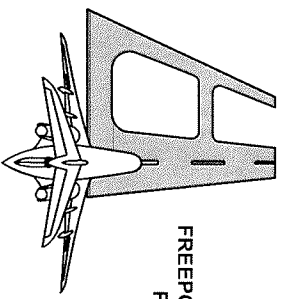


# 14A

FR039  
TOTAL  
SHEETS: 21

# CITY OF FREEPORT FREEPORT, ILLINOIS

## CONSTRUCTION PLANS FOR FREEPORT - ALBERTUS AIRPORT



FREEPORT - ALBERTUS AIRPORT  
FREEPORT, ILLINOIS

FREEPORT-ALBERTUS AIRPORT

APPROVED BY *Mary Ann George*  
HONORABLE GEORGE W. GAULRAPP, MAYOR

DATE \_\_\_\_\_  
MARCH 02, 2012

### DESIGN INFORMATION

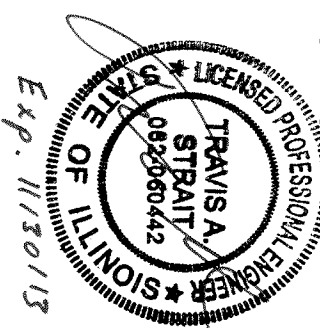
DESIGN AIRCRAFT APPROACH CATEGORY D  
DESIGN AIRCRAFT GROUP 2

TOWNSHIP: 26 N. RANGE: 8 E. TOWNSHIP: SILVER CREEK  
SECTION: 21 STEPHENSON COUNTY  
MAX. EQUIPMENT HT.: 25 FT.

CALL J.U.L.I.E  
BEFORE EXCAVATING  
1-800-892-0123

Illinois Professional Engineering Practice Act of 1989

**CMT**  
http://www.cmtengr.com  
Springfield, Illinois  
Aurora, Illinois  
Rockford, Illinois  
St. Louis, Mo.



SUBMITTED BY *[Signature]*

DATE \_\_\_\_\_  
MARCH 02, 2012  
license expires 11-30-2012

11294-02-00/06

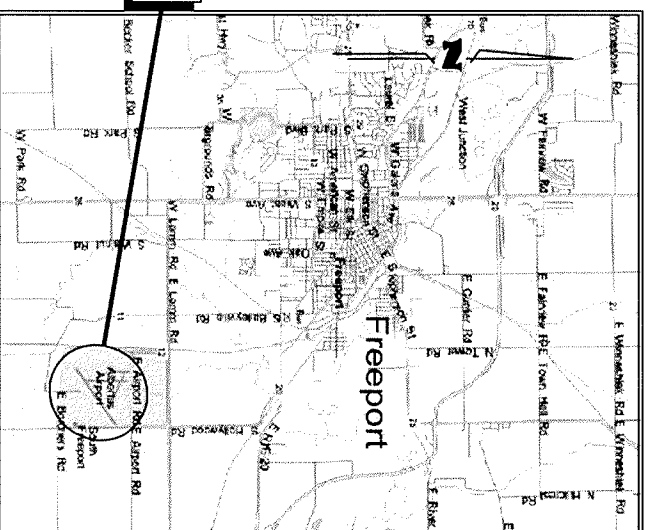
## Terminal Apron Rehabilitation / Rehabilitate Taxiways A and E Phase 2 Additive Alternate 1 (AA1): Install Electrical Duct Bank and HandHoles Additive Alternate 2 (AA2): Rehabilitate Taxiway Connectors to Runway 6/24

FINAL SUBMITTAL

ILLINOIS PROJECT: FEP-4174  
AIP PROJECT: 3-17-0045-B24

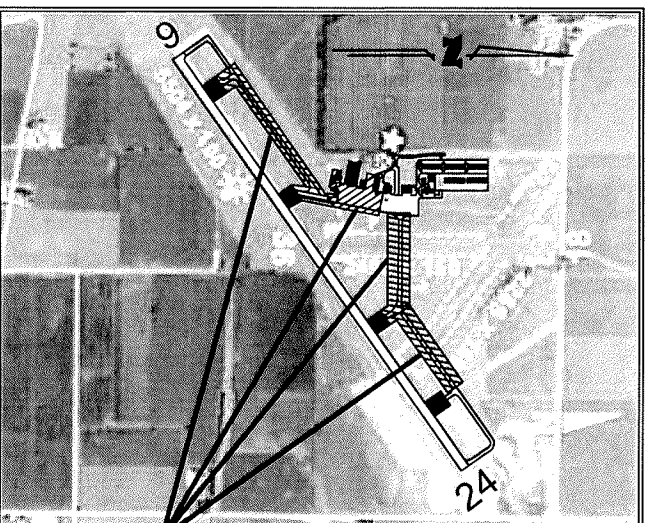
MARCH 02, 2012

PROJECT  
LOCATION



LOCATION MAP

PROJECT  
LOCATION



SITE PLAN

### INDEX TO SHEETS

- COVER SHEET (CVR1)
- SUMMARY OF QUANTITIES (QTY1)
- CONSTRUCTION PHASING PLAN (CPP1)
- CONSTRUCTION PHASING PLAN NOTES - 1 (CPN1)
- CONSTRUCTION PHASING PLAN NOTES - 2 (CPN2)

### TAXIWAYS A AND E

- SITE PLAN AND PROJECT CONTROL (SPL1)
- TYPICAL SECTIONS (TYP1)
- EXISTING CONDITIONS / PROPOSED REMOVALS (REM1)
- GRADING, MARKING AND LANDSCAPING PLAN - 1 (GRD1)
- GRADING, MARKING AND LANDSCAPING PLAN - 2 (GRD2)
- GRADING, MARKING AND LANDSCAPING PLAN - 3 (GRD3)
- ENGINEERING INFORMATION (ENG1)

### TERMINAL APRON

- SITE PLAN AND PROJECT CONTROL (SPL2)
- TYPICAL SECTIONS (TYP2)
- EXISTING CONDITIONS / PROPOSED REMOVALS (REM2)
- GRADING PLAN (GRD4)
- ELECTRICAL, MARKING AND LANDSCAPING PLAN (EML1)
- ENGINEERING INFORMATION (ENG2)
- ELECTRICAL DETAILS - 1 (EDL1)
- ELECTRICAL DETAILS - 2 (EDL2)
- MISCELLANEOUS DETAILS (DTL1)



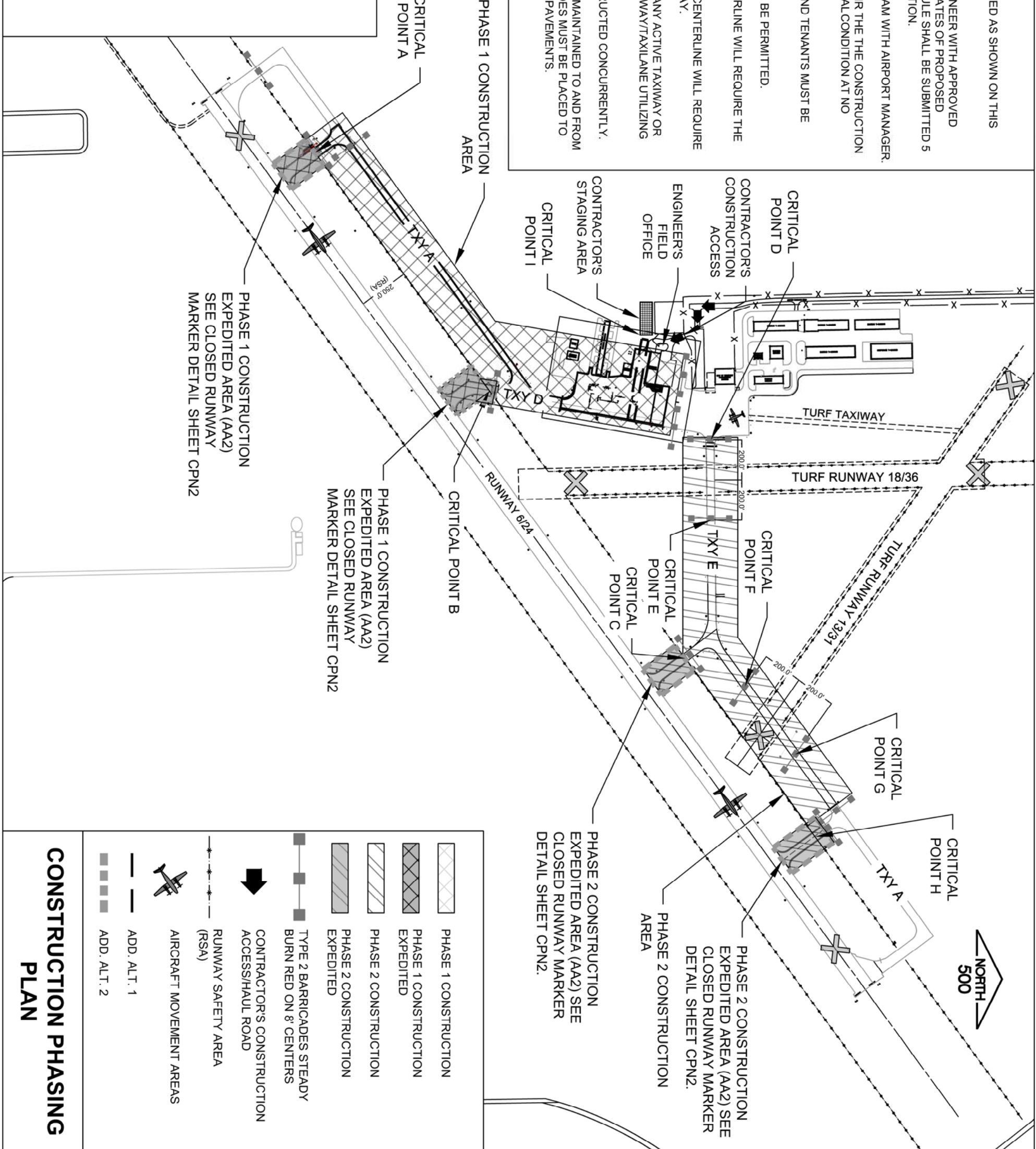
**GENERAL PHASING NOTES:**

1. CONTRACTOR STAGING AREA SHALL BE LOCATED AS SHOWN ON THIS SHEET.
2. CONTRACTOR SHALL PROVIDE RESIDENT ENGINEER WITH APPROVED PROGRESS SCHEDULE SHOWING START/ STOP DATES OF PROPOSED CONSTRUCTION. APPROVED PROGRESS SCHEDULE SHALL BE SUBMITTED 5 WORKING DAYS PRIOR TO START OF CONSTRUCTION.
3. RESIDENT ENGINEER SHALL COORDINATE NOTAM WITH AIRPORT MANAGER.
4. THE CONTRACTOR SHALL MAINTAIN AND REPAIR THE CONSTRUCTION ACCESS ROAD AND STAGING AREA IN ITS ORIGINAL CONDITION AT NO ADDITIONAL COST TO THE CONTRACT.
5. ACCESS TO THE APRON, EXISTING HANGARS AND TENANTS MUST BE MAINTAINED AT ALL TIMES.
6. SIMULTANEOUS RUNWAY CLOSURES WILL NOT BE PERMITTED.
7. ALL WORK WITHIN 250' OF RUNWAY 6/24 CENTERLINE WILL REQUIRE THE CLOSURE OF RUNWAY 6/24.
8. ALL WORK WITHIN 200' OF THE TURF RUNWAY CENTERLINE WILL REQUIRE THE CLOSURE OF THE ASSOCIATED TURF RUNWAY.
9. ALL WORK WITHIN 70' OF THE CENTERLINE OF ANY ACTIVE TAXIWAY OR TAXILANE WILL REQUIRE CLOSURE OF THAT TAXIWAY/TAXILANE UTILIZING BARRICADES AS DETAILED IN THE PLANS.
10. PHASE 1 AND PHASE 2 SHALL NOT BE CONSTRUCTED CONCURRENTLY.
11. DURING PHASE 2, TAXIWAY ACCESS MUST BE MAINTAINED TO AND FROM ACTIVE TURF RUNWAYS AT ALL TIMES. BARRICADES MUST BE PLACED TO SEPARATE WORK AREAS FROM ACTIVE AIRFIELD PAVEMENTS.
12. MAXIMUM EQUIPMENT HEIGHT = 25 FT.

