

F.A.U. ROUTE	MUNICIPAL SECTION	COUNTY	TOTAL SHEETS	SHTS. NO.
2576	05-00069-00-CH	DuPAGE	42	1
COVER SHEET			22 of 44	
F.H.W.A. REG.5 ILLINOIS CONTRACT NO. 63567				

04-27-12 LETTING ITEM 155

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880006-01	TRAFFIC SIGNAL MOUNTING DETAILS
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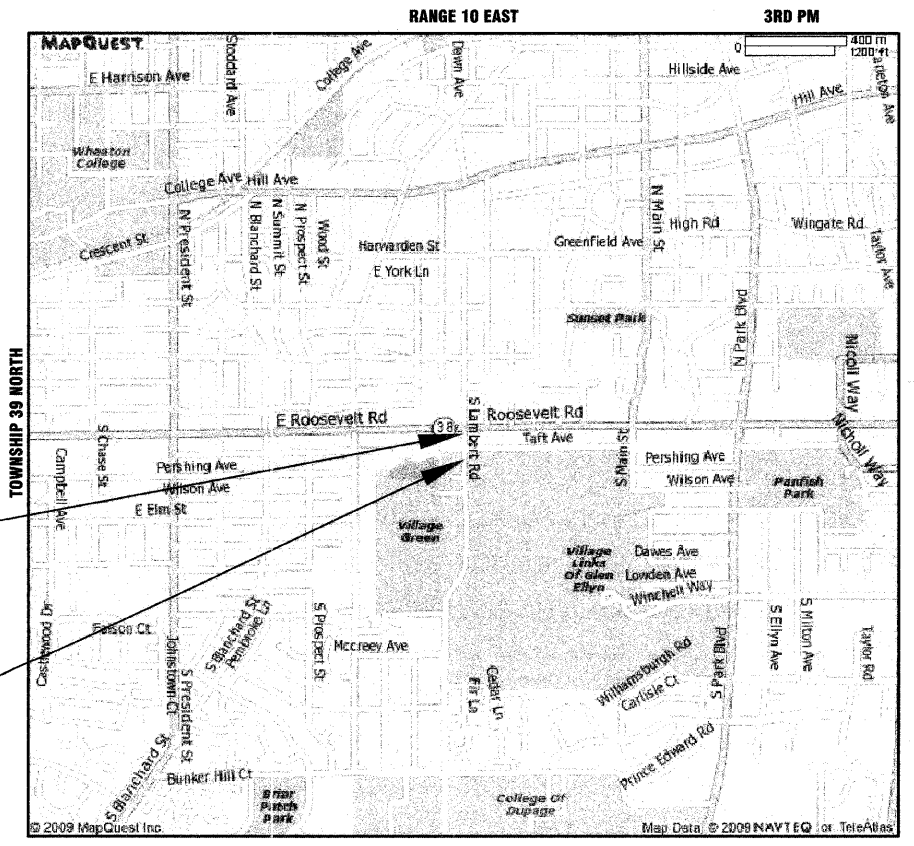
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

F.A.U. 2576 (LAMBERT ROAD) AT IL ROUTE 38 (ROOSEVELT ROAD)
INTERSECTION IMPROVEMENT
SECTION 05-00069-00-CH
PROJECT NO. M-8003 (923)
VILLAGE OF GLEN ELLYN
DuPAGE COUNTY
JOB NO. C-91-139-08

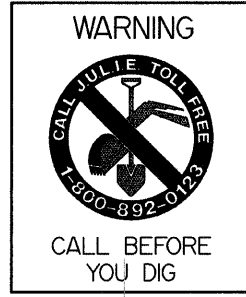


LOCATION OF SECTION INDICATED THIS: — ■ —



PROJECT ENDS
STATION 9+74

PROJECT BEGINS
STATION 4+25



ADT = 11,200 VPD
 POSTED SPEED LIMIT = 30 MPH
 DESIGN SPEED = 30 MPH
 DESIGN DESIGNATION = COLLECTOR URBAN
 CONTRACT NO. 63567

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

LOCATION MAP
 APPROXIMATE SCALE 1" = 600'
 GROSS LENGTH OF PROJECT = 549 FT (0.1 MILE)
 NET LENGTH OF PROJECT = 549 FT (0.1 MILE)

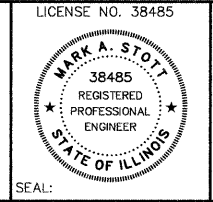
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

APPROVED 2/2 2012
Julia Hansen
 VILLAGE OF GLEN ELLYN

PASSED FEBRUARY 10 2012
C. Holt
 DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID
 BASED ON LIMITED
 REVIEW FEBRUARY 14 2012
Diana M. O'Keefe
 DEPUTY DIRECTOR OF HIGHWAYS REGION 1 ENGINEER

DATE: FEBRUARY 2, 2012
 BY: Mark A. Stott
 LICENSE EXPIRES: NOVEMBER 30, 2013



HLR
 Hampton Lenzini and Renwick, Inc.
 Civil Engineers
 Land Surveyors
 380 Shepard Drive
 Elgin, Illinois 60123-7010
 847.697.8700
 Account Number 07.0042.330

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. 847-705-4406, SCHAUMBURG, IL

GENERAL NOTES

GENERAL

ALL WORK SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION," ADOPTED JANUARY 1, 2007, (HEREINAFTER REFERRED TO AS THE STANDARD SPECIFICATIONS); THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS," ADOPTED JANUARY 1, 2011; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS"; THE DETAILS IN THE PLANS AND THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS.

THE CONTRACTOR SHALL COOPERATE WITH THE VILLAGE OF GLEN ELLYN IF ANY MUNICIPAL UTILITY IMPROVEMENTS ARE REQUIRED WITHIN THE DURATION OF THE CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT OR PROTECTION IS NECESSARY.

THE LOCATION ON THE PLANS OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC AND PRIVATE UTILITIES IS APPROXIMATE AND THEIR EXACT LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER AT THE CONTRACTOR'S EXPENSE.

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS AND REFERENCE MARKERS UNTIL THE OWNER, HIS AGENT OR A PROFESSIONAL LAND SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

ALL ELEVATIONS ARE ON U.S.G.S. DATUM.

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR HIS WORK. ANY WORK THAT IS VANDALIZED OR OTHERWISE DAMAGED AND JUDGED UNACCEPTABLE BY THE ENGINEER SHALL BE REMOVED AND REPLACED AT THE CONTRACTOR'S EXPENSE.

THE CONTRACTOR SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO BEGINNING WORK.

FORTY-EIGHT HOURS BEFORE STARTING EXCAVATION, THE CONTRACTOR WILL CALL J.U.L.I.E. (1-800-892-0123) TO HAVE THE LOCATION OF EXISTING UTILITIES STAKED.

THE CONTRACTOR SHALL AT ALL TIMES PROVIDE PROTECTION FOR TRAFFIC AS REQUIRED BY THE APPLICATION OF TRAFFIC CONTROL DEVICES, THE STANDARD SPECIFICATIONS AND THE PLANS.

ALL WORK INVOLVING EXISTING SIGNS SHALL BE GOVERNED BY THE FOLLOWING:

- A) SIGNS SHALL NOT BE REMOVED UNTIL THE PROGRESS OF WORK NECESSITATES IT.
- B) EACH SIGN TO BE REMOVED MUST BE RE-ERECTED AT A TEMPORARY LOCATION APPROVED BY THE ENGINEER IN A WORKMANLIKE MANNER AND SHALL BE VISIBLE TO THE TRAFFIC FOR WHICH IT IS INTENDED. ALL SUCH SIGNS SHALL BE MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF THE TEMPORARY SETTING.
- C) ALL SIGNS SHALL BE RE-ERECTED AT PERMANENT LOCATIONS AS THE COMPLETION OF THE ROADWAY IMPROVEMENTS PERMIT. LOCATIONS SHALL BE APPROVED BY THE ENGINEER.
- D) ALL UNUSED SIGNS SHALL BE STORED ON THE JOBSITE FOR PICKUP BY THE VILLAGE.
- E) LONGER POSTS MAY BE REQUIRED AT SOME TEMPORARY LOCATIONS TO MAINTAIN PROPER SIGN HEIGHT. IN SUCH CASES, POSTS SHALL BE FURNISHED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE BACK OF CURB, UNLESS OTHERWISE NOTED. ELEVATIONS SHOWN AT POINTS OF CURVE, ETC. ARE TO THE EDGE OF PAVEMENT, UNLESS OTHERWISE NOTED.

STRUCTURE LOCATIONS GIVEN ON THE PLANS ARE AS FOLLOWS:

- A) FOR STRUCTURES FALLING IN THE CURB & GUTTER - TO THE BACK OF CURB.
- B) FOR OTHER LOCATIONS - TO THE CENTER OF THE STRUCTURE.

CONSTRUCTION

ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS OR DRAINAGE STRUCTURES WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT.

WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN IN AN OPERATING CONDITION TEMPORARY OUTLETS AND CONNECTIONS FOR ALL DRAINS, SEWERS AND CATCH BASINS. THE CONTRACTOR SHALL PROVIDE FACILITIES WHICH HAVE THE CAPACITY TO RECEIVE AND DISCHARGE THE STORM FLOWS NORMALLY ACCEPTED AND RELEASED BY THE EXISTING FACILITIES. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT.

THE COST OF INTERCONNECTIONS BETWEEN THE PROPOSED AND EXISTING SEWER SYSTEMS AND PROPOSED AND EXISTING WATER MAIN SHALL BE INCLUDED IN THE VARIOUS UNIT PRICES OF THE PROPOSED UNITS OF WORK.

ALL FRAMES WITH CLOSED LIDS TO BE FURNISHED AS PART OF THIS CONTRACT SHALL HAVE CAST INTO THE LID THE WORD "STORM," "SANITARY" OR "WATER," AS APPROPRIATE TO THE TYPE OF STRUCTURE INVOLVED. ALL STORM SEWER FRAMES AND GRATES SHALL INCLUDE AN ENVIRONMENTAL NOTICE "DUMP NO WASTE, DRAINS TO WATERWAYS".

TRENCH BACKFILL QUANTITIES HAVE BEEN COMPUTED IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION, DIVISION OF HIGHWAYS, BUREAU OF CONSTRUCTION TRENCH BACKFILL TABLE, BASED ON PLAN INVERT DEPTH FROM SUBGRADE. ANY TRENCH BACKFILL REQUIRED IN EXCESS OF THE QUANTITY ESTABLISHED ABOVE, INCLUDING BEDDING MATERIAL, SHALL BE INCLUDED IN THE CCST OF THE CONTRACT.

IF THE CONTRACTOR CHOOSES TO DISPOSE OF UNCONTAMINATED SOIL OR UNCONTAMINATED SOIL MIXED WITH CLEAN CONSTRUCTION AND DEMOLITION DEBRIS (CCDD) AT A CCDD FILL OPERATION, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PERFORM ALL NECESSARY FIELD AND LABORATORY ANALYSIS AND TO OBTAIN THE LICENSED PROFESSIONAL ENGINEER'S CERTIFICATION REQUIRED AS PER PUBLIC ACT 96-1416 TO USE THE SITE. NO ADDITIONAL COMPENSATION WILL BE PROVIDED.

ANY MATERIALS CONSIDERED SUITABLE FOR SALVAGE BY THE ENGINEER SHALL BE STORED WITHIN THE RIGHT-OF-WAY FOR LATER REMOVAL BY THE VILLAGE OF GLEN ELLYN. UNUSABLE MATERIALS SHALL BE DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY IN ACCORDANCE WITH ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. TRENCH BACKFILL AND/OR PAVEMENT REPLACEMENT AND/OR AGGREGATE BASE COURSE TYPE A WILL BE PAID FOR WHEN THE WORK LIES UNDER EXISTING PAVEMENT AREAS.

TRENCHES ACROSS PAVED SURFACES SHALL BE PATCHED WITH EITHER PERMANENT OR TEMPORARY PAVEMENT AT THE END OF EACH WORK DAY. TEMPORARY PATCHING OF TRENCHES INCLUDING REMOVAL WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE ITEM PLACED IN THE TRENCH.

MISCELLANEOUS

THE CONTRACTOR SHALL PROVIDE ACCESS TO ABUTTING PROPERTIES AT ALL TIMES DURING CONSTRUCTION OF THIS PROJECT. ANY COST INCURRED BY THE CONTRACTOR TO MEET THIS REQUIREMENT THAT IS NOT COVERED BY A SPECIFIC PAY ITEM WILL BE INCLUDED IN THE COST OF THE CONTRACT.

SAWING OF REMOVAL ITEMS AS NOTED ON THE PLANS, SPECIFIED IN THE STANDARD SPECIFICATIONS OR AS REQUIRED BY THE ENGINEER SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED, UNLESS NOTED OTHERWISE.

THE EXISTING ASPHALT SURFACE SHALL BE SAW CUT TO A DEPTH OF TWO INCHES AT ALL BUTT JOINTS.

WHERE NEW WORK IS PROPOSED TO MEET EXISTING FEATURES, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FIELD CHECK ALL DIMENSIONS AND ELEVATIONS AND NOTIFY THE ENGINEER OF DISCREPANCIES BEFORE PROCEEDING WITH CONSTRUCTION.

WHERE PROPOSED CURB AND/OR CURB AND GUTTER MEETS EXISTING, THE PROPOSED SHALL TRANSITION TO THE EXISTING IN A DISTANCE OF TEN FEET OR AS DIRECTED BY THE ENGINEER. THE TRANSITION LENGTH WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEM.

ADA COMPLIANT SIDEWALK RAMP FOR THE HANDICAPPED SHALL BE INSTALLED AT ALL INTERSECTIONS AND DRIVEWAYS IN ACCORDANCE WITH CURRENT STANDARDS.

ANY TEMPORARY SHEETING AND/OR SHORING USED ON THIS IMPROVEMENT SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

PROTECTIVE COAT SHALL BE APPLIED TO ALL CURBS, GUTTERS AND P.C.C. PAVEMENTS, SIDEWALKS AND DRIVEWAYS.

SEDIMENTATION AND EROSION CONTROL

SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL BE UTILIZED IN CONSIDERATION OF TIME OF YEAR, SITE CONDITIONS AND THE SUITABILITY OF TEMPORARY VERSUS PERMANENT MEASURES.

SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.

DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN FOURTEEN CALENDAR DAYS OF THE END OF THE ACTIVE HYDROLOGIC DISTURBANCE.

ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY APPROPRIATE EROSION CONTROL MEASURES.

ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN THIRTY DAYS AFTER THE FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED.

ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES MUST BE MAINTAINED AND REPAIRED BY THE CONTRACTOR AS REQUIRED BY THE SPECIAL PROVISIONS.

IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH EFFECTIVE SEDIMENT CONTROL MEASURES (e.g., SEDIMENT TRAPS, SEDIMENT BASINS OR OTHER APPROPRIATE MEASURES). ADDITIONAL SEDIMENT CONTROL MEASURES SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER OR GOVERNING AGENCY.

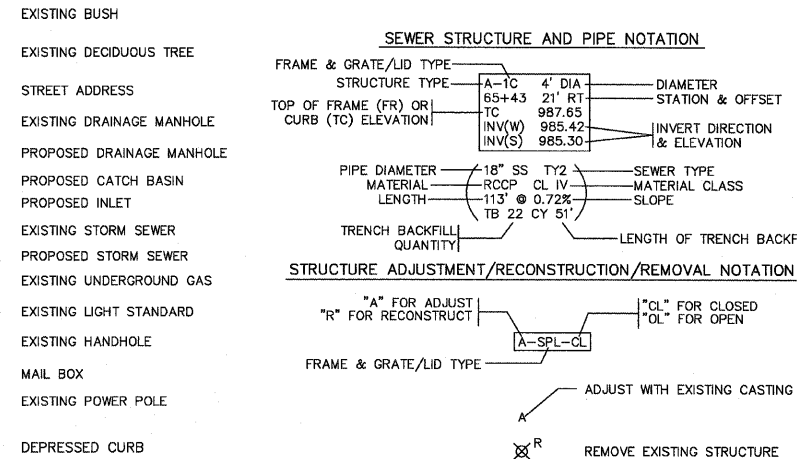
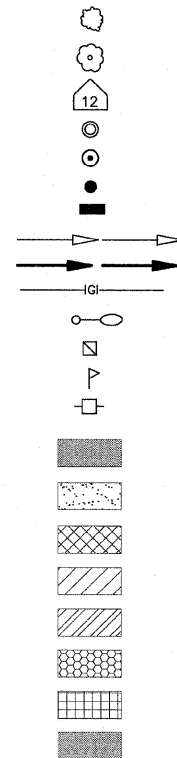
THE CONTRACTOR SHALL MILL BEFORE PATCHING

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
PAVEMENT RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5mm); 2"	4% @ 70 GYR.
LEVELING BINDER (MACHINE METHOD) N70 (IL 9.5mm)	4% @ 70 GYR.
PAVEMENT WIDENING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5mm); 2"	4% @ 70 GYR.
HOT-MIX ASPHALT BASE COURSE, (HMA BINDER IL-19mm); 9 1/4"	4% @ 70 GYR. (IN 3 LIFTS)
PATCHING, 9"	
CLASS D PATCHES (HMA BINDER IL-19mm)	4% @ 70 GYR. (IN 3 LIFTS)
HMA DRIVEWAY	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5mm); 2"	4% @ 70 GYR. (IN 3 LIFTS)
HOT-MIX ASPHALT BASE COURSE, (HMA BINDER IL-19mm); 9 1/4"	4% @ 70 GYR. (IN 3 LIFTS)

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

LEGEND



FILE NAME =	USER NAME =	DESIGNED --	REVISED --
December 18, 2010 2:48:23 p.m.		DRAWN --	REVISED --
P:\03\03040036\CAD\PHASE 2\DWG\040036-SHT-COVER.DWG		CHECKED --	REVISED --
		DATE --	REVISED --

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**



**LAMBERT ROAD AT ROUTE 38
 GENERAL NOTES AND LEGEND**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2567	05-00069-00-CH	DuPAGE	42	2
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 63567				

SCALE: SHEET NO. OF SHEETS STA. TO STA.

PI#	ITEM DESCRIPTION	SPECIAL PROVISION	SPECIALTY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
						0004	0021
						ROADWAY	TRAFFIC SIGNALS
20200100	EARTH EXCAVATION			CU YD	296	296	
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL			CU YD	50	50	
20800150	TRENCH BACKFILL			CU YD	27	27	
21101615	TOPSOIL FURNISH AND PLACE, 4"			SQ YD	360	360	
21301072	EXPLORATION TRENCH 72" DEPTH			FOOT	50	50	
25000400	NITROGEN FERTILIZER NUTRIENT			POUND	5	5	
25000600	POTASSIUM FERTILIZER NUTRIENT			POUND	5	5	
25200110	SODDING, SALT TOLERANT			SQ YD	360	360	
25200200	SUPPLEMENTAL WATERING			UNIT	11	11	
28000250	TEMPORARY EROSION CONTROL SEEDING			POUND	37	37	
28000400	PERIMETER EROSION BARRIER			FOOT	200	200	
28000510	INLET FILTERS			EACH	7	7	
31101400	SUBBASE GRANULAR MATERIAL, TYPE B 6"			SQ YD	453	453	
35501321	HOT-MIX ASPHALT BASE COURSE, 9 1/4"			SQ YD	340	340	
40201000	AGGREGATE FOR TEMPORARY ACCESS			TON	74	74	
40600100	BITUMINOUS MATERIALS (PRIME COAT)			GALLON	273	273	
40600300	AGGREGATE (PRIME COAT)			TON	11	11	
40600635	LEVELING BINDER (MACHINE METHOD), N70			TON	109	109	
40600882	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT			SQ YD	307	307	
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70			TON	329	329	
42001300	PROTECTIVE COAT			SQ YD	325	325	
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH			SQ YD	39	39	
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH			SQ FT	368	368	
42400800	DETECTABLE WARNINGS			SQ FT	39	39	
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"			SQ YD	943	943	
44000200	DRIVEWAY PAVEMENT REMOVAL			SQ YD	72	72	
44000300	CURB REMOVAL			FOOT	164	164	
44000500	COMBINATION CURB AND GUTTER REMOVAL			FOOT	745	745	
44000600	SIDEWALK REMOVAL			SQ FT	580	580	
44200114	PAVEMENT PATCHING, TYPE IV, 9 INCH			SQ YD	31	31	
44201337	CLASS C PATCHES, TYPE I, 9 INCH			SQ YD	3	3	
44201341	CLASS C PATCHES, TYPE II, 9 INCH			SQ YD	10	10	
44201347	CLASS C PATCHES, TYPE IV, 9 INCH			SQ YD	48	48	
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT			FOOT	485	485	
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"			FOOT	23	23	
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"			FOOT	48	48	
55100500	STORM SEWER REMOVAL 12"			FOOT	41	41	
56400200	FIRE HYDRANTS TO BE MOVED (SPECIAL)	*	X	EACH	1	1	
60200305	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 3 FRAME AND GRATE	*		EACH	4	4	
60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	*		EACH	1	1	
60250200	CATCH BASINS TO BE ADJUSTED			EACH	1	1	
60255500	MANHOLES TO BE ADJUSTED			EACH	5	5	
60265700	VALVE VAULTS TO BE ADJUSTED			EACH	2	2	
60266600	VALVE BOXES TO BE ADJUSTED			EACH	1	1	
60500050	REMOVING CATCH BASINS			EACH	1	1	
60600605	CONCRETE CURB, TYPE B			FOOT	104	104	
60604400	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6, 18			FOOT	718	718	
67100100	MOBILIZATION			L SUM	1	1	
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701			L SUM	1	1	
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801			L SUM	1	1	
70300100	SHORT TERM PAVEMENT MARKING		X	FOOT	125	125	

PI#	ITEM DESCRIPTION	SPECIAL PROVISION	SPECIALTY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
						0004	0021
						ROADWAY	TRAFFIC SIGNALS
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS			SQ FT	73	73	
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"			FOOT	1000	1000	
72000100	SIGN PANEL - TYPE 1		X	SQ FT	18	18	
72000200	SIGN PANEL - TYPE 2		X	SQ FT	10	10	
72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A		X	EACH	3	3	
72400600	RELOCATE SIGN PANEL ASSEMBLY - TYPE B		X	EACH	1	1	
72400710	RELOCATE SIGN PANEL - TYPE 1		X	SQ FT	7		7
72900100	METAL POST - TYPE A		X	FOOT	31	31	
72900200	METAL POST - TYPE B		X	FOOT	41	41	
78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS		X	SQ FT	136	136	
78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"		X	FOOT	1258	1258	
78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"		X	FOOT	1201	1201	
78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"		X	FOOT	128	128	
78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"		X	FOOT	89	89	
78100100	RAISED REFLECTIVE PAVEMENT MARKER		X	EACH	42	42	
78300100	PAVEMENT MARKING REMOVAL		X	SQ FT	786	786	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL		X	EACH	14	14	
80500010	SERVICE INSTALLATION - GROUND MOUNTED		X	EACH	1		1
81028190	UNDERGROUND CONDUIT, GALVANIZED STEEL, 1 1/2" DIA.		X	FOOT	180		180
81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.		X	FOOT	105		105
81028210	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.		X	FOOT	78		78
81028220	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.		X	FOOT	10		10
81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.		X	FOOT	290		290
81400100	HANDHOLE		X	EACH	1		1
81400200	HEAVY-DUTY HANDHOLE		X	EACH	1		1
81400300	DOUBLE HANDHOLE		X	EACH	1		1
81603176	UNIT DUCT, 600V, 6-1C NO.4, 1/2" NO.6 GROUND, (XLP-TYPE USE), 1 1/2" DIA. POLYETHYLENE		X	FOOT	308		308
81702100	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/2" NO. 12		X	FOOT	171		171
81702110	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/2" NO. 10		X	FOOT	375		375
83600200	LIGHT POLE FOUNDATION, 24" DIAMETER		X	FOOT	18		18
84200804	REMOVAL OF POLE FOUNDATION		X	EACH	2		2
84400105	RELOCATE EXISTING LIGHTING UNIT		X	EACH	1		1
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	*	X	EACH	2		2
85100100	PAINT EXISTING TRAFFIC SIGNAL EQUIPMENT	*	X	EACH	1		1
85100500	PAINT NEW TRAFFIC SIGNAL POST		X	EACH	3		3
86400100	TRANSCEIVER - FIBER OPTIC		X	EACH	1		1
87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C		X	FOOT	697		697
87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C		X	FOOT	936		936
87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C		X	FOOT	613		613
87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C		X	FOOT	1443		1443
87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR		X	FOOT	1625		1625
87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C		X	FOOT	42		42
87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C		X	FOOT	594		594
87502440	TRAFFIC SIGNAL POST, GALVANIZED STEEL 10 FT.		X	EACH	2		2
87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.		X	EACH	1		1
87800100	CONCRETE FOUNDATION, TYPE A		X	FOOT	16		16
87800150	CONCRETE FOUNDATION, TYPE C		X	FOOT	4		4
87800400	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER		X	FOOT	15		15
87900200	DRILL EXISTING HANDHOLE		X	EACH	4		4
88030100	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED		X	EACH	2		2
88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER		X	EACH	6		6

PI#	ITEM DESCRIPTION	SPECIAL PROVISION	SPECIALTY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
						000#	0021
						ROADWAY	TRAFFIC SIGNALS
88600100	INDUCTIVE LOOP DETECTOR		X	EACH	9		9
88600100	DETECTOR LOOP, TYPE I		X	FOOT	437		437
88800100	PEDESTRIAN PUSH-BUTTON		X	EACH	6		6
89000100	TEMPORARY TRAFFIC SIGNAL INSTALLATION		X	EACH	1		1
89500100	RELOCATE EXISTING SIGNAL HEAD		X	EACH	3		3
89501300	RELOCATE EXISTING MAST ARM ASSEMBLY AND POLE		X	EACH	1		1
89501400	RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, DETECTOR UNIT	*	X	EACH	1		1
89501410	RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, PHASING UNIT	*	X	EACH	1		1
89502300	REMOVE ELECTRIC CABLE FROM CONDUIT		X	FOOT	1088		1088
89502350	REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT		X	FOOT	713		713
89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	*	X	EACH	1		1
89502380	REMOVE EXISTING HANDHOLE	*	X	EACH	1		1
89502385	REMOVE EXISTING CONCRETE FOUNDATION	*	X	EACH	3		3
Z0004542	HOT-MIX ASPHALT REMOVAL (SPECIAL)	*		SQ YD	1484	1484	
Z0013798	CONSTRUCTION LAYOUT	*		L SUM	1	1	
Z0030850	TEMPORARY INFORMATION SIGNING	*		SQ FT	51		51
Z0033028	MAINTENANCE OF LIGHTING SYSTEM	*	X	CAL MO	3		3
Z0033044	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	*	X	EACH	1		1
Z0042002	POROUS GRANULAR EMBANKMENT, SUBGRADE	*		CU YD	50	50	
Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	*	X	EACH	1		1
X0322915	METAL LIGHT POLE, INSTALL ONLY	*	X	EACH	2		2
X0326864	BRICK SIDEWALK REMOVAL	*		SQ FT	1194	1194	
X0540000	BRICK PAVERS	*		SQ FT	100	100	
X8100105	CONDUIT SPLICE	*	X	EACH	1		1
X8410105	TEMPORARY LIGHTING SYSTEM	*	X	EACH	1		1
X8570226	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	*	X	EACH	1		1
X8620200	UNINTERRUPTABLE POWER SUPPLY, SPECIAL	*	X	EACH	1		1
X8730250	ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED	*	X	FOOT	243		243
XX001368	PULL EXISTING CABLE FROM UNIT DUCT	*	X	FOOT	55		55
XX002094	REMOVE AND REINSTALL FIBER OPTIC CABLE IN CONDUIT	*	X	L SUM	1		1
XX004688	BRICK PAVER SIDEWALK	*		SQ FT	970	970	
XX007259	SIGN AND POST REMOVAL	*		EACH	2	2	
XX007729	DETECTABLE WARNINGS, SPECIAL	*		SQ FT	72	72	

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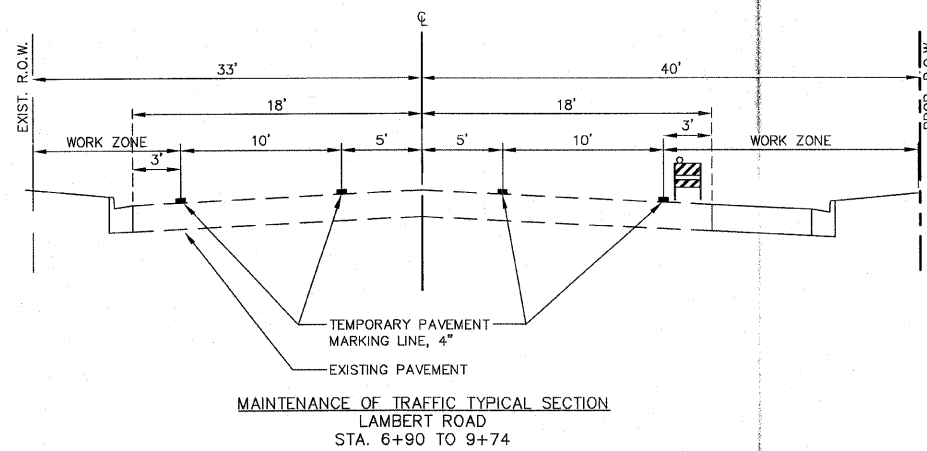
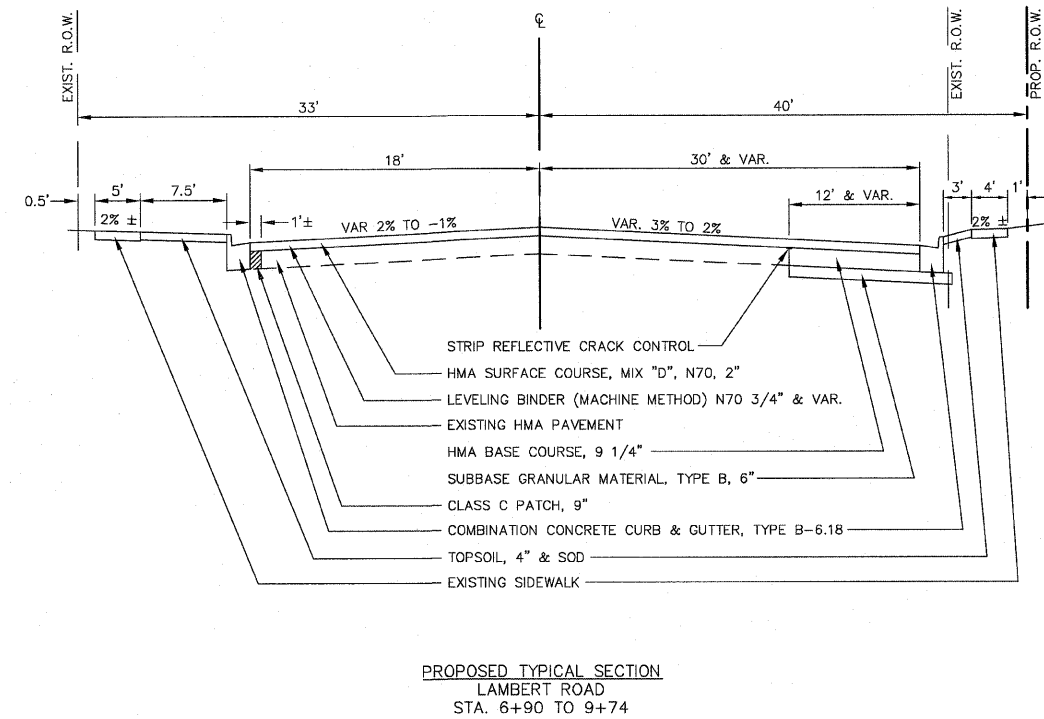
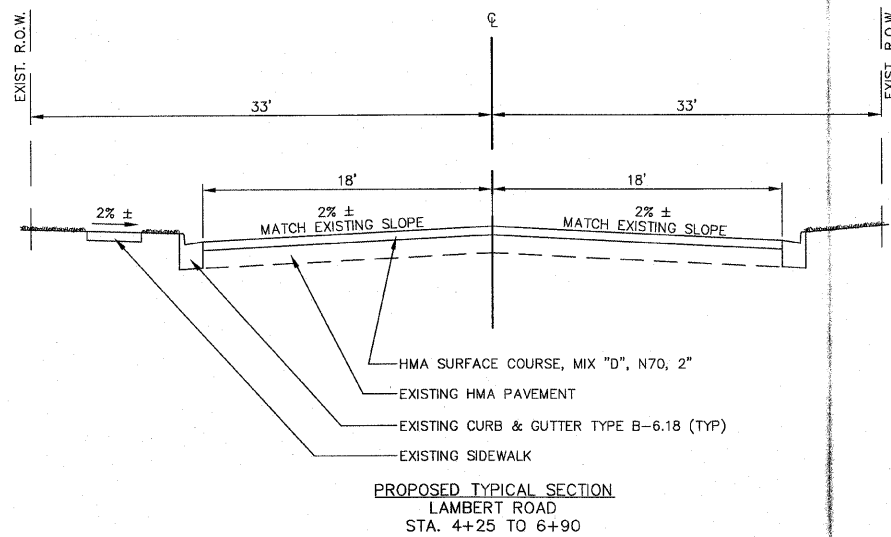
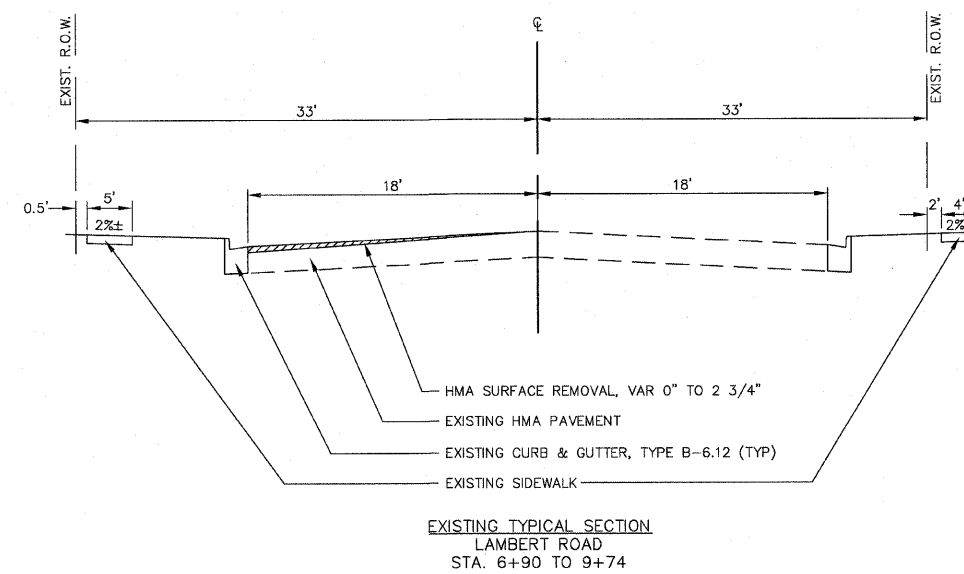
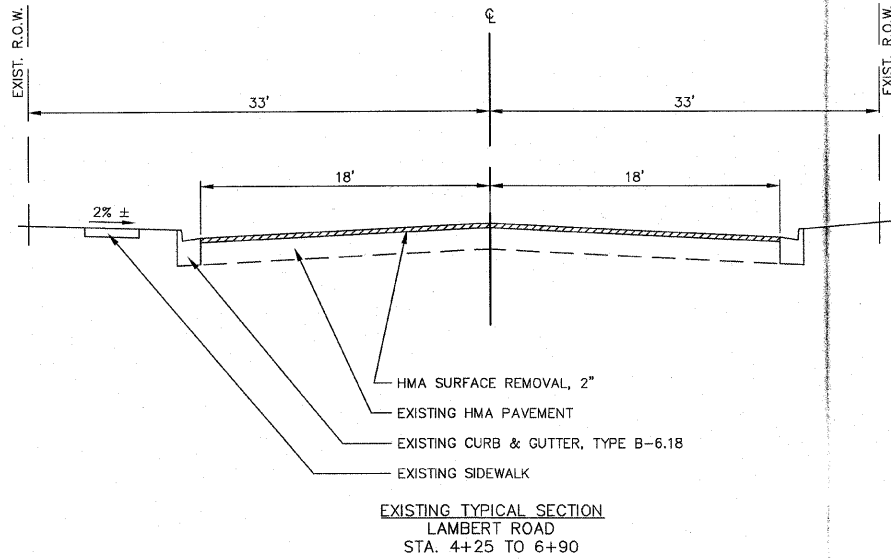
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**



