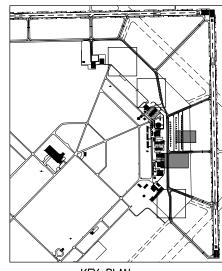
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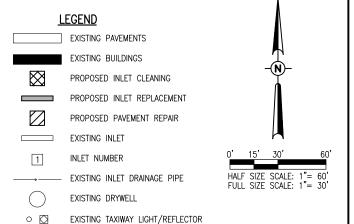
PROPOSED CONSTRUCTION PLAN - NORTH 2







<u>KEY PLAN</u>



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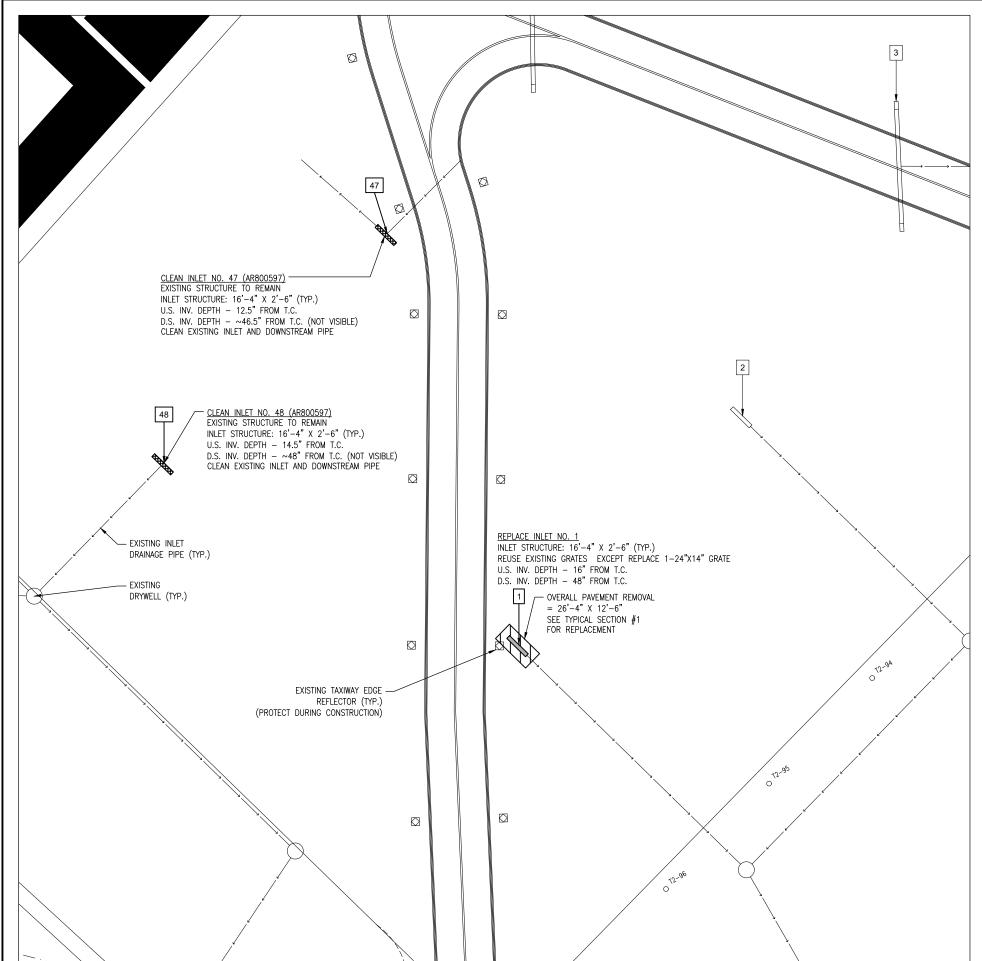
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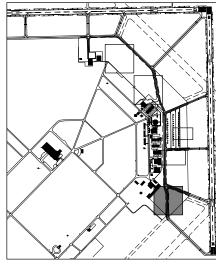
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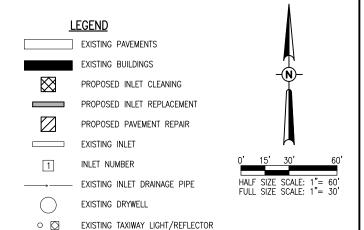
PROPOSED CONSTRUCTION PLAN - SOUTH 1

