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B. M. #49 : Southwest base bolt on aluminum light pole, north side of WB lanes, I-74 and 30 m± west of bridge, War Memorial Drive spur over I-74, El. 190.771 m  
 B. M. #3001 : Chiseled "□" on concrete sign base (I-74 - Bloomington/Galesburg) on west side of War Memorial Drive spur. El. 198.767 m  
 Construction Staging: Single Stage Construction

### INDEX OF SHEETS

1. General Plan and Elevation
2. Anchorage Slab and Parapet Reinforcing Details
3. Typical Sections and Misc. Details
4. Boring Logs I
5. Boring Logs II

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	631	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

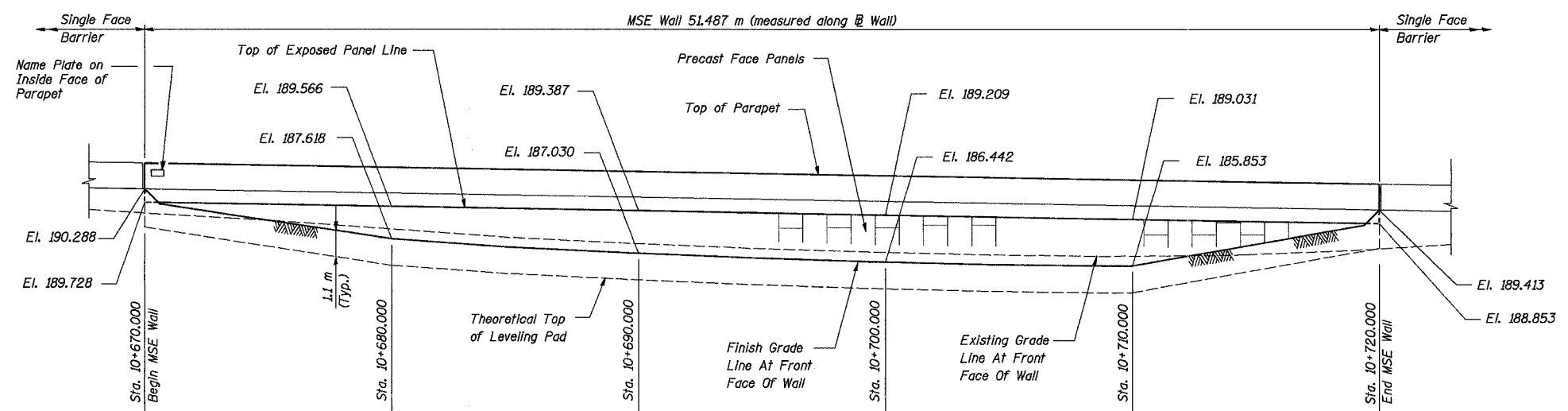
CONTRACT NO. 68200

### TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Name Plates	Each	1
French Drains	m <sup>3</sup>	9.2
Structure Excavation	m <sup>3</sup>	383
Concrete Structures	m <sup>3</sup>	47.5
Concrete Superstructure	m <sup>3</sup>	15.8
Reinforcement Bars, Epoxy Coated	kg	7540
Mechanically Stabilized Earth Retaining Wall	m <sup>2</sup>	157

### GENERAL NOTES

1. Reinforcement bars shall conform to the requirements of AASHTO M 31M or M 322M Grade 400.
2. All dimensions are in millimeters (mm) except as noted.
3. All construction joints shall be bonded.
4. See Special Provisions for Mechanically Stabilized Earth Wall design and construction requirements.
5. The texture on the precast wall panels shall be Split-Faced Limestone Block with an approximate depth of 30 mm. Cost included with Mechanically Stabilized Earth Retaining Wall.
6. The MSE wall supplier's internal stability design shall account for the anchorage slab bearing pressure surcharge and a horizontal sliding force of 7.3 kN/m of wall.



**WALL ELEVATION - RAMP B-4**  
(Looking East)

STA. 10+670.000 - STA. 10+720.000  
 BUILT 200\_ BY  
 STATE OF ILLINOIS  
 F.A.I. RT. 74 SEC (72-7)R-3  
 STR. NO. 072-8561

