

INDEX OF SHEETS

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| 1 | COVER SHEET |
| 2 | HIGHWAY STANDARDS, GENERAL NOTES & UTILITY COORDINATION |
| 3-5 | SUMMARY OF QUANTITIES |
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| 12-21 | PLAN FAP RTE 327 (US RTE 50) |
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PROPOSED
HIGHWAY PLANS

FAP ROUTE 327 (US RTE 50)
SECTION 30RS-5
PROJECT NHPP-3N34(733)
3P RESURFACING FROM IL RTE 4
TO 0.3 MI W. OF OLD US RTE 50
ST. CLAIR COUNTY

C-98-038-14

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|---------|-----------|--------------------|-----------|
| 327 | 30RS-5 | ST. CLAIR | 24 | 1 |
| | | ILLINOIS | CONTRACT NO. 76H13 | |

D-98-037-14



DESIGN DESIGNATION

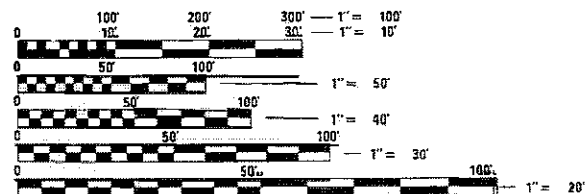
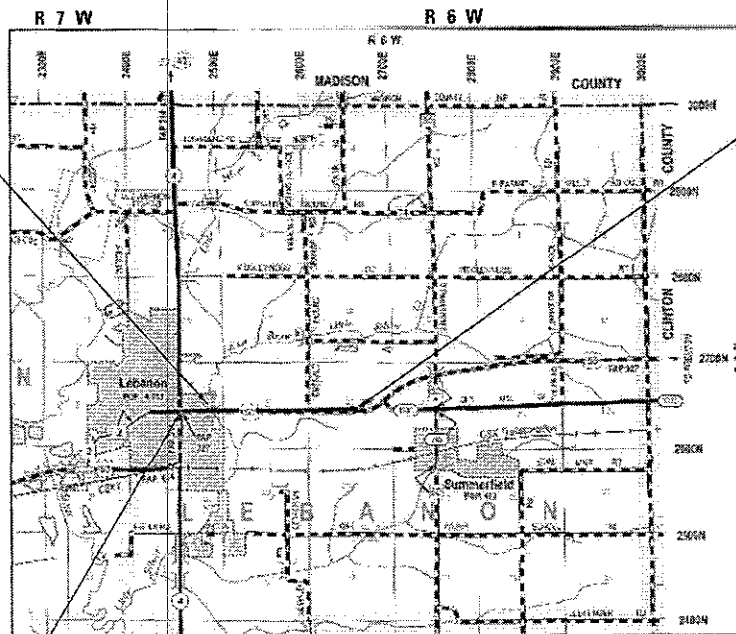
FUNCTIONAL CLASS: FAP RTE 327 (US RTE 50)
 PRINCIPAL ARTERIAL

ADT: 8550 (2013); 8900 (2014)
 SU = 401 MU = 632

BRIDGE OMISSION SN 082-0037
 STA. 19+77.11 TO STA. 21+25.39

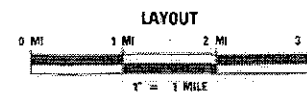
END SECTION 30RS-5
 STA. 110+90.00

BEGIN SECTION 30RS-5
 STA. 1+13.00

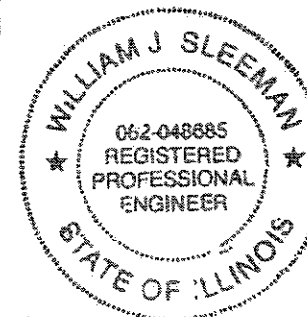


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123
 OR 811



GROSS LENGTH = 10,977.00 FEET = 2.079 MILES
 TOTAL NET LENGTH = 10,828.72 FEET = 2.051 MILES



STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

SUBMITTED *Feb 2 2018*

Jeffrey Z. Kammer
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

Mark 23 2018
 ENGINEER OF DESIGN AND ENVIRONMENT

David P. [Signature]
 DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PROJECT ENGINEER: TIM PADGETT 618-346-3325
 PROJECT MANAGER: PHIL COPPERNOLL 618-346-3480

CONTRACT NO. 76H13



PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

William J. Sleeman 11/30/18
 EXPIRES 11-30-2019

HIGHWAY STANDARDS

GENERAL NOTES

- THIS SECTION SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE PLANS, THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED APRIL 1, 2016, THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", AND THE SPECIAL PROVISIONS INCLUDED IN THE PROPOSAL. THE IDOT HIGHWAY STANDARDS LATEST REVISION NUMBERS SHALL APPLY TO THIS PROJECT.
- WHERE SECTION OR SUB-SECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKERS AND MONUMENTS UNTIL THE OWNER, AN AUTHORIZED SURVEYOR, OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THE LOCATION.
- THE CONTRACTOR AND ENGINEER SHALL BE AWARE THAT NO COMPLETE SURVEY WAS PERFORMED FOR THE PROJECT. THE TOPO SHOWN IN THE PLANS AT FRITZ STREET WAS ELECTRONICALLY COLLECTED. STATIONING AND ALL OTHER TOPO SHOWN IN THE PLANS WAS CREATED USING PREVIOUS PLANS, GOOGLE EARTH AND FIELD MEASUREMENTS MADE BY DESIGN PERSONNEL. BOTH SHALL BE ASSUMED TO BE APPROXIMATE.
- SAW CUTTING ON ALL EDGES FOR REMOVAL ITEMS SHALL BE INCLUDED IN THE COST OF THE REMOVAL ITEM AS INDICATED AND IN ACCORDANCE WITH SECTION 440 OF THE STANDARD SPECIFICATIONS.
- EXCESS BITUMEN REMOVAL SHALL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- MIXTURES FOR JOINTS, CRACKS AND FLANGEWAYS SHALL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- THE USE OF VIBRATORY ROLLERS IS PROHIBITED IN RESIDENTIAL AREAS. THIS DOES NOT RELIEVE THE CONTRACTOR OF DENSITY REQUIREMENTS FOR THE HOT-MIX ASPHALT PAVEMENTS AS SPECIFIED IN THE APPROPRIATE SECTIONS OF THE STANDARD SPECIFICATIONS.
- THE THICKNESS OF HOT-MIX ASPHALT SURFACE REMOVAL IS THE REMOVAL DEPTH AT THE CENTERLINE. AN ESTIMATED 3170 TONS OF HOT-MIX ASPHALT SURFACE WILL BE REMOVED.
- ILLINOIS STATE LAW REQUIRES A 48-HOUR NOTICE BE GIVEN TO ALL UTILITIES WITHIN THE PROJECT BEFORE DIGGING. FIELD MARKING OF FACILITIES MAY BE OBTAINED BY CONTACTING J.U.L.I.E. OR FOR NON-MEMBERS, THE UTILITY COMPANY DIRECTLY. AGENCIES KNOWN TO HAVE FACILITIES WITHIN THE PROJECT AREA ARE AS FOLLOWS:

- AMEREN ILLINOIS - GAS & ELECTRIC (AERIAL & BURIED)
- AT&T ILLINOIS - TELEPHONE (AERIAL & BURIED)
- CHARTER COMMUNICATIONS, INC. - CABLE TELEVISION (AERIAL & BURIED)
- FRONTIER NORTH, INC. - TELEPHONE (AERIAL & BURIED)
- CITY OF LEBANON - WATER & SEWER (BURIED)
- SLM WATER COMMISSION - WATER (BURIED)

MEMBERS OF J.U.L.I.E. CALL TOLL FREE (800)-892-0123 OR 811 ARE INDICATED BY AN *. NON-J.U.L.I.E. MEMBERS MUST BE NOTIFIED INDIVIDUALLY.

- THE THICKNESS OF THE HOT-MIX ASPHALT MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED.
- THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- THE ILLINOIS DEPARTMENT OF TRANSPORTATION STRONGLY ENCOURAGES THE PRIME CONTRACTOR AND THEIR APPROVED SUB-CONTRACTORS TO HIRE MINORITY, WOMEN AND DISADVANTAGED INDIVIDUALS FROM ITS FEDERALLY FUNDED HIGHWAY CONSTRUCTION CAREERS TRAINING PROGRAM (HCCTP) TO HELP MEET WORKFORCE AND TRAINEE GOALS. THIS PROGRAM IS TRAINING MINORITIES, WOMEN AND DISADVANTAGED INDIVIDUALS IN HIGHWAY CONSTRUCTION-RELATED SKILLS, E.G., MATH FOR TRADES, JOB READINESS, TECHNICAL SKILLS COURSEWORK (CARPENTRY, CONCRETE FLATWORK, BLUEPRINT READING, SITE PLANS, SITEWORK, TOOLS USE, ETC.) AND OSHA 10 HOUR CERTIFICATION, TO PREPARE THEM FOR A CAREER IN THE HIGHWAY CONSTRUCTION TRADES. GRADUATES ARE WELL-TRAINED AND READY TO BECOME PRODUCTIVE ENTRY-LEVEL CONSTRUCTION WORKERS. PLEASE CONTACT THE DISTRICT 8 EEO OFFICE AT 618-346-3360 AND/OR THE HCCTP COORDINATOR AT 618-874-6528 TO LEARN MORE ABOUT THE PROGRAM AND FOR ASSISTANCE IN MEETING WORKFORCE AND TRAINEE GOALS.
- ALL PATCHING ON THIS PROJECT SHALL BE COMPLETED PRIOR TO HOT-MIX ASPHALT SURFACE REMOVAL. THE RESIDENT ENGINEER SHALL DETERMINE FINAL PATCHING LOCATIONS IN THE FIELD.
- A QUANTITY FOR TEMPORARY PAVEMENT MARKING EQUAL TO THE AMOUNT OF PERMANENT PAVEMENT MARKING HAS BEEN ADDED TO THE PLANS. ALL TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED IN SUCH A MANNER SO AS NOT TO INTERFERE WITH THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THE PAVEMENT MARKING SHOWN ON THE PLANS IS APPROXIMATE. THE LOCATION OF THE PROPOSED PAVEMENT MARKINGS SHALL MATCH EXISTING FIELD CONDITIONS.
- IF THE SURFACE REMOVAL ON THIS PROJECT PRODUCES A MILLED EDGE NEAR CENTERLINE GREATER THAN 1.5 INCHES BETWEEN ADJACENT OPEN LANES OF TRAFFIC, ONE OF THE FOLLOWING SHALL APPLY:
 - THE CONTRACTOR SHALL ORGANIZE THE WORK TO AVOID THE MILLED EDGE
 - THE CONTRACTOR SHALL CONSTRUCT A TEMPORARY HOT-MIX ASPHALT WEDGE ALONG THE MILLED EDGE
 - THE CONTRACTOR SHALL CONSTRUCT A MILLED SLOPED EDGE (MINIMUM 1:3) ALONG THE MILLED EDGE

THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST PER SQUARE YARD FOR HOT-MIX ASPHALT SURFACE REMOVAL OF THE DEPTH SPECIFIED.

- 00001-06 - STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 406201-01 - MAILBOX TURNOUT
- 424001-10 - PERPENDICULAR CURB RAMPS FOR SIDEWALKS
- 424006-03 - DIAGONAL CURB RAMPS FOR SIDEWALKS
- 442201-03 - CLASS C AND D PATCHES
- 606001-07 - CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
- 642006 - SHOULDER RUMBLE STRIPS, 8 INCH
- 701006-05 - OFF-RD OPERATIONS, 2L, 2W 15' (4.5m) TO 24" (600mm) FROM PAVEMENT EDGE
- 701201-04 - LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS > 45 MPH
- 701301-04 - LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
- 701306-04 - LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS > 45 MPH
- 701311-03 - LANE CLOSURE, 2L, 2W, MOVING OPERATIONS-DAY ONLY
- 701336-06 - LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES, FOR SPEEDS > 45 MPH
- 701501-06 - URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
- 701801-06 - SIDEWALK, CORNER OR CROSSWALK CLOSURE
- 701901-07 - TRAFFIC CONTROL DEVICES
- 780001-05 - TYPICAL PAVEMENT MARKINGS
- 781001-04 - TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS

TRAFFIC CONTROL

- NO OVERNIGHT LANE CLOSURES WILL BE ALLOWED.
- ROAD CONSTRUCTION AHEAD SIGNS SHALL BE PLACED AT THE BEGINNING AND END OF THE PROJECT PLUS THE INTERSECTING SIDE ROADS AND SHALL BE INCLUDED IN THE TRAFFIC CONTROL PAY ITEMS. ALL CONSTRUCTION SIGNS SHALL BE (48x48) FLUORESCENT ORANGE.
- ALL EXCAVATION ADJACENT TO THE EDGE OF PAVEMENT SHALL BE PROTECTED WITH EXTENDED LEG BARRICADES AND APPROPRIATE LIGHTS.
- FLAGMEN SHALL BE PRESENT DURING ALL CLOSURE HOURS, INCLUDING LUNCH HOUR AND NO ADDITION COMPENSATION SHALL BE APPLIED.
- SHORT TERM PAVEMENT MARKING SHALL BE APPLIED TO THE MILLED, BINDER AND FINAL SURFACE. REMOVAL WILL BE PAID FOR AS SHORT TERM PAVEMENT MARKING REMOVAL.

COMMITMENTS

IF AT ANY POINT IN PROJECT IMPLEMENTATION OR OPERATION A COMMITMENT WILL BE AFFECTED BY SUBSEQUENT PROJECT DECISIONS OR COMMITMENTS, THE AFFECTED PARTIES, WHICH INCLUDES ALL DEPARTMENT PERSONNEL, WILL BE NOTIFIED AND THEIR COMMENTS WILL BE CONSIDERED PRIOR TO MAKING A FINAL DECISION ON THE ACTION AFFECTING THE EARLIER COMMITMENT. THE PROCEDURES AND THE RESULTS OF THE COORDINATION WITH AFFECTED PARTIES AND THE ULTIMATE DECISION ON THE PROPOSED CHANGE(S) TO THE COMMITMENT SHALL BE DOCUMENTED IN THE COMMITMENT FILE.

COMMITMENTS FOR THIS PROJECT ARE AS FOLLOWS:

NONE

MIXTURE REQUIREMENTS

| | |
|----------|-----------|
| ROUTE | FAP 327 |
| SECTION | 3ORS-5 |
| COUNTY | ST. CLAIR |
| CONTRACT | 76H13 |

DESCRIPTION: 3P RESURFACING - US50 FROM IL 4 N JCT IN LEBANON TO 0.3 MI W OF OLD US 50

| | |
|-----------------------|------|
| ADT (CONSTRUCTION YR) | 8900 |
| MU% | 7.1 |
| SU% | 4.5 |
| 20 YR ESAL'S | 3.89 |

| MIXTURE USE: | SURFACE | LEVEL BINDER | PATCHING | INCIDENTAL |
|-----------------------------|---------------------|---------------------|---------------------|---------------------|
| AC/PG | PG 64-22 | PG 64-22 | PG 64-22 | PG 64-22 |
| RAP% (MAX) | SEE SPEC. PROVISION | SEE SPEC. PROVISION | SEE SPEC. PROVISION | SEE SPEC. PROVISION |
| DESIGN AIR VOIDS | 4.0% @ Ndes = 70 | 4.0% @ Ndes = 70 | 4.0% @ Ndes = 70 | 4.0% @ Ndes = 70 |
| MIX COMPOSITION (GRADATION) | IL 9.5 | IL 9.5 FG | IL 19.0 | IL 9.5 |
| FRICTION AGG | MIXTURE "D" | MIXTURE "C" | MIXTURE "B" | MIXTURE "C" |
| QUALITY MGMT PROGRAM | OCP | OCP | OC/OA | OC/OA |

| MIXTURE USE: | SHOULDERS ≥ 2.25" | SHOULDERS < 2.25" | | |
|-----------------------------|---------------------|---------------------|--|--|
| AC/PG | PG 64-22 | PG 64-22 | | |
| RAP% (MAX) | SEE SPEC. PROVISION | SEE SPEC. PROVISION | | |
| DESIGN AIR VOIDS | 4.0% @ Ndes = 30 | 4.0% @ Ndes = 30 | | |
| MIX COMPOSITION (GRADATION) | IL 19.0 L | IL 9.5 L | | |
| FRICTION AGG | | | | |
| QUALITY MGMT PROGRAM | OC/OA | OC/OA | | |

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | CONSTRUCTION CODE | | | |
|--------------|---|-------|----------------|-------------------|------------|--|--|
| | | | | URBAN 0005 | RURAL 0005 | | |
| 40600290 | BITUMINOUS MATERIALS (TACK COAT) | POUND | 22,844 | 4019 | 18,825 | | |
| (S) 40600637 | LEVELING BINDER (MACHINE METHOD), IL-9.5FG, N70 | TON | 1284 | 280 | 1004 | | |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SQ YD | 171 | 90 | 81 | | |
| 40600985 | PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT | SQ YD | 44 | 44 | | | |
| 40600990 | TEMPORARY RAMP | SQ YD | 236 | 196 | 40 | | |
| 40603340 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 | TON | 2486 | 478 | 2008 | | |
| 40800029 | BITUMINOUS MATERIALS (TACK COAT) | POUND | 118 | 30 | 88 | | |
| 40800050 | INCIDENTAL HOT-MIX ASPHALT SURFACING | TON | 28 | 8 | 20 | | |
| 42400100 | PORTLAND CEMENT CONCRETE SIDEWALK 4 INCH | SQ FT | 198 | 198 | | | |
| 42400800 | DETECTABLE WARNINGS | SQ FT | 24 | 24 | | | |
| 44000155 | HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2" | SQ YD | 32,482 | 2604 | 29,878 | | |
| 44000158 | HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4" | SQ YD | 3483 | 3483 | | | |
| (S) 44000500 | COMBINATION CURB AND GUTTER REMOVAL | FOOT | 40 | 40 | | | |
| 44000600 | SIDEWALK REMOVAL | SQ FT | 201 | 201 | | | |
| 44200168 | PAVEMENT PATCHING, TYPE II, 14 INCH | SQ YD | 383 | 11 | 372 | | |

