

04-26-2024 LETTING ITEM 014

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR LIST OF HIGHWAY STANDARDS, SEE SHEET NO. 2

PROJECT IS LOCATED IN THE CITY OF CHICAGO

**TRAFFIC DATA:**

W FOSTER AVE  
S.N. 016-0249, S.N. 016-0250 2021 ADT = 17,770

DESIGN CLASSIFICATION = MINOR ARTERIAL

DESIGN SPEED = 35 MPH (ASSUMED)

POSTED SPEED = 30 MPH

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**PROPOSED  
HIGHWAY PLANS**

F.A.U. ROUTE 1360 FOSTER AVE OVER  
NORTH BRANCH CHICAGO RIVER  
SECTION: 2021-086-BDR&BJR  
PROJECT NO. STP-LR3B(277)  
S.N. 016-0249 & S.N. 016-0250 -  
BRIDGE DECK OVERLAY AND  
BRIDGE JOINT REPAIR  
COOK COUNTY

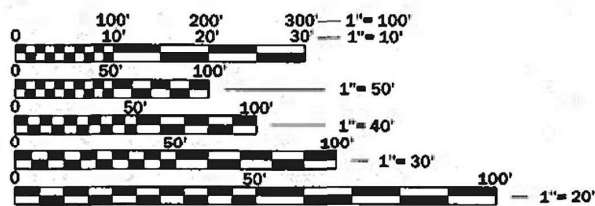
C-91-209-21

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	1
		ILLINOIS	CONTRACT NO. 62P69	

D-91-173-21

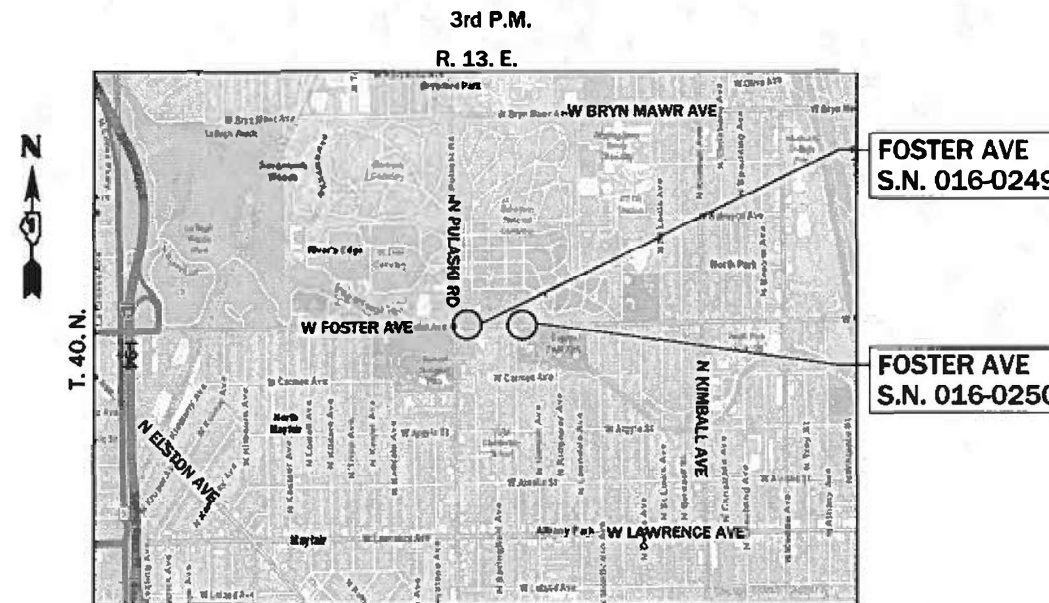


**LI** LIN ENGINEERING, LTD.  
Consulting Engineers  
Westmont, Illinois



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811



**LOCATION MAP**

NOT TO SCALE

GROSS LENGTH = 2,639 FT. = 0.50 MILES  
NET LENGTH = 1,088 FT. = 0.21 MILES



Shiraz Tarique  
Illinois Registered Engineer No. 062-064219  
Registration Expires Nov. 30, 2025

1/24/2024

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED Jan 29 2024  
Jose Rios IR  
REGIONAL ENGINEER

March 22, 2024 [Signature]  
ENGINEER OF DESIGN AND ENVIRONMENT

March 22, 2024 [Signature]  
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

**PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS**

PROJECT ENGINEER: PRAVEEN KAINI, PE. (847-705-4237)  
PROJECT MANAGER: J. ALAIN MIDY, PE. (847-221-3056)

CONTRACT NO. 62P69

**INDEX OF SHEETS**

1	COVER SHEET
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**HIGHWAY STANDARDS**

000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001006	DECIMAL OF AN INCH AND OF A FOOT
664001-02	CHAIN LINK FENCE
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 M) TO 24" (600 MM) FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≤ 40 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER, OR CROSSWALK CLOSURE
701901-09	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
782006-01	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

**DISTRICT STANDARDS**

BD-08	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
TC-14	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
TC-16	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS
TC-21	DETOUR SIGNING FOR CLOSING STATE HIGHWAYS
TC-22	ARTERIAL ROAD INFORMATION SIGNING
TC-24	CITY OF CHICAGO TYPICAL PAVEMENT MARKINGS
TC-26	DRIVEWAY ENTRANCE SIGNING

**GENERAL NOTES**

THE THICKNESS OF HMA SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA IS PLACED.

SODDING WILL NOT BE PERMITTED AT ANY TIME WHEN THEN GROUND IS FROZEN, WET, OR IN AN UNTILLABLE CONDITION. LOCATIONS TO BE SODDED WILL BE DETERMINED BY THE ENGINEER.

ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER SHOWN IN THE LIST OF STANDARDS OR THE COPY INCLUDED IN THESE PLANS.

THE CONTRACTOR SHALL CALL "J.U.L.I.E" AT (800) 892-0123 OR 811 AT LEAST 48 HOURS PRIOR TO EXCAVATION TO DETERMINE WHICH BURIED ELECTRIC, TELEPHONE, AND GAS UTILITIES ARE IN THE AREA. 48 HOUR NOTIFICATION IS REQUIRED.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS THAT CONFLICT WITH TEMPORARY MARKINGS, IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR PROPOSED STRIPING AT THE COMPLETION OF THIS CONTRACT. EXACT LOCATIONS OF ALL PROPOSED PAVEMENT MARKINGS SHALL BE DIRECTED BY THE RESIDENT ENGINEER.

THE CONTRACTOR WILL NOT BE ABLE TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING MATERIALS.

ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH AFFECTED UTILITY COMPANIES AND THE CITY OF CHICAGO.

THE CONTRACTOR SHALL MAINTAIN ALL ROADWAYS OPEN TO TRAFFIC AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS.

THE CONTRACTOR SHALL USE CARE NEAR ANY AND ALL EXISTING ITEMS THAT WILL NOT BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER AT THE CONTRACTOR'S OWN EXPENSE.

TWO WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS, THE ENGINEER SHALL CONTACT EMAD ALHUSSEINI THE AREA TRAFFIC FIELD ENGINEER, AT EMAD.ALHUSSEINI@ILLINOIS.GOV.

THE SUBGRADE STABILITY SHALL BE VERIFIED BY PROOF ROLLING WITH A FULL LOADED TANDEM AXLE TRUCK.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

DURING CONSTRUCTION OPERATIONS, IF ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, THE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKDAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DUST AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

PERMANENT PAVEMENT MARKINGS SHALL BE AS SPECIFIED IN THE PLANS AND SHALL BE PLACED IN ACCORDANCE WITH THE "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAILS. (TC-13, DISTRICT ONE TYPICAL PAVEMENT MARKINGS).

THE CONTRACTOR SHALL CONTACT KALPANA KANNAN-HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR, AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

ANY DAMAGE TO EXISTING TRAFFIC SIGNAL EQUIPMENT WILL HAVE TO BE RESTORED TO ORIGINAL CONDITIONS AND TO THE SATISFACTION OF THE ENGINEER AT NO COST TO THE DEPARTMENT.

**COMMITMENTS**

NONE.

HOT- MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AIR VOIDS @ NDES	QUALITY MANAGEMENT PROGRAM (QMP)
BUTT JOINT		
POLYMERIZED HMA SURFACE COURSE, IL-9.5, MIX "E" N70	4% @ 70 GYR.	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP)		

**MIXTURE TABLE NOTES**

- THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQYD/IN.
- THE AC TYPE FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE AC TYPE SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN CALCULATING PLAN QUANTITIES:

GRANULAR MATERIAL	2.05 TON/CU YD
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USER NAME = 14nho	DESIGNED - NH	REVISED -
	DRAWN - NH	REVISED -
PLOT SCALE = 2.0000' / in.	CHECKED - ST	REVISED -
PLOT DATE = 2/16/2024	DATE - 2/2024	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**F.A.U. 1360 (FOSTER AVE) OVER CHICAGO RIVER  
GENERAL NOTES**

SCALE: N.T.S. SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	2
			CONTRACT NO. 62P69	
		ILLINOIS	FED. AID PROJECT	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY <b>URBAN</b>	CONSTRUCTION CODE	
				80% FED/20% STATE	80% FED/20% STATE
				S.N. 016-0249	S.N. 016-0250
				0059	0059
				BRIDGE	BRIDGE
* 28000510	INLET FILTERS	EACH	1		1
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	633	343	290
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	938	509	429
40604172	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N70	TON	132	71	61
50102400	CONCRETE REMOVAL	CU YD	52.9	28.0	24.9
50300255	CONCRETE SUPERSTRUCTURE	CU YD	55.7	29.4	26.3
50300300	PROTECTIVE COAT	SQ YD	1,062	531	531
50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	3,706	1,580	2,126
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	8,280	4,170	4,110
50800515	BAR SPLICERS	EACH	112	56	56
52000110	PREFORMED JOINT STRIP SEAL	FOOT	330	166	164
58700300	CONCRETE SEALER	SQ FT	9,165	4,661	4,504
60250200	CATCH BASINS TO BE ADJUSTED	EACH	1		1
66400305	CHAIN LINK FENCE, 6'	FOOT	128		128

\* SPECIALTY ITEM



USER NAME = 14nho	DESIGNED - NH	REVISED -
	DRAWN - NH	REVISED -
PLOT SCALE = 2,0000' / in.	CHECKED - ST	REVISED -
PLOT DATE = 2/1/2024	DATE - 2/2024	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**F.A.U. 1360 (FOSTER AVE) OVER CHICAGO RIVER  
SUMMARY OF QUANTITIES**

SCALE: N.T.S. SHEET 1 OF 5 SHEETS STA. TO STA.

F.A.U. RTE. 1360	SECTION 2021-086-BDR&BJR	COUNTY COOK	TOTAL SHEETS 60	SHEET NO. 3
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P69	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY URBAN	CONSTRUCTION CODE	CONSTRUCTION CODE
				80% FED/20% STATE	80% FED/20% STATE
				S.N. 016-0249	S.N. 016-0250
				0059	0059
				BRIDGE	BRIDGE
66400505	CHAIN LINK FENCE, 8'	FOOT	133	133	
66402900	CHAIN LINK GATES, 6' X 6' SINGLE	EACH	2		2
66404900	CHAIN LINK GATES, 8' X 6' SINGLE	EACH	2	2	
67100100	MOBILIZATION	L SUM	1	0.5	0.5
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	150	75	75
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	7,186	3,593	3,593
70307100	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE	SQ FT	529	529	
70307120	TEMPORARY PAVEMENT MARKING-LINE 4" - TYPE IV TAPE	FOOT	15,048	8,595	6,453
70307130	TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE	FOOT	1,149	1,149	
70307140	TEMPORARY PAVEMENT MARKING - LINE 8" - TYPE IV TAPE	FOOT	417	417	
70307160	TEMPORARY PAVEMENT MARKING - LINE 12" - TYPE IV TAPE	FOOT	162	162	
70307210	TEMPORARY PAVEMENT MARKING - LINE 24" - TYPE IV TAPE	FOOT	313	249	64
70400100	TEMPORARY CONCRETE BARRIER	FOOT	1,012.5	562.5	450.0

\* SPECIALTY ITEM



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	DRAWN - NH	REVISED -
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PLOT DATE = 2/1/2024	DATE - 2/2024	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**F.A.U. 1360 (FOSTER AVE) OVER CHICAGO RIVER  
SUMMARY OF QUANTITIES**

SCALE: N.T.S. SHEET 2 OF 5 SHEETS STA. TO STA.

F.A.U. RTE. 1360	SECTION 2021-086-BDR&BJR	COUNTY COOK	TOTAL SHEETS 60	SHEET NO. 4
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P69	



CODE NO.	ITEM	UNIT	TOTAL QUANTITY URBAN	CONSTRUCTION CODE	CONSTRUCTION CODE
				80% FED/20% STATE	80% FED/20% STATE
				S.N. 016-0249	S.N. 016-0250
				0059	0059
				BRIDGE	BRIDGE
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	1,162.5	625.0	537.5
70600255	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	8	4	4
70600322	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	8	4	4
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	146	146	
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	4,712	2,944	1,768
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	504	504	
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1,286	1,040	246
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	155	113	42
* 78004635	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - STANDARD - LINE 7"	FOOT	104	52	52
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	411	204	207
* 78011040	GROOVING FOR RECESSED PAVEMENT MARKING 8"	FOOT	104	52	52
78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	2,083	1,329	754
* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2	1	1

\* SPECIALTY ITEM



USER NAME = 14nh	DESIGNED - NH	REVISED -
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PLOT DATE = 2/1/2024	DATE - 2/2024	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**F.A.U. 1360 (FOSTER AVE) OVER CHICAGO RIVER  
SUMMARY OF QUANTITIES**

SCALE: N.T.S. SHEET 3 OF 5 SHEETS STA. TO STA.

F.A.U. RTE. 1360	SECTION 2021-086-BDR&BJR	COUNTY COOK	TOTAL SHEETS 60	SHEET NO. 5
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P69	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY URBAN	CONSTRUCTION CODE	CONSTRUCTION CODE
				80% FED/20% STATE	80% FED/20% STATE
				S.N. 016-0249	S.N. 016-0250
				0059	0059
				BRIDGE	BRIDGE
* 85000400	MAINTENANCE OF TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	2	1	1
* 89000100	TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	2	1	1
K1004595	PRUNING FOR SAFETY AND EQUIPMENT CLEARANCE	LSUM	1	0.5	0.5
X0322102	TEMPORARY SIDEWALK RAMP	EACH	1	1	
X0327638	STREAM GAUGE	EACH	2	1	1
X5030250	BRIDGE DECK GROOVING (LONGITUDINAL)	SQ YD	896	448	448
X6700407	ENGINEERS' FIELD OFFICE, TYPE A (D1)	CAL MO	12	6	6
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	0.5	0.5
X7830050	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL	EACH	22	22	
* X7830052	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REPLACEMENT	EACH	22	22	
* X8891005	VIDEO VEHICLE DETECTION SYSTEM - TEMPORARY	EACH	2	1	1
Z0001800	APPROACH SLAB REPAIR (PARTIAL DEPTH)	SQ YD	7	3	4
Z0001903	STRUCTURAL STEEL REMOVAL	POUND	4,014	1,808	2,206
Z0006016	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 3/4 INCHES	SQ YD	946	473	473

\* SPECIALTY ITEM



USER NAME = 14nho	DESIGNED - NH	REVISED -
	DRAWN - NH	REVISED -
PLOT SCALE = 2,000' / in.	CHECKED - ST	REVISED -
PLOT DATE = 2/1/2024	DATE - 2/2024	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**F.A.U. 1360 (FOSTER AVE) OVER CHICAGO RIVER  
SUMMARY OF QUANTITIES**

SCALE: N.T.S. SHEET 4 OF 5 SHEETS STA. TO STA.

F.A.U. RTE. 1360	SECTION 2021-086-BDR&BJR	COUNTY COOK	TOTAL SHEETS 60	SHEET NO. 6
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P69	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY URBAN	CONSTRUCTION CODE	CONSTRUCTION CODE
				80% FED/20% STATE	80% FED/20% STATE
				S.N. 016-0249	S.N. 016-0250
				0059	0059
				BRIDGE	BRIDGE
Z0007400	BRIDGE SIDEWALK REPAIR (PARTIAL DEPTH)	SQ FT	121	116	5
Z0012130	BRIDGE DECK SCARIFICATION, 3/4"	SQ YD	946	473	473
Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	153	54	99
Z0015550	DEBRIS REMOVAL	CU YD	30.0	15.0	15.0
Z0016001	DECK SLAB REPAIR (FULL DEPTH, TYPE I)	SQ YD	6	3	3
Z0029090	DIAMOND GRINDING (BRIDGE SECTION)	SQ YD	904	452	452
* Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	486	332	154
Z0073200	TEMPORARY SHORING AND CRIBBING	EACH	3		3
* X8500112	MAINTENANCE OF EXISTING TRAFFIC SIGNAL AND FIRE ALARM	EACH	2	1	1
X0323599	LOCKS FOR GATES	EACH	4	2	2
Ø Z0076600	TRAINEES	HOURS	500	500	
Ø Z0076604	TRAINING PROGRAM GRADUATE	HOURS	500	500	

\* SPECIALTY ITEM



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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.U. 1360 (FOSTER AVE) OVER CHICAGO RIVER  
SUMMARY OF QUANTITIES

SCALE: N.T.S. SHEET 5 OF 5 SHEETS STA. TO STA.

F.A.U. RTE. 1360	SECTION 2021-086-BDR&BJR	COUNTY COOK	TOTAL SHEETS 60	SHEET NO. 7
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P69	

Ø 0042

REV-SEP

**MAINTENANCE OR TRAFFIC GENERAL NOTES**

**SUGGESTED SEQUENCE OF OPERATIONS**

1. THE MAINTENANCE OF TRAFFIC PLANS SHALL SERVE AS A GUIDE FOR THE SAFE DIVERSION OF TRAFFIC DURING THE EXECUTION OF THIS CONTRACT. THE CONTRACTOR MAY MODIFY THE MAINTENANCE OF TRAFFIC PLANS TO MEET CONSTRUCTION NEEDS BUT NOT AT THE EXPENSE OF PUBLIC SAFETY OR CONVENIENCE. ANY CHANGES TO THE TRAFFIC CONTROL PLANS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.
2. EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH TEMPORARY MARKINGS SHALL BE REMOVED. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR PAVEMENT MARKING REMOVAL - WATER BLASTING.
3. ALL EXISTING PAVEMENT MARKING LINES ALONG FOSTER AVE THAT ARE REMOVED AS A RESULT OF A CONFLICT WITH THE REVISED TRAFFIC PATTERNS, OUTSIDE OF THE PAVEMENT MARKING LIMITS SHOWN IN THE PLANS, SHALL BE RE-ESTABLISHED FOR PROPOSED STRIPING AT THE COMPLETION OF THIS CONTRACT. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF PAVEMENT MARKING REMOVAL - WATER BLASTING. THE EXACT LOCATIONS OF ALL PROPOSED PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE RESIDENT ENGINEER.
4. ALL TRAFFIC CONTROL DEVICES SHALL BE REFLECTORIZED PRIOR TO INSTALLATION AND CLEANED AS SPECIFIED IN THE TRAFFIC CONTROL SPECIAL PROVISIONS OR AS DIRECTED BY THE ENGINEER.
5. FOR STABILIZATION, ANY REQUIRED TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR SAND BAGS PER BARRICADE.
6. EXISTING SIGNS WITHIN THE LIMITS OF TRAFFIC CONTROL WHICH ARE OBSTRUCTED BY OR OTHERWISE INTERFERED WITH BY CONSTRUCTION OPERATIONS OF DESIGNATED TRAFFIC CONTROL, SHALL BE COVERED OR REMOVED BY THE CONTRACTOR UNLESS SPECIFIED IN THE PLANS OR WHEN DIRECTED BY THE ENGINEER. THIS WORK SHALL BE AS SPECIFIED IN ARTICLE 107.25 OF THE STANDARD SPECIFICATIONS.
7. CHANGEABLE MESSAGE SHALL BE INSTALLED 2 WEEKS PRIOR TO ALL TRAFFIC STAGE CHANGES ON EACH APPROACH TO THE EFFECTED ROADWAY TO WARN MOTORISTS OF THE UPCOMING EVENT. THE SIGN MESSAGES SHALL BE REVISED TWO WEEKS THEREAFTER WITH MESSAGES WARNING TRAFFIC OF POTENTIAL TRAFFIC DELAYS, QUEUING, AND/OR WITH MESSAGES NOTIFYING TRAFFIC TO USE ALTERNATE ROUTES. THE SIGN LOCATIONS AND MESSAGES SHALL BE DETERMINED BY THE ENGINEER.
8. SEE STRUCTURAL PLANS FOR BRIDGE DECK OVERLAY AND JOINT REPAIR INFORMATION.
9. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE ARTERIAL TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING ANY WORK.
10. THE ENGINEER SHALL COORDINATE CLOSURES WITH CTA AT LEAST TWO WEEKS PRIOR TO BEGINNING OF CONSTRUCTION.
11. THE CONTRACTOR SHALL COORDINATE ANY REQUIRED TEMPORARY TRAFFIC SIGNAL TIMING CHANGES WITH THE CDOT DEPARTMENT OF ELECTRICAL OPERATIONS AT LEAST TWO WEEKS PRIOR TO BEGINNING ANY WORK AND ANY STAGE CHANGES REQUIRED DURING CONSTRUCTION. THIS WORK WILL BE NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE TRAFFIC CONTROL ITEMS INCLUDED IN THIS CONTRACT.

**PRE-STAGE**

1. INSTALL TEMPORARY TRAFFIC SIGNALS AS SHOWN IN THE TRAFFIC SIGNAL PLANS.

**STAGE 1**

1. SET UP EB AND WB FOSTER AVE DETOURS AS SHOWN IN DETOUR PLANS.
2. INSTALL TRAFFIC CONTROL DEVICES AND TEMPORARY PAVEMENT MARKINGS AS SHOWN IN THE STAGE 1 STAGING PLANS.
3. CONVERT THE RIGHT LANE OF EB FOSTER AVE TO RIGHT TURN ONLY LANE AND CLOSE THE LEFT LANE OF WB FOSTER AVE. SHIFT EB FOSTER AVE THROUGH TRAFFIC ONTO THE LEFT LANE OF WB FOSTER AVE AND CLOSE EB FOSTER AVE AS SHOWN IN THE STAGE 1 STAGING PLANS.
4. CONSTRUCT BRIDGE DECK OVERLAY, JOINT REPAIR, AND HMA BUTT JOINT ALONG EB FOSTER AVE AS SHOWN IN THE STAGE 1 STAGING, ROADWAY, AND STRUCTURAL PLANS.
5. ADJUST EXISTING DRAINAGE STRUCTURE ALONG EB FOSTER AVE AS SHOWN ON THE ROADWAY PLAN.
6. REMOVE EB FOSTER AVE TO NB PULASKI RD DETOUR SIGNING. WB FOSTER AVE DETOUR TO REMAIN FOR STAGE 2.

**STAGE 2**

1. INSTALL TRAFFIC CONTROL DEVICES AND TEMPORARY PAVEMENT MARKINGS AS SHOWN IN THE STAGE 2 STAGING PLANS.
2. CLOSE THE LEFT LANE OF EB & WB FOSTER AVE AS SHOWN IN THE STAGE 2 STAGING PLANS.
3. CONSTRUCT BRIDGE DECK OVERLAY, JOINT REPAIR, & HMA BUTT JOINT ALONG EB & WB FOSTER AVE AS SHOWN IN THE STAGE 2 STAGING, ROADWAY, AND STRUCTURAL PLANS.

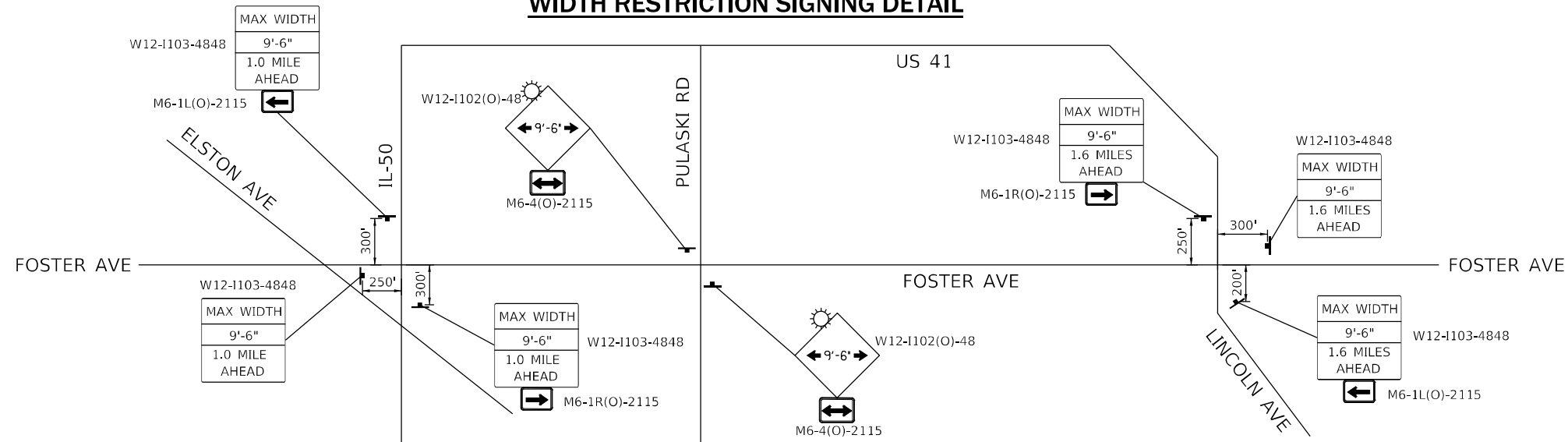
**STAGE 3**

1. SET UP EB FOSTER AVE DETOUR AS SHOWN IN DETOUR PLANS.
2. INSTALL TRAFFIC CONTROL DEVICES AND TEMPORARY PAVEMENT MARKINGS AS SHOWN IN THE STAGE 3 STAGING PLANS.
3. CLOSE THE LEFT LANE OF EB FOSTER ALONG WITH THE RIGHT LANE OF WB FOSTER AVE. SHIFT WB FOSTER AVE THROUGH TRAFFIC ONTO THE LEFT LANE OF EB FOSTER AVE AND CLOSE WB FOSTER AVE AS SHOWN IN THE STAGE 3 STAGING PLANS.
4. CONSTRUCT BRIDGE DECK OVERLAY, JOINT REPAIR, AND HMA BUTT JOINT ALONG WB FOSTER AVE AS SHOWN IN THE STAGE 3 STAGING, ROADWAY, AND STRUCTURAL PLANS.
5. CONSTRUCT SECURITY FENCE AT LOCATIONS SHOWN ON THE ROADWAY PLAN AND FENCING DETAILS.

**POST STAGE**

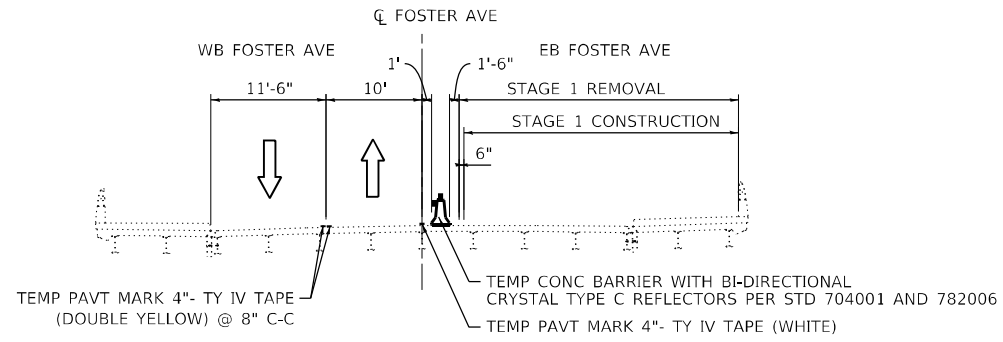
1. UTILIZING APPLICABLE DISTRICT AND HIGHWAY STANDARDS, PLACE PERMANENT PAVEMENT MARKINGS ALONG EB & WB FOSTER AVE AT THE LOCATIONS SHOWN IN THE PLANS.
2. PERFORM TRAFFIC SIGNAL MODERNIZATION AT THE LOCATIONS SHOWN IN THE PLANS.

**WIDTH RESTRICTION SIGNING DETAIL**



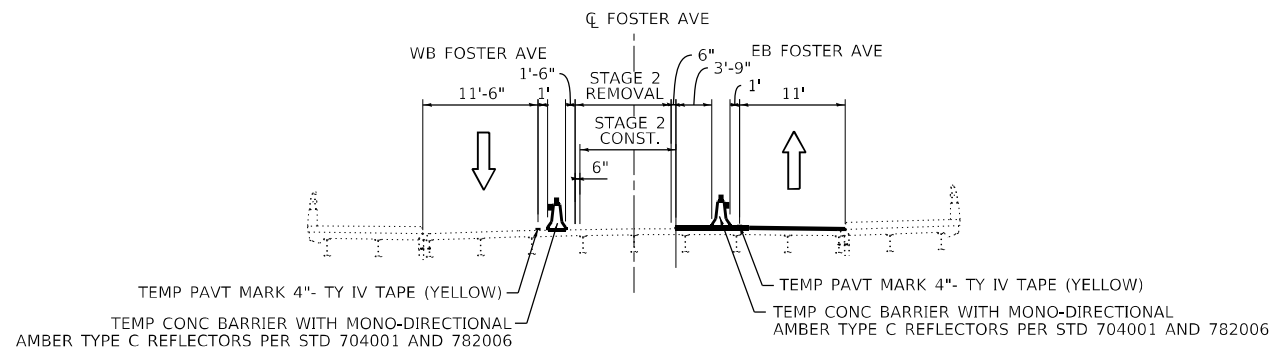
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	8
CONTRACT NO. 62P69			ILLINOIS FED. AID PROJECT	



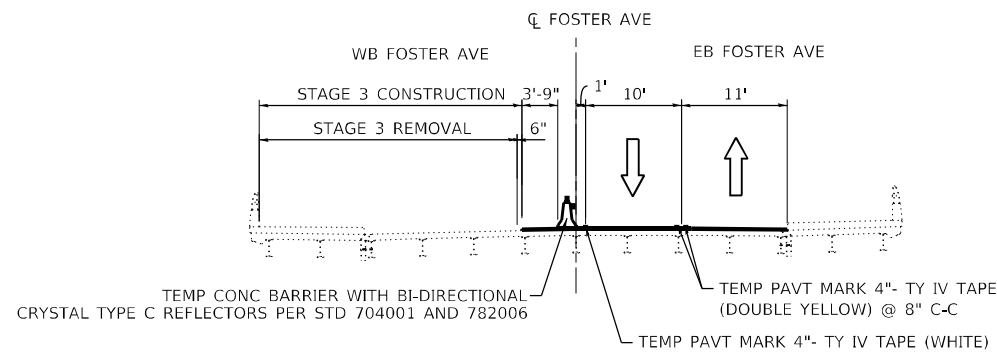
**S.N. 016-0249 STAGE 1 TYPICAL SECTION**

FOSTER AVE OVER N BRANCH CHICAGO RIVER  
LOOKING EAST



**S.N. 016-0249 STAGE 2 TYPICAL SECTION**

FOSTER AVE OVER N BRANCH CHICAGO RIVER  
LOOKING EAST

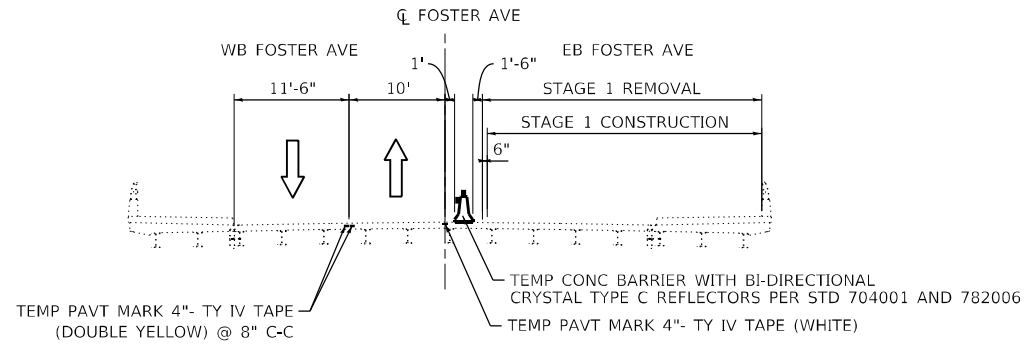


**S.N. 016-0249 STAGE 3 TYPICAL SECTION**

FOSTER AVE OVER N BRANCH CHICAGO RIVER  
LOOKING EAST

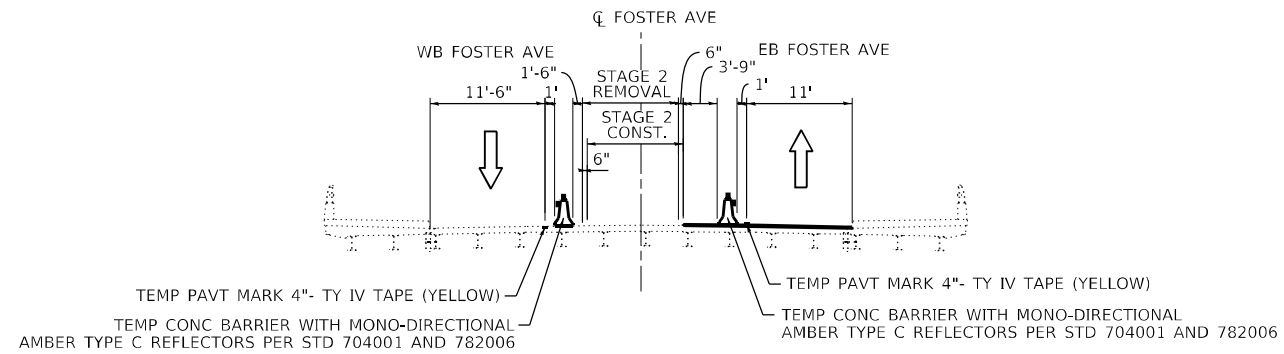
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	9
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P69	



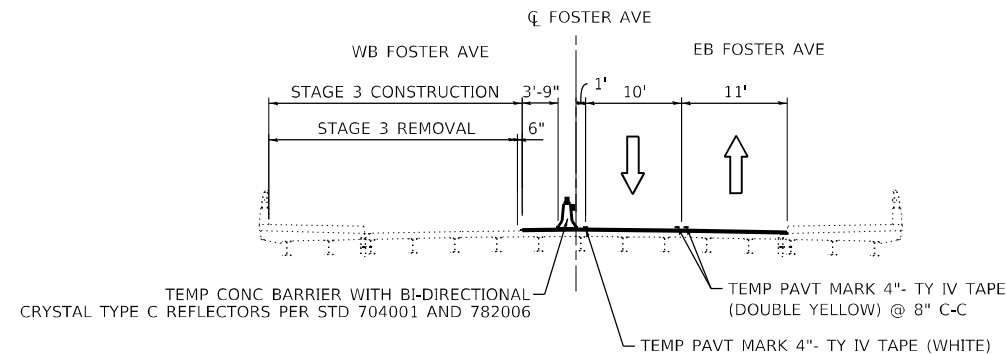
**S.N. 016-0250 STAGE 1 TYPICAL SECTION**

FOSTER AVE OVER N BRANCH CHICAGO RIVER  
LOOKING EAST



**S.N. 016-0250 STAGE 2 TYPICAL SECTION**

FOSTER AVE OVER N BRANCH CHICAGO RIVER  
LOOKING EAST













**S.N. 016-0250 STAGE 3 TYPICAL SECTION**

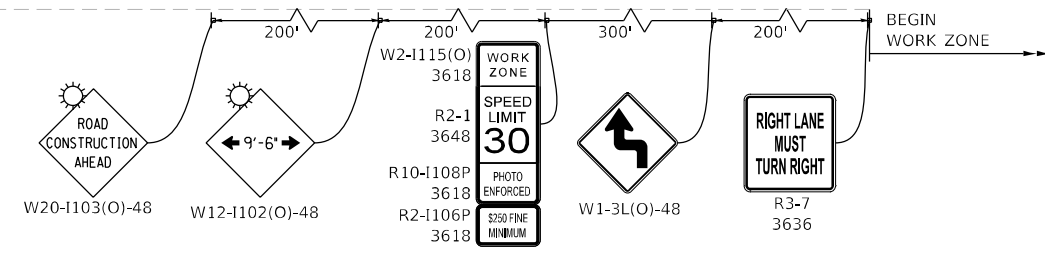
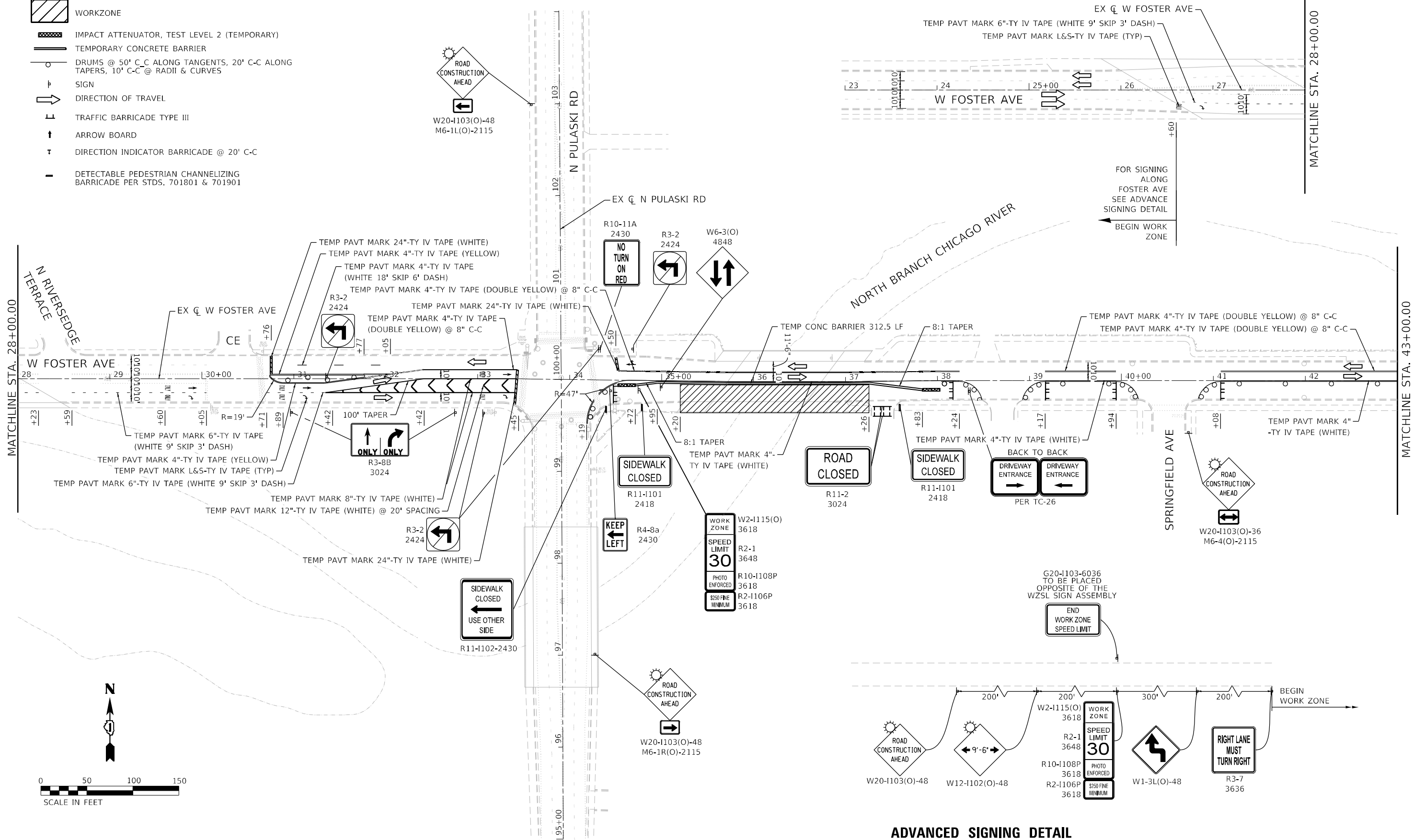
FOSTER AVE OVER N BRANCH CHICAGO RIVER  
LOOKING EAST

USER NAME = 14nho	DESIGNED - NH	REVISED -
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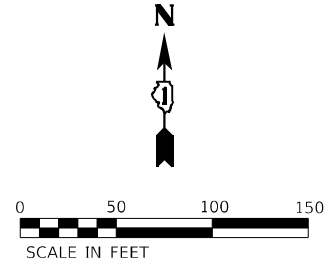
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	10
CONTRACT NO. 62P69			ILLINOIS FED. AID PROJECT	

**LEGEND**

-  WORKZONE
-  IMPACT ATTENUATOR, TEST LEVEL 2 (TEMPORARY)
-  TEMPORARY CONCRETE BARRIER
-  DRUMS @ 50' C-C ALONG TANGENTS, 20' C-C ALONG TAPERS, 10' C-C @ RADII & CURVES
-  SIGN
-  DIRECTION OF TRAVEL
-  TRAFFIC BARRICADE TYPE III
-  ARROW BOARD
-  DIRECTION INDICATOR BARRICADE @ 20' C-C
-  DETECTABLE PEDESTRIAN CHANNELIZING BARRICADE PER STDs. 701801 & 701901



