

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

FAP ROUTE 345 – U.S. ROUTE 20 (LAKE STREET) OVER McLEAN BOULEVARD
SECTION 8R-R

PROJECT: ACNHF-0345(056)
INTERCHANGE RECONSTRUCTION AND BRIDGE REPLACEMENT

KANE COUNTY

C-91-632-09

VOLUME I OF II

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	BR-R	KANE	794	1
ILLINOIS			CONTRACT NO. 60H45	

* 794 + 4 = 798

D-91-632-09



LOCATION OF SECTION INDICATED THUS: - **█** -

FOR INDEX OF SHEETS, SEE SHEET NO. 2

US ROUTE 20
DESIGN DESIGNATION - 56,200(12) ARTERIAL 34.29 (CRC-20)
ADT = 42,700 (2001) - 66,000 (2020)
DESIGN SPEED = 60 MPH
POSTED SPEED = 55 MPH

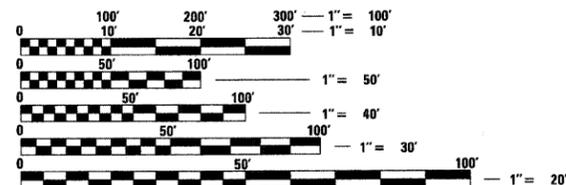
LILLIAN STREET
ADT = 12,700 (2001) - 16,500 (2020)
DESIGN SPEED = 35 MPH
POSTED SPEED = 30 MPH

FLEETWOOD DRIVE
ADT = 3,400 (2001) - 5,000 (2020)
DESIGN SPEED = 35 MPH
POSTED SPEED = 30 MPH

MCLEAN BOULEVARD (NORTH OF INTERCHANGE)
ADT = 30,000 (2001) - 41,000 (2020)
DESIGN SPEED = 35 MPH
POSTED SPEED = 30 MPH

MCLEAN BOULEVARD (SOUTH OF INTERCHANGE)
ADT = 26,000 (2001) - 44,000 (2020)
DESIGN SPEED = 35 MPH
POSTED SPEED = 35 MPH

PROJECT LOCATED IN CITY OF ELGIN

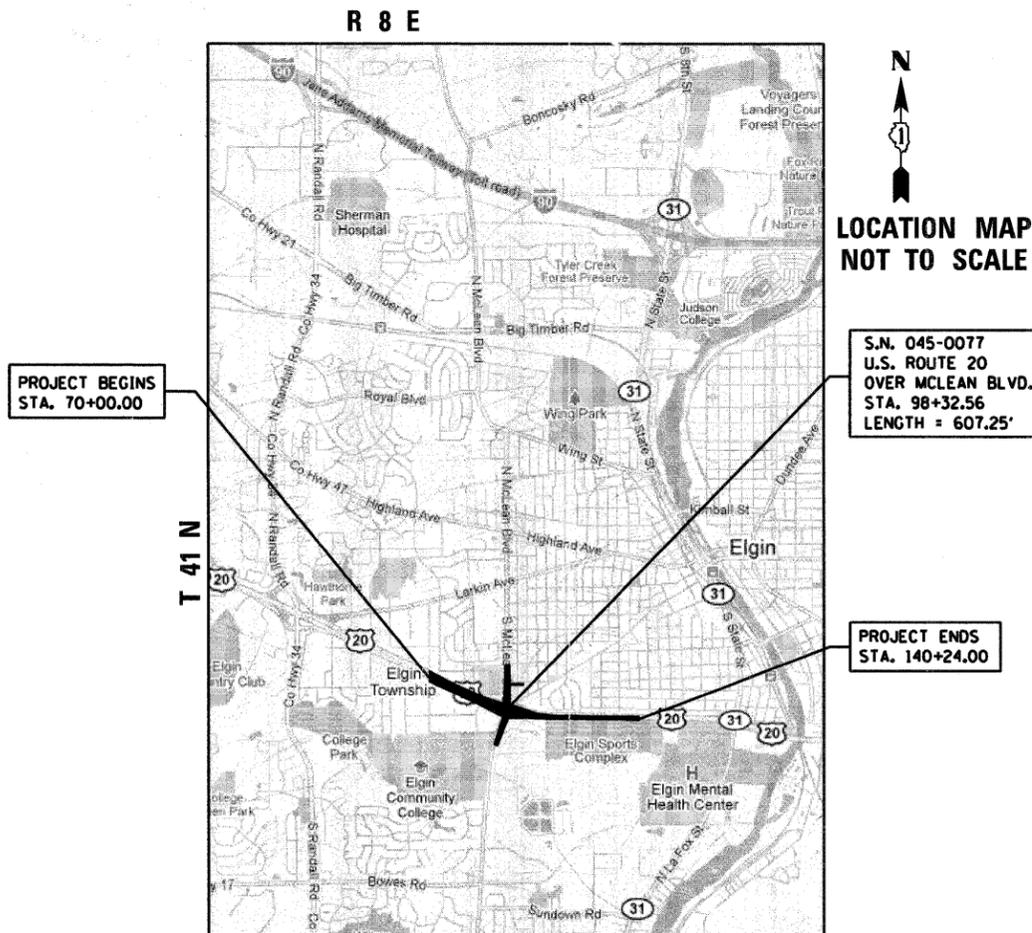


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT MANAGER BRIAN KUTTAB, P.E.
PROJECT ENGINEER RON ZENAROSA

CONTRACT NO. 60H45



GROSS LENGTH = 7,024 FT. = 1.330 MILES
NET LENGTH = 7,024 FT. = 1.330 MILES



DOUGLAS D. HANSEN, P.E.
IL. REG. NO. 062-45293
EXPIRES: 11/30/2013
DATE: 11/28/2011

TENG

TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
205 N. MICHIGAN AVE. CHICAGO, IL 60601
TELEPHONE: 312.616-0000

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED November 30, 2011

Diane M. O'Neil
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

March 23, 2012
John D. Baranzelli, P.E. /bc
acting ENGINEER OF DESIGN AND ENVIRONMENT

March 23, 2012
William R. Frey, Jr.
acting DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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OF THE STATE OF ILLINOIS**

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538 - 559	DISTRICT ONE DETAILS *
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*DISTRICT ONE TRAFFIC SIGNAL AND LIGHTING DETAILS ARE INCLUDED WITH THE TRAFFIC SIGNAL AND LIGHTING PLANS

**SEE SHEET 560 FOR INDEX OF CROSS SECTION SHEETS

LIST OF STATE STANDARDS

STD. NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
280001-06	TEMPORARY EROSION CONTROL SYSTEMS
420001-07	PAVEMENT JOINTS
420101-04	24' (7.2 m) JOINTED PCC PAVEMENT
420111-03	PCC PAVEMENT ROUNDOUTS
420206-08	ENTRANCE RAMP TERMINAL (JOINTED PCC RAMP PAVEMENT ADJACENT TO CRC MAINLINE PAVEMENT)
420306-06	EXIT RAMP TERMINAL (JOINTED PCC RAMP PAVEMENT ADJACENT TO CRC MAINLINE PAVEMENT)
420401-08	BRIDGE APPROACH PAVEMENT CONNECTOR
421001-02	BAR REINFORCEMENT FOR CRC PAVEMENT
421201-06	24' (7.2 m) CRC PAVEMENT (WITH LUG SYSTEM)
424001-06	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006	DIAGONAL CURB RAMPS FOR SIDEWALKS
424026	ENTRANCE / ALLEY PEDESTRIAN CROSSINGS
483001-04	PCC SHOULDER
515001-03	NAME PLATE FOR BRIDGES
542101-02	REINFORCED CONCRETE END SECTIONS FOR PIPE CULVERTS, 375 MM (15") THRU 900 MM (36") DIAMETER AT RIGHT ANGLES WITH ROADWAY
542201-02	REINFORCED CONCRETE END SECTIONS FOR PIPE CULVERTS, 375 MM (15") THRU 900 MM (36") DIAMETER SKEWED WITH ROADWAY
542301-03	PRECAST REINFORCED CONCRETE FLARED END SECTION
542311-03	GRATING FOR CONCRETE FLARED END SECTION (FOR 600 MM (24") THRU 1300 MM (54") PIPE)
601001-04	SUB-SURFACE DRAINS
601101-01	CONCRETE HEADWALL FOR PIPE DRAIN
602001-02	CATCH BASIN, TYPE A
602301-03	INLET, TYPE A
602306-03	INLET, TYPE B
602401-03	MANHOLE, TYPE A
602406-05	MANHOLE, TYPE A, 1.8 M (6') DIAMETER
602601-02	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-02	MANHOLE STEPS
604001-03	FRAME AND LIDS, TYPE 1
604036-02	GRATE, TYPE 8
604091-02	FRAME AND GRATE TYPE 24
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
606301-04	PC CONCRETE ISLANDS AND MEDIANS
630001-10	STEEL PLATE BEAM GUARDRAIL
630301-05	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
631011-08	TRAFFIC BARRIER TERMINAL, TYPE 2
631026-05	TRAFFIC BARRIER TERMINAL, TYPE 5
631031-10	TRAFFIC BARRIER TERMINAL, TYPE 6
635001-01	DELINEATORS
635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-02	REFLECTOR MARKER AND MOUNTING DETAILS
637001-04	CONCRETE BARRIER 815 MM (32 IN) HEIGHT
642001-02	SHOULDER RUMBLE STRIPS
664001-02	CHAIN LINK FENCE
701001-02	OFF-ROAD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5 m) AWAY
701006-03	OFF-ROAD OPERATIONS, 2L, 2W, 15' (4.5 m) TO 24' (600 mm) FROM PAVEMENT EDGE
701011-02	OFF-ROAD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701101-02	OFF-ROAD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24' (600 mm) FROM PAVEMENT EDGE
701106-02	OFF-ROAD OPERATIONS, MULTILANE, MORE THAN 15' (4.5 m) AWAY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701411-08	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS > OR = 45 MPH
701421-04	LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS > OR = 45 MPH TO 55 MPH
701426-04	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS > OR = 45 MPH TO 55 MPH
701602-05	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-08	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-08	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-02	TRAFFIC CONTROL DEVICES
704001-07	TEMPORARY CONCRETE BARRIER
720001-01	SIGN PANEL MOUNTING DETAILS
720006-03	SIGN PANEL ERECTION DETAILS
720021-02	SIGN PANELS, EXTRUDED ALUMINUM TYPE
728001-01	TELESCOPING STEEL SIGN SUPPORT
731001-01	BASE FOR TELESCOPING STEEL SIGN SUPPORT
780001-03	TYPICAL PAVEMENT MARKINGS
781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
782001	PRISMATIC CURB REFLECTORS

LIST OF STATE STANDARDS - cont.

STD. NO.	DESCRIPTION
814001-02	HANDHOLES
814006-02	DOUBLE HANDHOLES
857001-01	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
862001-01	UNINTERRUPTIBLE POWER SUPPLY (UPS)
873001-02	TRAFFIC SIGNAL GROUNDING & BONDING
877001-05	STEEL MAST ARM ASSEMBLY AND POLE 16' THROUGH 55'
877002-02	STEEL MAST ARM ASSEMBLY AND POLE 56' THROUGH 75'
877011-05	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 16' THROUGH 55'
878001-09	CONCRETE FOUNDATION DETAILS
880001-01	SPAN WIRE MOUNTED SIGNALS AND FLASHING BEACON INSTALLATION
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS
886001-01	DETECTOR LOOP INSTALLATIONS

LIST OF DISTRICT ONE DETAILS • INCLUDED AS PLAN SHEETS 538-559

STD. NO.	DESCRIPTION
BD01	DRIVEWAY DETAILS-DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER GREATER THAN OR EQUAL TO 15' (4.5m)
BD02	DRIVEWAY DETAILS-DISTANCE BETWEEN R.O.W. AND FACE OF CURB IS LESS THAN 15' (4.5m)
BD07	STORM SEWER CONNECTION TO EXISTING SEWER
BD08	FRAMES AND LIDS ADJUSTMENT WITH MILLING
BD22	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
BD32	BUTT JOINTS AND HMA TAPER
BD36	FIRE HYDRANT TO BE MOVED
BD48	PCC PAVEMENT ROUNDOUTS AT CURB AND GUTTER
BD49	DETAIL OF C.L. SAWCUT 16' (4.9 m) & VAR. JOINTED PCC PAV'T. FOR RAMPS
BD51	BENCHING DETAIL FOR EMBANKMENT WIDENING
BD52	DETAIL OF PAVEMENT SEPARATION JOINT FOR JOINTED PCC PAVEMENTS AT INTERSECTIONS
TC09	FREEWAY SINGLE & MULTI-LANE WEAVE
TC10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS
TC11	RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
TC13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC14	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
TC16	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
TC17	TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES
TC18	SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS
TC22	ARTERIAL ROAD INFORMATION SIGN
TC26	DRIVEWAY ENTRANCE SIGNING
TC27	MILE POST MARKERS-GORE SIGNS-MAJOR GUIDE SIGN LAYOUT-ARROWS

\FS-004\VA\VAULT\J-TRANS\07\2202\21379-000\CIVIL\CAD\0045\SHEET\062445-SHT-GENNOTED.DGN
 BAJZEKJ
 12-15-2011 14:16:03
 TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 CHICAGO, ILLINOIS

FILE NAME =	USER NAME = #USER#	DESIGNED - KDA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION FAP ROUTE 345 / US ROUTE 20	INDEX OF SHEETS AND STATE STANDARDS				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE#		DRAWN - JFS	REVISED -		345	BR-R	KANE	794	2				
PLOT SCALE = #SCALE#		CHECKED - DDH	REVISED -		CONTRACT NO. 60H45								
PLOT DATE = #DATE#		DATE - 12/16/11	REVISED -		SCALE: N.T.S.	SHEET NO.	OF	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOURS NOTIFICATION REQUIRED).
2. 10 FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS & GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
3. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH AFFECTED UTILITY COMPANIES AND MUNICIPALITIES.
4. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
5. WHEN ARTIFICIAL LIGHTING IS USED IN NIGHT OPERATIONS THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING COMMERCIAL AND RESIDENTIAL AREAS.
6. BARRIER WALL MARKERS, TYPE C SHALL BE INSTALLED ON MEDIAN CONCRETE BARRIER WALL AND PARAPETS AT A HEIGHT OF 28 INCHES FROM THE TOP OF SHOULDER TO THE BOTTOM OF THE REFLECTOR AND SPACED AT 50 FEET CENTER-TO-CENTER ON CURVES AND 100 FEET CENTER-TO-CENTER ON TANGENTS OR AS DIRECTED BY THE TRAFFIC OPERATIONS FIELD ENGINEER. THE BARRIER WALL MARKERS SHALL BE INSTALLED DURING STAGE I, PRIOR TO OPENING THE NEW PAVEMENT TO TRAFFIC.
7. ON STATE STANDARDS 482001 AND 483001 AGGREGATE SUBGRADE IMPROVEMENT 12" SHALL BE USED AS THE IMPROVED SUBGRADE. THE ADDITIONAL THICKNESS OF AGGREGATE SUBGRADE UNDER THE SHOULDER SHALL BE INCLUDED IN THE COST PER SQUARE YARD OF "AGGREGATE SUBGRADE IMPROVEMENT 12".
8. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS MINIMUM 1:3 (V:H).
9. A PAINT STRIPE SHALL BE APPLIED TO THE FACE OF BARRIER WALL ADJACENT TO ALL DRAINAGE STRUCTURES. THE STRIPE SHALL BE 4" WIDE BY 12 INCHES LONG AND EXTEND VERTICALLY AT 90 DEGREES FROM THE TOP EDGE OF THE BARRIER. THE PAINT WILL BE SPECIFIED AS ORANGE PAINT. THE COST OF THE STRIPE IS TO BE INCLUDED IN THE COST OF THE CONCRETE BARRIER OF TYPE AND SIZE SPECIFIED.
10. ANY GUARDRAIL REMOVED AND DEEMED SALVAGEABLE BY THE RESIDENT ENGINEER SHALL BE DELIVERED BY THE CONTRACTOR TO THE NEAREST IDOT MAINTENANCE FACILITY. TEMPORARY STORAGE AND DELIVERY SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR "GUARDRAIL REMOVAL".
11. BUTT JOINTS WILL BE INSTALLED AT THE END OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
12. THE BITUMINOUS MATERIAL PRIME COAT QUANTITIES HAVE BEEN DETERMINED USING A RATE OF 0.10 GAL/SQ. YD.
13. THE EXISTING US ROUTE 20 MAINLINE PAVEMENT IN BOTH THE EB AND WB DIRECTIONS CONSISTS OF A PCC BASE WITH A HMA OVERLAY. IN LOCATIONS WHERE PAVED SHOULDER REMOVAL IS SHOWN ON THE PLANS, THE CONTRACTOR SHALL REMOVE THE EXISTING HMA SHOULDER TO THE EDGE OF THE EXISTING PCC BASE COURSE TO THE SATISFACTION OF THE ENGINEER. THE COST FOR THIS WORK INCLUDING ANY ADDITIONAL SET UPS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR "PAVED SHOULDER REMOVAL". IN ADDITION, THE COST TO REPAIR ANY DAMAGE TO THE EXISTING MAINLINE PAVEMENT INCLUDING RAVELING OF THE EXISTING HMA OVERLAY AS A RESULT OF THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR "PAVED SHOULDER REMOVAL".

GENERAL NOTES (CONT.)

14. POROUS GRANULAR EMBANKMENT, SUBGRADE (PGES) HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSUITABLE OR UNSTABLE. THE ACTUAL NEED FOR THE REMOVAL AND REPLACEMENT WITH PGES WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 AND THE IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS ENCOUNTERED, THE SOIL SHALL BE REMOVED AND REPLACED WITH PGES OR EMBANKMENT AS DETERMINED BY THE GEOTECHNICAL ENGINEER. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.
15. TWO WEEKS PRIOR TO PLACING PERMANENT PAVEMENT MARKINGS, CONTACT MR. DON CHIARUGI, AREA FIELD ENGINEER, AT (847) 741-9857.
16. PAVED SHOULDER REMOVAL SHALL INCLUDE THE REMOVAL OF THE EXISTING PORTLAND CEMENT SHOULDER AND EXISTING HMA VARIABLE THICKNESS OVERLAY.
17. ALL EXISTING WHEEL STOPS, BOLLARDS, AND SIGNS LOCATED OUTSIDE OF THE EXISTING RIGHT-OF-WAY AND WITHIN PROPOSED RIGHT-OF-WAY OR EASEMENTS SHALL BE REMOVED AND SATISFACTORILY DISPOSED OF ACCORDING TO ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS. THE COST FOR THE REMOVAL AND DISPOSAL OF SUCH ITEMS THAT ARE NOT SPECIFICALLY INCLUDED IN A SEPARATE PAY ITEM SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR DRIVEWAY PAVEMENT REMOVAL.
18. THE PLATS OF HIGHWAYS INCLUDED IN THE CONTRACT PLANS ARE SUBJECT TO CHANGE. THE RESIDENT ENGINEER SHALL VERIFY THE RIGHT-OF-WAY AND EASEMENT INFORMATION SHOWN ON THE PLATS PRIOR TO CONSTRUCTION.
19. POURED-IN-PLACE CONCRETE DOMES WILL NOT BE ALLOWED AS A METHOD FOR INSTALLING DETECTABLE WARNINGS. DETECTABLE WARNINGS SHALL BE IN ACCORDANCE WITH THE LATEST STANDARDS OF THE CITY OF ELGIN AT THE TIME OF CONSTRUCTION.
20. THE LOCATIONS AND ELEVATIONS OF UTILITIES SHOWN ON THE PLANS ARE REPRESENTATIVE ONLY AND SHALL NOT BE CONSIDERED TO BE THE EXACT LOCATION OR ELEVATION OF THE FACILITY IN THE FIELD.
21. THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.
22. ANY WASTE GENERATED AS A SPECIAL WASTE OR A WASTE NOT CERTIFIED AS A NON-SPECIAL WASTE FROM THIS PROJECT SHOULD BE MANIFESTED OFF-SITE USING THE GENERATOR NUMBER ASSOCIATED WITH KANE COUNTY. THE GENERATOR NUMBER FOR KANE COUNTY IS 0898995009.
23. THE CONTRACTOR SHALL VERIFY THE EXISTING ELEVATIONS SHOWN IN THE PLANS WHERE THE PROPOSED PROFILE MEETS THE EXISTING PAVEMENT. THE ENGINEER SHALL BE NOTIFIED OF DISCREPANCIES BETWEEN THE EXISTING AND PROPOSED PAVEMENT ELEVATIONS, IN ADVANCE OF FINAL GRADING OPERATIONS, TO ALLOW APPROPRIATE ADJUSTMENTS TO BE MADE TO THE PROFILE. THE ELEVATIONS SHOWN ON THE PLANS ARE DERIVED FROM DIGITAL TERRAIN MODELING AND MAY REQUIRE ADJUSTMENT TO ENSURE A SMOOTH TRANSITION WHERE THE PROFILE MEETS THE EXISTING PAVEMENT.
24. THIS PROJECT WILL REQUIRE AUTHORIZATION UNDER NPDES PERMIT NUMBER ILR10 ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY (IEPA) FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES. A NOTICE OF INTENT MUST BE SUBMITTED TO IEPA 30 DAYS PRIOR TO THE START OF CONSTRUCTION.
25. THE TEMPORARY PAVEMENT SHOWN AS EXISTING PAVEMENT IS TO BE CONSTRUCTED IN CONTRACT #60K90 AS EITHER HMA OR PCC PAVEMENT. THAT CONTRACT HAS THE OPTION OF CONSTRUCTING THE TEMPORARY PAVEMENT ON US 20 OF EITHER 10-1/4" HMA PAVEMENT OR 8-1/4" PCC PAVEMENT AND THE TEMPORARY PAVEMENT ON MCLEAN BOULEVARD AND FLEETWOOD DRIVE OF EITHER 8" HMA PAVEMENT OR 7-1/2" PCC PAVEMENT. THE TYPICAL SECTIONS, CROSS SECTIONS, AND EARTHWORK CALCULATIONS ASSUME THAT THE TEMPORARY PAVEMENT TO BE CONSTRUCTED IN CONTRACT #60K90 WILL BE HMA PAVEMENT.
26. FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED, UNLESS NOTED ON THE PLAN.
27. SEE EROSION CONTROL PLAN, DETAIL FOR STABILIZED CONSTRUCTION ENTRANCE SHEET 255 FOR NOTES REGARDING PAYMENT FOR STABILIZED CONSTRUCTION ENTRANCES.
28. WHEREEVER THE CALLOUTS "AGGREGATE SUBGRADE 12" AND "AGG SUBGRADE 12" APPEAR ELSEWHERE ON THE PLANS, THEY SHALL BE CONSTRUED TO MEAN AND SHALL BE PAID FOR AS AGGREGATE SUBGRADE IMPROVEMENT 12".

COMMITMENTS

NONE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 FAP ROUTE 345 / US ROUTE 20

GENERAL NOTES

FILE NAME =	USER NAME = #USER#	DESIGNED - SLH	REVISED -	SCALE: N.T.S. SHEET NO. OF STA. TO STA.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE#		DRAWN - JFS	REVISED -		345	8R-R	KANE	794	3
	PLOT SCALE = #SCALE#	CHECKED - DDH	REVISED -		CONTRACT NO. 60H45				
	PLOT DATE = #DATE#	DATE - 02/22/12	REVISED -		FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				



\NFS-804\KAPV\VALT.D-TRANS.07\22802\21379-801\CIVIL\CAD\G0H45\SHEET.DWG\H45-SHT-GENNOTE.BLDGN
 KAPV\NTHM
 \N60H45-BORDER.BLDGN
 2-24-2012, 15:27:42

SEE SHEET 4 FOR DESCRIPTION OF COLUMN HEADINGS.

SPEC. ITEM	CODE NO.	ITEM NAME	UNIT	TOTAL QUANTITY	I	II	III	IV	V	VI	VII	VIII
					80% FED 20% STATE 0004	80% FED 20% STATE 0011	80% FED 20% STATE 0040	80% FED 20% STATE 0021	100% ELGIN 0021	80% FED 20% ELGIN 0021	100% STATE 0004	80% FED 20% ELGIN 0021
	48300515	PORTLAND CEMENT CONCRETE SHOULDERS 10 3/4"	SQ YD	29711	29711							
	50100300	REMOVAL OF EXISTING STRUCTURES NO. 1	EACH	1		1						
	50100400	REMOVAL OF EXISTING STRUCTURES NO. 2	EACH	1		1						
	50157300	PROTECTIVE SHIELD	SQ YD	876		876						
	50200100	STRUCTURE EXCAVATION	CU YD	1258		1141	117					
	50300225	CONCRETE STRUCTURES	CU YD	1110.4	8.5	1062.3	39.6					
	50300255	CONCRETE SUPERSTRUCTURE	CU YD	2187		2187						
	50300260	BRIDGE DECK GROOVING	SQ YD	6760		6760						
	50300280	CONCRETE ENCASEMENT	CU YD	31.6		31.6						
	50300285	FORM LINER TEXTURED SURFACE	SQ FT	540		540						
	50300300	PROTECTIVE COAT	SQ YD	8198	30	8146	22					
	50500105	FURNISHING AND ERECTING STRUCTURAL STEEL	L SUM	1		1						
	50500505	STUD SHEAR CONNECTORS	EACH	20586		20586						
	50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	771860	40810	726470	4580					
	50800515	BAR SPLICERS	EACH	5081		5081						
	50900105	ALUMINUM RAILING, TYPE L	FOOT	73			73					
	50901760	PIPE HANDRAIL	FOOT	32.5	32.5							
	51100100	SLOPE WALL 4 INCH	SQ YD	897		897						
	51201800	FURNISHING STEEL PILES HP14X73	FOOT	6290		6290						
	51202305	DRIVING PILES	FOOT	6290		6290						
	51203800	TEST PILE STEEL HP14X73	EACH	4		4						
	51204650	PILE SHOES	EACH	218		218						
	51500100	NAME PLATES	EACH	1		1						
	52000110	PREFORMED JOINT STRIP SEAL	FOOT	98		98						
	52100520	ANCHOR BOLTS, 1"	EACH	144		144						
	54213657	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12"	EACH	4	4							

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	PLOT DATE = #DATE#	DATE - 12/16/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 FAP ROUTE 345 / US ROUTE 20

SUMMARY OF QUANTITIES - IV

SCALE: N.T.S. SHEET NO. OF STA. TO STA.

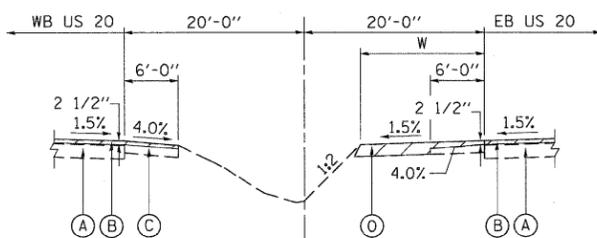
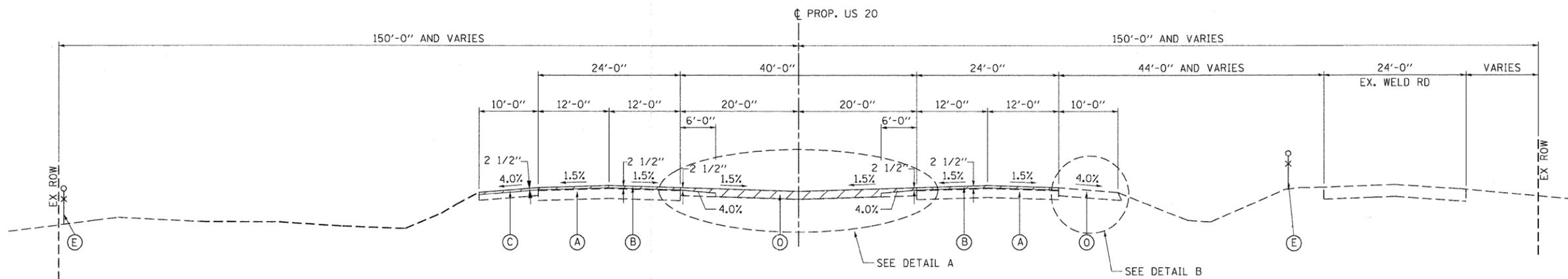
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	BR-R	KANE	794	7
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 60H45	

SEE SHEET 4
FOR DESCRIPTION OF
COLUMN HEADINGS.

SPEC. ITEM	CODE NO.	ITEM NAME	UNIT	TOTAL QUANTITY	I	II	III	IV	V	VI	VII	VIII
					80% FED 20% STATE 0004	80% FED 20% STATE 0011	80% FED 20% STATE 0040	80% FED 20% STATE 0021	100% ELGIN 0021	80% FED 20% ELGIN 0021	100% STATE 0004	80% FED 20% ELGIN 0021
	55200600	STORM SEWERS JACKED IN PLACE, 18"	FOOT	106	106							
	55200900	STORM SEWERS JACKED IN PLACE, 24"	FOOT	610	610							
	55201300	STORM SEWERS JACKED IN PLACE, 36"	FOOT	178	178							
	56106400	ADJUSTING WATER MAIN 8"	FOOT	125	125							
	56106600	ADJUSTING WATER MAIN 12"	FOOT	157	157							
	56400100	FIRE HYDRANTS TO BE MOVED	EACH	3	3							
	56400400	FIRE HYDRANTS TO BE RELOCATED	EACH	3	3							
	58700300	CONCRETE SEALER	SQ FT	2565		2565						
	59100100	GEOCOMPOSITE WALL DRAIN	SQ YD	287		254	33					
	60100060	CONCRETE HEADWALLS FOR PIPE DRAINS	EACH	27	27							
	60107600	PIPE UNDERDRAINS 4"	FOOT	2924	2924							
	60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	8	8							
	60200805	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 8 GRATE	EACH	26	26							
	60201340	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	119	119							
	60204505	CATCH BASINS, TYPE A, 5'-DIAMETER, TYPE 8 GRATE	EACH	9	9							
	60205040	CATCH BASINS, TYPE A, 5'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	2	2							
	60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	33	33							
	60221100	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	31	31							
	60223800	MANHOLES, TYPE A, 6'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	2	2							
	60240328	INLETS, TYPE B, TYPE 24 FRAME AND GRATE	EACH	32	32							
	60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	1	1							
	60255500	MANHOLES TO BE ADJUSTED	EACH	8	8							
	60255700	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	1	1							
	60260100	INLETS TO BE ADJUSTED	EACH	1	1							
	60260400	INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	8	8							
	60500040	REMOVING MANHOLES	EACH	12	12							

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#FILE#	PLOT SCALE = #SCALE#	DRAWN - KKS	REVISED -									345	BR-R	KANE	794	9
TENG TENG & ASSOCIATES, INC. ENGINEERS, ARCHITECTS, PLANNERS CHICAGO, ILLINOIS	PLOT DATE = #DATE#	CHECKED - DDH	REVISED -	SCALE: N.T.S.				SHEET NO.	OF	STA.	TO STA.	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
		DATE - 12/16/11	REVISED -									CONTRACT NO. 60H45				

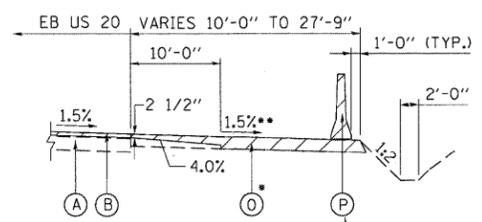


DETAIL A

STA 55+00 TO STA 70+00 (WB)
 STA 56+50 TO STA 70+00 (EB)
 W=6' STA 59+35 TO STA 60+00
 W=20' STA 60+00 TO STA 61+00

NOTES:

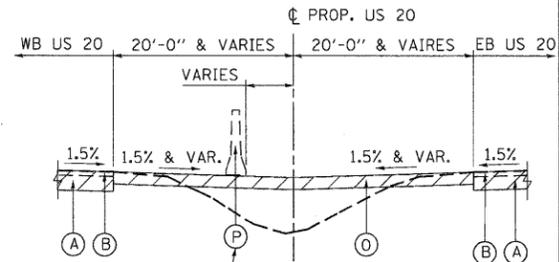
1. THE TEMPORARY CONCRETE BARRIER IS SECURED TO THE PAVEMENT USING SIX ANCHORING PINS.
2. EXISTING DIMENSIONS SHOWN ARE FROM RECORD PLANS. ACTUAL DIMENSIONS MAY VARY.
3. SEE GENERAL NOTES SHEET 3 FOR TEMPORARY PAVEMENT INFORMATION.
4. THE EXISTING PAVEMENT SLABS ARE REINFORCED WITH WIRE MESH.
5. EXISTING THICKNESSES SHOWN ARE FROM RECORD PLANS AND PAVEMENT CORES. ACTUAL THICKNESSES MAY VARY.



DETAIL B

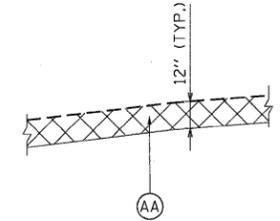
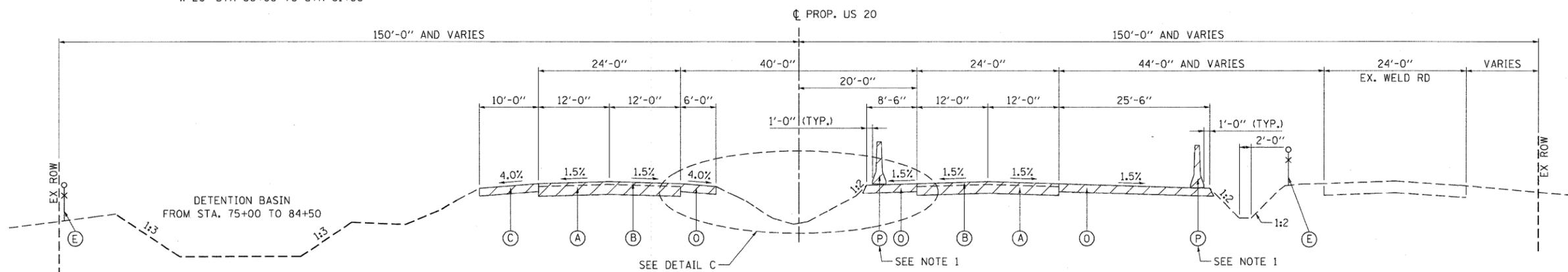
STA 56+50 TO STA 66+00

- EB US 20 OUTSIDE SHOULDER WIDENING BEGINS AT STA 64+37
- CROSS SLOPE TRANSITIONS FROM 4.0% TO 1.5% FROM STA 64+37 TO STA 65+00



DETAIL C

STA 78+50 TO STA 84+50



EXISTING US 20 TOP SOIL REMOVAL DETAIL

SEE US 20 CROSS SECTIONS FOR TOPSOIL REMOVAL LIMITS

EXISTING LEGEND

- | | | |
|------------------------------------|---|-----------------------------------|
| (A) PCC PAVEMENT (10" AND VARIES) | (K) PCC SIDE WALK | (U) PCC PAVEMENT |
| (B) HMA SURFACE (3" AND VARIES) | (L) CURB AND GUTTER | (V) PCC PAVEMENT (11" AND VARIES) |
| (C) HMA SHOULDER (8" AND VARIES) | (M) CONCRETE MEDIAN | (W) HMA SURFACE (4" AND VARIES) |
| (D) HMA SHOULDER (6" AND VARIES) | (N) STEEL PLATE BEAM GUARDRAIL | (X) HMA SURFACE (5" AND VARIES) |
| (E) FENCE | (O) TEMP PAVEMENT (SEE NOTE 3) | (Y) HMA SURFACE (6" AND VARIES) |
| (F) CONCRETE MEDIAN BARRIER & BASE | (P) TEMPORARY CONCRETE BARRIER | (Z) CONC CURB TB |
| (G) PCC SHOULDER (10" AND VARIES) | (R) RETAINING WALL (CAST-IN-PLACE) | (AA) REMOVAL AND DISPOSAL |
| (H) TOPSOIL REMOVAL (X") | (S) TEMP. SOIL RETENTION SYSTEM | OF UNSUITABLE MATERIAL |
| (I) HMA BASE COURSE | (T) SUBBASE GRANULAR MATERIAL, TYPE C (10") | |
| (J) HMA PAVEMENT | | |

PROPOSED LEGEND

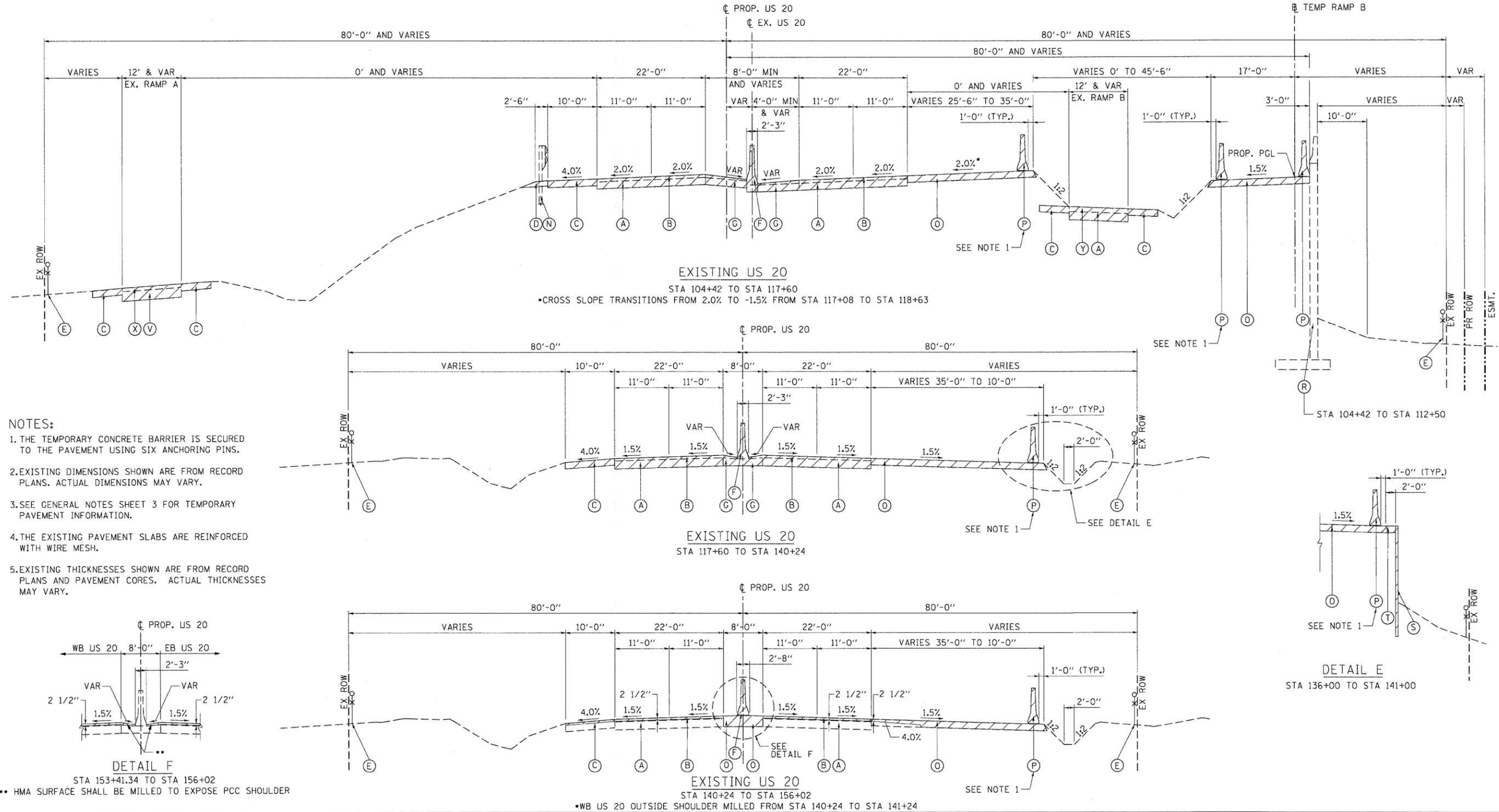
- | | |
|---|--|
| (1) CONTINUOUSLY REINFORCED CONCRETE PAVEMENT 10 3/4" | (11) COMB CC&G TB6.24 |
| (2) PCC PVT 10 JOINTED | (12) COMB CC&G TM4.24 |
| (3) STABILIZED SUBBASE (HMA) 4 1/2" | (13) CONC CURB TB SPL |
| (4) AGGREGATE SUBGRADE 12" | (14) PC CONC SIDEWALK 5 |
| (5) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2" | (15) SPBGR TY A 6FT POSTS |
| (6) SUB-BASE GRANULAR MATERIAL, TYPE B 12" | (16) EMBANKMENT |
| (7) CONCRETE BARRIER, DOUBLE FACE, 32" HEIGHT | (17) RETAINING WALL (SEE STRUCTURAL PLANS) |
| (8) CONCRETE BARRIER BASE | (18) DRILL AND GROUT #6 TIE BARS |
| (9) AGG WEDGE SHLD TYPE B | (19) TOPSOIL FURNISH AND PLACE, 6" |
| | (20) TOPSOIL FURNISH AND PLACE, 10" |

EXISTING TYPICAL SECTIONS

- | | |
|---|---|
| (21) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 3/4" | (33) AGGREGATE SHOULDERS, TYPE B 4" |
| (22) HOT-MIX ASPHALT BASE COURSE, 8" | (34) SEEDING (SEE LANDSCAPING PLANS) |
| (23) SHOULDER RUMBLE STRIPS, 16" | (35) PCC SHOULDERS 10 3/4" |
| (24) AGGREGATE SURFACE COURSE, TYPE B, 4" | (36) PCC SHOULDERS 10" |
| (25) CONCRETE MEDIAN SURFACE, 4 INCH | (37) AGGREGATE SHOULDERS, TYPE B 10" |
| (26) CONCRETE MEDIAN, TYPE SB (SPECIAL) | (38) CONCRETE BARRIER, SINGLE FACE 42" HEIGHT (SPECIAL) |
| (27) POLY. HMA SURFACE COURSE, MIX "F", N90 1 3/4" | (39) CORRUGATED MEDIAN |
| (28) CHAIN LINK FENCE, 4' | (40) HOT-MIX ASPHALT BASE COURSE, 7" |
| (29) POLY. LEVELING BINDER (MM), IL-4.75, N50 3/4" (MIN) | (41) SODDING, SALT TOLERANT |
| (30) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 3" | |
| (31) POLY. HMA SURFACE COURSE, MIX "F", N90 2" | |
| (32) COMB CC&G TB6.06 | |

\N168H45-XSCT1.DGN, \N168H45-BORDER.DGN, \N168H45-LEGEN.DGN, \N168H45-PLAN.DGN, \N168H45-TYPICAL.DGN
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PLOT DATE = #DATE#		DATE - 12/16/11	REVISED -			SCALE: 1"=10'	SHEET NO.	OF	STA. 56+50.00	TO STA. 89+00.00	FED. ROAD DIST. NO.



- NOTES:**
1. THE TEMPORARY CONCRETE BARRIER IS SECURED TO THE PAVEMENT USING SIX ANCHORING PINS.
 2. EXISTING DIMENSIONS SHOWN ARE FROM RECORD PLANS. ACTUAL DIMENSIONS MAY VARY.
 3. SEE GENERAL NOTES SHEET 3 FOR TEMPORARY PAVEMENT INFORMATION.
 4. THE EXISTING PAVEMENT SLABS ARE REINFORCED WITH WIRE MESH.
 5. EXISTING THICKNESSES SHOWN ARE FROM RECORD PLANS AND PAVEMENT CORES. ACTUAL THICKNESSES MAY VARY.

EXISTING LEGEND

- (A) PCC PAVEMENT (10" AND VARIES)
- (B) HMA SURFACE (3" AND VARIES)
- (C) HMA SHOULDER (8" AND VARIES)
- (D) HMA SHOULDER (6" AND VARIES)
- (E) FENCE
- (F) CONCRETE MEDIAN BARRIER & BASE
- (G) PCC SHOULDER (10" AND VARIES)
- (H) TOPSOIL REMOVAL (X")
- (I) HMA BASE COURSE
- (J) HMA PAVEMENT
- (K) PCC SIDE WALK
- (L) CURB AND GUTTER
- (M) CONCRETE MEDIAN
- (N) STEEL PLATE BEAM GUARDRAIL
- (O) TEMP PAVEMENT (SEE NOTE 3)
- (P) TEMPORARY CONCRETE BARRIER
- (Q) RETAINING WALL (CAST-IN-PLACE)
- (R) TEMP. SOIL RETENTION SYSTEM
- (S) SUBBASE GRANULAR MATERIAL, TYPE C (10")
- (U) PCC PAVEMENT
- (V) PCC PAVEMENT (11" AND VARIES)
- (W) HMA SURFACE (4" AND VARIES)
- (X) HMA SURFACE (5" AND VARIES)
- (Y) HMA SURFACE (6" AND VARIES)
- (Z) CONC CURB TB
- (AA) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL

PROPOSED LEGEND

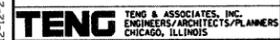
- (1) CONTINUOUSLY REINFORCED CONCRETE PAVEMENT 10 3/4"
- (2) PCC PVT 10 JOINTED
- (3) STABILIZED SUBBASE (HMA) 4 1/2"
- (4) AGGREGATE SUBGRADE 12"
- (5) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- (6) SUB-BASE GRANULAR MATERIAL, TYPE B 12"
- (7) CONCRETE BARRIER, DOUBLE FACE, 32" HEIGHT
- (8) CONCRETE BARRIER BASE
- (9) AGG WEDGE SHLD TYPE B
- (10) COMB CC&G TB6.12
- (11) COMB CC&G TB6.24
- (12) COMB CC&G TM4.24
- (13) CONC CURB TB SPL
- (14) PC CONC SIDEWALK 5
- (15) SPBGR TY A 6FT POSTS
- (16) EMBANKMENT
- (17) RETAINING WALL (SEE STRUCTURAL PLANS)
- (18) DRILL AND GROUT #6 TIE BARS
- (19) TOPSOIL FURNISH AND PLACE, 6"
- (20) TOPSOIL FURNISH AND PLACE, 10"
- (21) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 3/4"
- (22) HOT-MIX ASPHALT BASE COURSE, 8"
- (23) SHOULDER RUMBLE STRIPS, 16"
- (24) AGGREGATE SURFACE COURSE, TYPE B, 4"
- (25) CONCRETE MEDIAN SURFACE, 4 INCH
- (26) CONCRETE MEDIAN, TYPE SB (SPECIAL)
- (27) POLY. HMA SURFACE COURSE, MIX "F", N90 1 3/4"
- (28) CHAIN LINK FENCE, 4"
- (29) POLY. LEVELING BINDER (MM), IL-4.75, N50 3/4" (MIN)
- (30) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 3"
- (31) POLY. HMA SURFACE COURSE, MIX "F", N90 2"
- (32) COMB CC&G TB6.06
- (33) AGGREGATE SHOULDERS, TYPE B 4"
- (34) SEEDING (SEE LANDSCAPING PLANS)
- (35) PCC SHOULDERS 10 3/4"
- (36) PCC SHOULDERS 10"
- (37) AGGREGATE SHOULDERS, TYPE B 10"
- (38) CONCRETE BARRIER, SINGLE FACE 42" HEIGHT (SPECIAL)
- (39) CORRUGATED MEDIAN
- (40) HOT-MIX ASPHALT BASE COURSE, 7"
- (41) SODDING, SALT TOLERANT

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 FAP ROUTE 345 / US ROUTE 20

EXISTING TYPICAL SECTIONS
U.S. ROUTE 20

SCALE: 1"=10'	SHEET NO. OF	STA. 104+42.00 TO STA. 156+02.00	F.A.P. RTE. 345	SECTION 8R-R	COUNTY KANE	TOTAL SHEETS 794	SHEET NO. 23
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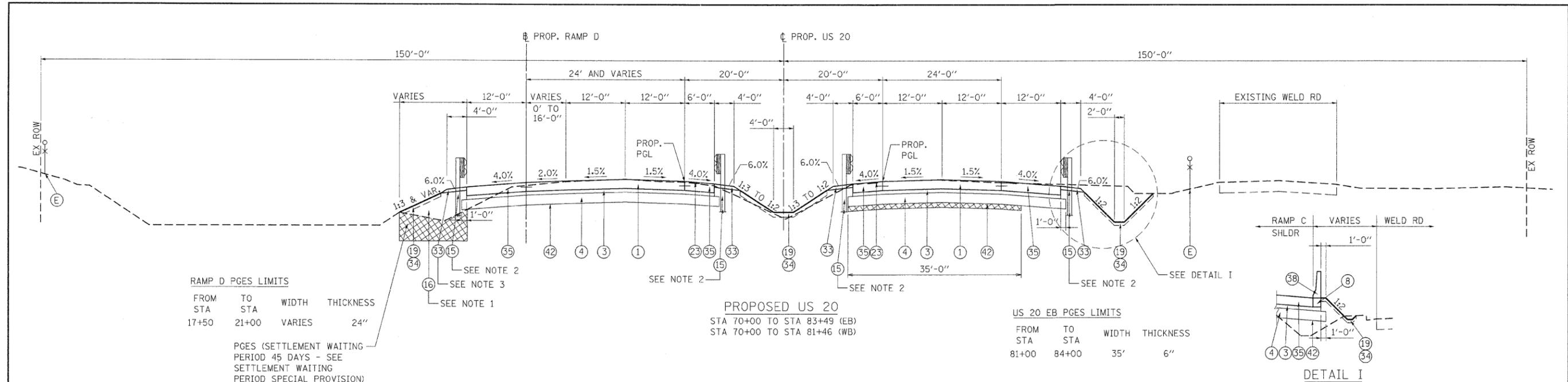
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TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 CHICAGO, ILLINOIS

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PLOT DATE = #DATE#		DATE - 12/16/11	REVISED -

CONTRACT NO. 60H45
 FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT



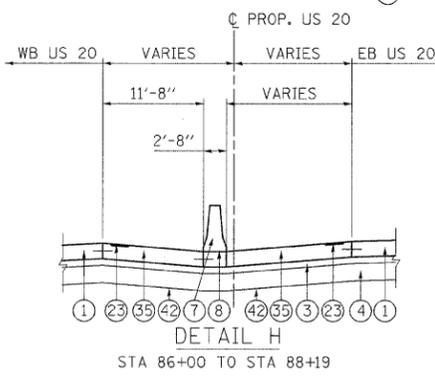
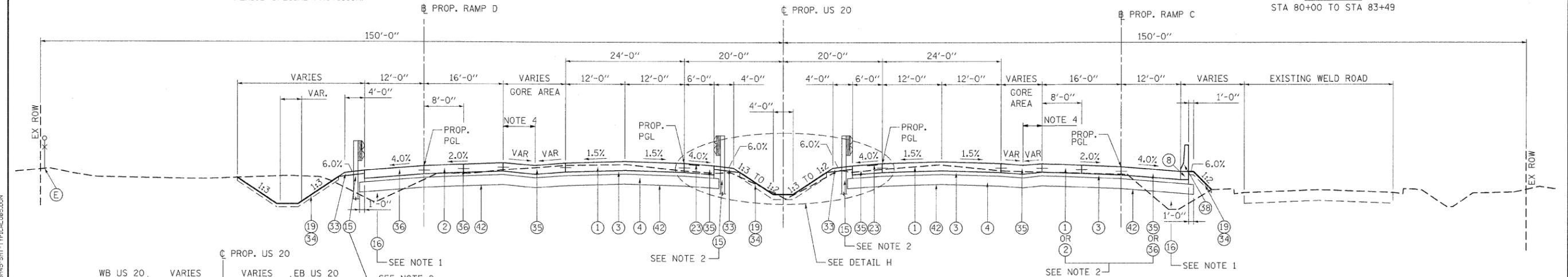
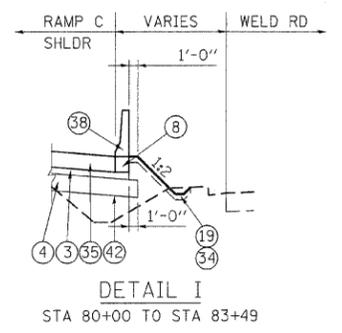
RAMP D PGES LIMITS

FROM STA	TO STA	WIDTH	THICKNESS
17+50	21+00	VARIES	24"

PGES (SETTLEMENT WAITING PERIOD 45 DAYS - SEE SETTLEMENT WAITING PERIOD SPECIAL PROVISION)

US 20 EB PGES LIMITS

FROM STA	TO STA	WIDTH	THICKNESS
81+00	84+00	35'	6"



- NOTES:**
1. ALL PROPOSED EMBANKMENT CONSTRUCTED ALONG ROUTE U.S. 20 SHALL BE CONSTRUCTED ACCORDING TO THE "EMBANKMENT I" AND "EMBANKMENT STABILITY" SPECIAL PROVISIONS. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR "EARTH EXCAVATION" OR "FURNISHED EXCAVATION" AS APPLICABLE.
 2. SEE ROADWAY PLANS FOR LIMITS.
 3. AGGREGATE SHOULDERS, TYPE B 10" SHALL BE USED AT NON GUARDRAIL LOCATIONS. SEE ROADWAY PLANS FOR LIMITS.
 4. TRANSITION SHOULDER THICKNESS FROM LOWPOINT (10 3/4") TO EDGE OF RAMP (10"). TRANSITION SHOULDER THICKNESS FROM MAINLINE PAVEMENT (10 3/4") TO RAMP (10") WHERE MAINLINE PAVEMENT CONNECTS TO RAMP WITH NO LOW POINT.
 5. SEE PAVEMENT JOINTING PLANS FOR PAVEMENT JOINTS AND ELEVATIONS AT GORE AREAS.