(S) SEE SPECIAL PROVISIONS * SPECIALITY ITEM

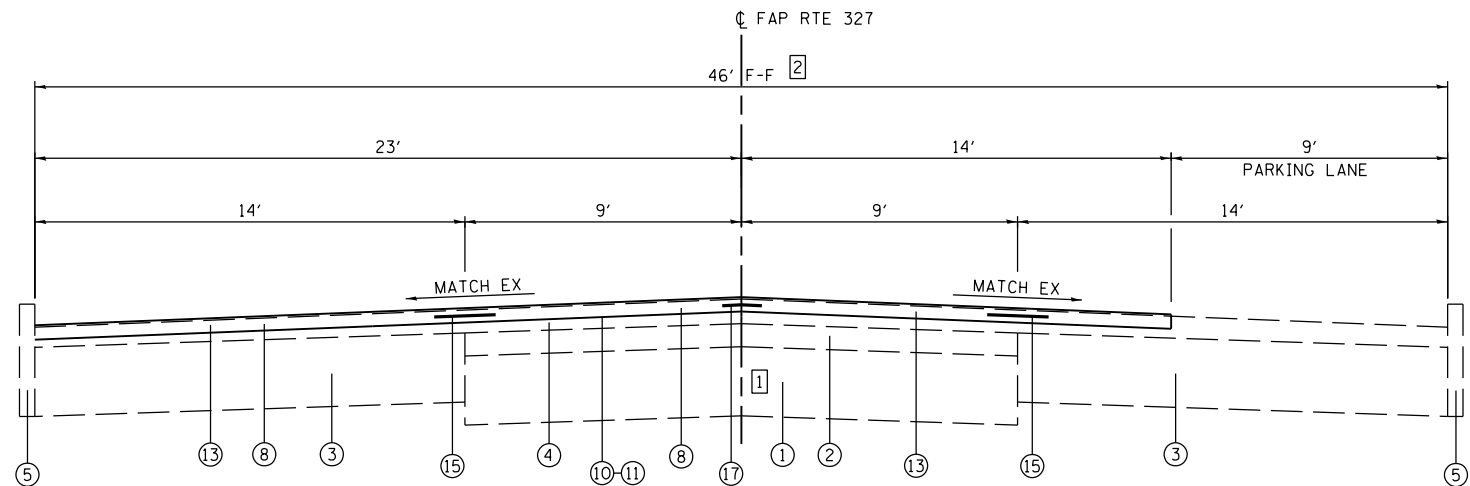
| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | CONSTRUCTION CODE | | | |
|--------------|--|--------|----------------|-------------------|------------|--|--|
| | | | | URBAN 0005 | RURAL 0005 | | |
| 44200172 | PAVEMENT PATCHING, TYPE III, 14 INCH | SO YD | 207 | | 207 | | |
| 44200174 | PAVEMENT PATCHING, TYPE IV, 14 INCH | SO YD | 253 | | 253 | | |
| 44200192 | PAVEMENT PATCHING, TYPE II, 16 INCH | SO YD | 18 | 18 | | | |
| 44300200 | STRIP REFLECTIVE CRACK CONTROL TREATMENT | FOOT | 2150 | 2150 | | | |
| 48102100 | AGGREGATE WEDGE SHOULDER, TYPE B | TON | 1064 | 83 | 981 | | |
| 48203100 | HOT-MIX ASPHALT SHOULDERS | TON | 819 | 66 | 753 | | |
| (S) 60604500 | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18 (ABUTTING EXISTING PAVEMENT) | FOOT | 40 | 40 | | | |
| 64200108 | SHOULDER RUMBLE STRIPS, 8 INCH | FOOT | 17,771 | 1296 | 16,475 | | |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 3 | 0.5 | 2.5 | | |
| 67100100 | MOBILIZATION | L SUM | 1 | 0.2 | 0.8 | | |
| 70100460 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701306 | L SUM | 1 | 0.2 | 0.8 | | |
| 70100600 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701336 | L SUM | 1 | 0.1 | 0.9 | | |
| (S) 70102620 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701501 | L SUM | 1 | 1 | | | |
| 70102640 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701801 | L SUM | 1 | 1 | | | |
| 70300100 | SHORT TERM PAVEMENT MARKING | FOOT | 3024 | 576 | 2448 | | |
| 70300150 | SHORT TERM PAVEMENT MARKING REMOVAL | SO FT | 1008 | 192 | 816 | | |

(S) SEE SPECIAL PROVISIONS * SPECIALITY ITEM

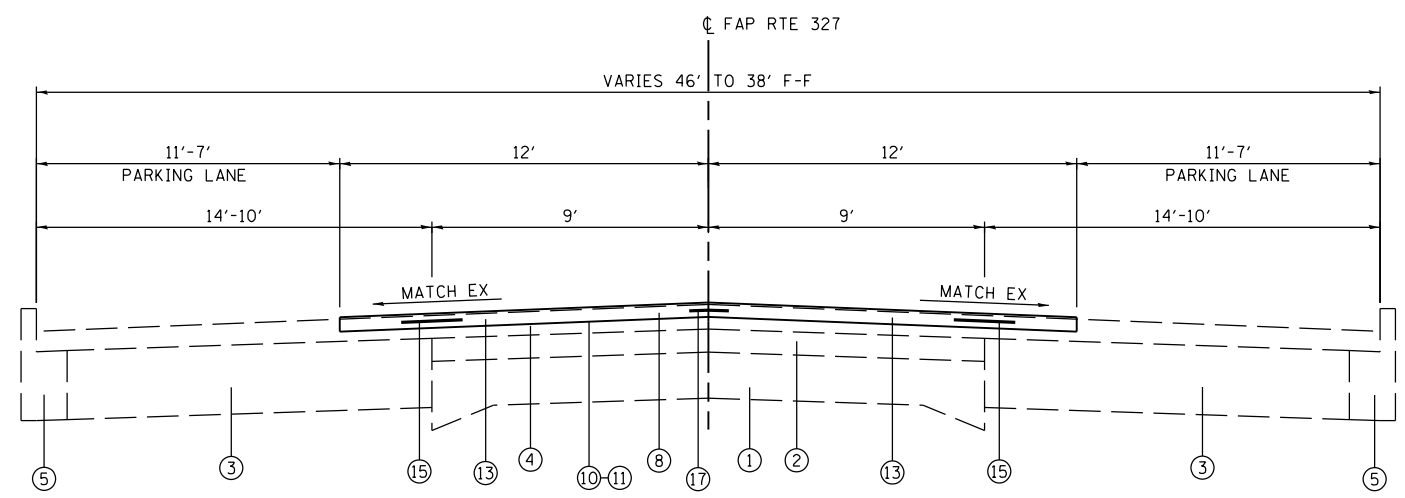
| | | | | | | | | | | | | |
|--|-----------------------------|---------------|-----------|---|--|--|--|---|---------|-----------|---------------------------|-----------|
| FILE NAME - P:\10e2166-26\30RS-5 76H13 US 50\DCAD0 | USER NAME - *USER* | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SUMMARY OF QUANTITIES FAP ROUTE 327 (US RTE 50) | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | Sheets\0876H13-sht-800.dgn | DRAWN - MCM | REVISED - | | | | | 327 | 30RS-5 | ST. CLAIR | 24 | 4 |
| | PLOT SCALE = 100.0000' / 1" | CHECKED - | REVISED - | | | | | SCALE: SHEET 2 OF 3 SHEETS STA. TO STA. | | | CONTRACT NO. 76H13 | |
| Default | PLOT DATE = 1/31/2018 | DATE - 1/2018 | REVISED - | | | | | ILLINOIS FED. AID PROJECT | | | | |

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | CONSTRUCTION CODE | | | | | |
|--------------|--|-------|----------------|-------------------|------------|--|--|--|--|
| | | | | URBAN 0005 | RURAL 0005 | | | | |
| 70300210 | TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS | SQ FT | 31 | 31 | | | | | |
| 70300220 | TEMPORARY PAVEMENT MARKING - LINE 4" | FOOT | 28,657 | 5068 | 23,589 | | | | |
| 70300240 | TEMPORARY PAVEMENT MARKING - LINE 6" | FOOT | 167 | 167 | | | | | |
| 70300260 | TEMPORARY PAVEMENT MARKING - LINE 12" | FOOT | 16 | 16 | | | | | |
| * 78000100 | THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS | SQ FT | 31 | 31 | | | | | |
| * 78000200 | THERMOPLASTIC PAVEMENT MARKING - LINE 4" | FOOT | 28,657 | 5068 | 23,589 | | | | |
| * 78000400 | THERMOPLASTIC PAVEMENT MARKING - LINE 6" | FOOT | 167 | 167 | | | | | |
| * 78000600 | THERMOPLASTIC PAVEMENT MARKING - LINE 12" | FOOT | 16 | 16 | | | | | |
| * 78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 140 | 28 | 112 | | | | |
| 78300200 | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | EACH | 140 | 28 | 112 | | | | |
| (S) X7030005 | TEMPORARY PAVEMENT MARKING REMOVAL | SQ FT | 9683 | 1820 | 7863 | | | | |
| (S) Z0033700 | LONGITUDINAL JOINT SEALANT | FOOT | 10,829 | 1864 | 8965 | | | | |
| (S) Z0034105 | MATERIAL TRANSFER DEVICE | TON | 3770 | 758 | 3012 | | | | |
| (S) Z0070100 | SURVEY MONUMENT COVER ASSEMBLY | EACH | 2 | 2 | | | | | |
| Ø Z0070600 | TRAINEES | HOOR | 500 | 500 | | | | | |
| Ø Z0070604 | TRAINEES TRAINING PROGRAM GRADUATE | HOOR | 500 | 500 | | | | | |

(S) SEE SPECIAL PROVISIONS * SPECIALITY ITEM Ø 0042



FAP ROUTE 327 (US ROUTE 50)
STA. 1+13.00 TO STA. 4+35.00

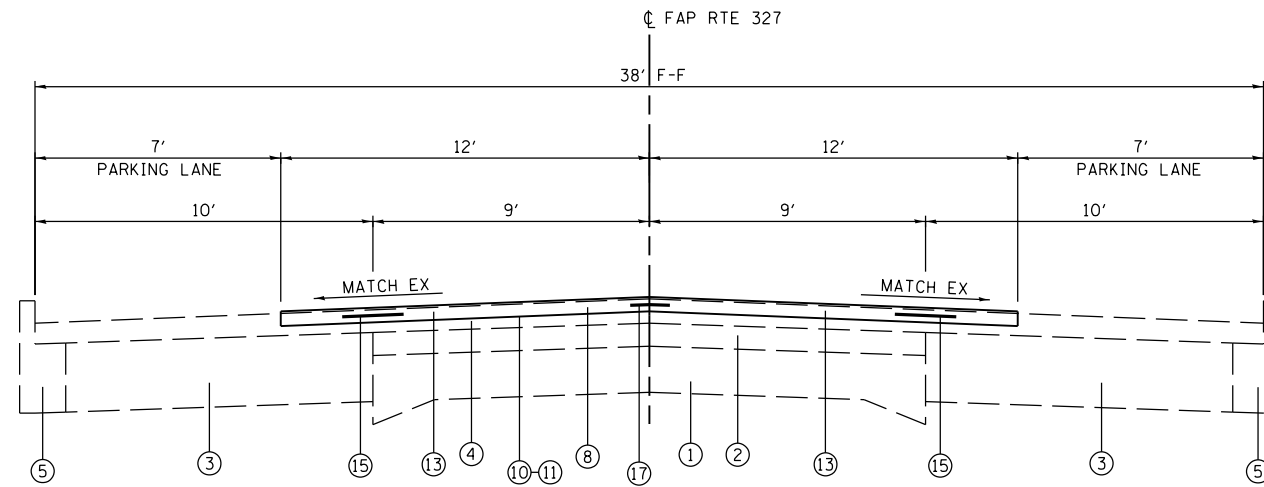


FAP ROUTE 327 (US ROUTE 50)
STA. 4+35.00 TO STA. 5+29.00

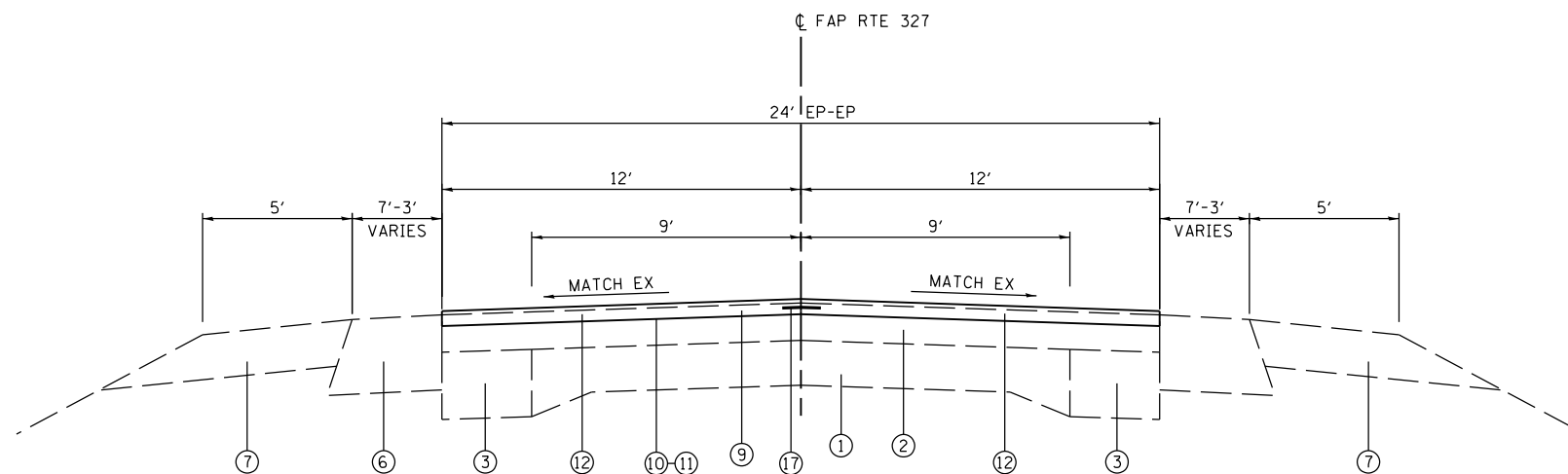
- ① EXISTING CONCRETE PAVEMENT 9"-6"-9"
- ② EXISTING HMA SURFACE COURSE 3"-5"
- ③ EXISTING PC CONCRETE BASE COURSE WIDENING 9"
- ④ EXISTING HMA SURFACE COURSE 4"
- ⑤ EXISTING CONC CURB AND/OR CURB AND GUTTER
- ⑥ EXISTING HMA SHOULDER
- ⑦ EXISTING AGGREGATE SHOULDER
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
- ⑩ PROPOSED BITUMINOUS MATERIALS (TACK COAT)
- ⑪ PROPOSED AGGREGATE (TACK COAT) (IF REQUIRED)
- ⑫ PROPOSED LEVELING BINDER (MACHINE METHOD), IL-9.5FG, N70 3/4"
- ⑬ PROPOSED LEVELING BINDER (MACHINE METHOD), IL-9.5FG, N70 1"
- ⑭ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 1 1/2"
- ⑮ PROPOSED HOT-MIX ASPHALT SHOULDER, 2 1/4"
- ⑯ PROPOSED STRIP REFLECTIVE CRACK CONTROL
- ⑰ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑱ PROPOSED LONGITUDINAL JOINT SEALANT UNDER SURFACE LIFT

NOTE:

- ① EXISTING CONCRETE PAVEMENT AND BRICK 9" STA. 1+13.00 TO 4+35.00, LT. & RT.
- ② MILLING AND RESURFACING SHALL BE FACE TO FACE OF CURB FROM STA. 1+13.00 TO 1+56.00
LEVELING BINDER (MACHINE METHOD) TO BE PLACED ON MAINLINE (US 50) ONLY



