

GENERAL NOTES

1. THE PROJECT PAY ITEMS ARE INTENDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL INCIDENTAL WORK REQUIRED TO COMPLETE THE PROJECT TO THE SATISFACTION OF THE RESIDENT ENGINEER IS TO BE INCLUDED IN THE COSTS OF PERFORMING THESE ITEMS. THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIAL, EQUIPMENT, AND TRANSPORTATION NECESSARY TO CONSTRUCT ALL ELEMENTS OF THE PROJECT AS DESCRIBED IN THE CONSTRUCTION PLANS AND SPECIFICATIONS.
2. THE RULES, REGULATIONS, AND SPECIFICATIONS ENUMERATED HEREIN SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS. THEY SHALL NOT PROHIBIT THE CONTRACTOR FROM FURNISHING AND INSTALLING HIGHER GRADES OF MATERIAL THAN ARE SPECIFIED HEREIN, IF APPROVED BY THE ENGINEER.
3. ACCESS TO THE SITE SHALL BE RESTRICTED EXCLUSIVELY TO THE DESIGNATED CONSTRUCTION ENTRANCE, STAGING AREA, AND HAUL ROUTE. NO EQUIPMENT OR PERSONNEL SHALL BE PERMITTED OUTSIDE THE GENERAL PROJECT AREA.
4. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT AND KEEP CLEAN OF DEBRIS ALL EXISTING AIRFIELD AND ROADWAY PAVEMENTS AT ALL TIMES. ANY DAMAGE TO EXISTING ELECTRICAL, DRAINAGE, AND PAVEMENT STRUCTURES SHALL BE IMMEDIATELY REPAIRED AT NO ADDITIONAL COST TO THE CONTRACT.
5. CONTRACTOR IS REQUIRED TO PROVIDE THEIR OWN RESTROOM FACILITIES.
6. THE OWNER SHALL HAVE THE RIGHT OF FIRST REFUSAL FOR ALL SALVAGEABLE MATERIAL REMOVED ON THE PROJECT.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING PERMITS FOR HAULING ON PUBLIC ROADS, AS APPLICABLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ANY DAMAGES TO ANY PAVEMENTS (PUBLIC OR PRIVATE) CAUSED BY HIS/HER CONSTRUCTION EQUIPMENT OR PERSONNEL.
8. THE CONTRACTOR SHALL PROVIDE ONE SET OF PRELIMINARY REDLINED RECORD DRAWINGS TO THE RESIDENT ENGINEER AT THE COMPLETION OF THE PROJECT FOR INCORPORATION INTO THE OFFICIAL RECORD DRAWINGS HE WILL PREPARE.
9. APPROXIMATE LOCATIONS OF UNDERGROUND UTILITIES ARE SHOWN THROUGHOUT THESE PLANS. THE CONTRACTOR SHALL DETERMINE EXACT LOCATIONS AND PROTECT THESE UTILITIES DURING CONSTRUCTION. ANY UTILITIES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL COORDINATE WITH THE PROPER AUTHORITIES FOR THE PURPOSE OF LOCATING AND PROTECTING EXISTING UNDERGROUND UTILITIES.
10. THE CONTRACTOR SHALL PROVIDE ALL LAYOUT NEEDS, BOTH VERTICALLY AND HORIZONTALLY.
11. ALL REQUESTS FOR INFORMATION SHALL BE PROVIDED IN WRITING AND ALL DIRECTIVES SHALL BE RECEIVED IN WRITING.
12. CHANGES TO THE PROJECT SHALL NOT BE INCORPORATED INTO THE PROJECT UNTIL EXPRESSLY APPROVED BY THE ENGINEER.
13. THE BUY AMERICAN WAIVER, IF REQUIRED FOR PROJECT, SHALL BE APPLIED FOR BY THE OWNER USING DATA COMPILED AND PROVIDED BY THE CONTRACTOR SHOWING DOMESTIC VERSUS NON-DOMESTIC CONTENT. THE WAIVER SHALL BE APPROVED PRIOR TO NOTICE TO PROCEED BEING ISSUED. NO DELAY MAY BE CLAIMED BY THE CONTRACTOR RELATED TO DURATION TO OBTAIN APPROVAL OF THE WAIVER.
14. NPDES PERMIT - THIS PROJECT WILL NOT DISTURB MORE THAN 1 ACRE, THEREFORE A NPDES PERMIT IS NOT REQUIRED.
15. MATERIAL CERTIFICATIONS - MATERIALS CANNOT BE INSTALLED UNTIL ALL THE MATERIAL CERTIFICATIONS FOR THAT ITEM HAVE BEEN RECEIVED, REVIEWED AND ACCEPTED BY THE RESIDENT ENGINEER. MATERIALS INSTALLED WITHOUT APPROVAL ARE SUBJECT TO REMOVAL AND REPLACEMENT AT THE CONTRACTOR'S EXPENSE.
16. CERTIFIED PAYROLLS - PAY APPLICATIONS CANNOT BE PROCESSED FOR PAYMENT UNTIL ALL CERTIFIED PAYROLLS FOR THAT PERIOD HAVE BEEN RECEIVED.

UTILITY NOTE

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123. CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

J.U.L.I.E. INFORMATION

COUNTY _____ CLAY
 CITY _____ FLORA
 TOWNSHIP _____ STANFORD
 SECTION NO. _____ 29
 ADDRESS _____ FLORA MUNICIPAL AIRPORT
 1 AIRPORT ROAD
 FLORA, ILLINOIS 62839

MAR 08, 2023 1:18 PM HAUJSM00682 I:\22\JOBS\22A0102D\CAD\AIRPORT\SHEETC-002-SQ.DWG

INDEX TO SHEETS	
SHEET NO.	SHEET TITLE
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5	SAFETY NOTES AND DETAILS
6	MARKING & CONSTRUCTION PLAN - RWY 3-21 SHEET 1
7	MARKING & CONSTRUCTION PLAN - RWY 3-21 SHEET 2
8	MARKING DETAILS

SUMMARY OF QUANTITIES				
ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	AS-BUILT QUANTITY
AR150520	MOBILIZATION	L SUM	1.0	
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	FOOT	2,000.0	
AR608510	EXISTING ASPHALT SURFACE TREATMENT	SQ YD	48,135.0	
AR608530	FRICTION TESTING	L SUM	1.0	
AR620520	PAVEMENT MARKING-WATERBORNE	SQ FT	30,783.0	
AR620525	PAVEMENT MARKING-BLACK BORDER	SQ FT	7,583.0	
AR620900	PAVEMENT MARKING REMOVAL	SQ FT	8,400.0	