- = GEOTECHNICAL FABRIC FOR GROUND STABILIZATION SHALL BE PLACED BENEATH THE ENTIRE LIMITS OF PROPOSED AGGREGATE SUBGRADE 12" ON US ROUTE 20 AND RAMPS.
- = FOR PAVEMENTS AND SHOULDERS ON US ROUTE 20 AND RAMPS, THESE ITEMS SHALL BE CONSTRUCTED TO 30-YEAR EXTENDED LIFE PAVEMENT REQUIREMENTS.

EXISTING LEGEND

- (A) PCC PAVEMENT (10" AND VARIES)
- (B) HMA SURFACE (3" AND VARIES)
- (C) HMA SHOULDER (8" AND VARIES)
- (D) HMA SHOULDER (6" AND VARIES)
- (E) FENCE
- (F) CONCRETE MEDIAN BARRIER & BASE
- (G) PCC SHOULDER (10" AND VARIES)
- (H) TOPSOIL REMOVAL (X")
- (I) HMA BASE COURSE
- (J) HMA PAVEMENT
- (K) PCC SIDE WALK
- (L) CURB AND GUTTER
- (M) CONCRETE MEDIAN
- (N) STEEL PLATE BEAM GUARDRAIL
- (O) TEMP PAVEMENT
- (P) TEMPORARY CONCRETE BARRIER
- (R) RETAINING WALL (CAST-IN-PLACE)
- (S) TEMP. SOIL RETENTION SYSTEM
- (T) SUBBASE GRANULAR MATERIAL, TYPE C (10")
- (U) PCC PAVEMENT (11" AND VARIES)
- (V) HMA SURFACE (4" AND VARIES)
- (W) HMA SURFACE (5" AND VARIES)
- (X) HMA SURFACE (6" AND VARIES)
- (Y) CONC CURB TB
- (Z) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL

PROPOSED LEGEND

- ① CONTINUOUSLY REINFORCED CONCRETE PAVEMENT 10 3/4"
- ② PCC PVT 10 JOINTED
- ③ STABILIZED SUBBASE (HMA) 4 1/2"
- ④ AGGREGATE SUBGRADE 12"
- ⑤ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- ⑥ SUB-BASE GRANULAR MATERIAL, TYPE B 12"
- ⑦ CONCRETE BARRIER, DOUBLE FACE, 32" HEIGHT
- ⑧ CONCRETE BARRIER BASE
- ⑨ AGG WEDGE SHLD TYPE B
- ⑩ COMB CC&G TB6.12
- ⑪ COMB CC&G TB6.24
- ⑫ COMB CC&G TM4.24
- ⑬ CONC CURB TB SPL
- ⑭ PC CONC SIDEWALK 5
- ⑮ SPBGR TY A 6FT POSTS
- ⑯ EMBANKMENT
- ⑰ RETAINING WALL (SEE STRUCTURAL PLANS)
- ⑱ DRILL AND GROUT #6 TIE BARS
- ⑲ TOPSOIL FURNISH AND PLACE, 6"
- ⑳ TOPSOIL FURNISH AND PLACE, 10"

- ㉑ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 3/4"
- ㉒ HOT-MIX ASPHALT BASE COURSE, 8"
- ㉓ SHOULDER RUMBLE STRIPS, 16"
- ㉔ AGGREGATE SURFACE COURSE, TYPE B, 4"
- ㉕ CONCRETE MEDIAN SURFACE, 4 INCH
- ㉖ CONCRETE MEDIAN, TYPE SB (SPECIAL)
- ㉗ POLY. HMA SURFACE COURSE, MIX "F", N90 1 3/4"
- ㉘ CHAIN LINK FENCE, 4"
- ㉙ POLY. LEVELING BINDER (MM), IL-4.75, N50 3/4" (MIN)
- ㉚ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 3"
- ㉛ POLY. HMA SURFACE COURSE, MIX "F", N90 2"
- ㉜ COMB CC&G TB6.06
- ㉝ AGGREGATE SHOULDERS, TYPE B 4"
- ㉞ SEEDING (SEE LANDSCAPING PLANS)
- ㉟ PCC SHOULDERS 10 3/4"
- ㊱ PCC SHOULDERS 10"
- ㊲ AGGREGATE SHOULDERS, TYPE B 10"
- ㊳ CONCRETE BARRIER, SINGLE FACE 42" HEIGHT (SPECIAL)
- ㊴ CORRUGATED MEDIAN
- ㊵ HOT-MIX ASPHALT BASE COURSE, 7"
- ㊶ SODDING, SALT TOLERANT
- ㊷ GEOTECH FAB F/GR STAB

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	PLOT DATE = #DATE#	DATE - 01/26/12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
FAP ROUTE 345 / US ROUTE 20

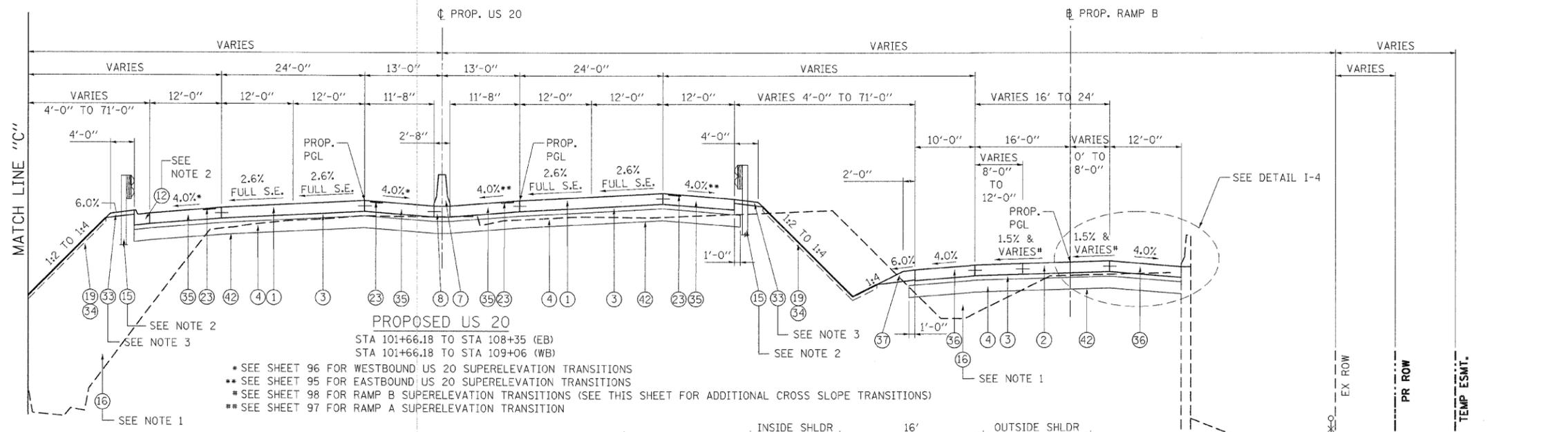
PROPOSED TYPICAL SECTIONS
U.S. ROUTE 20

SCALE: 1"=10'	SHEET NO. OF	STA. 70+00.00 TO STA. 88+19.00
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	8R-R	KANE	794	25
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 60H45	

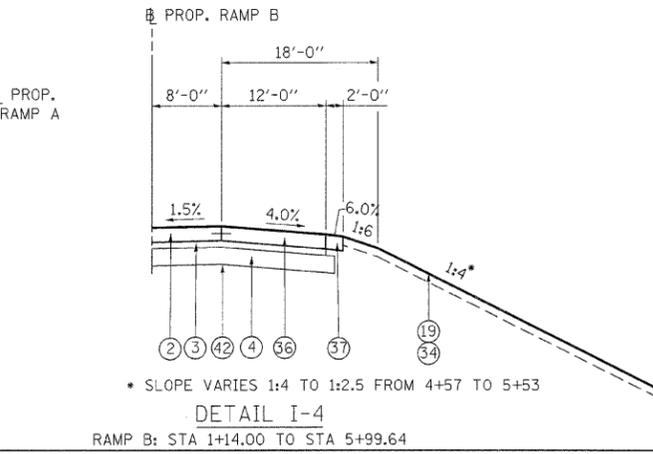
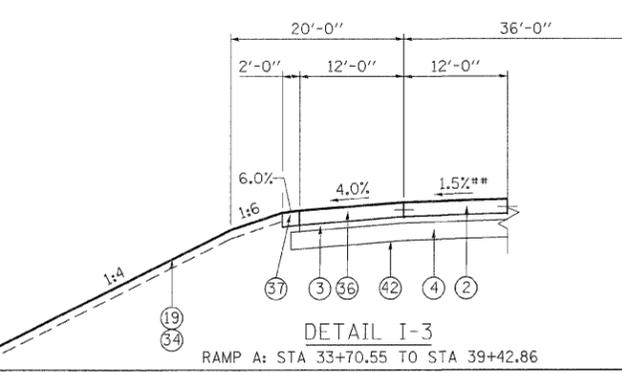
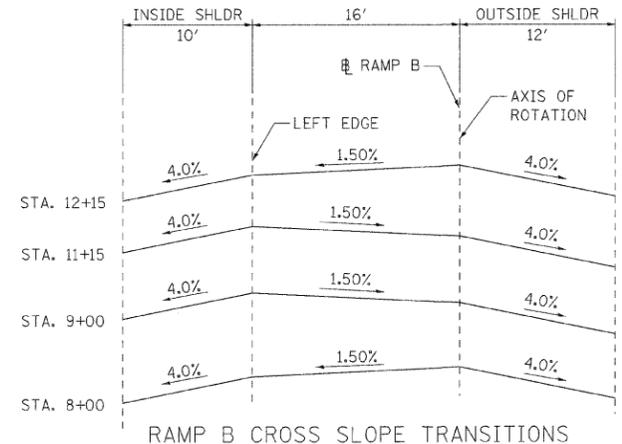
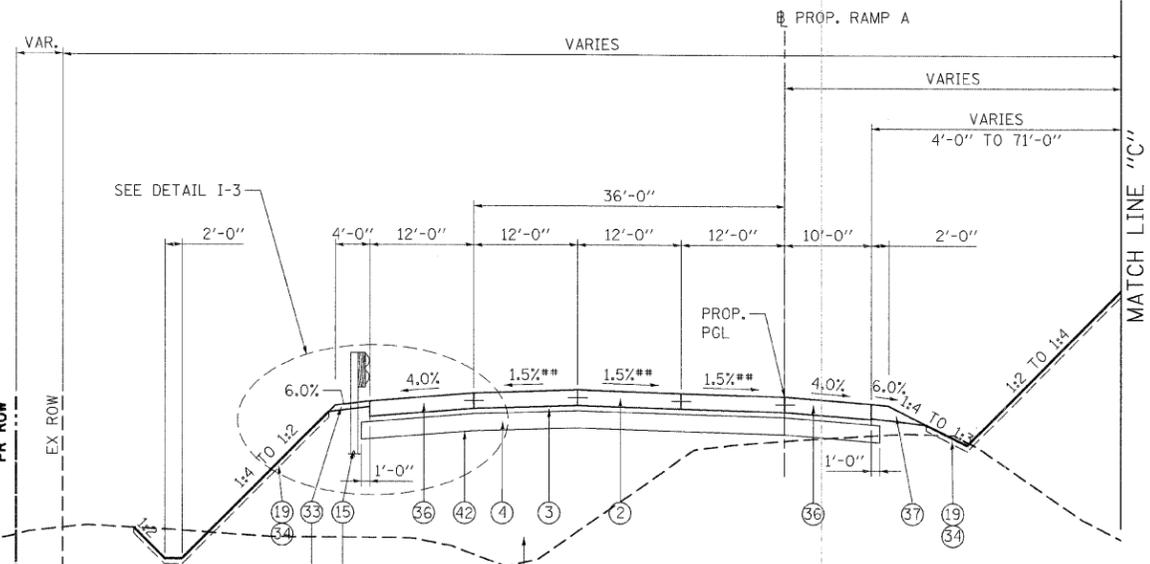
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PROPOSED US 20
 STA 101+66.18 TO STA 108+35 (EB)
 STA 101+66.18 TO STA 109+06 (WB)

* SEE SHEET 96 FOR WESTBOUND US 20 SUPERELEVATION TRANSITIONS
 ** SEE SHEET 95 FOR EASTBOUND US 20 SUPERELEVATION TRANSITIONS
 * SEE SHEET 98 FOR RAMP B SUPERELEVATION TRANSITIONS (SEE THIS SHEET FOR ADDITIONAL CROSS SLOPE TRANSITIONS)
 ** SEE SHEET 97 FOR RAMP A SUPERELEVATION TRANSITION



- NOTES:
- ALL PROPOSED EMBANKMENT CONSTRUCTED ALONG ROUTE U.S. 20 SHALL BE CONSTRUCTED ACCORDING TO THE "EMBANKMENT I" AND "EMBANKMENT STABILITY" SPECIAL PROVISIONS. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR "EARTH EXCAVATION" OR "FURNISHED EXCAVATION" AS APPLICABLE.
 - SEE ROADWAY PLANS FOR LIMITS.
 - AGGREGATE SHOULDERS, TYPE B 10" SHALL BE USED AT NON GUARDRAIL LOCATIONS. SEE ROADWAY PLANS FOR LIMITS.
 - SEE PAVEMENT JOINTING PLANS FOR PAVEMENT JOINTS.
- = GEOTECHNICAL FABRIC FOR GROUND STABILIZATION SHALL BE PLACED BENEATH THE ENTIRE LIMITS OF PROPOSED AGGREGATE SUBGRADE 12" ON US ROUTE 20 AND RAMPS.
 □ = FOR PAVEMENTS AND SHOULDERS ON US ROUTE 20 AND RAMPS, THESE ITEMS SHALL BE CONSTRUCTED TO 30-YEAR EXTENDED LIFE PAVEMENT REQUIREMENTS.

EXISTING LEGEND

(A) PCC PAVEMENT (10" AND VARIES)	(K) PCC SIDE WALK
(B) HMA SURFACE (3" AND VARIES)	(L) CURB AND GUTTER
(C) HMA SHOULDER (8" AND VARIES)	(M) CONCRETE MEDIAN
(D) HMA SHOULDER (6" AND VARIES)	(N) STEEL PLATE BEAM GUARDRAIL
(E) FENCE	(O) TEMP PAVEMENT
(F) CONCRETE MEDIAN BARRIER & BASE	(P) TEMPORARY CONCRETE BARRIER
(G) PCC SHOULDER (10" AND VARIES)	(R) RETAINING WALL (CAST-IN-PLACE)
(H) TOPSOIL REMOVAL (X")	(S) TEMP. SOIL RETENTION SYSTEM
(I) HMA BASE COURSE	(T) SUBBASE GRANULAR MATERIAL, TYPE C (10")
(J) HMA PAVEMENT	

PROPOSED LEGEND

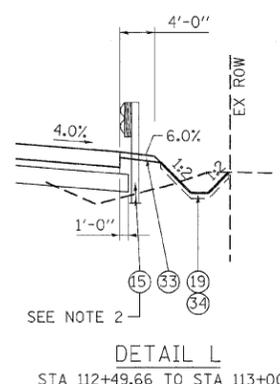
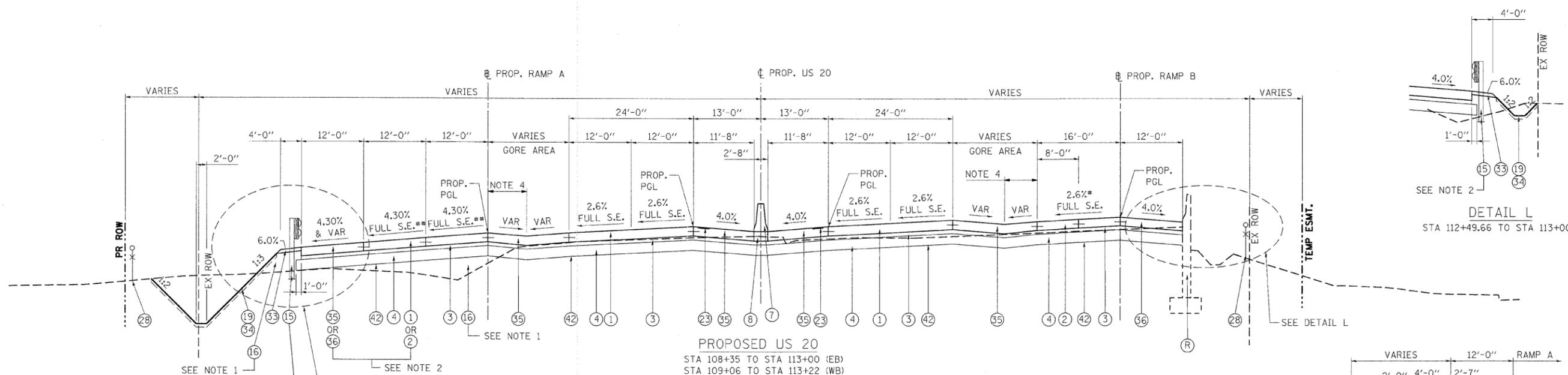
(U) PCC PAVEMENT	(V) PCC PAVEMENT (11" AND VARIES)
(W) HMA SURFACE (4" AND VARIES)	(X) HMA SURFACE (5" AND VARIES)
(Y) HMA SURFACE (6" AND VARIES)	(Z) CONC CURB TB
(AA) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	

- PROPOSED LEGEND**
- ① CONTINUOUSLY REINFORCED CONCRETE PAVEMENT 10 3/4"
 - ② PCC PVT 10 JOINTED
 - ③ STABILIZED SUBBASE (HMA) 4 1/2"
 - ④ AGGREGATE SUBGRADE 12"
 - ⑤ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
 - ⑥ SUB-BASE GRANULAR MATERIAL, TYPE B 12"
 - ⑦ CONCRETE BARRIER, DOUBLE FACE, 32" HEIGHT
 - ⑧ CONCRETE BARRIER BASE
 - ⑨ AGG WEDGE SHLD TYPE B
 - ⑩ COMB CC&G TB6.12
 - ⑪ COMB CC&G TB6.24
 - ⑫ COMB CC&G TM4.24
 - ⑬ CONC CURB TB SPL
 - ⑭ PC CONC SIDEWALK 5
 - ⑮ SPBGR TY A 6FT POSTS
 - ⑯ EMBANKMENT
 - ⑰ RETAINING WALL (SEE STRUCTURAL PLANS)
 - ⑱ DRILL AND GROUT #6 TIE BARS
 - ⑲ TOPSOIL FURNISH AND PLACE, 6"
 - ⑳ TOPSOIL FURNISH AND PLACE, 10"

- ㉑ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 3/4"
- ㉒ SEEDING (SEE LANDSCAPING PLANS)
- ㉓ SHOULDER RUMBLE STRIPS, 16"
- ㉔ AGGREGATE SURFACE COURSE, TYPE B, 4"
- ㉕ CONCRETE MEDIAN SURFACE, 4 INCH
- ㉖ CONCRETE MEDIAN, TYPE SB (SPECIAL)
- ㉗ POLY. HMA SURFACE COURSE, MIX "F", N90 1 3/4"
- ㉘ CHAIN LINK FENCE, 4'
- ㉙ POLY. LEVELING BINDER (MM), IL-4.75, N50 3/4" (MIN)
- ㉚ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 3"
- ㉛ POLY. HMA SURFACE COURSE, MIX "F", N90 2"
- ㉜ COMB CC&G TB6.06
- ㉝ AGGREGATE SHOULDERS, TYPE B 4"
- ㉞ PCC SHOULDERS 10 3/4"
- ㉟ PCC SHOULDERS 10"
- ㊱ AGGREGATE SHOULDERS, TYPE B 10" HEIGHT (SPECIAL)
- ㊲ CORRUGATED MEDIAN
- ㊳ HOT-MIX ASPHALT BASE COURSE, 7"
- ㊴ SODDING, SALT TOLERANT
- ㊵ GEOTECH FAB F/GR STAB

FILE NAME =	USER NAME = #USER#	DESIGNED - MRK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION FAP ROUTE 345 / US ROUTE 20	PROPOSED TYPICAL SECTIONS U.S. ROUTE 20	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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PLOT DATE = #DATE#		DATE - 01/26/12	REVISED -			SCALE: 1"=10'	SHEET NO.	OF	STA. 101+65.00	TO STA. 108+35.00	FED. ROAD DIST. NO.

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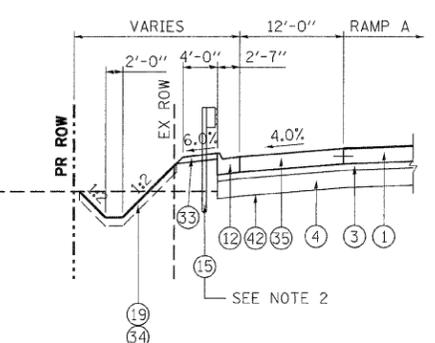
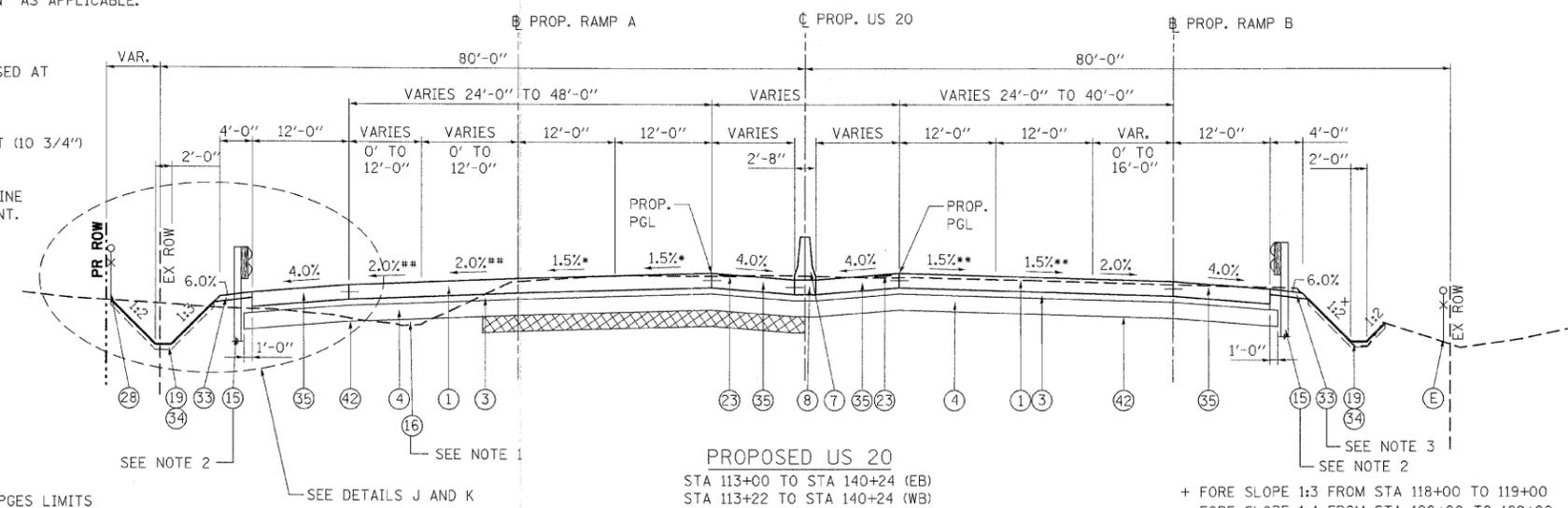


DETAIL L
STA 112+49.66 TO STA 113+00

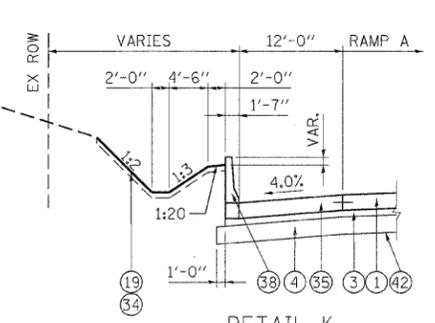
NOTES:

1. ALL PROPOSED EMBANKMENT CONSTRUCTED ALONG ROUTE U.S. 20 SHALL BE CONSTRUCTED ACCORDING TO THE "EMBANKMENT I" AND "EMBANKMENT STABILITY" SPECIAL PROVISIONS. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR "EARTH EXCAVATION" OR "FURNISHED EXCAVATION" AS APPLICABLE.
2. SEE ROADWAY PLANS FOR LIMITS.
3. AGGREGATE SHOULDERS, TYPE B 10" SHALL BE USED AT NON GUARDRAIL LOCATIONS. SEE ROADWAY PLANS FOR LIMITS.
4. TRANSITION SHOULDER THICKNESS FROM LOWPOINT (10 3/4") TO EDGE OF RAMP (10"). TRANSITION SHOULDER THICKNESS FROM MAINLINE PAVEMENT (10 3/4") TO RAMP (10") WHERE MAINLINE PAVEMENT CONNECTS TO RAMP WITH NO LOW POINT.
5. SEE PAVEMENT JOINTING PLANS FOR PAVEMENT JOINTS AND ELEVATIONS AT GORE AREAS.

- * SEE SHEET 96 FOR WESTBOUND US 20 SUPERELEVATION TRANSITIONS
- ** SEE SHEET 95 FOR EASTBOUND US 20 SUPERELEVATION TRANSITIONS
- *** SEE SHEET 98 FOR RAMP B SUPERELEVATION TRANSITIONS
- **** SEE SHEET 97 FOR RAMP A SUPERELEVATION TRANSITION



DETAIL J
STA 112+00 TO STA 126+50



DETAIL K
STA 126+50 TO STA 133+00

US 20 WB PGES LIMITS

FROM STA	TO STA	WIDTH	THICKNESS
116+00	119+00	CENTER LINE TO 40' LT (40')	12"

- * SEE SHEET 96 FOR WESTBOUND US 20 SUPERELEVATION TRANSITIONS
- ** SEE SHEET 95 FOR EASTBOUND US 20 SUPERELEVATION TRANSITIONS
- *** SEE SHEET 98 FOR RAMP B SUPERELEVATION TRANSITIONS
- **** SEE SHEET 97 FOR RAMP A SUPERELEVATION TRANSITION

- = GEOTECHNICAL FABRIC FOR GROUND STABILIZATION SHALL BE PLACED BENEATH THE ENTIRE LIMITS OF PROPOSED AGGREGATE SUBGRADE 12" ON US ROUTE 20 AND RAMPS.
- ▣ = FOR PAVEMENTS AND SHOULDERS ON US ROUTE 20 AND RAMPS, THESE ITEMS SHALL BE CONSTRUCTED TO 30-YEAR EXTENDED LIFE PAVEMENT REQUIREMENTS.

EXISTING LEGEND

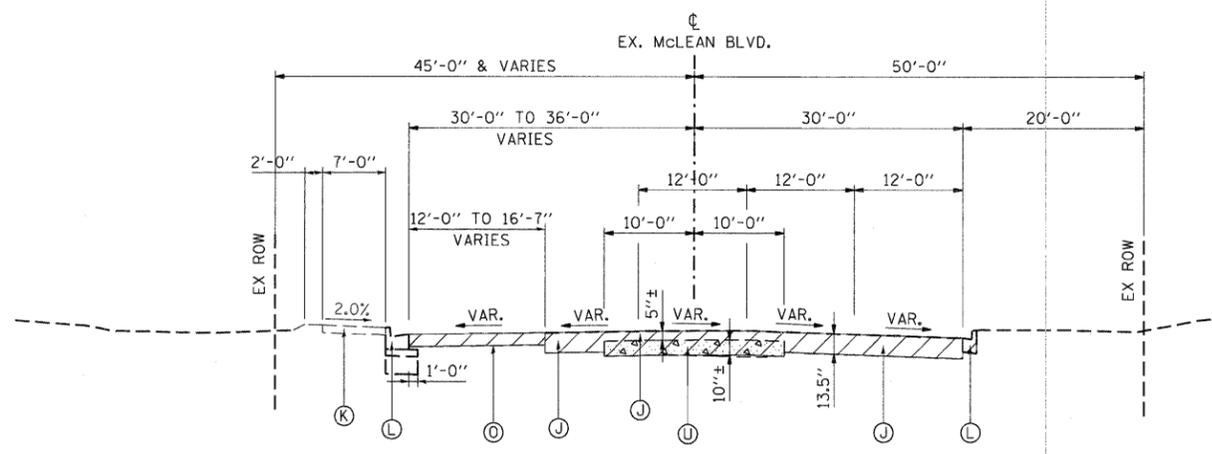
- (A) PCC PAVEMENT (10" AND VARIES)
- (B) HMA SURFACE (3" AND VARIES)
- (C) HMA SHOULDER (8" AND VARIES)
- (D) HMA SHOULDER (6" AND VARIES)
- (E) FENCE
- (F) CONCRETE MEDIAN BARRIER & BASE
- (G) PCC SHOULDER (10" AND VARIES)
- (H) TOPSOIL REMOVAL (X")
- (I) HMA BASE COURSE
- (J) HMA PAVEMENT
- (K) PCC SIDE WALK
- (L) CURB AND GUTTER
- (M) CONCRETE MEDIAN
- (N) STEEL PLATE BEAM GUARDRAIL
- (O) TEMP PAVEMENT
- (P) TEMPORARY CONCRETE BARRIER
- (R) RETAINING WALL (CAST-IN-PLACE)
- (S) TEMP. SOIL RETENTION SYSTEM
- (T) SUBBASE GRANULAR MATERIAL, TYPE C (10")
- (U) PCC PAVEMENT
- (V) PCC PAVEMENT (11" AND VARIES)
- (W) HMA SURFACE (4" AND VARIES)
- (X) HMA SURFACE (5" AND VARIES)
- (Y) HMA SURFACE (6" AND VARIES)
- (Z) CONC CURB TB
- (AA) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL

PROPOSED LEGEND

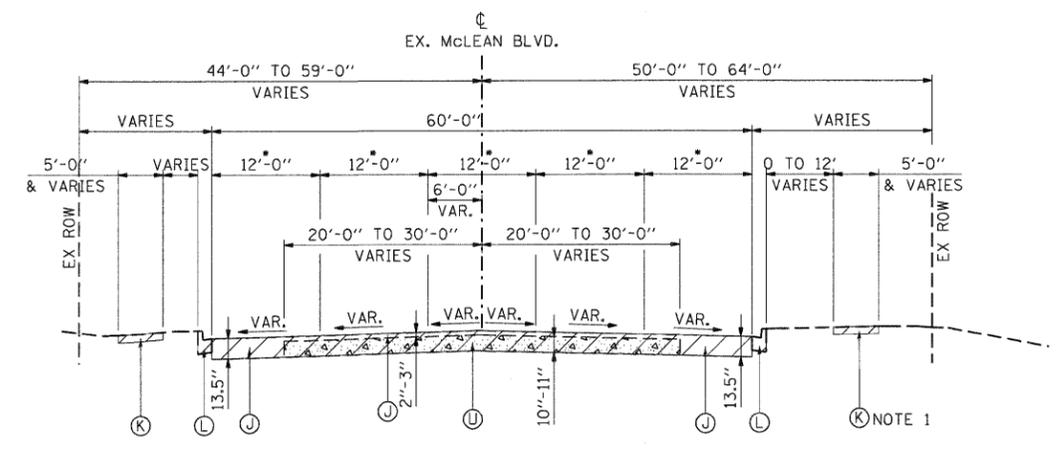
- ① CONTINUOUSLY REINFORCED CONCRETE PAVEMENT 10 3/4"
- ② PCC PVT 10 JOINTED
- ③ STABILIZED SUBBASE (HMA) 4 1/2"
- ④ AGGREGATE SUBGRADE 12"
- ⑤ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- ⑥ SUB-BASE GRANULAR MATERIAL, TYPE B 12"
- ⑦ CONCRETE BARRIER, DOUBLE FACE, 32" HEIGHT
- ⑧ CONCRETE BARRIER BASE
- ⑨ AGG WEDGE SHLD TYPE B
- ⑩ COMB CC&G TB6.12
- ⑪ COMB CC&G TB6.24
- ⑫ COMB CC&G TM4.24
- ⑬ CONC CURB TB SPL
- ⑭ PC CONC SIDEWALK 5
- ⑮ SPBGR TY A 6FT POSTS
- ⑯ EMBANKMENT
- ⑰ RETAINING WALL (SEE STRUCTURAL PLANS)
- ⑱ DRILL AND GROUT #6 TIE BARS
- ⑲ TOPSOIL FURNISH AND PLACE, 6"
- ⑳ TOPSOIL FURNISH AND PLACE, 10"
- ㉑ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 3/4"
- ㉒ HOT-MIX ASPHALT BASE COURSE, 8"
- ㉓ SHOULDER RUMBLE STRIPS, 16"
- ㉔ AGGREGATE SURFACE COURSE, TYPE B, 4"
- ㉕ CONCRETE MEDIAN SURFACE, 4 INCH
- ㉖ CONCRETE MEDIAN, TYPE SB (SPECIAL)
- ㉗ POLY. HMA SURFACE COURSE, MIX "F", N90 1 3/4"
- ㉘ CHAIN LINK FENCE, 4'
- ㉙ POLY. LEVELING BINDER (MM), IL-4.75, N50 3/4" (MIN)
- ㉚ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 3"
- ㉛ POLY. HMA SURFACE COURSE, MIX "F", N90 2"
- ㉜ COMB CC&G TB6.06
- ㉝ AGGREGATE SHOULDERS, TYPE B 4"
- ㉞ SEEDING (SEE LANDSCAPING PLANS)
- ㉟ PCC SHOULDERS 10 3/4" □
- ㊱ PCC SHOULDERS 10" □
- ㊲ AGGREGATE SHOULDERS, TYPE B 10" HEIGHT (SPECIAL)
- ㊳ CONCRETE BARRIER, SINGLE FACE 42" HEIGHT (SPECIAL)
- ㊴ CORRUGATED MEDIAN
- ㊵ HOT-MIX ASPHALT BASE COURSE, 7"
- ㊶ SODDING, SALT TOLERANT
- ㊷ GEOTECH FAB F/GR STAB □

FILE NAME =	USER NAME = #USER#	DESIGNED - MRK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION FAP ROUTE 345 / US ROUTE 20	PROPOSED TYPICAL SECTIONS U.S. ROUTE 20	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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PLOT DATE = #DATE#		DATE - 01/26/12	REVISED -			SCALE: 1"=10'	SHEET NO.	OF	STA. 108+35.00 TO STA. 140+24.00	FED. ROAD DIST. NO.	[ILLINOIS] FED. AID PROJECT

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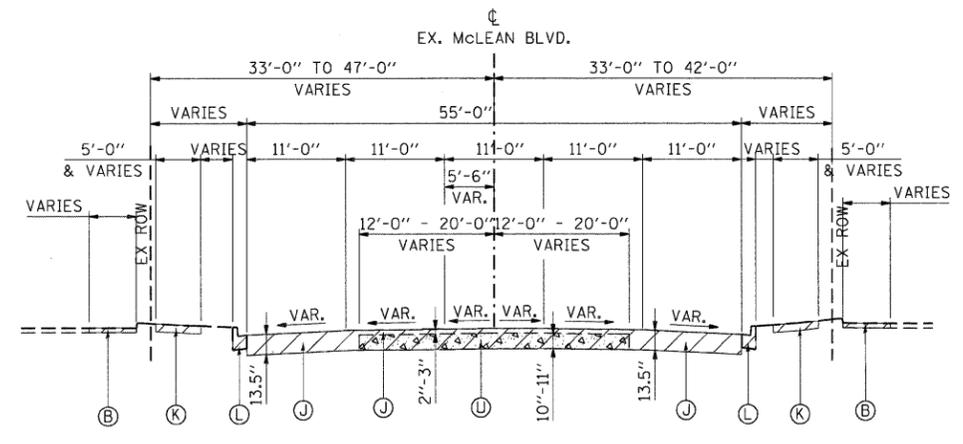
EXISTING TYPICAL SECTION
McLEAN BOULEVARD
 STA. 59+73.25 TO STA. 65+61.55



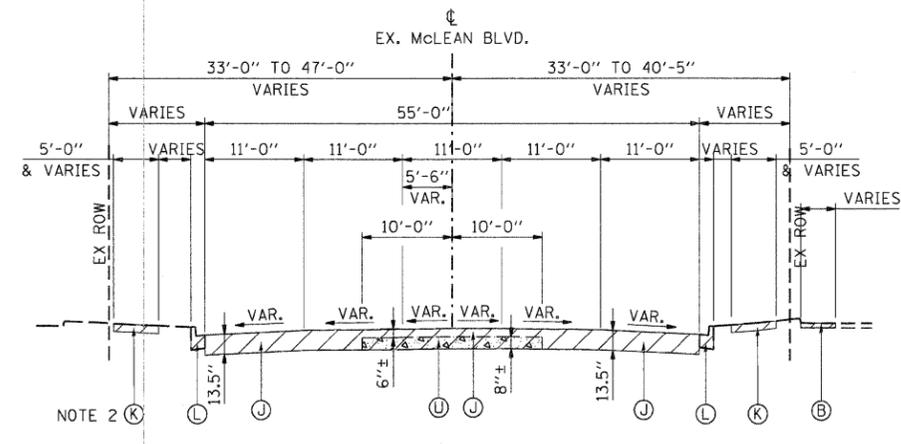
EXISTING TYPICAL SECTION
McLEAN BOULEVARD
 STA. 65+61.55 TO STA. 79+43

NOTE 1: NO SIDE WALK PRESENT BETWEEN STATIONS 69+14 AND 71+86 ON EAST SIDE OF ROAD.

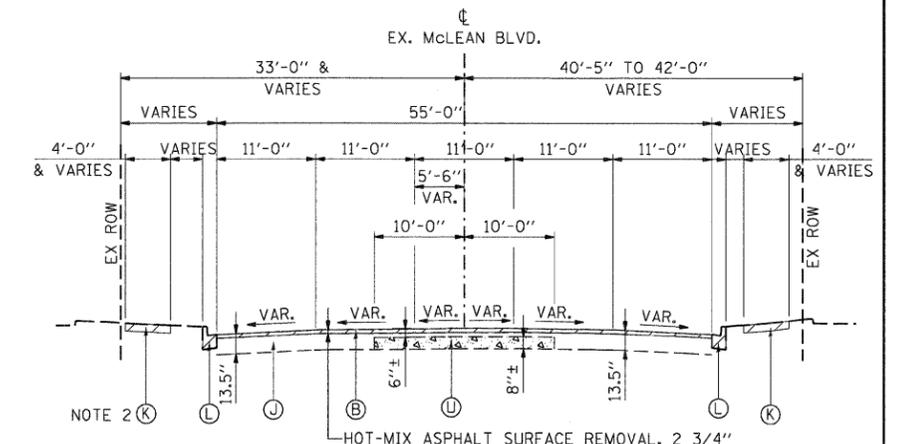
• STA. 77+43 TO STA. 79+43 - LANE WIDTHS TAPER DOWN FROM 12'-0" TO 11'-0"



EXISTING TYPICAL SECTION
McLEAN BOULEVARD
 STA. 79+43 TO STA. 82+36



EXISTING TYPICAL SECTION
McLEAN BOULEVARD
 STA. 82+36 TO STA. 86+70



EXISTING TYPICAL SECTION
McLEAN BOULEVARD
 STA. 86+70 TO STA. 89+00

NOTE 2: NO SIDE WALK PRESENT BETWEEN STATIONS 83+41 AND 86+89 ON WEST SIDE OF ROAD.

\D:\6845-XSCT1.DGN... \D:\6845-LEGEND.DGN... \D:\6845-SHEET\016845-SHT-TYPICAL.MXD... \D:\6845-SHEET\016845-SHT-TYPICAL.MXD... \D:\6845-SHEET\016845-SHT-TYPICAL.MXD...

NOTE 3: SEE GENERAL NOTES SHEET 3 FOR TEMPORARY PAVEMENT INFORMATION.

EXISTING LEGEND		REMOVAL
(A) PCC PAVEMENT (10" AND VARIES)	(K) PCC SIDE WALK	(U) PCC PAVEMENT
(B) HMA SURFACE (3" AND VARIES)	(L) CURB AND GUTTER	(V) PCC PAVEMENT (11" AND VARIES)
(C) HMA SHOULDER (8" AND VARIES)	(M) CONCRETE MEDIAN	(W) HMA SURFACE (4" AND VARIES)
(D) HMA SHOULDER (6" AND VARIES)	(N) STEEL PLATE BEAM GUARDRAIL	(X) HMA SURFACE (5" AND VARIES)
(E) FENCE	(O) TEMP PAVEMENT (SEE NOTE 3)	(Y) HMA SURFACE (6" AND VARIES)
(F) CONCRETE MEDIAN BARRIER & BASE	(P) TEMPORARY CONCRETE BARRIER	(Z) CONC CURB TB
(G) PCC SHOULDER (10" AND VARIES)	(R) RETAINING WALL (CAST-IN-PLACE)	(AA) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
(H) TOPSOIL REMOVAL (X")	(S) TEMP. SOIL RETENTION SYSTEM	
(I) HMA BASE COURSE	(T) SUBBASE GRANULAR MATERIAL, TYPE C (10")	
(J) HMA PAVEMENT		

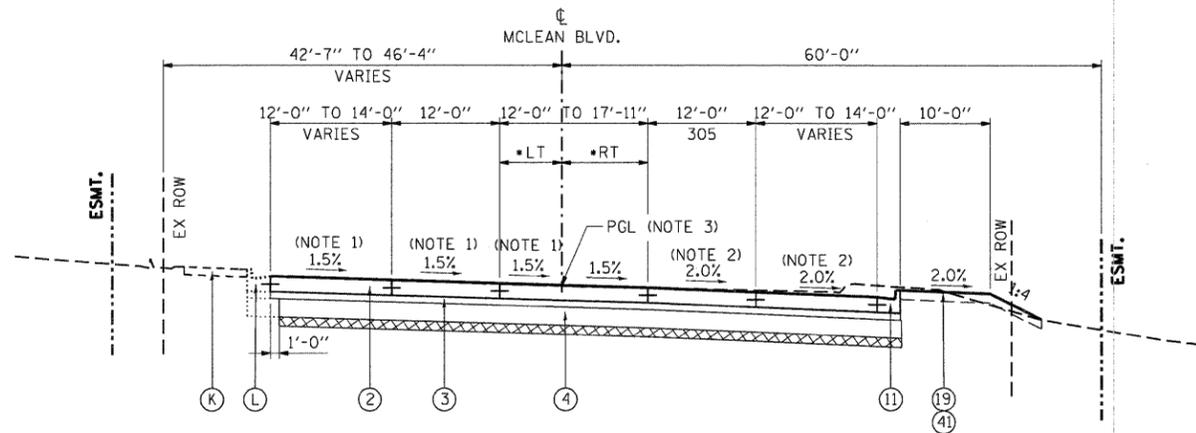
PROPOSED LEGEND		PGES
(1) CONTINUOUSLY REINFORCED CONCRETE PAVEMENT 10 3/4"	(10) COMB CC&G TB6.12	(33) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 3/4"
(2) PCC PVT 10 JOINTED PAVEMENT 10 3/4"	(11) COMB CC&G TB6.24	(34) AGGREGATE SHOULDERS, TYPE B 4"
(3) STABILIZED SUBBASE (HMA) 4 1/2"	(12) COMB CC&G TM4.24	(35) SEEDING (SEE LANDSCAPING PLANS)
(4) AGGREGATE SUBGRADE 12"	(13) CONC CURB TB SPL	(36) PCC SHOULDERS 10 3/4"
(5) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"	(14) PC CONC SIDEWALK 5	(37) PCC SHOULDERS 10"
(6) SUB-BASE GRANULAR MATERIAL, TYPE B 12"	(15) SPBGR TY A 6FT POSTS	(38) AGGREGATE SHOULDERS, TYPE B 10"
(7) CONCRETE BARRIER, DOUBLE FACE, 32" HEIGHT	(16) EMBANKMENT	(39) CONCRETE BARRIER, SINGLE FACE 42" HEIGHT (SPECIAL)
(8) CONCRETE BARRIER BASE	(17) RETAINING WALL (SEE STRUCTURAL PLANS)	(40) CORRUGATED MEDIAN
(9) AGG WEDGE SHLD TYPE B	(18) DRILL AND GROUT #6 TIE BARS	(41) HOT-MIX ASPHALT BASE COURSE, 7"
	(19) TOPSOIL FURNISH AND PLACE, 6"	(42) SODDING, SALT TOLERANT
	(20) TOPSOIL FURNISH AND PLACE, 10"	
		(21) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 3/4"
		(22) HOT-MIX ASPHALT BASE COURSE, 8"
		(23) SHOULDER RUMBLE STRIPS, 16"
		(24) AGGREGATE SURFACE COURSE, TYPE B, 4"
		(25) CONCRETE MEDIAN SURFACE, 4 INCH
		(26) CONCRETE MEDIAN, TYPE SB (SPECIAL)
		(27) POLY. HMA SURFACE COURSE, MIX "F", N90 1 3/4"
		(28) CHAIN LINK FENCE, 4'
		(29) POLY. LEVELING BINDER (MM), IL-4.75, N50 3/4" (MIN)
		(30) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 3"
		(31) POLY. HMA SURFACE COURSE, MIX "F", N90 2"
		(32) COMB CC&G TB6.06

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 FAP ROUTE 345 / US ROUTE 20

EXISTING TYPICAL SECTIONS			
McLEAN BOULEVARD			
SCALE:	SHEET NO.	OF	STA. 59+73.25 TO STA. 89+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	8R-R	KANE	794	30
CONTRACT NO. 60H45				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				



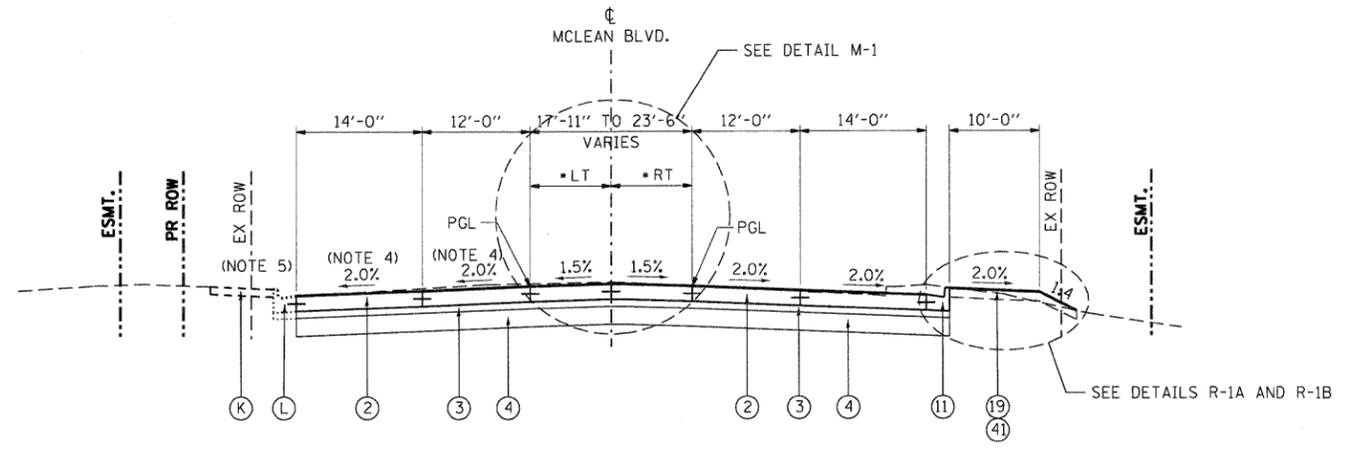


•LT - VARIES FROM 5'-8 1/2" TO 8'-7"
 •RT - VARIES FROM 6'-3 1/2" TO 9'-4"

McLEAN BLVD PGES LIMITS			
FROM STA	TO STA	WIDTH	THICKNESS
59+73	62+00	FULL WIDTH	6"

NOTE 1: SUPERELEVATION TRANSITION (1.5% TO -1.63%) FROM STA. 61+43.00 TO STA. 62+60.00
 NOTE 2: SUPERELEVATION TRANSITION (-1.5% TO -2.0%) FROM STA. 59+73.25 TO STA. 60+00.00
 NOTE 3: PGL SHIFTS FROM CENTERLINE TO INSIDE EDGE OF THRU LANES AT STA. 62+60.00

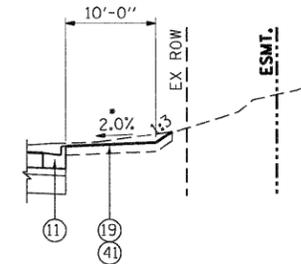
PROPOSED TYPICAL SECTION
 McLEAN BOULEVARD
 STA. 59+73.25 TO STA. 62+60.00



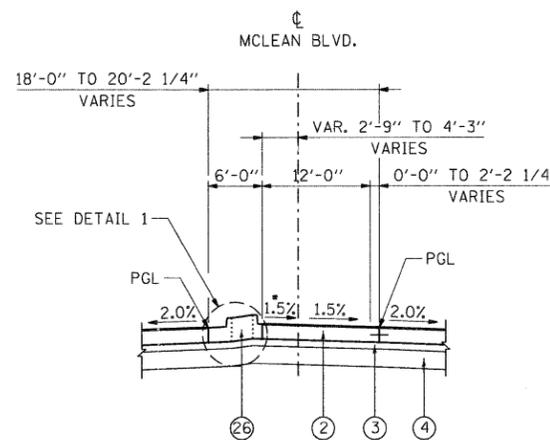
•LT - VARIES FROM 8'-7" TO 11'-9"
 •RT - VARIES FROM 9'-4" TO 11'-9"

NOTE 4: SUPERELEVATION TRANSITION (-1.63% TO -2.0%) FROM STA. 62+60.00 TO STA. 62+75.00
 NOTE 5: FROM STA. 63+27.65 TO STA. 65+52.50 - SEE INTERSECTION DETAILS

PROPOSED TYPICAL SECTION
 McLEAN BOULEVARD
 STA. 62+60.00 TO STA. 65+52.50

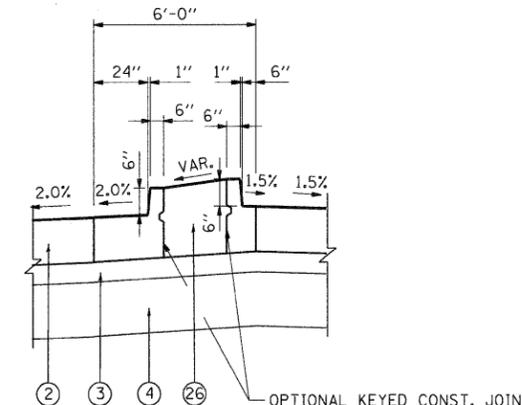


• TRANSITION -2.0% TO 2.0% FROM STA. 62+80 TO STA. 63+00
DETAIL R-1A
 STA. 63+00 TO STA. 64+10

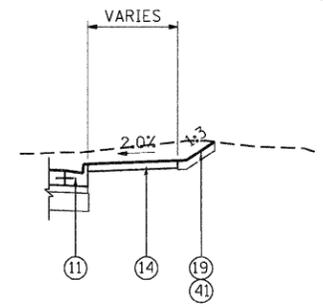


• USE 15' TRANSITION LENGTH PRIOR TO START OF CONCRETE MEDIAN TO MEET THIS SLOPE

DETAIL M-1
 STA. 62+72 TO STA. 64+00



DETAIL 1
 CONCRETE MEDIAN, TYPE SB (SPECIAL)



DETAIL R-1B
 STA. 64+10 TO STA. 65+52.50

NOTE: SEE PAVEMENT JOINTING PLANS FOR PAVEMENT JOINTS.

