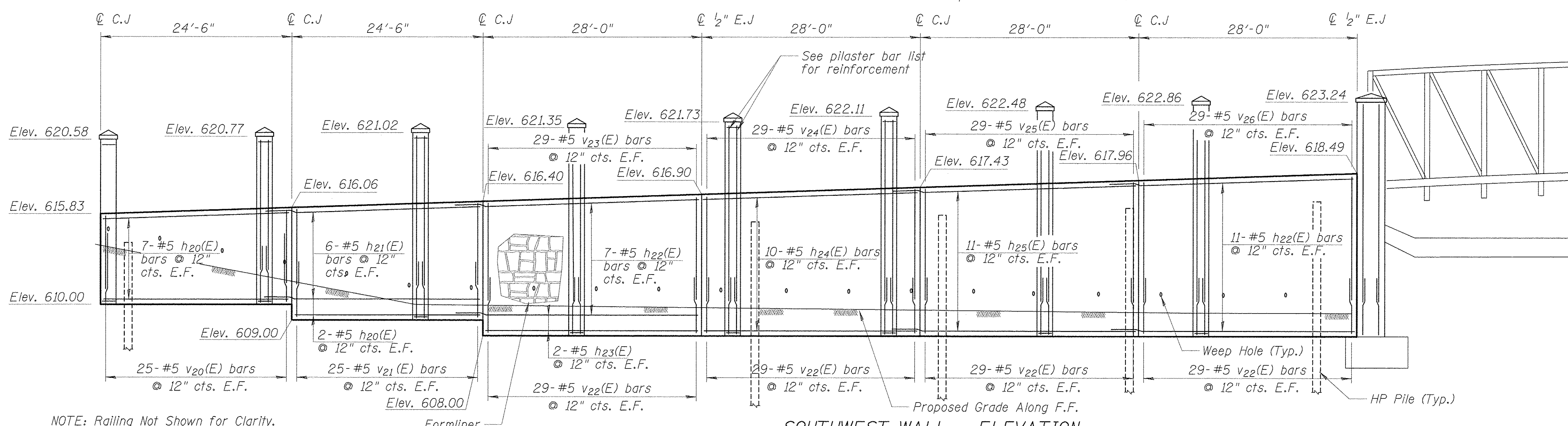
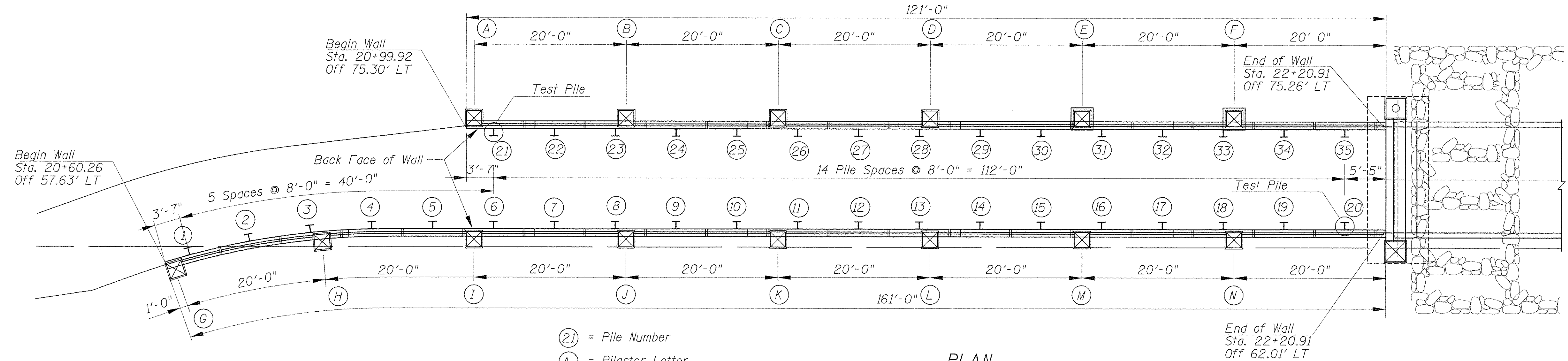
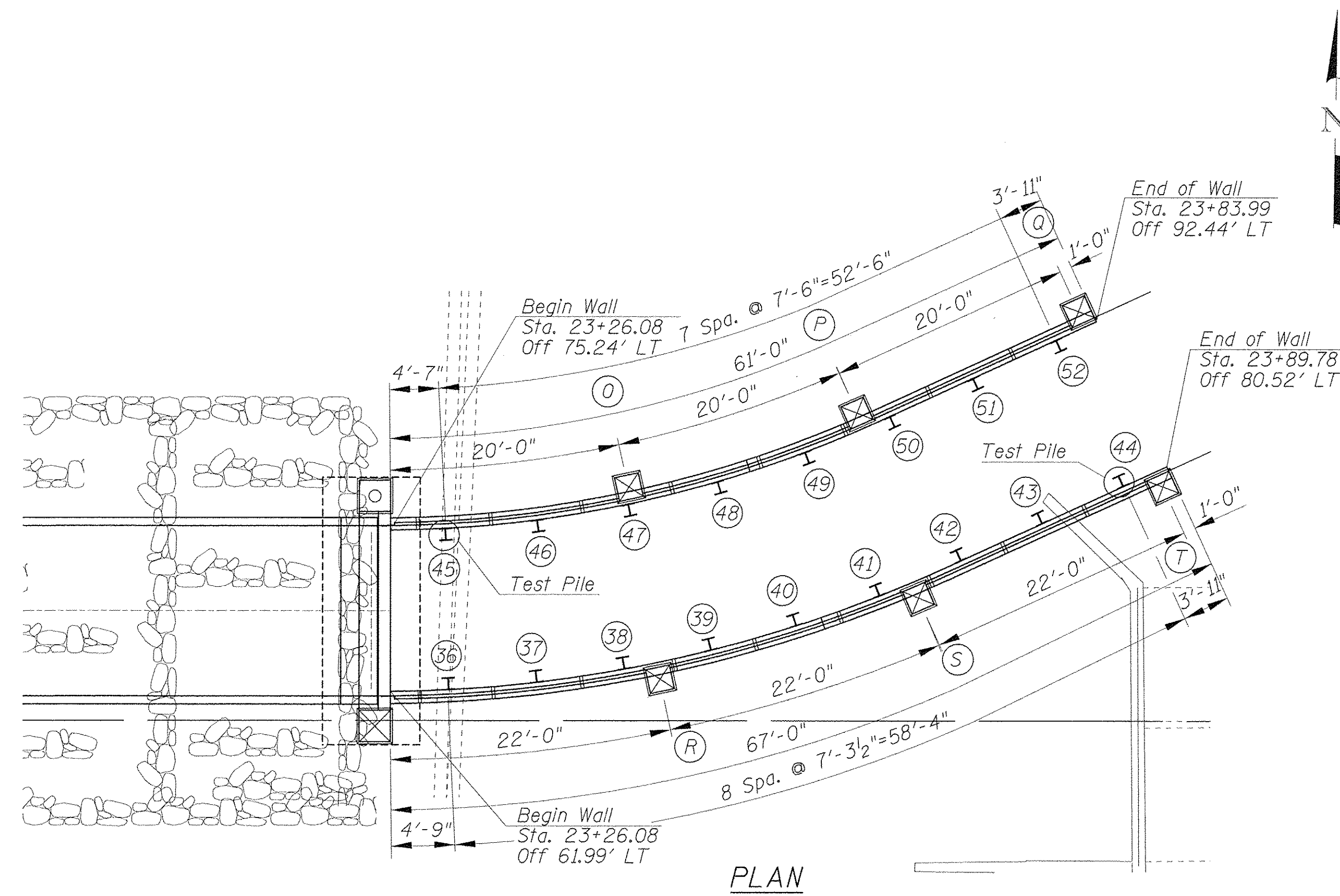


NORTHWEST WALL - ELEVATION



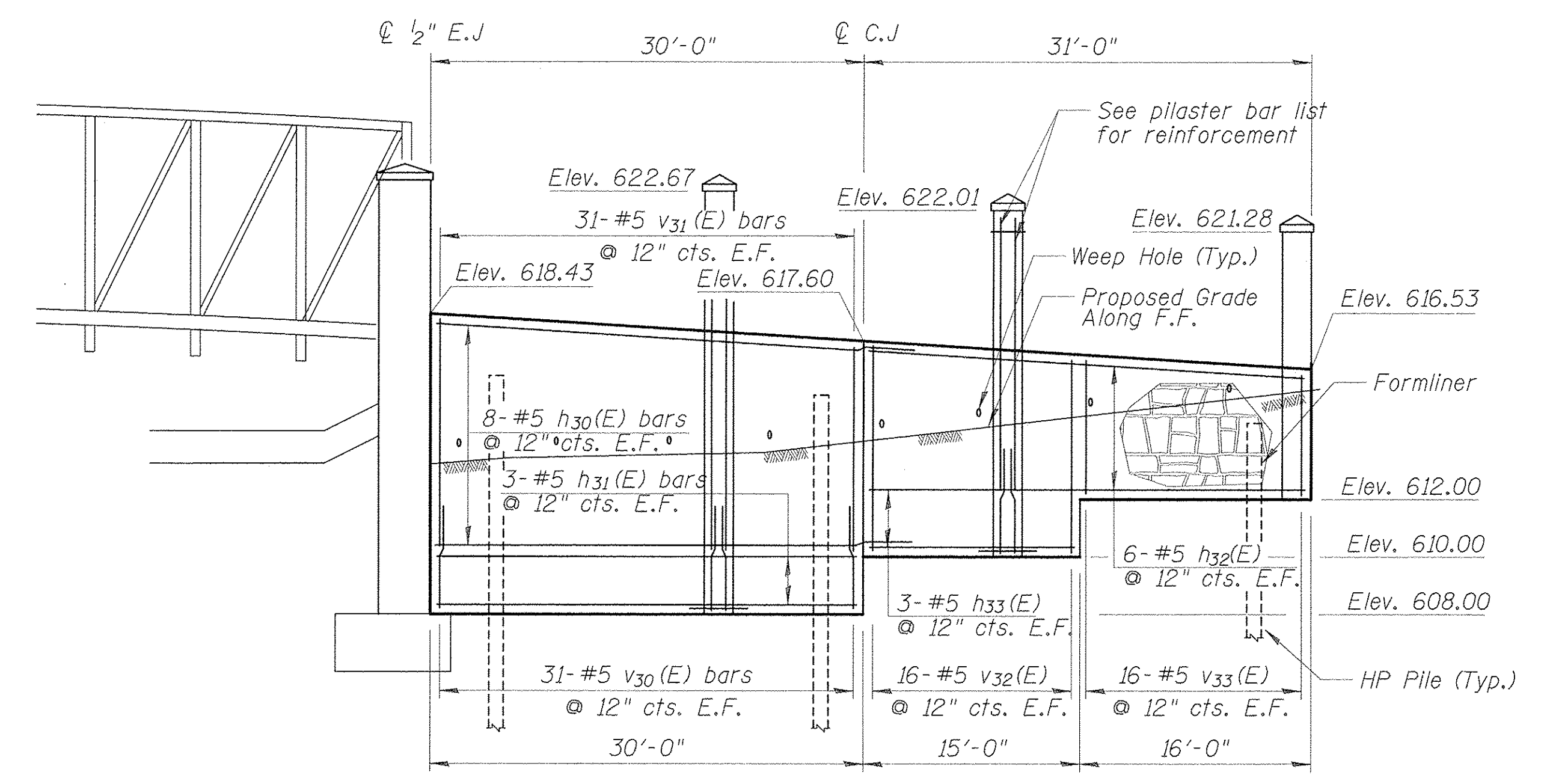
SOUTHWEST WALL - ELEVATION

FILE NAME =	USER NAME = jstsrck	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US ROUTE 52 RIVER ROAD WEST RETAINING WALLS			F.A.P. RTE. 607	SECTION 15-00040-00-CH	COUNTY WILL	TOTAL SHEETS 138	SHEET NO. 102		
Default	PLOT SCALE = 8'	CHECKED -	REVISED -					CONTRACT NO. 61D82			ILLINOIS FED. AID PROJECT			
	PLOT DATE = 2/23/2017	DATE -	REVISED -					SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
N:\Shorewood\070001\070001C\2016\C16-02\Struct\WEST-WALL.C1602.sht														



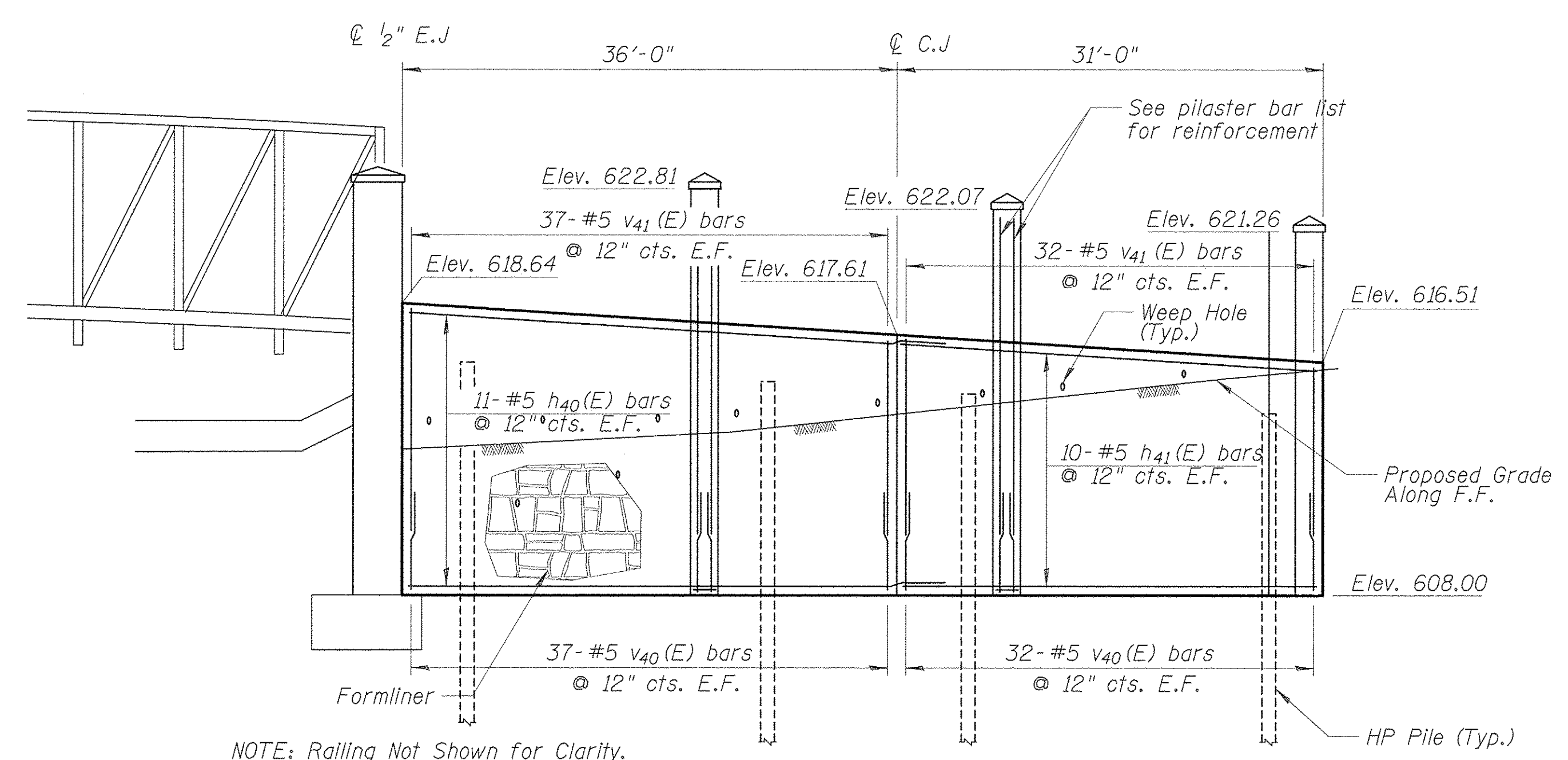
④1 = Pile Number

PLAN



NOTE: Railing Not Shown for Clarity.

NORTHEAST WALL ELEVATION



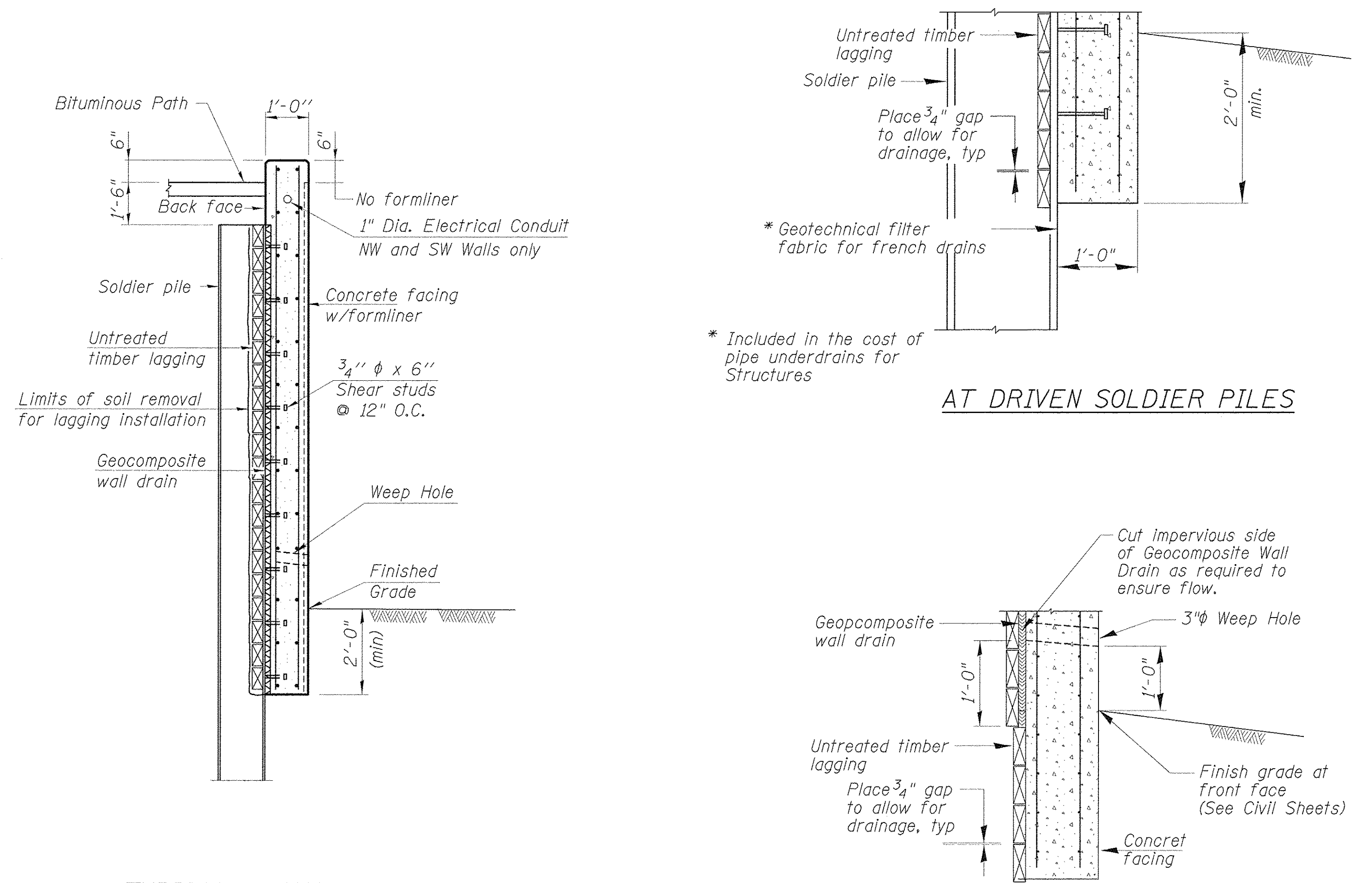
NOTE: Railing Not Shown for Clarity.

SOUTHEAST WALL ELEVATION

FILE NAME =	USER NAME = jstruck	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US ROUTE 52 RIVER ROAD EAST RETAINING WALLS				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	PLDT SCALE = 8'	DRAWN -	REVISED -						607	15-00040-00-CH	WILL	138	103
	PLDT DATE = 2/23/2017	CHECKED -	REVISED -						CONTRACT NO. 61D82				
		DATE	REVISED -						SCALE:	SHEET	OF	SHEETS	STA.

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	Pile Number	Type	Station	Offset (LT)	T/Pile Elev	Pile Tip Elev	Length	B/Wall	Shear Stu Quantity
SOUTHWEST WALL	1	HP14x73	20+63.54	58.59	613.85	589.00	24.9	610.00	4
	2	HP14x73	20+71.41	60.39	613.92	589.00	24.9	610.00	4
	3	HP14x73	20+79.41	61.55	614.00	589.00	25.0	610.00	4
	4	HP14x73	20+87.48	62.04	614.09	589.00	25.1	609.00	5
	5	HP14x73	20+95.50	62.06	614.20	589.00	25.2	609.00	5
	6	HP14x73	21+03.50	62.05	614.31	589.00	25.3	609.00	5
	7	HP14x73	21+11.50	62.05	614.44	589.00	25.4	608.00	6
	8	HP14x73	21+19.50	62.05	614.58	589.00	25.6	608.00	7
	9	HP14x73	21+27.50	62.05	614.73	589.00	25.7	608.00	7
	10	HP14x73	21+35.50	62.04	614.88	589.00	25.9	608.00	7
	11	HP14x73	21+43.50	62.04	615.03	589.00	26.0	608.00	7
	12	HP14x73	21+51.50	62.04	615.18	589.00	26.2	608.00	7
	13	HP14x73	21+59.50	62.03	615.33	589.00	26.3	608.00	7
	14	HP14x73	21+67.50	62.03	615.48	589.00	26.5	608.00	7
	15	HP14x73	21+75.50	62.03	615.63	589.00	26.6	608.00	8
	16	HP14x73	21+83.50	62.03	615.78	589.00	26.8	608.00	8
	17	HP14x73	21+91.50	62.02	615.93	589.00	26.9	608.00	8
	18	HP14x73	21+99.50	62.02	616.09	589.00	27.1	608.00	8
	19	HP14x73	22+07.50	62.02	616.24	589.00	27.2	608.00	8
	20	HP14x73	22+15.50	62.02	616.39	589.00	27.4	608.00	8
NORTHWEST WALL	21	HP14x73	21+03.50	75.30	614.10	600.00	14.1	613.00	1
	22	HP14x73	21+11.50	75.30	614.23	600.00	14.2	613.00	1
	23	HP14x73	21+19.50	75.30	614.37	600.00	14.4	613.00	1
	24	HP14x73	21+27.50	75.30	614.52	600.00	14.5	613.00	2
	25	HP14x73	21+35.50	75.29	614.67	600.00	14.7	613.00	2
	26	HP14x73	21+43.50	75.29	614.82	600.00	14.8	613.00	2
	27	HP14x73	21+51.50	75.29	614.97	600.00	15.0	613.00	2
	28	HP14x73	21+59.50	75.28	615.12	600.00	15.1	613.00	2
	29	HP14x73	21+67.50	75.28	615.27	600.00	15.3	613.00	2
	30	HP14x73	21+75.50	75.28	615.42	600.00	15.4	613.00	2
	31	HP14x73	21+83.50	75.28	615.57	600.00	15.6	613.00	3
	32	HP14x73	21+91.50	75.27	615.72	589.00	26.7	613.00	3
	33	HP14x73	21+99.50	75.27	615.88	589.00	26.9	608.00	8
	34	HP14x73	22+07.50	75.27	616.03	589.00	27.0	608.00	8
	35	HP14x73	22+15.50	75.27	616.18	589.00	27.2	608.00	8
SOUTHEAST WALL	36	HP14x73	23+30.86	62.13	616.53	594.00	22.5	608.00	9
	37	HP14x73	23+38.10	62.75	616.35	594.00	22.3	608.00	8
	38	HP14x73	23+45.29	63.83	616.14	594.00	22.1	608.00	8
	39	HP14x73	23+52.39	65.38	615.92	594.00	21.9	608.00	8
	40	HP14x73	23+59.37	67.39	615.68	594.00	21.7	608.00	8
	41	HP14x73	23+66.21	69.85	615.42	594.00	21.4	608.00	7
	42	HP14x73	23+72.89	72.75	615.17	594.00	21.2	608.00	7
	43	HP14x73	23+79.55	75.81	614.91	594.00	20.9	608.00	7
	44	HP14x73	23+86.21	78.87	614.65	594.00	20.7	608.00	7
NORTHEAST WALL	45	HP14x73	23+30.56	75.41	616.33	594.00	22.3	608.00	8
	46	HP14x73	23+38.10	76.10	616.14	594.00	22.1	608.00	8
	47	HP14x73	23+45.57	77.40	615.93	594.00	21.9	608.00	8
	48	HP14x73	23+52.91	79.26	615.69	594.00	21.7	608.00	8
	49	HP14x73	23+60.09	81.69	615.44	594.00	21.4	610.00	5
	50	HP14x73	23+67.05	84.65	615.18	594.00	21.2	611.00	4
	51	HP14x73	23+73.86	87.78	614.92	594.00	20.9	612.00	3
	52	HP14x73	23+80.67	90.91	614.66	594.00	20.7	612.00	3

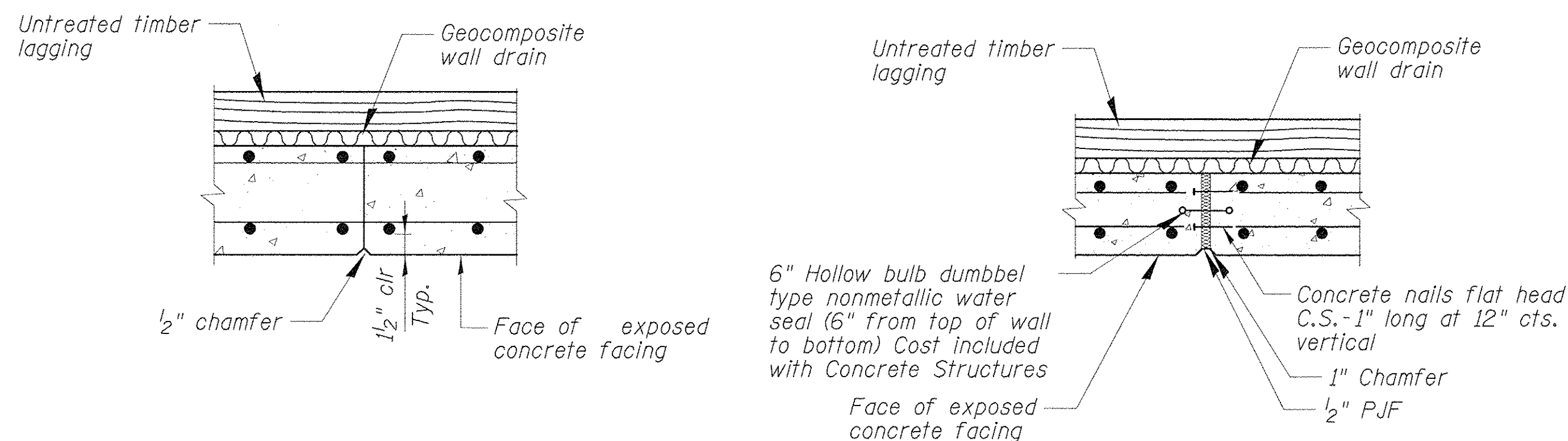


TYPICAL SECTION THRU DRIVEN SOLDIER PILE WALL

Note: Weep hole coring to be included in the cost of "Concrete Structures"

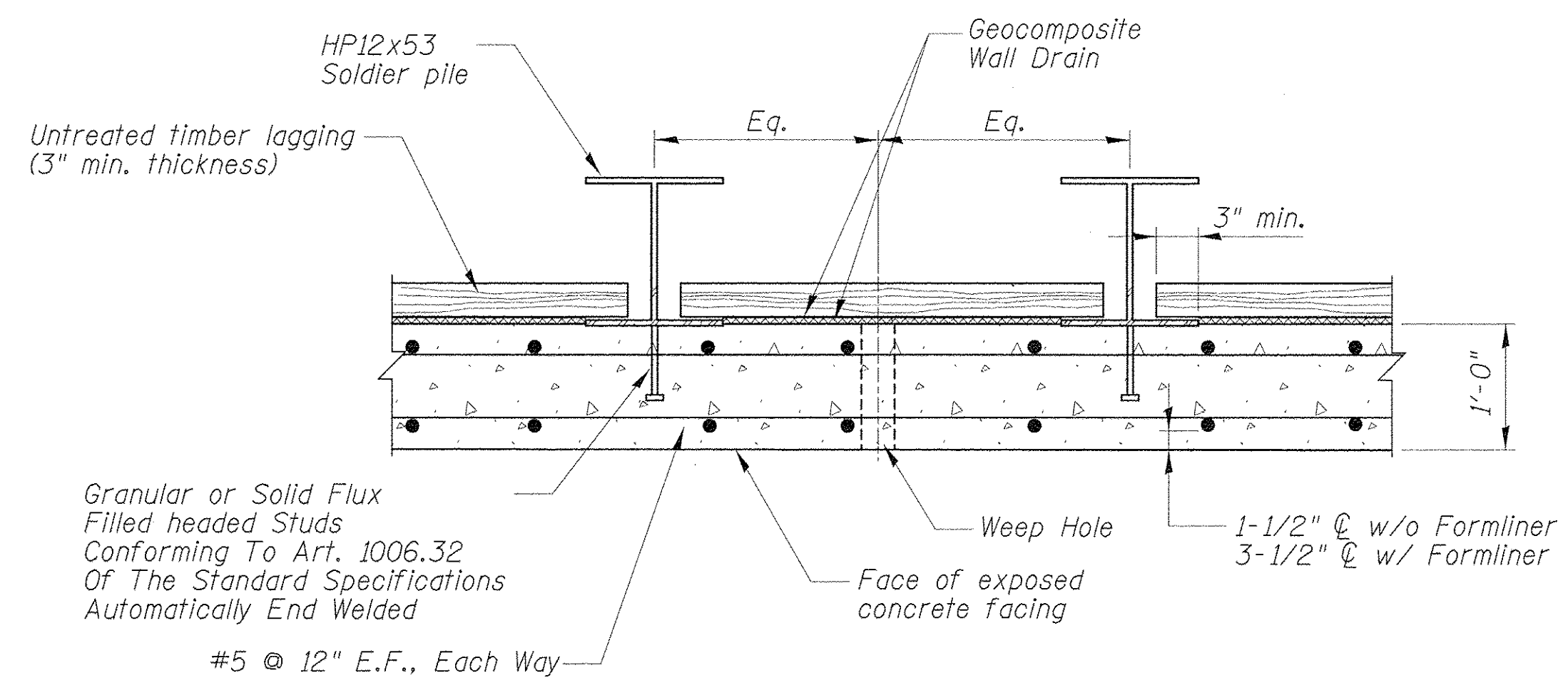
PILE DATA

(Station & Offset given with respect to back face of wall)
One test pile to be provided for each wall (4 Total).



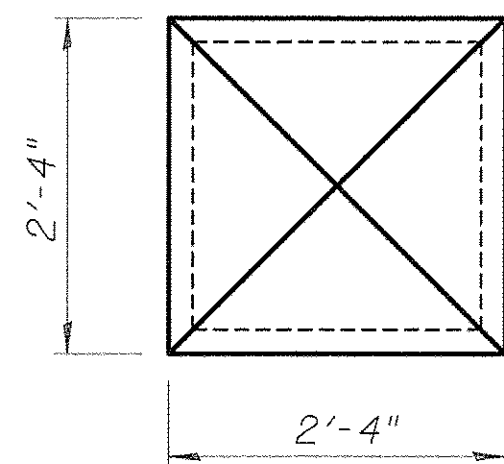
CONSTRUCTION JOINT

EXPANSION JOINT

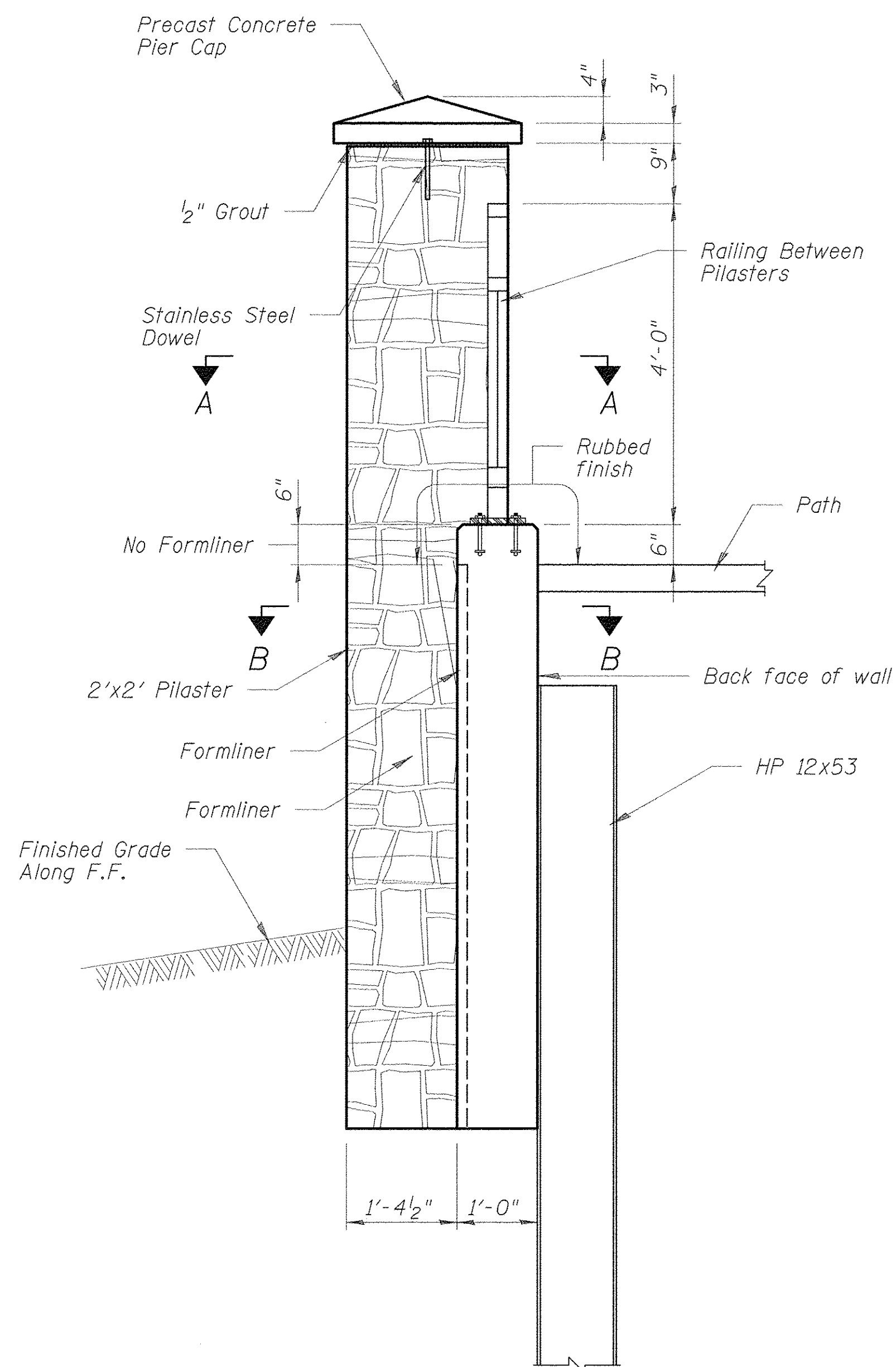


SECTION THRU DRILLED SOLDIER PILE WALL

FILE NAME =	USER NAME = jstrick	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US ROUTE 52 RIVER ROAD WALL DETAILS	F.A.P. RTE. 607	SECTION 15-00040-00-CH	COUNTY WILL	TOTAL SHEETS 138	SHEET NO. 104	
Default	PLOT SCALE = 1'	CHECKED -	REVISED -			CONTRACT NO. 61D82					
	PLOT DATE = 2/23/2017	DATE -	REVISED -			SCALE:	SHEET OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT		
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PIER CAP DETAIL



TYPICAL SECTION AT PILASTER

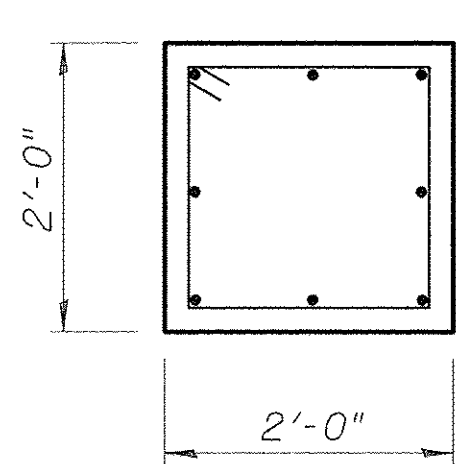
BAR LIST-NW WALL

Bar	No.	Size	Length	Shape
h ₁₀ (E)	8	#5	8'-10"	————
h ₁₁ (E)	20	#5	31'-5"	————
h ₁₂ (E)	10	#5	27'-10"	————
h ₁₃ (E)	10	#5	31'-7"	————
h ₁₄ (E)	10	#5	27'-8"	————
s ₁₀ (E)	30	#4	7'-5"	□
s ₁₁ (E)	34	#4	8'-1"	□
v ₁₀ (E)*	20	#5	2'-10"	————
v ₁₁ (E)*	58	#5	3'-4"	————
v ₁₂ (E)*	58	#5	3'-10"	————
v ₁₃ (E)*	58	#5	4'-5"	————
v ₁₄ (E)	58	#5	9'-0"	————
v ₁₅ (E)	58	#5	4'-2"	————
v ₁₆ (E)	40	#6	6'-7"	————
v ₁₇ (E)	48	#6	4'-9"	————
v ₁₈ (E)	8	#6	13'-3"	————
Structure Excavation			Cu. Yd.	27.0
Concrete Structures			Cu. Yd.	30.9
Reinforcement Bars, Epoxy Coated			Pound	4,423

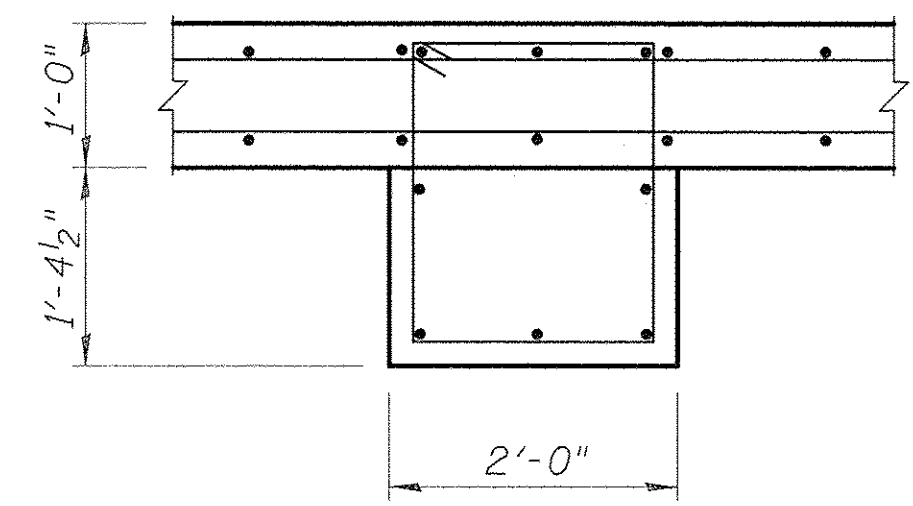
* Cut bars in field to fit

Reinforcement bars designated (E) shall be epoxy coated

Min. bar lap
 #4 - 2'-7"
 #5 - 3'-2" (vertical)
 #5 - 3'-7" (horizontal)
 #6 - 3'-10"



SECTION A-A



SECTION B-B

BAR LIST-SW WALL

Bar	No.	Size	Length	Shape
h ₂₀ (E)	18	#5	24'-4"	————
h ₂₁ (E)	12	#5	28'-1"	————
h ₂₂ (E)	36	#5	31'-5"	————
h ₂₃ (E)	4	#5	27'-8"	————
h ₂₄ (E)	20	#5	27'-10"	————
h ₂₅ (E)	22	#5	31'-7"	————
s ₂₀ (E)	40	#4	7'-5"	□
s ₂₁ (E)	74	#4	8'-1"	□
v ₂₀ (E)*	50	#5	5'-8"	————
v ₂₁ (E)*	50	#5	7'-0"	————
v ₂₂ (E)	232	#5	8'-0"	————
v ₂₃ (E)	58	#5	3'-9"	————
v ₂₄ (E)	58	#5	4'-4"	————
v ₂₅ (E)	58	#5	4'-10"	————
v ₂₆ (E)	58	#5	5'-4"	————
v ₂₇ (E)	64	#6	6'-7"	————
v ₂₈ (E)	24	#6	8'-4"	————
v ₂₉ (E)	40	#6	11'-2"	————
Structure Excavation			Cu. Yd.	100.3
Concrete Structures			Cu. Yd.	63.4
Reinforcement Bars, Epoxy Coated			Pound	8,612

BAR LIST-NE WALL

Bar	No.	Size	Length	Shape
h ₃₀ (E)	16	#5	33'-5"	————
h ₃₁ (E)	6	#5	30'-8"	————
h ₃₂ (E)	12	#5	30'-10"	————
h ₃₃ (E)	6	#5	14'-10"	————
s ₃₀ (E)	15	#4	7'-5"	□
s ₃₁ (E)	24	#4	8'-1"	□
v ₃₀ (E)	62	#5	8'-0"	————
v ₃₁ (E)	62	#5	4'-6"	————
v ₃₂ (E)*	32	#5	7'-4"	————
v ₃₃ (E)*	32	#5	4'-9"	————
v ₃₄ (E)	24	#6	8'-11"	————
v ₃₅ (E)	8	#6	8'-7"	————
v ₃₆ (E)	8	#6	6'-0"	————
Structure Excavation			Cu. Yd.	12.5
Concrete Structures			Cu. Yd.	22.4
Reinforcement Bars, Epoxy Coated			Pound	3,140

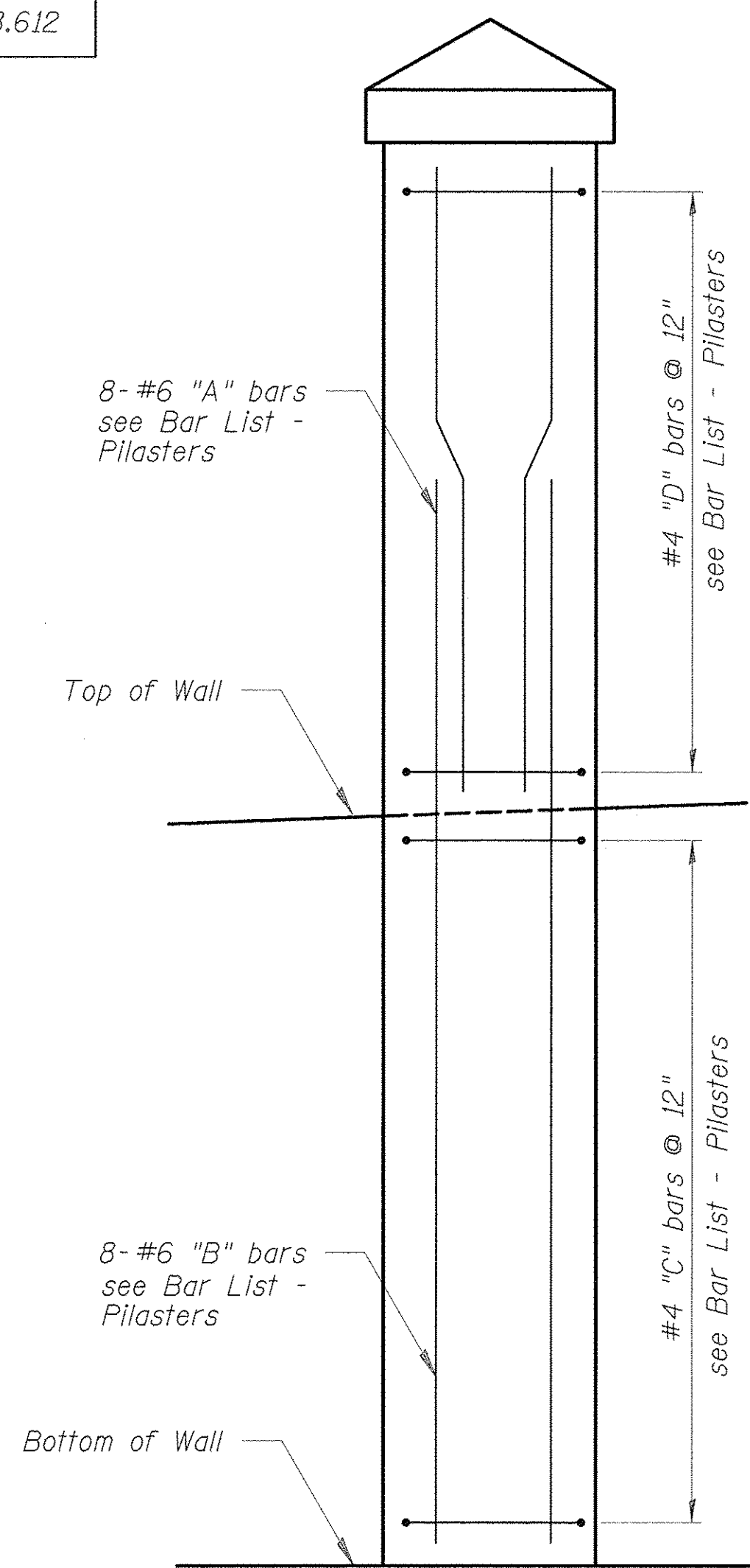
BAR LIST-SE WALL

Bar	No.	Size	Length	Shape
h ₄₀ (E)	22	#5	39'-5"	————
h ₄₁ (E)	20	#5	30'-10"	————
s ₄₀ (E)	15	#4	7'-5"	□
s ₄₁ (E)	31	#4	8'-1"	□
v ₄₀ (E)	138	#5	7'-6"	————
v ₄₁ (E)	138	#5	6'-0"	————
v ₄₂ (E)	24	#6	9'-4"	————
v ₄₃ (E)	24	#6	8'-11"	————
Structure Excavation			Cu. Yd.	31.4
Concrete Structures			Cu. Yd.	34.6
Reinforcement Bars, Epoxy Coated			Pound	4,390

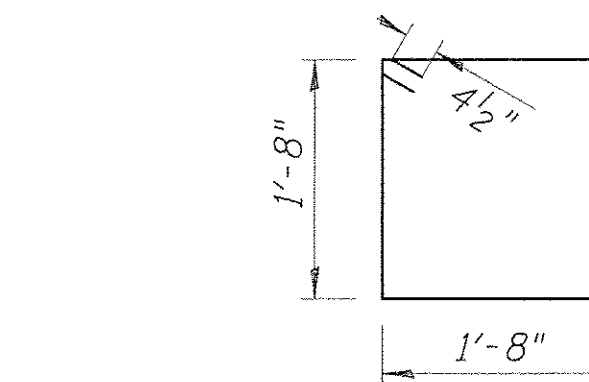
BAR LIST-PILASTERS

See individual wall bar list for count and lengths.