**ADVANCED SIGNING DETAIL**













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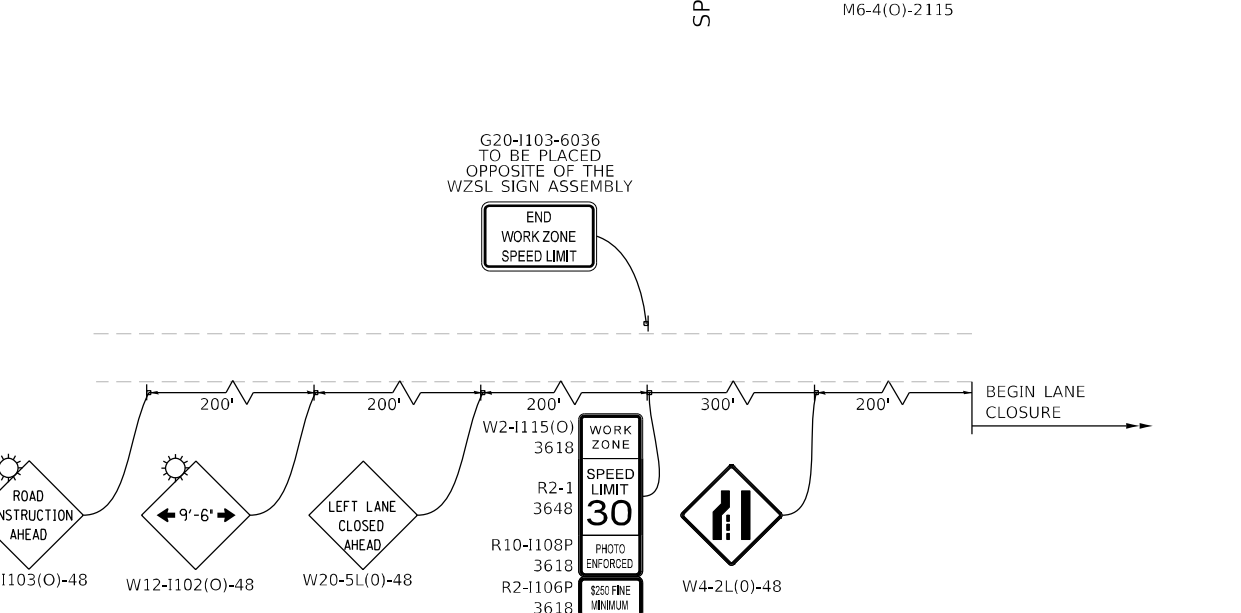
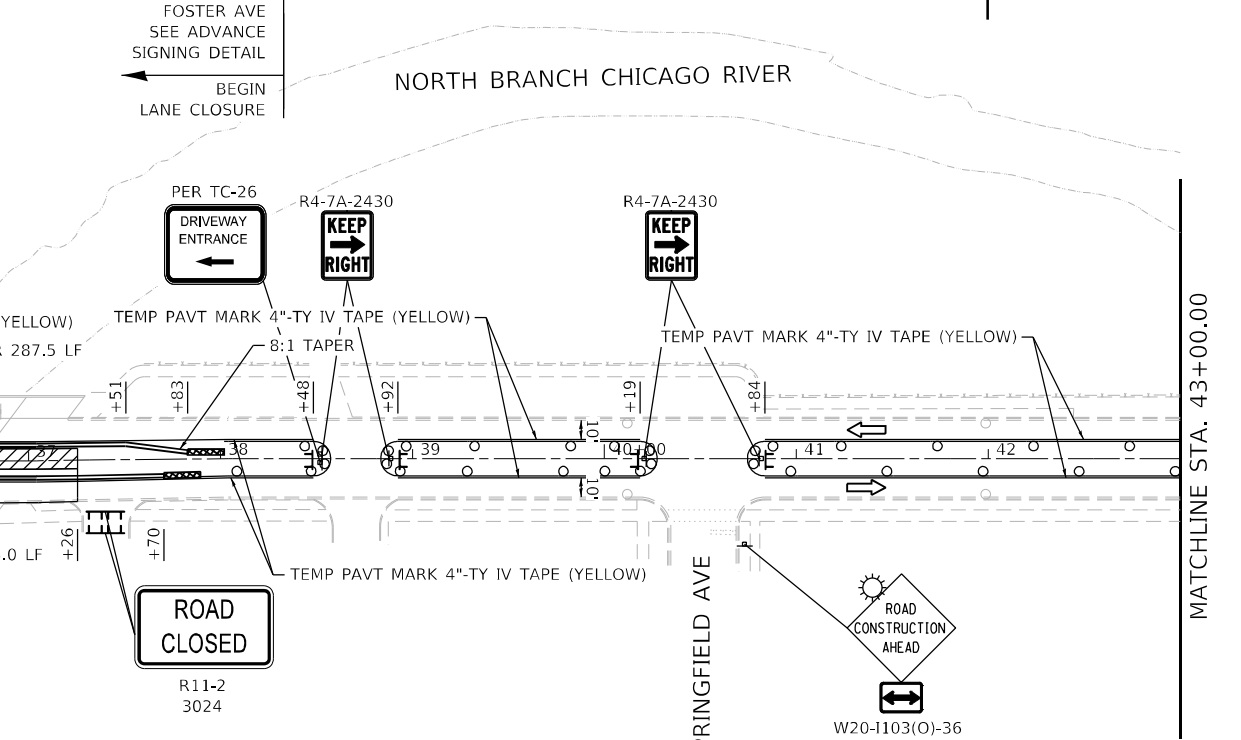
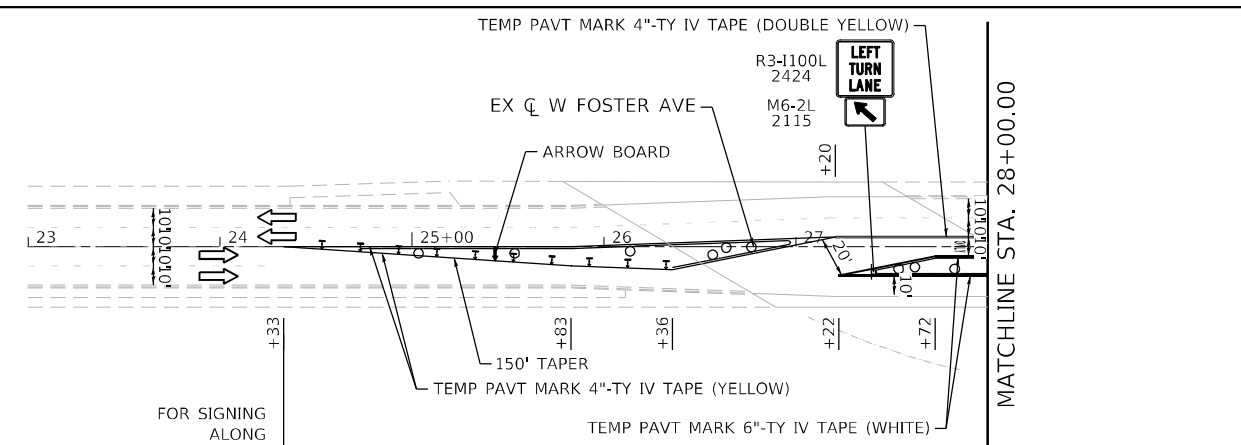
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ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P69	



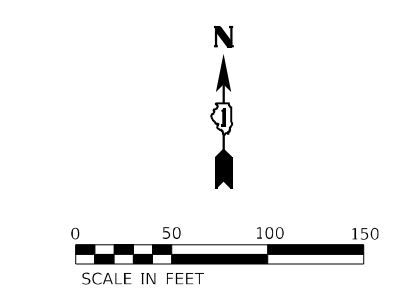
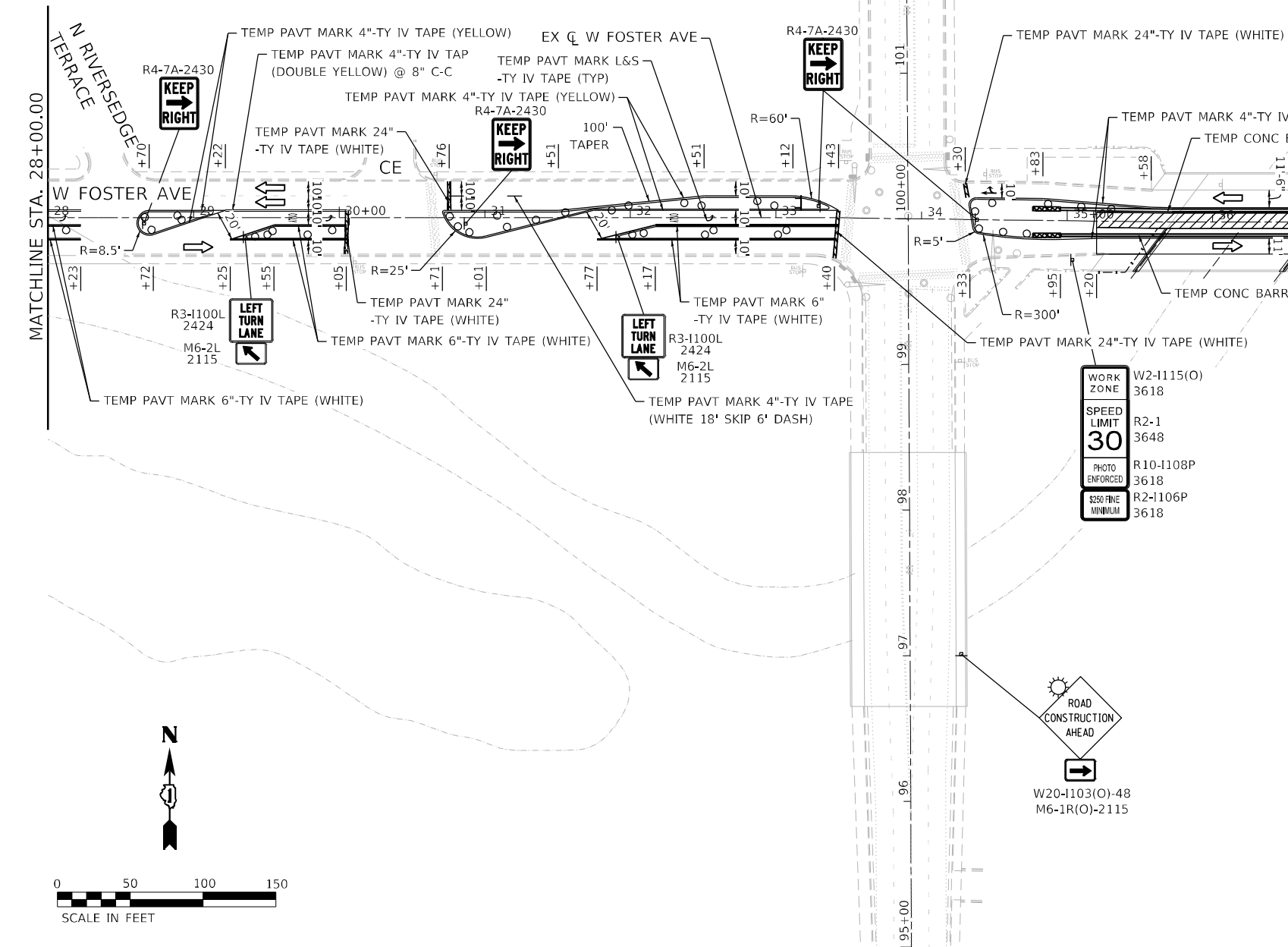


**LEGEND**

-  WORKZONE
-  IMPACT ATTENUATOR, TEST LEVEL 2 (TEMPORARY)
-  TEMPORARY CONCRETE BARRIER
-  DRUMS @ 50' C-C ALONG TANGENTS, 20' C-C ALONG TAPERS, 10' C-C @ RADII & CURVES
-  SIGN
-  DIRECTION OF TRAVEL
-  TRAFFIC BARRICADE TYPE III
-  ARROW BOARD
-  DIRECTION INDICATOR BARRICADE @ 20' C-C
-  DETECTABLE PEDESTRIAN CHANNELIZING BARRICADE PER STDS. 701801 & 701901













**ADVANCED SIGNING DETAIL**

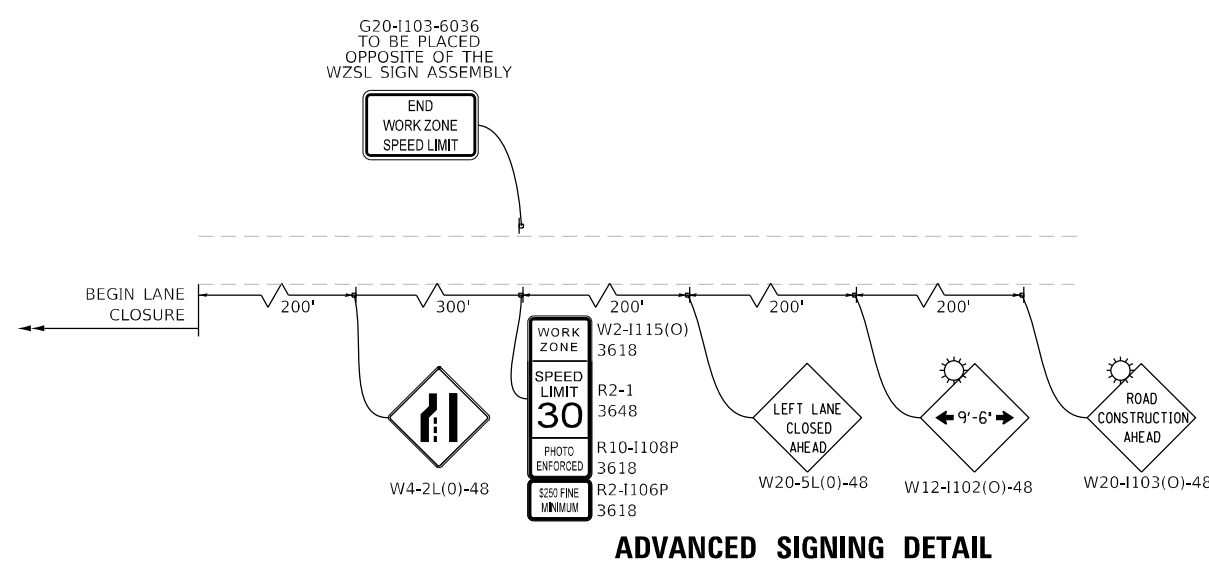
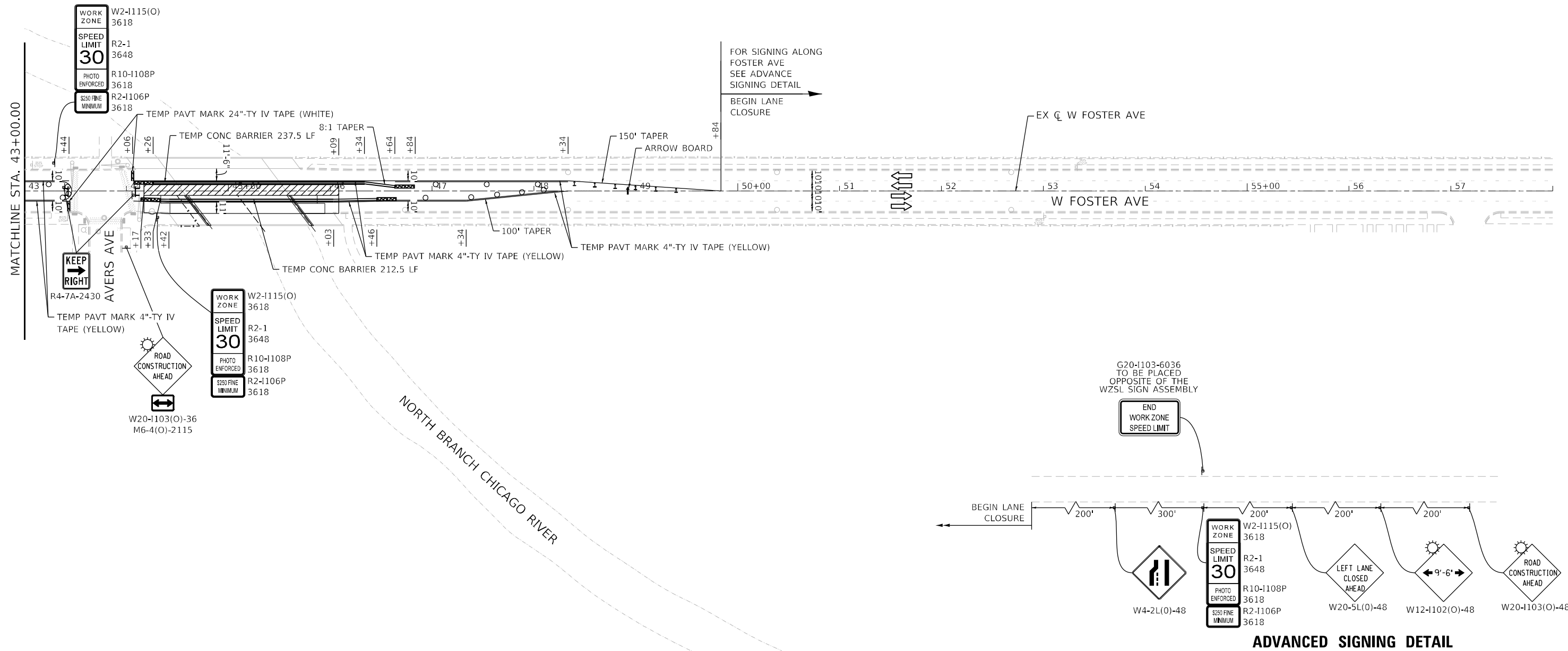
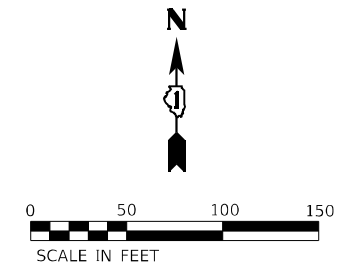


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	DATE - 2/2024	REVISED -

F.A.U. RTE. 1360	SECTION 2021-086-BDR&BJR	COUNTY COOK	TOTAL SHEETS 60	SHEET NO. 13
			CONTRACT NO. 62P69	
ILLINOIS FED. AID PROJECT				

**LEGEND**











-  WORKZONE
-  IMPACT ATTENUATOR, TEST LEVEL 2 (TEMPORARY)
-  TEMPORARY CONCRETE BARRIER
-  DRUMS @ 50' C-C ALONG TANGENTS, 20' C-C ALONG TAPERS, 10' C-C @ RADII & CURVES
-  SIGN
-  DIRECTION OF TRAVEL
-  TRAFFIC BARRICADE TYPE III
-  ARROW BOARD
-  DIRECTION INDICATOR BARRICADE @ 20' C-C
-  DETECTABLE PEDESTRIAN CHANNELIZING BARRICADE PER STDS. 701801 & 701901

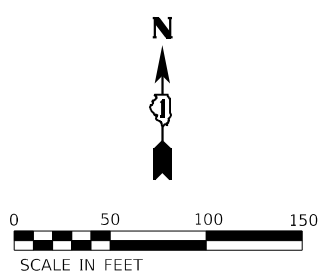
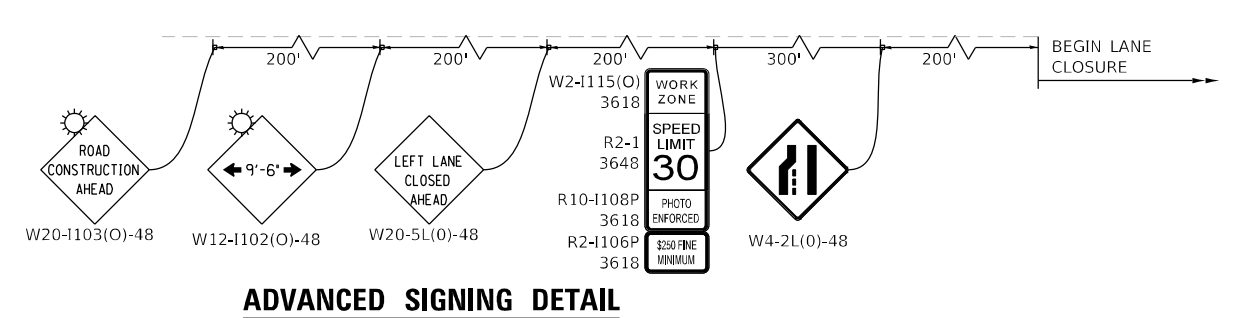
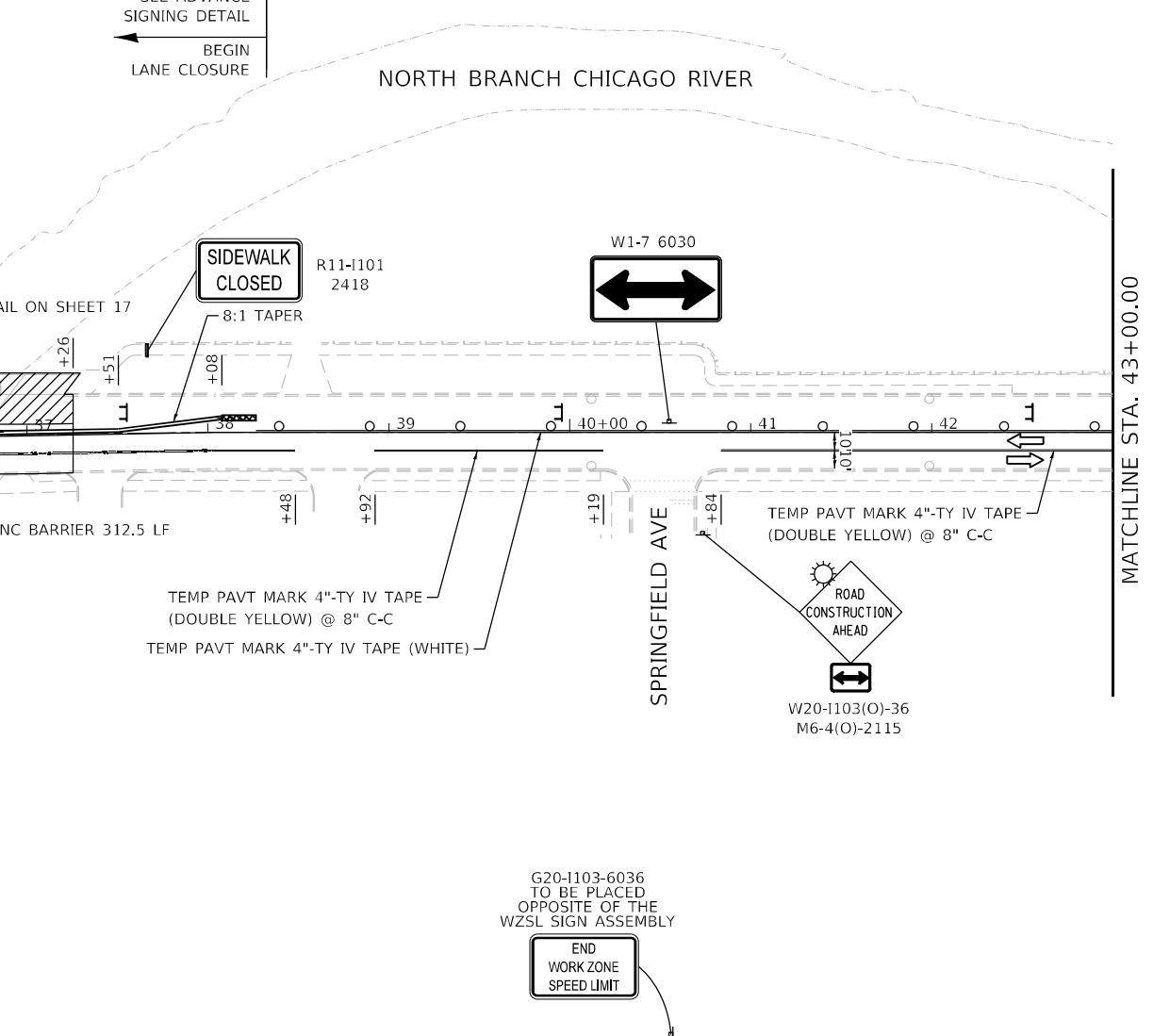
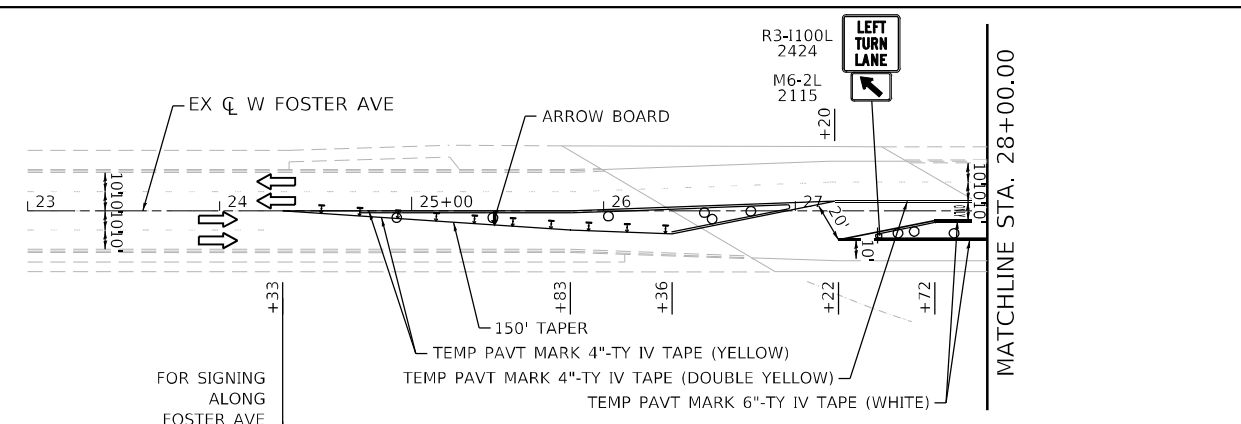


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	DATE - 2/2024	REVISED -

F.A.U. RTE. 1360	SECTION 2021-086-BDR&BIR	COUNTY COOK	TOTAL SHEETS 60	SHEET NO. 14
CONTRACT NO. 62P69			ILLINOIS FED. AID PROJECT	

**LEGEND**

-  WORKZONE
-  IMPACT ATTENUATOR, TEST LEVEL 2 (TEMPORARY)
-  TEMPORARY CONCRETE BARRIER
-  DRUMS @ 50' C-C ALONG TANGENTS, 20' C-C ALONG TAPERS, 10' C-C @ RADII & CURVES
-  SIGN
-  DIRECTION OF TRAVEL
-  TRAFFIC BARRICADE TYPE III
-  ARROW BOARD
-  DIRECTION INDICATOR BARRICADE @ 20' C-C
-  DETECTABLE PEDESTRIAN CHANNELIZING BARRICADE PER STDS. 701801 & 701901



**LE** LIN ENGINEERING, LTD.  
Consulting Engineers  
Westmont, Illinois

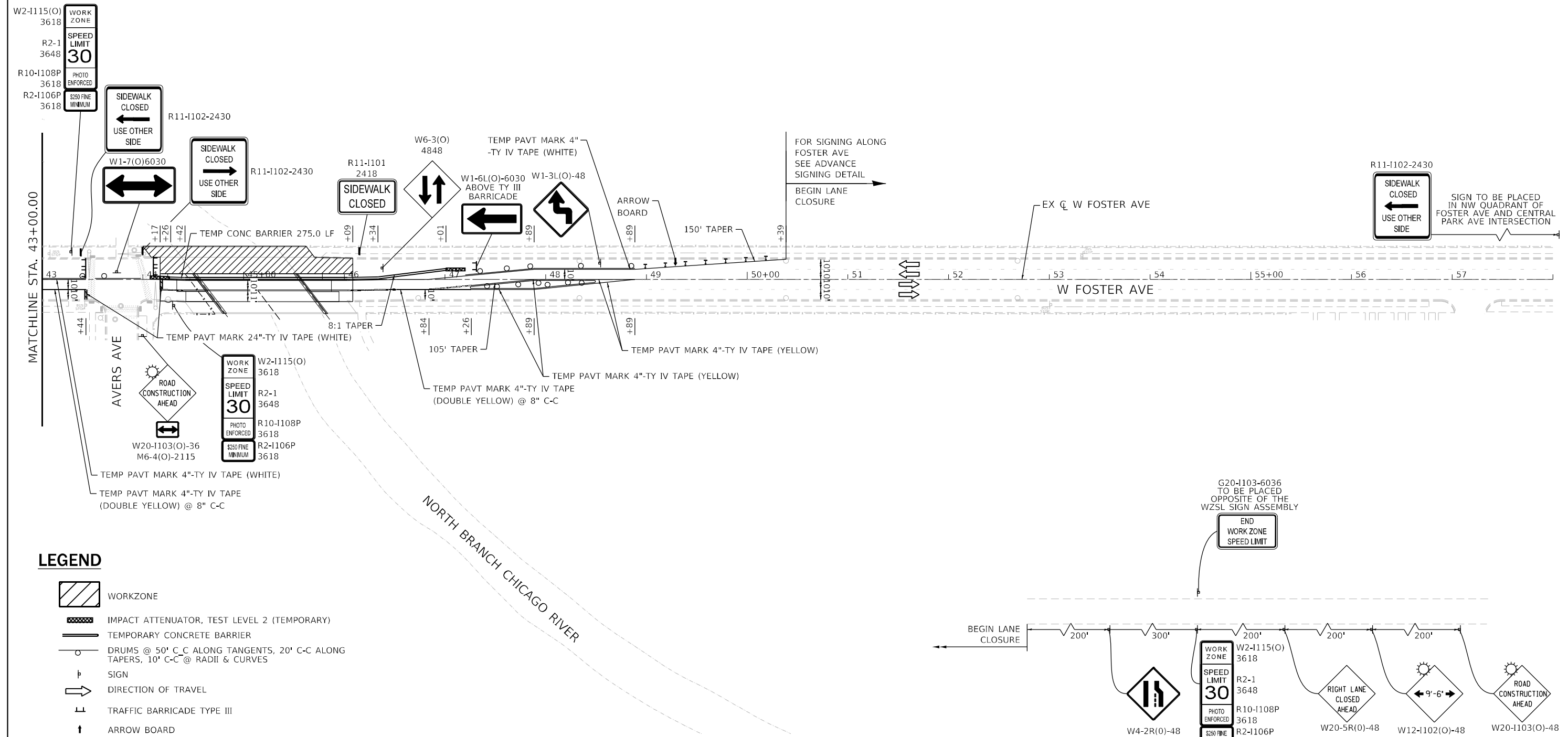
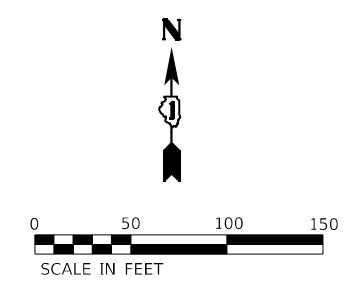
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	DATE - 2/2024	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**F.A.U. 1360 (FOSTER AVE) OVER CHICAGO RIVER**  
**STAGING PLAN - STAGE 3**

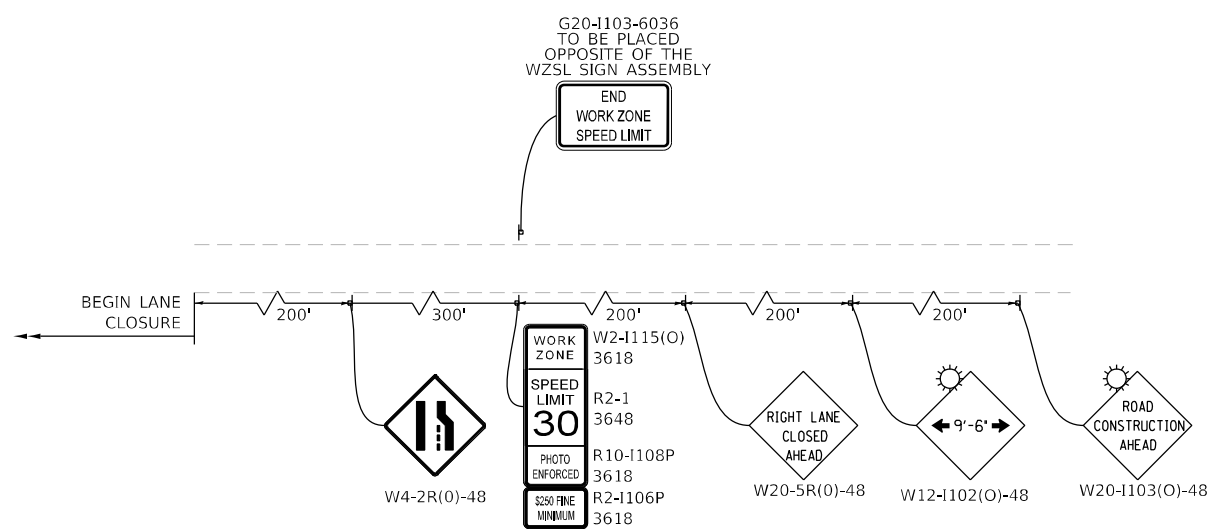
SCALE: 1"=50'    SHEET 8 OF 10 SHEETS    STA. 24+33.00 TO STA. 43+00.00

F.A.U. RTE. 1360	SECTION 2021-086-BDR&B/R	COUNTY COOK	TOTAL SHEETS 60	SHEET NO. 15
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P69	



**LEGEND**

- WORKZONE
- IMPACT ATTENUATOR, TEST LEVEL 2 (TEMPORARY)
- TEMPORARY CONCRETE BARRIER
- DRUMS @ 50' C-C ALONG TANGENTS, 20' C-C ALONG TAPERS, 10' C-C @ RADII & CURVES
- SIGN
- DIRECTION OF TRAVEL
- TRAFFIC BARRICADE TYPE III
- ARROW BOARD
- DIRECTION INDICATOR BARRICADE @ 20' C-C
- DETECTABLE PEDESTRIAN CHANNELIZING BARRICADE PER STDS. 701801 & 701901



**ADVANCED SIGNING DETAIL**



USER NAME = 14nho	DESIGNED - NH	REVISED -
PLOT SCALE = 100,0000' / in.	DRAWN - NH	REVISED -
PLOT DATE = 2/16/2024	CHECKED - ST	REVISED -
	DATE - 2/2024	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**F.A.U. 1360 (FOSTER AVE) OVER CHICAGO RIVER  
STAGING PLAN - STAGE 3**

F.A.U. RTE. 1360	SECTION 2021-086-BDR&BIR	COUNTY COOK	TOTAL SHEETS 60	SHEET NO. 16
			CONTRACT NO. 62P69	
ILLINOIS FED. AID PROJECT				

SCALE: 1"=50' SHEET 9 OF 10 SHEETS STA. 43+00.00 TO STA. 50+39.00

**TEMPORARY SIDEWALK RAMP**

DESCRIPTION

THIS WORK SHALL CONSIST OF INSTALLING AND MAINTAINING TEMPORARY ADA COMPLIANT RAMPS TO MAINTAIN ACCESS TO BUS STOPS DURING CONSTRUCTION AT THE LOCATION SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

CONSTRUCTION REQUIREMENTS

THIS WORK SHALL BE IN ACCORDANCE TO STANDARD 701801-06. TEMPORARY RAMPS TO MAINTAIN ACCESS SHALL BE COMPLIANT WITH THE ADA REQUIREMENTS AND SHALL BE USED AT LOCATIONS TO MAINTAIN A MINIMUM OF ONE ADA COMPLIANT PEDESTRIAN ACCESS PATH AROUND CONSTRUCTION ACTIVITIES AT ALL TIMES.

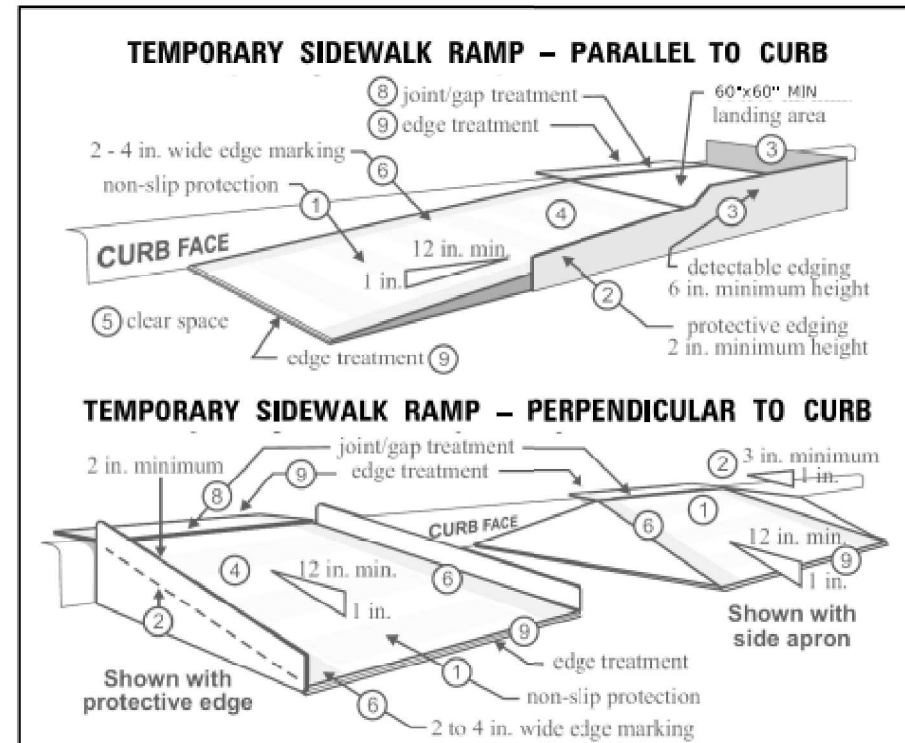
1. TRAFFIC CONTROL DEVICES USED ADJACENT TO PEDESTRIAN ACCESS PATHS FOR CHANNELIZING PEDESTRIANS SHALL BE ADA COMPLIANT WITH A BOTTOM EDGE AT LEAST 6 INCHES IN HEIGHT FROM THE WALKWAY AND A CONTINUOUS RAIL OR SURFACE AT 3 FEET ABOUT THE WALKWAY.
2. CURB RAMPS SHALL BE 8 FEET MINIMUM WIDTH WITH A FIRM, STABLE AND NON-SLIP SURFACE.
3. PROTECTIVE EDGING WITH A 2 IN. MINIMUM HEIGHT SHALL BE INSTALLED WHEN THE CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6 IN. OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3 INCHES OR MORE.
4. DETECTABLE EDGING WITH 6-INCH MINIMUM HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
5. CURB RAMPS AND LANDINGS SHOULD HAVE A 1:50 (2%) MAX CROSS-SLOPE.
6. CLEAR SPACE OF 5 FT. X 5 FT. MINIMUM SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
7. THE CURB RAMP WALKWAY EDGE SHALL BE MARKED WITH A CONTRASTING COLOR 2 TO 4 INCHES WIDE MARKING. THE MARKING IS OPTIONAL WHERE COLOR CONTRASTING EDGING IS USED.
8. WATER FLOW IN THE GUTTER SYSTEM SHALL HAVE MINIMAL RESTRICTION.
9. LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 0.5 INCH WIDTH.
10. CHANGES BETWEEN SURFACE HEIGHTS SHOULD NOT EXCEED 0.5 INCH, LATERAL EDGES SHOULD BE VERTICAL UP TO 0.25-INCH-HIGH AND BEVELED AT 1:2 BETWEEN 0.25 INCH AND 0.5 INCH HEIGHT.

METHOD OF MEASUREMENT

TEMPORARY SIDEWALK RAMP WILL BE MEASURED FOR PAYMENT IN PLACE ON AN EACH BASIS AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REINSTALLATION, ADJUSTMENT, OR MODIFICATION OF A TEMPORARY SIDEWALK RAMP AT THE SAME LOCATION WILL NOT BE MEASURED FOR PAYMENT.

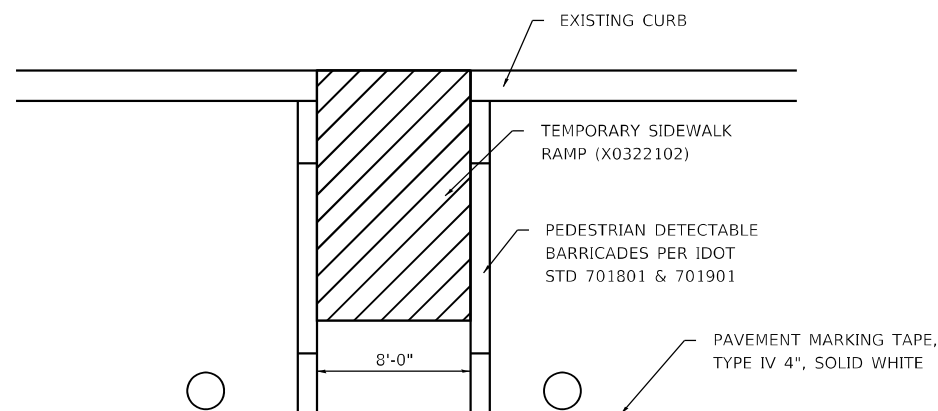
BASIS OF PAYMENT

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "TEMPORARY SIDEWALK RAMP" AND SHALL INCLUDE REMOVAL.



(FIGURE FROM MnDOT TPAR)

**CTA BUS STOP DETAIL**

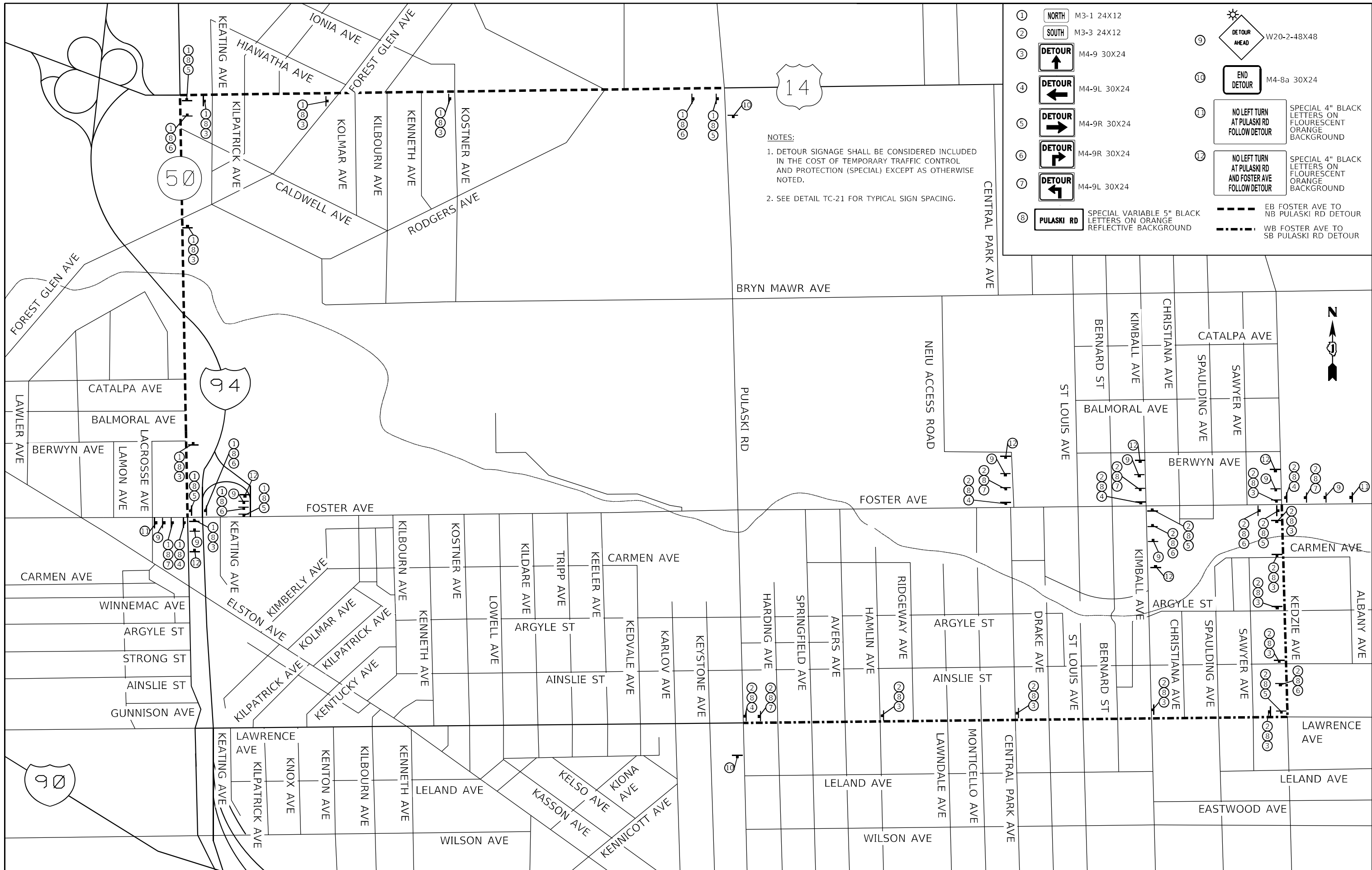


NOTES:

1. SPACE TYPE II BARRICADES TO AVOID BLOCKING THE 8'-0" WIDE PEDESTRIAN ACCESS TO THE BUS STOP.
2. DECREASE SPACING OF THE TYPE II BARRICADES TO 10' SPACING WITHIN 50' OF THE PEDESTRIAN ACCESS TO THE BUS STOP.

