

**INDEX OF SHEETS**

04-27-12 LETTING ITEM 101

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

DIVISION OF HIGHWAYS

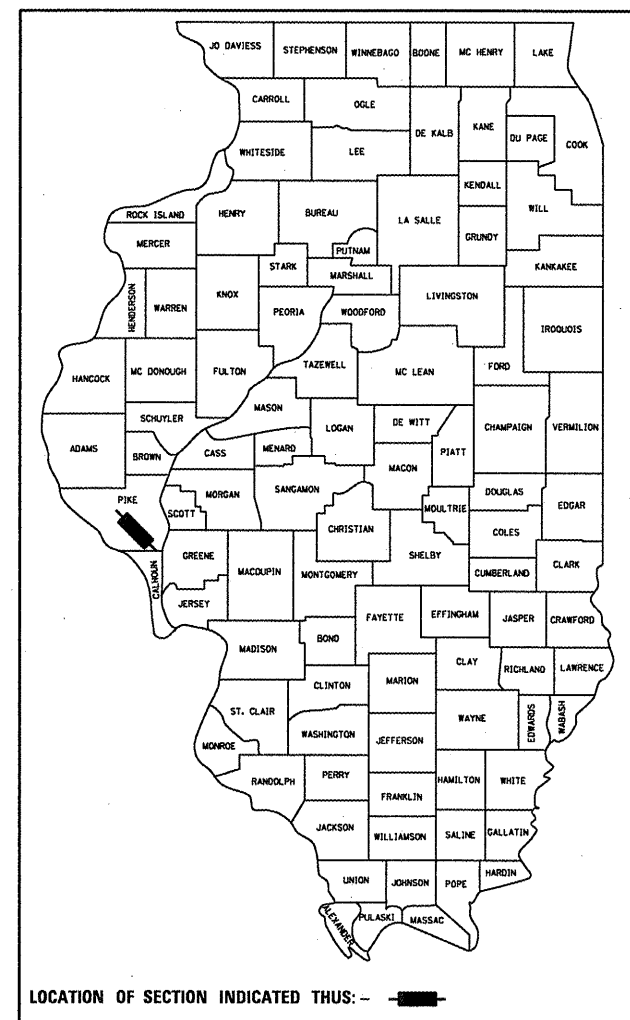
**PROPOSED  
HIGHWAY PLANS**

FAP ROUTE 304 (IL 96)  
SECTION 4RS-3, BR-1  
RESURFACING (3P)/BRIDGE REPAIR  
PIKE COUNTY

C-96-041-09  
PROJECT ACF-0304(044)

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	4RS-3, BR-1	PIKE	41	1
		ILLINOIS	CONTRACT NO. 72243	

**D-96-511-04**



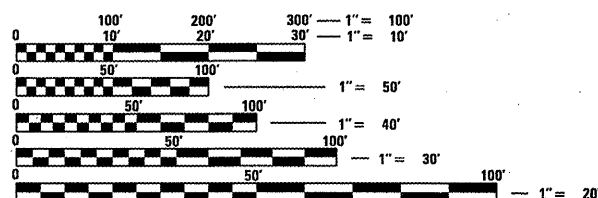
HIGHWAY CLASSIFICATION: OTHER PRINCIPAL ARTERIAL

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- 31 JOINT REPAIR DETAIL
- 32-41 CROSS SECTIONS

**STANDARDS**

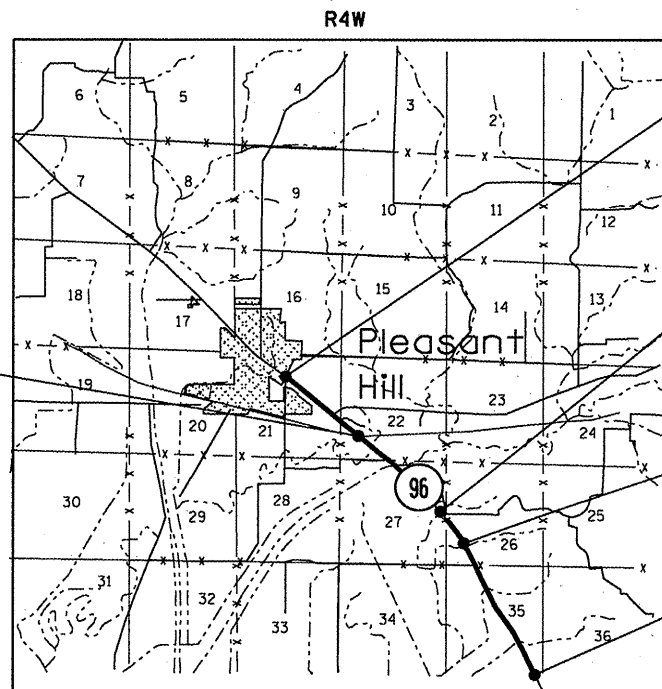
000001-06	630106	701311-03
280001-06	630301-05	701326-04
442201-03	635006-03	701901-02
542101-02	701001-02	720001-01
542401-01	701006-03	720006-02
606101-04	701201-04	728001-01
609006-05	701301-04	780001-03
630001-10	701306-03	781001-03

BRIDGE OMISSION SN 075-0047  
STA. 1310+28.36 TO STA. 1313+82.83



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

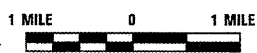


IMPROVEMENT BEGINS  
STA. 1273+10.50

BRIDGE OMISSION SN 075-0048  
STA. 1334+95.16 TO STA. 1340+29.50

STATION EQUATION  
STA. 1401+61.10 BK.=  
STA. 1404+93.40 AH.

IMPROVEMENT ENDS  
STA. 1472+32.00



**LOCATION MAP**

GROSS LENGTH = 19,589.200 FT. = 3.710 MILE  
NET LENGTH = 18,700.39 FT. = 3.542 MILE

**TRAFFIC COUNT**

ADT=1069 (2009)  
PV=882 (82.5%)  
SU=123 (11.5%)  
MU=64 (6.0%)

SENIOR TEAM ENGINEER: MARK DUST (217) 785-0597  
TEAM MANAGER: KEN ANDERSON (217) 524-7546

CONTRACT NO. 72243

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED February 8 2012  
Rosa Z. Dushay  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

March 23 2012  
John D. Baranzelli P.E./a  
acting ENGINEER OF DESIGN AND ENVIRONMENT

March 23 2012  
William R. Fryer/a  
acting DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS**

**GENERAL NOTES**

1. ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUB NUMBER LISTED IN THE INDEX OF SHEETS OR THE COPY OF THE STANDARD INCLUDED IN THESE PLANS.
2. IN ADDITION TO FIELD SURVEYS, PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING FACILITIES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND DETAILS IN THE FIELD. SUCH VARIATIONS SHALL NOT BE A CAUSE FOR ADDITIONAL COMPENSATION DUE TO A CHANGE IN THE SCOPE OF WORK; HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.
3. ACCESS TO ALL ENTRANCES AND SIDE ROADS SHALL BE MAINTAINED AT ALL TIMES.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS, AS OUTLINED IN ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS. THE J.U.L.I.E. NUMBER IS 1-800-892-0123. A MINIMUM OF 48 HOURS ADVANCE NOTICE IS REQUIRED.
5. THE THICKNESS OF BITUMINOUS MIXTURES SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED.
6. ALL SAW CUTS, NECESSARY TO COMPLETE THE WORK DETAILED IN THESE PLANS, SHALL BE INCLUDED IN THE COST FOR THE VARIOUS PAY ITEMS INVOLVED. THE MINIMUM SAW CUT DEPTH IN THE PAVEMENT SHALL BE 1 1/2 " UNLESS OTHERWISE SPECIFIED IN A DETAIL SHOWN IN THE PLANS.
7. UNLESS DIRECTED BY THE ENGINEER, PAVEMENT MARKING LINES SHALL NOT BE LAID DIRECTLY OVER A LONGITUDINAL CRACK OR JOINT NOR OVER A TAR OR ASPHALT PAINTED LINE. THE EDGE OF A CENTERLINE OR LANE LINE SHALL BE OFFSET A MINIMUM DISTANCE OF 2" FROM A LONGITUDINAL CRACK OR JOINT. EDGE LINES SHALL BE APPROXIMATELY 2" FROM THE EDGE LINE OF PAVEMENT. SEE SECTION 780 OF THE STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS.
8. NO PASSING ZONES TO BE FIELD VERIFIED BY THE BUREAU OF OPERATIONS. THE RESIDENT ENGINEER SHALL NOTIFY THE BUREAU OF OPERATIONS AT LEAST 14 DAYS PRIOR TO PLACEMENT OF FINAL PERMANENT PAVEMENT MARKING. (PH: 217-785-5312)
9. THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED TO CALCULATE THE PLAN QUANTITIES:  

BITUMINOUS MATERIALS (PRIME COAT)	0.00038 TON/SQ. YD. (ON PAVEMENT)
BITUMINOUS MATERIALS (PRIME COAT)	0.001425 TON/SQ. YD. (ON AGG.)
AGGREGATE PRIME COAT	0.002 TON/SQ. YD.
BITUMINOUS CONCRETE SURFACE / BINDER	0.056 TON/SQ. YD. PER 1"
AGGREGATE MATERIAL	2.05 TON/CU. YD.
10. THE EXISTING ROAD SIGNS THAT INTERFERE WITH CONSTRUCTION WILL BE REMOVED OR RELOCATED AS DIRECTED BY THE ENGINEER. AFTER THE CONSTRUCTION IS COMPLETED, THE CONTRACTOR WILL REPLACE THE SIGNS AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE CONTRACT, AND NO COMPENSATION WILL BE ALLOWED.

**COMMITMENTS**

1. THE RESIDENT ENGINEER SHALL CONTACT STUDIES & PLANS CONCERNING ANY MAJOR PLAN CHANGE, TO MAKE SURE NO PREVIOUS COMMITMENTS (NOT LISTED) WERE MADE AFFECTING THE DESIGN AND ALLOW AN IMPROVED DESIGN FOR FUTURE PROJECTS.

THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT:

MIXTURE USE(S):	HMA SURFACE COURSE	PATCHING	INCIDENTAL HMA SURFACE	HMA SHOULDER	HMA LEVEL BINDER
PG:	PG 64-22	PG 64-22	PG 64-22	PG 58-22	PG 64-22
DESIGN AIR VOIDS:	4.0% @ N 50	4.0% @ N 50	4.0% @ N 50	2.0% @ N 30	4.0% @ N50
MIXTURE COMPOSITION: (GRADATION MIXTURE)	IL 9.5 OR IL 12.5	IL 19.0	IL 9.5 OR IL 12.5	BAM	IL 9.5
FRICITION AGGREGATE:	MIX "C"	N/A	MIX "C"	N/A	N/A
20 YEAR ESAL	N/A	N/A	N/A	N/A	N/A

DISTRICT SIX	
EXAMINED <u>Jan 26</u>	20 <u>12</u>
<i>William M. Ryan</i> OPERATIONS ENGINEER	
EXAMINED <u>JAN 21</u>	20 <u>12</u>
<i>Tommy Lee</i> PROJECT IMPLEMENTATION ENGINEER	
EXAMINED <u>February 1</u>	20 <u>12</u>
<i>2RM/LJ</i> PROGRAM DEVELOPMENT ENGINEER	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE
				ROADWAY 80% FEDERAL 20% STATE 0005
20200100	EARTH EXCAVATION	CU YD	1,226	1,226
20200600	EXCAVATING AND GRADING EXISTING SHOULDER	UNIT	315	315
* 25000200	SEEDING, CLASS 2	ACRE	0.8	0.8
* 25000400	NITROGEN FERTILIZER NUTRIENT	POUND	72	72
* 25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	72	72
* 25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	72	72
* 25000700	AGRICULTURAL GROUND LIMESTONE	TON	1.6	1.6
* 25100115	MULCH, METHOD 2	ACRE	0.8	0.8
28001000	AGGREGATE (EROSION CONTROL)	TON	30	30
28100707	STONE DUMPED RIPRAP, CLASS A4	SQ YD	662	662
28200200	FILTER FABRIC	SQ YD	662	662
<b>31101000</b>	<b>SUBBASE GRANULAR MATERIAL, TYPE B</b>	<b>TON</b>	<b>346</b>	<b>346</b>
35800100	PREPARATION OF BASE	SQ YD	1,269	1,269
40200800	AGGREGATE SURFACE COURSE, TYPE B	TON	72	72
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	22.5	22.5

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE
				ROADWAY 80% FEDERAL 20% STATE 0005
40600300	AGGREGATE (PRIME COAT)	TON	102.5	102.5
40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	1,728	1,728
40600895	CONSTRUCTING TEST STRIP	EACH	1	1
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	1,124	1,124
40600990	TEMPORARY RAMP	SQ YD	199	199
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	5,010	5,010
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	406	406
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	1,790	1,790
44000400	GUTTER REMOVAL	FOOT	<b>1,012</b>	<b>1,012</b>
44200116	PAVEMENT PATCHING, TYPE I, 10 INCH	SQ YD	10	10
44200120	PAVEMENT PATCHING, TYPE II, 10 INCH	SQ YD	60	60
44200124	PAVEMENT PATCHING, TYPE III, 10 INCH	SQ YD	75	75
44200126	PAVEMENT PATCHING, TYPE IV, 10 INCH	SQ YD	95	95
44201353	CLASS C PATCHES, TYPE II, 10 INCH	SQ YD	120	120

\*SPECIALTY ITEM

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SCHEDULE OF QUANTITIES</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
c:\pwwork\pwwork\sparksgw\d0216263\067243-shc-500.dgn		DRAWN -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	304	4RS-3, BR-1	PIKE	41	3
		CHECKED -	REVISED -												
		DATE -	REVISED -												

ILLINOIS FED. AID PROJECT CONTRACT NO. 72243

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE
				ROADWAY 80% FEDERAL 20% STATE 0005
44201357	CLASS C PATCHES, TYPE III, 10 INCH	SQ YD	103	103
44213100	PAVEMENT FABRIC	SQ YD	223	223
48101200	AGGREGATE SHOULDERS, TYPE B	TON	3,135	3,135
48203023	HOT-MIX ASPHALT SHOULDERS, 6 1/2"	SQ YD	4,107	4,107
54215424	CAST-IN-PLACE REINFORCED CONCRETE END SECTIONS 24"	EACH	1	1
54215430	CAST-IN-PLACE REINFORCED CONCRETE END SECTIONS 30"	EACH	1	1
54215547	METAL END SECTIONS 12"	EACH	2	2
54390180	INSERTION CULVERT LINER 24"	FOOT	45	45
54390210	INSERTION CULVERT LINER 30"	FOOT	139	139
54390230	INSERTION CULVERT LINER 36"	FOOT	47	47
60100945	PIPE DRAINS 12"	FOOT	16	16
60600095	CLASS SI CONCRETE (OUTLET)	CU YD	10.5	10.5
60900140	TYPE B INLET BOX, STANDARD 609006	EACH	2	2
* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	550	550

\* \* \* \* \*

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE
				ROADWAY 80% FEDERAL 20% STATE 0005
* 63000007	STEEL PLATE BEAM GUARDRAIL, TYPE B, 6 FOOT POSTS	FOOT	150	150
* 63000370	LONG-SPAN GUARDRAIL OVER CULVERT, 25 FT SPAN	FOOT	50	50
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	4	4
63301990	REMOVE AND REERECT TRAFFIC BARRIER TERMINALS, TYPE 1	EACH	2	2
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6
67100100	MOBILIZATION	L SUM	1	1
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1
70100500	TRAFFIC CONTROL AND PROTECTION, STANDARD 701326	L SUM	1	1
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	10	10
70300100	SHORT TERM PAVEMENT MARKING	FOOT	5,612	5,612
70300230	TEMPORARY PAVEMENT MARKING - LINE 5"	FOOT	55,364	55,364
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	617	617
* 72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	1,052	1,052

\*SPECIALTY ITEM

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
c:\pwwork\pwwidot\sparksgw\d0216263\0672243-sht-500.dgn		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES			
SCALE:	SHEET	OF	SHEETS
		STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	4RS-3, BR-1	PIKE	41	4
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72243	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE
				ROADWAY 80% FEDERAL 20% STATE 0005
* 78001120	PAINT PAVEMENT MARKING - LINE 5"	FOOT	55,364	55,364
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	235	235
* 78200410	GUARDRAIL MARKERS, TYPE A	EACH	16	16
* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	4	4
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	235	235
Z0021908	SILICONE JOINT SEALER, 2"	FOOT	74	74
Z0021912	SILICONE JOINT SEALER, 2.5"	FOOT	74	74
Z0041895	POLYMER CONCRETE	CU FT	12.5	12.5
X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	45,048	45,048
● X5420624	PIPE CULVERTS TO BE CLEANED, 24"	FOOT	45	45
● X5420630	PIPE CULVERTS TO BE CLEANED, 30"	FOOT	139	139
● X5420636	PIPE CULVERTS TO BE CLEANED, 36"	FOOT	47	47
* X7200105	SIGN PANEL - TYPE 1 (SPECIAL)	SQ FT	175	175
* X7200205	SIGN PANEL - TYPE 2 (SPECIAL)	SQ FT	381	381

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE
				ROADWAY 80% FEDERAL 20% STATE 0005
X7240200	REMOVE SIGN PANEL ASSEMBLY - TYPE B (SPECIAL)	EACH	38	38
X7240110	REMOVE SIGN PANEL ASSEMBLY - TYPE A (SPECIAL)	EACH	14	14

● NONPARTICIPATING 100% STATE \*SPECIALTY ITEM

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
c:\pwr\work\p\idot\sparksgw\d0216263\067243-sht-500.dgn		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
						304	4RS-3, BR-1	PIKE	41	5
										CONTRACT NO. 72243
ILLINOIS FED. AID PROJECT										

GUARDRAIL SCHEDULE							
LOCATION	STEEL PLATE BEAM GUARDRAIL TYPE A, 6' POSTS (FOOT)	STEEL PLATE BEAM GUARDRAIL TYPE B, 6' POSTS (FOOT)	LONG-SPAN GUARDRAIL OVER CULVERT 25 FT (FOOT)	TRAFFIC BARRIER TERMINAL TYPE 1 SPL (TANGENT) (EACH)	REMOVE AND REERECT TRAFFIC BARRIER TERMINAL TYPE 1 SPL (TANGENT) (EACH)	GUARDRAIL MKR TYPE A (EACH)	TERM MARKER DIRECT APPLIED (EACH)
STATION TO STATION							
<b>FAP 304 (IL 96)</b>							
<b>RIGHT</b>							
STA. 1316+06.00 TO STA. 1316+56.00					1.0		
STA. 1316+81.00 TO STA. 1317+31.00							
STA. 1316+06.00 TO STA. 1316+81.00		75.0				2.00	
STA. 1372+05.50 TO STA. 1372+55.50				1.0			1.00
STA. 1372+55.50 TO STA. 1374+55.50	200.0					4.00	
STA. 1374+55.50 TO STA. 1374+80.50			25.0				
STA. 1374+80.50 TO STA. 1375+55.50	75.0					2.00	
STA. 1375+55.50 TO STA. 1376+05.50				1.0			1.00
<b>LEFT</b>							
STA. 1316+06.00 TO STA. 1316+56.00					1.0		
STA. 1316+81.00 TO STA. 1317+31.00							
STA. 1316+06.00 TO STA. 1316+81.00		75.0				2.00	
STA. 1373+73.50 TO STA. 1374+23.50				1.0			1.00
STA. 1374+23.50 TO STA. 1374+98.50	75.0					2.00	
STA. 1374+98.50 TO STA. 1375+23.50			25.0				
STA. 1375+23.50 TO STA. 1377+23.50	200.0					4.00	
STA. 1377+23.50 TO STA. 1377+73.50				1.0			1.00
<b>TOTAL</b>	<b>550.0</b>	<b>150.0</b>	<b>50.0</b>	<b>4.0</b>	<b>2.0</b>	<b>16.0</b>	<b>4.0</b>

PAVEMENT SCHEDULE										
LOCATION	PROPOSED PAVEMENT LENGTH (FT)	PROPOSED PAVEMENT WIDTH (FT)	HOT-MIX ASPHALT SURF. REM. BUTT-JOINT (SQ YD)	HOT-MIX ASPHALT SURF. REM. (VAR. DEPTH) (SQ YD)	HOT-MIX ASPHALT SURF. REM., 1 1/2" (SQ YD)	LEVEL BINDER (MACHINE METHOD) (3/4") (TON)	HMA SURF. COURSE MIX "C" N50 (1 1/2") (TON)	BIT. MAT. PRIME CT. (TON)	AGGREGATE PRIME COAT (TON)	
STATION TO STATION										
<b>FAP 304 (IL 96)</b>										
STA. 1273+10.50 TO STA. 1273+45.50	35.00	24	85.5				9.2	0.1	0.2	
STA. 1273+45.50 TO STA. 1304+76.00	3,130.50	24		7,652.3		350.5	826.5	3.2	16.8	
STA. 1304+76.00 TO STA. 1306+99.00	223.00	26.67		660.8		27.8	64.4	0.3	1.3	
STA. 1306+99.00 TO STA. 1307+45.00	46.00	26.67		136.3			13.3	0.1	0.3	
STA. 1307+45.00 TO STA. 1310+28.36	283.36	29			913.0		76.7	0.3	1.8	
<b>BRIDGE OMISSION STA. 1310+28.36 TO STA. 1313+82.83</b>										
STA. 1313+82.83 TO STA. 1316+55.00	272.17	29			877.0		73.7	0.3	1.8	
STA. 1316+55.00 TO STA. 1324+25.00	770.00	26.67		2,281.8			222.5	1.0	4.6	
STA. 1324+25.00 TO STA. 1328+00.00	375.00	24		916.7			99.0	0.4	2.0	
STA. 1328+00.00 TO STA. 1334+95.16	695.16	24		1,853.8			192.8	0.7	3.7	
<b>BRIDGE OMISSION STA. 1334+95.16 TO STA. 1340+29.50</b>										
STA. 1340+29.50 TO STA. 1348+00.00	770.50	24		2,054.67			207.6	0.8	4.1	
RT. STA. 1348+00.00 TO RT. STA. 1375+09.00	2,709.00	12		3,311.00		151.6	366.8	1.5	7.2	
LT. STA. 1348+00.00 TO LT. STA. 1375+89.00	2,789.00	12		3,408.78		156.2	374.8	1.5	7.4	
RT. STA. 1375+09.00 TO RT. STA. 1379+00.00	391.00	12		478.00		22.0	55.1	0.2	1.0	
LT. STA. 1375+89.00 TO LT. STA. 1379+00.00	311.00	12		380.11		17.4	47.1	0.2	0.8	
STA. 1379+00.00 TO STA. 1381+91.00	291.00	23		711.33		31.0	68.3	0.3	1.5	
STA. 1381+91.00 TO STA. 1401+61.10	1,970.10	24		4,815.80		220.7	520.1	2.0	10.5	
<b>STATION EQUATION STA. 1401+61.10 BK. = STA. 1404+93.40 AH.</b>										
STA. 1404+93.40 TO STA. 1471+97.00	6,703.60	24		16,386.58		750.8	1,782.9	7.0	35.8	
STA. 1471+97.00 TO STA. 1472+32.00	35.00	24	85.5				9.2	0.1	0.2	
<b>TOTAL</b>			<b>171.0</b>	<b>45,048.0</b>	<b>1,790.0</b>	<b>1,728.0</b>	<b>5,010.0</b>	<b>20.0</b>	<b>101.0</b>	

NOTE: QUANTITY FOR HMA SHOULDERS 6 1/2" AND SAFETY EDGE HAS BEEN INCLUDED IN THE QUANTITIES FOR HOT-MIX ASPHALT SURFACE COURSE, MIX "C" N50.

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SCHEDULE OF QUANTITIES</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\idot\sparksgw\0216263\067243-sht-schedule.dgn	DRAWN -	REVISED -	304						4RS-3, BR-1	PIKE	41	6	
PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -	SCALE:				SHEET NO. OF SHEETS STA. TO STA.		CONTRACT NO. 72243				
PLOT DATE = Feb-07-2012 02:51:25PM	DATE -	REVISED -	ILLINOIS FED. AID PROJECT										

HOT-MIX ASPHALT SHOULDERS						
LOCATION STATION TO STATION	LENGTH (FT)	WIDTH (FT)	HMA SHOULDERS (6 1/2") (SQ YD)	EXCAVATING AND GRADING EXISTING SHOULDERS (UNIT)		
<b>FAP 304 (IL 96)</b>						
<b>LEFT</b>						
STA. 1273+10.50 TO STA. 1304+76.00	3,165.50	1	351.6			31.8
STA. 1324+25.00 TO STA. 1328+00.00	375.00	1	41.7			3.8
STA. 1333+36.00 TO STA. 1334+95.00	159.00	4	70.7			1.6
STA. 1340+29.50 TO STA. 1341+31.00	101.50	4	45.1			1.0
STA. 1348+00.00 TO STA. 1373+48.00	2,548.00	1	283.1			25.5
STA. 1373+48.00 TO STA. 1377+98.00	450.00	4	200.0			4.5
STA. 1377+98.00 TO STA. 1379+00.00	102.00	1	11.5			1.0
STA. 1381+91.00 TO STA. 1401+61.10	1,970.10	1	219.0			19.7
<b>STATION EQUATION STA. 1401+61.10 BK. = STA 1404+93.40 AH.</b>						
STA. 1404+93.40 TO STA. 1423+55.00	1,861.60	1	206.8			18.6
STA. 1423+55.00 TO STA. 1425+21.00	166.00	4	73.8			1.7
STA. 1425+21.00 TO STA. 1472+32.00	4,711.00	1	523.4			47.1
<b>RIGHT</b>						
STA. 1273+10.50 TO STA. 1304+76.00	3,165.50	1	351.7			31.8
STA. 1324+25.00 TO STA. 1328+00.00	375.00	1	41.7			3.8
STA. 1333+26.00 TO STA. 1334+95.00	169.00	4	75.1			1.7
STA. 1340+29.00 TO STA. 1340+70.00	41.00	4	18.2			0.4
STA. 1348+00.00 TO STA. 1371+80.00	2380.00	1	264.4			23.8
STA. 1371+80.00 TO STA. 1376+30.00	450.00	4	200.0			4.5
STA. 1376+30.00 TO STA. 1401+61.10	2,531.10	1	281.5			25.3
<b>STATION EQUATION STA. 1401+61.10 BK. = STA 1404+93.40 AH.</b>						
STA. 1404+93.40 TO STA. 1422+53.00	1,759.60	1	195.5			17.6
STA. 1422+53.00 TO STA. 1425+50.00	297.00	4	132.0			3.0
STA. 1425+50.00 TO STA. 1472+32.00	4,682.00	1	520.2			46.8
<b>TOTAL</b>			<b>4,107.0</b>	<b>315.0</b>		

NOTE: QUANTITY FOR OVERLAY OF SHOULDERS WITH HMA SURFACE COURSE MIX "C" N50 ARE INCLUDED IN THE PAVEMENT SCHEDULE.