**CRITICAL POINTS**

| POINT NO. | LATITUDE       | LONGITUDE      | ELEV*  |
|-----------|----------------|----------------|--------|
| A         | 42° 14' 28.82" | 89° 35' 20.67" | 876.00 |
| B         | 42° 14' 37.43" | 89° 35' 04.81" | 870.00 |
| C         | 42° 14' 46.96" | 89° 34' 47.04" | 864.50 |
| D         | 42° 14' 48.20" | 89° 35' 01.45" | 867.50 |
| E         | 42° 14' 48.22" | 89° 34' 56.13" | 865.50 |
| F         | 42° 14' 49.87" | 89° 34' 45.10" | 863.50 |
| G         | 42° 14' 52.30" | 89° 34' 40.56" | 864.00 |
| H         | 42° 14' 53.25" | 89° 34' 35.07" | 867.50 |
| I         | 42° 14' 44.77" | 89° 35' 08.45" | 872.00 |

\* ELEVATION INCLUDES 25' ANTICIPATED EQUIPMENT HEIGHT  
 DESIGN AIRCRAFT APPROACH CATEGORY: D  
 DESIGN AIRCRAFT GROUP: II



|  |   |
|--|---|
|  | PHASE 1 CONSTRUCTION                            |
|  | PHASE 1 CONSTRUCTION EXPEDITED                  |
|  | PHASE 2 CONSTRUCTION                            |
|  | PHASE 2 CONSTRUCTION EXPEDITED                  |
|  | TYPE 2 BARRICADES STEADY BURN RED ON 8' CENTERS |
|  | CONTRACTOR'S CONSTRUCTION ACCESS/HAUL ROAD      |
|  | RUNWAY SAFETY AREA (RSA)                        |
|  | AIRCRAFT MOVEMENT AREAS                         |
|  | ADD. ALT. 1                                     |
|  | ADD. ALT. 2                                     |

**CONSTRUCTION PHASING PLAN**

**FR039**



Freeport - Albertus Airport



Freeport, Illinois

City of Freeport

AIP: 3-17-0045-B24  
 FEP-4174

Rehab. Terminal Apron / Rehab. Taxiways A and E Phase 2

| Date | Description |
|------|-------------|
|      |             |
|      |             |
|      |             |

0 1  
 THIS BAR IS EQUAL TO 1" AT FULL SCALE (17 X 11).  
 DESIGN BY: CMT-RFD  
 DRAWN BY: CMT-RFD  
 CHECKED BY: CMT-RFD  
 APPROVED BY: DLP  
 DATE: 3-2-2012  
 JOB No.: 11294-02-00/06

**CONSTRUCTION PHASING PLAN (CPP1)**



**PHASING PLAN PROJECT NOTES:**

1. APPROVED PROGRESS SCHEDULE:  
 PRIOR TO THE START OF CONSTRUCTION, AN APPROVED PROGRESS SCHEDULE SHALL BE EXECUTED BY THE RESIDENT ENGINEER AND THE CONTRACTOR. THIS SCHEDULE SHALL SHOW START/ STOP DATES OF ALL PHASES, INCLUDING ALL AOA WORK REQUIRING DAYTIME ONLY CLOSURES OF TAXIWAYS AND RUNWAYS. THE APPROVED PROGRESS SCHEDULE SHALL BE DISTRIBUTED TO ALL PARTIES 3 WORKING DAYS PRIOR TO START OF CONSTRUCTION.

2. NOTAM (NOTICE TO AIRMEN) COORDINATION:  
 THE RESIDENT ENGINEER SHALL COORDINATE NOTAMS WITH THE AIRPORT.

3. CONSTRUCTION SITE ACCESS AND STAGING AREA:  
 THE CONTRACTOR ACCESS ROAD AND STAGING AREAS SHALL BE AS SHOWN ON THE REFERENCED PLAN. THE CONTRACTOR SHALL MAINTAIN AND REPAIR THE CONSTRUCTION ACCESS ROAD AND STAGING AREA IN ITS ORIGINAL CONDITION AT NO ADDITIONAL COST TO THE CONTRACT. ALTERNATE STAGING AREAS AND ACCESS FOR THIS AREA WILL NOT BE ALLOWED. THIS WORK AREA SHALL BE RESTRICTED FROM ACTIVE AOA AREAS WITH THE BARRICADE/FENCING PERIMETERS SHOWN.

THE ENTRANCE SHALL BE SIGNED ACCORDINGLY AS TO ALLOW ONLY CONSTRUCTION VEHICLE ACCESS AND WILL ONLY BE ACCESSIBLE DURING THE CONTRACTOR'S SCHEDULED WORK DAY. ALL SIGNAGE SHALL CONFORM TO IDOT CONSTRUCTION STANDARDS FOR VEHICLES ENTERING AND LEAVING THE SITE.

4. CONSTRUCTION EQUIPMENT FLAGGING / BEACON REQUIREMENTS:  
 THE CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON (FLASHING YELLOW) LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION.

5. GATE SECURITY:

THE GATE SHALL BE MAINTAINED, CLOSED AND LOCKED AS DIRECTED BY THE AIRPORT MANAGER. SHOULD THE CONTRACTOR'S OPERATIONS REQUIRE THE GATE TO REMAIN OPEN TO PROVIDE ACCESS TO HAULING OPERATIONS, A COMPETENT GATE GUARD SHALL BE REQUIRED TO CONTROL ACCESS TO THE AIRFIELD. A \$1,000 FINE SHALL BE ASSESSED FOR ANY OCCURRENCE OF AN UNSECURED GATE THAT IS THE CONTRACTOR'S RESPONSIBILITY. AN UNSECURED GATE SHALL BE DEFINED AS ANY GATE THAT IS NOT WITHIN THE SIGHT AND PHYSICAL CONTROL OF THE CONTRACTOR'S GUARD. IN THE EVENT THAT THE GATE MAY NOT BE SECURED, THE CONTRACTOR WILL BE CHARGED FOR AIRPORT PERSONNEL TO REMAIN AT THE GATE UNTIL SECURED.

6. CONSTRUCTION OUTSIDE OF BARRICADED AREAS REQUIRING TAXIWAY / RUNWAY CLOSURES:

WORK OUTSIDE THE BARRICADED LINES WITHIN THE ACTIVE SAFETY AREA OF AN ACTIVE RUNWAY/TAXIWAY SHALL REQUIRE TEMPORARY DAYTIME ONLY, UNLESS OTHERWISE AUTHORIZED BY THE RESIDENT ENGINEER. CLOSURES OF THE AFFECTED TAXIWAYS OR RUNWAYS. THIS WORK SHALL BE EXPEDITED AND PRIORITIZED TO MINIMIZE CLOSURE TIME OF THE ACTIVE PAVEMENTS. CONSTRUCTION ACTIVITIES WILL NOT BE PERMITTED WITHIN 250' WITHIN THE CENTERLINE OF ACTIVE RUNWAY 6/24 OR WITHIN 200' OF ACTIVE RUNWAYS 18/36 OR 13/31 AS SHOWN ON SHEET 3 OR WITHIN 70' OF THE CENTERLINE OF ALL ACTIVE TAXIWAYS. IN ADDITION, THIS WORK WILL REQUIRE ALL CREWS TO SUPPLY AND HAVE IN THEIR POSSESSION AT ALL TIMES AT LEAST ONE AIRPORT RADIO TO COMMUNICATE DIRECTLY WITH THE AIRPORT GROUND CONTROL FREQUENCY. THE OPERATOR OF THE AIRPORT RADIO SHALL BE FAMILIAR WITH AIRPORT RADIO PROCEDURES AND BE TUNED INTO THE GROUND CONTROL FREQUENCY AT ALL TIMES.

7. UNAUTHORIZED ACCESS TO AIRFIELD:

THE CONTRACTOR SHALL RESTRICT ALL CONSTRUCTION ACTIVITIES TO THE CONSTRUCTION AREA DETAILED IN THE PHASING PLAN. ANY UNAUTHORIZED MOVEMENTS, PEDESTRIAN OR VEHICULAR, BEYOND THE CONSTRUCTION LIMITS SHOWN SHALL BE CONSIDERED AN AIRFIELD INCURSION. AIRFIELD INCURSIONS, AT THE DISCRETION OF THE AIRPORT MANAGER, MAY BE FINED \$5,000.00 PER INCIDENT. INCURSION FINES WILL BE ASSESSED IMMEDIATELY AND TAKEN FROM MONIES DUE THE CONTRACTOR ON THE NEXT CONSTRUCTION PAYMENT.

**ANTICIPATED WORK SCHEDULE**

| PHASE | WORK AREA  | ALLOWABLE WORK PERIODS | AIRPORT OPERATIONAL RESTRICTIONS   | ANTICIPATED SCHEDULE                                      |
|-------|--|------------------------|--|---|
| 1     | TAXIWAY A EAST AND SOUTH TERMINAL APRON                                      | 7:00 A.M. - 7:00 P.M.  | MAINTAIN AIRFIELD ACCESS TO T-HANGAR TENANTS DURING CONSTRUCTION VIA TAXIWAY E, AND TAXIWAY A WEST. TAXIWAY CLOSURES INCLUDE TAXIWAY A EAST OF CONNECTOR TAXIWAY D, CONNECTOR TAXIWAY D AND CONNECTOR TAXIWAY C  | JUNE 08 - JULY 17, 2012 (40 CALENDAR DAYS)                |
| 1     | AREA WITHIN RUNWAY 7/25 RSA / CONNECTOR TAXIWAY C AND D                      | 7:00 A.M. - 7:00 P.M.  | MAINTAIN AIRFIELD ACCESS TO T-HANGAR TENANTS DURING CONSTRUCTION VIA TAXIWAY E, AND TAXIWAY A WEST. TAXIWAY CLOSURES INCLUDE TAXIWAY A EAST OF CONNECTOR TAXIWAY D, CONNECTOR TAXIWAY D AND CONNECTOR TAXIWAY C  | *8 CALENDAR DAYS<br>*6 CALENDAR DAYS                      |
| 2     | TAXIWAY E AND TAXIWAY A FROM CONNECTOR TAXIWAY F EAST TO CONNECTOR TAXIWAY G | 7:00 A.M. - 7:00 P.M.  | MAINTAIN AIRFIELD ACCESS TO T-HANGAR TENANTS DURING CONSTRUCTION VIA TAXIWAY D AND TAXIWAY A WEST OF CONNECTOR TAXIWAY D. TAXIWAY CLOSURES INCLUDE TAXIWAY E, TAXIWAY A EAST OF CONNECTOR TAXIWAY F TO CONNECTOR TAXIWAY G, CONNECTOR TAXIWAY F. WHEN WORK IS TO OCCUR WITHIN THE TURF RUNWAY SAFETY AREAS THE ASSOCIATED RUNWAY WILL BE CLOSED. CONCURRENT RUNWAY CLOSURES WILL NOT BE ALLOWED. | JULY 18 - AUGUST 7 (21 CALENDAR DAYS)                     |
| 2     | CONNECTOR TAXIWAY F AND G  | 7:00 A.M. - 7:00 P.M.  | MAINTAIN AIRFIELD ACCESS TO T-HANGAR TENANTS DURING CONSTRUCTION VIA TAXIWAY D AND TAXIWAY A WEST OF CONNECTOR TAXIWAY D. TAXIWAY CLOSURES INCLUDE TAXIWAY E, TAXIWAY A EAST OF CONNECTOR TAXIWAY F TO CONNECTOR TAXIWAY G, CONNECTOR TAXIWAY F AND G. RUNWAY 7/25 CLOSURE, DAYTIME CLOSURES ONLY  | **5 CALENDAR DAYS<br><br>61 CALENDAR DAYS (BASE BID ONLY) |
|       | EXPEDITED (ADDITIVE ALTERNATE #2)  |                        |  |   |

\* IF ADD ALT #1 IS AWARDED, 8 ADDITIONAL CALENDAR DAYS WILL BE AWARDED (TOTAL PROJECT CALENDAR DAYS = 69 ).  
 \*\* IF ADD ALT #2 IS AWARDED, 11 ADDITIONAL CALENDAR DAYS WILL BE AWARDED (TOTAL PROJECT CALENDAR DAYS = 72 )  
 SHOULD BOTH ADDITIVE ALTERNATES BE AWARDED, 19 ADDITIONAL CALENDAR DAYS WILL BE AWARDED (TOTAL PROJECT CALENDAR DAYS = 80 )

**PHASING PLAN GENERAL NOTES:**

1. SUGGESTED SEQUENCE OF CONSTRUCTION:

THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE NEW IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED WITH THE APPROVAL OF THE RESIDENT ENGINEER. HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT MANAGER.

2. STAGING AREA RESTORATION:

ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A STAGING AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING SEEDING /MULCHING OR CONSTRUCTING THESE PAVEMENTS / AREAS SHALL BE INCIDENTAL TO THE CONTRACT.

3. HAUL ROUTE /HAUL ROUTE RESTORATION:

THE CONTRACTOR SHALL USE THE DESIGNATED HAUL ROUTE AS SHOWN IN THESE PLANS. ALL HAUL ROUTE(S) SHALL BE INCIDENTAL TO THE CONTRACT. THE COST OF MAINTAINING THE HAUL ROUTE(S) SHALL BE INCIDENTAL TO THE CONTRACT. ALL HAUL ROUTE(S) INCLUDING EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING SEEDING /MULCHING OR CONSTRUCTING THESE HAUL ROUTE(S) SHALL BE INCIDENTAL TO THE CONTRACT.

4. AIRPORT APPROVAL OF PHASING:

THE RESIDENT ENGINEER AND AIRPORT MANAGER OR HIS DESIGNATED REPRESENTATIVE SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION PHASING PLAN AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT OPERATIONS. AIRCRAFT OPERATIONS HAVE THE RIGHT-OF-WAY ON THE AIRFIELD. VEHICULAR TRAFFIC AND CONTRACTOR ACTIVITIES SHALL YIELD TO AIRCRAFT OPERATIONS. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT AT ANY TIME TO ALLOW AN AIRCRAFT TO PASS, THE CONTRACTOR SHALL DO SO IMMEDIATELY AT NO EXTRA COST TO THE OWNER.

5. AIRFIELD PAVEMENT / SITE DEBRIS REMOVAL:

THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING RUNWAYS AND TAXIWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER. SHOULD THE CONTRACTOR TRACK ANY DEBRIS ONTO EXISTING PAVEMENTS, THIS DEBRIS SHALL BE REMOVED IMMEDIATELY WITH A PICK UP SWEEPER. A PICK UP SWEEPER SHALL BE REQUIRED TO BE ON SITE AND OPERATE DURING ALL CONSTRUCTION OPERATION WORKING HOURS. THE CONTRACTOR SHALL PROVIDE WASTE RECEPTACLES THROUGHOUT THE WORK ZONE AND MAINTAIN SANITARY FACILITIES FOR EMPLOYEES TO USE. FACILITIES WITHIN THE HANGARS/AIRPORT BUILDINGS SHALL NOT BE USED.