SHEET TITLE





KEY PLAN



INLET REPLACEMENT NOTES:

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REPAIR EXISTING INLETS IN MAIN APRON AREA

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SBG No: 3-17-SBGP-144

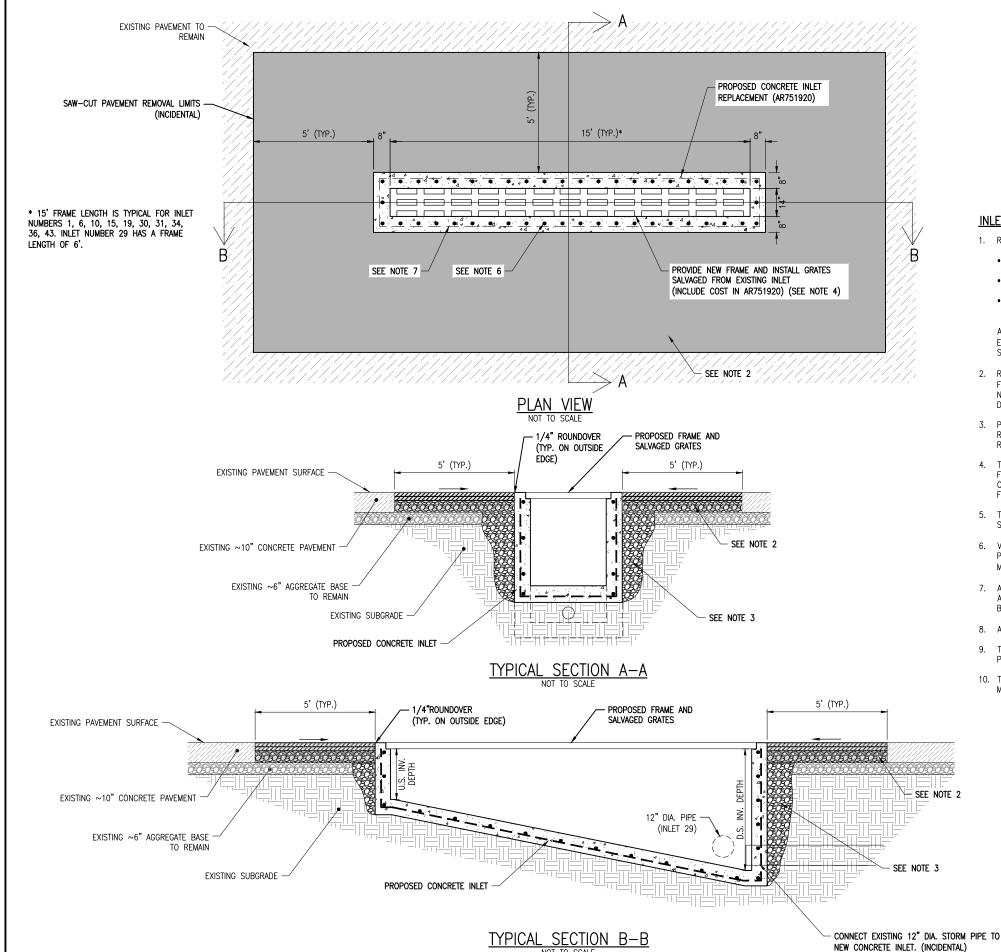
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CAD FILE: C-101-CON.DWG					

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DRAWN BY: JRH 02/06/2019 REVIEWED BY: BSS 03/01/2019

SHEET TITLE

PROPOSED CONSTRUCTION PLAN - SOUTH 2



INLET REPLACEMENT NOTES:

- 1. REPLACEMENT OF INLET STRUCTURES MAY OCCUR BY:
 - COMPLETE REMOVAL OF EXISTING STRUCTURE AND REPLACEMENT WITH A NEW PRE-CAST STRUCTURE.
- COMPLETE REMOVAL OF EXISTING STRUCTURE AND REPLACEMENT WITH A CAST-IN-PLACE STRUCTURE, OR
- PARTIAL REMOVAL OF THE EXISTING STRUCTURE AND RECONSTRUCTION WITH CAST—IN—PLACE COMPONENTS.

