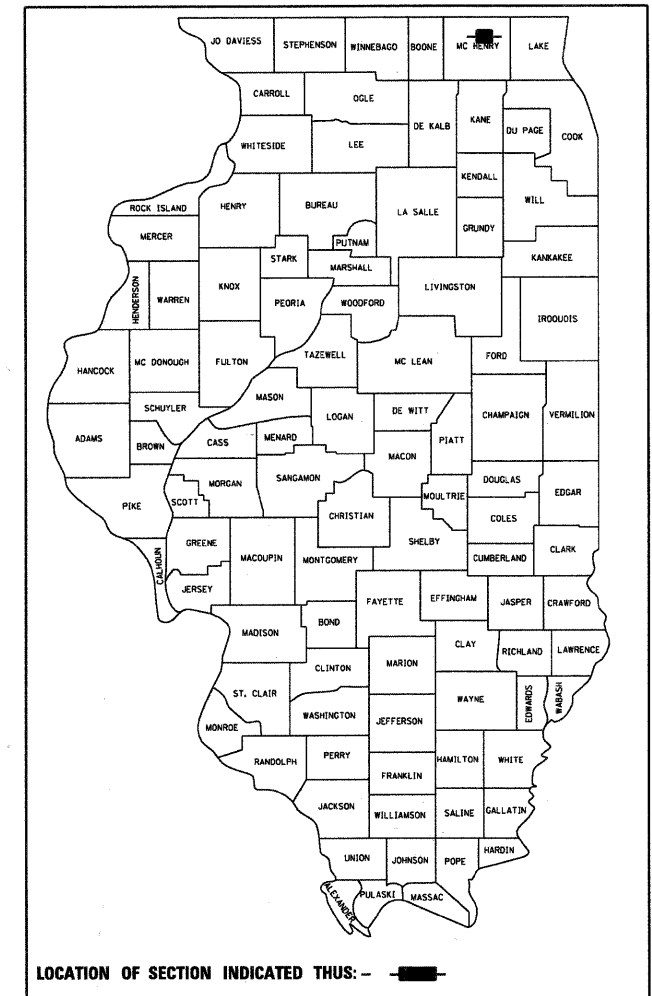


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY**

FAU ROUTE 4083 (BARNARD MILL ROAD)
FAU 0166 (RINGWOOD ROAD) TO FAU 4083 (HOWE ROAD)
RESURFACING
SECTION 11-00004-00-RS
PROJECT M-9003 (900)
VILLAGE OF RINGWOOD
MCHENRY COUNTY
C-91-173-12

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4083	11-00004-00-RS	MCHENRY	17	1
		ILLINOIS	CONTRACT NO. 63696	



FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR HIGHWAY STANDARDS, SEE SHEET NO. 2

TRAFFIC DATA

ADT:
BARNARD MILL ROAD 4,950 VPD (2009)

POSTED SPEED

45 MPH (EXISTING)
45 MPH (PROPOSED)

DESIGN SPEED

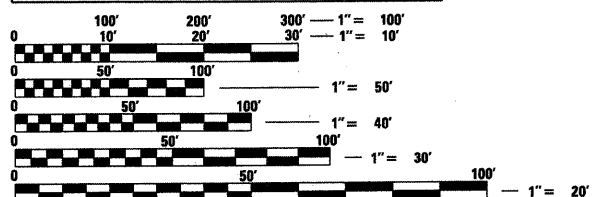
45 MPH (EXISTING)
45 MPH (PROPOSED)

DESIGN DESIGNATION

FAU 4083 (BARNARD MILL ROAD) - COLLECTOR

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. 847-705-4406 SCHAUMBURG, IL

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION CALL 811



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

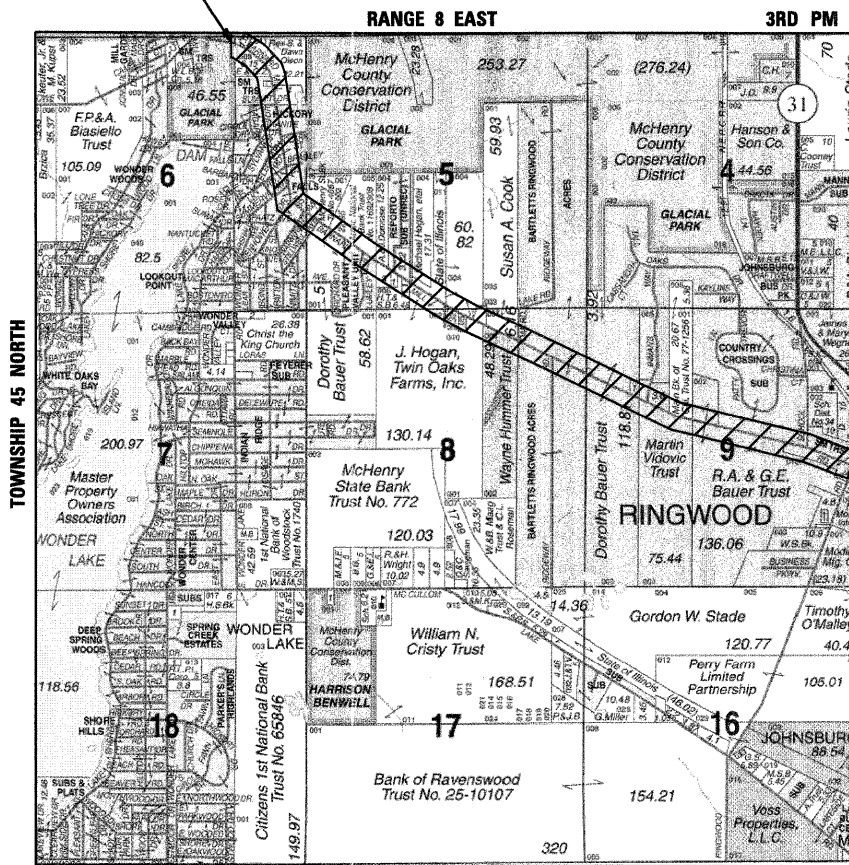
420 NORTH FRONT STREET, SUITE 100 | MCHENRY, ILLINOIS 60050
Phone: 815.385.1778 | Toll Free: 800.728.7805 | Fax: 815.385.1781 | HRGreen.com
ILLINOIS PROFESSIONAL DESIGN FIRM #184-001322

PROJECT ENGINEER: J. STRZALKA
PROJECT MANAGER: A. CHAUDHRY

CONTRACT NO. 63696

PROJECT BEGIN
STA 101+88

PROJECT LOCATION MAP
MCHENRY TOWNSHIP
N.T.S.



PROJECT END
STA 258+69

PROJECT LENGTH
NET LENGTH OF IMPROVEMENT (BARNARD MILL ROAD) = 15,681 FT (2.97 MI)
GROSS LENGTH OF PROJECT = 15,681 FT (2.97 MI)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED February 10th 2012
Richard E. Mack
VILLAGE OF RINGWOOD

PASSED FEBRUARY 21 2012
Chad E. Clark
DISTRICT ONE ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID
BASED ON LIMITED REVIEW FEBRUARY 23 2012
Diana M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

PROFESSIONAL ENGINEER'S SIGN & SEAL

Akram Chaudhry
AKRAM CHAUDHRY, P.E.
EXPIRES: 11-30-2013

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

COMPANY NAME: HRGreen
PROJECT CONTACT: RICK MACK
CLIENT: VILLAGE OF RINGWOOD
DATE PLOTTED: 2/1/2012 2:02:07 PM
FILE NAME: 8610277-cvr.dgn
PLOT DRIVER: STANDARD-TRANS.TBL

INDEX OF SHEETS

- 1 COVER SHEET
- 2 INDEX OF SHEETS, HIGHWAY STANDARDS, BENCHMARKS AND GENERAL NOTES
- 3 SUMMARY OF QUANTITIES
- 4 TYPICAL SECTIONS
- 5 SCHEDULE OF QUANTITIES
- 6-11 ROADWAY PLANS
- 12-17 CONSTRUCTION DETAILS

HIGHWAY STANDARDS

- 000001-06 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 280001-06 TEMPORARY EROSION CONTROL SYSTEMS
- 442201-03 CLASS C AND D PATCHES
- 482011-03 HMA SHLD STRIPS / SHLDS WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
- 701101-02 OFF-ROAD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
- 701311-03 LANE CLOSURE 2L, 2W MOVING OPERATIONS - DAY ONLY
- 701501-06 URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
- 701901-02 TRAFFIC CONTROL DEVICES
- 720001-01 SIGN PANEL MOUNTING DETAILS
- 720006-03 SIGN PANEL ERECTING DETAILS
- 781001-03 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
- BLR 24-2 MAILBOX TURNOUT FOR LOCAL ROADS

BENCHMARKS

BM1: CHISELED SQUARE IN TOP OF CONCRETE ABUTMENT LOCATED IN THE SOUTHWEST CORNER OF BARNARD MILL ROAD BRIDGE OVER THE NIPPERCREEK CREEK. ELEVATION=789.26 NAVD88

GENERAL NOTES

1. ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT), JANUARY 1, 2012.
2. ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.
3. PRIOR TO COMMENCEMENT OF CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS AFFECTING THEIR WORK WITH THE ACTUAL CONDITIONS AT THE JOB SITE. IN ADDITION, THE CONTRACTOR MUST VERIFY THE ENGINEER'S LINE AND GRADE STAKES. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, HE MUST IMMEDIATELY REPORT SAME TO THE ENGINEER BEFORE DOING ANY WORK. OTHERWISE THE CONTRACTOR ASSUMES FULL RESPONSIBILITY. IN THE EVENT OF DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS, STANDARD SPECIFICATIONS AND/OR SPECIAL DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTIONS FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY OMISSIONS OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTIONS, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS OWN RISK AND EXPENSE. IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTION PLANS OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.
4. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THE PROJECT.
5. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION) AT 8-1-1 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION IS REQUIRED).
6. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR VILLAGE PROPERTY WITHOUT WRITTEN PERMISSION FROM IDOT OR VILLAGE.
7. SAW CUTTING OF PAVEMENTS, AND OTHER SURFACES SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM REMOVED.
8. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, HIS AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
9. WHERE NEW WORK MEETS EXISTING FEATURES TO REMAIN, FIELD CHECK ALL DIMENSIONS AND ELEVATIONS BEFORE PROCEEDING WITH CONSTRUCTION. NOTIFY THE ENGINEER IMMEDIATELY OF ANY DISCREPANCIES.
10. THE INDISCRIMINATE USE OF FIRE HYDRANTS, EXISTING STREAMS, CREEKS, WETLANDS, OR PONDS IS STRICTLY PROHIBITED. THE CONTRACTOR SHALL PROVIDE A WATER TRUCK AND DRIVER AS REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING WATER FROM AN APPROVED SOURCE. IF THIS WATER IS FROM A SOURCE OTHER THAN HIS YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE OF THE WATER MUST BE RECEIVED BY THE CONTRACTOR PRIOR TO USE OF THE WATER.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SWEEPING AND CLEANING STREETS OF ANY DEBRIS AND MATERIAL THAT HAS ACCUMULATED AS A RESULT OF THE CONSTRUCTION ACTIVITY. A MECHANICAL SWEEPER, MECHANICALLY DRIVEN AIR AND HANDWORK WITH SHOVEL AND BROOM SHALL BE UTILIZED TO PROVIDE A CLEAN STREET FOR THE MOTORING PUBLIC. WITHIN 24 HOURS OF PLACING PRIME COAT AND THE LAYING OF HMA, THE CONTRACTOR SHALL SWEEP THE PAVEMENT AND REMOVE STANDING WATER, EARTH, WEEDS, LEAVES, DIRT, CONSTRUCTION DEBRIS AND ALL LOOSE MATERIAL.
12. THE CONTRACTOR WILL BE REQUIRED TO TEMPORARILY RESET ALL SUCH SIGNS THAT INTERFERE WITH CONSTRUCTION OPERATIONS. ALL SUCH SIGNS MUST BE MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF THE TEMPORARY SETTING AND MUST BE RE-ERECTED AT A TEMPORARY LOCATION IN A WORKMANLIKE MANNER AND BE VISIBLE TO THE TRAFFIC FOR WHICH IT IS INTENDED. THIS WORK WILL NOT BE PAID FOR SEPERATELY, BUT SHALL BE INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.
13. THE REMOVAL OF SHORT TERM PAVEMENT MARKINGS WILL NOT BE PAID FOR SEPARATELY BUT INCLUDED IN THE UNIT COST OF THE HOT-MIX ASPHALT PAVEMENT COURSE BEING CONSTRUCTED.
14. TEMPORARY RAMPS WILL BE CONSTRUCTED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS AND THE CONSTRUCTION DETAILS. THE INSTALLATION AND REMOVAL OF THE TEMPORARY RAMPS WILL NOT BE PAID FOR SEPARATELY BUT INCLUDED IN THE CONTRACT UNIT PRICE FOR HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT.
15. AGGREGATE SHOULDERS, TYPE B SHALL BE CONSTRUCTED WITH GRADATION CA-6 CRUSHED GRAVEL OR CRUSHED STONE. RECYCLED CONCRETE WILL NOT BE PERMITTED.
16. THE THICKNESS OF THE AGGREGATE SHOULDER SHALL EQUAL THE RESURFACING THICKNESS AT THE EDGE OF PAVEMENT AND WILL BE TAPERED TO ONE INCH (1") AT THE EDGE OF SHOULDER. THE CONTRACTOR WILL CONSTRUCT AND COMPACT THE SHOULDER TO THE SATISFACTION OF THE ENGINEER.
17. EXISTING AGGREGATE DRIVEWAYS WILL RECEIVE A TWENTY-FOUR INCH (24") WIDE ASPHALT APRON MATCHING THE PROPOSED RESURFACING LIFT THICKNESS. THE DRIVEWAY APRON WILL BE CONSTRUCTED IN CONJUNCTION WITH THE MAINLINE PAVING AND WILL NOT BE PAID FOR SEPARATELY.
18. EXISTING ASPHALT DRIVEWAYS WILL RECEIVE A FORTY-EIGHT INCH (48") WIDE ASPHALT APRON MATCHING THE PROPOSED RESURFACING LIFT THICKNESS. THE DRIVEWAY APRON WILL BE CONSTRUCTED IN CONJUNCTION WITH THE MAINLINE PAVING AND WILL NOT BE PAID FOR SEPARATELY.

