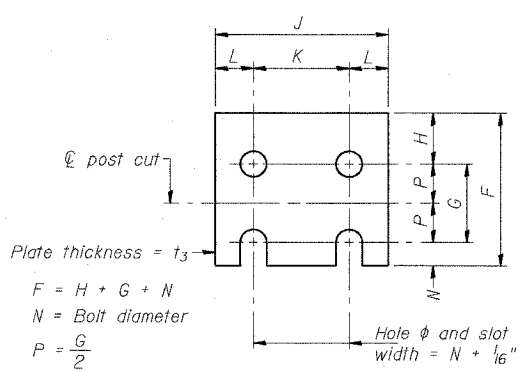
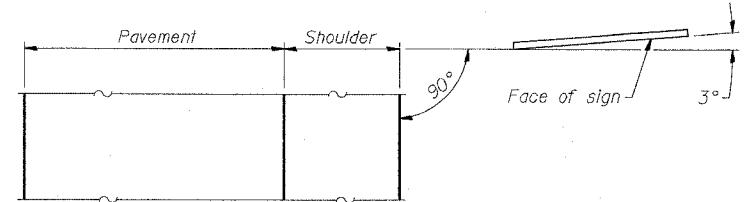
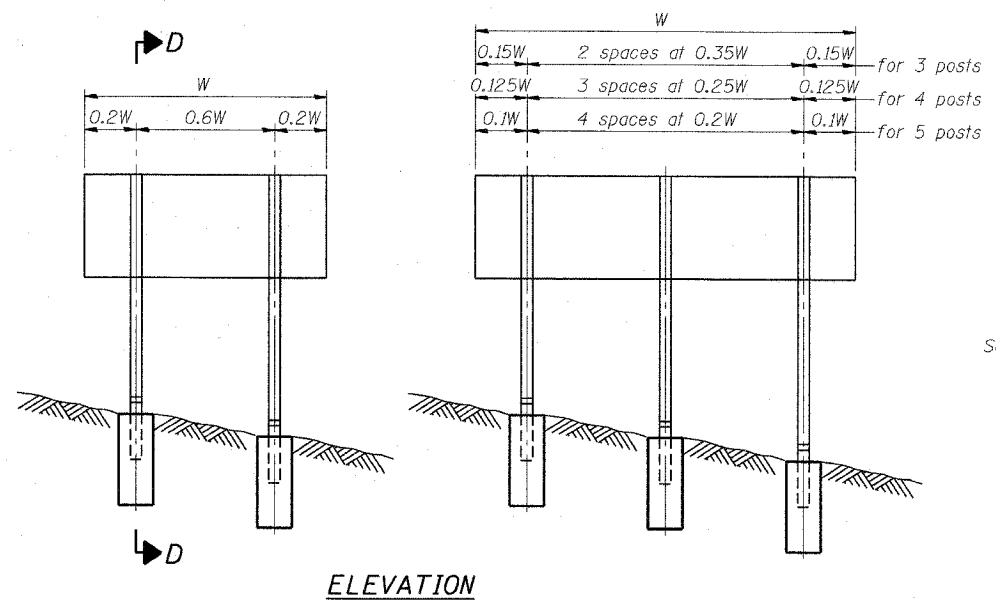
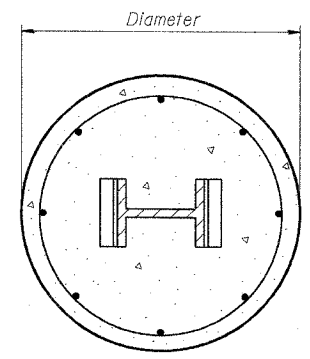