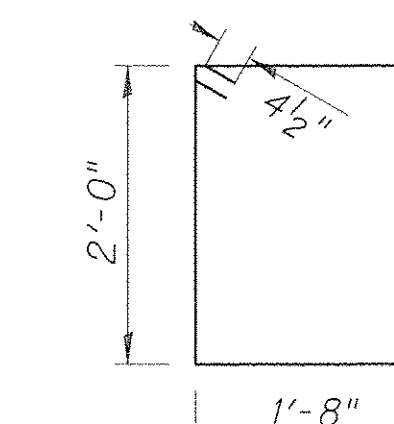
Column	A	B	C	D	
NW WALL	A	v ₁₇ (E)	v ₁₆ (E)	s ₁₁ (E)	s ₁₀ (E)
	B	v ₁₇ (E)	v ₁₆ (E)	s ₁₁ (E)	s ₁₀ (E)
	C	v ₁₇ (E)	v ₁₆ (E)	s ₁₁ (E)	s ₁₀ (E)
	D	v ₁₇ (E)	v ₁₆ (E)	s ₁₁ (E)	s ₁₀ (E)
	E	v ₁₇ (E)	v ₁₆ (E)	s ₁₁ (E)	s ₁₀ (E)
	F	v ₁₈ (E)	v ₁₆ (E)	s ₁₁ (E)	s ₁₀ (E)
	G	v ₂₈ (E)	v ₂₇ (E)	s ₂₁ (E)	s ₂₀ (E)
SW WALL	H	v ₂₈ (E)	v ₂₇ (E)	s ₂₁ (E)	s ₂₀ (E)
	I	v ₂₈ (E)	v ₂₇ (E)	s ₂₁ (E)	s ₂₀ (E)
	J	v ₂₉ (E)	v ₂₇ (E)	s ₂₁ (E)	s ₂₀ (E)
	K	v ₂₉ (E)	v ₂₇ (E)	s ₂₁ (E)	s ₂₀ (E)
	L	v ₂₉ (E)	v ₂₇ (E)	s ₂₁ (E)	s ₂₀ (E)
	M	v ₂₉ (E)	v ₂₇ (E)	s ₂₁ (E)	s ₂₀ (E)
	N	v ₂₉ (E)	v ₂₇ (E)	s ₂₁ (E)	s ₂₀ (E)
NE WALL	O	v ₃₅ (E)	v ₃₄ (E)	s ₃₁ (E)	s ₃₀ (E)
	P	v ₃₆ (E)	v ₃₄ (E)	s ₃₁ (E)	s ₃₀ (E)
SE WALL	Q	- - -	v ₃₄ (E)	s ₃₁ (E)	s ₃₀ (E)
	R	v ₄₄ (E)	v ₄₃ (E)	s ₄₁ (E)	s ₄₀ (E)
	S	v ₄₄ (E)	v ₄₃ (E)	s ₄₁ (E)	s ₄₀ (E)
T	v ₄₄ (E)	v ₄₃ (E)	s ₄₁ (E)	s ₄₀ (E)	



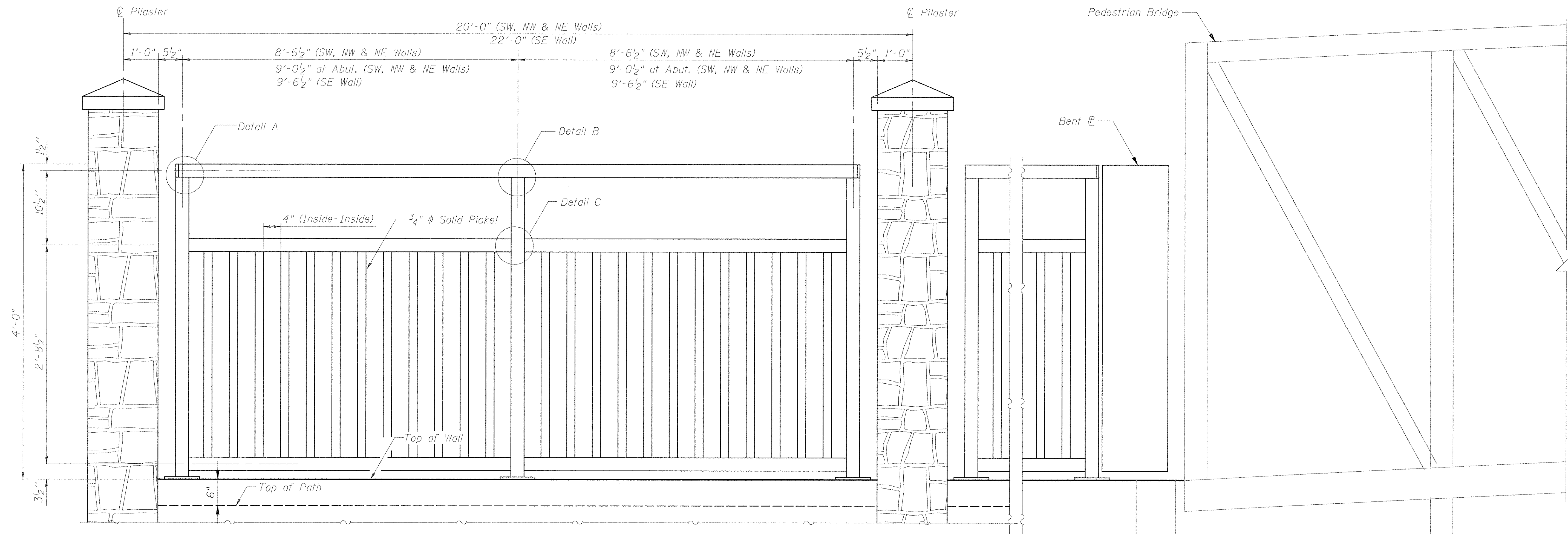
TYPICAL REINFORCEMENT AT PILASTER



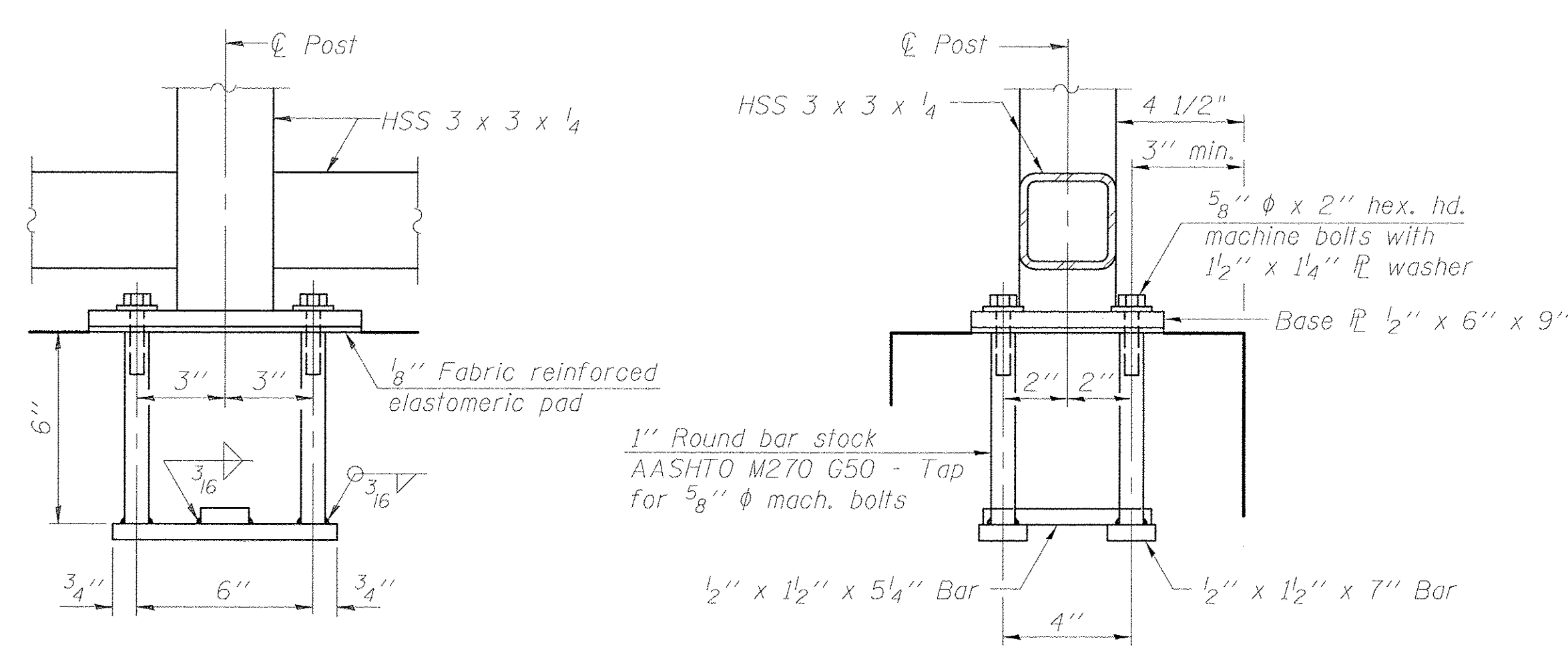
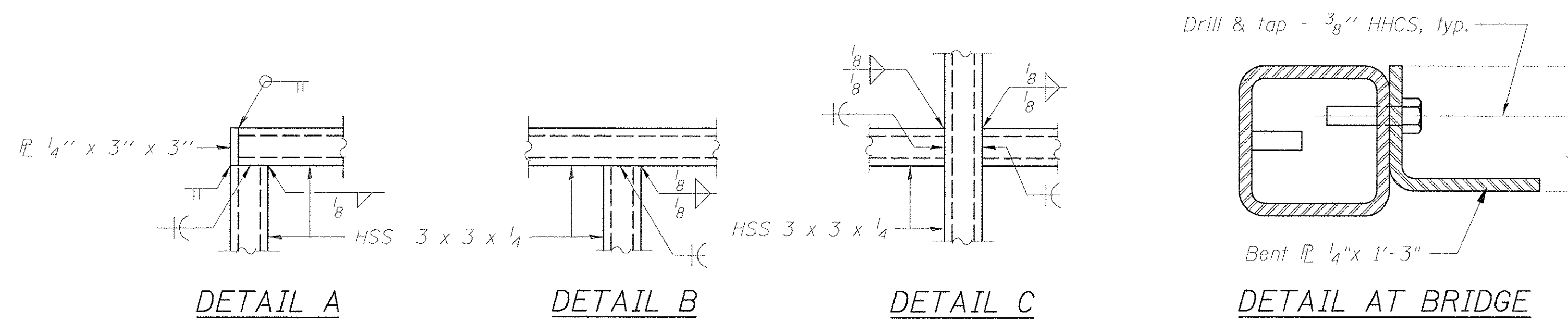
s₁₀(E), s₂₀(E), s₃₀(E), s₄₀(E) BAR



s₁₁(E), s₂₁(E), s₃₁(E), s₄₁(E) BAR

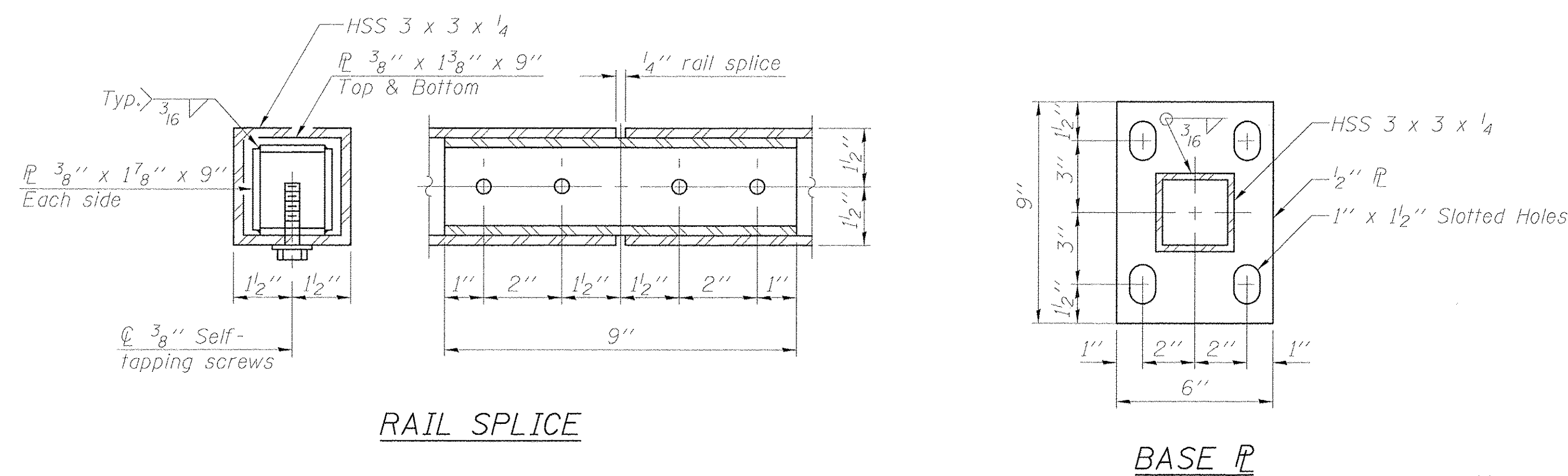


BICYCLE RAILING



ANCHOR BOLT DETAILS

In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting 5/8" ϕ anchor rods according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications.



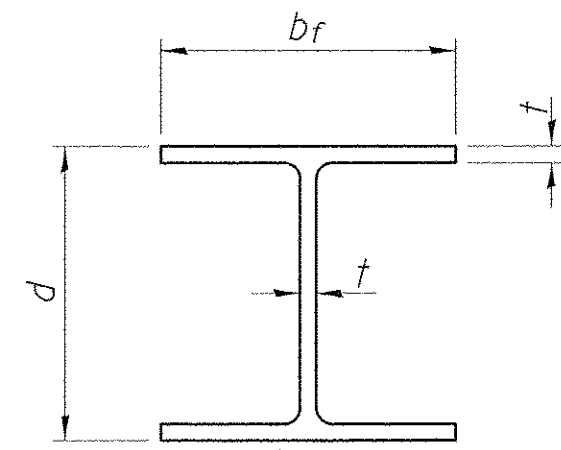
RAIL SPLICE

BASE PL

All post, railing, splices, anchor devices, and bent plates shall be powder coated black.

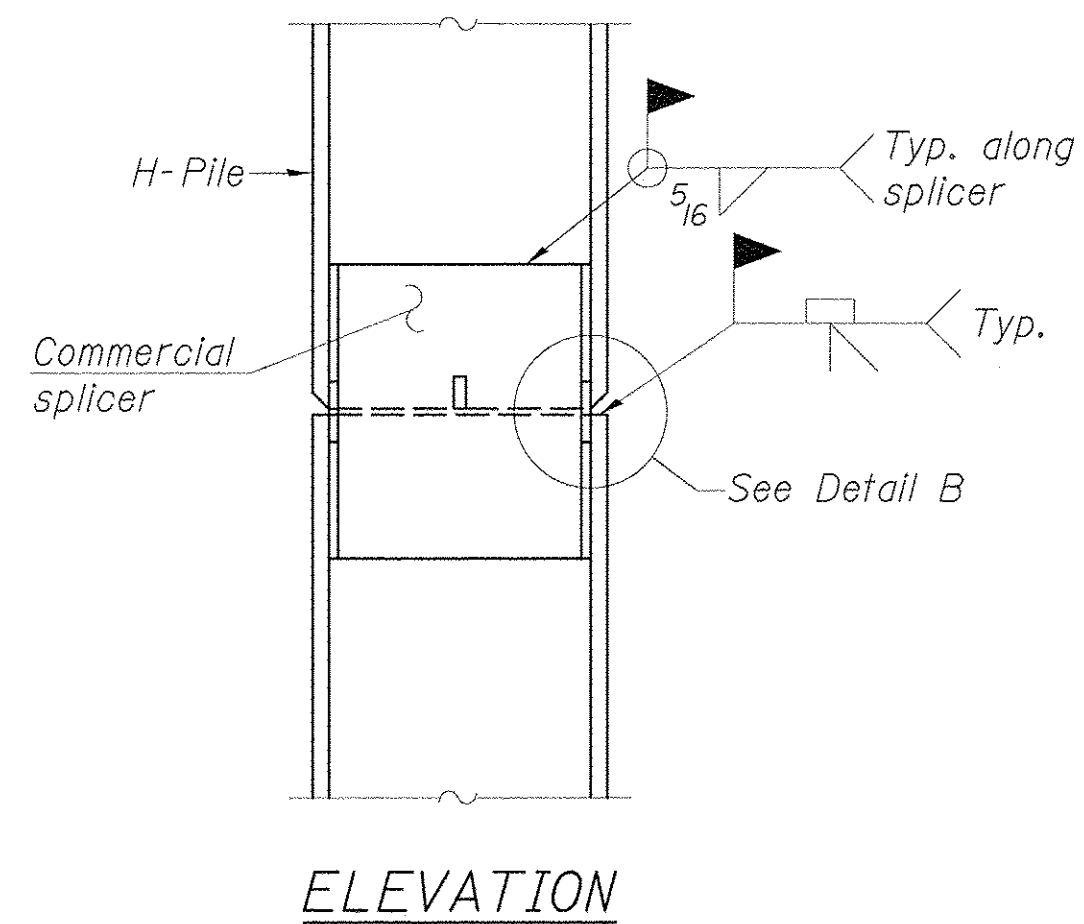
BILL OF MATERIAL

Item	Unit	Quantity
Bicycle Railing	Foot	353'-0"

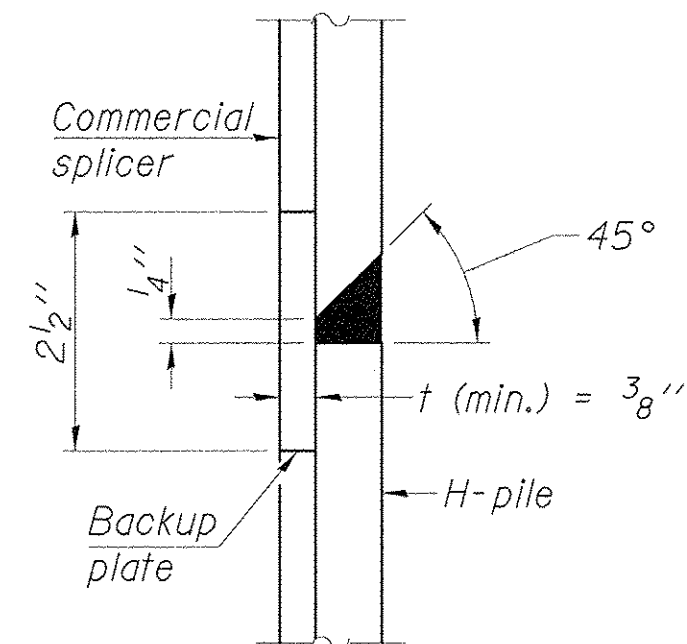


STEEL PILE TABLE

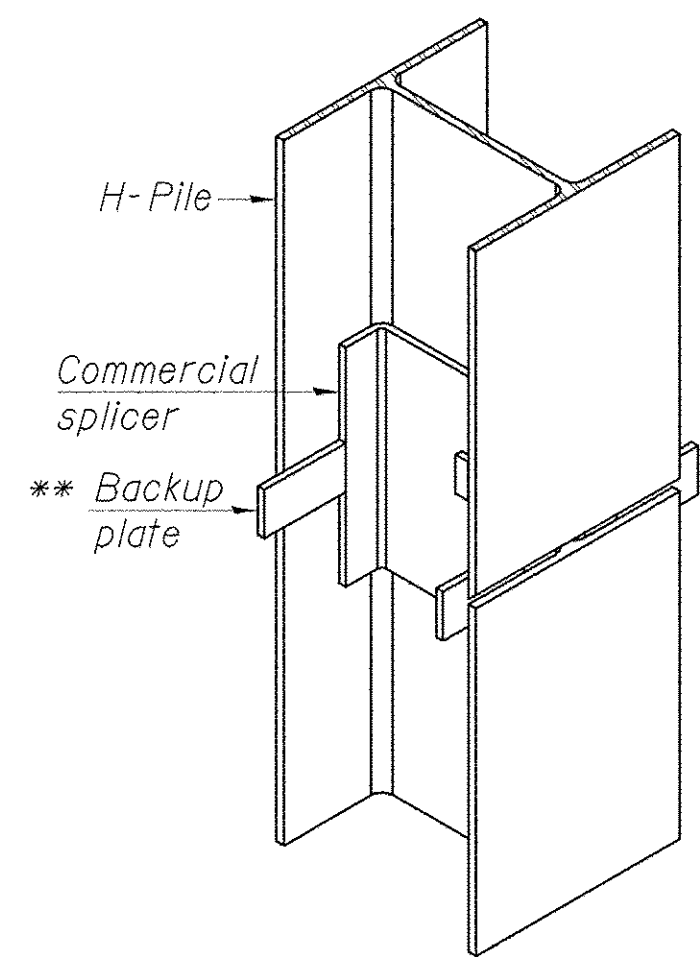
Designation	Depth d	Flange width br	Web and Flange thickness t	Encasement diameter A
HP 14x117	14 1/4"	14 7/8"	1 3/16"	30"
x102	14"	14 3/4"	1 1/16"	30"
x89	13 7/8"	14 3/4"	5/8"	30"
x73	13 5/8"	14 5/8"	1/2"	30"
HP 12x84	12 1/4"	12 1/4"	1 1/16"	24"
x74	12 1/8"	12 1/4"	5/8"	24"
x63	12"	12 1/8"	1/2"	24"
x53	11 3/4"	12"	7/16"	24"
HP 10x57	10"	10 1/4"	9/16"	24"
x42	9 3/4"	10 1/8"	7/16"	24"
HP 8x36	8"	8 1/8"	7/16"	18"



ELEVATION

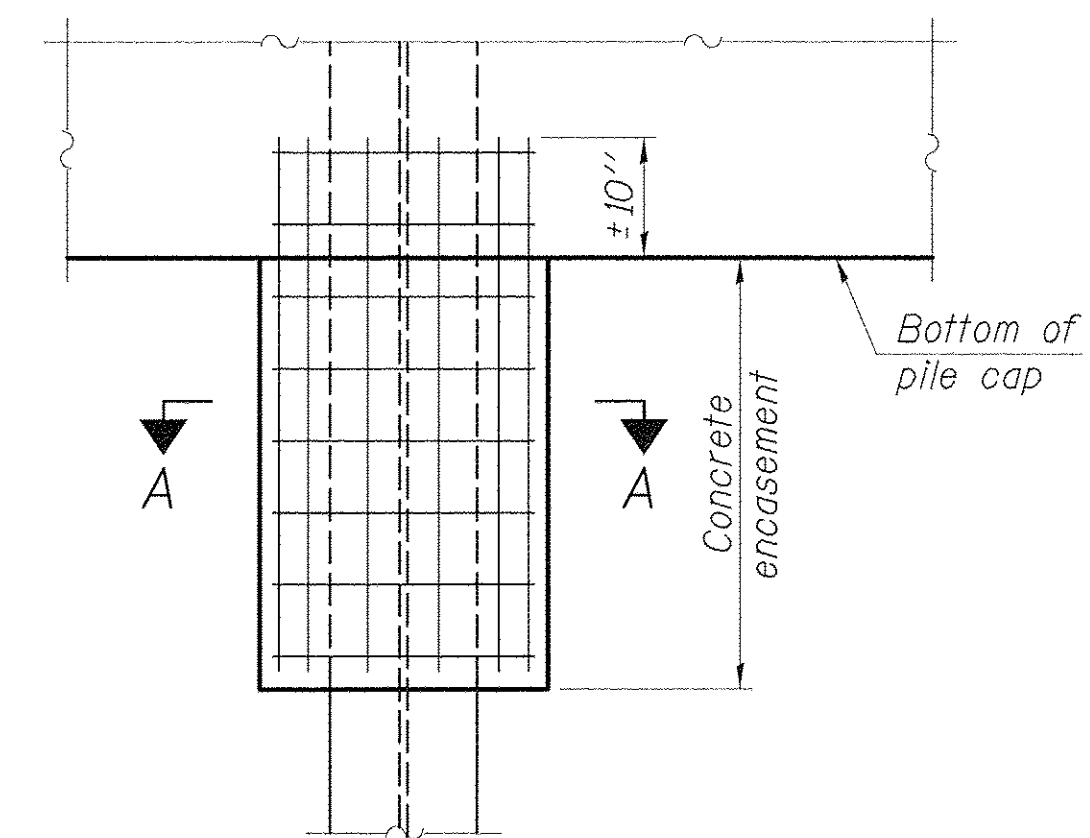


DETAIL "B"



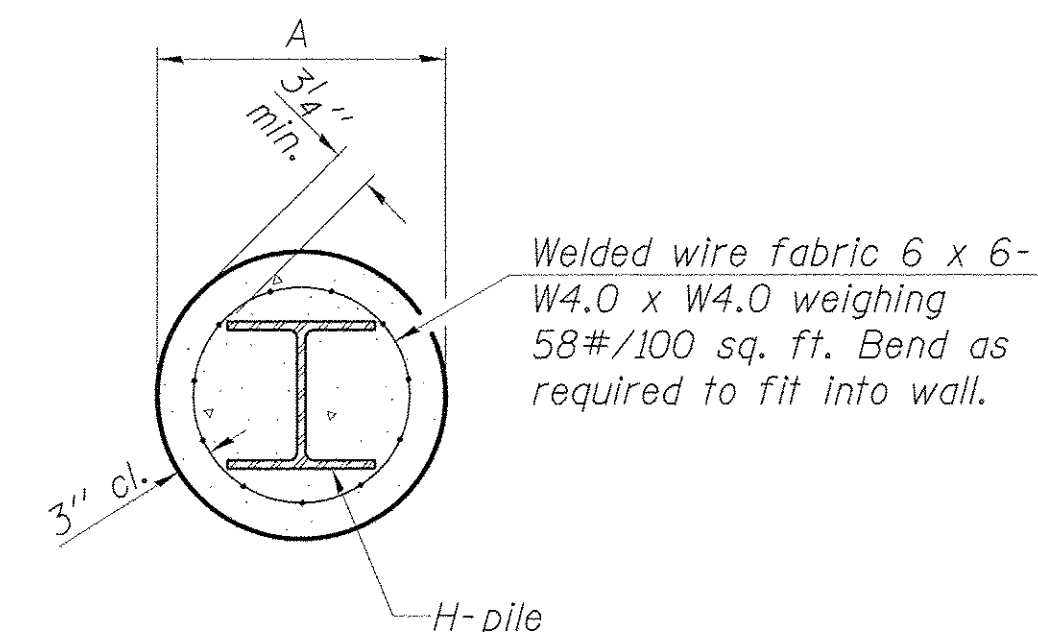
ISOMETRIC VIEW

WELDED COMMERCIAL SPLICE



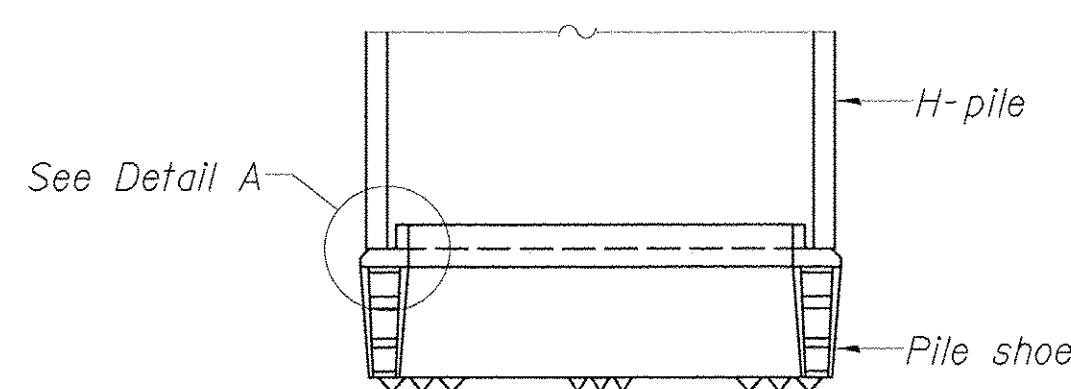
ELEVATION

PILE ENCASEMENT

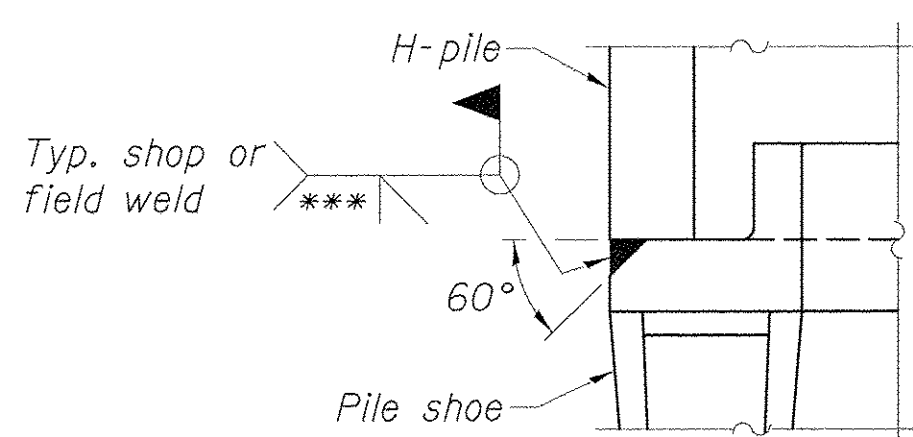


SECTION A-A

Note:
Forms for encasement may be omitted when soil conditions permit.

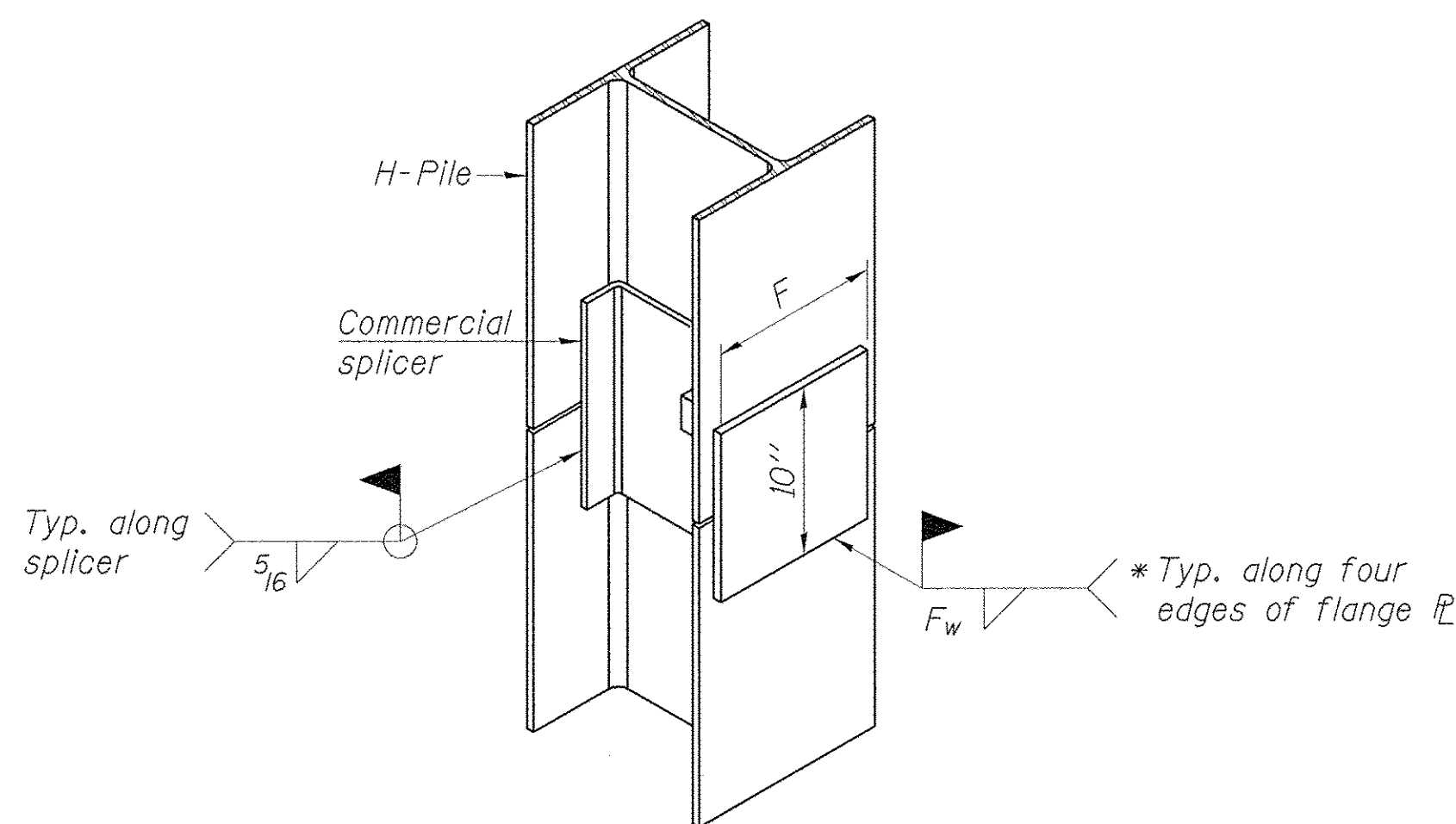


ELEVATION



DETAIL A

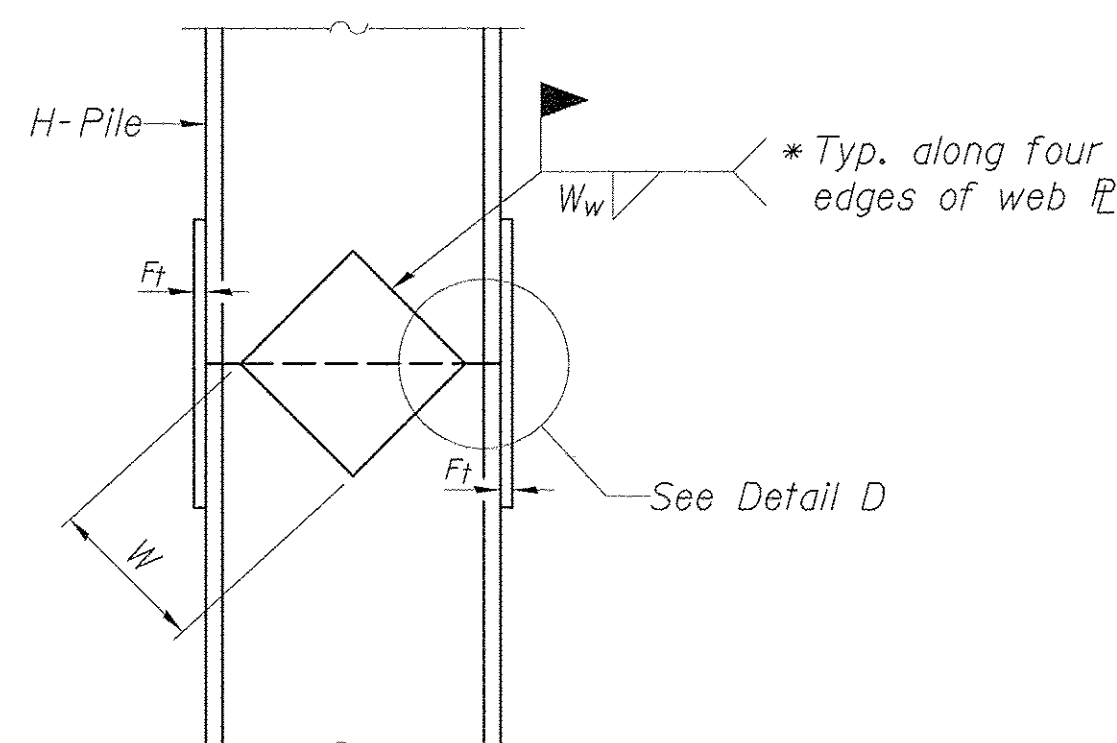
H-PILE SHOE ATTACHMENT



ISOMETRIC VIEW

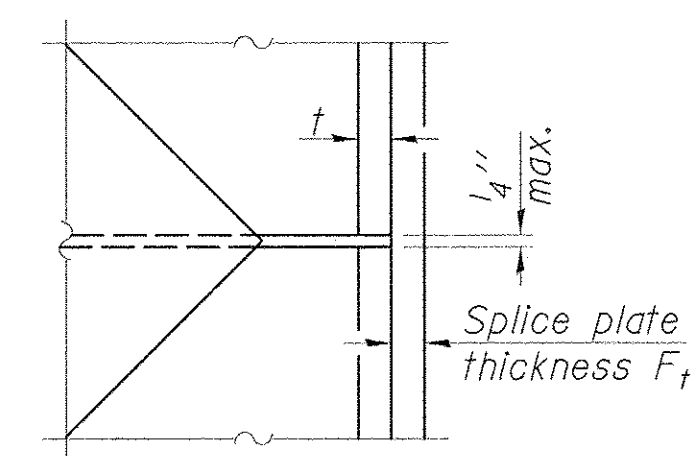
WELDED COMMERCIAL SPLICE ALTERNATE

- * Interrupt welds 1/4" from end of web and/or each flange.
- ** Remove portions of backup plates that extend outside the flanges.
- *** Weld size per pile shoe manufacturer (5/16" min.).