AGGREGATE SHOULDERS TYPE B					
LOCATION STATION TO STATION	LENGTH (FT)	THICKNESS (INCH)	WIDTH (FT)	AGGREGATE SHOULDERS TYPE B (TONS)	
<b>FAP 304 (IL 96)</b>					
<b>RIGHT</b>					
STA. 1273+10.50 TO STA. 1307+45.00	3,434.50	4.5" AVER.	3	293.4	
STA. 1307+45.00 TO STA. 1310+28.36	283.36	0	0	0.0	
<b>BRIDGE OMISSION STA. 1310+28.36 TO STA. 1313+82.83</b>					
STA. 1313+82.83 TO STA. 1316+55.00	272.17	0	0	0.0	
STA. 1316+55.00 TO STA. 1334+95.16	1,840.16	4.5" AVER.	3	157.2	
<b>BRIDGE OMISSION STA. 1334+95.16 TO STA. 1340+29.50</b>					
STA. 1340+29.50 TO STA. 1375+09.00	3,479.50	4.5" AVER.	3	301.2	
STA. 1375+09.00 TO STA. 1381+91.00	682.00	6"	3	77.6	
STA. 1381+91.00 TO STA. 1401+61.10	1,970.10	4.5" AVER.	3	168.3	
<b>STATION EQUATION STA. 1401+61.10 BK. = STA. 1404+93.40 AH.</b>					
STA. 1404+93.40 TO STA. 1472+32.00	6,738.60	4.5" AVER.	3	575.5	
<b>LEFT</b>					
STA. 1273+10.50 TO STA. 1307+45.00	3,434.50	4.5" AVER.	3	305.4	
STA. 1307+45.00 TO STA. 1310+28.36	283.36	0	0	0.0	
<b>BRIDGE OMISSION STA. 1310+28.36 TO STA. 1313+82.83</b>					
STA. 1313+82.83 TO STA. 1316+55.00	272.17	0	0	0.0	
STA. 1316+55.00 TO STA. 1334+95.16	1,840.16	4.5" AVER.	3	157.2	
<b>BRIDGE OMISSION STA. 1334+95.16 TO STA. 1340+29.50</b>					
STA. 1340+29.50 TO STA. 1375+89.00	3,559.50	4.5" AVER.	3	304.0	
STA. 1375+89.00 TO STA. 1379+00.00	311.00	6"	3	35.4	
STA. 1379+00.00 TO STA. 1381+91.00	291.00				
STA. 1381+91.00 TO STA. 1401+61.10	1,970.10	4.5" AVER.	3	168.3	
<b>STATION EQUATION STA. 1401+61.10 BK. = STA. 1404+93.40 AH.</b>					
STA. 1404+93.40 TO STA. 1472+32.00	6,738.60	4.5" AVER.	3	591.5	
<b>TOTAL</b>			<b>3,135.0</b>		

PAVEMENT PATCHING			
LOCATION STATION	CLASS C PATCHES TYPE II 10 INCH (SQ YD)	CLASS C PATCHES TYPE III 10 INCH (SQ YD)	PAVEMENT FABRIC (SQ YD)
<b>FAP 304 (IL 96)</b>			
<b>RIGHT</b>			
STA. 1275+94.00	11.0		11.0
STA. 1302+65.00	7.5		7.5
STA. 1352+00.00	11.0		11.0
STA. 1374+91.00		19.5	19.5
STA. 1382+19.00	11.0		11.0
STA. 1393+81.00	11.0		11.0
STA. 1419+02.00	8.5		8.5
STA. 1424+78.00		16.0	16.0
STA. 1453+67.00		16.0	16.0
<b>LEFT</b>			
STA. 1275+94.00	11.0		11.0
STA. 1302+65.00	7.5		7.5
STA. 1352+00.00	11.0		11.0
STA. 1374+91.00		19.5	19.5
STA. 1382+19.00	11.0		11.0
STA. 1393+81.00	11.0		11.0
STA. 1419+02.00	8.5		8.5
STA. 1424+78.00		16.0	16.0
STA. 1453+67.00		16.0	16.0
<b>TOTAL</b>	<b>120.0</b>	<b>103.0</b>	<b>223.0</b>

GUTTER REMOVAL	
LOCATION STATION TO STATION	GUTTER REMOVAL (FOOT)
<b>FAP 304 (IL 96)</b>	
<b>RIGHT</b>	
STA. 1316+55.00 TO STA. 1316+64.75	9.5
STA. 1375+09.00 TO STA. 1381+91.00	682.0
<b>LEFT</b>	
STA. 1316+55.00 TO STA. 1316+64.75	9.5
STA. 1375+89.00 TO STA. 1379+00.00	311.0
<b>TOTAL</b>	<b>1,012.0</b>

SUB-BASE GRANULAR MATERIAL, TYPE B	
LOCATION STATION TO STATION	SUB-BASE GRANULAR MATERIAL, TYPE B (TON)
<b>FAP 304 (IL 96)</b>	
<b>RIGHT</b>	
STA. 1375+09.00 TO STA. 1381+91.00	207.0
<b>LEFT</b>	
STA. 1375+89.00 TO STA. 1379+00.00	139.0
<b>TOTAL</b>	<b>346.0</b>

TEMPORARY RAMPS				
LOCATION STATION TO STATION		LENGTH (FT)	WIDTH (FT)	AREA (SQ YD)
<b>FAP304 (IL 96)</b>				
STA. 1273+10.50 TO STA. 1273+45.50	BUTT-JOINT	5	22	12.2
STA. 1471+97.00 TO STA. 1472+32.00	BUTT-JOINT	5	22	12.2
<b>SIDE ROADS</b>				
LT. STA. 1301+12.38.00	HIGHWAY 10	5	89	49.3
RT. STA. 1332+90.42.00	120TH AVENUE	5	95	52.7
LT. STA. 1341+62.43.00	120TH AVENUE	5	74	41.0
LT. STA. 1379+51.52.00	115TH AVENUE	-	-	-
RT. STA. 1410+38.22.00	110TH AVENUE	5	57	31.6
<b>TOTAL</b>			<b>199.0</b>	



ENTRANCE SCHEDULE										
LOCATION FAP 304 (IL 96)	WIDTH ACROSS FRONT (FT)	WIDTH ACROSS BACK (FT)	LENGTH OF IMPROVEMENT FROM EOP (FT)	EXIST. SURF. TYPE	PREPARATION OF BASE (SQ YD)	HOT-MIX ASPH. SURF. REMOVAL BUTT-JOINT (SQ YD)	INCIDENTAL HOT-MIX ASPH. SURF. (TON)	BIT. MAT. PRIME COAT (TON)	AGGREGATE PRIME COAT (TON)	AGGREGATE SURF. CSE. TYPE B (TON)
PE W/MBT LT. STA. 1275+61.00	69	44	8	AGG/HMA	21.5	28.6	6.1	0.05	---	---
PE W/MBT LT. STA. 1276+35.50	60	34	8	AGG/HMA	18.3	23.0	6.0	0.05	---	---
PE RT. STA. 1288+83.00	70	54	8	AGG	55.0	---	10.8	0.02	---	4.8
MBT LT. STA. 1288+83.00	64	20	8	AGG	37.3	---	7.3	0.06	---	---
PE RT. STA. 1291+93.50	29	13	8	HMA	---	18.7	1.6	0.01	---	---
MBT LT. STA. 1291+93.50	64	20	8	AGG	37.3	---	7.3	0.06	---	---
PE LT. STA. 1294+23.00	39.5	22	8	AGG/HMA	12.3	15.6	3.7	0.02	---	---
PE LT. STA. 1299+80.50	52	36	8	AGG	39.1	---	7.7	0.07	---	3.2
HWY 10 LT STA. 1301+12.50	89	56	10	HMA	---	80.5	6.8	0.03	0.2	---
MBT LT. STA. 1302+81.00	64	20	8	AGG	37.3	---	7.3	0.06	---	---
CE RT. STA. 1303+26.50	88.5	68.5	10	AGG	87.2	---	39.1	0.14	---	4.3
CF RT. STA. 1305+68.00	63	43	10	AGG	58.9	---	26.4	0.10	---	2.7
123rd LN. LT. STA. 1306+19.00	168	120.5	10	AGG	160.3	---	71.8	0.25	---	7.5
FE LT. STA. 1320+87.00	---	---	---	SOD	---	---	---	---	---	---
PE LT. STA. 1323+34.00	139	123	8.5	AGG	123.7	---	24.2	0.20	---	10.8
PE LT. STA. 1323+98.00	---	---	---	---	---	---	---	---	---	---
120th AVE. RT. STA. 1332+90.50	92.5	20	51	HMA/AGG	---	233.0	19.6	0.12	0.5	1.3
FE RT. STA. 1340+74.50	---	---	---	SOD	---	---	---	---	---	---
120th AVE. LT. STA. 1341+62.50	74	19	54	HMA/AGG	---	205.0	17.2	0.08	0.5	1.2
PE W/MBT RT. STA. 1345+45.50	64	39	8	HMA/AGG	29.0	18.5	7.3	0.06	---	---
PE RT. STA. 1347+50.00	30	14	8	AGG	19.6	---	3.8	0.10	---	1.2
FE RT. STA. 1347+52.00	---	---	---	SOD	---	---	---	---	---	---
FE LT. STA. 1348+52.00	---	---	---	SOD	---	---	---	---	---	---
FE RT. STA. 1357+54.50	---	---	---	SOD	---	---	---	---	---	---
FE LT. STA. 1357+99.50	---	---	---	SOD	---	---	---	---	---	---
FE LT. STA. 1360+95.00	---	---	---	SOD	---	---	---	---	---	---
FE RT. STA. 1360+98.50	---	---	---	SOD	---	---	---	---	---	---
115th AVE. LT. STA. 1379+51.50	---	---	---	HMA	---	---	---	---	---	---
FE RT. STA. 1381+47.00	---	---	---	SOD	---	---	---	---	---	---
FE LT. STA. 1384+78.50	---	---	---	SOD	---	---	---	---	---	---
FE RT. STA. 1391+93.00	---	---	---	SOD	---	---	---	---	---	---
PE LT. STA. 1392+51.50	36	20	8	AGG	24.9	---	4.9	0.04	---	1.8
PE LT. STA. 1393+51.00	34	18	8	HMA/AGG	---	23.1	1.9	0.01	---	1.6
FE LT. STA. 1394+28.50	---	---	---	SOD	---	---	---	---	---	---
CE LT. STA. 1405+61.00	129	109	10	HMA	---	132.1	11.1	0.05	---	---
CE LT. STA. 1405+99.00	---	---	---	---	---	---	---	---	---	---
FE LT. STA. 1409+16.00	---	---	---	SOD	---	---	---	---	---	---
110th AVE. RT. STA. 1410+38.50	57	23	24.5	HMA/AGG	---	108.8	9.1	0.04	0.3	1.5
PE LT. STA. 1416+51.00	53	37	8	AGG	40.0	---	7.8	0.07	---	3.3
FE RT. STA. 1418+92.00	---	---	---	SOD	---	---	---	---	---	---
FE RT. STA. 1420+61.00	---	---	---	SOD	---	---	---	---	---	---
PE LT. STA. 1420+62.50	68	52	8	AGG	53.3	---	10.5	0.10	---	4.6
PE LT. STA. 1425+41.50	46	30	8	AGG	33.8	---	6.6	0.06	---	2.7
MBT RT. STA. 1425+56.00	64	20	8	AGG	37.3	---	7.3	0.06	---	---
FE LT. STA. 1430+57.50	---	---	---	SOD	---	---	---	---	---	---
PE LT. STA. 1434+11.00	39	23	8	AGG	27.6	---	5.4	0.05	---	2.0
FE RT. STA. 1434+97.50	---	---	---	SOD	---	---	---	---	---	---
PE LT. STA. 1435+45.00	35	19.5	8	AGG	24.2	---	4.7	0.03	---	1.7
PE W/MBT RT. STA. 1443+82.50	55	29	8	HMA	21.9	15.4	5.6	0.05	---	---
MBT RT. STA. 1451+47.00	64	20	8	AGG	37.3	---	7.3	0.06	---	---
PE LT. STA. 1451+58.00	37	21	8	HMA	---	25.8	2.2	0.01	---	---
PE RT. STA. 1452+69.00	39	23	8	AGG	27.6	---	5.4	0.05	---	2.0
PE LT. STA. 1456+24.00	52	36	8	AGG	39.0	---	7.7	0.07	---	3.2
MBT RT. STA. 1456+30.00	64	20	8	AGG	37.3	---	7.3	0.06	---	---
PE RT. STA. 1465+99.50	56	42	8	AGG	43.6	---	8.5	0.07	---	3.7
PE LT. STA. 1466+33.50	83	67	8	AGG	66.6	---	13.1	0.10	---	5.8
PE RT. STA. 1466+68.00	36	20	8	HMA	---	24.9	2.1	0.01	---	---
PE LT. STA. 1467+47.50	28	12	8	AGG	17.8	---	3.5	0.03	---	1.1
<b>TOTAL</b>					1,269.0	953.0	406.0	2.5	1.5	72.0

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SCHEDULE OF QUANTITIES</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pw\work\p\idot\sparksgw\0216263\067243-sht-schedule.dgn	DRAWN -	REVISED -	304						4RS-3, BR-1	PIKE	41	8	
PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -	SCALE: SHEET NO. OF SHEETS STA. TO STA.				CONTRACT NO. 72243						
PLOT DATE = Feb-07-2012 02:51:58PM	DATE -	REVISED -	ILLINOIS FED. AID PROJECT										



RIPRAP SCHEDULE		
LOCATION STATION	STONE DUMPED RIPRAP CLASS A4 (SQ YD)	FILTER FABRIC (SQ YD)
<b>FAP 304 (IL 96)</b>		
<b>RIGHT</b>		
STA. 1281+98.00	60.0	60.0
STA. 1294+14.00	120.0	120.0
STA. 1298+95.00	60.0	60.0
STA. 1316+63.00	32.0	32.0
STA. 1321+97.00	30.0	30.0
STA. 1326+68.00	30.0	30.0
STA. 1329+59.00	30.0	30.0
STA. 1410+79.00	64.0	64.0
STA. 1424+78.00	45.0	45.0
STA. 1453+67.00	50.0	50.0
STA. 1462+85.00	5.0	5.0
STA. 1467+46.00	32.0	32.0
STA. 1469+76.00	32.0	32.0
<b>LEFT</b>		
STA. 1298+95.00	30.0	30.0
STA. 1316+63.00	30.0	30.0
STA. 1375+30.00	12.0	12.0
<b>TOTAL</b>	<b>662.0</b>	<b>662.0</b>

CAST - IN - PLACE REINFORCED END SECTIONS		
LOCATION STATION	CAST - IN - PLACE REINFORCED END SECTIONS 24' (EACH)	CAST - IN - PLACE REINFORCED END SECTIONS 30" (EACH)
<b>FAP 304 (IL 96)</b>		
<b>RIGHT</b>		
STA. 1298+95.00		1.0
STA. 1302+65.00	1.0	
<b>TOTAL</b>	<b>1.0</b>	<b>1.0</b>

CULVERT SCHEDULE						
LOCATION STATION	PIPE CULVERTS TO BE CLEANED 24" (FOOT)	PIPE CULVERTS TO BE CLEANED 30" (FOOT)	PIPE CULVERTS TO BE CLEANED 36" (FOOT)	INSERTION CULVERT LINER 24" (FOOT)	INSERTION CULVERT LINER 30" (FOOT)	INSERTION CULVERT LINER 36" (FOOT)
<b>FAP 304 (IL 96)</b>						
STA. 1294+14.00			47.00			47.0
STA. 1304+39.00		45.00			45.0	
STA. 1390+24.00		43.00			43.0	
STA. 1432+97.00		51.00			51.0	
STA. 1462+85.00	45.00			45.0		
<b>TOTAL</b>	<b>45.0</b>	<b>139.0</b>	<b>47.0</b>	<b>45.0</b>	<b>139.0</b>	<b>47.0</b>

CLASS SI CONCRETE (OUTLET)	
LOCATION STATION TO STATION	CLASS SI CONCRETE (OUTLET) (CU YD)
<b>FAP 304 (IL 96)</b>	
<b>RIGHT</b>	
STA. 1316+55.00 TO STA. 1316+64.75	1.5
<b>LEFT</b>	
STA. 1316+55.00 TO STA. 1316+64.75	1.5
STA. 1378+25.00 TO STA. 1379+00.00	7.5
<b>TOTAL</b>	<b>10.5</b>

DRAINAGE SCHEDULE			
LOCATION STATION	TYPE B INLET BOX (EACH)	METAL END SECTIONS 12" (EACH)	PIPE DRAIN 12" (FOOT)
<b>FAP 304 (IL 96)</b>			
<b>RIGHT</b>			
STA. 1316+63.00	1.0	1.0	7.0
<b>LEFT</b>			
STA. 1316+63.00	1.0	1.0	9.0
<b>TOTAL</b>	<b>2.0</b>	<b>2.0</b>	<b>16.0</b>

RAISED REFLECTIVE PAVEMENT MARKERS SCHEDULE				
LOCATION STATION TO STATION	LENGTH (FT)	TWO-WAY AMBER (EACH)	REMOVAL (EACH)	
<b>FAP 304 (IL 96)</b>				
STA. 1273+10.50 TO STA. 1310+28.36	3,717.86	47	47	
<b>BRIDGE OMISSION STA. 1310+28.36 TO STA. 1313+82.83</b>				
STA. 1313+82.83 TO STA. 1334+95.16	2,112.33	27	27	
<b>BRIDGE OMISSION STA. 1334+95.16 TO STA. 1340+29.50</b>				
STA. 1340+29.50 TO STA. 1401+61.10	6,131.60	77	77	
<b>STATION EQUATION STA. 1401+61.10 BK. = STA. 1404+93.40 AH.</b>				
STA. 1404+93.40 TO STA. 1472+32.00	6,738.60	84	84	
<b>TOTAL</b>		<b>235</b>	<b>235</b>	

EARTHWORK SCHEDULE				
LOCATION STATION TO STATION	1 EARTH EXCAVATION (CU YD)	2 * EXCAVATION TO BE USED IN EMBANKMENT (ADJUSTED FOR SHRINKAGE) (COLUMN 1 x 0.75) (CU YD)	3 * EMBANK. (FILL)	EARTHWORK BALANCE WASTE (+) OR FURNISHED EXCAVATION (-) (COLUMN 2 - COLUMN 3) (CU YD)
<b>F.A.P. (IL 96)</b>				
STA. 1374+50.00 TO STA. 1382+00.00	1,226.0	920.0	75.0	845.0
<b>TOTALS</b>	<b>1,226.0</b>	<b>920.0</b>	<b>75.0</b>	<b>845.0</b>

EARTH EXCAVATION SHRINKAGE FACTOR = 25%  
 ITEMS MARKED WITH ASTERISK (\*) ARE FOR INFORMATION PURPOSES ONLY

NOTE:  
 AN ESTIMATED QUANTITY OF 30 TONS OF AGGREGATE (EROSION CONTROL) HAS BEEN INCLUDED FOR USE FOR TEMPORARY EROSION CONTROL AS DIRECTED BY THE ENGINEER.

SEEDING SCHEDULE						
LOCATION	SEEDING CLASS 2 (ACRE)	NITROGEN FERTILIZER NUTRIENT (POUND)	PHOSPHORUS FERTILIZER NUTRIENT (POUND)	POTASSIUM FERTILIZER NUTRIENT (POUND)	AGRICULTURAL GROUND LIME (TON)	MULCH METHOD 2 (ACRE)
<b>F.A.P. (IL 96)</b>						
STA. 1374+50.00 TO STA. 1382+00.00	0.8	72.0	72.0	72.0	1.6	0.8
<b>TOTALS</b>	<b>0.8</b>	<b>72.0</b>	<b>72.0</b>	<b>72.0</b>	<b>1.6</b>	<b>0.8</b>

SHORT-TERM PAVEMENT MARKING							
LOCATION STATION TO STATION	DESCRIPTION	LENGTH (FT)	SPACING	NUMBER OF APPLICATIONS	SHORT-TERM PAVE MARK (FT)	WORK ZONE PAVE MARK REM (SQ FT)	
<b>FAP 304 (IL 96)</b>							
STA. 1273+10.50 TO STA. 1310+28.36	SKIP - DASH	3,717.86	4' @ 40'	3	1116.0	123.0	
<b>BRIDGE OMISSION STA. 1310+28.36 TO STA. 1313+82.83</b>							
STA. 1313+82.83 TO STA. 1334+95.16	SKIP - DASH	2,112.33	4' @ 40'	3	634.0	70.0	
<b>BRIDGE OMISSION STA. 1334+95.16 TO STA. 1340+29.50</b>							
STA. 1340+29.50 TO STA. 1401+61.10	SKIP - DASH	6,131.60	4' @ 40'	3	1,840.0	202.0	
<b>STATION EQUATION STA. 1401+61.10 BK. = STA. 1404+93.40 AH.</b>							
STA. 1404+93.40 TO STA. 1472+32.00	SKIP - DASH	6,738.60	4' @ 40'	3	2,022.0	222.0	
<b>TOTAL</b>					<b>5,612.0</b>	<b>617.0</b>	

NOTE: SHORT-TERM PAVEMENT MARKING QUANTITIES ARE FOR THREE APPLICATIONS.

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
ei:\pwwork\pwwork\sparksgw\10216263\067243-sht-schedule.dgn		DRAWN -	REVISED -
	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = Feb-07-2012 02:52:05PM	DATE -	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	4RS-3, BR-1	PIKE	41	9
CONTRACT NO. 72243				
ILLINOIS FED. AID PROJECT				

PAINT PAVEMENT MARKING - LINE 5" SCHEDULE						
LOCATION STATION TO STATION	DESCRIPTION	LENGTH (FT)	SPACING	LINE 5"		
				WHITE (FT)	YELLOW (FT)	
<b>FAP 304 (IL 96)</b>						
STA. 1273+10.50 TO STA. 1301+78.50	SOLID (EDGE LINE LT)	2,868.0	---	2,779.00	---	
STA. 1273+10.50 TO STA. 1301+78.50	SOLID (EDGE LINE RT)	2,868.0	---	2,868.00	---	
STA. 1273+10.50 TO STA. 1301+78.50	SKIP-DASH (CENTER)	2,868.0	10' @ 40'	---	717.00	
STA. 1301+78.50 TO STA. 1309+35.50	SOLID (EDGE LINE LT)	757.0	---	757.00	---	
STA. 1301+78.50 TO STA. 1309+35.50	SOLID (EDGE LINE RT)	757.0	---	757.00	---	
STA. 1301+78.50 TO STA. 1309+35.50	SKIP-DASH (CENTER)	757.0	10' @ 40'	---	189.00	
STA. 1301+78.50 TO STA. 1309+35.50	SOLID (NORTH BOUND NPZ)	757.0	---	---	757.00	
STA. 1309+35.50 TO STA. 1313+15.50	SOLID (EDGE LINE LT)	380.0	---	380.00	---	
STA. 1309+35.50 TO STA. 1313+15.50	SOLID (EDGE LINE RT)	380.0	---	380.00	---	
STA. 1309+35.50 TO STA. 1313+15.50	SOLID (NORTH BOUND NPZ)	380.0	---	---	380.00	
STA. 1309+35.50 TO STA. 1313+15.50	SOLID (SOUTH BOUND NPZ)	380.0	---	---	380.00	
STA. 1313+15.50 TO STA. 1324+55.50	SOLID (EDGE LINE LT)	1,140.0	---	1,140.00	---	
STA. 1313+15.50 TO STA. 1324+55.50	SOLID (EDGE LINE RT)	1,140.0	---	1,047.50	---	
STA. 1313+15.50 TO STA. 1324+55.50	SKIP-DASH (CENTER)	1,140.0	10' @ 40'	---	285.00	
STA. 1313+15.50 TO STA. 1324+55.50	SOLID (SOUTH BOUND NPZ)	1,140.0	---	---	215.00	
STA. 1324+55.50 TO STA. 1325+14.50	SOLID (EDGE LINE LT)	59.0	---	59.00	---	
STA. 1324+55.50 TO STA. 1325+14.50	SOLID (EDGE LINE RT)	59.0	---	59.00	---	
STA. 1324+55.50 TO STA. 1325+14.50	SOLID NORTH BOUND NPZ)	59.0	---	---	59.00	
STA. 1324+55.50 TO STA. 1325+14.50	SOLID (SOUTH BOUND NPZ)	59.0	---	---	59.00	
STA. 1325+14.50 TO STA. 1333+74.50	SOLID (EDGE LINE LT)	860.0	---	860.00	---	
STA. 1325+14.50 TO STA. 1333+74.50	SOLID (EDGE LINE RT)	860.0	---	860.00	---	
STA. 1325+14.50 TO STA. 1333+74.50	SKIP-DASH (CENTER)	860.0	10' @ 40'	---	215.00	
STA. 1325+14.50 TO STA. 1333+74.50	SOLID (NORTH BOUND NPZ)	860.0	---	---	860.00	
STA. 1333+74.50 TO STA. 1338+51.50	SOLID (EDGE LINE LT)	477.0	---	477.00	---	
STA. 1333+74.50 TO STA. 1338+51.50	SOLID (EDGE LINE RT)	477.0	---	477.00	---	
STA. 1333+74.50 TO STA. 1338+51.50	SOLID (NORTH BOUND NPZ)	477.0	---	---	477.00	
STA. 1333+74.50 TO STA. 1338+51.50	SOLID (SOUTH BOUND NPZ)	477.0	---	---	477.00	
STA. 1338+51.50 TO STA. 1347+37.50	SOLID (EDGE LINE LT)	812.0	---	886.00	---	
STA. 1338+51.50 TO STA. 1347+37.50	SOLID (EDGE LINE RT)	886.0	---	886.00	---	
STA. 1338+51.50 TO STA. 1347+37.50	SKIP-DASH (CENTER)	886.0	10' @ 40'	---	221.50	
STA. 1338+51.50 TO STA. 1347+37.50	SOLID (SOUTH BOUND NPZ)	886.0	---	---	886.00	
STA. 1347+37.50 TO STA. 1372+23.50	SOLID (EDGE LINE LT)	2,486.0	---	2,486.00	---	
STA. 1347+37.50 TO STA. 1372+23.50	SOLID (EDGE LINE RT)	2,486.0	---	2,486.00	---	
STA. 1347+37.50 TO STA. 1372+23.50	SKIP-DASH (CENTER)	2,486.0	10' @ 40'	---	621.50	
STA. 1372+23.50 TO STA. 1375+58.50	SOLID (EDGE LINE LT)	335.0	---	335.00	---	
STA. 1372+23.50 TO STA. 1375+58.50	SOLID (EDGE LINE RT)	335.0	---	335.00	---	
STA. 1372+23.50 TO STA. 1375+58.50	SKIP-DASH (CENTER)	335.0	10' @ 40'	---	84.00	
STA. 1372+23.50 TO STA. 1375+58.50	SOLID (NORTH BOUND NPZ)	335.0	---	---	335.00	
STA. 1375+58.50 TO STA. 1380+64.50	SOLID (EDGE LINE LT)	506.0	---	464.50	---	
STA. 1375+58.50 TO STA. 1380+64.50	SOLID (EDGE LINE RT)	506.0	---	506.00	---	
STA. 1375+58.50 TO STA. 1380+64.50	SOLID (NORTH BOUND NPZ)	506.0	---	---	506.00	
STA. 1375+58.50 TO STA. 1380+64.50	SOLID (SOUTH BOUND NPZ)	506.0	---	---	506.00	
STA. 1380+64.50 TO STA. 1389+08.50	SOLID (EDGE LINE LT)	844.0	---	844.00	---	
STA. 1380+64.50 TO STA. 1389+08.50	SOLID (EDGE LINE RT)	844.0	---	844.00	---	
STA. 1380+64.50 TO STA. 1389+08.50	SKIP-DASH (CENTER)	844.0	10' @ 40'	---	211.00	
STA. 1380+64.50 TO STA. 1389+08.50	SOLID (SOUTH BOUND NPZ)	844.0	---	---	844.00	
STA. 1389+08.50 TO STA. 1401+61.10	SOLID (EDGE LINE LT)	1,252.6	---	1,252.50	---	
STA. 1389+08.50 TO STA. 1401+61.10	SOLID (EDGE LINE RT)	1,252.6	---	1,252.50	---	
STA. 1389+08.50 TO STA. 1401+61.10	SKIP-DASH (CENTER)	1,252.6	10' @ 40'	---	313.00	
<b>STATION EQUATION STA. 1401+61.10 BK. = STA. 1404+93.40 AH.</b>						
<b>SUB-TOTAL TABLE 1</b>				<b>25,478.00</b>	<b>9,598.00</b>	
<b>TOTAL TABLE 1</b>				<b>35,076.00</b>		

