

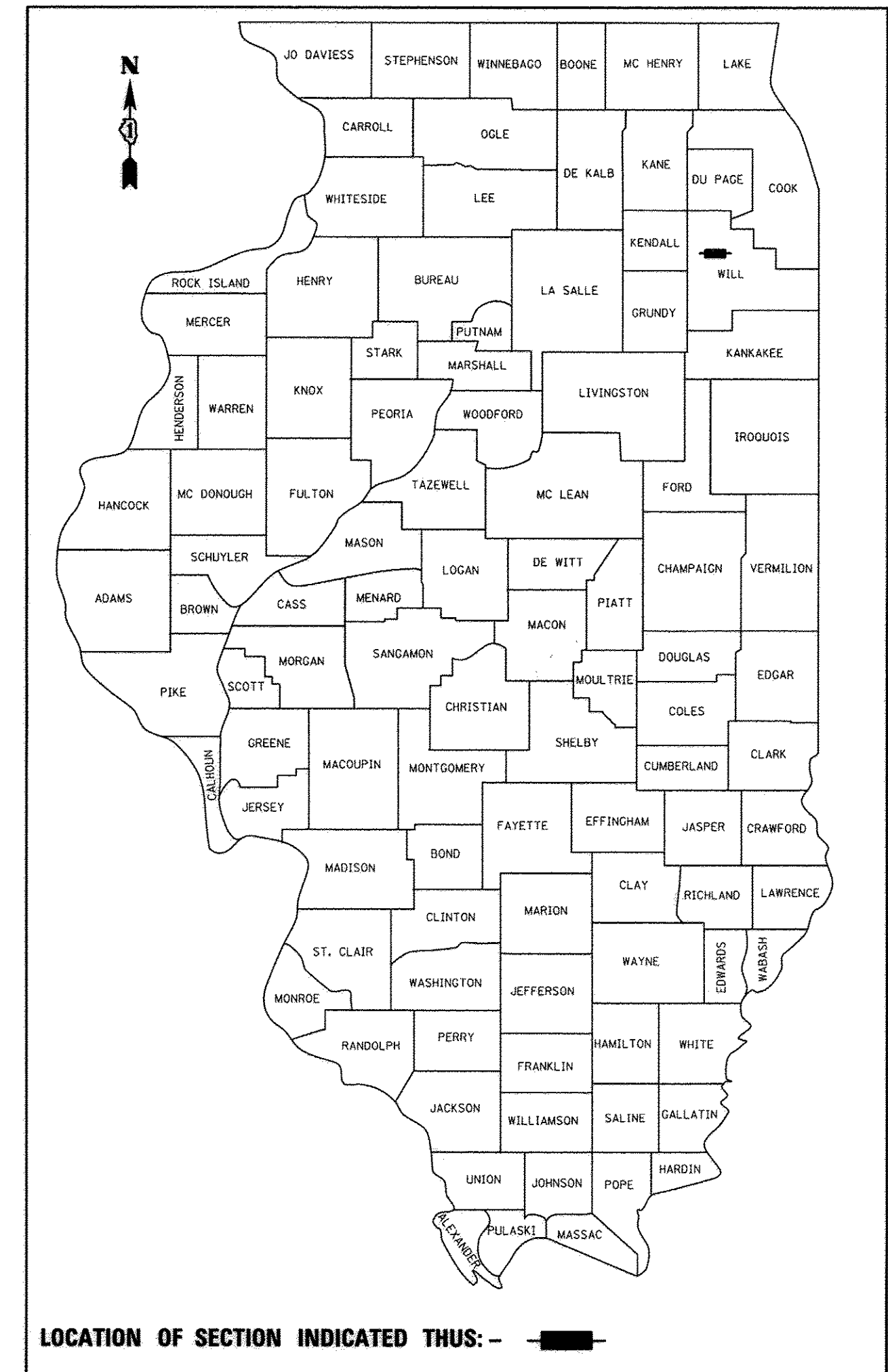
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0298	14-00038-00-RS	WILL	22	1
		ILLINOIS	CONTRACT NO. 61D72	

FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR LISTING OF HIGHWAY STANDARDS, SEE SHEET NO. 2

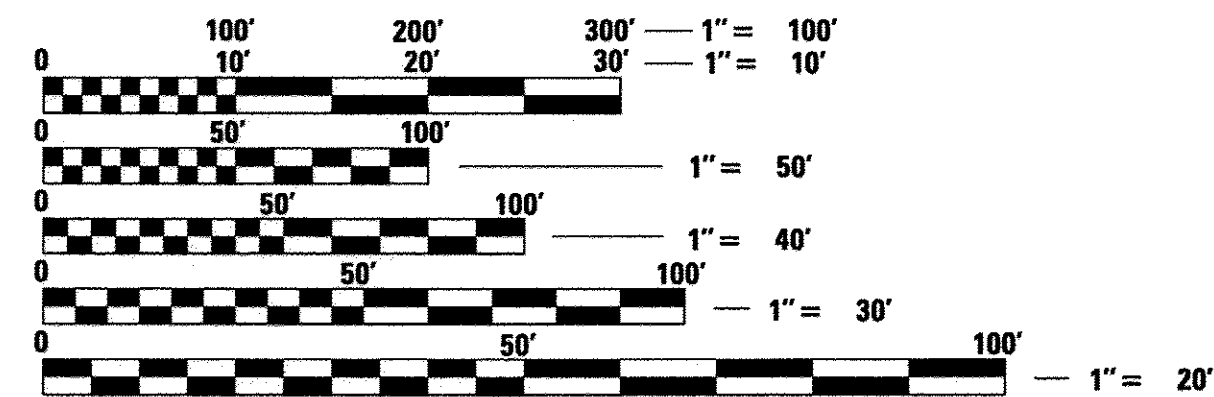
PLANS FOR PROPOSED FEDERAL AID HIGHWAY

FAU ROUTE 0298 (BLACK ROAD)
RIVER ROAD TO INTERSTATE 55
RESURFACING
SECTION 14-00038-00-RS
PROJECT NO. M-4003(414)
VILLAGE OF SHOREWOOD
WILL COUNTY
JOB NO. C-91-109-15



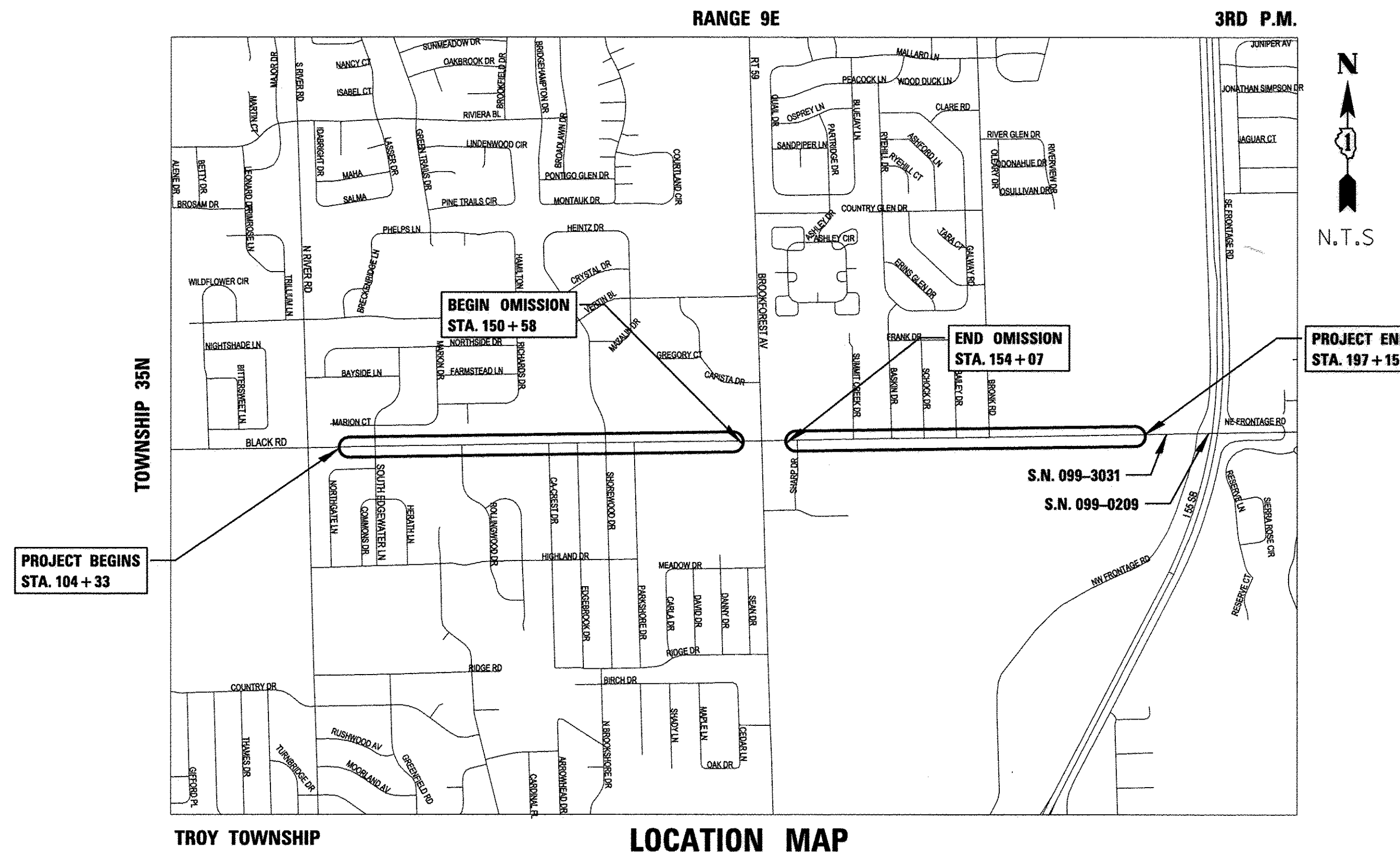
LOCATION OF SECTION INDICATED THUS: — ■ —

TRAFFIC DATA
ROUTE : MINOR ARTERIAL
POSTED SPEED = 40 M.P.H.
ADT (YEAR) = 18,700 (2012)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811



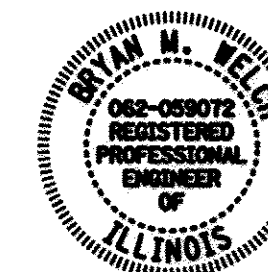
TROY TOWNSHIP LOCATION MAP

GROSS LENGTH = 9,282 FT. = 1.76 MILE
NET LENGTH = 8,933 FT. = 1.69 MILE

CB CHRISTOPHER B. BURKE ENGINEERING, LTD.
9575 W. Higgins Road, Suite 600
Rosemont, Illinois 60018
(847) 823-0500

PROFESSIONAL DESIGN FIRM NO.: 184-001175
EXPIRATION DATE: APRIL 30, 2017

CONTRACT NO. 61D72



Bryan M. Welch
ENGINEER
DATE: 1/10/2017

BRYAN M. WELCH
ILLINOIS REGISTRATION No. 062-059072
EXPIRATION DATE: 11/30/2017

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	
APPROVED	<i>Ge Chapman</i> JANUARY 11, 2017 MAYOR, VILLAGE OF SHOREWOOD
PASSED	<i>C. Thibault</i> JAN 31, 2017 DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS
RELEASED FOR BID BASED ON LIMITED REVIEW	<i>Paul A. Smeto</i> February 1, 2017 REGIONAL ENGINEER

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OF THE STATE OF ILLINOIS

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. (847) 705-4406 SCHAUMBURG, IL

GENERAL NOTES

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SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED APRIL 1, 2016; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2017; THE LATEST EDITIONS OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" (IMUTCD) AND "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS"; THE "DETAILS" IN THE PLANS; AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.

ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST IDOT STANDARD.

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 700 OF THE STANDARD SPECIFICATIONS.

UTILITIES

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS.

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE VILLAGE AND ENGINEER DO NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR AT HIS OWN EXPENSE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.

COORDINATION OF ALL UTILITY WORK INVOLVED IN THE CONSTRUCTION AREA WILL BE DISCUSSED AT THE PRECONSTRUCTION CONFERENCE.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 8-1-1 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS AND CABLE TELEVISION FACILITIES (48 HOURS NOTIFICATIONS IS REQUIRED). THE CONTRACTOR SHALL CONTACT IDOT'S BUREAU OF MATERIALS (PHONE 847-705-4337) AT LEAST 24 HOURS BEFORE PLACING HOT MIX ASPHALT OR PORTLAND CEMENT CONCRETE.

WATER, STORM SEWER AND SANITARY SEWER

ALL EXISTING OR PROPOSED STORM SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR.

THE CONTRACTOR SHALL NOT OPEN OR SHUT ANY WATER VALVES OR FIRE HYDRANTS. CONTACT THE VILLAGE OF SHOREWOOD WATER DEPARTMENT (TEL. NO. 815-725-2150) FOR THEM TO TURN VALVES OR OPERATE HYDRANTS. UNAUTHORIZED USE SHALL SUBJECT THE OFFENDER TO ARREST AND PROSECUTION.

MISCELLANEOUS

ACCESS: THE CONTRACTOR SHALL PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT, EXCEPT FOR PERIODS OF SHORT DURATION. THE COST TO PROVIDE ACCESS WILL BE PAID FOR AS AGGREGATE FOR TEMPORARY ACCESS.

DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

LIMITS OF PATCHING, CURB AND GUTTER REMOVAL AND REPLACEMENT, SIDEWALK REMOVAL AND REPLACEMENT, DRIVEWAY REMOVAL AND REPLACEMENT AND STRUCTURES TO BE ADJUSTED WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.

THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASES ON WHICH THEY ARE TO BE PLACED. PLAN THICKNESSES SHOULD BE CONSIDERED THE MINIMUM THICKNESS PERMITTED.

DETECTABLE WARNINGS FOR THE HANDICAPPED SHALL BE INSTALLED AT ALL INTERSECTING STREETS, DRIVEWAYS, AND ALLEYS AS DIRECTED BY THE ENGINEER.

PAVEMENT GRADES: THE ELEVATIONS INDICATED ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT OR SURFACE COURSE, UNLESS OTHERWISE INDICATED.

RELOCATING EXISTING SIGNS: EXISTING SIGNS WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED AND REINSTALLED UPON COMPLETION OF CONFLICTING IMPROVEMENTS IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" AND THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION". STOP SIGNS, SPEED LIMIT SIGNS, AND STREET NAME SIGNS SHALL BE UP AND VISIBLE AT ALL TIMES.

ALL UNDERGROUND, DRIVEWAY, CONCRETE, AND LANDSCAPE RESTORATION WORK IS TO BE COMPLETED BEFORE THE SURFACE COURSE CAN BE INSTALLED.

FOR HOT-MIX ASPHALT SURFACE REMOVAL, THE EXISTING ASPHALT SURFACE SHALL BE REMOVED TO THE DEPTH SPECIFIED. THE GRINDINGS SHALL BE REMOVED FROM THE SITE AND THE SURFACE MECHANICALLY BROOMED UNTIL THE SURFACE IS COMPLETELY FREE OF ANY LOOSE MATERIAL AND DEBRIS.

CLASS D PATCHES SHALL CONSIST OF REMOVAL AND REPLACEMENT OF EXISTING PAVEMENT AT LOCATIONS DIRECTED BY THE ENGINEER. AFTER MILLING IS COMPLETE, THE EXISTING PAVEMENT INCLUDING THE BASE AND HMA SURFACE, SHALL BE REMOVED TO A DEPTH OF SIX (6) INCHES AND REPLACED WITH SIX (6) INCHES OF HMA MIX. THE SURFACE OF THE PATCH SHALL MEET THE SURFACE OF THE HOT-MIX ASPHALT SURFACE REMOVAL. ALL HOLES, SOFT PLACES AND OTHER DEFECTS IN THE SUBBASE OR SUBGRADE SHALL BE CORRECTED BY THE CONTRACTOR BY REMOVING THE UNSUITABLE MATERIAL, ADDING MORE HMA MIX AS SPECIFIED IN SECTION 406 OF THE STANDARD SPECIFICATIONS.

FRESH OIL SIGNS SHALL BE POSTED AT BOTH ENDS OF THE ROADWAY AND ALL SIDE STREETS AS DIRECTED BY THE ENGINEER. CONSTRUCTION AHEAD SIGNS SHALL BE PLACED AT ALL SIDE STREETS AND BOTH ENDS OF THE ROADWAY WHILE CONSTRUCTION IS IN PROGRESS.

NO CONSTRUCTION SHALL BEGIN UNTIL ALL PROPER TEMPORARY SIGNS AND BARRICADES HAVE BEEN INSTALLED.

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR JOB SITE SAFETY AS WELL AS SUPERVISION/DIRECTION AND MEANS/METHODS OF CONSTRUCTION.

BEFORE BEGINNING CONSTRUCTION, THE CONTRACTOR SHALL VERIFY THE LINE AND GRADES SHOWN ON THE CONTRACT DRAWINGS. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONTRACT DRAWINGS, THE CONTRACTOR SHALL IMMEDIATELY REPORT SAME TO THE ENGINEER PRIOR TO PERFORMING WORK. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION OF WORK AS REQUIRED.

SOIL EROSION PROTECTION SHALL BE IN ACCORDANCE WITH IEPA STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL. ALL DISTURBED AREAS (NOT IMPERVIOUS IN NATURE) SHALL BE FINE GRADED, TOP SOIL RESTORED (MIN 4 INCHES) AND SEED/MULCH APPLIED UNLESS OTHERWISE SPECIFIED ON THE PLANS.

ANY DEFACED WORK SHALL BE CORRECTED OR REPLACED BY THE CONTRACTOR. THE VILLAGE WILL COOPERATE WITH THE CONTRACTOR TO MINIMIZE VANDALISM, BUT THE CONTRACTOR SHALL BE ULTIMATELY RESPONSIBLE TO CORRECT ANY DAMAGE PER CONDITIONS OF ARTICLE 107.30 OF THE STANDARD SPECIFICATIONS.

THE CONTRACTOR SHALL KEEP ONE LANE IN EACH DIRECTION OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION. THE CONTRACTOR SHALL COMPLETE ALL MILLING OPERATIONS BEFORE INSTALLING ANY HOT-MIX ASPHALT PAVEMENT.

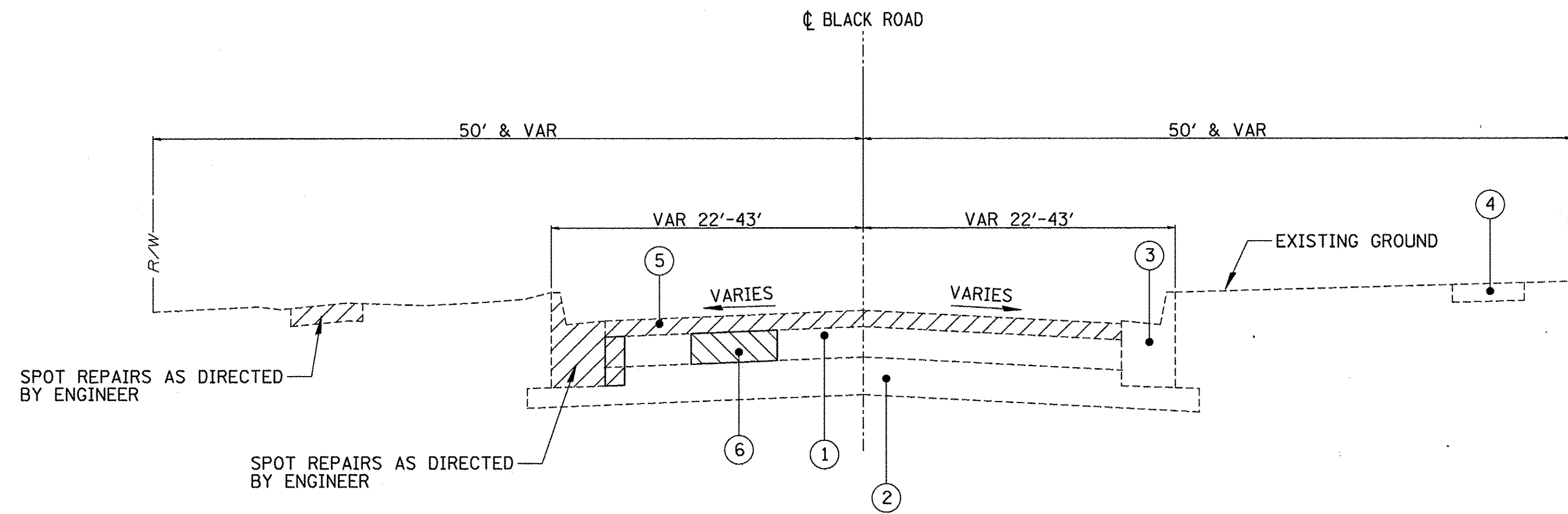
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	PLOT SCALE = NOT TO SCALE	CHECKED - BMW	REVISED -			CONTRACT NO. 61D72					
	PLOT DATE = 1/30/2017	DATE - 1/30/2017	REVISED -			SCALE: NTS		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.	
ILLINOIS FED. AID PROJECT											

SUMMARY OF QUANTITIES

FUNDING SOURCE				TOTAL QUANTITY
LOCATION OF WORK				BLACK ROAD
SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE 0005
CODE NO.	ITEM	UNIT		
20101200	TREE ROOT PRUNING	EACH		5
28000510	INLET FILTERS	EACH		95
* 35101582	AGGREGATE BASE COURSE, TYPE B 2"	SQ YD		23
* 35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD		177
* 35101800	AGGREGATE BASE COURSE, TYPE B 6"	SQ YD		127
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND		38692
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGWAYS	TON		82
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON		2408
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD		130
40600990	TEMPORARY RAMP	SQ YD		130
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON		5618
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD		23
42400800	DETECTABLE WARNINGS	SQ FT		164
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD		54590
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD		150
44000600	SIDEWALK REMOVAL	SQ FT		1890
44201717	CLASS D PATCHES, TYPE II, 6 INCH	SQ YD		50
44201721	CLASS D PATCHES, TYPE III, 6 INCH	SQ YD		20
44201723	CLASS D PATCHES, TYPE IV, 6 INCH	SQ YD		120
60266600	VALVE BOXES TO BE ADJUSTED	EACH		5
67100100	MOBILIZATION	LSUM		1
70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	LSUM		1
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	LSUM		1
70102634	TRAFFIC CONTROL AND PROTECTION, STANDARD 701611	LSUM		1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM		1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM		1