**LAMBERT ROAD AT ROUTE 38
 SUMMARY OF QUANTITIES**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2567	05-00069-00-CH	DuPAGE	42	3A
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 63567	



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 Layout: 4-Typical Sections

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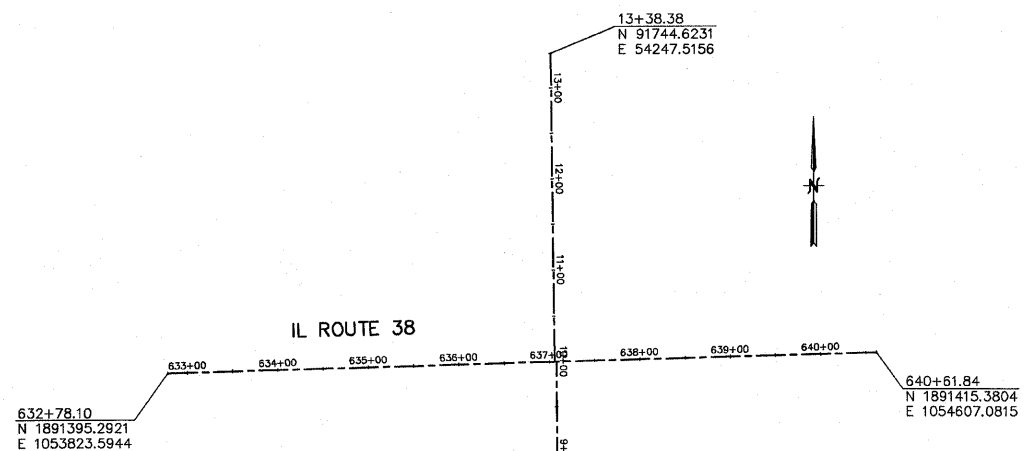
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



LAMBERT ROAD AT ROUTE 38
 TYPICAL SECTIONS

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2567	05-00069-00-CH	DuPAGE	42	4
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63567	



ILLINOIS ROUTE 38 COORDINATES

633+00	1891395.8534	1053845.4872	N 88-31-53 E
633+50	1891397.1350	1053895.4708	N 88-31-53 E
634+00	1891398.4165	1053945.4543	N 88-31-53 E
634+50	1891399.6981	1053995.4379	N 88-31-53 E
635+00	1891400.9796	1054045.4215	N 88-31-53 E
635+50	1891402.2612	1054095.4051	N 88-31-53 E
636+00	1891403.5428	1054145.3886	N 88-31-53 E
636+50	1891404.8243	1054195.3722	N 88-31-53 E
637+00	1891406.1059	1054245.3558	N 88-31-53 E
637+50	1891407.3875	1054295.3394	N 88-31-53 E
638+00	1891408.6690	1054345.3229	N 88-31-53 E
638+50	1891409.9506	1054395.3065	N 88-31-53 E
639+00	1891411.2321	1054445.2901	N 88-31-53 E
639+50	1891412.5137	1054495.2737	N 88-31-53 E
640+00	1891413.7953	1054545.2572	N 88-31-53 E
640+50	1891415.0768	1054595.2408	N 88-31-53 E

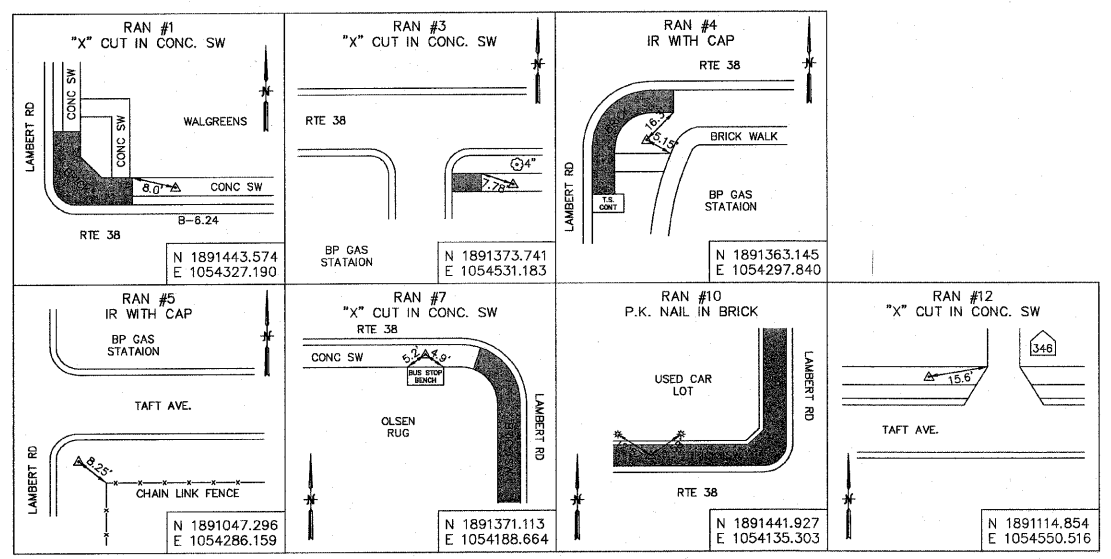
LAMBERT ROAD COORDINATES - NORTH

10+00	1891406.2604	1054251.3830	N 00-39-17 W
10+50	1891456.2572	1054250.8116	N 00-39-17 W
11+00	1891506.2539	1054250.2401	N 00-39-17 W
11+50	1891556.2506	1054249.6686	N 00-39-17 W
12+00	1891606.2474	1054249.0972	N 00-39-17 W
12+50	1891656.2441	1054248.5257	N 00-39-17 W
13+00	1891706.2408	1054247.9543	N 00-39-17 W

LAMBERT ROAD COORDINATES - SOUTH

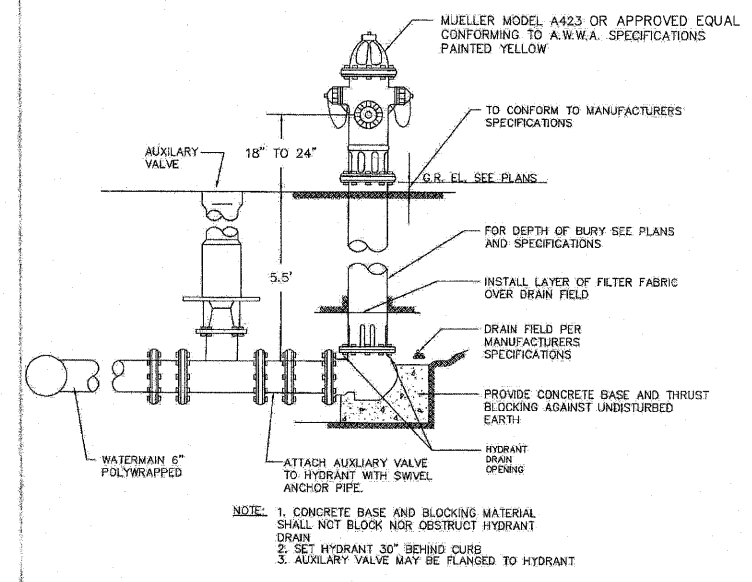
4+00	1890806.3156	1054255.0530	N 00-09-10 W
4+50	1890856.3155	1054254.9197	N 00-09-10 W
5+00	1890906.3153	1054254.7864	N 00-09-10 W
5+50	1890956.3151	1054254.6530	N 00-09-10 W
6+00	1891006.3149	1054254.5197	N 00-09-10 W
6+50	1891056.3148	1054254.3864	N 00-09-10 W
7+00	1891106.3146	1054254.2531	N 00-09-10 W
7+50	1891156.3144	1054254.1197	N 00-09-10 W
8+00	1891206.3142	1054253.9864	N 00-09-10 W
8+50	1891256.3140	1054253.8531	N 00-09-10 W
9+00	1891306.3139	1054253.7198	N 00-09-10 W
9+50	1891356.3137	1054253.5864	N 00-09-10 W
10+00	1891406.3135	1054253.4531	N 00-09-10 W

LAMBERT ROAD ALIGNMENT



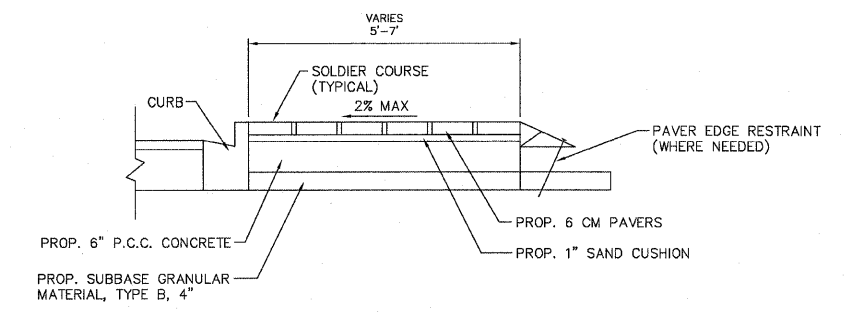
TIE POINTS

SEE PLAN SHEETS FOR BENCHMARKS

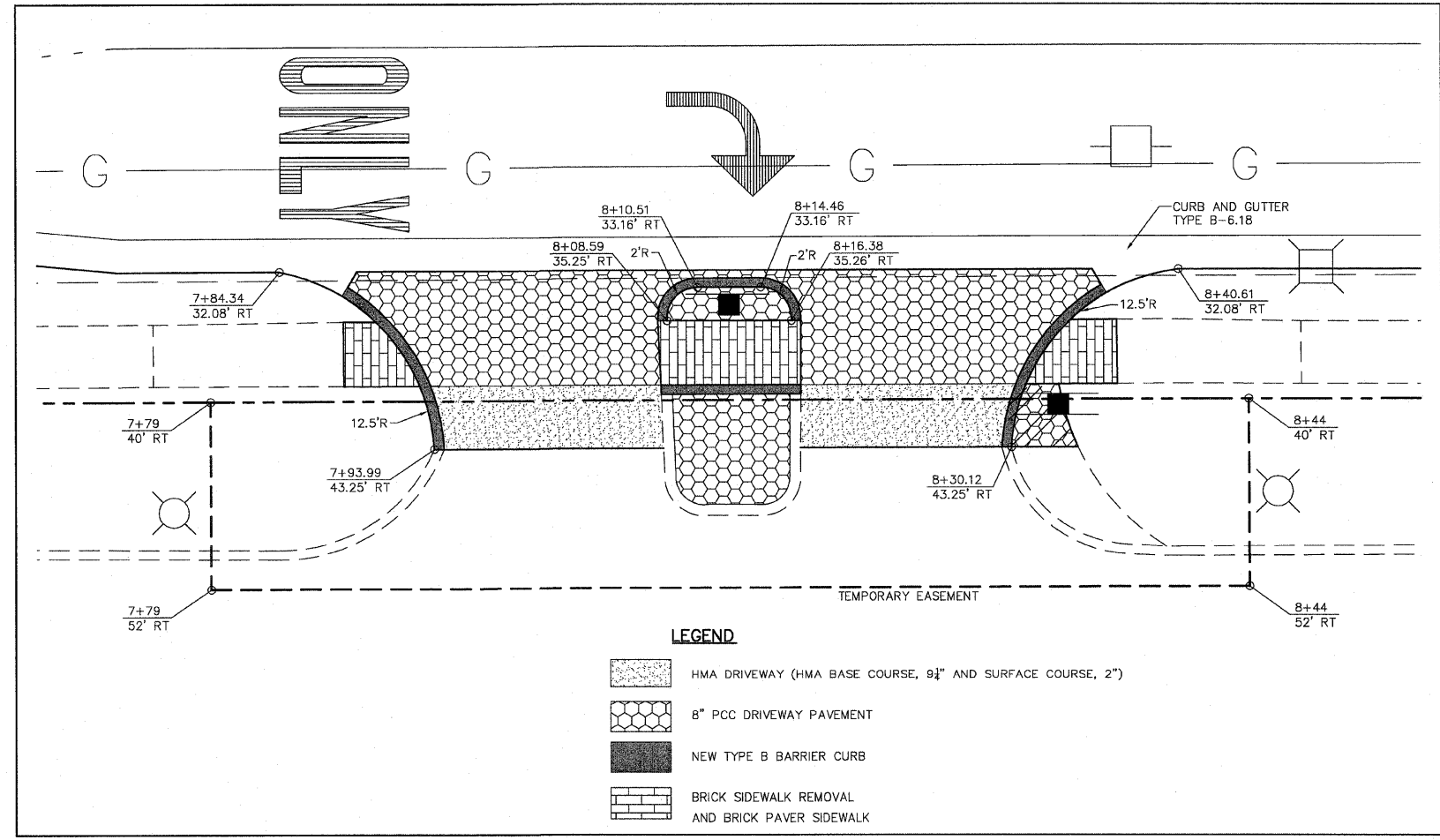


FIRE HYDRANT DETAIL
N.T.S.

- NOTE: 1. CONCRETE BASE AND BLOCKING MATERIAL SHALL NOT BLOCK NOR OBSTRUCT HYDRANT DRAIN
 2. SET HYDRANT 30" BEHIND CURB
 3. AUXILIARY VALVE MAY BE FLANGED TO HYDRANT



BRICK PAVER SIDEWALK DETAIL
NO SCALE



LEGEND

- HMA DRIVEWAY (HMA BASE COURSE, 9 1/2" AND SURFACE COURSE, 2")
- 8" PCC DRIVEWAY PAVEMENT
- NEW TYPE B BARRIER CURB
- BRICK SIDEWALK REMOVAL AND BRICK PAVER SIDEWALK

DRIVEWAY DETAIL
SCALE: 1"=5'

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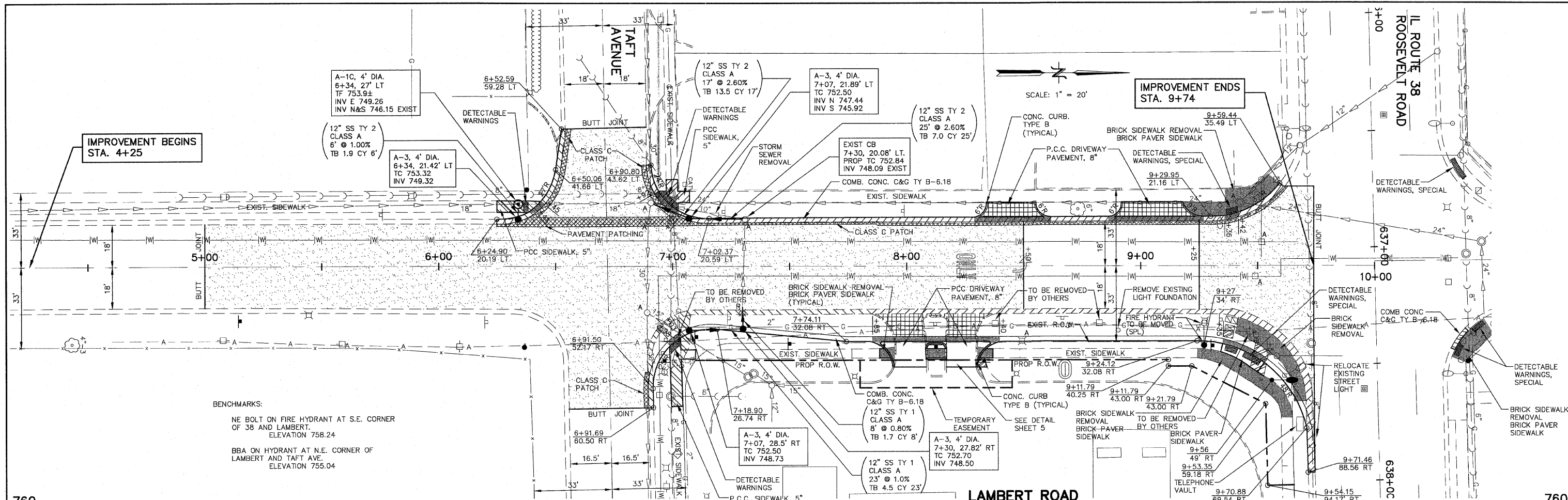
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



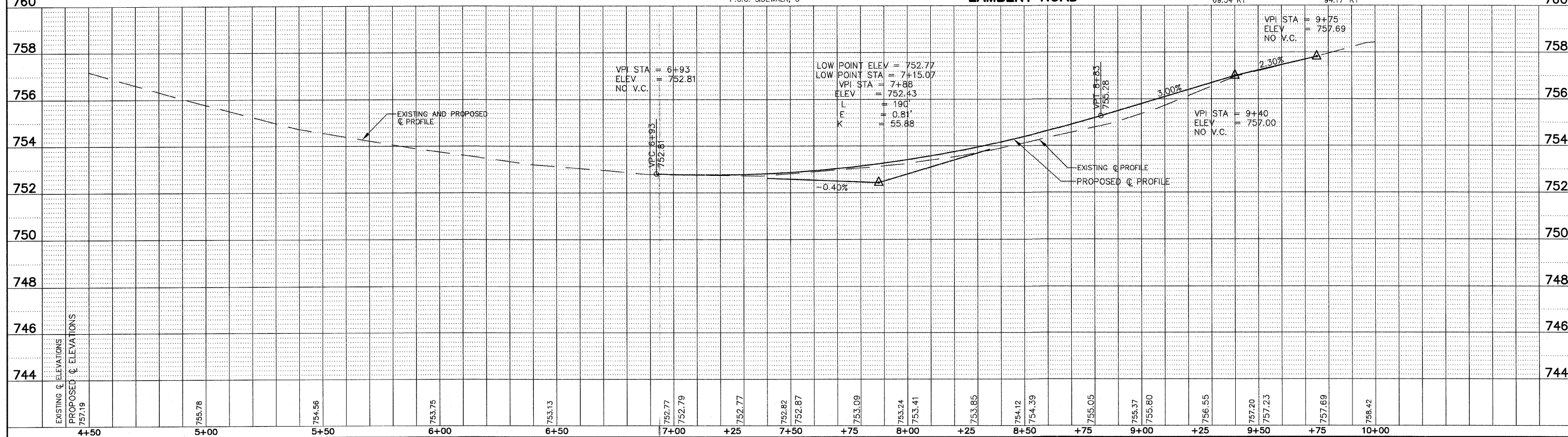
LAMBERT ROAD AT ROUTE 38
ALIGNMENT, TIES AND DETAILS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

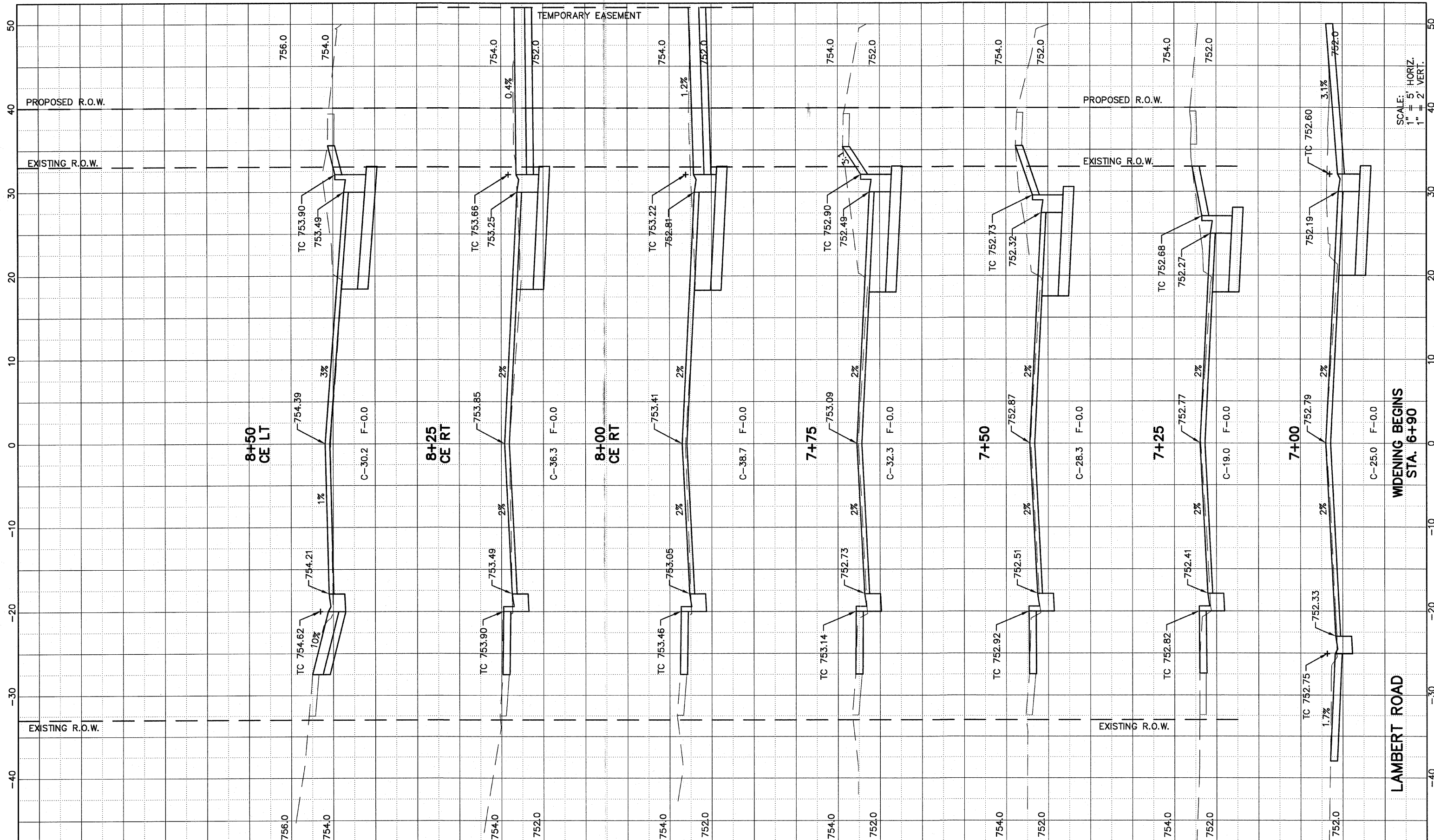
SCALE:	SHEET NO.	OF	SHEETS	STA.	TO	STA.
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BENCHMARKS:
 NE BOLT ON FIRE HYDRANT AT S.E. CORNER
 OF 38 AND LAMBERT.
 ELEVATION 758.24
 BBA ON HYDRANT AT N.E. CORNER OF
 LAMBERT AND TAFT AVE.
 ELEVATION 755.04



FILE NAME =	USER NAME =	DESIGNED =	REVISED =	STATE OF ILLINOIS		LAMBERT ROAD AT ROUTE 38		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
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PLOT SCALE =	PLOT DATE =	CHECKED =	REVISED =							FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO.	63567	



SCALE:
 1" = 5' HORIZ.
 1" = 2' VERT.

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 Layout: 7-lambert-xs+s003

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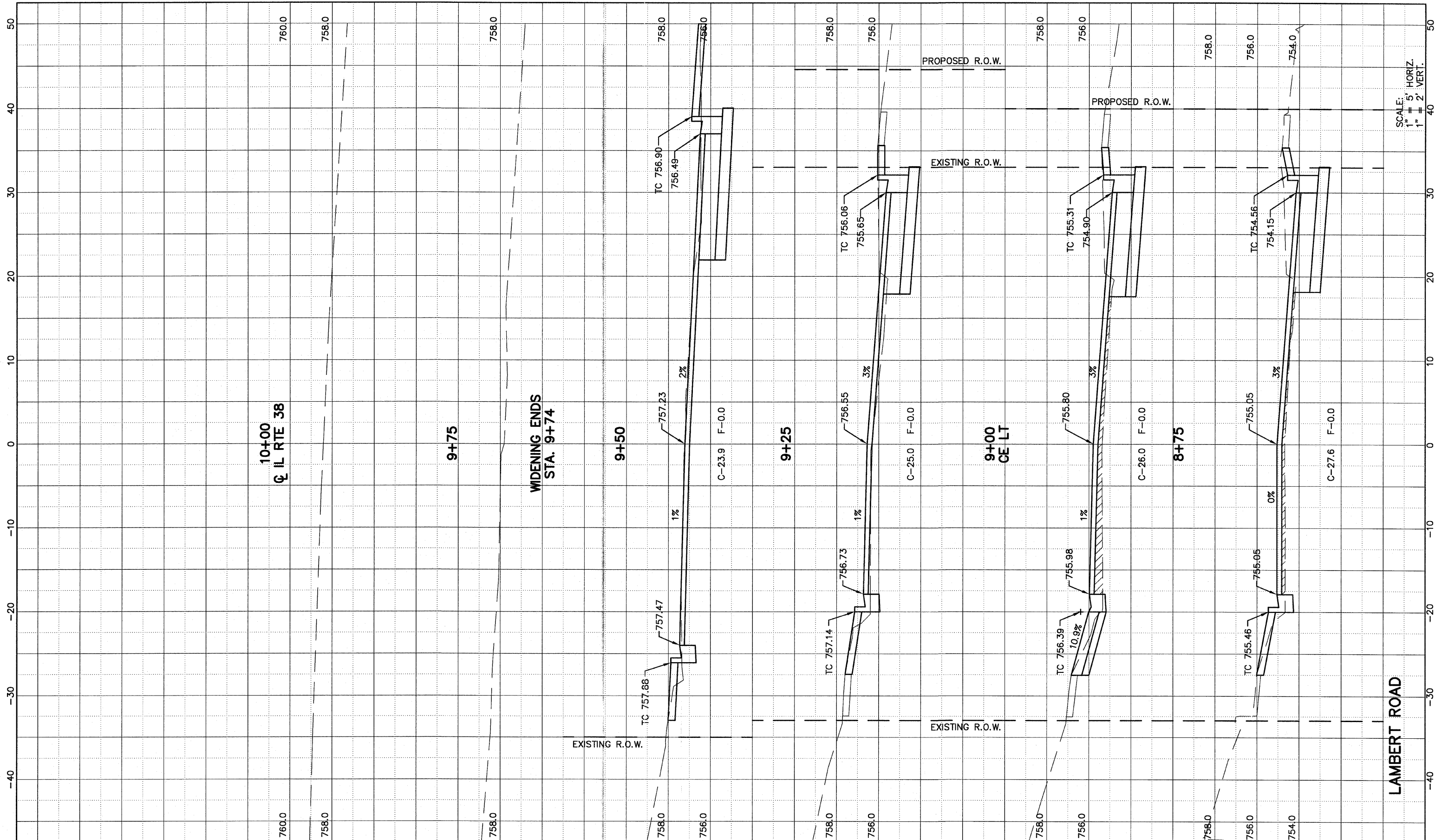
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION



LAMBERT ROAD AT IL ROUTE 38
 CROSS SECTIONS

SHEET NO. OF SHEETS STA. TO STA.

F.A.U. NO. 2567	SECTION 05-00069-00-CH	COUNTY DUPAGE	TOTAL SHEETS 42	SHEET NO. 7
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63567	



SCALE:
1" = 5' HORIZ.
1" = 2' VERT.

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Layout: 8-lambert-xx+so04

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



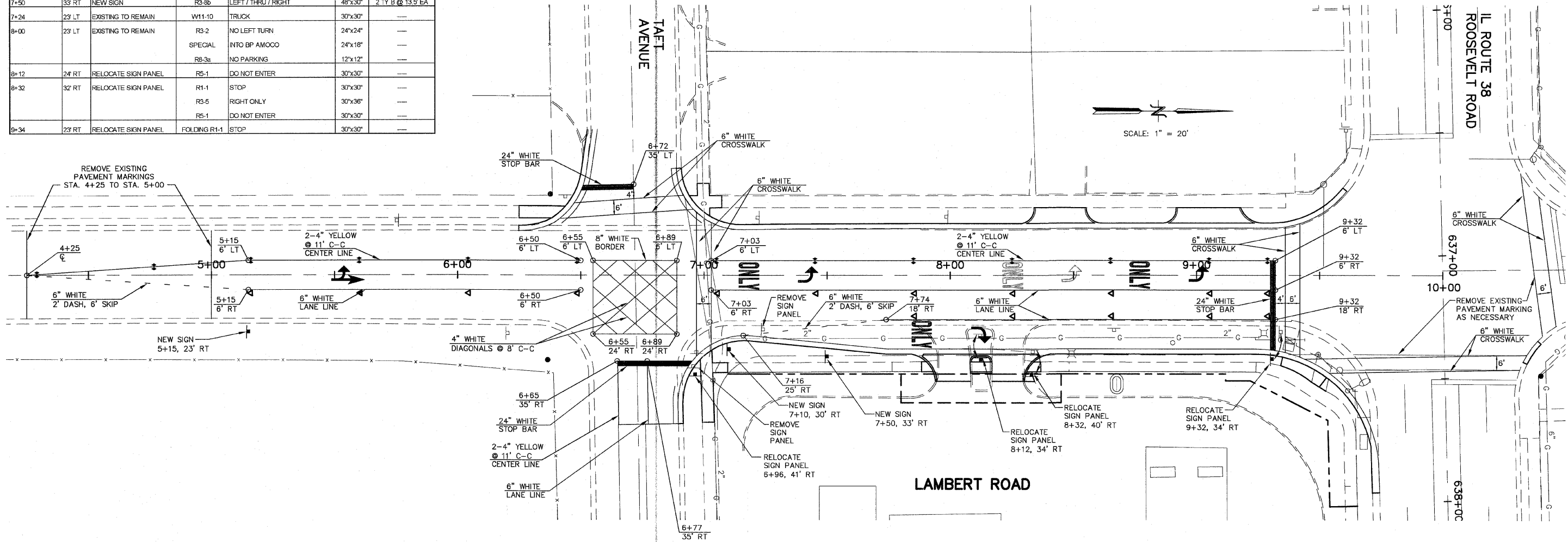
LAMBERT ROAD AT IL ROUTE 38
CROSS SECTIONS

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE. 2567	SECTION 05-00069-00-CH	COUNTY	TOTAL SHEETS 42	SHEET NO. 8
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	DUPAGE	CONTRACT NO. 63567	

SIGN TABLE

LOCATION	REF.	ACTION	SIGN TYPE	LEGEND	SIZE	POST TYPE & LENGTH
5+15	23 RT	NEW SIGNS	R3-5	LEFT ONLY	30"x36"	2 TY A @ 15.5' EA
			R3-5b	LEFT LANE	30"x12"	
			SPECIAL	FOR IL ROUTE 38	30"x12"	
5+75	22 LT	EXISTING TO REMAIN	R2-1	SPEED LIMIT 30	24"x30"	---
6+20	23 RT	EXISTING TO REMAIN	R10-7	DO NOT BLOCK INTERSECTION	30"x36"	---
6+44	32 LT	EXISTING TO REMAIN	R1-1	STOP	30"x30"	---
6+95	38 RT	REMOVE SIGN PANEL	SPECIAL	STREET NAME SIGN	---	---
6+96	32 RT	RELOCATE SIGN PANEL	R1-1	STOP	30"x30"	---
7+10	30 RT	NEW SIGN	R10-7	DO NOT BLOCK INTERSECTION	24"x30"	TY B @ 13.5'
7+24	23 RT	REMOVE SIGN PANEL	R3-8	LEFT / THRU-RIGHT	30"x30"	---
7+50	33 RT	NEW SIGN	R3-8b	LEFT / THRU / RIGHT	48"x30"	2 TY B @ 13.5' EA
7+24	23 LT	EXISTING TO REMAIN	W11-10	TRUCK	30"x30"	---
8+00	23 LT	EXISTING TO REMAIN	R3-2	NO LEFT TURN	24"x24"	---
			SPECIAL	INTO BP AMOCO	24"x18"	---
			R8-3a	NO PARKING	12"x12"	---
8+12	24 RT	RELOCATE SIGN PANEL	R5-1	DO NOT ENTER	30"x30"	---
8+32	32 RT	RELOCATE SIGN PANEL	R1-1	STOP	30"x30"	---
			R3-5	RIGHT ONLY	30"x36"	---
			R5-1	DO NOT ENTER	30"x30"	---
9+34	23 RT	RELOCATE SIGN PANEL	FOLDING R1-1	STOP	30"x30"	---



- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER
- TWO-WAY AMBER MARKER

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



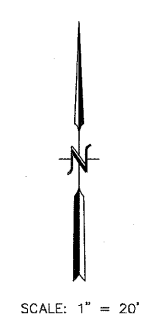
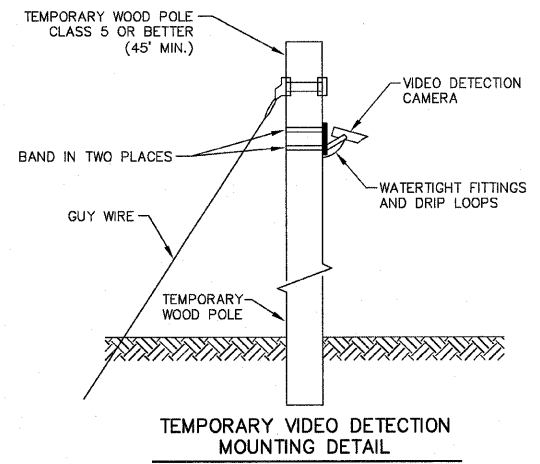
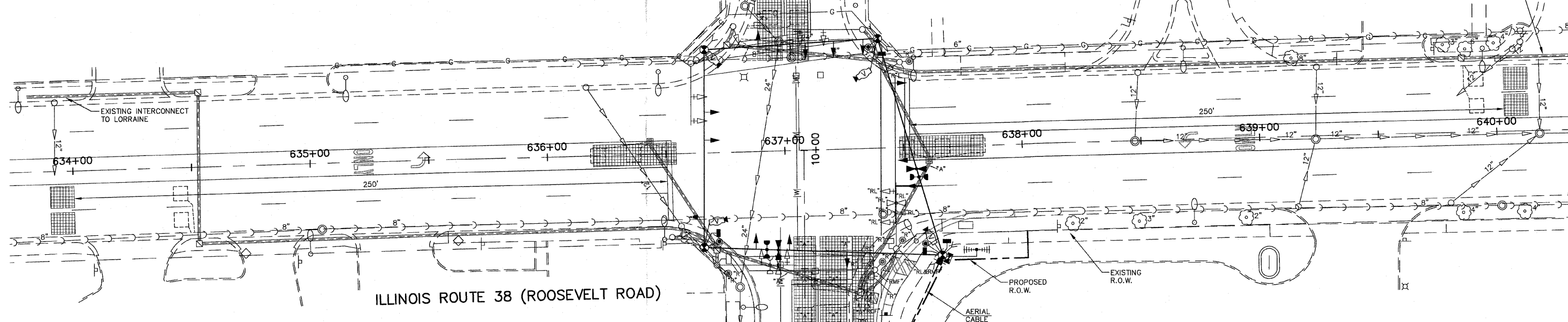
LAMBERT ROAD AT ROUTE 38
PAVEMENT MARKING PLAN

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2567	05-00069-00-CH	DuPAGE	42	9
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
CONTRACT NO.			63567	

SCALE: SHEET NO. OF SHEETS STA. TO STA.

