

# DUPAGE AIRPORT AUTHORITY WEST CHICAGO, ILLINOIS

## CONSTRUCTION PLANS FOR DUPAGE AIRPORT

### ROADWAY IMPROVEMENTS FOR SOUTH FLIGHT CENTER DEVELOPMENT - PHASE 1

ILLINOIS PROJECT: DPA-3325

MARCH 3, 2006

**DUPAGE AIRPORT**

TOWNSHIP: 40 NORTH      WAYNE TOWNSHIP  
RANGE: 9 EAST      (SECTIONS: 31)  
DUPAGE COUNTY

**SOUTH FLIGHT CENTER APRON**  
DESIGN AIRCRAFT APPROACH CATEGORY D  
DESIGN AIRCRAFT GROUP III

**CALL JULIE  
BEFORE EXCAVATING  
1-800-892-0123**

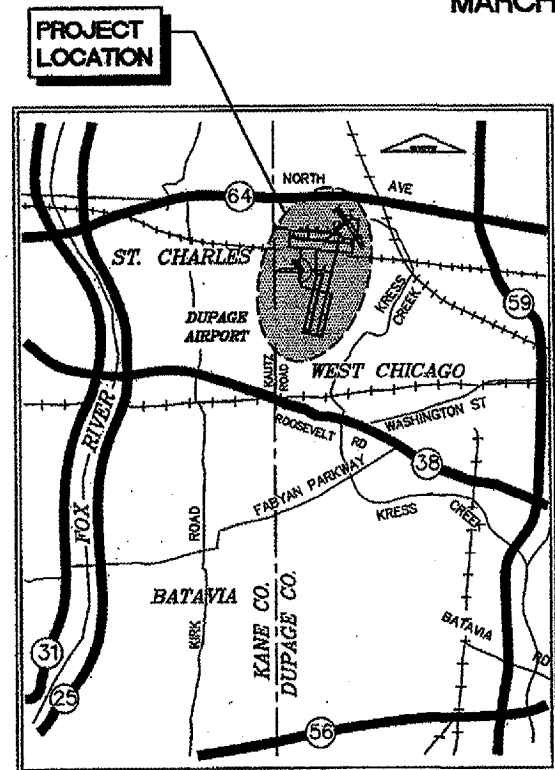
**CMT**  
CRAWFORD, MURPHY & TILLY, INC.  
CONSULTING ENGINEERS  
600 N. COMMONS DRIVE  
SUITE 107  
ALTOONA, IL 60504  
PHONE (830) 820-0242  
FAX (830) 820-0350

05257-02  
DANIEL L. PAPE, P.E.  
REGISTERED PROFESSIONAL ENGINEER  
ILLINOIS  
EXPIRES 11/30/07

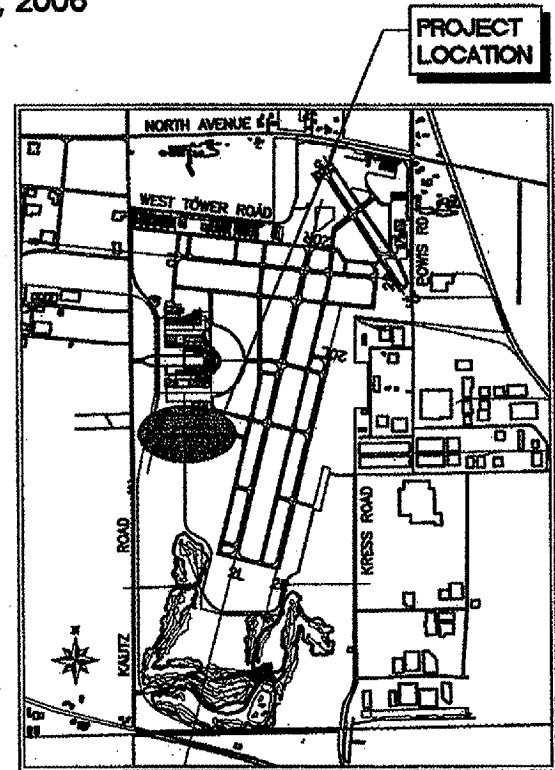
SUBMITTED BY: *[Signature]*  
DATE: 3/3/06

**DDA**  
**DuPage Airport**  
2700 INTERNATIONAL DRIVE  
SUITE 200  
WEST CHICAGO, IL 60095

APPROVED BY: *[Signature]*  
DAVID BIRD - EXECUTIVE DIRECTOR  
DATE: 3-2-06



LOCATION MAP



SITE PLAN

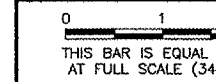
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**SUMMARY OF QUANTITIES**

ITEM NO.	DESCRIPTION	UNIT	FREEDOM DR. QUANTITY	DUPAGE DR. QUANTITY	TOTAL ESTIMATED QUANTITY	FREEDOM DR. RECORD QUANTITY	DUPAGE DR. RECORD QUANTITY	TOTAL RECORD QUANTITY
<b>STATE/LOCAL</b>								
AR110314	4" STEEL DUCT, JACKED	L.F.	55		55			
AR110504	4-WAY CONCRETE ENCASED DUCT	L.F.		40	40			
AR110946	ADJUST ELECTRICAL HANDHOLE	EACH	1		1			
AR150510	ENGINEER'S FIELD OFFICE	L.S.		1	1			
AR152410	UNCLASSIFIED EXCAVATION	C.Y.		7545	7545			
AR156510	SILT FENCE	L.F.	247	1843	2090			
AR156512	BALES	EACH	83	88	171			
AR162960	RELOCATE CLASS E FENCE	L.F.		200	200			
AR201610	BITUMINOUS BASE COURSE	TON		150	150			
AR208515	POROUS GRANULAR EMBANKMENT	C.Y.		50	50			
AR209608	CRUSHED AGG. BASE COURSE - 6"	SY		298	298			
AR209610	CRUSHED AGG. BASE COURSE - 10"	S.Y.		1259	1259			
AR209706	CRUSHED AGG. SHOULDER - 6"	S.Y.	1022		1022			
AR401610	BITUMINOUS SURFACE COURSE	TON	370	150	520			
AR401655	BUTT JOINT CONSTRUCTION	S.Y.	150		150			
AR401900	REMOVE BITUMINOUS PAVEMENT	S.Y.	105	5	110			
AR401910	REMOVE & REPLACE BIT. PAVEMENT	S.Y.	140		140			
AR602510	BITUMINOUS PRIME COAT	GAL.		386	386			
AR603510	BITUMINOUS TACK COAT	GAL.	472	193	665			
AR620520	PAVEMENT MARKING - WATERBORNE	S.F.	242	352	594			
AR701512	12" RCP, CLASS IV	L.F.		330	330			
AR705526	6" PERFORATED UNDERDRAIN W/ SOCK	L.F.		770	770			
AR751411	INLET - TYPE A	EACH		2	2			
AR751412	INLET - TYPE B	EACH		1	1			
AR751540	MANHOLE 4'	EACH		5	5			
AR754904	REMOVE COMB CURB & GUTTER	L.F.		10	10			
AR800011	PRECAST CONC. BOX CULVERT 10' X 4'	L.F.		120	120			
AR800053	SOIL GUARD	S.Y.	2584	7460	10044			
AR800140	2 1/C #8 XLP-USE, 1/C #10 GND - 1" UD	L.F.	1252		1252			
AR800147	BOX CULVERT END SECTION	EACH		2	2			
AR901510	SEEDING	ACRE	0.5	1.5	2			
AR910101	ROADWAY LIGHT POLE, TYPE A	EACH	9		9			
AR910121	ROADWAY LIGHT FIXTURE, TYPE A	EACH	9		9			
AR910200	ROADWAY SIGN	EACH		4	4			
<b>LOCAL ONLY</b>								
AR201610	BITUMINOUS BASE COURSE	TON		14	14			
AR209610	CRUSHED AGG. BASE COURSE - 10"	S.Y.		115	115			
AR401610	BITUMINOUS SURFACE COURSE	TON		14	14			
AR602510	BITUMINOUS PRIME COAT	GAL.		35	35			
AR603510	BITUMINOUS TACK COAT	GAL.		18	18			
AR754410	COMB CONCRETE CURB & GUTTER	L.F.		1010	1010			

REVISIONS		
NUMBER	BY	DATE





**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 ROADWAY IMPROVEMENTS FOR SOUTH FLIGHT CENTER  
 DEVELOPMENT - PHASE 1**

**SUMMARY OF QUANTITIES**

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APPROVED BY:	DLP
DATE:	03/03/06
JOB No:	05257-02

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NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).

**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 ROADWAY IMPROVEMENTS FOR SOUTH FLIGHT CENTER  
 DEVELOPMENT - PHASE 1**  
**SITE PLAN / HORIZONTAL  
 AND VERTICAL CONTROL**

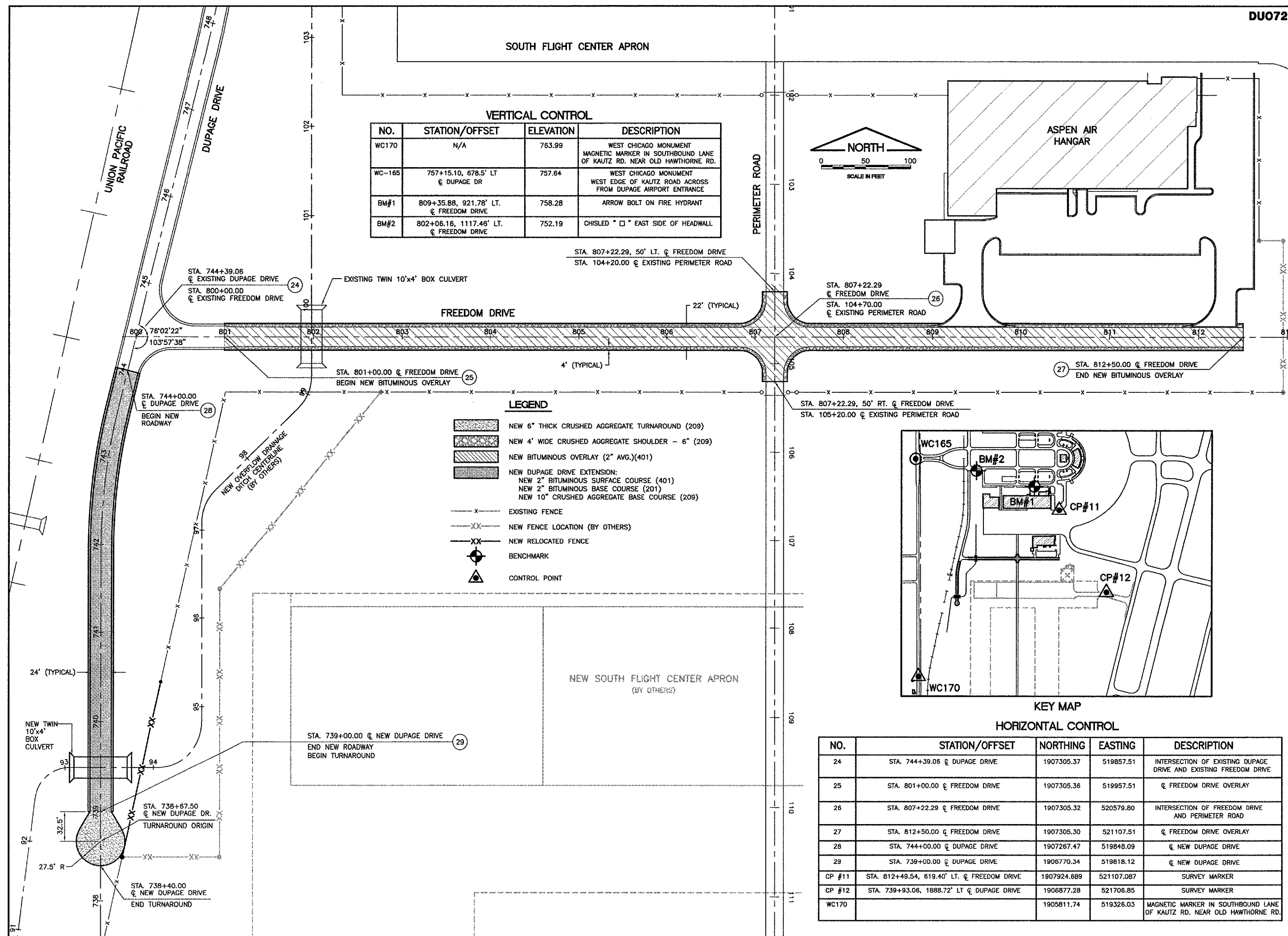
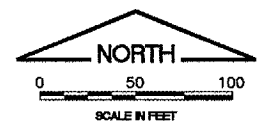
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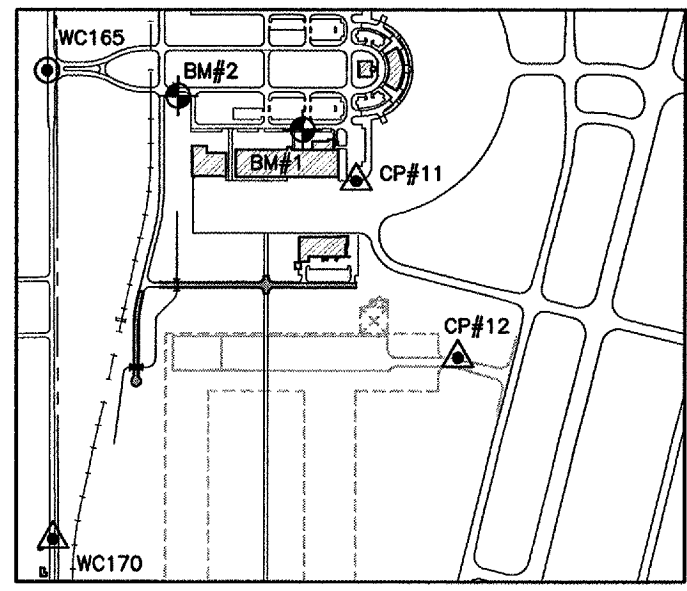
**VERTICAL CONTROL**

NO.	STATION/OFFSET	ELEVATION	DESCRIPTION
WC170	N/A	763.99	WEST CHICAGO MONUMENT MAGNETIC MARKER IN SOUTHBOUND LANE OF KAUTZ RD. NEAR OLD HAWTHORNE RD.
WC-165	757+15.10, 678.5' LT @ DUPAGE DR	757.64	WEST CHICAGO MONUMENT WEST EDGE OF KAUTZ ROAD ACROSS FROM DUPAGE AIRPORT ENTRANCE
BM#1	809+35.88, 921.78' LT. @ FREEDOM DRIVE	758.28	ARROW BOLT ON FIRE HYDRANT
BM#2	802+06.16, 1117.46' LT. @ FREEDOM DRIVE	752.19	CHISLED " □ " EAST SIDE OF HEADWALL



**LEGEND**

	NEW 6" THICK CRUSHED AGGREGATE TURNAROUND (209)
	NEW 4" WIDE CRUSHED AGGREGATE SHOULDER - 6" (209)
	NEW BITUMINOUS OVERLAY (2" AVG.)(401)
	NEW DUPAGE DRIVE EXTENSION: NEW 2" BITUMINOUS SURFACE COURSE (401) NEW 2" BITUMINOUS BASE COURSE (201) NEW 10" CRUSHED AGGREGATE BASE COURSE (209)
	EXISTING FENCE
	NEW FENCE LOCATION (BY OTHERS)
	NEW RELOCATED FENCE
	BENCHMARK
	CONTROL POINT



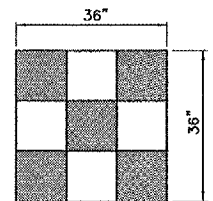
**HORIZONTAL CONTROL**

NO.	STATION/OFFSET	NORTHING	EASTING	DESCRIPTION
24	STA. 744+39.06 @ DUPAGE DRIVE	1907305.37	519857.51	INTERSECTION OF EXISTING DUPAGE DRIVE AND EXISTING FREEDOM DRIVE
25	STA. 801+00.00 @ FREEDOM DRIVE	1907305.36	519957.51	@ FREEDOM DRIVE OVERLAY
26	STA. 807+22.29 @ FREEDOM DRIVE	1907305.32	520579.80	INTERSECTION OF FREEDOM DRIVE AND PERIMETER ROAD
27	STA. 812+50.00 @ FREEDOM DRIVE	1907305.30	521107.51	@ FREEDOM DRIVE OVERLAY
28	STA. 744+00.00 @ DUPAGE DRIVE	1907267.47	519848.09	@ NEW DUPAGE DRIVE
29	STA. 739+00.00 @ DUPAGE DRIVE	1906770.34	519818.12	@ NEW DUPAGE DRIVE
CP #11	STA. 812+49.54, 619.40' LT. @ FREEDOM DRIVE	1907924.889	521107.087	SURVEY MARKER
CP #12	STA. 739+93.06, 1888.72' LT @ DUPAGE DRIVE	1906877.28	521706.85	SURVEY MARKER
WC170		1905811.74	519326.03	MAGNETIC MARKER IN SOUTHBOUND LANE OF KAUTZ RD. NEAR OLD HAWTHORNE RD.

