04-27-2018 LETTING ITEM 054

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
592	117-B-R-1	MADISON	19	1
	ILLINOIS	CONTRACT NO. 76L37		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROPOSED
HIGHWAY PLANS

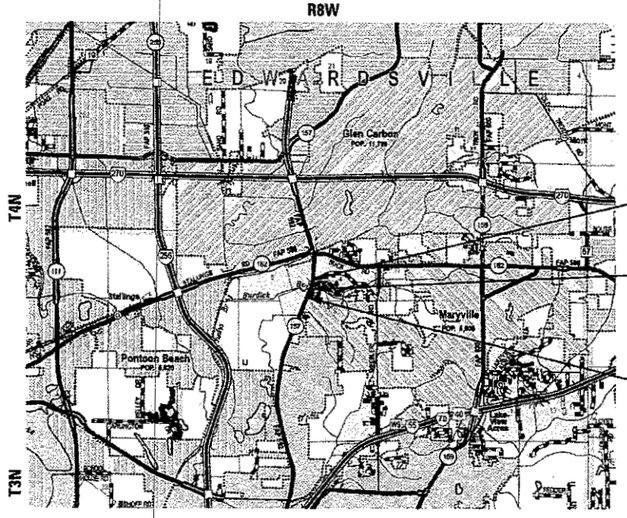
FAP ROUTE 592 (IL 157)
SECTION 117-B-R-1

BRIDGE DECK REPAIR
MADISON COUNTY

C-98-191-18

D-98-048-18

LOCATION OF SECTION INDICATED THUS: –



LOCATION MAP

0 0.5 1 2 3

1 IN. = 1 MILE

GROSS LENGTH = 405 FT. = 0.077 MILE

NET LENGTH = 405 FT. = 0.077 MILE

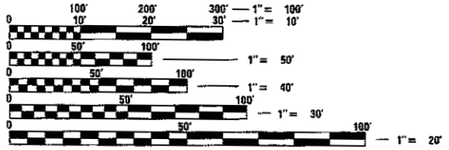
END PROJECT
STA. 559+23

STATION 557+19.85, IL RTE. 157
STRUCTURE OVER BURDICK BRANCH
S.N. 060-0088

BEGIN PROJECT
STA. 555+18

INDEX OF SHEETS

FUNCTIONAL CLASSIFICATION:	OTHER PRINCIPAL ARTERIAL
2017 ADT:	6400 (ACTUAL)
2018 ADT:	6500 (EST)
2038 ADT:	7900 (EST)
MU%	2.5%
SU%	7.0%



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: TIM PADGETT (618) 346-3325
PROJECT MANAGER: DIANA MURRAY (618) 346-3207

CONTRACT NO. 76L37

LICENSED PROFESSIONAL ENGINEER
CINDY TRAVIS MUELLER
062-046699
OF ILLINOIS

Cindy Travis Mueller

CINDY TRAVIS MUELLER, P.E.
LICENSED PROFESSIONAL ENGINEER
ILLINOIS NO. 062-046699
EXPIRES: 11-30-2019
DATE 2/5/18

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED Feb 8 20 18
Jeffrey E. Kern
REGIONAL ENGINEER
Mar 23 20 18
ENGINEER OF DESIGN AND ENVIRONMENT
Mar 23 20 18
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

GENERAL NOTES

1. ILLINOIS STATE LAW REQUIRES A 48-HOUR NOTICE BE GIVEN TO ALL UTILITIES BEFORE DIGGING. FIELD MARKING OF FACILITIES MAY BE OBTAINED BY CONTACTING J.U.L.I.E. OR FOR NON-MEMBERS, THE UTILITY COMPANY DIRECTLY. AGENCIES KNOWN TO HAVE FACILITIES WITHIN THE PROJECT AREA ARE AS FOLLOWS:

	ABOVE GROUND	BELOW GROUND
•AMEREN ILLINOIS, GAS AND ELECTRIC	X	X
•AT&T CORPORATION, COMMUNICATIONS	X	X
•CHARTER COMMUNICATIONS	X	X
•VILLAGE OF MARYVILLE, WATER & SEWER		X
•SOUTHWESTERN ELECTRIC COOPERATIVE	X	X
•ENABLE MIDSTREAM PARTNERS		X

   MEMBERS OF J.U.L.I.E. (800)-892-0123 OR 811 ARE INDICATED BY •. NON J.U.L.I.E. MEMBERS MUST BE NOTIFIED INDIVIDUALLY.

2. THE CONTRACTOR AND ENGINEER SHALL BE AWARE THAT NO SURVEY WAS PERFORMED FOR THIS PROJECT. THE STATIONING AND TOPOGRAPHY SHOWN IN THE PLANS WERE CREATED USING RECORD PLANS AND FIELD MEASUREMENTS MADE BY DESIGN PERSONNEL. BOTH SHALL BE ASSUMED TO BE APPROXIMATE.

3. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

4. THE THICKNESS OF THE BITUMINOUS MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED, SUCH AS ON THE NORTH APPROACH.

5. THE PROPOSED PAVEMENT MARKING SHALL MATCH THE LOCATIONS OF THE EXISTING PAVEMENT MARKING, AS DIRECTED BY THE ENGINEER.

6. THE HMA OVERLAY ON THE BRIDGE APPROACH SHOULDER PAVEMENT WILL BE HAND GRADED TO PROMOTE DRAINAGE INTO THE EXISTING INLET GRATES AT NO ADDITIONAL COST TO THE CONTRACT.

7. GUARDRAIL REFLECTORS TYPE A HAVE BEEN ADDED TO THE PROJECT TO REPLACE ANY MISSING REFLECTORS.

HIGHWAY STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
642006	SHOULDER RUMBLE STRIPS, 8 INCH
701201-04	LANE CLOSURE, 2 LANE, 2 WAY, DAY ONLY, FOR SPEEDS ≥45 MPH
701301-04	LANE CLOSURE, 2 LANE, 2 WAY, SHORT TIME OPERATIONS
701306-04	LANE CLOSURE, 2 LANE, 2 WAY, SLOW MOVING OPERATIONS, DAY ONLY, FOR SPEEDS ≥45 MPH
701311-03	LANE CLOSURE, 2 LANE, 2 WAY, MOVING OPERATIONS, DAY ONLY
701321-17	LANE CLOSURE, 2 LANE, 2 WAY, BRIDGE REPAIR WITH BARRIER
701326-04	LANE CLOSURE, 2 LANE, 2 WAY, PAVEMENT WIDENING FOR SPEEDS ≥45 MPH
701901-07	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
780001-05	TYPICAL PAVEMENT MARKINGS
781001-04	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
782006	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

COMMITMENTS

PASTOR ROGER WRIGHT OF THE BLUFFVIEW BAPTIST CHURCH• MUST BE NOTIFIED BY THE RESIDENT ENGINEER TWO WEEKS PRIOR TO THE TEMPORARY CLOSURE OF THE ENTRANCE ON IL 157.

•618-288-9832

THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT:

MIXTURE USE	POLY SURFACE	BASE CSE WIDE
AC/PG	SBS PG 76-22	PG 64-22
RAP %(MAX)	SEE SPECIAL PROVISION	SEE SPECIAL PROVISION
DESIGN AIR VOIDS	4.0% @ Ndes=70	4.0% @ Ndes=70
MIX COMPOSITION (GRADATION)	IL 9.5	IL 19.0
FRICTION AGG	MIXTURE "D"	MIXTURE "B"
QUALITY MGMT PROGRAM	QC/QA	QC/QA

MIXTURE USE	SHOULDERS ≥ 2.25"	SHOULDERS < 2.25"
AC/PG	PG 64-22	PG 64-22
RAP %(MAX)	SEE SPECIAL PROVISION	SEE SPECIAL PROVISION
DESIGN AIR VOIDS	4.0% @ Ndes=30	4.0% @ Ndes=30
MIX COMPOSITION (GRADATION)	IL 19.0L	IL 9.5L
FRICTION AGG		
QUALITY MGMT PROGRAM	QC/QA	QC/QA

PLAN QUANTITIES FOR BITUMINOUS CONCRETE SURFACE COURSE ITEMS ARE CALCULATED USING A UNIT WEIGHT OF 112 LBS/SQ YD/IN (59.8 KG/SQ M/25 MM THICKNESS)

PRELIMINARY – NOT FOR CONSTRUCTION

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES, STANDARDS & COMMITMENTS

SCALE: N/A | SHEET 1 OF 1 SHEETS | STA. N/A | TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
592	117-B-R-1	MADISON	19	2
		CONTRACT NO. 76L37		
	ILLINOIS	FED. AID PROJECT		

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE BRIDGE 0047 URBAN
20200600	EXCAVATING AND GRADING EXISTING SHOULDER	UNIT	4	4
35600716	HOT-MIX ASPHALT BASE COURSE WIDENING, 10"	SQ YD	315	315
40600295	POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)	POUND	470	470
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	240	240
40603540	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	99	99
44004250	PAVED SHOULDER REMOVAL	SQ YD	64	64
50102400	CONCRETE REMOVAL	CU YD	9.0	9.0
50300255	CONCRETE SUPERSTRUCTURE	CU YD	9.7	9.7
50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	2550	2550
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	1160	1160
50800515	BAR SPLICERS	EACH	12	12
52000110	PREFORMED JOINT STRIP SEAL	FOOT	58.5	58.5
58100200	WATERPROOFING MEMBRANE SYSTEM	SQ YD	580	580
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	5	5

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SUMMARY OF QUANTITIES

SCALE: N/A | SHEET 1 OF 3 SHEETS | STA. N/A TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
592	117-B-R-1	MADISON	19	3

CONTRACT NO. 76L37

ILLINOIS FED. AID PROJECT

				CONSTR. CODE
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	BRIDGE 0047 URBAN
67100100	MOBILIZATION	L SUM	1	1
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1
70100500	TRAFFIC CONTROL AND PROTECTION, STANDARD 701326	L SUM	1	1
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DAY	90	90
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1	1
70106700	TEMPORARY RUMBLE STRIPS	EACH	12	12
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	48	48
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	2239	2239
70400100	TEMPORARY CONCRETE BARRIER	FOOT	382	382
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	362	362
70600260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	2	2
70600332	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	2	2
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	2021	2021

* SPECIALTY ITEM

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: N/A | SHEET 2 OF 3 SHEETS | STA. N/A | TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
592	117-B-R-1	MADISON	19	4
		CONTRACT NO. 76L37		
	ILLINOIS FED. AID PROJECT			

	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE BRIDGE 0047 URBAN
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	9	9
*	78200005	GUARDRAIL REFLECTORS, TYPE A	EACH	16	16
*	78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	45	45
	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	9	9
	X0327979	PAVEMENT MARKING REMOVAL - GRINDING	SQ FT	390	390
	X1200030	FILLING INLETS, TEMPORARY	EACH	4	4
	X5870015	BRIDGE DECK CONCRETE SEALER	SQ FT	1030	1030
	X7010202	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321 (SPECIAL)	EACH	1	1
	X7015005	CHANGEABLE MESSAGE SIGN	CAL DAY	90	90
	X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SQ FT	747	747
	X7040125	PINNING TEMPORARY CONCRETE BARRIER	EACH	12	12
	Z0001903	STRUCTURAL STEEL REMOVAL	POUND	1950	1950
	Z0016200	DECK SLAB REPAIR (PARTIAL)	SQ YD	50	50
	Z0033700	LONGITUDINAL JOINT SEALANT	FOOT	261	261

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* SPECIALTY ITEM

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PLOT DATE = 1/25/2018	DATE	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

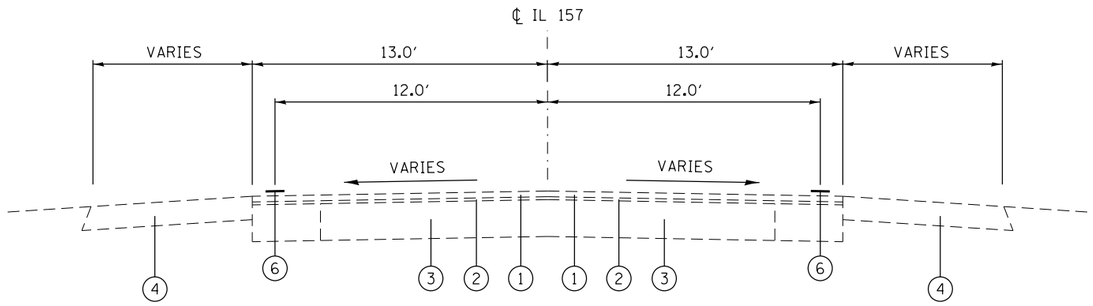
SUMMARY OF QUANTITIES

SCALE: N/A | SHEET 3 OF 3 SHEETS | STA. N/A | TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
592	117-B-R-1	MADISON	19	5
		CONTRACT NO. 76137		
		ILLINOIS FED. AID PROJECT		