USER NAME = 14nho	DESIGNED - NH	REVISED -
	DRAWN - NH	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED - ST	REVISED -
PLOT DATE = 2/3/2024	DATE - 2/2024	REVISED -

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	17
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P69	



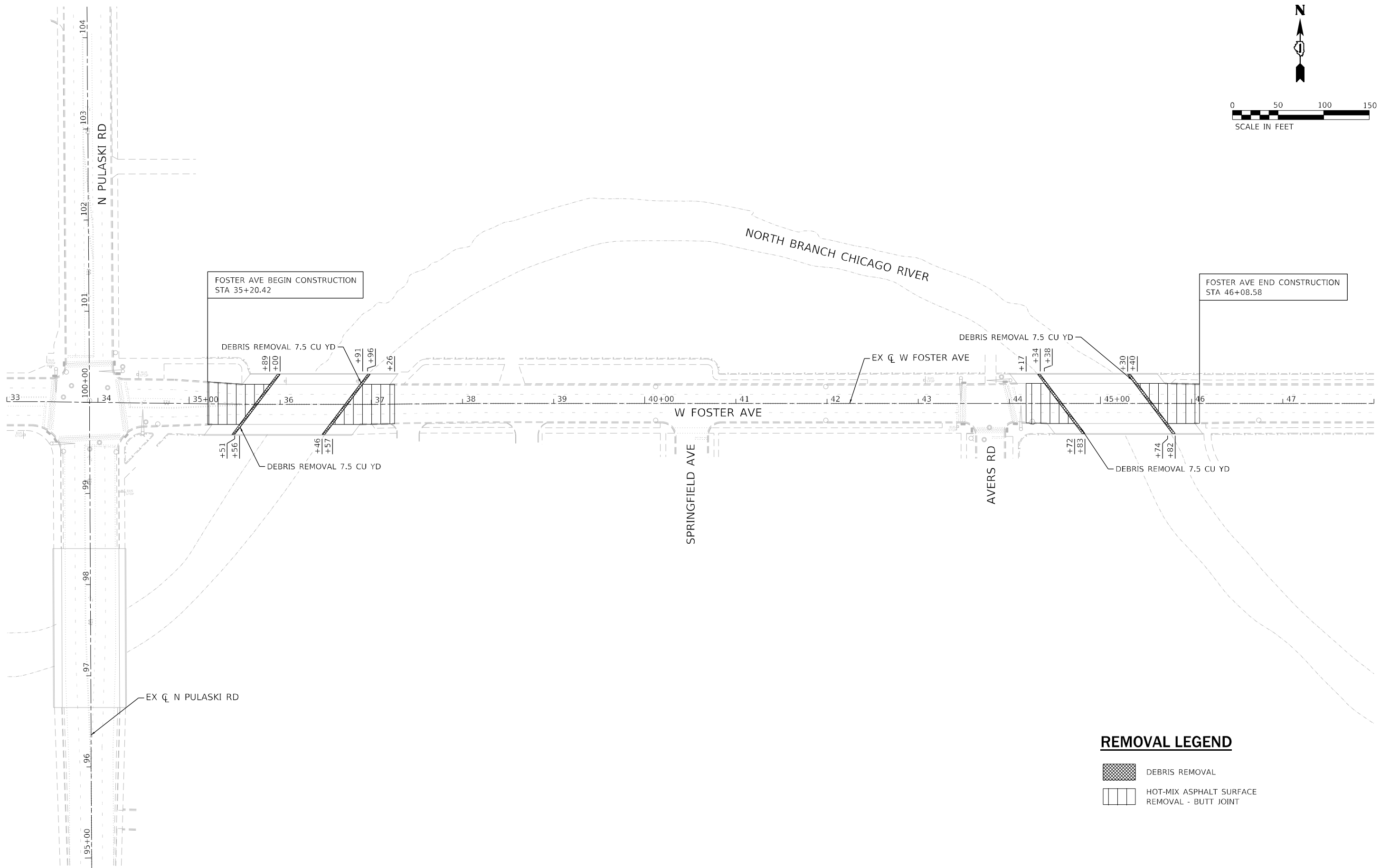
**NOTES:**

1. DETOUR SIGNAGE SHALL BE CONSIDERED INCLUDED IN THE COST OF TEMPORARY TRAFFIC CONTROL AND PROTECTION (SPECIAL) EXCEPT AS OTHERWISE NOTED.
2. SEE DETAIL TC-21 FOR TYPICAL SIGN SPACING.

①		M3-1 24X12	⑨		W20-2-48X48
②		M3-3 24X12	⑩		M4-8a 30X24
③		M4-9 30X24	⑪		SPECIAL 4" BLACK LETTERS ON FLOURESCENT ORANGE BACKGROUND
④		M4-9L 30X24	⑫		SPECIAL 4" BLACK LETTERS ON FLOURESCENT ORANGE BACKGROUND
⑤		M4-9R 30X24			
⑥		M4-9R 30X24			
⑦		M4-9L 30X24			
⑧		SPECIAL VARIABLE 5" BLACK LETTERS ON ORANGE REFLECTIVE BACKGROUND			
					EB FOSTER AVE TO NB PULASKI RD DETOUR
					WB FOSTER AVE TO SB PULASKI RD DETOUR

USER NAME = 14nh	DESIGNED - NH	REVISED -
PLOT SCALE = 2,000' / in.	DRAWN - NH	REVISED -
PLOT DATE = 2/1/2024	CHECKED - ST	REVISED -
	DATE - 2/2024	REVISED -

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&B/R	COOK	60	18
CONTRACT NO. 62P69				
ILLINOIS FED. AID PROJECT				



**REMOVAL LEGEND**

- DEBRIS REMOVAL
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT



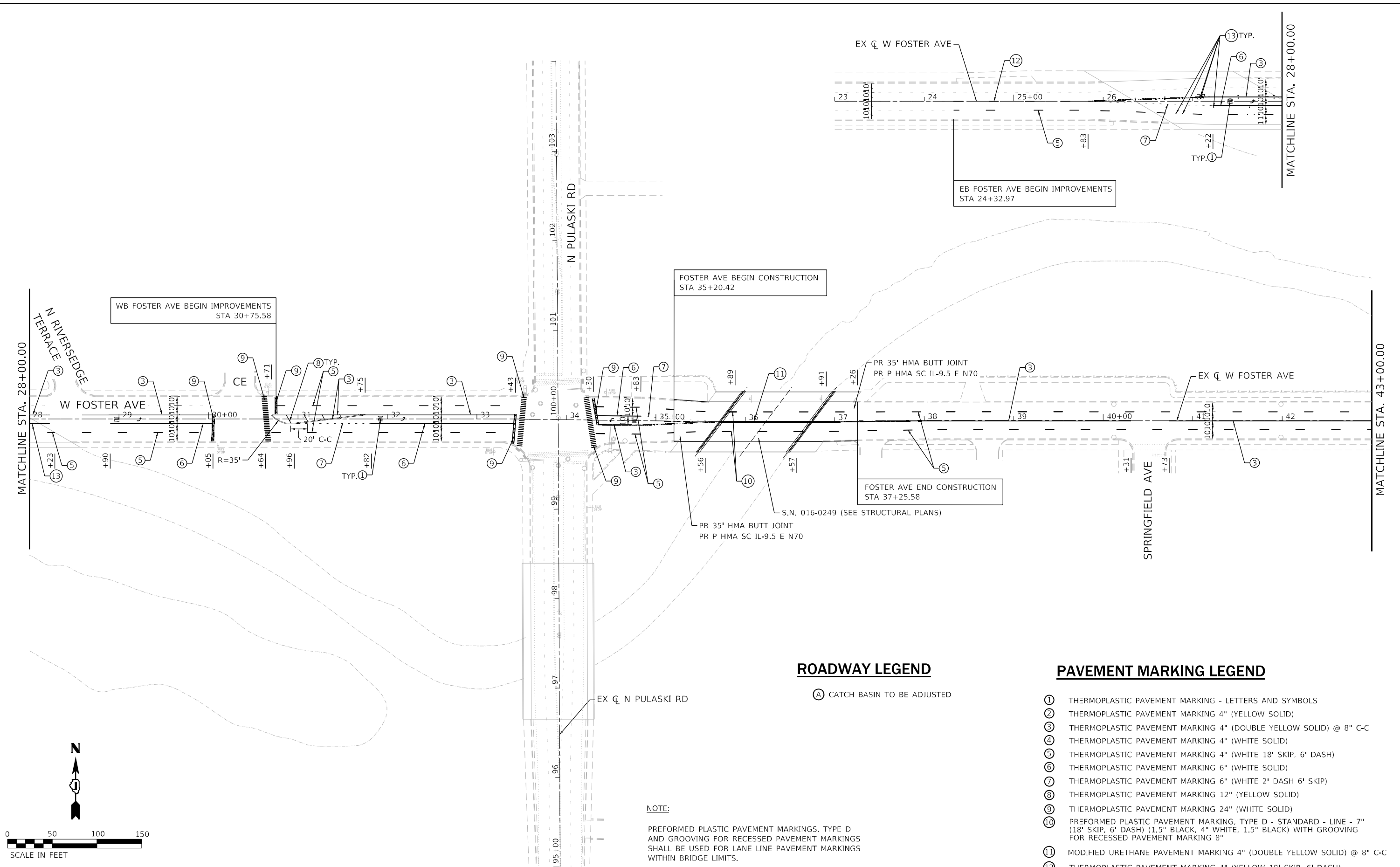
USER NAME = 14nho	DESIGNED - NH	REVISED -
DRAWN - NH	REVISIONS -	
PLOT SCALE = 100,0044' / in.	CHECKED - ST	REVISED -
PLOT DATE = 2/1/2024	DATE - 2/2024	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**F.A.U. 1360 (FOSTER AVE) OVER CHICAGO RIVER  
REMOVAL PLAN**

SCALE: 1"=50' SHEET 1 OF 1 SHEETS STA. 35+20.42 TO STA. 46+08.58

F.A.U. RTE. 1360	SECTION 2021-086-BDR&BJR	COUNTY COOK	TOTAL SHEETS 60	SHEET NO. 19
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P69	



**ROADWAY LEGEND**

(A) CATCH BASIN TO BE ADJUSTED

**PAVEMENT MARKING LEGEND**

- ① THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- ② THERMOPLASTIC PAVEMENT MARKING 4" (YELLOW SOLID)
- ③ THERMOPLASTIC PAVEMENT MARKING 4" (DOUBLE YELLOW SOLID) @ 8" C-C
- ④ THERMOPLASTIC PAVEMENT MARKING 4" (WHITE SOLID)
- ⑤ THERMOPLASTIC PAVEMENT MARKING 4" (WHITE 18' SKIP, 6' DASH)
- ⑥ THERMOPLASTIC PAVEMENT MARKING 6" (WHITE SOLID)
- ⑦ THERMOPLASTIC PAVEMENT MARKING 6" (WHITE 2' DASH 6' SKIP)
- ⑧ THERMOPLASTIC PAVEMENT MARKING 12" (YELLOW SOLID)
- ⑨ THERMOPLASTIC PAVEMENT MARKING 24" (WHITE SOLID)
- ⑩ PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - STANDARD - LINE - 7" (18' SKIP, 6' DASH) (1.5" BLACK, 4" WHITE, 1.5" BLACK) WITH GROOVING FOR RECESSED PAVEMENT MARKING 8"
- ⑪ MODIFIED URETHANE PAVEMENT MARKING 4" (DOUBLE YELLOW SOLID) @ 8" C-C
- ⑫ THERMOPLASTIC PAVEMENT MARKING 4" (YELLOW 18' SKIP, 6' DASH)
- ⑬ RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL AND REPLACEMENT

**NOTE:**

PREFORMED PLASTIC PAVEMENT MARKINGS, TYPE D AND GROOVING FOR RECESSED PAVEMENT MARKINGS SHALL BE USED FOR LANE LINE PAVEMENT MARKINGS WITHIN BRIDGE LIMITS.



USER NAME = 14nho	DESIGNED - NH	REVISED -
DRAWN - NH	REVISED -	
PLOT SCALE = 100,0000' / in.	CHECKED - ST	REVISED -
PLOT DATE = 2/1/2024	DATE - 2/2024	REVISED -

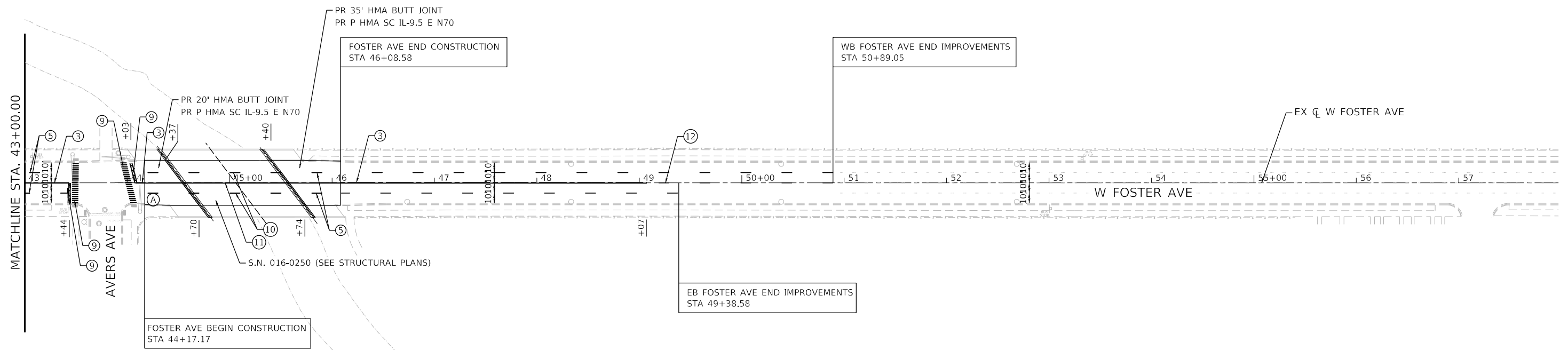
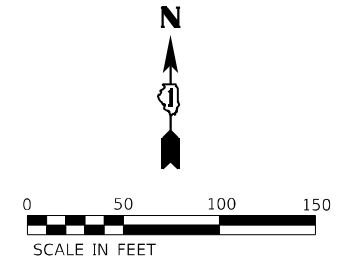
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**F.A.U. 1360 (FOSTER AVE) OVER CHICAGO RIVER  
ROADWAY PLAN**

SCALE: 1"=50' SHEET 1 OF 3 SHEETS STA. 24+32.97 TO STA. 43+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BR	COOK	60	20
CONTRACT NO. 62P69				
ILLINOIS FED. AID PROJECT				





**ROADWAY LEGEND**

(A) CATCH BASIN TO BE ADJUSTED

**PAVEMENT MARKING LEGEND**

- ① THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- ② THERMOPLASTIC PAVEMENT MARKING 4" (YELLOW SOLID)
- ③ THERMOPLASTIC PAVEMENT MARKING 4" (DOUBLE YELLOW SOLID) @ 8" C-C
- ④ THERMOPLASTIC PAVEMENT MARKING 4" (WHITE SOLID)
- ⑤ THERMOPLASTIC PAVEMENT MARKING 4" (WHITE 18' SKIP, 6' DASH)
- ⑥ THERMOPLASTIC PAVEMENT MARKING 6" (WHITE SOLID)
- ⑦ THERMOPLASTIC PAVEMENT MARKING 6" (WHITE 2' DASH 6' SKIP)
- ⑧ THERMOPLASTIC PAVEMENT MARKING 12" (YELLOW SOLID)
- ⑨ THERMOPLASTIC PAVEMENT MARKING 24" (WHITE SOLID)
- ⑩ PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - STANDARD - LINE - 7" (18' SKIP, 6' DASH) (1.5" BLACK, 4" WHITE, 1.5" BLACK) WITH GROOVING FOR RECESSED PAVEMENT MARKING 8"
- ⑪ MODIFIED URETHANE PAVEMENT MARKING 4" (DOUBLE YELLOW SOLID) @ 8" C-C
- ⑫ THERMOPLASTIC PAVEMENT MARKING 4" (YELLOW 18' SKIP, 6' DASH)
- ⑬ RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL AND REPLACEMENT

**NOTE:**  
 PREFORMED PLASTIC PAVEMENT MARKINGS, TYPE D AND GROOVING FOR RECESSED PAVEMENT MARKINGS SHALL BE USED FOR LANE LINE PAVEMENT MARKINGS WITHIN BRIDGE LIMITS.

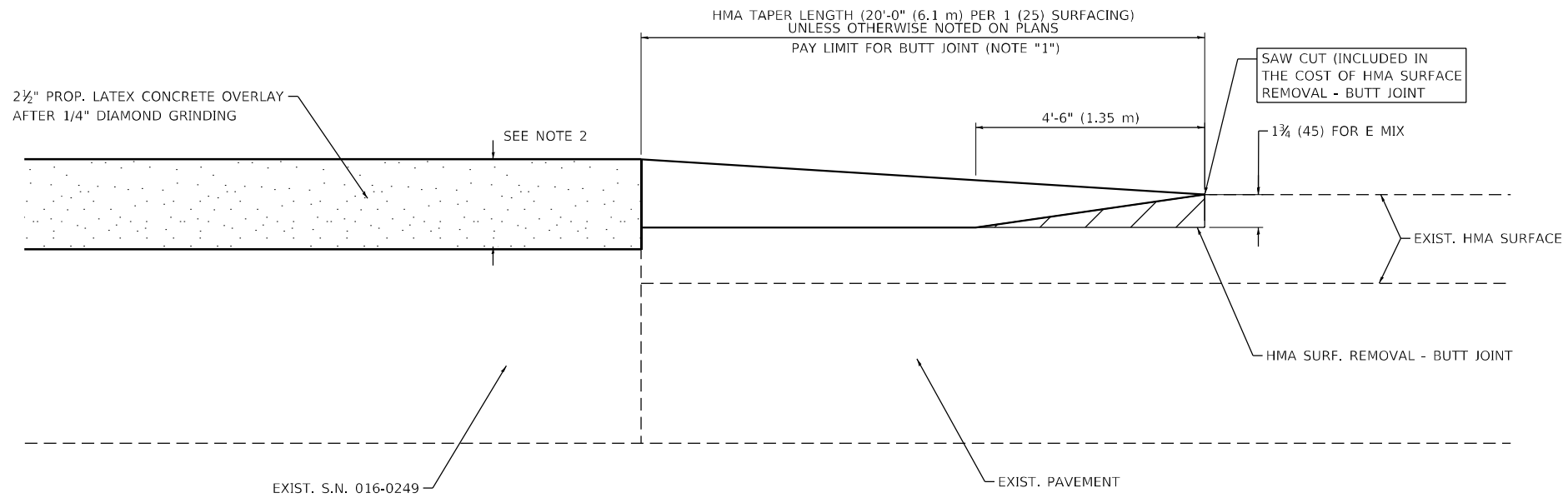


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	DATE - 2/2024	REVISED -

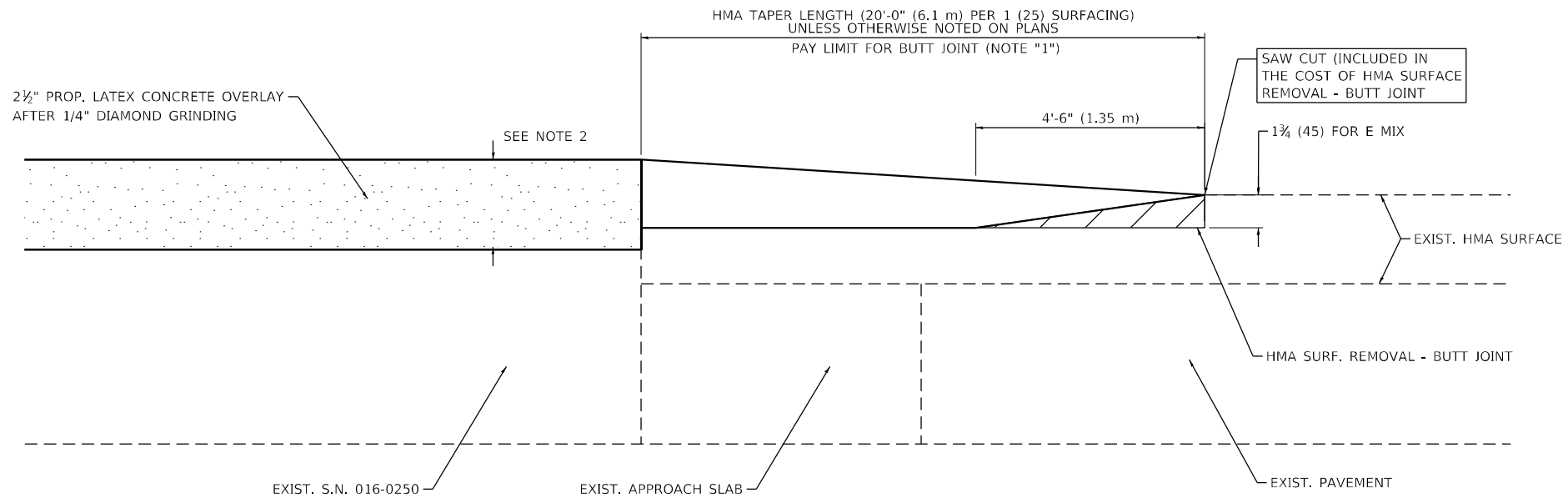
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**F.A.U. 1360 (FOSTER AVE) OVER CHICAGO RIVER  
 ROADWAY PLAN**  
 SCALE: 1"=50' SHEET 2 OF 3 SHEETS STA. 43+00.00 TO STA. 50+38.58

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	21
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P69	



**S.N. 016-0249 BUTT JOINT AND HMA TAPER  
FOR SCARIFICATION AND RESURFACING**



**S.N. 016-0250 BUTT JOINT AND HMA TAPER  
FOR SCARIFICATION AND RESURFACING**

**NOTES:**

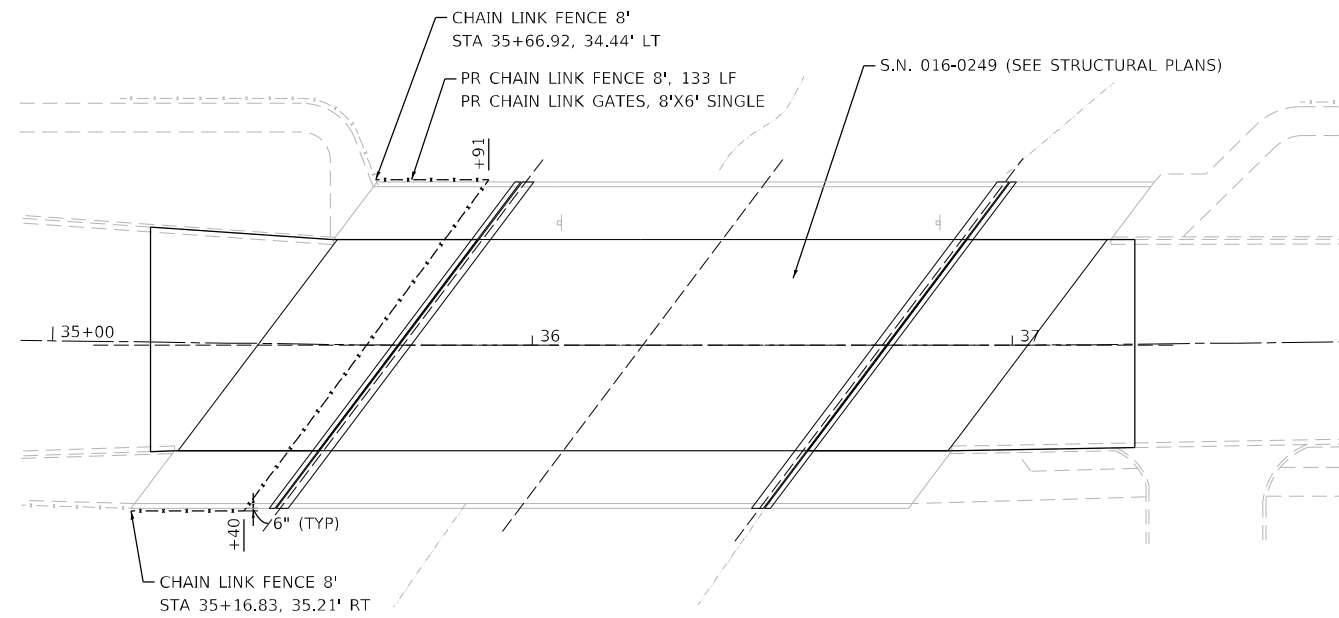
1. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
2. SEE BRIDGE PLANS FOR SCARIFICATION THICKNESS.
3. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

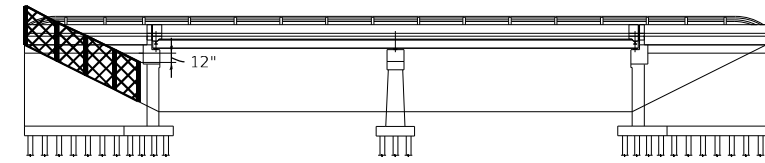
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	DRAWN - NH	REVISED -
PLOT SCALE = 2,0000' / in.	CHECKED - ST	REVISED -
PLOT DATE = 2/1/2024	DATE - 2/2024	REVISED -

<b>BUTT JOINT AND HMA TAPER DETAILS</b>		
SCALE: N.T.S.	SHEET 3 OF 3 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	22
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P69	

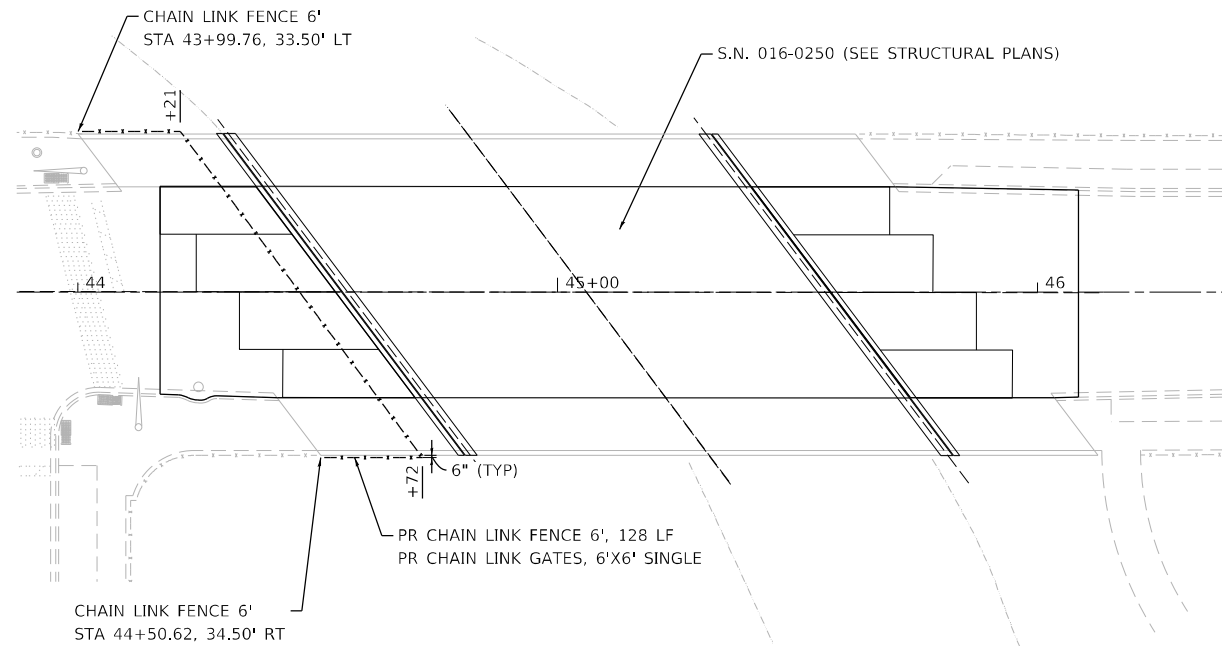


**TYPICAL PLAN VIEW**

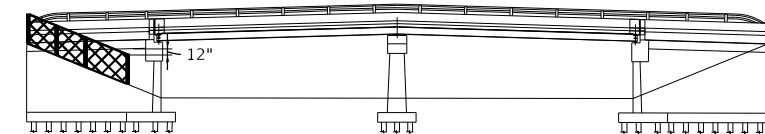


**TYPICAL ELEVATION VIEW**

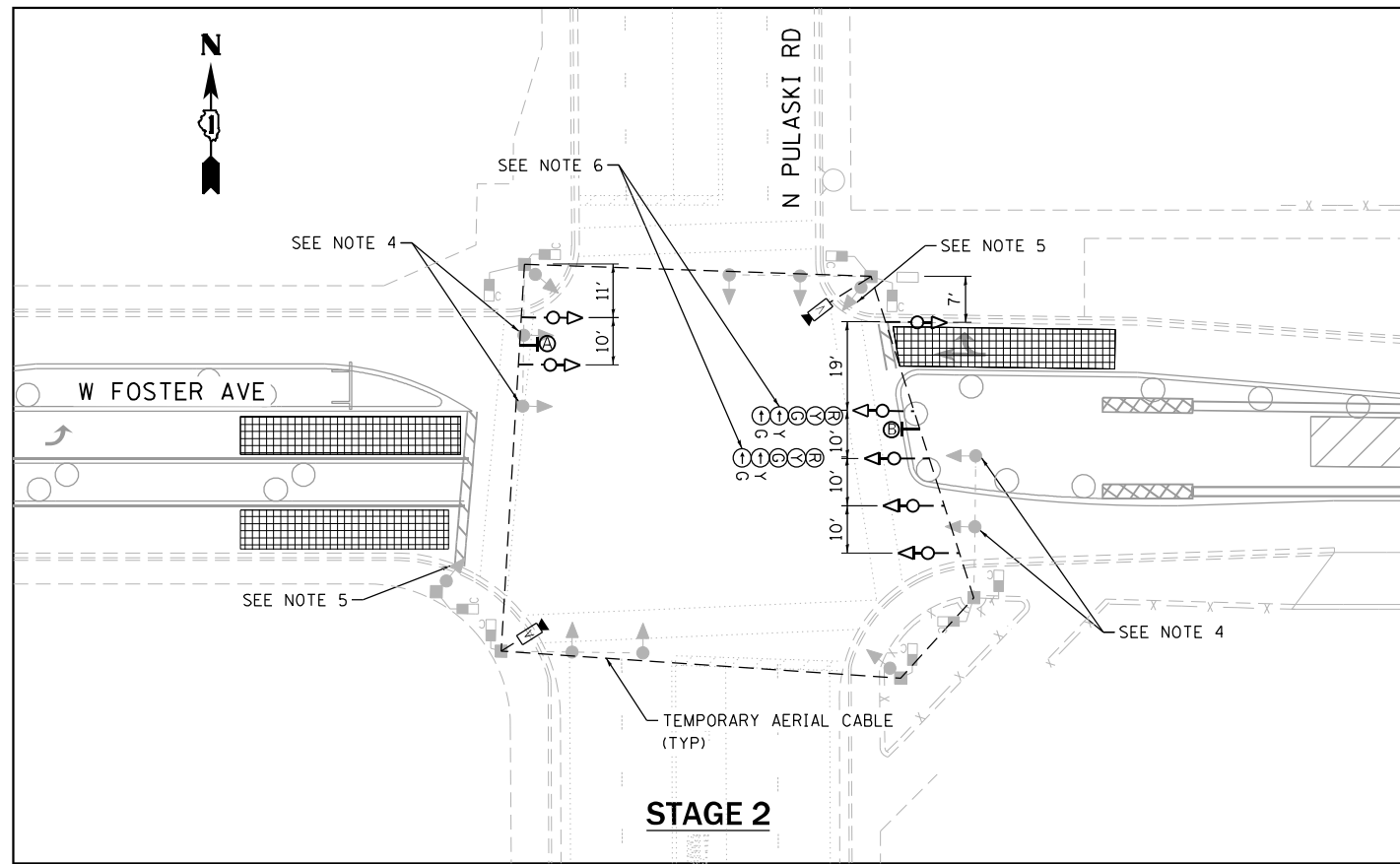
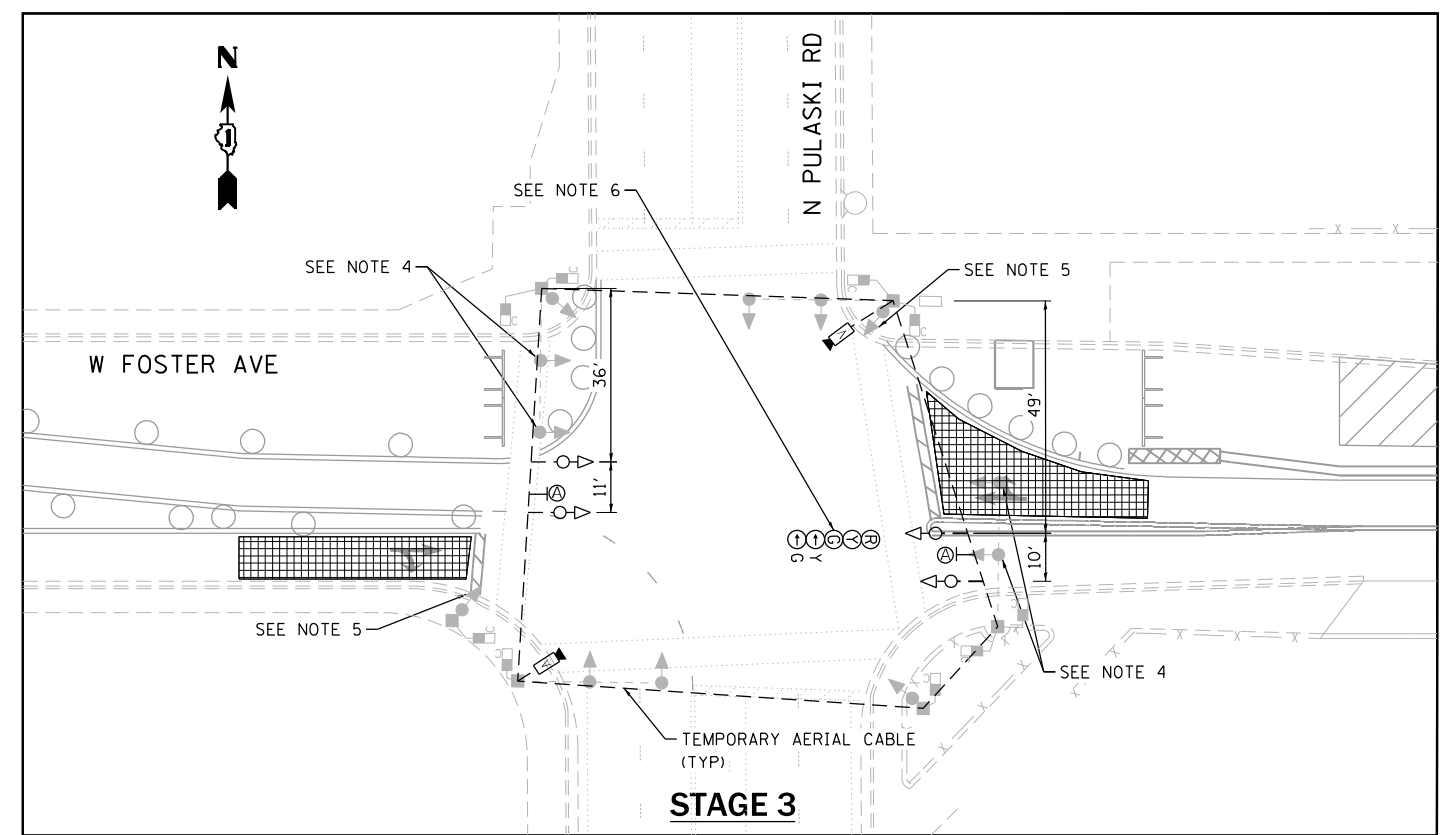
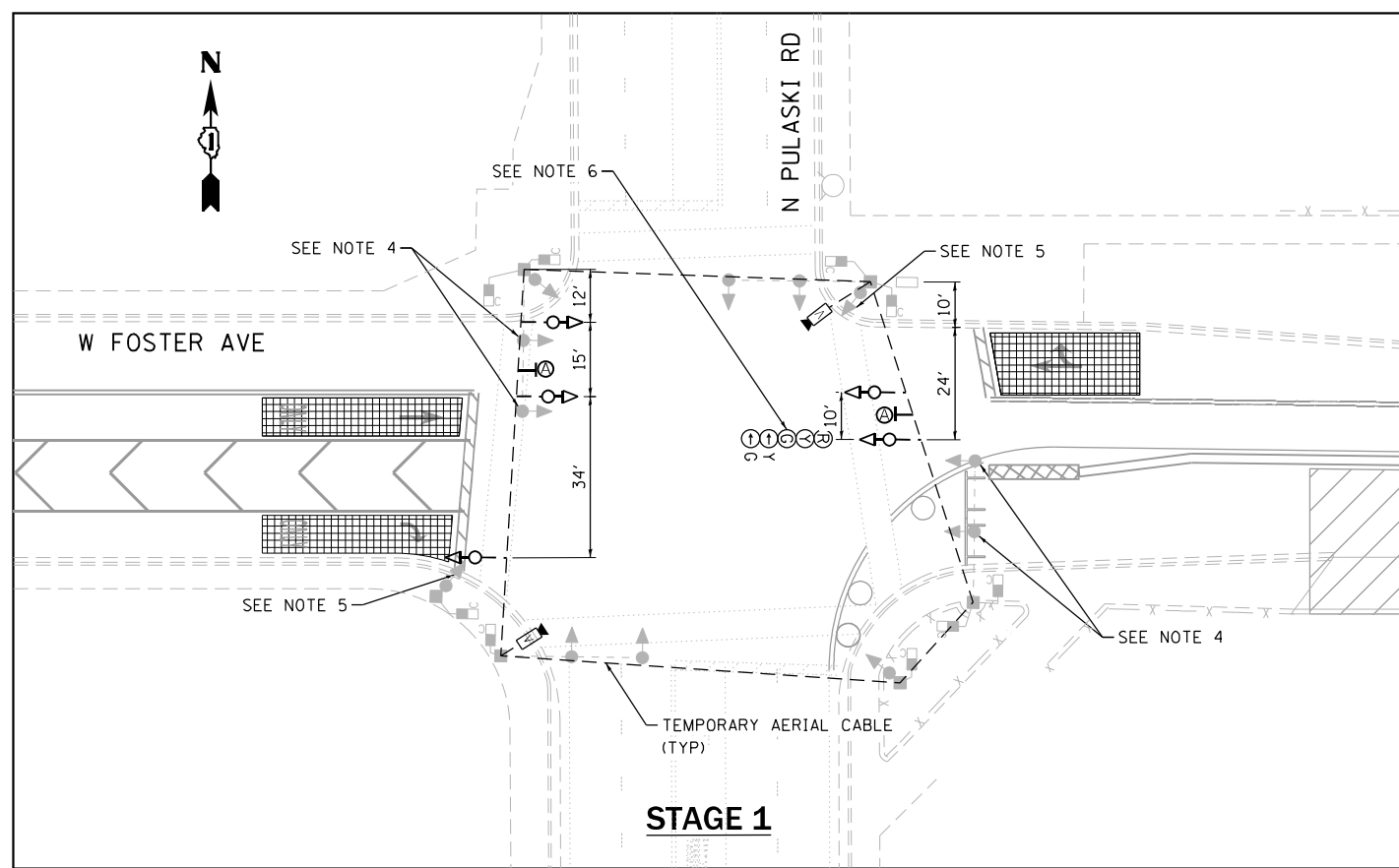
**NOTES:**  
 ALL FENCE SHALL BE 8' UNLESS OTHERWISE NOTED ON THE PLANS.  
 A 12" CLEARANCE BETWEEN THE TOP OF THE FENCE AND THE BOTTOM OF THE BRIDGE DECK FLOOR/BEAM SHALL BE MAINTAINED FOR THE PURPOSE OF FUTURE BRIDGE MAINTENANCE.  
 THE END POST SHALL BE WITHIN 6" OR LESS FROM THE ABUTMENT.  
 THE INTENT OF THIS CONTRACT IS TO DENY ACCESS OF ANY UNAUTHORIZED PERSONNEL UNDER THE BRIDGE. ACTUAL FENCE INSTALLATIONS MAY VARY FROM WHAT IS SHOWN ON THE PLANS. ALL FENCE INSTALLATIONS MUST BE APPROVED BY THE ENGINEER.  
 ALL HARDWARE AND LABOR NECESSARY TO CONSTRUCT AND INSTALL POSTS SHALL BE INCLUDED IN THE COST OF THE CHAIN LINK FENCE TO BE INSTALLED.



**TYPICAL PLAN VIEW**



**TYPICAL ELEVATION VIEW**



**LEGEND**

- EXISTING TRAFFIC SIGNAL HEAD
- PROPOSED TRAFFIC SIGNAL HEAD
- EXISTING COUNTDOWN PEDESTRIAN SIGNAL
- PROPOSED COUNTDOWN PEDESTRIAN SIGNAL
- EXISTING TRAFFIC SIGNAL POLE OR POST
- PROPOSED TRAFFIC SIGNAL POLE OR POST
- EXISTING TRAFFIC SIGNAL MAST ARM
- PROPOSED TRAFFIC SIGNAL MAST ARM
- ⊠ EXISTING COLUMNS
- ⊠ STRUCTURE MOUNTED
- PROPOSED BUS "QUEUE JUMP" SIGNAL
- PROPOSED BIKE SIGNAL
- RETROREFLECTIVE "LEFT/RIGHT TURN ON GREEN ARROW ONLY" SIGN
- RETROREFLECTIVE "BUS SIGNAL" SIGN
- ⊠ HORIZONTALLY MOUNTED
- EXISTING PUSH BUTTON
- ▨ VIDEO DETECTION ZONE
- ◀ VIDEO DETECTION CAMERA
- ⊠ R3-2, 30" X 36" "NO LEFT TURN" (3 REQUIRED)
- ⊠ LEFT ON GREEN ARROW ONLY R10-5, 30" X 36" (1 REQUIRED)

**NOTES**

1. EXISTING TRAFFIC SIGNAL MAST ARMS SHALL BE USED FOR PROPOSED OVERHANGING TEMPORARY TRAFFIC SIGNAL INSTALLATION. TEMPORARY TRAFFIC SIGNALS SHALL BE CONNECTED TO THE EXISTING CONTROLLER AND SYNCHRONIZED WITH THE EXISTING TRAFFIC SIGNAL SYSTEM. THE TEMPORARY TRAFFIC SIGNAL HEADS AND WIRINGS SHALL BE COMPATIBLE TO MATCH WITH THE EXISTING CONTROLLER AND TRAFFIC SYSTEM. THIS WORK SHALL NOT BE PAID SEPARATELY AND SHALL BE CONSIDERED INCLUDED IN "TEMPORARY TRAFFIC SIGNAL INSTALLATION".
2. CONTRACTOR TO COORDINATE WITH CITY OF CHICAGO DEPARTMENT OF ELECTRICAL SERVICE PRIOR TO THE INSTALLATION OF TEMPORARY TRAFFIC SIGNALS AND CONNECTING WITH THE EXISTING TRAFFIC SIGNAL SYSTEM/EXISTING CONTROLLER.
3. EXISTING TRAFFIC SIGNAL AND SIGNS THAT ARE IN CONFLICT WITH THE PROPOSED STAGING TRAFFIC SIGNALS AND SIGNS IN THE RESPECTIVE STAGE OF CONSTRUCTION SHALL NOT BE PAID SEPARATELY AND SHALL BE CONSIDERED INCLUDED IN "TEMPORARY TRAFFIC SIGNAL INSTALLATION".
4. EXISTING SIGNALS NEED TO BE BAGGED DURING CONSTRUCTION STAGES 1, 2 & 3 AND SHALL BE UNBAGGED AT THE END OF STAGE CONSTRUCTION OR AS DIRECTED BY THE ENGINEER.
5. LEFT TURN IS PROHIBITED DURING STAGES 1 & 3. EXISTING SIGNALS AND SIGNS THAT CONFLICT WITH THE PROPOSED STAGE OF CONSTRUCTION NEED TO BE BAGGED DURING THE CONSTRUCTION STAGE 1 & STAGE 3.
6. ARROW ON YELLOW AND ARROW ON GREEN SHALL BE BAGGED UNDER STAGE 1 & 3 AND UNBAGGED DURING CONSTRUCTION STAGE 2.
7. EXISTING TRAFFIC SIGNALS WHICH ARE NOT IN CONFLICT WITH THE PROPOSED TRAFFIC STAGE SHALL REMAIN OPERATIONAL. HOWEVER, MAY REQUIRE ADJUSTMENT OF POSITION AS DIRECTED BY THE ENGINEER. THIS WORK SHALL NOT BE PAID SEPARATELY AND SHALL BE CONSIDERED INCLUDED IN "TEMPORARY TRAFFIC SIGNAL INSTALLATION".
8. EXISTING PEDESTRIAN SIGNALS SHALL REMAIN OPERATIONAL AND PHASING SHALL BE ADJUSTED AS PER RESPECTIVE STAGE OF CONSTRUCTION. THIS WORK SHALL NOT BE PAID SEPARATELY AND SHALL BE CONSIDERED INCLUDED IN "TEMPORARY TRAFFIC SIGNAL INSTALLATION".
9. TEMPORARY VIDEO DETECTION CAMERAS SHALL BE RECALIBERATED/ADJUSTED AS PER RESPECTIVE STAGE OF TRAFFIC.
10. TRAFFIC SIGNAL MANAGEMENT SYSTEM SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED TRAFFIC SIGNAL HEADS/EQUIPMENT SHALL BE AS SHOWN/NOTED ON PLANS AND THE CONTRACTOR SHALL PLACE THEM IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
11. TEMPORARY SIGNAL SHALL FOLLOW IDOT SECTION #890 OF THE STANDARD SPECIFICATIONS.
12. TEMPORARY TRAFFIC SIGNAL TO BE MAINTAINED BY CONTRACTOR. CONTRACTOR TO PROVIDE CDOT WITH COMPANY CONTACT TO BE AVAILABLE 24/7.
13. CONTRACTOR TO ENSURE THAT POWER TO THE EXISTING COMBINATION LIGHT POLES AND STREET LIGHTS SHALL BE MAINTAINED DURING ALL STAGES OF CONSTRUCTION.