MARKING QUANTITIES	
WHITE MAKRING	
DESCRIPTION	TOTAL AREA
RUNWAY 3-21 CENTERLINES	8,010
RUNWAY DESIGNATOR NUMERAL 3	634
RUNWAY DESIGNATOR NUMERAL 21	968
AIMING POINT	9,000
THRESHOLD STRIPE	10,350
TOTAL WHITE MARKING	28,962
YELLOW MARKING	
TAXIWAY LEAD-IN LINES	1,545
TAXIWAY HOLD POSITIONS	276
TOTAL YELLOW MARKING	1,821
BLACK MARKING	
RUNWAY 3-21 CENTERLINES	2,758
RUNWAY DESIGNATOR NUMERAL 3	103
RUNWAY DESIGNATOR NUMERAL 21	174
AIMING POINT	664
THRESHOLD STRIPE	1,881
TAXIWAY LEAD-IN LINES	1,545
TAXIWAY HOLD POSITIONS	458
TOTAL BLACK MARKING	7,583



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FLORA MUNICIPAL AIRPORT

1 AIRPORT ROAD
 FLORA, ILLINOIS 62839

MAINTAIN AND REMARK RUNWAY 3-21 AND OTHER AIRFIELD PAVEMENTS

IDA No: FOA-4928

SBG No:
 3-17-SBGP-171/184

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: 11/18/22
 PROJECT NO: 22A0102D
 CAD FILE: G-002-SQ.DWG
 DESIGN BY: NLD 10/21/2022
 DRAWN BY: NLD 10/21/2022
 REVIEWED BY: LDH 11/17/22

SHEET TITLE

SUMMARY OF QUANTITIES AND INDEX OF SHEETS

CONSTRUCTION SAFETY PLAN

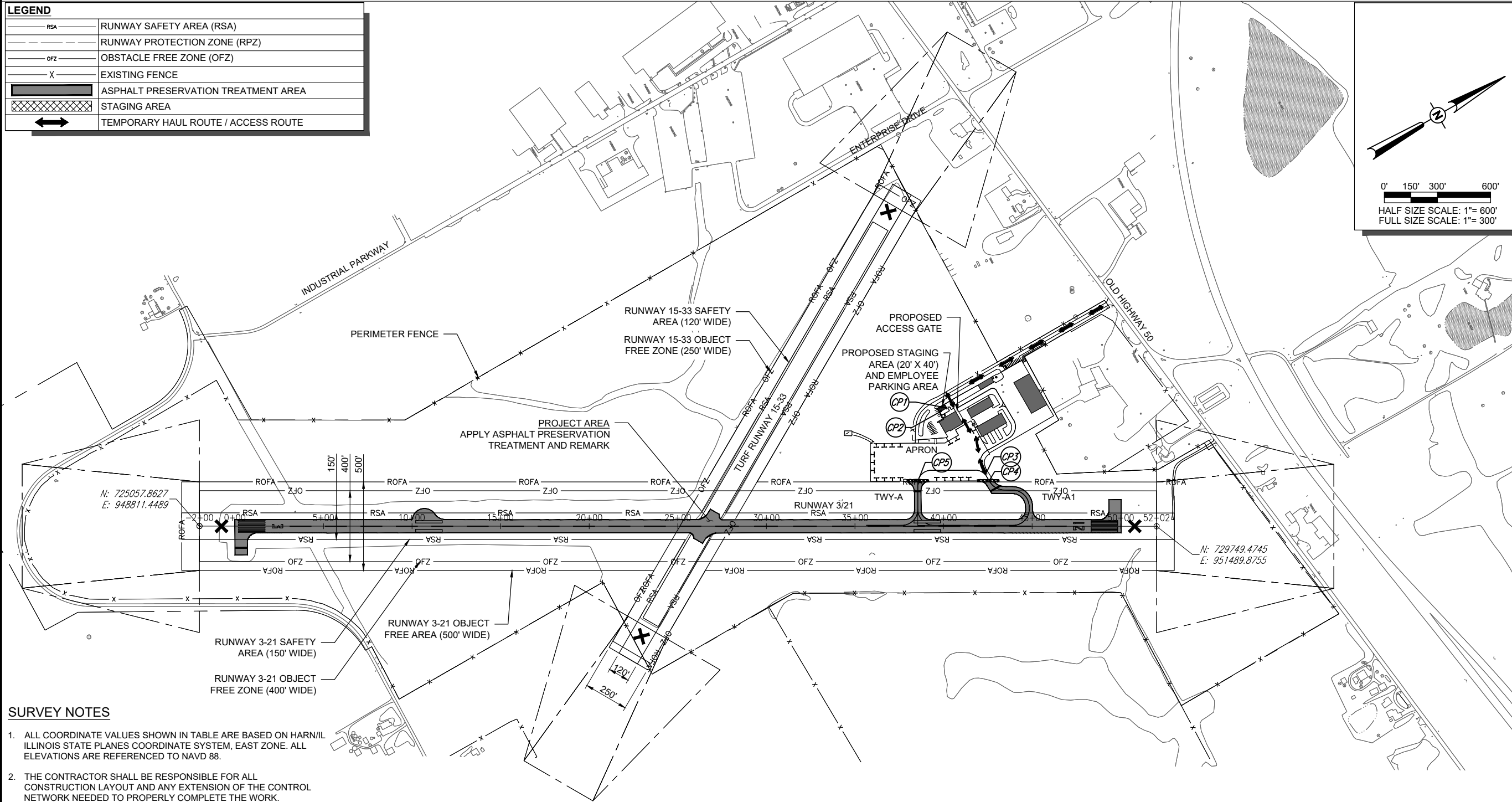
- SCOPE OF WORK - THE PROPOSED WORK CONSISTS OF APPLYING AN ASPHALT SURFACE TREATMENT TO EXISTING ASPHALT PAVEMENT RUNWAY 3-21, TAXIWAY A AND A1. WORK WILL ALSO INCLUDE PAVEMENT MARKING, MARKING REMOVAL AS NEEDED, AND CRACK SEALING.
- PROJECT SCHEDULE - DUE TO AIRPORT ACTIVITY CONSTRUCTION START WILL BE DELAYED UNTIL AFTER LABOR DAY 2023.
- THE AIRPORT WILL BE CLOSED DURING CONSTRUCTION. BOTH RUNWAY 3-21 AND 15-33 WILL BE CLOSED.
- GENERAL - THE FLORA AIRPORT IS COMPRISED OF TWO RUNWAYS: A SOD RUNWAY 15-33 AND A PAVED RUNWAY 3-21. THIS PROJECT REQUIRES THE CONTRACTOR TO LIMIT THE EQUIPMENT HEIGHT TO 25' WHILE WORKING UNDER THE RUNWAY APPROACHES.
- IDENTIFICATION - WHEN THE CONTRACTORS VEHICLES AND EQUIPMENT ARE ON THE AIRPORT PROPERTY THEY SHALL BE PROPERLY MARKED WITH THREE (3') FOOT SQUARE CHECKERED FLAGS (INTERNATIONAL ORANGE AND WHITE). THE CONTRACTOR WILL ALSO PROVIDE WORKERS WITH SOME TYPE OF TAG OR GARMENT TO IDENTIFY THE PERSON AS BEING PART OF THE CONSTRUCTION CREW.
- RADIO CONTROL - THE CONTRACTOR WILL BE REQUIRED TO BE IN TWO-WAY RADIO CONTACT (122.70 MHz.) WITH THE AIRPORT UNICOM. THIS WILL KEEP THE CONTRACTOR IN CONSTANT CONTACT WITH THE FLORA AIRPORT AND ENABLE THE AIRPORT TO IMMEDIATELY CONTACT

THE CONTRACTOR IN CASE OF AN AERONAUTIC EMERGENCY THAT WOULD REQUIRE ACTION BY THE CONTRACTOR AND/OR HIS PERSONNEL.

- AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. ONLY CONTRACTOR EMPLOYEES SHALL BE ALLOWED WITHIN THE PROJECT LIMITS. GATES SHALL BE CLOSED AT ALL TIMES UNLESS THE CONTRACTOR IS IN A CONTINUOUS HAULING OPERATIONS, DURING WHICH TIME HE WILL PROVIDE A PERSON TO MONITOR THE GATE AREA.
- THE CONTRACTOR WILL USE THE AGGREGATE DRIVE AND ENTRANCE TO THE PROPERTY AS THE DESIGNATED HAUL ROUTE. THE CONTRACTOR SHALL PARK EQUIPMENT AND PLACE MATERIALS WITHIN THE LIMITS OF THE SUBJECT PROPERTY. THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN THE PROPOSED HAUL ROUTE AND SUBJECT PROPERTY THROUGHOUT THE COURSE OF THE PROJECT. ANY AREAS DAMAGED OUTSIDE OF THESE AREAS WILL BE REPAIRED BY THE CONTRACTOR AND AT THE CONTRACTOR'S OWN EXPENSE. AT THE CONCLUSION OF THE PROJECT THE CONTRACTOR WILL RESTORE THE HAUL ROUTE AND PARKING AREA TO ITS ORIGINAL STATE. RESTORATION OF THE HAUL ROUTE AND ANY AREAS USED FOR PARKING WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ALL CONSTRUCTION EQUIPMENT ON THE AIRPORT SHALL BE MARKED, LIGHTED AND/OR FLAGGED IN ACCORDANCE WITH AC 150/5210-5 (CURRENT EDITION) AND 70/7460-1 (CURRENT EDITION).

CONTROL POINTS					
POINT #	DESCRIPTION	LATITUDE	LONGITUDE	GROUND (MSL)	HEIGHT (AGL)
1	CONST. EQUIP. 1	N038° 40' 07.59"	W088° 27' 07.77"	471'	25'
2	CONST. EQUIP. 2	N038° 40' 07.19"	W088° 27' 07.77"	471'	25'
3	HAUL ROUTE	N038° 40' 07.67"	W088° 27' 02.33"	467'	25'
4	BARRICADE 1	N038° 40' 07.63"	W088° 27' 01.77"	467'	2'
5	BARRICADE 2	N038° 40' 04.18"	W088° 27' 04.28"	463'	2'

LEGEND	
— RSA —	RUNWAY SAFETY AREA (RSA)
— RPZ —	RUNWAY PROTECTION ZONE (RPZ)
— OFZ —	OBSTACLE FREE ZONE (OFZ)
X	EXISTING FENCE
▨	ASPHALT PRESERVATION TREATMENT AREA
▩	STAGING AREA
↔	TEMPORARY HAUL ROUTE / ACCESS ROUTE



SURVEY NOTES

- ALL COORDINATE VALUES SHOWN IN TABLE ARE BASED ON HARN/IL ILLINOIS STATE PLANES COORDINATE SYSTEM, EAST ZONE. ALL ELEVATIONS ARE REFERENCED TO NAVD 88.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CONSTRUCTION LAYOUT AND ANY EXTENSION OF THE CONTROL NETWORK NEEDED TO PROPERLY COMPLETE THE WORK.



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FLORA MUNICIPAL AIRPORT

1 AIRPORT ROAD
FLORA, ILLINOIS 62839

MAINTAIN AND REMARK RUNWAY 3-21 AND OTHER AIRFIELD PAVEMENTS

IDA No: FOA-4928

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NO.	DATE	DESCRIPTION		
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SHEET TITLE

SCOPE OF WORK PLAN

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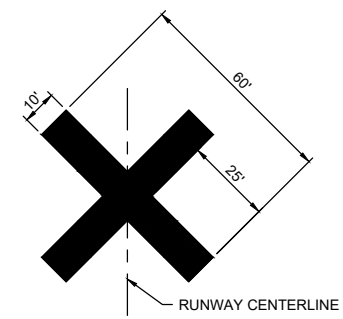
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SHEET TITLE