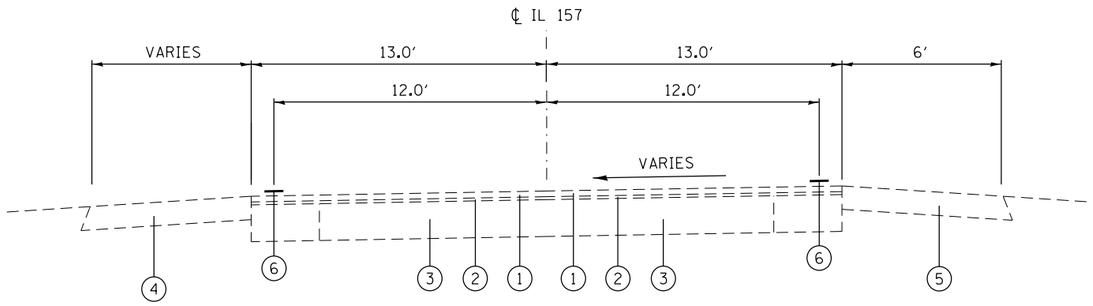
LEGEND

1. EXISTING SURFACE COURSE
2. EXISTING LEVELING BINDER
3. EXISTING PAVEMENT
4. EXISTING AGGREGATE SHOULDER
5. EXISTING PAVED SHOULDER
6. EXISTING PAVEMENT MARKING LINE, 4''
7. PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, 2''
8. PROPOSED POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)
9. PROPOSED HOT-MIX ASPHALT BASE COURSE WIDENING, 10''
10. PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4''
11. LONGITUDINAL JOINT SEALANT



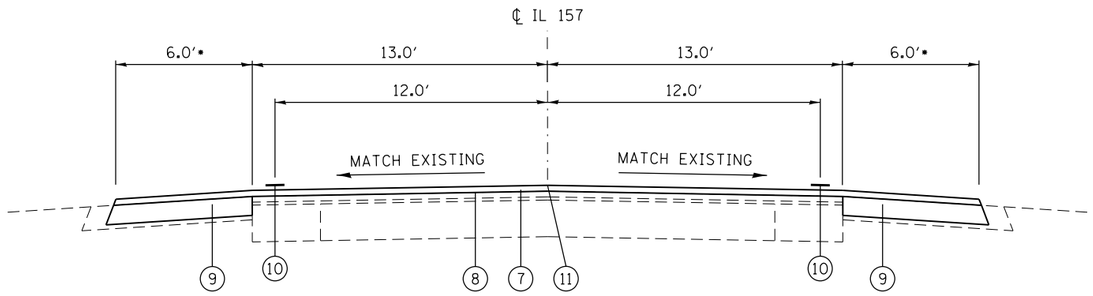
EXISTING TYPICAL SECTION

STA. 506+53 TO 556+58



EXISTING TYPICAL SECTION

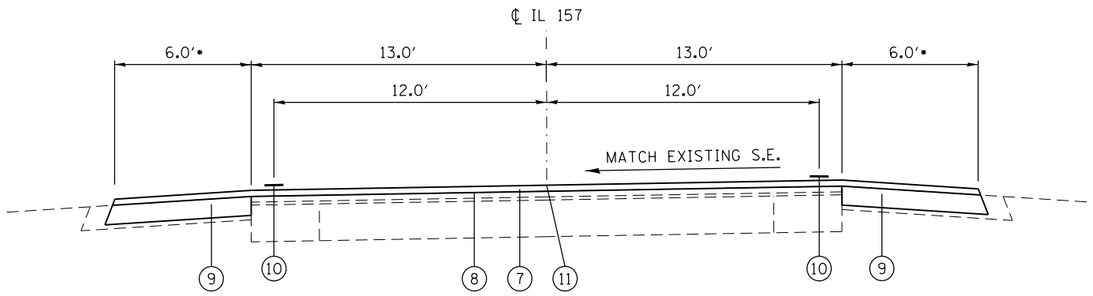
STA. 557+81 TO 567+39



PROPOSED TYPICAL SECTION

STA. 555+89.32 TO 556+58.69

•SHOULDER WIDENING FROM:
STA. 555+18 RT TO 556+58 RT
STA. 555+35 LT TO 556+31 LT



PROPOSED TYPICAL SECTION

STA. 557+81.01 TO 558+50.39

•SHOULDER WIDENING FROM:
STA. 558+08 RT TO 559+04 RT
STA. 557+83 LT TO 559+23 LT

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

SCALE: N/A | SHEET 1 OF 1 SHEETS | STA. N/A | TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
592	117-B-R-1	MADISON	19	6
		CONTRACT NO. 76L37		
	ILLINOIS FED. AID PROJECT			

STAGING SCHEDULE

STAGE	TEMPORARY CONCRETE BARRIER	RELOCATE TEMPORARY CONCRETE BARRIER	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW) TEST LEVEL 3	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW) TEST LEVEL 3	PAVEMENT MARKING TAPE, TYPE III 4"	TEMPORARY PAVEMENT MARKING LINE 24"	PAVEMENT MARKING REMOVAL - GRINDING	TEMPORARY PAVEMENT MARKING REMOVAL	HOT-MIX ASPHALT BASE COURSE WIDENING, 10"	EXCAVATING AND GRADING EXISTING SHOULDER	PAVED SHOULDER REMOVAL	TEMPORARY RUMBLE STRIPS
	(FOOT)	(FOOT)	(EACH)	(EACH)	(FOOT)	(FOOT)	(SQ FT)	(SQ FT)	(SQ YD)	(UNIT)	(SQ YD)	(EACH)
I	382	0	2		1016	24	164	339	157.3	2.5	0	6
II	0	362		2	1223	24	302	408	157.3	1.5	64	6
TOTAL	382	362	2	2	2239	48	466	747	314.6	4.0	64	12

PAVEMENT MARKING SCHEDULE

STATION	TO	STATION	NO PASSING DIRECTION NB/SB	THERMOPLASTIC PAVEMENT MARKING - LINE 4"			RAISED REFLECTIVE PAVEMENT MARKERS	
				WHITE	YELLOW	YELLOW		
				SOLID	SKIP	SOLID	AMBER	REMOVAL
				(FOOT)			(EACH)	
555+18 RT		559+04 RT		386				
555+35 LT		559+23 LT		388				
553+58		559+10	NB			552		
559+10		560+83	NB		50		3	3
553+58		557+81	SB		110		6	6
557+81		560+83	SB			302		
TOTAL				774	160	854	9	9

RESURFACING SCHEDULE

LOCATION	POLYMERIZED BITUMINOUS MATERIALS (TACK COAT) 0.5LB/SF	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	ROADWAY	BRIDGE
			POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	
	(POUND)	(SQ YD)	(TON)	(TON)
STAGE I	235	120	29	20.5
STAGE II	235	120	29	20.5
TOTAL	470	240	99	

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

SCALE: N/A | SHEET 1 OF 1 SHEETS | STA. N/A | TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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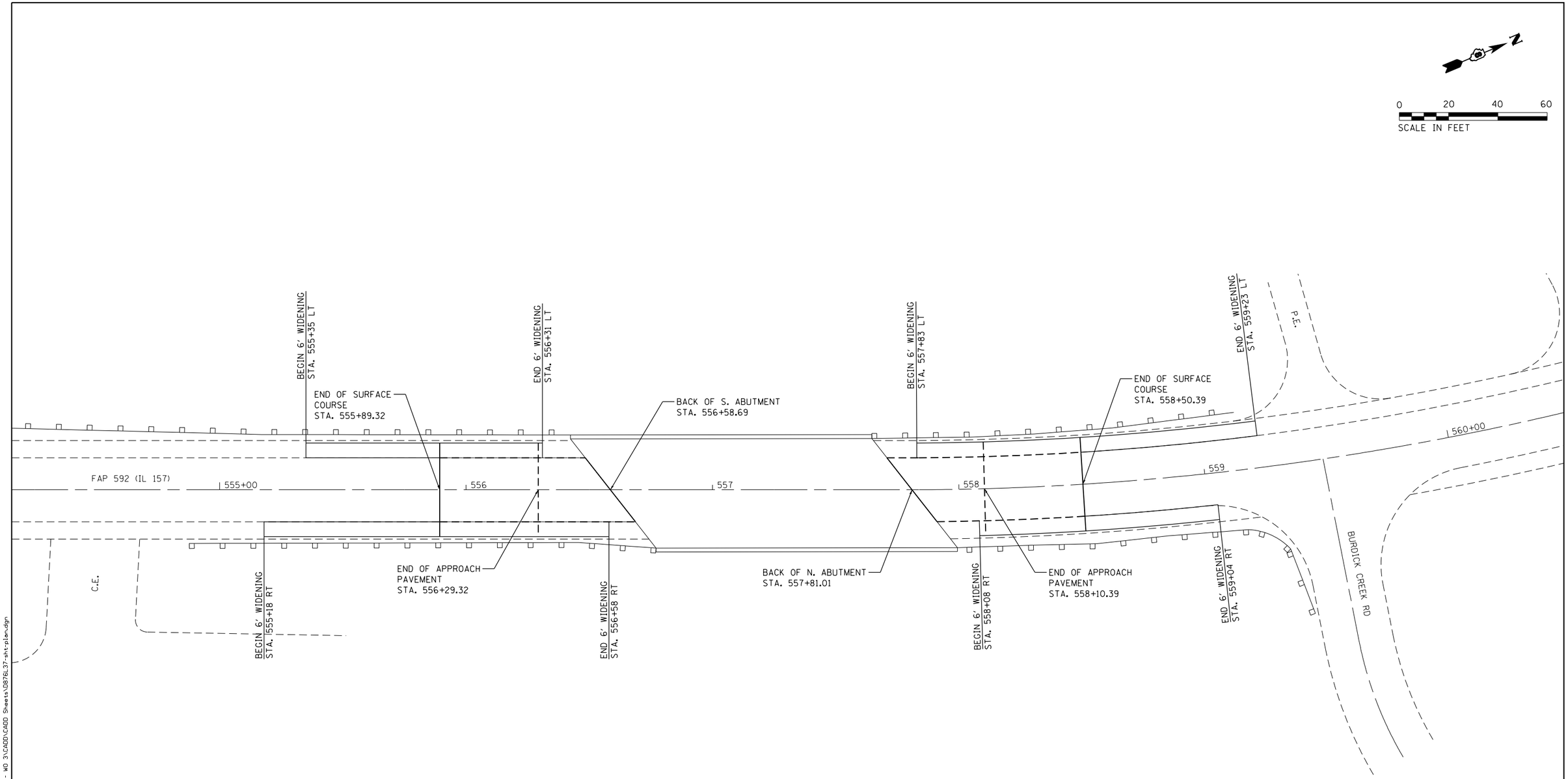
		CONTRACT NO. 76L37		
	ILLINOIS	FED. AID PROJECT		



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PLOT DATE = 1/25/2018	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN

SCALE: 1" = 20' | SHEET 1 OF 1 SHEETS | STA. 555+18 TO STA. 559+23

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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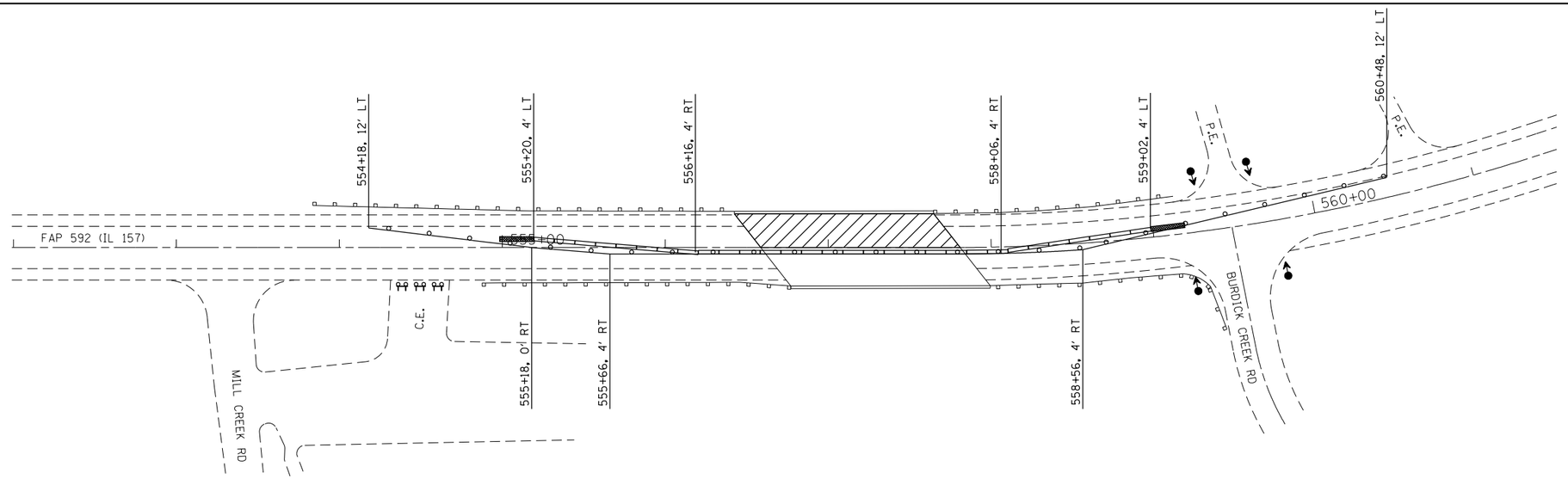
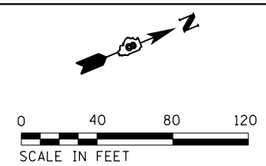
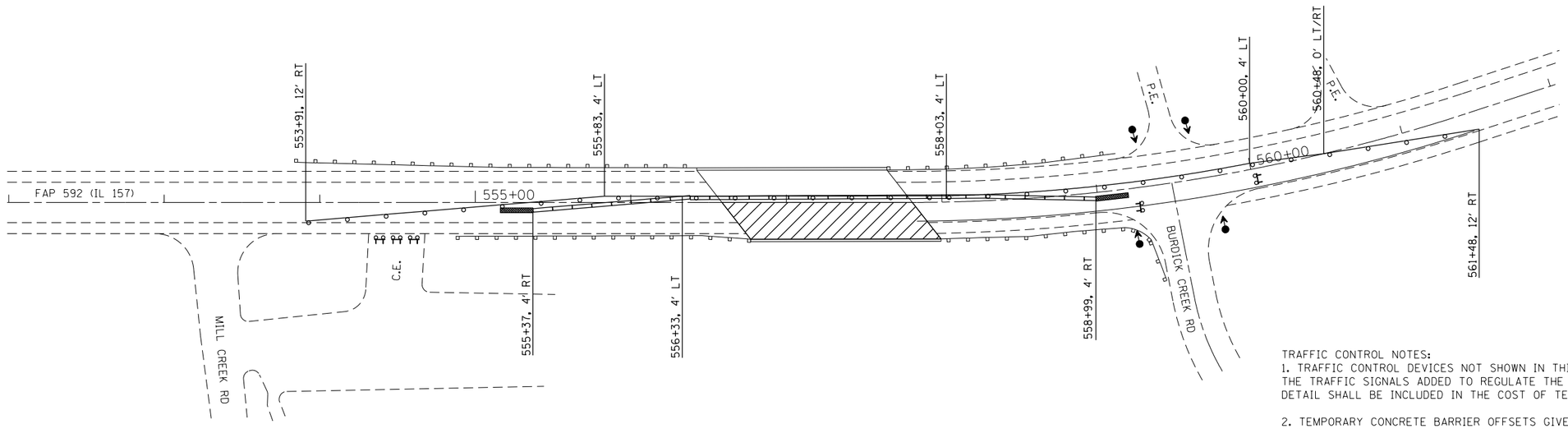
		CONTRACT NO. 76L37		
	ILLINOIS FED. AID PROJECT			





STAGE I



STAGE II

TRAFFIC CONTROL NOTES:

1. TRAFFIC CONTROL DEVICES NOT SHOWN IN THIS DETAIL SHALL BE PER STANDARD 701321. THE TRAFFIC SIGNALS ADDED TO REGULATE THE ENTRANCES AND SIDE ROADS SHOWN IN THIS DETAIL SHALL BE INCLUDED IN THE COST OF TEMPORARY BRIDGE TRAFFIC SIGNALS.

2. TEMPORARY CONCRETE BARRIER OFFSETS GIVEN IN THIS DETAIL REFER TO THE EDGE OF THE BARRIER NEAREST TO LIVE TRAFFIC.

3. RUMBLE STRIPS WILL BE REQUIRED AS SHOWN IN STANDARD 701321.

4. NO TURN ON RED SIGNS (R10-11b) SHALL BE INSTALLED TO THE SATISFACTION OF THE ENGINEER AT NO ADDED COST TO THE CONTRACT.

5. REFER TO STANDARD 701321 FOR TRAFFIC CONTROL SYMBOLS.

6. ATTENUATOR MUST BE 20' OR LESS IN LENGTH FOR STAGE II NORTH END SO THAT IT DOES NOT OBSTRUCT BURDICK CREEK ROAD.

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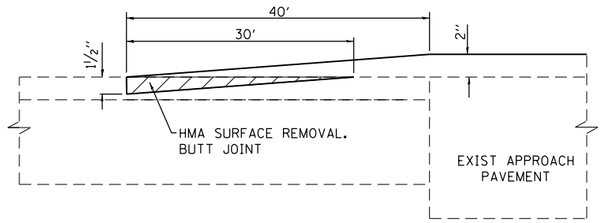
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGING TRAFFIC CONTROL DETAIL

SCALE: 1" = 40' | SHEET 1 OF 1 SHEETS | STA. N/A | TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
592	117-B-R-1	MADISON	19	9
		CONTRACT NO. 76L37		
	ILLINOIS FED. AID PROJECT			



BUTT JOINT DETAIL

NOTE: THE BUTT JOINT EXTENDS ACROSS THE WIDENING AS WELL AS THE PAVEMENT.

USER NAME = default	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 40.0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 1/25/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

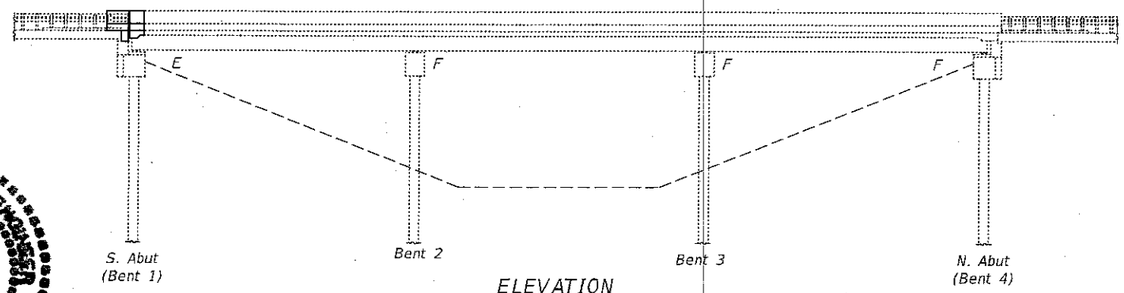
DETAILS

SCALE: N/A | SHEET 1 OF 1 SHEETS | STA. N/A | TO STA. N/A

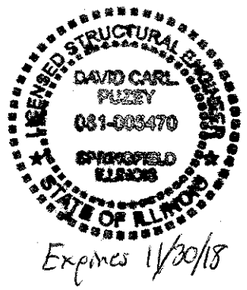
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
592	117-B-R-1	MADISON	19	10
		CONTRACT NO. 76L37		
	ILLINOIS	FED. AID PROJECT		

Existing Structure: 060-0088 Built in 1933 as SBI 157 Sec H7-B as a 3 wide flange simple spans supported on pile bent abutments and piers.
In 1932 the bridge was replaced with a new 3 span continuous wide flange superstructure (reusing abutments and pier piles) as FA 192 Sec 117-BR-1.

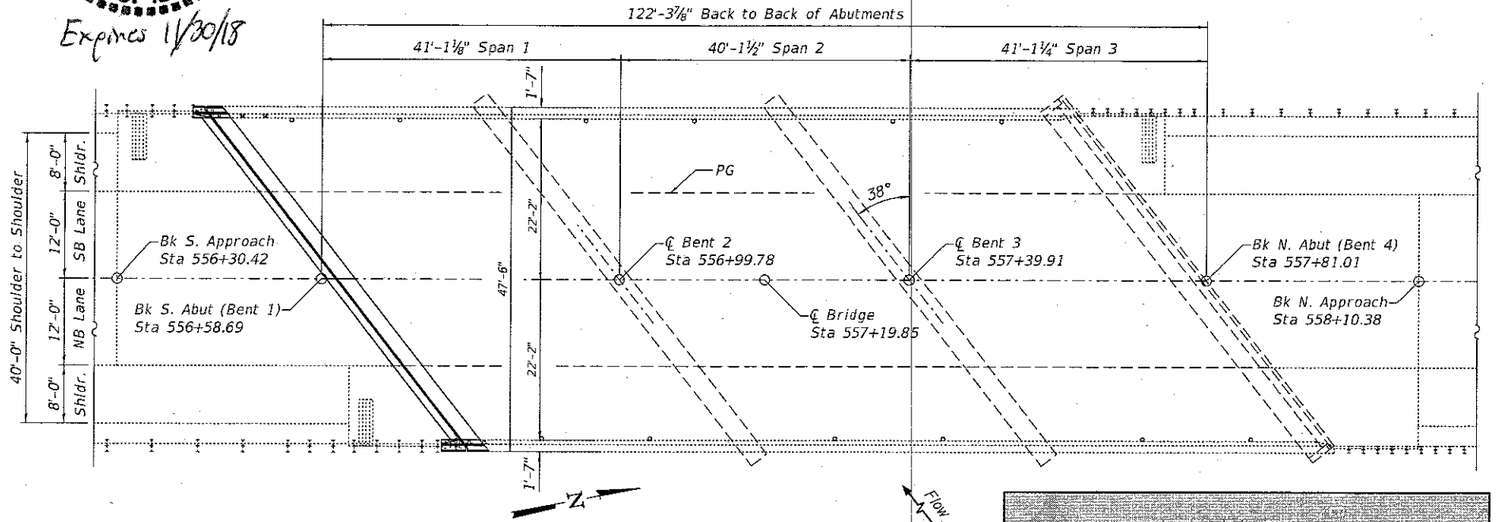
The north abutment is jointless with the deck extending on top of the mudwall, and will not be repaired. The south abutment deck ends and mudwalls shall be replaced with strip seals. At the south abutment, bearings stiffeners shall be added and end diaphragms replaced. The deck shall be patched and overlayed with HMA.



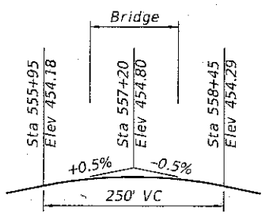
ELEVATION



Expires 11/30/18



PLAN



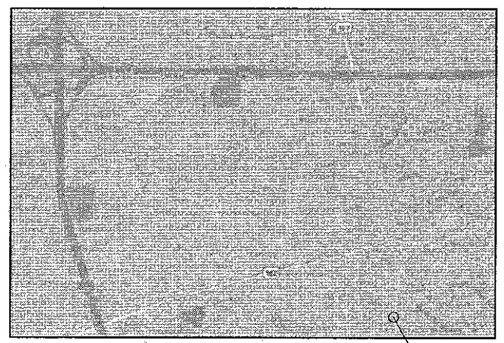
PROFILE GRADE

DESIGN STRESSES

FIELD UNITS

f'c = 3,500 psi
fy = 60,000 psi (Reinforcement)
fy = 36,000 psi (M270 Grade 36)

SHEET INDEX



LOCATION SKETCH

SN 060-0088

GENERAL NOTES

Fasteners shall be ASTM A325 Type 1, mechanically galvanized bolts. Bolts ¾"Ø, holes 13/16"Ø, unless otherwise noted.