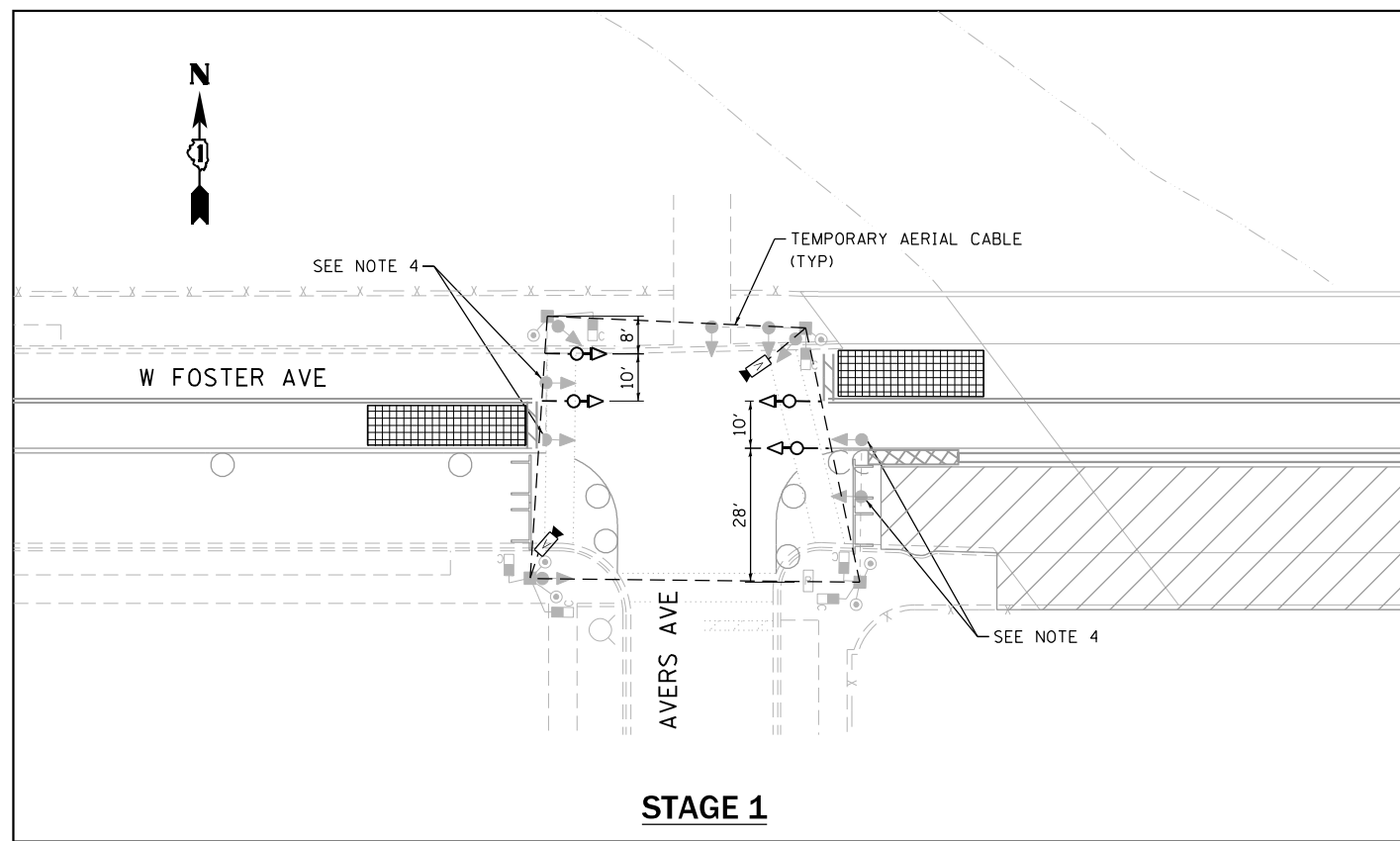
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DRAWN - IS	REVISED -	
PLOT SCALE = 40,0109' / in.	CHECKED - ST	REVISED -
PLOT DATE = 2/1/2024	DATE - 2/2024	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

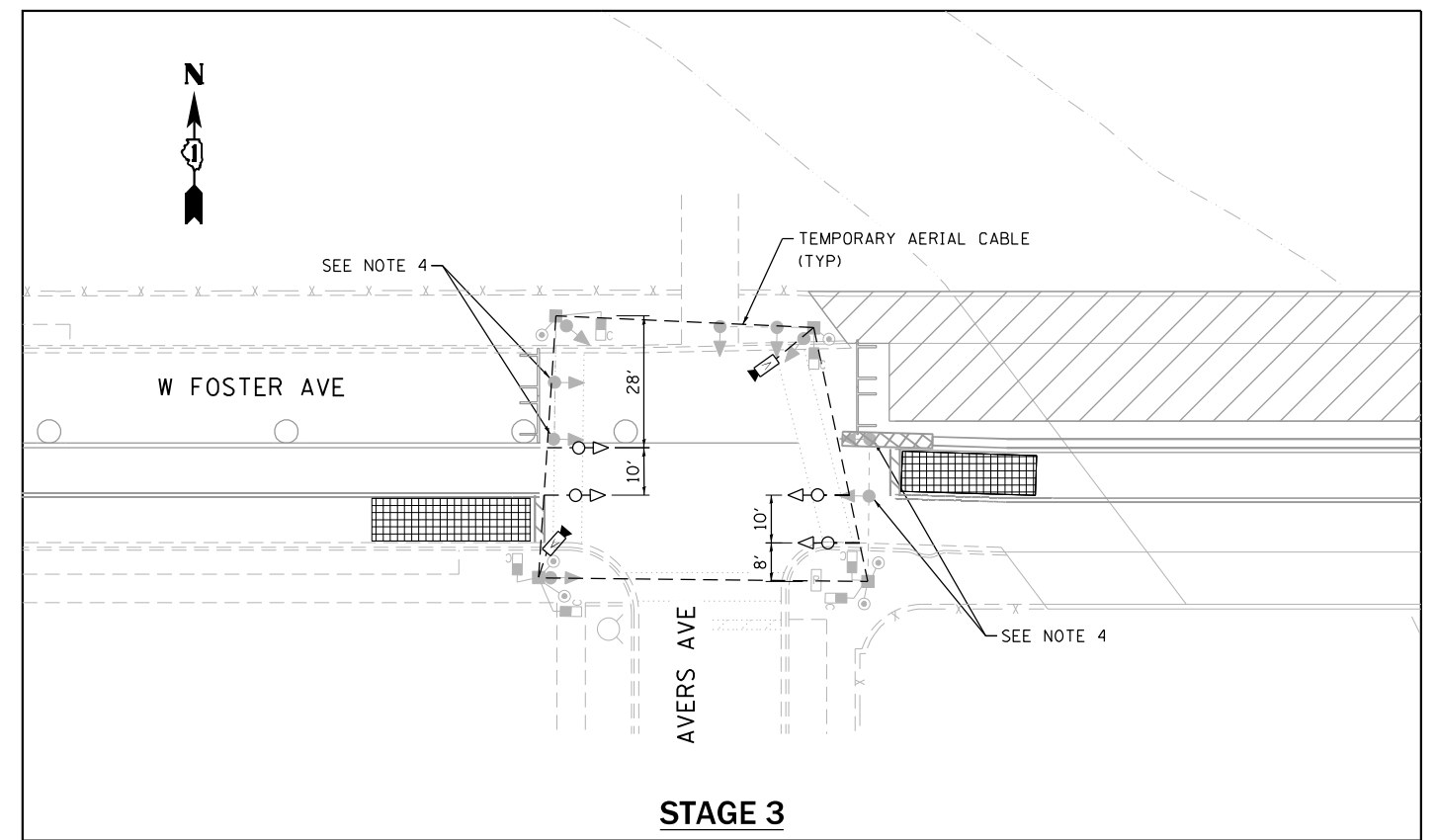
F.A.U. 1360 (FOSTER AVE) OVER CHICAGO RIVER  
W FOSTER AVE AT N PULASKI RD  
TEMPORARY TRAFFIC SIGNALS

SCALE: N.T.S. SHEET 1 OF 5 SHEETS STA. TO STA.

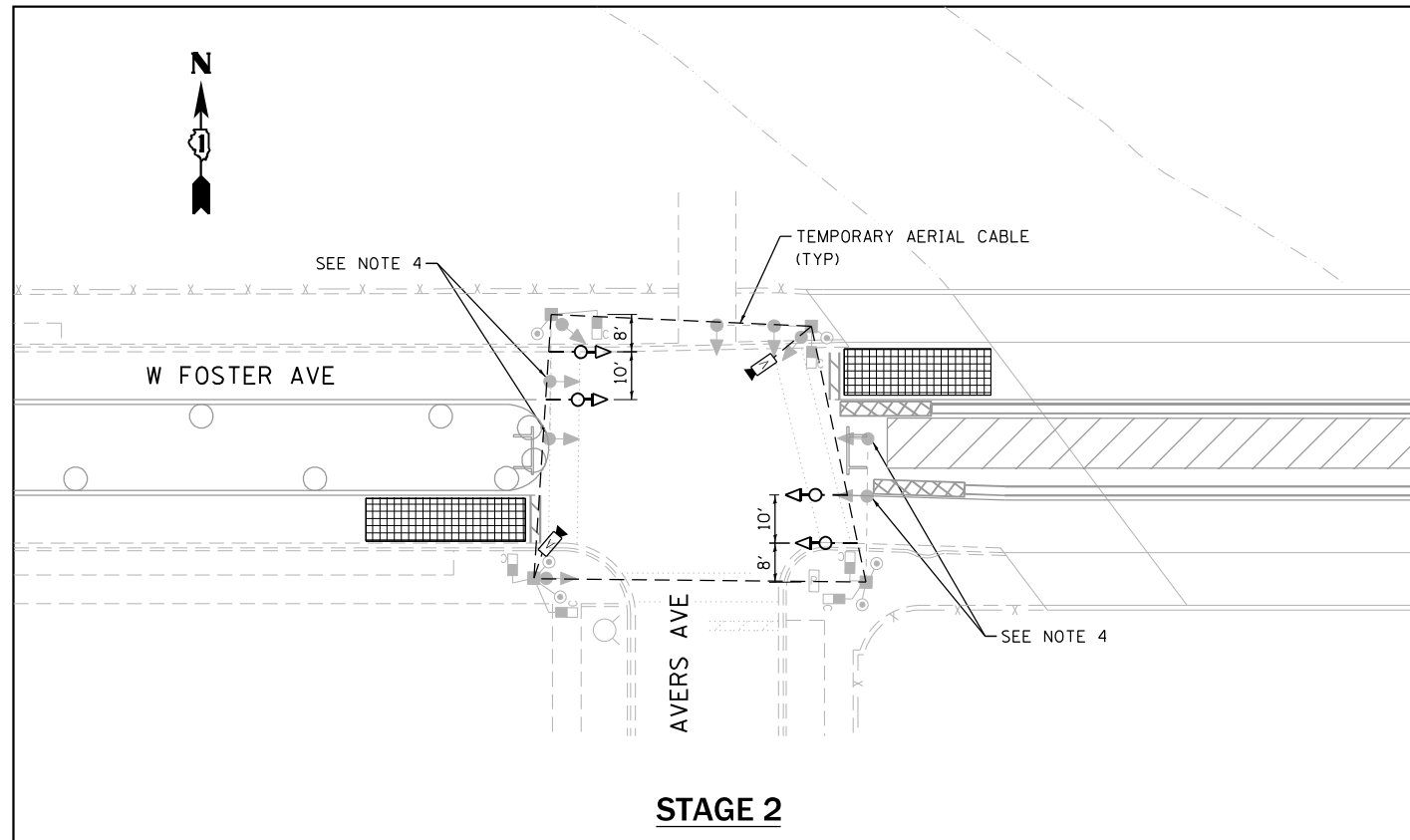
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	24
CONTRACT NO. 62P69				
ILLINOIS FED. AID PROJECT				



**STAGE 1**



**STAGE 3**



**STAGE 2**

**LEGEND**

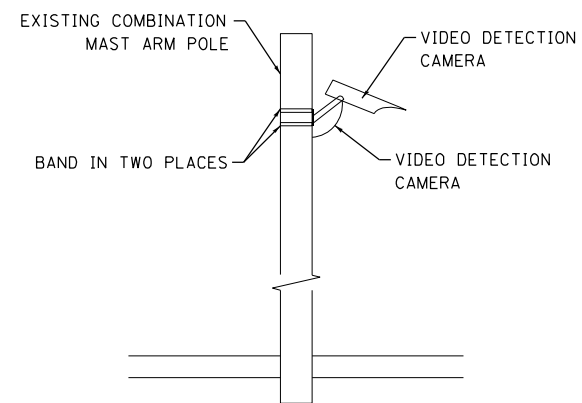
- EXISTING TRAFFIC SIGNAL HEAD
- PROPOSED TRAFFIC SIGNAL HEAD
- EXISTING COUNTDOWN PEDESTRIAN SIGNAL
- PROPOSED COUNTDOWN PEDESTRIAN SIGNAL
- EXISTING TRAFFIC SIGNAL POLE OR POST
- PROPOSED TRAFFIC SIGNAL POLE OR POST
- EXISTING TRAFFIC SIGNAL MAST ARM
- PROPOSED TRAFFIC SIGNAL MAST ARM
- ⊠ EXISTING COLUMNS
- ▧ STRUCTURE MOUNTED
- B PROPOSED BUS "QUEUE JUMP" SIGNAL
- B PROPOSED BIKE SIGNAL
- RETROREFLECTIVE "LEFT/RIGHT TURN ON GREEN ARROW ONLY" SIGN
- RETROREFLECTIVE "BUS SIGNAL" SIGN
- H HORIZONTALLY MOUNTED
- EXISTING PUSH BUTTON
- ▧ VIDEO DETECTION ZONE
- VIDEO DETECTION CAMERA

**NOTES**

1. EXISTING TRAFFIC SIGNAL MAST ARMS SHALL BE USED FOR PROPOSED OVERHANGING TEMPORARY TRAFFIC SIGNAL INSTALLATION. TEMPORARY TRAFFIC SIGNALS SHALL BE CONNECTED TO THE EXISTING CONTROLLER AND SYNCHRONIZED WITH THE EXISTING TRAFFIC SIGNAL SYSTEM. THE TEMPORARY TRAFFIC SIGNAL HEADS AND WIRINGS SHALL BE COMPATIBLE TO MATCH WITH THE EXISTING CONTROLLER AND TRAFFIC SYSTEM. THIS WORK SHALL NOT BE PAID SEPARATELY AND SHALL BE CONSIDERED INCLUDED IN "TEMPORARY TRAFFIC SIGNAL INSTALLATION".
2. CONTRACTOR TO COORDINATE WITH CITY OF CHICAGO DEPARTMENT OF ELECTRICAL SERVICE PRIOR TO THE INSTALLATION OF TEMPORARY TRAFFIC SIGNALS AND CONNECTING WITH THE EXISTING TRAFFIC SIGNAL SYSTEM/EXISTING CONTROLLER.
3. EXISTING TRAFFIC SIGNAL AND SIGNS THAT ARE IN CONFLICT WITH THE PROPOSED STAGING TRAFFIC SIGNALS AND SIGNS IN THE RESPECTIVE STAGE OF CONSTRUCTION SHALL NOT BE PAID SEPARATELY AND SHALL BE CONSIDERED INCLUDED IN "TEMPORARY TRAFFIC SIGNAL INSTALLATION".
4. EXISTING SIGNALS NEED TO BE BAGGED DURING CONSTRUCTION STAGES 1, 2 & 3 AND SHALL BE UNBAGGED AT THE END OF STAGE CONSTRUCTION OR AS DIRECTED BY THE ENGINEER.
5. EXISTING TRAFFIC SIGNALS WHICH ARE NOT IN CONFLICT WITH THE PROPOSED TRAFFIC STAGE SHALL REMAIN OPERATIONAL. HOWEVER, MAY REQUIRE ADJUSTMENT OF POSITION AS DIRECTED BY THE ENGINEER. THIS WORK SHALL NOT BE PAID SEPARATELY AND SHALL BE CONSIDERED INCLUDED IN "TEMPORARY TRAFFIC SIGNAL INSTALLATION".
6. EXISTING PEDESTRIAN SIGNALS SHALL REMAIN OPERATIONAL AND PHASING SHALL BE ADJUSTED AS PER RESPECTIVE STAGE OF CONSTRUCTION. THIS WORK SHALL NOT BE PAID SEPARATELY AND SHALL BE CONSIDERED INCLUDED IN "TEMPORARY TRAFFIC SIGNAL INSTALLATION".
7. TEMPORARY VIDEO DETECTION CAMERAS SHALL BE RECALIBERATED/ADJUSTED AS PER RESPECTIVE STAGE OF TRAFFIC.
8. TRAFFIC SIGNAL MANAGEMENT SYSTEM SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED TRAFFIC SIGNAL HEADS/EQUIPMENT SHALL BE AS SHOWN/NOTED ON PLANS AND THE CONTRACTOR SHALL PLACE THEM IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
9. TEMPORARY SIGNAL SHALL FOLLOW IDOT SECTION #890 OF THE STANDARD SPECIFICATIONS.
10. TEMPORARY TRAFFIC SIGNAL TO BE MAINTAINED BY CONTRACTOR. CONTRACTOR TO PROVIDE CDOOT WITH COMPANY CONTACT TO BE AVAILABLE 24/7.
11. CONTRACTOR TO ENSURE THAT POWER TO THE EXISTING COMBINATION LIGHT POLES AND STREET LIGHTS SHALL BE MAINTAINED DURING ALL STAGES OF CONSTRUCTION.

USER NAME = 14nho	DESIGNED - IS	REVISED -
DRAWN - IS	REVISED -	
PLOT SCALE = 40,0109' / 1"	CHECKED - ST	REVISED -
PLOT DATE = 2/1/2024	DATE - 2/2024	REVISED -

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	25
CONTRACT NO. 62P69				
ILLINOIS FED. AID PROJECT				



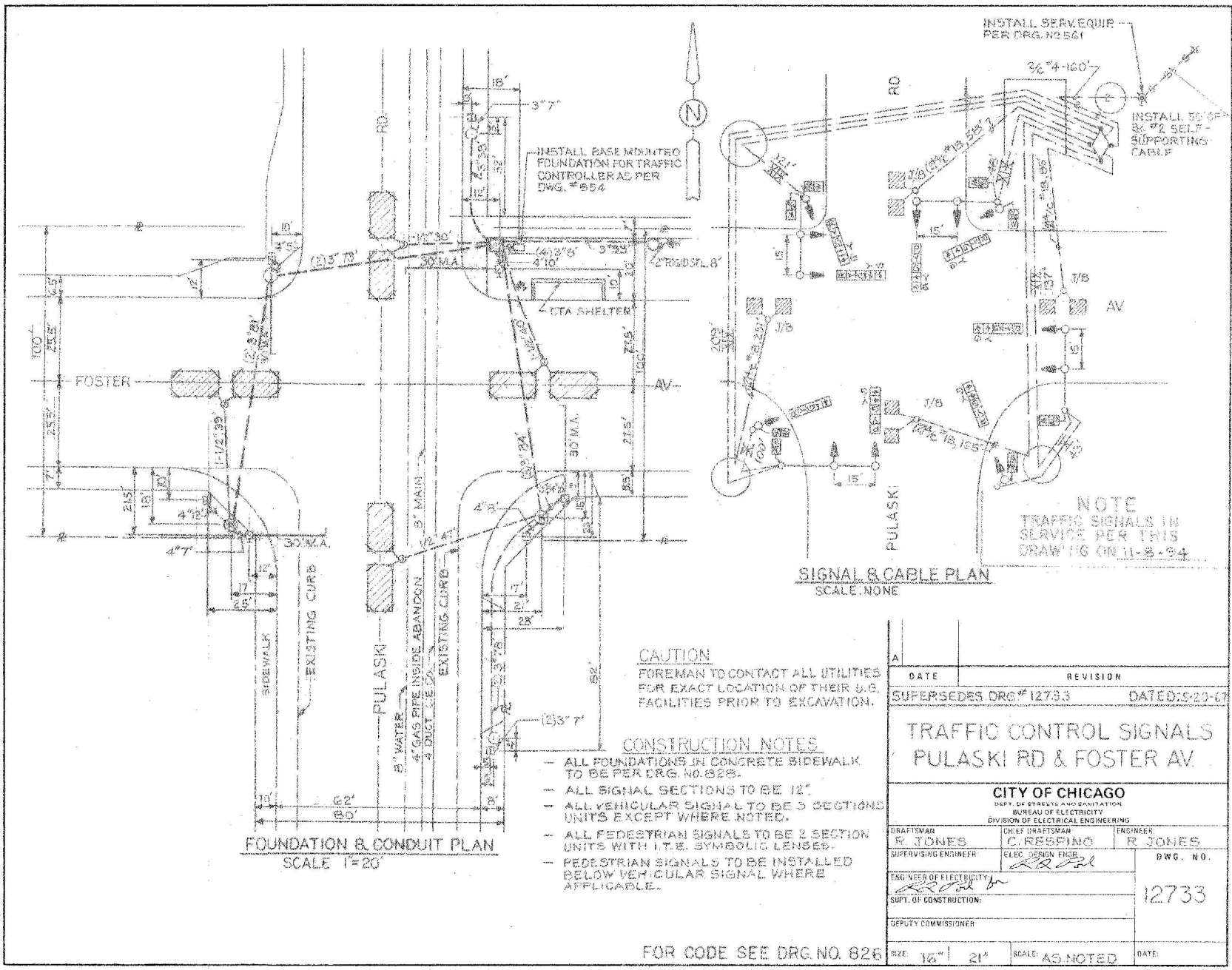
**TEMPORARY VIDEO DETECTION  
MOUNTING DETAIL**

(NOT TO SCALE)

USER NAME = 14nho	DESIGNED - NH	REVISED -
	DRAWN - NH	REVISED -
PLOT SCALE = 40,0000 * / in.	CHECKED - ST	REVISED -
PLOT DATE = 2/1/2024	DATE - 2/2024	REVISED -

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	26
			CONTRACT NO. 62P69	
ILLINOIS FED. AID PROJECT				

APPROVED AS WORKING PLAN	FEDERAL AID ROUTE NO.	SECTION
BY	IL	
REVISIONS OF WORKING PLAN	FHWA REG. NO.	ILLINOIS



**NOTE**  
FOR INSTALLATION AND REMOVAL OF STREET LIGHTING AND FIRE ALARM EQUIPMENT SEE DRG. NO. 93-110

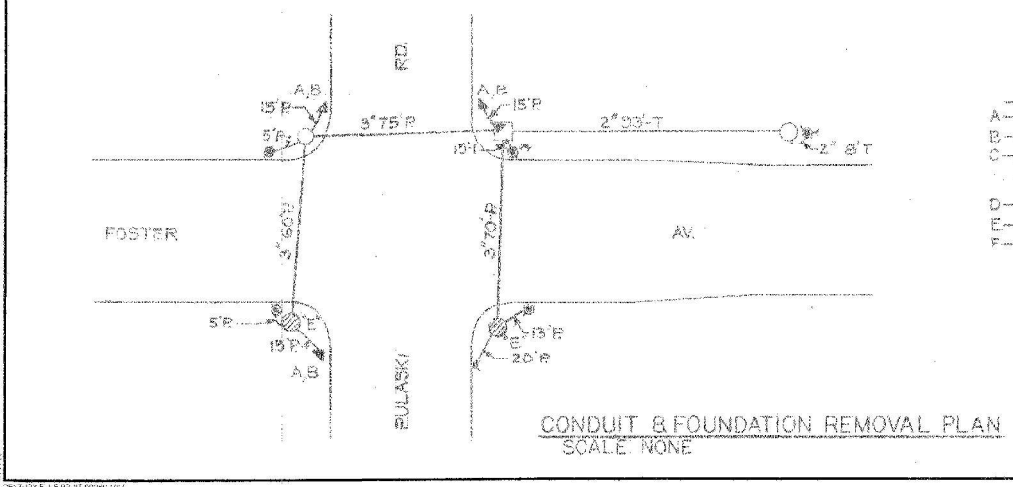
**FOR REFERENCE ONLY**

**NOTE:**  
EXISTING INFORMATION SHOWN ON THIS PLAN IS BASED ON THE LATEST INFORMATION RECEIVED FROM CDOT/IDOT AND SHALL BE FIELD VERIFIED.

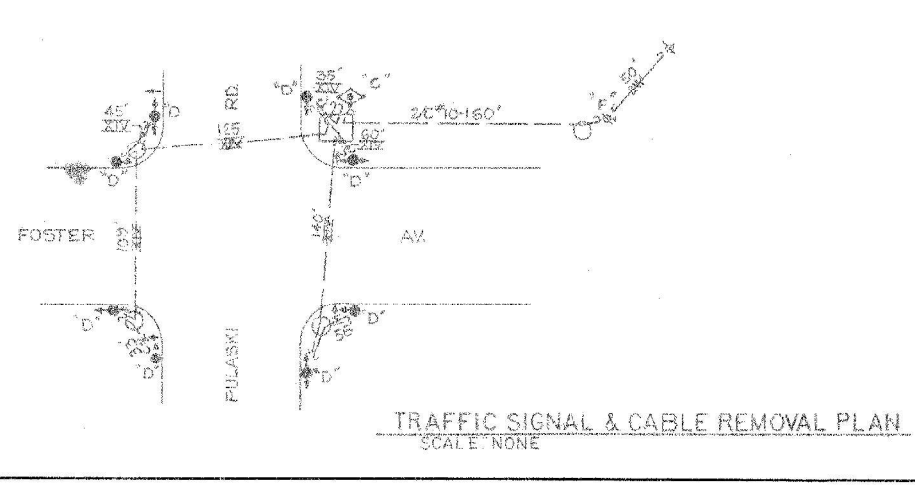
- CONSTRUCTION NOTES**
- ALL FOUNDATIONS IN CONCRETE SIDEWALK TO BE PER DRG. NO. 828.
  - ALL SIGNAL SECTIONS TO BE 12"
  - ALL VEHICULAR SIGNAL TO BE 3 SECTIONS UNITS EXCEPT WHERE NOTED.
  - ALL PEDESTRIAN SIGNALS TO BE 2 SECTION UNITS WITH I.T.E. SYMBOLIC LENSES.
  - PEDESTRIAN SIGNALS TO BE INSTALLED BELOW VEHICULAR SIGNAL WHERE APPLICABLE.

DATE	REVISION
SUPERSEDES DRG. # 12733 DATED: 9-20-07	
<b>TRAFFIC CONTROL SIGNALS PULASKI RD &amp; FOSTER AV.</b>	
<b>CITY OF CHICAGO</b> DEPT. OF STREETS AND SANITATION BUREAU OF ELECTRICITY DIVISION OF ELECTRICAL ENGINEERING	
DRAFTSMAN R. JONES	ENGINEER R. JONES
SUPERVISING ENGINEER C. RESPINO	ELEC. DESIGN ENGR. C. RESPINO
ENGINEER OF ELECTRICITY R. JONES	DWG. NO. 12733
SUPT. OF CONSTRUCTION:	
DEPUTY COMMISSIONER:	
SIZE 1/8" x 21"	SCALE: AS NOTED

FOR CODE SEE DRG. NO. 826



- TRAFFIC SIGNAL REMOVAL NOTES**
- A-BREAKDOWN TYPE 'A' FOUNDATION 3
  - B-REMOVE 17' POST & PEDESTAL 3
  - C-REMOVE TRAFFIC SIGNAL CONTROLLER & POST 1
  - BURN OFF ANCHOR RODS AND GROUT IN CONDUIT OPENINGS ON EXISTING MANHOLE ROOF
  - D-REMOVE TRAFFIC CONTROL SIGNALS 2
  - E-BREAKDOWN EXISTING HANDHOLE 2
  - F-REMOVE EXISTING SERVICE EQUIP 1



DATE	REVISION
WORK ORDER NO. 1211412 DATE 12-11-92	
COST ALLOCATION ACCOUNT	
APPROPRIATION ACCOUNT	MATERIAL
	LABOR
<b>TRAFFIC CONTROL SIGNALS PULASKI RD &amp; FOSTER AV.</b>	
<b>CITY OF CHICAGO</b> DEPT. OF STREETS AND SANITATION BUREAU OF ELECTRICITY DIVISION OF ELECTRICAL ENGINEERING	
DRAFTSMAN R. JONES	ENGINEER R. JONES
SUPERVISING ENGINEER C. RESPINO	ELEC. DESIGN ENGR. C. RESPINO
ENGINEER OF ELECTRICITY R. JONES	DWG. NO. 12733
SUPT. OF CONSTRUCTION:	
DEPUTY COMMISSIONER: James Hoffmann	
SIZE 22" x 30"	SCALE: AS NOTED



USER NAME = 14nh	DESIGNED - NH	REVISED -
PLOT SCALE = 40,0000 * 1/4"	DRAWN - NH	REVISED -
PLOT DATE = 2/1/2024	CHECKED - ST	REVISED -
	DATE - 2/2024	REVISED -

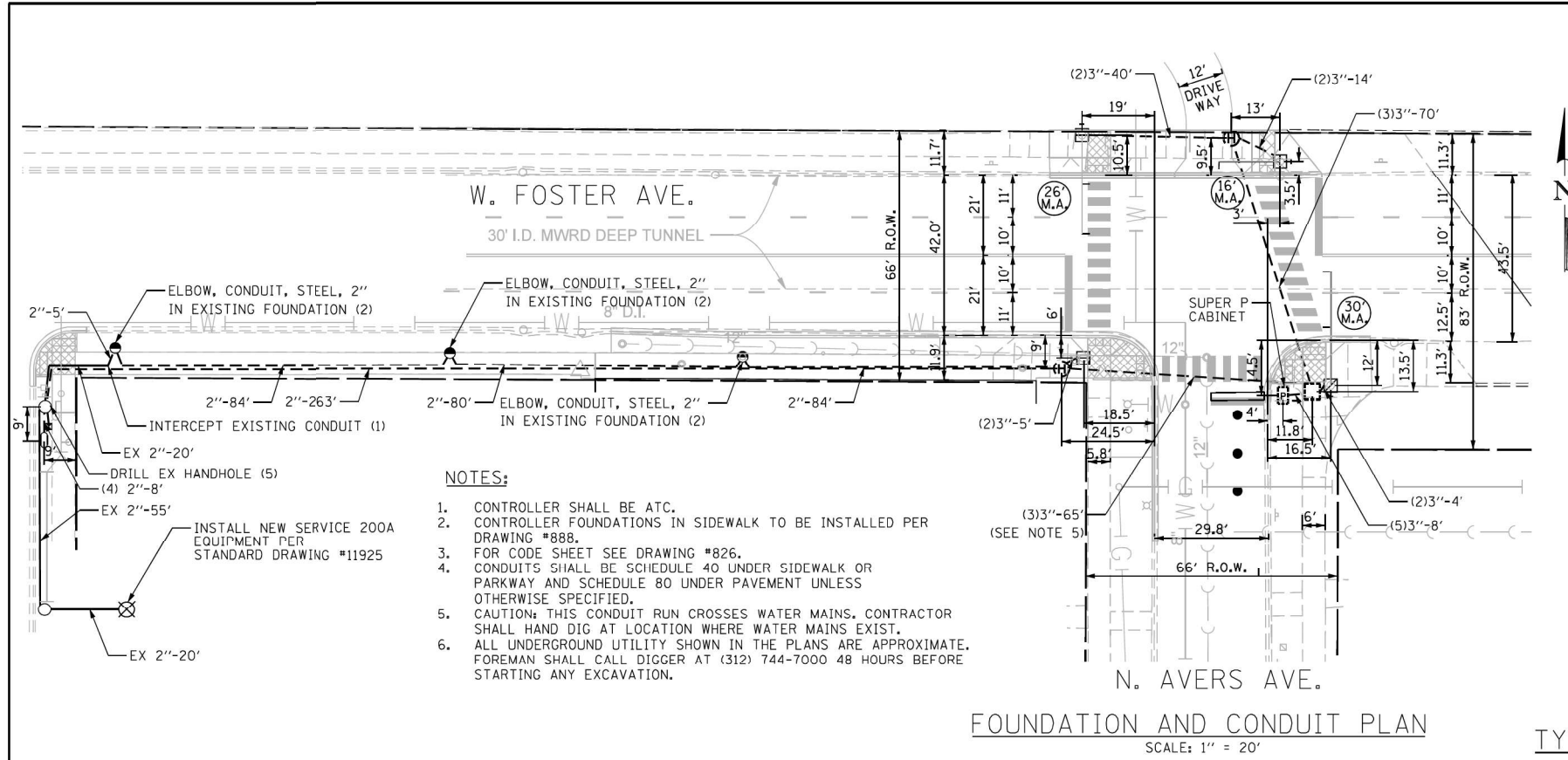
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EXISTING PLAN  
(FOSTER AVE AND PULASKI RD)**

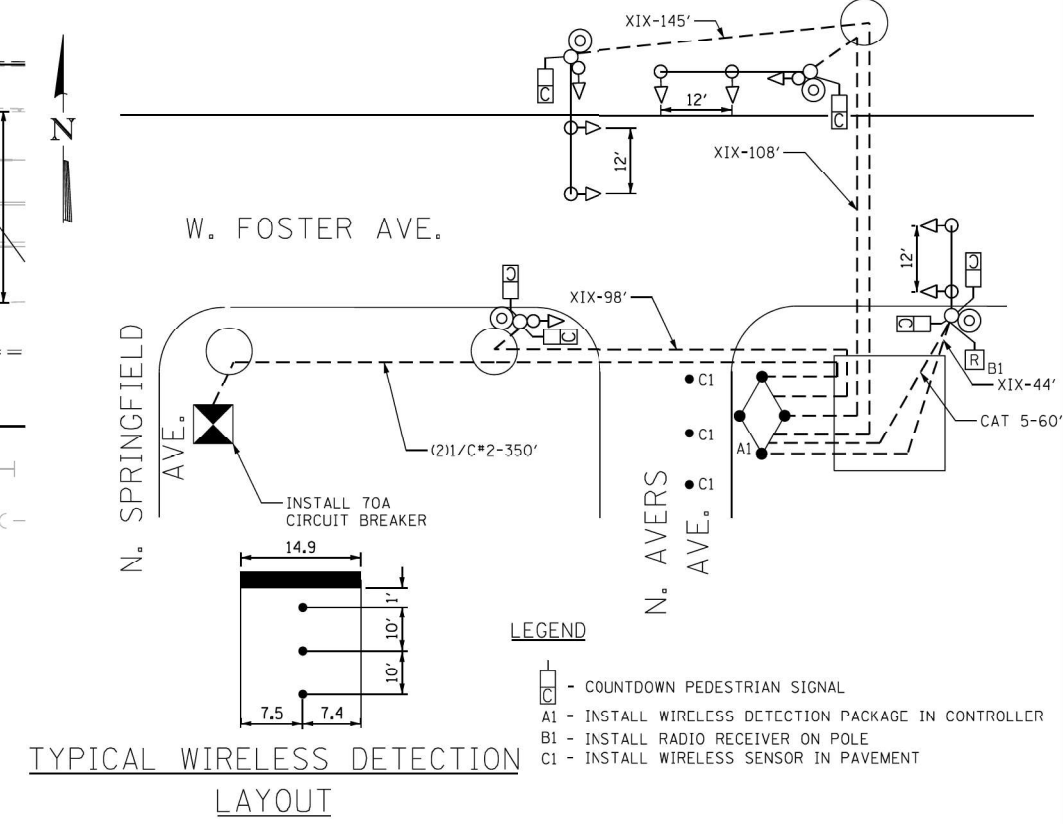
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	27
CONTRACT NO. 62P69				
ILLINOIS FED. AID PROJECT				

F.A. ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
XX-B41XX-00-TL	COOK		60	28
FHWA REG. NO.	ILLINOIS PROJECT			



FOUNDATION AND CONDUIT PLAN  
SCALE: 1" = 20'

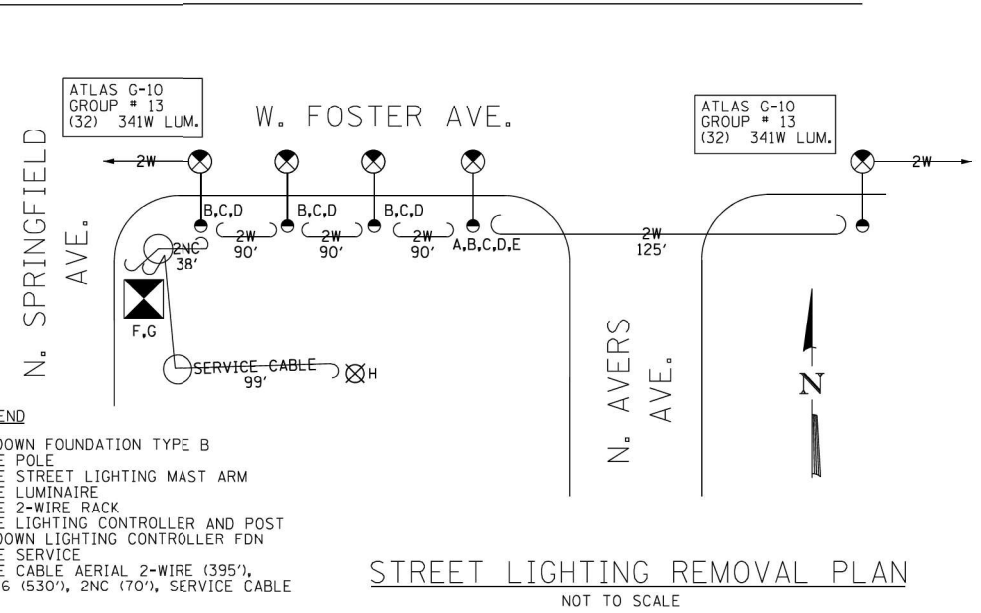


TYPICAL WIRELESS DETECTION LAYOUT

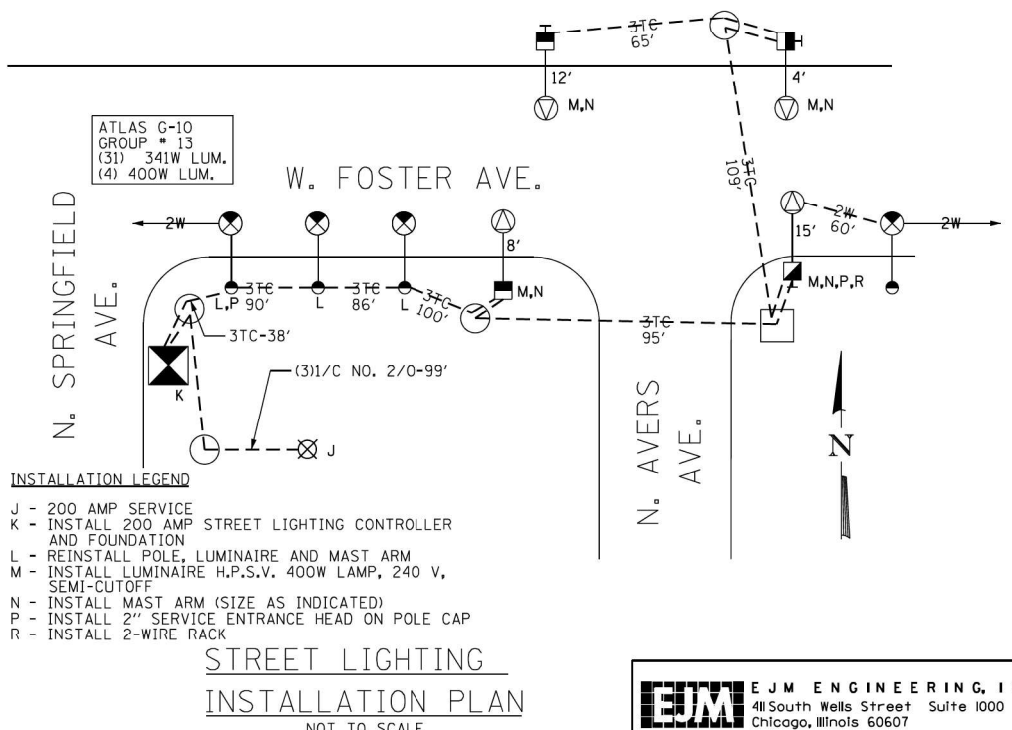
SIGNAL AND CABLE PLAN  
NOT TO SCALE

**FOR REFERENCE ONLY**

NOTE:  
EXISTING INFORMATION SHOWN ON THIS PLAN IS BASED ON THE LATEST INFORMATION RECEIVED FROM CDOT/IDOT AND SHALL BE FIELD VERIFIED.