PAINT PAVEMENT MARKING - LINE 5" SCHEDULE						
LOCATION STATION TO STATION	DESCRIPTION	LENGTH (FT)	SPACING	LINE 5"		
				WHITE (FT)	YELLOW (FT)	
<b>STATION EQUATION STA. 1401+61.10 BK. = STA. 1404+93.40 AH.</b>						
STA. 1404+93.40 TO STA. 1406+26.80	SOLID (EDGE LINE LT)	133.40	---	133.5	---	
STA. 1404+93.40 TO STA. 1406+26.80	SOLID (EDGE LINE RT)	133.40	---	133.5	---	
STA. 1404+93.40 TO STA. 1406+26.80	SKIP-DASH (CENTER)	133.40	10' @ 40'	---	33.5	
STA. 1406+26.80 TO STA. 1412+69.80	SOLID (EDGE LINE LT)	643.00	---	643.0	---	
STA. 1406+26.80 TO STA. 1412+69.80	SOLID (EDGE LINE RT)	586.00	---	643.0	---	
STA. 1406+26.80 TO STA. 1412+69.80	SKIP-DASH (CENTER)	643.00	10' @ 40'	---	161.0	
STA. 1406+26.80 TO STA. 1412+69.80	SOLID (NORTH BOUND NPZ)	643.00	---	---	643.0	
STA. 1412+69.80 TO STA. 1414+31.80	SOLID (EDGE LINE LT)	162.00	---	162.0	---	
STA. 1412+69.80 TO STA. 1414+31.80	SOLID (EDGE LINE RT)	162.00	---	162.0	---	
STA. 1412+69.80 TO STA. 1414+31.80	SOLID (NORTH BOUND NPZ)	162.00	---	---	162.0	
STA. 1412+69.80 TO STA. 1414+31.80	SOLID (SOUTH BOUND NPZ)	162.00	---	---	162.0	
STA. 1414+31.80 TO STA. 1423+31.80	SOLID (EDGE LINE LT)	900.00	---	900.0	---	
STA. 1414+31.80 TO STA. 1423+31.80	SOLID (EDGE LINE RT)	900.00	---	900.0	---	
STA. 1414+31.80 TO STA. 1423+31.80	SKIP-DASH (CENTER)	900.00	10' @ 40'	---	225.0	
STA. 1414+31.80 TO STA. 1423+31.80	SOLID (SOUTH BOUND NPZ)	900.00	---	---	900.0	
STA. 1423+31.80 TO STA. 1438+81.80	SOLID (EDGE LINE LT)	1,550.00	---	1,550.0	---	
STA. 1423+31.80 TO STA. 1438+81.80	SOLID (EDGE LINE RT)	1,550.00	---	1,550.0	---	
STA. 1423+31.80 TO STA. 1438+81.80	SKIP-DASH (CENTER)	1,550.00	10' @ 40'	---	387.5	
STA. 1438+81.80 TO STA. 1446+04.80	SOLID (EDGE LINE LT)	723.00	---	723.0	---	
STA. 1438+81.80 TO STA. 1446+04.80	SOLID (EDGE LINE RT)	723.00	---	723.0	---	
STA. 1438+81.80 TO STA. 1446+04.80	SKIP-DASH (CENTER)	723.00	10' @ 40'	---	181.0	
STA. 1438+81.80 TO STA. 1446+04.80	SOLID (NORTH BOUND NPZ)	723.00	---	---	723.0	
STA. 1446+04.80 TO STA. 1447+16.80	SOLID (EDGE LINE LT)	112.00	---	112.0	---	
STA. 1446+04.80 TO STA. 1447+16.80	SOLID (EDGE LINE RT)	112.00	---	112.0	---	
STA. 1446+04.80 TO STA. 1447+16.80	SKIP-DASH (CENTER)	112.00	10' @ 40'	---	28.0	
STA. 1447+16.80 TO STA. 1453+70.80	SOLID (EDGE LINE LT)	654.00	---	654.0	---	
STA. 1447+16.80 TO STA. 1453+70.80	SOLID (EDGE LINE RT)	654.00	---	654.0	---	
STA. 1447+16.80 TO STA. 1453+70.80	SKIP-DASH (CENTER)	654.00	10' @ 40'	---	163.5	
STA. 1447+16.80 TO STA. 1453+70.80	SOLID (SOUTH BOUND NPZ)	654.00	---	---	654.0	
STA. 1453+70.80 TO STA. 1454+51.80	SOLID (EDGE LINE LT)	81.00	---	81.0	---	
STA. 1453+70.80 TO STA. 1454+51.80	SOLID (EDGE LINE RT)	81.00	---	81.0	---	
STA. 1453+70.80 TO STA. 1454+51.80	SOLID (NORTH BOUND NPZ)	81.00	---	---	81.0	
STA. 1453+70.80 TO STA. 1454+51.80	SOLID (SOUTH BOUND NPZ)	81.00	---	---	81.0	
STA. 1454+51.80 TO STA. 1462+72.80	SOLID (EDGE LINE LT)	821.00	---	821.0	---	
STA. 1454+51.80 TO STA. 1462+72.80	SOLID (EDGE LINE RT)	821.00	---	821.0	---	
STA. 1454+51.80 TO STA. 1462+72.80	SKIP-DASH (CENTER)	821.00	10' @ 40'	---	205.5	
STA. 1454+51.80 TO STA. 1462+72.80	SOLID (NORTH BOUND NPZ)	821.00	---	---	821.0	
STA. 1462+72.80 TO STA. 1472+32.00	SOLID (EDGE LINE LT)	959.20	---	959.0	---	
STA. 1462+72.80 TO STA. 1472+32.00	SOLID (EDGE LINE RT)	959.20	---	959.0	---	
STA. 1462+72.80 TO STA. 1472+32.00	SKIP-DASH (CENTER)	959.20	10' @ 40'	---	240.0	
STA. 1462+72.80 TO STA. 1472+32.00	SOLID (SOUTH BOUND NPZ)	959.20	---	---	959.0	
<b>SUB-TOTAL TABLE 2</b>				<b>13,477.0</b>	<b>6,811.0</b>	
<b>TOTAL TABLE 2</b>				<b>20,288.0</b>		
<b>TOTAL TABLES 1 &amp; 2</b>				<b>55,364.0</b>		

NOTE: QUANTITIES FOR TEMPORARY PAVEMENT MARKING -LINE 5" ARE THE SAME AS PAINT PAVEMENT MARKING-LINE 5".

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SCHEDULE OF QUANTITIES</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ei:\pw\work\p\midot\sparksgw\10216263\0672243-sht-schedule.dgn	DRAWN -	REVISED -	304					4RS-3, BR-1	PIKE	41	10	
PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -	CONTRACT NO. 72243									
PLOT DATE = Feb-07-2012 02:52:15PM	DATE -	REVISED -	SCALE:		SHEET NO.	OF SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT			

SIGN REPLACEMENT SCHEDULE											
STATION	FACING	OFFSET	SIGN	SIGN DESIGNATION	SIGN SIZE (in.)	Sign Panel Ty 1 Spl (Sq. Ft.)	Sign Panel Ty 2 Spl (Sq. Ft.)	Rem Sign Panel Assembly Ty A Spl (Each)	Rem Sign Panel Assembly Ty B Spl (Each)	Number of Posts (Each)	Telescoping Stl Sign Support (Foot)
1272+58	WEST	35 RT	STOP	R1-1	36 x 36	-	9	-	1	1	14
1272+58	EAST	35 RT	STOP	R1-1	36 x 36	-	9	-	1	1	14
1272+67	EAST/WEST	37 RT	ILL 96	D3-1	30 x 24	5	-	1	-	1	14
1272+67	NORTH/SOUTH	37 RT	355TH ST	DO NOT REPLACE	-	-	-	-	-	-	-
1273+10	SOUTH	25 LT	SPEED LIMIT 25	R2-1	24 x 30	5	-	-	1	2	28
1273+10	SOUTH	25 LT	SCHOOL SPEED LIMIT 20	DISTRICT SIGN	36 x 72	-	18	-	1	2	28
1273+10	SOUTH	25 LT	PLEASANT HILL POP BOARD	DISTRICT SIGN	30 x 48	-	10	-	1	2	28
1273+20	NORTH	23 RT	SPEED LIMIT 55	R2-1	24 x 30	5	-	-	1	2	28
1273+20	NORTH	23 RT	BELLEVIEW 8, KAMPSVILLE 21	D2-2	24 x 48	8	-	-	1	2	28
1277+25	SOUTH	22 LT	INTERSECTION WARNING	W2-2	36 x 36	-	9	-	1	1	14
1278+22	SOUTH	25 LT	SCHOOL	S1-1	36 x 36	-	9	-	1	1	14
1278+22	SOUTH	20 LT	COMMUNITY AWARD	DO NOT REPLACE	-	-	-	-	-	-	-
1278+22	SOUTH	20 LT	CRIMESTOPPER	DO NOT REPLACE	-	-	-	-	-	-	-
1280+25	SOUTH	28 LT	SPEED LIMIT 25	R2-1	24 x 30	5	-	1	-	1	14
1294+70	NORTH	25 RT	INTERSECTION WARNING	W2-2	36 x 36	-	9	-	1	2	28
1294+70	NORTH	25 RT	VIN FIZ HWY (PLAQUE)	W16-8P	24 x 30	5	-	-	1	1	14
1294+84	SOUTH	24 LT	SPEED ZONE AHEAD 25MPH	W3-5	36 x 36	-	9	-	1	1	14
1297+15	NORTH	24 RT	TRUCK	W11-10	36 x 36	-	9	-	1	1	14
1298+65	SOUTH	20 LT	ILLINOIS 96	DISTRICT SIGN	30 x 24	5	-	1	-	1	14
1298+65	SOUTH	20 LT	NORTH	M3-1	30 x 15	3.1	-	-	1	1	14
1298+65	SOUTH	22 LT	GREAT RIVER RD	DISTRICT SIGN	36 x 36	-	9	-	1	1	14
1298+65	SOUTH	22 LT	NORTH	DISTRICT SIGN	30 x 15	3.1	-	-	1	1	14
1299+60	NORTH	25 RT	NEBO	D1-1	30 x 15	3.1	-	1	-	1	14
1300+87	WEST	33 LT	STOP	R1-1	36 x 36	-	9	-	1	1	14
1300+87	EAST	33 LT	STOP	R1-1	36 x 36	-	9	-	1	1	14
1301+10	ACROSS	30 RT	TWO-DIRECTION ARROW	W1-7	30 x 18	3.8	-	1	-	2	28
1301+10	ACROSS	30 RT	TYPE 1 OBJECT MARKER	OM1-3	18 x 18	2.3	-	-	-	-	-
1301+10	ACROSS	30 RT	TYPE 1 OBJECT MARKER	OM1-3	18 x 18	2.3	-	-	-	-	-
1301+57	EAST/WEST	33 LT	IL 96	D3-1	30 x 24	5	-	-	1	1	14
1301+57	NORTH/SOUTH	33 LT	HWY 10	D3-2	30 x 24	5	-	-	1	1	14
1302+40	NORTH	33 LT	NO PASSING ZONE	W14-3	64 x 48	-	10.7	-	1	1	14
1303+00	SOUTH	27 LT	NEBO	D1-1	30 x 15	3.1	-	1	-	1	14
1307+82	NORTH	20 RT	ILLINOIS 96	DISTRICT SIGN	30 x 24	5	-	-	1	1	14
1307+82	NORTH	20 RT	SOUTH	M3-3	30 x 15	3.1	-	-	1	1	14
1307+82	NORTH	20 RT	96 PIKE 3.00 CRM	DISTRICT SIGN	18 x 18	2.3	-	-	1	1	14
1307+82	NORTH	20 RT	96 PIKE 3.00 CRM	DISTRICT SIGN	18 x 18	2.3	-	-	1	1	14
1307+82	NORTH	22 RT	GREAT RIVER RD	DISTRICT SIGN	36 x 36	-	9	-	1	1	14
1307+82	NORTH	22 RT	SOUTH	DISTRICT SIGN	30 x 15	3.1	-	-	1	1	14
1307+82	SOUTH	22 LT	INTERSECTION WARNING	W2-2	36 x 36	-	9	-	1	2	28
1307+82	SOUTH	22 LT	VIN FIZ HWY (PLAQUE)	W16-8P	24 x 30	5	-	-	1	1	14
1317+75	SOUTH	22 LT	TRUCK	W11-10	36 x 36	-	9	-	1	1	14
1325+85	SOUTH	25 RT	NO PASSING ZONE	W14-3	64 x 48	-	10.7	-	1	2	28
1325+85	NORTH	25 LT	NO PASSING ZONE	W14-3	64 x 48	-	10.7	-	1	2	28
1327+24	NORTH	24 RT	INTERSECTION WARNING	W2-7R	36 x 36	-	9	-	1	1	14
SUB-TOTAL TABLE 1						89.6	186.1	6	19	32	448

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
et:\pwork\pwork\sparksgw\0216263\067243-sht-schedule.dgn		DRAWN -	REVISED -
	PLOT SCALE = 40.0000 ' / in.	CHECKED -	REVISED -
	PLOT DATE = Feb-07-2012 02:52:26PM	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SCHEDULE OF QUANTITIES**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	4RS-3, BR-1	PIKE	41	11
CONTRACT NO. 72243				
ILLINOIS FED. AID PROJECT				

SIGN REPLACEMENT SCHEDULE											
STATION	FACING	OFFSET	SIGN	SIGN DESIGNATION	SIGN SIZE (in.)	Sign Panel Ty 1 Spl (Sq. Ft.)	Sign Panel Ty 2 Spl (Sq. Ft.)	Rem Sign Panel Assembly Ty A Spl (Each)	Rem Sign Panel Assembly Ty B Spl (Each)	Number of Posts (Each)	Telescoping Stl Sign Support (Foot)
1333+18	EAST/WEST	38 RT	ILL 96	D3-1	30 x 24	5	-	1	-	1	14
1333+18	NORTH/SOUTH	38 RT	120TH AVE	D3-1	18 x 30	3.8	-	-	-	-	-
1334+80	NORTH	22 RT	BAY CREEK	I-3	18 x 24	3	-	1	-	1	14
1340+38	SOUTH	22 LT	BAY CREEK	I-3	18 x 24	3	-	1	-	1	14
1341+40	EAST/WEST	38 LT	ILL 96	D3-1	30 x 24	5	-	1	-	1	14
1341+40	NORTH/SOUTH	38 LT	120TH AVE	D3-1	18 x 30	3.8	-	-	-	-	-
1341+45	EAST	33 LT	STOP	R1-1	36 x 36	-	9	-	1	1	14
1341+45	WEST	33 LT	STOP	R1-1	36 x 36	-	9	-	1	1	14
1347+25	SOUTH	25 LT	INTERSECTION WARNING	W2-2	36 x 36	-	9	-	1	1	14
1347+70	SOUTH	25 RT	NO PASSING ZONE	W14-3	64 x 48	-	10.7	-	1	1	14
1363+75	NORTH	25 LT	96 PIKE 2.00 CRM	DISTRICT SIGN	18 x 18	2.3	-	1	-	1	14
1363+75	SOUTH	25 RT	96 PIKE 2.00 CRM	DISTRICT SIGN	18 x 18	2.3	-	-	-	-	-
1373+95	NORTH	25 RT	INTERSECTION WARNING	W2-2	36 x 36	-	9	-	1	1	14
1374+20	NORTH	30 LT	NO PASSING ZONE	W14-3	64 x 48	-	10.7	-	1	1	14
1379+25	EAST/WEST	30 LT	ILL 96	D3-1	30 x 24	5	-	1	-	1	14
1379+25	NORTH/SOUTH	30 LT	115TH AVE	D3-1	18 x 30	3.8	-	-	-	-	-
1379+30	EAST	22 LT	STOP	R1-1	36 x 36	-	9	-	1	1	14
1379+30	WEST	22 LT	STOP	R1-1	36 x 36	-	9	-	1	1	14
1385+32	SOUTH	25 LT	INTERSECTION WARNING	W2-2	36 x 36	-	9	-	1	1	14
1391+05	SOUTH	25 RT	NO PASSING ZONE	W14-3	64 x 48	-	10.7	-	1	2	28
1405+72	NORTH	22 RT	INTERSECTION WARNING	W2-2	36 x 36	-	9	-	1	1	14
1408+65	NORTH	30 LT	NO PASSING ZONE	W14-3	64 x 48	-	10.7	-	1	2	28
1410+60	EAST	33 RT	STOP	R1-1	36 x 36	-	9	-	1	1	14
1410+60	WEST	33 RT	STOP	R1-1	36 x 36	-	9	-	1	1	14
1410+65	EAST/WEST	36 RT	ILL 96	D3-1	30 x 24	5	-	1	-	1	14
1410+65	NORTH/SOUTH	36 RT	110TH AVE	DO NOT REPLACE	18 x 30	3.8	-	-	-	-	-
1415+65	SOUTH	22 LT	INTERSECTION WARNING	W2-2	36 x 36	-	9	-	1	1	14
1420+05	NORTH	30 LT	96 PIKE 1.00 CRM	DISTRICT SIGN	18 x 18	2.3	-	1	-	1	14
1420+05	SOUTH	30 LT	96 PIKE 1.00 CRM	DISTRICT SIGN	18 x 18	2.3	-	-	-	-	-
1425+95	SOUTH	22 RT	NO PASSING ZONE	W14-3	64 x 48	-	10.7	-	1	2	28
1442+00	NORTH	22 LT	NO PASSING ZONE	W14-3	64 x 48	-	10.7	-	1	2	28
1457+15	NORTH	28 LT	NO PASSING ZONE	W14-3	64 x 48	-	10.7	-	1	2	28
1457+35	SOUTH	30 RT	NO PASSING ZONE	W14-3	64 x 48	-	10.7	-	1	2	28
1473+00	NORTH	22 LT	NO PASSING ZONE	W14-3	64 x 48	-	10.7	-	1	2	28
1473+25	NORTH	28 RT	CALHOUN COUNTY	I-2	18 x 30	3.8	-	-	-	-	-
1473+25	NORTH	28 RT	ILLINOIS 96	DISTRICT SIGN	30 x 24	5	-	-	1	2	28
1473+25	NORTH	28 RT	SOUTH	M3-3	30 x 15	3.1	-	-	-	-	-
1473+25	NORTH	22 LT	CALHOUN COUNTY	I-2	30 x 24	5	-	-	-	-	-
1473+25	SOUTH	22 LT	PIKE COUNTY	I-2	30 x 24	5	-	-	-	-	-
1473+25	NORTH	22 LT	96 PIKE 0.00 CRM	DISTRICT SIGN	18 x 18	2.3	-	-	1	2	28
1473+25	SOUTH	22 LT	96 PIKE 0.00 CRM	DISTRICT SIGN	18 x 18	2.3	-	-	-	-	-
1473+25	SOUTH	22 LT	ILLINOIS 96	DISTRICT SIGN	30 x 24	5	-	-	-	-	-
1473+25	SOUTH	22 LT	NORTH	M3-1	30 x 15	3.1	-	-	-	-	-
ALLOWANCE FOR CUTTING LOSS											100
SUB-TOTAL TABLE 2						85	195.3	8	19	36	504
SUB-TOTAL TABLE 1						89.6	186.1	6	19	32	448
TOTAL TABLES 1 & 2						174.6	381.4	14	38	68	1,052

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
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		CHECKED -	REVISED -
		DATE -	REVISED -

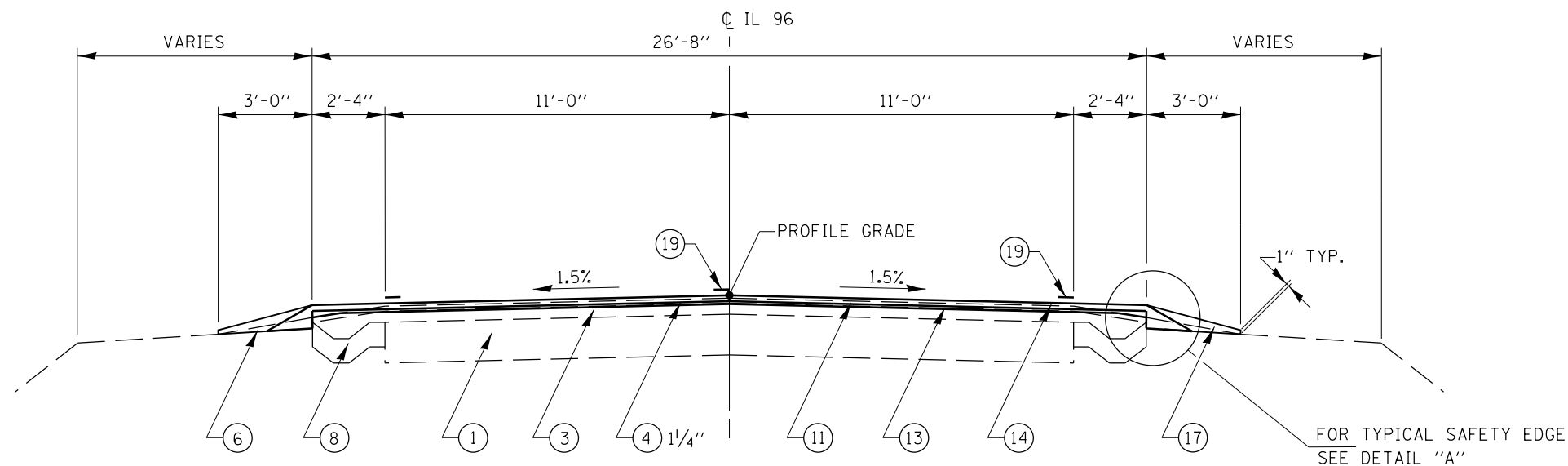
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	4RS-3, BR-1	PIKE	41	12
CONTRACT NO. 72243				
ILLINOIS FED. AID PROJECT				

**LEGEND**

- ① EXIST. CRUSHED STONE BASE WITH SURFACE TREATMENT ±10"
- ② EXIST. HMA SURFACE VARIABLE DEPTH
- ③ EXIST. LEVELING BINDER VARIABLE DEPTH
- ④ EXIST. HMA SURFACE COURSE 1 1/4" OR 1 1/2"
- ⑤ EXIST. HMA BASE COURSE WIDENING 9"
- ⑥ EXIST. AGGREGATE SHOULDERS
- ⑦ EXIST. COMBINATION CONCRETE CURB & GUTTER TYPE B-9.18
- ⑧ EXIST. CONCRETE GUTTER, TYPE C
- ⑨ EXIST. TYPE A GUTTER MODIFIED
- ⑩ EXIST. HMA SHOULDERS
- ⑪ PROP. HMA SURFACE REMOVAL (VARIABLE DEPTH)
- ⑫ PROP. HMA SURFACE REMOVAL 1 1/2"
- ⑬ PROP. LEVELING BINDER, MACHINE METHOD N50 (3/4")
- ⑭ PROP. HMA SURFACE COURSE, MIX "C" N50 (1 1/2")
- ⑮ PROP. SAFETY SHOULDER 1'-0" (HMA SHOULDER 6 1/2")
- ⑯ PROP. EXCAVATING & GRADING EXISTING SHOULDERS
- ⑰ PROP. AGGREGATE SHOULDERS, TYPE B
- ⑱ PROP. SUB-BASE GRANULAR MATERIAL, TYPE B (VARIABLE DEPTH)
- ⑲ PROP. PAINT PAVEMENT MARKING 5"



**TYPICAL SECTION #2**

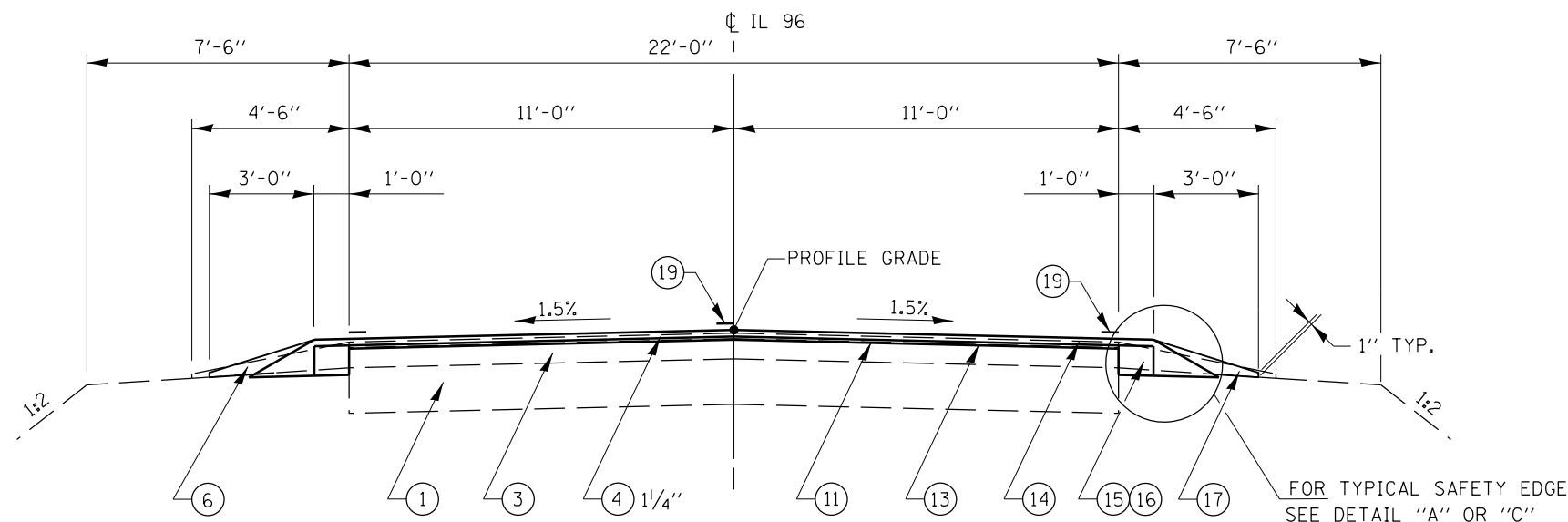
STA. 1304+76.00 TO STA. 1306+99.00

**CONSTRUCTION SEQUENCE  
FOR HMA SHOULDERS 6 1/2"**

1. MILL EXISTING PAVEMENT.
2. PLACE LEVELING BINDER ON MILLED PAVEMENT TO REDUCE MILLED SURFACE EXPOSURE.
3. CONSTRUCT 6 1/2" HMA SHOULDERS.
4. FINISH WITH HMA SURFACE COURSE (1 1/2").