All structural steel shall be AASHTO M 270 Grade 36. All structural steel, bolts, nuts, and washers shall be galvanized according AASHTO M111 or M232 as applicable. Cost included with "Furnishing and Erecting Structural Steel".

No field welding is permitted except as specified in the contract documents.

All reinforcement shall be epoxy coated.

Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer.

Any cracks that cannot be removed by grinding ¼ inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Cleaning and field painting of structural steel shall be done under a separate painting contract. Existing structural steel that will be in contact with new structural steel shall only be cleaned and painted prior to erection as required by the Special Provision for "Cleaning and Painting Contact Surface Areas of Existing Steel Structures".

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened, and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

The HMA quantities are shown for the bridge only. The quantity is calculated excluding an assumed ½" sand & ¼" waterproofing membrane, for an HMA thickness of 1¼" thickness

The joints shall be adjusted according to Article 520.04 of the Standard Specs.

The quantity for "Bridge Deck Concrete Sealer" are for the top and inside parapet surface, and all new concrete.

The quantity for "Deck Slab Repair (Partial Depth)" is estimated. The location and sizes of repairs are to be determined by the Engineer in the field.

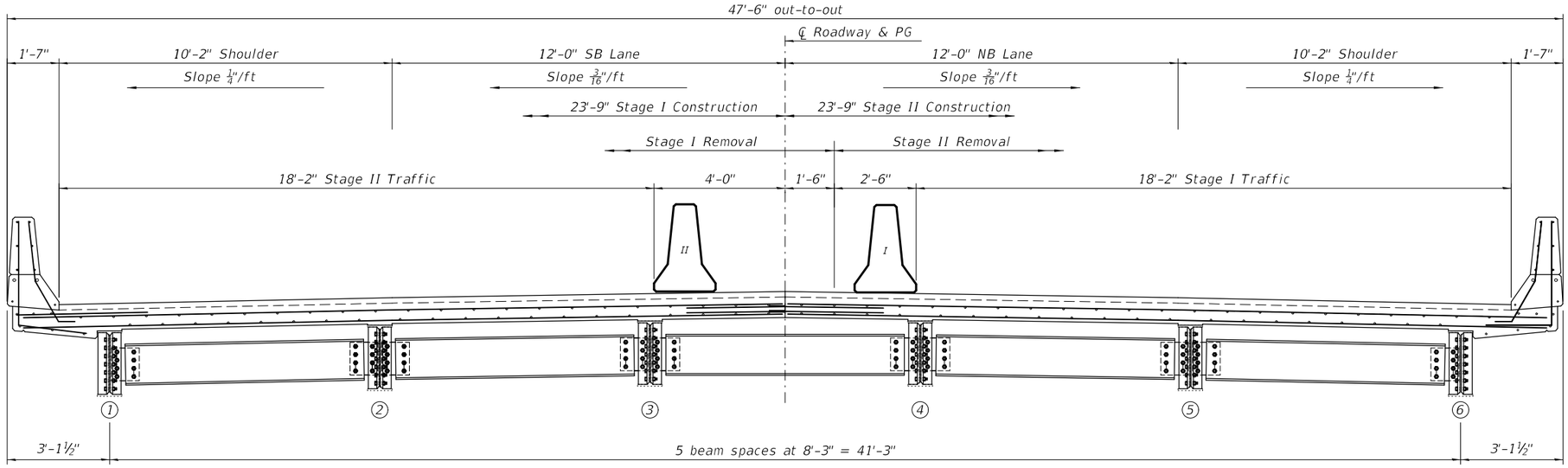
TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Polymerized HMA Surface Course, Mix "D", N70	Ton	41
Concrete Removal	Cu. Yd.	9.0
Concrete Superstructure	Cu. Yd.	9.7
Furnishing and Erecting Structural Steel	Pound	2550
Reinforcement Bars, Epoxy Coated	Pound	1160
Bar Splicers	Each	12
Preformed Joint Strip Seal	Foot	58.5
Waterproofing Membrane System	Sq. Yd.	580
Bridge Deck Concrete Sealer	Sq. Ft.	1030
Structural Steel Removal	Pound	1950
Deck Slab Repair (Partial)	Sq. Yd.	50

2/2/2018 2:08:26 PM

DESIGNED - J. Uehle	EXAMINED	DATE - FEBRUARY 1, 2018	**STATE OF ILLINOIS**
CHECKED - ATH	PASSED	REVISED	**DEPARTMENT OF TRANSPORTATION**
DRAWN - J. Uehle	ENGINEER OF STRUCTURAL SERVICES	REVISED	**GENERAL PLAN & ELEVATION**
CHECKED - ATH	ENGINEER OF BRIDGES AND STRUCTURES		**SN 060-0088 (IL 157 over Burdick Creek)**
			SHEET NO. 1 OF 9 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
592	117-B-R-1	MADISON	19	11
		CONTRACT NO. 76L37		
		ILLINOIS FED. AID PROJECT		

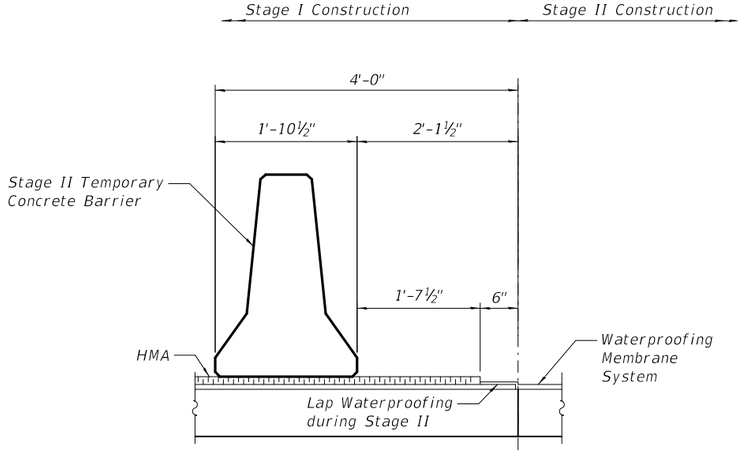


DECK SECTION

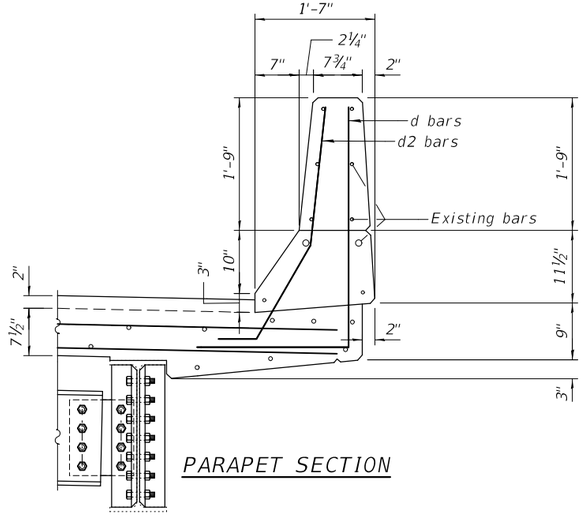
(Looking North)

* Sawcut HMA 1" deep directly over joint between end of deck and approach pavement. Sawcut shall be filled with poured joint filler, cost included with HMA.

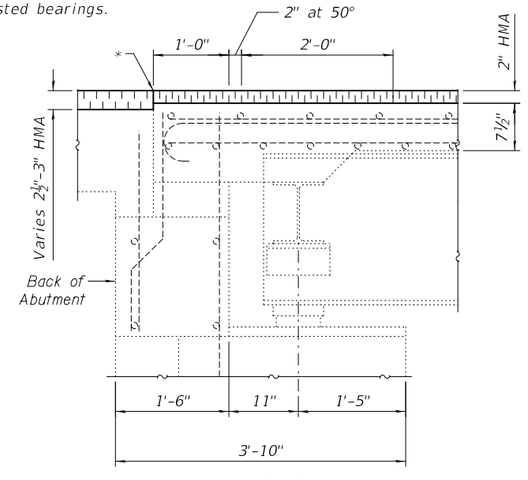
The north end of the bridge deck has raised varying ¼" to 1" above the approach slab, due to pack rusted bearings.



