

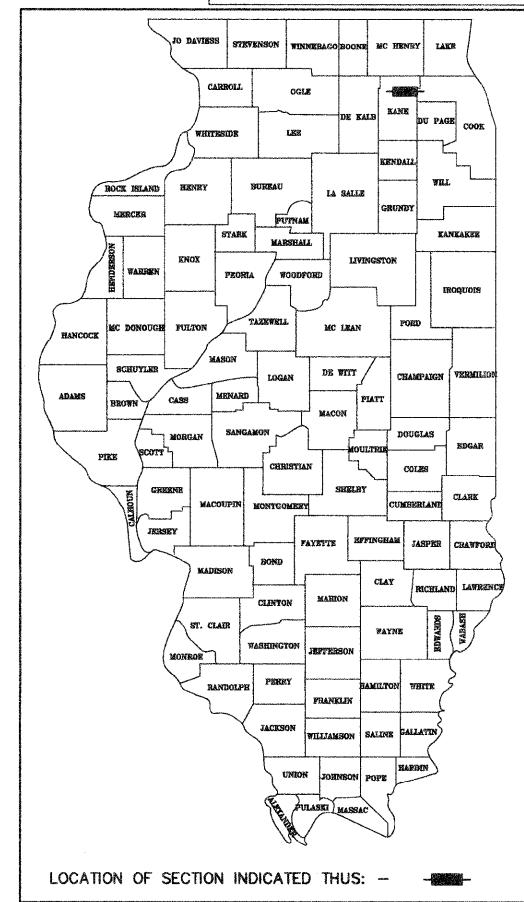
PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLER, P.E. (847) 705-4106 SCHAUMBURG, IL

FOR INDEX OF SHEETS, SEE SHEET NO.2

# STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS PLANS FOR PROPOSED FEDERAL AID PROJECT

**FAU 1314 (KIMBALL STREET)  
FAU 3887 (IL ROUTE 31, STATE STREET)  
TO FAU 1320 (DUNDEE AVENUE)  
RESURFACING  
SECTION NO. 11-00182-00-RS  
PROJECT NO. M-9003(847)  
CITY OF ELGIN  
KANE COUNTY  
JOB NO. C-91-642-11**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1314	11-00182-00-RS	KANE	25	1
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT - LAPO	
CONTRACT NO. 63689				

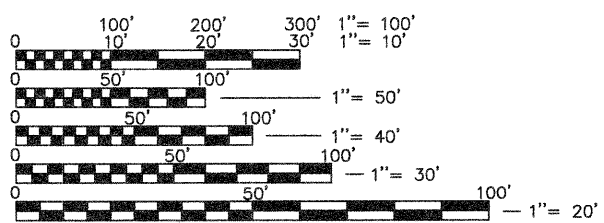


PROJECT LOCATED IN THE CITY OF ELGIN

**DESIGN DESIGNATION**  
KIMBALL STREET: MINOR ARTERIAL (URBAN)  
DESIGN SPEED = 30 M.P.H.  
POSTED SPEED = 30 M.P.H.  
ADT(2010) = 23,500 VPD

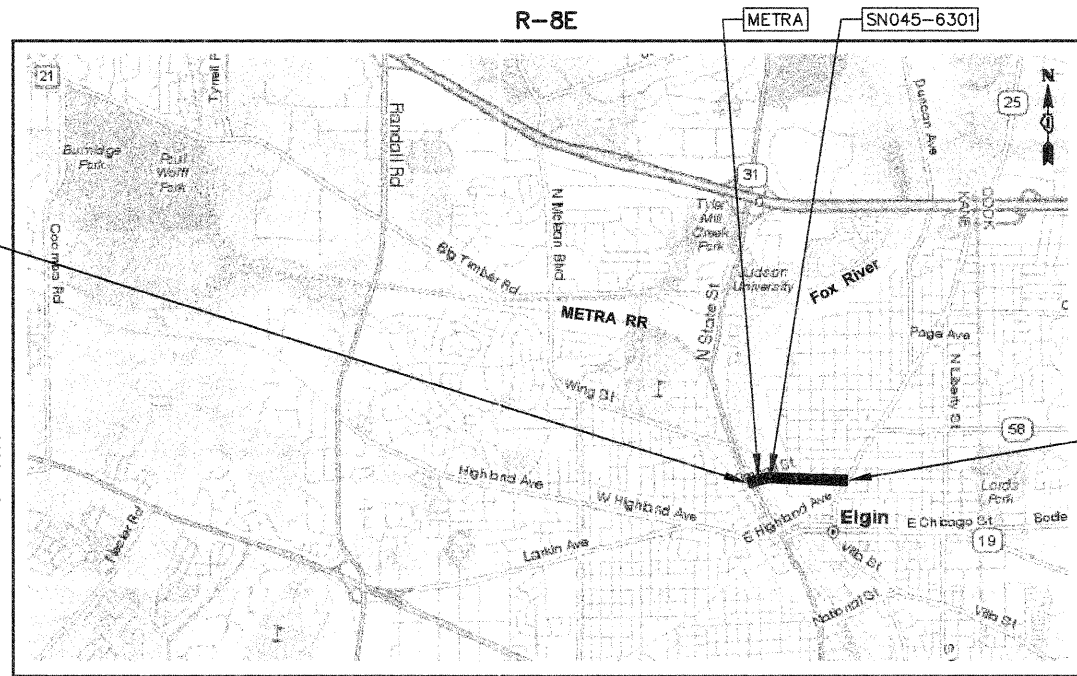
BENCHMARK #1: ALUMINUM CAP  
NW CORNER OF KIMBALL AND  
SPRING STREETS  
ELEV = 729.10

CONSULTING ENGINEER: ENGINEERING ENTERPRISES, INC.  
CONTACT: JASON M. BAUER (630)-466-6700



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

BEGIN IMPROVEMENTS  
STA 68+05  
KIMBALL ST.



END IMPROVEMENTS  
STA 98+68  
KIMBALL ST.

NE 1/4 SEC 14, NW 1/4 SEC 13, T-41N, R-8E, 3RD P.M., ELGIN TOWNSHIP

**LOCATION MAP**  
N.T.S.

NET LENGTH OF PROJECT = 3,063 FEET (0.58 MILES)  
GROSS LENGTH OF PROJECT = 3,063 FEET (0.58 MILES)

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS	
APPROVED	<i>February 6, 2012</i> <i>John B. ...</i> CITY OF ELGIN, CITY ENGINEER
PASSED	<i>FEBRUARY 10, 2012</i> <i>C. Holt</i> DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS
RELEASING FOR BID BASED ON LIMITED REVIEW	<i>FEBRUARY 14, 2012</i> <i>Diane M. O'Neil</i> DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

DATE: *February 6th 2012*

BY: *[Signature]*  
JASON M. BAUER

LICENSE EXPIRES: NOVEMBER 30, 2013

SEAL

**JULIE JOINT UTILITY**  
LOCATION INFORMATION FOR EXCAVATION  
CALL 811

Know what's below.  
Call before you dig.

CONTRACT 63689

**Engineering Enterprises, Inc.**  
CONSULTING ENGINEERS  
52 Wheeler Road  
Sugar Grove, Illinois 60554  
P: 630.466.6700 - W: www.eeiweb.com

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**GENERAL NOTES**

- STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION ADOPTED JAN. 1, 2012, SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIFICATIONS AND PROVISIONS ADOPTED JAN. 1, 2012, AND REVISIONS THERETO, THESE IMPROVEMENT PLANS, DETAILS, SPECIAL PROVISIONS, CODES AND ORDINANCES OF THE CITY OF ELGIN, ILLINOIS SHALL GOVERN APPLICABLE PORTIONS OF THIS PROJECT.
- LOCATIONS OF UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE ONLY AND ARE NOT NECESSARILY COMPLETE. EXISTING AND PUBLIC UTILITIES ARE SHOWN ON THE PLANS, ACCORDING TO INFORMATION OBTAINED FROM UTILITY COMPANIES, MUNICIPALITIES AND SURVEYS.
- THE CONTRACTOR SHALL PROTECT AND PRESERVE ALL SECTION OR SUBSECTION SURVEY MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE OWNER, HIS/HER AGENT OR AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- ALL ELEVATIONS ARE BASED ON U.S.G.S. DATUM.
- THE CONTRACT UNIT PRICE FOR ALL REMOVAL PAY ITEMS SHALL INCLUDE ANY REQUIRED SAW CUTS.
- CONTRACTOR SHALL PROVIDE AND INSTALL A MINIMUM OF TWO WEIGHTED SAND BAGS ON EACH TYPE I OR TYPE II BARRICADE USED (ONE WEIGHTED SAND BAG ACROSS EACH BOTTOM RAIL). THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE TRAFFIC CONTROL AND PROTECTION ITEMS.
- BEFORE STARTING ALL EXCAVATIONS, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT 811 FOR FIELD LOCATIONS OF BURIED UTILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
- 10 FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB & GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURB & GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN IN THE PLANS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- UNLESS OTHERWISE SPECIFIED, BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE THE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS.
- IN ACCORDANCE WITH ARTICLE 107.15, WHENEVER CONSTRUCTION DEBRIS OR LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS.
- IF, DURING CONSTRUCTION, THE CONTRACTOR ENCOUNTERS OR OTHERWISE BECOMES AWARE OF ANY SEWER, UNDER DRAINS OR FIELD DRAINS WITHIN THE RIGHT-OF-WAY, OTHER THAN THOSE SHOWN ON THE PLANS, HE/SHE SHALL INFORM THE ENGINEER, WHO SHALL DIRECT THE WORK NECESSARY TO MAINTAIN OR TO REPLACE THE FACILITIES IN SERVICE AND TO PROTECT THEM FROM DAMAGE. EXISTING FACILITIES TO BE MAINTAINED THAT ARE DAMAGED BECAUSE OF NON-COMPLIANCE WITH THIS PROVISION SHALL BE REPLACED AT THE CONTRACTORS OWN EXPENSE.
- THE RESIDENT ENGINEER SHALL CONTACT THE IDOT DISTRICT 1 TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TRAFFIC CONTROL DEVICES.
- IT IS THE CONTRACTORS SOLE RESPONSIBILITY TO COORDINATE WITH METRA WHENEVER CONSTRUCTION ACTIVITIES ARE WITHIN 50 FEET OF RAILROAD ROW. THE CONTRACTOR SHALL RETAIN FLAGMEN EMPLOYED AND DESIGNATED BY METRA TO MONITOR ON-COMING TRAIN TRAFFIC AND ADVISE CONTRACTOR PERSONNEL WHEN ACTIVITY ON OR NEAR THE RAILROAD ROW MAY PROCEED. THIS ITEM WILL BE PAID FOR ACCORDING TO ARTICLE 107.12 AND WILL BE REIMBURSED ACCORDING TO ARTICLE 109.05.
- ALL WORK PERFORMED RELATIVE TO THIS IMPROVEMENT SHALL COMPLY WITH ALL APPLICABLE RULES AND REGULATIONS OF O.S.H.A.

- KIMBALL STREET SHALL BE OPEN TO TRAFFIC AT ALL TIMES. WHEN IT IS NECESSARY TO CLOSE ONE LANE OF TRAFFIC DUE TO CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN AT LEAST ONE LANE OF TRAFFIC IN EACH DIRECTION DURING CONSTRUCTION HOURS WITH THE USE OF TRAFFIC CONTROL DEVICES, SIGNS AND FLAGGERS AS APPLICABLE IN THE TRAFFIC CONTROL STANDARDS.
- THE KIMBALL STREET BRIDGE SHOULD REMAIN OPEN AT ALL TIMES THROUGHOUT THE PROJECT. THE SUGGESTED STAGING OF THE WORK ON THE BRIDGE IS TO CLOSE THE OUTSIDE TWO LANES FIRST AND THEN MOVE TO THE INSIDE LANES. FINAL STAGING AS PROPOSED BY THE CONTRACTOR SHALL BE SUBJECT TO THE APPROVAL OF THE CITY.
- ANY INLET TO BE ADJUSTED SHALL HAVE ALL RINGS REMOVED AND DETERIORATED RINGS SHALL BE REPLACED. BUTYL ROPE SHALL BE USED WHEN RESETTING THE RINGS AND PRIOR TO ANY MORTAR REPAIR. ALL ADJUSTING RINGS, STRUCTURES AND PIPE ENTRANCES SHALL BE MORTARED (FROM BOTH THE INSIDE AND THE OUTSIDE AS NECESSARY) TO CORRECT ANY EXISTING INFILTRATION. THE INLET SHALL BE ADJUSTED TO GRADE AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT ANY RESIDENT OR BUSINESS OF ANY CONCRETE CURB AND GUTTER OR SIDEWALK REMOVAL AND REPLACEMENT THAT WILL INHIBIT OR PROHIBIT ACCESS TO THEIR DRIVEWAY, IN WRITING, A MINIMUM OF 48 HOURS BUT NOT MORE THAN 72 HOURS, PRIOR TO THE COMMENCEMENT OF THESE ACTIVITIES. THE MORNING OF THE WORK, THE CONTRACTOR SHALL AGAIN NOTIFY THE OWNER VERBALLY, TO ALLOW THE OWNER TIME TO MOVE THEIR VEHICLE SO AS NOT TO PROHIBIT THE VEHICLE FROM LEAVING THE DRIVEWAY UPON REMOVAL OF ANY MATERIAL. THE NOTICE GIVEN OUT BY THE CONTRACTOR SHOULD PROVIDE INFORMATION REGARDING THE ANTICIPATED DATE THAT FULL ACCESS WILL BE RESTORED. COORDINATION BETWEEN ACTIVITIES SHOULD ALLOW ALL WORK TO BE DONE IN A TIMELY MATTER SO AS TO PERMIT ACCESS TO THE ROADWAY. ANY ADDITIONAL COST OF STAGING REQUIRED TO MAINTAIN TO ACCESS IS CONSIDERED INCLUDED IN THE COST OF THE SPECIFIED PAY ITEMS.
- SOME LOCATIONS OF CURB AND GUTTER AND SIDEWALK MAY HAVE BEEN POURED MONOLITHICALLY WHEN CONSTRUCTED. AT EACH LOCATION WHERE THESE ITEMS ARE REMOVED, A FULL DEPTH SAWCUT SHALL BE CONSTRUCTED AT THE LIMITS OF REMOVAL. IF ADDITIONAL CONCRETE IS DAMAGED, IT SHALL BE REPLACED AT THE CONTRACTOR'S COST.
- DETECTABLE WARNINGS SHALL BE CHARCOAL COLORED ARMOR - TILE OR APPROVED EQUAL DETECTABLE WARNING PANELS. REPLACEMENT CONCRETE AROUND DETECTABLE WARNING PANELS SHALL BE RED COLORED CONCRETE AS DIRECTED BY THE ENGINEER. COLORING WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE ITEMS OF WORK SPECIFIED.

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2.	GENERAL NOTES, HIGHWAY STANDARDS, AND INDEX OF SHEETS
3.	SUMMARY OF QUANTITIES
4-7.	TYPICAL SECTIONS
8.	PLAN, CONSTRUCTION NOTES, AND DETAILS
9.	PLAN, CONSTRUCTION NOTES, AND DETAILS
10.	PLAN, CONSTRUCTION NOTES, AND DETAILS
11.	PLAN, CONSTRUCTION NOTES, AND DETAILS
12.	STRIPING AND DETECTOR LOOP PLANS
13.	STRIPING AND DETECTOR LOOP PLANS
14.	STRIPING AND DETECTOR LOOP PLANS
15.	STRIPING AND DETECTOR LOOP PLANS
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16.	(TC-10) TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
17.	(TC-13) DISTRICT ONE - TYPICAL PAVEMENT MARKINGS
18.	(TC-14) TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
19.	(TC-22) ARTERIAL ROAD INFORMATION SIGN
20.	(TC-23) TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS
21.	(BD-08) DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
22.	(BD-22) PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
23.	(BD-24) CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
24.	(BD-32) BUTT JOINT AND HMA TAPER DETAILS
25.	(TS-07) DISTRICT ONE - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

**HIGHWAY STANDARDS**

STD. NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
442201-03	CLASS C AND D PATCHES
424011	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424021	DEPRESSED CORNER FOR SIDEWALKS
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701427	LANE CLOSURE, MULTI-LANE, INTERMITTENT, OR MOVING OPERATIONS FOR SPEEDS < 40 MPH
701601-07	URBAN LANE CLOSURE, MULTI-LANE, 1W OR 2W WITH NON TRAVERSABLE MEDIAN
701606-08	URBAN LANE CLOSURE, MULTI-LANE, 2W WITH MOUNTABLE MEDIAN
701701-08	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	LANE CLOSURE MULTILANE, 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-02	TRAFFIC CONTROL DEVICES
780001-03	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS

Plotted: February 2, 2012 @ 3:41 PM By: Kris Pung - Tab: 02 Notes - 22x34  
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PLOT DATE =	DATE -	REVISED -																				
F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.																		
1314	11-00182-00-RS	KANE	25	2																		
SCALE: NONE    SHEET NO. 1 OF 1 SHEETS    STA. N/A TO STA. N/A			FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT NO. M-90038471																			

**SUMMARY OF QUANTITIES**

SPECIAL PROVISION SPECIALTY ITEM	CODE NO.	ITEM DESCRIPTION	UNIT	URBAN TOTAL QUANTITY	FEDERAL = 75%	
					CITY = 25%	
					ROADWAY	BRIDGE REHABILITATION
					0004	0014
	40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	3,198	3,198	
	40600300	AGGREGATE (PRIME COAT)	TON	68	68	
Δ	40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1,080	1,080	
	40600895	CONSTRUCTING TEST STRIP	EACH	2	2	
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	500	500	
	40600990	TEMPORARY RAMP	SQ YD	200	200	
	40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	1,888	1,888	
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	3,492	3,492	
	42400800	DETECTABLE WARNINGS	SQ FT	1,584	1,584	
	44000160	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	SQ YD	19,240	19,240	
	44000600	SIDEWALK REMOVAL	SQ FT	3,492	3,492	
	44201798	CLASS D PATCHES, TYPE I, 13 INCH	SQ YD	4	4	
	44201803	CLASS D PATCHES, TYPE II, 13 INCH	SQ YD	32	32	
	44201807	CLASS D PATCHES, TYPE III, 13 INCH	SQ YD	32	32	
	44201809	CLASS D PATCHES, TYPE IV, 13 INCH	SQ YD	123	123	
*	58100200	WATERPROOFING MEMBRANE SYSTEM	SQ YD	3,110		3,110
Δ	60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	1	1	
Δ	60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	43	43	
	67100100	MOBILIZATION	L SUM	1	1	
	70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1	
	70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1	
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1	
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1	

SPECIAL PROVISION SPECIALTY ITEM	CODE NO.	ITEM DESCRIPTION	UNIT	URBAN TOTAL QUANTITY	FEDERAL = 75%	
					CITY = 25%	
					ROADWAY	BRIDGE REHABILITATION
					0004	0014
	70106800	CHANGEABLE MESSAGE SIGN	CAL MO	5	5	
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	3,840	3,840	
	70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1,280	1,280	
	72000100	SIGN PANEL TYPE I	SQ FT	9	9	
	72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	16	16	
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	999	999	
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	4,420	4,420	
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	5,428	5,428	
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	795	795	
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	670	670	
Δ *	88600600	DETECTOR LOOP REPLACEMENT	FOOT	2,860	2,860	
Δ	X6024503	INLETS TO BE ADJUSTED WITH NEW FRAME AND GRATE (SPECIAL)	EACH	5	5	
Δ	X6026056	SANITARY MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	11	11	
Δ	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	44	44	
Δ *	XZ006305	BRIDGE DECK WEARING SURFACE	SQ YD	3,110		3,110
Δ *	Z0003700	BEARING PAD ADJUSTMENT	EACH	220		220
Δ *	Z0004556	HOT-MIX ASPHALT SURFACE REMOVAL (DECK)	SQ YD	3,110		3,110
Δ	Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	323	323	
Δ *	Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	200		200
Δ	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	129	129	
Δ *	Z0032700	KEYWAY REPAIR	FOOT	3,375		3,375
Δ	Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1	1

Δ SEE SPECIAL PROVISIONS  
\* SPECIALTY ITEMS

Plotted: February 15, 2012 @ 3:02 PM By: Kris Pung - Tab: 03 Quantities - 22x34

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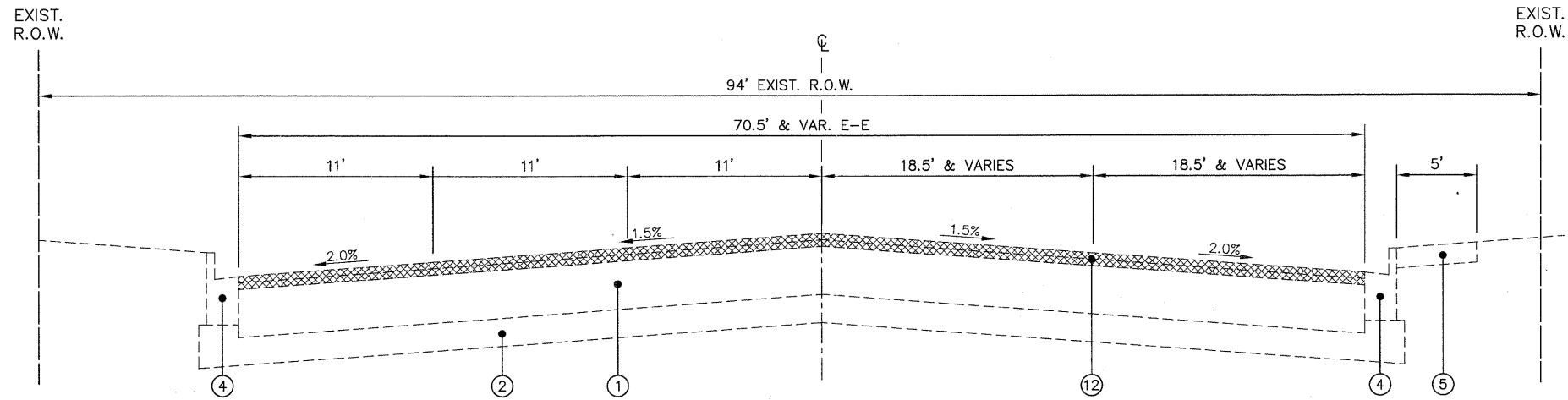
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

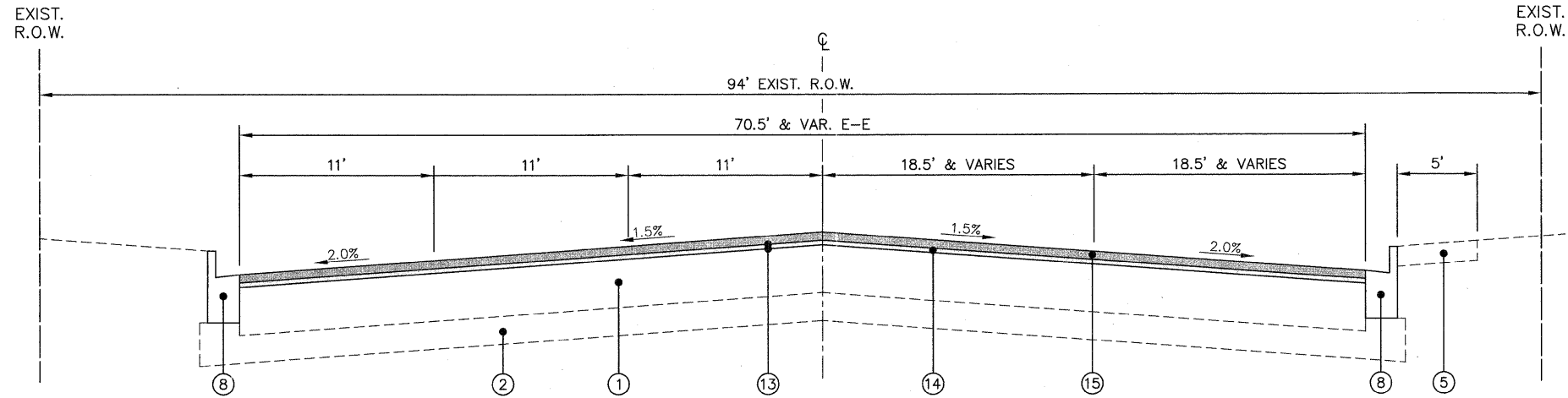
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F.A.U. RTE. 1314	SECTION 11-00182-00-RS	COUNTY KANE	TOTAL SHEETS 25	SHEET NO. 3
CONTRACT NO. 63689				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT NO. M-9003(B47)				

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**EXISTING TYPICAL SECTION**  
**STA 68+05 TO STA 69+06, KIMBALL STREET**  
 N.T.S.



**PROPOSED TYPICAL SECTION**  
**STA 68+05 TO STA 69+06, KIMBALL STREET**  
 N.T.S.

**LEGEND**

- |   |  |
|---|--|
| ① EXISTING 16" ASPHALT PAVEMENT   | ⑨ SIDEWALK REMOVAL AND PCC SIDEWALK, 5" (AT VARIOUS LOCATIONS)   |
| ② EXISTING 7" PORTLAND CEMENT CONCRETE BASE   | ⑩ HOT-MIX ASPHALT SURFACE REMOVAL (DECK)                         |
| ③ EXISTING 4" SUB-BASE GRANULAR MATERIAL, TYPE B                                      | ⑪ WATERPROOFING MEMBRANE SYSTEM                                  |
| ④ EXISTING B-6.18 COMBINATION CONCRETE CURB AND GUTTER                                | ⑫ HOT-MIX ASPHALT SURFACE REMOVAL, 2.75"                         |
| ⑤ EXISTING PORTLAND CEMENT CONCRETE SIDEWALK, 5" OR BRICK PAVER                       | ⑬ BITUMINOUS MATERIALS (PRIME COAT) AND AGGREGATE (PRIME COAT)   |
| ⑥ EXISTING 21"x48" PPC DECK BEAMS   | ⑭ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1" |
| ⑦ EXISTING 2" ASPHALT WEARING SURFACE   | ⑮ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1.75"            |
| ⑧ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AT VARIOUS LOCATIONS) | ⑯ BRIDGE DECK WEARING SURFACE                                    |

THE CONTRACTOR SHALL MILL BEFORE PATCHING

**HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

OPERATION	MIXTURE TYPE	AIR VOIDS @ N <sub>des</sub>
ROADWAY	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1.0"	3.5% @ 50 Gyr.
RESURFACING	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5 mm), 1.75"	4% @ 70 Gyr.
BRIDGE	BRIDGE DECK WEARING SURFACE	
RESURFACING	HMA SURFACE COURSE, MIX "D", N70 (IL-9.5 mm), 2.0" MIN.	4% @ 70 Gyr.
PATCHING	CLASS D PATCHES (HMA BINDER IL-19.0 mm), 13" (4 LIFTS)	4% @ 70 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/INCH.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE SPECIAL PROVISIONS

Plotted: January 19, 2012 @ 4:10 PM By: Josh Boatman - Tab: 04 Section - 22x34

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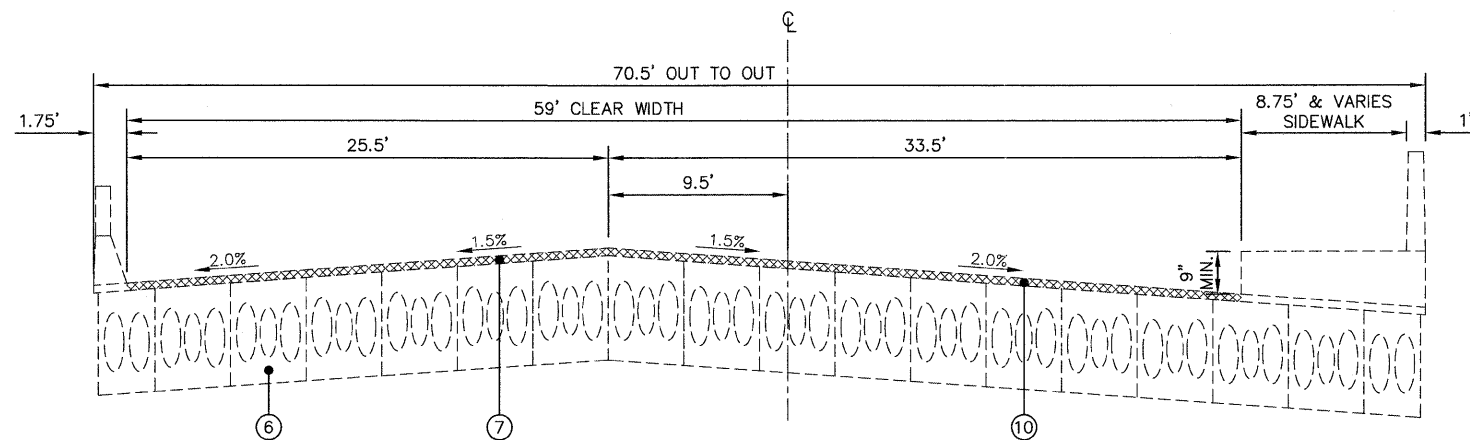
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS**

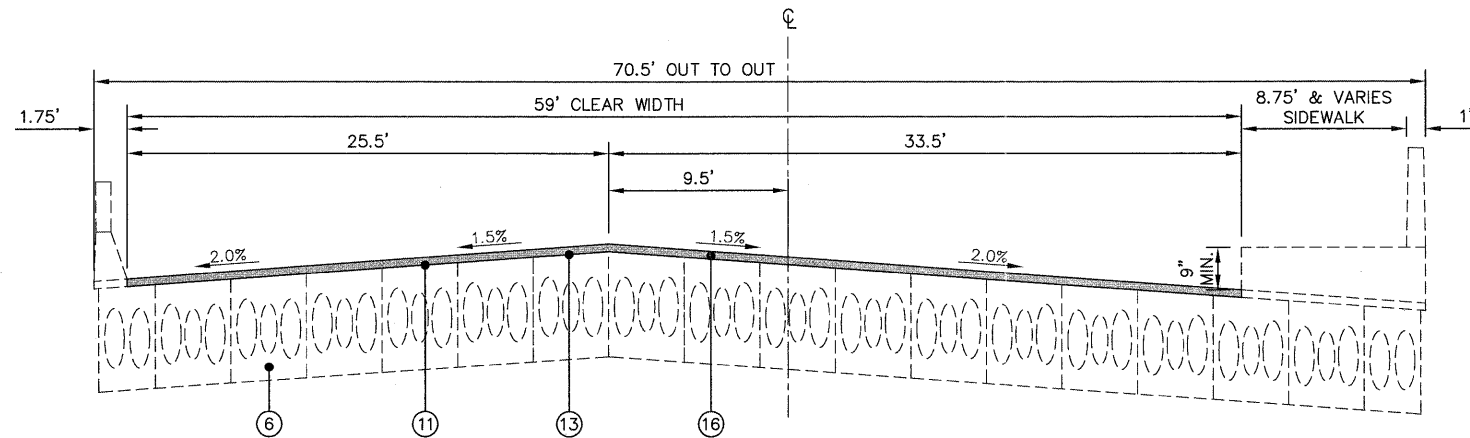
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1314	11-00182-00-RS	KANE	25	4
CONTRACT NO. 63689				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT NO. M-9003(847)				

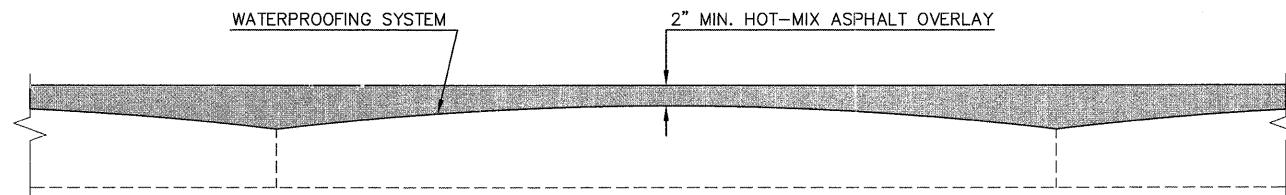
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**EXISTING TYPICAL SECTION**  
**STA 69+29 TO STA 74+27, KIMBALL STREET**  
 N.T.S.