NOTES FOR TEMPORARY TRAFFIC SIGNALS:

- ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
- ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
- ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE LED AND 12" (300mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT A RAILROAD INTERSECTION. THE CONTROLLER SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
- ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
- ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
- THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
- UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEMS SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL, TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS. TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
- DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
- WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.



CONSTRUCTION NOTES:

- RELOCATE EXISTING LIGHT DETECTOR AMPLIFIER TO THE NEW CONTROL CABINET.
- THE FOLLOWING EXISTING EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR, SHALL REMAIN THE PROPERTY OF THE STATE AND SHALL BE DELIVERED BY THE CONTRACTOR TO THE STATE'S TRAFFIC SIGNAL MAINTENANCE CONTRACTOR'S MAIN FACILITY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.
 - 1 EACH CONTROLLER AND CABINET (COMPLETE)
- THE FOLLOWING EXISTING EQUIPMENT SHALL BE CAREFULLY REMOVED AND SHALL BE SAFELY STORED BY THE CONTRACTOR FOR RELOCATION AND REINSTALLATION IN ACCORDANCE WITH THE PLANS FOR THE MODERNIZED TRAFFIC SIGNAL.
 - 1 EACH STEEL MAST ARM POLE - 26 FT (SPECIAL) WITH ORNAMENTAL BASE SHROUD
 - 1 EACH SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAM, W BACK PLATE
 - 1 EACH SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAM, W BACK PLATE
 - 1 EACH SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BM
 - 1 EACH EMERGENCY VEHICLE DETECTOR
 - 1 EACH EMERGENCY VEHICLE CONFIRMATION BEACON
- THE FOLLOWING EXISTING EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THE CONTRACTOR OUTSIDE THE RIGHT-OF-WAY AT THE CONTRACTOR'S EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.
 - 1 EACH SERVICE INSTALLATION
 - 1 EACH SIGNAL POST - 16'
 - 4 EACH PEDESTRIAN PUSH BUTTONS
 - 4 EACH PEDESTRIAN SIGNAL HEAD, LED, 1 FACE, BM
- IMMEDIATELY FOLLOWING THE ACTIVATION OF THE TEMPORARY TRAFFIC SIGNAL, THE CONTRACTOR SHALL SECURELY COVER ALL OF THE EXISTING LED TRAFFIC AND PEDESTRIAN SIGNALS NOT BEING REMOVED OR RELOCATED. THE TRAFFIC SIGNALS SHALL BE HOODED IN ACCORDANCE WITH SECTION 880.03 OF THE STANDARD SPECIFICATIONS. ALL EXISTING EQUIPMENT WHICH IS NOT REMOVED WILL BECOME A PART OF THE MODERNIZED TRAFFIC SIGNAL AND SHALL BE MAINTAINED BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPLACING ANY EQUIPMENT DAMAGED OR KNOCKED DOWN WHILE THE CONTRACTOR IS MAINTAINING THE EXISTING TRAFFIC SIGNAL INSTALLATION. THE CONTRACTOR'S MAINTENANCE RESPONSIBILITY SHALL TERMINATE WHEN THE MODERNIZED TRAFFIC SIGNAL IS ACCEPTED FOR PERPETUAL MAINTENANCE BY THE STATE.

TEMPORARY TRAFFIC SIGNAL ELECTRIC SERVICE INFORMATION:

AT THE TIME THE PLANS AND SPECIFICATIONS FOR THIS PROJECT WERE PREPARED THE EXISTING TRAFFIC SIGNAL SERVICE WAS PROVIDED BY AN OVERHEAD COMED LINE LOCATED ON THE EAST SIDE OF LAMBERT ROAD IN THE PARKWAY SOUTH OF IL 38. COMED PLANS TO REMOVE THEIR OVERHEAD LINES AND REPLACE THEM WITH UNDERGROUND LINES. THE RELOCATED UNDERGROUND LINE WILL BE LOCATED EAST OF THE PROPOSED LAMBERT RD WIDENING. THE CONTRACTOR SHALL VERIFY THE EXACT LOCATION OF THE UNDERGROUND SERVICE PRIOR TO THE START OF CONSTRUCTION. THE LENGTH OF THE AERIAL CABLE SHOWN ON THE PLAN IS APPROXIMATE. THE LENGTH OF THE AERIAL SPAN AND THE NUMBER OF WOOD POLES REQUIRED FOR THE SPAN WILL BE ADJUSTED BASED ON THE ACTUAL LOCATION OF THE COMED TRANSFORMER PAD. THIS WORK MUST BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION OF THE SPAN WIRE SIGNALS TO ENSURE THE OVERHEAD CABLE DOES NOT INFRINGE INTO THE AIR SPACE ABOVE THE ADJACENT PRIVATE PROPERTY. THE LENGTH OF THE TEMPORARY AERIAL SPAN AND THE NUMBER OF WOOD POLES USED FOR THE TEMPORARY SERVICE WIRE SPAN IS INCIDENTAL TO THE BID PRICE FOR "TEMPORARY TRAFFIC SIGNAL INSTALLATION".

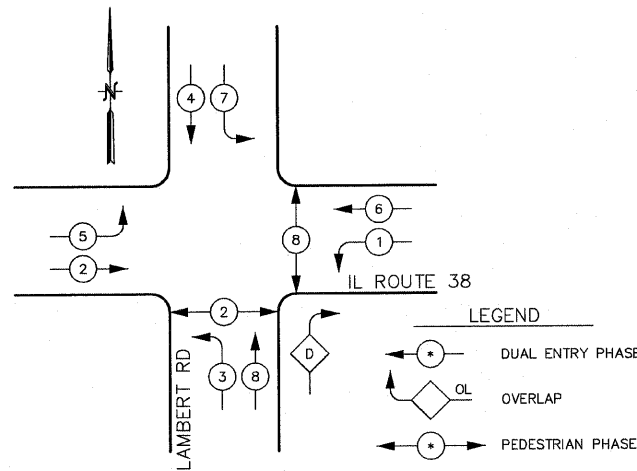
THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING SYSTEM.

RESTORATION OF WORK AREA

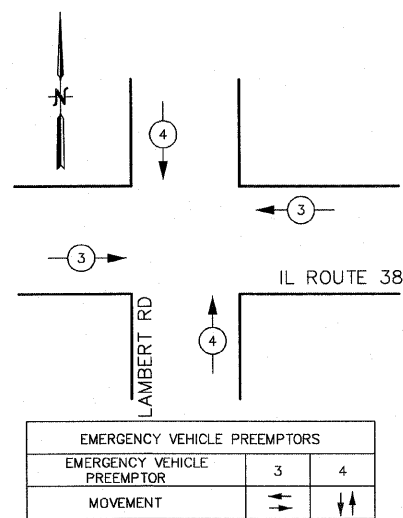
RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

FILE NAME =	USER NAME =	DESIGNED -- JAS	REVISED --	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		TEMPORARY TRAFFIC SIGNAL PLAN ILLINOIS ROUTE 38 AND LAMBERT ROAD	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
P:\03\03040036\cad\phase 2\dwg\040036-sht-temp_traffic_signals.dwg		DRAWN -- SMS	REVISED --				2567	05-00069-00-CH	ILLINOIS	38	10
Aug 29, 2011 at 8:57		CHECKED -- DAY	REVISED --				CONTRACT NO. 63567				
Layout: 10-Existing Traffic Signal Plan		DATE -- 6/29/09	REVISED --				ILLINOIS FED. AID PROJECT				
							SCALE: 1"=20'	SHEET NO. OF SHEETS STA. TO STA.	FED. ROAD DIST. NO.		

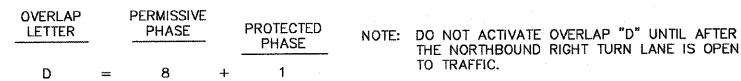
**TEMPORARY CONTROLLER SEQUENCE
PHASE DESIGNATION DIAGRAM**



**TEMPORARY EMERGENCY VEHICLE
PREEMPTION SEQUENCE**



RIGHT TURN OVERLAP PHASE DESIGNATION



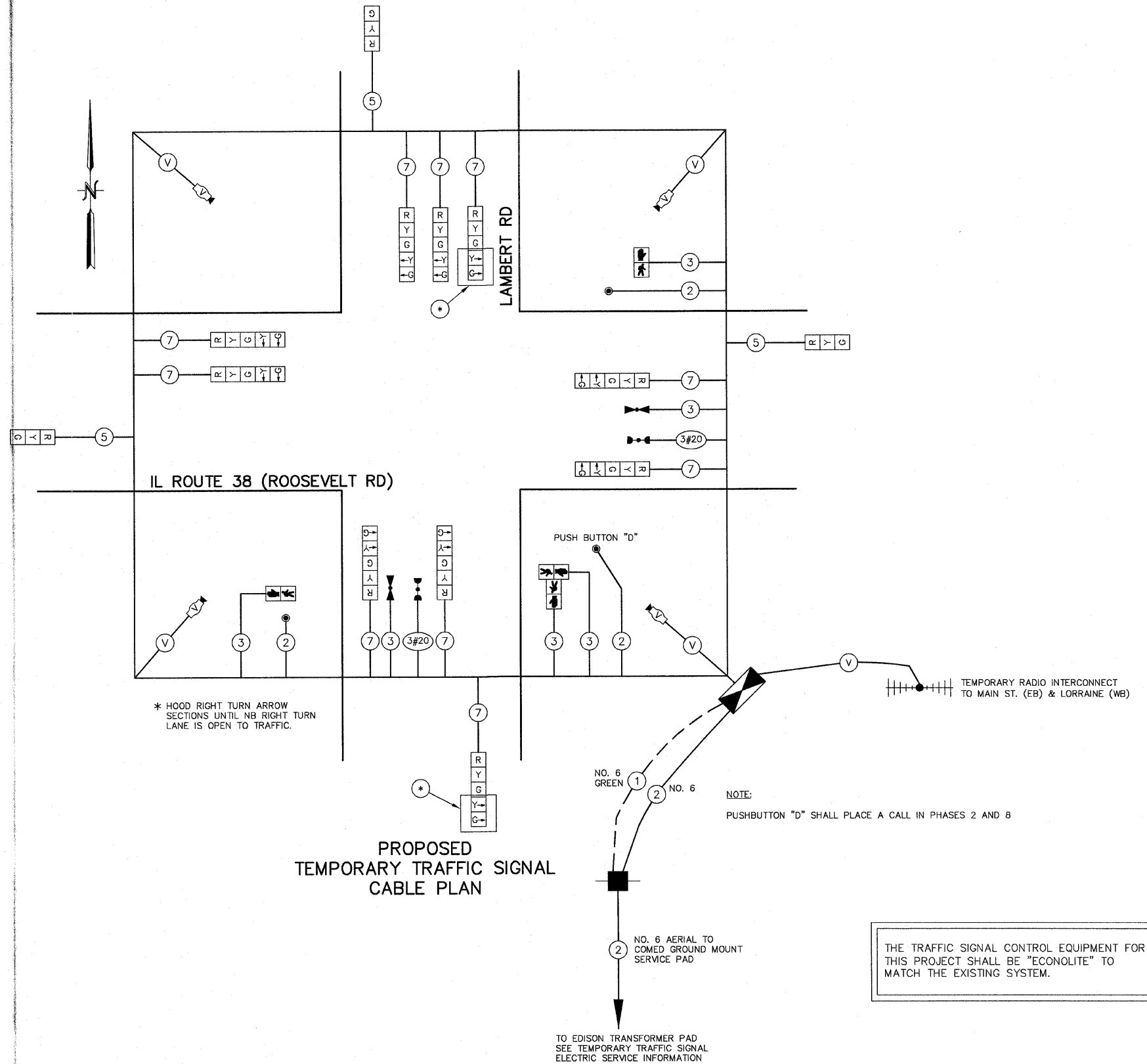
TEMPORARY TRAFFIC SIGNAL NOTES

- ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
- ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
- ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE LED AND 12" (300mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT A RAILROAD INTERSECTION. THE CONTROLLER SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
- ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
- ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
- THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
- UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEMS SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL, TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
- TRAFFIC SIGNALMANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
- DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
- WHEN PAN, TILT ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. OF LAMPS	WATTAGE		x % OPERATION	
		INCAND.	LED		
SIGNAL (RED)	13		8	0.65	67.60
(YELLOW)	13		19	0.04	9.88
(GREEN)	13		11	0.45	64.35
ARROW (RED)	0		5.5	0.00	0.00
(YELLOW)	10		9	0.03	2.70
(GREEN)	10		5.5	0.15	8.25
PED. SIGNAL	4		7	1.00	28.00
VIDEO SYSTEM	1	150		1.00	150.00
UPS SYSTEM	0		50	1.00	0.00
CONTROLLER	1		100	1.00	100.00
STREET LIGHT	0		400	1.00	0.00
					430.78

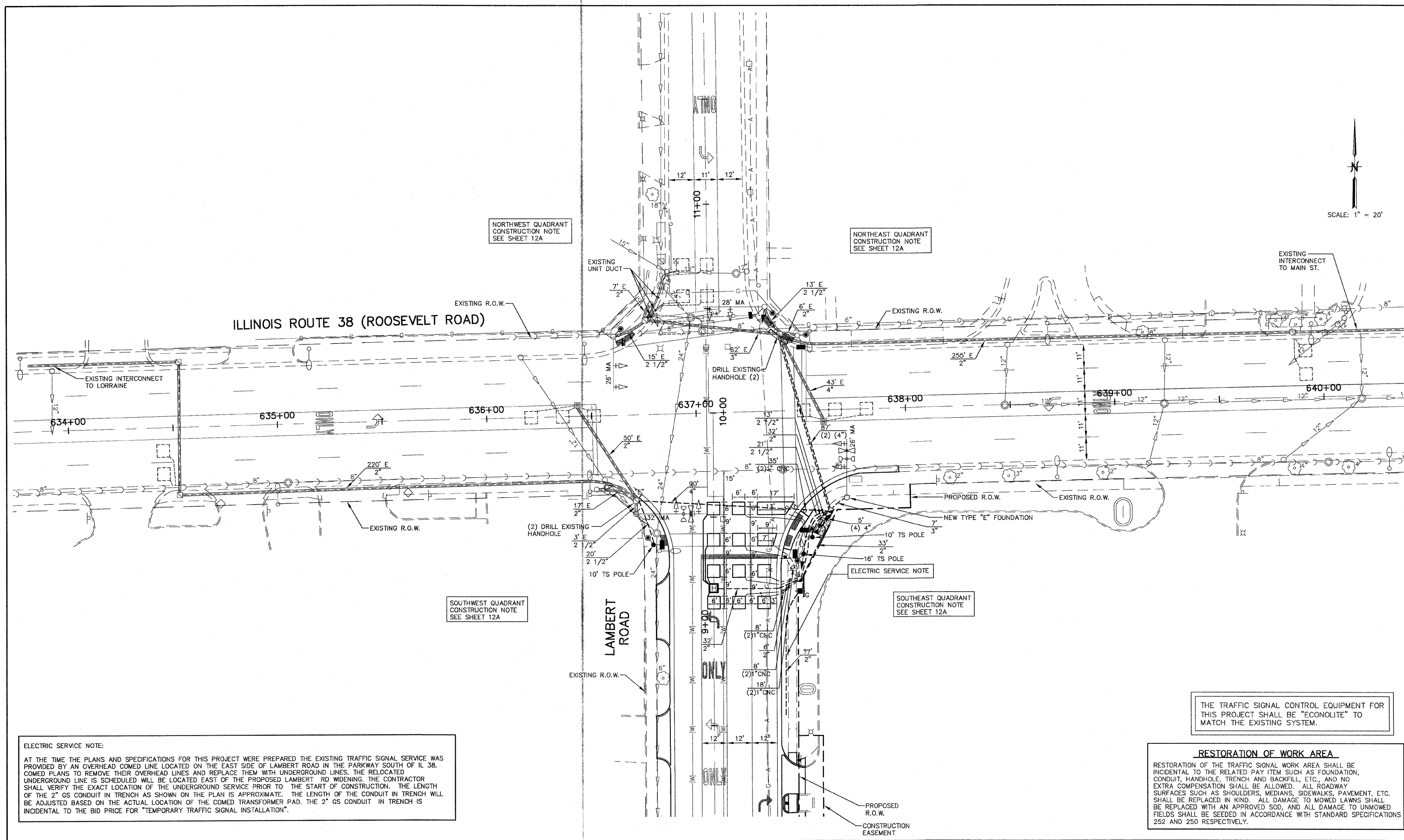
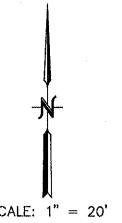
ENERGY COSTS TO: VILLAGE OF GLEN ELLEN

ENERGY SUPPLY: CONTACT: JOE STACHO
PHONE: (630)424-5704
COMPANY: COMED



**PROPOSED
TEMPORARY TRAFFIC SIGNAL
CABLE PLAN**

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING SYSTEM.



NORTHWEST QUADRANT
CONSTRUCTION NOTE
SEE SHEET 12A

NORTHEAST QUADRANT
CONSTRUCTION NOTE
SEE SHEET 12A

SOUTHWEST QUADRANT
CONSTRUCTION NOTE
SEE SHEET 12A

SOUTHEAST QUADRANT
CONSTRUCTION NOTE
SEE SHEET 12A

ELECTRIC SERVICE NOTE:
AT THE TIME THE PLANS AND SPECIFICATIONS FOR THIS PROJECT WERE PREPARED THE EXISTING TRAFFIC SIGNAL SERVICE WAS PROVIDED BY AN OVERHEAD COMED LINE LOCATED ON THE EAST SIDE OF LAMBERT ROAD IN THE PARKWAY SOUTH OF IL 38. COMED PLANS TO REMOVE THEIR OVERHEAD LINES AND REPLACE THEM WITH UNDERGROUND LINES. THE RELOCATED UNDERGROUND LINE IS SCHEDULED WILL BE LOCATED EAST OF THE PROPOSED LAMBERT RD WIDENING. THE CONTRACTOR SHALL VERIFY THE EXACT LOCATION OF THE UNDERGROUND SERVICE PRIOR TO THE START OF CONSTRUCTION. THE LENGTH OF THE 2" GS CONDUIT IN TRENCH AS SHOWN ON THE PLAN IS APPROXIMATE. THE LENGTH OF THE CONDUIT IN TRENCH WILL BE ADJUSTED BASED ON THE ACTUAL LOCATION OF THE COMED TRANSFORMER PAD. THE 2" GS CONDUIT IN TRENCH IS INCIDENTAL TO THE BID PRICE FOR "TEMPORARY TRAFFIC SIGNAL INSTALLATION".

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING SYSTEM.

RESTORATION OF WORK AREA
RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

FILE NAME = P:\03\03040036\cod\phase 2\dwg\040036-ant-traffic_signals.dwg Feb 01, 2012 at 11:46 Layout: 12-FRNP Traffic Signal Plan	USER NAME =	DESIGNED — JAS	REVISED — HLR 8/16/11
		DRAWN — SMS	REVISED —
	PLOT SCALE =	CHECKED — DAY	REVISED —
	PLOT DATE =	DATE — 6/29/09	REVISED —

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**



**TRAFFIC SIGNAL MODERNIZATION PLAN
ILLINOIS ROUTE 38 AND LAMBERT ROAD**

F.A.U. RTE. 2567	SECTION 05-00069-00-CH	COUNTY DuPAGE	TOTAL SHEETS 38	SHEET NO. 12
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63567	

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

PROPOSED TRAFFIC SIGNAL "SCHEDULE OF WORK" FOR THE NORTHWEST QUADRANT:

- INSTALL AND CABLE ONE (1), NEW, 1 FACE-1 SECTION LED BRACKET MOUNT PEDESTRIAN SIGNAL WITH COUNTDOWN TIMER ONTO THE EXISTING MAST ARM POLE (NEW CABLE: 3/C NO.14).
- INSTALL AND CABLE ONE (1), NEW, PEDESTRIAN PUSH BUTTON ONTO THE EXISTING MAST ARM POLE (NEW CABLE: 2/C NO.14).
- REMOVE EXISTING CABLE AND INSTALL NEW CABLE TO THE EXISTING 5-SECTION LED MAST ARM MOUNT TRAFFIC SIGNAL WITH AN EXISTING BACKPLATE ON THE EXISTING MAST ARM (NEW CABLE: 7/C NO.14).
- REMOVE EXISTING CABLE AND INSTALL NEW CABLE TO THE EXISTING 3-SECTION LED MAST ARM MOUNT TRAFFIC SIGNAL WITH AN EXISTING BACKPLATE ON THE EXISTING MAST ARM (NEW CABLE: 5/C NO.14).
- REMOVE EXISTING CABLE AND INSTALL NEW CABLE TO THE EXISTING 5-SECTION LED BRACKET MOUNT TRAFFIC SIGNAL ON THE EXISTING 16' TRAFFIC SIGNAL POLE (NEW CABLE: 7/C NO.14).
- REMOVE EXISTING CABLE, INSTALL NEW CABLE AND SPLICE THE EXISTING SOUTHBOUND LOOP DETECTORS IN THE EXISTING HANDHOLE, USE ONE PAIR OF LEAD-IN CABLE FOR THE LOOP DETECTORS IN THE LEFT TURN LANE AND USE ONE PAIR OF LEAD-IN CABLE FOR THE LOOP DETECTORS IN THE THRU/RIGHT LANE (NEW CABLES: TWO (2) 2/C NO.4 (TWISTED AND SHIELDED))
- INSTALL NEW 1/C NO.6 GREEN GROUND WIRES IN ACCORDANCE WITH DISTRICT ONE TRAFFIC SIGNAL STANDARD DRAWING.

PROPOSED TRAFFIC SIGNAL "SCHEDULE OF WORK" FOR THE NORTHEAST QUADRANT:

- DRILL EXISTING HANDHOLE (2) FOR NEW 4" CONDUITS
- INSTALL AND CABLE ONE (1), NEW, 1 FACE - 1 SECTION LED BRACKET MOUNT PEDESTRIAN SIGNAL WITH COUNT DOWN TIMER ONTO THE EXISTING MAST ARM POLE (NEW CABLE: 3/C NO.14).
- INSTALL AND CABLE ONE (1), NEW, PEDESTRIAN PUSH BUTTON ONTO THE EXISTING MAST ARM POLE (NEW CABLE: 2/C NO.14).
- INSTALL AND CABLE ONE (1), NEW, 5-SECTION LED BRACKET MOUNT TRAFFIC SIGNAL ONTO THE EXISTING MAST ARM POLE (NEW CABLE: 7/C NO.14).
- REMOVE EXISTING CABLE AND INSTALL NEW CABLE TO THE EXISTING 5-SECTION LED MAST ARM MOUNT TRAFFIC SIGNAL WITH EXISTING BACKPLATE ON THE EXISTING MAST ARM (NEW CABLE: 7/C NO.14).
- REMOVE EXISTING CABLE AND INSTALL NEW CABLE TO THE EXISTING 3-SECTION LED MAST ARM MOUNT TRAFFIC SIGNAL WITH EXISTING BACKPLATE ON THE EXISTING MAST ARM (NEW CABLE: 5/C NO.14).
- REMOVE EXISTING CABLE AND INSTALL NEW CABLE TO ONE (1), NEW, PEDESTRIAN PUSH BUTTON ONTO THE EXISTING 16' TRAFFIC SIGNAL POLE (NEW CABLE: 2/C NO.14).
- REMOVE EXISTING CABLE AND INSTALL NEW CABLE TO THE EXISTING 5-SECTION LED BRACKET MOUNT TRAFFIC SIGNAL ON THE EXISTING 16' TRAFFIC SIGNAL POLE (NEW CABLE: 7/C NO.14).
- REMOVE EXISTING CABLE AND INSTALL NEW CABLE TO A NEW 1 FACE-1 SECTION LED BRACKET MOUNT PEDESTRIAN SIGNAL WITH COUNT DOWN TIMER ON THE EXISTING 16' TRAFFIC SIGNAL POLE (NEW CABLE: 3/C NO.14).
- REMOVE EXISTING CABLE, INSTALL NEW CABLE AND SPLICE THE TWO EXISTING WESTBOUND LOOP DETECTORS IN THE EXISTING HANDHOLE AT STATION 639+85, USE ONE LEAD-IN CABLE FOR BOTH LOOP DETECTORS (NEW CABLE: 2/C NO.14 (TWISTED AND SHIELDED)).
- REMOVE EXISTING CABLE, INSTALL NEW CABLE, AND SPLICE THE THREE EXISTING WESTBOUND LEFT TURN LOOP DETECTORS IN THE EXISTING HEAVY DUTY HANDHOLE AT STATION 637+61, USE ONE LEAD-IN CABLE FOR THREE LOOP DETECTORS (NEW CABLE: 2/C NO.14 (TWISTED AND SHIELDED)).
- INSTALL NEW 1/C NO.6 GREEN GROUND WIRES IN ACCORDANCE WITH DISTRICT ONE TRAFFIC SIGNAL STANDARD DRAWING.

PROPOSED TRAFFIC SIGNAL "SCHEDULE OF WORK" FOR THE SOUTHWEST QUADRANT:

- DRILL EXISTING HANDHOLE (2) FOR NEW 2½" AND 4" CONDUITS
- CONSTRUCT ONE (1) TYPE A FOUNDATION, DRILL EXISTING HANDHOLE (2) AND INSTALL NEW 2½" CONDUIT AS SHOWN ON THE PLAN.
- INSTALL AND CABLE ONE (1), NEW 1 FACE 1-SECTION LED BRACKET MOUNT PEDESTRIAN SIGNAL WITH COUNT DOWN TIMER ONTO THE NEW 10' TRAFFIC SIGNAL POLE - POWDER COATED (NEW CABLE: 3/C NO.14).
- INSTALL AND CABLE ONE (1), NEW, PEDESTRIAN PUSH BUTTON ONTO THE NEW 10' TRAFFIC SIGNAL POLE (SPECIAL) (NEW CABLE: 2/C NO.14).
- REMOVE EXISTING CABLE AND INSTALL NEW CABLE TO THE EXISTING 5-SECTION LED MAST ARM MOUNT TRAFFIC SIGNAL WITH EXISTING BACKPLATE ON THE EXISTING MAST ARM (NEW CABLE: 7/C NO.14).
- REMOVE EXISTING CABLE AND INSTALL NEW CABLE TO THE EXISTING 3-SECTION LED MAST ARM MOUNT TRAFFIC SIGNAL WITH EXISTING BACKPLATE ON THE EXISTING MAST ARM NEW CABLE: 5/C NO.14).
- REMOVE EXISTING CABLE AND INSTALL NEW CABLE TO THE EXISTING EMERGENCY VEHICLE PREEMPTION LIGHT DETECTOR ON THE EXISTING MAST ARM (NEW CABLE: 3/C NO.20 (TWISTED AND SHIELDED)).
- REMOVE EXISTING CABLE AND INSTALL NEW CABLE TO THE EXISTING EMERGENCY VEHICLE PREEMPTION CONFIRMATION BEACON ON THE EXISTING MAST ARM (NEW CABLE: 3/C NO.14).
- REMOVE EXISTING CABLE AND INSTALL NEW CABLE TO THE EXISTING 5-SECTION LED BRACKET MOUNT TRAFFIC SIGNAL ON THE EXISTING 16' TRAFFIC SIGNAL POLE (NEW CABLE: 7/C NO.14).
- REMOVE EXISTING CABLE, INSTALL NEW CABLE AND SPLICE THE TWO EXISTING EASTBOUND LOOP DETECTORS IN THE EXISTING HANDHOLE AT STATION 634+51, USE ONE LEAD-IN CABLE FOR BOTH LOOP DETECTORS (NEW CABLE: 2/C NO.14 (TWISTED AND SHIELDED)).
- REMOVE EXISTING CABLE, INSTALL NEW CABLE AND SPLICE THE THREE EXISTING EASTBOUND LEFT TURN LOOP DETECTORS IN THE EXISTING HEAVY DUTY HANDHOLE AT STATION 636+41, USE ONE LEAD-IN CABLE FOR THREE LOOP DETECTORS (NEW CABLE: 2/C NO.14 (TWISTED AND SHIELDED)).
- INSTALL NEW 1/C NO.6 GREEN GROUND WIRES IN ACCORDANCE WITH DISTRICT ONE TRAFFIC SIGNAL STANDARD DRAWING.

PROPOSED TRAFFIC SIGNAL "SCHEDULE OF WORK" FOR THE SOUTHEAST QUADRANT:

- CONSTRUCT TWO (2) NEW TYPE "A" FOUNDATIONS, ONE (1) TYPE "C" FOUNDATION WITH A CONCRET PAD BENEATH THE UPS CABINET, ONE (1) TYPE "E" FOUNDATION, ONE (1) DOUBLE HANDHOLE, ONE (1) HEAVY DUTY HANDHOLE, ONE (1) STANDARD DUTY HANDHOLE, NEW CONDUITS, NEW TRAFFIC CONTROL CABINET, NEW LOOP DETECTORS, LOOP DETECTOR LEAD-INS AND ONE (1) GROUND MOUNT SERVICE INSTALLATION AS DETAILED.
- RELOCATE AND INSTALL THE EXISTING STEEL MAST ARM POLE WITH 26' MAST ARM (SPECIAL) WITH EXISTING DECORATIVE SHROUD AND CONCEALING METAL SKIRT ONTO THE NEW TYPE "E" FOUNDATION.
- RELOCATE, INSTALL AND CABLE THE EXISTING 5-SECTION LED MAST ARM MOUNT TRAFFIC SIGNAL WITH EXISTING BACKPLATE ON THE RELOCATED MAST ARM (NEW CABLE: 7/C NO.14).
- RELOCATE, INSTALL AND CABLE THE EXISTING 3-SECTION LED MAST ARM MOUNT TRAFFIC SIGNAL WITH EXISTING BACKPLATE BACK ONTO THE RELOCATED MAST ARM, (NEW CABLE: 5/C NO.14)
- RELOCATE, INSTALL AND CABLE THE EXISTING EMERGENCY VEHICLE PREEMPTION LIGHT DETECTOR BACK ONTO THE RELOCATED MAST ARM, (NEW CABLE: 3/C NO.20 (TWISTED AND SHIELDED)).
- RELOCATE, INSTALL AND CABLE THE EMERGENCY VEHICLE PREEMPTION CONFIRMATION BEACON BACK ONTO THE RELOCATED MAST ARM, (NEW CABLE: 3/C NO.14).
- INSTALL AND CABLE ONE (1), NEW 1 FACE - 1 SECTION LED BRACKET MOUNT PEDESTRIAN SIGNAL WITH COUNT DOWN TIMER ONTO THE NEW 10' TRAFFIC SIGNAL POLE-POWDER COATED (NEW CABLE: 3/C NO.14).
- INSTALL AND CABLE ONE (1), NEW, PEDESTRIAN PUSH BUTTON ONTO THE NEW 10' TRAFFIC SIGNAL POLE-POWDER COATED (NEW CABLE: 2/C NO.14).
- INSTALL AND CABLE ONE (1), NEW, 1 FACE - 2-SECTION LED BRACKET MOUNT PEDESTRIAN SIGNAL WITH COUNT DOWN TIMER ONTO THE NEW 16' TRAFFIC SIGNAL POLE-POWDER COATED (NEW CABLE: 3/C NO.14).
- INSTALL AND CABLE ONE (1), NEW, PEDESTRIAN PUSH BUTTON ONTO THE NEW 16' TRAFFIC SIGNAL POLE (SPECIAL) (NEW CABLE: 2/C NO.14).
- RELOCATE, INSTALL AND CABLE ONE (1), EXISTING 5-SECTION LED BRACKET MOUNT TRAFFIC SIGNAL FROM THE REMOVED 16' TRAFFIC SIGNAL POLE AND INSTALL AND CABLE ONE (1) NEW 5-SECTION LED BRACKET MOUNT SIGNAL ONTO THE SAME NEW 16' TRAFFIC SIGNAL POLE -POWDER COATED (NEW CABLE: TWO (2) 7/C NO.14).
- INSTALL NEW 1/C NO.6 GREEN GROUND WIRES IN ACCORDANCE WITH DISTRICT ONE TRAFFIC SIGNAL STANDARD DRAWING.