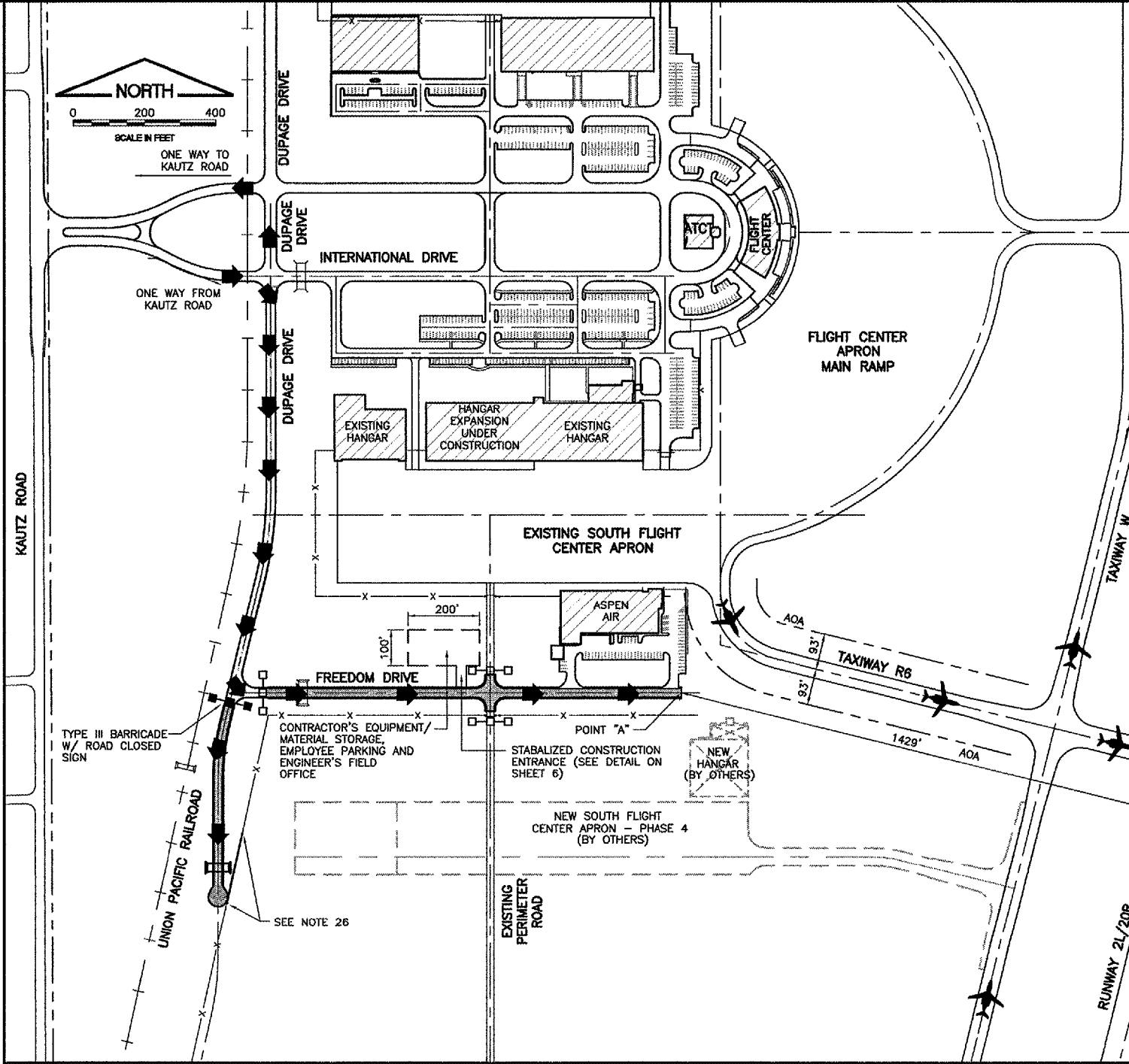
**GENERAL NOTES**

- ALL CONSTRUCTION SEQUENCING AND OPERATIONS SHALL CONFORM TO THE APPLICABLE PROVISIONS OF AC 150/5370-2E OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION (LATEST EDITION). PAYMENT FOR TRAFFIC CONTROL DEVICES AND MAINTENANCE OF TRAFFIC AND TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO, TEMPORARY PAVEMENT MARKING, TEMPORARY PAVEMENT MARKING REMOVALS, BARRICADES AND THE MOVING AND MAINTENANCE OF BARRICADES, TEMPORARY SIGNING, TEMPORARY SIGNING REMOVAL, AIR OPERATIONS AREA (A.O.A.) LATHE AND RIBBON, ETC. SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR MUST STAGE CONSTRUCTION AROUND AIRPORT OPERATIONS. STAGING SHOWN IS SUGGESTED AND IS INTENDED TO PROVIDE THE CONTRACTOR WITH MAJOR WORK AREAS WHILE MINIMIZING DISRUPTIONS TO AIRPORT OPERATIONS. THE CONTRACTOR MAY USE ALTERNATE STAGING PLANS; HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT DIRECTOR AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS.
- THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER TWO (2) WORKING DAYS IN ADVANCE OF ANY STAGING CHANGES, WHICH WILL REQUIRE CHANGES IN AIRCRAFT MOVEMENT. THE RESIDENT ENGINEER SHALL THEN NOTIFY THE AIRPORT, WHO WILL ISSUE APPROPRIATE NOTAMS.
- BARRICADES AT 15-FOOT CENTERS SHALL BE PLACED AT THE LOCATIONS SHOWN OR AS DIRECTED BY THE AIRPORT DIRECTOR IN CONSULTATION WITH THE RESIDENT ENGINEER. BARRICADES SHALL BE WEIGHED TO PREVENT BLOWING OVER, HAVE A FLASHING RED LIGHT AND CONFORM TO DOT STANDARD 702001, TYPE II AND TYPE III. ROPE WITH HIGH VISIBILITY ORANGE FLAGGING SHALL BE INSTALLED BETWEEN ALL BARRICADES.
- THE AIRPORT DIRECTOR IN CONSULTATION WITH THE RESIDENT ENGINEER, SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING PAVEMENTS, EXCEPT AS SHOWN OR WITH THE PRIOR APPROVAL OF THE AIRPORT DIRECTOR.
- EXISTING AREAS BEYOND PROJECT LIMITS INCLUDING THE HAUL ROAD(S) AND STAGING AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY THE CONTRACTOR TO PRE-CONSTRUCTION CONDITION AT NO ADDITIONAL COST TO THE OWNER AND TO THE SATISFACTION OF THE RESIDENT ENGINEER.
- EACH DAY AT THE COMPLETION OF WORK, OR MORE FREQUENTLY AS DETERMINED BY THE AIRPORT DIRECTOR THE CONTRACTOR SHALL BE REQUIRED TO USE A PICKUP TYPE SWEEPER IN ALL ACTIVE CONSTRUCTION AIRFIELD PAVEMENT AREAS AND AT EXISTING AIRPORT ROADS. THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. THE COST OF SWEEPING SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE AREA WITHIN THE LIMITS OF EACH PHASE AS DESIGNATED BY THE AIRPORT DIRECTOR WHEN CONSTRUCTION IS NOT IN PROGRESS. CONTRACTOR SHALL PLACE EQUIPMENT/MATERIAL STORAGE AND EMPLOYEE PARKING AREA WITHIN THE CONSTRUCTION SITE BOUNDARIES. AT NO TIME WILL THIS AREA BE ON EXISTING PAVEMENTS OR NEWLY CONSTRUCTED PAVEMENT.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
- MATERIALS REMOVED FROM THE PROJECT SHALL BE DISPOSED OF AT AN APPROVED SITE OFF OF THE AIRPORT PROPERTY.
- THE CONTRACTOR WILL BE REQUIRED TO PLACE A LINE OF LATHE AND RIBBON ALONG THE AIR OPERATIONS AREA (A.O.A.) LIMIT, RIGHT OF WAY LIMIT AND ALONG DESIGNATED LIMITS OF CONSTRUCTION AT LOCATIONS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER
- THE CONTRACTOR SHALL CONTACT THE RESIDENT ENGINEER AND THE AIRPORT DIRECTOR A MINIMUM OF FIVE (5) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
- THE CONTRACTOR SHALL PROVIDE WASTE RECEPTACLES THROUGHOUT THE WORK ZONE AND MAINTAIN SANITARY FACILITIES FOR EMPLOYEES TO USE. FACILITIES WITHIN THE HANGARS/AIRPORT BUILDINGS SHALL NOT BE USED.
- THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE A STONE DELIVERY DUMP TRUCK, WHICH HAS A MAXIMUM HEIGHT OF TWENTY-FIVE (25) FEET IN A DUMP POSITION.
- ALL EXISTING ROADS USED AS A HAUL ROAD BY THE CONTRACTOR SHALL BE RESTORED TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT DIRECTOR THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING HAUL ROADS SHALL BE INCIDENTAL TO THE CONTRACT.
- WEEKLY JOBSITE MEETINGS SHALL BE HELD TO COORDINATE THE WORK, PARTICULARLY PERTAINING TO ANY ACTIVITIES WHICH MAY IMPACT OR INTERFERE WITH OTHER CONTRACTORS AND AIRPORT OPERATIONS/TENANTS.
- OPEN TRENCHES, EXCAVATIONS AND STOCKPILED MATERIAL AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH ORANGE FLAGS AND LIGHTED WITH TYPE II BARRICADES HAVING FLASHING RED LIGHT UNITS DURING THE HOURS OF RESTRICTED VISIBILITY AND/OR DARKNESS.
- DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO WORKSITE.
- COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. SEE SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS.
- THE CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS BY OTHER CONTRACTORS. (SEE SPECIAL PROVISIONS SECTION 30-05).
- THE CONTRACTOR SHALL COORDINATE WORK ON ALL CONCURRENT PROJECTS WHICH MAY ARISE. NO CLAIMS FOR ADDITIONAL COMPENSATION FOR ADDITIONAL COORDINATION OR CHANGES IN MAINTENANCE OF TRAFFIC OR WORK CONFLICTS WILL BE CONSIDERED. (SEE SPECIAL PROVISIONS SECTION 30-05).
- CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON (FLASHING YELLOW) LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION.
- IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY THE AIRPORT DIRECTOR AND THE ENGINEER IMMEDIATELY.
- CONTRACTOR WILL BE REQUIRED TO MAINTAIN ACCESS (ONE LANE) TO ASPEN AIR HANGAR AND PERIMETER ROAD AT ALL TIMES.
- THE CONTRACTOR SHALL PLACE TEMPORARY CONSTRUCTION FENCING APPROVED BY THE AIRPORT AND ENGINEER ACROSS RELOCATED 6' CLASS E FENCE SECTION UNTIL WORK IS COMPLETED. (COST INCIDENTAL TO RELOCATE 6' CLASS E FENCE). THE AIRPORT SHALL SUPPLY THE CONTRACTOR WITH ADDITIONAL FABRIC NEEDED TO CROSS THE DITCH.
- ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTACT WITH THE CONTROL TOWER SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE. THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES AND DEMONSTRATE KNOWLEDGE OF SAME TO THE AIRPORT AUTHORITY. THE AIRPORT AUTHORITY RESERVES THE RIGHT TO APPROVE THE CROSSING GUARDS. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS. THIS COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF MUNICIPAL FINES (\$500 PER OCCURENCE) DUE TO AIRFIELD INCURSIONS BY HIS EMPLOYEES, SUBCONTRACTORS, SUPPLIERS, CONSULTANTS AND/OR AGENTS.
- ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT DIRECTOR AT NO ADDITIONAL COST TO THE OWNER.
- CONSTRUCTION WILL BE ALLOWED UP TO THE EDGE OF THE TAXIWAY PAVEMENTS WITHOUT CLOSURE ON A LIMITED BASIS. WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND DIRECTOR OF OPERATIONS TWO (2) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.
- AT TIMES WHEN THE TAXIWAYS ARE REQUIRED TO BE CLOSED THE CONTRACTOR SHALL PLACE TEMPORARY BARRICADES AS SHOWN. AT THE END OF EACH WORKING DAY THE TAXIWAY SHALL BE REOPENED. THE COST OF REMOVING AND REPLACING BARRICADES IS INCIDENTAL.

**CONTRACTOR CROSSING TAXIWAY AND WORK WITHIN AIR OPERATIONS AREA (A.O.A.) (IF NECESSARY)**



**CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG**  
NOT TO SCALE



CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE SPECIAL PROVISIONS SECTION 30-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT:

- TAXIWAY E WIDENING, REHABILITATION AND OVERLAY.
- HANGAR CONSTRUCTION
- REHABILITATION OF TAXIWAY A & C
- SOUTH FLIGHT CENTER APRON - PHASE 4

**DESIGN AIRCRAFT APPROACH CATEGORY: D**  
**DESIGN AIRCRAFT GROUP: III**  
CLOSEST POINT ON CONSTRUCTION SITE TO RUNWAY 2L/20R  
POINT "A"  
LATITUDE: 41°54'11.78" N. (NAD 83)  
LONGITUDE: 88°15'21.14" W.  
ELEVATION: 754.50

**NOTE**  
ALL EXISTING TAXIWAY LIGHTING CIRCUITS, BEACON CIRCUIT, FAA CABLES, VAULT EQUIPMENT AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS DIRECTED BY THE ENGINEER. ALL NECESSARY TEMPORARY CABLING, JUMPERS AND SPLICING SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.

- LEGEND**
- CONTRACTORS ACCESS TO SITE
  - TYPE II BARRICADES W/ FLASHING RED LIGHTS
  - TEMPORARY TYPE II BARRICADES W/FLASHING RED LIGHTS (WHEN REQUIRED)(SEE NOTE 25)
  - NEW ROADWAY CONSTRUCTION
  - AIRCRAFT MOVEMENT AREA
  - TAXIWAY AIR OPERATIONS AREA (AOA)
  - WORK BY OTHERS

- SUGGESTED SEQUENCE OF CONSTRUCTION**
- PLACE BARRICADES AND EROSION CONTROL DEVICES.
  - INSTALL NEW STORM SEWER AND BOX CULVERT.
  - CONSTRUCT ROADWAY EXTENSION EMBANKMENT AND CUT.
  - INSTALL ELECTRICAL DUCTS AS SHOWN.
  - INSTALL UNDERDRAIN AS SHOWN.
  - CONSTRUCT ROADWAY EXTENSION CURB AND GUTTER AS SHOWN.
  - CONSTRUCT ROADWAY EXTENSION AGGREGATE BASE AND FREEDOM DRIVE AGGREGATE SHOULDERS AS SHOWN.
  - CONSTRUCT ASPHALT PAVEMENTS AS SHOWN.
  - INSTALL LIGHTING.
  - PLACE TOPSOIL SEED AND MULCH.
  - INSTALL SIGNS AND MARKINGS AS SHOWN.
  - REMOVE BARRICADES AND EROSION CONTROL DEVICES.

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LAYOUT: Layout1  
UPDATE BY: jilike  
SURVEY BOOK #  
DATE: Tue 3/7/06 10:27am  
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roadbase.dwg  
tb.dwg

**REVISIONS**

NUMBER	BY	DATE

0 1 2  
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34x22).

**DUPAGE AIRPORT  
WEST CHICAGO, ILLINOIS  
ROADWAY IMPROVEMENTS FOR SOUTH FLIGHT CENTER  
DEVELOPMENT - PHASE 1**

**GENERAL NOTES/  
SEQUENCE OF CONSTRUCTION  
PER AC 150/5370-2E**

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Dupage Airport

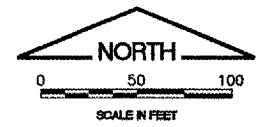
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DATE: 03/03/06  
JOB No: 05257-02

REVISIONS		
NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

EXISTING SOUTH FLIGHT CENTER APRON

ASPEN AIR HANGAR



FREEDOM DRIVE

AIRPORT PERIMETER ROAD

NEW SOUTH FLIGHT CENTER APRON  
 PHASE 4  
 (BY OTHERS)

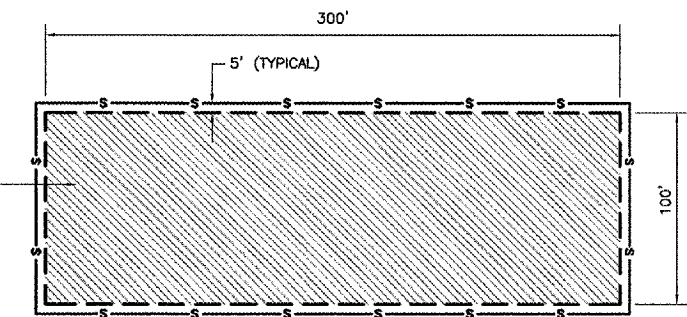
**LEGEND**

- NEW LIMITS OF SEEDING AND 10' WIDE SOIL GUARD
- NEW LIMITS OF SEED ONLY
- NEW SILT FENCE
- NEW BALE INLET PROTECTION
- NEW BALE DITCH CHECK
- EXISTING FENCE
- NEW FENCE (BY OTHERS)
- RELOCATE 6' CLASS E FENCE
- LIMITS OF GRADING
- FLOWLINE

**NOTES FOR EROSION CONTROL**

1. BALES SHALL BE PLACED AROUND ALL STORM SEWER INLETS AS DETAILED ON THE PLANS TO MINIMIZE SOIL INTRUSION INTO THE STORM SEWER SYSTEM. (SEE DETAILS SHEET 7)
2. THE CONTRACTOR SHALL BE REQUIRED TO COMPLY WITH THE REQUIREMENTS OF THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) STORM WATER PERMIT FOR CONSTRUCTION SITE ACTIVITIES.
3. ALL PROJECT AREAS, INCLUDING STOCKPILES, ABANDONED HAUL ROADS AND STAGING AREAS, AS SHOWN ON THE PLANS, SHALL HAVE 4 INCHES OF TOPSOIL PLACED AND BE SEEDED AND SOIL GUARDED IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS. AREAS DISTURBED OUTSIDE THE PROJECT LIMITS WILL BE SEEDED AND SOIL GUARDED BY THE CONTRACTOR AT HIS COST AND RESTORED TO ORIGINAL CONDITIONS.
4. FOR DETAILS, SEE STORMWATER POLLUTION PREVENTION NOTES AND DETAILS SHEET.
5. FOR EXACT LOCATIONS OF INLETS, SEE EXISTING CONDITIONS/PROPOSED REMOVALS SHEET, PLAN AND PROFILES AND DRAINAGE SCHEDULE.
6. SEDIMENT AND EROSION CONTROL MEASURES SHALL BE INSPECTED ON A WEEKLY BASIS AND AFTER EACH 1/2" RAIN EVENT.
7. THE CONTRACTOR SHALL HAVE A COPY OF THE APPROVED EROSION AND SEDIMENT CONTROL PLAN ON THE SITE AT ALL TIMES.
8. DURING DEWATERING OPERATION, WATER SHALL BE PUMPED INTO SEDIMENT BASINS OR SILT TRAPS.
9. FOR ADDITIONAL INFORMATION ON LANDSCAPING LIMITS SEE CROSS SECTIONS.
10. ANY FIELD/DRAIN TILES THAT ARE DAMAGED IN ANY WAY SHALL BE IMMEDIATELY REPAIRED ACCORDING TO THE PLANS/SPECIFICATIONS AND SHALL BE PROTECTED FROM SEDIMENT-LADEN WATER.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLATION OF ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AROUND STOCKPILES, STORAGE AREAS AND ANY OTHER AREAS PER THE CITY OF WEST CHICAGO OR ENGINEER.
12. THE CONTRACTOR SHALL PLACE SILT FENCE AROUND SURPLUS EMBANKMENT FILL LOCATION, SEED AND GRADE AREA TO DRAIN.

SURPLUS EMBANKMENT FILL LOCATION  
 (SEE NOTE 12)



**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 ROADWAY IMPROVEMENTS FOR SOUTH FLIGHT CENTER  
 DEVELOPMENT - PHASE 1  
 STORM WATER POLLUTION  
 PREVENTION PLAN**

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DRAWN BY:	JRO
CHECKED BY:	JRL
APPROVED BY:	DLP
DATE:	03/03/06
JOB No:	05257-02



# STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

## SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF CONSTRUCTING A 500' EXTENSION OF DUPAGE DRIVE AND BITUMINOUS OVERLAY OF FREEDOM DRIVE AT THE DUPAGE AIRPORT. THE PROJECT INCLUDES EXCAVATION, EMBANKMENT, DRAINAGE, VARIOUS PAVEMENT ITEMS, FENCING, ELECTRICAL IMPROVEMENTS AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS EXCAVATION AND GRADING:

- EXCAVATION AND EMBANKMENT WILL BE COMPLETED WITHIN THE PROJECT LIMITS TO GRADE OUT FOR THE PROPOSED DRAINAGE AND PAVEMENT IMPROVEMENTS.
- UNDERDRAIN INSTALLATION AND MANHOLE ADJUSTMENTS.
- PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL, SUCH AS PERIMETER SILT FENCE AND INLET PROTECTION.
- PAVEMENT CONSTRUCTION.
- FENCING AND ELECTRICAL IMPROVEMENTS.
- FINAL GRADING AND OTHER MISCELLANEOUS ITEMS.
- PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS SEEDING AND MULCHING.

## AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 3.0 ACRES OF WHICH 1.5 ACRES WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES.

## OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

- INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
- PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

## DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE KRESS CREEK THROUGH A STORM SEWER SYSTEM.

## CONTROLS-EROSION CONTROLS AND SEDIMENT CONTROL

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION

- THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS POSSIBLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, INLET PROTECTION AND PERIMETER SILT FENCE SHALL BE INSTALLED AS CALLED OUT IN THE PLANS OR AS DIRECTED BY THE ENGINEER.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER 1LR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

## DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

- WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
- EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN THREE DAYS. STOCKPILES SHALL NOT BE LOCATED IN SPECIAL MANAGEMENT AREAS.
- AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:
  - PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
  - CONSTRUCT DITCHES AND PROVIDE TEMPORARY EROSION CONTROL SYSTEMS.
  - BUILD NECESSARY EMBANKMENT AT CULVERT/STORM SEWER LOCATIONS AND THEN EXCAVATE AND PLACE PIPE.
  - EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.
  - ANY WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION SHALL BE FILTERED.

- CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

- THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT DAILY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2 INCH OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD.

- SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND EROSION CONTROL ITEMS.

- THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

## DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

## MAINTENANCE AFTER CONSTRUCTION

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

## CONTRACTORS

- THE STORM WATER POLLUTION PREVENTION PLAN MUST CLEARLY IDENTIFY FOR EACH MEASURE IDENTIFIED IN THE PLAN, THE CONTRACTOR(S) OR SUBCONTRACTOR(S) THAT WILL IMPLEMENT THE MEASURE. ALL CONTRACTORS AND SUBCONTRACTORS IDENTIFIED IN THE PLAN MUST SIGN A COPY OF THE CERTIFICATION STATEMENT IN PARAGRAPH 2 BELOW IN ACCORDANCE WITH PART VI.G (SIGNATORY REQUIREMENTS) OF THIS PERMIT. ALL CERTIFICATIONS MUST BE INCLUDED IN THE STORM WATER POLLUTION PREVENTION PLAN EXCEPT FOR OWNERS THAT ARE ACTING AS CONTRACTOR.
- CERTIFICATION STATEMENT. ALL CONTRACTORS AND SUBCONTRACTORS IDENTIFIED IN A STORM WATER POLLUTION PREVENTION PLAN IN ACCORDANCE WITH PARAGRAPH 1 ABOVE SHALL SIGN A COPY OF THE FOLLOWING CERTIFICATION STATEMENT BEFORE CONDUCTING ANY PROFESSIONAL SERVICE AT THE SITE IDENTIFIED IN THE STORM WATER POLLUTION PREVENTION PLAN:

"I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (1LR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION."

THE CERTIFICATION MUST INCLUDE THE NAME AND TITLE OF THE PERSON PROVIDING THE SIGNATURE IN ACCORDANCE WITH PART VI.G OF THIS PERMIT: THE NAME, ADDRESS AND TELEPHONE NUMBER OF THE CONTRACTING FIRM; THE ADDRESS (OR OTHER IDENTIFYING DESCRIPTION) OF THE SITE; AND THE DATE THE CERTIFICATION IS MADE.

## CONTRACTOR CERTIFICATION

"I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (1LR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION."

## GENERAL CONTRACTOR

SIGNATURE \_\_\_\_\_ TITLE \_\_\_\_\_ DATE \_\_\_\_\_  
 COMPANY \_\_\_\_\_



## ILLINOIS ENVIRONMENTAL PROTECTION AGENCY NOTICE OF INTENT (NOI) GENERAL PERMIT TO DISCHARGE STORM SEWER CONSTRUCTION SITE ACTIVITIES

IMPORTANT: FORM MUST BE TYPED TO ENABLE AUTOMATED OPTICAL PROCESSING.  
 SUBMIT ORIGINAL - DO NOT SUBMIT PHOTOCOPY

OWNER INFORMATION		OWNER TYPE (SELECT ONE AND TYPE "X")	
NAME: LAST FIRST MI. (SEE INSTRUCTIONS)	DUPAGE AIRPORT AUTHORITY	<input type="checkbox"/> PRIVATE	<input type="checkbox"/> COUNTY <input type="checkbox"/> STATE
MAILING ADDRESS:	2700 INTERNATIONAL DRIVE, SUITE 200	<input type="checkbox"/> CITY <input checked="" type="checkbox"/> SPECIAL DISTRICT	
CITY:	WEST CHICAGO	<input type="checkbox"/> FEDERAL	
STATE:	IL	TELEPHONE NUMBER:	AREA CODE NUMBER
CONTACT PERSON:	BYRON MILLER	630	208-6172

CONTRACTOR INFORMATION		TELEPHONE NUMBER:		AREA CODE:		NUMBER:	
NAME: LAST FIRST MI. (SEE INSTRUCTIONS)							
MAILING ADDRESS:		CITY:		ST.:		ZIP:	

CONSTRUCTION SITE INFORMATION		OTHER NPDES PERMIT NUMBER (IF APPLICABLE):	
FACILITY NAME:	DUPAGE AIRPORT	N/A	
MAILING ADDRESS:	2700 INTERNATIONAL DRIVE, SUITE 200	TELEPHONE NUMBER:	AREA CODE NUMBER
CITY:	WEST CHICAGO	630	208-6172
STATE:	IL	ZIP:	60185
COUNTY:	DUPAGE	SECTION:	29 31 & 32
TOWNSHIP:	40 NORTH	RANGE:	9 EAST
START CONSTRUCTION DATE:	MM/DD/YY	END CONSTRUCTION DATE:	MM/DD/YY
DATE:		DATE:	

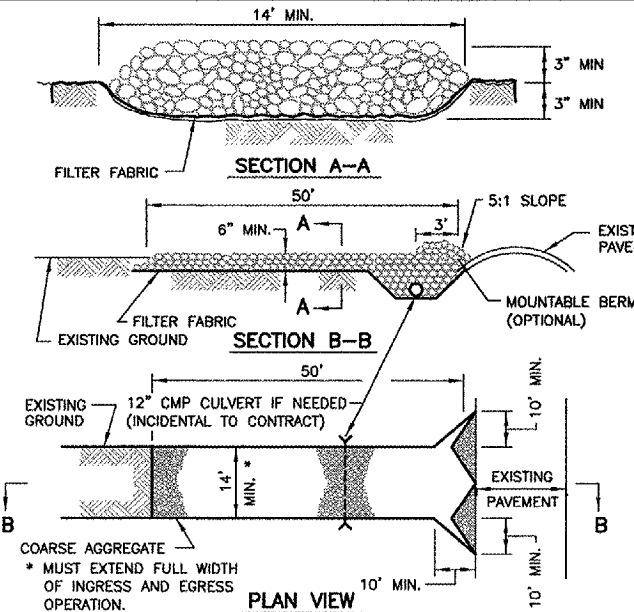
TYPE OF CONSTRUCTION	
<input type="checkbox"/> RESIDENTIAL	<input type="checkbox"/> COMMERCIAL
<input type="checkbox"/> INDUSTRIAL	<input type="checkbox"/> RECONSTRUCTION
<input checked="" type="checkbox"/> TRANSPORTATION	<input type="checkbox"/> OTHER

RECEIVING WATER INFORMATION	
DOES YOUR STORM WATER DISCHARGE DIRECTLY TO: (SELECT ONE AND TYPE "X")	
<input type="checkbox"/> WATER OF THE STATE	<input checked="" type="checkbox"/> STORM SEWER
NAME OF CLOSEST RECEIVING WATER (IF KNOWN):	KRESS CREEK
NAME OF STORM SEWER SYSTEM:	DUPAGE AIRPORT AUTHORITY
DOES THE QUANTITATIVE DATA CURRENTLY EXIST WHICH DESCRIBES THE CONCENTRATION OF POLLUTANTS IN THE STORM WATER DISCHARGES? <input type="checkbox"/> YES <input checked="" type="checkbox"/> NO	

"I certify under penalty of law that this document and all attachments were prepared under my direction and supervision in accordance with a system designed to assure that qualified personnel properly gather and evaluate the information submitted. Based on my inquiry of the person or persons who manage the system, or those persons directly responsible for gathering the information, the information submitted is, to the best of my knowledge and belief, true, accurate, and complete. I am aware that there are significant penalties for submitting false information, including the possibility of fine and imprisonment. In addition, I certify that the provisions of the permit, including the development and implementation of a Storm Water Pollution Prevention Plan and a Monitoring Program Plan, will be complied with."

MAIL COMPLETED FORM TO: (DO NOT SUBMIT ADDITIONAL DOCUMENTATION UNLESS REQUESTED)	ILLINOIS ENVIRONMENTAL PROTECTION AGENCY DIVISION OF WATER POLLUTION CONTROL ATTN: PERMIT SECTION 2200 CHEROKEE ROAD POST OFFICE BOX 19276 SPRINGFIELD, IL 62719-9276	LOG	FBI OFFICE USE ONLY
		PERMIT	
		DATE:	

This Agency is authorized to require this information under Illinois Revised Statute, 1991, Chapter 111 1/2, section 1030. Information is required under that Section. Failure to do so may prevent this form from being processed and could result in your application being denied. This form has been approved by the Forms Management Center.



