

LETTING ITEM NUMBER

03A

LETTING DATE

26 APRIL, 2024

LETTING CONTRACT NUMBER

WI061

CONSTRUCTION PLANS

TAXIWAY ALPHA & BRAVO REHABILITATION AT VETERANS AIRPORT OF SOUTHERN ILLINOIS MARION - HERRIN, WILLIAMSON COUNTY, ILLINOIS

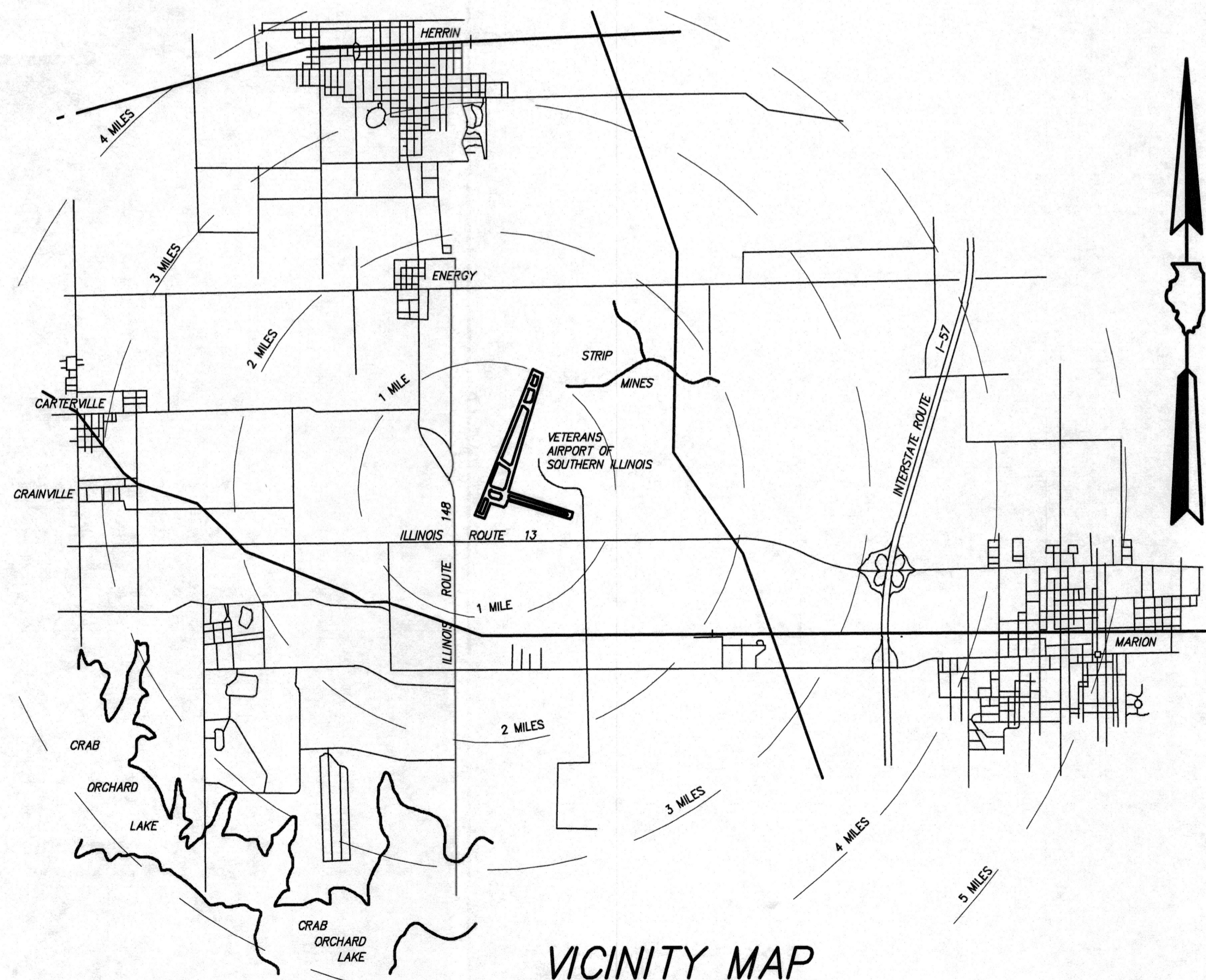
DATE : MARCH 1, 2024

A.I.P. No.: TBD

MWA - 4901

INDEX OF SHEETS

- 1. COVER
- 2. SCOPE OF WORK
- 3. SUMMARY OF QUANTITIES
- 4-7. SAFETY AND PHASING PLAN
- 8. TYPICAL SECTIONS
- 9-10. DEMOLITION PLAN
- 11. SLAB REPLACEMENT PLAN
- 12-20. PLAN AND PROFILE
- 21. INTERSECTION DETAIL
- 22-23. PAVEMENT MARKING PLAN
- 24. PAVEMENT MARKING DETAILS
- 25-26. ELECTRICAL PLANS
- 27-29. ELECTRICAL NOTES AND DETAILS
- 30. EROSION CONTROL & GRADING PLAN



VICINITY MAP

BOARD MEMBERS

CHAIRMAN: JAVIER MUNIZ

MEMBERS: RANDY YOUNGBLOOD
BOB MEES
MICHAEL TROOP
B.W. BRUCE

SECRETARY: JOHN O'KEEFE

AIRPORT DIRECTOR: DOUG KIMMEL



EXPIRES : NOVEMBER 30, 2024

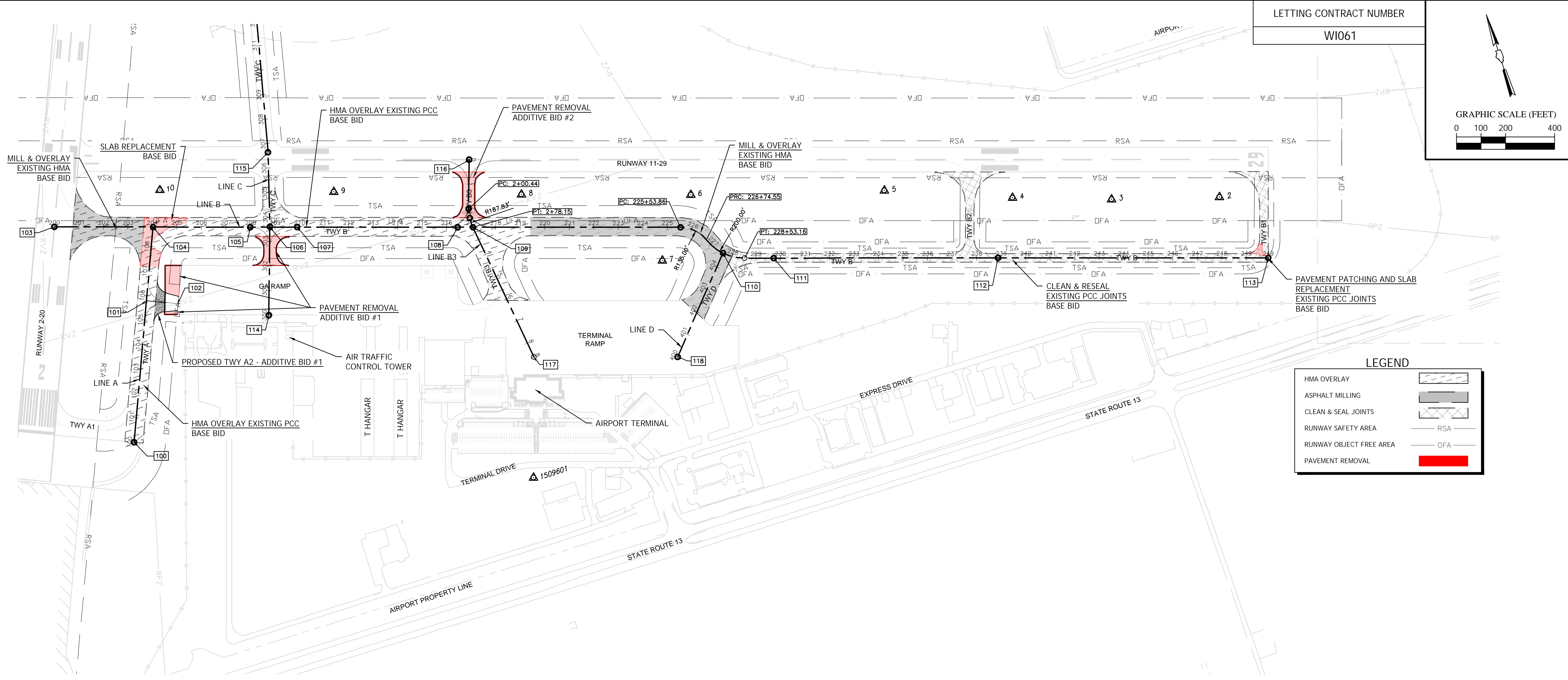
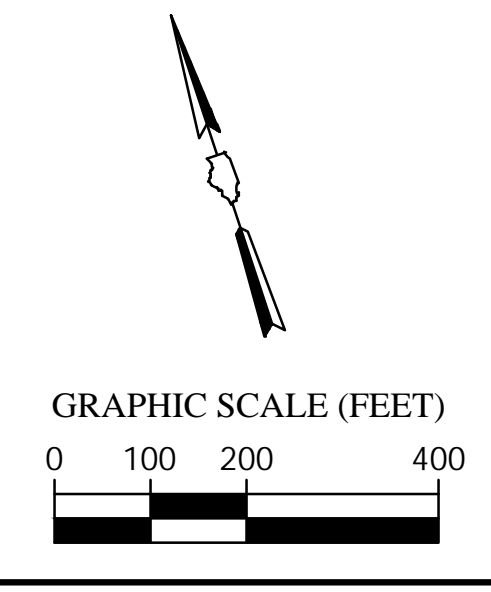
PLANS PREPARED BY :

<p>HORNER SHIFFRIN 410 NORTH COURT ST. MARION, ILLINOIS 62959 618-993-6411 FAX 618-993-6750 WWW.HORNERSHIFFRIN.COM</p>	<p>WILLIAMSON COUNTY AIRPORT AUTHORITY</p>
<p>REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS NO. 053881</p> <p>SUBMITTED <i>W. Brian Ziegler</i> ENGINEER W. BRIAN ZIEGLER</p> <p>DATE : 3/01/2024</p>	<p>APPROVED <i>[Signature]</i> CHAIRMAN DATE : 3/01/2024</p> <p>APPROVED <i>[Signature]</i> SECRETARY DATE : 3/01/2024</p>

PROJECT 19204CZ

LETTING CONTRACT NUMBER

WI061



LEGEND

HMA OVERLAY	
ASPHALT MILLING	
CLEAN & SEAL JOINTS	
RUNWAY SAFETY AREA	RSA
RUNWAY OBJECT FREE AREA	OFA
PAVEMENT REMOVAL	

GENERAL NOTES

- THE SCOPE OF WORK SHEET IS INTENDED ONLY AS A GENERAL DESCRIPTION OF WORK ITEMS, THEIR APPROXIMATE LOCATIONS, AND LIMITS FOR THE PURPOSE OF UNDERSTANDING THE SCOPE OF THE PROJECT. IT SHALL NOT BE USED AS A CONSTRUCTION PLAN. REFER TO THE PLAN SHEETS WHICH FOLLOW FOR DETAILED CONSTRUCTION REQUIREMENTS, LOCATIONS, AND ITEMS OF WORK.
- THESE DRAWINGS COVER THE REQUIREMENTS FOR THE REHABILITATION OF TAXIWAY ALPHA AND BRAVO. THE BASE BID INCLUDES, BUT IS NOT LIMITED TO MILLING AND BITUMINOUS PAVING OF PORTIONS OF TAXIWAYS A, B, C, AND D, PCC SLAB REPLACEMENT, CLEANING AND RESEALING EXISTING PCC JOINTS, AND PAVEMENT MARKINGS. THE ADDITIVE BIDS INCLUDE, BUT NOT LIMITED TO REMOVAL OF PORTIONS OF TAXIWAY CHARLIE AND BRAVO 3, AND RELOCATION OF EDGE LIGHTS.
- THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIAL, EQUIPMENT, PERMITS AND TRANSPORTATION NECESSARY TO CONSTRUCT ALL ELEMENTS AS DESCRIBED IN THE CONSTRUCTION PLANS AND SPECIFICATIONS.
- THE PROJECT PAY IS INTENDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL INCIDENTAL WORK TO COMPLETE THE PROJECT TO THE SATISFACTION OF THE ENGINEER SHALL BE INCLUDED IN THE COST OF PERFORMING THESE ITEMS.
- THE RULES, REGULATIONS, AND REFERENCE SPECIFICATIONS ENUMERATED HEREIN SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS. THEY SHALL NOT RELIEVE THE CONTRACTOR FROM FURNISHING AND INSTALLING HIGHER GRADES OF MATERIAL AND WORKMANSHIP THAN ARE SPECIFIED HEREIN, OR WHEN SO REQUIRED.
- THE CONTRACTOR SHALL PAY CLOSE ATTENTION TO THE SAFETY AND PHASING PLANS AND TO THE AIRPORT SAFETY REQUIREMENTS SECTION OF THE SPECIFICATIONS. THESE SHALL BE STRICTLY ENFORCED.
- CONSTRUCTION WILL BE WITHIN THE AIRCRAFT OPERATIONS AREA (AOA). ALL RUBBISH AND DEBRIS RESULTING FROM WORK SHALL BE REMOVED FROM THE SITE ON A DAILY BASIS BY THE CONTRACTOR.
- UPON COMPLETION OF THE WORK, ALL SURPLUS MATERIAL AND EQUIPMENT BELONGING TO THE CONTRACTOR (AND HIS/HER SUBCONTRACTORS) SHALL BE REMOVED FROM THE SITE.
- THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE PRIOR TO BEGINNING WORK TO COORDINATE WORK PROCEDURES WITH ALL INTERESTED PARTIES. THE CONTRACTOR SHALL ALSO ATTEND ANY JOB MEETINGS CALLED BY THE ENGINEER.
- ALL OPEN TRENCHES, EXCAVATION, AND STOCKPILED MATERIAL SHALL BE PROMINENTLY MARKED, LIGHTED, BARRICADED, ETC., AS DEEMED NECESSARY BY THE AIRPORT, FAA, AND/OR THE ENGINEER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ALL DAMAGE TO AIRPORT PROPERTY CAUSED BY HIM, HER OR THEIR SUBCONTRACTORS.
- TOTAL CONTRACT TIME FOR THIS PROJECT BASE BID IS 160 CALENDAR DAYS FROM THE DATE OF NOTICE TO PROCEED. BID ADDITIVE NUMBER 1 AND NUMBER 2 ARE INCLUDED IN THE PROJECT CALENDAR DAYS. NOTICE TO PROCEED IS ANTICIPATED FOR AUGUST 2024. LIQUIDATED DAMAGES IN THE AMOUNT OF \$2,500/CALENDAR DAY SHALL BE ASSESSED FOR FAILURE TO COMPLETE THE WORK WITHIN THE ALLOTTED TIME.
- THE CONTRACTOR SHALL SUPPLY ONE SET OF AS-BUILT DRAWINGS TO THE ENGINEER AT THE COMPLETION OF THE PROJECT. THE CONTRACTOR SHALL KEEP THE RED LINE DRAWINGS SET ON SITE AND SHALL UPDATE ON A REGULAR BASIS THROUGHOUT THE PROJECT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONSTRUCTION LAYOUT USING THE CONTROL POINTS AND ALIGNMENTS PROVIDED.

PROJECT CONTROL POINT TABLE

PT#	MVA LOCAL GROUND COORDINATES		STATION, LINE	DESCRIPTION
	NORTHING	EASTING		
100	394424.9629	786645.9685	100+00.00, LINE A	END TWY A
101	394957.2351	786872.9260	105+78.61, LINE A	INTERSECTION PT TWY A/A2
102	394907.2562	786990.0641	105+78.61 OFFSET 127.355, LINE A	END TWY A2
103	395359.8866	786608.7923	200+00.00, LINE B	END TWY B
104	395234.8904	786991.3165	108+80.48 & 204+02.43, LINE A/B	INTERSECTION PT TWY A/B
105	395111.8736	787367.7833	207+98.48, LINE B	PI TWY B
106	395086.9884	787444.2777	208+78.93 & 303+59.04, LINE B/C	INTERSECTION PT TWY B/C
107	395052.5752	787550.0596	209+90.16, LINE B	PI TWY B
108	394849.6873	788172.2539	216+44.60, LINE B	PI TWY B
109	394830.3754	788231.5086	217+06.92 & 2+78.15, LINE B/B3	INTERSECTION PT TWY B/B3
110	394414.7682	789168.8116	227+62.06 & 404+61.89, LINE B	INTERSECTION PT TWY B/D
111	394331.0421	789359.3238	229+72.28, LINE B	PI TWY B
112	394048.9384	790229.4687	238+87.01, LINE B	PI TWY B
113	393707.7494	791277.7148	249+89.38, LINE B	END TWY B
114	394747.9436	787329.0405	300+00.00, LINE C	END TWY C
115	395379.4748	787531.2975	306+64.20, LINE C	END TWY C
116	395096.8338	788302.1782	0+00.00, LINE B3	END TWY B3
117	394251.2148	788304.1716	8+61.85, LINE B3	END TWY B3
118	394069.4892	788862.0151	400+00.00, LINE D	END TWY D

AIRPORT CONTROL POINT TABLE

POINT #	NORTHING	EASTING	ELEVATION	DESCRIPTION
1509601	393782.901	788151.599	451.85	CP RB4
2	394004.655	791165.434	464.97	CP RB4
3	394131.874	790743.908	465.82	CP RB4
4	394263.158	790362.216	467.73	CP RB4
5	394452.738	789874.209	469.26	CP RB4
6	394680.788	789117.377	469.11	CP RB4
7	394461.989	788924.456	464.73	CP RB4
8	394896.039	788460.674	464.38	CP RB4
9	395142.425	787736.753	459.12	CP RB4
10	395368.876	787064.816	454.37	CP RB4

STATE OF ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184-004515

REVISIONS	
DATE	NO.

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SCOPE OF WORK
TAXIWAY ALPHA & BRAVO REHABILITATION
VETERANS AIRPORT OF SOUTHERN ILLINOIS

Date 3/1/2024 DR. By BLW CK'D By MKW
 Scale 1" = 200' Job No. 19204CZ Sheet No. 2 of 30


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LETTING CONTRACT NUMBER
WI061

SUMMARY OF QUANTITIES					
TAXIWAY A & B - BASE BID					
	PAY ITEM NO.	DESCRIPTION	QTY.	UNIT	AS BUILT QTY.
1	AW150512	ENGINEER'S FIELD OFFICE	5	CAL MO	
2	AW150520	MOBILIZATION	1	LSUM	
3	AW150530	TRAFFIC MAINTENANCE	1	LSUM	
4	AW209510	CRUSHED AGGREGATE BASE COURSE	32	TON	
5	AW401610	BITUMINOUS SURFACE COURSE	6800	TON	
6	AW401650	BITUMINOUS PAVEMENT MILLING	12910	SQ YD	
7	AW401655	BUTT JOINT CONSTRUCTION	1580	SQ YD	
8	AW401660	SAW & SEAL BIT. JOINTS	19401	FOOT	
9	AW401900	REMOVE BITUMINOUS PAVEMENT	11	SQ YD	
10	AW501509	PCC PAVEMENT, 9"	161	SQ YD	
11	AW501515	PCC PAVEMENT, 15'	1900	SQ YD	
12	AW501909	9" PCC PAVEMENT REMOVAL	161	SQ YD	
13	AW501915	15" PCC PAVEMENT REMOVAL	1900	SQ YD	
14	AW605540	CLEAN AND SEAL JOINTS	16593	FOOT	
15	AW605541	CLEAN AND SEAL CRACKS	1400	FOOT	
16	AW620520	PAVEMENT MARKING-WATERBORNE	13310	SQ FT	
17	AW620525	PAVEMENT MARKING-BLACK BORDER	9812	SQ FT	
18	AW620590	TEMPORARY MARKING	13310	SQ FT	
19	AW620900	PAVEMENT MARKING REMOVAL	5110	SQ FT	
20	AW701330	30" RCP, CLASS II	40	FOOT	
21	AW701840	CONCRETE CRADLE/COLLAR	2	CU YD	
22	AW701900	REMOVE PIPE	40	FOOT	
23	AW751903	REMOVE MANHOLE	2	EACH	
24	AW751943	ADJUST MANHOLE	1	EACH	
25	AW901510	SEEDING	1.85	ACRE	
26	AW905530	TOPSOIL	8403	SQ YD	
27	AW908514	LIGHT-DUTY HYDRAULIC MULCH	1.85	ACRE	

TAXIWAY A & B - ADDITIVE BID #2					
	PAY ITEM NO.	DESCRIPTION	QTY.	UNIT	AS BUILT QTY.
1	AY108158	1/C #8 5KV UG CABLE IN UD	300	FOOT	
2	AY125962	RELOCATE BASE MOUNTED LIGHT	12	EACH	
3	AY125964	RELOCATE TAXI GUIDANCE SIGN	4	EACH	
4	AY150530	TRAFFIC MAINTENANCE	1	LSUM	
5	AY152442	OFFSITE BORROW EXCAVATION	715	CU YD	
6	AY401900	REMOVE BITUMINOUS PAVEMENT	1300	SQ YD	
7	AY620900	PAVEMENT MARKING REMOVAL	600	SQ FT	
8	AY801371	REMOVE EDGE LIGHT BASE	12	EACH	
9	AY901510	SEEDING	0.3	ACRE	
10	AY908514	LIGHT-DUTY HYDRAULIC MULCH	0.3	ACRE	

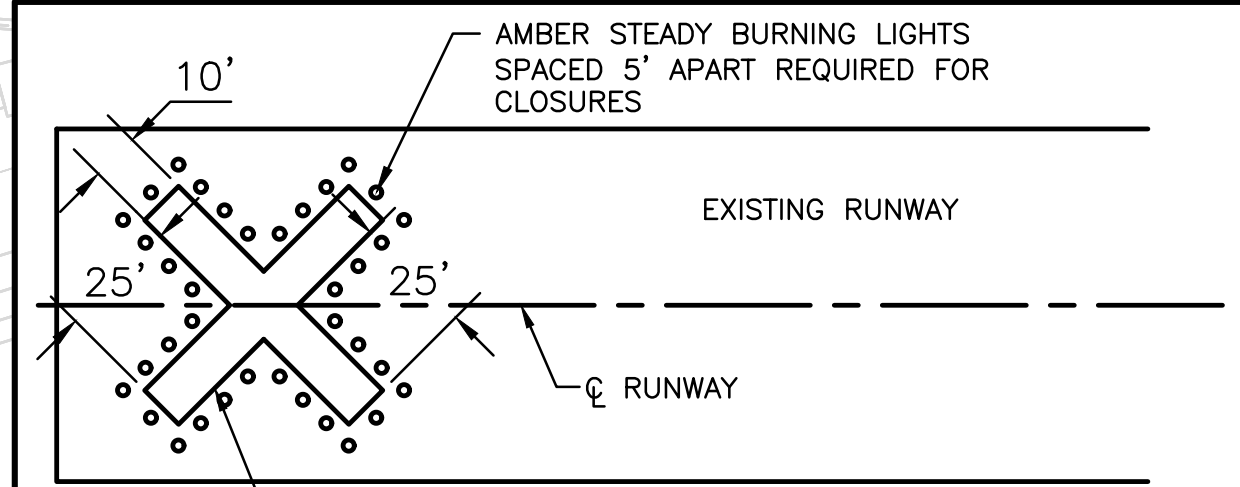
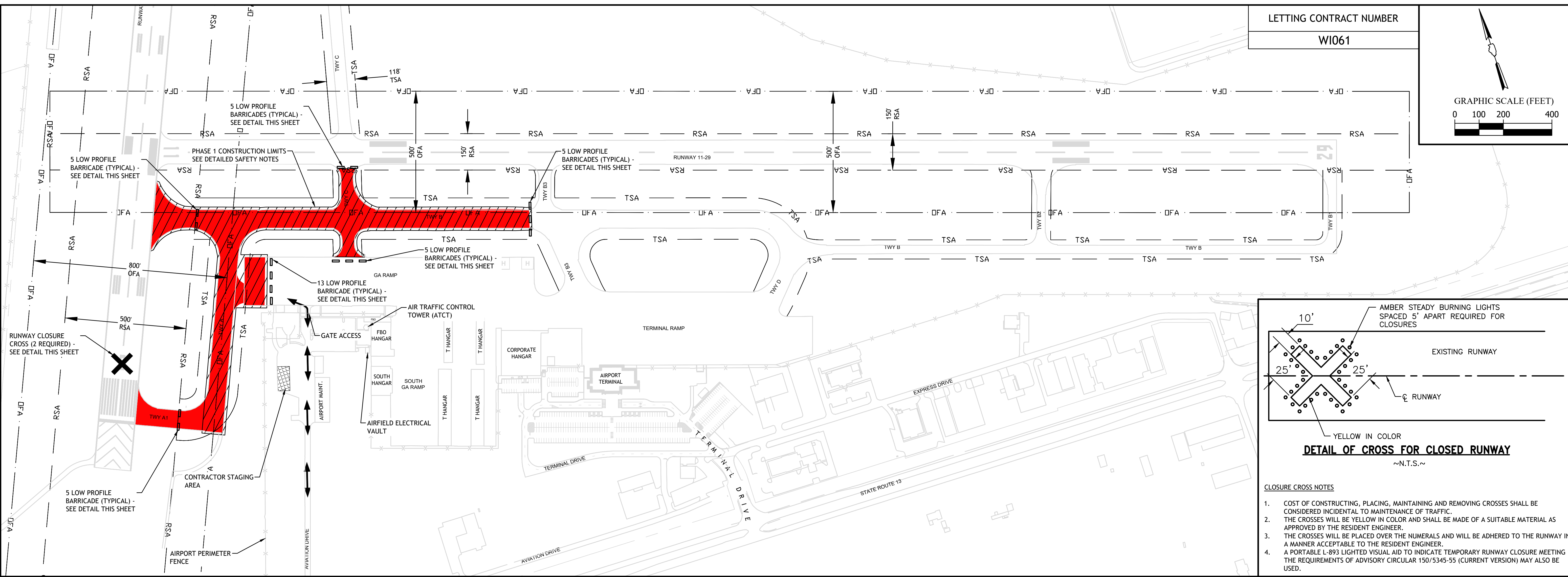
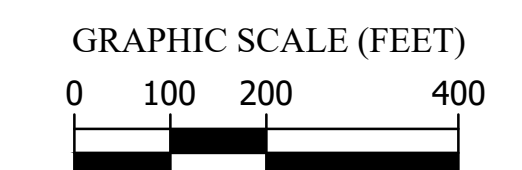
TAXIWAY A & B - ADDITIVE BID #1					
	PAY ITEM NO.	DESCRIPTION	QTY.	UNIT	AS BUILT QTY.
1	AX108158	1/C #8 5KV UG CABLE IN UD	1100	FOOT	
2	AX125932	REPLACE SIGN PANEL	6	EACH	
3	AX125962	RELOCATE BASE MOUNTED LIGHT	15	EACH	
4	AX125964	RELOCATE TAXI GUIDANCE SIGN	2	EACH	
5	AX150530	TRAFFIC MAINTENANCE	1	LSUM	
6	AX152410	UNCLASSIFIED EXCAVATION	325	CU YD	
7	AX152442	OFFSITE BORROW EXCAVATION	600	CU YD	
8	AX209510	CRUSHED AGGREGATE BASE COURSE	290	TON	
9	AX401610	BITUMINOUS SURFACE COURSE	150	TON	
10	AX401655	BUTT JOINT CONSTRUCTION	135	SQ YD	
11	AX401660	SAW & SEAL BIT. JOINTS	650	FOOT	
12	AX401900	REMOVE BITUMINOUS PAVEMENT	1225	SQ YD	
13	AX501515	PCC PAVEMENT, 15"	400	SQ YD	
14	AX501900	REMOVE PCC PAVEMENT	1010	SQ YD	
15	AX620520	PAVEMENT MARKING-WATERBORNE	675	SQ FT	
16	AX620525	PAVEMENT MARKING-BLACK BORDER	745	SQ FT	
17	AX801371	REMOVE EDGE LIGHT BASE	15	EACH	
18	AX901510	SEEDING	0.5	ACRE	
19	AX908514	LIGHT-DUTY HYDRAULIC MULCH	0.5	ACRE	

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SUMMARY OF QUANTITIES TAXIWAY ALPHA & BRAVO REHABILITATION VETERANS AIRPORT OF SOUTHERN ILLINOIS	
Date	3/1/2024
DR. By	BLW
CK'D By	MKW
Scale	N.T.S.
Job No.	19204CZ
Sheet No.	3 of 30

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LETTING CONTRACT NUMBER

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DETAIL OF CROSS FOR CLOSED RUNWAY
~N.T.S.~

CLOSURE CROSS NOTES

1. COST OF CONSTRUCTING, PLACING, MAINTAINING AND REMOVING CROSSES SHALL BE CONSIDERED INCIDENTAL TO MAINTENANCE OF TRAFFIC.
2. THE CROSSES WILL BE YELLOW IN COLOR AND SHALL BE MADE OF A SUITABLE MATERIAL AS APPROVED BY THE RESIDENT ENGINEER.
3. THE CROSSES WILL BE PLACED OVER THE NUMERALS AND WILL BE ADHERED TO THE RUNWAY IN A MANNER ACCEPTABLE TO THE RESIDENT ENGINEER.
4. A PORTABLE L-893 LIGHTED VISUAL AID TO INDICATE TEMPORARY RUNWAY CLOSURE MEETING THE REQUIREMENTS OF ADVISORY CIRCULAR 150/5345-55 (CURRENT VERSION) MAY ALSO BE USED.

GENERAL SAFETY NOTES

1. ALL CONSTRUCTION ACTIVITIES SHALL CONFORM TO THE CURRENT VERSION OF FAA ADVISORY CIRCULAR 150/5370-2, "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", AND THE AIRPORT SAFETY REQUIREMENTS CONTAINED IN THIS SAFETY AND PHASING PLAN.
2. AIRCRAFT AND EMERGENCY VEHICLES SHALL HAVE THE RIGHT OF WAY AT ALL TIMES.
3. APPROPRIATE NOTAMS (NOTICE TO AIRMEN) WILL BE ISSUED BY THE AIRPORT DIRECTOR. THE CONTRACTOR SHALL PROVIDE A MINIMUM OF 72 HOURS NOTICE PRIOR TO NAVOID OR PAVEMENT CLOSURES.
4. THE CONTRACTOR SHALL NOT ALLOW HIS/HER EMPLOYEES, INCLUDING SUBCONTRACTORS AND SUPPLIERS, TO CROSS OR PROCEED ON AN ACTIVE RUNWAY, TAXIWAY, OR CRITICAL/SAFETY AREA WITHOUT PRIOR AUTHORIZATION BY AIRPORT PERSONNEL.
5. STOCKPILED MATERIALS AND EQUIPMENT STORAGE ARE NOT PERMITTED WITHIN THE OBJECT FREE AREA (OFA) OF AN ACTIVE RUNWAY.
6. THE CONTRACTOR SHALL WORK ONLY WITHIN THE LIMITS SHOWN ON THE PLANS. ALL EQUIPMENT AND PERSONNEL SHALL BE LIMITED TO THE STAGING AREA, DESIGNATED HAUL ROUTES, OR WITHIN THE CONSTRUCTION LIMITS. ALL PERSONAL VEHICLES SHALL BE PARKED WITHIN THE CONTRACTORS STAGING AREA.
7. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY DAMAGE TO AIRPORT PROPERTY, TO THE SATISFACTION OF THE AIRPORT, CAUSED BY CONSTRUCTION PERSONNEL OR ACTIVITIES AT HIS/HER OWN EXPENSE.
8. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND PAVEMENTS BEFORE THE COMPLETION OF EACH PHASE TO THE SATISFACTION OF AIRPORT PERSONNEL.
9. EXISTING TURF AREAS OUTSIDE CONSTRUCTION LIMITS DISTURBED BY THE CONTRACTORS OPERATIONS SHALL BE RESTORED AT HIS/HER EXPENSE TO THE SATISFACTION OF THE AIRPORT.
10. HEIGHT OF CONSTRUCTION EQUIPMENT MAXIMUM IS ANTICIPATED TO BE 25', WHICH IS A RAISED TRUCK BED.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING TAXIWAY EDGE LIGHT CIRCUIT.
12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL UTILITY COMPANIES LINES IN THE WORK AREA. CALL J.U.L.I.E. (1-800-892-0123) FOR UTILITY LOCATING.
13. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN

BARRICADES AND CROSSES AS DIRECTED BY THE ENGINEER. THE BARRICADES WILL BE EQUIPPED WITH RED FLASHING LIGHTS. THE BARRICADES, THEIR MAINTENANCE, PLACEMENT AND REMOVAL WILL BE CONSIDERED AS AN INCIDENTAL TO MAINTENANCE OF TRAFFIC AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

14. IDENTIFICATION - WHEN THE CONTRACTOR'S VEHICLES AND EQUIPMENT ARE ON THE AIRPORT THEY SHALL BE PROPERLY MARKED WITH THREE FOOT (3) SQUARE CHECKERED AVIATION FLAGS (INTERNATIONAL ORANGE AND WHITE) OR FLASHING DOME TYPE AMBER COLORED BEACONS VISIBLE FROM THE CONTROL TOWER.
15. THE CONTRACTOR SHALL FURNISH AIRPORT RADIOS FOR HIS/HER USE. RADIOS SHALL BE CAPABLE OF MONITORING AND TRANSMITTING OVER THE AIRPORT'S GROUND CONTROL FREQUENCY (121.7 MHZ). THE CONTRACTOR SHALL MONITOR THE GROUND CONTROL FREQUENCY CONTINUOUSLY WHEN THE ATCT IS OPERATIONAL, 7:00 AM TO 7:00 PM. WHEN THE ATCT IS CLOSED THE CONTRACTOR SHALL MONITOR FREQUENCY (128.40).
16. CONTRACTOR PERSONNEL OPERATING VEHICLES ON THE AIRFIELD SHALL ATTEND AIRPORT DRIVERS TRAINING CLASS OR BE ESCORTED BY PERSONNEL WHO HAVE COMPLETED TRAINING PRIOR TO BEGINNING OF CONSTRUCTION.
17. A LIMITED NUMBER OF GATE ACCESS CARDS SHALL BE PROVIDED BY THE OWNER FOR USE DURING THE PROJECT. WHEN THE GATE IS REQUIRED TO REMAIN OPEN FOR HAUL VEHICLES THE CONTRACTOR SHALL MAINTAIN SECURITY (GATE GUARD) AT ACCESS GATES. THE GATES SHALL BE CLOSED AT THE END OF EACH WORK DAY. ALL COST FOR GATE GUARDS SHALL BE INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION SHALL BE ALLOWED.
18. RUNWAY AND TAXIWAY EDGE LIGHTS AND SIGNS WITHIN CLOSED PORTIONS OF PROJECT SHALL BE DISCONNECTED OR COVERED. IF EDGE LIGHTS ARE DISCONNECTED THE CONTRACTOR SHALL INSTALL A TEMPORARY #8 5KV JUMPER CABLE TO MAINTAIN POWER TO EDGE LIGHTS AND GUIDANCE SIGNS OUTSIDE THE CONSTRUCTION LIMITS. JUMPER CABLE SHALL BE INSTALLED ABOVE GROUND ENTIRELY IN CONDUIT ACROSS TAXIWAY IN A MANNER THAT PROTECTS IT FROM VEHICULAR TRAFFIC. COST OF TEMPORARY JUMPER SHALL BE INCIDENTAL TO MAINTENANCE OF TRAFFIC.
19. THE CONTRACTOR SHALL PREPARE AND SUBMIT TO THE ENGINEER A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) PRIOR TO BEGINNING WORK. THE INTENT OF THE SPCD IS TO DOCUMENT HOW THE CONTRACTOR WILL COMPLY WITH CONSTRUCTION SAFETY AND PHASING PLAN (CSPP). SEE CURRENT VERSION OF FAA ADVISORY CIRCULAR (AC) 150/5370-2 FOR ADDITIONAL INFORMATION.

DETAILED SAFETY NOTES

1. PHASE 1 SHALL CONSIST OF ALL WORK ON TAXIWAY A & B WEST OF TAXIWAY B3, EXCLUDING SEEDING AND FULL APPLICATION OF PAVEMENT MARKINGS. WORK FOR BID ADDITIVE 1, IF ACCEPTED, SHALL BE INCLUDED. TOTAL CONTRACT TIME FOR THE PROJECT WILL BE AS LISTED BELOW AND IS NOT PHASE SPECIFIC.

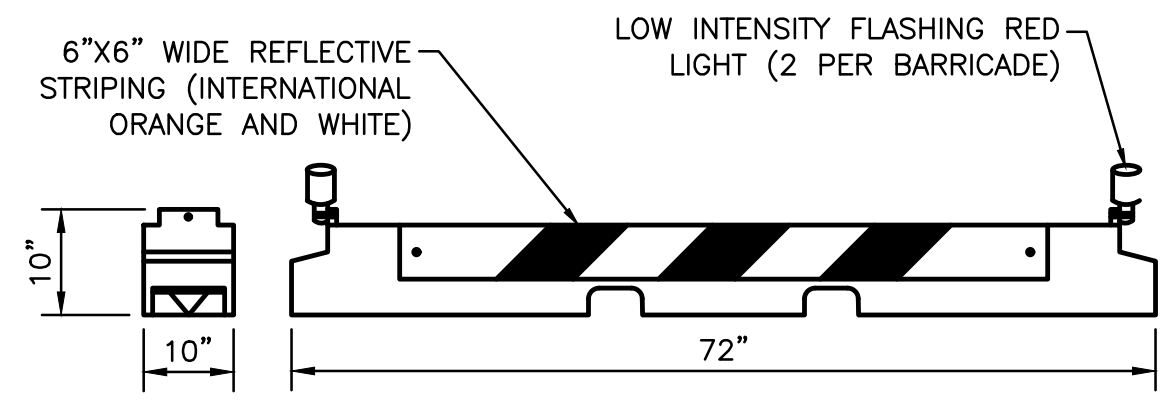
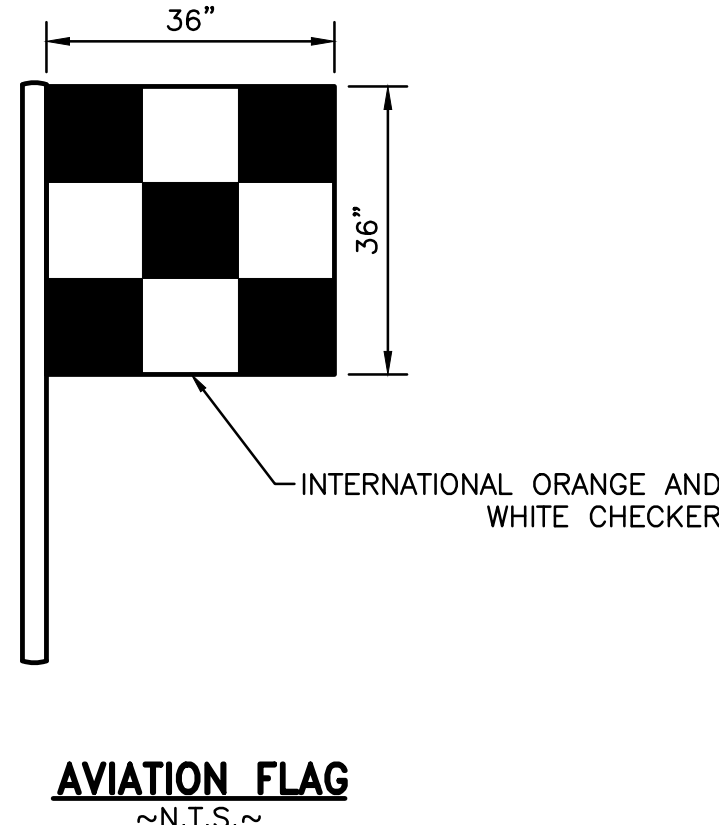
BASE BID - 112 CALENDAR DAYS
ADDITIVE BID 1 (IF ACCEPTED) - ADDITIONAL 12 CALENDAR DAYS
ADDITIVE BID 2 (IF ACCEPTED) - ADDITIONAL 6 CALENDAR DAYS
TOTAL ALL BIDS ACCEPTED - 130 CALENDAR DAYS
2. SHOULD BID ADDITIVE 1 NOT BE ACCEPTED CONTRACTOR CAN ACCESS WORK AREA VIA TAXIWAY C.
3. THE CONTRACTOR SHALL TO THE BEST OF HIS/HER ABILITY NOT PARK EQUIPMENT OR STORE MATERIALS WITHIN THE OBJECT FREE AREA (OFA) OF RUNWAY 2-20 OR RUNWAY 11-29.
4. WORK WITHIN THE RUNWAY 2-20 SAFETY AREA (RSA) SHALL BE COMPLETED WITH RUNWAY 2-20 CLOSED BETWEEN THE HOURS OF 8:00 PM AND 5:00 AM. ALL EQUIPMENT, MATERIALS, AND PERSONNEL SHALL BE CLEAR THE RSA NO LATER THAN 5:00 AM. THE CONTRACTOR SHALL NOT LEAVE THE SITE UNTIL AIRPORT PERSONNEL HAVE CONDUCTED A SAFETY INSPECTION AND DEEMED RUNWAY 2-20 SAFE TO REOPEN.
5. LIGHTED RUNWAY CLOSURE CROSSES SHALL BE ACTIVE DURING RUNWAY CLOSURES. COST OF CLOSURE CROSSES AND THEIR SETUP SHALL BE INCIDENTAL TO MAINTENANCE OF TRAFFIC.

LEGEND

PHASE 1 LIMITS	
AIRFIELD PAVEMENT CLOSURE	
CONTRACTOR STAGING AREA	
HAUL ROUTE	
RUNWAY SAFETY AREA	
TAXIWAY SAFETY AREA	
RUNWAY OBJECT FREE AREA	
LOW PROFILE BARRICADE	
RUNWAY CLOSURE CROSS	

AIRFIELD OPERATIONAL LIMITATIONS

FACILITY	PHASE 1 LIMITATIONS
RUNWAY 2-20	NIGHT TIME CLOSURE (8:00 PM TO 5:00 AM) AS NEEDED
RUNWAY 11-29	NONE
TAXIWAY A	CLOSED
TAXIWAY A1	CLOSED
TAXIWAY B	CLOSED WEST OF TAXIWAY B3
TAXIWAY B1	NONE
TAXIWAY B2	NONE
TAXIWAY B3	NONE
TAXIWAY C	CLOSED SOUTH OF RUNWAY 11-29
TAXIWAY D	NONE



LOW PROFILE BARRICADE DETAIL
~N.T.S.~

BARRICADE NOTES

1. LOW PROFILE BARRICADES SHALL MEET THE REQUIREMENTS OF FAA ADVISORY CIRCULAR 150/5370-2 (CURRENT VERSION).
2. BARRICADES MAXIMUM SPACING SHALL BE 15 FEET CENTER TO CENTER.
3. BARRICADES SHALL BE WEIGHTED TO PREVENT MOVEMENT.
4. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING PROPER POSITIONING OF ALL BARRICADES.
5. ALL COST TO PROVIDE AND MAINTAIN BARRICADES SHALL BE INCLUDED IN THE COST OF THE PROJECT.