FUNDING SOURCE				TOTAL QUANTITY
LOCATION OF WORK				BLACK ROAD
SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE 0005
CODE NO.	ITEM	UNIT		
~ 78009000	MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT		938
~ 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT		29620
~ 78009006	MODIFIED URETHANE PAVEMENT MARKING - LINE 6"	FOOT		4362
~ 78009012	MODIFIED URETHANE PAVEMENT MARKING - LINE 12"	FOOT		2201
~ 78009024	MODIFIED URETHANE PAVEMENT MARKING - LINE 24"	FOOT		236
~ 78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH		417
* ~ 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH		3
* ~ 88600600	DETECTOR LOOP REPLACEMENT	FOOT		2028
* ~ 89502376	REBUILD EXISTING HANDHOLE	EACH		4
* X2520650	SODDING, SALT TOLERANT (SPECIAL)	SQ YD		1656
* X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH		1
* X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH		21
* X4023000	TEMPORARY ACCESS (ROAD)	EACH		19
* X4240430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL	SQ FT		1550
* X7015005	CHANGEABLE MESSAGE SIGN	CAL DA		300
* ~ X7810300	RECESSED REFLECTIVE PAVEMENT MARKER	EACH		952
* ~ X7830068	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS	SQ FT		938
* ~ X7830070	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT		29620
* ~ X7830074	GROOVING FOR RECESSED PAVEMENT MARKING 7"	FOOT		4362
* ~ X7830078	GROOVING FOR RECESSED PAVEMENT MARKING 13"	FOOT		2201
* ~ X7830090	GROOVING FOR RECESSED PAVEMENT MARKING 25"	FOOT		236
* Z0004510	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"	SQ YD		127
* Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT		1634
* Z0013798	CONSTRUCTION LAYOUT	LSUM		1
* Z0017400	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED	EACH		31
* Z0017500	DRAINAGE & UTILITY STRUCTURE ADJUSTMENT (SPECIAL)	EACH		39

* INDICATES SPECIAL PROVISION
~ INDICATES SPECIALTY ITEM



EXISTING TYPICAL SECTION

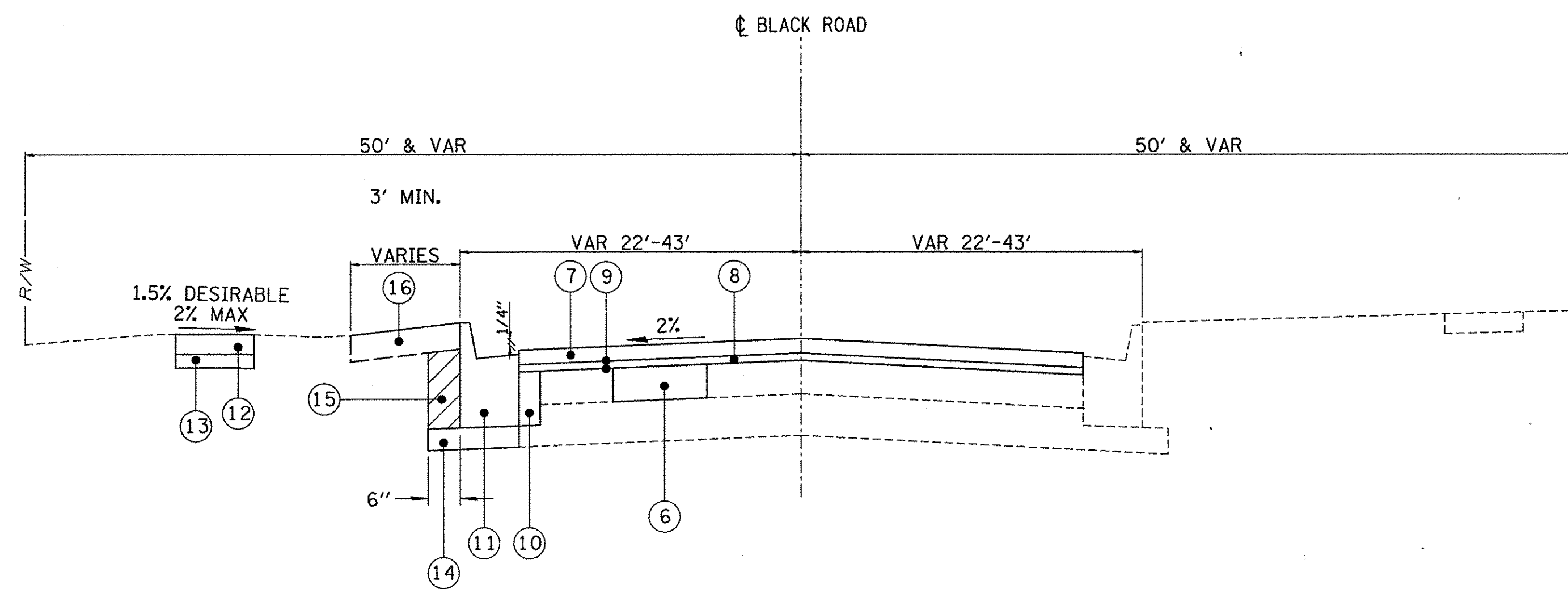
STA 104+33.00 TO STA 150+58.15, BLACK ROAD
 STA 154+06.89 TO STA 197+15.00, BLACK ROAD

LEGEND

- ① EXISTING HMA PAVEMENT (SEE PAVEMENT CORES)
- ② EXISTING AGGREGATE BASE (SEE PAVEMENT CORES)
- ③ EXISTING CONCRETE CURB AND GUTTER (TYPE VARIES)
- ④ EXISTING PCC SIDEWALK
- ⑤ HMA SURFACE REMOVAL, 2 1/4"
- ⑥ CLASS D PATCHES, TYPE SPECIFIED, 6"
- ⑦ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 3/4"
- ⑧ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 3/4"
- ⑨ BITUMINOUS MATERIALS (TACK COAT)
- ⑩ CLASS SI PCC, VIBRATED IN PLACE - TO BE POURED WITHIN 48 HOURS OF C&G (INCLUDED IN COST OF PROPOSED C&G)
- ⑪ COMBINATION CONCRETE C&G (MATCH EXISTING TYPE) - AS DIRECTED BY ENGINEER
- ⑫ PCC SIDEWALK, 5" - AS DIRECTED BY ENGINEER
- ⑬ AGGREGATE BASE COURSE, TYPE B, 4"
- ⑭ SUB-BASE GRANULAR MATERIAL, TYPE B, 4" (INCLUDED IN COST OF PROPOSED C&G)
- ⑮ EXCAVATION AND SUITABLE BACKFILL (INCLUDED IN COST OF PROPOSED C&G)
- ⑯ SODDING, SALT TOLERANT (SPECIAL)

NOTES:

- 1. CONTRACTOR SHALL SAWCUT PAVEMENT PRIOR TO REMOVING THE CURB.
- 2. CONTRACTOR SHALL MILL PRIOR TO PATCHING.
- 3. PAVEMENT REMOVAL ADJACENT TO PROPOSED CURB AND GUTTER SHALL BE INCLUDED IN THE COST OF THE PROPOSED CURB AND GUTTER.



PROPOSED TYPICAL SECTION

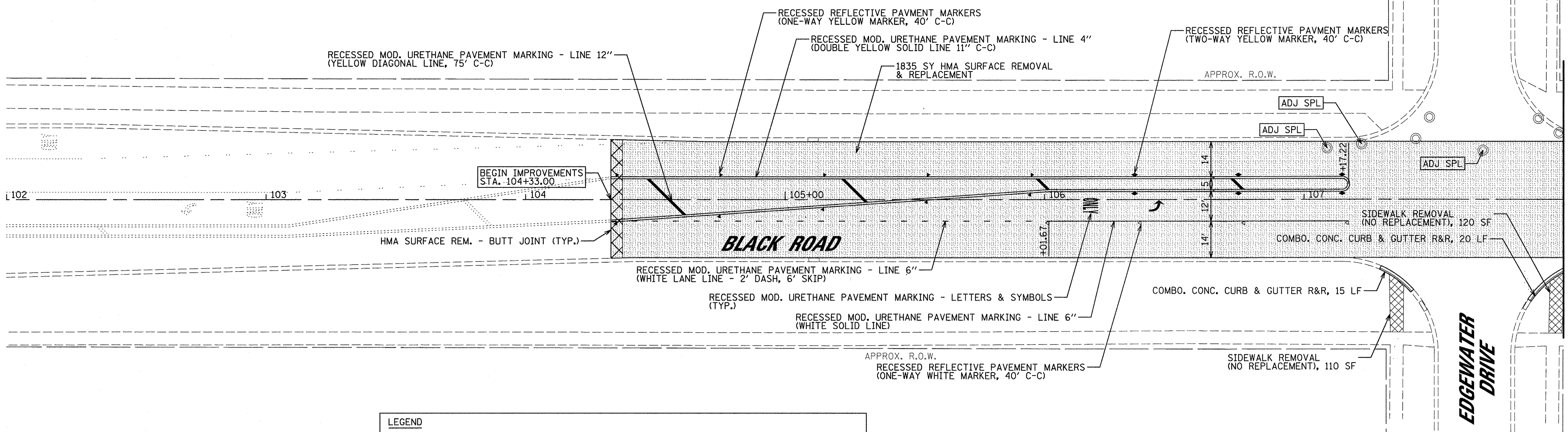
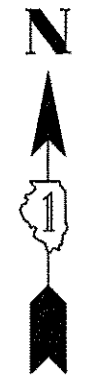
STA 104+33.00 TO STA 150+58.15, BLACK ROAD
 STA 154+06.89 TO STA 197+15.00, BLACK ROAD

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE ITEM	AIR VOIDS @ Ndes
PAVEMENT RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5 MM), 1 3/4"	4% @ 70 GYR
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"	3.5% @ 50 GYR
DRIVEWAYS	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 MM), 3" (2 LIFTS)	4% @ 50 GYR
PATCHING	
CLASS D PATCHES, 6" (HMA BINDER IL-19 MM) NOTE: SAW CUT PATCHES PRIOR TO REMOVAL	4% @ 70 GYR

NOTES:

- 1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
- 2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP AND RAS" SEE DISTRICT ONE SPECIAL PROVISIONS.

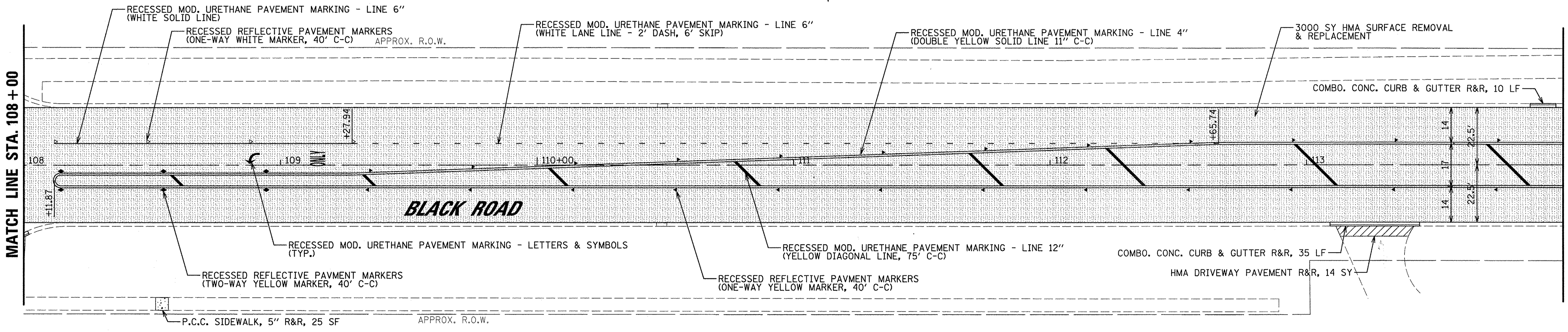
SCALE 1" = 20'



MATCH LINE STA. 108 + 00

LEGEND	
	HMA SURFACE REMOVAL & REPLACEMENT
	SIDEWALK REMOVAL & REPLACEMENT
	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED (SPECIAL)
	CLASS D PATCHES, 6"
	COMBO. CONC. CURB & GUTTER REMOVAL & REPLACEMENT
	DRIVEWAY PAVEMENT REMOVAL & REPLACEMENT
	REBUILD EXISTING HANDHOLE

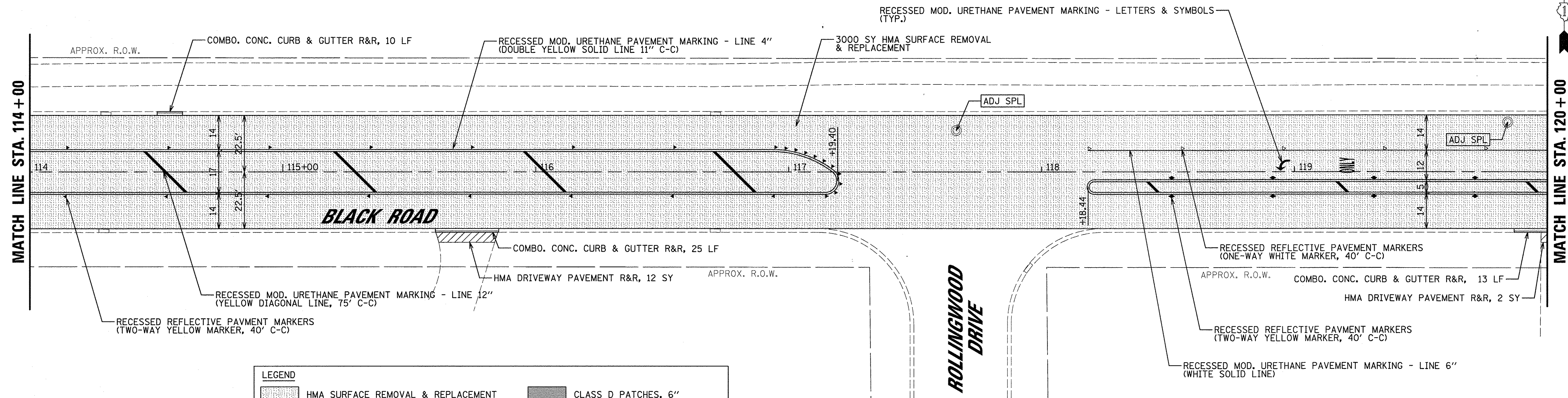
NOTE: ALL SIDEWALK AND CURB RAMPS SHALL BE INSTALLED IN FULL COMPLIANCE WITH ADA/PROWAG REQUIREMENTS.



MATCH LINE STA. 108 + 00

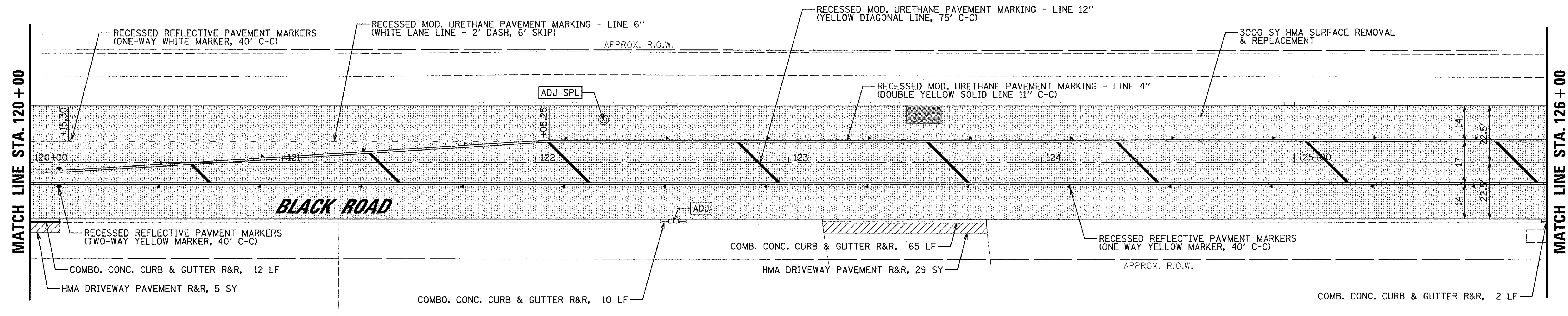
MATCH LINE STA. 114 + 00

FILE NAME =	USER NAME = bweilch	DESIGNED - AJS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BLACK ROAD RESURFACING PROPOSED PLAN		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\Shorewood\070001\070001C\2015\C15-03	Civil\PLN_C1503_01.sht	DRAWN - AJS	REVISED -				0298	14-00038-00-RS	WILL	22	5
PLOT SCALE = 20'	CHECKED - BMW	DATE - 1/30/2017	REVISED -				CONTRACT NO. 61D72				
PLOT DATE = 1/30/2017	DATE - 1/30/2017	REVISED -	ILLINOIS FED. AID PROJECT								
				SCALE: 20'	SHEET NO. 1 OF 9 SHEETS	STA. TO STA.					



LEGEND	
	HMA SURFACE REMOVAL & REPLACEMENT
	SIDEWALK REMOVAL & REPLACEMENT
	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED (SPECIAL)
	CLASS D PATCHES, 6"
	COMBO. CONC. CURB & GUTTER REMOVAL & REPLACEMENT
	DRIVEWAY PAVEMENT REMOVAL & REPLACEMENT
	REBUILD EXISTING HANDHOLE

NOTE:
ALL SIDEWALK AND CURB RAMPS SHALL BE INSTALLED
IN FULL COMPLIANCE WITH ADA/PROWAG REQUIREMENTS.



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CHECKED - BMW
DATE - 1/30/2017

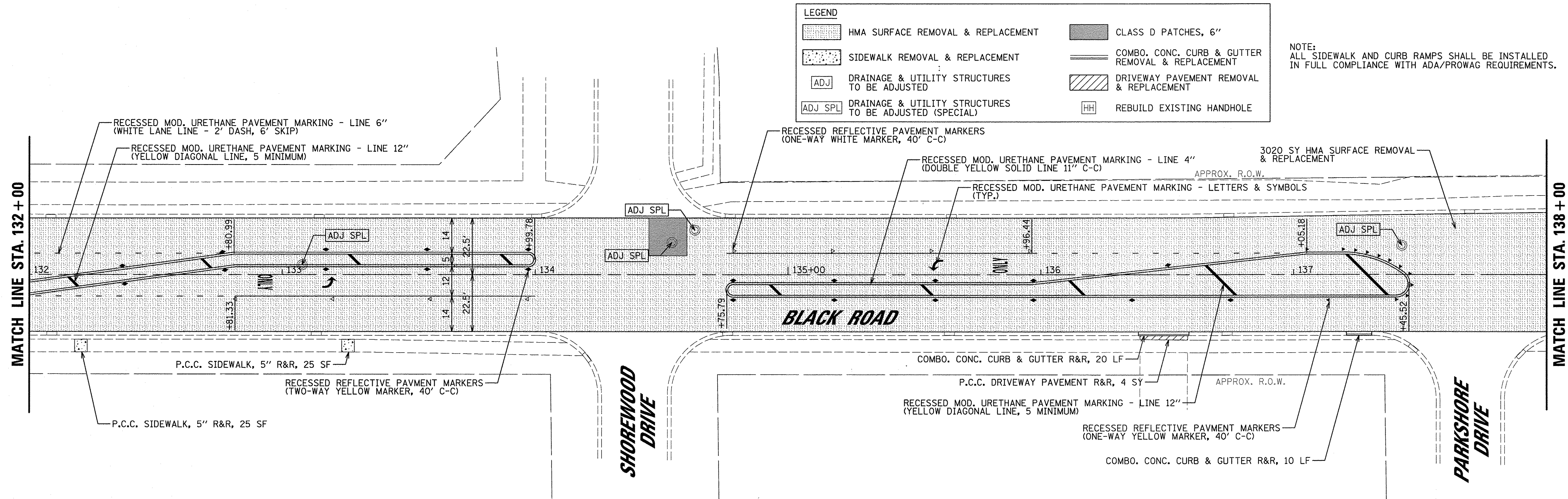
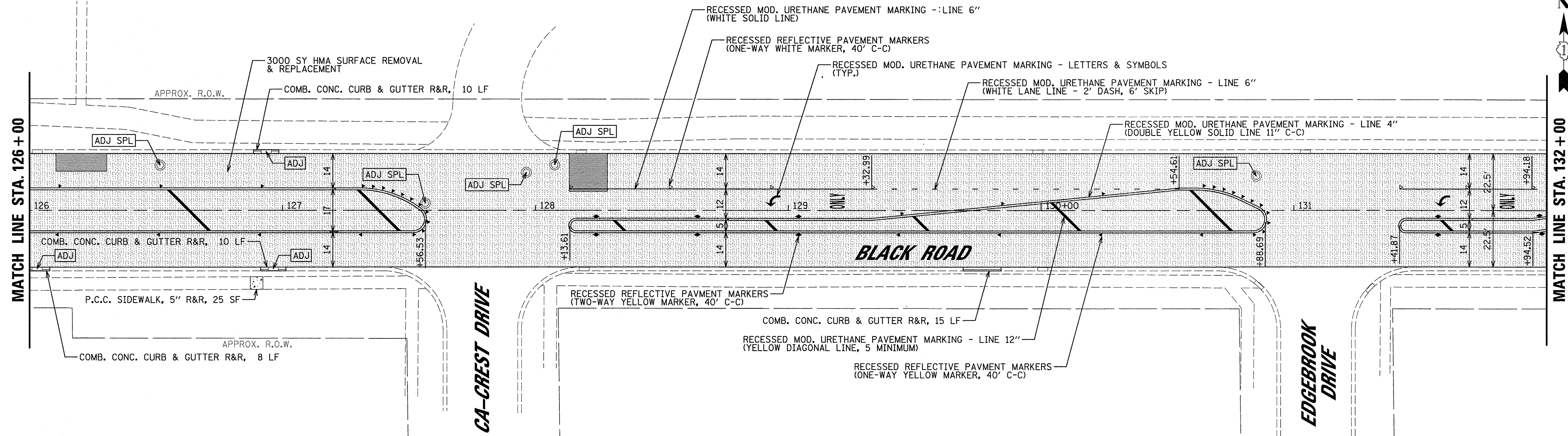
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BLACK ROAD RESURFACING
PROPOSED PLAN**

SCALE: 20' SHEET NO. 2 OF 9 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0298	14-00038-00-RS	WILL	22	6
CONTRACT NO. 61D72				
ILLINOIS FED. AID PROJECT				



LEGEND	
	HMA SURFACE REMOVAL & REPLACEMENT
	SIDEWALK REMOVAL & REPLACEMENT
	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED (SPECIAL)
	CLASS D PATCHES, 6"
	COMBO. CONC. CURB & GUTTER REMOVAL & REPLACEMENT
	DRIVEWAY PAVEMENT REMOVAL & REPLACEMENT
	REBUILD EXISTING HANDHOLE

NOTE: ALL SIDEWALK AND CURB RAMPS SHALL BE INSTALLED IN FULL COMPLIANCE WITH ADA/PROWAG REQUIREMENTS.