ELEVATION

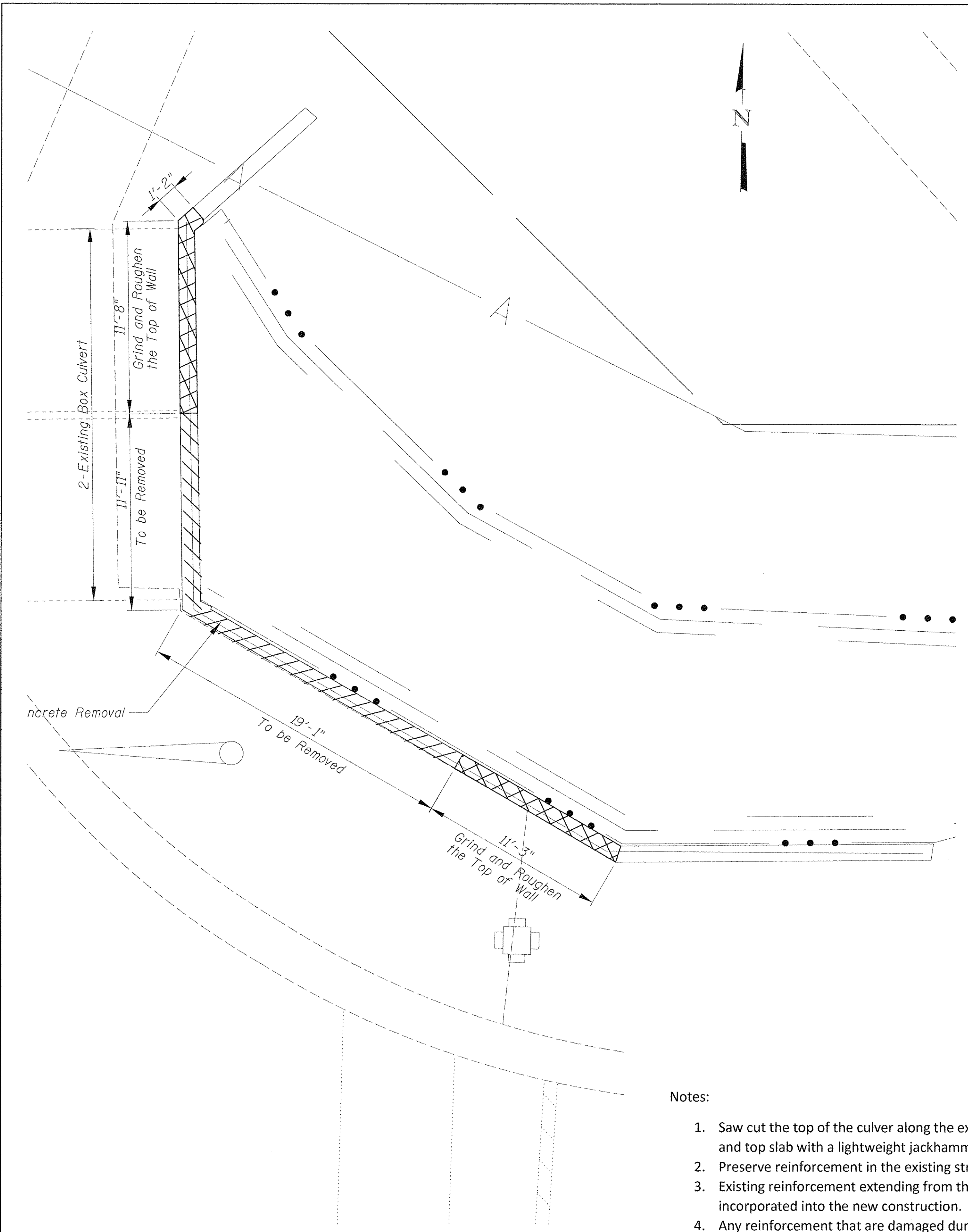
WELDED PLATE FIELD SPLICE



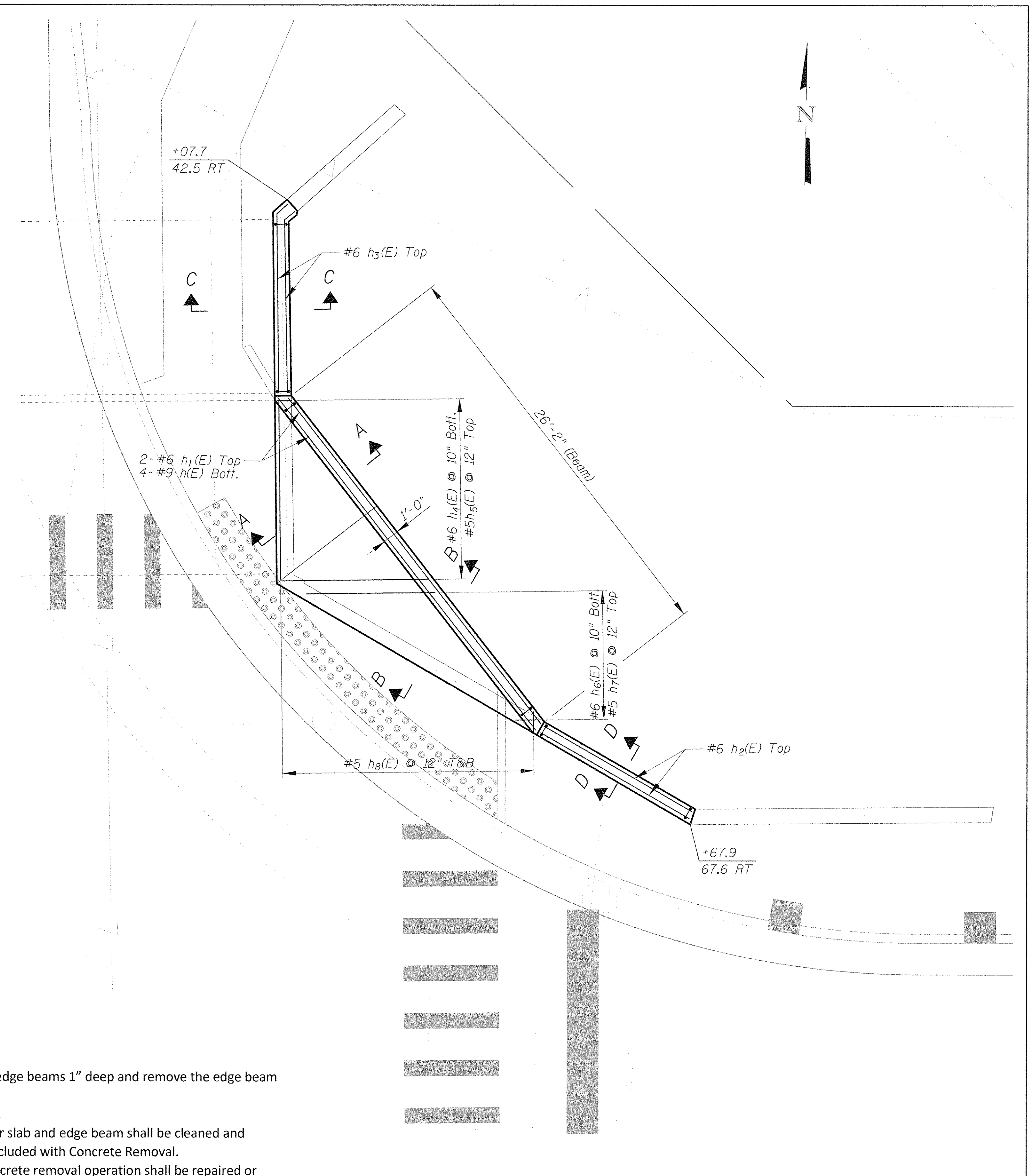
DETAIL D

Designation	F	Ft	Fw	W	Wt	Ww
HP 14x117	12 1/2"	1"	7/8"	7 3/4"	5/8"	1/2"
x102	12 1/2"	7/8"	3/4"	7 3/4"	5/8"	1/2"
x89	12 1/2"	3/4"	1/16"	7 3/4"	5/8"	1/2"
x73	12 1/2"	5/8"	9/16"	7 3/4"	5/8"	1/2"
HP 12x84	10"	7/8"	1/16"	6 1/2"	5/8"	1/2"
x74	10"	7/8"	1/16"	6 1/2"	5/8"	1/2"
x63	10"	5/8"	1/2"	6 1/2"	1/2"	3/8"
x53	10"	5/8"	1/2"	6 1/2"	1/2"	3/8"
HP 10x57	8"	3/4"	9/16"	5 1/4"	1/2"	3/8"
x42	8"	5/8"	9/16"	5 1/4"	1/2"	3/8"
HP 8x36	7"	5/8"	7/16"	4 1/4"	1/2"	3/8"

Note:
The steel H-piles shall be according to AASHTO M270 Grade 50.



REMOVAL PLAN



PROPOSED PLAN

Notes:

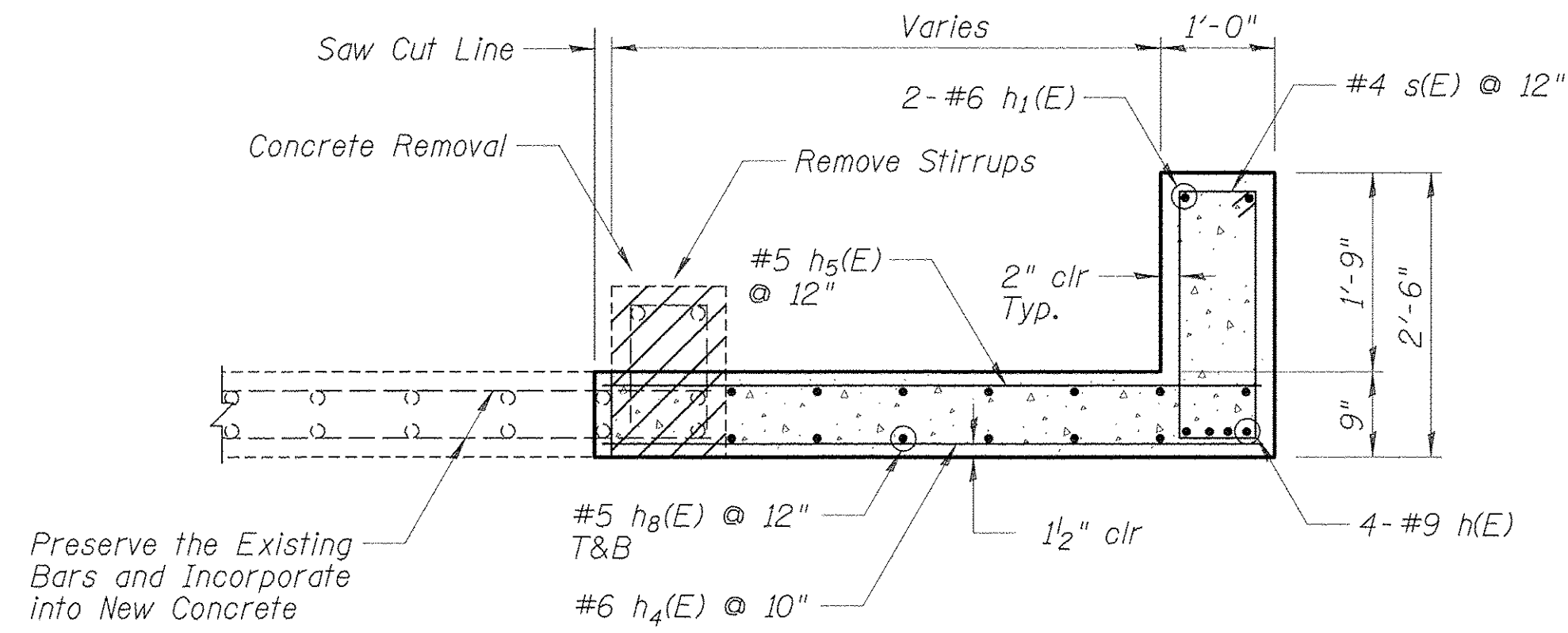
1. Saw cut the top of the culver along the existing edge beams 1' deep and remove the edge beam and top slab with a lightweight jackhammer.
2. Preserve reinforcement in the existing structure.
3. Existing reinforcement extending from the culver slab and edge beam shall be cleaned and incorporated into the new construction. Cost included with Concrete Removal.
4. Any reinforcement that are damaged during concrete removal operation shall be repaired or replaced using approved bar splicer or anchorage system. Cost included with Concrete Removal.
5. Saw cut and remove the top of the retaining wall, full width including the rebars, along creek as shown on the plans.

FILE NAME =	USER NAME = jstrick	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US ROUTE 52 RIVER ROAD CULVERT MODIFICATION			F.A.P. RTE. 607	SECTION 15-00040-00-CH	COUNTY WILL	TOTAL SHEETS 138	SHEET NO. 108
Default	PLOT SCALE = 4'	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 61D82				
	PLOT DATE = 2/23/2017	DATE -	REVISED -					ILLINOIS FED. AID PROJECT				
N:\Shorewood\070001\070001C\2016\C16-02\Struct\CULVERT_C1602-01.sht												

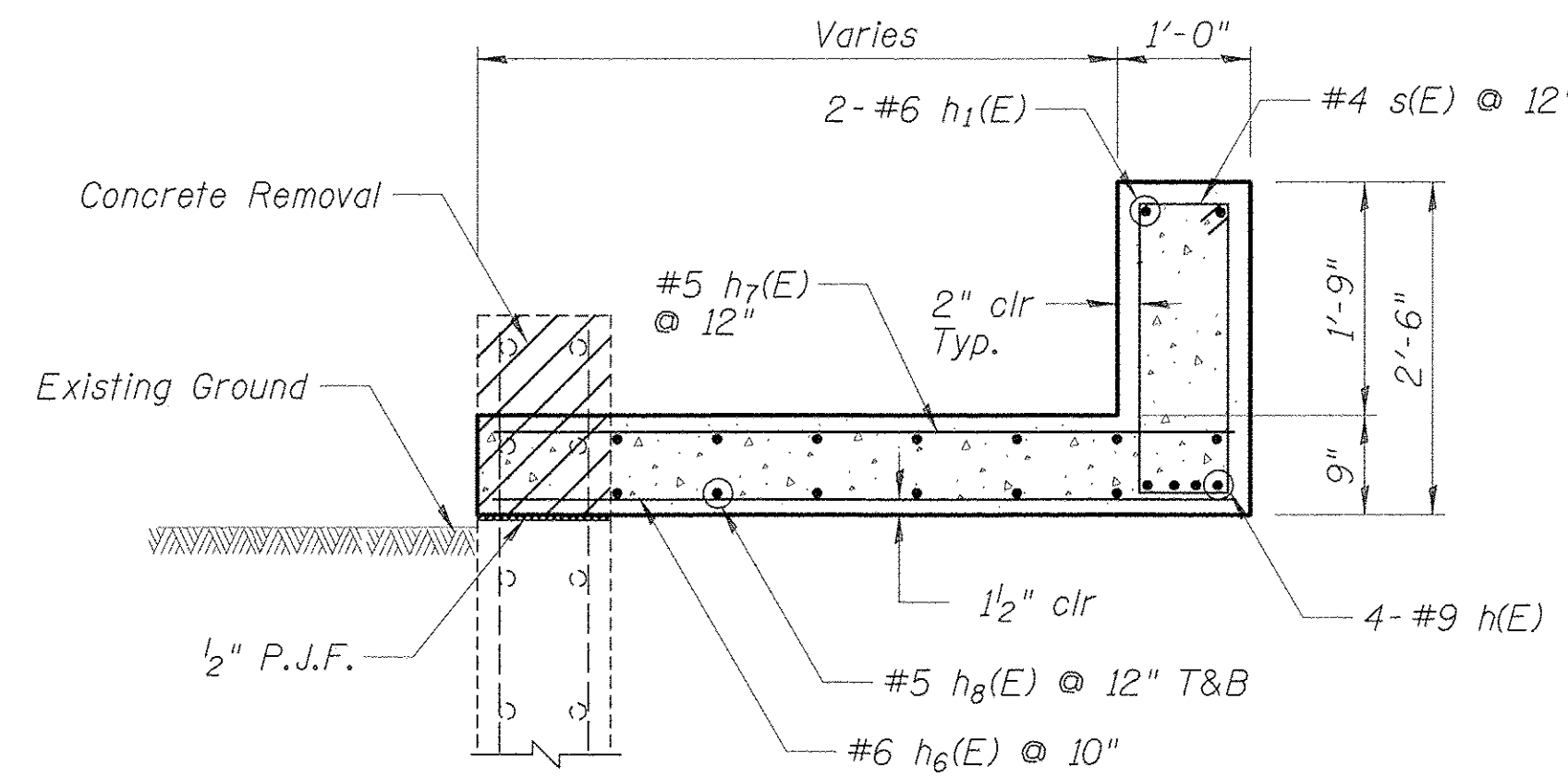
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d(E)	23	#6	1'-3"	
h(E)	4	#9	26'-2"	
h ₁ (E)	2	#6	26'-2"	
h ₂ (E)	2	#6	12'-1"	
h ₃ (E)	2	#6	13'-2"	
h ₄ (E)	7	#6	11'-10"	
h ₅ (E)	6	#5	11'-0"	
h ₆ (E)	5	#6	10'-0"	
h ₇ (E)	4	#5	10'-0"	
h ₈ (E)	16	#5	13'-9"	
s(E)	27	#4	6'-5"	□
u(E)	24	#4	2'-4"	□
Concrete Structures			Cu. Yd.	5.9
Reinforcement Bars, Epoxy Coated			Pound	1250

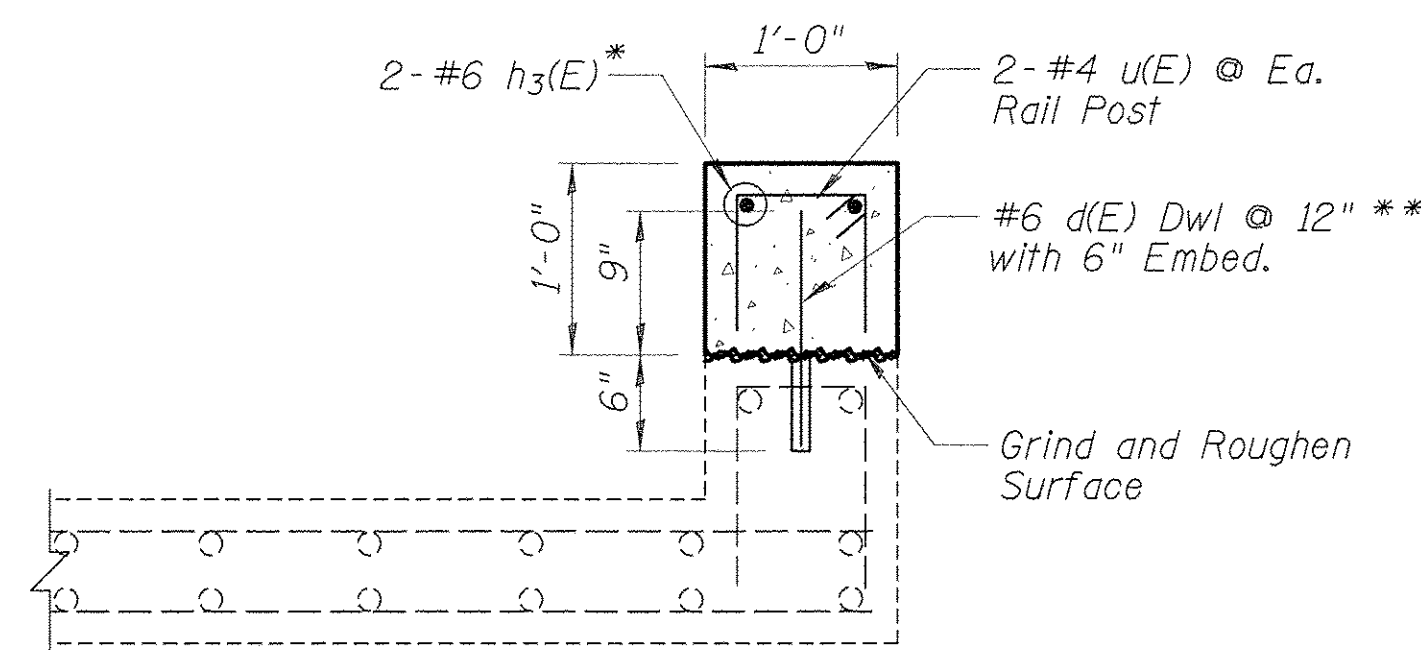
Reinforcement bars designated (E) shall be epoxy coated



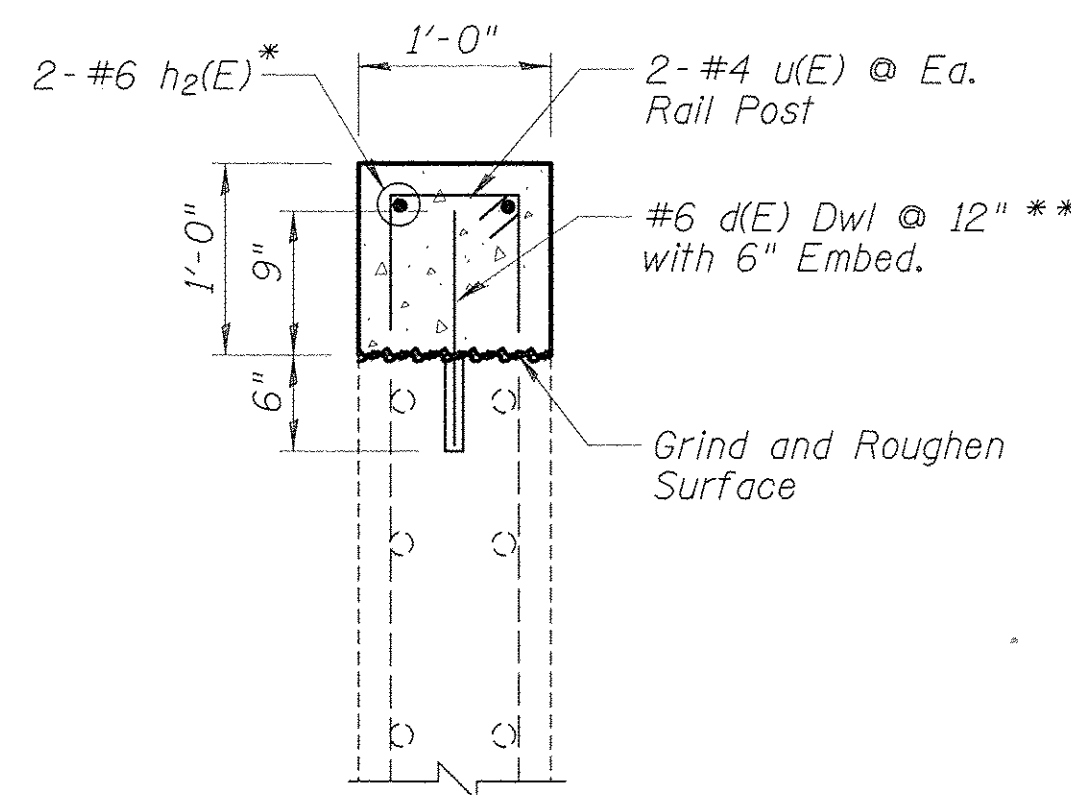
SECTION A-A



SECTION B-B

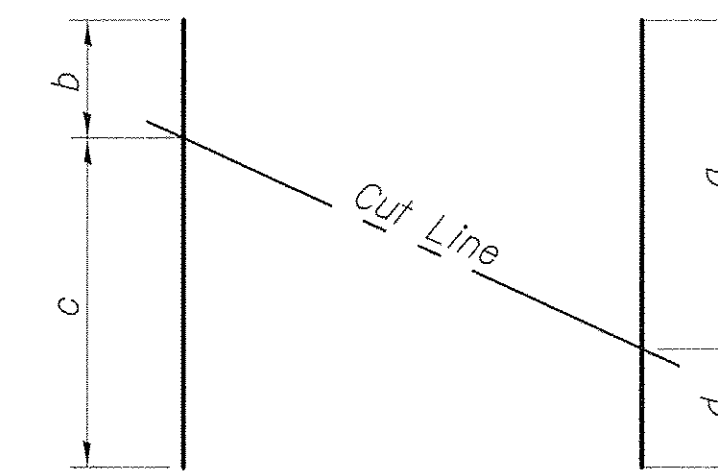


SECTION C-C

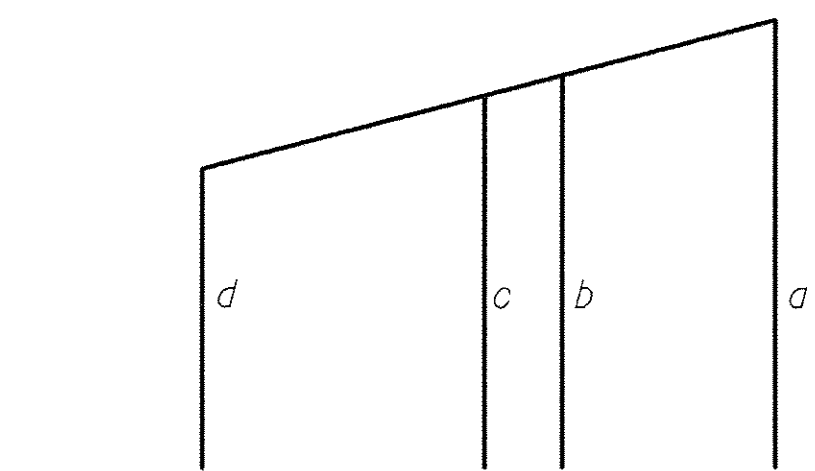


SECTION D-D

* Bend and Lap h₂(E) and h₃(E) Bars with h₁(E) Bars.
 ** Epoxy Grout d(E) Bars in 6" (Min.) Drilled Holes According to Section 584 of the IDOT Standard Specifications.

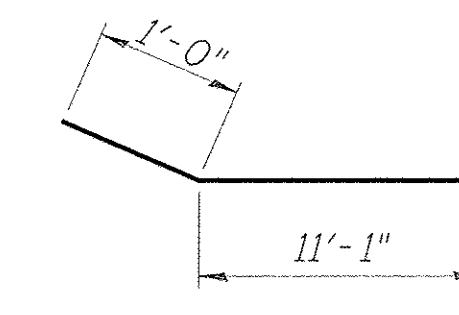


FIELD CUTTING DIAGRAM

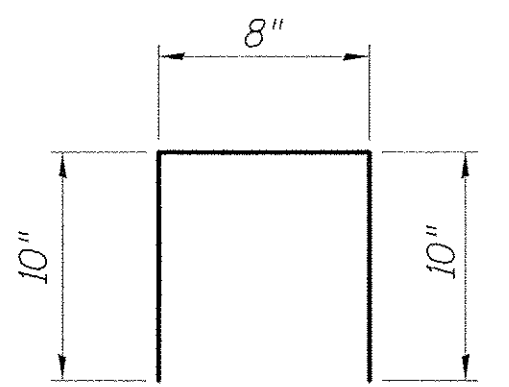


PLACEMENT DIAGRAM

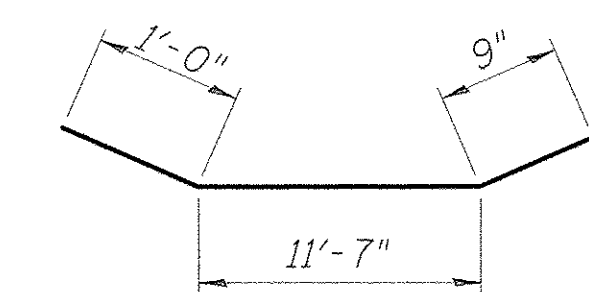
Bar	a	b	c	d
h ₄ (E)	10'-0"	6'-3"	5'-7"	1'-0"
h ₅ (E)	10'-0"	5'-11"	5'-1"	1'-0"
h ₆ (E)	9'-0"	5'-5"	4'-7"	1'-0"
h ₇ (E)	9'-0"	5'-7"	4'-5"	1'-0"
h ₈ (E)	11'-9"	7'-2"	6'-7"	2'-0"



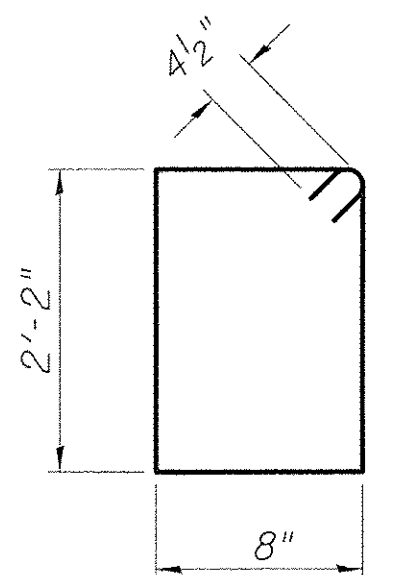
BAR h₂(E)



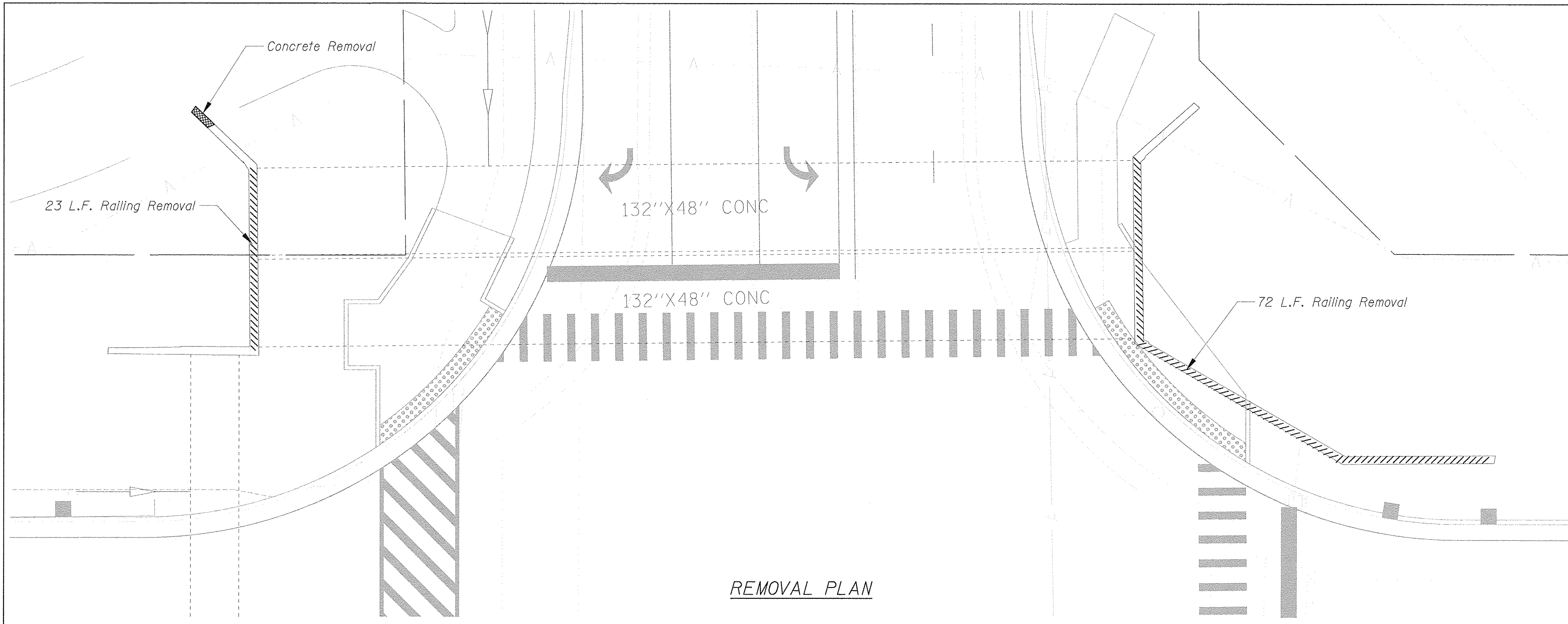
BAR u(E)



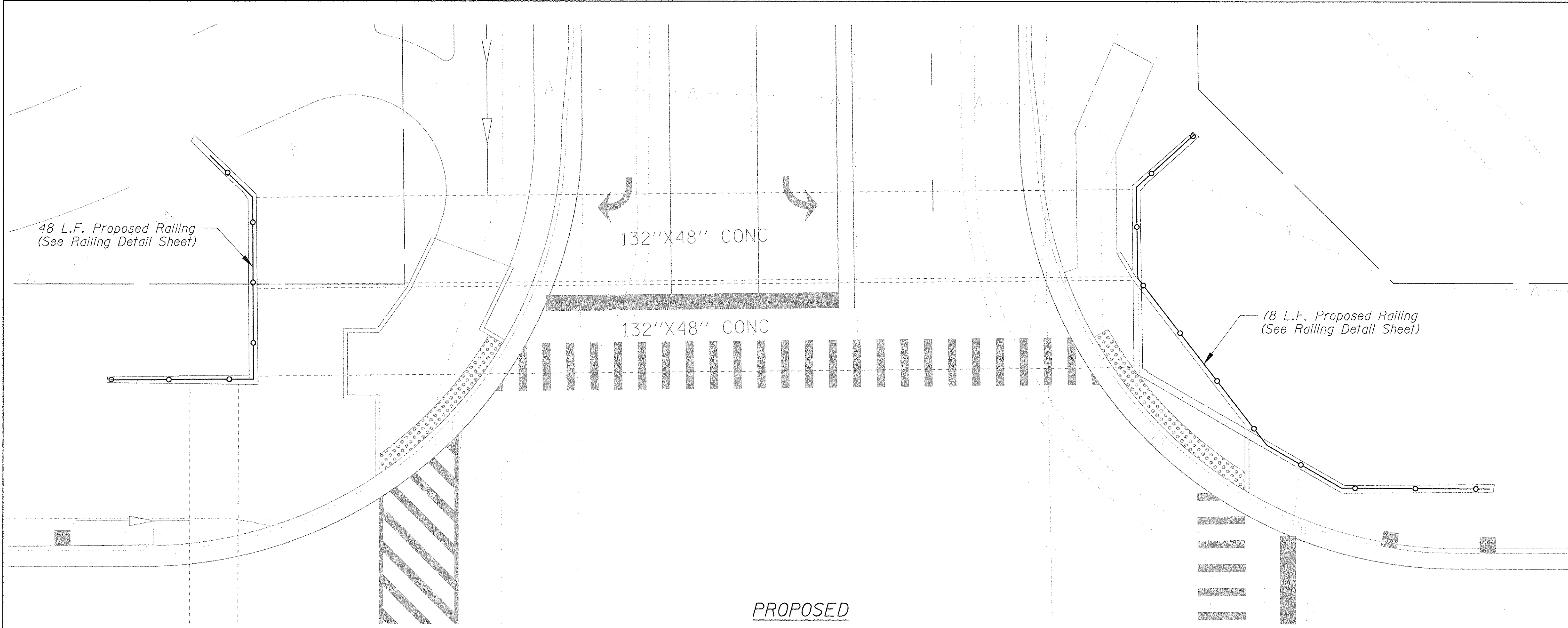
BAR h₃(E)



BARS s(E)