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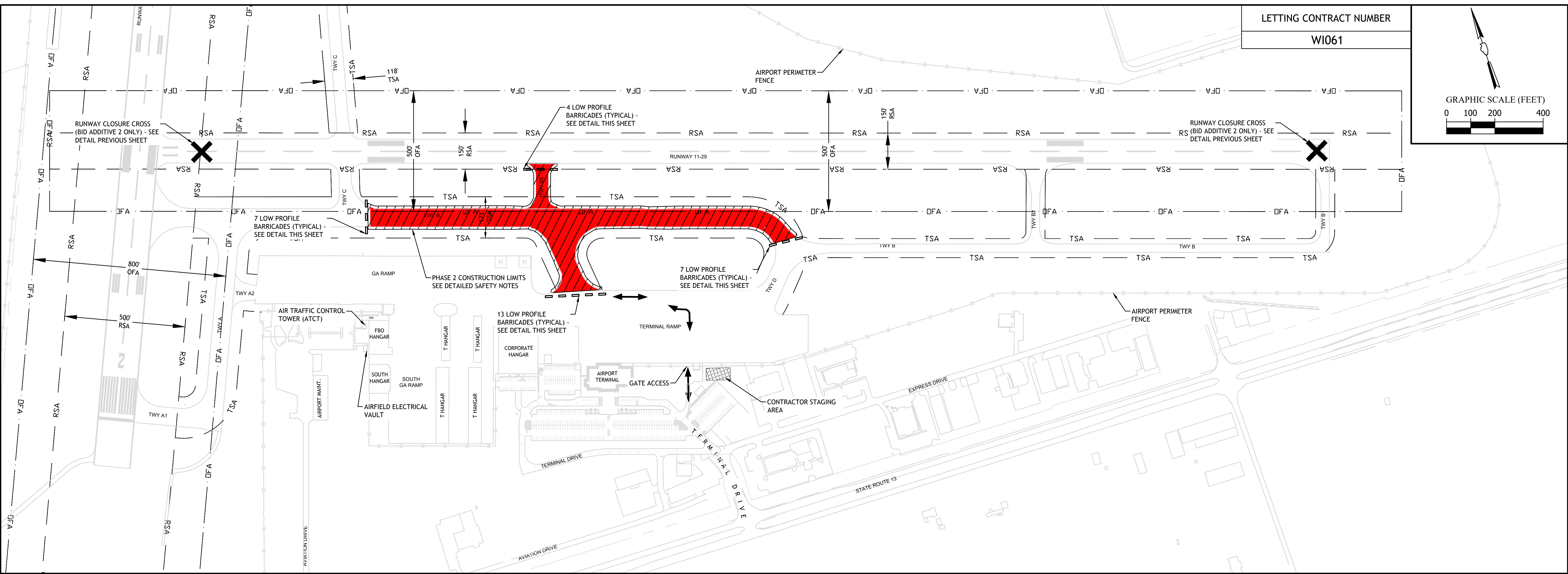
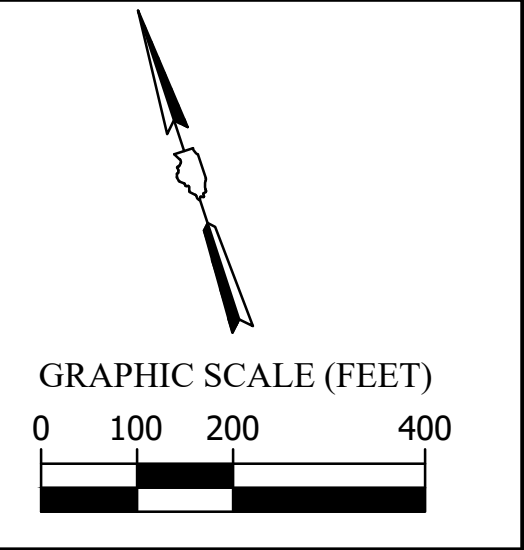
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618-993-6411 FAX 618-993-6750
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SAFETY AND PHASING - PHASE 1
TAXIWAY ALPHA & BRAVO REHABILITATION
VETERANS AIRPORT OF SOUTHERN ILLINOIS

Date 3/1/2024 DR. By BLW CK'D By MKW
Scale 1" = 100' Job No. 19204CZ Sheet No. 4 of 30

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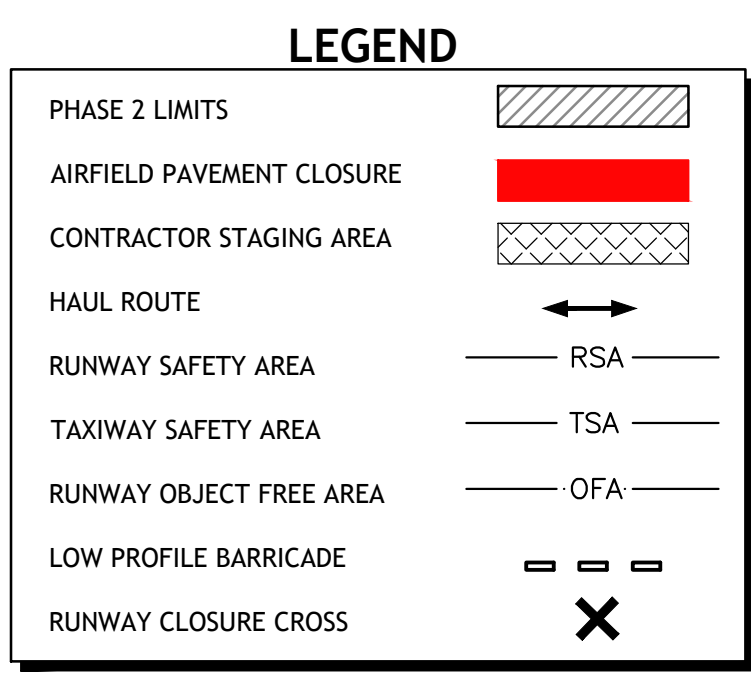
LETTING CONTRACT NUMBER
WI061



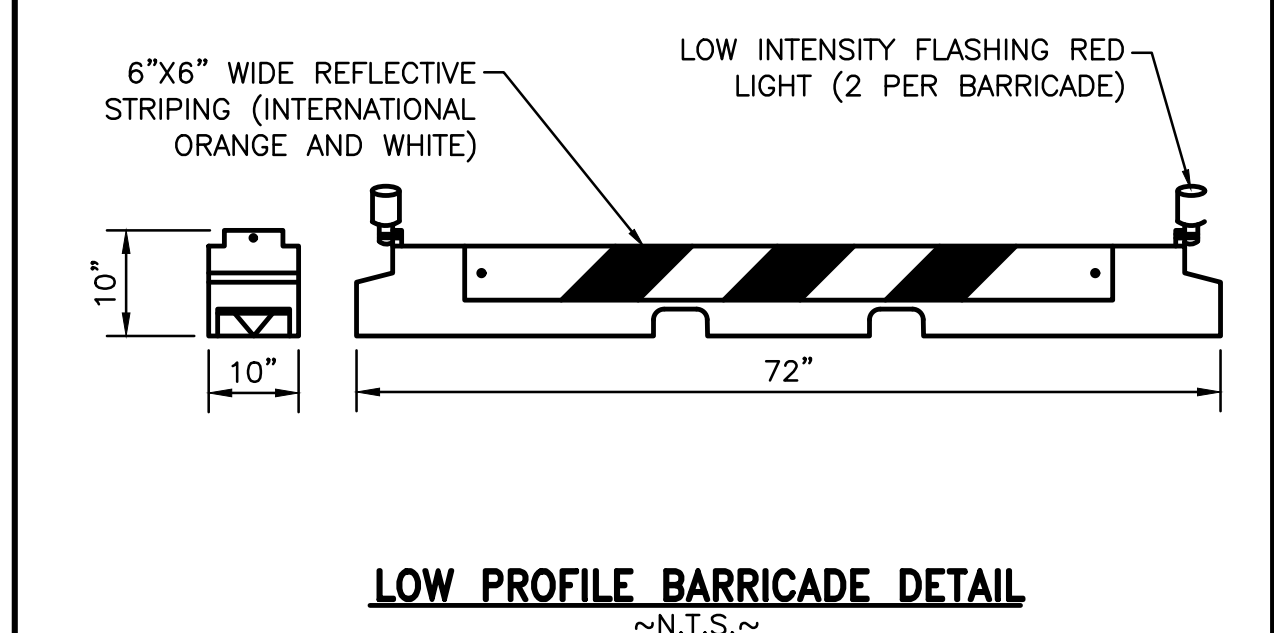
- GENERAL SAFETY NOTES**
- ALL CONSTRUCTION ACTIVITIES SHALL CONFORM TO THE CURRENT VERSION OF FAA ADVISORY CIRCULAR 150/5370-2, "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", AND THE AIRPORT SAFETY REQUIREMENTS CONTAINED IN THIS SAFETY AND PHASING PLAN.
 - AIRCRAFT AND EMERGENCY VEHICLES SHALL HAVE THE RIGHT OF WAY AT ALL TIMES.
 - APPROPRIATE NOTAMS (NOTICE TO AIRMEN) WILL BE ISSUED BY THE AIRPORT DIRECTOR. THE CONTRACTOR SHALL PROVIDE A MINIMUM OF 72 HOURS NOTICE PRIOR TO NAVAID OR PAVEMENT CLOSURES.
 - THE CONTRACTOR SHALL NOT ALLOW HIS/HER EMPLOYEES, INCLUDING SUBCONTRACTORS AND SUPPLIERS, TO CROSS OR PROCEED ON AN ACTIVE RUNWAY, TAXIWAY, OR CRITICAL/SAFETY AREA WITHOUT PRIOR AUTHORIZATION BY AIRPORT PERSONNEL.
 - STOCKPILED MATERIALS AND EQUIPMENT STORAGE ARE NOT PERMITTED WITHIN THE OBJECT FREE AREA (OFA) OF AN ACTIVE RUNWAY.
 - THE CONTRACTOR SHALL WORK ONLY WITHIN THE LIMITS SHOWN ON THE PLANS. ALL EQUIPMENT AND PERSONNEL SHALL BE LIMITED TO THE STAGING AREA, DESIGNATED HAUL ROUTES, OR WITHIN THE CONSTRUCTION LIMITS. ALL PERSONAL VEHICLES SHALL BE PARKED WITHIN THE CONTRACTORS STAGING AREA.
 - THE CONTRACTOR SHALL REPAIR OR REPLACE ANY DAMAGE TO AIRPORT PROPERTY, TO THE SATISFACTION OF THE AIRPORT, CAUSED BY CONSTRUCTION PERSONNEL OR ACTIVITIES AT HIS/HER OWN EXPENSE.
 - THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND PAVEMENTS BEFORE THE COMPLETION OF EACH PHASE TO THE SATISFACTION OF AIRPORT PERSONNEL.
 - EXISTING TURF AREAS OUTSIDE CONSTRUCTION LIMITS DISTURBED BY THE CONTRACTORS OPERATIONS SHALL BE RESTORED AT HIS/HER EXPENSE TO THE SATISFACTION OF THE AIRPORT.
 - HEIGHT OF CONSTRUCTION EQUIPMENT MAXIMUM IS ANTICIPATED TO BE 25', WHICH IS A RAISED TRUCK BED.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING TAXIWAY EDGE LIGHT CIRCUIT.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL UTILITY COMPANIES LINES IN THE WORK AREA. CALL J.U.L.I.E. (1-800-892-0123) FOR UTILITY LOCATING.
 - IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN

- BARRICADES AND CROSSES AS DIRECTED BY THE ENGINEER. THE BARRICADES WILL BE EQUIPPED WITH RED FLASHING LIGHTS. THE BARRICADES, THEIR MAINTENANCE, PLACEMENT AND REMOVAL WILL BE CONSIDERED AS AN INCIDENTAL TO MAINTENANCE OF TRAFFIC AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- IDENTIFICATION - WHEN THE CONTRACTOR'S VEHICLES AND EQUIPMENT ARE ON THE AIRPORT THEY SHALL BE PROPERLY MARKED WITH THREE FOOT (3) SQUARE CHECKERED AVIATION FLAGS (INTERNATIONAL ORANGE AND WHITE) OR FLASHING DOME TYPE AMBER COLORED BEACONS VISIBLE FROM THE CONTROL TOWER.
 - THE CONTRACTOR SHALL FURNISH AIRPORT RADIOS FOR HIS/HER USE. RADIOS SHALL BE CAPABLE OF MONITORING AND TRANSMITTING OVER THE AIRPORT'S GROUND CONTROL FREQUENCY (121.7 MHZ). THE CONTRACTOR SHALL MONITOR THE GROUND CONTROL FREQUENCY CONTINUOUSLY WHEN THE ATCT IS OPERATIONAL, 7:00 AM TO 7:00 PM. WHEN THE ATCT IS CLOSED THE CONTRACTOR SHALL MONITOR FREQUENCY (128.40).
 - CONTRACTOR PERSONNEL OPERATING VEHICLES ON THE AIRFIELD SHALL ATTEND AIRPORT DRIVERS TRAINING CLASS OR BE ESCORTED BY PERSONNEL WHO HAVE COMPLETED TRAINING PRIOR TO BEGINNING OF CONSTRUCTION.
 - A LIMITED NUMBER OF GATE ACCESS CARDS SHALL BE PROVIDED BY THE OWNER FOR USE DURING THE PROJECT. WHEN THE GATE IS REQUIRED TO REMAIN OPEN FOR HAUL VEHICLES THE CONTRACTOR SHALL MAINTAIN SECURITY (GATE GUARD) AT ACCESS GATES. THE GATES SHALL BE CLOSED AT THE END OF EACH WORK DAY. ALL COST FOR GATE GUARDS SHALL BE INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION SHALL BE ALLOWED.
 - RUNWAY AND TAXIWAY EDGE LIGHTS AND SIGNS WITHIN CLOSED PORTIONS OF PROJECT SHALL BE DISCONNECTED OR COVERED. IF EDGE LIGHTS ARE DISCONNECTED THE CONTRACTOR SHALL INSTALL A TEMPORARY #8 5KV JUMPER CABLE TO MAINTAIN POWER TO EDGE LIGHTS AND GUIDANCE SIGNS OUTSIDE THE CONSTRUCTION LIMITS. JUMPER CABLE SHALL BE INSTALLED ABOVE GROUND ENTIRELY IN CONDUIT ACROSS TAXIWAY IN A MANNER THAT PROTECTS IT FROM VEHICULAR TRAFFIC. COST OF TEMPORARY JUMPER SHALL BE INCIDENTAL TO MAINTENANCE OF TRAFFIC.
 - THE CONTRACTOR SHALL PREPARE AND SUBMIT TO THE ENGINEER A SAFETY PLAN COMPLIANCE DOCUMENT (SPCP) PRIOR TO BEGINNING WORK. THE INTENT OF THE SPCD IS TO DOCUMENT HOW THE CONTRACTOR WILL COMPLY WITH CONSTRUCTION SAFETY AND PHASING PLAN (CSPP). SEE CURRENT VERSION OF FAA ADVISORY CIRCULAR (AC) 150/5370-2 FOR ADDITIONAL INFORMATION.

- DETAILED SAFETY NOTES**
- PHASE 2 SHALL CONSIST OF ALL WORK ON TAXIWAY B & B3 BETWEEN TAXIWAY C AND TAXIWAY D, EXCLUDING SEEDING AND FULL APPLICATION OF PAVEMENT MARKINGS. WORK FOR BID ADDITIVE 2, IF ACCEPTED, SHALL BE INCLUDED. TOTAL CONTRACT TIME FOR THE PROJECT WILL BE AS LISTED BELOW AND IS NOT PHASE SPECIFIC.
BASE BID - 112 CALENDAR DAYS
ADDITIVE BID 1 (IF ACCEPTED) - ADDITIONAL 12 CALENDAR DAYS
ADDITIVE BID 2 (IF ACCEPTED) - ADDITIONAL 6 CALENDAR DAYS
TOTAL ALL BIDS ACCEPTED - 130 CALENDAR DAYS
 - THE CONTRACTOR SHALL PROVIDE A DEDICATED FLAGGER DURING OPERATIONS REQUIRING MULTIPLE HAUL TRUCKS CROSSING THE TERMINAL RAMP.
 - RUNWAY 11-29 SHALL BE CLOSED DURING WORK WITHIN THE RUNWAY SAFETY AREA (RSA), BID ADDITIVE 2 ONLY.
 - THE CONTRACTOR SHALL TO THE BEST OF HIS/HER ABILITY NOT PARK EQUIPMENT OR STORE MATERIALS WITHIN THE OBJECT FREE AREA (OFA) OF RUNWAY 11-29.
 - LIGHTED RUNWAY CLOSURE CROSSES SHALL BE ACTIVE DURING RUNWAY CLOSURES. COST OF CLOSURE CROSSES AND THEIR SETUP SHALL BE INCIDENTAL TO MAINTENANCE OF TRAFFIC.



AIRFIELD OPERATIONAL LIMITATIONS	
FACILITY	PHASE 2 LIMITATIONS
RUNWAY 2-20	NONE
RUNWAY 11-29	DAYTIME CLOSURE AS NEEDED
TAXIWAY A	NONE
TAXIWAY A1	NONE
TAXIWAY A2	NONE
TAXIWAY B (BETWEEN TAXIWAY C AND D)	CLOSED
TAXIWAY B (EAST OF TAXIWAY D)	NONE
TAXIWAY B (WEST OF TAXIWAY C)	NONE
TAXIWAY B1	NONE
TAXIWAY B2	NONE
TAXIWAY B3	CLOSED
TAXIWAY C	NONE
TAXIWAY D	NONE



- BARRICADE NOTES**
- LOW PROFILE BARRICADES SHALL MEET THE REQUIREMENTS OF FAA ADVISORY CIRCULAR 150/5370-2 (CURRENT VERSION).
 - BARRICADES MAXIMUM SPACING SHALL BE 15 FEET CENTER TO CENTER.
 - BARRICADES SHALL BE WEIGHTED TO PREVENT MOVEMENT.
 - CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING PROPER POSITIONING OF ALL BARRICADES.
 - ALL COST TO PROVIDE AND MAINTAIN BARRICADES SHALL BE INCLUDED IN THE COST OF THE PROJECT.

STATE OF ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184-004515

REVISIONS	
DATE	NO.

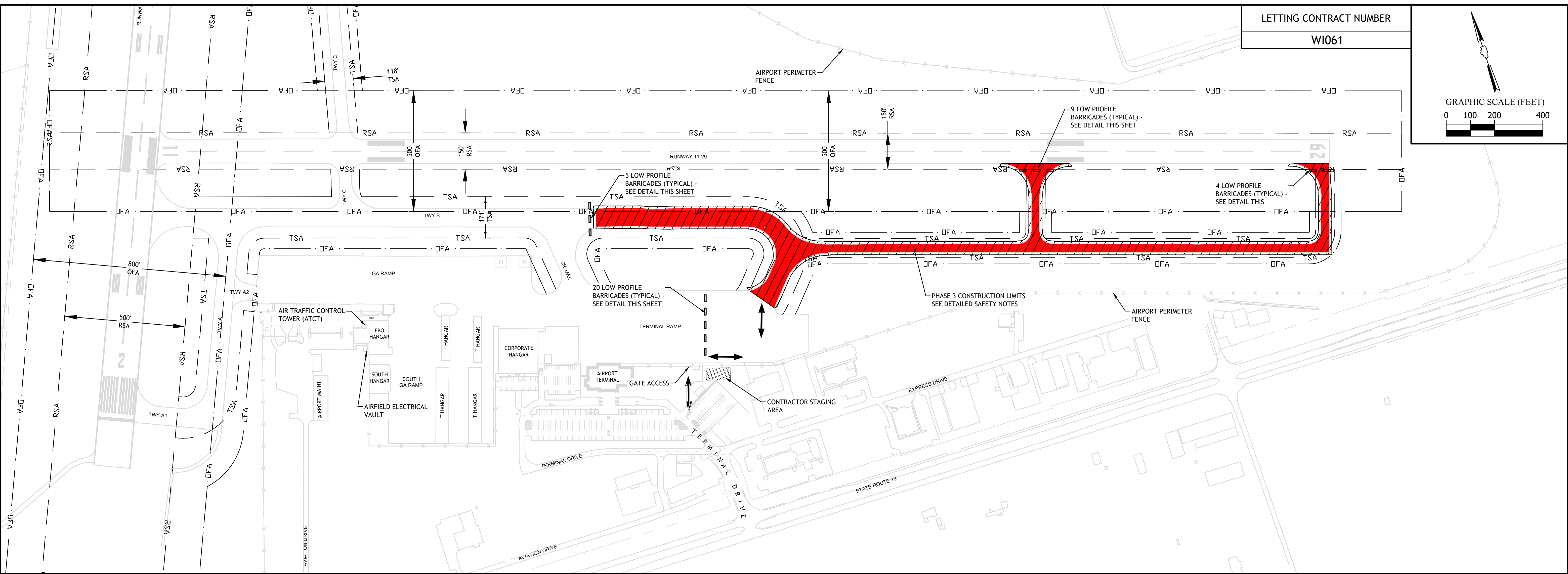
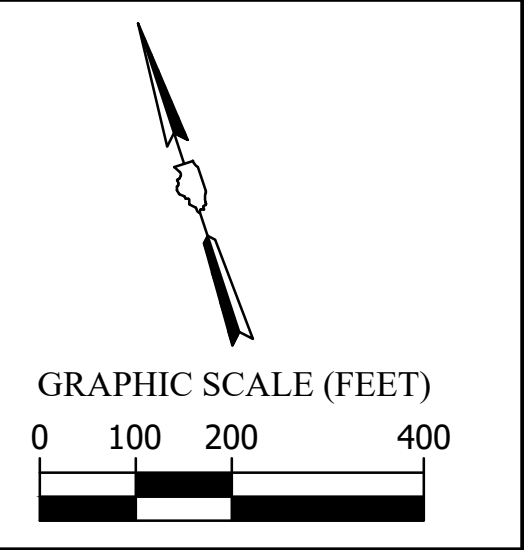
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**SAFETY AND PHASING - PHASE 2
TAXIWAY ALPHA & BRAVO REHABILITATION
VETERANS AIRPORT OF SOUTHERN ILLINOIS**

Date: 3/1/2024 DR. By: BLW CK'D By: MKW
Scale: 1" = 200' Job No.: 19204CZ Sheet No.: 5 of 30

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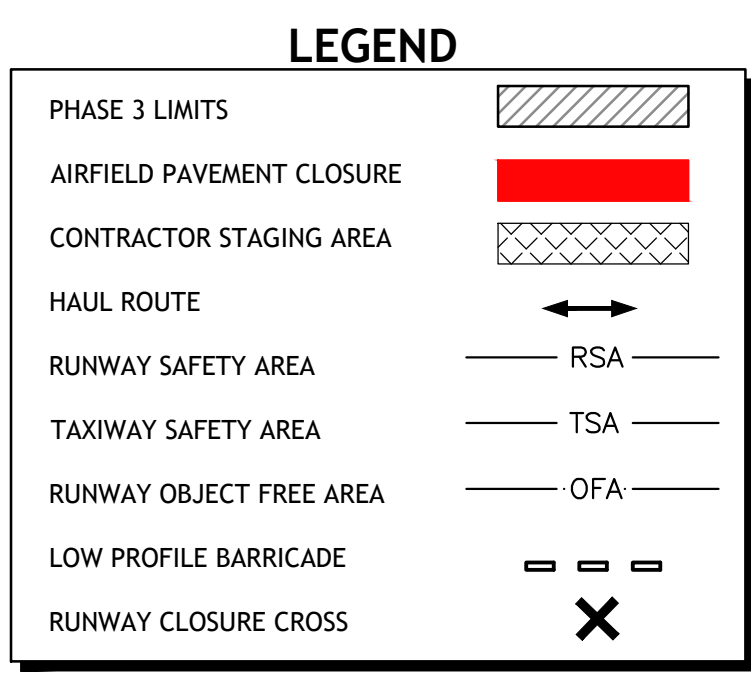
LETTING CONTRACT NUMBER
WI061



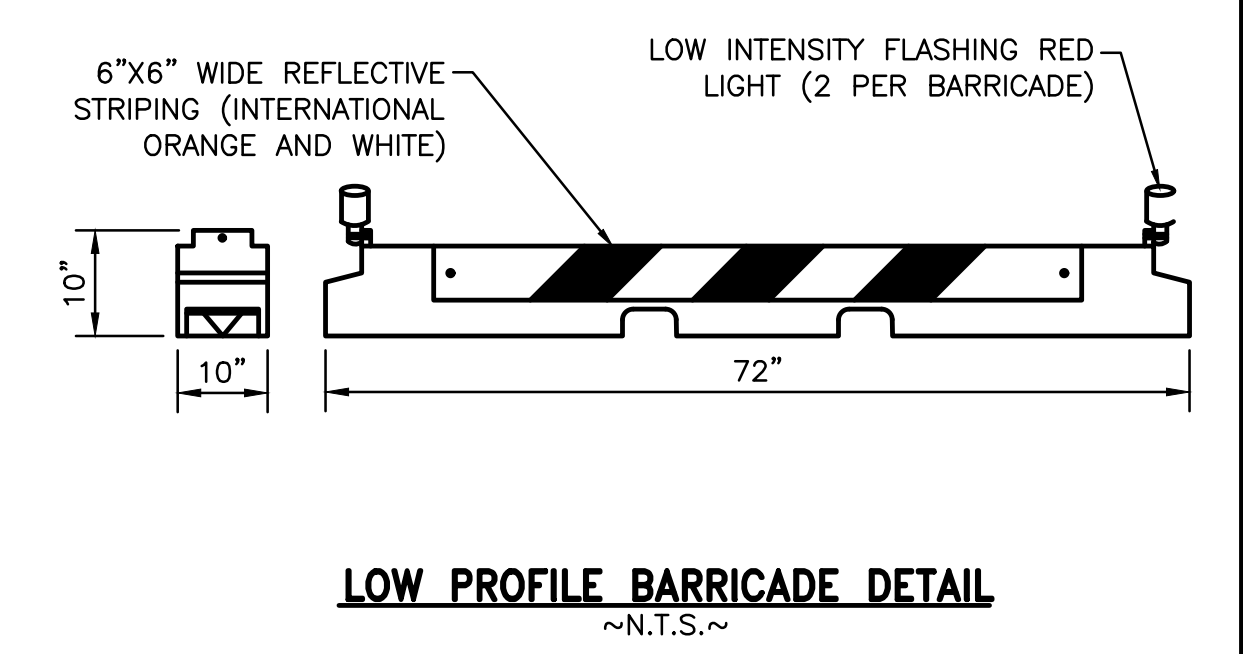
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 - IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN

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- DETAILED SAFETY NOTES**
- PHASE 3 SHALL CONSIST OF ALL WORK ON TAXIWAY B & D EAST OF TAXIWAY B3 EXCLUDING SEEDING AND FULL APPLICATION OF PAVEMENT MARKINGS. TOTAL CONTRACT TIME FOR THE PROJECT WILL BE AS LISTED BELOW AND IS NOT PHASE SPECIFIC.
BASE BID - 112 CALENDAR DAYS
ADDITIVE BID 1 (IF ACCEPTED) - ADDITIONAL 12 CALENDAR DAYS
ADDITIVE BID 2 (IF ACCEPTED) - ADDITIONAL 6 CALENDAR DAYS
TOTAL ALL BIDS ACCEPTED - 130 CALENDAR DAYS
 - RUNWAY 11-29 SHALL BE CLOSED DURING WORK WITHIN THE RUNWAY SAFETY AREA (RSA), BID ADDITIVE 2 ONLY.
 - THE CONTRACTOR SHALL TO THE BEST OF HIS/HER ABILITY NOT PARK EQUIPMENT OR STORE MATERIALS WITHIN THE OBJECT FREE AREA (OFA) OF RUNWAY 11-29.



AIRFIELD OPERATIONAL LIMITATIONS	
FACILITY	PHASE 3 LIMITATIONS
RUNWAY 2-20	NONE
RUNWAY 11-29	DAYTIME CLOSURE AS NEEDED
TAXIWAY A	NONE
TAXIWAY A1	NONE
TAXIWAY A2	NONE
TAXIWAY B (EAST OF TAXIWAY B3)	CLOSED
TAXIWAY B (WEST OF TAXIWAY B3)	NONE
TAXIWAY B1	CLOSED
TAXIWAY B2	CLOSED
TAXIWAY B3	NONE
TAXIWAY C	NONE
TAXIWAY D	CLOSED



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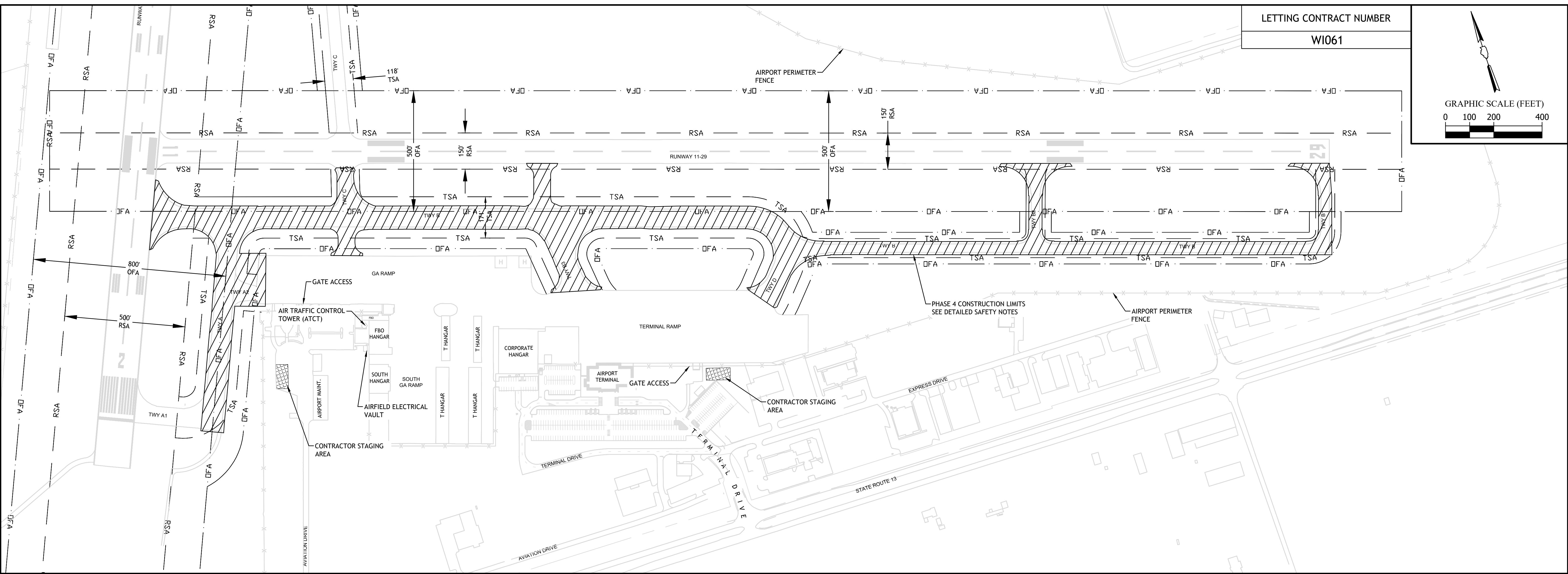
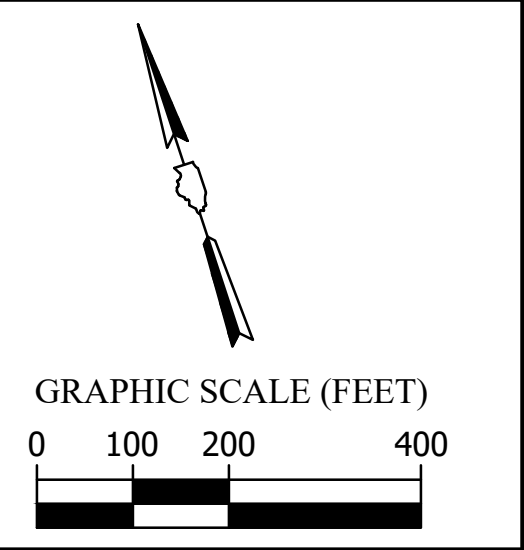
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**SAFETY AND PHASING - PHASE 3
TAXIWAY ALPHA & BRAVO REHABILITATION
VETERANS AIRPORT OF SOUTHERN ILLINOIS**

Date: 3/1/2024 DR. By: BLW CK'D By: MKW
Scale: 1" = 200' Job No.: 19204CZ Sheet No.: 6 of 30

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LETTING CONTRACT NUMBER
WI061



GENERAL SAFETY NOTES

- ALL CONSTRUCTION ACTIVITIES SHALL CONFORM TO THE CURRENT VERSION OF FAA ADVISORY CIRCULAR 150/5370-2, "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", AND THE AIRPORT SAFETY REQUIREMENTS CONTAINED IN THIS SAFETY AND PHASING PLAN.
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- IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN

DETAILED SAFETY NOTES

- PHASE 4 SHALL CONSIST OF PAVEMENT MARKINGS AND SEEDING AND SHALL BE COMPLETED UNDER AIR TRAFFIC CONTROL TOWER (ATCT) DIRECTION BETWEEN THE HOURS OF 7:00 AM AND 7:00 PM. TOTAL CONTRACT TIME FOR THE PROJECT WILL BE AS LISTED BELOW AND IS NOT PHASE SPECIFIC.
BASE BID - 112 CALENDAR DAYS
ADDITIVE BID 1 (IF ACCEPTED) - ADDITIONAL 12 CALENDAR DAYS
ADDITIVE BID 2 (IF ACCEPTED) - ADDITIONAL 6 CALENDAR DAYS
TOTAL ALL BIDS ACCEPTED - 130 CALENDAR DAYS
- ALL VEHICLES OPERATING INSIDE THE CONSTRUCTION LIMITS SHALL BE IN CONTACT WITH ATCT AND BE PREPARED TO MOVE IF DIRECTED.
- ALL EQUIPMENT AND MATERIALS SHALL BE REMOVED FROM THE CONSTRUCTION LIMITS AT THE END OF EACH DAY.
- IDENTIFICATION - WHEN THE CONTRACTOR'S VEHICLES AND EQUIPMENT ARE ON THE AIRPORT THEY SHALL BE PROPERLY MARKED WITH THREE FOOT (3) SQUARE CHECKERED AVIATION FLAGS (INTERNATIONAL ORANGE AND WHITE) OR FLASHING DOME TYPE AMBER COLORED BEACONS VISIBLE FROM THE CONTROL TOWER.
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LEGEND



AIRFIELD OPERATIONAL LIMITATIONS	
FACILITY	PHASE 4 LIMITATIONS
RUNWAY 2-20	NONE
RUNWAY 11-29	NONE
TAXIWAY A	NONE
TAXIWAY A1	NONE
TAXIWAY A2	NONE
TAXIWAY B (BETWEEN TAXIWAY C AND D)	NONE
TAXIWAY B (EAST OF TAXIWAY D)	NONE
TAXIWAY B (WEST OF TAXIWAY C)	NONE
TAXIWAY B1	NONE
TAXIWAY B2	NONE
TAXIWAY B3	NONE
TAXIWAY C	NONE
TAXIWAY D	NONE

STATE OF ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184-004515

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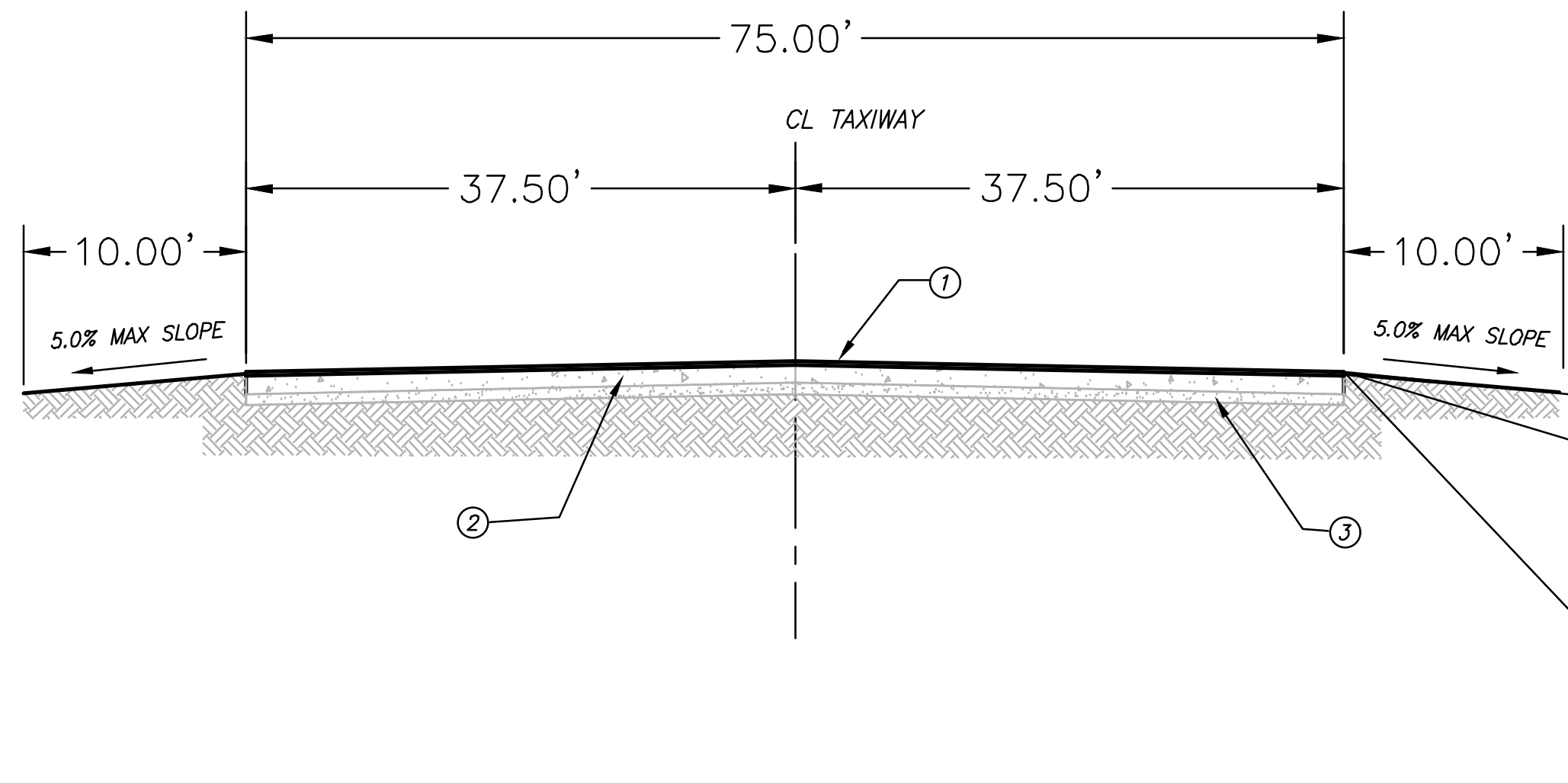
**SAFETY AND PHASING - PHASE 4
TAXIWAY ALPHA & BRAVO REHABILITATION
VETERANS AIRPORT OF SOUTHERN ILLINOIS**

Date 3/1/2024 DR. By BLW CK'D By MKW

Scale 1" = 200' Job No. 19204CZ Sheet No. 7 of 30

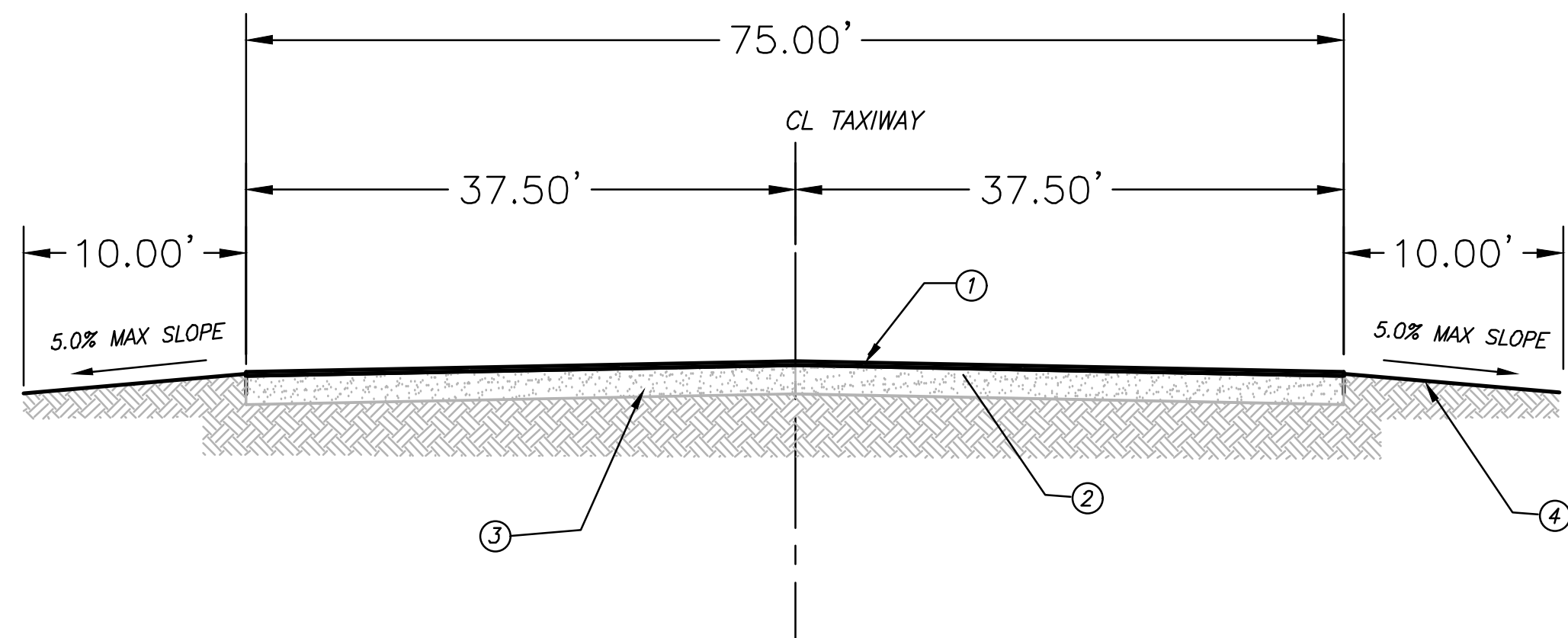
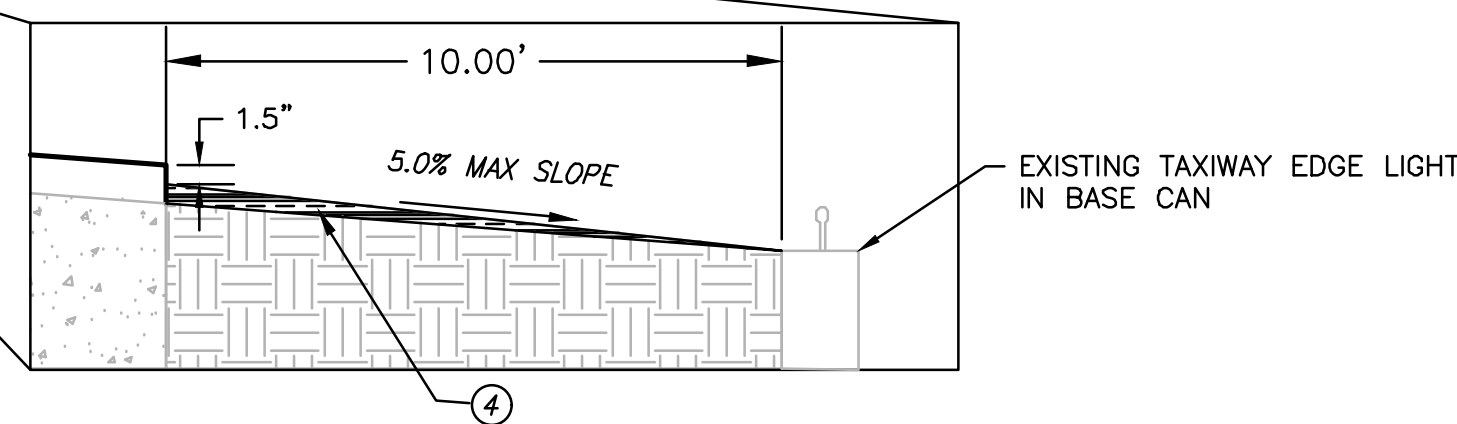
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**TYPICAL SECTION - TAXIWAY A , TAXIWAY B,
STATION 3+60 To 19+40, & TAXIWAY B3**



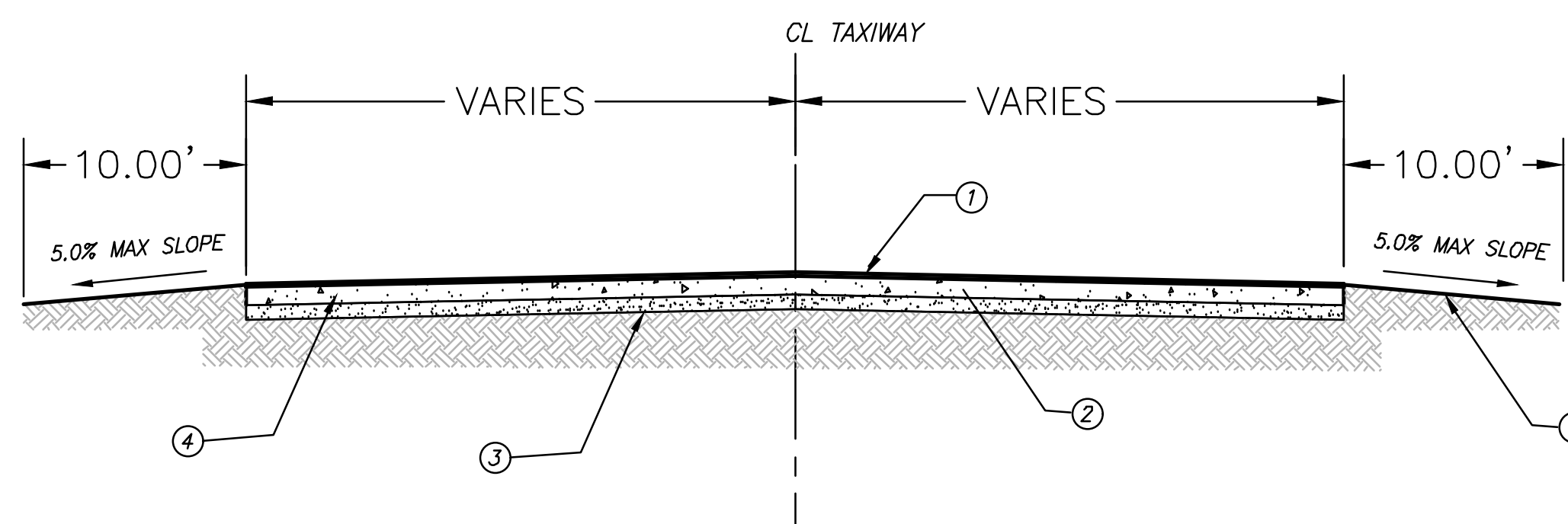
NOT TO SCALE

- ① 2 LIFTS OF 1.5" P-401 BITUMINOUS SURFACE COURSE
- ② 15" - EXISTING P.C.C.
- ③ 9" - EXISTING BITUMINOUS BASE
- ④ TOPSOIL VARIABLE DEPTH



NOT TO SCALE

- ① 2 LIFTS OF 1.5" P-401 BITUMINOUS SURFACE COURSE
- ② 1.5" - HMA SURFACE REMOVAL
- ③ 23.25" - EXISTING BITUMINOUS PAVEMENT
- ④ TOPSOIL VARIABLE DEPTH

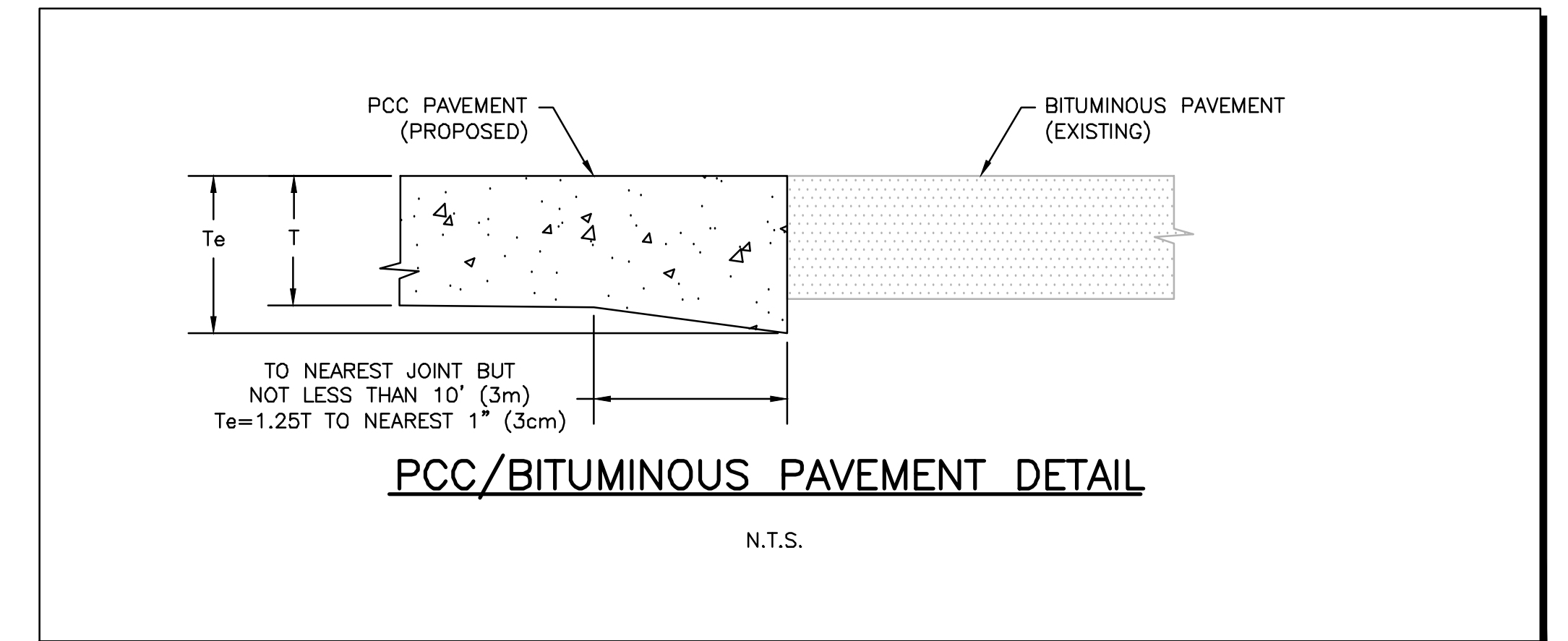


NOT TO SCALE

- ① 2 LIFTS OF 1.5" P-401 BITUMINOUS SURFACE COURSE
- ② 15" - P-501 CONCRETE
- ③ 12" - P-209 CRUSHED AGGREGATE BASE
- ④ TOPSOIL VARIABLE DEPTH

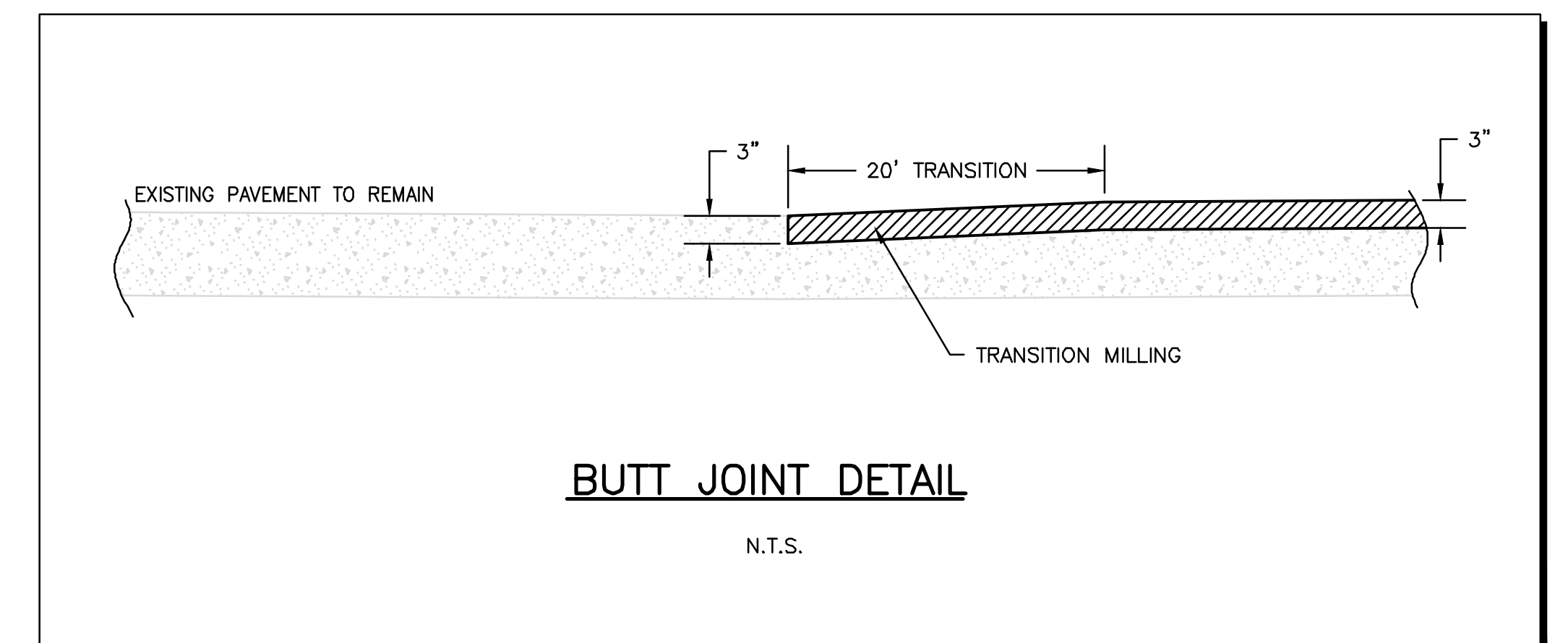
PAVEMENT NOTES:

1. PAVEMENTS ARE DESIGNED FOR AIRCRAFT OVER 60,000 POUNDS. ALL MATERIALS AND TESTING SHALL CONFORM TO THESE REQUIREMENTS.
2. CONTRACTOR SHALL PROVIDE 16 FOOT STRAIGHT EDGE FOR USE BY ENGINEER TO VERIFY PAVEMENT SMOOTHNESS CRITERIA IS BEING MET IN ACCORDANCE WITH SPECIFICATION P-401.
3. P-603 TACK COAT SHALL BE INCIDENTAL TO THE BITUMINOUS PAVEMENT CONSTRUCTION.
4. TOPSOIL SHALL BE PLACED AS NEEDED TO ENSURE A 1.5" DROP OFF FROM EDGE OF PAVEMENT TO UNPAVED SHOULDER.



