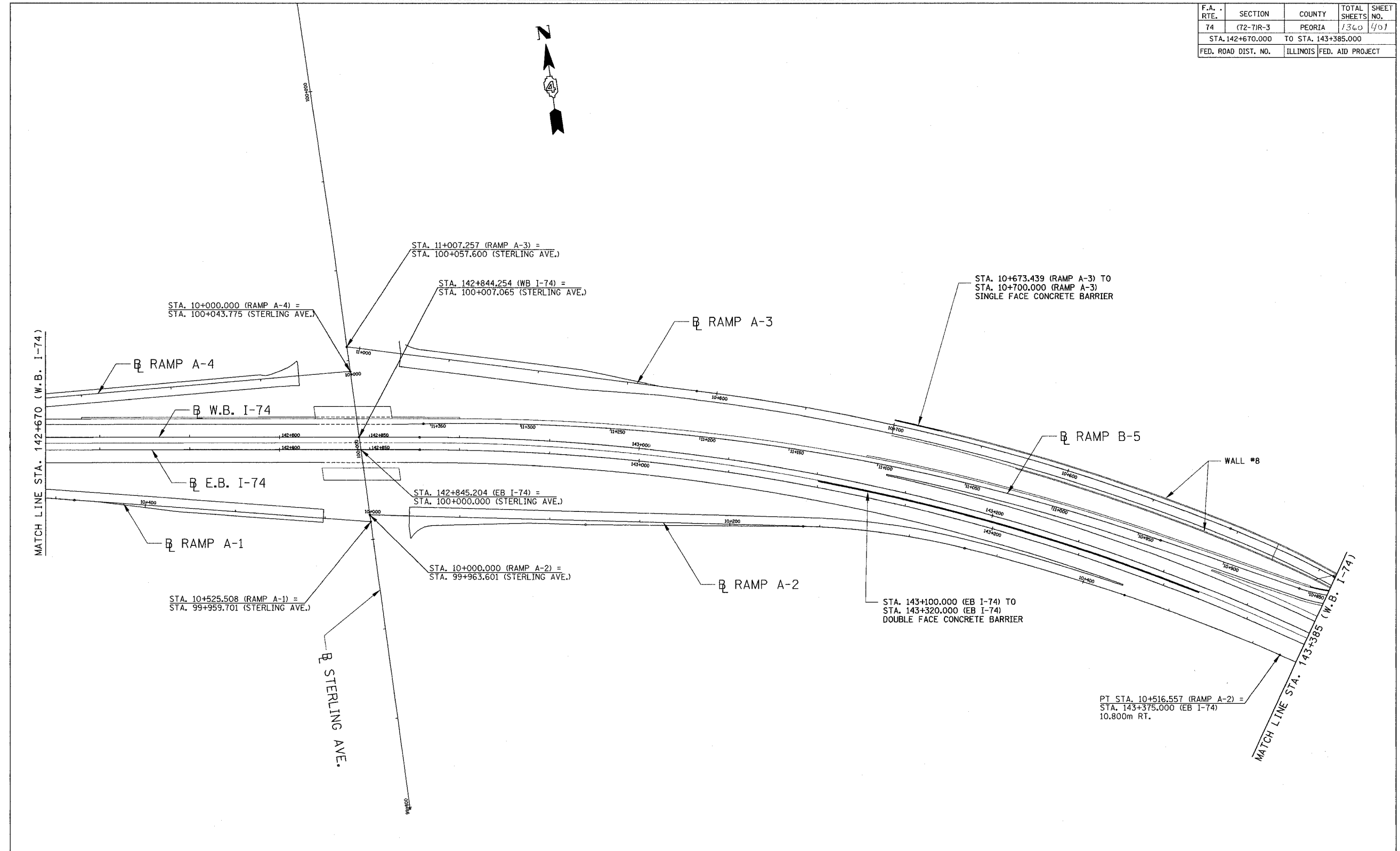
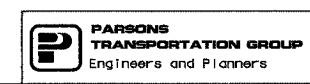
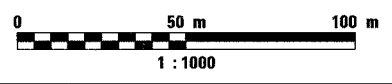


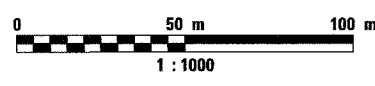
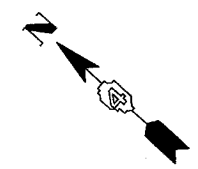
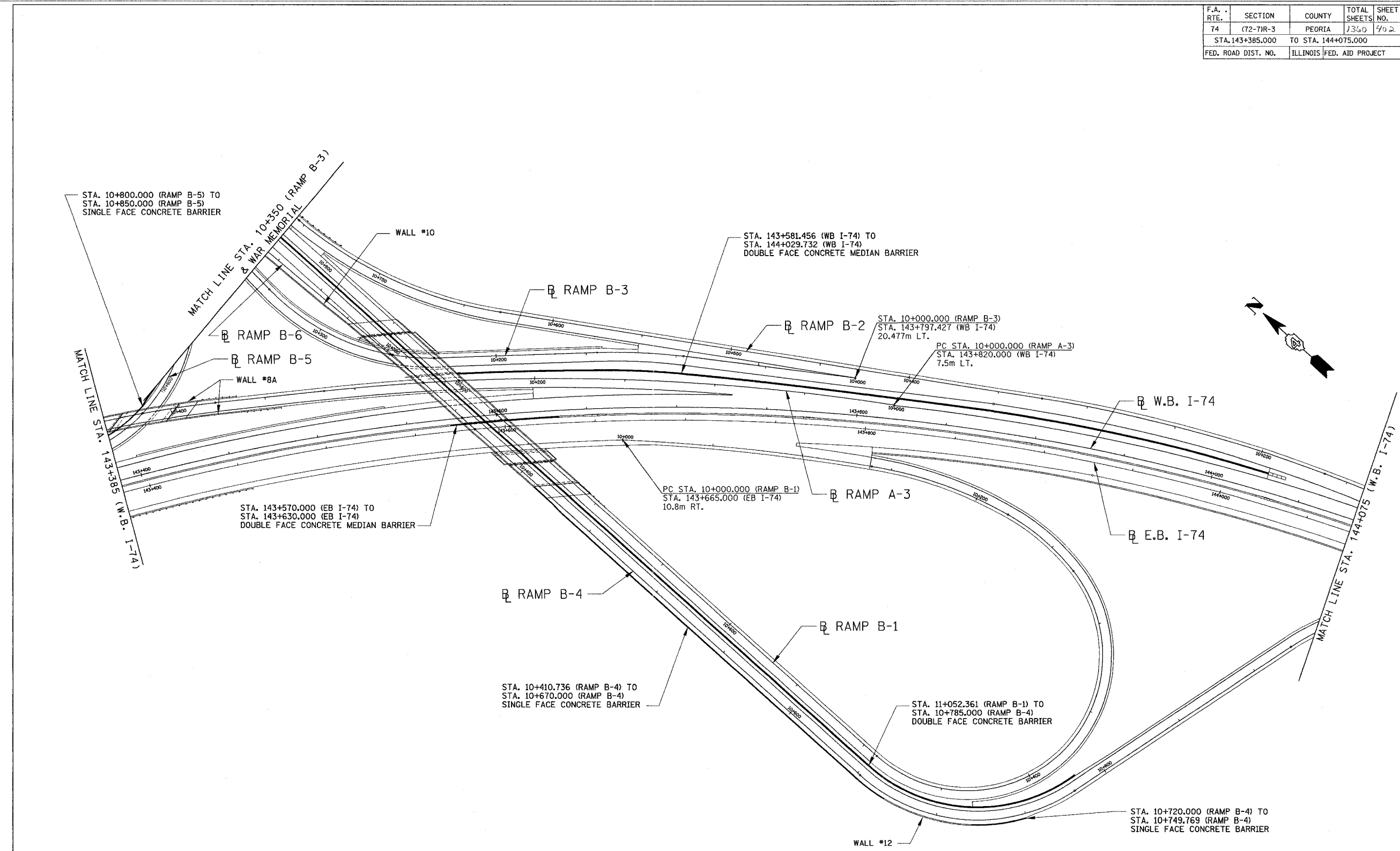
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(72-7)R-3	PEORIA	1360	401
STA. 142+670.000		TO STA. 143+385.000		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION CONCRETE BARRIER SCHEMATIC
NAME	DATE	
		FAI ROUTE 74 (I-74) (WB) STA. 142+670 TO STA. 143+385 DRAWN BY CADD CHECKED BY MWP



F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(72-71R-3)	PEORIA	1360	402
STA. 143+385.000 TO STA. 144+075.000				
FED. ROAD DIST. NO.			ILLINOIS	FED. AID PROJECT

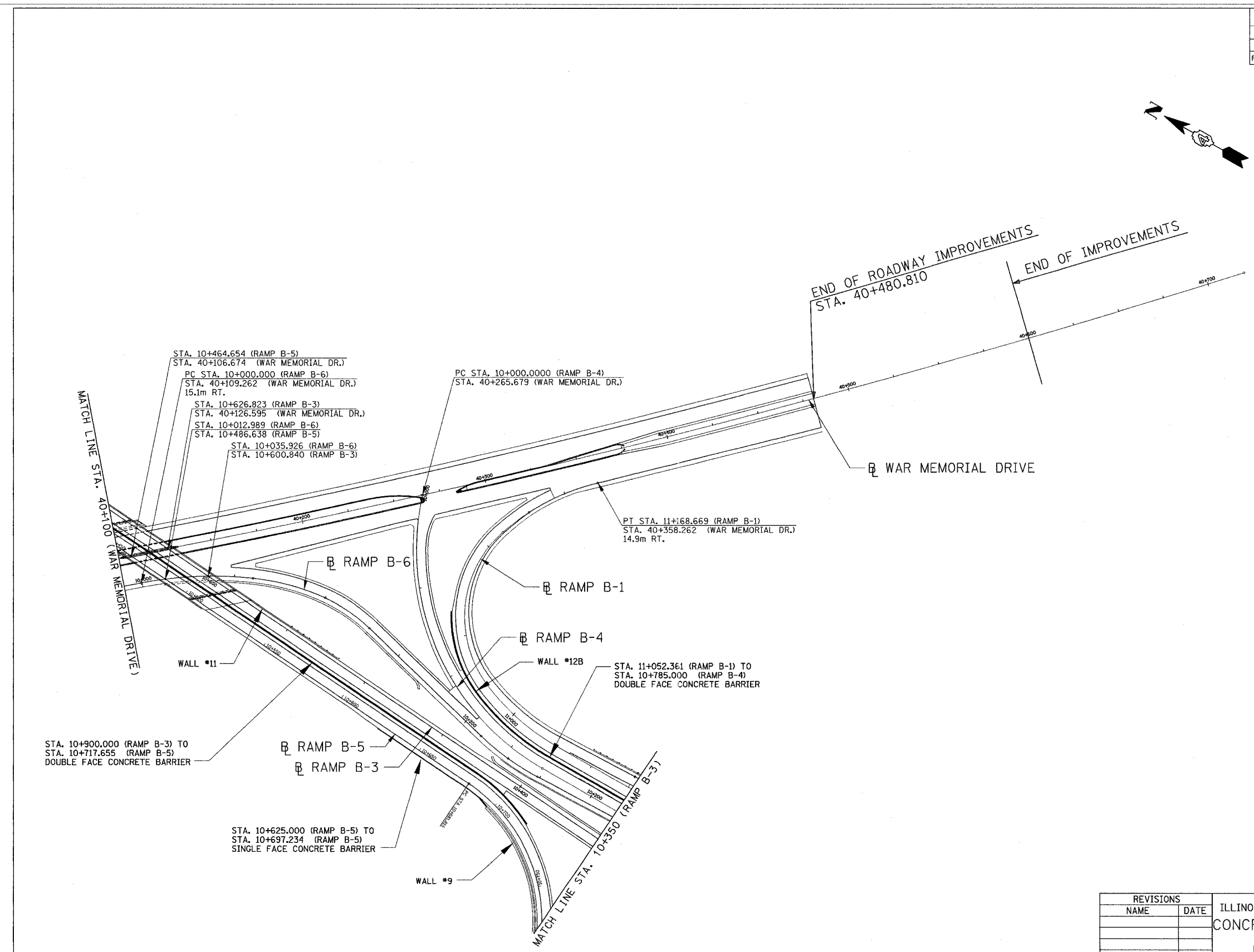
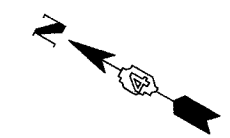


PARSONS
TRANSPORTATION GROUP
 Engineers and Planners

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
CONCRETE BARRIER SCHEMATIC
 FAI ROUTE 74 (I-74) (WB)
 STA. 143+385 TO STA. 144+075
 DRAWN BY CADD
 CHECKED BY MWP
 DATE 11/12/04

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(72-71R-3)	PEORIA	1366	403
STA. 40+100.000		TO STA. 40+480.810		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



END OF ROADWAY IMPROVEMENTS
STA. 40+480.810

END OF IMPROVEMENTS

WAR MEMORIAL DRIVE

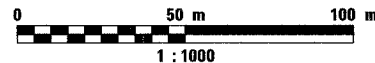
MATCH LINE STA. 40+100 (WAR MEMORIAL DRIVE)

MATCH LINE STA. 10+350 (RAMP B-3)

STA. 10+900.000 (RAMP B-3) TO STA. 10+717.655 (RAMP B-5) DOUBLE FACE CONCRETE BARRIER

STA. 10+625.000 (RAMP B-5) TO STA. 10+697.234 (RAMP B-5) SINGLE FACE CONCRETE BARRIER

STA. 11+052.361 (RAMP B-1) TO STA. 10+785.000 (RAMP B-4) DOUBLE FACE CONCRETE BARRIER



REVISIONS	
NAME	DATE

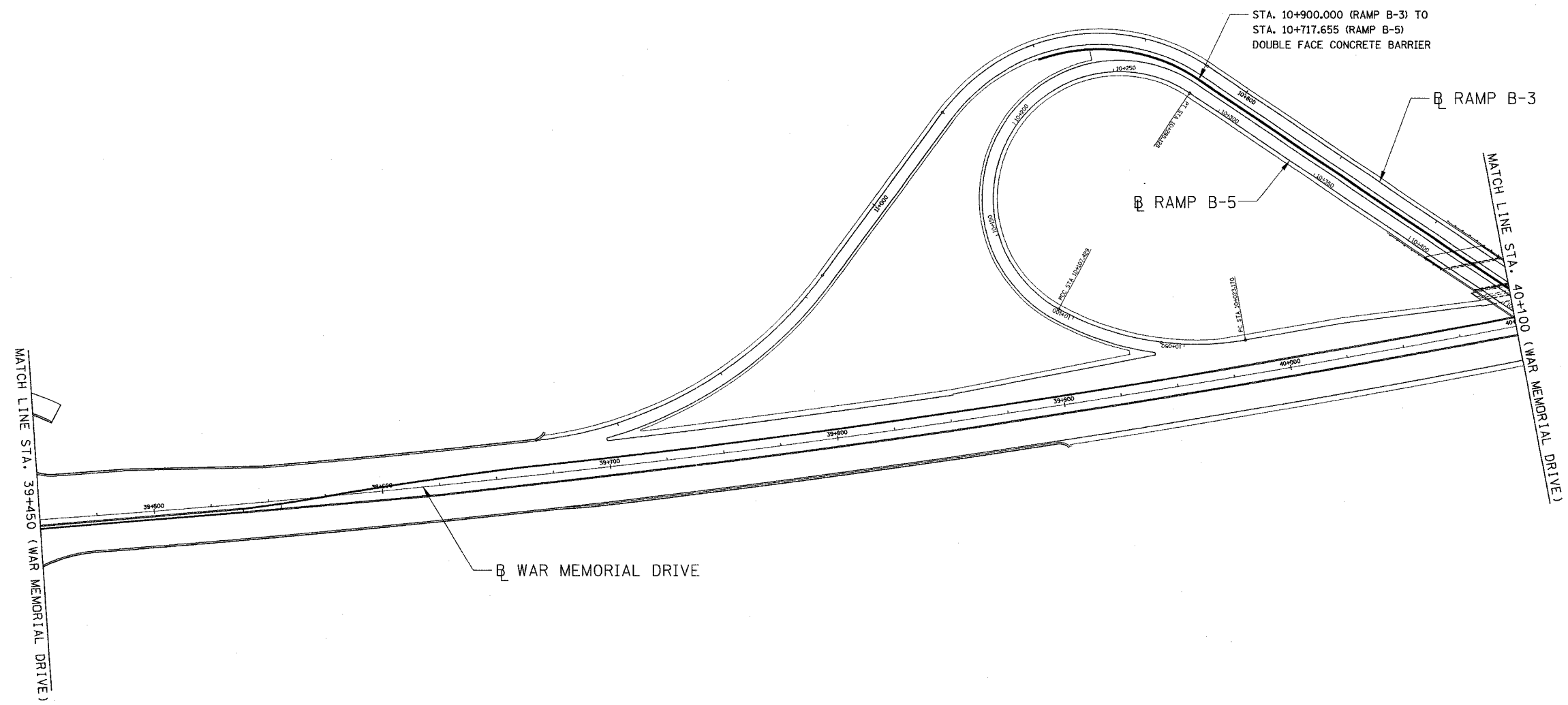
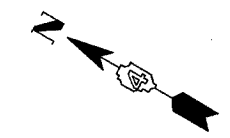
ILLINOIS DEPARTMENT OF TRANSPORTATION
CONCRETE BARRIER SCHEMATIC

FAI ROUTE 74 (I-74) (WB)
STA. 40+100 TO STA. 40+481

DATE 11/12/04

DRAWN BY CADD
CHECKED BY MWP

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(72-71R-3)	PEORIA	1360	404
STA. 39+450.000		TO STA. 40+100.000		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



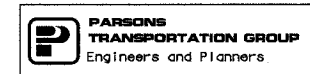
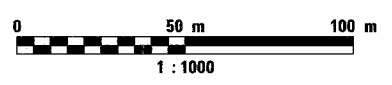
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 CONCRETE BARRIER SCHEMATIC

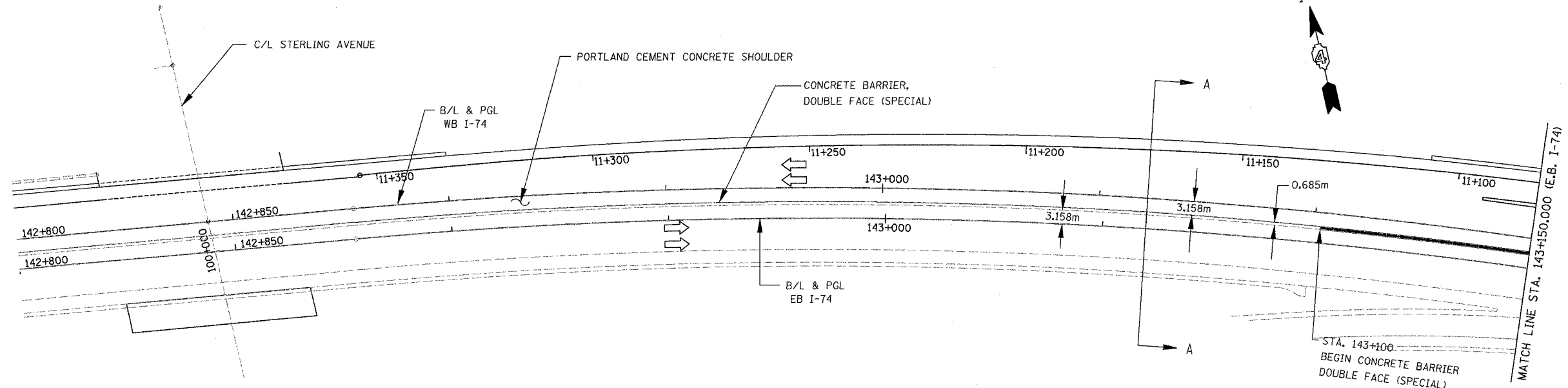
FAI ROUTE 74 (I-74) (WB)
 STA. 39+450 TO STA. 40+100

DRAWN BY CADD
 CHECKED BY MWP

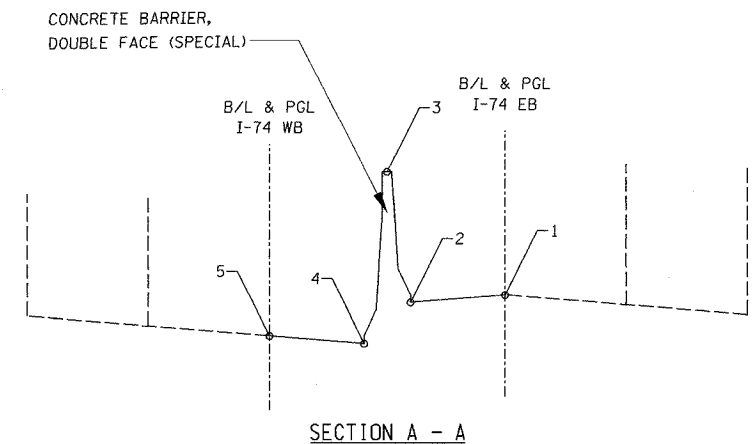
DATE 11/12/04



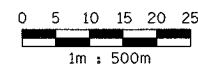
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(72-71R-3)	PEORIA	1360	405
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



I-74 WB STATION	ELEVATIONS					I-74 EB STATION
	5	4	3	2	1	
143+101.335	195.175	195.039	196.366	195.071	195.187	143+100.000
143+106.365	195.128	194.992	196.319	195.024	195.141	143+105.000
143+111.395	195.081	194.945	196.272	194.977	195.094	143+110.000
143+116.425	195.034	194.899	196.226	194.931	195.047	143+115.000
143+121.455	194.987	194.852	196.179	194.884	195.001	143+120.000
143+126.485	194.940	194.805	196.132	194.837	194.954	143+125.000
143+131.515	194.893	194.758	196.085	194.790	194.907	143+130.000
143+136.545	194.846	194.711	196.039	194.744	194.861	143+135.000
143+141.575	194.799	194.664	195.992	194.697	194.814	143+140.000
143+146.605	194.752	194.617	195.945	194.650	194.767	143+145.000
143+151.635	194.705	194.570	195.899	194.604	194.720	143+150.000



- NOTES:
1. BARRIER BASE WIDTH EQUALS 0.685m
 2. BARRIER HEIGHT EQUALS 1.295m ON HIGH SIDE OF SHOULDER. ON LOW SIDE OF SHOULDER, HEIGHT EQUALS DIFFERENCE BETWEEN SHOULDER ELEVATION AND TOP OF BARRIER ELEVATION.
 3. BARRIER ON LOW SIDE OF SHOULDER RETAINS GM-SHAPE TO 1.295m HIGH THEN PROVIDES VERTICAL FACE TO ACHIEVE REQUIRED HEIGHT. SEE CONCRETE BARRIER, DOUBLE FACE (SPECIAL) DETAIL.

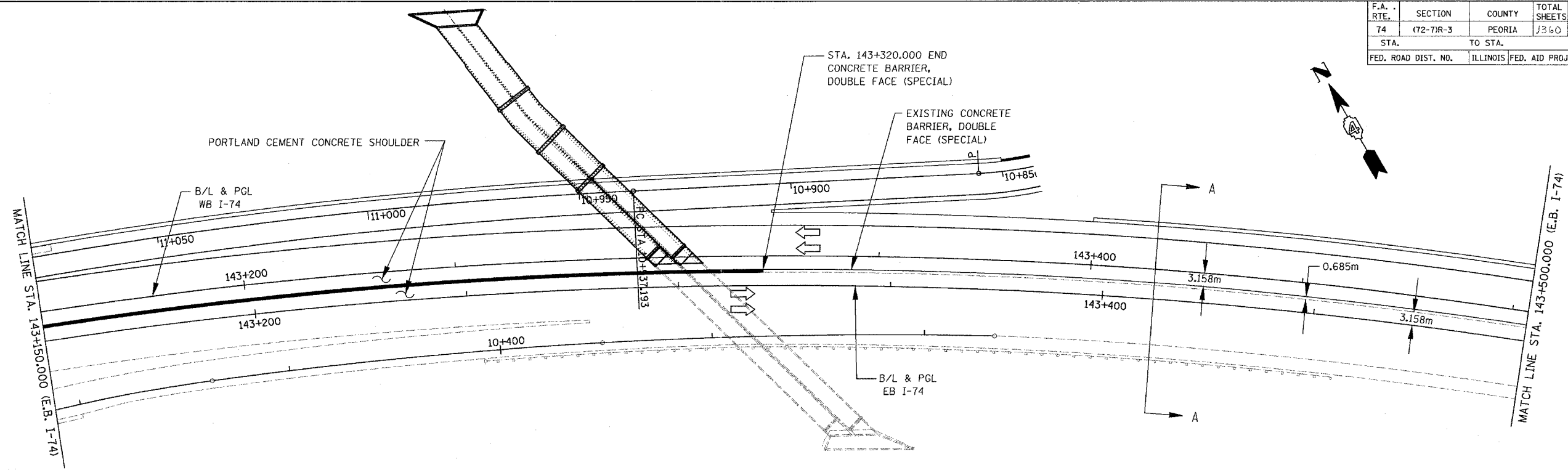


REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION CONCRETE BARRIER ELEVATIONS FAI ROUTE 74 (I-74) (WB) STA. 142+800.000 - STA. 143+150.000 DRAWN BY CADD CHECKED BY MWP
NAME	DATE	

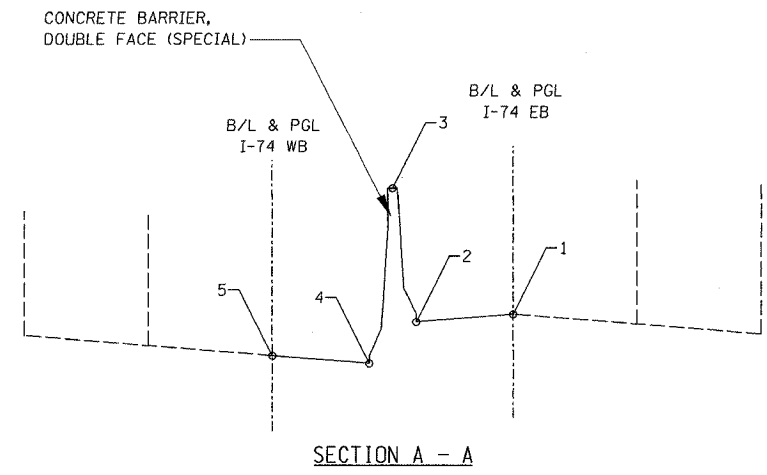


P:\643996\CIVIL\SHWB\BEO0E1-JAWB.DGN

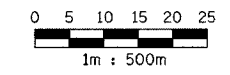
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	72-71R-3	PEORIA	1360	406
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



I-74 WB STATION	ELEVATIONS					I-74 EB STATION
	5	4	3	2	1	
143+151.635	194.705	194.570	195.899	194.604	194.720	143+150.000
143+156.665	194.658	194.523	195.852	194.557	194.674	143+155.000
143+161.695	194.611	194.476	195.805	194.510	194.627	143+160.000
143+166.725	194.564	194.429	195.759	194.464	194.580	143+165.000
143+171.756	194.517	194.382	195.712	194.417	194.534	143+170.000
143+176.786	194.470	194.335	195.665	194.370	194.487	143+175.000
143+181.816	194.424	194.288	195.618	194.323	194.440	143+180.000
143+186.846	194.377	194.241	195.572	194.277	194.394	143+185.000
143+191.876	194.330	194.194	195.525	194.230	194.347	143+190.000
143+196.906	194.283	194.147	195.478	194.183	194.300	143+195.000
143+201.936	194.236	194.100	195.432	194.137	194.253	143+200.000
143+206.966	194.189	194.053	195.385	194.090	194.207	143+205.000
143+211.996	194.142	194.006	195.338	194.043	194.160	143+210.000
143+217.026	194.095	193.959	195.292	193.997	194.113	143+215.000
143+222.056	194.048	193.912	195.245	193.950	194.067	143+220.000
143+227.086	194.001	193.865	195.198	193.903	194.020	143+225.000
143+232.116	193.954	193.818	195.151	193.856	193.973	143+230.000
143+237.146	193.907	193.771	195.105	193.810	193.927	143+235.000
143+242.176	193.860	193.724	195.058	193.763	193.880	143+240.000
143+247.206	193.813	193.677	195.011	193.716	193.833	143+245.000
143+252.236	193.766	193.630	194.965	193.670	193.786	143+250.000
143+257.266	193.719	193.583	194.918	193.623	193.740	143+255.000
143+262.296	193.672	193.536	194.871	193.576	193.693	143+260.000
143+267.326	193.625	193.489	194.825	193.530	193.646	143+265.000
143+272.356	193.578	193.442	194.778	193.483	193.600	143+270.000
143+277.386	193.531	193.395	194.731	193.436	193.553	143+275.000
143+282.416	193.484	193.348	194.684	193.389	193.506	143+280.000
143+287.447	193.437	193.301	194.638	193.343	193.460	143+285.000
143+292.477	193.390	193.254	194.591	193.296	193.413	143+290.000
143+297.507	193.343	193.207	194.544	193.249	193.366	143+295.000
143+302.537	193.296	193.160	194.498	193.203	193.319	143+300.000
143+307.567	193.249	193.113	194.451	193.156	193.273	143+305.000
143+312.597	193.202	193.066	194.404	193.109	193.226	143+310.000
143+317.627	193.155	193.019	194.358	193.063	193.179	143+315.000
143+322.657	193.108	192.972	194.311	193.016	193.133	143+320.000

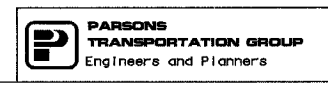


- NOTES:**
- BARRIER BASE WIDTH EQUALS 0.685m
 - BARRIER HEIGHT EQUALS 1.295m ON HIGH SIDE OF SHOULDER. ON LOW SIDE OF SHOULDER, HEIGHT EQUALS DIFFERENCE BETWEEN SHOULDER ELEVATION AND TOP OF BARRIER ELEVATION.
 - BARRIER ON LOW SIDE OF SHOULDER RETAINS GM-SHAPE TO 1.295m HIGH THEN PROVIDES VERTICAL FACE TO ACHIEVE REQUIRED HEIGHT SEE CONCRETE BARRIER, DOUBLE FACE (SPECIAL) DETAIL.