REMOVAL OF THE EXISTING DRIVEWAY APRON WILL BE SAWCUT AND MILLED TO THREE AND THREE QUARTER INCHES (3 3/4") FOR A SATISFACTORY TRANSITION BETWEEN THE ROADWAY PAVEMENT AND THE DRIVEWAY PAVEMENT REMAINING AT EXISTING GRADE. THE REMOVAL WILL BE INCLUDED IN THE CONTRACT UNIT COST FOR HOT-MIX ASPHALT SURFACE REMOVAL, 2" WHEN THE DRIVEWAY TRANSITION IS FORTY-EIGHT INCHES (48") OR LESS FROM THE EDGE OF PAVEMENT.

GENERAL NOTES (CONT.)

19. HOT-MIX ASPHALT DRIVEWAY APRONS EXCEEDING FORTY-EIGHT INCHES (48") FROM THE EDGE OF PAVEMENT TO THE SAW CUT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT IN ACCORDANCE WITH THE SPECIAL PROVISIONS.
20. EXISTING CONCRETE AND BRICK DRIVEWAYS WILL RECEIVE A SAW CUT ALONG THE FACE THE DRIVEWAY.
21. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.
22. THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.
23. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MIGHT NOT BE SHOWN ON THE PLANS. ANY UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE OWNER AT THE CONTRACTOR'S EXPENSE.
24. THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENANCES THAT MUST BE KEPT IN OPERATION.
25. THE CONTRACTOR SHALL ENSURE THAT ALL WATER SYSTEM VALVES, VALVE VAULTS, AND SANITARY SEWER MANHOLES REMAIN READILY ACCESSIBLE TO THE VILLAGE FOR EMERGENCY OPERATIONS. THE LOCATIONS OF ALL WATER AND SANITARY FACILITIES SHALL BE MARKED AND READILY VISIBLE AT ALL TIMES.
26. PIPE CULVERTS CLASS C SHALL BE CORRUGATED METAL PIPE. NO OTHER PIPE MATERIAL WILL BE PERMITTED UNLESS OTHERWISE NOTED ON THE PLANS.
27. TRENCH BACKFILL FOR PIPE CULVERTS WILL BE MEASURED AND PAID FOR IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.

THE MAXIMUM TRENCH WIDTHS HAVE BEEN DETERMINED IN ACCORDANCE WITH THE IDOT TRENCH BACKFILL TABLES AS FOLLOWS:

WHEN TRENCH IS LESS THAN FIVE FEET (5') IN DEPTH:
9" + OUTSIDE PIPE DIAMETER + 9"

WHEN TRENCH IS GREATER THAN FIVE FEET (5') IN DEPTH:
18" + OUTSIDE PIPE DIAMETER + 18"
28. CONTRACTOR SHALL MILL BEFORE PERFORMING CLASS D PATCHES. HMA SURFACE REMOVAL OVER PATCHES AND HMA REPLACEMENT OVER PATCHES WILL BE CONSTRUCTED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS AND STANDARD DETAIL AT PIPE CULVERT REPLACEMENT LOCATIONS PRIOR TO MILLING.
29. SIGN PANELS OF THE TYPE SPECIFIED WILL BE MOUNTED AND INSTALLED TO EXISTING SIGN POSTS REMAINING. THE REMOVAL OF EXISTING SIGN PANELS WILL BE INCLUDED IN THE CONTRACT UNIT COST OF SIGN PANEL. REMOVED SIGN PANELS WILL BE STORED AT THE RINGWOOD VILLAGE HALL FOR PICKUP BY THE VILLAGE OR ROAD DISTRICT. ADDITIONAL HARDWARE, IF REQUIRED TO PROPERLY MOUNT THE SIGN PANEL, WILL BE INCLUDED IN THE CONTRACT UNIT COST OF SIGN PANEL.
30. ALL SIGNS SHALL CONFORM WITH MUTCD AND INCLUDE DIAMOND GRADE DG3 REFLECTIVE SHEETING.
31. A CONTINGENCY QUANTITY OF METAL POSTS HAS BEEN INCLUDED FOR USE BY THE ENGINEER TO REPLACE DETERIORATED OR BROKEN POSTS.
32. FOR STORM SEWER CONSTRUCTED UNDER THE ROADWAY, BACKFILLING METHODS TWO AND THREE AUTHORIZED UNDER THE PROVISIONS OF ARTICLE 550.07 OF THE STANDARD SPECIFICATIONS WILL NOT BE ALLOWED.
33. EXISTING PIPE CULVERTS TO BE REPLACED, AS SHOWN ON THE PLANS, SHALL BE REPLACED IN KIND. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE SIZE AND TYPE OF PIPE TO MATCH EXISTING. THE REPLACEMENT CULVERTS SHALL BE INSTALLED IN KIND AT THE IDENTICAL LOCATION, ELEVATION AND MATERIAL TYPE.
34. AT PIPE CULVERT REMOVAL AND REPLACEMENT LOCATIONS, TRENCH BACKFILL SHALL BE PLACED TO THE TOP OF TRENCH AS A TEMPORARY CONDITION UNTIL THE FULL-DEPTH PATCH OR DRIVEWAY PAVEMENT IS CONSTRUCTED. MAINTENANCE OF THE TRENCH BACKFILL SURFACE IN THE TEMPORARY CONDITION WILL BE THE RESPONSIBILITY OF THE CONTRACTOR. REMOVAL OF THE TRENCH BACKFILL TO CONSTRUCT THE PATCH OR DRIVEWAY PAVEMENT WILL NOT BE PAID FOR SEPARATELY. THIS WORK WILL BE INCLUDED IN THE COST OF TRENCH BACKFILL.

BOXED ITEMS INDICATE WORK INCIDENTAL TO THE CONTRACT OR BY OTHERS.

SUMMARY OF QUANTITIES

SPECIALTY ITEMS (Δ)	SPECIAL PROVISION (*)	PAY ITEM NUMBER	PAY ITEM DESCRIPTION	UNITS	TOTAL QUANTITY	ROADWAY 80% FEDERAL 20% LOCAL 0005
		20800150	TRENCH BACKFILL	CU YD	155	155
	*	21400100	GRADING AND SHAPING DITCHES	FOOT	1,227	1,227
		28100805	STONE DUMPED RIPRAP, CLASS A3	TON	13	13
		28200200	FILTER FABRIC	SQ YD	20	20
		40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	4,790	4,790
		40600300	AGGREGATE (PRIME COAT)	TON	96	96
		40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	160	160
		40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	10	10
		40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	6,065	6,065
		40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	4,340	4,340
		44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	47,585	47,585
		44000163	HOT-MIX ASPHALT SURFACE REMOVAL, 3 1/2"	SQ YD	1,705	1,705
		44002209	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 2 1/4"	SQ YD	70	70
		44201717	CLASS D PATCHES, TYPE II, 6 INCH	SQ YD	190	190
		44201721	CLASS D PATCHES, TYPE III, 6 INCH	SQ YD	290	290
		44201723	CLASS D PATCHES, TYPE IV, 6 INCH	SQ YD	290	290
		48101200	AGGREGATE SHOULDERS, TYPE B	TON	1,177	1,177
		542C0217	PIPE CULVERTS, CLASS C, TYPE 1 12"	FOOT	286	286
		542C0220	PIPE CULVERTS, CLASS C, TYPE 1 15"	FOOT	339	339
		542C1060	PIPE CULVERTS, CLASS C, TYPE 2 15"	FOOT	160	160
		542C1063	PIPE CULVERTS, CLASS C, TYPE 2 18"	FOOT	139	139
		67100100	MOBILIZATION	L SUM	1	1
		70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1
		70300100	SHORT TERM PAVEMENT MARKING	FOOT	8,950	8,950
Δ		72900200	METAL POST - TYPE B	FOOT	60.0	60.0
Δ		78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	40.00	40.00
Δ		78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	50,164	50,164
Δ		78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	254	254
Δ		78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	120	120
Δ		78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	221	221
Δ		78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	560	560
		78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	395	395
	*	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	77.10	77.10
	*	X5015225	PIPE CULVERT REMOVAL (SPECIAL)	FOOT	959	959
Δ	*	X7200105	SIGN PANEL - TYPE 1 (SPECIAL)	SQ FT	389.50	389.50
	*	XX006343	SEEDING (COMPLETE)	SQ YD	1,644	1,644
	*	XX006947	HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD	1,365	1,365



HRGreen.com
 Illinois Professional Design Firm
 # 194-001322

USER NAME = JStorz
 DESIGNED - JJS
 DRAWN - JJS
 CHECKED - AC
 DATE - 1/31/12

REVISED -
 REVISED -
 REVISED -
 REVISED -

VILLAGE OF RINGWOOD

**BARNARD MILL ROAD
 SUMMARY OF QUANTITIES**

SCALE: NTS SHEET NO. 1 OF 1 SHEETS STA. NA TO STA. NA

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4083	11-00004-00-RS	MCHENRY	17	3
CONTRACT NO.			63696	
ILLINOIS FED. AID PROJECT				

HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

Table with 5 columns: LOCATION, WIDTH (FT), LENGTH (FT), AREA (SF), AREA (SY). Lists various locations like BRIDGE (BEGIN PROJECT), SUMMIT, GRANITE, TELEGRAPH, etc.

PIPE CULVERTS, CLASS C, TYPE 1 12"

Table with 6 columns: LOCATION, STATION, OFFSET, TO STATION, TO OFFSET, LENGTH (FT). Lists pipe culvert details for various stations.

PIPE CULVERTS, CLASS C, TYPE 1 15"

Table with 6 columns: LOCATION, STATION, OFFSET, TO STATION, TO OFFSET, LENGTH (FT). Lists pipe culvert details for various stations.

PIPE CULVERTS, CLASS C, TYPE 2 15"

Table with 6 columns: LOCATION, STATION, OFFSET, TO STATION, TO OFFSET, LENGTH (FT). Lists pipe culvert details for various stations.

PIPE CULVERTS, CLASS C, TYPE 2 18"

Table with 6 columns: LOCATION, STATION, OFFSET, TO STATION, TO OFFSET, LENGTH (FT). Lists pipe culvert details for various stations.

THERMOPLASTIC PAVEMENT MARKING - LINE 4"

Table with 8 columns: LOCATION, STA, OFFSET, TO STA, OFFSET, LENGTH (FT), TYPE, NET LENGTH (FT). Lists thermoplastic pavement marking details.

SIGN PANEL - TYPE 1 (SPECIAL)

WARNING SIGNS

Table with 6 columns: SIGN OR PLAQUE, DESIGNATION, SIZE, TAG NO., STATION, OFFSET. Lists warning signs like CURVE AHEAD - LEFT, REDUCED SPEED AHEAD - 30 MPH, etc.