FAP ROUTE 327 (US ROUTE 50)
STA. 5+29.00 TO STA. 11+67.00



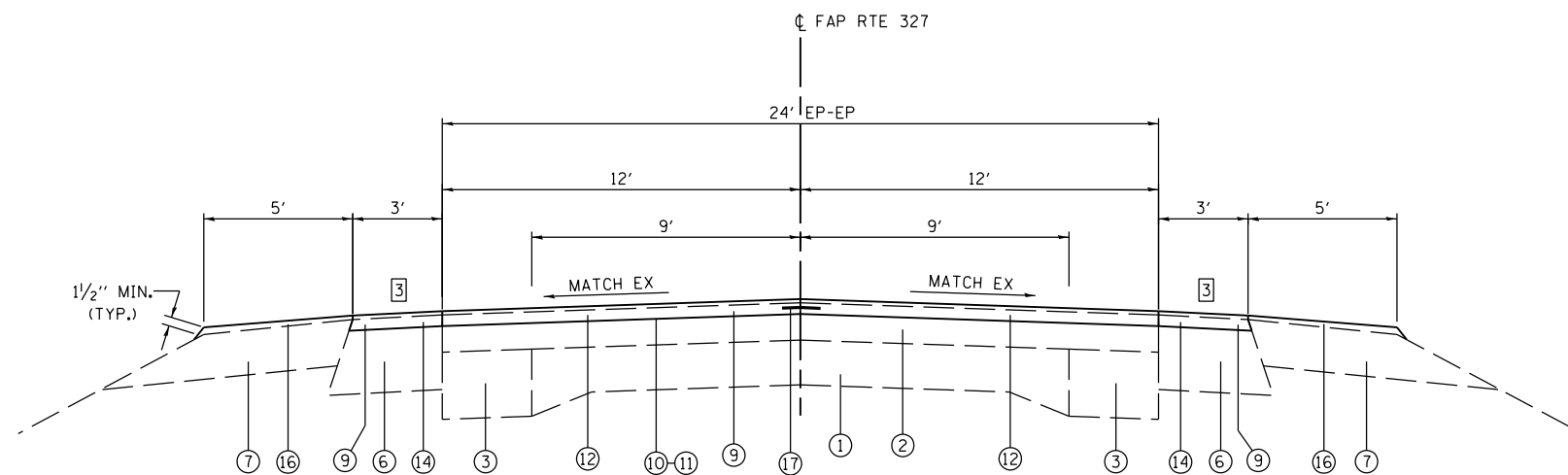
FAP ROUTE 327 (US ROUTE 50)
STA. 11+67.00 TO STA. 11+88.00

- ① EXISTING CONCRETE PAVEMENT 9"-6"-9"
- ② EXISTING HMA SURFACE COURSE 3"-5"
- ③ EXISTING PC CONCRETE BASE COURSE WIDENING 9"
- ④ EXISTING HMA SURFACE COURSE 4"
- ⑤ EXISTING CONC CURB AND/OR CURB AND GUTTER
- ⑥ EXISTING HMA SHOULDER
- ⑦ EXISTING AGGREGATE SHOULDER
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
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- ⑩ PROPOSED BITUMINOUS MATERIALS (TACK COAT)
- ⑪ PROPOSED AGGREGATE (TACK COAT) (IF REQUIRED)
- ⑫ PROPOSED LEVELING BINDER (MACHINE METHOD), IL-9.5FG, N70 3/4"
PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 1 1/2"
- ⑬ PROPOSED LEVELING BINDER (MACHINE METHOD), IL-9.5FG, N70 1"
PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 1 1/2"
- ⑭ PROPOSED HOT-MIX ASPHALT SHOULDER, 2 1/4"
- ⑮ PROPOSED STRIP REFLECTIVE CRACK CONTROL
- ⑯ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑰ PROPOSED LONGITUDINAL JOINT SEALANT UNDER SURFACE LIFT

NOTE:

LEVELING BINDER (MACHINE METHOD) TO BE PLACED ON MAINLINE (US 50) ONLY

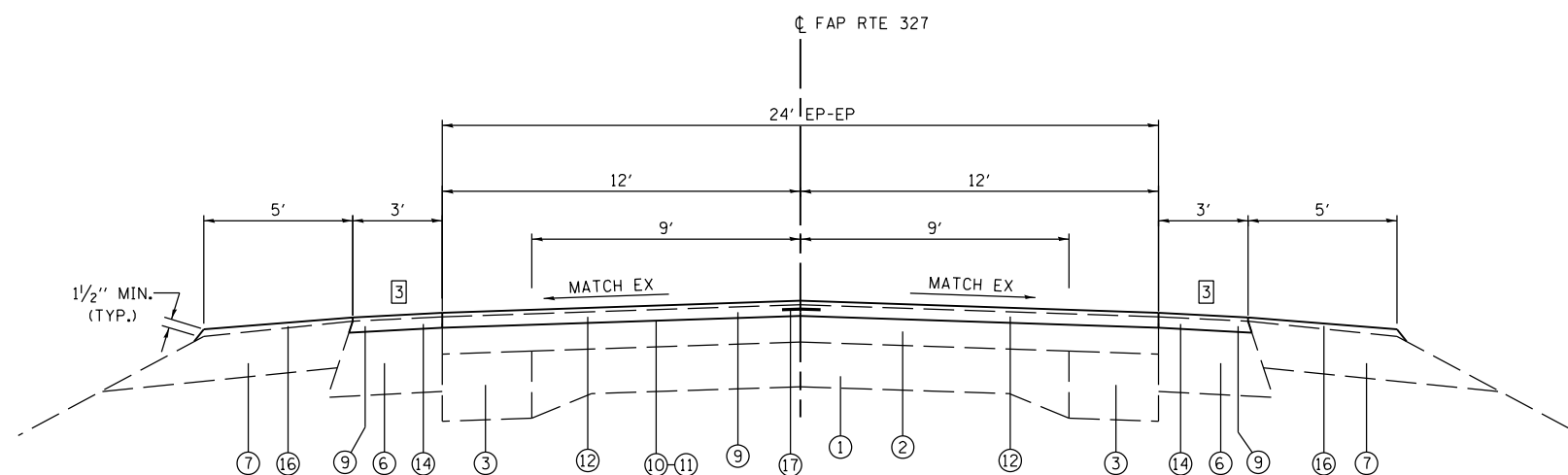
| | | | | | | | | | | | | |
|---|--------------------------------|---------------|-----------|---|---|---------------------|-----------------|--------------------|------------------|---------------------------|-------------|--|
| FILE NAME = P:\10e2166-26\30RS-5 76H13 US 50\CADD | USER NAME = #USER# | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL SECTIONS FAP ROUTE 327 (US RTE 50) | | F.A.P. RTE. 327 | SECTION 30RS-5 | COUNTY ST. CLAIR | TOTAL SHEETS 24 | SHEET NO. 7 | |
| Default | Sheets\0876H13-sht-typical.dgn | DRAWN - MGM | REVISED - | | SCALE: | SHEET 2 OF 3 SHEETS | STA. TO STA. | CONTRACT NO. 76H13 | | ILLINOIS FED. AID PROJECT | | |
| | PLOT SCALE = 100.0000' / 1" = | CHECKED - | REVISED - | | | | | | | | | |
| | PLOT DATE = 1/30/2018 | DATE - 1/2018 | REVISED - | | | | | | | | | |



FAP ROUTE 327 (US ROUTE 50)
STA. 11+88.00 TO STA. 19+77.11

BRIDGE OMISSION - STA. 19+77.11 TO 21+25.39

- ① EXISTING CONCRETE PAVEMENT 9"-6"-9"
- ② EXISTING HMA SURFACE COURSE 3"-5"
- ③ EXISTING PC CONCRETE BASE COURSE WIDENING 9"
- ④ EXISTING HMA SURFACE COURSE 4"
- ⑤ EXISTING CONC CURB AND/OR CURB AND GUTTER
- ⑥ EXISTING HMA SHOULDER
- ⑦ EXISTING AGGREGATE SHOULDER
- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
- ⑩ PROPOSED BITUMINOUS MATERIALS (TACK COAT)
- ⑪ PROPOSED AGGREGATE (TACK COAT) (IF REQUIRED)
- ⑫ PROPOSED LEVELING BINDER (MACHINE METHOD), IL-9.5FG, N70 3/4"
PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 1 1/2"
- ⑬ PROPOSED LEVELING BINDER (MACHINE METHOD), IL-9.5FG, N70 1"
PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 1 1/2"
- ⑭ PROPOSED HOT-MIX ASPHALT SHOULDER, 2 1/4"
- ⑮ PROPOSED STRIP REFLECTIVE CRACK CONTROL
- ⑯ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑰ PROPOSED LONGITUDINAL JOINT SEALANT UNDER SURFACE LIFT



FAP ROUTE 327 (US ROUTE 50)
STA. 21+25.39 TO STA. 110+90.00

NOTE:

- ③ SHOULDER RUMBLE STRIPS, 8 INCH SHALL BE INSTALLED FROM STA. 12+06.00 TO 110+90.00 (SEE PLAN SHEETS)
- LEVELING BINDER (MACHINE METHOD) TO BE PLACED ON MAINLINE (US 50) ONLY

| | | | | | | | | | | | | |
|---|--------------------------------|---------------|-----------|---|---|---------------------|-----------------|--------------------|------------------|---------------------------|-------------|--|
| FILE NAME = P:\10e2166-26\30RS-5 76H13 US 50\CADD | USER NAME = #USER# | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL SECTIONS FAP ROUTE 327 (US RTE 50) | | F.A.P. RTE. 327 | SECTION 30RS-5 | COUNTY ST. CLAIR | TOTAL SHEETS 24 | SHEET NO. 8 | |
| Default | Sheets\0876H13-sht-typical.dgn | DRAWN - MGM | REVISED - | | SCALE: | SHEET 3 OF 3 SHEETS | STA. TO STA. | CONTRACT NO. 76H13 | | ILLINOIS FED. AID PROJECT | | |
| | PLOT SCALE = 100.0000' / 1" | CHECKED - | REVISED - | | | | | | | | | |
| | PLOT DATE = 1/30/2018 | DATE - 1/2018 | REVISED - | | | | | | | | | |

SURFACE REMOVAL SCHEDULE

| LOCATION STATION TO STATION | HOT-MIX ASPHALT SURF REM BUTT JOINT | TEMPORARY RAMP | HOT-MIX ASPHALT SURF REM 1 1/2" | HOT-MIX ASPHALT SURF REM 2 1/4" |
|--|--|-------------------|--|--|
| | SO YD | SO YD | SO YD | SO YD |
| STA. 1+13 TO STA. 1+21, US 50, LT. & RT. | 40.9 | 40.9 | | |
| STA. 1+13 TO STA. 4+35, US 50, LT. & RT. | | | | 1433.8 |
| STA. 4+35 TO STA. 11+88, US 50, LT. & RT. | | | | 2008.0 |
| STA. 11+88 TO STA. 19+69.11, US 50, LT. & RT. | | | 2083.0 | |
| STA. 19+69.11 TO STA. 19+77.11, US 50, LT. & RT. | 26.7 | 20.0 | | |
| STA. 21+25.39 TO STA. 21+33.39, US 50, LT. & RT. | 26.7 | 20.0 | | |
| STA. 21+33.39 TO STA. 110+90, US 50, LT. & RT. | | | 23,884.3 | |
| STA. 110+82 TO STA. 110+90, US 50, LT. & RT. | 26.7 | 20.0 | | |
| TOTAL | 121.0* | 100.9* | 25,967.3* | 3441.8* |

* NOT A TOTAL QUANTITY

SHOULDER SCHEDULE

| LOCATION STATION TO STATION | BITUMINOUS MATERIALS (TACK COAT) | HOT-MIX ASPHALT SURF REM 1 1/2" | HOT-MIX ASPHALT SHOULDERS | AGGREGATE WEDGE SHOULDER, TYPE B | SHOULDER RUMBLE STRIPS, 8 INCH |
|--|--|--|---------------------------------|---|---|
| | POUND | SO YD | TON | TON | FOOT |
| STA. 11+88 TO STA. 19+77.11, US 50, LT. & RT. | 236.74 | 520.7 | 66.28 | 82.95 | |
| STA. 12+06 TO STA. 110+90, US 50, LT. & RT. | | | | | 17,771.0 |
| STA. 21+25.39 TO STA. 110+90, US 50, LT. & RT. | 2689.38 | 5971.1 | 753.02 | 980.76 | |
| TOTAL | 2926.12* | 6491.8* | 819.30 | 1063.71 | 17,771.0 |

* NOT A TOTAL QUANTITY

RESURFACING SCHEDULE

| LOCATION STATION TO STATION | BITUMINOUS MATERIALS (TACK COAT) | LEVELING BINDER, (MACHINE METHOD) IL-9.5FG, N70 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 | STRIP REFLECTIVE CRACK CONTROL TREATMENT | LONGITUDINAL JOINT SEALANT |
|--|--|---|---|---|-------------------------------|
| | POUND | TON | TON | FOOT | FOOT |
| STA. 1+13 TO STA. 11+88, US 50, LT. | | | | 1075.0 | |
| STA. 1+13 TO STA. 11+88, US 50, RT. | | | | 1075.0 | |
| STA. 1+13 TO STA. 19+77.11, US 50, LT. & RT. | 3743.60 | 280.34 | 465.87 | | |
| STA. 1+13 TO STA. 19+77.11, US 50, CL | | | | | 1864.1 |
| STA. 21+25.39 TO STA. 110+90, US 50, CL | | | | | 8964.6 |
| STA. 21+25.39 TO STA. 110+90, US 50, LT. & RT. | 16,136.30 | 1004.04 | 2008.07 | | |
| TOTAL | 19,879.90* | 1284.38 | 2473.94* | 2150.0 | 10,828.7 |

* NOT A TOTAL QUANTITY

SIDEWALK, CURB AND GUTTER SCHEDULE

| LOCATION STATION TO STATION | PC CONCRETE SIDEWALK 4 INCH | DETECTABLE WARNINGS | COMBINATION CURB AND GUTTER REMOVAL | SIDEWALK REMOVAL | COMB. CONC. CURB AND GUTTER, TYPE B-6.18 (ABUTTING EX PAVEMENT) |
|--------------------------------------|-----------------------------------|------------------------|--|---------------------|--|
| | SO FT | SO FT | FOOT | SO FT | FOOT |
| NW QUADRANT FAP 327 AND FRITZ STREET | 78.9 | 8.0 | 14.5 | 78.9 | 14.5 |
| SW QUADRANT FAP 327 AND FRITZ STREET | 86.0 | 8.0 | 15.0 | 86.0 | 15.0 |
| NE QUADRANT FAP 327 AND FRITZ STREET | 33.2 | 8.0 | 10.5 | 36.2 | 10.5 |
| TOTAL | 198.1 | 24.0 | 40.0 | 201.1 | 40.0 |

NOTE:

FACTORS USED FOR ESTIMATING PLAN QUANTITIES ARE AS FOLLOWS AND SHALL NOT BE USED FOR THE BASIS OF FINAL QUANTITIES:

ALL HOT-MIX ASPHALT..... 112 LB/SO YD/INCH
 BITUMINOUS MATERIALS (PRIME COAT)
 ON MILLED PAVEMENTS..... 0.05 LB/SO FT
 ON AGGREGATE SURFACES..... 0.25 LB/SO FT
 BITUMINOUS MATERIALS (TACK COAT)
 BETWEEN LIFTS..... 0.025 LB/SO FT
 ALL AGGREGATE..... 2.05 TON/CU YD

PAVEMENT PATCHING SCHEDULE

| LOCATION STATION TO STATION | LANE | LENGTH (FEET) | WIDTH (FEET) | PAVEMENT PATCHING, TYPE II, 14 INCH | PAVEMENT PATCHING, TYPE III, 14 INCH | PAVEMENT PATCHING, TYPE IV, 14 INCH | PAVEMENT PATCHING, TYPE II, 16 INCH |
|--------------------------------|------|------------------|-----------------|--|---|--|--|
| | | | | SQ YD | SQ YD | SQ YD | SQ YD |
| STA. 10+56 | WB | 6.0 | 12.0 | | | | 8.0 |
| STA. 10+56 | EB | 6.0 | 12.0 | | | | 8.0 |
| STA. 15+84 | WB | 4.0 | 12.0 | 5.3 | | | |
| STA. 15+84 | EB | 4.0 | 12.0 | 5.3 | | | |
| STA. 21+12 | WB | 10.0 | 12.0 | 13.3 | | | |
| STA. 21+12 | EB | 10.0 | 12.0 | 13.3 | | | |
| STA. 31+68 | WB | 6.0 | 12.0 | 8.0 | | | |
| STA. 31+68 | EB | 6.0 | 12.0 | 8.0 | | | |
| STA. 36+96 | WB | 15.0 | 12.0 | | 20.0 | | |
| STA. 36+96 | EB | 20.0 | 12.0 | | | 26.7 | |
| STA. 38+02 | WB | 15.0 | 12.0 | | 20.0 | | |
| STA. 38+02 | EB | 15.0 | 12.0 | | 20.0 | | |
| STA. 39+07 | WB | 10.0 | 12.0 | 13.3 | | | |
| STA. 48+05 | WB | 15.0 | 12.0 | | 20.0 | | |
| STA. 48+05 | EB | 15.0 | 12.0 | | 20.0 | | |
| STA. 48+58 | WB | 40.0 | 12.0 | | | 53.3 | |
| STA. 48+58 | EB | 10.0 | 12.0 | 13.3 | | | |
| STA. 49+10 | WB | 20.0 | 12.0 | | | 26.7 | |
| STA. 49+10 | EB | 10.0 | 12.0 | 13.3 | | | |
| STA. 49+63 | WB | 15.0 | 12.0 | | 20.0 | | |
| STA. 49+63 | EB | 6.0 | 12.0 | 8.0 | | | |
| STA. 50+16 | WB | 6.0 | 12.0 | 8.0 | | | |
| STA. 50+16 | EB | 6.0 | 12.0 | 8.0 | | | |
| STA. 58+08 | WB | 8.0 | 12.0 | 10.7 | | | |
| STA. 58+08 | EB | 8.0 | 12.0 | 10.7 | | | |
| STA. 63+36 | WB | 10.0 | 12.0 | 13.3 | | | |
| STA. 63+36 | EB | 15.0 | 12.0 | | 20.0 | | |
| STA. 68+64 | WB | 10.0 | 12.0 | 13.3 | | | |
| STA. 68+64 | EB | 10.0 | 12.0 | 13.3 | | | |
| STA. 79+20 | EB | 10.0 | 12.0 | 13.3 | | | |
| STA. 80+26 | WB | 10.0 | 12.0 | 13.3 | | | |
| STA. 80+26 | EB | 10.0 | 12.0 | 13.3 | | | |
| STA. 81+31 | WB | 10.0 | 12.0 | 13.3 | | | |
| STA. 81+31 | EB | 10.0 | 12.0 | 13.3 | | | |
| STA. 84+48 | WB | 10.0 | 12.0 | 13.3 | | | |
| STA. 84+48 | EB | 10.0 | 12.0 | 13.3 | | | |
| STA. 85+54 | WB | 10.0 | 12.0 | 13.3 | | | |
| STA. 85+54 | EB | 10.0 | 12.0 | 13.3 | | | |
| STA. 89+76 | WB | 20.0 | 12.0 | | | 26.7 | |
| STA. 89+76 | EB | 20.0 | 12.0 | | | 26.7 | |
| STA. 95+04 | WB | 20.0 | 12.0 | | | 26.7 | |
| STA. 96+10 | WB | 15.0 | 12.0 | | 20.0 | | |
| STA. 100+32 | WB | 10.0 | 12.0 | 13.3 | | | |
| STA. 100+32 | EB | 6.0 | 12.0 | 8.0 | | | |
| STA. 105+60 | WB | 15.0 | 12.0 | | 20.0 | | |
| STA. 110+78 | WB | 25.0 | 12.0 | | | 33.3 | |
| STA. 110+78 | EB | 10.0 | 12.0 | 13.3 | | | |
| SUBTOTAL | | | | 332.7 | 180.0 | 220.1 | 16.0 |
| ANTICIPATED FAILURES (15%) | | | | 50.0 | 27.0 | 33.0 | 2.4 |
| TOTAL | | | | 382.7 | 207.0 | 253.1 | 18.4 |