STREET LIGHTING REMOVAL PLAN  
NOT TO SCALE



STREET LIGHTING INSTALLATION PLAN  
NOT TO SCALE

NO.	DATE	REVISION
SUPERSEDES DRAWING NO. :		DATED:
CUC NO.	2016-69365	DATE
APPOX. ADDRESS 3800-3900 W FOSTER AVE. CHICAGO, IL 60625		
WORK ORDER NO.	11413015	DATE
COST ALLOCATION ACCOUNT		
APPROPRIATION ACCOUNT	MATERIAL	LABOR

TRAFFIC CONTROL SIGNALS  
N. AVERS AVE. AND  
W. FOSTER AVE.

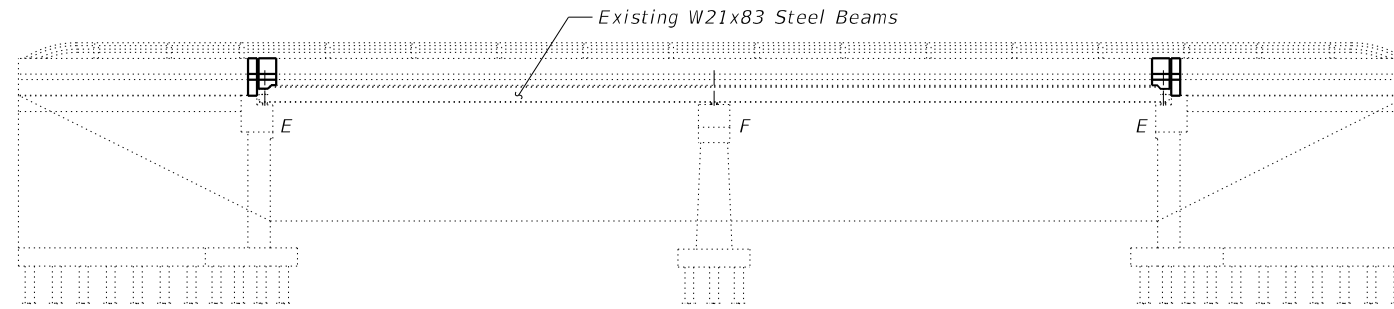
CITY OF CHICAGO DEPARTMENT OF TRANSPORTATION DIVISION OF ENGINEERING ELECTRICAL SECTION		
DRAFTSMAN:	CHIEF DRAFTSMAN:	ENGINEER:
		SA
SUPERVISING ENGINEER:	ELEC. DESIGN ENGR.	DWG. NO.
ENGINEER OF ELECTRICITY:		24010
SUPT. OF CONSTRUCTION:		
DEPUTY COMMISSIONER:		
SIZE: 22"   34"	SCALE: AS NOTED	DATE: 4-12-2016



Existing Structure: SN 016-0249 built in 1933 as SBI Route 21, Section 21-BZ-1 at Sta. 36+23. Bridge reconstruction in 1985 included superstructure replacement and substructure modifications. The structure is a two span bridge with 7½" deck on rolled steel beams, measuring 102'-0" back to back abutments, 68'-0" out to out deck with 37°00'00" left ahead skew. The concrete substructure units are comprised of closed abutments and a solid wall pier, all founded on untreated timber piles. Stage construction shall be utilized to maintain one lane of traffic in each direction.

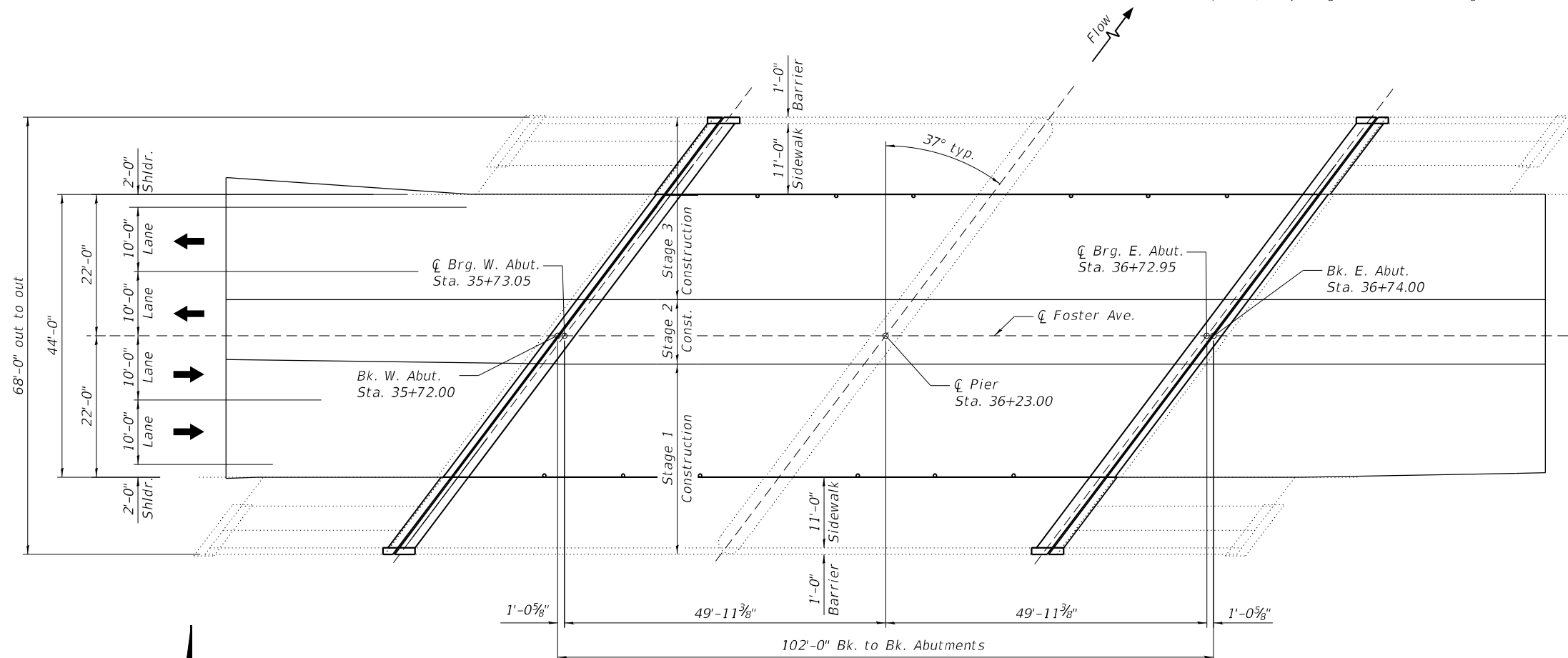
**SCOPE OF WORK**

1. Remove portions of existing concrete deck and sidewalks as required to replace expansion joints at abutments.
2. Perform ¾" scarification to top of existing bridge deck.
3. Complete concrete repairs to the approach pavements and sidewalks.
4. Provide new strip seal expansion joints and adjacent superstructure concrete over abutments.
5. Place 2¾" latex concrete overlay on bridge deck and asphalt overlay on the approach pavement.
6. Perform ¼" diamond grinding on new deck overlay.
7. Perform bridge deck grooving on new deck overlay and apply protective coat to new deck overlay and the top/inside surface of new sidewalk and barrier concrete.
8. Apply Concrete Sealer to top/inside surfaces of existing sidewalk and barrier surfaces, top surfaces of bridge seats, and backwall.
9. Perform concrete repairs on substructure units.
10. Perform removal and replacement of steel diaphragms as indicated.
11. Install stream gage to the east face of the pier.



**ELEVATION**

Note:  
Up to ¼" may be ground off the bridge deck overlay.



**PLAN**

**DESIGN SPECIFICATIONS**

(New Construction)  
2002 AASHTO Standard Specifications for Highway Bridges

**LOADING HS-20**

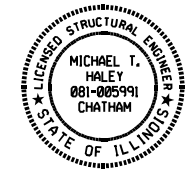
No allowance for future wearing surface.

**DESIGN STRESSES**

FIELD UNITS (New Construction)  
f<sub>c</sub> = 4,000 psi (Superstructure)  
f<sub>y</sub> = 60,000 psi (Reinforcement)  
f<sub>y</sub> = 36,000 psi (M270 Grade 36)

**INDEX OF SHEETS**

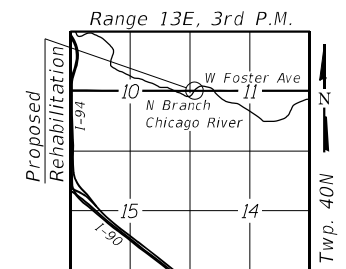
1. General Plan and Elevation
2. General Data
3. Stage Construction Details
4. Deck Slab Repair Plan
- 5-7. Joint Replacement Details
8. Preformed Joint Strip Seal
9. Structural Steel Details
10. Substructure Repair
11. Bar Splicer Assembly Details



Michael T. Haley  
Licensed Structural Engineer  
State of Illinois No. 081-005991  
Expires 11/30/2024

01/23/2024

Date



**LOCATION SKETCH**

**GENERAL PLAN AND ELEVATION  
FOSTER AVENUE OVER NORTH  
BRANCH CHICAGO RIVER (CENTER)  
F.A.U. RTE 1360 SECTION 2021-086-BDR&BJR  
COOK COUNTY  
STATION 36+23.00  
STRUCTURE NO. 016-0249**

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	CHECKED - CZ	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

SHEET 1 OF 11 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	29
CONTRACT NO. 62P69				

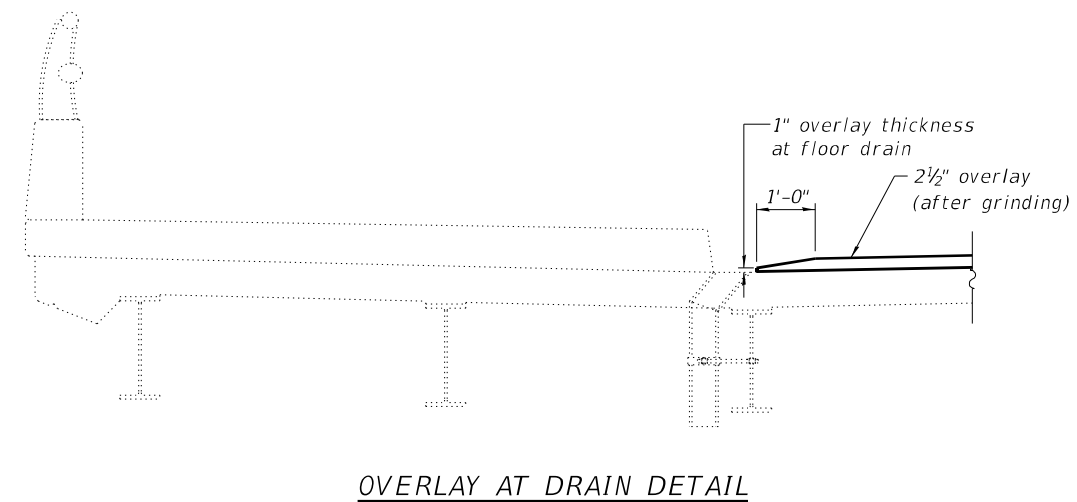
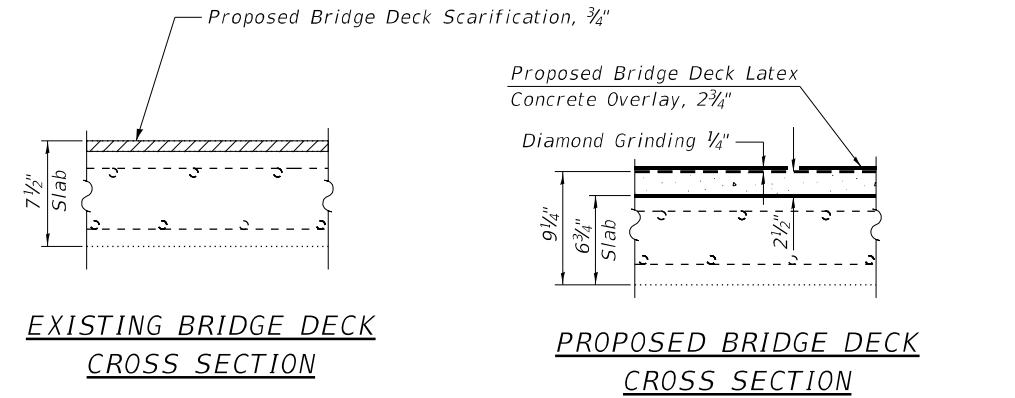
ILLINOIS FED. AID PROJECT

**GENERAL NOTES**

- Reinforcement bars designated (E) shall be epoxy coated.
- Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.  
  
As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer.  
  
Any cracks that cannot be removed by grinding 1/4 inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.
- Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
- Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
- Cost of removal and disposal of existing expansion joints shall be included in the cost of Concrete Removal.
- Protective Coat shall be applied to the top surface of new deck overlay and the inside and top faces of new concrete adjacent to joints.
- Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the concrete adjacent to joint is poured at an ambient temperature other than 50°F.
- Expansion joints shall be fabricated to conform to the existing cross slope of the bridge.
- Fasteners shall be ASTM F3125 Grade A325 Type 1, mechanically galvanized bolts. Bolts 7/8 in. Ø, holes 1 1/16 in. Ø, unless otherwise noted.
- Concrete Sealer shall be applied to the top of existing bridge seats, face of existing backwall, existing inside and top of parapets and sidewalk.
- The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.
- All structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted.
- Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the Special Provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures".
- All new structural steel and bearing assembly shall be hot-dip galvanized. See Special Provisions for "Hot Dip Galvanizing for Structural Steel".

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu. Yd.	28.0	-	28.0
Concrete Superstructure	Cu. Yd.	29.4	-	29.4
Protective Coat	Sq. Yd.	531	-	531
Furnishing And Erecting Structural Steel	Pound	1,580	-	1,580
Reinforcement Bars, Epoxy Coated	Pound	4,170	-	4,170
Bar Splicers	Each	56	-	56
Preformed Joint Strip Seal	Foot	166	-	166
Concrete Sealer	Sq. Ft.	4,661	-	4,661
Stream Gauge	Each	-	1	1
Bridge Deck Grooving (Longitudinal)	Sq. Yd.	448	-	448
Approach Slab Repair (Partial Depth)	Sq. Yd.	3	-	3
Structural Steel Removal	Pound	1,808	-	1,808
Bridge Deck Latex Concrete Overlay, 2 3/4 Inches	Sq. Yd.	473	-	473
Bridge Deck Scarification 3/4"	Sq. Yd.	473	-	473
Structural Repair Of Concrete (Depth Equal To Or Less Than 5 Inches)	Sq. Ft.	-	54	54
Diamond Grinding (Bridge Section)	Sq. Yd.	452	-	452
Deck Slab Repair (Full Depth, Type I)	Sq. Yd.	3	-	3
Bridge Sidewalk Repair (Partial Depth)	Sq. Ft.	116	-	116



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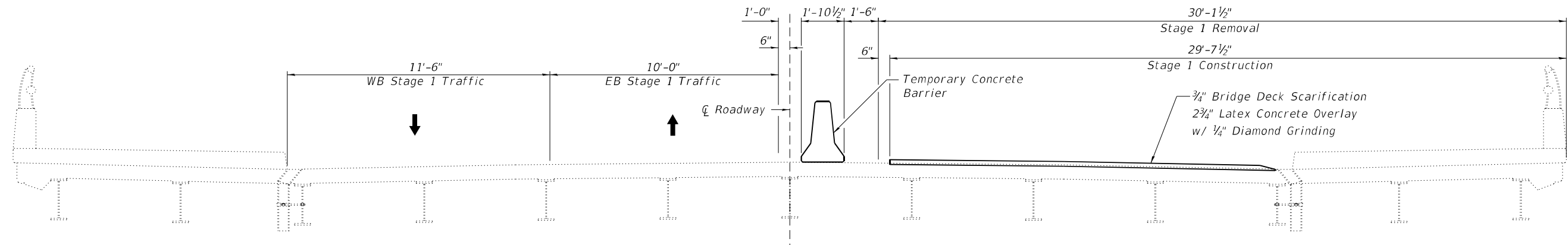
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

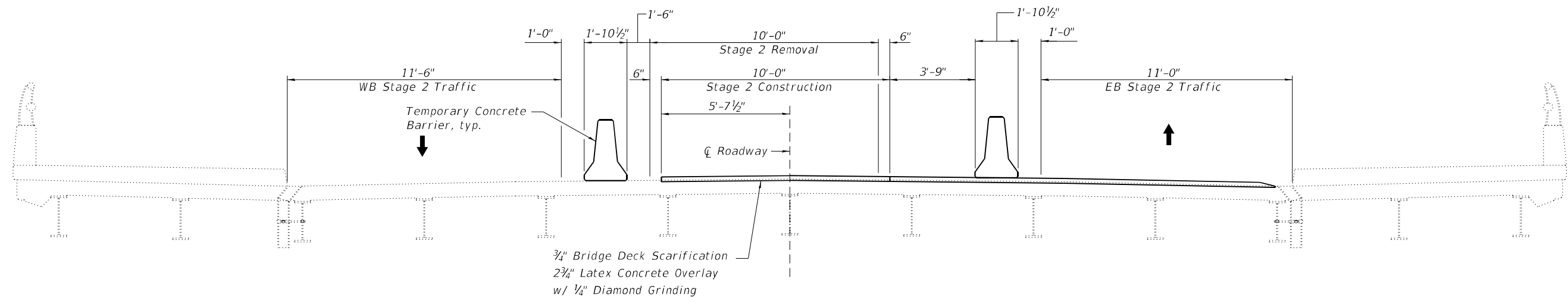
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STRUCTURE NO. 016-0249**

SHEET 2 OF 11 SHEETS

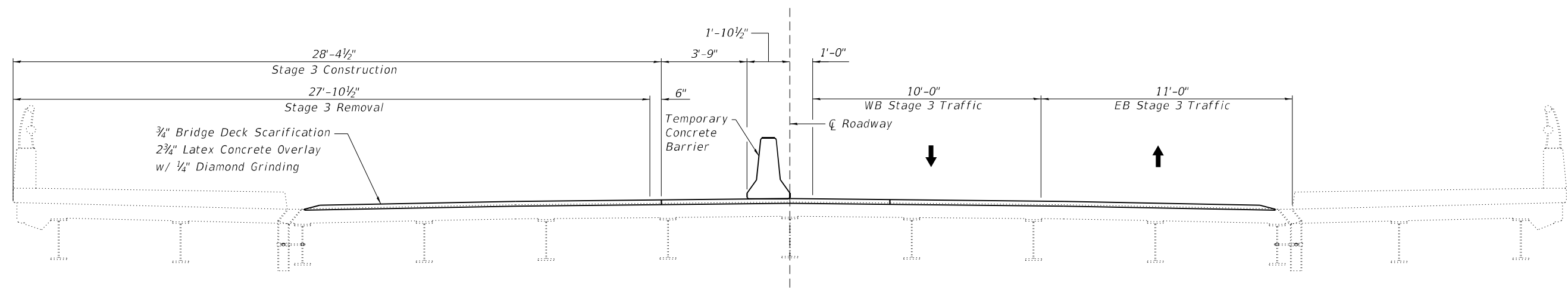
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	30
CONTRACT NO. 62P69				
ILLINOIS		FED. AID PROJECT		



**STAGE 1 REMOVAL AND CONSTRUCTION**



**STAGE 2 REMOVAL AND CONSTRUCTION**



**STAGE 3 REMOVAL AND CONSTRUCTION**

Notes:  
All sections are looking East.  
See Roadway plans for Temporary Concrete Barrier quantities.

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**LE** LIN ENGINEERING, LTD.  
Consulting Engineers  
Springfield, Illinois

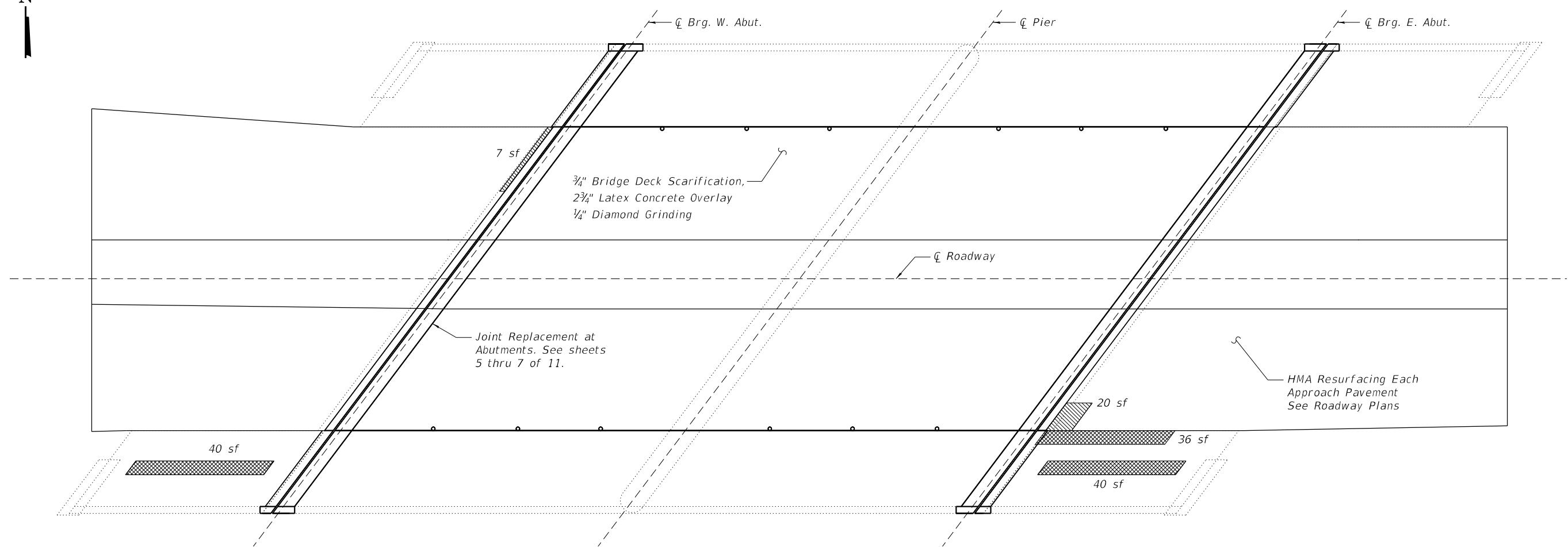
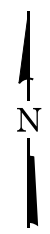
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

STAGE CONSTRUCTION DETAILS  
STRUCTURE NO. 016-0249

SHEET 3 OF 11 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	31
CONTRACT NO. 62P69				
ILLINOIS FED. AID PROJECT				



**DECK PLAN**

**LEGEND**

- Indicates Approach Slab Repair (Partial Depth)
- Bridge Sidewalk Repair (Partial Depth)
- sf - Square Feet

Notes:  
 Repair areas shown are estimated. The Engineer shall document actual locations of repairs on As-Built Plans.

**BILL OF MATERIAL**

ITEM	UNIT	TOTAL
Approach Slab Repair (Partial Depth)	Sq. Yd.	3
Bridge Sidewalk Repair (Partial Depth)	Sq. Ft	116
Deck Slab Repair (Full Depth, Type I)	Sq. Yd.	3

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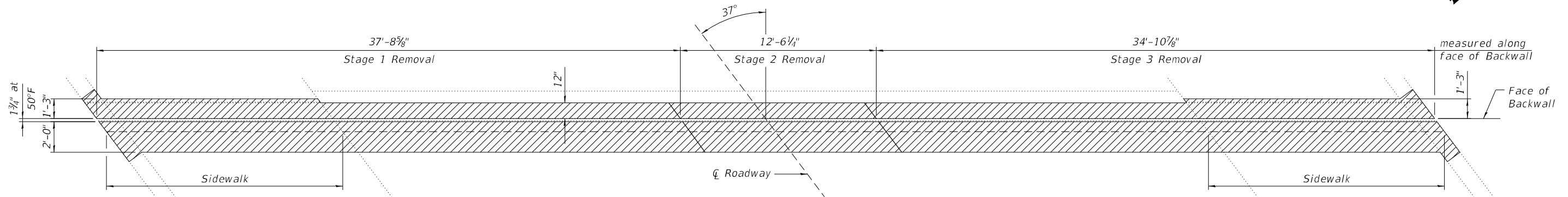
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

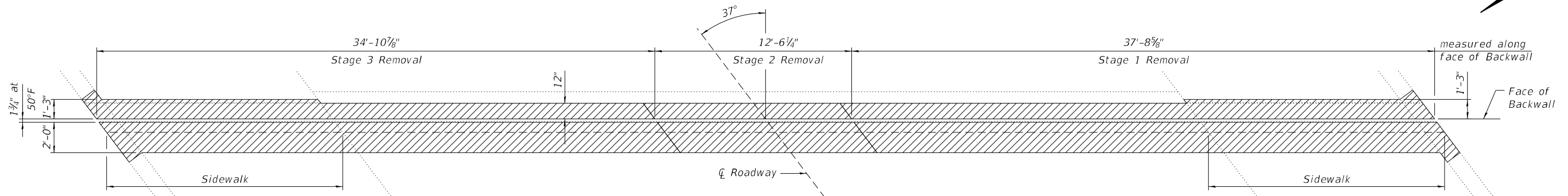
**DECK SLAB REPAIR PLAN  
 STRUCTURE NO. 016-0249**

SHEET 4 OF 11 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	32
CONTRACT NO. 62P69				
		ILLINOIS	FED. AID PROJECT	



JOINT REMOVAL AT WEST ABUTMENT



JOINT REMOVAL AT EAST ABUTMENT

Notes:  
 Hatched areas indicate limits of  
 Concrete Removal.  
 See Sheet 7 of 11 for Sections.

(Sheet 1 of 3)

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**LE** LIN ENGINEERING, LTD.  
 Consulting Engineers  
 Springfield, Illinois

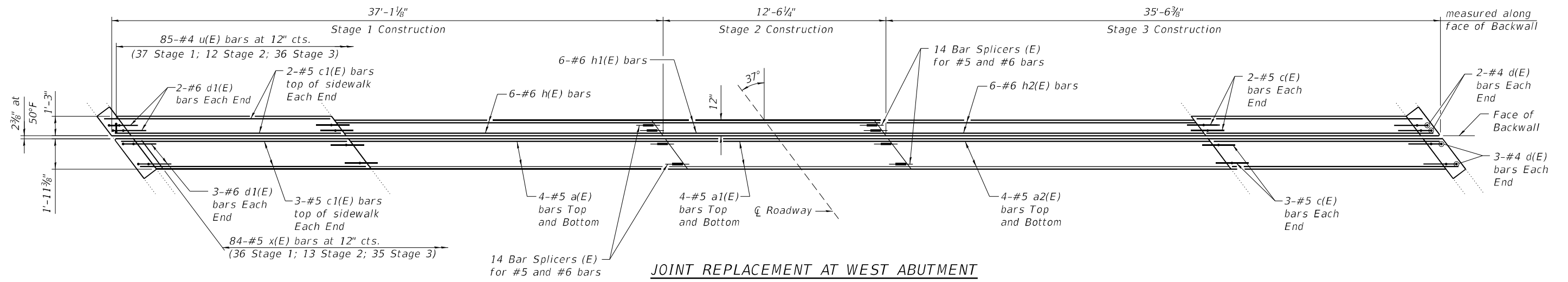
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

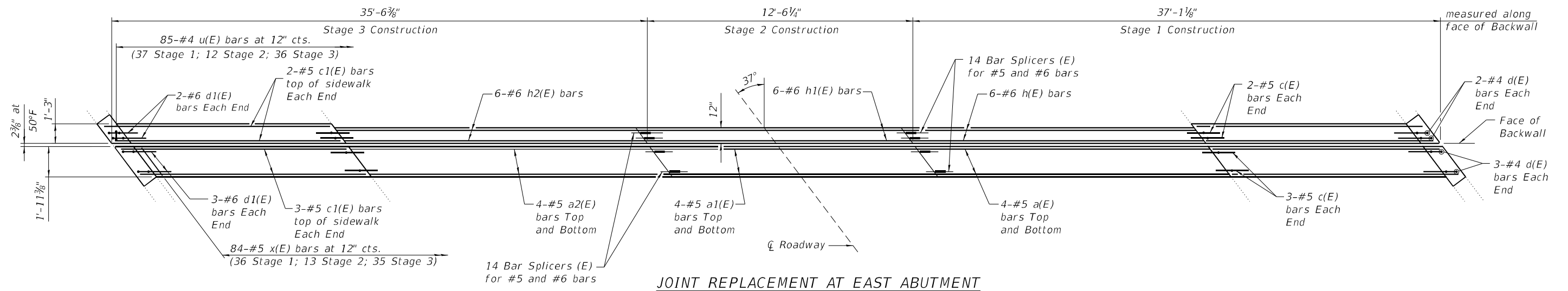
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 STRUCTURE NO. 016-0249**

SHEET 5 OF 11 SHEETS

F.A.U. RTE. 1360	SECTION 2021-086-BDR&BJR	COUNTY COOK	TOTAL SHEETS 60	SHEET NO. 33
CONTRACT NO. 62P69				
ILLINOIS FED. AID PROJECT				



**JOINT REPLACEMENT AT WEST ABUTMENT**



**JOINT REPLACEMENT AT EAST ABUTMENT**

(Sheet 2 of 3)

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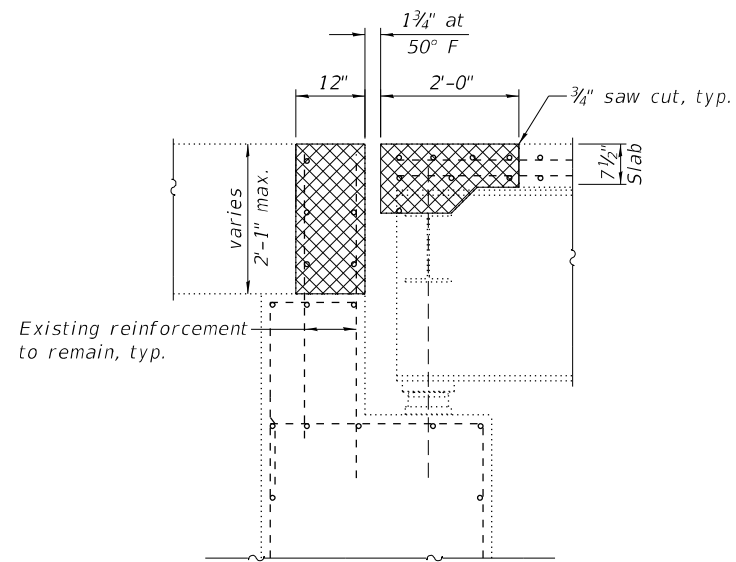
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

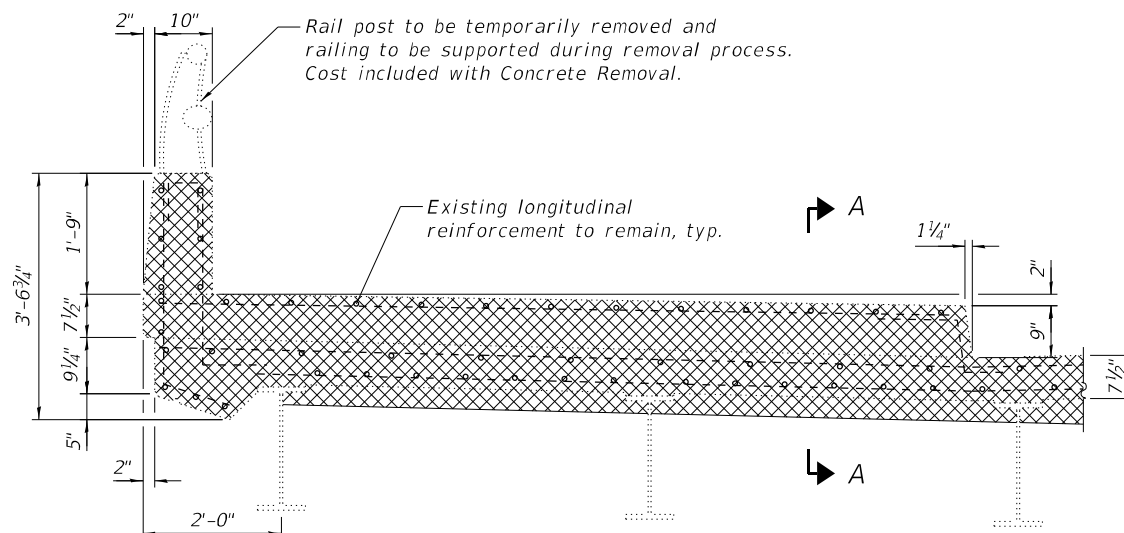
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STRUCTURE NO. 016-0249**

SHEET 6 OF 11 SHEETS

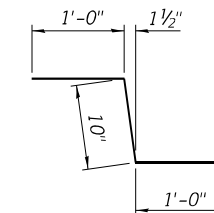
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CONTRACT NO. 62P69				
ILLINOIS FED. AID PROJECT				



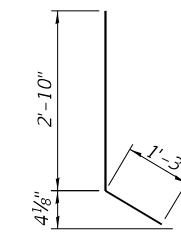
**SECTION THRU JOINT AT ABUTMENTS**  
(Showing Removal; Dimensions at right angles)



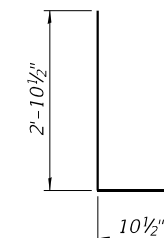
**SECTION THRU SIDEWALK AT JOINT**  
(Showing Removal)



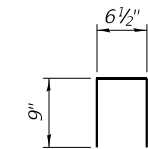
**BAR c(E)**



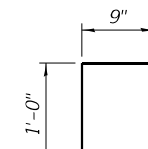
**BAR d(E)**



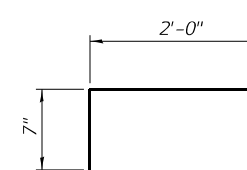
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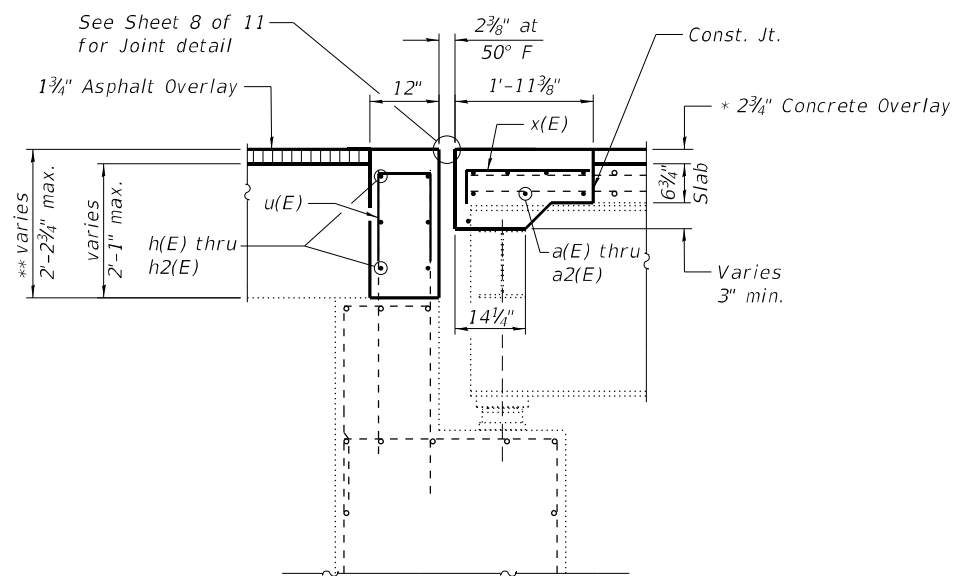
**BAR d2(E)**



**BAR u(E)**

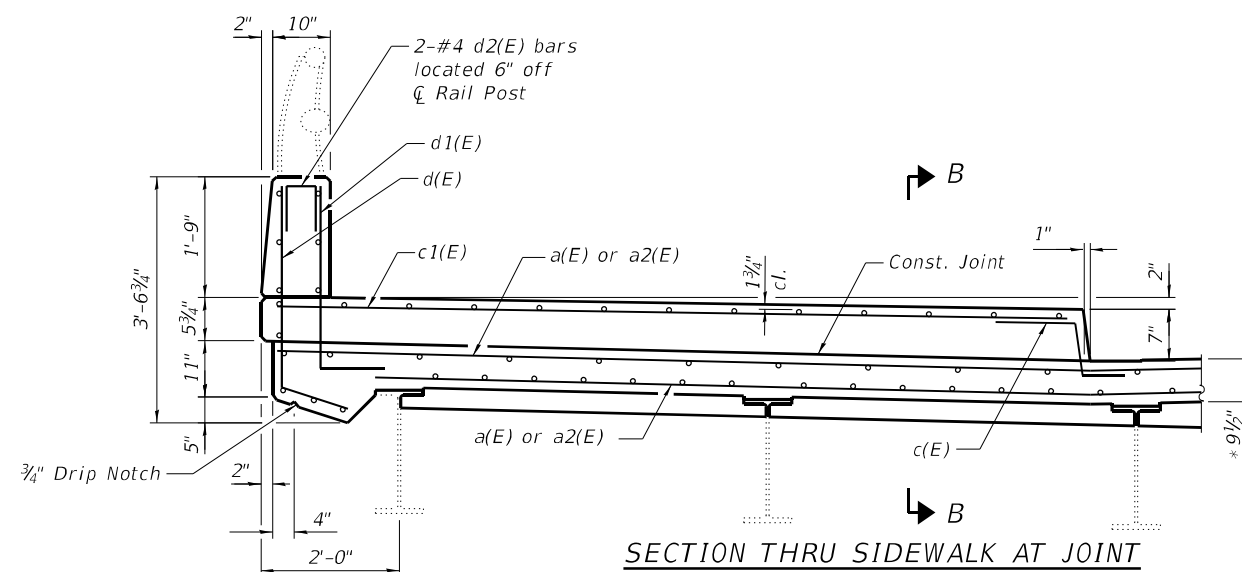


**BAR x(E)**

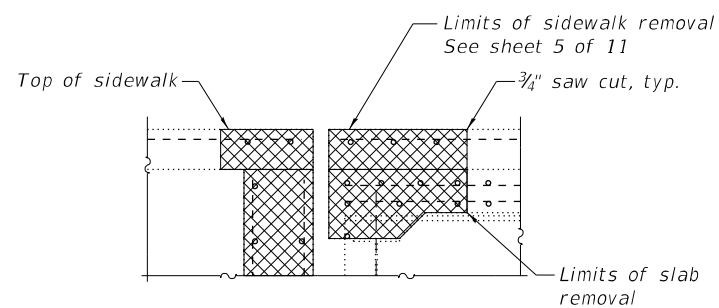


**SECTION THRU JOINT AT ABUTMENTS**  
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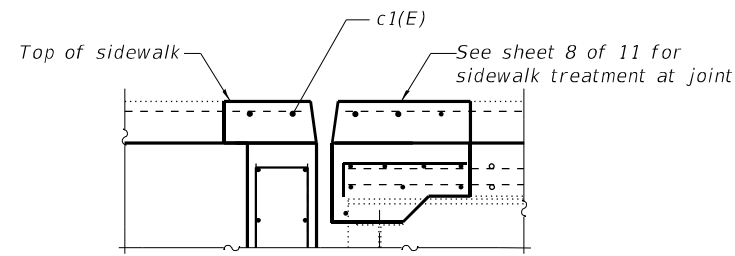
Note:  
Cross-hatched areas indicate limits of Concrete Removal.