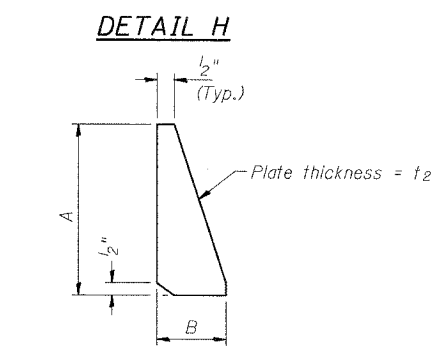
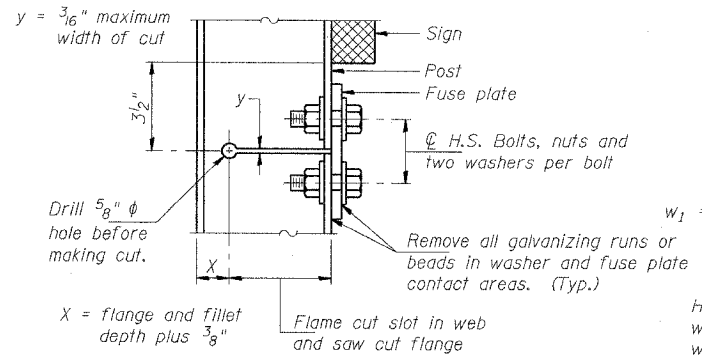
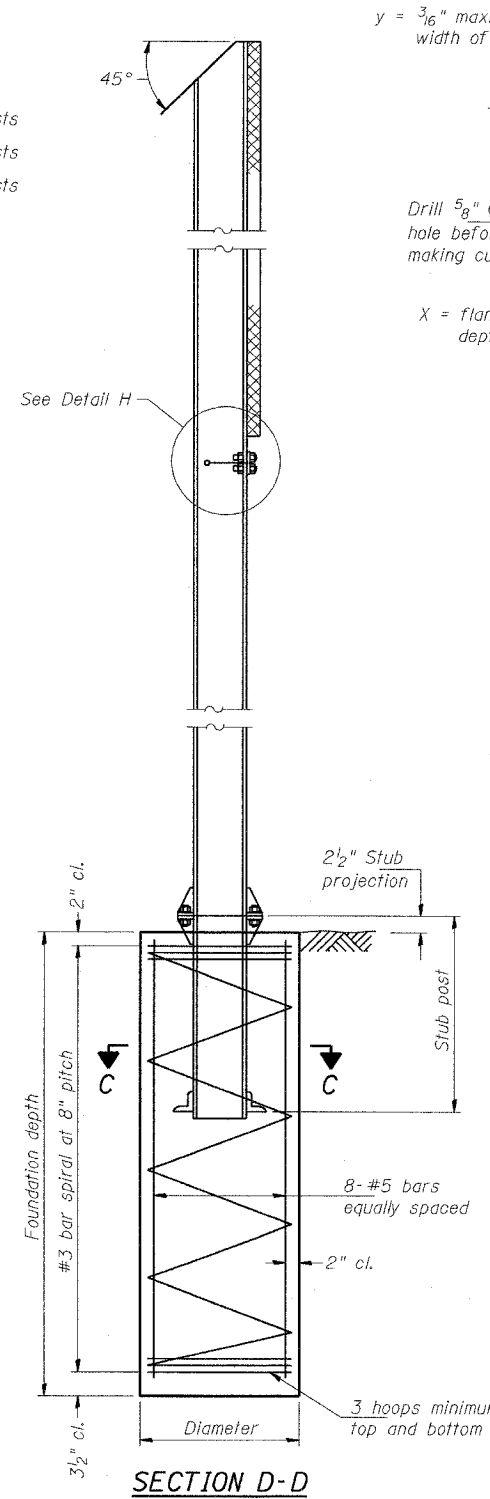