NOTE: PATCHING LOCATIONS/DIMENSIONS PROVIDED BY DISTRICT

ENTRANCE SCHEDULE

| LOCATION STATION TO STATION | TYPE | EXISTING SURFACE | LENGTH (FEET) | WIDTH (FEET) | BITUMINOUS MATERIALS (TACK COAT) | INCIDENTAL HOT-MIX ASPHALT SURFACING |
|--------------------------------|------|---------------------|------------------|-----------------|--|---|
| | | | | | POUND | TON |
| STA. 13+30, RT. | PE | AGG | 3 | 14 | 2.55 | 0.56 |
| STA. 14+11, RT. | PE | O&C | 3 | 14 | 2.55 | 0.56 |
| STA. 14+70, RT. | PE | EARTH | 3 | 20 | 3.45 | 0.75 |
| STA. 25+96, LT. | PE | AGG | 3 | 20 | 3.45 | 0.75 |
| STA. 29+33, LT. | PE | AGG | 3 | 35 | 5.70 | 1.24 |
| STA. 30+72, LT. | PE | AGG | 3 | 30 | 4.95 | 1.08 |
| STA. 31+89, LT. | PE | AGG | 3 | 24 | 4.05 | 0.88 |
| STA. 33+37, LT. | CE | AGG | 3 | 30 | 4.95 | 1.08 |
| STA. 34+95, LT. | CE | AGG | 3 | 30 | 4.95 | 1.08 |
| STA. 36+51, LT. | PE | AGG | 3 | 24 | 4.05 | 0.88 |
| STA. 37+69, LT. | PE | AGG | 3 | 18 | 3.15 | 0.69 |
| STA. 48+83, RT. | PE | AGG | 3 | 20 | 3.45 | 0.75 |
| STA. 50+63, LT. | PE | AGG | 3 | 40 | 6.45 | 1.40 |
| STA. 54+30, RT. | CE | AGG | 3 | 50 | 7.95 | 1.73 |
| STA. 58+20, RT. | PE | EARTH | 3 | 40 | 6.45 | 1.40 |
| STA. 70+41, RT. | FE | AGG | 3 | 26 | 4.35 | 0.95 |
| STA. 81+11, RT. | PE | AGG | 3 | 26 | 4.35 | 0.95 |
| STA. 104+91, LT. | PE | CONC | 5.5 | 26 | 7.98 | 1.74 |
| TOTAL | | | | | 84.78 | 18.47* |

* NOT A TOTAL QUANTITY

MAILBOX TURNOUT SCHEDULE

| LOCATION STATION TO STATION | BITUMINOUS MATERIALS (TACK COAT) | INCIDENTAL HOT-MIX ASPHALT SURFACING | HOT-MIX ASPHALT SURF REM 1 1/2" | | | |
|--------------------------------|--|---|--|--------|-------|-------|
| | POUND | TON | SQ YD | | | |
| STA. 13+11, LT. | 11.25 | 4.19 | 22.7 | | | |
| TOTAL | | | | 11.25* | 4.19* | 22.7* |

* NOT A TOTAL QUANTITY

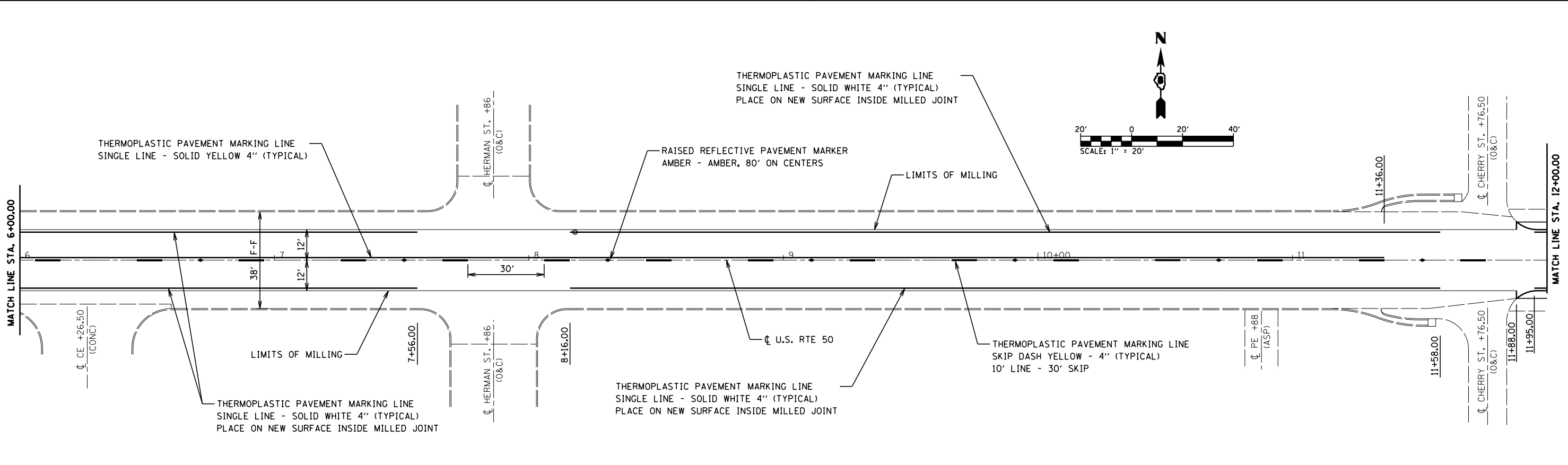
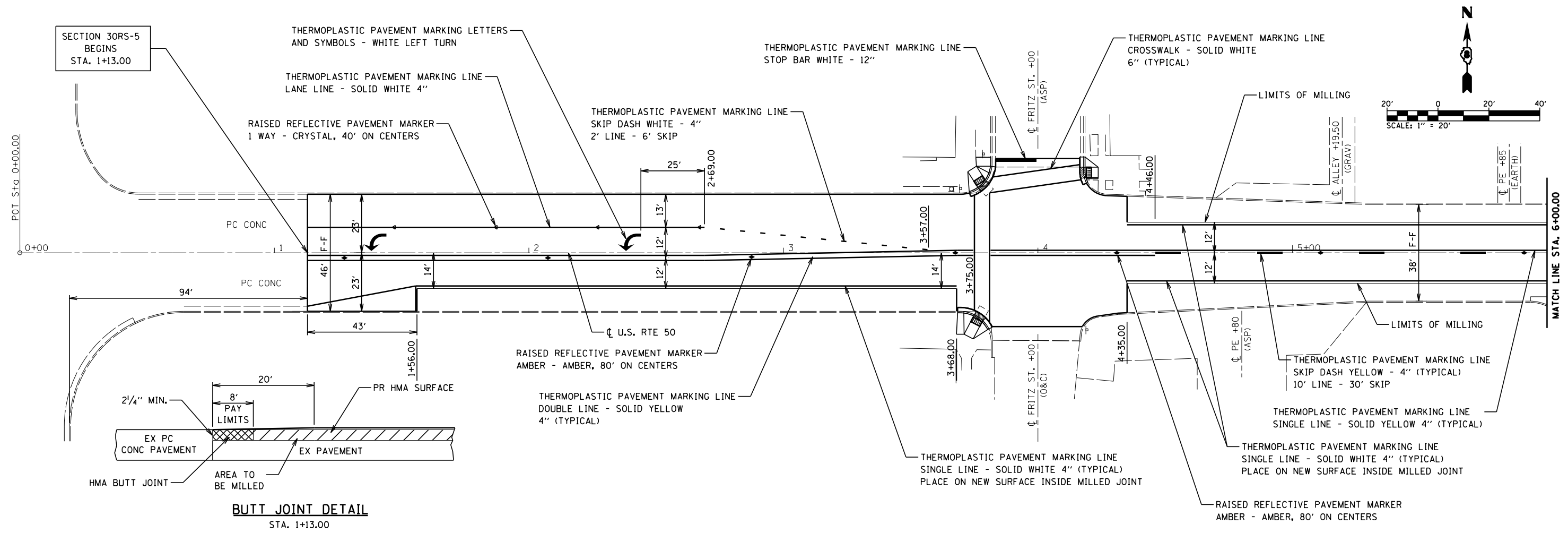
SIDEROAD SCHEDULE

| LOCATION STATION TO STATION | EXISTING SURFACE | LENGTH (FEET) | WIDTH (FEET) | HOT-MIX ASPHALT SURF REM BUTT JOINT | PC CONC SURF REM BUTT JOINT | TEMPORARY RAMP | BITUMINOUS MATERIALS (TACK COAT) | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 | BITUMINOUS MATERIALS (TACK COAT) | INCIDENTAL HOT-MIX ASPHALT SURFACING | HOT-MIX ASPHALT SURF REM 2 1/4" |
|---|---------------------|------------------|-----------------|--|-----------------------------------|-------------------|--|---|--|---|--|
| | | | | SQ YD | SQ YD | SQ YD | POUND | TON | POUND | TON | SQ YD |
| STA. 4+00, FRITZ ST., RT. | HMA/CONC | 6 | 33 | | 22.0 | 22.0 | 12.55 | 3.90 | | | 5.9 |
| STA. 4+00, FRITZ ST., LT. | HMA/CONC | 14 | 33 | | 22.0 | 22.0 | 25.75 | 8.01 | | | 35.2 |
| STA. 7+86, HERMAN ST., RT. | HMA/CONC | 6 | 44 | | | 29.3 | | | | | |
| STA. 7+86, HERMAN ST., LT. | HMA/CONC | 6 | 44 | | | 29.3 | | | | | |
| STA. 11+76.50, CHERRY ST., RT. | O&C | 6 | 24 | | | 16.0 | | | | | |
| STA. 11+76.50, CHERRY ST., LT. | O&C | 6 | 24 | | | 16.0 | | | | | |
| STA. 15+69.50, PLUM ST., RT. | O&C | 3.5 | 58 | 22.6 | | | | | 10.15 | 2.21 | |
| STA. 79+29, EMERALD MOUND GRANGE RD., LT. | O&C | 3.5 | 70 | 27.2 | | | | | 12.25 | 2.67 | |
| TOTAL | | | | 49.8* | 44.0 | 134.6* | 38.30* | 11.91* | 22.40* | 4.88* | 41.1* |

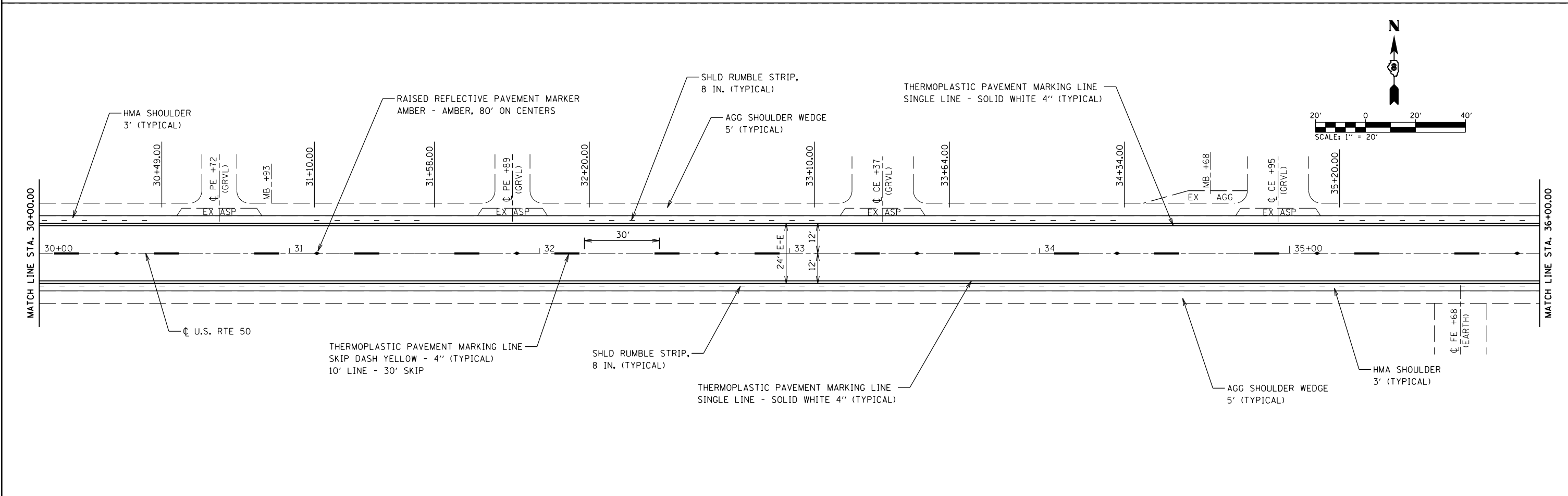
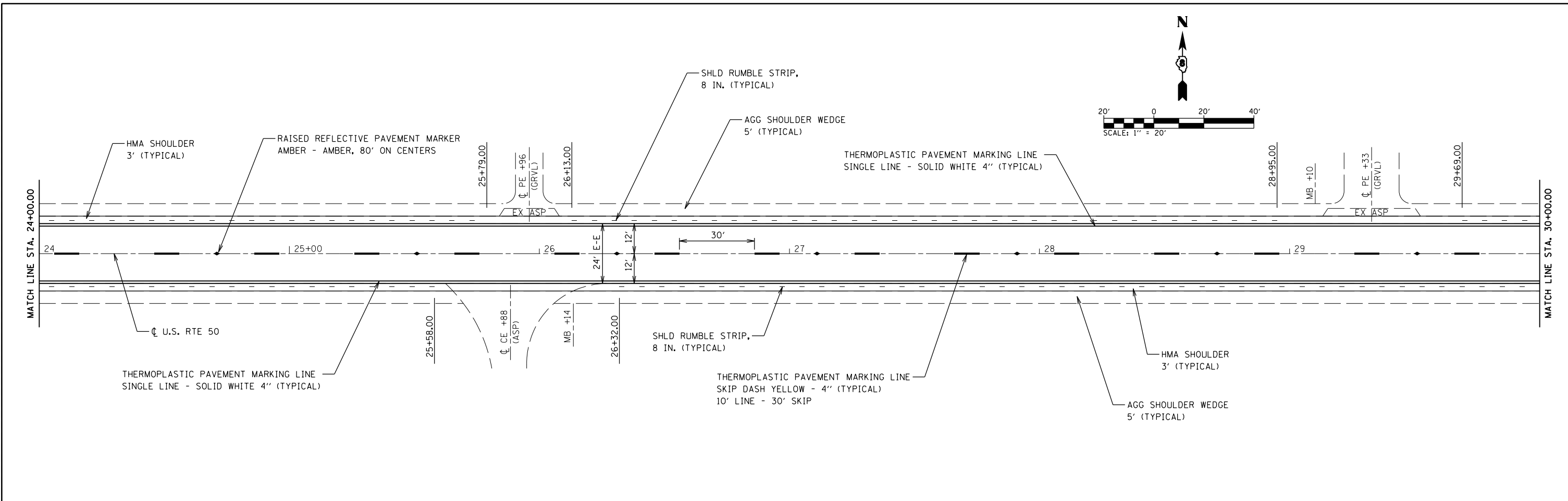
* NOT A TOTAL QUANTITY