WATERPROOFING STAGING



PARAPET SECTION

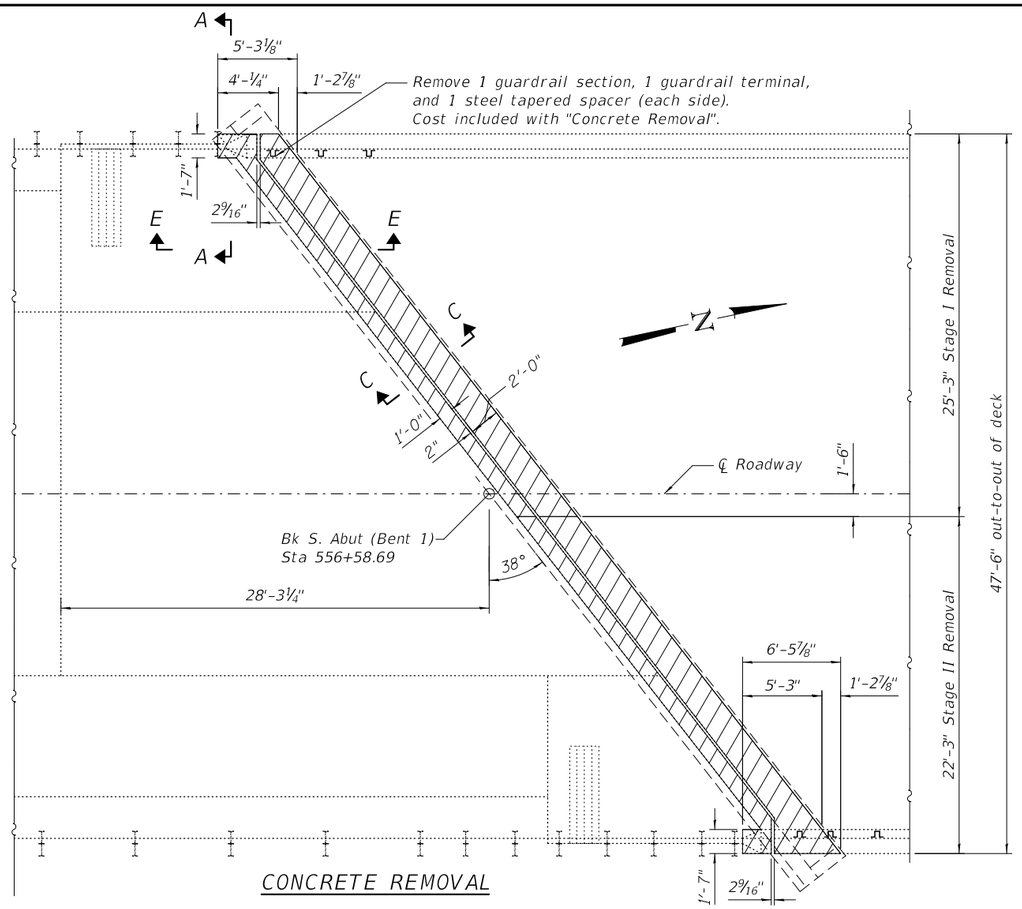


N ABUT SECTION

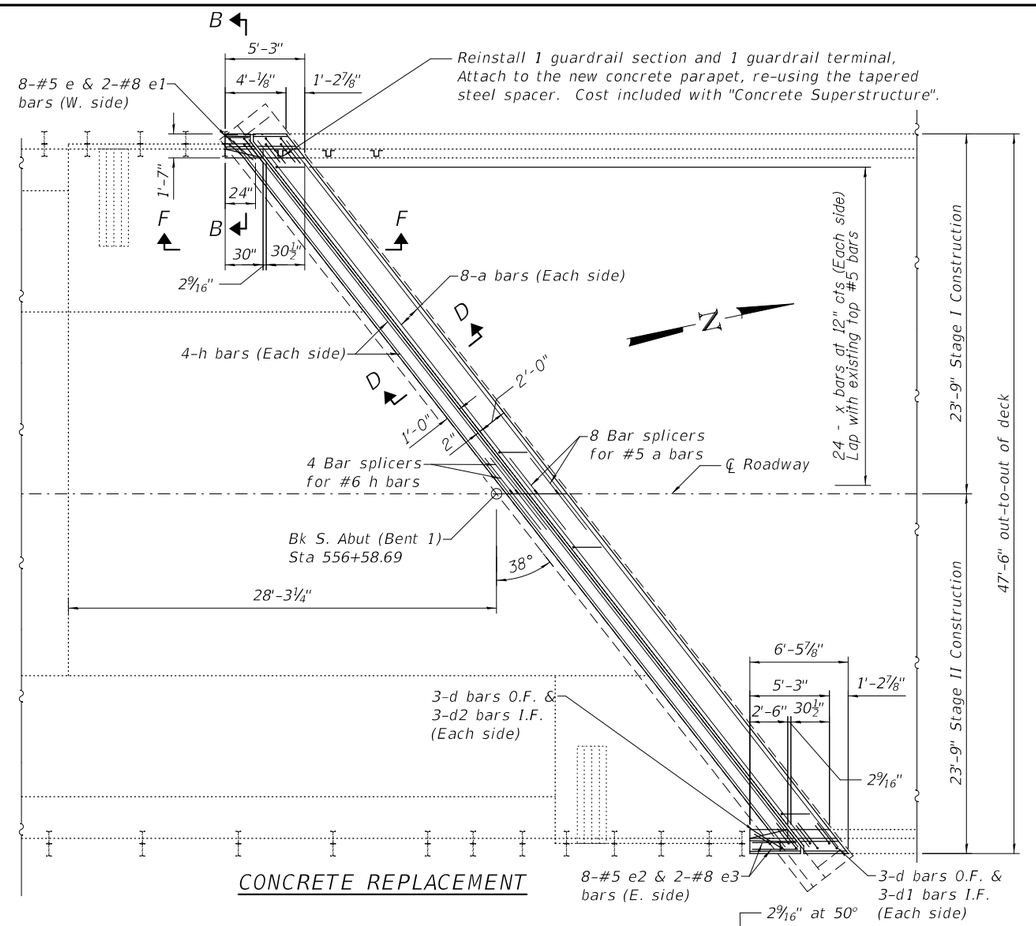
(Looking E. at Rt ℄'s to Abut)

2/2/2018 2:08:27 PM

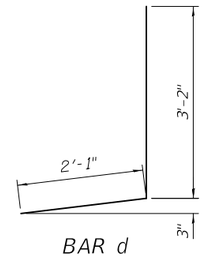
DESIGNED - J. Uehle	EXAMINED	DATE - FEBRUARY 1, 2018	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**	**DECK SECTION SN 060-0088**	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CHECKED - ATH	ACTING ENGINEER OF STRUCTURAL SERVICES	REVISED			592	117-B-R-1	MADISON	19	12
DRAWN - J. Uehle	PASSED	REVISED		SHEET NO. 2 OF 9 SHEETS			CONTRACT NO. 76L37		
CHECKED - ATH	ACTING ENGINEER OF BRIDGES AND STRUCTURES						ILLINOIS FED. AID PROJECT		



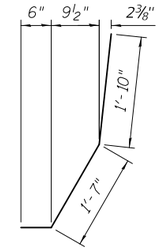
CONCRETE REMOVAL



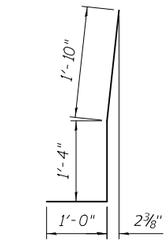
CONCRETE REPLACEMENT



BAR d



BAR d1

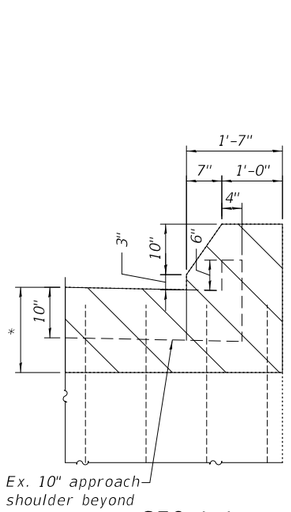


BAR d2

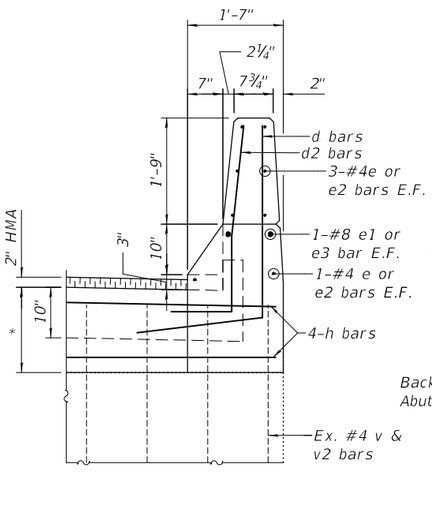
* Varies 1'-7 1/8" (E) to 1'-10 1/2" (℄) to 1'-4 1/2" (W)

** Varies 4 1/4" to 8 3/4"

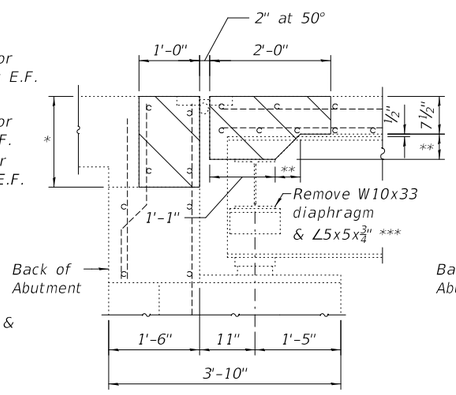
*** The 10 existing diapghragm shelf angles shall be removed using the air-arc method and grind smooth all weld material remaining on the web. Cost included with "Structural Steel Removal".



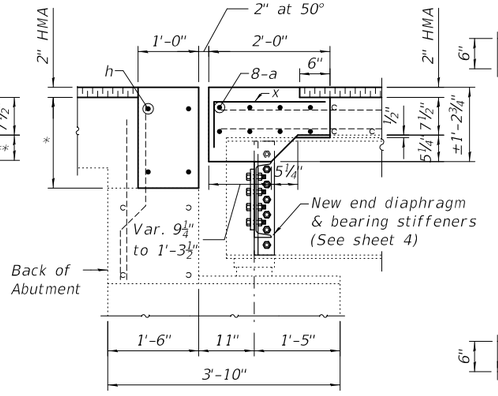
SEC A-A



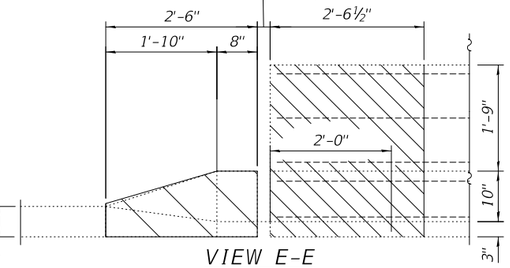
SEC B-B



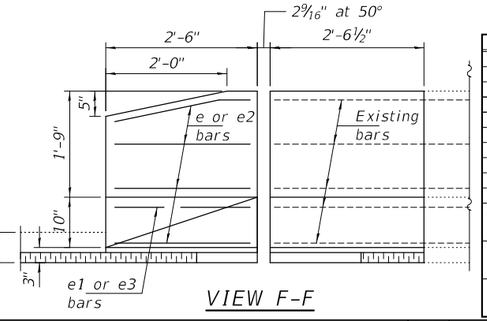
SEC C-C



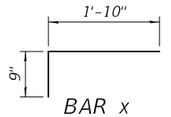
SECTION D-D



VIEW E-E



VIEW F-F



BAR x

BOTH ABUTMENTS
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a	16	#5	29'-7"	
e	8	#5	1'-10"	
e1	2	#8	1'-10"	
e2	8	#5	3'-0"	
e3	2	#8	3'-0"	
d	12	#5	5'-3"	
d1	6	#5	3'-11"	
d2	6	#5	4'-2"	
h	8	#6	29'-7"	
x	48	#5	2'-7"	
Reinforcement Bars, Epoxy Coated		Pound	1160	
Concrete Removal		Cu. Yds.	9.0	
Concrete Superstructure		Cu. Yds.	9.7	

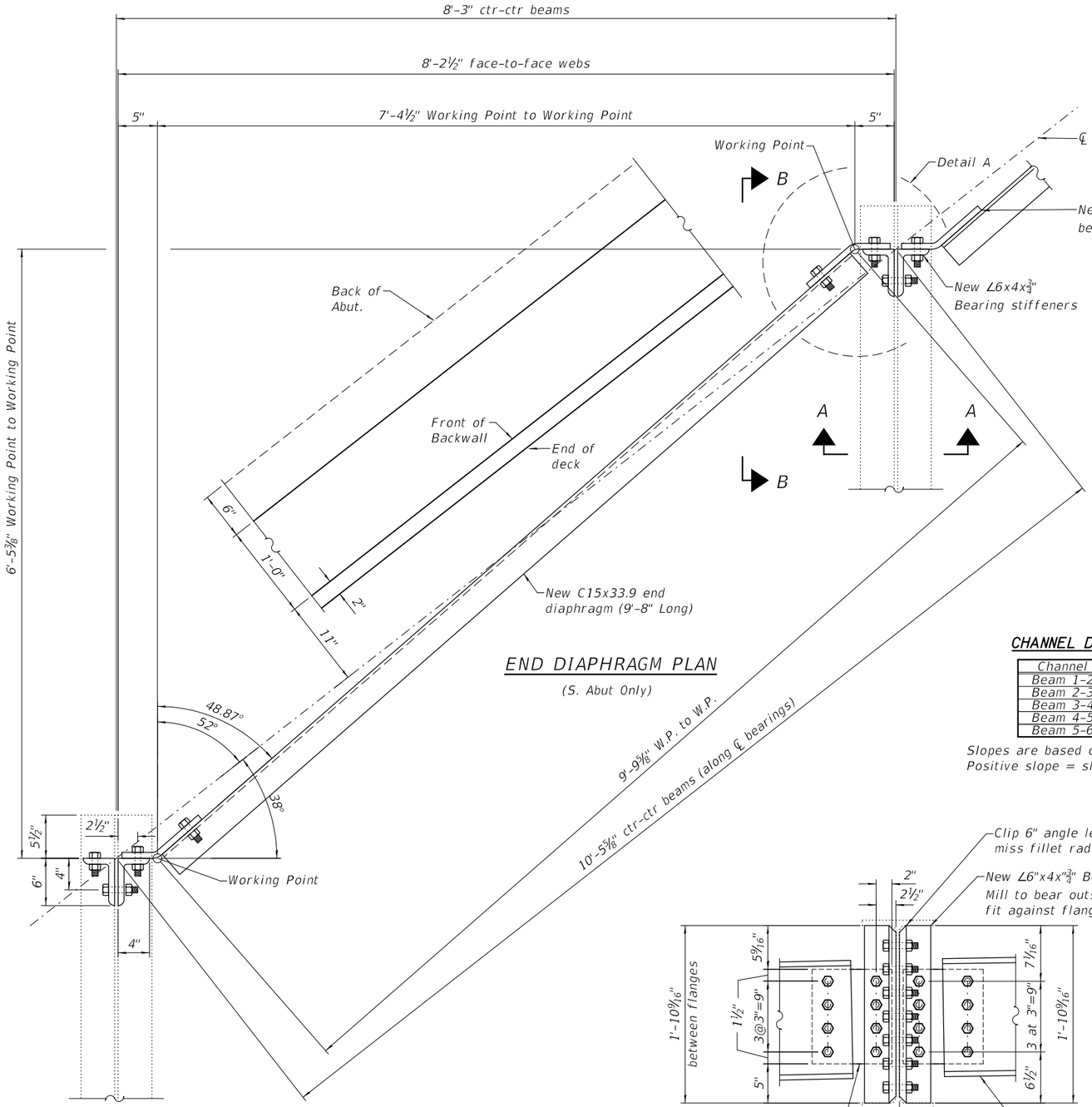
DESIGNED	-	J. Uehle	
CHECKED	-	ATH	
DRAWN	-	J. Uehle	
CHECKED	-	ATH	

EXAMINED
ACTING ENGINEER OF STRUCTURAL SERVICES
DATE - FEBRUARY 1, 2018
PASSED
ACTING ENGINEER OF BRIDGES AND STRUCTURES
REVISED
REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOUTH ABUTMENT
SN 060-0088
SHEET NO. 3 OF 9 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
592	117-B-R-1	MADISON	19	13
		CONTRACT NO. 76L37		
		ILLINOIS	FED. AID PROJECT	