- FILTER FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFIED FOR AR152540 IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF AERONAUTICS SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS.
- ROCK OR RECLAIMED CONCRETE SHALL MEET ONE OF THE FOLLOWING IDOT COARSE AGGREGATE GRADATION, CA-1, CA-2, CA-3 OR CA-4.
- ANY DRAINAGE FACILITIES REQUIRED BECAUSE OF WASHING SHALL BE CONSTRUCTED ACCORDING TO MANUFACTURERS SPECIFICATIONS AND SHALL BE INCIDENTAL TO THE CONTRACT.
- MINIMUM WIDTH IS 14' FOR ONE-WAY TRAFFIC AND 20' FOR TWO WAY TRAFFIC. TWO-WAY TRAFFIC WIDTHS SHALL BE INCREASED A MINIMUM OF 4' FOR TRAILER TRAFFIC, DEPENDING ON THE TYPE OF VEHICLE OR EQUIPMENT, SPEED, LOADS, CLIMATIC AND OTHER CONDITIONS UNDER WHICH VEHICLES AND EQUIPMENT OPERATE AN INCREASE IN THE MINIMUM WIDTHS MAY BE REQUIRED.
- ROADWAY SHALL FOLLOW THE CONTOUR OF THE NATURAL TERRAIN TO THE EXTENT POSSIBLE.
- CONTRACTOR SHALL REMOVE STABILIZED CONSTRUCTION ENTRANCE AND RESTORE AREA TO PRE-CONSTRUCTION CONDITIONS (INCIDENTAL TO CONTRACT).
- STABILIZED CONSTRUCTION ENTRANCE SHALL BE INCIDENTAL TO THE CONTRACT.

## STABILIZED CONSTRUCTION ENTRANCE

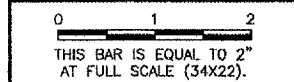
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## REVISIONS

NUMBER	BY	DATE



DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 ROADWAY IMPROVEMENTS FOR SOUTH FLIGHT CENTER  
 DEVELOPMENT - PHASE 1

STORM WATER POLLUTION  
 PREVENTION PLAN  
 NOTES

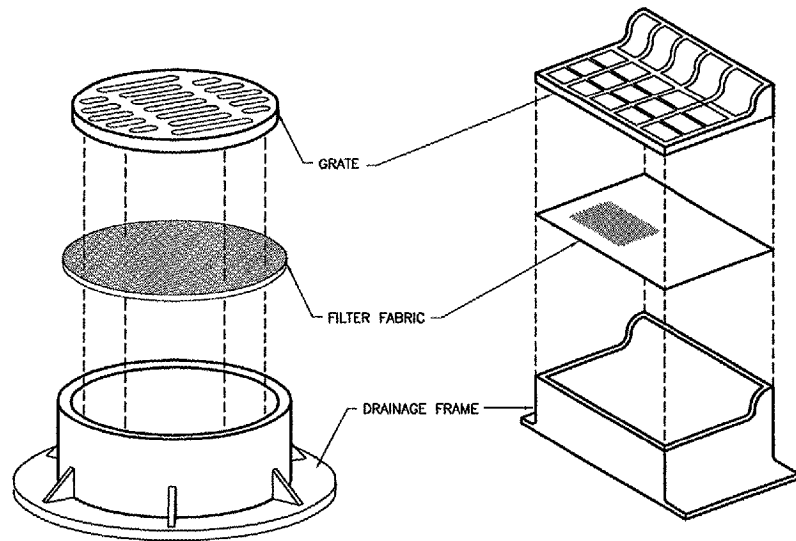
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DATE:	03/03/06
JOB No:	05257-02

ILLINOIS PROJECT: DPA-3325

SHEET 6 OF 21 SHEETS

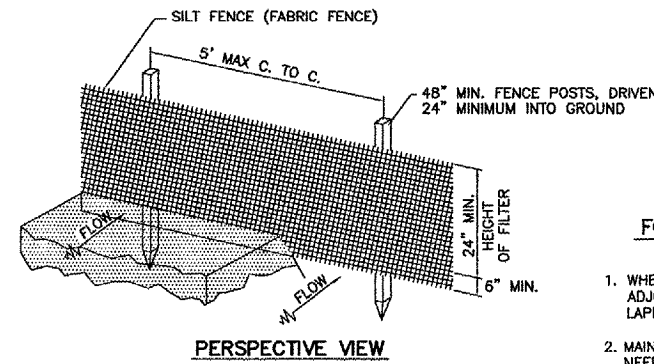


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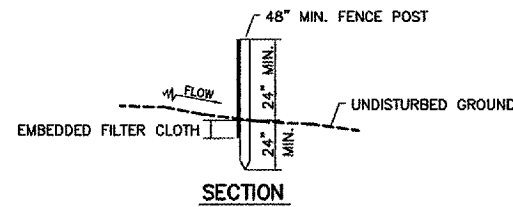
1. FILTER WRAP TO BE PLACED IN ALL SLOPE BOXES, INLETS, MANHOLES, TRENCH DRAINS AND CATCH BASINS LOCATED IN PAVED AREAS AND NONPAVED AREAS.
2. FABRIC SHALL BE IN CONFORMANCE WITH MATERIALS SPECIFIED FOR FABRIC FENCE.
3. FABRIC SHALL OVERLAY FRAME BY 2-INCH (MINIMUM).
4. CONTRACTOR SHALL CLEAR DEBRIS AND SILT AS REQUIRED FROM FABRIC TO MAINTAIN DRAINAGE THROUGH THE STRUCTURE.
5. FABRIC SHALL REMAIN IN PLACE UNTIL TURFED AREAS HAVE DEVELOPED A MINIMUM OF 80% OF COVERAGE.
6. COST OF FILTER WRAP SHALL BE CONSIDERED INCIDENTAL TO SILT FENCE.

**DRAINAGE STRUCTURE FILTER WRAP**

N.T.S.



**PERSPECTIVE VIEW**



**SECTION**

**CONSTRUCTION NOTES FOR SILT (FABRIC) FENCE**

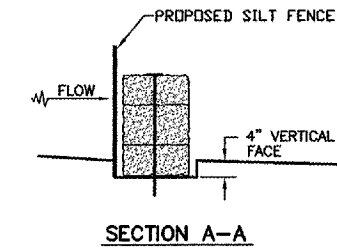
1. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED BY 6-INCH MIN. AND FOLDED.
2. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE, SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.
3. SILT FENCE SHALL BE INSTALLED PER STORM WATER POLLUTION PREVENTION PLAN OR AS DIRECTED BY THE ENGINEER.

**EROSION CONTROL FABRIC FENCE DETAIL**

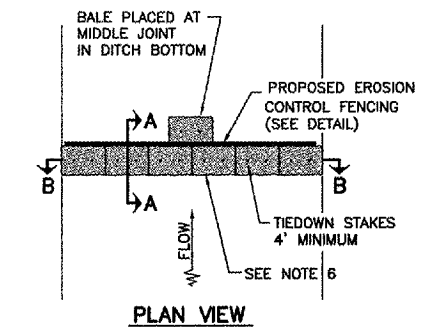
N.T.S.

**NOTES**

1. BALES SHALL BE PLACED AT THE TOE OF SLOPE OR ON A CONTOUR AND IN A ROW WITH ENDS TIGHTLY ABUTTING THE ADJACENT BALES.
2. EACH BALE SHALL BE EMBEDDED IN THE SOIL A MINIMUM OF 4 INCHES, AND PLACED SO THE BINDINGS ARE HORIZONTAL.
3. BALES SHALL BE SECURELY ANCHORED IN PLACE BY EITHER TWO STAKES OR REBARS DRIVEN THROUGH THE BALE. THE FIRST STAKE IN EACH BALE SHALL BE DRIVEN TOWARD THE PREVIOUSLY LAID BALE AT AN ANGLE TO FORCE THE BALES TOGETHER. STAKES SHALL BE DRIVEN FLUSH WITH THE BALE.
4. INSPECTION SHALL BE FREQUENT AND REPAIR / REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
5. BALES SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFULNESS SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE. COST OF REMOVAL / REPLACEMENT TO BE INCLUDED IN UNIT PRICE FOR BALES.
6. AFTER FINAL APPROVAL OF THE ENGINEER, STRAW BALES MAY BE REMOVED. CONTRACTOR SHALL PLACE SOD, EXCELSIOR BLANKET WITH SEED OR KNITTED STRAW MAT WITH SEED OVER THE DISTURBED AREAS. COST INCIDENTAL TO BALES.



**SECTION A-A**



**PLAN VIEW**

**HAY OR STRAW DITCH CHECK**

N.T.S.

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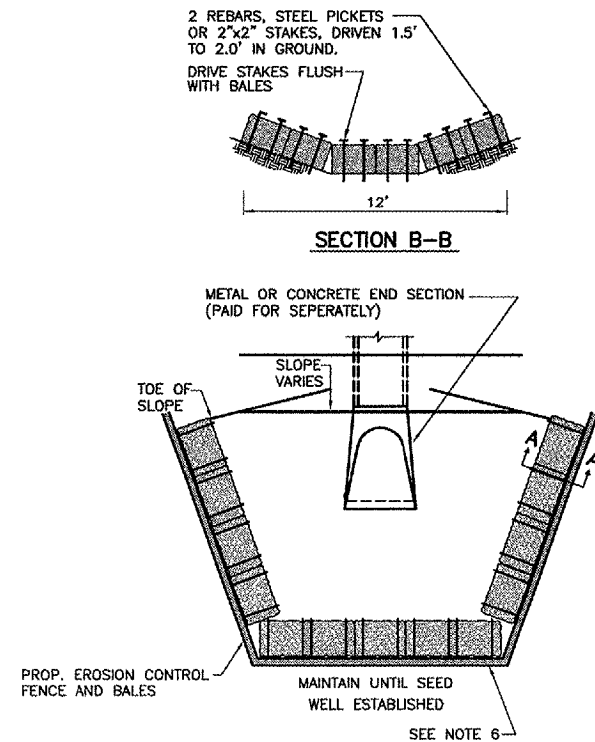
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THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

DUPAGE AIRPORT  
WEST CHICAGO, ILLINOIS  
ROADWAY IMPROVEMENTS FOR SOUTH FLIGHT CENTER  
DEVELOPMENT - PHASE 1

STORM WATER POLLUTION  
PREVENTION PLAN  
DETAILS

**NOTES**

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4. INSPECTION SHALL BE FREQUENT AND REPAIR / REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
5. BALES SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFULNESS SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE. COST OF REMOVAL / REPLACEMENT TO BE INCLUDED IN UNIT PRICE FOR BALES.
6. AFTER FINAL APPROVAL OF THE ENGINEER, STRAW BALES MAY BE REMOVED. CONTRACTOR SHALL PLACE SOD, EXCELSIOR BLANKET WITH SEED OR KNITTED STRAW MAT WITH SEED OVER THE DISTURBED AREAS. COST INCIDENTAL TO BALES.

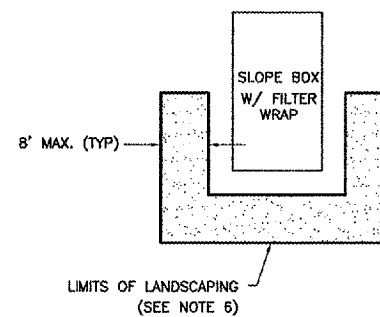


**INLET PROTECTION (END SECTION)**

N.T.S.

**NOTES**

1. BALES SHALL BE PLACED AT THE TOE OF SLOPE OR ON A CONTOUR AND IN A ROW WITH ENDS TIGHTLY ABUTTING THE ADJACENT BALES.
2. EACH BALE SHALL BE EMBEDDED IN THE SOIL A MINIMUM OF 4 INCHES, AND PLACED SO THE BINDINGS ARE HORIZONTAL.
3. BALES SHALL BE SECURELY ANCHORED IN PLACE BY EITHER TWO STAKES OR REBARS DRIVEN THROUGH THE BALE. THE FIRST STAKE IN EACH BALE SHALL BE DRIVEN TOWARD THE PREVIOUSLY LAID BALE AT AN ANGLE TO FORCE THE BALES TOGETHER. STAKES SHALL BE DRIVEN FLUSH WITH THE BALE.
4. INSPECTION SHALL BE FREQUENT AND REPAIR / REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
5. BALES SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFULNESS SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE. COST OF REMOVAL / REPLACEMENT TO BE INCLUDED IN UNIT PRICE FOR BALES.
6. AFTER FINAL APPROVAL OF THE ENGINEER, STRAW BALES MAY BE REMOVED. CONTRACTOR SHALL PLACE SOD, EXCELSIOR BLANKET WITH SEED OR KNITTED STRAW MAT WITH SEED OVER THE DISTURBED AREAS. COST INCIDENTAL TO BALES.

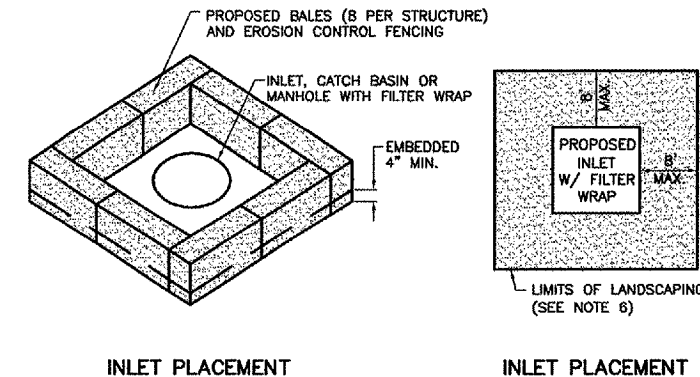


**INLET PROTECTION (SLOPE BOX)**

N.T.S.

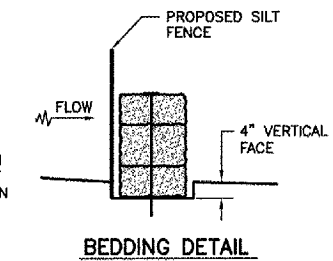
**NOTES**

1. BALES SHALL BE PLACED AT THE TOE OF SLOPE OR ON A CONTOUR AND IN A ROW WITH ENDS TIGHTLY ABUTTING THE ADJACENT BALES.
2. EACH BALE SHALL BE EMBEDDED IN THE SOIL A MINIMUM OF 4 INCHES, AND PLACED SO THE BINDINGS ARE HORIZONTAL.
3. BALES SHALL BE SECURELY ANCHORED IN PLACE BY EITHER TWO STAKES OR REBARS DRIVEN THROUGH THE BALE. THE FIRST STAKE IN EACH BALE SHALL BE DRIVEN TOWARD THE PREVIOUSLY LAID BALE AT AN ANGLE TO FORCE THE BALES TOGETHER. STAKES SHALL BE DRIVEN FLUSH WITH THE BALE.
4. INSPECTION SHALL BE FREQUENT AND REPAIR / REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
5. BALES SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFULNESS SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE. COST OF REMOVAL / REPLACEMENT TO BE INCLUDED IN UNIT PRICE FOR BALES.
6. AFTER FINAL APPROVAL OF THE ENGINEER, STRAW BALES MAY BE REMOVED. CONTRACTOR SHALL PLACE SOD AND MULCH OVER THE DISTURBED AREAS, COST INCIDENTAL TO BALES.



**INLET PLACEMENT**

**INLET PLACEMENT**



**BEDDING DETAIL**

**INLET PROTECTION (INLET/MANHOLES)**

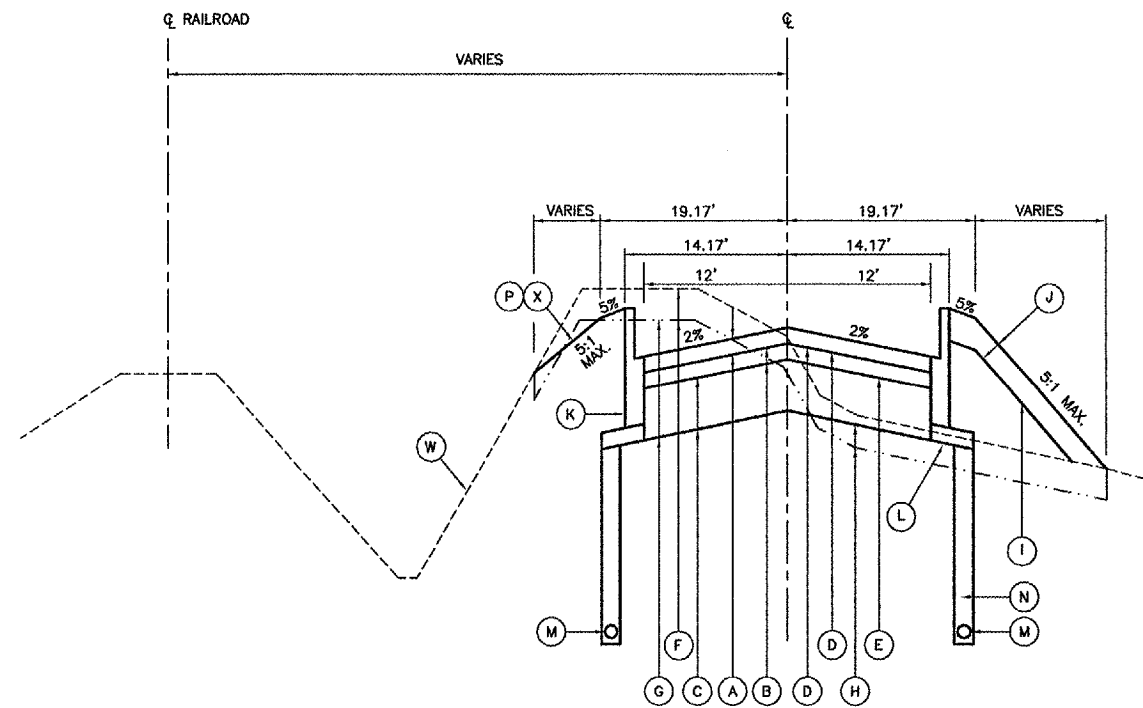
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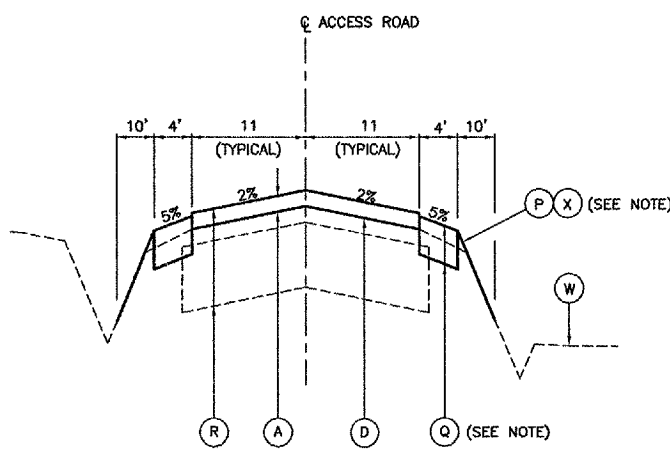
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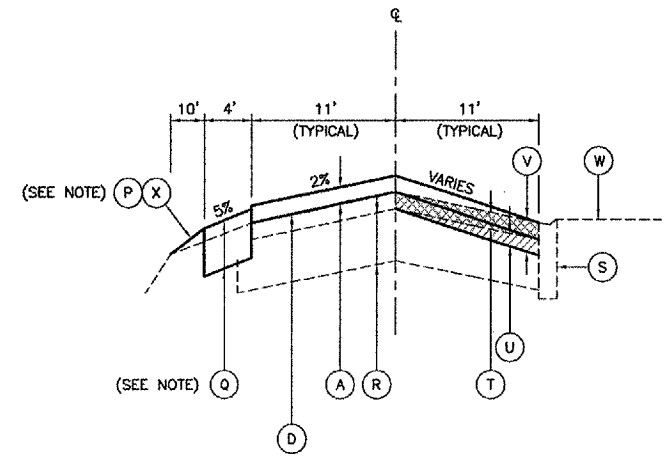


**SECTION A - A  
 DUPAGE DRIVE EXTENSION**  
 NOT TO SCALE



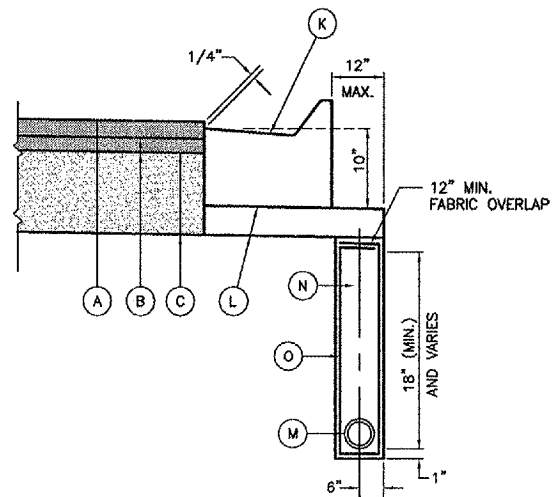
**SECTION B - B  
 FREEDOM DRIVE OVERLAY**  
 NOT TO SCALE

NOTE: EARTH EXCAVATION AND FILL FOR 4" WIDE CRUSHED AGGREGATE SHOULDER - 6" SHALL BE CONSIDERED INCIDENTAL.



**SECTION C - C  
 FREEDOM DRIVE OVERLAY**  
 NOT TO SCALE

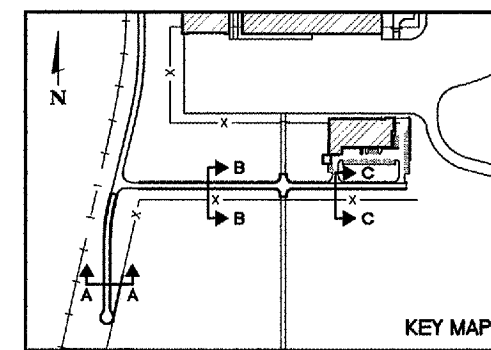
NOTE: EARTH EXCAVATION AND FILL FOR 4" WIDE CRUSHED AGGREGATE SHOULDER - 6" SHALL BE CONSIDERED INCIDENTAL.