EXISTING LEGEND

- | | | |
|------------------------------------|---|-----------------------------------|
| (A) PCC PAVEMENT (10" AND VARIES) | (K) PCC SIDE WALK | (U) PCC PAVEMENT |
| (B) HMA SURFACE (3" AND VARIES) | (L) CURB AND GUTTER | (V) PCC PAVEMENT (11" AND VARIES) |
| (C) HMA SHOULDER (8" AND VARIES) | (M) CONCRETE MEDIAN | (W) HMA SURFACE (4" AND VARIES) |
| (D) HMA SHOULDER (6" AND VARIES) | (N) STEEL PLATE BEAM GUARDRAIL | (X) HMA SURFACE (5" AND VARIES) |
| (E) FENCE | (O) TEMP PAVEMENT (SEE NOTE 3) | (Y) HMA SURFACE (6" AND VARIES) |
| (F) CONCRETE MEDIAN BARRIER & BASE | (P) TEMPORARY CONCRETE BARRIER | (Z) CONC CURB TB |
| (G) PCC SHOULDER (10" AND VARIES) | (R) RETAINING WALL (CAST-IN-PLACE) | (AA) REMOVAL AND DISPOSAL |
| (H) TOPSOIL REMOVAL (X") | (S) TEMP. SOIL RETENTION SYSTEM | OF UNSUITABLE MATERIAL |
| (I) HMA BASE COURSE | (T) SUBBASE GRANULAR MATERIAL, TYPE C (10") | |
| (J) HMA PAVEMENT | | |

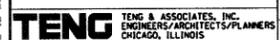
PROPOSED LEGEND

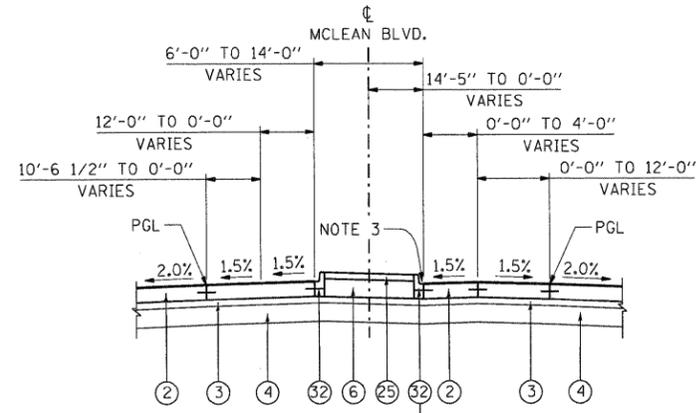
- | | |
|---|--|
| (1) CONTINUOUSLY REINFORCED CONCRETE PAVEMENT 10 3/4" | (10) COMB CC&G TB6.12 |
| (2) PCC PVT 10 JOINTED | (11) COMB CC&G TB6.24 |
| (3) STABILIZED SUBBASE (HMA) 4 1/2" | (12) COMB CC&G TM4.24 |
| (4) AGGREGATE SUBGRADE 12" | (13) CONC CURB TB SPL |
| (5) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2" | (14) PC CONC SIDEWALK 5 |
| (6) SUB-BASE GRANULAR MATERIAL, TYPE B 12" | (15) SPBCR TY A 6FT POSTS |
| (7) CONCRETE BARRIER, DOUBLE FACE, 32" HEIGHT | (16) EMBANKMENT |
| (8) CONCRETE BARRIER BASE | (17) RETAINING WALL (SEE STRUCTURAL PLANS) |
| (9) AGG WEDGE SHLD TYPE B | (18) DRILL AND GROUT #6 TIE BARS |
| | (19) TOPSOIL FURNISH AND PLACE, 6" |
| | (20) TOPSOIL FURNISH AND PLACE, 10" |

- | | |
|---|---|
| (21) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 3/4" | (33) AGGREGATE SHOULDERS, TYPE B 4" |
| (22) HOT-MIX ASPHALT BASE COURSE, 8" | (34) SEEDING (SEE LANDSCAPING PLANS) |
| (23) SHOULDER RUMBLE STRIPS, 16" | (35) PCC SHOULDERS 10 3/4" |
| (24) AGGREGATE SURFACE COURSE, TYPE B, 4" | (36) PCC SHOULDERS 10" |
| (25) CONCRETE MEDIAN SURFACE, 4 INCH | (37) AGGREGATE SHOULDERS, TYPE B 10" |
| (26) CONCRETE MEDIAN, TYPE SB (SPECIAL) | (38) CONCRETE BARRIER, SINGLE FACE 42" HEIGHT (SPECIAL) |
| (27) POLY. HMA SURFACE COURSE, MIX "F", N90 1 3/4" | (39) CORRUGATED MEDIAN |
| (28) CHAIN LINK FENCE, 4' | (40) HOT-MIX ASPHALT BASE COURSE, 7" |
| (29) POLY. LEVELING BINDER (MM), IL-4.75, N50 3/4" (MIN) | (41) SODDING, SALT TOLERANT |
| (30) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 3" | |
| (31) POLY. HMA SURFACE COURSE, MIX "F", N90 2" | |
| (32) COMB CC&G TB6.06 | |

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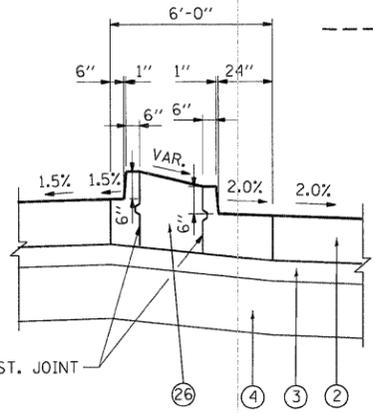
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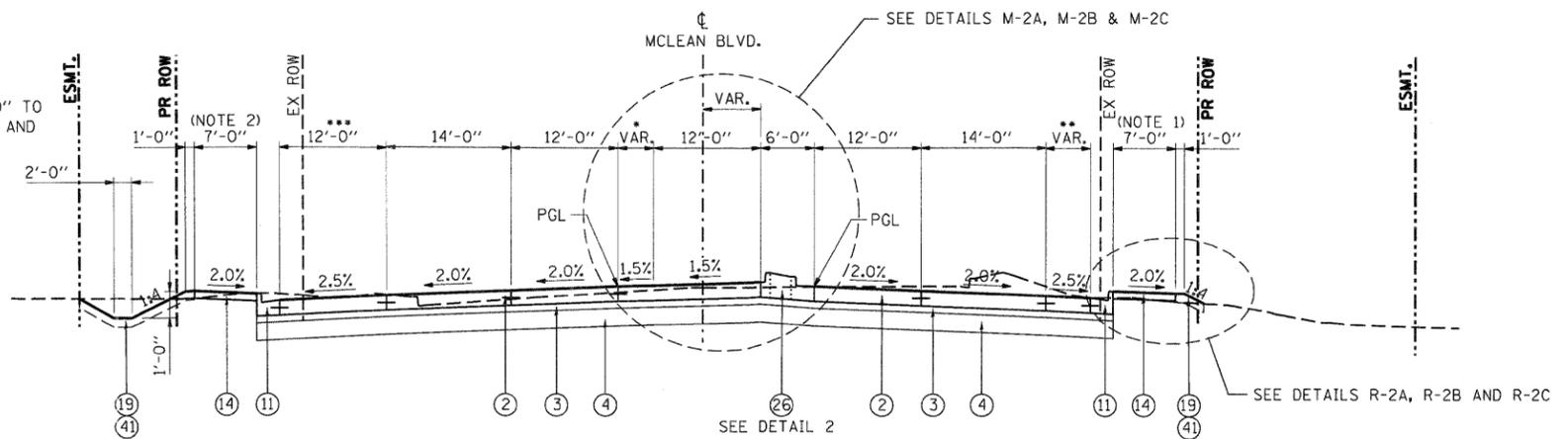


NOTE 3: GUTTER SLOPE TRANSITION (1.5% TO 6.0%) FROM STA. 68+97.00 TO STA. 69+10.00
 FROM STA. 67+70.50 TO STA. 67+96.56
DETAIL M-2A
 STA. 67+70.50 TO STA. 69+35.79

NOTE 2: SIDE WALK WIDTH VARIES 6'-0" TO 7'-0" BETWEEN STA. 67+57.81 AND STA. 68+82.84



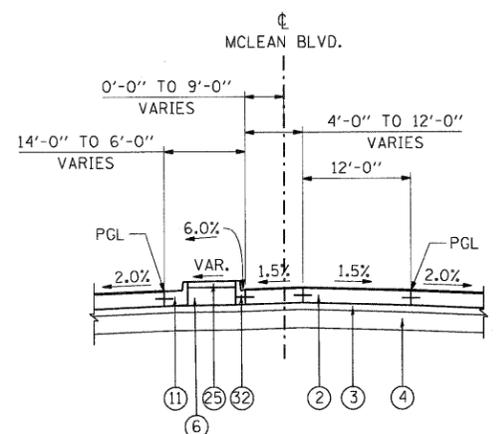
DETAIL 2
 CONCRETE MEDIAN, TYPE SB (SPECIAL)
 STA. 65+52.50 TO STA. 67+70.50



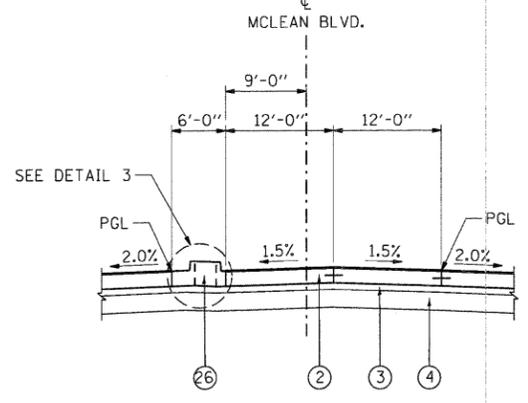
- STA. 65+52.50 TO STA. 67+70.50 - VARIES 5'-6 1/2" TO 10'-6 1/2"
- STA. 65+80 TO STA. 67+96 - N.B. RIGHT EXIT LANE VARIES 9'-1" TO 12'-0"
- STA. 67+96 TO STA. 71+41 - N.B. RIGHT EXIT LANE 12'
- STA. 71+41 TO STA. 72+34 - RAMP B: VARIES - SEE INTERSECTION DETAILS
- STA. 72+34 TO STA. 72+95 - RAMP B ISLAND: SEE DETAIL R-2B
- STA. 68+55 TO STA. 70+22.57 - S.B. RIGHT LANE TAPER VARIES 12'-0" TO 0'-0"
- STA. 70+22.57 TO STA. 72+59 - RAMP C: VARIES - SEE INTERSECTION DETAILS
- STA. 72+59 TO STA. 72+95 - RAMP C ISLAND: SEE DETAIL L-2

PROPOSED TYPICAL SECTION
 MCLEAN BOULEVARD
 STA. 65+52.50 TO STA. 72+95.50

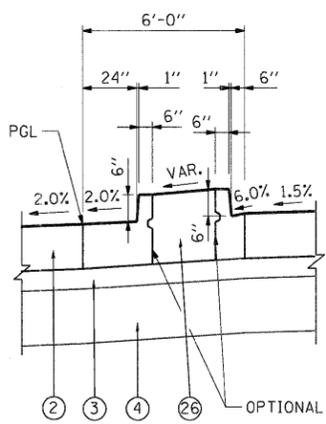
NOTE 1: SIDE WALK WIDTH IS 5'-0" FROM STA. 65+52.50 TO STA. 69+12.00 AND TRANSITION TO 7'-0" FROM STA. 69+12.00 TO STA. 69+22.00.



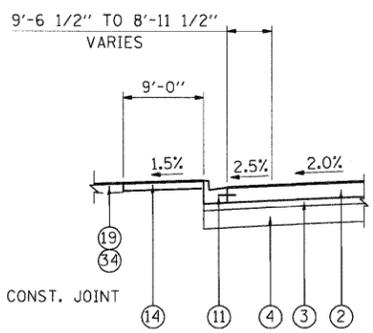
DETAIL M-2B
 STA. 69+35.79 TO STA. 70+45.24



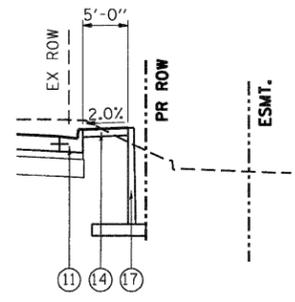
DETAIL M-2C
 STA. 70+45.24 TO STA. 72+95.50



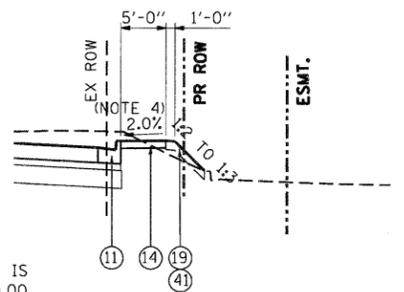
DETAIL 3
 CONCRETE MEDIAN, TYPE SB (SPECIAL)
 STA. 70+45.24 TO STA. 72+95.50



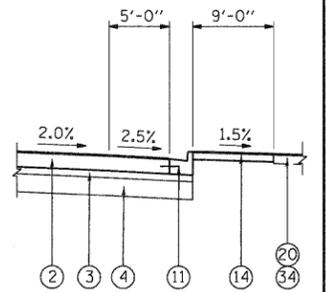
DETAIL L-2
 STA. 72+59.11 TO STA. 72+95.50



DETAIL R-2B
 STA. 68+25.66 TO STA. 68+99.35



DETAIL R-2A
 STA. 65+69.40 TO STA. 68+25.66



DETAIL R-2C
 STA. 72+36.20 TO STA. 72+95.50

NOTE: SEE PAVEMENT JOINTING PLANS FOR PAVEMENT JOINTS.

EXISTING LEGEND

- A PCC PAVEMENT (10" AND VARIES)
- B HMA SURFACE (3" AND VARIES)
- C HMA SHOULDER (8" AND VARIES)
- D HMA SHOULDER (6" AND VARIES)
- E FENCE
- F CONCRETE MEDIAN BARRIER & BASE
- G PCC SHOULDER (10" AND VARIES)
- H TOPSOIL REMOVAL (X")
- I HMA BASE COURSE
- J HMA PAVEMENT
- K PCC SIDE WALK
- L CURB AND GUTTER
- M CONCRETE MEDIAN
- N STEEL PLATE BEAM GUARDRAIL
- O TEMP PAVEMENT (SEE NOTE 3)
- P TEMPORARY CONCRETE BARRIER
- Q RETAINING WALL (CAST-IN-PLACE)
- R TEMP. SOIL RETENTION SYSTEM
- S SUBBASE GRANULAR MATERIAL, TYPE C (10")
- U PCC PAVEMENT
- V PCC PAVEMENT (11" AND VARIES)
- W HMA SURFACE (4" AND VARIES)
- X HMA SURFACE (5" AND VARIES)
- Y HMA SURFACE (6" AND VARIES)
- Z CONC CURB TB
- AA REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL

PROPOSED LEGEND

- 1 CONTINUOUSLY REINFORCED CONCRETE PAVEMENT 10 3/4"
- 2 PCC PVT 10 JOINTED
- 3 STABILIZED SUBBASE (HMA) 4 1/2"
- 4 AGGREGATE SUBGRADE 12"
- 5 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- 6 SUB-BASE GRANULAR MATERIAL, TYPE B 12"
- 7 CONCRETE BARRIER, DOUBLE FACE, 32" HEIGHT
- 8 CONCRETE BARRIER BASE
- 9 AGG WEDGE SHLD TYPE B
- 10 COMB CC&G TB6.12
- 11 COMB CC&G TB6.24
- 12 COMB CC&G TM4.24
- 13 CONC CURB TB SPL
- 14 PC CONC SIDEWALK 5
- 15 SPBRG TY A 6FT POSTS
- 16 EMBANKMENT
- 17 RETAINING WALL (SEE STRUCTURAL PLANS)
- 18 DRILL AND GROUT #6 TIE BARS
- 19 TOPSOIL FURNISH AND PLACE, 6"
- 20 TOPSOIL FURNISH AND PLACE, 10"

- 21 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 3/4"
- 22 HOT-MIX ASPHALT BASE COURSE, 8"
- 23 SHOULDER RUMBLE STRIPS, 16"
- 24 AGGREGATE SURFACE COURSE, TYPE B, 4"
- 25 CONCRETE MEDIAN SURFACE, 4 INCH
- 26 CONCRETE MEDIAN, TYPE SB (SPECIAL)
- 27 POLY. HMA SURFACE COURSE, MIX "F", N90 1 3/4"
- 28 CHAIN LINK FENCE, 4'
- 29 POLY. LEVELING BINDER (MM), IL-4.75, N50 3/4" (MIN)
- 30 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 3"
- 31 POLY. HMA SURFACE COURSE, MIX "F", N90 2"
- 32 COMB CC&G TB6.06
- 33 AGGREGATE SHOULDERS, TYPE B 4"
- 34 SEEDING (SEE LANDSCAPING PLANS)
- 35 PCC SHOULDERS 10 3/4"
- 36 PCC SHOULDERS 10"
- 37 AGGREGATE SHOULDERS, TYPE B 10"
- 38 CONCRETE BARRIER, SINGLE FACE 42" HEIGHT (SPECIAL)
- 39 CORRUGATED MEDIAN
- 40 HOT-MIX ASPHALT BASE COURSE, 7"
- 41 SODDING, SALT TOLERANT

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 CHECKED - DDH
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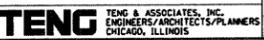
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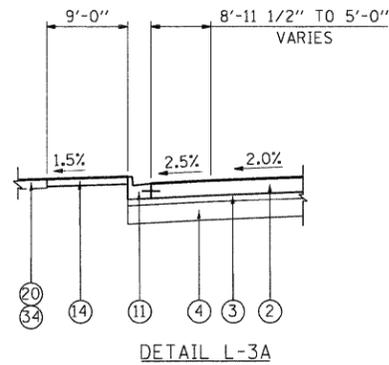
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 FAP ROUTE 345 / US ROUTE 20

PROPOSED TYPICAL SECTIONS
 MCLEAN BOULEVARD
 SCALE: SHEET NO. OF STA. 65+52.50 TO STA. 72+95.50

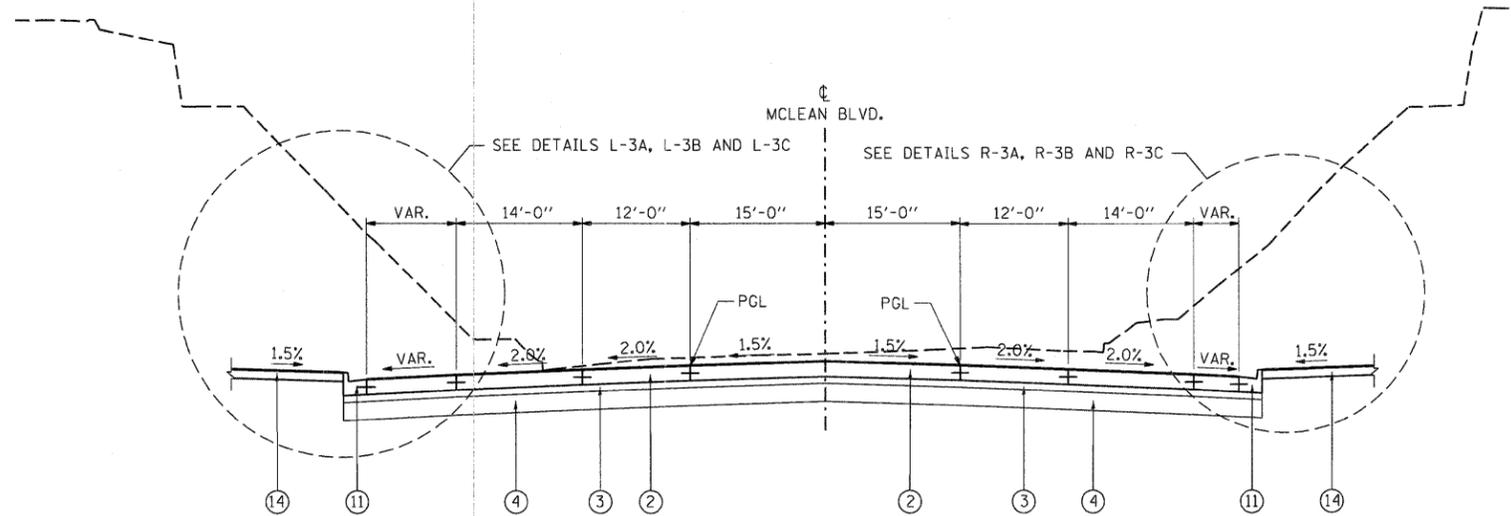
F.A.P. RTE. 345 SECTION 8R-R COUNTY KANE TOTAL SHEETS 794 SHEET NO. 32 CONTRACT NO. 60H45
 FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT

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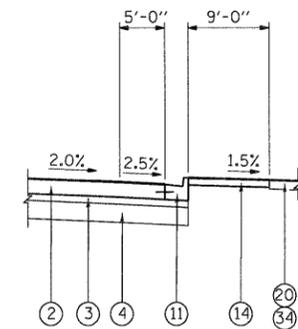




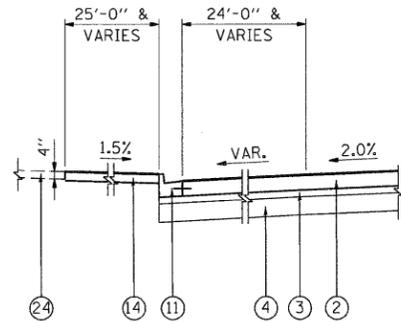
DETAIL L-3A
SB STA. 72+95.50 TO STA. 73+96.20



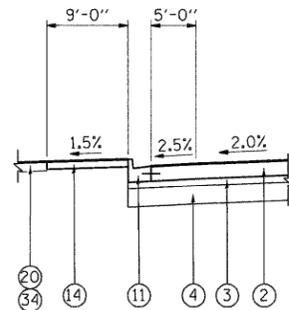
PROPOSED TYPICAL SECTION
McLEAN BOULEVARD
STA. 72+95.50 TO STA. 75+93.96



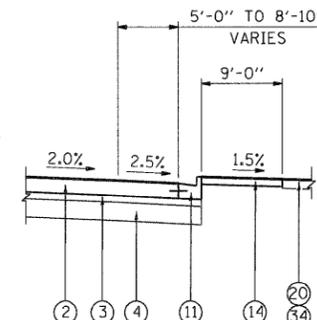
DETAIL R-3A
SB STA. 72+95.50 TO STA. 73+63.69



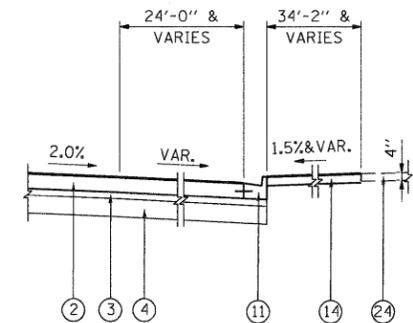
DETAIL L-3B
SB STA. 73+96.20 TO STA. 75+34.48



DETAIL L-3C
SB STA. 75+34.48 TO STA. 75+93.96



DETAIL R-3C
SB STA. 74+92.31 TO STA. 75+93.96



DETAIL R-3B
NB STA. 73+63.69 TO STA. 74+92.31

NOTE: SEE PAVEMENT JOINTING PLANS FOR PAVEMENT JOINTS.

EXISTING LEGEND

- | | | |
|------------------------------------|---|--|
| (A) PCC PAVEMENT (10" AND VARIES) | (K) PCC SIDE WALK | (U) PCC PAVEMENT |
| (B) HMA SURFACE (3" AND VARIES) | (L) CURB AND GUTTER | (V) PCC PAVEMENT (11" AND VARIES) |
| (C) HMA SHOULDER (8" AND VARIES) | (M) CONCRETE MEDIAN | (W) HMA SURFACE (4" AND VARIES) |
| (D) HMA SHOULDER (6" AND VARIES) | (N) STEEL PLATE BEAM GUARDRAIL | (X) HMA SURFACE (5" AND VARIES) |
| (E) FENCE | (O) TEMP PAVEMENT (SEE NOTE 3) | (Y) HMA SURFACE (6" AND VARIES) |
| (F) CONCRETE MEDIAN BARRIER & BASE | (P) TEMPORARY CONCRETE BARRIER | (Z) CONC CURB TB |
| (G) PCC SHOULDER (10" AND VARIES) | (R) RETAINING WALL (CAST-IN-PLACE) | (AA) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL |
| (H) TOPSOIL REMOVAL (X") | (S) TEMP. SOIL RETENTION SYSTEM | |
| (I) HMA BASE COURSE | (T) SUBBASE GRANULAR MATERIAL, TYPE C (10") | |
| (J) HMA PAVEMENT | | |

PROPOSED LEGEND

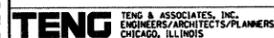
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| (1) CONTINUOUSLY REINFORCED CONCRETE PAVEMENT 10 3/4" | (10) COMB CC&G TB6.12 |
| (2) PCC PVT 10" JOINTED PAVEMENT 10 3/4" | (11) COMB CC&G TB6.24 |
| (3) STABILIZED SUBBASE (HMA) 4 1/2" | (12) COMB CC&G TM4.24 |
| (4) AGGREGATE SUBGRADE 12" | (13) CONC CURB TB SPL |
| (5) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2" | (14) PC CONC SIDEWALK 5 |
| (6) SUB-BASE GRANULAR MATERIAL, TYPE B 12" | (15) SPBGR TY A 6FT POSTS |
| (7) CONCRETE BARRIER, DOUBLE FACE, 32" HEIGHT | (16) EMBANKMENT |
| (8) CONCRETE BARRIER BASE | (17) RETAINING WALL (SEE STRUCTURAL PLANS) |
| (9) AGG WEDGE SHLD TYPE B | (18) DRILL AND GROUT #6 TIE BARS |
| | (19) TOPSOIL FURNISH AND PLACE, 6" |
| | (20) TOPSOIL FURNISH AND PLACE, 10" |

- | | |
|---|---|
| (21) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 3/4" | (33) AGGREGATE SHOULDERS, TYPE B 4" |
| (22) HOT-MIX ASPHALT BASE COURSE, 8" | (34) SEEDING (SEE LANDSCAPING PLANS) |
| (23) SHOULDER RUMBLE STRIPS, 16" | (35) PCC SHOULDERS 10 3/4" |
| (24) AGGREGATE SURFACE COURSE, TYPE B, 4" | (36) PCC SHOULDERS 10" |
| (25) CONCRETE MEDIAN SURFACE, 4 INCH | (37) AGGREGATE SHOULDERS, TYPE B 10" |
| (26) CONCRETE MEDIAN, TYPE SB (SPECIAL) | (38) CONCRETE BARRIER, SINGLE FACE 42" HEIGHT (SPECIAL) |
| (27) POLY. HMA SURFACE COURSE, MIX "F", N90 1 3/4" | (39) CORRUGATED MEDIAN |
| (28) CHAIN LINK FENCE, 4' | (40) HOT-MIX ASPHALT BASE COURSE, 7" |
| (29) POLY. LEVELING BINDER (MM), IL-4.75, N50 3/4" (MIN) | (41) SODDING, SALT TOLERANT |
| (30) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 3" | |
| (31) POLY. HMA SURFACE COURSE, MIX "F", N90 2" | |
| (32) COMB CC&G TB6.06 | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
FAP ROUTE 345 / US ROUTE 20

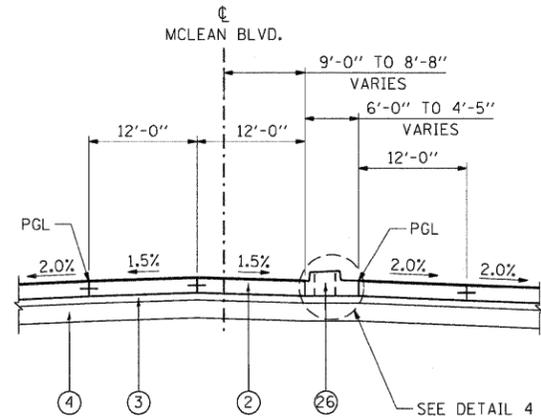
PROPOSED TYPICAL SECTIONS
McLEAN BOULEVARD

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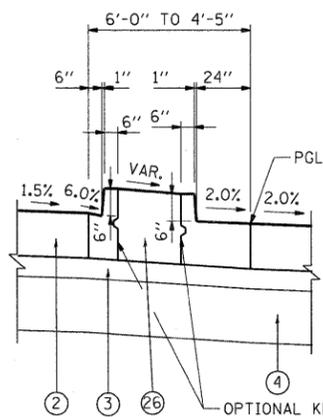


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	8R-R	KANE	794	33
CONTRACT NO. 60H45				
SCALE:	SHEET NO.	OF	STA. 72+95.50 TO STA. 75+93.96	FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT

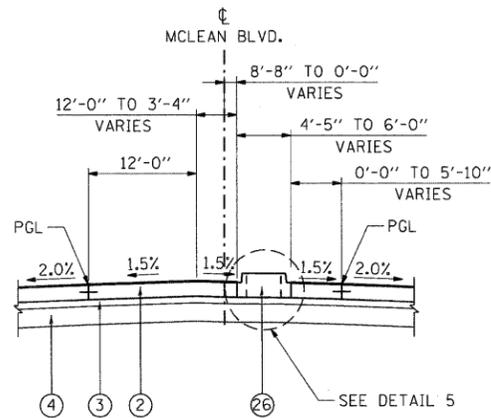
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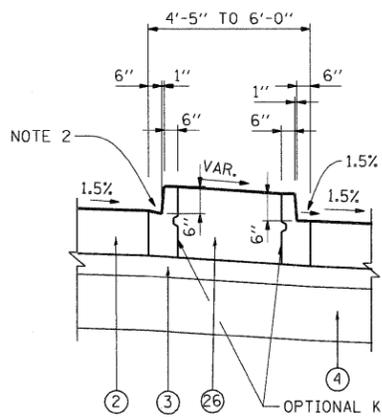
DETAIL M-4A
STA. 75+93.96 TO STA. 78+21.58



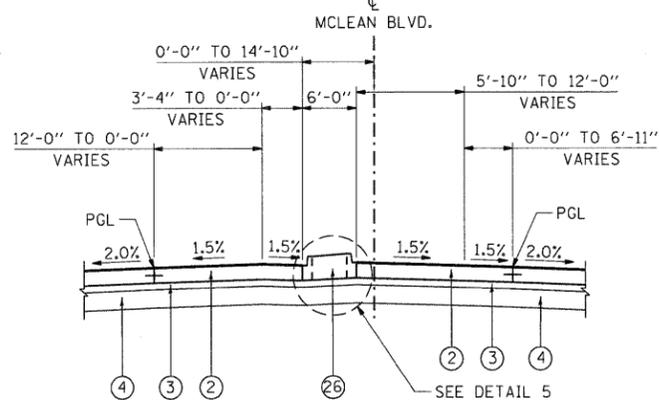
DETAIL 4
CONCRETE MEDIAN, TYPE SB (SPECIAL)
STA. 75+93.96 TO STA. 78+21.58



DETAIL M-4B
STA. 78+21.58 TO STA. 78+98.91



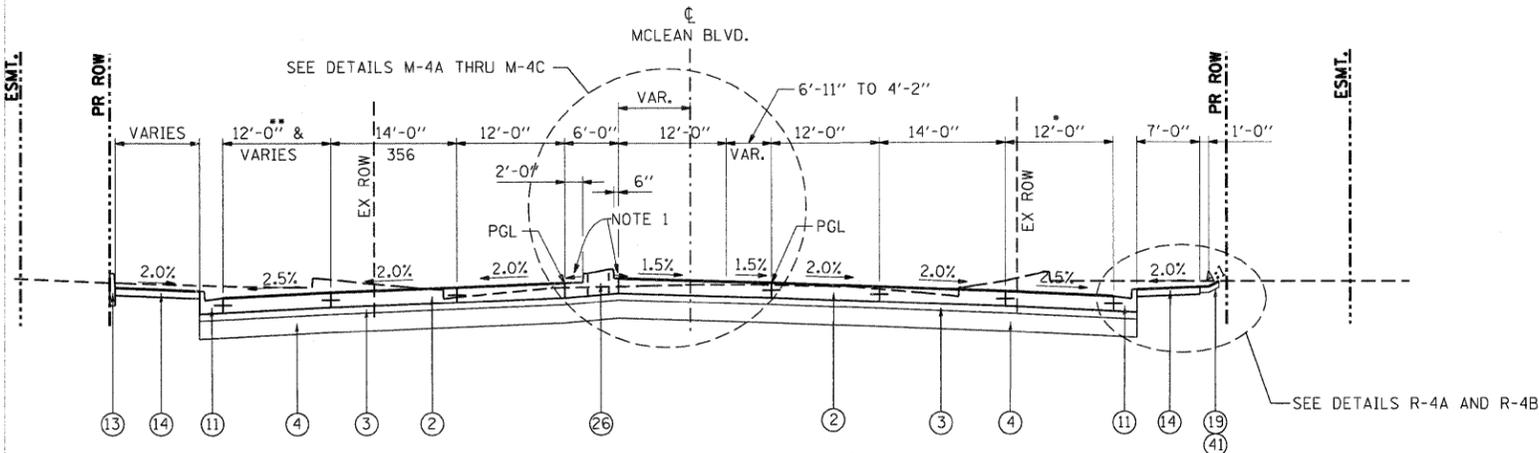
DETAIL L-4
STA. 75+93.96 TO STA. 76+48.62



DETAIL M-4C
STA. 78+98.91 TO STA. 80+35.38

NOTE 2: GUTTER SLOPE IS -6.0% FROM STA. 78+21.58 TO STA. 79+28.29 AND TRANSITION (-6.0% TO 1.5%) FROM STA. 79+28.29 TO 79+40.00

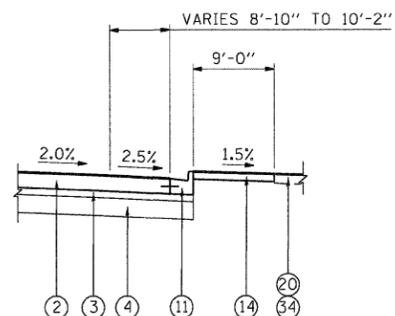
DETAIL 5
CONCRETE MEDIAN, TYPE SB (SPECIAL)
STA. 78+21.58 TO STA. 80+35.38



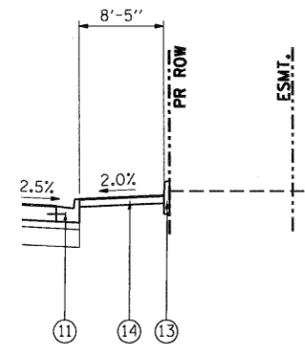
PROPOSED TYPICAL SECTION
MCLEAN BOULEVARD
STA. 75+93.96 TO STA. 82+06.25

NOTE 1: SLOPES TO MATCH ADJACENT PAVEMENT SLOPES

- STA. 75+94 TO STA. 76+19 - RAMP A ISLAND: SEE DETAIL R-4A
- STA. 76+19 TO STA. 77+54 - RAMP A: VARIES - SEE INTERSECTION DETAILS
- STA. 77+54 TO STA. 79+04 - N.B. RIGHT EXIT LANE TAPER VARIES 0'-0" TO 12'-0"
- STA. 75+94 TO STA. 76+51 - RAMP D ISLAND: SEE DETAIL L-4



DETAIL R-4A
STA. 75+93.96 TO STA. 76+17.30



DETAIL R-4B
STA. 78+34.00 TO STA. 82+06.25

NOTE: SEE PAVEMENT JOINTING PLANS FOR PAVEMENT JOINTS.

EXISTING LEGEND

- (A) PCC PAVEMENT (10" AND VARIES)
- (B) HMA SURFACE (3" AND VARIES)
- (C) HMA SHOULDER (8" AND VARIES)
- (D) HMA SHOULDER (6" AND VARIES)
- (E) FENCE
- (F) CONCRETE MEDIAN BARRIER & BASE
- (G) PCC SHOULDER (10" AND VARIES)
- (H) TOPSOIL REMOVAL (X")
- (I) HMA BASE COURSE
- (J) HMA PAVEMENT
- (K) PCC SIDE WALK
- (L) CURB AND GUTTER
- (M) CONCRETE MEDIAN
- (N) STEEL PLATE BEAM GUARDRAIL
- (O) TEMP PAVEMENT (SEE NOTE 3)
- (P) TEMPORARY CONCRETE BARRIER
- (R) RETAINING WALL (CAST-IN-PLACE)
- (S) TEMP. SOIL RETENTION SYSTEM
- (T) SUBBASE GRANULAR MATERIAL, TYPE C (10')
- (U) PCC PAVEMENT
- (V) PCC PAVEMENT (11" AND VARIES)
- (W) HMA SURFACE (4" AND VARIES)
- (X) HMA SURFACE (5" AND VARIES)
- (Y) HMA SURFACE (6" AND VARIES)
- (Z) CONC CURB TB
- (AA) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL

PROPOSED LEGEND

- (1) CONTINUOUSLY REINFORCED CONCRETE PAVEMENT 10 3/4"
- (2) PCC PVT 10' JOINTED
- (3) STABILIZED SUBBASE (HMA) 4 1/2"
- (4) AGGREGATE SUBGRADE 12"
- (5) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- (6) SUB-BASE GRANULAR MATERIAL, TYPE B 12"
- (7) CONCRETE BARRIER, DOUBLE FACE, 32" HEIGHT
- (8) CONCRETE BARRIER BASE
- (9) AGG WEDGE SHLD TYPE B
- (10) COMB CC&G TB6.12
- (11) COMB CC&G TB6.24
- (12) COMB CC&G TM4.24
- (13) CONC CURB TB SPL
- (14) PC CONC SIDEWALK 5
- (15) SPBGR TY A 6FT POSTS
- (16) EMBANKMENT
- (17) RETAINING WALL (SEE STRUCTURAL PLANS)
- (18) DRILL AND GROUT #6 TIE BARS
- (19) TOPSOIL FURNISH AND PLACE, 6"
- (20) TOPSOIL FURNISH AND PLACE, 10"
- (21) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 3/4"
- (22) HOT-MIX ASPHALT BASE COURSE, 8"
- (23) SHOULDER RUMBLE STRIPS, 16"
- (24) AGGREGATE SURFACE COURSE, TYPE B, 4"
- (25) CONCRETE MEDIAN SURFACE, 4 INCH
- (26) CONCRETE MEDIAN, TYPE SB (SPECIAL)
- (27) POLY. HMA SURFACE COURSE, MIX "F", N90 1 3/4"
- (28) CHAIN LINK FENCE, 4'
- (29) POLY. LEVELING BINDER (MM), IL-4.75, N50 3/4" (MIN)
- (30) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 3"
- (31) POLY. HMA SURFACE COURSE, MIX "F", N90 2"
- (32) COMB CC&G TB6.06
- (33) AGGREGATE SHOULDERS, TYPE B 4"
- (34) SEEDING (SEE LANDSCAPING PLANS)
- (35) PCC SHOULDERS 10 3/4"
- (36) PCC SHOULDERS 10"
- (37) AGGREGATE SHOULDERS, TYPE B 10"
- (38) CONCRETE BARRIER, SINGLE FACE 42" HEIGHT (SPECIAL)
- (39) CORRUGATED MEDIAN
- (40) HOT-MIX ASPHALT BASE COURSE, 7"
- (41) SODDING, SALT TOLERANT

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
FAP ROUTE 345 / US ROUTE 20

PROPOSED TYPICAL SECTIONS
MCLEAN BOULEVARD

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USER NAME = #USER#
DESIGNED - MRK
DRAWN - MRK
CHECKED - DDH
DATE - 12/16/11

PLOT SCALE = #SCALE#
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SCALE: SHEET NO. OF STA. 75+93.96 TO STA. 82+06.25

F.A.P. RTE. 345 SECTION 8R-R COUNTY KANE TOTAL SHEETS 794 SHEET NO. 34 CONTRACT NO. 60H45

FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT

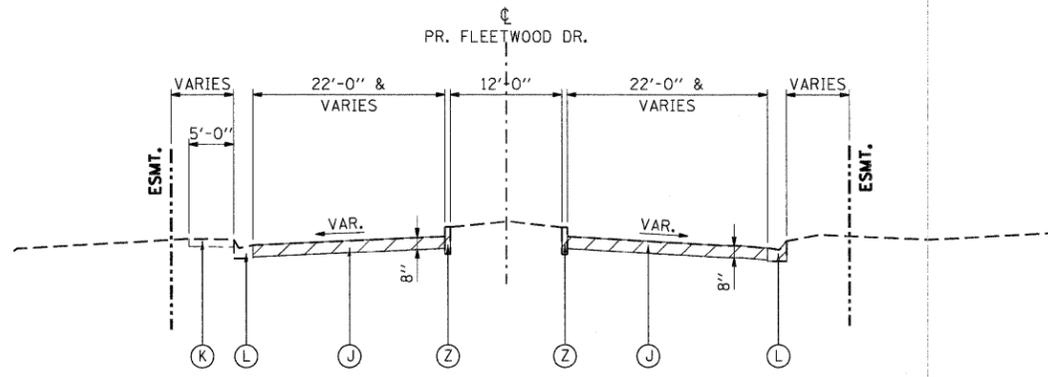
TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
CHICAGO, ILLINOIS

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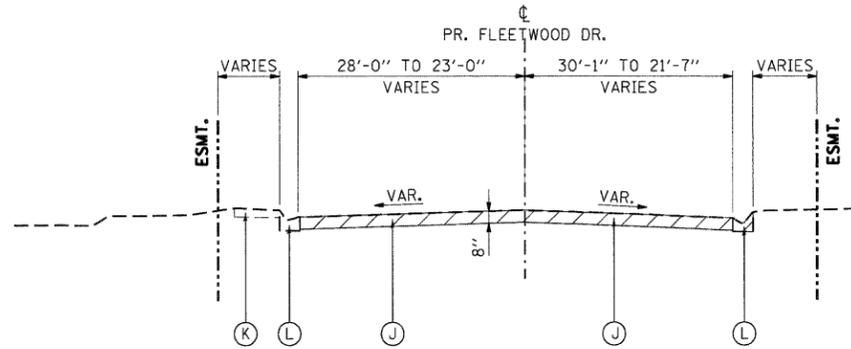
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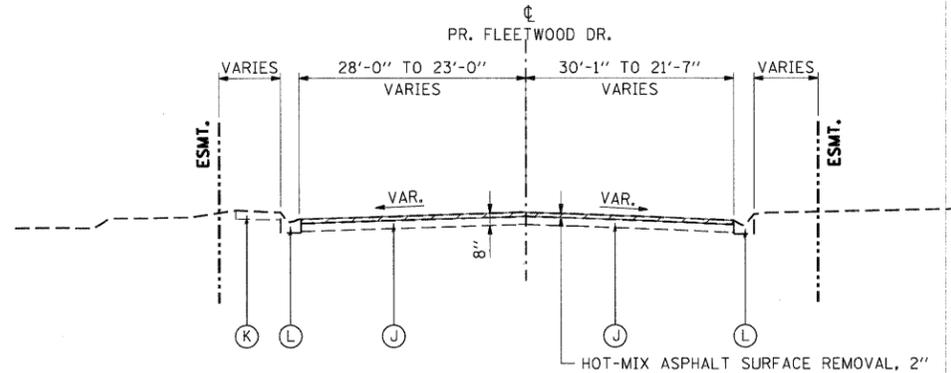
EXISTING TYPICAL SECTION
FLEETWOOD DR.

STA. 310+50.00 TO STA. 311+77.68



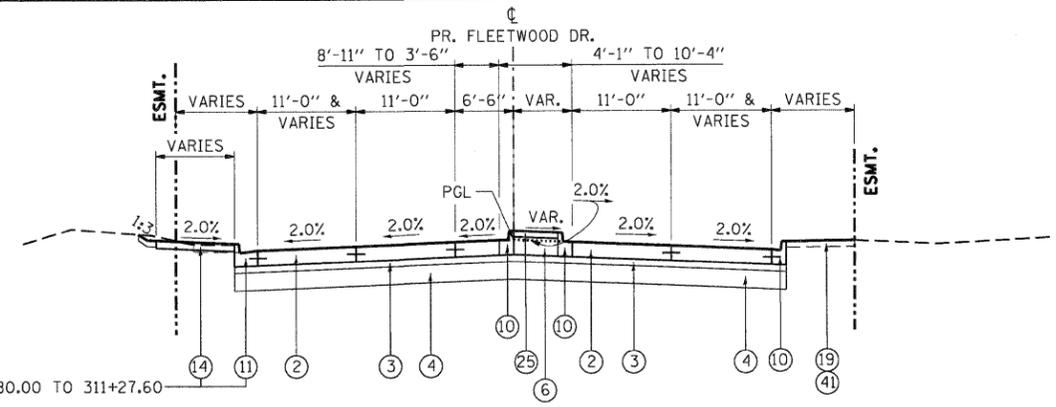
EXISTING TYPICAL SECTION
FLEETWOOD DR.

STA. 311+77.68 TO STA. 312+44.84



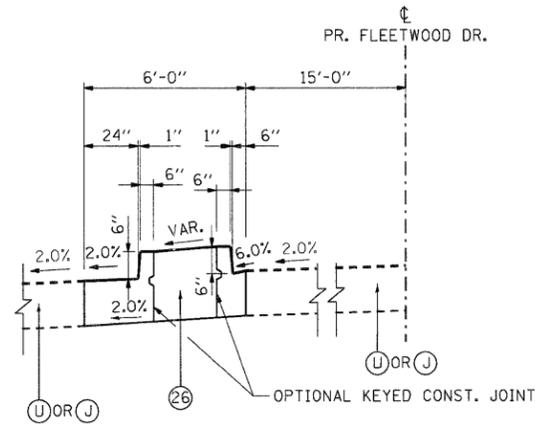
EXISTING TYPICAL SECTION
FLEETWOOD DR.

STA. 312+44.84 TO STA. 312+64.84



PROPOSED TYPICAL SECTION
FLEETWOOD DR.

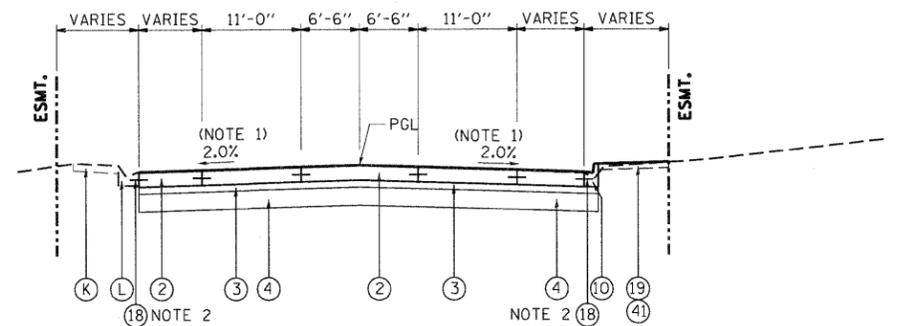
STA. 310+88.40 TO STA. 311+76.74



PROPOSED TYPICAL SECTION

CONCRETE MEDIAN, TYPE SB (SPECIAL)

STA. 307+31.89 TO STA. 309+38.00

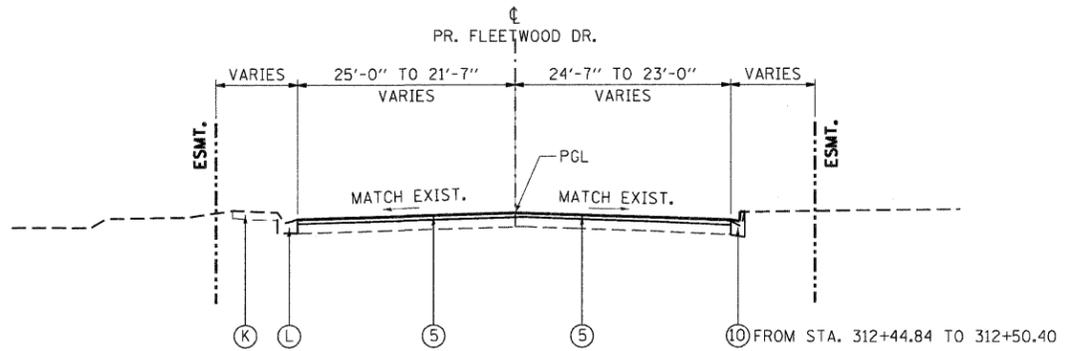


PROPOSED TYPICAL SECTION
FLEETWOOD DR.

STA. 311+76.74 TO STA. 312+44.84

NOTE 1: CROSS SLOPE TRANSITION (-2.00% TO -1.50%) FROM STA. 312+32.00 TO STA. 312+44.84

NOTE 2: SPACE AT 24" CENTER-TO-CENTER



PROPOSED TYPICAL SECTION
FLEETWOOD DR.

STA. 312+44.84 TO STA. 312+64.84

NOTE: SEE PAVEMENT JOINTING PLANS FOR PAVEMENT JOINTS.