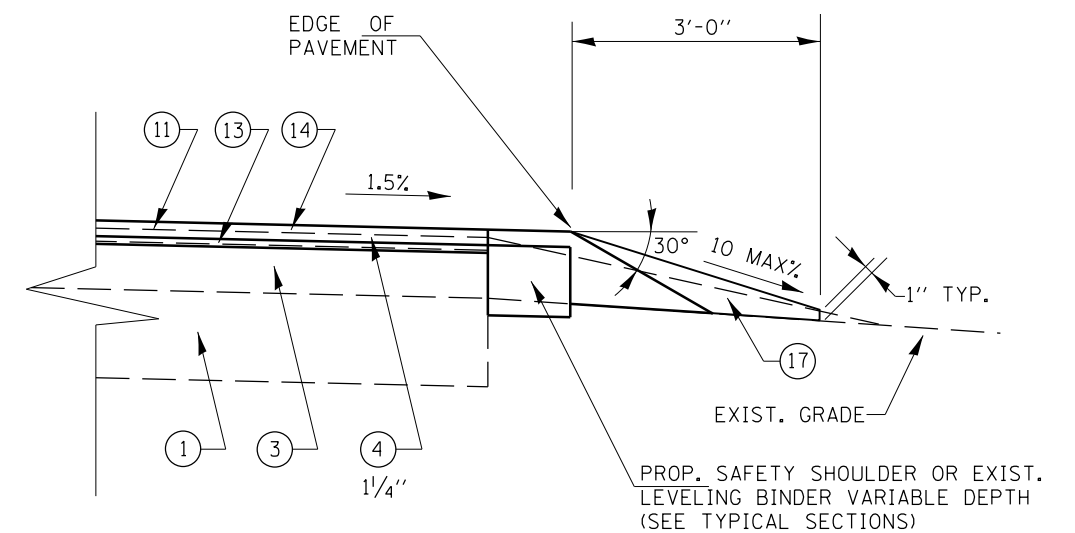
**NOTE**

1. IF SUPERELEVATION EXIST THE SHOULDER SLOPES ON THE HIGH SIDE MAXIMUM BREAK - OVER SHOULD BE NO GREATER THAN 8% AND ON THE LOW SIDE SHOULDER SLOPE SHALL BE 4% OR THE SAME AS S.E. IF S.E. IS OVER 4%



**TYPICAL SECTION #1**

STA. 1273+10.50 TO STA. 1304+76.00  
 RT. STA. 1348+00.00 TO STA. 1375+09.00  
 LT. STA. 1348+00.00 TO STA. 1375+89.00  
 STA. 1381+91.00 TO STA. 1401+61.10  
 STATION EQUATION STA. 1401+61.10 BK. = STA. 1404+93.40 AH.  
 STA. 1404+93.40 TO STA. 1472+32.00



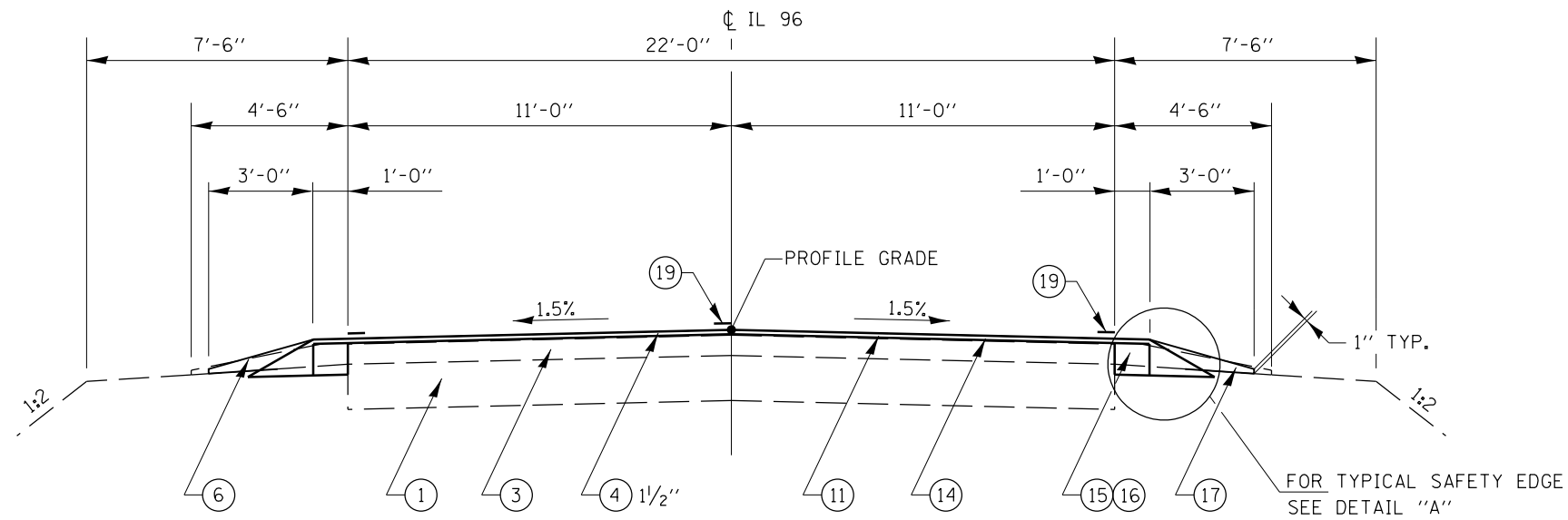
**SAFETY EDGE NOTES:**

1. THE CROSS SECTION AREA FOR THE SAFETY EDGE IS ESTIMATED AT 0.20 SQ. FT..
2. QUANTITY FOR SAFETY EDGE HAS BEEN INCLUDED IN THE QUANTITIES FOR HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50.

**DETAIL "A"**

PROPOSED SAFETY EDGE

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL SECTIONS</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\idot\sparksgw\0216263\067243-sht-typical.dgn	DRAWN -	REVISED -	304					4RS-3, BR-1	PIKE	41	13	
PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -	CONTRACT NO. 72243									
PLOT DATE = Feb-07-2012 02:52:43PM	DATE -	REVISED -	ILLINOIS FED. AID PROJECT									
				SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.				



**TYPICAL SECTION #4**

STA. 1324+25.00 TO STA. 1328+00.00

**CONSTRUCTION SEQUENCE  
FOR HMA SHOULDERS 6 1/2"**

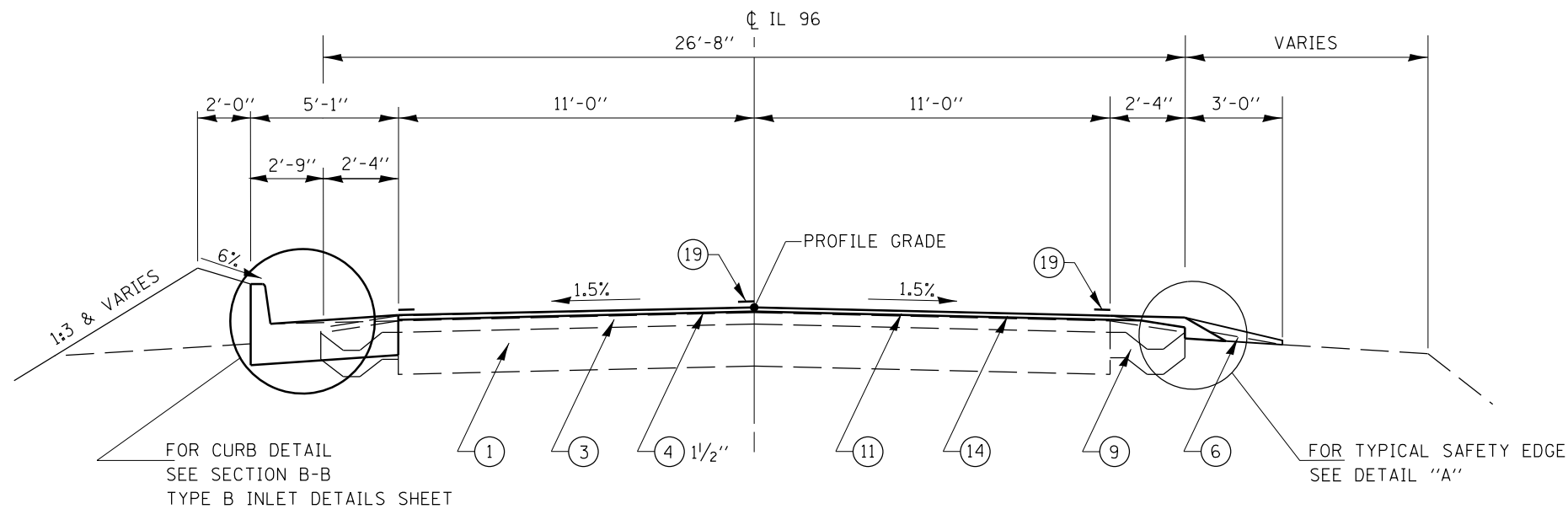
1. MILL EXISTING PAVEMENT.
2. PLACE LEVELING BINDER ON MILLED PAVEMENT TO REDUCE MILLED SURFACE EXPOSURE.
3. CONSTRUCT 6 1/2" HMA SHOULDERS.
4. FINISH WITH HMA SURFACE COURSE (1 1/2").

**LEGEND**

- ① EXIST. CRUSHED STONE BASE WITH SURFACE TREATMENT ±10"
- ② EXIST. HMA SURFACE VARIABLE DEPTH
- ③ EXIST. LEVELING BINDER VARIABLE DEPTH
- ④ EXIST. HMA SURFACE COURSE 1 1/4" OR 1 1/2"
- ⑤ EXIST. HMA BASE COURSE WIDENING 9"
- ⑥ EXIST. AGGREGATE SHOULDERS
- ⑦ EXIST. COMBINATION CONCRETE CURB & GUTTER TYPE B-9.18
- ⑧ EXIST. CONCRETE GUTTER, TYPE C
- ⑨ EXIST. TYPE A GUTTER MODIFIED
- ⑩ EXIST. HMA SHOULDERS
- ⑪ PROP. HMA SURFACE REMOVAL (VARIABLE DEPTH)
- ⑫ PROP. HMA SURFACE REMOVAL 1 1/2"
- ⑬ PROP. LEVELING BINDER, MACHINE METHOD N50 (3/4")
- ⑭ PROP. HMA SURFACE COURSE, MIX "C" N50 (1 1/2")
- ⑮ PROP. SAFETY SHOULDER 1'-0" (HMA SHOULDER 6 1/2")
- ⑯ PROP. EXCAVATING & GRADING EXISTING SHOULDERS
- ⑰ PROP. AGGREGATE SHOULDERS, TYPE B
- ⑱ PROP. SUB-BASE GRANULAR MATERIAL, TYPE B (VARIABLE DEPTH)
- ⑲ PROP. PAINT PAVEMENT MARKING 5"

**NOTE**

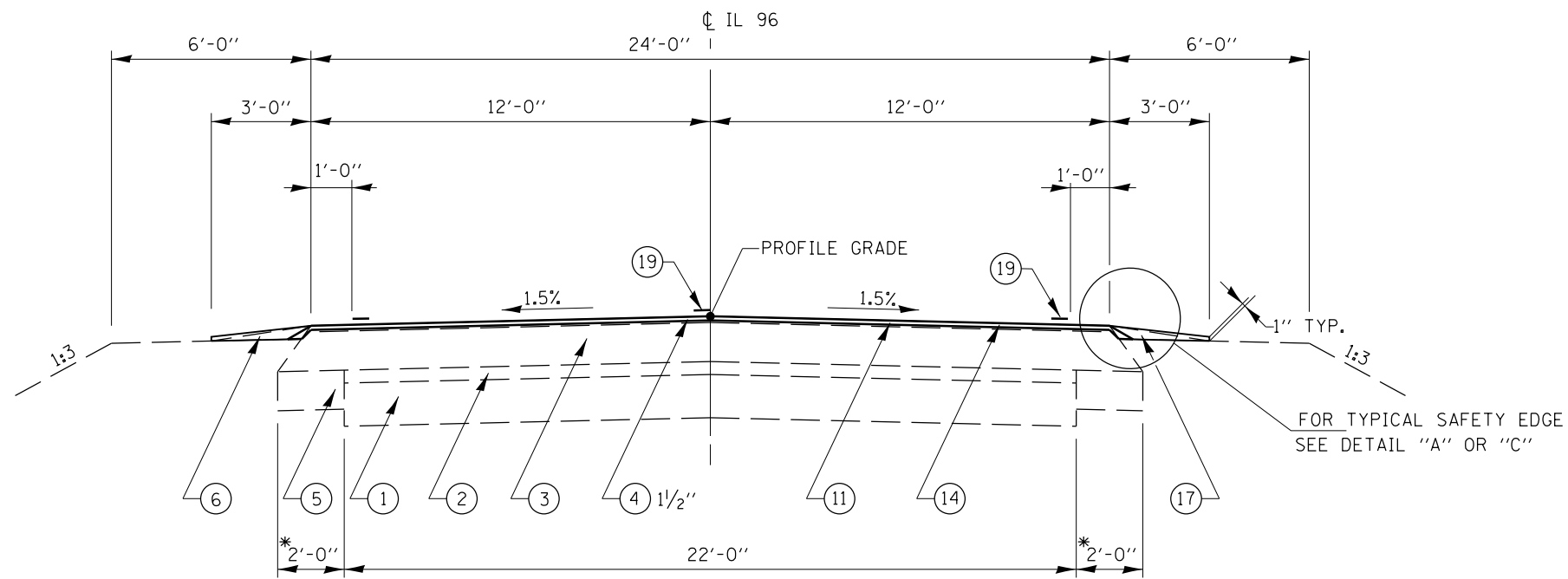
1. IF SUPERELEVATION EXIST THE SHOULDER SLOPES ON THE HIGH SIDE MAXIMUM BREAK - OVER SHOULD BE NO GREATER THAN 8% AND ON THE LOW SIDE SHOULDER SLOPE SHALL BE 4% OR THE SAME AS S.E. IF S.E. IS OVER 4%.



**TYPICAL SECTION #3**

STA. 1306+99.00 TO STA. 1307+45.00  
STA. 1316+55.00 TO STA. 1324+25.00

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL SECTIONS</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pw\work\p\id\dot\sparksgw\10216263\067243-sht-typical.dgn		DRAWN -	REVISED -		304	4RS-3, BR-1	PIKE	41	14				
PLOT SCALE = 40.0000' / in.		CHECKED -	REVISED -		CONTRACT NO. 72243								
PLOT DATE = Feb-08-2012 08:22:04AM		DATE -	REVISED -		ILLINOIS FED. AID PROJECT								
					SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.				



**TYPICAL SECTION #6**

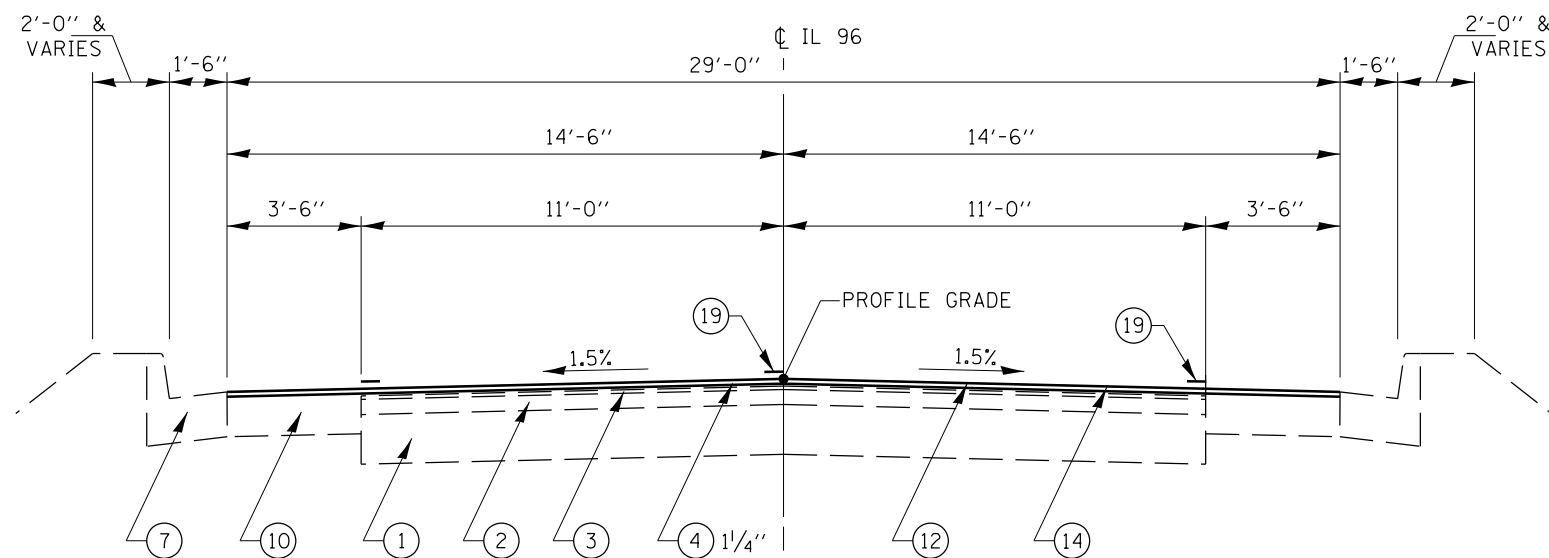
STA. 1328+00.00 TO STA. 1334+95.16  
 BRIDGE OMISSION STA. 1334+95.16 TO STA. 1340+29.50  
 STA. 1340+29.50 TO STA. 1348+00.00

**\* NOTE :**

WIDTH OF EXISTING BASE COURSE WIDENING, 9'  
 3'-0" FROM LT. & RT. STA. 1342+50.00 TO  
 STA. 1345+00.00

**LEGEND**

- ① EXIST. CRUSHED STONE BASE WITH SURFACE TREATMENT ±10"
- ② EXIST. HMA SURFACE VARIABLE DEPTH
- ③ EXIST. LEVELING BINDER VARIABLE DEPTH
- ④ EXIST. HMA SURFACE COURSE 1 1/4" OR 1 1/2"
- ⑤ EXIST. HMA BASE COURSE WIDENING 9"
- ⑥ EXIST. AGGREGATE SHOULDERS
- ⑦ EXIST. COMBINATION CONCRETE CURB & GUTTER TYPE B-9.18
- ⑧ EXIST. CONCRETE GUTTER, TYPE C
- ⑨ EXIST. TYPE A GUTTER MODIFIED
- ⑩ EXIST. HMA SHOULDERS
- ⑪ PROP. HMA SURFACE REMOVAL (VARIABLE DEPTH)
- ⑫ PROP. HMA SURFACE REMOVAL 1 1/2"
- ⑬ PROP. LEVELING BINDER, MACHINE METHOD N50 (3/4")
- ⑭ PROP. HMA SURFACE COURSE, MIX "C" N50 (1 1/2")
- ⑮ PROP. SAFETY SHOULDER 1'-0" (HMA SHOULDER 6 1/2")
- ⑯ PROP. EXCAVATING & GRADING EXISTING SHOULDERS
- ⑰ PROP. AGGREGATE SHOULDERS, TYPE B
- ⑱ PROP. SUB-BASE GRANULAR MATERIAL, TYPE B (VARIABLE DEPTH)
- ⑲ PROP. PAINT PAVEMENT MARKING 5"



**TYPICAL SECTION #5**

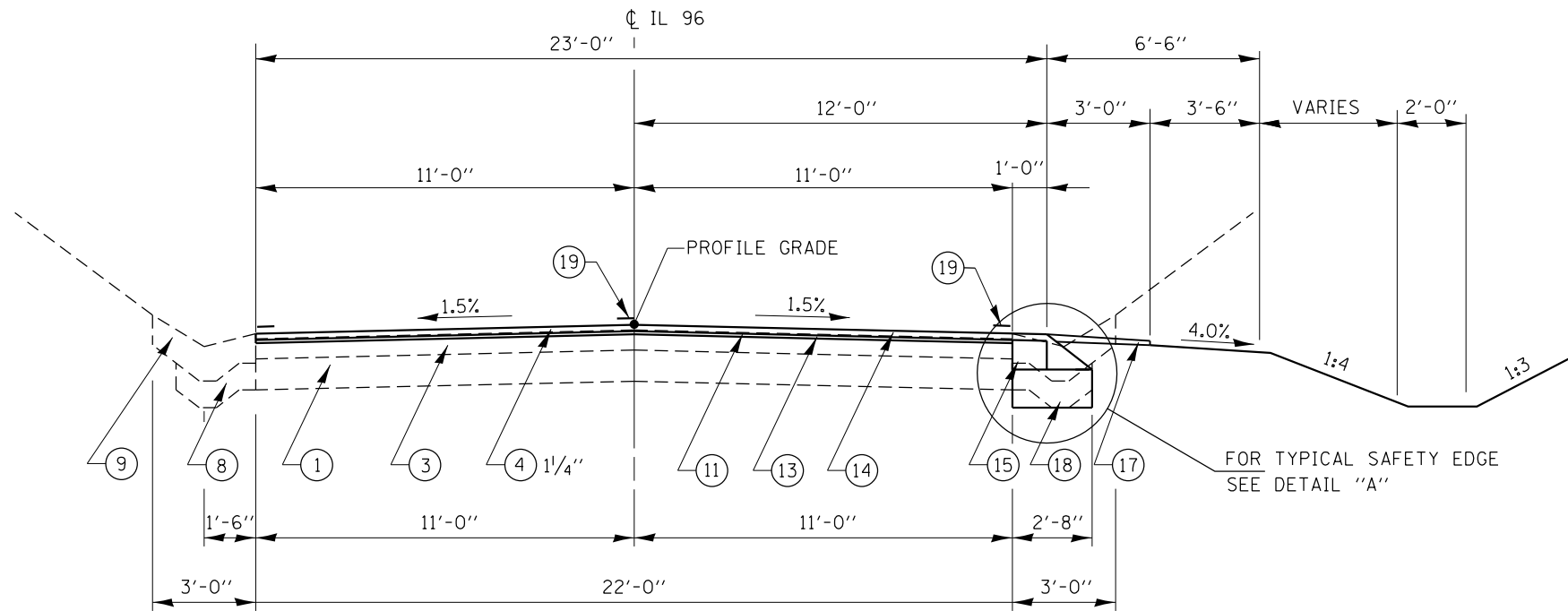
STA. 1307+45.00 TO STA. 1310+28.36  
 BRIDGE OMISSION STA. 1310+28.36 TO STA. 1313+82.83  
 STA. 1313+82.83 TO STA. 1316+55.00

**NOTE**

- 1. IF SUPERELEVATION EXIST THE SHOULDER SLOPES ON THE HIGH SIDE MAXIMUM BREAK - OVER SHOULD BE NO GREATER THAN 8% AND ON THE LOW SIDE SHOULDER SLOPE SHALL BE 4% OR THE SAME AS S.E. IF S.E. IS OVER 4%

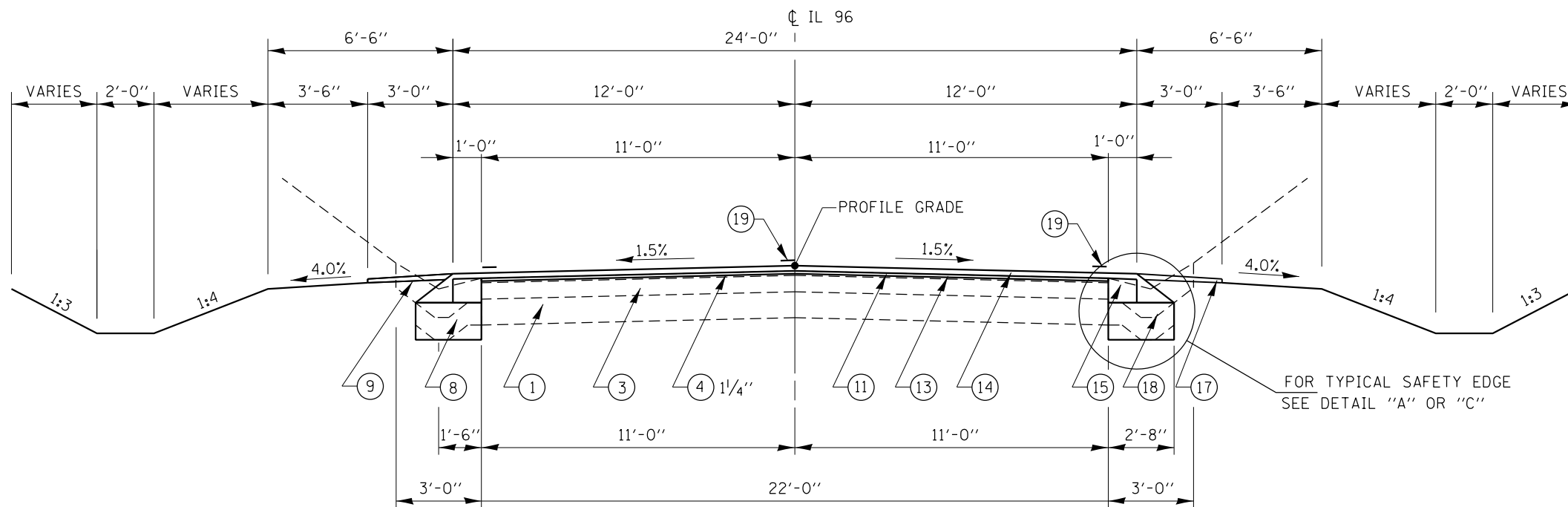
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et:\pw\work\p\id\sparksgw\0216263\067243-sht-typical.dgn		DRAWN -	REVISED -					304	4RS-3, BR-1	PIKE	41	15
PLOT SCALE = 40.0000' / in.		CHECKED -	REVISED -		CONTRACT NO. 72243							
PLOT DATE = Feb-07-2012 02:52:43PM		DATE -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		