SIGN PANEL - TYPE 1 (SPECIAL)

REGULATORY SIGNS

Table with 6 columns: SIGN OR PLAQUE, DESIGNATION, SIZE, TAG NO., STATION, OFFSET. Lists regulatory signs like STOP, SPEED LIMIT 30 MPH, DO NOT PASS, etc.

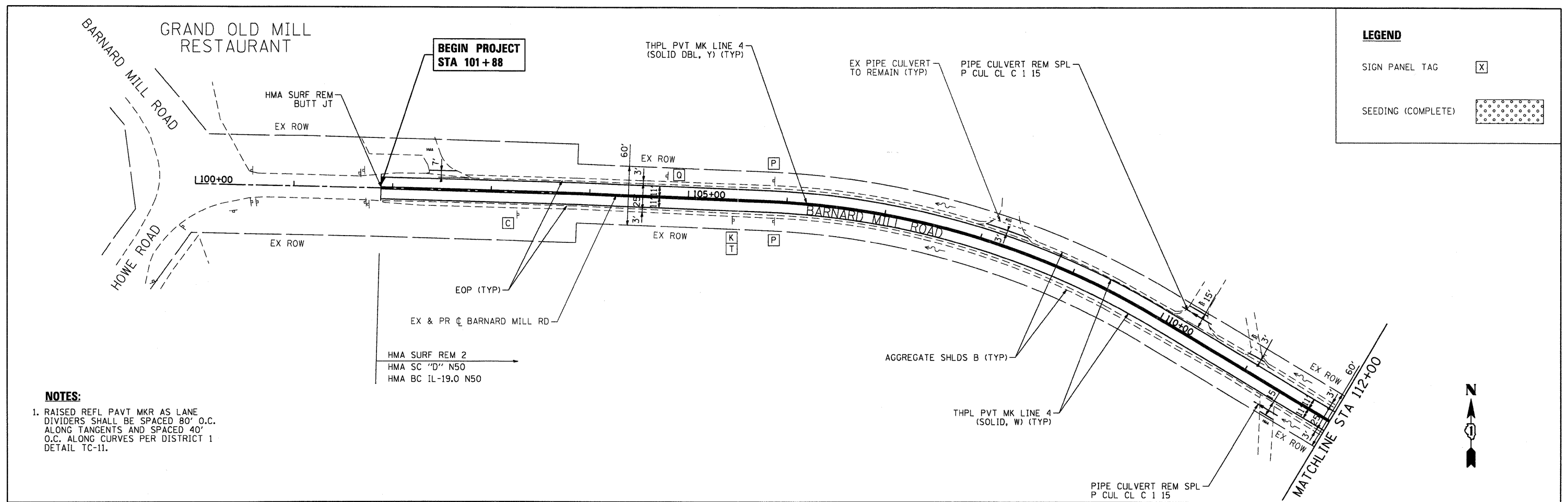
GRADING AND SHAPING DITCHES

Table with 6 columns: LOCATION, STA, OFFSET, TO STA, OFFSET, LENGTH (FT). Lists grading and shaping ditch details.

Project information block including HRGreen logo, user name (JStz01), designer (JJS), drafter (JJS), checker (AC), date (2/1/2012), project name (VILLAGE OF RINGWOOD), and schedule of quantities.

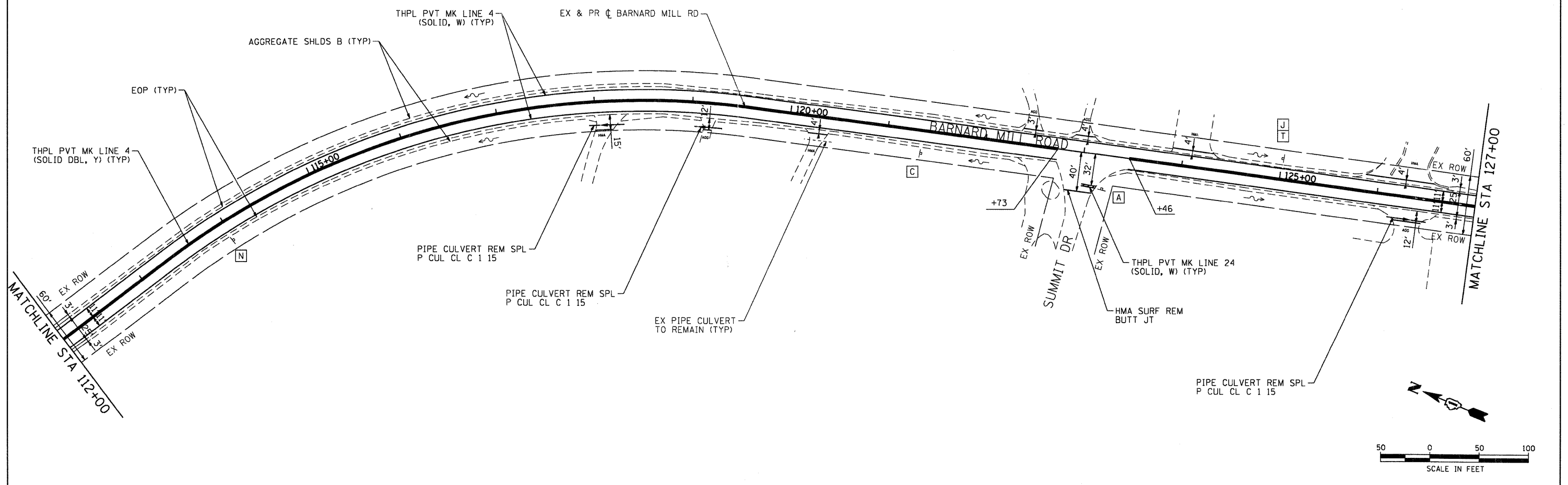
LEGEND

SIGN PANEL TAG	X
SEEDING (COMPLETE)	[Pattern]

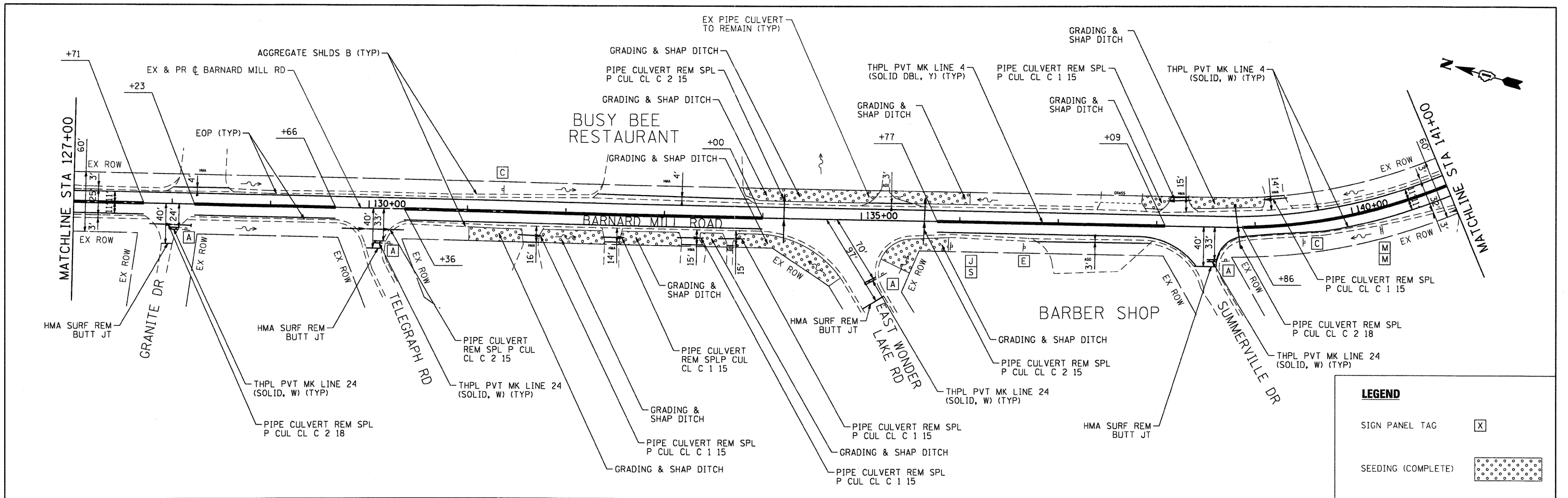


NOTES:

1. RAISED REFL PAVT MKR AS LANE DIVIDERS SHALL BE SPACED 80' O.C. ALONG TANGENTS AND SPACED 40' O.C. ALONG CURVES PER DISTRICT 1 DETAIL TC-11.



<p>HRGreen.com Illinois Professional Design Firm #184-001322</p>	USER NAME = JStroz	DESIGNED - JJS	REVISED -	VILLAGE OF RINGWOOD	BARNARD MILL ROAD ROADWAY PLAN			F.A.U. RTE. 4083	SECTION 11-00004-00-RS	COUNTY MCHENRY	TOTAL SHEETS 17	SHEET NO. 6	
	PLOT SCALE = 1"=50'	CHECKED - AC	REVISED -		SCALE: 1"=50'	SHEET NO. 1 OF 6 SHEETS	STA. BEG TO STA. 127+00	CONTRACT NO. 63696					
	PLOT DATE = 1/31/2012	DATE - 1/31/12	REVISED -		ILLINOIS FED. AID PROJECT								