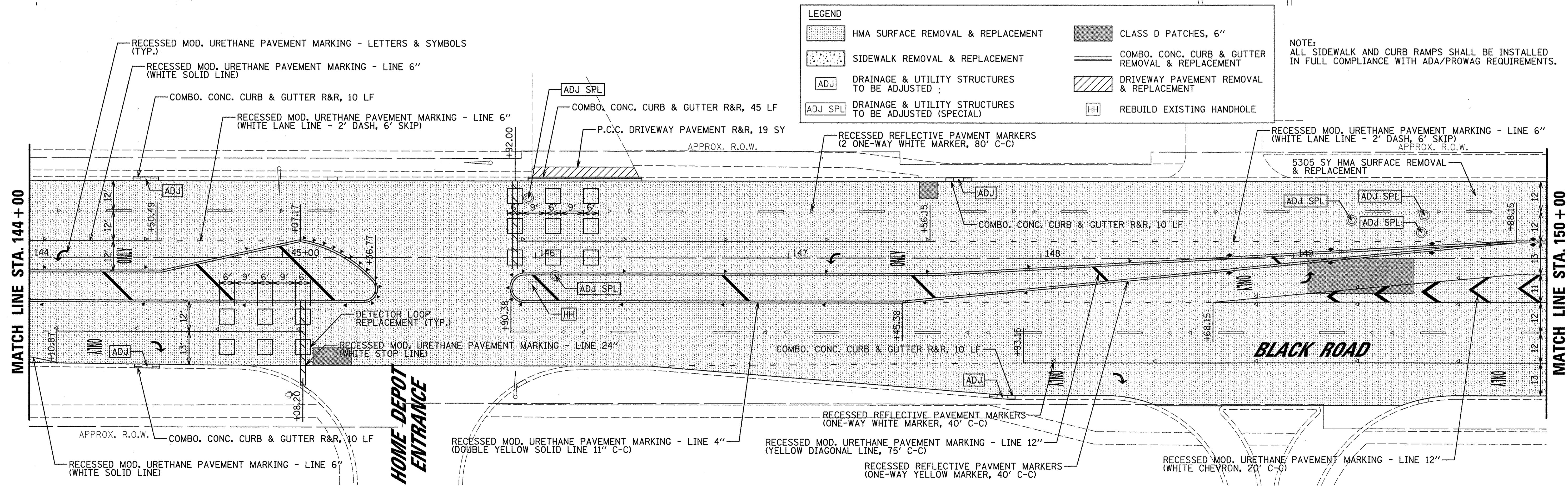
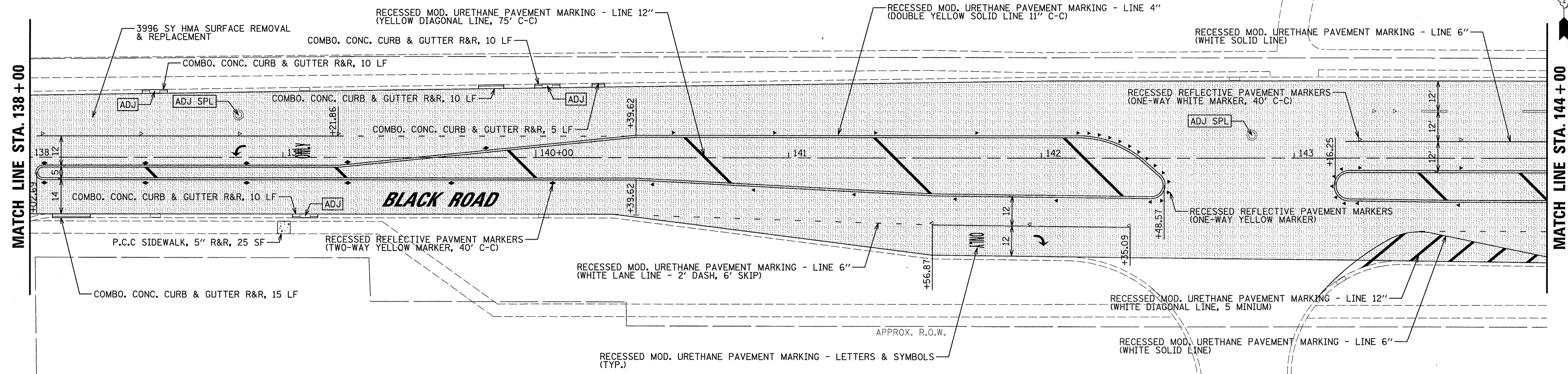
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	PLOT DATE = 1/30/2017	DATE - 1/30/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BLACK ROAD RESURFACING
PROPOSED PLAN

SCALE: 20' SHEET NO. 3 OF 9 SHEETS STA. TO STA.

F.A.U. RTE. 0298	SECTION 14-00038-00-RS	COUNTY WILL	TOTAL SHEETS 22	SHEET NO. 7
CONTRACT NO. 61D72			ILLINOIS FED. AID PROJECT	



LEGEND

HMA SURFACE REMOVAL & REPLACEMENT	CLASS D PATCHES, 6"
SIDEWALK REMOVAL & REPLACEMENT	COMBO. CONC. CURB & GUTTER REMOVAL & REPLACEMENT
DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED	DRIVEWAY PAVEMENT REMOVAL & REPLACEMENT
DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED (SPECIAL)	REBUILD EXISTING HANDHOLE

NOTE:
ALL SIDEWALK AND CURB RAMPS SHALL BE INSTALLED IN FULL COMPLIANCE WITH ADA/PROWAG REQUIREMENTS.

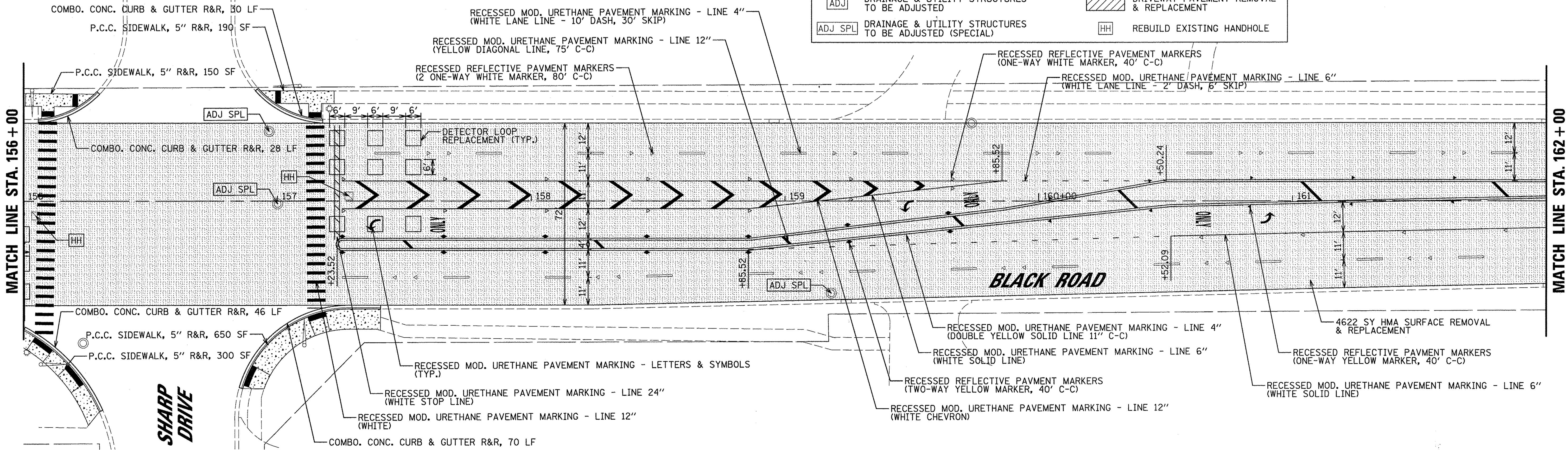
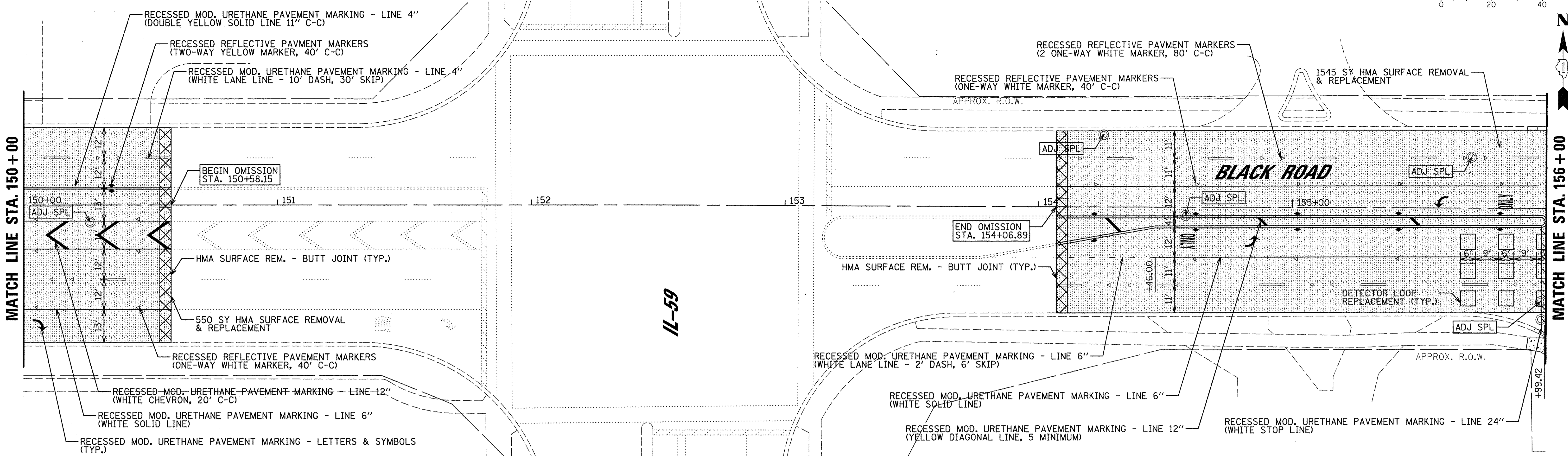
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	PLOT DATE = 1/30/2017	DATE - 1/30/2017	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BLACK ROAD RESURFACING
PROPOSED PLAN**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0298	14-00038-00-RS	WILL	22	8
CONTRACT NO. 61D72			ILLINOIS FED. AID PROJECT	

SCALE: 20' SHEET NO. 4 OF 9 SHEETS STA. TO STA.



LEGEND

	HMA SURFACE REMOVAL & REPLACEMENT		CLASS D PATCHES, 6"
	SIDEWALK REMOVAL & REPLACEMENT		COMBO. CONC. CURB & GUTTER REMOVAL & REPLACEMENT
	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED		DRIVEWAY PAVEMENT REMOVAL & REPLACEMENT
	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED (SPECIAL)		REBUILD EXISTING HANDHOLE

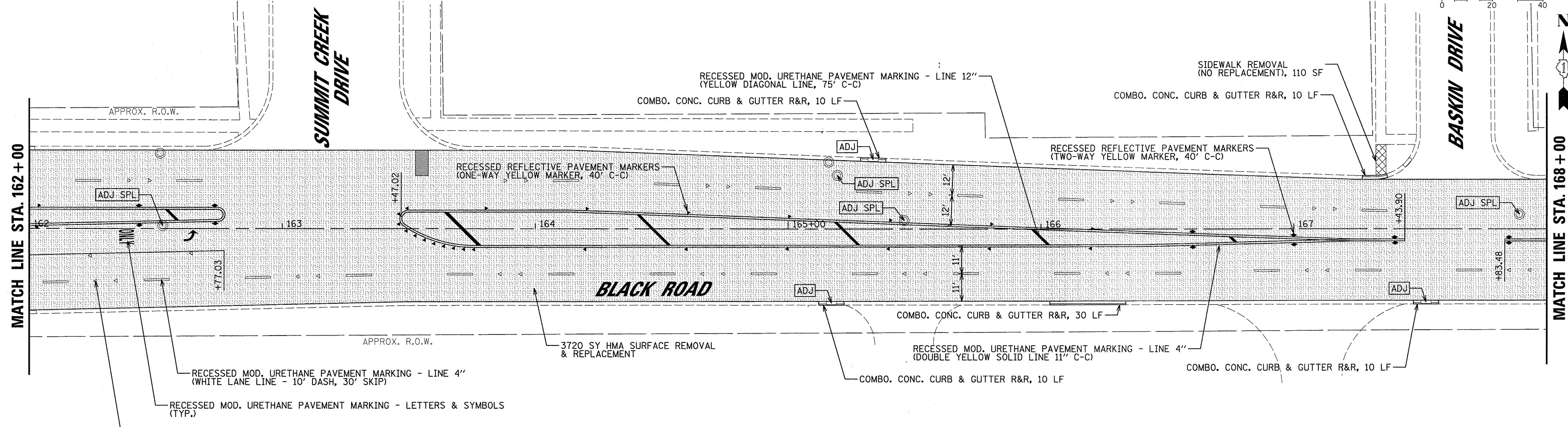
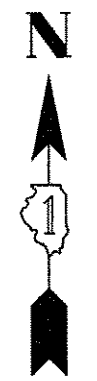
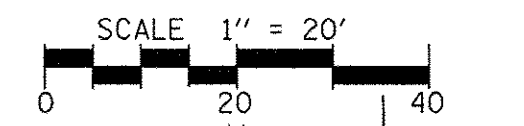
NOTE: ALL SIDEWALK AND CURB RAMPS SHALL BE INSTALLED IN FULL COMPLIANCE WITH ADA/PROWAG REQUIREMENTS.

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	PLOT SCALE = 20'	CHECKED - BMW	REVISED -
	PLOT DATE = 1/30/2017	DATE - 1/30/2017	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BLACK ROAD RESURFACING PROPOSED PLAN	
SCALE: 20'	SHEET NO. 5 OF 9 SHEETS STA. TO STA.

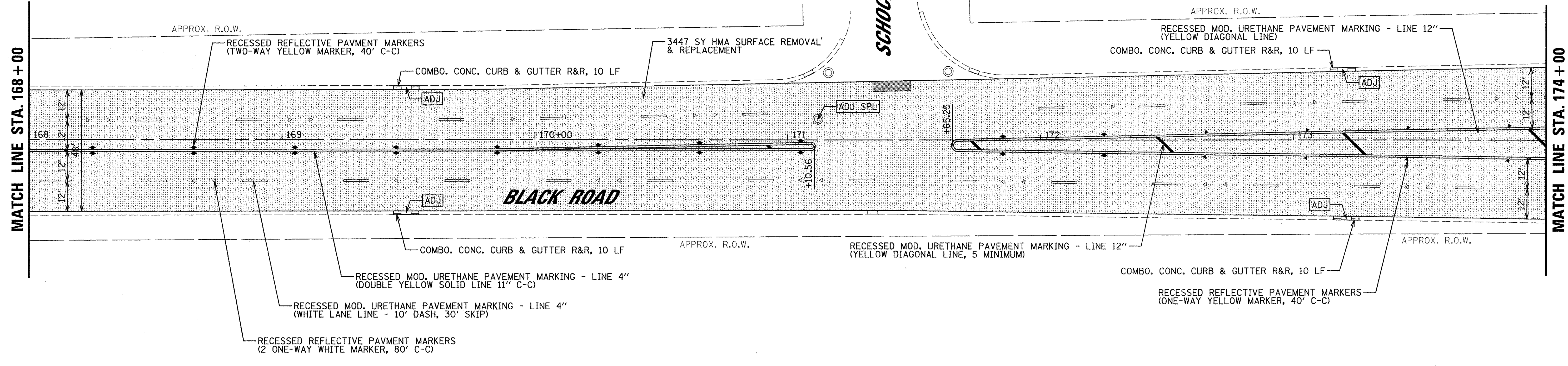
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0298	14-00038-00-RS	WILL	22	9
CONTRACT NO. 61D72			ILLINOIS FED. AID PROJECT	



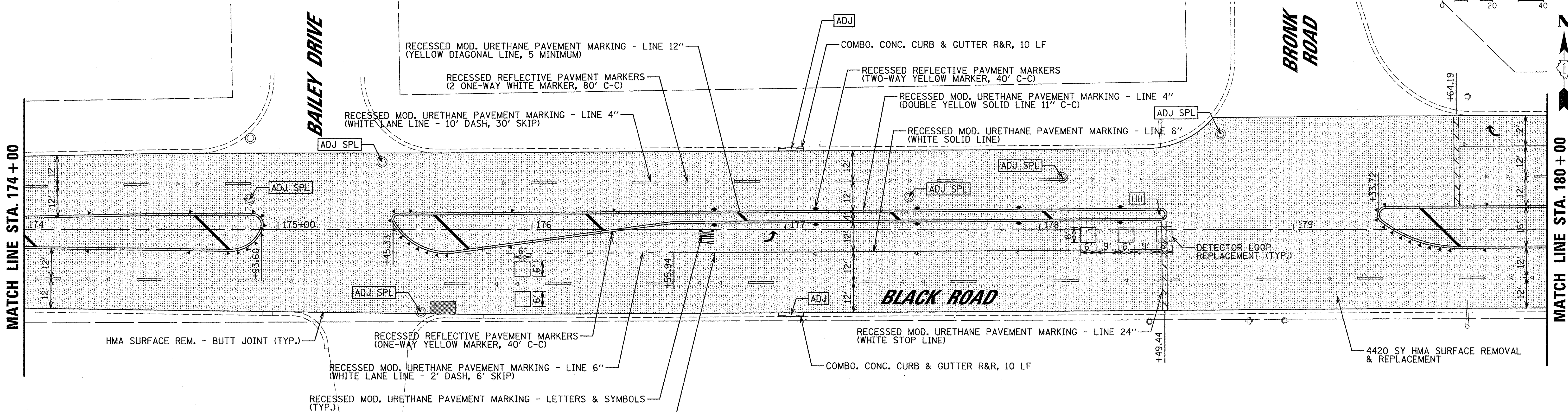
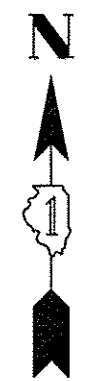
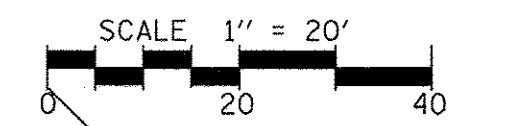
NOTE:
ALL SIDEWALK AND CURB RAMPS SHALL BE INSTALLED
IN FULL COMPLIANCE WITH ADA/PROWAG REQUIREMENTS.

LEGEND

<ul style="list-style-type: none"> HMA SURFACE REMOVAL & REPLACEMENT SIDEWALK REMOVAL & REPLACEMENT DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED (SPECIAL) 	<ul style="list-style-type: none"> CLASS D PATCHES, 6" COMBO. CONC. CURB & GUTTER REMOVAL & REPLACEMENT DRIVEWAY PAVEMENT REMOVAL & REPLACEMENT REBUILD EXISTING HANDHOLE
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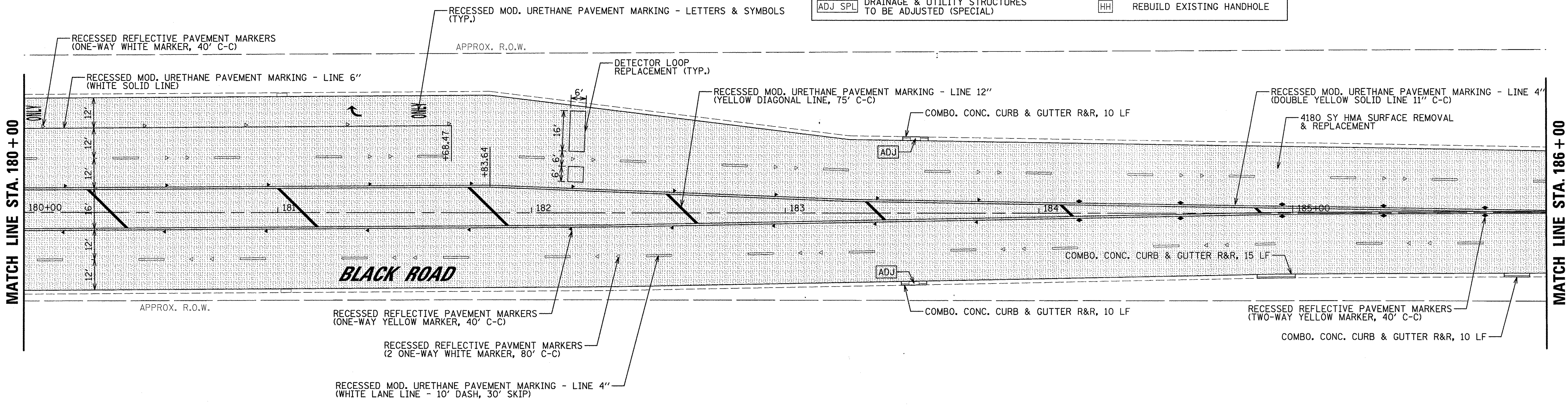


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		DRAWN - AJS	REVISED -			SCALE: 20' SHEET NO. 6 OF 9 SHEETS STA. TO STA.					
		CHECKED - BMW	REVISED -			ILLINOIS FED. AID PROJECT					
		DATE - 1/30/2017	REVISED -			CONTRACT NO. 61D72					



LEGEND	
	HMA SURFACE REMOVAL & REPLACEMENT
	SIDEWALK REMOVAL & REPLACEMENT
	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED (SPECIAL)
	CLASS D PATCHES, 6"
	COMBO. CONC. CURB & GUTTER REMOVAL & REPLACEMENT
	DRIVEWAY PAVEMENT REMOVAL & REPLACEMENT
	REBUILD EXISTING HANDHOLE

NOTE:
ALL SIDEWALK AND CURB RAMPS SHALL BE INSTALLED IN FULL COMPLIANCE WITH ADA/PROWAG REQUIREMENTS.