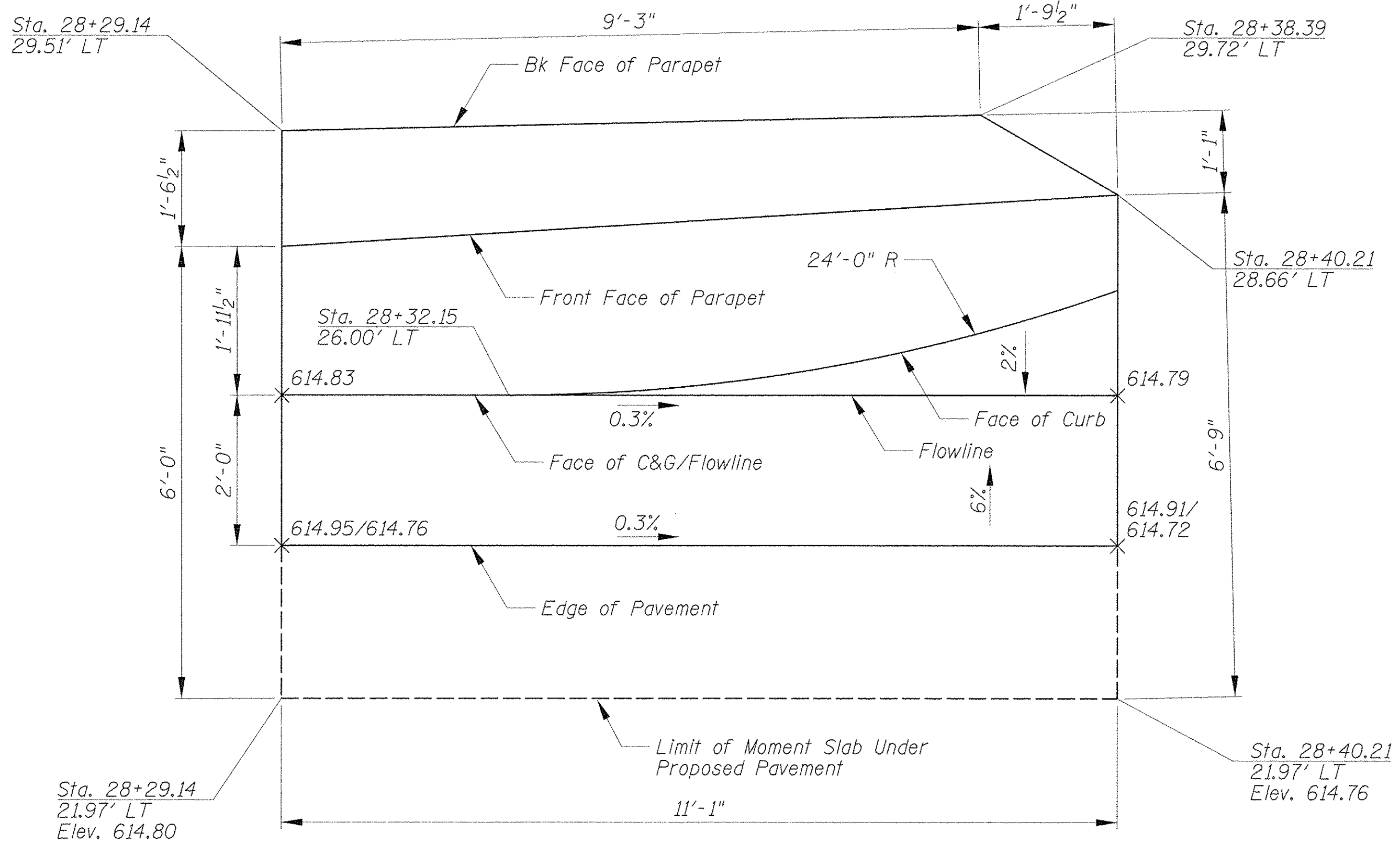


REMOVAL PLAN



PROPOSED

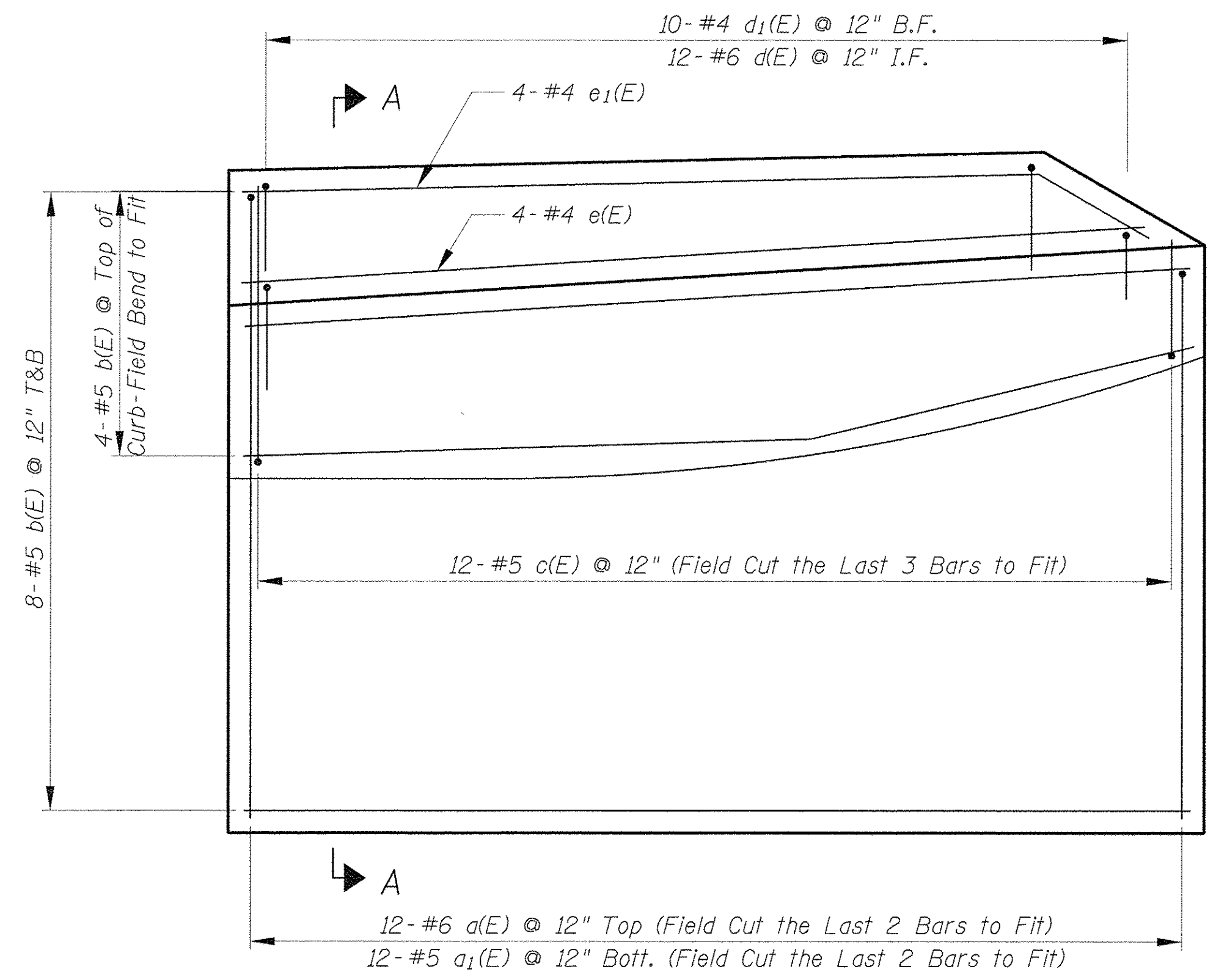
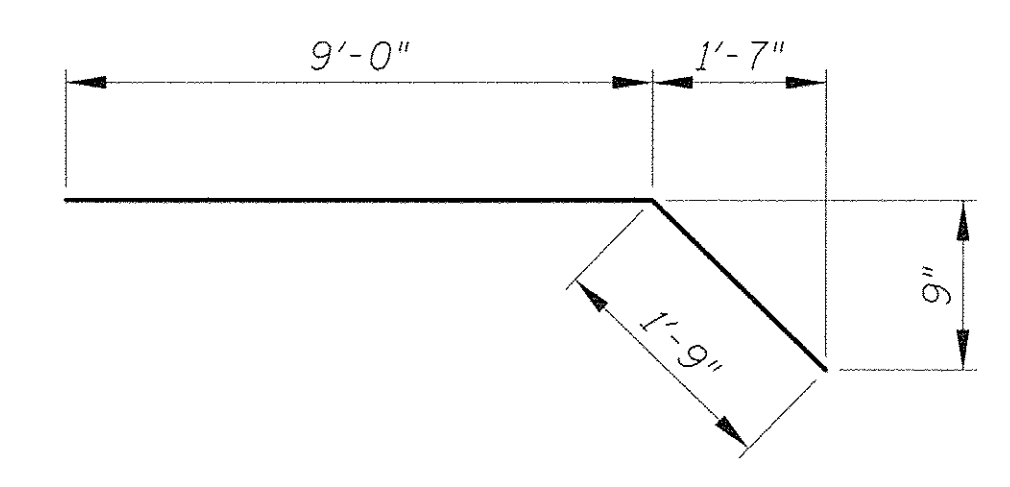
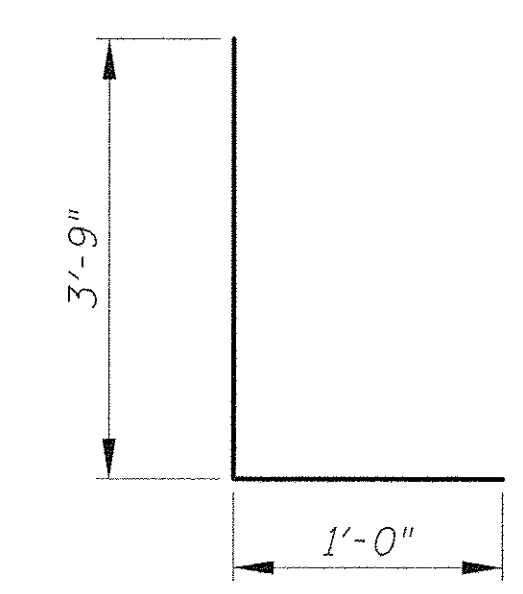
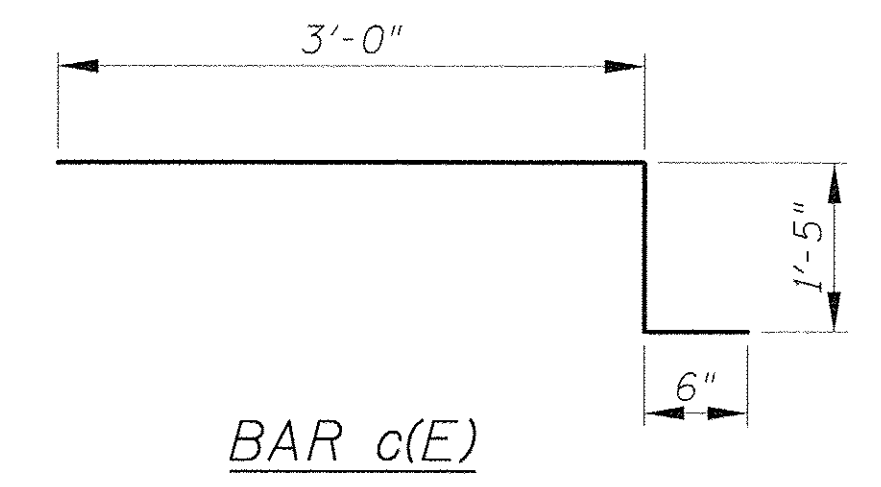
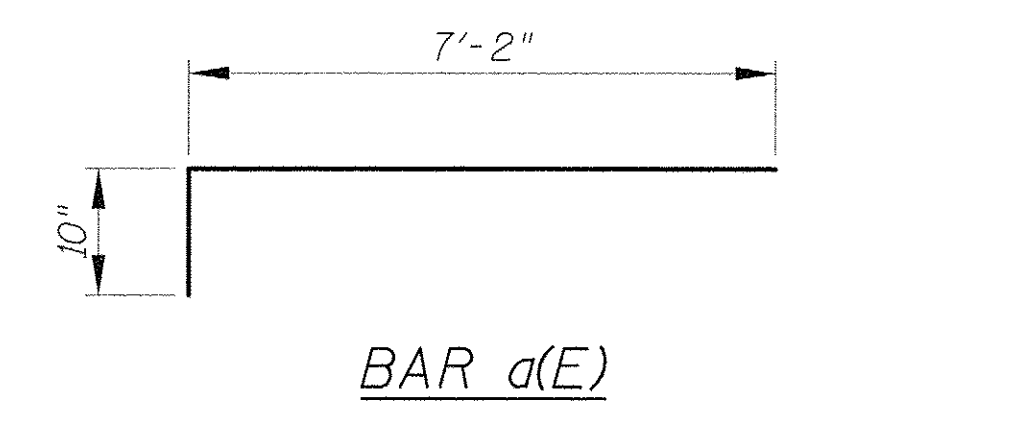
FILE NAME =	USER NAME = jstrick	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US ROUTE 52 RIVER ROAD CULVERT MODIFICATION			F.A.P. RTE. 607	SECTION 15-00040-00-CH	COUNTY WILL	TOTAL SHEETS 138	SHEET NO. 110
Default	PLOT SCALE = 8'	CHECKED -	REVISED -					SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 61D82	
	PLOT DATE = 2/23/2017	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							
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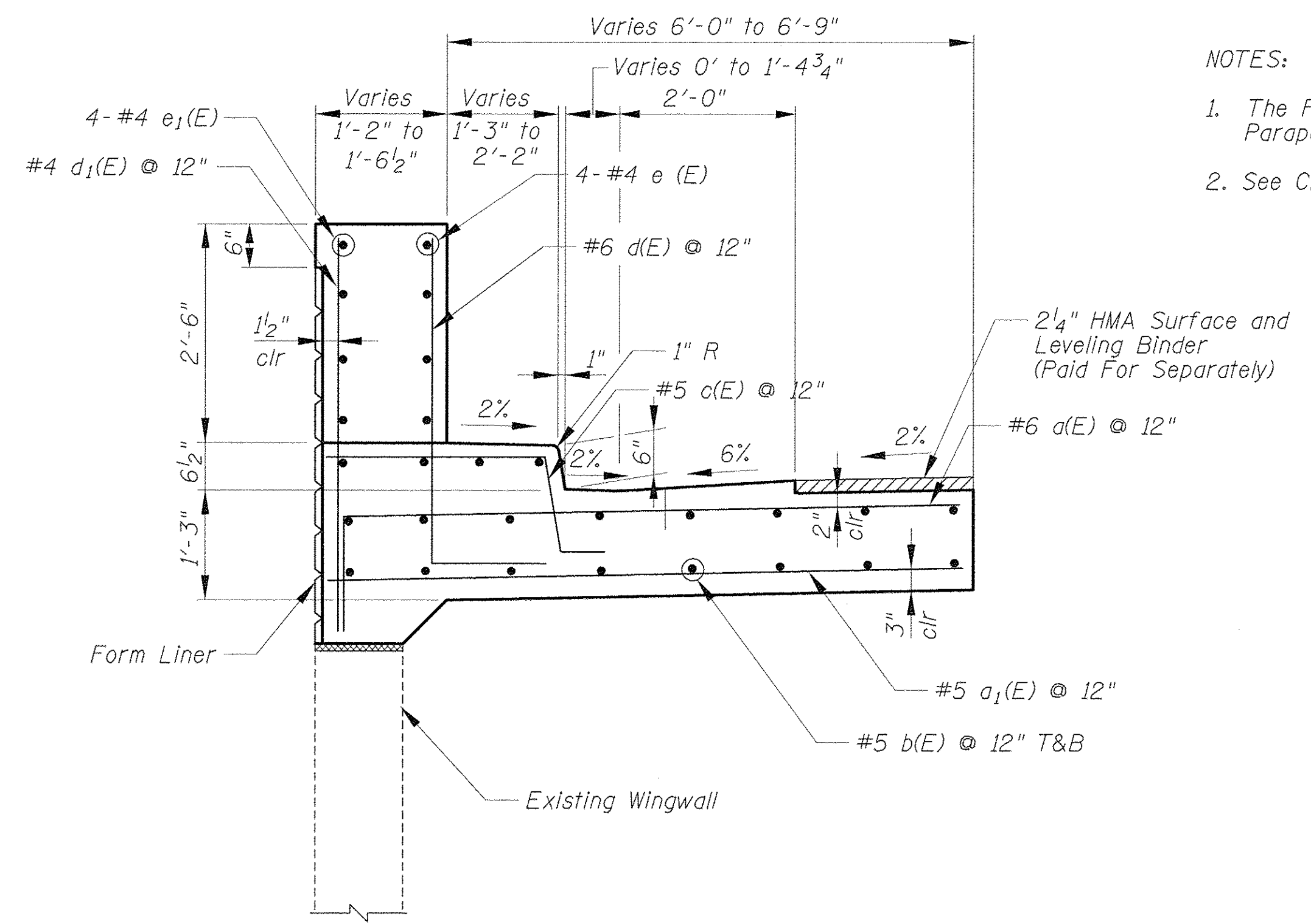
PLAN-MOMENT SLAB
(See Civil Plans For Location)

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	12	#6	8'-0"	
a ₁ (E)	12	#5	7'-2"	
b(E)	20	#5	10'-9"	
c(E)	12	#5	4'-11"	
d(E)	12	#6	4'-9"	
d ₁ (E)	10	#4	4'-9"	
e(E)	4	#4	10'-9"	
e ₁ (E)	4	#4	10'-9"	
Concrete Structures			Cu. Yd.	13.8
Reinforcement Bars, Epoxy Coated			Pound	690



PLAN SHOWING REINFORCEMENT



SECTION A-A

- NOTES:
1. The Form Liner Shall Match the Form Liner of the Adjacent Parapet and Column.
 2. See Civil Drawings for Top of Slab and Curb Elevations.

Testing Service Corporation

STRUCTURE BORING LOG

Page 1 of 1

Date Started 9/6/16

Date Completed 9/6/16

ROUTE 15-00040-00-CH DESCRIPTION Multi-Use Path Retaining Wall

SECT. 15-00040-00-CH STRUCT. NO. DRILLED BY TSC/L-85,713

COUNTY Will LOCATION West of Bridge S. 8SW TWP. 35N. RNG. 9E

Table with columns for Boring No., Station, Offset, Surface Elev., and soil descriptions with blow count data.

SPT. (N) = Sum of last two blow values in sample. (Qu) S=Shear, B=Bulge at 15% Strain, P=Penetration Test.

Testing Service Corporation

STRUCTURE BORING LOG

Page 1 of 1

Date Started 9/6/16

Date Completed 9/6/16

ROUTE 15-00040-00-CH DESCRIPTION Multi-Use Path Retaining Wall

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Table with columns for Boring No., Station, Offset, Surface Elev., and soil descriptions with blow count data.

SPT. (N) = Sum of last two blow values in sample. (Qu) S=Shear, B=Bulge at 15% Strain, P=Penetration Test.

Testing Service Corporation

STRUCTURE BORING LOG

Page 1 of 2

Date Started 9/6/16

Date Completed 9/6/16

ROUTE 15-00040-00-CH DESCRIPTION Pedestrian Bridge Over Spillway, U.S. Route 52

SECT. 15-00040-00-CH STRUCT. NO. DRILLED BY TSC/L-85,713

COUNTY Will LOCATION West Abutment S. 8SW TWP. 35N. RNG. 9E

Table with columns for Boring No., Station, Offset, Surface Elev., and soil descriptions with blow count data.

SPT. (N) = Sum of last two blow values in sample. (Qu) S=Shear, B=Bulge at 15% Strain, P=Penetration Test.

Testing Service Corporation

STRUCTURE BORING LOG

Page 2 of 2

Date Started 9/6/16

Date Completed 9/6/16

ROUTE 15-00040-00-CH DESCRIPTION Pedestrian Bridge Over Spillway, U.S. Route 52

SECT. 15-00040-00-CH STRUCT. NO. DRILLED BY TSC/L-85,713

COUNTY Will LOCATION West Abutment S. 8SW TWP. 35N. RNG. 9E

Table with columns for Boring No., Station, Offset, Surface Elev., and soil descriptions with blow count data.

SPT. (N) = Sum of last two blow values in sample. (Qu) S=Shear, B=Bulge at 15% Strain, P=Penetration Test.

Testing Service Corporation

STRUCTURE BORING LOG

Page 1 of 2

Date Started 9/9/16

Date Completed 9/9/16

ROUTE 15-00040-00-CH DESCRIPTION Pedestrian Bridge Over Spillway, U.S. Route 52

SECT. 15-00040-00-CH STRUCT. NO. DRILLED BY TSC/L-85,713

COUNTY Will LOCATION East Abutment S. 8SW TWP. 35N. RNG. 9E

Table with columns for Boring No., Station, Offset, Surface Elev., and soil descriptions with blow count data.

SPT. (N) = Sum of last two blow values in sample. (Qu) S=Shear, B=Bulge at 15% Strain, P=Penetration Test.

Testing Service Corporation

STRUCTURE BORING LOG

Page 2 of 2

Date Started 9/9/16

Date Completed 9/9/16

ROUTE 15-00040-00-CH DESCRIPTION Pedestrian Bridge Over Spillway, U.S. Route 52

SECT. 15-00040-00-CH STRUCT. NO. DRILLED BY TSC/L-85,713

COUNTY Will LOCATION East Abutment S. 8SW TWP. 35N. RNG. 9E

Table with columns for Boring No., Station, Offset, Surface Elev., and soil descriptions with blow count data.

SPT. (N) = Sum of last two blow values in sample. (Qu) S=Shear, B=Bulge at 15% Strain, P=Penetration Test.

Testing Service Corporation

STRUCTURE BORING LOG

Page 1 of 1

Date Started 9/9/16

Date Completed 9/9/16

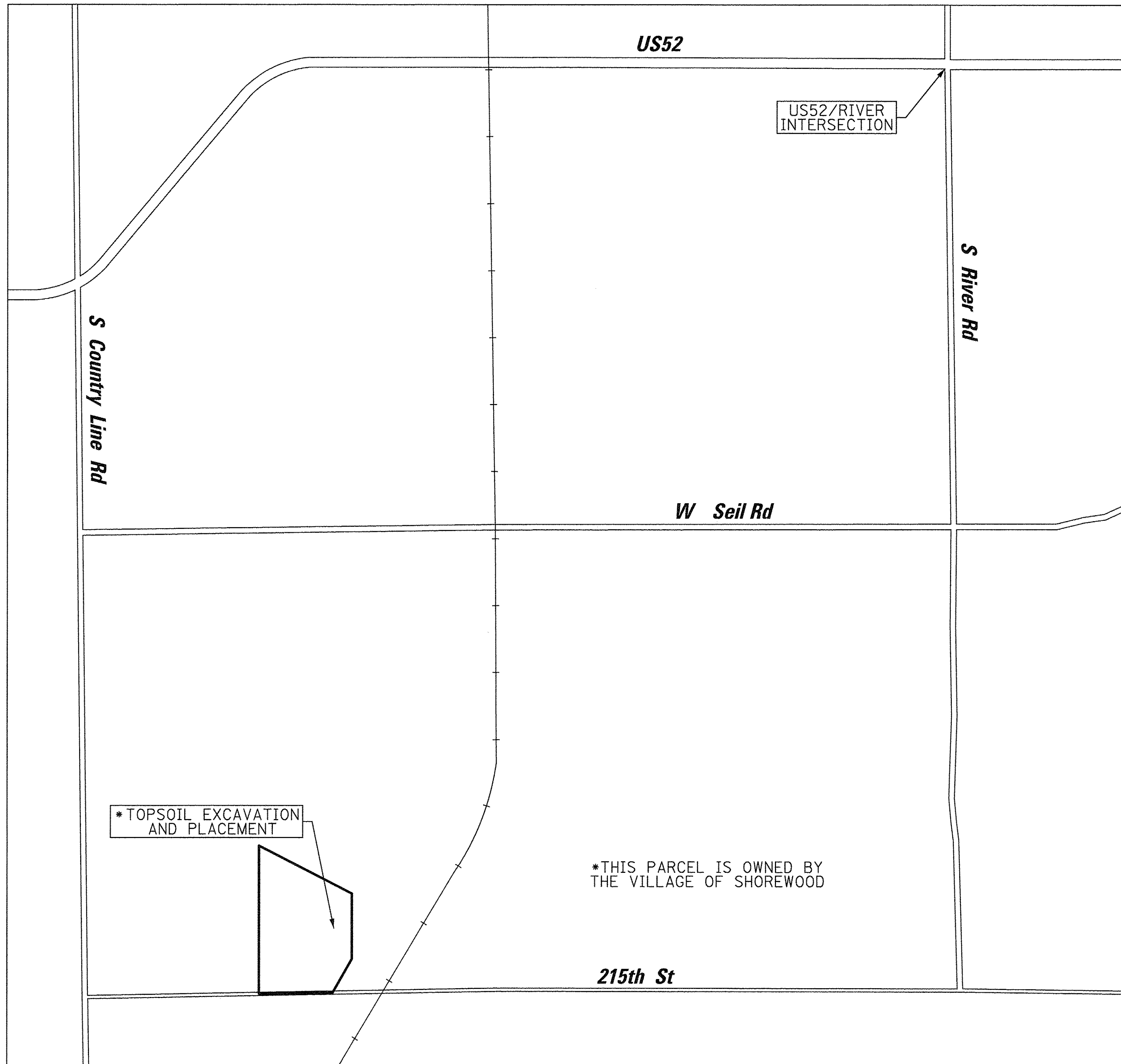
ROUTE 15-00040-00-CH DESCRIPTION Multi-Use Path Retaining Wall

SECT. 15-00040-00-CH STRUCT. NO. DRILLED BY TSC/L-85,713

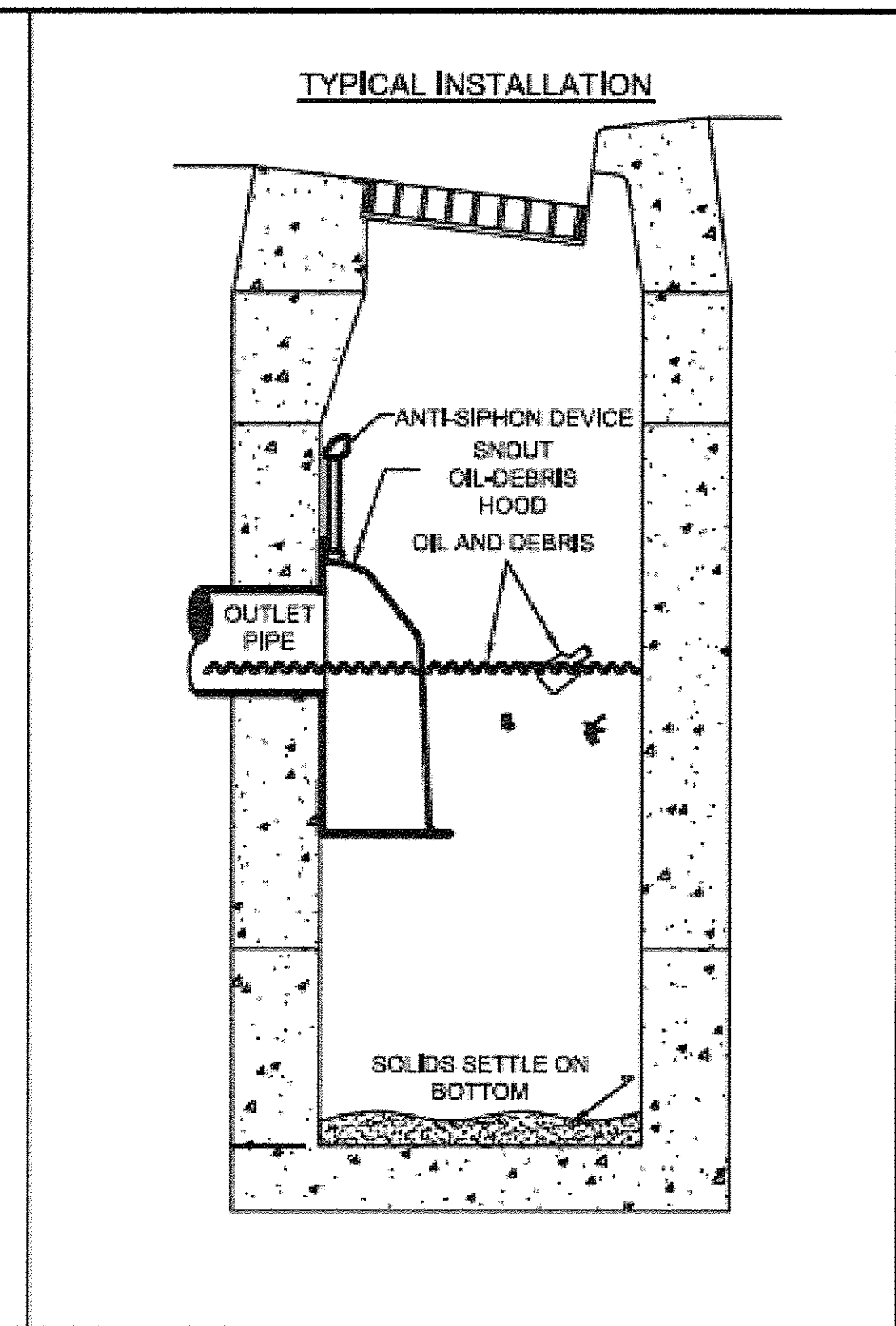
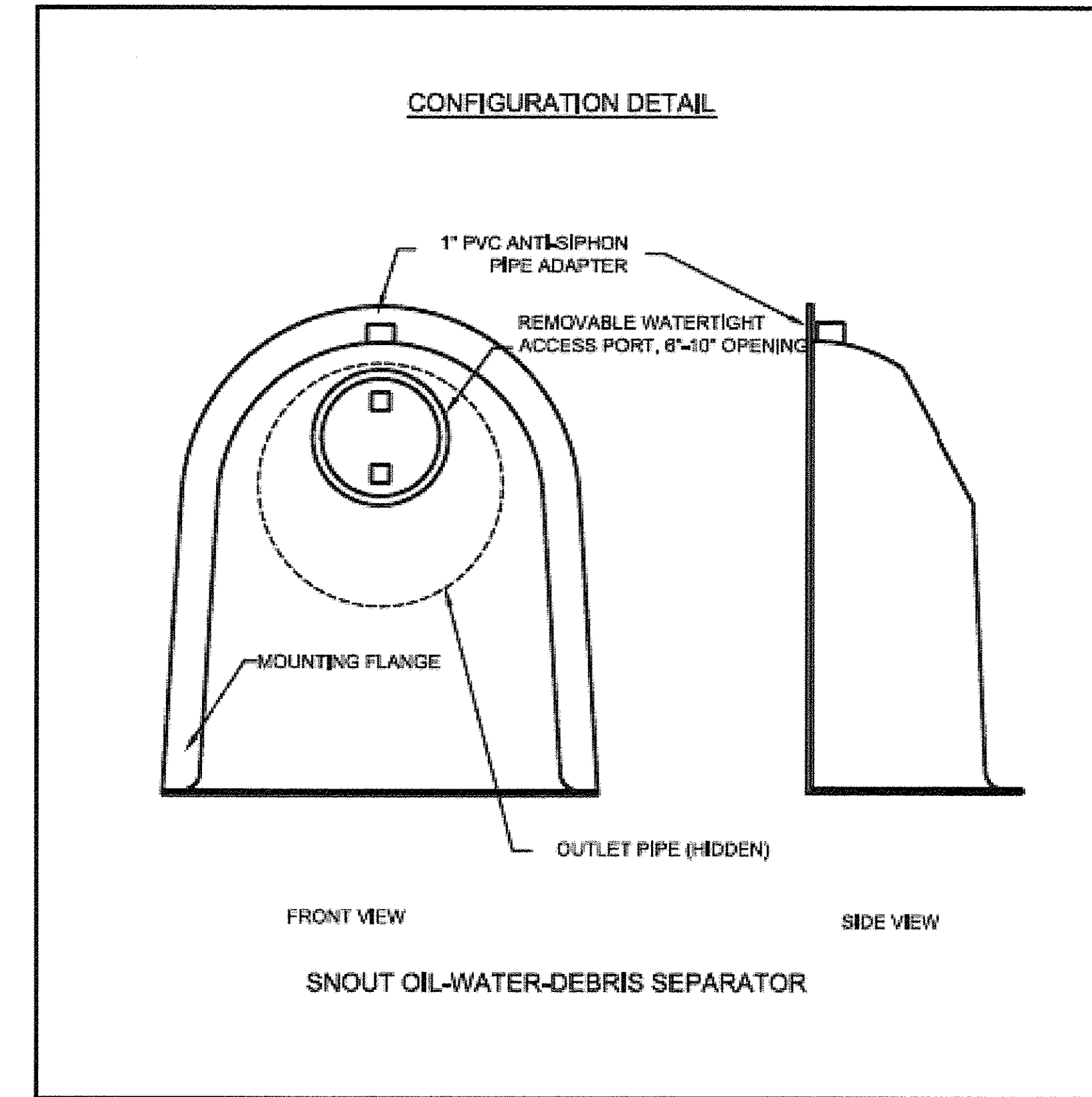
COUNTY Will LOCATION East of Bridge S. 8SW TWP. 35N. RNG. 9E

Table with columns for Boring No., Station, Offset, Surface Elev., and soil descriptions with blow count data.

SPT. (N) = Sum of last two blow values in sample. (Qu) S=Shear, B=Bulge at 15% Strain, P=Penetration Test.



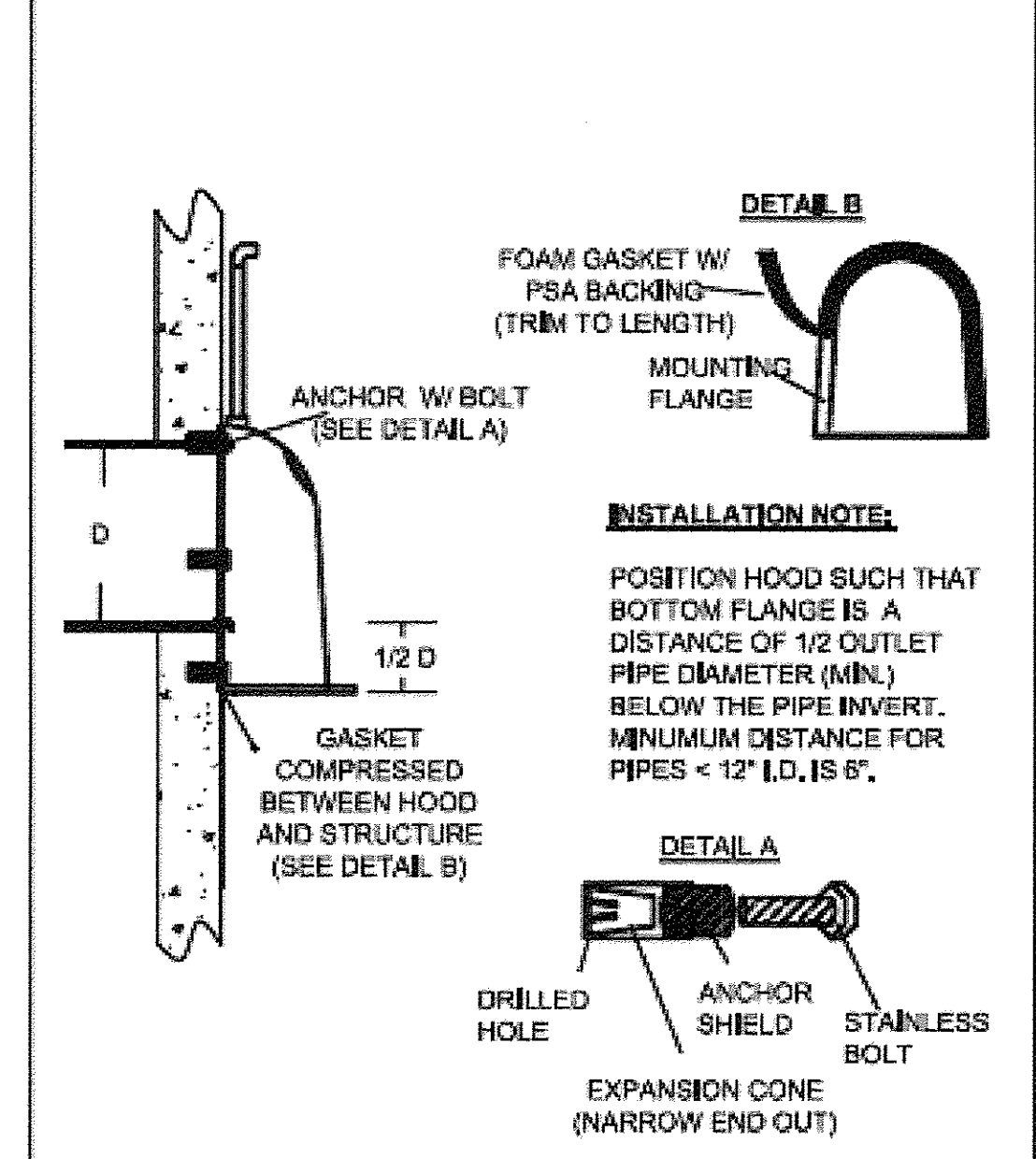
TOPSOIL EXCAVATION AND PLACEMENT, SPECIAL LOCATION MAP



NOTES:

1. ALL HOODS SHALL BE CONSTRUCTED OF A GLASS REINFORCED RESIN COMPOSITE WITH ISO GEL COAT EXTERIOR FINISH WITH A MINIMUM 0.125" LAMINATE THICKNESS.
2. ALL HOODS SHALL BE EQUIPPED WITH A WATERTIGHT ACCESS PORT, A MOUNTING FLANGE, AND AN ANTI-SIPHON VENT PIPE AND ELBOW AS DRAWN, (SEE CONFIGURATION DETAIL)
3. THE SIZE AND POSITION OF THE HOOD SHALL BE DETERMINED BY OUTLET PIPE SIZE AS PER MANUFACTURER'S RECOMMENDATION (SNOUT SIZE ALWAYS LARGER THAN PIPE SIZE).
4. THE BOTTOM OF THE HOOD SHALL EXTEND DOWNWARD A MINIMUM DISTANCE EQUAL TO 1/2 THE OUTLET PIPE DIAMETER WITH A MINIMUM DISTANCE OF 6" FOR PIPES <12" I.D.
5. THE ANTI-SIPHON VENT SHALL EXTEND ABOVE HOOD BY MINIMUM OF 3" AND A MAXIMUM OF 12" ACCORDING TO STRUCTURE CONFIGURATION.
6. THE SURFACE OF THE STRUCTURE WHERE THE HOOD IS MOUNTED SHALL BE FINISHED SMOOTH AND FREE OF LOOSE MATERIAL AND PIPE SHALL BE FINISHED FLUSH TO WALL.
7. THE HOOD SHALL BE SECURELY ATTACHED TO STRUCTURE WALL WITH 3/8" STAINLESS STEEL BOLTS AND OIL-RESISTANT GASKET AS SUPPLIED BY MANUFACTURER, (SEE INSTALLATION DETAIL)
8. INSTALLATION INSTRUCTIONS SHALL BE FURNISHED WITH MANUFACTURER SUPPLIED INSTALLATION KIT.
 INSTALLATION KIT SHALL INCLUDE:
 A. INSTALLATION INSTRUCTIONS
 B. PVC ANTI-SIPHON VENT PIPE AND ADAPTER
 C. OIL-RESISTANT CRUSHED CELL FOAM GASKET WITH PSA BACKING
 D. 3/8" STAINLESS STEEL BOLTS
 E. ANCHOR SHIELDS

INSTALLATION DETAIL



FILE NAME =	USER NAME = jstrick	DESIGNED -	REVISED -
Default	PLDT SCALE = 5'	DRAWN -	REVISED -
	PLDT DATE = 2/23/2017	CHECKED -	REVISED -
		DATE -	REVISED -

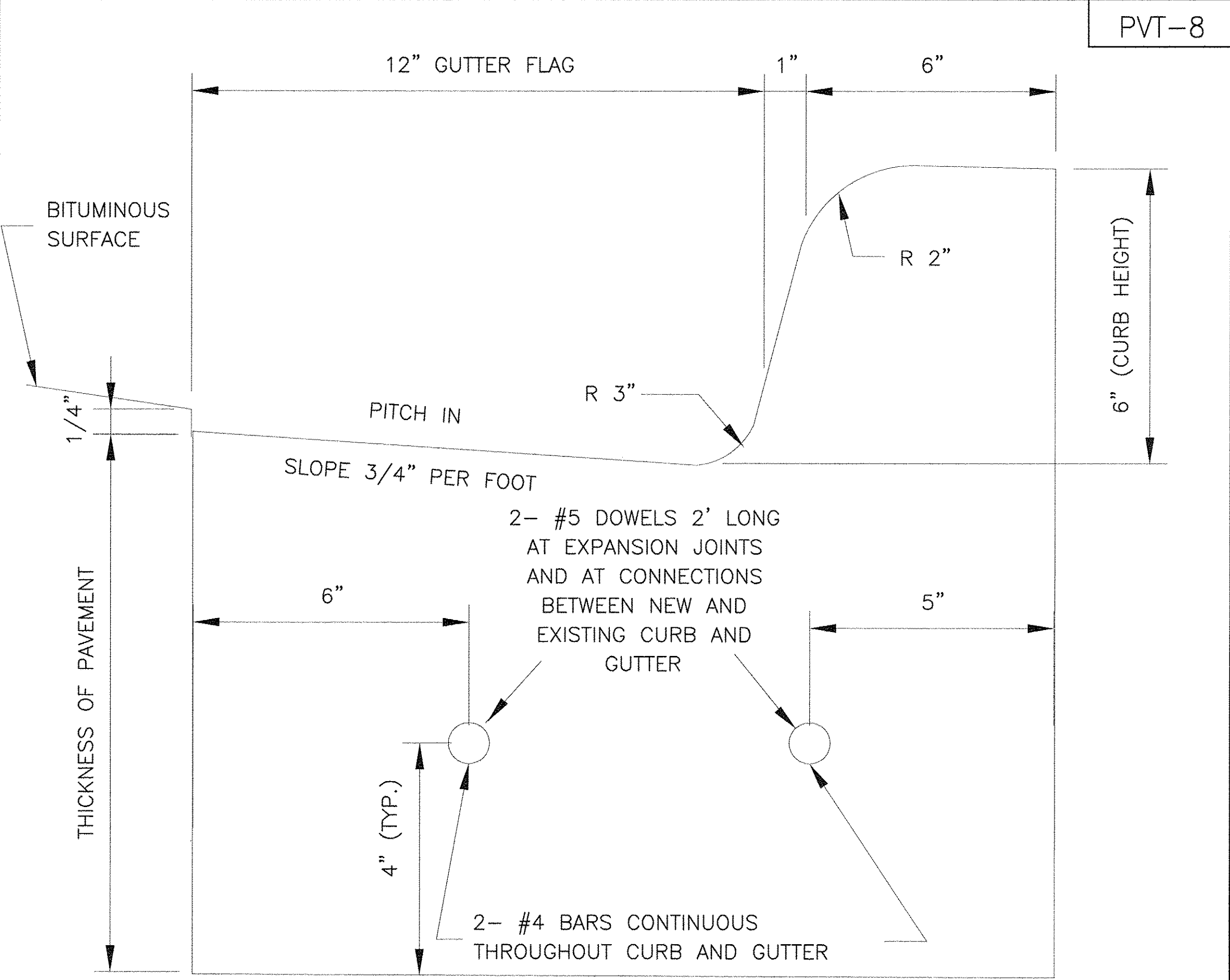
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**US ROUTE 52 RIVER ROAD
CONSTRUCTION DETAILS**

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
607	15-00040-00-CH	WILL	138	113
CONTRACT NO. 61D82				
ILLINOIS FED. AID PROJECT				

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NOT TO SCALE

NOTES:

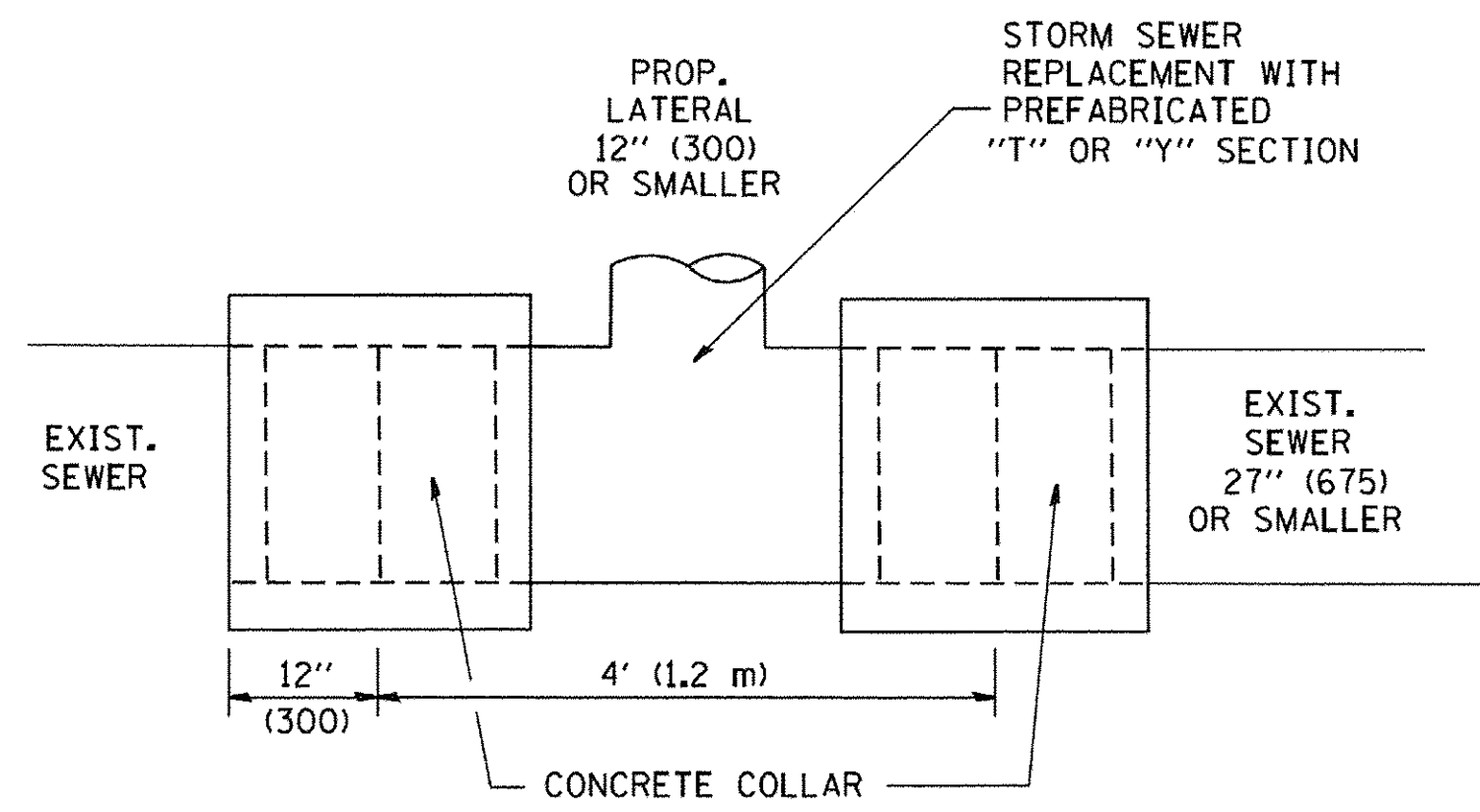
1. ALL CURB AND GUTTER SHALL BE CONSTRUCTED WITH IDOT CLASS SI CONCRETE WITH A MINIMUM COMPRESSIVE STRENGTH OF 4000 PSI AT 14 DAYS. PROVIDE AND TEST 3 CYLINDERS FOR EACH DAY'S POUR, OR 50 CUBIC YARDS, WHICHEVER IS LESS.
2. CONTRACTION JOINTS SHALL BE SAW-CUT AT 20' INTERVALS AND CAULKED.
3. PREFORMED EXPANSION JOINTS, 3/4" THICK, SHALL BE PLACED FIVE FEET EITHER SIDE OF STORM STRUCTURES IN CURB AND GUTTER, AT CURB RETURNS AND AT POINTS OF CURVATURE, AT ALL CONNECTIONS BETWEEN NEW AND EXISTING CURB AND GUTTER, AND AT 100' INTERVALS ON TANGENTS.
4. CURB AND GUTTER AT STORM STRUCTURES SHALL BE BOXED-OUT AND HAND-FORMED BETWEEN EXPANSION JOINTS. FORMS SHALL BE PLACED AND INSPECTED BY VILLAGE PRIOR TO POURING CONCRETE. STRUCTURE FRAMES SHALL BE PLACED AND ADJUSTED PRIOR TO THIS INSPECTION.
5. THE FOLLOWING SHALL BE STAMPED IN THE CURB AT THE INDICATED LOCATIONS:
 "W" FOR WATER SERVICES
 "S" FOR SANITARY SEWER SERVICES
 "ST" FOR STORM SEWER SERVICES
 "CO" FOR STORM SEWER SERVICE LINE CLEAN-OUTS
6. DOWELS AT EXPANSION JOINTS SHALL BE CENTERED ON THE JOINT (DRILLED INTO EXISTING CURB AND GUTTER), AND SHALL BE INSTALLED WITH GREASE CAPS ON ONE SIDE.