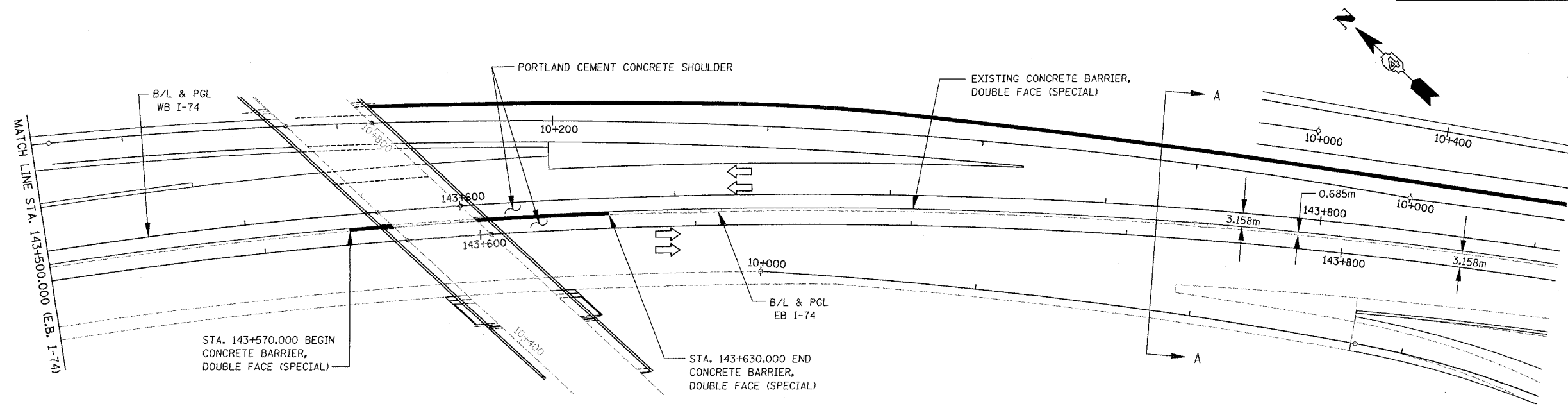


REVISIONS	
NAME	DATE

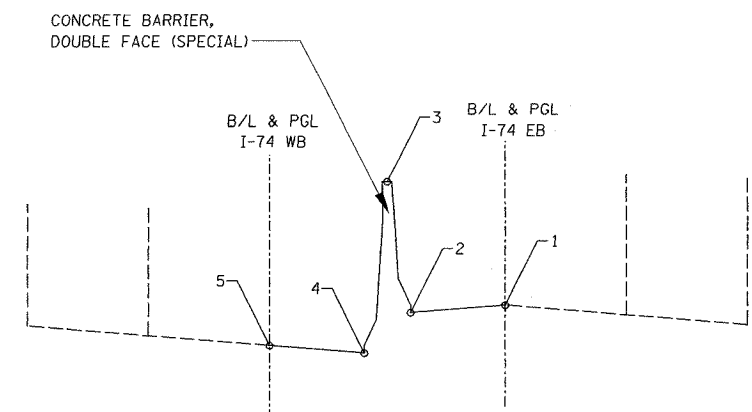
ILLINOIS DEPARTMENT OF TRANSPORTATION
CONCRETE BARRIER ELEVATIONS
 FAI ROUTE 74 (I-74) (WB)
 STA. 143+150.000 - STA. 143+500.000
 DRAWN BY CADD
 CHECKED BY MWP
 DATE 11/12/04



F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(72-71R-3)	PEORIA	1360	407
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

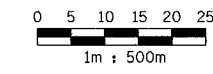


I-74 WB STATION	ELEVATIONS					I-74 EB STATION
	5	4	3	2	1	
143+574.159	190.759	190.623	191.976	190.681	190.798	143+570.000
143+579.189	190.712	190.576	191.929	190.634	190.751	143+575.000
143+584.219	190.665	190.529	191.882	190.587	190.704	143+580.000
143+589.249	190.618	190.482	191.836	190.541	190.658	143+585.000
143+594.279	190.571	190.435	191.789	190.494	190.611	143+590.000
143+599.309	190.524	190.388	191.742	190.447	190.564	143+595.000
143+604.339	190.477	190.341	191.696	190.401	190.517	143+600.000
143+609.369	190.430	190.294	191.649	190.354	190.471	143+605.000
143+614.399	190.383	190.247	191.602	190.307	190.424	143+610.000
143+619.429	190.336	190.200	191.556	190.261	190.377	143+615.000
143+624.459	190.289	190.153	191.509	190.214	190.331	143+620.000
143+629.489	190.242	190.106	191.462	190.167	190.284	143+625.000
143+634.519	190.195	190.059	191.415	190.120	190.237	143+630.000
143+639.550	190.148	190.012	191.369	190.074	190.191	143+635.000
143+644.580	190.101	189.965	191.322	190.027	190.144	143+640.000
143+649.610	190.054	189.918	191.275	189.980	190.097	143+645.000
143+654.640	190.007	189.871	191.229	189.934	190.050	143+650.000
143+659.670	189.960	189.824	191.182	189.887	190.004	143+655.000
143+664.700	189.913	189.777	191.135	189.840	189.957	143+660.000
143+669.730	189.866	189.731	191.089	189.794	189.910	143+665.000
143+674.760	189.819	189.684	191.042	189.747	189.864	143+670.000
143+679.790	189.772	189.637	190.995	189.700	189.817	143+675.000
143+684.820	189.725	189.590	190.948	189.653	189.770	143+680.000
143+689.850	189.678	189.543	190.902	189.607	189.724	143+685.000
143+694.880	189.631	189.496	190.855	189.560	189.677	143+690.000
143+699.910	189.584	189.449	190.808	189.513	189.630	143+695.000
143+704.940	189.537	189.402	190.762	189.467	189.583	143+700.000



SECTION A - A

- NOTES:
1. BARRIER BASE WIDTH EQUALS 0.685m
 2. BARRIER HEIGHT EQUALS 1.295m ON HIGH SIDE OF SHOULDER. ON LOW SIDE OF SHOULDER, HEIGHT EQUALS DIFFERENCE BETWEEN SHOULDER ELEVATION AND TOP OF BARRIER ELEVATION.
 3. BARRIER ON LOW SIDE OF SHOULDER RETAINS GM-SHAPE TO 1.295m HIGH THEN PROVIDES VERTICAL FACE TO ACHIEVE REQUIRED HEIGHT. SEE CONCRETE BARRIER, DOUBLE FACE (SPECIAL) DETAIL.

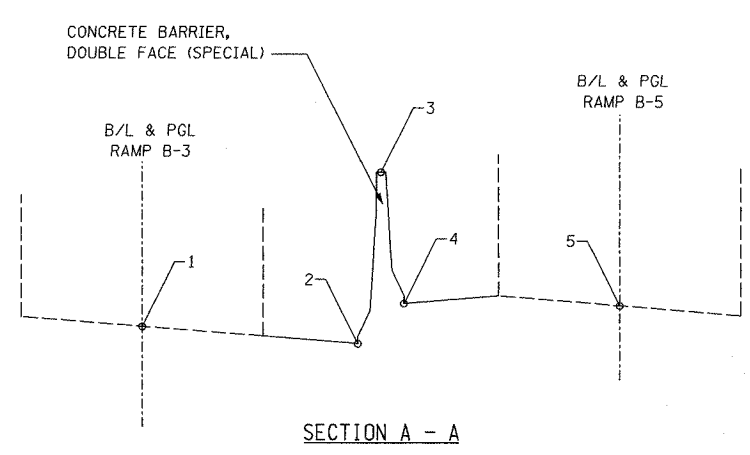
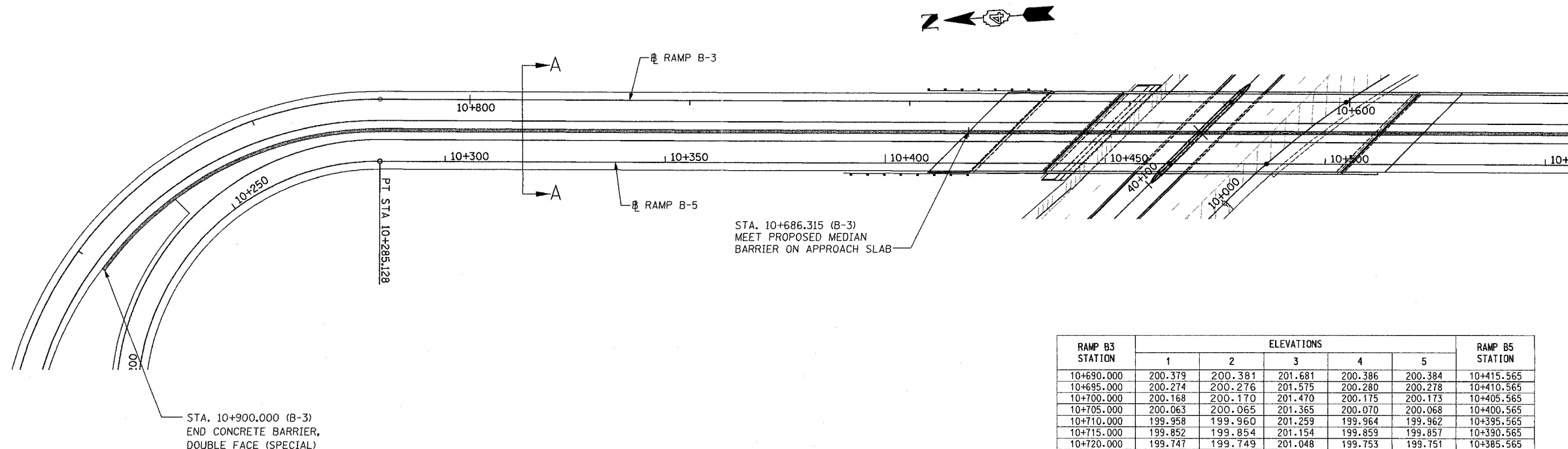


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
CONCRETE BARRIER ELEVATIONS
 FAI ROUTE 74 (I-74) (WB)
 STA. 143+500.000 - STA. 143+850.000
 DRAWN BY CADD
 CHECKED BY MWP
 DATE 11/12/04

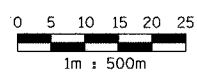


F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(72-71R-3)	PEORIA	1360	908
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



RAMP B3 STATION	ELEVATIONS					RAMP B5 STATION
	1	2	3	4	5	
10+690.000	200.379	200.381	201.681	200.386	200.384	10+415.565
10+695.000	200.274	200.276	201.575	200.280	200.278	10+410.565
10+700.000	200.168	200.170	201.470	200.175	200.173	10+405.565
10+705.000	200.063	200.065	201.365	200.070	200.068	10+400.565
10+710.000	199.958	199.960	201.259	199.964	199.962	10+395.565
10+715.000	199.852	199.854	201.154	199.859	199.857	10+390.565
10+720.000	199.747	199.749	201.048	199.753	199.751	10+385.565
10+725.000	199.641	199.643	200.943	199.648	199.646	10+380.565
10+730.000	199.536	199.538	200.837	199.542	199.540	10+375.565
10+735.000	199.430	199.432	200.732	199.437	199.435	10+370.565
10+740.000	199.325	199.327	200.626	199.331	199.329	10+365.565
10+745.000	199.220	199.222	200.521	199.226	199.224	10+360.565
10+750.000	199.114	199.116	200.416	199.121	199.119	10+355.565
10+755.000	199.009	199.011	200.310	199.015	199.013	10+350.565
10+760.000	198.906	198.908	200.205	198.910	198.908	10+345.565
10+765.000	198.810	198.812	200.107	198.804	198.802	10+340.565
10+770.000	198.720	198.722	200.017	198.699	198.697	10+335.565
10+775.000	198.636	198.624	199.919	198.593	198.591	10+330.565
10+780.000	198.558	198.513	199.808	198.488	198.486	10+325.565
10+785.000	198.487	198.403	199.698	198.382	198.380	10+320.565
10+790.000	198.421	198.304	199.599	198.277	198.275	10+315.565
10+795.000	198.362	198.212	199.507	198.201	198.170	10+310.565
10+800.000	198.306	198.126	199.424	198.129	198.064	10+305.565
10+805.000	198.250	198.036	199.358	198.063	197.959	10+300.565
10+810.000	198.193	197.945	199.292	197.997	197.853	10+295.565
10+815.000	198.137	197.849	199.234	197.939	197.748	10+290.565
10+820.000	198.081	197.753	199.175	197.880	197.642	10+285.565
10+825.000	198.025	197.650	199.128	197.833	197.555	10+281.416
10+830.000	197.968	197.546	199.082	197.787	197.469	10+277.352
10+835.000	197.912	197.450	199.038	197.743	197.384	10+273.298
10+840.000	197.856	197.347	198.986	197.691	197.299	10+269.260
10+845.000	197.800	197.264	198.901	197.606	197.214	10+265.242
10+850.000	197.743	197.207	198.817	197.522	197.130	10+261.249
10+855.000	197.687	197.151	198.733	197.438	197.046	10+257.286
10+860.000	197.631	197.095	198.650	197.355	196.963	10+253.356
10+865.000	197.575	197.039	198.568	197.273	196.881	10+249.463
10+870.000	197.518	196.982	198.487	197.192	196.800	10+245.609
10+875.000	197.462	196.926	198.406	197.111	196.719	10+241.798
10+880.000	197.406	196.870	198.325	197.030	196.640	10+238.030
10+885.000	197.350	196.814	198.244	196.949	196.561	10+234.306
10+890.000	197.293	196.757	198.163	196.868	196.484	10+230.630
10+895.000	197.237	196.701	198.082	196.787	196.407	10+227.001
10+900.000	197.181	196.645	198.001	196.706	196.332	10+223.420

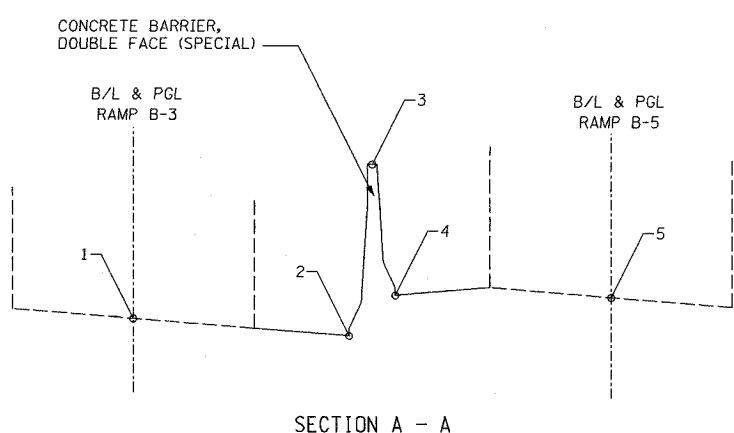
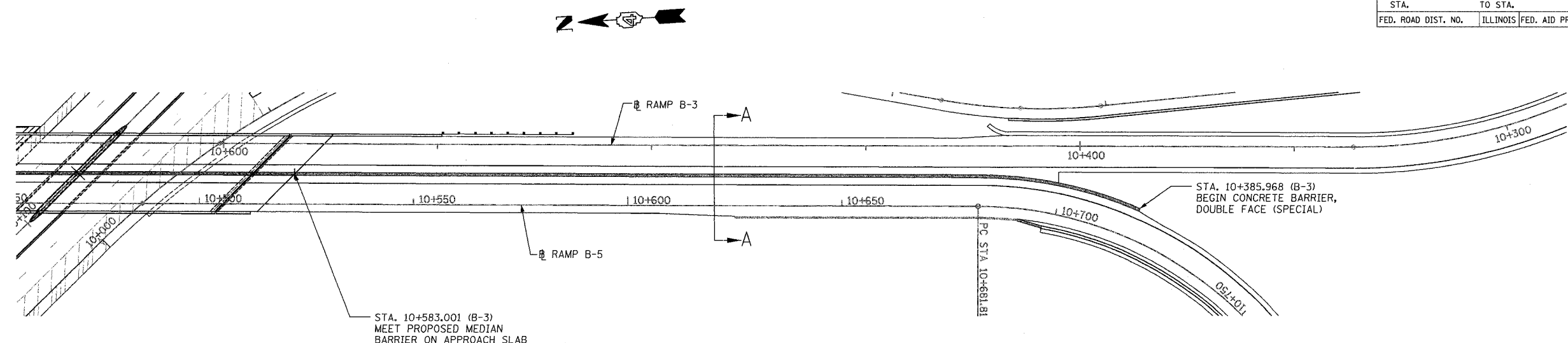
- NOTES:**
- BARRIER BASE WIDTH EQUALS 0.685m
 - BARRIER HEIGHT EQUALS 1.295m ON HIGH SIDE OF SHOULDER. ON LOW SIDE OF SHOULDER, HEIGHT EQUALS DIFFERENCE BETWEEN SHOULDER ELEVATION AND TOP OF BARRIER ELEVATION.
 - BARRIER ON LOW SIDE OF SHOULDER RETAINS GM-SHAPE TO 1.295m HIGH THEN PROVIDES VERTICAL FACE TO ACHIEVE REQUIRED HEIGHT. SEE CONCRETE BARRIER, DOUBLE FACE (SPECIAL) DETAIL.



REVISIONS	
NAME	DATE

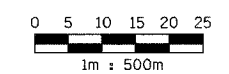
ILLINOIS DEPARTMENT OF TRANSPORTATION
CONCRETE BARRIER ELEVATIONS
 FAI ROUTE 74 (I-74) (WB)
 RAMP B-3, B-5
 DRAWN BY CADD
 CHECKED BY MWP
 DATE 11/12/04

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(72-71R-3)	PEORIA	1360	409
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



- NOTES:**
1. BARRIER BASE WIDTH EQUALS 0.685m
 2. BARRIER HEIGHT EQUALS 1.295m ON HIGH SIDE OF SHOULDER. ON LOW SIDE OF SHOULDER, HEIGHT EQUALS DIFFERENCE BETWEEN SHOULDER ELEVATION AND TOP OF BARRIER ELEVATION.
 3. BARRIER ON LOW SIDE OF SHOULDER RETAINS GM-SHAPE TO 1.295m HIGH THEN PROVIDES VERTICAL FACE TO ACHIEVE REQUIRED HEIGHT (SEE SPECIAL DETAIL)

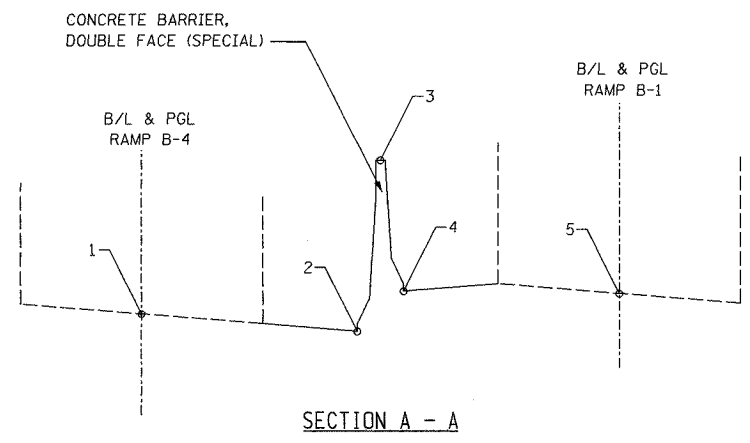
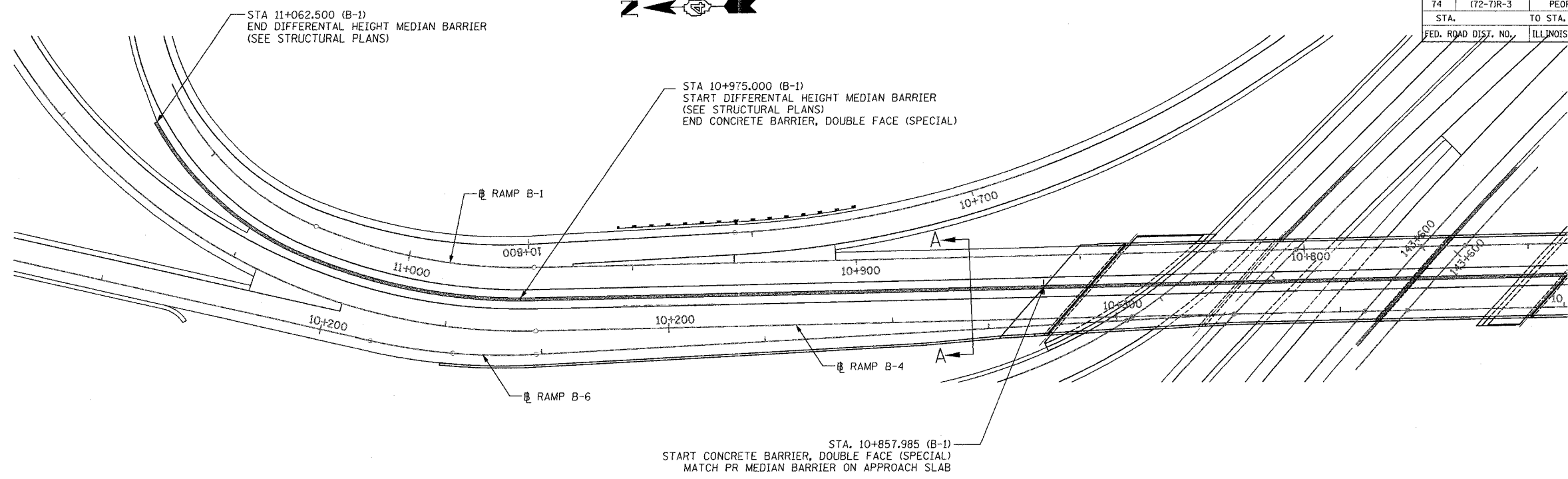
RAMP B3 STATION	ELEVATIONS					RAMP B5 STATION
	1	2	3	4	5	
10+390.000	194.053	194.500	195.795	194.487	194.102	10+713.883
10+395.000	194.217	194.600	195.943	194.648	194.263	10+708.960
10+400.000	194.380	194.261	196.101	194.806	194.421	10+704.123
10+405.000	194.544	194.471	196.258	194.963	194.578	10+699.355
10+410.000	194.708	194.670	196.386	195.091	194.732	10+694.638
10+415.000	194.872	194.857	196.492	195.197	194.885	10+689.958
10+420.000	195.035	195.037	196.604	195.309	195.038	10+685.301
10+425.000	195.199	195.201	196.719	195.424	195.193	10+680.565
10+430.000	195.363	195.365	196.836	195.541	195.357	10+675.565
10+435.000	195.527	195.529	196.952	195.657	195.520	10+670.565
10+440.000	195.690	195.692	197.063	195.788	195.684	10+665.565
10+445.000	195.854	195.856	197.213	195.918	195.848	10+660.565
10+450.000	196.018	196.020	197.343	196.048	196.012	10+655.565
10+455.000	196.182	196.184	197.479	196.181	196.175	10+650.565
10+460.000	196.346	196.348	197.643	196.341	196.339	10+645.565
10+465.000	196.509	196.511	197.806	196.505	196.503	10+640.565
10+470.000	196.673	196.675	197.970	196.669	196.667	10+635.565
10+475.000	196.837	196.839	198.134	196.832	196.830	10+630.565
10+480.000	197.001	197.003	198.298	196.996	196.994	10+625.565
10+485.000	197.164	197.166	198.461	197.160	197.158	10+620.565
10+490.000	197.328	197.330	198.625	197.324	197.322	10+615.565
10+495.000	197.492	197.494	198.789	197.487	197.485	10+610.565
10+500.000	197.656	197.658	198.953	197.651	197.649	10+605.565
10+505.000	197.819	197.821	199.116	197.815	197.813	10+600.565
10+510.000	197.983	197.985	199.280	197.979	197.977	10+595.565
10+515.000	198.147	198.149	199.444	198.142	198.140	10+590.565
10+520.000	198.311	198.313	199.608	198.306	198.304	10+585.565
10+525.000	198.475	198.477	199.772	198.470	198.468	10+580.565
10+530.000	198.638	198.640	199.935	198.634	198.632	10+575.565
10+535.000	198.802	198.804	200.099	198.797	198.795	10+570.565
10+540.000	198.966	198.968	200.263	198.961	198.959	10+565.565
10+545.000	199.130	199.132	200.427	199.125	199.123	10+560.565
10+550.000	199.293	199.295	200.590	199.289	199.287	10+555.565
10+555.000	199.457	199.459	200.754	199.452	199.450	10+550.565
10+560.000	199.621	199.623	200.918	199.616	199.614	10+545.565
10+565.000	199.785	199.787	201.082	199.780	199.778	10+540.565
10+570.000	199.949	199.951	201.246	199.944	199.942	10+535.565
10+575.000	200.112	200.114	201.409	200.107	200.105	10+530.565
10+580.000	200.276	200.278	201.573	200.271	200.269	10+525.565



REVISIONS	
NAME	DATE

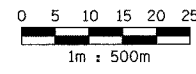
ILLINOIS DEPARTMENT OF TRANSPORTATION
CONCRETE BARRIER ELEVATIONS
 FAI ROUTE 74 (I-74) (WB)
 RAMP B-3, B-5
 DRAWN BY CADD
 CHECKED BY MWP
 DATE 11/12/04

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(72-7)R-3	PEORIA	1360	470
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			



- NOTES:**
1. BARRIER BASE WIDTH EQUALS 0.685m
 2. BARRIER HEIGHT EQUALS 1.295m ON HIGH SIDE OF SHOULDER. ON LOW SIDE OF SHOULDER, HEIGHT EQUALS DIFFERENCE BETWEEN SHOULDER ELEVATION AND TOP OF BARRIER ELEVATION.
 3. BARRIER ON LOW SIDE OF SHOULDER RETAINS GM-SHAPE TO 1.295m HIGH THEN PROVIDES VERTICAL FACE TO ACHIEVE REQUIRED HEIGHT. SEE CONCRETE BARRIER, DOUBLE FACE (SPECIAL) DETAILS.

RAMP B4 STATION	ELEVATIONS					RAMP B1 STATION
	1	2	3	4	5	
10+170.000	195.721	195.332	197.269	195.974	195.723	10+972.199
10+175.000	195.803	195.455	197.311	196.016	195.805	10+967.224
10+180.000	195.886	195.578	197.353	196.058	195.887	10+962.224
10+185.000	195.968	195.700	197.395	196.100	195.969	10+957.224
10+190.000	196.050	195.811	197.446	196.151	196.051	10+952.224
10+195.000	196.132	195.923	197.498	196.203	196.133	10+947.224
10+200.000	196.215	196.035	197.551	196.256	196.215	10+942.224
10+205.000	196.297	196.147	197.599	196.304	196.298	10+937.224
10+210.000	196.379	196.263	197.677	196.382	196.380	10+932.224
10+215.000	196.461	196.374	197.759	196.464	196.462	10+927.224
10+220.000	196.543	196.486	197.841	196.546	196.544	10+922.224
10+225.000	196.626	196.598	197.923	196.628	196.626	10+917.224
10+230.000	196.708	196.710	198.006	196.711	196.709	10+912.224
10+235.000	196.790	196.792	198.088	196.793	196.791	10+907.224
10+240.000	196.872	196.874	198.170	196.875	196.873	10+902.224
10+245.000	196.955	196.957	198.252	196.957	196.955	10+897.224
10+250.000	197.037	197.039	198.334	197.039	197.037	10+892.224
10+255.000	197.119	197.121	198.417	197.122	197.120	10+887.224
10+260.000	197.201	197.203	198.499	197.204	197.202	10+882.224
10+265.000	197.283	197.285	198.581	197.286	197.284	10+877.224
10+270.000	197.366	197.368	198.663	197.368	197.366	10+872.224
10+275.000	197.448	197.450	198.745	197.450	197.448	10+867.224
10+280.000	197.530	197.532	198.828	197.533	197.531	10+862.224



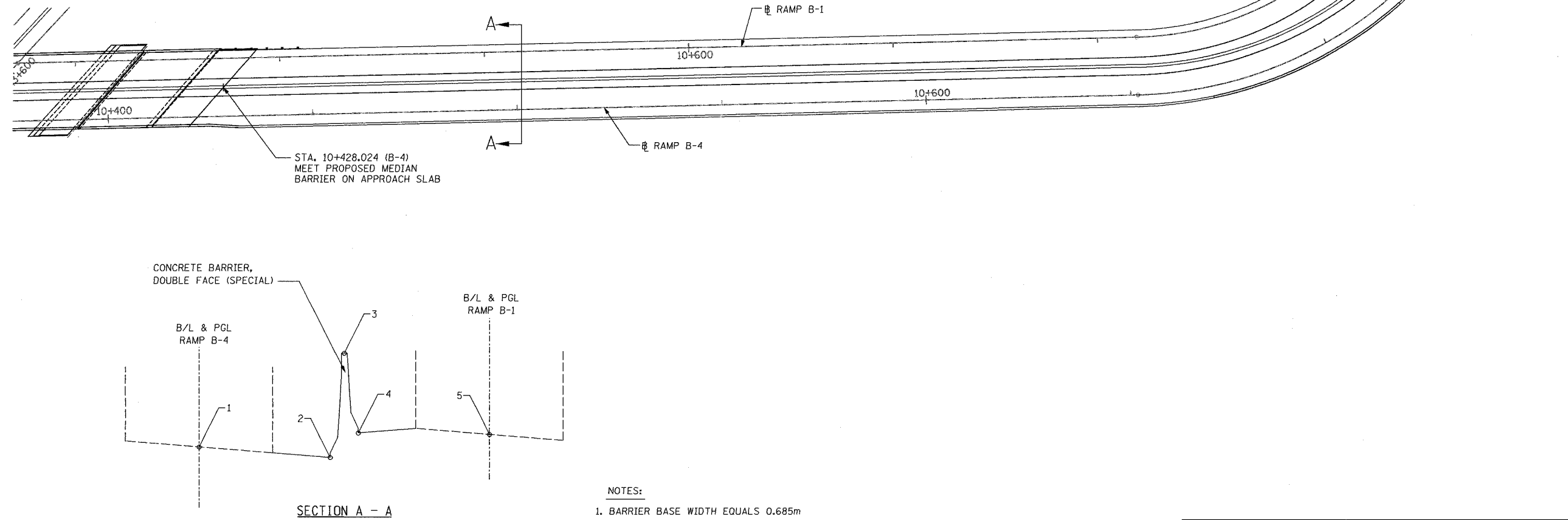
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
CONCRETE BARRIER ELEVATIONS
 FAI ROUTE 74 (I-74) (WB)
 RAMP B-4, B-1
 DRAWN BY CADD
 CHECKED BY MWP
 DATE 11/12/04

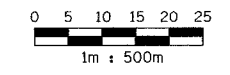
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(72-7)R-3	PEORIA	1300	411
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