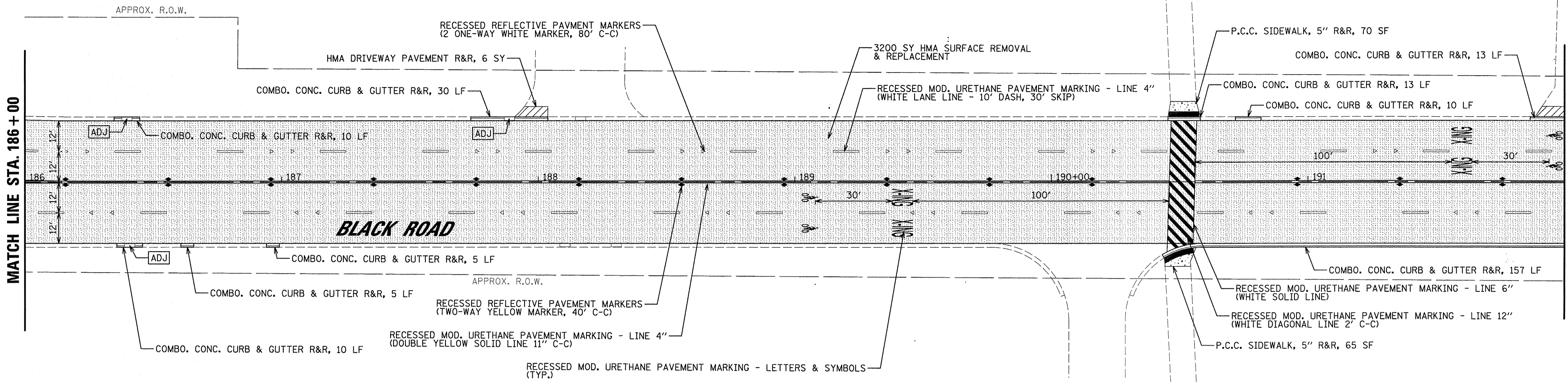
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	PLOT DATE = 1/30/2017	DATE - 1/30/2017	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BLACK ROAD RESURFACING
PROPOSED PLAN**

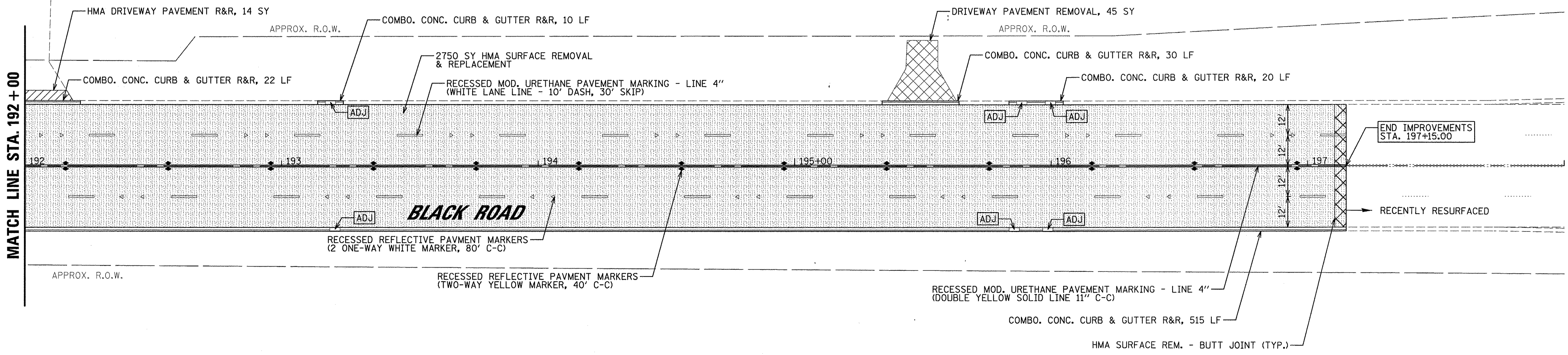
SCALE: 20' SHEET NO. 7 OF 9 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0298	14-00038-00-RS	WILL	22	11
CONTRACT NO. 61D72				
ILLINOIS FED. AID PROJECT				

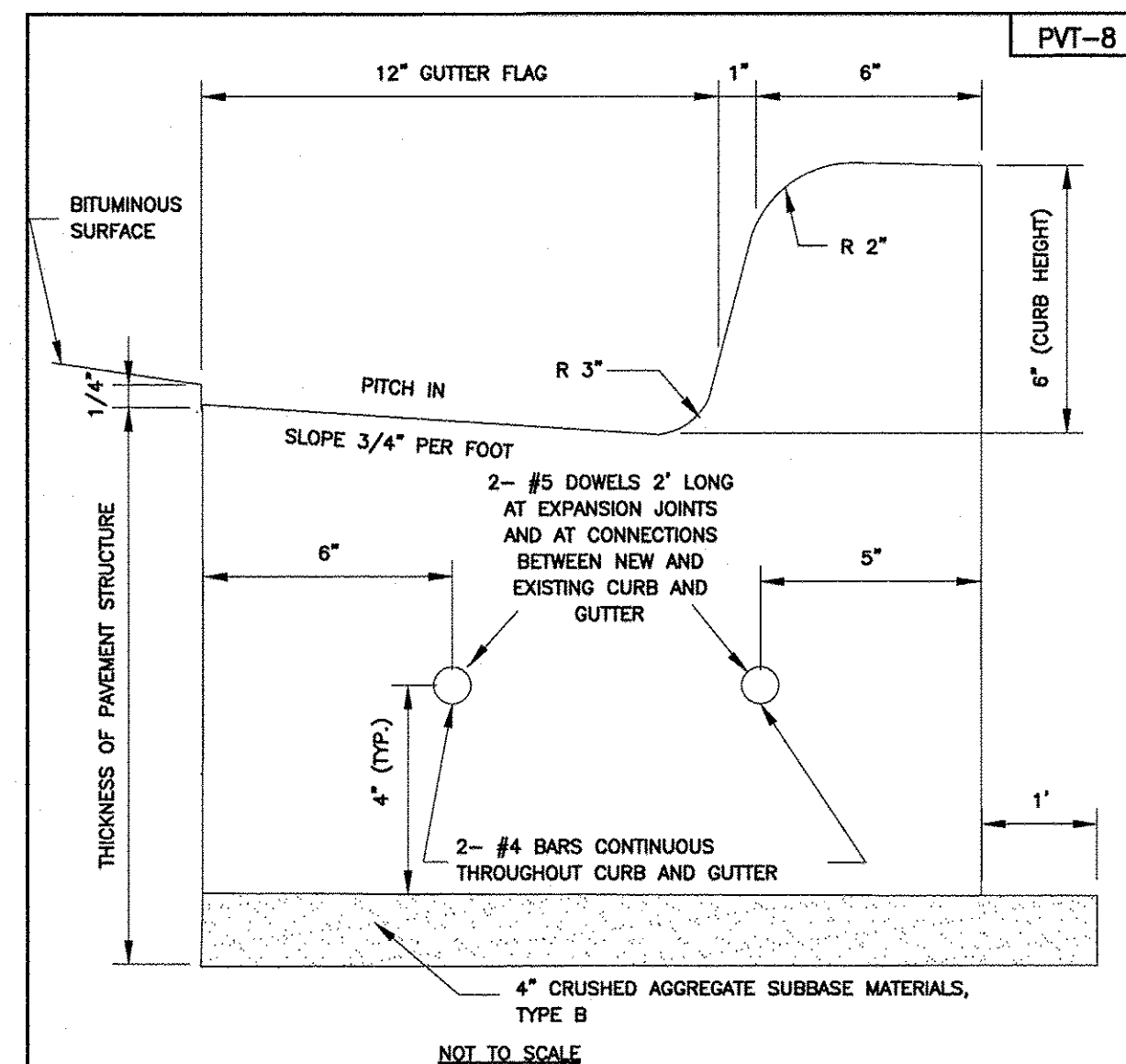


LEGEND	
	HMA SURFACE REMOVAL & REPLACEMENT
	SIDEWALK REMOVAL & REPLACEMENT
	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED (SPECIAL)
	CLASS D PATCHES, 6"
	COMBO. CONC. CURB & GUTTER REMOVAL & REPLACEMENT
	DRIVEWAY PAVEMENT REMOVAL & REPLACEMENT
	REBUILD EXISTING HANDHOLE

NOTE: ALL SIDEWALK AND CURB RAMPS SHALL BE INSTALLED IN FULL COMPLIANCE WITH ADA/PROWAG REQUIREMENTS.

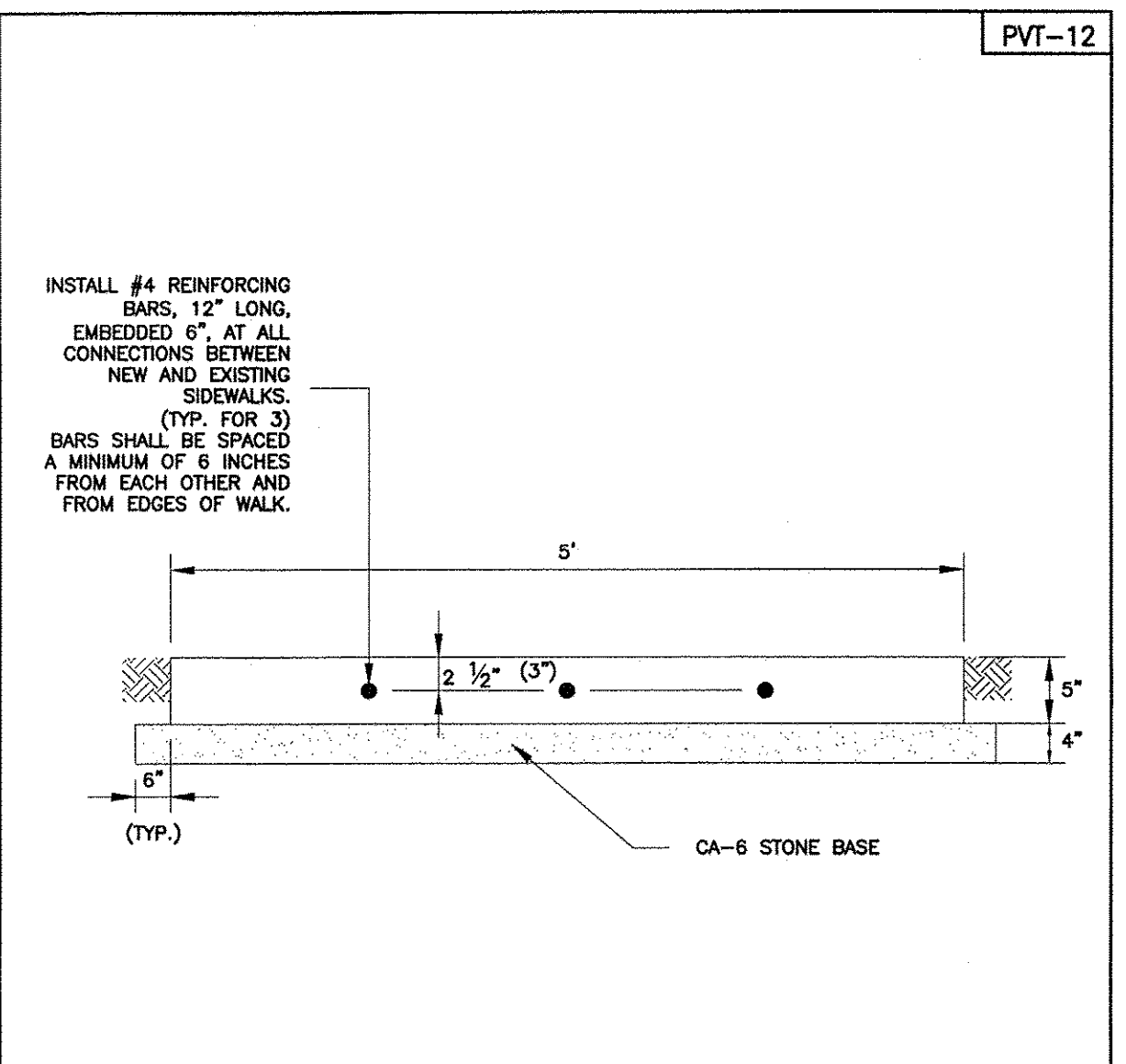


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N:\Shorewood\078001\078001C\2015\C15-03	C:\PLN\C1503_08.sht	DRAWN - AJS	REVISED -			SCALE: 20'	SHEET NO. 8 OF 9 SHEETS	STA. TO STA.	CONTRACT NO. 61D72			
		CHECKED - BMW	REVISED -			ILLINOIS FED. AID PROJECT						
		DATE - 1/30/2017	REVISED -									



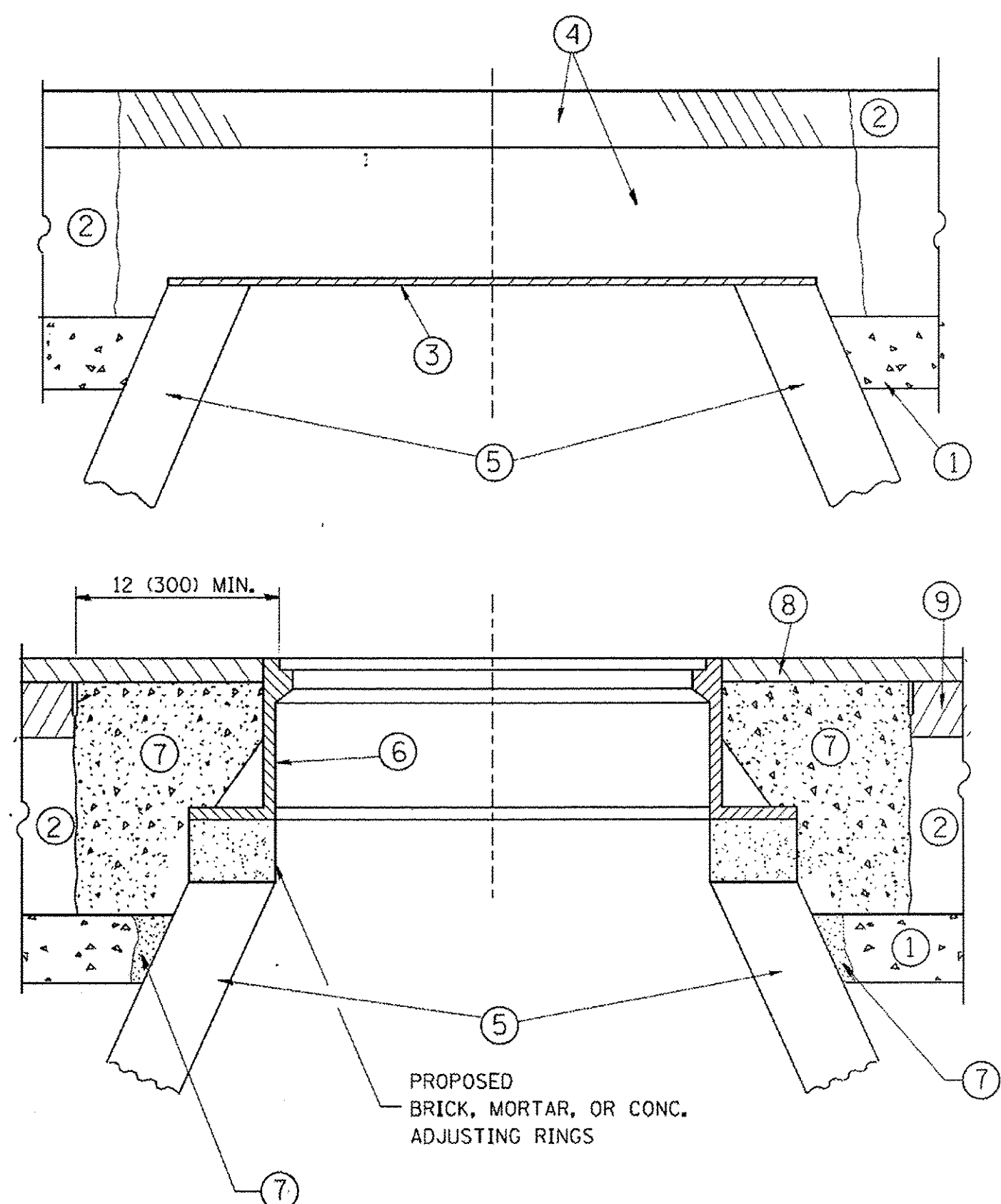
NOTES:
 1. ALL CURB AND GUTTER SHALL BE CONSTRUCTED WITH IDOT CLASS SI CONCRETE.
 2. CONTRACTION JOINTS SHALL BE SAW-CUT AT 20' INTERVALS AND CAULKED.
 3. PREFORMED EXPANSION JOINTS, 3/4" THICK, SHALL BE PLACED FIVE FEET EITHER SIDE OF STORM STRUCTURES IN CURB AND GUTTER, AT CURB RETURNS AND AT POINTS OF CURVATURE, AT ALL CONNECTIONS BETWEEN NEW AND EXISTING CURB AND GUTTER, AND AT 100' INTERVALS ON TANGENTS.
 4. CURB AND GUTTER AT STORM STRUCTURES SHALL BE BOXED-OUT AND HAND-FORMED BETWEEN EXPANSION JOINTS. FORMS SHALL BE PLACED AND INSPECTED BY VILLAGE PRIOR TO POURING CONCRETE. STRUCTURE FRAMES SHALL BE PLACED AND ADJUSTED PRIOR TO THIS INSPECTION.
 5. THE FOLLOWING SHALL BE STAMPED IN THE CURB AT THE INDICATED LOCATIONS:
 "W" FOR WATER SERVICES
 "S" FOR SANITARY SEWER SERVICES
 "ST" FOR STORM SEWER SERVICES
 "CO" FOR STORM SEWER SERVICE LINE CLEAN-OUTS
 6. DOWELS AT EXPANSION JOINTS SHALL BE CENTERED ON THE JOINT (DRILLED INTO EXISTING CURB AND GUTTER), AND SHALL BE INSTALLED WITH GREASE CAPS ON ONE SIDE.

CONCRETE CURB & GUTTER
 TYPE B-6.12
 DATE: JUNE 2007
 -V15-070408.PLT_C1503-01.dwg



NOTES:
 1. ALL SIDEWALK SHALL BE CONSTRUCTED WITH IDOT CLASS "SI" CONCRETE WITH A MINIMUM COMPRESSIVE STRENGTH OF 3,500 PSI AT 14 DAYS
 2. PREFORMED EXPANSION JOINTS (3/4" THICK) SHALL BE CONSTRUCTED IN SIDEWALK EVERY 100 FEET AND AT ALL ABUTTING DRIVEWAYS AND CURB AND GUTTER.
 3. TOOLED CONTRACTION JOINTS SHALL BE CONSTRUCTED IN SIDEWALK EVERY FIVE FEET.
 4. SIDEWALK SHALL HAVE 1/4" PER FOOT CROSS-SLOPE.
 5. WELDED WIRE FABRIC (6X6/6X6) OR FIBER MESH SHALL BE INSTALLED THROUGH DRIVEWAYS AT 2" ABOVE SLAB BOTTOM.
 6. FORMBOARD REQUIREMENTS: MINIMUM 2" X 6".
 7. USE 2#4 REINFORCING BARS, 10' LONG OVER ALL UTILITY TRENCHES FOR NEW SIDEWALK.
 8. AT DRIVEWAYS, SIDEWALK PCC AND BASE THICKNESS SHALL MATCH THAT OF THE DRIVEWAY.