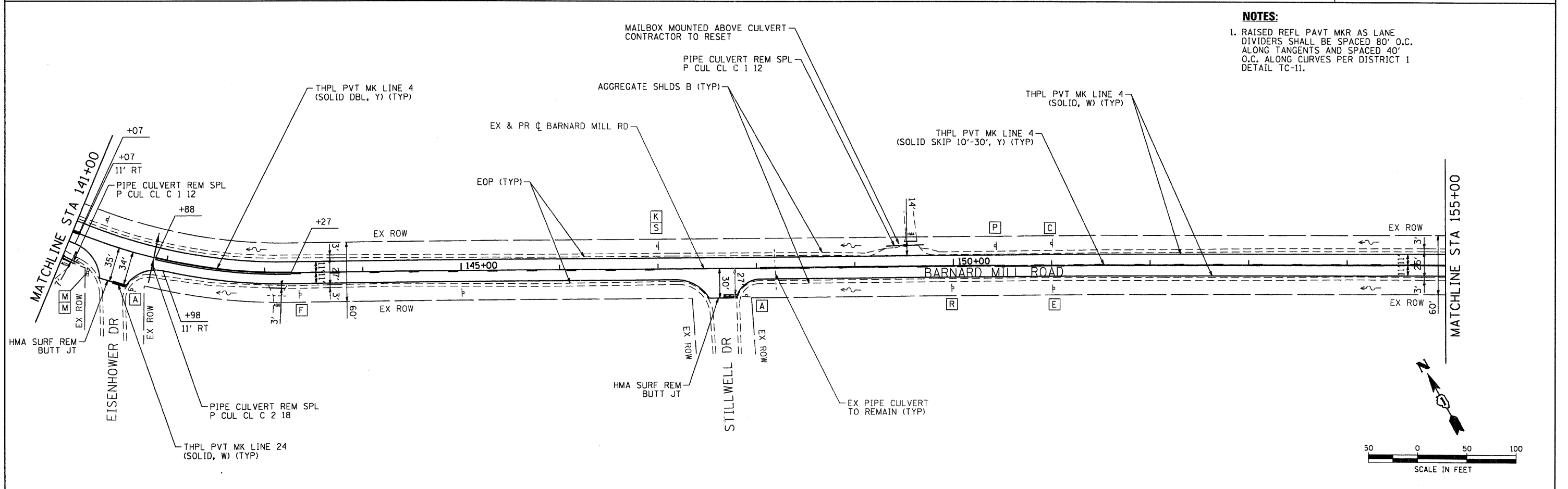
LEGEND

SIGN PANEL TAG X

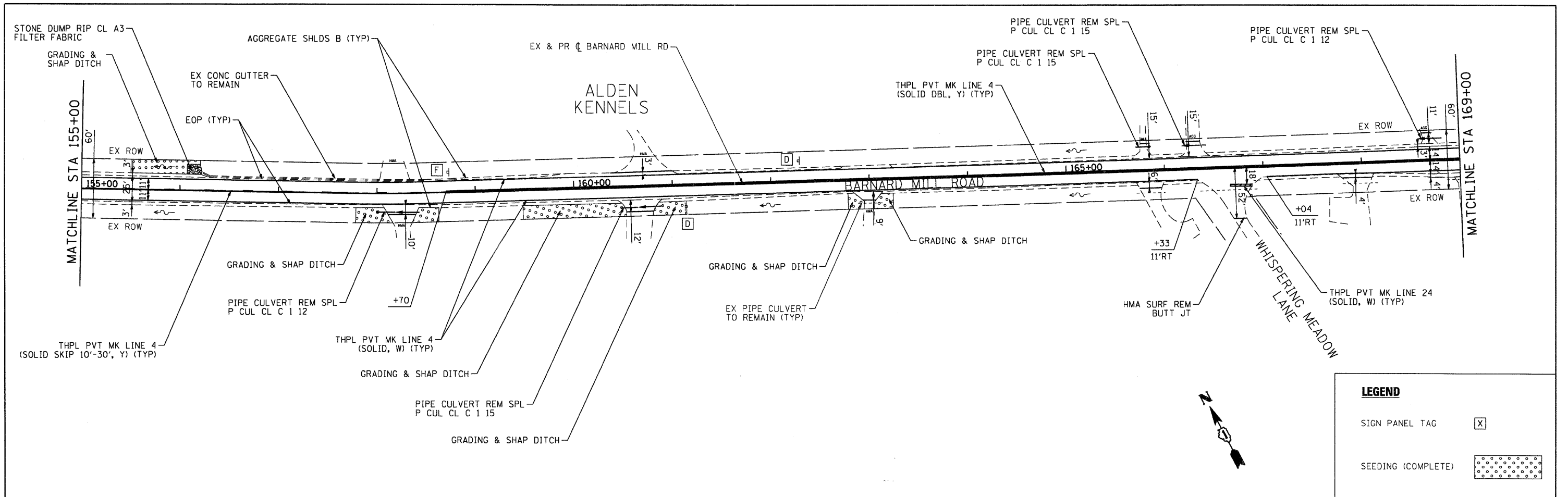
SEEDING (COMPLETE)

NOTES:

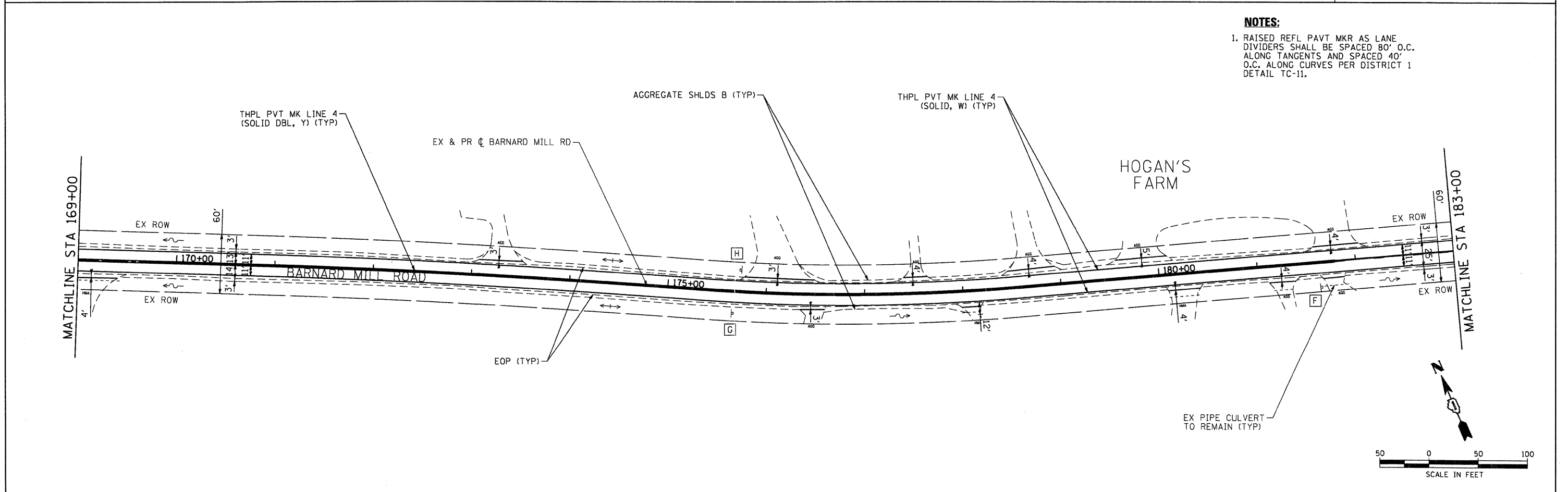
1. RAISED REFL PAVT MKR AS LANE DIVIDERS SHALL BE SPACED 80' O.C. ALONG TANGENTS AND SPACED 40' O.C. ALONG CURVES PER DISTRICT 1 DETAIL TC-11.



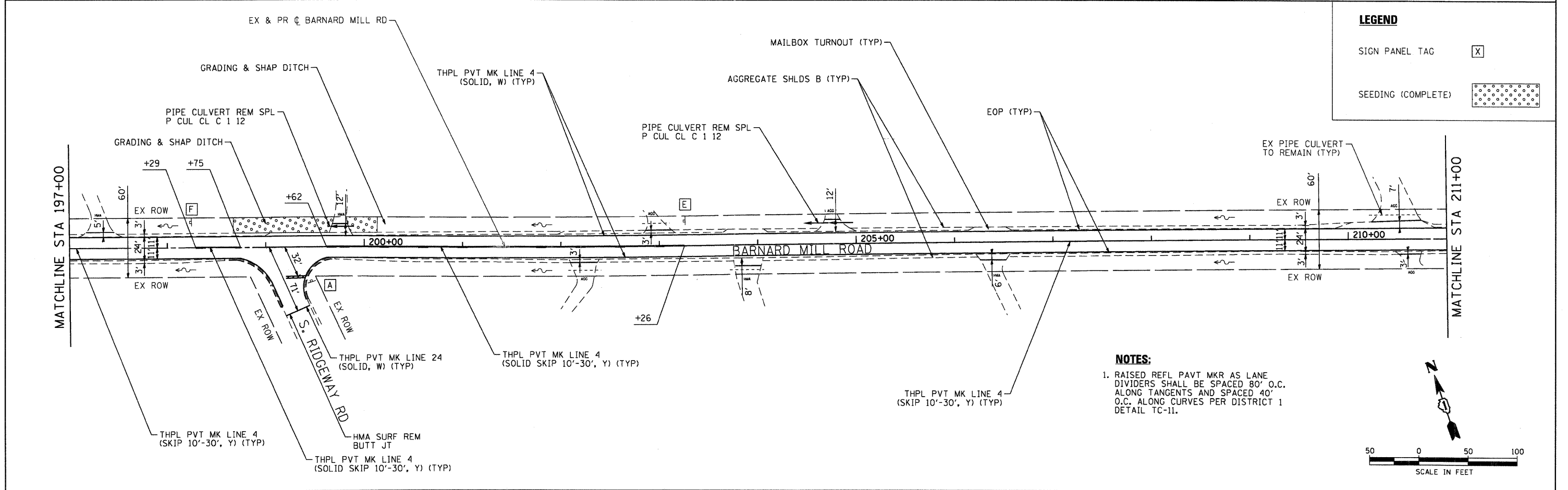
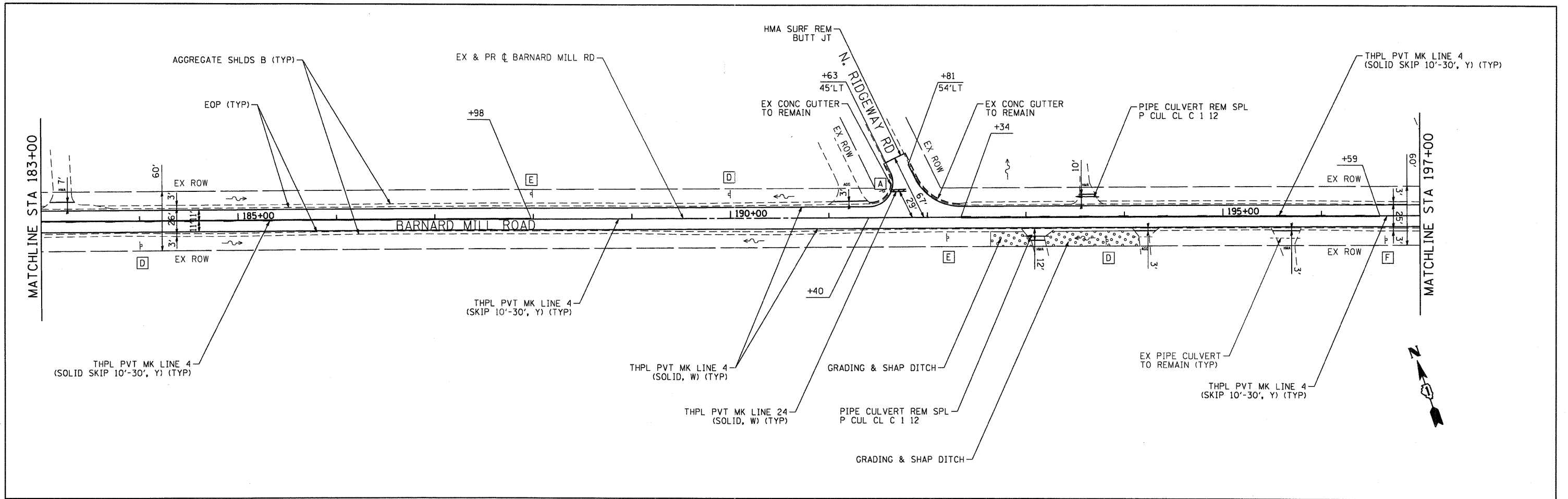
	USER NAME = JStrzel	DESIGNED - JJS	REVISED -	VILLAGE OF RINGWOOD	BARNARD MILL ROAD ROADWAY PLAN			F.A.J. RTE. 4083	SECTION 11-00004-00-RS	COUNTY MCHENRY	TOTAL SHEETS 17	SHEET NO. 7
	PLOT SCALE = 1"=50'	CHECKED - AC	REVISIED -		SCALE: 1"=50'	SHEET NO. 2 OF 6 SHEETS	STA. 127+00	TO STA. 155+00	CONTRACT NO. 63696			
	PLOT DATE = 1/31/2012	DATE = 1/31/12	REVISIED -		ILLINOIS FED. AID PROJECT							



- NOTES:**
1. RAISED REFL PAVT MKR AS LANE DIVIDERS SHALL BE SPACED 80' O.C. ALONG TANGENTS AND SPACED 40' O.C. ALONG CURVES PER DISTRICT 1 DETAIL TC-11.