RAMP B4 STATION	ELEVATIONS					RAMP B1 STATION
	1	2	3	4	5	
10+430.000	196.528	196.530	197.825	196.530	196.528	10+712.224
10+435.000	196.371	196.373	197.669	196.374	196.372	10+707.224
10+440.000	196.215	196.217	197.513	196.218	196.216	10+702.224
10+445.000	196.059	196.061	197.357	196.062	196.060	10+697.224
10+450.000	195.903	195.905	197.200	195.905	195.903	10+692.224
10+455.000	195.747	195.749	197.044	195.749	195.747	10+687.224
10+460.000	195.591	195.593	196.888	195.593	195.591	10+682.224
10+465.000	195.435	195.437	196.732	195.437	195.435	10+677.224
10+470.000	195.279	195.281	196.576	195.281	195.279	10+672.224
10+475.000	195.122	195.124	196.420	195.125	195.123	10+667.224
10+480.000	194.966	194.968	196.264	194.969	194.967	10+662.224
10+485.000	194.810	194.812	196.108	194.813	194.811	10+657.224
10+490.000	194.654	194.656	195.952	194.657	194.655	10+652.224
10+495.000	194.498	194.500	195.795	194.500	194.498	10+647.224
10+500.000	194.342	194.344	195.639	194.344	194.342	10+642.224
10+505.000	194.186	194.188	195.483	194.188	194.186	10+637.224
10+510.000	194.030	194.032	195.327	194.032	194.030	10+632.224
10+515.000	193.873	193.875	195.171	193.876	193.874	10+627.224
10+520.000	193.717	193.719	195.015	193.720	193.718	10+622.224
10+525.000	193.561	193.563	194.859	193.564	193.562	10+617.224
10+530.000	193.405	193.407	194.703	193.408	193.406	10+612.224
10+535.000	193.249	193.251	194.547	193.252	193.250	10+607.224
10+540.000	193.093	193.095	194.390	193.095	193.093	10+602.224
10+545.000	192.937	192.939	194.234	192.939	192.937	10+597.224
10+550.000	192.781	192.783	194.078	192.783	192.781	10+592.224
10+555.000	192.628	192.630	193.925	192.627	192.625	10+587.224
10+560.000	192.482	192.484	193.779	192.471	192.469	10+582.224
10+565.000	192.342	192.344	193.639	192.315	192.313	10+577.224
10+570.000	192.210	192.212	193.507	192.159	192.157	10+572.224
10+575.000	192.084	192.086	193.381	192.003	192.001	10+567.224
10+580.000	191.965	191.967	193.262	191.851	191.849	10+562.224
10+585.000	191.852	191.854	193.149	191.701	191.699	10+557.224
10+590.000	191.746	191.748	193.043	191.555	191.553	10+552.224
10+595.000	191.647	191.649	192.944	191.412	191.410	10+547.224
10+600.000	191.555	191.557	192.852	191.273	191.271	10+542.224
10+605.000	191.466	191.468	192.763	191.136	191.134	10+537.224

RAMP B4 STATION	ELEVATIONS					RAMP B1 STATION
	1	2	3	4	5	
10+610.000	191.377	191.352	192.647	191.003	191.001	10+532.224
10+615.000	191.288	191.230	192.525	190.873	190.871	10+527.224
10+620.000	191.199	191.146	192.441	190.743	190.741	10+522.224
10+625.000	191.110	190.984	192.279	190.642	190.641	10+517.224
10+630.000	191.021	190.866	192.161	190.546	190.481	10+512.224
10+635.000	190.932	190.742	192.037	190.451	190.351	10+507.224
10+640.000	190.843	190.619	191.914	190.358	190.221	10+502.224
10+645.000	190.754	190.496	191.791	190.275	190.091	10+497.224
10+650.000	190.665	190.370	191.665	190.186	189.961	10+492.224
10+655.000	190.576	190.234	191.529	190.108	189.843	10+487.224
10+660.000	190.487	190.098	191.393	190.038	189.733	10+482.224
10+665.000	190.398	189.963	191.263	189.968	189.623	10+477.224
10+670.000	190.309	189.833	191.200	189.905	189.513	10+472.224
10+675.000	190.220	189.697	191.090	189.795	189.403	10+467.224
10+680.000	190.131	189.608	190.980	189.685	189.293	10+462.224
10+685.000	190.041	189.518	190.870	189.575	189.183	10+457.224
10+690.000	189.952	189.429	190.761	189.466	189.074	10+452.224
10+695.000	189.863	189.340	190.652	189.357	188.965	10+447.224
10+700.000	189.774	189.251	190.546	189.248	188.856	10+442.224
10+705.000	189.685	189.162	190.457	189.139	188.747	10+437.224
10+710.000	189.596	189.073	190.368	189.031	188.639	10+432.224
10+715.000	189.507	188.984	190.279	188.923	188.531	10+427.224
10+720.000	189.418	188.895	190.190	188.816	188.454	10+422.224
10+725.000	189.329	188.806	190.101	188.709	188.317	10+417.224
10+730.000	189.240	188.717	190.012	188.602	188.210	10+412.224
10+735.000	189.151	188.628	189.923	188.496	188.104	10+407.224
10+740.000	189.062	188.539	189.834	188.390	187.998	10+402.224
10+745.000	188.973	188.450	189.745	188.285	187.893	10+397.224
10+750.000	188.884	188.361	189.656	188.180	187.788	10+392.224
10+755.000	188.795	188.272	189.567	188.113	187.684	10+387.224
10+760.000	188.706	188.230	189.525	188.047	187.580	10+382.224
10+765.000	188.617	188.188	189.483	187.981	187.477	10+377.224
10+770.000	188.528	188.139	189.434	187.915	187.374	10+372.224
10+775.000	188.439	188.097	189.392	187.850	187.272	10+367.224
10+780.000	188.350	188.055	189.350	187.785	187.170	10+362.224
10+785.000	188.261	188.003	189.298	187.715	187.061	10+357.224



- NOTES:
1. BARRIER BASE WIDTH EQUALS 0.685m
 2. BARRIER HEIGHT EQUALS 1.295m ON HIGH SIDE OF SHOULDER. ON LOW SIDE OF SHOULDER, HEIGHT EQUALS DIFFERENCE BETWEEN SHOULDER ELEVATION AND TOP OF BARRIER ELEVATION.
 3. BARRIER ON LOW SIDE OF SHOULDER RETAINS GM-SHAPE TO 1.295m HIGH THEN PROVIDES VERTICAL FACE TO ACHIEVE REQUIRED HEIGHT. SEE CONCRETE BARRIER, DOUBLE FACE (SPECIAL) DETAILS.



PARSONS
TRANSPORTATION GROUP
Engineers and Planners

REVISIONS	
NAME	DATE

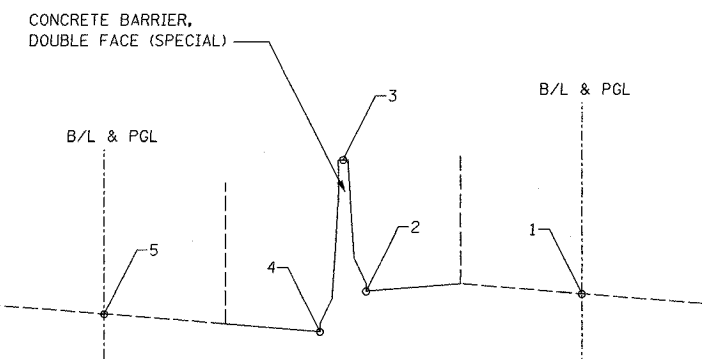
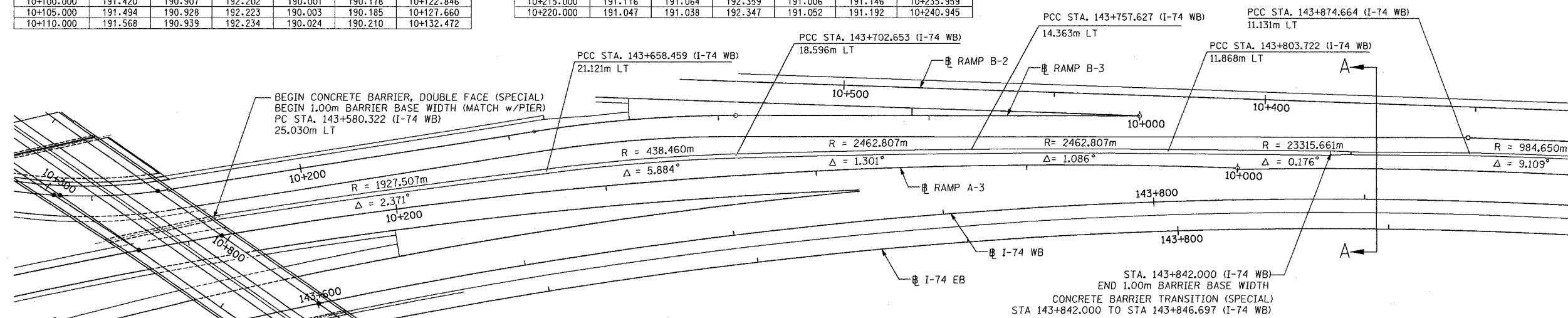
ILLINOIS DEPARTMENT OF TRANSPORTATION
CONCRETE BARRIER ELEVATIONS
FAI ROUTE 74 (I-74) (WB)
RAMP B-4, B-1
DATE 11/12/04
DRAWN BY CADD
CHECKED BY MWP

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(72-7)R-3	PEORIA	1360	412
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

RAMP B3 STATION	ELEVATIONS					RAMP A3 STATION
	1	2	3	4	5	
10+000.000	189.939	189.743	191.038	188.933	189.056	10+023.269
10+005.000	190.010	189.815	191.110	188.993	189.114	10+028.256
10+010.000	190.081	189.886	191.181	189.051	189.171	10+033.243
10+015.000	190.153	189.958	191.253	189.108	189.228	10+038.231
10+020.000	190.224	190.030	191.325	189.166	189.286	10+043.218
10+025.000	190.304	190.109	191.404	189.224	189.344	10+048.206
10+030.000	190.384	190.188	191.483	189.281	189.402	10+053.193
10+035.000	190.448	190.250	191.545	189.338	189.460	10+058.180
10+040.000	190.511	190.312	191.607	189.394	189.518	10+063.167
10+045.000	190.583	190.382	191.677	189.450	189.576	10+068.154
10+050.000	190.655	190.452	191.747	189.504	189.632	10+073.141
10+055.000	190.742	190.535	191.830	189.559	189.690	10+078.127
10+060.000	190.828	190.618	191.913	189.613	189.748	10+083.113
10+065.000	190.902	190.689	191.984	189.667	189.806	10+088.099
10+070.000	190.976	190.729	192.024	189.721	189.864	10+093.084
10+075.000	191.050	190.774	192.069	189.774	189.922	10+098.068
10+080.000	191.124	190.814	192.109	189.826	189.979	10+103.052
10+085.000	191.198	190.853	192.148	189.877	190.036	10+108.036
10+090.000	191.272	190.884	192.179	189.929	190.094	10+113.019
10+095.000	191.346	190.896	192.191	189.980	190.151	10+118.000
10+100.000	191.420	190.907	192.202	190.001	190.178	10+122.846
10+105.000	191.494	190.928	192.223	190.003	190.185	10+127.660
10+110.000	191.568	190.939	192.234	190.024	190.210	10+132.472

RAMP B3 STATION	ELEVATIONS					RAMP A3 STATION
	1	2	3	4	5	
10+115.000	191.642	190.999	192.294	190.064	190.252	10+137.282
10+120.000	191.715	191.070	192.365	190.105	190.294	10+142.091
10+125.000	191.782	191.175	192.470	190.148	190.336	10+146.900
10+130.000	191.835	191.258	192.553	190.191	190.378	10+151.709
10+135.000	191.873	191.331	192.632	190.237	190.420	10+156.519
10+140.000	191.897	191.394	192.689	190.283	190.462	10+161.329
10+145.000	191.906	191.447	192.742	190.333	190.507	10+166.180
10+150.000	191.901	191.478	192.773	190.384	190.553	10+171.163
10+155.000	191.882	191.500	192.795	190.434	190.599	10+176.146
10+160.000	191.849	191.498	192.793	190.484	190.645	10+181.129
10+165.000	191.801	191.474	192.769	190.533	190.691	10+186.110
10+170.000	191.739	191.435	192.730	190.583	190.737	10+191.097
10+175.000	191.670	191.381	192.676	190.631	190.783	10+196.081
10+180.000	191.601	191.334	192.629	190.678	190.827	10+201.065
10+185.000	191.531	191.281	192.576	190.726	190.873	10+206.049
10+190.000	191.462	191.234	192.529	190.774	190.919	10+211.034
10+195.000	191.393	191.176	192.471	190.821	190.964	10+216.019
10+200.000	191.324	191.142	192.437	190.868	191.010	10+221.004
10+205.000	191.255	191.117	192.412	190.914	191.055	10+225.989
10+210.000	191.185	191.090	192.385	190.961	191.101	10+230.974
10+215.000	191.116	191.064	192.359	191.006	191.146	10+235.959
10+220.000	191.047	191.038	192.347	191.052	191.192	10+240.945

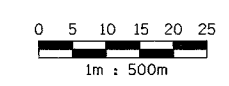
RAMP B2 STATION	ELEVATIONS					RAMP A3 STATION
	1	2	3	4	5	
10+410.000	189.573	189.537	190.832	188.697	188.831	10+003.986
10+415.000	189.648	189.592	190.887	188.759	188.889	10+008.972
10+420.000	189.722	189.651	190.946	188.820	188.947	10+013.960
10+425.000	189.796	189.743	191.038	188.881	189.005	10+018.948



SECTION A - A

NOTES:

- BARRIER BASE WIDTH EQUALS 0.685m, UNLESS OTHERWISE NOTED
- BARRIER HEIGHT EQUALS 1.295m ON HIGH SIDE OF SHOULDER, ON LOW SIDE OF SHOULDER, HEIGHT EQUALS DIFFERENCE BETWEEN SHOULDER ELEVATION AND TOP OF BARRIER ELEVATION.
- BARRIER ON LOW SIDE OF SHOULDER RETAINS GM-SHAPE TO 1.295m HIGH THEN PROVIDES VERTICAL FACE TO ACHIEVE REQUIRED HEIGHT. SEE CONCRETE BARRIER, DOUBLE FACE (SPECIAL) DETAILS.



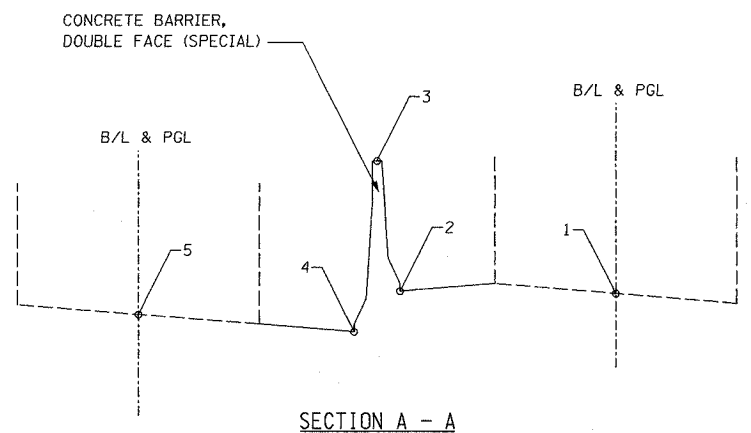
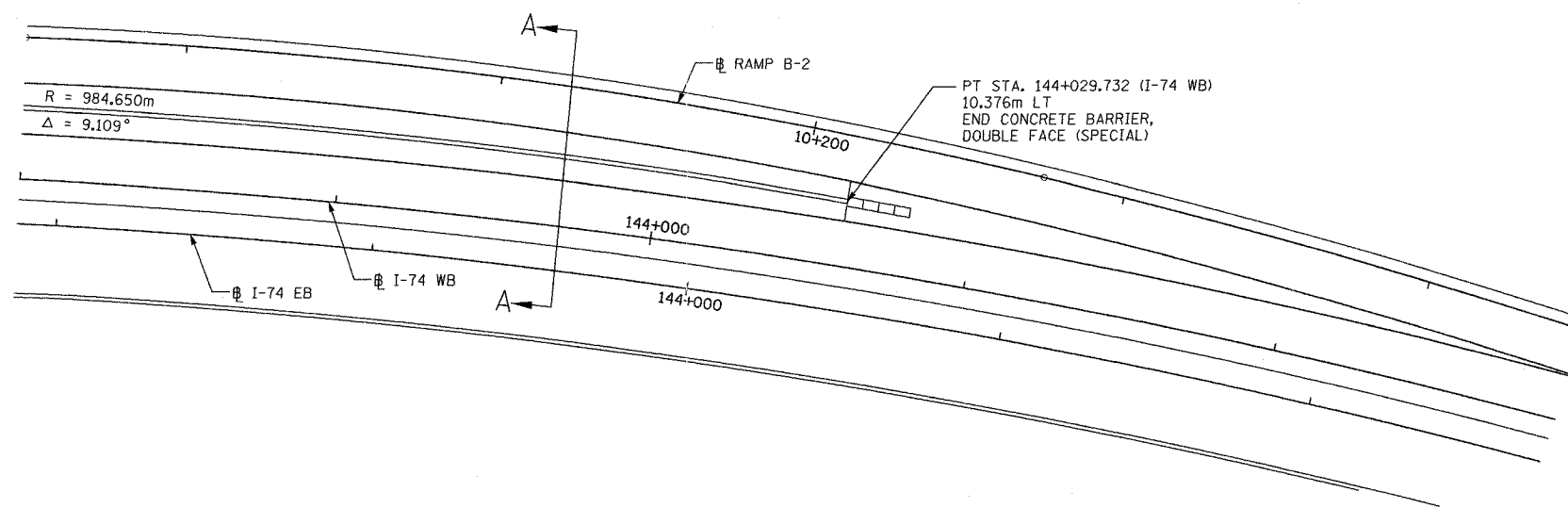
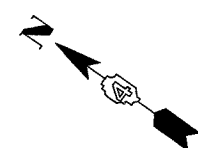
PARSONS TRANSPORTATION GROUP
Engineers and Planners

REVISIONS	
NAME	DATE

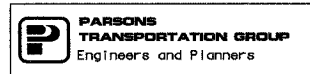
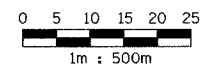
ILLINOIS DEPARTMENT OF TRANSPORTATION
CONCRETE BARRIER ELEVATIONS
FAI ROUTE 74 (I-74) (WB)
RAMP A-3, B-2, B-3
DRAWN BY CADD
CHECKED BY MWP
DATE 11/12/04

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(72-7)R-3	PEORIA	1360	413
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

RAMP B2 STATION	ELEVATIONS					I-74 WB STATION
	1	2	3	4	5	
10+195.000	186.842	186.441	187.744	186.449	186.246	144+027.446
10+200.000	186.901	186.495	187.800	186.505	186.305	144+022.555
10+205.000	186.961	186.552	187.856	186.561	186.364	144+017.665
10+210.000	187.020	186.607	187.912	186.617	186.423	144+012.776
10+215.000	187.080	186.664	187.968	186.673	186.482	144+007.888
10+220.000	187.139	186.719	188.025	186.730	186.541	144+003.001
10+225.000	187.198	186.775	188.082	186.787	186.600	143+998.115
10+230.000	187.258	186.833	188.138	186.843	186.659	143+993.229
10+235.000	187.317	186.889	188.195	186.900	186.718	143+988.345
10+240.000	187.377	186.947	188.252	186.957	186.777	143+983.460
10+245.000	187.436	187.004	188.308	187.013	186.835	143+978.577
10+250.000	187.495	187.062	188.365	187.070	186.893	143+973.693
10+255.000	187.555	187.120	188.420	187.125	186.950	143+968.811
10+260.000	187.614	187.178	188.476	187.181	187.007	143+963.928
10+265.000	187.673	187.236	188.532	187.237	187.064	143+959.046
10+270.000	187.733	187.295	188.590	187.291	187.119	143+954.164
10+275.000	187.792	187.354	188.649	187.345	187.174	143+949.282
10+280.000	187.852	187.414	188.709	187.399	187.229	143+944.401
10+285.000	187.911	187.473	188.768	187.453	187.283	143+939.519
10+290.000	187.970	187.532	188.827	187.506	187.337	143+934.638
10+295.000	188.030	187.592	188.887	187.559	187.390	143+929.757
10+300.000	188.089	187.652	188.947	187.611	187.442	143+924.875
10+305.000	188.149	187.713	189.008	187.663	187.494	143+919.993
10+310.000	188.208	187.773	189.068	187.715	187.546	143+915.111
10+315.000	188.267	187.833	189.128	187.767	187.597	143+910.229
10+320.000	188.327	187.893	189.188	187.817	187.647	143+905.346
10+325.000	188.386	188.086	189.381	187.868	187.697	143+900.463



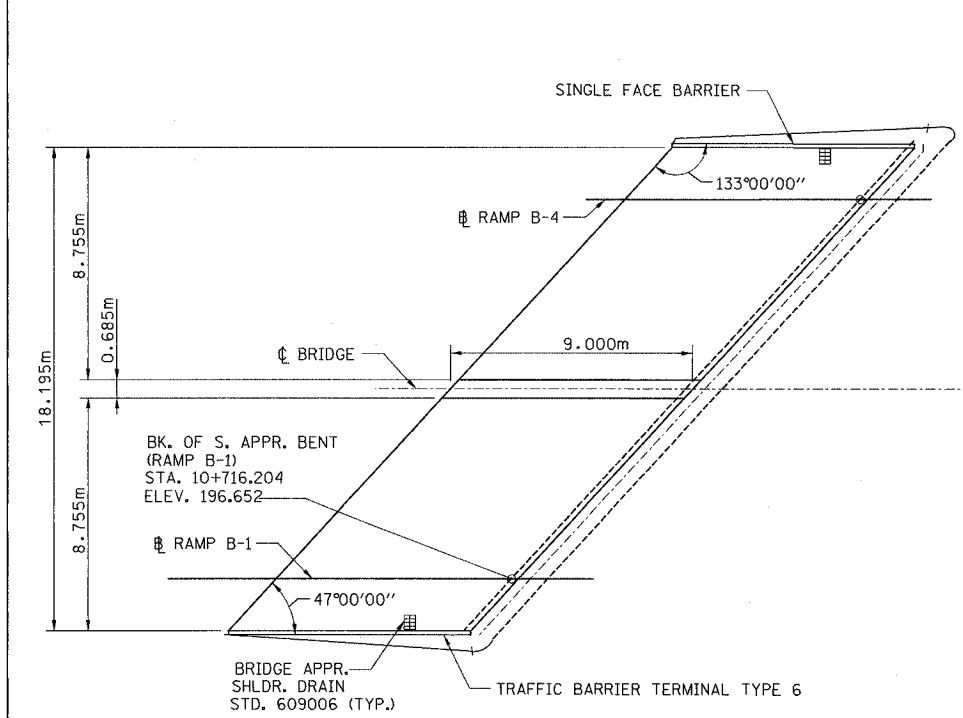
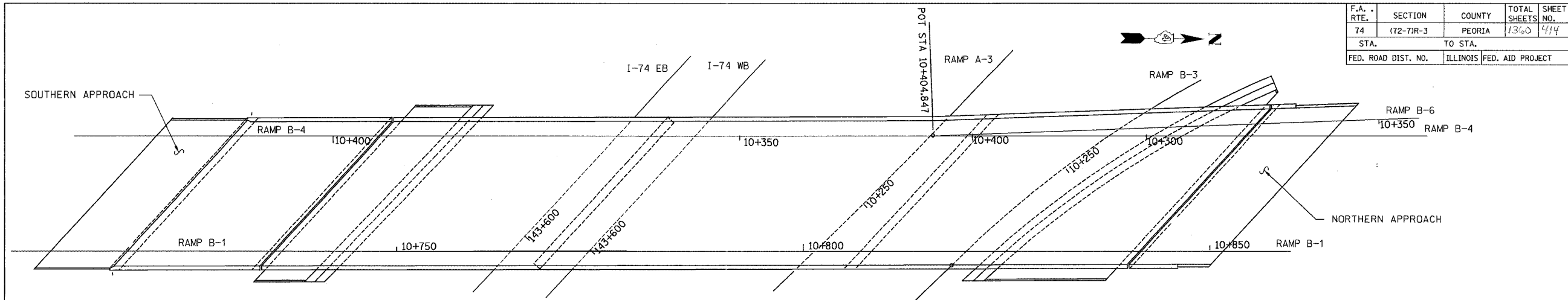
- NOTES:
1. BARRIER BASE WIDTH EQUALS 0.685m, UNLESS OTHERWISE NOTED
 2. BARRIER HEIGHT EQUALS 1.295m ON HIGH SIDE OF SHOULDER, ON LOW SIDE OF SHOULDER, HEIGHT EQUALS DIFFERENCE BETWEEN SHOULDER ELEVATION AND TOP OF BARRIER ELEVATION.
 3. BARRIER ON LOW SIDE OF SHOULDER RETAINS GM-SHAPE TO 1.295m HIGH THEN PROVIDES VERTICAL FACE TO ACHIEVE REQUIRED HEIGHT. SEE CONCRETE BARRIER, DOUBLE FACE (SPECIAL) DETAILS.



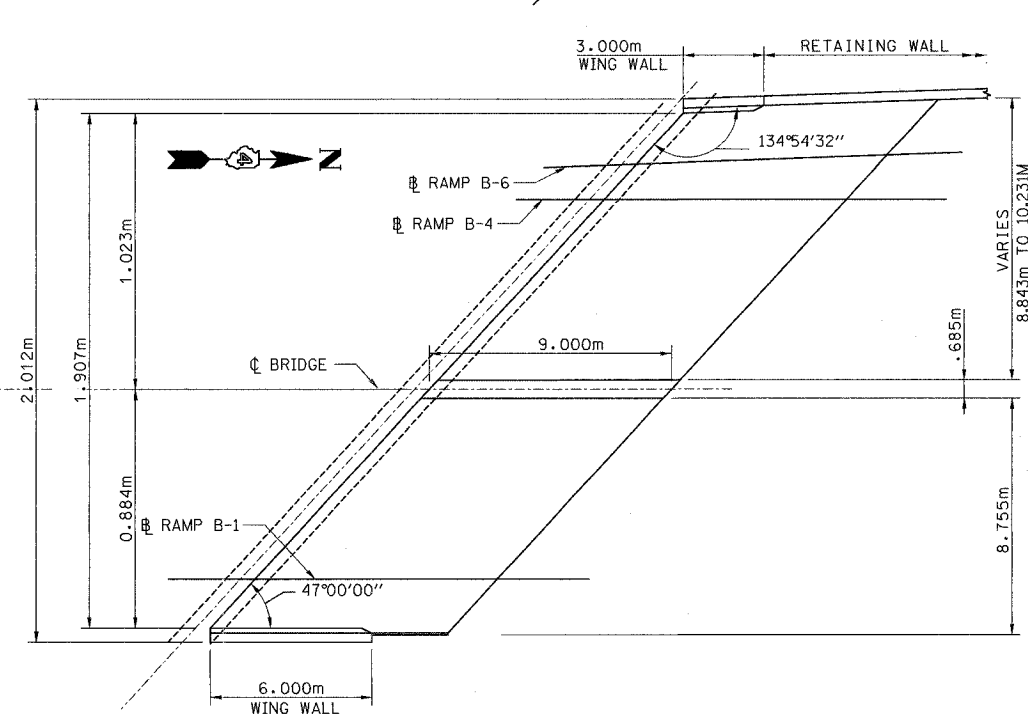
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 CONCRETE BARRIER ELEVATIONS
 FAI ROUTE 74 (I-74) (WB)
 RAMP A-3, B-2, B-3
 DRAWN BY CADD
 CHECKED BY MWP
 DATE 11/12/04

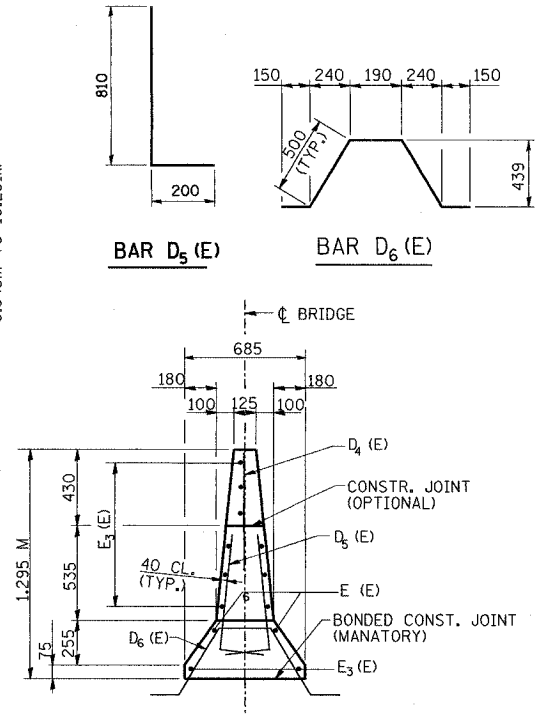
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(72-71R-3)	PEORIA	1360	414
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



SLAB 1 SOUTH WEST
NTS



SLAB 1 NORTH WEST
NTS

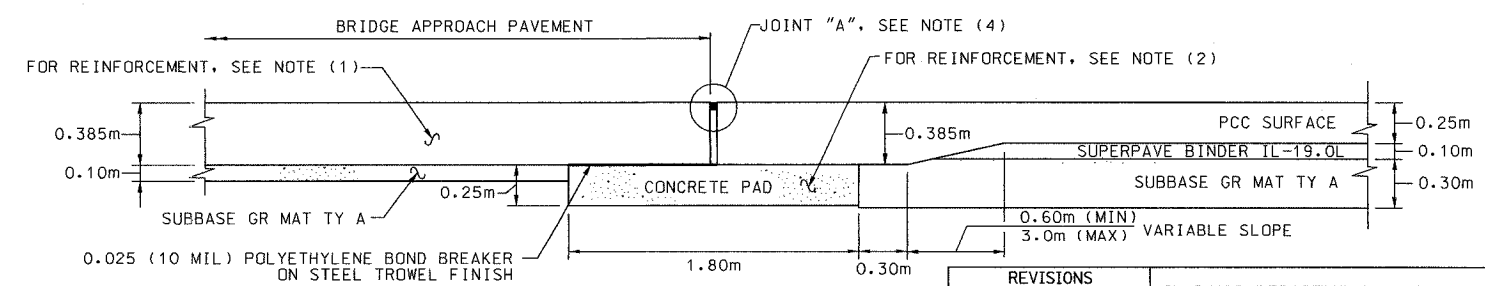


SECTION THRU MEDIAN BARRIER

NOTES:

- FOR APPROACH SLAB REINFORCEMENT SEE SHT. 1 & 3 OF STANDARD 420401, "BRIDGE APPROACH PAVEMENT".
- FOR CONCRETE PAD REINFORCEMENT SEE SEC G-G RIGID PAVEMENT OF STANDARD 420401, "BRIDGE APPROACH PAVEMENT".
- REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.
- FOR JOINT "A" DETAIL, SEE PROJECT STANDARD 420402-174, "DETAIL A".
- COST OF REINFORCEMENT AND FLARED COIL LOOPS SHALL BE INCLUDED IN THE SQUARE METER COST OF BRIDGE APPROACH PAVEMENT.