VILLAGE OF SHOREWOOD
 STANDARD DETAIL
 FOR
 SIDEWALK
 DATE: JUNE 2007
 -V15-070408.PLT_C1503-01.dwg



CONSTRUCTION PROCEDURES

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
 - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
 - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
 - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.
- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- *UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

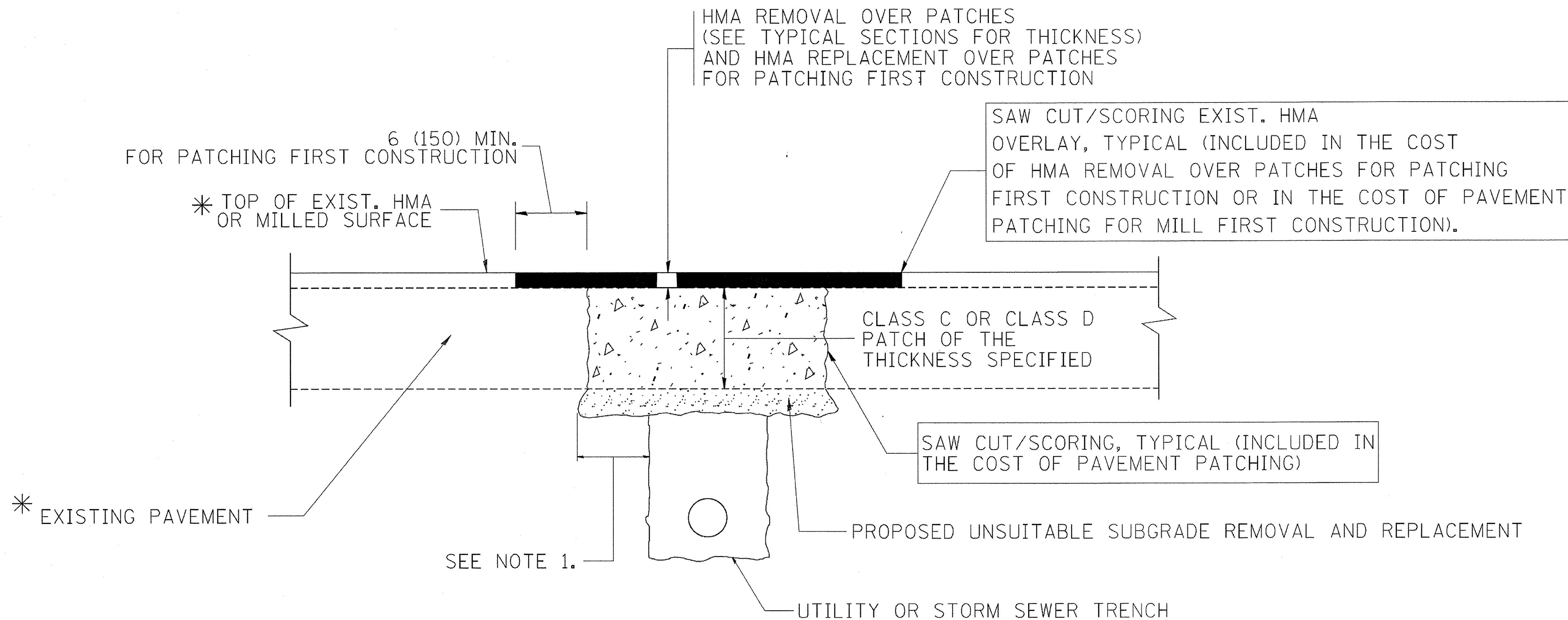
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
ct\pw_work\pwt\dot\bauerdl\20109315\bd08.dgn		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 1/8" = 1'-0"	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE. 0298	SECTION 14-00038-00-RS	COUNTY WILL	TOTAL SHEETS 22	SHEET NO. 14
BD600-03 (BD-8)		CONTRACT NO. 61D72		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

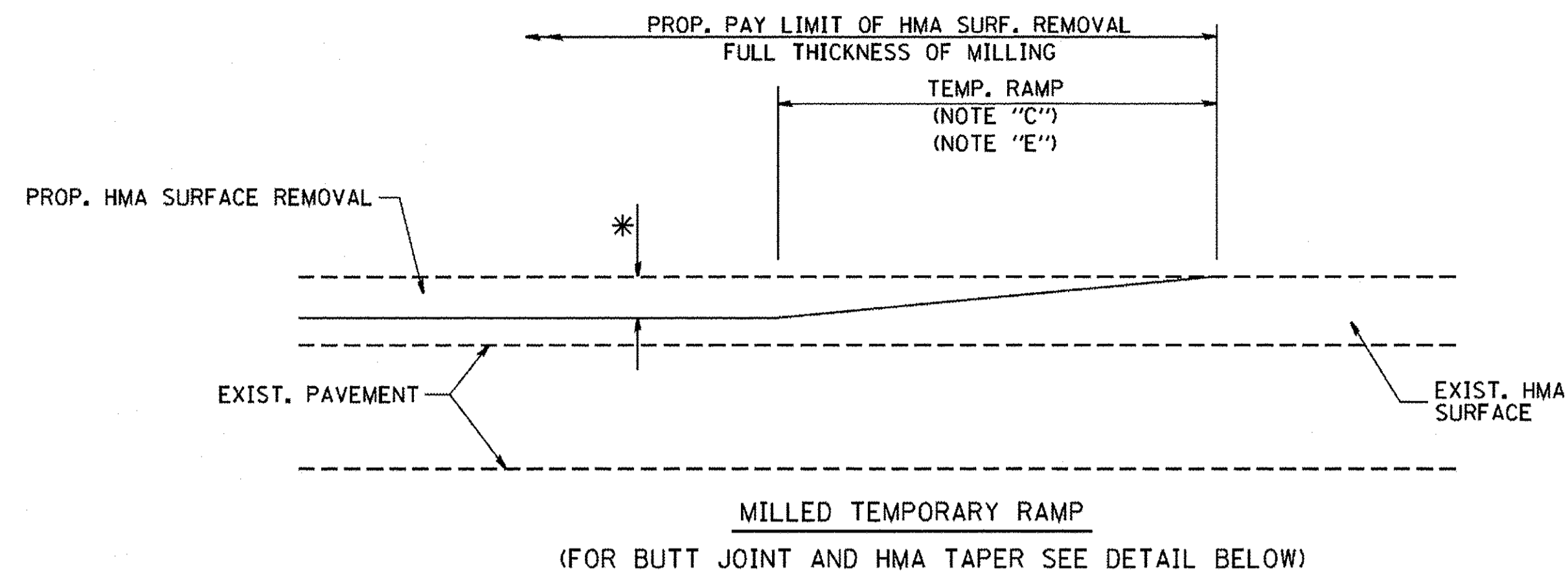
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

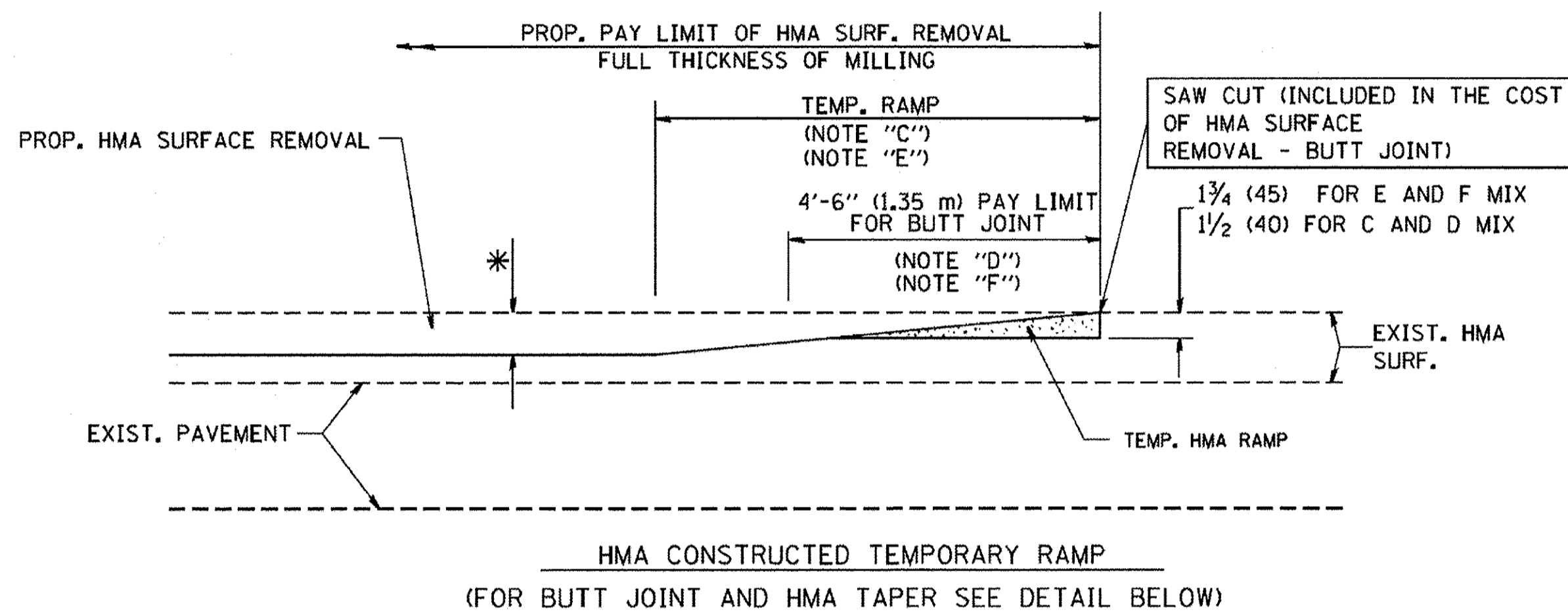
1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = cr\projects\diststd22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A. RTE. *	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - R. BORO 01-01-07					0298	14-00038-00-RS	WILL	22	15
PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08	SCALE: NONE		SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BD400-04 (BD-22)		CONTRACT NO.	61D72	
							FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

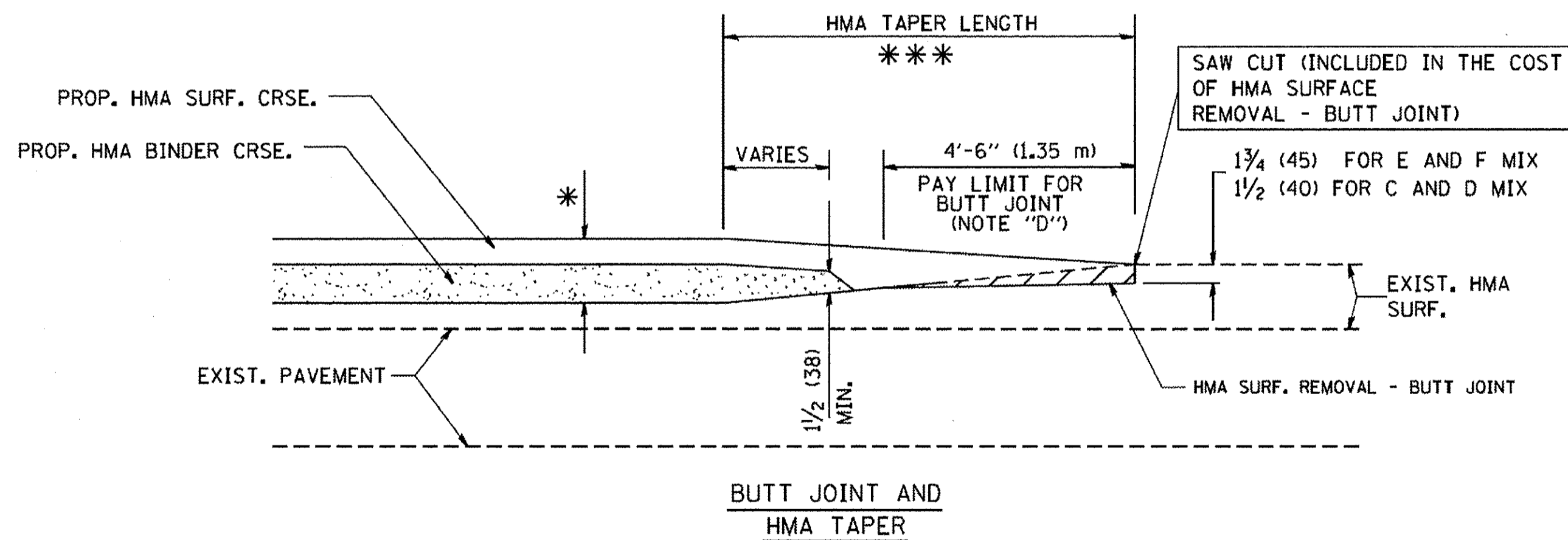


OPTION 1

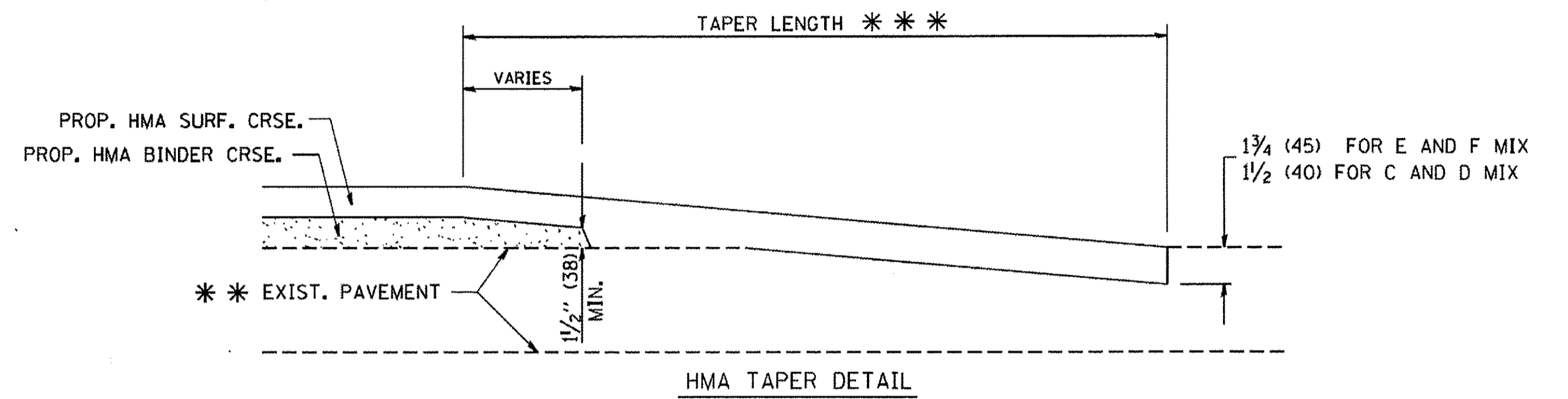
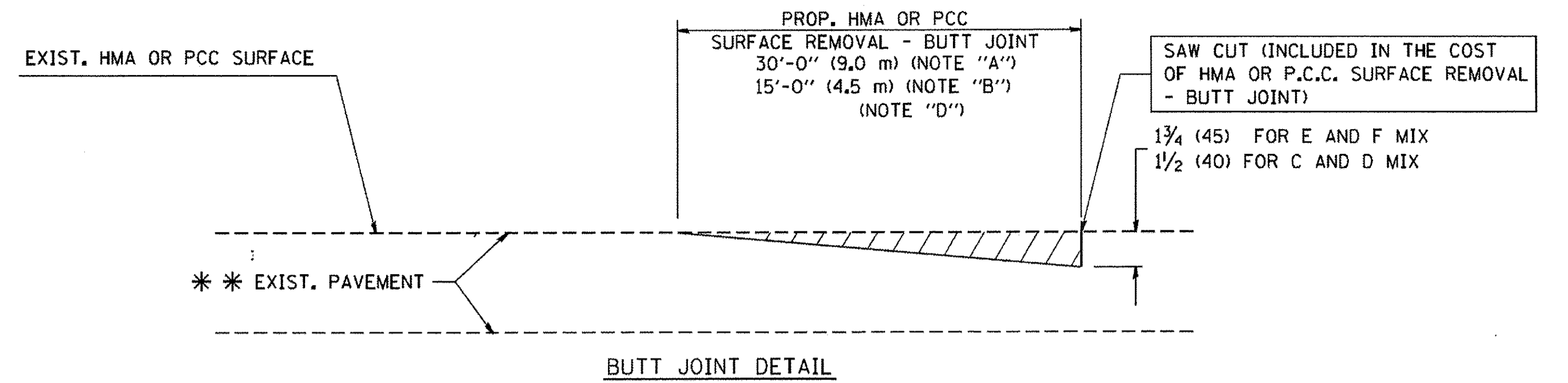


OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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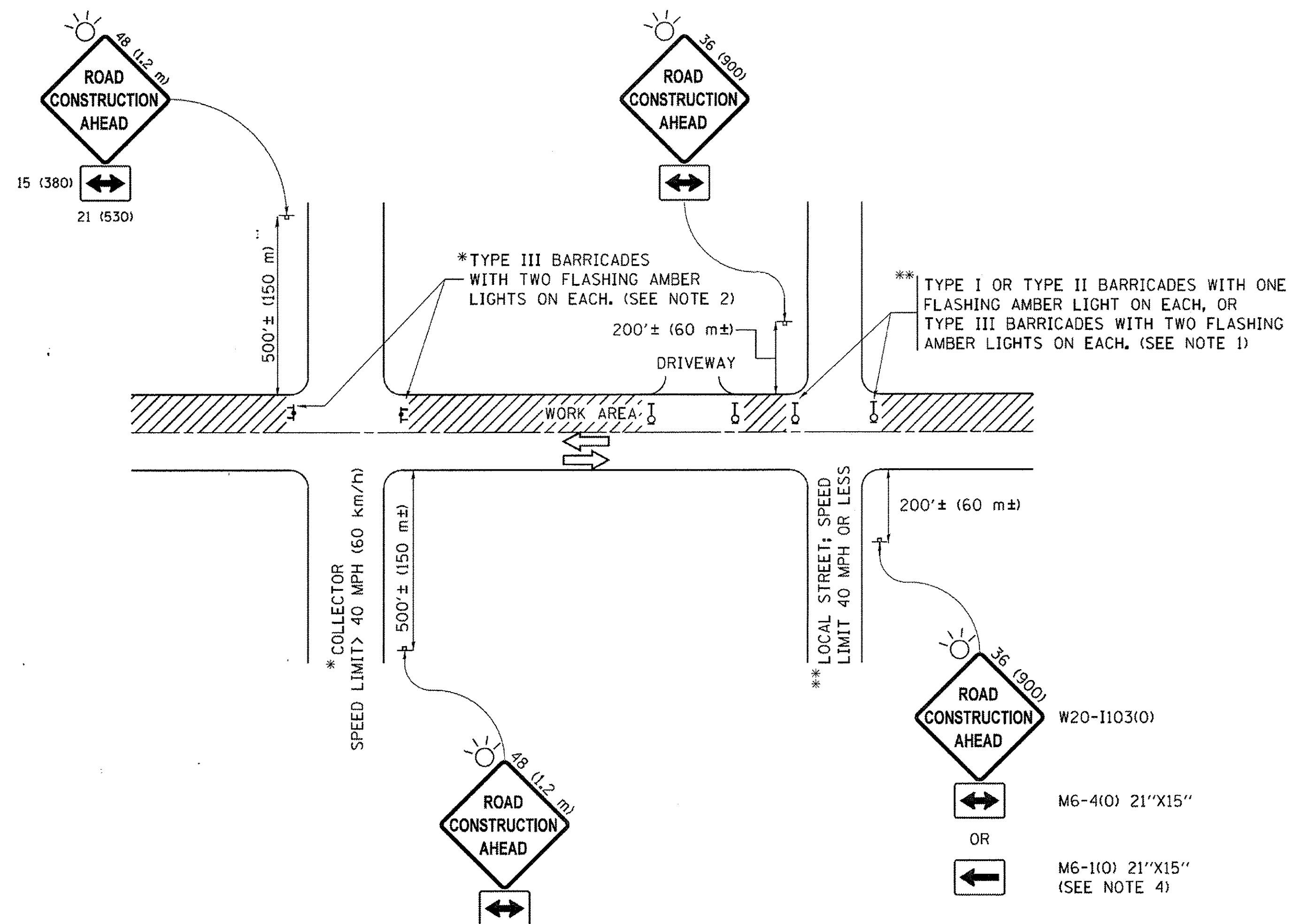
DESIGNED - M. DE YONG
 DRAWN -
 CHECKED -
 DATE - 06-13-90

REVISED - R. SHAH 10-25-94
 REVISED - A. ABBAS 03-21-97
 REVISED - M. GOMEZ 04-06-01
 REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
 HMA TAPER DETAILS**
 SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0298	14-00038-00-RS	WILL	22	16
BD400-05 BD32			CONTRACT NO. 61D72	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

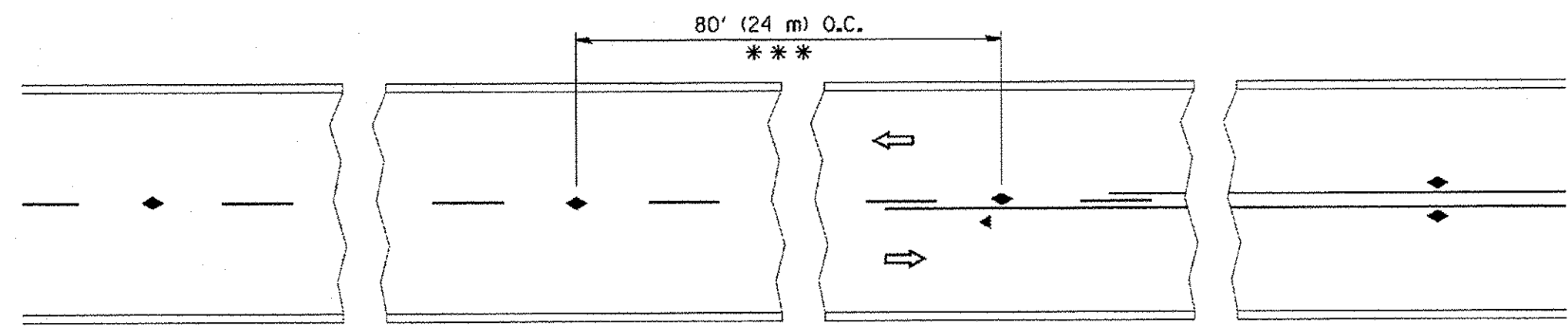
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	PLOT DATE = 9/15/2016	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

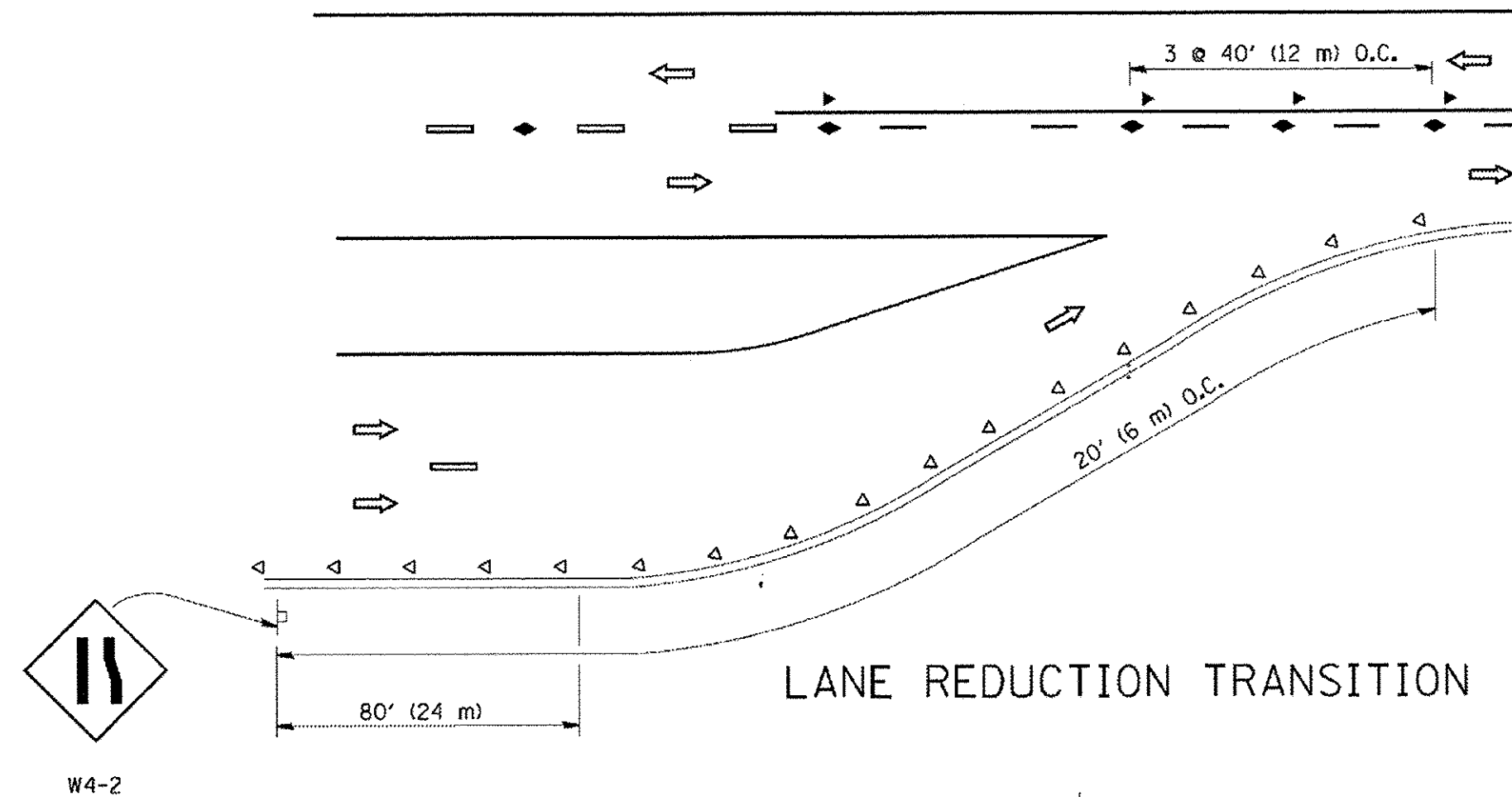
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TC-10			CONTRACT NO. 61D72	
ILLINOIS FED. AID PROJECT				