VILLAGE OF SHOREWOOD
 STANDARD DETAIL
 FOR
 CONCRETE CURB & GUTTER
 TYPE B-6.12

DATE: JUNE 2007

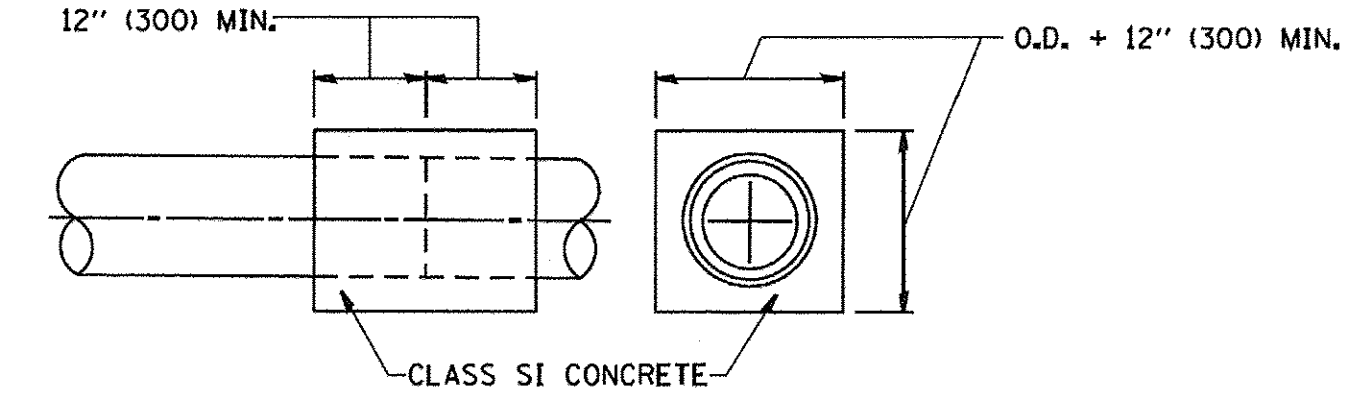
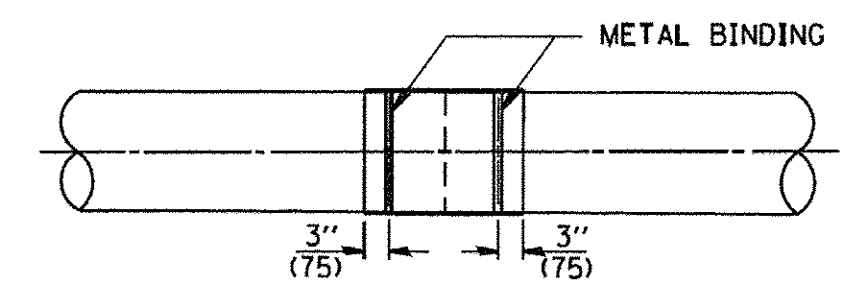
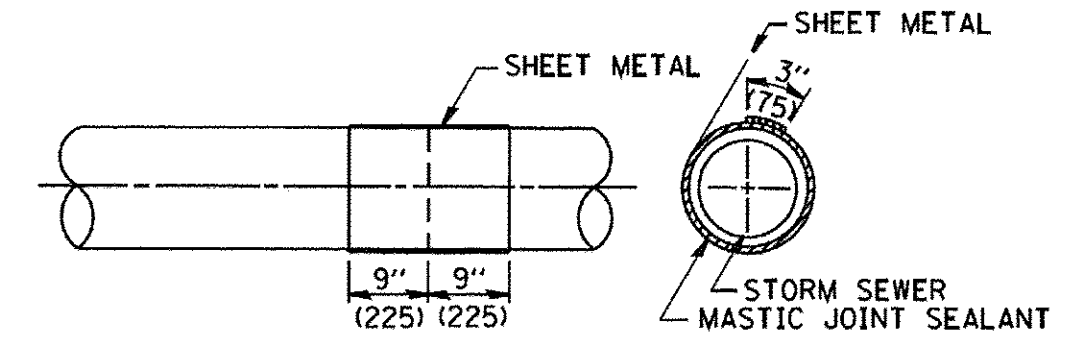
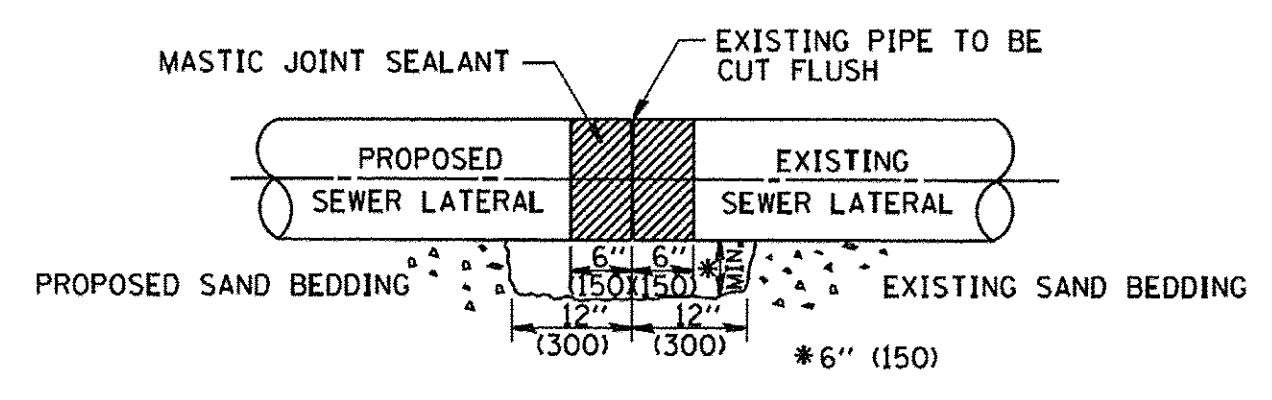
FILE NAME =	USER NAME = jstrick	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	US ROUTE 52 RIVER ROAD CONSTRUCTION DETAILS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default		DRAWN -	REVISED -		607	15-00040-00-CH	WILL	138	114			
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	PLDT DATE = 2/23/2017	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

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DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER

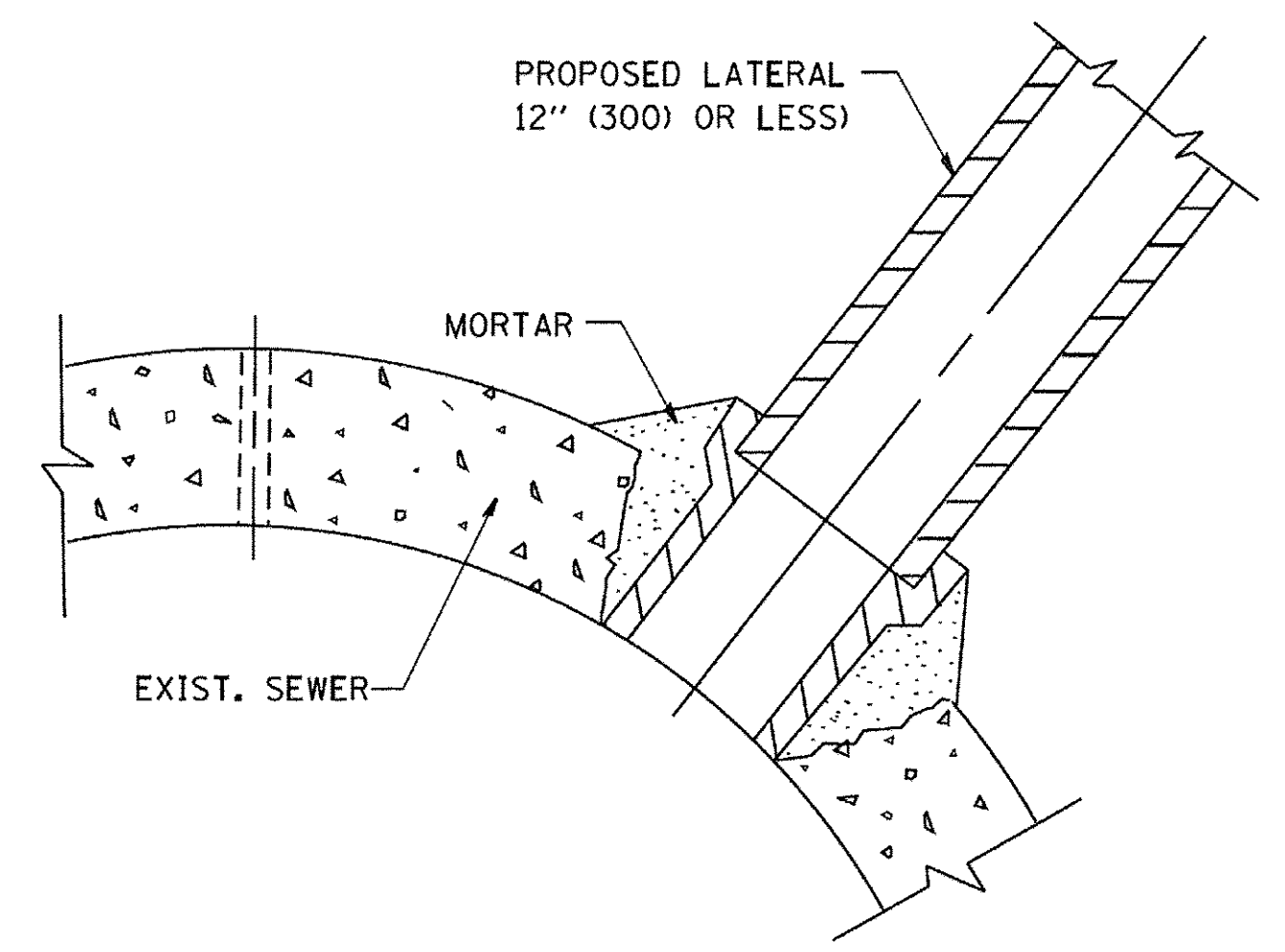


DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12' x 6" (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
5. WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
6. LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
8. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
9. PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- I. THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
 - II. CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - A) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - B) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".
- IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

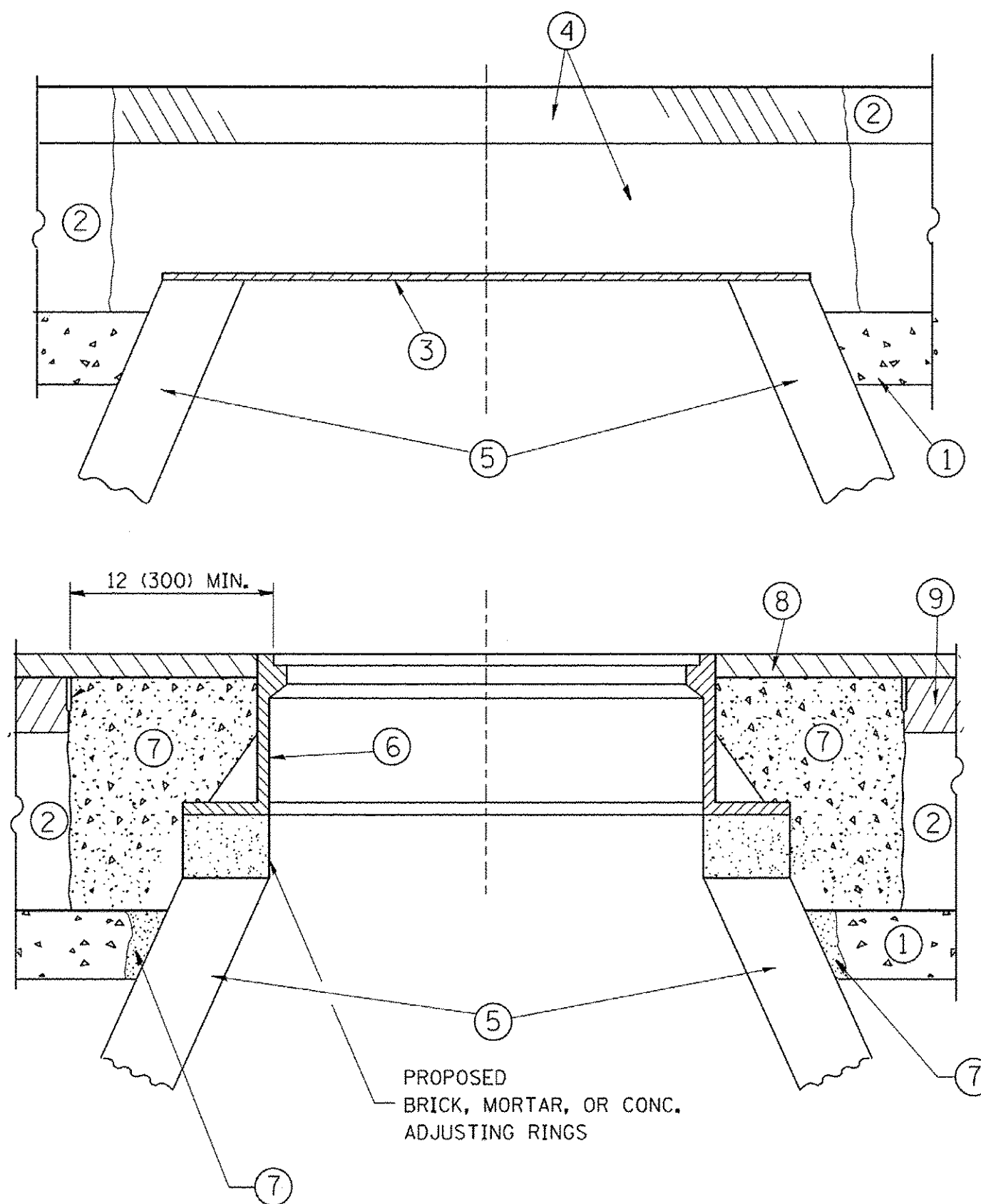
TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\bd07.dgn	USER NAME = geglennobt	DESIGNED - M. DE YONG	REVISED - M. DE YONG 05-08-92	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER			F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. SHAH 09-09-94		607	15-00040-00-CH	WILL	138	115			
		PLOT SCALE = 50,000' / IN.	REVISED - R. SHAH 10-25-94		BD500-01 (BD-7)			CONTRACT NO. 61D82				
		PLOT DATE = 1/4/2008	REVISED - R. SHAH 06-12-96		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

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CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- | | |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS PP-1* CONCRETE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

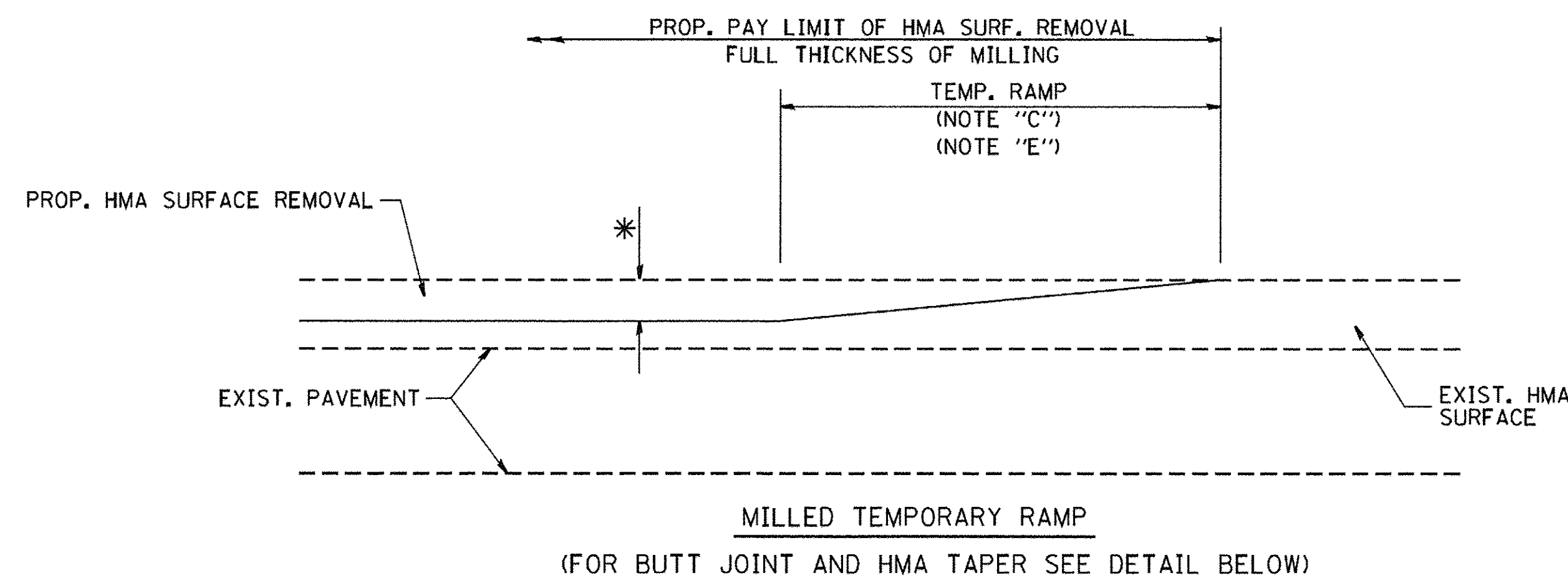
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

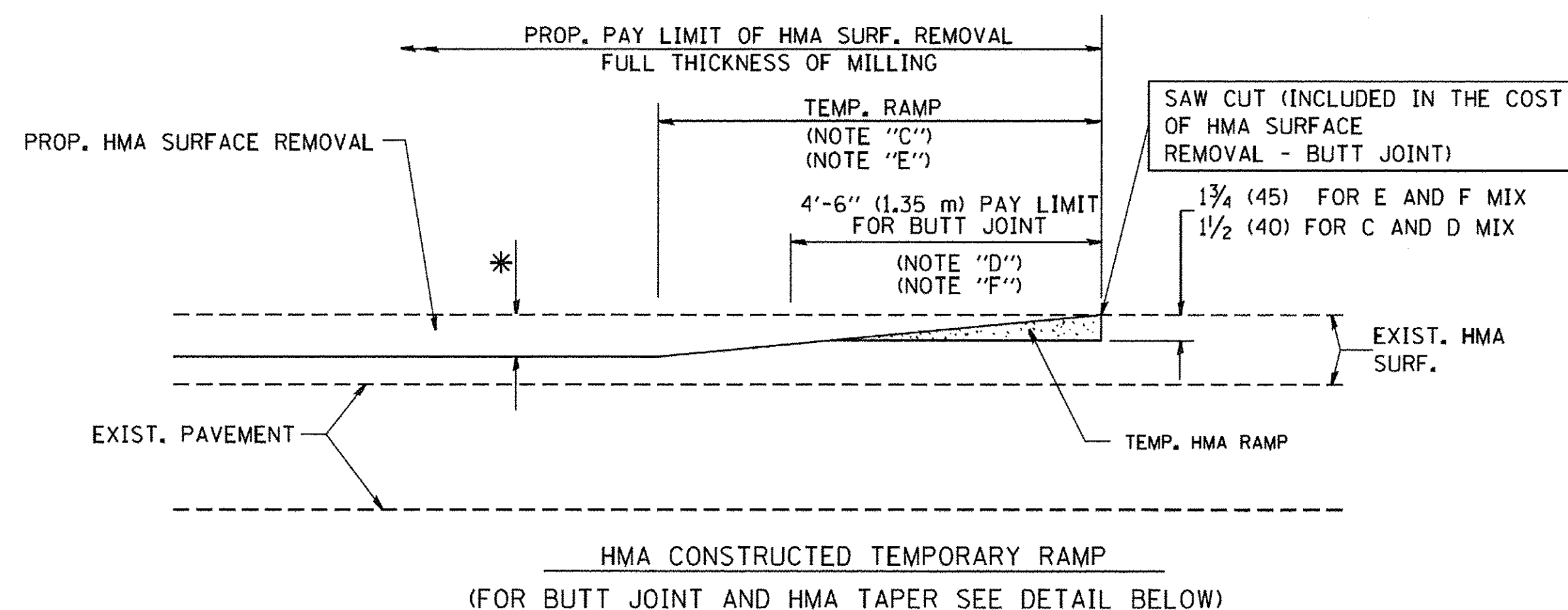
DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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	PLOT SCALE = 1/8" = 1'-0"	CHECKED -	REVISED - R. BORO 03-09-11					BD600-03 (BD-8)			CONTRACT NO. 61D82	
PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11	SCALE: NONE		SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

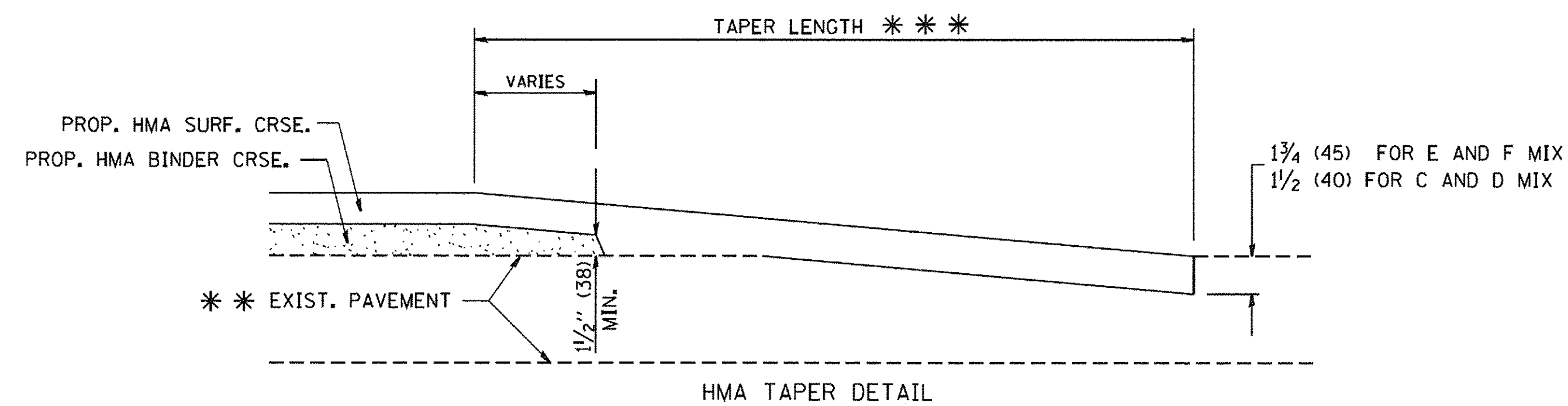
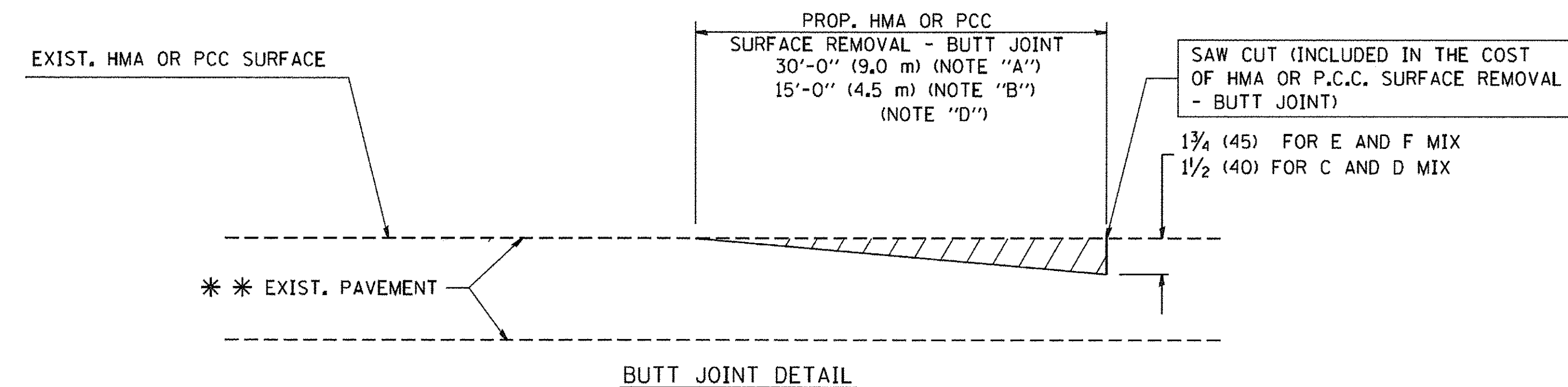


OPTION 1



OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

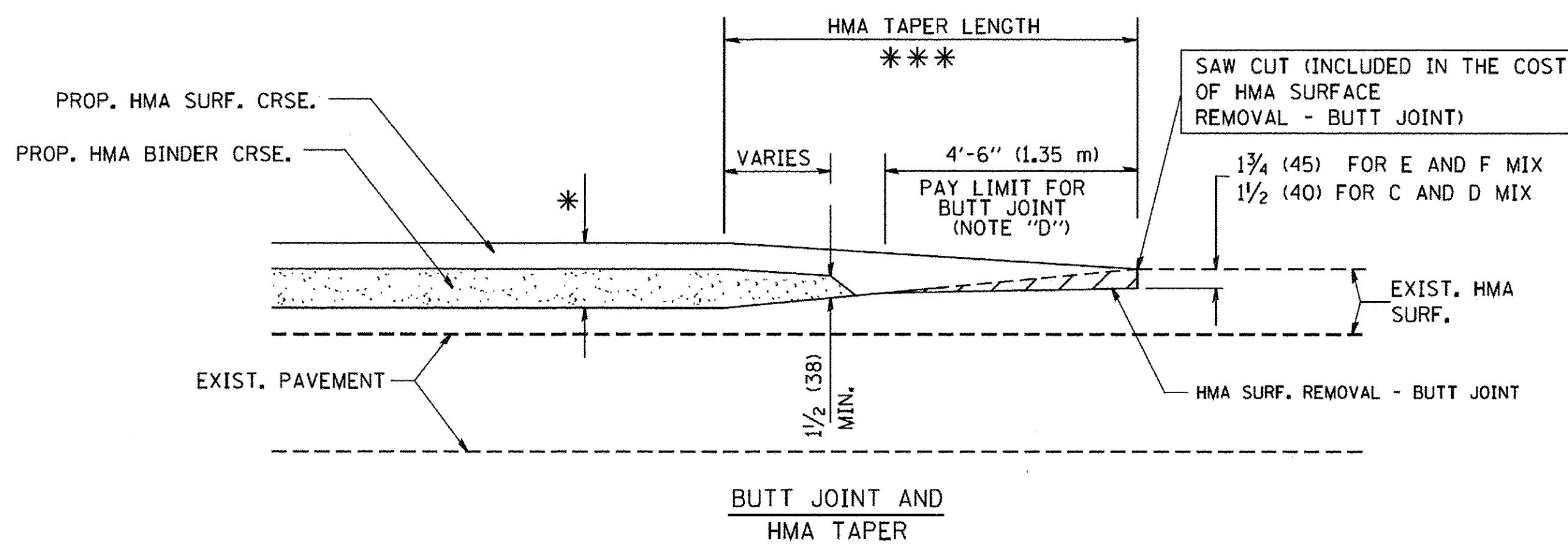
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

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PLOT DATE = 1/4/2008

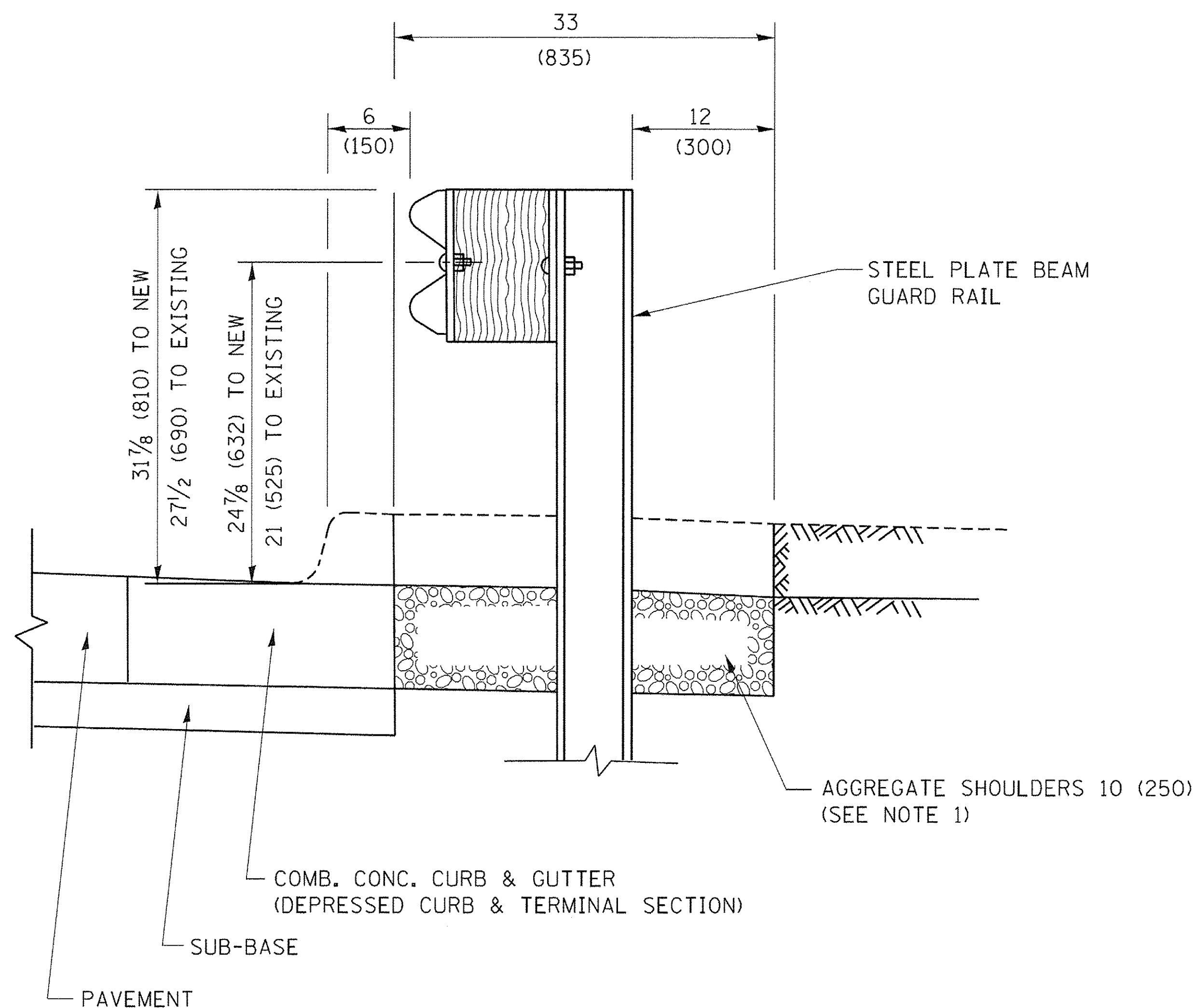
DESIGNED - M. DE YONG
DRAWN -
CHECKED -
DATE - 06-13-90

REVISED - R. SHAH 10-25-94
REVISED - A. ABBAS 03-21-97
REVISED - M. GOMEZ 04-06-01
REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BD400-05 BD32		CONTRACT NO.	61D82	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

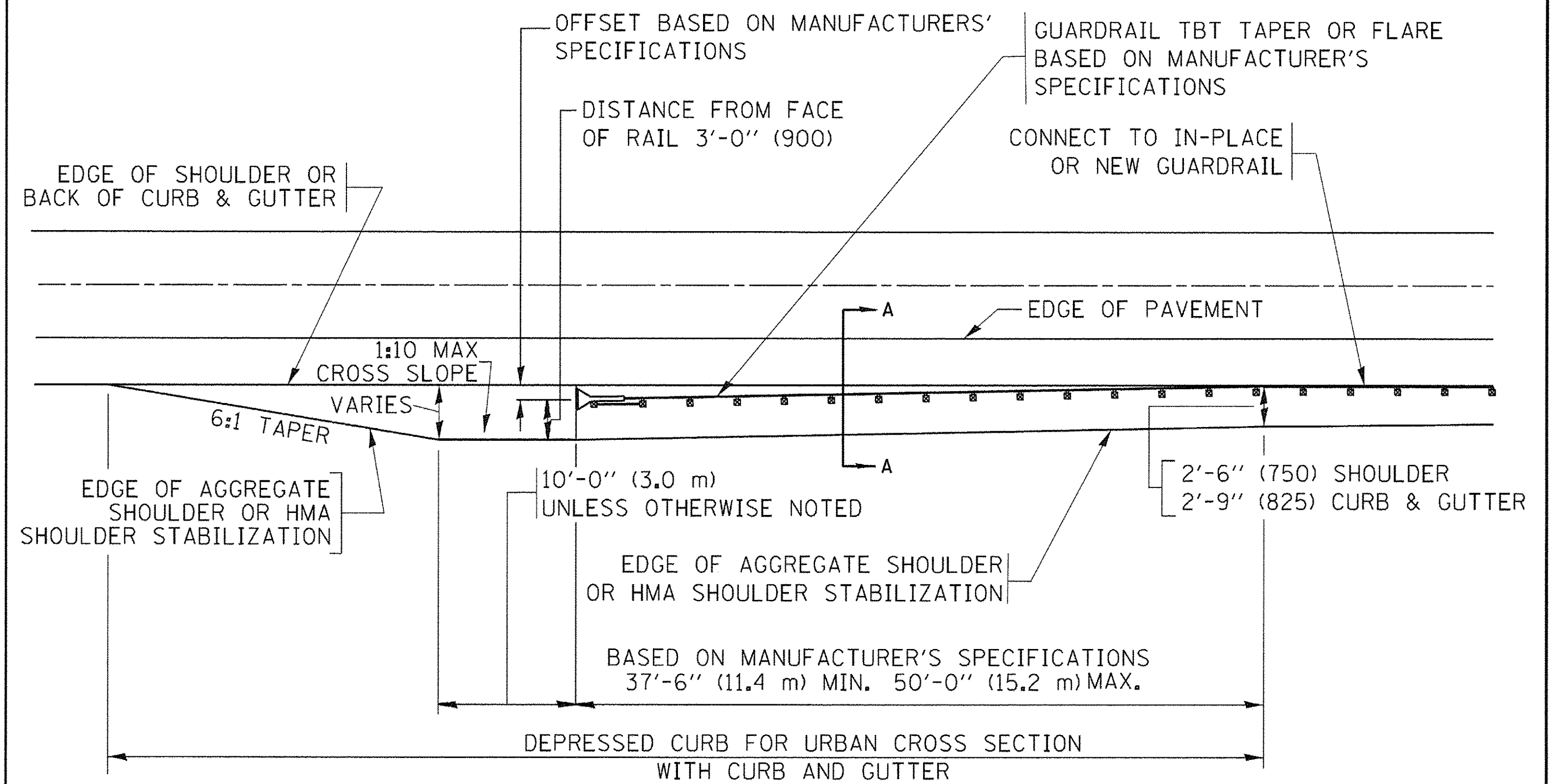


SECTION A-A

- NOTES:
1. THE AGGREGATE SHOULDER, 10 (250) OR HMA SHOULDER, 6 (150) (IF REQUIRED) SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL.
 2. "EXISTING" GUARDRAIL REFERS TO CONNECTING TERMINAL SECTION TO GUARD RAILING PRIOR TO THE MIDWEST GUARDRAIL SYSTEM.
 3. THE CONTRACTOR SHALL VERIFY THE TYPE/HEIGHT OF GUARDRAIL IN-PLACE BEFORE ORDERING THE NEW TERMINAL SECTION. COST INCLUDED WITH THE COST OF THE TERMINAL. THE TERMINAL SECTION HEIGHT TO BE PLACED MUST MATCH THE HEIGHT OF THE IN-PLACE GUARDRAIL.

DETAILS FOR STEEL PLATE BEAM
GUARD RAIL ADJACENT TO CURB AND GUTTER

[FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]



DEPRESSED CURB AND GUTTER AND
SHOULDER TREATMENT AT TBT TY. 1 SPL.

AGGREGATE SHOULDER, 10 (250) WILL BE PAID ACCORDING TO SECTION 481.

HMA SHOULDERS 6 (150) (IF REQUIRED) WILL BE PAID ACCORDING TO SECTION 482.

COMB. CONC. C&G, STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

TBT = TRAFFIC BARRIER TERMINAL
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

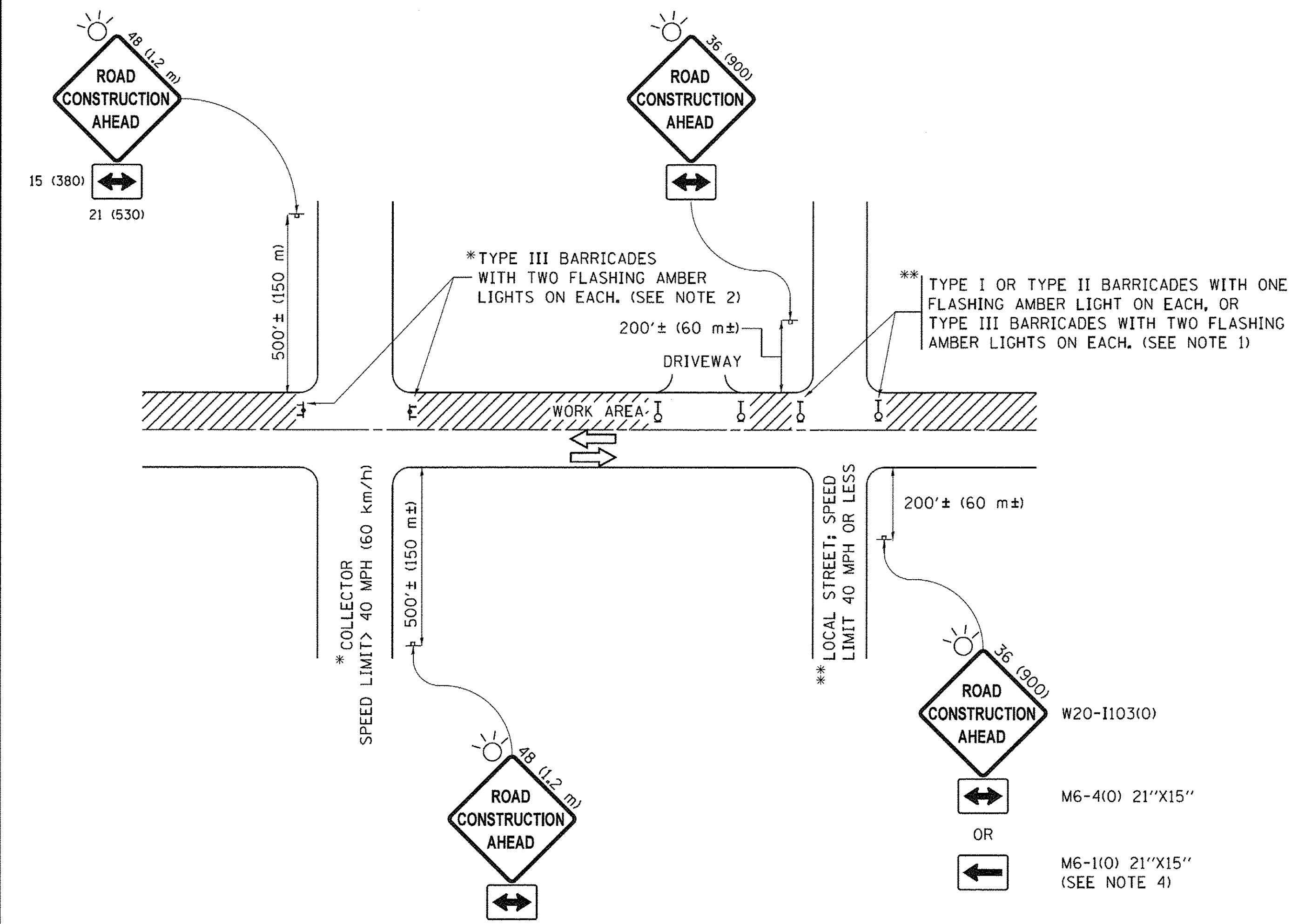
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Default	PLOT SCALE = 50.0000' / 1"	DATE - 09-22-90	REVISED - R. BORO 08-06-2012
	PLOT DATE = 12/21/2015		REVISED - R. BORO 05-08-2015

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR DEPRESSED CURB & GUTTER AND
SHOULDER TREATMENT AT TBT TY. 1 SPL.