PAY ITEM CODE	ITEM	UNIT	QUANTITY
M4205050	BRIDGE APPROACH PAVEMENT (SPECIAL)	SQ M	334.5



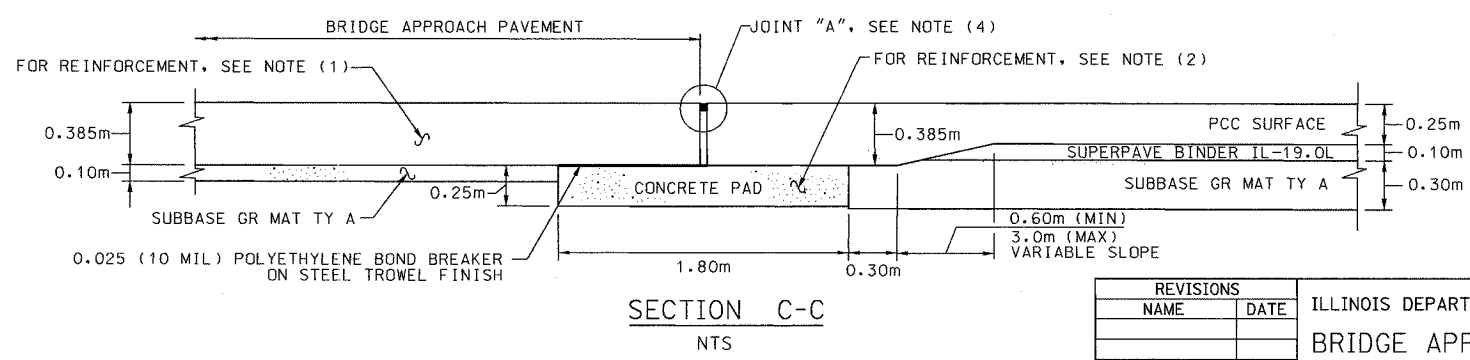
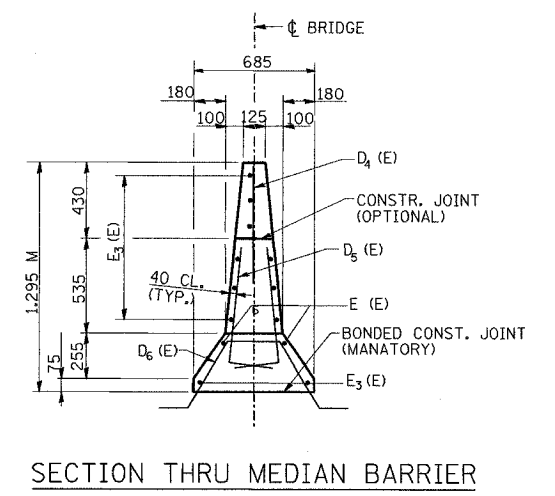
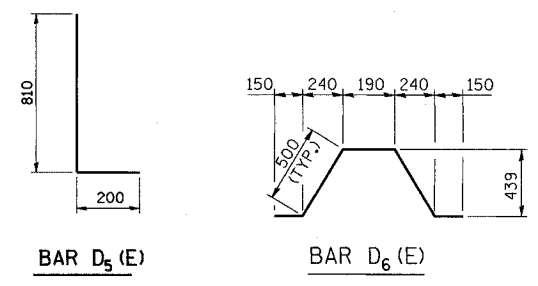
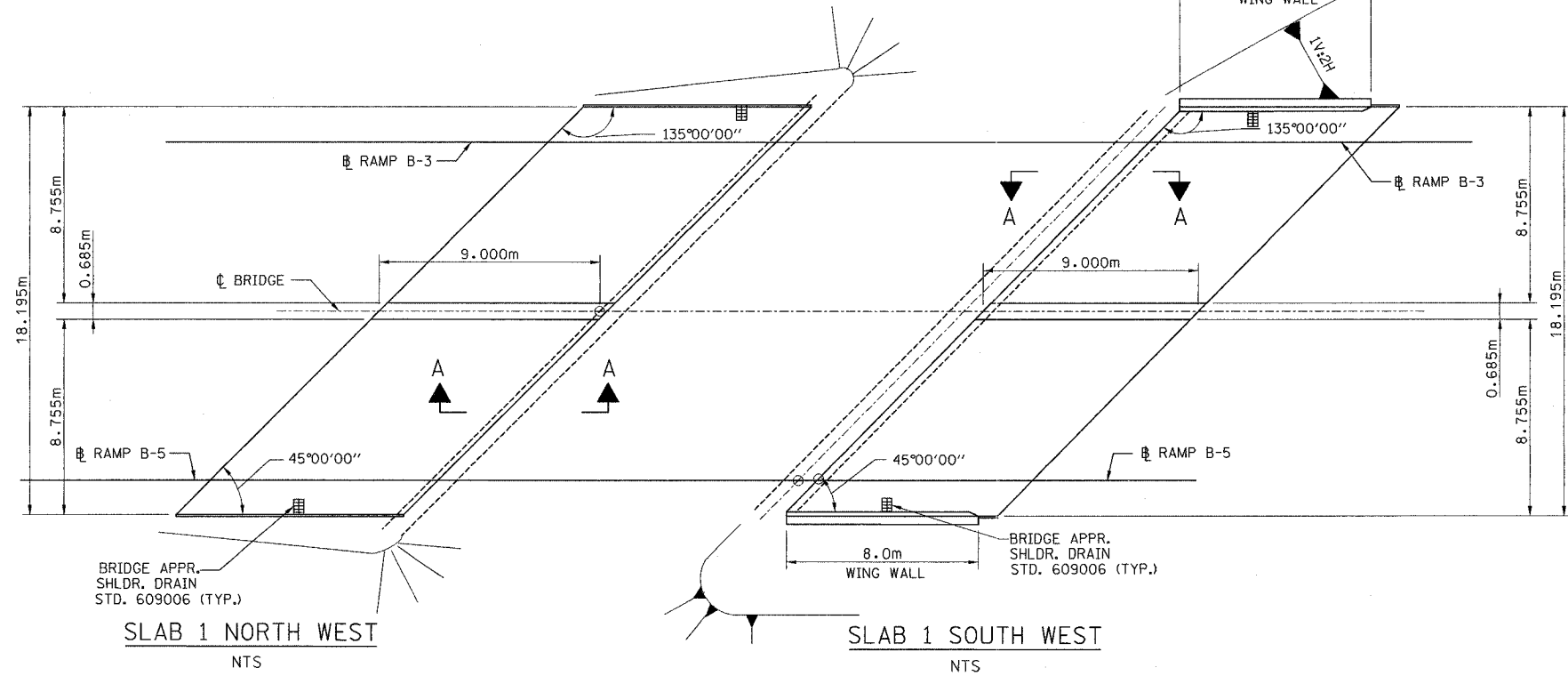
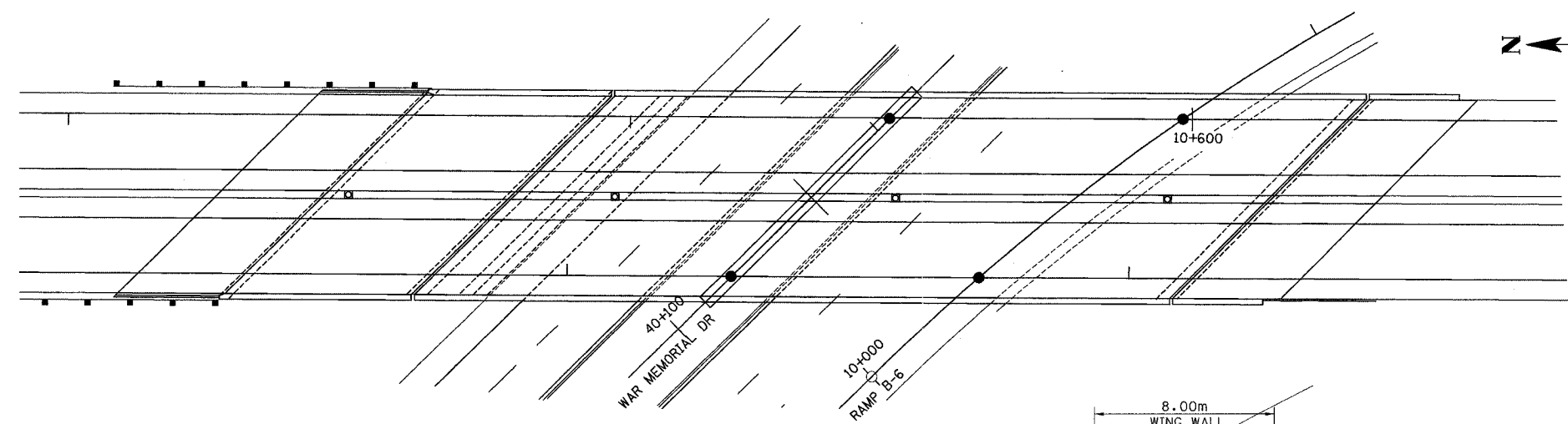
SECTION C-C
NTS

REVISIONS	NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 BRIDGE APPROACH PAVEMENT
 FAI ROUTE 74 (I-74) (WB)
 RAMPS B-1/B-4 OVER I-74
 DRAWN BY CADD
 CHECKED BY MWP
 DATE 11/12/04

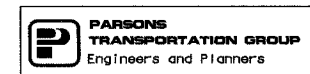


F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(72-71R-3)	PEORIA	1360	115
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			



- NOTES:
- FOR APPROACH SLAB REINFORCEMENT SEE SHT. 1 & 3 OF STANDARD 420401, "BRIDGE APPROACH PAVEMENT".
 - FOR CONCRETE PAD REINFORCEMENT SEE SEC G-G RIGID PAVEMENT OF STANDARD 420401, "BRIDGE APPROACH PAVEMENT".
 - REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.
 - FOR JOINT "A" DETAIL, SEE PROJECT STANDARD 420402-174, "DETAIL A".

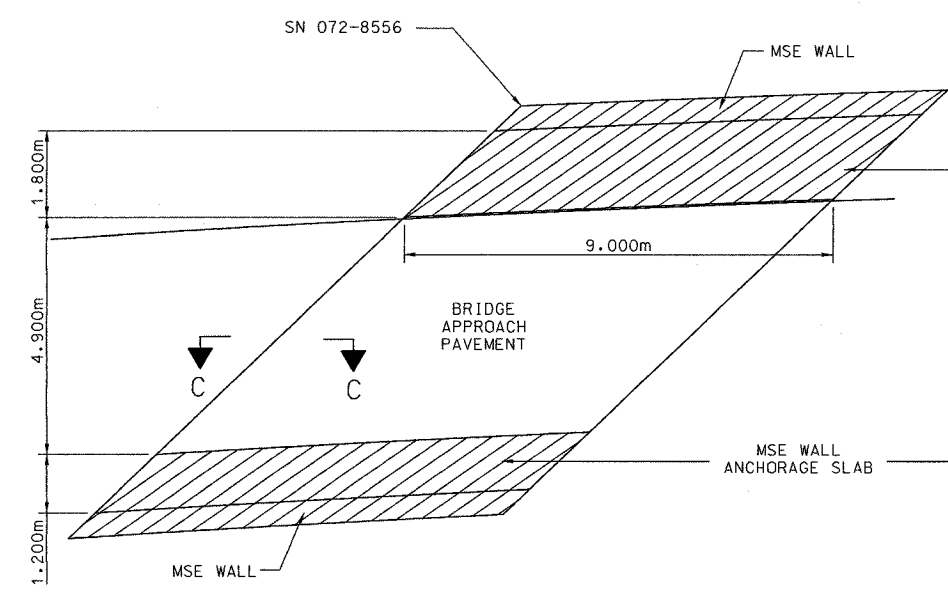
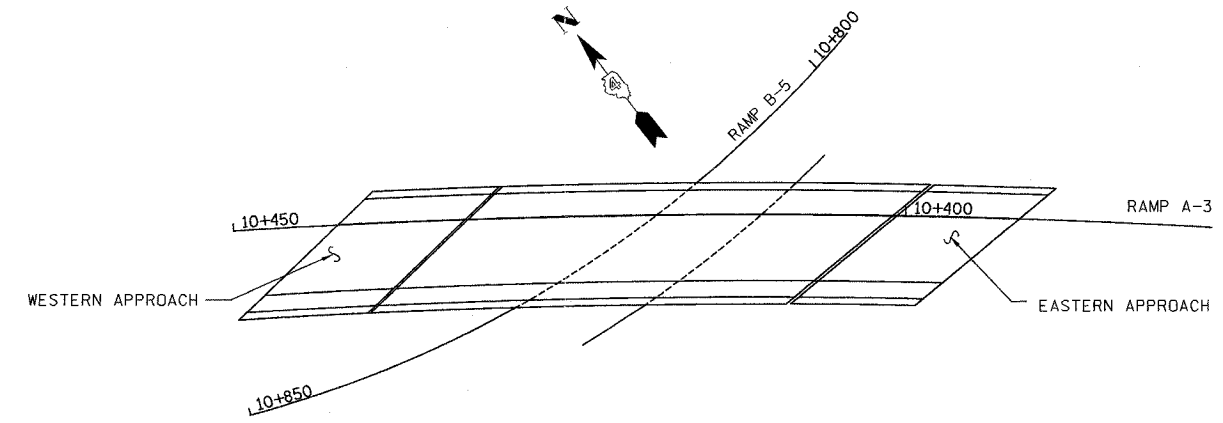
PAY ITEM CODE	ITEM	UNIT	QUANTITY
M4205050	BRIDGE APPROACH PAVEMENT (SPECIAL)	SQ M	327.5



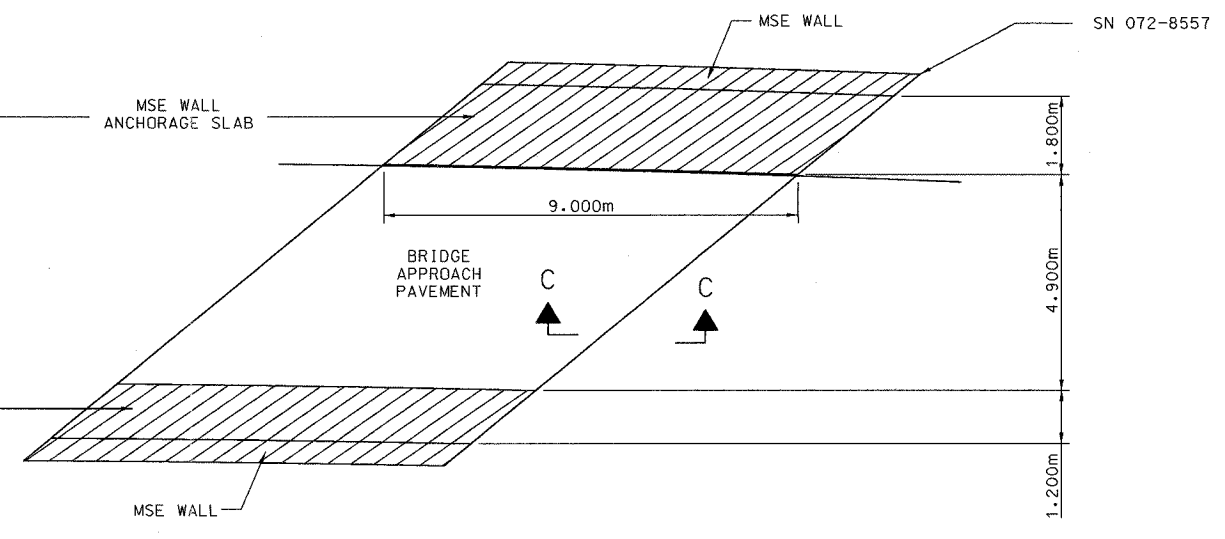
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 BRIDGE APPROACH PAVEMENT
 FAI ROUTE 74 (I-74) (WB)
 RAMPS B-3/B-5 OVER WAR MEMORIAL DRIVE
 DRAWN BY CADD
 CHECKED BY MWP
 DATE 11/12/04

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(72-71R-3)	PEORIA	1360	416
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



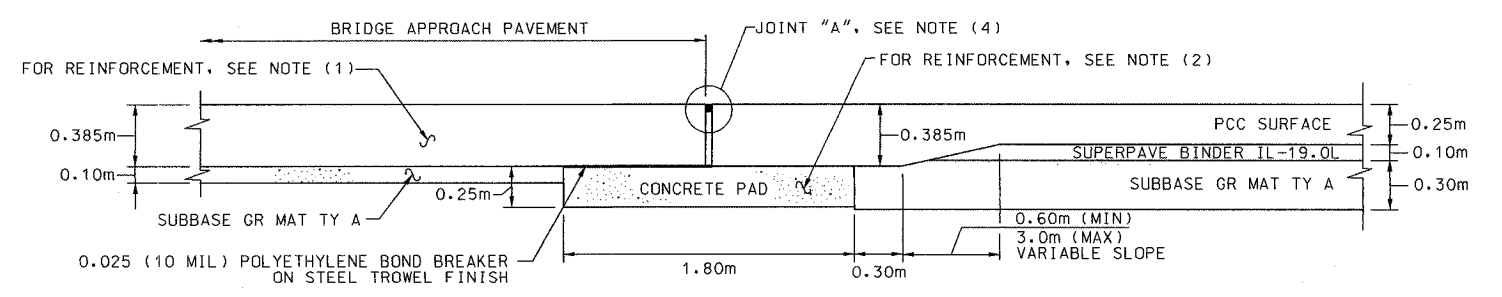
PLAN VIEW
WESTERN APPROACH
NTS



PLAN VIEW
EASTERN APPROACH
NTS

NOTES:

- FOR APPROACH SLAB REINFORCEMENT SEE SHT. 1 & 3 OF STANDARD 420401, "BRIDGE APPROACH PAVEMENT".
- FOR CONCRETE PAD REINFORCEMENT SEE SEC G-G RIGID PAVEMENT OF STANDARD 420401, "BRIDGE APPROACH PAVEMENT".
- REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.
- FOR JOINT "A" DETAIL, SEE PROJECT STANDARD 420402-174, "DETAIL A".
- COST OF REINFORCEMENT AND FLARED COIL LOOPS SHALL BE INCLUDED IN THE SQUARE METER COST OF BRIDGE APPROACH PAVEMENT.



SECTION C-C
NTS

PAY ITEM CODE	ITEM	UNIT	QUANTITY
M4205050	BRIDGE APPROACH PAVEMENT (SPECIAL)	SQ M	88.2

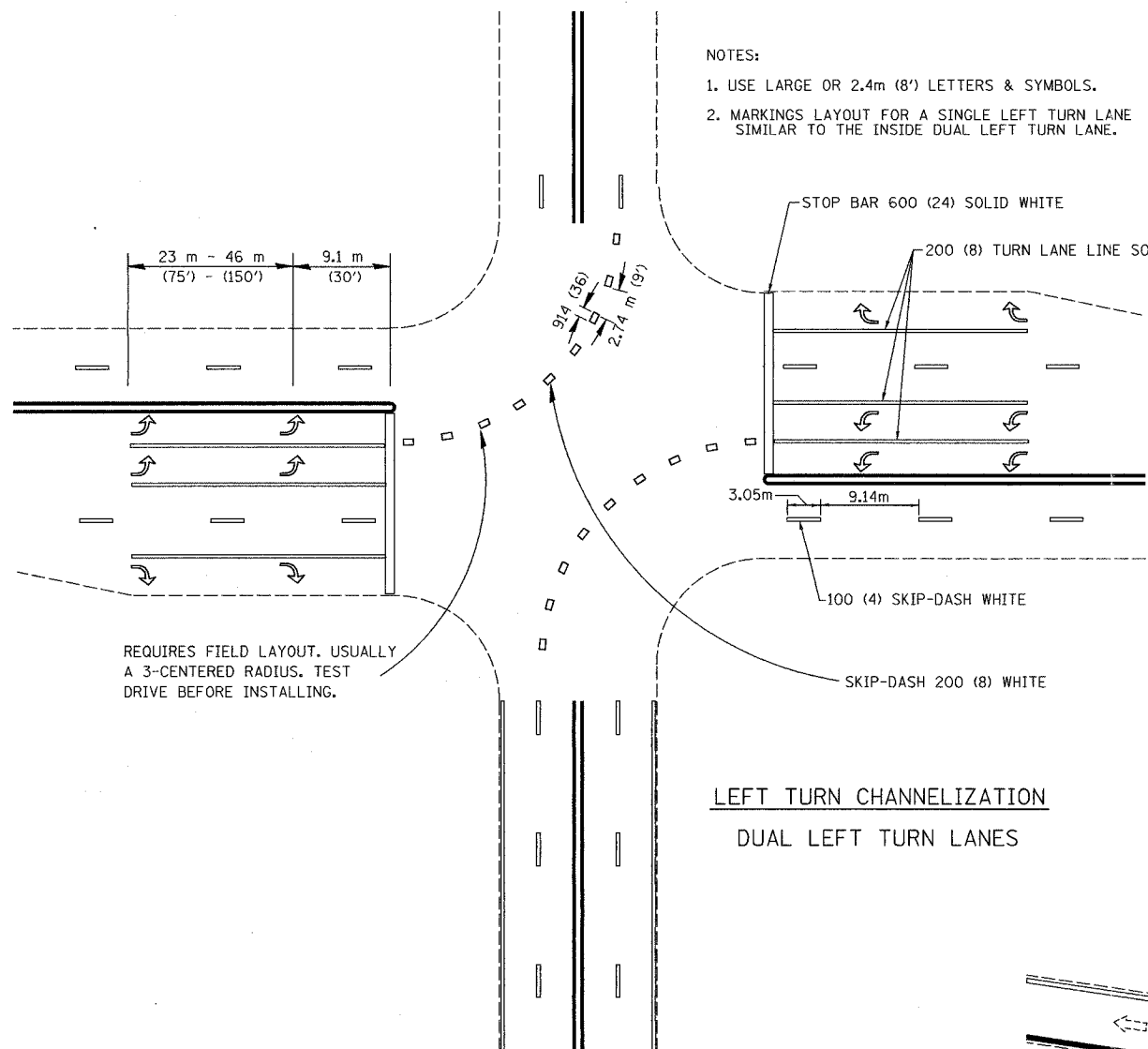
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
BRIDGE APPROACH PAVEMENT
FAI ROUTE 74 (I-74) (WB)
RAMP A-3 OVER RAMP B-5
DRAWN BY CADD
CHECKED BY MWP
DATE 11/12/04

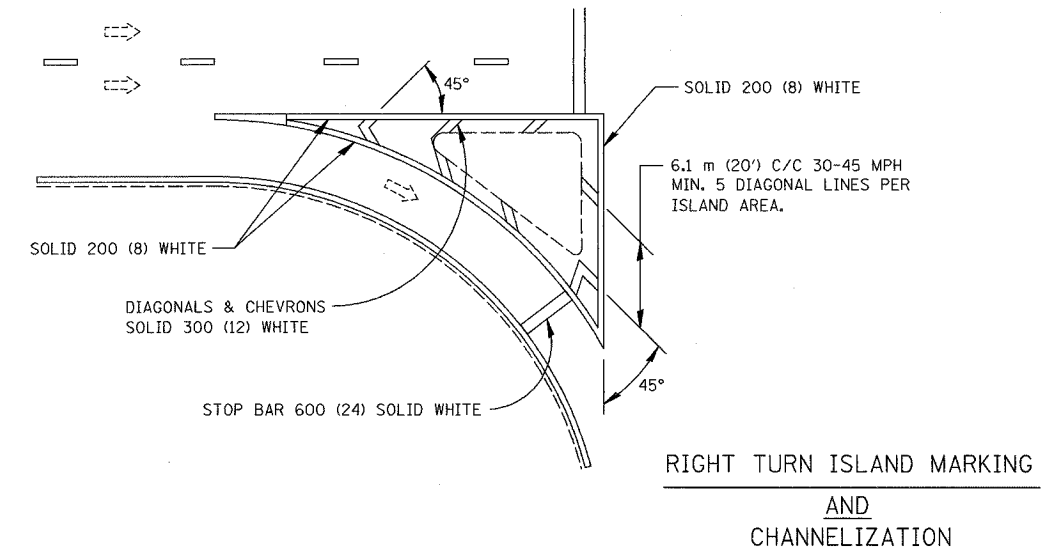


F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(72-7)R-3	PEORIA	1360	417
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

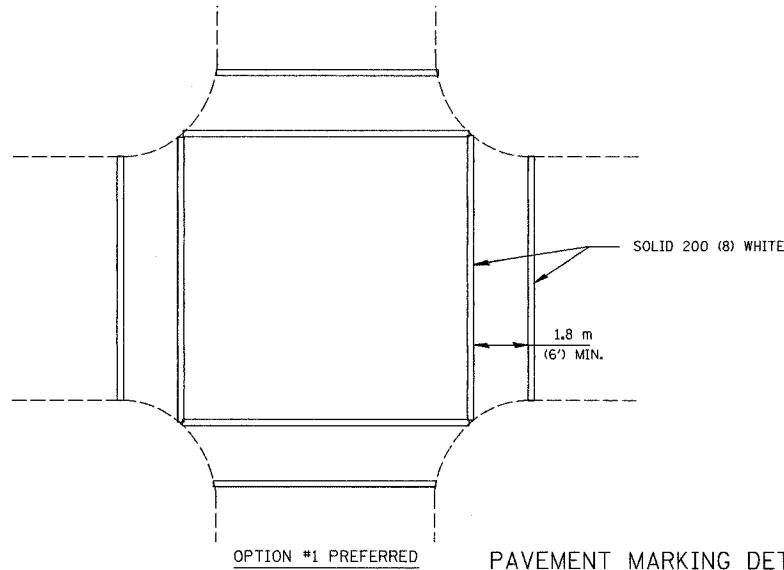
- NOTES:
1. USE LARGE OR 2.4m (8') LETTERS & SYMBOLS.
 2. MARKINGS LAYOUT FOR A SINGLE LEFT TURN LANE SIMILAR TO THE INSIDE DUAL LEFT TURN LANE.



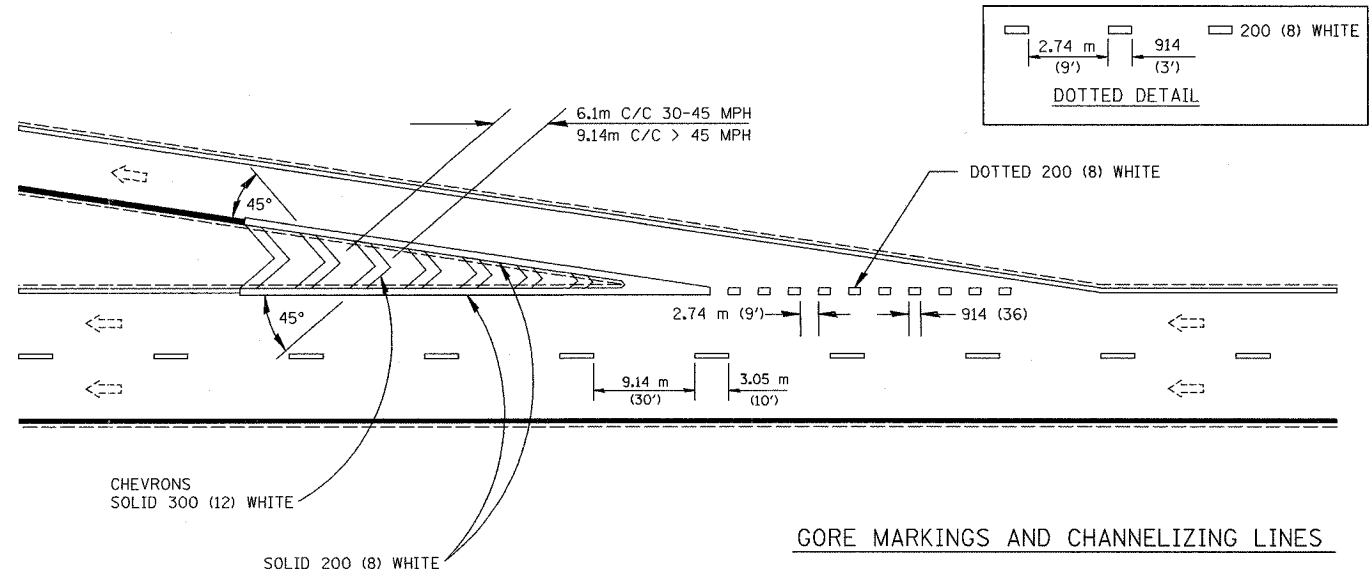
LEFT TURN CHANNELIZATION
DUAL LEFT TURN LANES



RIGHT TURN ISLAND MARKING
AND
CHANNELIZATION



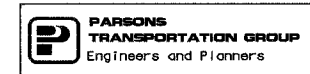
PAVEMENT MARKING DETAILS CROSSWALKS



GORE MARKINGS AND CHANNELIZING LINES

REVISIONS	
NAME	DATE

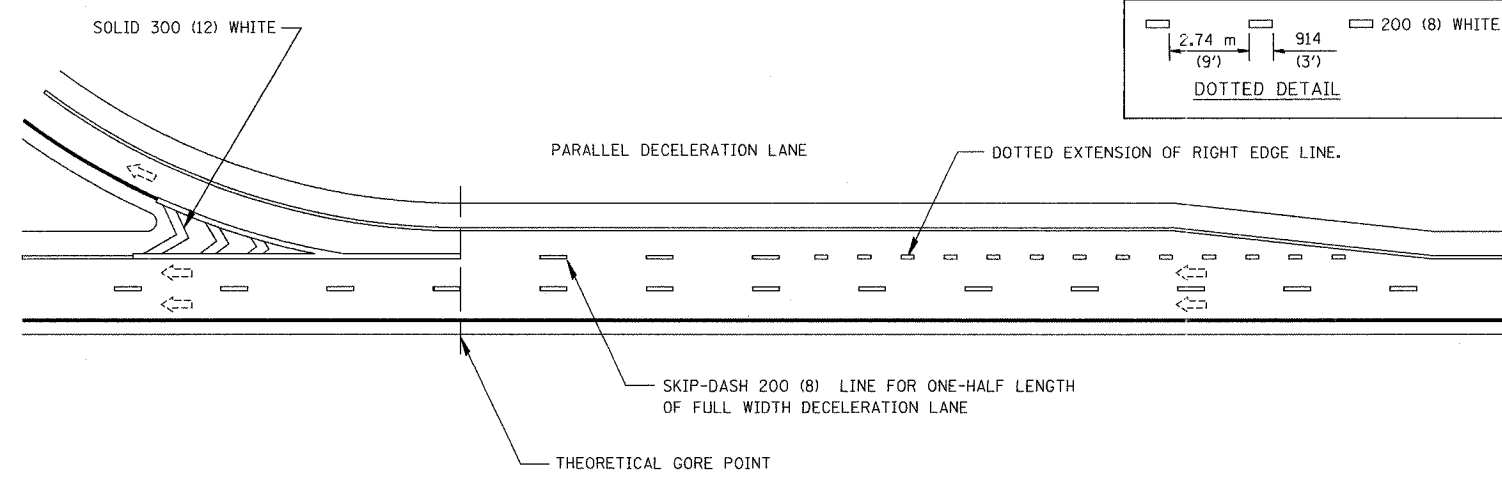
ILLINOIS DEPARTMENT OF TRANSPORTATION
PAVEMENT MARKING DETAILS
FAI ROUTE 74 (I-74) (WB)
WAR MEMORIAL DRIVE
DRAWN BY CADD
CHECKED BY KJW
DATE 11/12/04



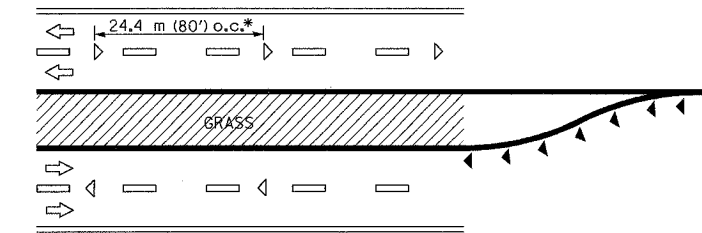
All dimensions are in millimeters (inches) unless otherwise noted.