SAFETY NOTES

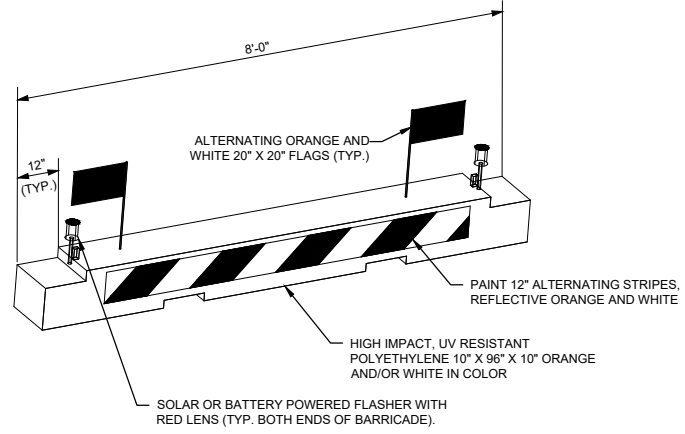
- ALL PROVISIONS OF THE LATEST EDITION OF FAA ADVISORY CIRCULAR AC 150/5370-2 (CURRENT EDITION), "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", APPLY TO THIS CONTRACT, EXCEPT AS MODIFIED BY THIS SAFETY PLAN. ANY FURTHER MODIFICATIONS REQUESTED MUST BE APPROVED BY THE AIRPORT, RESIDENT ENGINEER/TECHNICIAN AND THE FAA.
- THE CONTRACTORS SHALL MINIMIZE DISRUPTION OF STANDARD OPERATING PROCEDURES FOR AERONAUTICAL ACTIVITY BY REMAINING WITHIN THE PRESCRIBED STAGING, CONSTRUCTION, AND PHASING AREAS PRESENTED ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEETS.
- NO UNAUTHORIZED PERSONNEL SHALL ENTER ANY AREA OF THE AIRPORT THAT COULD POTENTIALLY BE HAZARDOUS. THE AIRPORT MANAGER RESERVES THE RIGHT TO SUSPEND OPERATIONS IN ORDER TO MAINTAIN SAFETY AT THE AIRPORT.
- NO CONSTRUCTION VEHICLES SHALL BE DRIVEN ACROSS ANY ACTIVE AIRFIELD PAVEMENT AREA WITHOUT AN APPROPRIATE ESCORT. CONSTRUCTION EQUIPMENT OR CONSTRUCTION ACTIVITY WILL NOT BE PERMITTED WITHIN 250' OF ANY ACTIVE RUNWAY CENTERLINE OR WITHIN 131' OF ANY OTHER ACTIVE AIRPORT TAXIWAY OR APRON.
- CONTRACTOR EQUIPMENT, VEHICLES, AND PROJECT MATERIALS SHALL BE STORED AT THE STAGING AREA SHOWN ON THE PLAN VIEW, EXCEPT AS OTHERWISE PROVIDED FOR AT THE PRECONSTRUCTION CONFERENCE.
- ALL CONSTRUCTION EQUIPMENT OPERATING IN THE PRESCRIBED CONSTRUCTION AREA IS REQUIRED TO DISPLAY A CHECKERBOARD FLAG PROPERLY LOCATED OR A ROTATING BEACON (STROBE) AS SPECIFIED IN AC 150/5210-5, "PAINTING, MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT" LATEST EDITION.
- NO CONSTRUCTION MATERIAL STOCKPILES SHALL BE LOCATED WITHIN 500' OF ANY ACTIVE RUNWAY, WITHIN 131' OF ANY OTHER ACTIVE AIRPORT OPERATIONS AREA, OR PENETRATE A PART 77 IMAGINARY SURFACE (PROVIDED BY THE RESIDENT ENGINEER/TECHNICIAN) EXTENDING OUT AND UPWARDS FROM ALL SIDES OF AN ACTIVE RUNWAY.
- CLOSED AIRFIELD PHASING AREAS, OPEN TRENCHES, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH LIGHTED BARRICADES WITH STEADY BURNING OR FLASHING RED LIGHTS AS SPECIFIED IN 150/5370-2, "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION", LATEST EDITION. LIGHTED BARRICADES MUST BE NO TALLER THAN 18" (EXCLUSIVE OF SUPPLEMENTARY LIGHTS AND FLAGS) ON THE TAXIWAYS AND COMPLY WITH ADVISORY CIRCULAR 150/5370-2, LATEST EDITION. CONTRACTOR SHALL NIGHT CHECK BARRICADES DAILY FOR PROPER OPERATION.
- OPEN TRENCHES, EXCAVATIONS, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHOULD BE PROMINENTLY MARKED WITH ORANGE FLAGS AND LIGHTED WITH FLASHING RED LIGHTS DURING HOURS OF RESTRICTED VISIBILITY AND/OR DARKNESS.
- NO CONSTRUCTION EQUIPMENT GREATER THAN 25' TALL WILL BE PERMITTED ON THE AIRPORT WITHOUT THE APPROVAL OF THE AIRPORT MANAGER AND ADDITIONAL AIRSPACE APPROVAL BY THE FAA. AIRSPACE APPROVALS REQUIRE CONSIDERABLE LEAD TIME AND SHOULD BE REQUESTED WELL IN ADVANCE.
- NO OPEN FLAME WELDING OR TORCH CUTTING OPERATION IS PERMITTED UNLESS ADEQUATE FIRE AND SAFETY PRECAUTIONS ARE PROVIDED AND HAVE BEEN APPROVED BY THE AIRPORT MANAGER NO FLARE POTS ARE ALLOWED ON THE PROJECT.
- SOIL, DEBRIS, AND LOOSE MATERIAL DROPPED OR TRUCKED ONTO AIRPORT ROADS, TAXIWAYS, AND SOD SURFACES, OR WHICH CAN BE BLOWN ONTO SUCH SURFACES, SHALL BE IMMEDIATELY SWEEPED, PICKED UP AND REMOVED, OR PLACED INTO CLOSED CONTAINERS. ANY DAMAGE TO AIRPORT PROPERTY SHALL BE REPAIRED IMMEDIATELY AT NO COST TO THE OWNER.
- CONTRACTOR SHALL TAKE MEASURES TO AVOID TRACKING ASPHALT MATERIALS ONTO ADJACENT PAVEMENT AREAS, UNLESS SUFFICIENT PROTECTION HAS BEEN APPLIED. HEAVY TRACKING OR DAMAGE TO ADJACENT PAVEMENTS MAY BE CAUSE FOR STOPPING THE WORK UNTIL ACCEPTABLE PROTECTION OR CHANGE IN WORK METHODS HAS BEEN PROVIDED.
- EACH CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND MAINTAINING AIRPORT LIGHTING AND NAVIGATIONAL ELECTRICAL SYSTEMS DURING CONSTRUCTION. A CONTACT PERSON AND TELEPHONE NUMBER FOR 24 HOUR EMERGENCY IMMEDIATE REPAIR SHALL BE SUBMITTED TO THE AIRPORT MANAGER AND RESIDENT ENGINEER/TECHNICIAN. HAUL ROUTES CROSSING PAVEMENT, DRAINAGE, MISCELLANEOUS. STRUCTURES AND/OR AIRFIELD CABLES SHALL BE PROTECTED FROM DAMAGE.
- ALL AIRCRAFT AND AIRPORT OPERATIONS HAVE THE RIGHT-OF-WAY. CONTRACTOR TO YIELD TO VEHICLES AND REMAIN CLEAR AT ALL TIMES.