PCC/BITUMINOUS PAVEMENT DETAIL

N.T.S.



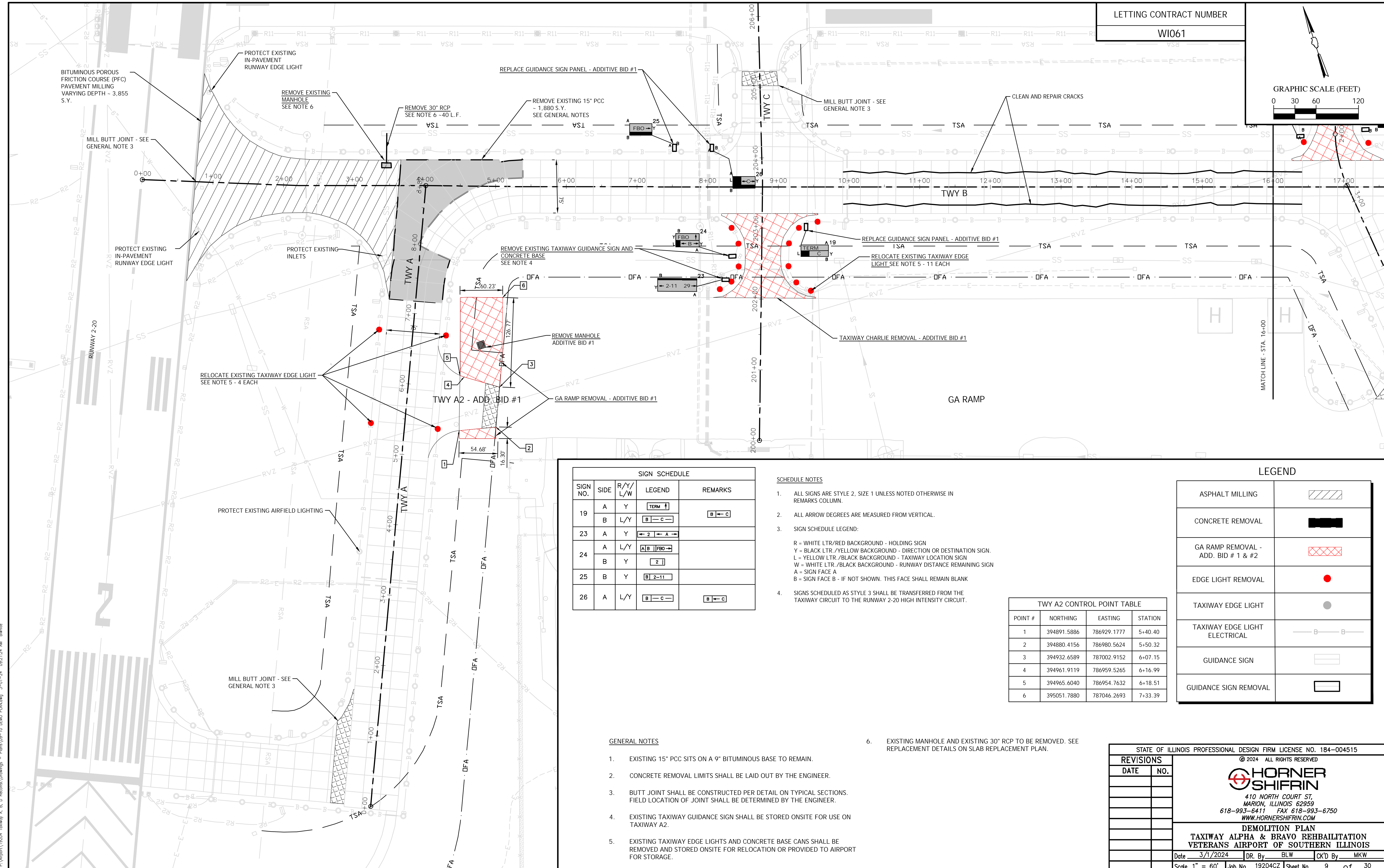
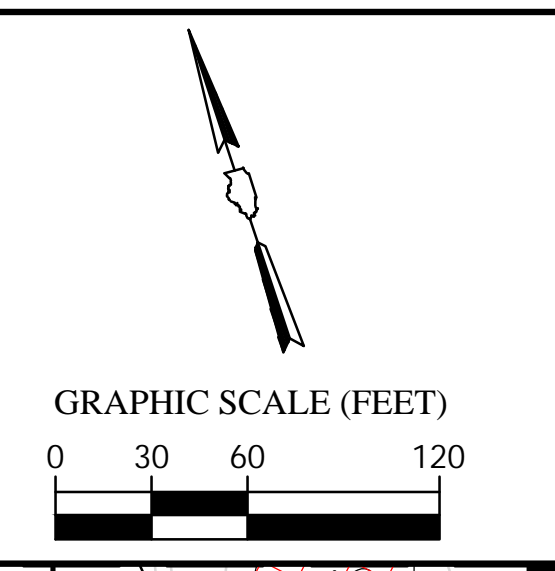
BUTT JOINT DETAIL

N.T.S.

P:\airport\19204 Taxiway A, B, D Reconst\Drawings - Plans\07 Typical Section.dwg 2-26-24 02:38:26 PM bwhite

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410 NORTH COURT ST. MARION, ILLINOIS 62859 618-993-6411 FAX 618-993-6750 WWW.HORNERSHIFFRIN.COM	
TYPICAL SECTIONS TAXIWAY ALPHA & BRAVO REHABILITATION VETERANS AIRPORT OF SOUTHERN ILLINOIS	
Date	3/1/2024
DR. By	BLW
CK'D By	MKW
Scale	NTS
Job No.	19204CZ
Sheet No.	8 of 30

LETTING CONTRACT NUMBER
W1061



SIGN SCHEDULE				
SIGN NO.	SIDE	R/Y/L/W	LEGEND	REMARKS
19	A	Y		
	B	L/Y		
23	A	Y		
24	A	L/Y		
	B	Y		
25	B	Y		
26	A	L/Y		

SCHEDULE NOTES

- ALL SIGNS ARE STYLE 2, SIZE 1 UNLESS NOTED OTHERWISE IN REMARKS COLUMN.
- ALL ARROW DEGREES ARE MEASURED FROM VERTICAL.
- SIGN SCHEDULE LEGEND:
R = WHITE LTR./RED BACKGROUND - HOLDING SIGN
Y = BLACK LTR./YELLOW BACKGROUND - DIRECTION OR DESTINATION SIGN.
L = YELLOW LTR./BLACK BACKGROUND - TAXIWAY LOCATION SIGN
W = WHITE LTR./BLACK BACKGROUND - RUNWAY DISTANCE REMAINING SIGN
A = SIGN FACE A
B = SIGN FACE B - IF NOT SHOWN, THIS FACE SHALL REMAIN BLANK
- SIGNS SCHEDULED AS STYLE 3 SHALL BE TRANSFERRED FROM THE TAXIWAY CIRCUIT TO THE RUNWAY 2-20 HIGH INTENSITY CIRCUIT.

POINT #	NORTHING	EASTING	STATION
1	394891.5886	786929.1777	5+40.40
2	394880.4156	786980.5624	5+50.32
3	394932.6589	787002.9152	6+07.15
4	394961.9119	786959.5265	6+16.99
5	394965.6040	786954.7632	6+18.51
6	395051.7880	787046.2693	7+33.39

LEGEND	
ASPHALT MILLING	
CONCRETE REMOVAL	
GA RAMP REMOVAL - ADD. BID #1 & #2	
EDGE LIGHT REMOVAL	
TAXIWAY EDGE LIGHT	
TAXIWAY EDGE LIGHT ELECTRICAL	
GUIDANCE SIGN	
GUIDANCE SIGN REMOVAL	

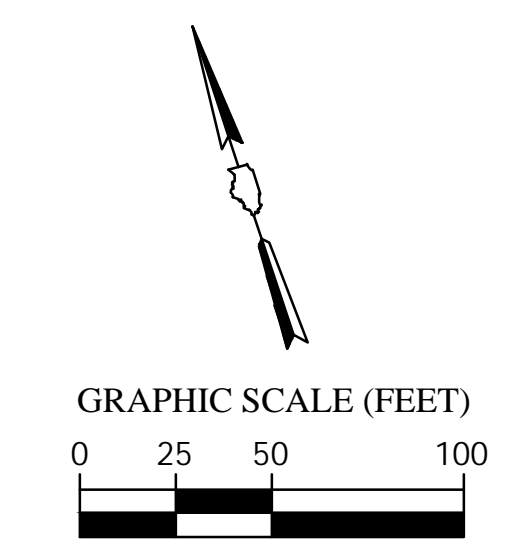
- GENERAL NOTES**
- EXISTING 15" PCC SITS ON A 9" BITUMINOUS BASE TO REMAIN.
 - CONCRETE REMOVAL LIMITS SHALL BE LAID OUT BY THE ENGINEER.
 - BUTT JOINT SHALL BE CONSTRUCTED PER DETAIL ON TYPICAL SECTIONS. FIELD LOCATION OF JOINT SHALL BE DETERMINED BY THE ENGINEER.
 - EXISTING TAXIWAY GUIDANCE SIGN SHALL BE STORED ONSITE FOR USE ON TAXIWAY A2.
 - EXISTING TAXIWAY EDGE LIGHTS AND CONCRETE BASE CANS SHALL BE REMOVED AND STORED ONSITE FOR RELOCATION OR PROVIDED TO AIRPORT FOR STORAGE.

- EXISTING MANHOLE AND EXISTING 30" RCP TO BE REMOVED. SEE REPLACEMENT DETAILS ON SLAB REPLACEMENT PLAN.

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 410 NORTH COURT ST, MARION, ILLINOIS 62959 618-993-6411 FAX 618-993-6750 WWW.HORNERSHIFRIN.COM	
DEMOLITION PLAN TAXIWAY ALPHA & BRAVO REHABILITATION VETERANS AIRPORT OF SOUTHERN ILLINOIS	
Date	DR. By
3/1/2024	BLW
CK'D By	MKW
Scale 1" = 60'	Job No. 19204CZ
Sheet No. 9	of 30

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LETTING CONTRACT NUMBER
W1061



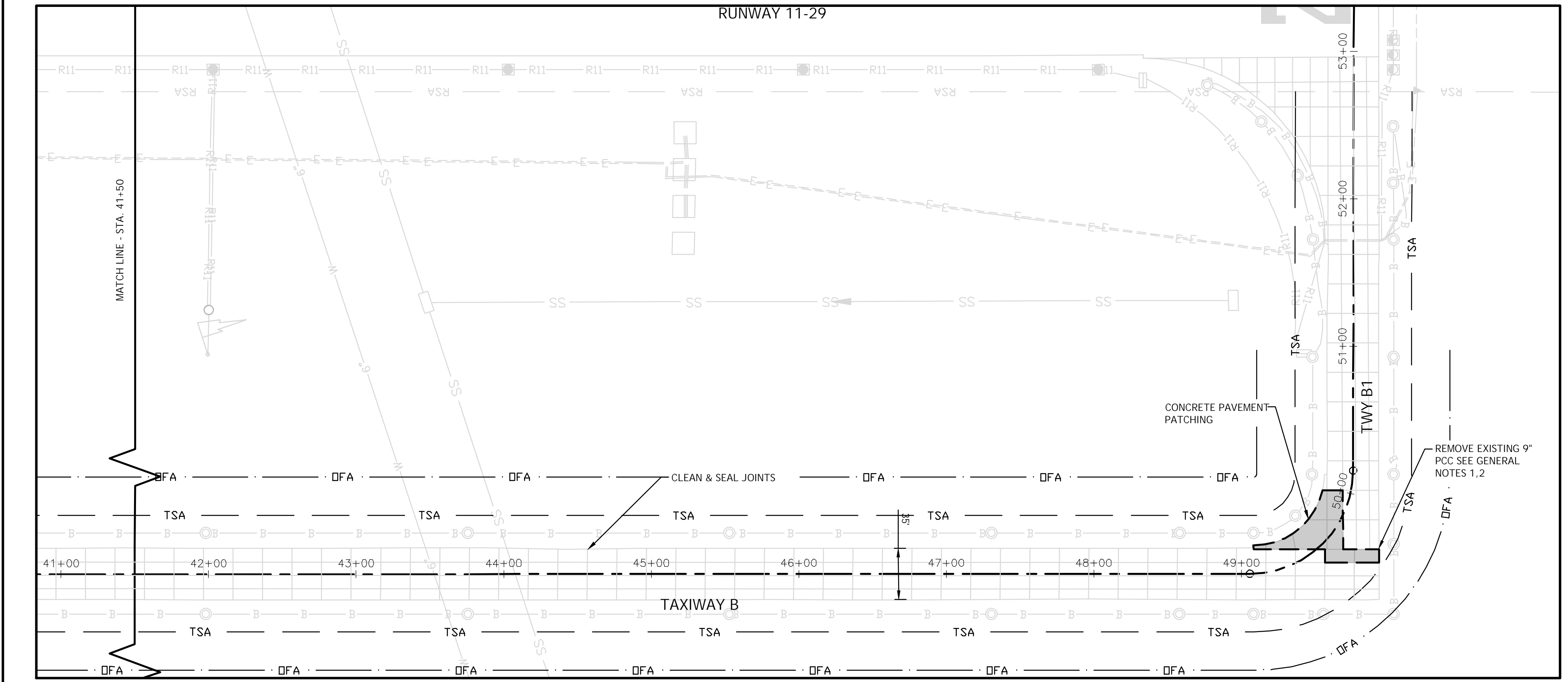
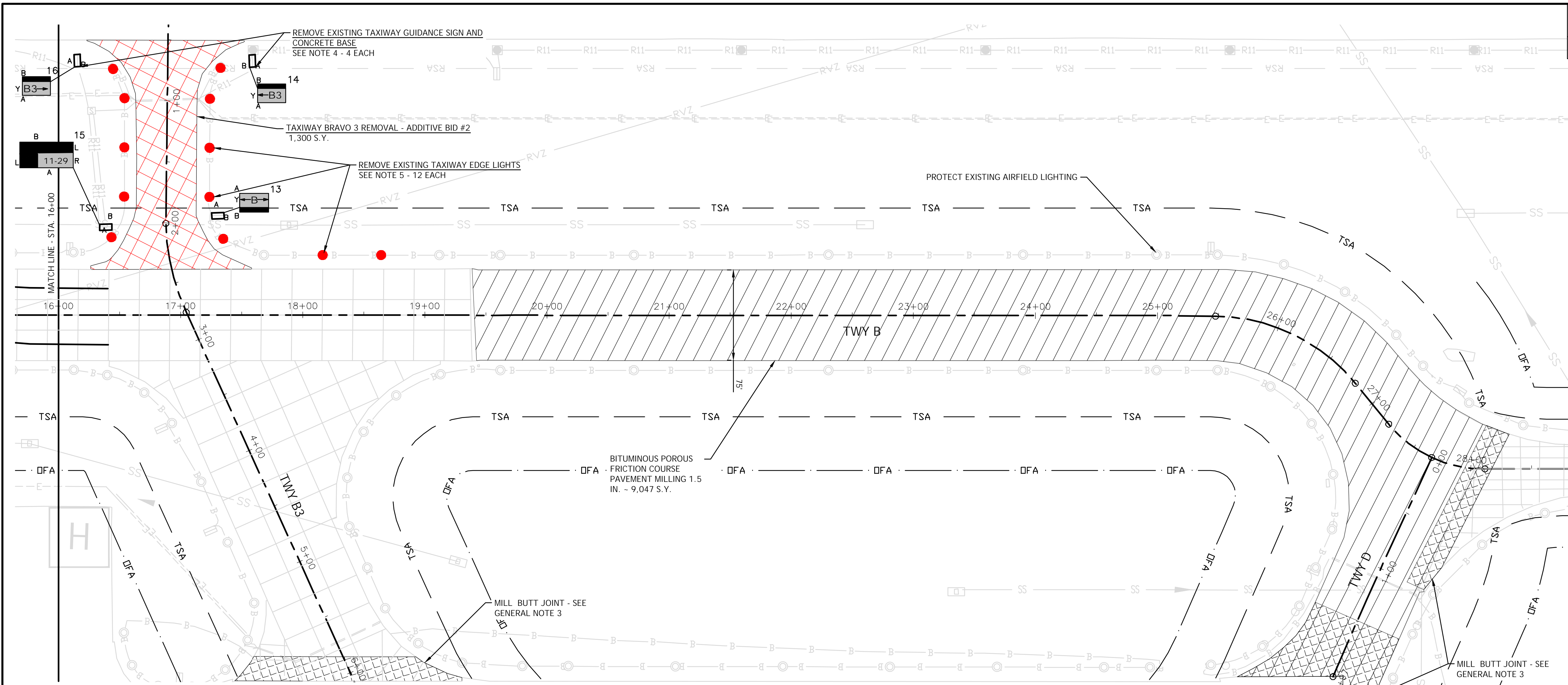
LEGEND

ASPHALT MILLING	
CONCRETE REMOVAL	
TAXIWAY REMOVAL - ALT BID # 1 & #2	
EDGE LIGHT REMOVAL	
TAXIWAY EDGE LIGHT	
TAXIWAY EDGE LIGHT ELECTRICAL	
GUIDANCE SIGN	
GUIDANCE SIGN REMOVAL	

GENERAL NOTES

- EXISTING 9" PCC SITS ON A 4" COMPACTED SUBBASE TO REMAIN.
- CONCRETE REMOVAL LIMITS SHALL BE LAID OUT BY THE ENGINEER.
- BUTT JOINT SHALL BE CONSTRUCTED PER DETAIL ON TYPICAL SECTIONS. FIELD LOCATION OF JOINT SHALL BE DETERMINED BY THE ENGINEER.
- EXISTING TAXIWAY GUIDANCE SIGN SHALL BE STORED ONSITE FOR USE ON TAXIWAY A2 OR PROVIDED TO AIRPORT FOR STORAGE.
- EXISTING TAXIWAY EDGE LIGHTS AND CONCRETE BASE CANS SHALL BE REMOVED AND STORED ONSITE FOR RELOCATION OR PROVIDED TO AIRPORT FOR STORAGE.

SIGN SCHEDULE				
SIGN NO.	SIDE	R/Y/L/W	LEGEND	REMARKS
13	A	Y		
14	A	Y		
15	A	L/R		
	B	L		
16	A	Y		



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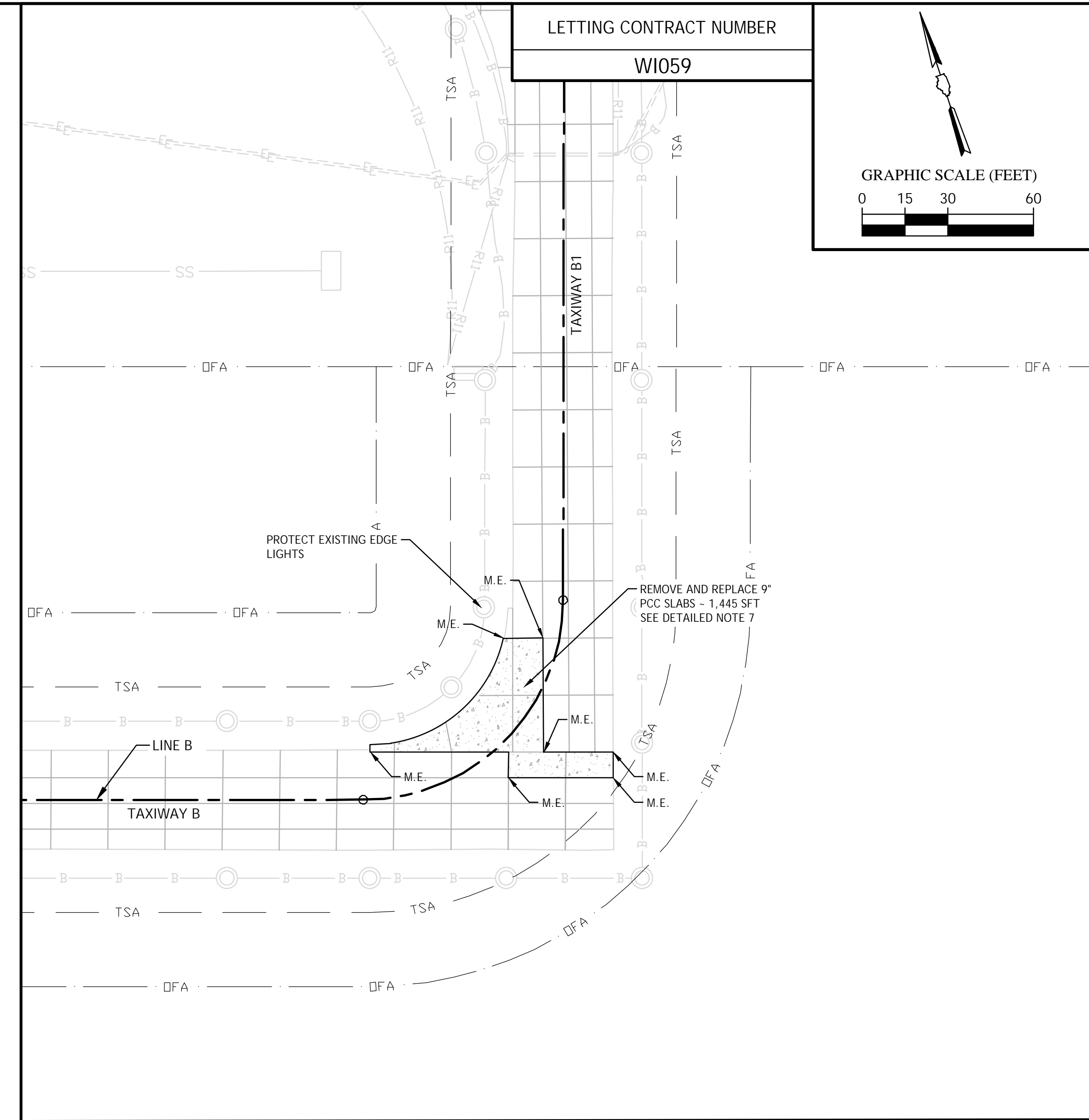
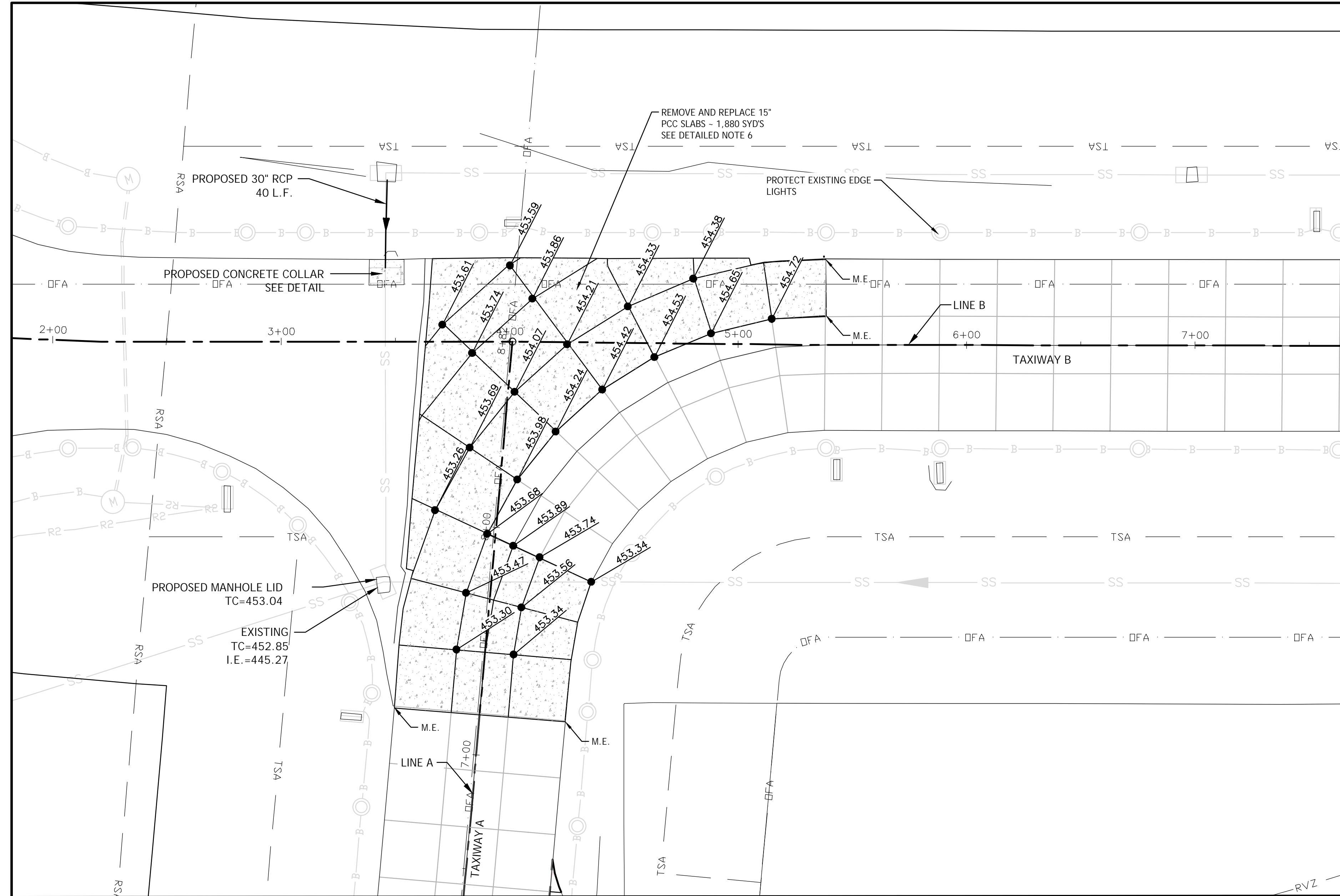
DEMOLITION PLAN
TAXIWAY ALPHA & BRAVO REHABILITATION
VETERANS AIRPORT OF SOUTHERN ILLINOIS

REVISIONS		
DATE	NO.	

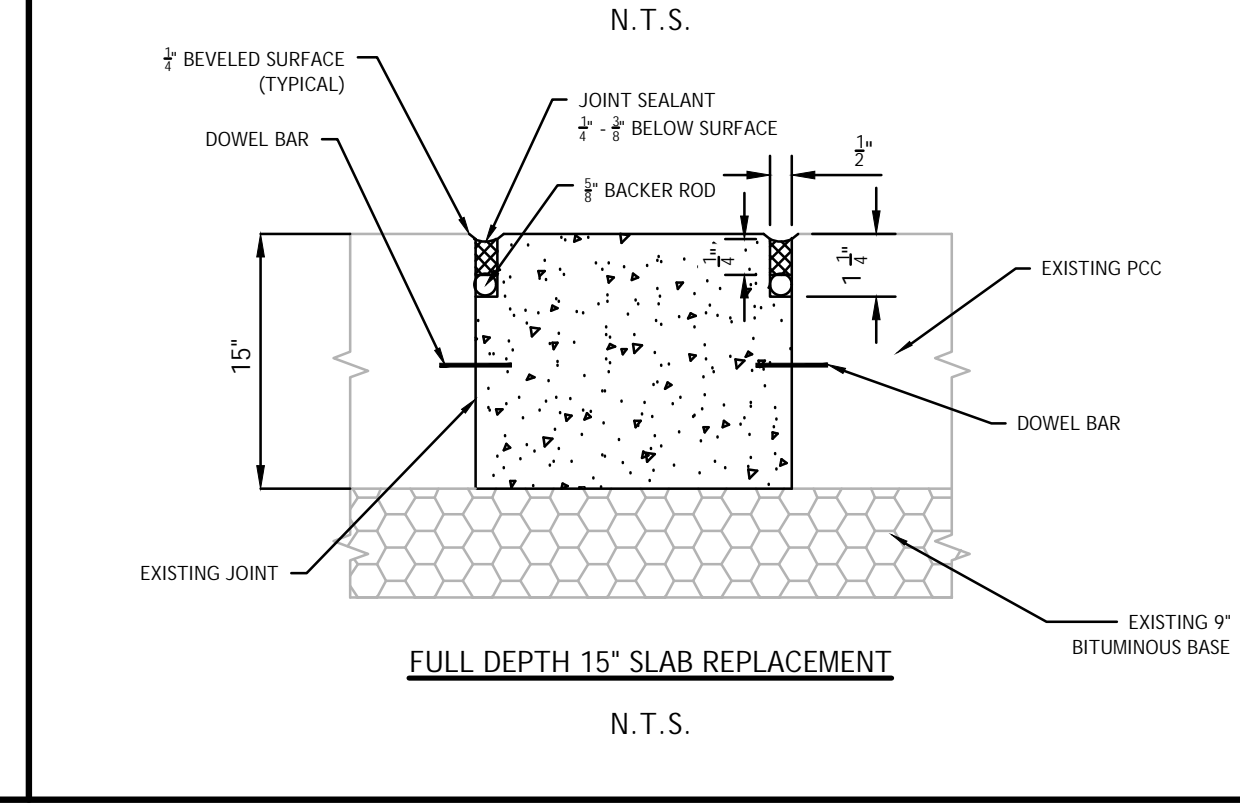
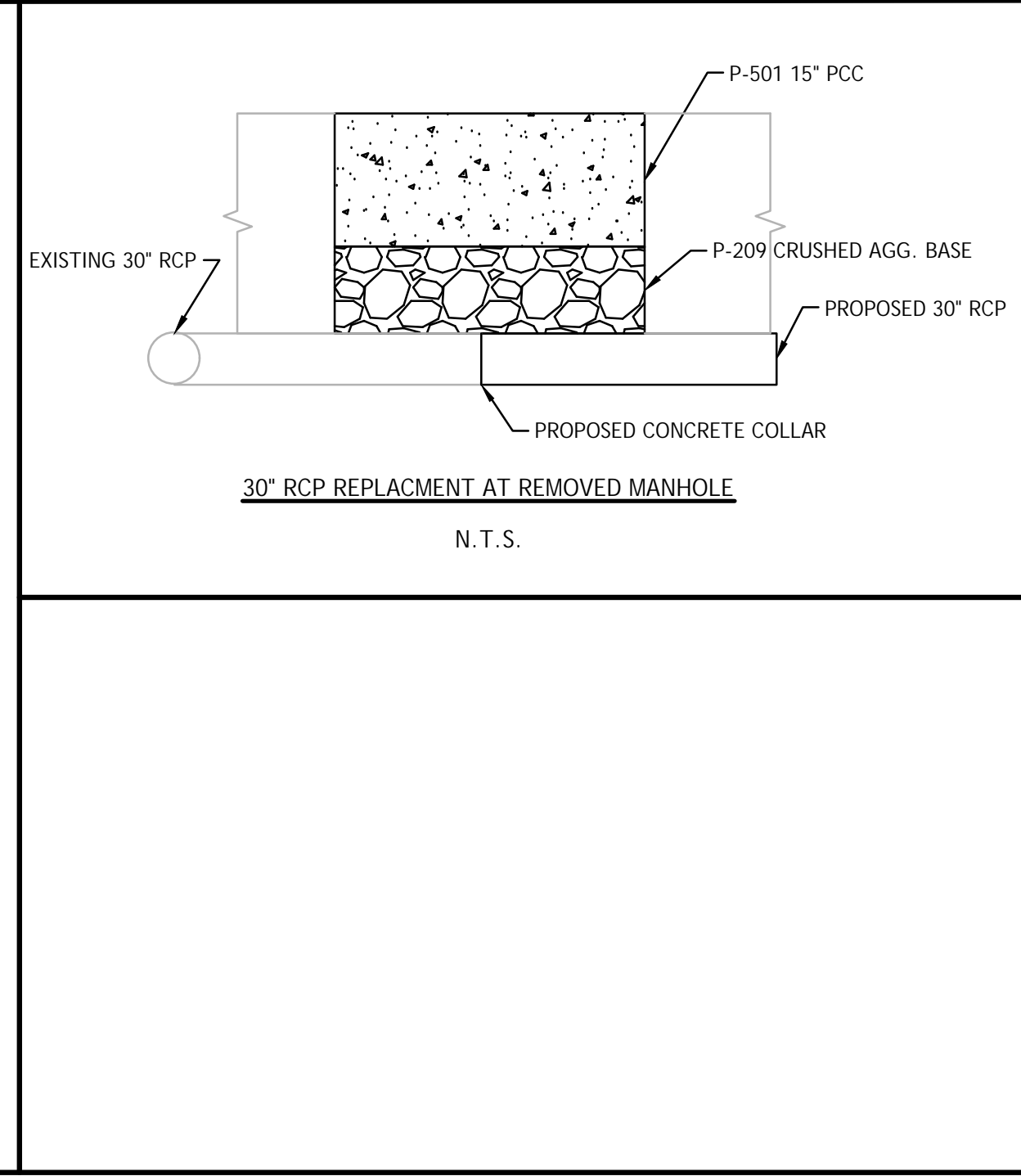
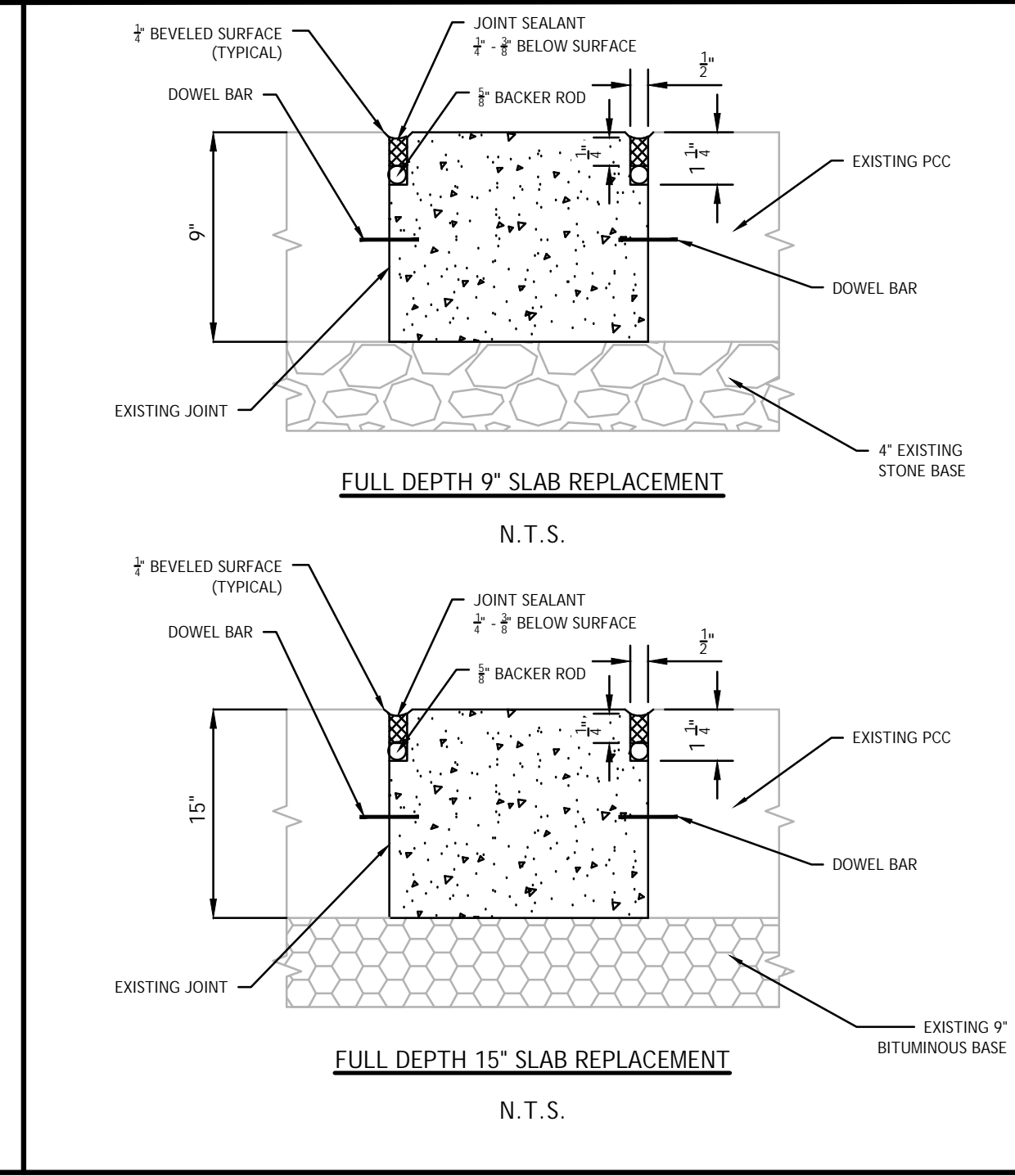
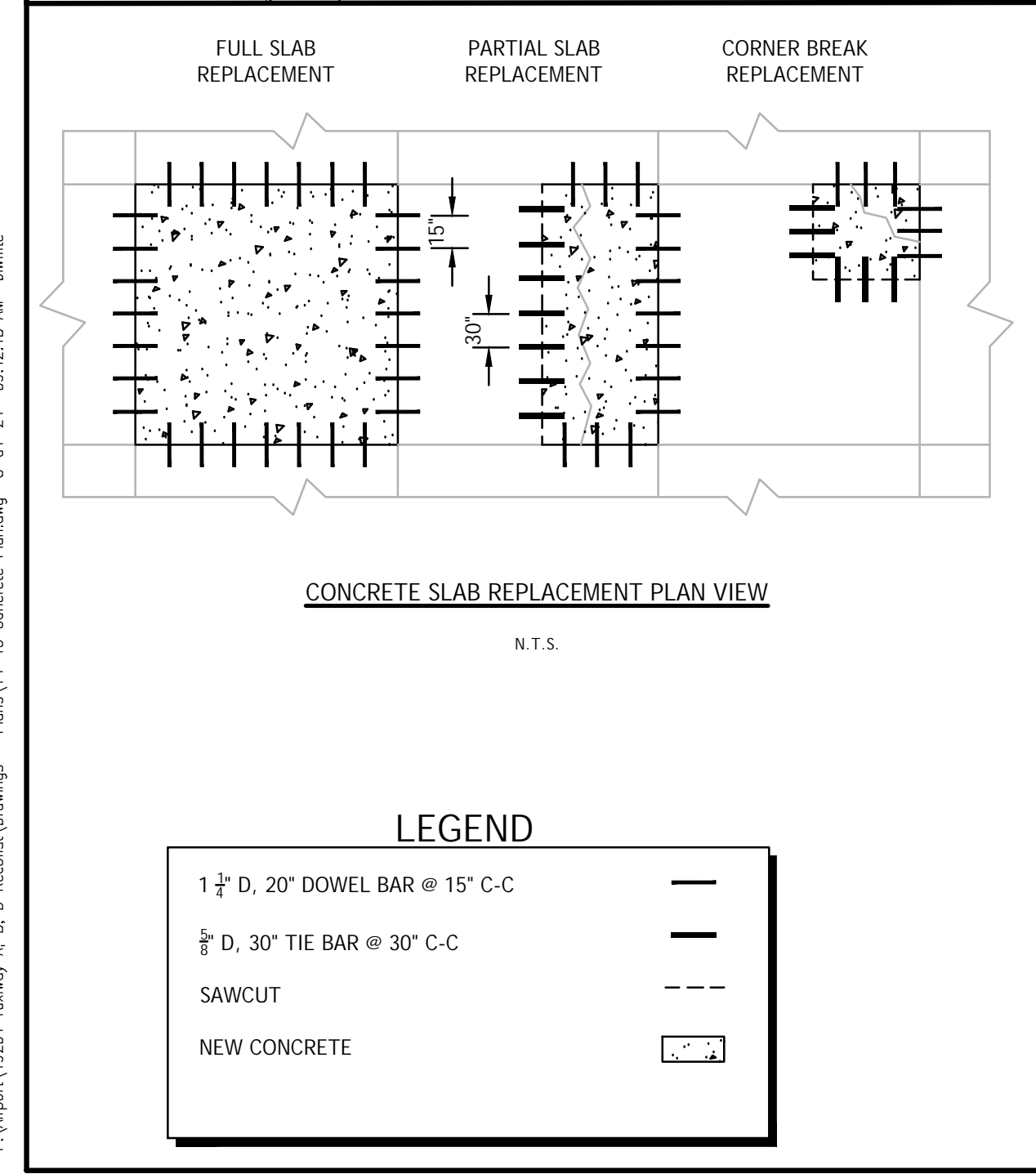
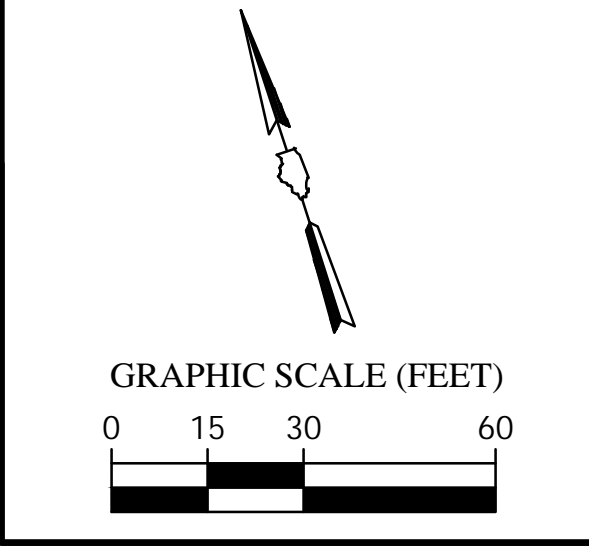
Date 3/1/2024 DR. By BLW CK'D By MKW

Scale 1" = 50' Job No. 19204CZ Sheet No. 10 of 30

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LETTING CONTRACT NUMBER
W1059



DETAILED NOTES

- SEE SAFETY AND PHASING PLANS FOR PHASE LIMITATIONS.
- TAXIWAY PAVEMENT ARE DESIGNED FOR AIRCRAFT OVER 60,000 POUNDS. ALL MATERIALS AND TESTING SHALL CONFORM TO THESE REQUIREMENTS.
- THE ENGINEER SHALL LAYOUT ALL PAVEMENT REMOVAL LIMITS.
- CONCRETE EDGES AND JOINTS SHALL MATCH THE SURROUNDING SLABS.
- FULL DEPTH SAW CUT REQUIRED AT PAVEMENT REMOVAL LIMITS. ALL SAW CUTTING COST SHALL BE CONSIDERED INCIDENTAL TO THE PAVEMENT REMOVAL.
- THE CONTRACTOR SHALL TAKE EXTRA CARE TO NOT DAMAGE HMA BASE UNDER 15" PCC.
- THE CONTRACTOR SHALL COMPACT STONE SUBBASE PRIOR TO INSTALLATION OF NEW 9" PCC.
- PAVEMENT SURFACES SHALL BE KEPT CLEAN AND FREE OF DEBRIS AT ALL TIMES.
- ALL SPOIL MATERIAL RESULTING FROM CLEANING AND PAVEMENT REMOVAL SHALL BE PROPERLY DISPOSED OF OFF AIRPORT PROPERTY.
- INITIAL SAW CUT FOR CONTRACTION JOINTS BETWEEN TWO NEW SLABS SHALL BE THICKNESS/4.