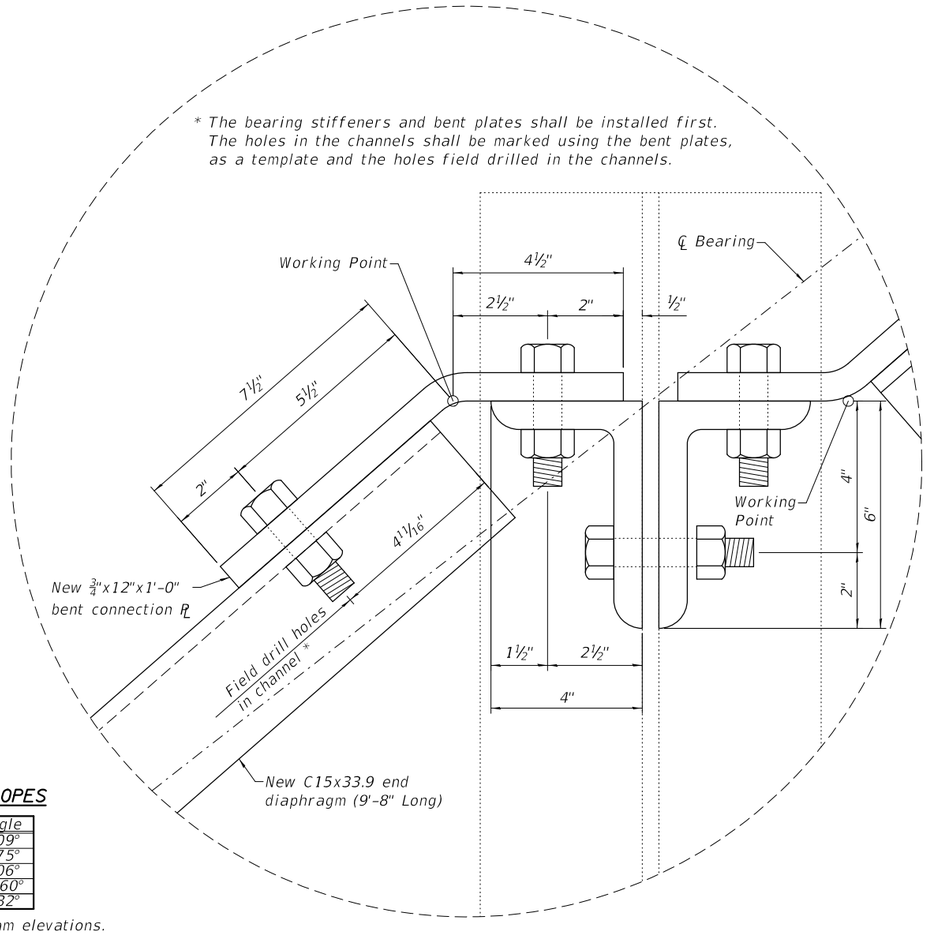
END DIAPHRAGM PLAN

(S. Abut Only)

CHANNEL DIAPHRAGM SLOPES

Channel	Slope	Angle
Beam 1-2	1.91%	1.09°
Beam 2-3	3.06%	1.75°
Beam 3-4	0.10%	0.06°
Beam 4-5	-1.05%	-0.60°
Beam 5-6	-0.57%	0.32°

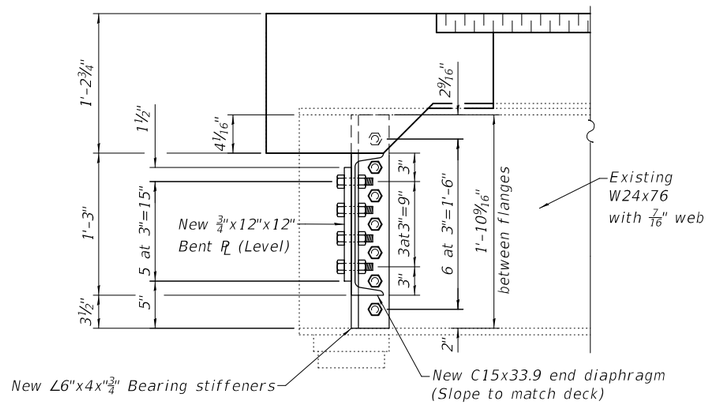
Slopes are based on surveyed beam elevations.
Positive slope = sloping upwards from W. to E.



DETAIL A

SECTION A-A

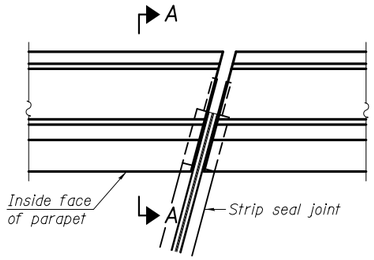
(S. Abut Only)



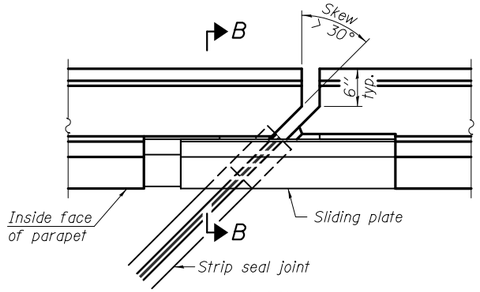
SECTION B-B

(S. Abut Only)

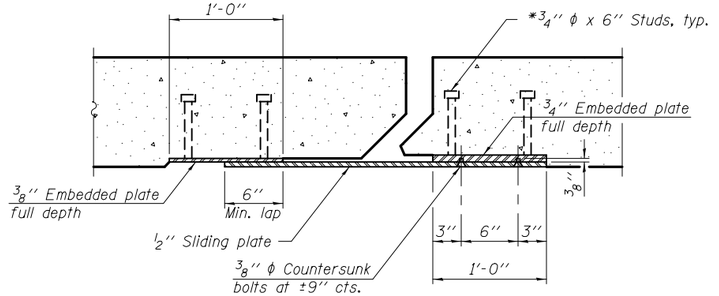
DESIGNED - J. Uehle	EXAMINED	DATE - FEBRUARY 1, 2018	**STATE OF ILLINOIS**	**STEEL DETAILS**	F.A.P. RTE. 592 | SECTION 117-B-R-1 | COUNTY MADISON | TOTAL SHEETS 19 | SHEET NO. 14
CHECKED - ATH	ACTING ENGINEER OF STRUCTURAL SERVICES	REVISED	**DEPARTMENT OF TRANSPORTATION**	**SN 060-0088**	CONTRACT NO. 76L37
DRAWN - J. Uehle	PASSED	REVISED		SHEET NO. 4 OF 9 SHEETS	ILLINOIS FED. AID PROJECT
CHECKED - ATH	ACTING ENGINEER OF BRIDGES AND STRUCTURES				



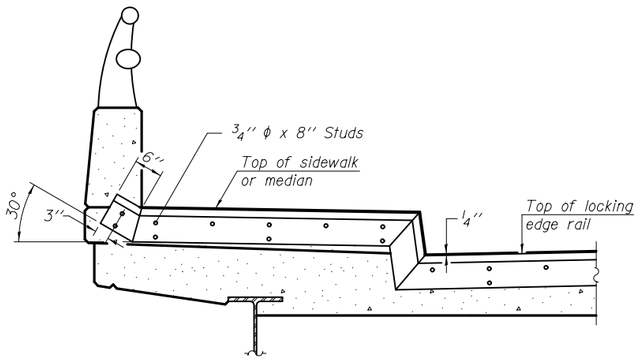
PLAN
(For skews ≤ 30°)



PLAN
(For skews > 30°)
Showing point block

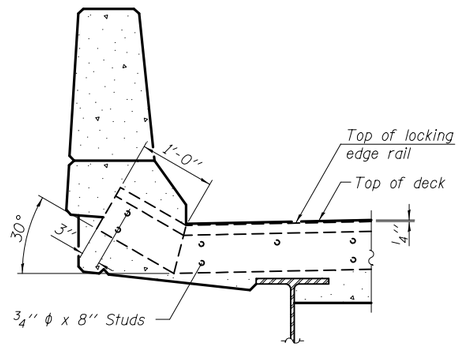


SECTION C-C

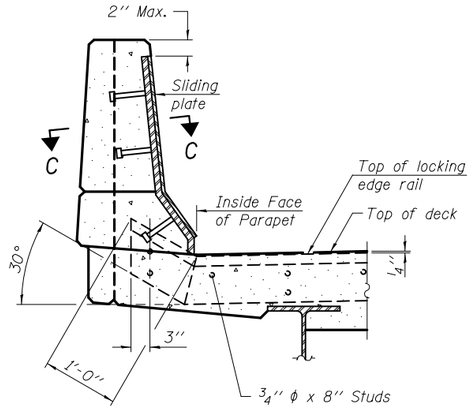


TYPICAL END TREATMENT AT SIDEWALK OR MEDIAN

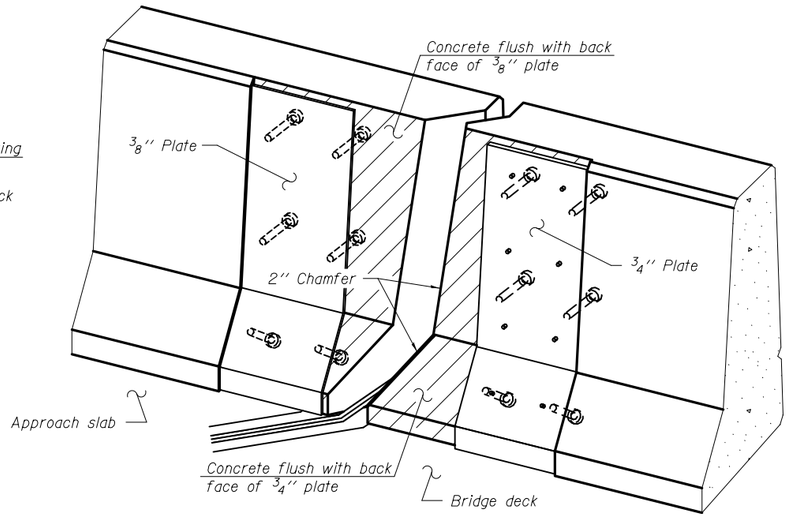
Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.



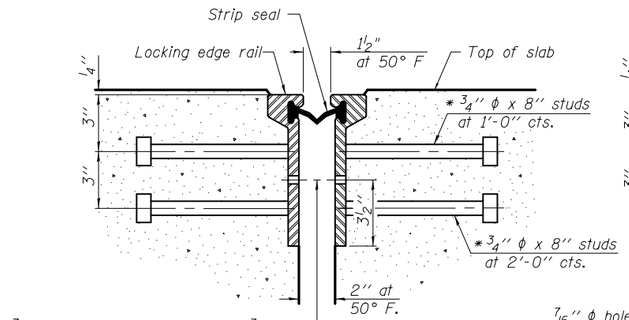
SECTION A-A



SECTION B-B

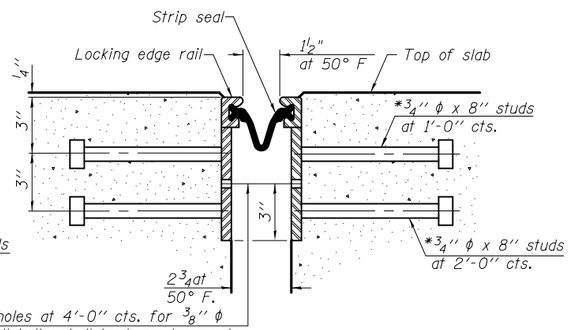


TRIMETRIC VIEW
(Showing back plates only)



7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

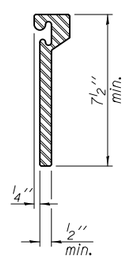
SECTION THRU ROLLED RAIL JOINT



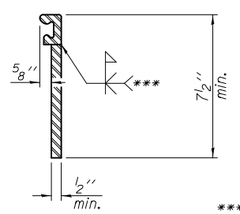
7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

SECTION THRU WELDED RAIL JOINT

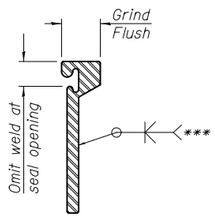
* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



ROLLED EXTRUDED RAIL



WELDED RAIL



*** Back gouge not required if complete joint penetration is verified by mock-up.

LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue.