- CONTRACTOR SHALL PLACE, SECURE, AND MAINTAIN LIGHTED BARRICADES AND CLOSURE CROSSES WHEN A RUNWAY/TAXIWAY/APRON IS CLOSED OR AS REQUIRED BY THE PLANS AND DESIGNATED BY THE RESIDENT ENGINEER/TECHNICIAN.
- CONTRACTOR SHALL MARK HAZARDOUS AREA WITH STEADY-BURNING OR FLASHING RED LIGHTS DURING PERIODS OF LOW VISIBILITY AS REQUIRED.
- THE CONTRACTOR SHALL PERIODICALLY PERFORM ONSITE INSPECTIONS THROUGHOUT THE DURATION OF THE PROJECT WITH THE IMMEDIATE REMEDY OF ANY DIFFERENCES, WHETHER CAUSED BY NEGLIGENCE, OVERSIGHT, OR PROJECT SCOPE CHANGE.
- CONTRACTOR SHALL MOVE MAINTENANCE OF TRAFFIC COMPONENTS AT THE WRITTEN DIRECTION OF THE RESIDENT ENGINEER/TECHNICIAN AT NO ADDITIONAL COST.
- CONTRACTOR SHALL NOT REMOVE THE BARRICADES WITHOUT THE APPROVAL BY THE RESIDENT ENGINEER/TECHNICIAN.
- CONTRACTOR SHALL MAINTAIN FLASHERS, SIGNS AND/OR BARRICADES AS REQUIRED BY THE PLANS, CITY OR COUNTY REGULATIONS OR CONTRACTOR ACTIVITIES. CONTRACTOR SHALL OBTAIN ANY AND ALL REQUIRED LOCAL PERMITS UNLESS SPECIFIED OTHERWISE.
- THE CONTRACTOR SHALL UTILIZE WATER AND/OR CHEMICALS APPROVED BY THE RESIDENT ENGINEER/TECHNICIAN AS NECESSARY TO CONTROL DUST.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR IMPLEMENTING MEASURES TO CONTROL OR AVOID CREATING ATTRACTANTS TO WILDLIFE. MEASURES MAY INCLUDE CONTINUOUSLY REMOVING ANY WASTE OR LOOSE MATERIALS, PLACEMENT OF MATERIALS IN APPROPRIATE STORAGE CONTAINERS, PROPERLY MAINTAINING FENCES AND GATES TO PREVENT ACCESS, AND PREVENTING PONDING OF WATER THROUGHOUT THE SITE.
- UNLESS SPECIFIED OTHERWISE, COST FOR SAFETY, STAGING, AND TRAFFIC MAINTENANCE ITEMS IS TO BE CONSIDERED INCIDENTAL TO THE PROJECT. SEPARATE PAYMENT SHALL NOT BE MADE.
- THE CONTRACTOR SHALL HAVE THE SAFETY PLAN COMPLIANCE DOCUMENT (SPCD), AS DETAILED IN THE SPECIAL PROVISIONS, SUBMITTED AND APPROVED PRIOR TO BEING ISSUED "NOTICE TO PROCEED".
- ALL RUNWAY/TAXIWAY CLOSURES SHALL BE COORDINATED WITH AIRPORT MANAGEMENT A MINIMUM OF 7 DAYS BEFORE THE DESIRED CLOSING TIME TO ALLOW FOR THE PROPER COORDINATION. AIRPORT MANAGEMENT HAS COMPLETE AUTHORITY IN DETERMINING WHEN THE RUNWAY/TAXIWAY MAY BE CLOSED.
- RUNWAY/TAXIWAY CLOSURE PROCEDURES:
 - CONTACT THE AIRPORT MANAGEMENT OR ASSIGNED REPRESENTATIVE A MINIMUM OF 7 DAYS BEFORE THE DESIRED CLOSING TIME.
 - ISSUANCE OF NOTAM AND DEACTIVATION OF THE APPLICABLE AIRFIELD LIGHTING AND NAVAIDS BY THE AIRPORT MANAGEMENT AND/OR FAA.
 - PLACEMENT OF CROSSES AND BARRICADES.
 - ONLY AT THE TIME THAT ALL OF THE ABOVE ARE COMPLETED MAY ANY CONSTRUCTION OPERATIONS BEGIN WITHIN THE RUNWAY/TAXIWAY AIR OPERATIONS AREA.
- RUNWAY/TAXIWAY RE-OPENING PROCEDURES:
 - ENSURE ALL PERSONNEL, EQUIPMENT AND MATERIALS ARE CLEAR OF THE AIR OPERATIONS AREA.
 - INSPECT THE AREA FOR LOOSE OR TRACKED DEBRIS, PAVEMENT DROP-OFFS, AND OPEN TRENCHES.
 - CONTACT AIRPORT MANAGEMENT OR REPRESENTATIVE FOR FINAL INSPECTION OF THE AREA.
 - REMOVE BARRICADES AND CROSSES.
 - ACTIVATION OF THE AIRFIELD LIGHTING AND NAVAIDS AND CANCELLATION OF THE NOTAM BY THE AIRPORT MANAGEMENT AND/OR FAA.



- TEMPORARY "CLOSED RUNWAY" MARKINGS SHALL BE "AVIATION YELLOW"
- TEMPORARY "CLOSED RUNWAY" MARKINGS SHALL BE CONSTRUCTED OF PLYWOOD, DOUBLE-LAYERED SNOW FENCE OR APPROVED FABRIC AND SHALL BE SECURED TO PAVEMENT BY SANDBAGS OR OTHER APPROVED METHOD.
- TEMPORARY "CLOSED RUNWAY" MARKINGS SHALL BE PLACED OVER THE RUNWAY DESIGNATION NUMBERS UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN.
- COST FOR PROVIDING, PLACING, MAINTAINING, RELOCATING AND REMOVING "CLOSED RUNWAY" MARKINGS SHALL BE INCLUDED AS AN INCIDENTAL COST TO THE CONTRACT, UNLESS OTHERWISE NOTED.

TEMPORARY CLOSURE CROSS DETAIL
NOT TO SCALE



LOW PROFILE AIRCRAFT BARRICADE DETAIL

BARRICADE NOTES

- ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SUPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE. THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN.
- BARRICADES SHALL BE INTERLOCKED END TO END OVER THE LENGTH OF THE PAVEMENT WHERE PROTECTING OPEN RUNWAYS, AND SPACED END TO END A MAXIMUM OF 4 FEET IN OTHER ALL OTHER AREAS. BARRICADES ARE TO BE SET BACK FROM THE ACTIVE RUNWAY OR TAXIWAY CENTERLINE THE DISTANCE AS SHOWN ON THE PLANS.
- CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY MAY BE USED IN EITHER A STEADY BURN (TYPE C) OR LOW INTENSITY FLASHING MODE (TYPE A) UNLESS NOTED OTHERWISE.
- THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR AFTER SUNSET AND 1/2 HOUR BEFORE SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION.
- BARRICADES SHALL BE SECURED TO THE GROUND BY APPROVED METHODS TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS.
- THE ONLY COLOR COMBINATION ON BARRICADES IS ORANGE AND WHITE. THE ORANGE STRIPES SHALL BE ENCAPSULATED LENS REFLECTIVE SHEETING. THE WHITE STRIPES SHALL BE EITHER ENCAPSULATED OR ENCLOSED LENS REFLECTIVE SHEETING AND MUST BE IN ACCEPTABLE CONDITION.
- COST FOR PROVIDING, PLACING, MAINTAINING, RELOCATING AND REMOVING BARRICADES SHALL BE INCLUDED AS AN INCIDENTAL COST TO THE CONTRACT, UNLESS OTHERWISE NOTED.