LEGEND

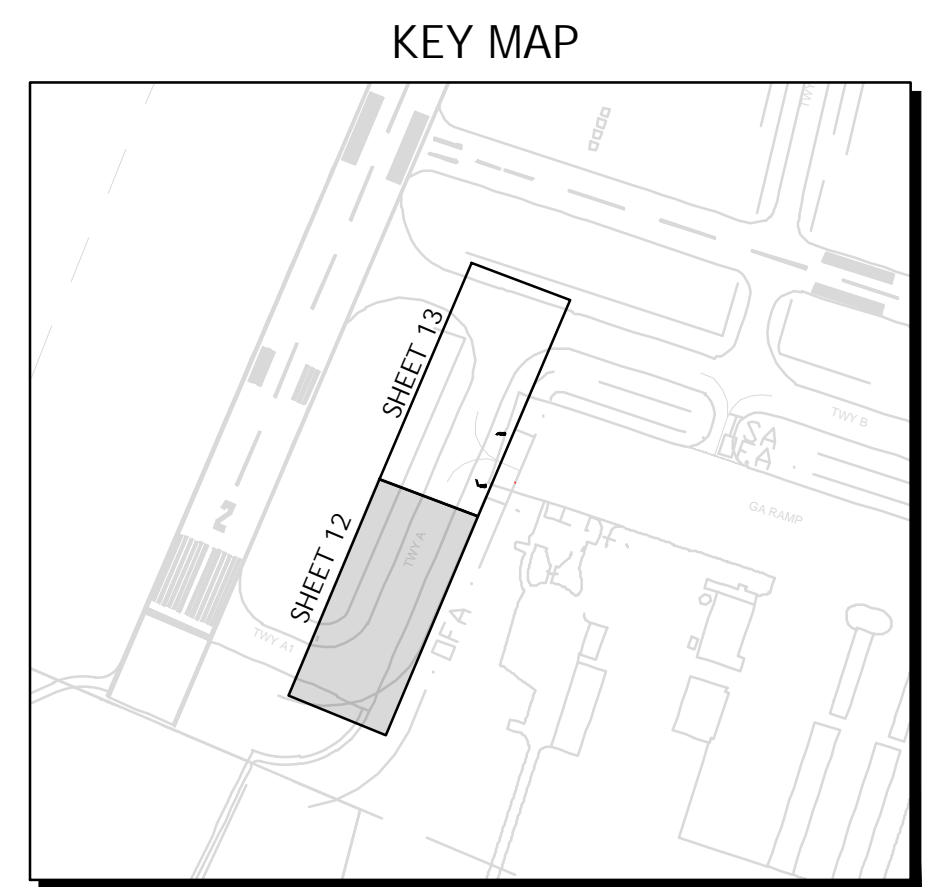
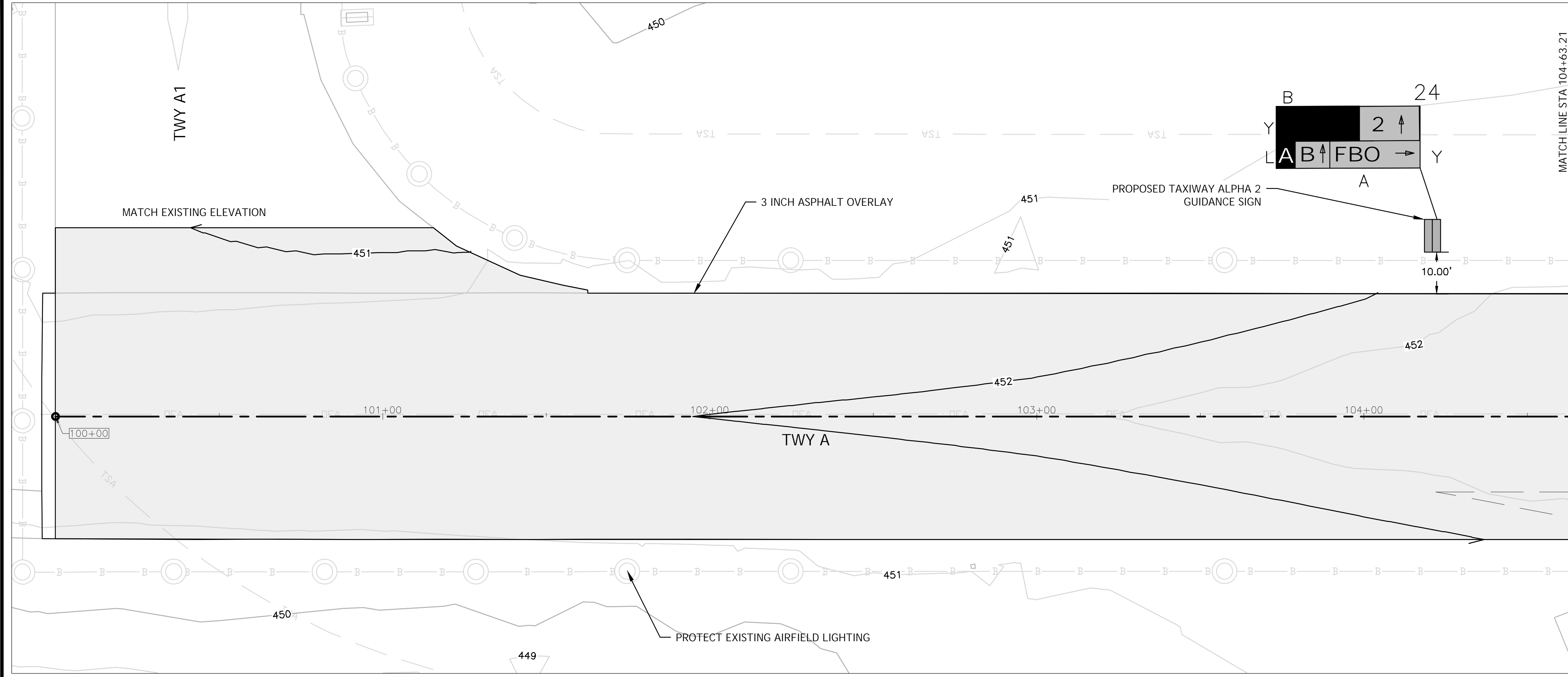
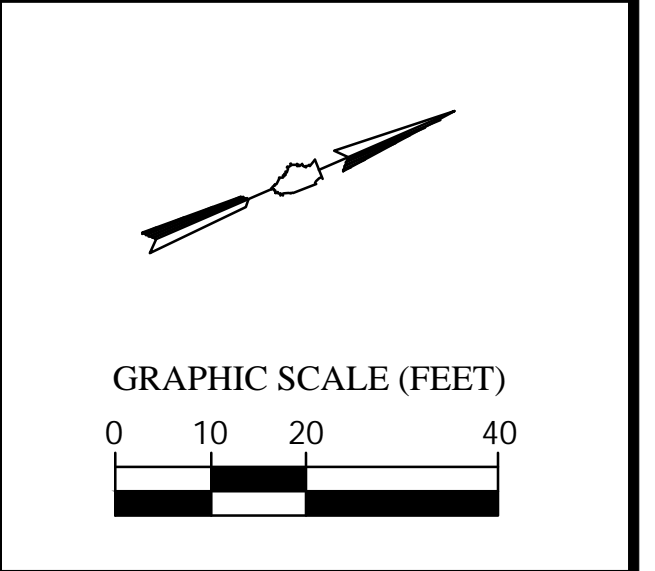
PROPOSED PCC	
TAXIWAY EDGE LIGHT	
TAXIWAY EDGE LIGHT ELECTRICAL	
GUIDANCE SIGN	

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SLAB REPLACEMENT PLAN TAXIWAY ALPHA & BRAVO REHABILITATION VETERANS AIRPORT OF SOUTHERN ILLINOIS	
Date 3/1/2024	DR. By BLW CK'D By MKW
Scale 1" = 30'	Job No. 19204CZ Sheet No. 11 of 30

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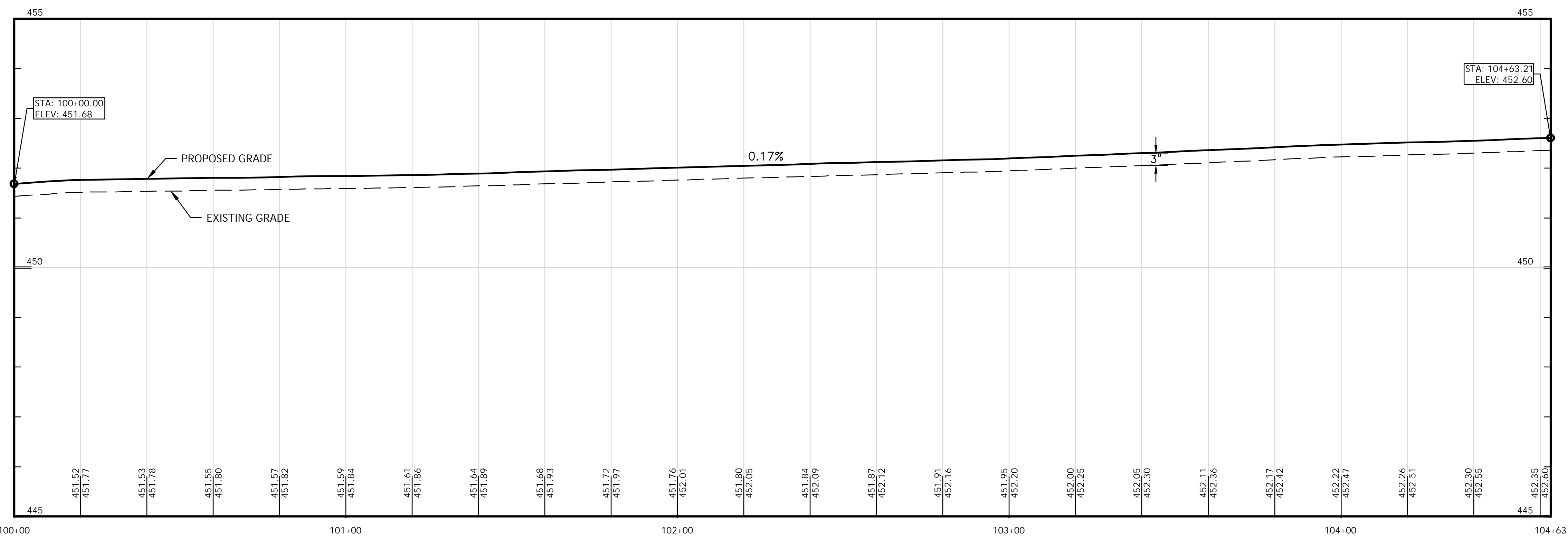
LETTING CONTRACT NUMBER
WI061



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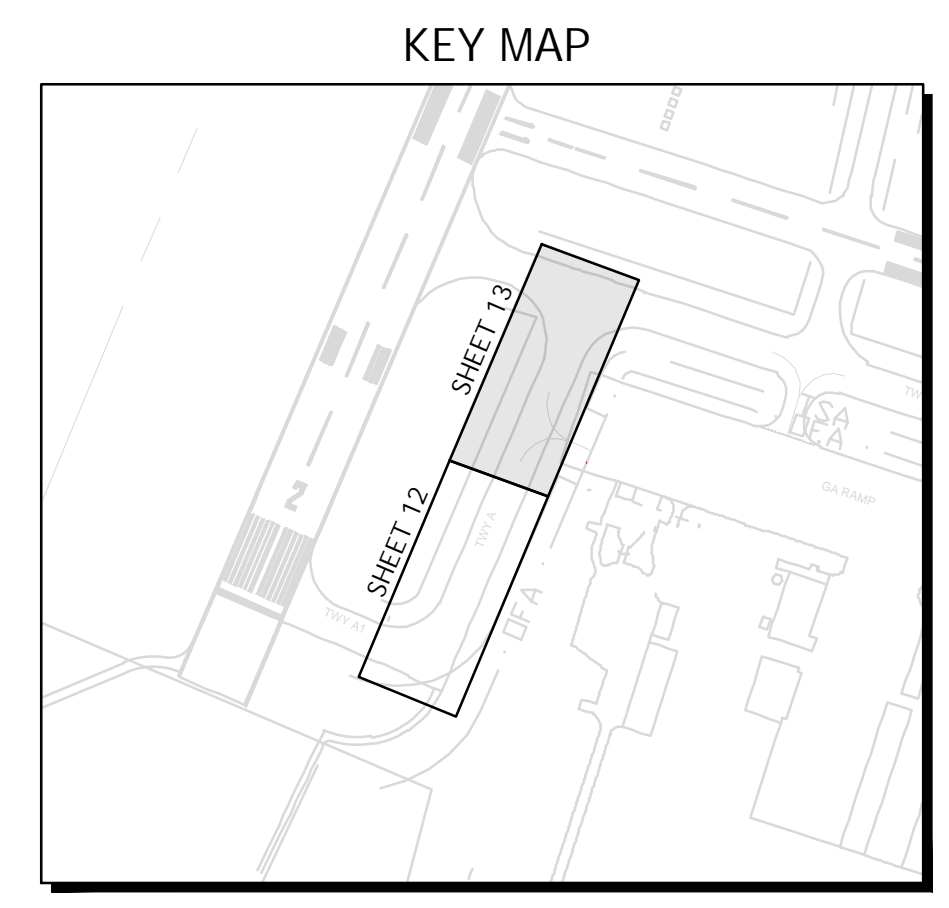
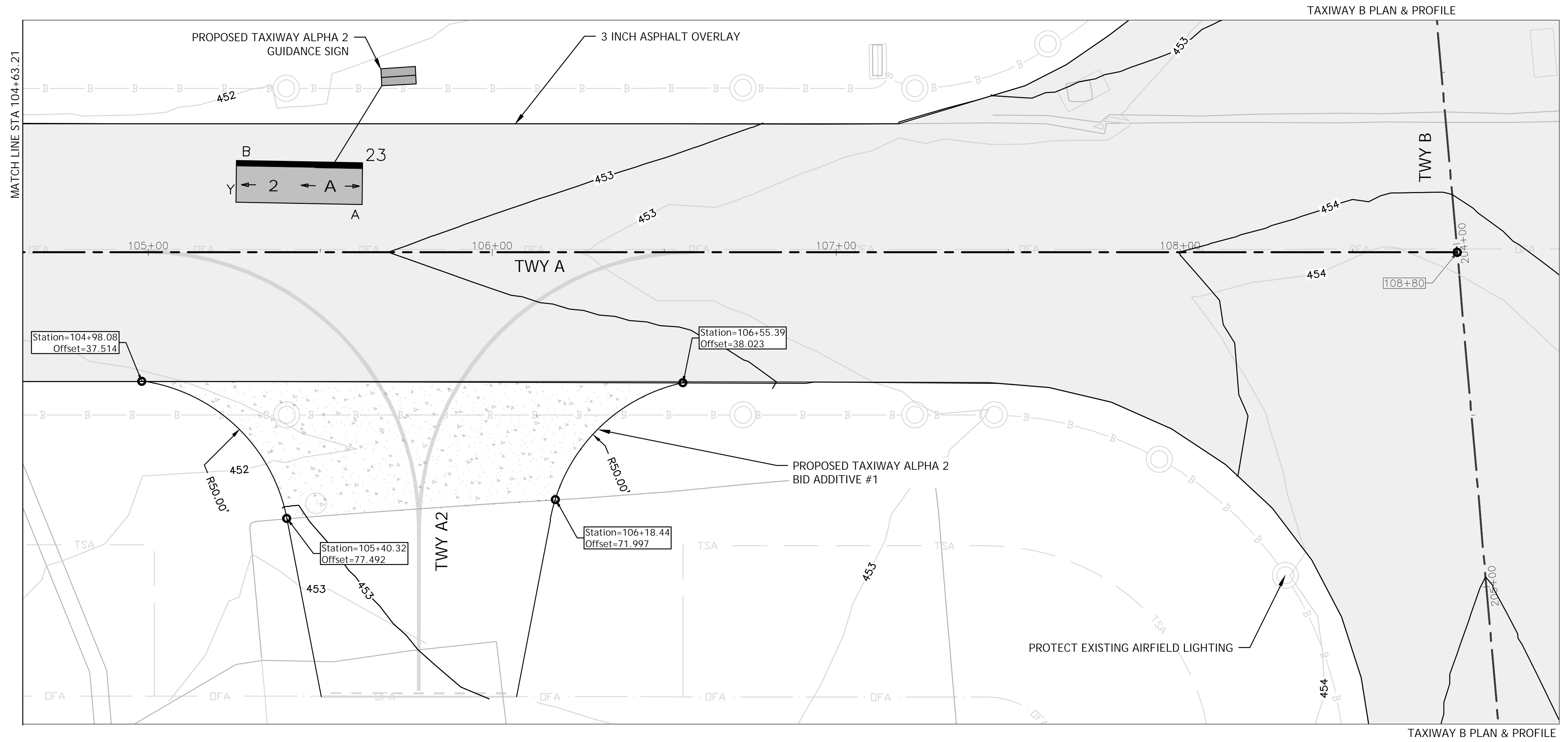
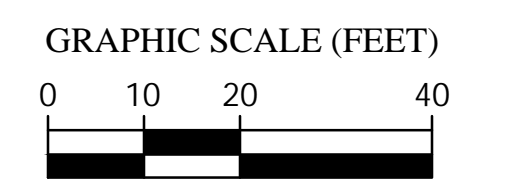
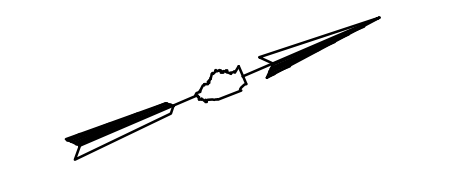
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TAXIWAY EDGE LIGHT ELECTRICAL	
EXISTING CONTOURS	
PROPOSED CONTOURS	
GUIDANCE SIGN	

- PAVING NOTES**
1. THE CONTRACTOR SHALL AVOID TRACKING TRUCKS AND EQUIPMENT OVER ADJACENT PAVEMENT AREAS UNLESS SUFFICIENT PROTECTION HAS BEEN PROVIDED AND APPROVAL FROM THE AIRPORT ENGINEER HAS BEEN GRANTED.
 2. TRACKING OF TACK COAT, DAMAGE TO THE PAVEMENT, OR DAMAGE TO GROOVED SURFACES WILL REQUIRE THE WORK TO STOP UNTIL ACCEPTABLE PROTECTION OR A CHANGE IN THE CONSTRUCTION METHOD HAS BEEN APPROVED.
 3. ANY DAMAGE SHALL BE REPAIRED OR RESTORED TO ITS ORIGINAL CONDITIONS AT THE CONTRACTOR'S OWN EXPENSE.



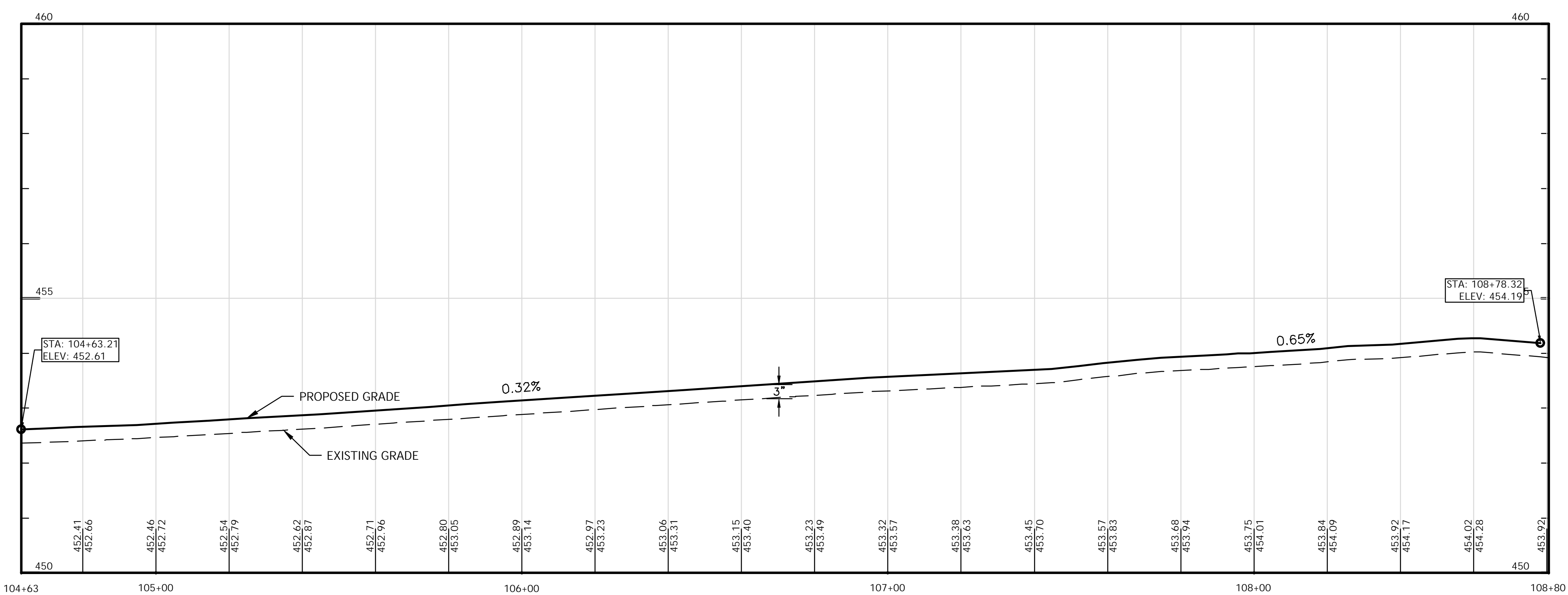
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LETTING CONTRACT NUMBER
WI061



LEGEND

ASPHALT OVERLAY	
TAXIWAY EDGE LIGHT	
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EXISTING CONTOURS	
PROPOSED CONTOURS	
GUIDANCE SIGN	
PROPOSED TAXIWAY A1	

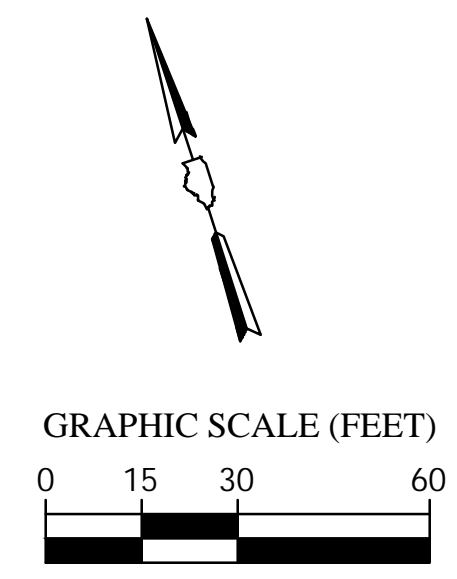


- PAVING NOTES
1. THE CONTRACTOR SHALL AVOID TRACKING TRUCKS AND EQUIPMENT OVER ADJACENT PAVEMENT AREAS UNLESS SUFFICIENT PROTECTION HAS BEEN PROVIDED AND APPROVAL FROM THE AIRPORT ENGINEER HAS BEEN GRANTED.
 2. TRACKING OF TACK COAT, DAMAGE TO THE PAVEMENT, OR DAMAGE TO GROOVED SURFACES WILL REQUIRE THE WORK TO STOP UNTIL ACCEPTABLE PROTECTION OR A CHANGE IN THE CONSTRUCTION METHOD HAS BEEN APPROVED.
 3. ANY DAMAGE SHALL BE REPAIRED OR RESTORED TO ITS ORIGINAL CONDITIONS AT THE CONTRACTOR'S OWN EXPENSE.

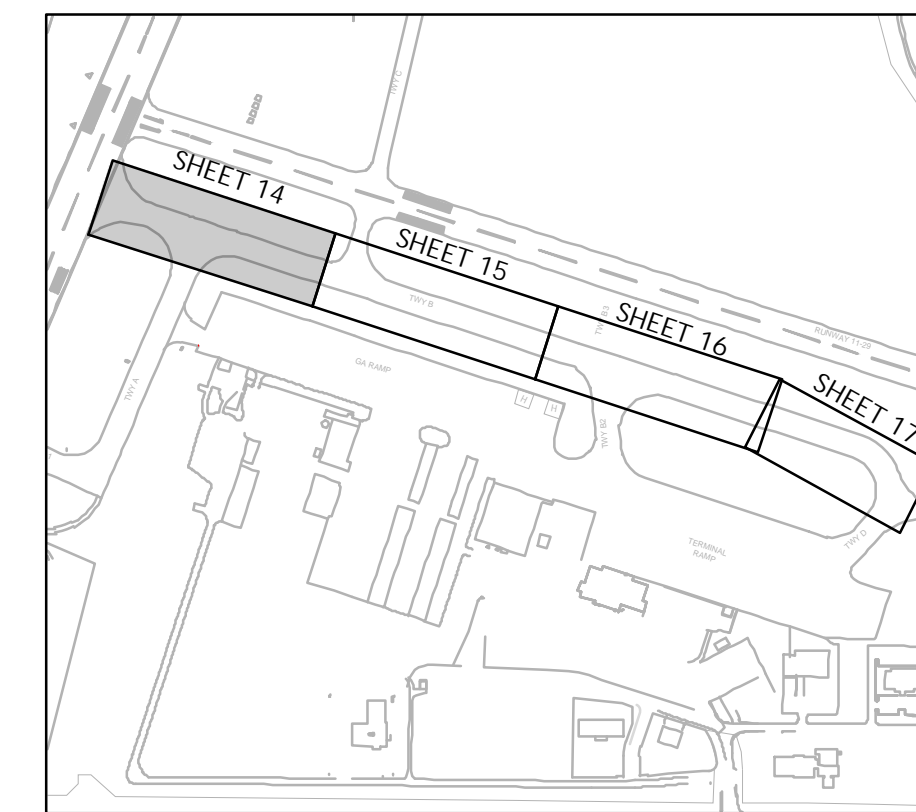
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LETTING CONTRACT NUMBER

WI061



KEY MAP

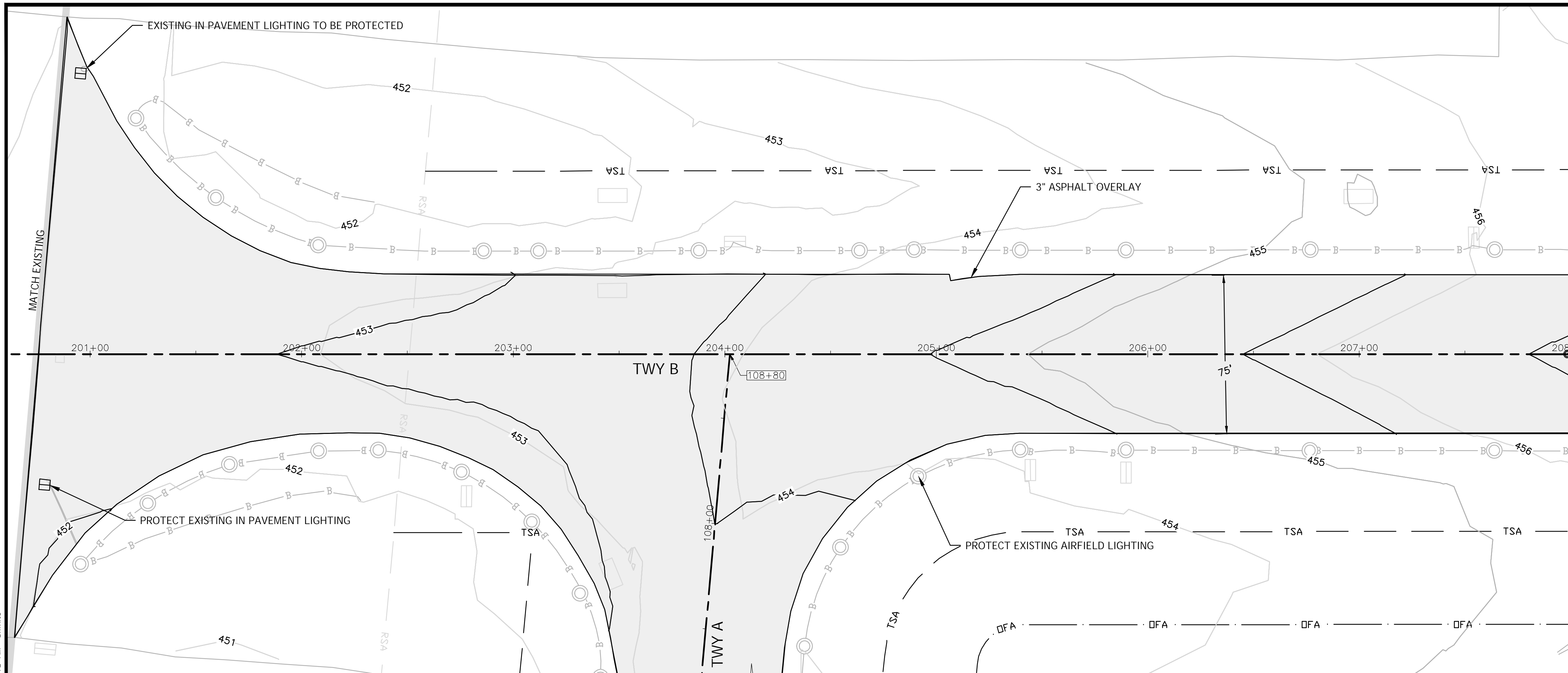


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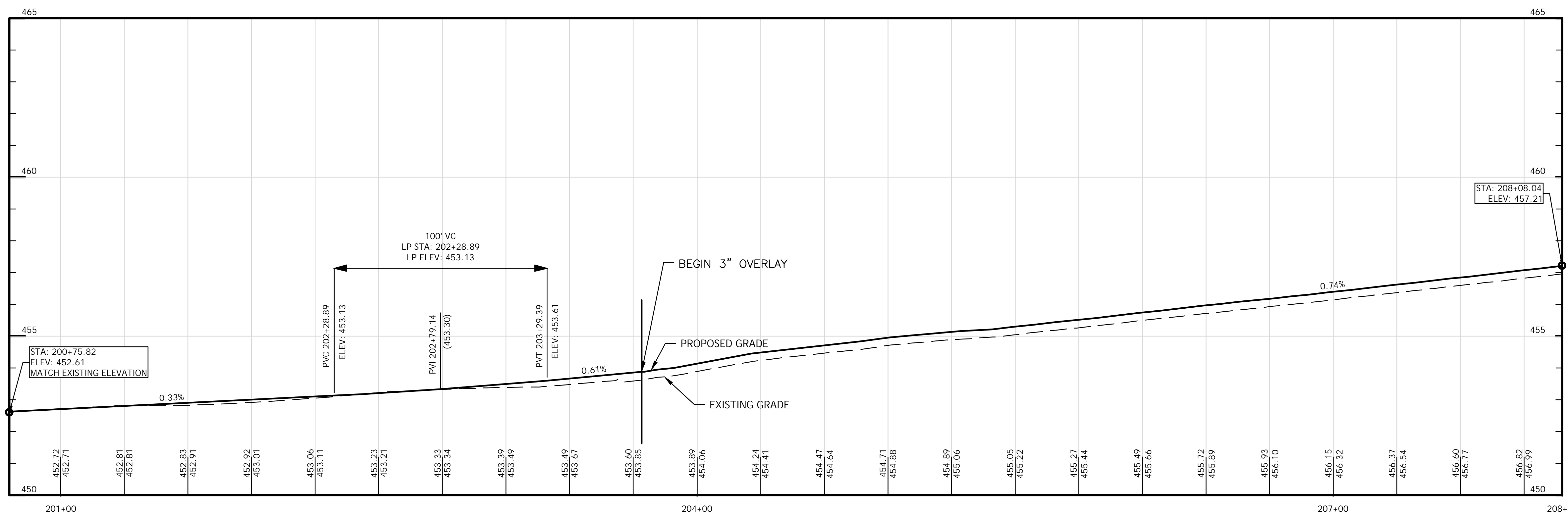
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TAXIWAY EDGE LIGHT	
TAXIWAY EDGE LIGHT ELECTRICAL	
EXISTING CONTOURS	
PROPOSED CONTOURS	
GUIDANCE SIGN	
RUNWAY IN-PAVEMENT LIGHT	
PROPOSED X" RCP STORM SEWER	
RUNWAY SAFETY AREA	

PAVING NOTES

1. THE CONTRACTOR SHALL AVOID TRACKING TRUCKS AND EQUIPMENT OVER ADJACENT PAVEMENT AREAS UNLESS SUFFICIENT PROTECTION HAS BEEN PROVIDED AND APPROVAL FROM THE AIRPORT ENGINEER HAS BEEN GRANTED.
2. TRACKING OF TACK COAT, DAMAGE TO THE PAVEMENT, OR DAMAGE TO GROOVED SURFACES WILL REQUIRE THE WORK TO STOP UNTIL ACCEPTABLE PROTECTION OR A CHANGE IN THE CONSTRUCTION METHOD HAS BEEN APPROVED.
3. ANY DAMAGE SHALL BE REPAIRED OR RESTORED TO ITS ORIGINAL CONDITIONS AT THE CONTRACTOR'S OWN EXPENSE.

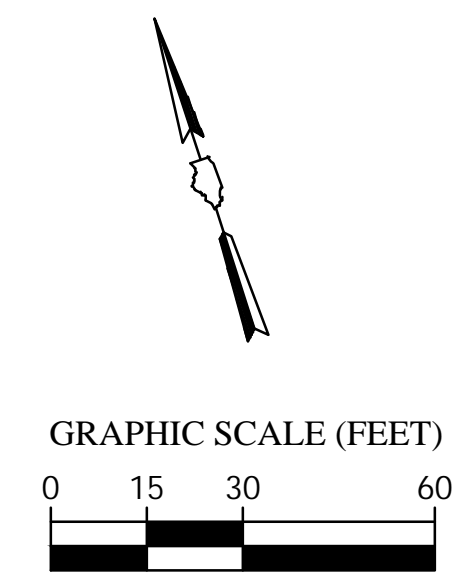


TAXIWAY ALPHA PLAN & PROFILE

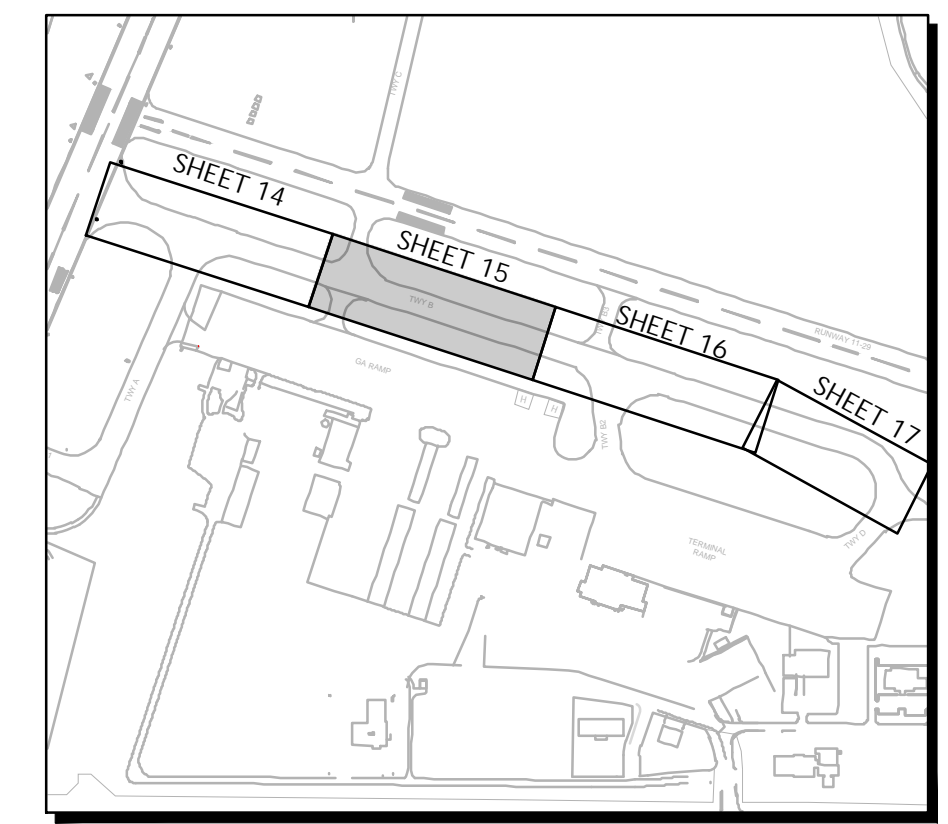


LETTING CONTRACT NUMBER

WI061



KEY MAP

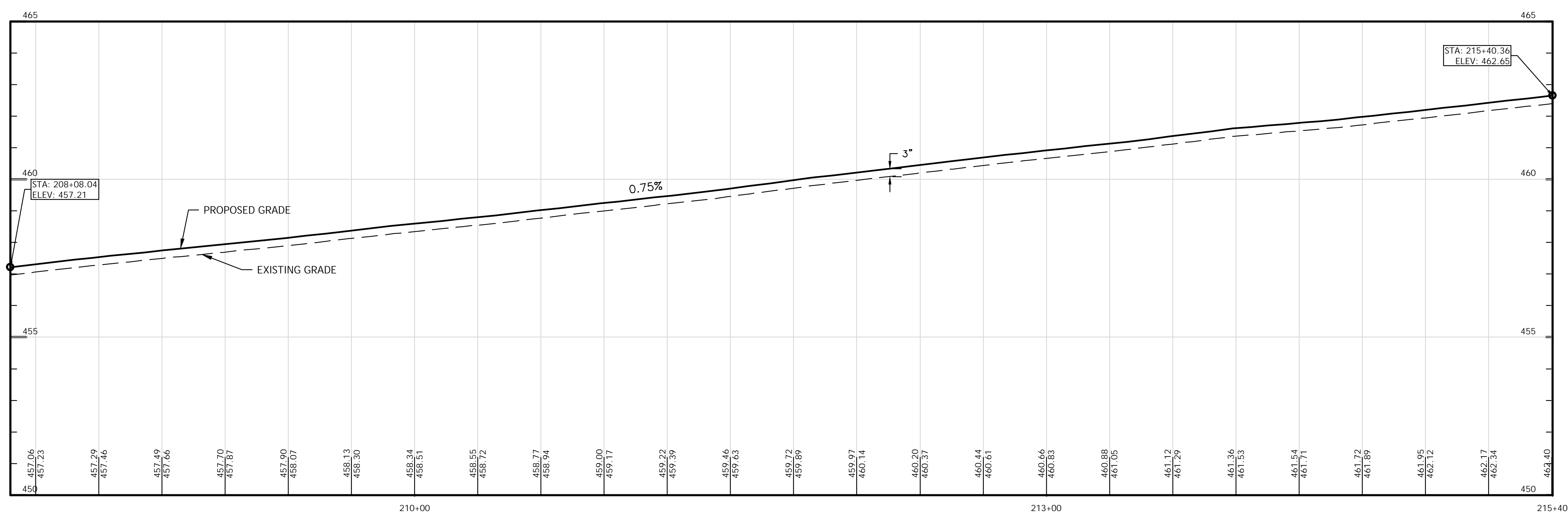
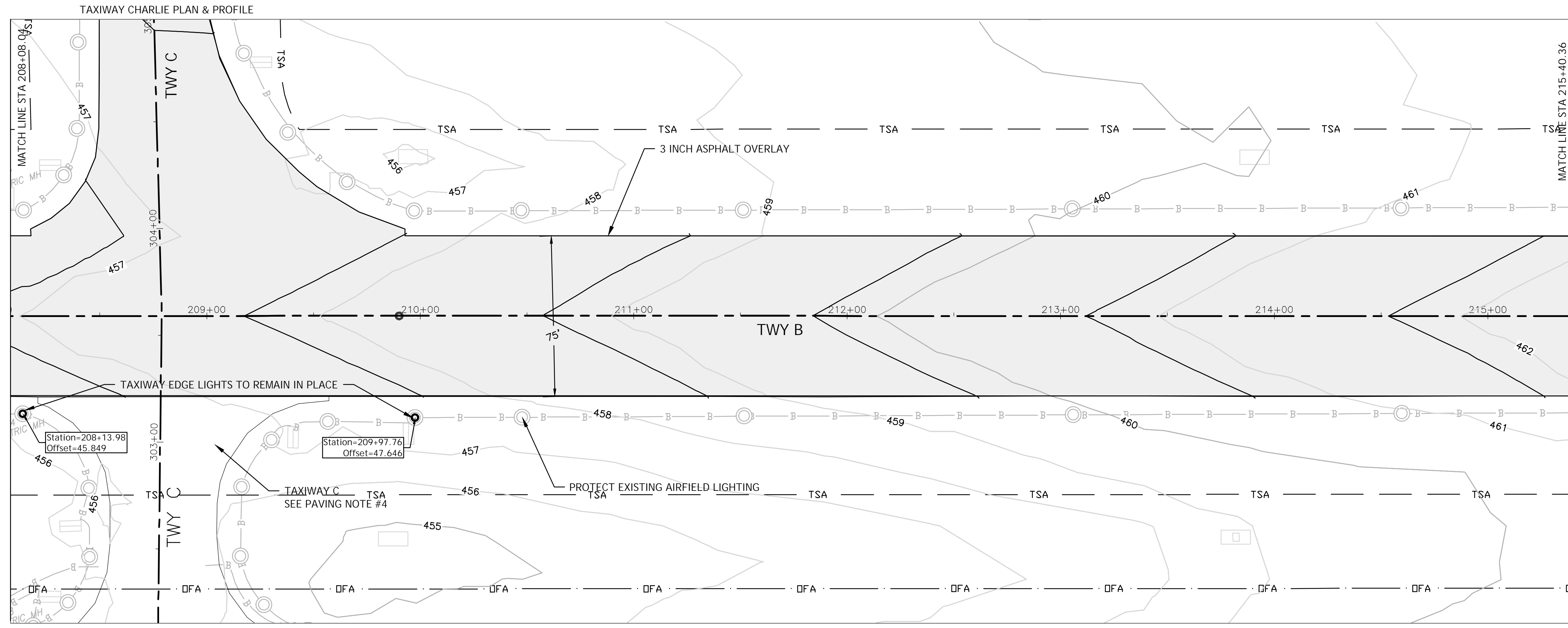


LEGEND

ASPHALT OVERLAY	
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EXISTING CONTOURS	
PROPOSED CONTOURS	
GUIDANCE SIGN	

PAVING NOTES

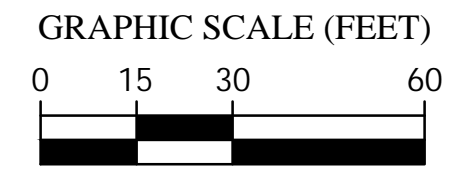
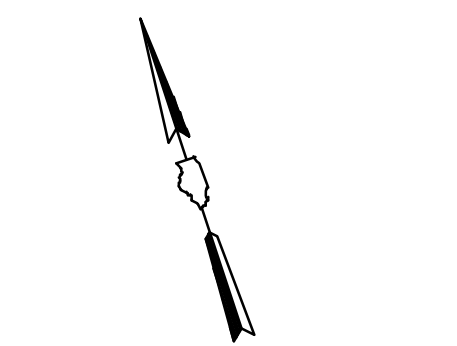
1. THE CONTRACTOR SHALL AVOID TRACKING TRUCKS AND EQUIPMENT OVER ADJACENT PAVEMENT AREAS UNLESS SUFFICIENT PROTECTION HAS BEEN PROVIDED AND APPROVAL FROM THE AIRPORT ENGINEER HAS BEEN GRANTED.
2. TRACKING OF TACK COAT, DAMAGE TO THE PAVEMENT, OR DAMAGE TO GROOVED SURFACES WILL REQUIRE THE WORK TO STOP UNTIL ACCEPTABLE PROTECTION OR A CHANGE IN THE CONSTRUCTION METHOD HAS BEEN APPROVED.
3. ANY DAMAGE SHALL BE REPAIRED OR RESTORED TO ITS ORIGINAL CONDITIONS AT THE CONTRACTOR'S OWN EXPENSE.
4. IF ADDITIVE BID #1 IS ACCEPTED A PORTION OF TAXIWAY CHARLIE BETWEEN TAXIWAY BRAVO AND THE GA RAMP SHALL BE REMOVED, SEE DEMOLITION PLAN SHEET. IF ADDITIVE BID #1 IS REJECTED, A BUTT JOINT SHALL BE INSTALLED AS INSTRUCTED BY ENGINEER



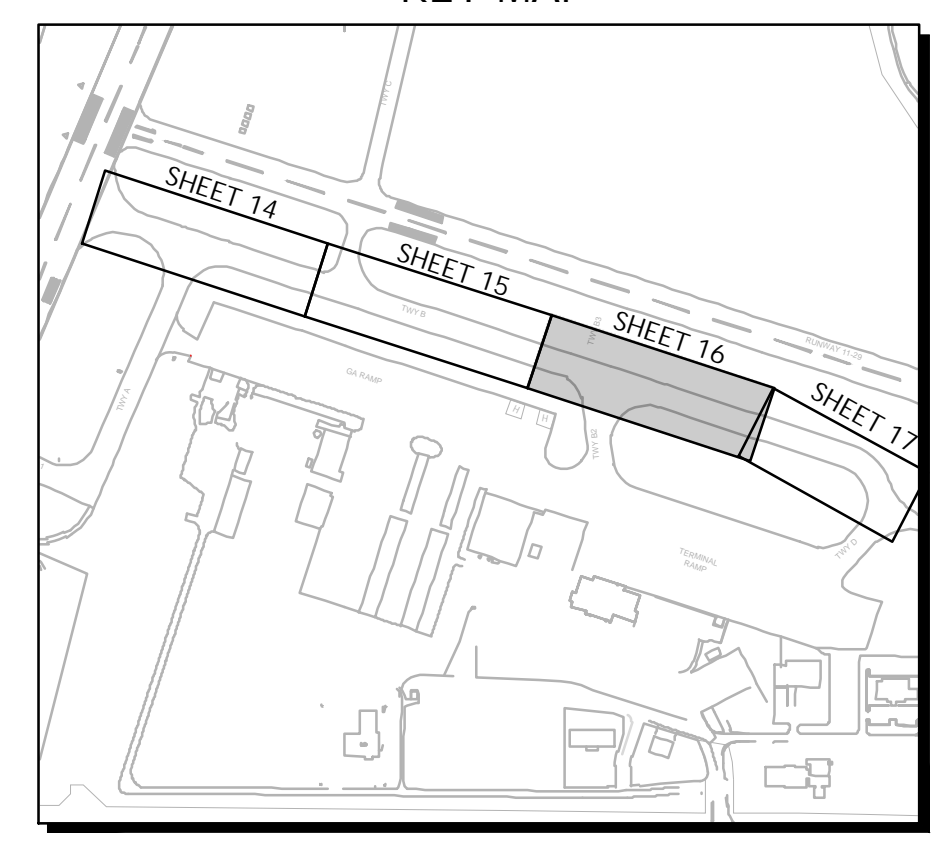
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LETTING CONTRACT NUMBER

WI061



KEY MAP

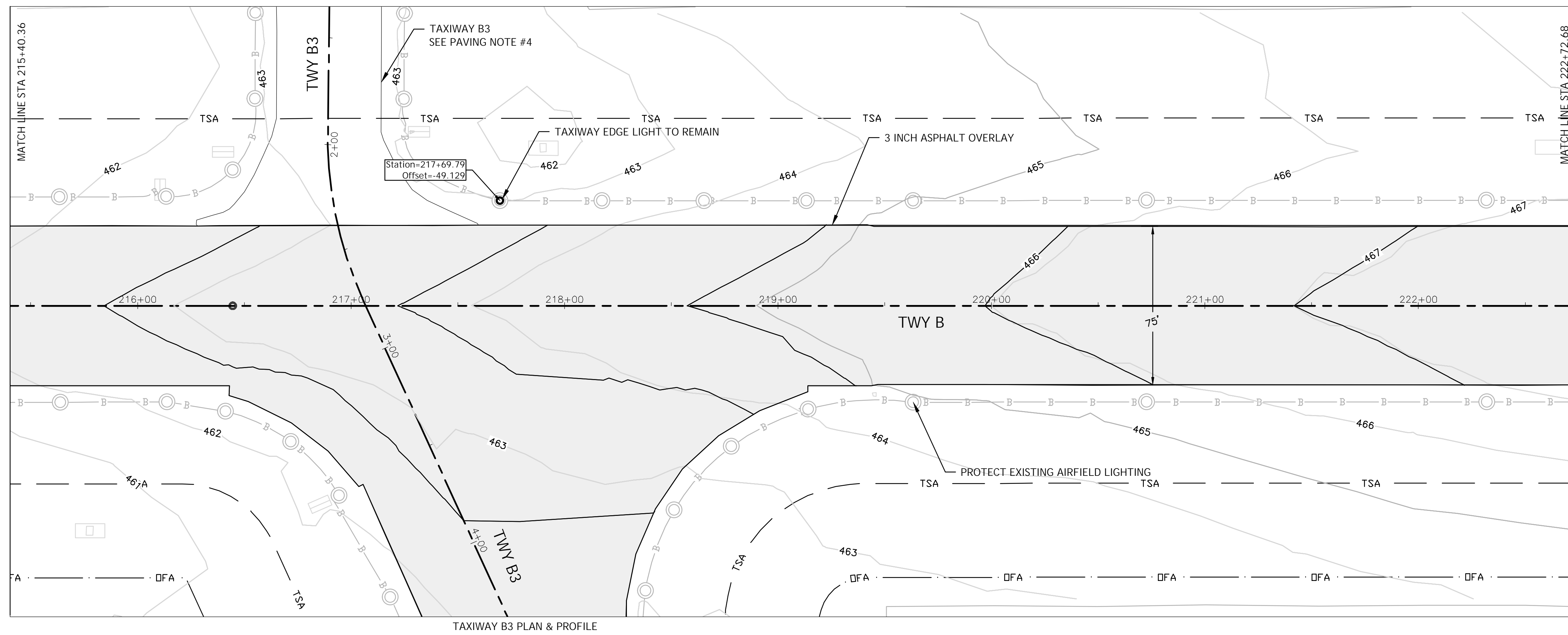


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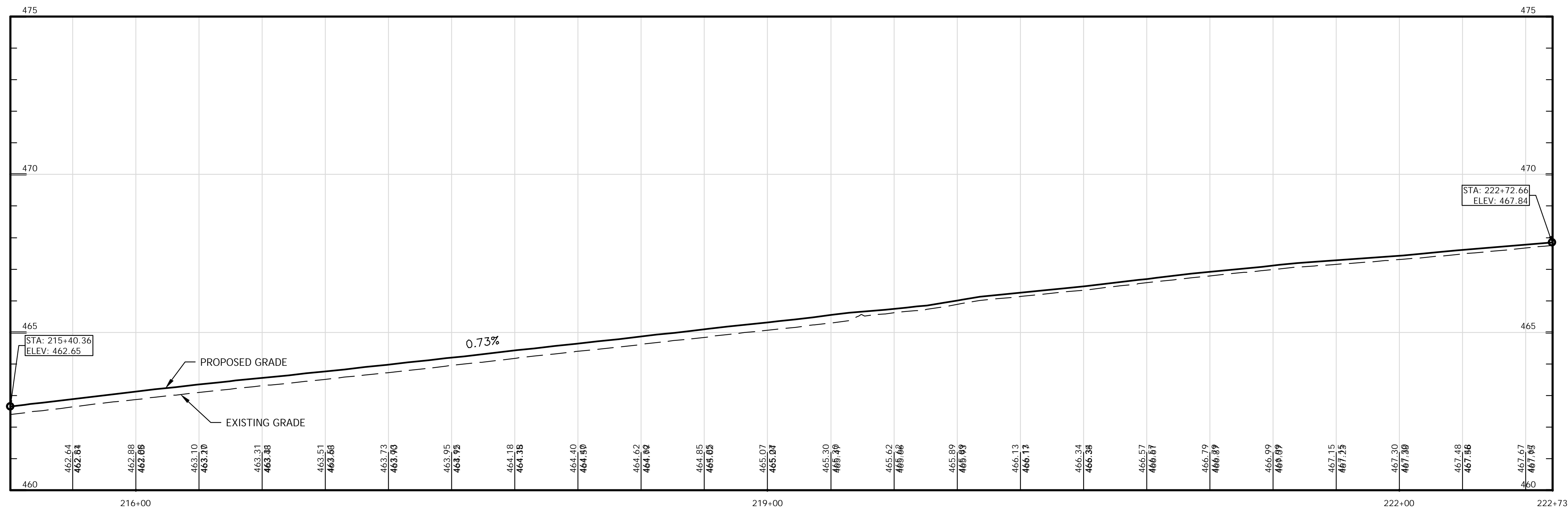
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EXISTING CONTOURS	
PROPOSED CONTOURS	
GUIDANCE SIGN	

PAVING NOTES

1. THE CONTRACTOR SHALL AVOID TRACKING TRUCKS AND EQUIPMENT OVER ADJACENT PAVEMENT AREAS UNLESS SUFFICIENT PROTECTION HAS BEEN PROVIDED AND APPROVAL FROM THE AIRPORT ENGINEER HAS BEEN GRANTED.
2. TRACKING OF TACK COAT, DAMAGE TO THE PAVEMENT, OR DAMAGE TO GROOVED SURFACES WILL REQUIRE THE WORK TO STOP UNTIL ACCEPTABLE PROTECTION OR A CHANGE IN THE CONSTRUCTION METHOD HAS BEEN APPROVED.
3. ANY DAMAGE SHALL BE REPAIRED OR RESTORED TO ITS ORIGINAL CONDITIONS AT THE CONTRACTOR'S OWN EXPENSE.
4. IF ADDITIVE BID #2 IS ACCEPTED A PORTION OF TAXIWAY BRAVO 3 BETWEEN TAXIWAY BRAVO AND RUNWAY 11-29 SHALL BE REMOVED, SEE DEMOLITION PLAN SHEET. IF ADDITIVE BID #2 IS REJECTED, A BUTT JOINT SHALL BE INSTALLED AS INSTRUCTED BY ENGINEER