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.J. RTE. 607	SECTION 15-00040-00-CH	COUNTY WILL	TOTAL SHEETS 138	SHEET NO. 118
BD600-10 (BD 34)		CONTRACT NO. 61D82		
ILLINOIS FED. AID PROJECT				

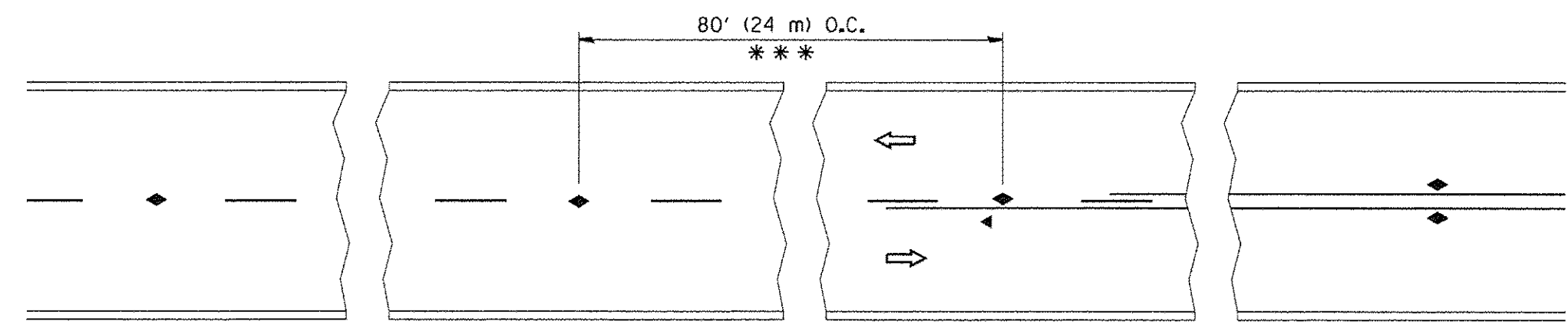


NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

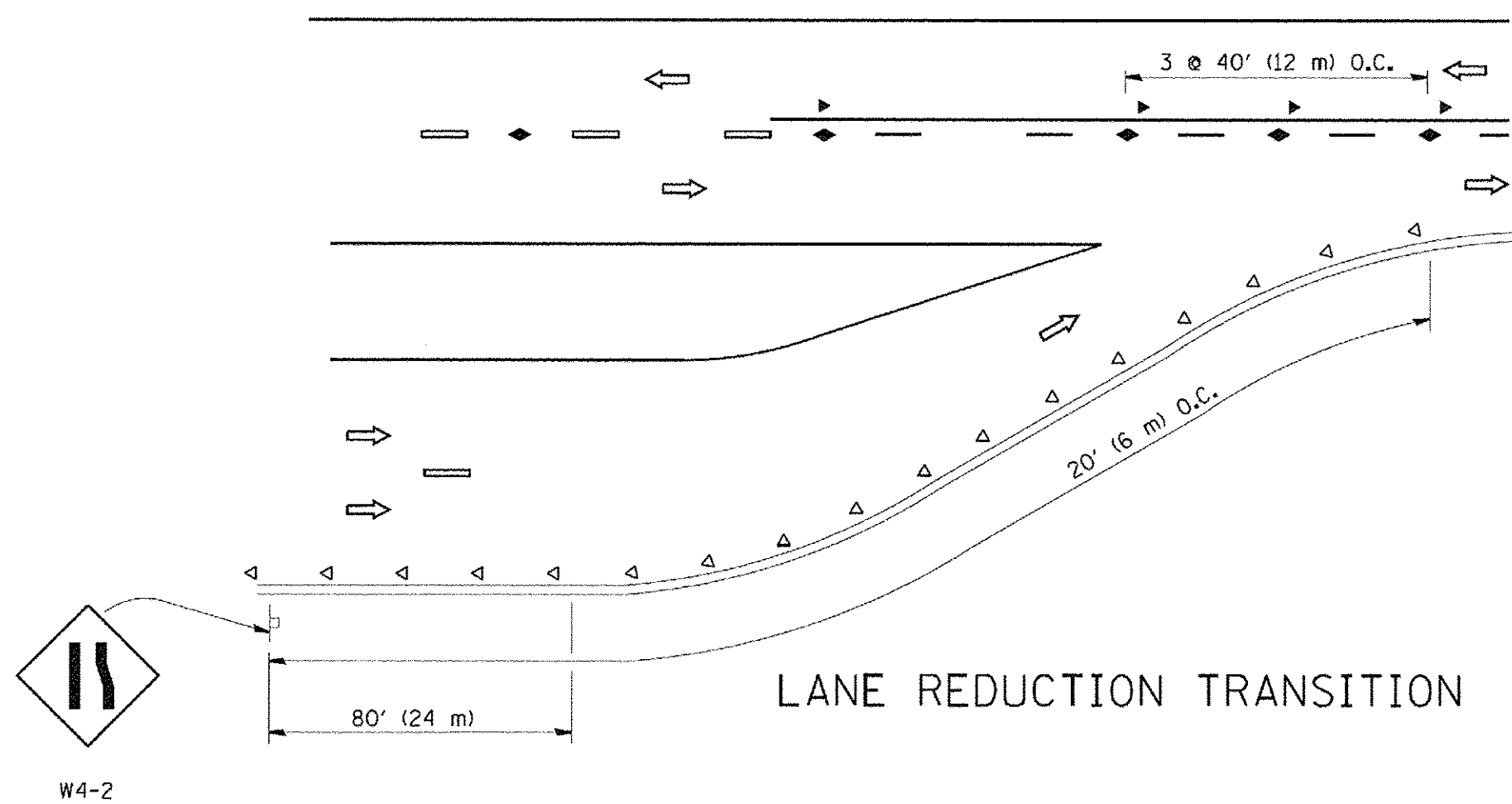
All dimensions are in inches (millimeters) unless otherwise shown.

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		DATE - 06-89	REVISED - A. SCHUETZE 07-01-13			TC-10		CONTRACT NO. 61D82			
			REVISED - A. SCHUETZE 09-15-16			SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT		

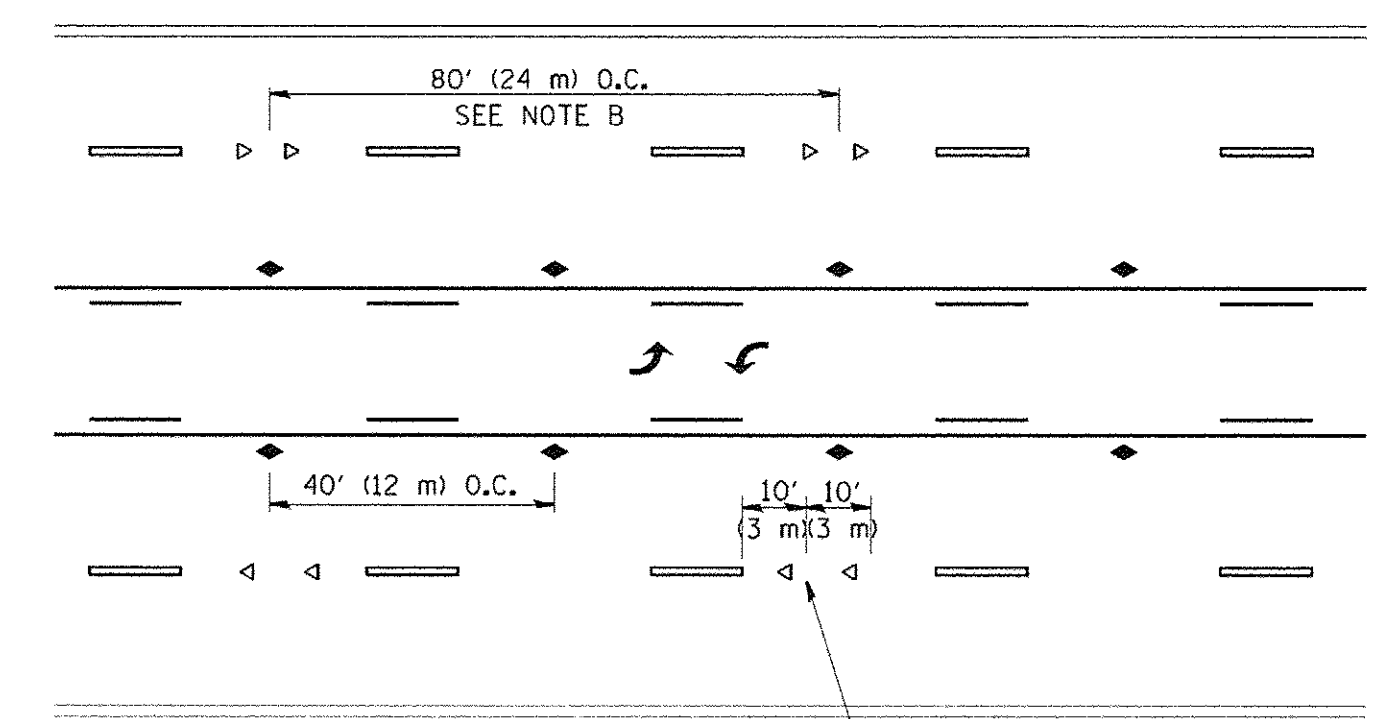


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

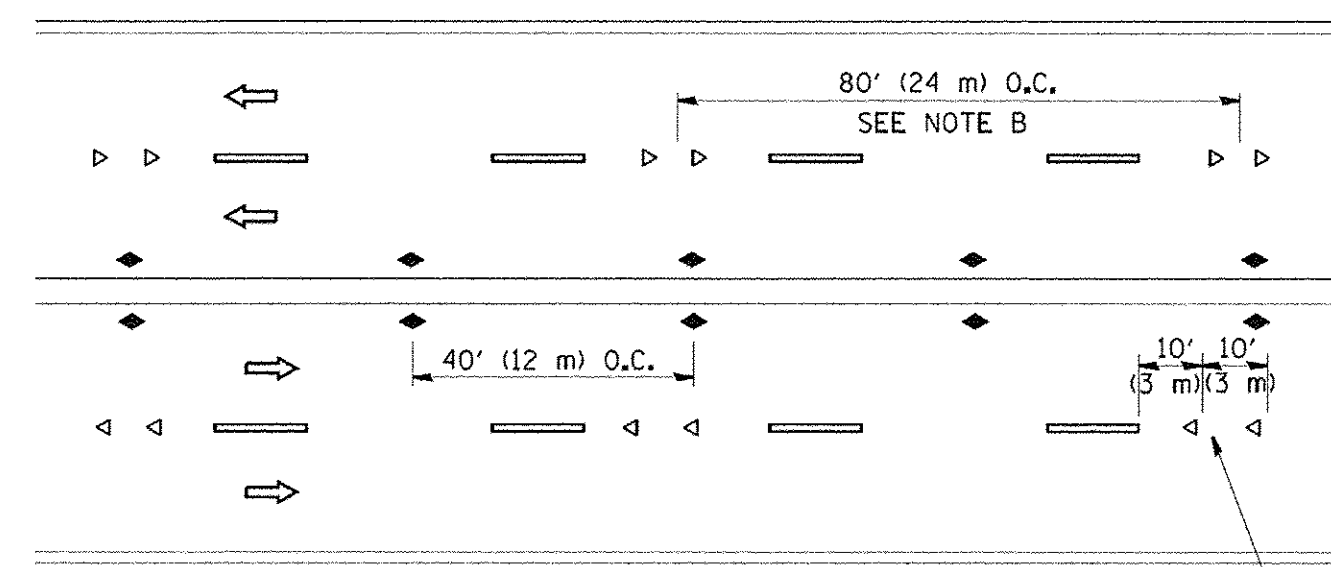
TWO-LANE/TWO-WAY



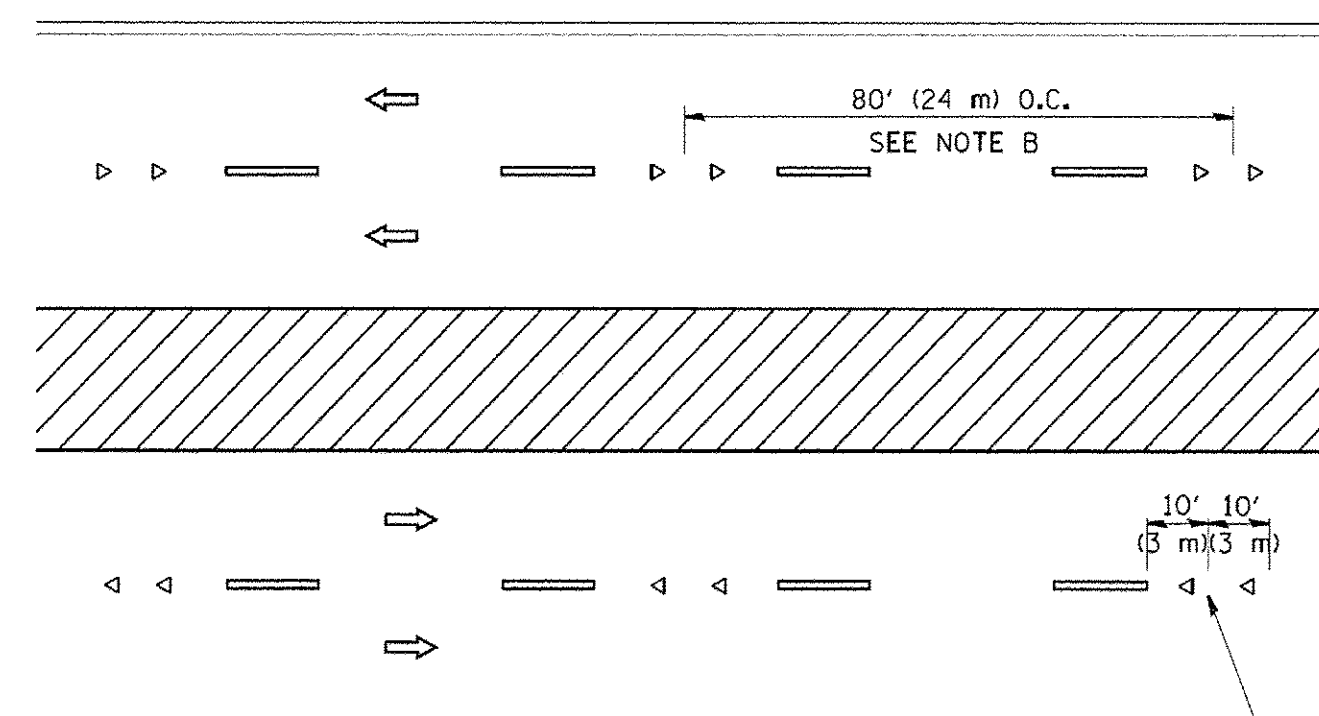
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

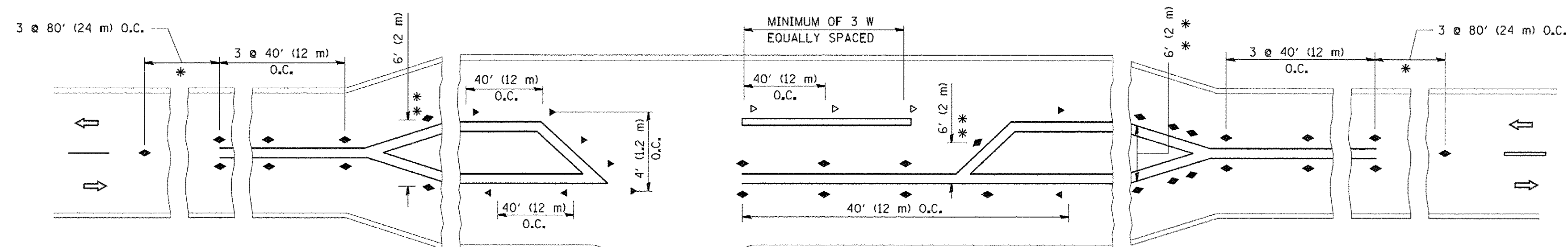
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

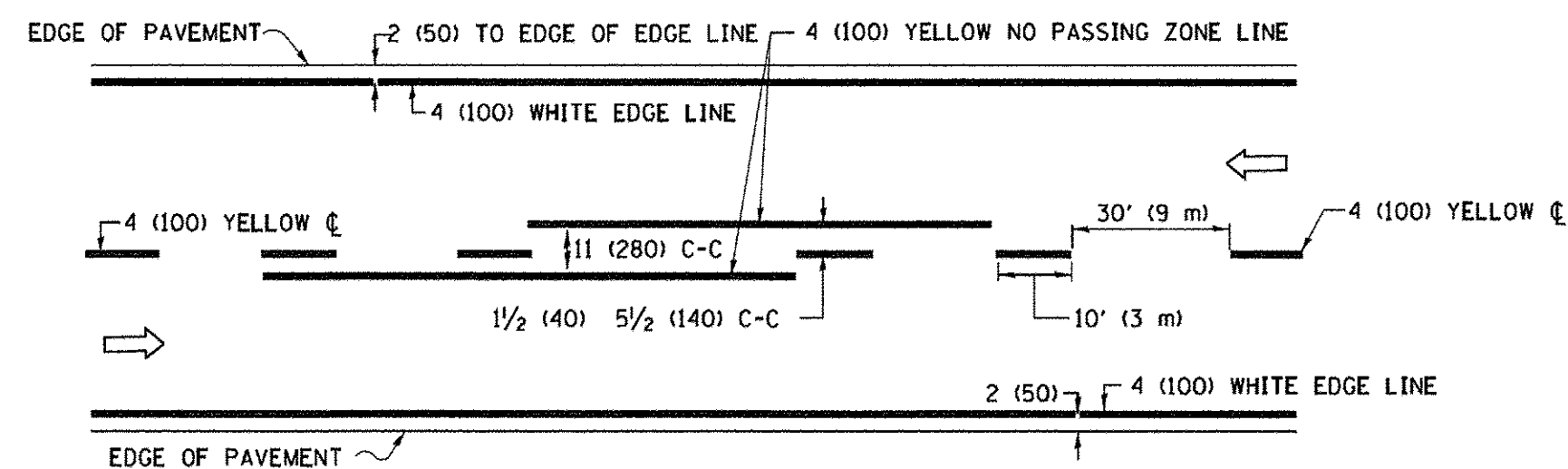


LEFT TURN

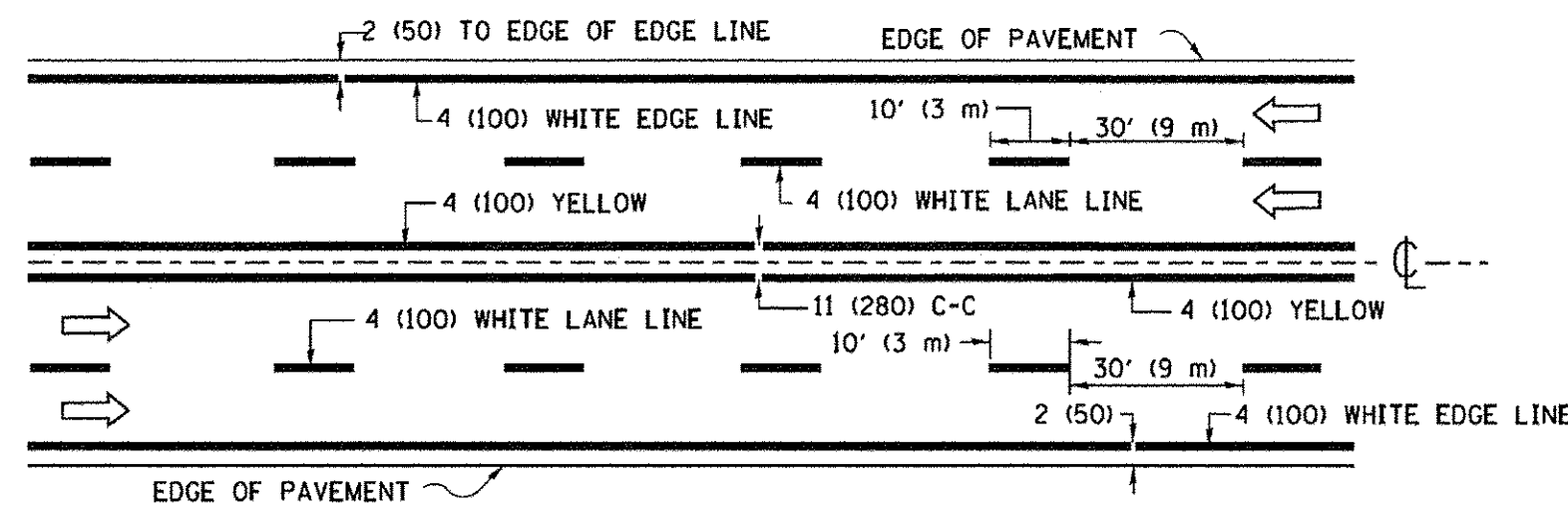
* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

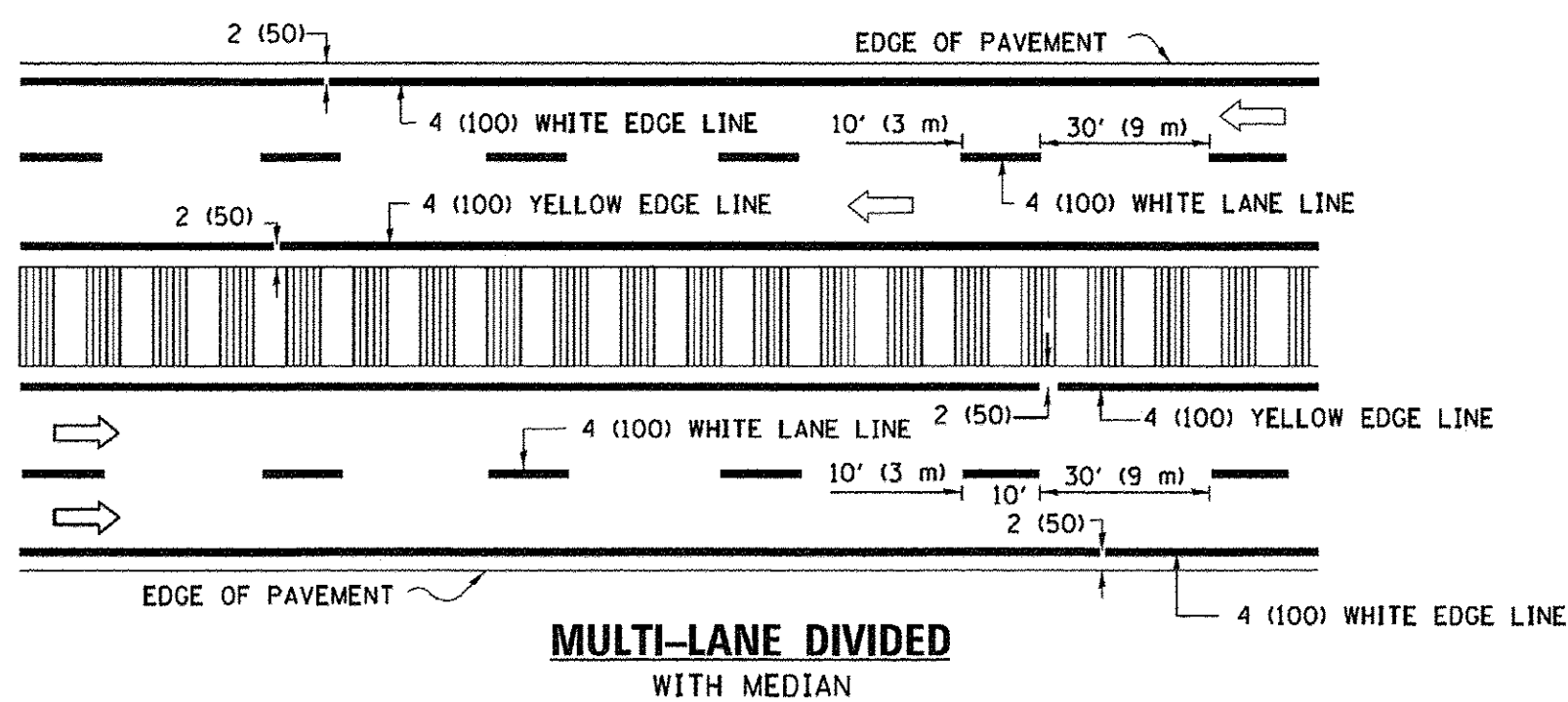
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		DATE -	REVISED - C. JUCIUS 09-09-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



2-LANE ROADWAY

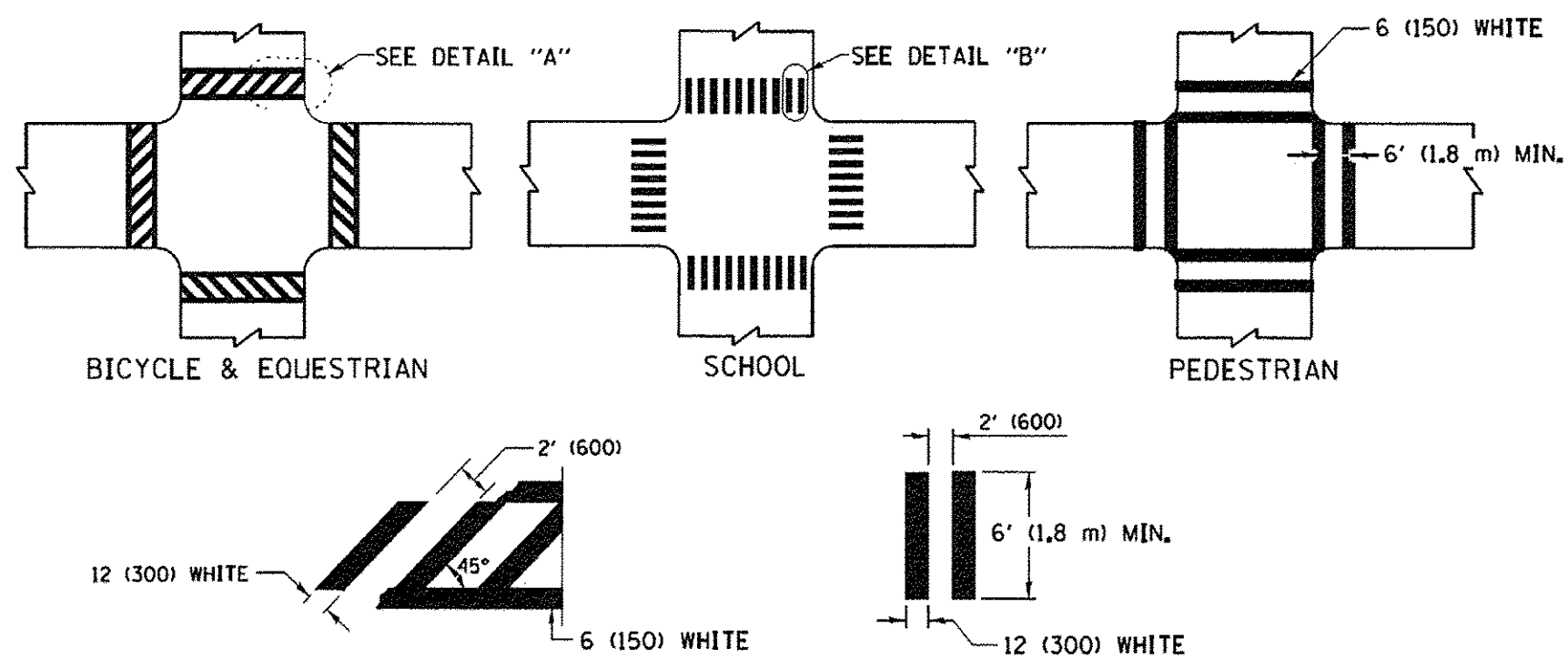


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

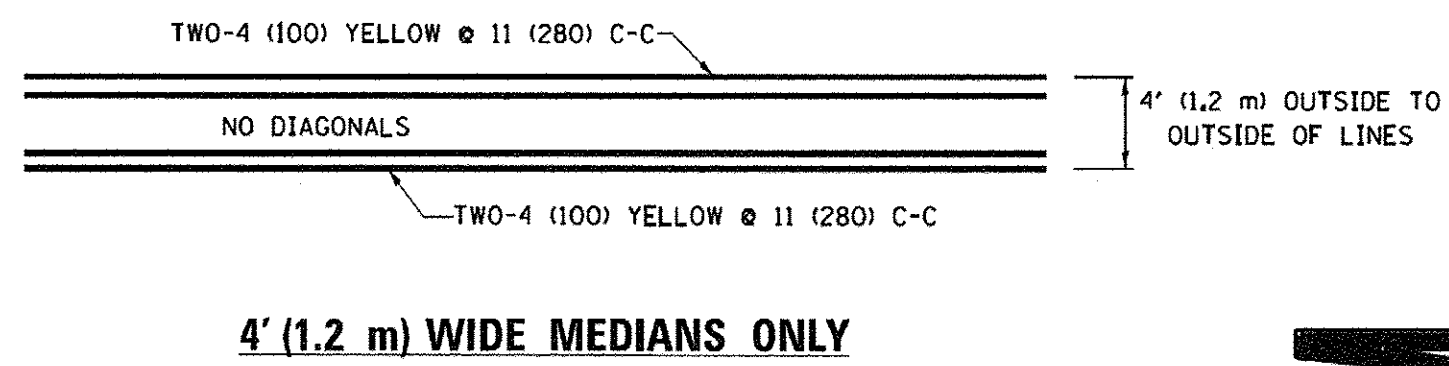


DETAIL "A"

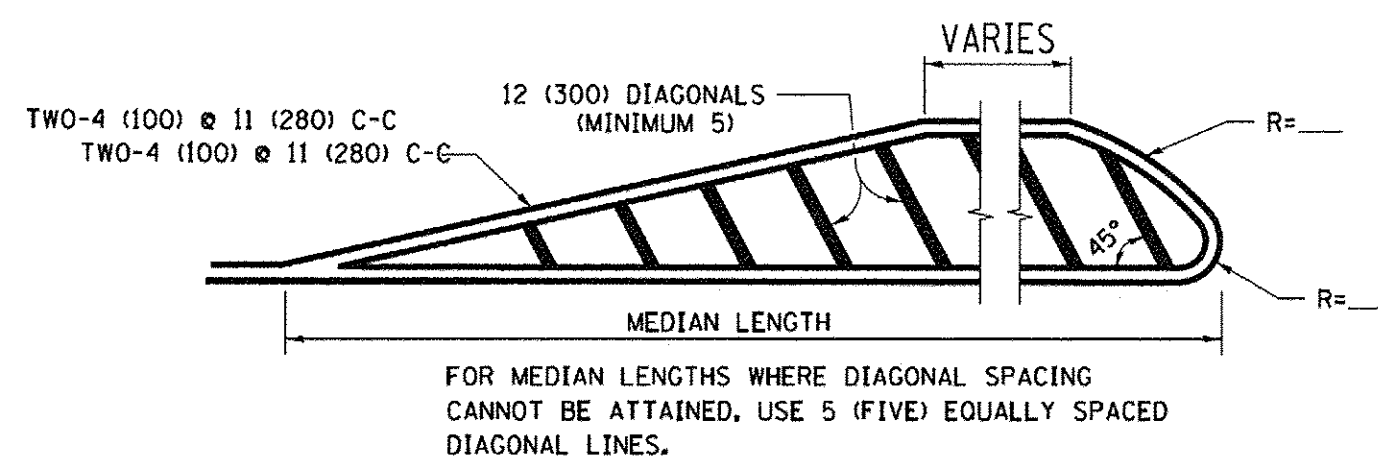
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

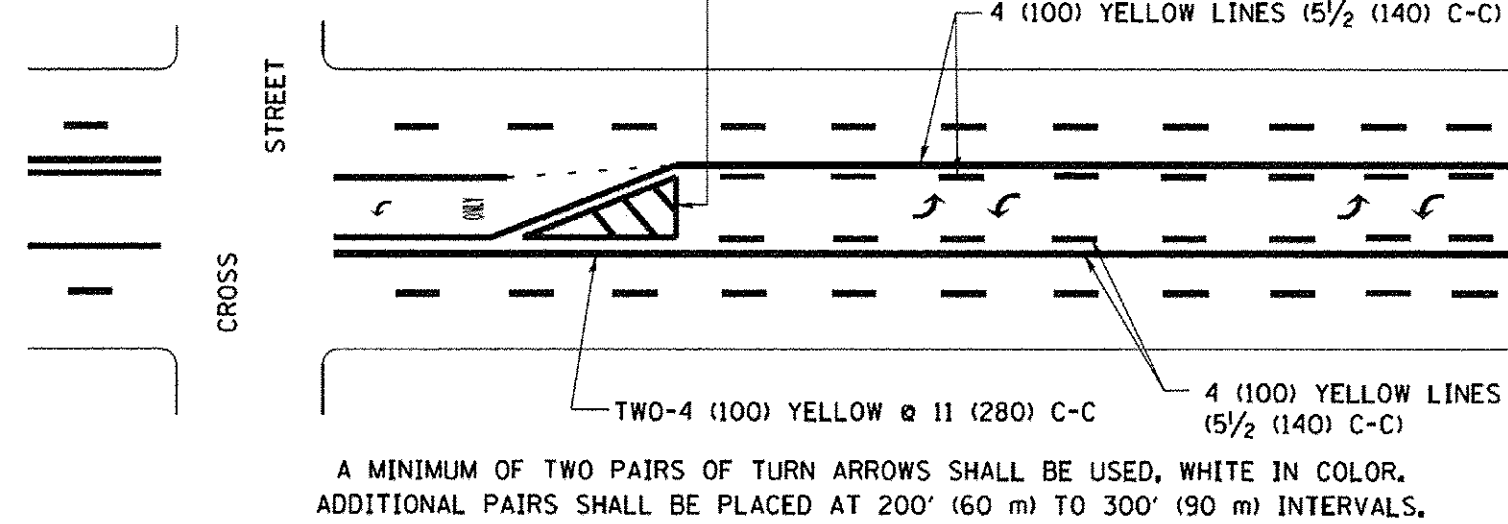


4' (1.2 m) WIDE MEDIANS ONLY

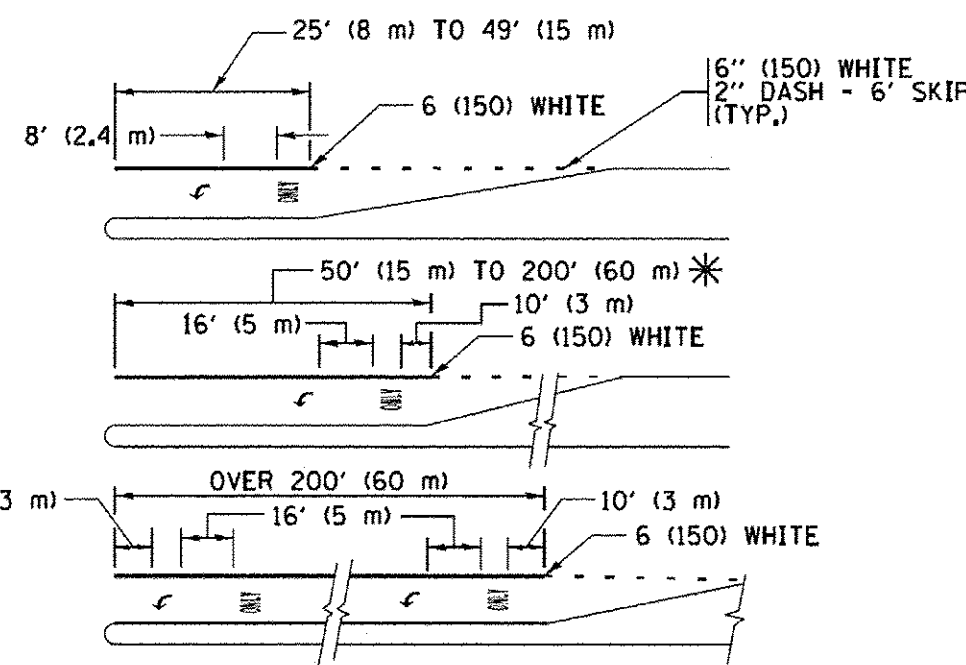


MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



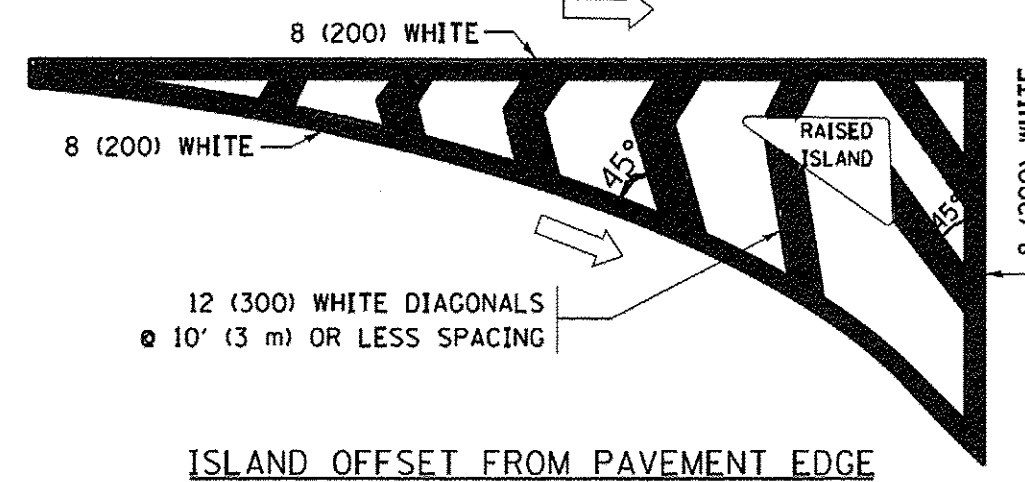
MEDIAN WITH TWO-WAY LEFT TURN LANE
TYPICAL PAINTED MEDIAN MARKING



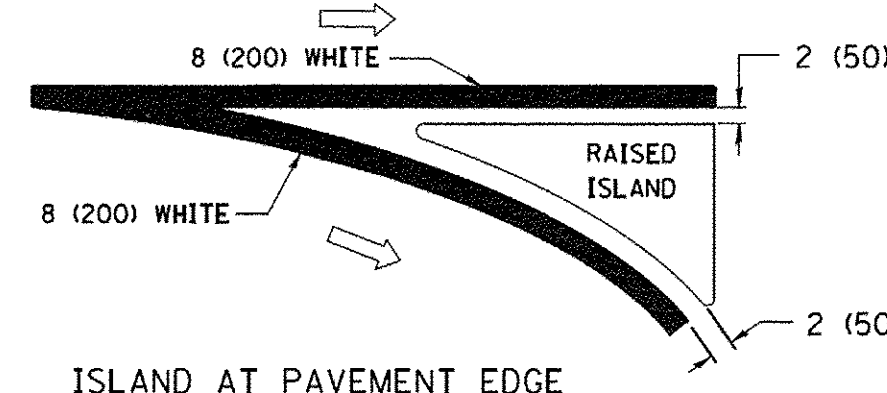
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

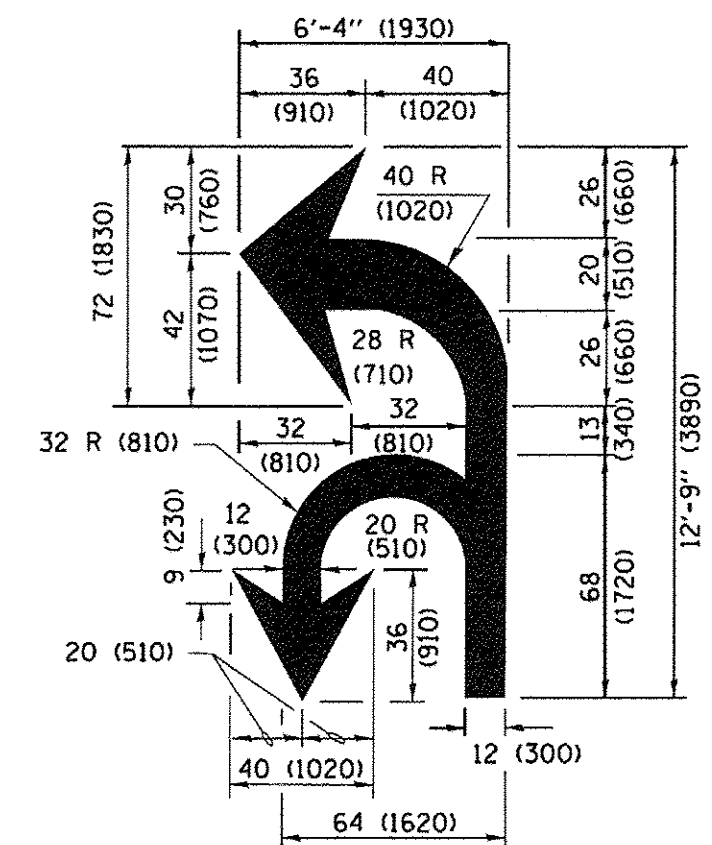


ISLAND OFFSET FROM PAVEMENT EDGE

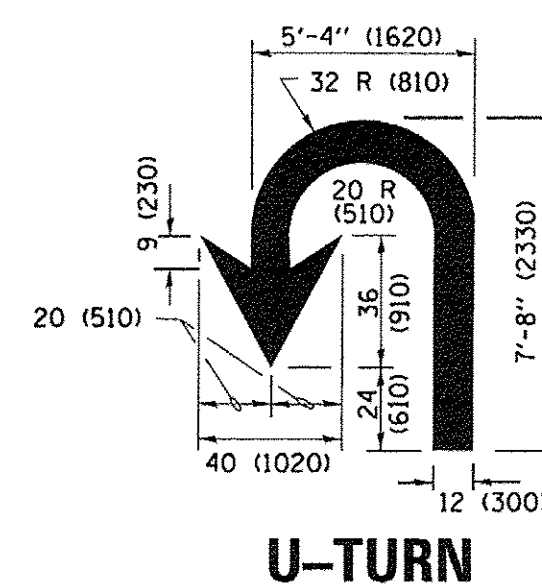


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH AND SOLID IN PAIRS SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