NO.	DATE	DESCRIPTION		
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CAD FILE: C-101-CON.DWG

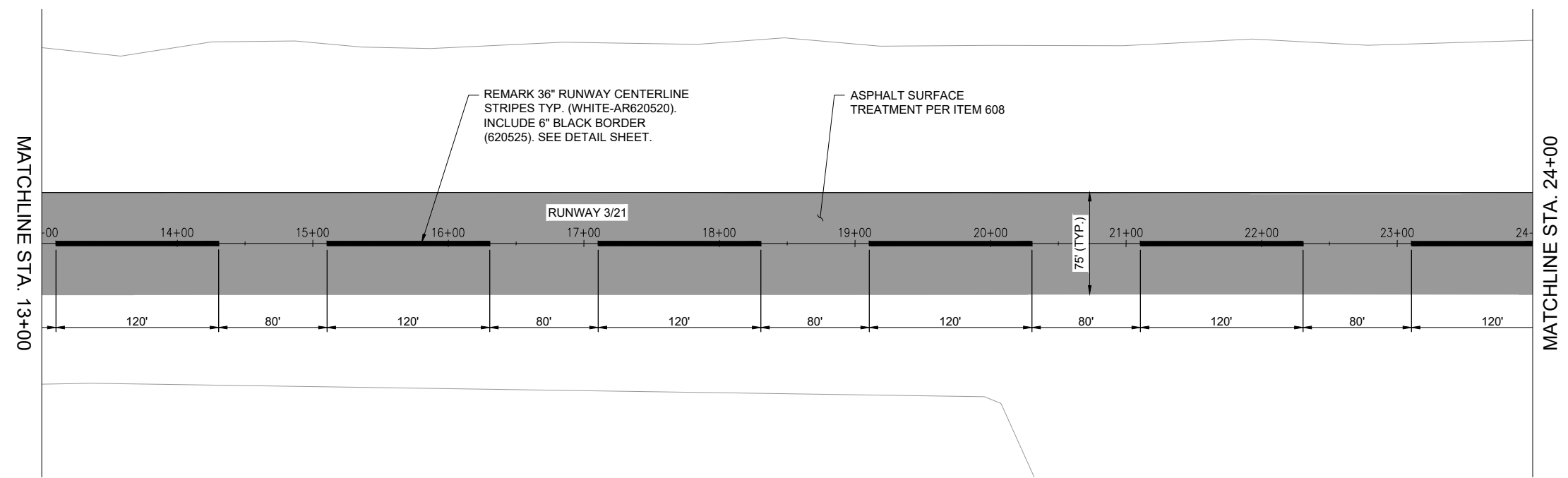
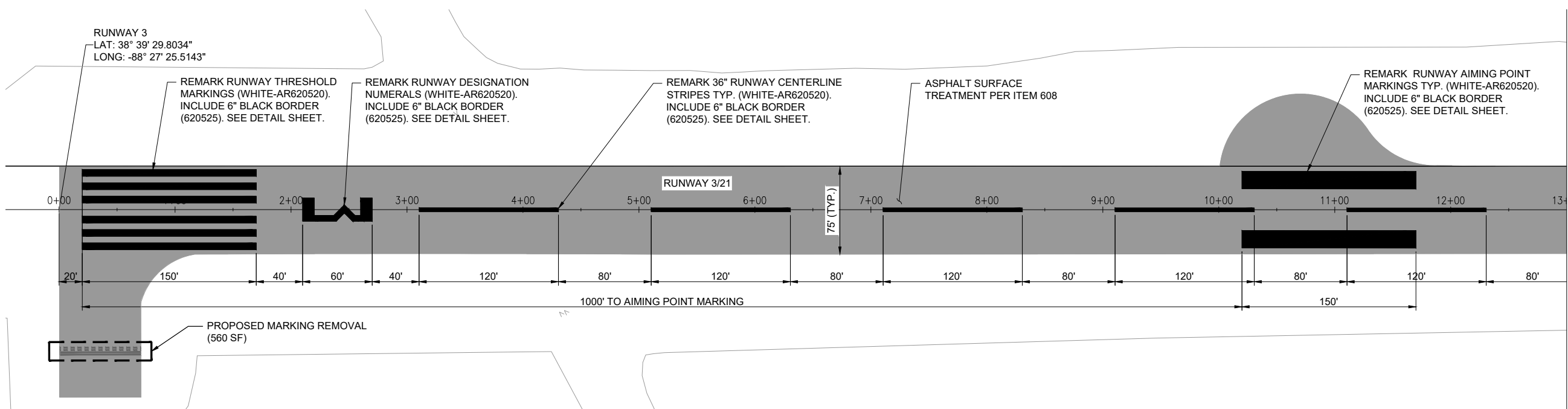
DESIGN BY: CWS 3/15/2019

DRAWN BY: CWS 3/15/2019

REVIEWED BY:

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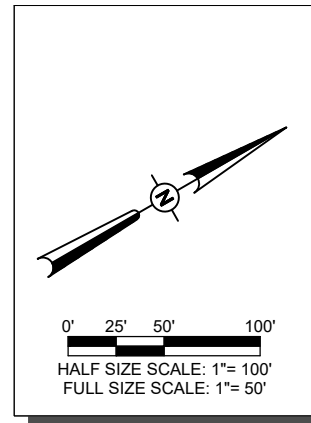
MARKING &
CONSTRUCTION
PLAN - RWY 3-21
SHEET 1



CONSTRUCTION NOTES

1. THE EXISTING ASPHALT SURFACE TREATMENT, INCLUDING FRICTION TESTING, SHALL BE COMPLETED IN ACCORDANCE WITH ITEM 608 OF THE SPECIAL PROVISIONS.
2. ALL PAVEMENT MARKING SHALL BE REMARKED TO MATCH EXISTING, EXCEPT RUNWAY END 21 THRESHOLD BAR MARKING. THE EXISTING RUNWAY END 21 THRESHOLD BARS SHALL BE REMOVED AND REMARKED.
3. ALL PAVEMENT MARKING SHALL BE WATERBORNE AND COMPLETED IN ACCORDANCE WITH ITEM 620 OF THE SPECIAL PROVISIONS.
4. ALL MARKING, EXCEPT BLACK BORDER, WILL BE APPLIED WITH REFLECTIVE MEDIA IN THE SECOND APPLICATION IN ACCORDANCE WITH THE SPECIFICATIONS.
5. ALL PROPOSED MARKING ITEMS WILL BE PAID FOR AS FOLLOWS: AR620520 "PAVEMENT MARKING-WATERBORNE" _____ PER SQ. FT. AND AR620525 PAVEMENT MARKING - BLACK BORDER _____ PER SQ. FT.
6. THE CONTRACTOR SHALL PROTECT, BY ANY MEANS NECESSARY, ALL ADJACENT BUILDINGS, CONCRETE PADS, LIGHT FIXTURES, ETC. TO PREVENT ACCIDENTAL OVERSPRAY WHEN APPLYING ASPHALT SURFACE TREATMENT. ANY OVERSPRAY ON ADJACENT OBJECTS SHALL BE CLEANED IMMEDIATELY.
7. THE ASPHALT SURFACE TREATMENT WILL REQUIRE A 30 DAY CURE TIME BEFORE THE 2ND COAT OF PAVEMENT MARKING IS APPLIED. CONTRACT TIME WILL BE SUSPENDED OVER THAT PERIOD AFTER THE FIRST COAT HAS BEEN APPLIED AND THE FRICTION TESTING HAS BEEN COMPLETED.
8. CUT-OFF SHEETS WILL BE REQUIRED TO ENSURE STRAIGHT ENDS OF PAVEMENT MARKINGS AND ASPHALT SURFACE TREATMENT LIMITS.
9. LOCATIONS OF THE BITUMINOUS CRACKS TO BE CLEANED AND SEALED (AR201661) ARE NOT SHOWN AND WILL BE LAID OUT AND MEASURED IN THE FIELD BY THE RESIDENT ENGINEER/TECHNICIAN UPON THE START OF CONSTRUCTION.