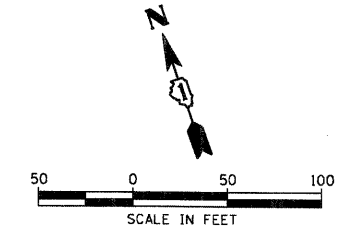
	USER NAME = JSt-zol	DESIGNED - JJS	REVISED -	VILLAGE OF RINGWOOD	BARNARD MILL ROAD ROADWAY PLAN	F.A.U. RTE. 4083	SECTION 11-00004-00-RS	COUNTY MCHENRY	TOTAL SHEETS 17	SHEET NO. 8			
	PLOT SCALE = 1"=50'	CHECKED - AC	REVISED -			SCALE: 1"=50'	SHEET NO. 3 OF 6 SHEETS	STA. 155+00	TO STA. 183+00	CONTRACT NO. 63696			
	PLOT DATE = 2/1/2012	DATE - 2/1/12	REVISED -			ILLINOIS FED. AID PROJECT							



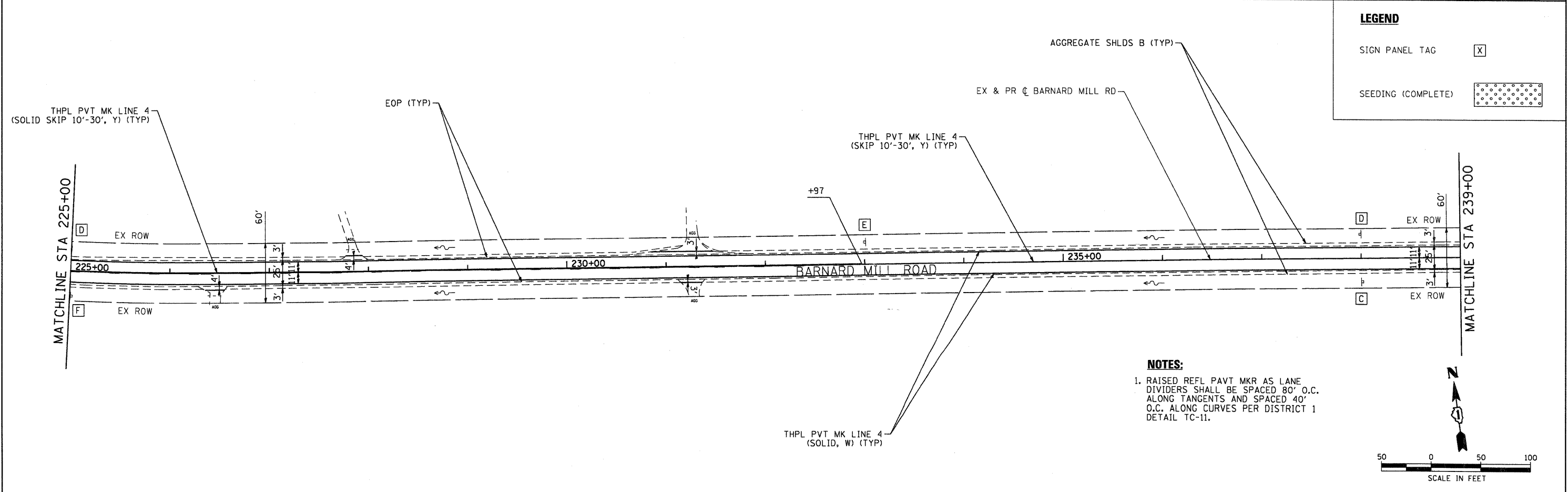
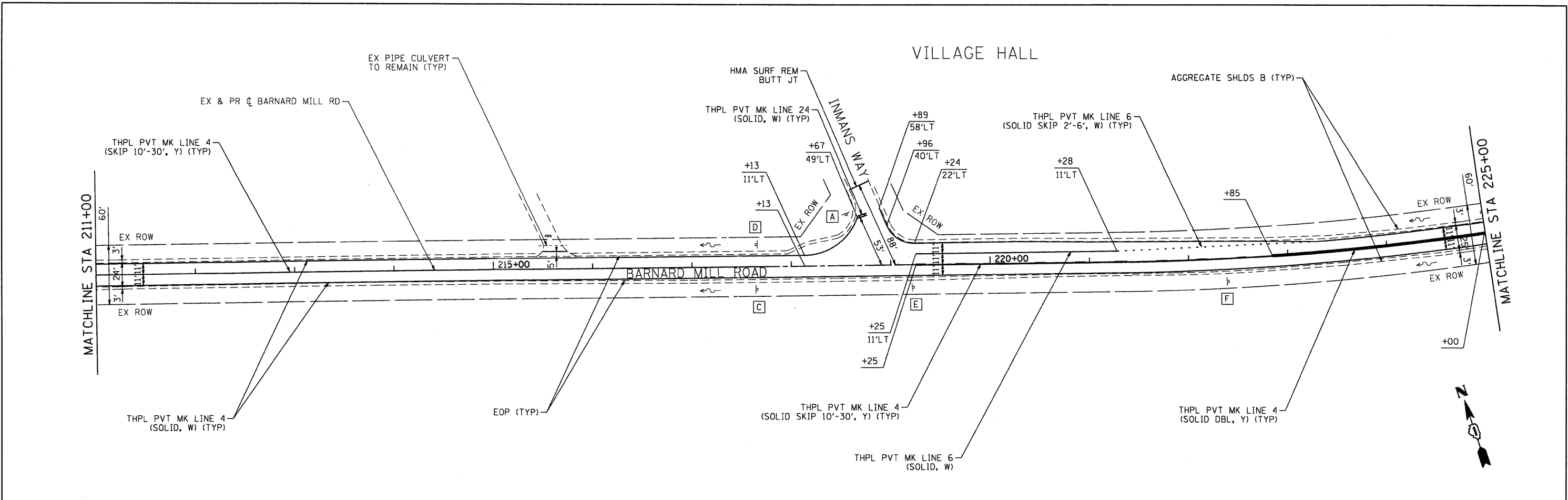
LEGEND

SIGN PANEL TAG	X
SEEDING (COMPLETE)	[Pattern of dots]

NOTES:
 1. RAISED REFL PAVT MKR AS LANE DIVIDERS SHALL BE SPACED 80' O.C. ALONG TANGENTS AND SPACED 40' O.C. ALONG CURVES PER DISTRICT 1 DETAIL TC-11.



	USER NAME = JStzoi PLOT SCALE = 1"=50' PLOT DATE = 2/1/2012	DESIGNED - JJS DRAWN - JJS CHECKED - AC DATE - 2/1/12	REVISED - REVISED - REVISED - REVISED -	VILLAGE OF RINGWOOD	BARNARD MILL ROAD ROADWAY PLAN	F.A.J. RTE. 4083 SECTION 11-00004-00-RS COUNTY MCHENRY TOTAL SHEETS 17 SHEET NO. 9	CONTRACT NO. 63696 [ILLINOIS] FED. AID PROJECT
	SCALE: 1"=50' SHEET NO. 4 OF 6 SHEETS STA. 183+00 TO STA. 211+00				[ILLINOIS] FED. AID PROJECT		

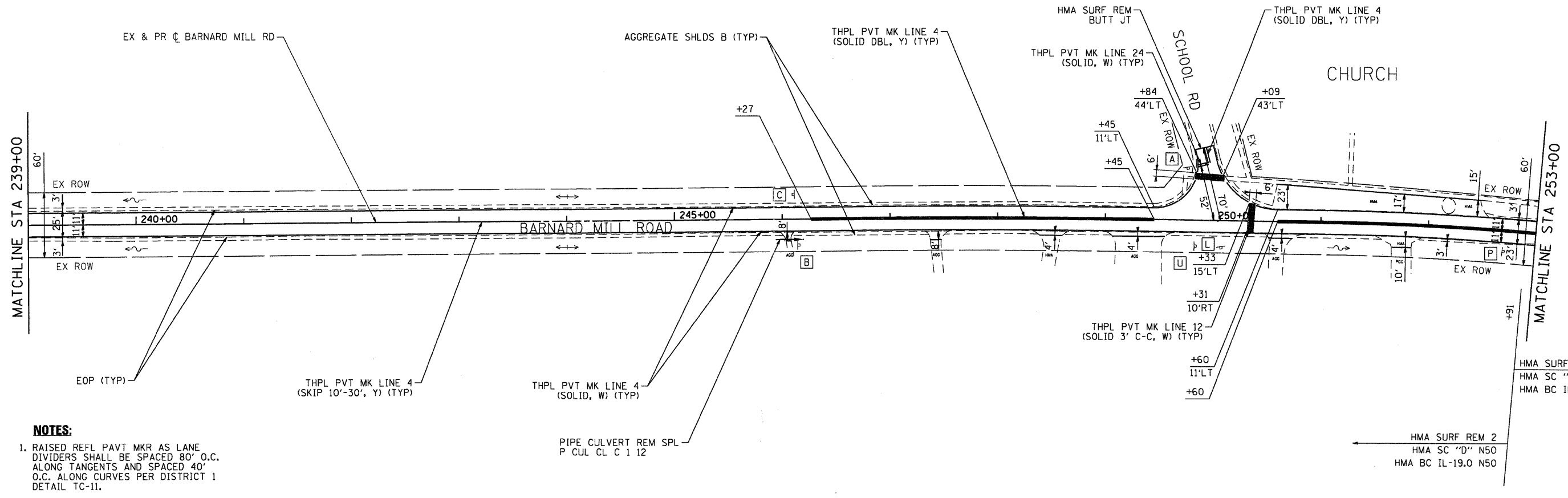


LEGEND

SIGN PANEL TAG	[X]
SEEDING (COMPLETE)	[Grid Pattern]

NOTES:
 1. RAISED REFL PAVT MKR AS LANE DIVIDERS SHALL BE SPACED 80' O.C. ALONG TANGENTS AND SPACED 40' O.C. ALONG CURVES PER DISTRICT 1 DETAIL TC-11.

HRGreen.com Illinois Professional Design Firm #184-001322	USER NAME = JStrzel	DESIGNED - JJS	REVISED -	VILLAGE OF RINGWOOD	BARNARD MILL ROAD ROADWAY PLAN		F.A.J. RTE. 4083	SECTION 11-00004-00-RS	COUNTY MCHENRY	TOTAL SHEETS 17	SHEET NO. 10
	PLOT SCALE = 1"=50'	CHECKED - AC	REVISED -		SCALE: 1"=50'	SHEET NO. 5 OF 6 SHEETS	STA. 211+00 TO STA. 239+00	CONTRACT NO. 63696		ILLINOIS FED. AID PROJECT	
	PLOT DATE = 1/31/2012	DATE - 1/31/12	REVISED -								

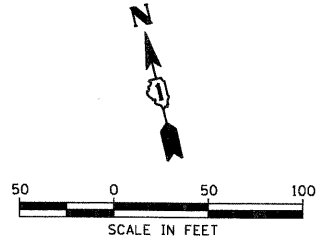
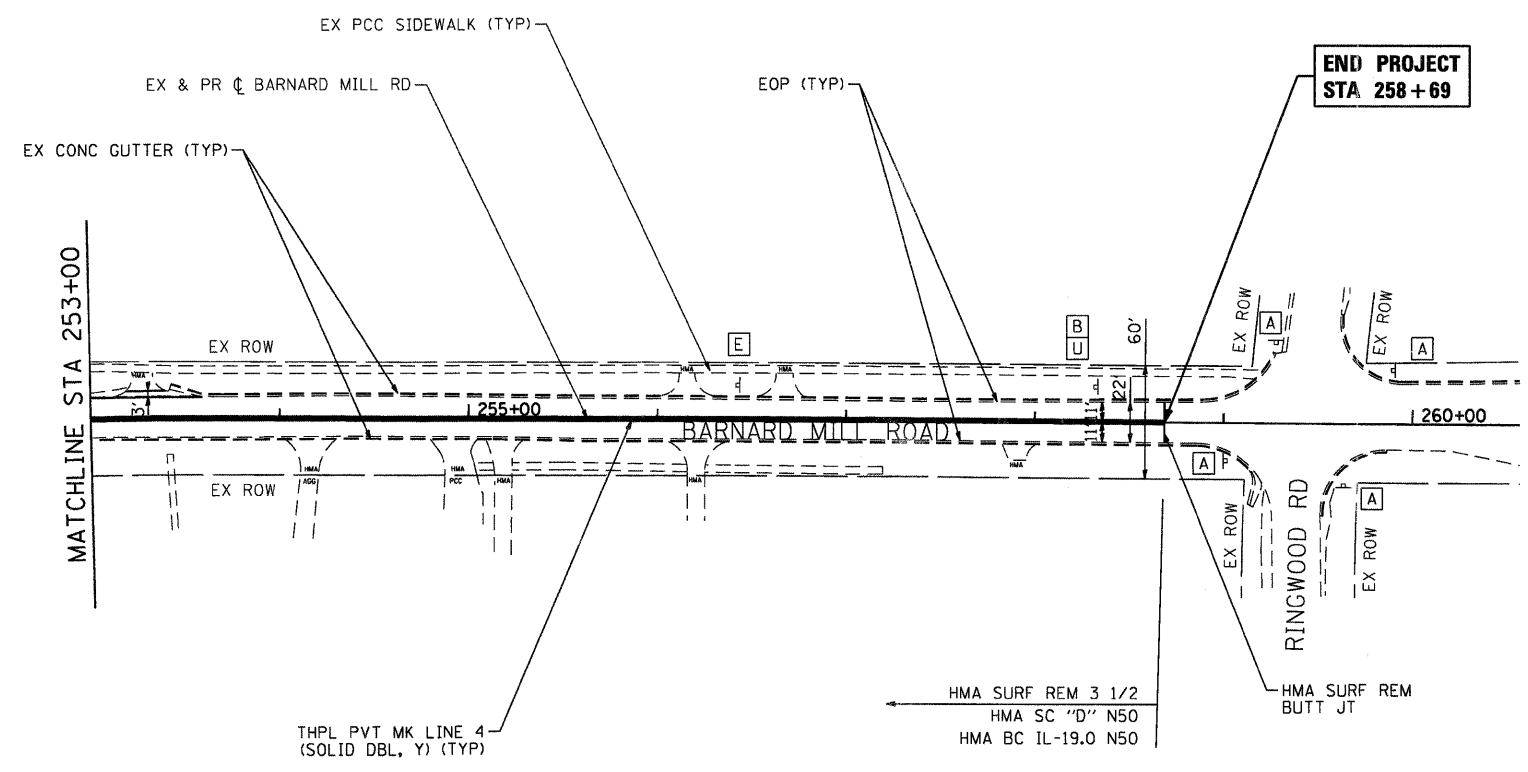