NOT TO SCALE

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(72-71R-3)	PEORIA	1360	418
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

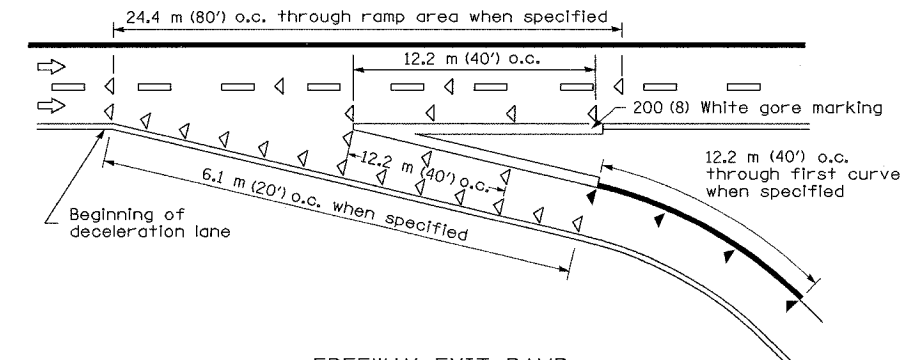
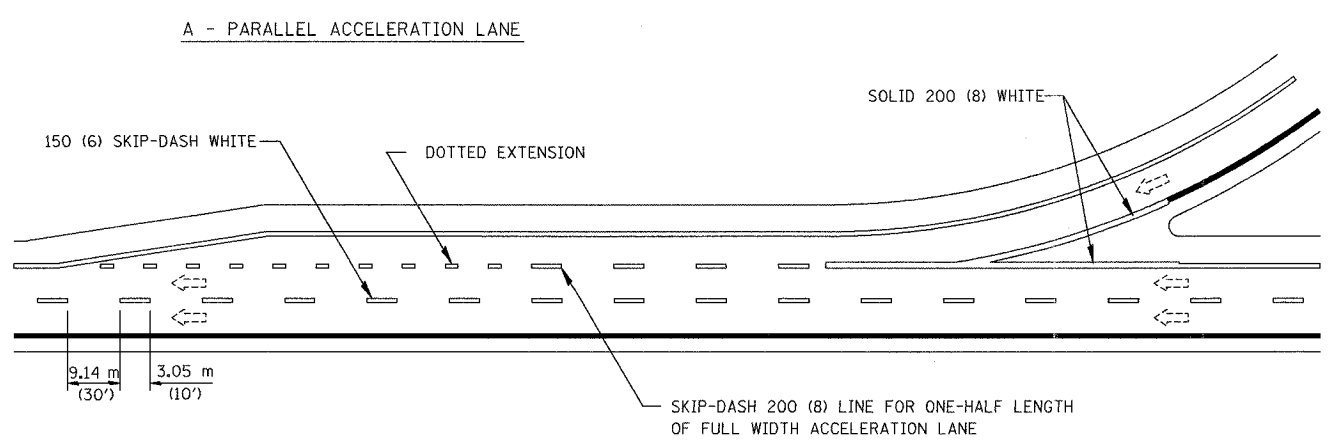


EXIT RAMP MARKINGS

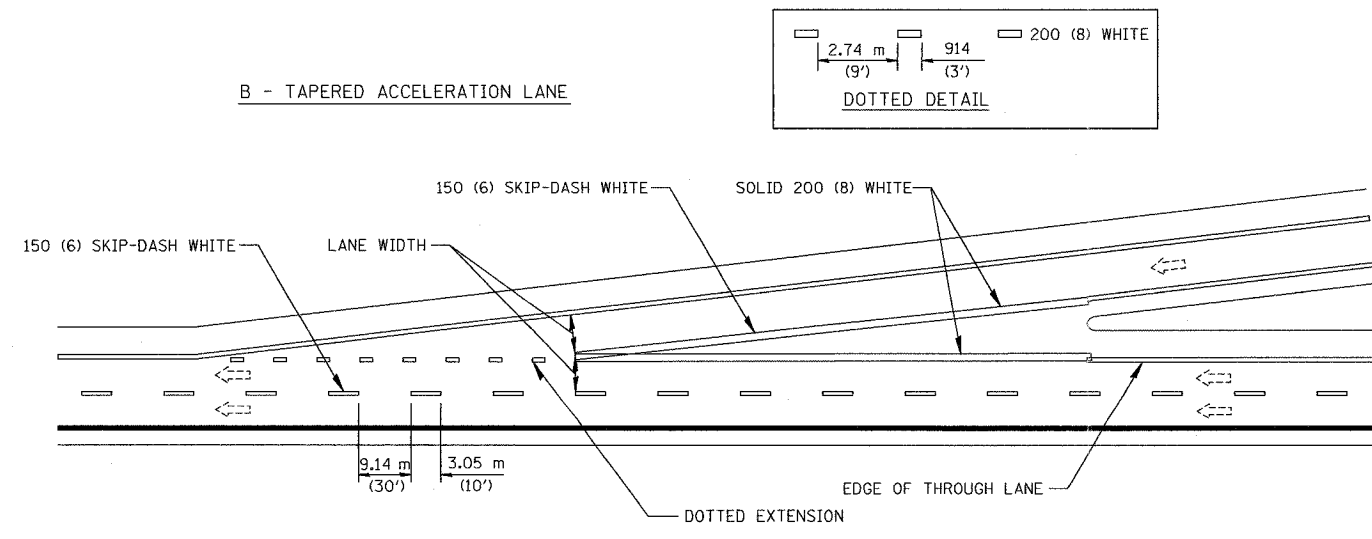


* Reduce to 12.2 m (40') o.c. on curves where advisory speeds are 15 km/h (10 mph) lower than posted speeds.

MULTI-LANE DIVIDED



FREEWAY EXIT RAMP



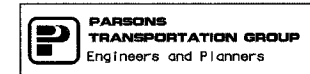
ENTRANCE RAMP MARKINGS

- SYMBOLS
- Yellow stripe
 - White stripe
 - One-way amber marker
 - One-way crystal marker
 - Two-way amber marker

RAISED REFLECTIVE PAVEMENT MARKERS

REVISIONS	
NAME	DATE

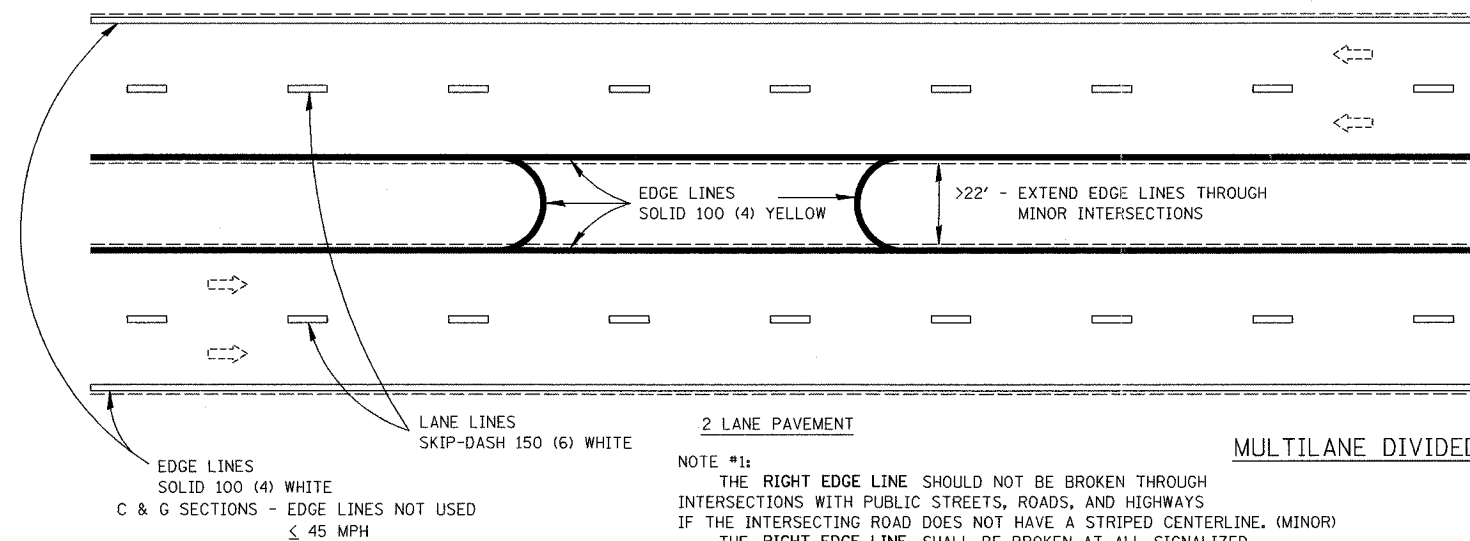
ILLINOIS DEPARTMENT OF TRANSPORTATION
PAVEMENT MARKING DETAILS
FAI ROUTE 74 (I-74) (WB)
WAR MEMORIAL DRIVE
DRAWN BY CADD
CHECKED BY KJW
DATE 11/12/04



All dimensions are in millimeters (inches) unless otherwise noted.

NOT TO SCALE

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(72-71R-3)	PEORIA	1360	419
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



NOTE #1:
THE RIGHT EDGE LINE SHOULD NOT BE BROKEN THROUGH INTERSECTIONS WITH PUBLIC STREETS, ROADS, AND HIGHWAYS IF THE INTERSECTING ROAD DOES NOT HAVE A STRIPED CENTERLINE. (MINOR)
THE RIGHT EDGE LINE SHALL BE BROKEN AT ALL SIGNALIZED INTERSECTIONS AND AT ALL INTERSECTIONS WHERE THE ROUTE BEING STRIPED HAS A STOP CONTROL. (MAJOR)

NOTE #2:
THE LEFT EDGE LINE SHALL BE CONTINUOUS THROUGH MEDIAN OPENINGS AND CROSSOVERS IF THE MEDIAN IS AT LEAST 22 FEET WIDE AND THE TRAFFIC CONTROL DOES NOT CONSIST OF TRAFFIC SIGNALS OR A FOUR-WAY STOP.

NOT TO SCALE

All dimensions are in millimeters (Inches) unless otherwise noted.



REVISIONS	
NAME	DATE

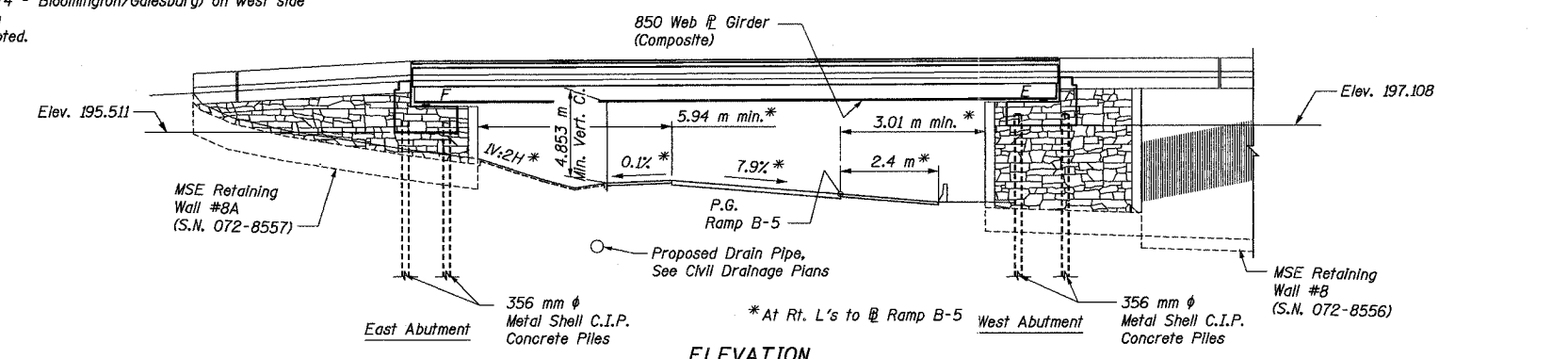
ILLINOIS DEPARTMENT OF TRANSPORTATION
PAVEMENT MARKING DETAILS
FAI ROUTE 74 (I-74) (WB)
WAR MEMORIAL DRIVE
DRAWN BY CADD
CHECKED BY K.W.
DATE 11/12/04

Date: 11/22/2004
 Time: 10:24:43 AM
 File: P:\643996\structure\072-0172\Sheet\Tracings\GP0001-A0720172.dgn

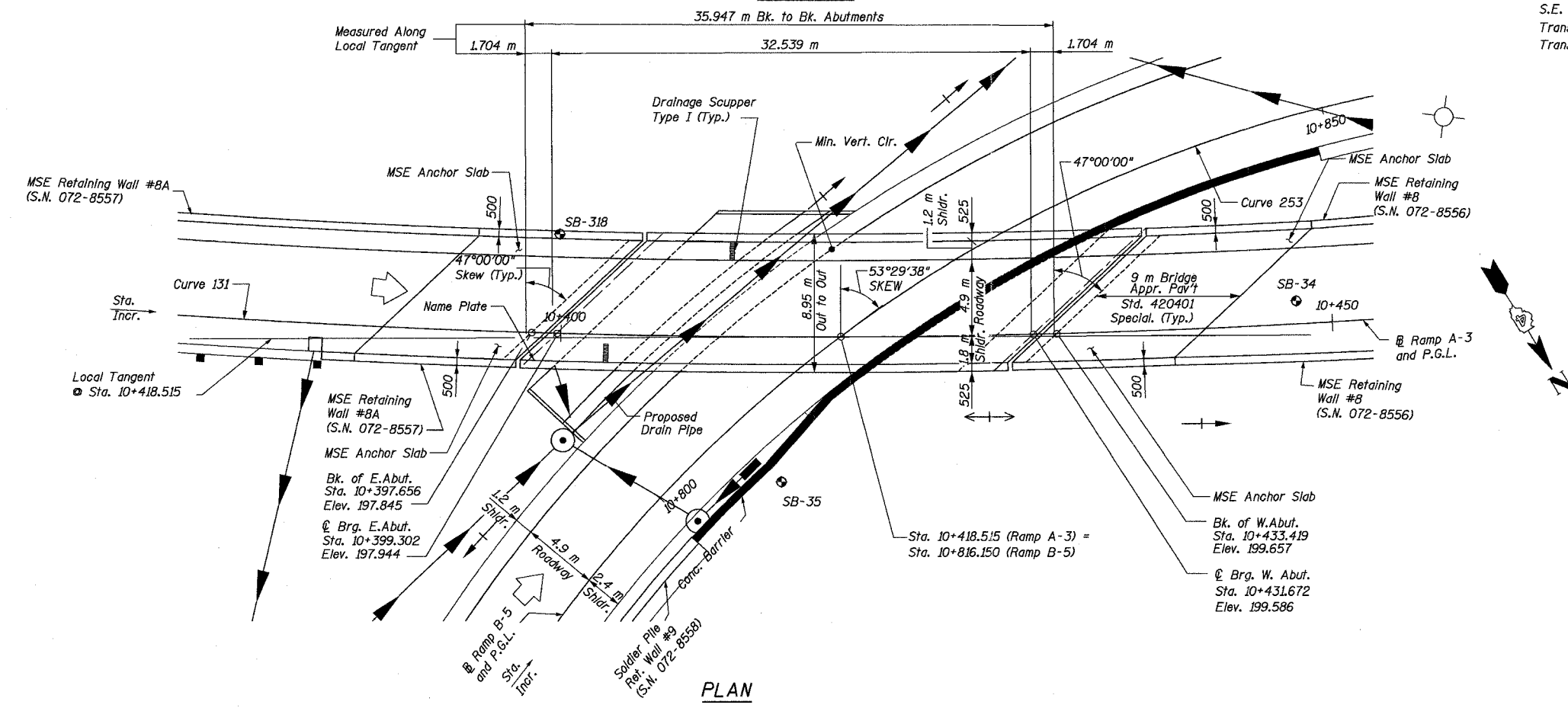
B. M. #49 : Southwest base bolt on aluminum light pole, north side of WB lanes, I-74 and 30 m west of bridge, War Memorial Drive spur over I-74, El. 190.771 m
 B. M. #3001 : Chiseled "□" on concrete sign base (I-74 - Bloomington/Galesburg) on west side of War Memorial Drive spur, El. 198.767 m
 Note: All dimensions are in millimeters (mm) except as noted.

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	420	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

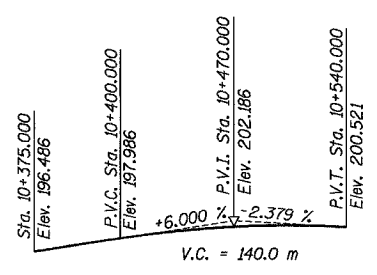
CONTRACT NO. 68200



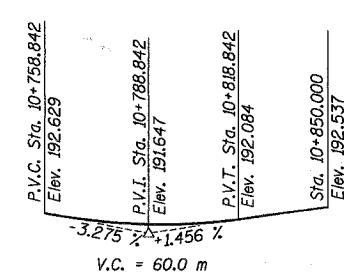
ELEVATION



PLAN



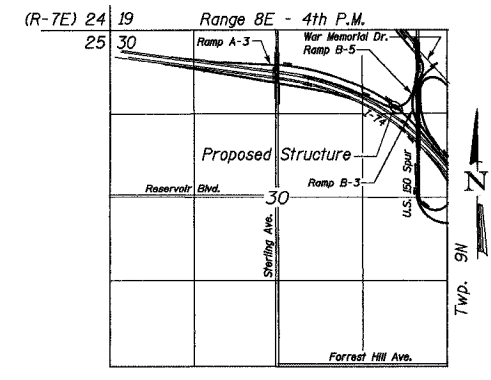
PROFILE GRADE RAMP A-3
(Along @ Ramp A-3)



PROFILE GRADE RAMP B-5
(Along @ Ramp B-5)

Borings Location
 STA. 10+418.515
 BUILT 200... BY
 STATE OF ILLINOIS
 F.A.I. ROUTE 74 SEC (72-7)R-3
 LOADING MS-18 & ALT
 STR. NO. 072-0172

NAME PLATE
See Std. 515:001



LOCATION SKETCH

CURVE DATA

RAMP A-3 - CURVE 131 $\Delta = 11^\circ 59' 18''$ (LT) $R = 605,000$ m $T = 63,526$ m $L = 126,588$ m $E = 3,326$ m PC STA = 10+377.679 PI STA = 10+441.205 PT STA = 10+504.267 $S.E. = -3.4\%$ Transition in N/A Transition out Sta. 10+499 to Sta. 10+504	RAMP B-5 - CURVE 253 $\Delta = 113^\circ 09' 52''$ (RT) $R = 88,000$ m $T = 133,370$ m $L = 173,809$ m $E = 71,786$ m PC STA = 10+681.815 PI STA = 10+815.184 PT STA = 10+855.624 $S.E. = 7.9\%$ Transition in 10+650 to 10+698 Transition out Sta. 10+840 to Sta. 10+888
---	---

DESIGN SPECIFICATIONS
1996 AASHTO with Interlms through 2002

LOADING MS18 & ALT.
Allow 2.4 kN/m² for future wearing surface.

DESIGN STRESSES

FIELD UNITS
 $f'c = 24$ MPa
 $f_y = 400$ MPa (Reinf.)
 $f_y = 345$ MPa (Struct.) (M 270M Grade 345)

SEISMIC DATA

Seismic Performance Category (SPC) = A
 Bedrock Acceleration Coefficient (A) = 0.043g
 Site Coefficient (S) = 1.0



REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION GENERAL PLAN RAMP A-3 OVER RAMP B-5 F.A.I. ROUTE 74 (SECTION 72-7)R-3 PEORIA COUNTY STA. 10+418.515 (RAMP A-3) STRUCTURE NUMBER 072-0172 PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. G-1	SCALE N.T.S.	DATE 11/16/04
		SHEET NO. 1

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	421	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200

GENERAL NOTES

Fasteners shall be high strength bolts. Bolts M20, open holes 24 mm ϕ , unless otherwise noted.

Calculated mass of Structural Steel = 63,560 kg (AASHTO M 270M Grade 345)

Field welding of construction accessories will not be permitted to the bottom flange of girders.

Anchor bolts shall be set before bolting diaphragms over supports.

The main load carrying member components subject to tensile stress shall conform to the Supplemental Requirements for Notch Toughness Zone 2. These components are the tension flanges, webs and all splice plate material except fill plates.

The structural steel bearing plates of the Elastomeric Bearing Assembly shall conform to the requirements of AASHTO M 270M Grade 345.

Reinforcement bars shall conform to the requirements of AASHTO M 31M or M 322M Grade 400.

Slope wall shall be reinforced with welded wire fabric, 152 x 152 - MW25.8 x MW25.8 with a mass of 2.91 kg/m²

The Contractor shall drive one (1) test pile in a permanent location at the west abutment as directed by the Engineer before ordering the remainder of piles.

The concrete for bridge floors finished according to Article 503.17 of the Standard Specifications, shall be placed and compacted parallel to the skew in uniform increments along centerline of bridge. The finishing machine, when required, shall be set parallel to the skew for striking off and screeding the concrete.

Bridge Seat Sealer shall be applied to the seat area of the abutments.

All dimensions are in millimeters (mm) except as noted.

The organic zinc rich primer / Epoxy / Urethane Paint System shall be used for painting of new structural steel except where otherwise noted. The color of the final finish coat for all steel surfaces shall be light gray, Munsell No 10Y 7/1. See Special Provision for "Cleaning and Painting New Metal Structures." The three coat paint system shall be applied in the shop.

The SSPC-QP1 Painting Contractor certification will not be required for this bridge.

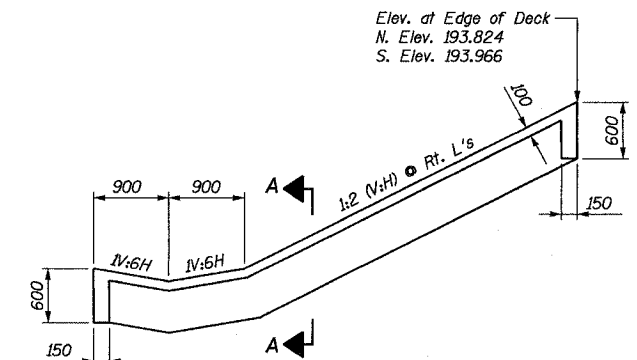
All construction joints shall be bonded.

INDEX OF DRAWINGS

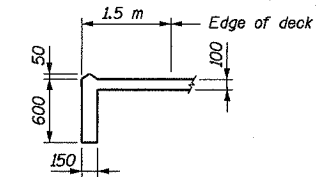
Sheet #	Drwg. No.	Description
1	G-1	General Plan
2	G-2	General Notes, Total Bill of Material, Misc. Details
3	F-1	Foundation Layout
4	SLB-1	Top of Slab Elevations I
5	SLB-2	Top of Slab Elevations II
6	DK-1	Deck Plan & Section
7	DK-2	Parapet Elevation & Details
8	S-1	Framing Plan
9	S-2	Girder Elevation and Details
10	S-3	Steel Details
11	BRG-1	Fixed & Elastomeric Expansion Bearings
12	AB-1	West Abutment
13	AB-2	West Abutment Details
14	AB-3	East Abutment
15	AB-4	East Abutment Details
16	D-1	Drainage Scupper, Type 1
17	EX-1	Continuous Seal Type Neoprene Expansion Joints
18	BS-1	Bar Splicer Assembly Details
19	PL-1	Metal Shell Pile Details
20	A-1	Anchor Bolt Details
21	RL-1	Railing Details - Single Rail
22	BL-1	Boring Logs I
23	BL-2	Boring Logs II

TOTAL BILL OF MATERIAL

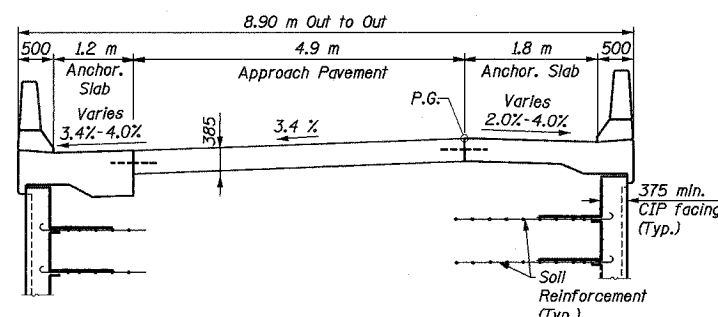
ITEM	UNIT	SUPER	SUB	TOTAL
Elastomeric Bearing Assembly, Type I	Each	6		6
Stud Shear Connectors	Each	1038		1038
Name Plates	Each	1		1
Neoprene Expansion Joint 50 mm	m	24.4		24.4
Concrete Structures	m ³		77.2	77.2
Concrete Superstructure	m ³	89.4		89.4
Bridge Deck Grooving	m ²	269		269
Protective Coat	m ²	338		338
Furnishing and Erecting Structural Steel	L.Sum			1
Reinforcement Bars, Epoxy Coated	kg	10,850	4730	15,580
Slope Wall 100 mm	m ²		111	111
Furnishing Metal Pile Shells 356mm	m		437	437
Driving And Filling Shells	m		437	437
Test Pile Metal Shells	Each		1	1
Bridge Seat Sealer	m ²		41	41
Drainage Scuppers, Type I	Each	2		2
Bar Splicers	Each		78	78
Aluminum Railing, Type H (Special)	m	68.0		68.0



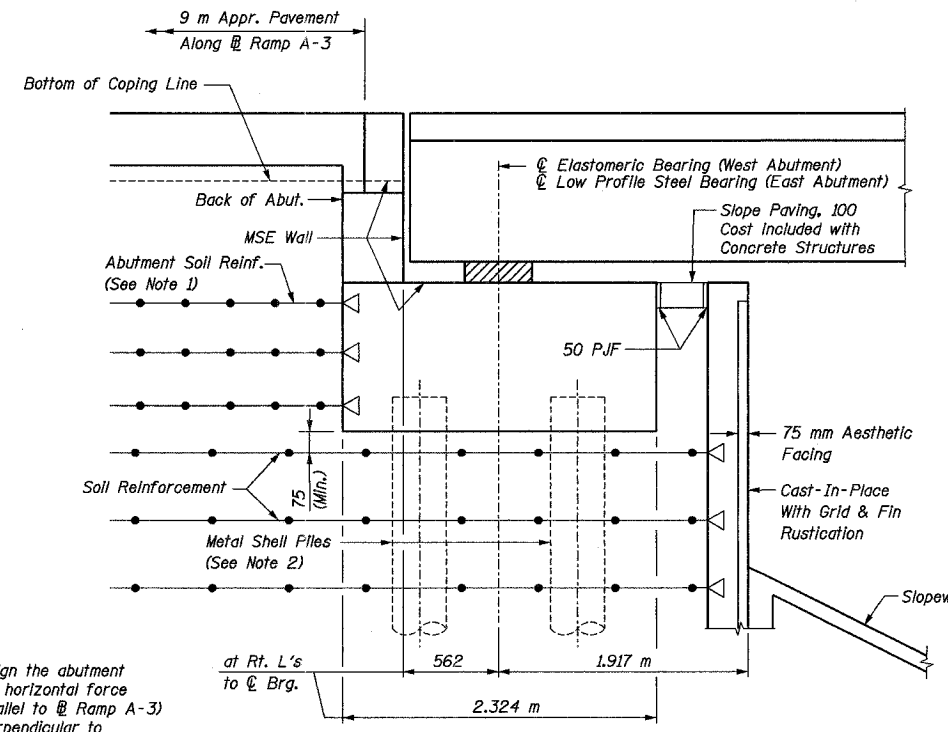
SECTION THRU SLOPEWALL



SECTION A-A



SECTION THRU APPROACH SLAB
Looking Upstation



SECTION THRU ABUTMENT
(Typ. Both Ends)

- Notes on Abutments:
- The MSE wall supplier to design the abutment soil reinforcement to resist a horizontal force of 30 kN/m of abutment (parallel to ϕ Ramp A-3) and 21 kN/m of abutment (perpendicular to ϕ Ramp A-3)
 - Precore piles to 3.0 m below base of MSE material. Piles must be driven prior to placement of MSE. Coat piles above existing ground with 6 mm min. thickness of either asphaltic cement or S.C. or M.C. liquid asphalt.