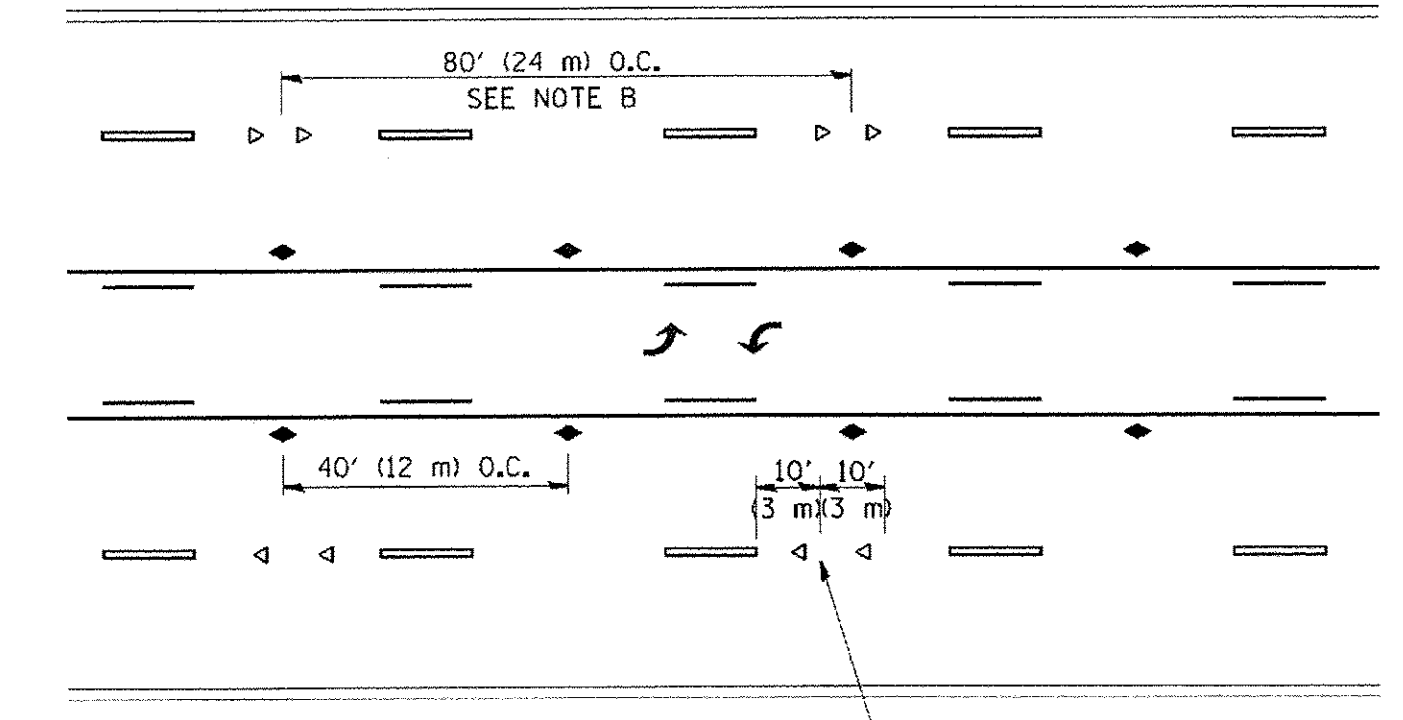


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

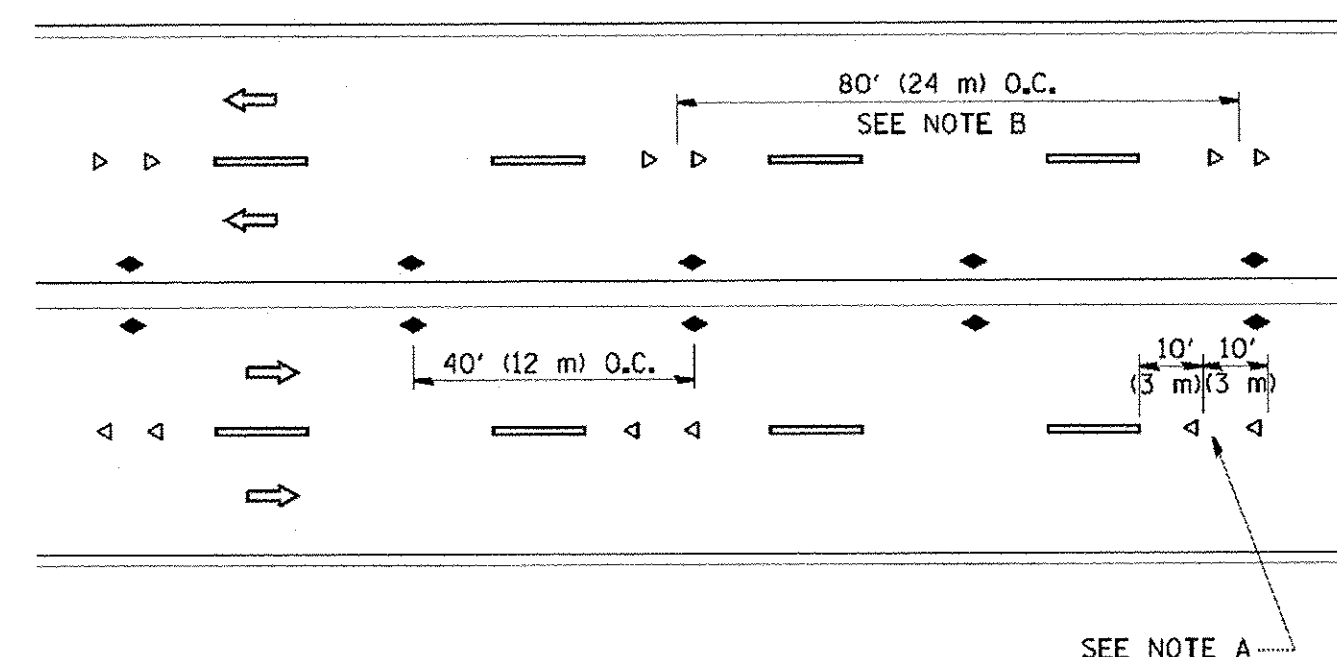
TWO-LANE/TWO-WAY



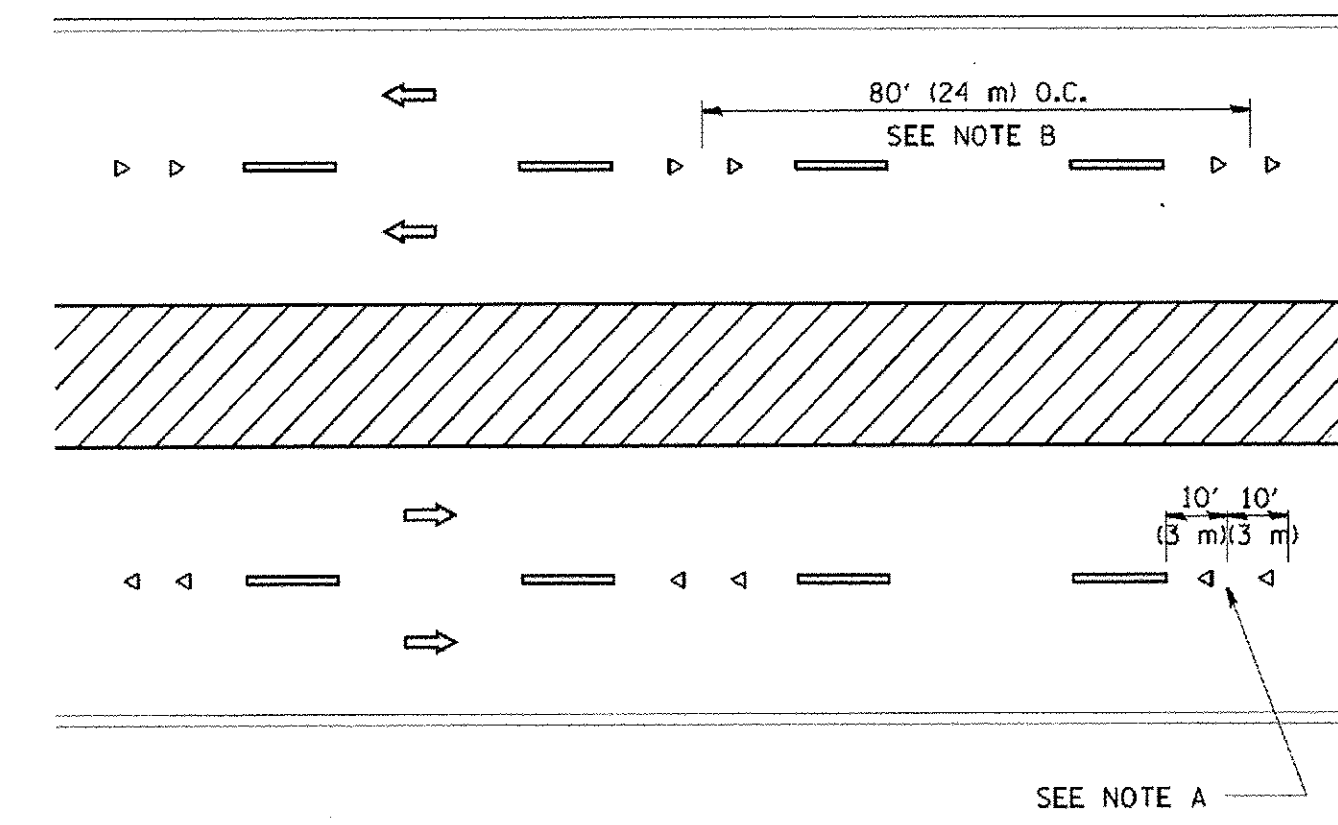
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

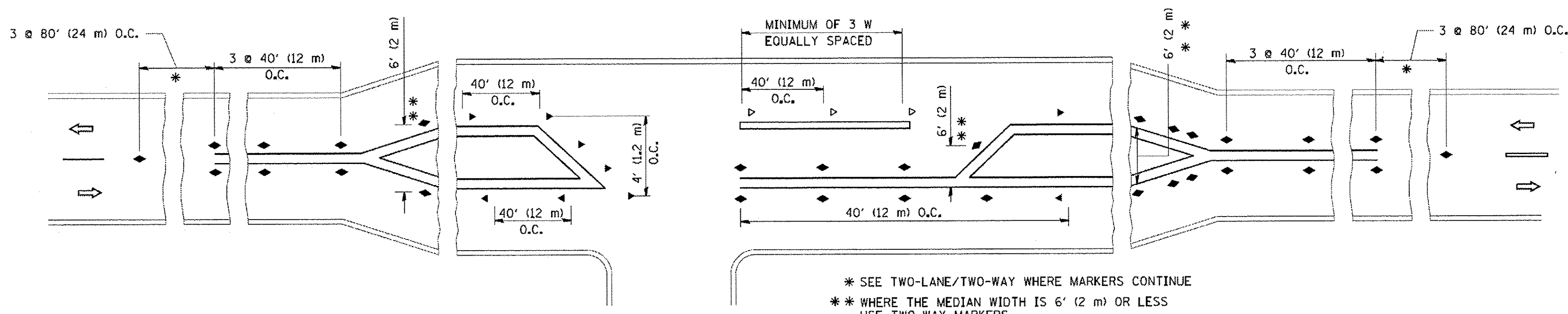
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

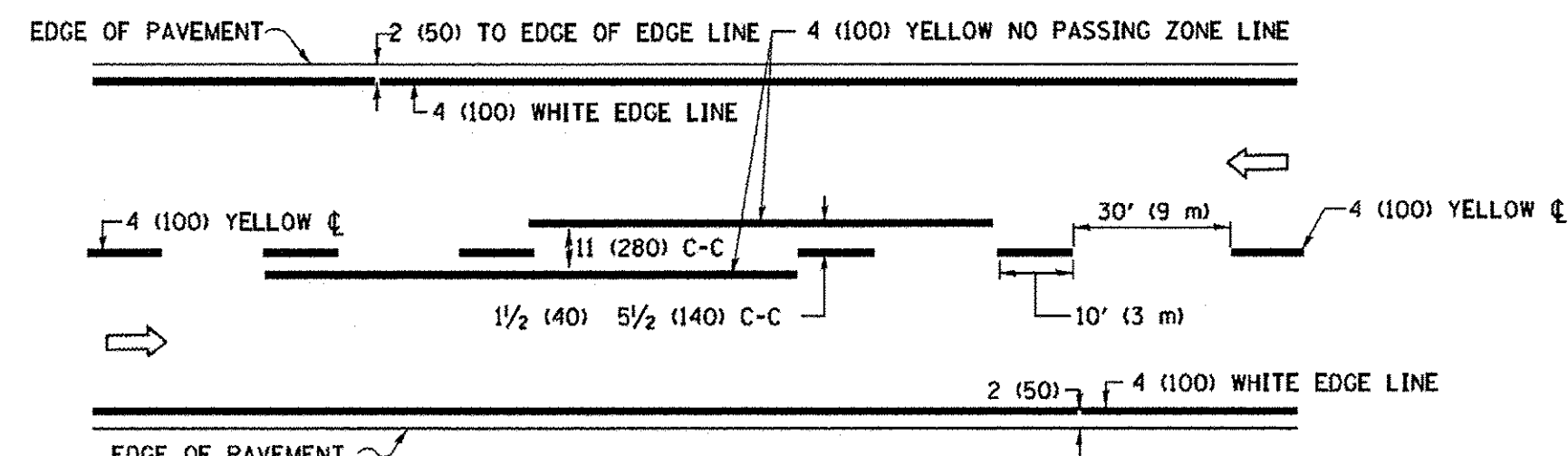


LEFT TURN

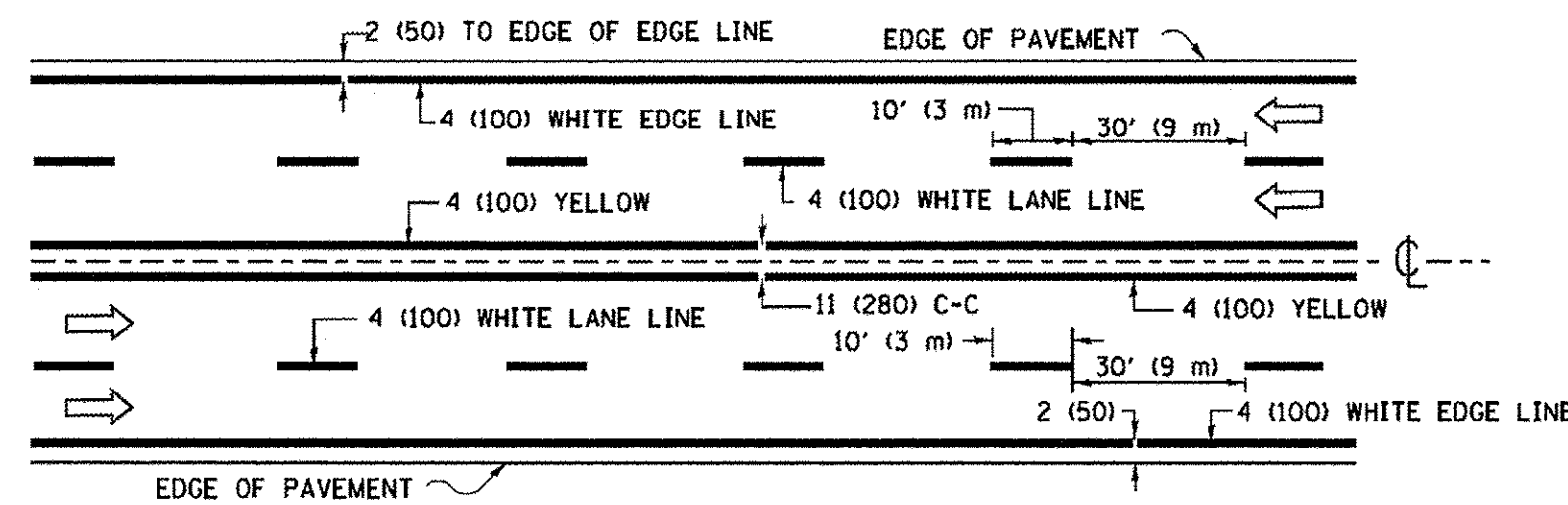
* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

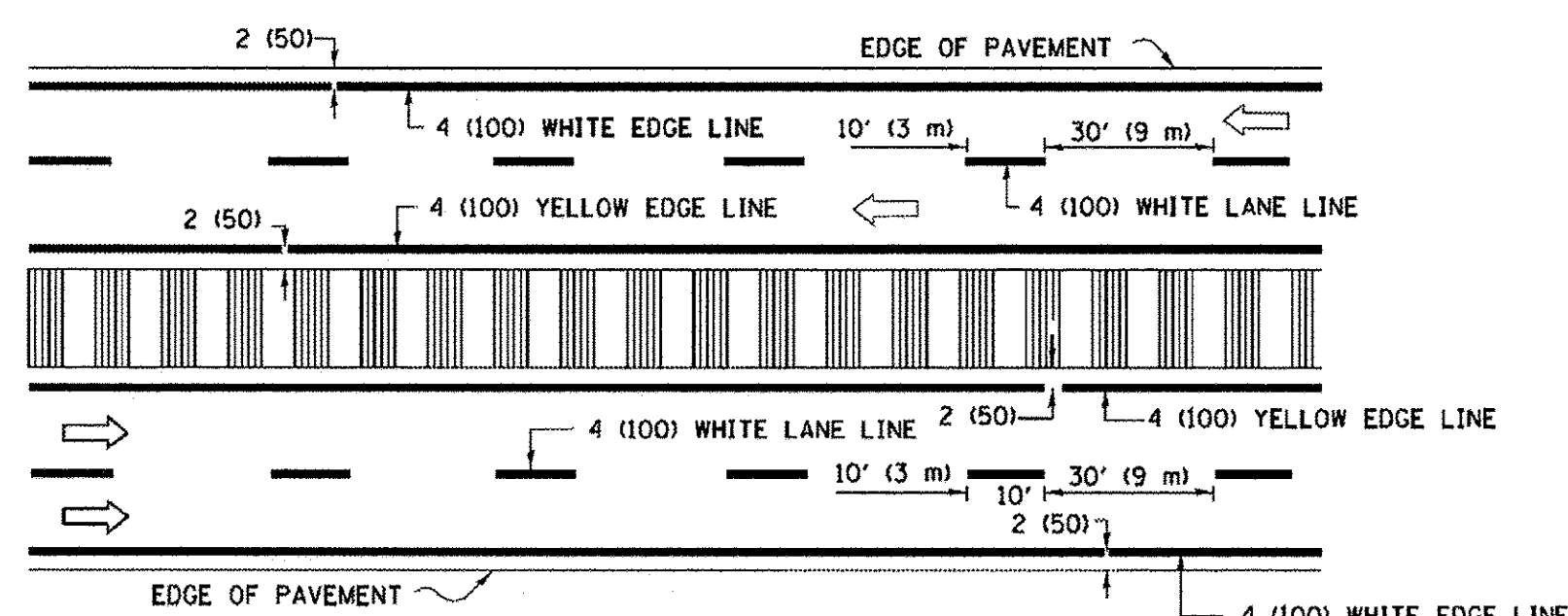
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PLOT SCALE = 50,000 / IN.		DRAWN -	REVISED - T. RAMMACHER 03-12-99		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	TC-11		CONTRACT NO. 61D72		
PLOT DATE = 3/2/2011		CHECKED -	REVISED - T. RAMMACHER 01-06-00				FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT			
		DATE -	REVISED - C. JUCIUS 09-09-09									



