

BITUMINOUS PAVEMENT REMOVAL NOTES

THE BITUMINOUS PAVEMENT REMOVAL SHALL CONSIST OF REMOVING THE EXISTING RUNWAY PAVEMENT AT THE LOCATIONS SHOWN ON THE CONSTRUCTION DRAWINGS. THE EXISTING BITUMINOUS SURFACE SHALL BE REMOVED FULL DEPTH AND DISPOSED OF BY THE CONTRACTOR OFF THE AIRPORT PROPERTY.

THE CONTRACTOR IS REQUIRED TO SAWCUT THE EXISTING PAVEMENT USING A WHEEL SAW AT THE LOCATIONS WHERE THE PROPOSED WIDENING WILL MATCH THE EXISTING PAVEMENT, AS SHOWN.

THE PAVEMENT SAWING WILL BE CONSIDERED INCIDENTAL TO THE PAVEMENT REMOVAL AND NO ADDITIONAL COMPENSATION WILL BE MADE.

ANY ADJACENT PAVEMENT DAMAGED BY THE PAVEMENT REMOVAL OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

THE QUANTITY OF BITUMINOUS PAVEMENT REMOVAL TO BE PAID FOR SHALL BE THE NUMBER OF SQUARE YARDS OF PAVEMENT REMOVED, AND DISPOSED OF IN ACCORDANCE WITH THE SPECIFICATIONS, CONSTRUCTION DRAWINGS, AND ACCEPTED BY THE ENGINEER. MEASUREMENT OF THE BITUMINOUS PAVEMENT REMOVAL SHALL BE TO THE NEAREST SQUARE YARD.

ALL BITUMINOUS PAVEMENT REMOVAL WILL BE PAID FOR UNDER ITEM:
AR401900 REMOVE BITUMINOUS PAVEMENT ____ 285 S.Y.

152410-UNCLASSIFIED EXCAVATION NOTES:

THE PROPOSED UNCLASSIFIED EXCAVATION SHALL BE PLACED IN ACCORDANCE WITH ITEM 152 "EXCAVATION AND EMBANKMENT" AS STATED ON PAGE 25 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THE EARTH SUBGRADE WILL BE FIRST ROUGH CUT TO THE SPECIFIED ELEVATIONS, THE ELEVATIONS SHOWN ON THE STAKING PLAN ARE FOR FINISHED PAVEMENT. SUBGRADE ELEVATIONS WILL BE THE GRADE SHOWN MINUS THE PAVEMENT THICKNESS (INCLUDING BASE).

MATERIAL TO BE USED FOR SHOULDERING WILL BE STORED OUTSIDE OF THE PROPOSED PAVEMENT AREA AND WITHIN THE GRADING LIMITS.

THE MATERIAL TO BE USED FOR THE SHOULDERING WILL BE TOPSOIL OBTAINED FROM THE ON SITE UNCLASSIFIED EXCAVATION.

THE EXCAVATED AREAS WILL BE COMPACTED IN ACCORDANCE WITH PROCEDURES FOR AIRCRAFT WEIGHING LESS THAN 60,000 POUNDS.

THE PROPOSED SHOULDER MATERIAL WILL ONLY REQUIRE "LIGHT COMPACTION" TO THE SATISFACTION OF THE RESIDENT ENGINEER.

ALL EARTHWORK WILL BE CLASSIFIED AS "UNCLASSIFIED EXCAVATION".

THE PROPOSED UNCLASSIFIED EXCAVATION WILL BE PAID FOR UNDER ITEM:
AR152410 UNCLASSIFIED EXCAVATION ____ 2,999 C.Y.

602-BITUMINOUS PRIME COAT NOTES:

THE BITUMINOUS PRIME COAT (602) SHALL BE PLACED IN ACCORDANCE WITH ITEM AR602 "BITUMINOUS PRIME COAT" AS STATED ON PAGE 58 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THE PROPOSED BITUMINOUS PRIME COAT SHALL BE PLACED ON THE PROPOSED AGGREGATE BASE COURSE PRIOR TO THE PLACEMENT OF THE FIRST LIFT OF PROPOSED BITUMINOUS BASE COURSE. THE PROPOSED AGGREGATE BASE COURSE SHALL HAVE A PRIME COAT OF BITUMINOUS MATERIAL APPLIED IN ACCORDANCE WITH THE REQUIREMENTS OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS.

THE PROPOSED BITUMINOUS PRIME COAT WILL BE PAID FOR UNDER ITEM:
AR602510 BITUMINOUS PRIME COAT ____ 3,183 GAL.