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
GENERAL NOTES. TOTAL BILL OF MATERIAL, MISC. DETAILS		
RAMP A-3 OVER RAMP B-5 F.A.I. ROUTE 74 (SECTION 72-7)R-3 PEORIA COUNTY STA. 10+418.515 (RAMP A-3) STRUCTURE NUMBER 072-0172		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. G-2	SCALE N.T.S.	DATE 11/16/04
		SHEET NO. 2

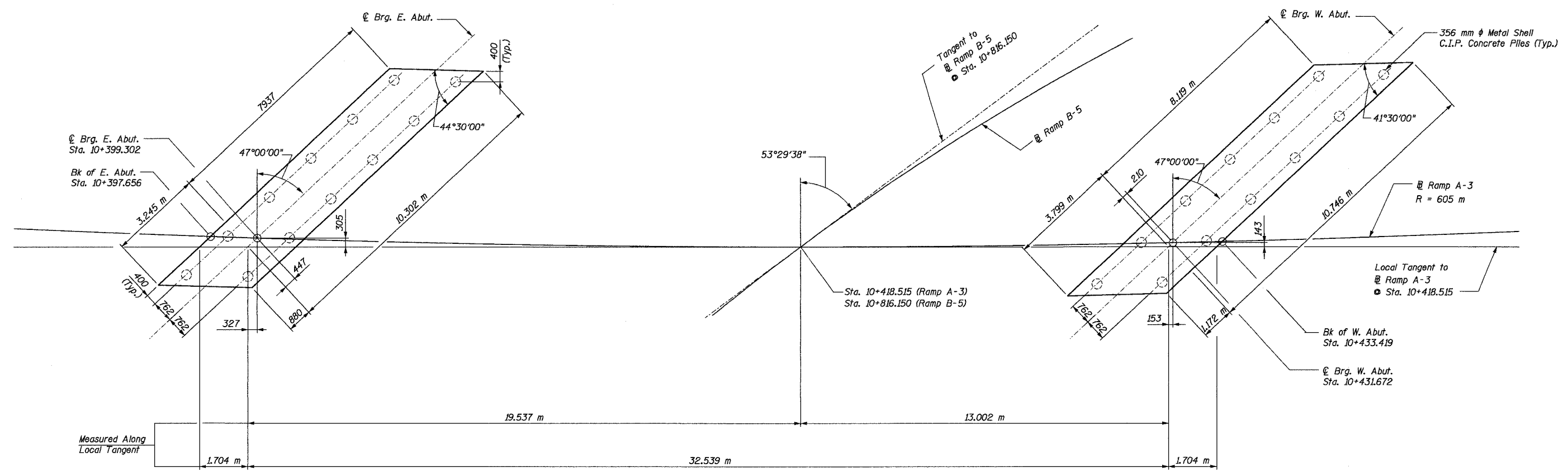
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Designed by: WEE
Checked by: AK
Drafted by: RKS
Checked by: WEE

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	422	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200

Date: 11/19/2004 Time: 09:00:09 AM
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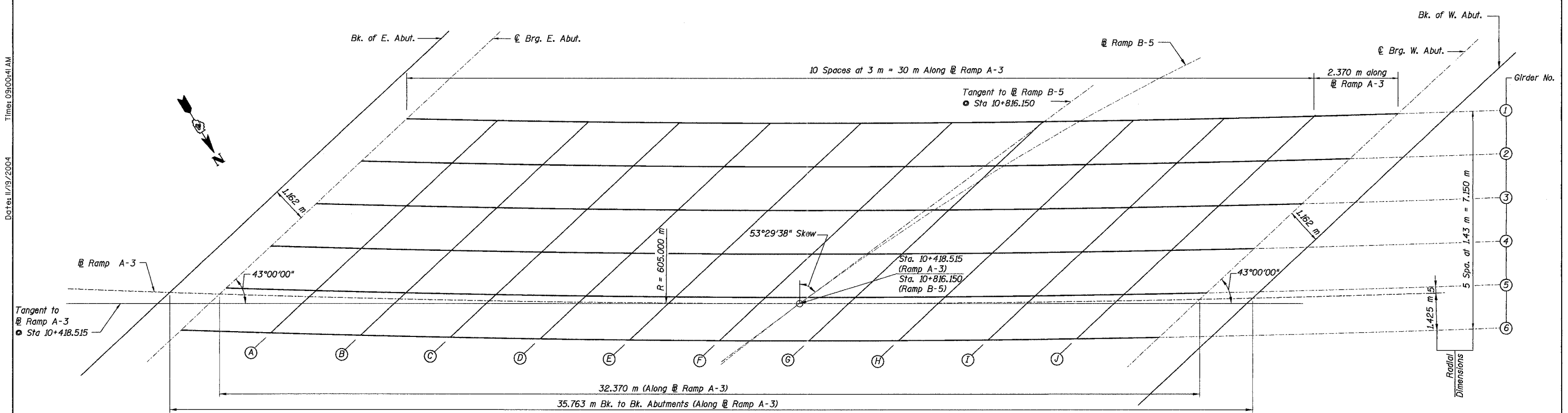
FOUNDATION LAYOUT

Note:
 Footing dimensioned to local tangent along centerline of bridge.
 All dimensions are in millimeters (mm) unless noted otherwise

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
FOUNDATION LAYOUT		
RAMP A-3 OVER RAMP B-5 F.A.I. ROUTE 74 (SECTION 72-7)R-3 PEORIA COUNTY STA. 10+418.515 (RAMP A-3) STRUCTURE NUMBER 072-0172		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. F-1	SCALE N.T.S.	DATE 11/16/04
		SHEET NO. 3

Designed by:	AK
Checked by:	WEE
Drafted by:	RKS
Checked by:	WEE

Date: 11/19/2004 Time: 09:00:41 AM
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PLAN

GIRDER 1

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. E. Abut.	10403.469	-5.725	197.996	197.996
Brg. E. Abut.	10405.147	-5.725	198.092	198.092
A	10408.176	-5.725	198.262	198.300
B	10411.204	-5.725	198.426	198.498
C	10414.233	-5.725	198.585	198.684
D	10417.262	-5.725	198.738	198.857
E	10420.290	-5.725	198.886	199.015
F	10423.319	-5.725	199.028	199.157
G	10426.348	-5.725	199.164	199.283
H	10429.376	-5.725	199.296	199.393
I	10432.405	-5.725	199.421	199.490
J	10435.434	-5.725	199.542	199.576
Brg. W. Abut.	10438.186	-5.725	199.646	199.646
Bk. W. Abut.	10439.972	-5.725	199.712	199.712

GIRDER 2

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. E. Abut.	10402.001	-4.295	197.959	197.959
Brg. E. Abut.	10403.671	-4.295	198.056	198.056
A	10406.693	-4.295	198.228	198.262
B	10409.714	-4.295	198.395	198.459
C	10412.736	-4.295	198.556	198.646
D	10415.757	-4.295	198.711	198.819
E	10418.778	-4.295	198.861	198.973
F	10421.800	-4.295	199.006	199.122
G	10424.821	-4.295	199.145	199.251
H	10427.843	-4.295	199.279	199.366
I	10430.864	-4.295	199.407	199.463
J	10433.886	-4.295	199.529	199.560
Brg. W. Abut.	10436.540	-4.295	199.633	199.633
Bk. W. Abut.	10438.316	-4.295	199.700	199.700

GIRDER 3

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. E. Abut.	10400.544	-2.865	197.921	197.921
Brg. E. Abut.	10402.206	-2.865	198.019	198.019
A	10405.220	-2.865	198.194	198.228
B	10408.235	-2.865	198.362	198.427
C	10411.249	-2.865	198.526	198.616
D	10414.263	-2.865	198.684	198.791
E	10417.277	-2.865	198.836	198.952
F	10420.292	-2.865	198.983	199.099
G	10423.306	-2.865	199.124	199.230
H	10426.320	-2.865	199.260	199.348
I	10429.335	-2.865	199.391	199.452
J	10432.349	-2.865	199.516	199.546
Brg. W. Abut.	10434.906	-2.865	199.618	199.618
Bk. W. Abut.	10436.673	-2.865	199.687	199.687

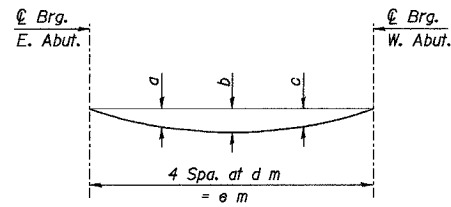
Notes:
 Work this Dwg. with Dwg. No. SLB-2.
 All elevations and offsets are in meters.
 All elevations are at top of slab.

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION TOP OF SLAB ELEVATIONS I RAMP A-3 OVER RAMP B-5 F.A.I. ROUTE 74 (SECTION 72-7)R-3 PEORIA COUNTY STA. 10+418.515 (RAMP A-3) STRUCTURE NUMBER 072-0172 PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. SLB-1	SCALE N.T.S.	DATE 11/16/04
		SHEET NO. 4

Designed by: AK
 Checked by: WEE
 Drafted by: JV
 Checked by: EHF

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	424	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200

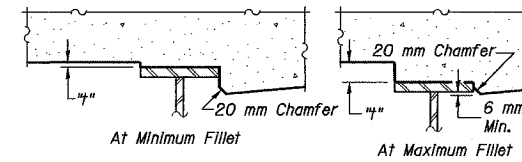


DEAD LOAD DEFLECTION DIAGRAM
(Includes weight of concrete only)

Note: The above deflections are not to be used in the field if the Engineer is working from the theoretical grade elevations adjusted for dead load deflections as shown below and on Dwg. No. SLB-1.

TABLE OF a THRU e DIMENSIONS

Girder No.	a	b	c	d (m)	e (m)
1	93	131	93	8.182	32.726
2	84	118	84	8.159	32.635
3	84	118	84	8.136	32.545
4	81	114	81	8.114	32.457
5	81	114	81	8.093	32.370
6	88	124	88	8.071	32.284



To determine "4": After all structural steel has been erected, elevations of the top flanges of the girders shall be taken at intervals shown on Dwg. No. SLB-1. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown below and on Dwg. No. SLB-1, minus slab thickness, equals the fillet heights "4" above top flange of girders.

FILLET HEIGHTS

GIRDER 4

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. E. Abut.	10399.097	-1.435	197.883	197.883
☉ Brg. E. Abut.	10400.752	-1.435	197.982	197.982
A	10403.759	-1.435	198.159	198.192
B	10406.766	-1.435	198.329	198.393
C	10409.773	-1.435	198.495	198.582
D	10412.780	-1.435	198.655	198.760
E	10415.787	-1.435	198.810	198.922
F	10418.794	-1.435	198.959	199.071
G	10421.801	-1.435	199.103	199.205
H	10424.809	-1.435	199.242	199.325
I	10427.816	-1.435	199.375	199.432
J	10430.823	-1.435	199.502	199.530
☉ Brg. W. Abut.	10433.286	-1.435	199.603	199.603
Bk. W. Abut.	10435.043	-1.435	199.672	199.672

GIRDER 5

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. E. Abut.	10397.661	-0.005	197.846	197.846
☉ Brg. E. Abut.	10399.307	-0.005	197.944	197.944
A	10402.307	-0.005	198.123	198.156
B	10405.307	-0.005	198.296	198.359
C	10408.307	-0.005	198.464	198.551
D	10411.307	-0.005	198.626	198.730
E	10414.307	-0.005	198.783	198.896
F	10417.307	-0.005	198.935	199.046
G	10420.307	-0.005	199.081	199.183
H	10423.307	-0.005	199.222	199.305
I	10426.308	-0.005	199.357	199.414
J	10429.308	-0.005	199.487	199.514
☉ Brg. W. Abut.	10431.677	-0.005	199.586	199.586
Bk. W. Abut.	10433.425	-0.005	199.657	199.657

RAMP A-3

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. E. Abut.	10397.656	0.000	197.845	197.845
☉ Brg. E. Abut.	10399.302	0.000	197.944	197.944
A	10402.302	0.000	198.123	198.156
B	10405.302	0.000	198.296	198.359
C	10408.302	0.000	198.464	198.551
D	10411.302	0.000	198.626	198.730
E	10414.302	0.000	198.783	198.895
F	10417.302	0.000	198.935	199.046
G	10420.302	0.000	199.081	199.183
H	10423.302	0.000	199.222	199.305
I	10426.302	0.000	199.357	199.414
J	10429.302	0.000	199.487	199.514
☉ Brg. W. Abut.	10431.672	0.000	199.586	199.586
Bk. W. Abut.	10433.419	0.000	199.657	199.657

GIRDER 6

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. E. Abut.	10396.235	1.425	197.732	197.732
☉ Brg. E. Abut.	10397.873	1.425	197.830	197.830
A	10400.866	1.425	198.009	198.046
B	10403.859	1.425	198.185	198.253
C	10406.852	1.425	198.355	198.450
D	10409.845	1.425	198.519	198.633
E	10412.838	1.425	198.678	198.801
F	10415.831	1.425	198.832	198.954
G	10418.824	1.425	198.981	199.091
H	10421.817	1.425	199.124	199.214
I	10424.810	1.425	199.262	199.323
J	10427.803	1.425	199.394	199.422
☉ Brg. W. Abut.	10430.081	1.425	199.492	199.492
Bk. W. Abut.	10431.820	1.425	199.564	199.564

Designed by: AK
Checked by: WEE
Drafted by: JV
Checked by: EHF

Notes:
Work this drawing with Dwg. No. SLB-1.
All elevations and offsets are in meters.
All elevations are at top of slab.
All dimensions are in millimeters (mm) unless noted otherwise.

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
TOP OF SLAB ELEVATIONS II		
RAMP A-3 OVER RAMP B-5 F.A.I. ROUTE 74 (SECTION 72-7)R-3 PEORIA COUNTY STA. 10+418.515 (RAMP A-3) STRUCTURE NUMBER 072-0172		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. SLB-2	SCALE N.T.S.	DATE 11/16/04
		SHEET NO. 5

Time: 09:00:59 AM

Date: 11/19/2004

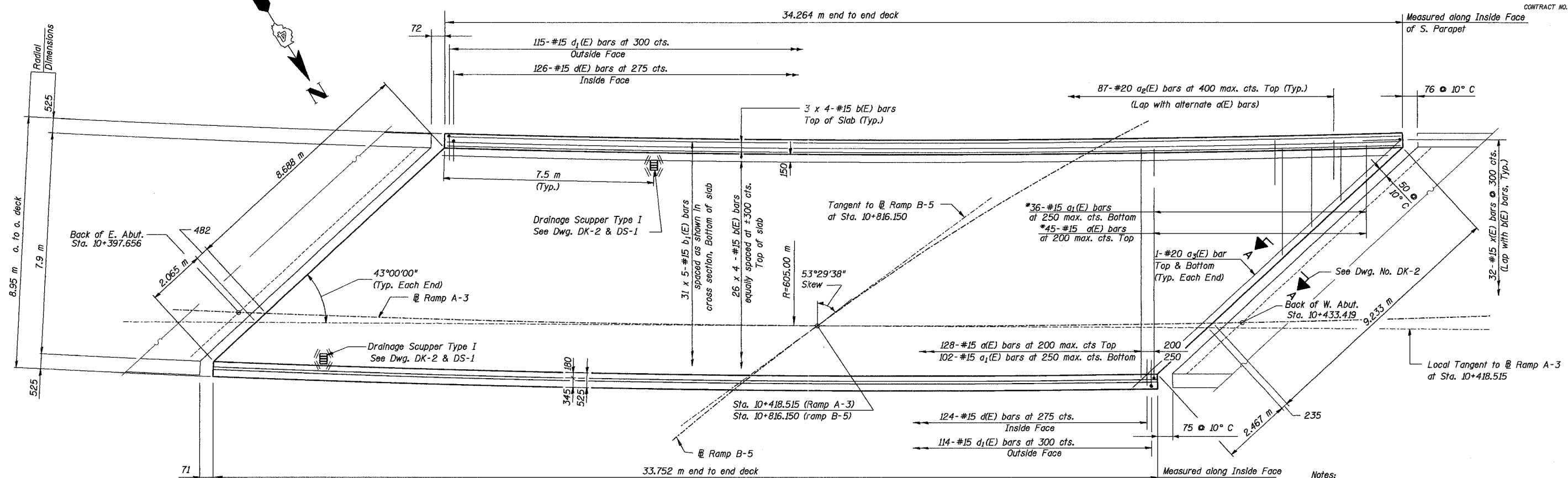
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ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	495	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200

Date: 11/19/2004 Time: 09:01 AM

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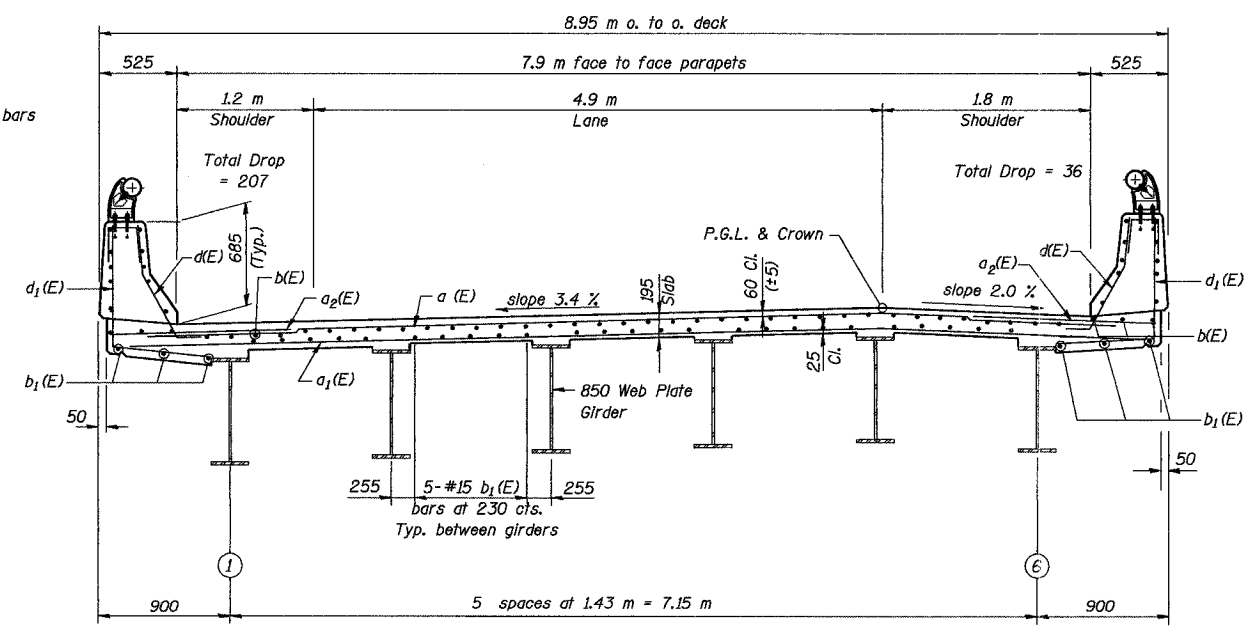
PLAN

Notes:

- Work this drawing with Dwg. No. DK-2.
- For Superstructure Details, Bill of Materials and Parapet Reinforcement see Dwg. No. DK-2.
- Reinforcement bars designated (E) shall be epoxy coated.
- Bars indicated thus 26 x 4-#15 etc. indicates 26 lines of bars with 4 lengths per line.
- All dimensions are in millimeters (mm) except as noted.
- Cut longitudinal bars in slab to clear drainage scuppers.
- Transverse bars shall be placed radially.
- Longitudinal bars shall be placed circumferentially.

* Order a(E) & a1(E) bars full length. Cut to fit skew and use remainder of bars in opposite end.

MIN BAR LAP
#15 bar = 640



CROSS SECTION

Looking Upstation
All dimensions are radial except as noted

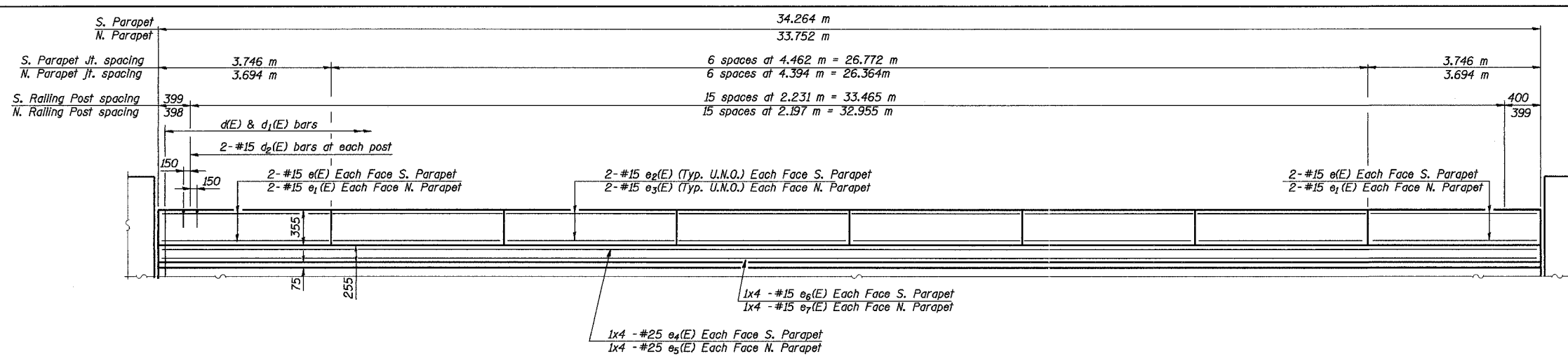
REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
DECK PLAN & SECTION		
RAMP A-3 OVER RAMP B-5 F.A.I. ROUTE 74 (SECTION 72-7)R-3 PEORIA COUNTY STA. 10+418.515 (RAMP A-3) STRUCTURE NUMBER 072-0172		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. DK-1	SCALE N.T.S.	DATE 11/16/04
		SHEET NO. 6

Designed by: AK
Checked by: WEE
Drafted by: RKS
Checked by: AK

Date: 11/19/2004
 Time: 09:03:36 AM
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ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	426	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

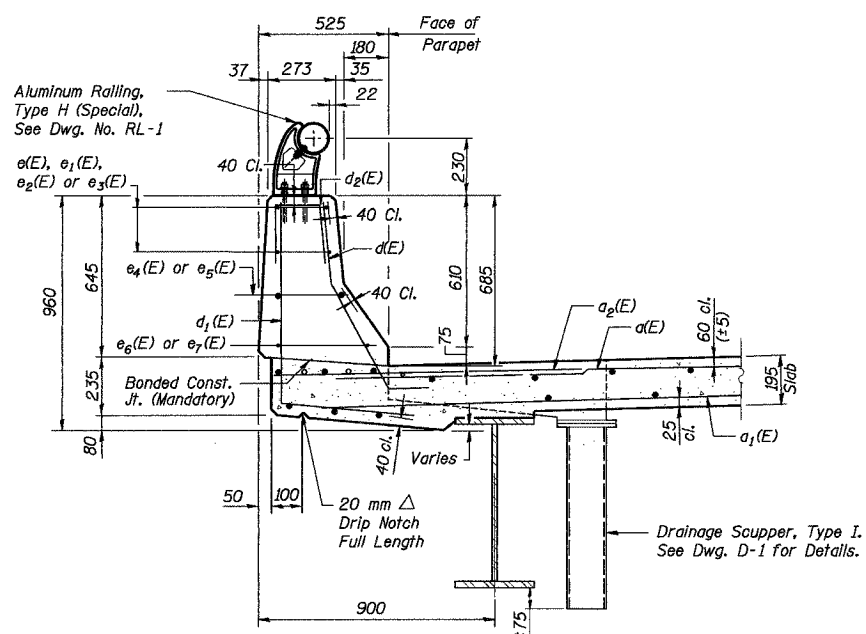
CONTRACT NO. 68200



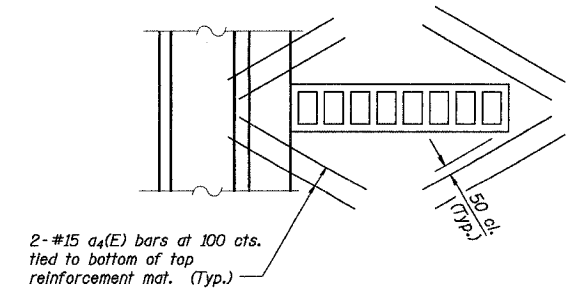
INSIDE ELEVATION OF PARAPET
(Dimensions are along inside face of parapet)

SUPERSTRUCTURE BILL OF MATERIAL

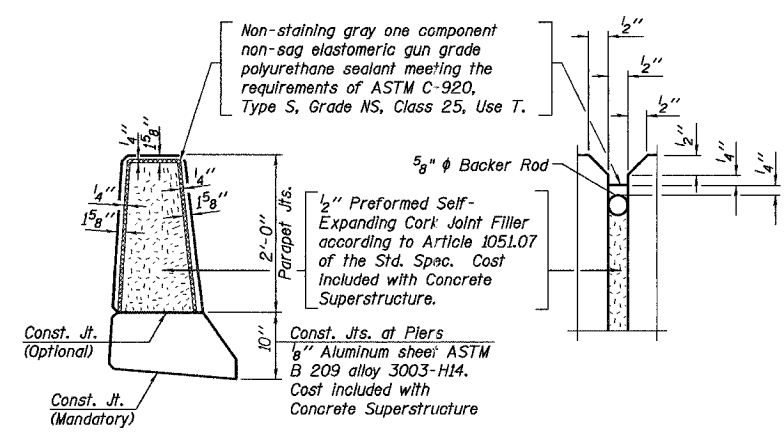
Bar	No.	Size	Length (m)	Shape
$a(E)$	173	15	8.75	
$a_1(E)$	138	15	8.50	
$a_2(E)$	174	20	1.20	
$a_3(E)$	4	20	13.47	
$a_4(E)$	16	15	0.60	
$b(E)$	128	15	9.05	
$b_1(E)$	155	15	7.36	
$d(E)$	250	15	0.97	
$d_1(E)$	229	15	1.59	
$d_2(E)$	64	15	0.64	
$e(E)$	8	15	3.66	
$e_1(E)$	8	15	3.60	
$e_2(E)$	24	15	4.38	
$e_3(E)$	24	15	4.30	
$e_4(E)$	8	25	9.54	
$e_5(E)$	8	25	9.41	
$e_6(E)$	8	15	9.03	
$e_7(E)$	8	15	8.89	
$x(E)$	64	15	1.28	
Concrete Superstructure			m ³	89.4
Reinforcement Bars, Epoxy Coated			kg	10850



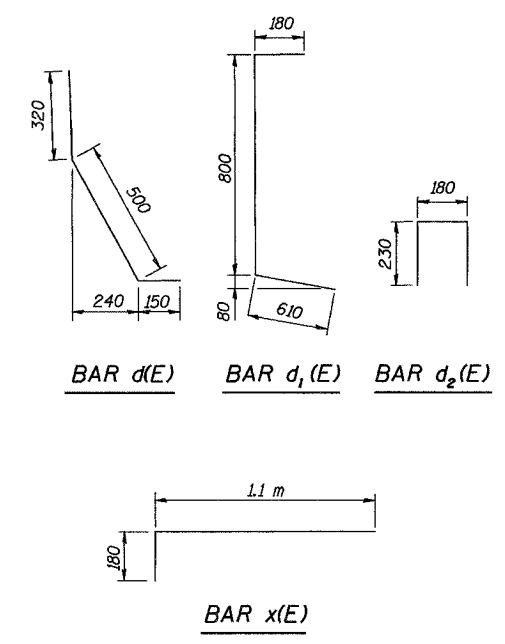
SECTION THRU PARAPET
Note: All edges shall have 20 mm chamfer.



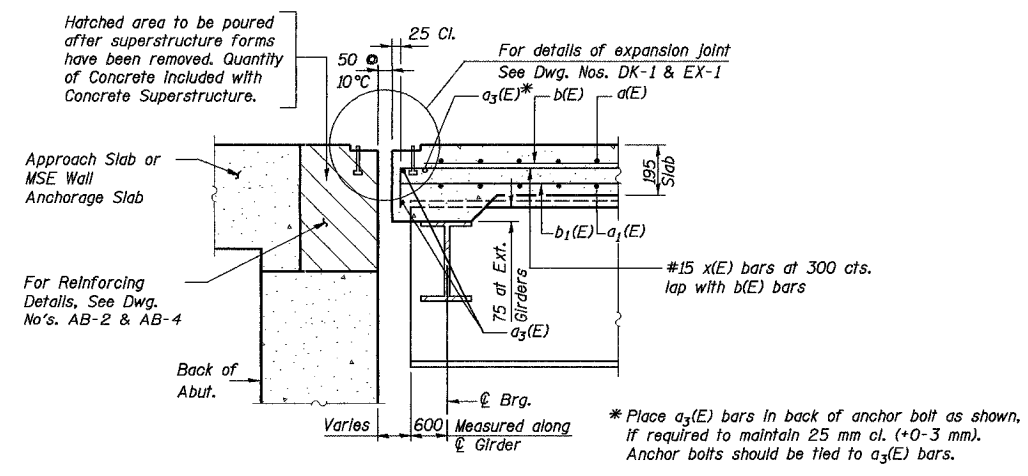
PLAN AT DRAINAGE SCUPPER



PARAPET JOINT DETAILS



MIN. BAR LAP
#15 Bar = 640 mm
#25 Bar = 1.32 m

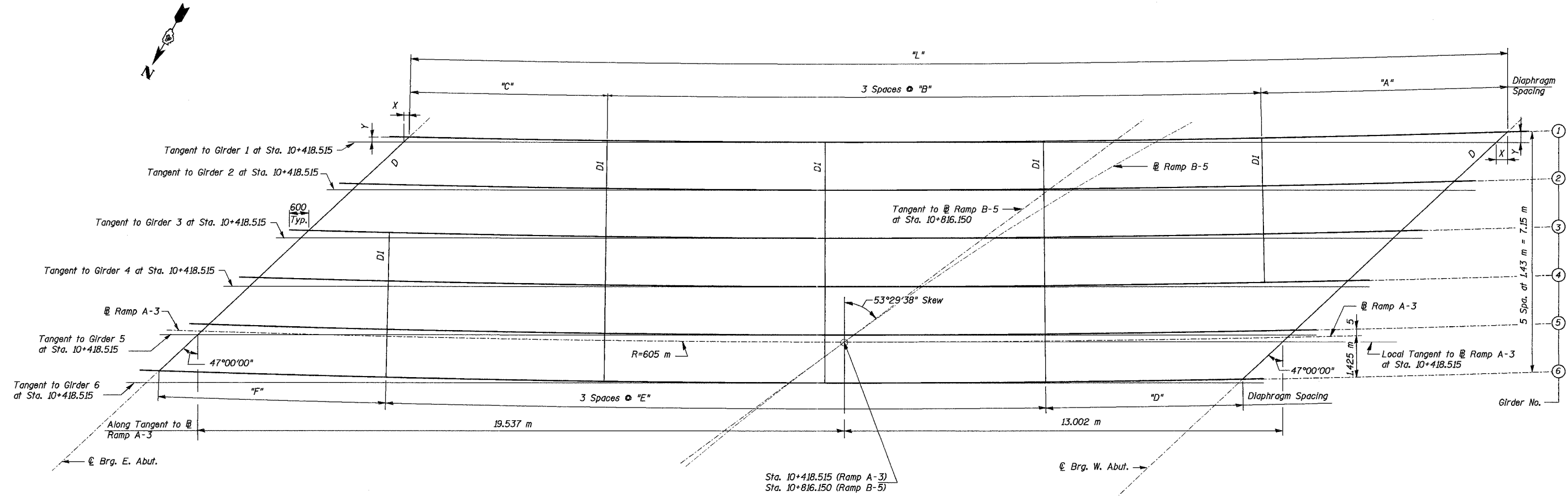


SECTION A-A
Section at West Abutment Shown
Section at East Abutment Similar

Notes:
 Work this drawing with Dwg. No. DK-1.
 Bars indicated thus 1x3-#15 etc. indicates 1 line of bars with 3 lengths per line.
 Reinforcement bars designated (E) shall be epoxy coated.
 All dimensions are in millimeters (mm) except as noted.

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION PARAPET ELEVATION & DETAILS RAMP A-3 OVER RAMP B-5 F.A.I. ROUTE 74 (SECTION 72-7)R-3 PEORIA COUNTY STA. 10+418.515 (RAMP A-3) STRUCTURE NUMBER 072-0172 PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. DK-2	SCALE N.T.S.	DATE 11/16/04
		SHEET NO. 7

Designed by: EHF
 Checked by: AK
 Drafted by: RKS
 Checked by: AK



FRAMING PLAN

Notes:
 Diaphragms D1 shall be placed radially.
 All "Y" dimensions are given perpendicular to the tangent to girder at Sta. 10+418.515.
 All "X" dimensions are given parallel to the tangent to girder at Sta. 10+418.515.
 Work this Dwg. with Dwg. Nos. S-2, and S-3.
 All dimensions are in millimeters (mm) unless noted otherwise.

TABLE OF A THRU F & L DIMENSIONS
(In meters)

Girder No.	Radius	A	B	C	D	E	F	L
1	599.275	7.326	6.500	5.900	-	-	-	32.726
2	600.705	5.709	6.516	7.378	-	-	-	32.635
3	602.135	4.097	6.531	-	-	6.531	2.324	32.545
4	603.565	2.490	6.547	-	-	6.547	3.779	32.457
5	604.995	-	-	-	7.450	6.562	5.234	32.370
6	606.425	-	-	-	5.868	6.578	6.682	32.284

TABLE OF OFFSET DIMENSIONS X AND Y

Girder No.	☉ Brg. W Abut.		☉ Brg. E Abut.	
	X	Y	X	Y
1	340	317	157	146
2	286	267	194	181
3	237	221	235	219
4	193	180	279	260
5	154	143	327	305
6	119	111	379	353

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
FRAMING PLAN		
RAMP A-3 OVER RAMP B-5 F.A.I. ROUTE 74 (SECTION 72-7)R-3 PEORIA COUNTY STA. 10+418.515 (RAMP A-3) STRUCTURE NUMBER 072-0172		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. S-1	SCALE N.T.S.	DATE 11/16/04
		SHEET NO. 8

Time: 09:11:00 AM
 Date: 11/19/2004
 File name: P:\643996\structure\A3\072-0172\Sheet\Tracings\ST0001-1A0720172.dgn

Designed by: AK
 Checked by: WEE
 Drafted by: RKS
 Checked by: EHF

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	428	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200

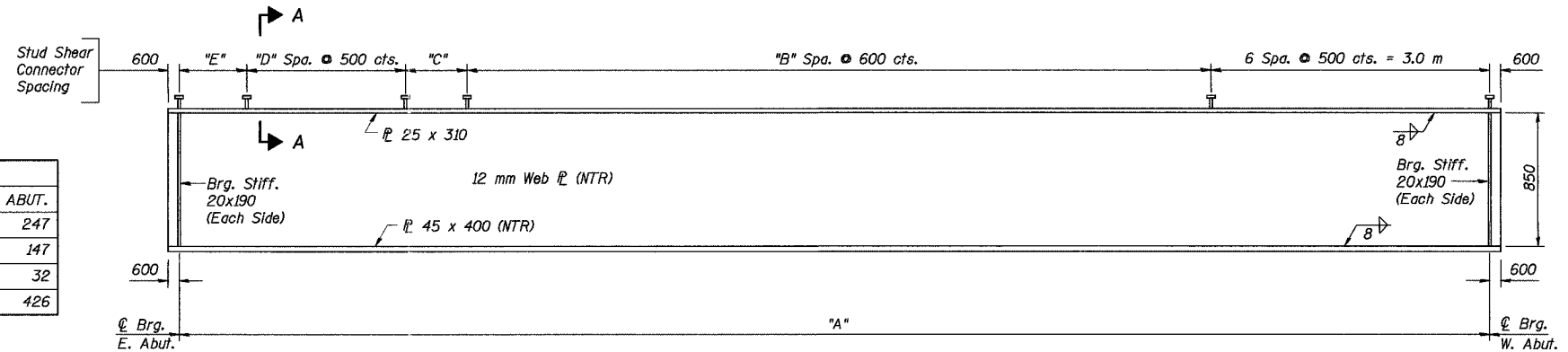
TOP OF WEB ELEVATIONS

(For Fabrication Only)

	Girder 1	Girder 2	Girder 3	Girder 4	Girder 5	Girder 6
℄ Brg. East Abutment	197.837	197.801	197.765	197.727	197.689	197.575
℄ Brg. West Abutment	199.391	199.368	199.348	199.348	199.328	199.237

INTERIOR GIRDER MOMENT TABLE	
	0.5 Span
I_s	(10^6 mm ⁴) 5100
I_c (n)	(10^6 mm ⁴) 13328
I_c (3n)	(10^6 mm ⁴) 9165
S_s	(10^3 mm ³) 14990
S_c (n)	(10^3 mm ³) 19785
S_c (3n)	(10^3 mm ³) 18124
S_{bl}	(10^3 mm ³) 1200
\bar{D}	(kN/m) 9.90
$M\bar{D}$	(kN-m) 1326
$S\bar{D}$	(kN/m) 5.20
$M_s\bar{D}$	(kN-m) 696
$M\bar{L}$	(kN-m) 956
M (Imp)	(kN-m) 206
$^5_3 [M\bar{L} + M(\text{Imp})]$	(kN-m) 1936
M_a	(kN-m) 5145
M_{bl}	(kN-m) 40
$f_s\bar{D}$ (Non-comp.)	(MPa) 88
$f_s\bar{D}$ (comp.)	(MPa) 38
f_s $^5_3 [M\bar{L} + M(\text{Imp})]$	(MPa) 98
f_w	(MPa) 33
$f_s + f_w$ (Overload)	(MPa) 250
f_s (Total)	(MPa) 292
f_s (Total) + f_w	(MPa) -
VR	(kN) 178
Fb	(MPa) 345

INTERIOR GIRDER REACTION TABLE			
		E. ABUT.	W. ABUT.
R DL	(kN)	247	247
R LL	(kN)	147	147
R IMP	(kN)	32	32
R (TOTAL)	(kN)	426	426

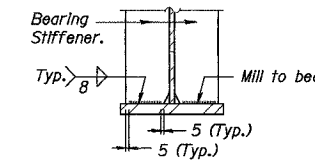


GIRDER ELEVATION

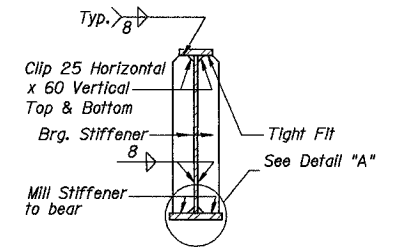
"NTR" denotes plates to which notch toughness requirements are applicable.

TABLE OF A THRU E DIMENSIONS

Girder No.	A	B	C	D	E
1	32.726 m	44	-	6	326
2	32.635 m	44	-	6	235
3	32.545 m	44	-	6	145
4	32.457 m	43	-	7	157
5	32.370 m	43	570	6	-
6	32.284 m	43	-	6	484



DETAIL "A"



SECTION AT ABUTMENT

Fb- Maximum allowable stress Fbu or Fby computed according to AASHTO [Guide Specifications for Horizontally Curved Highway Bridges Section 2.12(B) and 2.16].

I_s and S_s are the moment of inertia and section modulus of the steel section used in computing f_s (Total and Overload)

$I_c(n)$ and $S_c(n)$ are the moment of inertia and section modulus of the composite section used in computing stresses due to live load.

$I_c(3n)$ and $S_c(3n)$ are the moment of inertia and section modulus of the composite section used in computing stresses due to superimposed dead load. (See AASHTO 10.38).

VR is the maximum \bar{L} + Impact shear range in span.

M_a (Applied Moment) = $1.3[M\bar{D} + M_s\bar{D} + ^5_3(M\bar{L} + M(\text{Imp}))]$.

$f_s + f_w$ (Overload) is the sum of the stress due to $M\bar{D} + M_s\bar{D} + ^5_3(M\bar{L} + M(\text{Imp})) + M_{bl}/1.3$.

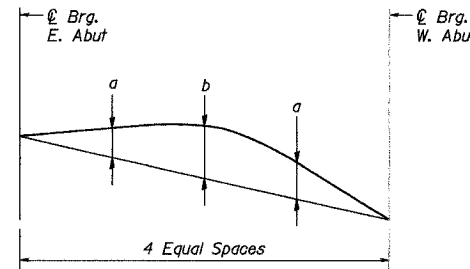
f_s (Total) is the sum of the stress due to $1.3[M\bar{D} + M_s\bar{D} + ^5_3(M\bar{L} + M(\text{Imp}))]$.

S_{bl} is the section modulus for one flange plate for lateral flange bending.

M_{bl} is the lateral bending moment for flange plate (factored).

f_w is the calculated normal stress at the edge of flange due to lateral bending (factored).

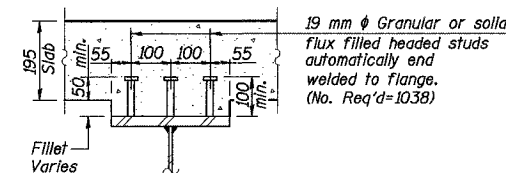
$M\bar{L}$ and $R\bar{L}$ include the effects of centrifugal force and superelevation.



CAMBER DIAGRAM

TABLE OF CAMBER

Girder No.	a	b
1	170	238
2 - 3	158	222
4 - 5	153	215
6	159	223



SECTION A-A

Notes:

Work this Drawing with S-1.

Beams shall be fabricated to their respective radii.

All structural steel for flanges and webs of main girders and bearing stiffeners shall be AASHTO M 270M Grade 345.

For additional steel details see Dwg. S-3.

All dimensions are in millimeters (mm) except as noted.

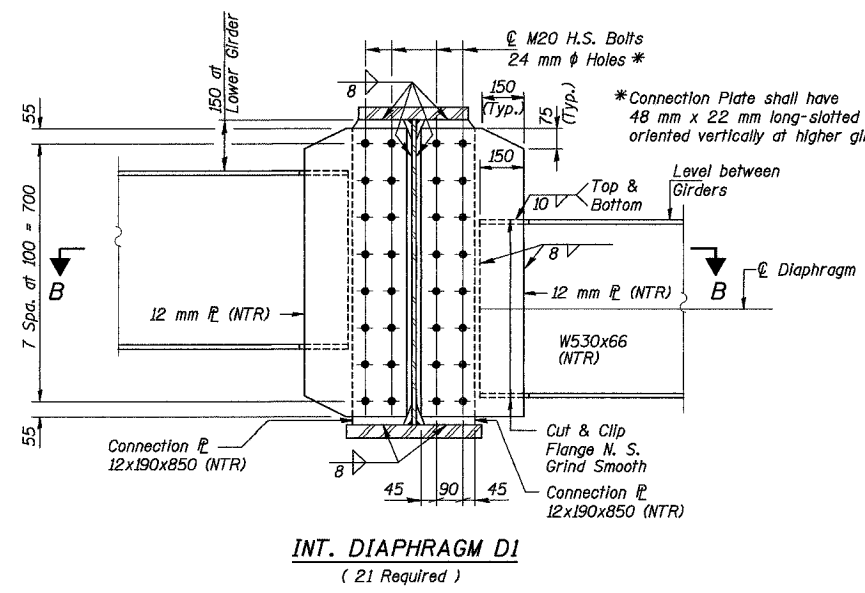
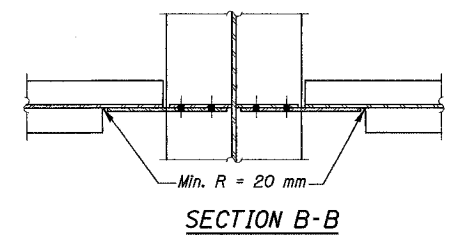
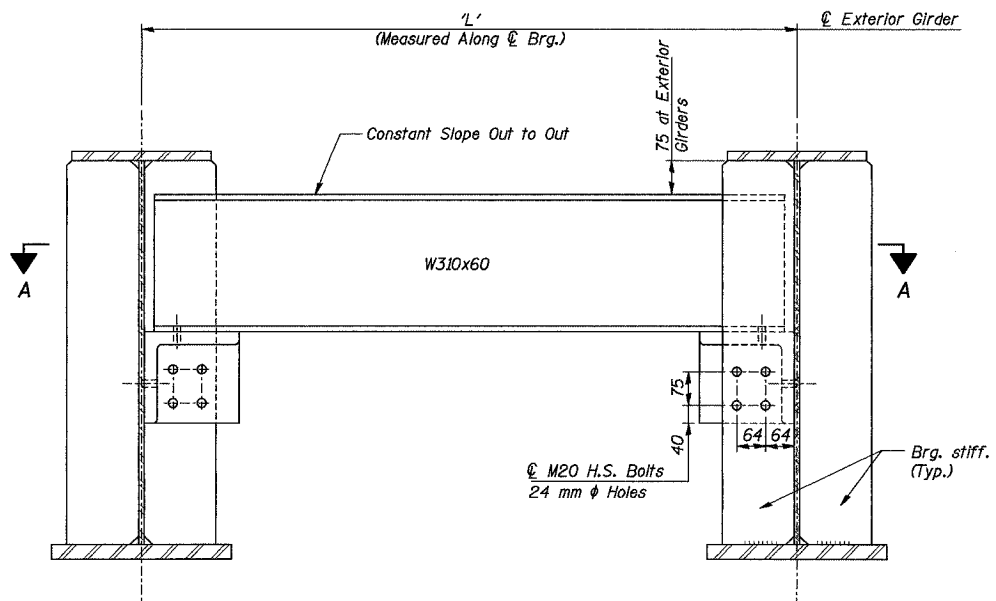
REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
GIRDER ELEVATION & DETAILS		
RAMP A-3 OVER RAMP B-5 F.A.I. ROUTE 74 (SECTION 72-7)R-3 PEORIA COUNTY STA. 10+418.515 (RAMP A-3) STRUCTURE NUMBER 072-0172		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. S-2	SCALE N.T.S.	DATE 11/16/04
		SHEET NO. 9

Time: 09:11:24 AM

Date: 11/19/2004

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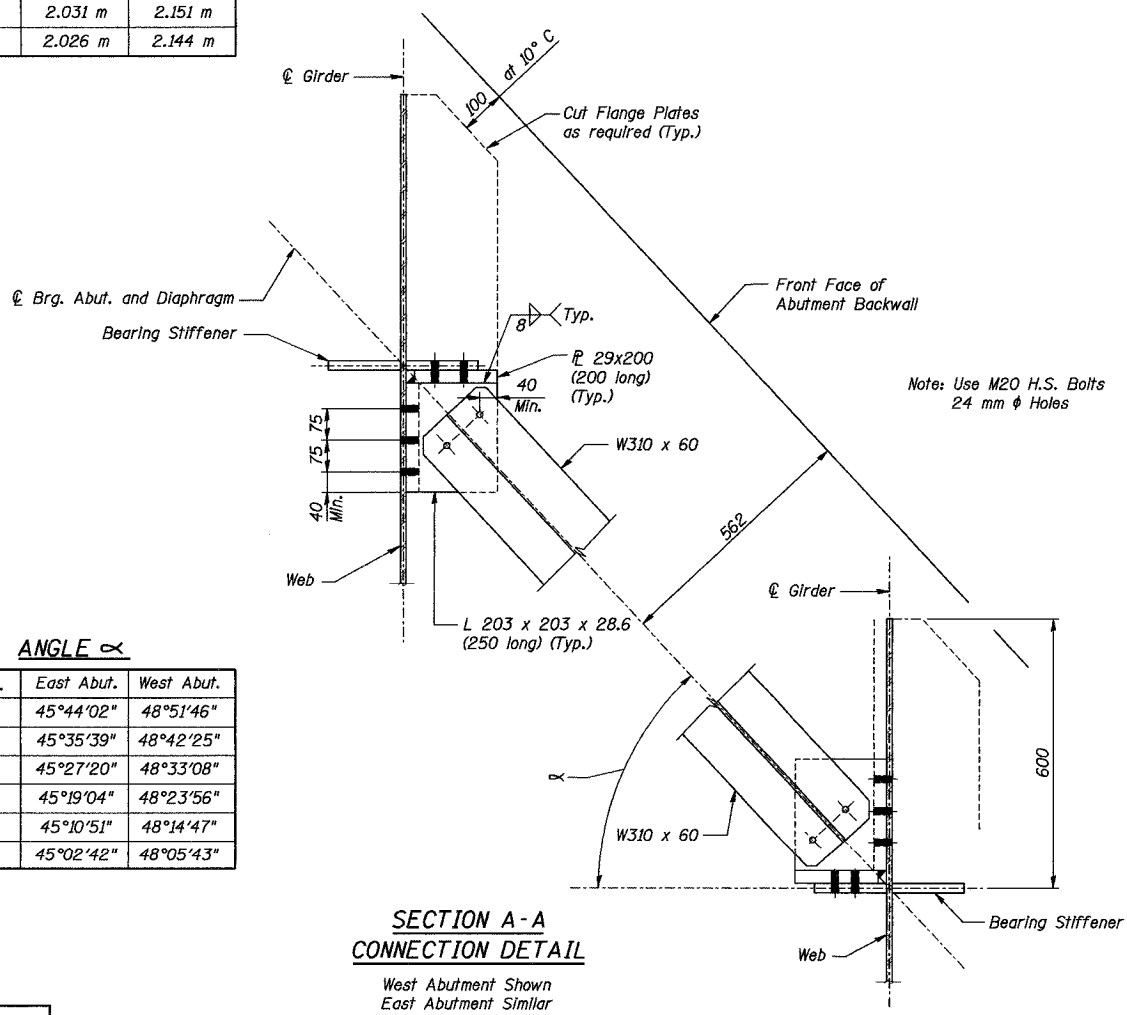
Designed by: AK
Checked by: WEE
Drafted by: JAG
Checked by: WEE



LENGTH 'L'

Girder No.	East Abut.	West Abut.
1 - 2	2.046 m	2.171 m
2 - 3	2.041 m	2.164 m
3 - 4	2.036 m	2.157 m
4 - 5	2.031 m	2.151 m
5 - 6	2.026 m	2.144 m

END DIAPHRAGM D
(10 Required)



ANGLE α

Girder No.	East Abut.	West Abut.
1	45°44'02"	48°51'46"
2	45°35'39"	48°42'25"
3	45°27'20"	48°33'08"
4	45°19'04"	48°23'56"
5	45°10'51"	48°14'47"
6	45°02'42"	48°05'43"

SECTION A-A CONNECTION DETAIL
West Abutment Shown
East Abutment Similar

Notes:

All diaphragms, plates and angles shall be AASHTO M270M Grade 345.

"NTR" denotes plates to which notch toughness requirements are applicable.

Two hardened washers shall be required over all oversized holes.

All dimensions are in millimeters (mm) except as noted.

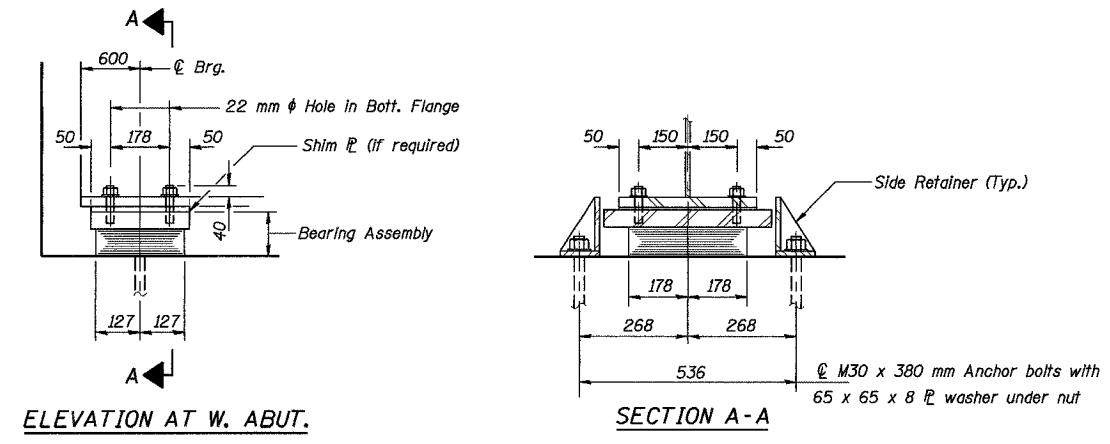
REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
STEEL DETAILS		
RAMP A-3 OVER RAMP B-5 F.A.I. ROUTE 74 (SECTION 72-7)R-3 PEORIA COUNTY STA. 10+418.515 (RAMP A-3) STRUCTURE NUMBER 072-0172		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. S-3	SCALE N.T.S.	DATE 11/16/04
		SHEET NO. 10

Date: 11/19/2004 Time: 09:11:44 AM
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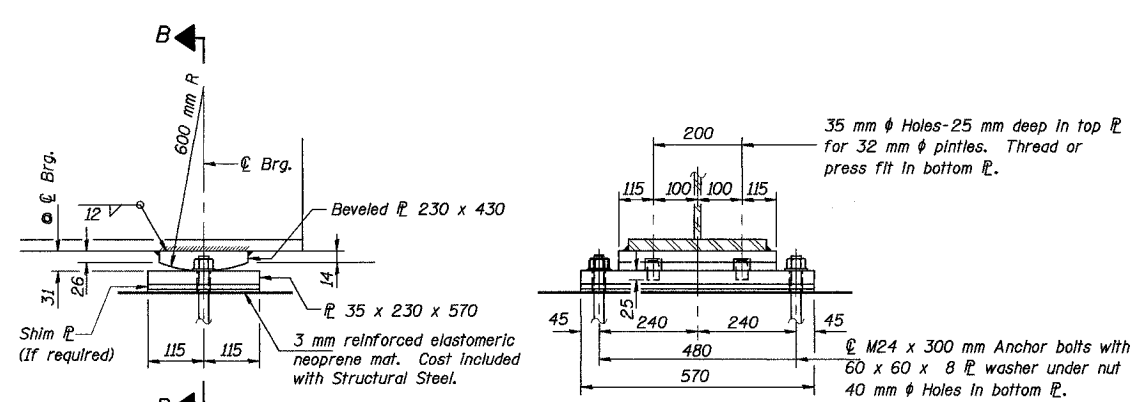
Designed by: AK
Checked by: WEE
Drafted by: JMG
Checked by: AK

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	430	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

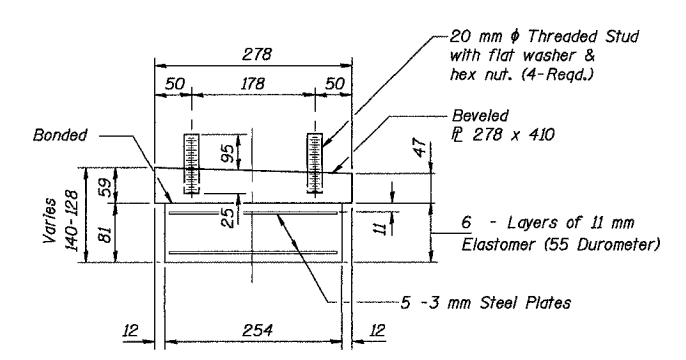
CONTRACT NO. 68200



ELEVATION AT W. ABUT.
TYPE I ELASTOMERIC EXP. BRG.

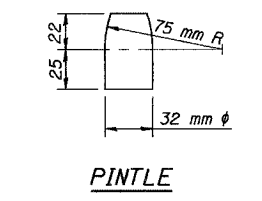


ELEVATION AT E ABUT.
FIXED BEARING



BEARING ASSEMBLY

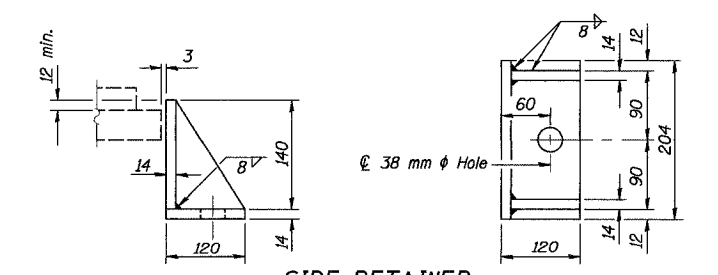
Note: Shim plates shall not be placed under Bearing Assembly.



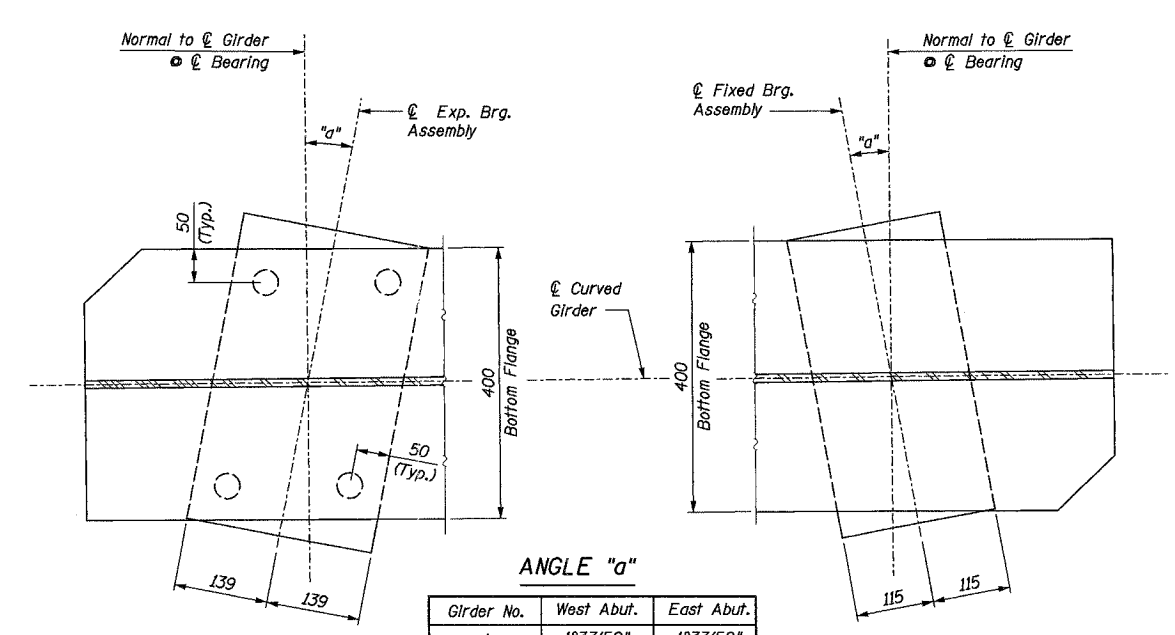
PINTLE

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly Type I	Each	6



SIDE RETAINER
Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates. Mass included with Structural Steel.



PLAN (WEST ABUTMENT)

PLAN (EAST ABUTMENT)

ANGLE "a"

Girder No.	West Abut.	East Abut.
1	1°33'52"	1°33'52"
2	1°33'23"	1°33'23"
3	1°32'54"	1°32'54"
4	1°32'26"	1°32'26"
5	1°31'58"	1°31'58"
6	1°31'30"	1°31'30"

Notes:
All structural steel shall be AASHTO M 270M Grade 345.
Anchor bolts at fixed bearings may be built into the masonry.
See Dwg. No. A-1 for Anchor Bolt Installation.
All dimensions are in millimeters (mm) unless noted otherwise.