EXISTING LEGEND

- (A) PCC PAVEMENT (10" AND VARIES)
- (B) HMA SURFACE (3" AND VARIES)
- (C) HMA SHOULDER (8" AND VARIES)
- (D) HMA SHOULDER (6" AND VARIES)
- (E) FENCE
- (F) CONCRETE MEDIAN BARRIER & BASE
- (G) PCC SHOULDER (10" AND VARIES)
- (H) TOPSOIL REMOVAL (X")
- (I) HMA BASE COURSE
- (J) HMA PAVEMENT
- (K) PCC SIDE WALK
- (L) CURB AND GUTTER
- (M) CONCRETE MEDIAN
- (N) STEEL PLATE BEAM GUARDRAIL
- (O) TEMP PAVEMENT (SEE NOTE 3)
- (P) TEMPORARY CONCRETE BARRIER
- (Q) RETAINING WALL (CAST-IN-PLACE)
- (R) TEMP. SOIL RETENTION SYSTEM
- (S) SUBBASE GRANULAR MATERIAL, TYPE C (10")
- (U) PCC PAVEMENT
- (V) PCC PAVEMENT (11" AND VARIES)
- (W) HMA SURFACE (4" AND VARIES)
- (X) HMA SURFACE (5" AND VARIES)
- (Y) HMA SURFACE (6" AND VARIES)
- (Z) CONC CURB TB
- (AA) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL

PROPOSED LEGEND

- (1) CONTINUOUSLY REINFORCED CONCRETE PAVEMENT 10 3/4"
- (2) PCC PVT 10 JOINTED
- (3) STABILIZED SUBBASE (HMA) 4 1/2"
- (4) AGGREGATE SUBGRADE 12"
- (5) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- (6) SUB-BASE GRANULAR MATERIAL, TYPE B 12"
- (7) CONCRETE BARRIER, DOUBLE FACE, 32" HEIGHT
- (8) CONCRETE BARRIER BASE
- (9) AGG WEDGE SHLD TYPE B
- (10) COMB CC&G TB6.12
- (11) COMB CC&G TB6.24
- (12) COMB CC&G TM4.24
- (13) CONC CURB TB SPL
- (14) PC CONC SIDEWALK 5
- (15) SPBGR TY A 6FT POSTS
- (16) EMBANKMENT
- (17) RETAINING WALL (SEE STRUCTURAL PLANS)
- (18) DRILL AND GROUT #6 TIE BARS
- (19) TOPSOIL FURNISH AND PLACE, 6"
- (20) TOPSOIL FURNISH AND PLACE, 10"

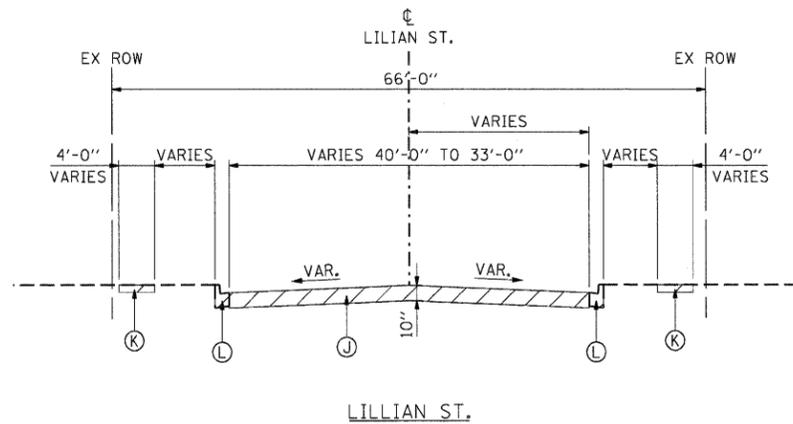
- (21) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 3/4"
- (22) HOT-MIX ASPHALT BASE COURSE, 8"
- (23) SHOULDER RUMBLE STRIPS, 16"
- (24) AGGREGATE SURFACE COURSE, TYPE B, 4"
- (25) CONCRETE MEDIAN SURFACE, 4 INCH
- (26) CONCRETE MEDIAN, TYPE SB (SPECIAL)
- (27) POLY. HMA SURFACE COURSE, MIX "F", N90 1 3/4"
- (28) CHAIN LINK FENCE, 4'
- (29) POLY. LEVELING BINDER (MM), IL-4.75, N50 3/4" (MIN)
- (30) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 3"
- (31) POLY. HMA SURFACE COURSE, MIX "F", N90 2"
- (32) COMB CC&G TB6.06
- (33) AGGREGATE SHOULDERS, TYPE B 4"
- (34) SEEDING (SEE LANDSCAPING PLANS)
- (35) PCC SHOULDERS 10 3/4"
- (36) PCC SHOULDERS 10"
- (37) AGGREGATE SHOULDERS, TYPE B 10"
- (38) CONCRETE BARRIER, SINGLE FACE 42" HEIGHT (SPECIAL)
- (39) CORRUGATED MEDIAN
- (40) HOT-MIX ASPHALT BASE COURSE, 7"
- (41) SODDING, SALT TOLERANT

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
FAP ROUTE 345 / US ROUTE 20

EXISTING AND PROPOSED TYPICAL SECTIONS
FLEETWOOD DRIVE

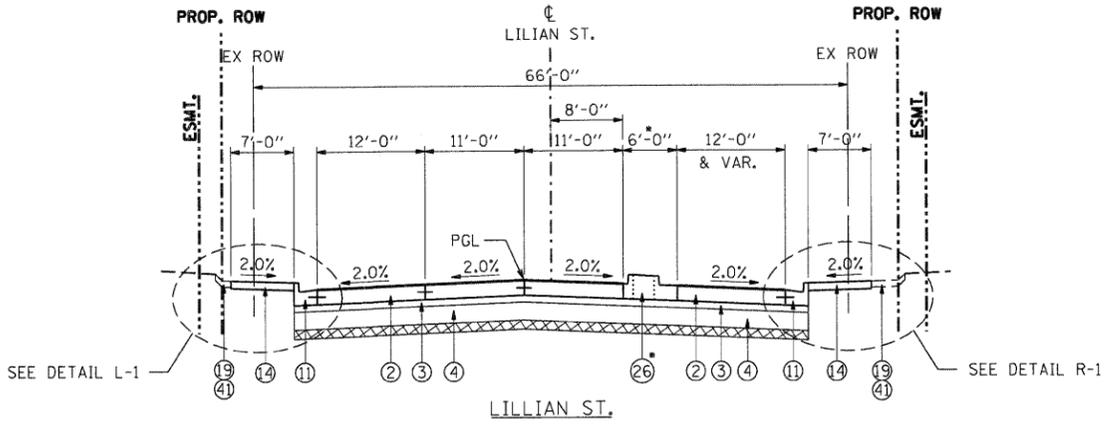
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\$FILEL\$		DRAWN - MRK	REVISED -						
		CHECKED - DDH	REVISED -						
		DATE - 12/16/11	REVISED -						
					CONTRACT NO. 60H45				

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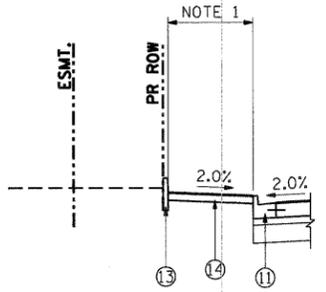
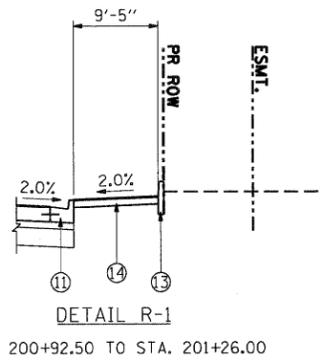


LILLIAN ST. PGES LIMITS

FROM STA	TO STA	WIDTH	THICKNESS
200+40	202+50	FULL WIDTH	6"



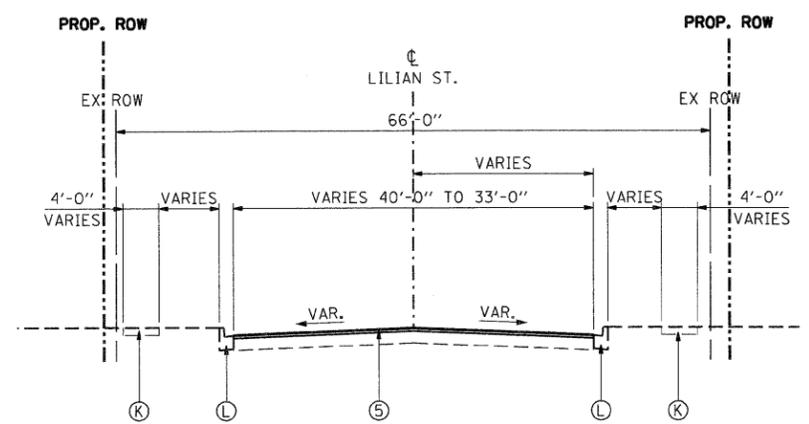
NOTE 2: HOT-MIX ASPHALT SURFACE REMOVAL, 2" FROM STA. 203+56.11 TO STA. 205+13.40



NOTE 1: STA. 200+63.90 TO STA. 201+12.14 - SIDEWALK WIDTH VARIES FROM 8'-10" TO 7'-5"; STA. 201+12.14 TO STA. 201+68.11 - SIDEWALK WIDTH IS 7'-5"

DETAIL L-1

STA. 200+64.00 TO STA. 201+68.11



LILLIAN ST.
STA. 198+30.19 TO STA. 198+89.62
STA. 203+56.11 TO STA. 205+13.40

NOTE: SEE PAVEMENT JOINTING PLANS FOR PAVEMENT JOINTS.

EXISTING LEGEND

- (A) PCC PAVEMENT (10" AND VARIES)
- (B) HMA SURFACE (3" AND VARIES)
- (C) HMA SHOULDER (8" AND VARIES)
- (D) HMA SHOULDER (6" AND VARIES)
- (E) FENCE
- (F) CONCRETE MEDIAN BARRIER & BASE
- (G) PCC SHOULDER (10" AND VARIES)
- (H) TOPSOIL REMOVAL (X")
- (I) HMA BASE COURSE
- (J) HMA PAVEMENT
- (K) PCC SIDE WALK
- (L) CURB AND GUTTER
- (M) CONCRETE MEDIAN
- (N) STEEL PLATE BEAM GUARDRAIL
- (O) TEMP PAVEMENT (SEE NOTE 3)
- (P) TEMPORARY CONCRETE BARRIER
- (R) RETAINING WALL (CAST-IN-PLACE)
- (S) TEMP. SOIL RETENTION SYSTEM
- (T) SUBBASE GRANULAR MATERIAL, TYPE C (10")

REMOVAL

PROPOSED LEGEND

- (1) CONTINUOUSLY REINFORCED CONCRETE PAVEMENT 10 3/4"
- (2) PCC PVT 10 JOINTED
- (3) STABILIZED SUBBASE (HMA) 4 1/2"
- (4) AGGREGATE SUBGRADE 12"
- (5) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- (6) SUB-BASE GRANULAR MATERIAL, TYPE B 12"
- (7) CONCRETE BARRIER, DOUBLE FACE, 32" HEIGHT
- (8) CONCRETE BARRIER BASE
- (9) AGG WEDGE SHLD TYPE B
- (10) COMB CC&G TB6.12
- (11) COMB CC&G TB6.24
- (12) COMB CC&G TM4.24
- (13) CONC CURB TB SPL
- (14) PC CONC SIDEWALK 5
- (15) SPBGR TY A 6FT POSTS
- (16) EMBANKMENT
- (17) RETAINING WALL (SEE STRUCTURAL PLANS)
- (18) DRILL AND GROUT #6 TIE BARS
- (19) TOPSOIL FURNISH AND PLACE, 6"
- (20) TOPSOIL FURNISH AND PLACE, 10"
- (21) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 3/4"
- (22) HOT-MIX ASPHALT BASE COURSE, 8"
- (23) SHOULDER RUMBLE STRIPS, 16"
- (24) AGGREGATE SURFACE COURSE, TYPE B, 4"
- (25) CONCRETE MEDIAN SURFACE, 4 INCH
- (26) CONCRETE MEDIAN, TYPE SB (SPECIAL)
- (27) POLY. HMA SURFACE COURSE, MIX "F", N90 1 3/4"
- (28) CHAIN LINK FENCE, 4"
- (29) POLY. LEVELING BINDER (MM), IL-4.75, N50 3/4" (MIN)
- (30) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 3"
- (31) POLY. HMA SURFACE COURSE, MIX "F", N90 2"
- (32) COMB CC&G TB6.06
- (33) AGGREGATE SHOULDERS, TYPE B 4"
- (34) SEEDING (SEE LANDSCAPING PLANS)
- (35) PCC SHOULDERS 10 3/4"
- (36) PCC SHOULDERS 10"
- (37) AGGREGATE SHOULDERS, TYPE B 10"
- (38) CONCRETE BARRIER, SINGLE FACE 42" HEIGHT (SPECIAL)
- (39) CORRUGATED MEDIAN
- (40) HOT-MIX ASPHALT BASE COURSE, 7"
- (41) SODDING, SALT TOLERANT

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
FAP ROUTE 345 / US ROUTE 20

EXISTING AND PROPOSED TYPICAL SECTIONS
LILLIAN STREET

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	BR-R	KANE	794	37
CONTRACT NO. 60H45				

SCALE: 1"=10' SHEET NO. OF STA. 200+63.90 TO STA. 205+13.40 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

\\D:\60H45\XSET1.DGN... \D:\60H45\BORDER.DGN... \D:\60H45\LEGEND.DGN... \D:\60H45\PLAN\T.D\TRANS.B7\2202\21379-00\ACIVIL\CAD\60H45\SHEET.DWG\60H45-SHT-TYPICAL.DGN
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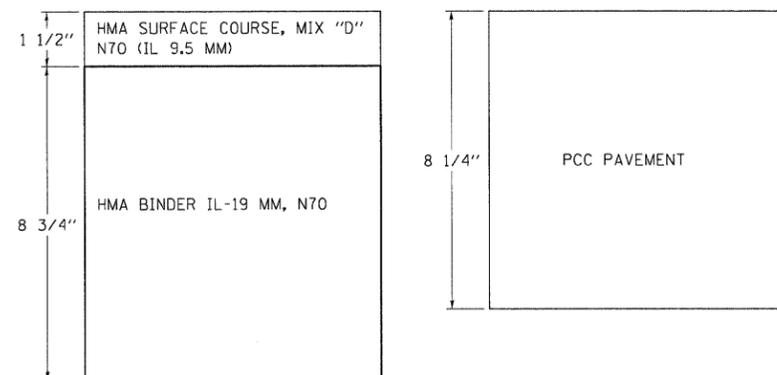
TENG TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
CHICAGO, ILLINOIS

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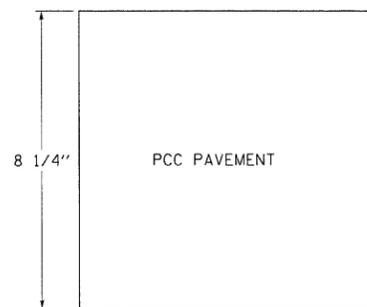
HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS@NDES
PAVEMENT RESURFACING (US 20)	
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL 9.5 mm) (1 3/4")	4% @ 90 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL 4.75, N50 (3/4")	3.5% @ 50 GYR.
SHOULDER RESURFACING (US 20)	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) (1 3/4")	4% @ 70 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL 4.75, N50 (3/4")	3.5% @ 50 GYR.
HMA SHOULDER CONSTRUCTION (US 20)	
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL 9.5 mm) (1 3/4")	4% @ 90 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL 4.75, N50 (3/4")	3.5% @ 50 GYR.
HOT-MIX ASPHALT SHOULDER (HMA BINDER IL-19 mm) (8")	4% @ 70 GYR.
PAVEMENT RESURFACING (MCLEAN BLVD)	
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL 9.5 mm) (2")	4% @ 90 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL 4.75, N50 (3/4")	3.5% @ 50 GYR.
PAVEMENT WIDENING (MCLEAN BLVD)	
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL 9.5 mm) (2")	4% @ 90 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL 4.75, N50 (3/4")	3.5% @ 50 GYR.
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19 mm) (7")	4% @ 90 GYR.
PAVEMENT RESURFACING (LILLIAN ST AND EAST LEG OF FLEETWOOD DR @ MCLEAN BLVD)	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5mm) (2")	4% @ 70 GYR.
TEMPORARY PAVEMENT (US 20) (SEE NOTE 3)	
HMA SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) (1 1/2")	4% @ 70 GYR.
TEMP PAVEMENT (HMA BINDER, N70 IL-19mm) (8 3/4")	4% @ 70 GYR.
TEMPORARY PAVEMENT (MCLEAN BLVD & LILLIAN ST) (SEE NOTE 3)	
HMA SURFACE COURSE, MIX "D", N50 (IL 9.5 mm) (1 1/2")	4% @ 50 GYR.
TEMP PAVEMENT (HMA BINDER, N50 IL-19mm) (6 1/2")	4% @ 50 GYR.
DRIVEWAYS	
HMA SURFACE COURSE, MIX "D", N50 (IL 9.5 mm) (2")	4% @ 50 GYR.
HMA BASE COURSE (HMA BINDER, N50 IL-19 mm) PE-6", CE-8"	4% @ 50 Gyr.
STABILIZED SUBBASE (US 20)	
STABILIZED SUBBASE-HOT MIX ASPHALT (IL-19mm) (4 1/2")	3% @ 50 Gyr
STABILIZED SUBBASE (MCLEAN BLVD, FLEETWOOD DR & LILLIAN ST)	
STABILIZED SUBBASE-HOT MIX ASPHALT (IL-19mm) (4 1/2")	2% @ 30 Gyr
PATCHING	
CLASS D PATCHES (HMA BINDER IL-19MM) (10")	4% @ 70 Gyr.
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19MM) (3")	4% @ 70 Gyr.

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR PERCENT RAP SEE DISTRICT ONE SPECIAL PROVISIONS.
3. PC CONCRETE TEMPORARY PAVEMENT SHALL CONSIST OF CLASS PV CONCRETE MEETING THE REQUIREMENTS OF ART. 1020 OF THE STANDARD SPECIFICATIONS.

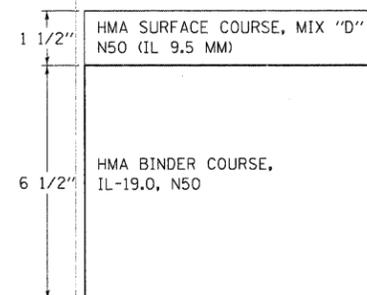


FLEXIBLE DESIGN

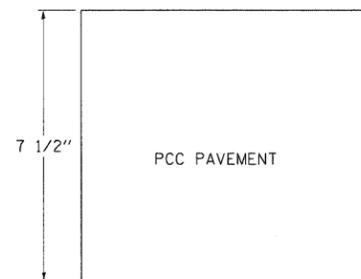


RIGID DESIGN

TEMPORARY PAVEMENT
US ROUTE 20 AND RAMPS



FLEXIBLE DESIGN



RIGID DESIGN

TEMPORARY PAVEMENT
MCLEAN BLVD & LILLIAN ST

TEMPORARY PAVEMENT STRUCTURE DETAILS

STRUCTURAL DESIGN TRAFFIC:		YEAR <u>2012</u>
PV = <u>49,168</u>	SU = <u>0</u>	MU = <u>5,463</u>
ROAD/STREET CLASSIFICATION:		CLASS <u>1</u>
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:		
P = <u>32%</u>	S = <u>45%</u>	M = <u>45%</u>
TRAFFIC FACTOR	ACTUAL TF = <u>34.29</u>	
	MINIMUM TF = <u>10.05</u>	
PG GRADE:	BINDER = <u>NA</u>	SURFACE = <u>NA</u>
SUBGRADE SUPPORT RATING:		
SSR = <u>POOR</u> (STA 70+60 TO 140+24) US-20		
SSR = <u>POOR</u> (STA 59+72 TO 89+00) MCLEAN		

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 BAJZEKKJ
 12-12-2011 12:40:36
 TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 CHICAGO, ILLINOIS

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	PLOT SCALE = #SCALE#	CHECKED - DDH	REVISED -
	PLOT DATE = #DATE#	DATE - 12/16/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 FAP ROUTE 345 / US ROUTE 20

TYPICAL SECTIONS			
MIX DESIGN REQUIREMENTS			
AND MISCELLANEOUS DETAILS			
SCALE: 1"=5'	SHEET NO.	OF	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	8R-R	KANE	794	38
CONTRACT NO. 60H45				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

TREE REMOVAL (6 TO 15 UNITS DIAMETER)				TOTAL	727	UNIT
LOCATION STATION	OFFSET FT.	DIAMETER IN.	SUBTOTALS			
LILLIAN ST. 201+20.15	38.97 RT	6	6			
McLEAN BLVD.						
81+01.29	38.65 LT	6	6			
83+27.02	96.15LT	10	10			
79+98.23	45.00 LT	6	6			
79+44.37	40.41 RT	6	6			
75+07.09	94.26 RT	7	98			
68+98.43	56.95 RT	13	13			
67+33.15	57.62 RT	13	13			
66+96.39	56.13 RT	14	14			
U.S. 20						
100+02.44	98.79 LT	12	12			
101+05.67	97.04 LT	15	15			
101+67.56	284.24 LT	14	14			
102+98.32	52.32LT	6	6			
103+00.53	266.35LT	12	12			
103+03.12	51.94 LT	6	6			
103+17.80	285.41LT	12	12			
103+68.85	266.15 LT	14	14			
103+79.62	261.52 LT	13	13			
104+40.59	231.61 LT	8	8			
105+02.95	197.40 LT	12	12			
105+13.53	193.85 LT	10	10			
105+35.50	181.52 LT	6	6			
105+77.96	167.56 LT	13	13			
105+92.77	157.09 LT	14	14			
106+44.64	150.77 LT	13	13			
106+62.43	142.59 LT	6	6			
106+69.52	148.60 LT	7	7			
106+88.72	143.18 LT	9	9			
107+00.29	144.46 LT	12	12			
107+41.06	143.41 LT	12	12			
107+54.69	136.79 LT	12	12			
107+77.84	138.92 LT	12	12			
108+11.20	132.48 LT	8	8			
108+21.62	140.87 LT	13	13			
108+36.55	134.04 LT	6	6			
108+61.00	133.04 LT	14	14			
108+82.21	121.63 LT	7	7			
110+03.95	115.94 LT	12	12			
110+50.82	103.37 LT	14	14			
112+90.85	77.48 LT	12	12			
113+04.38	86.19 LT	12	12			
113+25.01	75.63 LT	6	6			
113+50.74	74.93 LT	8	8			
116+76.03	74.89 LT	14	14			
113+89.76	75.70 LT	6	6			
114+06.00	74.49 LT	10	10			
114+24.62	73.82 LT	14	14			
114+35.95	76.38 LT	10	10			
114+58.76	73.46 LT	8	8			
114+73.47	66.05 LT	8	8			
114+83.92	69.67 LT	8	8			
115+65.66	70.34 LT	12	12			
115+88.57	71.30 LT	10	10			
116+34.53	66.44 LT	14	14			
117+54.84	66.23 LT	10	10			
118+42.69	85.69 LT	10	10			
118+97.57	82.95 LT	12	12			
119+31.29	86.36 LT	10	10			
122+23.75	73.66 LT	12	12			
124+56.12	63.92 LT	10	10			
130+97.83	64.29 LT	6	6			
131+11.11	64.09 LT	5	5			
131+31.61	63.85 LT	4	4			

TREE REMOVAL (OVER 15 UNITS DIAMETER)				TOTAL	879	UNIT
LOCATION STATION	OFFSET FT.	DIAMETER IN.	SUBTOTALS			
McLEAN BLVD.						
80+64.58	35.26 RT	18	18			
80+39.77	37.47 RT	21	21			
77+68.18	44.67 RT	20	20			
77+31.23	52.28 RT	24	24			
75+67.18	75.39 LT	24	24			
75+57.64	69.16 LT	20	20			
70+83.42	60.34 RT	20	20			
LILLIAN ST						
201+28.52	58.80 RT	16	16			
U.S. 20						
99+65.24	125.53 LT	24	24			
99+80.40	102.85 LT	24	24			
100+19.16	271.49 LT	17	17			
100+30.38	167.89 LT	17	17			
102+52.54	261.05 LT	18	18			
103+08.78	264.63 LT	27	27			
103+17.83	285.98LT	16	16			
103+36.34	85.89 LT	24	24			
104+97.16	181.26 LT	19	19			
105+09.81	152.14LT	18	18			
105+62.81	172.29 LT	16	16			
106+11.09	132.79 LT	20	20			
106+20.76	152.79 LT	16	16			
106+71.77	128.06 LT	20	20			
107+12.52	156.71 LT	20	20			
107+32.66	149.85 LT	18	18			
107+99.08	138.83 LT	18	18			
109+15.68	97.98 LT	23	23			
109+75.93	81.53 LT	27	27			
110+37.59	74.14 LT	23	23			
111+17.47	72.56 LT	18	18			
111+66.46	70.18 LT	16	16			
112+43.11	84.43 LT	20	20			
114+40.24	68.92 LT	16	16			
116+94.75	74.40 LT	16	16			
118+18.46	76.67 LT	18	18			
120+55.92	62.24 LT	34	34			
121+41.31	75.47 LT	16	16			
122+01.94	75.25 LT	16	16			
122+55.51	76.04 LT	16	16			
123+18.96	72.47 LT	20	20			
124+71.31	60.53 LT	27	27			
124+80.49	75.59 LT	34	34			
129+95.90	64.32 LT	20	20			
130+40.71	66.62 LT	24	24			

TEMPORARY FENCE					TOTAL	258	FOOT
LOCATION	STA.	OFFSET FT.	LT/RT	SUBTOTALS			
RAMP D	1+55.25	51.12	RT	126			
RAMP D	2+08.52	51.68	RT	132			

TREE TRUNK PROTECTION					TOTAL	6	EACH
LOCATION	STA.	OFFSET FT.	RT/LT	SUBTOTALS			
US 20	85+13.18	123.1	LT	1			
US 20	94+80.62	171.34	LT	1			
US 20	100+91.15	275.28	LT	1			
US 20	101+14.77	244.43	RT	1			
US 20	101+83.35	237.85	RT	1			
US 20	102+12.07	285.27	LT	1			

TREE ROOT PRUNING					TOTAL	6	EACH
LOCATION	STA.	OFFSET FT.	RT/LT	SUBTOTALS			
US 20	85+13.18	123.1	LT	1			
US 20	94+80.62	171.34	LT	1			
US 20	100+91.15	275.28	LT	1			
US 20	101+14.77	244.43	RT	1			
US 20	101+83.35	237.85	RT	1			
US 20	102+12.07	285.27	LT	1			

TREE PRUNING (OVER 10 INCH DIAMETER)				TOTAL	8	EACH
LOCATION	STA.	OFFSET FT.	RT/LT	SUBTOTALS		
US 20	85+13.18	123.1	LT	1		
US 20	94+80.62	171.34	LT	1		
US 20	100+91.15	275.28	LT	1		
US 20	101+14.77	244.43	RT	1		
US 20	101+83.35	237.85	RT	1		
US 20	102+12.07	285.27	LT	1		
RAMP D	1+55.25	51.12	RT	1		
RAMP D	2+08.52	51.68	RT	1		

SUB-BASE GRANULAR MATERIAL, TYPE B 12"					TOTAL	276	SO YD
LOCATION	FROM STA.	TO STA.	AREA SQ FT	SUBTOTALS			
McLEAN	67+70.50	70+45.24	2,140.00	238			
FLEETWOOD MEDIAN	310+90.00	311+75.60	346.67	39			

HOT-MIX ASPHALT BASE COURSE, 7"					TOTAL	184	SO YD
LOCATION	STA.	STA.	AREA SQ. FT.	SUBTOTALS			
McLEAN BLVD							
S.B. W. EDGE	86+71.77	88+71.38	873.10	97.01			
N.B. E. EDGE	86+71.77	88+66.53	786.74	87.42			

POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50					TOTAL	1075	TON
LOCATION	FROM STA.	TO STA.	SUBTOTALS				
US 20 W.B.	55+00.00	70+00.00	168.00				
US 20			70.00				
RT. SHLD.			42.00				
LT. SHLD.							
US 20 W.B.	140+24.00	156+01.87	162.01				
US 20			74.10				
RT. SHLD.			20.22				
LT. SHLD.							
US 20 E.B.	56+50.00	70+00.00	151.20				
US 20			26.74				
RT. SHLD.			37.80				
LT. SHLD.							
US 20 E.B.	140+24.00	156+01.87	161.99				
US 20			74.10				
RT. SHLD.			19.33				
LT. SHLD.							
McLEAN BLVD	86+71.77	89+00.00	59.39				
McLEAN BLVD							
S.B. W. EDGE	86+71.77	88+71.38	4.07				
N.B. E. EDGE	86+71.77	88+66.53	3.67				

FILE NAME = \\F5-0014\MYVAULT.D - TRANS.07\2202\21379-00\1\CIVIL\CD\60845\SHEET.DWG\45-SHT - SCHEDULE.DWG
 STANKIRJ
 TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 CHICAGO, ILLINOIS

HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT			TOTAL	235	SO YD
STA.			SUBTOTALS		
55+00.00	W.B. US 20 PAVEMENT AND SHOULDER		20		
56+50.00	E.B. US 20 PAVEMENT AND SHOULDER		15		
156+01.87	W.B. US 20 PAVEMENT AND SHOULDER		18		
156+01.87	E.B. US 20 PAVEMENT AND SHOULDER		17		
132+64.84	FLEETWOOD DR		22		
198+30.19	LILLIAN ST		28		
205+13.40	LILLIAN ST		19		
59+53.35	MCLEAN BLVD		30		
89+00.00	MCLEAN BLVD		28		
	WESTON AVE. AT NORTH EDGE OF FLEETWOOD DR.		15		
	WESTON AVE. AT SOUTH EDGE OF FLEETWOOD DR.		16		
	Prestage to stage II		8		

HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50				TOTAL	89	TON
LOCATION	STA.	RT/LT		SUBTOTALS		
DRIVEWAYS						
LILLIAN ST.	201+03.00	LT	4.63			
MCLEAN BLVD.	67+91.00	LT	1.19			
MCLEAN BLVD.	67+96.00	RT	0.89			
MCLEAN BLVD.	77+84.00	RT	0.43			
MCLEAN BLVD.	78+28.00	RT	1.10			
MCLEAN BLVD.	78+96.00	RT	3.16			
MCLEAN BLVD.	80+10.00	LT	1.17			
MCLEAN BLVD.	80+80.00	LT	0.50			
MCLEAN BLVD.	80+92.00	RT	40.54			
MCLEAN BLVD.	81+39.00	LT	0.72			
MCLEAN BLVD.	85+46.00	RT	3.99			
MCLEAN BLVD.	86+11.00	RT	6.10			
MCLEAN BLVD.	86+71.00	LT	4.78			
MCLEAN BLVD.	87+01.00	RT	4.83			
MCLEAN BLVD.	87+91.00	RT	1.33			
MCLEAN BLVD.	88+44.00	LT	0.52			
LILLIAN ST.	199+04.00	RT	4.91			
PARKING LOT PAVEMENT						
MCLEAN BLVD.	83+76.71	RT	8.10			
MCLEAN BLVD.	85+87.04	RT	0.18			

HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70				TOTAL	923	TON
LOCATION	FROM STA.	TO STA.		SUBTOTALS		
FLEETWOOD DR. E. END	312+45.00	312+65.00	11.66			
LILLIAN ST. START	198+30.19	198+89.62	41.85			
LILLIAN ST. E. END	203+53.91	205+13.40	115.39			
US 20 W.B.	55+00.00	70+00.00				
RT. SHLD.			163.33			
LT. SHLD.			98.00			
US 20 W.B.	140+24.00	156+01.87	171.81			
RT. SHLD.						
US 20 E.B.			61.30			
RT. SHLD.	64+37.00	70+00.00	88.20			
LT. SHLD.	56+50.00	70+00.00				
US 20 E.B.	140+24.00	156+01.87	171.81			
RT. SHLD.						

POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90				TOTAL	1787	TON
LOCATION	FROM STA.	TO STA.		SUBTOTALS		
US 20 W.B.	55+00.00	70+00.00	392.00			
US 20 W.B.	140+24.00	156+01.87	378.02			
US20 W B LT SHLD	140+24.01	156+01.88	45.87			
US 20 E.B.	56+50.00	70+00.00	352.80			
US 20 E.B.	140+24.00	156+01.87	377.99			
US20 E B LT SHLD	140+24.01	156+01.88	45.87			
MCLEAN BLVD	59+53.25	59+73.25	14.95			
MCLEAN BLVD	86+71.77	89+00.00	0.00			
MCLEAN BLVD			158.37			
MCLEAN BLVD						
S.B. W. EDGE	86+71.77	88+71.38	10.87			
N.B. E. EDGE	86+71.77	88+66.53	9.79			

PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)				TOTAL	50903	SO YD
LOCATION	FROM STA.	TO STA.		SUBTOTALS		
MCLEAN BLVD	59+73.25	86+70.00	28,674.38			
LILLIAN ST	198+30.36	198+95.00	774.26			
LILLIAN ST	201+00.00	203+55.06	2,367.23			
RAMP A	27+05.09		5,308.45			
RAMP B		14+72.13	4,434.50			
RAMP C	4+61.77		4,729.96			
RAMP D		16+41.14	4,613.74			
* INCLUDES FLEETWOOD DRIVE WEST OF MCLEAN AND DRIVEWAY TO BUENA VISTA APTS AT FLEETWOOD EAST OF MCLEAN AREAS MEASURED TO SEPARATION JOINTS						

BRIDGE APPROACH PAVEMENT CONNECTOR (PCC)				TOTAL	1067	SO YD
LOCATION	FROM STA.	TO STA.		SUBTOTALS		
U.S. 20 E.B.	93+99.04	94+99.04	533			
U.S. 20 W.B.	101+66.32	102+66.32	533			

CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 10 3/4" TOTAL 40206 SO YD			
LOCATION	FROM STA.	TO STA.	SUBTOTALS
US 20 EB	70+00.00	85+60.69	4,162
US 20 EB	85+60.69	93+98.93	2,239
US 20 EB	102+65.43	117+02.09	3,869
US 20 EB	117+02.09	138+24.08	5,658
US 20 EB	138+24.08	140+24.00	510
US 20 WB	70+00.00	87+99.40	4,798
US 20 WB	87+99.40	90+38.53	636
US 20 WB	90+38.53	93+98.93	957
US 20 WB	102+65.43	112+40.37	2,590
US 20 WB	112+40.37	116+48.04	2,163
US 20 WB	116+48.04	119+24.21	1,472
US 20 WB	119+24.21	122+24.26	1,400
US 20 WB	122+24.26	135+24.13	5,199
US 20 WB	135+24.13	138+24.12	1,000
US 20 WB	138+24.12	140+24.00	510
RAMP A (WB EXIT)	25+81.38	27+05.09	328
RAMP B (EB ENT)	14+72.13	24+25.25	1,079
RAMP C (EB EXIT)	00+20.71	03+51.67	331
RAMP C (EB EXIT)	03+51.67	04+61.77	196
RAMP D (WB ENT)	16+41.14	25+91.19	1,108

LUG SYSTEM COMPLETE 24'				TOTAL	8	EACH
LOCATION	FROM STA.	TO STA.		SUBTOTALS		
U.S. 20 E.B.	93+44.81	93+99.10	1			
U.S. 20 E.B.	102+65.64	103+19.43	1			
U.S. 20 W.B.	93+44.81	93+98.75	1			
U.S. 20 W.B.	102+65.11	103+19.43	1			
U.S. 20 E.B.	71+00.00	71+54.00	1			
U.S. 20 E.B.	138+70.00	139+24.00	1			
U.S. 20 W.B.	71+00.00	71+54.00	1			
U.S. 20 W.B.	138+70.00	139+24.00	1			

PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH				TOTAL	98	EACH
LOCATION	STA.	RT/LT		SUBTOTALS		
LILLIAN ST.	202+10.00	RT	40			
LILLIAN ST.	202+59.00	LT	45			
MCLEAN BLVD.	88+92.00	LT	13			

PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH				TOTAL	747	SO YD
LOCATION	STA.	RT/LT		SUBTOTALS		
LILLIAN ST.	201+03.00	LT	34			
LILLIAN ST.	201+39.00	RT	93			
MCLEAN BLVD.	67+91.00	LT	28			
MCLEAN BLVD.	67+93.00	RT	29			
MCLEAN BLVD.	77+84.00	RT	24			
MCLEAN BLVD.	78+28.00	RT	15			
MCLEAN BLVD.	78+96.00	RT	34			
MCLEAN BLVD.	80+10.00	LT	17			
MCLEAN BLVD.	80+80.00	LT	24			
MCLEAN BLVD.	80+92.00	RT	28			
MCLEAN BLVD.	81+39.00	LT	32			
MCLEAN BLVD.	81+48.00	RT	39			
MCLEAN BLVD.	85+46.00	RT	36			
MCLEAN BLVD.	86+11.00	RT	37			
MCLEAN BLVD.	86+71.00	LT	38			
MCLEAN BLVD.	87+01.00	RT	35			
MCLEAN BLVD.	87+91.00	RT	36			
MCLEAN BLVD.	88+44.00	LT	40			
MCLEAN BLVD.	88+59.00	RT	44			
MAIN LN	DRIVEWAY	NORTH	23			
MAIN LN	DRIVEWAY	SOUTH	61			

PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH				TOTAL	46709	SO FT
LOCATION	FROM STA.	TO STA.	RT/LT	SUBTOTALS		
MCLEAN BLVD.	64+08.41	64+66.71	RT	446		
MCLEAN BLVD.	65+45.19	67+74.00	RT	1,487		
MCLEAN BLVD.	68+09.33	69+12.35	RT	511		
MCLEAN BLVD.	69+12.35	72+26.82	RT	2,254		
MCLEAN BLVD.	64+14.05	64+29.96	LT	83		
MCLEAN BLVD.	65+05.38	67+73.67	LT	2,522		
MCLEAN BLVD.	68+08.04	72+36.23	LT	2,961		
MCLEAN BLVD.	72+40.26	73+59.05	RT	713		
MCLEAN BLVD.	73+83.00	74+69.94	RT	3,105		
MCLEAN BLVD.	74+96.18	76+13.81	RT	1,097		
MCLEAN BLVD.	76+40.20	77+74.61	RT	978		
MCLEAN BLVD.	78+00.00	78+23.00	RT	167		
MCLEAN BLVD.	78+39.38	78+78.52	RT	358		
MCLEAN BLVD.	72+61.64	73+91.92	LT	1,170		

CONTINUED NEXT SHEET

\D:\6045-BORDER\01.DGN, \D:\6045-SCHEDULE\01.DGN, \D:\6045-SCHEDULE\02.DGN
 12-14-2011, 9:58:28 AM
 BAJZEKJ \S:\6045\WALL\TD-TRANS\97-2282-21379-001\CIVIL\ACAD\6045-SHEET\6045-SHT-SCHEDULE02.DGN

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION FAP ROUTE 345 / US ROUTE 20		SCHEDULE OF QUANTITIES - II		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE.#		DRAWN - JFS	REVISED -					345	BR-R	KANE	794	40
PLOT SCALE = #SCALE#		CHECKED - DDH	REVISED -					CONTRACT NO. 60H45				
PLOT DATE = #DATE#		DATE - 12/16/11	REVISED -					FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				
SCALE: N.T.S.		SHEET NO. OF		STA. TO STA.								



TENC ENGINEERS/ARCHITECTS/PLANNERS
 CHICAGO, ILLINOIS

CONTINUED FROM PREVIOUS SHEET

PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH				TOTAL 46709 SQ FT
McLEAN BLVD.	74+22.71	75+17.00	LT	2,334
McLEAN BLVD.	75+40.18	76+43.24	LT	930
McLEAN BLVD.	76+54.45	78+50.86	LT	2,406
McLEAN BLVD.	79+05.02	80+01.56	LT	927
McLEAN BLVD.	80+22.80	80+64.40	LT	312
McLEAN BLVD.	80+95.08	81+16.79	LT	171
McLEAN BLVD.	79+15.27	80+78.12	RT	1,395
McLEAN BLVD.	81+12.78	81+25.34	RT	183
McLEAN BLVD.	81+69.28	82+60.50	RT	1,472
McLEAN BLVD.	81+61.23	82+57.09	LT	1,096
McLEAN BLVD.	83+30.67	86+13.46	LT	2,410
McLEAN BLVD.	86+13.46	86+50.37	LT	327
McLEAN BLVD.	86+90.99	88+22.52	LT	1,107
McLEAN BLVD.	88+67.04	88+81.45	LT	121
McLEAN BLVD.	88+97.62	89+00.00	LT	7
McLEAN BLVD.	83+27.02	85+26.44	RT	2,491
McLEAN BLVD.	85+65.71	85+90.94	RT	233
McLEAN BLVD.	86+32.69	86+80.18	RT	379
McLEAN BLVD.	87+22.89	87+73.92	RT	392
McLEAN BLVD.	88+09.93	88+40.12	RT	268
McLEAN BLVD.	88+75.96	89+24.10	RT	357
LILLIAN ST.	201+49.43	201+96.62	RT	334
LILLIAN ST.	202+24.30	203+49.63	RT	1,016
LILLIAN ST.	201+21.34	203+44.41	LT	911
LILLIAN ST.	202+71.21	203+47.03	LT	594
FLEETWOOD DR.	301+08.66	302+75.13	LT	824
FLEETWOOD DR.	303+64.93	304+66.25	LT	479
FLEETWOOD DR.	304+91.26	306+12.32	LT	835
FLEETWOOD DR.	306+41.86	306+80.86	LT	303
FLEETWOOD DR.	307+15.86	308+09.65	LT	861
FLEETWOOD DR.	302+62.87	304+03.97	RT	961
FLEETWOOD DR.	304+44.44	305+99.18	RT	1,048
FLEETWOOD DR.	306+25.65	307+01.73	RT	499
FLEETWOOD DR.	307+19.68	307+60.38	RT	244
FLEETWOOD DR.	307+95.44	308+88.81	RT	634

PAVEMENT REMOVAL				TOTAL 140195 SQ YD
LOCATION	FROM STA.	TO STA.		SUBTOTALS
RAMP D	01+33.66	20+16.55		2259
RAMP C & WELD RD	06+17.67	17+40.24		2447
RAMP B	01+21.95	15+04.96		2801
RAMP A	39+04.05	26+45.86		2042
McLEAN BLVD. (INCLUDING FLEETWOOD EAST LEG)	59+73.25	67+50.00		7117
McLEAN BLVD.	67+50.00	80+00.00		10307
McLEAN BLVD.	80+00.00	86+70.00		4518
FLEETWOOD DR.	307+31.89	309+37.80		137
LILLIAN ST.	201+00.00	203+55.43		1181
US 20 E.B.	64+37.00	97+64.96		18542
US 20 E.B.	99+12.79	156+01.87		29083
US 20 W.B.	70+00.00	97+54.94		7353
US 20 W.B.	99+02.68	140+23.80		10915
US 20 MEDIAN	60+00.00	70+00.00		2801
US 20 MEDIAN	78+50.00	84+50.00		2111
ELM GROVE DR	113+30.00	113+65.00		143
MOT QUANTITY TEMPORARY PAVEMENT				36436

* REFER TO MOT TEMPORARY PAVEMENT

HOT-MIX ASPHALT SURFACE REMOVAL, 2"				TOTAL 1642 SQ YD
LOCATION	FROM STA.	TO STA.		SUBTOTALS
FLEETWOOD DR. E. END	312+45.00	312+65.00		104
LILLIAN ST. START	198+30.19	198+89.62		374
LILLIAN ST. E. END	203+55.11	205+13.40		1,030
McLEAN BLVD S. END	59+53.25	59+73.25		133

HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"				TOTAL 19211 SQ YD
LOCATION	FROM STA.	TO STA.		SUBTOTALS
US 20 W.B.	55+00.00	70+00.00		5,976
	140+24.00	156+01.87		5,657
US 20 E.B.	56+50.00	70+00.00		3,786
	140+24.00	156+01.87		3,793

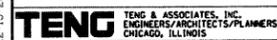
HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"				TOTAL 1414 SQ YD
LOCATION	FROM STA.	TO STA.		SUBTOTALS
McLEAN BLVD. N. END	86+70.00	89+00.00		1,414

DRIVEWAY PAVEMENT REMOVAL				TOTAL 7570 SQ YD
LOCATION		FROM STA.		SUBTOTALS
RAMP A	N. SIDE	32+69.00		33
McLEAN BLVD	W. SIDE	88+92.00		15
McLEAN BLVD	W. SIDE	88+41.44		41
McLEAN BLVD	W. SIDE	86+71.17		34
McLEAN BLVD	W. SIDE	86+13.00		208
McLEAN BLVD	W. SIDE	86+33.00		35
McLEAN BLVD	E. SIDE	88+59.11		51
McLEAN BLVD	E. SIDE	87+91.31		62
McLEAN BLVD	E. SIDE	87+01.16		53
McLEAN BLVD	E. SIDE	85+87.00		190
McLEAN BLVD	E. SIDE	86+11.20		48
McLEAN BLVD	E. SIDE	83+25.00		517
LILLIAN ST.	N. SIDE	201+03.34		44
LILLIAN ST.	N. SIDE	201+00.00		74
LILLIAN ST.	N. SIDE	202+59.22		55
LILLIAN ST.	S. SIDE	200+82.00		67
LILLIAN ST.	S. SIDE	201+00.00		34
LILLIAN ST.	S. SIDE	201+42.00		193
LILLIAN ST.	S. SIDE	202+10.00		50
McLEAN BLVD	W. SIDE	81+39.15		40
McLEAN BLVD	W. SIDE	81+11.00		1245
McLEAN BLVD	W. SIDE	80+79.77		24
McLEAN BLVD	W. SIDE	80+00.00		273
McLEAN BLVD	W. SIDE	80+09.55		19
McLEAN BLVD	W. SIDE	79+45.00		23
McLEAN BLVD	W. SIDE	78+27.00		74
McLEAN BLVD	W. SIDE	76+85.00		351
McLEAN BLVD	W. SIDE	77+21.00		81
McLEAN BLVD	E. SIDE	82+17.00		37
McLEAN BLVD	E. SIDE	81+48.00		25
McLEAN BLVD	E. SIDE	81+18.00		493
McLEAN BLVD	E. SIDE	80+90.00		29
McLEAN BLVD	E. SIDE	80+00.00		25
McLEAN BLVD	E. SIDE	78+40.00		247
McLEAN BLVD	E. SIDE	78+27.00		47
McLEAN BLVD	E. SIDE	77+83.00		60
McLEAN BLVD	E. SIDE	77+00.00		56
McLEAN BLVD	W. SIDE	67+92.00		134
McLEAN BLVD	E. SIDE	67+89.00		110
MAIN LN	S. SIDE			63
MAIN LN	N. SIDE			26
BUILDING # 1	McLEAN E SIDE	79+59.00		399
BUILDING # 2	McLEAN E SIDE	80+09.00		1069
BUILDING # 3	McLEAN W SIDE	79+15.00		700
BUILDING # 5	US 20 N. SIDE	114+20.00		85
BUILDING # 6	US 20 N. SIDE	115+50.00		33

CURB REMOVAL				TOTAL 1997 FOOT
LOCATION	FROM STA.	TO STA.		SUBTOTALS
RAMP A	32+69.00	33+13.00		53
McLEAN BLVD.	61+48.80	64+41.60		293
McLEAN BLVD.	64+97.00	65+59.00		78
McLEAN BLVD.	67+89.00	67+95.00		30
McLEAN BLVD.	67+65.00	67+74.00		21
McLEAN BLVD.	68+10.00	68+22.00		21
McLEAN BLVD.	72+64.00	72+80.00		46
McLEAN BLVD.	76+38.00	76+60.00		91
McLEAN BLVD.	76+85.00			19
McLEAN BLVD.	77+11.00	77+16.00		22
McLEAN BLVD.	76+34.00	76+48.00		42
McLEAN BLVD.	76+98.00	77+10.00		18
McLEAN BLVD.	77+42.00	77+43.00		23
McLEAN BLVD.	77+63.00	77+73.00		29
McLEAN BLVD.	77+93.00	78+03.00		29
McLEAN BLVD.	78+00.00	78+08.00		23
McLEAN BLVD.	78+42.00	78+53.00		18
McLEAN BLVD.	80+09.00	80+25.00		66
McLEAN BLVD.	81+10.00	81+36.00		90
McLEAN BLVD.	81+60.00	81+98.00		38
McLEAN BLVD.	82+35.00	82+65.00		50
McLEAN BLVD.	79+15.00	79+39.00		29
McLEAN BLVD.	79+56.00	80+05.00		71
McLEAN BLVD.	80+21.00	80+68.00		60
McLEAN BLVD.	80+90.00	81+06.00		46
McLEAN BLVD.	81+10.00	81+22.00		38
McLEAN BLVD.	81+58.00	82+47.00		109
McLEAN BLVD.	85+77.00	85+87.00		30
McLEAN BLVD.	86+30.00	86+75.00		45
McLEAN BLVD.	86+48.00	86+57.00		45
LILLIAN ST.	197+72.00	197+77.00		26
LILLIAN ST.	200+76.00	201+32.00		129
FLEETWOOD DR.	310+50.00	311+78.00		271

COMBINATION CURB AND GUTTER REMOVAL				TOTAL 10970 FOOT
LOCATION				SUBTOTALS
WELD RD. @ RAMP C INTERSECTION				NW QUADRANT 58 NE QUADRANT 77 SW QUADRANT 54 SE. QUADRANT 34
EB EXIT RAMP				LT 298 RT 88
McLEAN BLVD. & PARTIAL RAMPS STA. 72+33 to STA. 76+24				LT 480.95 RT 720
McLEAN BLVD. STA. 57+87.08 to STA. 61+48.80				RT 359.1 239.9
FLEETWOOD DR. EB STA 310+30 TO STA 312+50				RT ISLAND 89
FLEETWOOD DR. N.E. QUADRANT				RT 810.48 LT 648.3
McLEAN BLVD. STA. 65+30 to STA. 72+15				ISLAND 122
McLEAN S.W. QUADRANT				RT 623.1 LT 751.3
McLEAN BLVD. STA. 80+00 to STA. 83+14				RT 326.72 LT 291
McLEAN DRIVEWAY 67+74				RT 34
McLEAN DRIVEWAY 68+04				RT 42
McLEAN BLVD. STA. 83+14 to STA. 89+00				RT 639 LT 584
LILLIAN ST. STA. 201+00 to STA. 203+55.04				RT 280.72 LT 284.29
LILLIAN ST. STA. 198+30 to STA. 199+21				LT 117.7
FLEETWOOD & SHEPARD NW RADIUS RETURN				9.1
FLEETWOOD & SHEPARD NE RADIUS RETURN				18.6
ELM GROVE DR, US 20 STA 113+42 LT				66.8
ELM GROVE DR, US 20 STA 113+67 LT				68.6
McLEAN BLVD. N.B. RT. 58+00.00	62+90.00	(MOT)	TB6-12	487.00
McLEAN BLVD. N.B. RT. 64+10.00	69+10.00	(MOT)	TB6-12	471.40
McLEAN BLVD. N.B. RT. 70+20.00	70+71.00	(MOT)	TB6-12	70.00
McLEAN BLVD. N.B. RT. 75+06.00	75+13.00	(MOT)	TB6-12	10.00
McLEAN BLVD. N.B. RT. 76+59.82	88+27.00	(MOT)	TB6-12	1,141.50
LILLIAN (N.EDGE)	200+92.00	203+40.30	(MOT)	TB6-12 248.70
LILLIAN (S.EDGE)	200+96.10	202+98.40	(MOT)	TB6-12 203.44
McLEAN BLVD. N.B. RT. 62+90.00	64+10.00	(MOT)	TB9-12	122.40

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COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24				TOTAL 8705 FOOT
LOCATION	FROM STA.	TO STA.	SUBTOTALS	
McLEAN BLVD. N.B. RT.	59+73.25	64+70.64	514	
McLEAN BLVD. N.B. RT.	65+40.16	67+50.00	258	
McLEAN BLVD. N.B. RT.	67+50.00	72+45.49	530	
McLEAN BLVD. N.B. RT.	RAMP ISLAND B		441	
McLEAN BLVD. N.B. RT.	RAMP EDGES B to A		514	
McLEAN BLVD. N.B. RT.	RAMP ISLAND A		527	
McLEAN BLVD. N.B. RT.	76+34.93	80+00.00	374	
McLEAN BLVD. N.B. RT.	80+00.00	82+56.65	283	
McLEAN BLVD. N.B. RT.	83+25.93	89+00.00	624	
LILLIAN ST. E.B. RT	201+00.37	203+53.91	275	
LILLIAN ST. E.B. LT	201+00.00	203+56.29	301	
LILLIAN ST. W.LEG RT	198+30.19	199+27.89	99	
LILLIAN ST. W.LEG LT	198+89.62	199+38.30	70	
McLEAN BLVD. S.B. RT.	89+14.00	83+29.45	598	
McLEAN BLVD. S.B. RT.	82+20.91	80+00.00	220	
McLEAN BLVD. S.B. RT.	80+00.00	78+98.66	244	
McLEAN BLVD. S.B. RT.	78+58.08	76+37.67	427	
McLEAN BLVD. S.B. RT.	RAMP ISLAND D		309	
McLEAN BLVD. S.B. RT.	RAMP EDGES D to C		481	
McLEAN BLVD. S.B. RT.	RAMP ISLAND C		548	
McLEAN BLVD. S.B. RT.	72+41.72	64+91.97	767	
FLEETWOOD & SHEPARD NW RADIUS RETURN			9	
FLEETWOOD & SHEPARD NE RADIUS RETURN			19	
CORNER ISLAND - S.W. QUADRANT AT McLEAN BLVD. & FLETWOOD AVE.			50.50	
Main Lane	78+98.66		110.00	
	78+58.08		112.00	

HOT-MIX ASPHALT BASE COURSE, 8"					TOTAL 1577 SQ YD
LOCATION	STA.	RT/LT	AREA SQ. FT.	SUBTOTALS	
DRIVEWAYS					
LILLIAN ST.	201+03.00	LT	371.81	41	
McLEAN BLVD.	67+91.00	LT	95.38	11	
McLEAN BLVD.	67+96.00	RT	71.77	8	
McLEAN BLVD.	77+84.00	RT	34.21	4	
McLEAN BLVD.	78+28.00	RT	88.63	10	
McLEAN BLVD.	78+96.00	RT	253.61	28	
McLEAN BLVD.	80+10.00	LT	94.39	10	
McLEAN BLVD.	80+80.00	LT	39.84	4	
McLEAN BLVD.	80+92.00	RT	3,258.00	362	
McLEAN BLVD.	81+39.00	LT	58.14	6	
McLEAN BLVD.	85+46.00	RT	320.44	36	
McLEAN BLVD.	86+11.00	RT	490.48	54	
McLEAN BLVD.	86+71.00	LT	384.27	43	
McLEAN BLVD.	87+01.00	RT	387.87	43	
McLEAN BLVD.	87+91.00	RT	106.72	12	
McLEAN BLVD.	88+44.00	LT	41.49	5	
LILLIAN ST.	199+04.00	RT	394.76	44	
US 20 E.B.	STA.		WIDTH		
LT SHLD	140+24.00		2.67	391	
US 20 W.B.					
LT SHLD	140+24.00		2.67	391	
PARKING LOT PAVEMENT IS LISTED BELOW					
McLEAN BLVD.	83+76.71	N.B. RT.	650.56	72	
McLEAN BLVD.	85+87.04	N.B. RT.	14.78	2	

CONCRETE MEDIAN SURFACE, 4 INCH				TOTAL 2622 SQ FT
LOCATION	FROM STA.	TO STA.	SUBTOTALS	
McLEAN BLVD	67+70.5	70+45.24	2,141	
FLEETWOOD MEDIAN	310+90	311+75.60	347	
S.W. CORNER OF McLEAN BLVD. AND FLEETWOOD DR.			134	

CORRUGATED MEDIAN				TOTAL 300.1 SQ FT
LOCATION	FROM STA.	TO STA.	SUBTOTALS	
McLEAN BLVD	84+67.63	85+17.65	300.12	

STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS				TOTAL 15964 FOOT
LOCATION	FROM STA.	TO STA.	SUBTOTALS	
US 20 E.B. LT	57+00.00	85+75.36	2,875	
US 20 E.B. RT	69+04.05	79+54.05	1,050	
US 20 E.B. RT	91+17.04	94+65.46	348	
US 20 E.B. RT	101+71.16	106+55.24	484	
US 20 W.B. LT	56+72.50	85+85.33	2,913	
US 20 W.B. RT	94+97.10	90+06.24	491	
US 20 W.B. RT	106+88.58	101+96.49	492	
US 20 W.B. RT	141+18.15	133+43.15	775	
RAMP A (US 20 STATIONS)	126+35.35	104+72.59	2,151	
RAMP B	5+06.53	5+56.50	50	
RAMP B (US 20 STATIONS)	112+64.40	139+10.24	2,646	
RAMP C	9+54.05	10+67.05	113	
RAMP D	23+83.03	8+06.77	1,576	

TRAFFIC BARRIER TERMINAL, TYPE 2				TOTAL 8 EACH
LOCATION	FROM STA.	TO STA.	SUBTOTALS	
US 20 W.B. LT	56+60.50		1	
RAMP D RT	23+83.03		1	
US 20 E. B. LT	85+75.36		1	
RAMP C RT	10+67.05		1	
US 20 W. B. RT	89+94.24		1	
US 20 E. B. RT	106+55.24		1	
RAMP A RT	33+58.55		1	
US 20 E.B. RT	139+10.24		1	

TRAFFIC BARRIER TERMINAL, TYPE 5				TOTAL 7 EACH
LOCATION	FROM STA.	TO STA.	SUBTOTALS	
US 20 W.B. LT	86+00.00		1	
RAMP D RT	07+92.10		1	
RAMP C RT	09+39.38		1	
US 20 W.B. RT	95+11.77		1	
US 20 E.B. RT	101+56.49		1	
RAMP B RT	14+20.74		1	
US 20 W.B. RT	126+50.02		1	

TRAFFIC BARRIER TERMINAL, TYPE 6				TOTAL 6 EACH
LOCATION	FROM STA.	TO STA.	SUBTOTALS	
RAMP D	03+90.05	03+46.90	1	
US20 E.B. (RT)	79+54.05	79+97.20	1	
US20 E.B. (RT)	94+65.46	95+08.61	1	
RAMP B	05+56.50	05+99.65	1	
US20 W.B. (RT)	101+53.34	101+96.49	1	
US20 W.B. (RT)	133+00.00	133+43.15	1	

TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT				TOTAL 7 EACH
LOCATION	FROM STA.	TO STA.	SUBTOTALS	
US 20 E.B. LT	56+50.00	57+00.00	1	
US20 E.B. RT	68+54.05	69+04.05	1	
US20 E.B. RT	90+67.84	91+17.84	1	
RAMP D RT	02+96.95	03+46.95	1	
RAMP B RT	04+56.53	05+06.53	1	
US 20 W.B. RT	107+38.58	106+88.58	1	
US 20 W. B. RT	141+68.15	141+18.15	1	

GUARDRAIL REMOVAL					TOTAL 1154 FOOT
LOCATION	FROM STA.	OFFSET	TO STA.	OFFSET	SUBTOTALS
US 20	94+23.11		97+41.29		318
US 20	98+97.17		106+63.09		766
McLEAN	88+78	39.6	88+78	46.0	6
RAMP C	11+54.74		12+17.79		63

CONCRETE BARRIER, DOUBLE FACE, 32 INCH HEIGHT					TOTAL 6074 FOOT
LOCATION	FROM STA.	TO STA.	SUBTOTALS		
US 20 - Double Face			899		
US 20	86+00.00	94+98.93	899		
US 20	101+66.18	153+41.34	5,175		

CONCRETE BARRIER BASE					TOTAL 7658 FOOT
LOCATION	FROM STA.	TO STA.	SUBTOTALS		
RAMP C	0+00	09+39.38	934		
US 20	126+50	133+00	650		
US 20	86+00.00	94+98.93	899		
US 20	101+66.18	153+41.34	5,175		

SHOULDER RUMBLE STRIPS, 16 INCH				TOTAL 27598 FOOT
LOCATION	FROM STA.	TO STA.	SUBTOTALS	
US 20				
E.B. RT	56+50.00	70+00.00	1,350	
E.B. LT	56+50.00	70+00.00	1,350	
W.B. RT	55+00.00	70+00.00	1,500	
W.B. LT	55+00.00	70+00.00	1,500	
E.B. LT	70+00.00	88+19.00	1,819	
W.B. LT	70+00.00	87+15.00	1,715	
E.B. RT	88+19.00	94+98.93	680	
E.B. LT	88+19.00	94+98.93	680	
W.B. RT	87+15.00	94+98.93	784	
W.B. LT	87+15.00	94+98.93	784	
E.B. RT	101+66.18	108+35.00	669	
E.B. LT	101+66.18	108+35.00	669	
W.B. RT	101+66.18	109+06.00	740	
W.B. LT	101+66.18	109+06.00	740	
E.B. LT	108+35.00	140+24.00	3,189	
W.B. LT	109+06.00	140+24.00	3,118	
E.B. RT	140+24.00	156+02.00	1,578	
E.B. LT	140+24.00	156+02.00	1,578	
W.B. RT	140+24.00	156+02.00	1,578	
W.B. LT	140+24.00	156+02.00	1,578	

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CONCRETE PAD			TOTAL	352	SQ YD
LOCATION	AREA SQ FT	SUBTOTALS			
SPUI ISLANDS					
NORTHEAST	1006.8		111.87		
SOUTHEAST	729.16		81.02		
SOUTHWEST	1035.53		115.06		
NORTHWEST	395.05		43.89		

CHAIN LINK FENCE, 4'					TOTAL	3933	FOOT
LOCATION	FROM STA.	TO STA.	SUBTOTALS				
US20 E.B.	77+80.27	90+55.46			1,343		
US20 E.B.	90+55.46	98+16.65			815		
US20 W.B.	105+00.00	120+88.89			1775		

PORTLAND CEMENT CONCRETE SIDEWALK 6 INCH, SPECIAL					TOTAL	101	SQ FT
LOCATION	FROM STA.	TO STA.	RT/LT	SUBTOTALS			
EL Faro Restaurant	850+13.60	850+47.25	LT			101.0	

HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH					TOTAL	3920	SQ YD
LOCATION	FROM STA.	TO STA.	SUBTOTALS				
US 20 E.B.	64+37.00	70+00.00			637		
	59+34.86	70+00.00			710		
US 20 W.B.							
	60+00.00	70+00.00			667		
US 20 E. B.	140+24.00	156+01.87			1,753		
	153+41.34	156+01.87			154		

ISLAND PAVEMENT REMOVAL			TOTAL	149	SQ YD
LOCATION	AREA SQ.FT	SUBTOTALS			
MCLEAN BLVD. & FLEETWOOD DR. S.W. CORNER					
MCLEAN BLVD. & FLEETWOOD DR. N.E. CORNER					
MCLEAN BLVD. N.B. LT STA. 72+72.55	525.00			58	
MCLEAN BLVD & RAMP D N.W. CORNER	213.00			24	
MCLEAN BLVD & RAMP A N.E. CORNER	169.49			19	
	80.40			9	
	355.00			39	

FOUNDATION REMOVAL			TOTAL	3	EACH
LOCATION	STATION	OFFSET	SUBTOTALS		
MCLEAN	78+37.38	42.1 RT			1
MCLEAN	83+67.38	52.4' LT			1
MCLEAN	83+67.44	42.5' LT			1

CONCRETE CURB, TYPE B (SPECIAL)					TOTAL	1536	FOOT
LOCATION	FROM STA.	TO STA.	SUBTOTALS				
MCLEAN BLVD. N.B. LT.	76+85.64	78+33.39			155		
MCLEAN BLVD. N.B. LT.	79+11.98	80+01.56			105		
MCLEAN BLVD. N.B. LT.	80+18.47	80+68.07			50		
MCLEAN BLVD. N.B. LT.	80+92.44	81+22.43			30		
MCLEAN BLVD. N.B. LT.	81+56.49	822+49.33			101		
MCLEAN BLVD. N.B. RT.	78+34.63	78+80.96			43		
MCLEAN BLVD. N.B. RT.	79+10.76	80+80.51			170		
MCLEAN BLVD. N.B. RT.	81+04.47	81+30.16			26		
MCLEAN BLVD. N.B. RT.	81+65.11	82+50.55			123		
LILLIAN ST. E.B. LT.	201+18.34	201+68.11			50		
MCLEAN BLVD.N.B. RT.	76+88.54	77+13.17			25		
MCLEAN BLVD.N.B. RT.	83+34.66	85+29.97			211		
MCLEAN BLVD.N.B. RT.	85+62.01	85+93.92			32		
MCLEAN BLVD.N.B. RT.	86+29.02	86+83.28			54		
MCLEAN BLVD.N.B. RT.	87+18.48	87+25.61			7		
MCLEAN BLVD.N.B. LT.	86+13.23	86+54.19			41		
MCLEAN BLVD.N.B. LT.	86+88.33	88+27.91			140		
MCLEAN BLVD.N.B. LT.	88+60.62	88+74.37			14		
LILLIAN ST. W.B. RT.	201+00.00	201+38.00			38		
Fleetwood LT	306+52.61	306+80.86			28		
Fleetwood LT	307+15.86	308+09.54			94		

CONCRETE MEDIAN, TYPE SB (SPECIAL)					TOTAL	10122	SQ FT
LOCATION	FROM STA.	TO STA.	SUBTOTALS				
MCLEAN BLVD.	62+63.52	64+08.63			871		
MCLEAN BLVD.	65+48.03	67+70.50			1,335		
MCLEAN BLVD.	70+45.24	73+02.82			1,545		
MCLEAN BLVD.	75+93.12	82+20.00			3,679		
MCLEAN BLVD.	83+55.00	84+67.63			676		
LILLIAN ST.	200+60.00	201+89.87			779		
FLEETWOOD DR.	307+31.89	309+38.00			1,237		

COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24					TOTAL	2529	FOOT
LOCATION	FROM STA.	TO STA.	SUBTOTALS				
US 20 N. EDGE	89+94.36	94+98.67			504		
US 20 N. EDGE	101+66.44	107+38.58			572		
US 20 N. EDGE	111+97.91	126+50.01			1,452		

CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)					TOTAL	1589	FOOT
LOCATION	FROM STA.	OFFSET FT	TO STA.	OFFSET FT	SUBTOTALS		
US 20	126+50.00		133+00.00				650
RAMP C	0+00.00	12.00	3+51.00	12.00			351
RAMP C	3+51.00	12.00	9+39.38	12.00			588

SIGN REMOVAL				TOTAL	1	EACH
LOCATION	STA.	OFFSET	SUBTOTALS			
MCLEAN BLVD	84+74.00	48' RT			1.00	

AGGREGATE SURFACE COURSE, TYPE B 4"			TOTAL	3916	SQ YD
LOCATION	SQ FT		SUBTOTALS		
BENEATH US 20 BRIDGE WEST OF MCLEAN		17,452.00			1939
BENEATH US 20 BRIDGE WEST OF MCLEAN		17,793.00			1977

CHAIN LINK FENCE REMOVAL						TOTAL	1722	FOOT
LOCATION	STA.	OFFSET	STA.	OFFSET	SUBTOTALS			
US 20 W.B.	105+01.69	-197.18	105+97.90	-146.21			107	
US 20 W.B.	105+97.90	-146.21	109+03.60	-119.87			300	
US 20 W.B.	109+03.60	-119.87	109+19.70	-145.18			30	
US 20 W.B.	109+03.60	-119.87	110+08.15	-106.68			104	
US 20 W.B.	110+08.15	-106.68	110+10.60	-119.82			13	
US 20 W.B.	110+08.15	-106.68	112+13.47	-77.93			204	
US 20 W.B.	112+13.47	-77.93	112+17.39	-117.97			40	
US 20 W.B.	121+98.00	-78.65	122+62.19	-80.00			64	
US 20 W.B.	114+89.15	-75.30	114+80.68	-108.24			34	
US 20 W.B.	116+55.38	-89.56	117+10.51	-105.18			57	
US 20 W.B.	118+36.09	-80.24	118+45.93	-80.03			10	
US 20 W.B.	118+45.93	-80.03	118+43.00	-91.14			11	
US 20 W.B.	118+45.93	-80.03	118+93.08	-80.36			47	
US 20 W.B.	118+93.08	-80.36	118+92.89	-90.61			10	
US 20 W.B.	118+93.08	-80.36	119+37.17	-79.92			44	
US 20 W.B.	119+37.17	-79.92	119+37.03	-90.14			10	
US 20 W.B.	119+37.17	-79.92	119+87.78	-80.00			51	
US 20 W.B.	119+87.78	-80.00	119+87.91	-87.25			7	
US 20 W.B.	119+87.78	-80.00	120+37.52	-75.4			50	
US 20 W.B.	120+37.52	-75.40	120+37.71	-83.63			8	
US 20 W.B.	120+37.52	-75.40	120+88.89	-73.54			51	
US 20 W.B.	120+88.89	-73.54	120+88.91	-80.00			6	
US 20 W.B.	120+88.89	-73.54	121+33.94	-77.78			45	
US 20 W.B.	121+33.94	-77.78	121+33.90	-80.00			2	
MCLEAN BLVD	78+33.58	41.62	78+34.69	56.92			15	
MCLEAN BLVD	79+75.26	-63.23	79+67.36	-168.24			105	
MCLEAN BLVD	73+57.95	37.09	75+02.07	35.93			151	
MCLEAN BLVD	73+65.99	35.51	75+06.15	34.82			142	

FENCE REMOVAL					TOTAL	1343	FOOT
LOCATION	STA.	OFFSET	STA.	OFFSET	SUBTOTALS		
US 20 E.B.	77+80.27	81.6	90+55.46	251.9			1,343

D:\06045-BORDER\06045-1\06045-SCHEDULED\06045-12-14-2011-050612.DWG
 12-14-2011 05:06:12
 BAIZEKJ
 \S-0044\MAIN\VAULT.D-TRANS-07\2202-21379-001\CIVIL\06045\06045-SHEET\06045-SHT-SCHEDULED\06045-12-14-2011-050612.DWG

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -
#FILE#		DRAWN - JFS	REVISED -
	PLOT SCALE = #SCALE#	CHECKED - DDH	REVISED -
	PLOT DATE = #DATE#	DATE - 12/16/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 FAP ROUTE 345 / US ROUTE 20

SCHEDULE OF QUANTITIES - VI

SCALE: N.T.S.	SHEET NO.	OF	STA.	TO STA.
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	8R-R	KANE	794	44
CONTRACT NO. 60H45				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

EARTHWORK SCHEDULE

STA FROM	TO STA	EARTH EXCAVATION			REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS			EXCAVATION TO BE USED IN EMBANKMENT (15% SHRINKAGE)			EMBANKMENT			EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)		
		STAGE IA	STAGE IC	STAGE ID	STAGE IA	STAGE IC	STAGE ID	STAGE IA	STAGE IC	STAGE ID	STAGE IA	STAGE IC	STAGE ID	STAGE IA	STAGE IC	STAGE ID
		CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD
US 20 MEDIAN TEMP PAVEMENT																
69+00.00	70+00.00	0.0	0.0	0.0	0.0	68.8	0.0	0.0	0.0	0.0	0.0	152.9	0.0	0.0	-152.9	0.0
70+00.00	71+00.00	0.0	0.6	0.0	0.0	75.4	0.0	0.0	0.5	0.0	0.0	163.8	0.0	0.0	-163.3	0.0
71+00.00	72+00.00	0.0	2.3	0.0	0.0	75.9	0.0	0.0	2.0	0.0	0.0	160.5	0.0	0.0	-158.5	0.0
72+00.00	73+00.00	0.0	6.6	0.0	0.0	75.5	0.0	0.0	5.6	0.0	0.0	148.1	0.0	0.0	-142.5	0.0
73+00.00	73+80.00	0.0	7.1	0.0	0.0	62.4	0.0	0.0	6.0	0.0	0.0	141.9	0.0	0.0	-135.9	0.0
73+80.00	74+00.00	0.0	1.6	0.0	0.0	15.9	0.0	0.0	1.3	0.0	0.0	40.4	0.0	0.0	-39.1	0.0
74+00.00	75+00.00	0.0	7.1	0.0	0.0	76.8	0.0	0.0	6.1	0.0	0.0	162.4	0.0	0.0	-156.3	0.0
75+00.00	76+00.00	0.0	3.6	0.0	0.0	76.0	0.0	0.0	3.0	0.0	0.0	153.1	0.0	0.0	-150.1	0.0
76+00.00	77+00.00	0.0	0.2	0.0	0.0	78.2	0.0	0.0	0.2	0.0	0.0	174.2	0.0	0.0	-174.0	0.0
77+00.00	78+00.00	0.0	0.3	0.0	0.0	78.1	0.0	0.0	0.2	0.0	0.0	169.2	0.0	0.0	-169.0	0.0
78+00.00	79+00.00	0.0	0.2	0.0	0.0	38.4	0.0	0.0	0.2	0.0	0.0	92.2	0.0	0.0	-92.0	0.0
79+00.00	79+85.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	18.5	0.0	0.0	-18.5	0.0
79+85.00	80+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	2.0	0.0	0.0	-2.0	0.0
80+00.00	81+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	5.1	0.0	0.0	-5.1	0.0
81+00.00	82+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	3.7	0.0	0.0	-3.7	0.0
82+00.00	83+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	5.3	0.0	0.0	-5.3	0.0
83+00.00	84+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	26.5	0.0	0.0	-26.5	0.0
84+00.00	85+00.00	0.0	0.0	0.0	0.0	36.6	0.0	0.0	0.0	0.0	0.0	104.7	0.0	0.0	-104.7	0.0
85+00.00	86+00.00	0.0	0.0	0.0	0.0	73.7	0.0	0.0	0.0	0.0	0.0	178.0	0.0	0.0	-178.0	0.0
86+00.00	87+00.00	0.0	0.0	0.0	0.0	73.8	0.0	0.0	0.0	0.0	0.0	196.9	0.0	0.0	-196.9	0.0
87+00.00	88+00.00	0.0	0.0	0.0	0.0	74.2	0.0	0.0	0.0	0.0	0.0	219.6	0.0	0.0	-219.6	0.0
88+00.00	88+80.00	0.0	0.0	0.0	0.0	60.8	0.0	0.0	0.0	0.0	0.0	200.0	0.0	0.0	-200.0	0.0
88+80.00	89+00.00	0.0	0.0	0.0	0.0	15.1	0.0	0.0	0.0	0.0	0.0	49.4	0.0	0.0	-49.4	0.0
89+00.00	89+94.68	0.0	0.0	0.0	0.0	70.5	0.0	0.0	0.0	0.0	0.0	249.8	0.0	0.0	-249.8	0.0
89+94.68	90+00.00	0.0	0.0	0.0	0.0	4.0	0.0	0.0	0.0	0.0	0.0	15.8	0.0	0.0	-15.8	0.0
90+00.00	91+00.00	0.0	0.0	0.0	0.0	72.3	0.0	0.0	0.0	0.0	0.0	272.1	0.0	0.0	-272.1	0.0
91+00.00	92+00.00	0.0	0.0	0.0	0.0	66.1	0.0	0.0	0.0	0.0	0.0	238.2	0.0	0.0	-238.2	0.0
92+00.00	93+00.00	0.0	0.0	0.0	0.0	57.4	0.0	0.0	0.0	0.0	0.0	215.2	0.0	0.0	-215.2	0.0
93+00.00	94+00.00	0.0	0.0	0.0	0.0	48.1	0.0	0.0	0.0	0.0	0.0	225.2	0.0	0.0	-225.2	0.0
94+00.00	95+00.00	0.0	0.0	0.0	0.0	40.4	0.0	0.0	0.0	0.0	0.0	263.4	0.0	0.0	-263.4	0.0
95+00.00	95+28.43	0.0	0.0	0.0	0.0	5.2	0.0	0.0	0.0	0.0	0.0	39.1	0.0	0.0	-39.1	0.0
95+28.43	END															
PROPOSED BRIDGE																
101+36.68	102+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	46.7	33.1	0.0	-46.7	-33.1
102+00.00	102+17.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	24.6	17.2	0.0	-24.6	-17.2
102+17.00	103+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	107.2	80.7	0.0	-107.2	-80.7
103+00.00	104+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	98.5	83.4	0.0	-98.5	-83.4
104+00.00	104+41.98	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	33.4	26.7	0.0	-33.4	-26.7
104+41.98	105+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	44.6	36.0	0.0	-44.6	-36.0
105+00.00	106+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	77.4	69.6	0.0	-77.4	-69.6
106+00.00	107+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	78.6	71.3	0.0	-78.6	-71.3
107+00.00	108+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	75.7	66.6	0.0	-75.7	-66.6
108+00.00	109+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	67.6	60.0	0.0	-67.6	-60.0
109+00.00	110+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	58.5	55.8	0.0	-58.5	-55.8
110+00.00	111+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	51.7	50.9	0.0	-51.7	-50.9
111+00.00	112+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	43.4	45.9	0.0	-43.4	-45.9
112+00.00	112+49.65	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	17.5	20.3	0.0	-17.5	-20.3
112+49.65	113+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	14.7	17.1	0.0	-14.7	-17.1
113+00.00	114+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	30.0	26.8	0.0	-30.0	-26.8
114+00.00	115+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	30.4	21.9	0.0	-30.4	-21.9
115+00.00	116+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	28.9	21.9	0.0	-28.9	-21.9

CONTINUED NEXT SHEET

NOTE: SEE GENERAL NOTES SHEET 3 FOR TEMPORARY PAVEMENT INFORMATION.

\NFS-004\AVM\VALT.D - TRANS. 07-2202-21379-001\CIVIL\CAD\60H45 SHEET\0160H45-SHT-SCHEDULE09.DGN
 STANKIRJ
 12-12-2011 17:59:25
 \NFS-004\AVM\VALT.D - TRANS. 07-2202-21379-001\CIVIL\CAD\60H45 SHEET\0160H45-SHT-SCHEDULE09.DGN

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION FAP ROUTE 345 / US ROUTE 20	SCHEDULE OF QUANTITIES - IX EARTHWORK SCHEDULES US 20 TEMP PAVEMENT - STAGES IA, IC AND ID			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILEL#		DRAWN - MRK	REVISED -		345	BR-R	KANE	794	47			
PLOT SCALE = #SCALE#		CHECKED - DDH	REVISED -		SCALE: N.T.S. SHEET NO. OF STA. TO STA.				CONTRACT NO. 60H45			
PLOT DATE = #DATE#		DATE - 12/16/11	REVISED -		FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT							



EARTHWORK SCHEDULE

CONTINUED FROM PREVIOUS SHEET

		EARTH EXCAVATION			REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS			EXCAVATION TO BE USED IN EMBANKMENT (15% SHRINKAGE)			EMBANKMENT			EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)		
STA FROM	TO STA	STAGE IA	STAGE IC	STAGE ID	STAGE IA	STAGE IC	STAGE ID	STAGE IA	STAGE IC	STAGE ID	STAGE IA	STAGE IC	STAGE ID	STAGE IA	STAGE IC	STAGE ID
		CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD
US 20 MEDIAN TEMP PAVEMENT																
116+00.00	116+20.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	6.2	4.6	0.0	-6.2	-4.6
116+20.00	117+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	20.6	11.8	0.0	-20.6	-11.8
117+00.00	118+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	20.2	6.7	0.0	-20.2	-6.7
118+00.00	118+70.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	12.0	4.7	0.0	-12.0	-4.7
118+70.00	119+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	4.3	1.7	0.0	-4.3	-1.7
119+00.00	120+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	7.7	6.9	2.4	-7.7	-6.9	-2.4
120+00.00	121+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	15.8	0.0	0.0	-15.8	0.0	0.0
121+00.00	121+20.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	3.4	0.0	0.0	-3.4	0.0	0.0
121+20.00	122+00.00	1.1	0.0	0.0	0.0	0.0	0.0	0.9	0.0	0.0	10.9	0.0	0.0	-10.0	0.0	0.0
122+00.00	123+00.00	1.4	0.0	0.0	0.0	0.0	0.0	1.2	0.0	0.0	25.6	0.0	0.0	-24.4	0.0	0.0
123+00.00	123+70.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	24.3	0.0	0.0	-24.3	0.0	0.0
123+70.00	124+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	7.8	0.0	0.0	-7.8	0.0	0.0
124+00.00	125+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	23.3	0.0	0.0	-23.3	0.0	0.0
125+00.00	126+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	22.9	0.0	0.0	-22.9	0.0	0.0
126+00.00	126+20.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	5.0	0.0	0.0	-5.0	0.0	0.0
126+20.00	127+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	20.0	0.0	0.0	-20.0	0.0	0.0
127+00.00	128+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	17.5	0.0	0.0	-17.5	0.0	0.0
128+00.00	128+70.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	8.5	0.0	0.0	-8.5	0.0	0.0
128+70.00	129+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	3.6	0.0	0.0	-3.6	0.0	0.0
129+00.00	130+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	12.4	0.0	0.0	-12.4	0.0	0.0
130+00.00	131+00.00	0.9	0.0	0.0	0.0	0.0	0.0	0.8	0.0	0.0	9.1	0.0	0.0	-8.3	0.0	0.0
131+00.00	131+20.00	0.4	0.0	0.0	0.0	0.0	0.0	0.4	0.0	0.0	0.8	0.0	0.0	-0.5	0.0	0.0
131+20.00	132+00.00	1.1	0.0	0.0	0.0	0.0	0.0	1.0	0.0	0.0	2.6	0.0	0.0	-1.6	0.0	0.0
132+00.00	133+00.00	0.2	0.0	0.0	0.0	0.0	0.0	0.1	0.0	0.0	4.4	0.0	0.0	-4.2	0.0	0.0
133+00.00	134+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	5.8	0.0	0.0	-5.8	0.0	0.0
134+00.00	135+00.00	0.3	0.0	0.0	0.0	0.0	0.0	0.2	0.0	0.0	4.4	0.0	0.0	-4.2	0.0	0.0
135+00.00	136+00.00	37.4	0.0	0.0	45.0	0.0	0.0	31.8	0.0	0.0	38.0	0.0	0.0	-6.3	0.0	0.0
136+00.00	137+00.00	43.1	0.0	0.0	86.2	0.0	0.0	36.6	0.0	0.0	111.3	0.0	0.0	-74.7	0.0	0.0
137+00.00	138+00.00	7.8	0.0	0.0	85.1	0.0	0.0	6.6	0.0	0.0	199.1	0.0	0.0	-192.5	0.0	0.0
138+00.00	138+93.00	2.6	0.0	0.0	87.9	0.0	0.0	2.2	0.0	0.0	217.4	0.0	0.0	-215.2	0.0	0.0
138+93.00	139+00.00	0.2	0.0	0.0	7.1	0.0	0.0	0.1	0.0	0.0	14.6	0.0	0.0	-14.4	0.0	0.0
139+00.00	140+00.00	10.5	0.0	0.0	109.6	0.0	0.0	8.9	0.0	0.0	205.6	0.0	0.0	-196.7	0.0	0.0
140+00.00	140+24.00	4.4	0.0	0.0	28.2	0.0	0.0	3.7	0.0	0.0	51.2	0.0	0.0	-47.5	0.0	0.0
140+24.00	141+00.00	21.4	0.0	0.0	97.0	0.0	0.0	18.2	0.0	0.0	154.1	0.0	0.0	-135.8	0.0	0.0
141+00.00	142+00.00	74.9	0.0	0.0	148.0	0.0	0.0	63.7	0.0	0.0	179.1	0.0	0.0	-115.5	0.0	0.0
142+00.00	143+00.00	86.1	0.0	0.0	158.4	0.0	0.0	73.2	0.0	0.0	165.0	0.0	0.0	-91.8	0.0	0.0
143+00.00	144+00.00	30.2	0.0	0.0	121.1	0.0	0.0	25.7	0.0	0.0	150.9	0.0	0.0	-125.2	0.0	0.0
144+00.00	145+00.00	0.0	0.0	0.0	59.8	0.0	0.0	0.0	0.0	0.0	84.2	0.0	0.0	-84.2	0.0	0.0
145+00.00	END															
GRAND TOTAL:		324.0	29.6	0.0	1033.3	1419.5	0.0	275.4	25.2	0.0	1806.4	5086.9	837.0	-1531.0	-5061.8	-837.0

EARTH WORK INCLUDES THE TEMPORARY PAVEMENT CONSTRUCTED OFF OF US 20 WB PROPOSED OUTSIDE SHOULDER FROM STA 135+00 TO 145+00 IN STAGE IA

		EARTH EXCAVATION	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS	PGES PLACEMENT	EXCAVATION TO BE USED IN EMBANKMENT (15% SHRINKAGE)	EMBANKMENT	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)
STA FROM	TO STA	STAGE IIA	STAGE IIA	STAGE IIA	STAGE IIA	STAGE IIA	STAGE IIA
		CU YD	CU YD	CU YD	CU YD	CU YD	CU YD
US 20 MEDIAN TEMP PAVEMENT							
71+00.00	72+00.00	3.3	11.9	0.0	2.8	21.3	-18.5
72+00.00	73+00.00	6.3	11.8	0.0	5.3	20.1	-14.7
73+00.00	73+80.00	3.1	4.3	0.0	2.7	11.8	-9.1
73+80.00	74+00.00	0.7	0.0	0.0	0.6	2.2	-1.6
74+00.00	75+00.00	6.6	5.4	0.0	5.6	15.4	-9.8
75+00.00	76+00.00	4.5	8.1	0.0	3.8	19.0	-15.2
76+00.00	77+00.00	1.6	6.4	0.0	1.4	20.9	-19.5
77+00.00	78+00.00	0.0	10.2	0.0	0.0	23.7	-23.7
78+00.00	END						
GRAND TOTAL:		26.1	58.0	0.0	22.2	134.3	-112.1

NOTE: SEE GENERAL NOTES SHEET 3 FOR TEMPORARY PAVEMENT INFORMATION.

\F5-00\4\AMVAULT.D-TRANS.87\2202\21379-801\CIVIL\CAD\G08H5\SHEET.DWG\45-SHT-SCHEDULE.BDDN
 STANKIRJ
 \D:\6045-BORDER\JDN
 12-12-2011, 1:59:27

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION FAP ROUTE 345 / US ROUTE 20	SCHEDULE OF QUANTITIES - X EARTHWORK SCHEDULES US 20 TEMP PAVEMENT - STAGES IA, IC, ID AND IIA	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
#FILEL#		DRAWN - MRK	REVISED -			345	8R-R	KANE	794	48	
PLOT SCALE = #SCALE#		CHECKED - DDH	REVISED -			CONTRACT NO. 60H45					
PLOT DATE = #DATE#		DATE - 12/16/11	REVISED -			SCALE: N.T.S.	SHEET NO.	OF	STA.	TO STA.	FED. ROAD DIST. NO.



EARTHWORK SCHEDULE

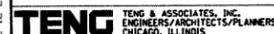
STA FROM	TO STA	EARTH EXCAVATION		REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS		PGES PLACEMENT		EXCAVATION TO BE USED IN EMBANKMENT (15% SHRINKAGE)		EMBANKMENT		EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)	
		STAGE IA	STAGE IIA	STAGE IA	STAGE IIA	STAGE IA	STAGE IIA	STAGE IA	STAGE IIA	STAGE IA	STAGE IIA	STAGE IA	STAGE IIA
		CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD
TEMPORARY RAMP A													
403+00.00	403+50.00	3.7	0.0	0.0	0.0	0.0	0.0	3.2	0.0	0.0	0.0	3.2	0.0
403+50.00	404+00.00	4.4	0.0	0.0	0.0	0.0	0.0	3.7	0.0	0.9	0.0	2.8	0.0
404+00.00	404+50.00	2.9	0.0	5.6	0.0	0.0	0.0	2.5	0.0	11.0	0.0	-8.5	0.0
404+50.00	405+00.00	2.7	0.0	26.5	0.0	0.0	0.0	2.3	0.0	48.5	0.0	-46.2	0.0
405+00.00	405+50.00	0.5	0.0	47.9	0.0	0.0	0.0	0.4	0.0	125.8	0.0	-125.4	0.0
405+50.00	406+00.00	0.0	0.0	56.7	0.0	0.0	0.0	0.0	0.0	237.6	0.0	-237.6	0.0
406+00.00	406+50.00	0.0	0.0	62.4	0.0	0.0	0.0	0.0	0.0	370.1	0.0	-370.1	0.0
406+50.00	407+00.00	0.0	0.0	67.7	0.0	0.0	0.0	0.0	0.0	432.9	0.0	-432.9	0.0
407+00.00	407+50.00	0.0	0.0	72.4	0.0	0.0	0.0	0.0	0.0	410.2	0.0	-410.2	0.0
407+50.00	408+00.00	0.0	0.0	74.9	0.0	0.0	0.0	0.0	0.0	315.8	0.0	-315.8	0.0
408+00.00	408+50.00	0.0	0.0	66.2	0.0	0.0	0.0	0.0	0.0	183.4	0.0	-183.4	0.0
408+50.00	409+00.00	0.0	0.0	56.4	0.0	0.0	0.0	0.0	0.0	96.1	0.0	-96.1	0.0
409+00.00	409+50.00	2.1	0.0	55.8	0.0	0.0	0.0	1.8	0.0	41.8	0.0	-40.1	0.0
409+50.00	410+00.00	4.6	0.0	56.0	0.0	0.0	0.0	3.9	0.0	18.4	0.0	-14.5	0.0
410+00.00	410+50.00	3.1	0.0	55.7	0.0	0.0	0.0	2.6	0.0	23.8	0.0	-21.2	0.0
410+50.00	411+00.00	0.6	0.0	55.4	0.0	0.0	0.0	0.5	0.0	35.1	0.0	-34.6	0.0
411+00.00	411+50.00	0.1	0.0	55.3	0.0	0.0	0.0	0.1	0.0	49.6	0.0	-49.6	0.0
411+50.00	412+00.00	0.0	0.0	55.4	0.0	0.0	0.0	0.0	0.0	70.0	0.0	-70.0	0.0
412+00.00	412+50.00	0.0	0.0	54.1	0.0	0.0	0.0	0.0	0.0	78.6	0.0	-78.6	0.0
412+50.00	412+92.53	0.0	0.0	22.5	0.0	0.0	0.0	0.0	0.0	33.1	0.0	-33.1	0.0
412+92.53	END												
	SUB-TOTAL	24.7	0.0	946.8	0.0	0.0	0.0	21.0	0.0	2582.7	0.0	-2561.7	0.0
TEMPORARY RAMP B													
500+00.00	500+50.00	4.8	0.0	15.4	0.0	0.0	0.0	4.1	0.0	20.8	0.0	-16.7	0.0
500+50.00	501+00.00	5.9	0.0	47.4	0.0	0.0	0.0	5.1	0.0	150.5	0.0	-145.4	0.0
501+00.00	501+50.00	1.1	0.0	74.3	0.0	0.0	0.0	0.9	0.0	371.9	0.0	-371.0	0.0
501+50.00	502+00.00	0.0	0.0	92.7	0.0	0.0	0.0	0.0	0.0	583.3	0.0	-583.3	0.0
502+00.00	502+50.00	0.0	0.0	98.9	0.0	0.0	0.0	0.0	0.0	758.9	0.0	-758.9	0.0
502+50.00	503+00.00	0.0	0.0	84.8	0.0	0.0	0.0	0.0	0.0	849.4	0.0	-849.4	0.0
503+00.00	503+50.00	0.0	0.0	60.5	0.0	0.0	0.0	0.0	0.0	800.7	0.0	-800.7	0.0
503+50.00	504+00.00	0.0	0.0	36.4	0.0	0.0	0.0	0.0	0.0	655.3	0.0	-655.3	0.0
504+00.00	504+50.00	0.0	0.0	12.2	0.0	0.0	0.0	0.0	0.0	521.9	0.0	-521.9	0.0
504+50.00	505+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	382.8	0.0	-382.8	0.0
505+00.00	505+55.38	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	162.9	0.0	-162.9	0.0
505+55.38	END												
	SUB-TOTAL	11.9	0.0	522.5	0.0	0.0	0.0	10.1	0.0	5258.5	0.0	-5248.3	0.0

STA FROM	TO STA	EARTH EXCAVATION		REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS		PGES PLACEMENT		EXCAVATION TO BE USED IN EMBANKMENT (15% SHRINKAGE)		EMBANKMENT		EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)	
		STAGE IA	STAGE IIA	STAGE IA	STAGE IIA	STAGE IA	STAGE IIA	STAGE IA	STAGE IIA	STAGE IA	STAGE IIA	STAGE IA	STAGE IIA
		CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD
TEMPORARY RAMP C													
604+41.39	604+50.00	0.0	0.0	0.0	4.3	0.0	1.4	0.0	0.0	0.0	13.8	0.0	-13.8
604+50.00	605+00.00	0.0	0.0	0.0	63.0	0.0	30.0	0.0	0.0	0.0	318.7	0.0	-318.7
605+00.00	605+50.00	0.0	0.0	0.0	94.4	0.0	78.1	0.0	0.0	0.0	559.3	0.0	-559.3
605+50.00	606+00.00	0.0	0.0	0.0	106.5	0.0	106.5	0.0	0.0	0.0	574.0	0.0	-574.0
606+00.00	606+50.00	0.0	0.0	0.0	92.8	0.0	92.8	0.0	0.0	0.0	409.9	0.0	-409.9
606+50.00	607+00.00	0.0	0.0	0.0	71.4	0.0	42.5	0.0	0.0	0.0	175.7	0.0	-175.7
607+00.00	607+50.00	0.0	0.2	0.0	62.2	0.0	0.0	0.0	0.1	0.0	56.3	0.0	-56.1
607+50.00	608+00.00	0.0	13.4	0.0	53.9	0.0	0.0	0.0	11.4	0.0	68.2	0.0	-56.8
608+00.00	608+50.00	0.0	24.4	0.0	35.2	0.0	0.0	0.0	20.7	0.0	34.3	0.0	-13.6
608+50.00	609+00.00	0.0	11.1	0.0	37.1	0.0	0.0	0.0	9.4	0.0	51.4	0.0	-42.0
609+00.00	609+50.00	0.0	0.0	0.0	51.5	0.0	0.0	0.0	0.0	0.0	112.2	0.0	-112.2
609+50.00	610+00.00	0.0	3.9	0.0	60.6	0.0	0.0	0.0	3.3	0.0	130.2	0.0	-126.9
610+00.00	610+50.00	0.0	3.9	0.0	65.4	0.0	0.0	0.0	3.3	0.0	136.1	0.0	-132.8
610+50.00	611+00.00	0.0	0.0	0.0	63.2	0.0	0.0	0.0	0.0	0.0	137.6	0.0	-137.6
611+00.00	611+50.00	0.0	0.0	0.0	60.6	0.0	0.0	0.0	0.0	0.0	143.9	0.0	-143.9
611+50.00	612+00.00	0.0	0.0	0.0	62.2	0.0	0.0	0.0	0.0	0.0	151.9	0.0	-151.9
612+00.00	612+50.00	0.0	0.0	0.0	57.9	0.0	0.0	0.0	0.0	0.0	151.6	0.0	-151.6
612+50.00	612+88.70	0.0	0.0	0.0	20.9	0.0	0.0	0.0	0.0	0.0	58.8	0.0	-58.8
612+88.70	END												
	SUB-TOTAL	0.0	56.8	0.0	1063.0	0.0	351.3	0.0	48.3	0.0	3283.8	0.0	-3235.5
TEMPORARY RAMP D													
700+31.69	700+50.00	15.6	0.0	23.0	0.0	0.0	0.0	13.3	0.0	6.6	0.0	6.7	0.0
700+50.00	701+00.00	107.1	0.0	108.4	0.0	0.0	0.0	91.0	0.0	34.5	0.0	56.5	0.0
701+00.00	701+50.00	96.5	0.0	110.2	0.0	0.0	0.0	82.1	0.0	41.0	0.0	41.1	0.0
701+50.00	702+00.00	42.7	0.0	107.4	0.0	0.0	0.0	36.3	0.0	47.6	0.0	-11.3	0.0
702+00.00	702+50.00	11.9	0.0	70.0	0.0	0.0	0.0	10.2	0.0	48.7	0.0	-38.6	0.0
702+50.00	703+00.00	2.3	0.0	41.1	0.0	0.0	0.0	2.0	0.0	41.3	0.0	-39.3	0.0
703+00.00	703+50.00	2.0	0.0	22.3	0.0	0.0	0.0	1.7	0.0	21.8	0.0	-20.1	0.0
703+50.00	704+00.00	1.7	0.0	15.9	0.0	0.0	0.0	1.4	0.0	11.1	0.0	-9.7	0.0
704+00.00	END												
	SUB-TOTAL	279.9	0.0	498.3	0.0	0.0	0.0	237.9	0.0	252.5	0.0	-14.6	0.0
GRAND TOTAL:		316.5	56.8	1967.6	1063.0	0.0	351.3	269.0	48.3	8093.7	3283.8	-7824.7	-3235.5

NOTE: SEE GENERAL NOTES SHEET 3 FOR TEMPORARY PAVEMENT INFORMATION.

\S:\044\VA\VAULT.T.D-TRANS.DT\2202\21379-001\CIVIL\CAD\60445\ASHEET.DWG\60445-SHT-SCHEDULE.LDGN
 STANKIRJ
 12-12-2011 17:59:30

FILE NAME =	USER NAME = *USER*	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION FAP ROUTE 345 / US ROUTE 20	SCHEDULE OF QUANTITIES - XI EARTHWORK SCHEDULES TEMPORARY RAMPS - STAGES IA AND IIA			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE#		DRAWN - MRK	REVISED -		345	8R-R	KANE	794	49			
	PLOT SCALE = *SCALE*	CHECKED - DDH	REVISED -		CONTRACT NO. 60445							
	PLOT DATE = *DATE*	DATE - 12/16/11	REVISED -		SCALE: N.T.S.	SHEET NO.	OF	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	



TENG & ASSOCIATES, INC.
 ENGINEERS ARCHITECTS/PLANNERS
 CHICAGO, ILLINOIS

EARTHWORK SCHEDULE

STA FROM	TO STA	EARTH EXCAVATION	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS	PGES PLACEMENT	EXCAVATION TO BE USED IN EMBANKMENT (15% SHRINKAGE)	EMBANKMENT	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)
		STAGE II.1	STAGE II.1	STAGE II.1	STAGE II.1	STAGE II.1	STAGE II.1
		CU YD	CU YD	CU YD	CU YD	CU YD	CU YD
MCLEAN TEMP WIDENING							
58+50.00	59+00.00	0.0	23.6	0.0	0.0	15.4	-15.4
59+00.00	59+50.00	0.1	35.7	0.0	0.1	18.0	-17.9
59+50.00	59+73.25	0.1	19.8	0.0	0.1	8.3	-8.2
59+73.25	60+00.00	0.1	25.5	0.0	0.1	16.6	-16.5
60+00.00	60+50.00	0.1	48.9	0.0	0.1	40.6	-40.6
60+50.00	61+00.00	1.0	49.2	0.0	0.8	37.0	-36.2
61+00.00	61+50.00	6.7	42.2	0.0	5.7	23.0	-17.3
61+50.00	62+00.00	11.1	36.9	0.0	9.4	27.1	-17.6
62+00.00	62+50.00	10.2	36.1	0.0	8.7	32.6	-24.0
62+50.00	63+00.00	18.1	33.7	0.0	15.4	13.2	2.1
63+00.00	63+50.00	41.8	37.0	0.0	35.5	2.7	32.9
63+50.00	64+00.00	41.2	38.4	0.0	35.0	2.9	32.1
64+00.00	64+50.00	17.0	29.1	0.0	14.4	3.1	11.3
64+50.00	65+50.00	14.9	50.8	0.0	12.7	7.4	5.3
65+50.00	66+00.00	7.9	29.4	0.0	6.7	4.6	2.0
66+00.00	66+50.00	15.0	29.3	0.0	12.8	3.5	9.2
66+50.00	67+00.00	21.3	28.5	0.0	18.1	2.4	15.7
67+00.00	67+50.00	20.5	27.6	0.0	17.5	2.5	15.0
67+50.00	68+00.00	18.7	13.6	0.0	15.9	3.1	12.8
68+00.00	68+50.00	18.4	12.1	0.0	15.7	2.7	13.0
68+50.00	69+00.00	11.3	26.2	0.0	9.6	7.3	2.4
69+00.00	69+50.00	6.6	38.8	0.0	5.6	11.2	-5.6
69+50.00	70+00.00	5.9	47.7	0.0	5.0	18.9	-13.9
70+00.00	70+50.00	2.9	46.1	0.0	2.4	26.6	-24.2
70+50.00	71+00.00	4.3	45.5	0.0	3.6	20.7	-17.0
71+00.00	71+50.00	6.9	54.1	0.0	5.8	30.3	-24.5
71+50.00	72+00.00	32.9	31.6	0.0	28.0	22.3	5.7
72+00.00	72+50.00	45.1	42.7	0.0	38.3	3.1	35.2
72+50.00	73+00.00	49.4	75.5	0.0	42.0	3.8	38.3
73+00.00	73+50.00	35.4	32.8	0.0	30.1	0.9	29.2
73+50.00	74+00.00	4.1	0.0	0.0	3.5	0.2	3.3
74+00.00	74+50.00	5.4	0.0	0.0	4.5	0.2	4.3
74+50.00	75+00.00	7.6	0.0	0.0	6.5	0.2	6.3
75+00.00	75+50.00	12.2	23.4	0.0	10.3	7.5	2.9
75+50.00	76+00.00	13.5	63.6	0.0	11.5	78.0	-66.4
76+00.00	77+00.00	17.8	80.5	0.0	15.1	141.8	-126.6
77+00.00	77+50.00	7.9	12.2	0.0	6.7	1.6	5.1
77+50.00	78+00.00	7.4	23.0	0.0	6.3	2.5	3.8
78+00.00	78+50.00	11.2	22.6	0.0	9.5	1.8	7.7
78+50.00	78+78.00	14.8	6.6	0.0	12.6	0.4	12.2
78+78.00	79+00.00	12.5	0.0	0.0	10.6	0.6	10.0
79+00.00	79+50.00	25.3	6.4	0.0	21.5	1.5	20.0
79+50.00	80+00.00	42.2	6.4	0.0	35.9	0.3	35.6
80+00.00	80+50.00	44.8	4.0	0.0	38.1	0.3	37.7
80+50.00	81+00.00	36.1	4.0	0.0	30.7	1.8	28.8
81+00.00	81+50.00	37.5	0.0	0.0	31.9	3.3	28.6
81+50.00	82+00.00	36.5	0.0	0.0	31.0	2.3	28.7
82+00.00	82+50.00	38.0	0.0	0.0	32.3	0.8	31.5
82+50.00	83+50.00	86.3	8.5	0.0	73.4	0.5	72.8
83+50.00	84+00.00	39.6	9.1	0.0	33.7	0.3	33.3
84+00.00	84+50.00	32.3	10.1	0.0	27.5	0.3	27.1
84+50.00	85+00.00	29.8	10.9	0.0	25.4	0.3	25.0
85+00.00	85+50.00	31.1	5.6	0.0	26.5	0.2	26.2
85+50.00	86+00.00	33.0	0.0	0.0	28.1	0.2	27.9
86+00.00	86+50.00	26.4	0.0	0.0	22.5	0.8	21.7
86+50.00	86+70.00	7.6	0.0	0.0	6.5	0.5	6.0
86+70.00	87+00.00	8.7	0.0	0.0	7.4	0.5	7.0
87+00.00	87+50.00	6.3	5.6	0.0	5.4	1.2	4.1
87+50.00	88+00.00	1.5	5.6	0.0	1.3	2.3	-1.0
88+00.00	END						
	SUB-TOTAL	1142.7	1396.6	0.0	971.3	664.1	307.2

STA FROM	TO STA	EARTH EXCAVATION	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS	PGES PLACEMENT	EXCAVATION TO BE USED IN EMBANKMENT (15% SHRINKAGE)	EMBANKMENT	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)
		STAGE II.1	STAGE II.1	STAGE II.1	STAGE II.1	STAGE II.1	STAGE II.1
		CU YD	CU YD	CU YD	CU YD	CU YD	CU YD
LILLIAN TEMP WIDENING							
200+50.00	201+00.00	150.6	28.7	0.0	128.0	1.6	126.4
201+00.00	201+39.02	106.3	11.8	0.0	90.3	1.3	89.0
201+39.02	201+50.00	26.6	2.7	0.0	22.6	0.1	22.4
201+50.00	202+00.00	50.2	17.5	0.0	42.7	2.3	40.4
202+00.00	202+50.00	30.2	16.7	0.0	25.7	4.7	21.0
202+50.00	203+00.00	17.3	11.7	0.0	14.7	3.3	11.4
203+00.00	203+50.00	4.9	6.2	0.0	4.2	0.4	3.8
203+50.00	END						
	SUB-TOTAL	386.0	95.2	0.0	328.1	13.7	314.4
	GRAND TOTAL	1528.6	1491.8	0.0	1299.4	677.8	621.5

NOTE: SEE GENERAL NOTES SHEET 3 FOR TEMPORARY PAVEMENT INFORMATION.

FILE NAME = \\FS-0204\AMV\VAULT\T.D. TRANS. 07\2202\21379-000\CIVIL\LOAD\GPH45\SHEET\0160H45-SHT - SCHEDULE12.DGN
 STANKIRJ
 12-12-2011 17:59:33
 TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 CHICAGO, ILLINOIS

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION FAP ROUTE 345 / US ROUTE 20	SCHEDULE OF QUANTITIES - XII EARTHWORK SCHEDULES MCLEAN AND LILLIAN TEMP PAVEMENT - STAGE II.1			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE#		DRAWN - MRK	REVISED -					345	8R-R	KANE	794	50
PLOT SCALE = #SCALE#		CHECKED - DDH	REVISED -					CONTRACT NO. 60H45				
PLOT DATE = #DATE#		DATE - 12/16/11	REVISED -					SCALE: N.T.S.	SHEET NO.	OF	STA.	TO STA.

EARTHWORK SCHEDULE

CONTINUED FROM PREVIOUS SHEET

		EARTH EXCAVATION						REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS						PGES PLACEMENT						EXCAVATION TO BE USED IN EMBANKMENT (15% SHRINKAGE)						EMBANKMENT						EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)							
STA FROM	TO STA	ST IA	ST IB	ST IC	ST IIA	ST IIB	ST III	ST IA	ST IB	ST IC	ST IIA	ST IIB	ST III	ST IA	ST IB	ST IC	ST IIA	ST IIB	ST III	ST IA	ST IB	ST IC	ST IIA	ST IIB	ST III	ST IA	ST IB	ST IC	ST IIA	ST IIB	ST III	ST IA	ST IB	ST IC	ST IIA	ST IIB	ST III		
		CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD		
143+00.00	144+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
144+00.00	145+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
145+00.00	146+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
146+00.00	147+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
147+00.00	148+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
148+00.00	149+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
149+00.00	150+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
150+00.00	151+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
151+00.00	152+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
152+00.00	153+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
153+00.00	154+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
154+00.00	155+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
155+00.00	156+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
156+00.00	156+01.87	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
156+01.87	END																																						
GRAND TOTAL		29343.6	7236.3	5659.8	41646.4	12406.4	30158.5	7998.4	3511.6	2808.9	1254.4	1774.2	1939.9	905.1	0.0	864.1	257.4	256.6	315.2	24942.1	6150.8	4810.8	35399.4	10545.4	25634.8	7301.6	7870.4	10864.8	4435.3	4592.3	818.5	17640.5	-1719.6	-6054.0	30964.1	5953.2	24816.3		

		EARTH EXCAVATION	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS	PGES PLACEMENT	EXCAVATION TO BE USED IN EMBANKMENT (15% SHRINKAGE)	EMBANKMENT	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)
STA FROM	TO STA	STAGE IV	STAGE IV	STAGE IV	STAGE IV	STAGE IV	STAGE IV
		CU YD	CU YD	CU YD	CU YD	CU YD	CU YD
US ROUTE 20							
NO EARTH WORK IN STAGE IV BEFORE STATION 135+00							
135+00.00	136+00.00	19.0	0.0	0.0	16.1	4.5	11.6
136+00.00	137+00.00	51.8	0.0	0.0	44.1	9.1	35.0
137+00.00	138+00.00	89.0	0.0	0.0	75.6	9.1	66.5
138+00.00	138+93.00	106.8	0.0	0.0	90.8	8.9	81.9
138+93.00	139+00.00	7.3	0.0	0.0	6.2	0.9	5.3
139+00.00	140+00.00	96.0	0.0	0.0	81.6	20.1	61.5
140+00.00	140+24.00	23.8	0.0	0.0	20.3	6.2	14.1
140+24.00	141+00.00	165.0	0.0	0.0	140.3	25.9	114.4
141+00.00	142+00.00	306.7	0.0	0.0	260.7	47.1	213.6
142+00.00	143+00.00	305.2	0.0	0.0	259.4	45.8	213.6
143+00.00	144+00.00	294.0	0.0	0.0	249.9	19.8	230.1
144+00.00	145+00.00	228.1	0.0	0.0	193.9	3.2	190.7
145+00.00	146+00.00	190.5	0.0	0.0	161.9	3.1	158.8
146+00.00	147+00.00	197.0	0.0	0.0	167.4	0.9	166.5
147+00.00	148+00.00	212.6	0.0	0.0	180.8	0.8	179.9
148+00.00	149+00.00	187.1	0.0	0.0	159.1	0.9	158.2
149+00.00	150+00.00	149.4	0.0	0.0	127.0	0.8	126.3
150+00.00	151+00.00	91.1	0.0	0.0	77.4	0.7	76.7
151+00.00	152+00.00	87.9	0.0	0.0	74.7	0.7	74.1
152+00.00	153+00.00	79.4	0.0	0.0	67.5	0.7	66.8
153+00.00	154+00.00	39.5	0.0	0.0	33.6	0.7	32.9
154+00.00	155+00.00	33.6	0.0	0.0	28.5	0.7	27.8
155+00.00	156+00.00	17.7	0.0	0.0	15.0	0.7	14.4
156+00.00	156+01.87	0.4	0.0	0.0	0.3	0.0	0.3
156+01.87	END						
GRAND TOTAL		2978.9	0.0	0.0	2532.1	211.1	2321.0

NOTE: SEE GENERAL NOTES SHEET 3 FOR TEMPORARY PAVEMENT INFORMATION.

\\FS-0644\AM\VALU\JD-TRANS\07-2282-21379-001\CIVIL\CAD\60H45\BHEE\1\B60H45-SHT-SCHEDULE15.DGN
 STANKTRJ
 12-12-2011, 17:59:41

FILE NAME -	USER NAME - #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION FAP ROUTE 345 / US ROUTE 20	SCHEDULE OF QUANTITIES - XV EARTHWORK SCHEDULES U.S. ROUTE 20 - STAGES IA TO III AND IV			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILEL#		DRAWN - MRK	REVISED -		345	8R-R	KANE	794	53			
PLOT SCALE = #SCALE#		CHECKED - DDH	REVISED -		CONTRACT NO. 60H45							
PLOT DATE = #DATE#		DATE - 12/16/11	REVISED -		SCALE: N.T.S.	SHEET NO.	OF	STA.	TO STA.	FED. ROAD DIST. NO.	[ILLINOIS] FED. AID PROJECT	



		EARTH EXCAVATION			REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS			PGES PLACEMENT			EXCAVATION TO BE USED IN EMBANKMENT (15% SHRINKAGE)			EMBANKMENT			EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)		
STA FROM	TO STA	STAGE II.2	STAGE II.3	STAGE II.4	STAGE II.2	STAGE II.3	STAGE II.4	STAGE II.2	STAGE II.3	STAGE II.4	STAGE II.2	STAGE II.3	STAGE II.4	STAGE II.2	STAGE II.3	STAGE II.4	STAGE II.2	STAGE II.3	STAGE II.4
		CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD
MCLEAN BLVD																			
58+00.00	58+50.00	0.0	0.0	0.5	0.0	0.0	25.6	0.0	0.0	0.0	0.0	0.0	0.4	0.0	0.0	32.4	0.0	0.0	-31.9
58+50.00	59+00.00	0.0	0.0	2.1	0.0	0.0	27.9	0.0	0.0	0.0	0.0	0.0	1.8	0.0	0.0	44.5	0.0	0.0	-42.7
59+00.00	59+50.00	0.0	0.0	3.9	0.0	0.0	16.1	0.0	0.0	0.0	0.0	0.0	3.3	0.0	0.0	34.3	0.0	0.0	-31.0
59+50.00	59+73.25	15.7	8.6	9.6	6.4	4.0	7.4	6.4	4.0	3.1	13.3	7.3	8.2	0.0	0.0	13.4	13.3	7.3	-5.3
59+73.25	60+00.00	37.3	20.7	22.4	14.8	9.4	9.7	14.8	9.4	7.3	31.7	17.6	19.0	0.0	0.0	11.9	31.6	17.6	7.1
60+00.00	60+50.00	72.6	41.8	48.4	28.1	18.6	17.2	28.1	18.6	14.0	61.7	35.5	41.2	0.1	0.0	16.7	61.6	35.5	24.5
60+50.00	61+00.00	75.0	43.9	51.6	29.2	19.7	17.5	29.2	19.7	14.6	63.7	37.3	43.9	0.1	0.0	15.5	63.6	37.3	28.4
61+00.00	61+50.00	74.5	41.2	48.8	30.7	20.2	20.4	30.7	20.2	15.0	63.3	35.0	41.4	0.1	0.0	19.1	63.2	35.0	22.3
61+50.00	62+00.00	69.8	34.2	44.9	31.5	20.3	19.7	31.5	20.3	15.3	59.3	29.1	38.2	0.1	0.0	22.5	59.2	29.1	15.6
62+00.00	62+50.00	71.6	32.2	42.7	15.7	10.1	11.4	15.7	10.1	7.7	60.9	27.4	36.3	0.1	0.0	24.5	60.8	27.4	11.9
62+50.00	63+00.00	69.9	41.7	38.1	0.0	0.0	6.2	0.0	0.0	0.0	59.4	35.5	32.4	0.1	0.0	34.8	59.3	35.5	-2.5
63+00.00	63+50.00	63.4	54.0	39.2	0.0	0.0	2.9	0.0	0.0	0.0	53.9	45.9	33.3	0.1	0.0	43.3	53.8	45.9	-10.0
63+50.00	64+00.00	73.6	63.1	44.6	0.0	0.0	1.6	0.0	0.0	0.0	62.5	53.6	37.9	0.0	0.0	37.0	62.5	53.6	0.9
64+00.00	64+50.00	123.0	74.3	64.7	0.0	0.0	19.3	0.0	0.0	0.0	104.5	63.2	55.0	0.0	0.0	31.9	104.5	63.2	23.1
64+50.00	65+00.00	206.6	88.7	110.7	0.0	0.0	17.7	0.0	0.0	0.0	175.6	75.4	94.1	0.8	0.0	17.0	174.8	75.4	77.2
65+00.00	65+50.00	201.8	84.8	170.5	40.7	0.0	16.2	0.0	0.0	0.0	171.5	72.1	144.9	13.5	0.0	0.0	158.0	72.1	144.9
65+50.00	66+00.00	137.5	71.1	168.8	72.4	0.0	21.2	0.0	0.0	0.0	116.8	60.5	143.5	25.4	0.0	3.6	91.5	60.5	139.9
66+00.00	66+50.00	112.4	60.7	114.5	63.4	0.0	13.3	0.0	0.0	0.0	95.5	51.6	97.3	22.6	0.0	10.6	72.9	51.6	86.8
66+50.00	67+00.00	87.9	45.0	86.2	63.3	0.0	19.8	0.0	0.0	0.0	74.7	38.3	73.2	18.8	0.0	17.7	56.0	38.3	55.5
67+00.00	67+50.00	70.5	35.1	79.5	63.3	0.0	26.3	0.0	0.0	0.0	59.9	29.9	67.6	20.1	0.0	20.8	39.7	29.9	46.8
67+50.00	68+00.00	82.3	32.5	94.7	31.7	0.0	14.8	0.0	0.0	0.0	69.9	27.6	80.5	11.4	0.0	10.1	58.5	27.6	70.4
68+00.00	68+50.00	78.3	38.4	112.3	31.2	0.0	13.3	0.0	0.0	0.0	66.6	32.6	95.4	6.9	0.0	7.5	59.6	32.6	87.9
68+50.00	69+00.00	66.3	47.7	120.5	62.3	0.0	31.3	0.0	0.0	0.0	56.4	40.6	102.4	15.7	0.0	21.3	40.7	40.6	81.2
69+00.00	69+50.00	67.0	52.6	127.8	62.0	0.0	29.4	0.0	0.0	0.0	57.0	44.7	108.6	17.7	0.0	21.8	39.3	44.7	86.8
69+50.00	70+00.00	57.9	60.6	137.5	62.0	0.0	23.7	0.0	0.0	0.0	49.2	51.5	116.9	15.6	0.0	17.8	33.6	51.5	99.1
70+00.00	70+50.00	63.2	71.7	152.4	63.2	0.0	24.2	0.0	0.0	0.0	53.7	61.0	129.6	11.6	0.0	11.1	42.2	61.0	118.4
70+50.00	71+00.00	78.8	80.0	165.7	64.1	0.0	23.6	0.0	0.0	0.0	67.0	68.0	140.9	7.5	0.0	2.4	59.5	68.0	138.5
71+00.00	71+50.00	108.0	85.7	176.9	62.8	0.0	11.9	0.0	0.0	0.0	91.8	72.8	150.3	3.4	0.0	5.3	88.3	72.8	145.0
71+50.00	72+00.00	117.3	87.3	239.0	30.6	0.0	0.3	0.0	0.0	0.0	99.7	74.2	203.2	6.1	0.0	4.4	93.6	74.2	198.8
72+00.00	72+50.00	222.2	91.2	331.4	0.0	0.0	7.7	0.0	0.0	0.0	188.9	77.6	281.7	5.4	0.0	0.0	183.5	77.6	281.7
72+50.00	73+00.00	214.7	96.9	314.9	82.9	0.0	32.3	0.0	0.0	0.0	182.5	82.3	267.7	74.8	0.0	0.0	107.7	82.3	267.7
73+00.00	73+50.00	167.5	99.4	304.0	82.9	0.0	24.6	0.0	0.0	0.0	142.4	84.5	258.4	74.8	0.0	1.4	67.6	84.5	257.0
73+50.00	74+00.00	341.6	99.2	321.0	0.0	0.0	0.0	0.0	0.0	0.0	290.4	84.3	272.8	0.0	0.0	1.4	290.4	84.3	271.4
74+00.00	74+50.00	322.5	95.9	269.2	0.0	0.0	0.0	0.0	0.0	0.0	274.1	81.5	228.8	1.7	0.0	0.0	272.4	81.5	228.8
74+50.00	75+00.00	237.4	92.3	353.5	0.0	0.0	0.0	0.0	0.0	0.0	201.8	78.5	300.5	1.7	0.0	1.6	200.0	78.5	298.9
75+00.00	75+50.00	266.9	89.4	379.7	0.0	0.0	71.9	0.0	0.0	0.0	226.9	76.0	322.8	0.0	0.0	1.6	226.9	76.0	321.2
75+50.00	76+00.00	182.6	86.0	295.3	43.5	0.0	136.6	0.0	0.0	0.0	155.2	73.1	251.0	45.4	0.0	41.1	109.8	73.1	209.9
76+00.00	76+50.00	117.3	77.2	275.0	43.5	0.0	107.8	0.0	0.0	0.0	99.7	65.6	233.8	69.4	0.0	45.5	30.4	65.6	188.3
76+50.00	77+00.00	123.1	60.6	187.3	12.9	0.0	43.1	0.0	0.0	0.0	104.6	51.5	159.2	23.9	0.0	4.4	80.7	51.5	154.9
77+00.00	77+50.00	99.6	43.5	106.4	31.9	0.0	0.0	0.0	0.0	0.0	84.6	37.0	90.5	1.7	0.0	1.7	82.9	37.0	88.8
77+50.00	78+00.00	83.3	29.1	90.2	35.9	0.0	0.0	0.0	0.0	0.0	70.8	24.8	76.6	6.5	0.0	2.5	64.3	24.8	74.1
78+00.00	78+50.00	99.4	13.9	83.6	16.9	0.0	0.0	0.0	0.0	0.0	84.5	11.8	71.1	8.7	0.8	0.9	75.7	11.1	70.2
78+50.00	78+78.00	49.8	3.8	52.2	0.0	0.0	0.0	0.0	0.0	0.0	42.3	3.2	44.4	4.8	0.6	0.0	37.5	2.6	44.4
78+78.00	79+00.00	25.1	3.6	51.5	0.0	0.0	0.0	0.0	0.0	0.0	21.3	3.1	43.8	5.6	0.3	0.0	15.8	2.8	43.8
79+00.00	79+50.00	71.6	10.3	110.7	0.0	0.0	0.0	0.0	0.0	0.0	60.9	8.8	94.1	10.8	0.2	0.0	50.0	8.6	94.1
79+50.00	80+00.00	88.5	12.2	93.4	15.9	0.0	0.0	0.0	0.0	0.0	75.2	10.3	79.4	3.4	0.0	0.0	71.9	10.3	79.4
80+00.00	80+50.00	97.1	13.4	78.3	25.2	0.0	0.0	0.0	0.0	0.0	82.5	11.4	66.5	0.5	0.0	0.1	82.1	11.4	66.4
80+50.00	81+00.00	104.8	16.4	75.9	16.8	0.0	0.0	0.0	0.0	0.0	89.1	13.9	64.5	0.0	0.0	0.1	89.1	13.9	64.4
81+00.00	81+50.00	94.5	17.3	74.7	7.5	0.0	0.0	0.0	0.0	0.0	80.4	14.7	63.5	0.0	0.0	0.3	80.4	14.7	63.2
81+50.00	82+00.00	89.2	15.5	60.6	6.8	0.0	0.0	0.0	0.0	0.0	75.8	13.1	51.5	3.2	0.0	0.4	72.7	13.1	51.1
82+00.00	82+50.00	139.9	13.9	55.8	6.8	0.0	0.0	0.0	0.0	0.0	118.9	11.8	47.4	4.8	0.0	0.1	114.1	11.8	47.3
82+50.00	83+00.00	154.3	14.1	49.6	0.0	0.0	0.0	0.0	0.0	0.0	131.1	12.0	42.2	1.7	0.0	0.0	129.5	12.0	42.2
83+00.00	83+50.00	101.8	21.1	43.3	34.6	0.0	0.0	0.0	0.0	0.0	86.5	17.9	36.8	3.7	0.0	16.3	82.8	17.9	20.5
83+50.00	84+00.00	64.2	30.0	33.9	57.8	0.0	0.0	0.0	0.0	0.0	54.6	25.5	28.8	7.1	0.0	29.8	47.5	25.5	-1.0
84+00.00	84+50.00	46.7	33.5	27.6	45.3	0.0	0.0	0.0	0.0	0.0	39.7	28.4	23.5	9.5	0.0	26.1	30.3	28.4	-2.7
84+50.00	85+00.00	48.1	36.1	28.6	44.4	0.0	0.0	0.0	0.0	0.0	40.9	30.7	24.3	11.7	0.0	24.1	29.2	30.7	0.3
85+00.00	85+50.00	57.0	33.2	43.8	44.6	0.0	0.0	0.0	0.0	0.0	48.5	28.2	37.2	8.1	0.0	14.7	40.4	28.2	22.6
85+50.00	86+00.00	62.4	29.0	58.3	44.2	0.0	0.0	0.0	0.0	0.0	53.0	24.6	49.5	5.9	0.0	5.5	47.1	24.6	44.0
86+00.00	86+50.00	75.8	32.2	60.5	21.9	0.0	0.0	0.0	0.0	0.0	64.5	27.4	51.4	3.9	0.0	5.2	60.5	27.4	46.3
86+50.00	86+70.00	36.7	13.9	25.4	0.0	0.0	0.0	0.0	0.0	0.0	31.2	11.8	21.6	0.2	0.0	1.9	31.0	11.8	19.7
86+70.00	87+00.00	37.8	10.6	35.9	3.2	0.0	0.0	0.0	0.0	0.0	32.1	9.0	30.5	0.0	0.0	1.0	32.1	9.0	29.5
87+00.00	87+50.00	28.2	0.0	37.6	10.6	0.0	12.6	0.0	0.0	0.0	24.0	0.0	32.0	0.0	0.0	6.1	24.0	0.0	25.9
87+50.00	88+00.00	23.2	0.0	32.3	10.5	0.0	12.6	0.0	0.0	0.0	19.8	0.0	27.4	0.5	0.0	6.0	19.2	0.0	21.4
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CONTINUED FROM PREVIOUS SHEET

		EARTH EXCAVATION			REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS			PGES PLACEMENT			EXCAVATION TO BE USED IN EMBANKMENT (15% SHRINKAGE)			EMBANKMENT			EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)		
STA FROM	TO STA	STAGE II.2	STAGE II.3	STAGE II.4	STAGE II.2	STAGE II.3	STAGE II.4	STAGE II.2	STAGE II.3	STAGE II.4	STAGE II.2	STAGE II.3	STAGE II.4	STAGE II.2	STAGE II.3	STAGE II.4	STAGE II.2	STAGE II.3	STAGE II.4
		CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD	CU YD
FLEETWOOD DR																			
301+15.23	301+50.00	0.0	0.0	0.0	10.2	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	2.3	0.0	0.0	-2.3	0.0	0.0
301+50.00	302+00.00	0.0	0.0	0.0	14.6	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	2.4	0.0	0.0	-2.4	0.0	0.0
302+00.00	302+42.26	0.0	0.0	0.0	12.2	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.7	0.0	0.0	-0.7	0.0	0.0
302+42.26	302+50.00	0.0	0.0	0.0	2.2	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.0	0.0	-0.1	0.0	0.0
302+50.00	303+00.00	4.5	0.0	0.0	13.5	0.0	0.0	0.0	0.0	0.0	3.9	0.0	0.0	0.4	0.0	0.0	3.4	0.0	0.0
303+00.00	303+50.00	9.3	0.0	0.0	12.4	0.0	0.0	0.0	0.0	0.0	7.9	0.0	0.0	0.1	0.0	0.0	7.8	0.0	0.0
303+50.00	304+00.00	6.2	0.0	0.0	19.9	0.0	0.0	0.0	0.0	0.0	5.3	0.0	0.0	0.1	0.0	0.0	5.1	0.0	0.0
304+00.00	304+50.00	4.9	0.0	0.0	26.7	0.0	0.0	0.0	0.0	0.0	4.2	0.0	0.0	0.1	0.0	0.0	4.1	0.0	0.0
304+50.00	305+00.00	7.4	0.0	0.0	22.5	0.0	0.0	0.0	0.0	0.0	6.2	0.0	0.0	0.1	0.0	0.0	6.1	0.0	0.0
305+00.00	305+50.00	5.3	0.0	0.0	18.8	0.0	0.0	0.0	0.0	0.0	4.5	0.0	0.0	1.1	0.0	0.0	3.4	0.0	0.0
305+50.00	306+00.00	1.4	0.0	0.0	23.2	0.0	0.0	0.0	0.0	0.0	1.2	0.0	0.0	1.6	0.0	0.0	-0.5	0.0	0.0
306+00.00	306+50.00	1.7	0.0	0.0	37.5	0.0	0.0	0.0	0.0	0.0	1.4	0.0	0.0	1.2	0.0	0.0	0.3	0.0	0.0
306+50.00	307+00.00	1.7	0.0	0.0	23.4	0.0	0.0	0.0	0.0	0.0	1.4	0.0	0.0	0.5	0.0	0.0	0.9	0.0	0.0
307+00.00	307+50.00	0.3	0.0	0.0	10.5	0.0	0.0	0.0	0.0	0.0	0.2	0.0	0.0	2.1	0.0	0.0	-1.9	0.0	0.0
307+50.00	307+83.84	0.4	0.0	0.0	10.9	0.0	0.0	0.0	0.0	0.0	0.3	0.0	0.0	2.0	0.0	0.0	-1.6	0.0	0.0
307+83.84	308+00.00	0.3	0.0	0.0	5.1	0.0	0.0	0.0	0.0	0.0	0.3	0.0	0.0	0.6	0.0	0.0	-0.4	0.0	0.0
308+00.00	308+50.00	0.7	0.0	0.0	15.0	0.0	0.0	0.0	0.0	0.0	0.6	0.0	0.0	1.7	0.0	0.0	-1.1	0.0	0.0
308+50.00	309+00.00	0.1	0.0	0.0	4.6	0.0	0.0	0.0	0.0	0.0	0.1	0.0	0.0	0.5	0.0	0.0	-0.5	0.0	0.0
309+00.00	END																		
McLEAN BLVD INTERSECTION																			
310+70.00	311+00.00	0.0	0.0	148.5	0.0	0.0	52.2	0.0	0.0	0.0	0.0	0.0	126.3	0.0	0.0	9.4	0.0	0.0	116.9
311+00.00	311+50.00	0.0	0.0	183.4	0.0	0.0	49.9	0.0	0.0	0.0	0.0	0.0	155.9	0.0	0.0	8.1	0.0	0.0	147.8
311+50.00	312+00.00	0.0	0.0	175.5	0.0	0.0	26.0	0.0	0.0	0.0	0.0	0.0	149.2	0.0	0.0	8.9	0.0	0.0	140.2
312+00.00	312+44.84	0.0	0.0	146.9	0.0	0.0	12.6	0.0	0.0	0.0	0.0	0.0	124.9	0.0	0.0	7.8	0.0	0.0	117.1
312+44.84	312+50.00	0.0	0.0	7.5	0.0	0.0	1.0	0.0	0.0	0.0	0.0	0.0	6.3	0.0	0.0	0.6	0.0	0.0	5.7
312+50.00	312+64.84	0.0	0.0	0.0	0.0	0.0	1.4	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.9	0.0	0.0	-0.9
312+64.84	END																		
	SUB-TOTAL	44.0	0.0	661.9	283.3	0.0	143.0	0.0	0.0	0.0	37.4	0.0	562.6	17.7	0.0	35.7	19.7	0.0	526.9
LILLIAN ST																			
200+50.00	201+00.00	0.0	0.0	298.4	0.0	0.0	95.2	0.0	0.0	95.2	0.0	0.0	253.6	0.0	0.0	2.6	0.0	0.0	251.0
201+00.00	201+50.00	0.0	0.0	189.6	0.0	0.0	61.7	0.0	0.0	61.7	0.0	0.0	161.2	0.0	0.0	8.9	0.0	0.0	152.3
201+50.00	202+00.00	0.0	0.0	158.4	0.0	0.0	55.6	0.0	0.0	55.6	0.0	0.0	134.6	0.0	0.0	13.7	0.0	0.0	120.9
202+00.00	202+50.00	0.0	0.0	149.0	0.0	0.0	58.1	0.0	0.0	53.2	0.0	0.0	126.6	0.0	0.0	10.2	0.0	0.0	116.4
202+50.00	203+00.00	0.0	0.0	147.0	0.0	0.0	49.4	0.0	0.0	26.5	0.0	0.0	125.0	0.0	0.0	4.7	0.0	0.0	120.3
203+00.00	203+50.00	0.0	0.0	204.1	0.0	0.0	46.1	0.0	0.0	0.0	0.0	0.0	173.5	0.0	0.0	3.5	0.0	0.0	170.0
203+50.00	204+00.00	0.0	0.0	131.9	0.0	0.0	28.1	0.0	0.0	0.0	0.0	0.0	112.1	0.0	0.0	1.6	0.0	0.0	110.5
204+00.00	END																		
	SUB-TOTAL	0.0	0.0	1278.5	0.0	0.0	394.1	0.0	0.0	292.2	0.0	0.0	1086.7	0.0	0.0	45.3	0.0	0.0	1041.4
	GRAND TOTAL:	6348.9	2732.4	8960.3	1964.6	102.2	1487.7	156.5	102.2	369.2	5396.6	2322.5	7616.3	615.8	1.9	877.9	4780.8	2320.7	6738.4

NOTE: SEE GENERAL NOTES SHEET 3 FOR TEMPORARY PAVEMENT INFORMATION.

\\FS-004\A\VAL\T.D. TRANS. 07\2202\21379-001\ACVIL\LOAD\60H45\SHEET.DWG\H45-SHT-SCHEDULE17.DGN
 STANKRZJ
 12-12-2011 10:05:57
 TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS

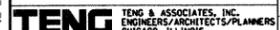
FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION FAP ROUTE 345 / US ROUTE 20	SCHEDULE OF QUANTITIES - XVII EARTHWORK SCHEDULES FLEETWOOD AND LILLIAN - STAGES II.2 TO II.4				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILEL#		DRAWN - MRK	REVISED -		345	8R-R	KANE	794	55				
PLOT SCALE = #SCALE#		CHECKED - DDH	REVISED -		SCALE: N.T.S. SHEET NO. OF STA. TO STA.				FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
PLOT DATE = #DATE#		DATE - 12/16/11	REVISED -		CONTRACT NO. 60H45								

COORDINATES

PROP. US RTE 20

DESCRIPTION	NORTHING	EASTING	STATION	OFFSET
P.O.T.	1,952,952.3492	985,917.9827	53+43.42	0.00 FT
P.C. P20-1	1,951,510.2513	989,058.7084	87+99.40	0.00 FT
P.I. P20-1	1,951,381.5199	989,339.0708	91+07.91	4.52 FT
P.T. P20-1	1,951,269.4383	989,626.4950	94+16.23	0.00 FT
P.C. P20-2	1,951,269.4383	989,626.4950	94+16.23	0.00 FT
P.I. P20-2	1,950,854.9009	990,689.5427	105+57.25	100.20 FT
P.T. P20-2	1,950,830.4201	991,830.2935	116+74.87	0.00 FT
P.O.T.	1,950,748.7368	995,636.5447	154+82.00	0.00 FT
P.O.T.	1,950,742.3002	995,936.4756	157+82.00	0.00 FT
PROP. RAMP A				
P.O.T.	1,950,812.2984	993,979.7469	0+00.00	0.00 FT
P.C. RAMP A-1	1,950,868.0393	991,804.4234	21+76.04	0.00 FT
P.I. RAMP A-1	1,950,872.1942	991,642.2769	23+38.24	-2.05 FT
P.T. RAMP A-1	1,950,884.5458	991,480.5482	25+00.37	0.00 FT
P.C. RAMP A-2	1,950,884.5458	991,480.5482	25+00.37	0.00 FT
P.I. RAMP A-2	1,950,901.8787	991,253.5662	27+28.01	-11.91 FT
P.T. RAMP A-2	1,950,965.9340	991,035.1213	29+53.99	0.00 FT
P.C. RAMP A-3	1,951,132.7776	990,466.1424	35+46.93	0.00 FT
P.I. RAMP A-3	1,951,198.1004	990,243.3750	37+79.08	-10.68 FT
P.T. RAMP A-3	1,951,303.0670	990,036.3137	40+09.92	0.00 FT
PROP. RAMP B				
P.O.T.	1,950,942.7889	989,987.6730	0+00.00	0.00 FT
P.C. RAMP B-1	1,950,883.4435	990,443.3082	4+59.48	0.00 FT
P.I. RAMP B-1	1,950,818.0256	990,945.5656	9+65.98	10.03 FT
P.T. RAMP B-1	1,950,792.5327	991,451.4235	14+71.95	0.00 FT
P.C. RAMP B-2	1,950,792.5327	991,451.4235	14+71.95	0.00 FT
P.I. RAMP B-2	1,950,780.4851	991,690.4849	17+11.32	3.18 FT
P.T. RAMP B-2	1,950,781.1454	991,929.8488	19+50.57	0.00 FT
P.O.T.	1,950,782.4548	992,404.5281	24+25.25	0.00 FT
PROP. RAMP C				
P.O.T.	1,951,804.8818	988,311.5907	0+00.00	0.00 FT
P.C. RAMP C-1	1,951,079.8838	989,708.4331	15+73.78	0.00 FT
P.I. RAMP C-1	1,951,009.3124	989,844.4019	17+26.97	11.36 FT
P.T. RAMP C-1	1,950,981.4837	989,995.0450	18+77.92	0.00 FT
PROP. RAMP D				
P.O.T.	1,951,299.0820	990,036.0173	0+00.00	0.00 FT
P.C. RAMP D-1	1,951,302.3530	989,992.4277	0+43.71	0.00 FT
P.I. RAMP D-1	1,951,309.8363	989,892.7044	1+43.72	-6.64 FT
P.T. RAMP D-1	1,951,343.1875	989,798.4259	2+42.55	0.00 FT
P.C. RAMP D-2	1,951,558.8016	989,188.9198	8+89.07	0.00 FT
P.I. RAMP D-2	1,951,650.8917	988,928.5958	11+65.20	-4.86 FT
P.T. RAMP D-2	1,951,761.0743	988,675.3983	14+41.10	0.00 FT
P.O.T.	1,952,239.9310	987,574.9946	26+41.18	0.00 FT
PROP. LILLIAN STREET				
P.O.T. LILLIAN-1	1,951,969.3306	989,746.1789	196+80.00	0.00 FT
P.O.T. LILLIAN-2	1,951,974.2192	990,066.1416	200+00.00	0.00 FT
P.O.T. LILLIAN-3	1,951,988.3358	990,990.0844	209+24.05	0.00 FT
PROP. FLEETWOOD DRIVE				
P.O.T.	1,950,185.2675	988,758.6544	300+00.00	0.00 FT
P.O.T.	1,950,187.4810	988,973.8430	302+15.20	0.00 FT
P.C. FLEETWOOD-1	1,950,197.0839	989,907.4238	311+48.83	0.00 FT
P.I. FLEETWOOD-1	1,950,198.3037	990,026.0038	312+67.42	22.59 FT
P.T. FLEETWOOD-1	1,950,280.2716	990,111.7008	313+74.69	0.00 FT
P.O.T.	1,950,317.1212	990,150.2269	314+28.00	0.00 FT
PROP. MCLEAN BLVD				
P.O.T.	1,948,495.2410	989,416.1420	0+00.00	0.00 FT
P.C. PMC LEAN-1	1,948,991.0447	989,464.3844	52+29.48	0.00 FT
P.I. PMC LEAN-1	1,949,490.6559	989,512.9980	57+31.44	-29.94 FT
P.T. PMC LEAN-1	1,949,964.6734	989,678.1558	62+28.66	0.00 FT
P.C. PMC LEAN-2	1,950,536.5081	989,877.3952	68+34.21	0.00 FT
P.I. PMC LEAN-2	1,950,941.5867	990,018.5333	72+63.17	30.51 FT
P.T. PMC LEAN-2	1,951,369.9864	990,040.4944	76+86.36	0.00 FT
P.C. PMC LEAN-3	1,951,369.9864	990,040.4944	76+86.36	0.00 FT
P.I. PMC LEAN-3	1,952,022.7217	990,073.9557	83+39.95	6.21 FT
P.T. PMC LEAN-3	1,952,676.2573	990,082.5769	89+93.38	0.00 FT
P.O.T.	1,953,372.0776	990,091.7559	96+89.26	0.00 FT

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TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 CHICAGO, ILLINOIS

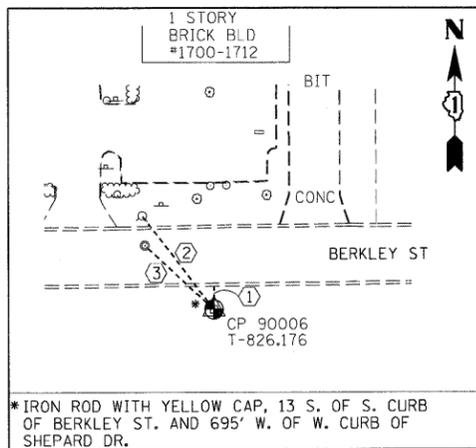
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	DATE - 12/16/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 FAP ROUTE 345 / US ROUTE 20

ALIGNMENT, TIES AND BENCHMARKS
II

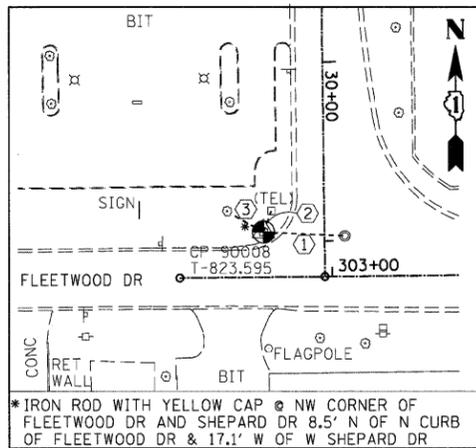
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	BR-R	KANE	794	57
CONTRACT NO. 6045				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				



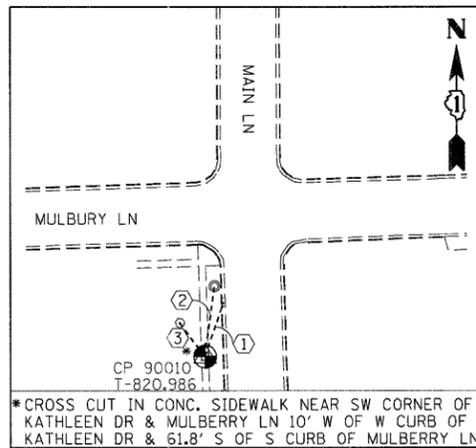
*IRON ROD WITH YELLOW CAP, 13 S. OF S. CURB OF BERKLEY ST. AND 695' W. OF W. CURB OF SHEPARD DR.

- (N 1950816.625, E 988336.015, ELEV. 826.040)
- ① 13.05' TO THE BACK OF CURB
 - ② 66.30' TO THE CENTER OF FIRE HYDRANT
 - ③ 52.20' TO THE CENTER OF WATER VALVE VAULT (MH)



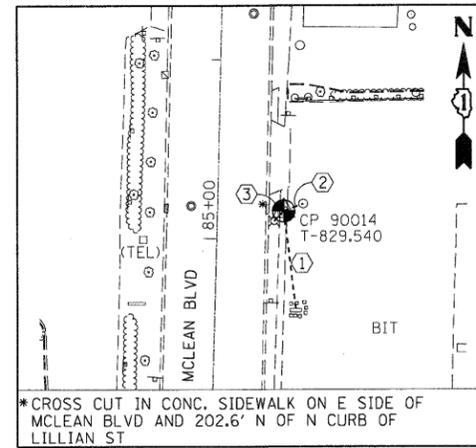
*IRON ROD WITH YELLOW CAP @ NW CORNER OF FLEETWOOD DR AND SHEPARD DR 8.5' N OF N CURB OF FLEETWOOD DR & 17.1' W OF W SHEPARD DR

- (N 1950212.996, E 989020.547, ELEV. 823.480)
- ① 45.30' TO THE CENTER OF WATER VALVE VAULT (MH)
 - ② 12.40' TO THE CENTER OF TELEPHONE CABLE BOX
 - ③ 20.50' TO THE PK NAIL SET ON SOUTH FACE OF 12" TREE



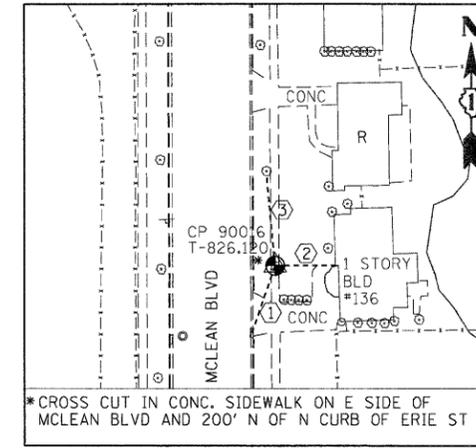
*CROSS CUT IN CONC. SIDEWALK NEAR SW CORNER OF KATHLEEN DR & MULBERRY LN 10' W OF W CURB OF KATHLEEN DR & 61.8' S OF S CURB OF MULBERRY LN

- (N 1952288.720, E 988592.527, ELEV. 820.880)
- ① 31.90' TO THE CENTER OF GUTTER INLET
 - ② 39.50' TO THE CENTER OF CATCH BASIN (MH)
 - ③ 23.40' TO THE PK NAIL SET IN EAST FACE OF 6" TREE



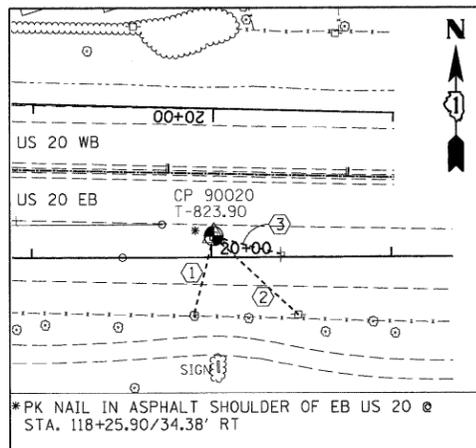
*CROSS CUT IN CONC. SIDEWALK ON E SIDE OF MCLEAN BLVD AND 202.6' N OF N CURB OF FLEETWOOD ST

- (N 1952198.206, E 990110.538, ELEV. 829.370)
- ① 54.45' TO THE NW CORNER OF PIETROS PIZZA'S SIGN
 - ② 11.60' TO THE CENTER OF CATCH BASIN
 - ③ 7.80' TO THE CENTER OF LIGHT POLE



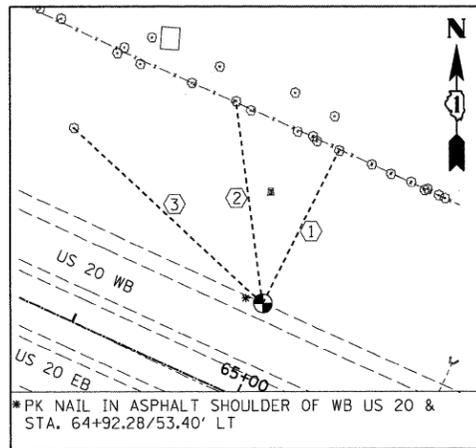
*CROSS CUT IN CONC. SIDEWALK ON E SIDE OF MCLEAN BLVD AND 200' N OF N CURB OF ERIE ST

- (N 1953522.787, E 990122.104, ELEV. 825.980)
- ① 39.10' TO THE CENTER OF GUTTER INLET
 - ② 33.85' TO THE CORNER OF THE 1 STORY FRAME BLD #169
 - ③ 51.60' TO THE PK NAIL SET IN SOUTH FACE OF 26" TREE



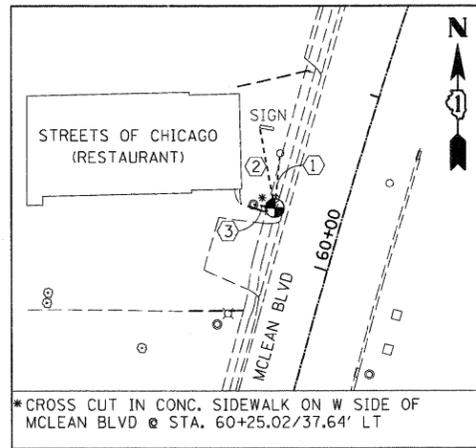
*PK NAIL IN ASPHALT SHOULDER OF EB US 20 @ STA. 118+25.90/34.38' RT

- (N 1950792.807, E 991980.556, ELEV. 823.810)
- ① 45.00' TO THE PK NAIL SET IN NORTH FACE OF 16" TREE
 - ② 64.10' TO THE PK NAIL SET IN NORTH FACE OF POWER POLE
 - ③ 38.70' TO THE CENTER OF SNTG (EMERGENCY PARKING ONLY)



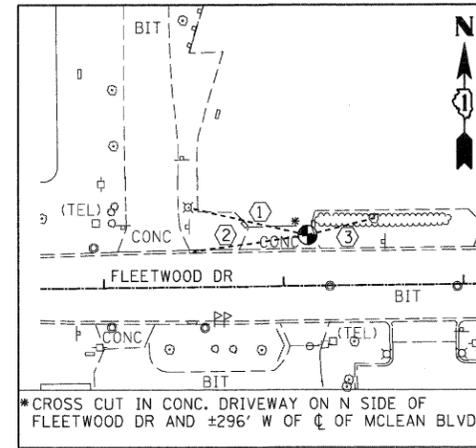
*PK NAIL IN ASPHALT SHOULDER OF WB US 20 @ STA. 64+92.28/53.40' LT

- (N 1952521.485, E 986984.329, ELEV. 823.490)
- ① 94.20' TO THE PK NAIL SET IN SOUTH FACE OF 10" TREE
 - ② 110.85' TO THE PK NAIL SET IN SOUTH FACE OF 12" TREE
 - ③ 142.50' TO THE PK NAIL SET IN EAST FACE OF 12" TREE



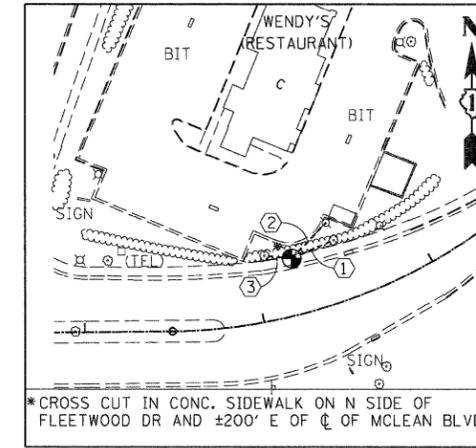
*CROSS CUT IN CONC. SIDEWALK ON W SIDE OF MCLEAN BLVD @ STA. 60+25.02/37.64' LT

- (N 1949781.472, E 989579.743, ELEV. 826.940)
- ① 30.40' TO THE CENTER OF WATER BUFFALO BOX
 - ② 44.35' TO THE SW CORNER OF RESTAURANT'S SIGN
 - ③ 12.00' TO THE CENTER OF SANITARY MANHOLE



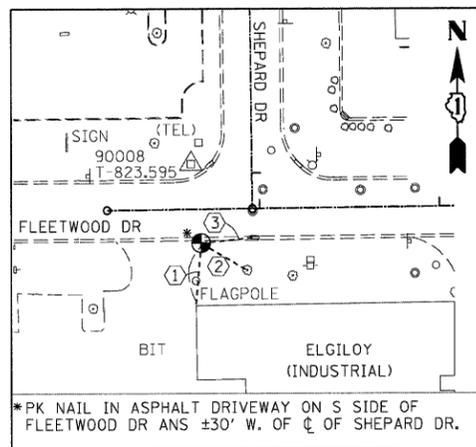
*CROSS CUT IN CONC. DRIVEWAY ON N SIDE OF FLEETWOOD DR AND ±296' W OF CL OF MCLEAN BLVD

- (N 1950220.667, E 989471.903, ELEV. 825.320)
- ① 68.20' TO THE CENTER OF LIGHT POLE
 - ② 64.20' TO THE CENTER OF GUTTER INLET
 - ③ 37.50' TO THE PK NAIL SET IN SOUTH FACE OF 16" TREE



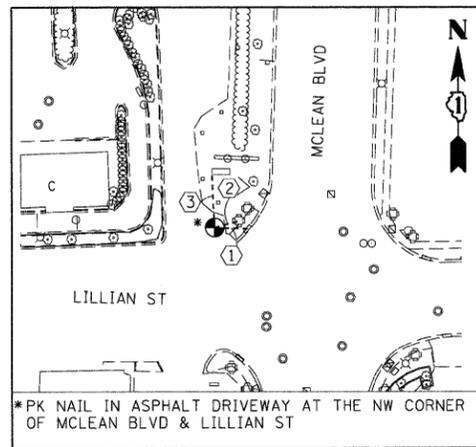
*CROSS CUT IN CONC. SIDEWALK ON N SIDE OF FLEETWOOD DR AND ±200' E OF CL OF MCLEAN BLVD

- (N 1950237.454, E 989973.590, ELEV. 824.710)
- ① 25.30' TO THE PK NAIL SET IN SOUTH FACE OF 12" TREE
 - ② 27.45' TO THE CENTER OF LIGHT POLE
 - ③ 15.35' TO THE PK NAIL SET IN SOUTH FACE OF 14" TREE



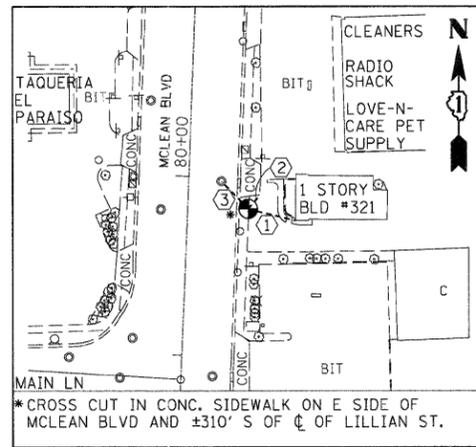
*PK NAIL IN ASPHALT DRIVEWAY ON S SIDE OF FLEETWOOD DR AND ±30' W. OF CL OF SHEPARD DR.

- (N 1950169.350, E 989026.104, ELEV. 823.170)
- ① 34.70' TO THE NW CORNER OF 1 STORY BRICK BLD
 - ② 29.40' TO THE PK NAIL SET IN NORTH FACE OF 18" TREE
 - ③ 30.00' TO THE CENTER OF GUTTER INLET



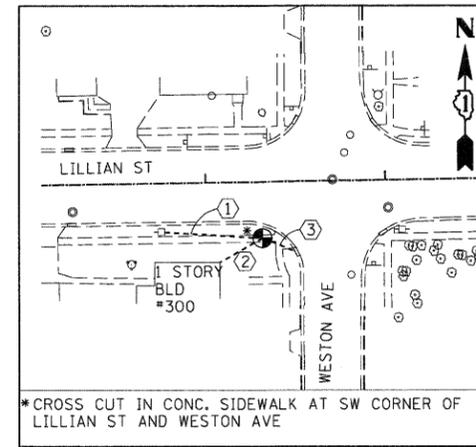
*PK NAIL IN ASPHALT DRIVEWAY AT THE NW CORNER OF MCLEAN BLVD & LILLIAN ST

- (N 1952013.765, E 990009.054, ELEV. 829.860)
- ① 18.60' TO THE CENTER OF GUTTER INLET
 - ② 13.55' TO THE CENTER OF TRAFFIC SIGNAL POLE
 - ③ 29.25' TO THE SW CORNER OF ELGIN MALL'S SIGN



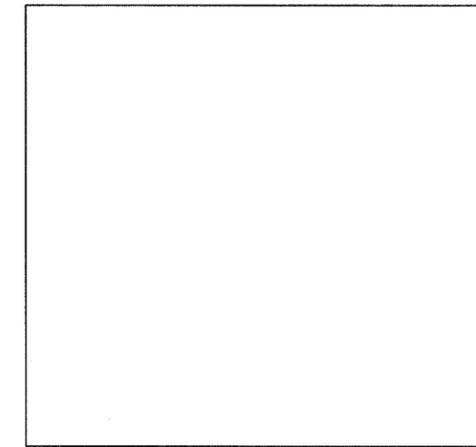
*CROSS CUT IN CONC. SIDEWALK ON E SIDE OF MCLEAN BLVD AND ±310' S OF CL OF LILLIAN ST.

- (N 1951665.021, E 990089.560, ELEV. 831.840)
- ① 28.30' TO THE SW CORNER OF 1 STORY FRAME BLD #321
 - ② 10.10' TO THE SE CORNER OF CONC. DRIVEWAY
 - ③ 21.00' TO THE CENTER OF SANITARY MANHOLE



*CROSS CUT IN CONC. SIDEWALK AT SW CORNER OF LILLIAN ST AND WESTON AVE

- (N 1951948.255, E 990397.884, ELEV. 831.060)
- ① 56.05' TO THE PK NAIL SET IN EAST FACE OF POWER POLE
 - ② 27.25' TO THE NE CORNER OF 1 STORY FRAME BLD #300
 - ③ 20.40' TO THE CENTER OF GUTTER INLET



*CROSS CUT IN CONC. SIDEWALK AT SW CORNER OF LILLIAN ST AND WESTON AVE

- (N 1951948.255, E 990397.884, ELEV. 831.060)
- ① 56.05' TO THE PK NAIL SET IN EAST FACE OF POWER POLE
 - ② 27.25' TO THE NE CORNER OF 1 STORY FRAME BLD #300
 - ③ 20.40' TO THE CENTER OF GUTTER INLET

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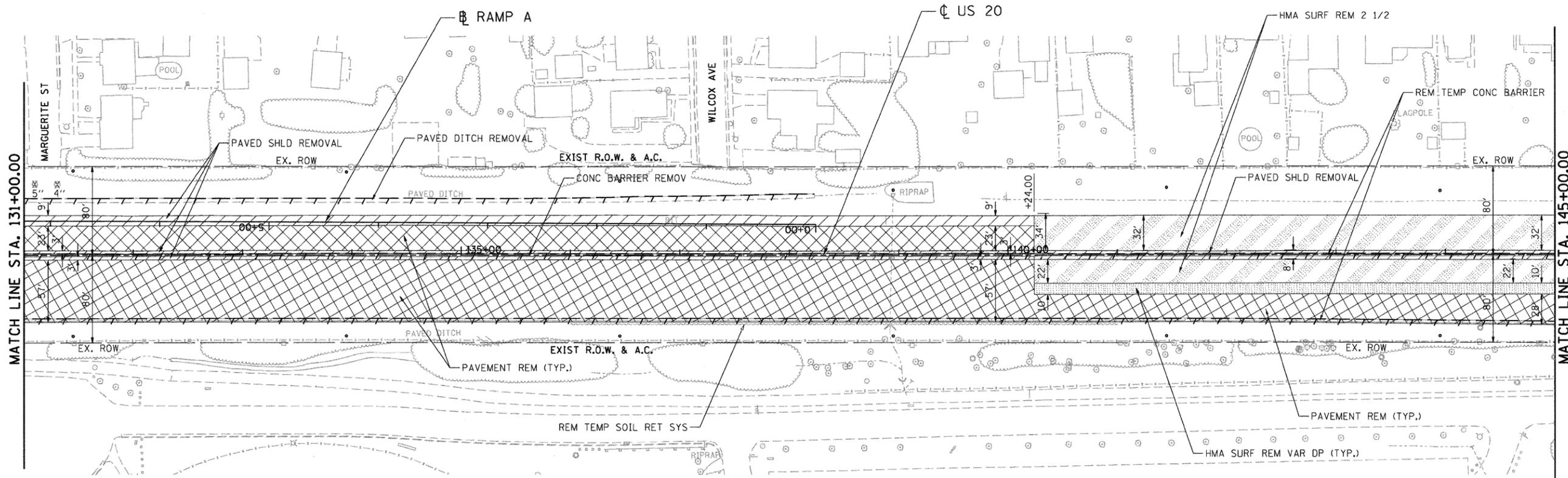
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 FAP ROUTE 345 / US ROUTE 20

ALIGNMENT, TIES AND BENCHMARKS
III

SCALE: 1"=200' SHEET NO. OF STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	8R-R	KANE	794	58
CONTRACT NO. 60H45				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				

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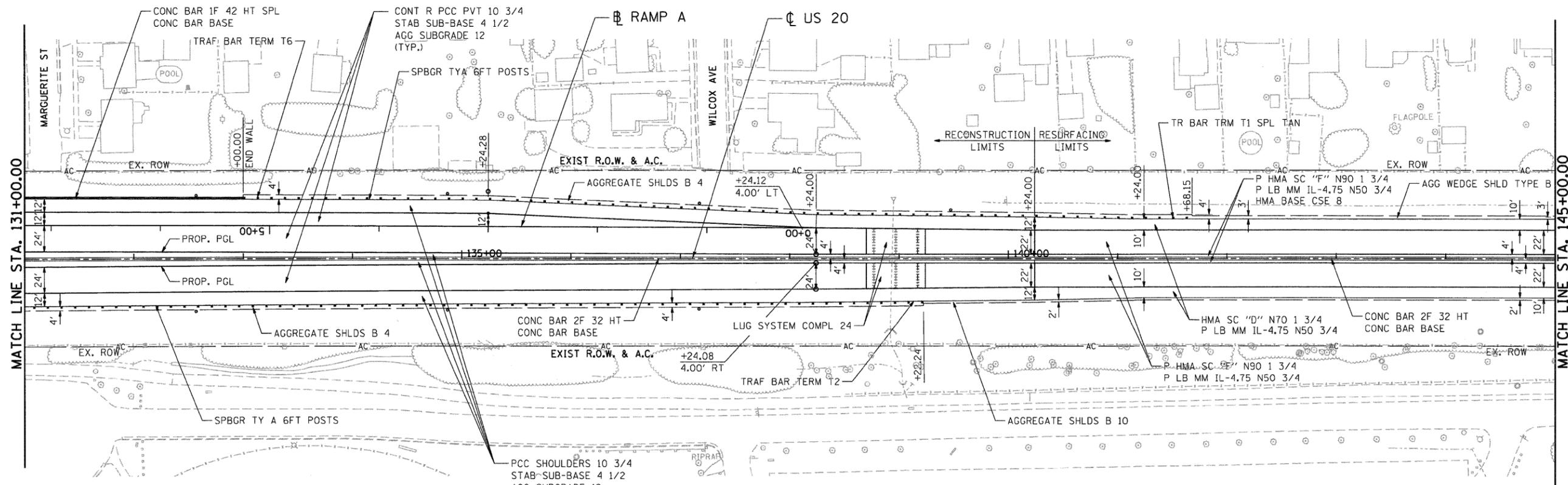


LEGEND

- TEMPORARY LIGHT POLE (SEE LIGHTING PLANS)

EXISTING PLAN

NOTE: SEE DRAINAGE AND UTILITY PLANS FOR REMOVAL OF DRAINAGE RELATED ITEMS.