**UNDERDRAIN/BACK OF CURB/  
 EDGE OF PAVEMENT DETAIL**  
 NOT TO SCALE

**LEGEND**

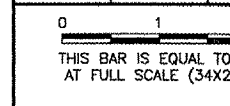
- (A) NEW 2" BITUMINOUS SURFACE COURSE (1 LIFT)(401)
- (B) NEW 2" BITUMINOUS BASE COURSE (1 LIFT)(201)
- (C) NEW 10" CRUSHED AGGREGATE BASE COURSE (2 LIFTS)(209)
- (D) NEW TACK COAT (603)
- (E) NEW PRIME COAT (602)
- (F) NEW 12" (AVG.) TOPSOIL STRIPPING (152)
- (G) NEW UNCLASSIFIED EXCAVATION (152)
- (H) NEW EMBANKMENT FILL (152)
- (I) NEW SHOULDER FILL (152)
- (J) NEW 4" MINIMUM TOPSOIL PLACEMENT (152) (INCIDENTAL TO UNCLASSIFIED EXCAVATION)
- (K) NEW M-6.18 COMBINATION CONCRETE CURB AND GUTTER (754)
- (L) NEW CRUSHED AGGREGATE BASE COURSE (209 MATERIAL) 4" MIN. THICKNESS (INCIDENTAL TO CURB)
- (M) NEW 6" PERFORATED UNDERDRAIN W/SOCK (705) (SOCK INCIDENTAL TO UNDERDRAIN)
- (N) NEW POROUS BACKFILL (705) (INCIDENTAL TO UNDERDRAIN)
- (O) NEW UNDERDRAIN TRENCH FABRIC ENVELOPE (705) (INCIDENTAL TO UNDERDRAIN)
- (P) NEW SEEDING (901) AND SOIL GUARD (800)
- (Q) NEW 4" WIDE CRUSHED AGGREGATE SHOULDER - 6" (209)
- (R) EXISTING 2" BITUMINOUS BASE ON 10" CRUSHED AGGREGATE BASE.
- (S) EXISTING DEPRESSED CURB AND GUTTER DRIVE ENTRANCE
- (T) REMOVE BITUMINOUS PAVEMENT (2"-4")
- (U) EXISTING CRUSHED AGGREGATE BASE COURSE REMOVAL (COST INCIDENTAL TO AR401900)
- (V) NEW 4" BITUMINOUS SURFACE COURSE (2 LIFTS)(401)
- (W) EXISTING GROUNDLINE
- (X) NEW GROUNDLINE (SEE NOTE)



**KEY MAP**

**REVISIONS**

NUMBER	BY	DATE



**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 ROADWAY IMPROVEMENTS FOR SOUTH FLIGHT CENTER  
 DEVELOPMENT - PHASE 1**

**TYPICAL SECTIONS**

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0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).

**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 ROADWAY IMPROVEMENTS FOR SOUTH FLIGHT CENTER  
 DEVELOPMENT - PHASE 1**

**PLAN AND PROFILE  
 FREEDOM DRIVE - SHEET 1**

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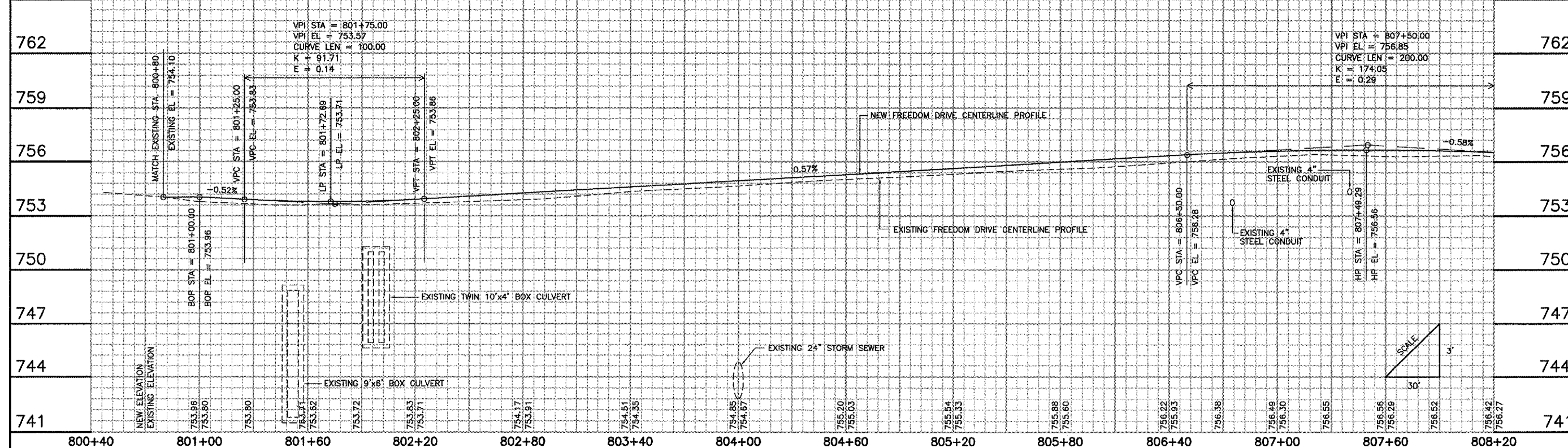
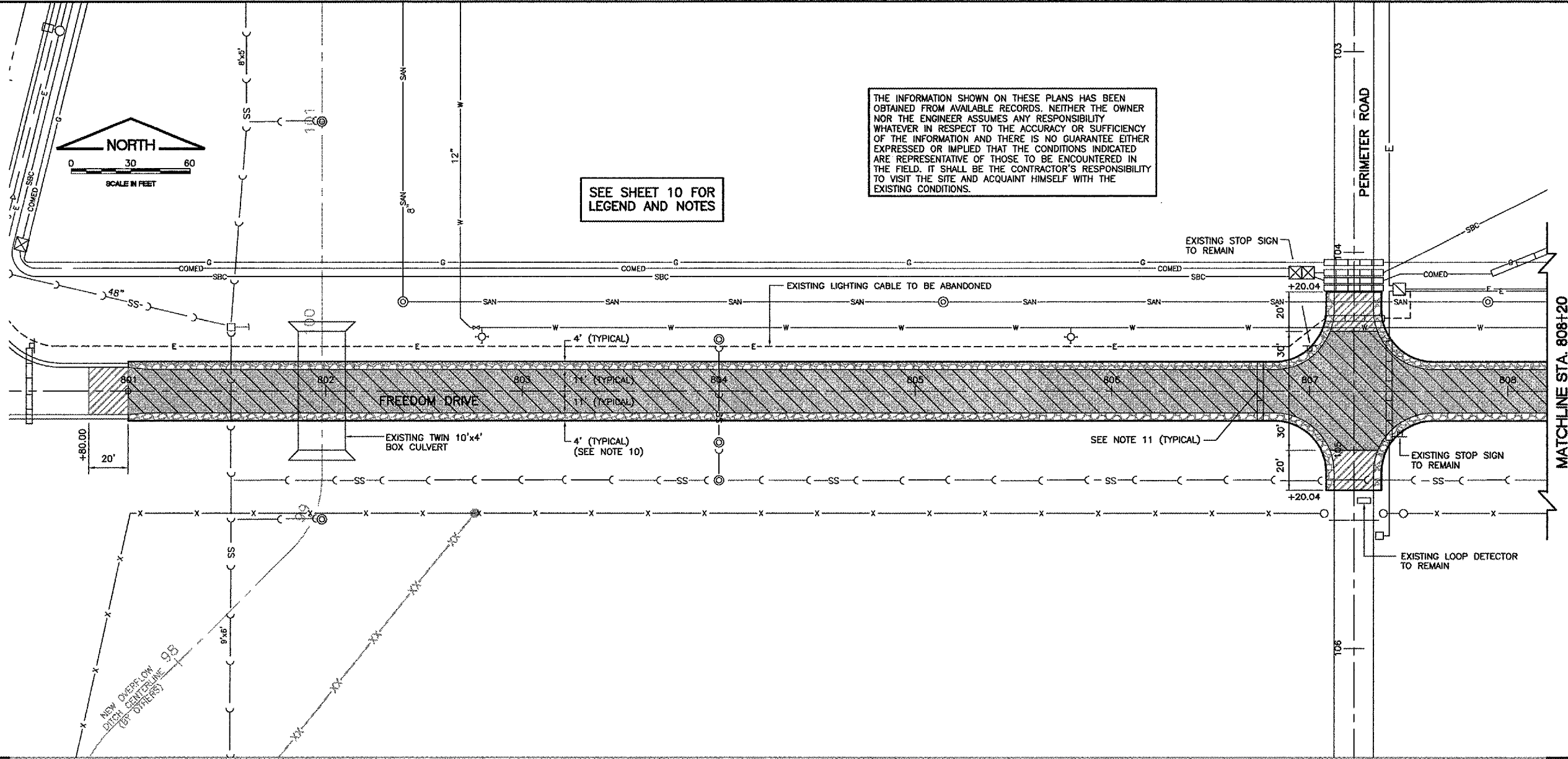
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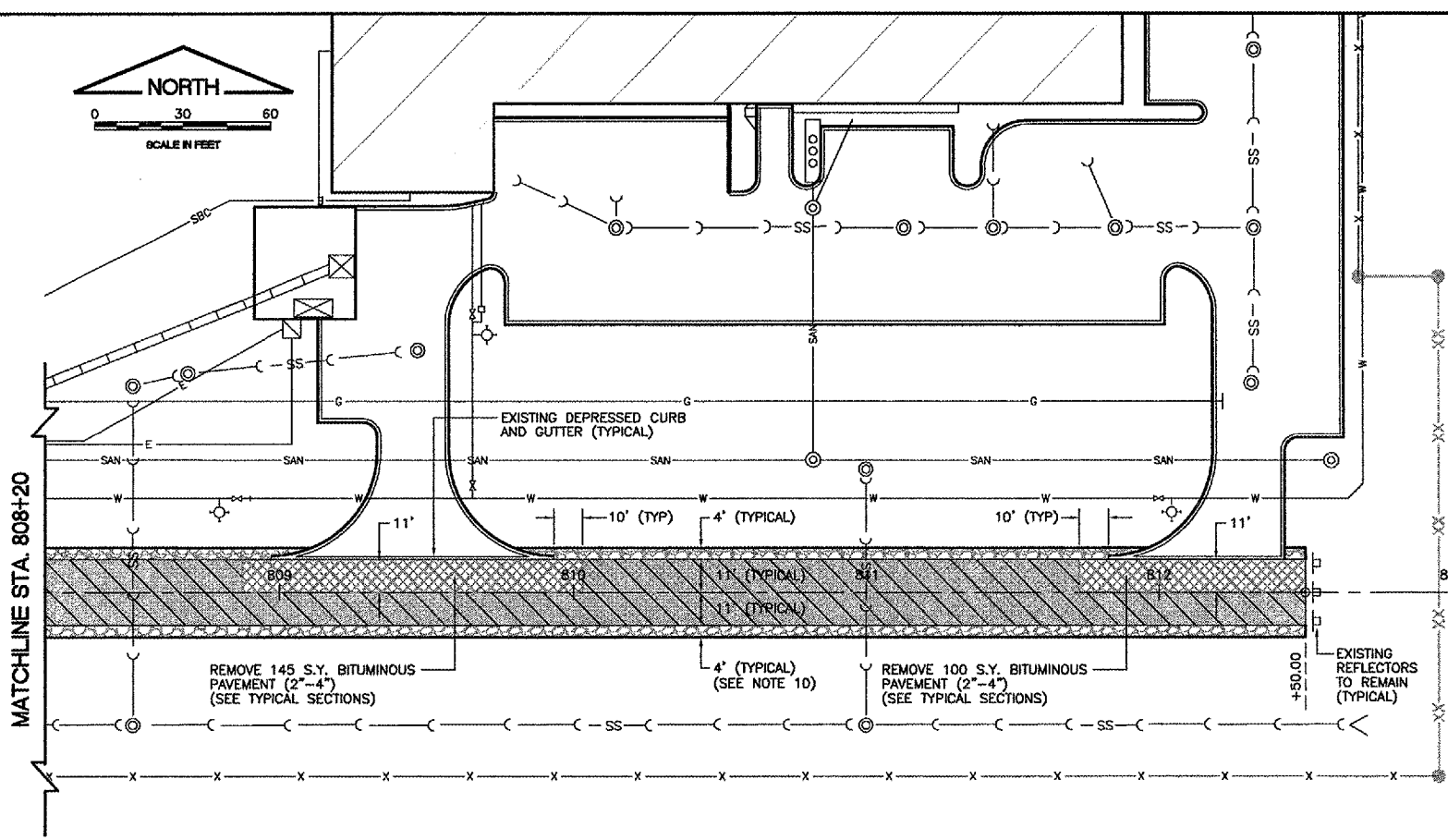
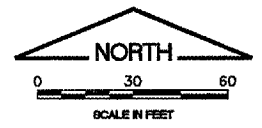
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SEE SHEET 10 FOR  
 LEGEND AND NOTES





**NOTES**

1. THE CONTRACTOR SHALL PLACE TEMPORARY CONSTRUCTION FENCING APPROVED BY THE AIRPORT AND ENGINEER ACROSS RELOCATED 6' CLASS E FENCE SECTION UNTIL WORK IS COMPLETED. (COST INCIDENTAL TO RELOCATE 6' CLASS E FENCE). THE AIRPORT SHALL SUPPLY THE CONTRACTOR WITH ADDITIONAL FABRIC NEEDED TO CROSS THE DITCH.
2. THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND THE PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
3. SEE ENGINEERING INFORMATION SHEETS FOR BORING LOGS.
4. THE CONTRACTOR SHALL TURN OVER ALL REMOVED FENCE, SIGNS AND REFLECTORS TO THE AIRPORT UNLESS THE ENGINEER OR AIRPORT INFORMS THE CONTRACTOR TO DISPOSE OF OFF SITE (INCIDENTAL TO CONTRACT).
5. ALL UNDERDRAIN ENDS SHALL BE CAPPED (INCIDENTAL TO 6" UNDERDRAIN).
6. CONTRACTOR SHALL FIELD VERIFY EXISTING UNDERDRAIN INVERTS BEFORE INSTALLING NEW UNDERDRAIN CONNECTIONS.
7. INSTALL NEW ELECTRICAL DUCTS/CONDUITS TO BE CLEAR OF UNDERDRAIN, COST INCIDENTAL.
8. UNDERDRAIN CONFLICTS WITH EXISTING CONDITIONS SHALL BE RESOLVED AND COST SHALL BE INCIDENTAL TO UNDERDRAIN.
9. REMOVAL OF EXISTING STORM SEWER MANHOLE/INLET CONCRETE BENCHES, CORING AND CONCRETE COLLARS TO FACILITATE CONNECTIONS OF NEW STORM SEWER AND UNDERDRAIN PIPE SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE PIPE.
10. EARTH EXCAVATION AND FILL FOR 4' CRUSHED AGGREGATE SHOULDER 6" SHALL BE CONSIDERED INCIDENTAL.
11. THE CONTRACTOR SHALL LOCATE, REMOVE EXISTING DUCT MARKERS AND INSTALL THEM UPON COMPLETION OF THE BITUMINOUS PAVEMENT OVERLAY. REMOVAL AND INSTALLATION SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

**LEGEND**

- NEW BITUMINOUS PAVEMENT OVERLAY (2" AVG.)(401)
- NEW 4' WIDE CRUSHED AGGREGATE SHOULDER - 6" (209)(SEE NOTE 10)
- NEW BUTT JOINT CONSTRUCTION (401)
- REMOVE BITUMINOUS PAVEMENT (2"-4")(401)
- NEW DUPAGE DRIVE EXTENSION  
2" BITUMINOUS SURFACE COURSE (401)  
2" BITUMINOUS BASE COURSE (201)  
10" CRUSHED AGGREGATE BASE COURSE (209)
- NEW 6" THICK CRUSHED AGGREGATE TURNAROUND (209)
- NEW MANHOLE/INLET
- EXISTING FIRE HYDRANT
- EXISTING MANHOLE/INLET/FLARED END SECTION
- NEW STORM SEWER
- EXISTING STORM SEWER
- ITEM TO BE REMOVED
- EXISTING FENCE
- NEW CLASS E FENCE (BY OTHERS)
- RELOCATE 6' CLASS E FENCE
- EXISTING UNDERDRAIN
- EXISTING LIGHTING CABLE
- EXISTING LIGHTING CABLE TO BE ABANDONED
- EXISTING COMMUNICATIONS CABLE
- EXISTING COMED CABLE
- EXISTING GAS LINE
- NEW 6" PERFORATED UNDERDRAIN W/ SOCK
- NEW CONTOURS
- EXISTING CONTOURS
- NEW CONTOURS (BY OTHERS)
- EXISTING HANDHOLE
- EXISTING ELECTRICAL ENCLOSURE
- EXISTING WATER VALVE
- EXISTING SIGN

**DU072**

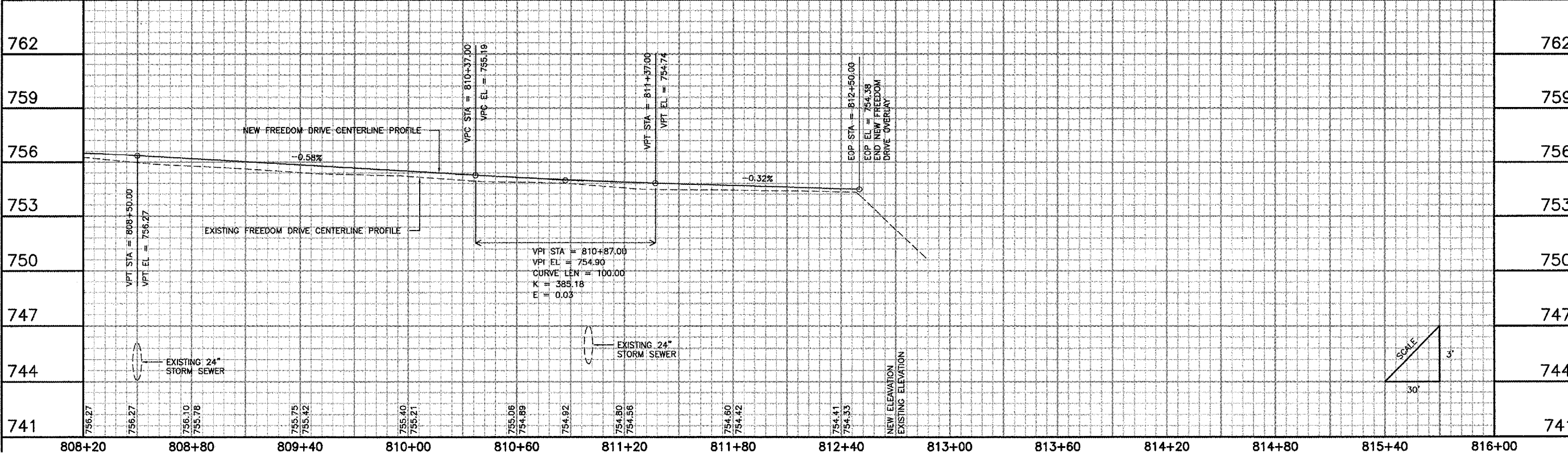
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**DUPAGE AIRPORT**  
**WEST CHICAGO, ILLINOIS**  
**ROADWAY IMPROVEMENTS FOR SOUTH FLIGHT CENTER DEVELOPMENT - PHASE 1**  
**PLAN AND PROFILE**  
**FREEDOM DRIVE - SHEET 2**



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JOB No:	05257-02
ILLINOIS PROJECT: DPA-3325	
SHEET 10 OF 21 SHEETS	



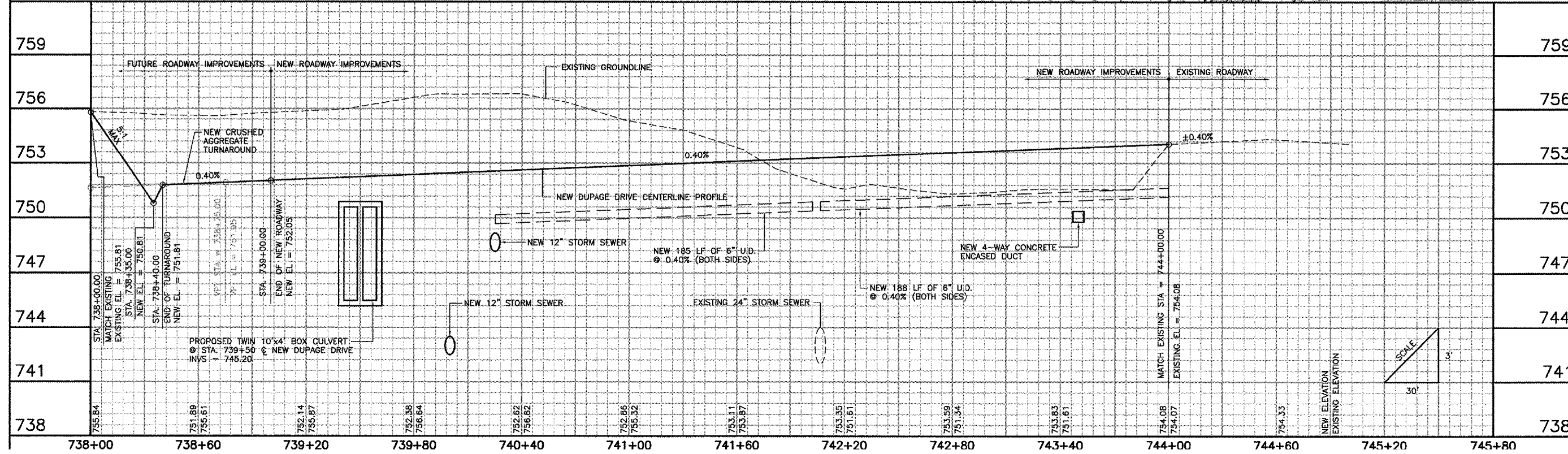
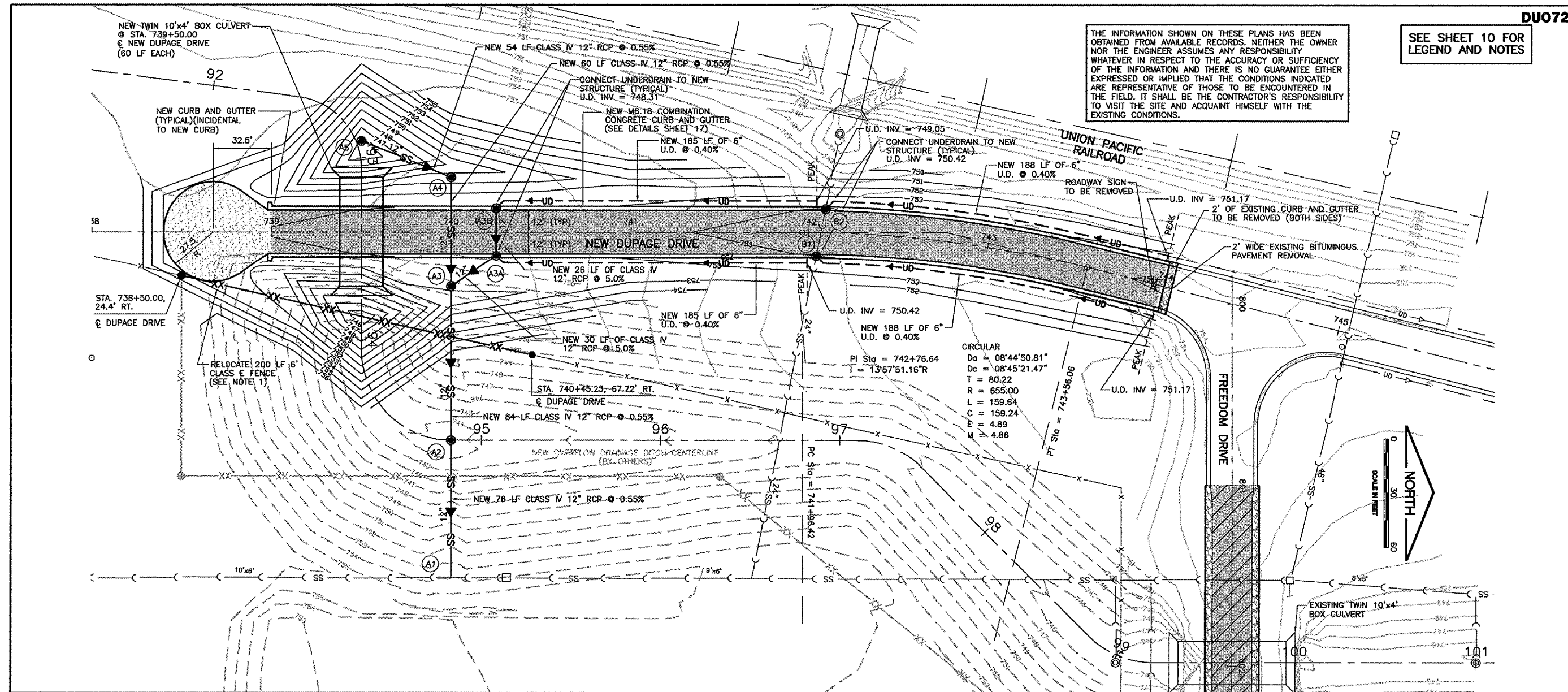
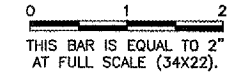
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SEE SHEET 10 FOR  
 LEGEND AND NOTES