**TYPICAL SECTION #8**

STA. 1379+00.00 TO STA. 1381+91.00

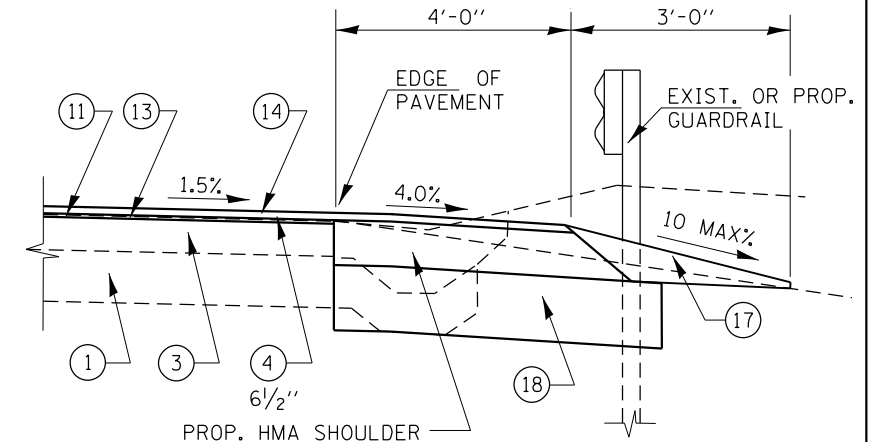


**TYPICAL SECTION #7**

RT. STA. 1375+09.00 TO RT. STA. 1379+00.00  
LT. STA. 1375+89.00 TO LT. STA. 1379+00.00

**LEGEND**

- ① EXIST. CRUSHED STONE BASE WITH SURFACE TREATMENT ±10"
- ② EXIST. HMA SURFACE VARIABLE DEPTH
- ③ EXIST. LEVELING BINDER VARIABLE DEPTH
- ④ EXIST. HMA SURFACE COURSE 1/4" OR 1/2"
- ⑤ EXIST. HMA BASE COURSE WIDENING 9"
- ⑥ EXIST. AGGREGATE SHOULDERS
- ⑦ EXIST. COMBINATION CONCRETE CURB & GUTTER TYPE B-9.18
- ⑧ EXIST. CONCRETE GUTTER, TYPE C
- ⑨ EXIST. TYPE A GUTTER MODIFIED
- ⑩ EXIST. HMA SHOULDERS
- ⑪ PROP. HMA SURFACE REMOVAL (VARIABLE DEPTH)
- ⑫ PROP. HMA SURFACE REMOVAL 1/2"
- ⑬ PROP. LEVELING BINDER, MACHINE METHOD N50 (3/4")
- ⑭ PROP. HMA SURFACE COURSE, MIX "C" N50 (1/2")
- ⑮ PROP. SAFETY SHOULDER 1'-0" (HMA SHOULDER 6 1/2")
- ⑯ PROP. EXCAVATING & GRADING EXISTING SHOULDERS
- ⑰ PROP. AGGREGATE SHOULDERS, TYPE B
- ⑱ PROP. SUB-BASE GRANULAR MATERIAL, TYPE B (VARIABLE DEPTH)
- ⑲ PROP. PAINT PAVEMENT MARKING 5"



SEE TYPICAL SECTION #1  
RT. STA. 1371+80.00 TO RT. STA. 1375+09.00  
LT. STA. 1373+48.00 TO LT. STA. 1375+89.00  
RT. STA. 1422+53.00 TO RT. STA. 1425+90.00  
LT. STA. 1423+55.00 TO LT. STA. 1425+21.00

SEE TYPICAL SECTION #6  
RT. STA. 1333+26.00 TO RT. STA. 1334+95.00  
LT. STA. 1333+36.00 TO RT. STA. 1334+95.00  
RT. STA. 1340+29.00 TO RT. STA. 1340+70.00  
LT. STA. 1340+29.00 TO LT. STA. 1341+31.00

SEE TYPICAL SECTION #7  
(LIMITS OF PROP. SUB-BASE GRANULAR MATERIAL, TYPE B)  
RT. STA. 1375+09.00 TO RT. STA. 1376+30.00  
LT. STA. 1375+89.00 TO LT. STA. 1377+98.00

**DETAIL "C"**

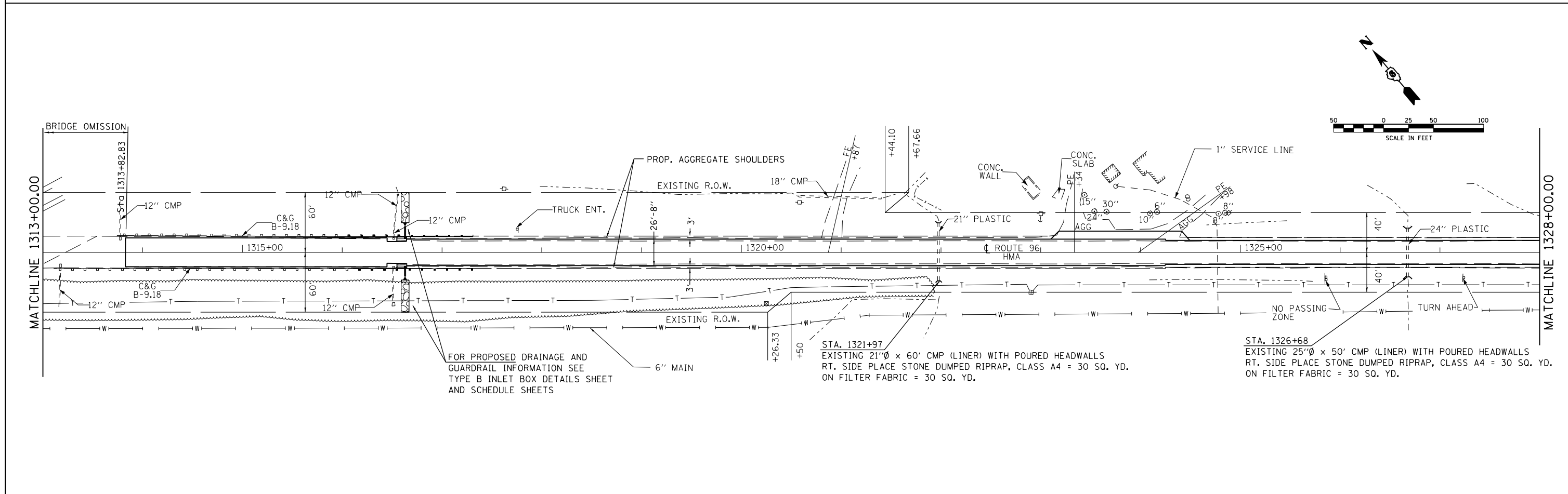
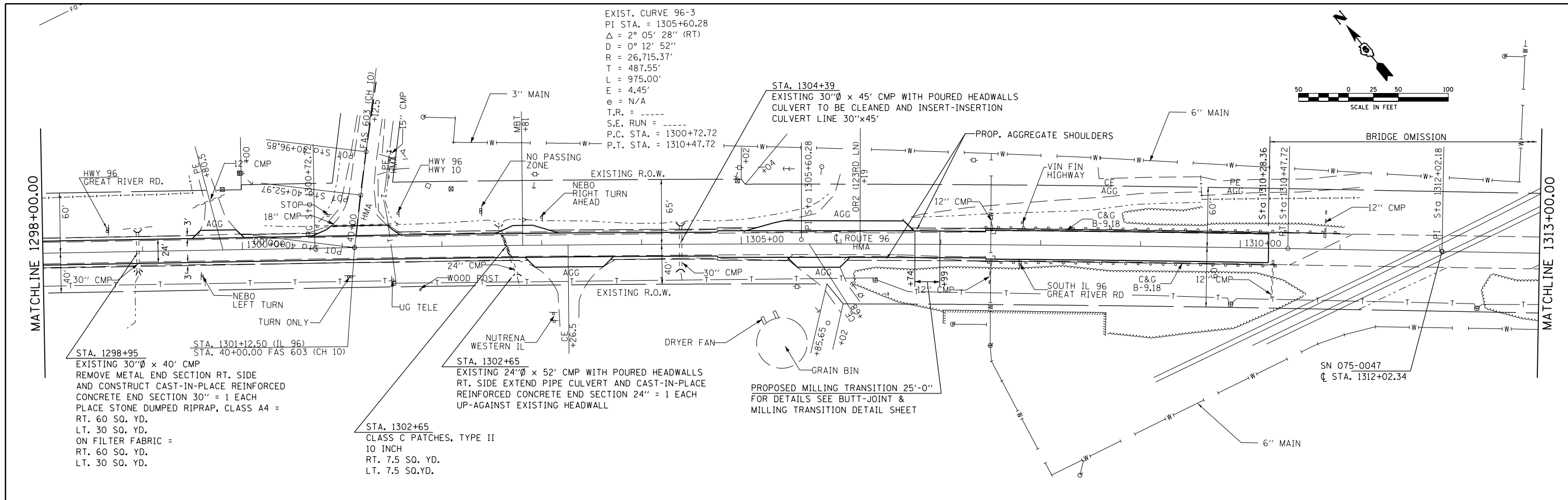
PROPOSED 4'-0" HMA SHOULDER

**NOTE**

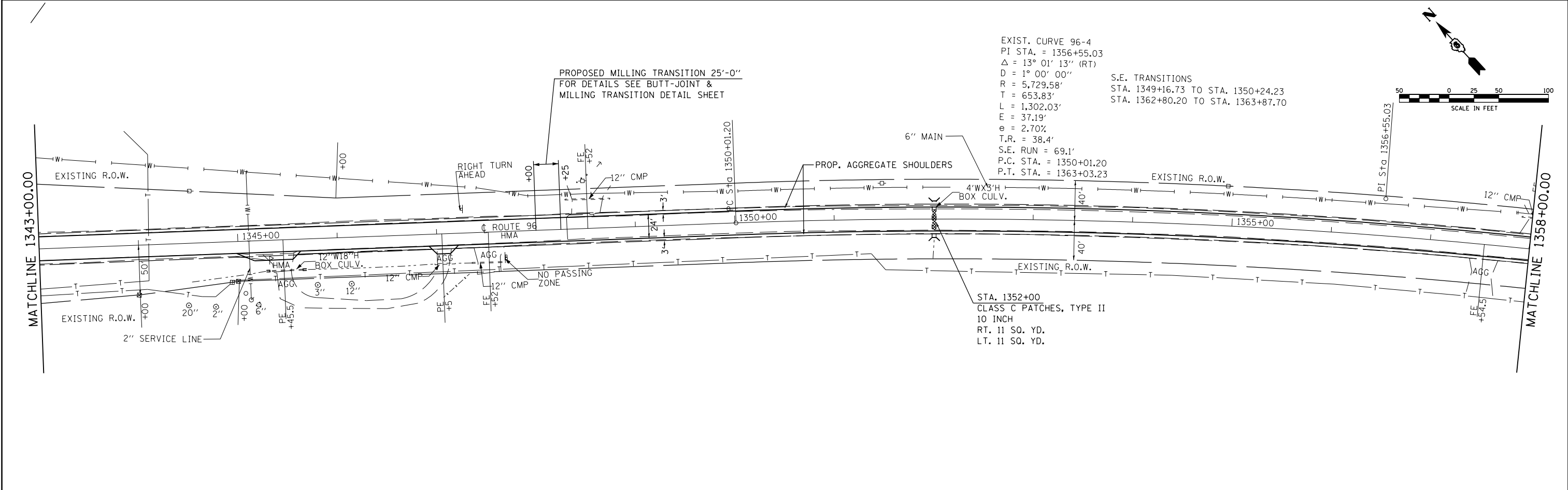
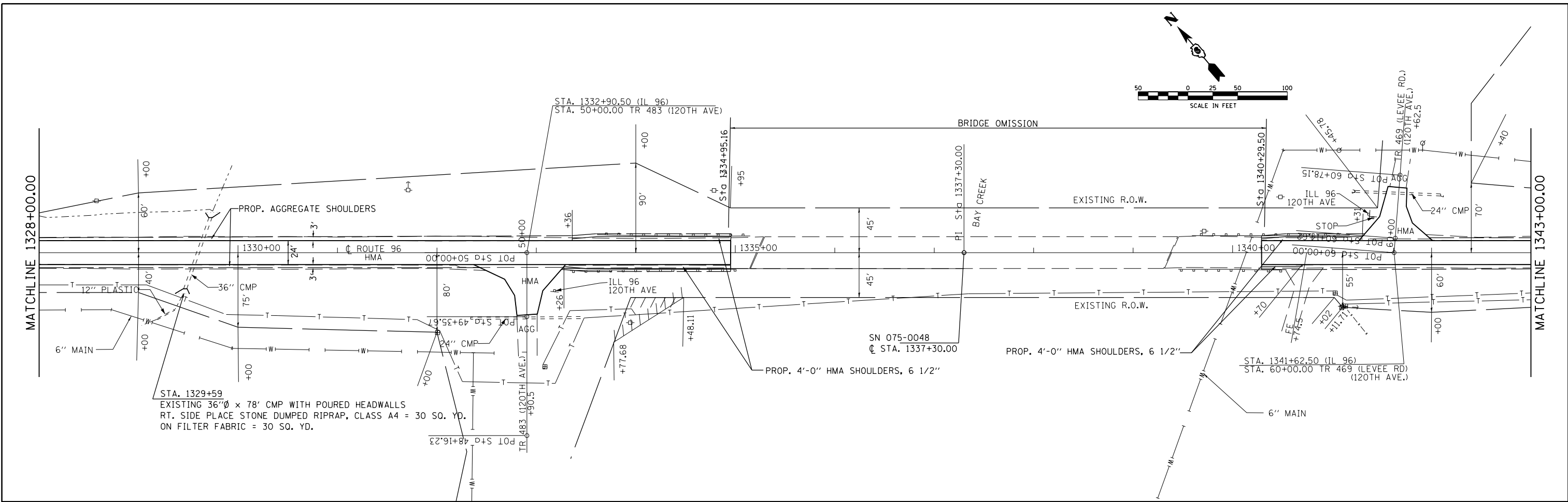
1. IF SUPERELEVATION EXIST THE SHOULDER SLOPES ON THE HIGH SIDE MAXIMUM BREAK - OVER SHOULD BE NO GREATER THAN 8% AND ON THE LOW SIDE SHOULDER SLOPE SHALL BE 4% OR THE SAME AS S.E. IF S.E. IS OVER 4%
2. EXISTING CONCRETE GUTTER, TYPE C & EXISTING TYPE A GUTTER MODIFIED TO BE REMOVED RT. STA. 1375+09.00 TO RT. STA. 1381+91.00 & LT. STA. 1375+89.00 TO LT. STA. 1379+00.00. QUANTITIES FOR THE REMOVAL OF BOTH GUTTERS HAVE BEEN INCLUDED IN THE GUTTER REMOVAL SCHEDULE AT THE CONTRACT UNIT PRICE PER FOOT.

FILE NAME =	USER NAME = martnjk	DESIGNED -	REVISED 02/16/2012	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL SECTIONS</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT DATE = Feb-16-2012 01:06:41PM	DATE -	REVISED -	ILLINOIS FED. AID PROJECT									

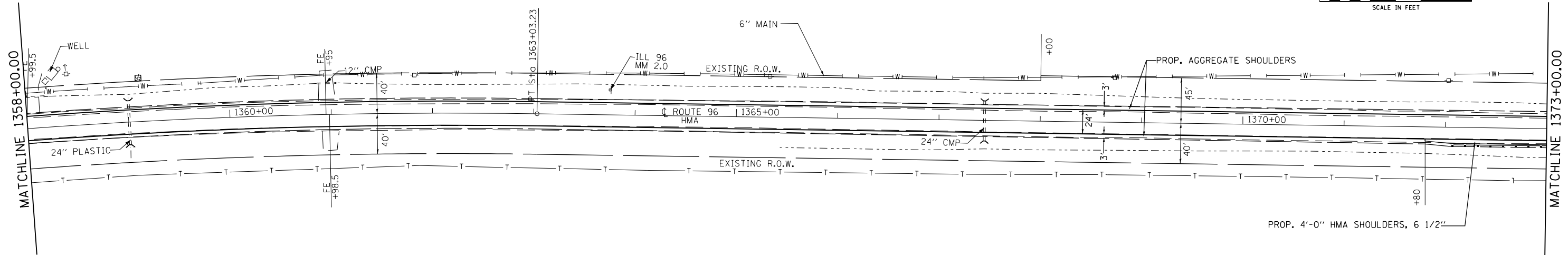
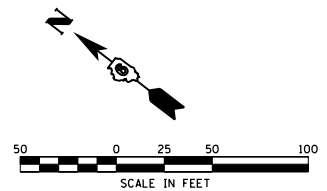




FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEET</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ei:\pw\work\p\idot\sparksgw\10216263\067243-sht.pln.dgn		DRAWN -	REVISED -			304	4RS-3, BR-1	PIKE	41	18	
		CHECKED -	REVISED -			CONTRACT NO. 72243					
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
PLOT SCALE = 100.0000' / in.				SCALE:	SHEET NO. 2 OF 7 SHEETS	STA. 1298+00.00 TO STA. 1328+00.00					



FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEET</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\pwork\dot\sparksgw\0216263\0672243-sht.pln.dgn	DRAWN -	REVISED -	304			4RS-3, BR-1	PIKE	41	19	
PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -	CONTRACT NO. 72243							
PLOT DATE = Feb-07-2012 02:52:49PM	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							
SCALE:						SHEET NO. 3 OF 7 SHEETS		STA. 1328+00.00 TO STA. 1358+00.00		



FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
ei:\pw\work\p\idot\sparksgw\0216263\067243-sht.pln.dgn		DRAWN -	REVISED -
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	PLOT DATE = Feb-07-2012 02:52:49PM	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

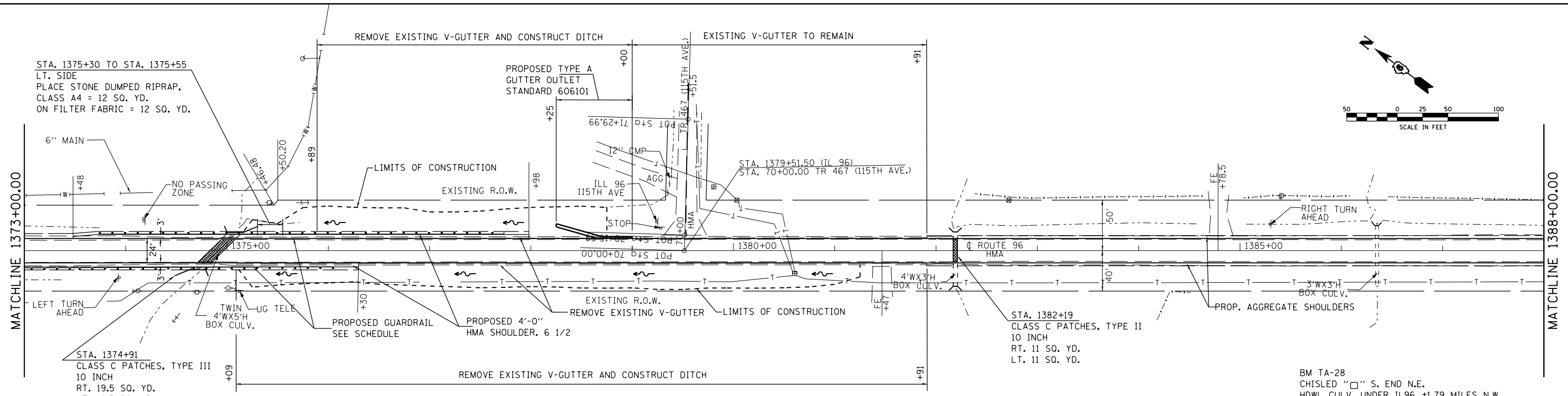
**PLAN SHEET**

SCALE: SHEET NO. 4 OF 7 SHEETS STA. 1358+00.00 TO STA. 1388+00.00

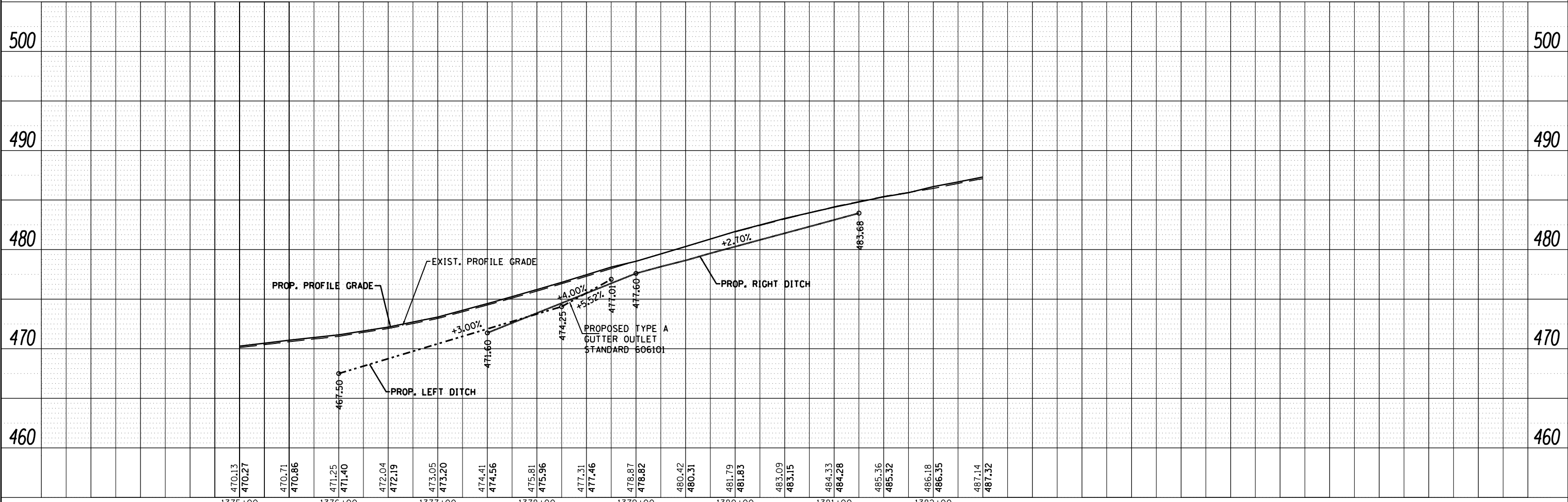
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304	4RS-3, BR-1	PIKE	41	20
<b>CONTRACT NO. 72243</b>				
ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NOTE BOOK NO.	
	FILE NAME	



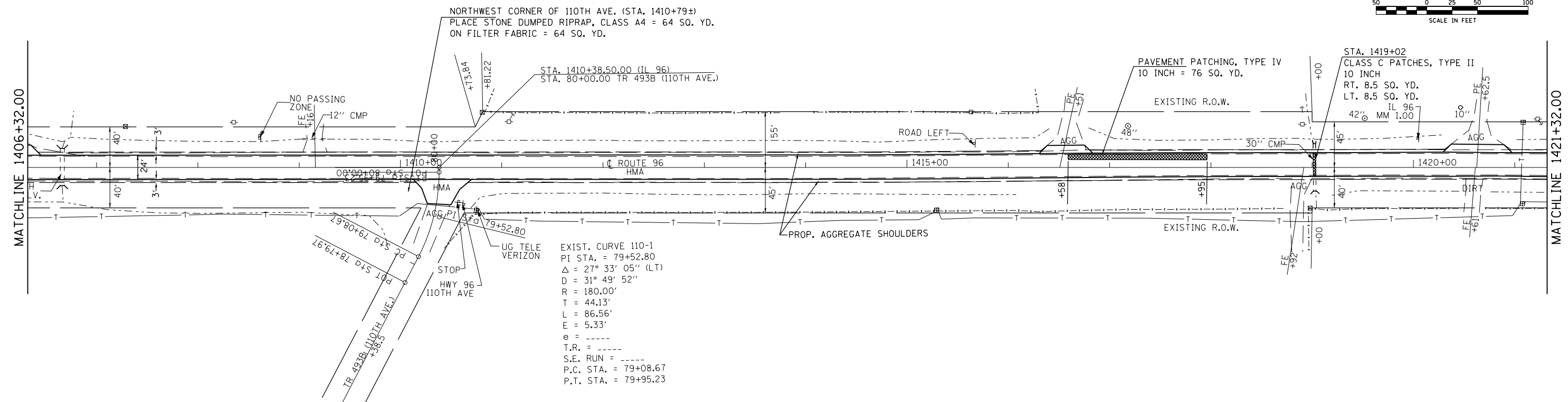
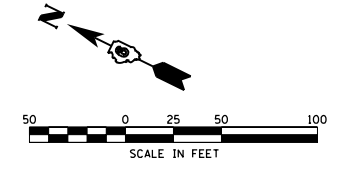
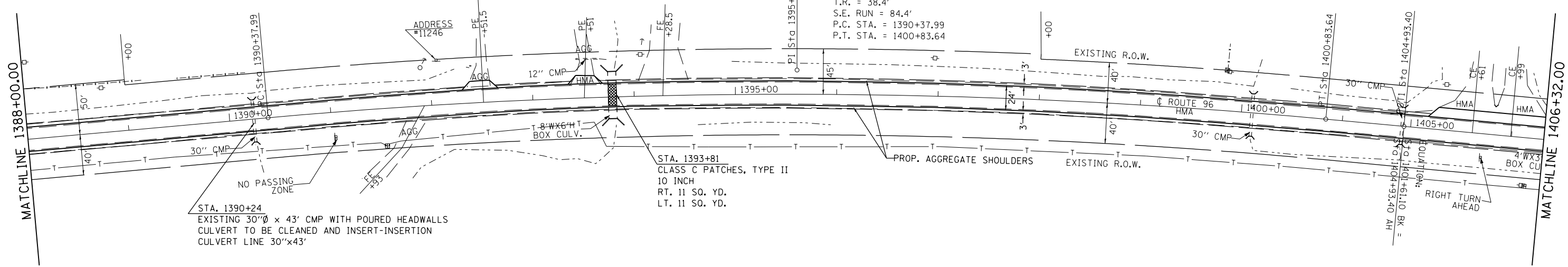
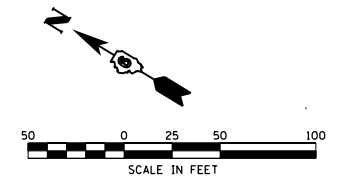
BM TA-28  
CHISLED "□" S. END N.E.  
HDWL CULV. UNDER IL96 ±1.79 MILES N.W.  
FROM PIKE/CALHOUN CO. LINE, 1ST CULV.  
NW OF TR 115TH AVE.  
STA. ±1375+16 L.T.  
ELEV. 469.250 (NAVO 88)



FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN &amp; PROFILE SHEET</b>				F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\pwork\sparksgw\d0216263\0672243-sht-p&p.dgn		DRAWN -	REVISED -						304	4RS-3, BR-1	PIKE	41	21
		CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.				CONTRACT NO. 72243				
		DATE -	REVISED -						ILLINOIS FED. AID PROJECT				

EXIST. CURVE 96-5  
 PI STA. = 1395+62.27  
 $\Delta = 10^\circ 27' 23''$  (RT)  
 $D = 1^\circ 00' 00''$   
 $R = 5,729.58'$   
 $T = 524.28'$   
 $L = 1,045.65'$   
 $E = 23.94'$   
 $e = 3.30\%$   
 $T.R. = 38.4'$   
 $S.E. RUN = 84.4'$   
 $P.C. STA. = 1390+37.99$   
 $P.T. STA. = 1400+83.64$

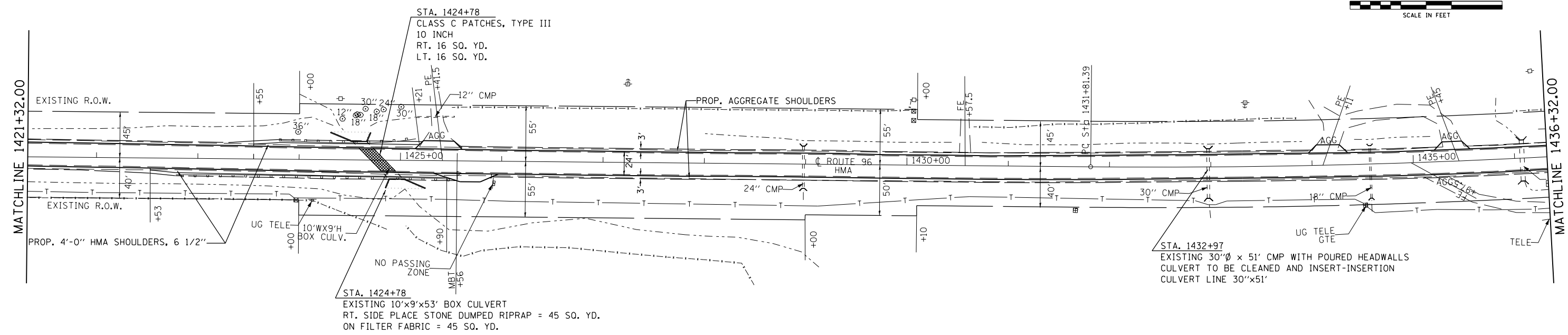
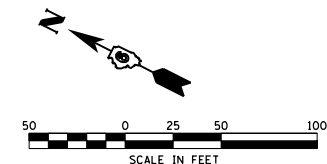
S.E. TRANSITIONS  
 STA. 1389+43.32 TO STA. 1390+55.12  
 STA. 1400+55.51 TO STA. 1401+78.31



EXIST. CURVE 110-1  
 PI STA. = 79+52.80  
 $\Delta = 27^\circ 33' 05''$  (LT)  
 $D = 31^\circ 49' 52''$   
 $R = 180.00'$   
 $T = 44.13'$   
 $L = 86.56'$   
 $E = 5.33'$   
 $e =$   
 $T.R. =$   
 $S.E. RUN =$   
 $P.C. STA. = 79+08.67$   
 $P.T. STA. = 79+95.23$

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEET</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ei:\pwork\pwork\sparksgw\10216263\0672243-sht.pln.dgn		DRAWN -	REVISED -			304	4RS-3, BR-1	PIKE	41	22	
PLOT SCALE = 100.0000' / 1"		CHECKED -	REVISED -			<b>CONTRACT NO. 72243</b>					
PLOT DATE = Feb-07-2012 02:52:58PM		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
					SCALE:	SHEET NO. 5 OF 7 SHEETS		STA. 1388+00.00 TO STA. 1421+32.00			

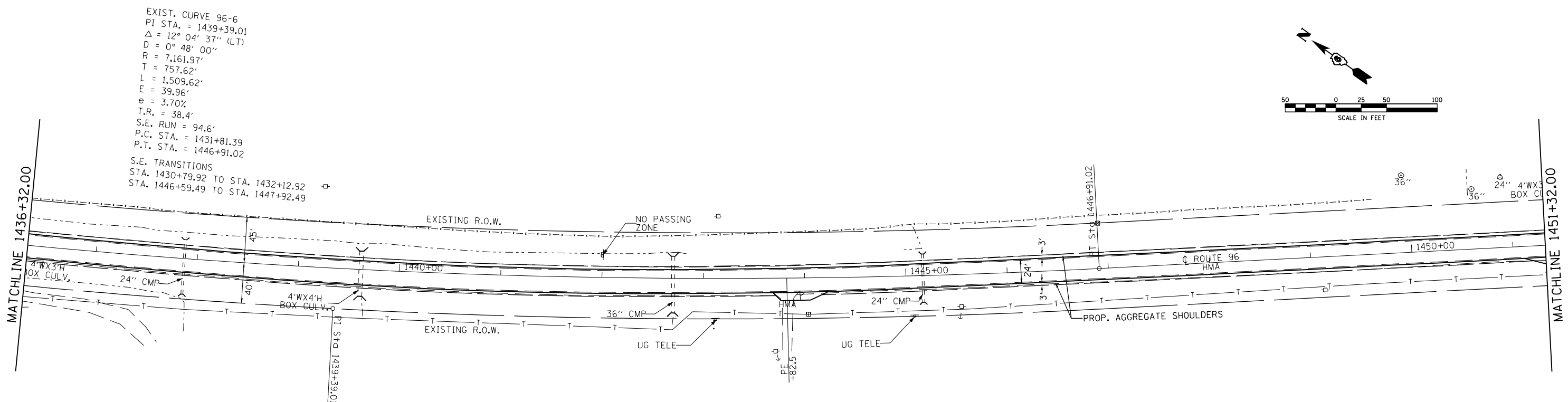
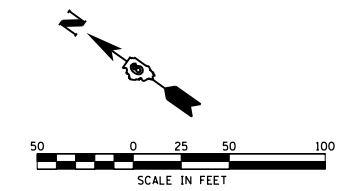




STA. 1424+78  
 CLASS C PATCHES, TYPE III  
 10 INCH  
 RT. 16 SQ. YD.  
 LT. 16 SQ. YD.

STA. 1424+78  
 EXISTING 10'x9'x53' BOX CULVERT  
 RT. SIDE PLACE STONE DUMPED RIPRAP = 45 SQ. YD.  
 ON FILTER FABRIC = 45 SQ. YD.

STA. 1432+97  
 EXISTING 30"Ø x 51' CMP WITH POURED HEADWALLS  
 CULVERT TO BE CLEANED AND INSERT-INSERTION  
 CULVERT LINE 30"x51'



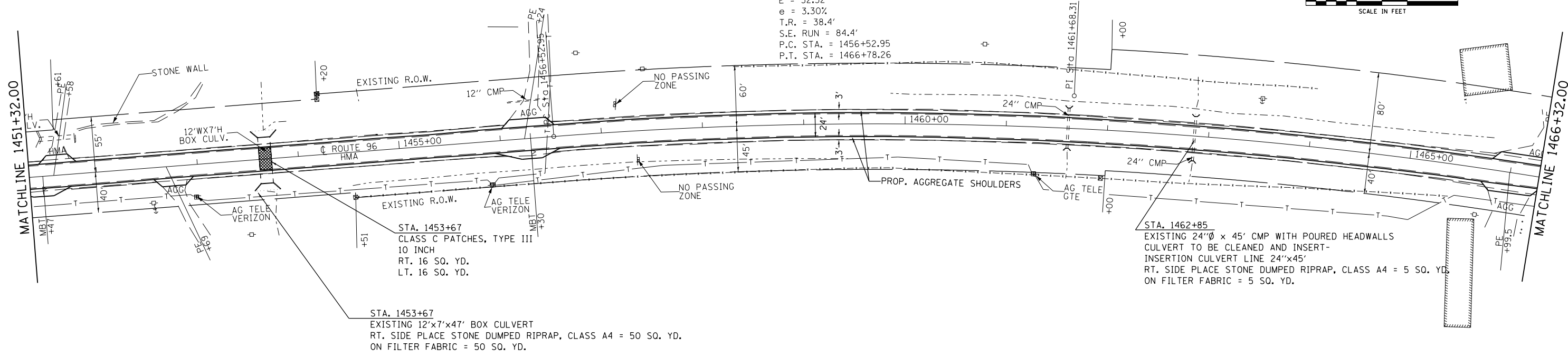
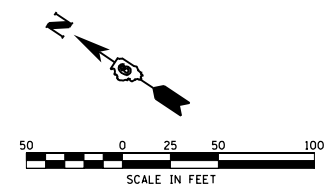
EXIST. CURVE 96-6  
 PI STA. = 1439+39.01  
 $\Delta = 12^\circ 04' 37''$  (LT)  
 $D = 0^\circ 48' 00''$   
 $R = 7,161.97'$   
 $T = 757.62'$   
 $L = 1,509.62'$   
 $E = 39.96'$   
 $e = 3.70\%$   
 $T.R. = 38.4'$   
 $S.E. RUN = 94.6'$   
 $P.C. STA. = 1431+81.39$   
 $P.T. STA. = 1446+91.02$

S.E. TRANSITIONS  
 STA. 1430+79.92 TO STA. 1432+12.92  
 STA. 1446+59.49 TO STA. 1447+92.49

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEET</b>		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ei:\pwwork\pwwork\sparksgw\10216263\067243-shr-pln.dgn		DRAWN -	REVISED -		SCALE:	SHEET NO. 6 OF 7 SHEETS	STA. 1421+32.00 TO STA. 1451+32.00	304	4RS-3, BR-1	PIKE	41	23
		CHECKED -	REVISED -		CONTRACT NO. 72243							
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

EXIST. CURVE 96-7  
 PI STA. = 1461+68.31  
 $\Delta = 14^\circ 21' 15''$  (RT)  
 $D = 1^\circ 24' 00''$   
 $R = 4,092.56'$   
 $T = 515.35'$   
 $L = 1,025.31'$   
 $E = 32.32'$   
 $e = 3.30\%$   
 $T.R. = 38.4'$   
 $S.E. RUN = 84.4'$   
 $P.C. STA. = 1456+52.95$   
 $P.T. STA. = 1466+78.26$

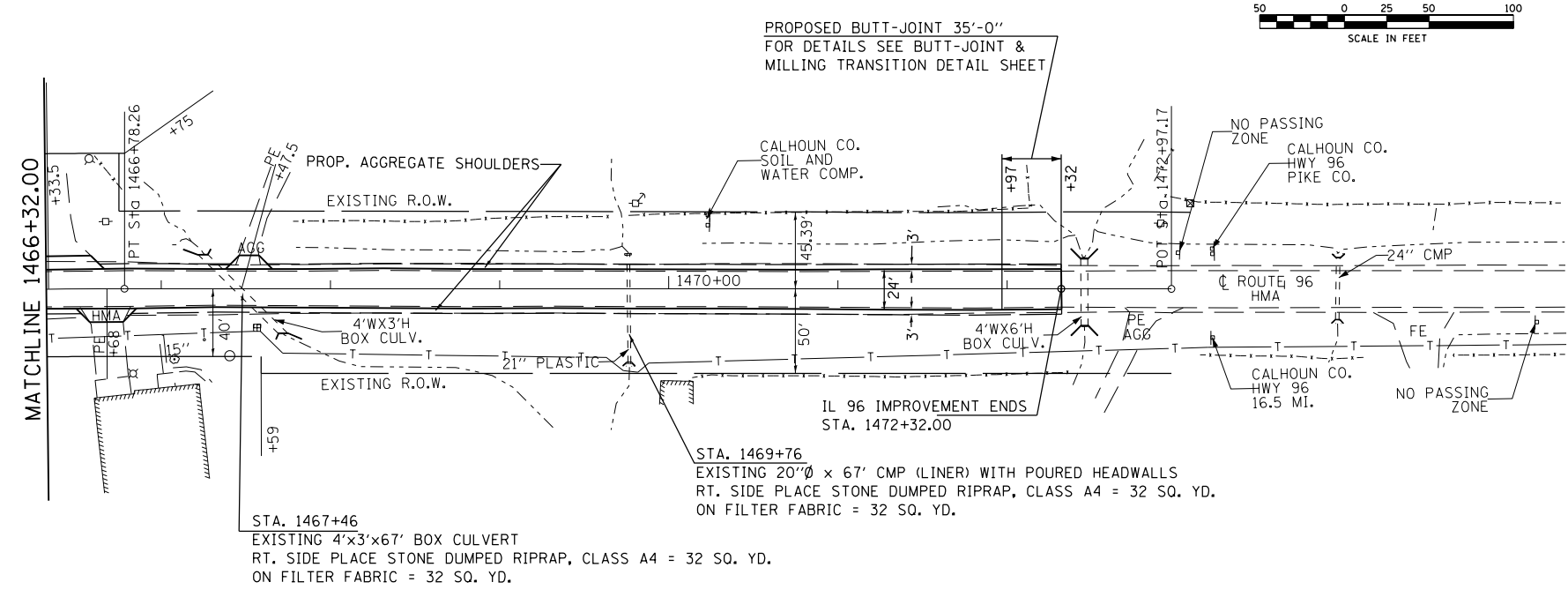
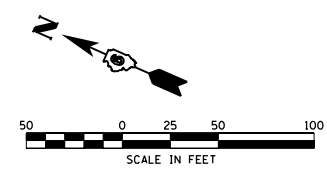
S.E. TRANSITIONS  
 STA. 1455+58.28 TO STA. 1456+81.08  
 STA. 1466+50.13 TO STA. 1467+72.93



STA. 1453+67  
 EXISTING 12'x7'x47' BOX CULVERT  
 RT. SIDE PLACE STONE DUMPED RIPRAP, CLASS A4 = 50 SQ. YD.  
 ON FILTER FABRIC = 50 SQ. YD.

STA. 1453+67  
 CLASS C PATCHES, TYPE III  
 10 INCH  
 RT. 16 SQ. YD.  
 LT. 16 SQ. YD.

STA. 1462+85  
 EXISTING 24'x45' CMP WITH POURED HEADWALLS  
 CULVERT TO BE CLEANED AND INSERT-  
 INSERTION CULVERT LINE 24'x45'  
 RT. SIDE PLACE STONE DUMPED RIPRAP, CLASS A4 = 5 SQ. YD.  
 ON FILTER FABRIC = 5 SQ. YD.



STA. 1467+46  
 EXISTING 4'x3'x67' BOX CULVERT  
 RT. SIDE PLACE STONE DUMPED RIPRAP, CLASS A4 = 32 SQ. YD.  
 ON FILTER FABRIC = 32 SQ. YD.

STA. 1469+76  
 EXISTING 20'x67' CMP (LINER) WITH POURED HEADWALLS  
 RT. SIDE PLACE STONE DUMPED RIPRAP, CLASS A4 = 32 SQ. YD.  
 ON FILTER FABRIC = 32 SQ. YD.

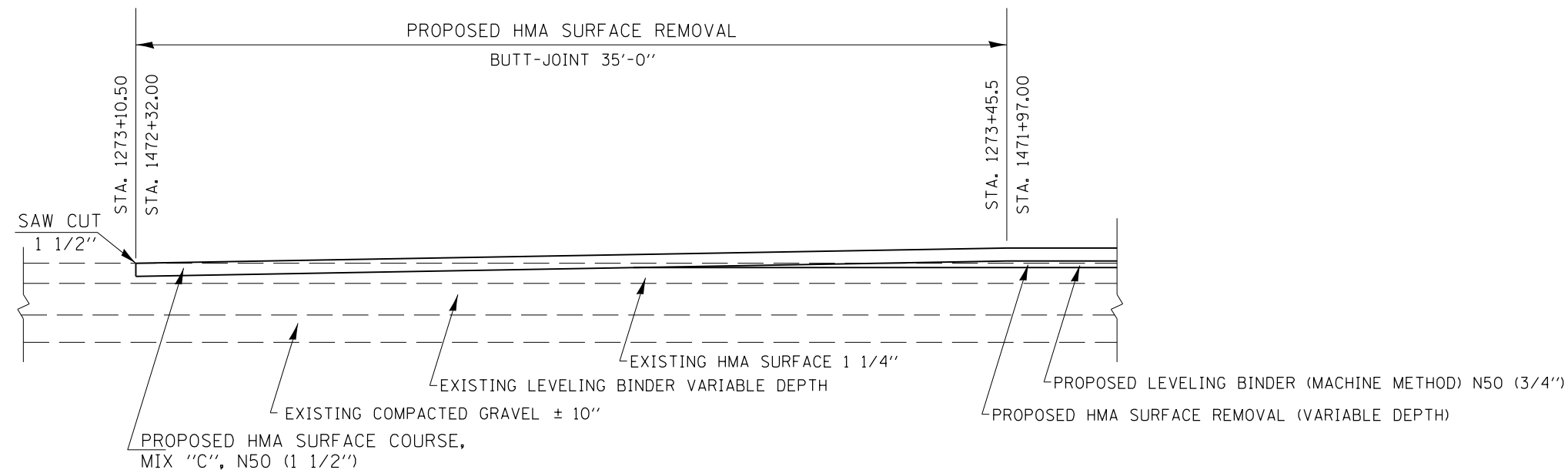
PROPOSED BUTT-JOINT 35'-0"  
 FOR DETAILS SEE BUTT-JOINT &  
 MILLING TRANSITION DETAIL SHEET

FILE NAME =	USER NAME = sparksq	DESIGNED -	REVISED -
ei:\pw\work\p\idot\sparksq\140216263\067243-sht.pln.dgn		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

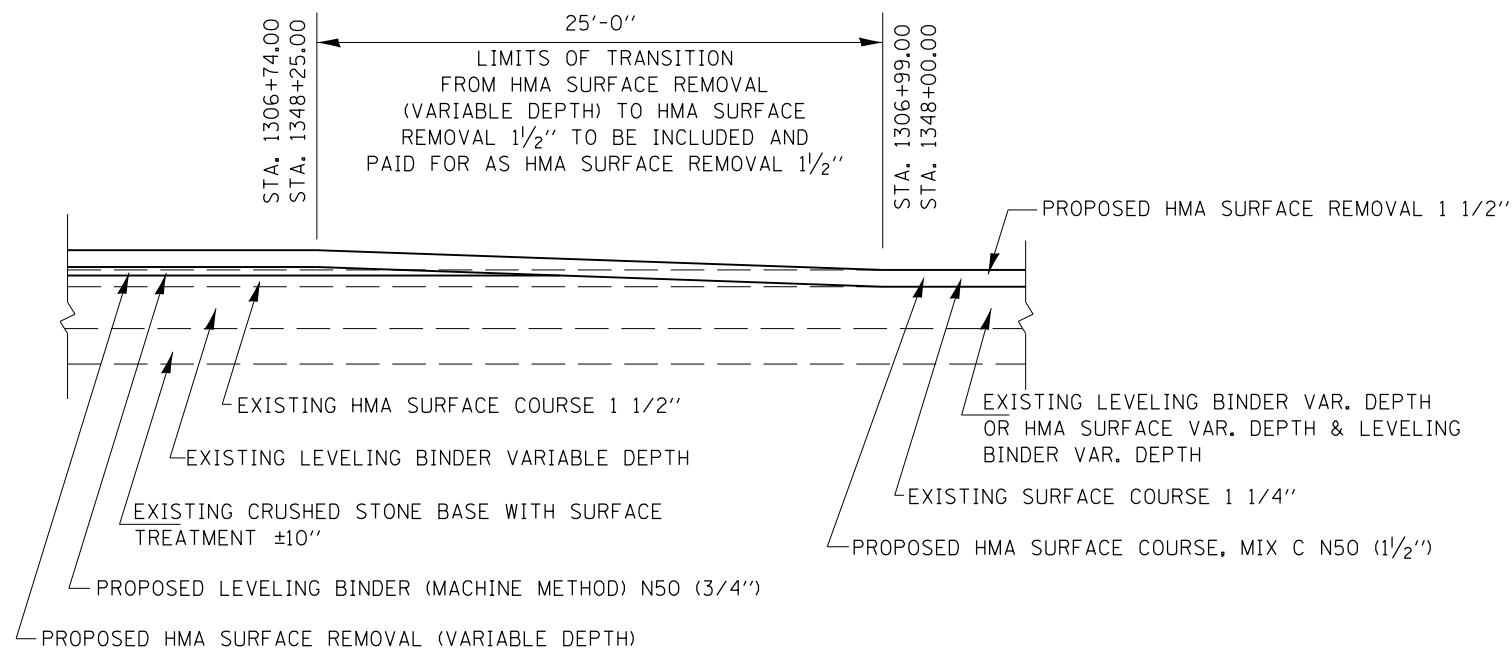
<b>PLAN SHEET</b>	
SCALE:	SHEET NO. 7 OF 7 SHEETS
STA. 1451+32.00 TO STA. 1472+30.00	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	4RS-3, BR-1	PIKE	41	24
CONTRACT NO. 72243				
ILLINOIS FED. AID PROJECT				



### 35' BUTT-JOINT DETAIL

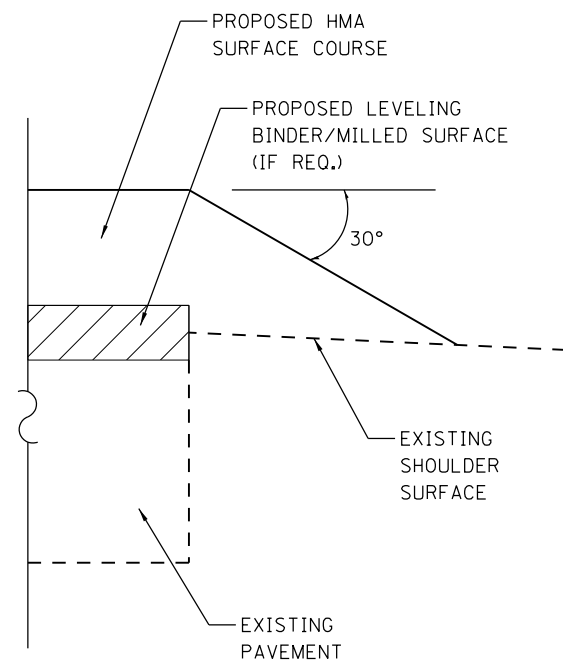
STA. 1273+10.50 TO STA. 1273+45.50  
 STA. 1471+97.00 TO STA. 1472+32.00



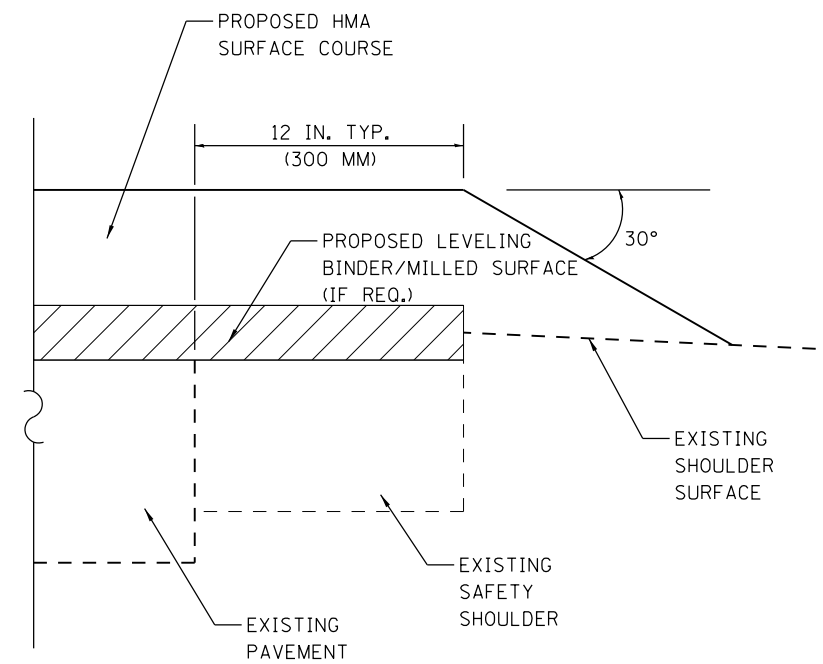
### 25' MILLING TRANSITION DETAIL

STA. 1306+74.00 TO STA. 1306+99.00  
 STA. 1346+00.00 TO STA. 1346+25.00

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BUTT-JOINT &amp; MILLING TRANSITION DETAILS</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ci:\pwork\pwork\sparksgw\0216263\067243-sht-detail.dgn		DRAWN -	REVISED -		304	4RS-3, BR-1	PIKE	41	25			
PLOT SCALE = 40.0000' / in.		CHECKED -	REVISED -		CONTRACT NO. 72243			ILLINOIS FED. AID PROJECT				
PLOT DATE = Feb-07-2012 02:53:02PM		DATE -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.			