NOTES:

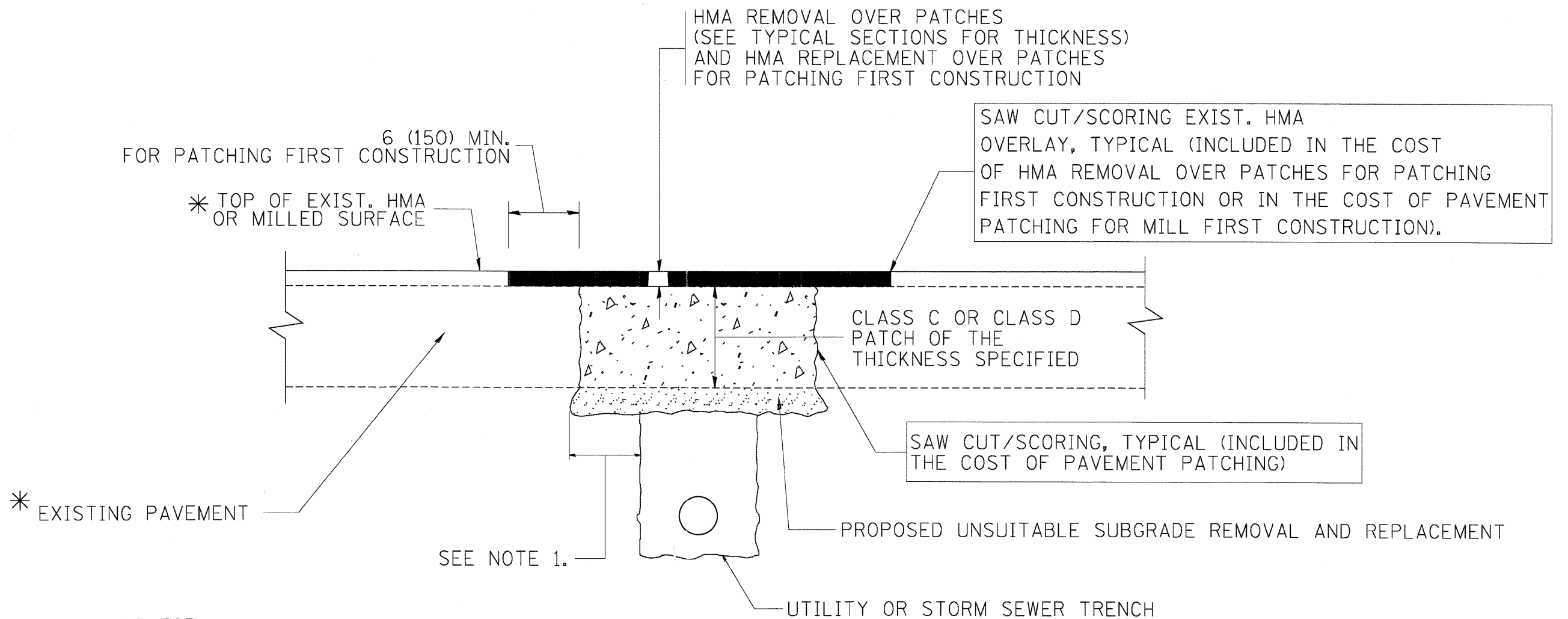
1. RAISED REFL PAVT MKR AS LANE DIVIDERS SHALL BE SPACED 80' O.C. ALONG TANGENTS AND SPACED 40' O.C. ALONG CURVES PER DISTRICT 1 DETAIL TC-11.

LEGEND

- SIGN PANEL TAG X
- SEEDING (COMPLETE)



	USER NAME = JStzsl	DESIGNED - JJS	REVISED -	VILLAGE OF RINGWOOD	BARNARD MILL ROAD ROADWAY PLAN	F.A.U. RTE. 4083	SECTION 11-00004-00-RS	COUNTY MCHENRY	TOTAL SHEETS 17	SHEET NO. 11		
	PLOT SCALE = 1"=50'	CHECKED - AC	REVISED -			SCALE: 1"=50'	SHEET NO. 6 OF 6 SHEETS	STA. 239+00	TO STA. END	CONTRACT NO. 63696		
	PLOT DATE = 1/31/2012	DATE - 1/31/12	REVISED -			ILLINOIS FED. AID PROJECT						



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

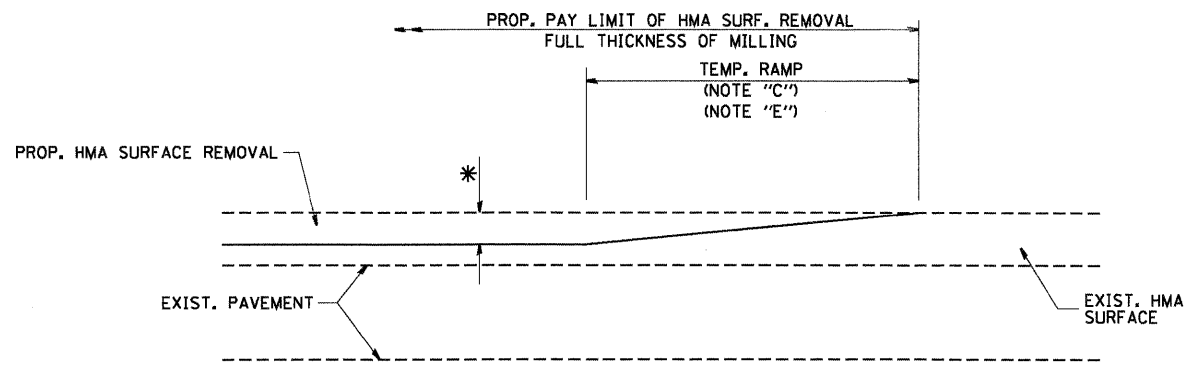
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

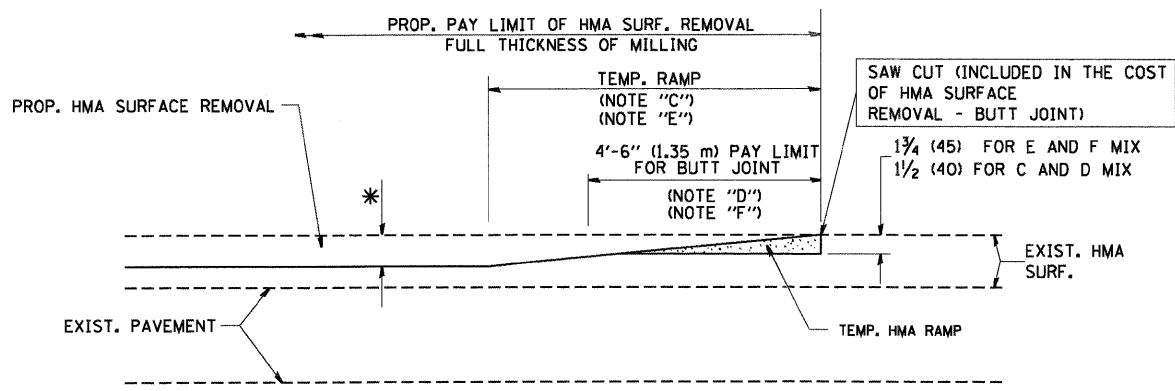
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

	USER NAME = JStrzol PLOT SCALE = PLOT DATE = 1/31/2012	DESIGNED - R. SHAH DRAWN - CHECKED - DATE - 10-25-94	REVISED - A. ABBAS 04-27-98 REVISED - R. BORO 01-01-07 REVISED - R. BORO 09-04-07 REVISED - K. ENG 10-27-08	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.J. RTE. 4083 SECTION 11-00004-00-RS COUNTY MCHENRY TOTAL SHEETS 17 SHEET NO. 12
	SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	BD400-04 (BD-22) CONTRACT NO. 63696			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



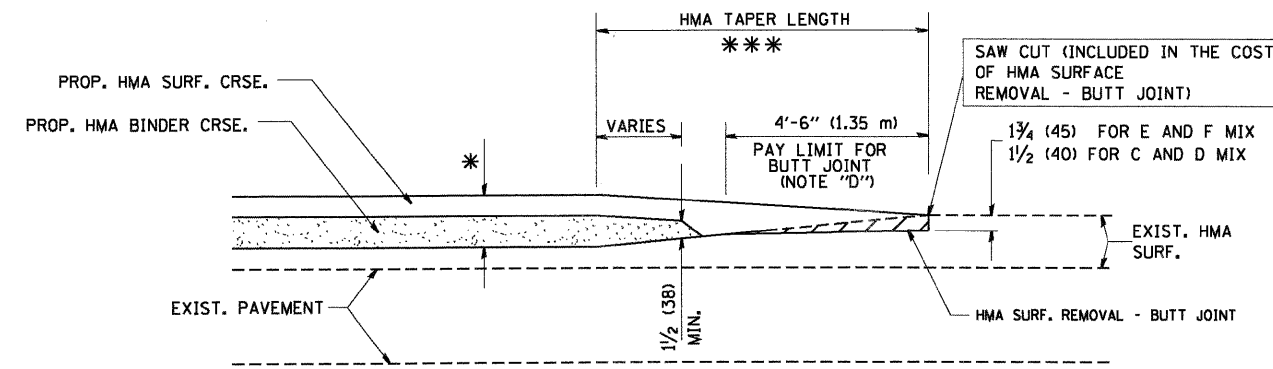
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

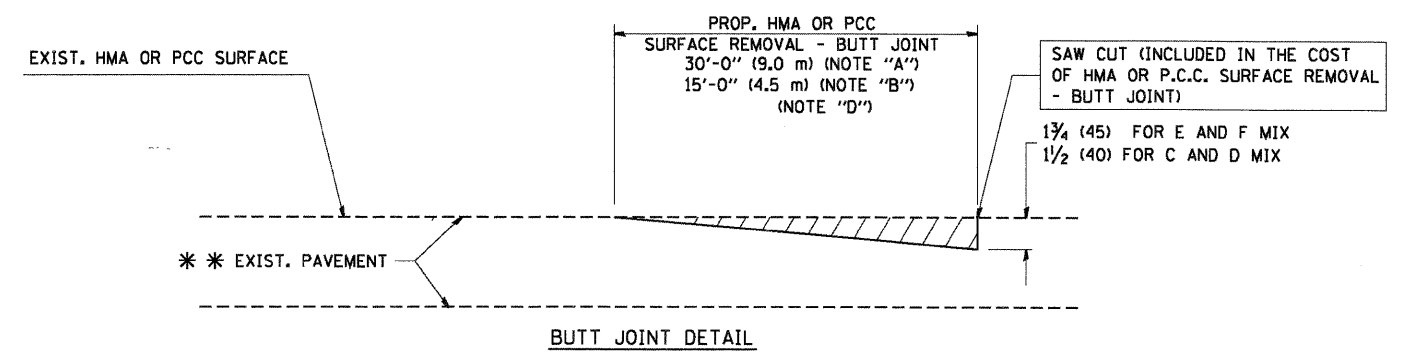


HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

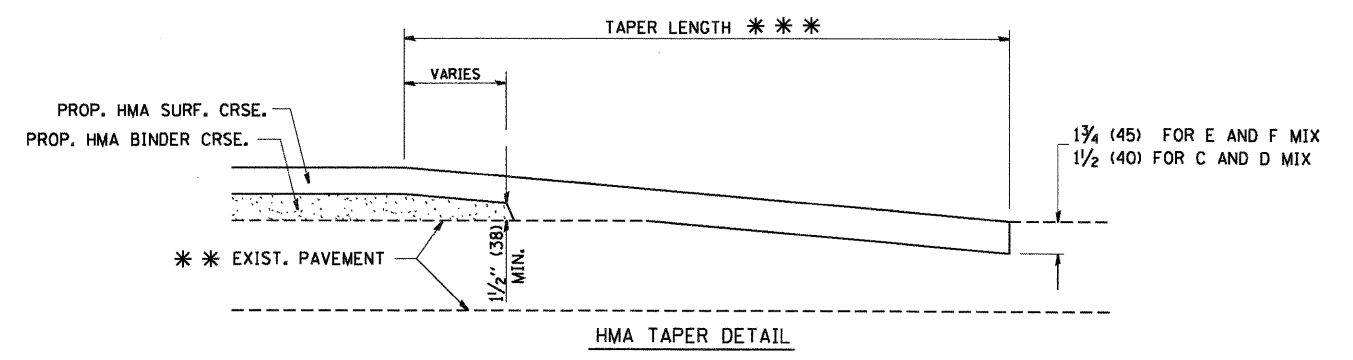
OPTION 2
TYPICAL TEMPORARY RAMP



BUTT JOINT AND HMA TAPER
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

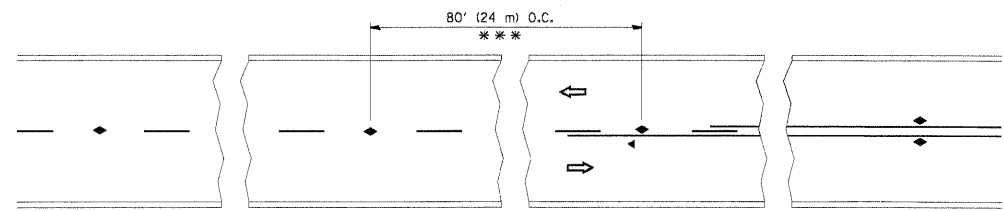


USER NAME = JStrzol	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
PLOT SCALE =	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT DATE = 1/31/2012	CHECKED -	REVISED - M. GOMEZ 04-06-01
	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