**PROPOSED TYPICAL SECTION**  
**STA 69+29 TO STA 74+27, KIMBALL STREET**  
 N.T.S.



**OVERLAY DETAIL**  
**(PROFILE VIEW)**

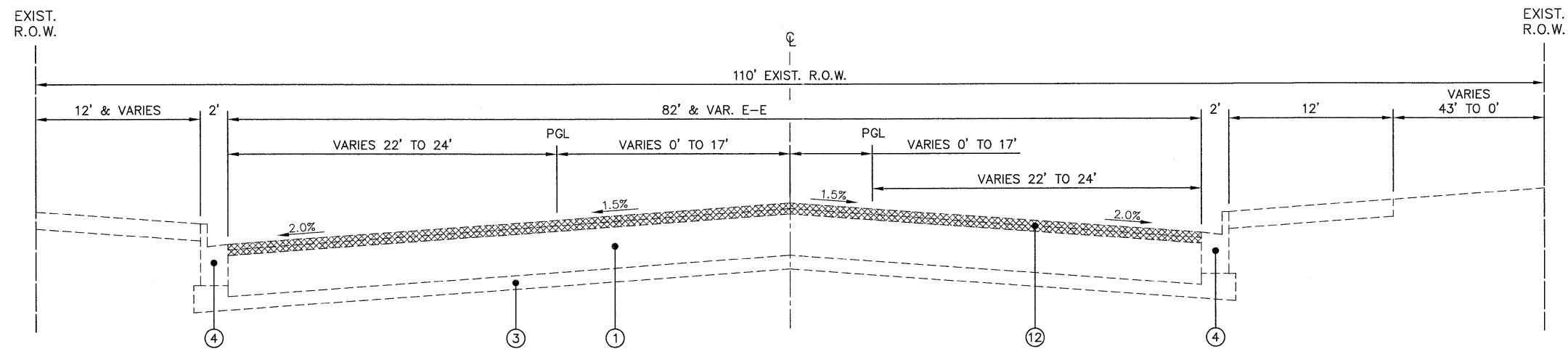
**LEGEND**

- ① EXISTING 16" ASPHALT PAVEMENT
- ② EXISTING 7" PORTLAND CEMENT CONCRETE BASE
- ③ EXISTING 4" SUB-BASE GRANULAR MATERIAL, TYPE B
- ④ EXISTING B-6.18 COMBINATION CONCRETE CURB AND GUTTER
- ⑤ EXISTING PORTLAND CEMENT CONCRETE SIDEWALK, 5" OR BRICK PAVER
- ⑥ EXISTING 21"x48" PPC DECK BEAMS
- ⑦ EXISTING 2" ASPHALT WEARING SURFACE
- ⑧ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AT VARIOUS LOCATIONS)
- ⑨ SIDEWALK REMOVAL AND PCC SIDEWALK, 5" (AT VARIOUS LOCATIONS)
- ⑩ HOT-MIX ASPHALT SURFACE REMOVAL (DECK)
- ⑪ WATERPROOFING MEMBRANE SYSTEM
- ⑫ HOT-MIX ASPHALT SURFACE REMOVAL, 2.75"
- ⑬ BITUMINOUS MATERIALS (PRIME COAT) AND AGGREGATE (PRIME COAT)
- ⑭ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- ⑮ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1.75"
- ⑯ BRIDGE DECK WEARING SURFACE

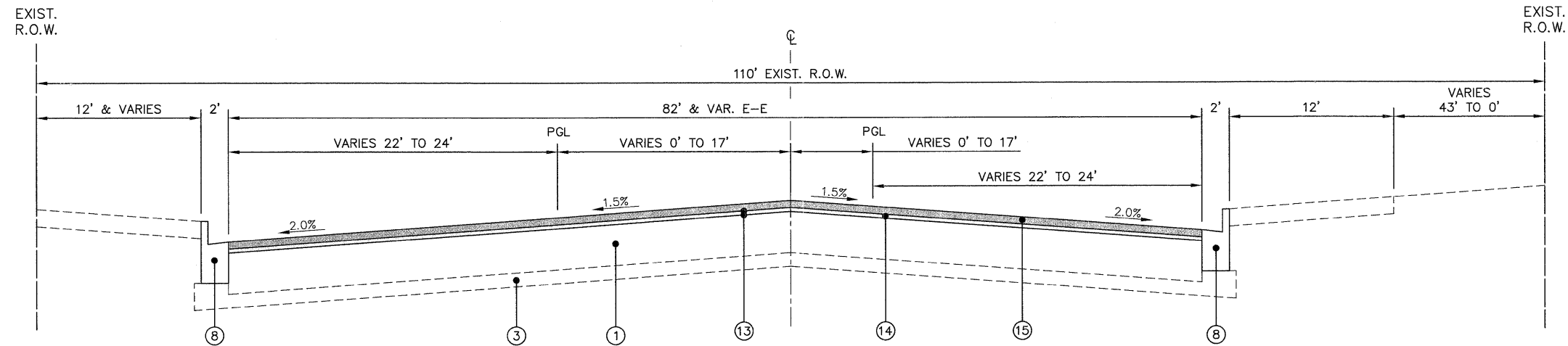
Plotted: January 19, 2012 @ 4:10 PM By: Josh Boatman - Tab: 05 Section - 22x34

COPYRIGHT © 2012 ENGINEERING ENTERPRISES, INC.			<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>		<b>TYPICAL SECTIONS</b>		F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
USER NAME =	DESIGNED -	REVISED -					1314	11-00182-00-RS	KANE	25	5
PLOT SCALE =	DRAWN -	REVISED -					CONTRACT NO. 63689				
PLOT DATE =	CHECKED -	REVISED -					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT NO. M-900308471				
			DATE -	REVISED -	SCALE: NONE	SHEET NO. 2 OF 4 SHEETS	STA. N/A	TO STA. N/A			

PATH:\S\518\PROJ\EGT005\DWG\FINAL\ENG\EGT005-CYR



**EXISTING TYPICAL SECTION**  
**STA 74+27 TO STA 76+87, KIMBALL STREET**  
 N.T.S.



**PROPOSED TYPICAL SECTION**  
**STA 74+27 TO STA 76+87, KIMBALL STREET**  
 N.T.S.

**LEGEND**

- |   |  |
|---|--|
| ① EXISTING 16" ASPHALT PAVEMENT   | ⑨ SIDEWALK REMOVAL AND PCC SIDEWALK, 5" (AT VARIOUS LOCATIONS)   |
| ② EXISTING 7" PORTLAND CEMENT CONCRETE BASE   | ⑩ HOT-MIX ASPHALT SURFACE REMOVAL (DECK)                         |
| ③ EXISTING 4" SUB-BASE GRANULAR MATERIAL, TYPE B                                      | ⑪ WATERPROOFING MEMBRANE SYSTEM                                  |
| ④ EXISTING B-6.18 COMBINATION CONCRETE CURB AND GUTTER                                | ⑫ HOT-MIX ASPHALT SURFACE REMOVAL, 2.75"                         |
| ⑤ EXISTING PORTLAND CEMENT CONCRETE SIDEWALK, 5" OR BRICK PAVER                       | ⑬ BITUMINOUS MATERIALS (PRIME COAT) AND AGGREGATE (PRIME COAT)   |
| ⑥ EXISTING 21"x48" PPC DECK BEAMS   | ⑭ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1" |
| ⑦ EXISTING 2" ASPHALT WEARING SURFACE   | ⑮ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1.75"            |
| ⑧ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AT VARIOUS LOCATIONS) | ⑯ BRIDGE DECK WEARING SURFACE                                    |

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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

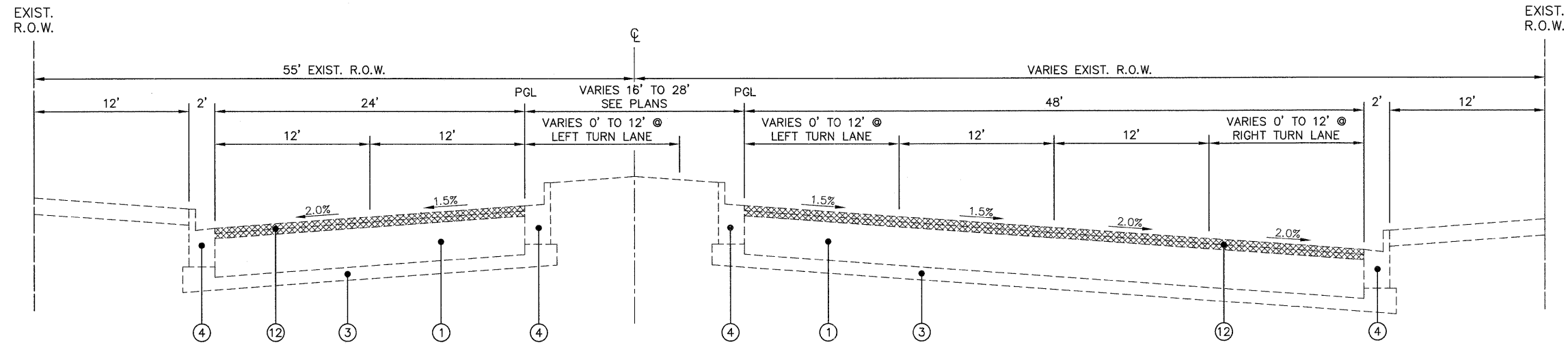
**TYPICAL SECTIONS**

SCALE: NONE SHEET NO. 3 OF 4 SHEETS STA. N/A TO STA. N/A

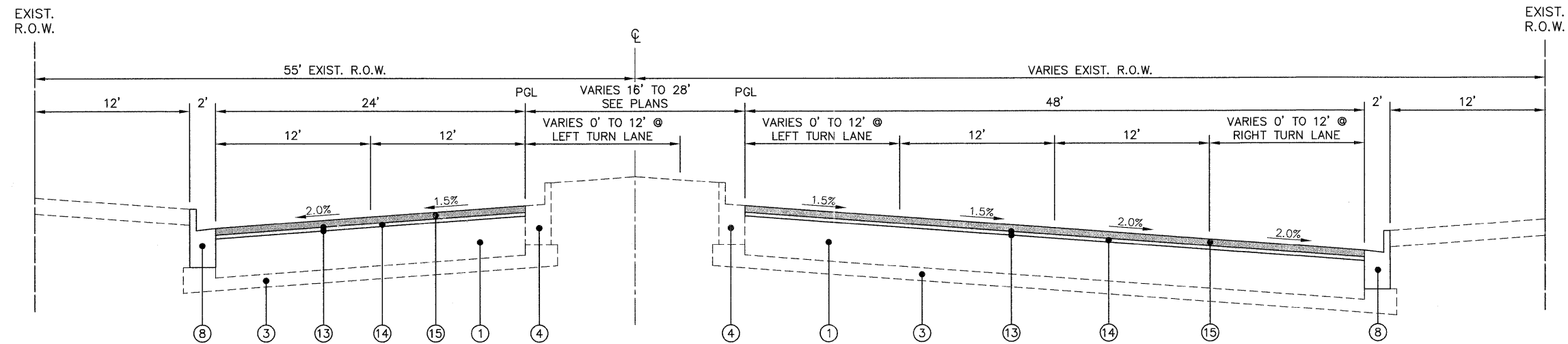
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1314	11-00182-00-RS	KANE	25	6
CONTRACT NO. 63689				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT NO. M-9003(847)				

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**EXISTING TYPICAL SECTION**  
**STA 76+87 TO STA 98+68, KIMBALL STREET**  
 N.T.S.



**PROPOSED TYPICAL SECTION**  
**STA 76+87 TO STA 98+68, KIMBALL STREET**  
 N.T.S.

**LEGEND**

- |   |  |
|---|--|
| ① EXISTING 16" ASPHALT PAVEMENT   | ⑨ SIDEWALK REMOVAL AND PCC SIDEWALK, 5" (AT VARIOUS LOCATIONS)   |
| ② EXISTING 7" PORTLAND CEMENT CONCRETE BASE   | ⑩ HOT-MIX ASPHALT SURFACE REMOVAL (DECK)                         |
| ③ EXISTING 4" SUB-BASE GRANULAR MATERIAL, TYPE B                                      | ⑪ WATERPROOFING MEMBRANE SYSTEM                                  |
| ④ EXISTING B-6.18 COMBINATION CONCRETE CURB AND GUTTER                                | ⑫ HOT-MIX ASPHALT SURFACE REMOVAL, 2.75"                         |
| ⑤ EXISTING PORTLAND CEMENT CONCRETE SIDEWALK, 5" OR BRICK PAVER                       | ⑬ BITUMINOUS MATERIALS (PRIME COAT) AND AGGREGATE (PRIME COAT)   |
| ⑥ EXISTING 21"x48" PPC DECK BEAMS   | ⑭ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1" |
| ⑦ EXISTING 2" ASPHALT WEARING SURFACE   | ⑮ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1.75"            |
| ⑧ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AT VARIOUS LOCATIONS) | ⑯ BRIDGE DECK WEARING SURFACE                                    |

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PLOT DATE =	CHECKED -	REVISED -
	DATE -	REVISED -

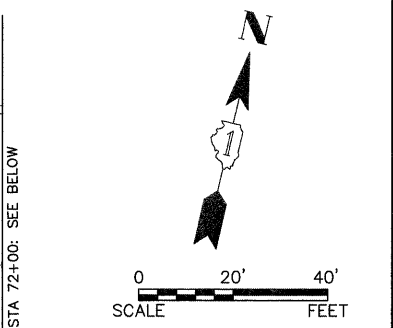
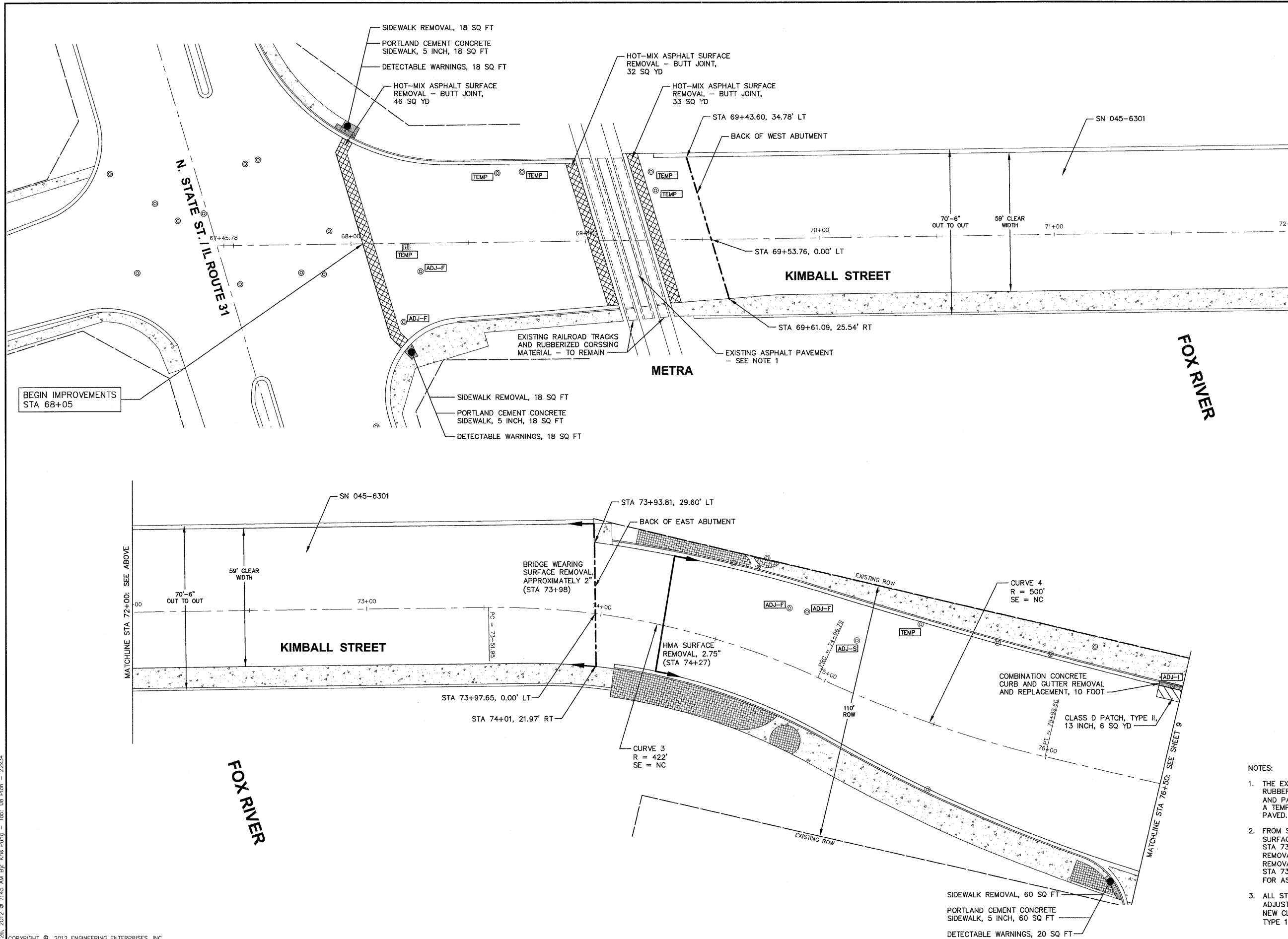
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS**

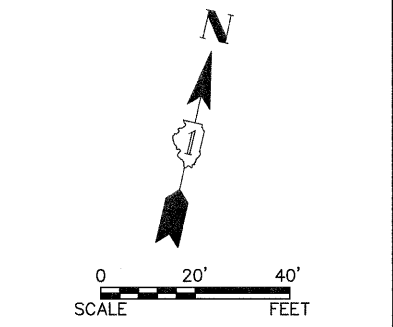
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F.A.J. RTE. 1314	SECTION 11-00182-00-RS	COUNTY KANE	TOTAL SHEETS 25	SHEET NO. 7
CONTRACT NO. 63689				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT NO. M-9003(847)				

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- LEGEND**
- EXISTING SIDEWALK
  - CLASS D PATCHES
  - HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
  - CURB & GUTTER REMOVAL AND REPLACEMENT & SIDEWALK REMOVAL AND REPLACEMENT
  - EXISTING BRICK PAVERS
  - ADJ-F FRAMES & LIDS TO BE ADJUSTED (SPECIAL)
  - ADJ-S SANITARY MANHOLE TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID
  - ADJ-I INLET TO BE ADJUSTED WITH NEW FRAME AND GRATE (SPECIAL)
  - TEMP TEMPORARY RAMP TO BE CONSTRUCTED AT HANDHOLE/UTILITY STRUCTURE UNLESS OTHERWISE NOTED (NO ADJUSTMENT)



- NOTES:**
1. THE EXISTING ASPHALT PAVEMENT BETWEEN THE RUBBERIZED CROSSING MATERIAL WILL BE REMOVED AND PAID FOR AS HMA SURFACE REMOVAL, 2.75". A TEMPORARY RAMP WILL BE PROVIDED UNTIL IT IS PAVED.
  2. FROM STA 68+05 TO STA 69+06, PERFORM HMA SURFACE REMOVAL, 2.75". FROM STA 69+29 TO STA 73+98, PERFORM BRIDGE WEARING SURFACE REMOVAL (2"). THE TRANSITION OF HMA SURFACE REMOVAL FROM 2" TO 2.75" SHALL OCCUR BETWEEN STA 73+98 AND STA 74+27, AND SHALL BE PAID FOR AS HMA SURFACE REMOVAL, 2.75".
  3. ALL STRUCTURES WITH FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) SHALL BE REPLACED WITH A NEW CLOSED FRAME AND LID (FRAMES AND LIDS, TYPE 1, CLOSED LID TO BE PAID FOR SEPARATELY).

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PLOT DATE =	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

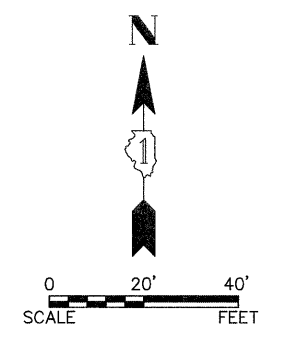
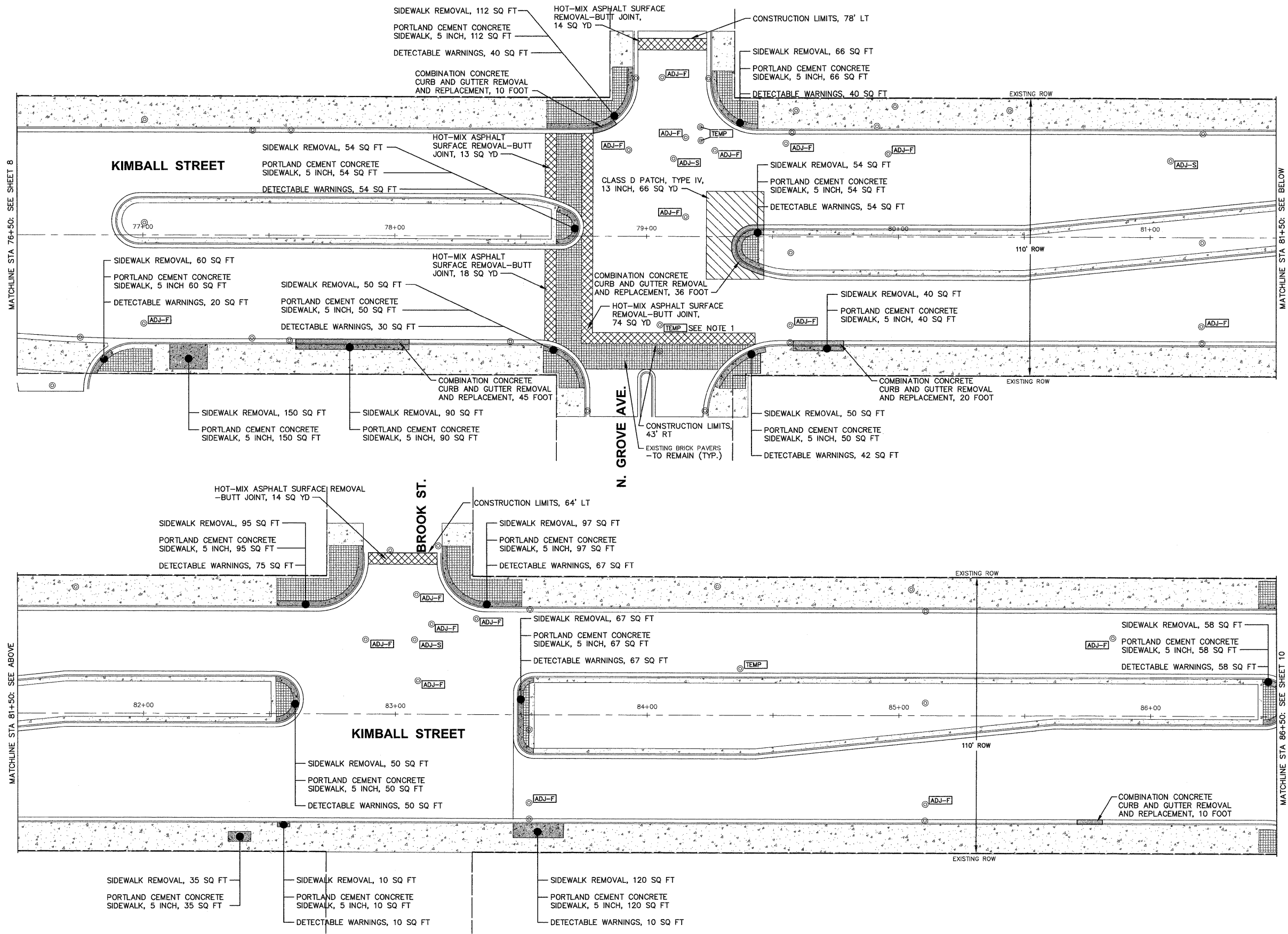
**PLAN, CONSTRUCTION NOTES**  
**AND DETAILS**

SCALE: 1"=20'    SHEET NO. 1 OF 4 SHEETS    STA. 68+05 TO STA. 76+50

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1314	11-00182-00-RS	KANE	25	8
CONTRACT NO. 63689				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT NO. M-90031847				

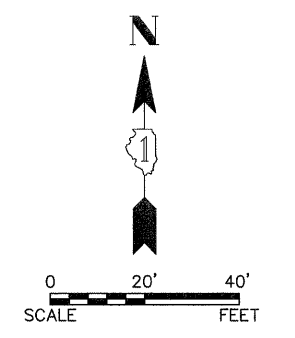
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**LEGEND**

- EXISTING SIDEWALK
- CLASS D PATCHES
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- CURB & GUTTER REMOVAL AND REPLACEMENT & SIDEWALK REMOVAL AND REPLACEMENT
- EXISTING BRICK PAVERS
- FRAMES & LIDS TO BE ADJUSTED (SPECIAL)
- SANITARY MANHOLE TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID
- INLET TO BE ADJUSTED WITH NEW FRAME AND GRATE (SPECIAL)
- TEMPORARY RAMP TO BE CONSTRUCTED AT HANDHOLE/UTILITY STRUCTURE UNLESS OTHERWISE NOTED (NO ADJUSTMENT)



- NOTES:**
- ALL STRUCTURES WITH FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) SHALL BE REPLACED WITH A NEW CLOSED FRAME AND LID (FRAMES AND LIDS, TYPE 1, CLOSED LID TO BE PAID FOR SEPARATELY) EXCEPT STRUCTURE AT STA. 79+06, 35' RT, WHICH IS AN OPEN LID STORM STRUCTURE (PAID FOR AS FRAMES AND LIDS, TYPE 1, OPEN LID).

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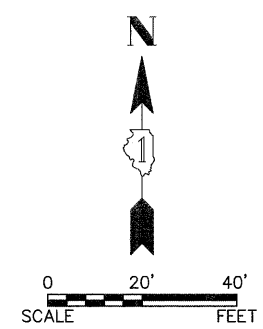
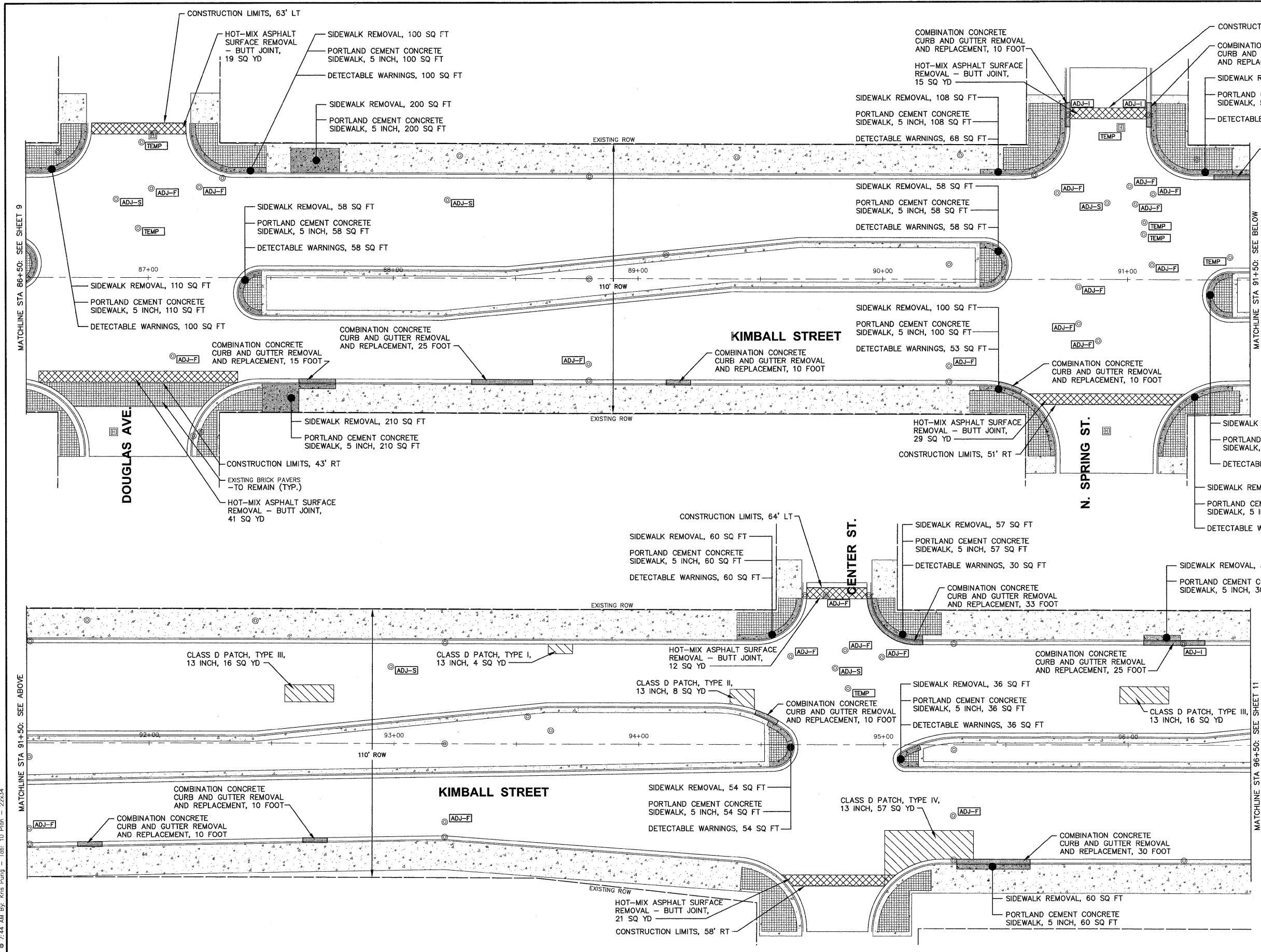
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PLOT SCALE =	DRAWN -	REVISED -
PLOT DATE =	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PLAN, CONSTRUCTION NOTES  
AND DETAILS**

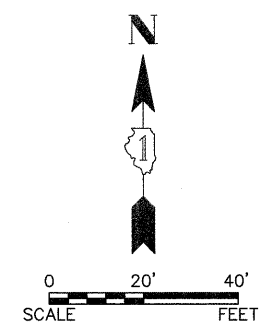
SCALE: 1"=20' SHEET NO. 2 OF 4 SHEETS STA. 76+50 TO STA. 86+50

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1314	11-00182-00-RS	KANE	25	9
CONTRACT NO. 63689			FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT NO. M-9003(847)	



**LEGEND**

- EXISTING SIDEWALK
- CLASS D PATCHES
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- CURB & GUTTER REMOVAL AND REPLACEMENT & SIDEWALK REMOVAL AND REPLACEMENT
- EXISTING BRICK PAVERS
- FRAMES & LIDS TO BE ADJUSTED (SPECIAL)
- SANITARY MANHOLE TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID
- INLET TO BE ADJUSTED WITH NEW FRAME AND GRATE (SPECIAL)
- TEMPORARY RAMP TO BE CONSTRUCTED AT HANDHOLE/UTILITY STRUCTURE UNLESS OTHERWISE NOTED (NO ADJUSTMENT)



- NOTES:**
- ALL STRUCTURES WITH FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) SHALL BE REPLACED WITH A NEW CLOSED FRAME AND LID (FRAMES AND LIDS, TYPE 1, CLOSED LID TO BE PAID FOR SEPARATELY).

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PLOT DATE =	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

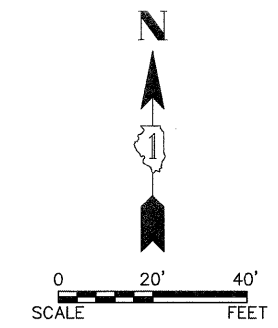
**PLAN, CONSTRUCTION NOTES  
AND DETAILS**

SCALE: 1"=20' SHEET NO. 3 OF 4 SHEETS STA. 86+50 TO STA. 96+50

F.A.U. RTE. 1314	SECTION 11-00182-00-RS	COUNTY KANE	TOTAL SHEETS 25	SHEET NO. 10
FED. ROAD DIST. NO. 1 [ILLINOIS]			CONTRACT NO. 63689	
FED. AID PROJECT NO. M-9003(847)				

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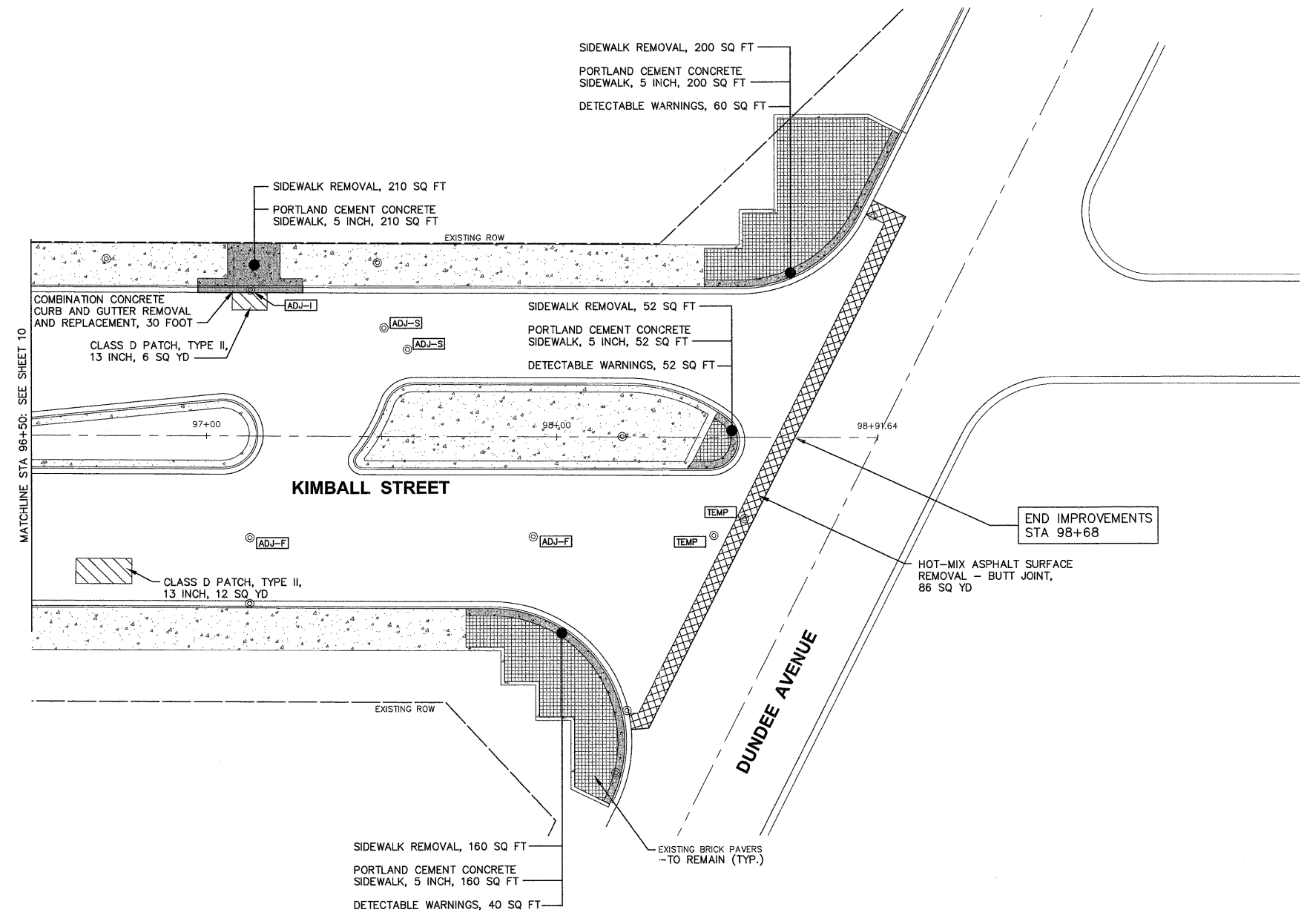
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**LEGEND**

- EXISTING SIDEWALK
- CLASS D PATCHES
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- CURB & GUTTER REMOVAL AND REPLACEMENT & SIDEWALK REMOVAL AND REPLACEMENT
- EXISTING BRICK PAVERS
- FRAMES & LIDS TO BE ADJUSTED (SPECIAL)
- SANITARY MANHOLE TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID
- INLET TO BE ADJUSTED WITH NEW FRAME AND GRATE (SPECIAL)
- TEMPORARY RAMP TO BE CONSTRUCTED AT HANDHOLE/UTILITY STRUCTURE UNLESS OTHERWISE NOTED (NO ADJUSTMENT)

- NOTES:
1. ALL STRUCTURES WITH FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) SHALL BE REPLACED WITH A NEW CLOSED FRAME AND LID (FRAMES AND LIDS, TYPE 1, CLOSED LID TO BE PAID FOR SEPARATELY).



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PLOT DATE =	DATE -	REVISED -	

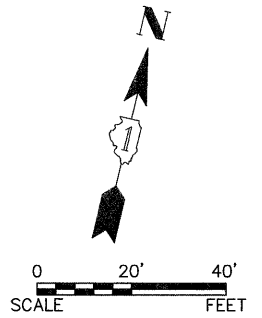
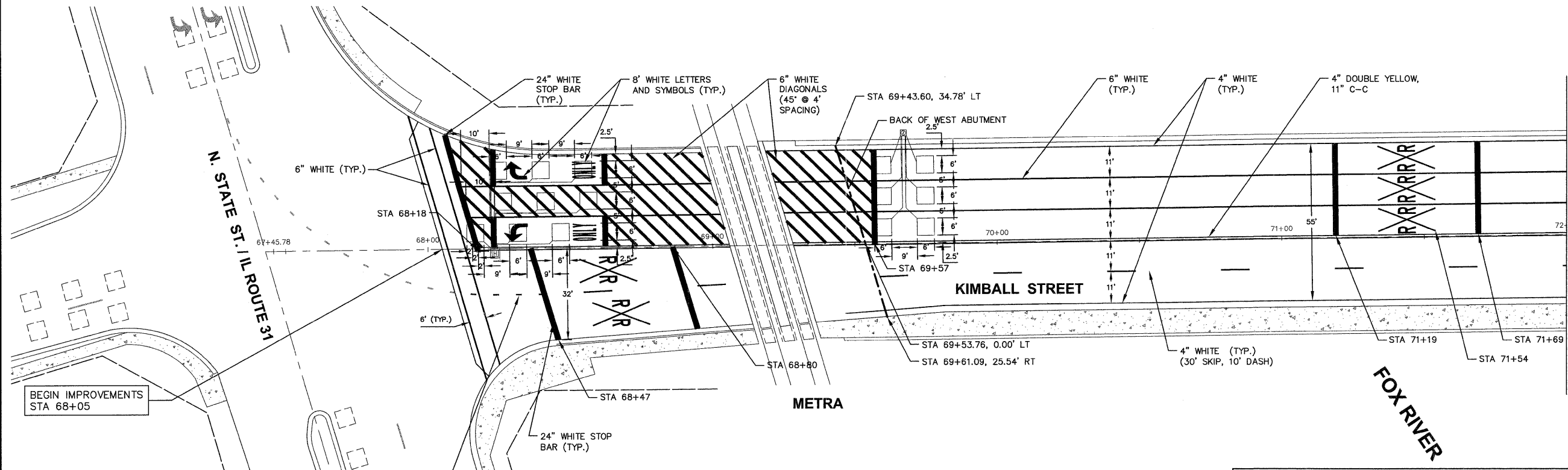
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PLAN, CONSTRUCTION NOTES  
AND DETAILS**

SCALE: 1"=20'    SHEET NO. 4 OF 4 SHEETS    STA. 96+50 TO STA. 98+68

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1314	11-00182-00-RS	KANE	25	11
CONTRACT NO. 63689				
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT NO. M-9003847				

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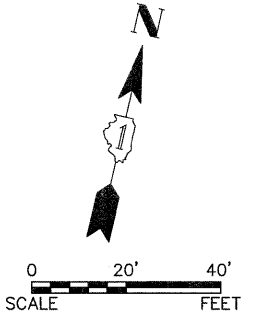
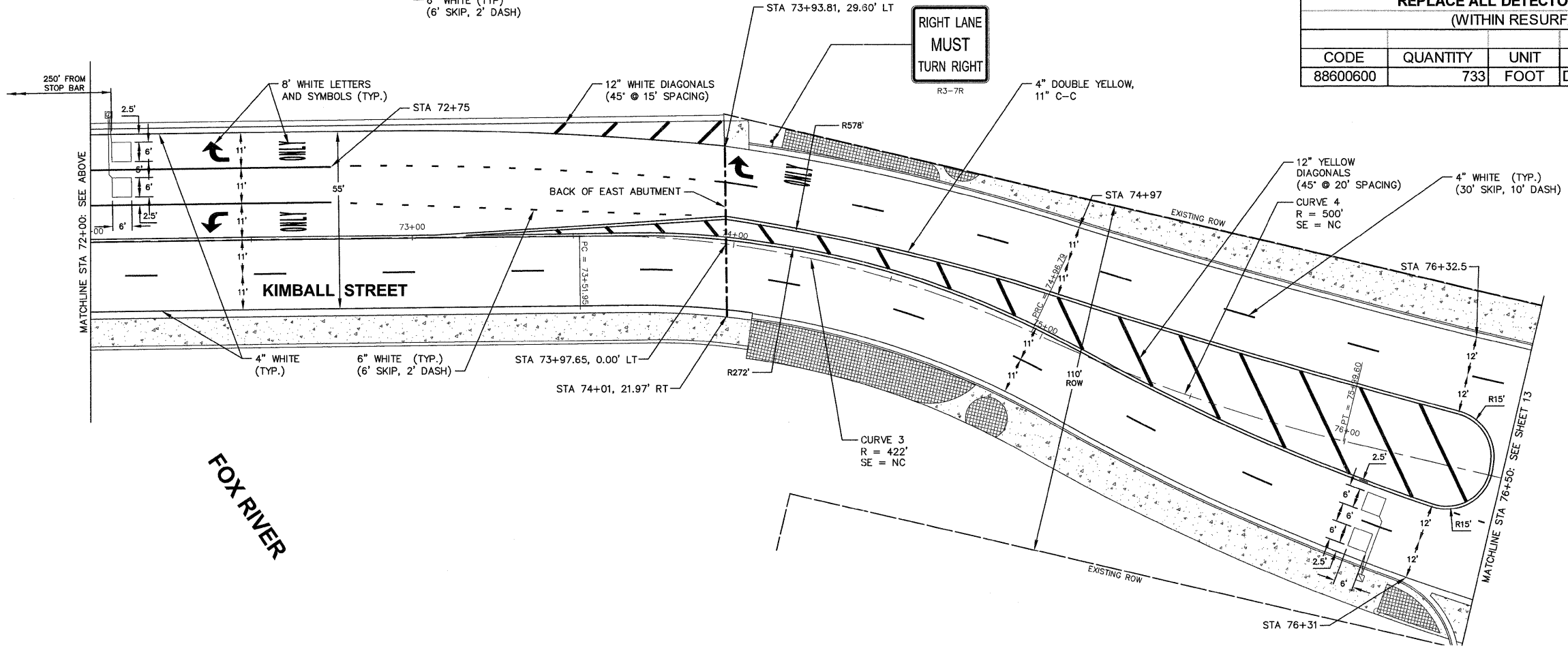
**LEGEND**

	EXISTING SIDEWALK
	EXISTING BRICK PAVERS

BEGIN IMPROVEMENTS  
STA 68+05

**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN RESURFACING LIMITS)**

CODE	QUANTITY	UNIT	ITEM
88600600	733	FOOT	DETECTOR LOOP REPLACEMENT



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	DATE -	REVISED -

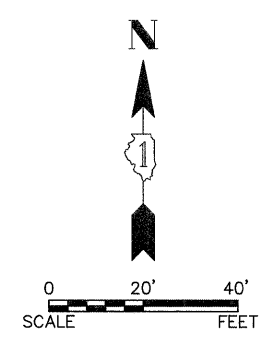
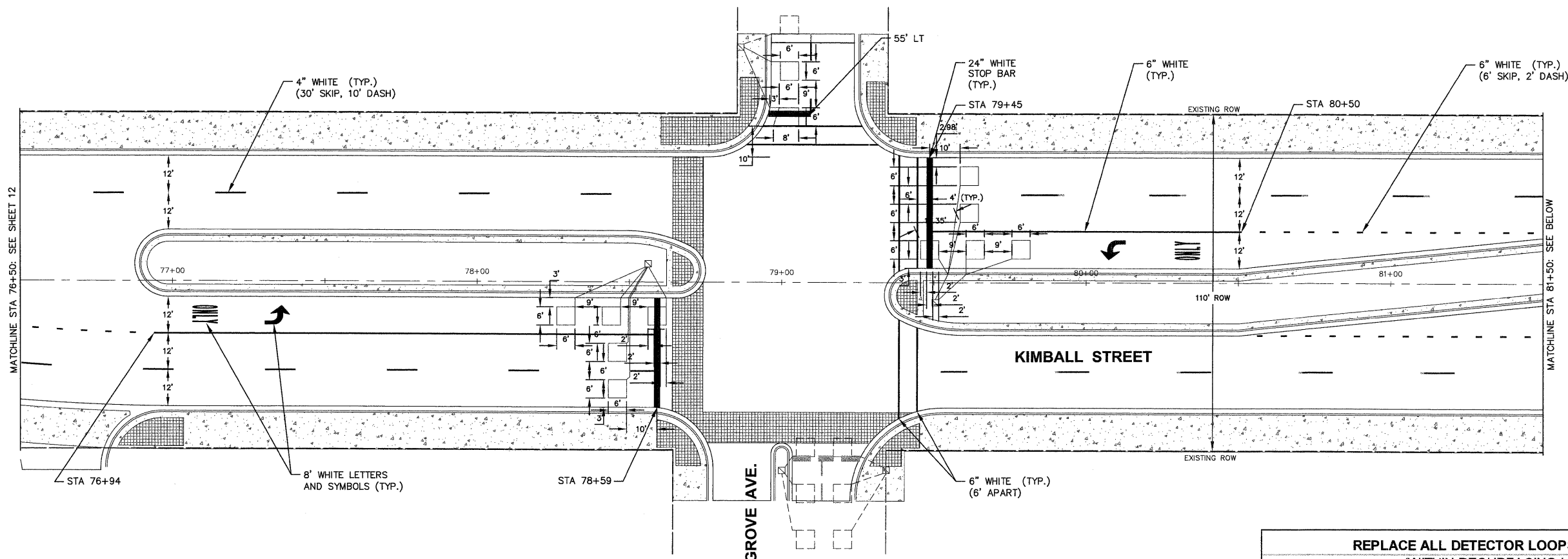
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STRIPING AND DETECTOR  
LOOP PLANS**

SCALE: 1"=20' SHEET NO. 1 OF 4 SHEETS STA. 68+05 TO STA. 76+50

F.A.J. RTE. 1314	SECTION 11-00182-00-RS	COUNTY KANE	TOTAL SHEETS 25	SHEET NO. 12
CONTRACT NO. 63689				
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT NO. M-9003(847)				

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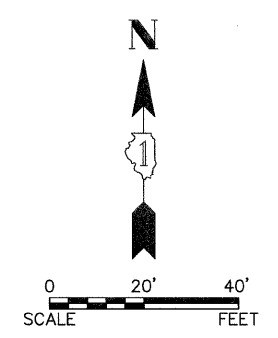
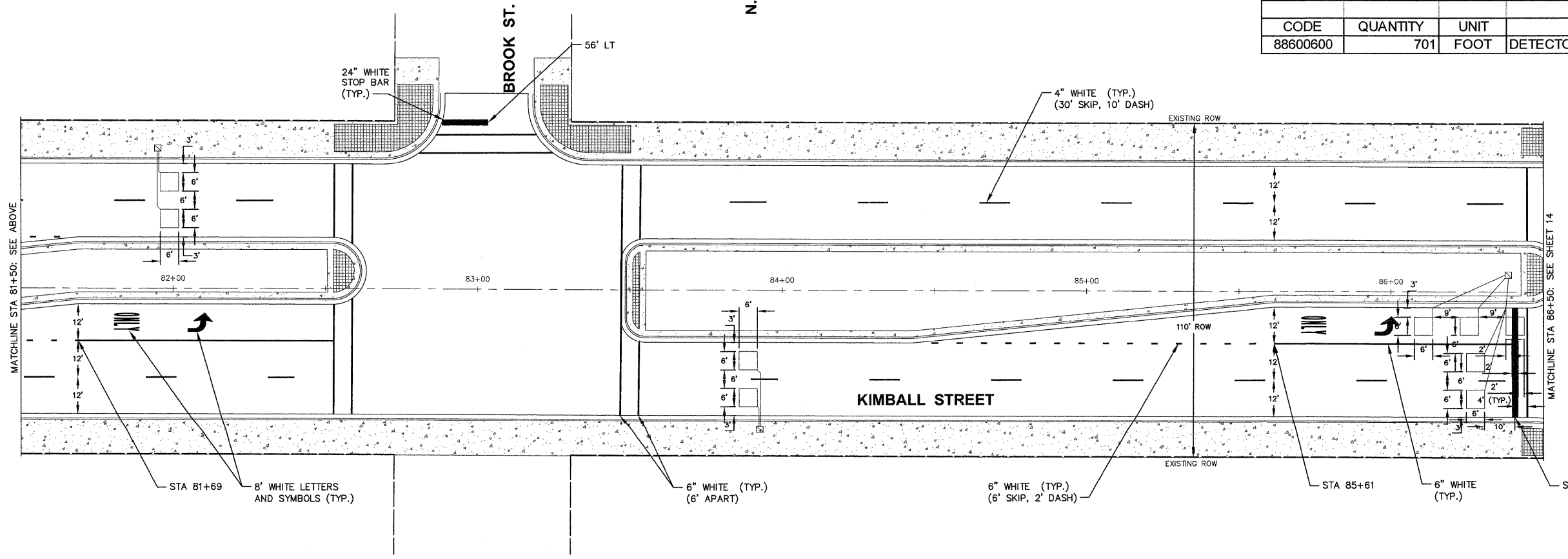


**LEGEND**

	EXISTING SIDEWALK
	EXISTING BRICK PAVERS

**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN RESURFACING LIMITS)**

CODE	QUANTITY	UNIT	ITEM
88600600	701	FOOT	DETECTOR LOOP REPLACEMENT



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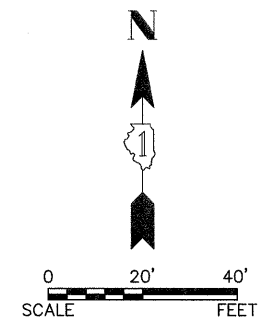
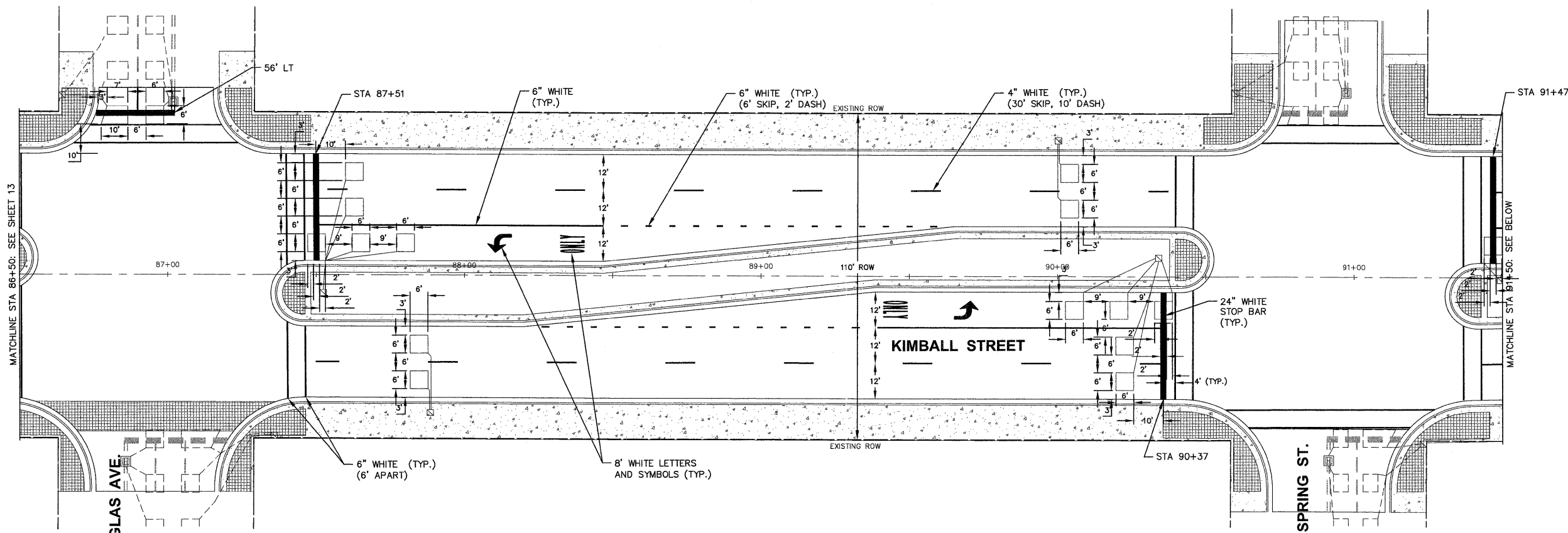
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STRIPING AND DETECTOR  
LOOP PLANS**

SCALE: 1"=20'    SHEET NO. 2 OF 4 SHEETS    STA. 76+50 TO STA. 86+50

F.A.J. RTE. 1314	SECTION 11-00182-00-RS	COUNTY KANE	TOTAL SHEETS 25	SHEET NO. 13
CONTRACT NO. 63689			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT NO. M-900318471	

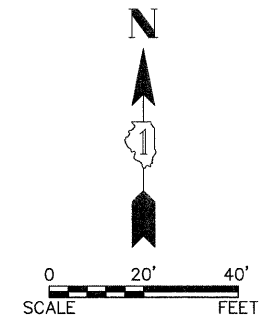
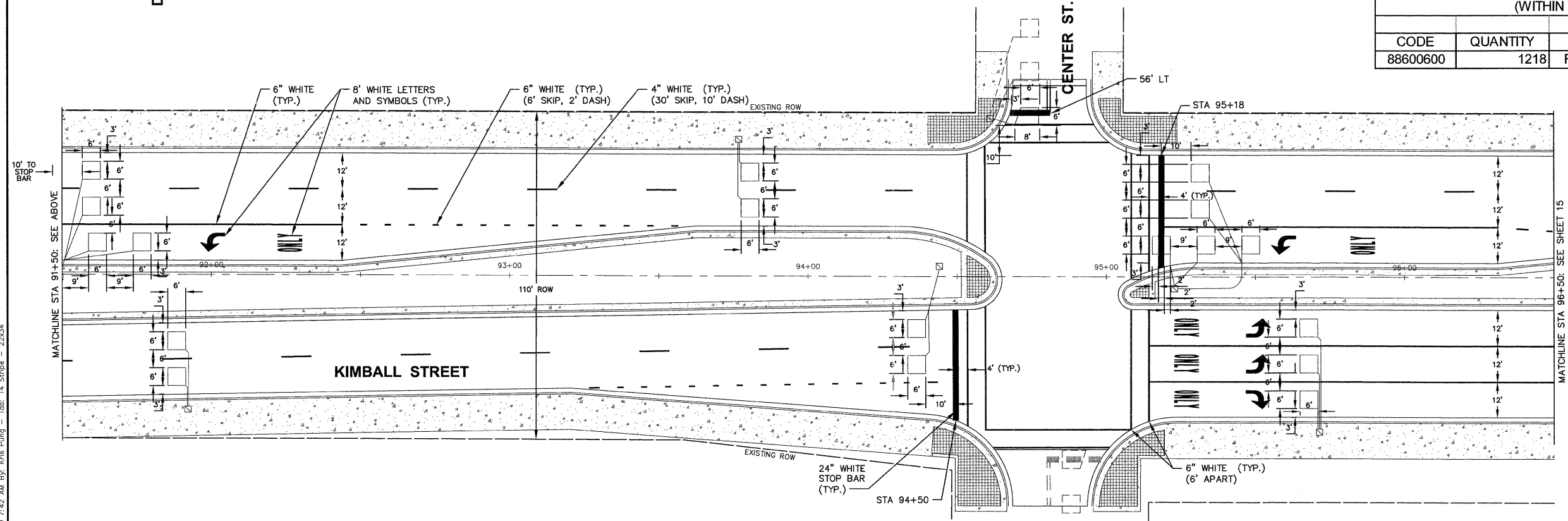
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**LEGEND**

	EXISTING SIDEWALK
	EXISTING BRICK PAVERS

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN RESURFACING LIMITS)			
CODE	QUANTITY	UNIT	ITEM
88600600	1218	FOOT	DETECTOR LOOP REPLACEMENT



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PLOT DATE =	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

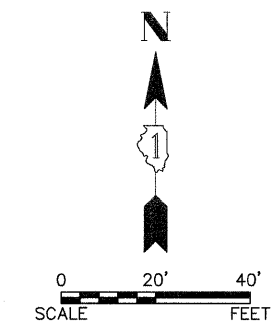
**STRIPING AND DETECTOR  
LOOP PLANS**

SCALE: 1"=20'    SHEET NO. 3 OF 4 SHEETS    STA. 86+50 TO STA. 96+50



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CONTRACT NO. 63689				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT NO. M-9003(847)				

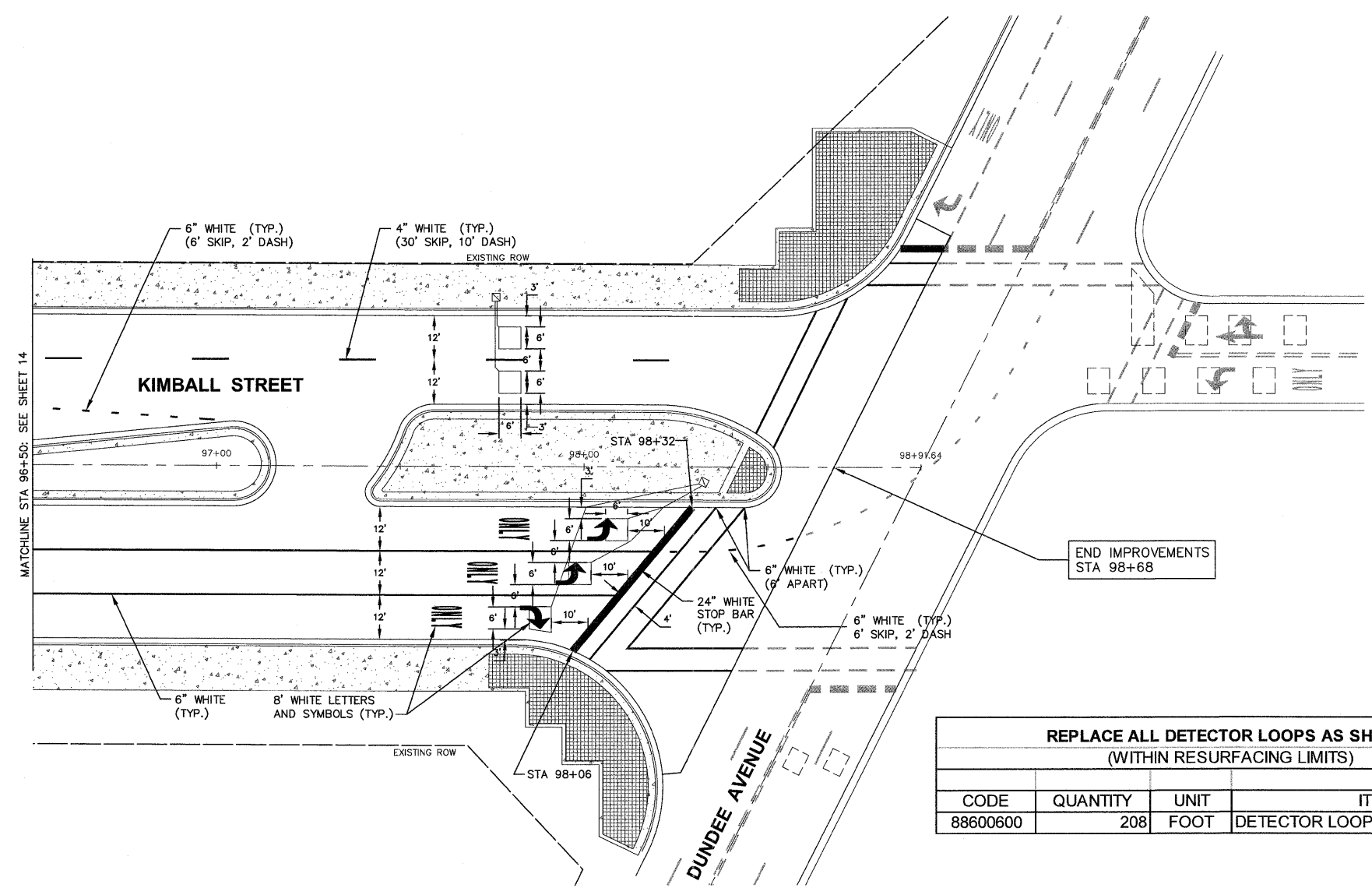
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**LEGEND**

-  EXISTING SIDEWALK
-  EXISTING BRICK PAVERS



REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN RESURFACING LIMITS)			
CODE	QUANTITY	UNIT	ITEM
88600600	208	FOOT	DETECTOR LOOP REPLACEMENT

Plotted: January 26, 2012 @ 7:41 AM By: Kris Pung - Tab: 15 Stripe - 22x34  
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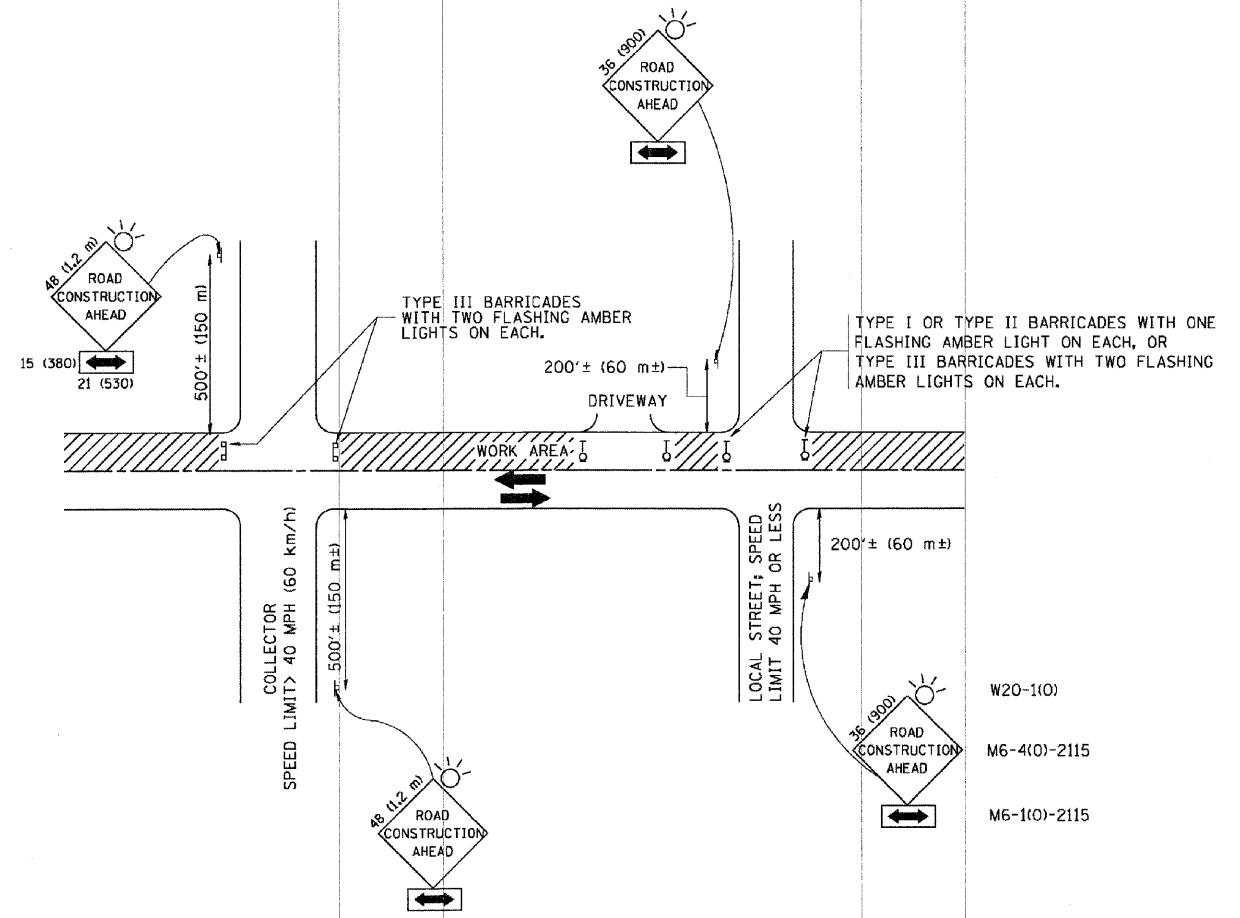
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STRIPING AND DETECTOR  
LOOP PLANS**

SCALE: 1"=20'    SHEET NO. 4 OF 4 SHEETS    STA. 96+50 TO STA. 98+68

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1314	11-00182-00-RS	KANE	25	15
CONTRACT NO. 63689				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT NO. M-9003(847)				

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W20-1(0)  
M6-4(0)-2115  
M6-1(0)-2115

**TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

**NOTES:**

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

All dimensions are in millimeters (inches) unless otherwise shown.

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PLOT DATE = 1/4/2008	DATE - 06-89		REVISIONS

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS</b>			
SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA. TO STA.