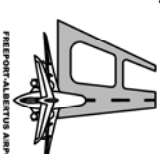
(NOTES CONTINUED ON PHASING PLAN DETAILS - CPN2)

**FR039**



GRAVFORO, MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS  
OWNER'S CONSULTANT

**Freeport - Albetus Airport**



**Freeport, Illinois**

**City of Freeport**

**AIP: 3-17-0045-B24  
 FEP-4174**

**Rehab. Terminal  
 Apron /  
 Rehab. Taxiways A  
 and E Phase 2**

**Revisions**

| Date | Description |
|------|-------------|
|      |             |
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0 1  
 THIS BAR IS EQUAL TO 1"  
 AT FULL SCALE (17 X 11).

|              |                |
|--------------|----------------|
| DESIGN BY:   | CMT - RFD      |
| DRAWN BY:    | CMT - RFD      |
| CHECKED BY:  | CMT - RFD      |
| APPROVED BY: | DLP            |
| DATE:        | 3-2-2012       |
| JOB No:      | 11294-02-00/06 |

**CONSTRUCTION PHASING PLAN  
 NOTES - 1  
 (CPN1)**

**4**

SHEET 4 OF 21 SHEETS

**CONSTRUCTION PHASING PLAN**

**NOTES - 1**



(NOTES CONTINUED ON PHASING PLAN NOTES - PPN1)

6. PROJECT LIGHTING OUTSIDE OF DAYLIGHT HOURS:  
 WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVEABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY.

7. EXISTING UTILITY COORDINATION:

COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. SEE SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES, PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY/OWNER OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

SHOULD A UTILITY COMPANY OR GOVERNMENT AGENCY BE UNABLE TO LOCATE FACILITIES, THE CONTRACTOR SHALL LOCATE THESE FACILITIES. PAYMENT FOR THIS LOCATION SHALL BE INCIDENTAL TO THE IMPROVEMENTS REQUIRING THE LOCATIONS.

8. TRAFFIC CONTROL PAYMENT:

PAYMENT FOR ALL AIRSIDE AND ROADWAY TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO, TEMPORARY CONSTRUCTION FENCING, BARRICADES, SIGNING, AIR OPERATIONS AREA (A.O.A.) LATH AND RIBBON, ETC. SHALL BE PAID CONSIDERED INCIDENTAL TO THE CONTRACT. LOW PROFILE BARRICADES SHALL BE USED ON ALL PAVEMENT AREAS OR AS DIRECTED BY THE ENGINEER. WHEN NOT IN USE, THESE BARRICADES SHALL BE STORED AT THE CONTRACTOR'S STAGING AREA OR OFF SITE. ACCESS TO THE ACTIVE RUNWAY AND TAXIWAY PAVEMENTS SHALL BE SIGNED WITH STOP SIGNS MOUNTED ON TYPE II BARRICADES (2 EACH, RIGHT AND LEFT) AT THE ENTRANCE. IN ADDITION TO THE STOP SIGNS, WARNING SIGNS (2 EACH, RIGHT AND LEFT) SHALL BE MOUNTED. WARNING SIGNS SHALL STATE "SAFETY AREA / UNAUTHORIZED ACCESS SUBJECT TO FINE." ALL TYPE II BARRICADES SHALL CONFORM TO IDOT STANDARD DETAIL 702001. ALL PAVEMENT DROP-OFFS GREATER THAN 24" REQUIRE TYPE II BARRICADES WITH EXTENDED LEGS. FOR AIRSIDE BARRICADE PLACEMENT, SEE SEQUENCE OF CONSTRUCTION SHEETS.

9. DRIVERS TRAINING:

DRIVERS TRAINING SHALL BE REQUIRED FOR THE CONTRACTOR'S SUPERVISORY PERSONNEL. OTHER CONSTRUCTION PERSONNEL CAN BE WITHIN THE AIRFIELD LIMITS PROVIDED THAT THEY ARE UNDER ESCORT AND IN THE PRESENCE OF AN AUTHORIZED SUPERVISOR. THE DRIVER'S TRAINING OF THE INITIAL SUPERVISORY PERSONNEL MUST BE COMPLETED PRIOR TO THE START OF CONSTRUCTION.

10. DUST CONTROL REQUIREMENTS:

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE DUST CONTROL AT ALL TIMES DURING THE PROJECT DURATION. A WATER TRUCK SHALL BE REQUIRED TO BE ON SITE DURING ALL CONSTRUCTION OPERATION WORKING HOURS, UNLESS WAIVED BY THE AIRPORT MANAGER. PAYMENT FOR DUST CONTROL SHALL BE INCIDENTAL TO THE CONTRACT.

11. OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION (AC 150/5370-2F):

ALL WORK SHALL CONFORM TO AC 150/5370-2F (LATEST EDITION) OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION. THIS AC IS AVAILABLE AT [www.faa.gov/arp/pdf/15370-2f.pdf](http://www.faa.gov/arp/pdf/15370-2f.pdf).

12. STAGING AREA:

THE CONTRACTOR'S MATERIAL AND EQUIPMENT, WHEN NOT IN USE, SHALL BE STORED IN THE CONTRACTOR'S STAGING AREA. ALL DELIVERIES, EQUIPMENT REFUELING, EQUIPMENT MAINTENANCE AND EQUIPMENT TRANSFERS SHALL TAKE PLACE WITHIN THE CONTRACTOR'S STAGING AREA.

13. AIRFIELD LIGHTING COORDINATION:

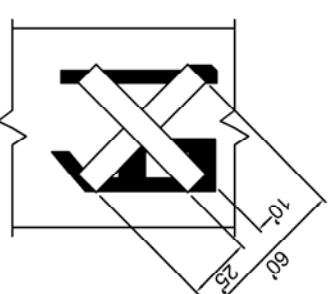
THE CONTRACTOR SHALL BE REQUIRED TO ESTABLISH A COORDINATION PLAN WITH THE AIRPORT MANAGER OR HIS DESIGNATED REPRESENTATIVE, REGARDING DE-ENERGIZING AND ENERGIZING OF THE AIRFIELD LIGHTING CIRCUITS AT THE START AND END OF EACH CONSTRUCTION DAY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL AIRPORT OWNED UTILITIES AND SHALL BE DONE SO AT NO EXTRA COST TO THE CONTRACT.

14. WEEKLY COORDINATION MEETINGS:

WEEKLY COORDINATION MEETINGS SHALL BE REQUIRED TO DISCUSS PROJECT PROGRESS. AS A MINIMUM, PROJECT SCHEDULE AND GATE VISITOR LOGS SHALL BE DISCUSSED. REPRESENTATION BY THE PRIME CONTRACTOR IS MANDATORY.

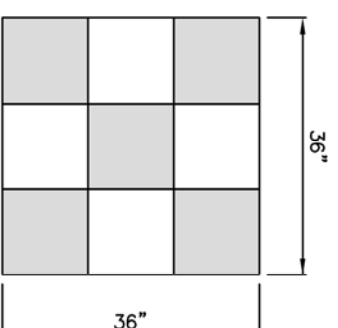
15. TEMPORARY CABLES:

ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER AND AIRPORT MANAGER FOR ALL PHASES. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.



**CLOSED RUNWAY MARKER DETAIL**  
ON PAVEMENT - NO SCALE

1. CLOSED RUNWAY MARKERS SHALL BE YELLOW.
2. MARKERS SHALL BE A MATERIAL APPROVED BY THE ENGINEER AND THE AIRPORT.
3. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
4. MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
5. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
6. DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT AND TOWER PERSONNEL. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING A YELLOW CROSS AT THE LOCATION AND DIMENSIONS DETAILED ON THIS SHEET OR AS DIRECTED BY THE ENGINEER AND AIRPORT. THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.
7. FOR TURF RUNWAYS, ENDS 36 AND 31, PLACE CLOSED RUNWAY MARKER OUTSIDE RUNWAY 6/24 RSA (250' FROM CENTERLINE) AS SHOWN ON SHEET 3.



**CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG**  
NOT TO SCALE

**FR039**



Freeport - Albertus Airport



Freeport, Illinois

City of Freeport

AIP: 3-17-0045-B24  
FEP-4174

Rehab. Terminal Apron / Rehab. Taxiways A and E Phase 2

Revisions

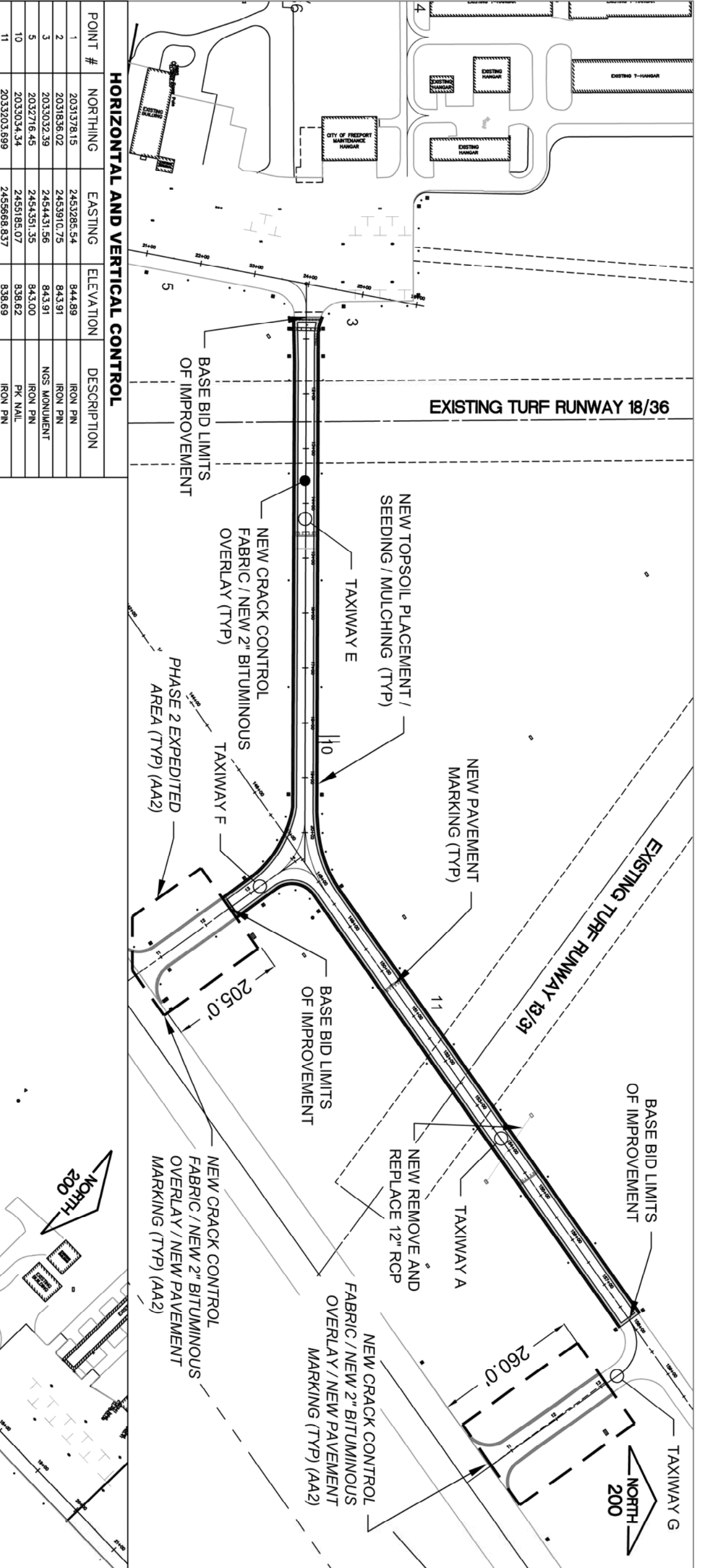
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| APPROVED BY: | DLP            |
| DATE:        | 3-2-2012       |
| JOB No:      | 11294-02-00/06 |

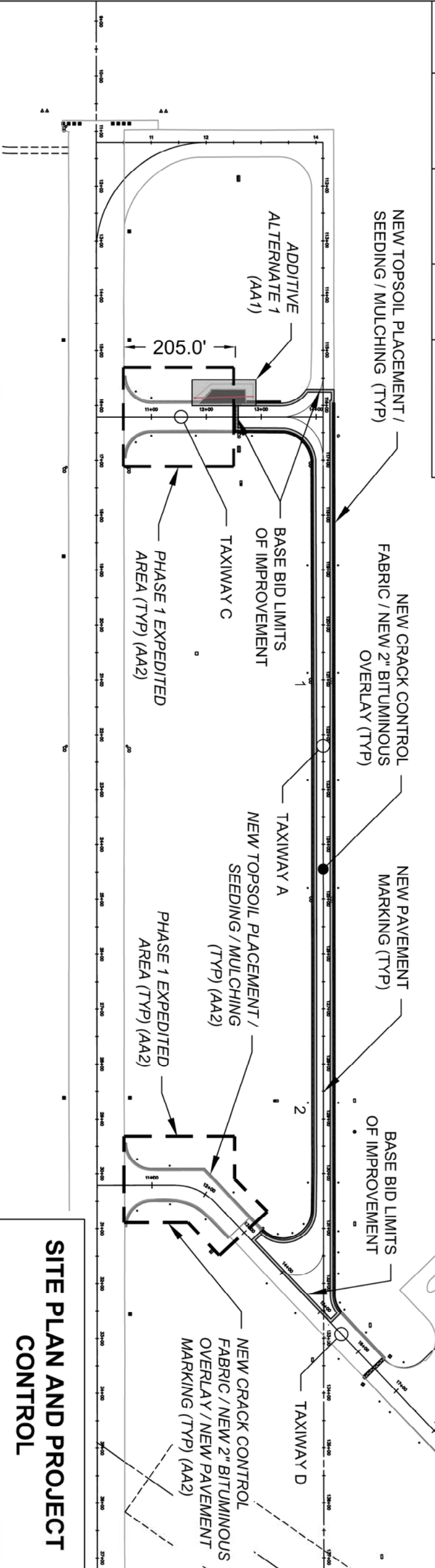
**CONSTRUCTION PHASING PLAN NOTES - 2 (CPN2)**

**CONSTRUCTION PHASING PLAN NOTES - 2**



**HORIZONTAL AND VERTICAL CONTROL**

| POINT # | NORTHING    | EASTING     | ELEVATION | DESCRIPTION  |
|---------|-------------|-------------|-----------|--------------|
| 1       | 2031378.15  | 2453285.54  | 844.89    | IRON PIN     |
| 2       | 2031836.02  | 2453910.75  | 843.91    | IRON PIN     |
| 3       | 2033032.39  | 2454431.56  | 843.91    | NGS MONUMENT |
| 5       | 2032716.45  | 2454351.35  | 843.00    | IRON PIN     |
| 10      | 2033034.34  | 2455185.07  | 838.62    | PK NAIL      |
| 11      | 2033203.699 | 2455668.837 | 838.69    | IRON PIN     |



FR039



Freeport - Albertus Airport  
 Freeport, Illinois  
 City of Freeport  
 AIP: 3-17-0045-B24  
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 Rehab. Terminal Apron / Rehab. Taxiways A and E Phase 2

Revisions

| Date | Description |
|------|-------------|
|      |             |

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 CHECKED BY: CMT - RFD  
 APPROVED BY: DLP  
 DATE: 3-2-2012  
 JOB NO.: 11294-02-00/06

**SITE PLAN AND PROJECT CONTROL (SPL1)**







ADJUST UNDERDRAIN CLEANOUT  
 PROP. RIM = 849.20  
 EXISTING RIM = 848.74

NEW 4' WIDE TOPSOIL  
 PLACEMENT / SEEDING  
 AND MULCHING (TYP)

ADJUST UNDERDRAIN CLEANOUT  
 PROP. RIM = 845.42  
 EXISTING RIM = 845.15

ADJUST UNDERDRAIN CLEANOUT  
 PROP. RIM = 848.28  
 EXISTING RIM = 847.97

TAXIWAY A

ADJUST UNDERDRAIN CLEANOUT  
 PROP. RIM = 845.42  
 EXISTING RIM = 845.18

NEW 12" WIDE YELLOW TAXIWAY  
 MARKING W/6" BLACK BORDER  
 (TYP)

ADJUST UNDERDRAIN CLEANOUT  
 PROP. RIM = 850.46  
 EXISTING RIM = 850.56

NEW RUNWAY HOLDING  
 POSITION MARKING (TYP)

ADJUST UNDERDRAIN CLEANOUT  
 PROP. RIM = 850.88  
 EXISTING RIM = 850.98

ADJUST UNDERDRAIN CLEANOUT  
 PROP. RIM = 845.01  
 EXISTING RIM = 844.81

NEW 12" WIDE YELLOW TAXIWAY  
 MARKING W/6" BLACK BORDER  
 (TYP)

ADJUST UNDERDRAIN CLEANOUT  
 PROP. RIM = 845.01  
 EXISTING RIM = 844.75





NEW 4' WIDE TOPSOIL  
 PLACEMENT / SEEDING  
 AND MULCHING (TYP)

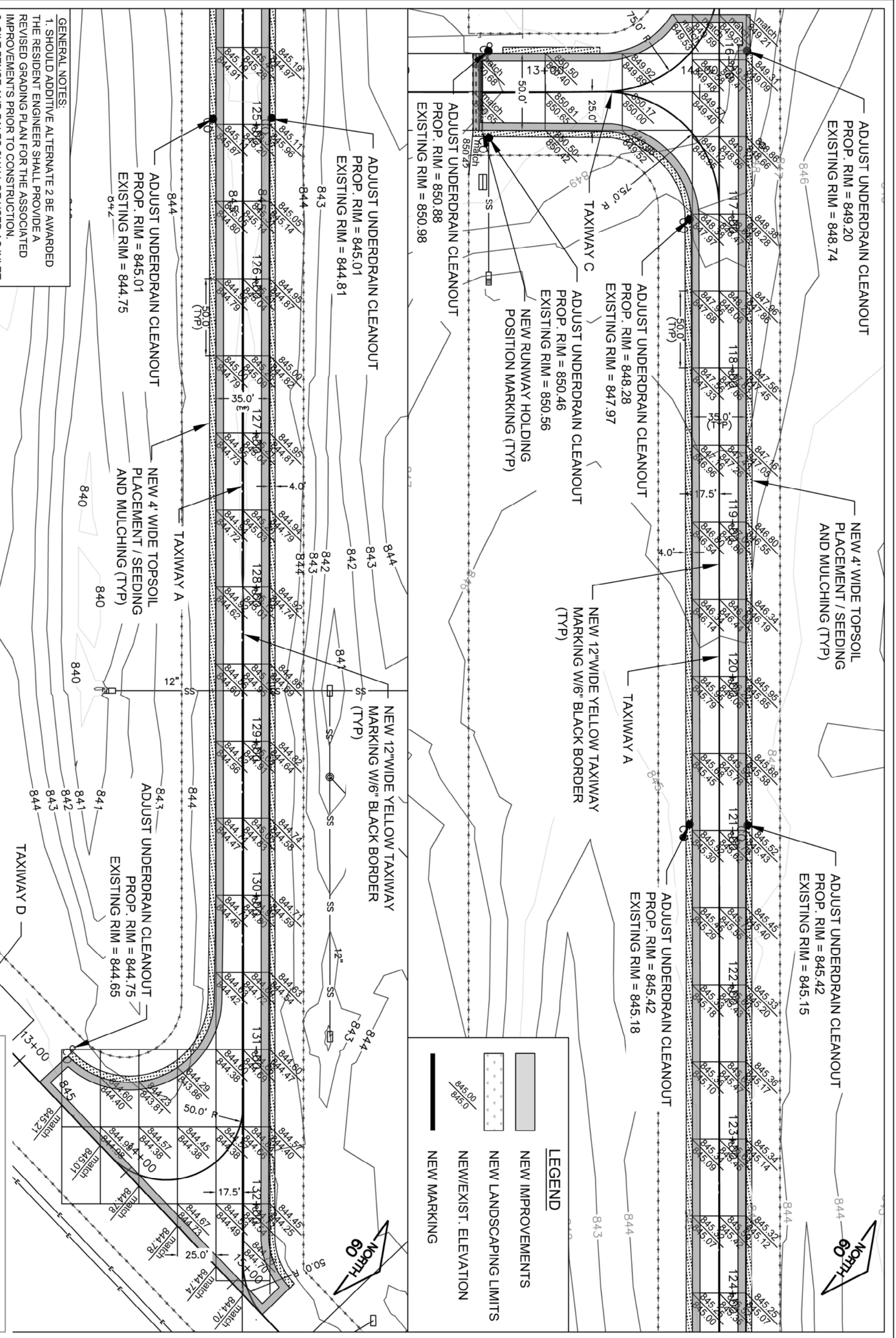
ADJUST UNDERDRAIN CLEANOUT  
 PROP. RIM = 844.75  
 EXISTING RIM = 844.65

GENERAL NOTES:  
 1. SHOULD ADDITIVE ALTERNATE 2 BE AWARDED  
 THE RESIDENT ENGINEER SHALL PROVIDE A  
 REVISED GRADING PLAN FOR THE ASSOCIATED  
 IMPROVEMENTS PRIOR TO CONSTRUCTION.  
 2. SILT FENCE AND BALES SHALL BE USED AS INLET  
 PROTECTION. ITEMS SHALL PLACED AT LOCATIONS,  
 ADJACENT TO THE PROPOSED IMPROVEMENTS AS  
 DIRECTED BY THE RESIDENT ENGINEER.

**GRADING PLAN - 1**

**LEGEND**

-  NEW IMPROVEMENTS
-  NEW LANDSCAPING LIMITS
-  NEW/EXIST. ELEVATION
-  NEW MARKING



**FR039**



CRANFORD, MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS  
 CMT/CMT INC.

Freeport, Illinois  
 City of Freeport  
 AIP: 3-17-0045-B24  
 FEP-4174

Rehab. Terminal  
 Apron /  
 Rehab. Taxiways A  
 and E Phase 2

Revisions

| Date | Description |
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 CHECKED BY: CMT - RFD  
 APPROVED BY: DLP  
 DATE: 3-2-2012  
 JOB NO.: 11294-02-00106

**GRADING, MARKING AND LANDSCAPING PLAN - 1 (GRD1)**

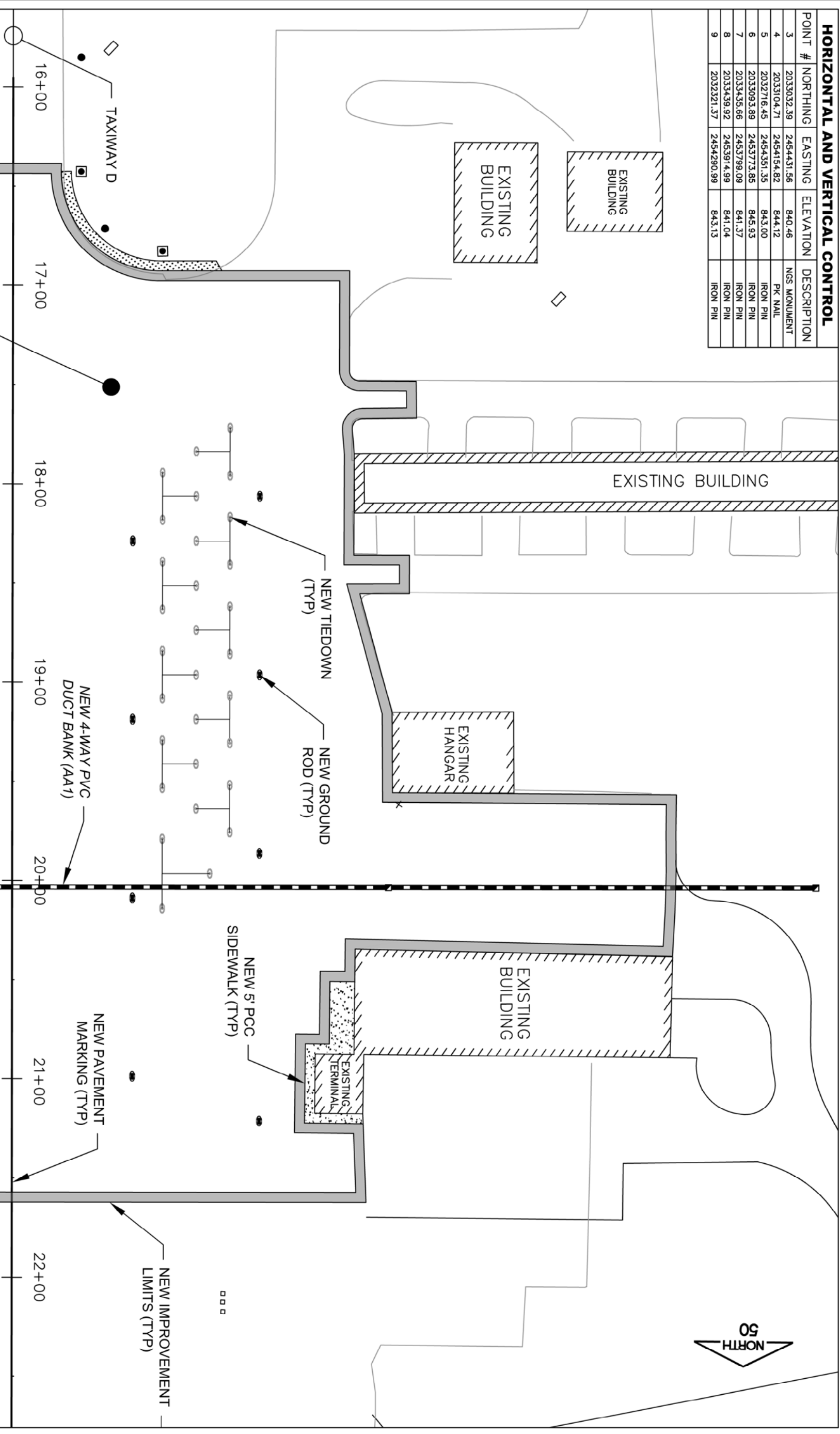








| POINT # | NORTHING   | EASTING    | ELEVATION | DESCRIPTION  |
|---------|------------|------------|-----------|--------------|
| 3       | 2033032.39 | 2454431.56 | 840.46    | NGS MONUMENT |
| 4       | 2033104.71 | 2454154.82 | 844.12    | PK NAIL      |
| 5       | 2032716.45 | 2454351.35 | 843.00    | IRON PIN     |
| 6       | 2033093.89 | 2453773.85 | 845.93    | IRON PIN     |
| 7       | 2033435.66 | 2453799.09 | 841.37    | IRON PIN     |
| 8       | 2033439.92 | 2453914.99 | 841.04    | IRON PIN     |
| 9       | 2032321.37 | 2454290.99 | 843.13    | IRON PIN     |



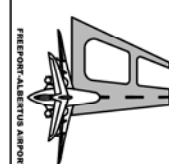
**SITE PLAN AND PROJECT CONTROL**

**FR039**



GRAFFORD, MURPHY & TILLY, INC.  
CONSULTING ENGINEERS  
OWNER/CMT, INC.

Freeport - Albertus Airport



Freeport, Illinois  
City of Freeport

AIP: 3-17-0045-B24  
FEP-4174

Rehab. Terminal  
Apron /  
Rehab. Taxiways A  
and E Phase 2

| Date | Description |
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| APPROVED BY: | DLP            |
| DATE:        | 3-2-2012       |
| JOB No.:     | 11294-02-00106 |

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**SITE PLAN AND PROJECT CONTROL (SPL2)**

**13**



**FR039**



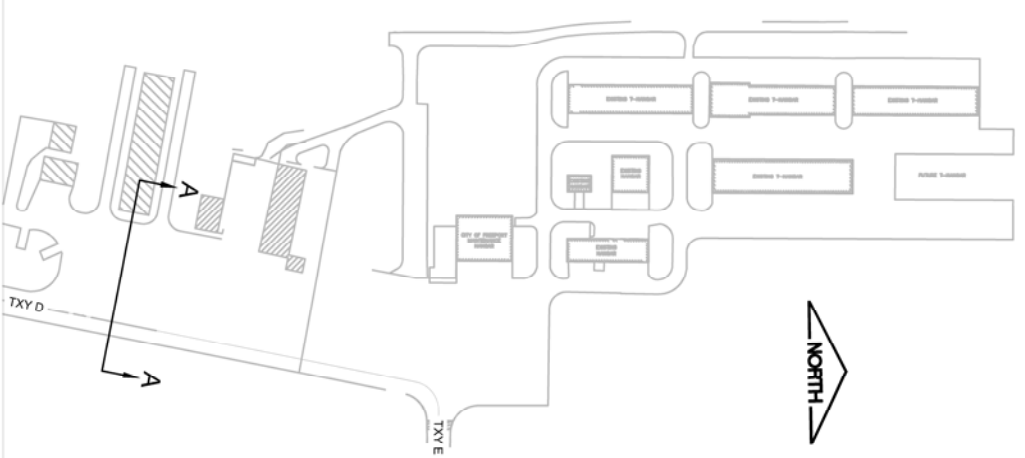
Freepoint - Albertus Airport



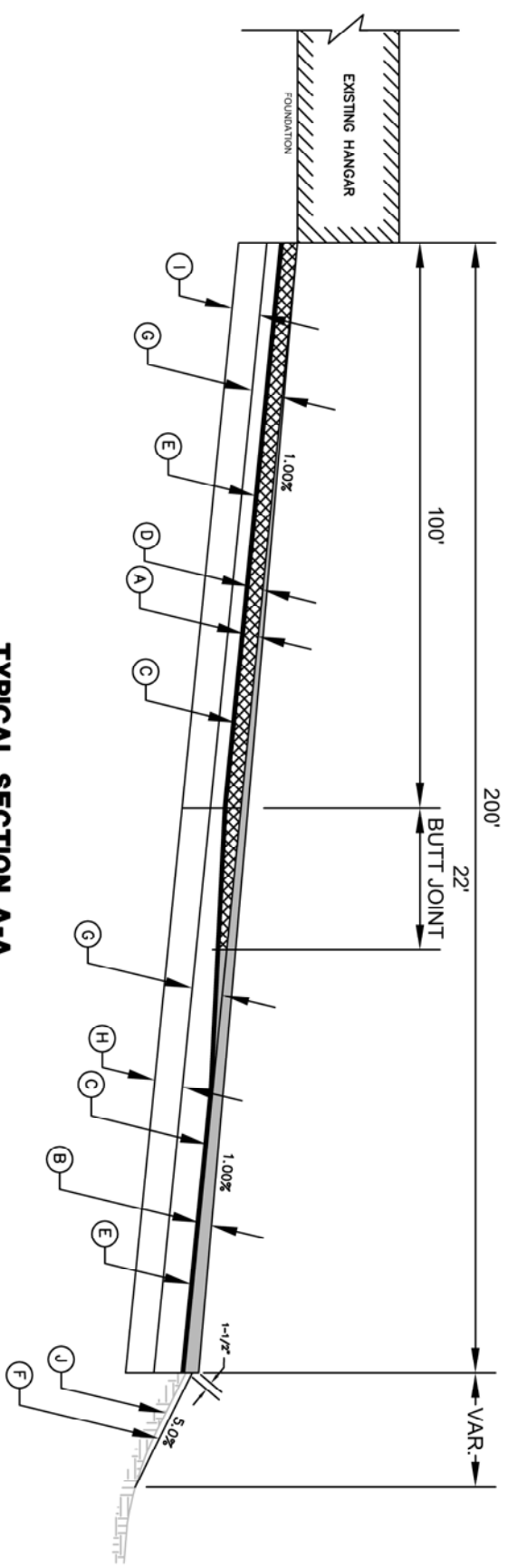
Freepoint, Illinois  
City of Freepoint

AIP: 3-17-0045-B24  
FEP-4174

Rehab. Terminal  
Apron /  
Rehab. Taxiways A  
and E Phase 2

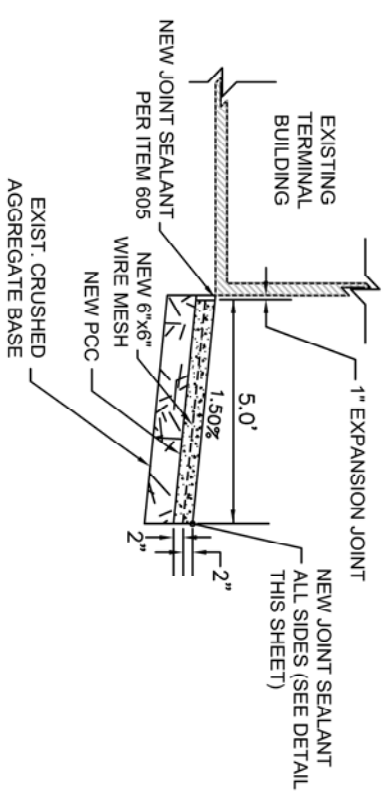


**TYPICAL SECTION A-A**  
(TERMINAL APRON)

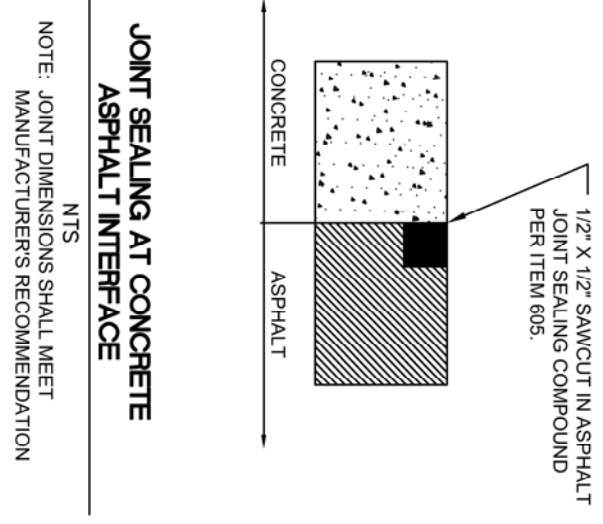


**TYPICAL SECTION A-A LEGEND**

- (A) PROPOSED 2-1/2" LFTS BITUMINOUS SURFACE COURSE (401)
  - (B) PROPOSED VAR. DEPTH BITUMINOUS SURFACE COURSE (401)
  - (C) PROPOSED BITUMINOUS TACK COAT (603)
  - (D) PROPOSED 3" BITUMINOUS MILLING (401)
  - (E) PROPOSED CRACK CONTROL FABRIC (201)
  - (F) PROPOSED TOPSOILING (905)
  - (G) EXISTING 5 1/2"-7 1/2" BITUMINOUS SURFACE COURSE
  - (H) EXISTING 6" BITUMINOUS BASE COURSE
  - (I) EXISTING 6" SOIL CEMENT BASE
  - (J) EXISTING GROUND LINE
- NOTE: A BITUMINOUS TACK COAT (603) IS REQUIRED BETWEEN ALL LFTS OF BITUMINOUS SURFACE COURSE (401)**



**6\"**



**JOINT SEALING AT CONCRETE  
ASPHALT INTERFACE**

NOTE: JOINT DIMENSIONS SHALL MEET  
MANUFACTURER'S RECOMMENDATION

**TYPICAL SECTIONS**

**TYPICAL  
SECTIONS  
(TYP2)**

**14**



Freeport - Albertus Airport

Freeport, Illinois  
City of Freeport

AIP: 3-17-0045-B24  
FEP-4174

Rehab. Terminal  
Apron /  
Rehab. Taxiways A  
and E Phase 2

| Date | Description |
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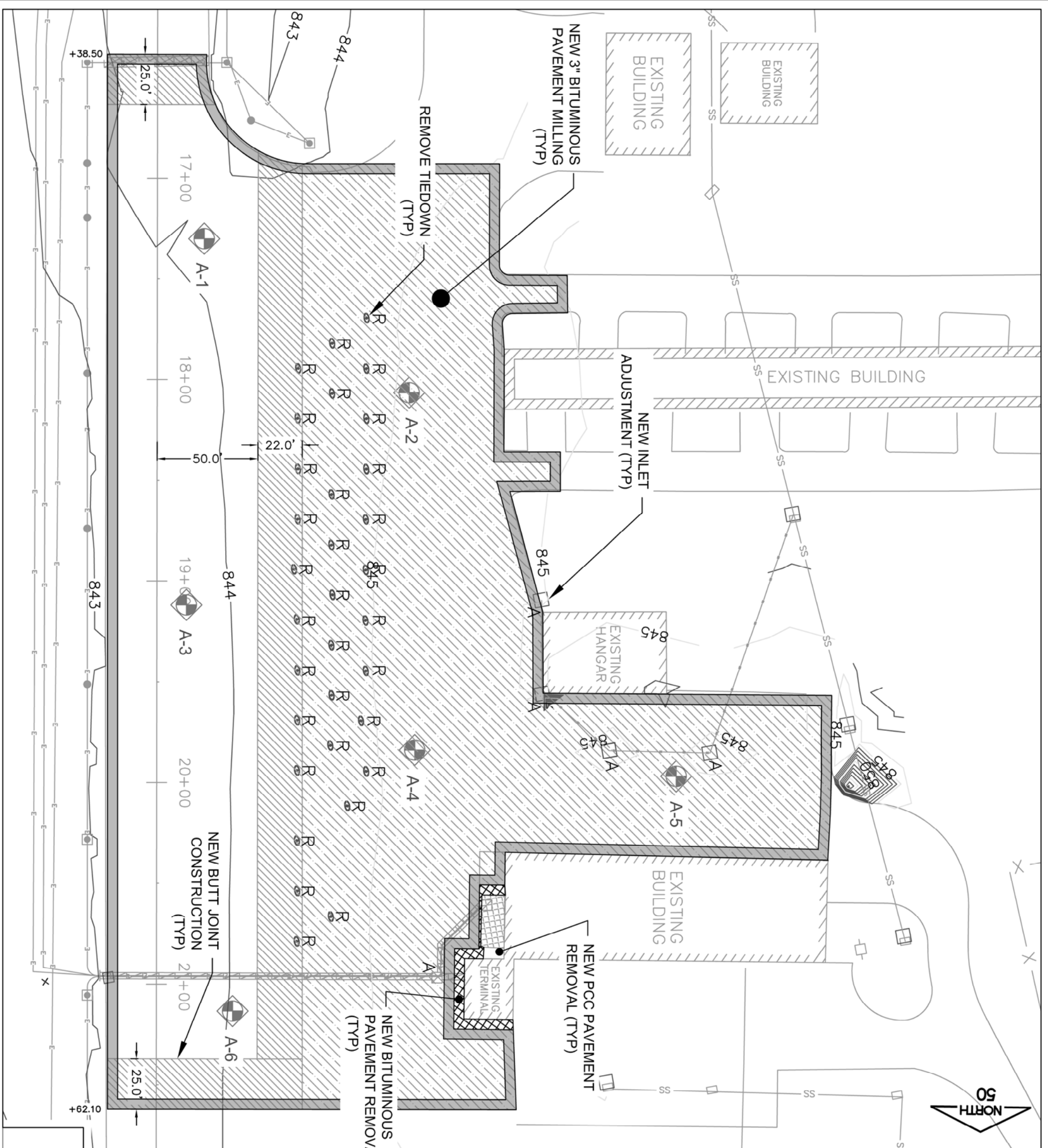
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APPROVED BY: DLP  
DATE: 3-2-2012  
JOB No: 11294-02-00106

**EXISTING**  
CONDITIONS /  
PROPOSED  
REMOVALS  
(REM2)

**LEGEND**





- BIT. PAVEMENT MILLING (3")
- BIT. PAVEMENT REMOVAL
- BUTT JOINT CONSTRUCTION
- EXISTING AIRFIELD CIRCUIT
- EXIST. DUCT
- EXIST. STORM SEWER
- EXIST. GAS MAIN
- EXIST. FENCE
- EXIST. PAVEMENT MARKING
- EXIST. MITL, STAKE-MOUNTED
- EXIST. MITL, BASE-MOUNTED
- EXIST. ELECTRICAL HANDHOLE
- EXIST. DRAINAGE INLET
- EXIST. TIEDOWN
- EXIST. ITEM TO BE REMOVED
- EXIST. ITEM TO BE ADJUSTED

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS AND FIELD SURVEYS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WITH RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION PRESENTED HEREIN AND THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

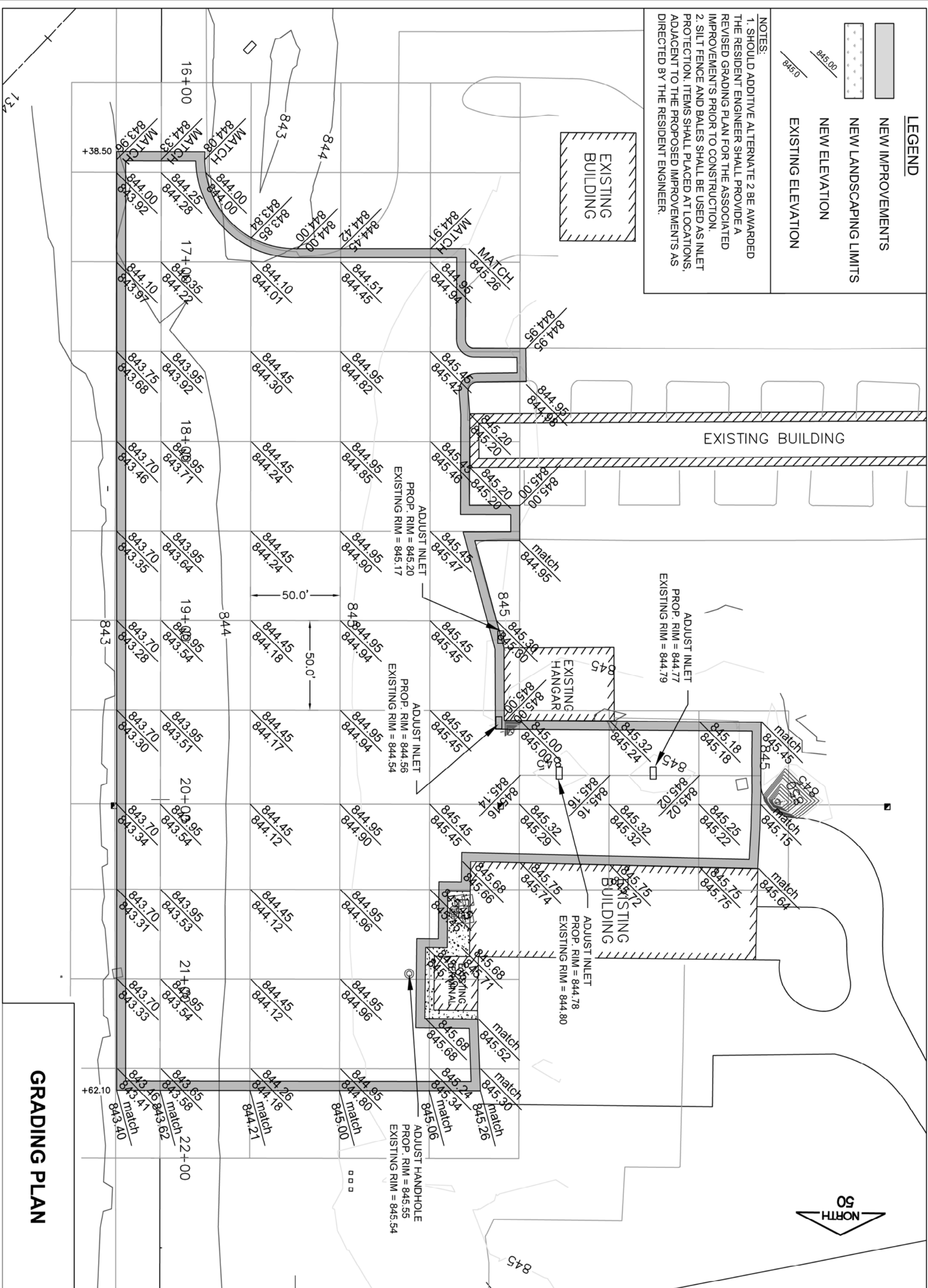


**EXISTING CONDITIONS /  
PROPOSED REMOVALS**

**LEGEND**

-  NEW IMPROVEMENTS
-  NEW LANDSCAPING LIMITS
-  NEW ELEVATION
-  EXISTING ELEVATION

**NOTES:**  
 1. SHOULD ADDITIVE ALTERNATE 2 BE AWARDED THE RESIDENT ENGINEER SHALL PROVIDE A REVISED GRADING PLAN FOR THE ASSOCIATED IMPROVEMENTS PRIOR TO CONSTRUCTION.  
 2. SILT FENCE AND BALES SHALL BE USED AS INLET PROTECTION. ITEMS SHALL PLACED AT LOCATIONS, ADJACENT TO THE PROPOSED IMPROVEMENTS AS DIRECTED BY THE RESIDENT ENGINEER.



**GRADING PLAN**

**FR039**



**Freepoint - Albertus Airport**  
 City of Freepoint  
 AIP: 3-17-0045-B24  
 FEP-4174  
 Rehab. Terminal Apron / Rehab. Taxiways A and E Phase 2

| Date | Description |
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 CHECKED BY: CMT-RFD  
 APPROVED BY: DLP  
 DATE: 3-2-2012  
 JOB No.: 11294-02-00106

**GRADING PLAN (GRD4)**



FR039



Freeport - Albertus Airport  
 City of Freeport  
 AIP: 3-17-0045-B24  
 FEP-4174  
 Rehab. Terminal Apron / Rehab. Taxiways A and E Phase 2

Rehab. Terminal Apron / Rehab. Taxiways A and E Phase 2

| Date | Description |
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







Revisions

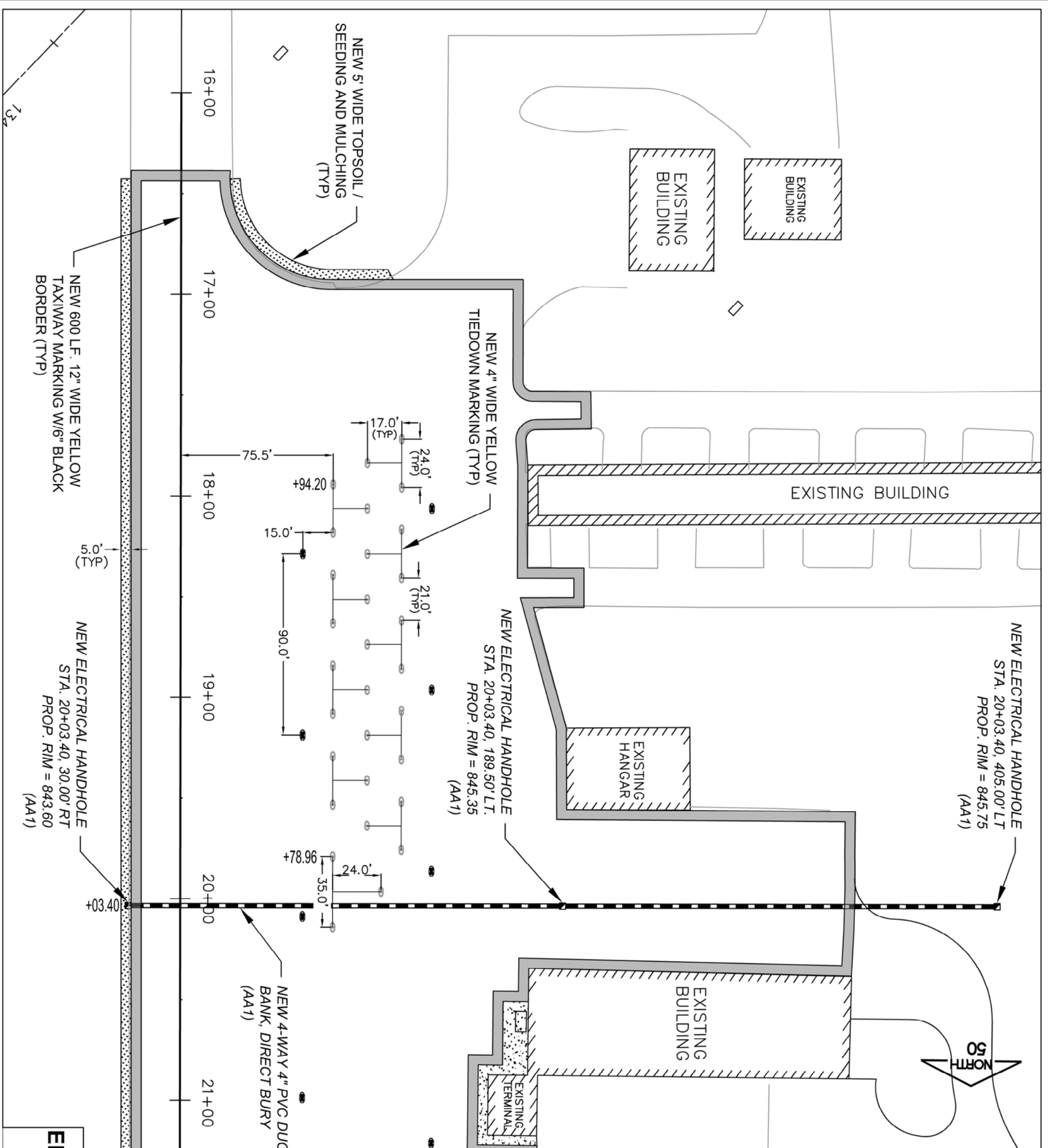
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 APPROVED BY: DLP  
 DATE: 3-2-2012  
 JOB NO: 11294-02-00/06

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**ELECTRICAL, MARKING AND LANDSCAPING PLAN (EML1)**

**LEGEND**

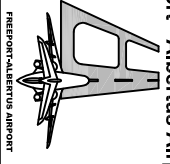
-  EXISTING CIRCUITS
-  NEW PAVEMENT MARKING
-  NEW ELECTRICAL HANDHOLE (AA1)
-  EXISTING HANDHOLE TO REMAIN
-  NEW 4-WAY CONCRETE ENCASED DUCT (AA1)
-  EXISTING CONDUIT/DUCT
-  NEW TIEDOWN
-  NEW GROUND ROD
-  EXISTING STAKE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
-  NEW SEEDING AND MULCHING



**ELECTRICAL, MARKING AND LANDSCAPING PLAN**



Freeport - Albertus Airport



Freeport, Illinois  
City of Freeport

AIP: 3-17-0045-B24  
FEP-4174

Rehab. Terminal Apron / Rehab. Taxiways A and E Phase 2

| Revisions |             |
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| Date      | Description |
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CHECKED BY: CMT-RFD  
APPROVED BY: DLP  
DATE: 3-2-2012  
JOB No.: 11294-02-00/06

ENGINEERING INFORMATION (ENG2)

| Pavement Core No. | Location                    | Pavement Material                        | Thickness, inches |
|-------------------|-----------------------------|--|-------------------|
| A-1               | South End of Terminal Apron | Hot-Mix Asphalt Surface & Binder Courses | 8.1               |
| A-2               | Terminal Apron, South Half  | No Granular Base Course Encountered      | -                 |
| A-2               | Terminal Apron, South Half  | Hot-Mix Asphalt Surface Course           | 5.8               |
| A-2               | Terminal Apron, South Half  | No Granular Base Course Encountered      | -                 |

| Pavement Core No. | Location                             | Pavement Material                        | Thickness, inches |
|-------------------|--------------------------------------|--|-------------------|
| A-3               | Terminal Apron, South Half           | Hot-Mix Asphalt Surface & Binder Courses | 8.2               |
| A-4               | Terminal Apron, South Half           | Crushed Stone Granular Base Course       | 5.8               |
| A-4               | Terminal Apron, South Half           | Hot-Mix Asphalt Surface Course           | 5.5               |
| A-5               | Terminal Apron, South Half, West End | No Granular Base Course Encountered      | -                 |
| A-5               | Terminal Apron, South Half, West End | Hot-Mix Asphalt Surface & Binder Courses | 6.2               |
| A-6               | Terminal Apron, North Half           | No Granular Base Course Encountered      | -                 |
| A-6               | Terminal Apron, North Half           | Hot-Mix Asphalt Surface & Binder Courses | 8.0               |
| A-7               | Terminal Apron, North Half           | No Granular Base Course Encountered      | -                 |
| A-7               | Terminal Apron, North Half           | Hot-Mix Asphalt Surface Course           | 9.0               |
| A-7               | Terminal Apron, North Half           | No Granular Base Course Encountered      | -                 |

| Pavement Core No. | Location                              | Pavement Material                  | Thickness, inches |
|-------------------|---------------------------------------|------------------------------------|-------------------|
| A-8               | Terminal Apron, North Half            | Hot-Mix Asphalt Surface Course     | 4.2               |
| A-9               | Terminal Apron, North Half, North End | Crushed Stone Granular Base Course | 11.8              |
| A-9               | Terminal Apron, North Half, North End | Hot-Mix Asphalt Surface Course     | 5.2               |
| A-10              | Terminal Apron, North Half            | Crushed Stone Granular Base Course | 7.8               |
| A-10              | Terminal Apron, North Half            | Hot-Mix Asphalt Surface Course     | 4.6               |
| A-11              | Terminal Apron, North Half            | Crushed Stone Granular Base Course | 5.4               |
| A-11              | Terminal Apron, North Half            | Hot-Mix Asphalt Surface Course     | 2.8               |
| A-12              | South T- Hangar Taxiway, East End     | Crushed Stone Granular Base Course | 5.2               |
| A-12              | South T- Hangar Taxiway, East End     | Hot-Mix Asphalt Surface Course     | 3.8               |
| A-12              | South T- Hangar Taxiway, East End     | Crushed Stone Granular Base Course | 4.2               |

| Pavement Core No. | Location  | Pavement Material                  | Thickness, inches |
|-------------------|---|------------------------------------|-------------------|
| A-13              | Middle South T-Hangar, East Side Taxiway, South End | Hot-Mix Asphalt Surface Course     | 4.5               |
| A-14              | Middle South T-Hangar, East Side Taxiway, North End | Crushed Stone Granular Base Course | 3.5               |
| A-14              | Middle South T-Hangar, East Side Taxiway, North End | Hot-Mix Asphalt Surface Course     | 3.0               |
| A-15              | Northeast T- Hangar Taxiway, South Half             | Crushed Stone Granular Base Course | 6                 |
| A-15              | Northeast T- Hangar Taxiway, South Half             | Hot-Mix Asphalt Surface Course     | 4.2               |
| A-16              | Southwest T- Hangar, East Side Taxiway              | Crushed Stone Granular Base Course | 9.3               |
| A-16              | Southwest T- Hangar, East Side Taxiway              | Hot-Mix Asphalt Surface Course     | 4.2               |
| A-17              | South T- Hangar Taxiway, West End                   | Crushed Stone Granular Base Course | 13.8              |
| A-17              | South T- Hangar Taxiway, West End                   | Hot-Mix Asphalt Surface Course     | 4.0               |
| A-17              | South T- Hangar Taxiway, West End                   | Crushed Stone Granular Base Course | 12                |

| Pavement Core No. | Location                               | Pavement Material                  | Thickness, inches |
|-------------------|--|------------------------------------|-------------------|
| A-18              | Southwest T- Hangar, West Side Taxiway | Hot-Mix Asphalt Surface Course     | 5.2               |
| A-18              | Southwest T- Hangar, West Side Taxiway | Crushed Stone Granular Base Course | 11.8              |

NOTE: SEE SHEET REM2 FOR PAVEMENT CORE LOCATIONS

ENGINEERING INFORMATION



Freeport - Albertus Airport



Freeport, Illinois  
City of Freeport

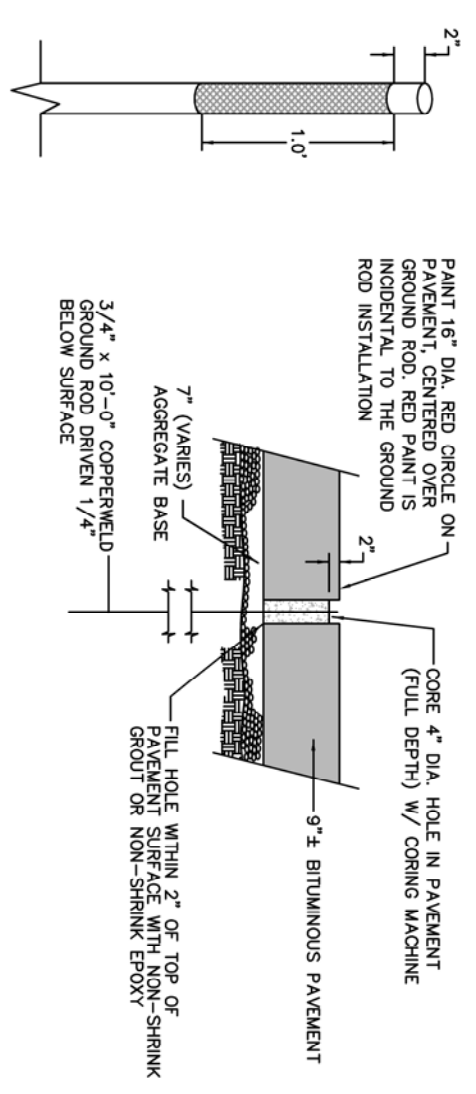
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Rehab. Terminal Apron / Rehab. Taxiways A and E Phase 2

| Revisions | Date | Description |
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| DESIGN BY:   | CMT-RFD        |
| DRAWN BY:    | CMT-RFD        |
| CHECKED BY:  | CMT-RFD        |
| APPROVED BY: | DLP            |
| DATE:        | 3-2-2012       |
| JOB No.:     | 11294-02-00106 |

ELECTRICAL DETAILS - 1 (EDL1)



GROUND ROD KNURL NOT TO SCALE

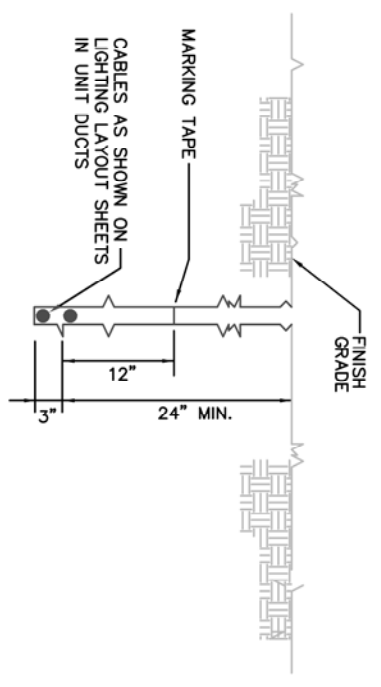
GROUND ROD DETAIL NOT TO SCALE

- TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
- DEPTH OF TRENCHES FOR AIRFIELD LIGHTING SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS. DEPTH OF FAA CABLES SHALL BE 36" UNLESS OTHERWISE SHOWN.
- SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
- ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO TRENCH RETURNING MATERIALS.
- THE CONTRACTOR SHALL HAVE THE OPTION TO TRENCH OR FLOW UNIT DUCT. NO ADDITIONAL PAYMENT SHALL BE MADE FOR TRENCHING.

(AA1)

TRENCH DETAIL FOR CABLE IN UNIT DUCT NOT TO SCALE

NOTES

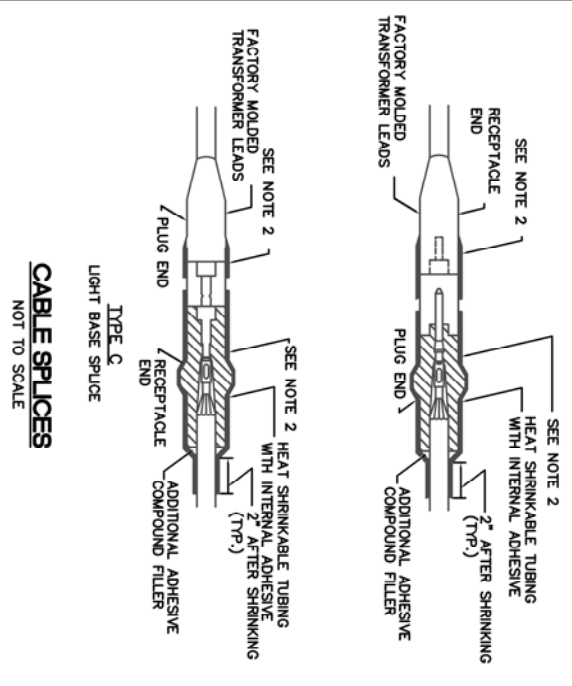
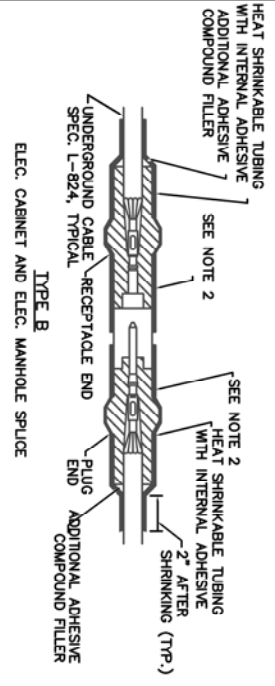
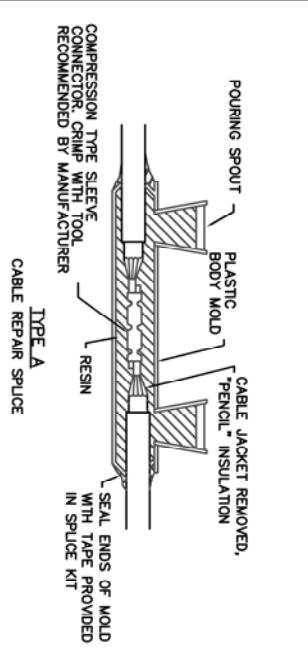


FLOWED CABLE IN UNIT DUCT NOT TO SCALE

NOTES

- ONLY CABLES OF THE SAME CIRCUIT WILL BE ALLOWED TO BE FLOWED IN TOGETHER.
- THE CONTRACTOR SHALL HAVE THE OPTION TO TRENCH OR FLOW UNIT DUCT. NO ADDITIONAL PAYMENT SHALL BE MADE FOR TRENCHING.

(AA1)

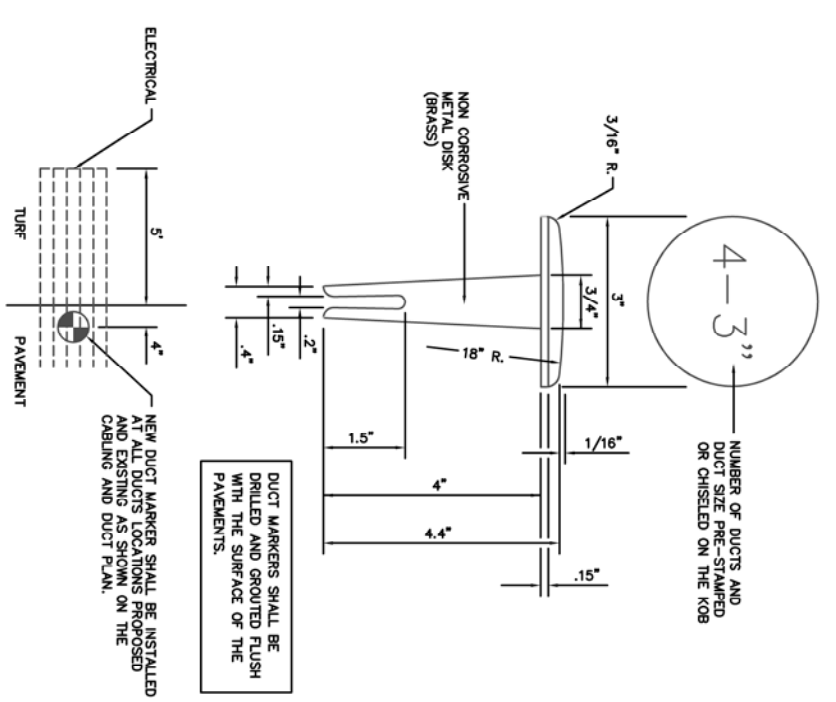


NOTES

- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE OF THE CABLE.
- WRAP WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.
- TO FURTHER REDUCE THE POSSIBILITY OF WATER/MOISTURE ENTRANCE INTO THE CONNECTOR BETWEEN THE CABLE AND THE FIELD ATTACHED CONNECTOR, IT IS REQUIRED THAT A HEAT SHRINK TUBING WITH INTERNAL ADHESIVE BE APPLIED OVER THE ENTIRE CABLE.
- SPLICE CAN SHALL BE REQUIRED FOR ALL TYPE A SPLICE LOCATIONS. THE COST SHALL BE INCIDENTAL TO THE CABLE REPAIR.

(AA1)

DUCT MARKER DETAIL NOT TO SCALE



ELECTRICAL DETAILS - 1





Freeport - Albertus Airport



Freeport, Illinois

City of Freeport

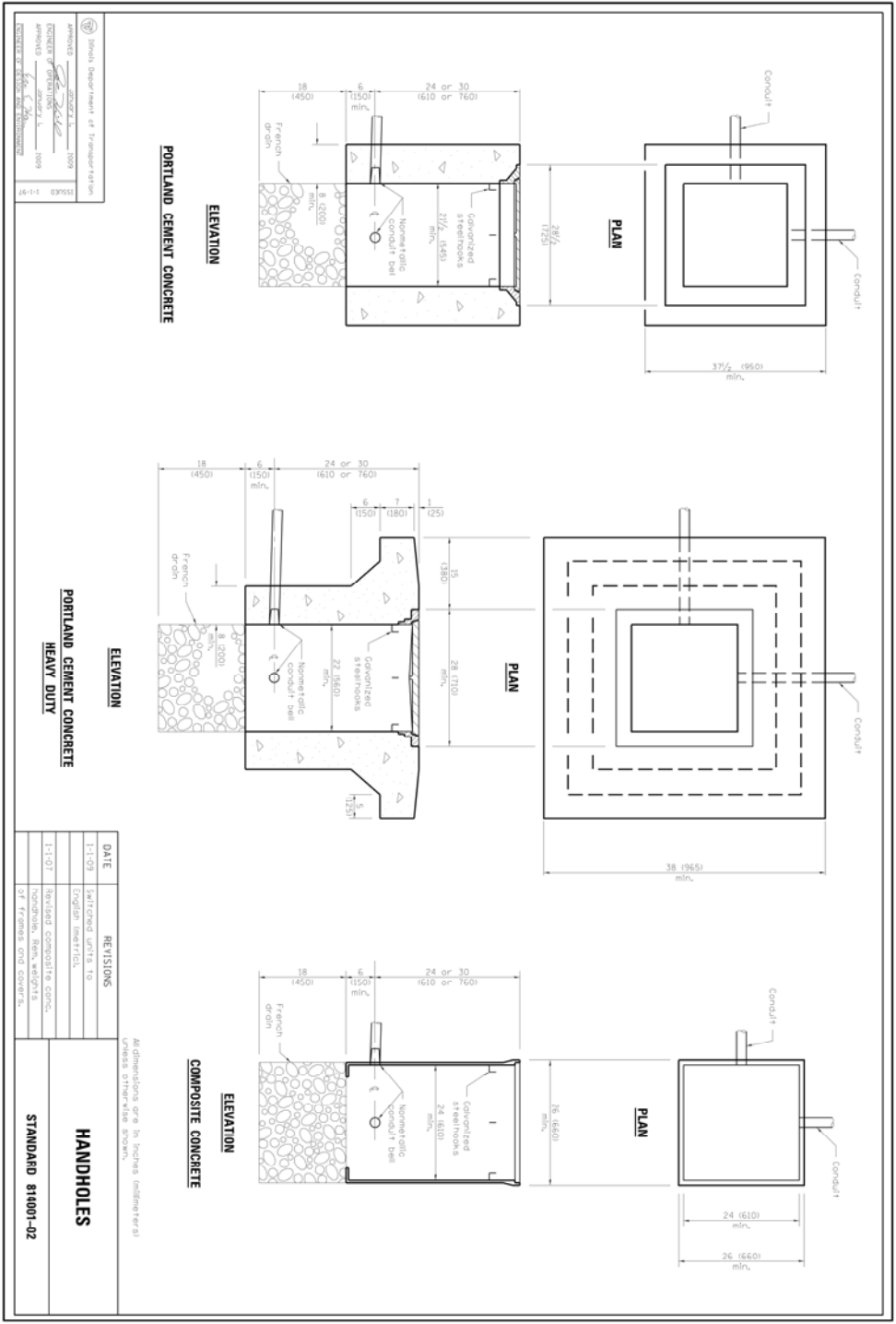
AIP: 3-17-0045-B24  
FEP-4174

Rehab. Terminal  
Apron /  
Rehab. Taxiways A  
and E Phase 2

**HANDHOLE NOTES**

1. CONTRACTOR SHALL INSTALL HANDHOLES IN LOCATIONS SPECIFIED. ELECTRICAL HANDHOLES SHALL MEET THE REQUIREMENTS OF IDOT STANDARD 814001 AND SHALL BE PORTLAND CEMENT CONCRETE HANDHOLES.
2. HANDHOLE LIDS FOR AIRFIELD LIGHTING CIRCUIT SHALL BE LABELED "HIGH VOLTAGE".

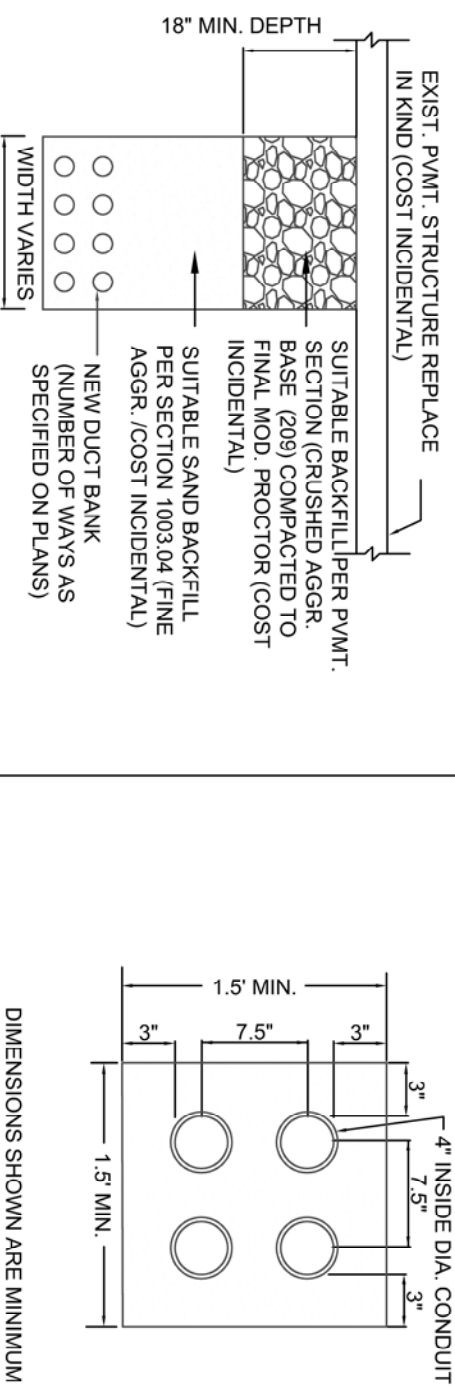
(AA1)



| DATE   | REVISIONS  |
|--------|--|
| 1-1-09 | SAITONG LUTIS TO ENGINE DRAWINGS                   |
| 1-1-07 | REVISED CONCRETE CONC. TO PORTLAND CEMENT CONCRETE |

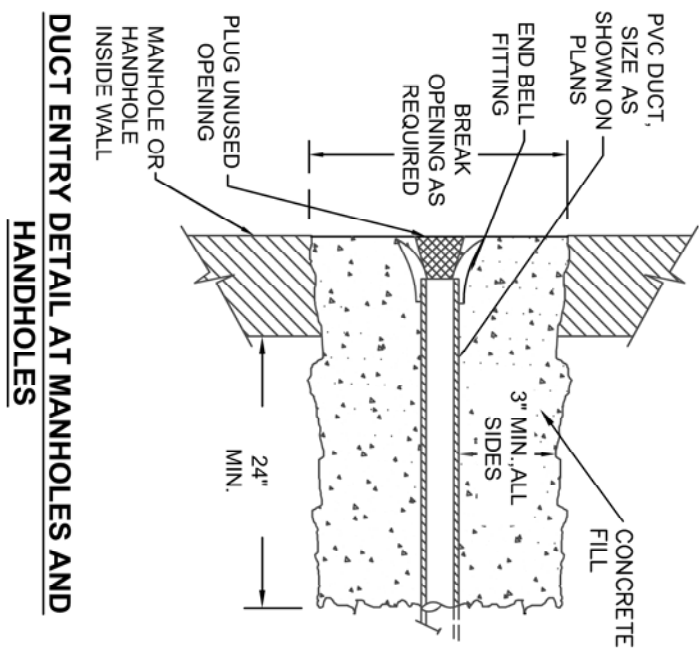
**STD. 814001-01**

(AA1)



**DUCT BANK**  
NUMBER OF WAYS PER PLAN

(AA1)



**DUCT ENTRY DETAIL AT MANHOLES AND HANDHOLES**

NOT TO SCALE

**HANDHOLES**

(AA1)

**ELECTRICAL DETAILS - 2**

**ELECTRICAL  
DETAILS - 2  
(EDL2)**



Freeport - Albertus Airport



Freeport, Illinois  
City of Freeport

AIP: 3-17-0045-B24  
FEP-4174

Rehab. Terminal Apron / Rehab. Taxiways A and E Phase 2

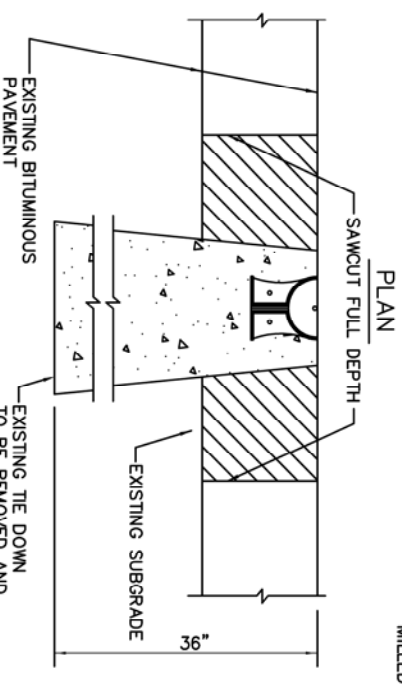
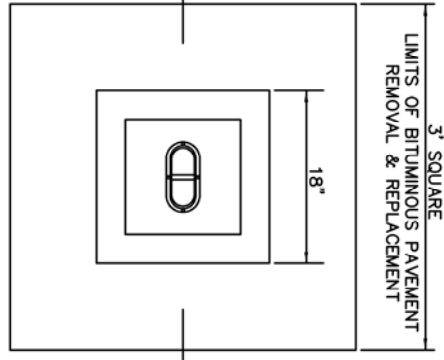
| Revisions |             |
|-----------|-------------|
| Date      | Description |
|           |             |
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0 1  
THIS BAR IS EQUAL TO 1" AT FULL SCALE (17 X 11).

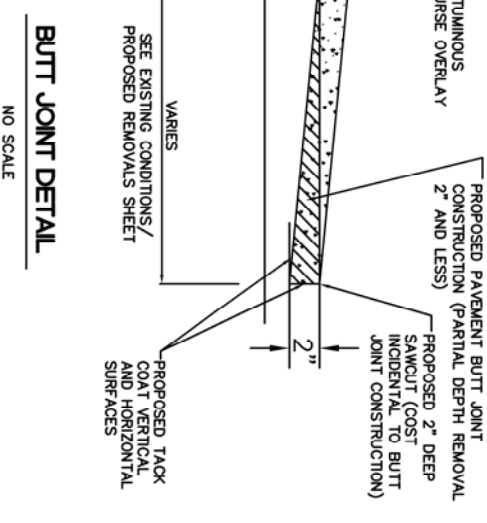
|              |                |
|--------------|----------------|
| DESIGN BY:   | CMT - RFD      |
| DRAWN BY:    | CMT - RFD      |
| CHECKED BY:  | CMT - RFD      |
| APPROVED BY: | DLP            |
| DATE:        | 3-2-2012       |
| JOB No:      | 11294-02-00/06 |

MISCELLANEOUS DETAILS (DTL1)

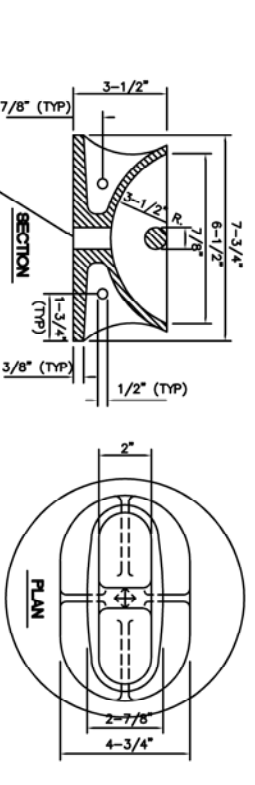
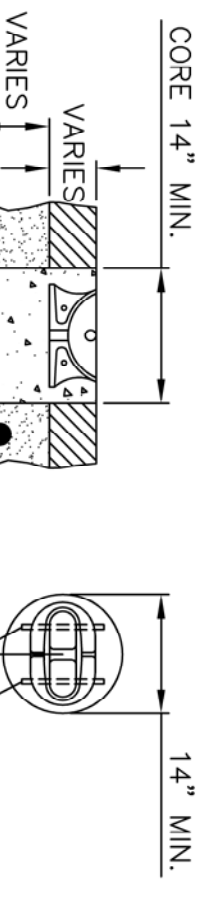
- NOTES:
1. THE SAWCUT AND BITUMINOUS PAVEMENT REMOVAL AND REPLACEMENT SHALL BE CONSIDERED INCIDENTAL TO THE MOORING EYE REMOVAL.
  2. THE VOID LEFT BY THE REMOVAL SHALL BE BACKFILLED TO 9" BELOW EXISTING PAVEMENT SURFACE WITH CLSM MATERIAL. THE FINAL 9" SHALL BE FILLED WITH BITUMINOUS SURFACE COURSE TO MATCH THE MILLED SURFACE.



THE DOWN REMOVAL DETAIL  
NOT TO SCALE

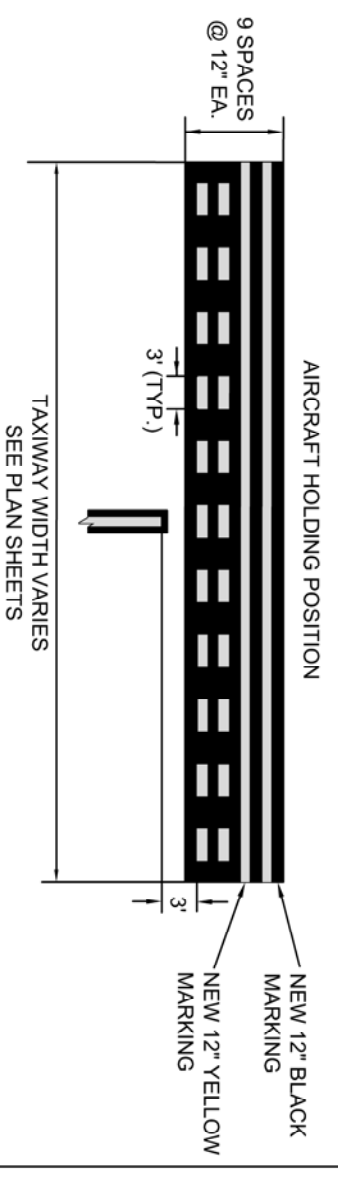


BUTT JOINT DETAIL  
NO SCALE

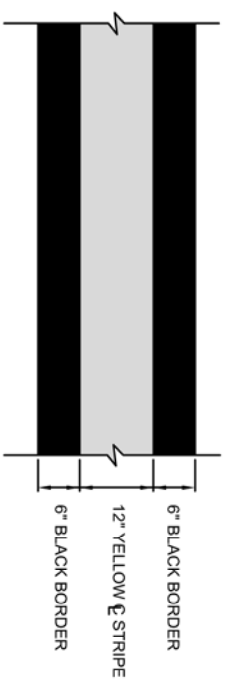


MOORING EYE DETAIL  
N.T.S.

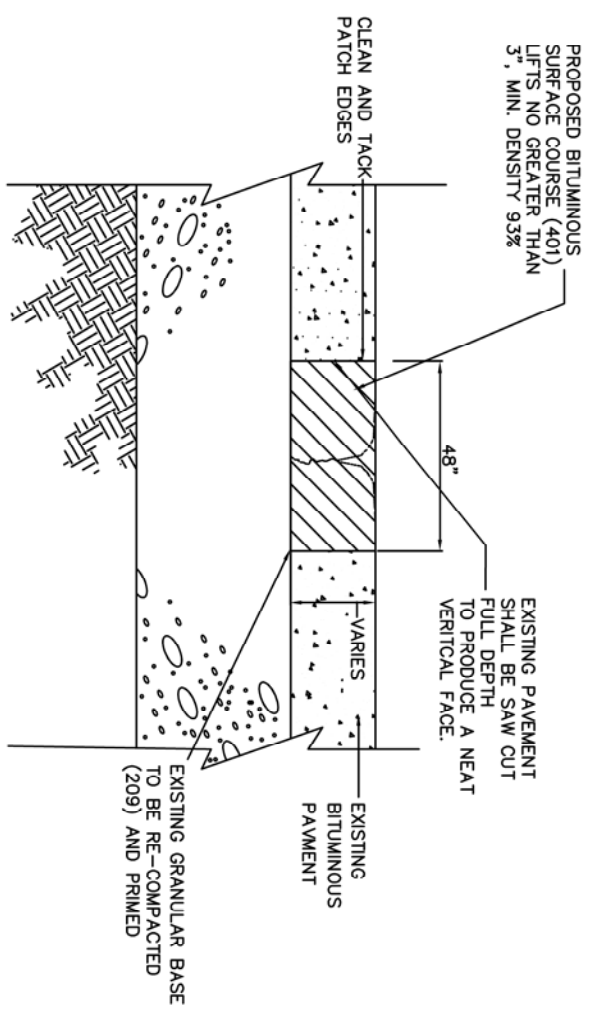
- NOTES
- 1.) MOORING CASTINGS SHALL BE NEENAH SEMI-STEEL AIRPORT MOORING EYE CATALOG NO. R3490 OR APPROVED EQUIVALENT.
  - 2.) ANCHOR RODS SHALL BE #3 DEFORMED REBAR, 15" LONG AND SHALL BE BENT DOWNWARD AT 45°.
  - 3.) MOORING EYES IN PROPOSED P.C.C. PAVEMENT SHALL BE INSTALLED DURING THE PAVING OPERATION WHILE CONCRETE IS STILL PLASTIC.
  - 4.) FINISH SHALL SHOW NO VOIDS AND HAVE A BROOM FINISH.



TAXIWAY HOLDING POSITION MARKING  
NO SCALE



TAXIWAY / APRON CENTERLINE MARKING  
DETAIL  
NO SCALE



PROPOSED REPAIR METHOD FOR FULL DEPTH PATCHING (REMOVE AND REPLACE BITUMINOUS PAVEMENT)  
NO SCALE

MISCELLANEOUS DETAILS