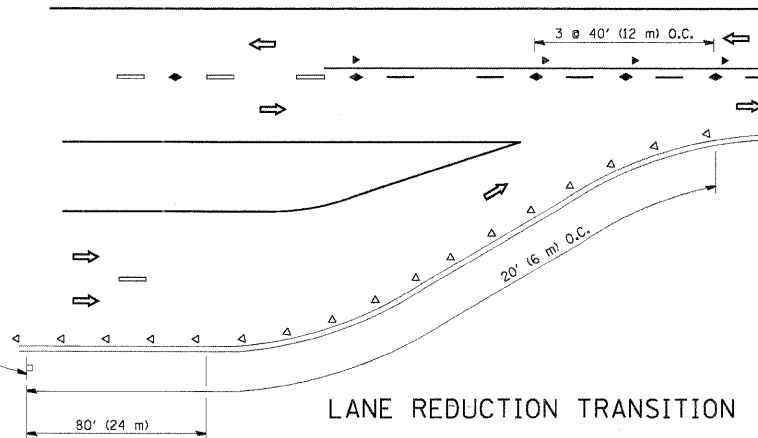
BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4083	11-00004-00-RS	MCHENRY	17	13
BD400-05 BD32			CONTRACT NO. 63696	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

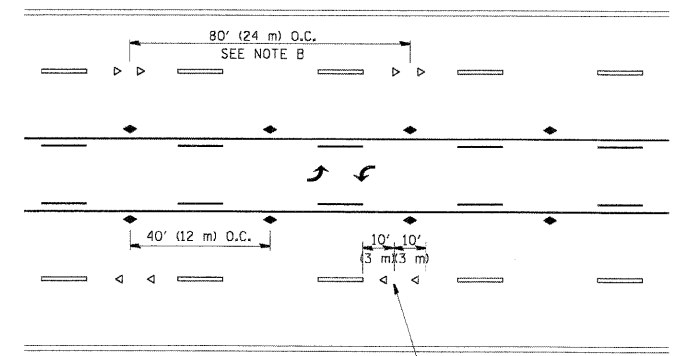


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

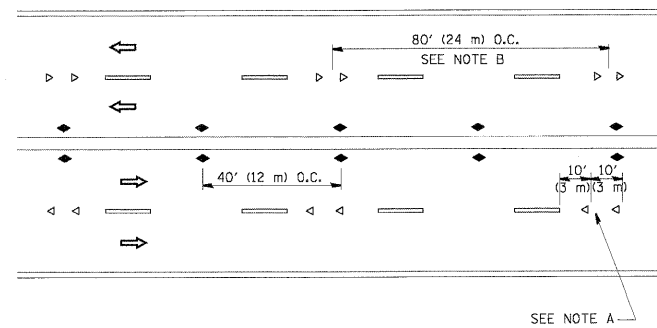
TWO-LANE/TWO-WAY



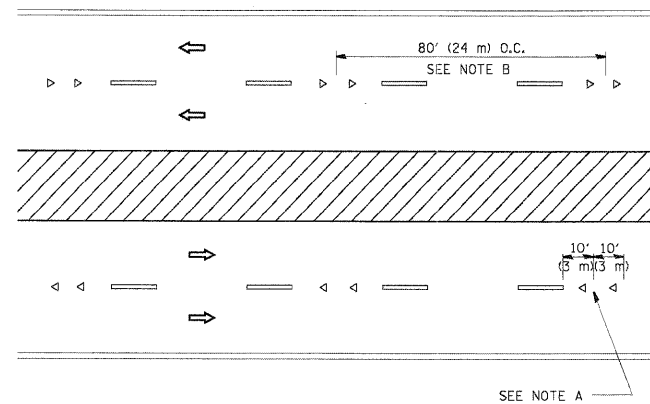
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

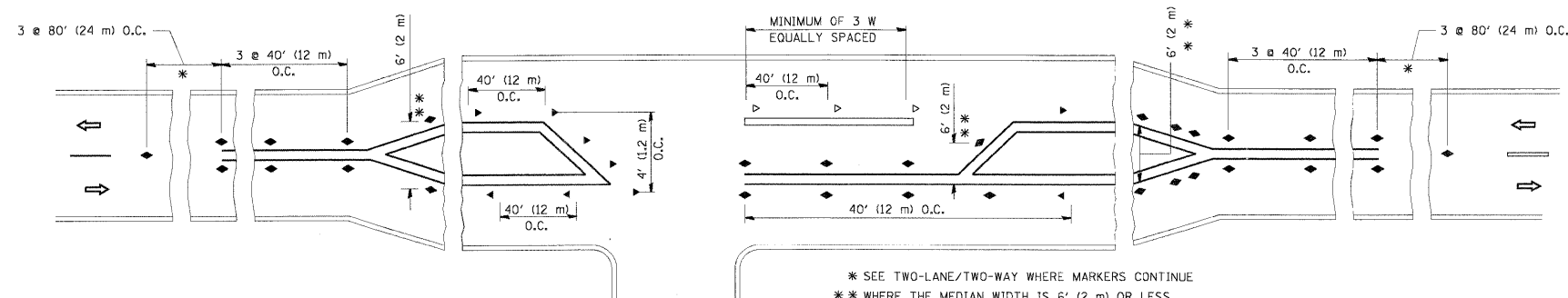
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.



USER NAME = JStrzol
 PLOT SCALE =
 PLOT DATE = 1/31/2012

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

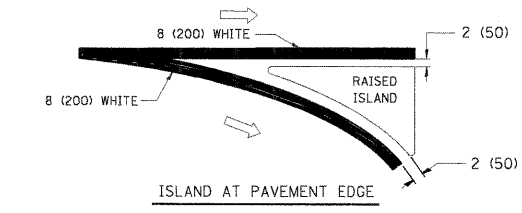
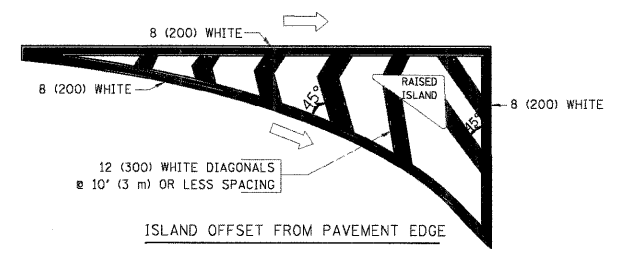
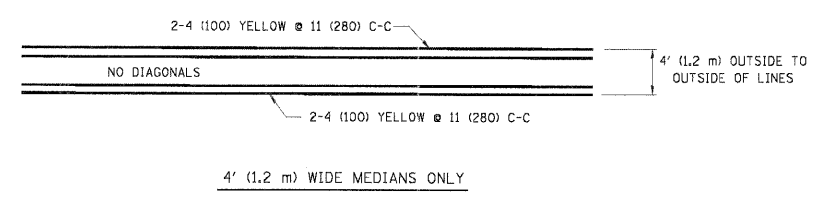
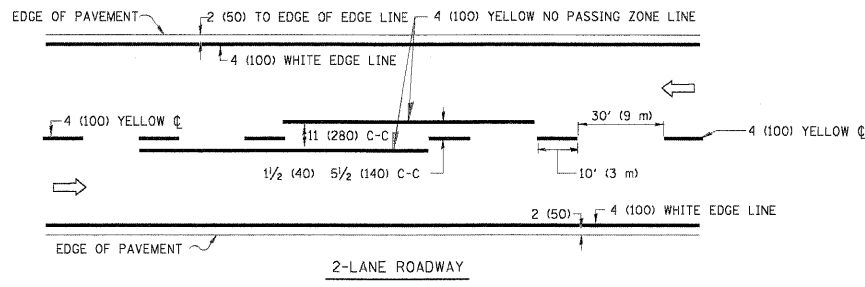
REVISED - T. RAMMACHER 09-19-94
 REVISED - T. RAMMACHER 03-12-99
 REVISED - T. RAMMACHER 01-06-00
 REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

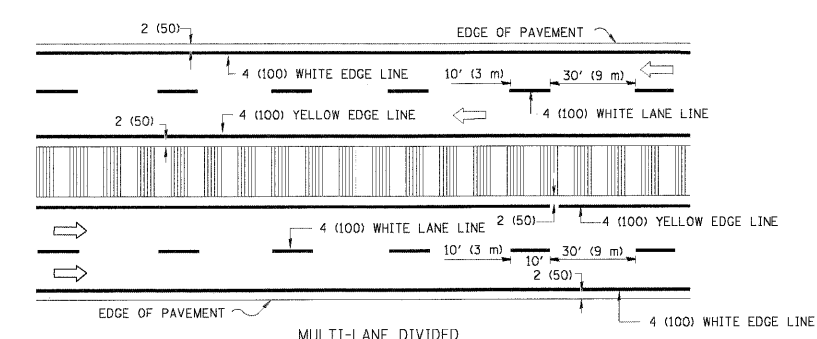
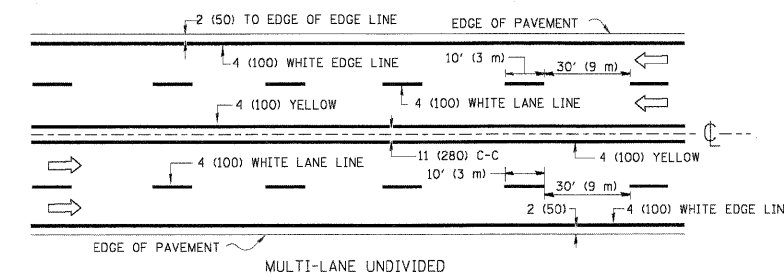
TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4083	11-00004-00-RS	MCHENRY	17	14
TC-11			CONTRACT NO.	63696
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

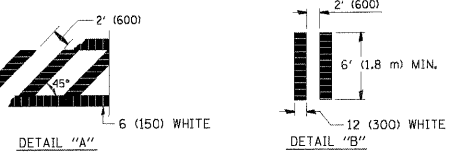
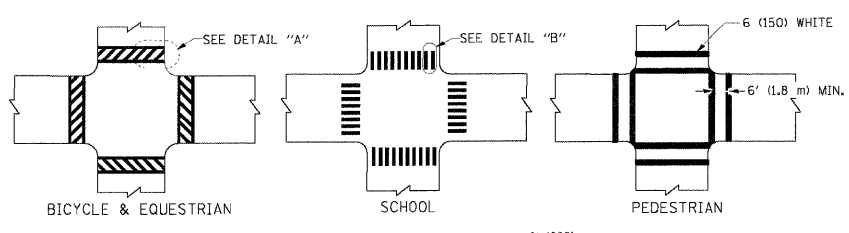