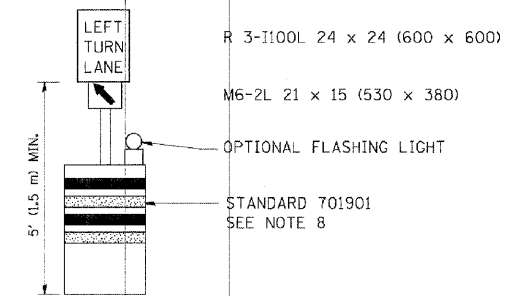
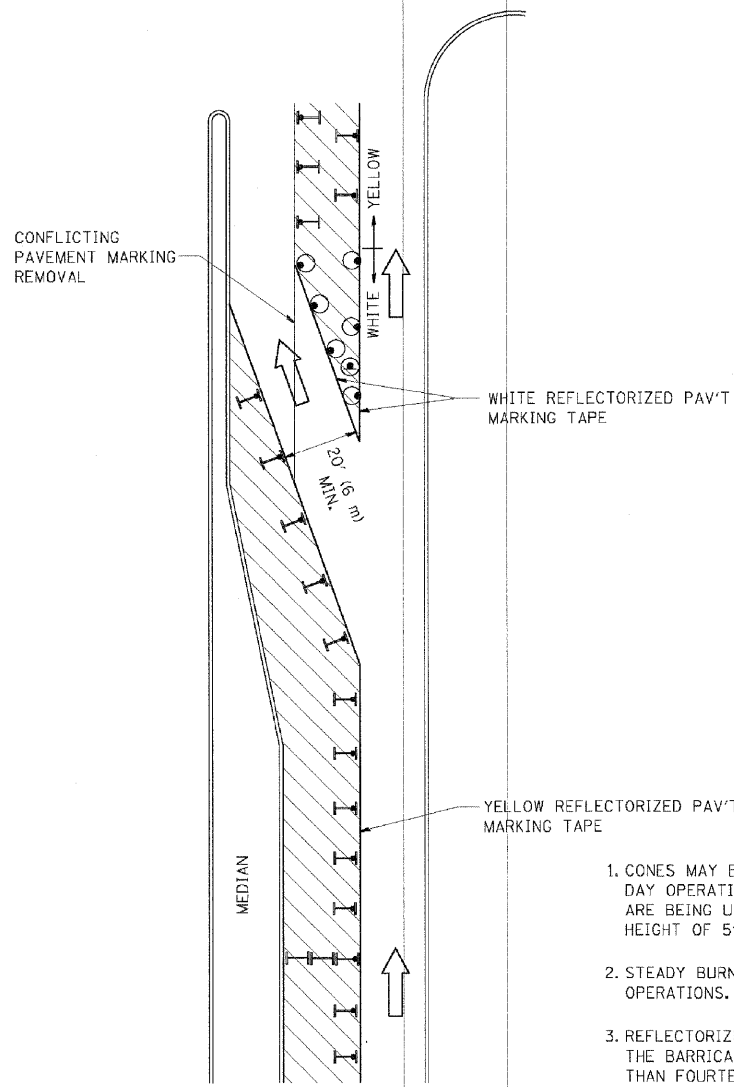
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TC-10		CONTRACT NO. 63689		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

Plotted: January 19, 2012 @ 4:12 PM By: Josh Bodman - Tab: 16 TC-10 22x34

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Project: January 19, 2012 @ 4:13 PM By: Josh Boatman - Job: 18 TD-14 22434



**GENERAL NOTES**

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

**LEGEND**

- WORK AREA
- LANE OPEN TO TRAFFIC
- TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

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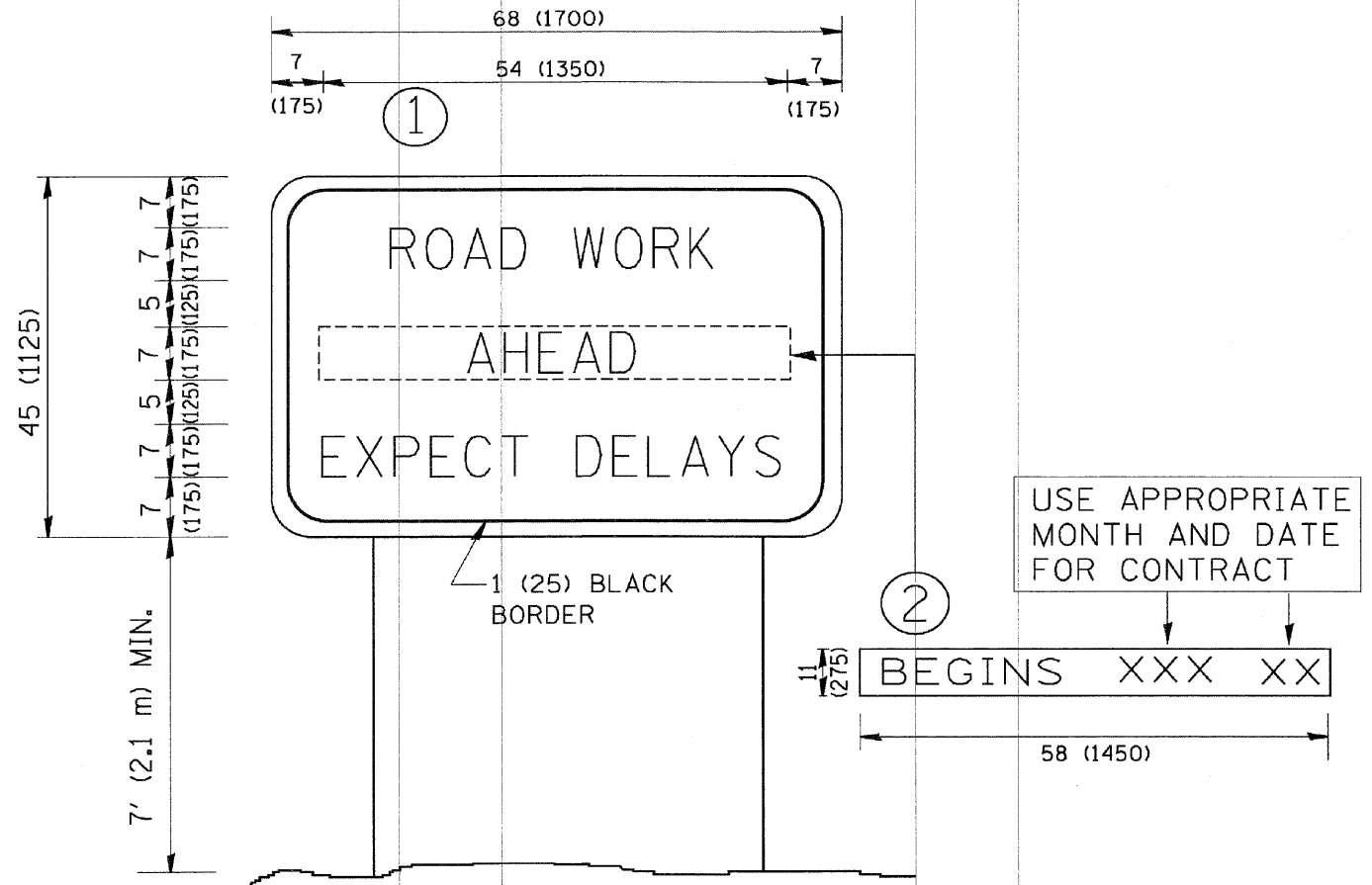
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS  
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.D. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1314	11-00182-00-RS	KANE	25	18
<b>TC-14</b>		<b>CONTRACT NO. 63689</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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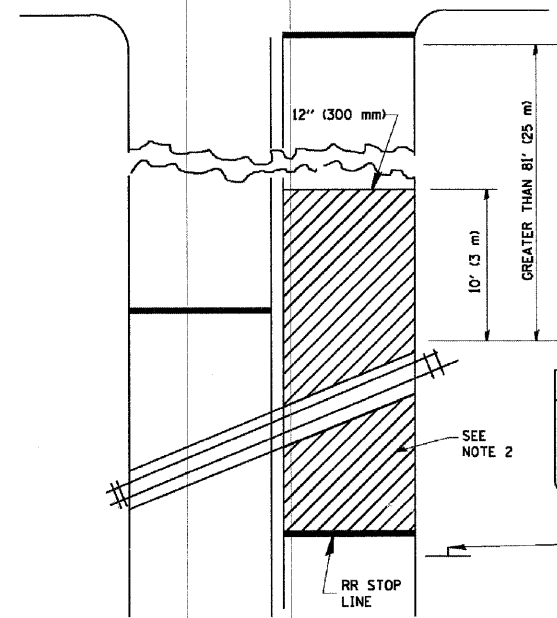
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

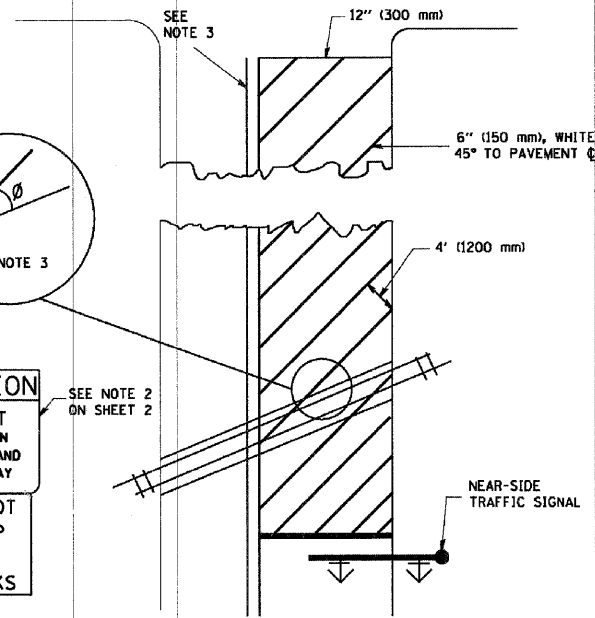
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TC-22		CONTRACT NO. 63689		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

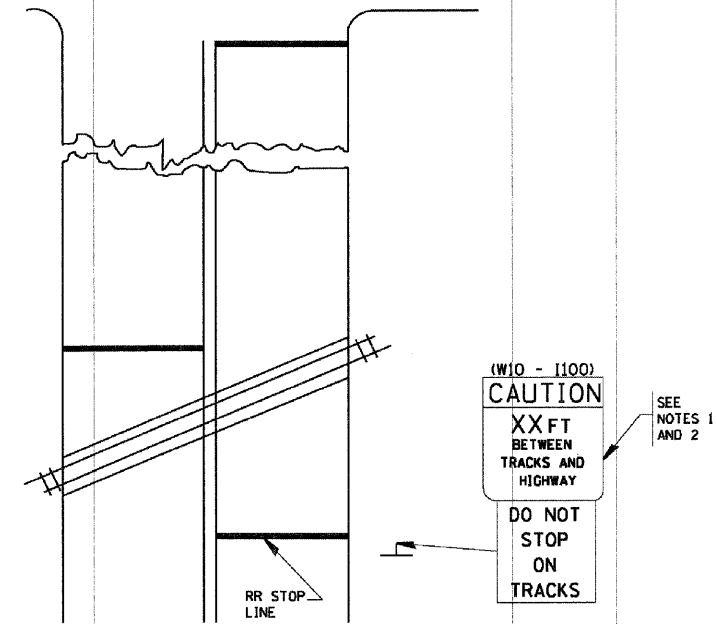
WITH INTERSECTION TRAFFIC SIGNALS



WITH NEAR-SIDE TRAFFIC SIGNALS



WITH NONSIGNALIZED INTERSECTION  
81' (25 m) OR LESS TO CLOSEST RAIL



NOTES:

1. PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
2. WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED, THE PAVEMENT MARKINGS EXTENDS TO THE INTERSECTION.
3. WHERE THE ANGLE BETWEEN THE DIAGONAL STRIPES AND THE TRACK ( $\phi$ ) WOULD BE LESS THAN APPROXIMATELY 20°, THE STRIPES SHOULD BE SLOPED IN THE OPPOSITE DIRECTION FROM THAT SHOWN.

NOTE:

1. DISTANCE TO BE SHOWN ON SIGN MEASURED FROM A POINT 6 FEET (1.8 m) FROM THE RAIL CLOSEST TO THE INTERSECTION TO THE STOP LINE OR CROSSWALK, WHICHEVER IS CLOSEST, ROUNDED DOWN TO THE NEAREST 5 FEET (1.5 m). WHERE THERE IS NO STOP LINE, MEASURE TO POINT WHERE THE DRIVER HAS A VIEW OF APPROACHING TRAFFIC.
2. THE CLEARANCE SIGN IS ALSO TO BE USED AS AN INTERIM MEASURE AT LOCATIONS WITH INTERCONNECTED INTERSECTION TRAFFIC SIGNALS WHERE IT IS PLANNED TO CHANGE THEM TO NEAR-SIDE SIGNALS AT A FUTURE TIME. IN THIS CASE, THE DISTANCE TO BE SHOWN ON THE SIGN IS MEASURED FROM THE EDGE OF THE STRIPED-OUT AREA INSTEAD OF 6 FEET FROM THE RAIL. THE SIGN IS TO BE REMOVED WHEN THE NEAR-SIDE SIGNALS ARE INSTALLED AND THE PAVEMENT MARKINGS EXTEND TO THE INTERSECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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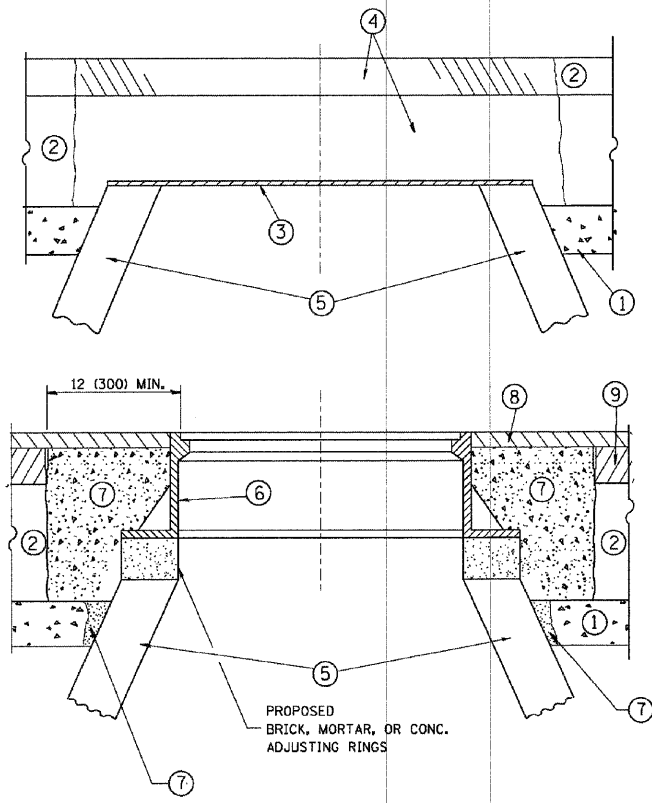
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING  
TREATMENT FOR RAILROAD CROSSINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.L. RTE. 1314	SECTION 11-00182-00-RS	COUNTY KANE	TOTAL SHEETS 25	SHEET NO. 20
TC-23		CONTRACT NO. 63689		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

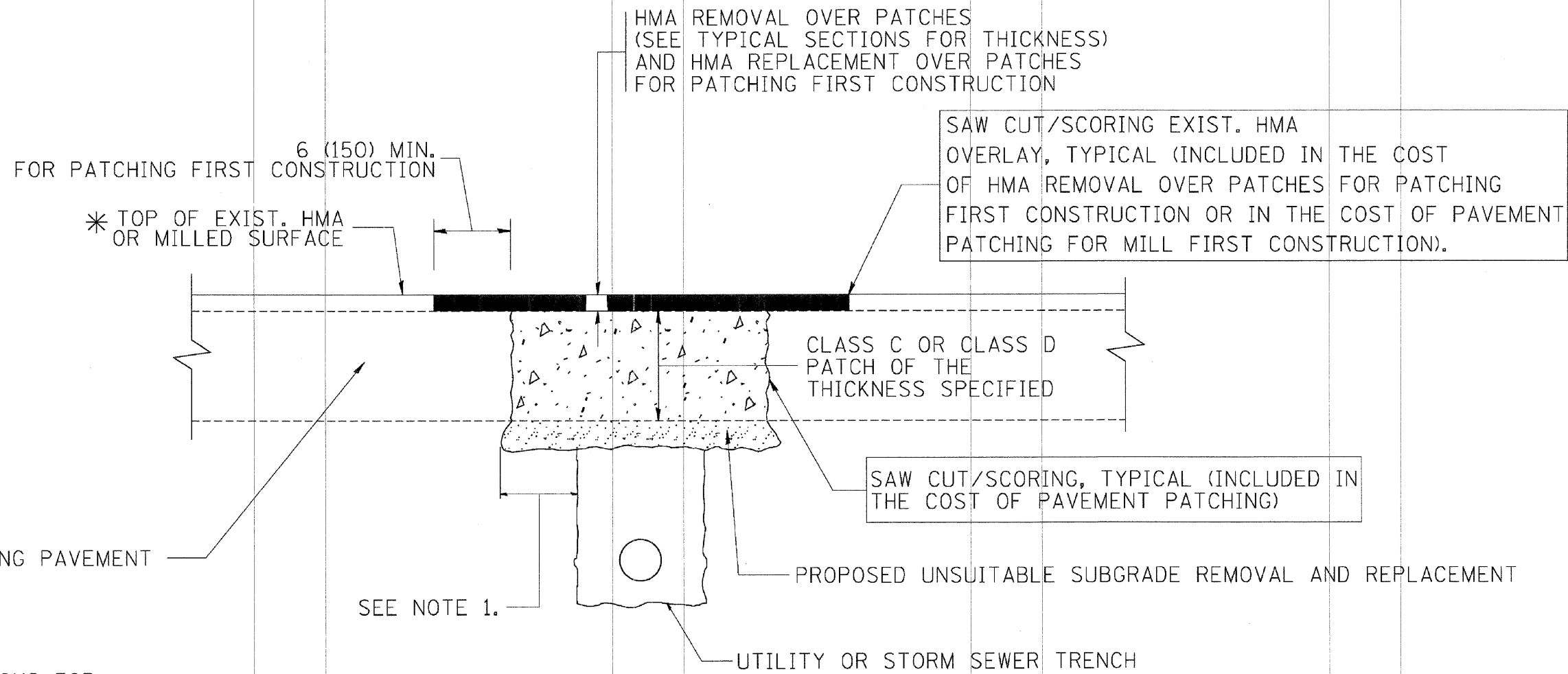
**DETAILS FOR  
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1314	11-00182-00-RS	KANE	25	21
<b>BD600-03 (BD-8)</b>			<b>CONTRACT NO. 63689</b>	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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PLOT DATE = 10/27/2008		DATE - 10-25-94	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT PATCHING FOR  
HMA SURFACED PAVEMENT**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1914	11-00162-00-B5	KANE	25	22
BD400-04 (BD-22)			CONTRACT NO. 63689	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001

18" (450) MAX.

EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

1/4" (5) \*\*

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

3" (75) MIN.

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY. SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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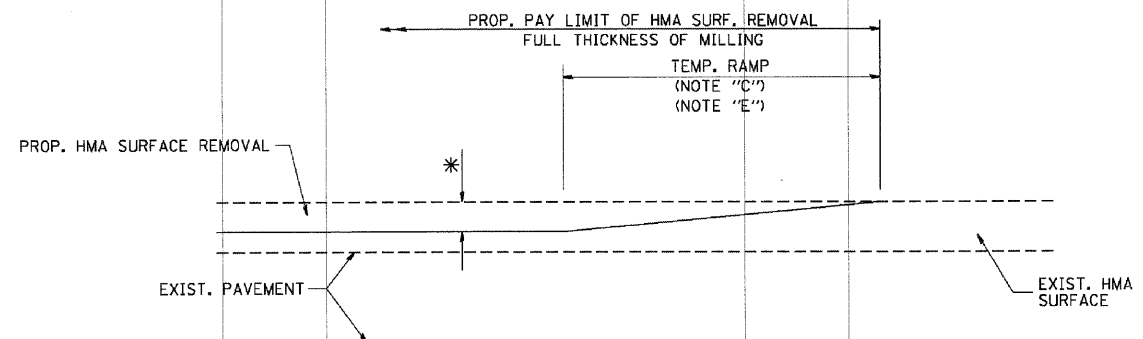
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PLOT DATE = 12/15/2009		DATE - 03-11-94	REVISED - R. BORO 12-15-09

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

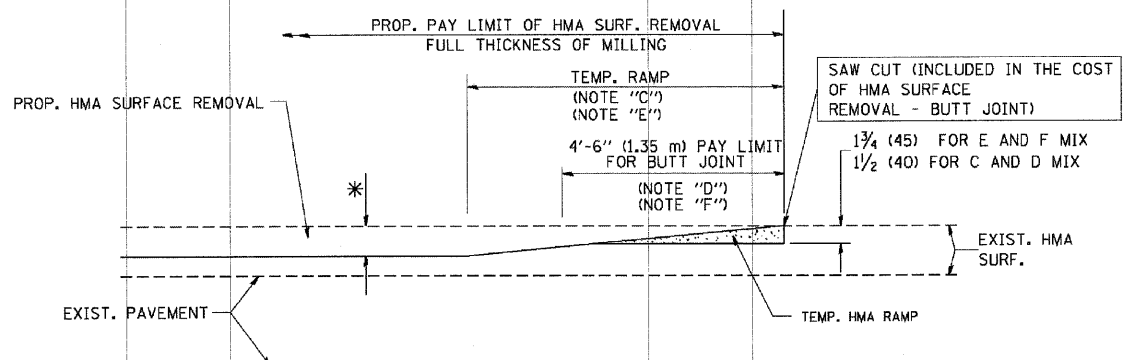
**CURB OR CURB AND GUTTER  
REMOVAL AND REPLACEMENT**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

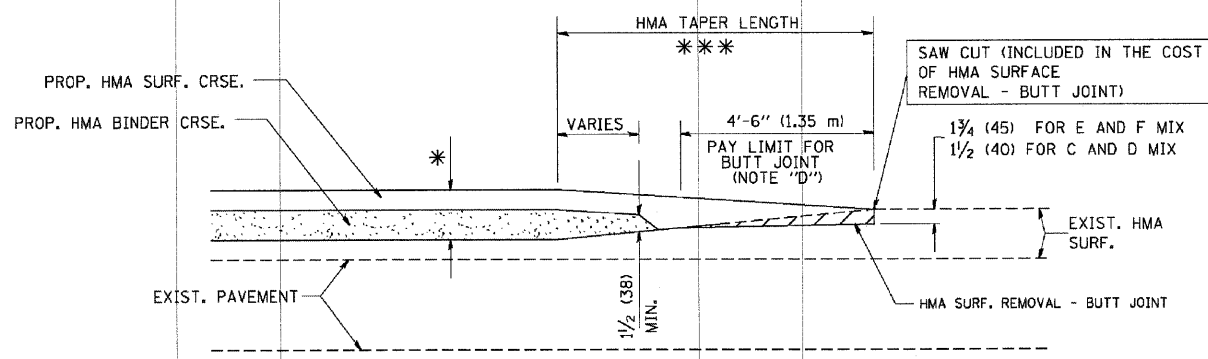
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BD600-06 (BD-24)			CONTRACT NO. 63689	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



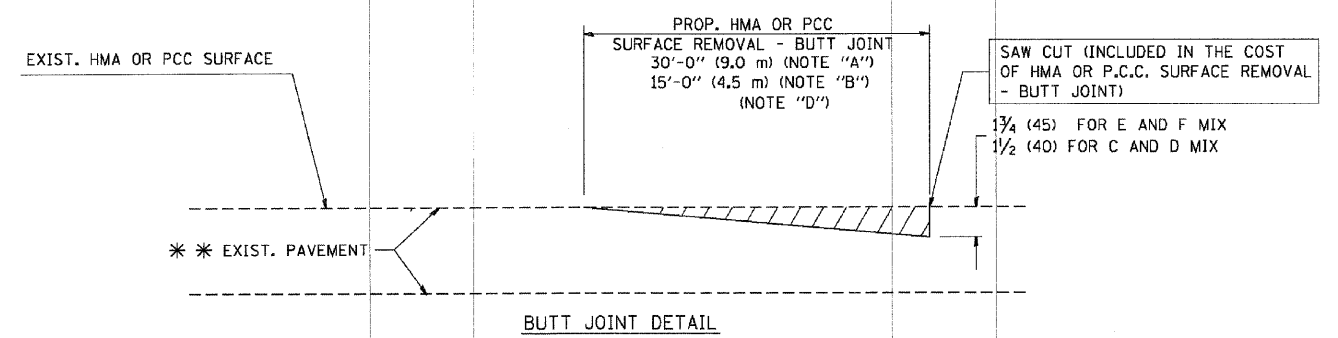
MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)  
**OPTION 1**



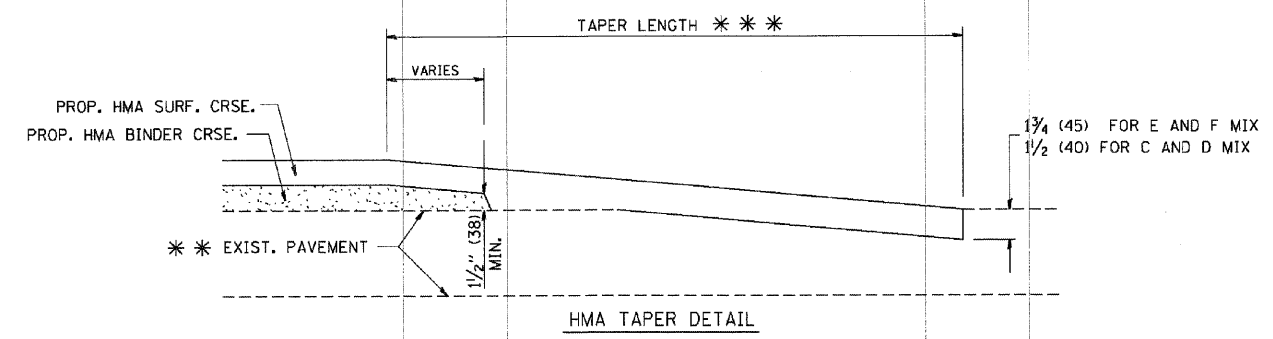
HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)  
**OPTION 2**  
**TYPICAL TEMPORARY RAMP**



**BUTT JOINT AND HMA TAPER**  
**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0\" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6\" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\* 20'-0\" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0\" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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PLOT DATE = 1/4/2008	DATE - 06-13-90		REVISED - M. GOMEZ 04-06-01
			REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND HMA TAPER DETAILS**

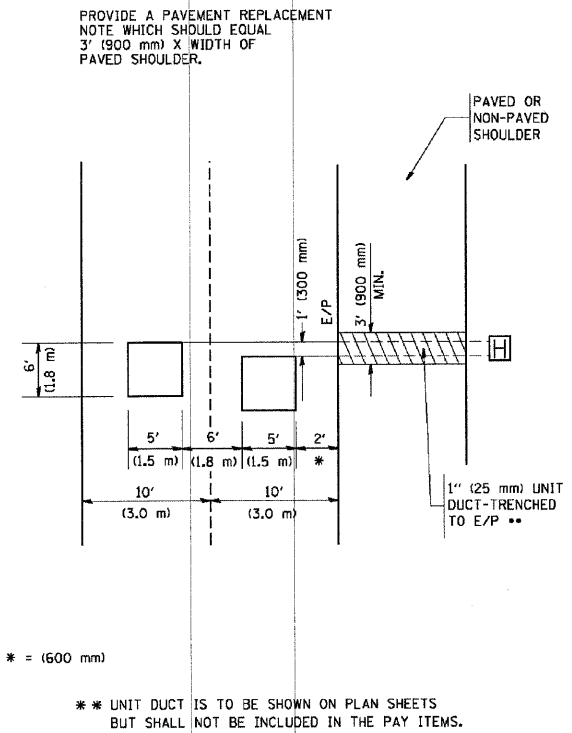
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BD400-05 BD32			CONTRACT NO. 63689	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

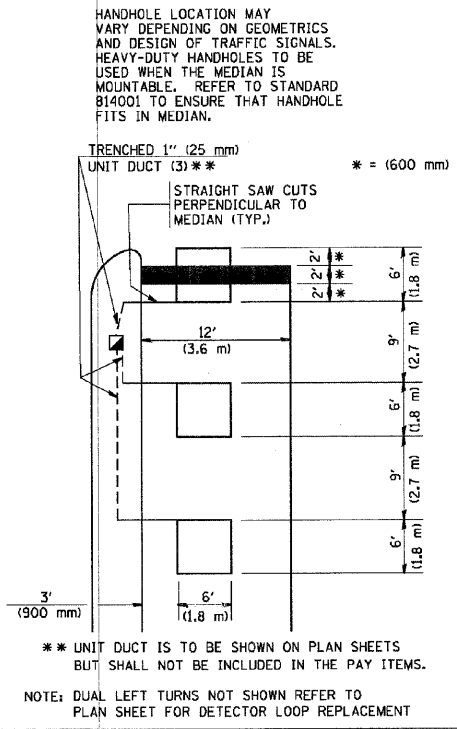
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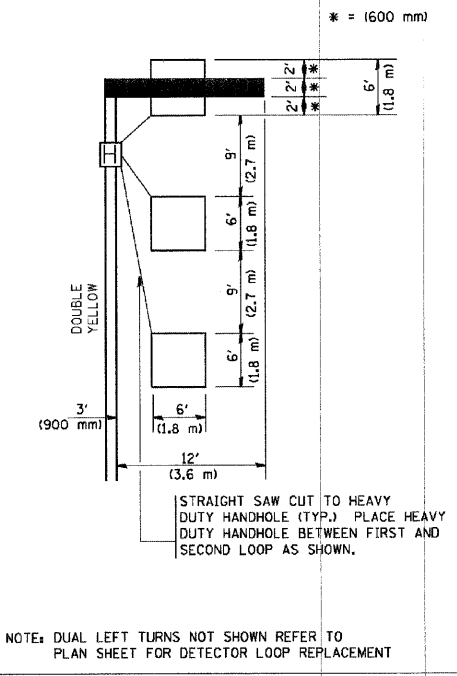
**LOOPS NEXT TO SHOULDERS**



**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)**



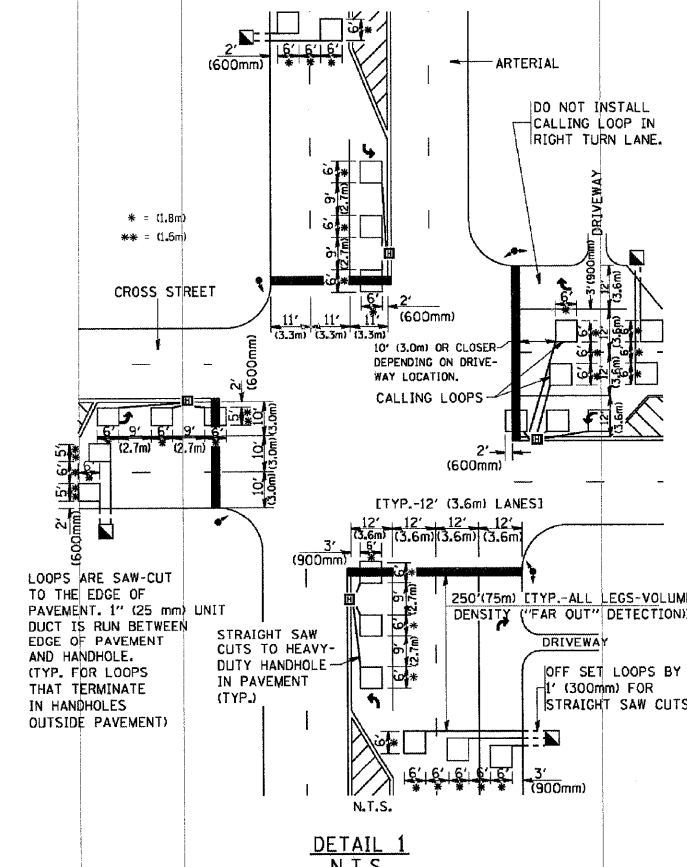
**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)**



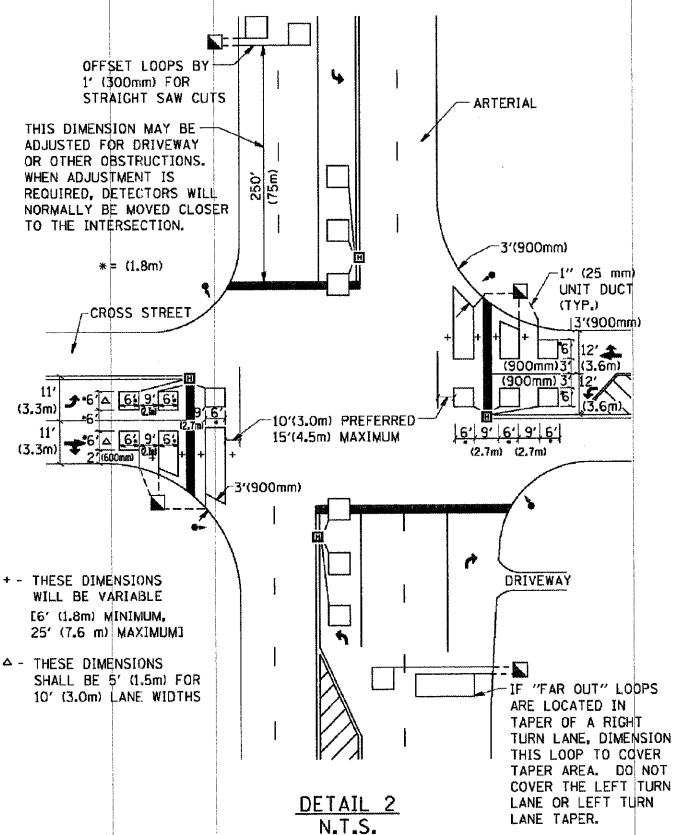
**NOTES:**

- VEHICLES LOOP DETECTORS**
- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
  - \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
  - \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
  - \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
  - \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
  - \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
  - \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN LABEL CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION  
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.R.T. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TS-07			CONTRACT NO. 63689	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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