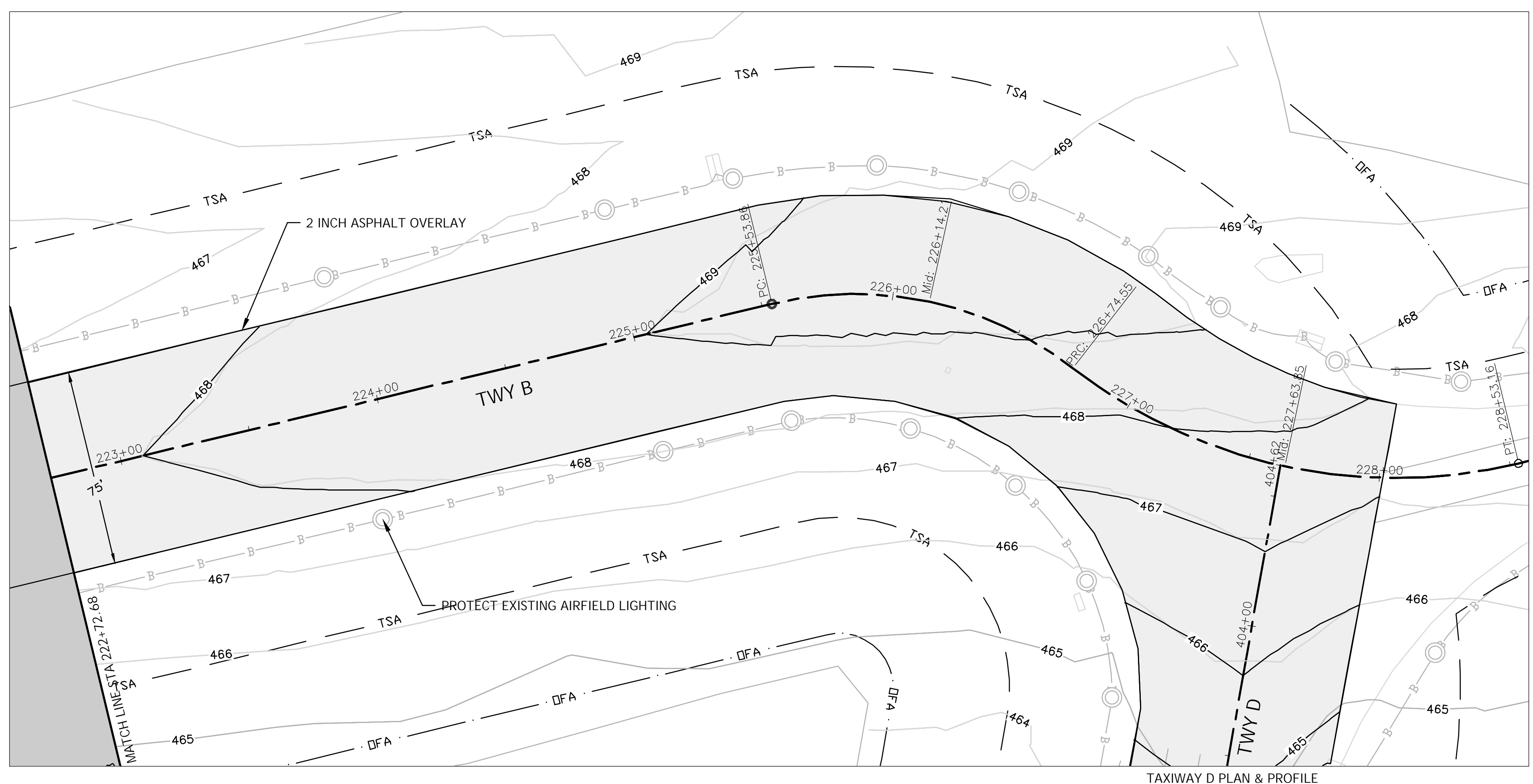
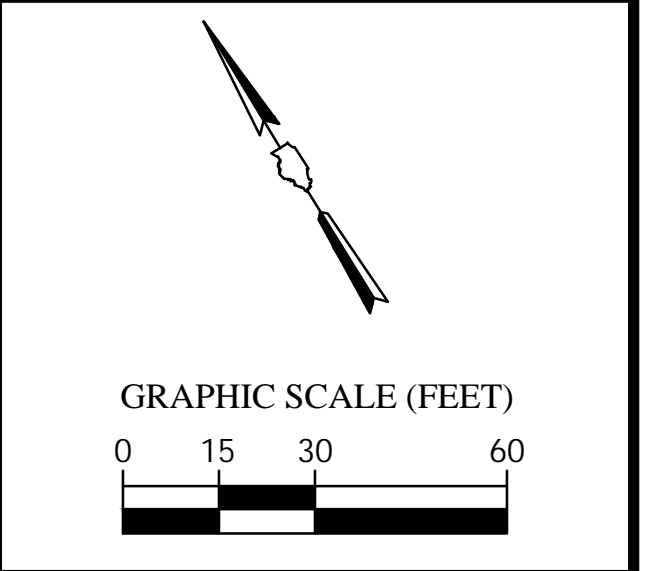


TAXIWAY B3 PLAN & PROFILE

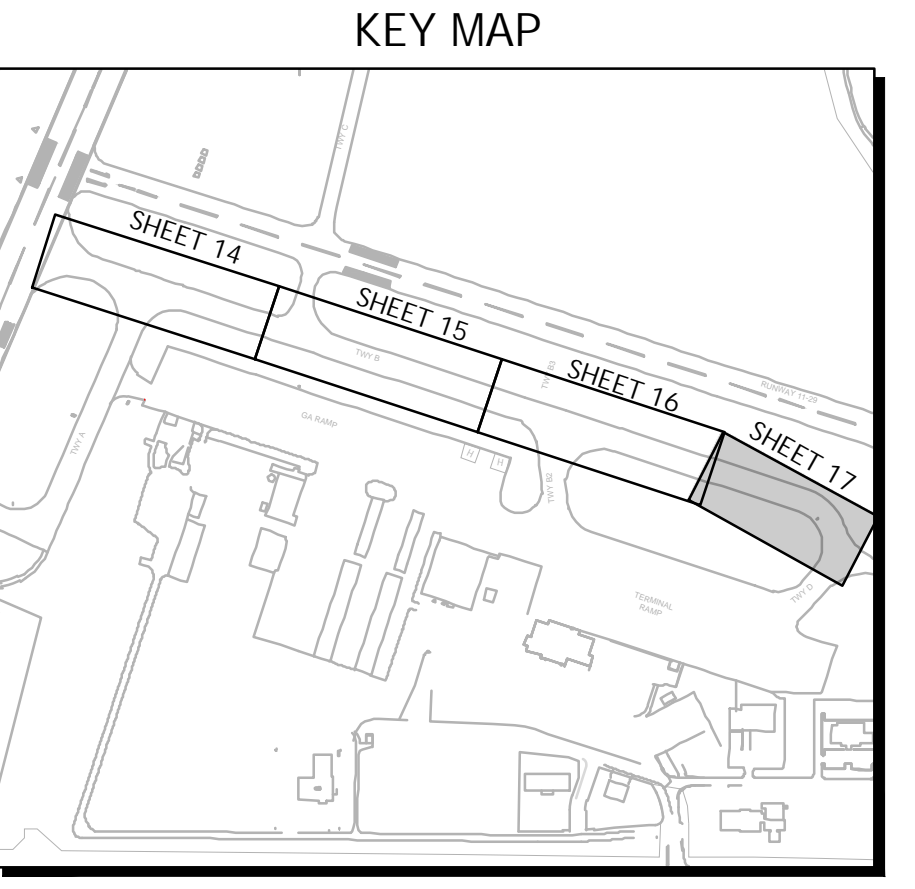


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LETTING CONTRACT NUMBER
WI061



TAXIWAY D PLAN & PROFILE

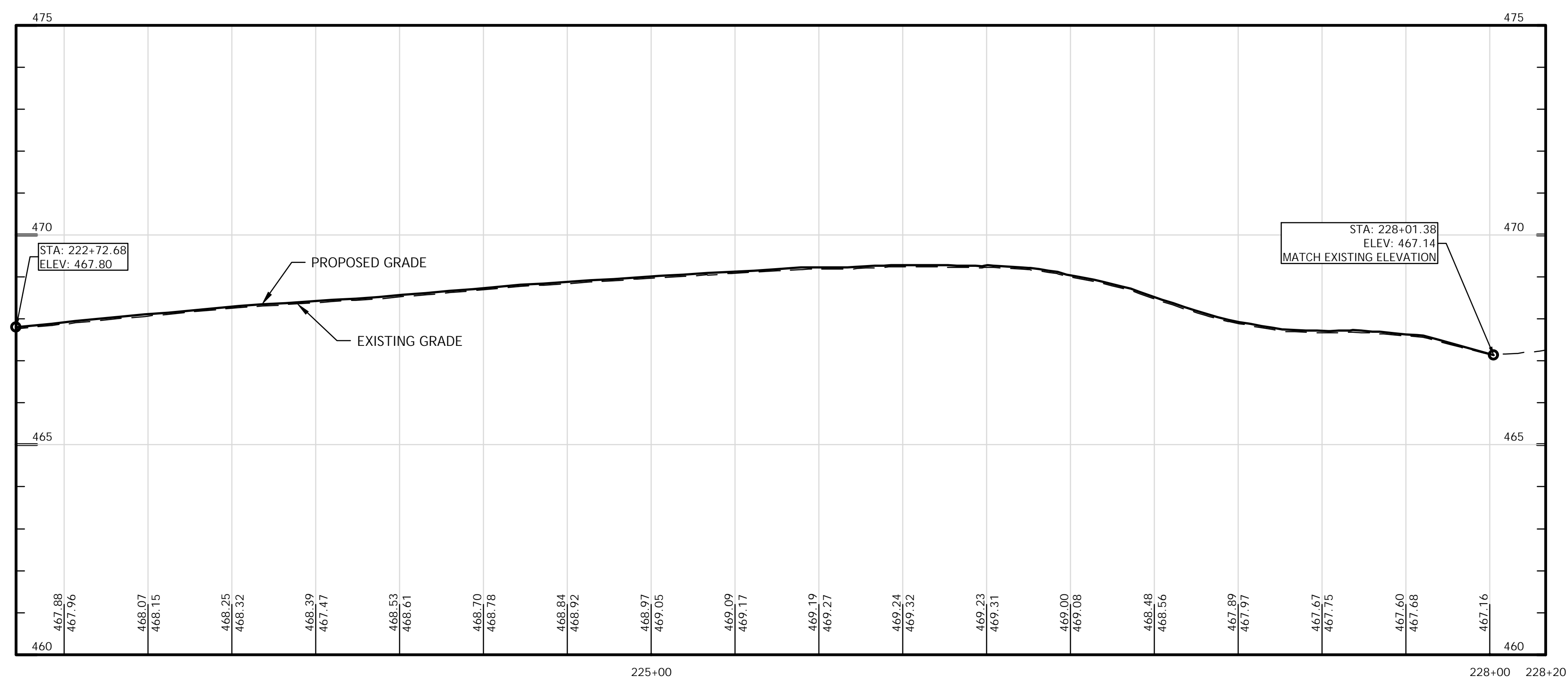


LEGEND

ASPHALT OVERLAY	
TAXIWAY EDGE LIGHT	
TAXIWAY EDGE LIGHT ELECTRICAL	
EXISTING CONTOURS	
PROPOSED CONTOURS	
GUIDANCE SIGN	

PAVING NOTES

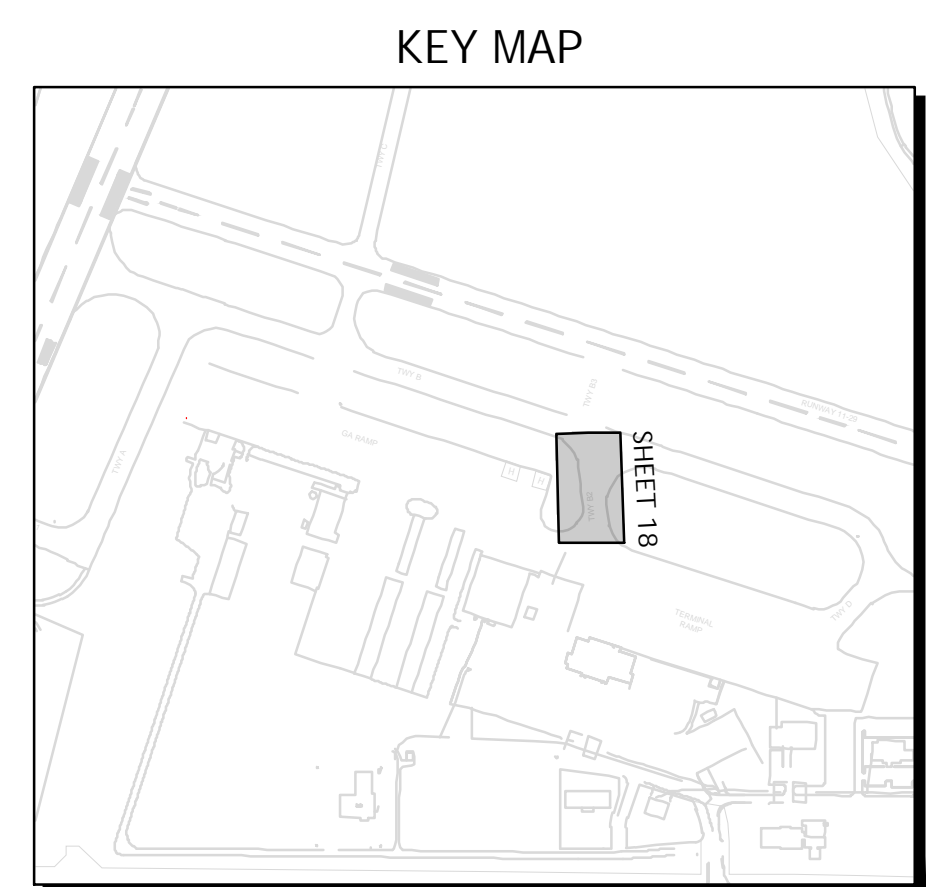
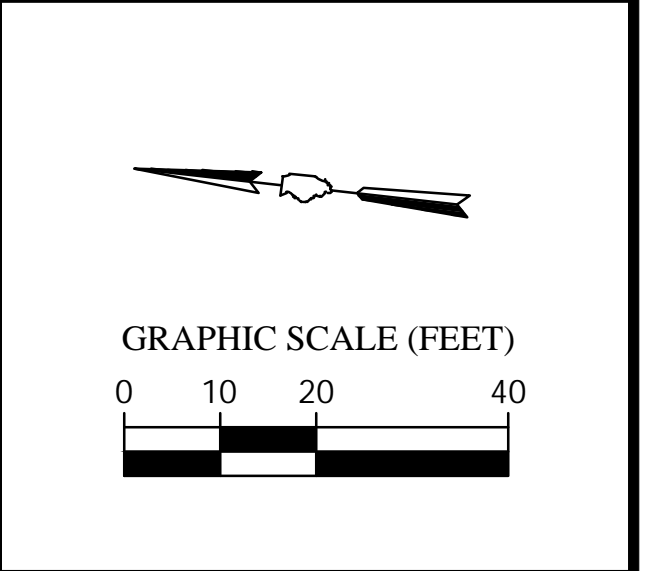
1. THE CONTRACTOR SHALL AVOID TRACKING TRUCKS AND EQUIPMENT OVER ADJACENT PAVEMENT AREAS UNLESS SUFFICIENT PROTECTION HAS BEEN PROVIDED AND APPROVAL FROM THE AIRPORT ENGINEER HAS BEEN GRANTED.
2. TRACKING OF TACK COAT, DAMAGE TO THE PAVEMENT, OR DAMAGE TO GROOVED SURFACES WILL REQUIRE THE WORK TO STOP UNTIL ACCEPTABLE PROTECTION OR A CHANGE IN THE CONSTRUCTION METHOD HAS BEEN APPROVED.
3. ANY DAMAGE SHALL BE REPAIRED OR RESTORED TO ITS ORIGINAL CONDITIONS AT THE CONTRACTOR'S OWN EXPENSE.



P:\airport\19204 Taxiway A, B, D Reconstr\Drawings - Plans\18-21 TWY B - Plan and Profile - (STA 200+75.72 to 228+01.38).dwg 3-01-24 10:21:25 AM blwhite

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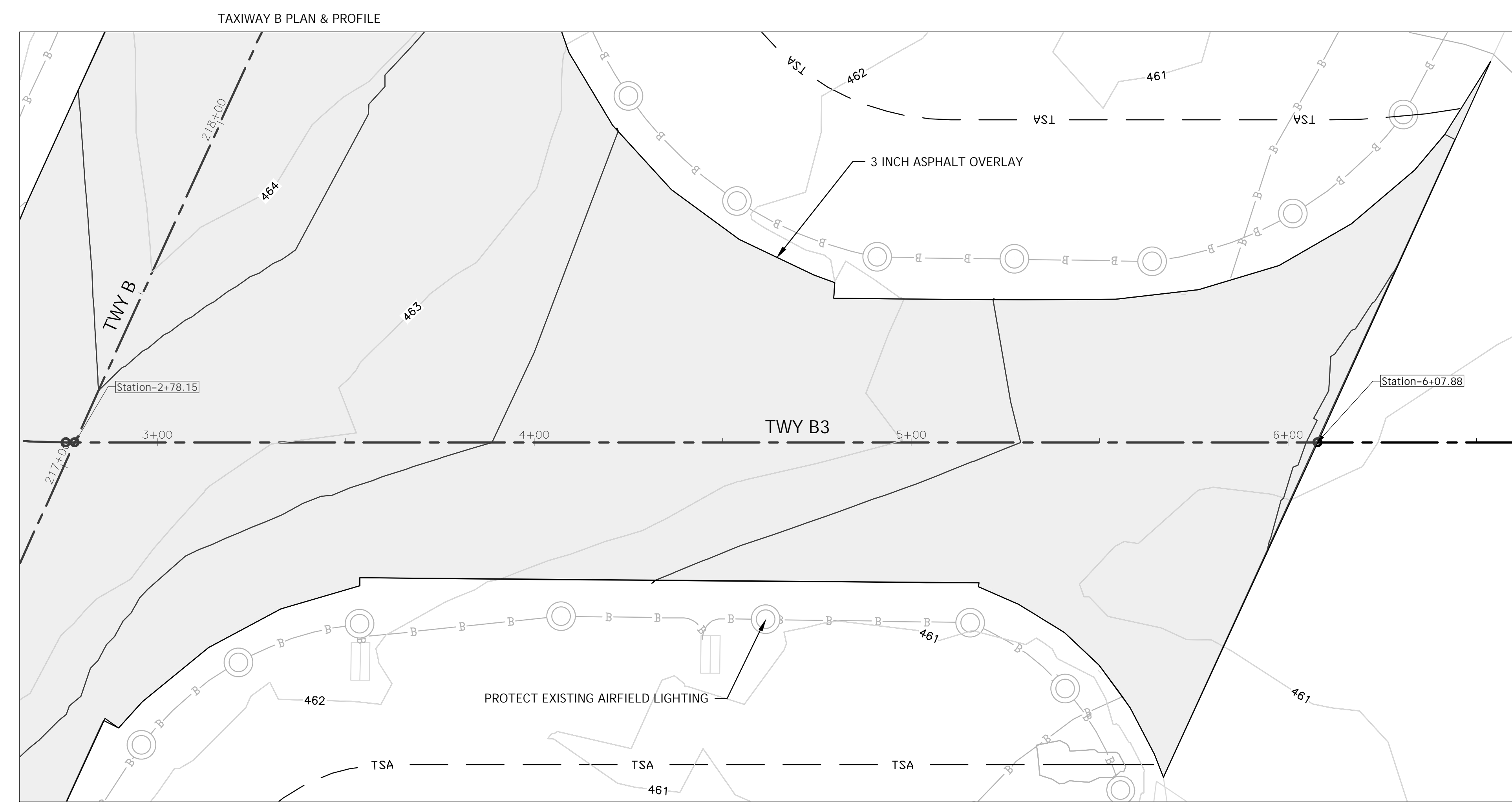
LETTING CONTRACT NUMBER
WI061



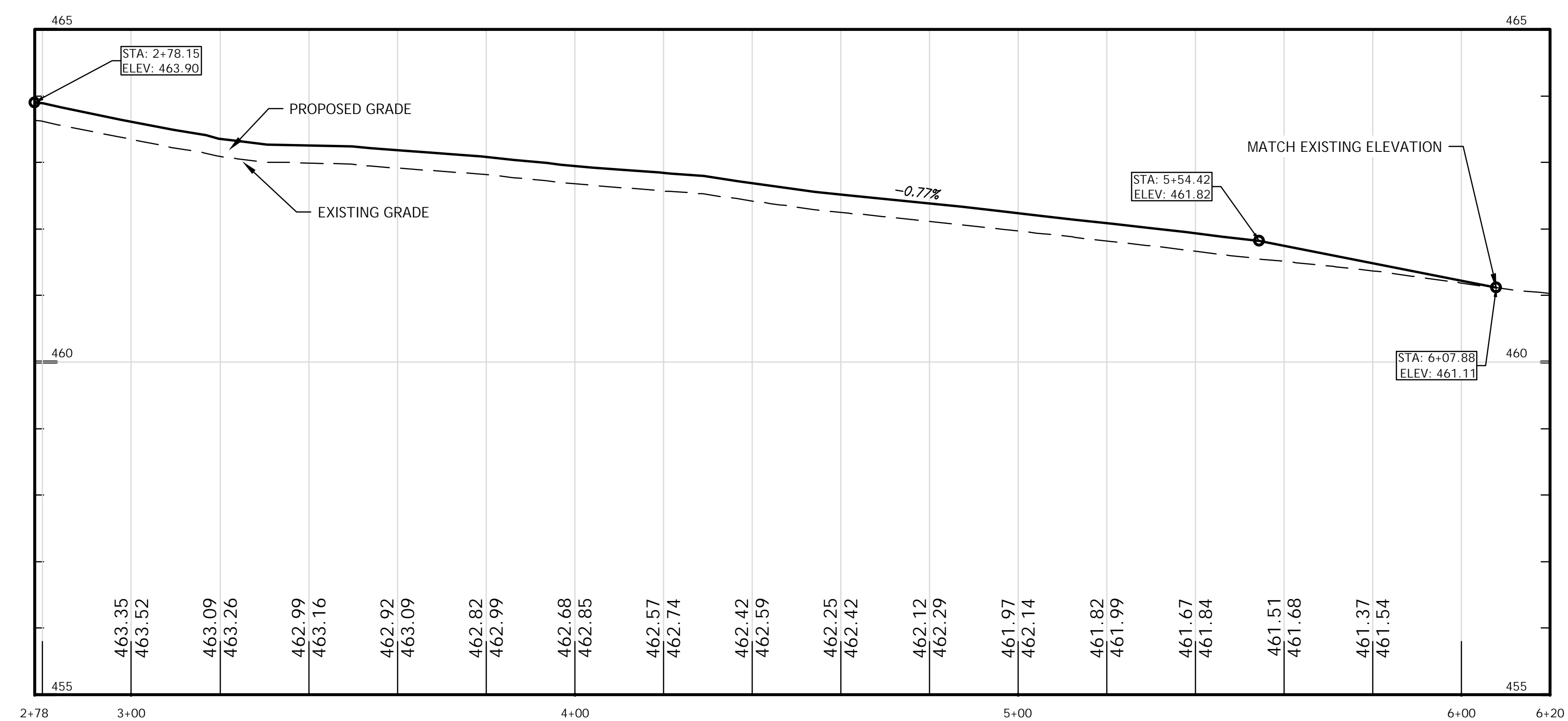
LEGEND

ASPHALT OVERLAY	
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EXISTING CONTOURS	
PROPOSED CONTOURS	
GUIDANCE SIGN	

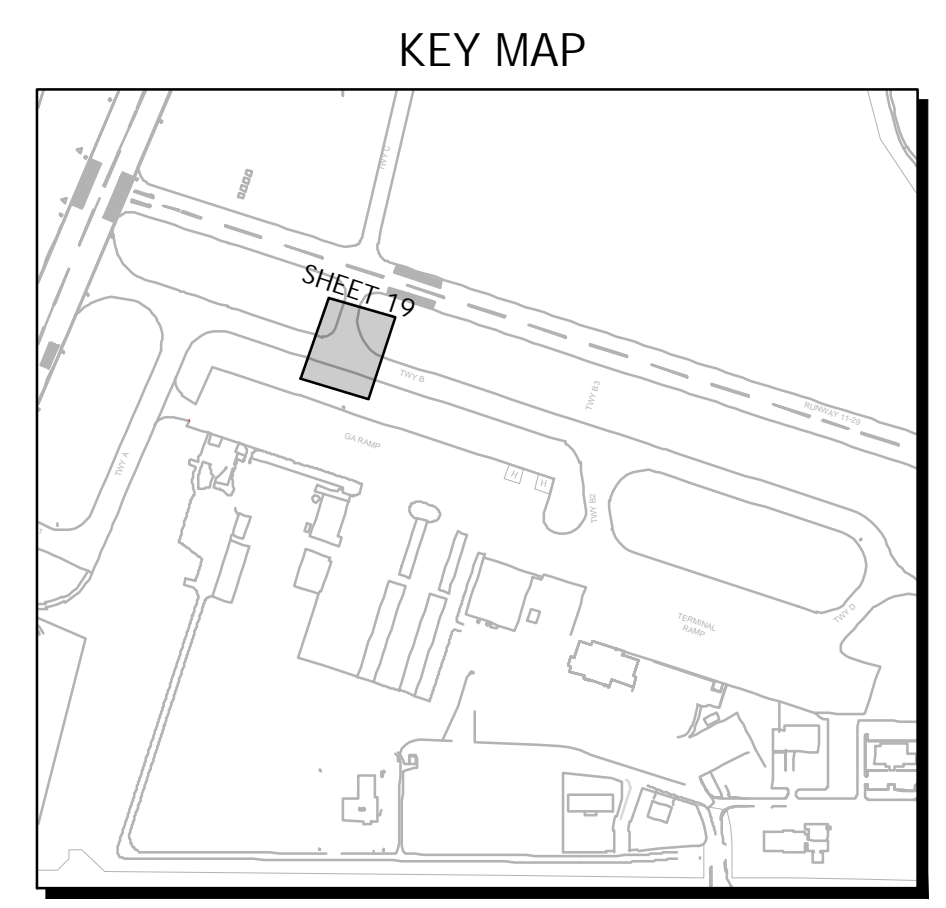
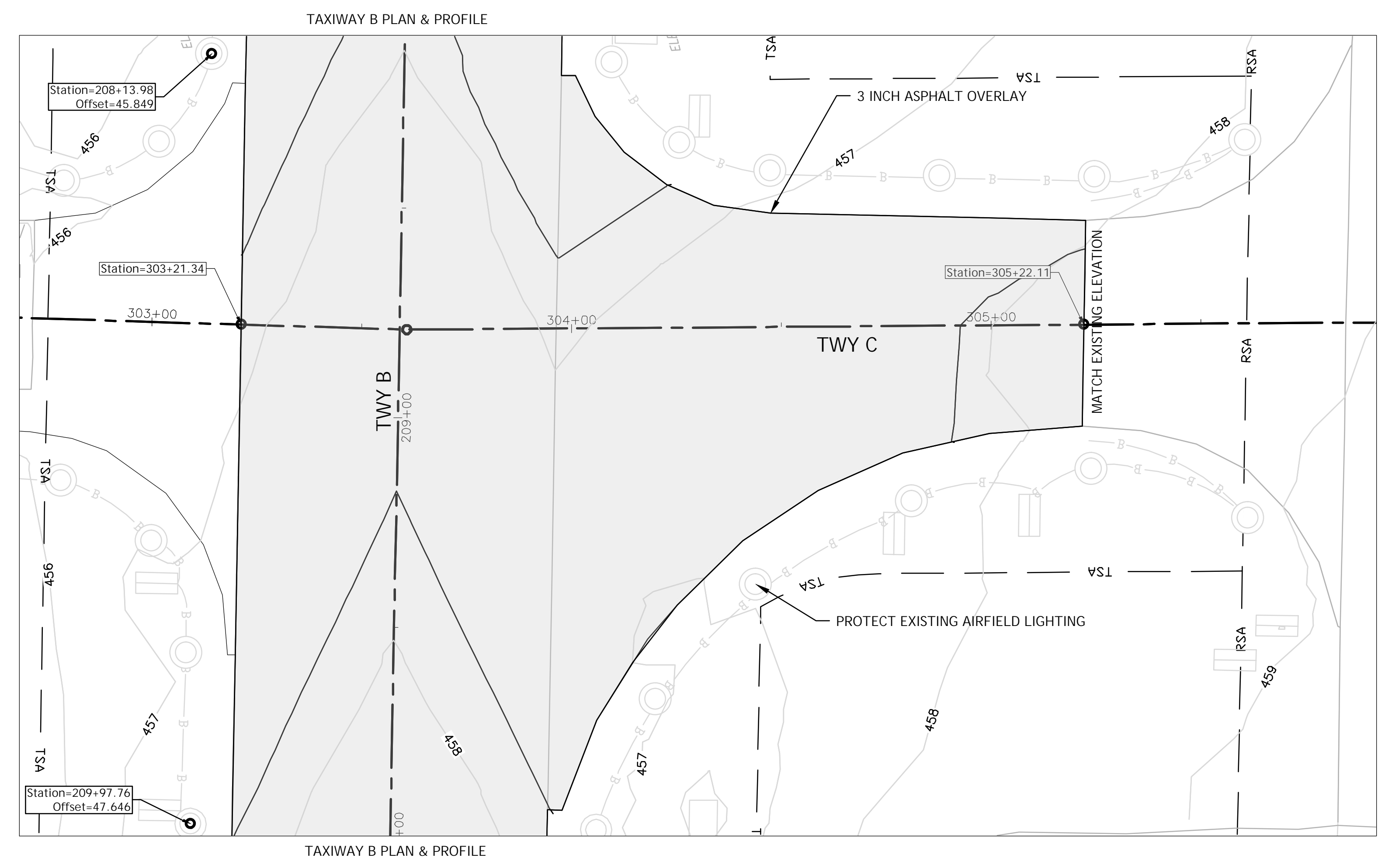
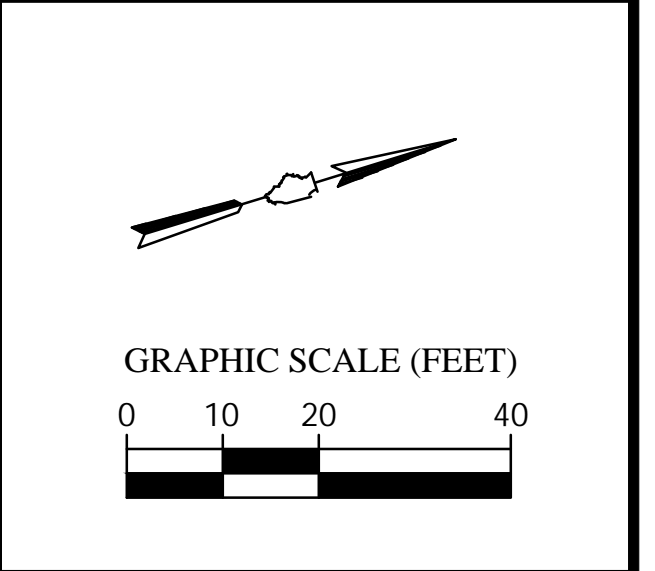
- PAVING NOTES
1. THE CONTRACTOR SHALL AVOID TRACKING TRUCKS AND EQUIPMENT OVER ADJACENT PAVEMENT AREAS UNLESS SUFFICIENT PROTECTION HAS BEEN PROVIDED AND APPROVAL FROM THE AIRPORT ENGINEER HAS BEEN GRANTED.
 2. TRACKING OF TACK COAT, DAMAGE TO THE PAVEMENT, OR DAMAGE TO GROOVED SURFACES WILL REQUIRE THE WORK TO STOP UNTIL ACCEPTABLE PROTECTION OR A CHANGE IN THE CONSTRUCTION METHOD HAS BEEN APPROVED.
 3. ANY DAMAGE SHALL BE REPAIRED OR RESTORED TO ITS ORIGINAL CONDITIONS AT THE CONTRACTOR'S OWN EXPENSE.



TAXIWAY B PLAN & PROFILE

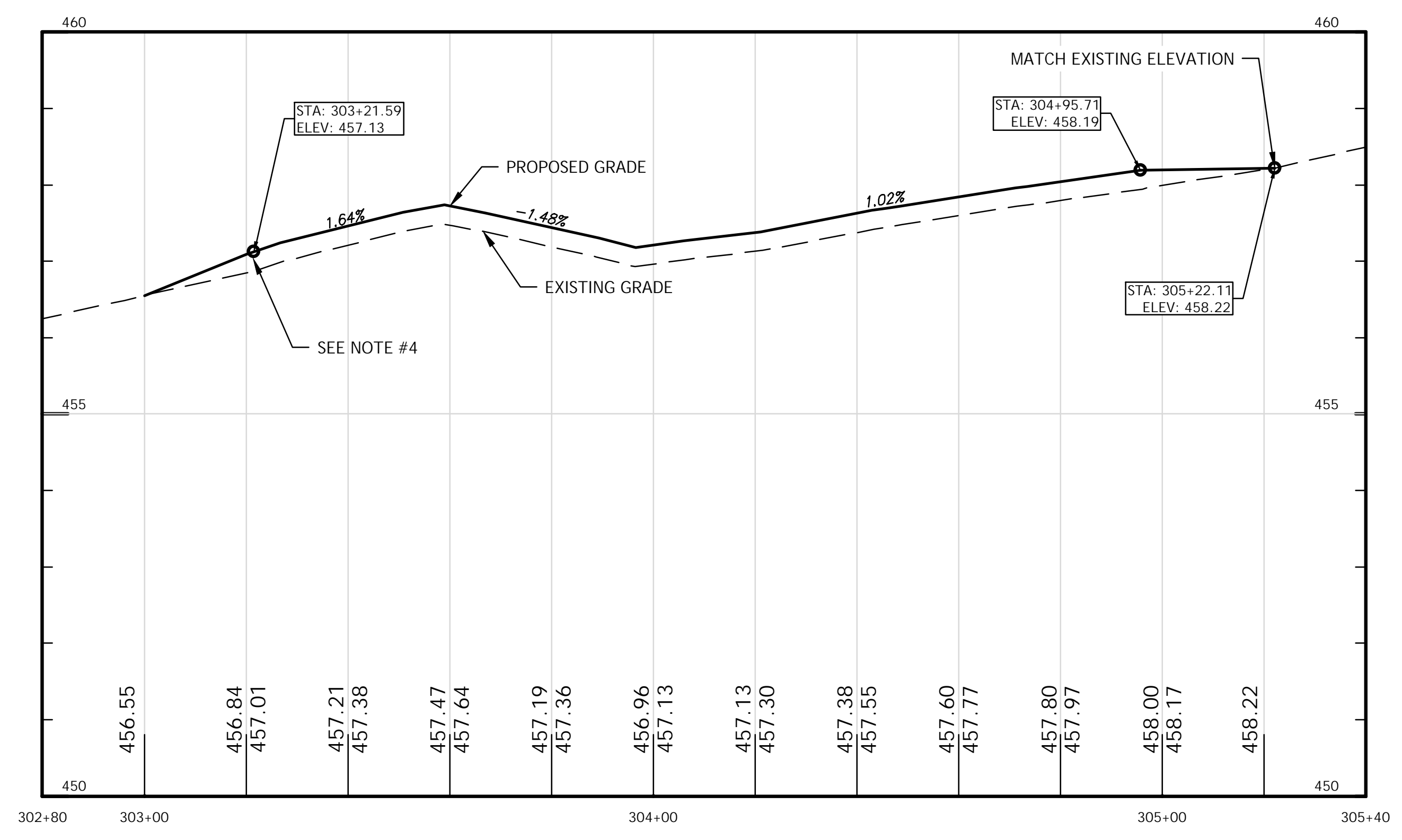


LETTING CONTRACT NUMBER
WI061



LEGEND

ASPHALT OVERLAY	
TAXIWAY EDGE LIGHT	
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EXISTING CONTOURS	
PROPOSED CONTOURS	
GUIDANCE SIGN	
RUNWAY SAFETY AREA	

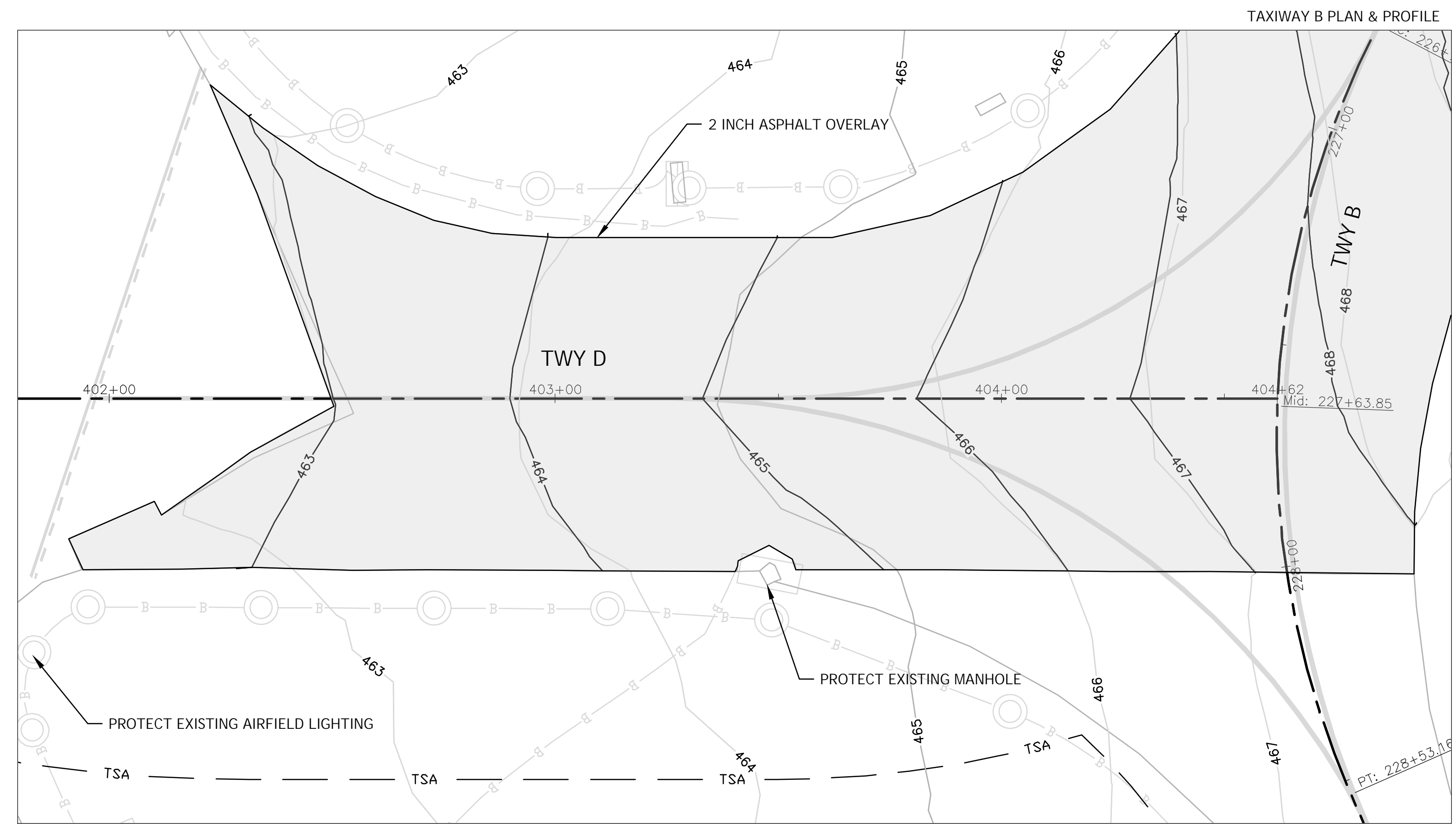


PAVING NOTES

1. THE CONTRACTOR SHALL AVOID TRACKING TRUCKS AND EQUIPMENT OVER ADJACENT PAVEMENT AREAS UNLESS SUFFICIENT PROTECTION HAS BEEN PROVIDED AND APPROVAL FROM THE AIRPORT ENGINEER HAS BEEN GRANTED.
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3. ANY DAMAGE SHALL BE REPAIRED OR RESTORED TO ITS ORIGINAL CONDITIONS AT THE CONTRACTOR'S OWN EXPENSE.
4. IF ADDITIVE BID #1 IS ACCEPTED A PORTION OF TAXIWAY CHARLIE BETWEEN TAXIWAY BRAVO AND THE GA RAMP SHALL BE REMOVED, SEE DEMOLITION PLAN SHEET. IF ADDITIVE BID #1 IS REJECTED, A BUTT JOINT SHALL BE INSTALLED AS INSTRUCTED BY ENGINEER

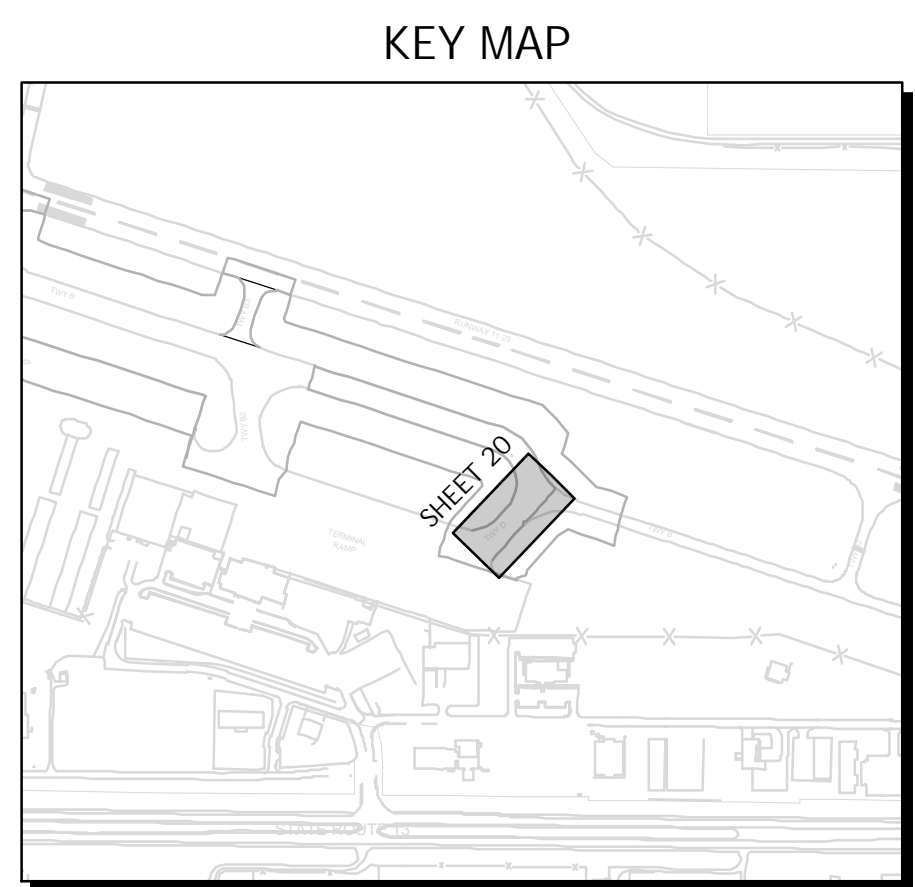
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LETTING CONTRACT NUMBER
WI061

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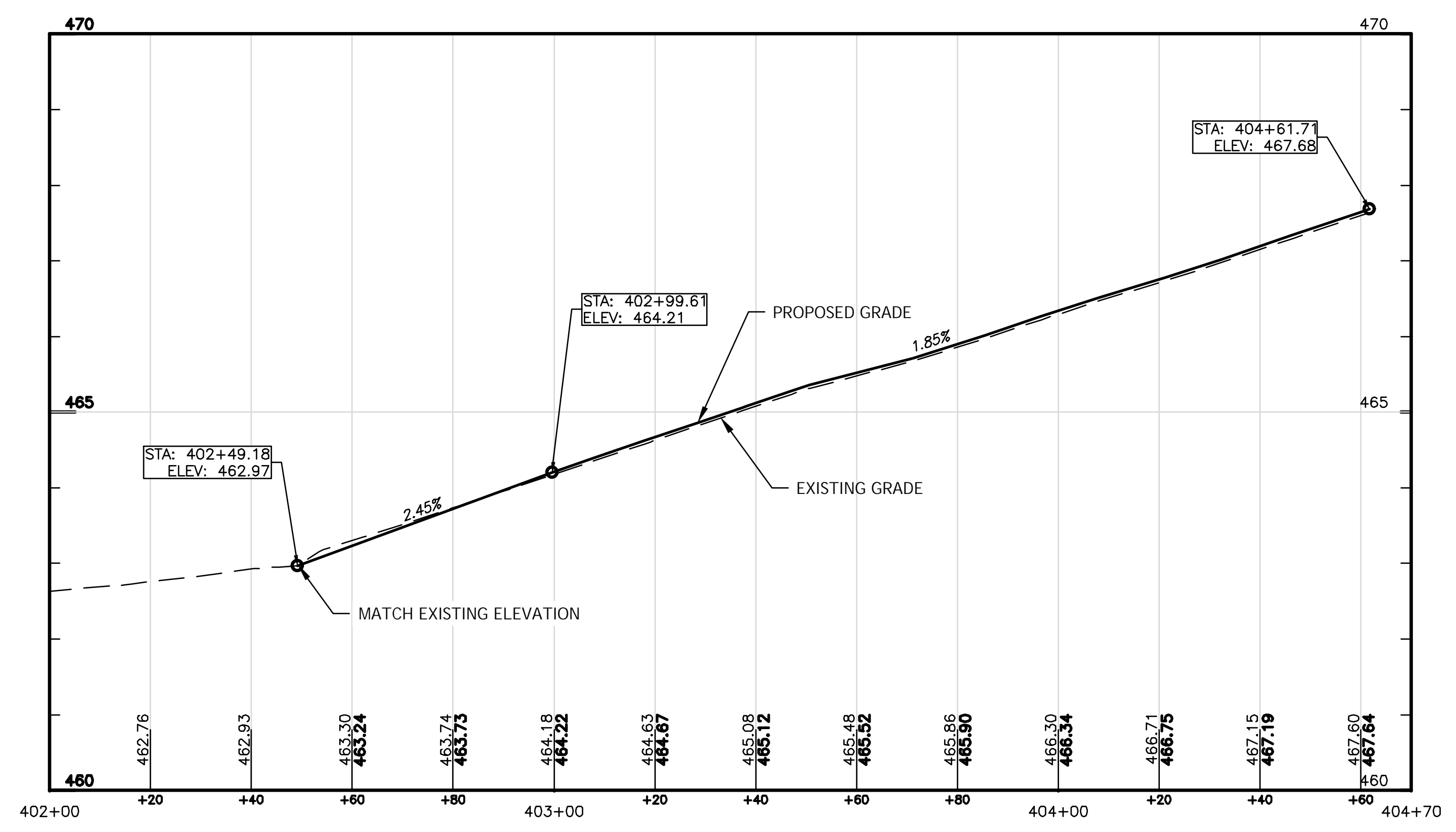