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**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 ROADWAY IMPROVEMENTS FOR SOUTH FLIGHT CENTER  
 DEVELOPMENT - PHASE 1**

**PLAN AND PROFILE  
 DUPAGE DRIVE EXTENSION**

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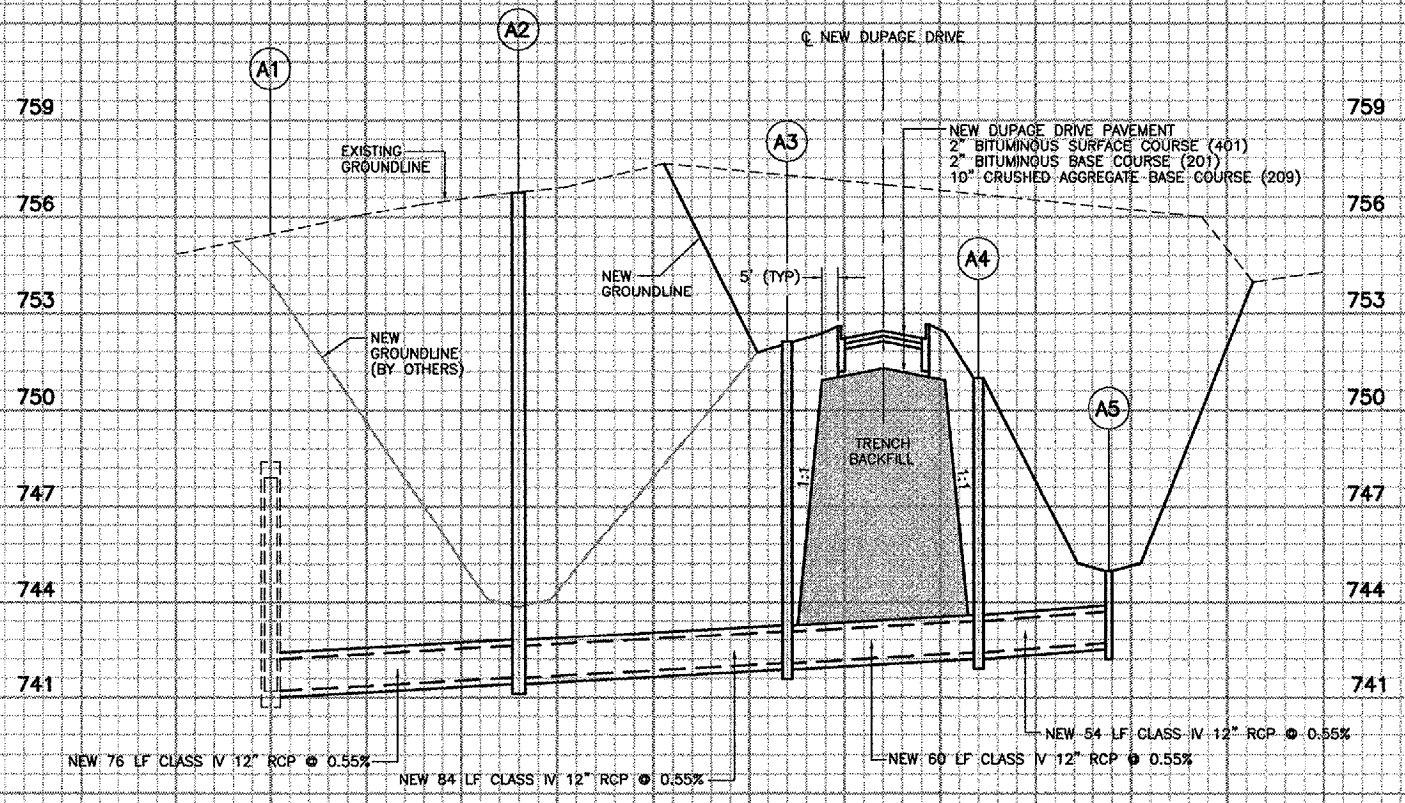
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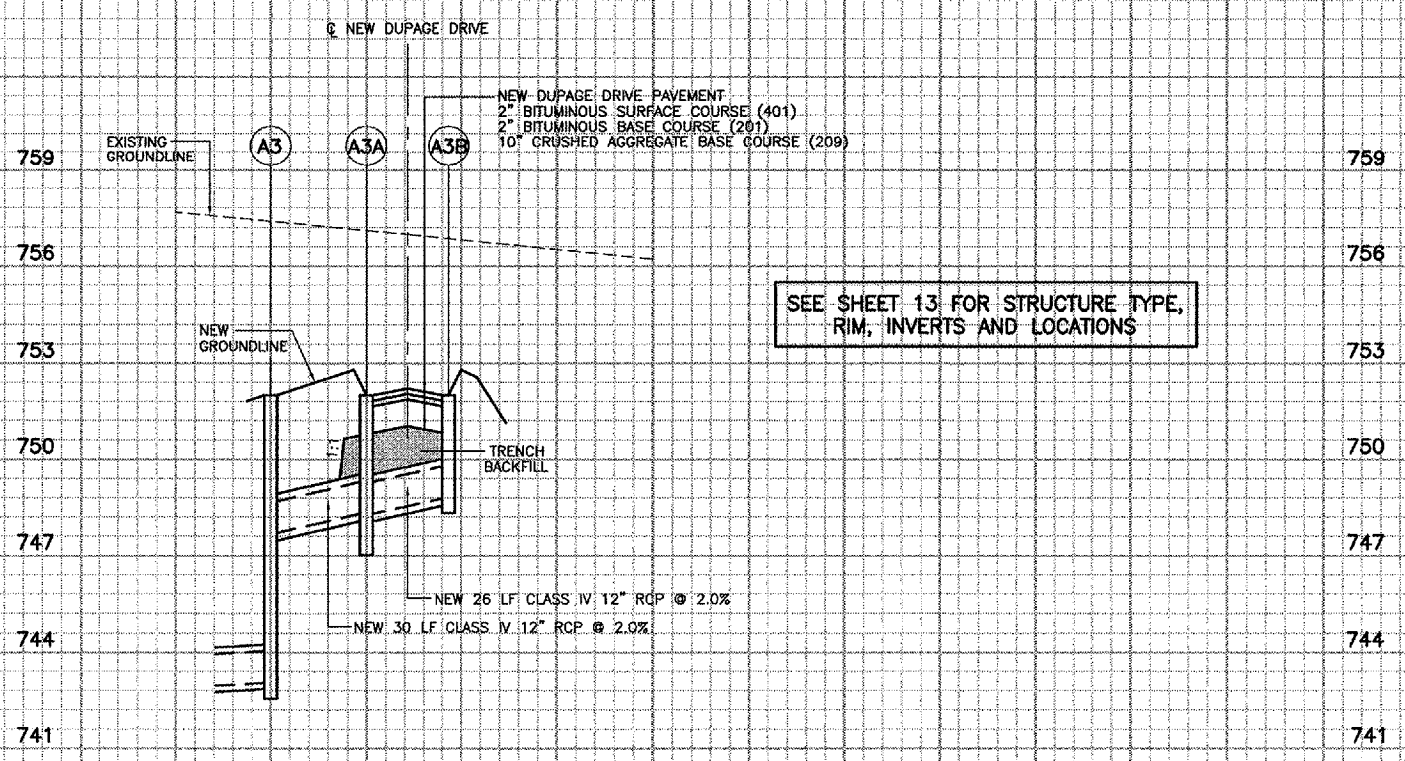
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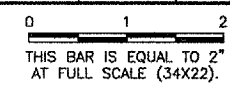


A1 TO A5



A3 TO A3B

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**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 ROADWAY IMPROVEMENTS FOR SOUTH FLIGHT CENTER  
 DEVELOPMENT - PHASE 1**

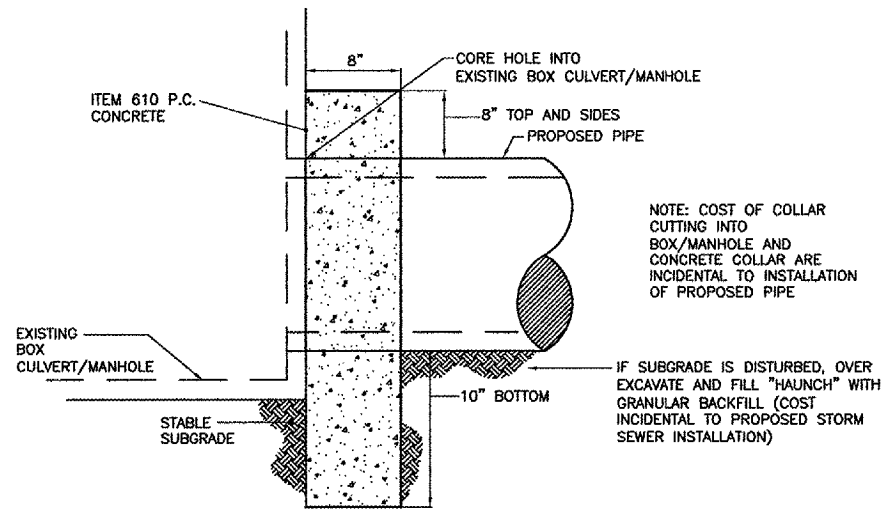
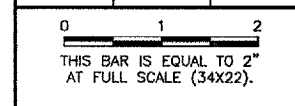
**STORM SEWER PROFILES**

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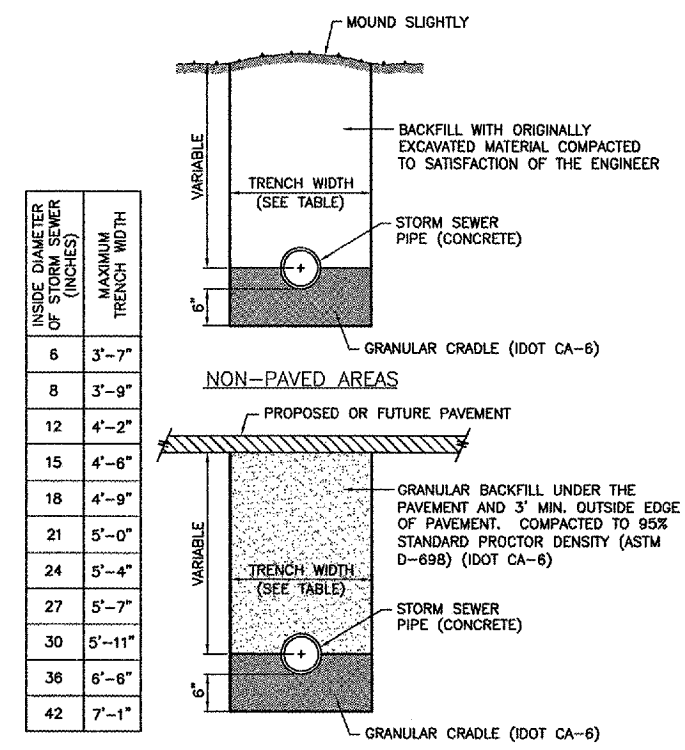


NOTE: COST OF COLLAR CUTTING INTO BOX/MANHOLE AND CONCRETE COLLAR ARE INCIDENTAL TO INSTALLATION OF PROPOSED PIPE

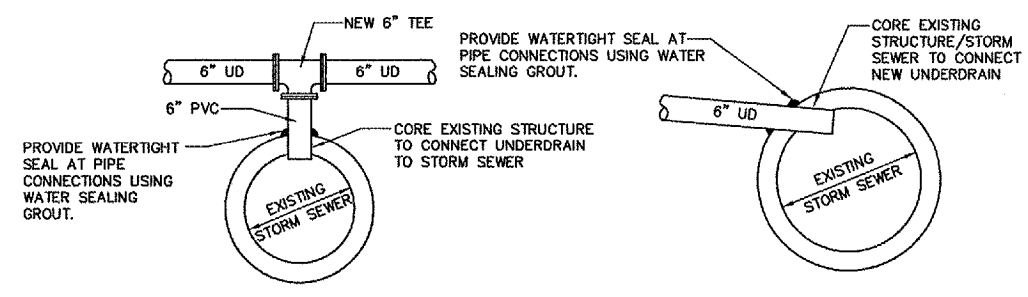
IF SUBGRADE IS DISTURBED, OVER EXCAVATE AND FILL "HAUNCH" WITH GRANULAR BACKFILL (COST INCIDENTAL TO PROPOSED STORM SEWER INSTALLATION)

**CONCRETE COLLAR - STORM SEWER**  
 NOT TO SCALE

NOTE: IF EXISTING STRUCTURE THAT IS BEING CORED INTO HAS A BENCH THE CONTRACTOR MUST CUT NEW FLOWLINE TO MATCH FLOWLINE OF NEW PIPE. (COST INCIDENTAL TO PROPOSED STORM SEWER INSTALLATION).



**TRENCH DETAILS - STORM SEWER AND WATERMAIN**  
 N.T.S.



**UNDERDRAIN CONNECTION DETAILS**  
 N.T.S.

NOTE: UNDERDRAIN CONNECTIONS AND FITTINGS, TEES AND ELBOWS USED FOR CONNECTIONS TO NEW STRUCTURES / EXISTING STORM SEWERS SHALL BE CONSIDERED INCIDENTAL TO THE NEW UNDERDRAIN.

**DRAINAGE SCHEDULE**

STRUCTURE	STATION/OFFSET	STRUCTURE	RIM	INVERT
A1	STA. 740+00, ±190.3' RT. CENTERLINE DUPAGE DRIVE	CONNECT TO EXISTING 9'x6' BOX CULVERT	N.A.	NEW 12" (E) = 741.20 EXISTING BOX INV = ±740.00
A2	STA. 740+00, 114.2' RT. CENTERLINE DUPAGE DRIVE	TYPE A-4 MANHOLE W/ NEENAH R-1706-1 FRAME AND OPEN LID	756.76	NEW 12" (E) = 741.62 NEW 12" (W) = 741.62
A3	STA. 740+00, 30' RT. CENTERLINE DUPAGE DRIVE	TYPE A-4 MANHOLE W/ TYPE 1 FRAME AND CLOSED LID	752.00	NEW 12" (E) = 742.08 NEW 12" (W) = 742.08 NEW 12" (NW) = 747.71
A3A	STA. 740+25, 13.1' LT. CENTERLINE DUPAGE DRIVE	TYPE B INLET W/ TYPE 12 FRAME AND GRATE	752.00	NEW 12" (SE) = 748.31 NEW 12" (W) = 748.31
A3B	STA. 740+25, 13.1' RT. CENTERLINE DUPAGE DRIVE	TYPE A INLET W/ TYPE 12 FRAME AND GRATE	752.00	NEW 12" (E) = 748.83
A4	STA. 740+00, 30' LT. CENTERLINE DUPAGE DRIVE	TYPE A-4 MANHOLE W/ TYPE 1 FRAME AND CLOSED LID	751.00	NEW 12" (E) = 742.41 NEW 12" (W) = 742.41
A5	STA. 739+50, 50.0' LT. CENTERLINE DUPAGE DRIVE	TYPE A INLET W/ TYPE 1 FRAME AND OPEN LID	745.00	NEW 12" (NE) = 742.71
B1	STA. 742+04, 13.1' LT. CENTERLINE DUPAGE DRIVE	TYPE A-4 MANHOLE W/ TYPE 12 FRAME AND GRATE	752.72	EXISTING 24" (SE) = ±741.97 EXISTING 24" (NW) = ±741.97
B2	STA. 742+09, 13.1' LT. CENTERLINE DUPAGE DRIVE	TYPE A-4 MANHOLE W/ TYPE 12 FRAME AND GRATE	752.72	EXISTING 24" (SE) = ±742.08 EXISTING 24" (NW) = ±742.08

- NOTES: 1. STATION AND OFFSETS ARE TO THE CENTER OF THE STRUCTURE.  
 2. CONTRACTOR TO VERIFY ALL EXISTING INVERTS PRIOR TO ORDERING AND INSTALLING MATERIALS.

**DUPAGE AIRPORT**  
**WEST CHICAGO, ILLINOIS**  
**ROADWAY IMPROVEMENTS FOR SOUTH FLIGHT CENTER DEVELOPMENT - PHASE 1**  
**STORM SEWER SCHEDULE AND DETAILS**

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NOTE:  
 DIMENSIONS ARE AT RIGHT ANGLES  
 TO  $\phi$  ROADWAY UNLESS NOTED  
 OTHERWISE.

**BENCH MARK**

**EXISTING STRUCTURE**

NONE

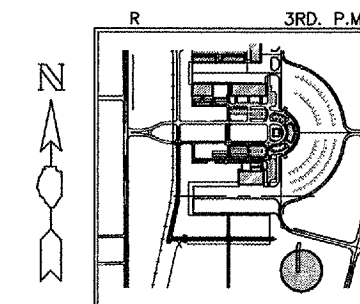
**PROPOSED STRUCTURE**

DOUBLE 10' x 4' PRECAST BOX  
 CULVERT WITH CAST-IN-PLACE  
 END SECTIONS.

DUPAGE TERMINAL OVERFLOW  
 DITCH CROSSING  
 BUILT 2006 BY  
 DUPAGE AIRPORT AUTHORITY  
 STATION 739+50.00  
 STR. NO. 101 LOADING HS 20

**NAME PLATE**

IDOT STANDARD  
 515001-02



LOCATION MAP

**DESIGN SPECIFICATIONS**

2002 AASHTO "STANDARD SPECIFICATIONS  
 FOR HIGHWAY BRIDGES" AND INTERIMS  
 THROUGH 2006.

**PRECAST UNITS**

$f'_c = 5,000$  psi  
 $f_y = 65,000$  psi  
 (WELDED WIRE FABRIC)

**LOADING HS 20**

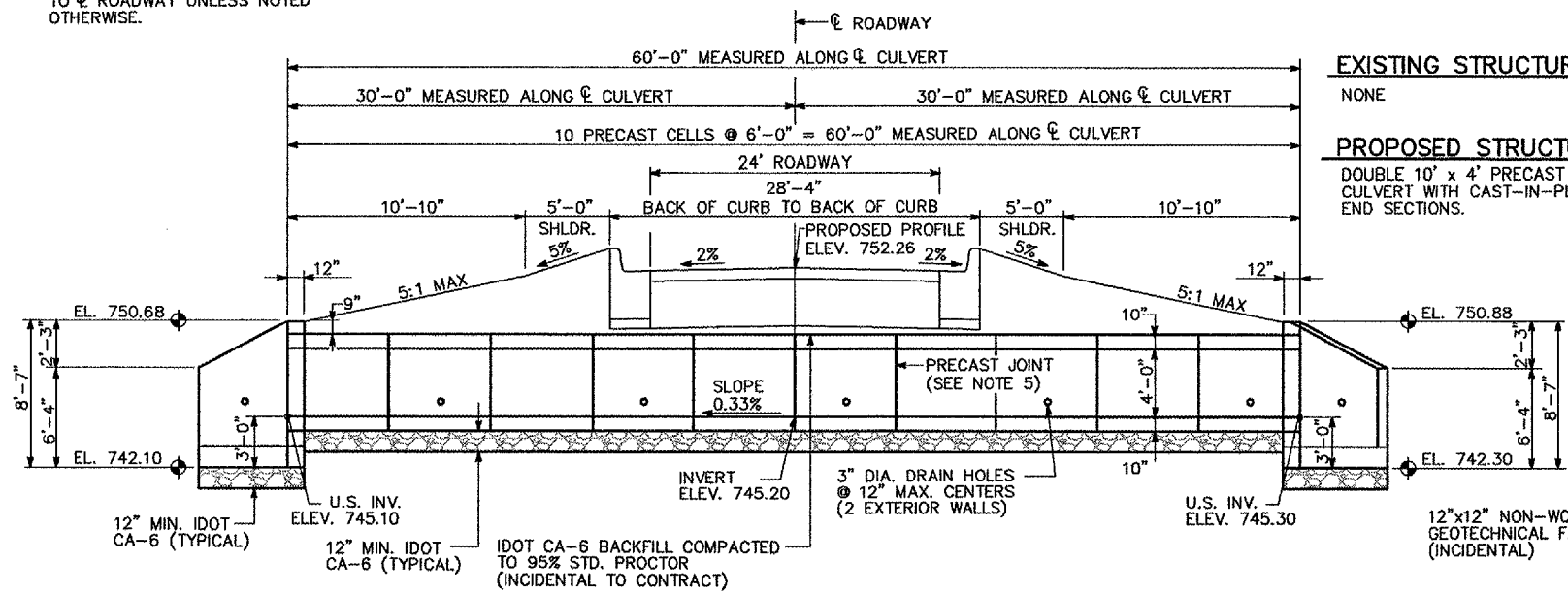
ALLOW 50 psf FUTURE  
 WEARING SURFACE

**DESIGN STRESSES**

$f'_c = 3,500$  psi  
 $f_y = 60,000$  psi (REINFORCEMENT)  
 ALLOWABLE SOIL PRESSURE = 1500 PSF