PAVEMENT MARKING SCHEDULE

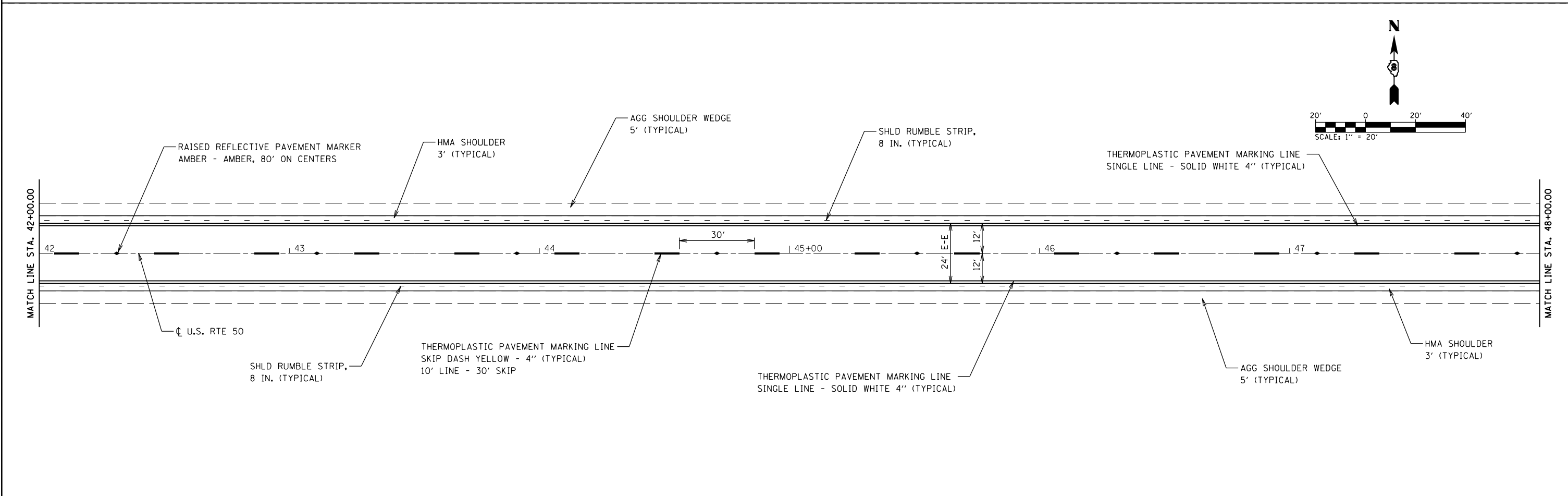
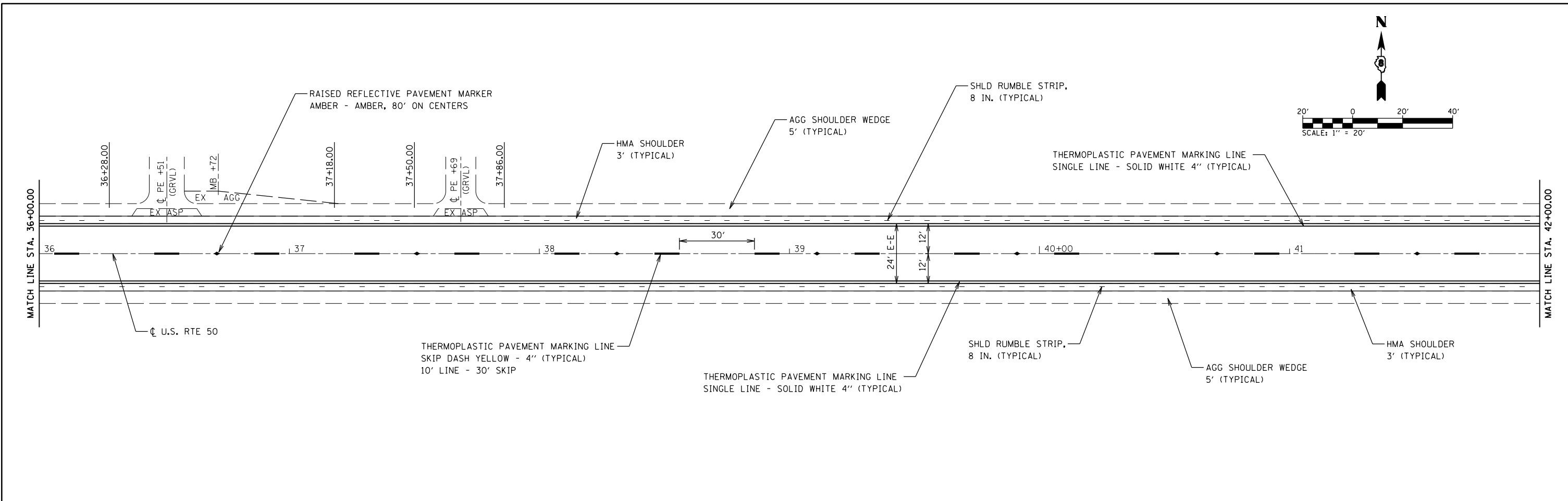
| LOCATION STATION TO STATION | SHORT TERM PAVEMENT MARKING | SHORT TERM PAVEMENT MARKING REMOVAL | TEMPORARY PAVEMENT MARKING LETTERS & SYMBOLS | TEMPORARY PAVEMENT MARKING - LINE 4" | TEMPORARY PAVEMENT MARKING - LINE 6" | TEMPORARY PAVEMENT MARKING - LINE 12" | TEMPORARY PAVEMENT MARKING REMOVAL | THERMOPLASTIC PAVEMENT MARKING LETTERS & SYMBOLS | THERMOPLASTIC PAVEMENT MARKING - LINE 4" | THERMOPLASTIC PAVEMENT MARKING - LINE 6" | THERMOPLASTIC PAVEMENT MARKING - LINE 12" | RAISED REFLECTIVE PAVEMENT MARKER | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL |
|---------------------------------------|-----------------------------------|--|---|---|---|--|---|---|---|---|--|--|---|
| | FOOT | SO FT | SO FT | FOOT | FOOT | FOOT | SO FT | SO FT | FOOT | FOOT | FOOT | EACH | EACH |
| STA. 1+13 TO STA. 3+57, US 50, LT. | 60.0 | 20.01 | | 178.0 | | | 59.3 | | 178.0 | | | 4 | 4 |
| STA. 1-13 TO STA. 3+68, US 50, RT. | | | | 265.0 | | | 88.3 | | 265.0 | | | | |
| STA. 1+13 TO STA. 19+61, US 50, CL | 516.0 | 171.99 | | | | | | | | | | | |
| STA. 1+13 TO STA. 3+75, US 50, CL | | | | 524.0 | | | 174.7 | | 524.0 | | | 4 | 4 |
| STA. 1+40, US 50, LT. | | | 15.6 | | | | 15.6 | 15.6 | | | | | |
| STA. 2+40, US 50, LT. | | | 15.6 | | | | 15.6 | 15.6 | | | | | |
| STA. 3+78, US 50, LT. & RT. | | | | | 96.5 | | 48.3 | | | 96.5 | | | |
| STA. 3+81 TO STA. 4+17, US 50, LT. | | | | | 70.5 | | 35.3 | | | 70.5 | | | |
| STA. 3+81 TO STA. 4+46, US 50, CL | | | | 130.0 | | | 43.3 | | 130.0 | | | | |
| STA. 3+83 TO STA. 3+99, US 50, LT. | | | | | | 16.0 | 16.0 | | | | 16.0 | | |
| STA. 4+35 TO STA. 7+56, US 50, LT. | | | | 321.0 | | | 107.0 | | 321.0 | | | | |
| STA. 4+35 TO STA. 7+56, US 50, RT. | | | | 321.0 | | | 107.0 | | 321.0 | | | | |
| STA. 4+46 TO STA. 19+76, US 50, CL | | | | 1080.0 | | | 360.0 | | 1080.0 | | | 20 | 20 |
| STA. 8+16 TO STA. 11+58, US 50, LT. | | | | 342.0 | | | 114.0 | | 342.0 | | | | |
| STA. 8+16 TO STA. 11+58, US 50, RT. | | | | 342.0 | | | 114.0 | | 342.0 | | | | |
| STA. 11+95 TO STA. 19+74, US 50, LT. | | | | 779.0 | | | 259.7 | | 779.0 | | | | |
| STA. 11+95 TO STA. 19+81, US 50, RT. | | | | 786.0 | | | 262.0 | | 786.0 | | | | |
| STA. 21+21 TO STA. 110+90, US 50, LT. | | | | 8969.0 | | | 2989.7 | | 8969.0 | | | | |
| STA. 21+26 TO STA. 110+90, US 50, CL | 2448.0 | 816.0 | | 5658.0 | | | 1886.0 | | 5658.0 | | | 112 | 112 |
| STA. 21+28 TO STA. 110+90, US 50, RT. | | | | 8962.0 | | | 2987.3 | | 8962.0 | | | | |
| TOTAL | 3024.0 | 1008.0 | 31.2 | 28,657.0 | 167.0 | 16.0 | 9683.1 | 31.2 | 28,657.0 | 167.0 | 16.0 | 140 | 140 |



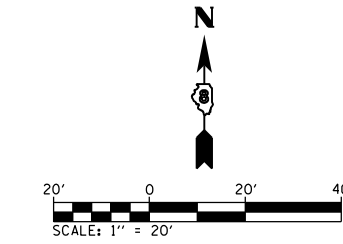
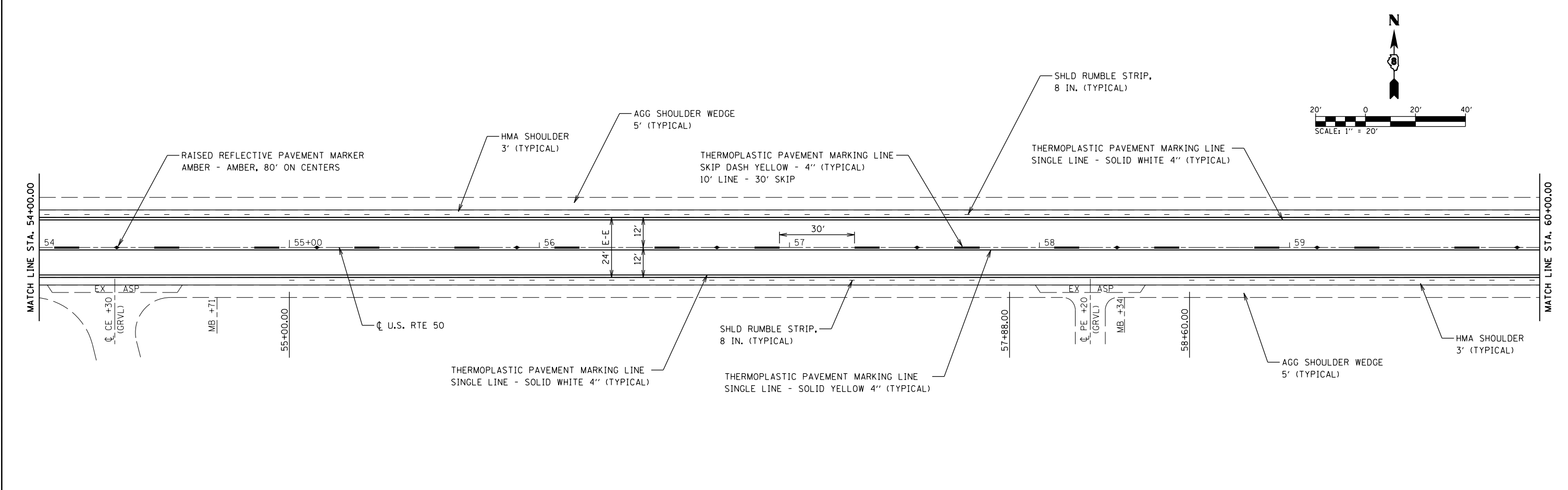
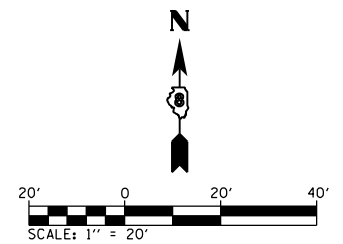
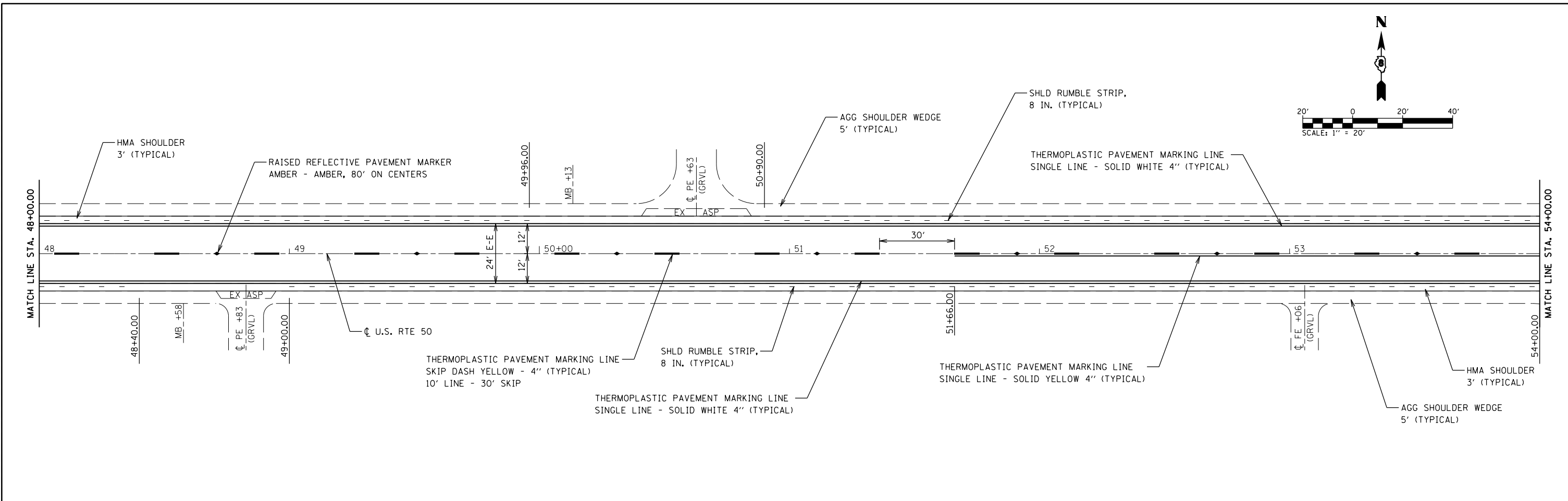
| | | | | | | | | | | | |
|---|-----------------------------|----------------|---------------------|---|----------------------------------|----------------------|-------------------------------|---------|---------------------------|--------------|-----------|
| FILE NAME = | USER NAME = \$USER\$ | DESIGNED - | REVISED - 3/22/2018 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PLAN | | F.A.P. R.T.E. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| P:\10E2166-21\30RS-5 76H13 US 50\CADD S | sets\0876H13-sht-plan.dgn | DRAWN - MGM | REVISED - | | FAP ROUTE 327 (US RTE 50) | | 327 | 30RS-5 | ST. CLAIR | 24 | 12 |
| Default | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | | SCALE: | SHEET 1 OF 10 SHEETS | STA. 0+00.00 TO STA. 12+00.00 | | CONTRACT NO. 76H13 | | |
| | PLOT DATE = 3/22/2018 | DATE - 3/10/14 | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | |



| | | | | | | | | | | | | |
|---|-----------------------------|---------------|-----------|---|---|---------|--------------|-----------------|------------------|---------------------------|-----------------|--------------|
| FILE NAME = P:\10e2166-26\30RS-5 76H13 US 50\CADD | USER NAME = \$USER\$ | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PLAN FAP ROUTE 327 (US RTE 50) | | | F.A.P. RTE. 327 | SECTION 30RS-5 | COUNTY ST. CLAIR | TOTAL SHEETS 24 | SHEET NO. 14 |
| Default | Sheets\0876H13-sht-plan.dgn | DRAWN - MGM | REVISED - | | SCALE: | SHEET 3 | OF 10 SHEETS | STA. 24+00.00 | TO STA. 36+00.00 | CONTRACT NO. 76H13 | | |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | | | | | | | ILLINOIS FED. AID PROJECT | | |
| | PLOT DATE = 1/29/2018 | DATE - 1/2018 | REVISED - | | | | | | | | | |



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|--|--|----------------------------|--|---|---|----------------------|--------------------------------|---|-------------------|---------------------|-----------------------|--------------------|
| FILE NAME = P:\10e2166-26\30RS-5 76H13 US 50\CADD | USER NAME = \$USER\$ Sheets\0876H13-sht-plan.dgn | DESIGNED - DRAWN - MGM | REVISED - REVISED - REVISED - REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PLAN FAP ROUTE 327 (US RTE 50) | | | F.A.P. RTE. 327 | SECTION 30RS-5 | COUNTY ST. CLAIR | TOTAL SHEETS 24 | SHEET NO. 15 |
| Default | PLOT SCALE = 40.0000' / in. PLOT DATE = 1/29/2018 | CHECKED - DATE - 1/2018 | REVISED - | | SCALE: | SHEET 4 OF 10 SHEETS | STA. 36+00.00 TO STA. 48+00.00 | CONTRACT NO. 76H13 ILLINOIS FED. AID PROJECT | | | | |