SAFETY EDGE AT EDGE OF EXISTING PAVEMENT



SAFETY EDGE AT EDGE OF EXISTING SAFETY SHOULDER

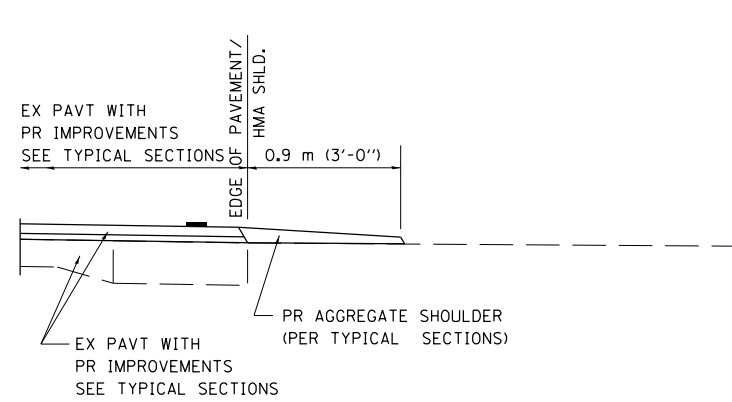
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	PLOT DATE = Feb-07-2012 02:53:03PM	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

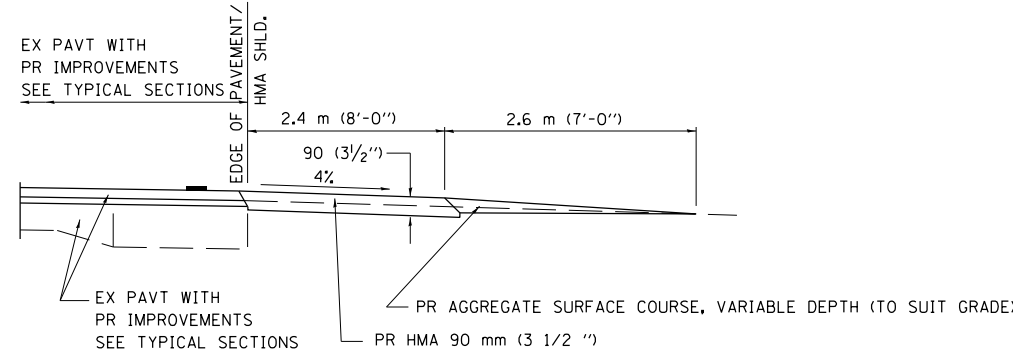
**SAFETY EDGE DETAILS**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

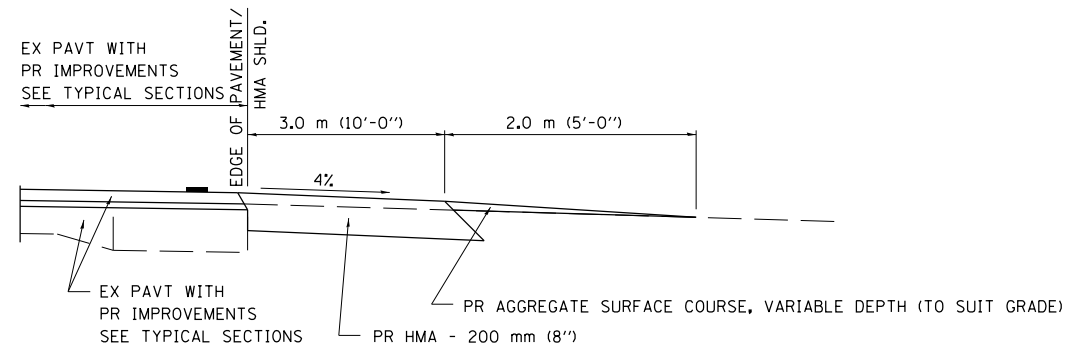
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	4RS-3, BR-1	PIKE	41	26
<b>CONTRACT NO. 72243</b>				
ILLINOIS FED. AID PROJECT				



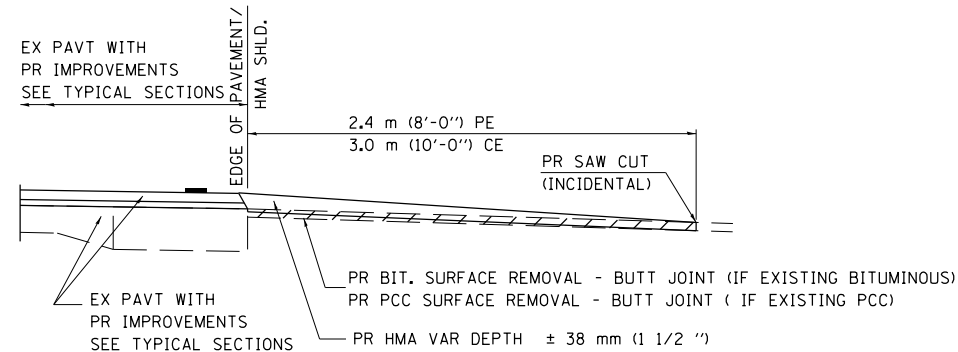
**SECTION A-A FOR EX EARTH/AGGREGATE FE**



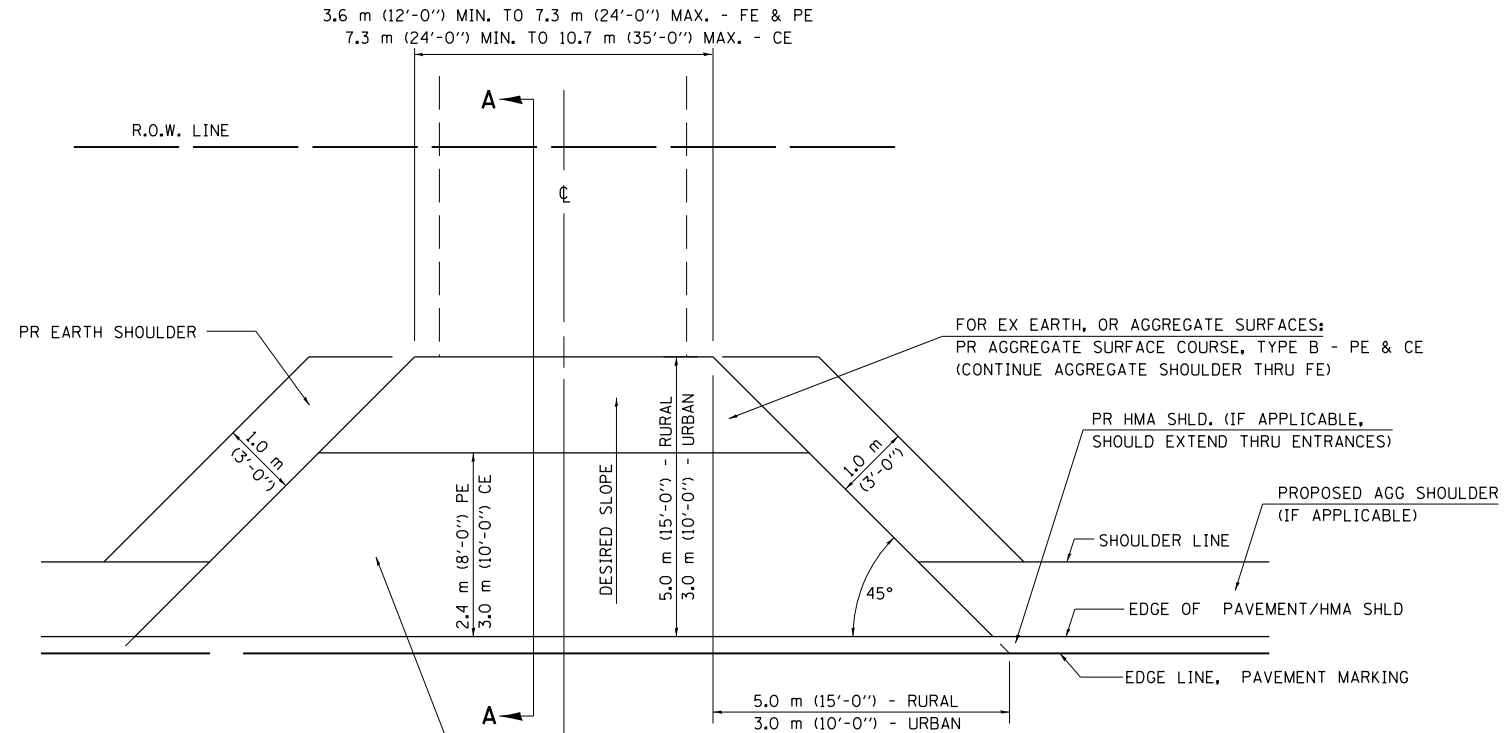
**SECTION A-A FOR EX EARTH/AGGREGATE PE**



**SECTION A-A FOR EX EARTH/AGGREGATE CE & SIDE ROAD**



**SECTION A-A FOR EX BITUMINOUS/PC CONCRETE PE, CE & SIDE ROAD**



FOR EX EARTH OR AGGREGATE SURFACES:  
 PR BIT SURFACE REMOVAL (IF APPLICABLE)  
 PR AGGREGATE SHOULDER THRU - FE  
 PR BITUMINOUS CONCRETE 90 mm (3 1/2") - PE  
 PR BITUMINOUS CONCRETE 200mm (8") - CE

FOR EX BITUMINOUS CONCRETE SURFACES:  
 PR BITUMINOUS SURFACE REMOVAL-BUTT JOINT

FOR EX PCC SURFACES:  
 PR PCC SURFACE REMOVAL-BUTT JOINT

**GENERAL NOTES:**

THE RESIDENT ENGINEER WILL DETERMINE THE EXACT TYPE OF IMPROVEMENT TO BE COMPLETED FOR ALL ENTRANCES, SIDEROADS AND MAILBOX TURNOUTS ON THIS PROJECT.

THE PLAN DETAILS AND SCHEDULES SHOULD BE USED AS A GUIDE FOR THE ENGINEER TO IMPLEMENT THE FINAL DESIGN. THE ENGINEER MAY DECIDE TO SALVAGE PORTIONS OF THE EXISTING ENTRANCE PAVEMENT STRUCTURE; THEREFORE, REDUCING PAY ITEM QUANTITIES. NO ADDITIONAL PAYMENT WILL BE ALLOWED FOR THIS REDUCTION IN QUANTITIES.

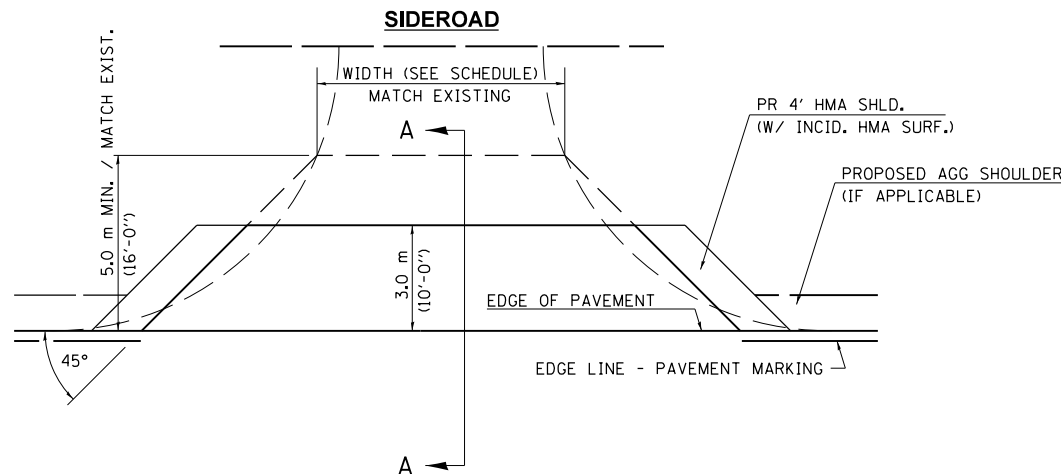
ANY WORK THE ENGINEER REQUIRES WHICH IS NOT COVERED BY A PAY ITEM CONTAINED IN THE PLANS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

BITUMINOUS CONCRETE REQUIRED TO CONSTRUCT THE ENTRANCES SHALL BE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 406 AND 408 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER.

WHEN THE BITUMINOUS CONCRETE PROPOSED FOR THE IMPROVEMENT IS THICKER THAN 75 mm (3 INCHES) AND REQUIRE PLACEMENT IN MORE THAN ONE LIFT. THE BOTTOM LIFT(S) SHALL MEET THE REQUIREMENTS OF BITUMINOUS BASE COURSE IN SECTION 406 OF THE STANDARD SPECIFICATIONS AND THE TOP LIFT OF 50 mm (2 INCHES) SHALL MEET THE REQUIREMENTS OF BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE.

THIS WORK WILL BE PAID FOR IN ACCORDANCE WITH SECTIONS 351, 358, 408, 423 AND 440 OF THE STANDARD SPECIFICATIONS.

ALL DIMENSIONS ARE IN MILLIMETERS ( INCHES ) UNLESS OTHERWISE SHOWN.



FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
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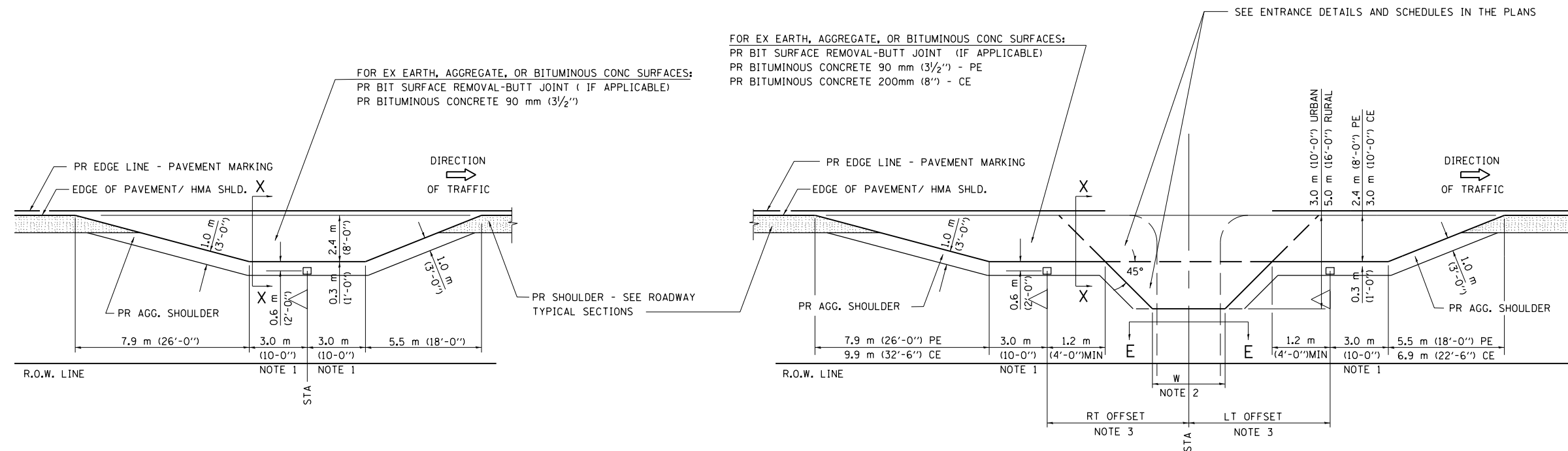
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DIST. 6 DETAILS FOR RURAL/URBAN ENT., MAILBOX  
TURNOUT & SIDEROADS W/O CONC. GUTTER (3P-PROJ.)**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	4RS-3, BR-1	PIKE	41	27
CONTRACT NO. 72243				
ILLINOIS FED. AID PROJECT				

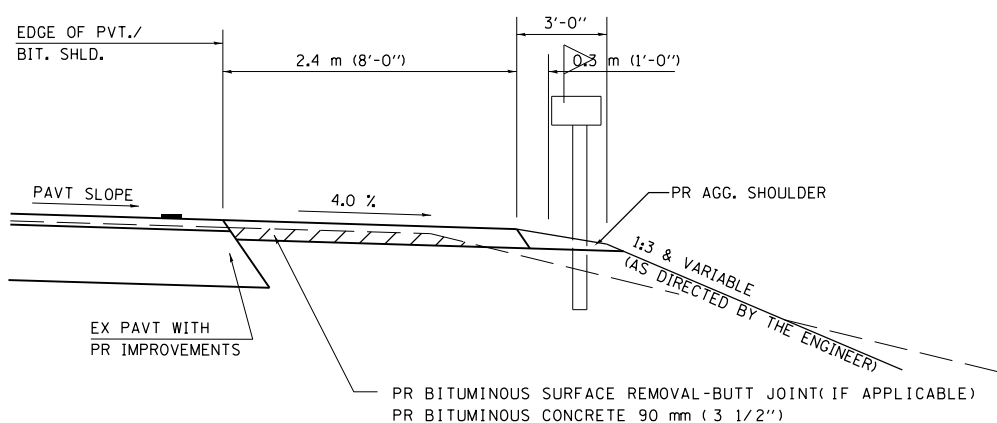
SCALE: SHEET NO. OF SHEETS STA. TO STA.

# DETAILS OF MAILBOX TURNOUTS



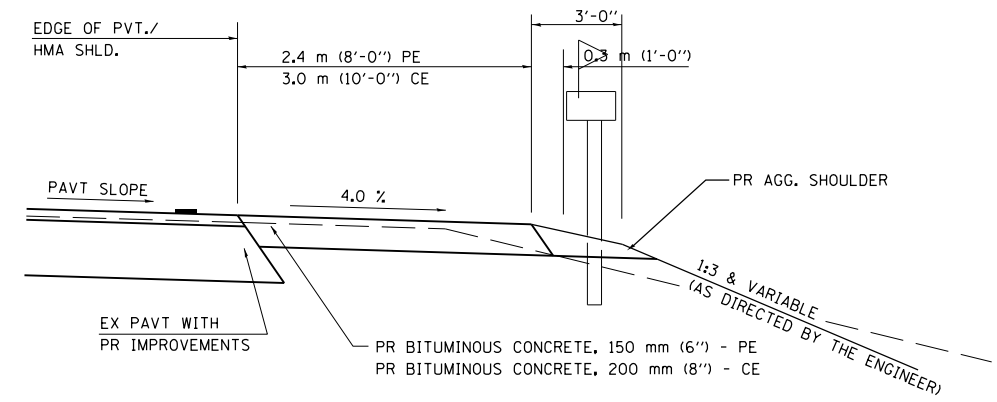
**PLAN - MAILBOX TURNOUTS**

**PLAN - COMBINED MAILBOX TURNOUT WITH TRAILING OR LEADING ENTRANCE**



**SECTION X-X THRU MAILBOX TURNOUT  
ALSO APPLIES TO MAILBOX TURNOUTS COMBINED WITH  
EX EARTH, AGGREGATE, OR BITUMINOUS PE & FE**

(DETAIL APPLIES WHEN M.B. TURNOUT DOES NOT EXIST.  
IF EXISTING, TREAT SAME AS ENTRANCE.)



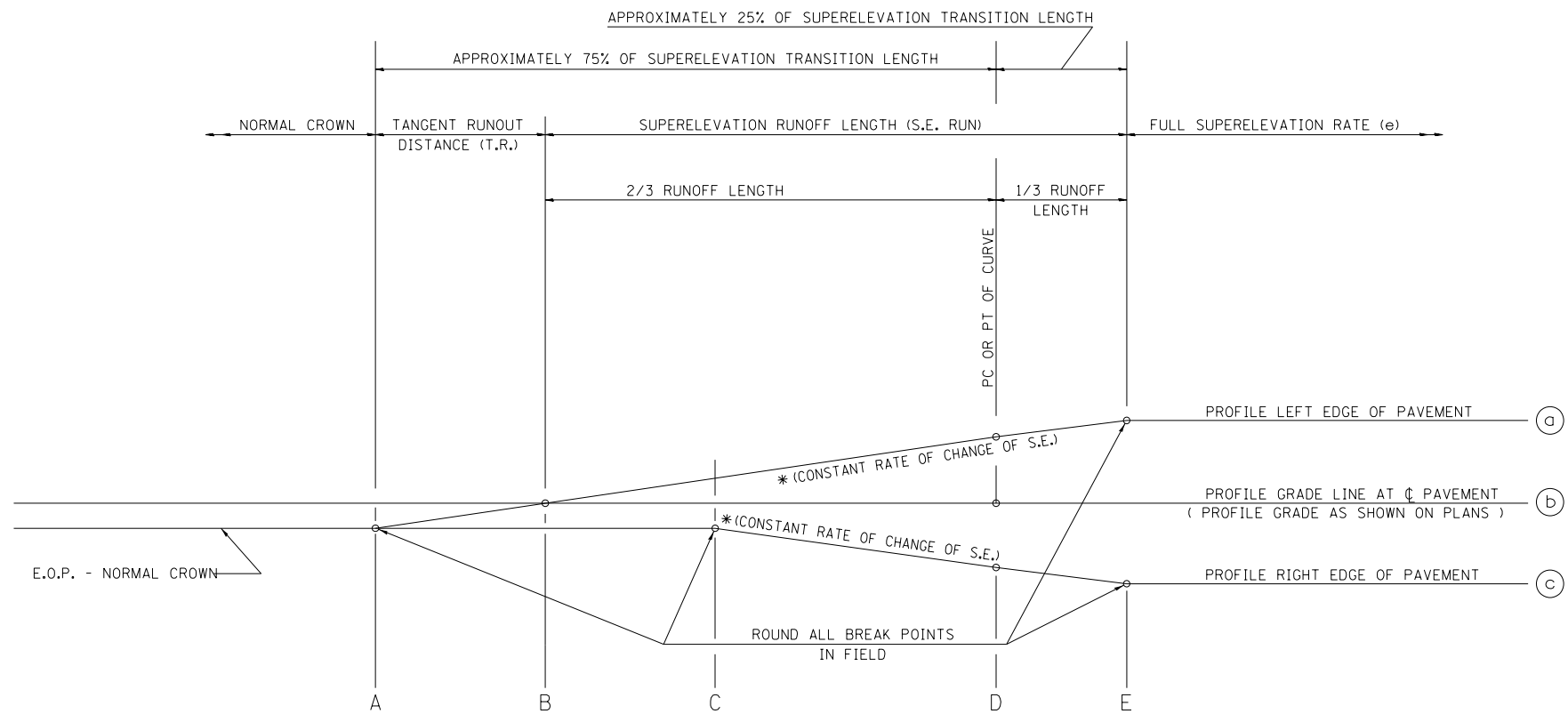
**SECTION X-X THRU MAILBOX TURNOUT  
COMBINED WITH EX BITUMINOUS CONC & PC CONC PE & CE**

(DETAIL APPLIES WHEN M.B. TURNOUT DOES NOT EXIST.  
IF EXISTING, TREAT SAME AS ENTRANCE.)

- NOTE 1 IF MORE THAN ONE MAILBOX IS PRESENT, DIMENSION FROM CENTER OF END MAILBOX.
- NOTE 2 FOR ENTRANCE LAYOUT DIMENSIONS AND SECTIONS A-A & E-E REFER TO THE SCHEDULES IN THE PLANS.
- NOTE 3 BOTH LT OR RT OFFSETS FOR MAILBOX SHOWN USE OFFSET DIMENSION PER SCHEDULE AND REFER TO LAYOUT SHOWN ON THE PLAN.

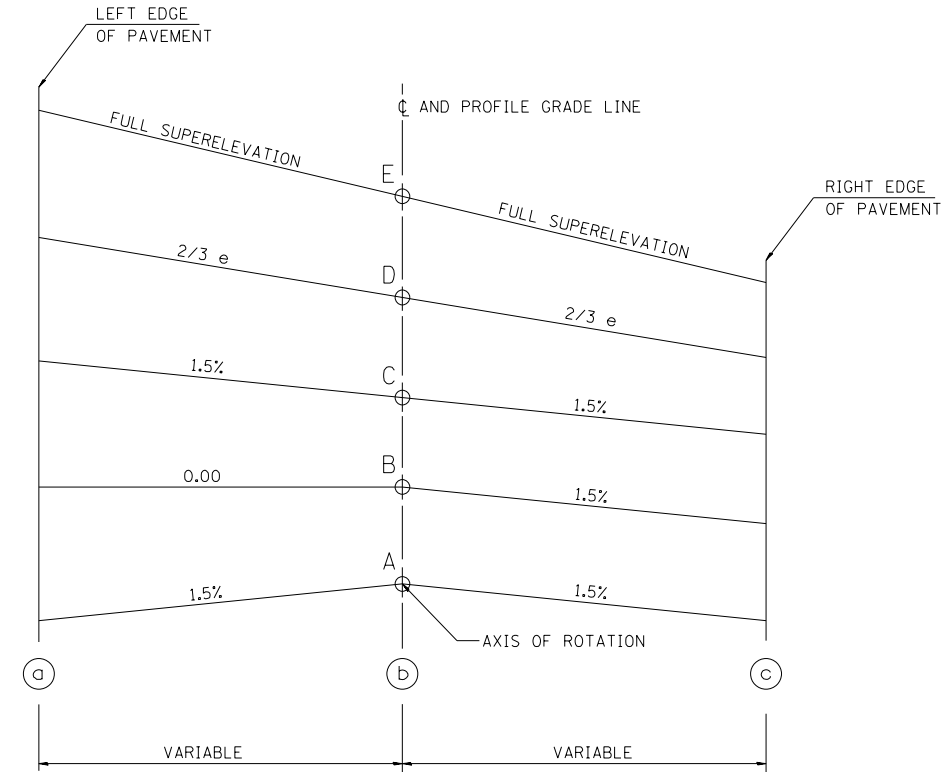
ALL DIMENSIONS ARE IN MILLIMETERS ( INCHES )  
UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DIST.6 DETAILS FOR RURAL/URBAN ENT., MAILBOX TURNOUT &amp; SIDEROADS W/O CONC. GUTTER (3P-PROJ.)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ci:\pw\work\p\idot\sparksgw\0216263\067243-sht-detail.dgn		DRAWN -	REVISED -			304	4RS-3, BR-1	PIKE	41	28	
PLOT SCALE = 40.0000' / in.		CHECKED -	REVISED -			<b>CONTRACT NO. 72243</b>					
PLOT DATE = Feb-07-2012 02:53:03PM		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
					SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.		