FILE NAME =
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 Aug 29, 2011 at 9:58
 Layout: 12a-Construction Notes

USER NAME =	DESIGNED -- JAS	REVISED --
PLOT SCALE =	DRAWN -- PBF	REVISED --
PLOT DATE =	CHECKED -- JAS	REVISED --
	DATE -- 8/16/11	REVISED --

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

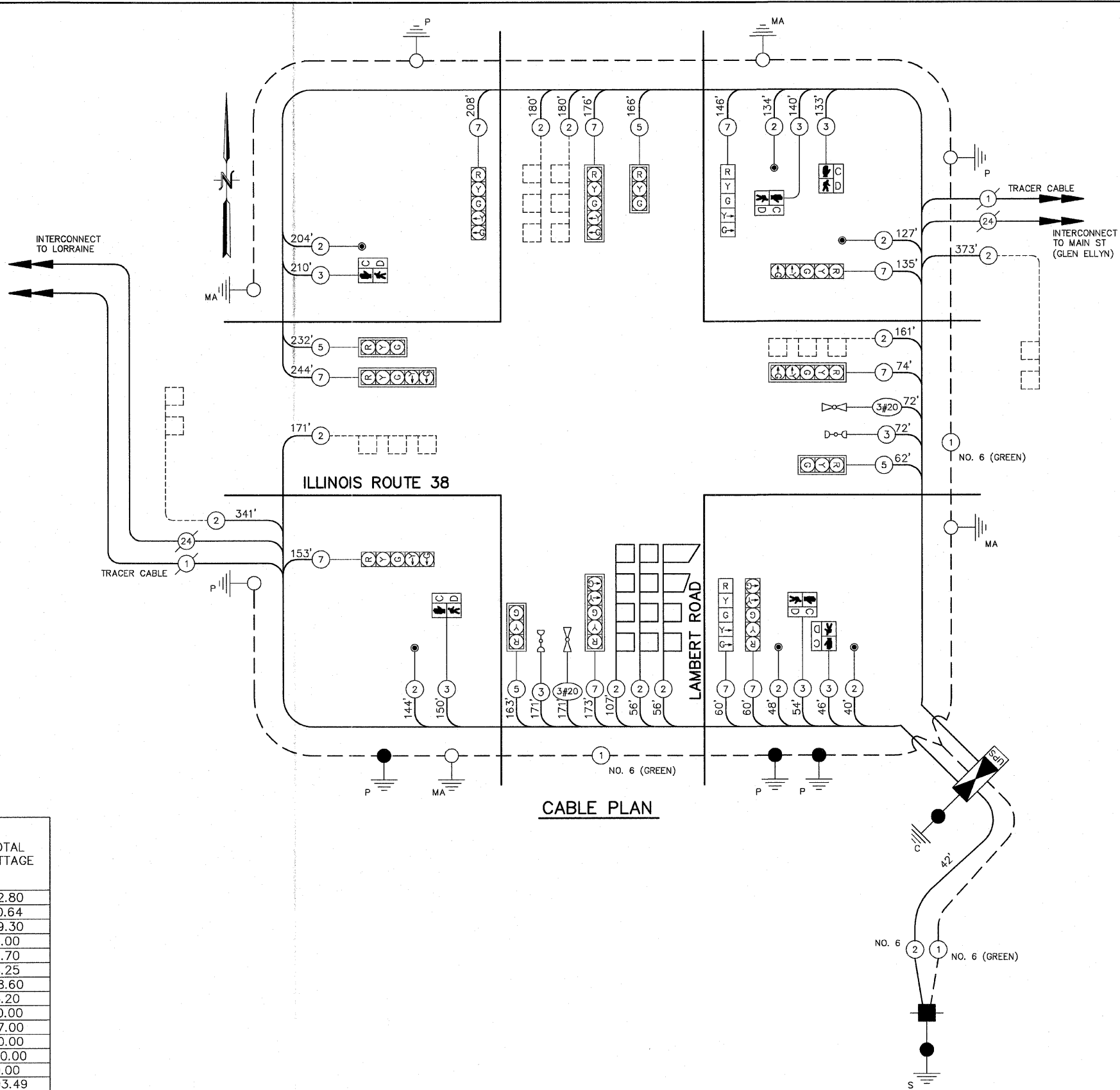
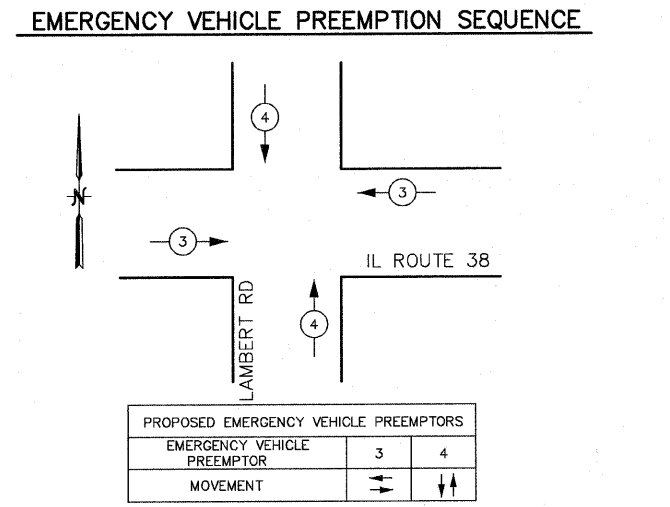
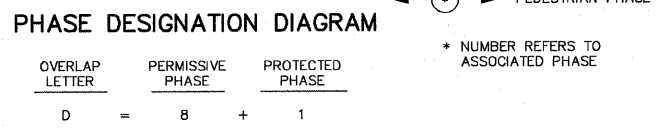
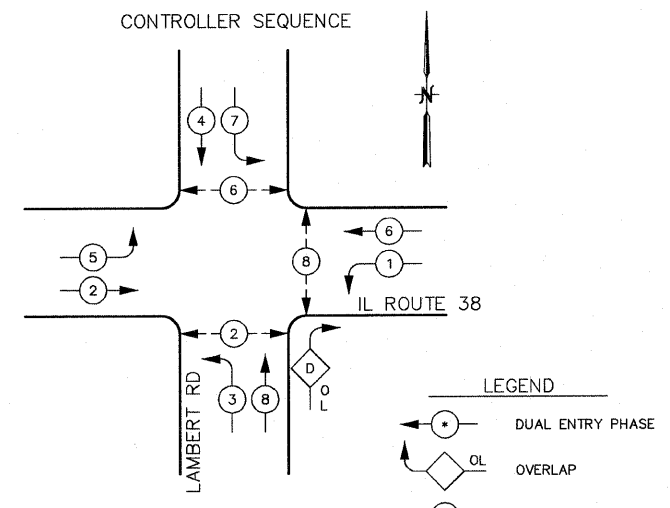


TRAFFIC SIGNAL CONSTRUCTION NOTES
 ILLINOIS ROUTE 38 AND LAMBERT ROAD

SCALE: NONE

SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2567	05-00069-00-CH	DuPAGE	38	12A
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 63567	



I.D.O.T.
TRAFFIC SIGNAL INSTALLATION
ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	WATTAGE		x % OPERATION	TOTAL WATTAGE
		x INCAND.	LED		
SIGNAL (RED)	14		8	0.65	72.80
(YELLOW)	14		19	0.04	10.64
(GREEN)	14		11	0.45	69.30
ARROW (RED)	0		5.5	0.00	0.00
(YELLOW)	10		9	0.03	2.70
(GREEN)	10		5.5	0.15	8.25
HAND LED	6		9	0.90	48.60
PERSON LED	6		7	0.10	4.20
COUNTDOWN LED	2		5	0.10	10.00
LOOP DETECTION	9		3	1.00	27.00
UPS SYSTEM	1		50	1.00	50.00
CONTROLLER	1		100	1.00	100.00
STREET LIGHT	0		400	1.00	0.00
					403.49

ENERGY COSTS TO: VILLAGE OF GLEN ELLEN

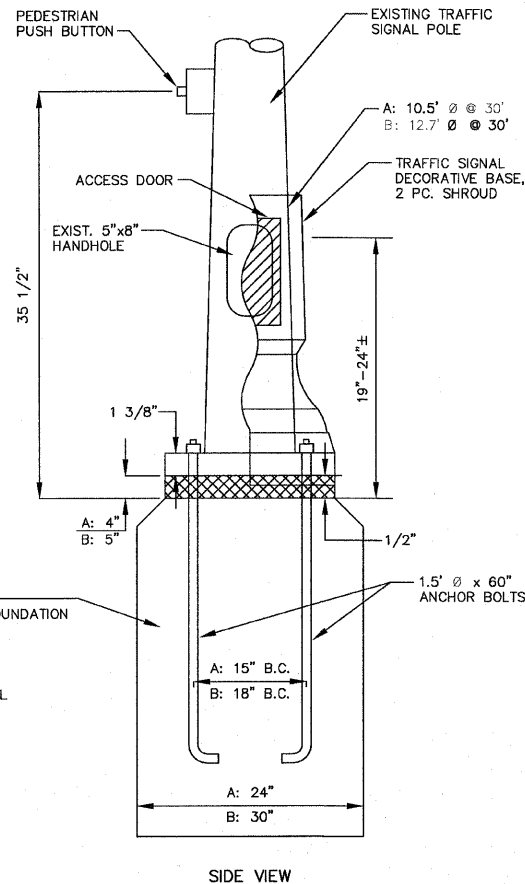
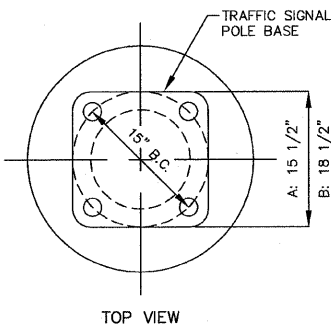
ENERGY SUPPLY: CONTACT: JOE STACHO
PHONE: (630)424-5704
COMPANY: COMED

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING SYSTEM.

SCHEDULE OF QUANTITIES

NOTE:

TAMPER RESISTANT STAINLESS STEEL BOLTS SHALL BE PROVIDED
 TREAT ALL ACCESS BOLTS WITH 'NEVER SEEZ' OR EQUAL.
 THE ACCESS DOOR SHALL LINE UP WITH THE HAND HOLE FOR THE TRAFFIC SIGNAL POLE.
 THE TRAFFIC SIGNAL SHROUD BASE SHALL BE SUPPORTED ON THE TRAFFIC SIGNAL POLE BASE PLATE.



EXISTING TRAFFIC BASE SCHEDULE

POLE LOCATION	LAMBERT	MAIN	PARK	NICOLL	BAKER HILL
NORTHWEST	A	A	A	A	
NORTHEAST	A	B	B	B	
SOUTHEAST	A	A	A	A	A
SOUTHWEST	B	B	B	B	

BASE A: 15" B.C. W/24" CONC. FOUNDATION A TOTAL: 10
 BASE B: 18" B.C. 2/30" CONC. FOUNDATION B TOTAL: 7
 TOTAL: 17

GLEN ELLYN
 TRAFFIC SIGNAL
 DECORATIVE BASE DETAIL
 (FOR INFORMATION)
 NO SCALE

RELOCATE EXISTING MAST ARM ASSEMBLY AND POLE (SPECIAL)
 NOTE: NEW TYPE "E" FOUNDATION SHALL BE 30" DIAMETER x 15'-0" IN DEPTH

ITEM DESCRIPTION	UNIT	QTY
SERVICE INSTALLATION - GROUND MOUNTED	EACH	1
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	105
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	63
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	10
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	290
HANDHOLE	EACH	1
HEAVY-DUTY HANDHOLE	EACH	1
DOUBLE HANDHOLE	EACH	1
PAINT EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
PAINT NEW TRAFFIC SIGNAL POST	EACH	3
TRANSCEIVER - FIBER OPTIC	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	697
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	936
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	613
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1443
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	1625
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	42
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1 C	FOOT	594
TRAFFIC SIGNAL POST, GALVANIZED STEEL 10 FT.	EACH	2
TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	1
CONCRETE FOUNDATION, TYPE A	FOOT	16
CONCRETE FOUNDATION, TYPE C	FOOT	4
CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	15
DRILL EXISTING HANDHOLE	EACH	4
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	2
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	6
INDUCTIVE LOOP DETECTOR	EACH	9
DETECTOR LOOP, TYPE I	FOOT	437
PEDESTRIAN PUSH-BUTTON	EACH	6
TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1
RELOCATE EXISTING SIGNAL HEAD	EACH	3
RELOCATE EXISTING MAST ARM ASSEMBLY AND POLE	EACH	1
RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, DETECTOR UNIT	EACH	1
RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, PHASING UNIT	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	1088
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING HANDHOLE	EACH	1
REMOVE EXISTING CONCRETE FOUNDATION	EACH	3
TEMPORARY INFORMATION SIGNING	SQ FT	51
TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1
FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	1
UNINTERRUPTABLE POWER SUPPLY, SPECIAL	EACH	1
ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED	FOOT	243
RELOCATE SIGN PANEL, TYPE 1	SQ FT	7

FILE NAME = P:\03\03040036\cond\phase 2\dwg\040036-sht-interconnect.dwg
 Plt 06, 2012 at 10:56
 Layout: 14-Schedule - Details

USER NAME =	DESIGNED - JAS	REVISED -
PLOT SCALE =	DRAWN - SS	REVISED -
PLOT DATE =	CHECKED - DY	REVISED -
	DATE - 6/29/09	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

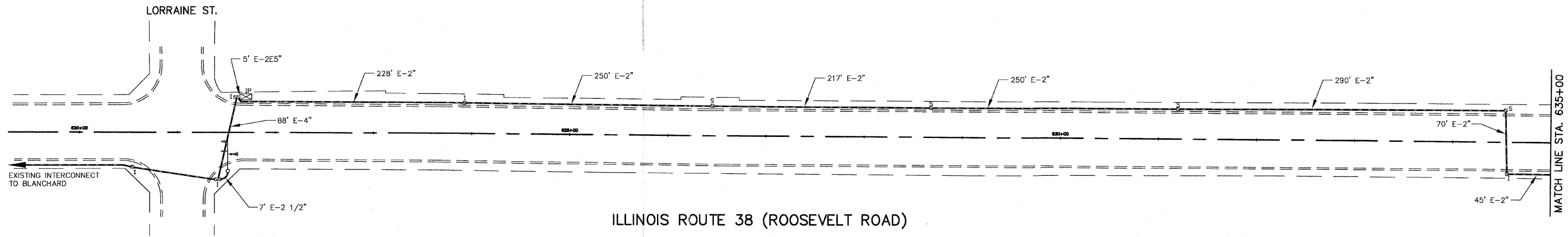


SCHEDULE OF QUANTITIES, EXISTING MAST
 ARM POLE DETAIL & DECORATIVE BASE
 ILLINOIS ROUTE 38 AND LAMBERT ROAD

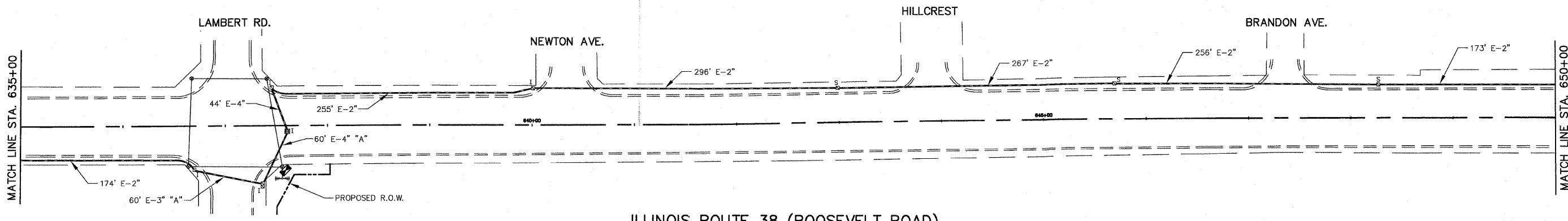
SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE. 2567	SECTION 05-00069-00-CH	COUNTY DuPAGE	TOTAL SHEETS 42	SHEET NO. 14
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

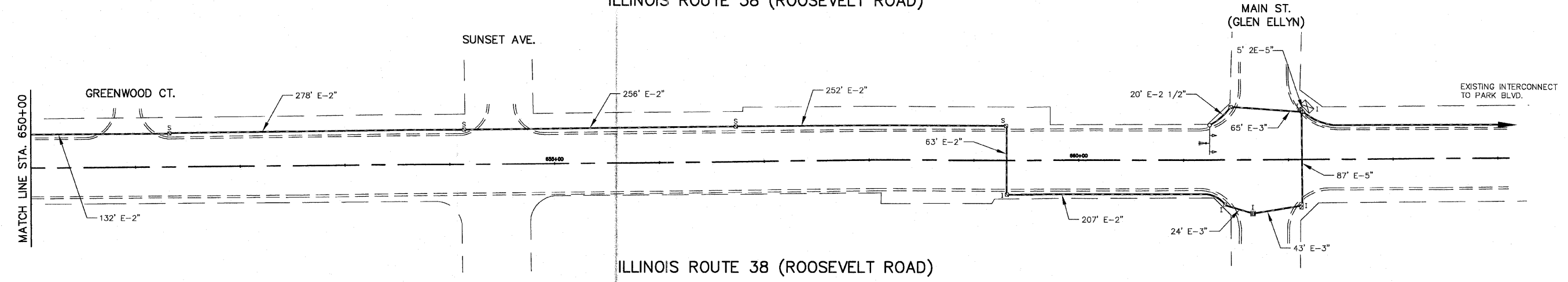
CONTRACT NO. 63567



ILLINOIS ROUTE 38 (ROOSEVELT ROAD)



ILLINOIS ROUTE 38 (ROOSEVELT ROAD)



ILLINOIS ROUTE 38 (ROOSEVELT ROAD)

INTERCONNECTION CONSTRUCTION NOTES

NOTES FOR LAMBERT:

- DISCONNECT FIBER AND TRACER CABLES FROM LAMBERT ROAD AND PULL BACK, COIL AND STORE CABLES IN CONDUITS AND HANDHOLES AT STATION 634+51 (EB) AND STATION 637+61 (WB) WHICH ARE TO REMAIN. REINSTALL CABLES IN CONDUITS, HANDHOLES AND CONTROLLER CABINET AS SHOWN IN THE PLANS.
- EXISTING INTERCONNECT CONDUIT AND SYSTEM HANDHOLES ARE TO BE UTILIZED FOR THE PERMANENT INTERCONNECT. USE SLACK FIBER CABLE AND SLACK TRACER WIRE BETWEEN LORRAINE AND LAMBERT AS REQUIRED TO COMPLETE THE REINSTALLATION.
- TEMPORARY RADIO INTERCONNECT TO MAIN STREET (EB) AND LORRAINE ROAD (WB).
- INSTALL TWO ANTENNAS ON THE TEMPORARY SPAN WIRE POLE LOCATED IN THE SOUTHEAST QUADRANT OF THE INTERSECTION OR AS RECOMMENDED BY THE VENDOR OF THE RADIO INTERCONNECTION COMMUNICATION SYSTEM.

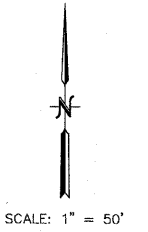
NOTE FOR MAIN STREET:

TEMPORARY RADIO INTERCONNECT TO LAMBERT ROAD
INSTALL ANTENNA ON THE WESTBOUND IL 38 MAST ARM POLE LOCATED IN THE NORTHWEST QUADRANT OF THE INTERSECTION OR AS RECOMMENDED BY THE VENDOR OF THE RADIO INTERCONNECTION COMMUNICATION SYSTEM.

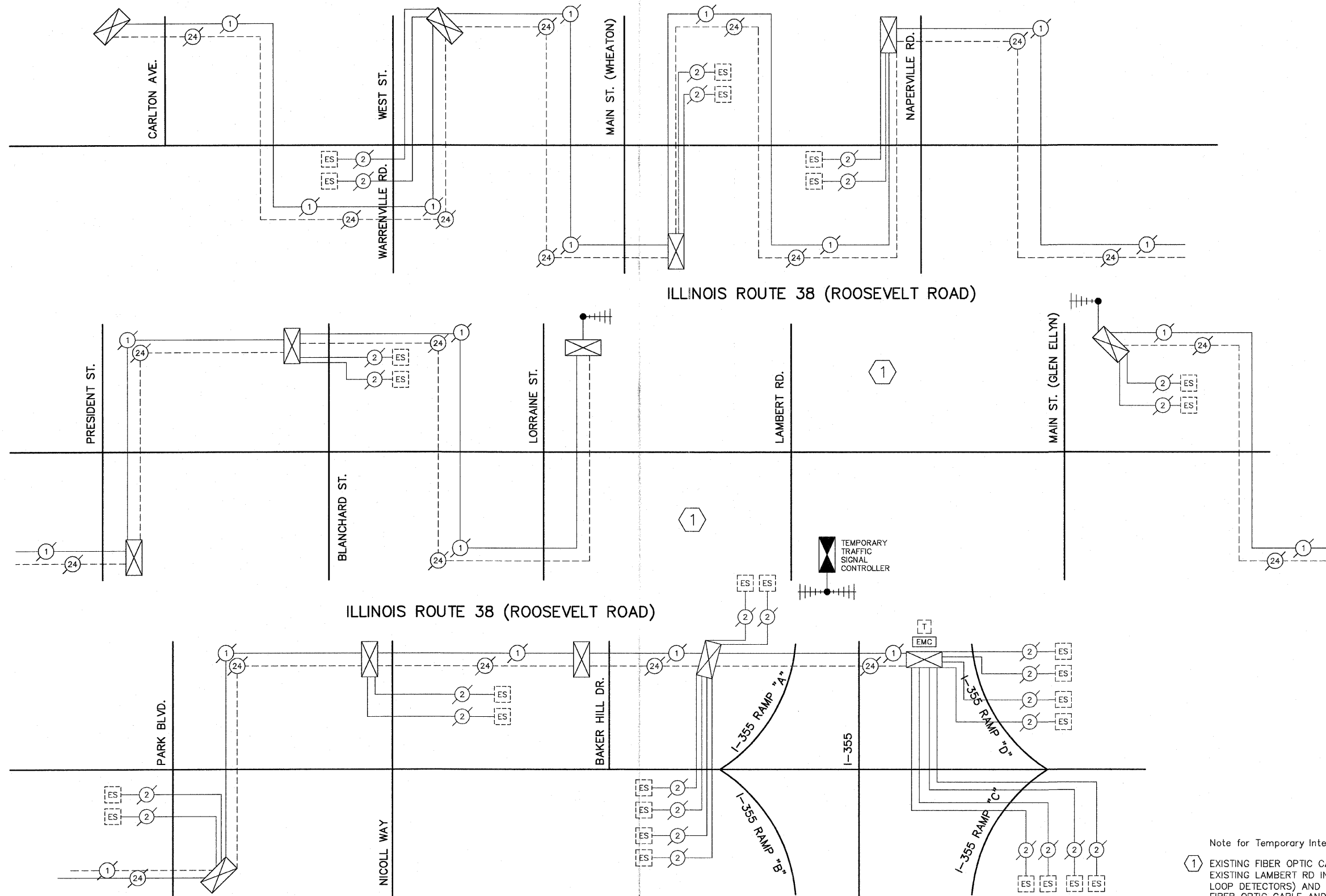
NOTE FOR LORRAINE ROAD:

TEMPORARY RADIO INTERCONNECT TO LAMBERT ROAD
INSTALL ANTENNA ON THE EASTBOUND IL 38 MAST ARM POLE LOCATED IN THE SOUTHEAST QUADRANT OF THE INTERSECTION OR AS RECOMMENDED BY THE VENDOR OF THE RADIO INTERCONNECTION COMMUNICATION SYSTEM.

RESTORATION OF WORK AREA
RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



FILE NAME = F:\03\03040036\cond\phase 2\dwg\040036-sht-interconnect.dwg Aug 29, 2011 at 8:47 Layout: 15-Temporary Interconnect Plan	USER NAME =	DESIGNED -- JAS	REVISED --	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HLR	TEMPORARY INTERCONNECT PLAN ILLINOIS ROUTE 38 AND LAMBERT ROAD	F.A.U. RTE. = 2567	SECTION = 05-00069-00-CH	COUNTY = DuPAGE	TOTAL SHEETS = 42	SHEET NO. = 15
PLOT SCALE =	DRAWN -- SS	REVISED --	SHEET NO. OF SHEETS STA. TO STA.				CONTRACT NO. = 63567				
PLOT DATE =	CHECKED -- DY	REVISED --	FED. ROAD DIST. NO. = ILLINOIS				FED. AID PROJECT				
	DATE -- 6/29/09	REVISED --									



Note for Temporary Interconnect Schematic

1 EXISTING FIBER OPTIC CABLE AND TRACER WIRE TO BE PULLED BACK TO THE EXISTING LAMBERT RD INTERSECTION HANDHOLES AT STATION 639+85 (WB FAR OUT LOOP DETECTORS) AND AT STATION 634+51 (EB FAR OUT LOOP DETECTORS). THE FIBER OPTIC CABLE AND TRACER WIRE SHALL BE REINSTALLED TO THE NEW CONTROL CABINET AT LAMBERT ROAD

FILE NAME = F:\03\03040036\coord\phase 2\dwg\040036-sht-Interconnect.dwg
 Aug 29, 2011 at 8:47
 Layout: 16-Temporary Interconnect Schematic

USER NAME =	DESIGNED -- JAS	REVISED --
PLOT SCALE =	DRAWN -- SS	REVISED --
PLOT DATE =	CHECKED -- DY	REVISED --
	DATE -- 6/29/09	REVISED --

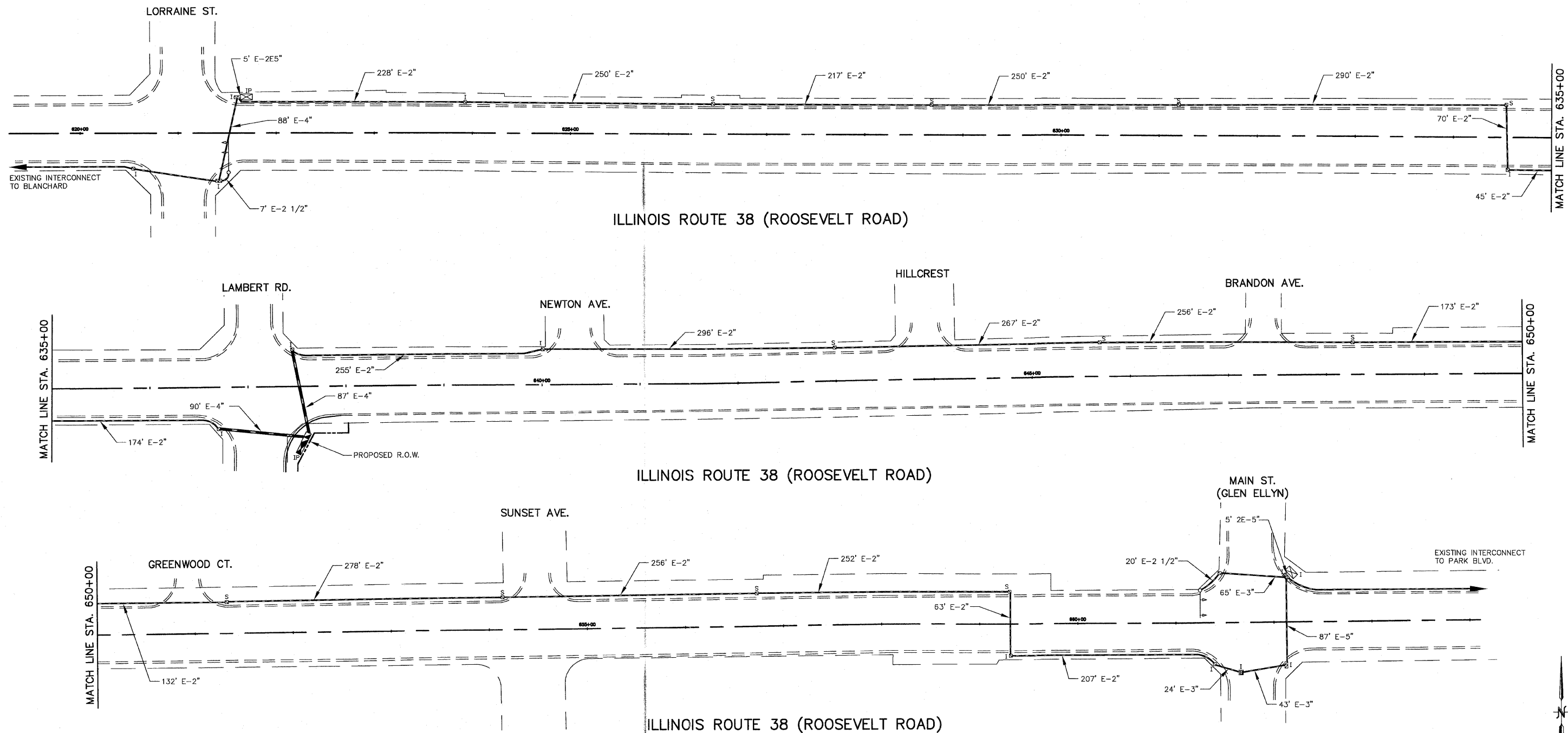
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION



TEMPORARY INTERCONNECT SCHEMATIC
 IL RTE 38 (ROOSEVELT) FROM
 CARLTON AVE TO I-355 (E. RAMP)

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2567	05-00069-00-CH	DuPAGE	42	16
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		CONTRACT NO. 63567		



INTERCONNECTION CONSTRUCTION NOTES

Notes for Lambert:

- EXISTING FIBER OPTIC CABLE AND TRACER WIRE TO BE PULLED BACK TO THE EXISTING LAMBERT INTERSECTION HANDHOLES AT STATION 637+61 (WB FAR OUT LOOP DETECTORS) AND AT STATION 634+51 (EB FAR OUT LOOP DETECTORS). THE FIBER OPTIC CABLE AND TRACER WIRE SHALL BE REINSTALLED TO THE NEW CONTROL CABINET AT LAMBERT ROAD
- EXISTING INTERCONNECT CONDUIT AND SYSTEM HANDHOLES ARE TO BE UTILIZED FOR THE PERMANENT INTERCONNECT. USE SLACK FIBER CABLE AND SLACK TRACER WIRE BETWEEN LORRAINE AND LAMBERT AS REQUIRED TO COMPLETE THE REINSTALLATION.

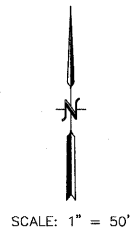
Note for Main Street:

REMOVE TEMPORARY RADIO INTERCONNECT AND ANTENNA. RESTORE EXISTING CONTROLLER CABINET AND TRAFFIC SIGNALS.

Note for Lorraine Road:

REMOVE TEMPORARY RADIO INTERCONNECT AND ANTENNA. RESTORE EXISTING CONTROLLER CABINET AND TRAFFIC SIGNALS.

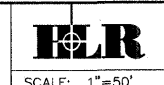
RESTORATION OF WORK AREA
 RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



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 Aug 29, 2011 at 9:48
 Layout: 17-Interconnect Plan

USER NAME =	DESIGNED -- JAS	REVISED --
	DRAWN -- SS	REVISED --
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PLOT DATE =	DATE -- 6/29/09	REVISED --

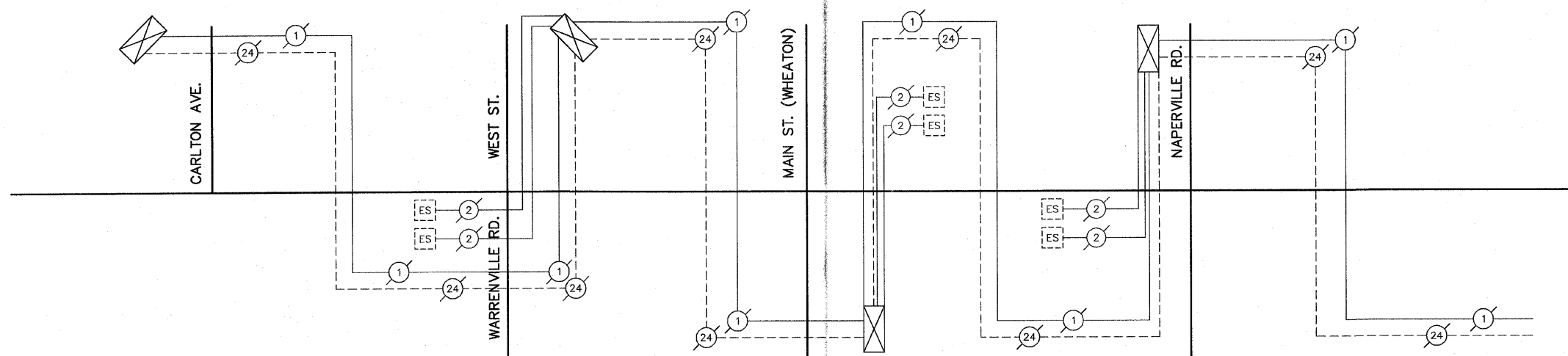
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**



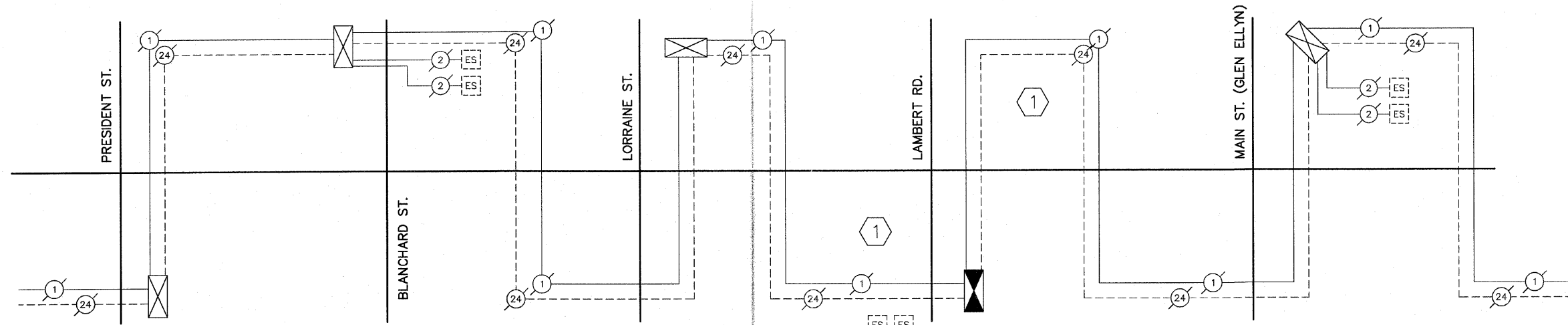
INTERCONNECT PLAN
 ILLINOIS ROUTE 38 AND LAMBERT ROAD

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2567	05-00069-00-CH	DUPAGE	42	17
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

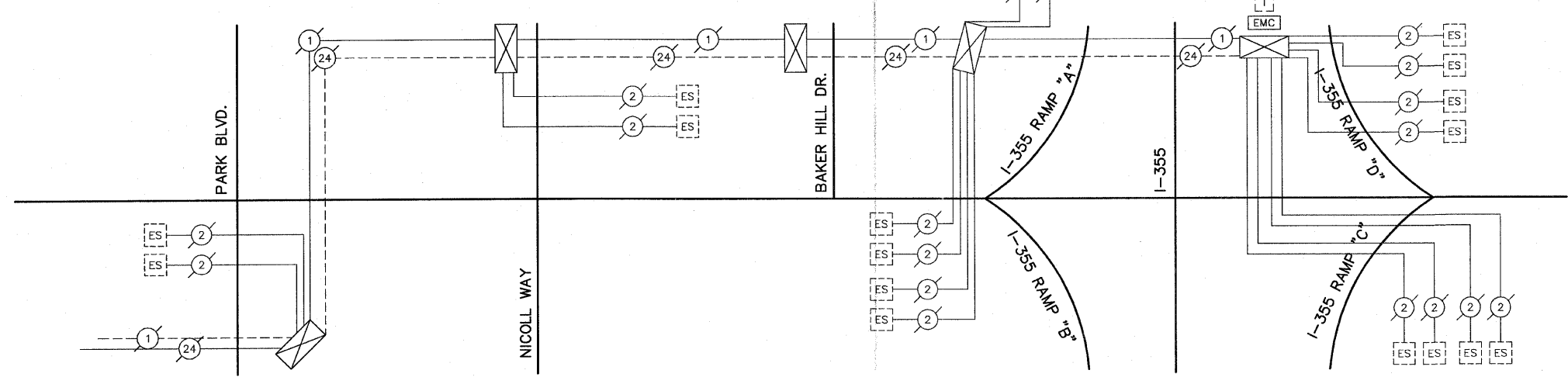
SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.



ILLINOIS ROUTE 38 (ROOSEVELT ROAD)



ILLINOIS ROUTE 38 (ROOSEVELT ROAD)



SCHEDULE OF QUANTITIES

ITEM	UNIT	TOTAL
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2
REMOVE & REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	713
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1
REMOVE & REINSTALL FIBER OPTIC CABLE IN CONDUIT	L SUM	1

Note for Interconnect Schematic
 ① EXISTING FIBER OPTIC CABLE AND TRACER WIRE TO BE PULLED BACK TO THE EXISTING INTERSECTION HANDHOLES AT STATION 639+85 (WB FAR OUT LOOP DETECTORS) AND AT STATION 634+51 (EB FAR OUT LOOP DETECTORS). THE FIBER OPTIC CABLE AND TRACER WIRE SHALL BE REINSTALLED TO THE NEW CONTROL CABINET AT LAMBERT ROAD

FILE NAME =
 P:\03\03040036\cod\phase 2\dwg\040036-sh1-interconnect.dwg
 Feb 06, 2012 at 10:53
 Layout: 18-Proposed Interconnect Schematic

USER NAME =
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 DRAWN -- SS
 CHECKED -- DY
 DATE -- 6/29/09

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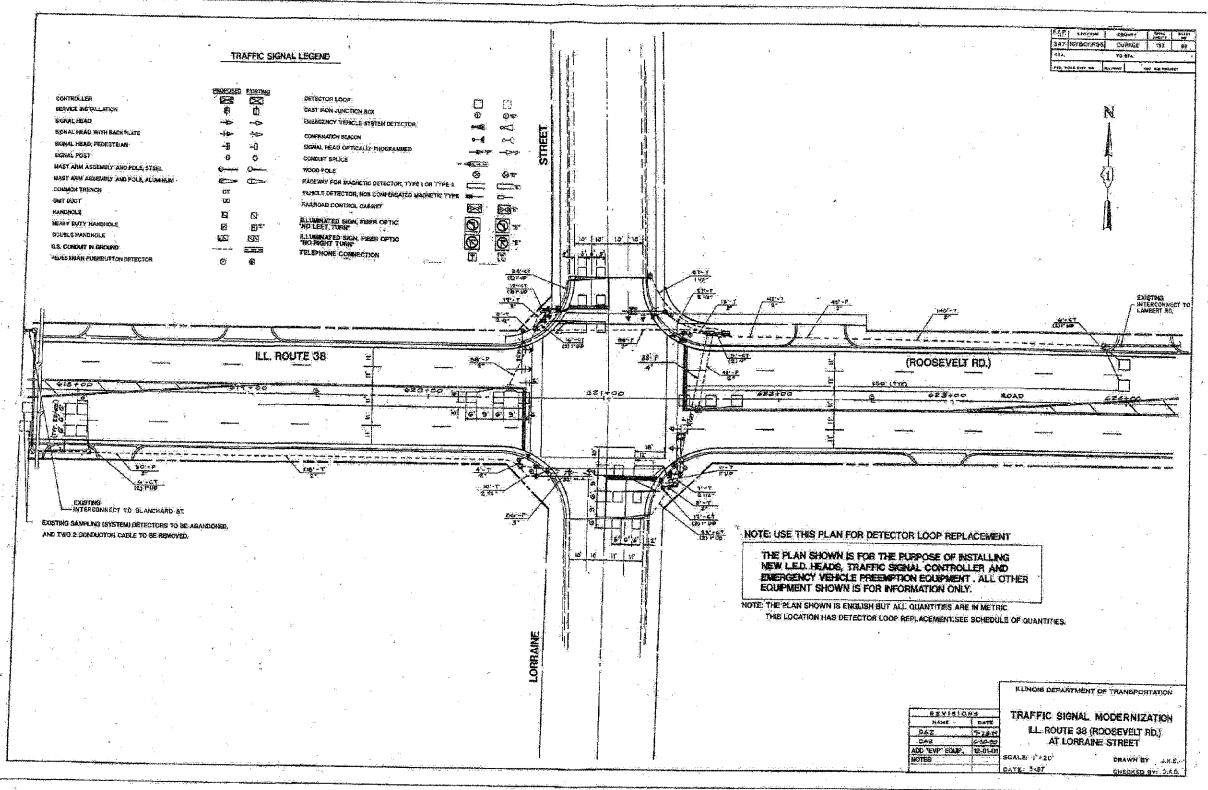
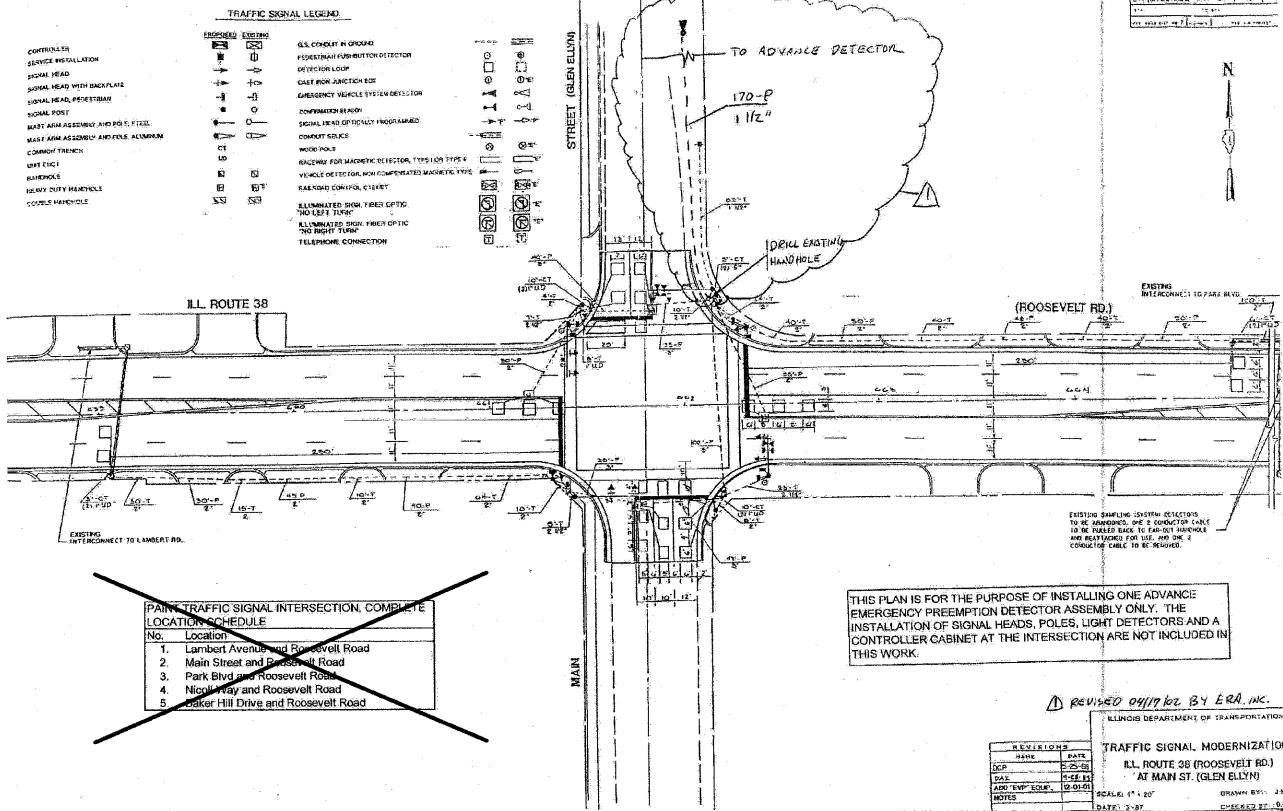
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION



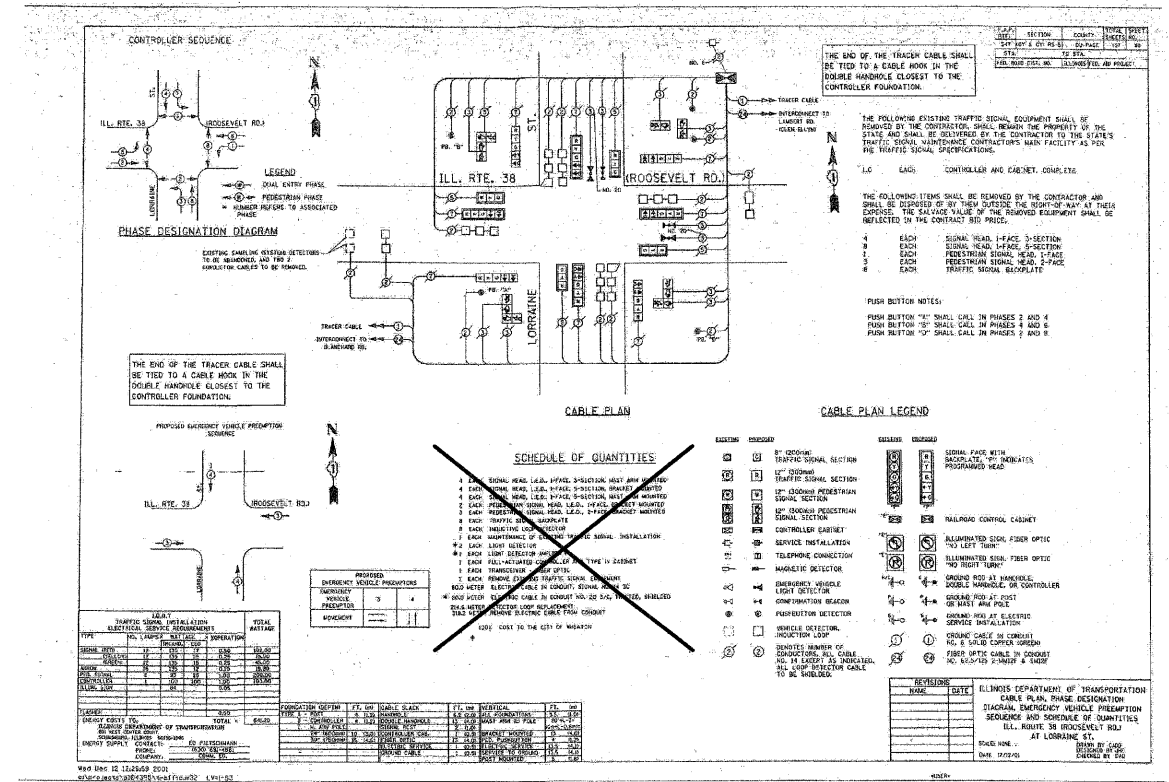
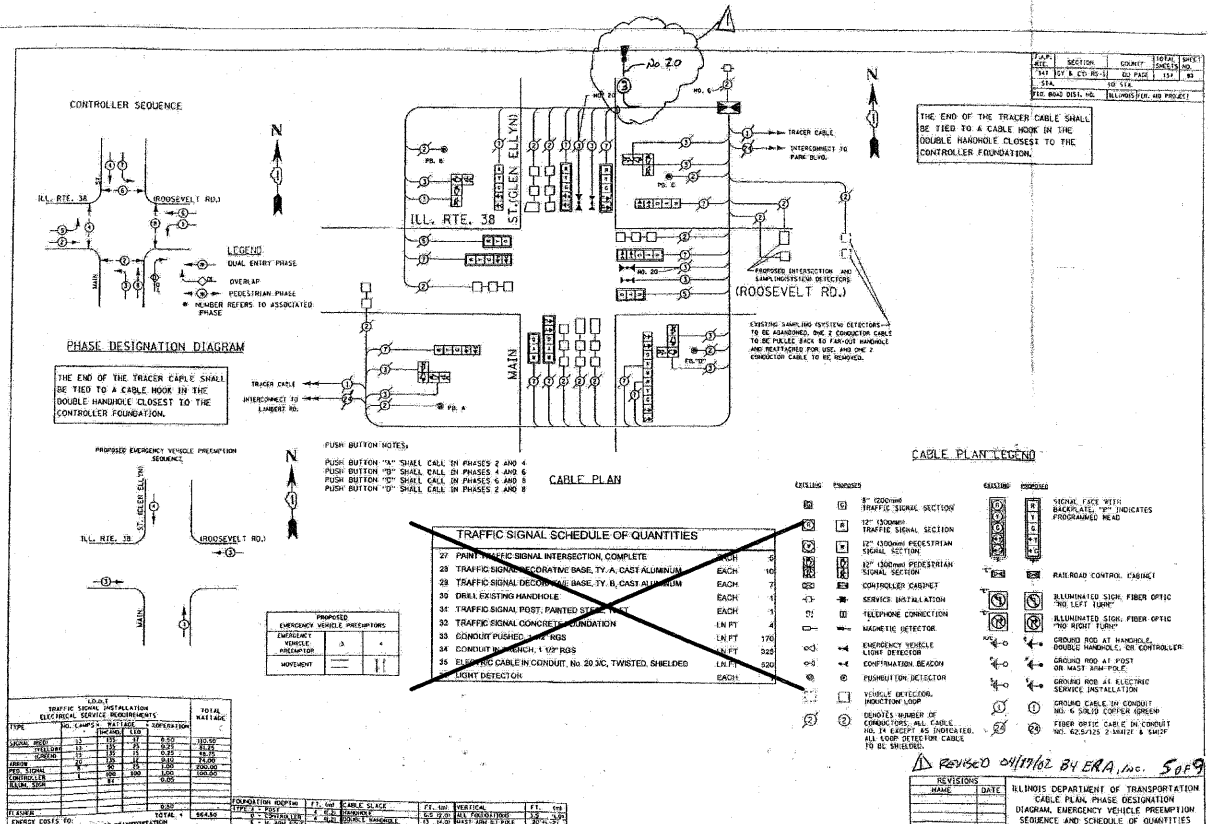
INTERCONNECT SCHEMATIC
 IL RTE 38 (ROOSEVELT) FROM
 CARLTON AVE TO I-355 (E. RAMP)

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2567	05-00069-00-CH	DuPAGE	42	18
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		CONTRACT NO. 63567		



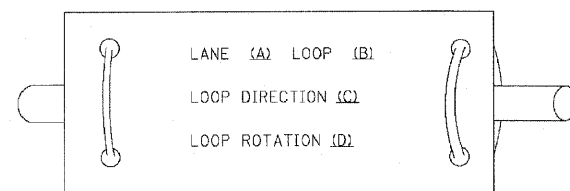
FOR INFORMATION ONLY



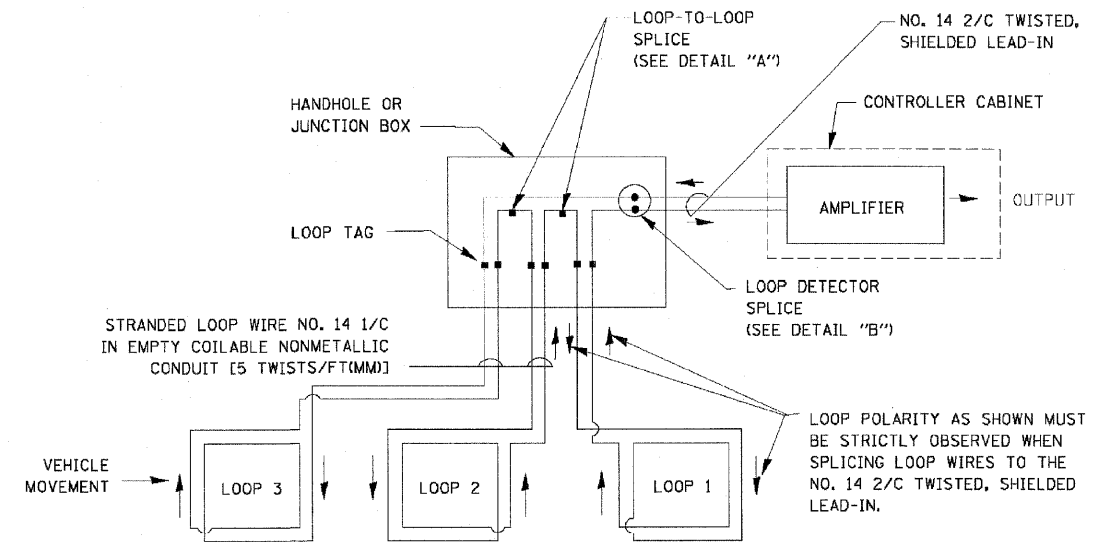
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

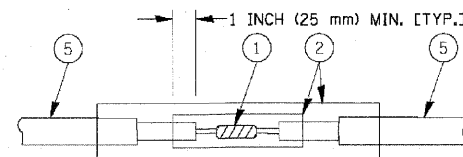


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

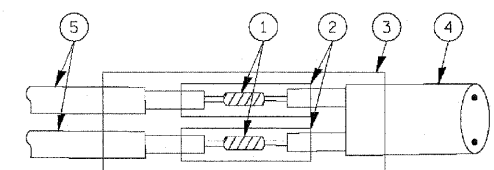


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

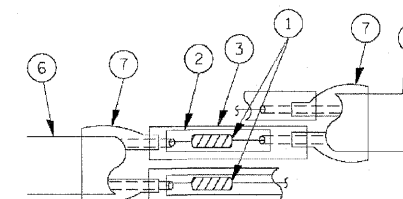


DETAIL "A"
LOOP-TO-LOOP SPLICE

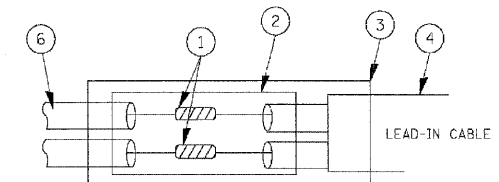


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

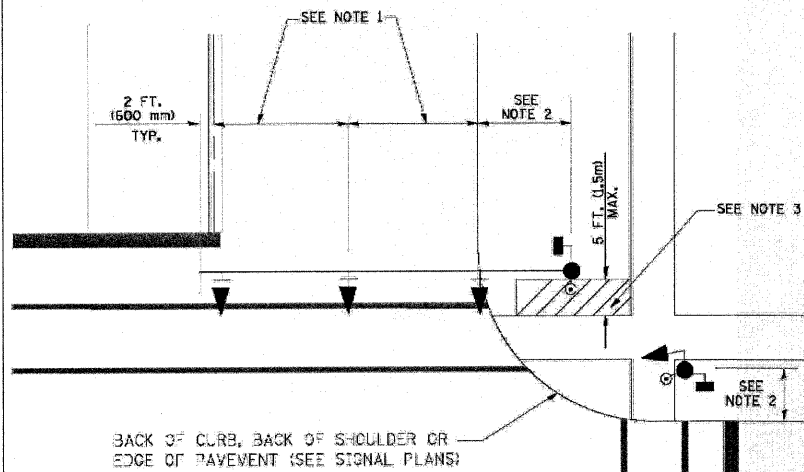
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: SHEET NO. 1 OF 6 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2567	05-00069-00-CH	DuPAGE	42	20
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 63567	

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

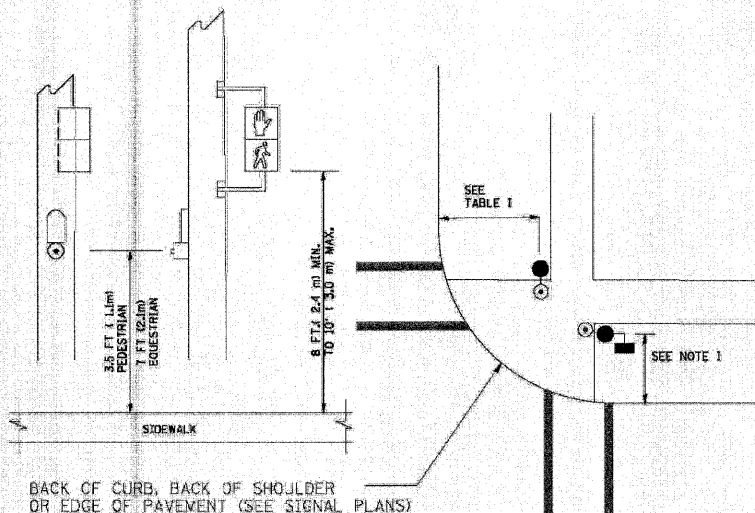
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

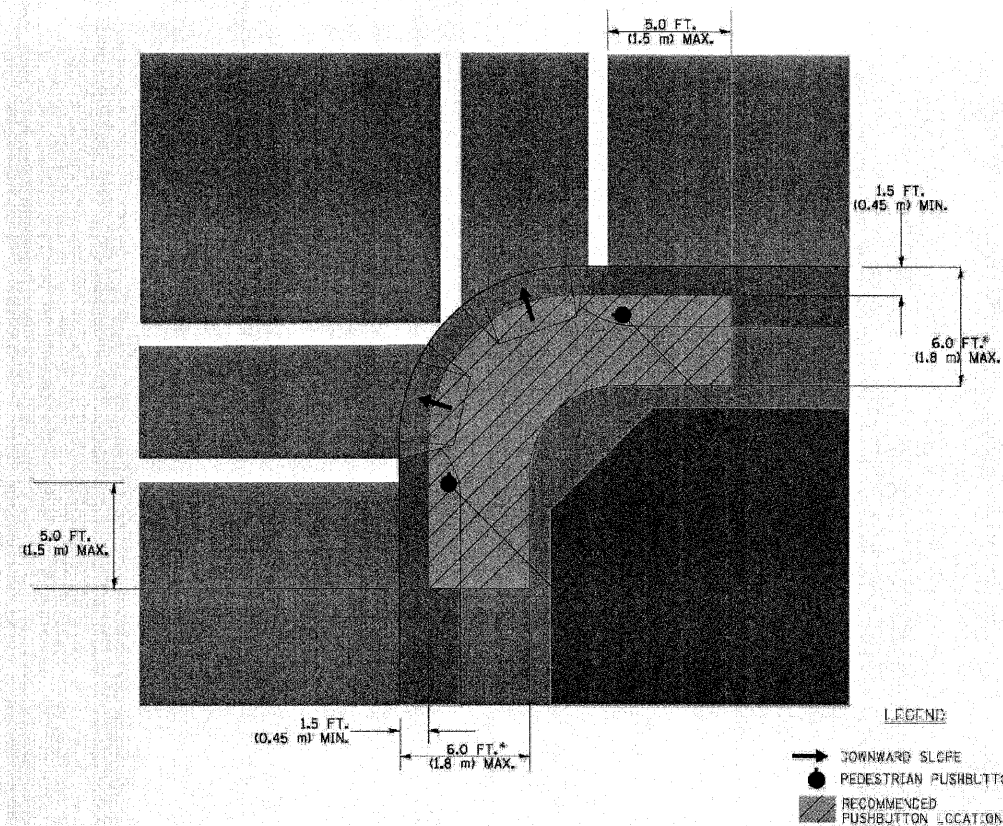
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



- * WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- ** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WERE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.2 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD AFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

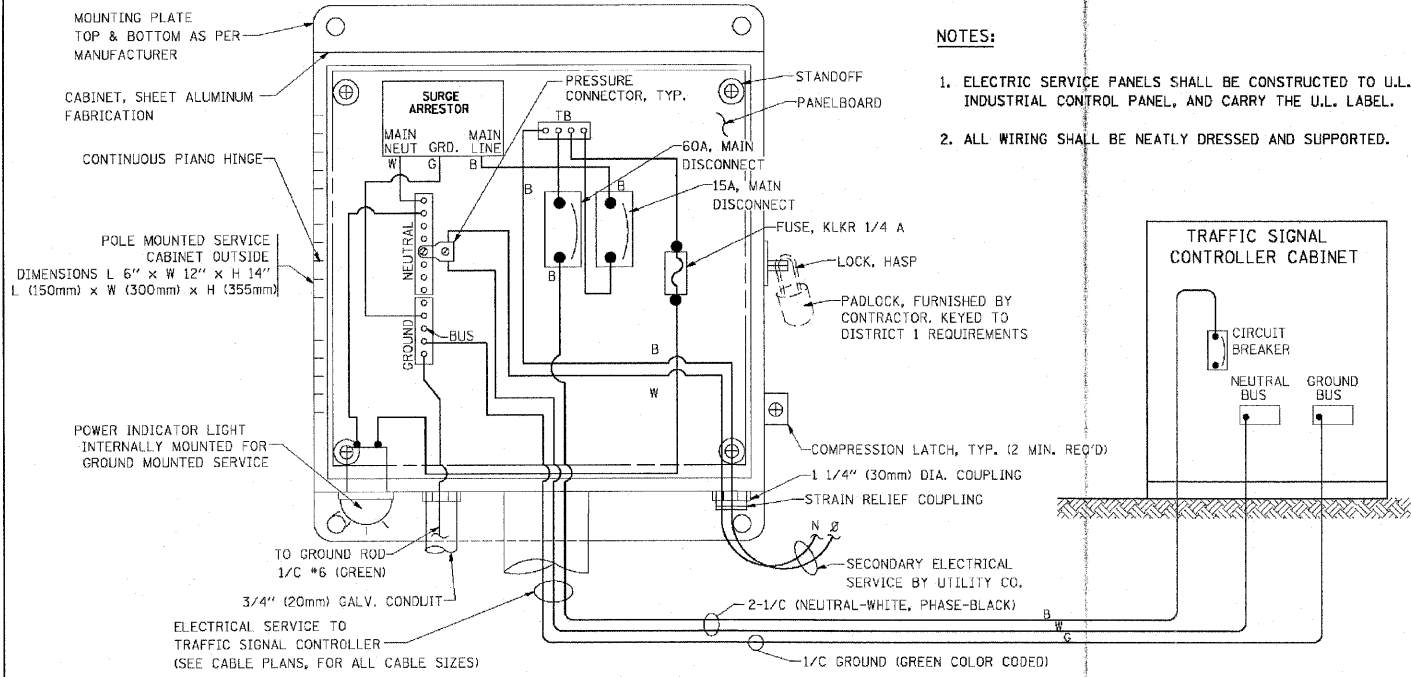
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

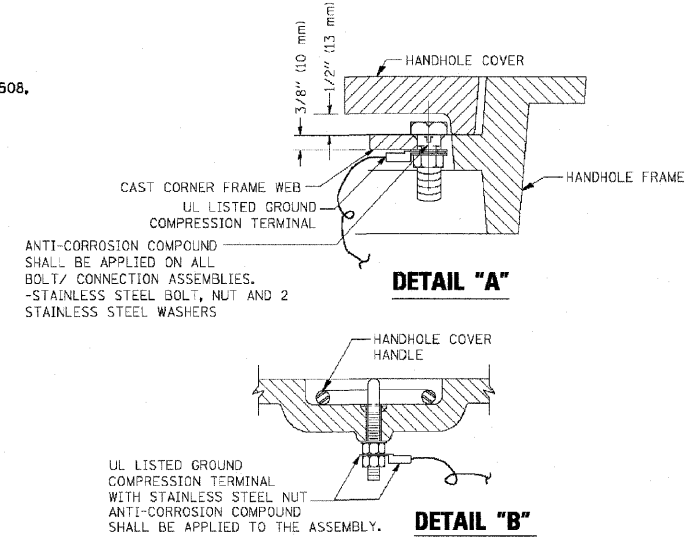
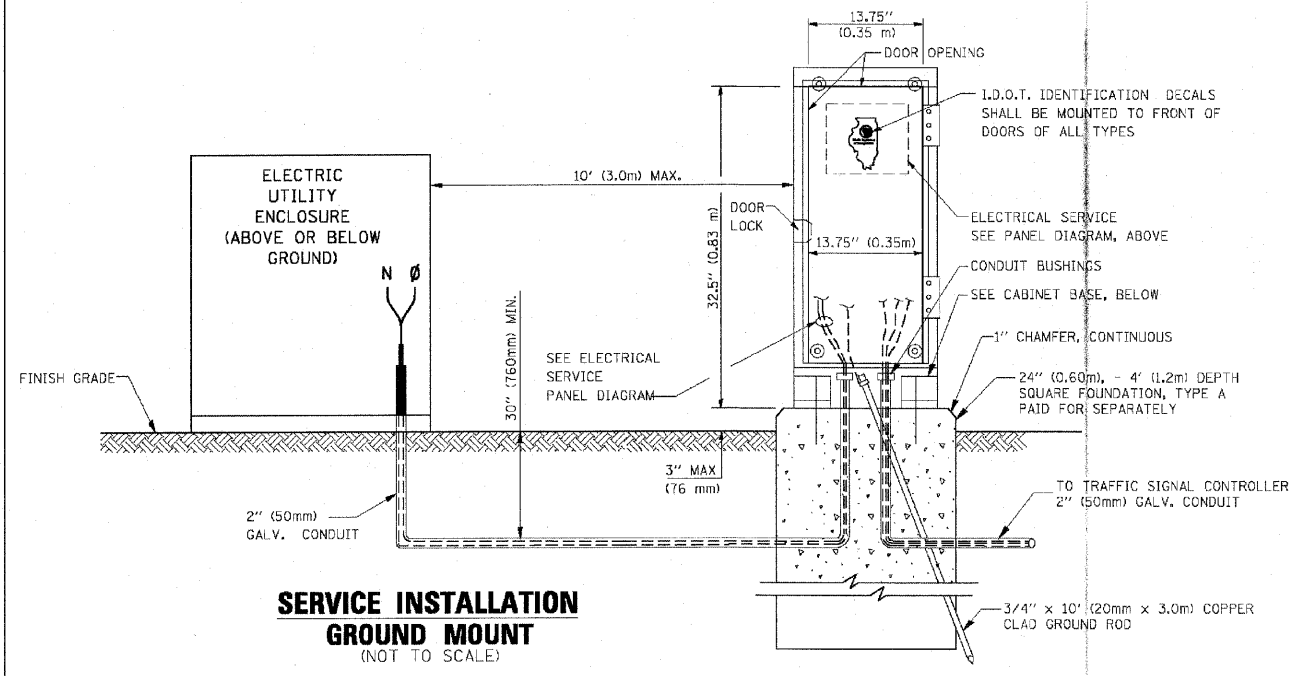
**DISTRICT 1
 STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: SHEET NO. 2 OF 6 SHEETS STA. TO STA.

F.A. II RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO.		63567		



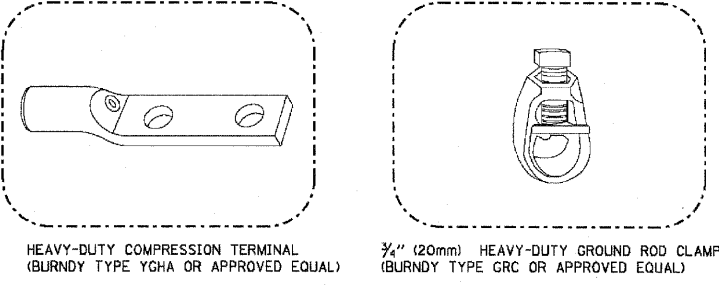
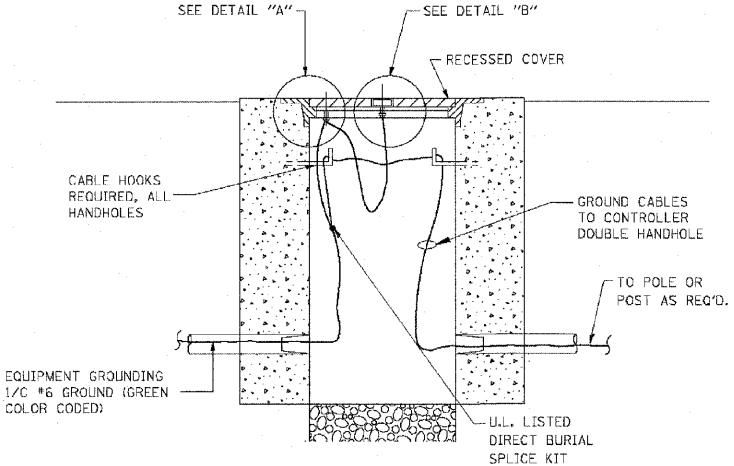
ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
 (NOT TO SCALE)



NOTES:

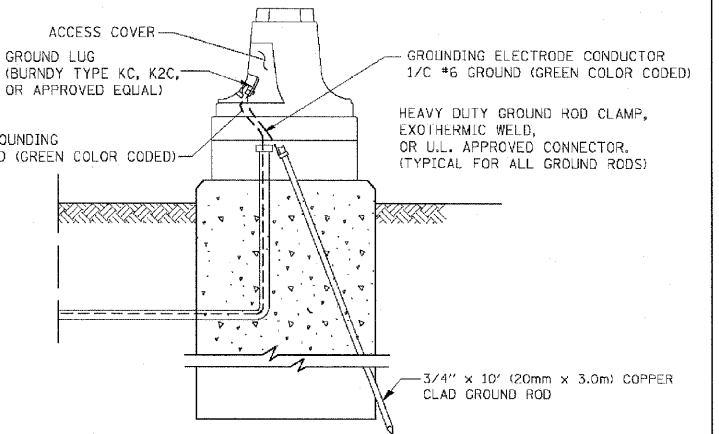
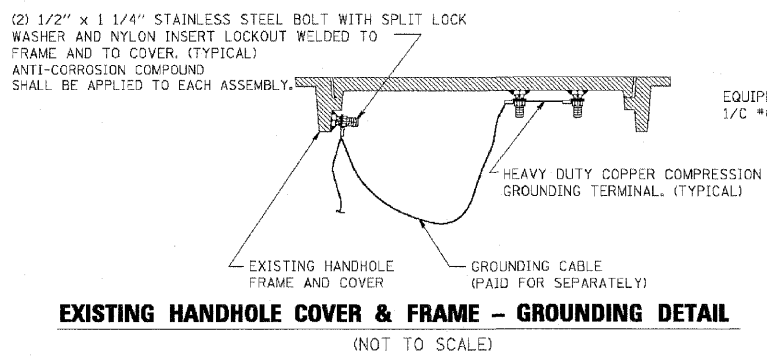
GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, U.L. APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



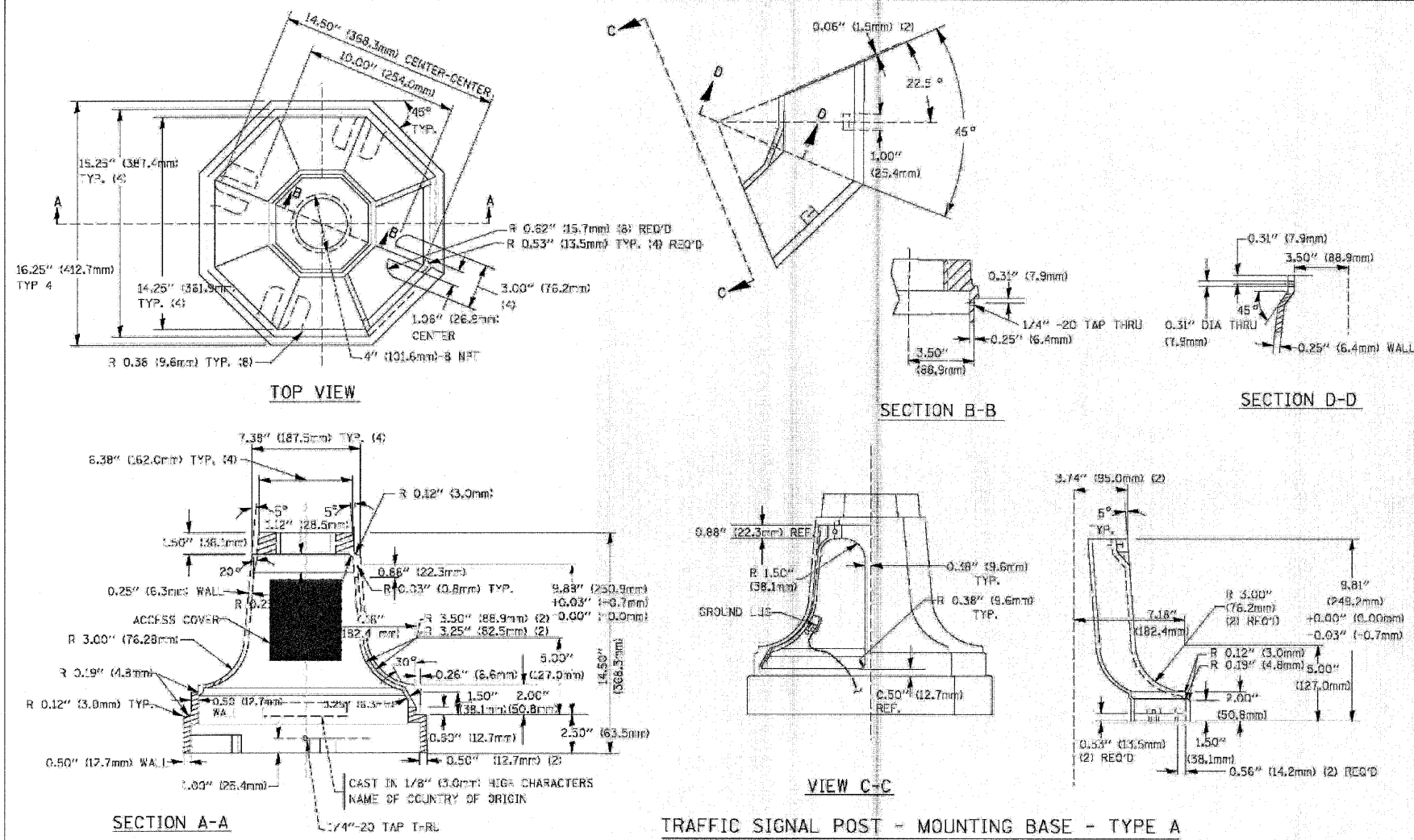
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

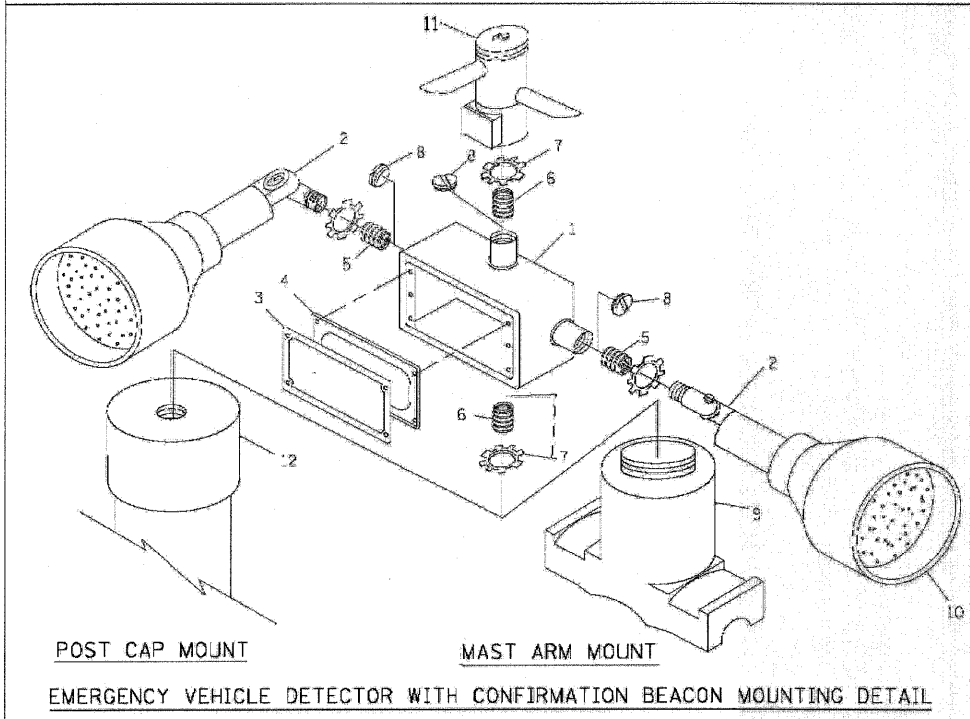
DISTRICT 1
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	05-00069-00-CH	DuPAGE	42	22
CONTRACT NO. 63567				
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

SCALE: SHEET NO. 3 OF 6 SHEETS STA. TO STA.



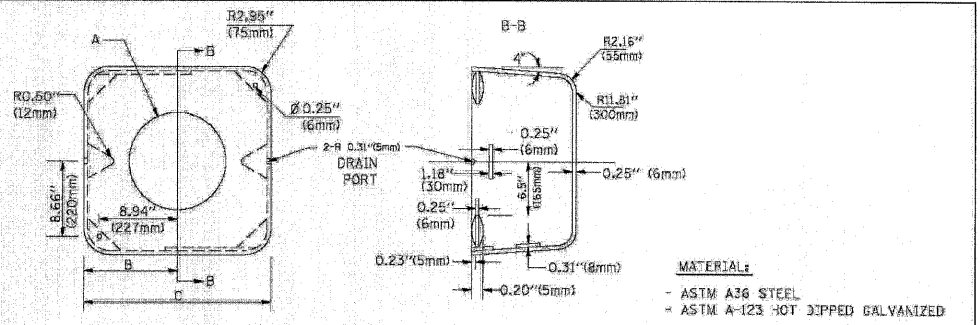
TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A



POST CAP MOUNT
 MAST ARM MOUNT
 EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL

ITEM NO.	IDENTIFICATION
1	CLIFF BOX - GALV. 21 CU LIN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	CLIFF BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP (18 FT. (5.4 m) POST MIN.)

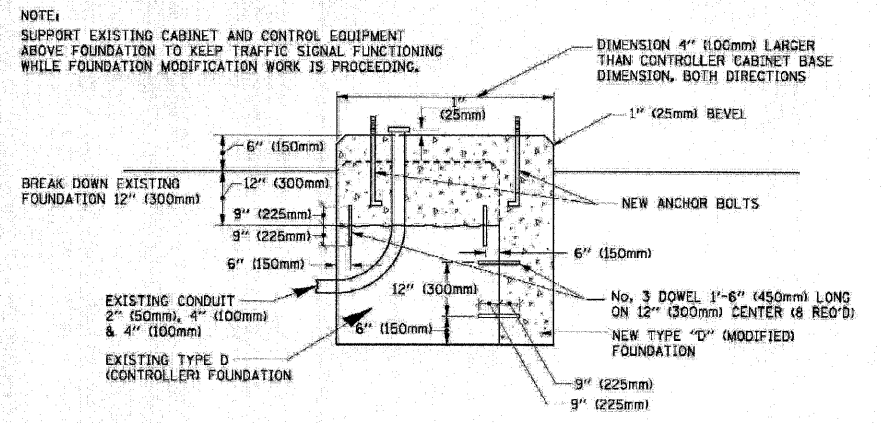
- NOTES:**
- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
 - ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
 ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
 ITEM #8- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
 - WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



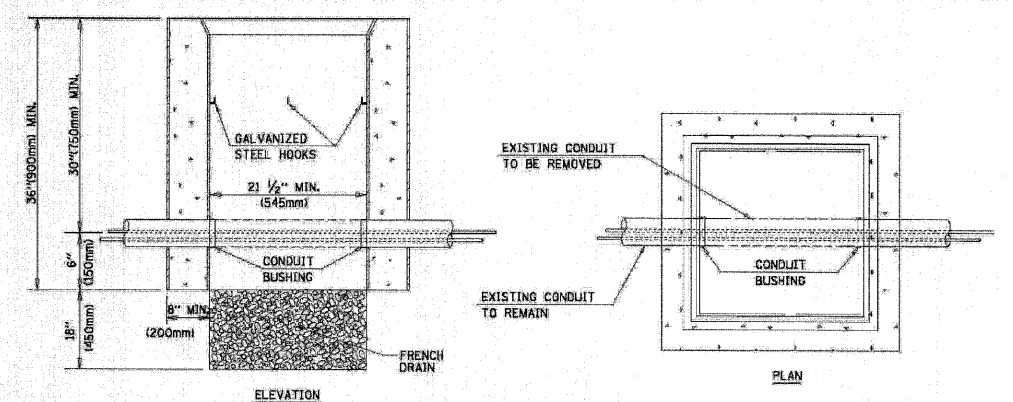
A	B	C	HEIGHT	WEIGHT
VARIES	9.5" (241mm)	19" (483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75" (273mm)	21.5" (546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0" (330mm)	26" (660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18.5" (470mm)	37" (940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

SHROUD

- NOTES:**
- DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
 - THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
 - THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.



MODIFY EXISTING TYPE "D" FOUNDATION



- NOTES:**
- HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
 - REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCIDENTAL TO THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

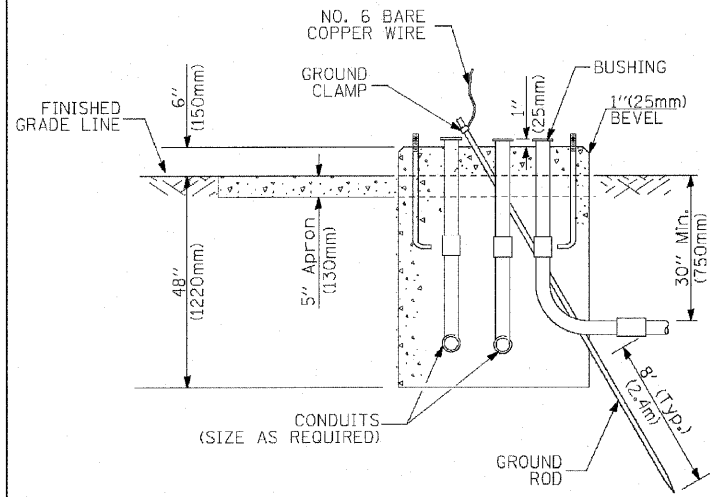
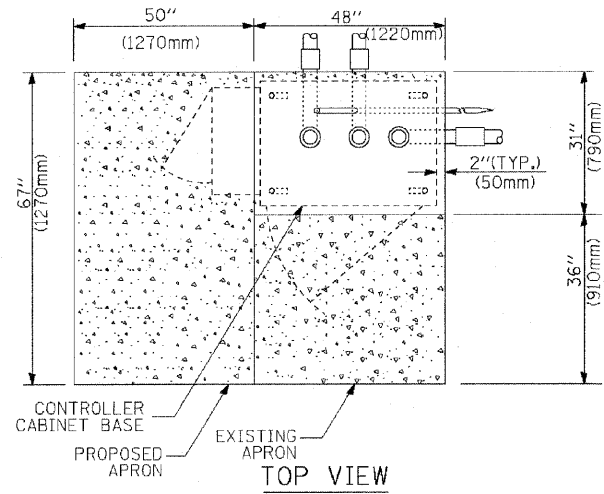
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

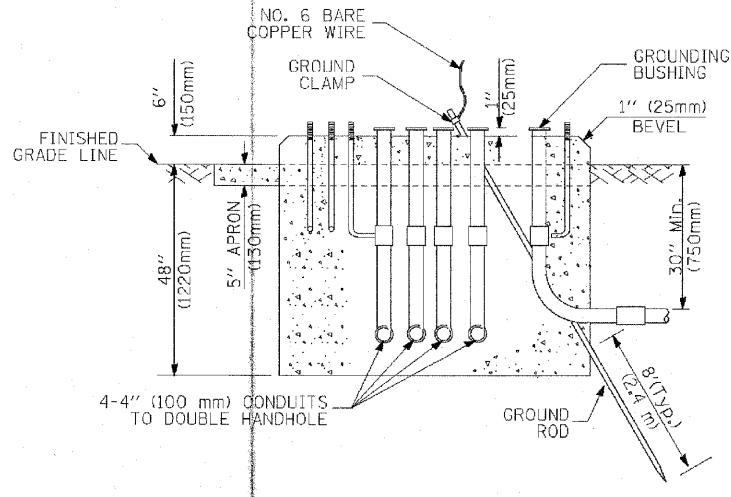
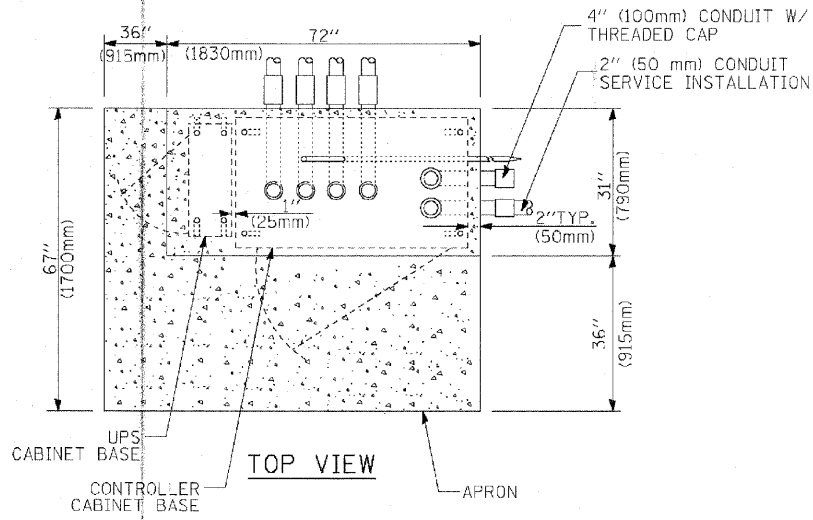
DISTRICT 1
 STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: SHEET NO. 4 OF 6 SHEETS STA. TO STA.

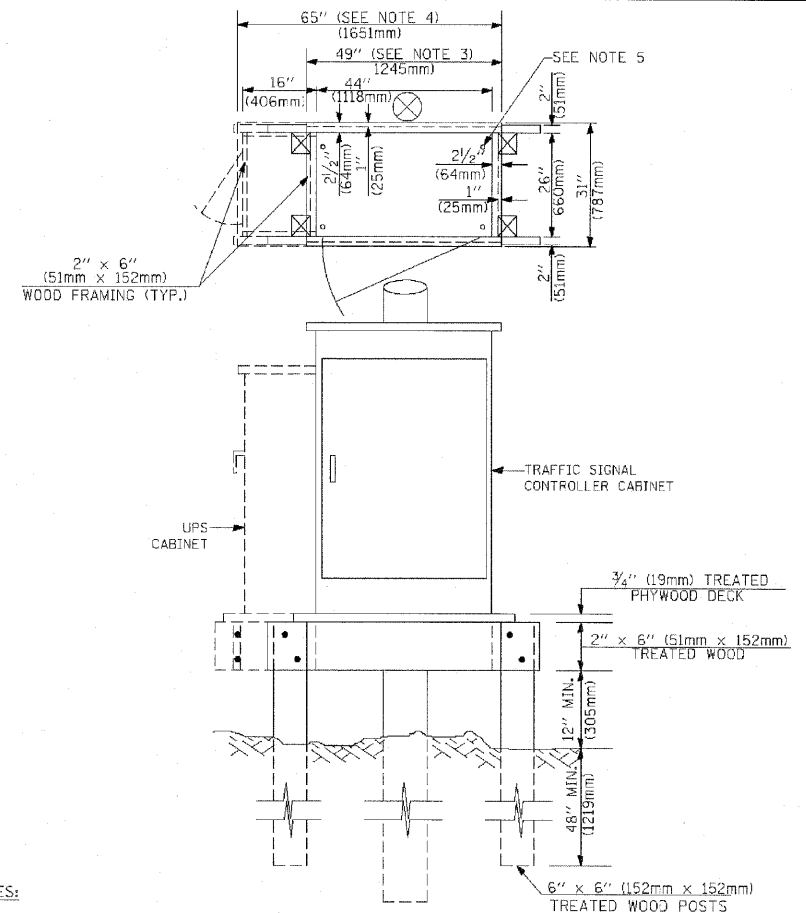
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2567	05-00069-00-CH	DuPAGE	42	23
CONTRACT NO. 63567				
FED. ROAD DIST. NO. [IL] [CN] [S] FED. AID PROJECT				



**TYPE D
 FOR GROUND MOUNTED
 CONTROLLER CABINET
 AND UPS BATTERY CABINET**



**TYPE C
 FOR GROUND MOUNTED
 CONTROLLER CABINET
 AND UPS BATTERY CABINET**



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

**TEMPORARY SIGNAL CONTROLLER
 WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

MAST ARM LENGTH	FOUNDATION DEPTH	FOUNDATION DIAMETER	SPIRAL DIAMETER	QUANTITY OF REBARS	SIZE OF REBARS
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

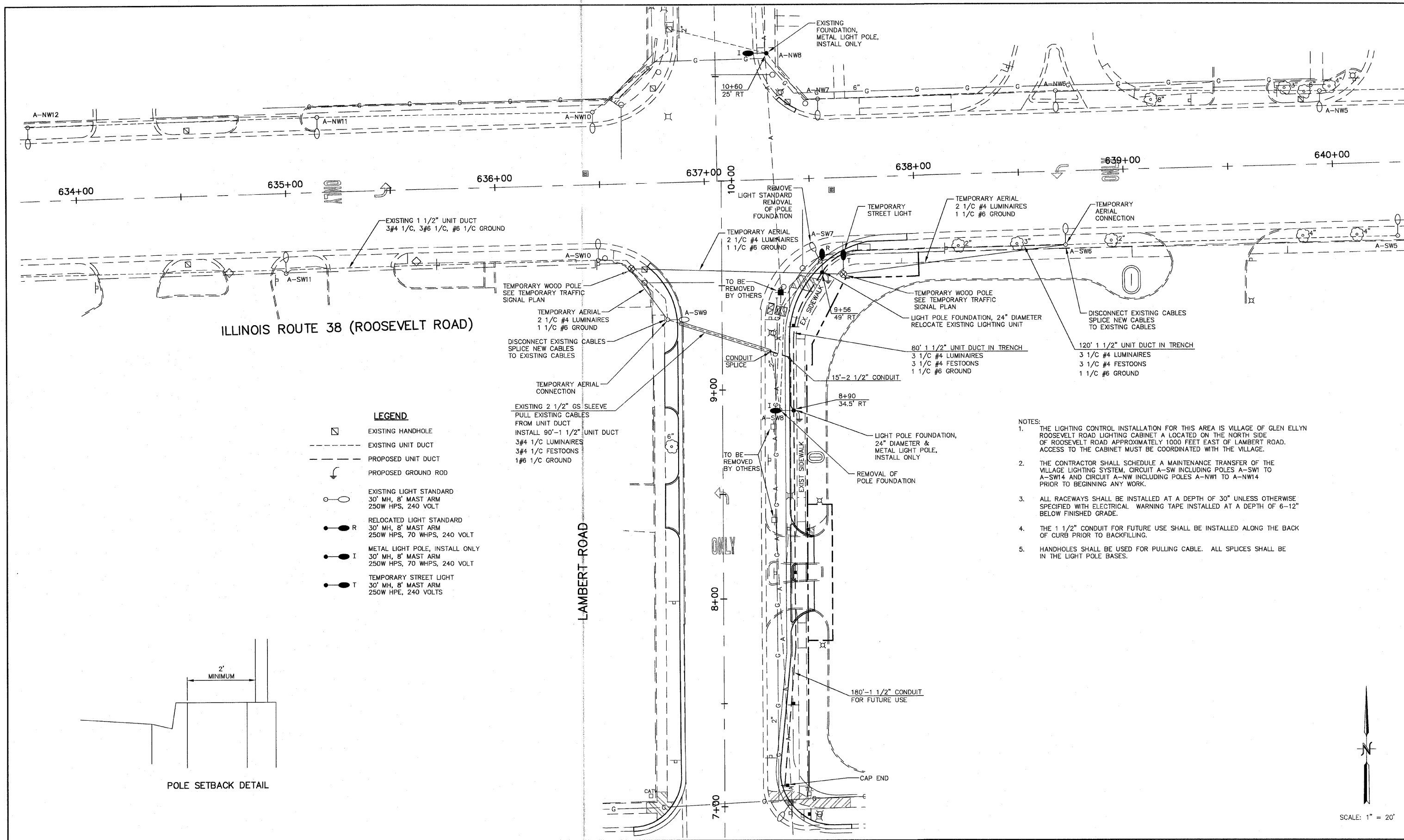
NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (QU) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001.

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

TRAFFIC SIGNAL LEGEND

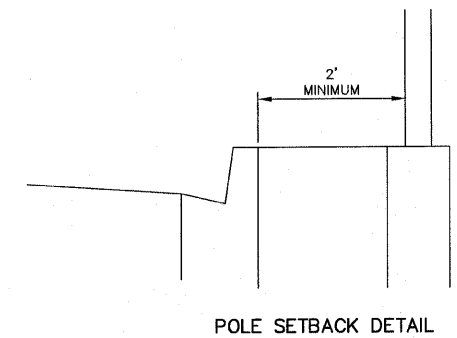
ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED
CONTROLLER CABINET				EMERGENCY VEHICLE LIGHT DETECTOR				ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE			
RAILROAD CONTROL CABINET				CONFIRMATION BEACON				COAXIAL CABLE			
COMMUNICATIONS CABINET				HANDHOLE				VENDOR CABLE FOR CAMERA			
MASTER CONTROLLER				HEAVY DUTY HANDHOLE				COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED			
MASTER MASTER CONTROLLER				DOUBLE HANDHOLE				FIBER OPTIC CABLE NO. 62.5/125, MMI2F			
UNINTERRUPTIBLE POWER SUPPLY				JUNCTION BOX				FIBER OPTIC CABLE NO. 62.5/125, MMI2F SMI2F			
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT				GALVANIZED STEEL CONDUIT IN TRENCH (T) OR PUSHED (P)				FIBER OPTIC CABLE NO. 62.5/125, MMI2F SMI2F			
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT				TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE				FIBER OPTIC CABLE NO. 62.5/125, (NUMBER OF FIBERS & TYPE TO BE NOTED ON PLANS)			
STEEL MAST ARM ASSEMBLY AND POLE				COMMON TRENCH			CT	GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE			
ALUMINUM MAST ARM ASSEMBLY AND POLE				COILABLE NONMETALLIC CONDUIT (EMPTY)			CNC	CONTROLLER CABINET AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE				SYSTEM ITEM		S	S	STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA				INTERSECTION ITEM		I	IP	ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED			
SIGNAL POST				REMOVE ITEM	R			STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED			
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM				RELOCATE ITEM	RL			SIGNAL POST AND FOUNDATION TO BE REMOVED			
GUY WIRE				ABANDON ITEM	A			INTERSECTION & SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD				12" (300mm) TRAFFIC SIGNAL SECTION				SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)				12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE				EXISTING INTERSECTION LOOP DETECTOR PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD WITH BACKPLATE				SIGNAL FACE				EXISTING PREFORMED INTERSECTION LOOP DETECTOR PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD OPTICALLY PROGRAMMED				SIGNAL FACE WITH BACKPLATE, "P" INDICATES PROGRAMMED HEAD				PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
FLASHER INSTALLATION (S DENOTES SOLAR POWER)				12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL				PREFORMED SAMPLING (SYSTEM) DETECTOR			
PEDESTRIAN SIGNAL HEAD				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED				RAILROAD SYMBOLS			
PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID				EXISTING		PROPOSED	
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR				PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER				RAILROAD CONTROL CABINET			
ILLUMINATED SIGN "NO LEFT TURN"				RADIO INTERCONNECT				RAILROAD CANTILEVER MAST ARM			
ILLUMINATED SIGN "NO RIGHT TURN"				RADIO REPEATER				FLASHING SIGNAL			
DETECTOR LOOP, TYPE I				DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED				CROSSING GATE			
PREFORMED DETECTOR LOOP				GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)				CROSSBUCK			
MICROWAVE VEHICLE SENSOR											
VIDEO DETECTION CAMERA											
VIDEO DETECTION ZONE											
PAN, TILT, ZOOM CAMERA											
WIRELESS DETECTOR SENSOR											
WIRELESS ACCESS POINT											



ILLINOIS ROUTE 38 (ROOSEVELT ROAD)

LAMBERT ROAD

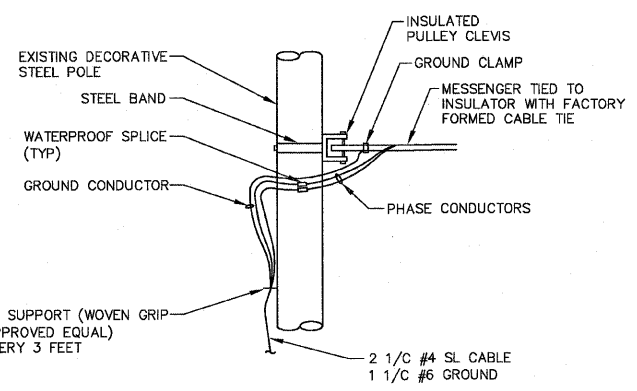
- LEGEND**
- EXISTING HANDHOLE
 - - - EXISTING UNIT DUCT
 - - - PROPOSED UNIT DUCT
 - ⤴ PROPOSED GROUND ROD
 - EXISTING LIGHT STANDARD
30' MH, 8' MAST ARM
250W HPS, 240 VOLT
 - R RELOCATED LIGHT STANDARD
30' MH, 8' MAST ARM
250W HPS, 70 WHPS, 240 VOLT
 - I METAL LIGHT POLE, INSTALL ONLY
30' MH, 8' MAST ARM
250W HPS, 70 WHPS, 240 VOLT
 - T TEMPORARY STREET LIGHT
30' MH, 8' MAST ARM
250W HPS, 240 VOLTS



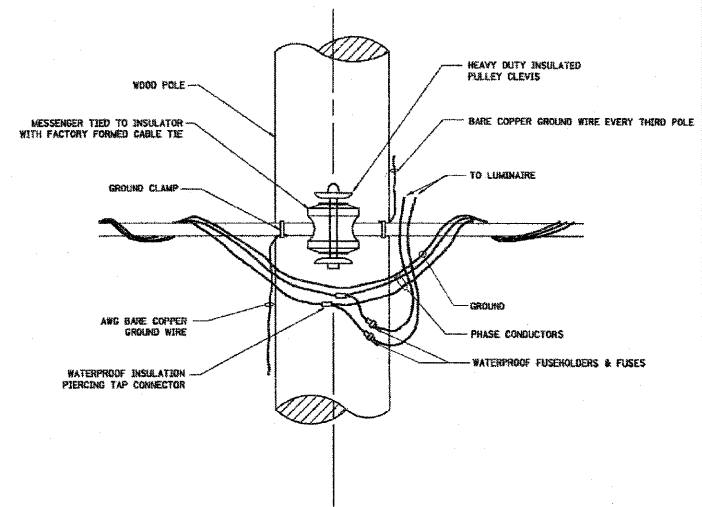
- NOTES:**
- THE LIGHTING CONTROL INSTALLATION FOR THIS AREA IS VILLAGE OF GLEN ELLYN ROOSEVELT ROAD LIGHTING CABINET A LOCATED ON THE NORTH SIDE OF ROOSEVELT ROAD APPROXIMATELY 1000 FEET EAST OF LAMBERT ROAD. ACCESS TO THE CABINET MUST BE COORDINATED WITH THE VILLAGE.
 - THE CONTRACTOR SHALL SCHEDULE A MAINTENANCE TRANSFER OF THE VILLAGE LIGHTING SYSTEM, CIRCUIT A-SW INCLUDING POLES A-SW1 TO A-SW14 AND CIRCUIT A-NW INCLUDING POLES A-NW1 TO A-NW14 PRIOR TO BEGINNING ANY WORK.
 - ALL RACEWAYS SHALL BE INSTALLED AT A DEPTH OF 30" UNLESS OTHERWISE SPECIFIED WITH ELECTRICAL WARNING TAPE INSTALLED AT A DEPTH OF 6-12" BELOW FINISHED GRADE.
 - THE 1 1/2" CONDUIT FOR FUTURE USE SHALL BE INSTALLED ALONG THE BACK OF CURB PRIOR TO BACKFILLING.
 - HANDHOLES SHALL BE USED FOR PULLING CABLE. ALL SPLICES SHALL BE IN THE LIGHT POLE BASES.

FILE NAME = P:\03\03040036\cod\phase 2\dwg\040036-ght-lighting.dwg Feb 01, 2012 at 13:50 Layout: 26-lighting Plan	USER NAME =	DESIGNED -- DAY	REVISED --	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HLR	TEMPORARY STREET LIGHT INSTALLATION STREET LIGHT MODIFICATIONS ILLINOIS ROUTE 38 AND LAMBERT ROAD	F.A.U. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE =	CHECKED -- DAY	REVISED --				2567	05-00069-00-CH	DuPAGE	38	26
PLOT DATE =	DATE -- 7/24/09	REVISED --		SCALE: 1"=20'	SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 63567		

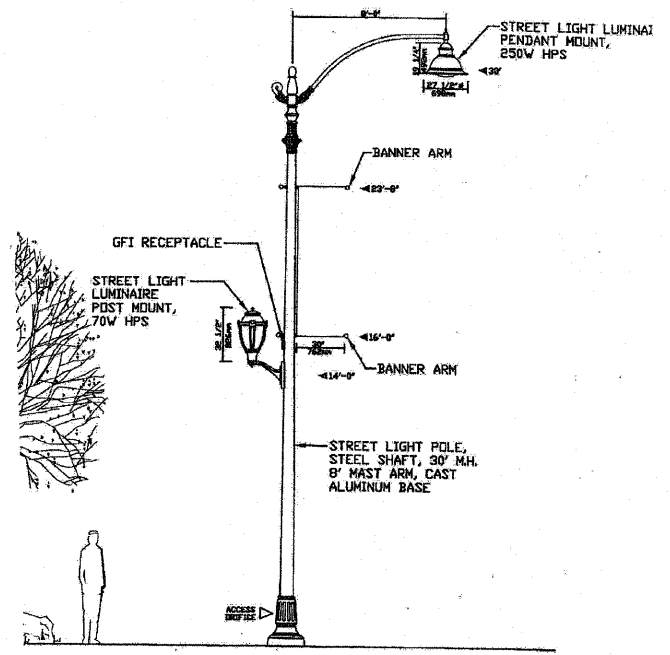
- GENERAL NOTES:**
1. THE CONTRACTOR SHALL VERIFY ALL OF THE INFORMATION SHOWN ON THE CONTRACT DRAWINGS, WHICH WOULD AFFECT THE WORK UNDER THIS CONTRACT.
 2. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ASCERTAIN EXISTING FIELD CONDITIONS BEFORE BIDDING ON THIS PROJECT, SPECIFICALLY AS THEY RELATE TO LUMP SUM ITEMS AND UNIT PRICE ITEMS.
 3. ALL NEW CONDUITS, UNIT DUCTS, DIRECT BURIAL CABLES, AND APPURTENANCES ARE INDICATED DIAGRAMMATICALLY ON THE DRAWINGS. THE ACTUAL LOCATIONS IN THE FIELD SHALL MEET WITH APPROVAL OF THE ENGINEER.
 4. THE ELECTRICAL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE 2007 STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND ASSOCIATED SUPPLEMENTAL CONDITIONS.
 5. THE SCALE SHOWN ON PLAN DRAWINGS APPLIES ONLY TO THE FULL SIZE PLANS AND NOT TO REDUCED SIZE PLANS.
 6. ALL LUMINAIRES SHALL BE ORIENTED WITH THE OPTICS PERPENDICULAR TO THE ROADWAY UNLESS OTHERWISE INDICATED OR DIRECTED BY THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE INSTALL LIGHT STANDARD ITEM. SEPARATE PAYMENT WILL NOT BE MADE.
 7. CONDUITS AND UNIT DUCTS SHALL BE INSTALLED AT A MINIMUM 30" DEPTH BELOW GRADE AND POSITIONED IN THE FIELD TO AVOID CONFLICT WITH ROADWAY UNDERDRAINS AND OTHER EXISTING AND PROPOSED UTILITIES. THE CONTRACTOR SHALL INCREASE DEPTH OF UNIT DUCT AND CONDUIT AS REQUIRED AT NO ADDITIONAL COST. THE CONTRACTOR SHALL COORDINATE RACEWAY DEPTH WITH THE ELECTRICAL DETAILS AND THE ENGINEER.
 8. WHERE THE CONTRACTOR'S EXCAVATION MEETS AN OBSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE ENGINEER FOR DIRECTION IN WRITING PRIOR TO EXCAVATION. THE CONTRACTOR SHALL RESTORE ANY DAMAGE TO EXISTING SYSTEMS OR UTILITIES AND REMOVE EXISTING OBSTRUCTIONS AND FOUNDATIONS TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE APPROPRIATE PAY ITEM.



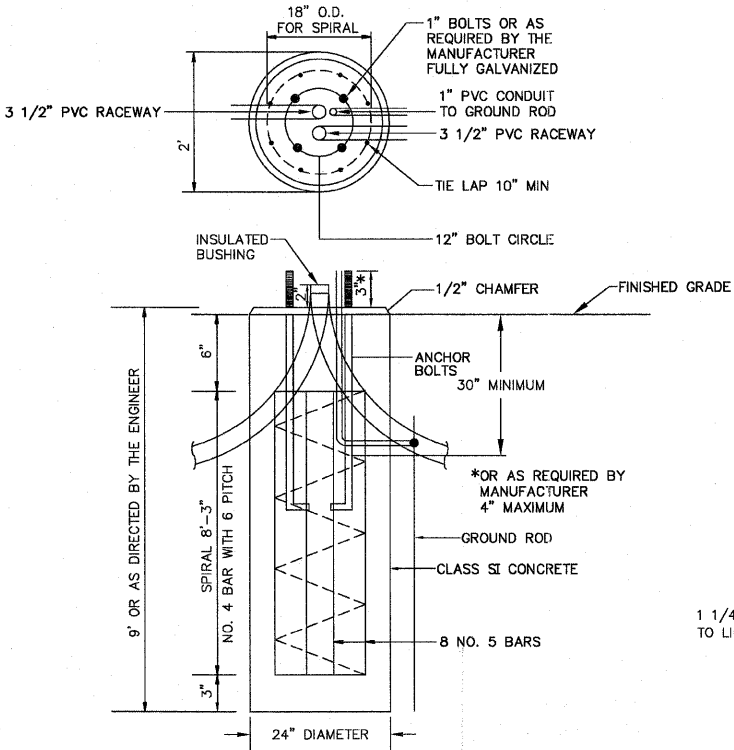
TEMPORARY AERIAL CABLE CONNECTION DETAILS
N.T.S.