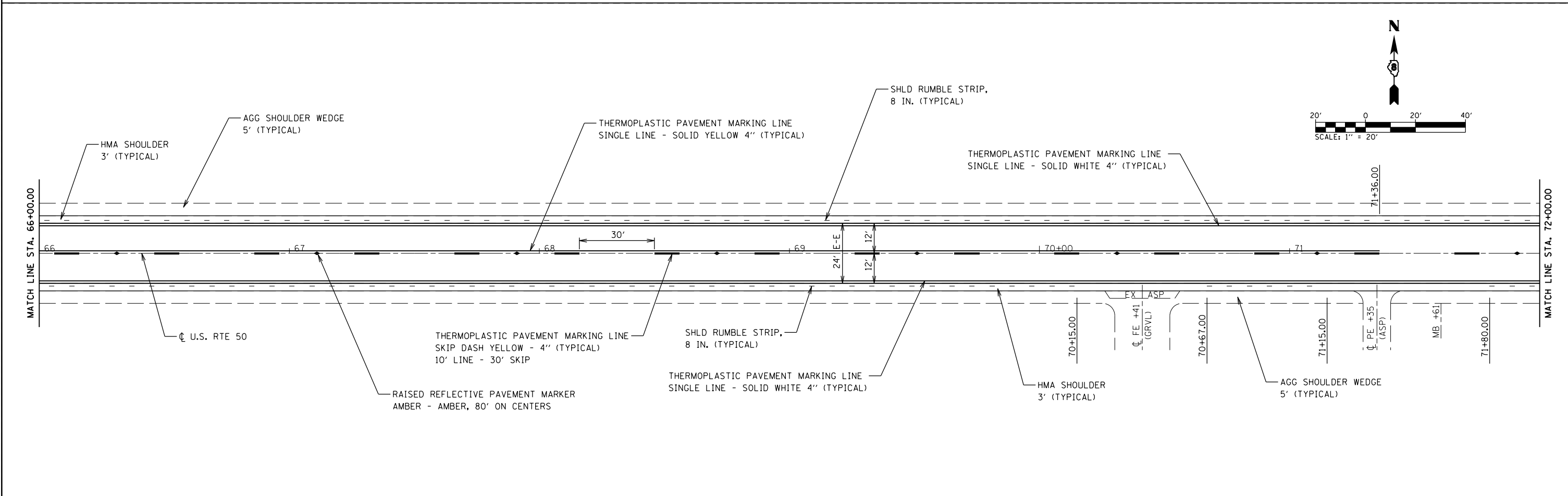
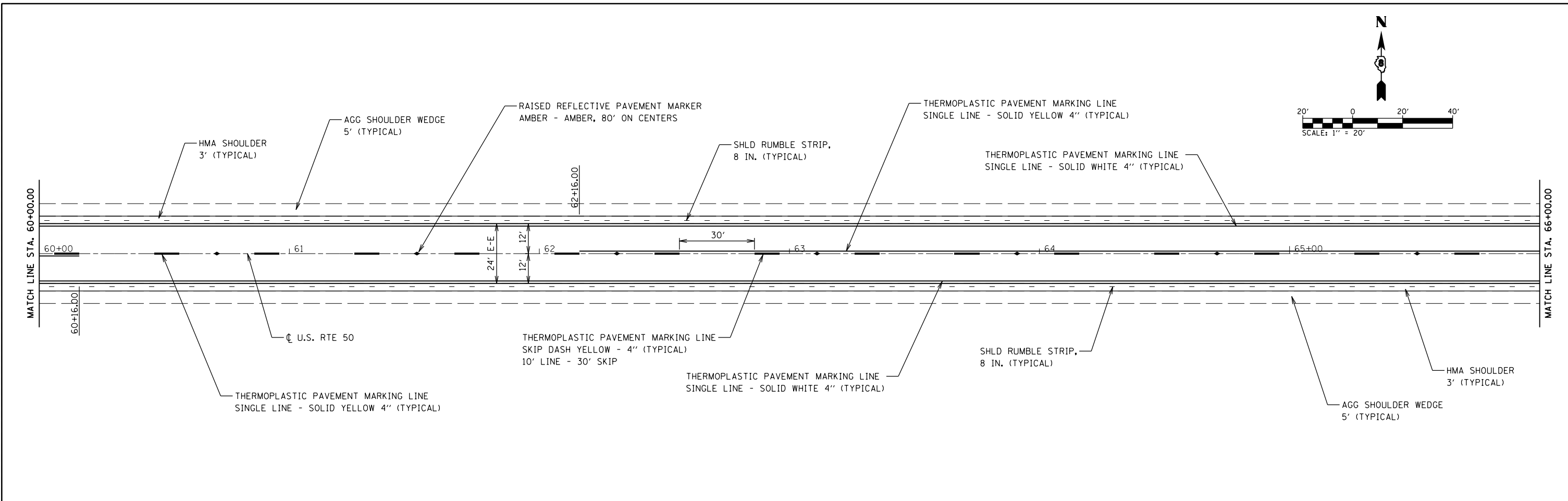
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| FILE NAME = P:\10e2166-26\30RS-5 76H13 US 50\CADD | USER NAME = \$USER\$ | DESIGNED - | REVISED - |
| Sheets\0876H13-sht-plan.dgn | | DRAWN - MGM | REVISED - |
| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - |
| PLOT DATE = 1/29/2018 | | DATE - 1/2018 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

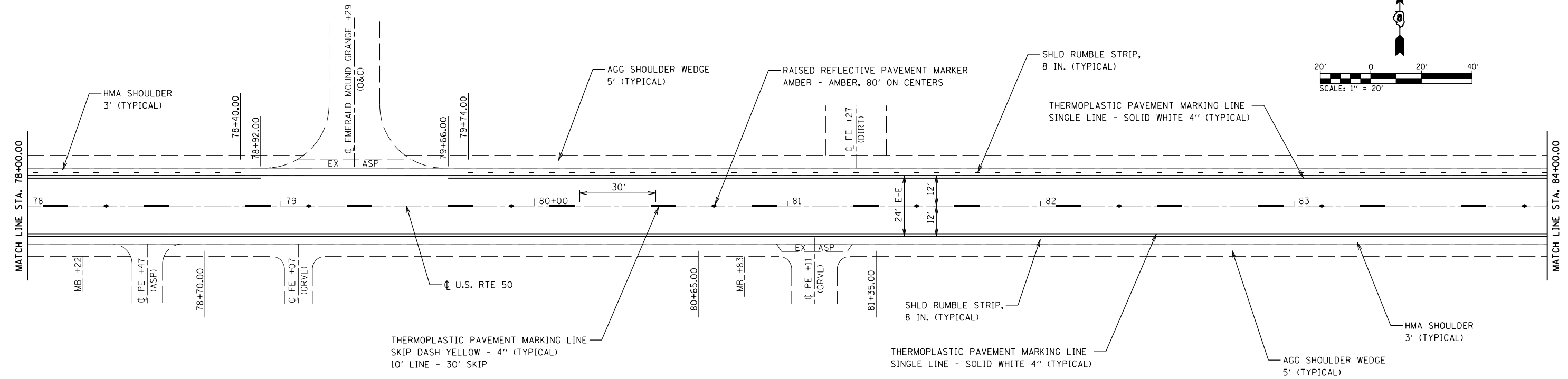
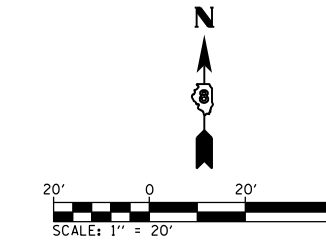
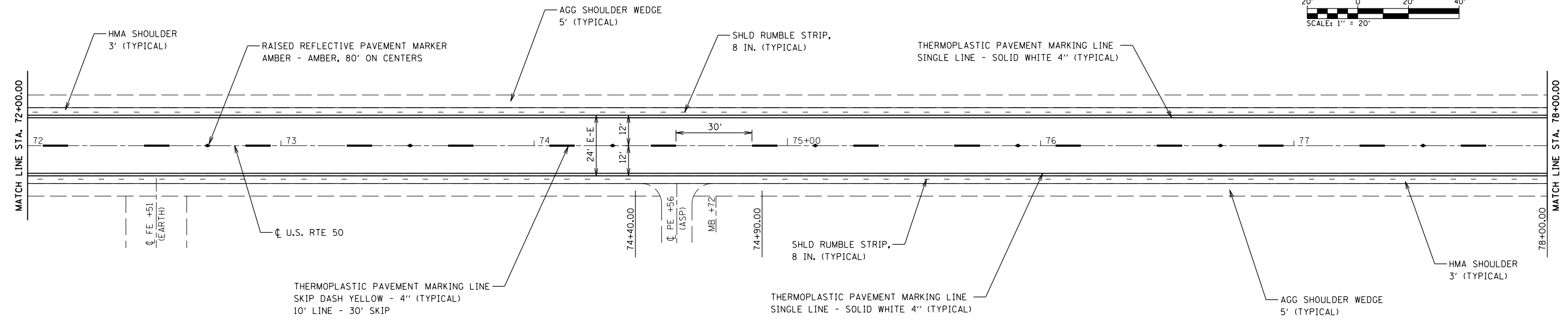
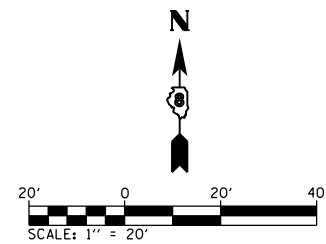
**PLAN
FAP ROUTE 327 (US RTE 50)**

SCALE: SHEET 5 OF 10 SHEETS STA. 48+00.00 TO STA. 60+00.00

| | | | | |
|---------------------------|---------|-----------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 327 | 30RS-5 | ST. CLAIR | 24 | 16 |
| CONTRACT NO. 76H13 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



| | | | | | | | | | | | |
|---|-----------------------------|---------------|-----------|---|---|--|--------------------|------------------|-----------------|--------------|--|
| FILE NAME = P:\10e2166-26\30RS-5 76H13 US 50\CADD Sheets\0876H13-sht-plan.dgn | USER NAME = \$USER\$ | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PLAN FAP ROUTE 327 (US RTE 50) | F.A.P. RTE. 327 | SECTION 30RS-5 | COUNTY ST. CLAIR | TOTAL SHEETS 24 | SHEET NO. 17 | |
| Default | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | | | SCALE: SHEET 6 OF 10 SHEETS STA. 60+00.00 TO STA. 72+00.00 | CONTRACT NO. 76H13 | | | | |
| | PLOT DATE = 1/29/2018 | DATE - 1/2018 | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | | |
| | | | | | | | | | | | |



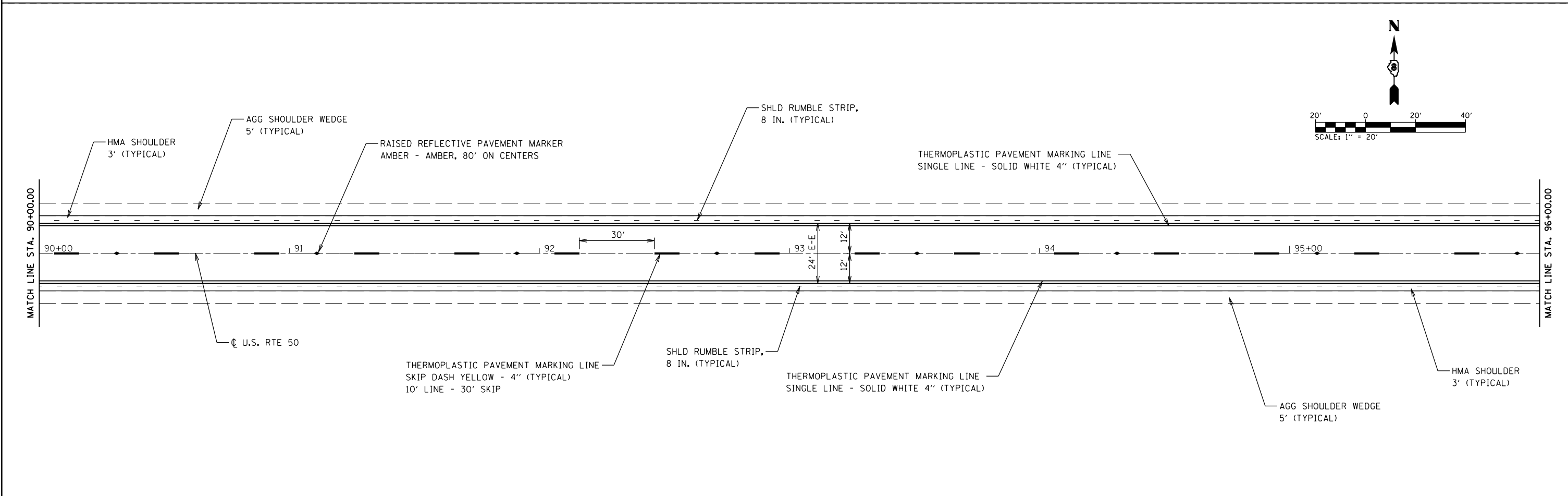
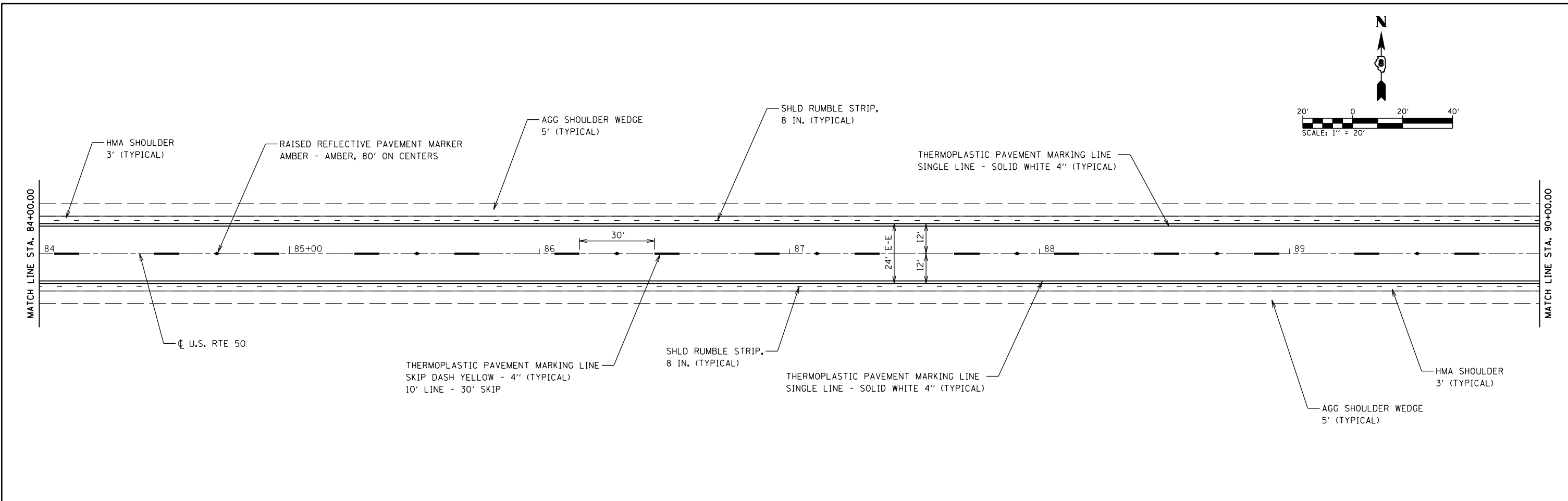
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| FILE NAME = P:\10e2166-26\30RS-5 76H13 US 50\CADD | USER NAME = \$USER\$ | DESIGNED - | REVISED - |
| Sheets\0876H13-sht-plan.dgn | | DRAWN - MGM | REVISED - |
| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - |
| PLOT DATE = 1/29/2018 | | DATE - 1/2018 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

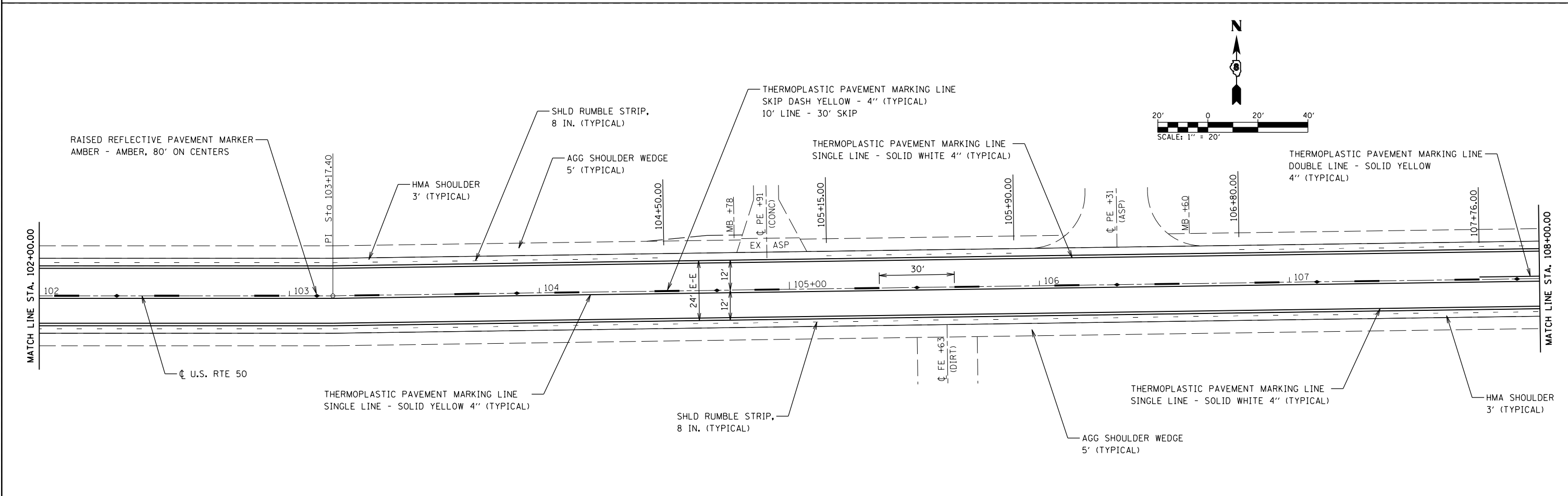
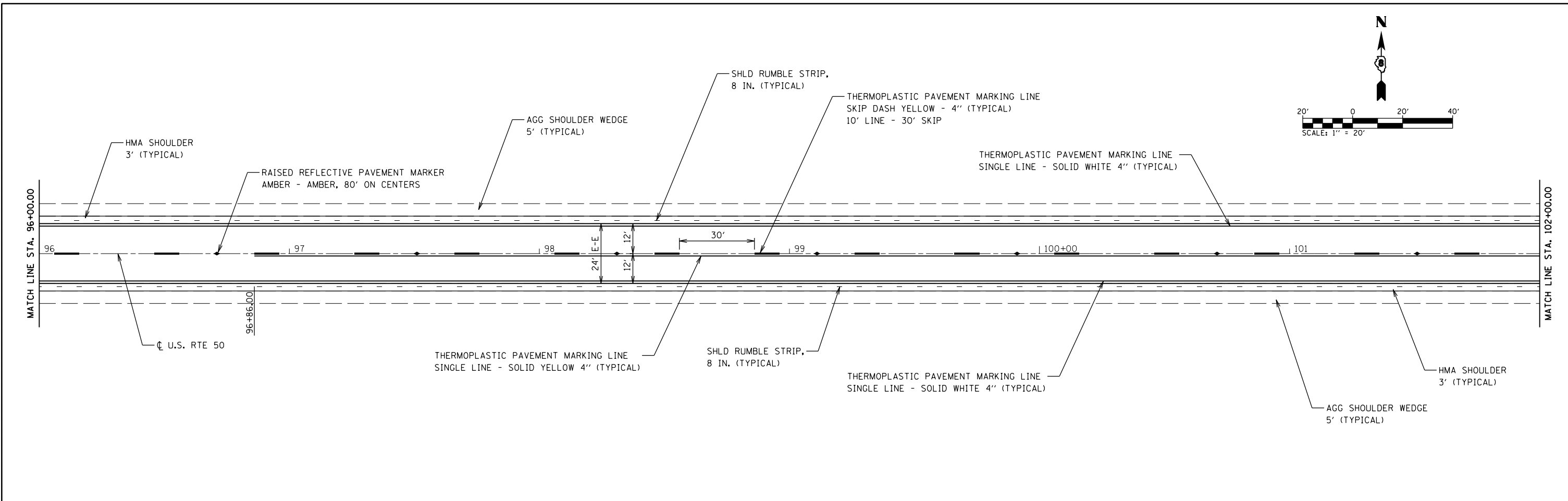
**PLAN
FAP ROUTE 327 (US RTE 50)**