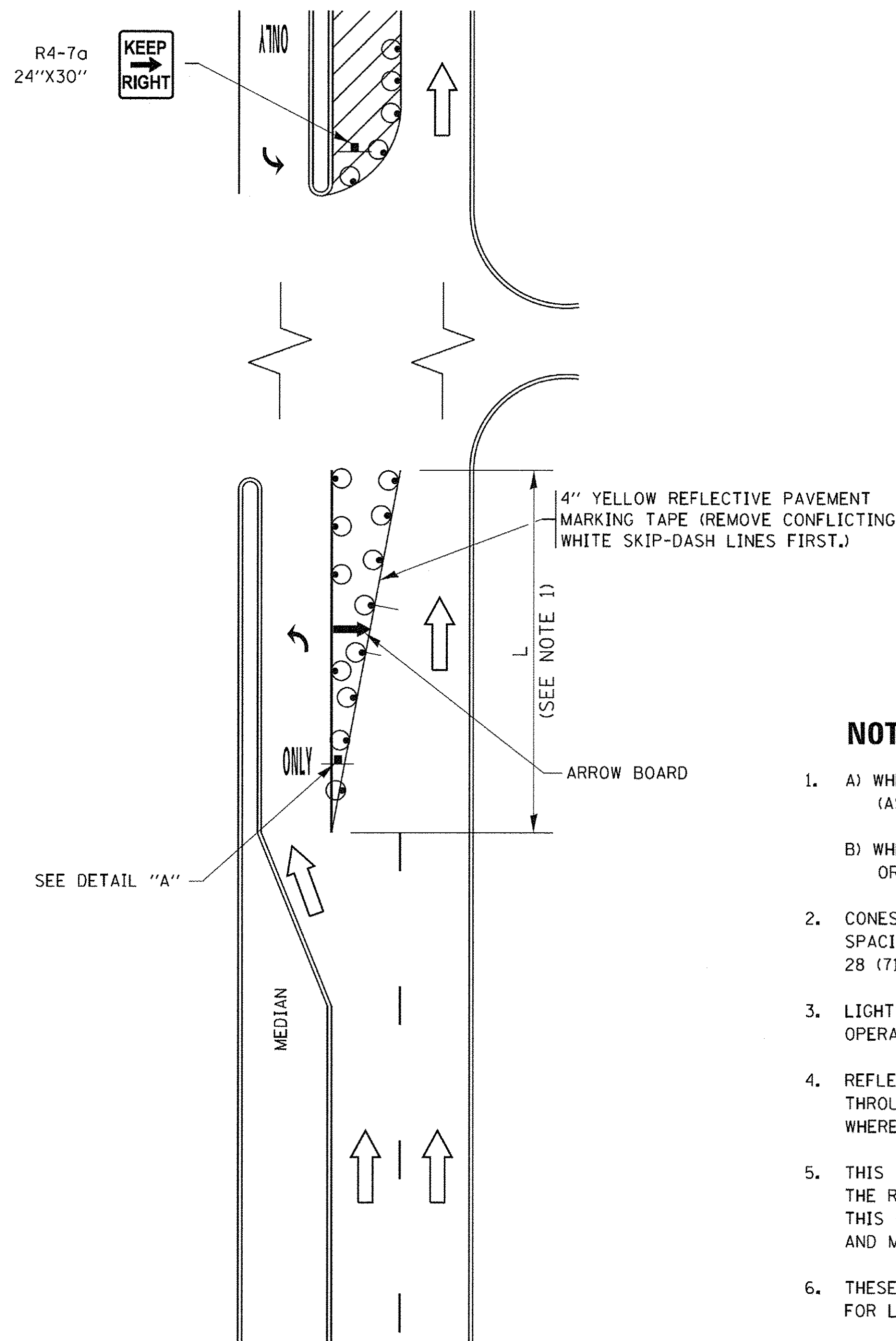


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

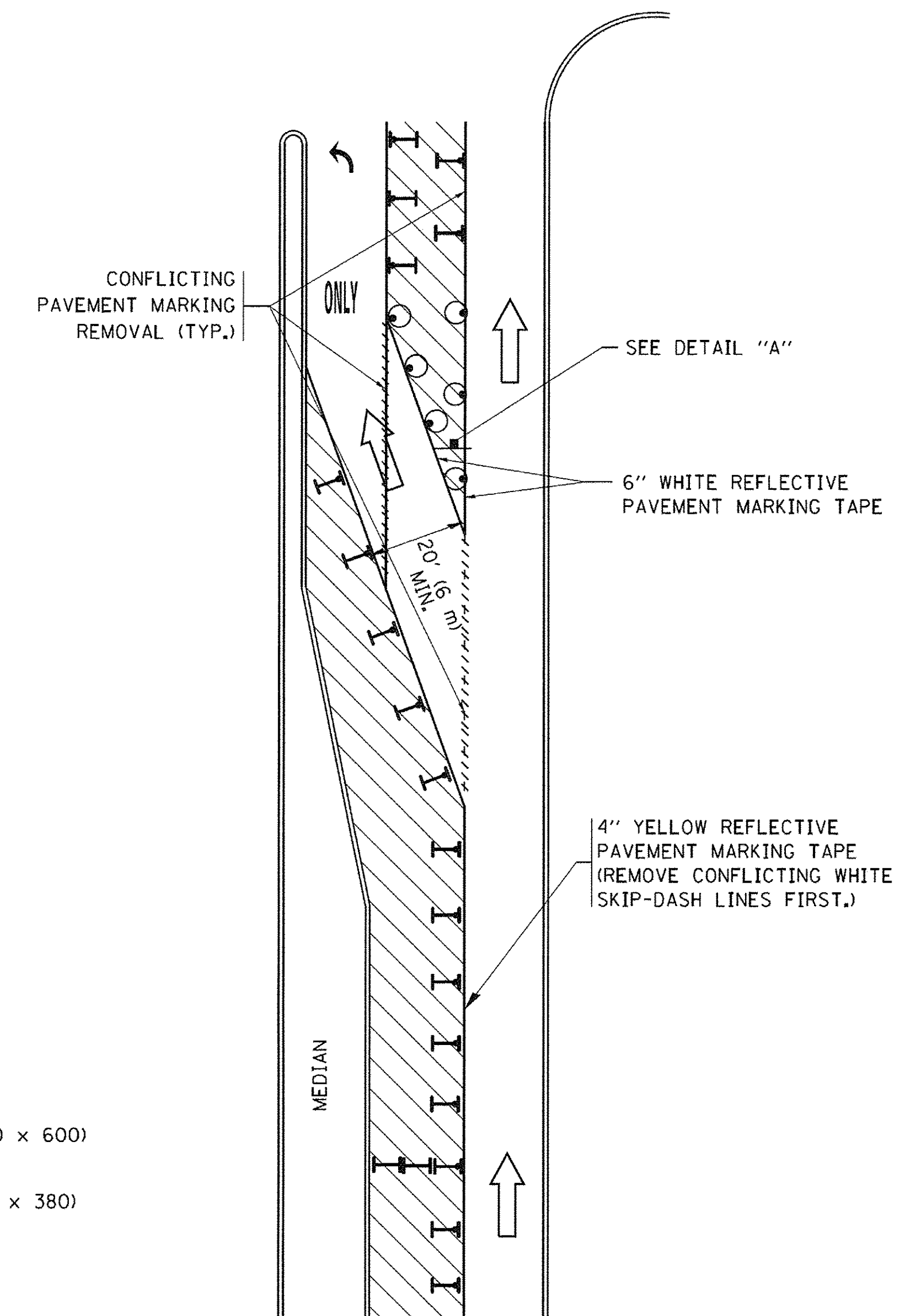


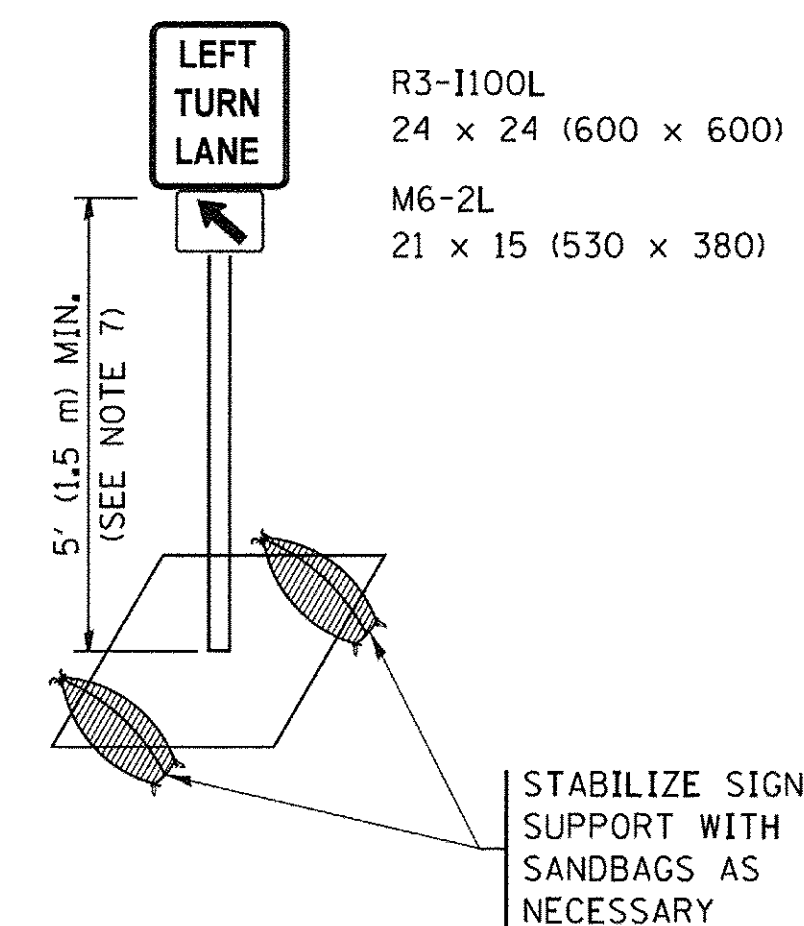
FIGURE 2

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

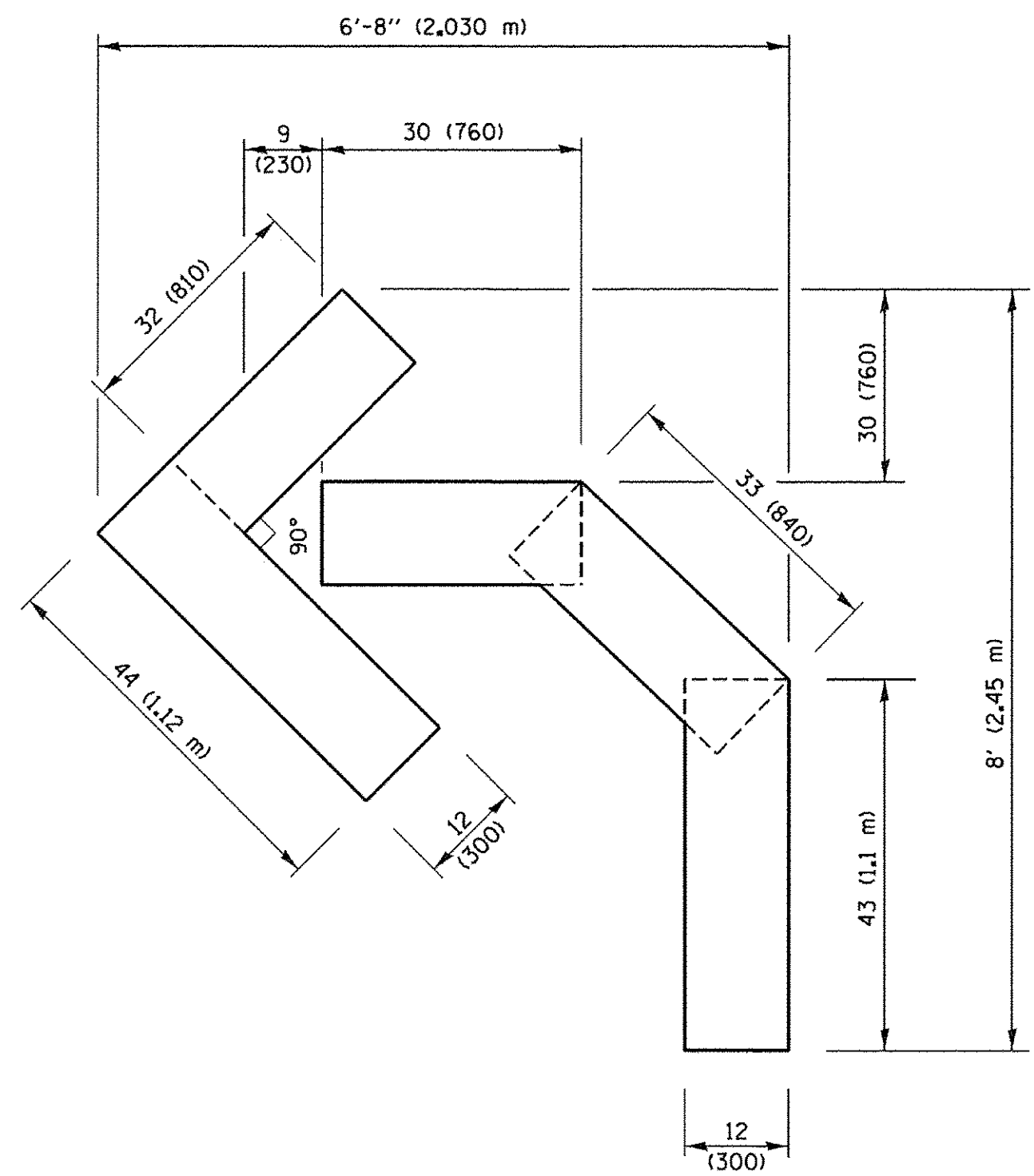
1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



DETAIL A

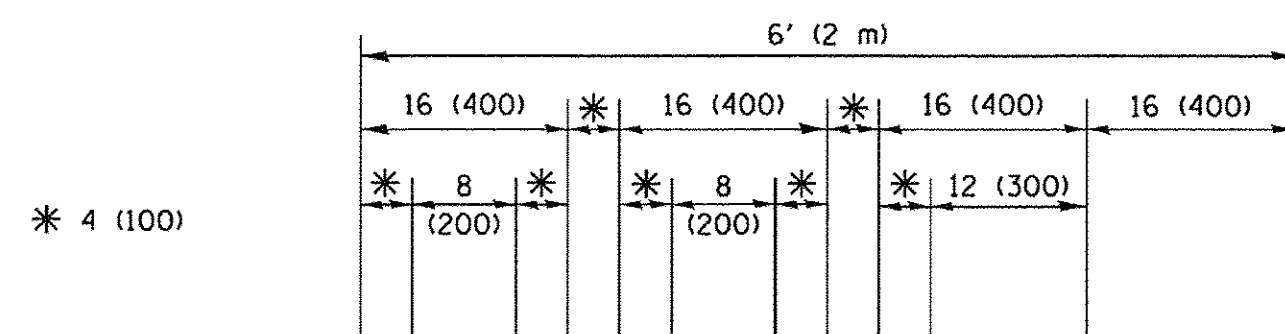
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = Footemj	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)	F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default		REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 07-01-13			607	15-00040-00-CH	WILL	138	122	
	PLOT SCALE = 50,0000' / 1"	REVISED - T. RAMMACHER 01-06-00	REVISED - A. SCHUETZE 09-15-16			TC-14		CONTRACT NO. 61D82			
	PLOT DATE = 9/15/2016					SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT		



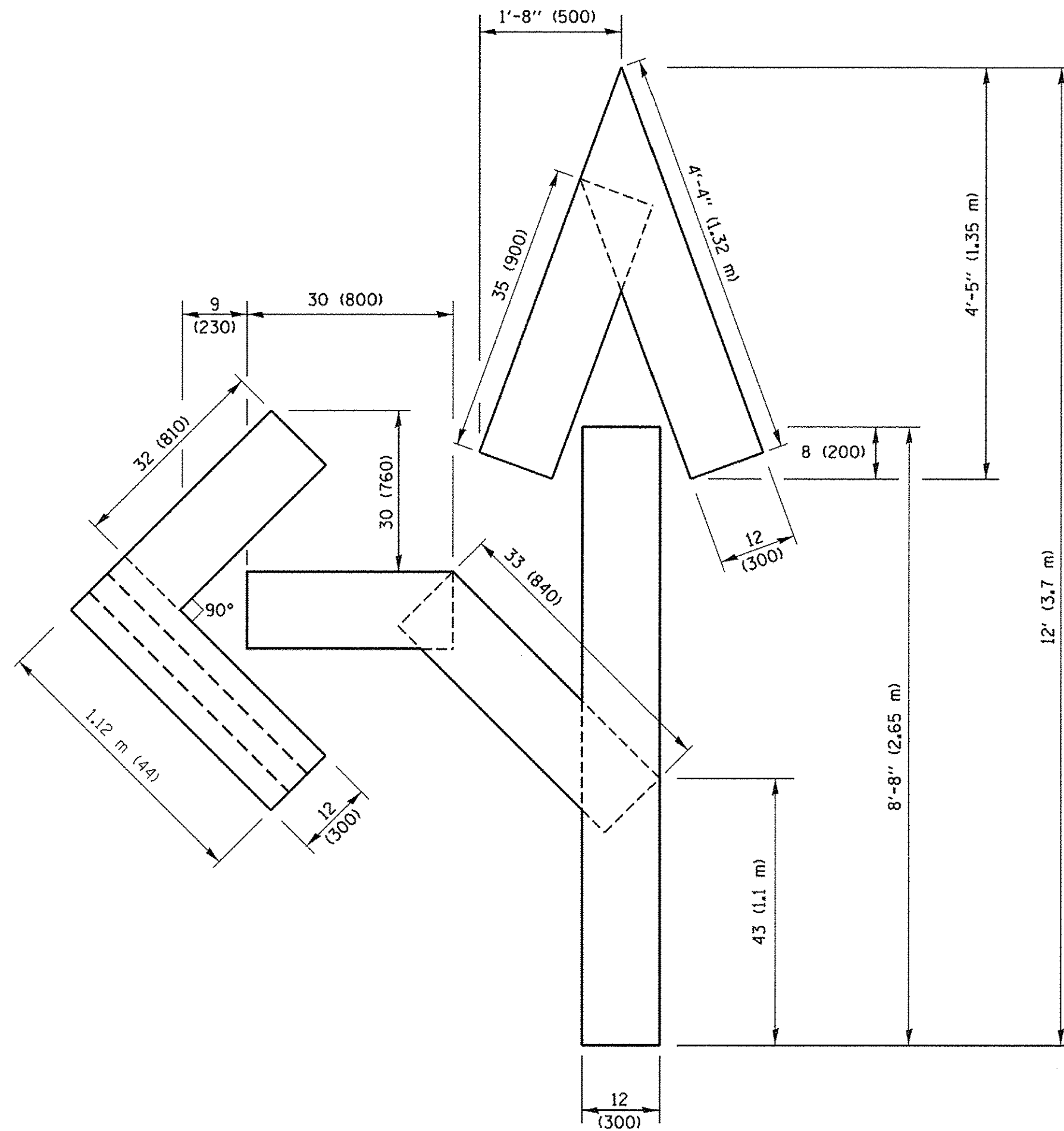
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

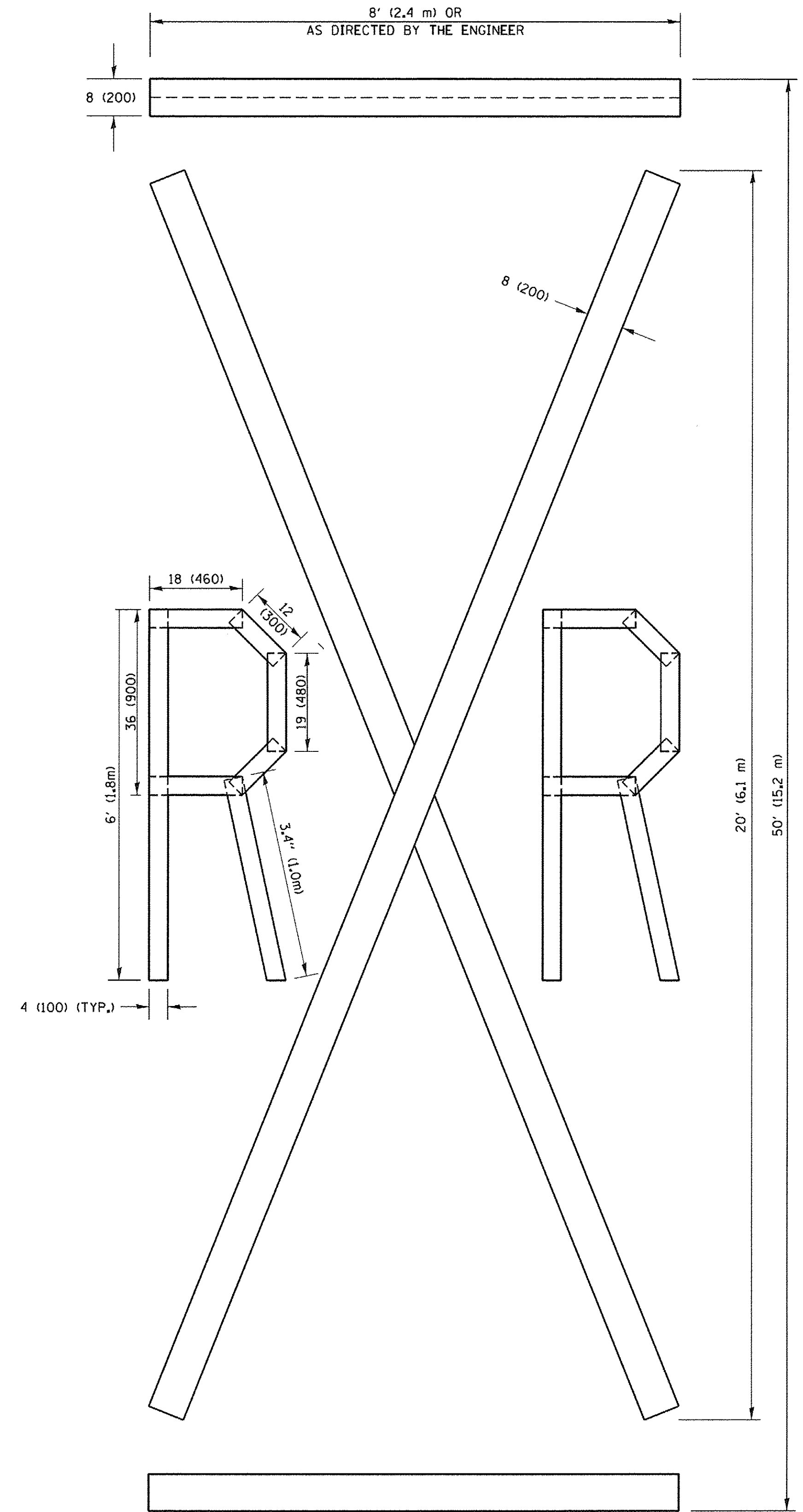


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.

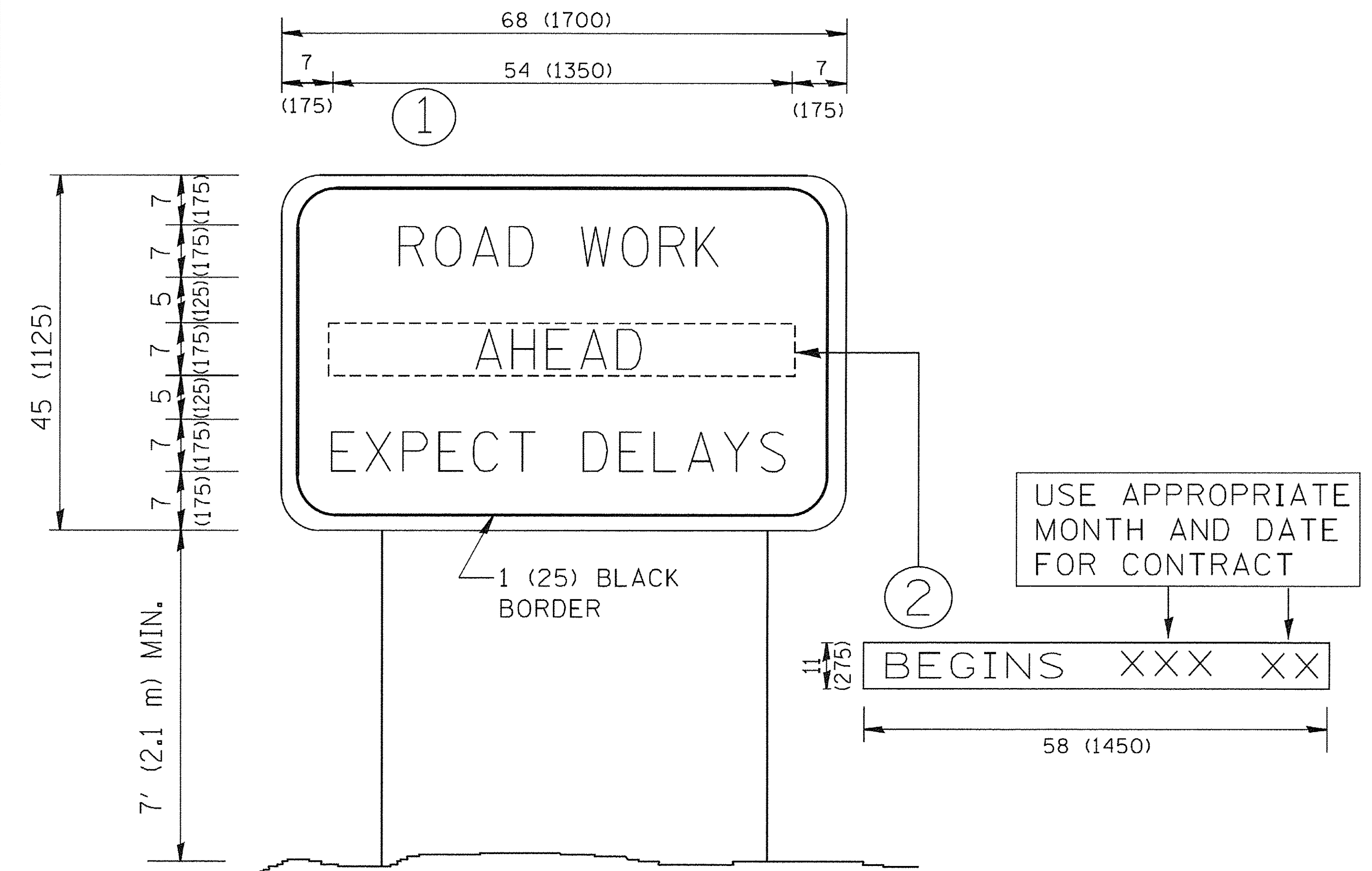


QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = foometj	DESIGNED -	REVISED -T. RAMMACHER 03-02-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS		F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
607	15-00040-00-CH	WILL	138				123					
							TC-16		CONTRACT NO. 61D82			
							FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

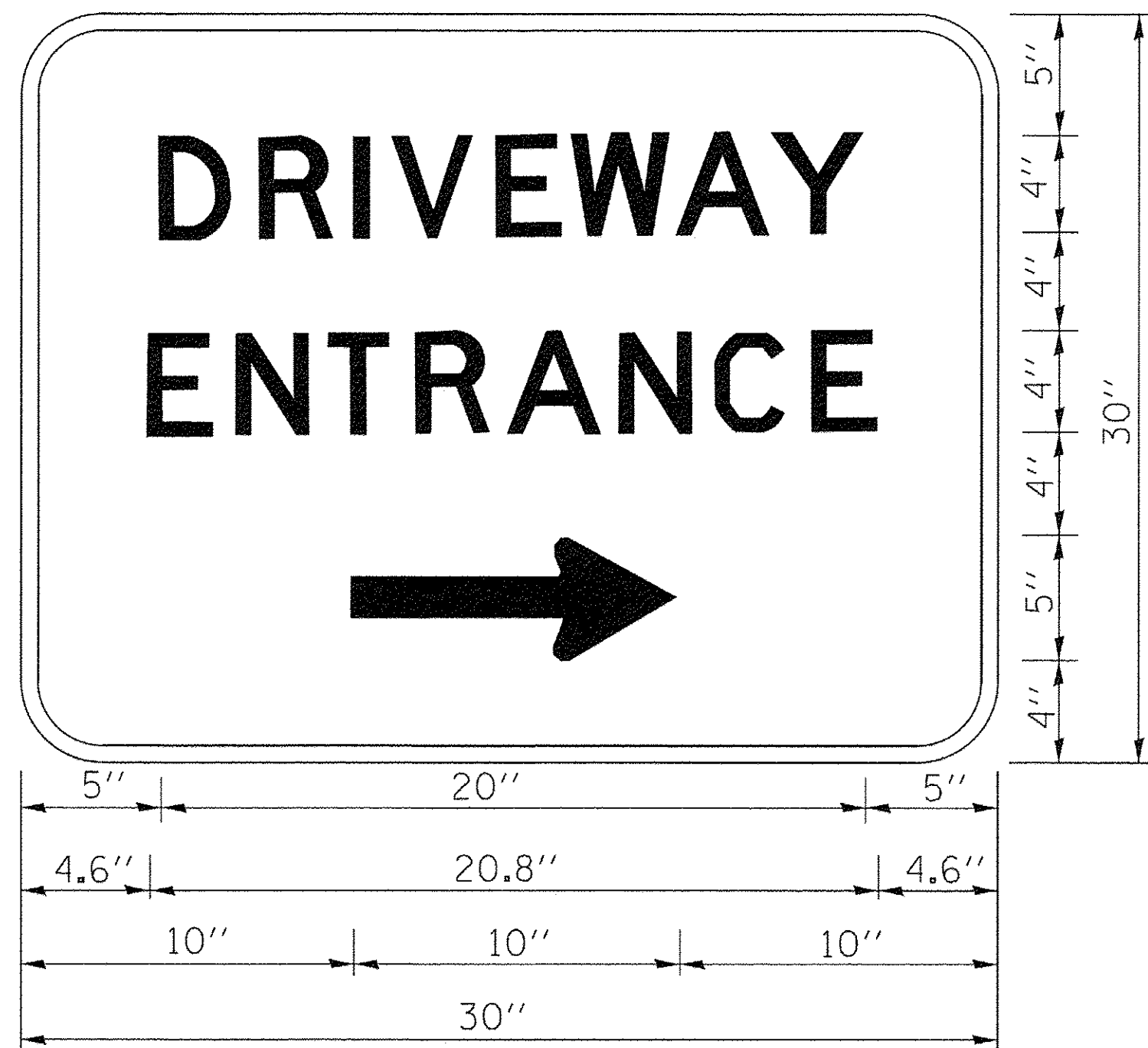
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		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.J. RTE. 607	SECTION 15-00040-00-CH	COUNTY WILL	TOTAL SHEETS 138	SHEET NO. 124
TC-22			CONTRACT NO. 61082	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME =	USER NAME = geglionobt	DESIGNED -	REVISED - C. JUCIUS 02-15-07
at\pwork\pwork\geglionobt\d0108315\tc26.dgn		DRAWN -	REVISED -
	PLOT SCALE = 50.000 1 / in.	CHECKED -	REVISED -
	PLOT DATE = 12/13/2012	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

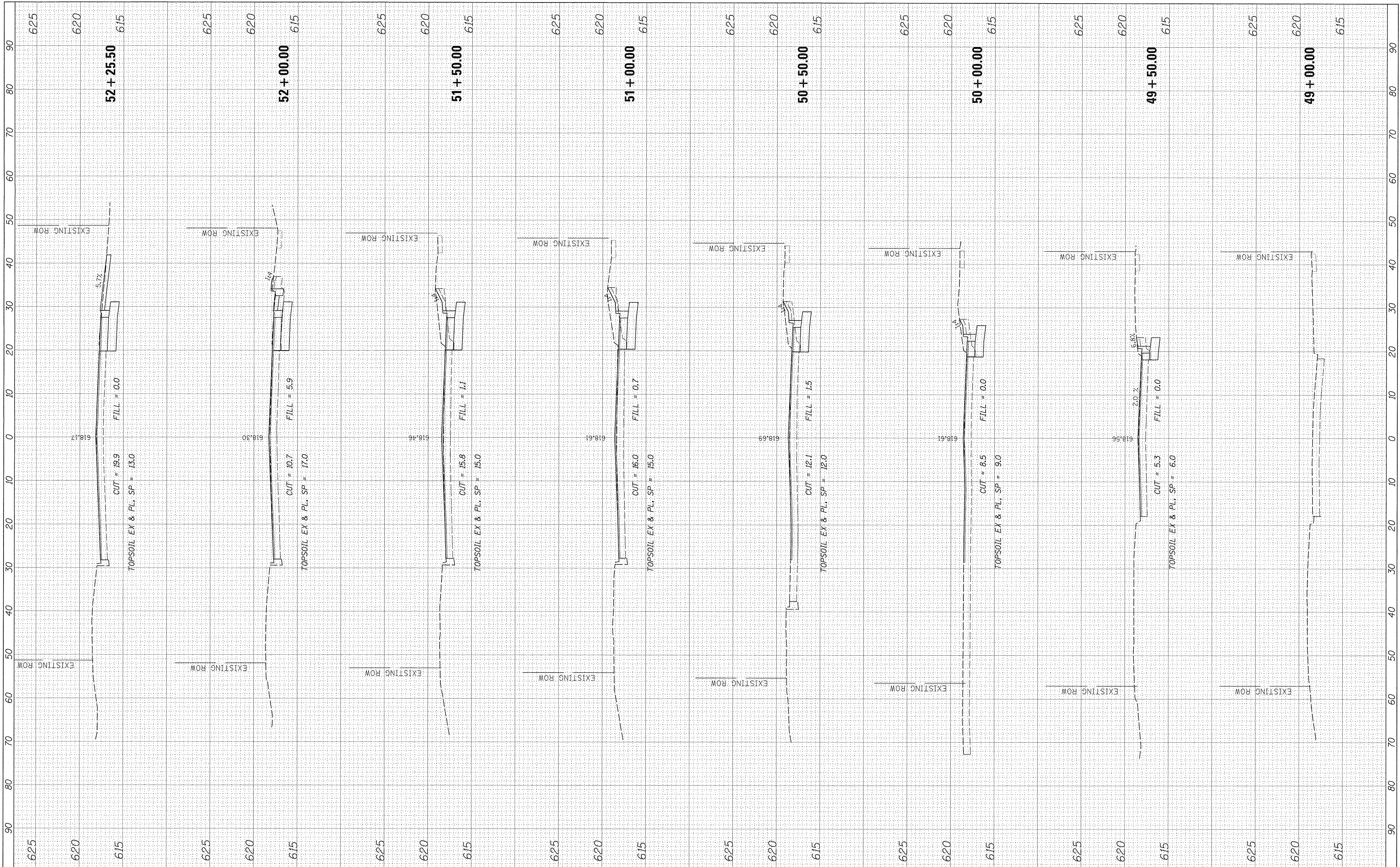
DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
607	15-00040-00-CH	WILL	138	125
TC-26			CONTRACT NO. 61D82	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FINAL SURVEY	BY	DATE
SURVEYED		
NOTED		
TEMPLATE		
AREAS		
CHECKED		
NO.		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
NOTED		
TEMPLATE		
AREAS		
CHECKED		
NO.		



FILE NAME =
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USER NAME = jstrick
 PLOT SCALE = 1" = 10'
 PLOT DATE = 2/23/2017

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

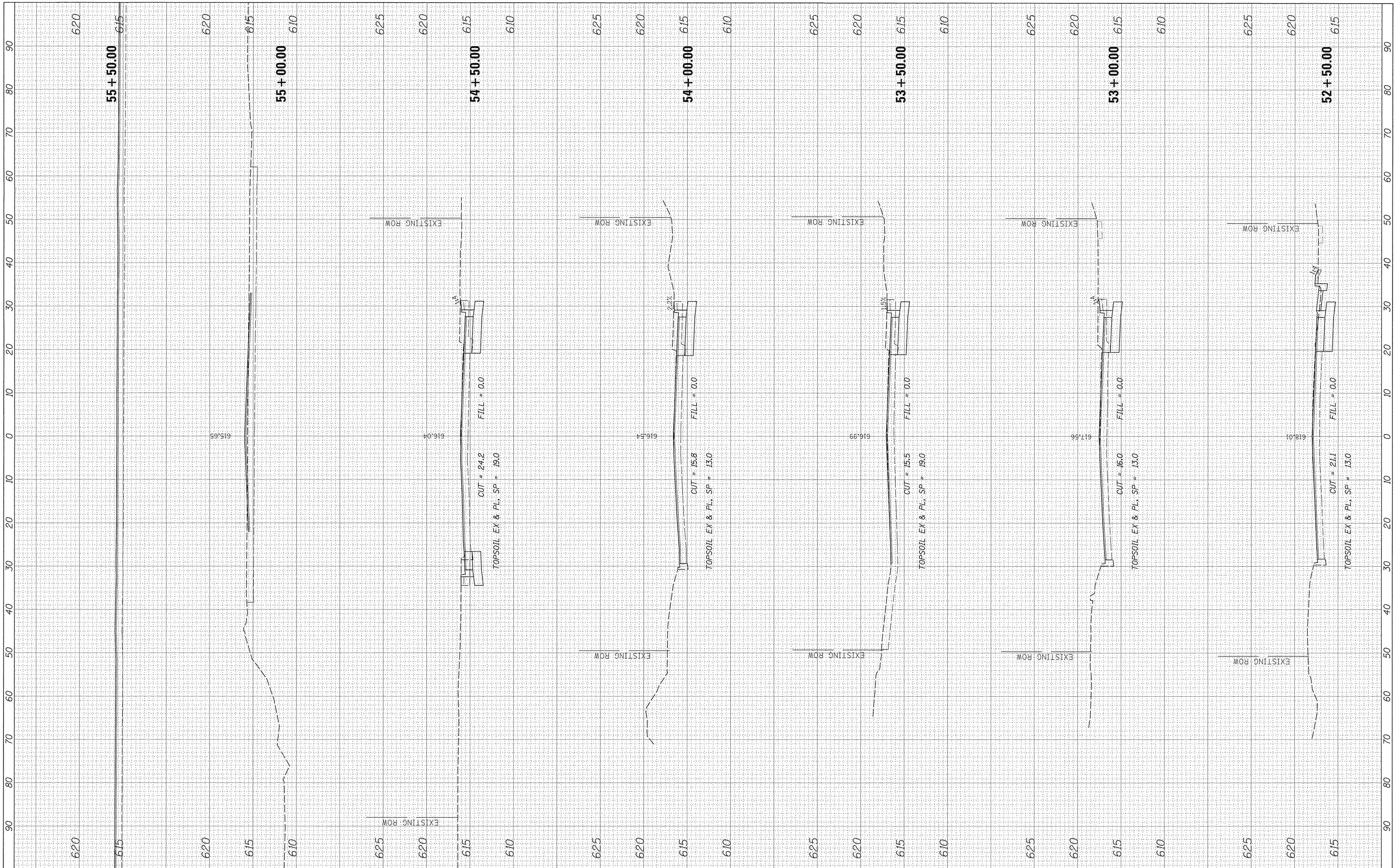
**RIVER ROAD
 CROSS SECTIONS**

SCALE: SHEET 1 OF 4 SHEETS STA. 49+00.00 TO STA. 52+25.50

F.A.U. RTE. 607	SECTION 15-00040-00-CH	COUNTY WILL	TOTAL SHEETS 138	SHEET NO. 126
CONTRACT NO. 61D82			ILLINOIS FED. AID PROJECT	

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS		
	CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS		
	CHECKED		



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 USER NAME = jstrick
 PLOT SCALE = 10'
 PLOT DATE = 2/23/2017

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

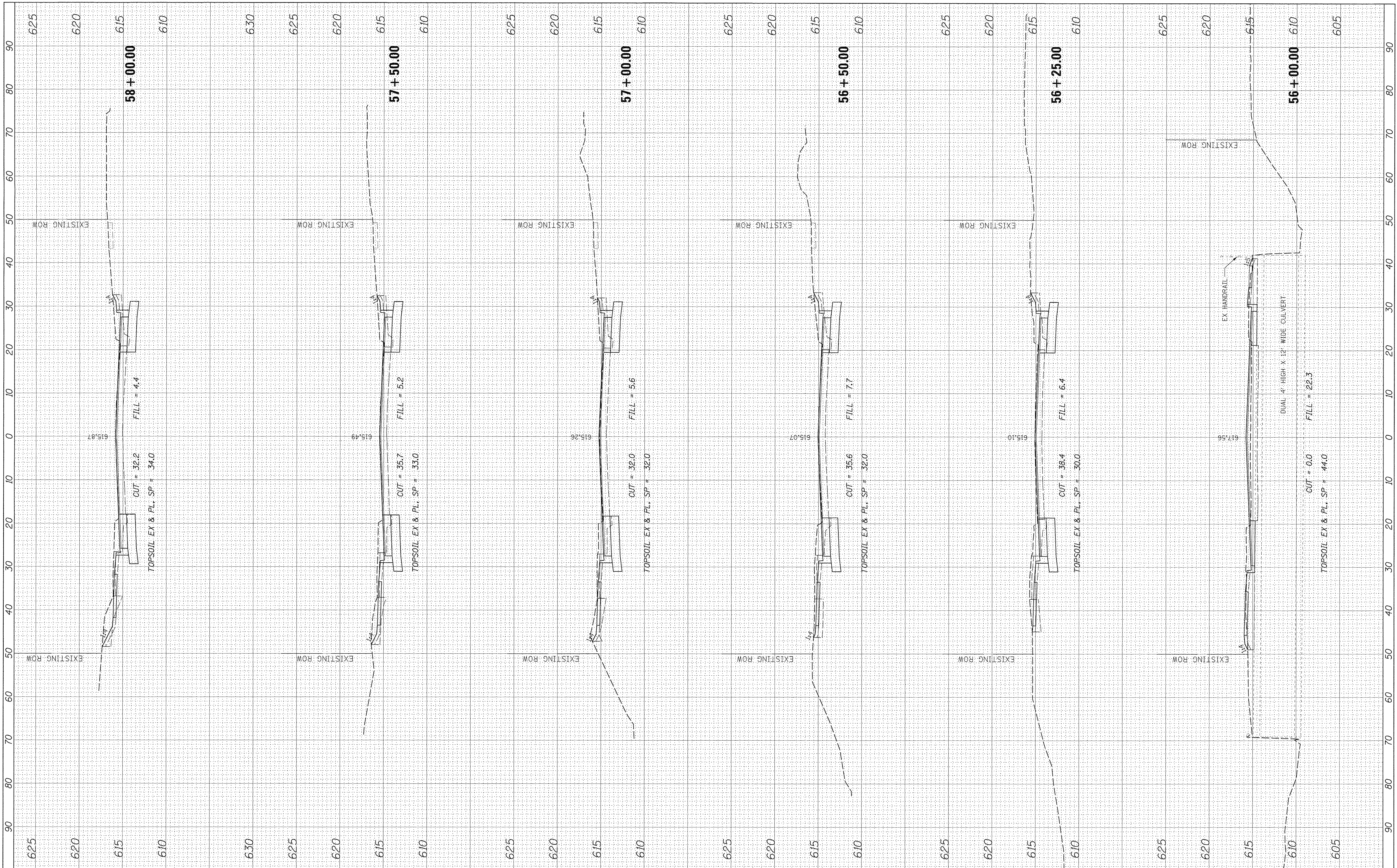
**RIVER ROAD
 CROSS SECTIONS**

SCALE: SHEET 2 OF 4 SHEETS STA. 52+50.00 TO STA. 55+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
607	15-00040-00-CH	WILL	138	127
CONTRACT NO. 61D82				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY	BY	DATE
CHECKED		
NOTED		
PLotted		
TEMPLATE		
AREAS		
CHECKED		

ORIGINAL SURVEY	BY	DATE
CHECKED		
NOTED		
PLotted		
TEMPLATE		
AREAS		
CHECKED		



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 PLOT SCALE = 1"=10'
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DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