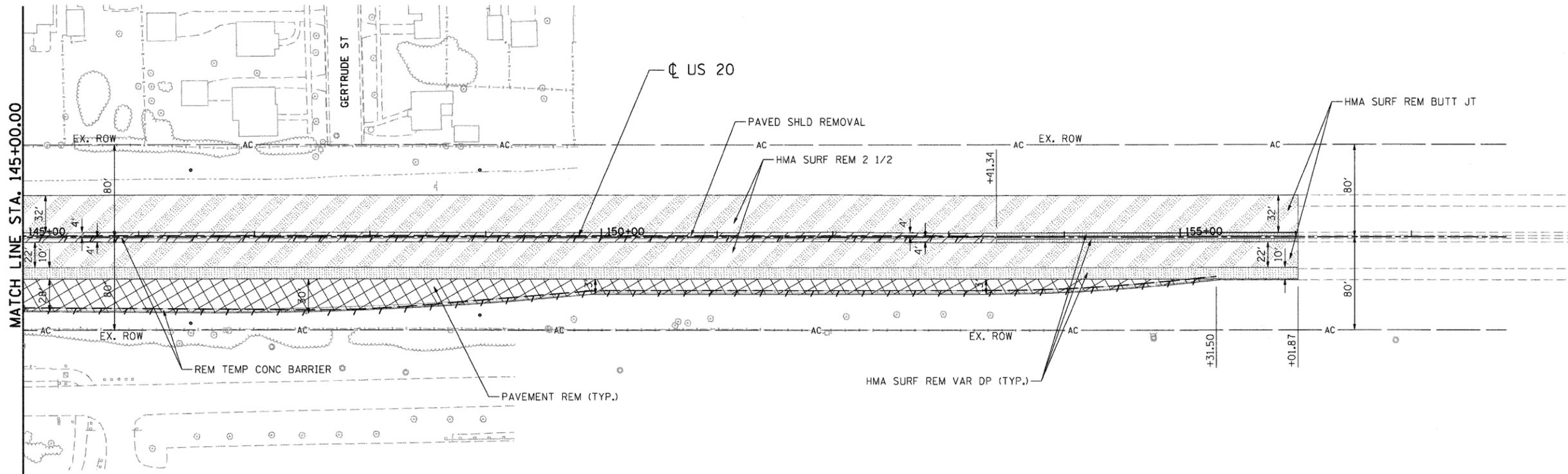
LEGEND

- PROPOSED LIGHT POLE (SEE LIGHTING PLANS)
- PROPOSED TRAFFIC SIGNAL POLE (SEE TRAFFIC SIGNAL PLANS)

PROPOSED PLAN

FILE NAME = #FILE#	USER NAME = #USER#	DESIGNED - MRK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION FAP ROUTE 345 / US ROUTE 20	ROADWAY PLAN U.S. ROUTE 20		F.A.P. RTE. 345	SECTION 8R-R	COUNTY KANE	TOTAL SHEETS 794	SHEET NO. 66	
	PLOT SCALE = #SCALE#	DRAWN - MRK	REVISED -		SCALE: 1"=50'	SHEET NO. OF SHEETS	STA. 131+00.00 TO STA. 145+00.00	CONTRACT NO. 60H45				
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		DATE - 12/16/11	REVISED -									



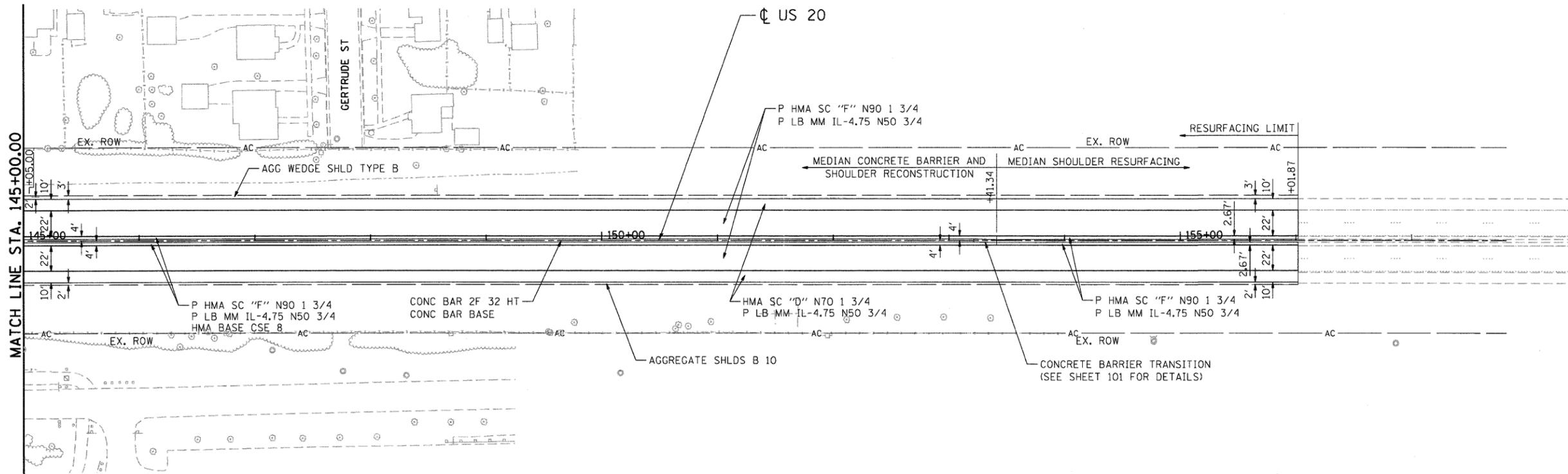


LEGEND

- TEMPORARY LIGHT POLE (SEE LIGHTING PLANS)

EXISTING PLAN

NOTE: SEE DRAINAGE AND UTILITY PLANS FOR REMOVAL OF DRAINAGE RELATED ITEMS.



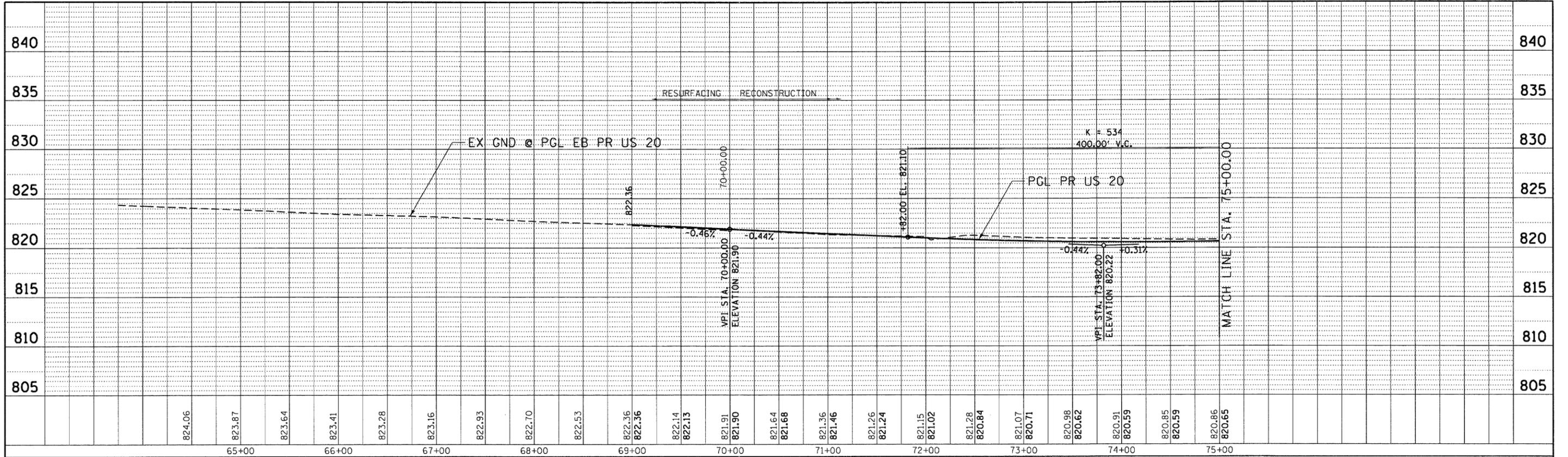
LEGEND

- PROPOSED LIGHT POLE (SEE LIGHTING PLANS)
- PROPOSED TRAFFIC SIGNAL POLE (SEE TRAFFIC SIGNAL PLANS)

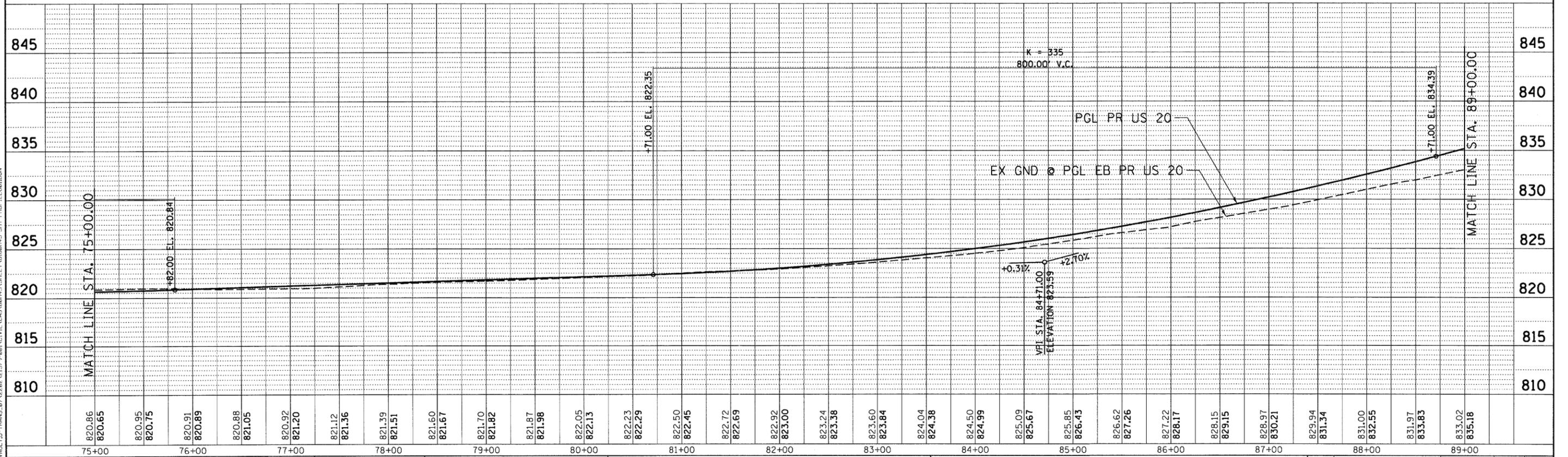
PROPOSED PLAN

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TENG TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT DATE = #DATE#	DATE - 12/16/11	REVISED -							

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	NOTE BOOK		
	NO.		
	NO. OF WAY CHECKED		
	PAID FILE NAME		



PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	NOTE BOOK		
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	NO. OF WAY CHECKED		
	STRUCTURE NOTATION		
	PROFILE		



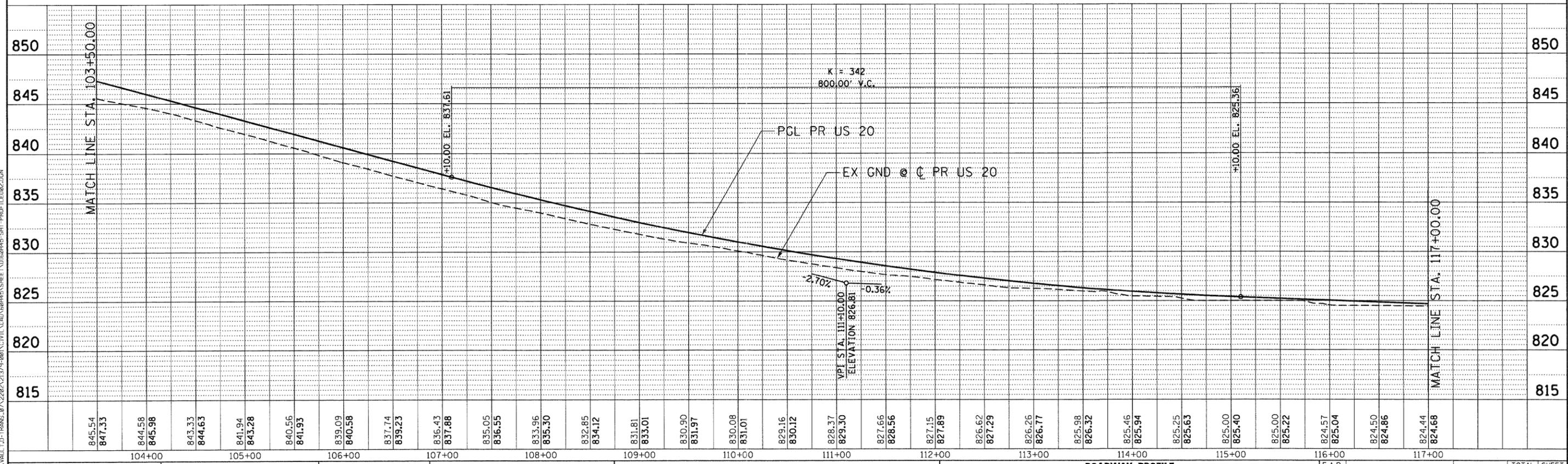
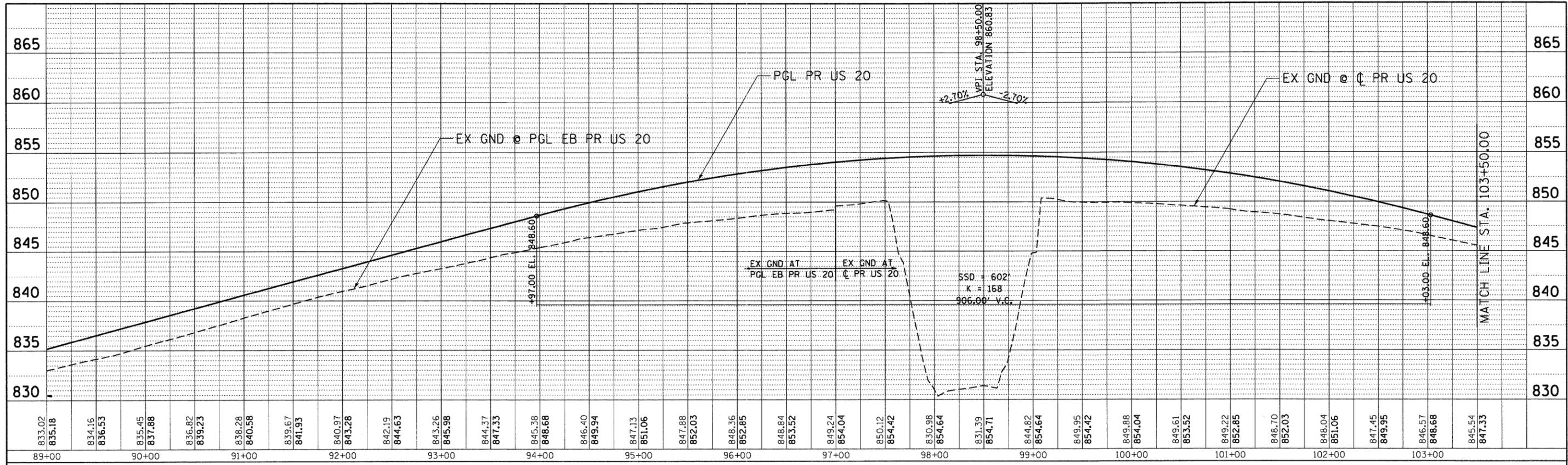
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	PLOTTED		
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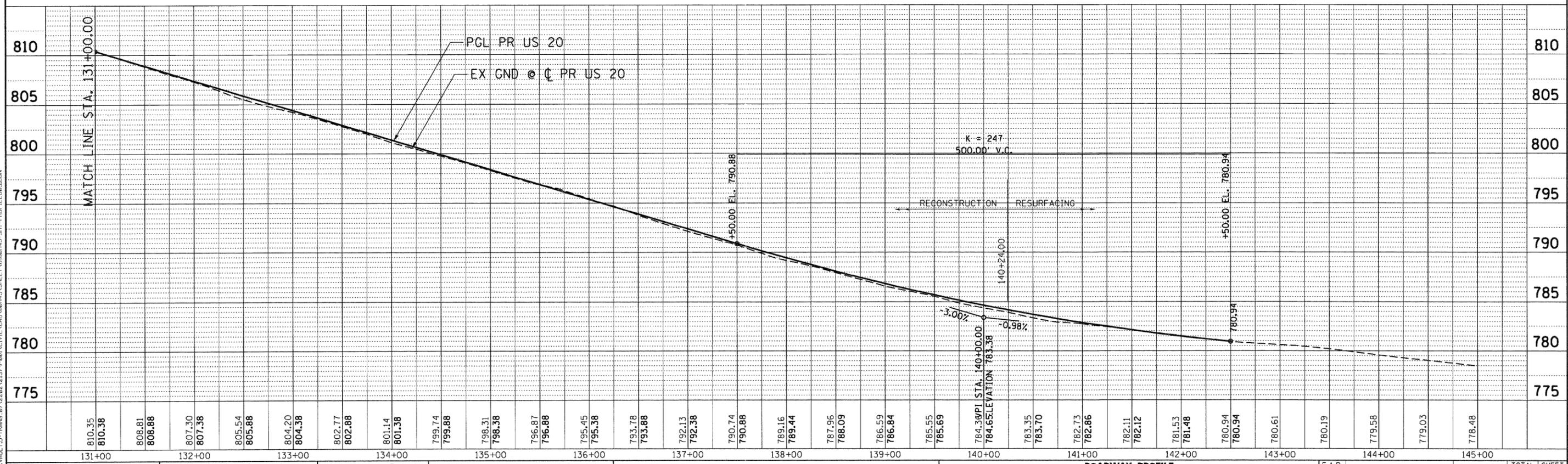
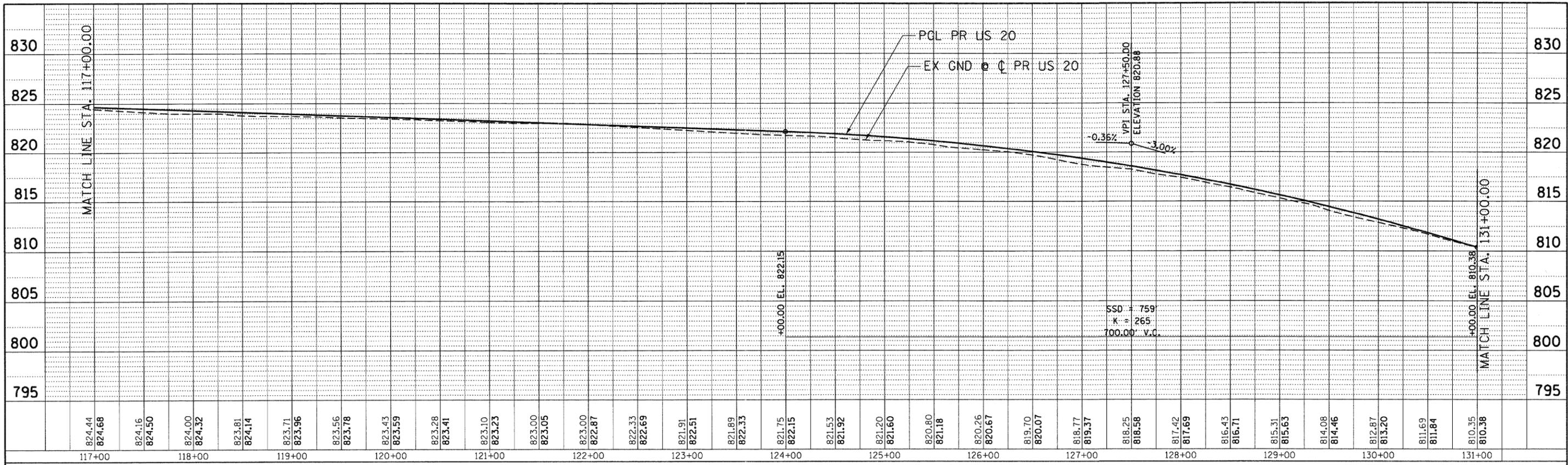


TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 CHICAGO, ILLINOIS

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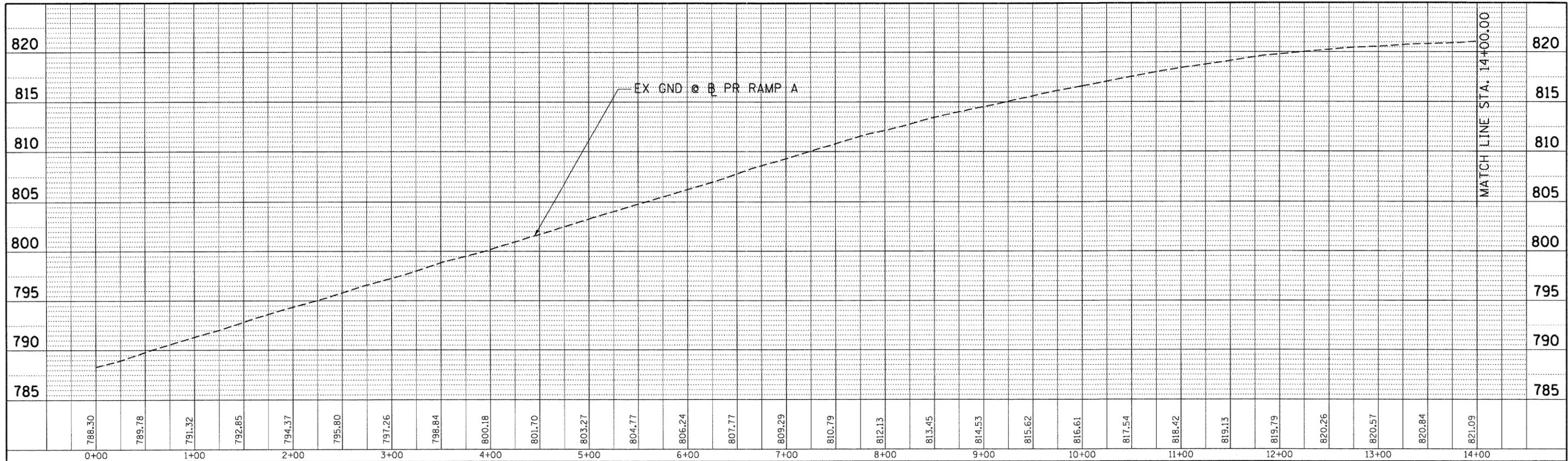
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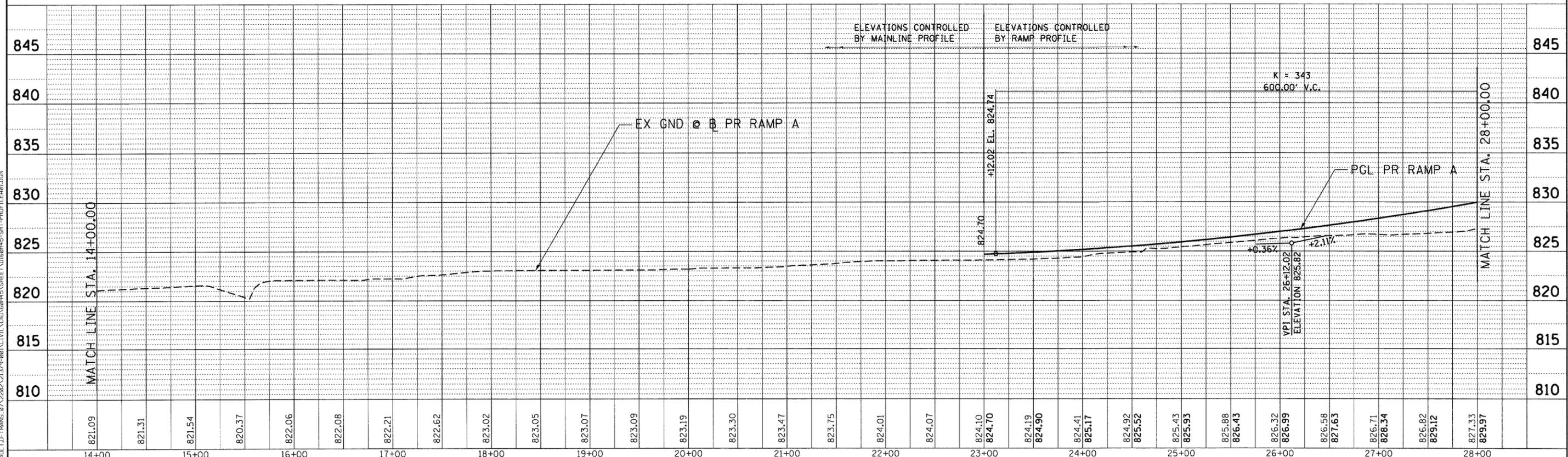


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TENG	TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	CHECKED - DDH	REVISED -					FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				
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	CAD FILE NAME		



PROFILE	SURVEYED	BY	DATE
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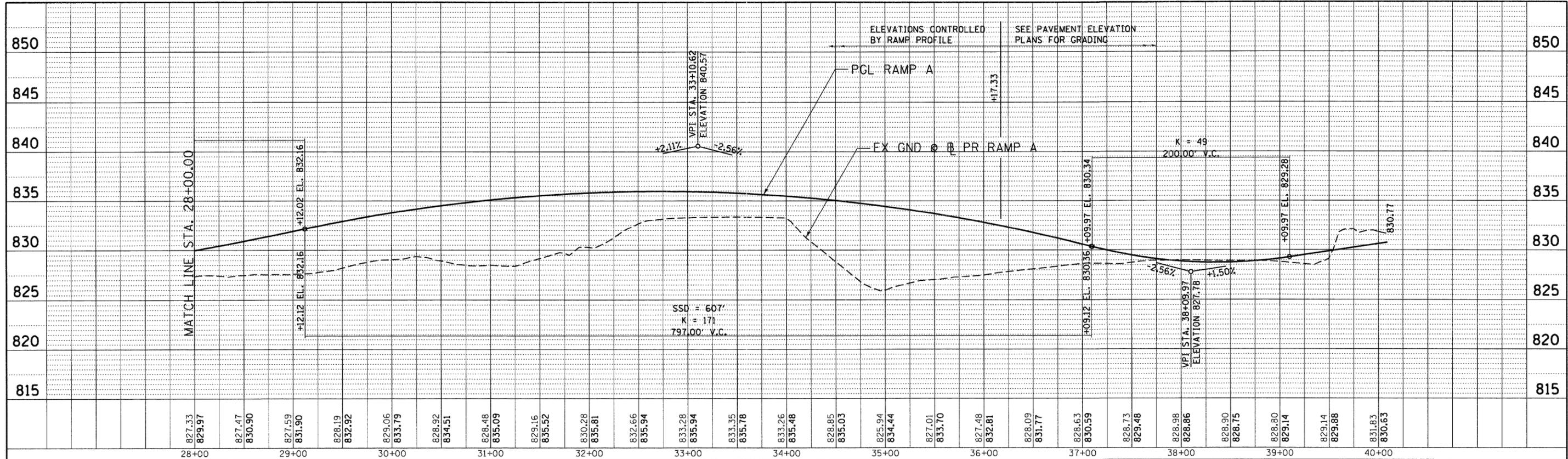
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PLAN SURVEYED BY DATE
 PLOTTED BY DATE
 NOTE BOOK NO. CHECKED BY DATE
 NO. OF WAYS CHECKED
 CAD FILE NAME

PROFILE SURVEYED BY DATE
 PLOTTED BY DATE
 NOTE BOOK NO. CHECKED BY DATE
 NO. OF WAYS CHECKED
 STRUCTURE NOTATIONS OK'D

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DESIGNED - MRK
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 DATE - 12/16/11

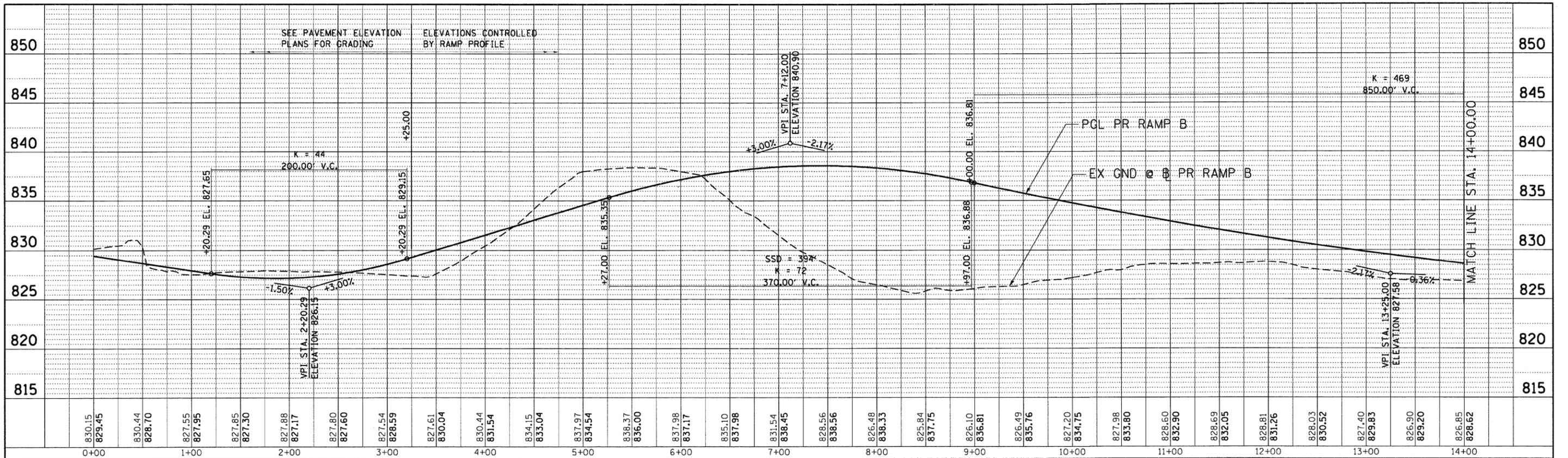
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 FAP ROUTE 345 / US ROUTE 20

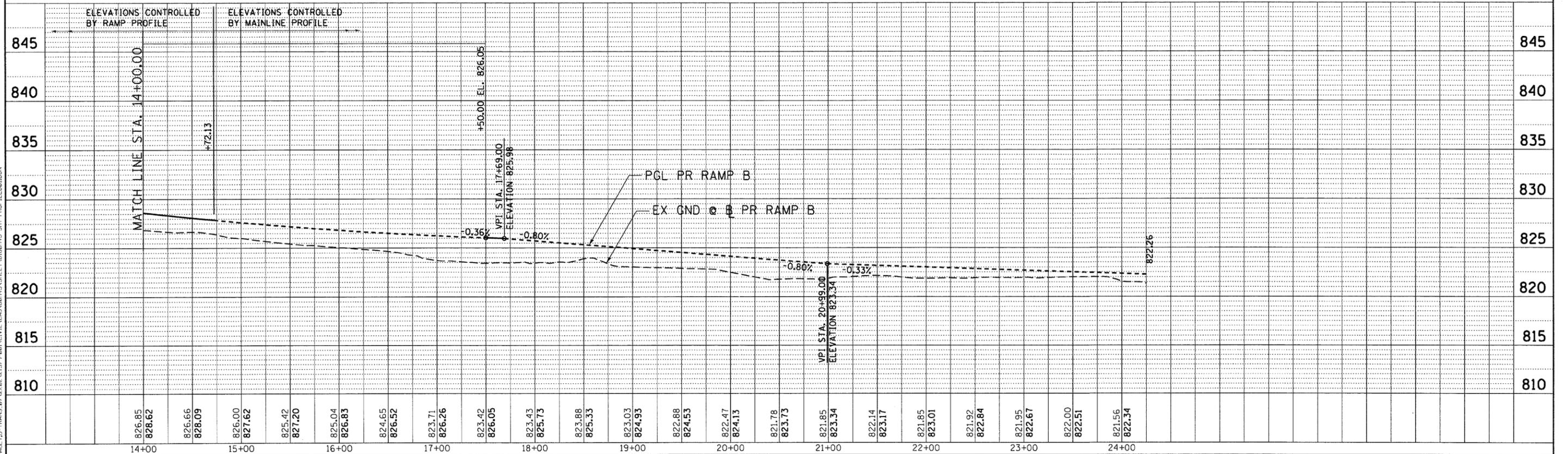
ROADWAY PROFILE
 U.S. ROUTE 20
 WB EXIT RAMP A
 SCALE: HORIZ. 1"=50' VERT. 1"=50'
 STA. 28+00.00 TO STA. 40+09.92

F.A.P. RTE. 345	SECTION 8R-R	COUNTY KANE	TOTAL SHEETS 794	SHEET NO. 72
CONTRACT NO. 60H45				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	NOTE BOOK		
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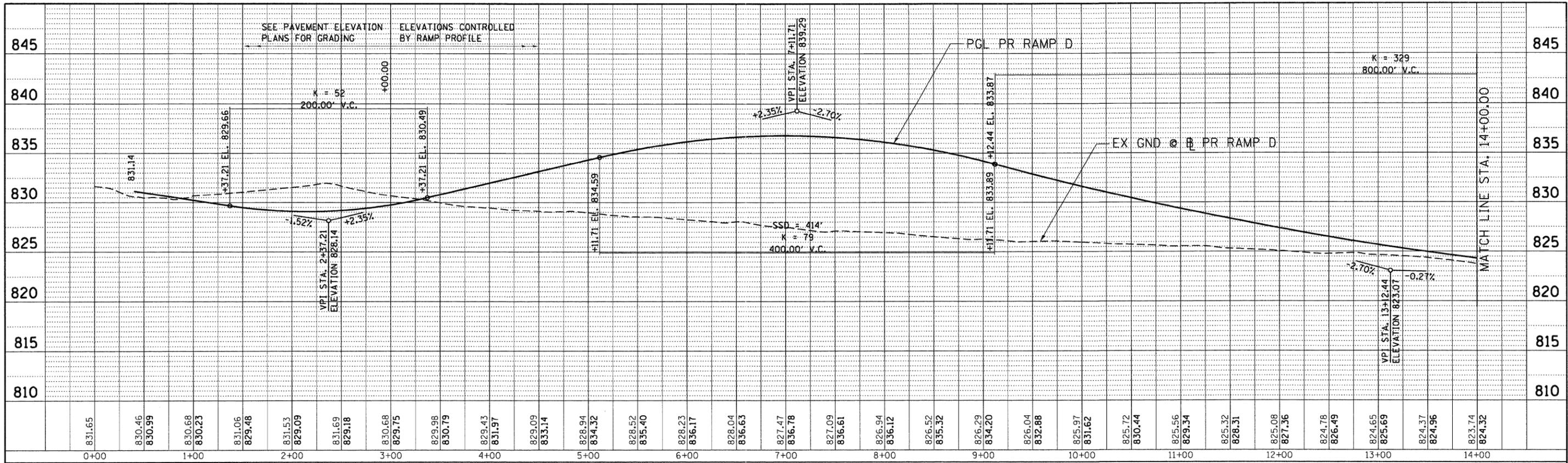
PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	NOTE BOOK		
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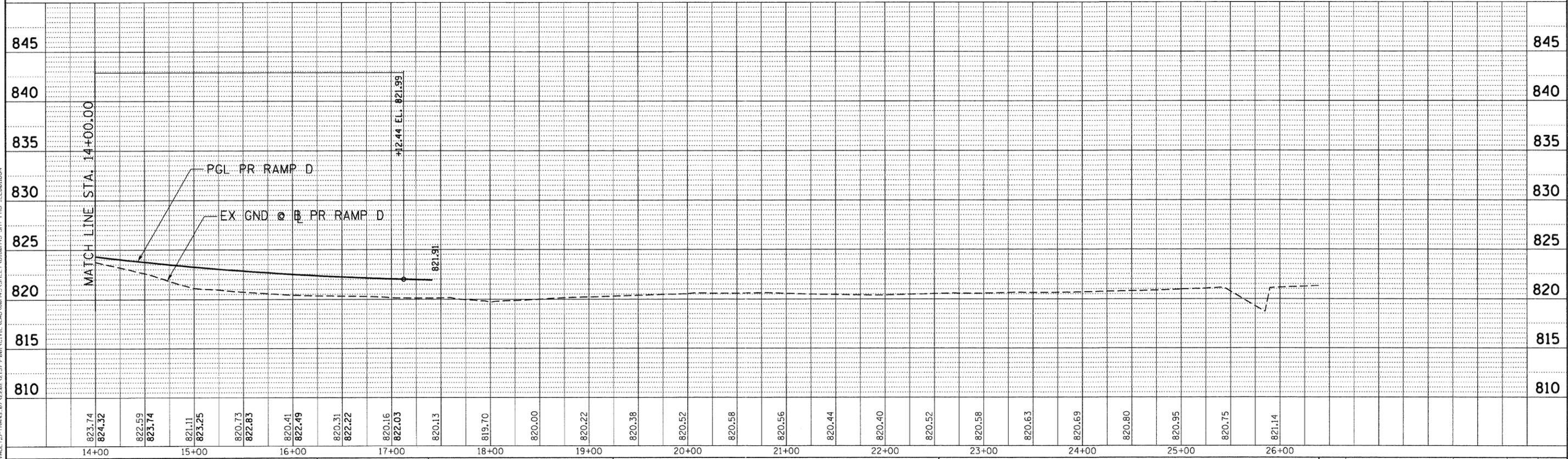
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	PLLOT DATE = #DATE#	CHECKED - DDH	REVISED -		FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT							
		DATE - 12/16/11	REVISED -									

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	PLOTTED		
	NOTE BOOK		
	NO.		
	CADD FILE NAME		



PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	NOTE BOOK		
	NO.		
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	DRAWN - MRK
	CHECKED - DDH
	DATE - 12/16/11

PLOT SCALE = *SCALE#	REVISED -
PLOT DATE = *DATE#	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
FAP ROUTE 345 / US ROUTE 20

ROADWAY PROFILE
U.S. ROUTE 20
WB ENTRANCE RAMP D

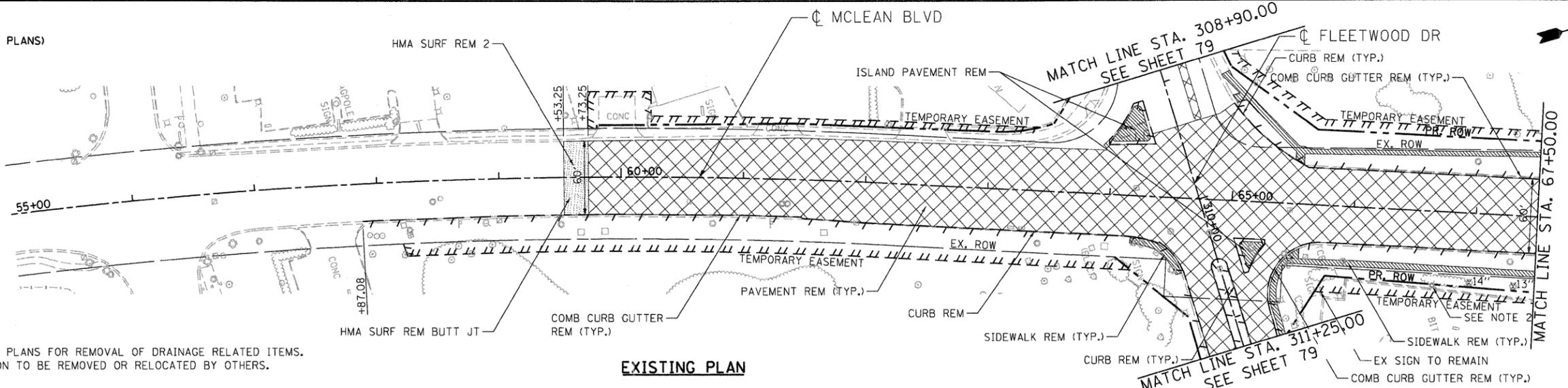
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	8R-R	KANE	794	75

SCALE: HORIZ. 1"=50'	VERT. 1"=5'	STA. 0+00.00 TO STA. 26+41.18
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CONTRACT NO. 60H45	
ILLINOIS FED. AID PROJECT	

LEGEND

- TEMPORARY LIGHT POLE (SEE LIGHTING PLANS)



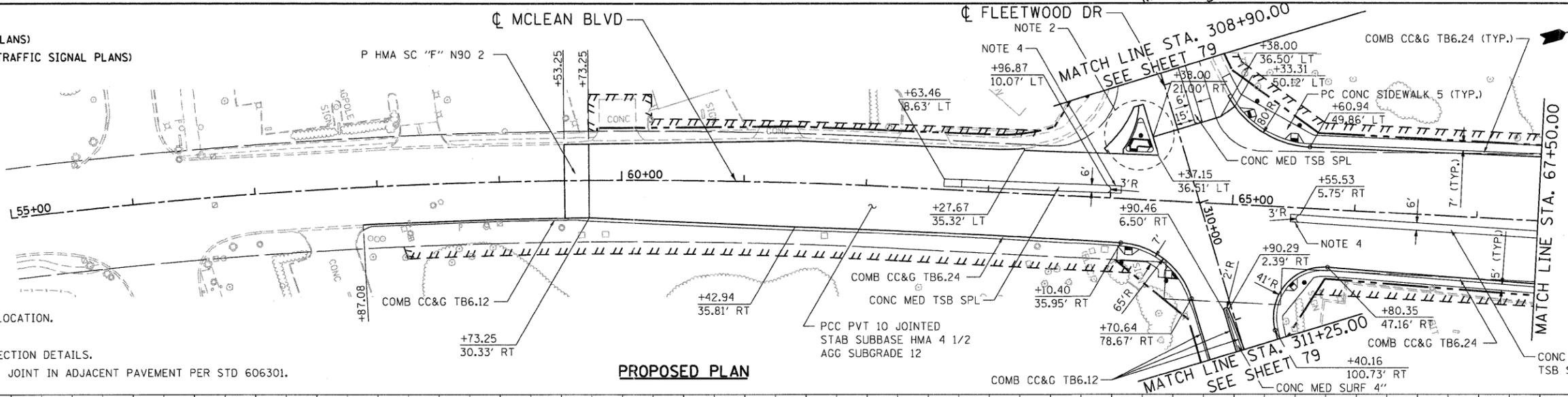
NOTES:

1. SEE DRAINAGE AND UTILITY PLANS FOR REMOVAL OF DRAINAGE RELATED ITEMS.
2. LIGHT POLE AND FOUNDATION TO BE REMOVED OR RELOCATED BY OTHERS.

EXISTING PLAN

LEGEND

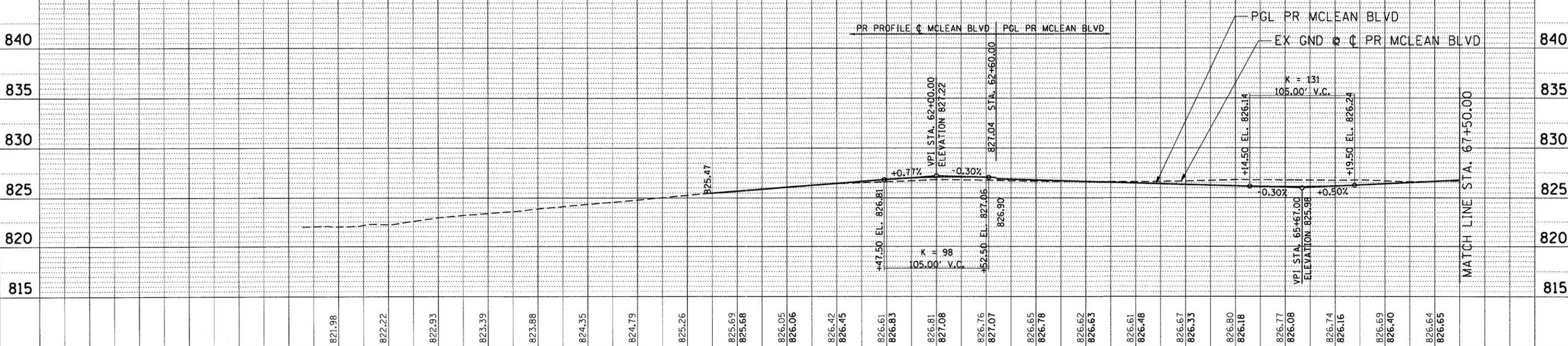
- PROPOSED LIGHT POLE (SEE LIGHTING PLANS)
- PROPOSED TRAFFIC SIGNAL POLE (SEE TRAFFIC SIGNAL PLANS)



NOTES:

1. SEE SHEET 103 FOR MCLEAN BLVD PGL LOCATION.
2. SEE SHEET 108 FOR ISLAND DETAILS.
3. SEE SHEET 105 FOR ADDITIONAL INTERSECTION DETAILS.
4. EXTEND CONC MED TSB SPL TO NEAREST JOINT IN ADJACENT PAVEMENT PER STD 606301.

PROPOSED PLAN



FILE NAME =	USER NAME = #USER#	DESIGNED - MRK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION FAP ROUTE 345 / US ROUTE 20		ROADWAY PLAN AND PROFILE MCLEAN BOULEVARD		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLLOT DATE = #DATE#		DATE - 12/16/11	REVISED -					FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

PLAN	SURVEYED	BY	DATE
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	REVISIONS		
	CADD FILE NAME		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	NOTED		
	REVISIONS		
	CADD FILE NAME		

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\\FC-004\A\WVA\J.D.-FRANKS.07\2202\21379-00\ACIVIL\ROAD\80H45\SHEET\06\80H45-SHT-PL-PROF.MXD

LEGEND

- TEMPORARY LIGHT POLE (SEE LIGHTING PLANS)

NOTES:

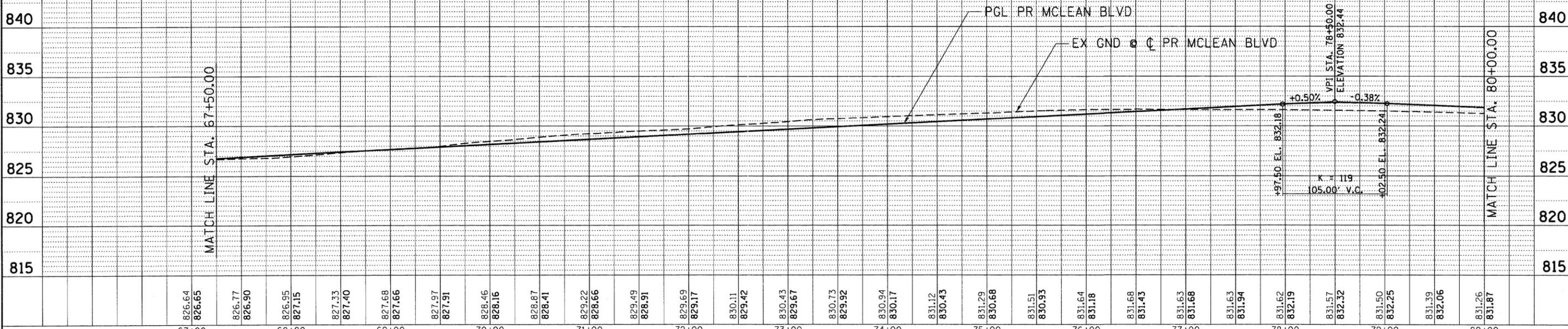
1. SEE DRAINAGE AND UTILITY PLANS FOR REMOVAL OF DRAINAGE RELATED ITEMS.
2. ITEM TO BE REMOVED BY OTHERS.
3. THE COST TO REMOVE THE CONCRETE IMMEDIATELY ADJACENT TO THE FOUNDATION ONE FOOT BELOW THE PROPOSED SUBGRADE SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR FOUNDATION REMOVAL.

LEGEND

- PROPOSED LIGHT POLE (SEE LIGHTING PLANS)
- PROPOSED TRAFFIC SIGNAL POLE (SEE TRAFFIC SIGNAL PLANS)

NOTES:

1. SEE SHEET 106 FOR DRIVEWAY SCHEDULES AND DETAILS.
2. SEE SHEET 104 FOR ADDITIONAL INTERSECTION DETAILS.
3. CONC CURB TB SPL NOT CONTINUED THROUGH THE DRIVEWAYS.
4. SEE SHEET 76 FOR NOTE 4.
5. SEE SHEET 108 FOR CONC CURB TB SPL DETAIL.



FILE NAME =	USER NAME = #USER#	DESIGNED - MRK	REVISED -	STATE OF ILLINOIS		ROADWAY PLAN AND PROFILE		F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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		CHECKED - DDH	REVISED -	FAP ROUTE 345 / US ROUTE 20		SCALE: HORIZ. 1"=50' VERT. 1"=5'		STA. 67+50.00 TO STA. 80+00.00		CONTRACT NO. 60H45		
		DATE - 12/16/11	REVISED -					FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				

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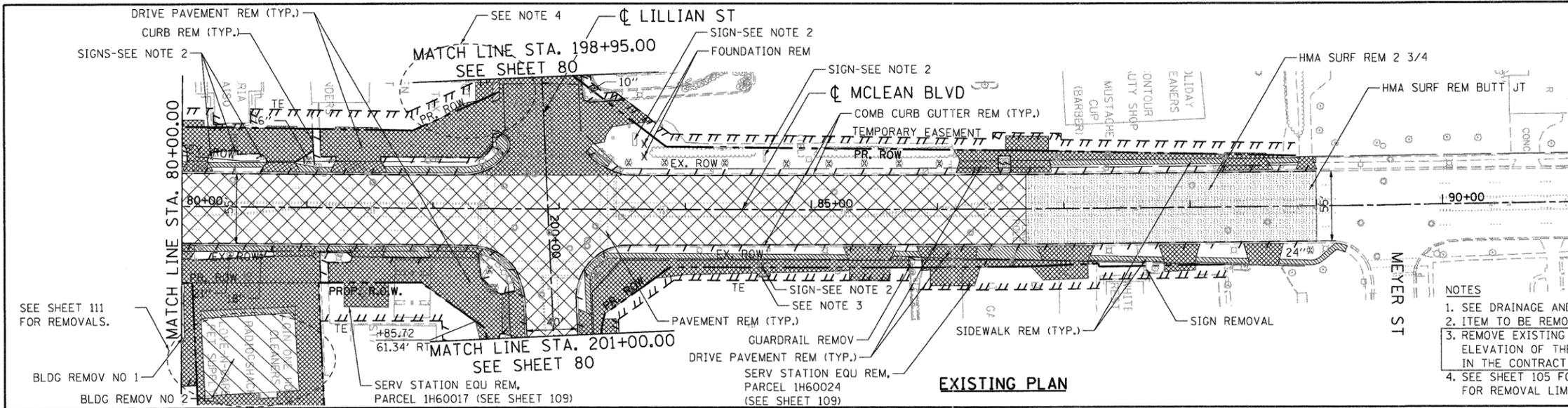
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PLAN	BY	DATE
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NO. OF WAY CHECKED		
STRUCTURE NOTATIONS CHECKED		
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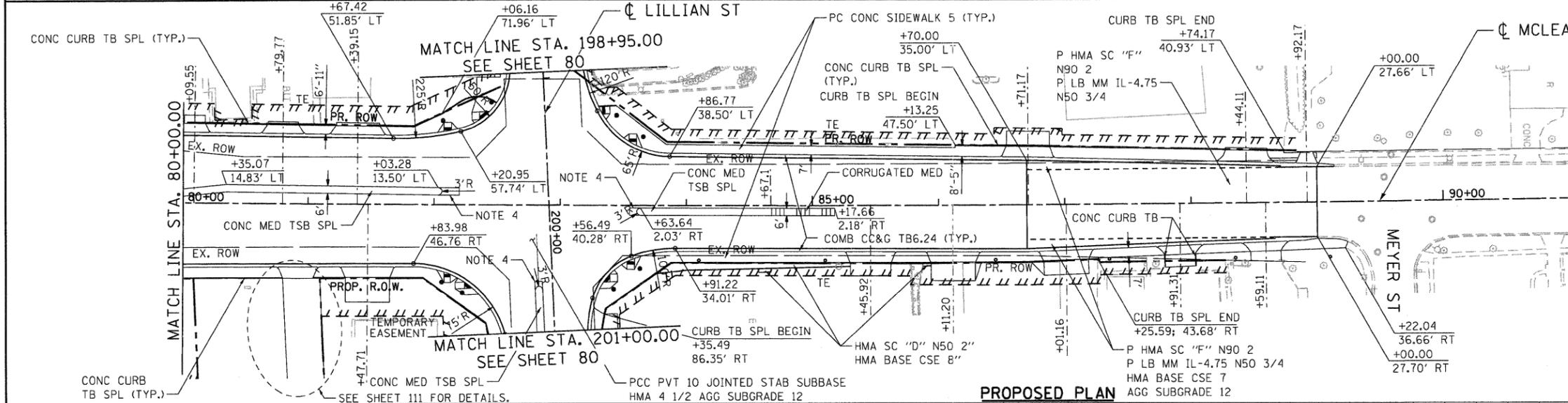


LEGEND

- TEMPORARY LIGHT POLE (SEE LIGHTING PLANS)

NOTES

1. SEE DRAINAGE AND UTILITY PLANS FOR REMOVAL OF DRAINAGE RELATED ITEMS.
2. ITEM TO BE REMOVED BY OTHERS.
3. REMOVE EXISTING BOLLARDS AND FOUNDATIONS TO 12" BELOW THE PROPOSED ELEVATION OF THE SUBGRADE OR EXISTING GROUND SURFACE. COST TO BE INCLUDED IN THE CONTRACT UNIT PRICE FOR DRIVEWAY PAVEMENT REMOVAL.
4. SEE SHEET 105 FOR PROPOSED GEOMETRY AND USE THE PROPOSED STATION/OFFSETS FOR REMOVAL LIMITS.