SCALE: SHEET 7 OF 10 SHEETS STA. 72+00.00 TO STA. 84+00.00

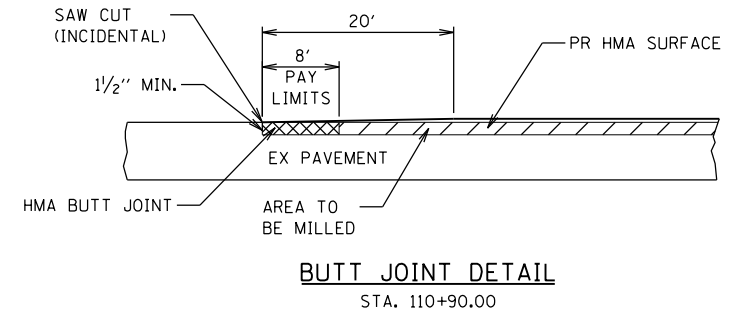
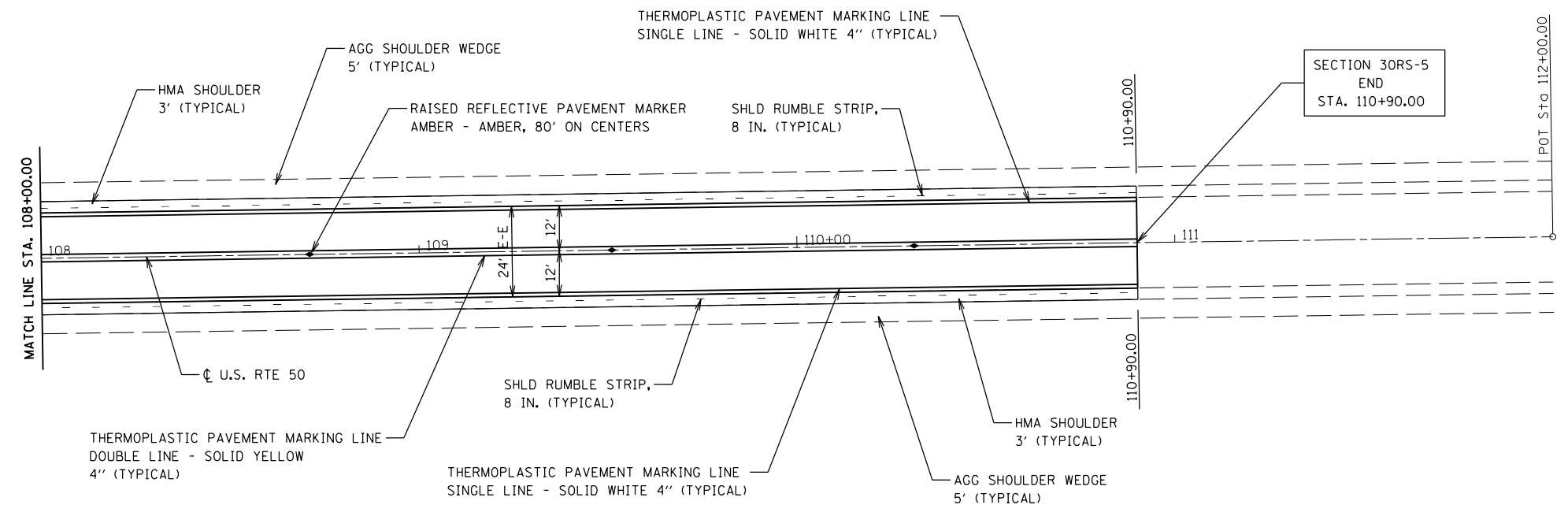
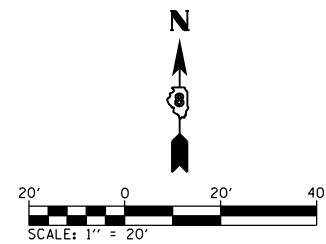
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|---------------------------|---------|-----------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 327 | 30RS-5 | ST. CLAIR | 24 | 18 |
| CONTRACT NO. 76H13 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



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|---|-----------------------------|---------------|-----------|---|---|----------------------|--------------------------------|--------------------|------------------|---------------------------|--------------|--|
| FILE NAME = P:\10e2166-26\30RS-5 76H13 US 50\CADD | USER NAME = \$USER\$ | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PLAN FAP ROUTE 327 (US RTE 50) | | F.A.P. RTE. 327 | SECTION 30RS-5 | COUNTY ST. CLAIR | TOTAL SHEETS 24 | SHEET NO. 19 | |
| Default | Sheets\0876H13-sht-plan.dgn | DRAWN - MGM | REVISED - | | SCALE: | SHEET 8 OF 10 SHEETS | STA. 84+00.00 TO STA. 96+00.00 | CONTRACT NO. 76H13 | | ILLINOIS FED. AID PROJECT | | |
| | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | | | | | | | | | |
| | PLOT DATE = 1/29/2018 | DATE - 1/2018 | REVISED - | | | | | | | | | |



| | | | | | | | | | | | |
|---------------------------------------|-----------------------------|---------------|-----------|---|---|---------------------------|---------|-----------|--------------|-----------|--|
| FILE NAME = | USER NAME = \$USER\$ | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PLAN FAP ROUTE 327 (US RTE 50) | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| P:\10e2166-26\30RS-5 76H13 US 50\CADD | Sheets\0876H13-sht-plan.dgn | DRAWN - MGM | REVISED - | | | 327 | 30RS-5 | ST. CLAIR | 24 | 20 | |
| Default | PLOT SCALE = 40.0000' / in. | CHECKED - | REVISED - | | | CONTRACT NO. 76H13 | | | | | |
| | PLOT DATE = 1/29/2018 | DATE - 1/2018 | REVISED - | | | ILLINOIS FED. AID PROJECT | | | | | |

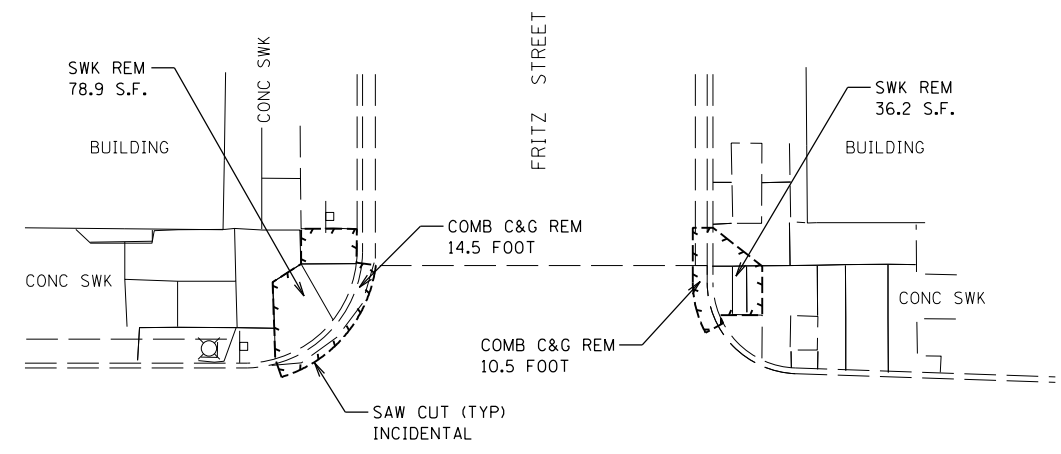
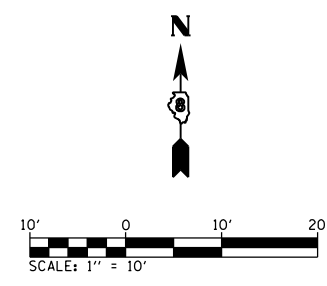


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|---|----------------------|---------------|-----------|
| FILE NAME = P:\10e2166-26\30RS-5 76H13 US 50\CADD | USER NAME = \$USER\$ | DESIGNED - | REVISED - |
| Sheets\0876H13-sht-plan.dgn | | DRAWN - MGM | REVISED - |
| PLOT SCALE = 40.0000' / in. | | CHECKED - | REVISED - |
| PLOT DATE = 1/29/2018 | | DATE - 1/2018 | REVISED - |

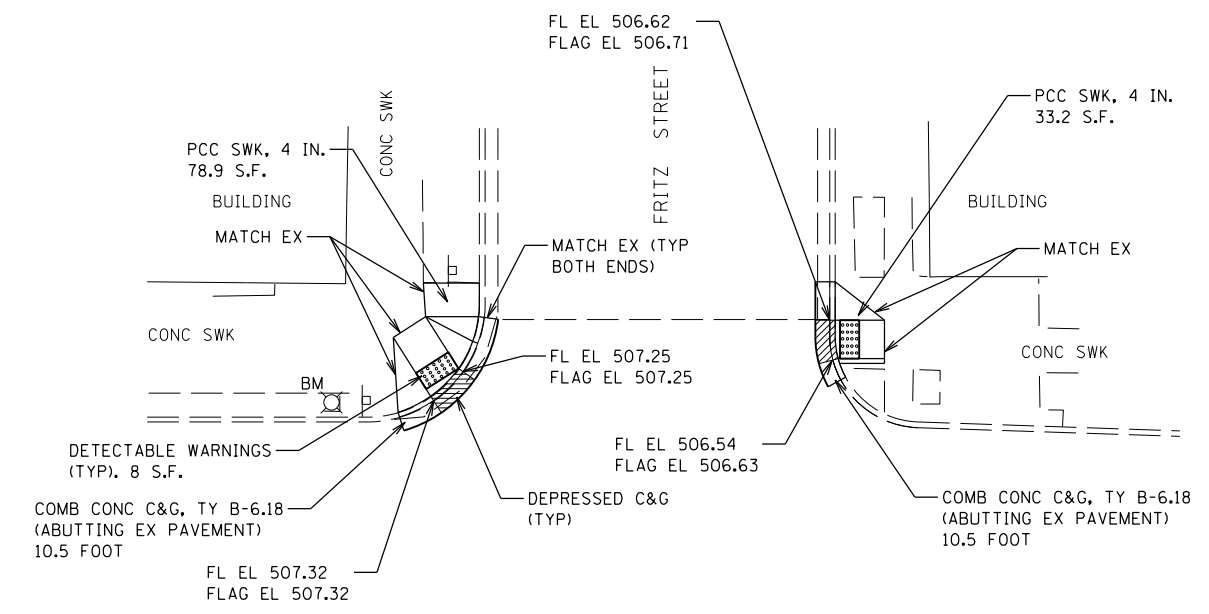
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | |
|----------------------------------|--|
| PLAN | |
| FAP ROUTE 327 (US RTE 50) | |
| SCALE: | SHEET 10 OF 10 SHEETS STA. 108+00.00 TO STA. 112+00.00 |

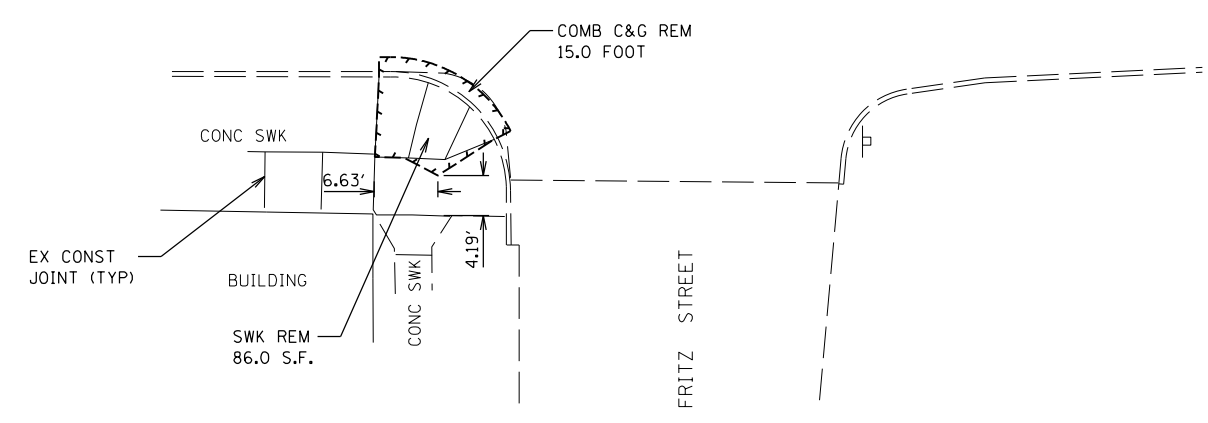
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|---------------------------|---------|-----------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 327 | 30RS-5 | ST. CLAIR | 24 | 21 |
| CONTRACT NO. 76H13 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



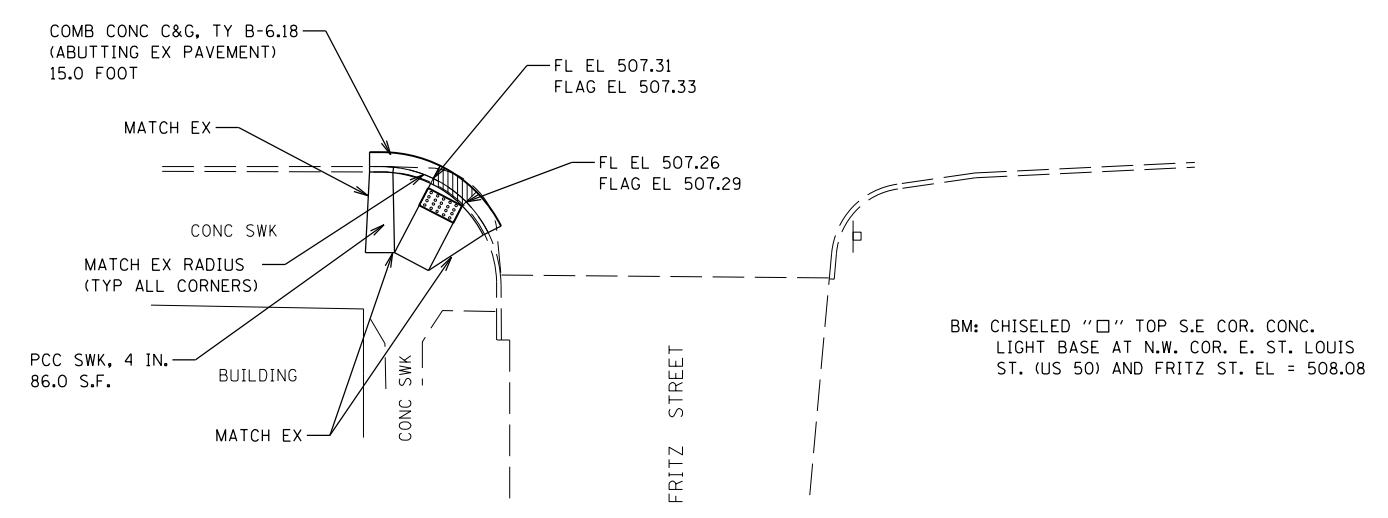
FAP ROUTE 327 (US RTE 50)



FAP ROUTE 327 (US RTE 50)



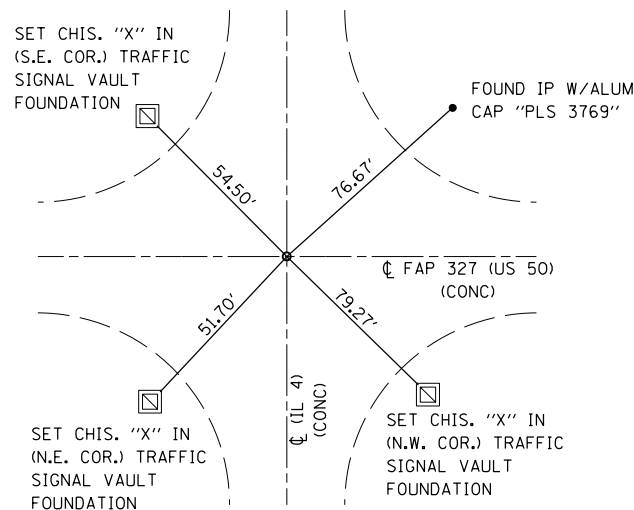
EXISTING ADA RAMP IMPROVEMENT REMOVAL PLAN