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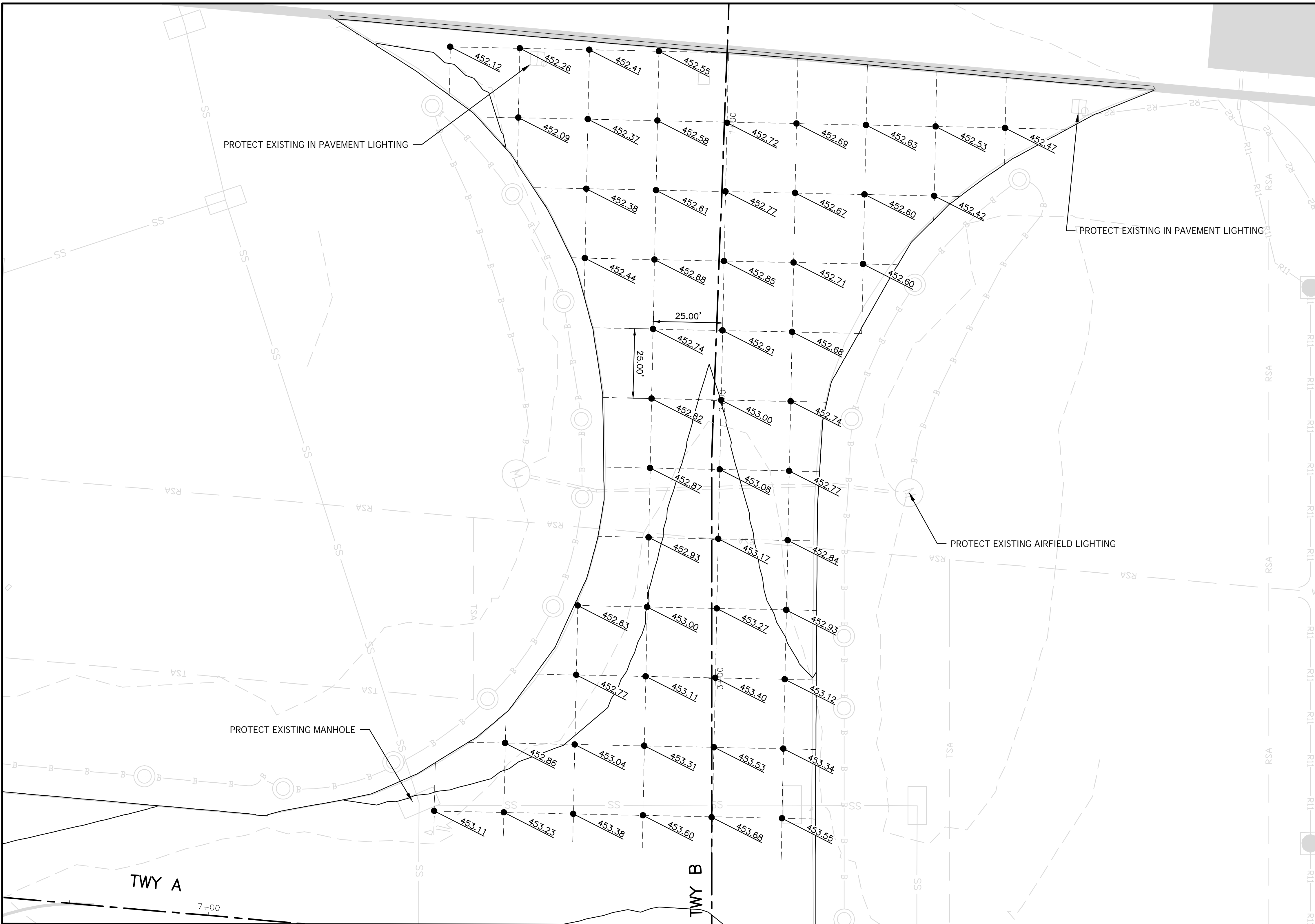
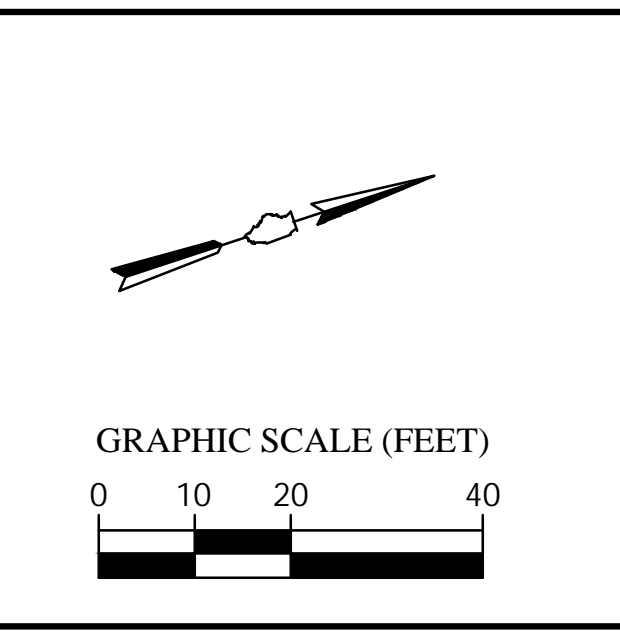
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TAXIWAY EDGE LIGHT	
TAXIWAY EDGE LIGHT ELECTRICAL	
EXISTING CONTOURS	
PROPOSED CONTOURS	
GUIDANCE SIGN	

PAVING NOTES

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LETTING CONTRACT NUMBER
WI061



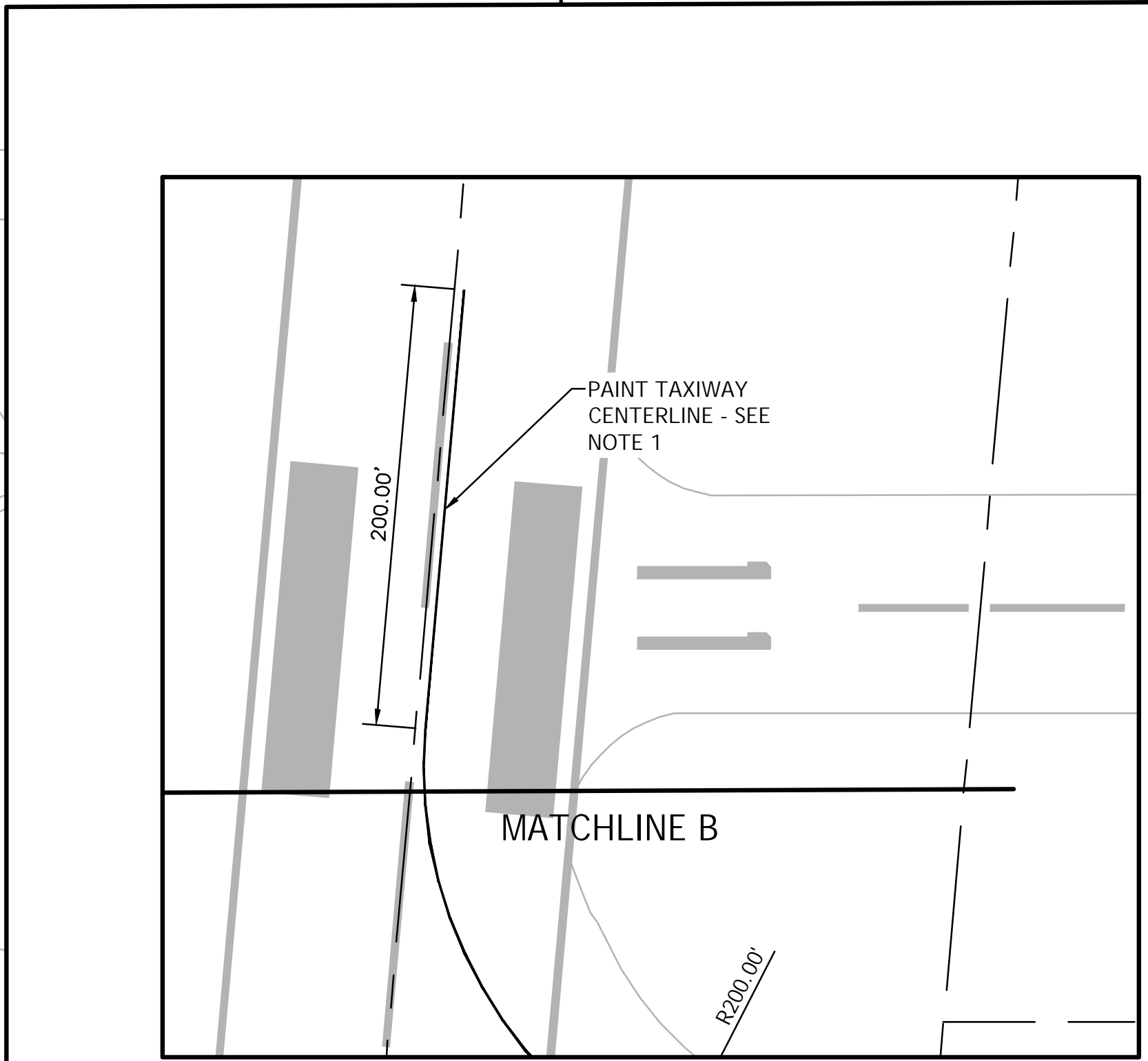
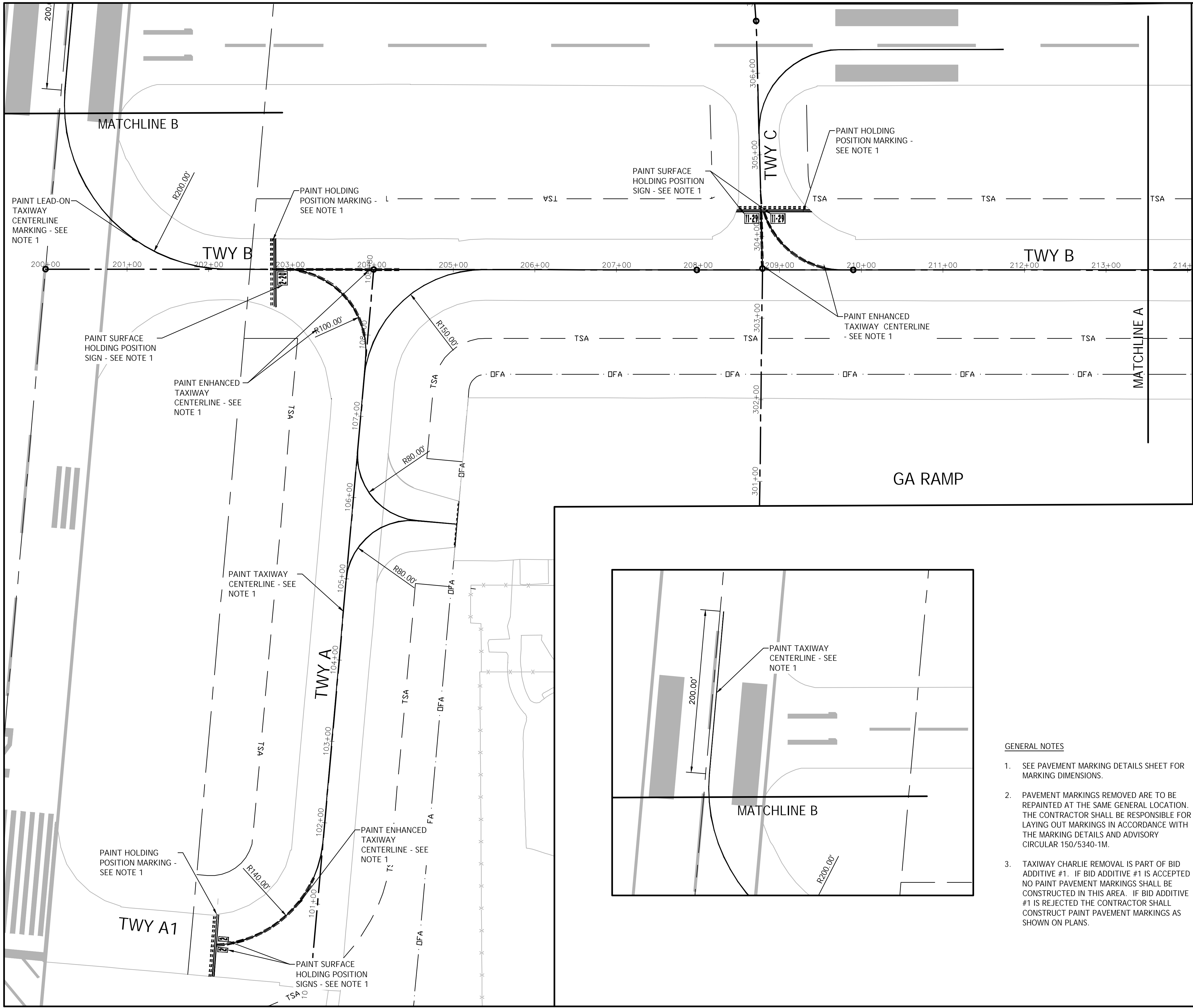
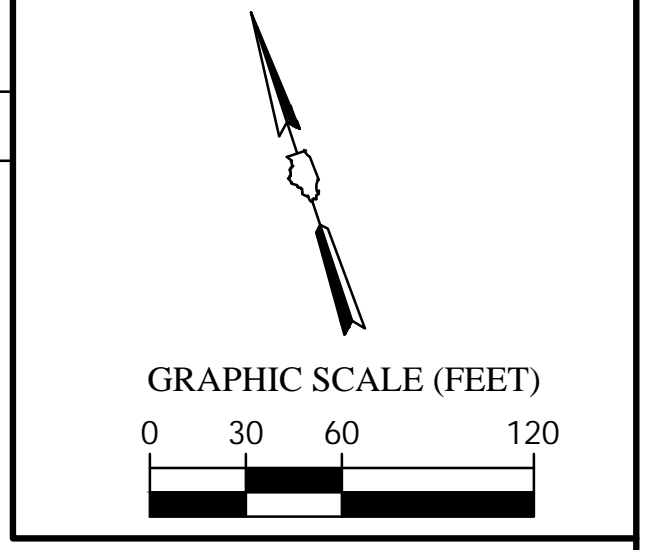
LEGEND

SPOT ELEVATION	###
TAXIWAY EDGE LIGHT	●
TAXIWAY EDGE LIGHT ELECTRICAL	— B — B —
EXISTING INLET STRUCTURE	□
RUNWAY IN-PAVEMENT LIGHT	□
RUNWAY SAFETY AREA	— RSA —

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STATE OF ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184-004515		© 2024 ALL RIGHTS RESERVED			
REVISIONS	DATE	NO.	<p>INTERSECTION DETAIL TAXIWAY ALPHA & BRAVO REHABILITATION VETERANS AIRPORT OF SOUTHERN ILLINOIS</p>		
Date	3/1/2024	DR. By	BLW	CK'D By	MKW
Scale	1" = 20'	Job No.	19204CZ	Sheet No.	21 of 30

LETTING CONTRACT NUMBER
W1061



PAVEMENT MARKING SCHEDULE	
PAVEMENT MARKING - WATERBORNE (YELLOW)	
DESCRIPTION	UNIT AREA (S.F.)
TAXIWAY CENTERLINES	9255
ENHANCED TAXIWAY - TWY A1	315
ENHANCED TAXIWAY - TWY B	315
ENHANCED TAXIWAY - TWY C	660
ENHANCED TAXIWAY - TWY B1	256
ENHANCED TAXIWAY - TWY B2	154
ENHANCED TAXIWAY - TWY B3	370
HOLDING POSTION - TWY A1	231
HOLDING POSTION - TWY B	254
HOLDING POSTION - TWY B1	100
HOLDING POSTION - TWY B2	100
HOLDING POSTION - TWY C	269
NON-MOVEMENT LINE - GA RAMP @ PROPOSED TWY A2	60
NON-MOVEMENT LINE - TWY B3	136
NON-MOVEMENT LINE - TWY D	90
NON-MOVEMENT LINE - TWY A2	45
PAVEMENT MARKING - BLACK BORDER	
DESCRIPTION	UNIT AREA (S.F.)
TAXIWAY CENTERLINES	8648
ENHANCED TAXIWAY - TWY A1	54
ENHANCED TAXIWAY - TWY B1	262
ENHANCED TAXIWAY - TWY B2	39
HOLDING POSTION - TWY A1	377
HOLDING POSTION - TWY B1	180
HOLDING POSTION - TWY B2	180
NON-MOVEMENT LINE - TWY A2	72
PAVEMENT MARKING SYMBOLS (INCLUDES WHITE, RED AND BLACK BORDER)	
DESCRIPTION	UNIT AREA (S.F.)
"2" SURFACE PAINTING HOLDING POSTION SIGN	72
"2-20" SURFACE PAINTING HOLDING POSTION SIGN	211
"11-29" SURFACE PAINTING HOLDING POSTION SIGN	223
"29" SURFACE PAINTING HOLDING POSTION SIGN	125

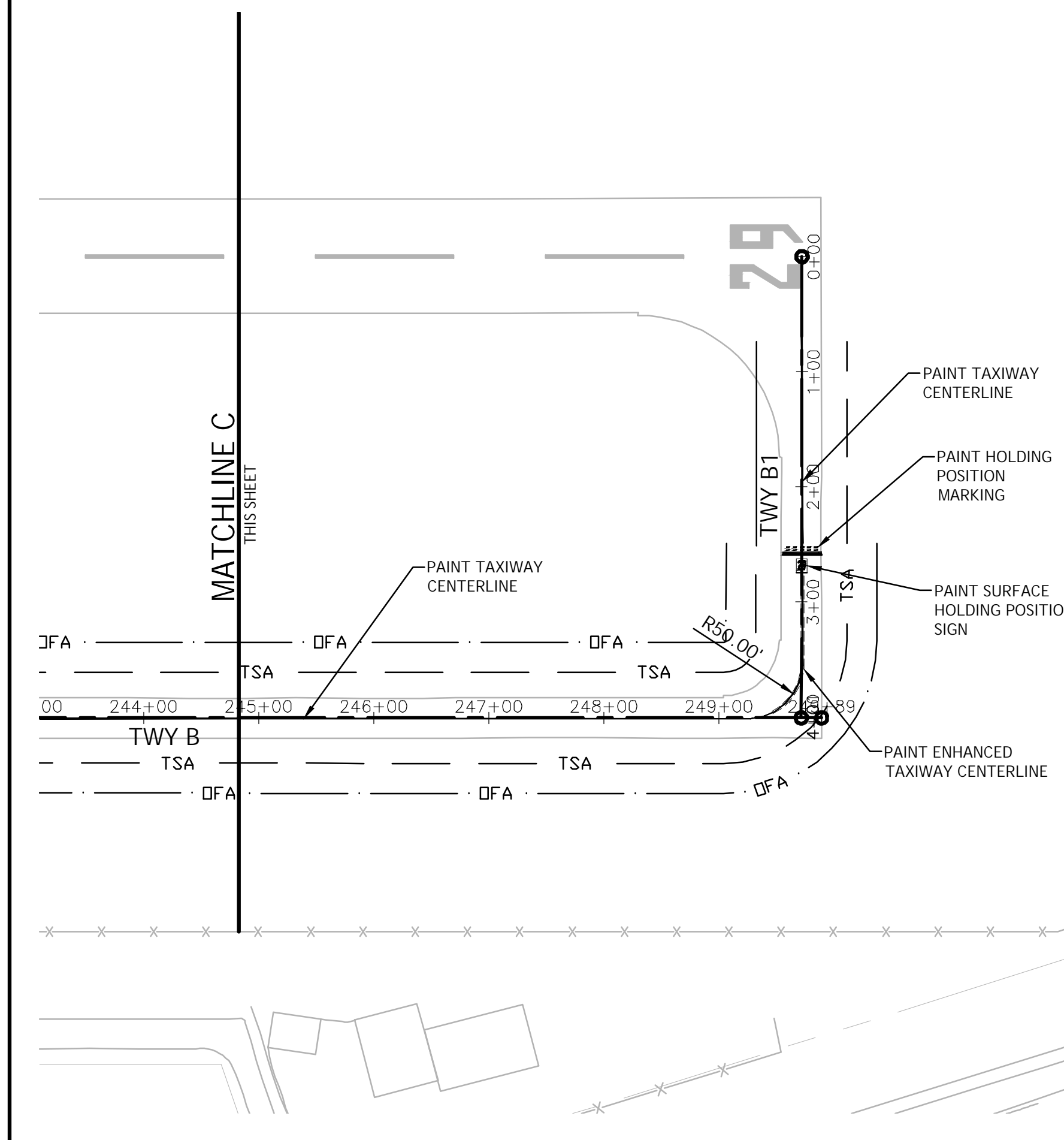
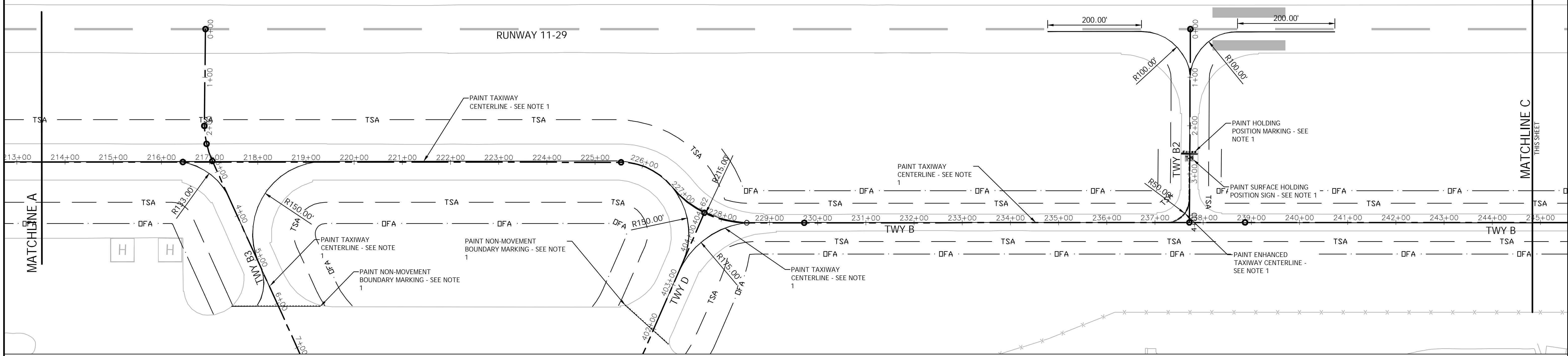
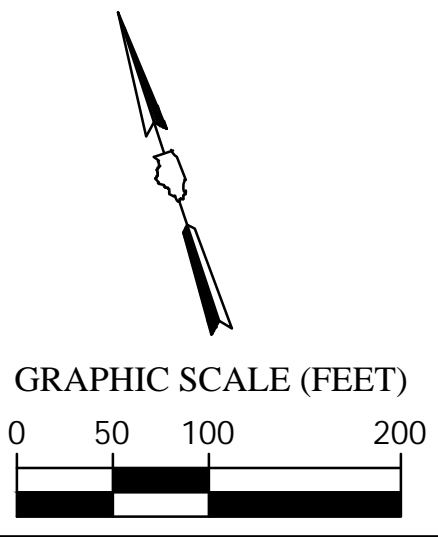
GENERAL NOTES

- SEE PAVEMENT MARKING DETAILS SHEET FOR MARKING DIMENSIONS.
- PAVEMENT MARKINGS REMOVED ARE TO BE REPAINTED AT THE SAME GENERAL LOCATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LAYING OUT MARKINGS IN ACCORDANCE WITH THE MARKING DETAILS AND ADVISORY CIRCULAR 150/5340-1M.
- TAXIWAY CHARLIE REMOVAL IS PART OF BID ADDITIVE #1. IF BID ADDITIVE #1 IS ACCEPTED NO PAINT PAVEMENT MARKINGS SHALL BE CONSTRUCTED IN THIS AREA. IF BID ADDITIVE #1 IS REJECTED THE CONTRACTOR SHALL CONSTRUCT PAINT PAVEMENT MARKINGS AS SHOWN ON PLANS.

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PAVEMENT MARKINGS PLAN TAXIWAY ALPHA & BRAVO REHABILITATION VETERANS AIRPORT OF SOUTHERN ILLINOIS	
Date	3/1/2024
DR. By	BLW
CK'D By	MKW
Scale	1" = 60'
Job No.	19204CZ
Sheet No.	22 of 30

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LETTING CONTRACT NUMBER
W1061



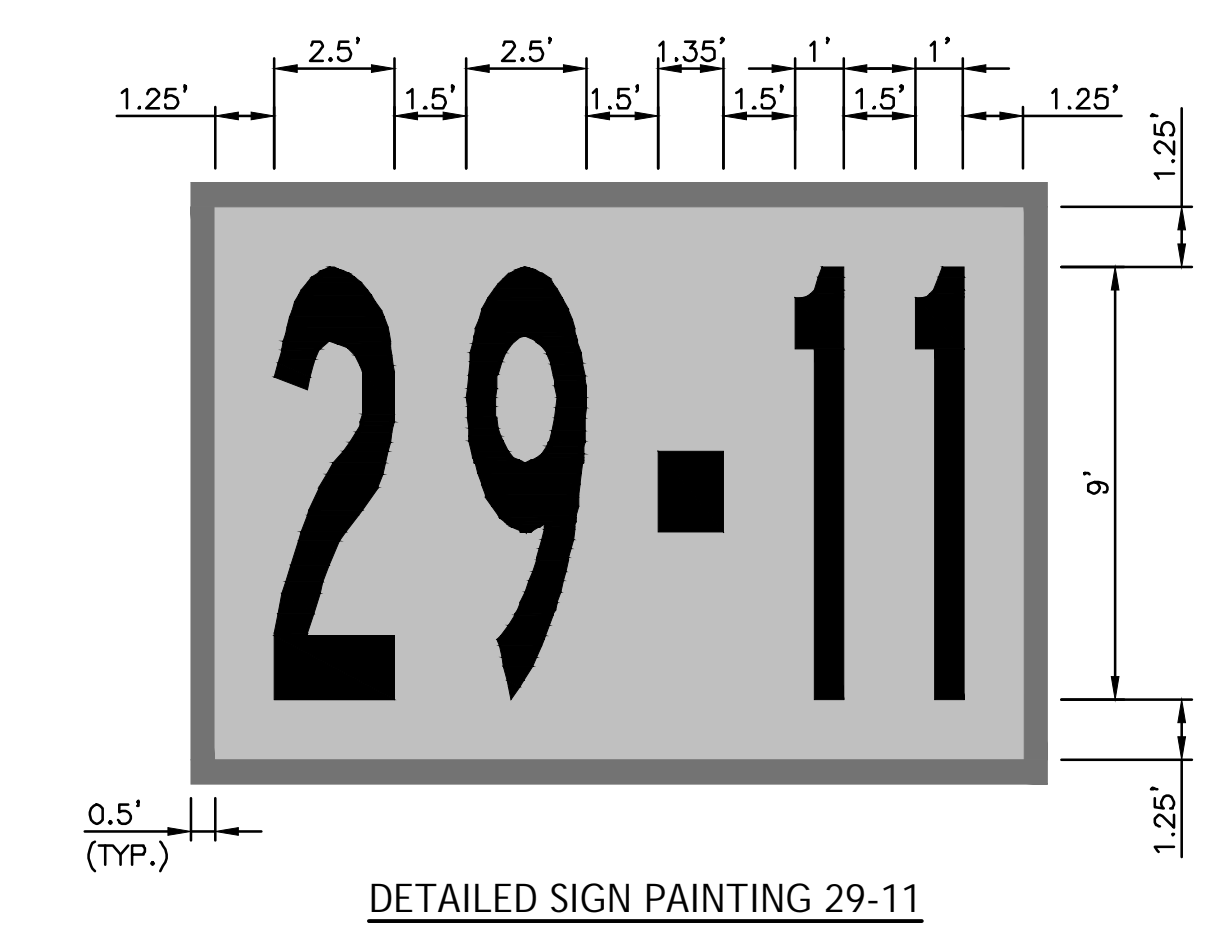
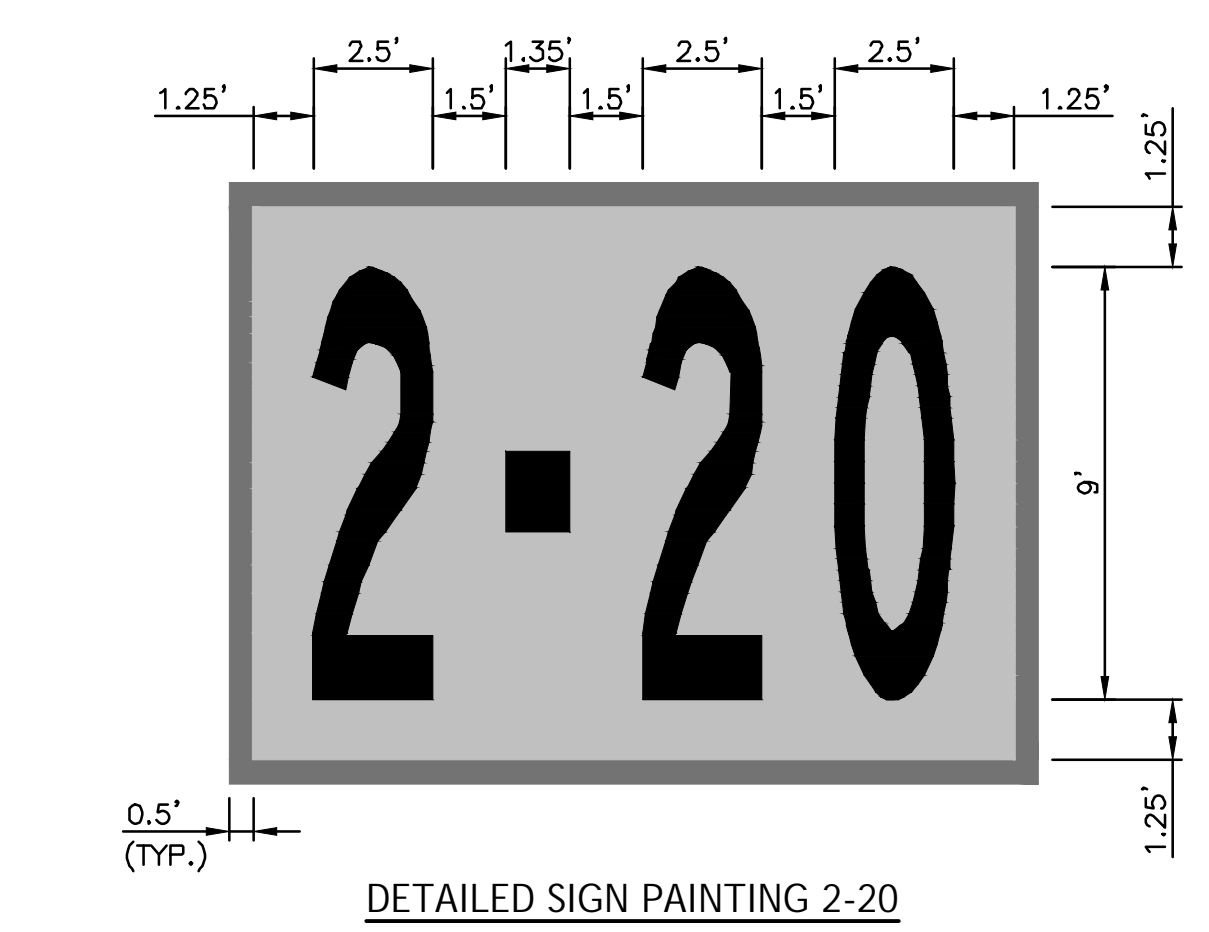
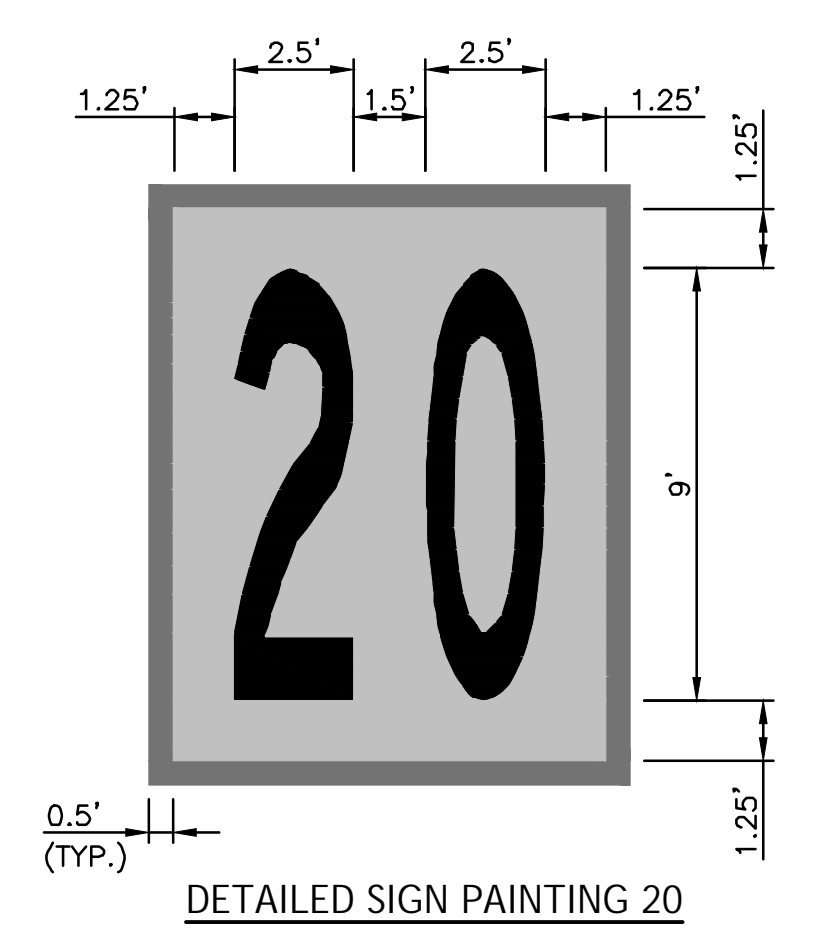
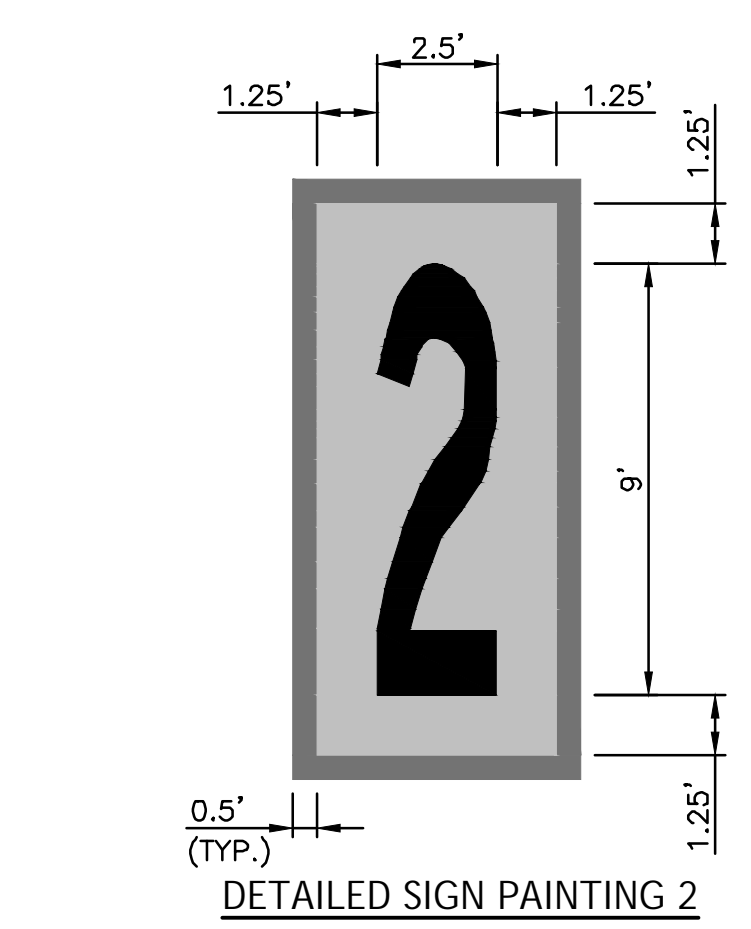
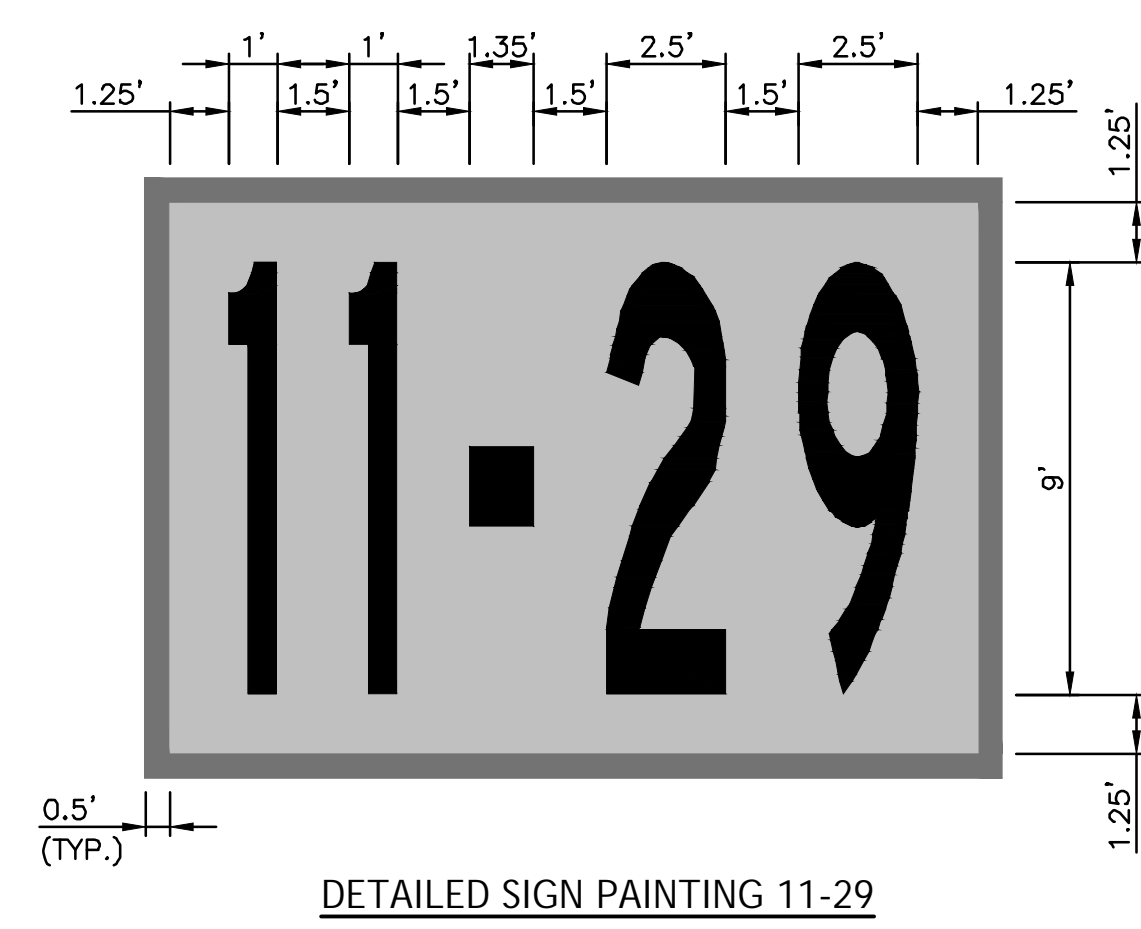
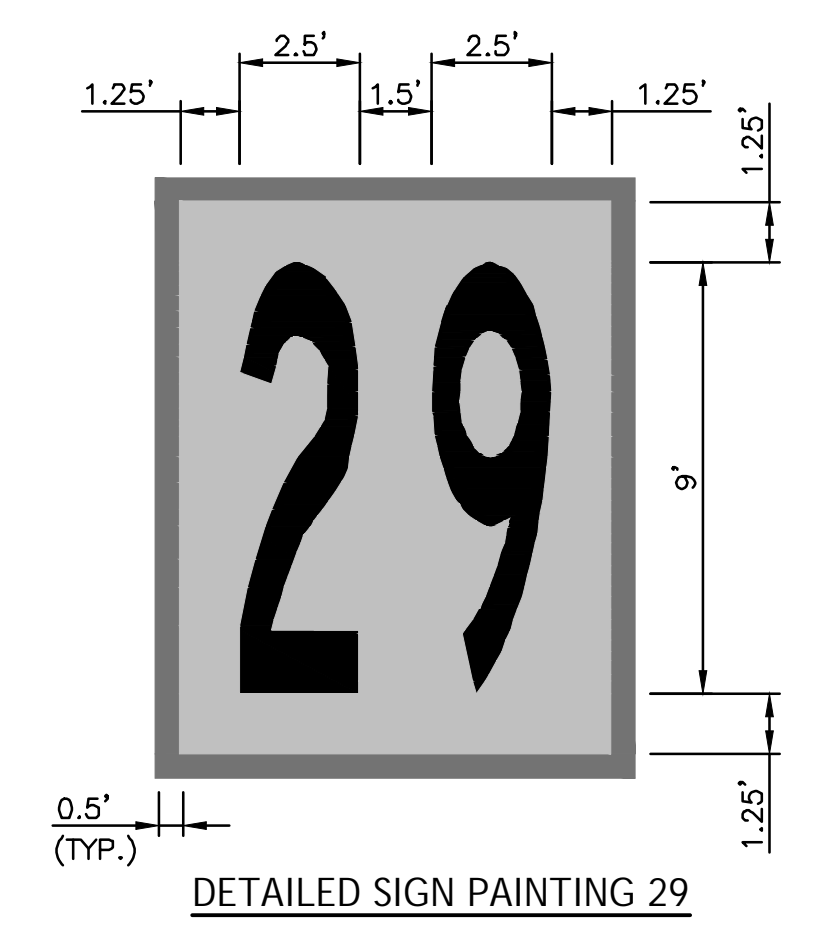
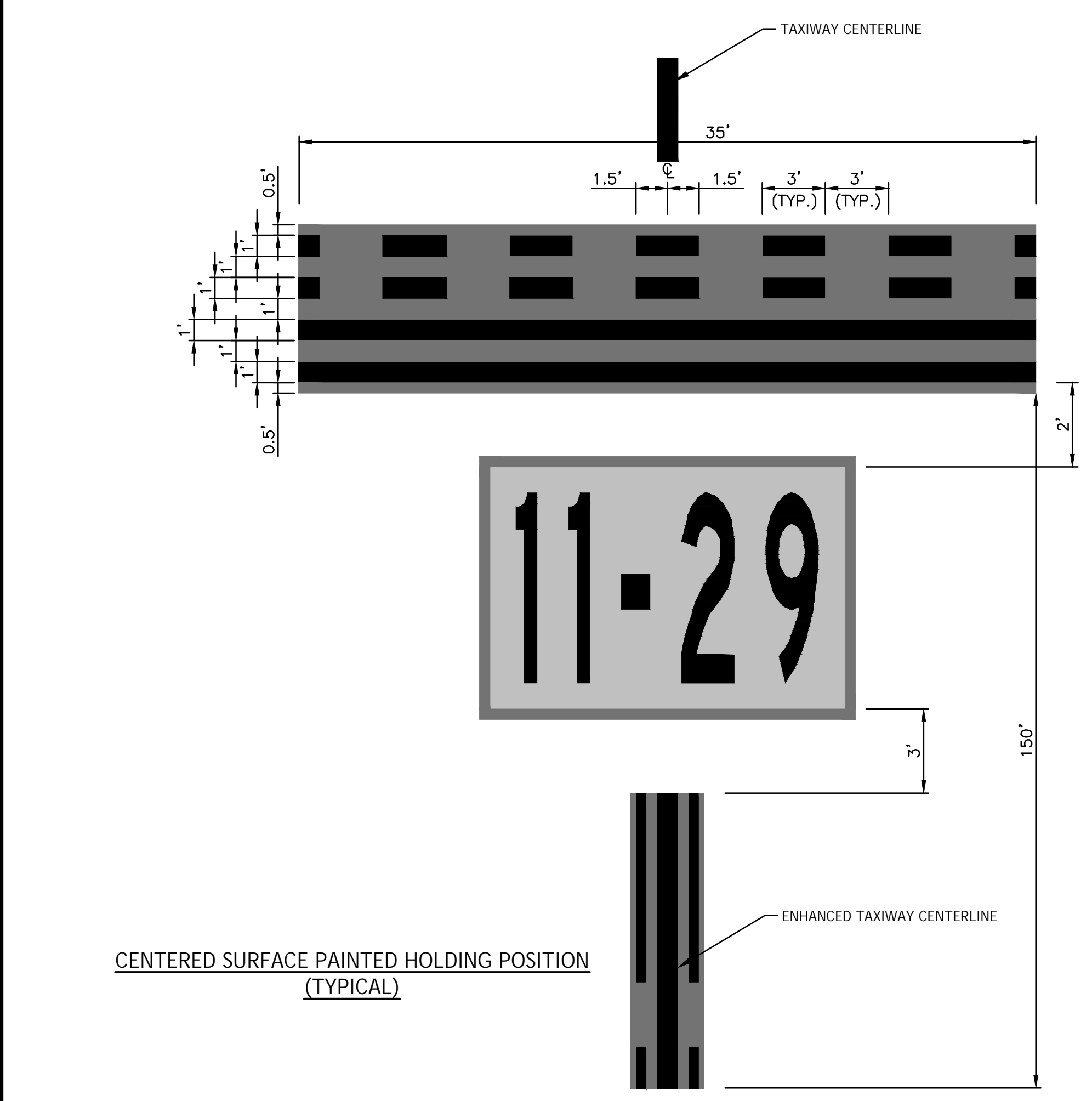
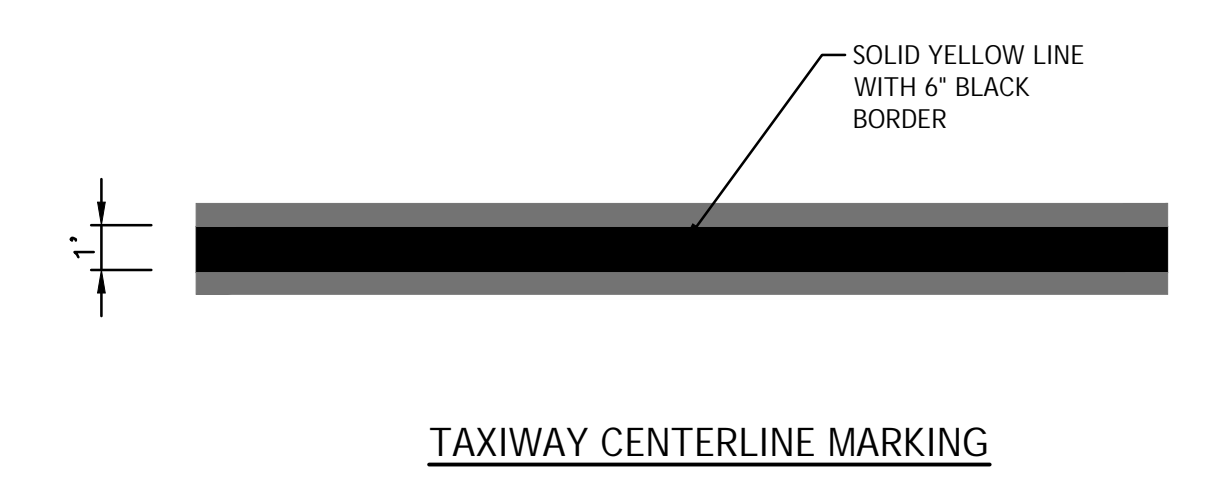
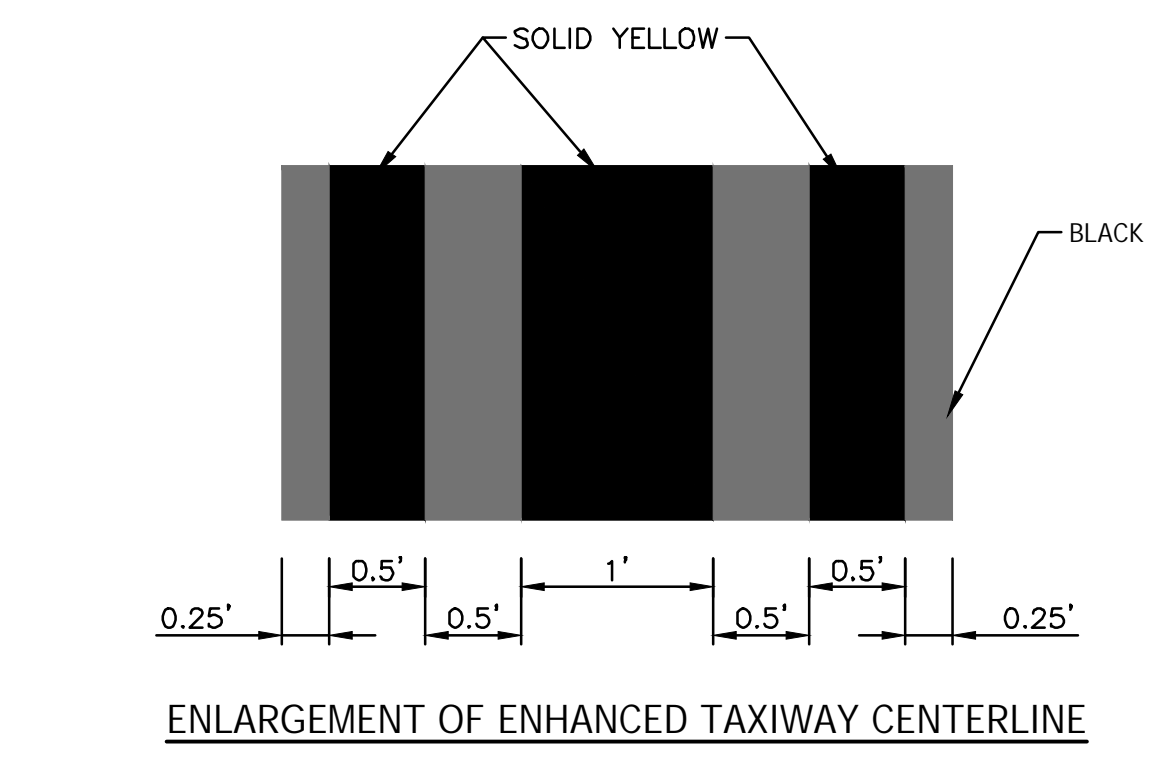
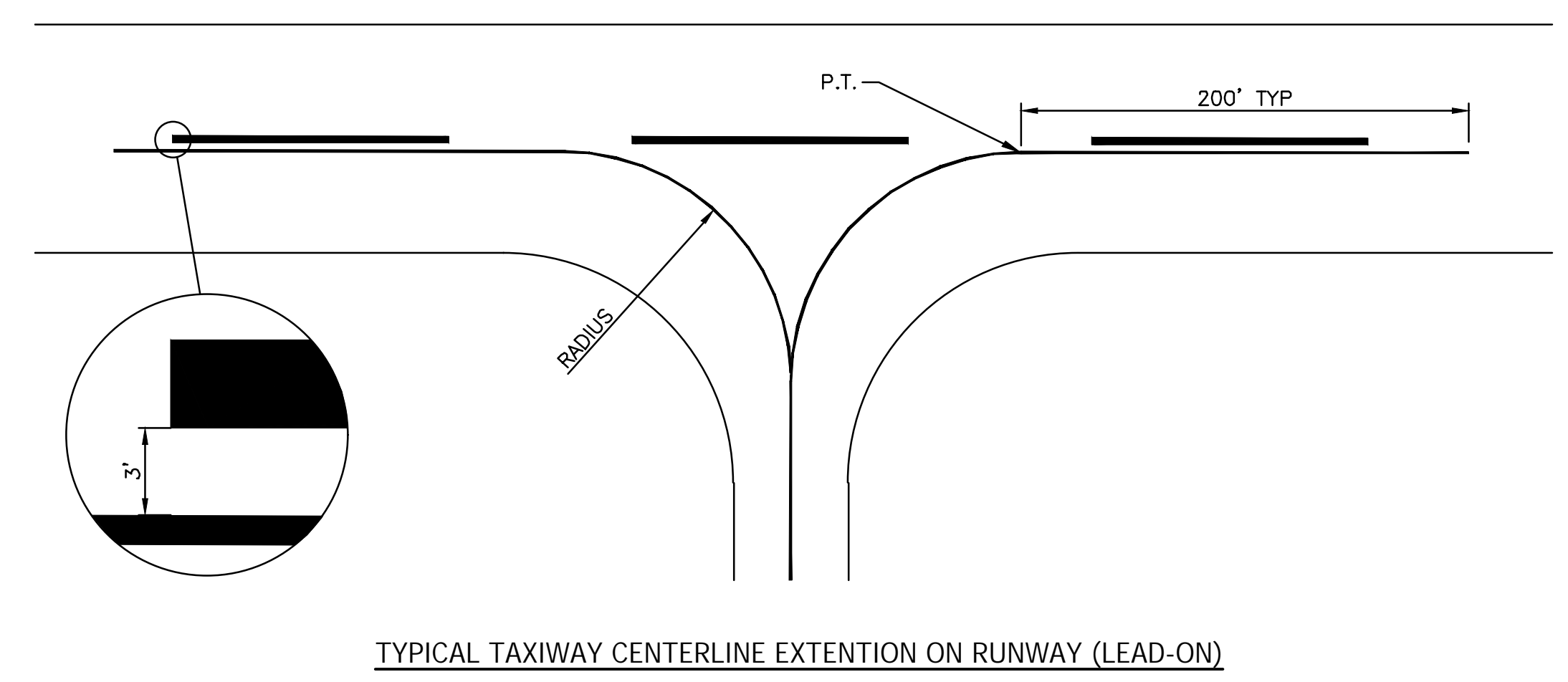
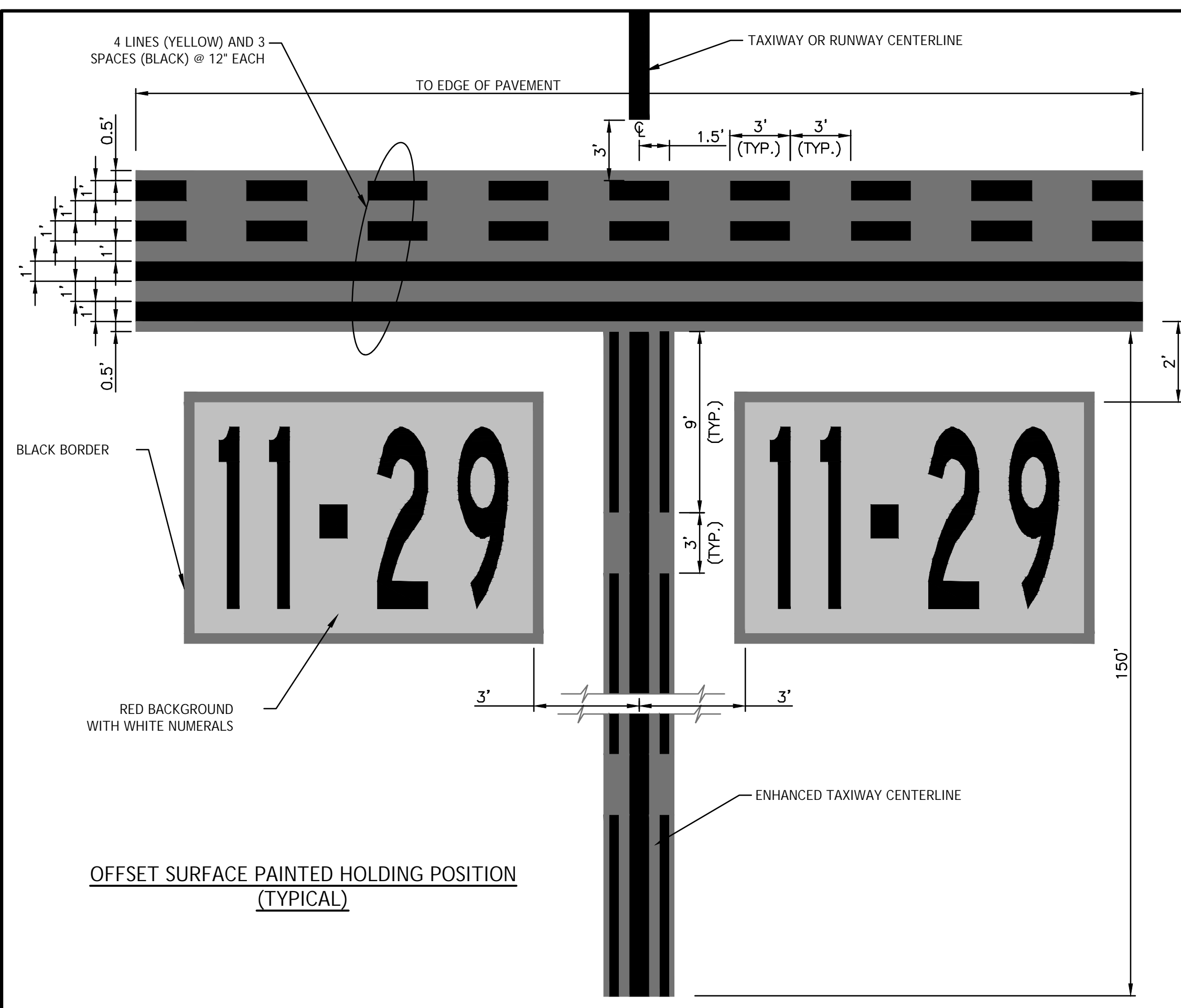
- GENERAL NOTES**
- SEE PAVEMENT MARKING DETAILS SHEET FOR MARKING DIMENSIONS.
 - PAVEMENT MARKINGS REMOVED ARE TO BE REPAINTED AT THE SAME GENERAL LOCATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LAYING OUT MARKINGS IN ACCORDANCE WITH THE MARKING DETAILS AND ADVISORY CIRCULAR 150/5340-1M.
 - TAXIWAY BRAVO 3 REMOVAL IS PART OF BID ADDITIVE #2. IF BID ADDITIVE #2 IS ACCEPTED NO PAINT PAVEMENT MARKINGS SHALL BE CONSTRUCTED IN THIS AREA. IF BID ADDITIVE #2 IS REJECTED THE CONTRACTOR SHALL CONSTRUCT PAINT PAVEMENT MARKINGS AS SHOWN ON PLANS.
 - IF ADDITIVE BID #2 IS ACCEPTED AND TAXIWAY B3 IS REMOVED, PAVEMENT MARKINGS ON RUNWAY 11-29 SHALL BE REMOVED AS DIRECTED BY THE ENGINEER.