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. RTE. 90/94	2003-0311	COOK	976	469
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT-	62583	



N = Bolt Diameter	G	H
1/2"	2"	1 1/8"
5/8"	2 1/4"	1 1/4"
3/4"	2 1/2"	1 3/8"
7/8"	2 3/4"	1 1/2"
1"	3"	1 5/8"
1 1/8"	3 1/4"	1 3/4"



GENERAL NOTES

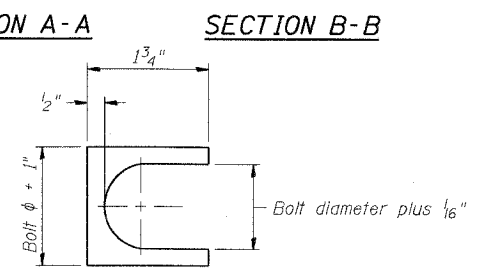
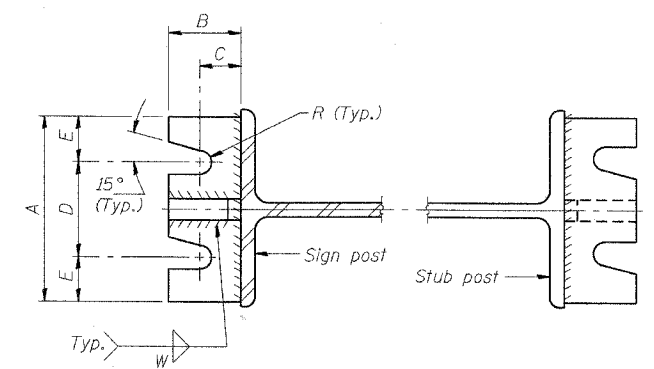
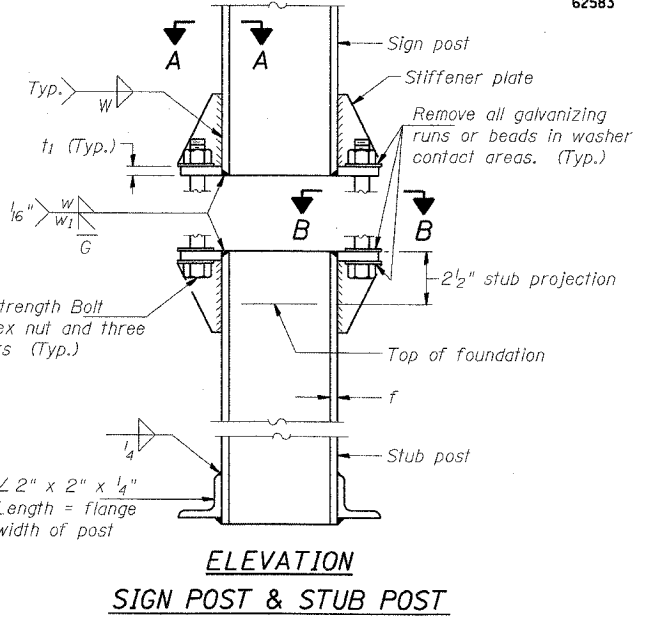
Posts shall be plumbed by using shims with post-to-stub post connection bolts snug tight only. Final tightening of all High Strength Bolts shall be in accordance with Article 505.04(f)(3), and threads at the junction of the bolt and nut shall be burred or center punched to prevent the nut from loosening.

LOADING: 80 m.p.h. wind with 30% gust factor, normal to sign.

DESIGN STRESSES:
Structural steel - 20,000 p.s.i.
Reinforcing steel - 20,000 p.s.i.
Concrete - 1,400 p.s.i.
Footing soil pressure - 2,000 p.s.f.

After fabrication, the post, fuse plate and upper 6" (Minimum) of the stub post shall be hot-dip galvanized in accordance with AASHTO M111. All bolts, nuts and washers shall be hot-dip galvanized in accordance with AASHTO M232.

Work this sheet with Base Sheet BAW-A-2.



Furnish two 0.01" thick and two 0.03" thick stainless steel or brass (ASTM B36) shims per post.

**BREAK-AWAY WIDE FLANGE
STEEL SIGN POST DETAILS**

F.A.I. 94/90 (DAN RYAN EXPRESSWAY)
31ST STREET TO I-57

DESIGNED - SLW	20
CHECKED - PJO	EXAMINED
DRAWN - SLW	PASSED
CHECKED - PJO	ENGINEER OF STRUCTURAL SERVICES
	ENGINEER OF BRIDGES AND STRUCTURES

NUMBER	REVISION	DATE

BAW-A-1 11/1/2002