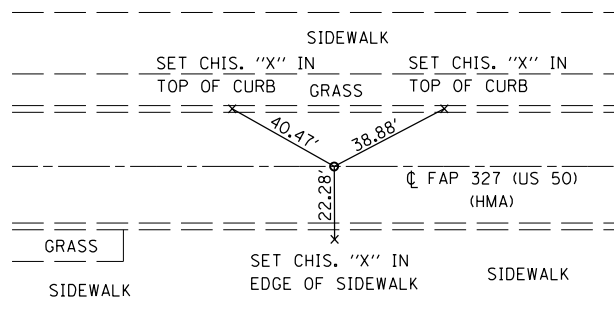
PROPOSED ADA RAMP IMPROVEMENT PLAN

| | | | | | | | | | | | |
|---|-----------------------------|---------------|-----------|---|---|--------------------|---------------------|-----------|--------------|---------------------------|--|
| FILE NAME = | USER NAME = \$USER\$ | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | ADA SIDEWALK PLAN (FRITZ STREET) FAP ROUTE 327 (US RTE 50) | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| P:\10e2166-26\30RS-5 76H13 US 50\CADD Sheets\0876H13-sht-ada-improvements.dgn | | DRAWN - MGM | REVISED - | | | 327 | 30RS-5 | ST. CLAIR | 24 | 22 | |
| Default | PLOT SCALE = 20.0000' / in. | CHECKED - | REVISED - | | | CONTRACT NO. 76H13 | | | | | |
| | PLOT DATE = 1/29/2018 | DATE - 1/2018 | REVISED - | | | SCALE: | SHEET 1 OF 1 SHEETS | STA. | TO STA. | ILLINOIS FED. AID PROJECT | |

GPS POINT #1
N=705,579.135
E=2,399,186.595



GPS POINT #2
N=705,576.140
E=2,399,460.946

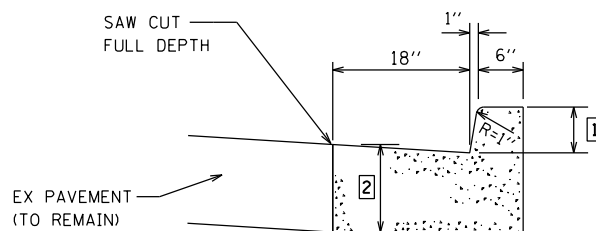


NOTE:
ALL TIES PULLED DIRECT

FOUND ALUMINUM DISK "IDOT", POINT #1

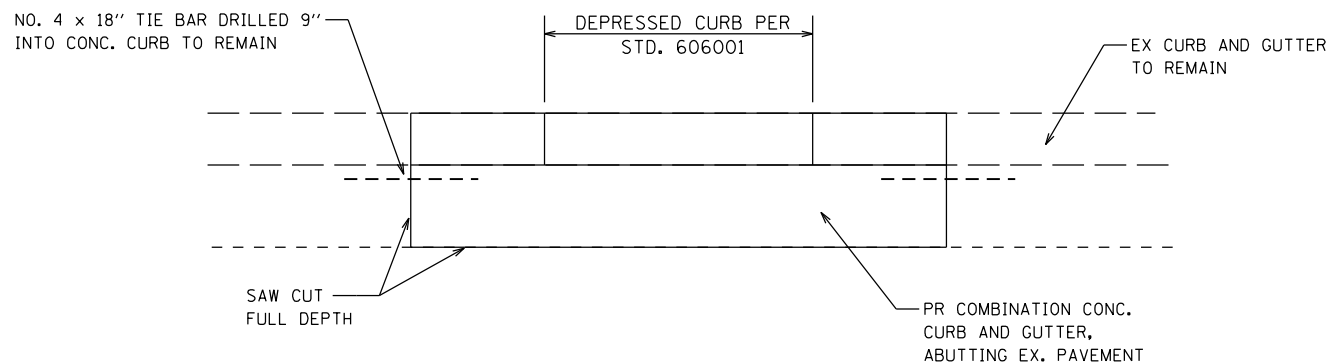
FOUND ALUMINUM DISK "IDOT", POINT #2

SURVEY MARKER MONUMENT TIES
NOT TO SCALE

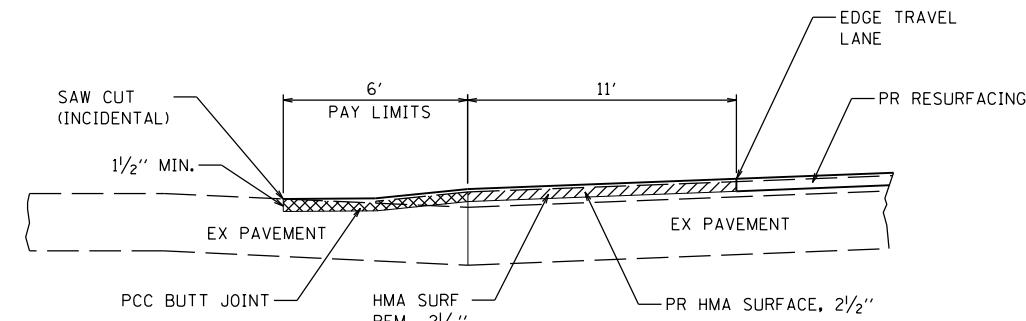


NOTE:
1 MATCH HEIGHT OF EX CURB AND GUTTER.
2 THICKNESS VARIES. MATCH EX. PAVEMENT (TO REMAIN). MINIMUM THICKNESS OF 9".

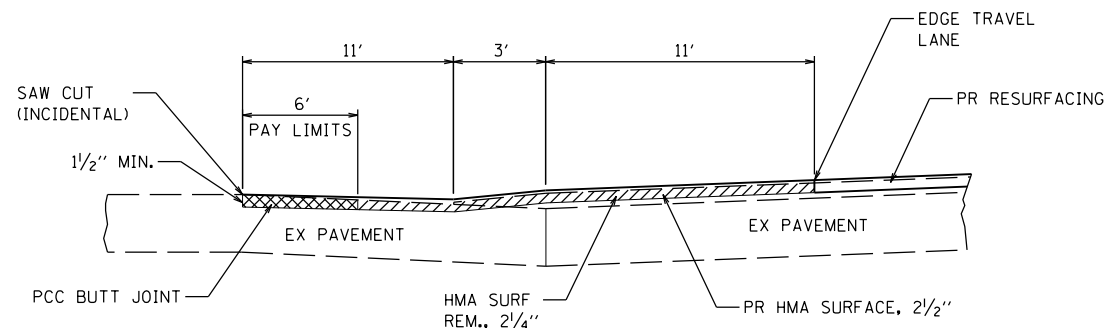
TYPICAL COMBINATION CURB AND GUTTER SECTION
NOT TO SCALE



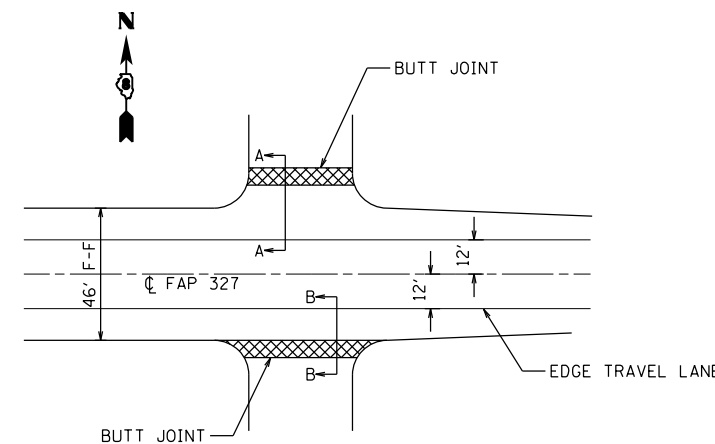
PLAN COMBINATION CURB AND GUTTER
NOT TO SCALE



SECTION B-B

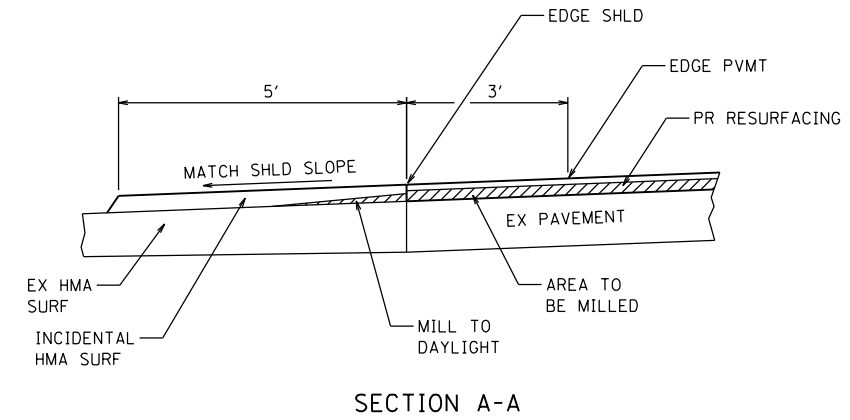
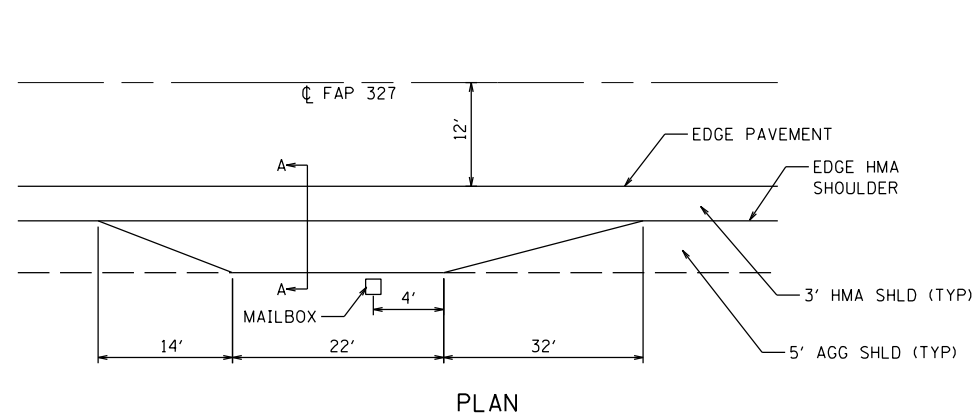


SECTION A-A

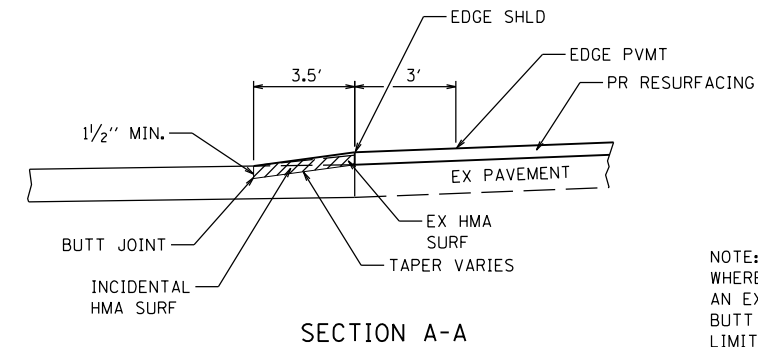
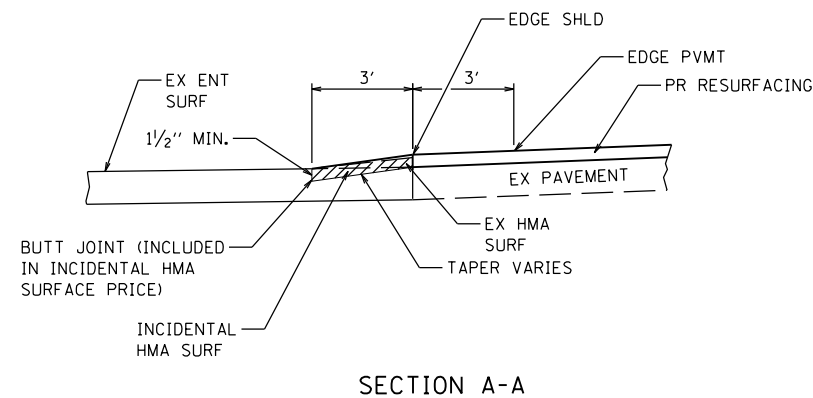


BUTT JOINT DETAIL FRITZ STREET
NOT TO SCALE

| | | | | | | | | | | | |
|---|--------------------------------------|---------------|-----------|---|---|-----------------|---------------------|------------------|---------------------------|--------------|--|
| FILE NAME = P:\10e2166-26\30RS-5 76H13 US 50\CADD | USER NAME = \$USER\$ | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | CONSTRUCTION DETAILS FAP ROUTE 327 (US RTE 50) | F.A.P. RTE. 327 | SECTION 30RS-5 | COUNTY ST. CLAIR | TOTAL SHEETS 24 | SHEET NO. 23 | |
| Default | Sheets\0876H13-sht-const-details.dgn | DRAWN - MGM | REVISED - | | | SCALE: | SHEET 1 OF 2 SHEETS | TO STA. | ILLINOIS FED. AID PROJECT | | |
| | PLOT SCALE = 100.0000' / 1" | CHECKED - | REVISED - | | | | | | CONTRACT NO. 76H13 | | |
| | PLOT DATE = 1/29/2018 | DATE - 1/2018 | REVISED - | | | | | | | | |

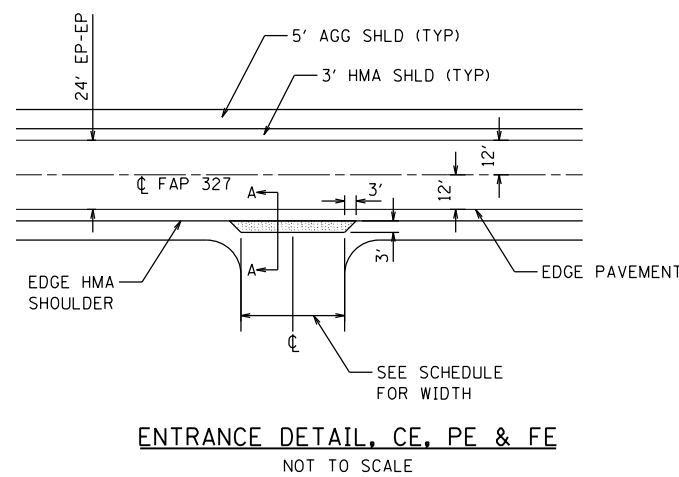


MAILBOX TURNOUT DETAIL
STA. 13+11.00, LT SIDE

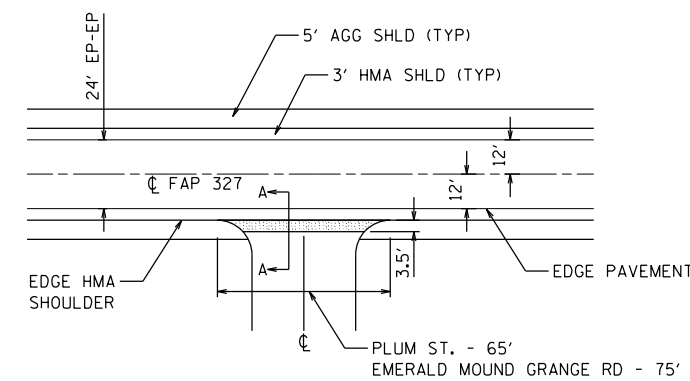


NOTE:
WHERE THE HMA TRANSITION IS MATCHING INTO AN EXISTING HMA SIDE ROAD SURFACE, A MILLED BUTT JOINT SHALL BE CONSTRUCTED WITHIN THE LIMIT OF THE TOTAL TRANSITION LENGTH ON THE LOCAL ROUTE.

| EOP ELEVATION CHANGE | TOTAL TRANSITION LENGTH |
|----------------------|-------------------------|
| 3/4" | 3.5' |
| 1" | 5' |
| 1-1/2" | 7.5' |
| 2" | 10' |
| 2-1/2" | 12.5' |
| 3" | 15' |
| 3-1/2" | 17.5' |
| 4" | 20' |
| 4-1/2" | 22.5' |
| 5" | 25' |
| 5-1/2" | 27.5' |
| 6" | 30' |



ENTRANCE DETAIL, CE, PE & FE
NOT TO SCALE



SIDE ROAD DETAIL
NOT TO SCALE

| | | | |
|---|-----------------------------|---------------|-----------|
| FILE NAME = P:\10e2166-26\30RS-5 76H13 US 50\CADD | USER NAME = \$USER\$ | DESIGNED - | REVISED - |
| Sheets\0876H13-sht-const-details.dgn | | DRAWN - MGM | REVISED - |
| | PLOT SCALE = 100.0000' / 1" | CHECKED - | REVISED - |
| Default | PLOT DATE = 1/29/2018 | DATE - 1/2018 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS
FAP ROUTE 327 (US RTE 50)

SCALE: SHEET 2 OF 2 SHEETS STA. TO STA.

| | | | | |
|--------------------|----------------|------------------|---------------------------|--------------|
| F.A.P. RTE. 327 | SECTION 30RS-5 | COUNTY ST. CLAIR | TOTAL SHEETS 24 | SHEET NO. 24 |
| CONTRACT NO. 76H13 | | | ILLINOIS FED. AID PROJECT | |