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PAVEMENT MARKINGS PLAN TAXIWAY ALPHA & BRAVO REHABILITATION VETERANS AIRPORT OF SOUTHERN ILLINOIS	
Date	3/1/2024
DR. By	BLW
CK'D By	MKW
Scale	1" = 100'
Job No.	19204CZ
Sheet No.	23 of 30

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LETTING CONTRACT NUMBER
W1061

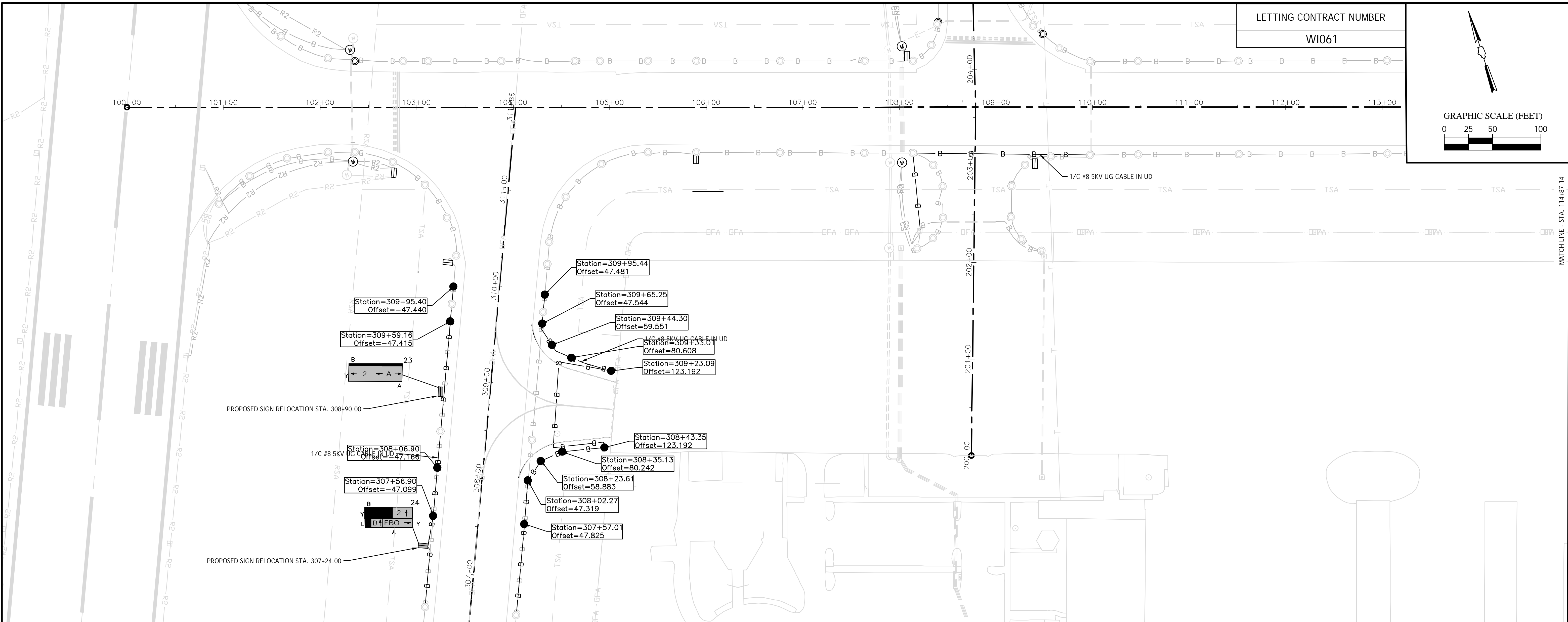
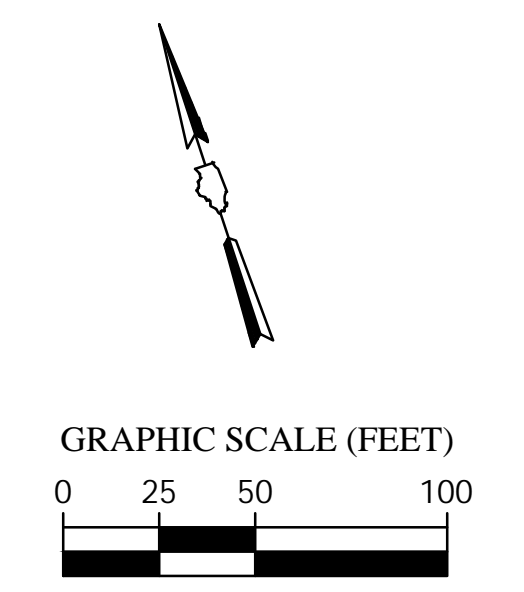
- PAVEMENT MARKING NOTES:**
1. IMMEDIATELY PRIOR TO THE APPLICATION OF PAINT, ALL SURFACES SHALL BE DRY AND FREE FROM DIRT, GREASE, OIL, LAITANCE, OR OTHER FOREIGN MATERIAL WHICH WOULD REDUCE THE BOND BETWEEN THE PAINT AND THE PAVEMENT. THIS SHALL INCLUDE EXISTING PAINTED AREAS.
 2. ALL TAXIWAY CENTERLINE MARKINGS ARE TO BE INTERRUPTED FOR RUNWAY MARKINGS
 3. SURFACE PAINTED HOLDING POSITION SIGNS SHALL BE RED BACKGROUND WITH WHITE NUMERALS.
 4. NUMERALS SHALL CONFORM IN STYLE, SIZE AND APPEARANCE TO THOSE USED IN APPENDIX B OF AC150/5340-1M.
 5. ALL MARKINGS SHALL HAVE A 6 INCH BLACK BORDER.
 6. PAINT SHALL MEET REQUIREMENTS OF TECHNICAL SPECIFICATION P-620.
 7. ALL PAVEMENT MARKINGS SHALL CONFORM TO FAA AC 150/5340-1, CURRENT EDITION.



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PAVEMENT MARKINGS DETAILS	
TAXIWAY ALPHA & BRAVO REHABILITATION	
VETERANS AIRPORT OF SOUTHERN ILLINOIS	
Date	3/1/2024
DR. By	BLW
CK'D By	MKW
Scale	NTS
Job No.	19204CZ
Sheet No.	24 of 30

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LETTING CONTRACT NUMBER
W1061



- PLAN NOTES**
1. CONTRACTOR SHALL HAND DIG TO LOCATE EXISTING UNDERGROUND CABLE.
 2. EXISTING UNDERGROUND CABLE FEEDING RUNWAY 11-29 EDGE LIGHTING IN THESE AREAS. LOCATE ALL SUCH CABLE AND HAND EXCAVATE WHEREVER CROSSING IS NECESSARY FOR PROPOSED CABLE INSTALLATION.
 3. LOCATE AND EXPOSE ENDS OF EXISTING DUCT FOR ADDITION OF PROPOSED TAXIWAY B CABLE. HAND-EXCAVATE AS NECESSARY TO AVOID DAMAGE TO EXISTING RUNWAY 11-29 CABLE. DISCONNECT AND REMOVE EXISTING CABLES TO BE REPLACED FOR TAXIWAY B CIRCUIT.
 4. EXISTING PAPI/REIL 29 UNDERGROUND CABLE TO REMAIN.
 5. PROPOSED CONCRETE FOUNDATION AND L867 BASE FOR THIS TAXI GUIDANCE SIGN ENTIRELY. INSTALL PER DETAIL, SHEET 31.
 6. SERIES-CONNECT PROPOSED FEEDS TO SIGN TO THE EXISTING RUNWAY 2-20 CIRCUIT AT DISTANCE REMAINING SIGN SHOWN. PROVIDE 2/C, #6, 5 KV, TYPE C CABLE IN UNIT DUCT. OFFSET CABLE ROUTING AS REQUIRED TO AVOID EXISTING UNDERGROUND CABLES TO REMAIN. REMOVE EXISTING TAXIWAY CABLES FROM THE SIGN BASE.
 7. 1/C, #8, 5 KV, TYPE C CABLE IN UNIT DUCT - TYPICAL FOR ALL TAXIWAY LIGHTING CABLE UNLESS NOTED OTHERWISE. OFFSET CABLE ROUTING AS REQUIRED TO AVOID EXISTING UNDERGROUND CABLES TO REMAIN.
 8. REMOVE AND REPLACE EXISTING CABLE FROM EXISTING DUCT BANK AS FOLLOWS:
TAXIWAY B - 1 - #8, 5 KV, TYPE C
 9. 1/C, #6, 5 KV, TYPE C CABLE IN UNIT DUCT.
 10. SERIES-CONNECT PROPOSED FEEDS TO SIGN TO THE EXISTING RUNWAY 2-20 CIRCUIT AT RUNWAY EDGE LIGHT SHOWN. PROVIDE 1/C, #6, 5 KV, TYPE C CABLE IN UNIT DUCT. OFFSET CABLE ROUTING AS REQUIRED TO AVOID EXISTING UNDERGROUND CABLES TO REMAIN.
 11. CORE DRILL EXISTING MANHOLE FOR CONDUIT INSTALLATION. SEAL ALL PENETRATIONS WATERTIGHT WITH NON-SHRINK GROUT.
 12. INSTALL BASE MOUNTED L-861T LED MEDIUM INTENSITY TAXIWAY EDGE LIGHTS (MITL) BLUE - TYPICAL FOR ALL TAXIWAY LIGHTS. SEE SHEET 31 FOR DETAIL.
 13. REMOVED SIGNS SHALL BE RELOCATED AS DIRECTED BY THE ENGINEER. ALL REMAINING SIGNS TO BE REMOVED SHALL BE PROTECTED AND PLACED IN LOCATION DESIGNATED BY AIRPORT MAINTENANCE.

14. REMOVED LIGHTS AND BASE CANS SHALL BE RELOCATED AS DIRECTED BY THE ENGINEER. ALL REMAINING LIGHTS AND BASE CANS TO BE REMOVED SHALL BE PROTECTED AND PLACED IN LOCATION DESIGNATED BY AIRPORT MAINTENANCE.
15. ALL TAXIWAY LIGHTS SHALL BE LOCATED 10 FEET FROM EDGE OF PAVEMENT TO CENTER.

LEGEND

TAXIWAY B CIRCUIT	
TAXIWAY C NORTH CIRCUIT	
TAXIWAY C SOUTH CIRCUIT	
EXISTING CIRCUIT AS NOTED	
RUNWAY 2-20 CIRCUIT	
RUNWAY 11-29 CIRCUIT	
PROPOSED GUIDANCE SIGN NUMBER	
PROPOSED EDGE LIGHT ON NEW BASE	
GUIDANCE SIGN	

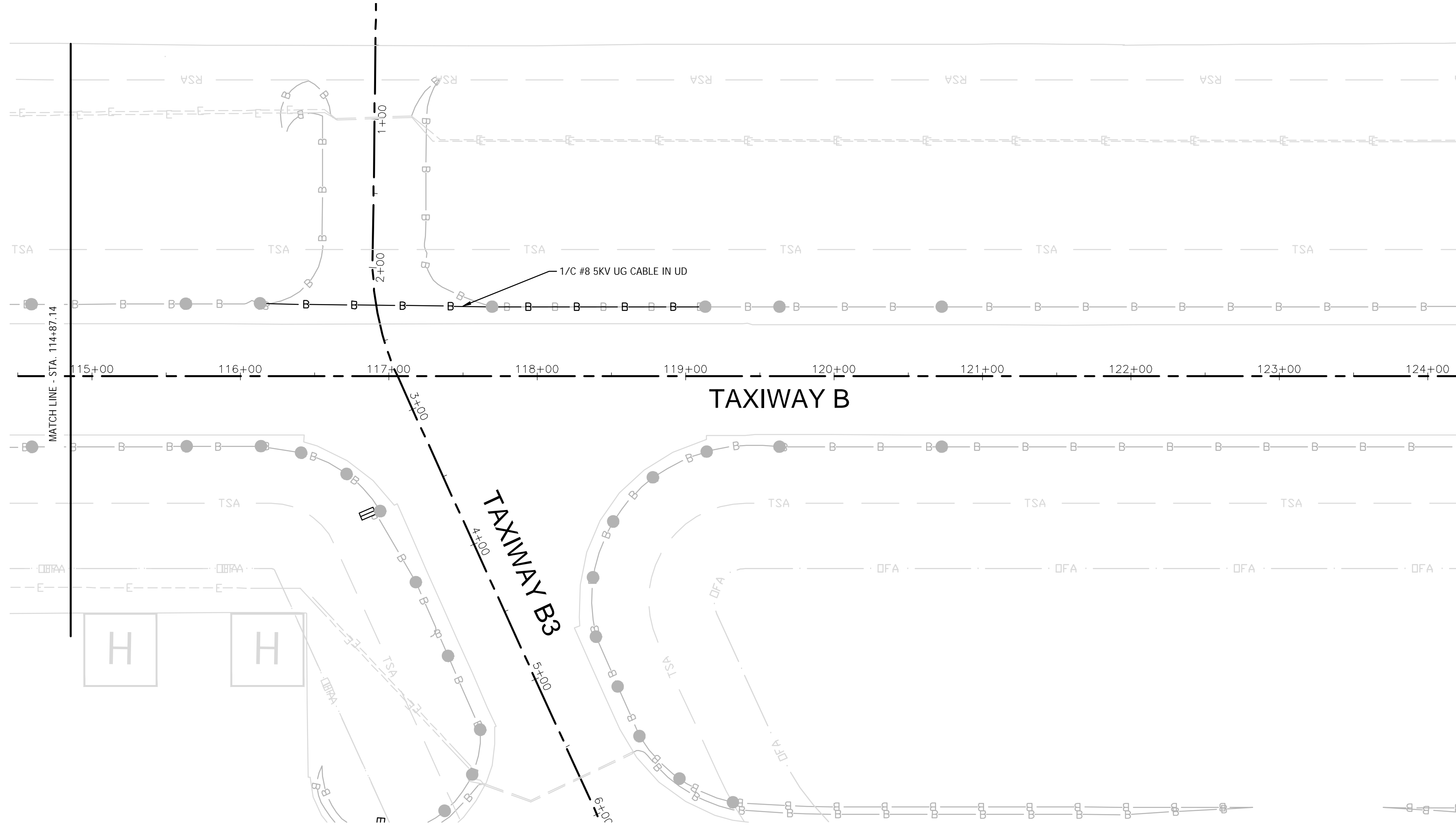
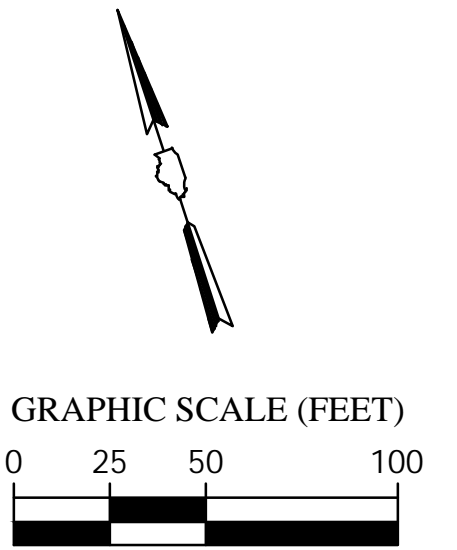
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ELECTRICAL PLAN	
TAXIWAY ALPHA & BRAVO REHABILITATION	
VETERANS AIRPORT OF SOUTHERN ILLINOIS	
Date	3/1/2024
DR. By	BLW
CK'D By	MKW
Scale	1" = 50'
Job No.	19204CZ
Sheet No.	25 of 30

P:\Airport\19204 Taxiway A, B, D Reconstruct\Drawings - Plans\29-30 ELECTRICAL PLAN.dwg 3-01-24 10:36:04 AM blwhite

MATCH LINE - STA. 114+87.14

LETTING CONTRACT NUMBER

WI061



PLAN NOTES

- 1 CONTRACTOR SHALL HAND DIG TO LOCATE EXISTING UNDERGROUND CABLE.
- 2 EXISTING UNDERGROUND CABLE FEEDING RUNWAY 11-29 EDGE LIGHTING IN THESE AREAS. LOCATE ALL SUCH CABLE AND HAND EXCAVATE WHEREVER CROSSING IS NECESSARY FOR PROPOSED CABLE INSTALLATION.
- 3 LOCATE AND EXPOSE ENDS OF EXISTING DUCT FOR ADDITION OF PROPOSED TAXIWAY B CABLE. HAND-EXCAVATE AS NECESSARY TO AVOID DAMAGE TO EXISTING RUNWAY 11-29 CABLE. DISCONNECT AND REMOVE EXISTING CABLES TO BE REPLACED FOR TAXIWAY B CIRCUIT.
- 4 EXISTING PAPI/REIL 29 UNDERGROUND CABLE TO REMAIN.
- 5 PROPOSED CONCRETE FOUNDATION AND L867 BASE FOR THIS TAXI GUIDANCE SIGN ENTIRELY. INSTALL PER DETAIL, SHEET 31.
- 6 SERIES-CONNECT PROPOSED FEEDS TO SIGN TO THE EXISTING RUNWAY 2-20 CIRCUIT AT DISTANCE REMAINING SIGN SHOWN. PROVIDE 1/C, #6, 5 KV, TYPE C CABLE IN UNIT DUCT. OFFSET CABLE ROUTING AS REQUIRED TO AVOID EXISTING UNDERGROUND CABLES TO REMAIN. REMOVE EXISTING TAXIWAY CABLES FROM THE SIGN BASE.
- 7 1/C, #8, 5 KV, TYPE C CABLE IN UNIT DUCT - TYPICAL FOR ALL TAXIWAY LIGHTING CABLE UNLESS NOTED OTHERWISE. OFFSET CABLE ROUTING AS REQUIRED TO AVOID EXISTING UNDERGROUND CABLES TO REMAIN.
- 8 REMOVE AND REPLACE EXISTING CABLE FROM EXISTING DUCT BANK AS FOLLOWS:
TAXIWAY B - 1 - #8, 5 KV, TYPE C
- 9 1/C, #6, 5 KV, TYPE C CABLE IN UNIT DUCT.
- 10 SERIES-CONNECT PROPOSED FEEDS TO SIGN TO THE EXISTING RUNWAY 2-20 CIRCUIT AT RUNWAY EDGE LIGHT SHOWN. PROVIDE 1/C, #6, 5 KV, TYPE C CABLE IN UNIT DUCT. OFFSET CABLE ROUTING AS REQUIRED TO AVOID EXISTING UNDERGROUND CABLES TO REMAIN.
- 11 CORE DRILL EXISTING MANHOLE FOR CONDUIT INSTALLATION. SEAL ALL PENETRATIONS WATERTIGHT WITH NON-SHRINK GROUT.
- 12 INSTALL BASE MOUNTED L-861T LED MEDIUM INTENSITY TAXIWAY EDGE LIGHTS (MITL) BLUE - TYPICAL FOR ALL TAXIWAY LIGHTS. SEE SHEET 31 FOR DETAIL.
- 13 REMOVED SIGNS SHALL BE RELOCATED AS DIRECTED BY THE ENGINEER. ALL REMAINING SIGNS TO BE REMOVED SHALL BE PROTECTED AND PLACED IN LOCATION DESIGNATED BY AIRPORT MAINTENANCE.

- 14 REMOVED LIGHTS AND BASE CANS SHALL BE RELOCATED AS DIRECTED BY THE ENGINEER. ALL REMAINING LIGHTS AND BASE CANS TO BE REMOVED SHALL BE PROTECTED AND PLACED IN LOCATION DESIGNATED BY AIRPORT MAINTENANCE.
- 15 ALL TAXIWAY EDGE LIGHTS SHALL BE LOCATED 10 FEET FROM EDGE OF PAVEMENT TO CENTER.

LEGEND

TAXIWAY B CIRCUIT	— B —
TAXIWAY C NORTH CIRCUIT	— CN —
TAXIWAY C SOUTH CIRCUIT	— CS —
EXISTING CIRCUIT AS NOTED	— B —
RUNWAY 2-20 CIRCUIT	— R2 —
RUNWAY 11-29 CIRCUIT	— R11 —
GUIDANCE SIGN TO BE REMOVED OR PANEL CHANGE	##
PROPOSED EDGE LIGHT ON NEW BASE	⊙
GUIDANCE SIGN	≡

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ELECTRICAL PLAN TAXIWAY ALPHA & BRAVO REHABILITATION VETERANS AIRPORT OF SOUTHERN ILLINOIS	
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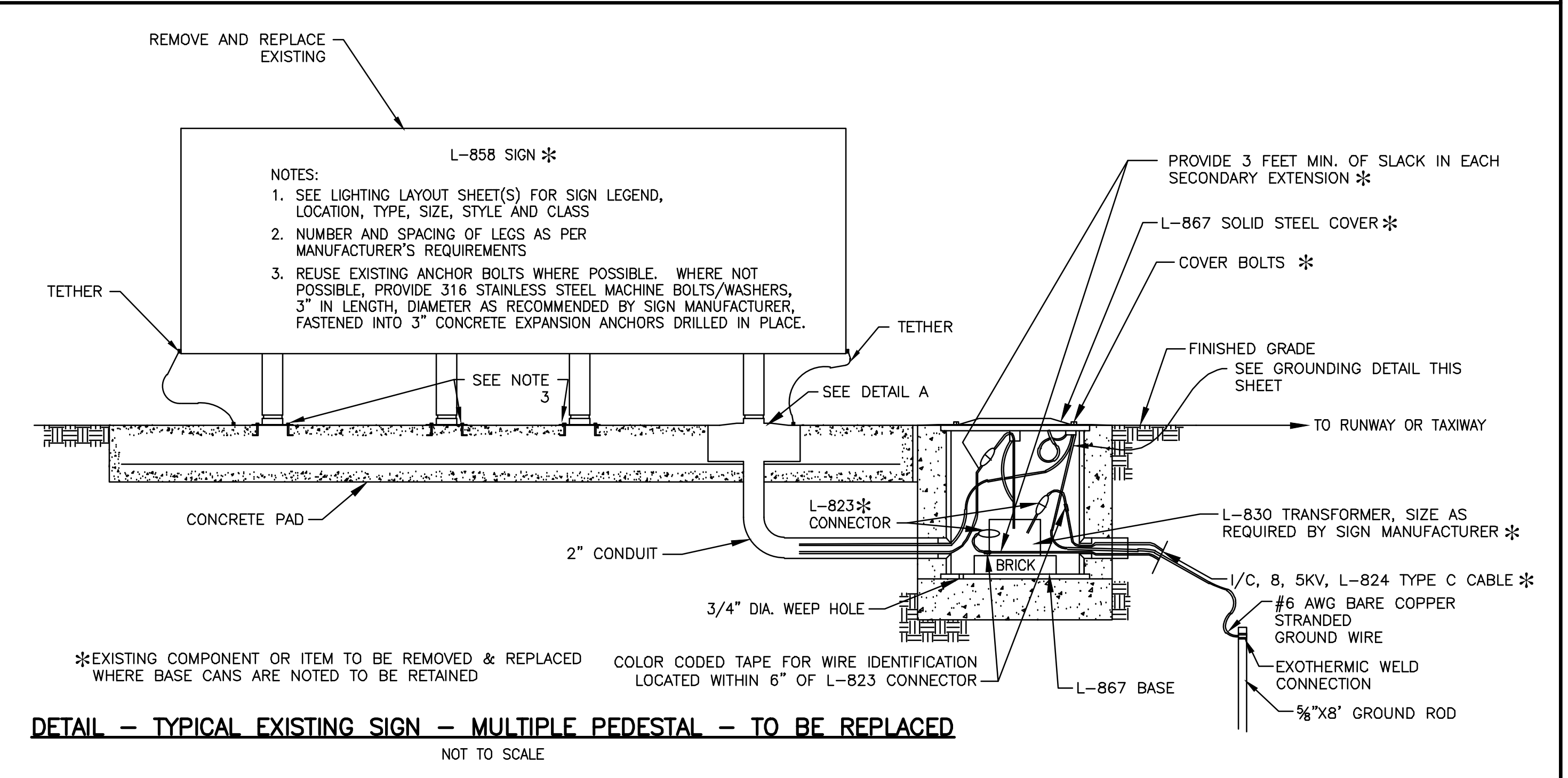
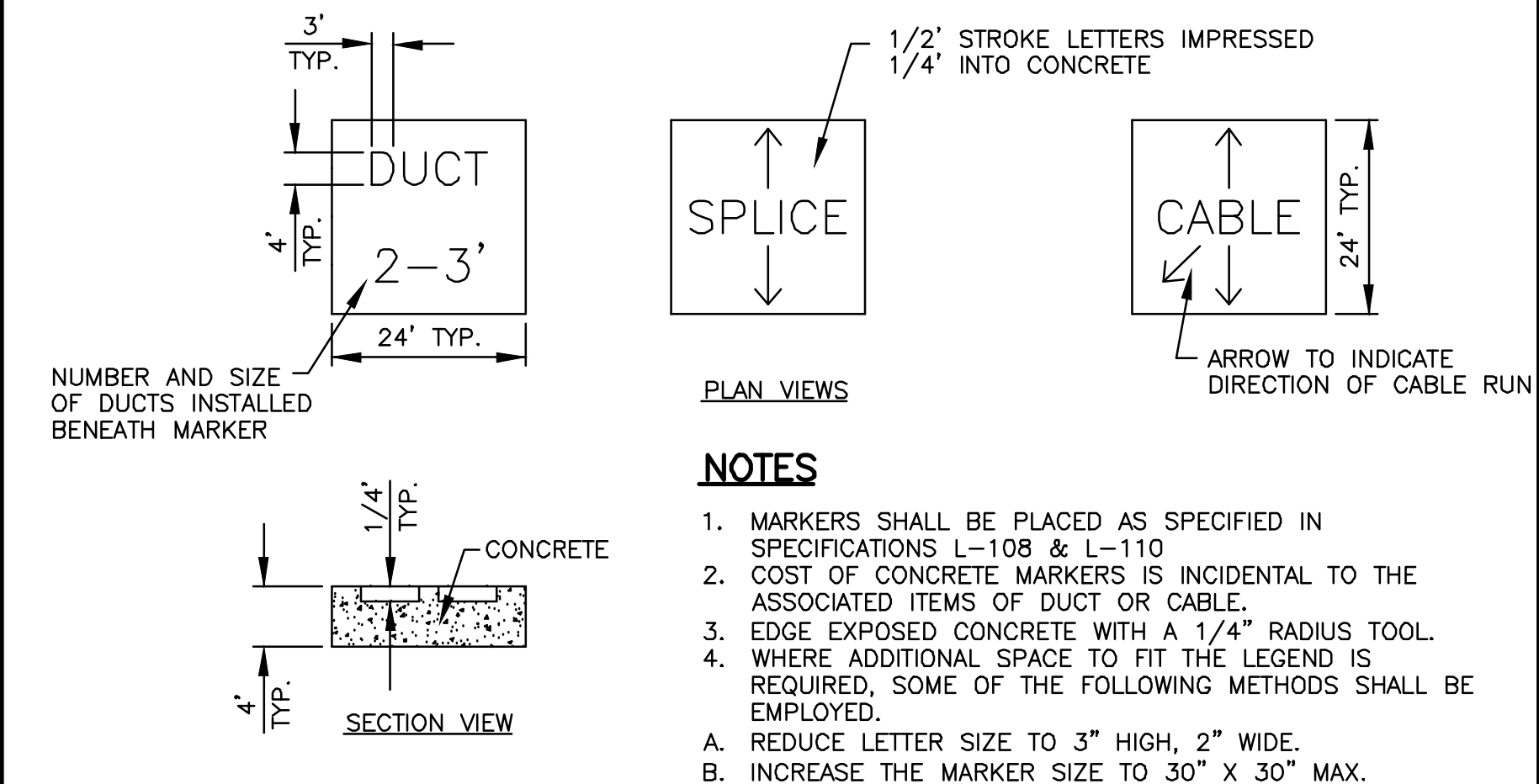
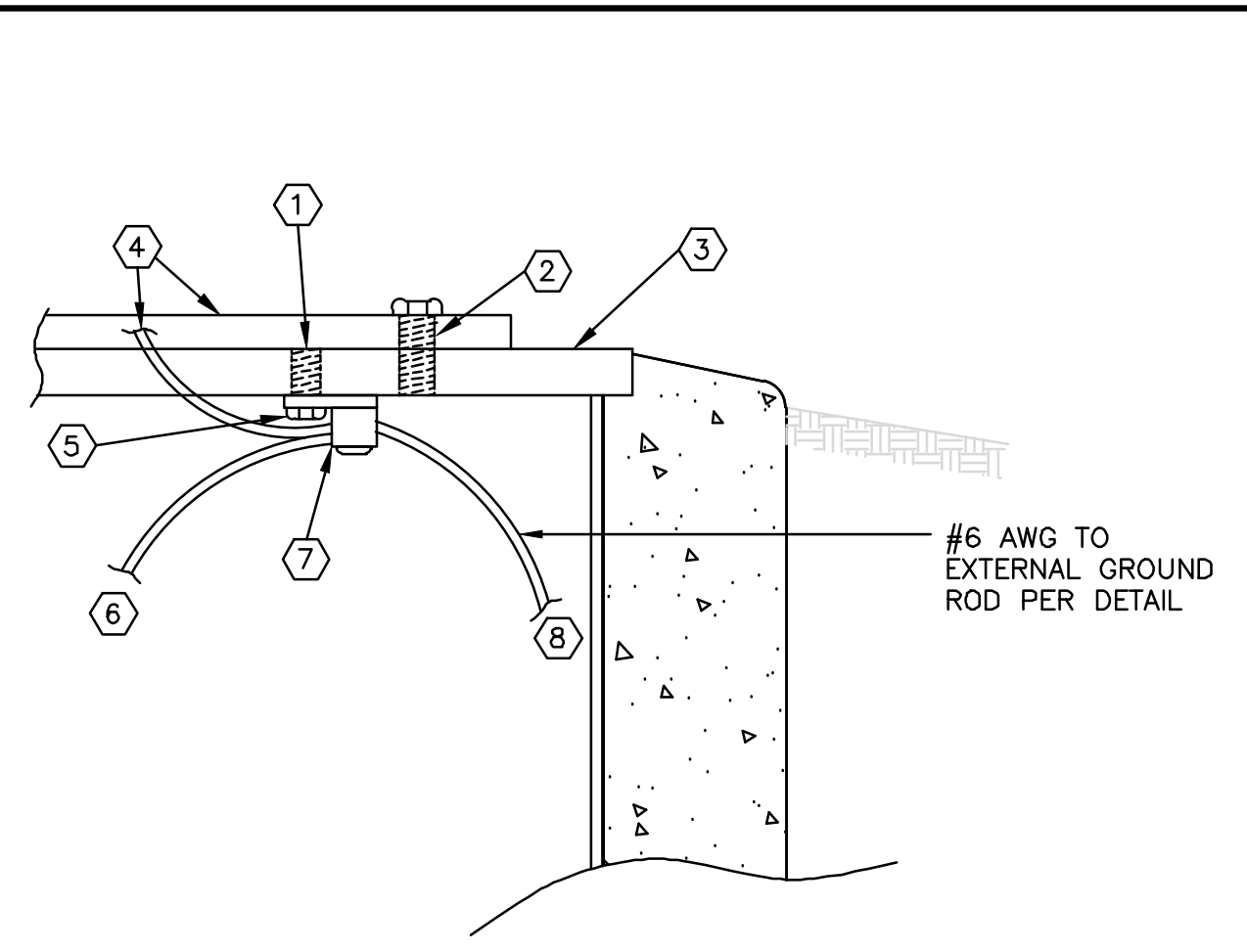
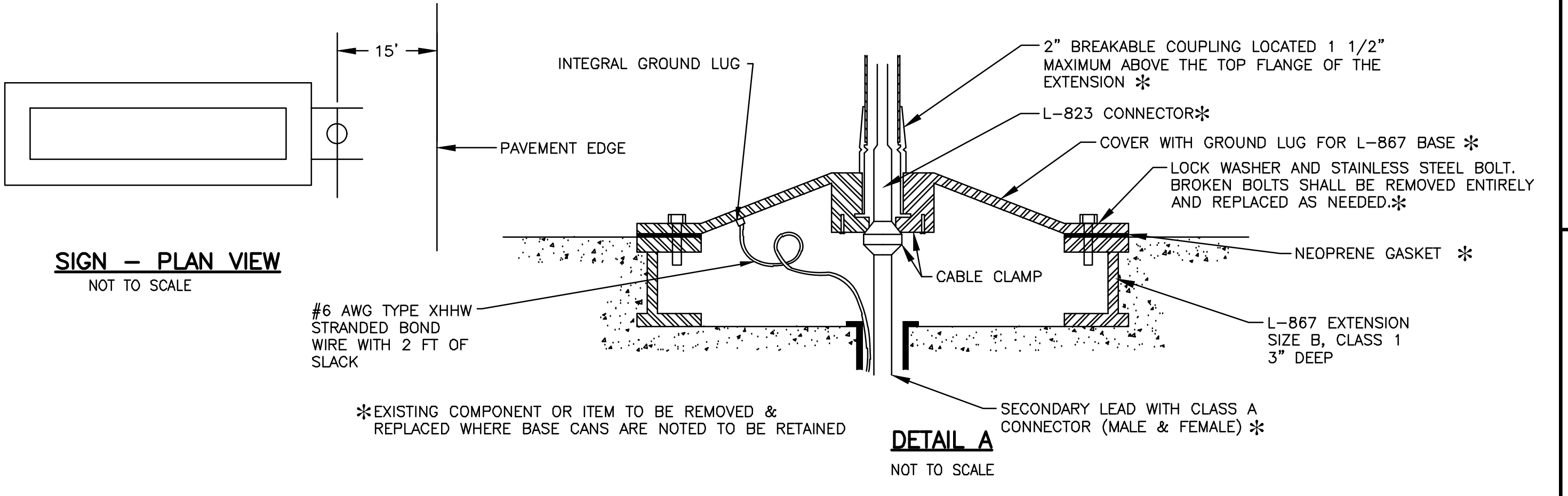
GENERAL ELECTRICAL NOTES

LETTING CONTRACT NUMBER
W1061

1. THE ELECTRICAL INSTALLATION, AS A MINIMUM, SHALL MEET THE NATIONAL ELECTRICAL CODE AND LOCAL REGULATIONS.
2. COLOR CODE ALL PHASE WIRING BY THE USE OF COLORED WIRE INSULATION AND/OR COLORED TAPE. WHERE TAPE IS USED, THE WIRE INSULATION SHALL BE BLACK. BLACK AND RED SHALL BE USED FOR SINGLE PHASE, THREE WIRE SYSTEMS, AND BLACK, RED AND BLUE SHALL BE USED FOR THREE PHASE SYSTEMS. NEUTRAL CONDUCTORS SIZE NO. 6 AWG OR SMALLER SHALL BE IDENTIFIED BY A CONTINUOUS WHITE OR NATURAL GRAY OUTER FINISH ALONG ITS ENTIRE LENGTH. NEUTRAL CONDUCTORS SIZE LARGER THAN NO. 6 SHALL BE IDENTIFIED EITHER BY A CONTINUOUS WHITE OR NATURAL GRAY OUTER FINISH ALONG ITS ENTIRE LENGTH OR BY THE USE OF WHITE TAPE AT ITS TERMINATIONS AND INSIDE ACCESSIBLE ENCLOSURES.
3. ALL BRANCH CIRCUIT CONDUCTORS CONNECTED TO A PARTICULAR PHASE SHALL BE IDENTIFIED WITH THE SAME COLOR. THE COLOR CODING SHALL BE EXTENDED TO THE POINT OF UTILIZATION.
4. NEATLY LACE WIRING IN DISTRIBUTION PANELS, SWITCHES AND JUNCTION/PULL BOXES.
5. GROUND ALL NONCURRENT-CARRYING METAL PARTS OF ELECTRICAL EQUIPMENT BY USING INSULATED COPPER WIRE TO BE RUN INSIDE CABINETS AND IN CONDUITS TOGETHER WITH OTHER WIRES.
6. ALL GROUND CONNECTIONS TO BUSSES, PANELS, ETC., SHALL BE MADE WITH PRESSURE TYPE SOLDERLESS LUG CLAMPS. SOLDERED OR BOLT & WASHER TYPE CONNECTIONS ARE NOT ACCEPTABLE. CLEAN ALL METAL SURFACES BEFORE MAKING GROUND CONNECTIONS. CONNECTIONS TO GROUND RODS SHALL BE MADE WITH EXOTHERMIC WELDING PROCESS.
7. SCHEDULE 40 RIGID STEEL CONDUIT SHALL BE USED THROUGHOUT THE INSTALLATION UNLESS OTHERWISE SPECIFIED.
8. ALL STEEL CONDUITS, FITTINGS, NUTS, BOLTS, ETC., SHALL BE GALVANIZED.
9. USE INSULATED CONDUIT BUSHINGS AT EACH CONDUIT TERMINATION.
10. USE DOUBLE LOCK NUTS AT EACH CONDUIT TERMINATION.
11. UNLESS OTHERWISE SHOWN, ALL EXPOSED CONDUITS SHALL BE RUN PARALLEL TO OR AT RIGHT ANGLES WITH THE LINES OF THE STRUCTURE.
12. UNLESS OTHERWISE NOTED, ALL SINGLE CONTROL CONDUCTORS SHALL BE NO. 12 AWG, THHN, STRANDED COPPER. EXTENSIONS TO EXISTING CONTROL CONDUCTORS SHALL BE THE SAME COLOR AS EXISTING.
13. BOTH ENDS OF EACH CONTROL CONDUCTOR SHALL BE TERMINATED AT A TERMINAL BLOCK. THE TERMINAL

14. BOTH ENDS OF ALL CONTROL CONDUCTORS SHALL BE IDENTIFIED AS TO THE CIRCUIT, TERMINAL BLOCK, AND TERMINAL NUMBER. ONLY SHRINKABLE PERMANENT LABELS SHALL BE USED.
15. A SEPARATE AND CONTINUOUS NEUTRAL CONDUCTOR SHALL BE INSTALLED AND CONNECTED FOR EACH CIRCUIT IN THE POWER PANEL(S) FROM THE NEUTRAL BAR TO EACH POWER/CONTROL CIRCUIT.
16. SPLICES AND JUNCTION POINTS SHALL BE PERMITTED ONLY IN JUNCTION BOXES, LIGHT BASES, WIREWAYS EQUIPPED WITH REMOVABLE COVERS AND AT EASILY ACCESSIBLE LOCATIONS.
17. UNLESS OTHERWISE NOTED, ALL UNDERGROUND FIELD POWER MULTIPLE AND SERIES CIRCUIT CONDUCTORS SHALL BE FAA APPROVED L-824, TYPE, INSULATION, VOLTAGE AND SIZE SHALL BE AS SPECIFIED.
18. THE JOINT OF THE PRIMARY L-823 PRIMARY CONNECTORS SHALL BE WRAPPED WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF THE JOINT. HEAT-SHRINK TUBING SHALL BE APPLIED WHERE CABLE ENTERS BACK OF CONNECTOR, SEE DETAIL DWG., SHEET 31.
19. THE ID OF THE PRIMARY L-823 FIELD ATTACHED CONNECTORS SHALL MATCH THE CABLE OD TO PROVIDE A WATERTIGHT CABLE ENTRANCE.
20. THE RESISTANCE TO GROUND OF THE GROUNDING SYSTEM WITH COMMERCIAL POWER LINE NEUTRAL DISCONNECTED SHALL NOT EXCEED 25 OHMS.
21. ALL POWER AND CONTROL CIRCUIT CONDUCTORS SHALL BE COPPER, ALUMINUM SHALL NOT BE ACCEPTED. THIS INCLUDES WIRE, CABLE, BUSSES, TERMINALS, SWITCH/PANEL COMPONENTS, ETC.
22. CABLE/SPLICE/DUCT MARKERS SHALL BE PRECAST CONCRETE OF SIZE SHOWN. LETTERS/NUMBERS FOR THE LEGEND TO BE IMPRESSED INTO TOPS OF THE MARKERS SHALL BE PREASSEMBLED AND SECURED IN MOLD BEFORE CONCRETE IS POURED. LEGEND INSCRIBED BY HAND IN WET CONCRETE SHALL NOT BE ACCEPTABLE.
23. THE CONTRACTOR SHALL ASCERTAIN THAT ALL LIGHTING SYSTEM COMPONENTS FURNISHED BY HIM (INCLUDING FAA APPROVED EQUIPMENT) ARE COMPATIBLE IN ALL RESPECTS WITH EACH OTHER AND REMAINDER OF THE NEW/EXISTING SYSTEM. ANY NONCOMPATIBLE COMPONENTS FURNISHED BY THIS CONTRACTOR SHALL BE REPLACED BY HIM AT NO ADDITIONAL COST TO THE AIRPORT SPONSOR WITH A SIMILAR UNIT, APPROVED BY THE ENGINEER (DIFFERENT MODEL OF DIFFERENT MANUFACTURER) THAT IS COMPATIBLE WITH THE REMAINDER OF THE AIRPORT LIGHTING SYSTEM.