**NAME PLATE**  
 See Std. 515001

### DESIGN SPECIFICATIONS

1996 AASHTO With 1997 Through 2002 Interims

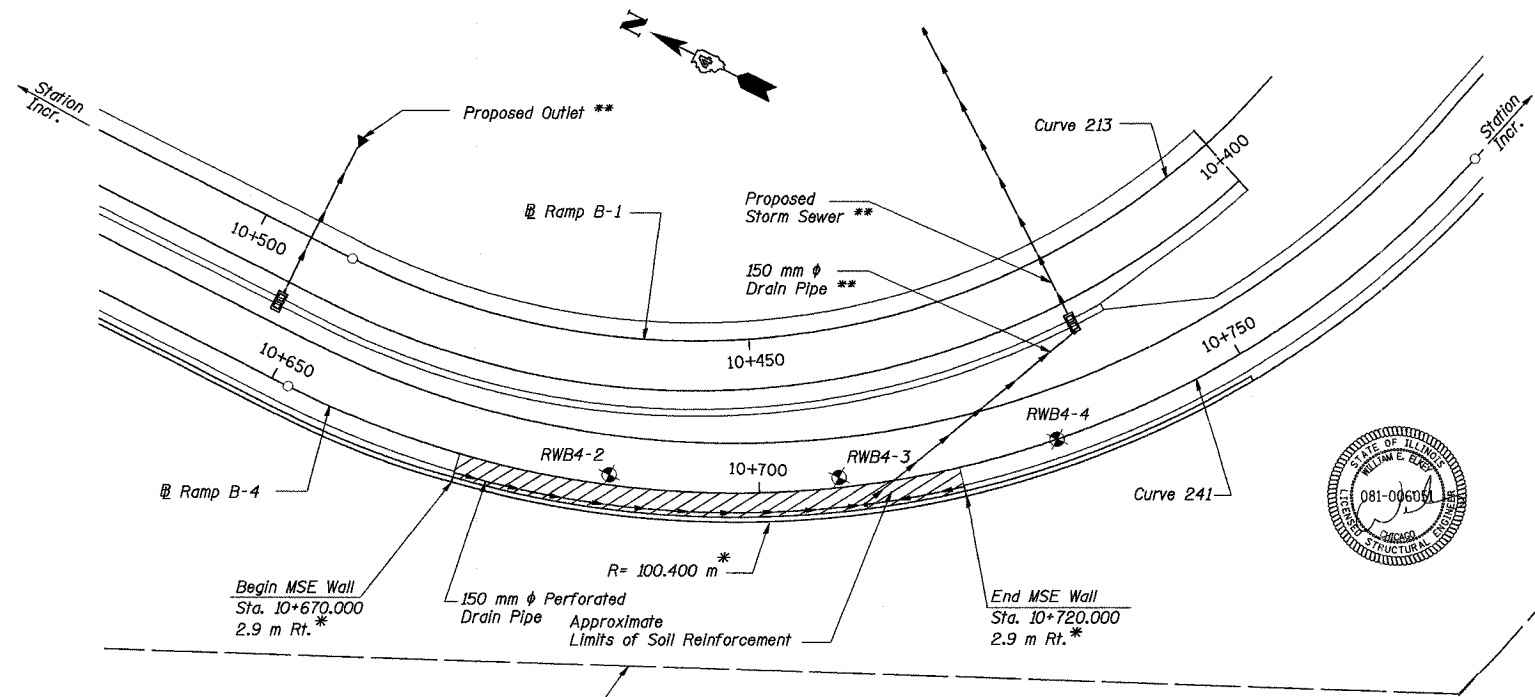
### DESIGN STRESSES

#### FIELD UNITS

$f'_c = 24 \text{ MPa}$   
 $f_y = 400 \text{ MPa (Reinf.)}$

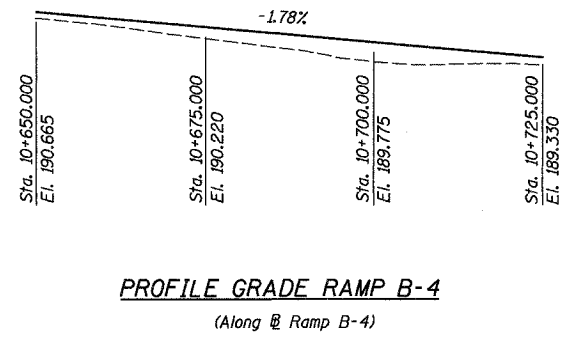
### CURVE DATA

RAMP B-4 CURVE 241	RAMP B-1 CURVE 213
$\Delta = 75^\circ 30' 13'' \text{ (LT)}$	$\Delta = 170^\circ 13' 17'' \text{ (RT)}$
$T = 75.498 \text{ m}$	$T = 876.770 \text{ m}$
$R = 97.500 \text{ m}$	$R = 75.000 \text{ m}$
$L = 128.484 \text{ m}$	$L = 222.819 \text{ m}$
$E = 25.813 \text{ m}$	$E = 804.972 \text{ m}$
PC Sta. 10+651.773	PC Sta. 10+267.631
PI Sta. 10+727.271	PI Sta. 11+144.401
PT Sta. 10+780.257	PT Sta. 10+490.451
S.E. = -7.8%	S.E. = 8.0%
Transition In 10+606 to 10+675	Transition In N/A
Transition Out 10+757 to 10+826	Transition Out 10+474 to 10+522

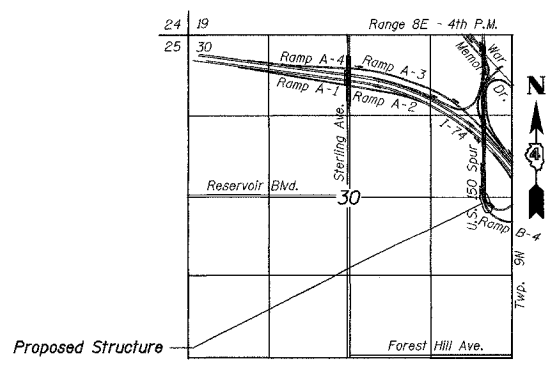


**PLAN**

- \* Offsets and radius refer to Ramp B-4 at Wall @. See Typical Section on Sheet 3 for Details.
- \*\* See Drainage Plans
- ⊕ Boring Location



**PROFILE GRADE RAMP B-4**  
(Along @ Ramp B-4)



**LOCATION SKETCH**

Designed by: WEE/EI  
 Checked by: AK  
 Drafted by: WEE  
 Checked by: AK

REVISION	DATE	DESCRIPTION

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**GENERAL PLAN AND ELEVATION**  
 WALL NO. 12 - RAMP B-4  
 F.A.I. ROUTE 74 SECTION (72-7) R-3  
 PEORIA COUNTY  
 STA. 10+670 TO STA 10+720 (RAMP B-4)  
 STRUCTURE NUMBER 072-8561  
 PARSONS TRANSPORTATION GROUP  
 CHICAGO, ILLINOIS

DRAWING NO.	SCALE	DATE	SHEET NO.
1	N.T.S.	6/25/04	1