PROPOSED	LEGEND
	PAVEMENT MARKING (AR620520, AR620525)
	ASPHALT SURFACE TREATMENT (AR608510)

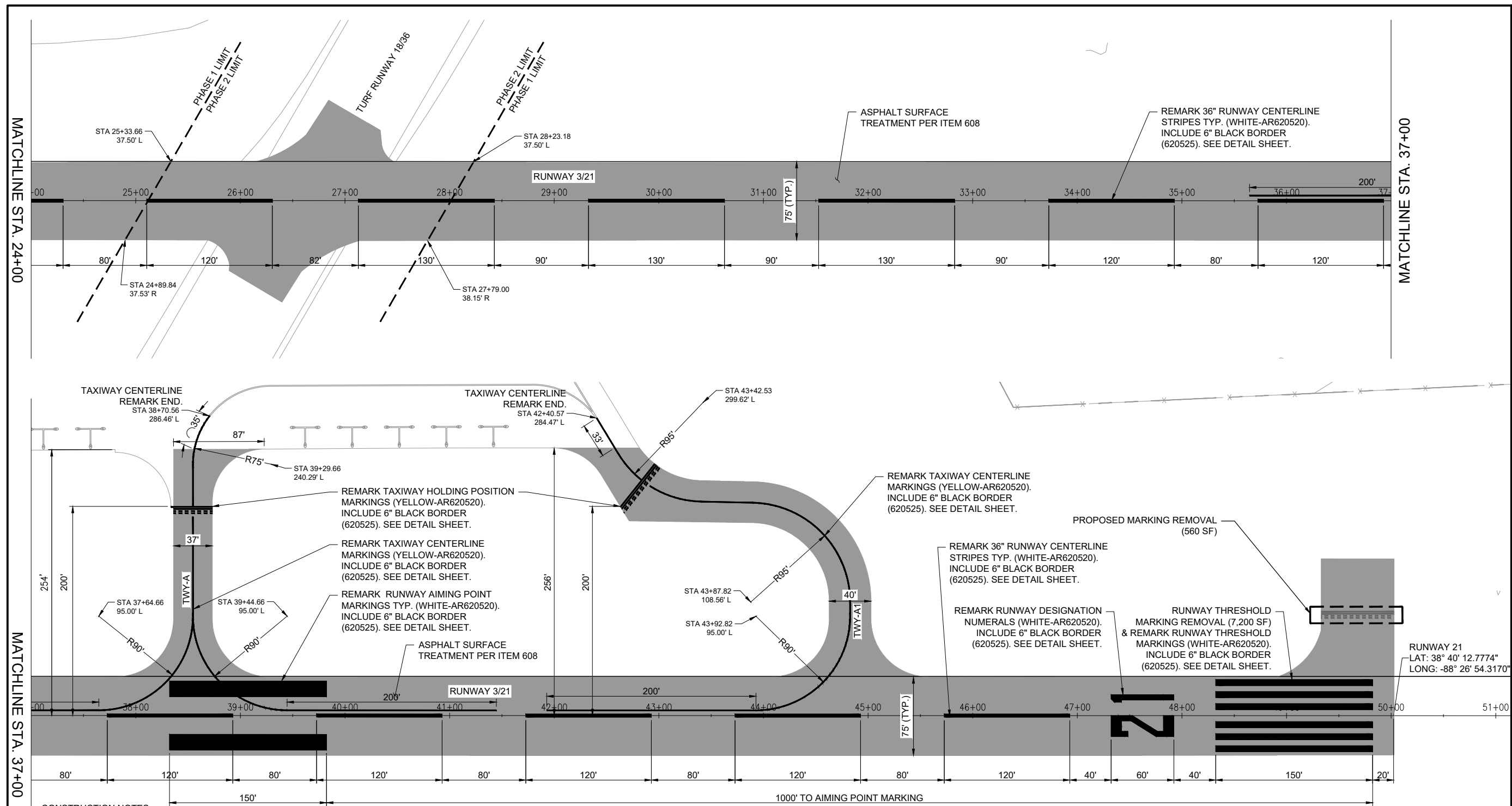


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NO.	DATE	DESCRIPTION		
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ISSUE: 11/18/22
PROJECT NO: 22A0102D
CAD FILE: C-101-CON.DWG
DESIGN BY: CWS 3/15/2019
DRAWN BY: CWS 3/15/2019
REVIEWED BY:

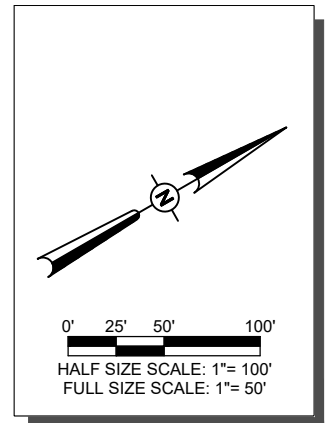
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CONSTRUCTION NOTES

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PROPOSED	LEGEND
	PAVEMENT MARKING (AR620520, AR620525)
	ASPHALT SURFACE TREATMENT (AR608510)