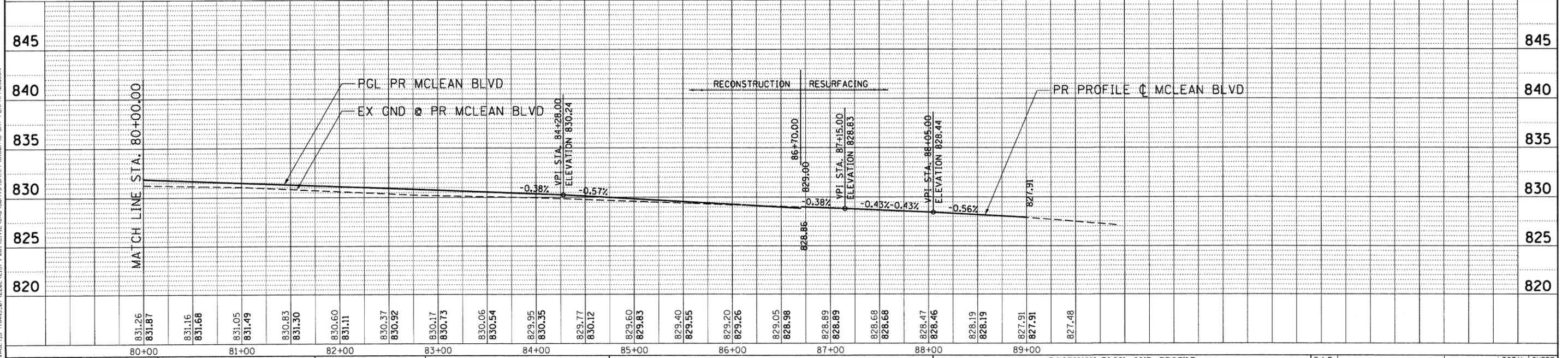


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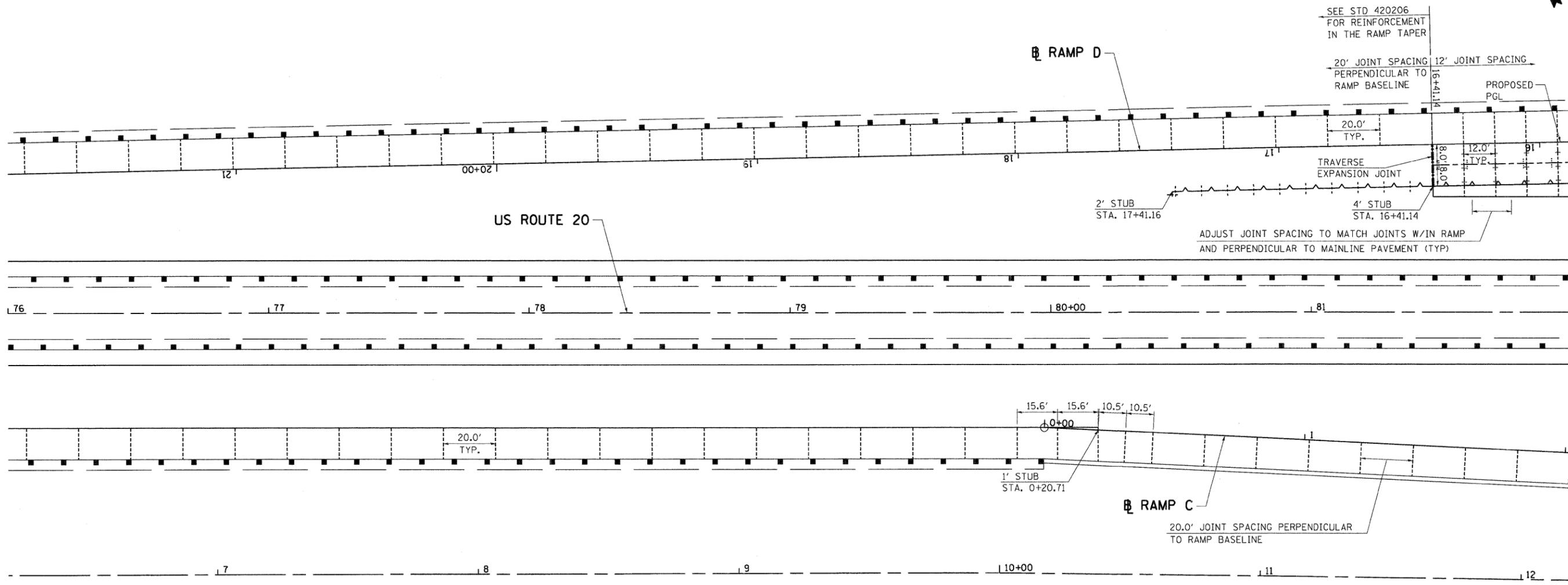
- PROPOSED LIGHT POLE (SEE LIGHTING PLANS)
- PROPOSED TRAFFIC SIGNAL POLE (SEE TRAFFIC SIGNAL PLANS)

NOTES

1. SEE SHEET 106 FOR DRIVEWAY SCHEDULES AND DETAILS.
2. CONC CURB TB SPL NOT CONTINUED THROUGH THE DRIVEWAYS.
3. SEE SHEET 105 FOR ADDITIONAL INTERSECTION DETAILS.
4. EXTEND CONC MED TSB SPL TO NEAREST JOINT IN ADJACENT PAVEMENT PER STD 606301.
5. SEE SHEET 108 FOR CONC CURB TB SPL DETAIL.



FILE NAME =	USER NAME = #USER#	DESIGNED - MRK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION FAP ROUTE 345 / US ROUTE 20	ROADWAY PLAN AND PROFILE MCLEAN BOULEVARD	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
#FILE#	PLOT SCALE = #SCALE#	DRAWN - MRK	REVISED -			345	8R-R	KANE	794	78	
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		DATE - 12/16/11	REVISED -			FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT					



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PAVEMENT JOINT LEGEND

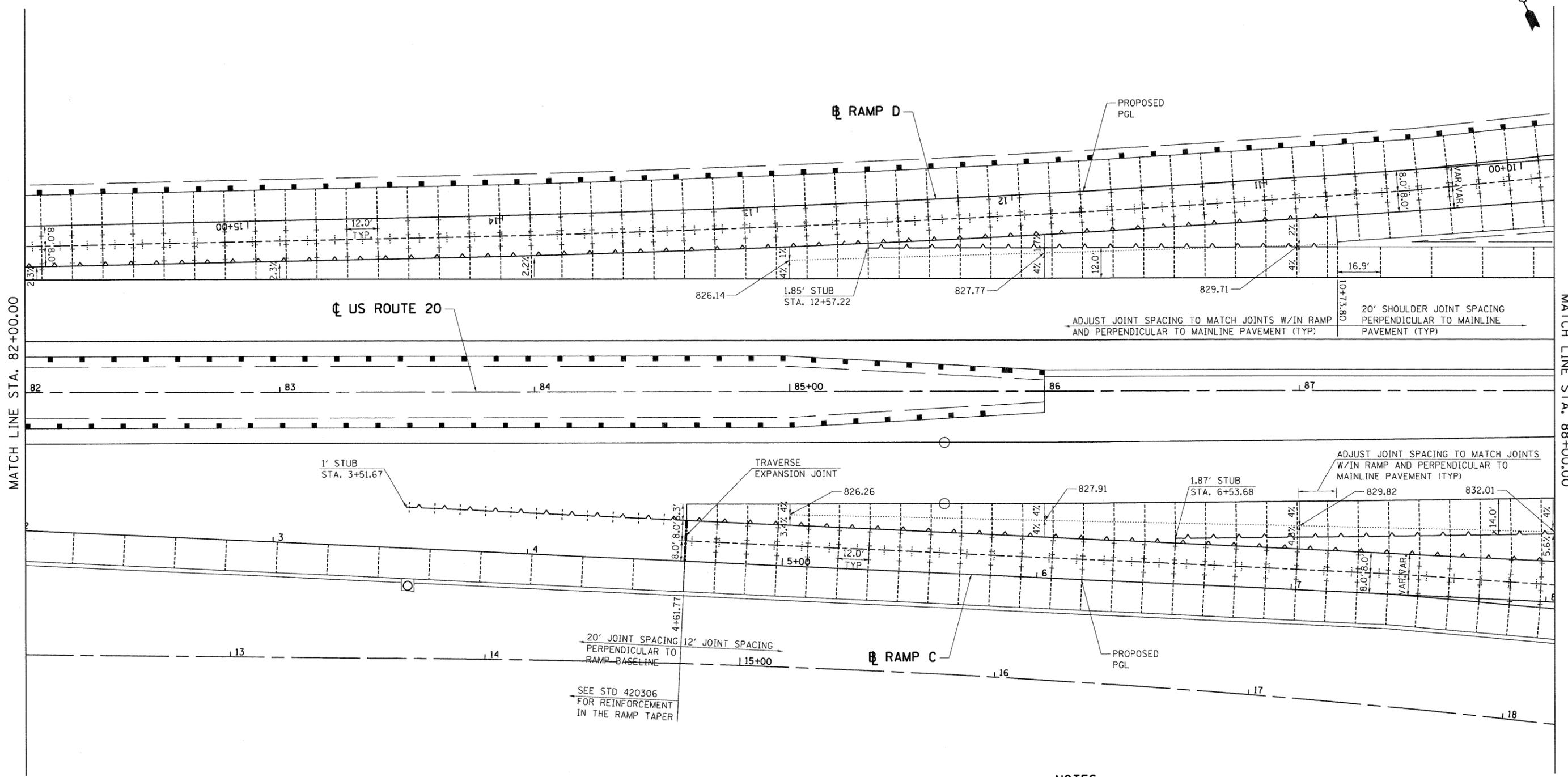
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- SAWED TRANSVERSE CONTRACTION JOINT W/IN PCC SHLD
- LONGITUDINAL CONSTRUCTION JOINT WITH TIE BARS
- TRANSVERSE EXPANSION JOINT
- SAWED TRANSVERSE CONTRACTION JOINT
- PAVEMENT SEPARATION JOINT
- SAWED LONGITUDINAL JOINT

NOTES

1. SEE HIGHWAY STD 420001, 420306 & 420206 AND DISTRICT 1 DETAILS BD49 & BD52 FOR PAVEMENT JOINT DETAILS.
2. LONGITUDINAL CONSTRUCTION JOINTS W/ TIE BARS SHALL BE PLACED BETWEEN PCC PAVEMENT AND PCC SHOULDER, NOT SHOWN FOR CLARITY.
3. MAINLINE AND RAMP CRCP PAVEMENT REINFORCEMENT & JOINTS ARE NOT SHOWN FOR CLARITY. SEE STANDARDS 420206, 420306, AND 421201.
4. GORE NOSE LOCATIONS SHALL BE ALIGNED WITH MAINLINE SHOULDER TRANSVERSE JOINT.
5. ADDITIONAL SAWED TRANSVERSE CONTRACTION JOINTS MAY BE REQUIRED IN THE FIELD. EXACT LOCATIONS TO BE VERIFIED IN THE FIELD AND APPROVED BY THE ENGINEER.
6. CONCRETE STUBS SHALL HAVE A 1' MINIMUM WIDTH.

FILE NAME =	USER NAME = #USER#	DESIGNED - JP	REVISED -	STATE OF ILLINOIS	PAVEMENT JOINTING PLAN	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE#		DRAWN - AG	REVISED -	DEPARTMENT OF TRANSPORTATION	U.S. ROUTE 20	345	8R-R	KANE	794	81
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	PLOT DATE = #DATE#	DATE - 12/16/11	REVISED -			FED. ROAD DIST. NO.		[ILLINOIS] FED. AID PROJECT		





PAVEMENT JOINT LEGEND

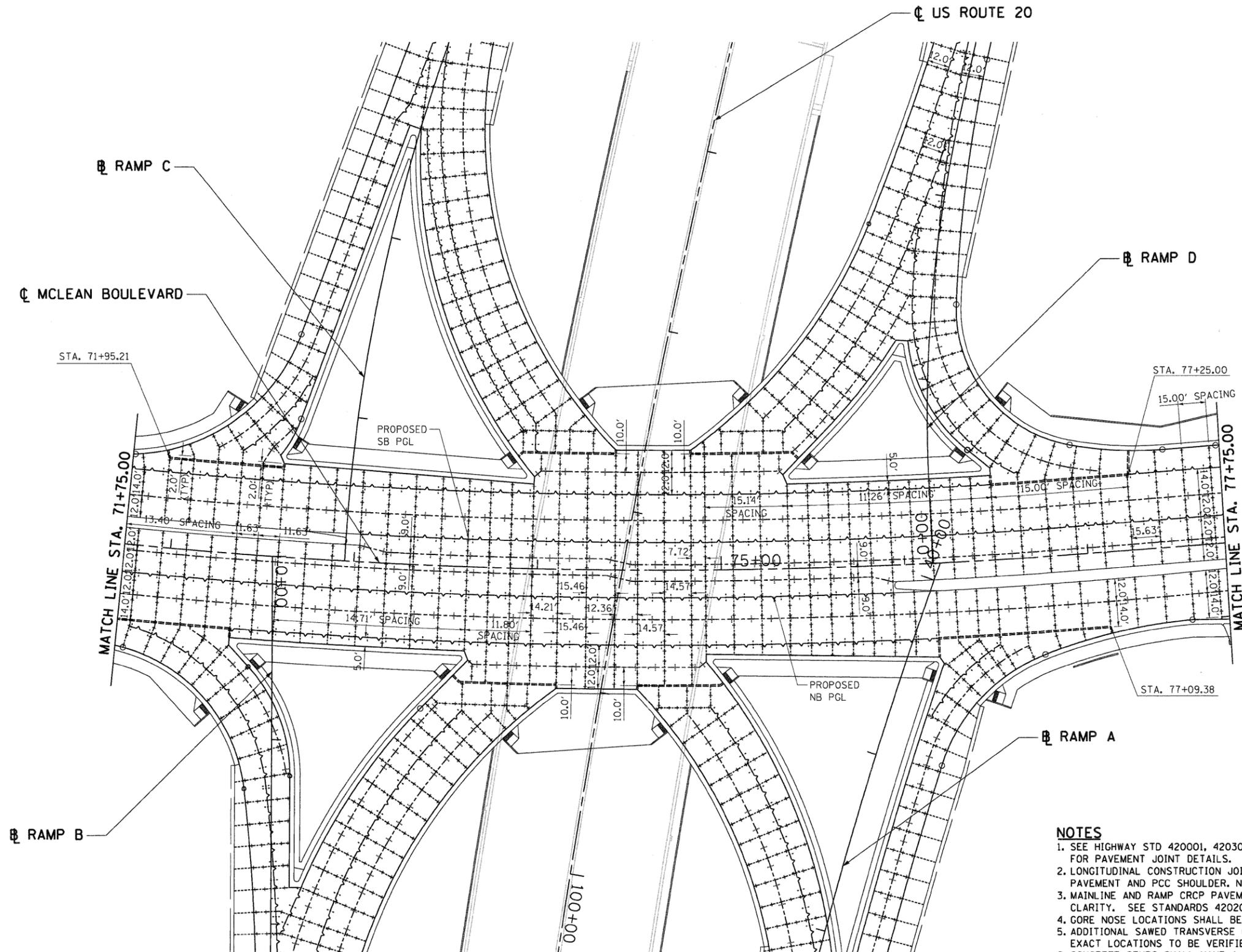
	LONGITUDINAL KEYED JOINT WITHOUT TIE BARS		SAWED TRANSVERSE CONTRACTION JOINT W/IN PCC SHLD
	LONGITUDINAL CONSTRUCTION JOINT WITH TIE BARS		TRANSVERSE EXPANSION JOINT
	SAWED TRANSVERSE CONTRACTION JOINT		PAVEMENT SEPARATION JOINT
	SAWED LONGITUDINAL JOINT		

NOTES

- SEE HIGHWAY STD 420001, 420306 & 420206 AND DISTRICT 1 DETAILS BD49 & B052 FOR PAVEMENT JOINT DETAILS.
- LONGITUDINAL CONSTRUCTION JOINTS W/ TIE BARS SHALL BE PLACED BETWEEN PCC PAVEMENT AND PCC SHOULDER. NOT SHOWN FOR CLARITY.
- MAINLINE AND RAMP CRCP PAVEMENT REINFORCEMENT & JOINTS ARE NOT SHOWN FOR CLARITY. SEE STANDARDS 420206, 420306, AND 421201.
- GORE NOSE LOCATIONS SHALL BE ALIGNED WITH MAINLINE SHOULDER TRANSVERSE JOINT.
- ADDITIONAL SAWED TRANSVERSE CONTRACTION JOINTS MAY BE REQUIRED IN THE FIELD. EXACT LOCATIONS TO BE VERIFIED IN THE FIELD AND APPROVED BY THE ENGINEER.
- CONCRETE STUBS SHALL HAVE A 1' MINIMUM WIDTH.

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		DATE - 12/16/11	REVISED -								



NOTES

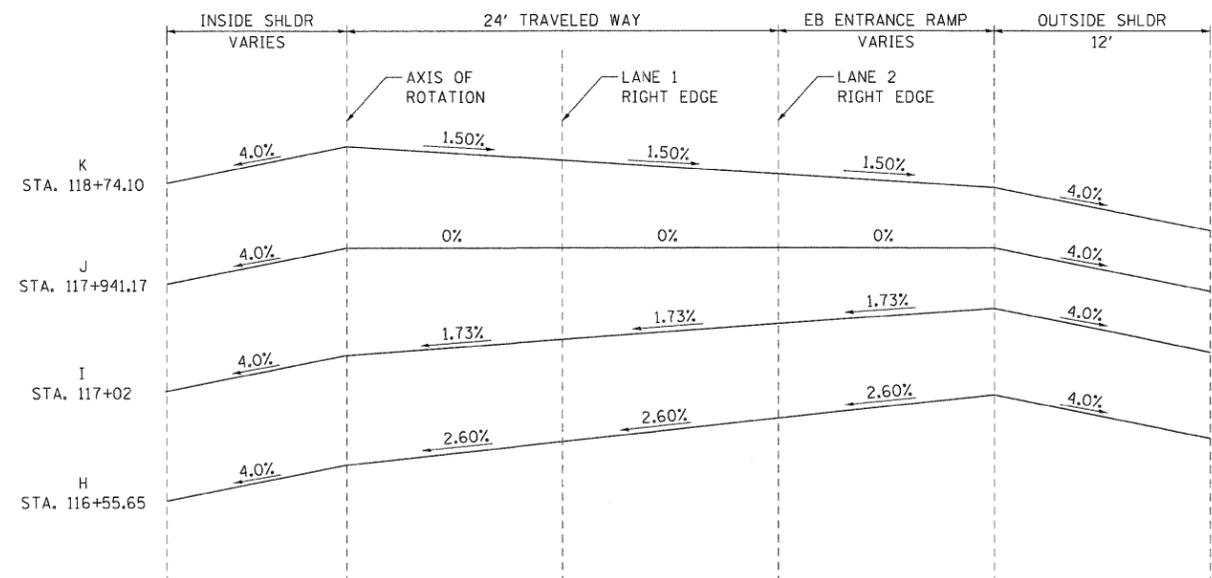
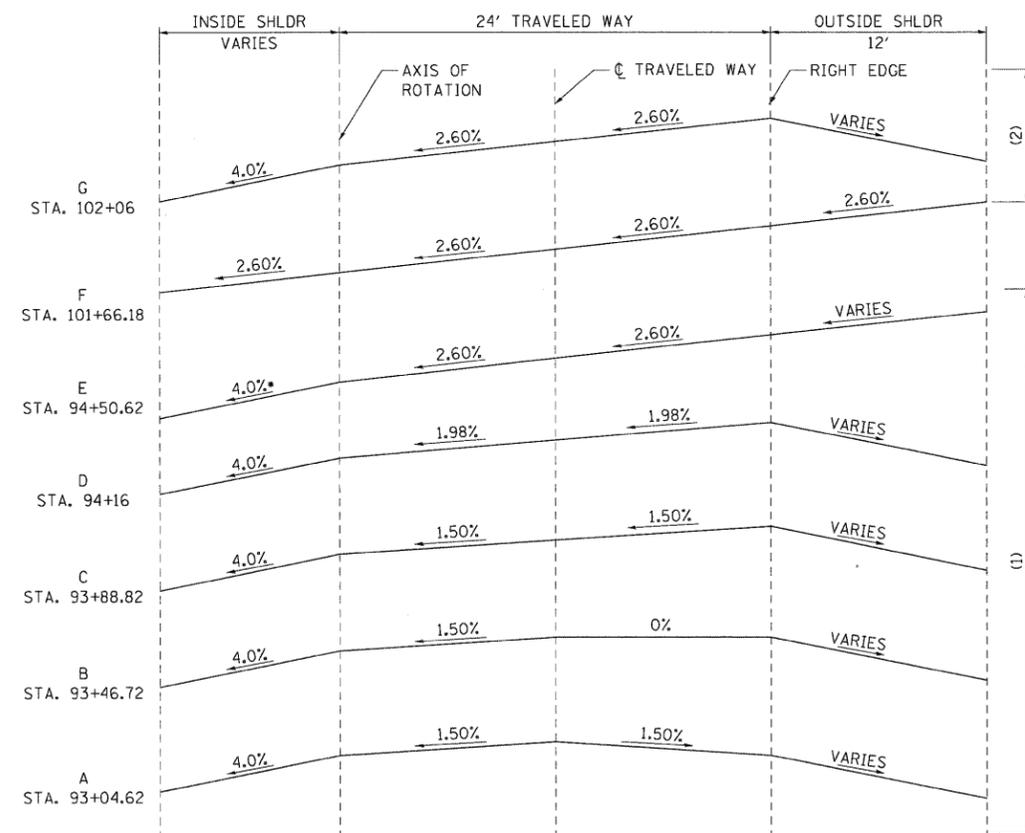
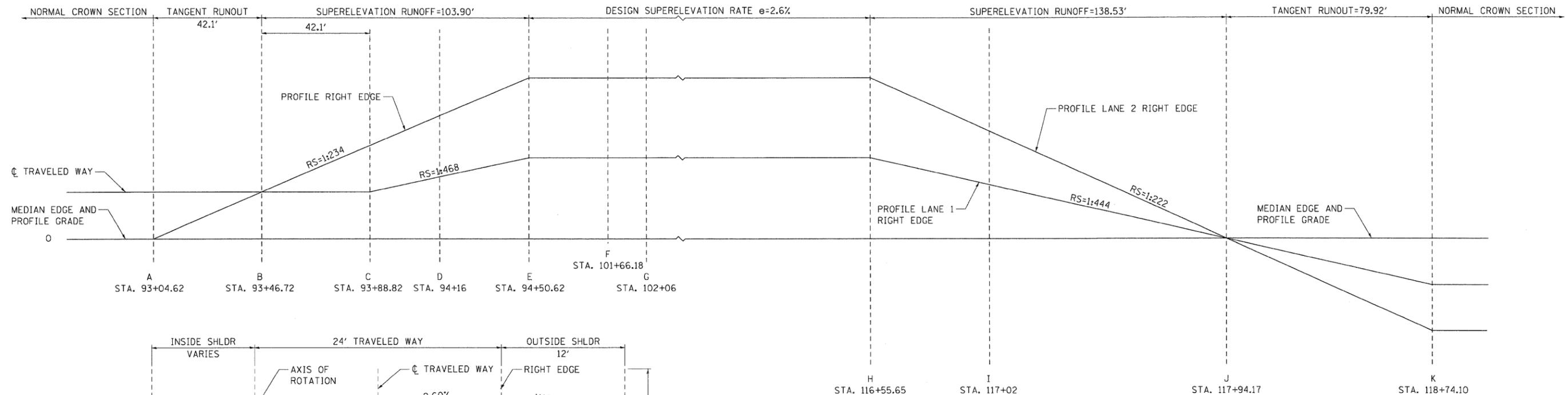
1. SEE HIGHWAY STD 420001, 420306 & 420206 AND DISTRICT 1 DETAILS BD49 & BD52 FOR PAVEMENT JOINT DETAILS.
2. LONGITUDINAL CONSTRUCTION JOINTS W/ TIE BARS SHALL BE PLACED BETWEEN PCC PAVEMENT AND PCC SHOULDER, NOT SHOWN FOR CLARITY.
3. MAINLINE AND RAMP CRCP PAVEMENT REINFORCEMENT & JOINTS ARE NOT SHOWN FOR CLARITY. SEE STANDARDS 420206, 420306, AND 421201.
4. GORE NOSE LOCATIONS SHALL BE ALIGNED WITH MAINLINE SHOULDER TRANSVERSE JOINT.
5. ADDITIONAL SAWED TRANSVERSE CONTRACTION JOINTS MAY BE REQUIRED IN THE FIELD. EXACT LOCATIONS TO BE VERIFIED IN THE FIELD AND APPROVED BY THE ENGINEER.
6. CONCRETE STUBS SHALL HAVE A 1' MINIMUM WIDTH.

PAVEMENT JOINT LEGEND

	LONGITUDINAL KEYED JOINT WITHOUT TIE BARS		SAWED TRANSVERSE CONTRACTION JOINT W/IN PCC SHLD
	LONGITUDINAL CONSTRUCTION JOINT WITH TIE BARS		TRANSVERSE EXPANSION JOINT
	SAWED TRANSVERSE CONTRACTION JOINT		PAVEMENT SEPARATION JOINT
	SAWED LONGITUDINAL JOINT		

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FILE NAME =	USER NAME = #USER#	DESIGNED - JP	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION FAP ROUTE 345 / US ROUTE 20	PAVEMENT JOINTING PLAN MCLEAN BOULEVARD			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE#		DRAWN - MH	REVISED -		345	BR-R	KANE	794	89			
		CHECKED - DDH	REVISED -		CONTRACT NO. 60H45							
		DATE - 12/16/11	REVISED -		SCALE: 1"=30'	SHEET NO.	OF	STA. 71+75.00	TO STA. 77+75.00	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT

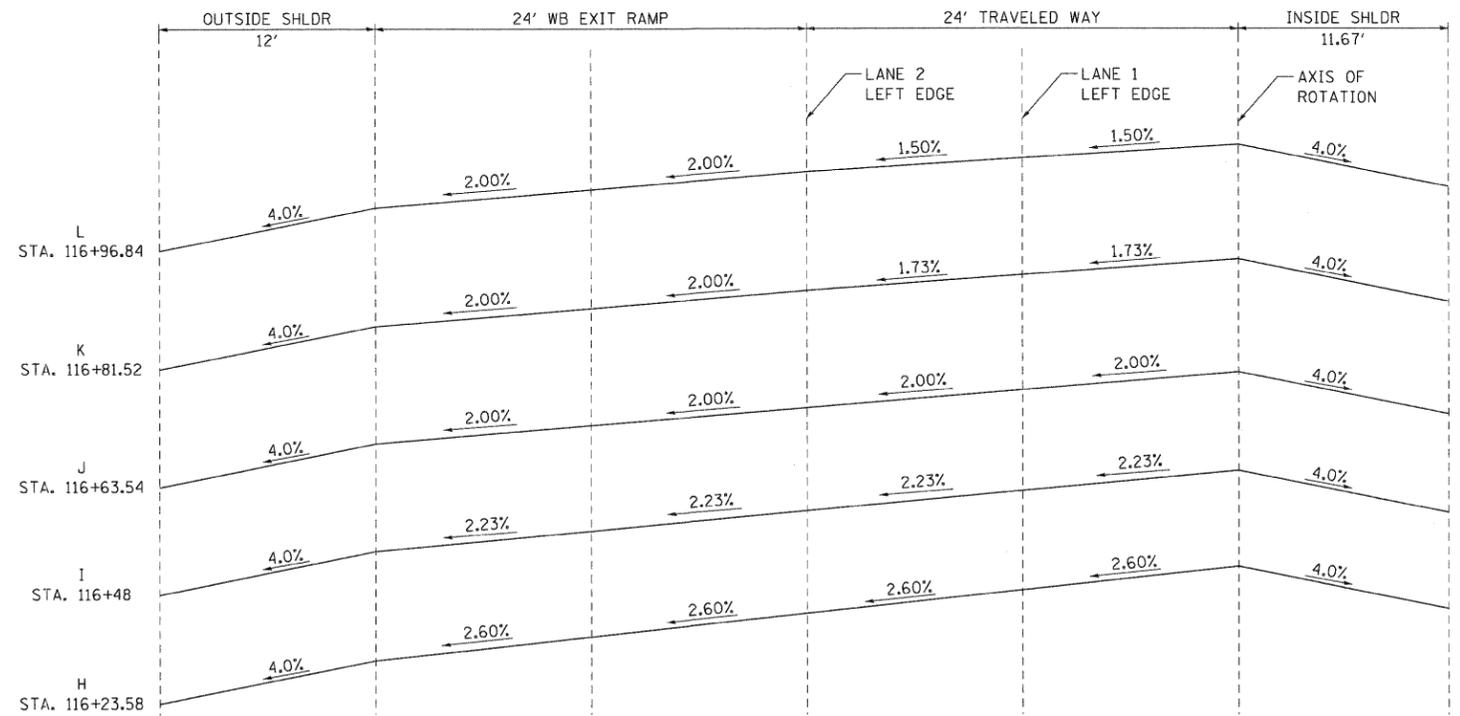
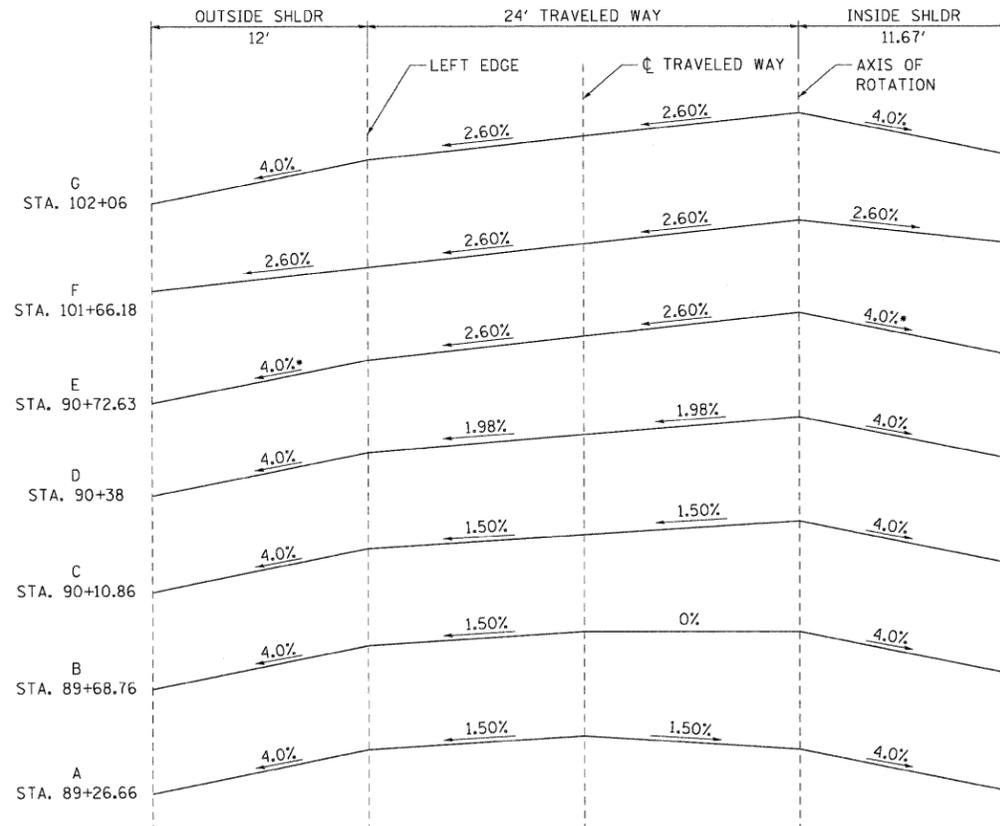
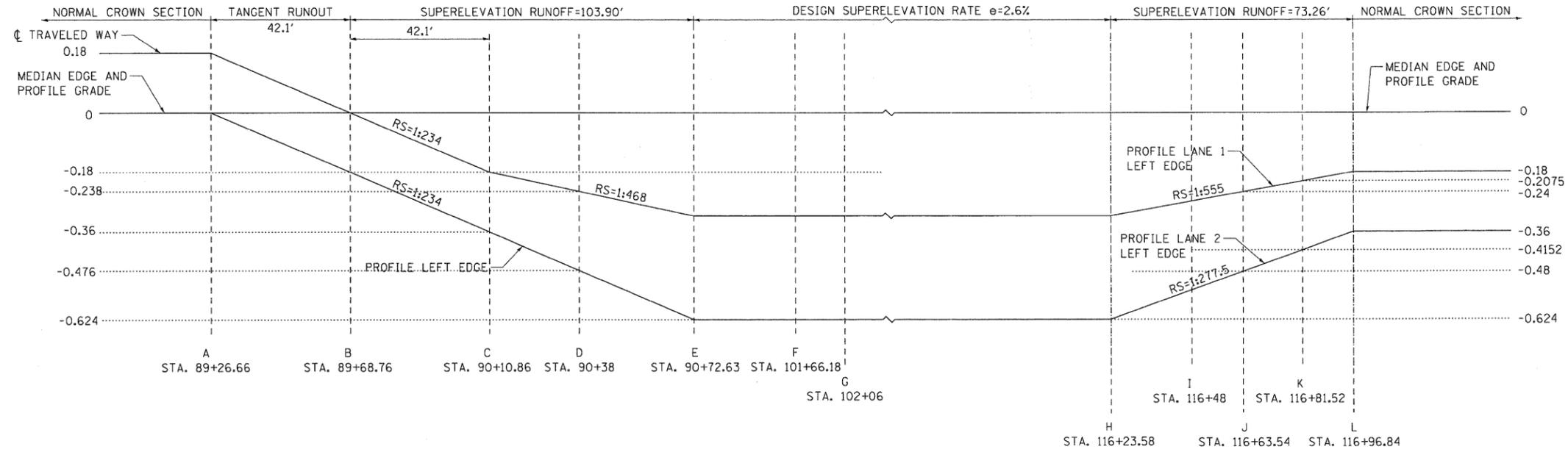


- * INSIDE SHOULDER SHALL ROTATE FROM 4.0% TO 2.6% BETWEEN STA. 94+59 AND STA. 94+98.93
- (1) OUTSIDE SHOULDER SHALL ROTATE FROM -4.0% TO 2.6% FROM STA. 93+14 AND STA. 94+98.93
- (2) OUTSIDE SHOULDER SHALL ROTATE FROM 2.6% TO -4.0% FROM STA. 101+66.18 AND STA. 103+51.

EASTBOUND U.S. ROUTE 20 SUPERELEVATION DETAILS

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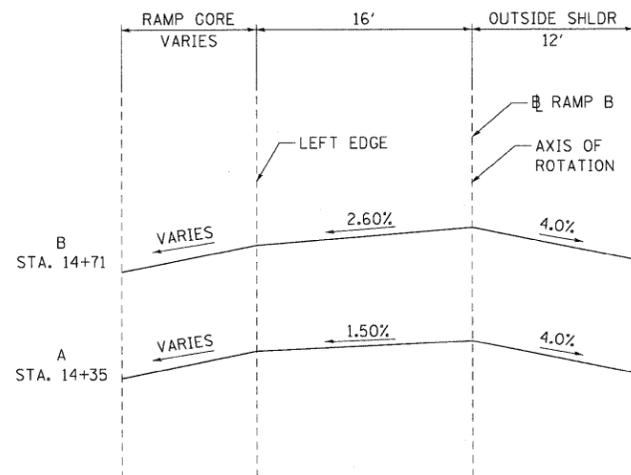
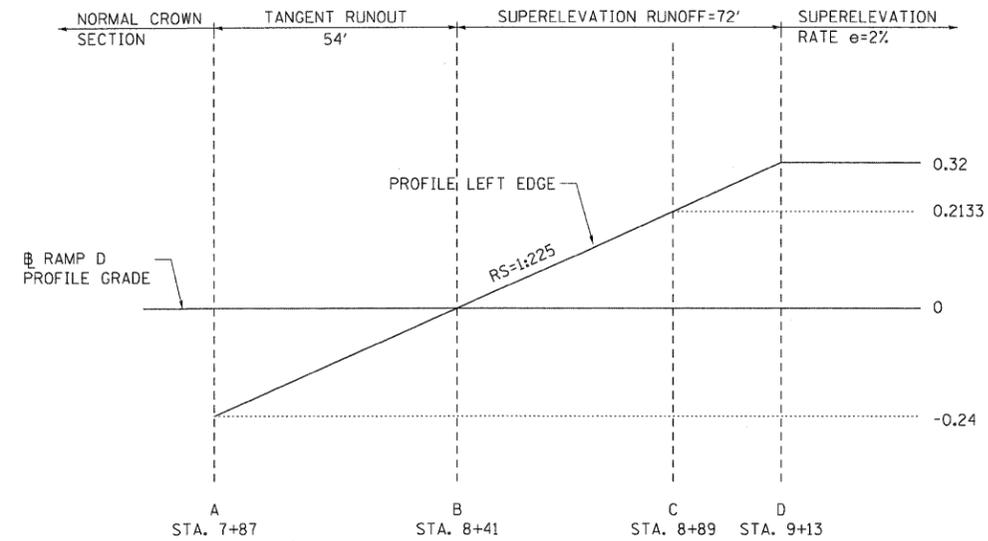
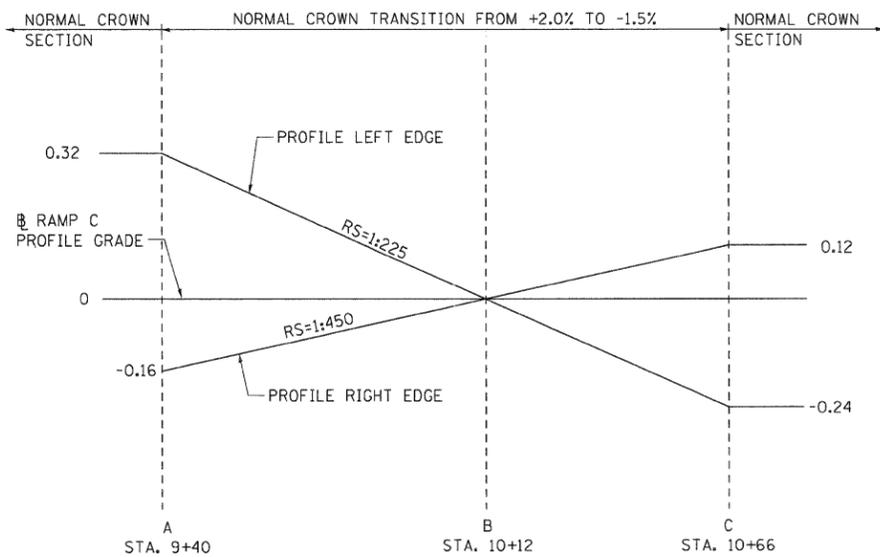
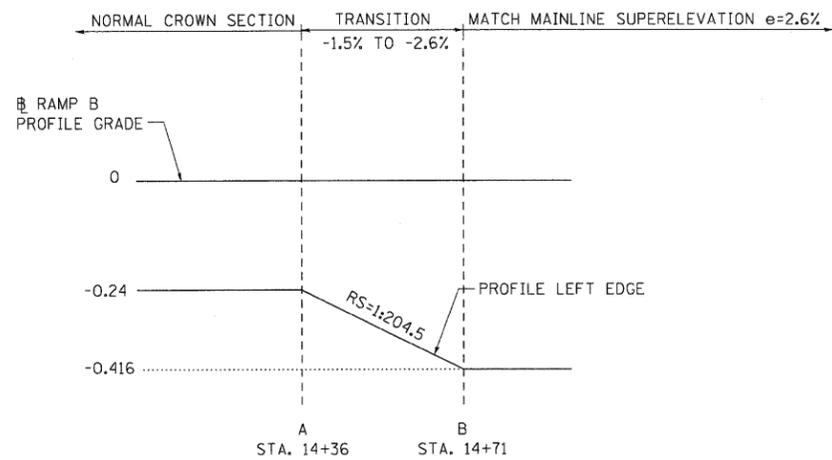


* SHOULDER SHALL ROTATE FROM 4.0% TO 2.6% BETWEEN STA. 94+59 AND STA. 94+98.93

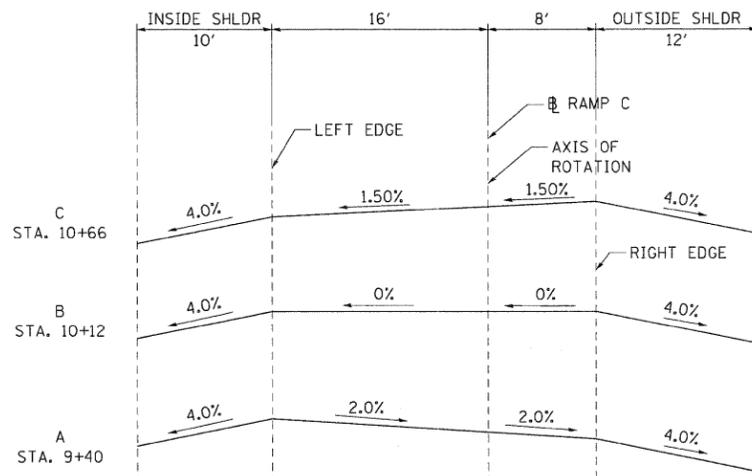
WESTBOUND US 20 SUPERELEVATION DETAILS

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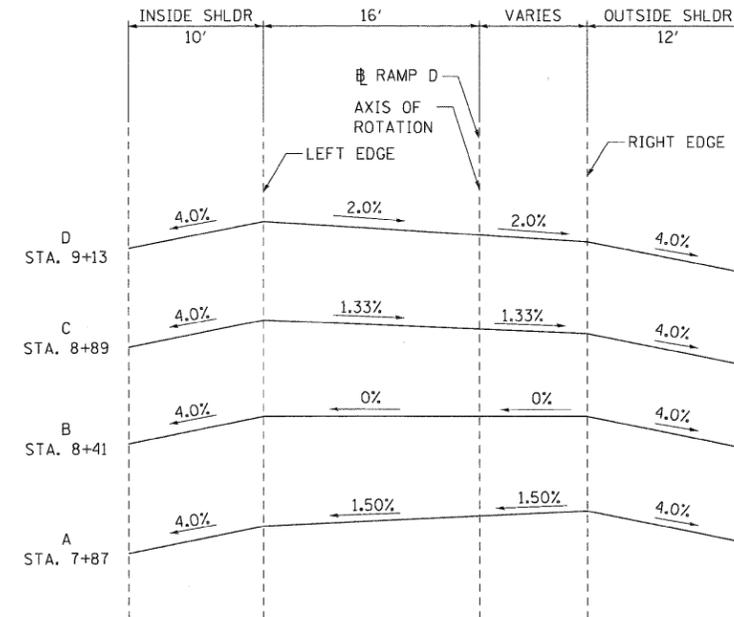
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TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS												



U.S. ROUTE 20 RAMP B SUPERELEVATION DETAILS



U.S. ROUTE 20 RAMP C SUPERELEVATION DETAILS



U.S. ROUTE 20 RAMP D SUPERELEVATION DETAILS

\\168\45-DETAILS\10.DGN, \\168\45-BORDER\10.DGN, \\168\45-SHEET, \\168\45-SHT-ROADDETS\10.DGN
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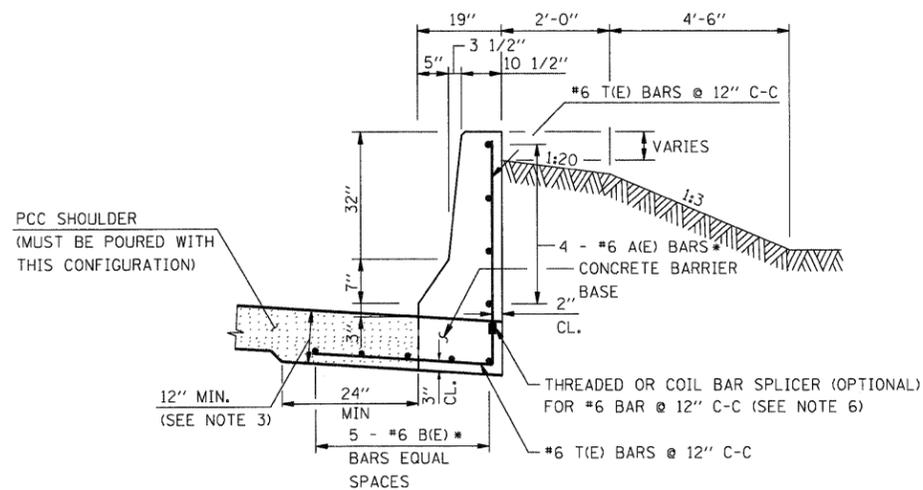
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DATE - 12/16/11	REVISED -

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PLOT DATE = *DATE*

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 FAP ROUTE 345 / US ROUTE 20

ROADWAY DETAILS SUPERELEVATION TRANSITION DETAIL U.S. ROUTE 20 RAMP B, C & D			
SCALE: 1"=10'	SHEET NO.	OF	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	8R-R	KANE	794	98
CONTRACT NO. 60H45				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



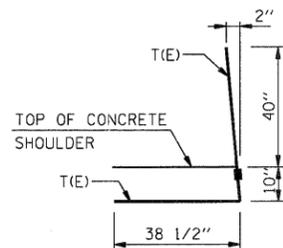
CONCRETE BARRIER, SINGLE FACE, 42" HEIGHT (SPECIAL)

ALONG US 20 WB (SEE SHEETS 65-66)

N.T.S.

CUT SECTION
REINFORCING BARS
20' SECTION

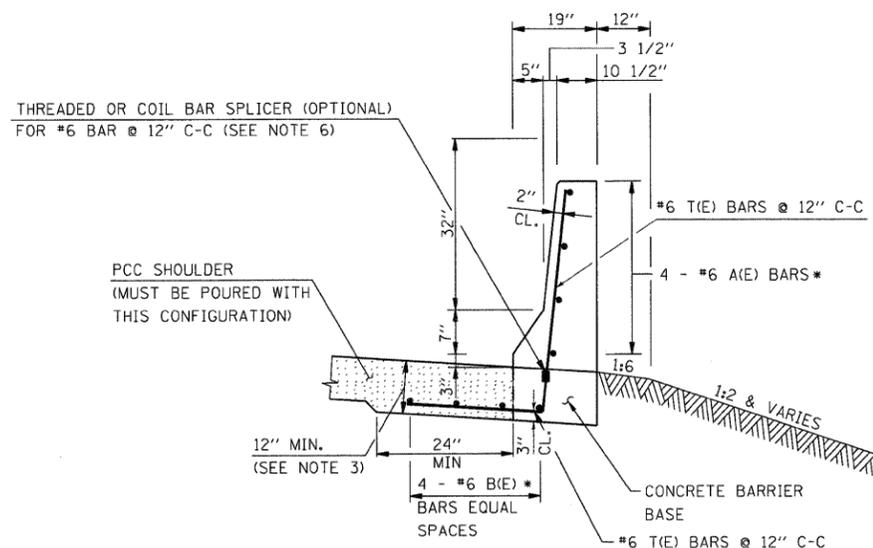
BAR NO.	SIZE	LENGTH	SHAPE
A(E)	#6	23'-6"	—
B(E)	#6	23'-6"	—
T(E)	#6	7'-4 1/2"	┘
REBAR REQUIRED FOR 20' SECTION		POUND	540
REINFORCEMENT BARS, EPOXY COATED (TOTAL FOR 650' BARRIER)		POUND	17,550



T(E) BAR DETAIL

CUT SECTION

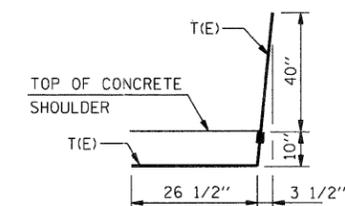
N.T.S.



CONCRETE BARRIER, SINGLE FACE, 42" HEIGHT (SPECIAL)

ALONG RAMP C

N.T.S.



T(E) BAR DETAIL

FILL SECTION

N.T.S.

FILL SECTION
REINFORCING BARS
20' SECTION

BAR NO.	SIZE	LENGTH	SHAPE
A(E)	#6	23'-6"	—
B(E)	#6	23'-6"	—
T(E)	#6	6'-4 1/2"	┘
REBAR REQUIRED FOR 20' SECTION		POUND	480
REINFORCEMENT BARS, EPOXY COATED (TOTAL FOR 940' BARRIER)		POUND	22,560

LEGEND:

PAY LIMITS OF PCC SHOULDERS.

NOTES:

- FOR ADDITIONAL SHOULDER DETAILS, SEE TYPICAL SECTIONS AND STANDARDS 482006, 483001 & 601001 AS APPLICABLE.
- A 1" RADIUS CAN BE SUBSTITUTED FOR THE 1/2" CHAMFER AT THE TOP OF THE BARRIER WALL.
- THE THICKENED PCC SHOULDER FOR THE REINFORCED CONCRETE BARRIER SECTION SHALL BE INCLUDED IN THE COST OF THE CONCRETE BARRIER OF THE TYPE AND HEIGHT SPECIFIED.
- ALL REINFORCING BARS, BAR SPLICERS, TIE BARS AND DOWEL BARS SHALL BE EPOXY COATED AND SHALL BE SEATED IN THE FINAL POSITION PRIOR TO THE CONCRETE OPERATIONS. BARS SHALL NOT BE MUCKED INTO PLACE.
- THE BAR SPLICER SHALL BE CAPABLE OF DEVELOPING A MINIMUM OF 125% OF THE YIELD STRENGTH OF A #6 BAR.
- THE FURNISHING AND PLACING OF OPTIONAL BAR SPLICERS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR CONCRETE BARRIER OF THE TYPE AND HEIGHT SPECIFIED.
- TRANSVERSE CONTRACTION AND EXPANSION JOINTS SHALL BE CONSTRUCTED IN LINE WITH THE CONTRACTION AND EXPANSION JOINTS IN THE ADJACENT SHOULDER.
- THE MINIMUM LAP FOR A(E) AND B(E) BARS SHALL BE 3'-6".

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
FAP ROUTE 345 / US ROUTE 20

ROADWAY DETAILS
CONCRETE BARRIER, SINGLE FACE

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	8R-R	KANE	794	99
CONTRACT NO. 60H45				

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TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
CHICAGO, ILLINOIS

SCALE: N.T.S. SHEET NO. OF STA. TO STA.

FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT

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