TYPICAL ISLAND MARKING

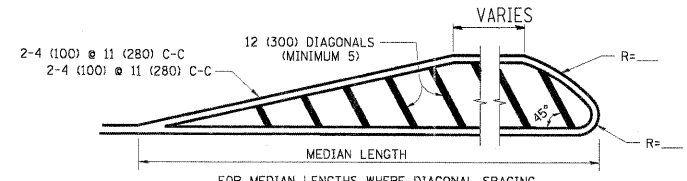


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



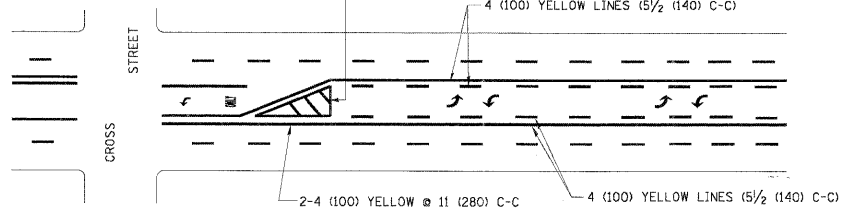
TYPICAL CROSSWALK MARKING



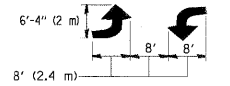
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

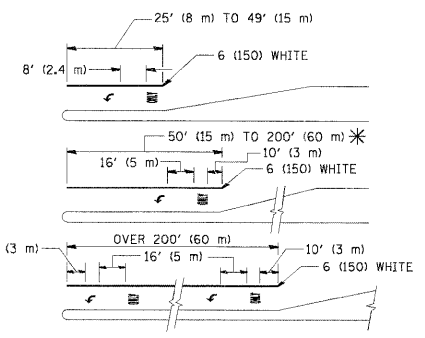


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

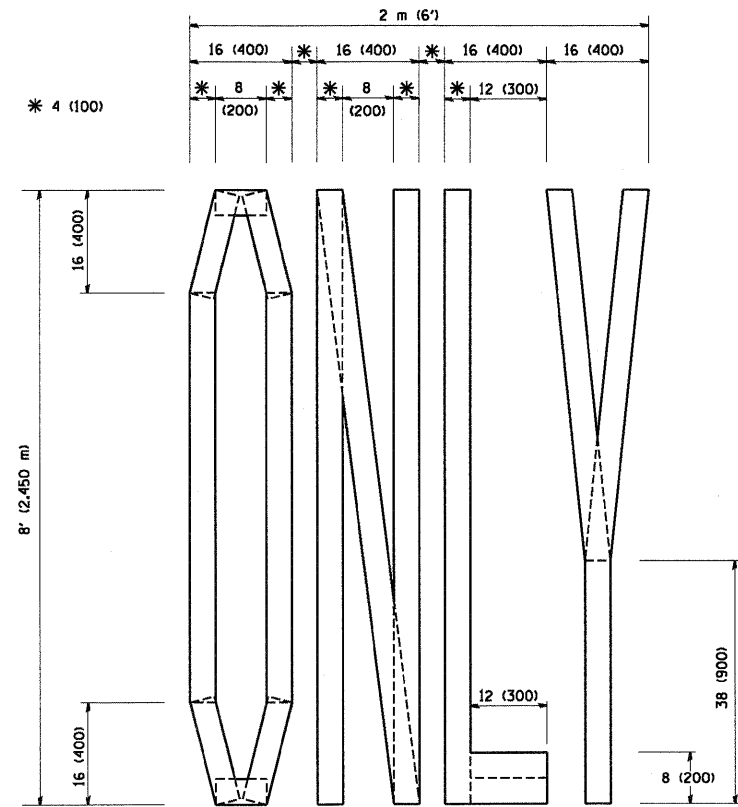
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

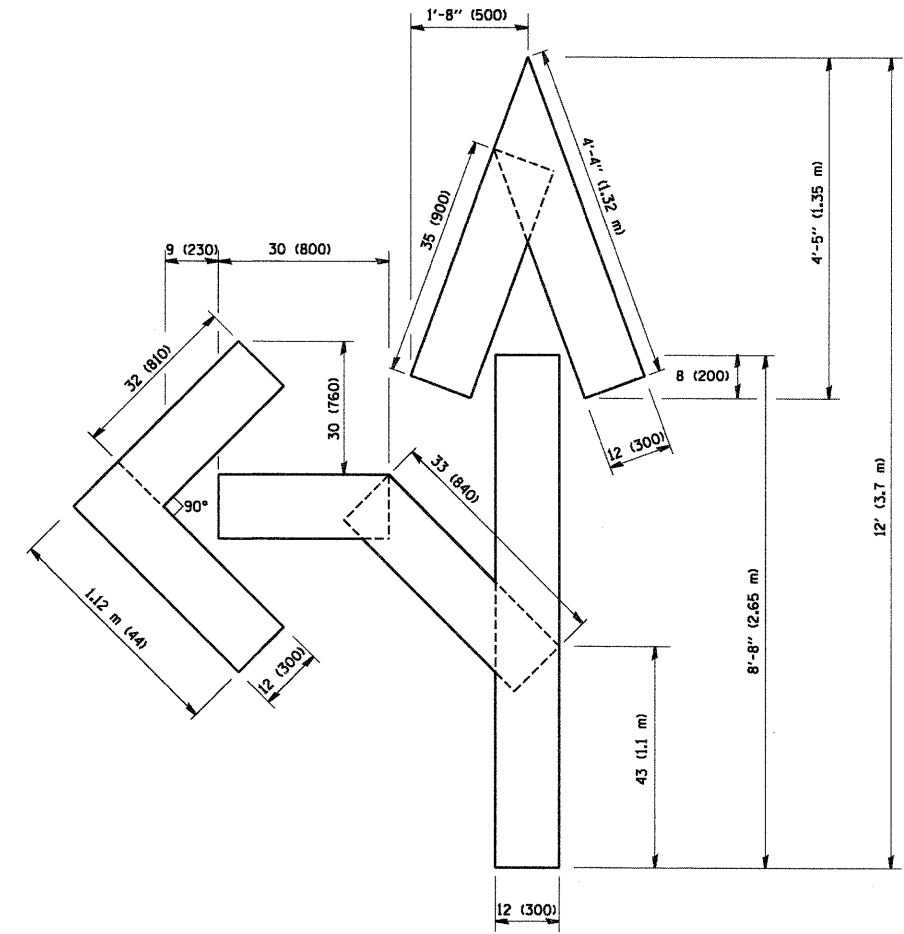
TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100)	SKIP-DASH	WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
LANE LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	5 (125) ON FREEWAYS	SKIP-DASH	WHITE	
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
	NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS			
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

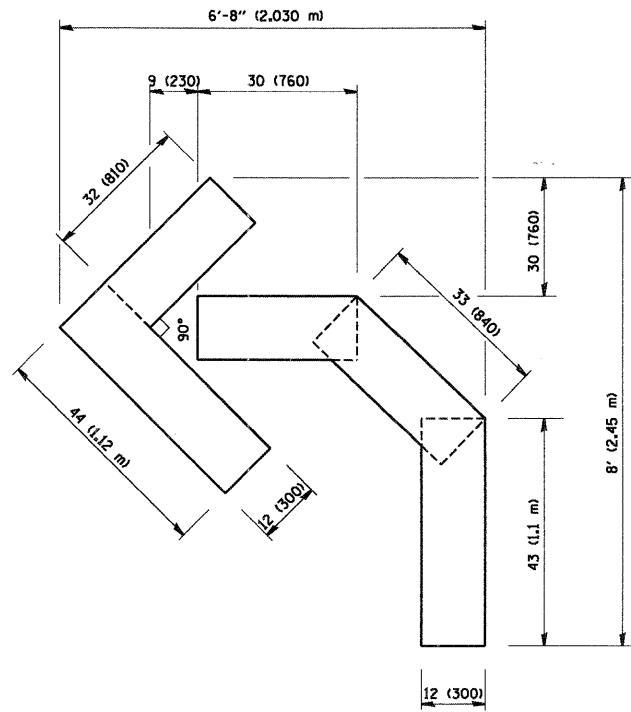
All dimensions are in inches (millimeters) unless otherwise shown.



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



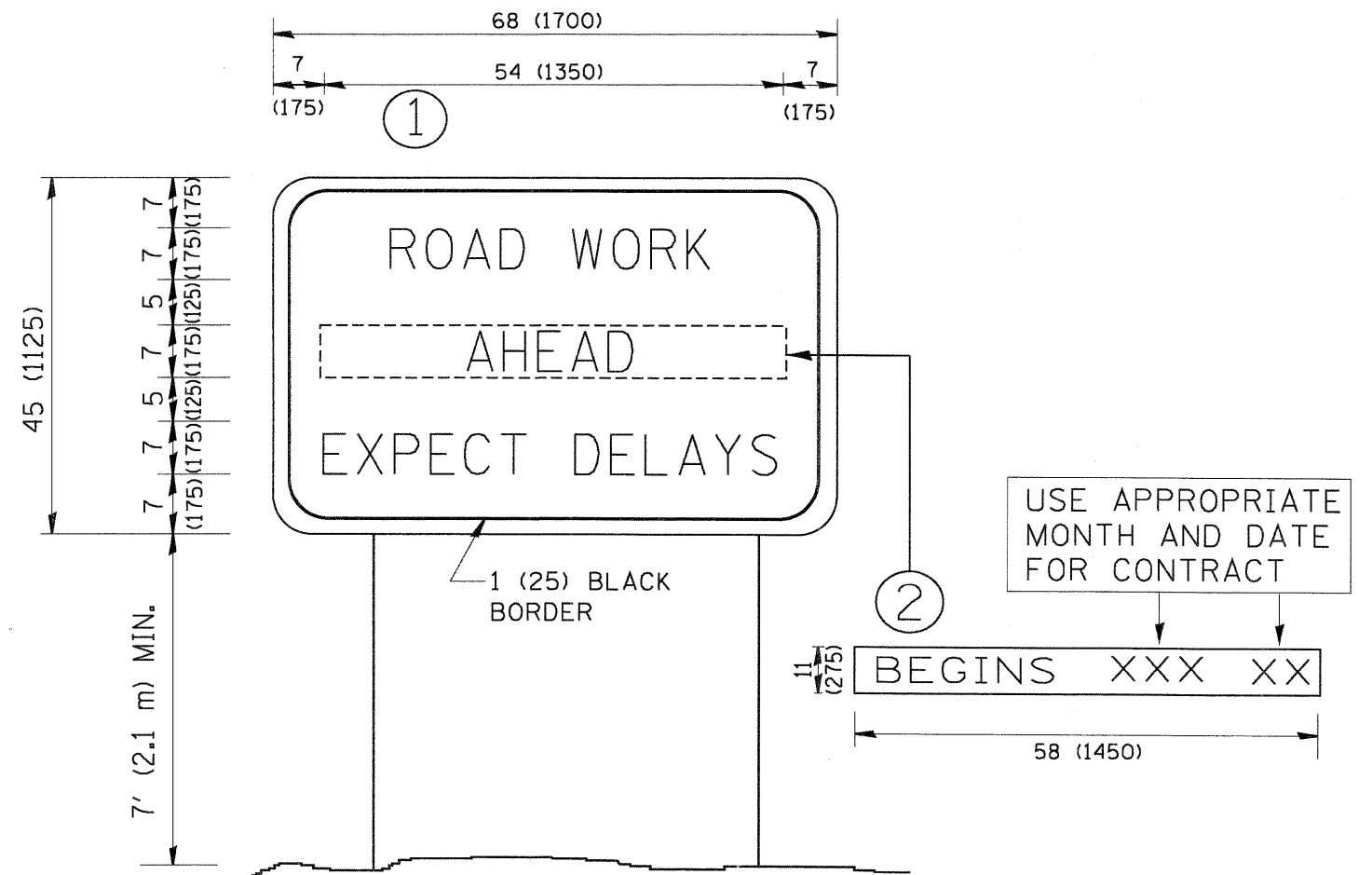
QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

HRGreen.com Illinois Professional Design Firm # 184-001322	USER NAME = JStrzel	DESIGNED -	REVISED -T. RAMMACHER 06-05-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING			F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE =	DRAWN -	REVISED -T. RAMMACHER 11-04-97		4083	11-00004-00-RS	MCHENRY	17	16			
PLOT DATE = 1/31/2012	CHECKED -	REVISED -T. RAMMACHER 03-02-98										
	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TC-16 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		CONTRACT NO.	63696



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

HRGreen.com Illinois Professional Design Firm # 184-001322	USER NAME = JStrzel	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE =	CHECKED -	REVISED - R. MIRS 12-11-97		4083	11-00004-00-RS	MCHENRY	17	17		
PLOT DATE = 1/31/2012	DATE -	REVISED - T. RAMMACHER 02-02-99	REVISED - C. JUCIUS 01-31-07	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TC-22	CONTRACT NO.	63696	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT