

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-7	DuPAGE/COOK	*35	1
ILLINOIS			CONTRACT NO. 62F56	

\*35 + 9 = 44 TOTAL SHEETS

D-91-014-18



FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED IN THE VILLAGE OF HINSDALE

PROPOSED  
HIGHWAY PLANS

FAP ROUTE 311: US 34 (OGDEN AVENUE)  
EAST OF IL 83 (KINGERY HWY) TO WEST OF I-294 (TRI-STATE TOLLWAY)  
SECTION: 9Y-RS-7  
PROJECT: NHPP-XA04(258)  
RESURFACING (3P), PEDESTRIAN RAMPS  
DuPAGE / COOK COUNTY

C-91-015-18

TRAFFIC DATA:

US 34 (OGDEN AVE):

PROJECT BEGIN TO YORK RD:

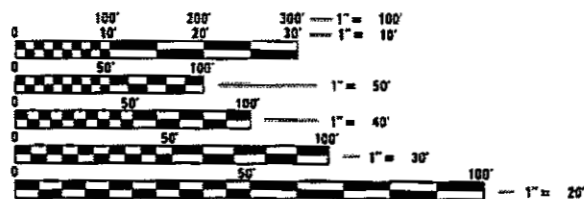
ADT (2016) = 31,900

SPEED LIMIT = 35 MPH (POSTED)

YORK RD TO PROJECT END:

ADT (2016) = 34,700

SPEED LIMIT = 35 MPH (POSTED)

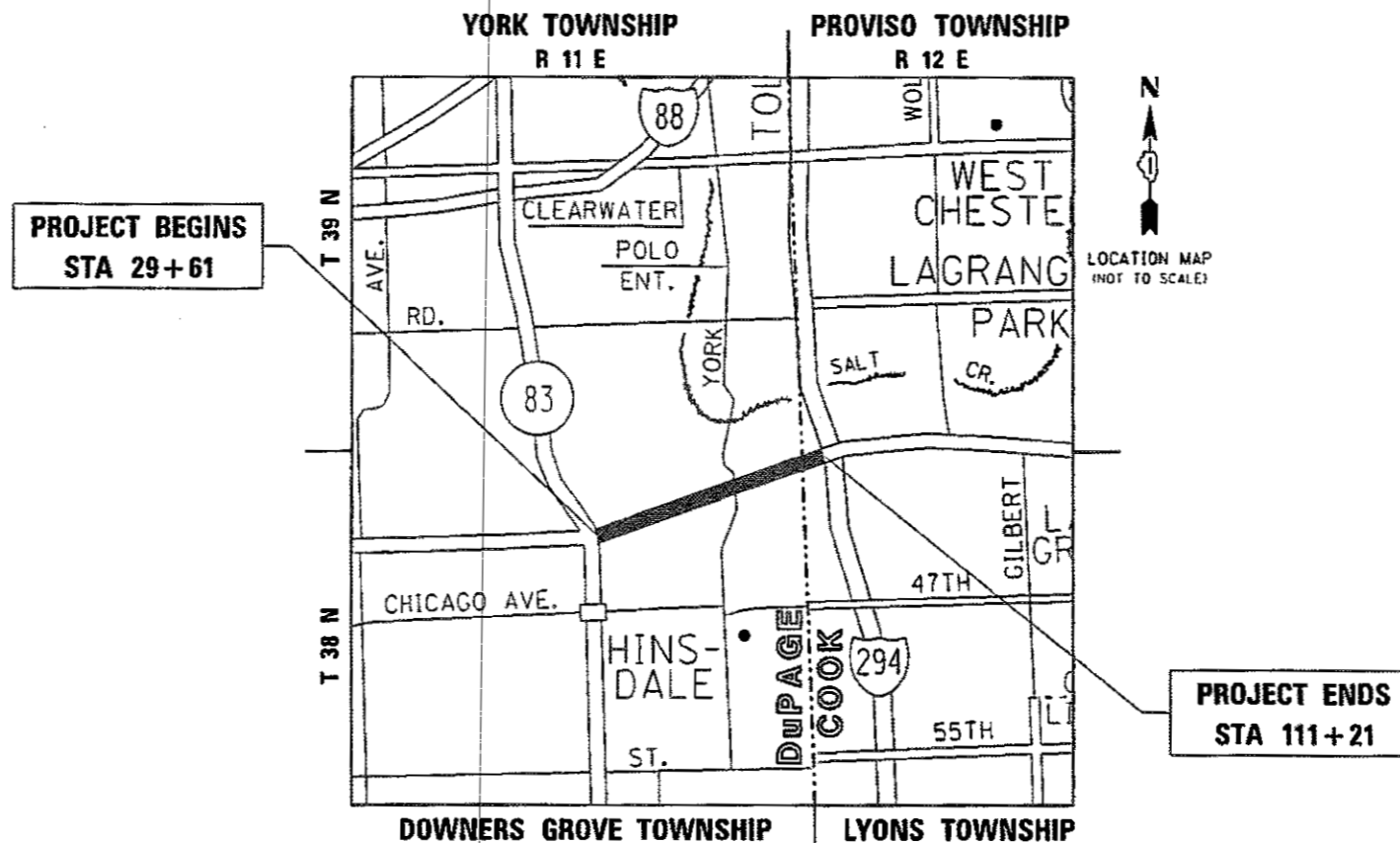


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: KARI SMITH (847) 705-4437  
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

CONTRACT NO. 62F56



GROSS & NET LENGTH = 8,160 FT. = 1.545 MILES

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED January 30, 2018

Anthony J. Quigley / P.E. REGIONAL ENGINEER  
March 23, 2018  
C. E. Etk ENGINEER OF DESIGN AND ENVIRONMENT  
March 23, 2018  
Paul J. [Signature] DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS



**GENERAL NOTES (CONTINUED)**

21. WHEN EXISTING SIDEWALK IS TO BE REMOVED WITHOUT PROPOSED SIDEWALK REPLACEMENT, IT SHALL BE REPLACED WITH TOPSOIL AND SOD.
22. THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS THAT WILL NOT BE REMOVED INCLUDING PREVIOUSLY SEEDED AREAS. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE TO THE SATISFACTION OF THE ENGINEER.
23. LANDSCAPED AREAS AFFECTED BY SIDEWALK CONSTRUCTION SHALL BE RESTORED WITH 18-INCH WIDE STRIP OF "SODDING, SALT TOLERANT" AND "TOPSOIL FURNISH AND PLACE, 4-INCH" INSTALLED FROM THE BACK OF THE SIDEWALK, OR AS DETERMINED BY THE RESIDENT ENGINEER/TECHNICIAN.
24. ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF CURB OR DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER, SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCIDENTAL.
25. THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENT FIRM TO CONTINUOUSLY MONITOR FOR WORKER SAFETY AND SOIL CONTAMINATION AT SEVERAL LOCATIONS. SEE SPECIAL PROVISION AND SUPPLEMENTAL SPECIFICATIONS FOR DETAILS.
26. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPLACEMENT OF ANY DETECTOR LOOPS DAMAGED DURING CONSTRUCTION.
27. THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.
28. CONTACT THE IDOT ROADSIDE DEVELOPMENT UNIT AT 847-705-4171 AT LEAST 2 WEEKS PRIOR TO BEGINNING FORESTRY WORK FOR LAYOUT.

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USER NAME = Velichkovv	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 7/1/2018	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**US 34 (E. OF IL 83 (KINGERY HWY) TO W. OF I-294 (TRI-STATE TOLLWAY))  
 INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-7	DUPAGE/COOK	35	3
CONTRACT NO. 62F56				
ILLINOIS FED. AID PROJECT				

URBAN

URBAN

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 80% FED 20% STATE DUPAGE COUNTY	0005 80% FED 20% STATE COOK COUNTY		
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	60	60			
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	30	30			
20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	5	5			
20200100	EARTH EXCAVATION	CU YD	33	32	1		
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	215	210	5		
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	3	2.9	0.1		
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	3	2.9	0.1		
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	3	2.9	0.1		
25200110	SODDING, SALT TOLERANT	SO YD	237	231	6		
25200200	SUPPLEMENTAL WATERING	UNIT	10	10			
35501308	HOT-MIX ASPHALT BASE COURSE, .6"	SO YD	9	9			
35501316	HOT-MIX ASPHALT BASE COURSE, .8"	SO YD	42	42			
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	36760	30830	5930		
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	83	69	14		
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	2260	1920	340		

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 80% FED 20% STATE DUPAGE COUNTY	0005 80% FED 20% STATE COOK COUNTY		
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	489	394	95		
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	1480	1380	100		
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	6	6			
42001300	PROTECTIVE COAT	SO YD	461	447	14		
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SO YD	60	60			
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	3237	3155	82		
42400800	DETECTABLE WARNINGS	SO FT	266	256	10		
44000156	HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"	SO YD	1050		1050		
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	53710	45630	8080		
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	51	51			
44000600	SIDEWALK REMOVAL	SO FT	3180	3115	65		
44002214	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3 1/2"	SO YD	485		485		

\* = SPECIALTY ITEMS

FILE NAME =	USER NAME = VeltNovy	DESIGNED -	REVISED -
D:\ANL\BHE\BID\IT\EG\H\trd\spw\FW\DOT\Documents\ND\	Offices\District 1\Projects\014\B\CADD\Design\DRM\	DRM	REVISED -
PLOT SCALE = 1000000' / 1"	CHECKED -	REVISED -	REVISED -
PLOT DATE = 2/1/2018	DATE -	REVISED -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

US 34 (E. OF IL 83 (KINGERY HWY) TO W. OF I-294 (TRI-STATE TOLLWAY))  
SUMMARY OF QUANTITIES  
SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-7	DUPAGE/COOK	35	4
CONTRACT NO. 62F56				
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				

REV

URBAN

URBAN

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE						
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 80% FED 20% STATE DUPAGE COUNTY	0005 80% FED 20% STATE COOK COUNTY				
44002236	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 9"	SQ YD	2738	2738					
44201737	CLASS D PATCHES, TYPE I, 8 INCH	SQ YD	137	137					
44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	548	548					
44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	685	685					
44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	1369	1369					
44201798	CLASS D PATCHES, TYPE I, 13 INCH	SQ YD	25		25				
44201803	CLASS D PATCHES, TYPE II, 13 INCH	SQ YD	97		97				
44201807	CLASS D PATCHES, TYPE III, 13 INCH	SQ YD	122		122				
44201809	CLASS D PATCHES, TYPE IV, 13 INCH	SQ YD	243		243				
60250200	CATCH BASINS TO BE ADJUSTED	EACH	4	4					
60250400	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE I FRAME, OPEN LID	EACH	1	1					
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	2	2					
60255500	MANHOLES TO BE ADJUSTED	EACH	3	3					
60260100	INLETS TO BE ADJUSTED	EACH	5	4	1				
60262700	INLETS TO BE RECONSTRUCTED	EACH	2	2					
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	16	15	1				

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE						
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 80% FED 20% STATE DUPAGE COUNTY	0005 80% FED 20% STATE COOK COUNTY				
60400210	FRAMES, TYPE 3	EACH	1	1					
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	5	5					
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	5	5					
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	33	32	1				
* 66900450	SPECIAL WASTE PLANS AND REPORTS	L SUM	1	1					
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	3	3					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	5	1				
67100100	MOBILIZATION	L SUM	1	0.8	0.2				
70100420	TRAFFIC CONTROL AND PROTECTION STANDARD 70111	EACH	2	2					
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	0.8	0.2				
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	0.8	0.2				
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	0.8	0.2				
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	0.8	0.2				
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	0.8	0.2				

\* = SPECIALTY ITEMS

FILE NAME =	USER NAME = yvlatkovy	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>US 34 (E. OF IL 83 (KINGERY HWY) TO W. OF I-294 (TRI-STATE TOLLWAY) SUMMARY OF QUANTITIES</b>	F.A.P. RTE. 311	SECTION 9Y-RS-7	COUNTY	TOTAL SHEETS 35	SHEET NO. 5
OFFICE/PROJECT NAME/PROJECT NO./JOB NO./DESIGN NO./DATE	CHECKED -	REVISED -	DUPAGE/COOK			CONTRACT NO. 62F56				
PLOT SCALE = 100,000' / 1" =	DATE -	REVISED -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
PLOT DATE = 2/1/2018			SCALE:			SHEET NO. OF SHEETS STA. TO STA.				

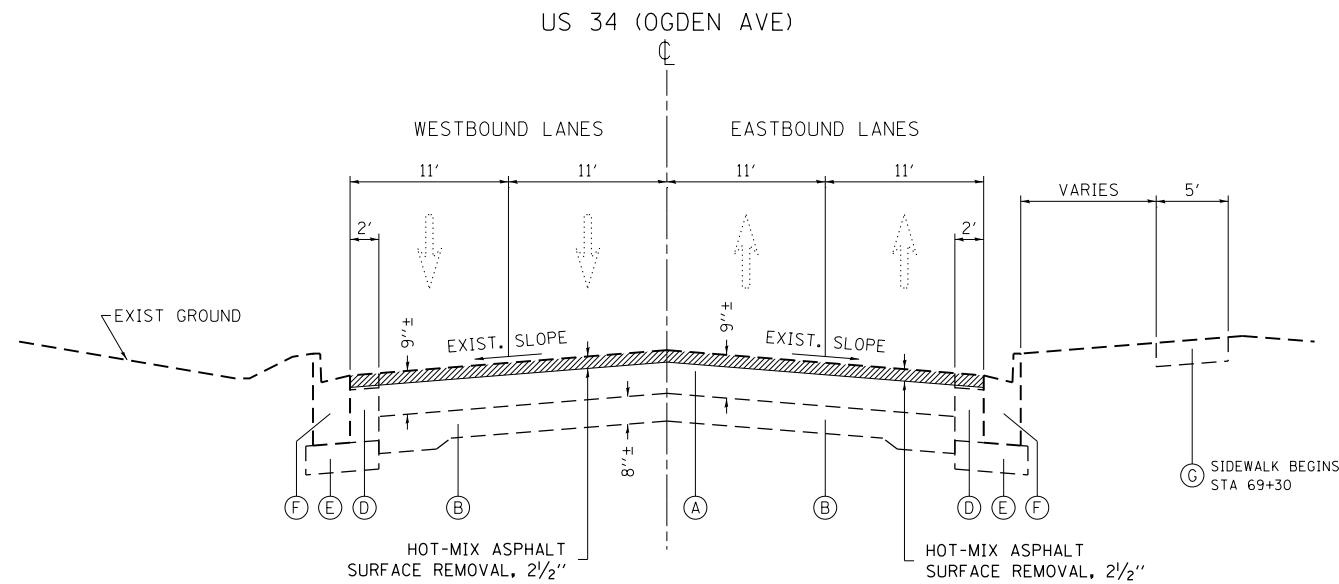
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SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE				SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE			
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 80% FED 20% STATE DUPAGE COUNTY	0005 80% FED 20% STATE COOK COUNTY			CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005 80% FED 20% STATE DUPAGE COUNTY	0005 80% FED 20% STATE COOK COUNTY		
70300100	SHORT TERM PAVEMENT MARKING	FOOT	17520	16290	1230			* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	920	480	440		
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	5840	5430	410			* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	440	440			
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	580	580				* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	825	740	85		
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	25040	21240	3800			78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	743	666	77		
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	3090	3090				* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1	1			
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	750	240	510			* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	2503	2503			
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	920	480	440			* 89500400	RELOCATE EXISTING PEDESTRIAN PUSH-BUTTON	EACH	10	10			
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	440	440				* 89502376	REBUILD EXISTING HANDHOLE	EACH	4	4			
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	5840	5430	410			X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1			
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	580	580				X0325222	WEED CONTROL, BASAL TREATMENT	GALLON	4	4			
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	25040	21240	3800			X0327611	REMOVE AND REINSTALL GRACK PAVEMENT	SQ FT	100	100			
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3090	3090				X4060004	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, NBD	TON	5367	4472	895		
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	750	240	510			Δ X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	124	124			

FILE NAME =	USER NAME = jvalokov	DESIGNED -	REVISION -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US 34 (E. OF IL 83 (KINGERY HWY) TO W. OF I-294 (TRI-STATE TOLLWAY))	F.A.P. RATE.	SECTION	COUNTY	TOTAL SHEET NO.
OFFICE/PROJECT/DATE	OFFICE/PROJECT/DATE	CHECKED -	REVISION -		SCALE:	SHEET NO. OF SHEETS STA. TO STA.	311	9V-RS-7	DUPAGE/COOK 35 6
PLOT SCALE = 1/8" = 100'	PLOT DATE = 8/1/2008	CHECKED -	REVISION -						CONTRACT NO. 62F56
		DATE -	REVISION -						FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT

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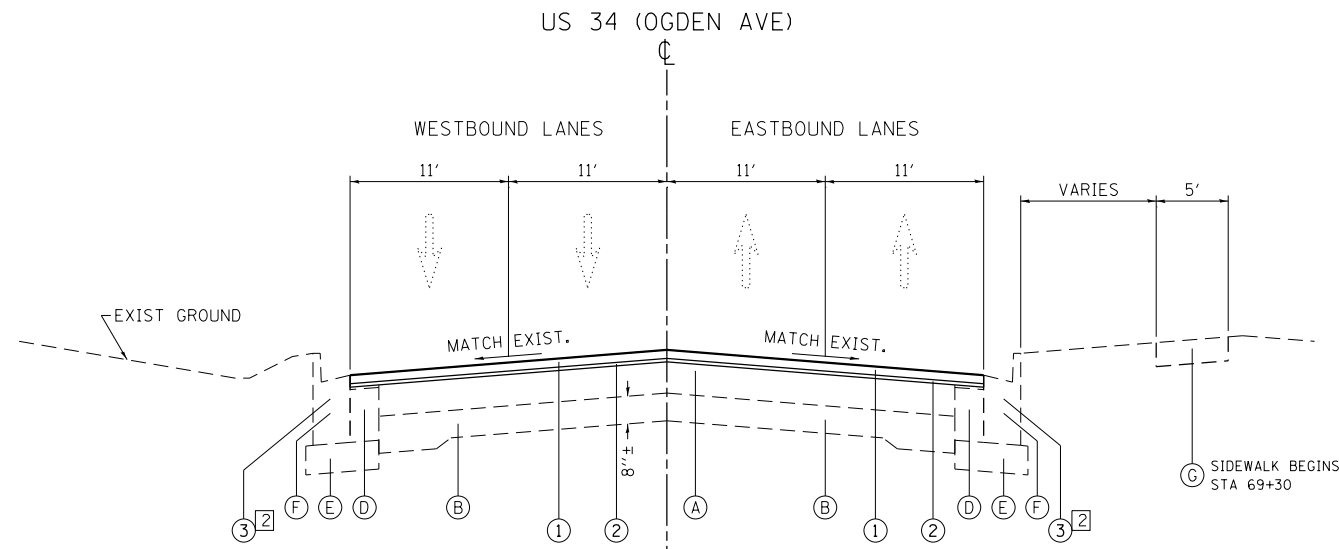
**EXISTING TYPICAL SECTION**  
 STA 29+61 (PROJECT BEGIN) TO STA 74+58.6\*  
 \* = 5 LANE SECTION AT MADISON ST.

**LEGEND - EXISTING:**

- (A) HMA OVERLAY
- (B) PCC PAVEMENT
- (C) NOT USED
- (D) WIDENING:  
 HMA SURFACE COURSE, 1 1/2"  
 HMA BINDER, 1 1/2"  
 PCC BASE COURSE WIDENING, 11"±
- (E) GRANULAR SUBBASE (4" - 6")
- (F) COMB. CONC. CURB & GUTTER, TYPE B-6.24
- (G) PCC SIDEWALK
- (H) HMA SURFACE (4 1/2" AND VARIES)
- (I) PCC PAVEMENT (10" AND VARIES)
- (J) HMA SURFACE (2 1/2" AND VARIES)
- (K) HMA SURFACE (VARIABLE DEPTH)

**NOTES:**

THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING.



**PROPOSED TYPICAL SECTION**  
 STA 29+61 (PROJECT BEGIN) TO STA 74+58.6\*  
 \* = 5 LANE SECTION AT MADISON ST.

**LEGEND - PROPOSED**

- (1) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80, 1 3/4"
- (2) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- (3) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

(2) = LOCATIONS OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER/TECHNICIAN.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR VOIDS @ Ndes	
<b>PAVEMENT RESURFACING</b>		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80	3.5% AT 80 GYR.	QCP
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5% AT 50 GYR.	QCP
<b>DRIVEWAYS</b>		
HMA SURFACE COURSE, MIX D, N50 (IL 9.5 mm); 2"	4% AT 50 GYR.	QC/QA
HMA BASE COURSE (HMA BINDER IL-19 mm); PE - 6", CE - 8"	4% AT 50 GYR.	QC/QA
<b>PATCHING</b>		
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% AT 70 GYR.	QC/QA
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm)	4% AT 70 GYR.	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP)		

- NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
- NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
- NOTE 3: QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

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USER NAME = Velichkovv	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 2/1/2018	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**US 34 (E. OF IL 83 (KINGERY HWY) TO W. OF I-294 (TRI-STATE TOLLWAY))**  
**TYPICAL SECTIONS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-7	DUPAGE/COOK	35	8
CONTRACT NO. 62F56				
ILLINOIS FED. AID PROJECT				



**LEGEND - EXISTING:**

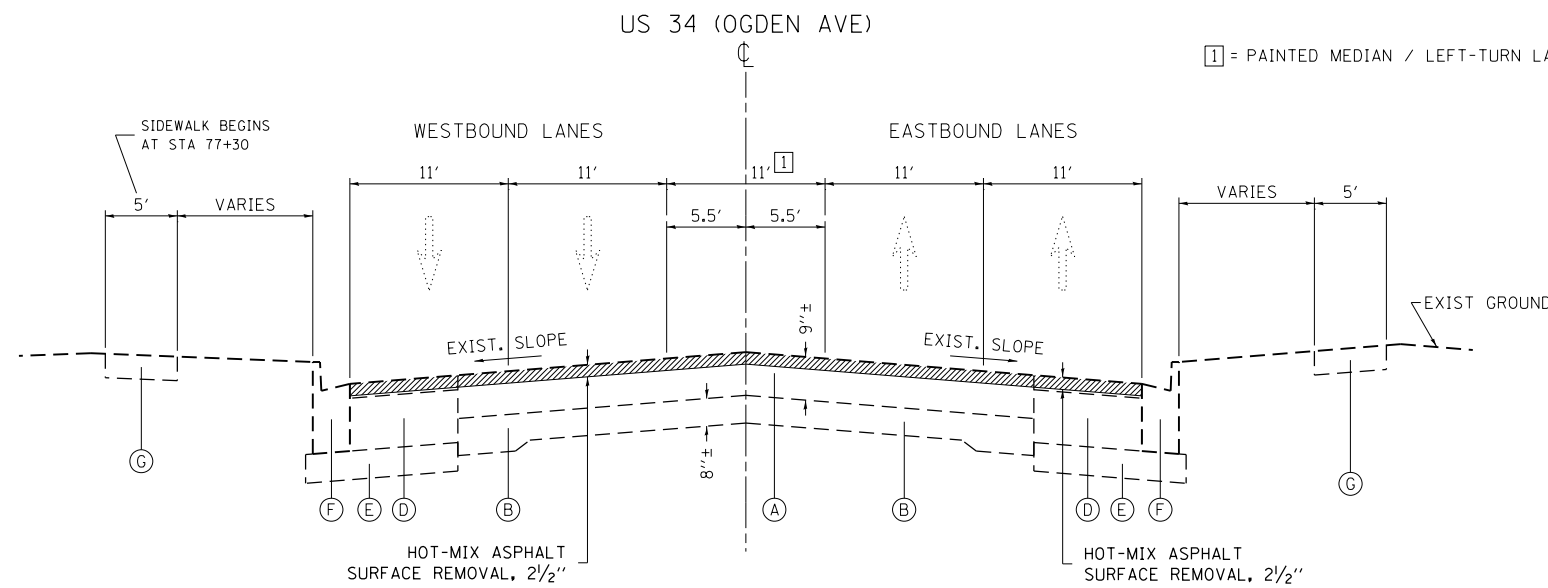
- (A) HMA OVERLAY
- (B) PCC PAVEMENT
- (C) NOT USED
- (D) WIDENING:  
HMA SURFACE COURSE, 1 1/2"  
HMA BINDER, 1 1/2"  
PCC BASE COURSE WIDENING, 11"±
- (E) GRANULAR SUBBASE (4" - 6")
- (F) COMB. CONC. CURB & GUTTER, TYPE B-6.24
- (G) PCC SIDEWALK
- (H) HMA SURFACE (4 1/2" AND VARIES)
- (I) PCC PAVEMENT (10" AND VARIES)
- (J) HMA SURFACE (2 1/2" AND VARIES)
- (K) HMA SURFACE (VARIABLE DEPTH)

**NOTES:**

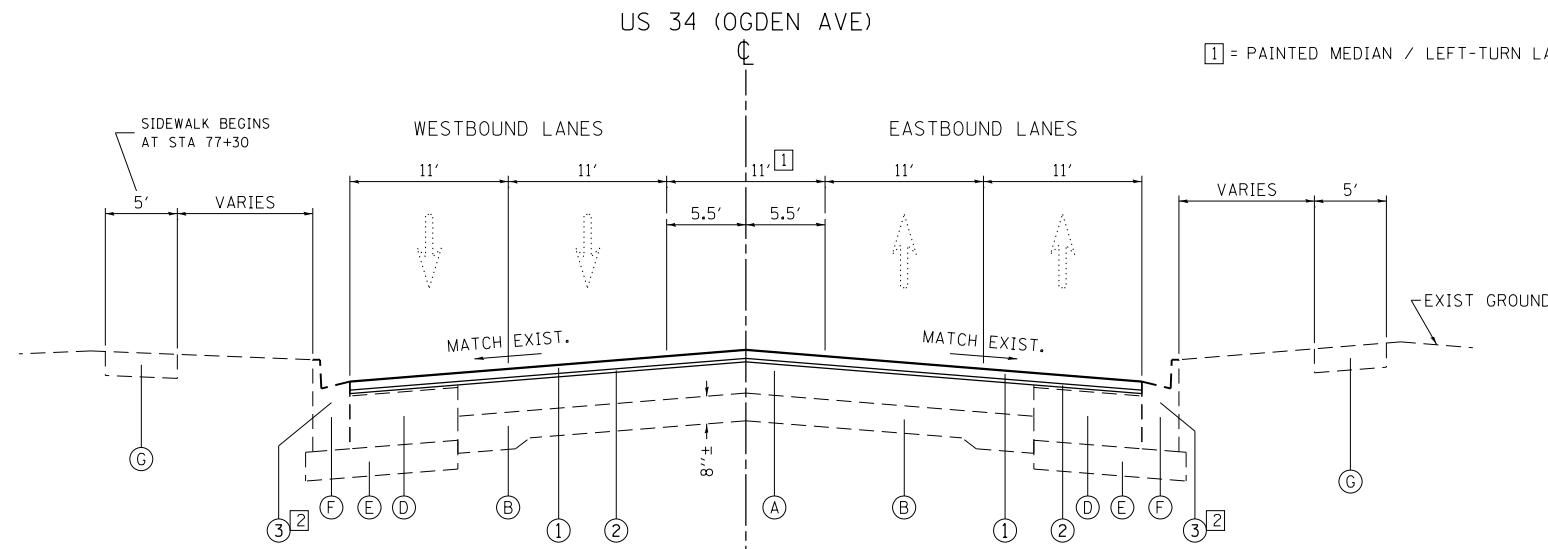
THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING.

**LEGEND - PROPOSED**

- (1) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80, 1 3/4"
- (2) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- (3) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT



**EXISTING TYPICAL SECTION**  
STA 74+58.6 TO STA 102+10 (COUNTY LINE) \*  
\* = 7 LANE SECTION AT COUNTY LINE RD



**PROPOSED TYPICAL SECTION**  
STA 74+58.6 TO STA 102+10 (COUNTY LINE) \*  
\* = 7 LANE SECTION AT COUNTY LINE RD

(2) = LOCATIONS OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER/TECHNICIAN.

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USER NAME = Velichkovv	DESIGNED -	REVISED -
PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -
PLOT DATE = 2/1/2018	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>US 34 (E. OF IL 83 (KINGERY HWY) TO W. OF I-294 (TRI-STATE TOLLWAY))</b>		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		311	9Y-RS-7	DUPAGE/COOK	35	9
		SCALE:		SHEET OF SHEETS		STA. TO STA.

<b>TYPICAL SECTIONS</b>		CONTRACT NO. 62F56	
		ILLINOIS	FED. AID PROJECT

**LEGEND - EXISTING:**

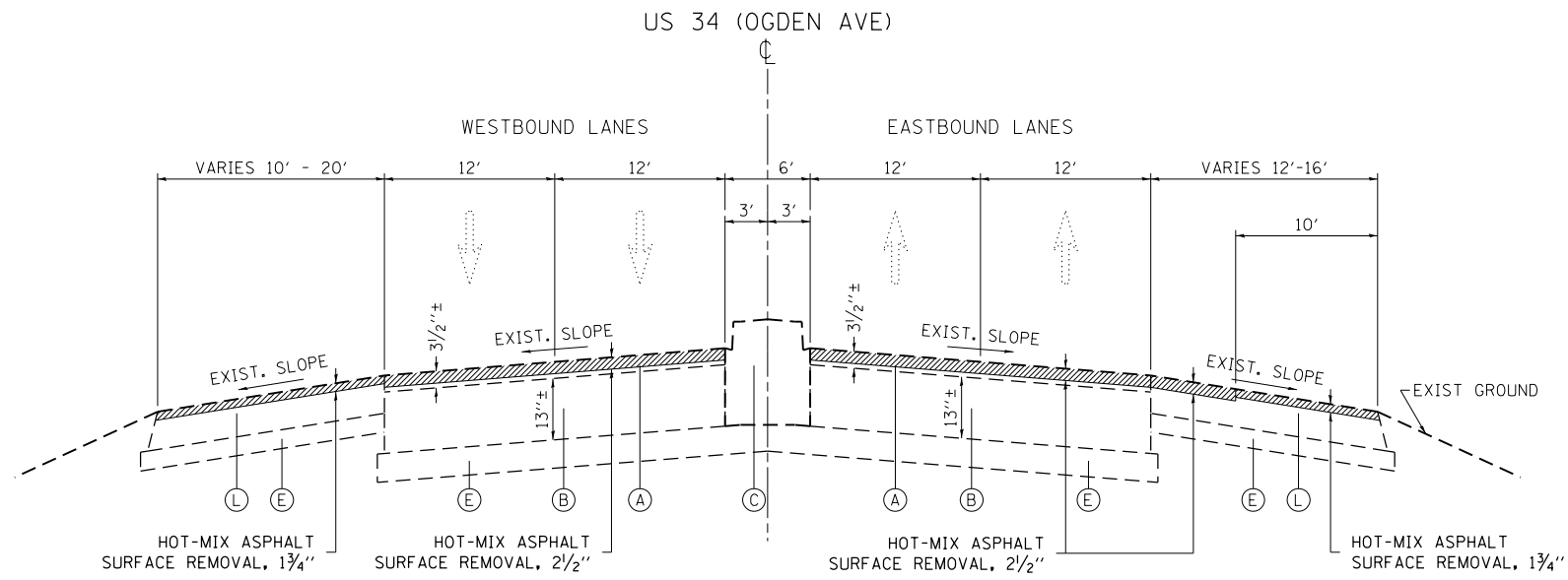
- (A) HMA OVERLAY
- (B) PCC PAVEMENT
- (C) PCC MEDIAN (BARRIER)
- (D) WIDENING:  
HMA SURFACE COURSE, 1 1/2"  
HMA BINDER, 1 1/2"  
PCC BASE COURSE WIDENING, 11"±
- (E) GRANULAR SUBBASE (4" - 6")
- (F) COMB. CONC. CURB & GUTTER, TYPE B-6.24
- (G) PCC SIDEWALK
- (H) HMA SURFACE (4 1/2" AND VARIES)
- (I) PCC PAVEMENT (10" AND VARIES)
- (J) HMA SURFACE (2 1/2" AND VARIES)
- (K) HMA SURFACE (VARIABLE DEPTH)
- (L) HMA SHOULDER (8")

**NOTES:**

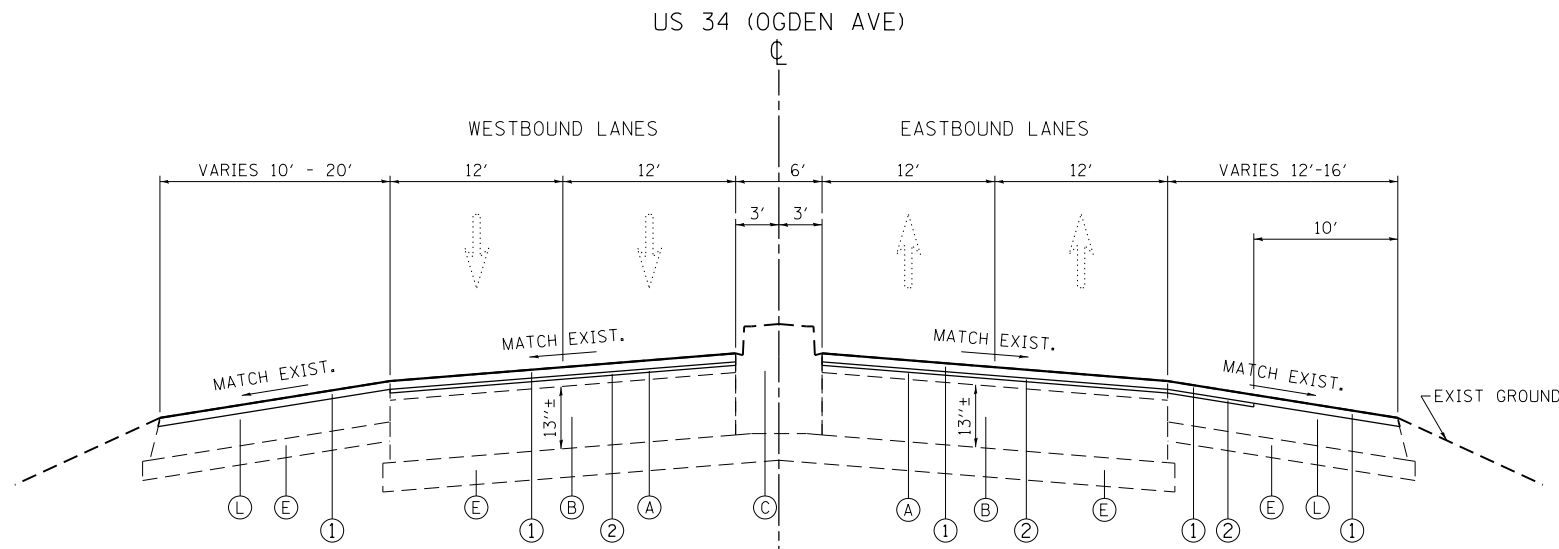
THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING.

**LEGEND - PROPOSED**

- (1) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80, 1 3/4"
- (2) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- (3) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT



**EXISTING TYPICAL SECTION**  
STA 102+10 (COUNTY LINE) TO STA 111+21 (PROJECT END)\*  
\* = 7 LANE SECTION AT COUNTY LINE RD



**PROPOSED TYPICAL SECTION**  
STA 102+10 (COUNTY LINE) TO STA 111+21 (PROJECT END)\*  
\* = 7 LANE SECTION AT COUNTY LINE RD

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	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 2/1/2018	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**US 34 (E. OF IL 83 (KINGERY HWY) TO W. OF I-294 (TRI-STATE TOLLWAY))**  
**TYPICAL SECTIONS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-7	DUPAGE/COOK	35	10
CONTRACT NO. 62F56			ILLINOIS FED. AID PROJECT	

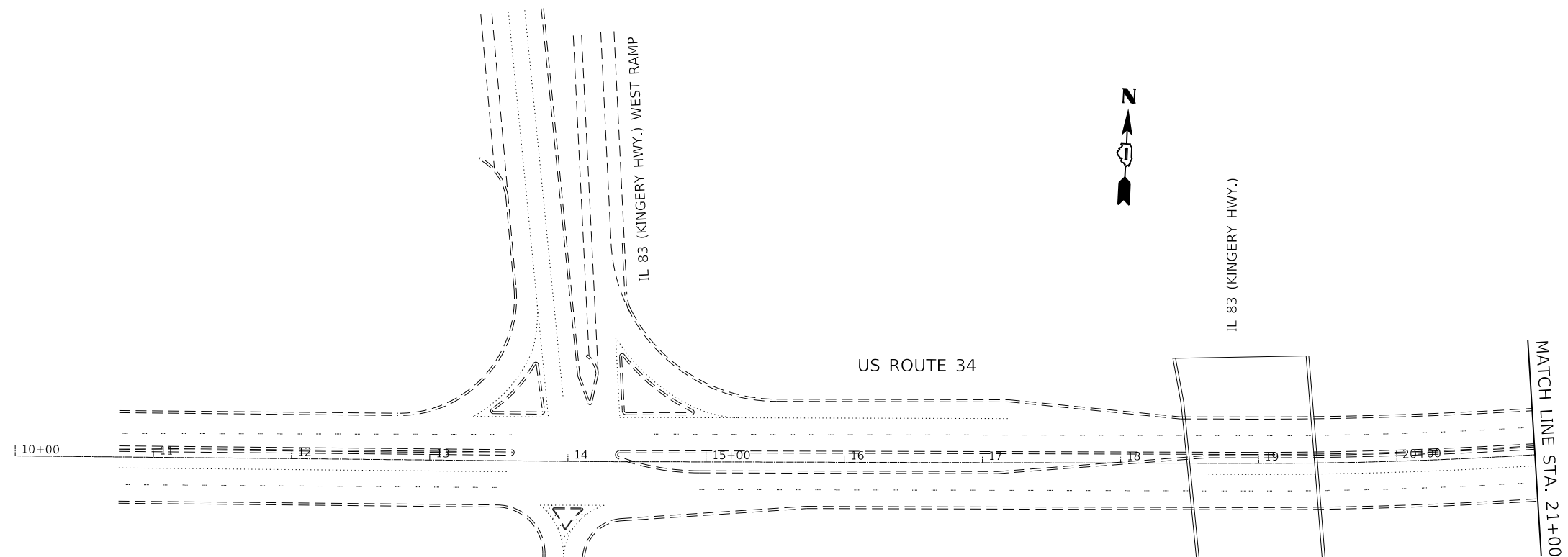


**NOTES:**

- 1) LIMIT OF RESURFACING ON THE SIDE- STREETS THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER / TECHNICIAN.
- 2) ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE).
- 3) ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH TC-13 "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" STANDARD DETAIL.
- 4) ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH TC-11 "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" STANDARD DETAIL.

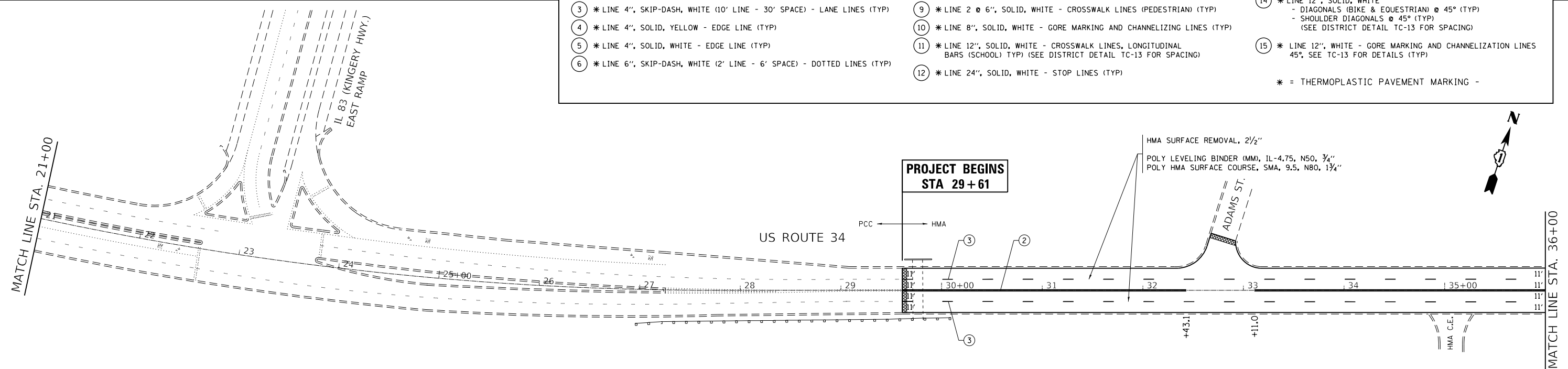
**ROADWAY LEGEND**

 HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT



**LEGEND: PROPOSED PAVEMENT STRIPING**

<p>① * LINE 4", SKIP-DASH, YELLOW (10' LINE - 30' SPACE) - CENTERLINE (TYP)</p> <p>② * LINE 2 @ 4", SOLID, YELLOW, 11" C-C - CENTERLINE (TYP)</p> <p>③ * LINE 4", SKIP-DASH, WHITE (10' LINE - 30' SPACE) - LANE LINES (TYP)</p> <p>④ * LINE 4", SOLID, YELLOW - EDGE LINE (TYP)</p> <p>⑤ * LINE 4", SOLID, WHITE - EDGE LINE (TYP)</p> <p>⑥ * LINE 6", SKIP-DASH, WHITE (2' LINE - 6' SPACE) - DOTTED LINES (TYP)</p>	<p>⑦ * LINE 6", SOLID, WHITE - TURN LANE MARKINGS (TYP)</p> <p>⑧ * LETTERS &amp; SYMBOLS, 8", SOLID, WHITE - TURN LANE MARKINGS (TYP)</p> <p>⑨ * LINE 2 @ 6", SOLID, WHITE - CROSSWALK LINES (PEDESTRIAN) (TYP)</p> <p>⑩ * LINE 8", SOLID, WHITE - GORE MARKING AND CHANNELIZING LINES (TYP)</p> <p>⑪ * LINE 12", SOLID, WHITE - CROSSWALK LINES, LONGITUDINAL BARS (SCHOOL) TYP (SEE DISTRICT DETAIL TC-13 FOR SPACING)</p> <p>⑫ * LINE 24", SOLID, WHITE - STOP LINES (TYP)</p>	<p>⑬ * LINE 12", SOLID, YELLOW - PAINTED MEDIANS DIAGONALS (TYP) (SEE DISTRICT DETAIL TC-13 FOR SPACING)</p> <p>⑭ * LINE 12", SOLID, WHITE - DIAGONALS (BIKE &amp; EQUESTRIAN) @ 45° (TYP) - SHOULDER DIAGONALS @ 45° (TYP) (SEE DISTRICT DETAIL TC-13 FOR SPACING)</p> <p>⑮ * LINE 12", WHITE - GORE MARKING AND CHANNELIZATION LINES 45°. SEE TC-13 FOR DETAILS (TYP)</p> <p style="text-align: right;">* = THERMOPLASTIC PAVEMENT MARKING -</p>
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	DRAWN -	REVISED -	
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -	
PLOT DATE = 2/1/2018	DATE -	REVISED -	

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**US 34 (E. OF IL 83 (KINGERY HWY) TO W. OF I-294 (TRI-STATE TOLLWAY)  
ROADWAY AND PAVEMENT MARKING PLAN**

SCALE: 1"= 50'      SHEET      OF      SHEETS      STA.      TO      STA.

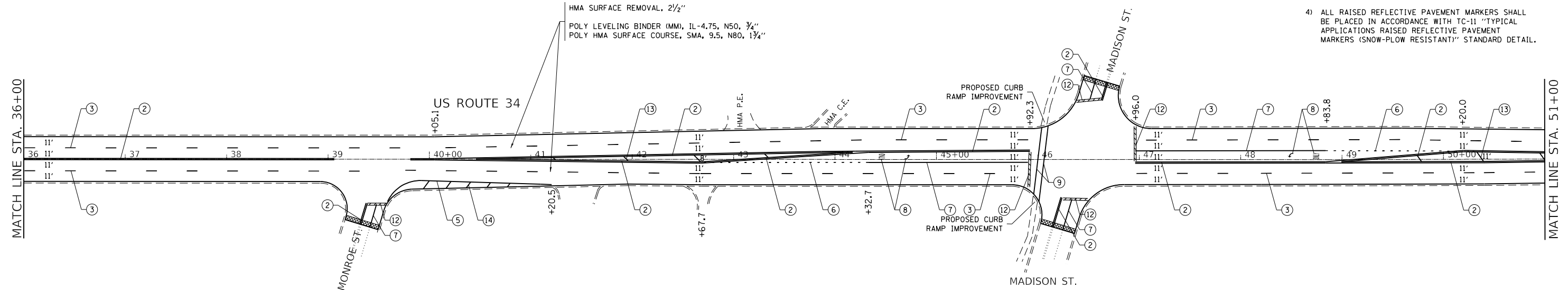
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-7	DUPAGE/COOK	35	12
CONTRACT NO. 62F56				
ILLINOIS FED. AID PROJECT				

**ROADWAY LEGEND**

☒ HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

**NOTES:**

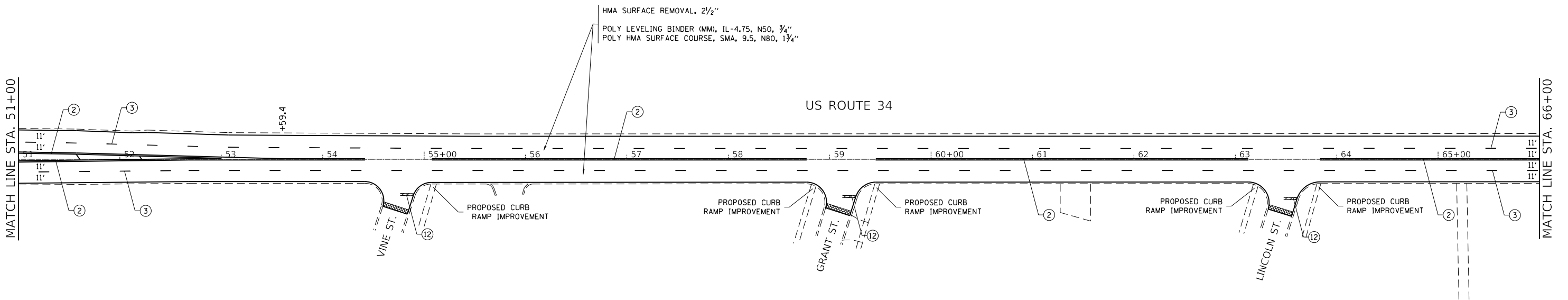
- 1) LIMIT OF RESURFACING ON THE SIDE- STREETS THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER / TECHNICIAN.
- 2) ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE).
- 3) ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH TC-13 "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" STANDARD DETAIL.
- 4) ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH TC-11 "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" STANDARD DETAIL.



**LEGEND: PROPOSED PAVEMENT STRIPING**

- |  |  |   |
|--|--|---|
| ① * LINE 4", SKIP-DASH, YELLOW (10' LINE - 30' SPACE) - CENTERLINE (TYP) | ⑦ * LINE 6", SOLID, WHITE - TURN LANE MARKINGS (TYP)   | ⑬ * LINE 12", SOLID, YELLOW - PAINTED MEDIANS DIAGONALS (TYP) (SEE DISTRICT DETAIL TC-13 FOR SPACING)   |
| ② * LINE 2 @ 4", SOLID, YELLOW, 11" C-C - CENTERLINE (TYP)               | ⑧ * LETTERS & SYMBOLS, 8", SOLID, WHITE - TURN LANE MARKINGS (TYP)   | ⑭ * LINE 12", SOLID, WHITE - DIAGONALS (BIKE & EQUESTRIAN) @ 45° (TYP) - SHOULDER DIAGONALS @ 45° (TYP) (SEE DISTRICT DETAIL TC-13 FOR SPACING) |
| ③ * LINE 4", SKIP-DASH, WHITE (10' LINE - 30' SPACE) - LANE LINES (TYP)  | ⑨ * LINE 2 @ 6", SOLID, WHITE - CROSSWALK LINES (PEDESTRIAN) (TYP)   | ⑮ * LINE 12", WHITE - GORE MARKING AND CHANNELIZATION LINES 45°. SEE TC-13 FOR DETAILS (TYP)  |
| ④ * LINE 4", SOLID, YELLOW - EDGE LINE (TYP)                             | ⑩ * LINE 8", SOLID, WHITE - GORE MARKING AND CHANNELIZING LINES (TYP)  |   |
| ⑤ * LINE 4", SOLID, WHITE - EDGE LINE (TYP)                              | ⑪ * LINE 12", SOLID, WHITE - CROSSWALK LINES, LONGITUDINAL BARS (SCHOOL) TYP (SEE DISTRICT DETAIL TC-13 FOR SPACING) |   |
| ⑥ * LINE 6", SKIP-DASH, WHITE (2' LINE - 6' SPACE) - DOTTED LINES (TYP)  | ⑫ * LINE 24", SOLID, WHITE - STOP LINES (TYP)  |   |

\* = THERMOPLASTIC PAVEMENT MARKING -



**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

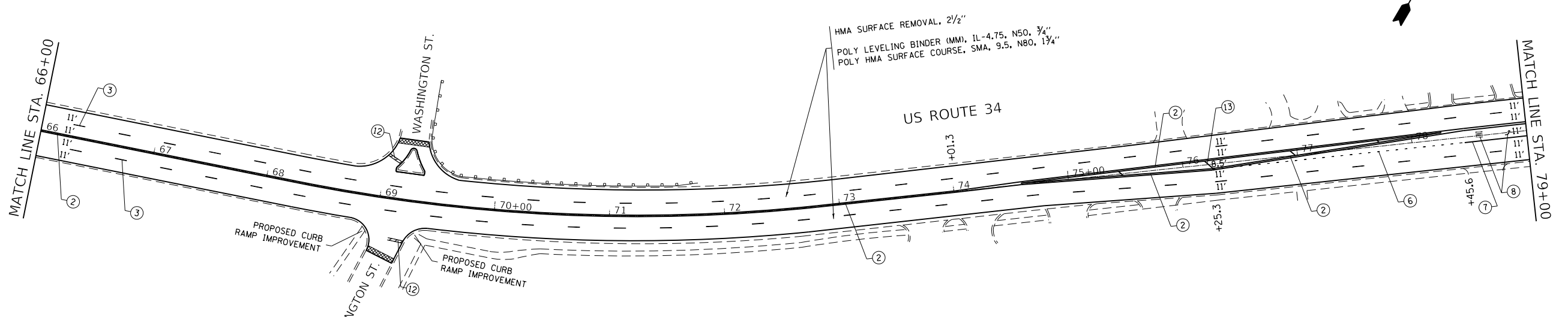
**US 34 (E. OF IL 83 (KINGERY HWY) TO W. OF I-294 (TRI-STATE TOLLWAY)  
ROADWAY AND PAVEMENT MARKING PLAN**

USER NAME = Velichkovv	DESIGNED -	REVISED -
PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -
PLOT DATE = 2/1/2018	CHECKED -	REVISED -
	DATE -	REVISED -

SCALE: 1"= 50'    SHEET    OF    SHEETS    STA.    TO    STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-7	DUPAGE/COOK	35	13
CONTRACT NO. 62F56				
ILLINOIS		FED. AID PROJECT		

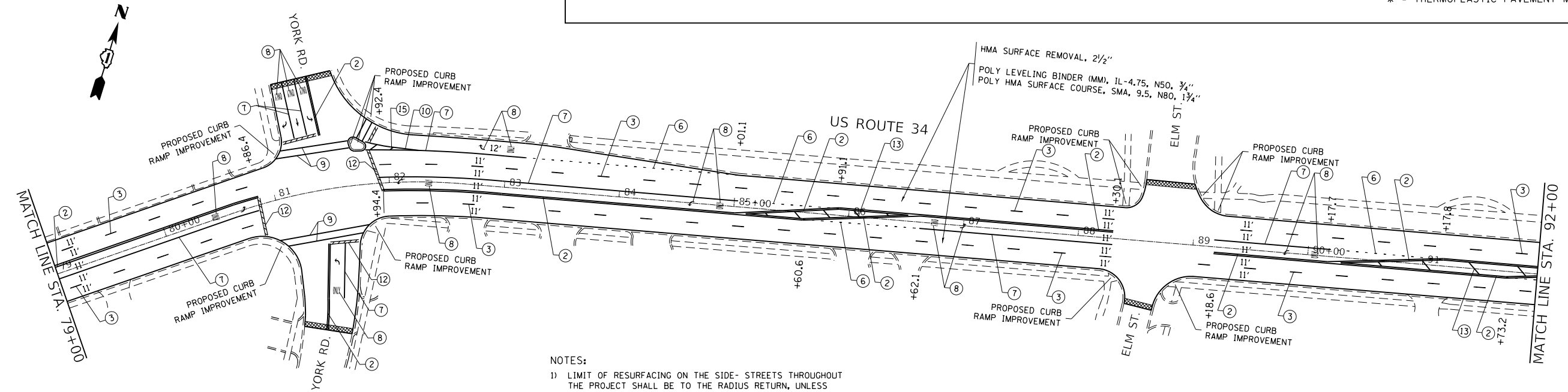
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**LEGEND: PROPOSED PAVEMENT STRIPING**

<ul style="list-style-type: none"> <li>1 * LINE 4", SKIP-DASH, YELLOW (10' LINE - 30' SPACE) - CENTERLINE (TYP)</li> <li>2 * LINE 2 @ 4", SOLID, YELLOW, 11" C-C - CENTERLINE (TYP)</li> <li>3 * LINE 4", SKIP-DASH, WHITE (10' LINE - 30' SPACE) - LANE LINES (TYP)</li> <li>4 * LINE 4", SOLID, YELLOW - EDGE LINE (TYP)</li> <li>5 * LINE 4", SOLID, WHITE - EDGE LINE (TYP)</li> <li>6 * LINE 6", SKIP-DASH, WHITE (2' LINE - 6' SPACE) - DOTTED LINES (TYP)</li> </ul>	<ul style="list-style-type: none"> <li>7 * LINE 6", SOLID, WHITE - TURN LANE MARKINGS (TYP)</li> <li>8 * LETTERS &amp; SYMBOLS, 8", SOLID, WHITE - TURN LANE MARKINGS (TYP)</li> <li>9 * LINE 2 @ 6", SOLID, WHITE - CROSSWALK LINES (PEDESTRIAN) (TYP)</li> <li>10 * LINE 8", SOLID, WHITE - GORE MARKING AND CHANNELIZING LINES (TYP)</li> <li>11 * LINE 12", SOLID, WHITE - CROSSWALK LINES, LONGITUDINAL BARS (SCHOOL) TYP (SEE DISTRICT DETAIL TC-13 FOR SPACING)</li> <li>12 * LINE 24", SOLID, WHITE - STOP LINES (TYP)</li> </ul>	<ul style="list-style-type: none"> <li>13 * LINE 12", SOLID, YELLOW - PAINTED MEDIANS DIAGONALS (TYP) (SEE DISTRICT DETAIL TC-13 FOR SPACING)</li> <li>14 * LINE 12", SOLID, WHITE - DIAGONALS (BIKE &amp; EQUESTRIAN) @ 45° (TYP) - SHOULDER DIAGONALS @ 45° (TYP) (SEE DISTRICT DETAIL TC-13 FOR SPACING)</li> <li>15 * LINE 12", WHITE - GORE MARKING AND CHANNELIZATION LINES 45°, SEE TC-13 FOR DETAILS (TYP)</li> </ul>
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\* = THERMOPLASTIC PAVEMENT MARKING -



- NOTES:**
- 1) LIMIT OF RESURFACING ON THE SIDE-STREETS THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER / TECHNICIAN.
  - 2) ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE).
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  - 4) ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH TC-11 "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-LOW RESISTANT)" STANDARD DETAIL.

**ROADWAY LEGEND**

[Pattern] HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

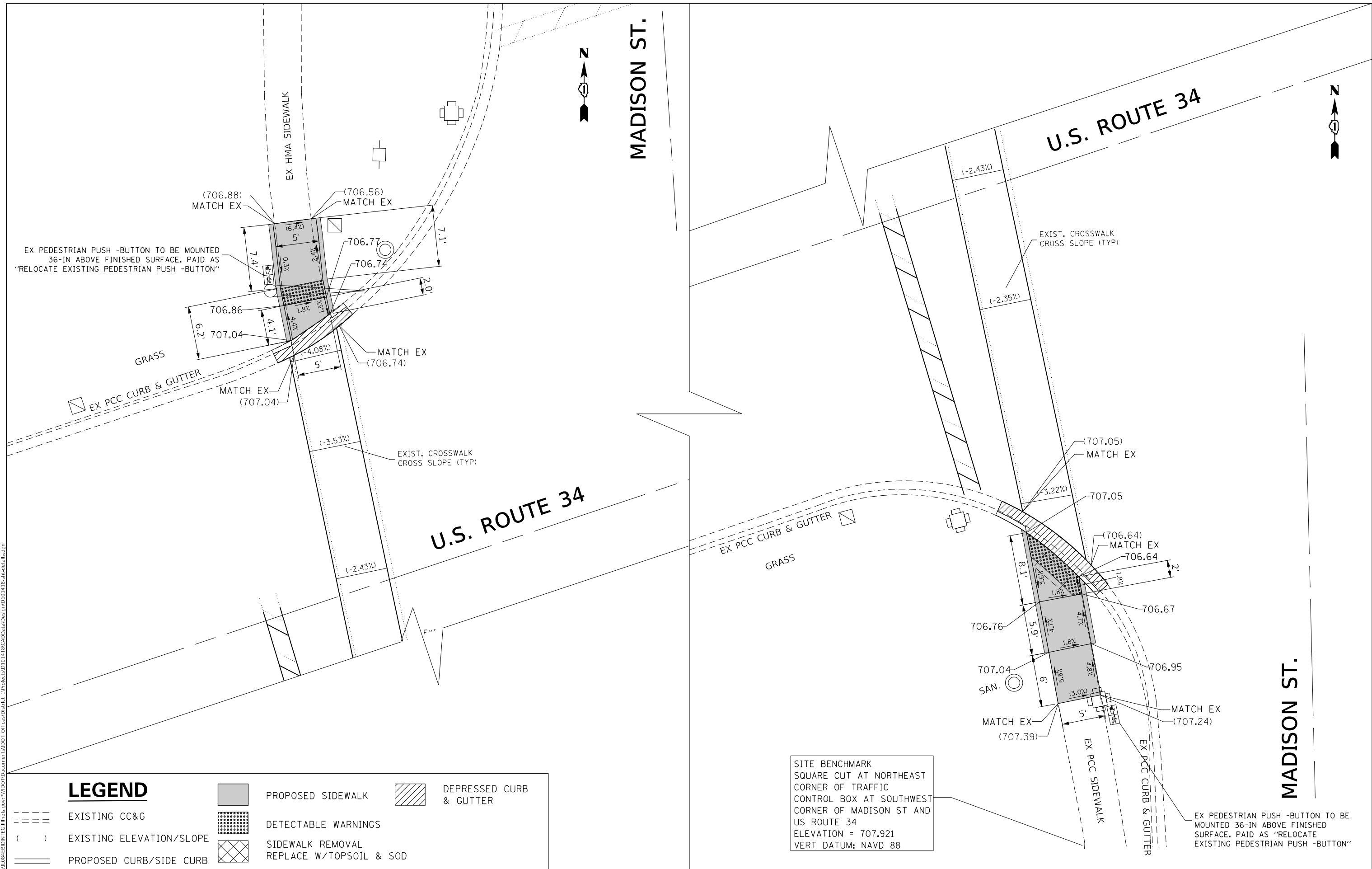
**US 34 (E. OF IL 83 (KINGERY HWY) TO W. OF I-294 (TRI-STATE TOLLWAY))  
ROADWAY AND PAVEMENT MARKING PLAN**

SCALE: 1" = 50'    SHEET    OF    SHEETS    STA.    TO    STA.

F.A.P. RTE. 311	SECTION 9Y-RS-7	COUNTY DUPAGE/COOK	TOTAL SHEETS 35	SHEET NO. 14
CONTRACT NO. 62F56				
ILLINOIS		FED. AID PROJECT		



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EX PEDESTRIAN PUSH -BUTTON TO BE MOUNTED 36-IN ABOVE FINISHED SURFACE. PAID AS "RELOCATE EXISTING PEDESTRIAN PUSH -BUTTON"

EX PEDESTRIAN PUSH -BUTTON TO BE MOUNTED 36-IN ABOVE FINISHED SURFACE. PAID AS "RELOCATE EXISTING PEDESTRIAN PUSH -BUTTON"

**LEGEND**

	EXISTING CC&G		PROPOSED SIDEWALK		DEPRESSED CURB & GUTTER
	EXISTING ELEVATION/SLOPE		DETECTABLE WARNINGS		SIDEWALK REMOVAL
	PROPOSED CURB/SIDE CURB		REPLACE W/TOPSOIL & SOD		

SITE BENCHMARK  
 SQUARE CUT AT NORTHEAST  
 CORNER OF TRAFFIC  
 CONTROL BOX AT SOUTHWEST  
 CORNER OF MADISON ST AND  
 US ROUTE 34  
 ELEVATION = 707.921  
 VERT DATUM: NAVD 88

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

US 34 (E. OF IL 83 (KINGERY HWY) TO W. OF I-294 (TRI-STATE TOLLWAY))  
 PEDESTRIAN (CURB) RAMPS IMPROVEMENT PLANS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-7	DUPAGE/COOK	35	16
CONTRACT NO. 62F56				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.



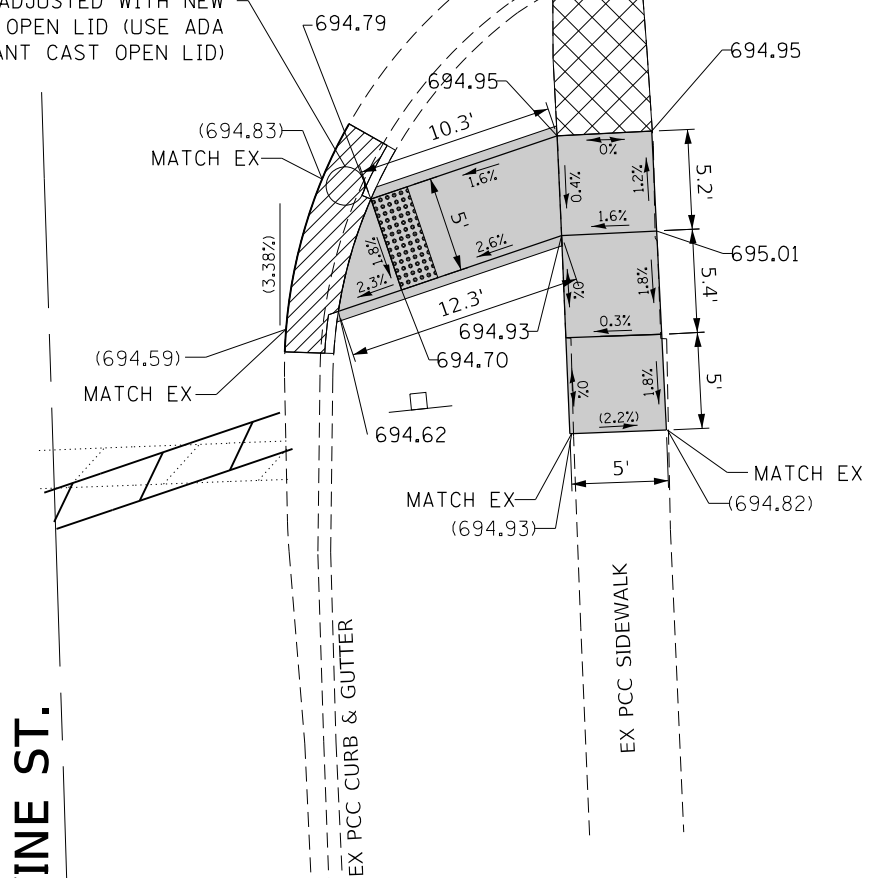


U.S. ROUTE 34

EX PCC CURB & GUTTER

GRASS

CATCH BASIN TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID (USE ADA COMPLIANT CAST OPEN LID)



VINE ST.

EX PCC CURB & GUTTER

EX PCC SIDEWALK

SITE BENCHMARK  
NORTHEAST BOLT OF FIRE  
HYDRANT AT SOUTHWEST  
CORNER OF GRANT ST AND  
US ROUTE 34  
ELEVATION = 694.462  
VERT DATUM: NAVD 88

**LEGEND**

- EXISTING CC&G
- ( ) EXISTING ELEVATION/SLOPE
- ==== PROPOSED CURB/SIDE CURB
- PROPOSED SIDEWALK
- ▒ DETECTABLE WARNINGS
- ▤ SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD
- ▨ DEPRESSED CURB & GUTTER

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	DRAWN -	REVISED -
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PLOT DATE = 3/20/2018	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

US 34 (E. OF IL 83 (KINGERY HWY) TO W. OF I-294 (TRI-STATE TOLLWAY))  
PEDESTRIAN (CURB) RAMPS IMPROVEMENT PLANS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-7	DUPAGE/COOK	35	16A
CONTRACT NO. 62F56			ILLINOIS FED. AID PROJECT	

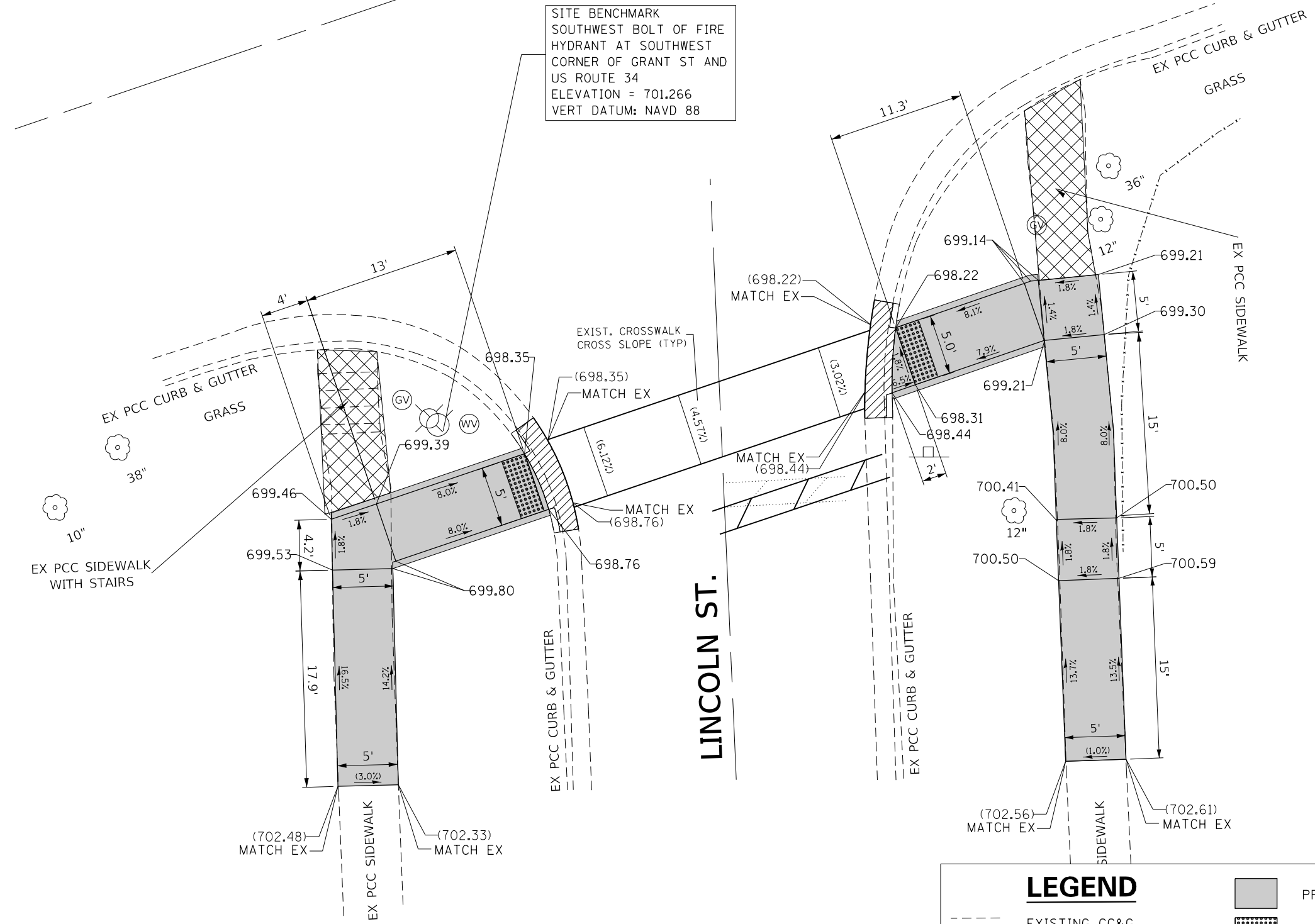
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**U.S. ROUTE 34**

SITE BENCHMARK  
SOUTHWEST BOLT OF FIRE  
HYDRANT AT SOUTHWEST  
CORNER OF GRANT ST AND  
US ROUTE 34  
ELEVATION = 701.266  
VERT DATUM: NAVD 88



**LEGEND**

	PROPOSED SIDEWALK		DEPRESSED CURB & GUTTER
	EXISTING CC&G		DETECTABLE WARNINGS
	EXISTING ELEVATION/SLOPE		SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD
	PROPOSED CURB/SIDE CURB		

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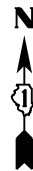
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PLOT DATE = 3/20/2018	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

US 34 (E. OF IL 83 (KINGERY HWY) TO W. OF I-294 (TRI-STATE TOLLWAY))  
PEDESTRIAN (CURB) RAMPS IMPROVEMENT PLANS

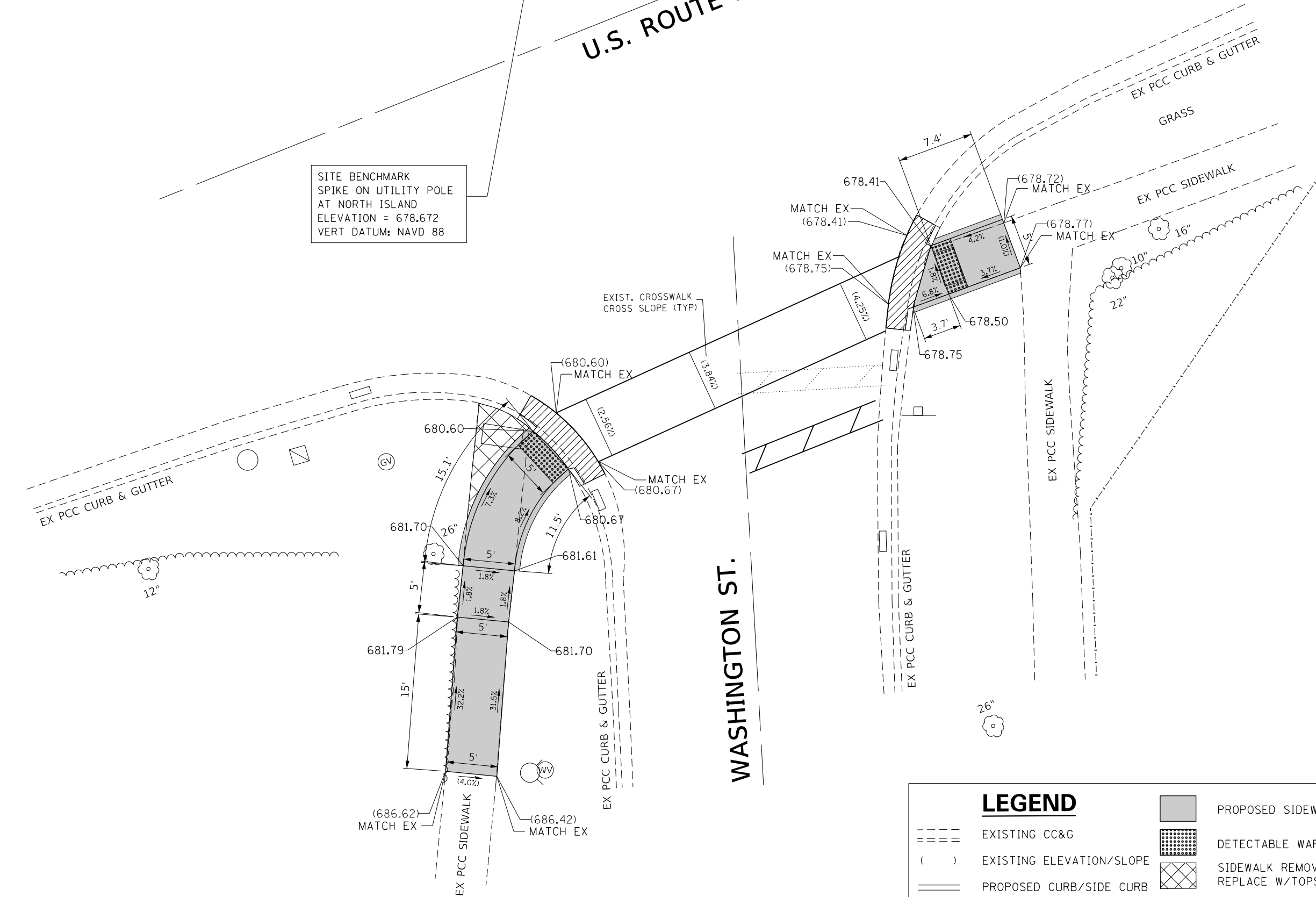
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-7	DUPAGE/COOK	35	16C
				CONTRACT NO. 62F56
				ILLINOIS FED. AID PROJECT



U.S. ROUTE 34

SITE BENCHMARK  
SPIKE ON UTILITY POLE  
AT NORTH ISLAND  
ELEVATION = 678.672  
VERT DATUM: NAVD 88



**LEGEND**

- EXISTING CC&G
- PROPOSED CURB/SIDE CURB
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- DEPRESSED CURB & GUTTER
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

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PLOT DATE = 3/20/2018	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**








**US 34 (E. OF IL 83 (KINGERY HWY) TO W. OF I-294 (TRI-STATE TOLLWAY))  
PEDESTRIAN (CURB) RAMPS IMPROVEMENT PLANS**

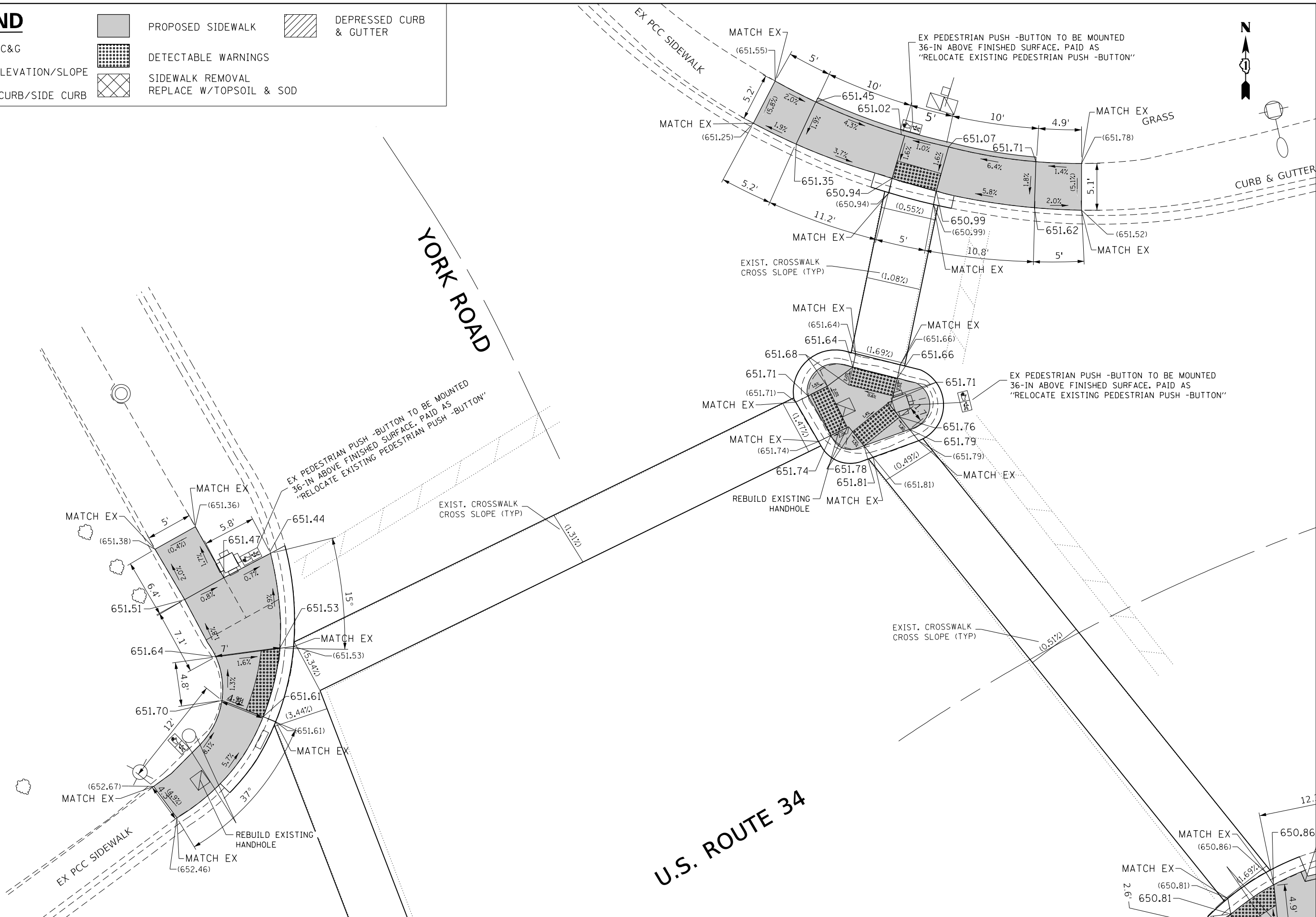
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F.A.P. RTE. 311	SECTION 9Y-RS-7	COUNTY DUPAGE/COOK	TOTAL SHEETS 35	SHEET NO. 16D
CONTRACT NO. 62F56				

ILLINOIS FED. AID PROJECT

# LEGEND

-  EXISTING CC&G
-  EXISTING ELEVATION/SLOPE
-  PROPOSED CURB/SIDE CURB
-  PROPOSED SIDEWALK
-  DETECTABLE WARNINGS
-  SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD
-  DEPRESSED CURB  
& GUTTER



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PLOT DATE = 3/20/2018	DATE -	REVISED -

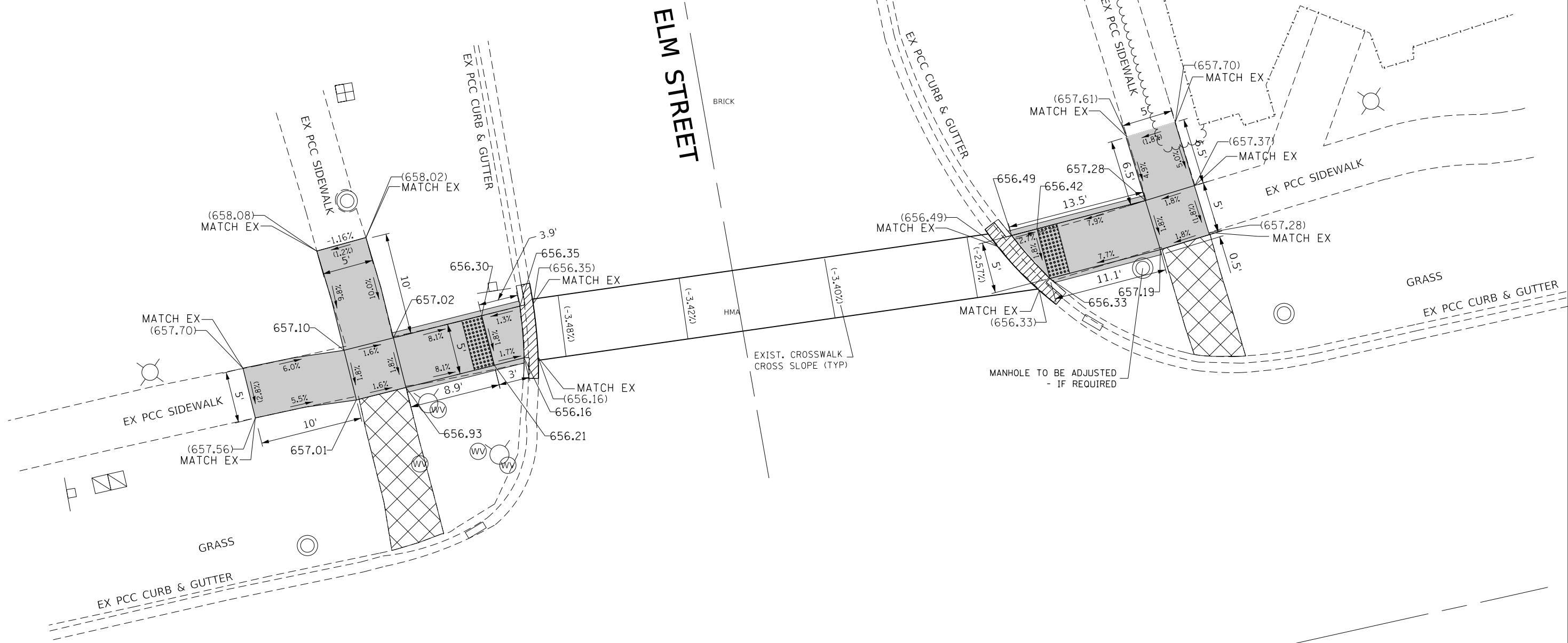
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**US 34 (E. OF IL 83 (KINGERY HWY) TO W. OF I-294 (TRI-STATE TOLLWAY))**  
**PEDESTRIAN (CURB) RAMPS IMPROVEMENT PLANS**

SCALE:      SHEET      OF      SHEETS      STA.      TO      STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-7	DUPAGE/COOK	35	16E
CONTRACT NO. 62F56				
ILLINOIS FED. AID PROJECT				





**U.S. ROUTE 34**

LEGEND	
	EXISTING CC&G
	PROPOSED CURB/SIDE CURB
	PROPOSED SIDEWALK
	DETECTABLE WARNINGS
	DEPRESSED CURB & GUTTER
	SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

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PLOT DATE = 3/20/2018	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

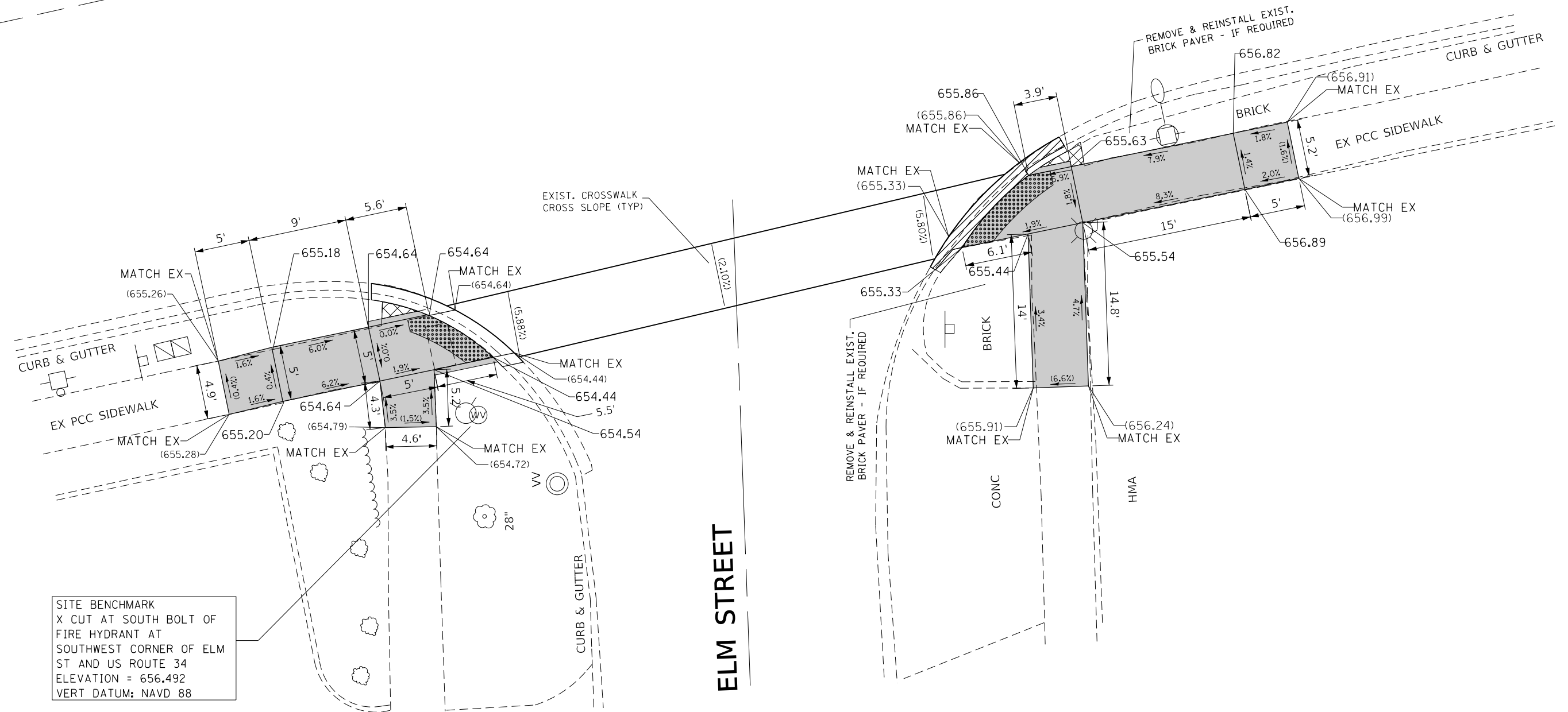
**US 34 (E. OF IL 83 (KINGERY HWY) TO W. OF I-294 (TRI-STATE TOLLWAY))  
PEDESTRIAN (CURB) RAMPS IMPROVEMENT PLANS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-7	DUPAGE/COOK	35	16G
CONTRACT NO. 62F56			ILLINOIS FED. AID PROJECT	



U.S. ROUTE 34



SITE BENCHMARK  
 X CUT AT SOUTH BOLT OF  
 FIRE HYDRANT AT  
 SOUTHWEST CORNER OF ELM  
 ST AND US ROUTE 34  
 ELEVATION = 656.492  
 VERT DATUM: NAVD 88

MODEL: Default  
 FILE: Name: p:\1108\B&E\BID\TEC\Illinois\gov\PR\DOT\Documents\310118\CAD\Drawings\310118-eh-02.dgn  
 PROJECT: 310118-CAD\Drawings\310118-eh-02.dgn

LEGEND	
	PROPOSED SIDEWALK
	DEPRESSED CURB & GUTTER
	EXISTING CC&G
	DETECTABLE WARNINGS
	EXISTING ELEVATION/SLOPE
	SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD
	PROPOSED CURB/SIDE CURB

USER NAME = Velichkovv	DESIGNED -	REVISED -
PLOT SCALE = 10.0000 ' / in.	DRAWN -	REVISIED -
PLOT DATE = 3/20/2018	CHECKED -	REVISIED -
	DATE -	REVISIED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**US 34 (E. OF IL 83 (KINGERY HWY) TO W. OF I-294 (TRI-STATE TOLLWAY))  
 PEDESTRIAN (CURB) RAMPS IMPROVEMENT PLANS**

SCALE: SHEET OF SHEETS STA. TO STA.

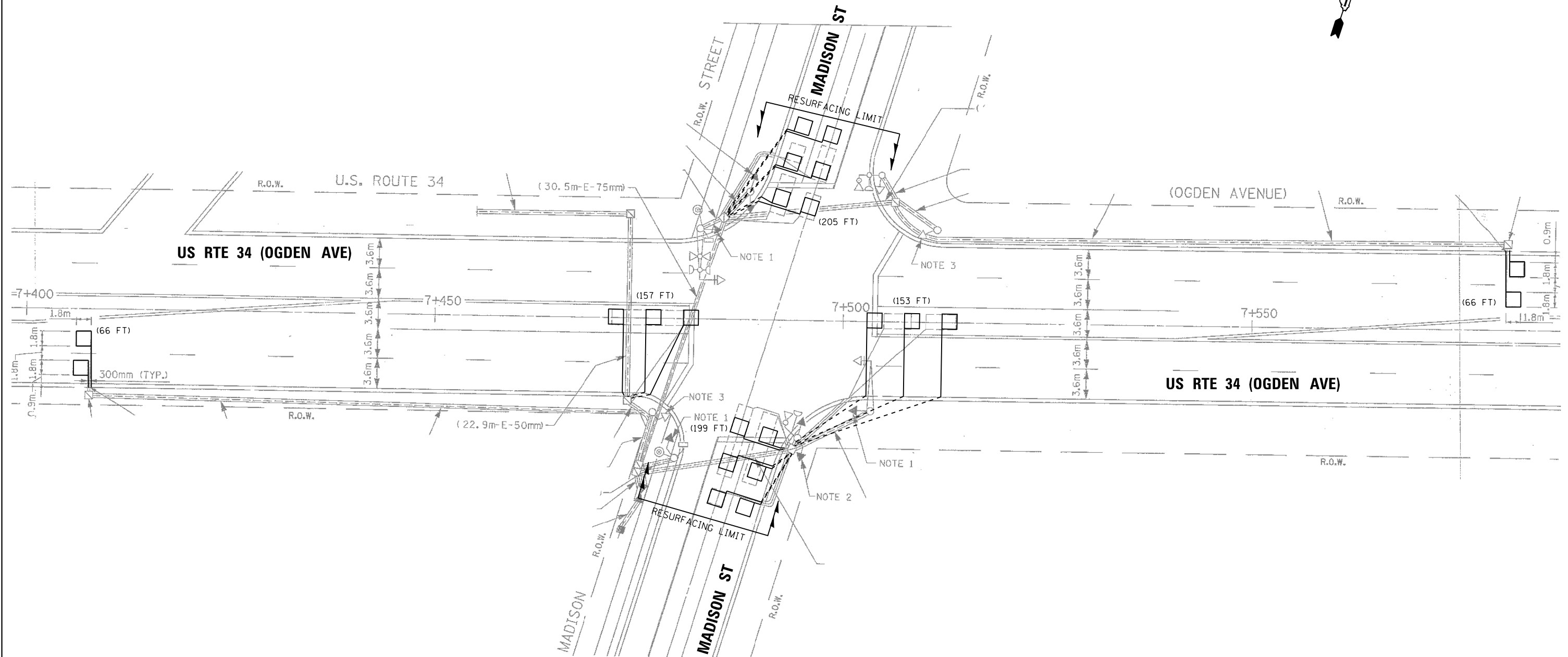
F.A.P. RTE. 311	SECTION 9Y-RS-7	COUNTY DUPAGE/COOK	TOTAL SHEETS 35	SHEET NO. 16H
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62F56	





**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	847	FOOT

FILE NAME = TS6130 - US RTE 34 AT MADISON ST_01312018.dgn	USER NAME = rostkowski	DESIGNED - BAS	REVISED -
		DRAWN - BAS	REVISED -
	PLOT SCALE = 40.0000' / in.	CHECKED - LP	REVISED -
Default	PLOT DATE = 1/31/2018	DATE - 09/13/2017	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN  
US RTE 34 (OGDEN AVE) AT MADISON ST**

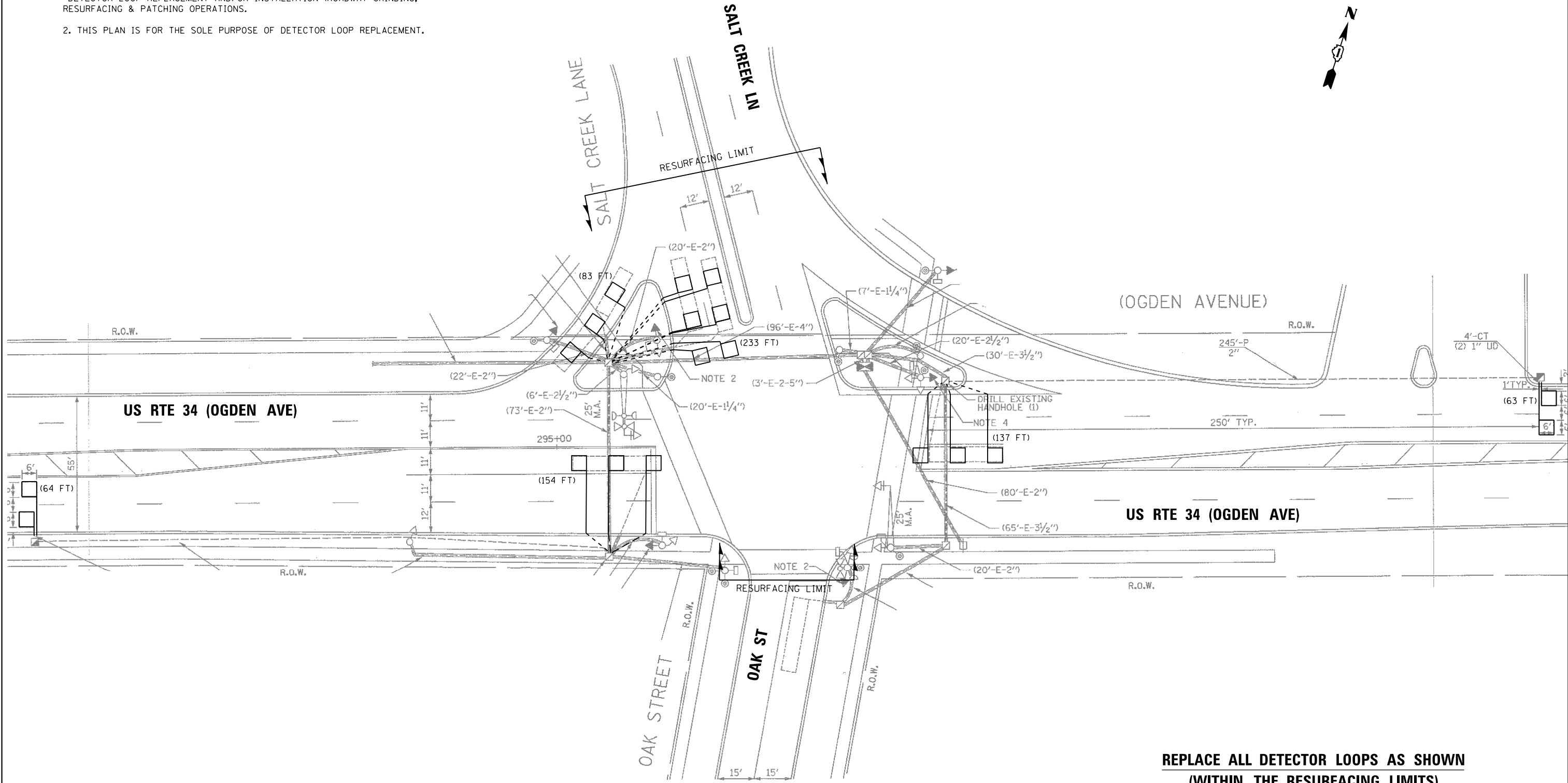
SCALE: NONE SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 311	SECTION 9Y-RS-7	COUNTY DUPAGE	TOTAL SHEETS 35	SHEET NO. 17
CONTRACT NO. 62F56				
ILLINOIS FED. AID PROJECT				



**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

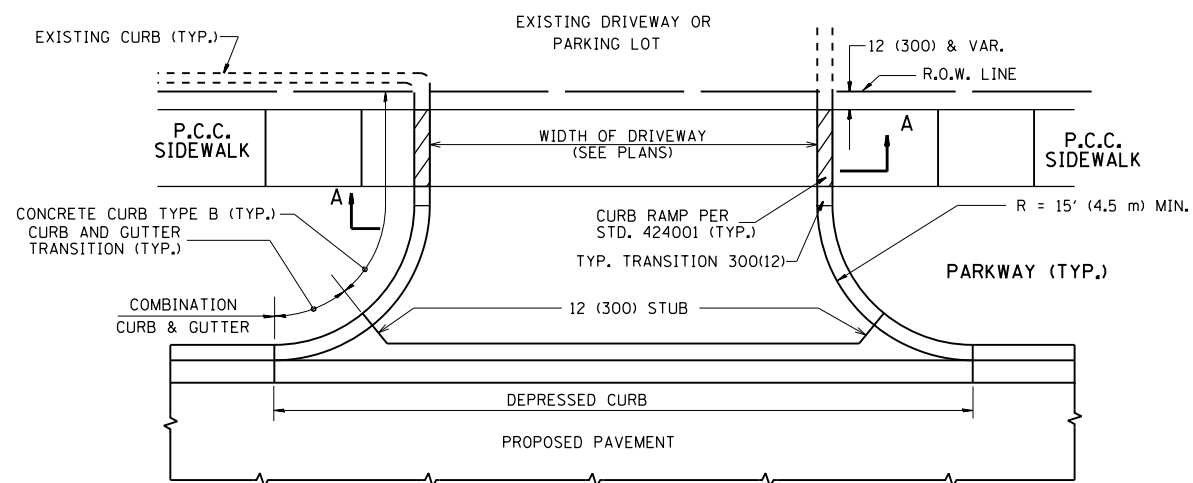
CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	735	FOOT

FILE NAME = TS6116 - US RTE 34 AT SALT CREEK LN-OAK ST.dgn	USER NAME = rostkowski	DESIGNED - BAS	REVISED -
Default	ST_01312018.dgn	DRAWN - BAS	REVISED -
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	PLOT DATE = 1/31/2018	DATE - 09/13/2017	REVISED -

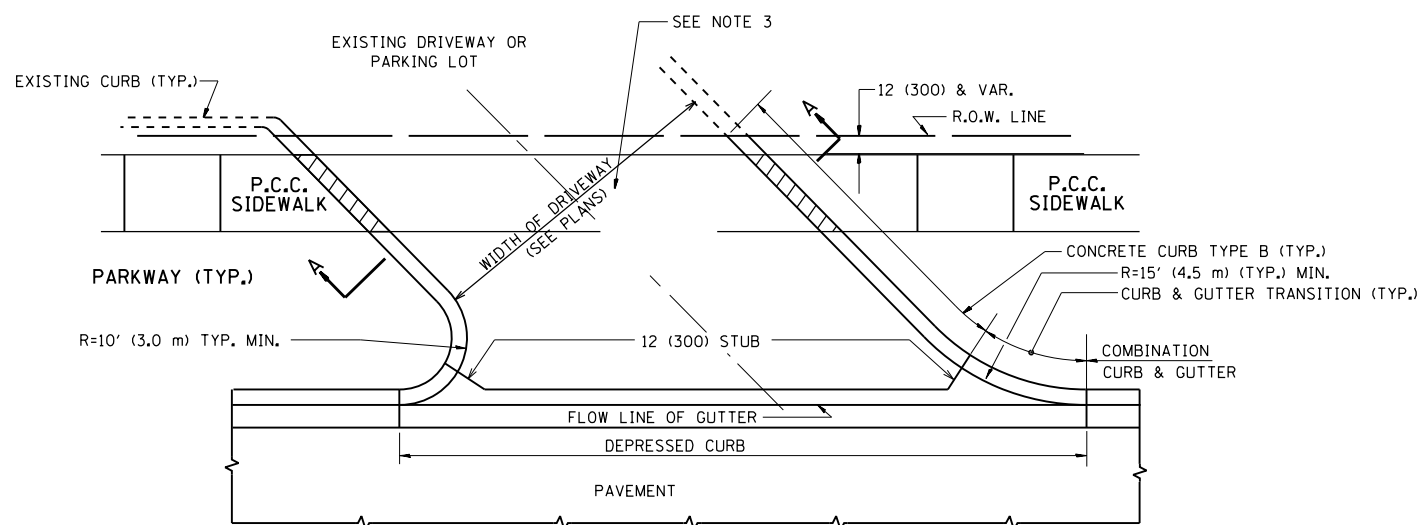
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DETECTOR LOOP REPLACEMENT PLAN</b>			
<b>US RTE 34 (OGDEN AVE) AT SALT CREEK LN /OAK ST</b>			
SCALE: NONE	SHEET OF SHEETS	STA. TO STA.	

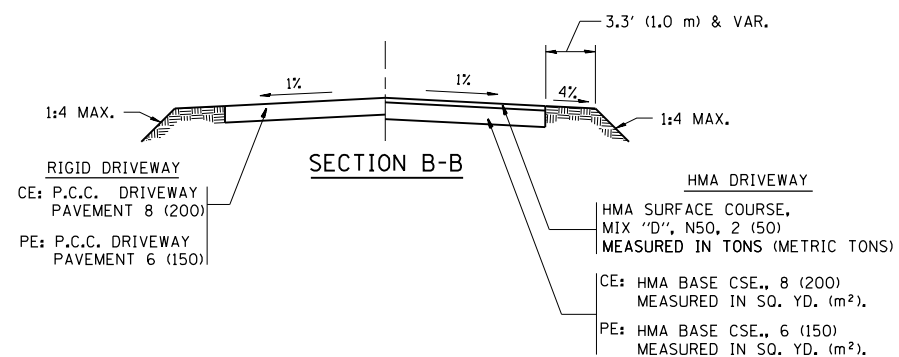
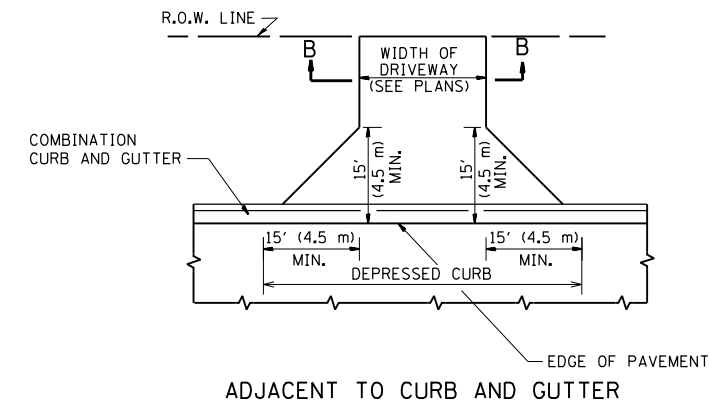
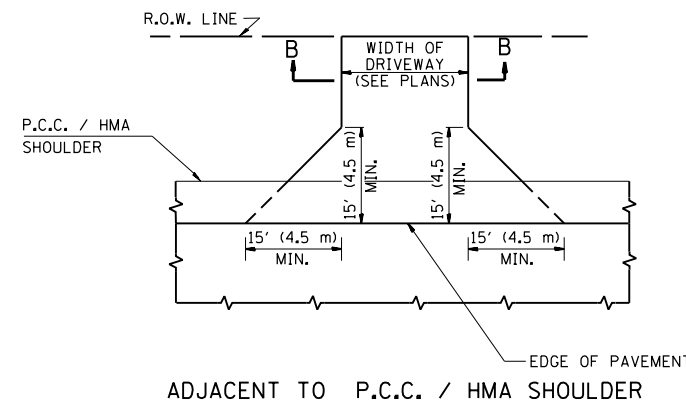
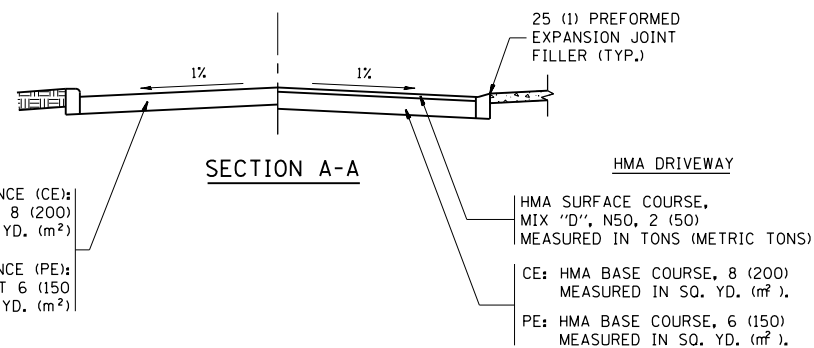
F.A.P. RTE. 311	SECTION 9Y-RS-7	COUNTY DUPAGE	TOTAL SHEETS 35	SHEET NO. 19
CONTRACT NO. 62F56				
ILLINOIS FED. AID PROJECT				



WITH CONCRETE CURB, TYPE B



WITH CONCRETE CURB, TYPE B



RURAL FIELD ENTRANCE (FE)  
 HMA SURFACE COURSE,  
 MIX "D", N50, 2 (50)  
 MEASURED IN TONS (METRIC TONS)  
 AGGREGATE BASE CSE., TYPE B, 8 (200)  
 MEASURED IN SQ. YD. (m²).

**GENERAL NOTES:**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

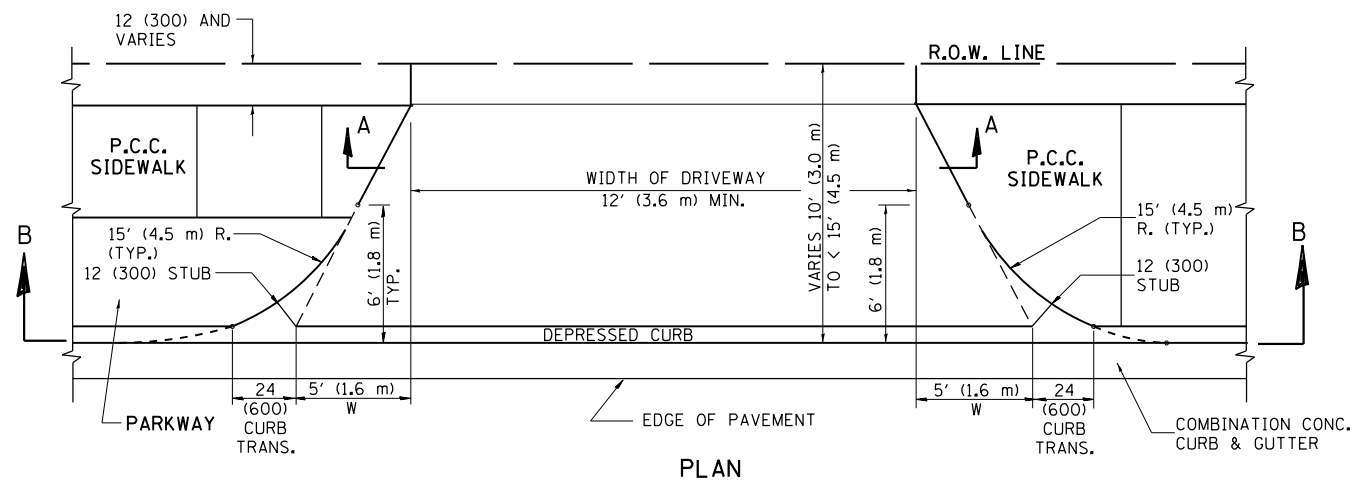
WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

FILE NAME =	USER NAME = Velichkovv	DESIGNED - R. SHAH	REVISED - P. LaFLUER 04-15-03
pw:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI01\BROW\Notes\Design\DistStd.dgn		CHECKED -	REVISED - R. BORO 01-01-07
		DATE - 11-04-95	REVISED - R. BORO 06-11-08
			REVISED - R. BORO 09-06-11

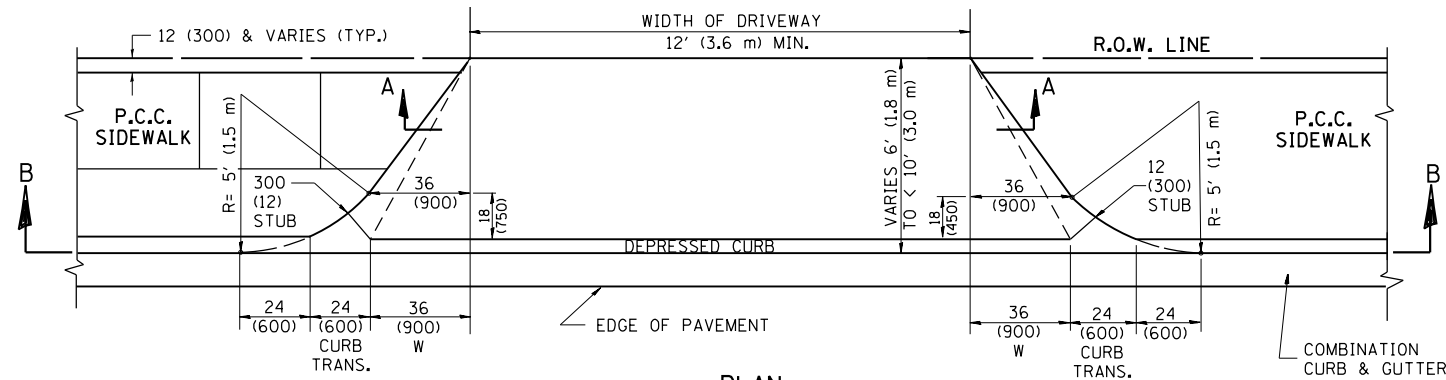
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W.    AND FACE OF CURB &amp; EDGE OF SHOULDER &gt;= 15' (4.5 m)</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

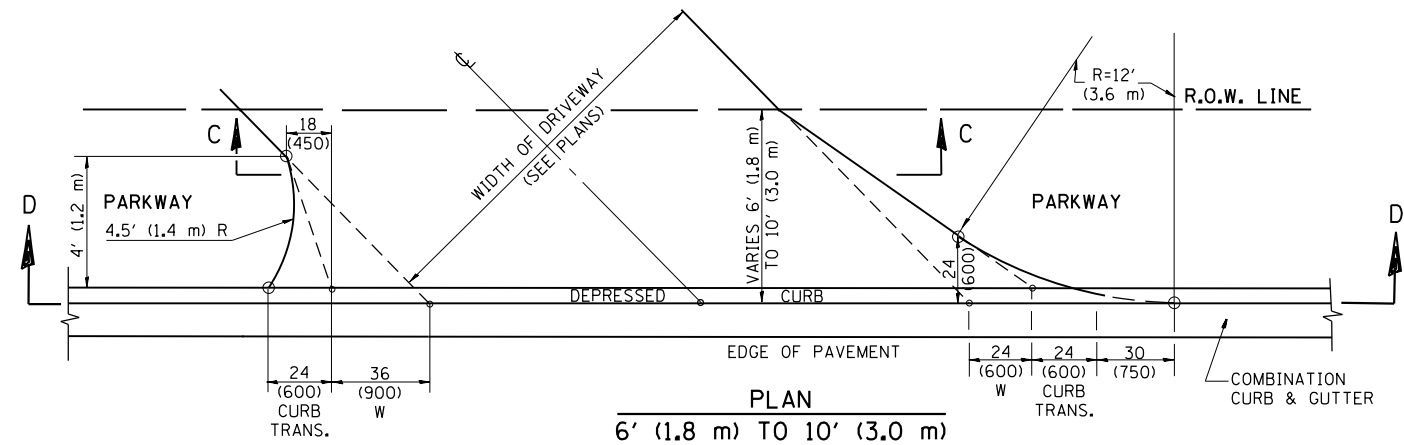
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-7	DuPAGE/COOK	35	20
<b>BD0156-07 (BD-01)</b>		<b>CONTRACT NO.</b>	62F59	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



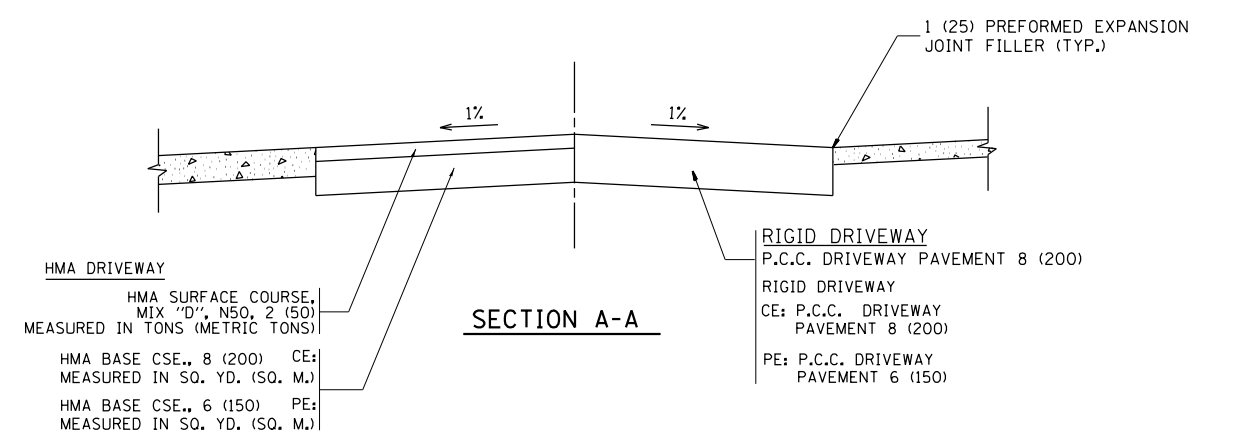
PLAN  
10' (3.0 m) TO < 15' (4.5 m)



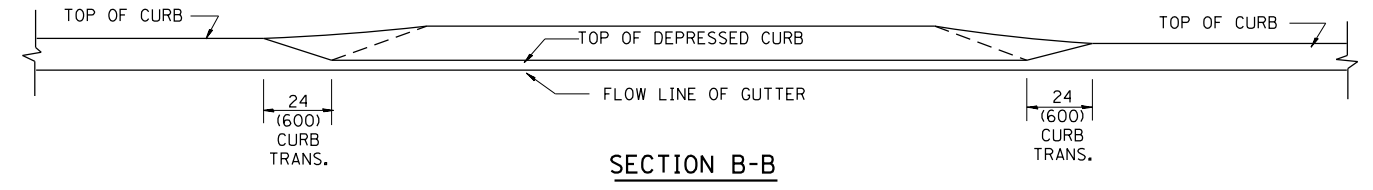
PLAN  
6' (1.8 m) TO < 10' (3.0 m)



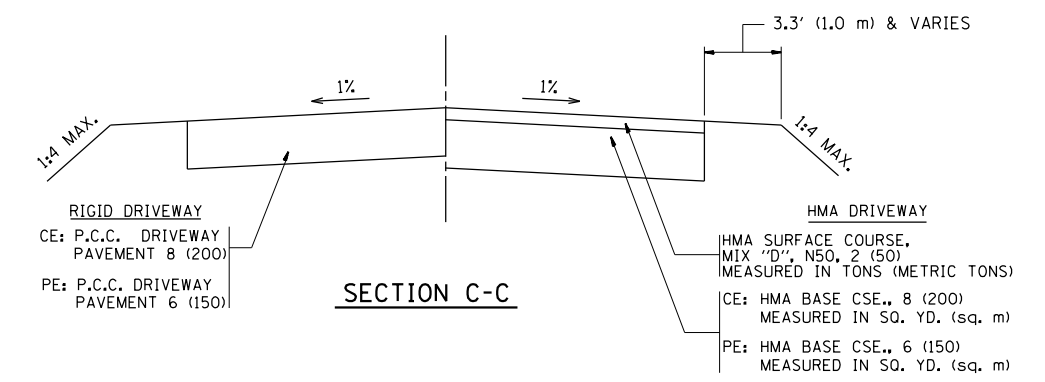
PLAN  
6' (1.8 m) TO 10' (3.0 m)



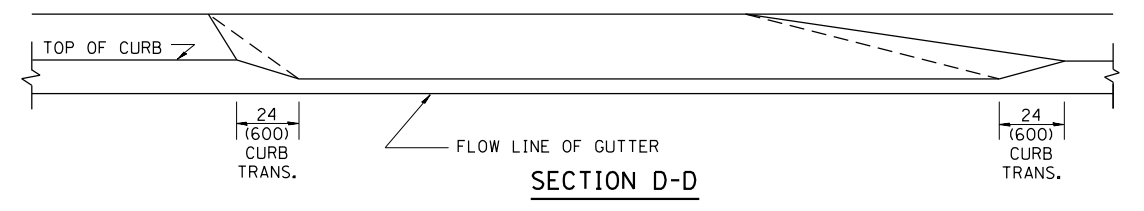
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

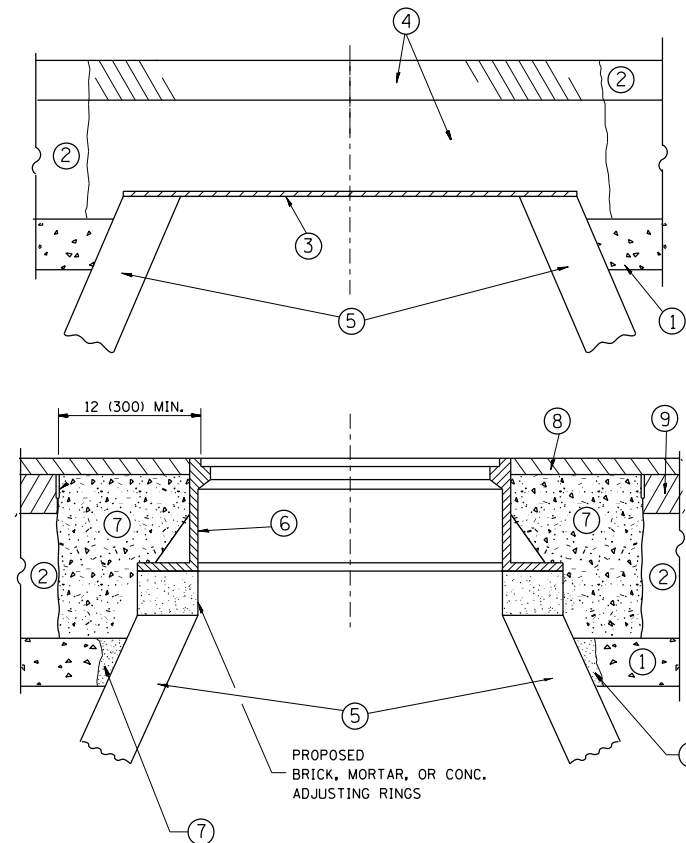
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME =	USER NAME = Velichkovv	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-06-01
pw\11\084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI01\BRAW\Notes\Design\DistStd.dgn		CHECKED -	REVISED - P. LaFLEUR 04-15-03
PLOT SCALE = 100.0000' / 1"		DATE - 11-06-95	REVISED - R. BORO 01-01-07
PLOT DATE = 2/1/2018			REVISED - R. BORO 01-01-07

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS	
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-7	DUPAGE/COOK	35	21
BD400-02 (BD-02)			CONTRACT NO. 62F59	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

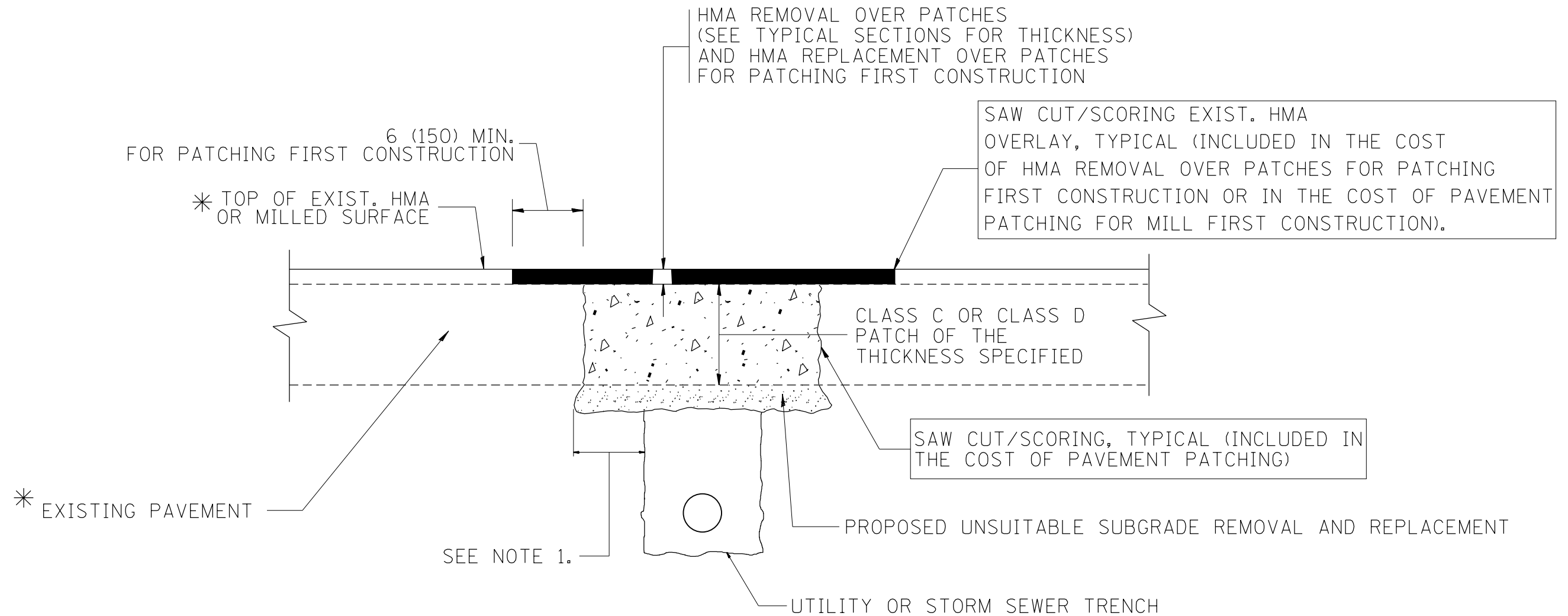
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = Velichkovv	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
pw:\IL\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI01\BRAW\Notes\Design\DistStd.dgn			REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 2/1/2018	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-7	DuPAGE/COOK	35	22
<b>BD600-03 (BD-8)</b>		<b>CONTRACT NO.</b>	62F59	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

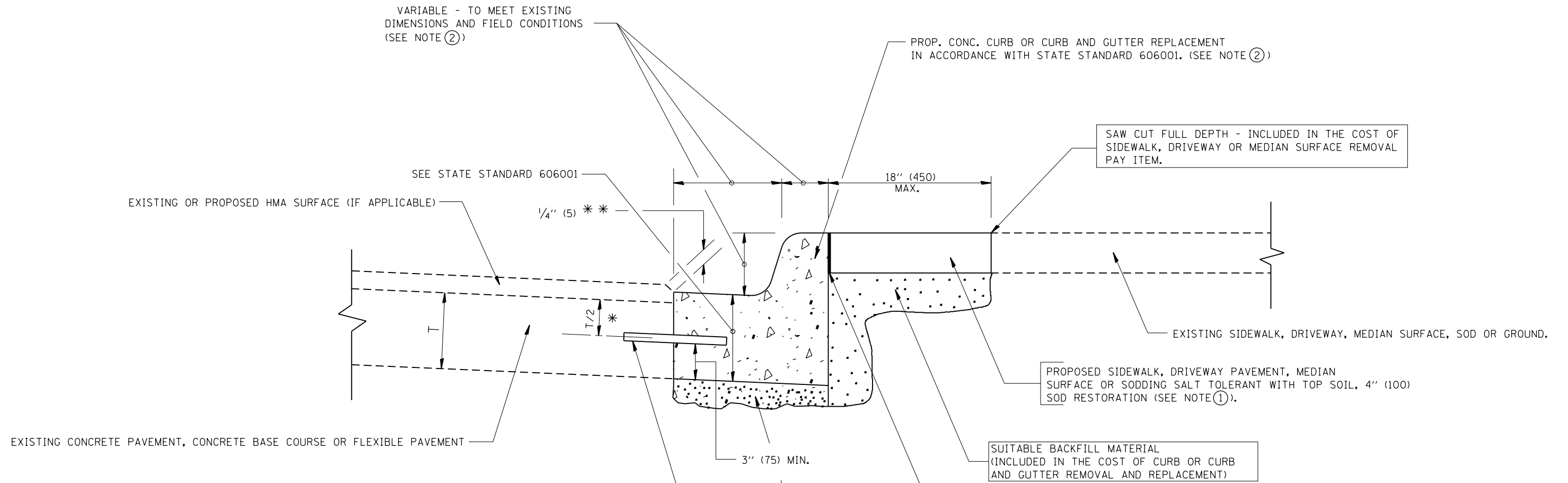
SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = Velichkovv	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI01\BRAW\Design\DistStd.dgn	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 01-01-07					311	9Y-RS-7	DuPAGE/COOK	35	23
	PLOT DATE = 2/1/2018	DATE - 10-25-94	REVISED - R. BORO 09-04-07		<b>BD400-04 (BD-22)</b>			<b>CONTRACT NO. 62F59</b>				
			REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
  - \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
  - ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
  - ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
  - ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
  - ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
  - ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
  - ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

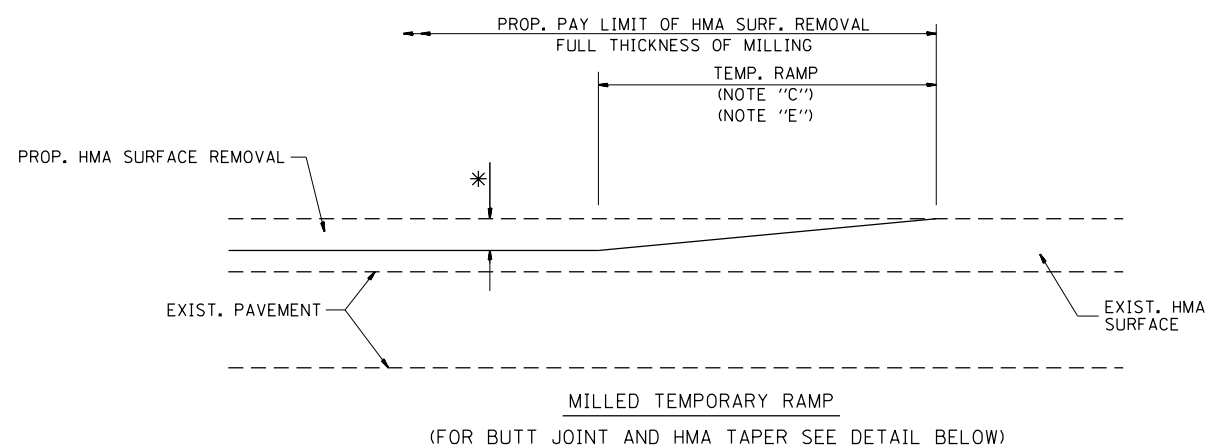
- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**  
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

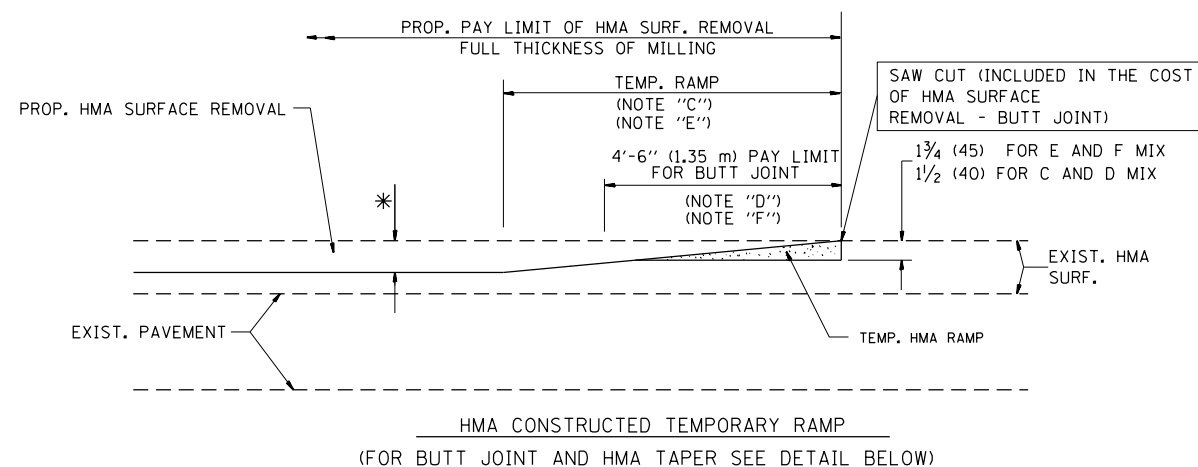
# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = Velichkovv	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
			REVISED - A. ABBAS 03-21-97			311	9Y-RS-7	DuPAGE/COOK	35	24	
			REVISED - M. GOMEZ 01-22-01			<b>BD600-06 (BD-24)</b>		<b>CONTRACT NO. 62F59</b>			
			REVISED - R. BORO 12-15-09			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

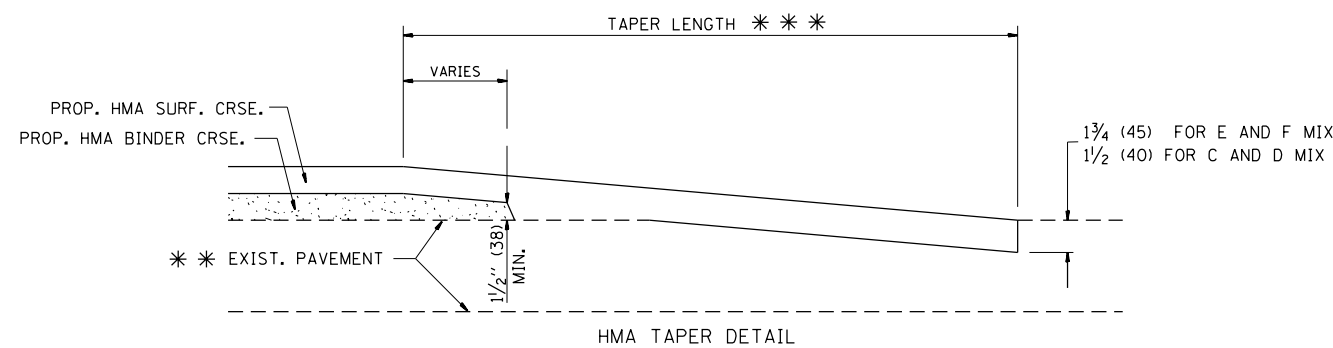
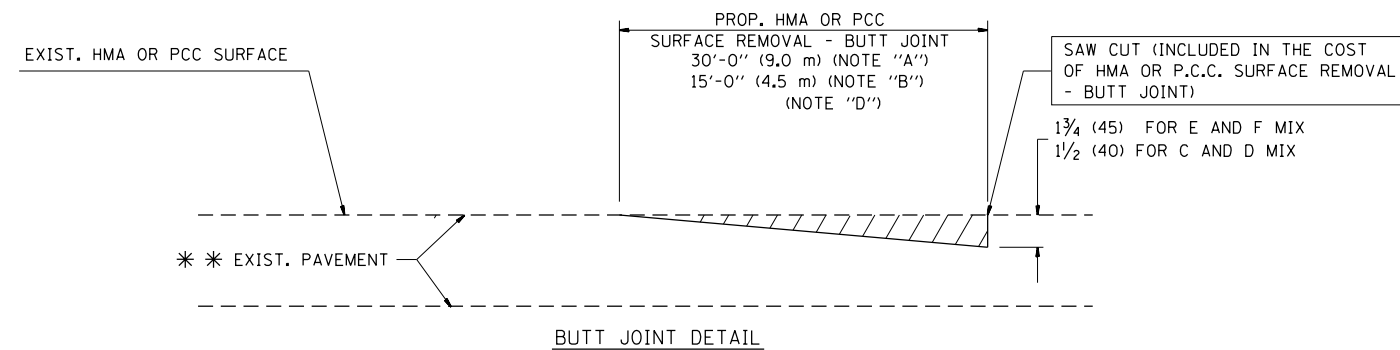


**OPTION 1**



**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\* \* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

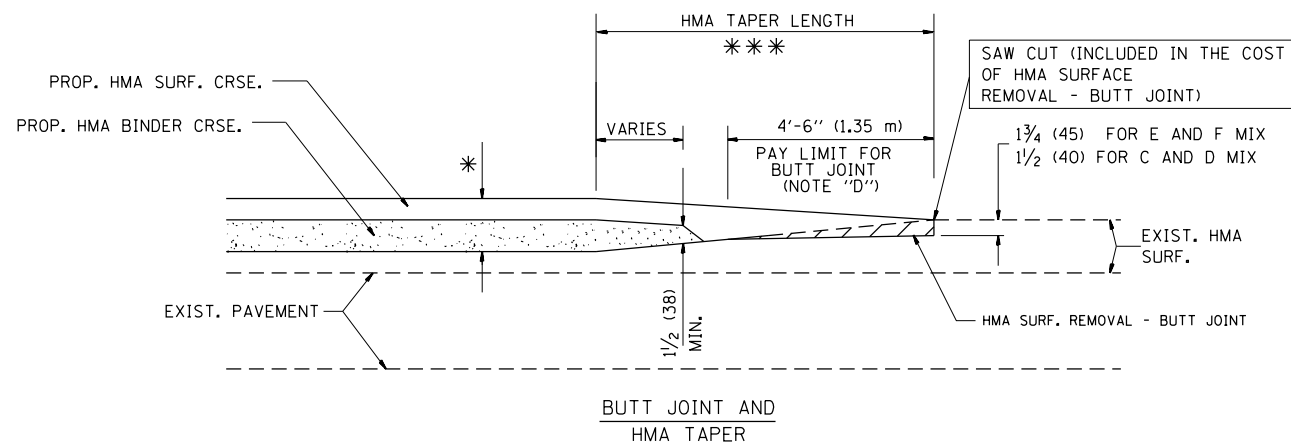
\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

\* \* \* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

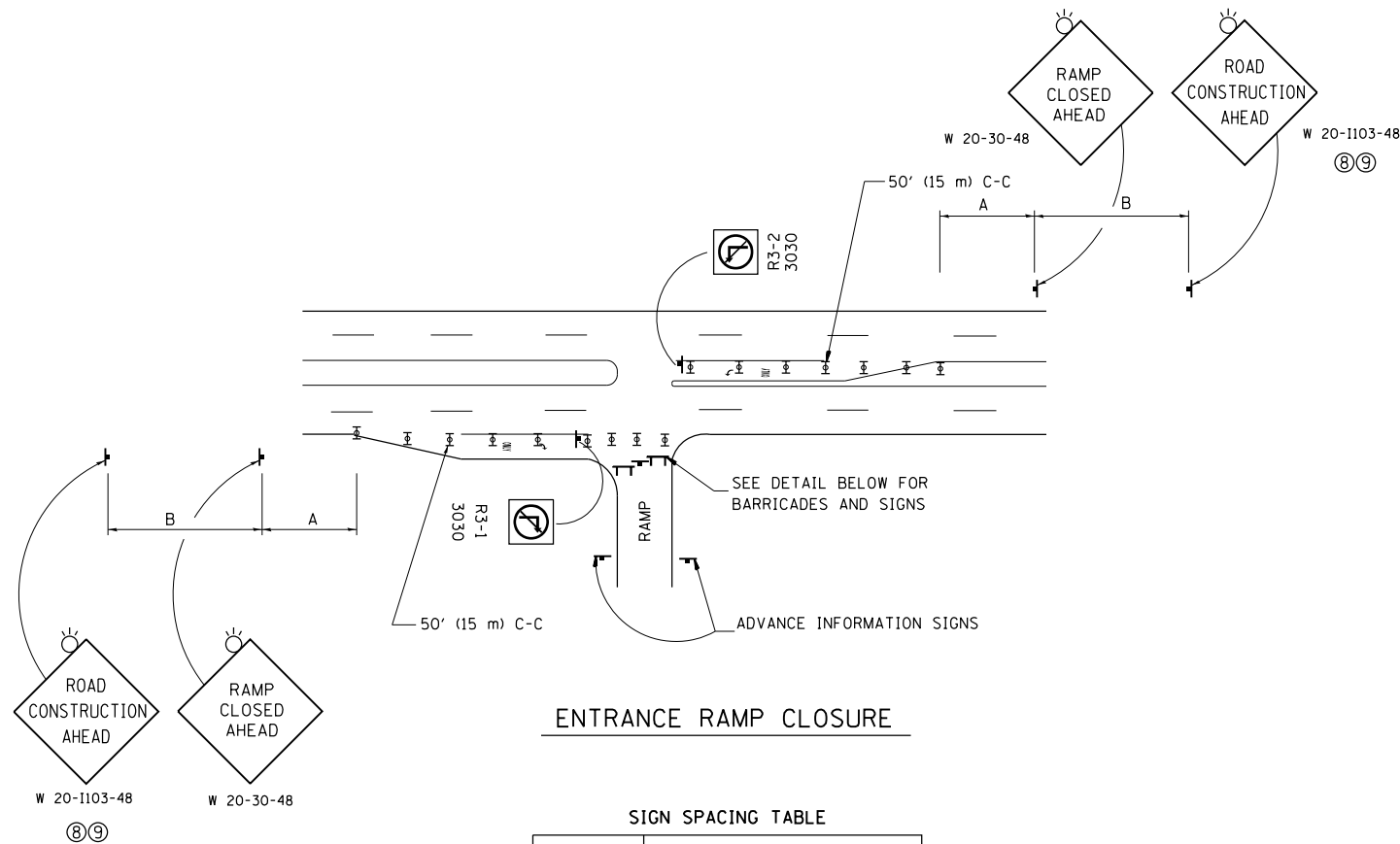
FILE NAME =	USER NAME = Velichkovv	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
pw\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI01\BRAW\Notes\Design\DistStd.dgn			REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 2/1/2018	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND  
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-7	DuPAGE/COOK	35	25
<b>BD400-05 BD32</b>		<b>CONTRACT NO.</b>	62F59	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

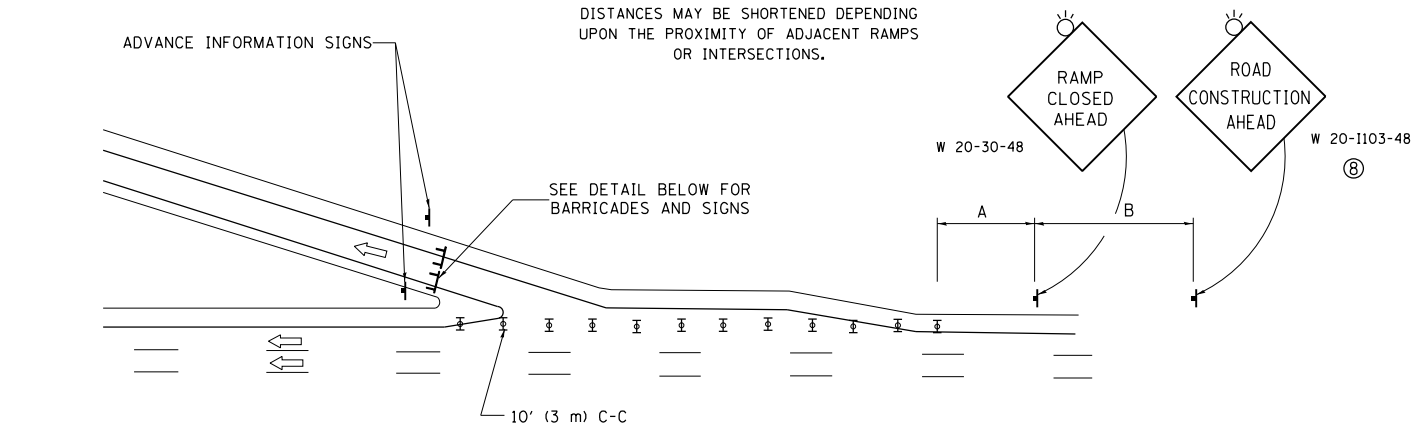


**ENTRANCE RAMP CLOSURE**

**SIGN SPACING TABLE**

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY <24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

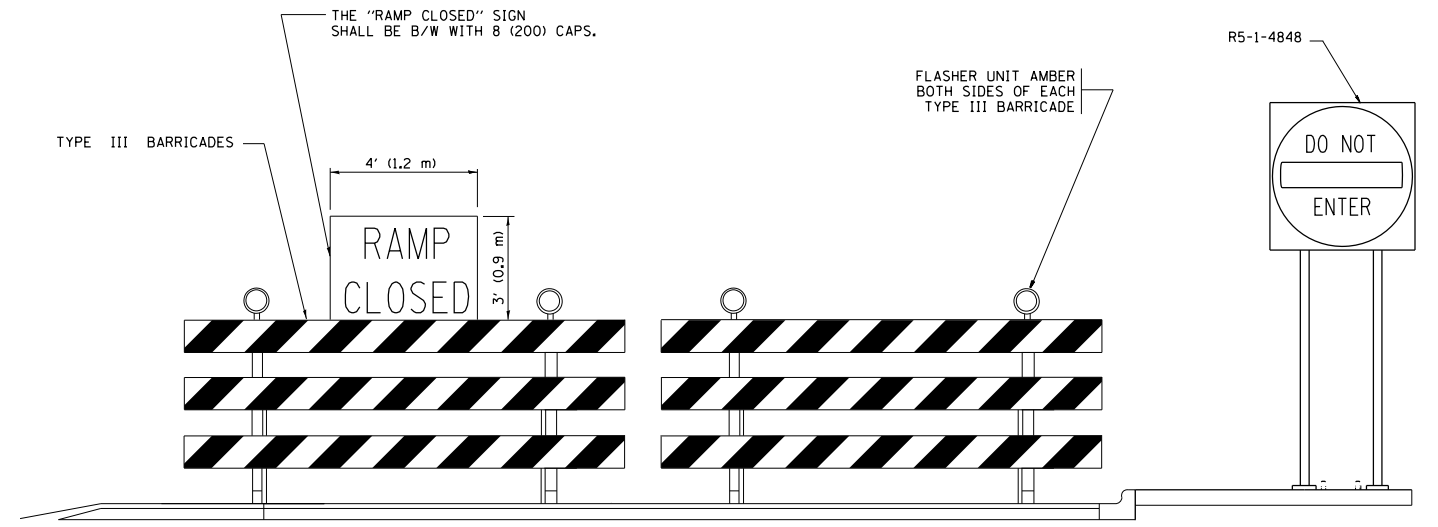
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



**EXIT RAMP CLOSURE**

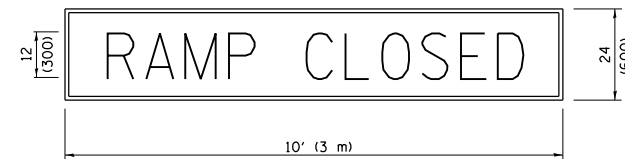
**SYMBOLS**

- ⊥ TYPE II BARRICADE OR DRUM
- ⊥ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



**DETAIL FOR REQUIRED BARRICADES & SIGNS**

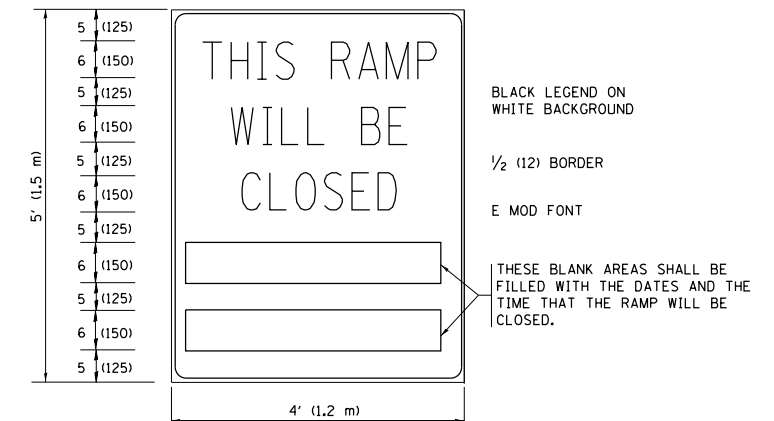
**RAMP CLOSURE ADVANCE WARNING SIGN**



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY  
E MOD FONT  
1 (25) BORDER

THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

**RAMP CLOSURE ADVANCE INFORMATION SIGN**



BLACK LEGEND ON WHITE BACKGROUND

1/2 (12) BORDER

E MOD FONT

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

**GENERAL NOTES:**

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② VERTICAL BARRICADES SHALL NOT BE USED FOR RAMP CLOSURES.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

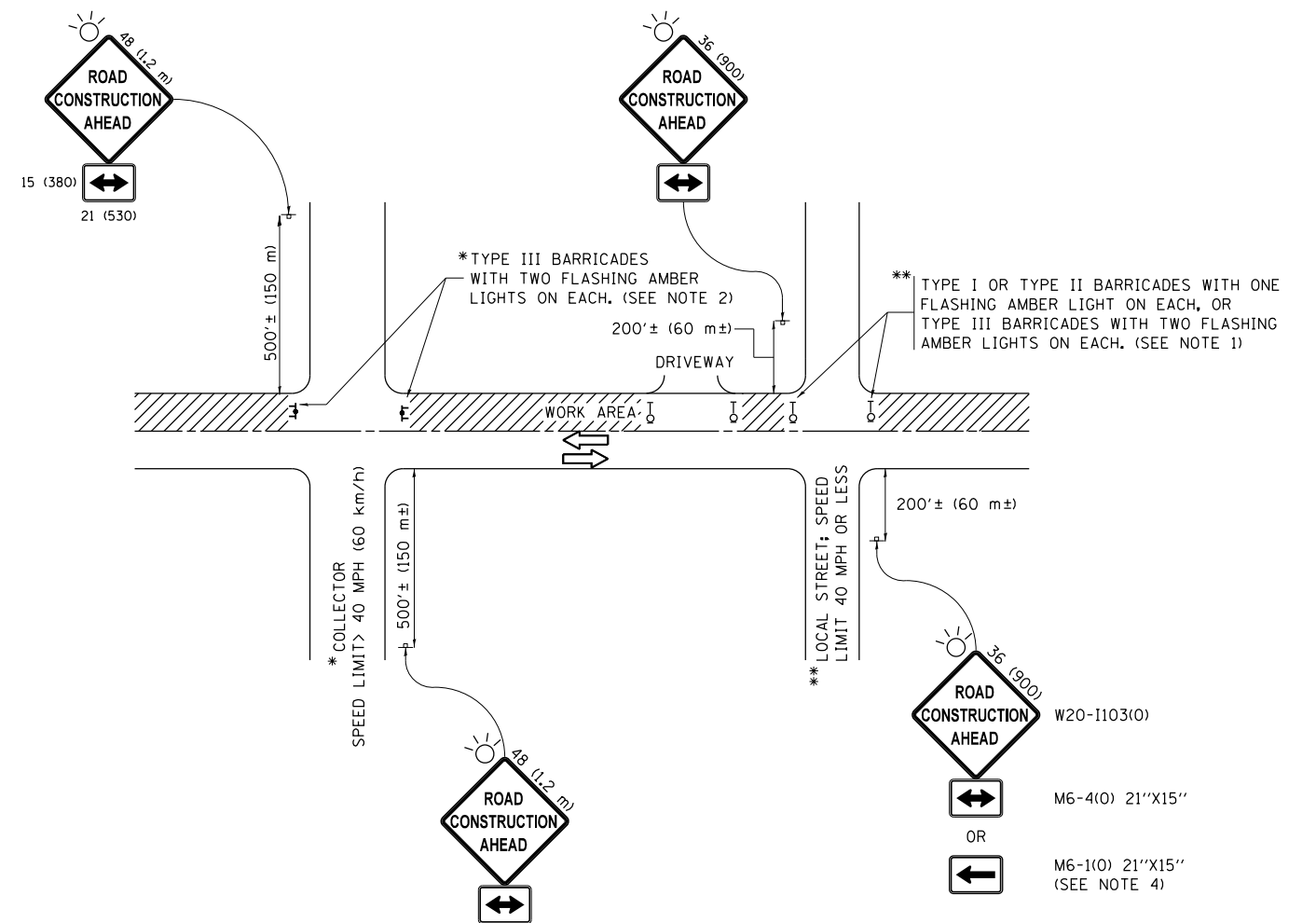
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pw\11084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI01\BRAW\Braw\Design\DistStd.dgn		CHECKED -	REVISED - S.P.B. 12-09
Default	PLOT SCALE = 100.0000' / in.	DATE - 02-83	REVISED - M.D. 06-13
	PLOT DATE = 2/1/2018		REVISED - M.D. 01-18

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ENTRANCE AND EXIT RAMP  
CLOSURE DETAILS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-7	DuPAGE/COOK	35	26
<b>TC-08</b>		<b>CONTRACT NO.</b>	<b>62F59</b>	
ILLINOIS FED. AID PROJECT				



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

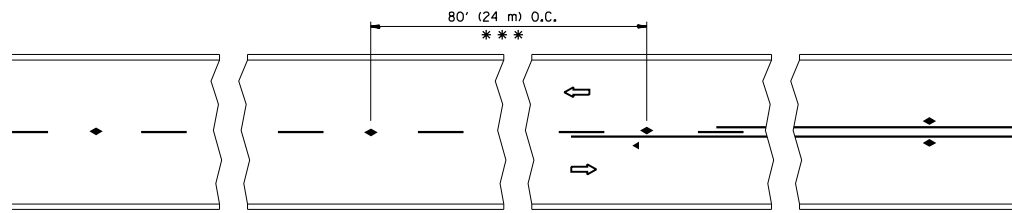
FILE NAME =	USER NAME = Velichkovv	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
pw\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI01\BRAW\Notes\Design\DistStd.dgn			REVISED - T. RAMMACHER 01-06-00
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 2/1/2018	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

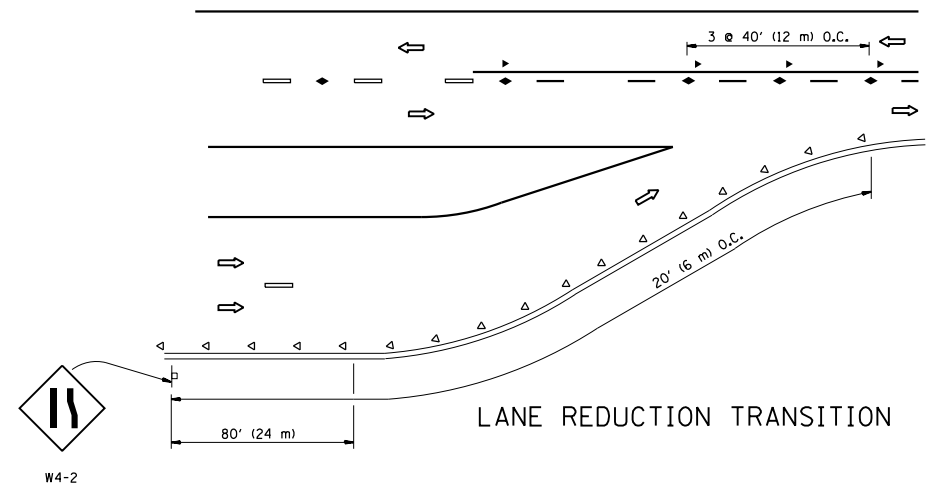
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

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<b>TC-10</b>			<b>CONTRACT NO. 62F59</b>	
ILLINOIS FED. AID PROJECT				

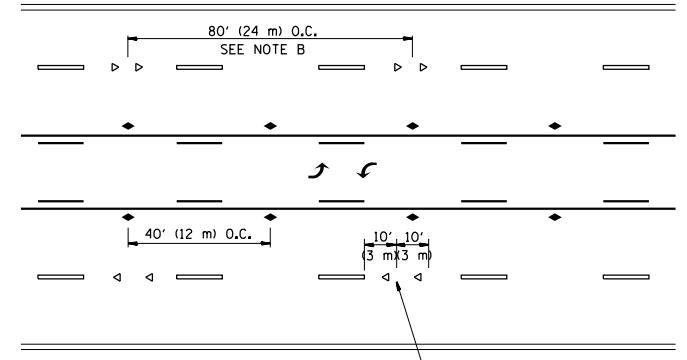


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

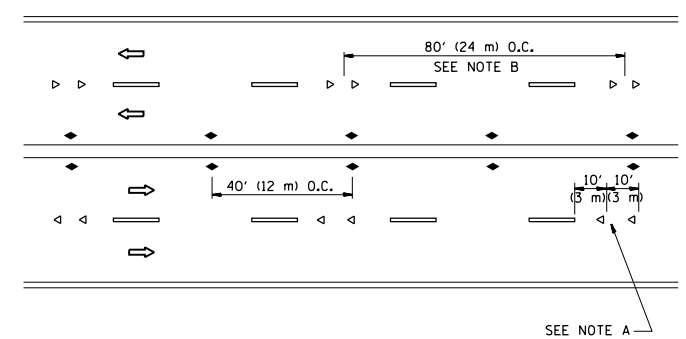
TWO-LANE/TWO-WAY



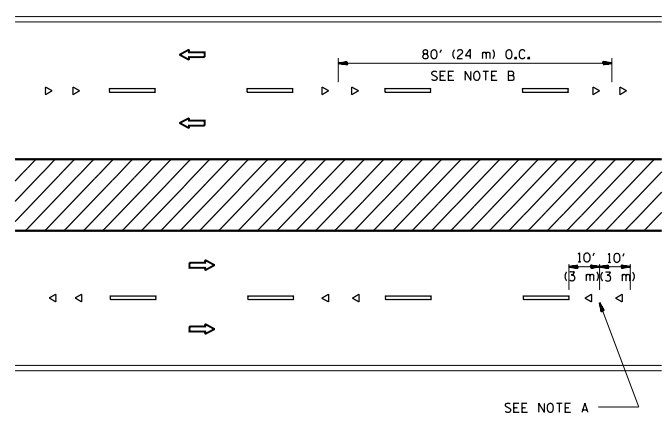
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

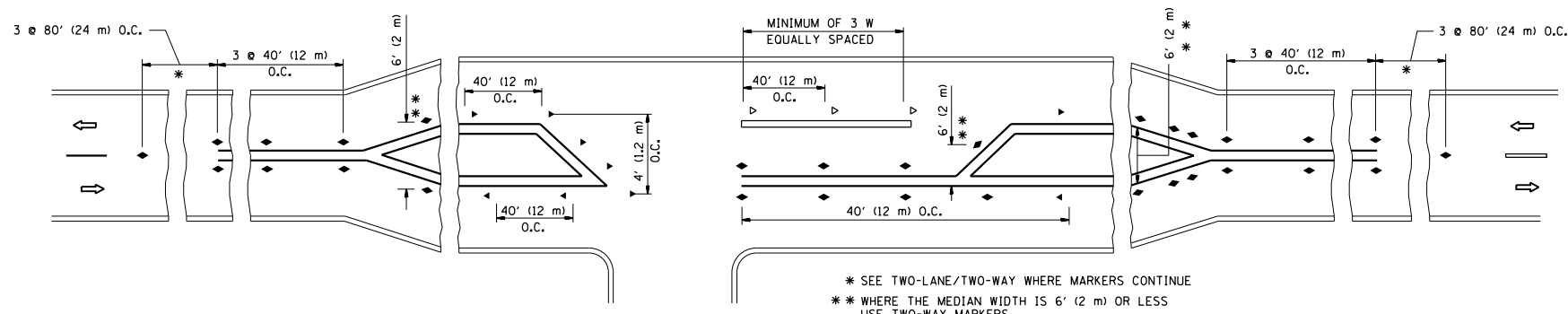
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

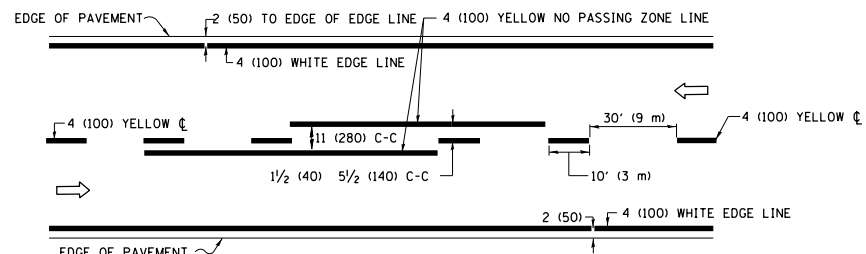


LEFT TURN

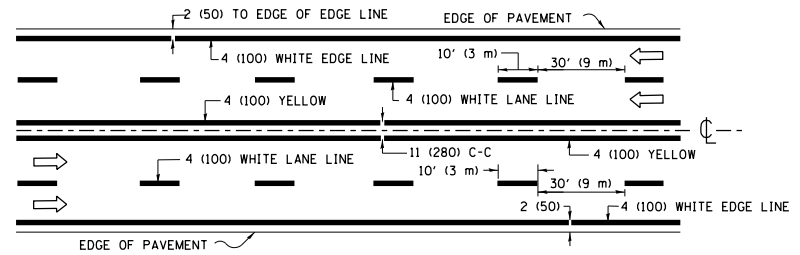
\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

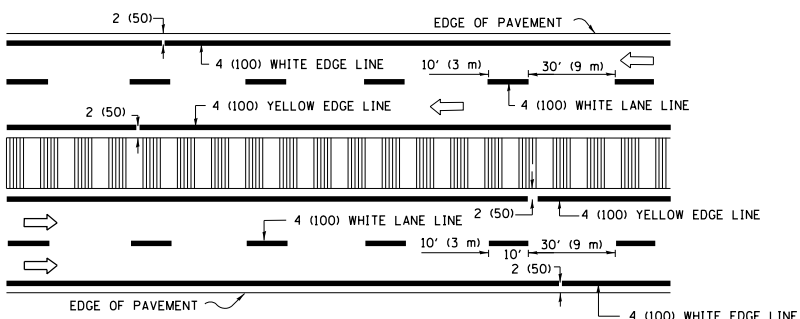
FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI01\BRAW\Design\DistStd.dgn	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 03-12-99					311	9Y-RS-7	DuPAGE/COOK	35	28
PLOT DATE = 2/1/2018	DATE -	REVISED - T. RAMMACHER 01-06-00	REVISED - C. JUCIUS 09-09-09		<b>TC-11</b>			<b>CONTRACT NO. 62F59</b>				
					SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



**2-LANE ROADWAY**

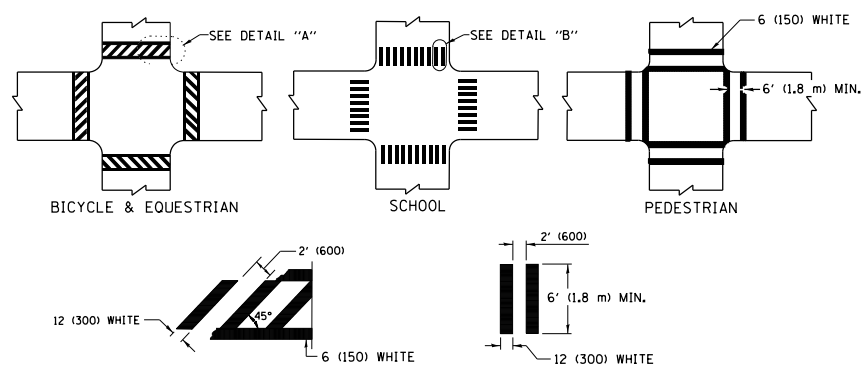


**MULTI-LANE UNDIVIDED**



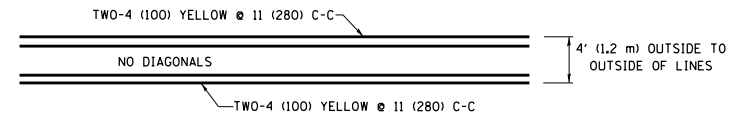
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

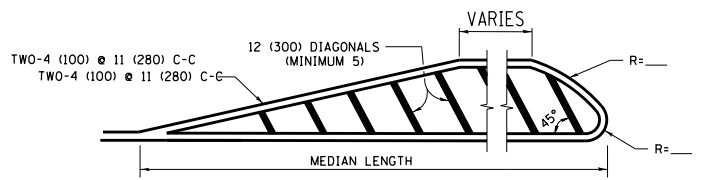


**TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

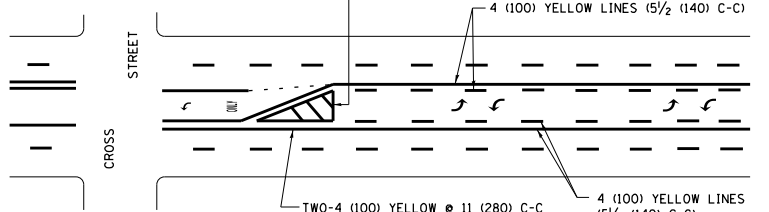


**4' (1.2 m) WIDE MEDIANS ONLY**



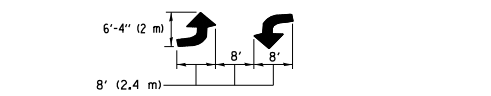
**MEDIANS OVER 4' (1.2 m) WIDE**

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



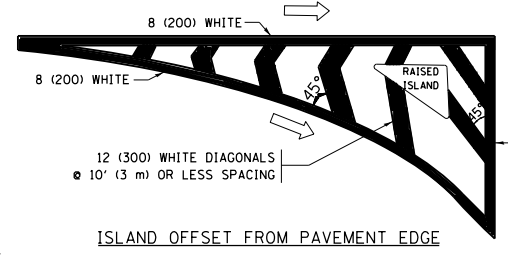
**MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING**

A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

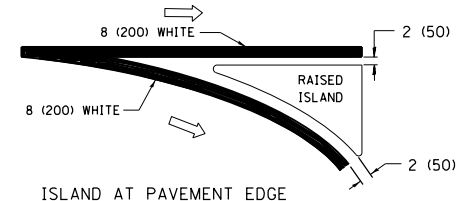


**TYPICAL LEFT (OR RIGHT) TURN LANE TYPICAL TURN LANE MARKING**

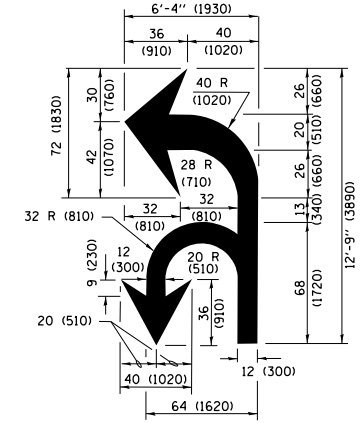
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



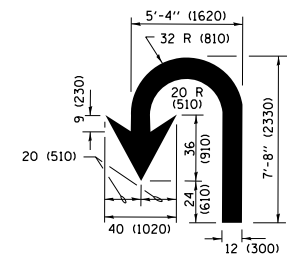
**ISLAND OFFSET FROM PAVEMENT EDGE**



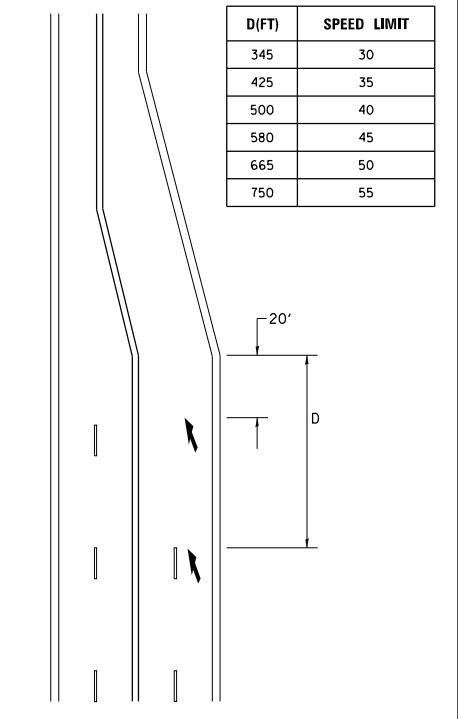
**ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**



**LANE REDUCTION TRANSITION**  
\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = Velichkovv	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
pw\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\0101\BROWN\Design\DistStd.dgn		CHECKED -	REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 100.0000' / in.	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 2/1/2018		REVISED - C. JUCIUS 04-12-16

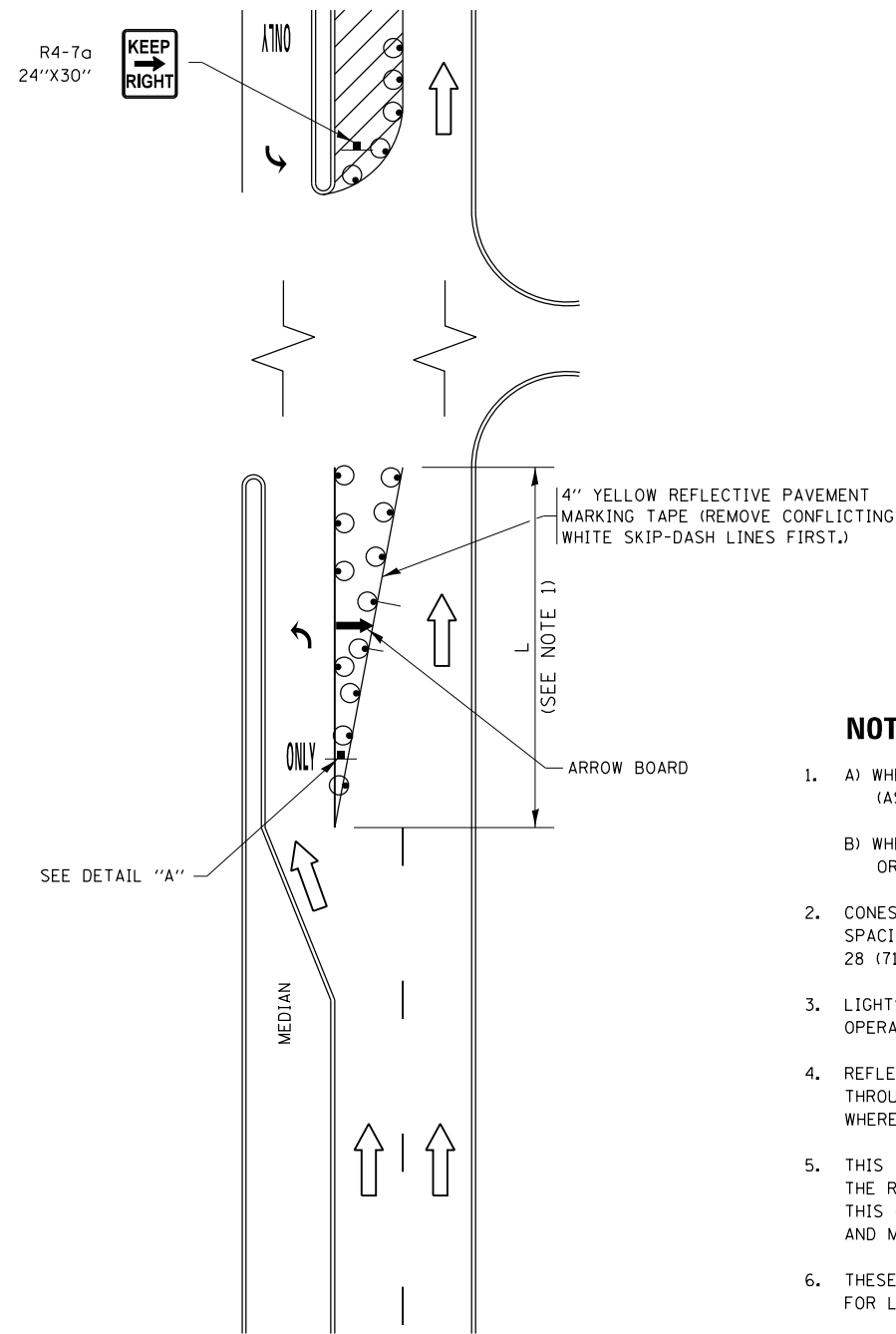
**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

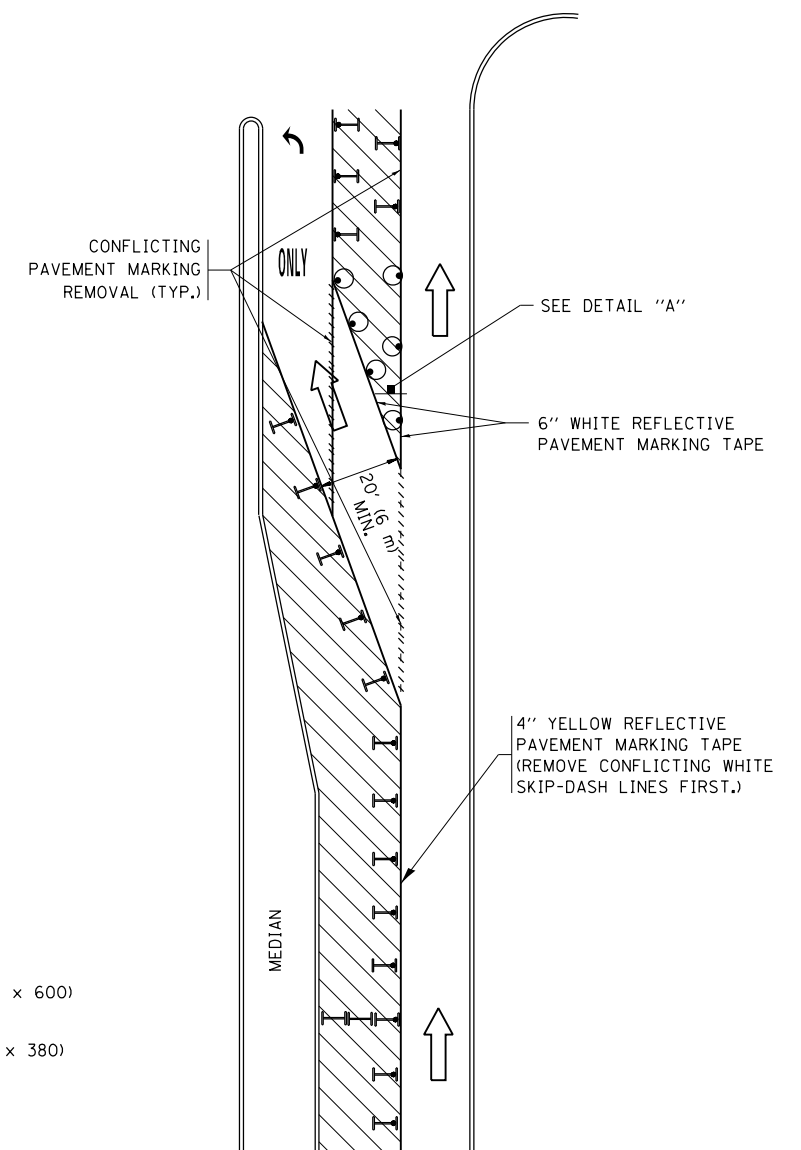
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-7	DUPAGE/COOK	35	29
<b>TC-13</b>			<b>CONTRACT NO.</b>	<b>62F59</b>
ILLINOIS FED. AID PROJECT				

# TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

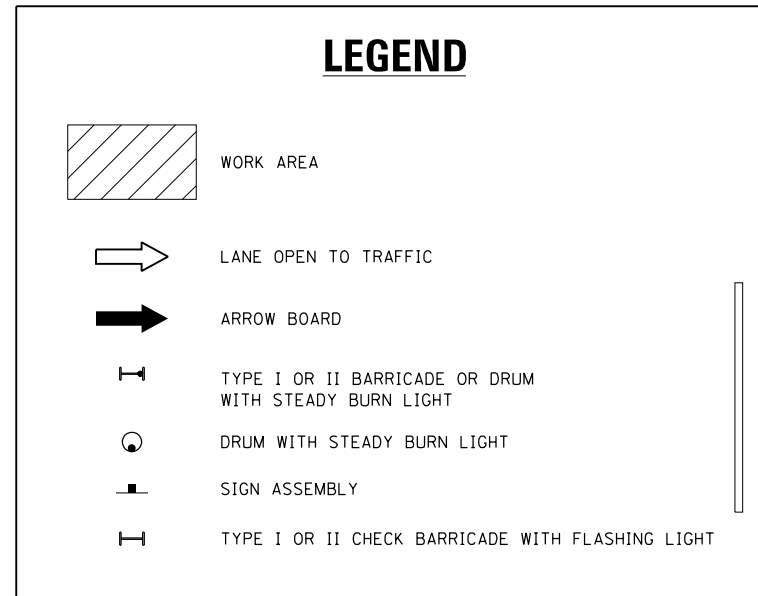


**FIGURE 1**

# TURN BAY ENTRANCE WITHIN A LANE CLOSURE

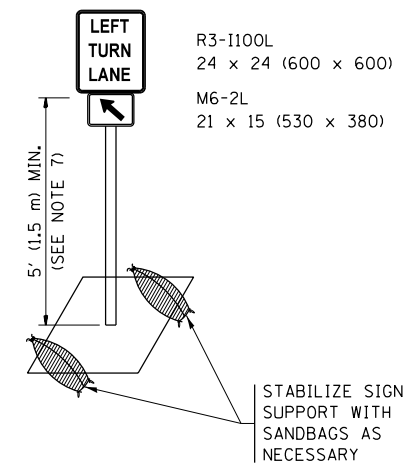


**FIGURE 2**



### NOTES:

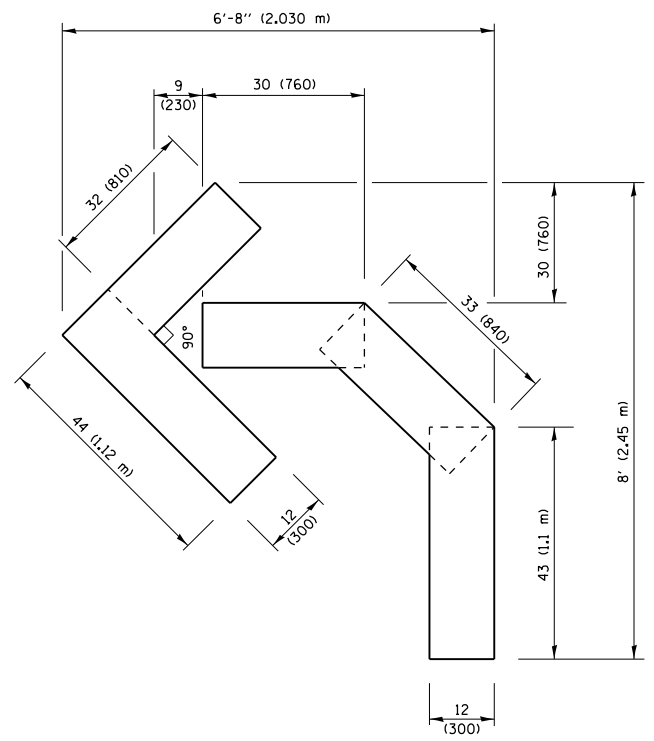
1. A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



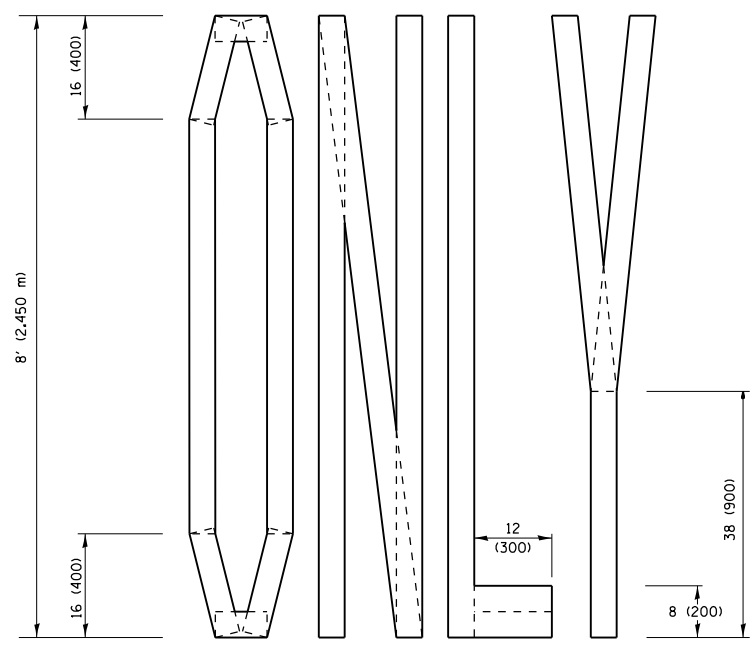
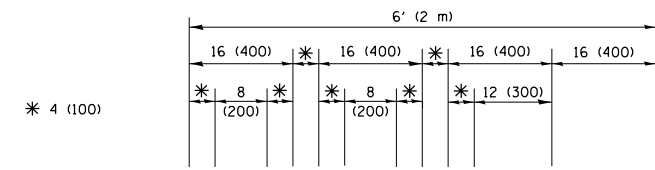
**DETAIL A**

All dimensions are in inches (millimeters) unless otherwise shown.

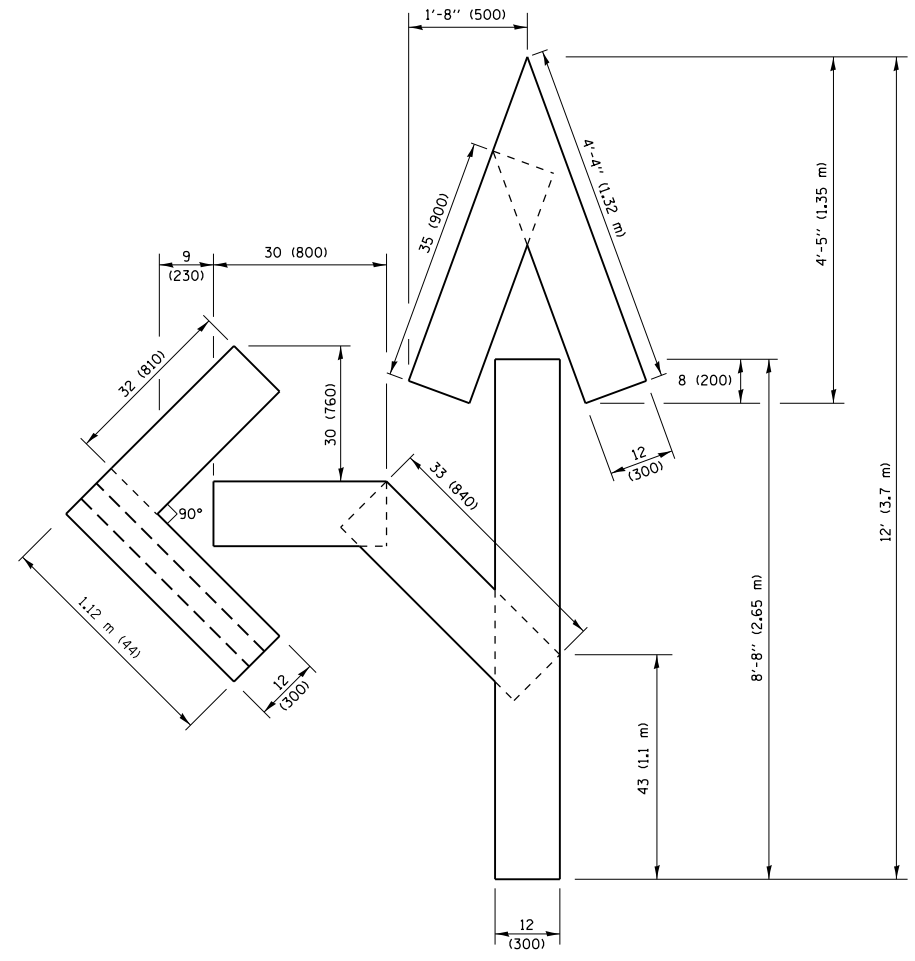
FILE NAME =	USER NAME = Velichkovv	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI01\REVISED Design\A11056111-07-95	REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13	REVISED - A. SCHUETZE 07-01-13			311	9Y-RS-7	DuPAGE/COOK	35	30
Default	PLOT SCALE = 100.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16			<b>TC-14</b>		<b>CONTRACT NO. 62F59</b>		ILLINOIS FED. AID PROJECT
	PLOT DATE = 2/1/2018	REVISED - T. RAMMACHER 01-06-00	REVISED -	SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.			



**QUANTITY**  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.41 sq. m)

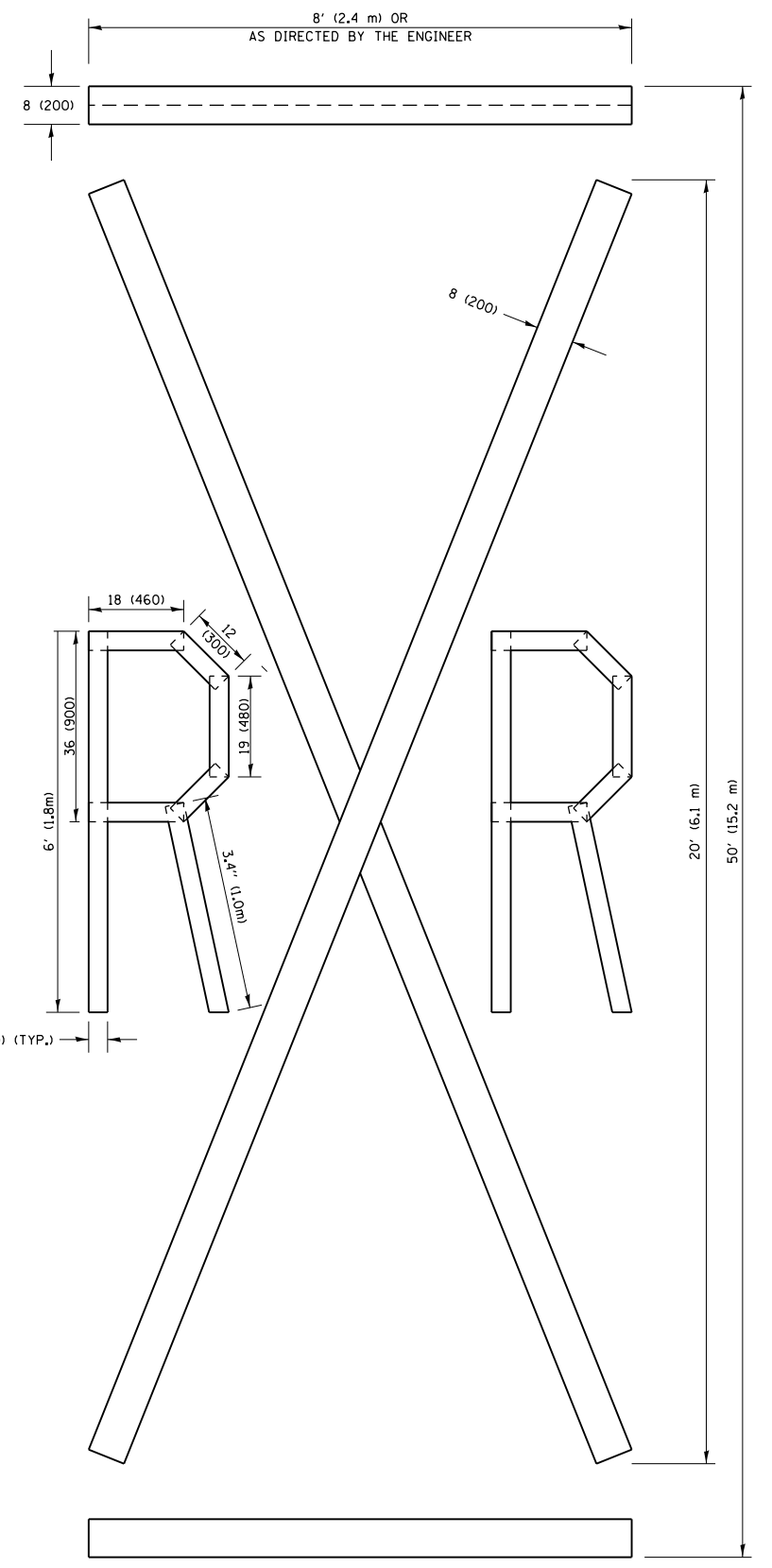


**QUANTITY**  
 4 (100) LINE = 64.1 ft. (19.5 m)  
 21.4 sq. ft. (1.99 sq. m)



**QUANTITY**  
 4 (100) LINE = 82.5 ft. (25.1 m)  
 27.5 sq. ft. (2.53 sq. m)

**NOTE:**  
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED  
 IN LINEAR FEET OF 4" LINES TO MATCH THE  
 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS  
 THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**  
 4 (100) LINE = 225.9 ft. (68.9 m)  
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters)  
 unless otherwise shown.

FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED -T. RAMMACHER 03-02-98
pw:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI01\BRAWN\Design\DistStd.dgn		CHECKED -	REVISED -E. GOMEZ 08-28-00
		DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00
			REVISED -A. SCHUETZE 09-15-16

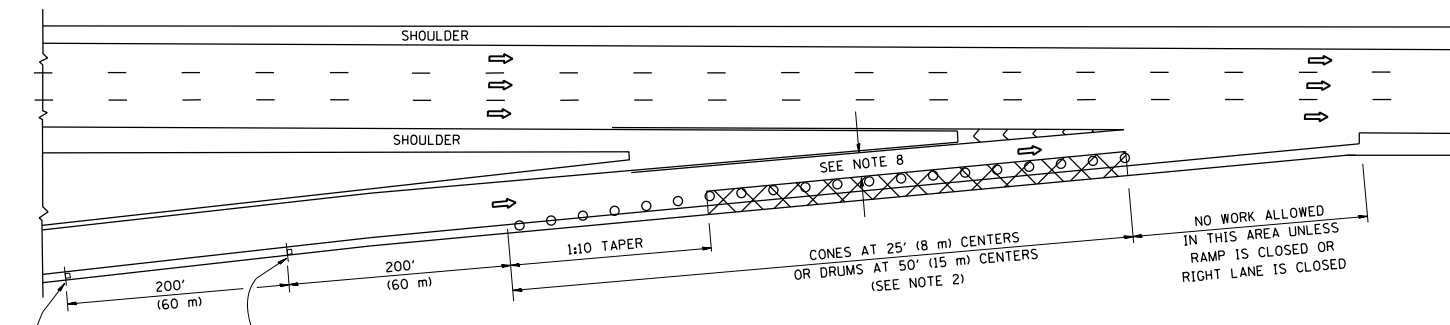
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

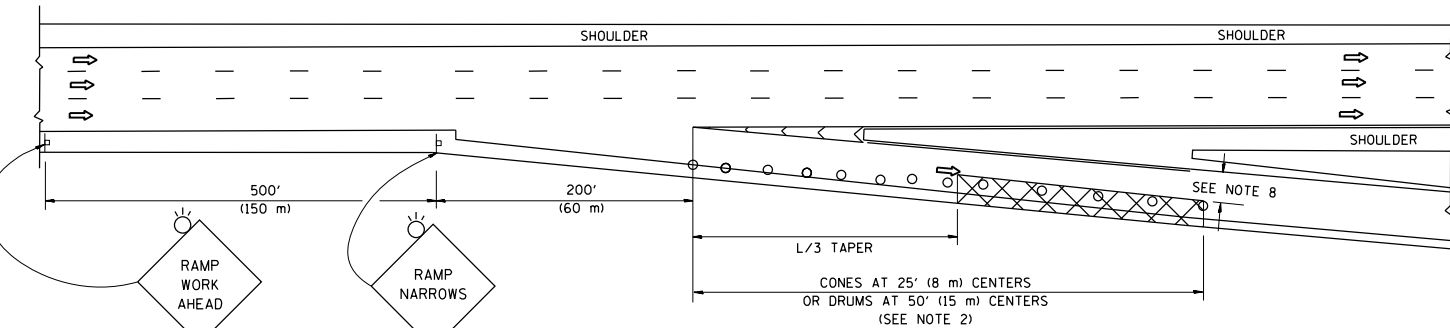
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-7	DuPAGE/COOK	35	31
<b>TC-16</b>		<b>CONTRACT NO.</b>	62F59	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



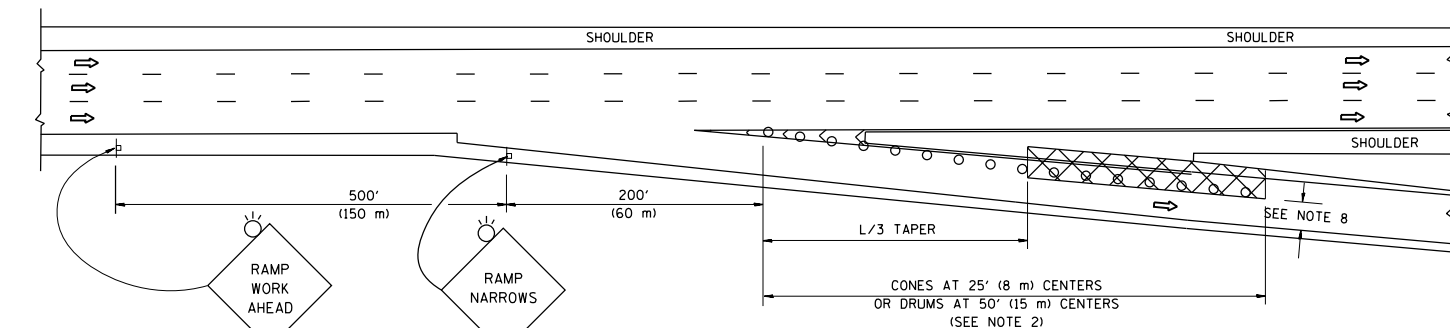
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

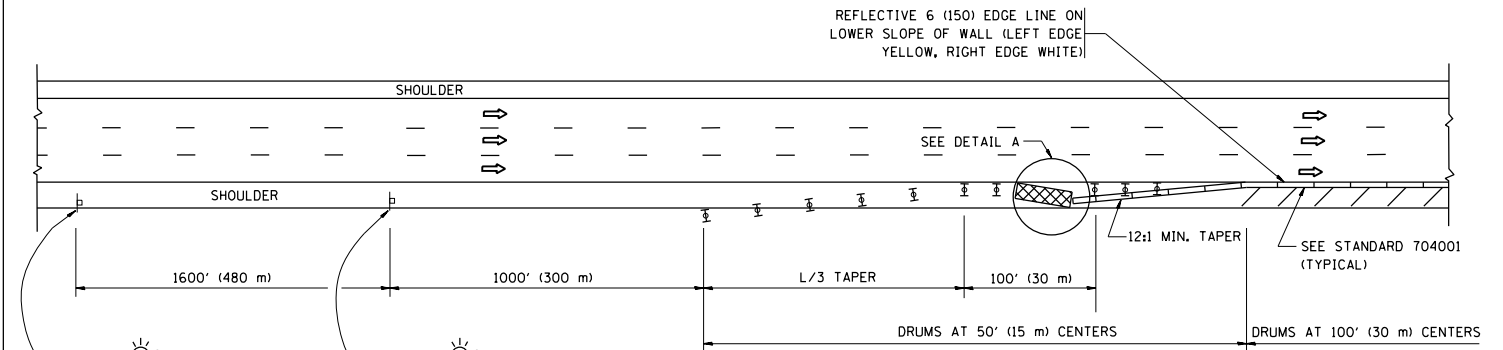
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

GENERAL NOTES

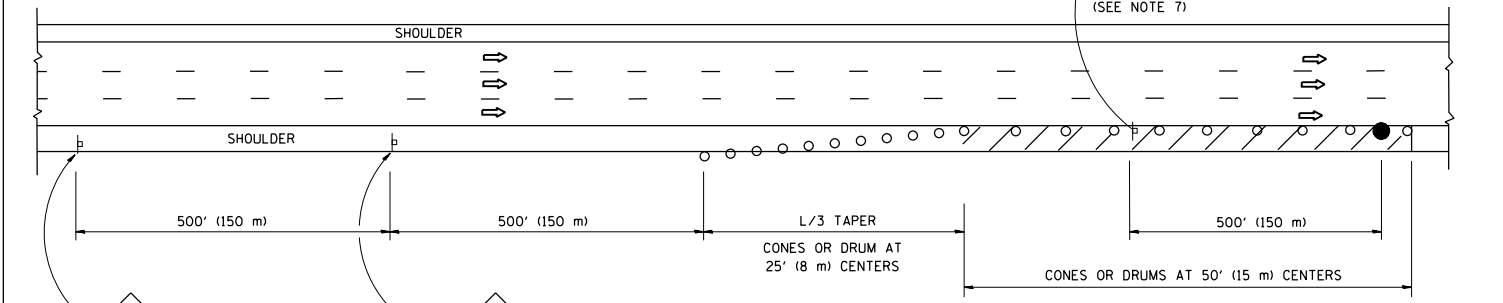
1. THE "L" DISTANCE EQUALS:
 

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC    ENGLISH L=0.65(W)(S)    L=(W)(S)
W = WIDTH OF OFFSET IN FEET (METERS) S = NORMAL POSTED SPEED MPH (KM/H)	
2. TYPE II BARRICADES OR DRUMS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES. TYPE II BARRICADES OR DRUMS WITH MONODIRECTIONAL STEADY BURN LIGHTS ARE REQUIRED FOR DELINEATING OBSTACLES, EXCAVATIONS, OR HAZARDS EXCEEDING 100 FT (30m) IN LENGTH AT NIGHT.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.
5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
  - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
  - b. THE WORK ACTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12' MIN. WIDTH TANGENT SECTION  
16' MIN. WIDTH CURVE SECTION.

SHOULDER CLOSURE DETAILS

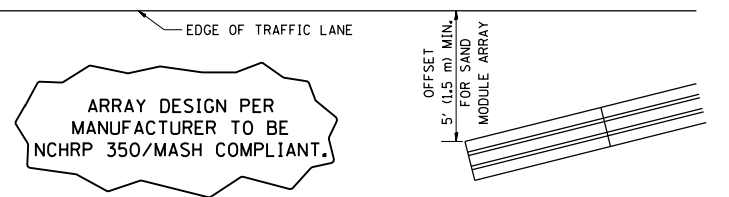


PERMANENT SHOULDER CLOSURE



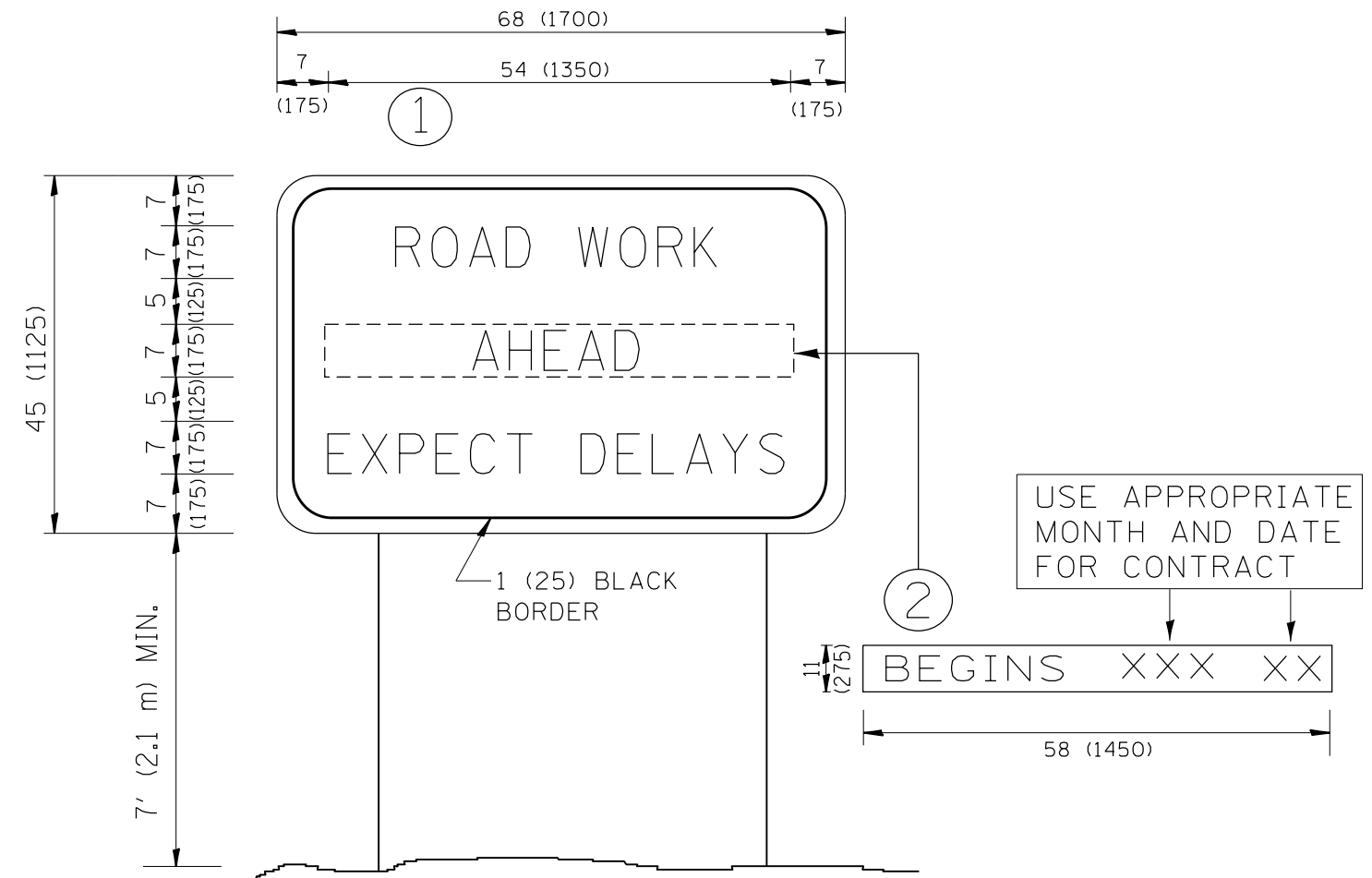
DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:  
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCROACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED - S.P.B. 01-07	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
			REVISED - S.P.B. 12-09			311	9Y-RS-7	DuPAGE/COOK	35	32	
			REVISED - M.D. 06-13			<b>TC-17</b>		<b>CONTRACT NO.</b>		62F59	
			REVISED - M.D. 01-18			SCALE: NONE    SHEET 1 OF 1 SHEETS    STA. TO STA.		ILLINOIS FED. AID PROJECT			



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

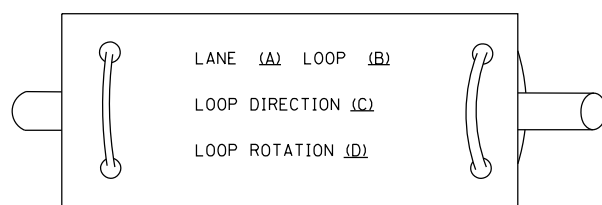
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED - R. MIRS 09-15-97	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARTERIAL ROAD INFORMATION SIGN</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PIDOT\Documents\DOT Offices\District 1\Projects\DI01\BROW\Notes\Design\DistStd.dgn			REVISED - R. MIRS 12-11-97			311	9Y-RS-7	DuPAGE/COOK	35	33
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99				<b>TC-22</b>		<b>CONTRACT NO. 62F59</b>		
PLOT DATE = 2/1/2018	DATE -	REVISED - C. JUCIUS 01-31-07	SCALE: NONE			SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT	

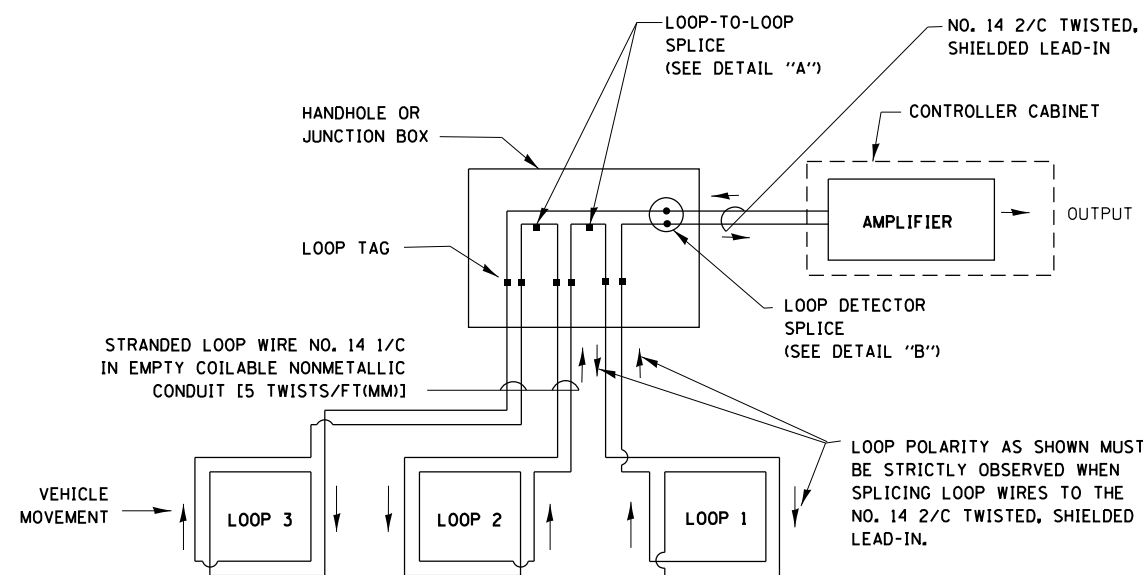
**LOOP DETECTOR NOTES**

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

**LOOP LEAD-IN CABLE TAG**

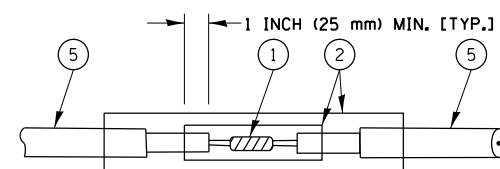


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

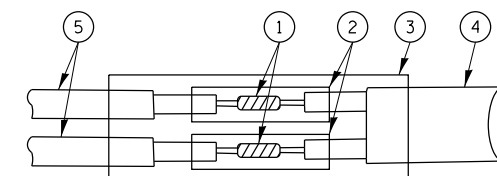


**DETECTOR LOOP WIRING SCHEMATIC**

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

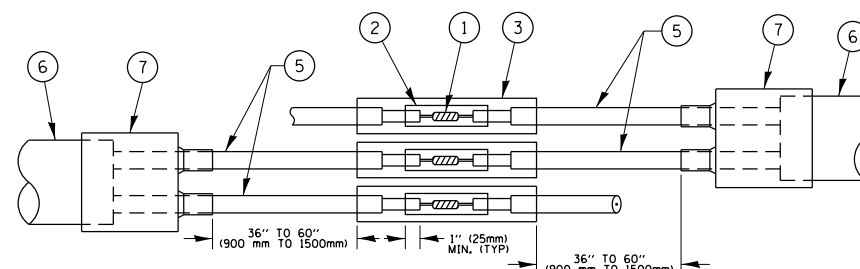


DETAIL "A"  
LOOP-TO-LOOP SPLICE

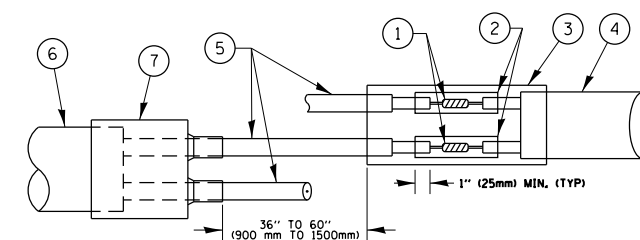


DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

**TYPE I LOOP**



DETAIL "A"  
LOOP-TO-LOOP SPLICE



DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

**LOOP DETECTOR SPLICE**

- ① WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH, THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- ② WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- ③ WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- ④ NO. 14 2/C TWISTED, SHIELDED CABLE.
- ⑤ LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- ⑥ PRE-FORMED LOOP
- ⑦ XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED -
p:\11\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI01\BRAW\Design\DistStd.dgn		CHECKED -	REVISED -
Default	PLOT DATE = 2/1/2018	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

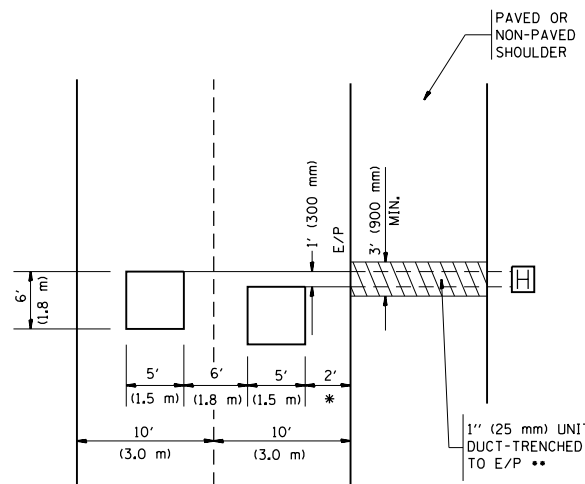
**DISTRICT ONE  
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET 2 OF 7 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-7	DuPAGE/COOK	35	34
<b>TS-05</b>		<b>CONTRACT NO.</b>	62F59	
ILLINOIS FED. AID PROJECT				

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



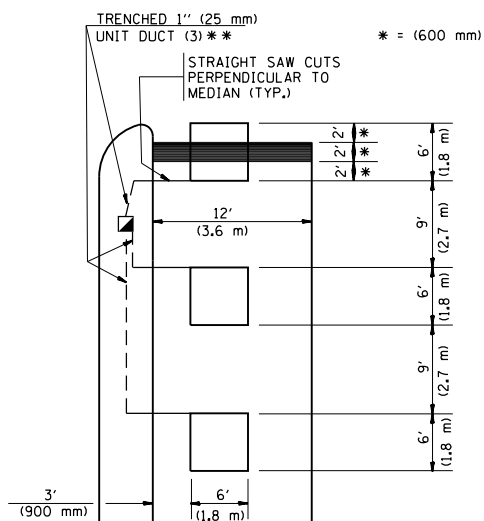
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

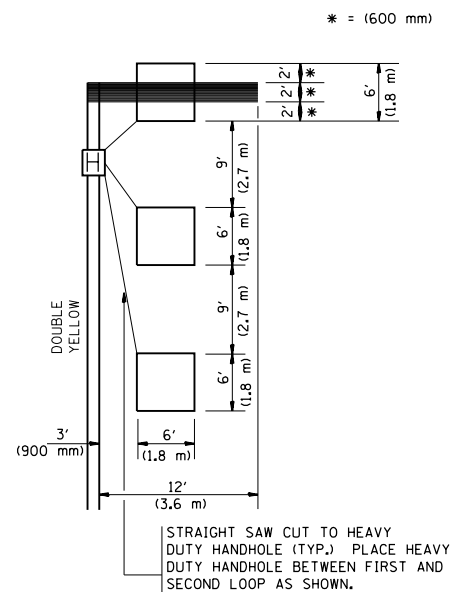


\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

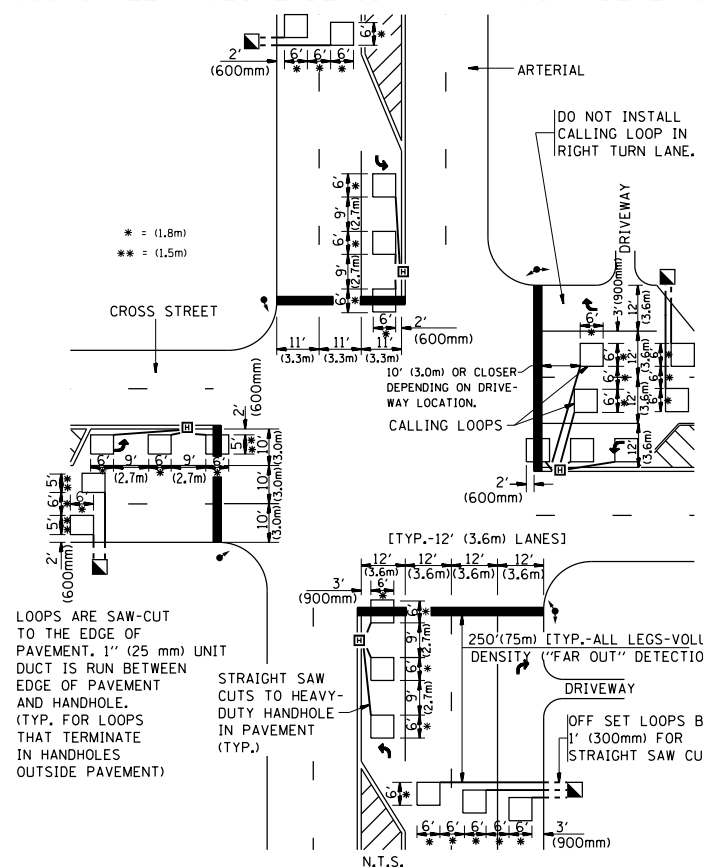
**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)



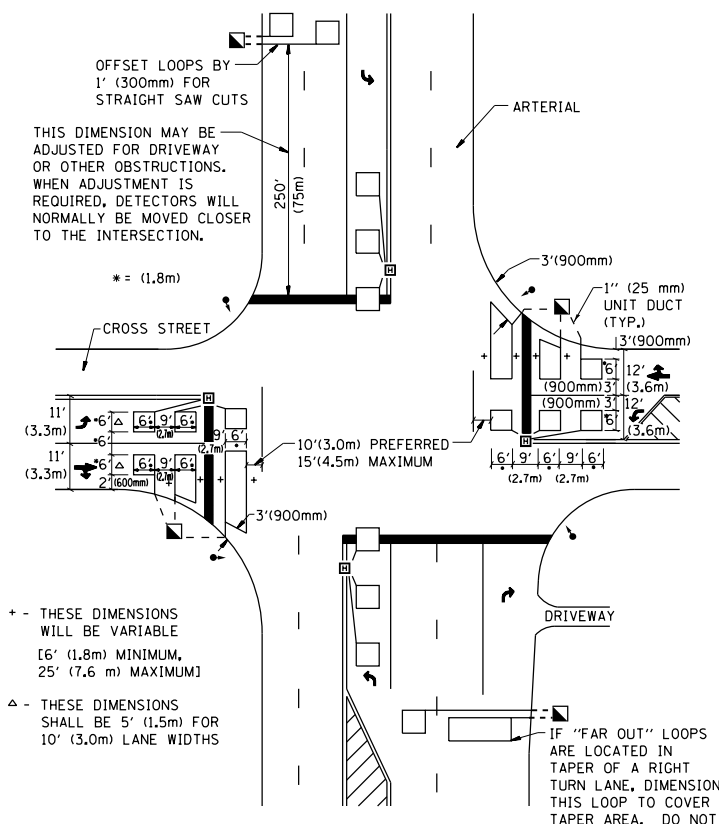
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1  
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2  
N.T.S.**

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT DATE = 2/1/2018	DATE -	REVISED -	REVISED -			<b>TS-07</b>		<b>CONTRACT NO. 62F59</b>		
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.			