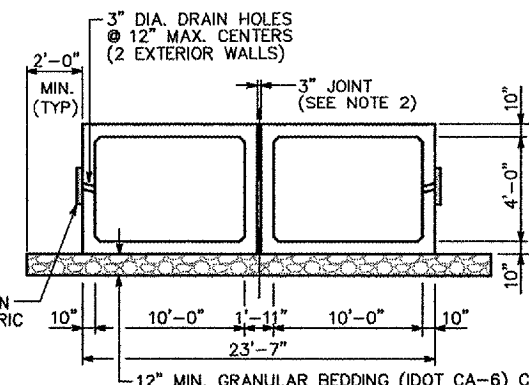
**SEISMIC DATA**

SEISMIC PERFORMANCE CATEGORY (SPC) = A  
 BEDROCK ACCELERATION COEFFICIENT (A) = 0.04  
 SITE COEFFICIENT (S) = 1.5



HALF LONG. SECTION

HALF ELEVATION



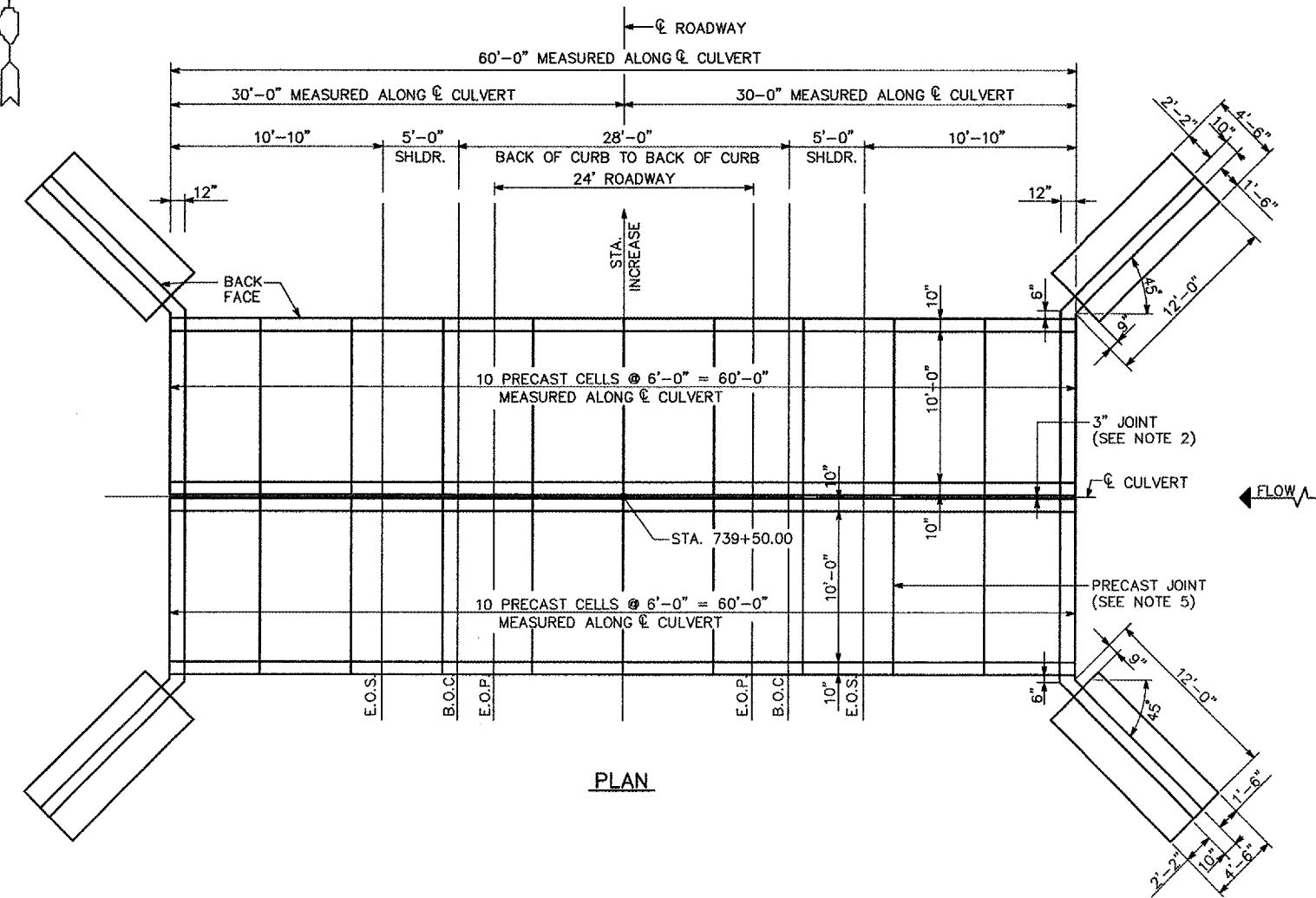
SECTION THRU PRECAST BARREL

**BILL OF MATERIAL**

ITEM	UNIT	QUANTITY
PRECAST CONCRETE BOX CULVERT 10' x 4'	FOOT	120
BOX CULVERT END SECTIONS	EACH	2

**NOTES**

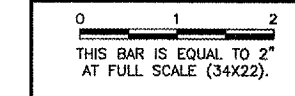
- ALL PRECAST BOX CULVERTS SHALL BE DESIGNED IN ACCORDANCE WITH AASHTO M273, HS-20 LOADING.
- PRIOR TO BACKFILLING, FILL SOLID THE 3" JOINT BETWEEN BARRELS WITH CONCRETE (610).
- GROUT ALL LIFTING HOLES BEFORE BACKFILLING, ALLOW GROUT TO ACHIEVE MINIMUM STRENGTH BEFORE BACKFILLING.
- DURING BACKFILL PLACEMENT, DO NOT PERMIT A DIFFERENCE IN FILL ELEVATION ON THE WALLS OF THE CULVERT IN EXCESS OF 2 FEET. DURING COMPACTION, DO NOT ALLOW WHEELS OF ROLLERS TO COME CLOSER THAN ONE FOOT TO THE FACE OF THE STRUCTURE.
- JOINT MATERIAL BETWEEN PRECAST SECTIONS SHALL CONFORM TO SECTION 1056 OR SECTION 1055 OF IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, ADOPTED JANUARY 1, 2002.
- ANY ACCUMULATED DEBRIS WITHIN THE PROJECT AS A RESULT OF THE IMPROVEMENT SHALL BE REMOVED AND DISPOSED OF SATISFACTORILY BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER. THE COST OF THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE MAINTENANCE OF DRAINAGE AT ALL TIMES DURING THE CONSTRUCTION OF THE CULVERT. THE METHODS USED BY THE CONTRACTOR SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER. THE COST FOR THIS REQUIREMENT WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE JOINTS SHALL BE EXTERNALLY SEALED ON ALL FOUR SIDES USING EITHER 13 INCH WIDE EXTERNAL SEALING BANDS CONFORMING TO SECTION 1057.01 OR 24" WIDE NONWOVEN GEOTECHNICAL FABRIC MEETING THE REQUIREMENTS OF SECTION 1080.01, IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, EXCEPT THE MINIMUM WEIGHT SHALL BE 4 OZ/SY. THE SEAL OR FABRIC SHALL, BE CENTERED OVER THE JOINT AND SECURED TO REMAIN IN PLACE DURING THE BACKFILLING OPERATION.
- REINFORCEMENT BARS SHALL CONFORM TO THE REQUIREMENTS OF THE AASHTO M31, M42 OR M53 GRADE 60.
- PRECAST END SECTIONS WILL NOT BE ALLOWED.
- BEDDING FOR THE BOX CULVERT AND END SECTIONS SHALL BE A MINIMUM OF 12" THICK AND SHALL CONFORM TO IDOT GRADATION CA-6.
- THE CONTRACT UNIT PER LINEAR FOOT FOR THE PRECAST CONCRETE BOX CULVERT SHALL BE FULL PAYMENT FOR FURNISHING AND INSTALLING ALL MATERIALS INCLUDING BUT NOT LIMITED TO CONCRETE FILLER, GROUT, JOINT MATERIAL, GEOTECHNICAL FABRIC AND FOR ALL EXCAVATION EARTH BACKFILL, GRANULAR CRADLE, SELECT GRANULAR BACKFILL PLACEMENT, COMPACTION AND SURFACE GRADING AND FOR ALL LABOR, EQUIPMENT AND TOOLS NECESSARY TO COMPLETE THIS ITEM OF THE SIZE AND TYPE TO THE PLANS AND THE SATISFACTION OF THE ENGINEER.



PLAN

REVISIONS

NUMBER	BY	DATE



**DUPAGE AIRPORT**  
**WEST CHICAGO, ILLINOIS**  
**ROADWAY IMPROVEMENTS FOR SOUTH FLIGHT CENTER**  
**DEVELOPMENT - PHASE 1**  
**BOX CULVERT PLAN AND ELEVATION**

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0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

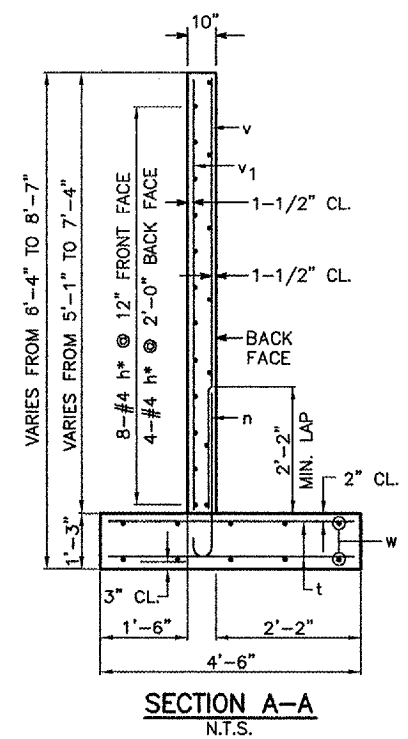
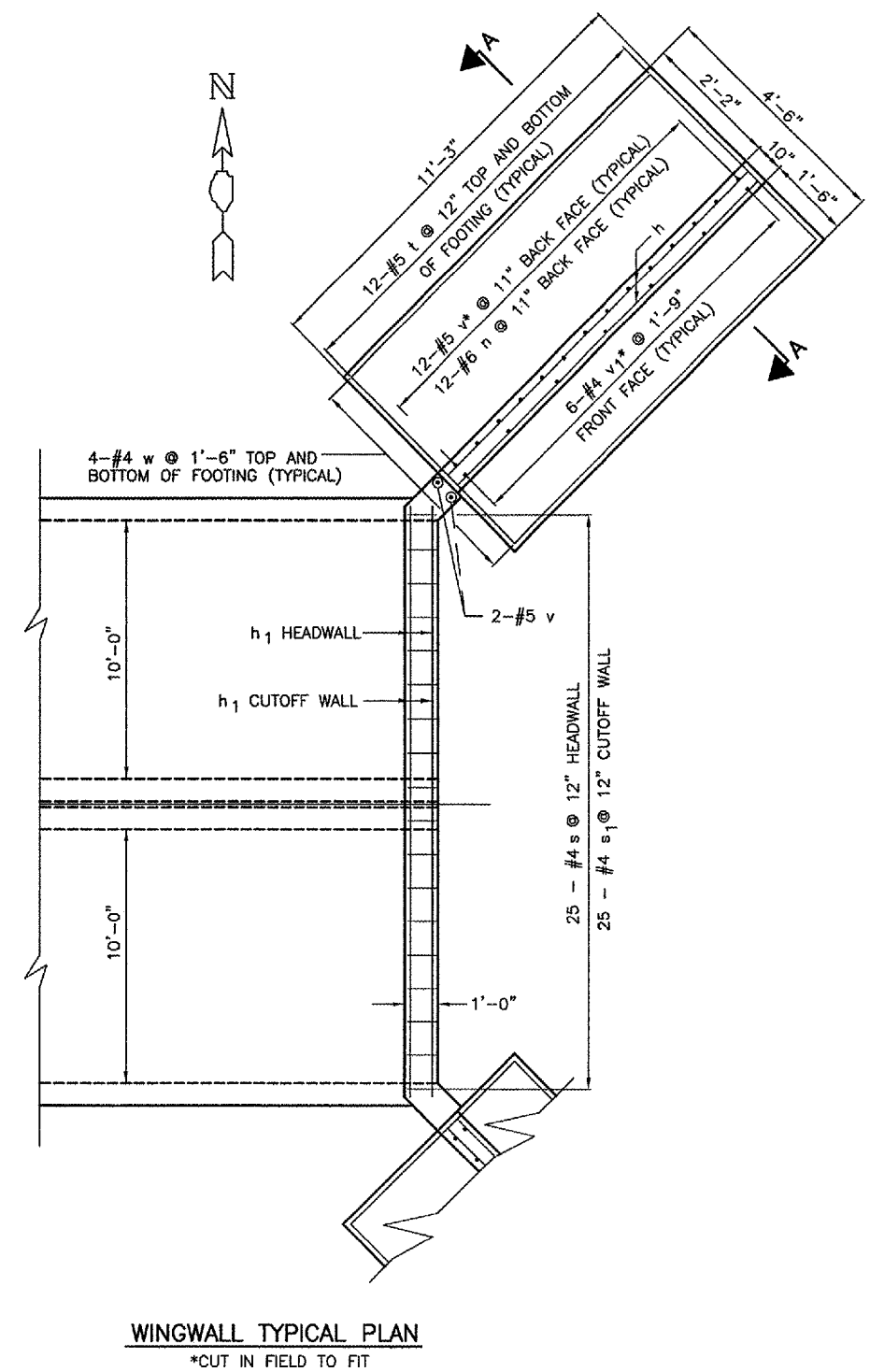
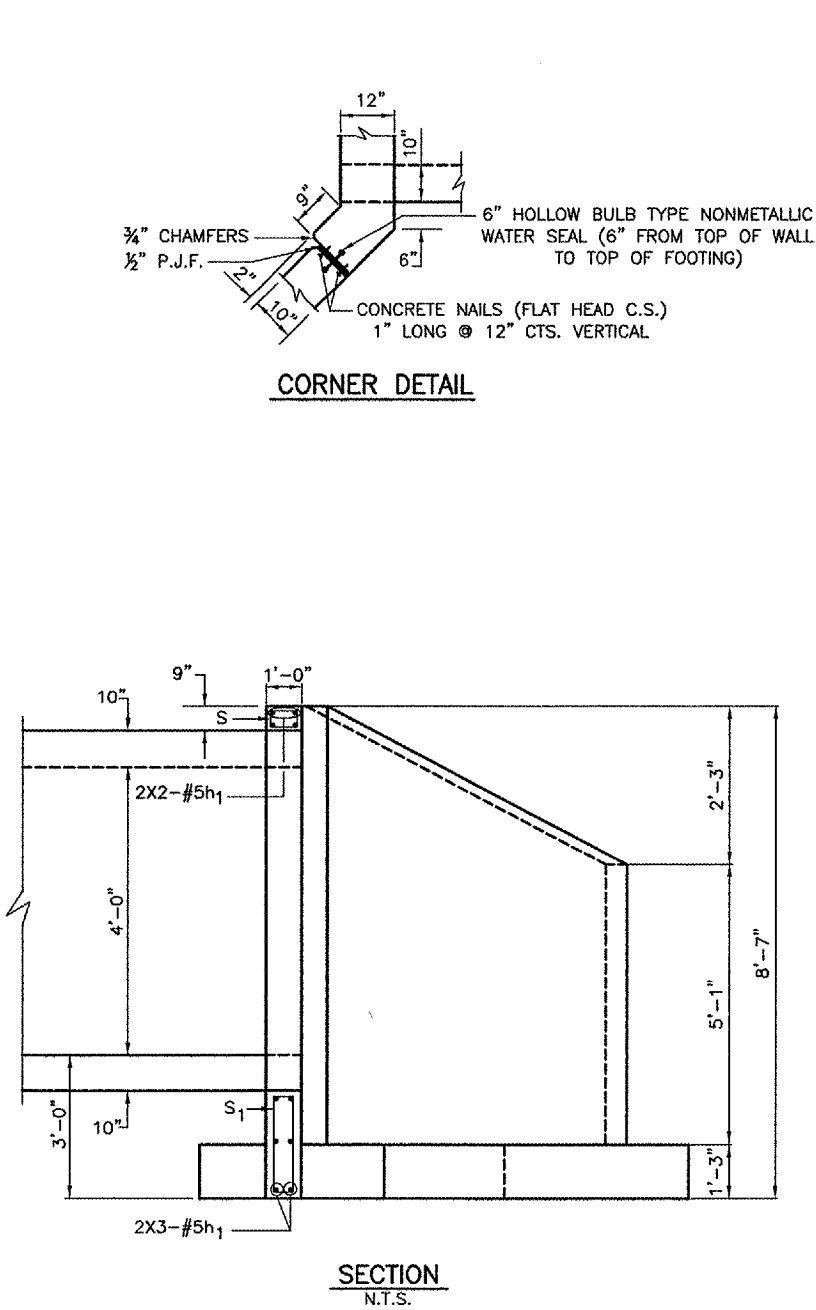
**DUPAGE AIRPORT  
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**BOX CULVERT END SECTION DETAILS**

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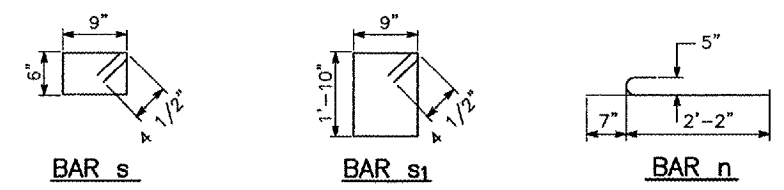


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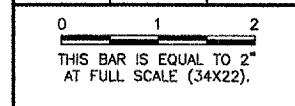


**BILL OF MATERIAL (FOR INFORMATION ONLY)**

BAR	NO.	SIZE	LENTGH	SHAPE	
h	48	#4	11'-0"	—	
h <sub>1</sub>	20	#5	24'-3"	—	
n	48	#5	2'-9"	C	
s	50	#4	3'-3"	L	
s <sub>1</sub>	50	#4	5'-11"	L	
t	96	#5	4'-2"	—	
v	48	#5	7'-0"	—	
v <sub>1</sub>	24	#4	7'-0"	—	
w	32	#4	10'-11"	—	
REINFORCEMENT BARS				POUND	2,420



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**DUPAGE AIRPORT  
WEST CHICAGO, ILLINOIS  
ROADWAY IMPROVEMENTS FOR SOUTH FLIGHT CENTER  
DEVELOPMENT - PHASE 1**

**LIGHTING / PAVEMENT MARKING /  
SIGNING PLAN - FREEDOM DRIVE**

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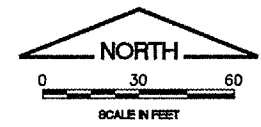
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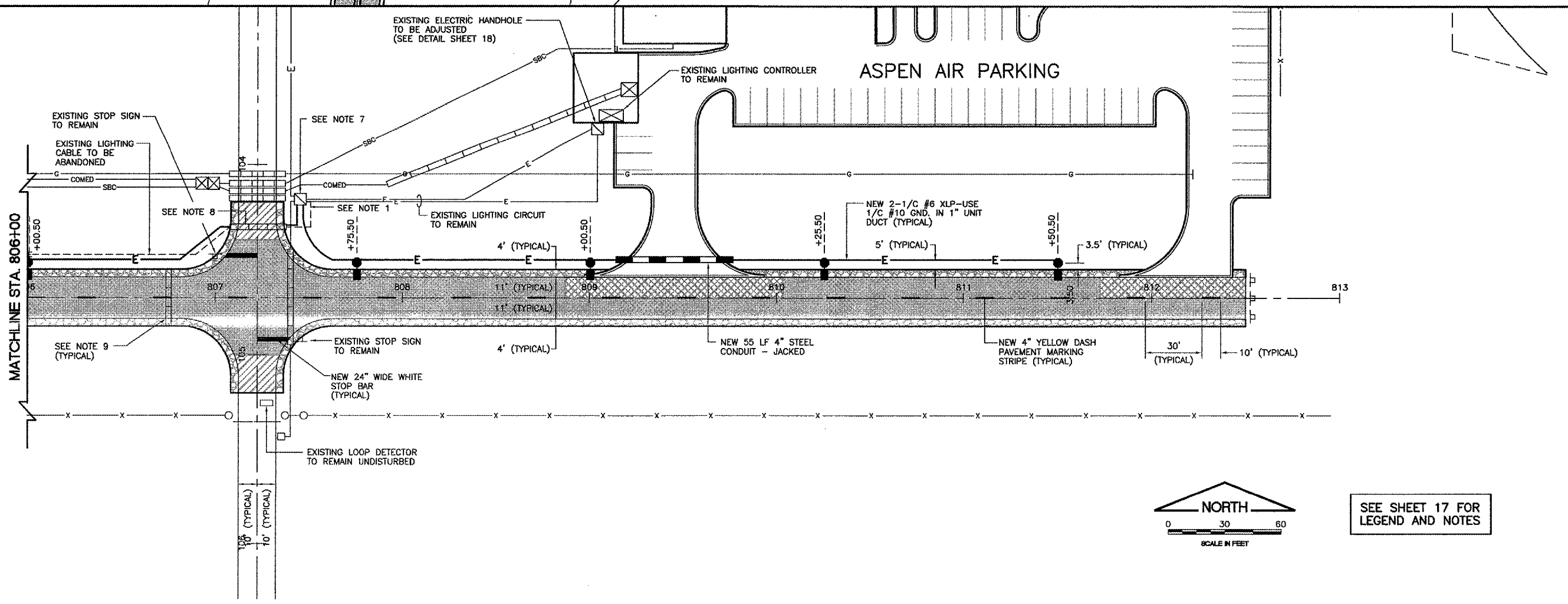
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SEE SHEET 17 FOR  
LEGEND AND NOTES





























UNION PACIFIC  
RAILROAD

MATCHLINE STA. 806+00



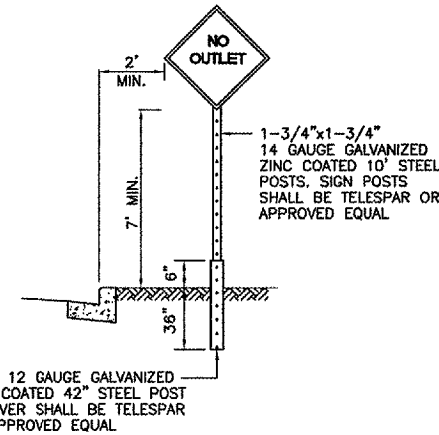
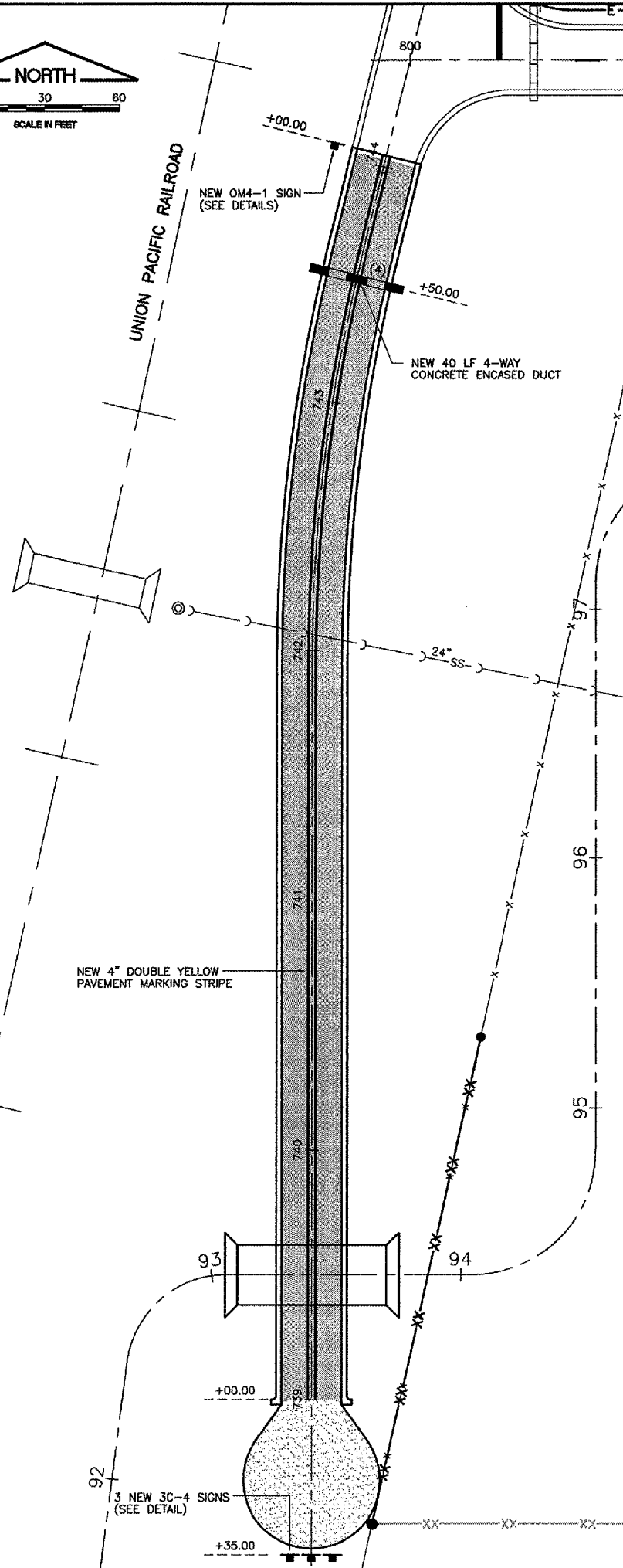
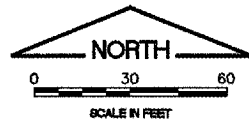
SEE SHEET 17 FOR  
LEGEND AND NOTES