Rolled rail shown, welded rail similar.

LOCKING EDGE RAILS

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.

The manufacturer's recommended installation methods shall be followed.

The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

Maximum space between rail segments shall be 3/16", sealed with a suitable sealant. Joints in rails within 10 ft. of curbs shall be welded.

Parapet plates and anchorage studs for skews > 30° included in the cost of Preformed Joint Strip Seal.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	58.5

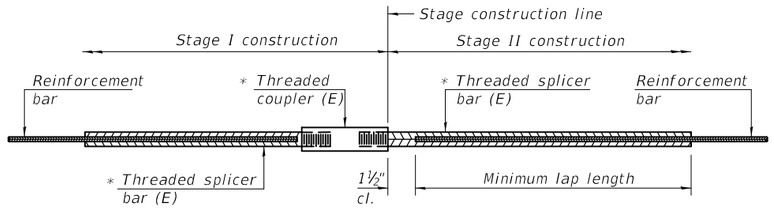
2/2/2018 2:08:29 PM

EJ-SSJ 1-27-12

DESIGNED - J. Uehle	EXAMINED	DATE - FEBRUARY 1, 2018	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
CHECKED - ATH	ACTING ENGINEER OF STRUCTURAL SERVICES		
DRAWN - J. Uehle	PASSED	REVISED	
CHECKED - ATH	ACTING ENGINEER OF BRIDGES AND STRUCTURES	REVISED	

PREFORMED JOINT STRIP SEAL
SN 060-0088
SHEET NO. 5 OF 9 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
592	117-B-R-1	MADISON	19	15
		CONTRACT NO. 76L37		
	ILLINOIS FED. AID PROJECT			

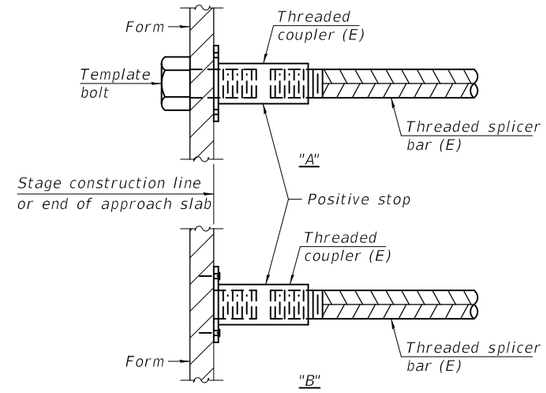


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1½" + thread length

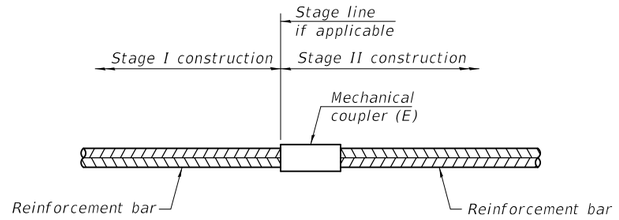
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Abutment Mudwall	#6	4	4'-0"
Deck End	#5	8	3'-6"



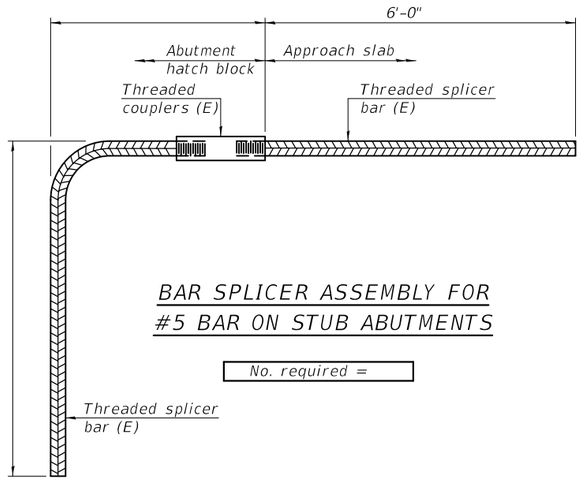
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars.

Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

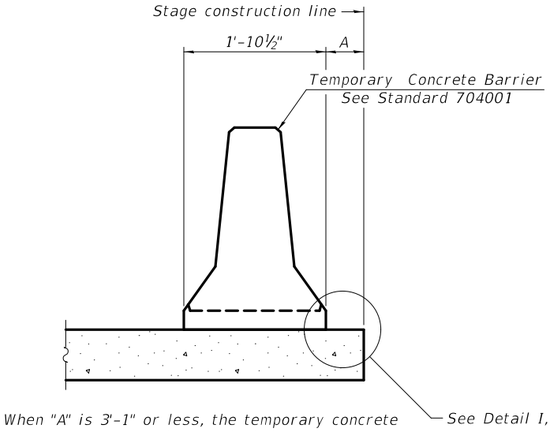
2/2/2018 2:08:29 PM

BSD-1 2-17-2017

DESIGNED - J. Uehle	EXAMINED	DATE - FEBRUARY 1, 2018	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
CHECKED - ATH	ACTING ENGINEER OF STRUCTURAL SERVICES		
DRAWN - J. Uehle	PASSED	REVISED	
CHECKED - ATH	ACTING ENGINEER OF BRIDGES AND STRUCTURES	REVISED	

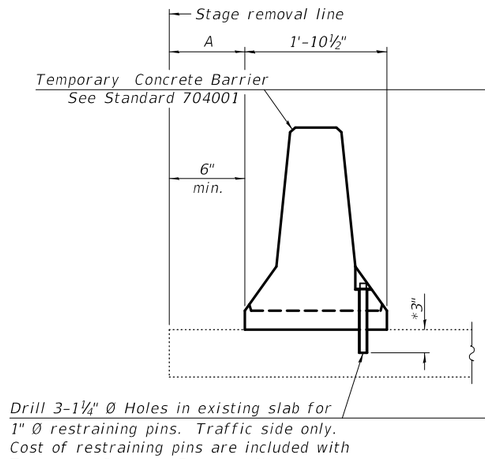
BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
SN 060-0088
SHEET NO. 6 OF 9 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
592	117-B-R-1	MADISON	19	16
		CONTRACT NO. 76L37		
	ILLINOIS	FED. AID PROJECT		



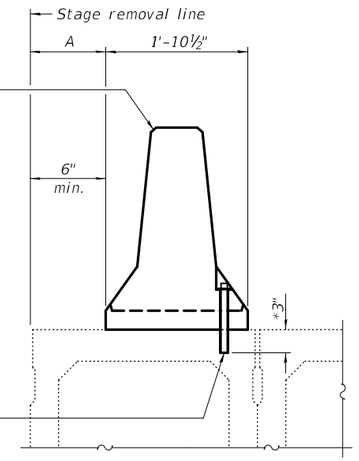
When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



Drill 3-1¼" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

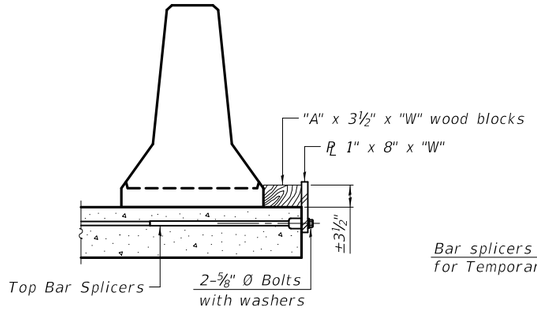
EXISTING SLAB



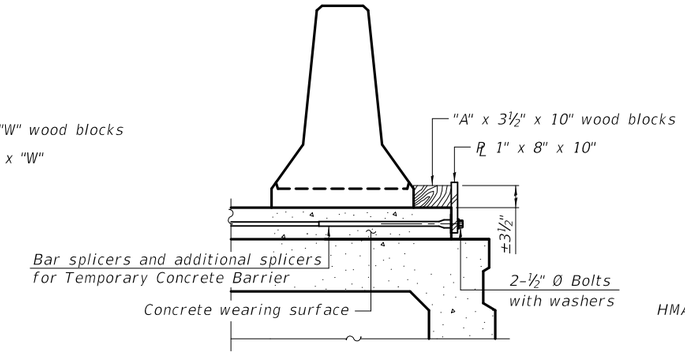
* When hot-mix asphalt wearng surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

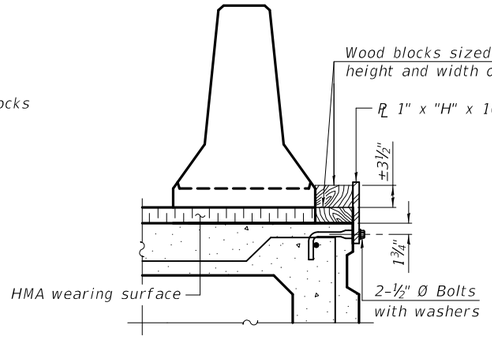
SECTIONS THRU SLAB OR DECK BEAM



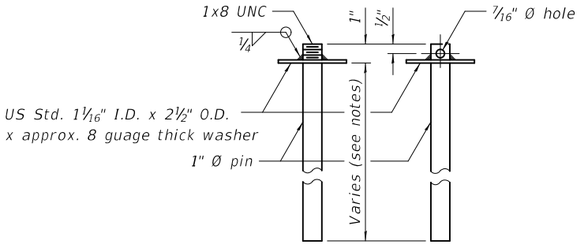
DETAIL I



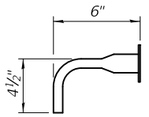
DETAIL II



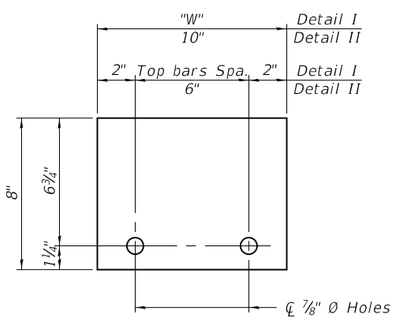
DETAIL III



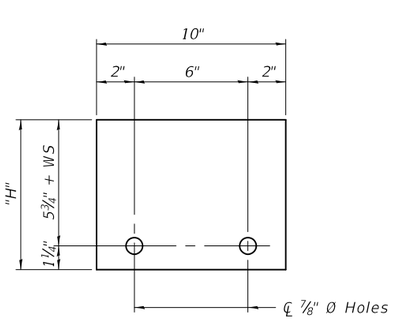
RESTRAINING PIN



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER ℄ 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER ℄ 1" x "H" x 10"
(Detail III)

Notes:

Cost of retainer assembly is included with Temporary Concrete Barrier.

A retainer assembly shall be located at the approximate ℄ of each temporary concrete barrier.

The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.

When the 'A' dimension is less than 1½", the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.

Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.

Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

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R-27

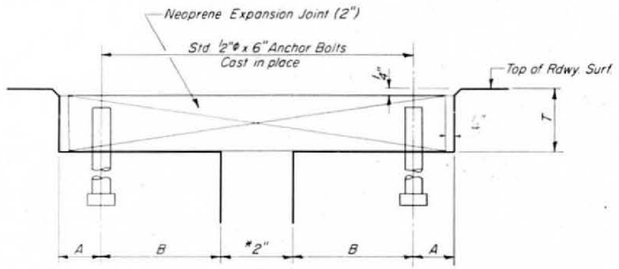
8-11-2017

DESIGNED - J. Uehle	EXAMINED	DATE - FEBRUARY 1, 2018	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION SN 060-0088	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CHECKED - ATH	ACTING ENGINEER OF STRUCTURAL SERVICES	REVISED			592	117-B-R-1	MADISON	19	17
DRAWN - J. Uehle	PASSED	REVISED					CONTRACT NO. 76L37		
CHECKED - ATH	ACTING ENGINEER OF BRIDGES AND STRUCTURES			SHEET NO. 7 OF 9 SHEETS			ILLINOIS FED. AID PROJECT		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

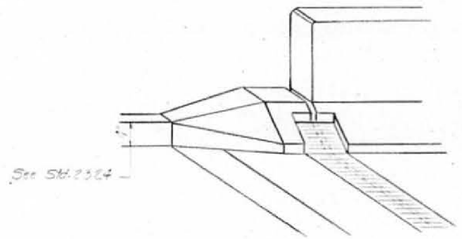
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.P. 592	117BR-1	Madison	20	16
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

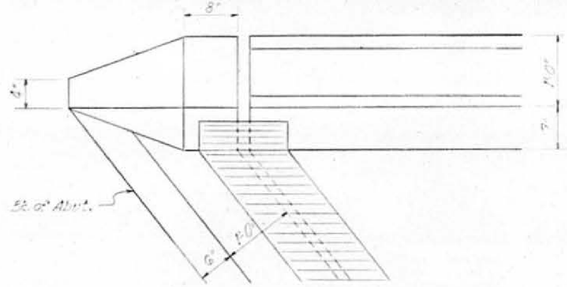
SHEET NO. 19 OF 19 SHEETS



CROSS SECTION
*At 50°F
Dimensions are at right angles

SW CURB



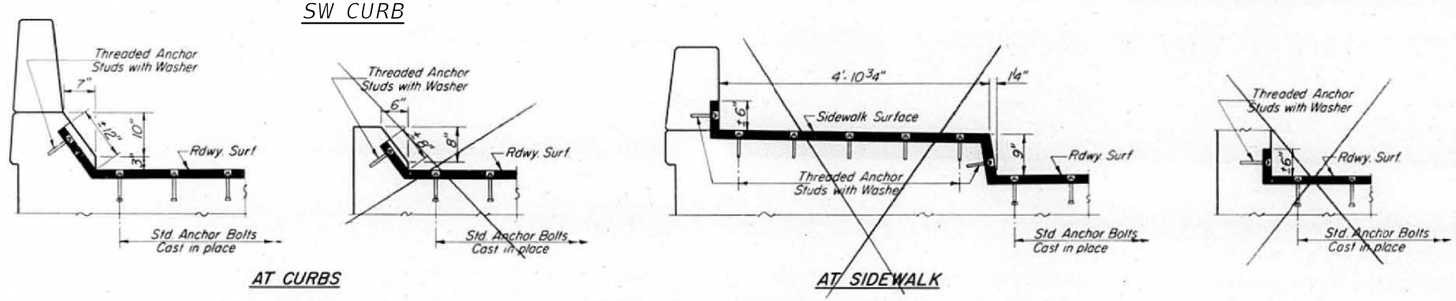


CURB & EXP. JT. DETAIL AT S. ABUT.

ALTERNATE NEOPRENE EXPANSION JOINTS (2")
(See Special Provisions)

Model	Supplier	Blockout Dimensions
TRANSFLEX, MODEL 200A	General Tire Company	T = 1 13/16", A = 1 1/8", B = 3 5/16"
FEL-SPAN, MODEL T-30 Set joint seal 1 5/8" at 50°F	Fel-Pro Building Products Inc.	T = 1 3/4", A = 2 1/4", B = 2 13/16"
WABO ELASTODAM, TYPE 300 Set joint seal 1 5/8" at 50°F	Watson Bowman Associates, Inc.	T = 1 3/4", A = 2 1/4", B = 2 13/16"
WABO ALU-STRIP, TYPE III S300 Set joint seal 1/2" at 50°F Permitted for up to 50° skew	Watson Bowman Associates, Inc.	T = 1 3/4", A = 1 5/8", B = 2 3/4"
LOW PROFILE ONFLEX-25 Set joint seal 1/2" at 50°F Roadway bolt channel shall be filled with approved grout. Permitted for up to 50° skew	Structural Accessories, Inc.	T = 1 3/4", A = 1 5/8", B = 2 3/8"

SE CURB



AT CURBS

AT SIDEWALK

TYPICAL END TREATMENTS

DESIGNED A. J. Ehle
CHECKED P. Struck M. P. Struck
DRAWN S.V.
CHECKED Pump

February 10, 1982
EXAMINED James J. Rayburn
ENGINEER OF BRIDGE DESIGN
PASSED Carl E. Thummanah
ENGINEER OF BRIDGES AND STRUCTURES
APPROVED
DIRECTOR OF HIGHWAYS

FOR INFORMATION ONLY

NEOPRENE EXPANSION JOINTS (2")
FOR EXPANSION LENGTH OF DECK = 0' to 160 ft.

F.A.R.T. 592 SEC. 117 BR-1
MADISON COUNTY
STA. 557+20.00

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DESIGNED - J. Uehle	EXAMINED	DATE - FEBRUARY 1, 2018	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
CHECKED - ATH	ACTING ENGINEER OF STRUCTURAL SERVICES		
DRAWN - J. Uehle	PASSED	REVISED	
CHECKED - ATH	ACTING ENGINEER OF BRIDGES AND STRUCTURES	REVISED	

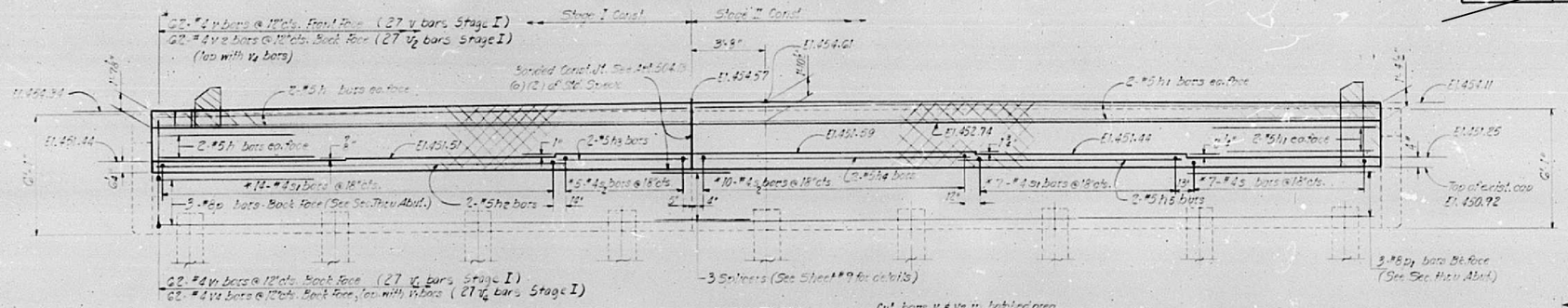
FOR INFORMATION ONLY – 1982 PLANS JOINT & CURB DETAILS
SN 060-0088
SHEET NO. 8 OF 9 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
592	117-B-R-1	MADISON	19	18
			CONTRACT NO. 76L37	
	ILLINOIS	FED. AID PROJECT		

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.S. 592	117BR-1	Madison	20	18
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT:		

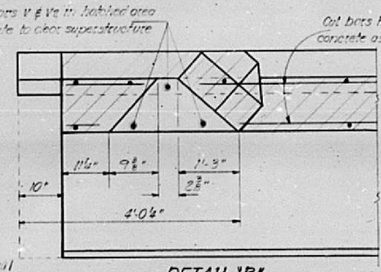
SHEET NO. 12
14 SHEETS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

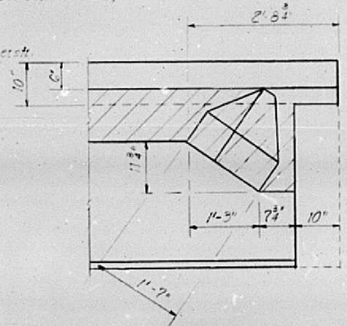


ELEVATION
(Looking South)

*Drill 5/8"ø hole 3" deep for bars s thru s2 & v. See Special Provisions for epoxy grouting.

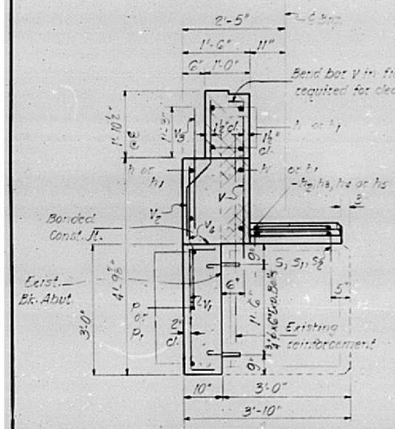


DETAIL "B"

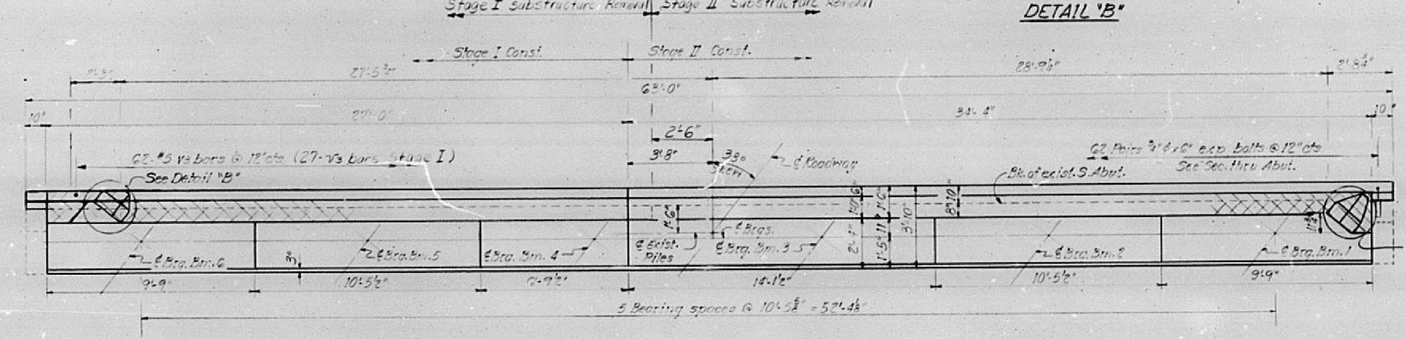


DETAIL "A"
for Curb detail see Sheet #10

FOR INFORMATION ONLY



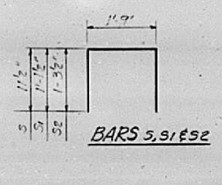
SEC. THRU ABUT.



PLAN

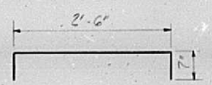
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h	8	#5	20'-9"	
h1	8	#5	34'-0"	
h1a	2	#5	21'-6"	
h3	2	#5	6'-6"	
h4	2	#5	13'-10"	
h5	2	#5	13'-0"	
p	3	#8	27'-7"	
p1	3	#8	34'-11"	
s	7	#4	3'-8"	
s1	21	#4	4'-0"	
s2	15	#4	4'-4"	
v	62	#4	4'-2"	
v1	62	#4	3'-8"	
v2	62	#4	3'-3"	
v3	62	#5	2'-6"	
v4	62	#4	3'-4"	
Concrete Removal		Cu.Yd.	4.5	
Class X Concrete		Cu.Yd.	18.8	
Reinforcement Bars		Pound	2,000	
Expansion Bolts ¾"ø		Each	124	



BARS s, s1 & s2

BAR V2



BAR V1

Notes:
Hatched area to be poured after superstructure forms have been removed.
Quantity of Class X Concrete included with superstructure.
Cross-hatched area indicates Concrete Removal.
All edges shall have standard ¾" chamfers unless otherwise noted.
Expansion bolts shall be anchored in sound concrete.
Existing reinforcement extending into removed area shall be cleaned and incorporated into the new construction.

DESIGNED	A.T. Brown
CHECKED	Patrick M. Palmer
DRAWN	S.W.W.
CHECKED	PMP

February 10, 1982

EXAMINED James T. Rowbaun
ENGINEER OF STRUCTURAL SERVICES

PASSED Carl E. Tennant
ENGINEER OF BRIDGES AND STRUCTURES

APPROVED
DIRECTOR OF HIGHWAYS

SOUTH ABUTMENT
F.A.R.T. 592 SEC. 117BR-1
MADISON COUNTY
STA. 557+20.00

2/2/2018 2:08:36 PM

DESIGNED - J. Uehle	EXAMINED	DATE - FEBRUARY 1, 2018	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
CHECKED - ATH	ACTING ENGINEER OF STRUCTURAL SERVICES		
DRAWN - J. Uehle	PASSED	REVISED	
CHECKED - ATH	ACTING ENGINEER OF BRIDGES AND STRUCTURES	REVISED	

FOR INFORMATION ONLY – 1982 PLANS SOUTH ABUTMENT DETAILS
SN 060-0088
SHEET NO. 9 OF 9 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
592	117-B-R-1	MADISON	19	19
		CONTRACT NO. 76L37		
	ILLINOIS	FED. AID PROJECT		