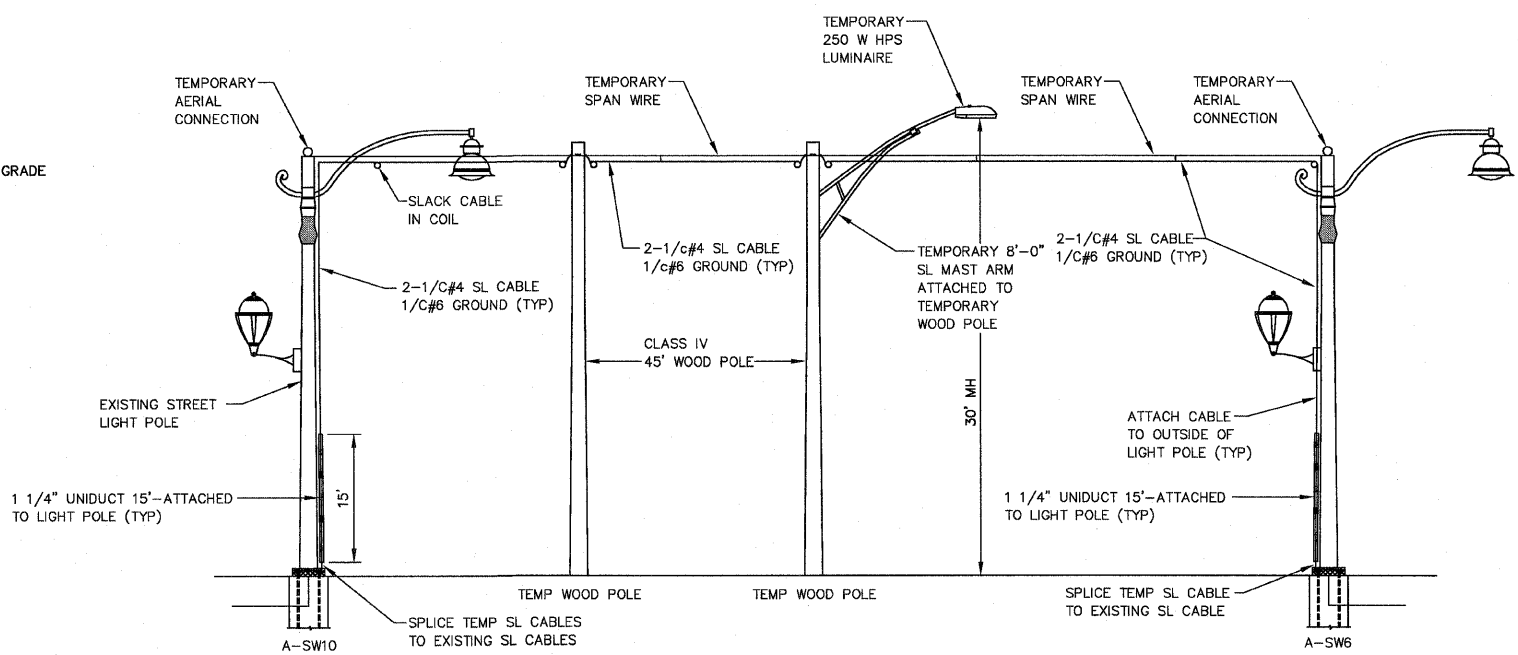
TEMPORARY WOOD POLE ATTACHMENT DETAIL



STREET LIGHT DETAIL 30'
RELOCATE EXISTING LIGHTING UNIT DETAIL
METAL LIGHT POLE, INSTALL ONLY DETAIL

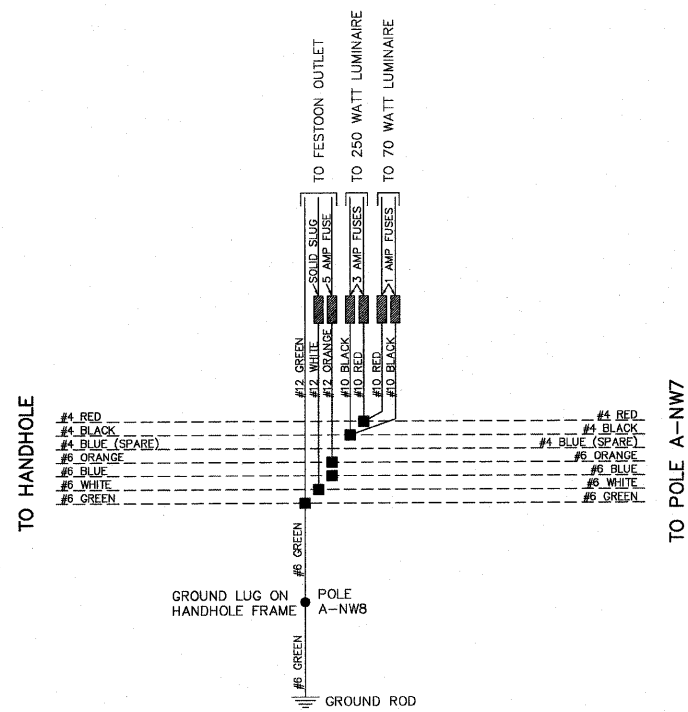


CONCRETE FOUNDATION, 24" DIAMETER DETAIL FOR STREET LIGHT POLE



TEMPORARY STREET LIGHT CONNECTIONS DETAIL
CIRCUIT A-SW

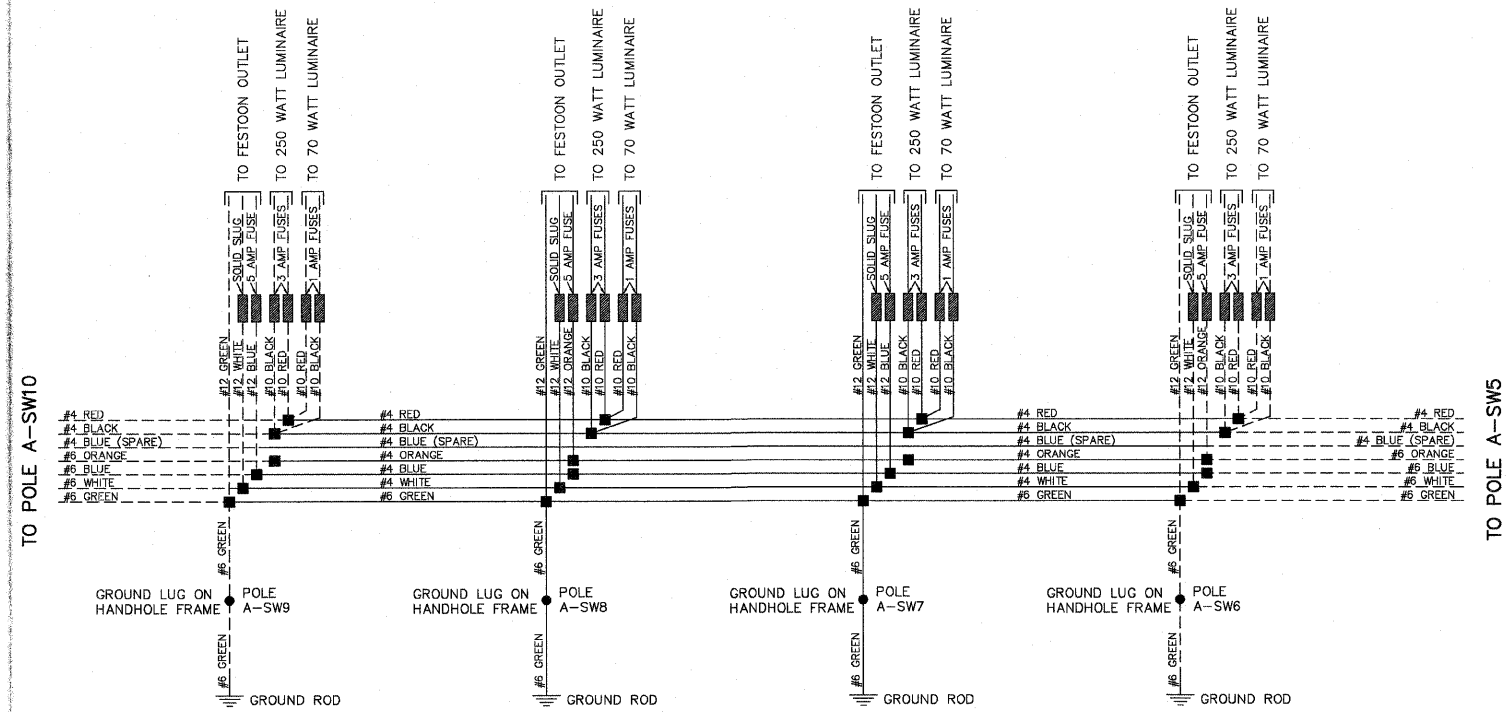
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P:\03\03040036\cad\phase 2\dwg\040036-sht-lighting.dwg		DRAWN - SMS	REVISED -				2567	05-00069-00-CH	DuPAGE	38	27
Feb 01, 2012 at 13:53		CHECKED - DAY	REVISED -				CONTRACT NO. 63567				
Layout: 27-Temporary Details		DATE - 6/29/09	REVISED -				FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



PROPOSED CABLE PLAN
CIRCUIT A-NW

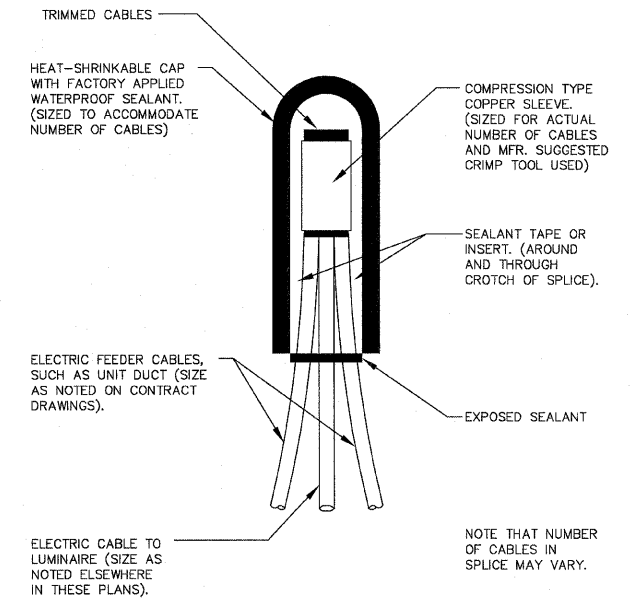
LEGEND

- EXISTING CABLE
- PROPOSED CABLE
- SPLICE



PROPOSED CABLE PLAN
CIRCUIT A-SW

NOTE: TAPE ENDS OF #4 BLUE (SPARE)
CABLE TOGETHER IN EACH POLE.



SPLICING ELECTRIC CABLES BASIC MATERIALS AND METHODS

FILE NAME =	USER NAME =
P:\03\03040036\cod\phase 2\dwg\040036-sht-lighting.dwg	
Aug 28, 2011 at 10:02	
Layout: 28-Wiring Details	

DESIGNED — DAY	REVISED —
DRAWN — SMS	REVISED —
CHECKED — DAY	REVISED —
DATE — 7/24/09	REVISED —

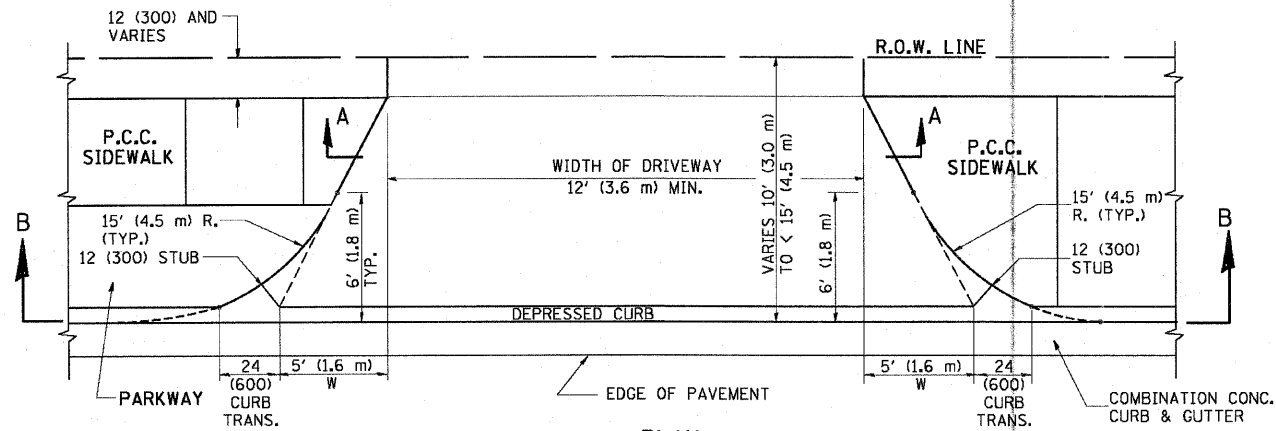
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**



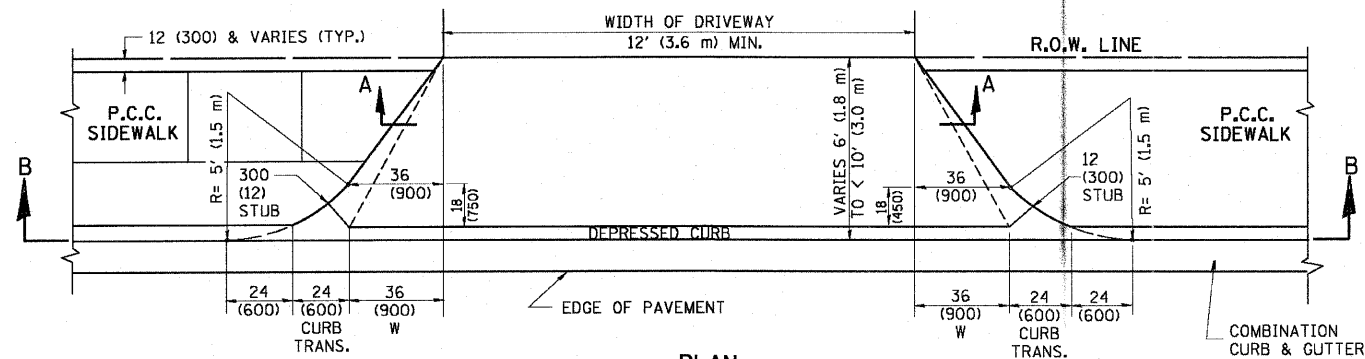
**STREET LIGHTING WIRING DETAILS
ILLINOIS ROUTE 38 AND LAMBERT ROAD**

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

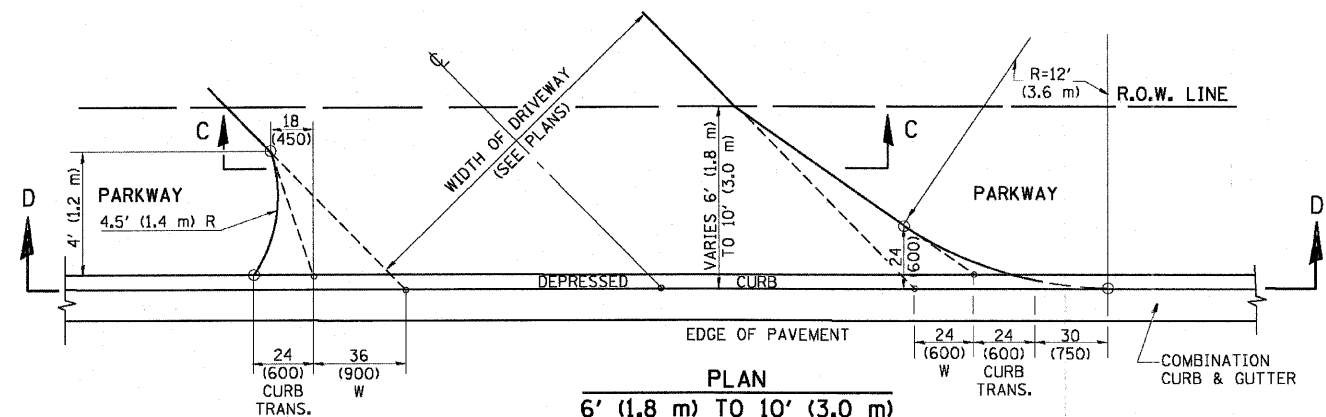
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2567	05-00069-00-CH	DuPAGE	38	28
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63567	



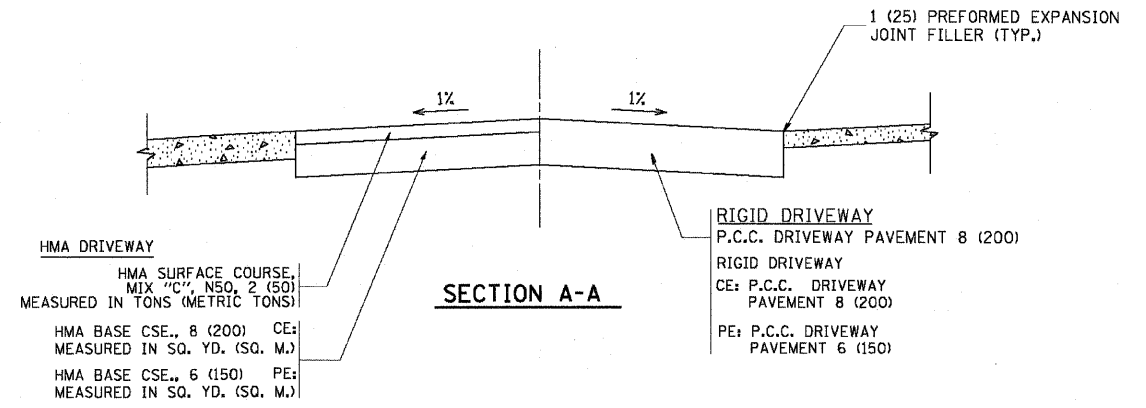
PLAN
 10' (3.0 m) TO < 15' (4.5 m)



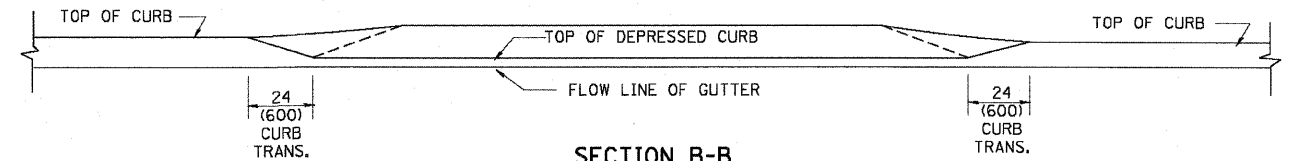
PLAN
 6' (1.8 m) TO < 10' (3.0 m)



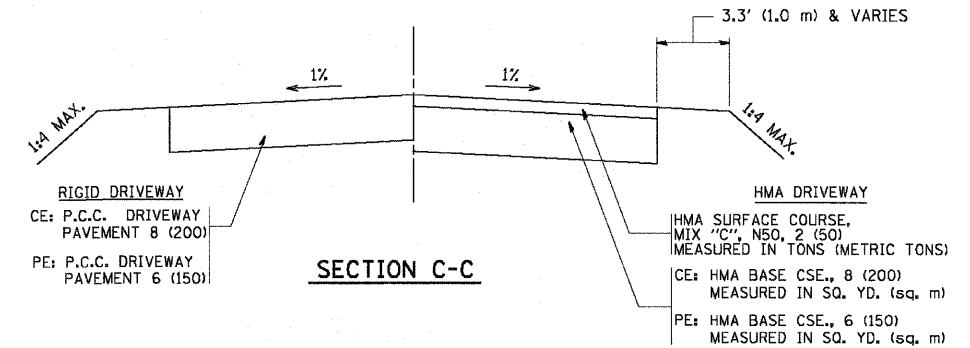
PLAN
 6' (1.8 m) TO 10' (3.0 m)



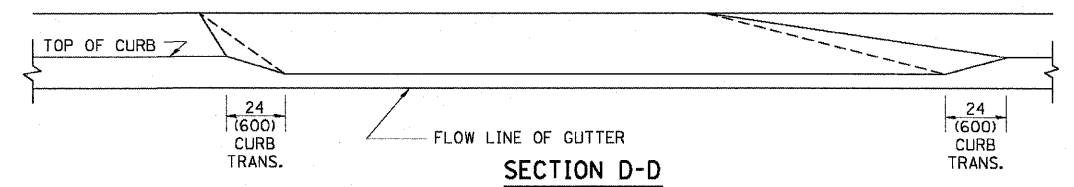
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

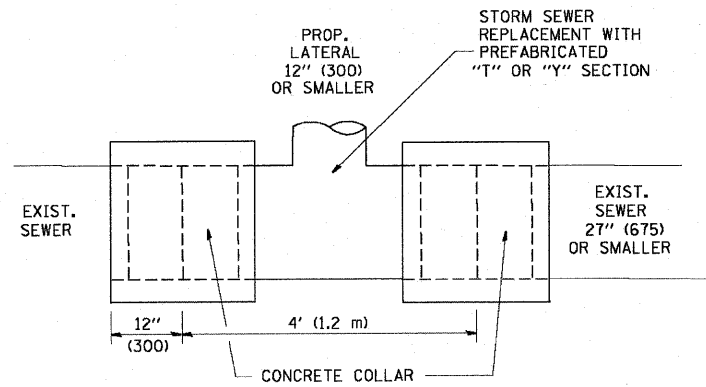
COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

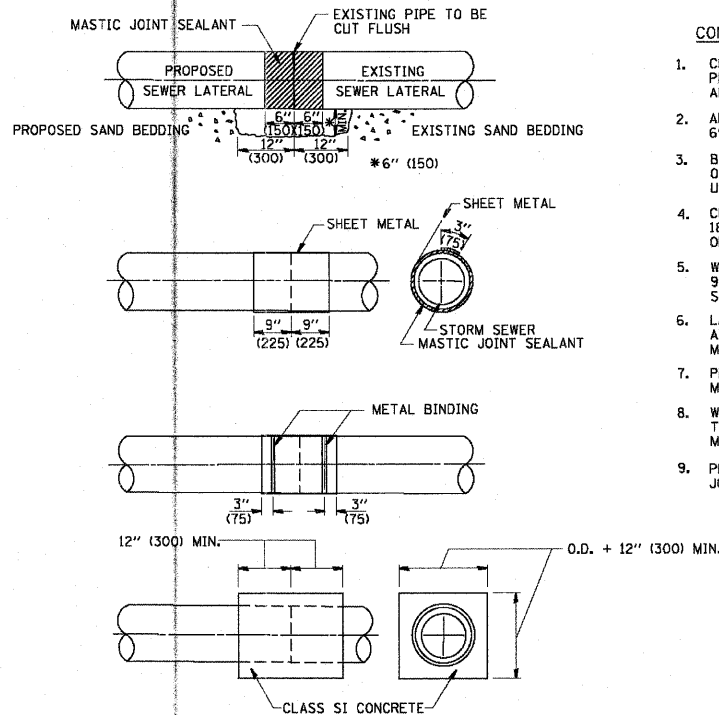
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME = W:\distatd\22x34\bd02.dgn	USER NAME = gaglianob	DESIGNED - R. SHAH	REVISED - T. HOLTZ 04-08-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY DETAILS		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLDT SCALE = 50.0000' / IN.	DRAWN -	REVISED - M. GOMEZ 04-06-01	CHECKED - P. LAFLEUR 04-15-03		DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)		2567	05-00069-00-CH	DuPAGE	42	29
PLDT DATE = 1/4/2008	DATE - 11-06-95	REVISED - R. BORO 01-01-07			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		BD400-02 (BD-02)	
								FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		CONTRACT NO. 63567	



DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER

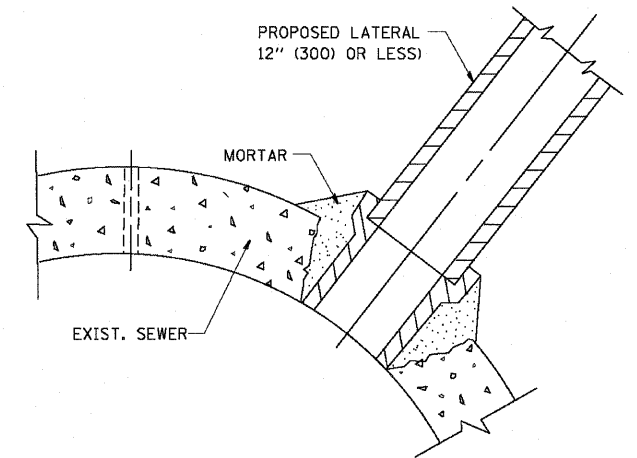


DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12' x 6' (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. CUT A PIECE OF SHEET METAL GAGE NO. 19 I.I (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
5. WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
6. LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
8. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT DOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
9. PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- I. THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- II. CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - A) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - B) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

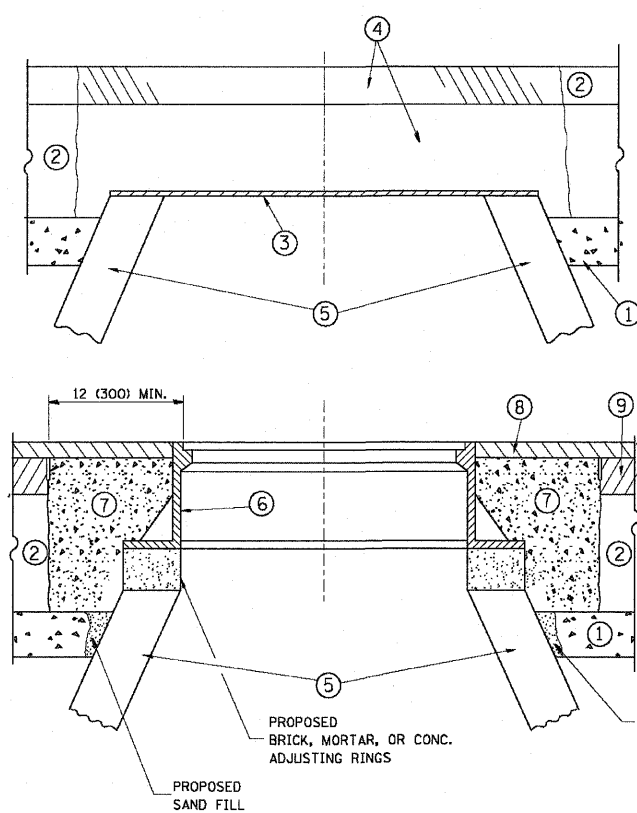
REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\bd07.dgn	USER NAME = gaglianobt	DESIGNED - M. DE YONG	REVISED - M. DE YONG 05-08-92	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	PLOT SCALE = 5/8" = 1' IN.	DRAWN -	REVISED - R. SHAH 09-09-94		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	2567	05-00069-00-CH	DuPAGE	42	30
	PLOT DATE = 1/4/2008	CHECKED -	REVISED - R. SHAH 10-25-94					BD500-01 (BD-7)			CONTRACT NO. 63567		
		DATE - 07-25-90	REVISED - R. SHAH 06-12-96		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT								



- CONSTRUCTION PROCEDURES**
- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
 - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
 - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
 - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.
- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS S1 CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

- LEGEND**
- ① SUB-BASE GRANULAR MATERIAL
 - ② EXISTING PAVEMENT
 - ③ 36 (900) DIAMETER METAL PLATE
 - ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
 - ⑤ EXISTING STRUCTURE
 - ⑥ FRAME AND LID (SEE NOTES)
 - ⑦ CLASS S1 CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
 - ⑧ PROPOSED HMA SURFACE COURSE
 - ⑨ PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"
 NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

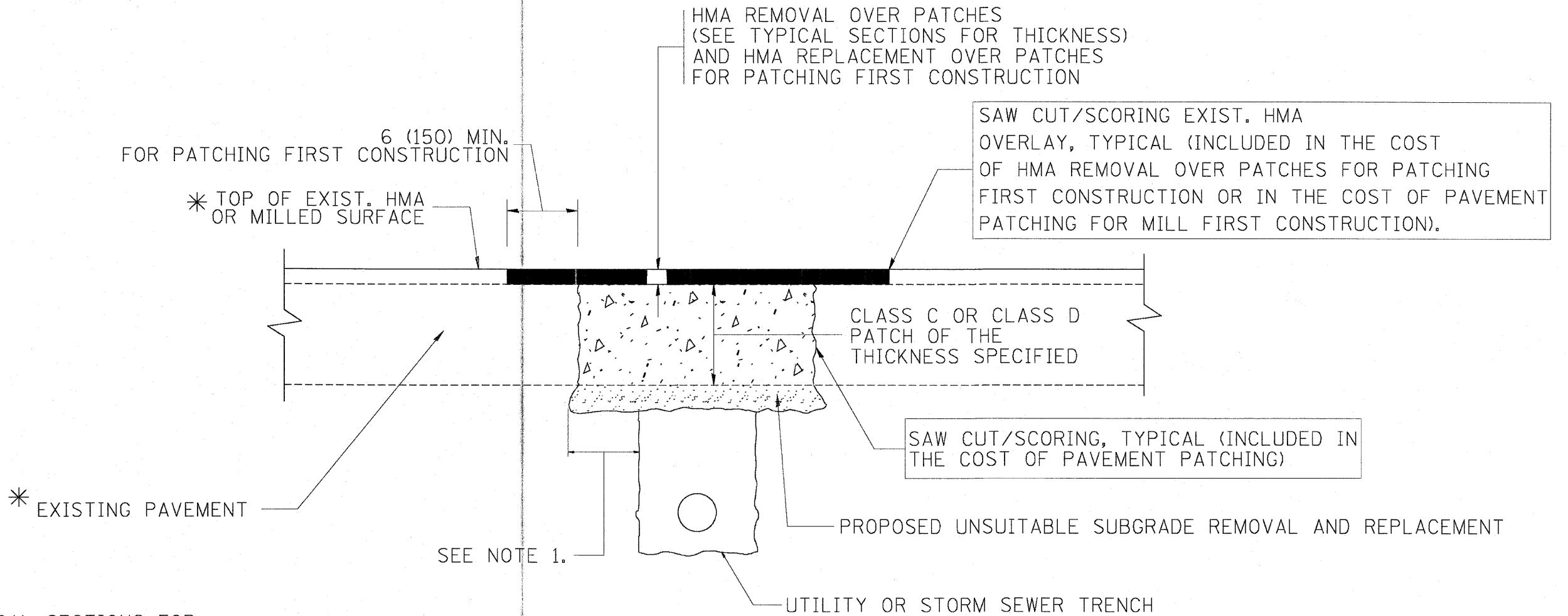
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = gaglienobt	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95
W:\diststd\22x34\bd88.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - R. WIEDEMAN 05-14-04
	PLOT DATE = 1/4/2008	DATE - 10-25-94	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2567	05-00069-00-CH	DuPAGE	42	31
BD600-03 (BD-8)				CONTRACT NO. 63567
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

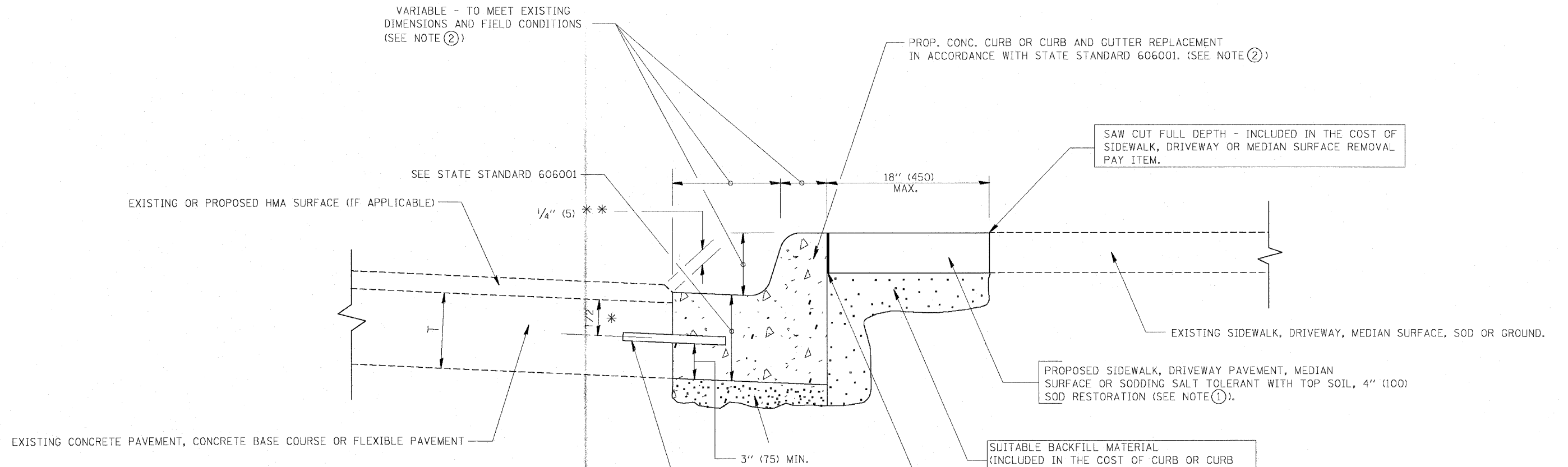
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		DRAWN -	REVISED - R. BORO 01-01-07
	PLDT SCALE = 50.000' / IN.	CHECKED -	REVISED - R. BORO 09-04-07
	PLDT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT PATCHING FOR
 HMA SURFACED PAVEMENT**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2567	05-00069-00-CH	DuPAGE	42	32
BD400-04 (BD-22)			CONTRACT NO. 63567	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
 * * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
 SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

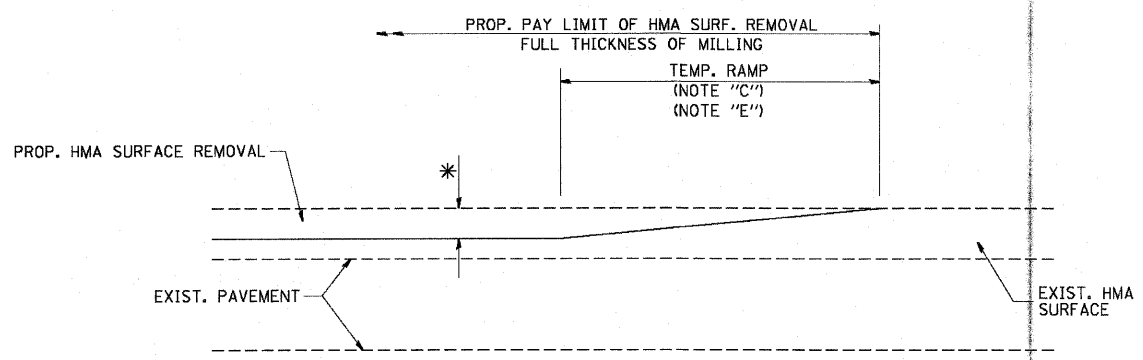
- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

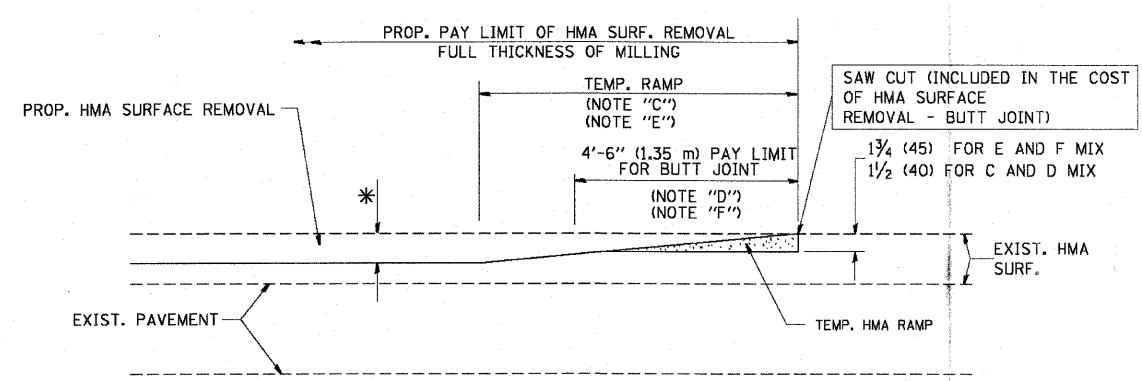
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drsvakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pv_work\p\dot\drsvakosgn\08180315\bc24.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97	2567			05-00069-00-CH	DuPAGE	42	33	
PLOT SCALE = 5/8"=1'-0"	CHECKED -	REVISED - M. GOMEZ 01-22-01	BD800-06 (BD-24)			CONTRACT NO. 63567				
PLOT DATE = 12/15/2009	DATE - 03-11-94	REVISED - R. BORO 12-15-09	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.			