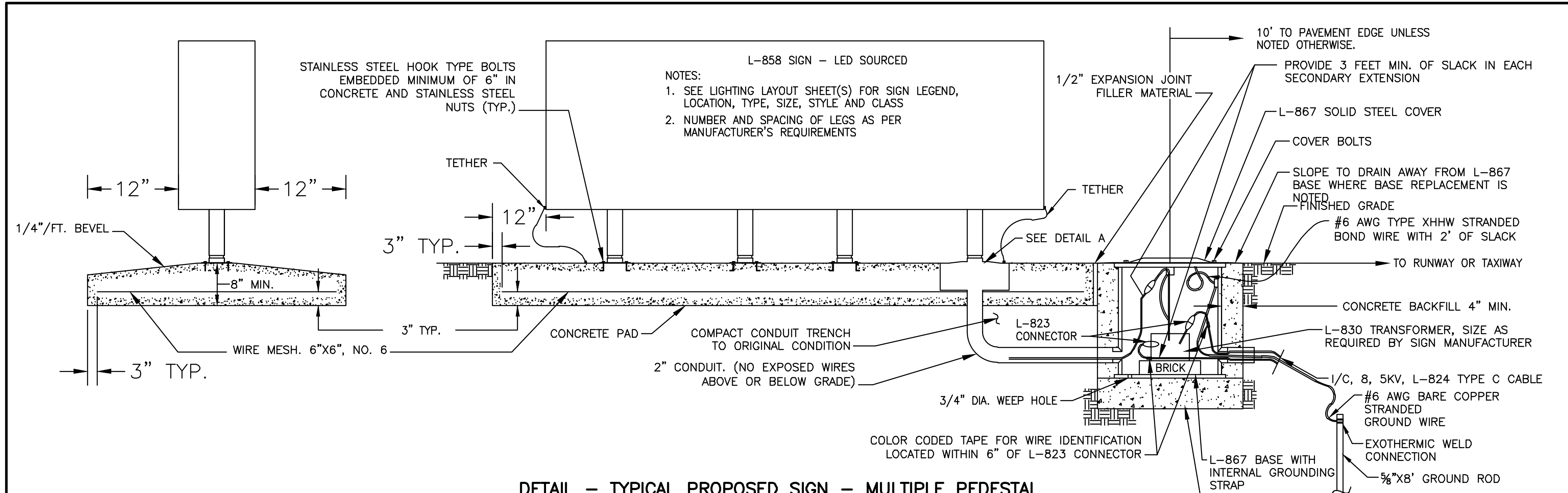
24. IN CASE THE CONTRACTOR SELECTS TO FURNISH AND INSTALL AIRPORT LIGHTING EQUIPMENT REQUIRING ADDITIONAL WIRING, TRANSFORMERS, ADAPTERS, MOUNTINGS, ETC., DIFFERENT FROM THOSE SHOWN ON THE DRAWINGS AND/OR LISTED IN THE SPECIFICATIONS, ANY COST FOR THESE ITEMS SHALL BE INCIDENTAL TO THE EQUIPMENT COST.
25. THE CONTRACTOR INSTALLED EQUIPMENT (INCLUDING FAA APPROVED) SHALL NOT GENERATE ANY ELECTRO-MAGNETIC INTERFERENCE IN THE EXISTING AND/OR NEW COMMUNICATIONS, WEATHER, AND AIR TRAFFIC CONTROL EQUIPMENT. ANY EQUIPMENT GENERATING SUCH INTERFERENCE SHALL BE REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST BY EQUIPMENT MEETING THE APPLICABLE SPECIFICATIONS AND NOT GENERATING ANY INTERFERENCE.
26. UNLESS NOTED OTHERWISE FOR IN PAVEMENT LIGHTS, WHERE EXISTING FIELD LIGHTS ARE TO BE REMOVED, THE AREA SHALL BE BACKFILLED WITH EARTH TO THE ORIGINAL GRADE, COMPACTED AND SEEDED. SUCH LIGHTS AND CONCRETE BASES SHALL BECOME CONTRACTOR SALVAGE AND SHALL BE REMOVED FROM THE PREMISES ENTIRELY.
27. CONTRACTOR SHALL LOCATE EXISTING UNDERGROUND CIRCUITS WITH A PORTABLE CABLE LOCATOR WHERE POSSIBLE TO AVOID DAMAGE TO EXISTING CIRCUITS TO BE RETAINED. EXCAVATION REQUIRED IN CONGESTED AREAS CONTAINING OTHER CIRCUITS SHALL BE DONE BY HAND. ANY SUCH WIRING DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED IMMEDIATELY AFTER DISCOVERY AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. ALL UNDERGROUND SPLICES SHALL BE INSPECTED BY THE ENGINEER PRIOR TO BACKFILLING TRENCHES.
28. REMOVAL OF EXISTING UNDERGROUND BURIED CABLE SHALL BE LIMITED ONLY TO THOSE AREAS WHERE A CABLE LOCATOR DOES NOT INDICATE EXISTING CIRCUITS TO BE RETAINED EITHER CROSSING OR IN THE SAME TRENCH. SUCH CIRCUITS INCLUDE THE CEILING LIGHT, FAA COMMUNICATION AND MONITORING CIRCUITS, RUNWAY 11 PCAL/REIL, RUNWAY 29 PCAL/REIL, MID-FIELD WINDCONE, RUNWAY 11-29 EDGE LIGHTING, RUNWAY 2-20 EDGE LIGHTING, AND RUNWAY 2-20 REIL.
29. SHOP DRAWINGS SHALL BE REQUIRED FOR THE FOLLOWING ITEMS: CONDUIT, WIRE & CABLE, CABLE IN UNIT DUCT, FIELD LIGHTS, LAMPS, SPLICE CANS, FIELD LIGHT MODIFICATIONS, CABLE CONNECTORS, PULL BOXES, REGULATORS, PCAL SYSTEM, PCAL CONTROL PANEL, PRECAST CONCRETE MANHOLES, TAXI GUIDANCE SIGNS.
30. A MINIMUM OF EIGHT HOURS SHALL BE PROVIDED FOR TRAINING AIRPORT MAINTENANCE PERSONNEL ON THE PROPOSED AIRFIELD LIGHTING SYSTEMS.



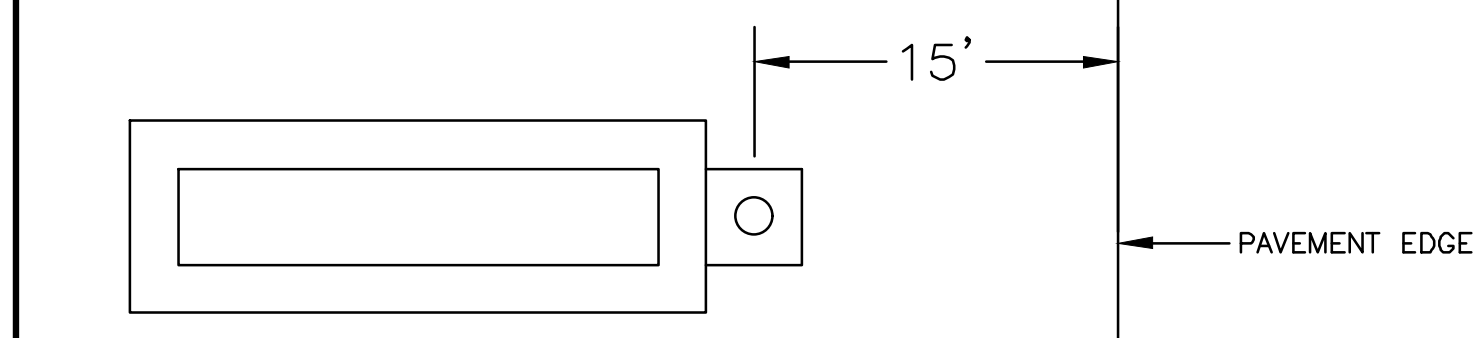
- DETAIL NOTES**
1. DRILL AND TAP A 1/2" X 20 THREADED HOLE IN THE BASE LIP FOR GROUND SCREW.
 2. EXISTING THREADED HOLE FOR COVER BOLT.
 3. EXISTING L-867 BASE EMBEDDED IN CONCRETE FOUNDATION TO BE RETAINED.
 4. #6 GROUND WIRE WITH 2 FT OF SLACK TO PROPOSED BASE BLANK COVER PLATE GROUND LUG.
 5. STAINLESS STEEL 1/2" X 20 HEX-HEAD BOLT AND WASHER. CUT OFF ANY EXCESS BOLT ABOVE THE LIP, FLUSH WITH TOP OF BASE.
 6. #6 GROUND WIRE BONDED TO PROPOSED SIGN EXTENSION COVER PER DETAIL A.
 7. 125 AMP RATED COPPER SOLDERLESS LUG BOLTED TO UNDERSIDE OF BASE LIP.
 8. #6 GROUND WIRE BONDED TO PROPOSED EXTERNAL GROUND ROD PER DETAIL.

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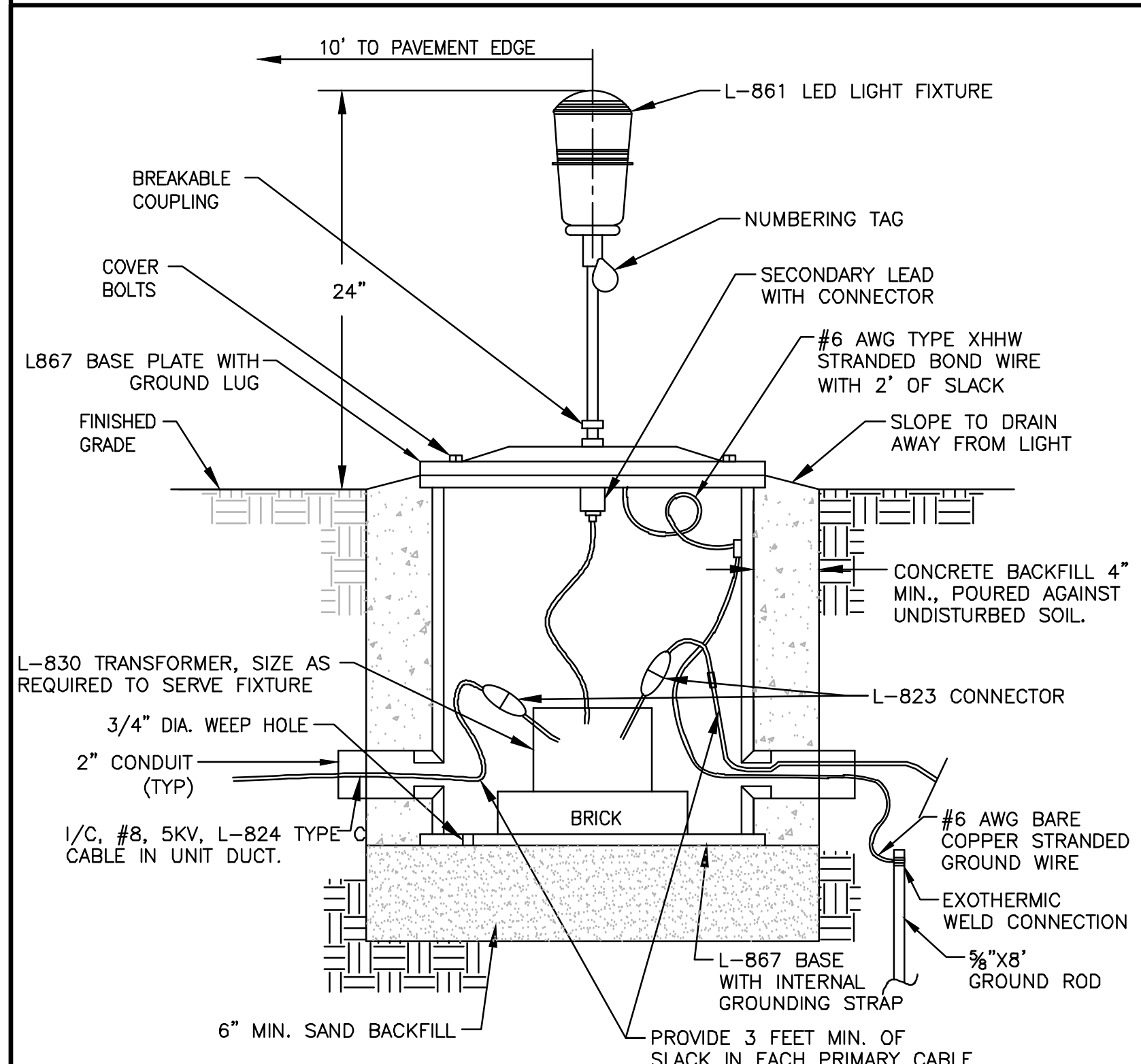
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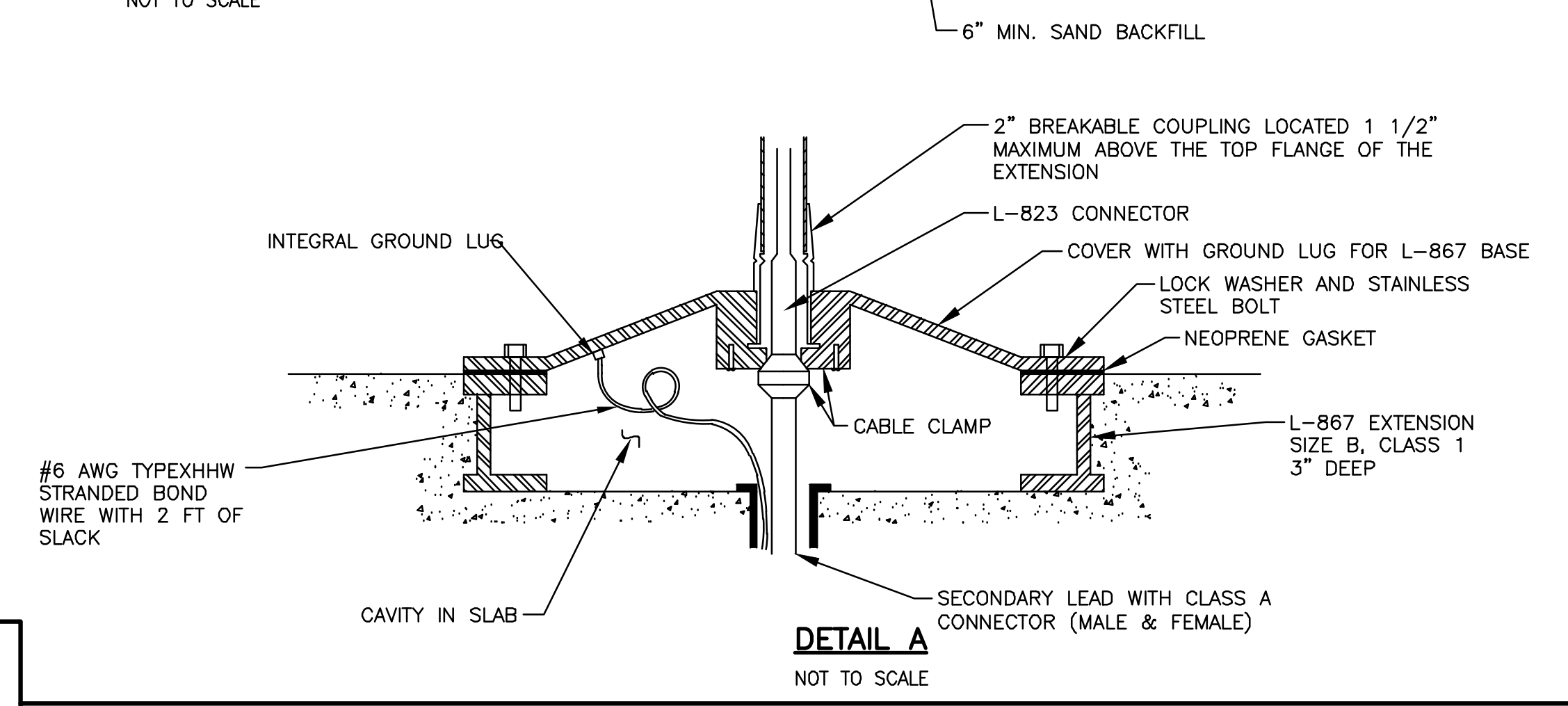
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SIGN - PLAN VIEW
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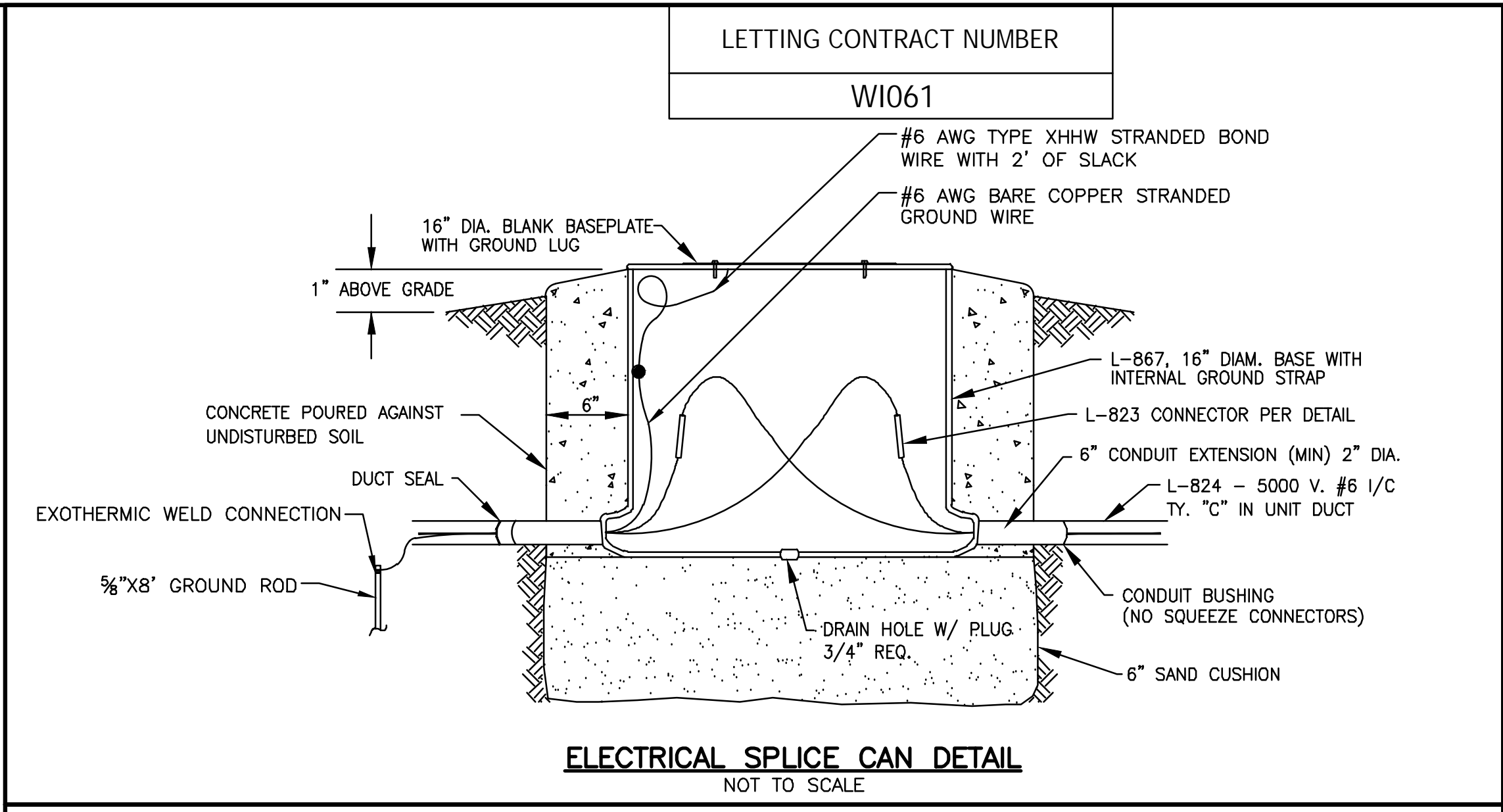
MEDIUM INTENSITY LIGHT - BASE MOUNTED
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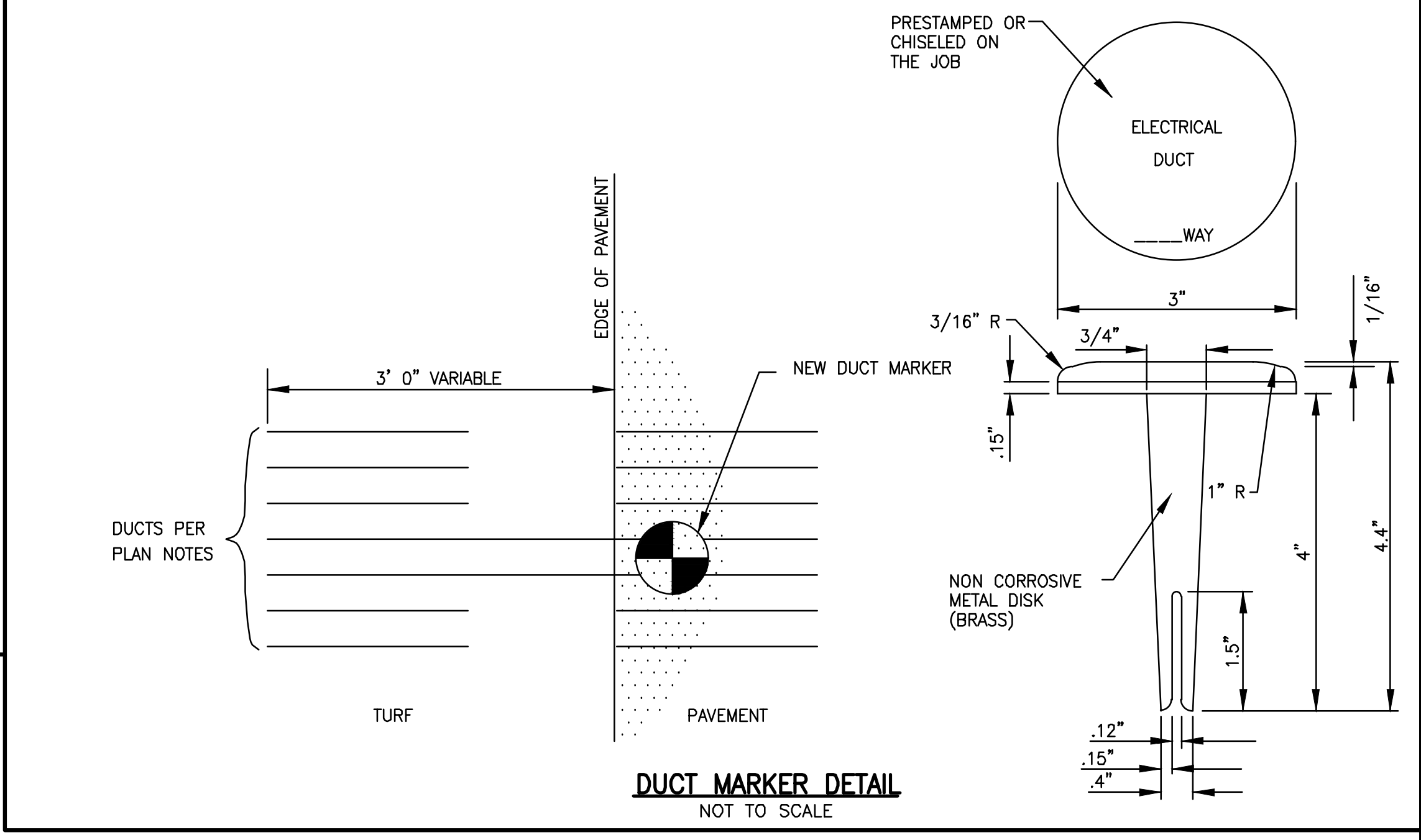
DETAIL A
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CABLE SPlicing NOTES:

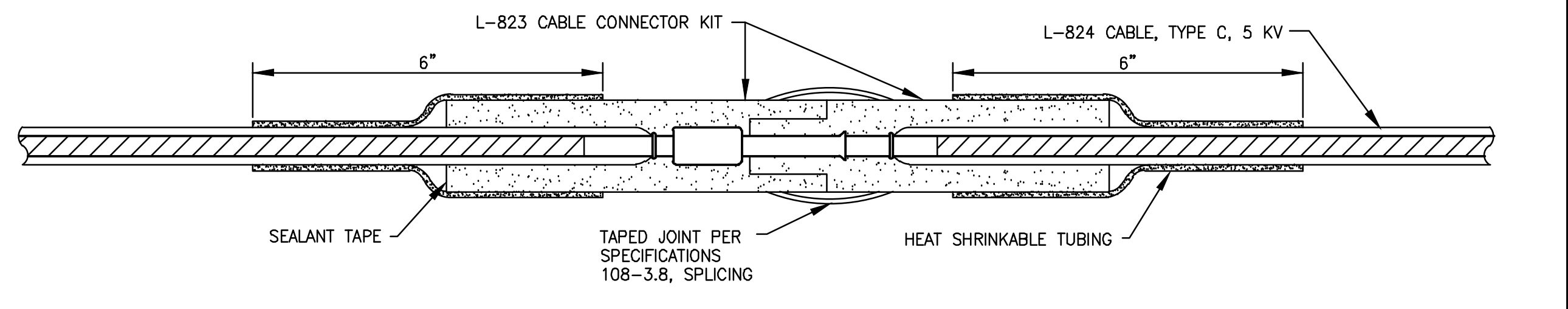
- HEAT-SHRINKABLE TUBING WILL NOT BE REQUIRED FOR ISOLATION TRANSFORMER PRIMARY CONNECTORS WHICH ARE MOLDED ON TO THE CABLE LEADS AT THE FACTORY.
- THE SEALANT TAPE AROUND THE CONNECTOR SHALL BE WATER INSOLUBLE, MAINTAINING ELASTICITY OVER WIDE RANGE OF TEMPERATURE, AND SHALL BE RAYCHEM NO. 5-1011 OR APPROVED EQUAL.
- HEAT-SHRINKABLE TUBING SHALL HAVE MINIMUM EXPANDED I.D. OF 1.200", MAXIMUM RECOVERED I.D. OF .300", MINIMUM EXPANDED WALL THICKNESS OF .04", NOMINAL RECOVERED WALL THICKNESS OF 0.17", A MASTIC WATER SEALANT COATING APPLIED ON THE INSIDE, AND SHALL BE RAYCHEM NO. WCS-300-6-S OR APPROVED EQUAL.
- MINIMUM LENGTH OF THE HEAT-SHRINKABLE TUBING SHALL BE 6".
- CLEAN THE CABLE INSULATION BEFORE APPLYING THE TUBING WITH A SOLVENT SPECIFIED BY THE TUBING MANUFACTURER.
- TO HEAT THE TUBING, USE PROPANE TORCH, OR ELECTRIC HEATER RECOMMENDED BY THE TUBING MANUFACTURER.
- BEGIN HEATING THE TUBING AT THE CENTER, GO COMPLETELY AROUND, THEN MOVE TOWARD THE ENDS.
- CONTINUE HEATING THE TUBING UNTIL IT SHRINKS COMPLETELY AND THE SEALANT IS BEING SQUEEZED OUT AT BOTH ENDS.
- IF THERE IS ANY NOTICEABLE HEAT DAMAGE TO THE CABLE OR THE TUBING, THE CONNECTION, INCLUDING THE DAMAGED PORTION, WILL BE REMOVED AND ANOTHER CONNECTION MADE.
- ALLOW THE CONNECTION TO COOL BEFORE HANDLING.
- INSTALL THE CONNECTIONS WITHOUT BENDING THEM.



ELECTRICAL SPLICE CAN DETAIL
NOT TO SCALE



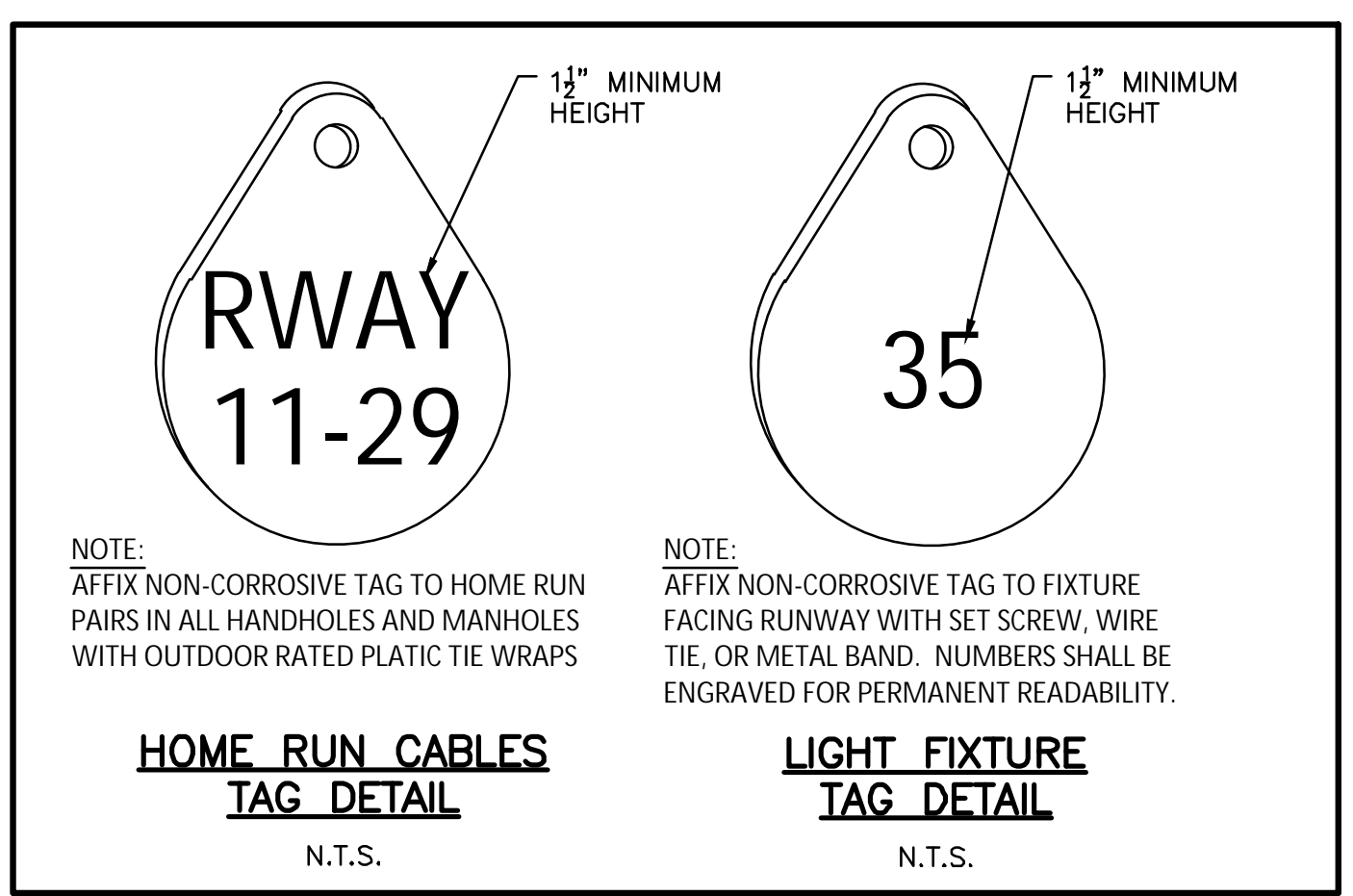
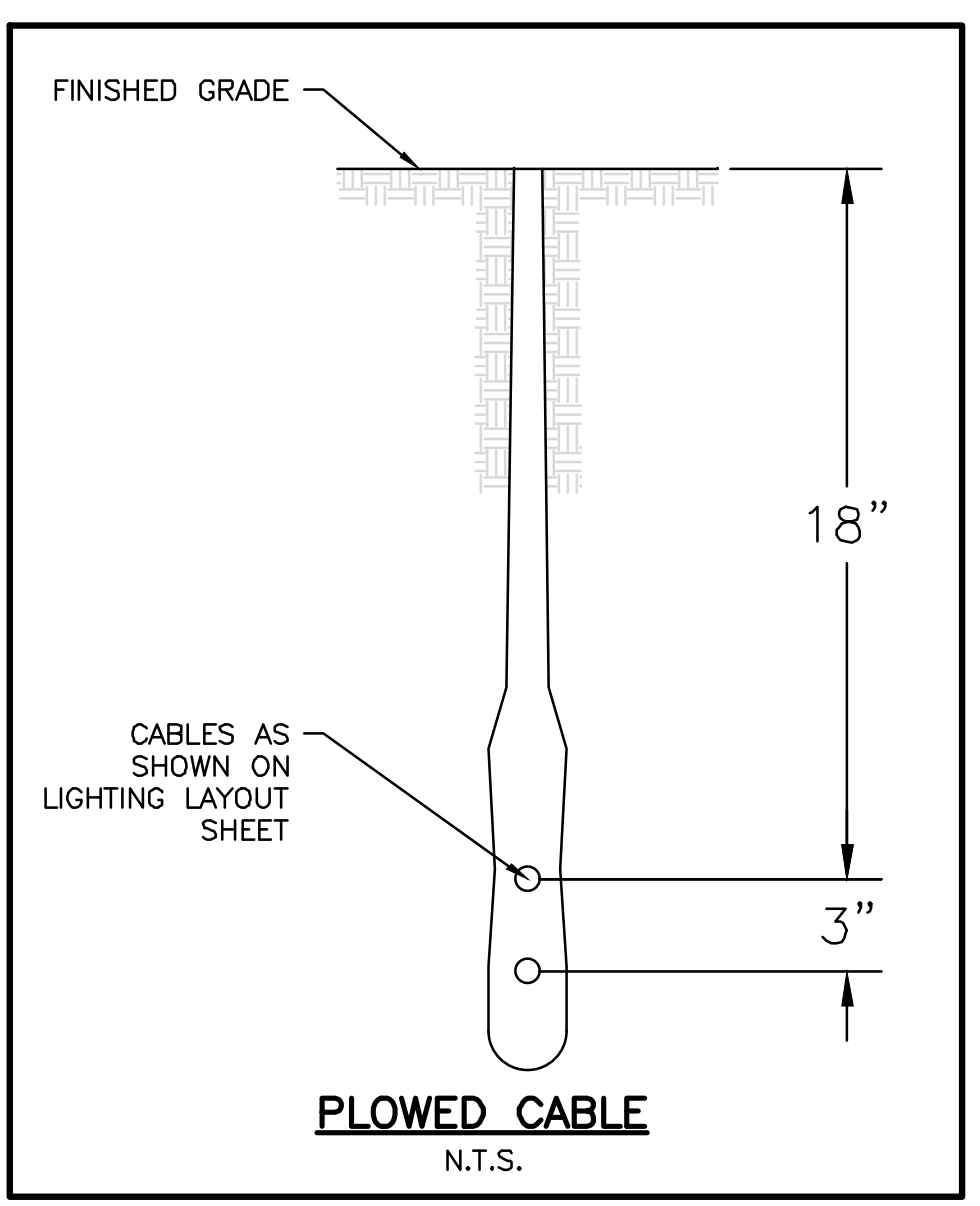
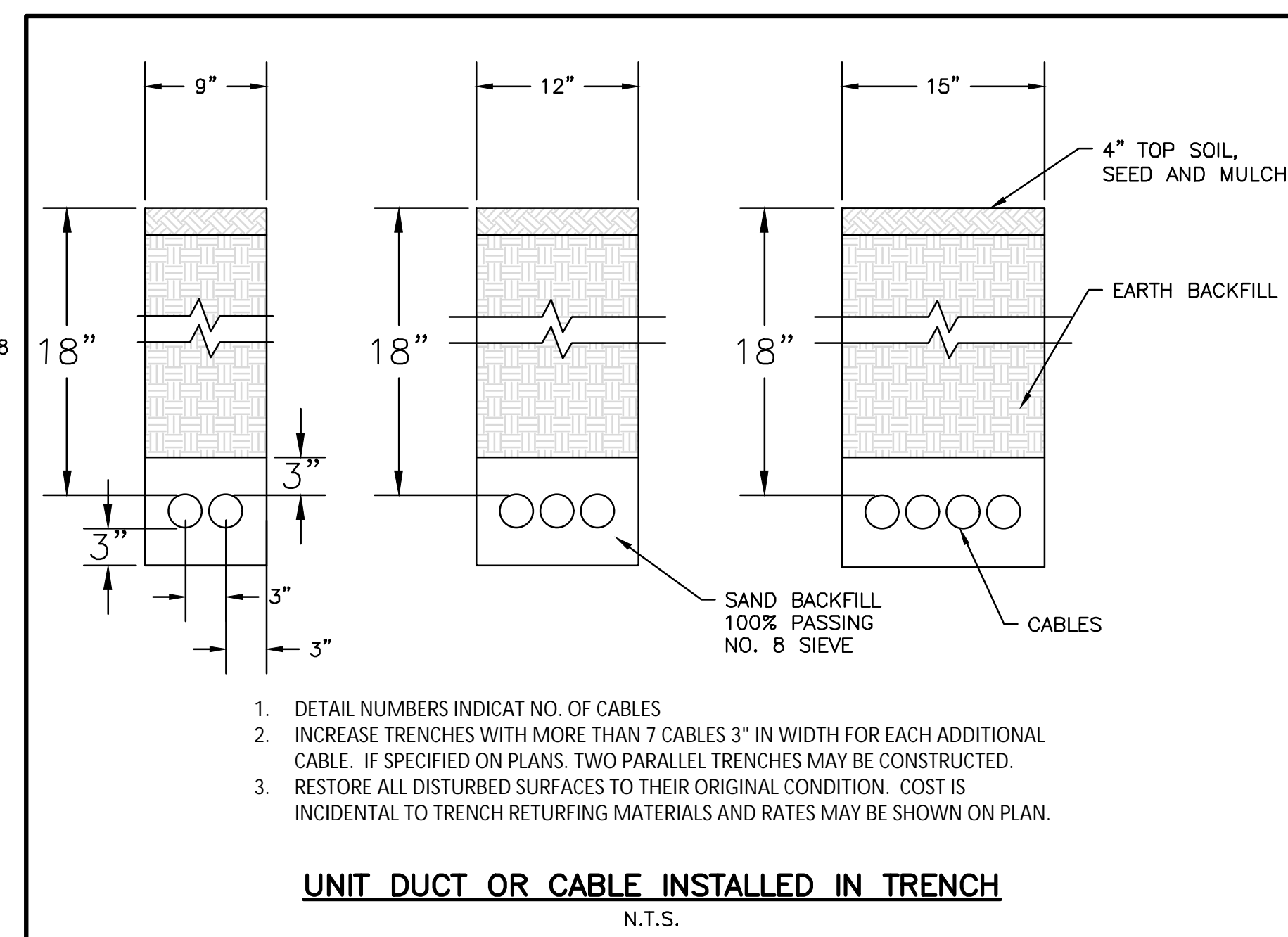
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DETAIL - CABLE SPlicing
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Job No.	19204CZ
Sheet No.	29 of 30

GENERAL EROSION NOTES

- A. BEST MANAGEMENT PRACTICES (BMP'S) AND CONTROLS SHALL CONFORM TO FEDERAL, STATE, OR LOCAL REQUIREMENTS OR MANUAL OF PRACTICE AS APPLICABLE. CONTRACTOR SHALL IMPLEMENT ADDITIONAL CONTROLS AS DIRECTED BY PERMITTING AGENCY OR OWNER.
- B. CONTRACTOR SHALL MINIMIZE CLEARING TO THE MAXIMUM EXTENT PRACTICAL OR AS REQUIRED BY THE GENERAL PERMIT.
- C. GENERAL CONTRACTOR SHALL DENOTE ON PLAN THE TEMPORARY PARKING AND STORAGE AREA WHICH SHALL ALSO BE USED AS THE EQUIPMENT MAINTENANCE AND CLEANING AREA, EMPLOYEE PARKING AREA, AND AREA FOR LOCATING PORTABLE FACILITIES, OFFICE TRAILERS, AND TOILET FACILITIES.
- D. ALL WASH WATER (CONCRETE TRUCKS, VEHICLE CLEANING, EQUIPMENT CLEANING, ETC.) SHALL BE DETAINED AND PROPERLY TREATED OR DISPOSED.
- E. SUFFICIENT OIL AND GREASE ABSORBING MATERIALS AND FLOTATION BOOMS SHALL BE MAINTAINED ON SITE OR READILY AVAILABLE TO CONTAIN AND CLEAN-UP FUEL OR CHEMICAL SPILLS AND LEAKS.
- J. DUST ON THE SITE SHALL NOT BE CONTROLLED BY THE USE OF MOTOR OILS AND OTHER PETROLEUM BASED OR TOXIC LIQUIDS FOR DUST SUPPRESSION OPERATIONS.
- K. RUBBISH, TRASH, GARBAGE, LITTER, OR OTHER SUCH MATERIALS SHALL BE DEPOSITED INTO SEALED CONTAINERS. MATERIALS SHALL BE PREVENTED FROM LEAVING THE PREMISES THROUGH THE ACTION OF WIND OR STORMWATER DISCHARGE INTO DRAINAGE DITCHES OR WATERS OF THE STATE.
- L. ALL STORM WATER POLLUTION PREVENTION MEASURES PRESENTED ON THIS PLAN SHALL BE INITIATED AS SOON AS PRACTICABLE.
- M. DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITY HAS PERMANENTLY STOPPED SHALL BE PERMANENTLY SEEDED. THESE AREAS SHALL BE SEEDED NO LATER THAN 14 DAYS AFTER THE LAST CONSTRUCTION ACTIVITY OCCURRING IN THESE AREAS. REFER TO THE GRADING PLAN AND/OR LANDSCAPE PLAN.
- N. IF THE ACTION OF VEHICLES TRAVELING OVER THE GRAVEL CONSTRUCTION ENTRANCES IS NOT SUFFICIENT TO REMOVE THE MAJORITY OF DIRT OR MUD, THEN THE TIRES MUST BE WASHED BEFORE THE VEHICLES ENTER A PUBLIC ROAD. IF WASHING IS USED, PROVISIONS MUST BE MADE TO INTERCEPT THE WASH WATER AND TRAP THE SEDIMENT BEFORE IT IS CARRIED OFF THE SITE.
- O. ALL MATERIALS SPILLED, DROPPED, WASHED, OR TRACKED FROM VEHICLES ONTO ROADWAYS OR INTO STORM DRAINS MUST BE REMOVED IMMEDIATELY.
- P. ON-SITE & OFFSITE SOIL STOCKPILE AND BORROW AREAS SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION THROUGH IMPLEMENTATION OF BEST MANAGEMENT PRACTICES. STOCKPILE AND BORROW AREA LOCATIONS SHALL BE NOTED ON THE SITE MAP AND PERMITTED IN ACCORDANCE WITH GENERAL PERMIT REQUIREMENTS.
- Q. SLOPES SHALL BE LEFT IN A ROUGHENED CONDITION DURING THE GRADING PHASE TO REDUCE RUNOFF VELOCITIES AND EROSION.
- R. DUE TO THE GRADE CHANGES DURING THE DEVELOPMENT OF THE PROJECT, THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING THE EROSION CONTROL MEASURES (SEDIMENT LOGS, ETC.) TO PREVENT EROSION.
- S. ALL CONSTRUCTION SHALL BE STABILIZED AT THE END OF EACH WORKING DAY, THIS INCLUDES BACKFILLING OF TRENCHES FOR UTILITY CONSTRUCTION AND PLACEMENT OF GRAVEL OR BITUMINOUS PAVING FOR ROAD CONSTRUCTION.

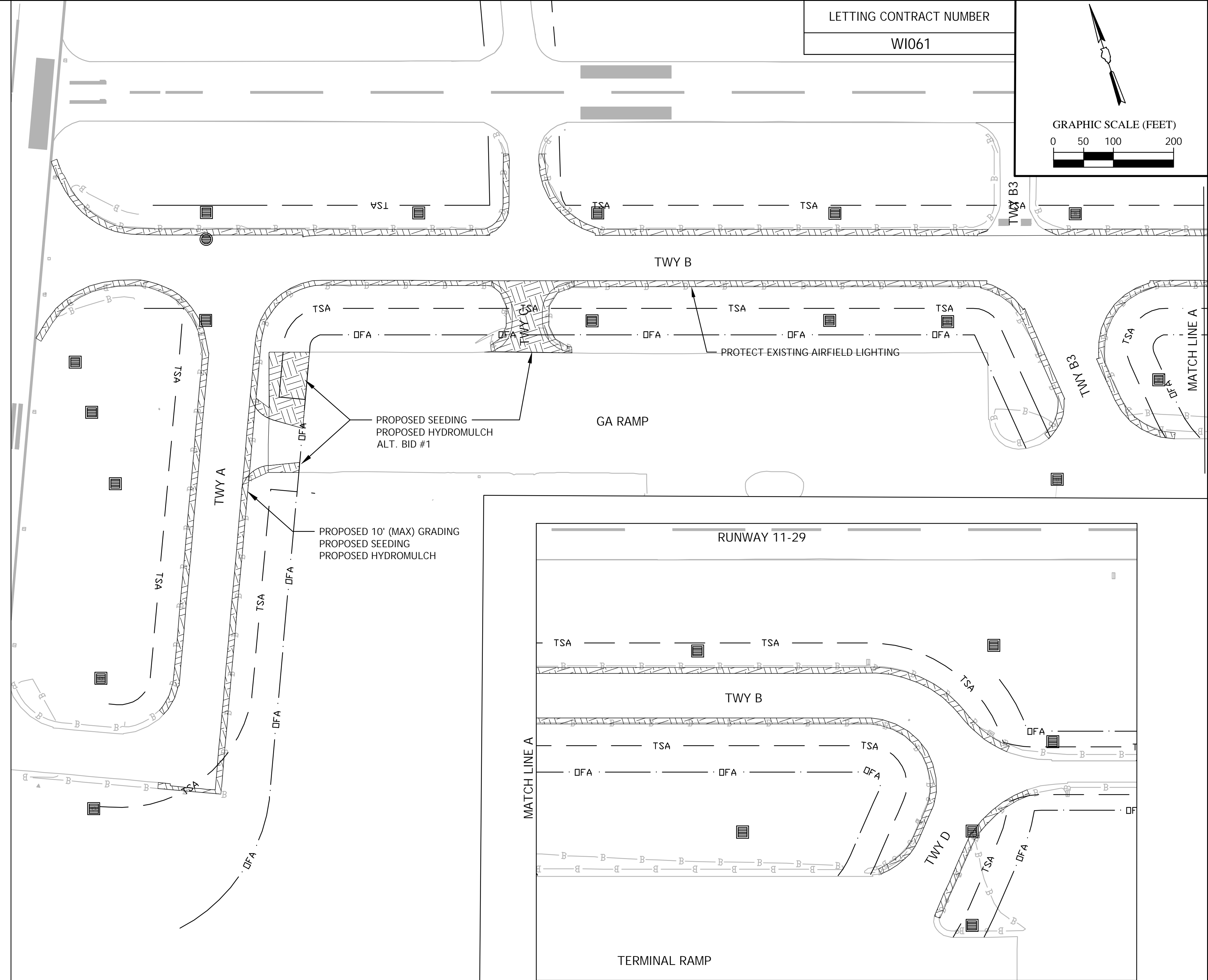
MAINTENANCE

ALL MEASURES STATED ON THIS EROSION AND SEDIMENT CONTROL PLAN SHALL BE MAINTAINED IN FULLY FUNCTIONAL CONDITION UNTIL NO LONGER REQUIRED FOR A COMPLETED PHASE OF WORK OR FINAL STABILIZATION OF THE SITE. ALL EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE CHECKED BY A QUALIFIED PERSON AT LEAST ONCE EVERY SEVEN CALENDAR DAYS AND WITHIN 24 HOURS OF THE END OF A 0.5" RAINFALL EVENT, AND CLEANED AND REPAIRED IN ACCORDANCE WITH THE FOLLOWING:

1. INLET PROTECTION DEVICES AND BARRIERS SHALL BE REPAIRED OR REPLACED IF THEY SHOW SIGNS OF UNDERMINING, OR DETERIORATION.
2. ALL SEEDED AREAS SHALL BE CHECKED REGULARLY TO SEE THAT A GOOD STAND IS MAINTAINED. AREAS SHOULD BE FERTILIZED, WATERED, AND RESEED AS NEEDED.
3. THE CONSTRUCTION ENTRANCES SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOW OF MUD ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING OF THE CONSTRUCTION ENTRANCES AS CONDITIONS DEMAND.
4. THE TEMPORARY PARKING AND STORAGE AREA SHALL BE KEPT IN GOOD CONDITION (SUITABLE FOR PARKING AND STORAGE). THIS MAY REQUIRE PERIODIC TOP DRESSING OF THE TEMPORARY PARKING AS CONDITIONS DEMAND.
5. OUTLET STRUCTURES IN THE SEDIMENTATION BASINS SHALL BE MAINTAINED IN OPERATIONAL CONDITIONS AT ALL TIMES. SEDIMENT SHALL BE REMOVED FROM SEDIMENT BASINS OR TRAPS WHEN THE DESIGN CAPACITY HAS BEEN REDUCED BY 50%.

LETTING CONTRACT NUMBER
W1061

GRAPHIC SCALE (FEET)
0 50 100 200



LEGEND

SHOULDER ADJUSTMENT	
TAXIWAY EDGE LIGHT	
TAXIWAY EDGE LIGHT ELECTRICAL	
GUIDANCE SIGN	

STATE OF ILLINOIS PROFESSIONAL DESIGN FIRM LICENSE NO. 184-004515	
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GRADING AND EROSION CONTROL PLAN TAXIWAY ALPHA & BRAVO REHABILITATION VETERANS AIRPORT OF SOUTHERN ILLINOIS	
Date	3/1/2024
DR. By	BLW
CK'D By	MKW
Scale	1" = 100'
Job No.	19204CZ
Sheet No.	30 of 30