2-LANE ROADWAY

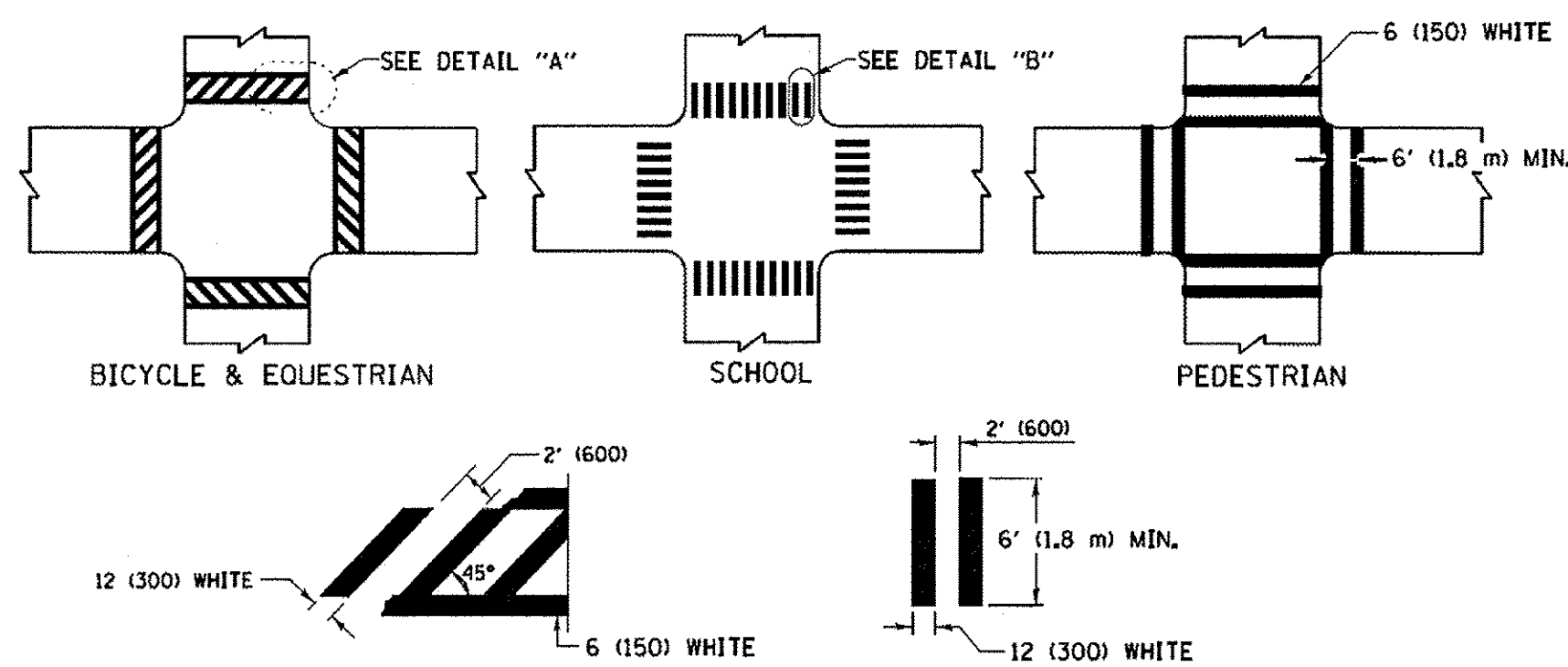


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

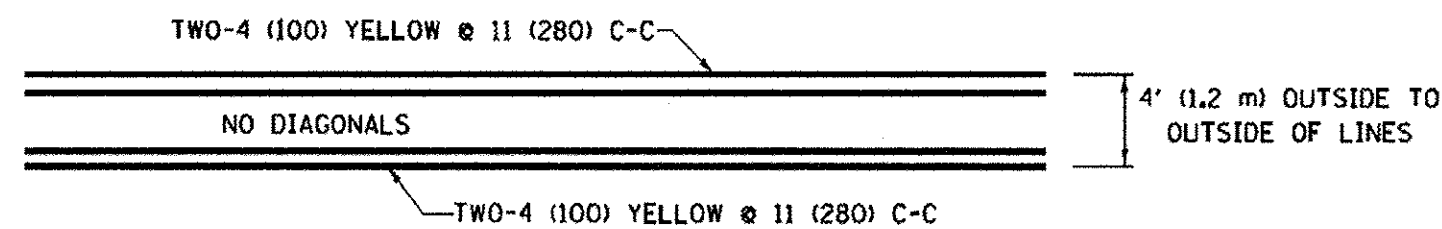


DETAIL "A"

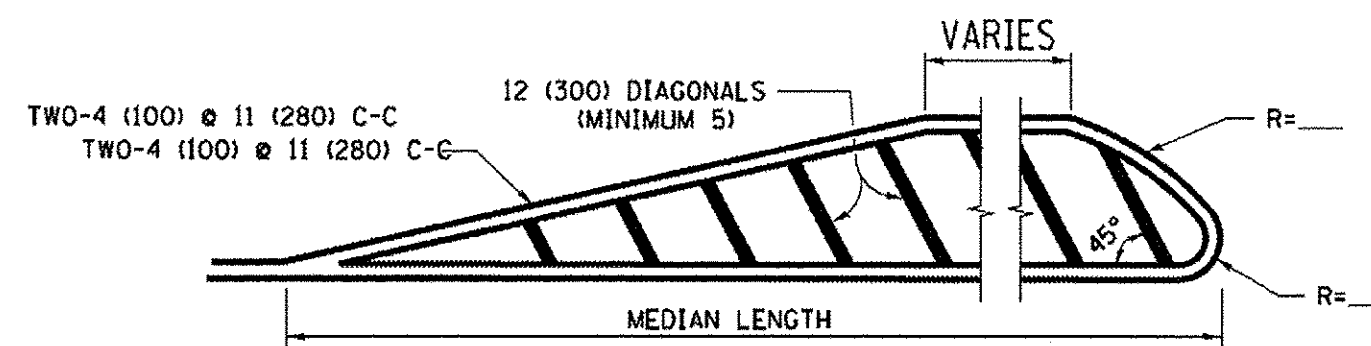
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



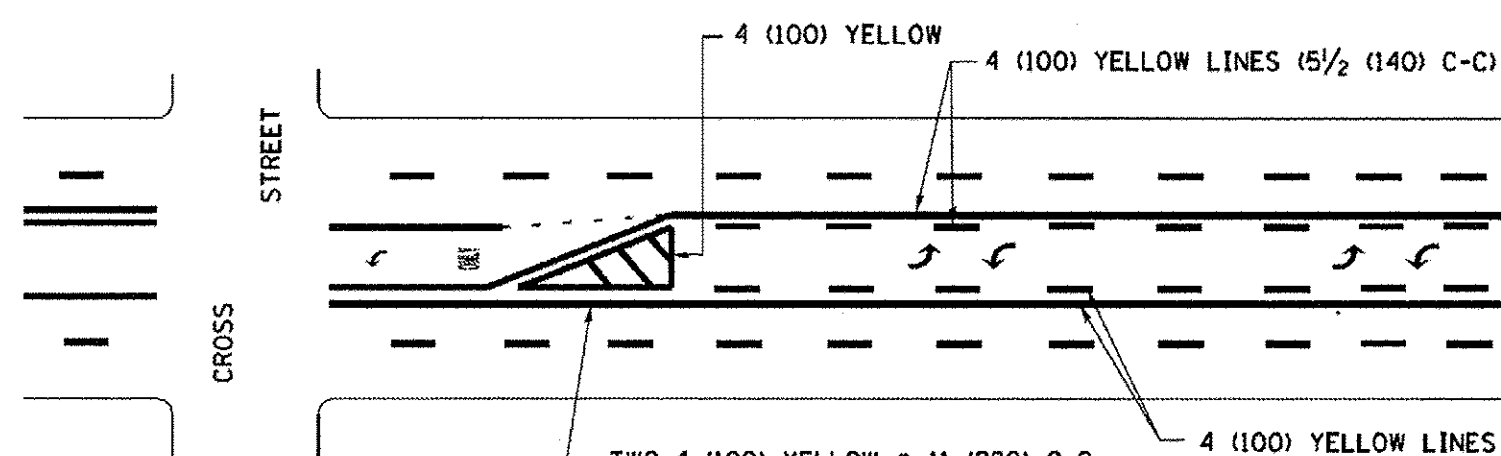
4' (1.2 m) WIDE MEDIANS ONLY



FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

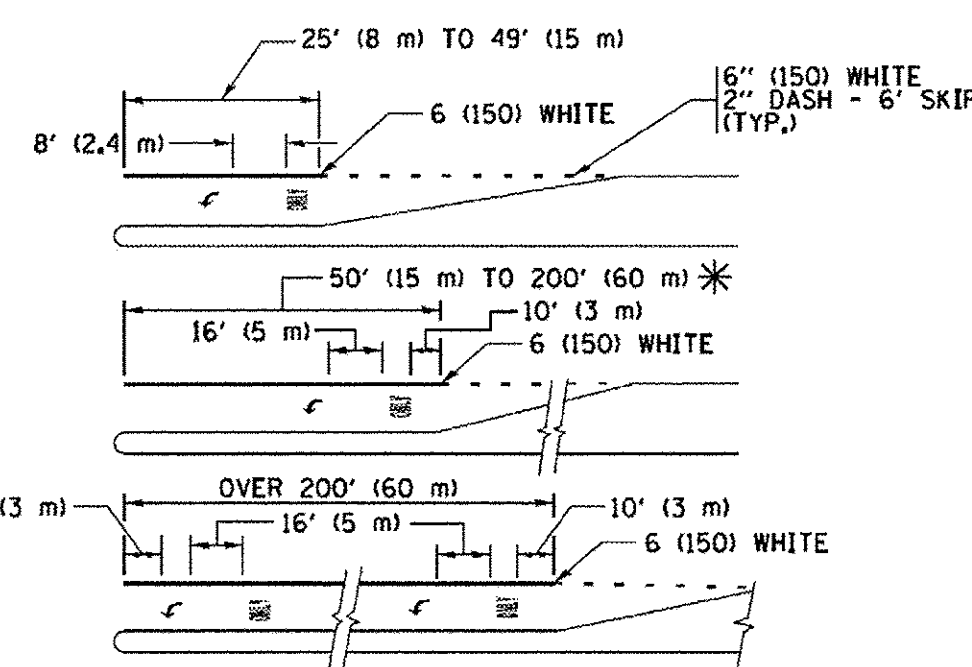
MEDIANS OVER 4' (1.2 m) WIDE



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

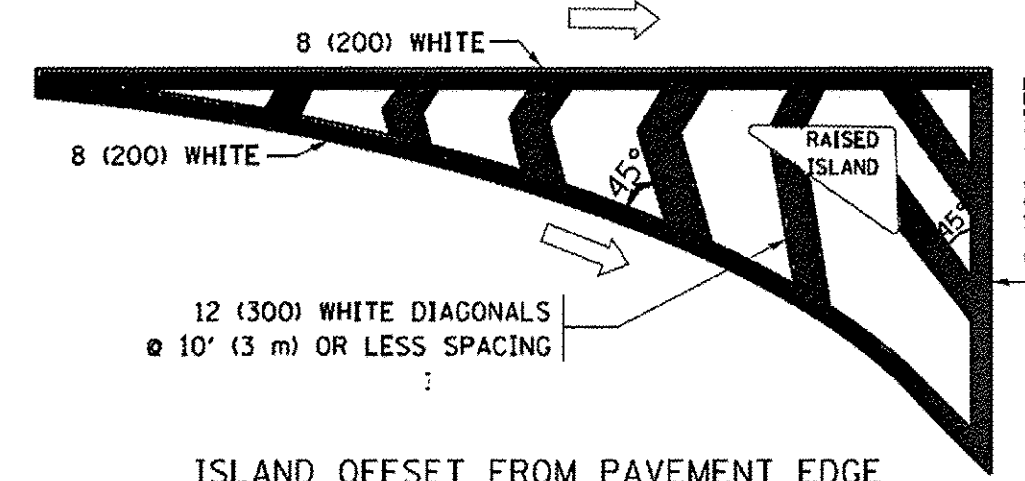


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

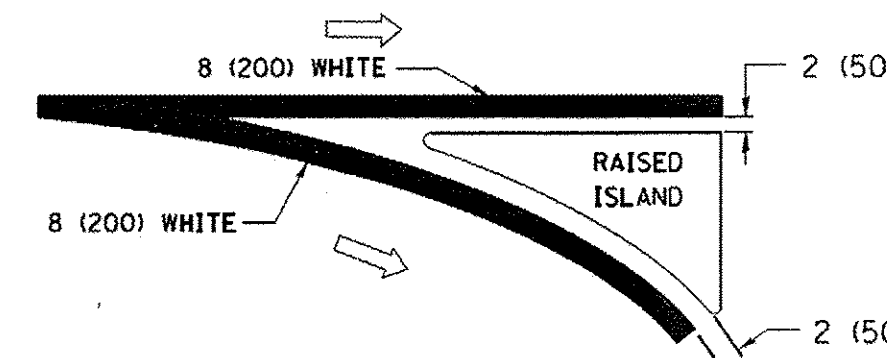
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

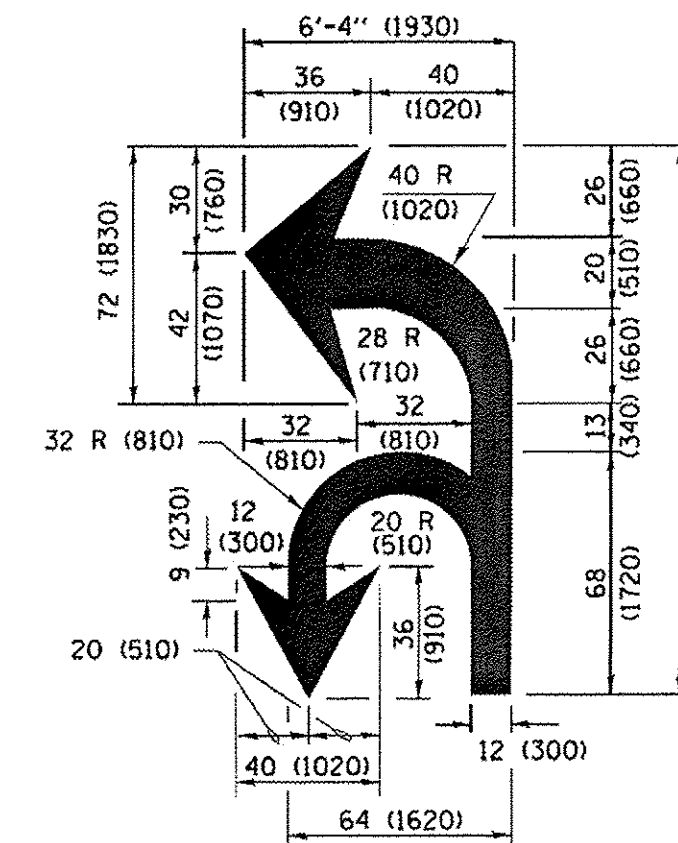


ISLAND OFFSET FROM PAVEMENT EDGE

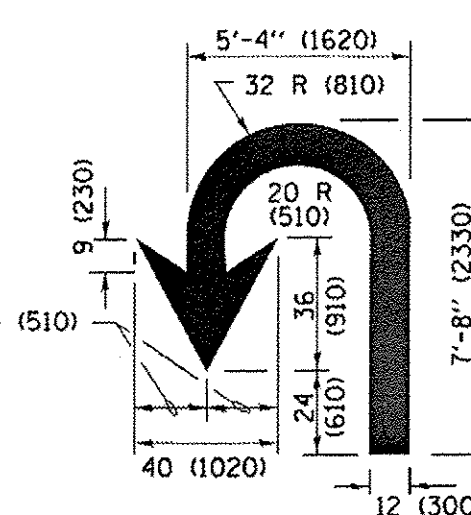


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE. SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15' 6" (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

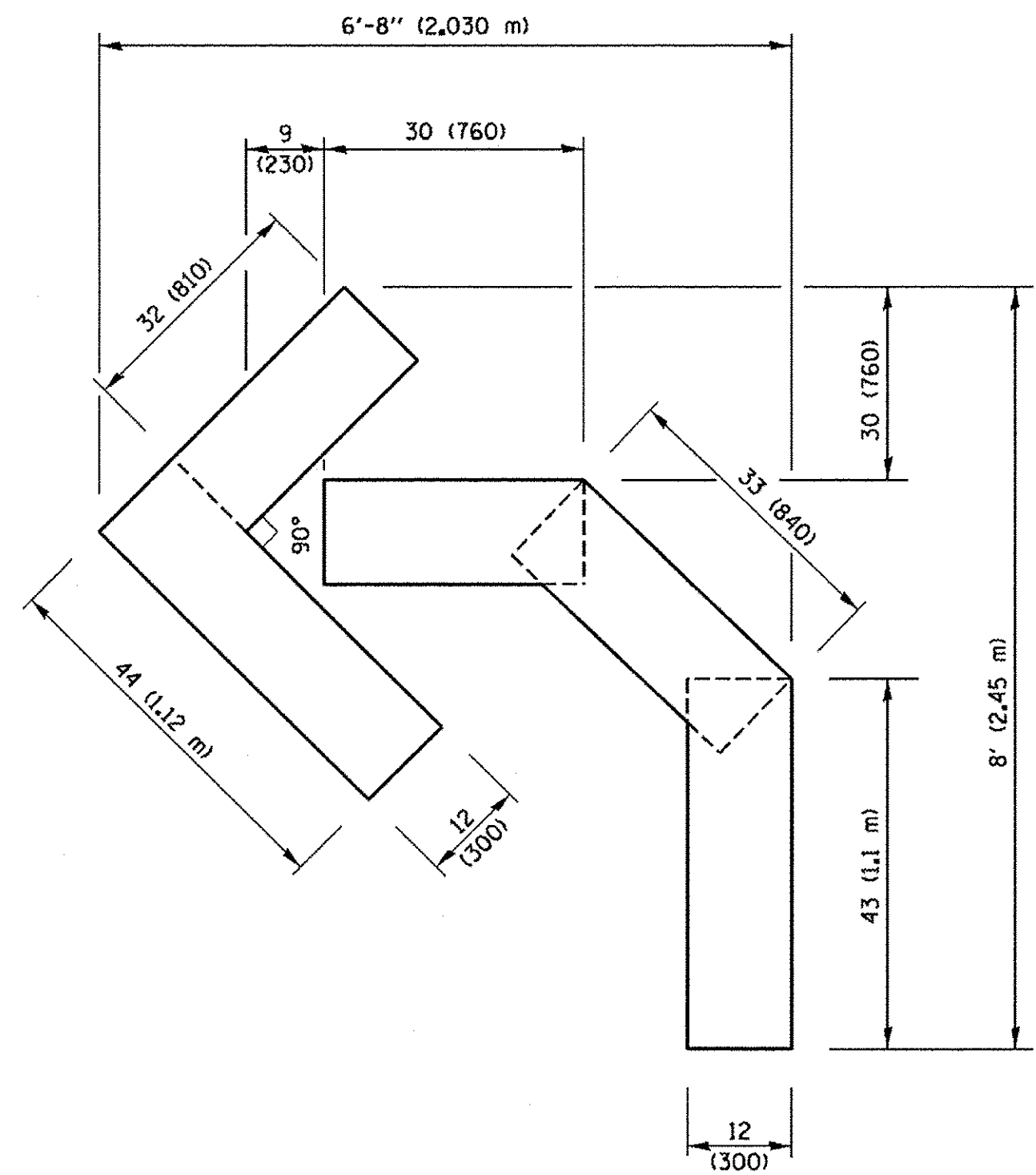
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	PLOT DATE = 4/13/2016		REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
TYPICAL PAVEMENT MARKINGS**

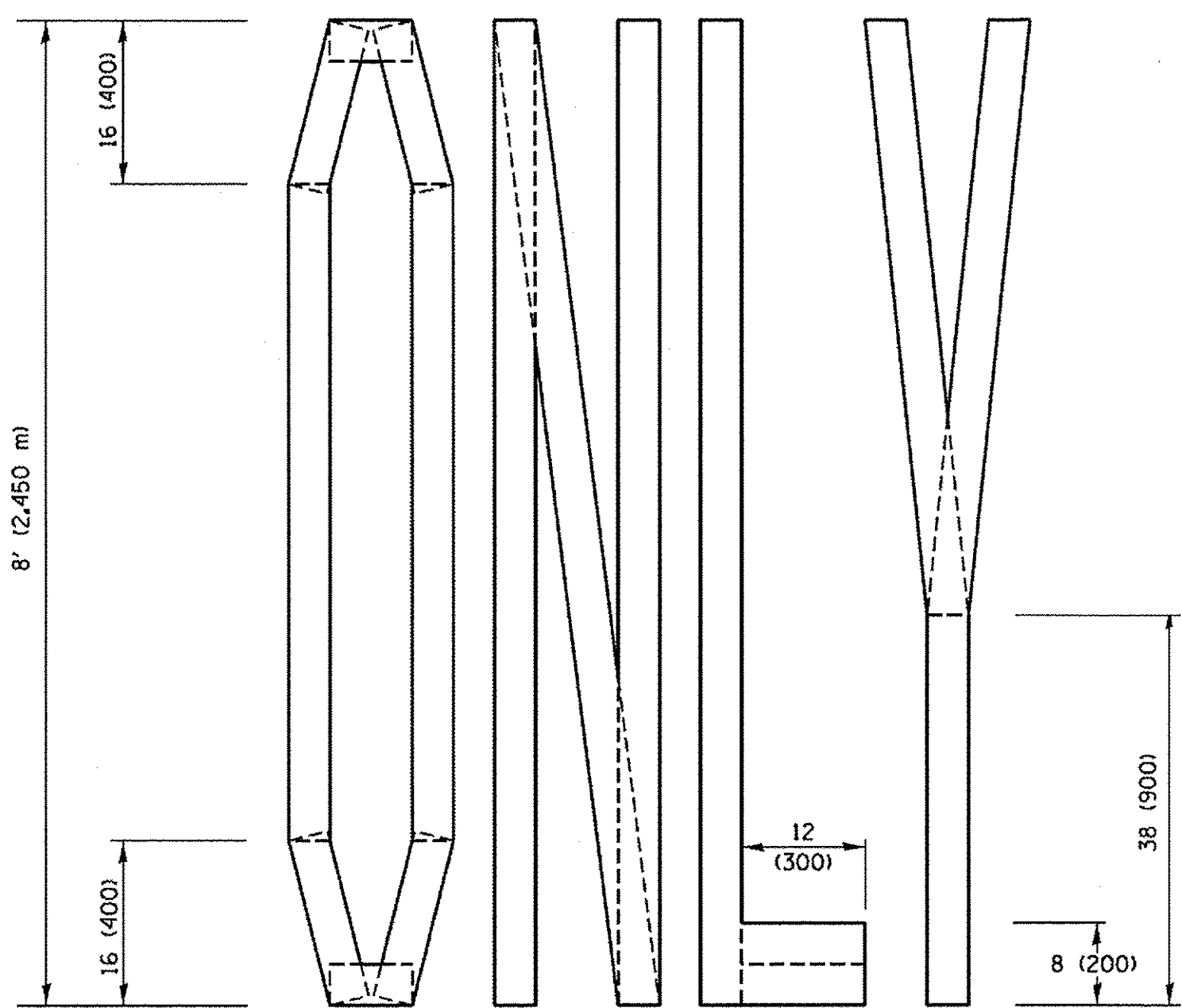
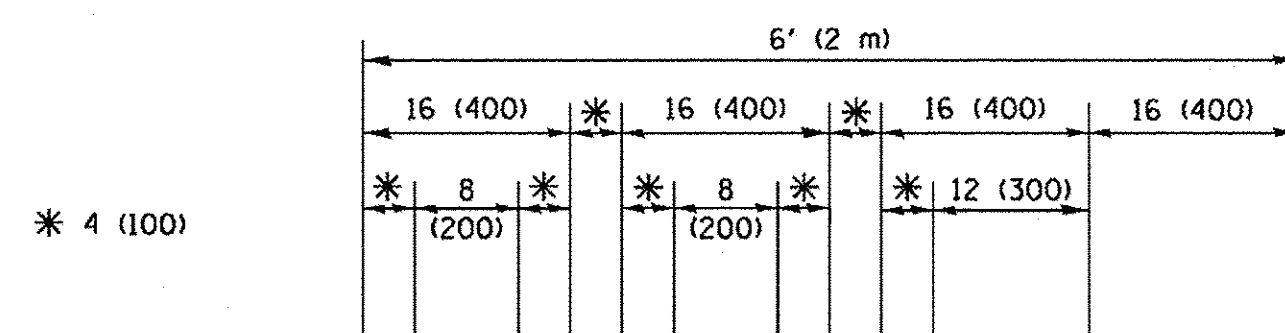
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F.A. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-13		CONTRACT NO.	61D72	
ILLINOIS FED. AID PROJECT				