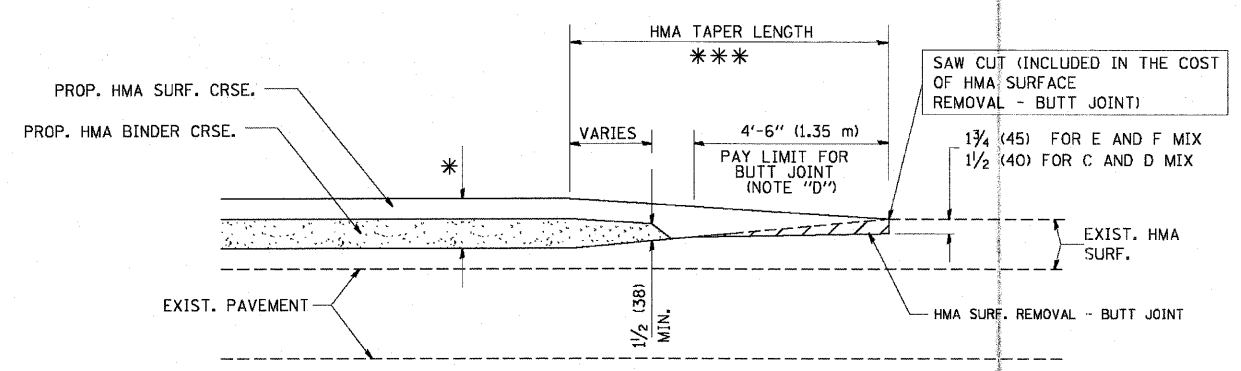
MILLED TEMPORARY RAMP
 (FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1



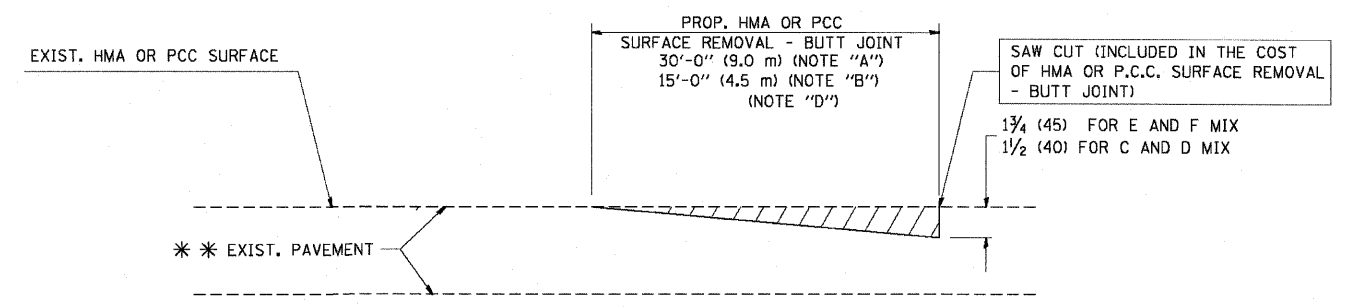
HMA CONSTRUCTED TEMPORARY RAMP
 (FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2
TYPICAL TEMPORARY RAMP

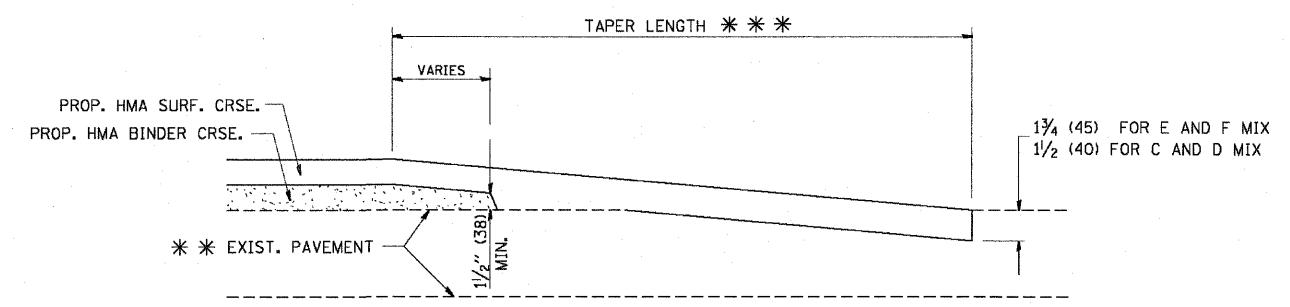


BUTT JOINT AND
 HMA TAPER

**TYPICAL BUTT JOINT AND HMA TAPER
 FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
 FOR RESURFACING ONLY**

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

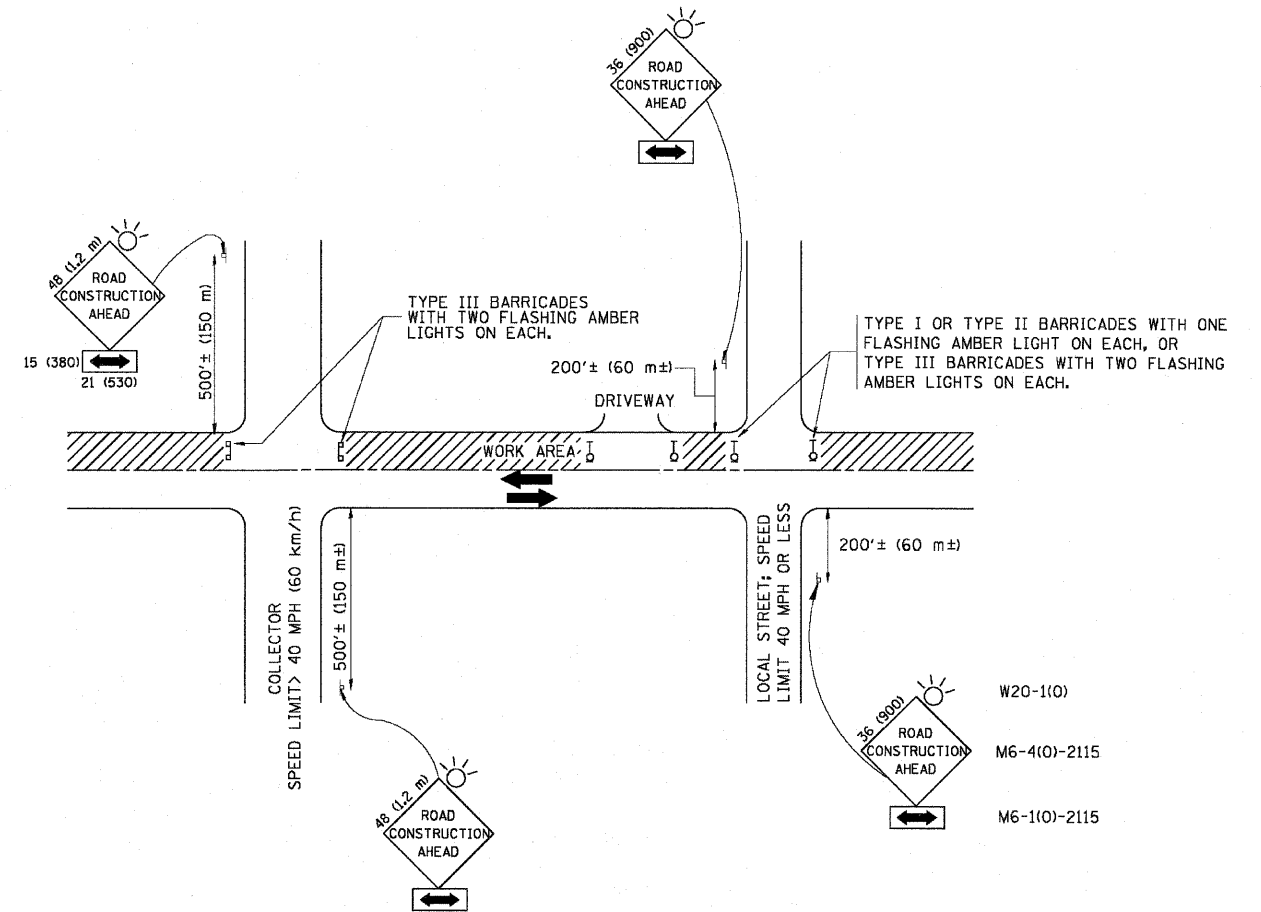
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\dststd\22x34\bd32.dgn	USER NAME = gqglentobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2567	05-00069-00-CH	DuPAGE	42	34
BD400-05 BD32			CONTRACT NO. 63567	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

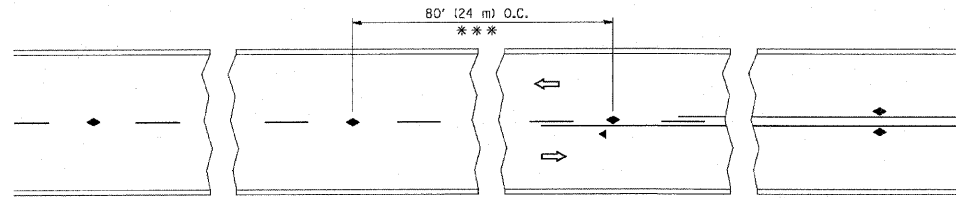
All dimensions are in millimeters (inches) unless otherwise shown.

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		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

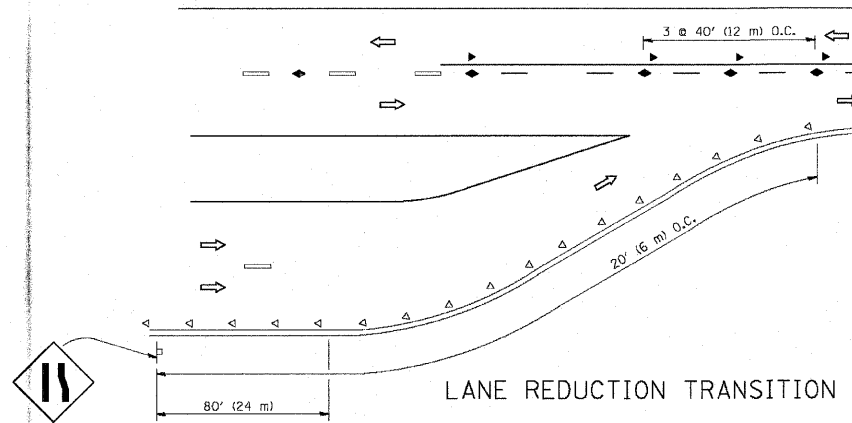
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2567	05-00069-00-CH	DuPAGE	42	36
TC-10			CONTRACT NO. 63567	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				

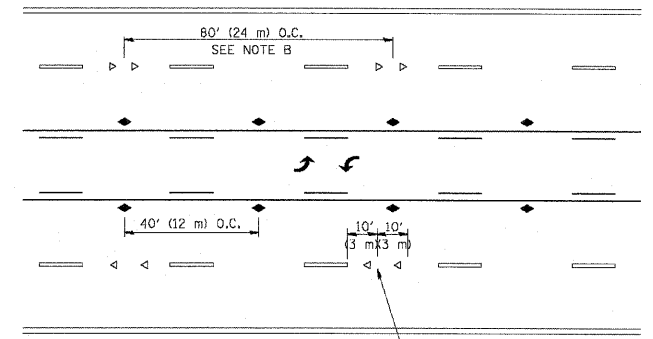


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 Km/h) OR LESS.

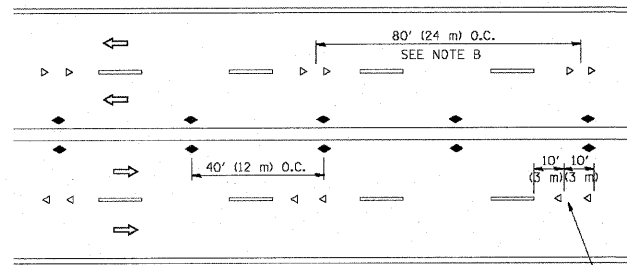
TWO-LANE/TWO-WAY



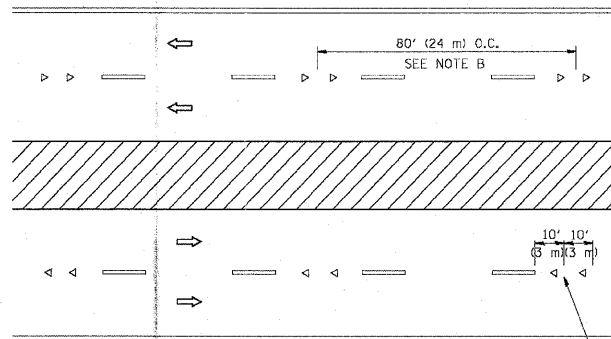
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

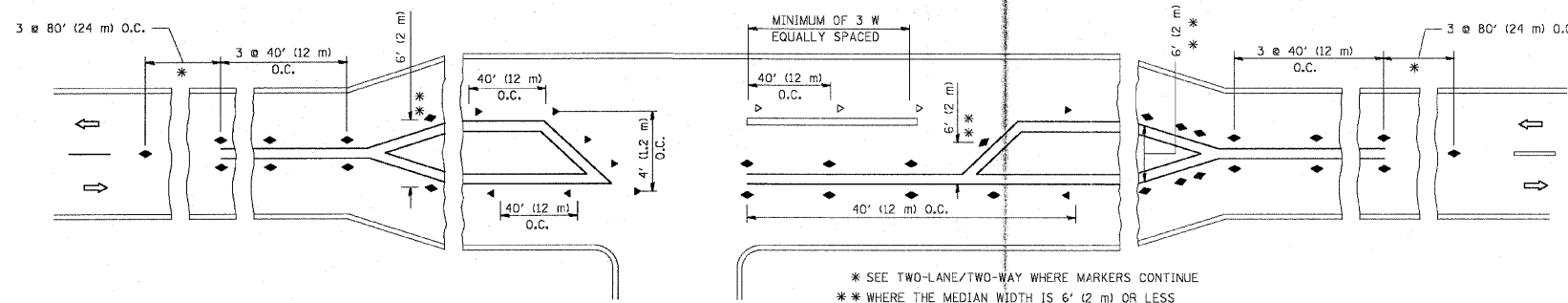
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 Km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

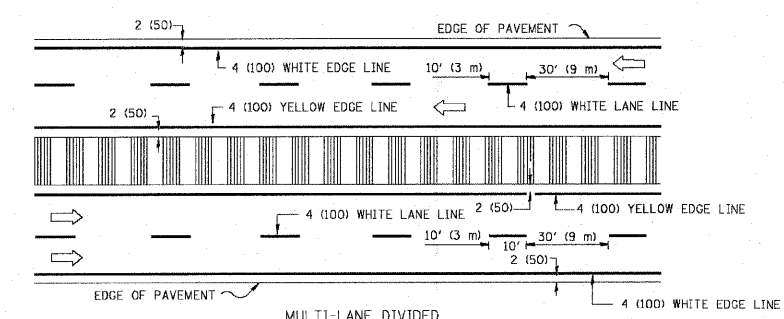
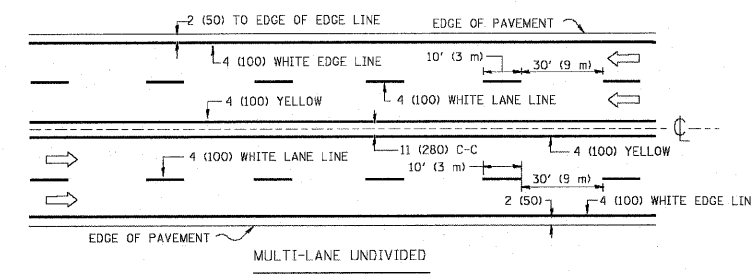
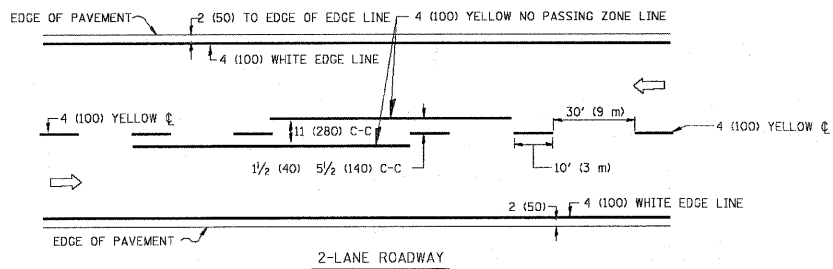


LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

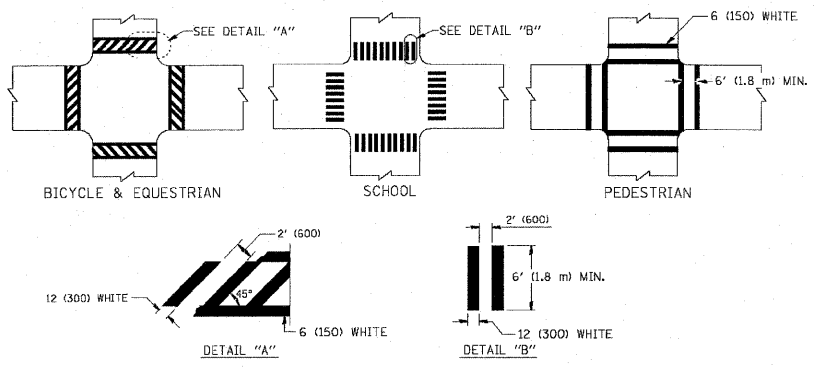
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = drvakosgn	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cr:\pw\work\pwwork\drvakosgn\08180315\to1.dgn		DRAWN -	REVISED - T. RAMMACHER 03-12-99		2567	05-00069-00-CH	DuPAGE	42	37			
PLOT SCALE = 80.000 ' / IN.		CHECKED -	REVISED - T. RAMMACHER 01-06-00		TC-11				CONTRACT NO. 63567			
PLOT DATE = 9/9/2009		DATE -	REVISED - C. JUCIUS 09-09-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

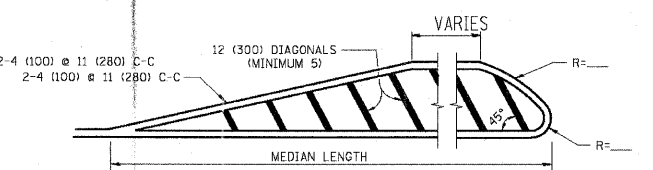
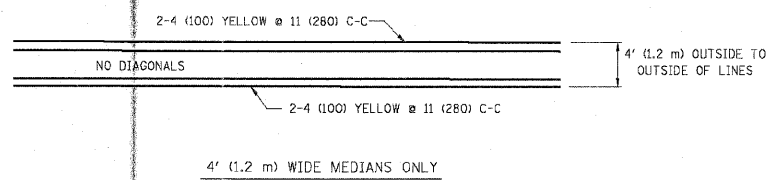


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

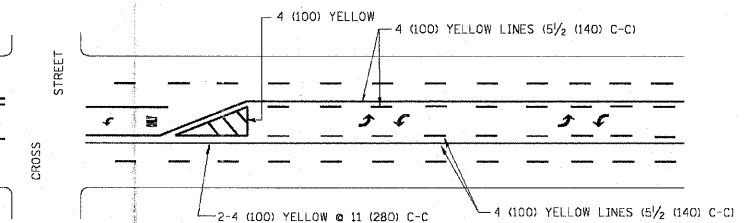


TYPICAL CROSSWALK MARKING

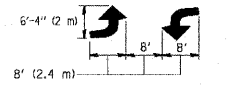


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h))
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

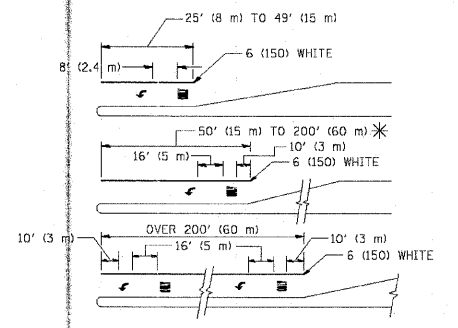


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

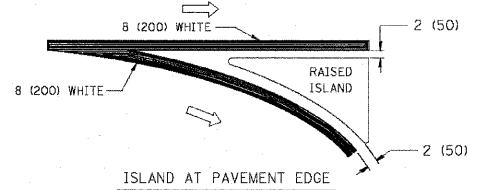
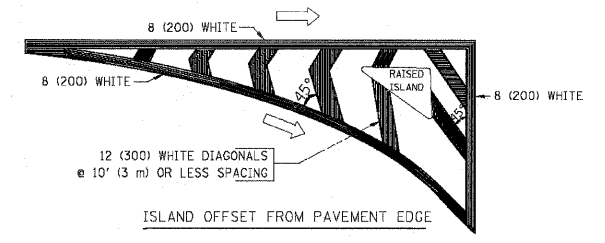


FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.
 * AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

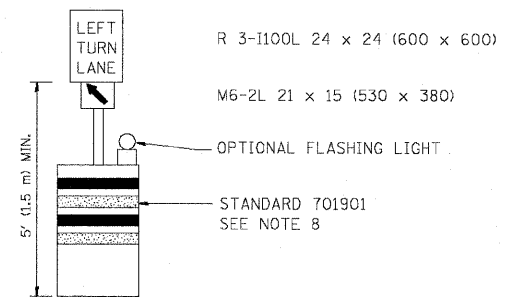
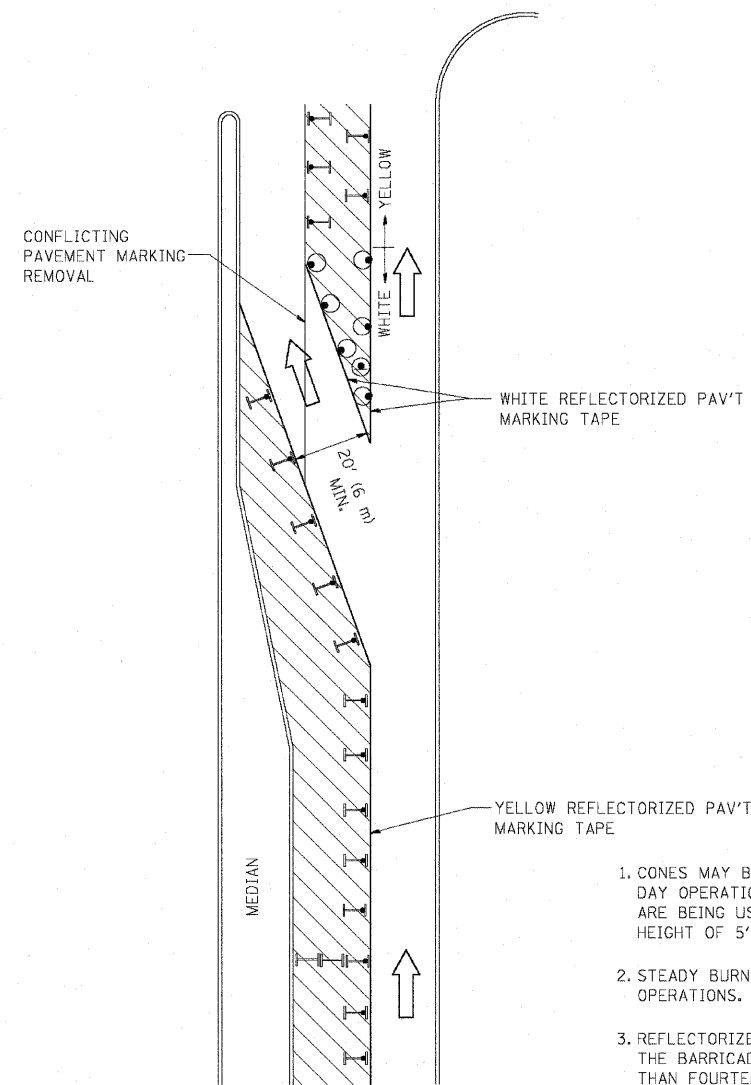
All dimensions are in inches (millimeters) unless otherwise shown.

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PLOT SCALE = 50.200' / IN.		CHECKED -	REVISED -
PLOT DATE = 9/9/2009		DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DISTRICT ONE		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TYPICAL PAVEMENT MARKINGS		2567	05-00069-00-CH	DuPAGE	42	38
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA.	TO STA.	

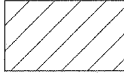





CONTRACT NO. 63567		ILLINOIS FED. AID PROJECT	
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GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in inches (millimeters) unless otherwise shown.

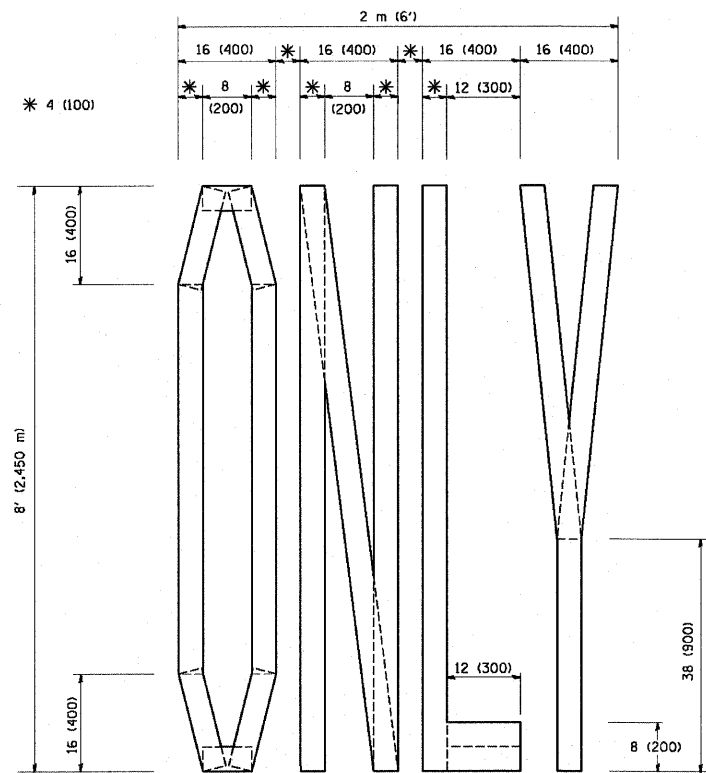
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PLOT SCALE = 49.9999' / IN.		REVISED - A. HOUSEH 10-12-96	REVISED -
PLOT DATE = 9/14/2009		REVISED - T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

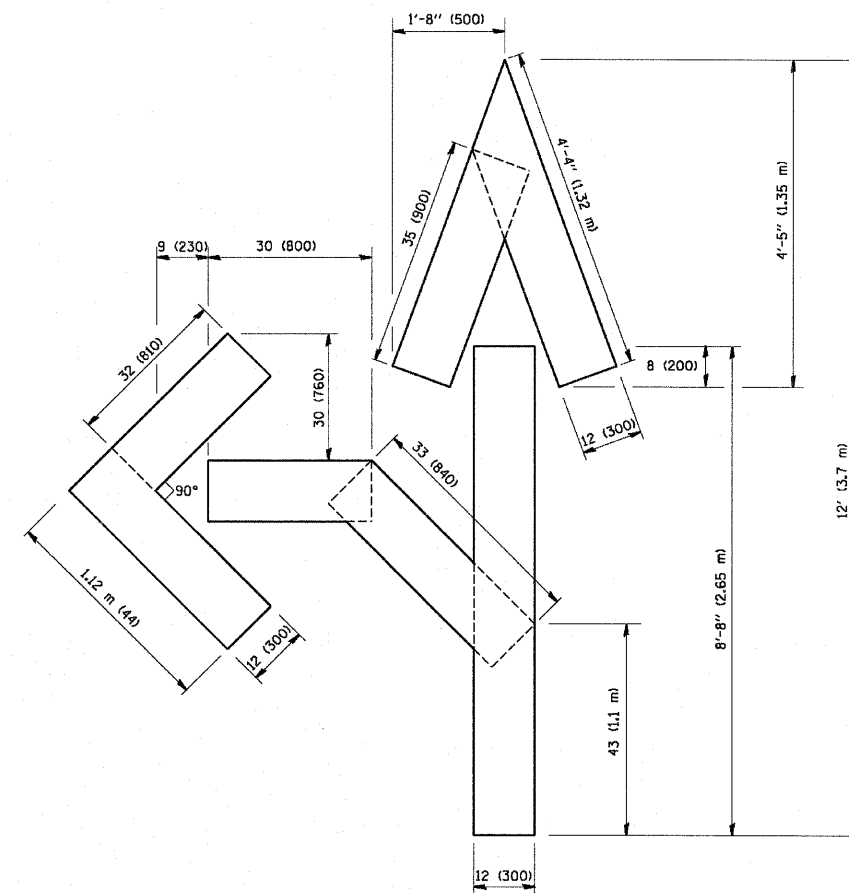
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
 (TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

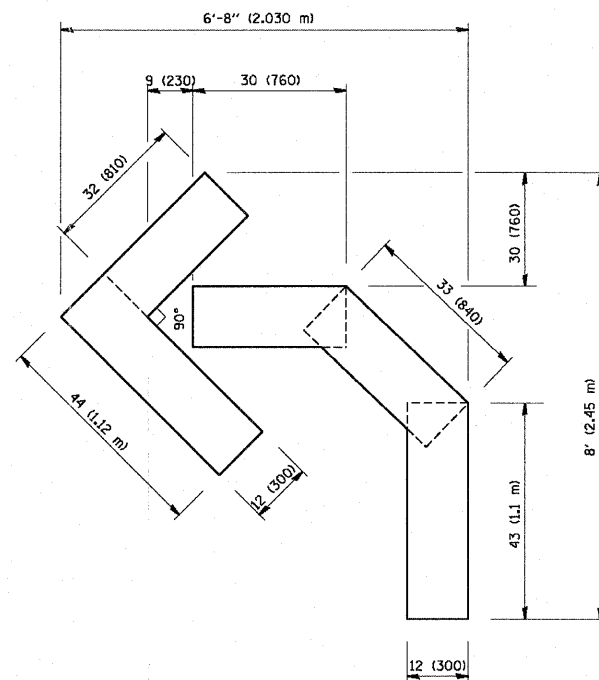
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2567	05-00069-00-CH	DuPAGE	42	39
TC-14				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
			CONTRACT NO. 63567	



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in Inches (millimeters) unless otherwise shown.

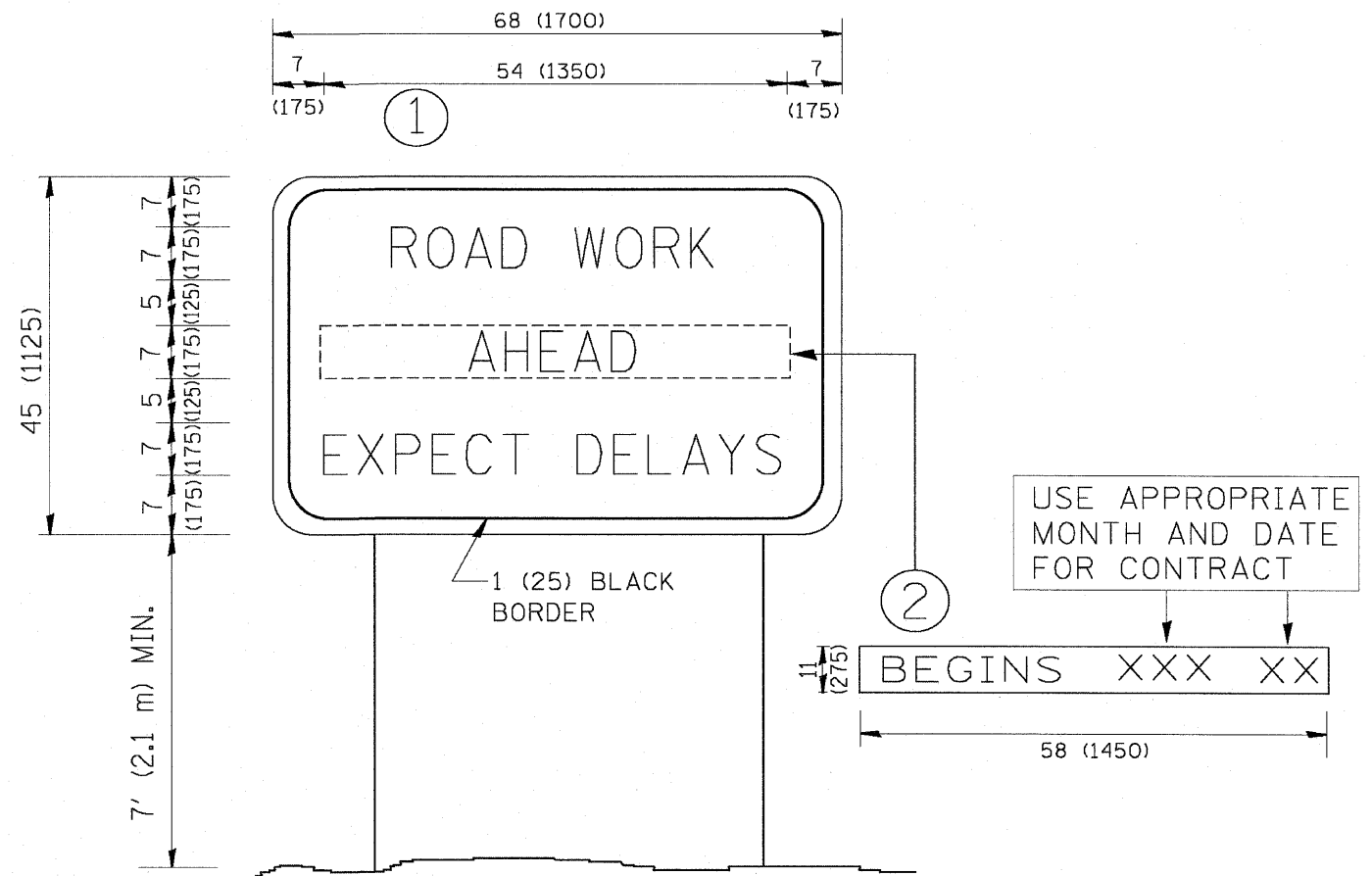
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		DRAWN -	REVISED - T. RAMMACHER 11-04-97
	PLLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - T. RAMMACHER 03-02-98
	PLLOT DATE = 1/4/2008	DATE = 09-18-94	REVISED - E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2567	05-00069-00-CH	DuPAGE	42	40
TC-16				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63567	



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

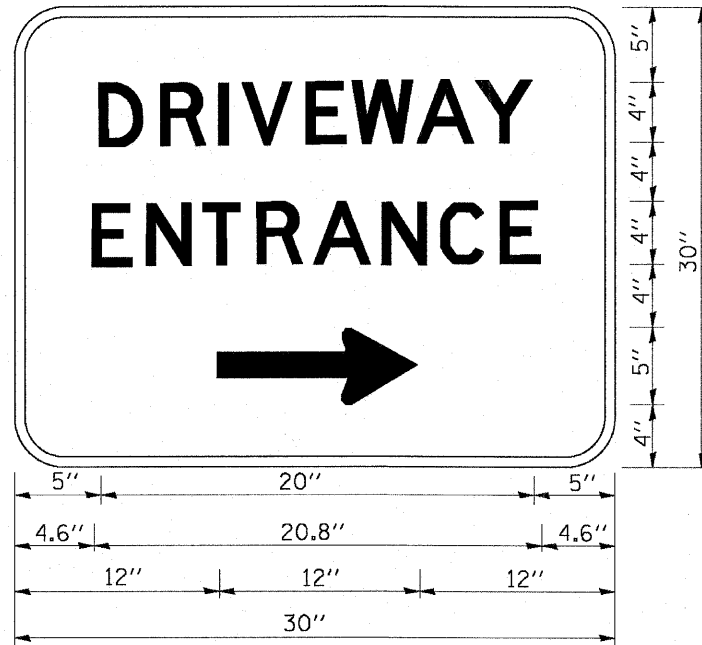
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - R. MIRS 12-11-97
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	PLLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ARTERIAL ROAD INFORMATION SIGN	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 2567	SECTION 05-00069-00-CH	COUNTY DuPAGE	TOTAL SHEETS 42	SHEET NO. 41
TC-22			CONTRACT NO. 63567	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
 PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN)
 SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
 AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
 FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME = W:\data\td\22x31\to26.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED - C. JUCIUS 02-15-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY ENTRANCE SIGNING				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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		DATE -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1	SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			