**LEGEND**

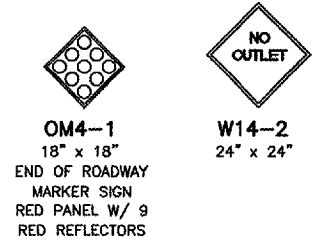
-  BUTT JOINT CONSTRUCTION
-  NEW 4' WIDE AGGREGATE SHOULDER - 6"
-  NEW BITUMINOUS PAVEMENT
-  NEW 6" THICK CRUSHED AGGREGATE TURNAROUND
-  EXISTING BASE MOUNTED TAXI LIGHT
-  EXISTING DUCT BANK
-  EXISTING COMED CABLE(S)
-  EXISTING LIGHTING CIRCUIT
-  EXISTING LIGHTING CIRCUIT TO BE ABANDONED
-  EXISTING WATERMAIN
-  EXISTING GAS LINE
-  EXISTING SANITARY SEWER
-  EXISTING STORM SEWER
-  EXISTING UNDERDRAIN
-  EXISTING MANHOLE/INLET/FLARED END SECTION
-  EXISTING FENCE
-  EXISTING SBC COMMUNICATION CABLE(S)
-  EXISTING ROADWAY SIGN
-  NEW ROADWAY SIGN
-  EXISTING ROADWAY LIGHT
-  EXISTING HANDHOLE
-  NEW STORM SEWER
-  NEW MANHOLE/INLET/FLARED END SECTION
-  NEW DUCT BANK OR STEEL CONDUIT
-  NEW LIGHTING CIRCUIT
-  NEW ROADWAY LIGHT (SEE DETAIL)

**ELECTRICAL NOTES**

1. CONTRACTOR SHALL SPLICE EXISTING LIGHTING CABLE WITH NEW CABLE. COST SHALL BE INCIDENTAL TO AR800140.
2. THE ROUTING OF THE PROPOSED CABLES AND CONDUIT SHOWN IS FOR INFORMATION ONLY. THE EXACT ROUTING SHALL BE COORDINATED WITH THE ENGINEER. THE MINIMUM BURIAL DEPTH OF THE CABLES SHALL BE 30" BELOW FINISHED GRADE. PROPOSED CONDUITS SHALL BE INSTALLED AT A DEPTH LOWER THAN THE PROPOSED UNDERDRAIN BY A MINIMUM OF 1 FOOT.
3. IT IS CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE LOCATION OF THE EXISTING UTILITIES. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT CONTRACTOR'S EXPENSE.
4. ANY SPLICES TO THE PROPOSED CABLES SHALL BE MADE INSIDE THE HANDHOLE OR POLE BASES. NO DIRECT BURIED SPLICES WILL BE ALLOWED. ALL SPLICES SHALL BE WATER PROOF AND SHALL HAVE HEAT SHRINK TUBING OVER ENTIRE SPLICE.
5. ALL AREAS DISTURBED AS A RESULT OF THE WORK UNDER THIS CONTRACT SHALL BE RESTORED. THE COST OF RESTORATION SHALL BE INCIDENTAL TO THE RESPECTIVE ITEMS.
6. WHEN CROSSING THE EXISTING UNDERGROUND UTILITIES, CONTRACTOR SHALL HAND DIG THE TRENCH TO AVOID ANY DAMAGE. CONTRACTOR MAY BE REQUIRED TO ROUTE THE PROPOSED CABLES UNDER THE EXISTING UTILITIES AS DIRECTED BY THE ENGINEER. NO ADDITIONAL COMPENSATION WILL BE MADE FOR WORK REQUIRED TO PLACE CABLES BENEATH EXISTING UTILITIES.
7. CONTRACTOR SHALL CORE HOLE INTO EXISTING MANHOLE/HANDHOLE TO ROUTE PROPOSED UNIT DUCT. COST SHALL BE INCIDENTAL TO THE INSTALLATION OF CABLE IN UNIT DUCT.
8. INSTALL PROPOSED CABLE IN UNIT DUCT IN EXISTING CONDUIT. CONTRACTOR SHALL USE EXISTING CONDUIT ALREADY USED FOR LIGHTING CIRCUITS.
9. THE CONTRACTOR SHALL LOCATE, REMOVE EXISTING DUCT MARKERS AND INSTALL THEM UPON COMPLETION OF THE BITUMINOUS PAVEMENT OVERLAY. REMOVAL AND INSTALLATION SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
10. THE EXISTING LIGHTING CABLE SHALL REMAIN IN SERVICE UNTIL THE NEW CIRCUIT AND LIGHTS ARE COMPLETED AND WORKING.



**POST MOUNTED SIGN LOCATION DETAIL**  
N.T.S.



**SIGN LEGEND**  
NO SCALE

**SIGN NOTES**

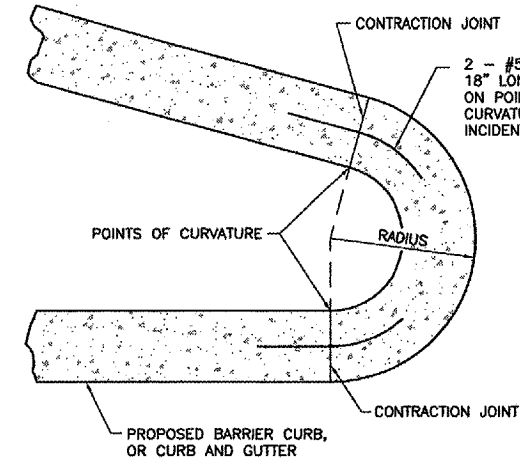
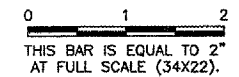
1. ALL SIGNS SHALL BE 3M DIAMOND GRADE VIP REFLECTIVE SHEETING SERIES 3990 OR APPROVED EQUAL.
2. ALL SIGNS ARE STANDARD COLORS AND LETTER STYLE AND SIZE AS SHOWN IN THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES", LATEST EDITION.
3. ALL NEW SIGN POSTS SHALL HAVE 4"x72" DIAMOND GRADE, COLOR MATCHED, SQUARE TUBE REFLECTIVE POST PANELS INSTALLED (COST INCIDENTAL TO NEW SIGNS).

DU072

PATH: K:\DUPAGEAP\DRAW\SHEETS  
FILE: Ph1-lightdupagedr.dwg  
UPDATE BY: .  
SURVEY BOOK #  
XREF DWG:  
XREF DWG:  
DATE: .

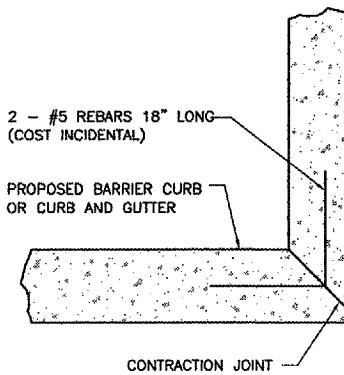
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**ISLAND RADII REINFORCEMENT DETAIL**  
N.T.S.

INSTALL TIE BARS IN LIEU OF DOWEL BARS WHEN RADII ARE 7.5' AND LESS.



**CORNER REINFORCEMENT DETAIL**  
N.T.S.

REINFORCEMENTS SHALL BE INSTALLED AT ALL CORNERS.

**CURB AND GUTTER TYPE M-6.18 NOTES**

1. CONTRACTION JOINTS SHALL BE PLACED AT TEN (10) FOOT MINIMUM ON CENTERS AND SHALL BE SAW CUT TO MINIMUM DEPTH OF TWO (2) INCHES FROM FRONT TO BACK AS SOON AS THE CONCRETE IS SUFFICIENTLY CURED TO ALLOW CUTTING. AS A MAXIMUM, THE CURB SHALL BE SAWED WITHIN TWENTY-FOUR (24) HOURS OF CONCRETE PLACEMENT. CONTRACTION JOINTS SHALL BE FILLED WITH GRAY NP1 OR EQUIVALENT.
2. EXPANSION JOINTS SHALL BE CONSTRUCTED AT A 50' MAXIMUM SPACING.
3. ALL CURBS SHALL BE CURED AND PROTECTED TO THE REQUIREMENTS OF ARTICLE 606 OF THE STATE OF ILLINOIS "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" LATEST EDITION. MEMBRANE CURING WITH W.R. MEADOWS CS 309, OR APPROVED EQUAL, WILL BE ALLOWED WITH A WHITE FUGITIVE DYE, AS PER TYPE II MEMBRANE CURING.
4. CURB AND GUTTER SHALL HAVE A LIGHT BROOM FINISH.
5. EXPANSION JOINTS SHALL ALSO BE PLACED AT ALL POINTS OF CURVATURE, AT 5' EACH SIDE OF ALL DRAINAGE STRUCTURES, AT THE END OF THE DAYS POUR, OR AS DIRECTED BY THE ENGINEER.
6. TWO (2) NO. 5 REBARS (TEN) 10' LONG SHALL BE PLACED ON EITHER SIDE OF ALL PROPOSED UTILITY TRENCHES.
7. THREE (3) NO. 5 REBARS SPACED 5" APART SHALL BE PLACED THE LENGTH OF ALL DEPRESSED CURB.

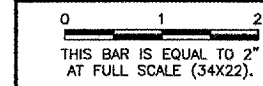
**DUPAGE AIRPORT**  
**WEST CHICAGO, ILLINOIS**  
**ROADWAY IMPROVEMENTS FOR SOUTH FLIGHT CENTER**  
**DEVELOPMENT - PHASE 1**  
**PAVEMENT MARKING / SIGNING PLAN /**  
**MISCELLANEOUS DETAILS - DUPAGE DRIVE**

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**DUPAGE AIRPORT  
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 ROADWAY IMPROVEMENTS FOR SOUTH FLIGHT CENTER  
 DEVELOPMENT - PHASE 1**

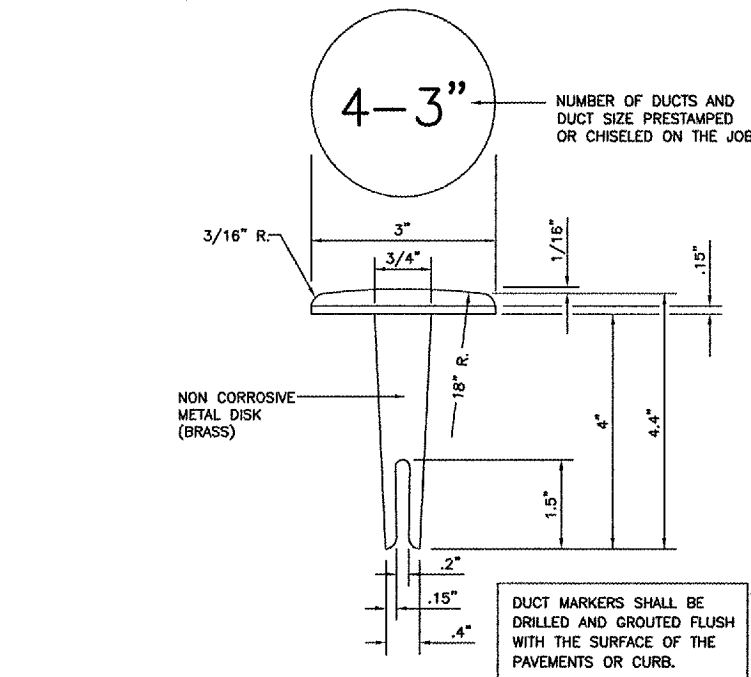
**ELECTRICAL DETAILS**

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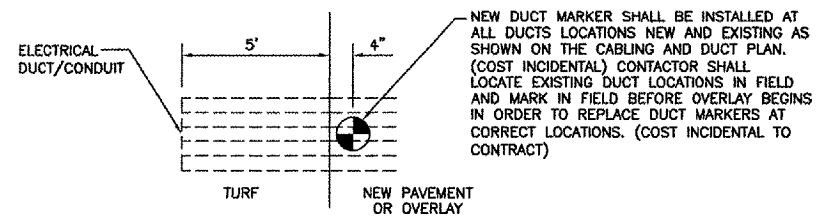


**DuPage Airport**

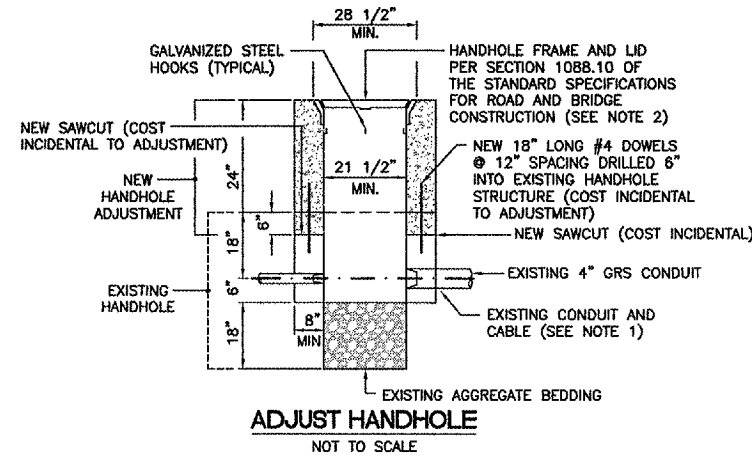
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**DUCT/CONDUIT MARKER DETAIL**  
 NOT TO SCALE

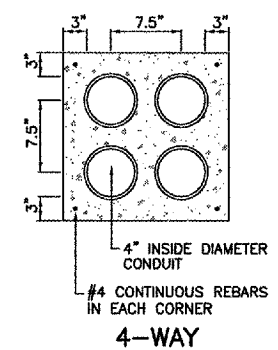


**DUCT/CONDUIT MARKER DETAIL**  
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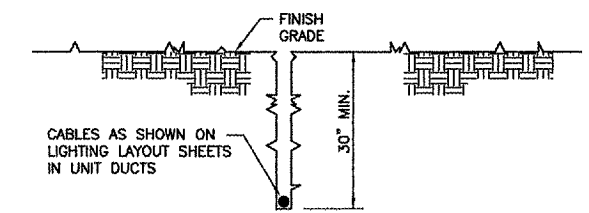
**ADJUST HANDHOLE**  
 NOT TO SCALE

- NOTES:**
1. THE CONTRACTOR SHALL PROTECT EXISTING CABLE AND CONDUIT DURING THE HANDHOLE ADJUSTMENT CONSTRUCTION (INCIDENTAL TO ADJUSTMENT).
  2. THE CONTRACTOR SHALL REUSE THE EXISTING FRAME AND LID. IF THE CONTRACTOR DAMAGES THE EXISTING FRAME AND LID DURING CONSTRUCTION THE CONTRACTOR SHALL INSTALL A NEW FRAME AND LID AT NO ADDITIONAL COST.



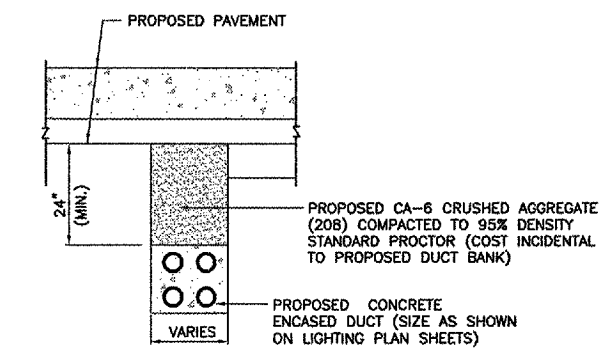
**CONCRETE ENCASED DUCT BANK**  
 NOT TO SCALE

- NOTES:**
1. DIMENSIONS ARE MINIMUM.
  2. CONCRETE SHALL CONFORM TO ITEM 610.
  3. ALL CONDUIT SHALL BE SCHEDULE 40 PVC.
  4. TOP OF CONCRETE ENCASEMENT IN TURF AREAS SHALL NOT BE LESS THAN 24" BELOW FINISHED GRADE.
  5. 4" SPLIT DUCT SHALL BE CONCRETE ENCASED WITH 3" MIN. CONCRETE SURROUNDING 4" CONDUIT. COST INCIDENTAL TO SPLIT DUCT.

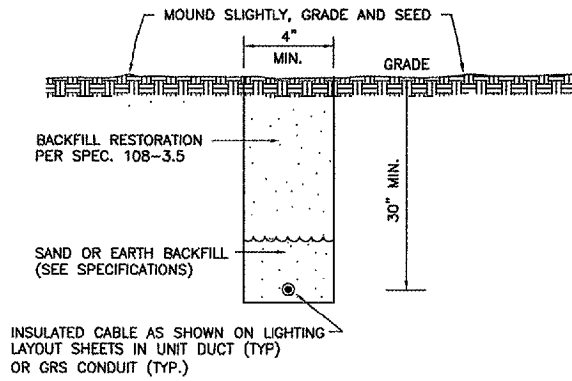


**CABLE IN UNIT-DUCT-PLOWED**  
 NO SCALE

- NOTE**
1. TO FURTHER REDUCE THE POSSIBILITY OF WATER/MOISTURE ENTERING INTO THE CONNECTOR BETWEEN THE CABLE AND THE FIELD ATTACHED CONNECTOR, IT IS REQUIRED THAT A HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE BE APPLIED OVER THE ENTIRE CABLE CONNECTOR.
  2. DUCT MARKERS SHALL BE INSTALLED AT EVERY NEW DUCT AND AT EVERY EXISTING DUCT USED FOR THIS PROJECT.
  3. CONTRACTOR SHALL HAVE THE OPTION TO TRENCH OR PLOW UNIT DUCT. NO ADDITIONAL PAYMENT SHALL BE MADE FOR TRENCHING.



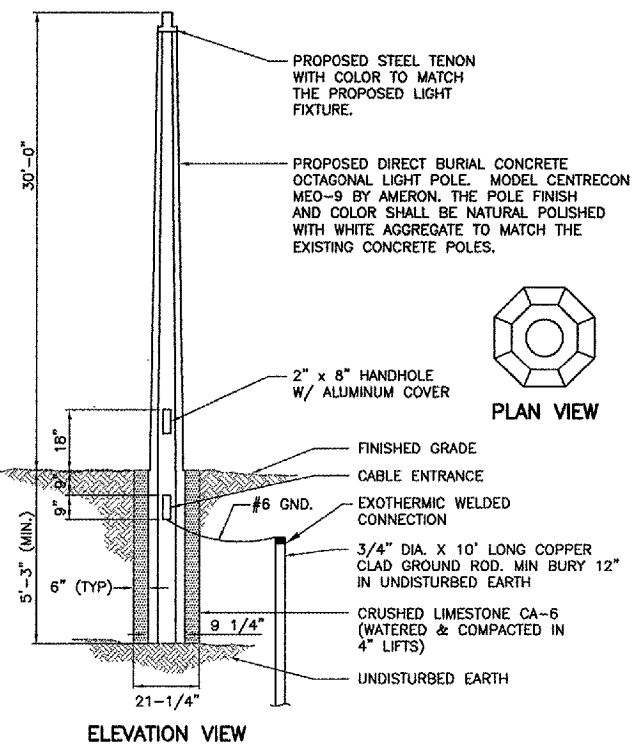
**CONCRETE ENCASED DUCT BACKFILL**  
 NOT TO SCALE



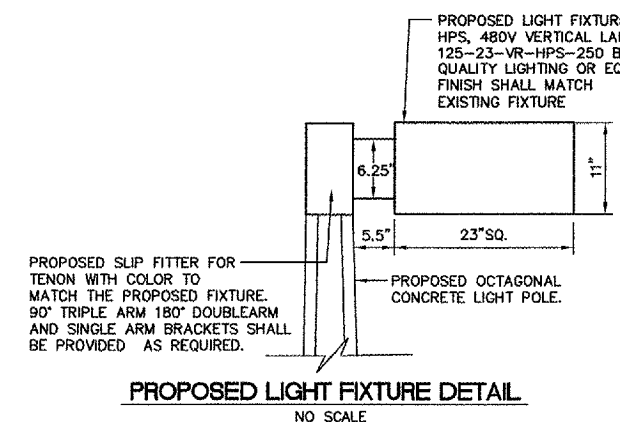
**TRENCH DETAIL**  
 NO SCALE

- NOTE**
1. TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
  2. DEPTH OF TRENCHES SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS.
  3. SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
  4. ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO TRENCH RETURNING MATERIALS.

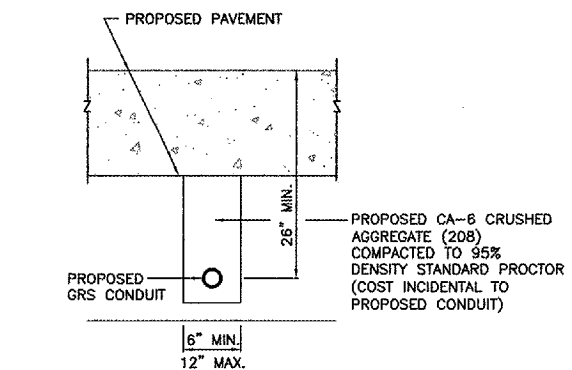
NOTE: AT CONTRACTOR'S OPTION, CABLE PLOWING MAY BE USED IN LIEU OF TRENCHING.



**PROPOSED DIRECT BURIED CONCRETE TYPE A LIGHT POLE DETAIL**  
 NOT TO SCALE



**PROPOSED LIGHT FIXTURE DETAIL**  
 NO SCALE

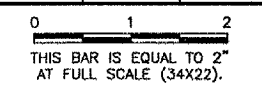


**GRS CONDUIT UNDER PAVEMENT DETAIL**  
 NOT TO SCALE

- NOTES**
1. PROPOSED CONDUIT SHALL BE INSTALLED AT AN ELEVATION THAT WILL NOT CONFLICT WITH EXISTING OR PROPOSED UTILITIES INCLUDING STORM SEWER, UNDERDRAIN, CONDUIT, DUCT, GAS, WATERMAIN, PHONE, ELECTRICAL AT NO ADDITIONAL COST TO THE CONTRACT.