**SECTION THRU SIDEWALK AT JOINT**  
(Showing Proposed)



**SECTION A-A**



**SECTION B-B**

**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a(E)	16	#5	36'-7"	—
a1(E)	16	#5	12'-3"	—
a2(E)	16	#5	35'-1"	—
c(E)	20	#5	2'-10"	⌋
c1(E)	20	#5	11'-9"	—
d(E)	20	#4	4'-1"	⌋
d1(E)	20	#6	3'-9"	⌋
d2(E)	8	#4	2'-1"	⌋
h(E)	12	#6	36'-7"	—
h1(E)	12	#6	12'-3"	—
h2(E)	12	#6	35'-1"	—
u(E)	170	#4	2'-9"	⌋
x(E)	168	#5	2'-7"	—
Concrete Removal			Cu. Yd.	28.0
Concrete Superstructure			Cu. Yd.	29.4
Reinforcement Bars, Epoxy Coated			Pound	4,170

(Sheet 3 of 3)

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**LE** LIN ENGINEERING, LTD.  
Consulting Engineers  
Springfield, Illinois

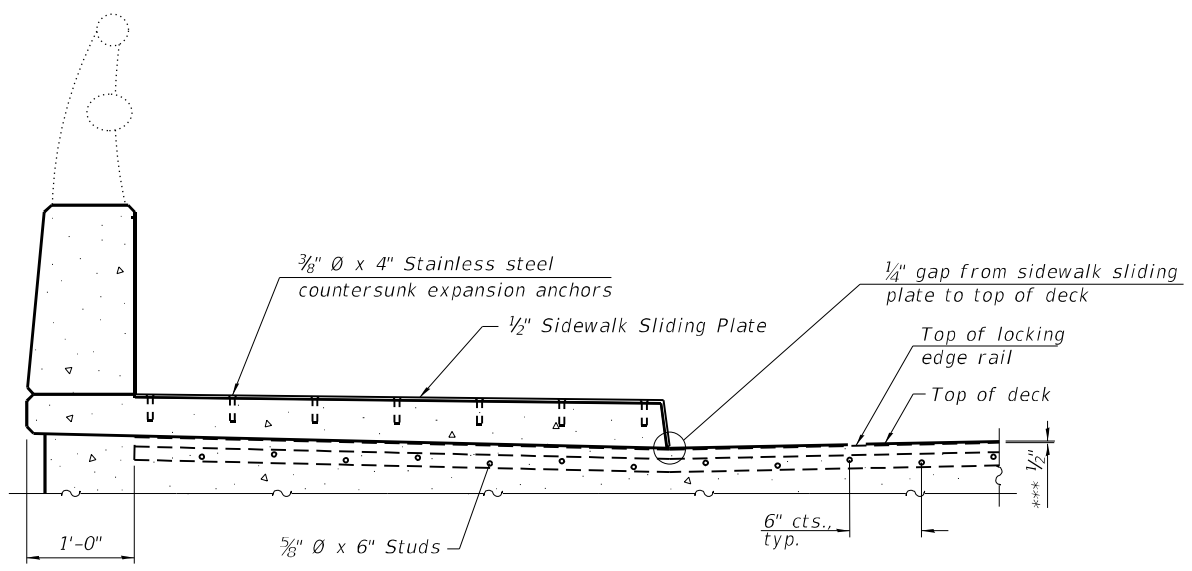
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**DEPARTMENT OF TRANSPORTATION**

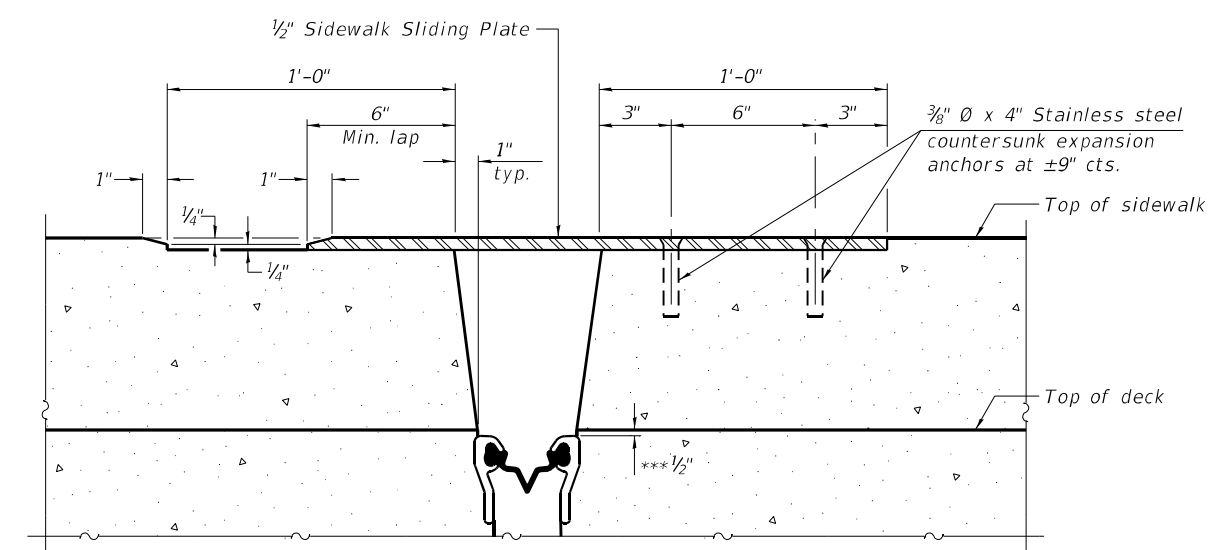
**JOINT REPLACEMENT DETAILS**  
**STRUCTURE NO. 016-0249**

SHEET 7 OF 11 SHEETS

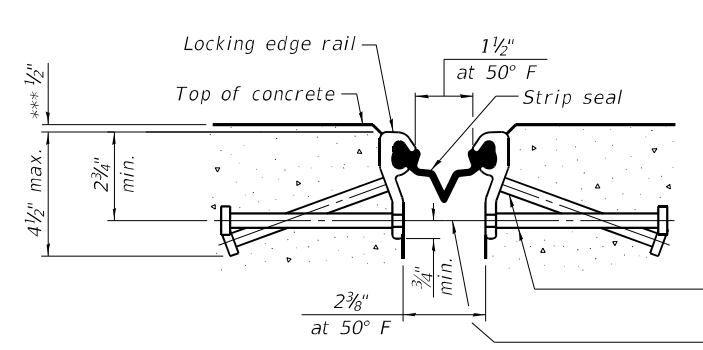
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1360	2021-086-BDR&BJR	COOK	60	35
CONTRACT NO. 62P69				
ILLINOIS FED. AID PROJECT				



**SECTION AT RAISED SIDEWALK**



**SECTION B-B**

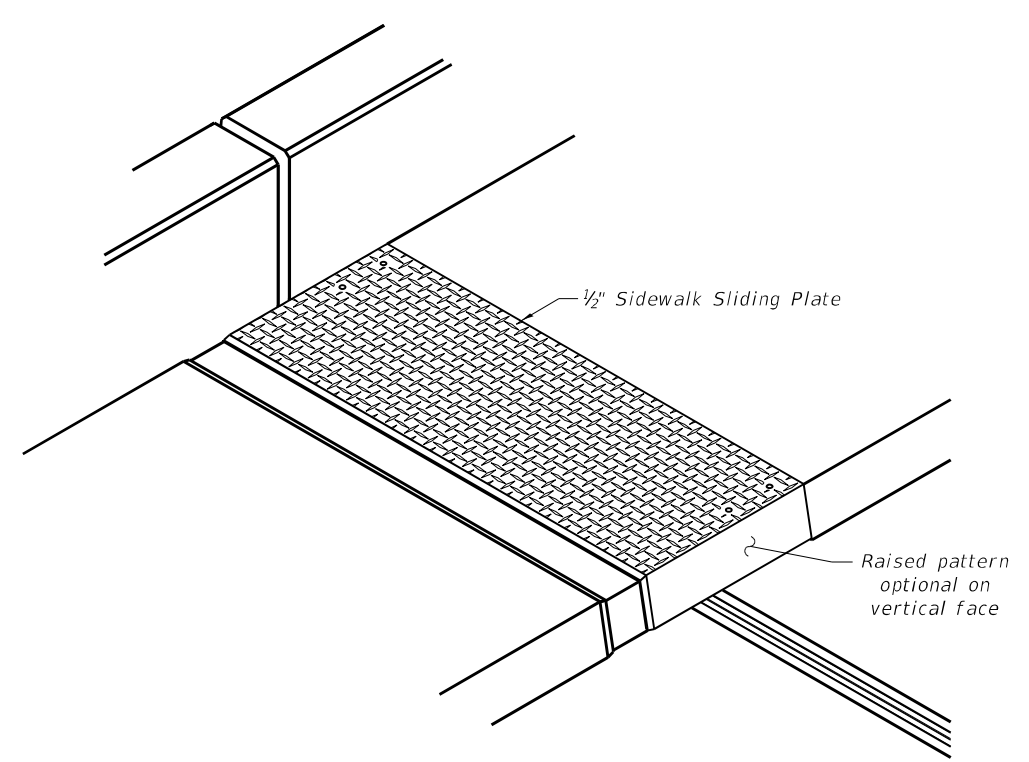


**SHOWING ROLLED RAIL JOINT**

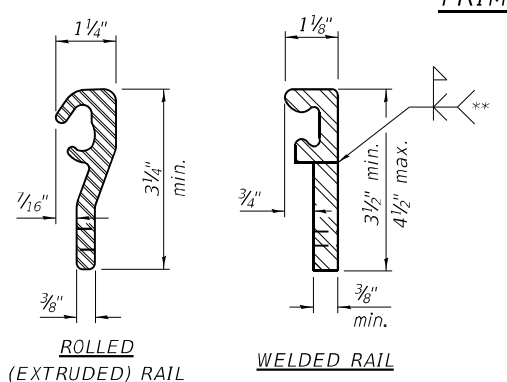
\*  $\frac{5}{8}$ "  $\phi$  x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)  
 $\frac{3}{8}$ "  $\phi$  threaded rods in  $\frac{1}{16}$ "  $\phi$  holes at  $\pm 4$ "-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

**SECTION A-A**

\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

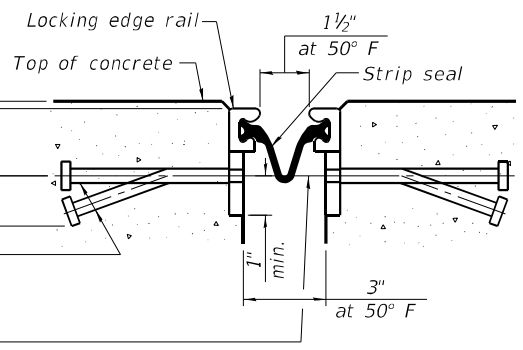


**TRIMETRIC VIEW**



**LOCKING EDGE RAILS**

\*\* Back gouge not required if complete joint penetration is verified by mock-up.

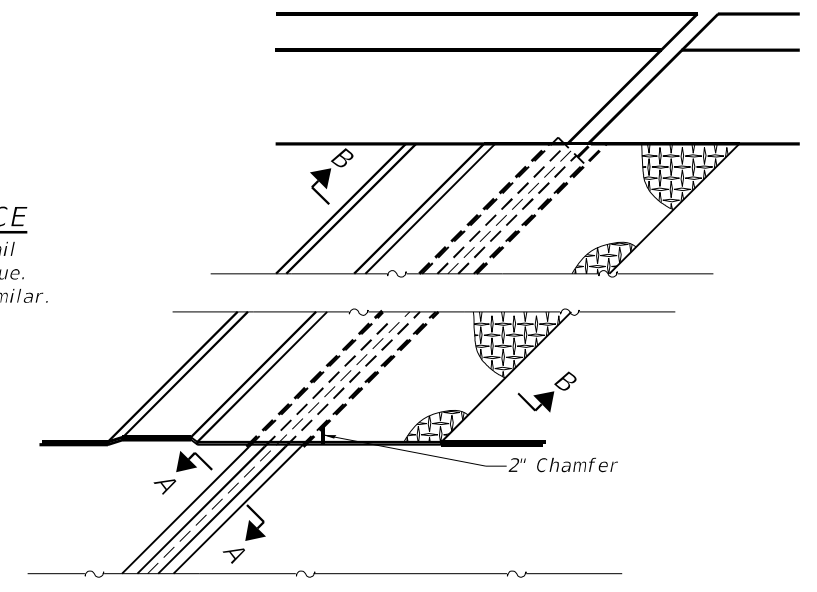


**SHOWING WELDED RAIL JOINT**

**LOCKING EDGE RAIL SPLICE**

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

**Notes:**  
 The strip seal shall be made continuous and shall have a minimum thickness of  $\frac{1}{4}$ ". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.  
 The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the  $4\frac{1}{2}$ " maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.  
 The manufacturer's recommended installation methods shall be followed.  
 All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.  
 The Maximum space between locking edge rail segments shall be  $\frac{3}{16}$ " and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.  
 The top surface of sidewalk sliding plates shall have a raised pattern according to ASTM A786.  
 Cost of sidewalk sliding plates, anchorage studs, and expansion anchors included with Preformed Joint Strip Seal.  
 The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required.



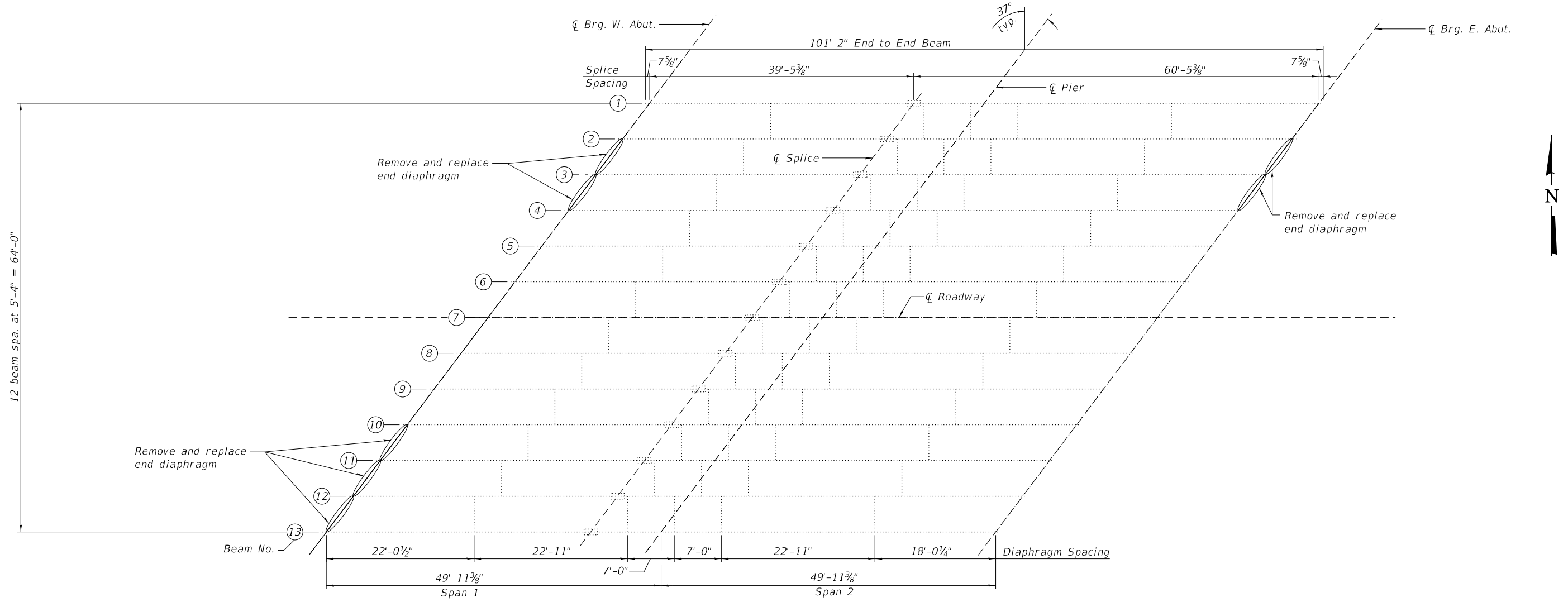
**PLAN AT RAISED SIDEWALK**

**BILL OF MATERIAL**

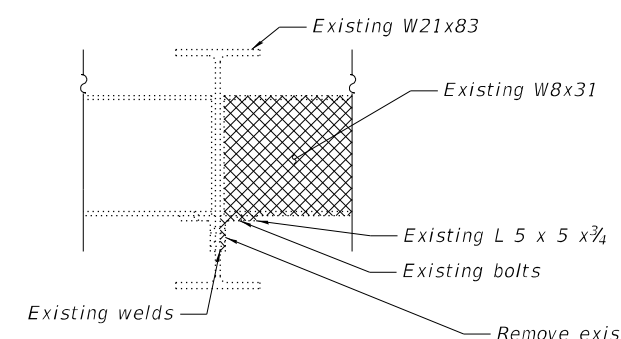
Item	Unit	Total
Preformed Joint Strip Seal	Foot	166

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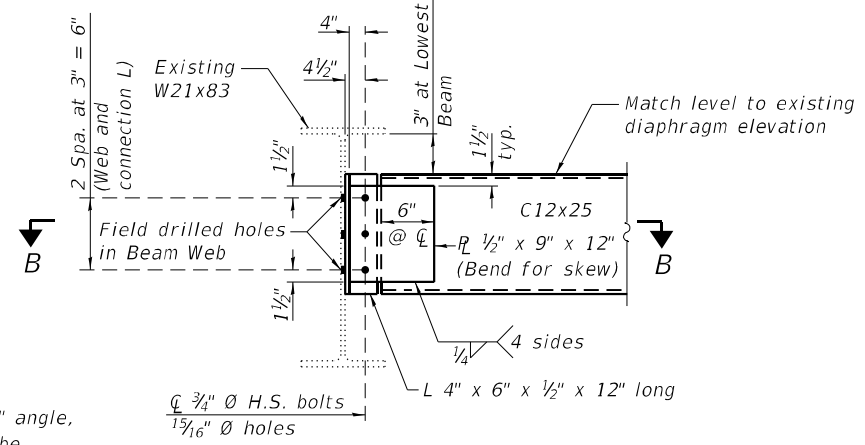


**FRAMING PLAN**



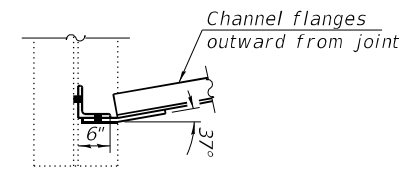
**DIAPHRAGM REMOVAL DETAIL**

Remove existing 5" x 5" x 3/4" angle, each end. Existing angle to be removed using the air-arc method and grind smooth all weld material remaining on the web. Cost included with Structural Steel Removal.



**NEW END DIAPHRAGM**

Note:  
Two hardened washers required for each set of oversized holes.



**SECTION B-B**

Notes:  
Cost of new end diaphragms included with Furnishing and Erecting Structural Steel.  
Cross-hatch areas indicate limits of diaphragm removal.

**BILL OF MATERIAL**

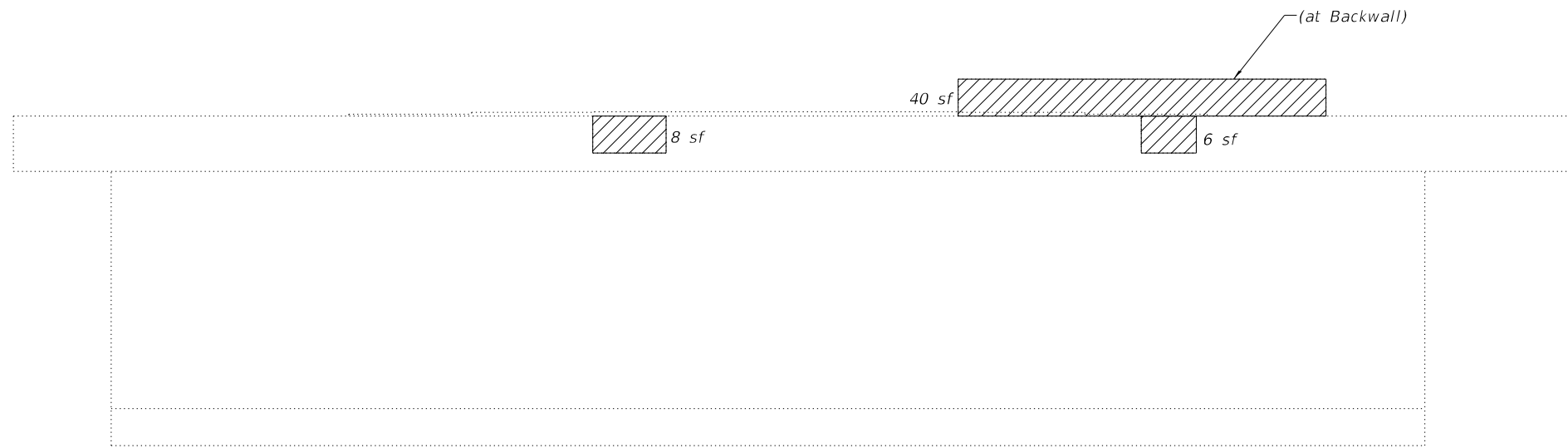
Item	Unit	Total
Furnishing and Erecting Structural Steel	Pound	1,580
Structural Steel Removal	Pound	1,808

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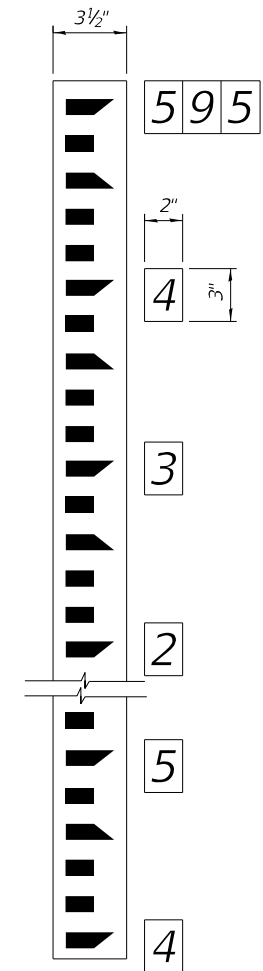


Stream Gauge, see detail on this sheet.

**EAST FACE OF PIER**  
(Looking West)



**WEST ABUTMENT**  
(Looking West)



**STREAM GAUGE DETAIL**

**Stream Gauge Notes:**

The gauge plates shall be porcelain enameled iron plate graduated in feet and tenths, unnumbered, and 3 1/2" wide. Gauge plates shall be Watermark Style "E" or approved equivalent.

Each individual number plate should be a black numeral on 2" X 3" white porcelain enameled iron plate. Number plates shall be "Watermark" Style "E" or approved equivalent.

Both the gauge plates and number plates shall be fastened directly to the pier with a 1/4" diameter, 1 1/2" long masonry screw with a hex washer head.

Three digit elevations to be installed at the top of the gauge and at every elevation ending with 0. At all of the other whole elevations, place the last digit as shown in the example to the top left.

**LEGEND**

Structural Repair of Concrete (Depth equal to or less than 5 inches)

sf Square Feet

**Note:**

Repair of the existing piers and abutments shall include but may not be limited to the areas shown. The actual area to be repaired will be determined by the Engineer at the time of construction.

**BILL OF MATERIAL**

Item	Unit	Total
Stream Gauge	Each	1
Structural Repair of Concrete (Depth equal to or less than 5 inches)	Sq. Ft.	54

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Consulting Engineers  
Springfield, Illinois

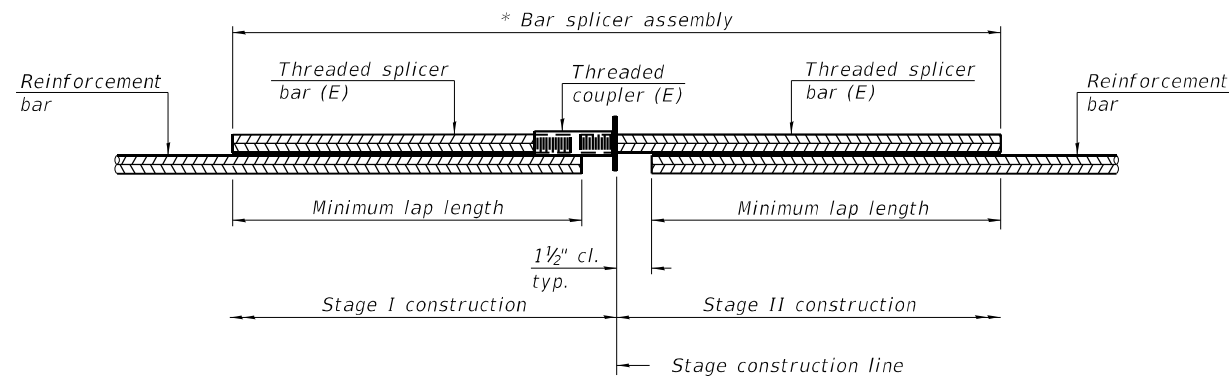
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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**SUBSTRUCTURE REPAIR**  
**STRUCTURE NO. 016-0249**

SHEET 10 OF 11 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	38
CONTRACT NO. 62P69				
ILLINOIS FED. AID PROJECT				

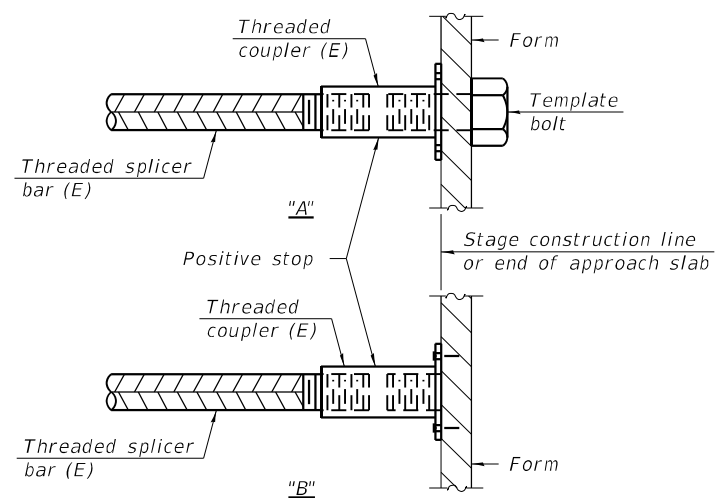


**STANDARD BAR SPLICER ASSEMBLY PLAN**  
 (All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

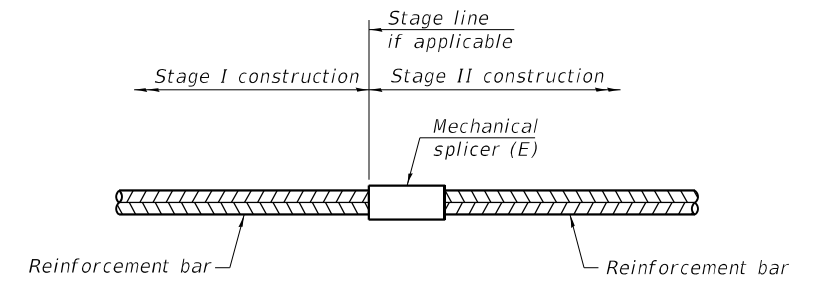
\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Deck	#5	32	3'-6"
Abutment	#6	24	4'-0"



**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.  
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
 (E) : Indicates epoxy coating.



**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required

Notes:  
 Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.  
 All reinforcement shall be lapped and tied to the splicer bars.  
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.  
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICE DETAILS  
 STRUCTURE NO. 016-0249

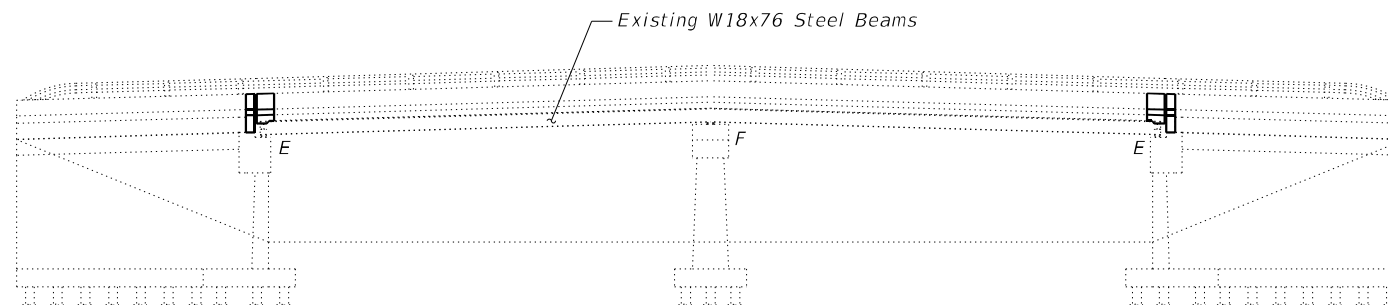
SHEET 11 OF 11 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	39
CONTRACT NO. 62P69				
ILLINOIS FED. AID PROJECT				

Existing Structure: SN 016-0250 built in 1933 as SBI Rte. 21, Section 21BZ-1 at Sta. 45+06. Bridge reconstruction in 1985 included superstructure replacement and substructure modifications. The structure is a two span bridge with 7½" deck on rolled beams, measuring 102'-0" back to back abutments, 67'-0" out to out deck with a 37°00'00" right ahead skew. The concrete substructure units are comprised of closed abutments and a solid wall pier, all founded on untreated timber piles. Stage construction will be utilized to maintain one lane of traffic in each direction.

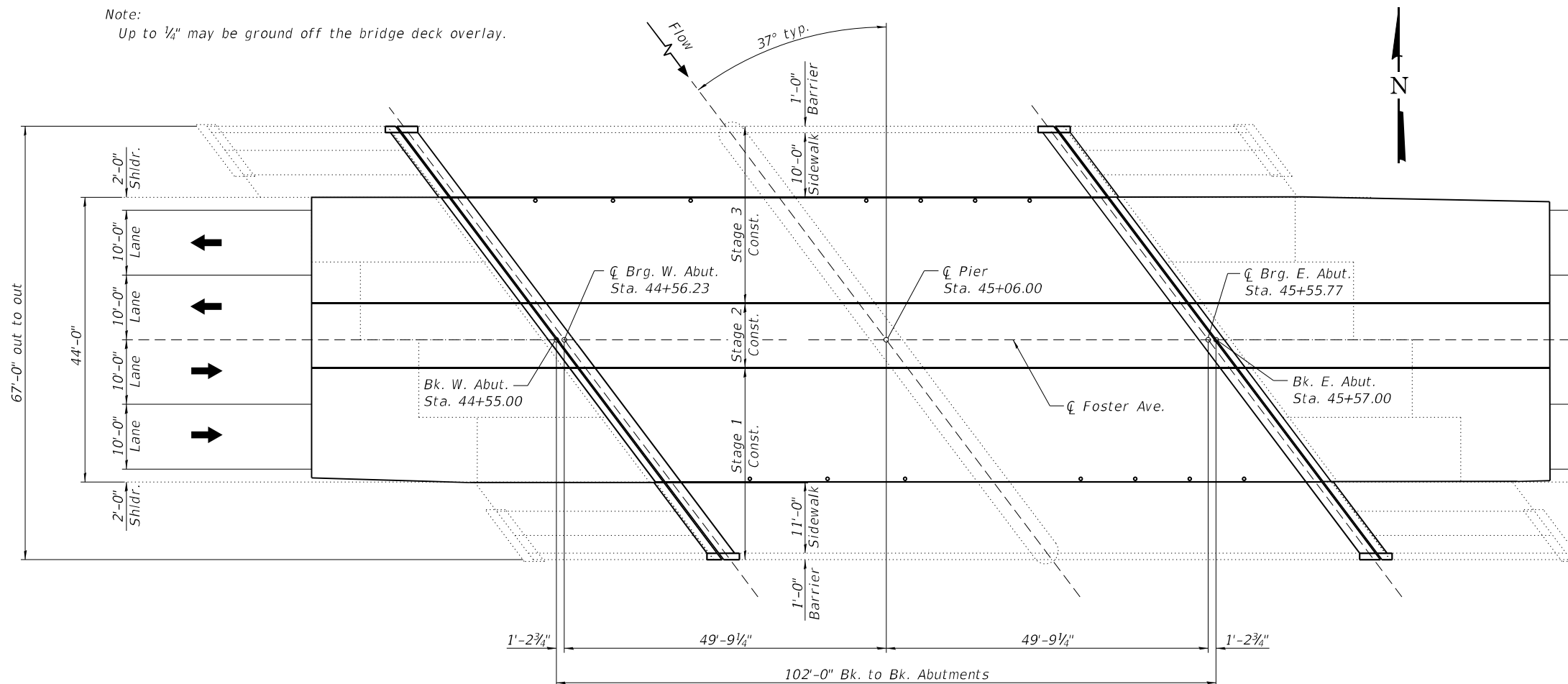
**SCOPE OF WORK**

1. Remove portions of existing concrete deck and sidewalks as required to replace expansion joints at abutments.
2. Perform ¾" scarification to top of existing bridge deck.
3. Complete concrete repairs to the approach pavements and sidewalks.
4. Provide new strip seal expansion joints and adjacent superstructure concrete over abutments.
5. Place 2¾" latex concrete overlay on bridge deck and asphalt overlay on the approach pavement.
6. Perform ¼" diamond grinding on new deck overlay.
7. Perform bridge deck grooving on new deck overlay and apply protective coat to new deck overlay and the top/inside surface of new sidewalk and barrier concrete.
8. Apply Concrete Sealer to top/inside surfaces of existing sidewalk and barrier surfaces, top surfaces of bridge seats, and backwall.
9. Perform concrete repairs on east abutment utilizing temporary shoring of existing beams.
10. Perform removal and replacement of steel diaphragms as indicated.
11. Install stream gage to the face of the east abutment.



**ELEVATION**

Note:  
Up to ¼" may be ground off the bridge deck overlay.



**PLAN**

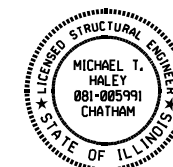
**LOADING HS-20**  
No allowance for future wearing surface.

**DESIGN STRESSES**  
**FIELD UNITS (NEW CONSTRUCTION)**  
f'c = 4,000 psi (Superstructure)  
fy = 60,000 psi (Reinforcement)  
fy = 36,000 psi (M270 Grade 36)

**DESIGN SPECIFICATIONS**  
(New Construction)  
2002 AASHTO Standard Specifications  
for Highway Bridges

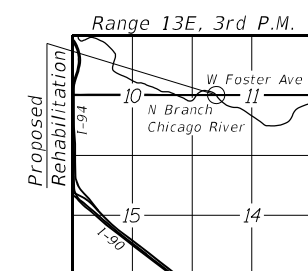
**INDEX OF SHEETS**

1. General Plan and Elevation
2. General Data
3. Stage Construction Details
4. Deck Slab Repair Plan
- 5-7. Joint Replacement Details
8. Preformed Joint Strip Seal
9. Structural Steel Details
10. Substructure Repair
11. Bar Splicer Assembly Details



Michael T. Haley  
Licensed Structural Engineer  
State of Illinois No. 081-005991  
Expires 11/30/2024

01/23/2024  
Date



**LOCATION SKETCH**

**GENERAL PLAN AND ELEVATION**  
**FOSTER AVENUE OVER NORTH**  
**BRANCH CHICAGO RIVER (EAST)**  
**F.A.U. RTE. 1360 SECTION 2021-086-BDR&BJR**  
**COOK COUNTY**  
**STATION 45+06.00**  
**STRUCTURE NO. 016-0250**

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SHEET 1 OF 11 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62P69				

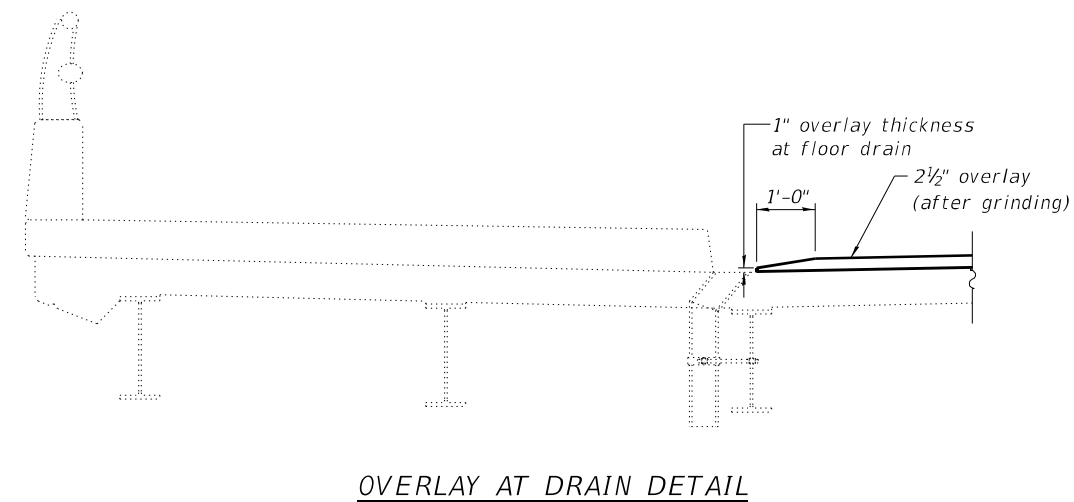
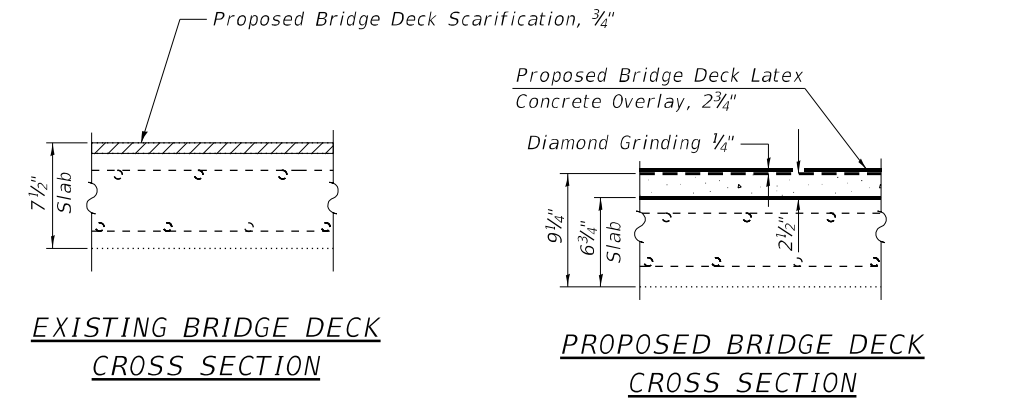
ILLINOIS FED. AID PROJECT

**GENERAL NOTES**

- Reinforcement bars designated (E) shall be epoxy coated.
- Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.  
  
As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer.  
  
Any cracks that cannot be removed by grinding 1/4 inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.
- Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
- Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
- Cost of removal and disposal of existing expansion joints shall be included in the cost of Concrete Removal.
- Protective Coat shall be applied to the top surface of new deck overlay and the inside and top faces of new concrete adjacent to joints.
- Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the concrete adjacent to joint is poured at an ambient temperature other than 50°F.
- Expansion joints shall be fabricated to conform to the existing cross slope of the bridge.
- Fasteners shall be ASTM F3125 Grade A325 Type 1, mechanically galvanized bolts. Bolts 7/8 in. Ø, holes 1 1/16 in. Ø, unless otherwise noted.
- Concrete Sealer shall be applied to the top of existing bridge seats, face of existing backwall, existing inside and top of parapets and sidewalk.
- The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.
- All structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted.
- Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the Special Provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures".
- All new structural steel and bearing assembly shall be hot-dip galvanized. See Special Provisions for "Hot Dip Galvanizing for Structural Steel".

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu. Yd.	24.9	-	24.9
Concrete Superstructure	Cu. Yd.	26.3	-	26.3
Protective Coat	Sq. Yd.	531	-	531
Furnishing And Erecting Structural Steel	Pound	2,126	-	2,126
Reinforcement Bars, Epoxy Coated	Pound	4,110	-	4,110
Bar Splicers	Each	56	-	56
Preformed Joint Strip Seal	Foot	164	-	164
Concrete Sealer	Sq. Ft.	4,504	-	4,504
Stream Gauge	Each	-	1	1
Bridge Deck Grooving (Longitudinal)	Sq. Yd.	448	-	448
Approach Slab Repair (Partial Depth)	Sq. Yd.	4	-	4
Structural Steel Removal	Pound	2,206	-	2,206
Bridge Deck Latex Concrete Overlay, 2 3/4 Inches	Sq. Yd.	473	-	473
Bridge Deck Scarification 3/4"	Sq. Yd.	473	-	473
Structural Repair Of Concrete (Depth Equal To Or Less Than 5 Inches)	Sq. Ft.	-	99	99
Diamond Grinding (Bridge Section)	Sq. Yd.	452	-	452
Temporary Shoring and Cribbing	Each	3	-	3
Deck Slab Repair (Full Depth, Type I)	Sq. Yd.	3	-	3
Bridge Sidewalk Repair (Partial Depth)	Sq. Ft.	5	-	5



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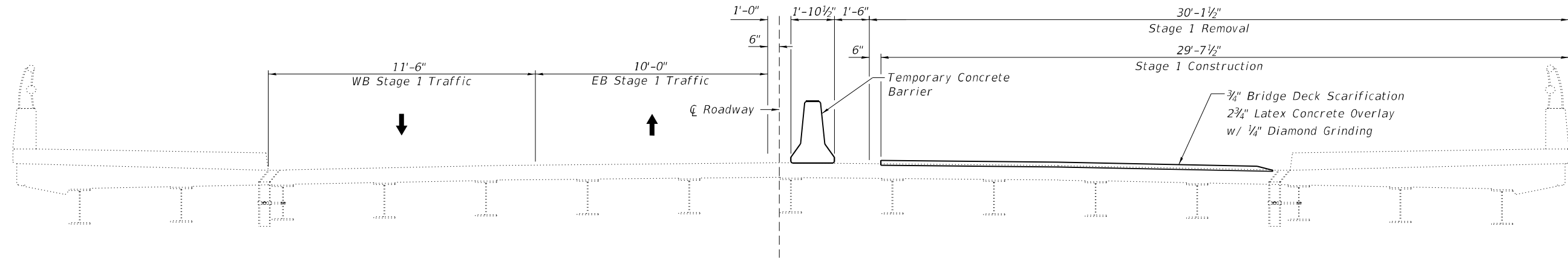
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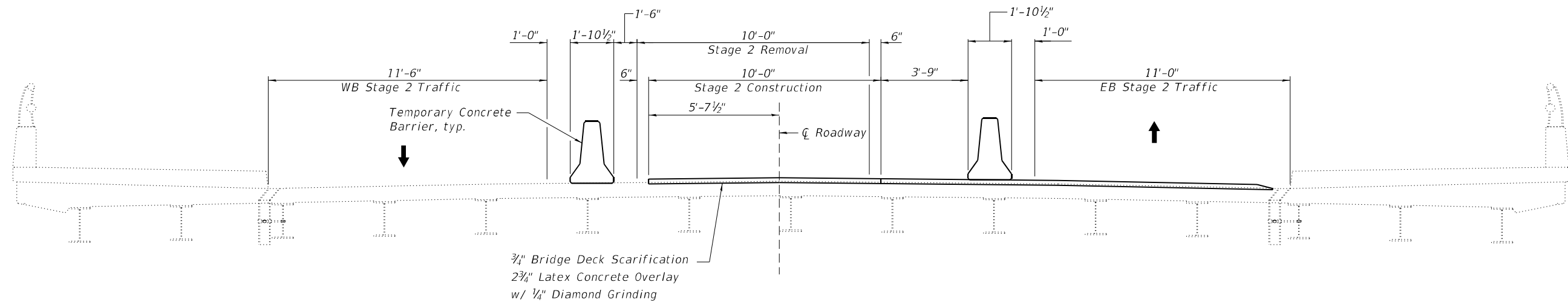
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STRUCTURE NO. 016-0250**

SHEET 2 OF 11 SHEETS

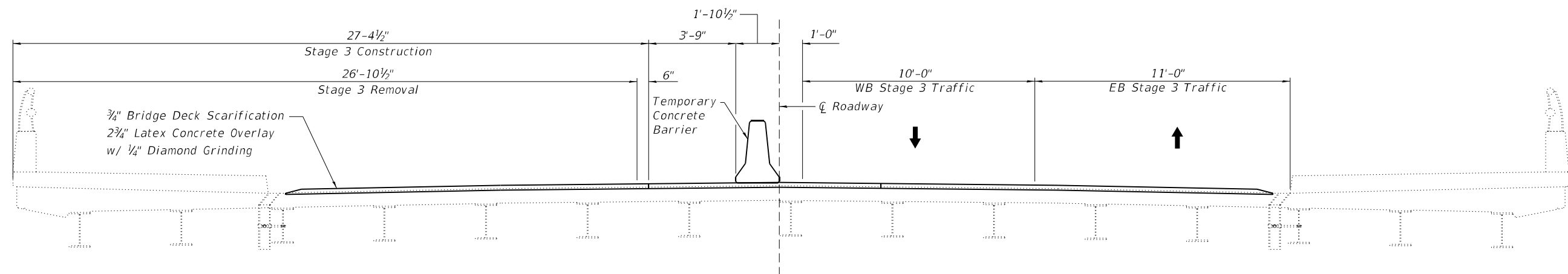
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1360	2021-086-BDR&BJR	COOK	60	41
CONTRACT NO. 62P69				
ILLINOIS FED. AID PROJECT				



**STAGE 1 REMOVAL AND CONSTRUCTION**



**STAGE 2 REMOVAL AND CONSTRUCTION**



**STAGE 3 REMOVAL AND CONSTRUCTION**

Notes:  
 All sections are looking East.  
 See Roadway Plans for Temporary Concrete Barrier quantities.

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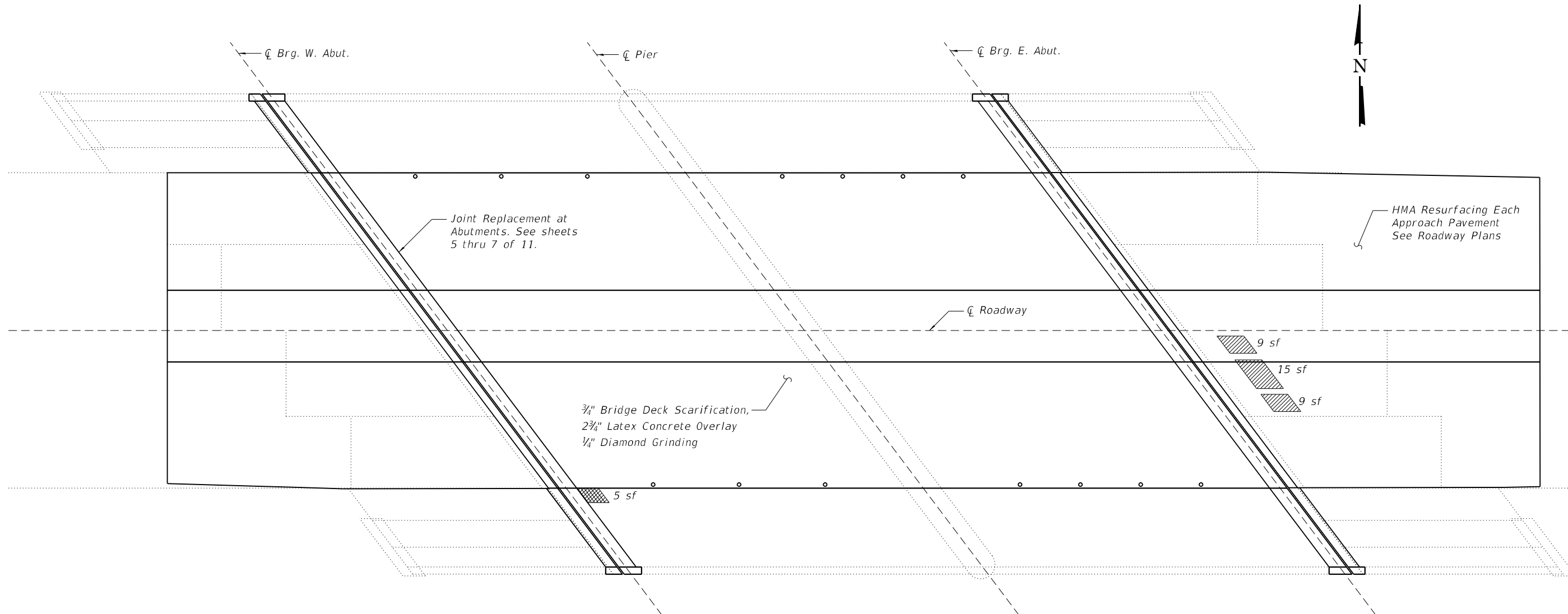
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 DEPARTMENT OF TRANSPORTATION

STAGE CONSTRUCTION DETAILS  
 STRUCTURE NO. 016-0250

SHEET 3 OF 11 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	42
CONTRACT NO. 62P69				

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DECK PLAN

LEGEND

Indicates Approach Slab Repair (Partial Depth)

Bridge Sidewalk Repair (Partial Depth)

sf - Square Feet

Notes:  
 Repair areas shown are estimated. The Engineer shall document actual locations of repairs on As-Built Plans.