ANY PARTIAL REMOVAL/RECONSTRUCTION SHALL BE APPROVED BY THE RESIDENT ENGINEER/TECHNICIAN PRIOR TO REMOVAL, AND ALL OTHER DIMENSIONS AND CONSTRUCTION STANDARDS SHALL BE ADHERED TO IN ORDER TO MEET THE INTENT OF THE PROJECT SCOPE.

- REMOVE EXISTING PAVEMENT TO BOTTOM OF CONCRETE (~10" FOR TYPICAL SECTION #1; ~14"
 FOR TYPICAL SECTION #2) AND REPLACE WITH PROPOSED PAVEMENT AS SHOWN ON NEXT SHEET.
 NEW PAVEMENT SECTION SHALL BE PAID FOR UNDER THE ITEMS LISTED ON THE PROPOSED
 DETAIL.
- PROPOSED EARTH OR AGGREGATE BACKFILL, AS REQUIRED TO FILL VOID FROM INLET REPLACEMENT, SHALL NOT BE MEASURED FOR PAYMENT AND SHALL BE INCIDENTAL TO INLET REPLACEMENT, AR751920.
- 4. THE CONTRACTOR SHALL OBTAIN A NEW INLET FRAME AND RE-USE THE EXISTING GRATES. THE FRAME SHALL BE NEENAH R4990-DX, TYPE C; EAST JORDAN V-7344, OR AN APPROVED EQUAL. CONTRACTOR SHALL VERIEY DIMENSIONS OF EXISTING GRATES WILL BE COMPATIBLE WITH NEW FRAMES EPIDE TO OPDEPING
- 5. THE CONCRETE USED FOR THE REPLACEMENT INLET SHALL CONFORM TO IDOT AERONAUTICS STANDARD SPECIFICATION ITEM 610. STRUCTURAL PORTLAND CEMENT CONCRETE.
- 6. VERTICAL REINFORCING BARS (No. 4) SHALL BE SPACED 12" ON CENTER AROUND THE INLET PERIMETER. AS APPLICABLE, THE VERTICAL BARS SHALL BE CUT TO FIT IN LENGTH WITH A MINIMUM OF 3" OF THE BAR DRILLED AND GROUTED INTO THE EXISTING STRUCTURE.
- 7. A HORIZONTAL STRESS RING (NO. 4 BAR) SHALL BE SPACED 6" MAX. AND RUN CONTINUOUSLY
 AROUND THE PERIMETER OF THE STRUCTURE AND SHALL BE TIED TO THE VERTICAL REINFORCING
 BARS
- 8. ALL REINFORCING STEEL SHALL HAVE A MINIMUM 2" COVER FROM THE CONCRETE WALL EDGES.
- 9. THE PROPOSED INLET SHALL BE FLUSH WITH THE SURROUNDING PAVEMENT. THE REPLACEMENT PAVEMENT SHALL HAVE POSITIVE DRAINAGE TO THE NEW INLET.
- 10. THICKNESS OF EXISTING FEATURES SHOWN MAY VARY BY LOCATION. NO ADJUSTMENTS WILL BE MADE TO PAY ITEMS OR UNIT PRICES DUE TO THICKNESS VARIATIONS.



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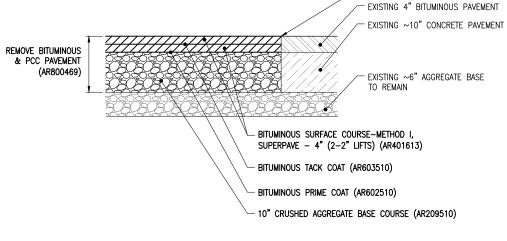
REVIEWED BY: BSS 03/01/2019

SHEET TITLE

TYPICAL CONSTRUCTION DETAILS 1

TYPICAL PAVEMENT REPLACEMENT SECTION #1

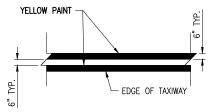
NOT TO SCALE



SAW-CUT (INCIDENTAL)

TYPICAL PAVEMENT REPLACEMENT SECTION #2

NOT TO SCALE



TAXIWAY EDGE STRIPE DETAIL

NOT TO SCALE

PAVEMENT MARKING SHALL BE INCIDENTAL TO OTHER PAY ITEMS.



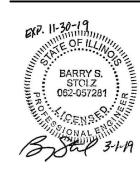
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SHEET TITLE

TYPICAL CONSTRUCTION DETAILS 2

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