REVISED -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

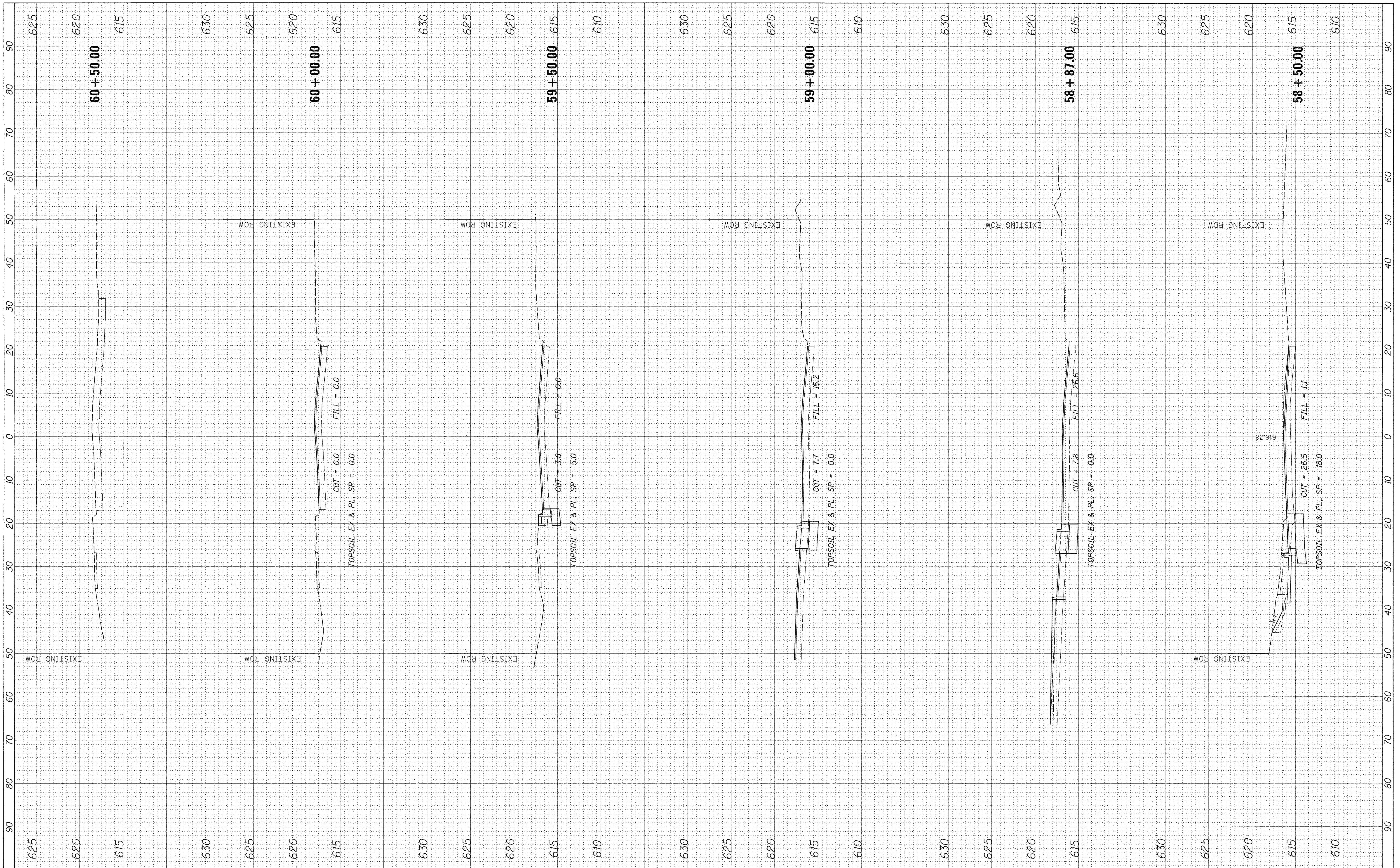
**RIVER ROAD
 CROSS SECTIONS**

SCALE: SHEET 3 OF 4 SHEETS STA. 56+00.00 TO STA. 58+00.00

F.A.U. RTE. 607	SECTION 15-00040-00-CH	COUNTY WILL	TOTAL SHEETS 138	SHEET NO. 128
CONTRACT NO. 61D82			ILLINOIS FED. AID PROJECT	

FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
NOTE BOOK		
TEMPLATE		
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AREAS CHECKED		
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ORIGINAL SURVEY	BY	DATE
SURVEYED		
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TEMPLATE		
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AREAS CHECKED		
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 PLOT SCALE = 10'
 PLOT DATE = 2/23/2017

DESIGNED -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

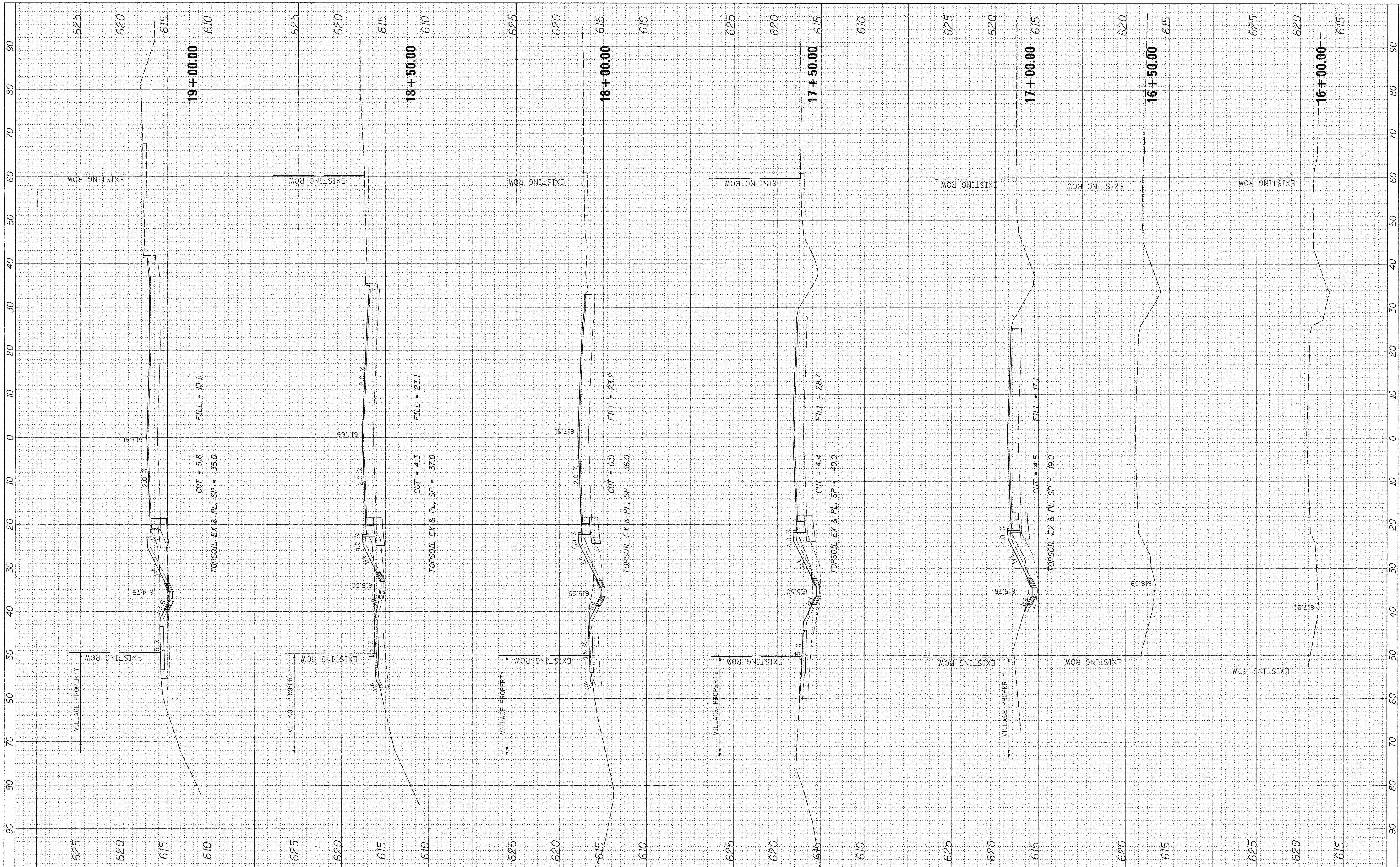
**RIVER ROAD
 CROSS SECTIONS**

SCALE: SHEET 4 OF 4 SHEETS STA. 58+50.00 TO STA. 60+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
607	15-00040-00-CH	WILL	138	129
CONTRACT NO. 61D82				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY	DATE
SUPERVISED	BY
NOTE BOOK	
TEMPLATE	
AREAS	
CHECKED	

ORIGINAL SURVEY	DATE
SUPERVISED	BY
NOTE BOOK	
TEMPLATE	
AREAS	
CHECKED	



FILE NAME =
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 CHECKED -
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REVISOR -
 REVISIONS -
 REVISIONS -
 REVISIONS -
 REVISIONS -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

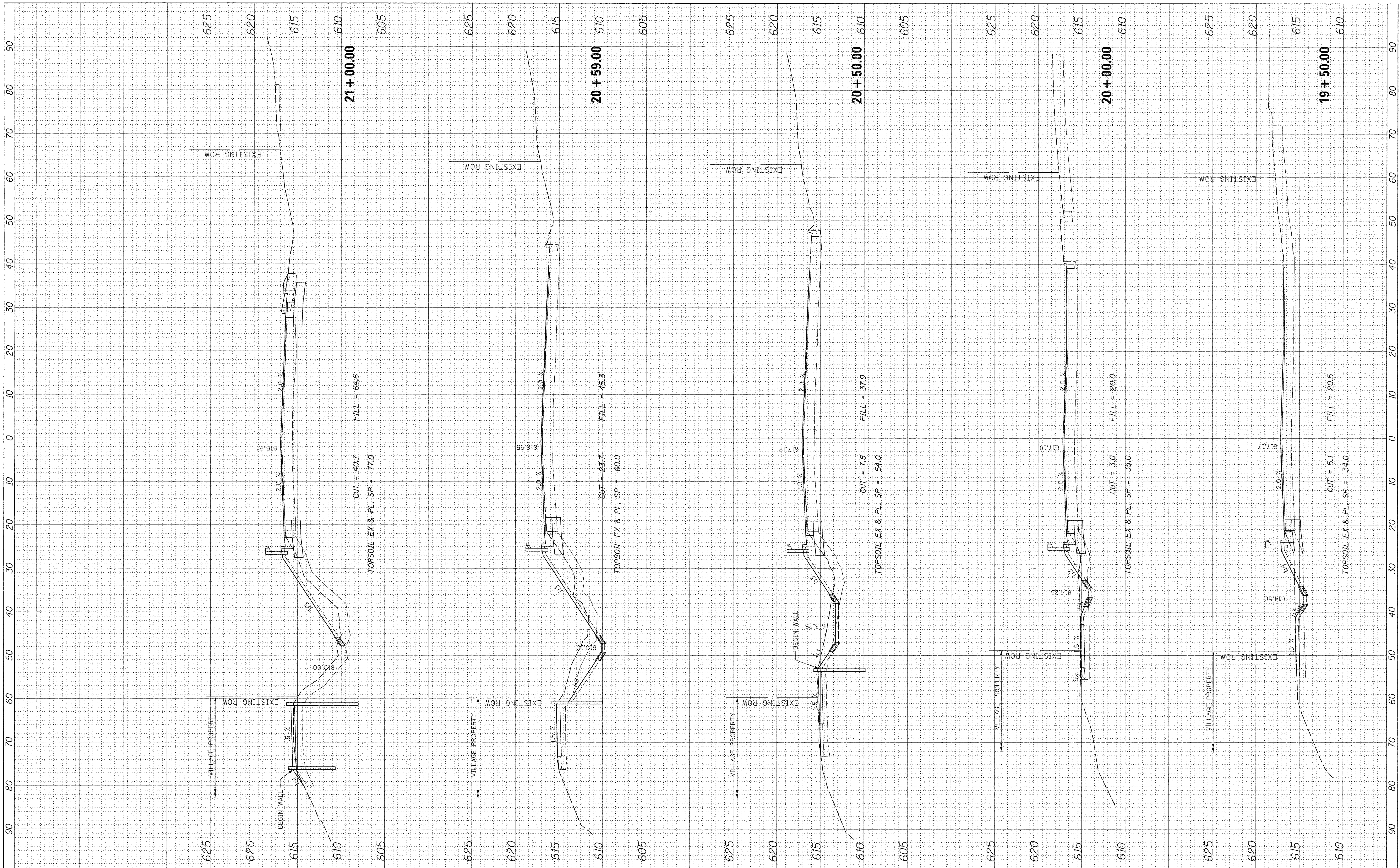
**US ROUTE 52
 CROSS SECTIONS**

SCALE: SHEET 1 OF 9 SHEETS STA. 16+00.00 TO STA. 19+00.00

F.A.U. RTE. 607	SECTION 15-00040-00-CH	COUNTY WILL	TOTAL SHEETS 138	SHEET NO. 130
CONTRACT NO. 61D82				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS		
	CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS		
	CHECKED		



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 USER NAME = jstrick
 PLOT SCALE = 10'
 PLOT DATE = 2/23/2017

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

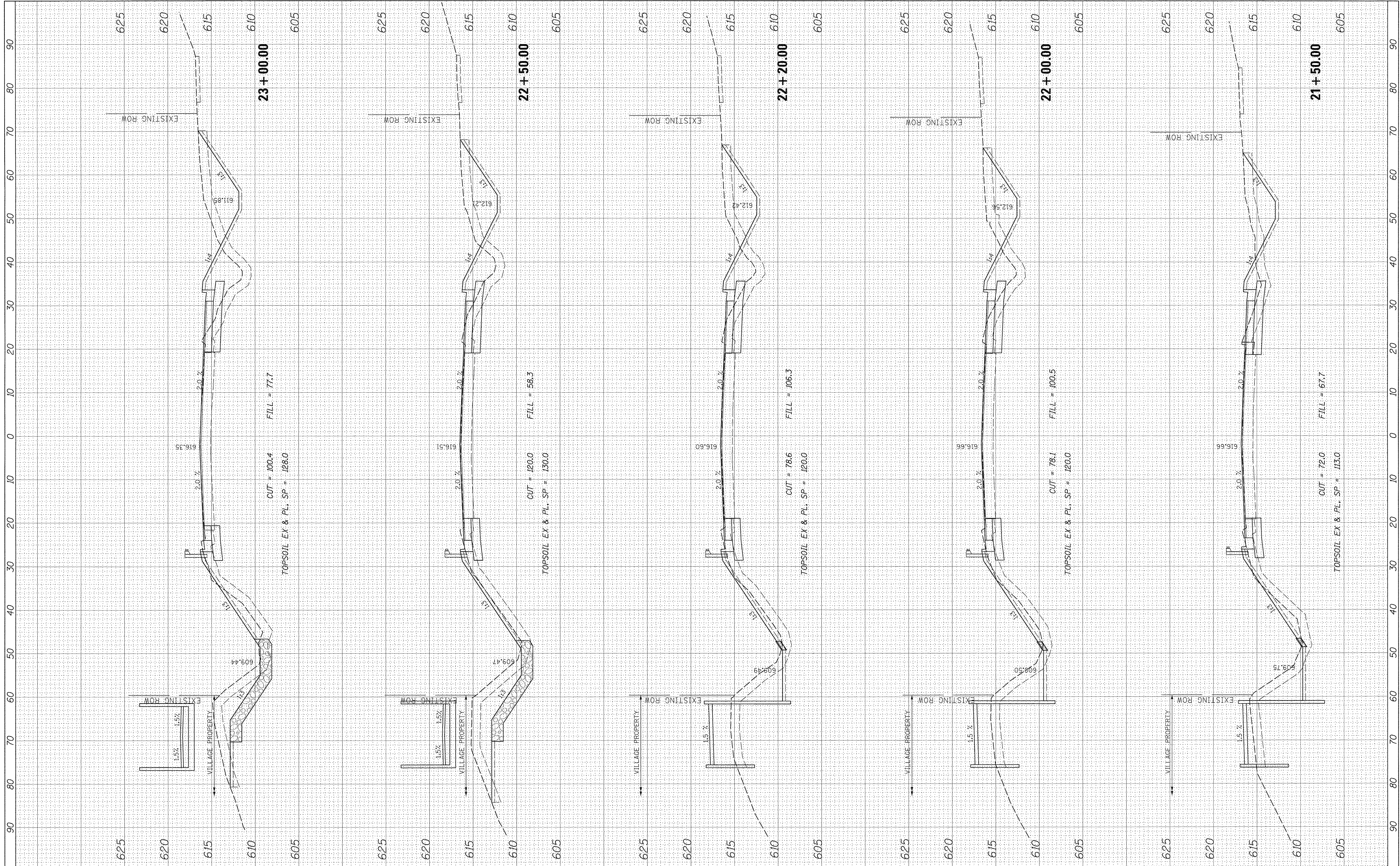
**US ROUTE 52
 CROSS SECTIONS**

SCALE: SHEET 2 OF 9 SHEETS STA. 19+50.00 TO STA. 21+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
607	15-00040-00-CH	WILL	138	131
CONTRACT NO. 61D82				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY	BY	DATE
NOTE BOOK		
NO.		
SURVEYED		
TEMPLATE		
AREAS		
CHECKED		

ORIGINAL SURVEY	BY	DATE
NOTE BOOK		
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SURVEYED		
TEMPLATE		
AREAS		
CHECKED		



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USER NAME = jstrick
 PLOT SCALE = 10'
 PLOT DATE = 2/23/2017

DESIGNED -
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REVISED -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

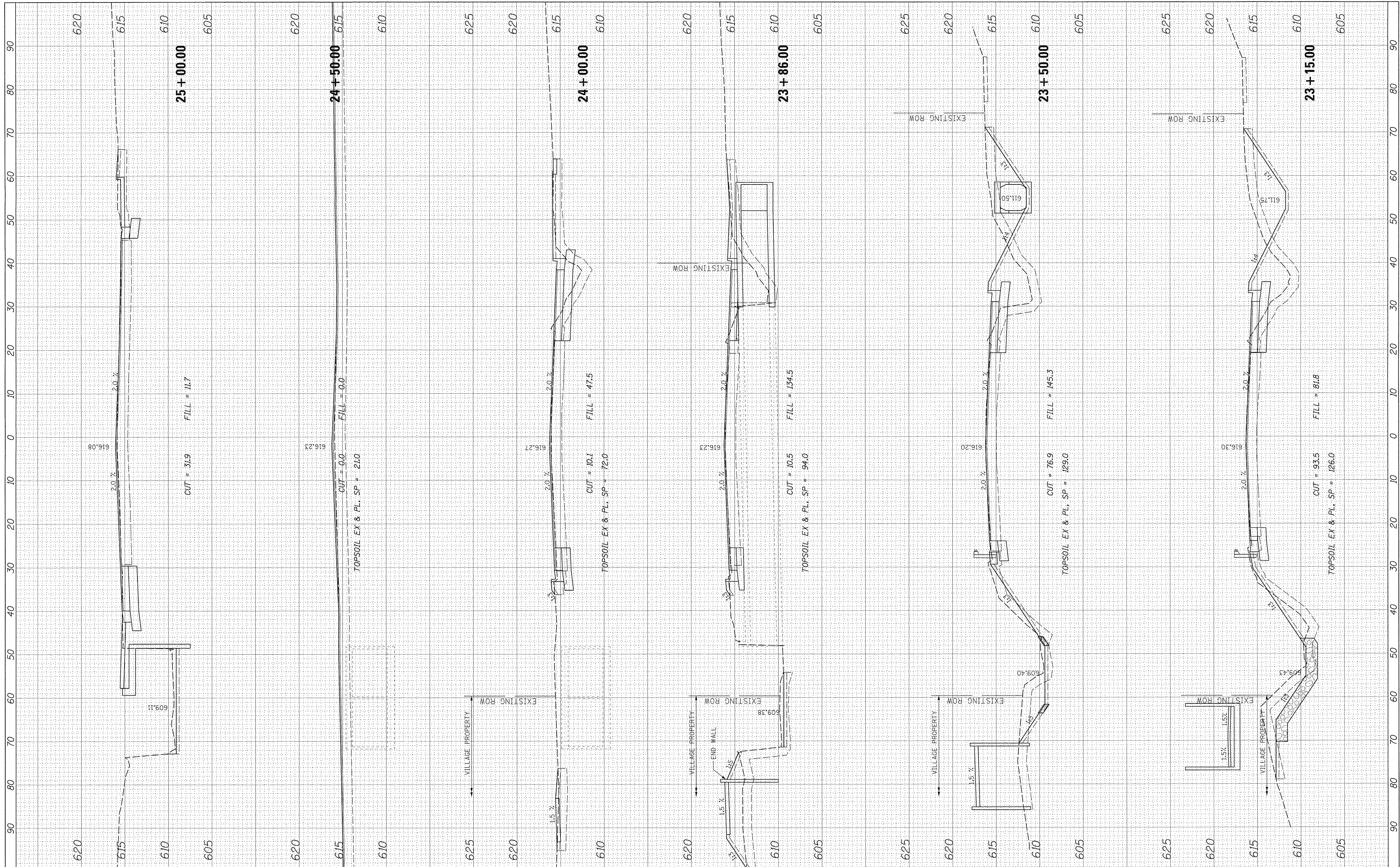
**US ROUTE 52
 CROSS SECTIONS**

SCALE: SHEET 3 OF 9 SHEETS STA. 21+50.00 TO STA. 23+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
607	15-00040-00-CH	WILL	138	132
CONTRACT NO. 61D82			ILLINOIS FED. AID PROJECT	

FINAL SURVEY	BY	DATE
NOTE BOOK		
NO.		

ORIGINAL SURVEY	BY	DATE
NOTE BOOK		
NO.		



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USER NAME = jstrick
 PLOT SCALE = 10'
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DESIGNED -
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 DATE -

REVISED -
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 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

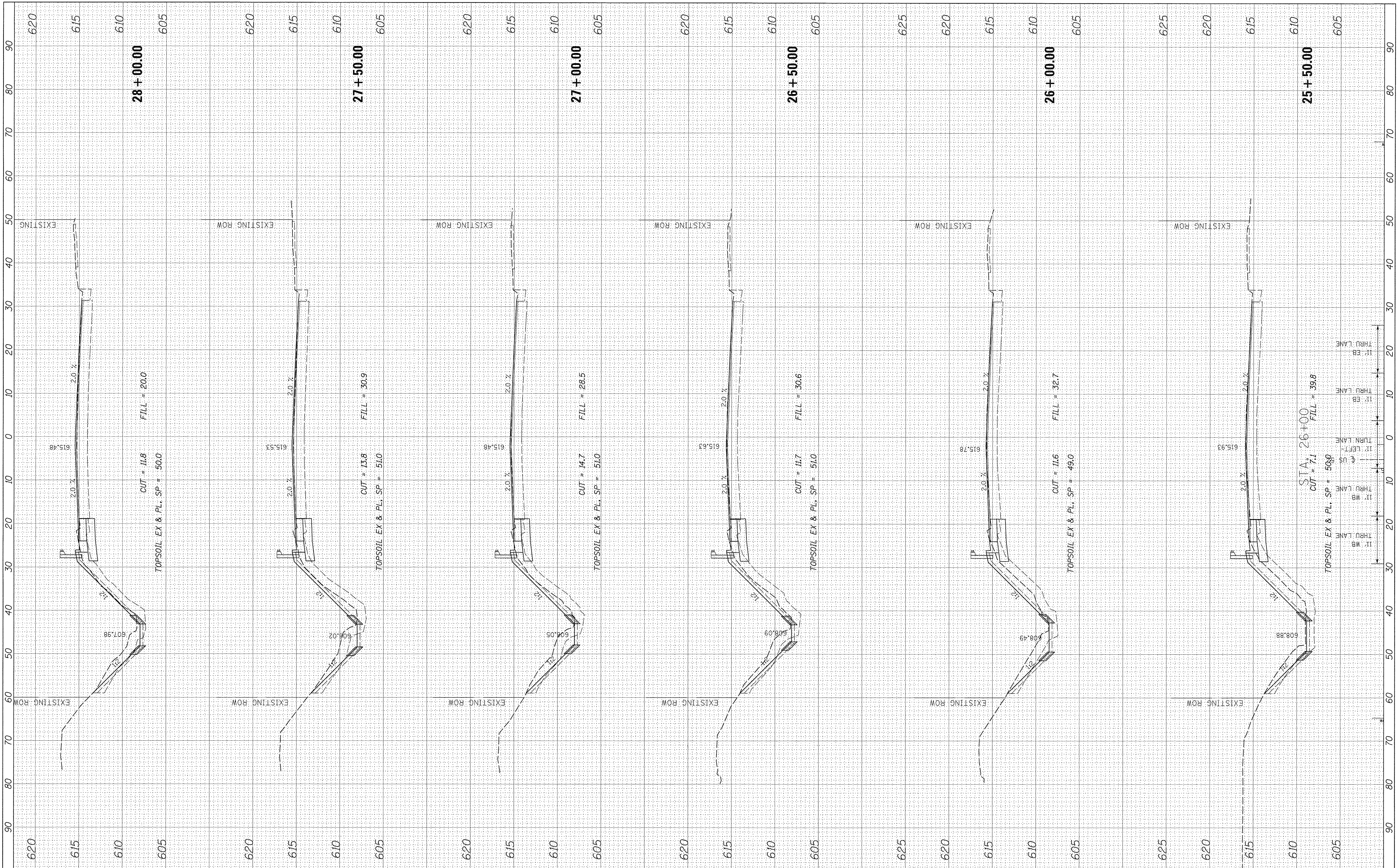
**US ROUTE 52
 CROSS SECTIONS**

SCALE: SHEET 4 OF 9 SHEETS STA. 23+15.00 TO STA. 25+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
607	15-00040-00-CH	WILL	138	133
CONTRACT NO. 61D82				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
TEMPLATE		
NOTE BOOK		
AREAS		
CHECKED		
NO.		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
TEMPLATE		
NOTE BOOK		
AREAS		
CHECKED		
NO.		



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 PLOT DATE = 2/23/2017

DESIGNED	-	REVISED	-
DRAWN	-	REVISED	-
CHECKED	-	REVISED	-
DATE	-	REVISED	-

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

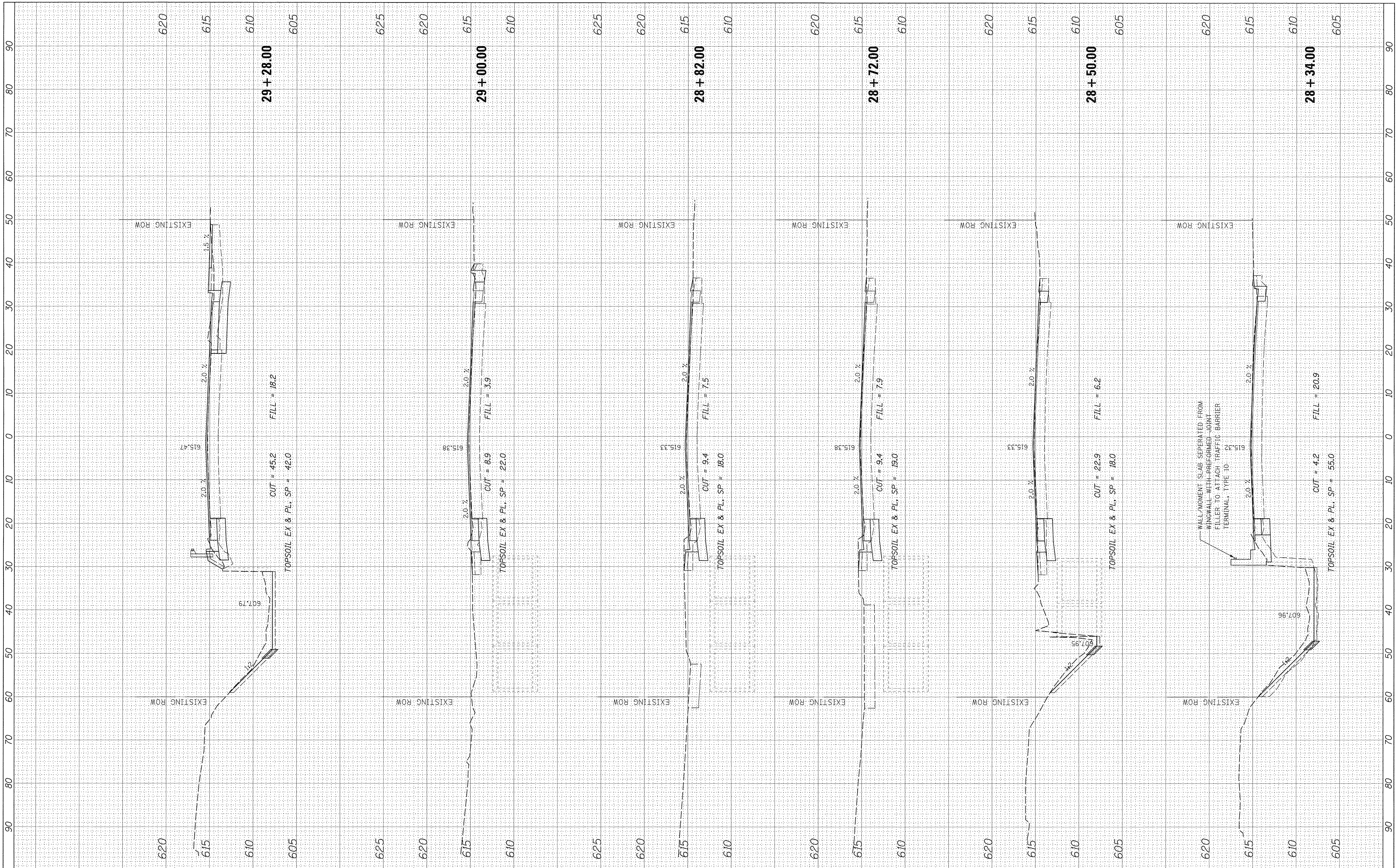
**US ROUTE 52
 CROSS SECTIONS**

SCALE: SHEET 5 OF 9 SHEETS STA. 25+50.00 TO STA. 28+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
607	15-00040-00-CH	WILL	138	134
CONTRACT NO. 61D82			ILLINOIS FED. AID PROJECT	

FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
TEMPLATE		
AREAS		
CHECKED		
NO.		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
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 DRAWN -
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REVISOR -
 REVISED -
 REVISED -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

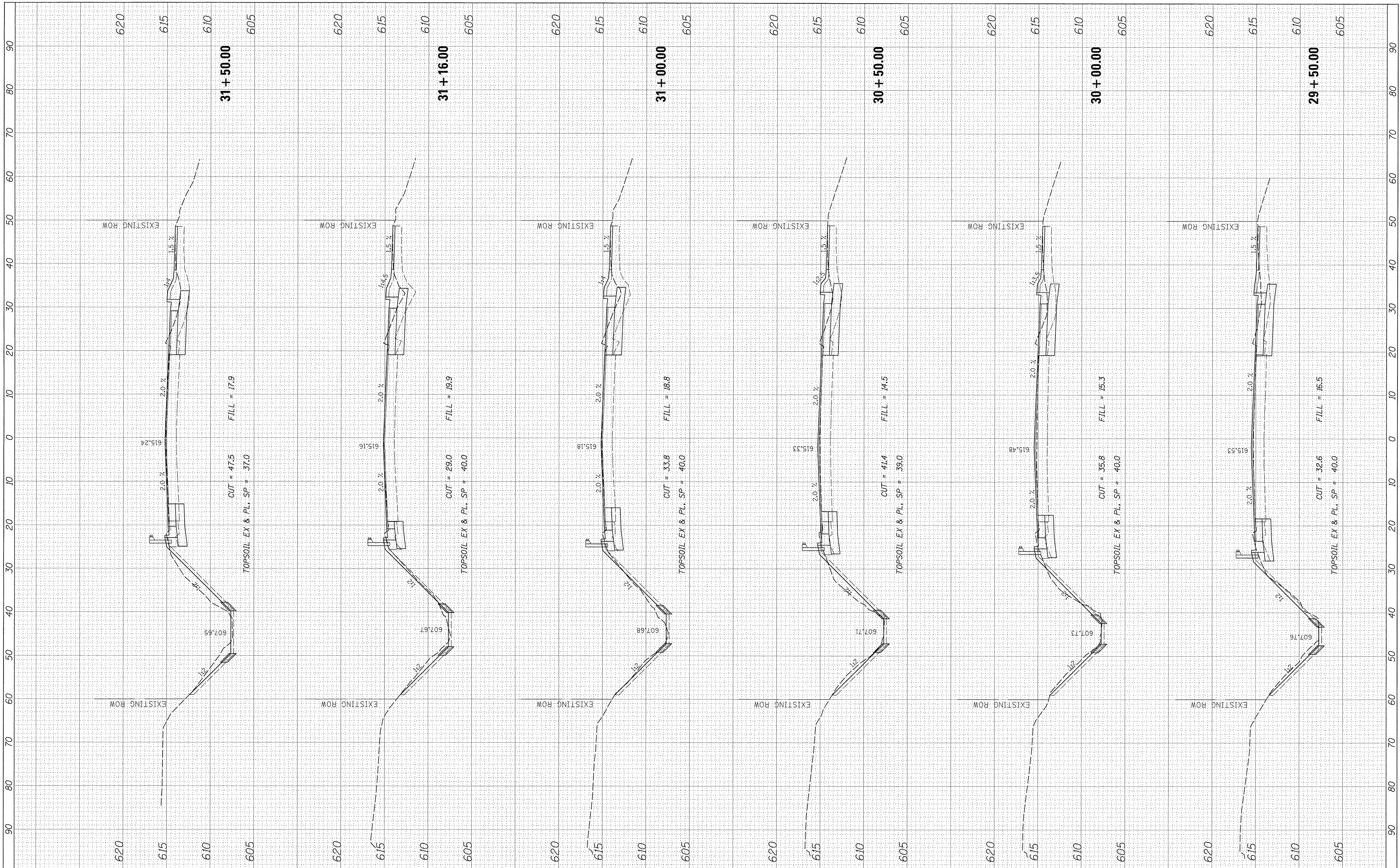
**US ROUTE 52
 CROSS SECTIONS**

SCALE: SHEET 6 OF 9 SHEETS STA. 28+34.00 TO STA. 29+28.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
607	15-00040-00-CH	WILL	138	135
CONTRACT NO. 61D82				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY	BY	DATE
SURVEYED		
NOTE BOOK		
NO.		
AREAS CHECKED		
AREAS CHECKED		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
NOTE BOOK		
NO.		
AREAS CHECKED		
AREAS CHECKED		



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USER NAME = jstrick
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 CHECKED -
 DATE - 2/23/2017

REVISOR -
 REVISIONS -
 REVISIONS -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

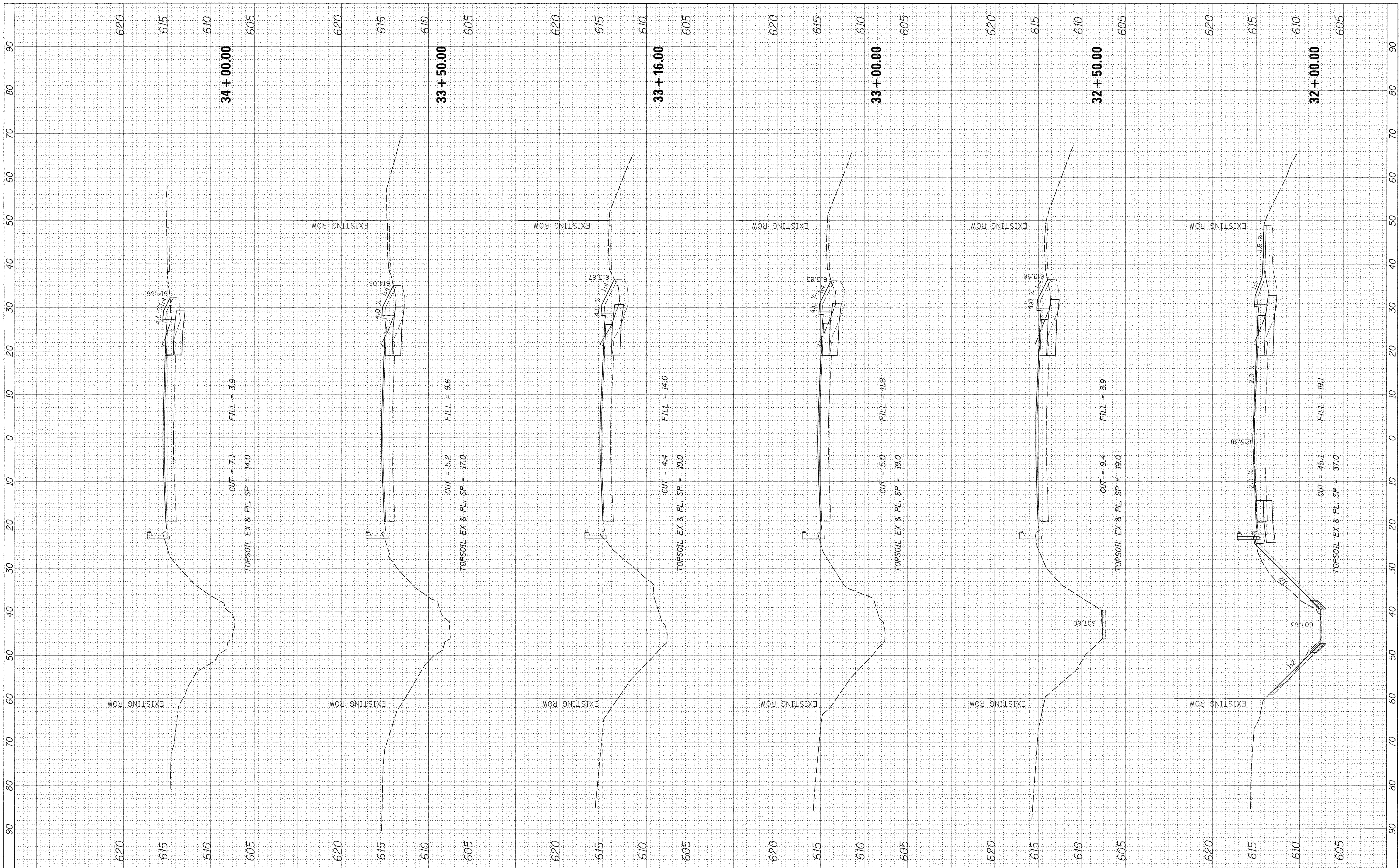
**US ROUTE 52
 CROSS SECTIONS**

SCALE: SHEET 7 OF 9 SHEETS STA. 29+50.00 TO STA. 31+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
607	15-0040-00-CH	WILL	138	136
CONTRACT NO. 61D82				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY	BY	DATE
SURVEYED		
NOTE BOOK		
AREAS CHECKED		
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ORIGINAL SURVEY	BY	DATE
SURVEYED		
NOTE BOOK		
AREAS CHECKED		
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REVISOR -
 REVISED -
 REVISED -
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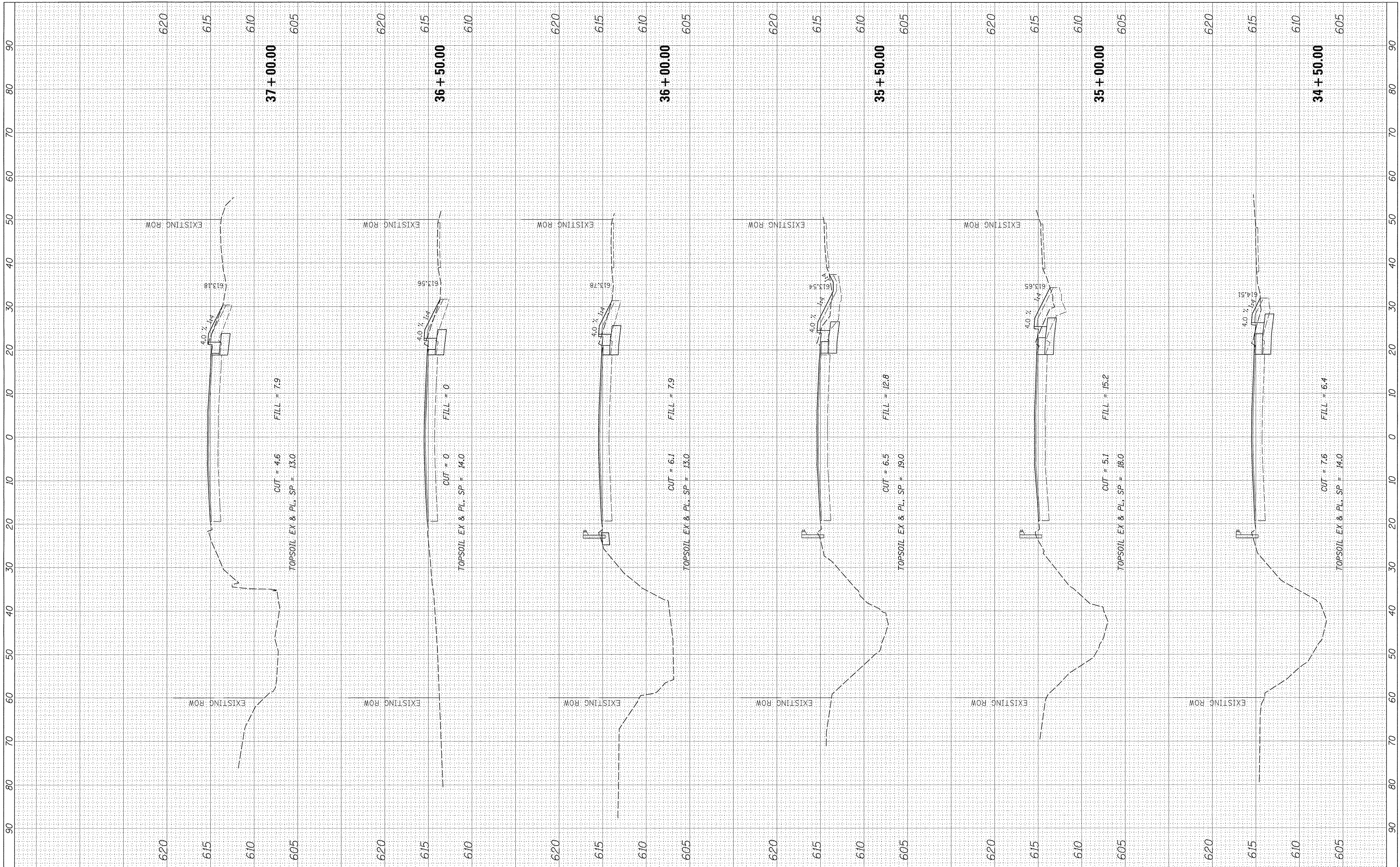
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**US ROUTE 52
 CROSS SECTIONS**
 SCALE: SHEET 8 OF 9 SHEETS STA. 32+00.00 TO STA. 34+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
607	15-00040-00-CH	WILL	138	137
CONTRACT NO. 61D82				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY	BY	DATE
SUPERSEDED		
PLOTTED		
NOTE BOOK		
TEMPLATE		
AREAS		
CHECKED		

ORIGINAL SURVEY	BY	DATE
SUPERSEDED		
PLOTTED		
NOTE BOOK		
TEMPLATE		
AREAS		
CHECKED		



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USER NAME = jst-rick
 PLOT SCALE = 10'
 PLOT DATE = 2/23/2017

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**US ROUTE 52
 CROSS SECTIONS**

SCALE: SHEET 9 OF 9 SHEETS STA. 34+50.00 TO STA. 37+00.00

F.A.U. RTE. 607	SECTION 15-00040-00-CH	COUNTY WILL	TOTAL SHEETS 138	SHEET NO. 138
CONTRACT NO. 61D82			ILLINOIS FED. AID PROJECT	