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Approach Slab Repair (Partial Depth)	Sq. Yd.	4
Bridge Sidewalk Repair (Partial Depth)	Sq. Ft	5
Deck Slab Repair (Full Depth, Type I)	Sq. Yd.	3

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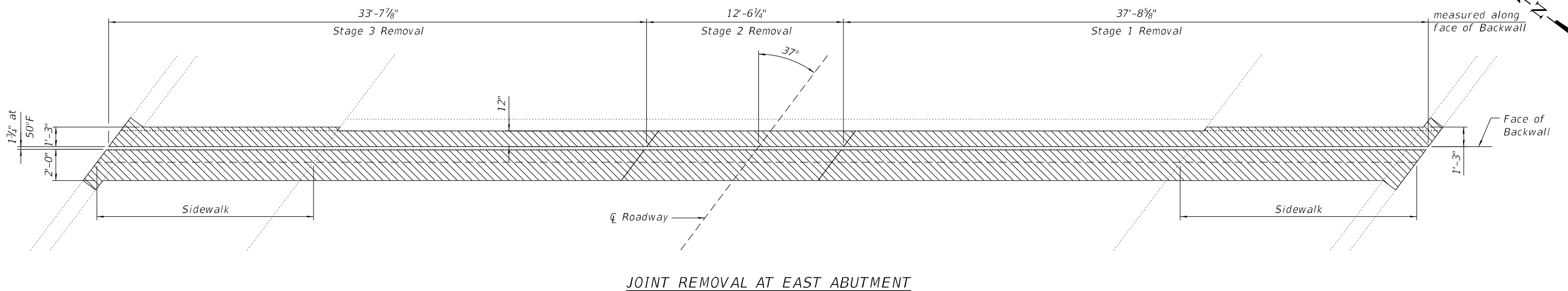
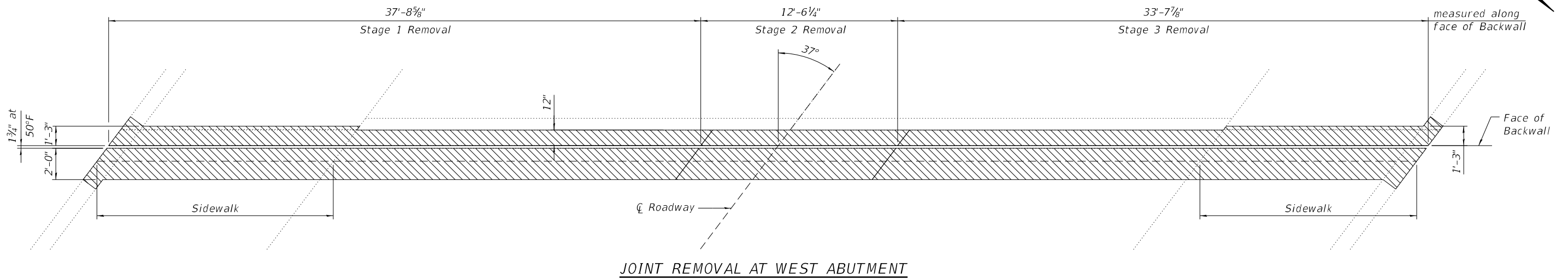
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DECK SLAB REPAIR PLAN  
 STRUCTURE NO. 016-0250

SHEET 4 OF 11 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	43
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62P69	



Notes:  
 Hatched areas indicate limits of  
 Concrete Removal.  
 See Sheet 7 of 11 for Sections.

(Sheet 1 of 3)

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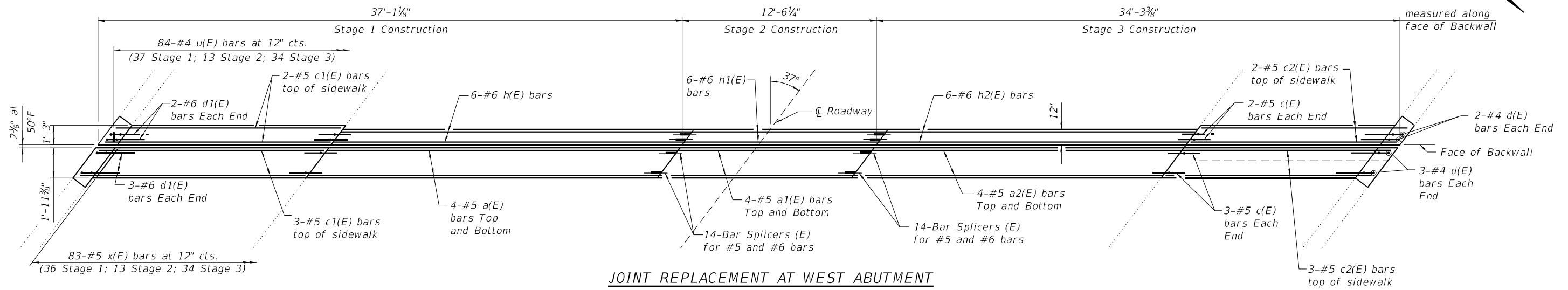
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**DEPARTMENT OF TRANSPORTATION**

**JOINT REPLACEMENT DETAILS**  
**STRUCTURE NO. 016-0250**

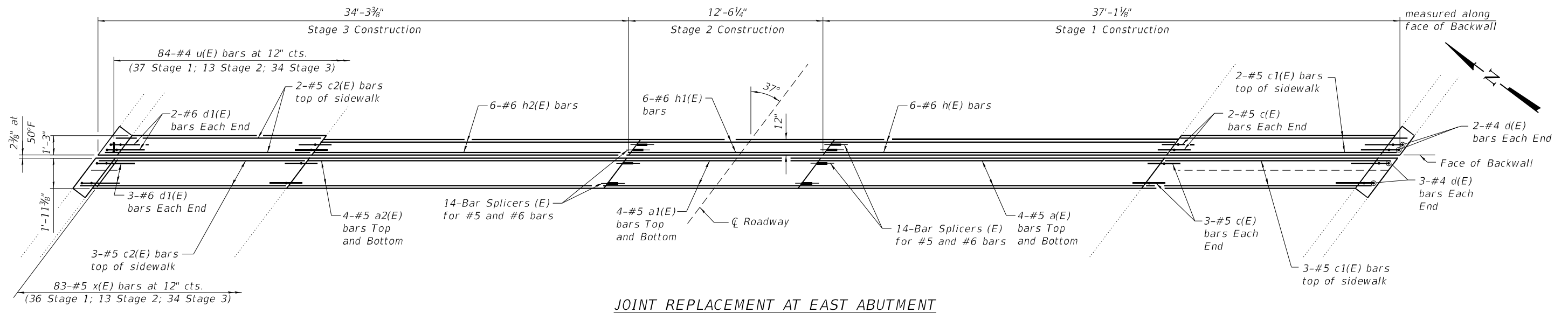
SHEET 5 OF 11 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	44
CONTRACT NO. 62P69				
ILLINOIS FED. AID PROJECT				





**JOINT REPLACEMENT AT WEST ABUTMENT**



**JOINT REPLACEMENT AT EAST ABUTMENT**

(Sheet 2 of 3)

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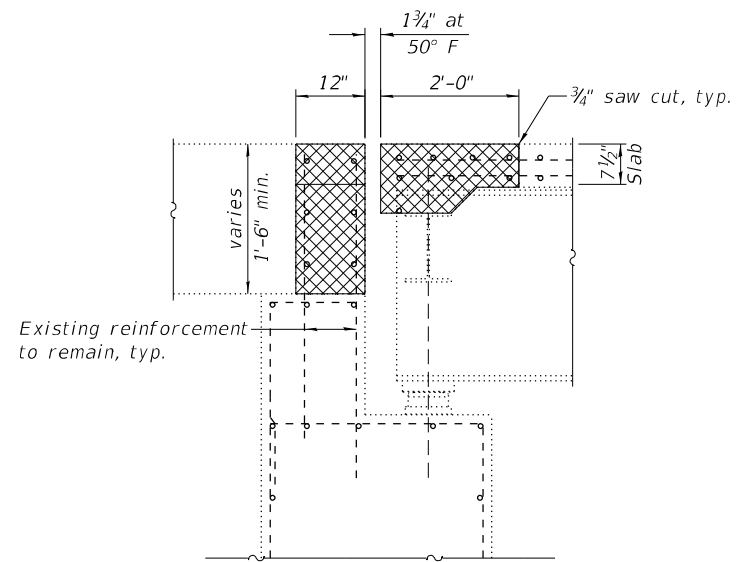
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 DEPARTMENT OF TRANSPORTATION**

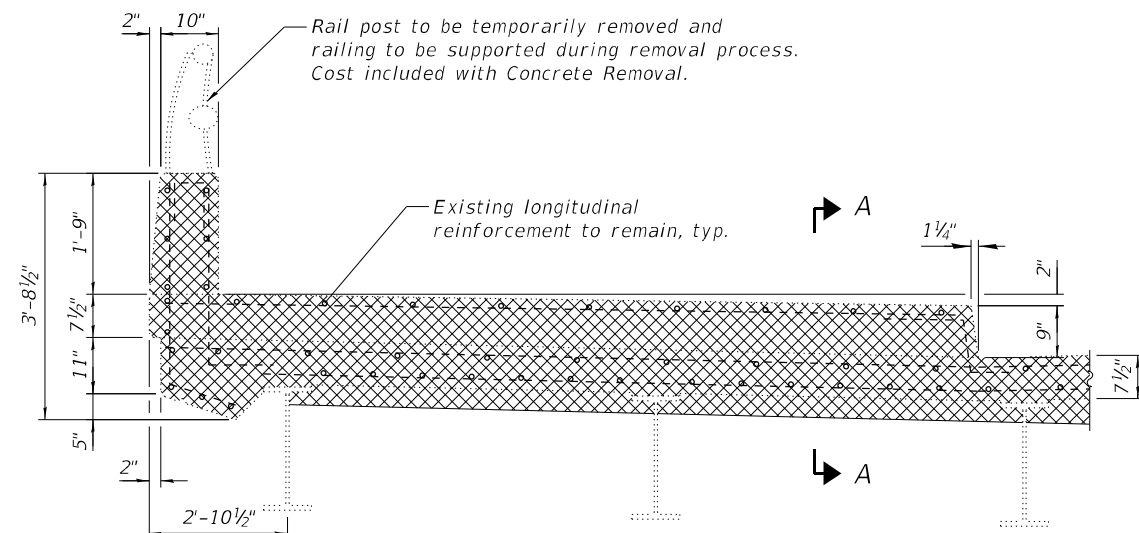
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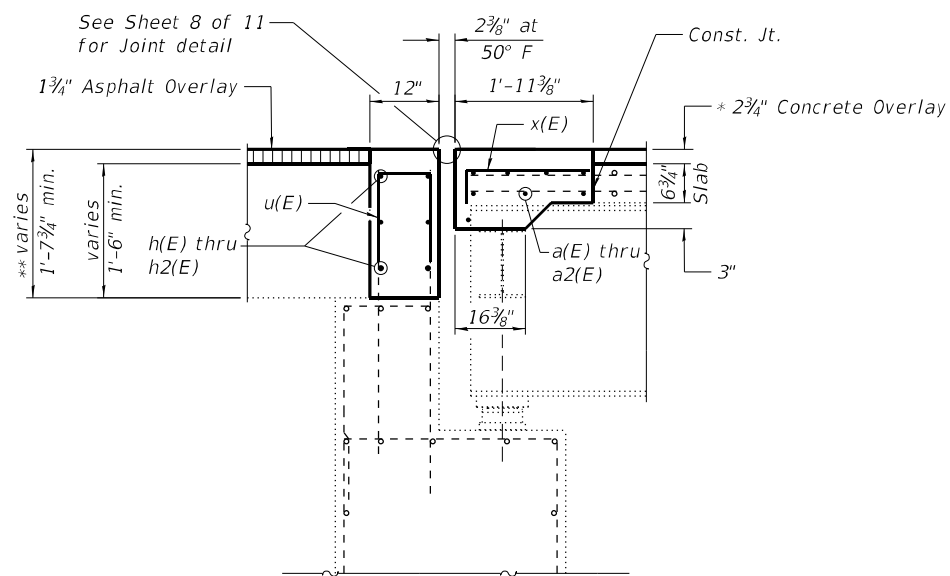
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CONTRACT NO. 62P69				
ILLINOIS FED. AID PROJECT				



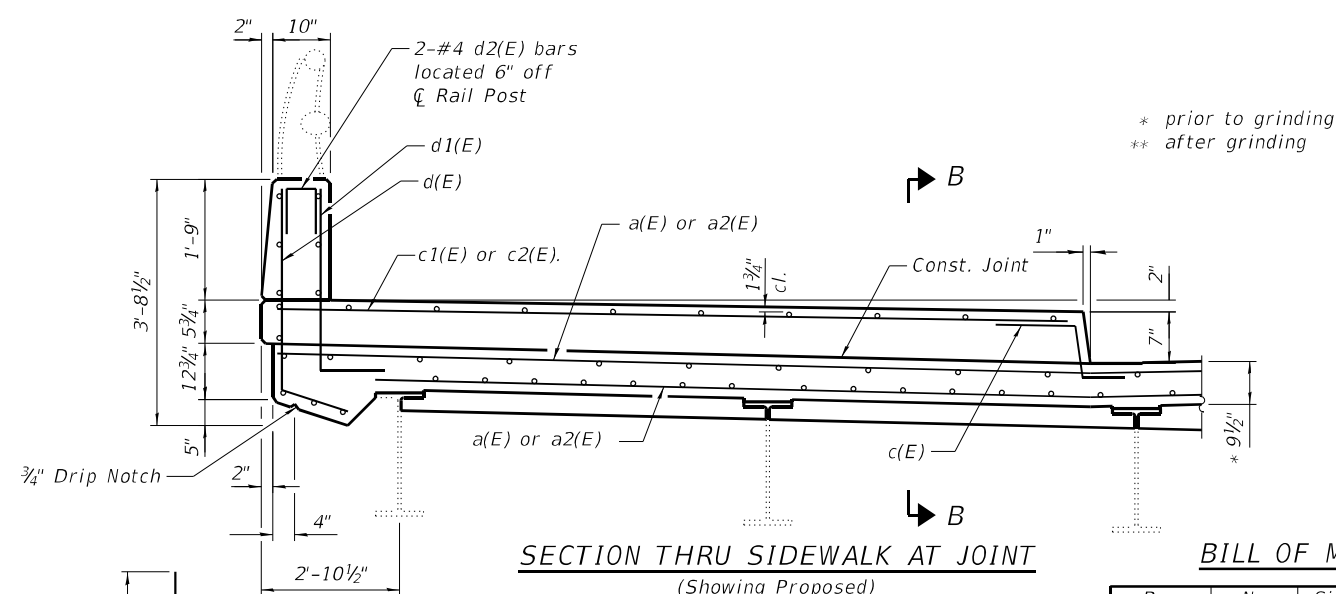
**SECTION THRU JOINT AT ABUTMENTS**  
(Showing Removal; Dimensions at right angles)



**SECTION THRU SIDEWALK AT JOINT**  
(Showing Removal)



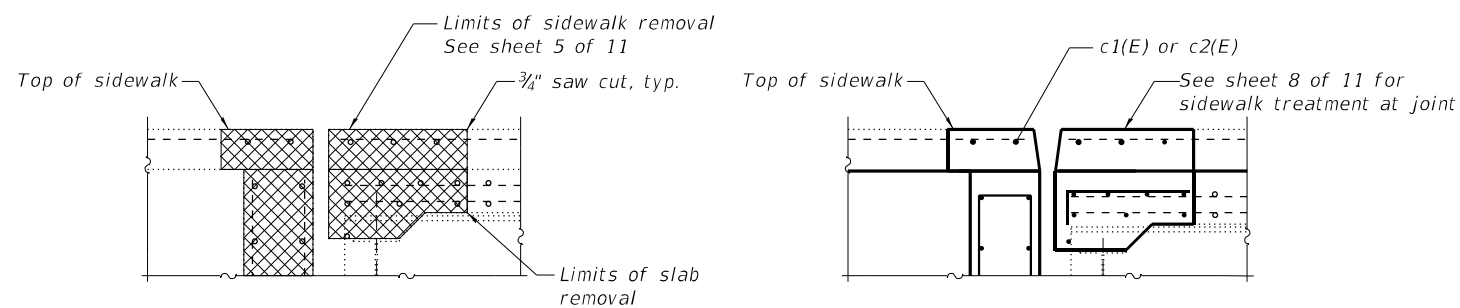
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**SECTION THRU SIDEWALK AT JOINT**  
(Showing Proposed)

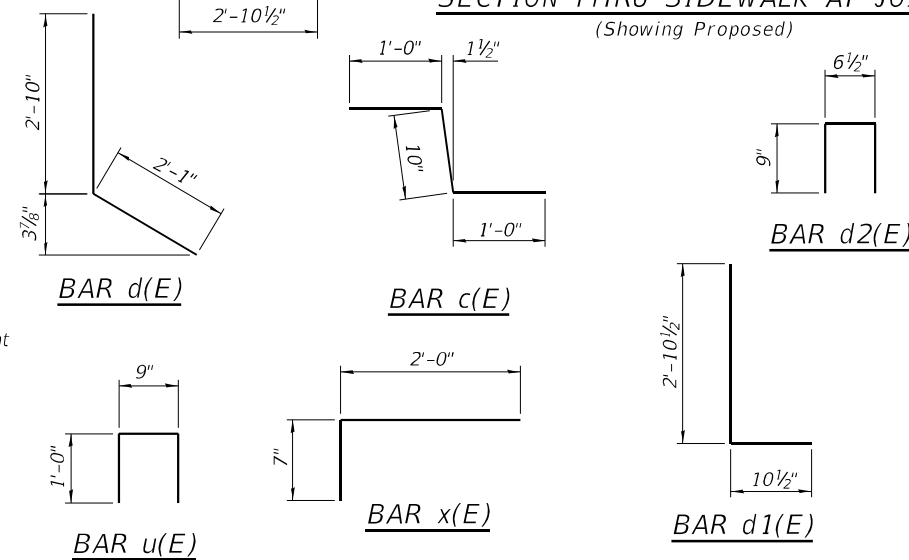
\* prior to grinding  
\*\* after grinding

Note:  
Cross-hatched areas indicate limits of  
Concrete Removal.



**SECTION A-A**

**SECTION B-B**



**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a(E)	16	#5	36'-7"	—
a1(E)	16	#5	12'-3"	—
a2(E)	16	#5	33'-10"	—
c(E)	20	#5	2'-10"	⌒
c1(E)	10	#5	11'-9"	—
c2(E)	10	#5	10'-9"	—
d(E)	20	#4	4'-11"	L
d1(E)	20	#6	3'-9"	L
d2(E)	8	#4	2'-1"	n
h(E)	12	#6	36'-7"	—
h1(E)	12	#6	12'-3"	—
h2(E)	12	#6	33'-10"	—
u(E)	168	#4	2'-9"	n
x(E)	166	#5	2'-7"	┌
Concrete Removal			Cu. Yd.	24.9
Concrete Superstructure			Cu. Yd.	26.3
Reinforcement Bars, Epoxy Coated			Pound	4,110

(Sheet 3 of 3)

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**LE** LIN ENGINEERING, LTD.  
Consulting Engineers  
Springfield, Illinois

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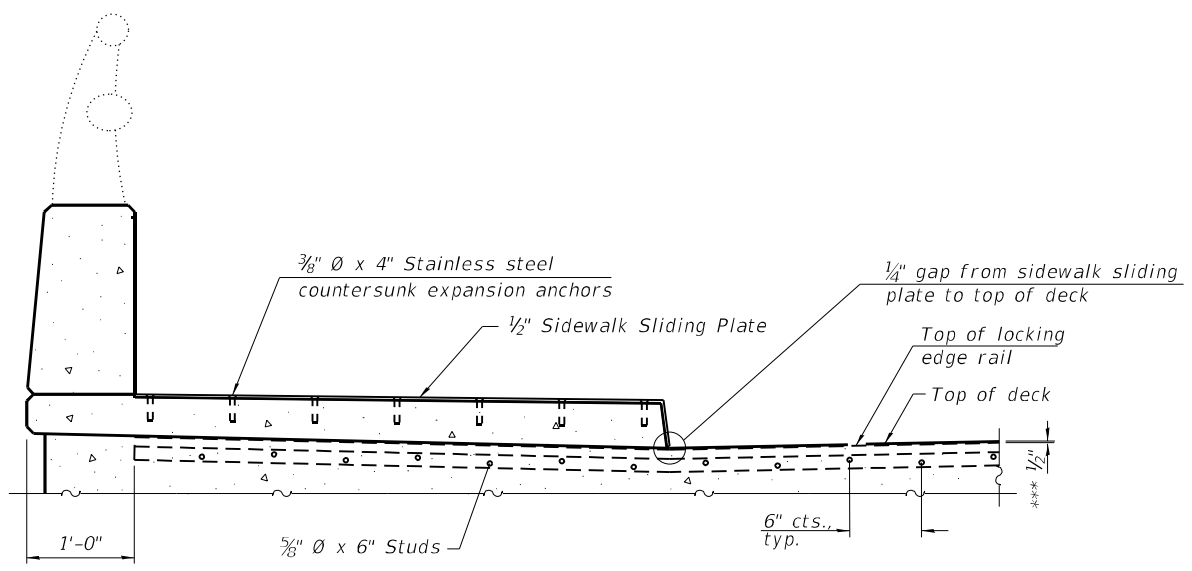
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DEPARTMENT OF TRANSPORTATION

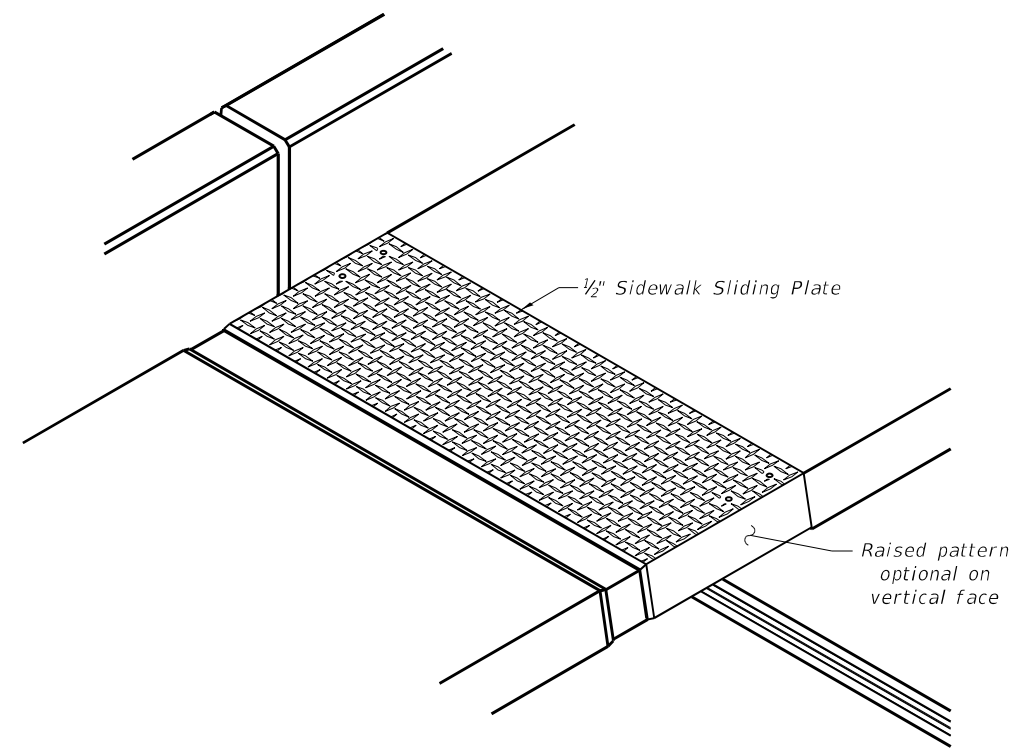
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STRUCTURE NO. 016-0250

SHEET 7 OF 11 SHEETS

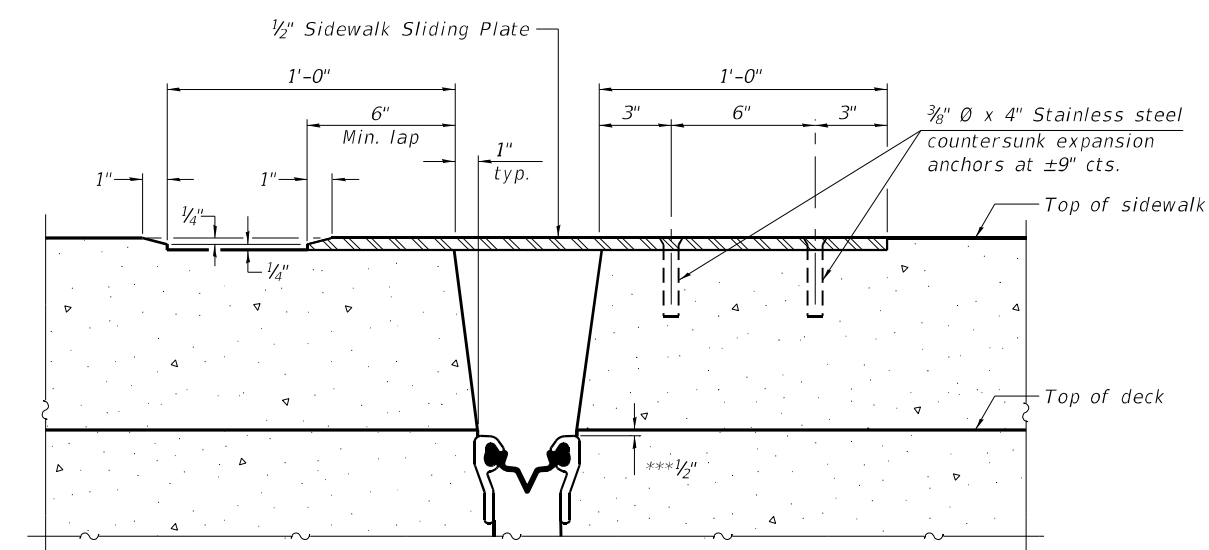
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CONTRACT NO. 62P69				
ILLINOIS FED. AID PROJECT				



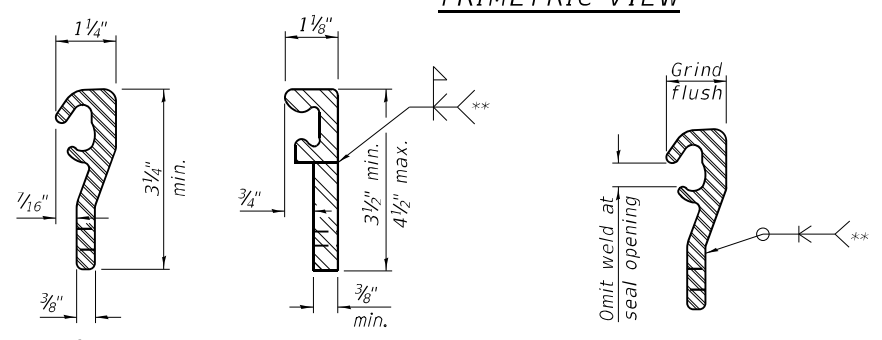
**SECTION AT RAISED SIDEWALK**



**TRIMETRIC VIEW**



**SECTION B-B**

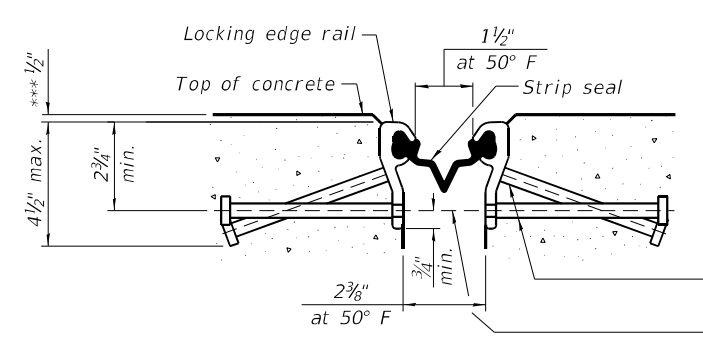


**LOCKING EDGE RAILS**

\*\* Back gouge not required if complete joint penetration is verified by mock-up.

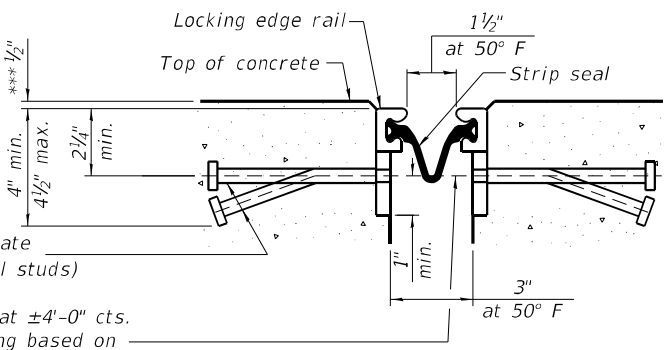
**LOCKING EDGE RAIL SPLICE**

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.



**SHOWING ROLLED RAIL JOINT**

\* 5/8" Ø x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)  
 3/8" φ threaded rods in 1/16" φ holes at ±4'-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

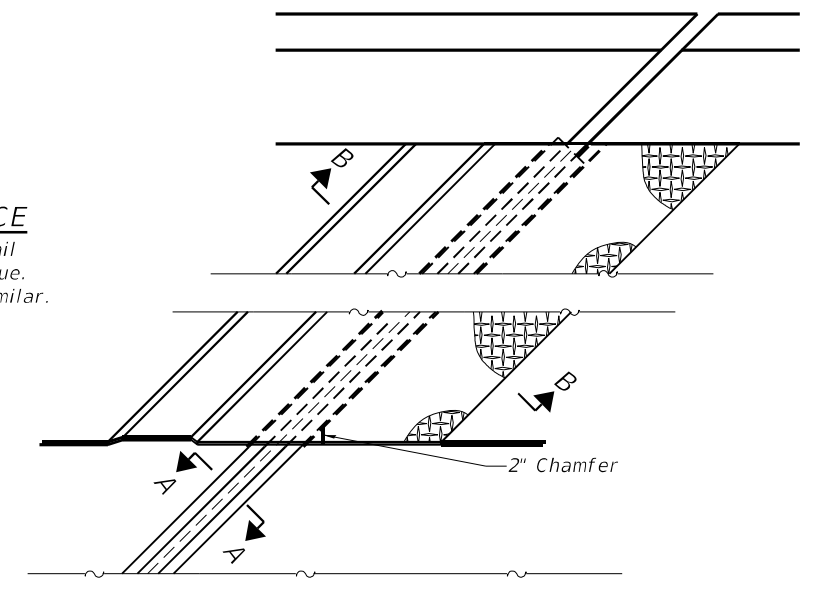


**SHOWING WELDED RAIL JOINT**

**SECTION A-A**

\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

**Notes:**  
 The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.  
 The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4 1/2" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.  
 The manufacturer's recommended installation methods shall be followed.  
 All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.  
 The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.  
 The top surface of sidewalk sliding plates shall have a raised pattern according to ASTM A786.  
 Cost of sidewalk sliding plates, anchorage studs, and expansion anchors included with Preformed Joint Strip Seal.  
 The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required.



**PLAN AT RAISED SIDEWALK**  
 (FOR SKEWS > 30°)

**BILL OF MATERIAL**

Item	Unit	Total
Preformed Joint Strip Seal	Foot	164

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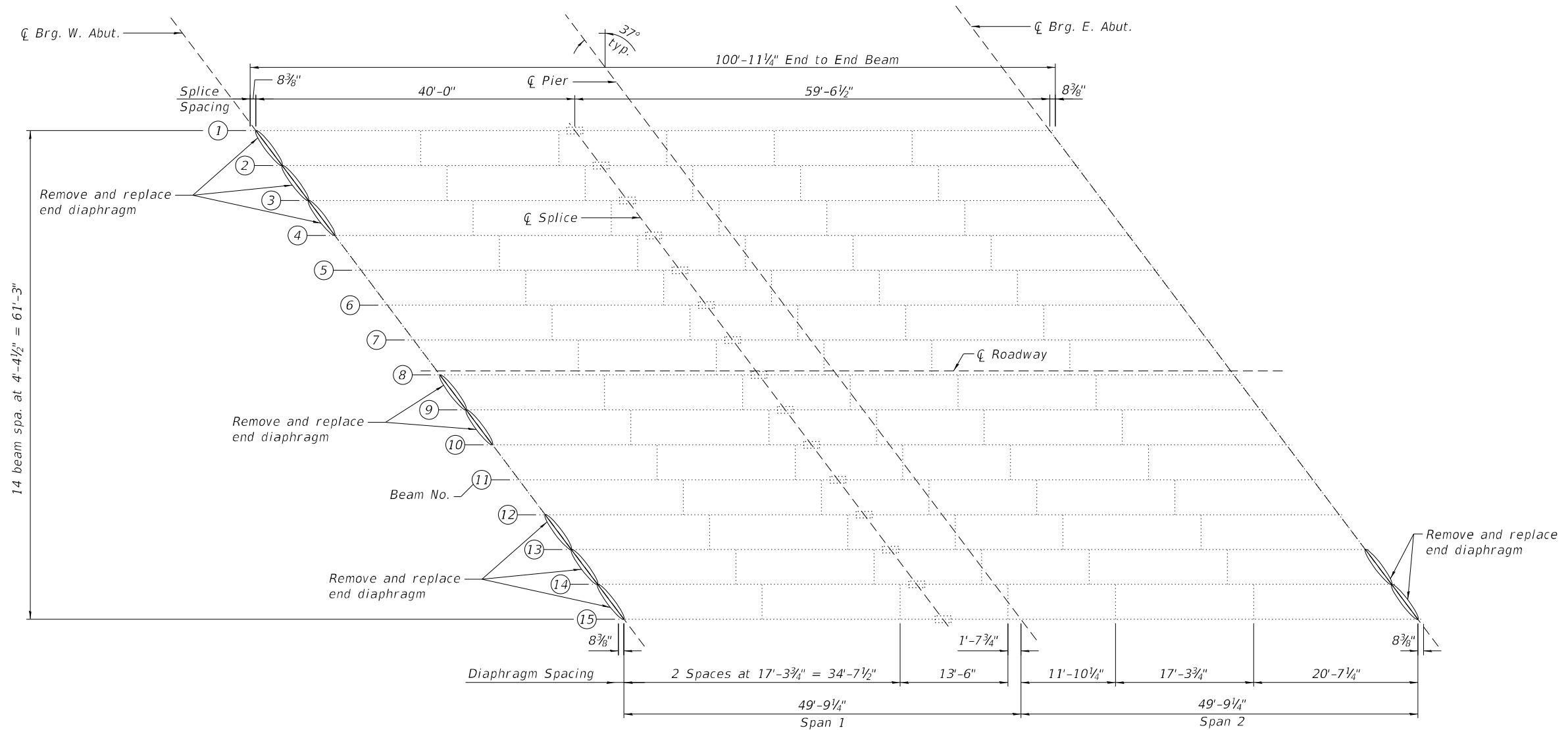
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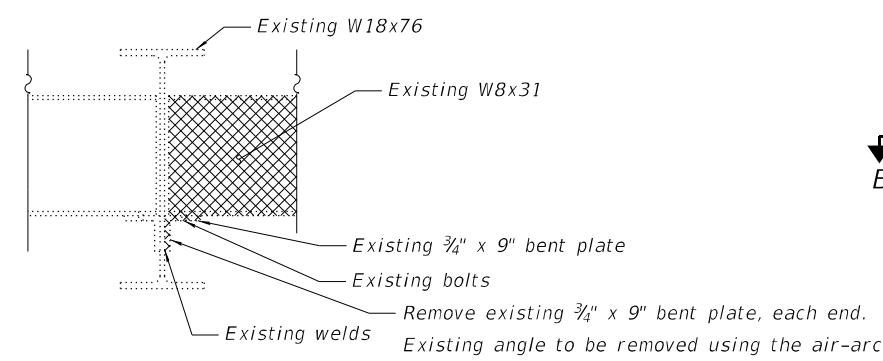
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**DEPARTMENT OF TRANSPORTATION**

**PREFORMED JOINT STRIP SEAL**  
**STRUCTURE NO. 016-0250**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62P69				
ILLINOIS / FED. AID PROJECT				

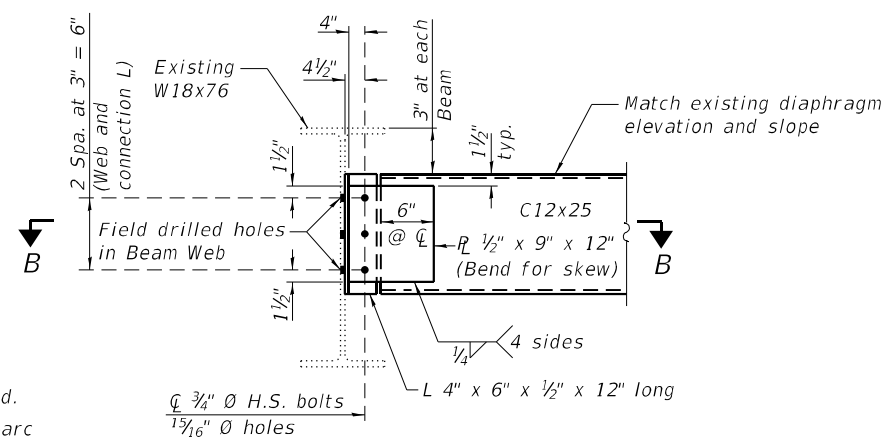


**FRAMING PLAN**



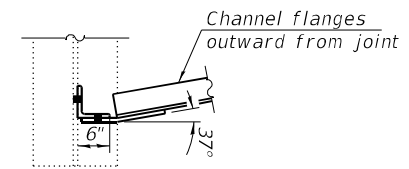
**DIAPHRAGM REMOVAL DETAIL**

Remove existing 3/4" x 9" bent plate, each end. Existing angle to be removed using the air-arc method and grind smooth all weld material remaining on the web. Cost included with Structural Steel Removal.



**NEW END DIAPHRAGM**

Note:  
Two hardened washers required for each set of oversized holes.



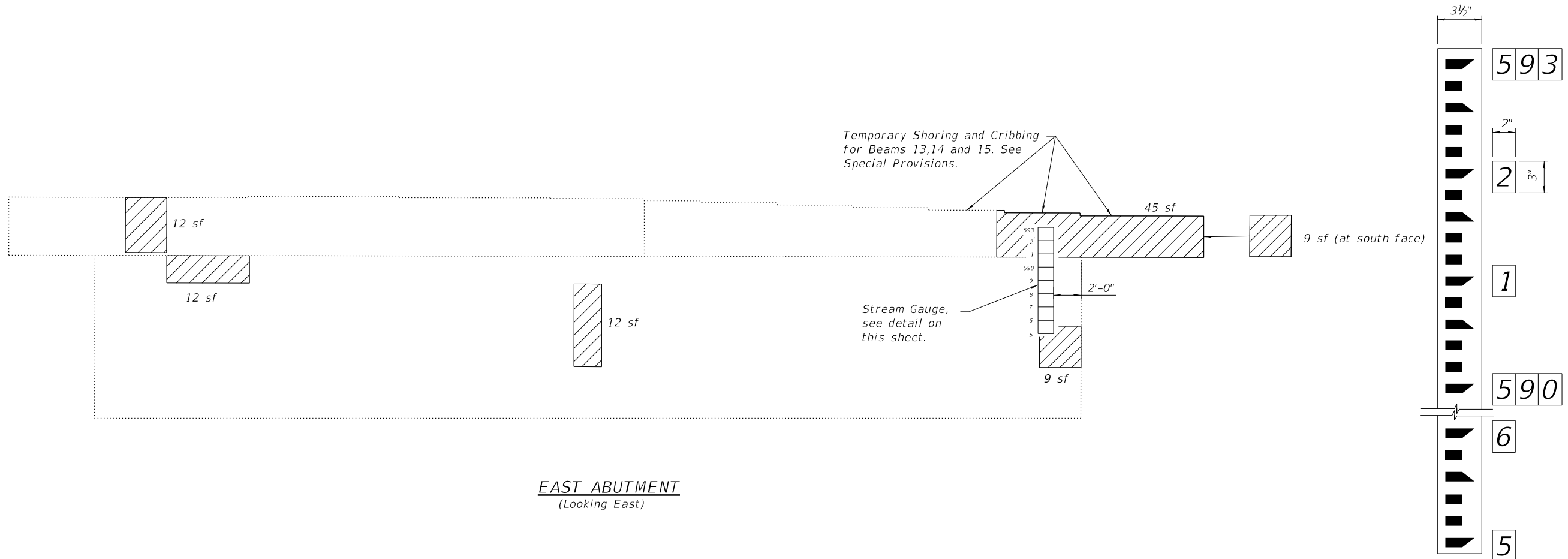
**SECTION B-B**

**BILL OF MATERIAL**

Item	Unit	Total
Furnishing and Erecting Structural Steel	Pound	2,126
Structural Steel Removal	Pound	2,206

Notes:  
Cost of new end diaphragms included with Furnishing and Erecting Structural Steel.  
Cross-hatch areas indicate limits of diaphragm removal.

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**EAST ABUTMENT**  
(Looking East)

**STREAM GAUGE DETAIL**

**LEGEND**

- Structural Repair of Concrete  
(Depth equal to or less than 5 inches)
- sf Square Feet

**Stream Gauge Notes:**

The gauge plates shall be porcelain enameled iron plate graduated in feet and tenths, unnumbered, and 3 1/2" wide. Gauge plates shall be Watermark Style "E" or approved equivalent.

Each individual number plate should be a black numeral on 2" X 3" white porcelain enameled iron plate. Number plates shall be "Watermark" Style "E" or approved equivalent.

Both the gauge plates and number plates shall be fastened directly to the pier with a 1/4" diameter, 1 1/2" long masonry screw with a hex washer head.