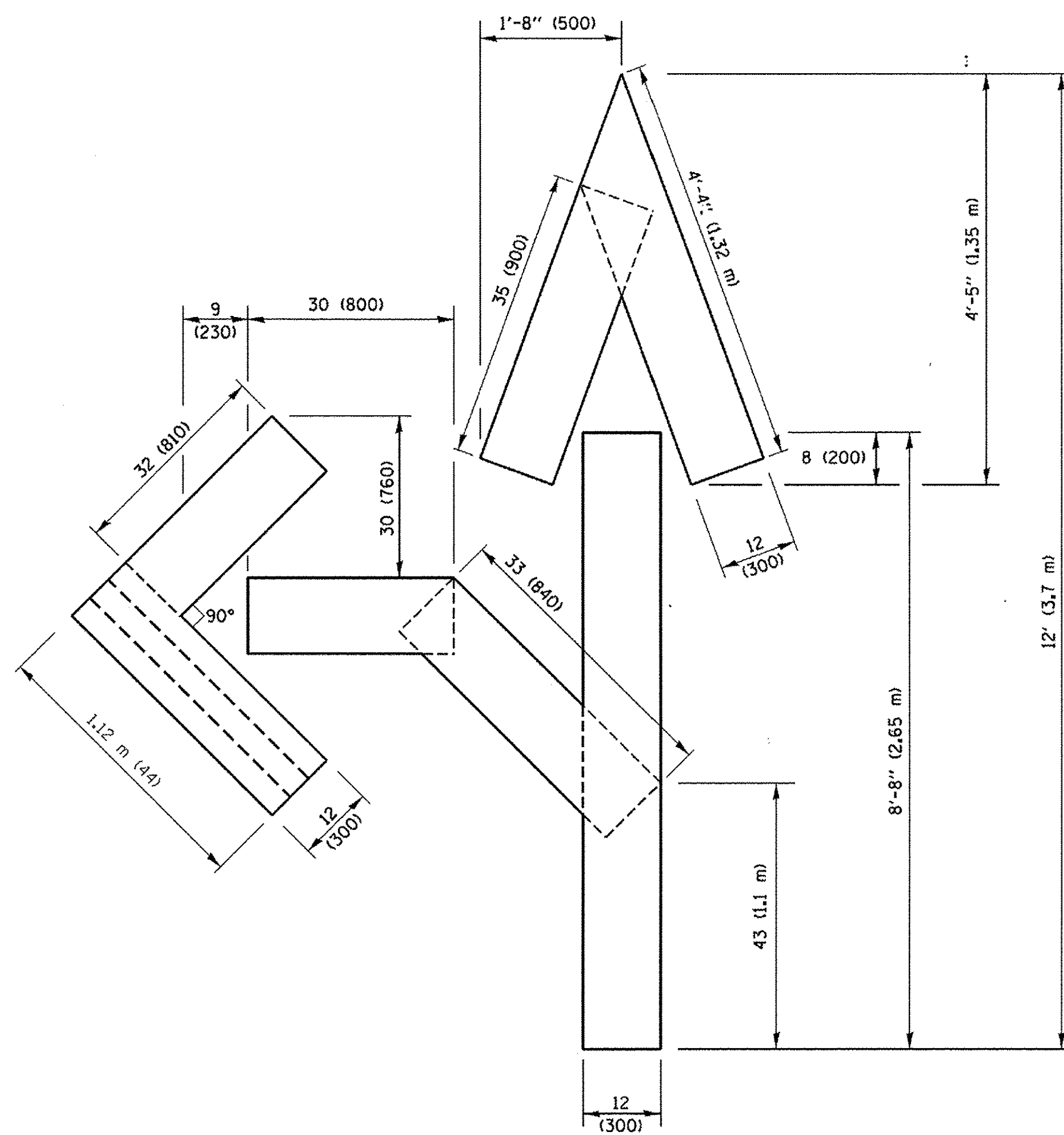
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

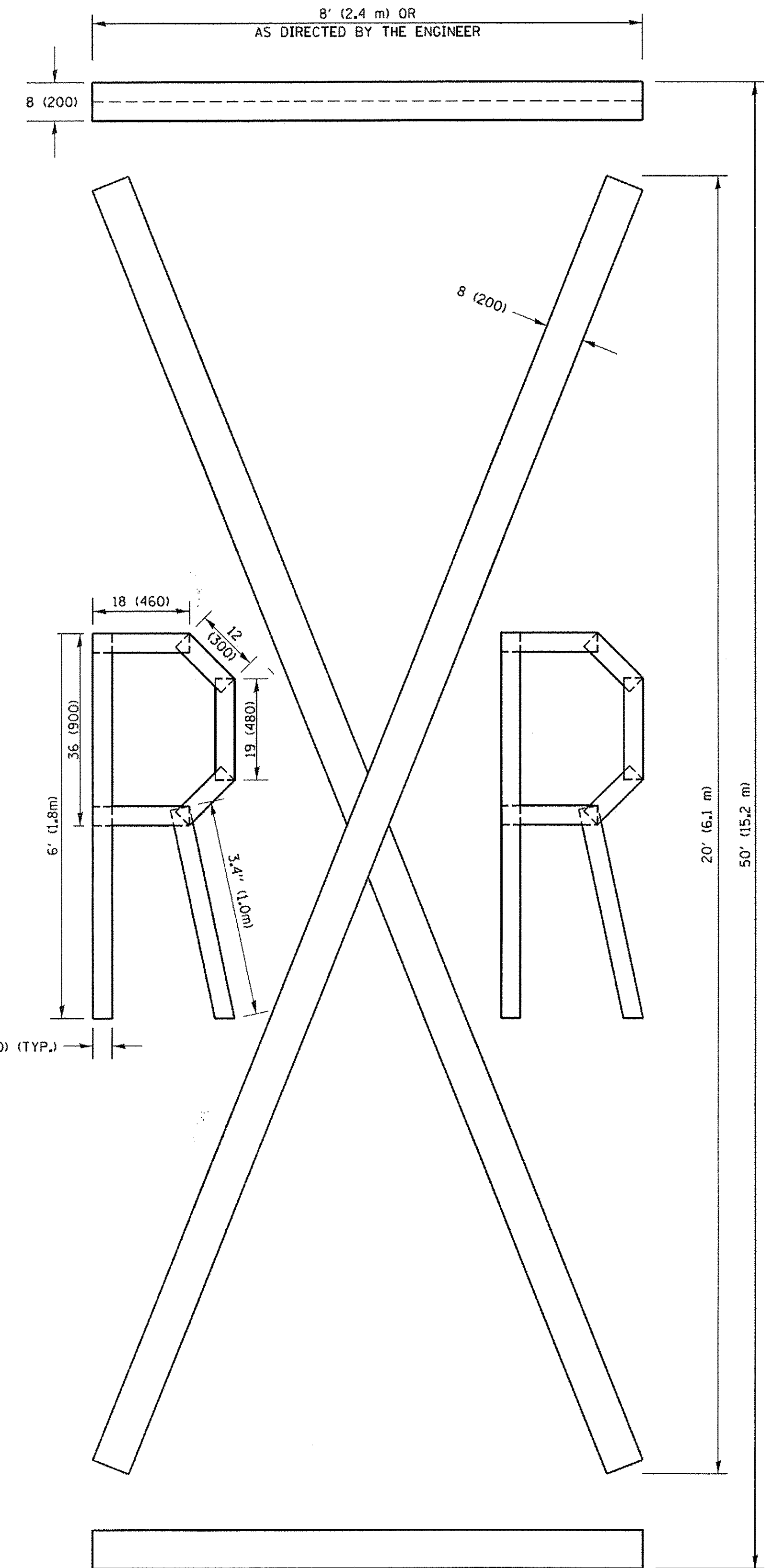


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = foatemj	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
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PLOT DATE = 9/15/2016		DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

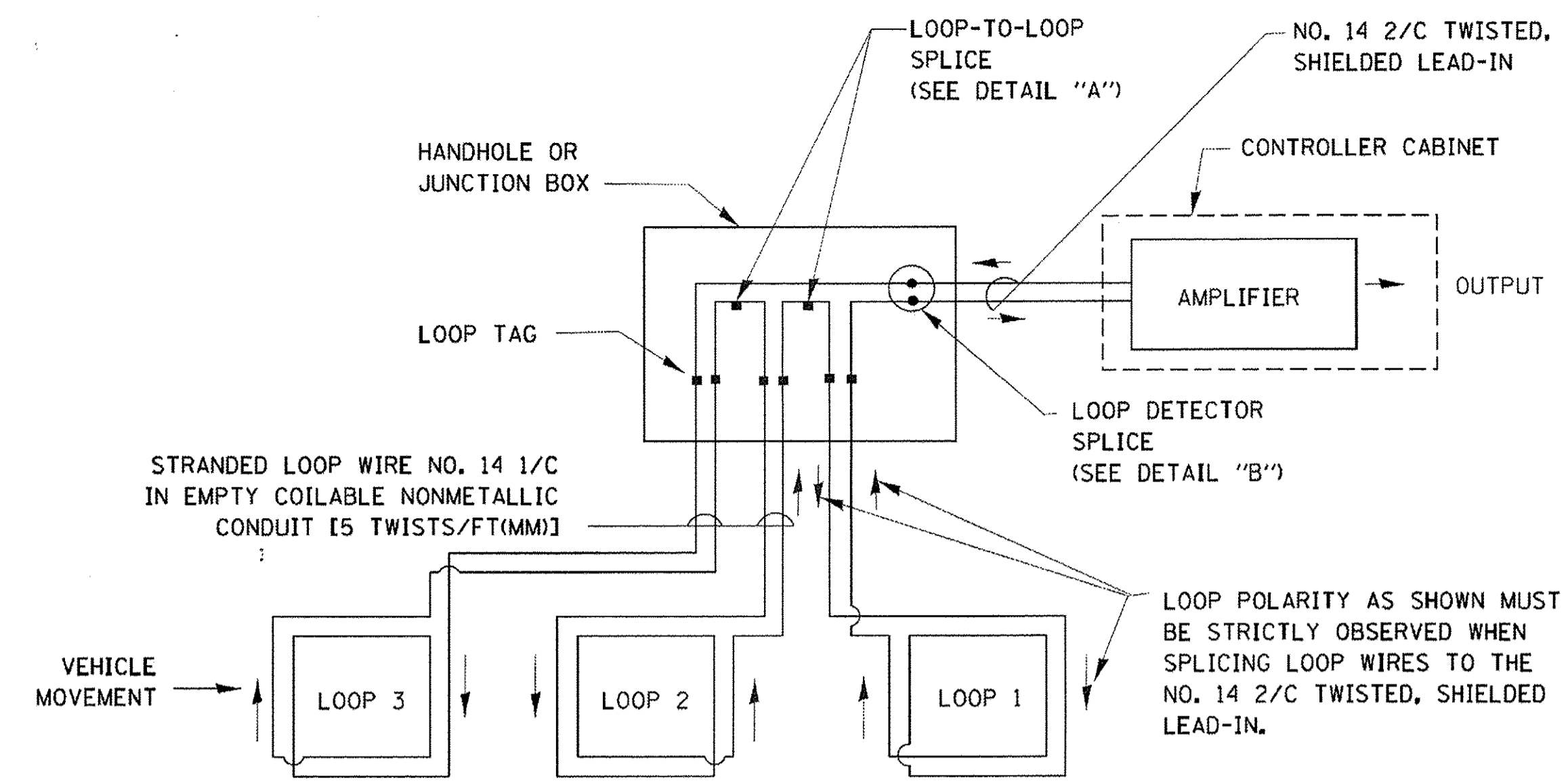
SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE. 0298	SECTION 14-00038-00-RS	COUNTY WILL	TOTAL SHEETS 22	SHEET NO. 20
TC-16		CONTRACT NO. 61D72		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

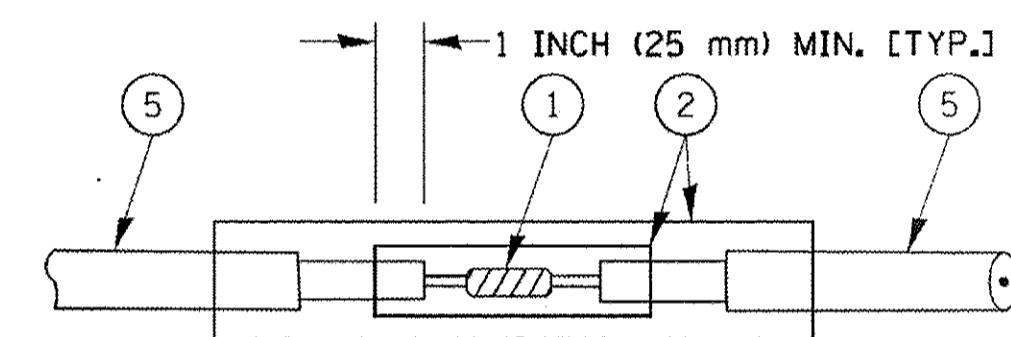
LOOP DETECTOR NOTES

- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

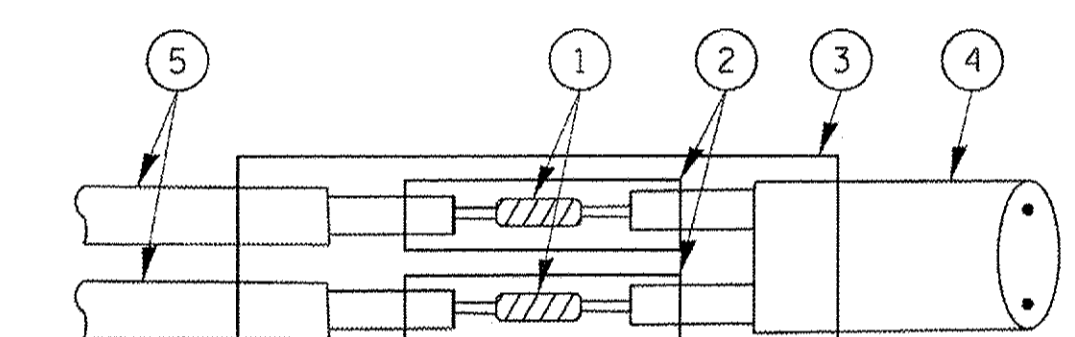


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm), IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.



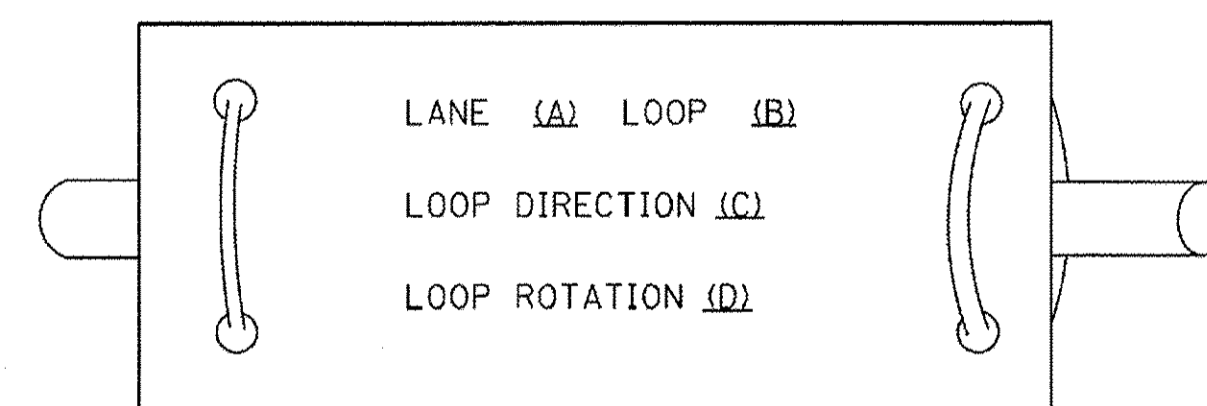
DETAIL "A"
LOOP-TO-LOOP SPLICE



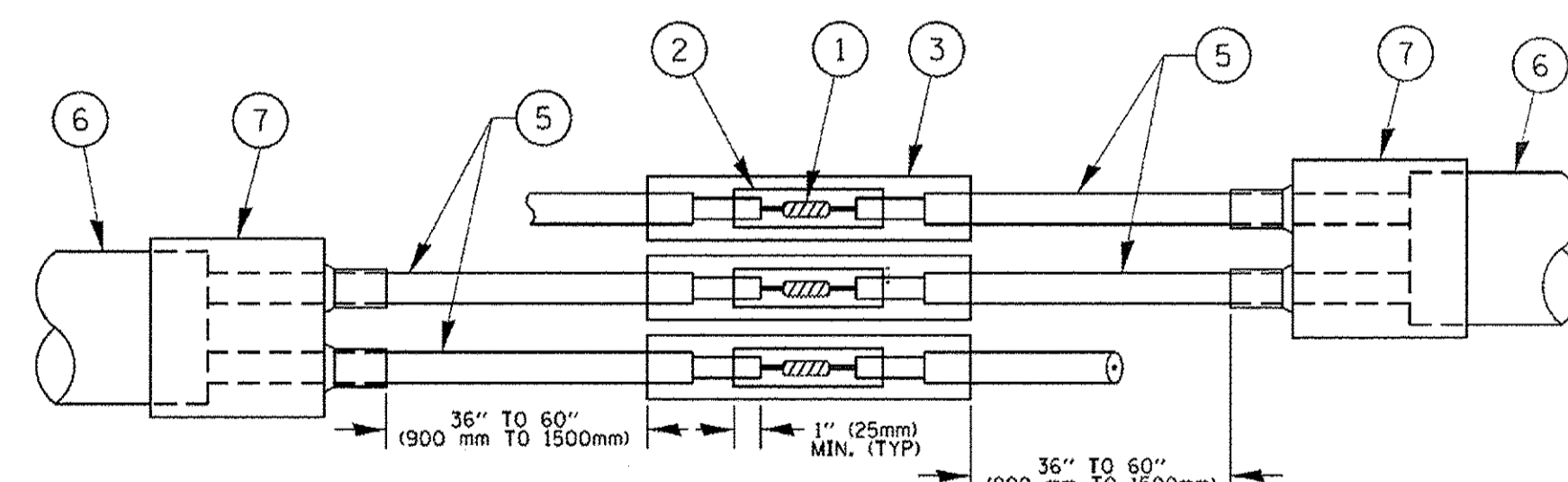
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP

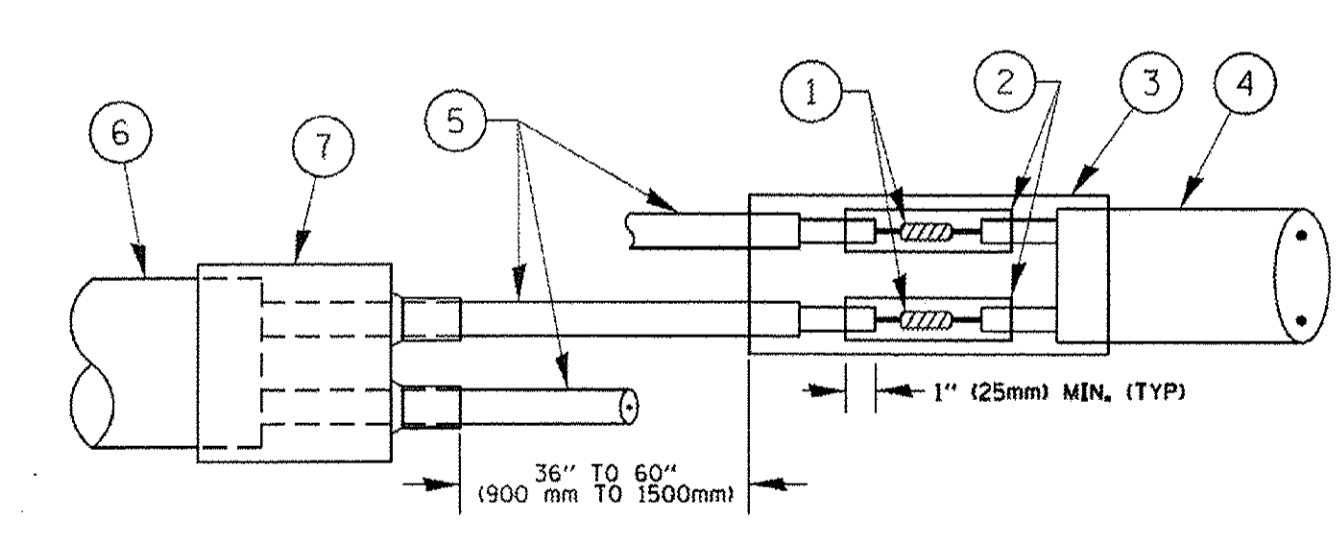
LOOP LEAD-IN CABLE TAG



- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PREFORMED LOOP

LOOP DETECTOR SPLICE

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- PREFORMED LOOP
- XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