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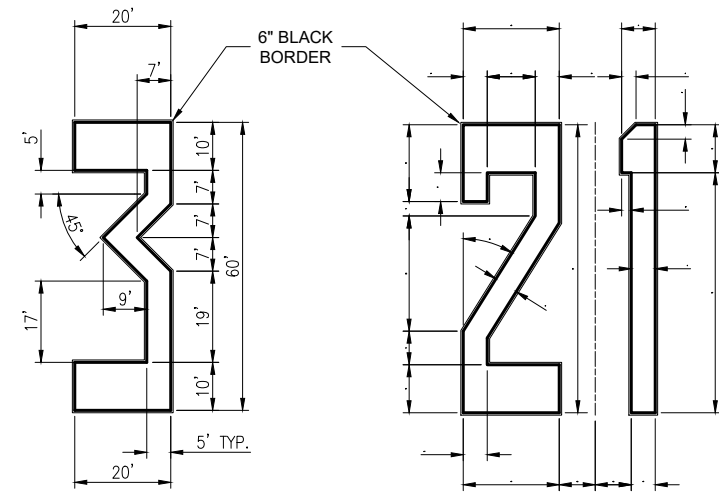
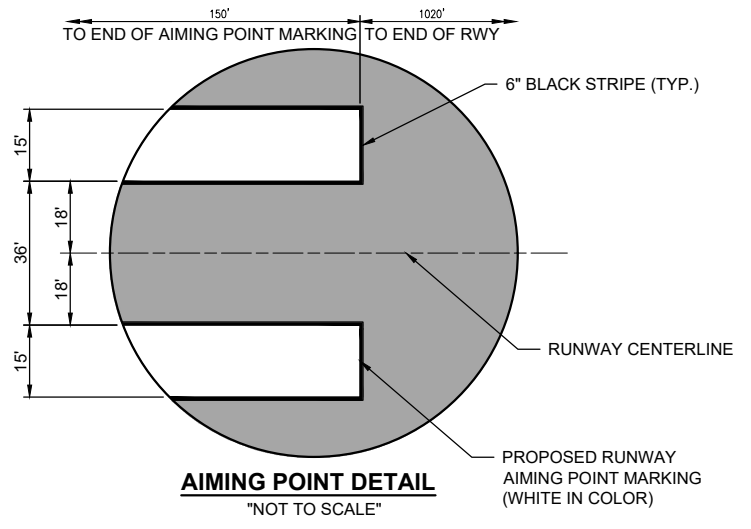
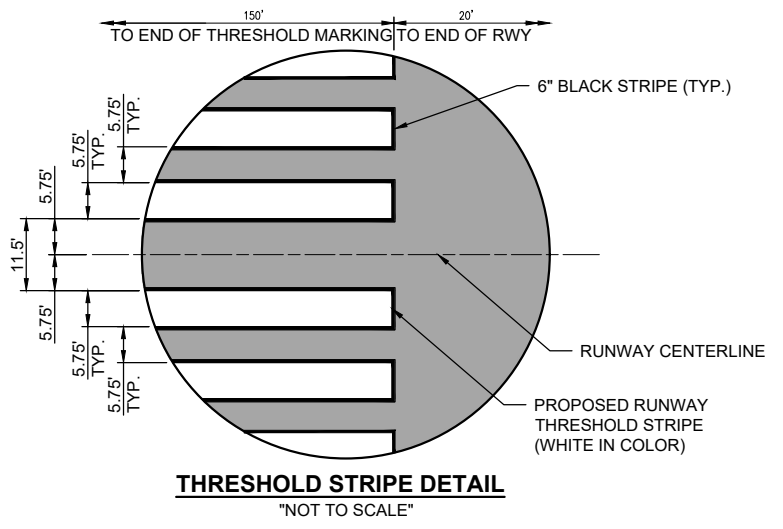
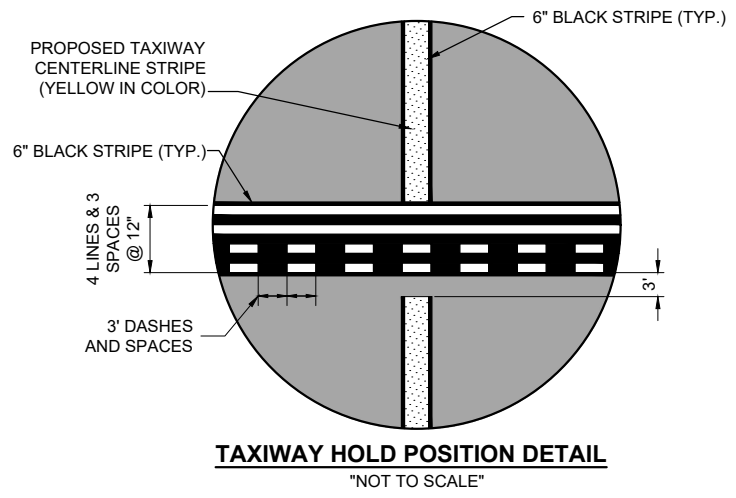
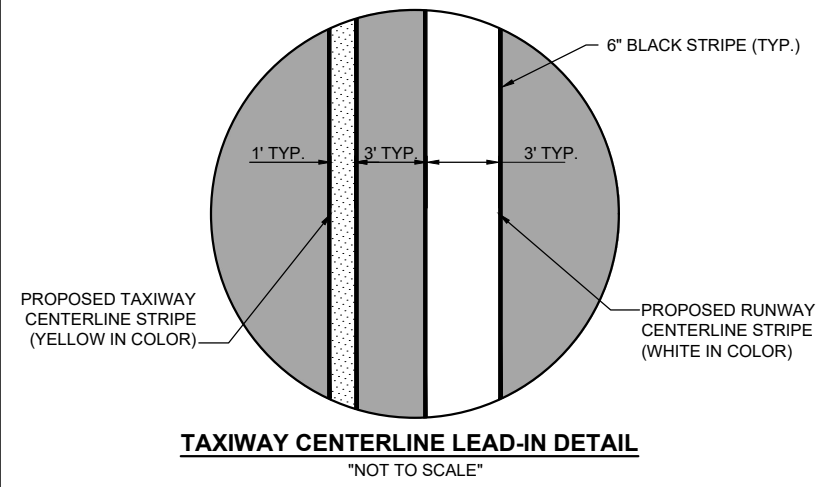
ISSUE: 11/18/22
PROJECT NO: 22A0102D
CAD FILE: C-501-MRK.DWG
DESIGN BY: NLD OCTOBER 21, 2022
DRAWN BY: NLD OCTOBER 21, 2022
REVIEWED BY: LDH 11/17/22

SHEET TITLE

MARKING DETAILS

EXISTING MARKING REMOVAL NOTES:

1. THE AREAS THAT ARE DESIGNATED EXISTING MARKING (TO BE REMOVED) WILL BE REMOVED AS DESCRIBED IN THE SPECIAL PROVISIONS.
2. ALL AREAS TO BE REMOVED ARE CALCULATED AREAS. ANY ADDITIONAL AREAS, DUE TO OVER SPRAY, SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
3. THE PROPOSED MARKING REMOVAL SHALL BE PAID FOR UNDER ITEM: AR620900 "PAVEMENT MARKING REMOVAL" *** S.F.



WHITE 634 S.F.
BLACK 102 S.F.

WHITE 967 S.F.
BLACK 172 S.F.

NUMERAL 3
"NOT TO SCALE"

NUMERAL 21
"NOT TO SCALE"

RUNWAY NUMERAL DIMENSIONING NOTES:

1. ALL DIMENSIONS ARE EXPRESSED IN INCREMENTS OF FEET.
2. DOUBLE DIGIT NUMERAL DESIGNATIONS ARE CENTERED ON THE RUNWAY PAVEMENT CENTERLINE BASED ON THE CENTER OF THE OUTER EDGES OF THE TWO NUMERALS.
3. CUT-OFF SHEETS WILL BE REQUIRED TO INSURE STRAIGHT EDGES.

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