Three digit elevations to be installed at the top of the gauge and at every elevation ending with 0. At all of the other whole elevations, place the last digit as shown in the example above.

**Note:**

Repair of the existing abutments shall include but may not be limited to the areas shown. The actual area to be repaired will be determined by the Engineer at the time of construction.

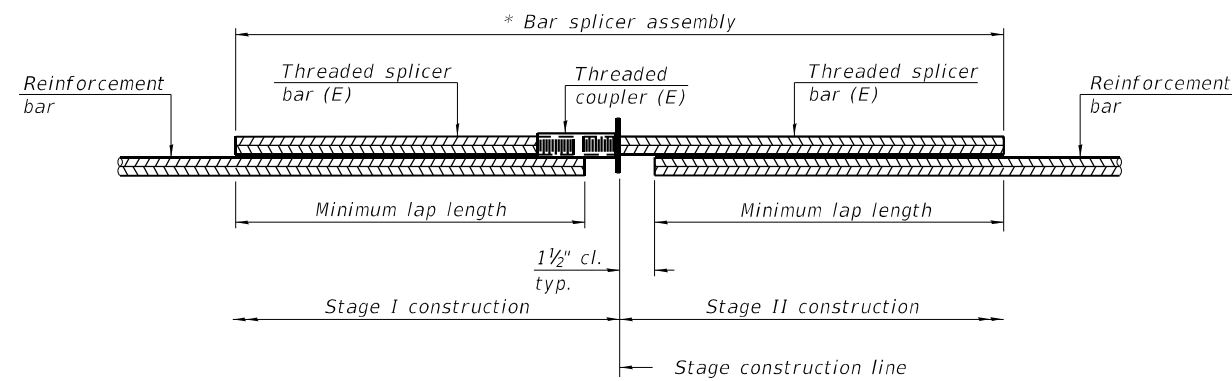
**BEAM REACTION TABLE**

Dead Load (kips)	15.9
Live Load (kips)	22.6
Impact (kips)	6.5
Total (kips)	45.0
Min. Jacking Capacity (Tons)	46

**BILL OF MATERIAL**

Item	Unit	Total
Stream Gauge	Each	1
Structural Repair of Concrete (Depth equal to or less than 5 inches)	Sq. Ft.	99
Temporary Shoring and Cribbing	Each	3

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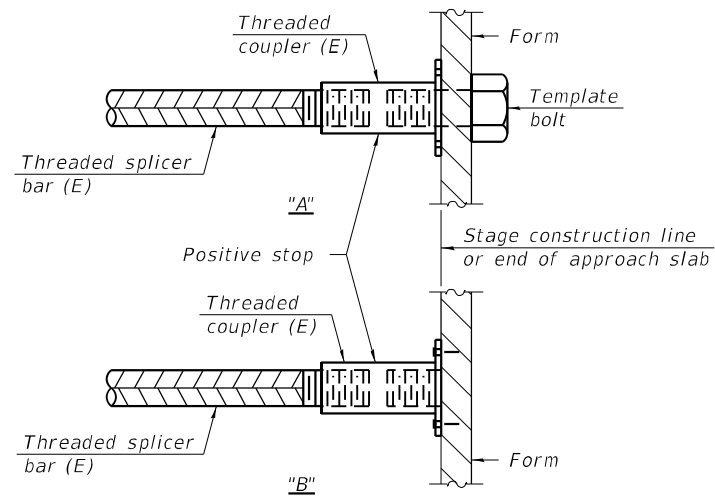


**STANDARD BAR SPLICER ASSEMBLY PLAN**  
 (All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Deck	#5	32	3'-6"
Abutment	#6	24	4'-0"

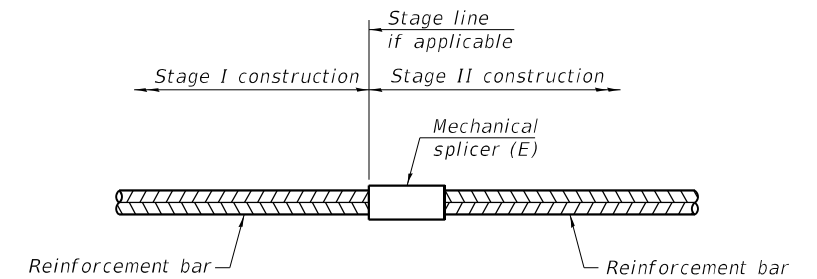


**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.

"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required

**Notes:**

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars.

Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

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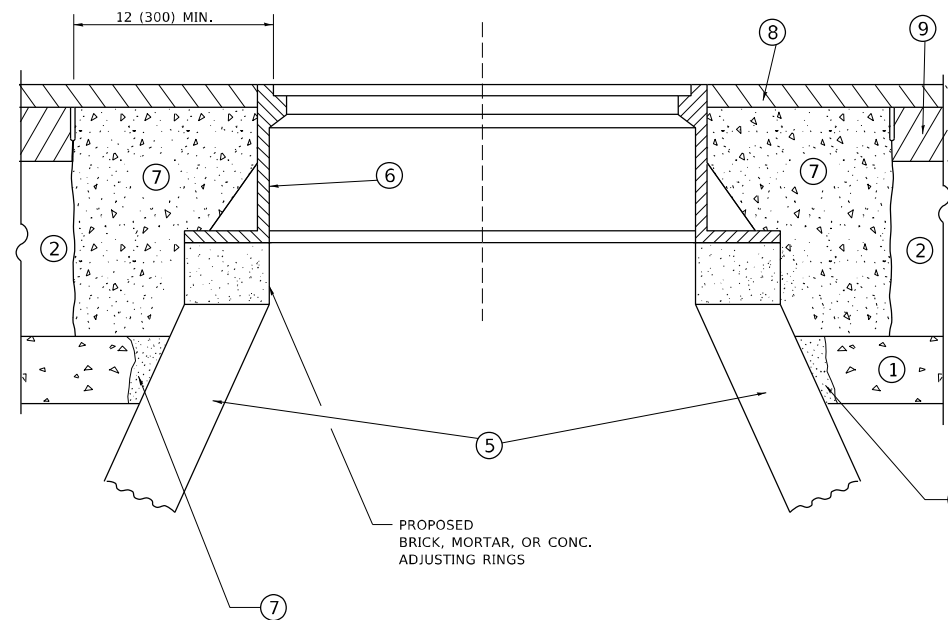
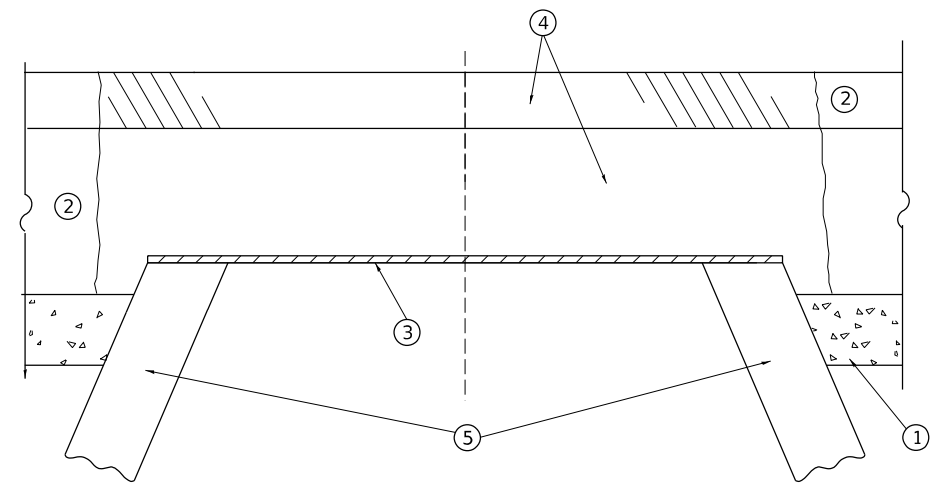
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 DEPARTMENT OF TRANSPORTATION**

**BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS  
 STRUCTURE NO. 016-0250**

SHEET 11 OF 11 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	50
CONTRACT NO. 62P69				
ILLINOIS FED. AID PROJECT				



**DETAILS FOR FRAMES AND LIDS ADJUSTMENT  
WITH MILLING**

**NOTES**

1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
3. CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
4. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
5. THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- |  |                               |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL                 | ⑥ FRAME AND LID (SEE NOTES)   |
| ② EXISTING PAVEMENT                          | ⑦ CLASS PP-2* CONCRETE        |
| ③ 36 (900) DIAMETER METAL PLATE              | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE  |
| ⑤ EXISTING STRUCTURE                         |                               |

**LOCATION OF STRUCTURES**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT**

1. REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
2. THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
3. NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
4. WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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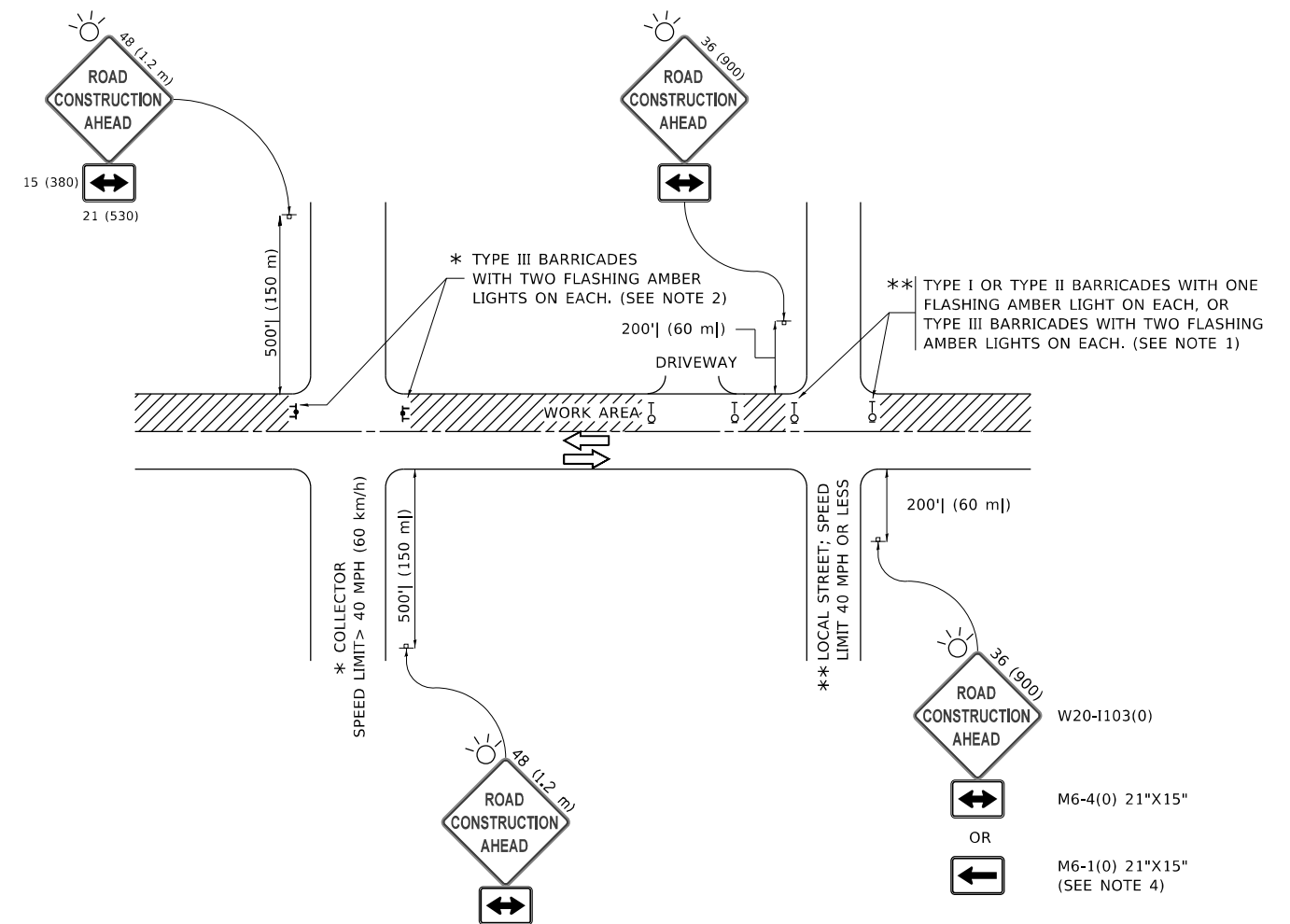
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DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR  
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	51
BD600-03 (BD-08)			CONTRACT NO. 62P69	
ILLINOIS FED. AID PROJECT				



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4),
 

WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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**STATE OF ILLINOIS  
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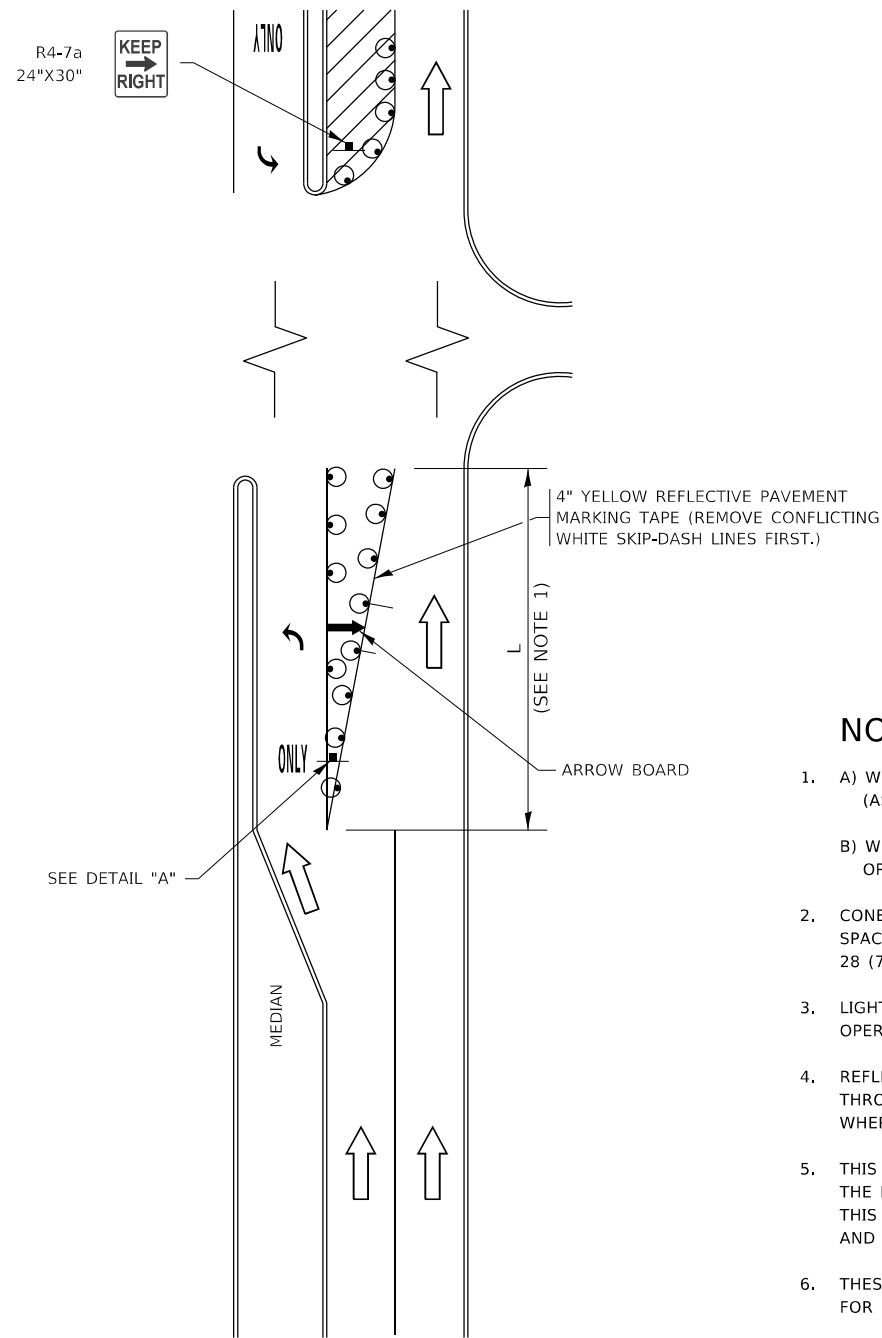
**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	52
<b>TC-10</b>			CONTRACT NO. 62P69	
ILLINOIS FED. AID PROJECT				

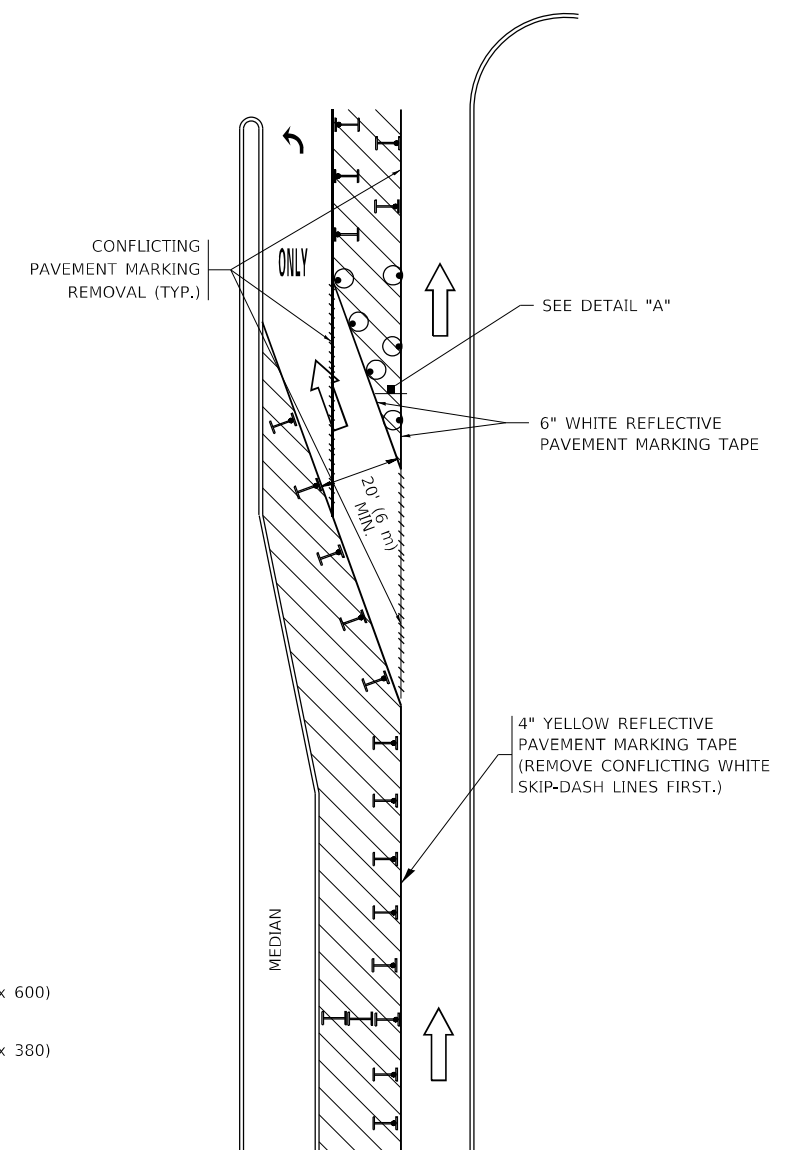


# TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER



**FIGURE 1**

# TURN BAY ENTRANCE WITHIN A LANE CLOSURE



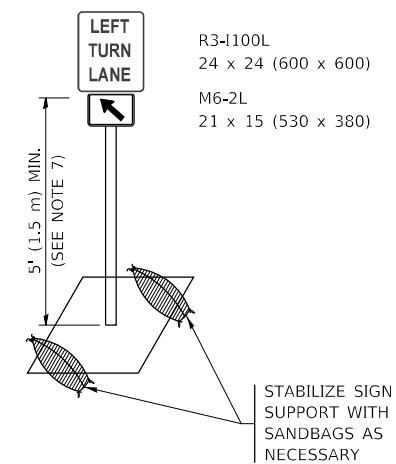
**FIGURE 2**

### LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

### NOTES:

1. A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
 B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



**DETAIL A**

All dimensions are in inches (millimeters) unless otherwise shown.

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USER NAME = footemj	DESIGNED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
	DRAWN - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13
PLOT SCALE = 50,0000 ' / in.	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16
PLOT DATE = 3/4/2019	DATE - T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS  
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	53
TC-14		CONTRACT NO. 62P69		
ILLINOIS FED. AID PROJECT				



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**ROUTE MARKERS**

FOR U.S. ROUTES  
M1-40-2424

FOR ILLINOIS ROUTES  
M1-50-05-14

R.R. UNMARKED ROUTES  
SPECIAL 24" x 18" VARIABLE  
4" BLACK LETTERS ON WHITE  
REFLECTIVE BACKGROUND

**ARROWS SIGNS**

M5-1L-2115

M5-1R-2115

M6-1-2115

M6-1-2115

M6-3-2115

**CARDINAL DIRECTION & DETOUR SIGNS**

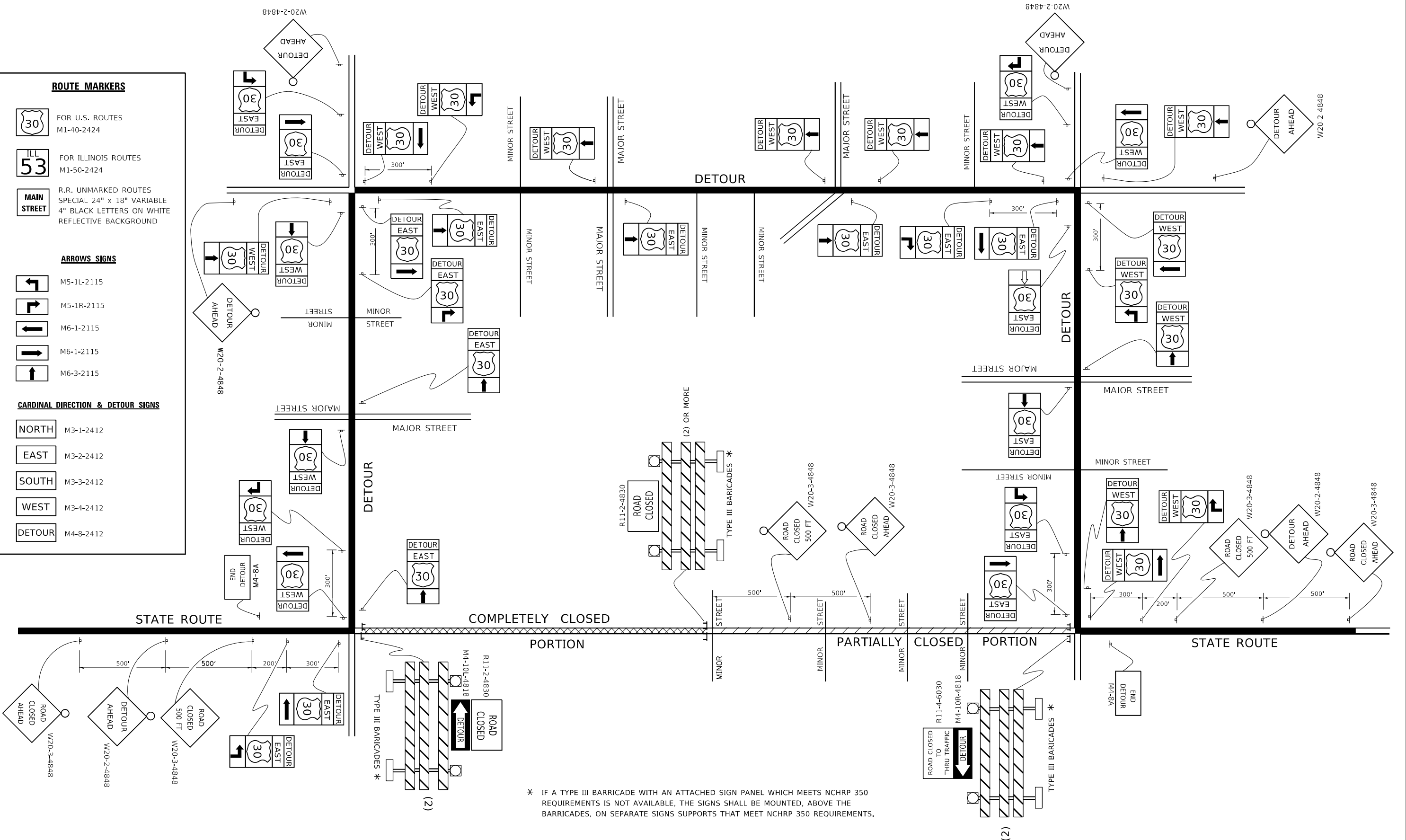
NORTH M3-1-2412

EAST M3-2-2412

SOUTH M3-3-2412

WEST M3-4-2412

DETOUR M4-8-2412



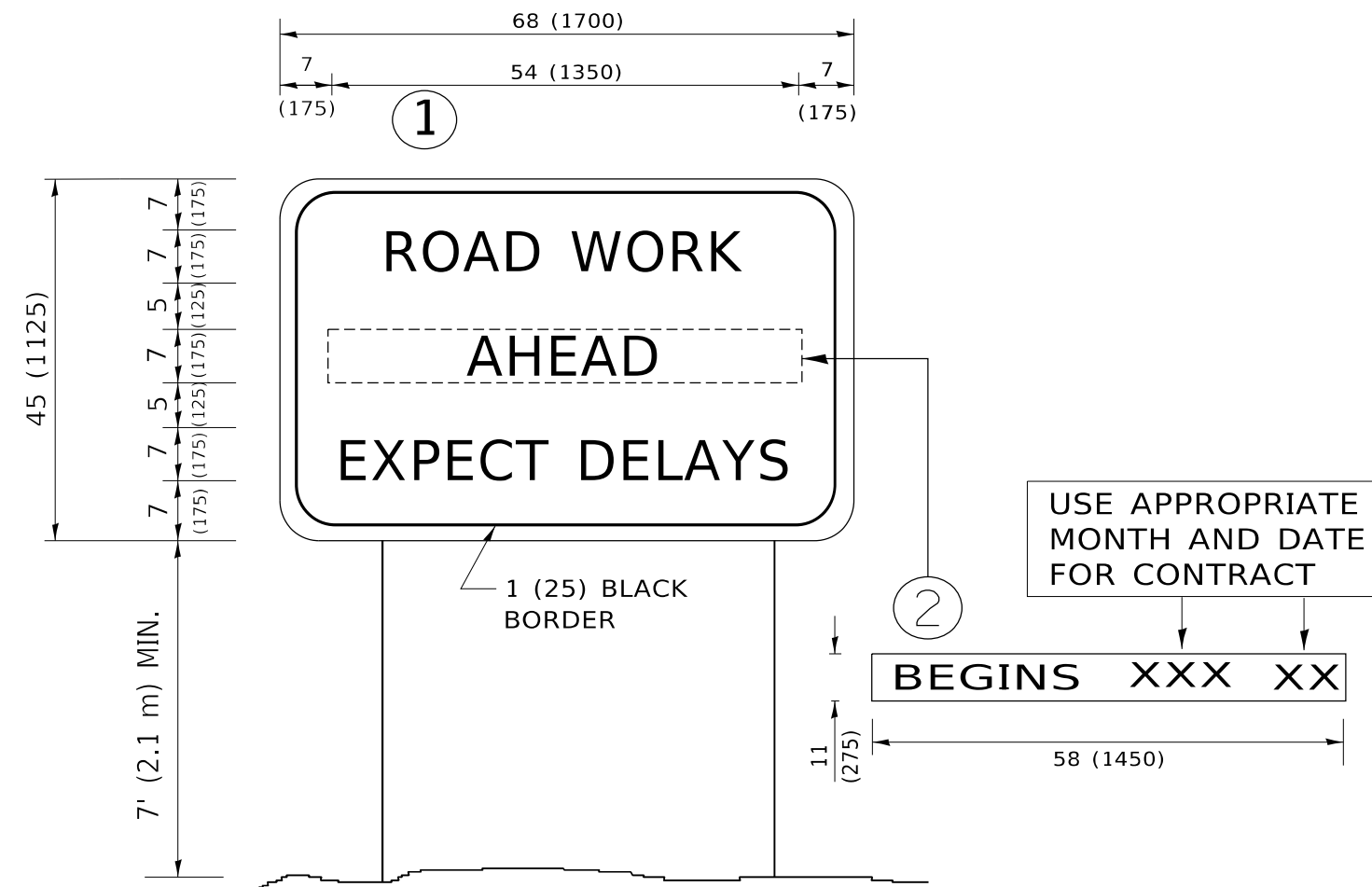
\* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

USER NAME = footemj	DESIGNED -	REVISED - 10-18-02
	DRAWN -	REVISED - R. BORO 09-14-09
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PLOT DATE = 3/4/2019	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

DETOUR SIGNING FOR CLOSING STATE HIGHWAYS			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	55
TC-21		CONTRACT NO. 62P69		
ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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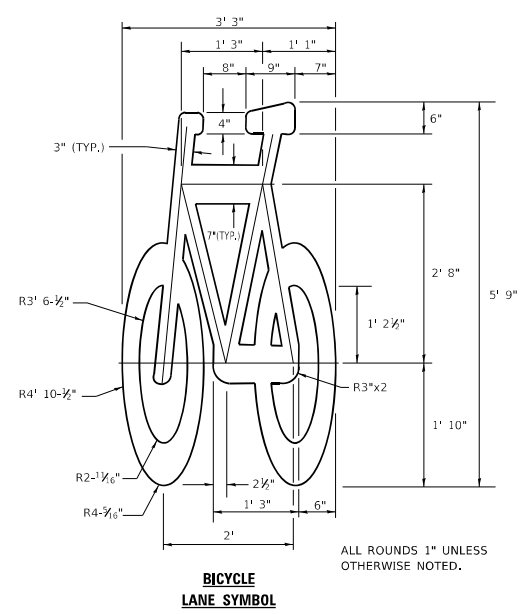
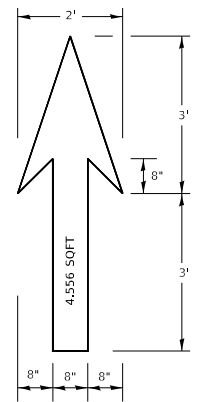
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

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<b>TC-22</b>			CONTRACT NO. 62P69	
ILLINOIS FED. AID PROJECT				



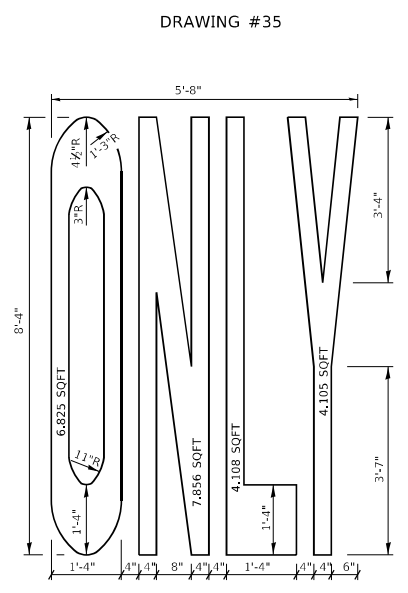
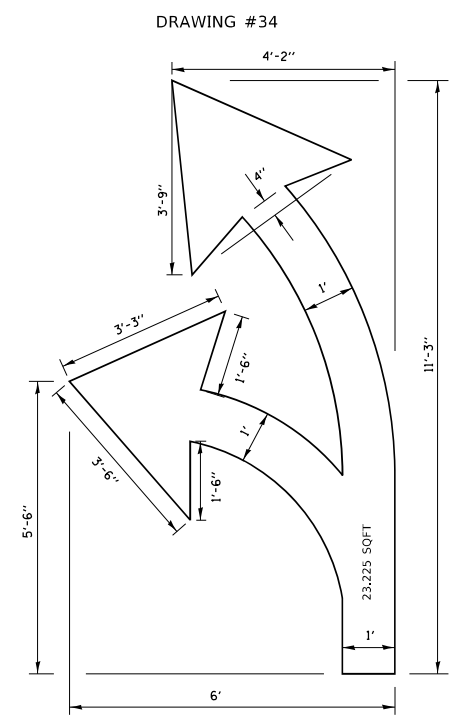
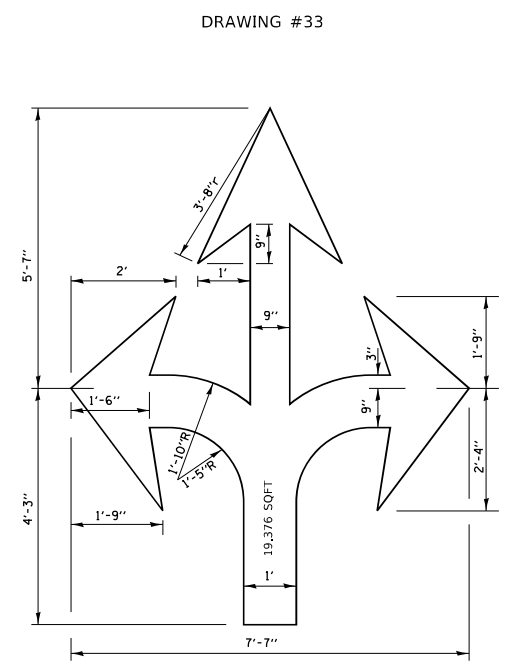
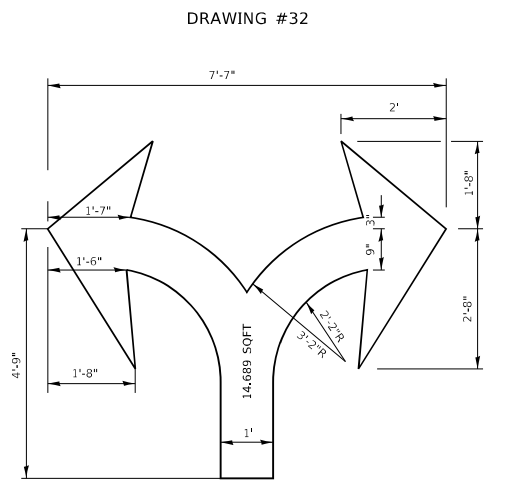
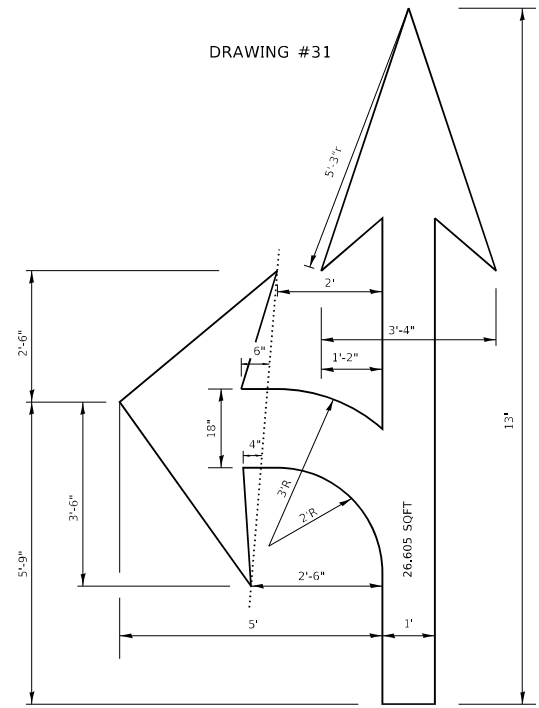
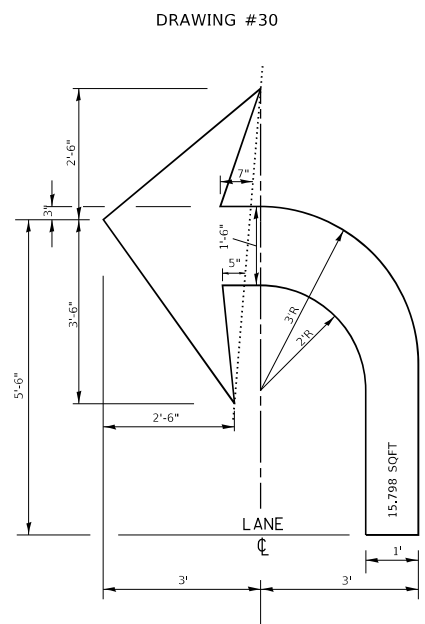
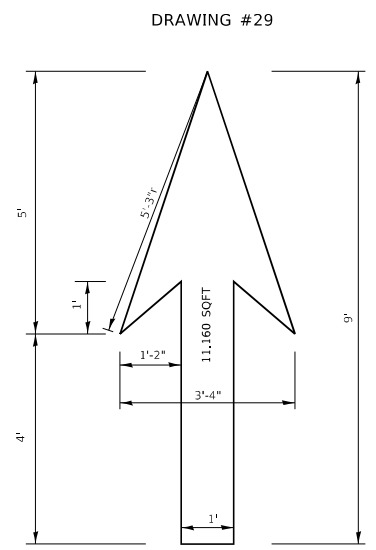


**BICYCLE  
LANE SYMBOL**

ALL ROUNDS 1" UNLESS OTHERWISE NOTED.

- NOTE:**
- FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.
  - THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

**TYPICAL BIKE LANE SYMBOLS  
DRAWING #28**



**NOTE:**  
ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

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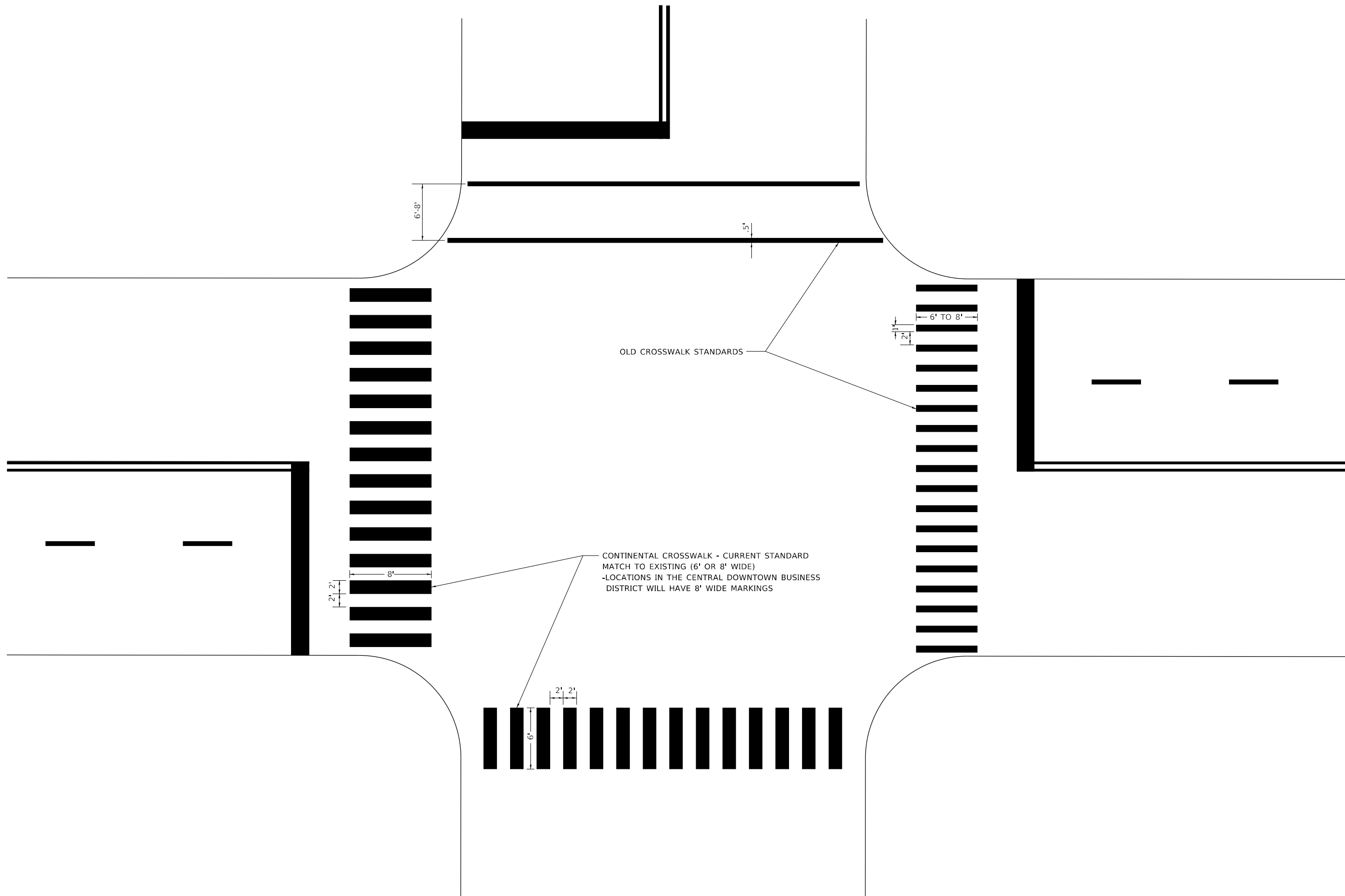
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CITY OF CHICAGO  
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 2 OF 3 SHEETS STA. TO STA.

F.A.U. RTE. 1360	SECTION 2021-086-BDR&BJR	COUNTY COOK	TOTAL SHEETS 60	SHEET NO. 58
<b>TC-24</b>		CONTRACT NO. 62P69		
ILLINOIS		FED. AID PROJECT		

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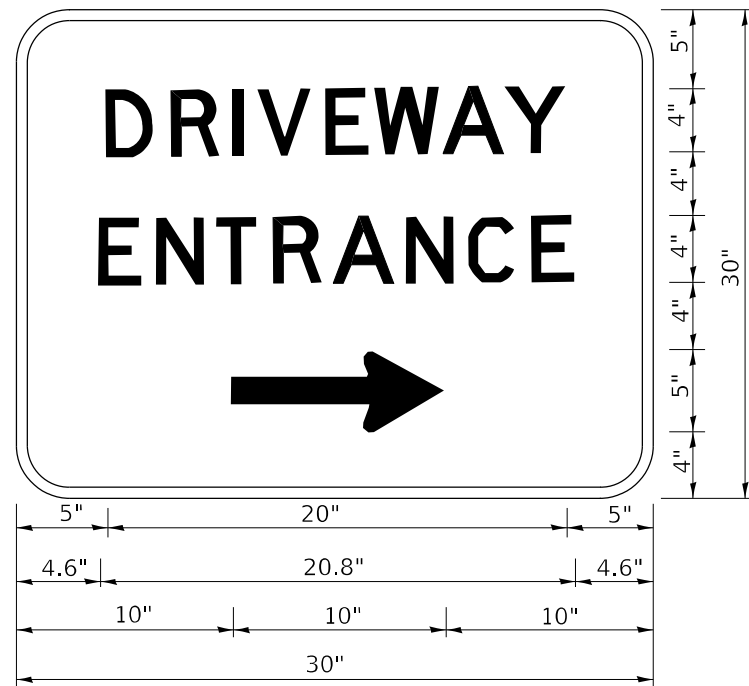
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PLOT DATE = 3/4/2019	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**CITY OF CHICAGO  
 TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 3 OF 3 SHEETS STA. TO STA.

F.A. U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	59
<b>TC-24</b>			CONTRACT NO. 62P69	
ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED  
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

**NOTES:**

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

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PLOT DATE = 8/6/2021	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DRIVEWAY ENTRANCE SIGNING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1360	2021-086-BDR&BJR	COOK	60	60
<b>TC-26</b>			CONTRACT NO. 62P69	
		ILLINOIS	FED. AID PROJECT	