SEE PLANS FOR CURVE DATA INFORMATION  
 CURVE DATA  
 P.I. STA=  
 $\Delta$ =  
 R=  
 T=  
 L=  
 E=  
 e= SUPERELEVATION RATE IN PERCENT  
 T.R.= TANGENT RUNOUT DISTANCE  
 S.E. RUN= SUPERELEVATION RUNOFF LENGTH  
 P.C. STA=  
 P.T. STA=

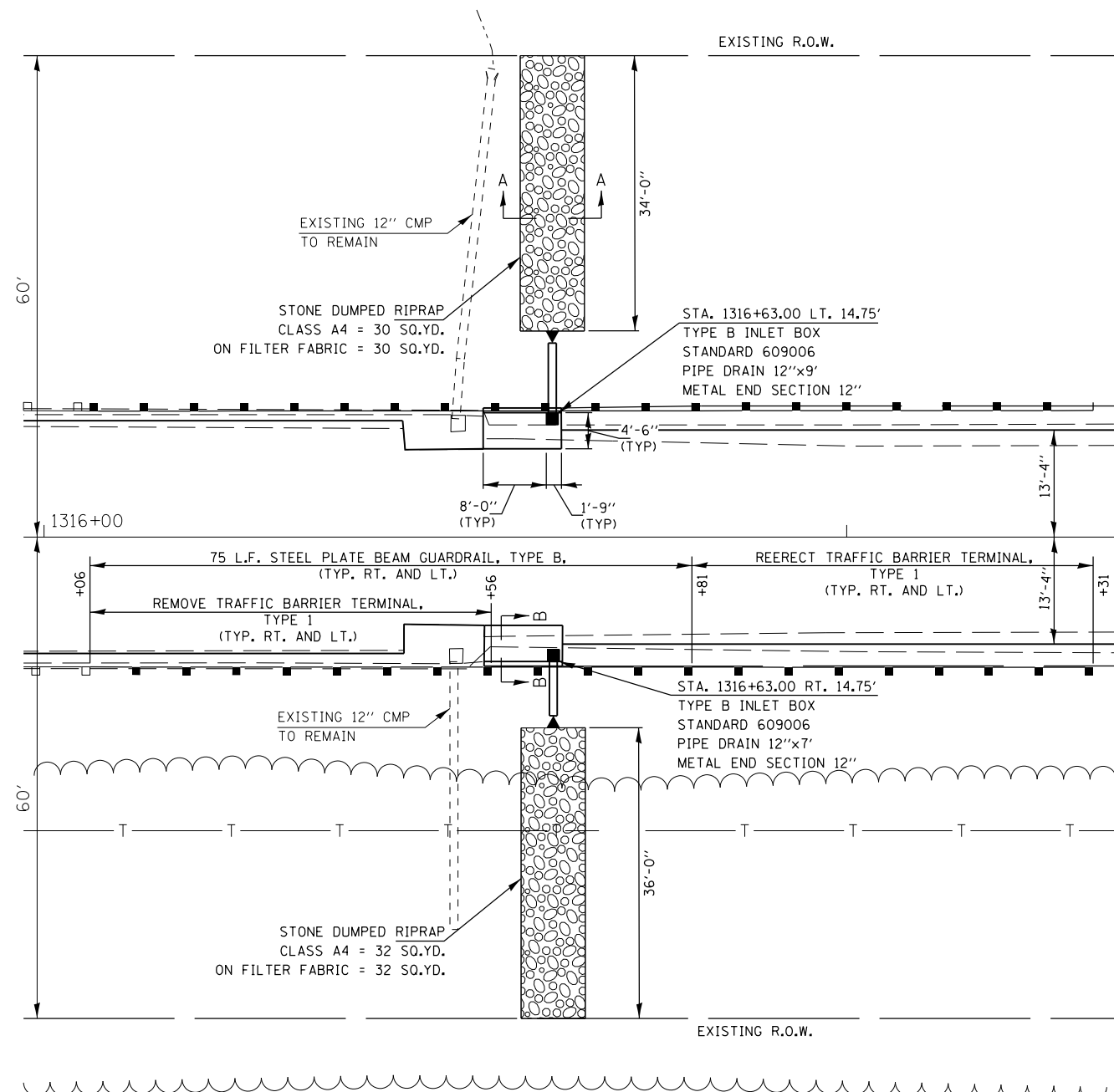
TYPICAL PROFILE - S.E. TRANSITION



TYPICAL CROSS SECTION - S.E. TRANSITION

TABLE OF SUPERELEVATION BREAK POINT LOCATIONS							
CURVE NO.	e	A	B	C	D	E	TRANSITION
96-1	4.70%	1266+88.84	1267+27.24	126765.64	1268+07.37	1268+47.44	TRANS. IN
		1279+27.62	1278+89.22	1278+50.82	1278+09.09	1277+69.02	TRANS. OUT
96-2	0.00%	EXISTING CURVE NOT SUPERELEVATED					
96-3	0.00%	EXISTING CURVE NOT SUPERELEVATED					
96-4	2.70%	1349+16.73	1349+55.13	1349+93.53	1350+01.2	1350+24.23	TRANS. IN
		1363+87.70	1363+49.30	1363+10.90	1363+03.23	1362+80.20	TRANS. OUT
96-5	3.30%	1389+43.32	1389+81.72	1390+20.12	1390+37.99	1390+66.12	TRANS. IN
		1401+78.31	1401+39.91	1401+01.51	1400+83.64	1400+55.51	TRANS. OUT
96-6	3.70%	1430+79.92	1431+18.32	1431+56.72	1431+81.39	1432+12.92	TRANS. IN
		1447+92.49	1447+54.09	1447+15.69	1446+91.02	1446+59.49	TRANS. OUT
96-7	3.30%	1455+58.28	1455+96.68	1456+35.08	1456+52.95	1456+81.08	TRANS. IN
		1467+72.93	1467+34.53	1466+96.13	1466+78.26	1466+50.13	TRANS. OUT

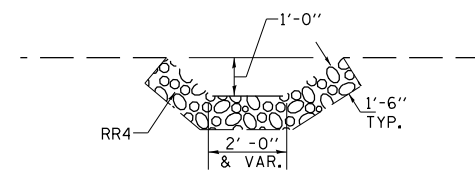




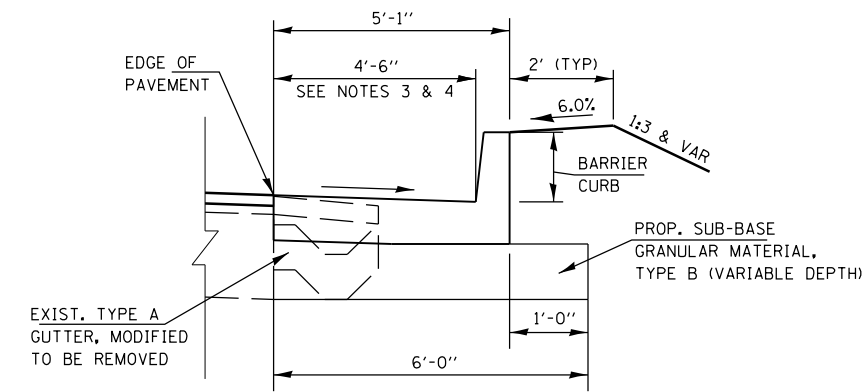
**PLAN**

**NOTES :**

1. DIMENSIONS AND ELEVATIONS ARE APPROXIMATE. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND ELEVATIONS PRIOR TO ORDERING MATERIAL.
2. ALL SAW CUTS NECESSARY TO COMPLETE THE WORK DETAIL, SHALL BE INCLUDED IN THE COST FOR THE VARIOUS PAY ITEMS INVOLVED.



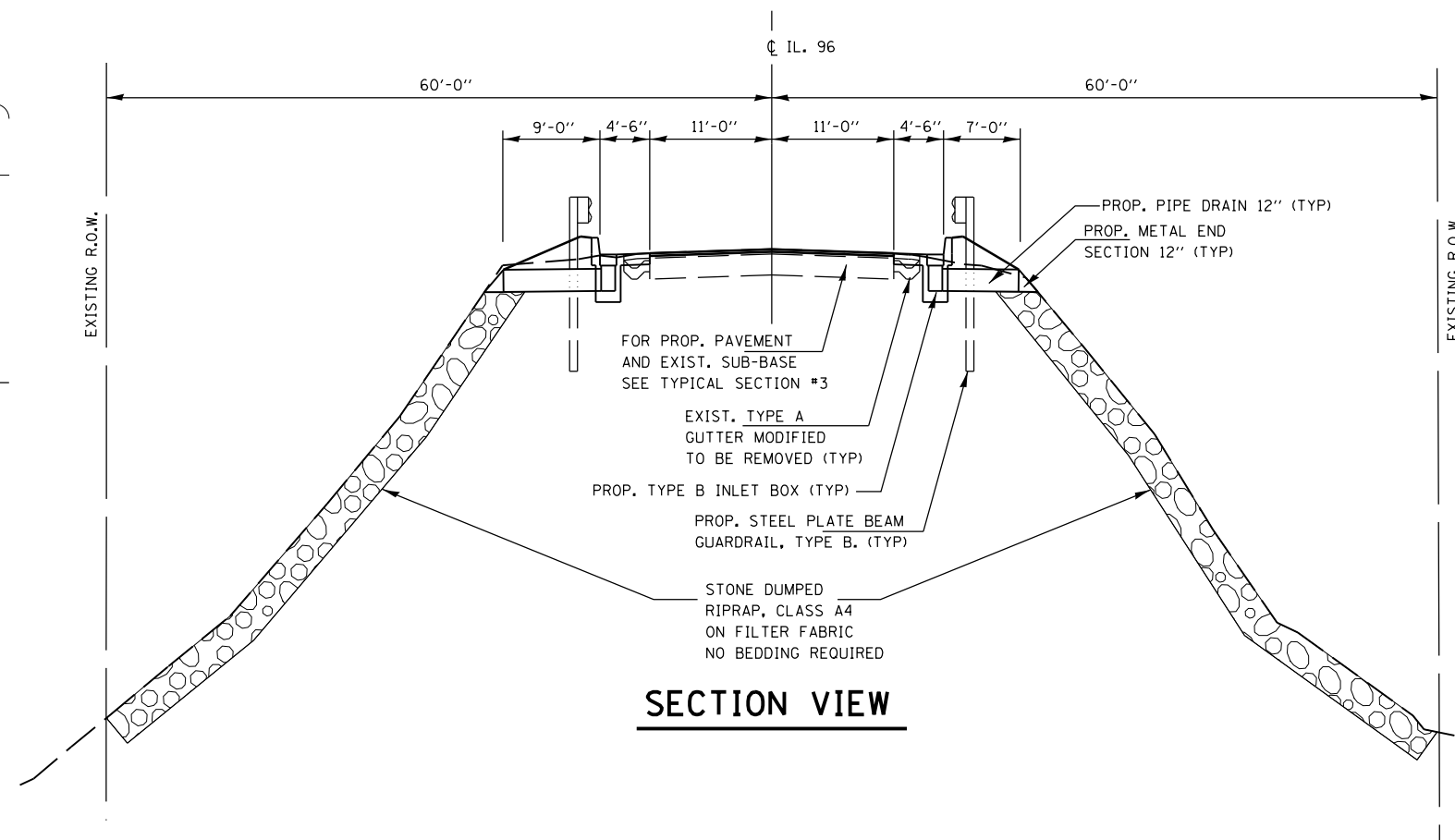
**SECTION A-A**



**SECTION B-B**

**NOTES:**

3. WHEN CONSTRUCTING OUTLETS FOR CURB & GUTTER USING STANDARD 609006, MODIFY AS REQUIRED TO MATCH EXISTING.
4. PAY FOR CONCRETE GUTTER AS CLASS SI CONCRETE OUTLET STANDARD 606101.



**SECTION VIEW**

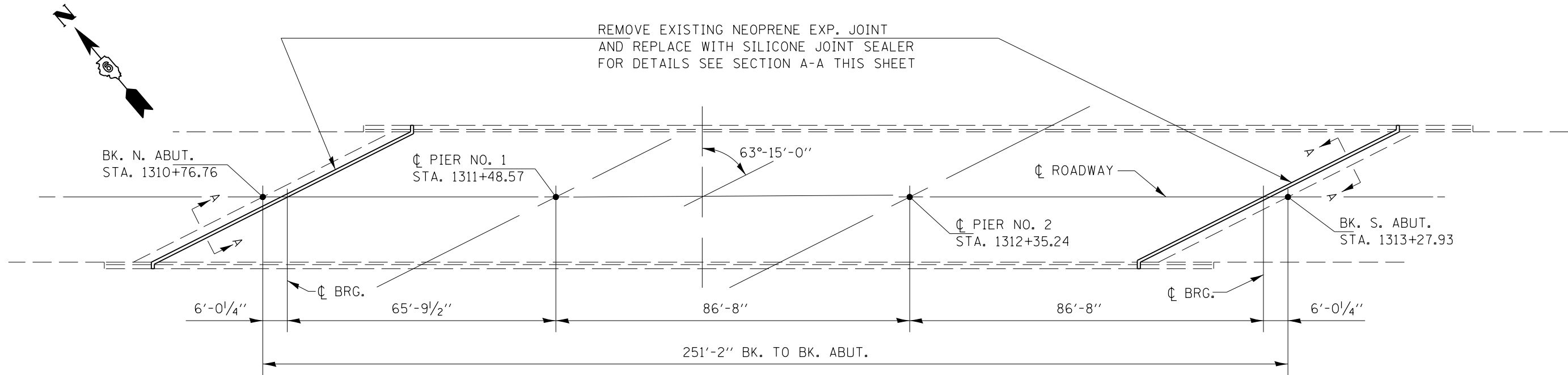
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		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPE B INLET BOX DETAILS**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

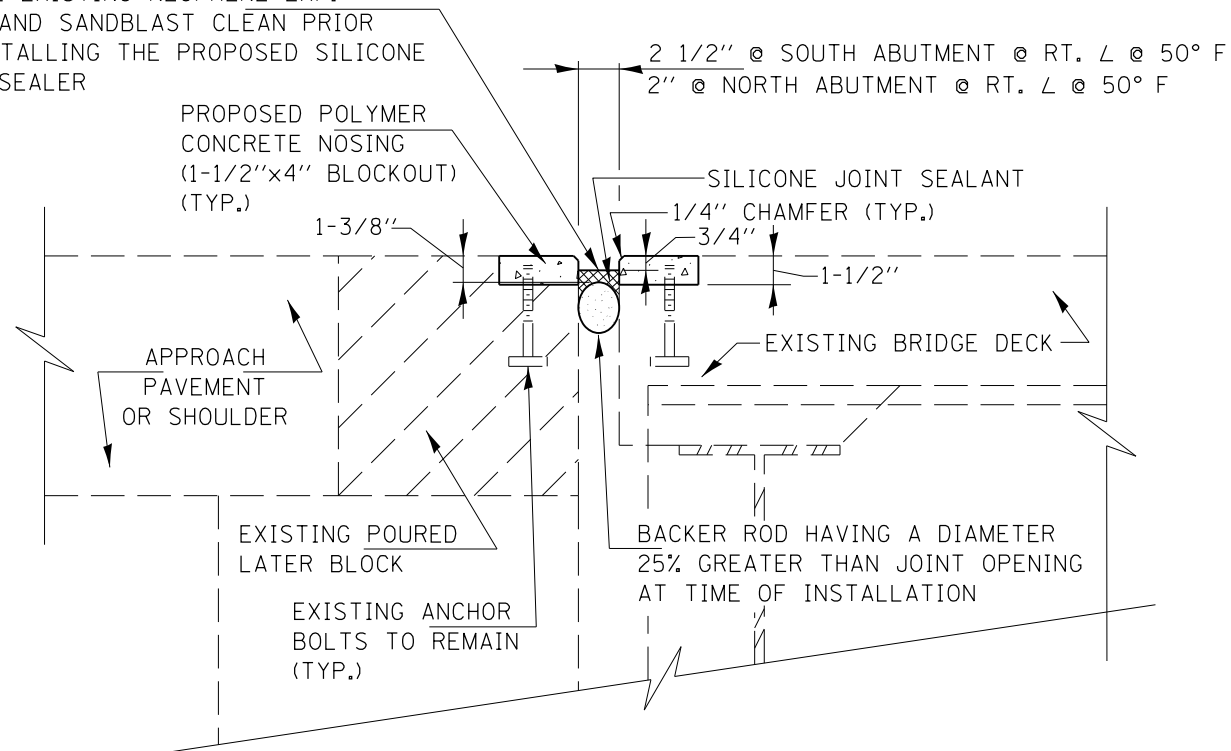
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	4RS-3, BR-1	PIKE	41	30
CONTRACT NO. 72243			ILLINOIS FED. AID PROJECT	



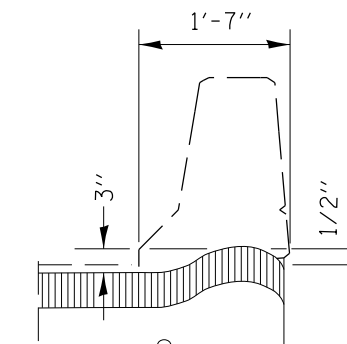
**PLAN**

SN 075-0047

REMOVE EXISTING NEOPRENE EXP. JOINT AND SANDBLAST CLEAN PRIOR TO INSTALLING THE PROPOSED SILICONE JOINT SEALER



**SECTION A-A**  
SILICONE JOINT DETAIL



**TYPICAL END OF SEAL TREATMENT**

**BILL OF MATERIAL**

ITEM	UNIT	TOTAL
SILICONE JOINT SEALER, 2"	FOOT	74
SILICONE JOINT SEALER, 2 1/2"	FOOT	74
POLYMER CONCRETE	CU. FT.	12.5

FILE NAME =	USER NAME = sparksgw	DESIGNED -	REVISED -
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		DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**JOINT REPAIR DETAILS**

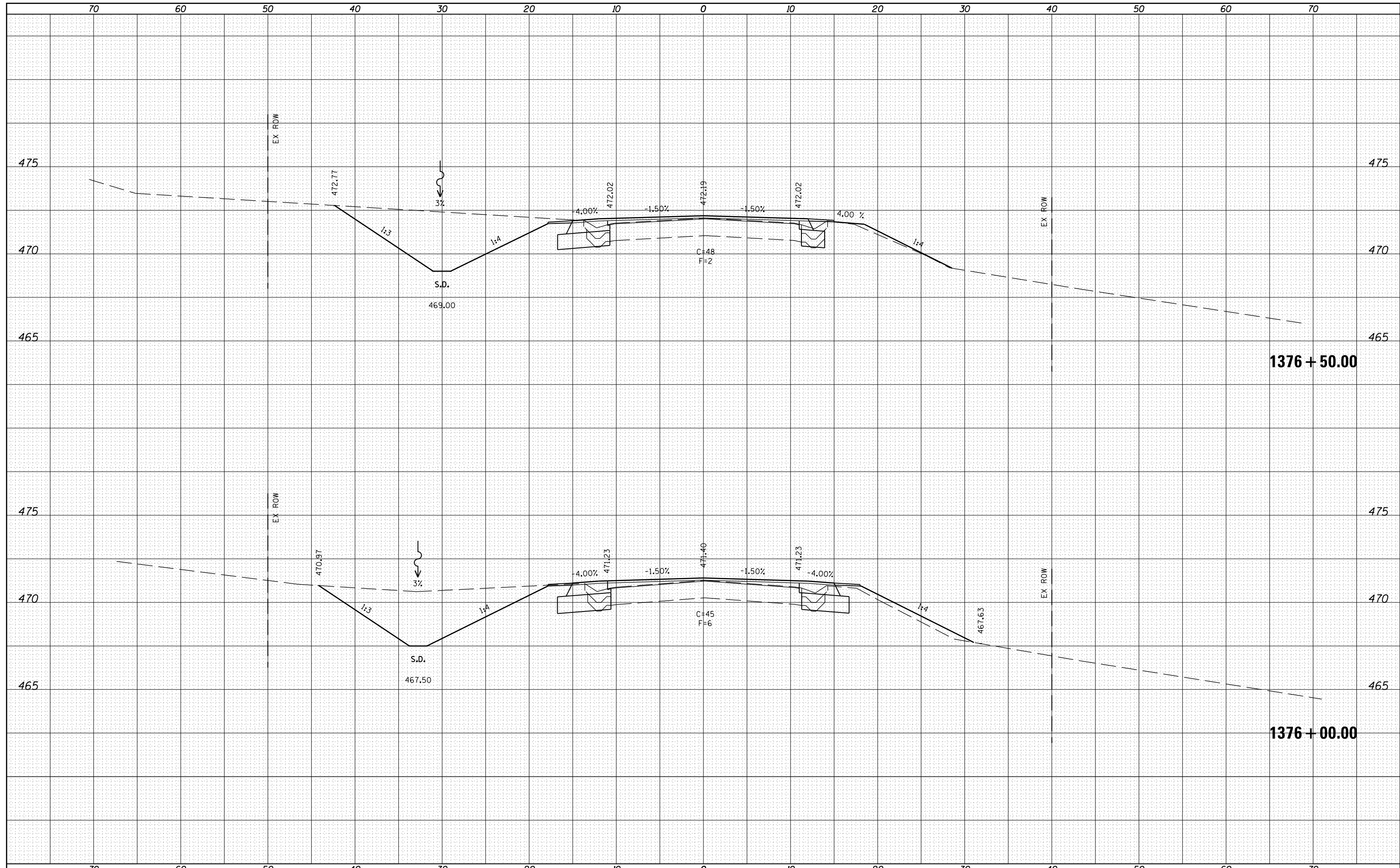
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	4RS-3, BR-1	PIKE	41	31
CONTRACT NO. 72243				
ILLINOIS FED. AID PROJECT				



DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS
	CHECKED

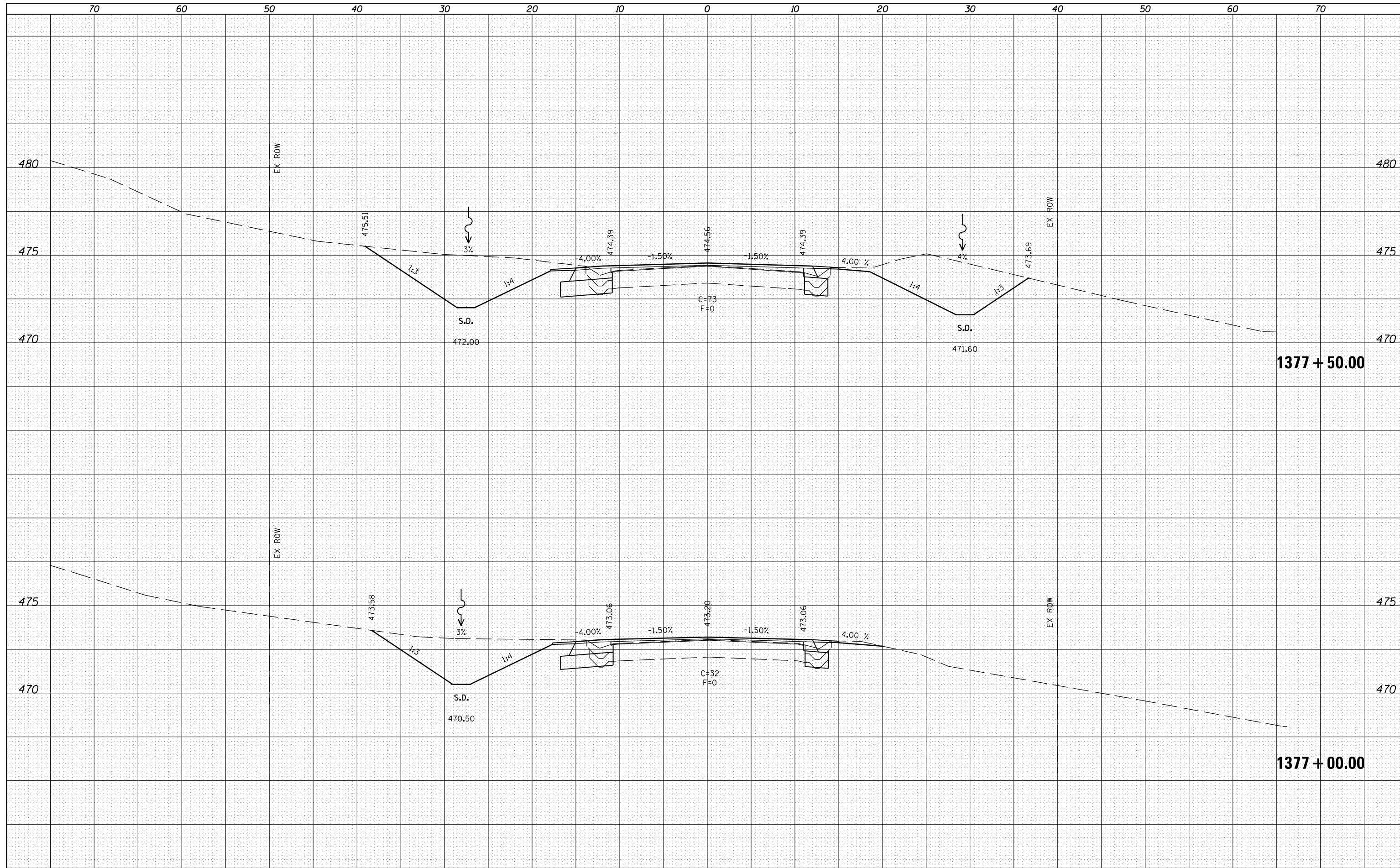
DATE	
BY	
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NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS
	CHECKED



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		DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA. 1376+00.00
					ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS
	CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS
	CHECKED



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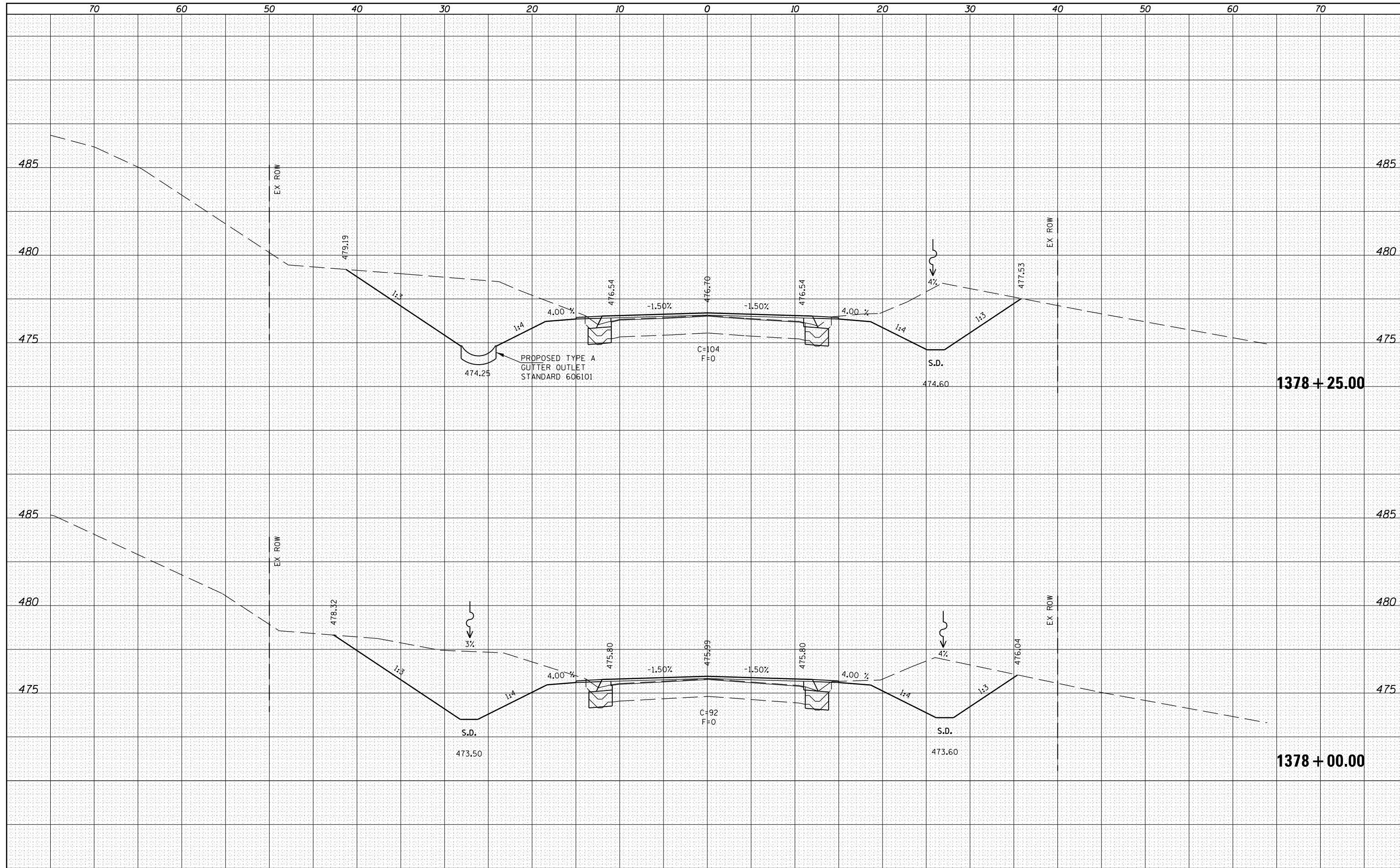
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

SCALE:	SHEET	OF	SHEETS	STA. 1377+00.00	TO	STA. 1377+50.00
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	4RS-3, BR-1	PIKE	41	34
CONTRACT NO. 72243				
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED



**1378 + 25.00**

**1378 + 00.00**

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

SCALE:	SHEET	OF	SHEETS	STA. 1378+00.00	TO STA. 1378+25.00
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	4RS-3, BR-1	PIKE	41	35
CONTRACT NO. 72243				
ILLINOIS FED. AID PROJECT				