REVISIONS		
NUMBER	BY	DATE



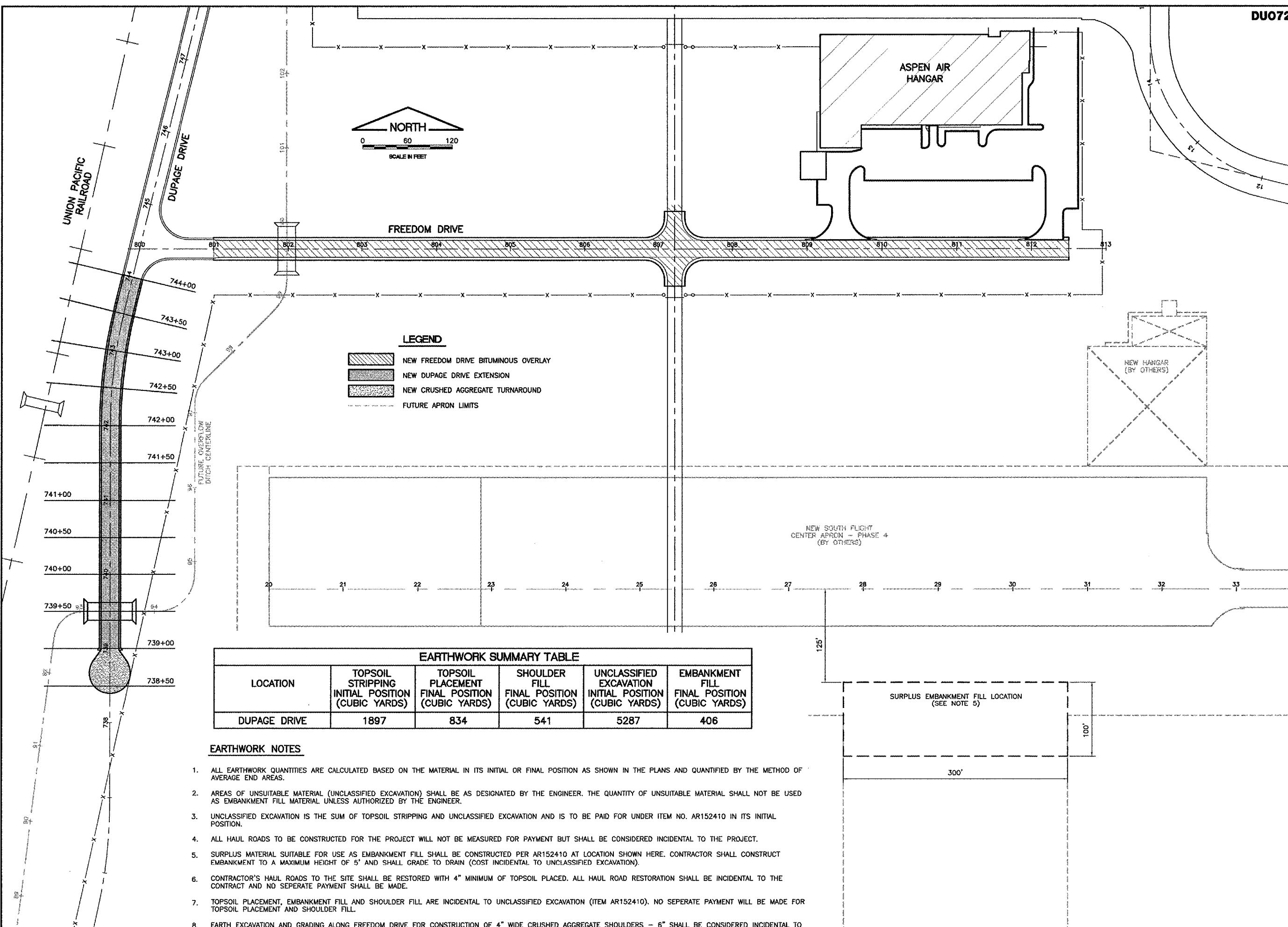
**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 ROADWAY IMPROVEMENTS FOR SOUTH FLIGHT CENTER  
 DEVELOPMENT - PHASE 1**

**INDEX TO CROSS SECTIONS**

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DESIGN BY:	JRL
DRAWN BY:	JRO
CHECKED BY:	MJS
APPROVED BY:	DLP
DATE:	03/03/06
JOB No:	05257-02



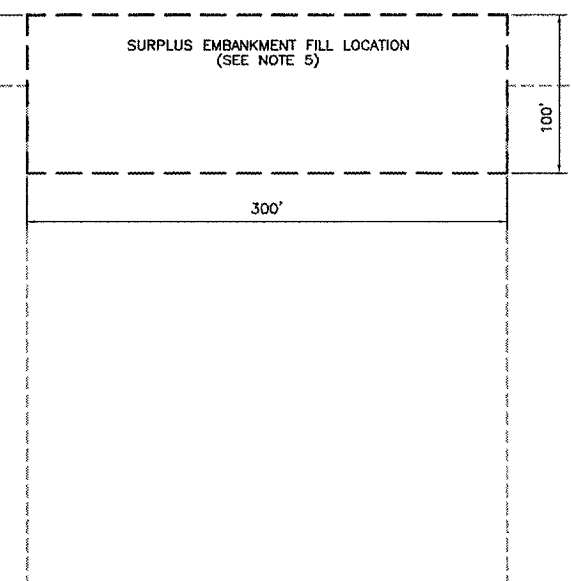
**LEGEND**

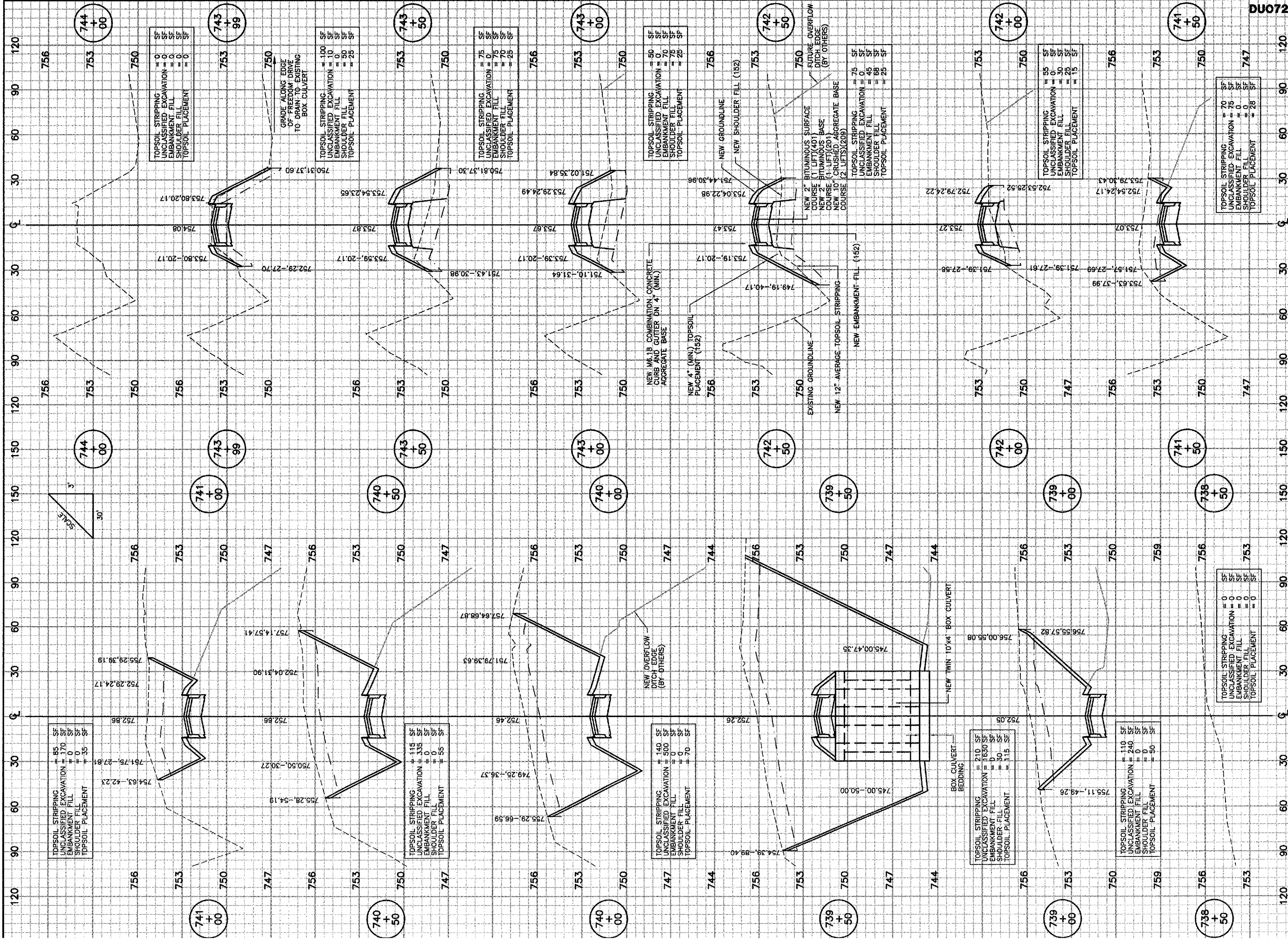
	NEW FREEDOM DRIVE BITUMINOUS OVERLAY
	NEW DUPAGE DRIVE EXTENSION
	NEW CRUSHED AGGREGATE TURNAROUND
	FUTURE APRON LIMITS

**EARTHWORK SUMMARY TABLE**

LOCATION	TOPSOIL STRIPPING INITIAL POSITION (CUBIC YARDS)	TOPSOIL PLACEMENT FINAL POSITION (CUBIC YARDS)	SHOULDER FILL FINAL POSITION (CUBIC YARDS)	UNCLASSIFIED EXCAVATION INITIAL POSITION (CUBIC YARDS)	EMBANKMENT FILL FINAL POSITION (CUBIC YARDS)
DUPAGE DRIVE	1897	834	541	5287	406

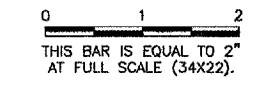
- EARTHWORK NOTES**
- ALL EARTHWORK QUANTITIES ARE CALCULATED BASED ON THE MATERIAL IN ITS INITIAL OR FINAL POSITION AS SHOWN IN THE PLANS AND QUANTIFIED BY THE METHOD OF AVERAGE END AREAS.
  - AREAS OF UNSUITABLE MATERIAL (UNCLASSIFIED EXCAVATION) SHALL BE AS DESIGNATED BY THE ENGINEER. THE QUANTITY OF UNSUITABLE MATERIAL SHALL NOT BE USED AS EMBANKMENT FILL MATERIAL UNLESS AUTHORIZED BY THE ENGINEER.
  - UNCLASSIFIED EXCAVATION IS THE SUM OF TOPSOIL STRIPPING AND UNCLASSIFIED EXCAVATION AND IS TO BE PAID FOR UNDER ITEM NO. AR152410 IN ITS INITIAL POSITION.
  - ALL HAUL ROADS TO BE CONSTRUCTED FOR THE PROJECT WILL NOT BE MEASURED FOR PAYMENT BUT SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
  - SURPLUS MATERIAL SUITABLE FOR USE AS EMBANKMENT FILL SHALL BE CONSTRUCTED PER AR152410 AT LOCATION SHOWN HERE. CONTRACTOR SHALL CONSTRUCT EMBANKMENT TO A MAXIMUM HEIGHT OF 5' AND SHALL GRADE TO DRAIN (COST INCIDENTAL TO UNCLASSIFIED EXCAVATION).
  - CONTRACTOR'S HAUL ROADS TO THE SITE SHALL BE RESTORED WITH 4" MINIMUM OF TOPSOIL PLACED. ALL HAUL ROAD RESTORATION SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPERATE PAYMENT SHALL BE MADE.
  - TOPSOIL PLACEMENT, EMBANKMENT FILL AND SHOULDER FILL ARE INCIDENTAL TO UNCLASSIFIED EXCAVATION (ITEM AR152410). NO SEPERATE PAYMENT WILL BE MADE FOR TOPSOIL PLACEMENT AND SHOULDER FILL.
  - EARTH EXCAVATION AND GRADING ALONG FREEDOM DRIVE FOR CONSTRUCTION OF 4" WIDE CRUSHED AGGREGATE SHOULDERS - 6" SHALL BE CONSIDERED INCIDENTAL TO ITEM AR209706.





DU072  
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 FILE: RC002001.DWG  
 UPDATE BY:  
 SURVEY BOOK #  
 XREF DWG:  
 XREF DWG:  
 DATE:

REVISIONS		
NUMBER	BY	DATE



**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 ROADWAY IMPROVEMENTS FOR SOUTH FLIGHT CENTER  
 DEVELOPMENT - PHASE 1  
 CROSS SECTIONS  
 DUPAGE DRIVE EXTENSION**

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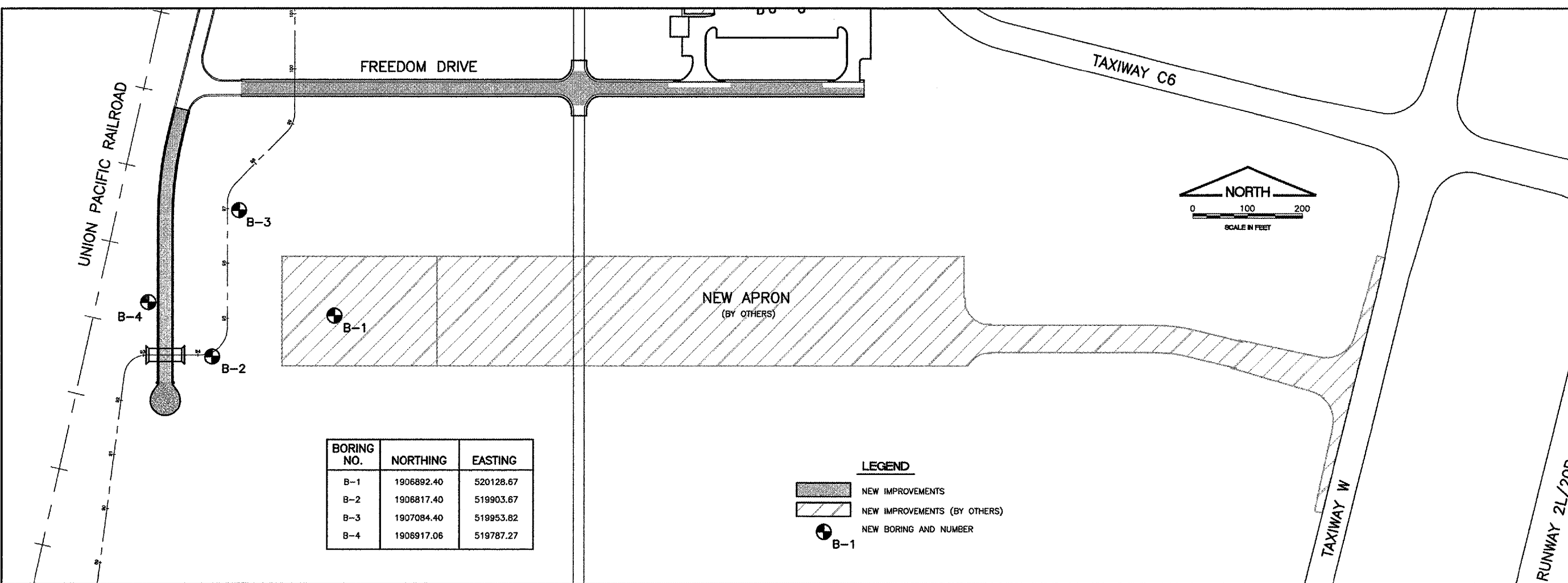
**DDA**  
 DuPage Airport

DESIGN BY:	JRL
DRAWN BY:	JRL
CHECKED BY:	JRL
APPROVED BY:	DLP
DATE:	03/03/06
JOB No:	05257-02

PATH: K:\0525702\Draw\sheets  
 FILE: ph1-enginfo1.dwg  
 UPDATE BY: johse  
 SURVEY BOOK #  
 XREF DWG:  
 XREF DWG:  
 DATE: Mon 10/11/04 3:14pm

REVISIONS		
NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).



BORING NO.	NORTHING	EASTING
B-1	1906892.40	520128.67
B-2	1906817.40	519903.67
B-3	1907084.40	519953.62
B-4	1906917.06	519787.27

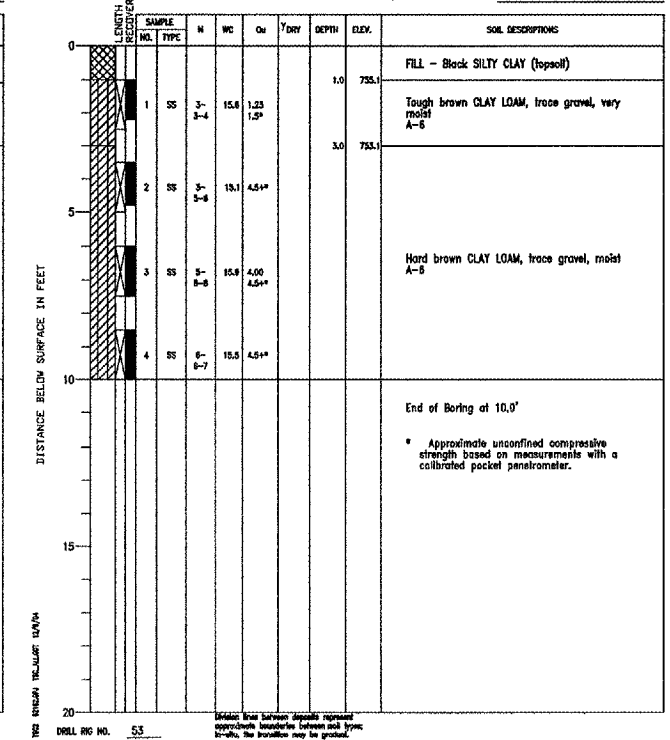
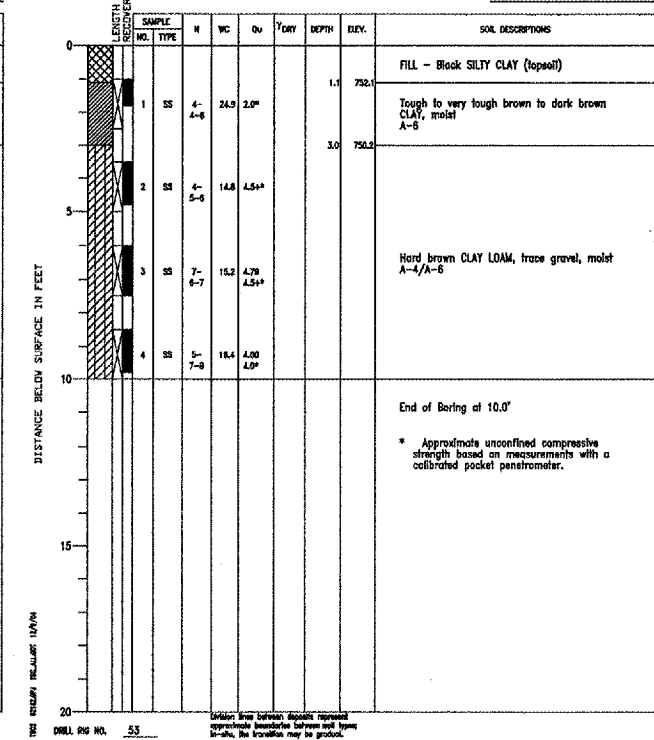
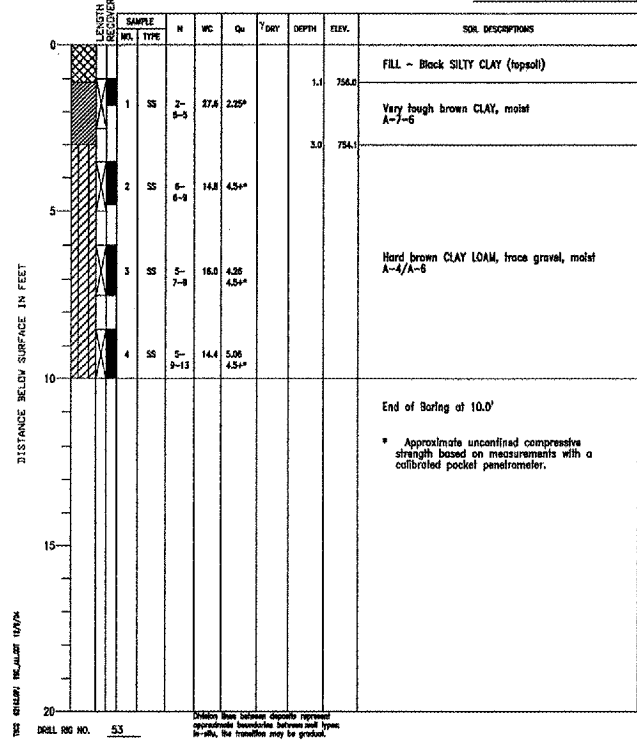
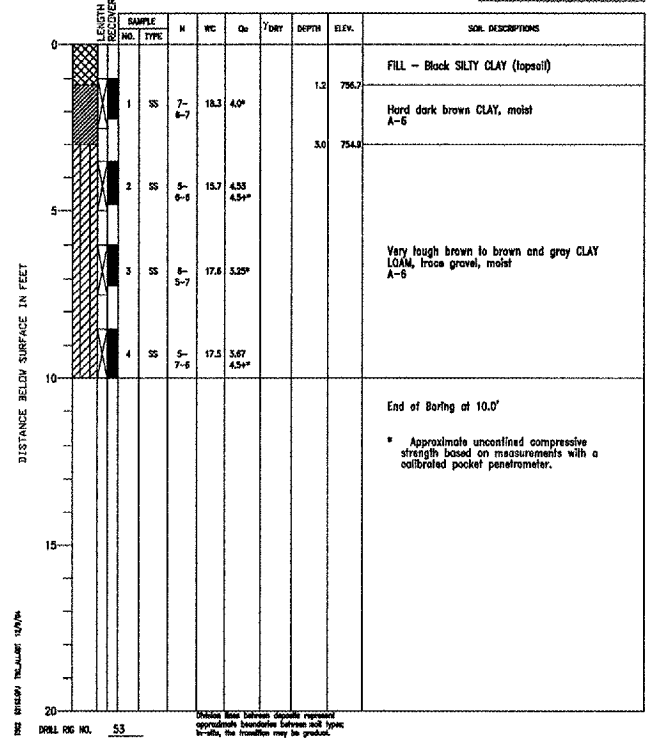
**LEGEND**  
 NEW IMPROVEMENTS  
 NEW IMPROVEMENTS (BY OTHERS)  
 NEW BORING AND NUMBER  
 B-1

PROJECT DuPage Airport - Apron Phase 4, South Flight Center, West Chicago, Illinois  
 CLIENT Crawford, Murphy & Tilly, Inc., Aurora, Illinois  
 BORING B-1 DATE STARTED 11-23-04 DATE COMPLETED 11-23-04 JOB L-62,162  
 ELEVATIONS: GROUND SURFACE 757.9, END OF BORING 747.9  
 WHILE DRILLING: Dry  
 WATER LEVEL OBSERVATIONS: Dry  
 AT END OF BORING: Dry  
 24 HOURS

PROJECT DuPage Airport - Apron Phase 4, South Flight Center, West Chicago, Illinois  
 CLIENT Crawford, Murphy & Tilly, Inc., Aurora, Illinois  
 BORING B-2 DATE STARTED 11-23-04 DATE COMPLETED 11-23-04 JOB L-62,162  
 ELEVATIONS: GROUND SURFACE 757.1, END OF BORING 747.1  
 WHILE DRILLING: Dry  
 WATER LEVEL OBSERVATIONS: Dry  
 AT END OF BORING: Dry  
 24 HOURS

PROJECT DuPage Airport - Apron Phase 4, South Flight Center, West Chicago, Illinois  
 CLIENT Crawford, Murphy & Tilly, Inc., Aurora, Illinois  
 BORING B-3 DATE STARTED 11-23-04 DATE COMPLETED 11-23-04 JOB L-62,162  
 ELEVATIONS: GROUND SURFACE 753.2, END OF BORING 743.2  
 WHILE DRILLING: Dry  
 WATER LEVEL OBSERVATIONS: Dry  
 AT END OF BORING: Dry  
 24 HOURS

PROJECT DuPage Airport - Apron Phase 4, South Flight Center, West Chicago, Illinois  
 CLIENT Crawford, Murphy & Tilly, Inc., Aurora, Illinois  
 BORING B-4 DATE STARTED 11-23-04 DATE COMPLETED 11-23-04 JOB L-62,162  
 ELEVATIONS: GROUND SURFACE 756.1, END OF BORING 746.1  
 WHILE DRILLING: Dry  
 WATER LEVEL OBSERVATIONS: Dry  
 AT END OF BORING: Dry  
 24 HOURS



**DUPAGE AIRPORT  
 WEST CHICAGO, ILLINOIS  
 ROADWAY IMPROVEMENTS FOR SOUTH FLIGHT CENTER  
 DEVELOPMENT - PHASE 1**

**ENGINEERING INFORMATION**

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DESIGN BY: JRL  
 DRAWN BY: JRO  
 CHECKED BY: JRL  
 APPROVED BY: DLP  
 DATE: 1/27/06  
 JOB No: 05257-02