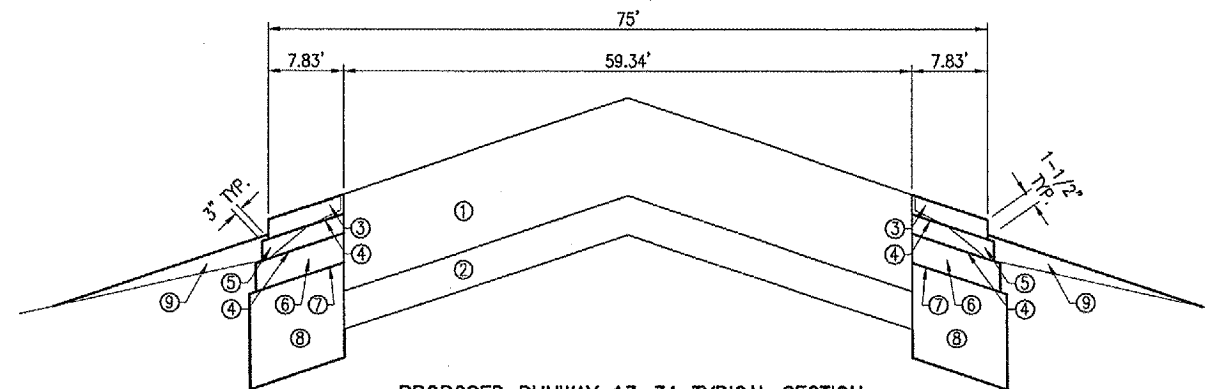
603-BITUMINOUS TACK COAT NOTES:

THE BITUMINOUS TACK COAT (603) SHALL BE PLACED IN ACCORDANCE WITH ITEM AR603 "BITUMINOUS TACK COAT" AS STATED ON PAGE 62 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

AN APPLICATION OF BITUMINOUS TACK WILL BE SPRAYED BEFORE THE PLACEMENT OF THE LIFT OF BITUMINOUS SURFACE COURSE AND IN BETWEEN LIFTS OF BITUMINOUS BASE COURSE. THE PLAN QUANTITY IS BASED ON THREE APPLICATIONS AT A RATE OF 0.1 GALLONS PER SQUARE YARD. THE PROPOSED BITUMINOUS PAVEMENT SHALL HAVE A TACK COAT OF BITUMINOUS MATERIAL APPLIED IN ACCORDANCE WITH THE REQUIREMENTS OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS.

THE VERTICAL FACE OF ALL BUTT JOINTS WILL BE PAINTED WITH A LIQUID ASPHALT (TACK COAT).

THE PROPOSED BITUMINOUS TACK COAT WILL BE PAID FOR UNDER ITEM:
AR603510 BITUMINOUS TACK COAT ____ 1,782 GAL.



PROPOSED RUNWAY 13-31 TYPICAL SECTION

"NOT TO SCALE"

NOTE:

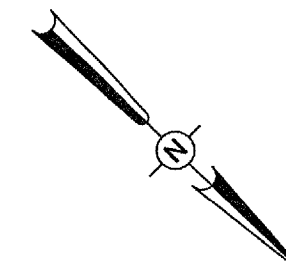
THE EXISTING PAVEMENT CONFIGURATION IS, FROM STA. 189+47 TO STA. 200+00 IS 10" BITUMINOUS ON 4" CRUSHED AGGREGATE BASE COURSE, FROM STA. 200+00 TO STA. 229+47 IS 8" BITUMINOUS ON 7" CRUSHED AGGREGATE BASE COURSE.

LEGEND FOR RUNWAY 13-31 TYPICAL SECTION

- ① EXISTING BITUMINOUS
- ② EXISTING CRUSHED AGGREGATE BASE COURSE
- ③ PROPOSED 401 BITUMINOUS SURFACE COURSE (2" DEPTH)
- ④ PROPOSED 603 BITUMINOUS TACK COAT (0.15 GAL./S.Y.)
- ⑤ PROPOSED 201 BITUMINOUS BASE COURSE (2" DEPTH)
- ⑥ PROPOSED 201 BITUMINOUS BASE COURSE (3" DEPTH)
- ⑦ PROPOSED 602 BITUMINOUS PRIME COAT (0.35 GAL./S.Y.)
- ⑧ PROPOSED 209 CRUSHED AGGREGATE BASE COURSE (10" DEPTH)
- ⑨ PROPOSED 152 UNCLASSIFIED EXCAVATION

LEGEND

- EXISTING PAVEMENT
- PROPOSED PAVEMENT WIDENING
- PROPOSED BITUMINOUS PAVEMENT REMOVAL



0' 15' 30' 60'
HALF SIZE SCALE: 1" = 60'
FULL SIZE SCALE: 1" = 30'

DATE	REVISION	BY

TRI-TOWNSHIP AIRPORT
SAVANNA, CARROLL COUNTY
ILLINOIS
A.I.P. PROJ.: 3-17-0991-B12
IL PROJ.: SPY-3538

HEL Project No. 826-08RWTD_0800	FILENAME R-121CON.DWG	DATE 10/26/05
Scale 1"=30'	Scale 1"=30'	DATE 10/26/05
DATE 10/26/05	DATE 10/26/05	DATE 01/12/06
LAYOUT B.A.K.	DRAWN B.A.K.	REVIEWED C.A.H.

HANSON
Professional Services Inc.
1525 South Sixth Street
Springfield, Illinois 62705-2986
Offices Nationwide

PROPOSED WIDENING
OF RUNWAY 13/31
PROPOSED
CONSTRUCTION PLAN
STA. 221+50 TO STA. 229+47.00

MAR 29 2006 10:49 AM CAH I:\AIRPORTS\SAVANNA\826-08RWTD\AIRPORT\SHEETS\R-121CON.DWG - STA. 221+50 TO STA. 229+47.00