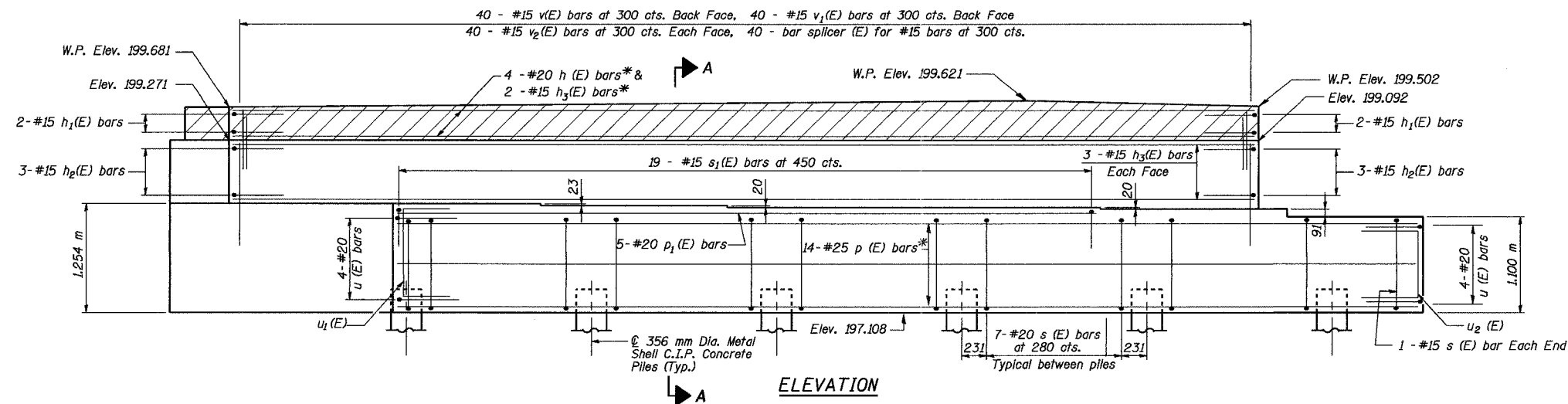
REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION FIXED & ELASTOMERIC EXPANSION BEARINGS RAMP A-3 OVER RAMP B-5 F.A.I. ROUTE 74 (SECTION 72-7)R-3 PEORIA COUNTY STA. 10+418.515 (RAMP A-3) STRUCTURE NUMBER 072-0172 PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. BRG-1	SCALE N.T.S.	DATE 11/16/04
		SHEET NO. 11

Date: 11/19/2004 Time: 09:42:49 AM File: P:\643996\structural\A3\072-0172-0172-Sheet\Tracings\BRG0001-A0720172.dgn

Designed by: AK
Checked by: WEE
Drafted by: RKS
Checked by: WEE

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	431	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200



SEAT ELEVATIONS

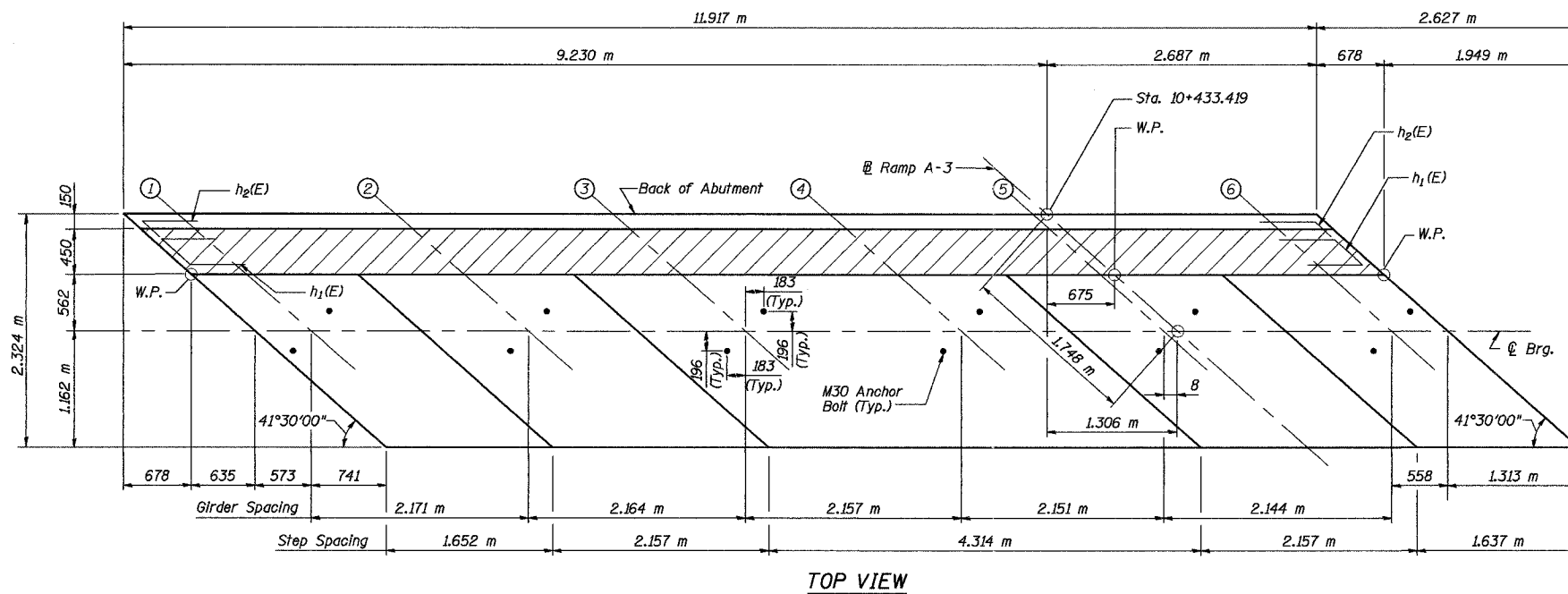
Girder No.	Seat Elevation
1	198.362
2	198.339
3	198.319
4	198.319
5	198.299
6	198.208

PILE DATA

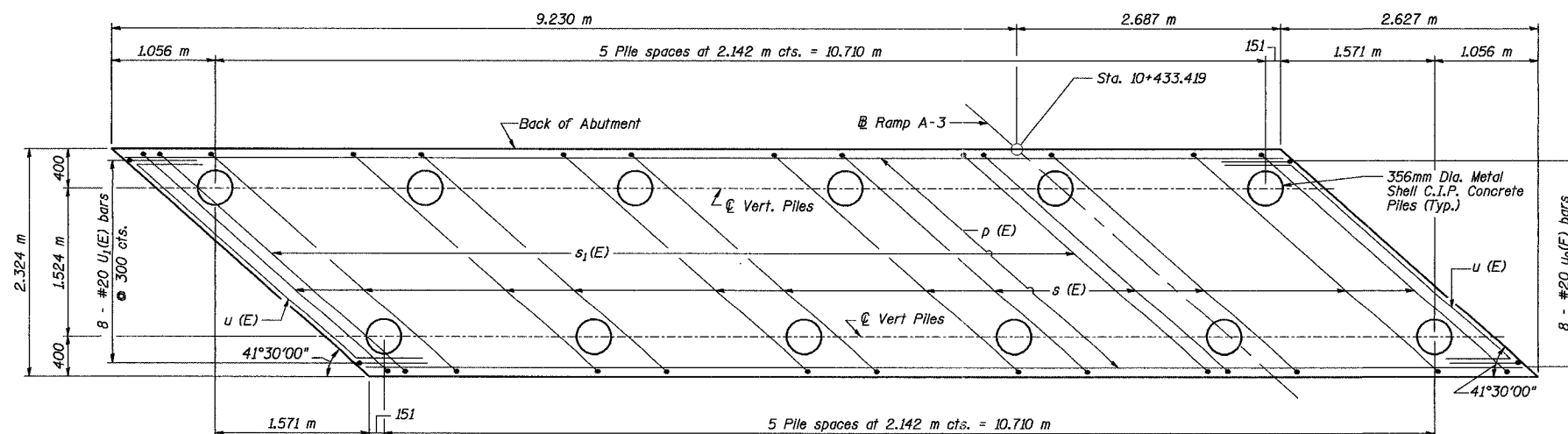
Type: 356 mm Dia. Metal Shell C.I.P. Concrete Piles
 Capacity: 500 kN
 Est. Length: 19.0 m
 No. Required: 11 plus 1 test pile

Notes:

- Work this drawing with Dwg. No. AB-2.
- Top of backwall shall conform to grade and cross slope of roadway.
- All dimensions are in millimeters (mm) except as noted.
- Space reinforcement in cap to miss anchor bolts.
- Pour steps monolithically with cap.
- Reinforcement bars designated (E) shall be epoxy coated.
- Hatched area to be poured after superstructure forms have been removed. Quantity of concrete included with Concrete Superstructure on Dwg No. DK-2.
- For Bar Schedule and Bill of Material, See Dwg. No. AB-2.
- For Metal Shell Pile Details, see Dwg.No. PL-1.
- For Anchor Bolt Details, see Dwg. No. A-1.
- Bridge Seat Sealer shall be applied to abutment bridge seat.
- All concrete edges shall have 20mm chamfer, unless noted otherwise.
- * See Section A-A, Dwg. No. AB-2 for bar placement.



TOP VIEW



PLAN-PILE CAP

Date: 11/19/2004 Time: 09:42:35 AM

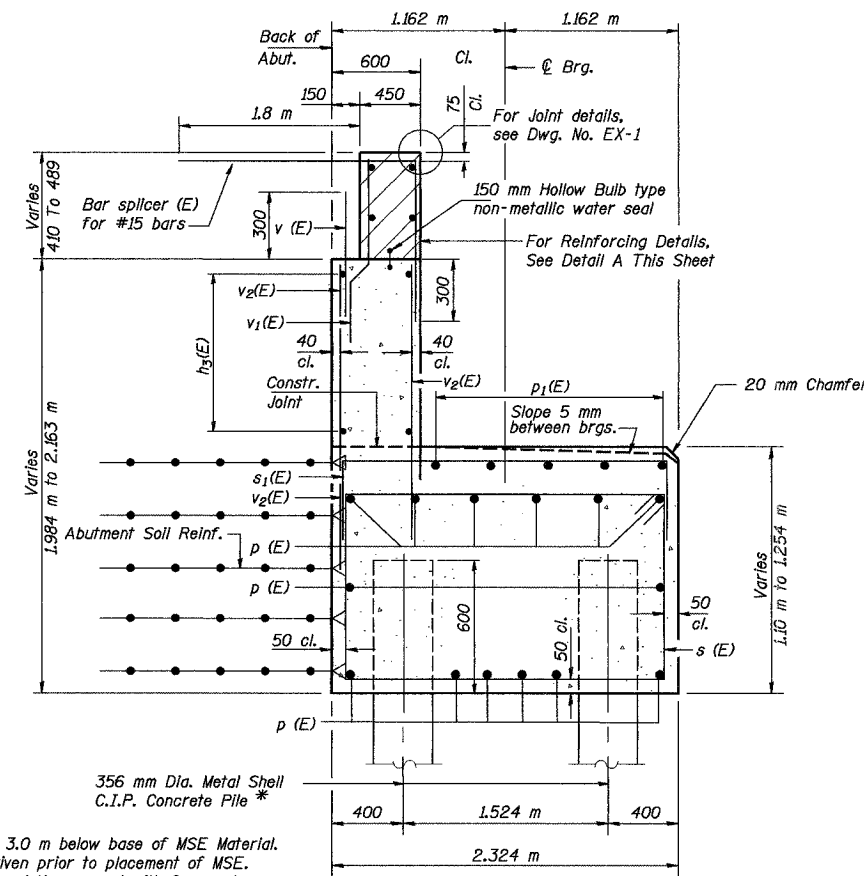
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Designed by: WEE
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 Checked by: WEE

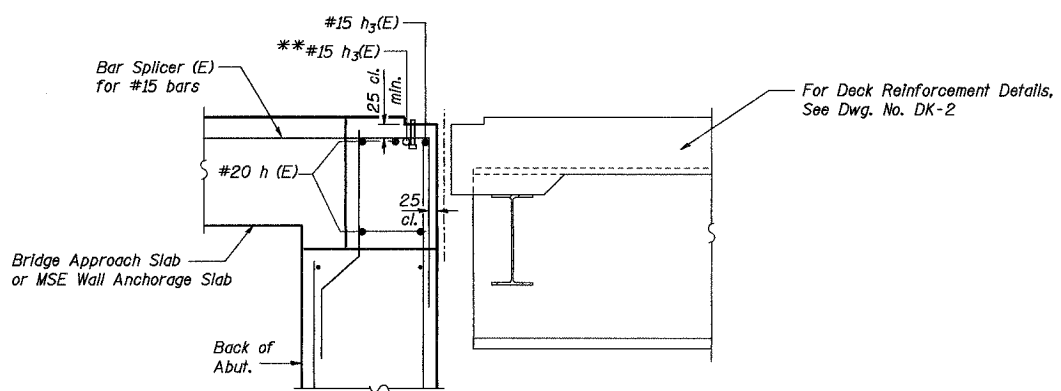
REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
WEST ABUTMENT		
RAMP A-3 OVER RAMP B-5 F.A.I. ROUTE 74 (SECTION 72-7)R-3 PEORIA COUNTY STA. 10+418.515 (RAMP A-3) STRUCTURE NUMBER 072-0172		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. AB-1	SCALE N.T.S.	DATE 11/16/04
		SHEET NO. 12

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	432	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

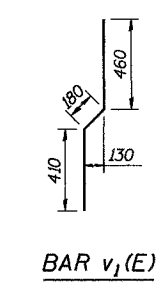
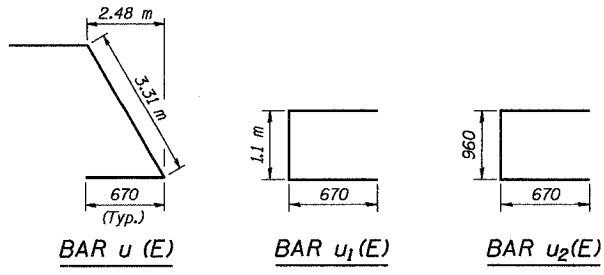
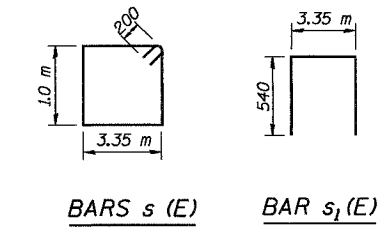
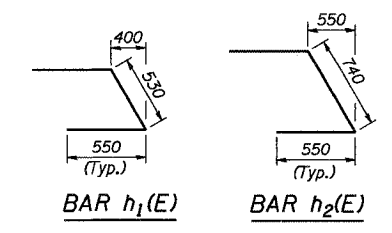
CONTRACT NO. 68200



* Precast Piles to 3.0 m below base of MSE Material. Piles must be driven prior to placement of MSE. Coat Piles above existing ground with 6 mm min. thickness of either asphaltic cement or S.C. or M.C. liquid asphalt. (See S.N. 072-8556)



**Place $h_3(E)$ bars in back of anchor bolt as shown, if required to maintain 25 mm cl. (+0-3 mm). Anchor bolts should be tied to $h_3(E)$ bars.



**WEST ABUTMENT
BILL OF MATERIAL**

Bar	No.	Size	Length (m)	Shape
$h(E)$	4	#20	11.82	
$h_1(E)$	4	#15	1.63	
$h_2(E)$	6	#15	1.84	
$h_3(E)$	7	#15	11.82	
$p(E)$	14	#25	11.82	
$p_1(E)$	5	#20	8.02	
$s(E)$	37	#20	9.10	
$s_1(E)$	19	#15	4.43	
$u(E)$	8	#20	4.65	
$u_1(E)$	8	#20	2.44	
$u_2(E)$	8	#20	2.30	
$v(E)$	40	#15	0.70	
$v_1(E)$	40	#15	1.05	
$v_2(E)$	80	#15	1.30	
Concrete Structures		m^3	39.6	
Reinforcement Bars, Epoxy Coated		kg	2390	
Bridge Seat Sealer		m^2	21	
Furnishing Metal		m	209	
Pile Shells, 356 mm		m	209	
Driving and Filling Shells		m	209	
Test Pile Metal Shells		Ea.	1	

Notes:
 Work this drawing with Dwg. No. AB-1.
 Reinforcement bars designated (E) shall be epoxy coated.
 Dimensions are in millimeters (mm) unless noted otherwise.

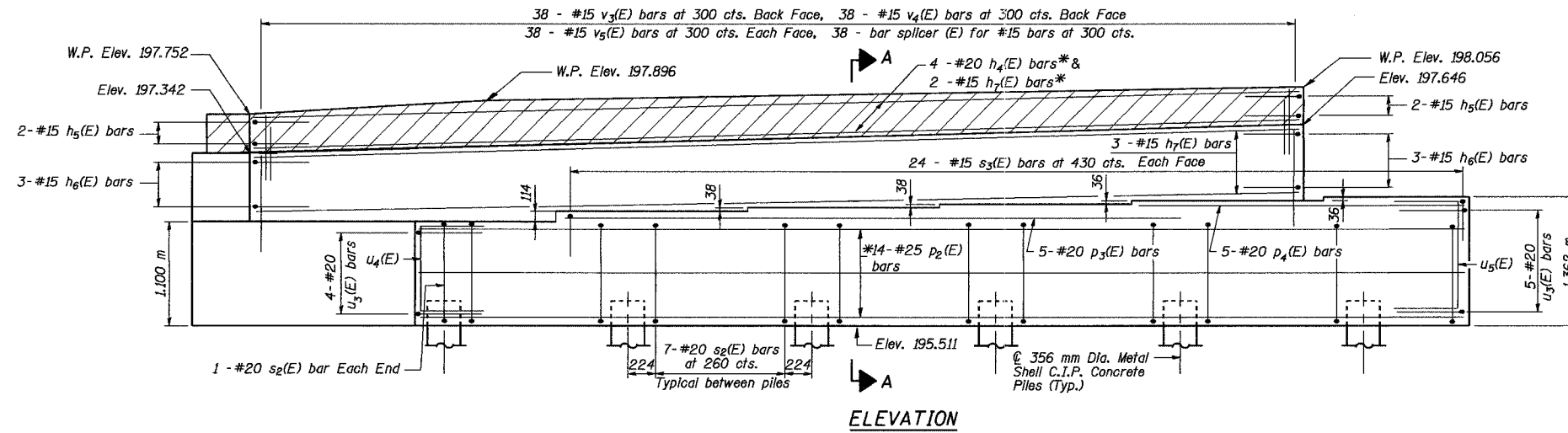
REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
WEST ABUTMENT DETAILS		
RAMP A-3 OVER RAMP B-5 F.A.I. ROUTE 74 (SECTION 72-7)R-3 PEORIA COUNTY STA. 10+418.515 (RAMP A-3) STRUCTURE NUMBER 072-0172		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. AB-2	SCALE N.T.S.	DATE 11/16/04
		SHEET NO. 13

Date: 11/19/2004 Time: 09:21:57 AM File name: P:\643996\struc\ur\A3\072-0172\Sheet\Tracings\AB0002-1\A0720172.dgn

Designed by: WEE
 Checked by: RW
 Drafted by: RKS
 Checked by: WEE

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	433	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200



SEAT ELEVATIONS

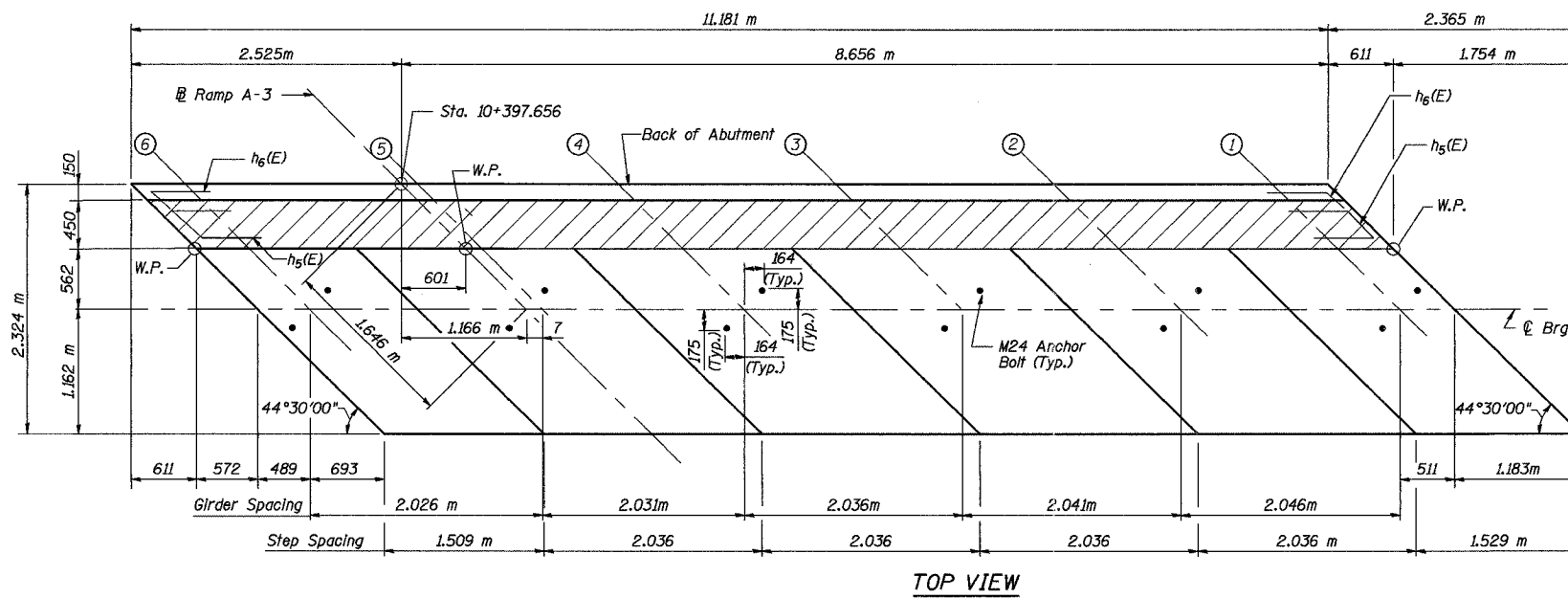
Girder No.	Seat Elevation
1	196.873
2	196.837
3	196.801
4	196.763
5	196.725
6	196.611

PILE DATA

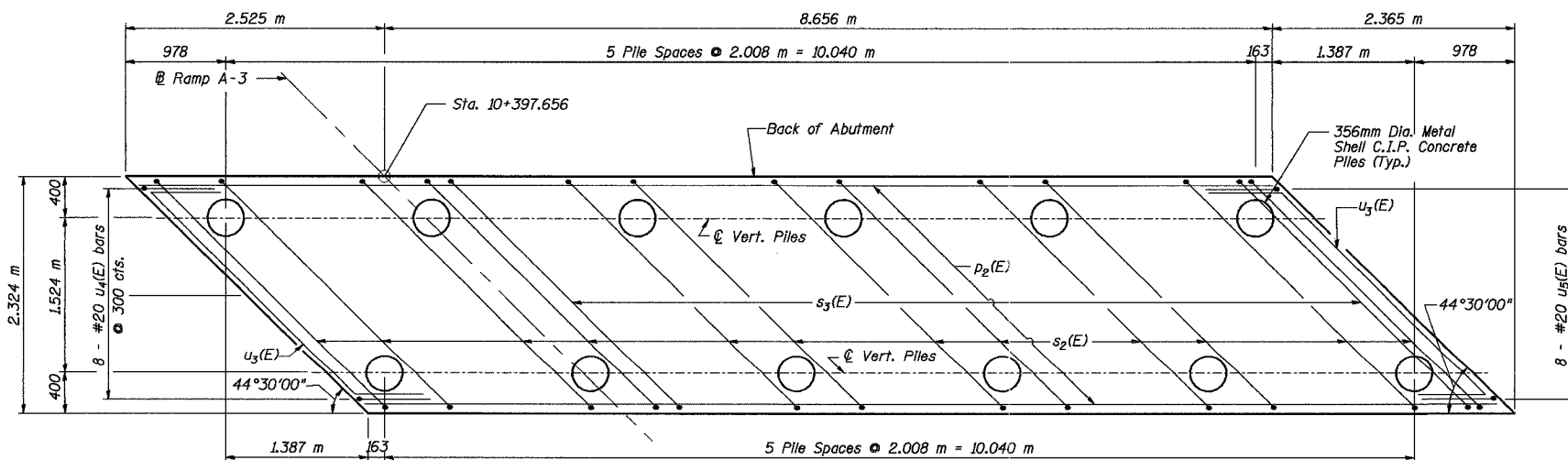
Type: 356 mm Dia. Metal Shell C.I.P. Concrete Piles
 Capacity: 500 kN
 Est. Length: 19.0 m
 No. Required: 12

Notes:

- Work this drawing with Dwg. No. AB-4.
- Top of backwall shall conform to grade and cross slope of roadway.
- All dimensions are in millimeters (mm) except as noted.
- Space reinforcement in cap to miss anchor bolts.
- Pour steps monolithically with cap.
- Reinforcement bars designated (E) shall be epoxy coated.
- Hatched area to be poured after superstructure forms have been removed. Quantity of concrete included with Concrete Superstructure on Dwg. No. DK-2.
- For Bar Schedule and Bill of Material, See Dwg. No. AB-4.
- For Metal Shell Pile Details, see Dwg. No. PL-1.
- For Anchor Bolt Details, See Dwg. No. A-1.
- Bridge Seat Sealer shall be applied to abutment bridge seat.
- All concrete edges shall have 20 mm chamfer unless noted otherwise.
- * See Section A-A, Dwg. No. AB-4 for bar placement.



TOP VIEW

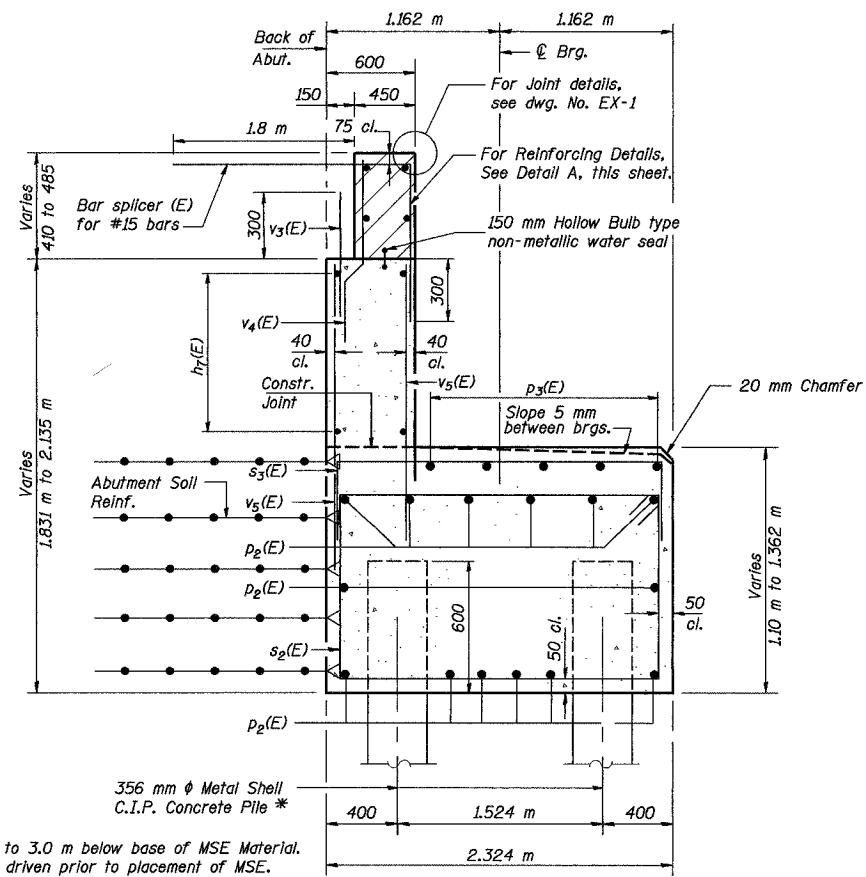


PLAN-PILE CAP

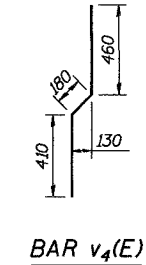
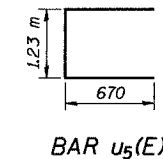
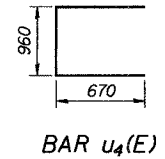
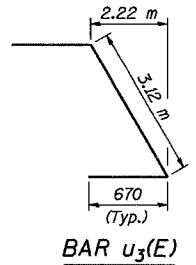
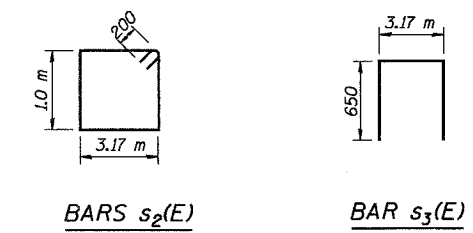
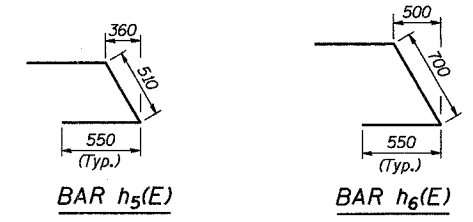
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Designed by:	WEE
Checked by:	RW
Drafted by:	RKS
Checked by:	WEE

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
EAST ABUTMENT		
RAMP A-3 OVER RAMP B-5 F.A.I. ROUTE 74 (SECTION 72-7)R-3 PEORIA COUNTY STA. 10+418.515 (RAMP A-3) STRUCTURE NUMBER 072-0172		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. AB-3	SCALE N.T.S.	DATE 11/16/04
		SHEET NO. 14

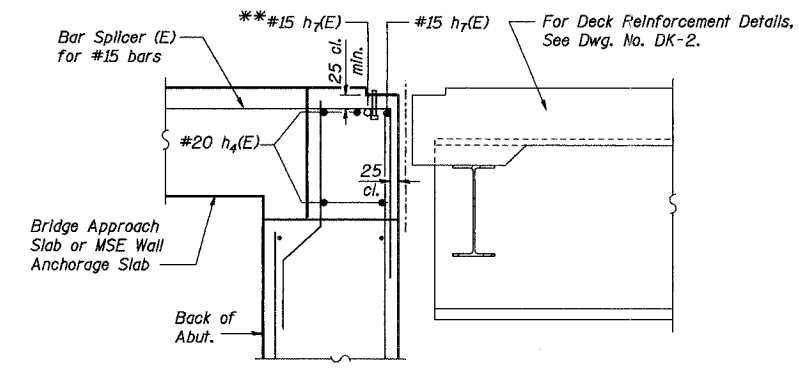


* Precast Piles to 3.0 m below base of MSE Material. Piles must be driven prior to placement of MSE. Coat Piles above existing ground with 6 mm min. thickness of either asphaltic cement or S.C. or M.C. liquid asphalt. (See S.N. 072-8557).



BILL OF MATERIAL

Bar	No.	Size	Length (m)	Shape
$h_4(E)$	4	#20	11.10	
$h_5(E)$	4	#15	1.61	
$h_6(E)$	6	#15	1.80	
$h_7(E)$	7	#15	11.10	
$p_2(E)$	14	#25	11.08	
$p_3(E)$	5	#20	6.72	
$p_4(E)$	5	#20	3.47	
$s_2(E)$	37	#20	8.74	
$s_3(E)$	24	#15	4.47	
$u_3(E)$	9	#20	4.46	
$u_4(E)$	8	#20	2.30	
$u_5(E)$	8	#20	2.57	
$v_3(E)$	38	#15	0.70	
$v_4(E)$	38	#15	1.05	
$v_5(E)$	76	#15	1.10	
Concrete Structures			m ³	37.6
Reinforcement Bars, Epoxy Coated			kg	2340
Furnishing Metal Pile Shells, 356 mm			m	228
Driving and Filling Shells			m	228
Bridge Seat Sealer			m ²	20



** Place $h_7(E)$ bars in back of anchor bolt as shown, if required to maintain 25 mm cl. (+0-3 mm) Anchor bolts should be tied to $h_7(E)$ bars.

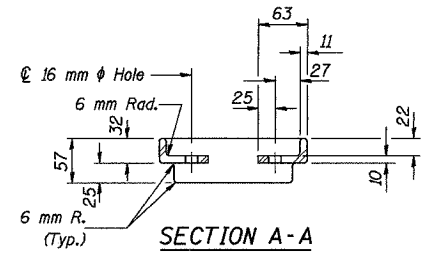
Notes:
 Work this drawing with Dwg. No. AB-3.
 Reinforcement bars designated (E) shall be epoxy coated.
 All dimensions are in millimeters (mm) unless noted otherwise.

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
EAST ABUTMENT DETAILS		
RAMP A-3 OVER RAMP B-5 F.A.I. ROUTE 74 (SECTION 72-7)R-3 PEORIA COUNTY STA. 10+418.515 (RAMP A-3) STRUCTURE NUMBER 072-0172		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. AB-4	SCALE N.T.S.	DATE 11/16/04
		SHEET NO. 15

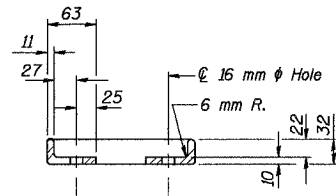
Designed by: WEE
 Checked by: RW
 Drafted by: RKS
 Checked by: WEE

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	435	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

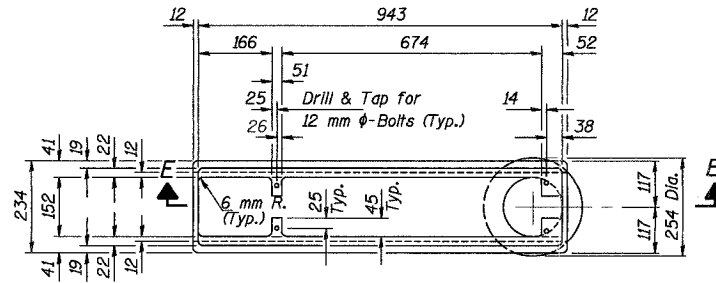
CONTRACT NO. 68200



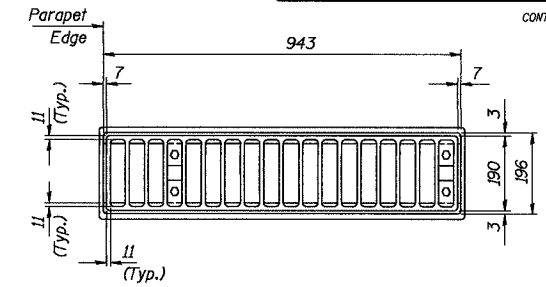
SECTION A-A



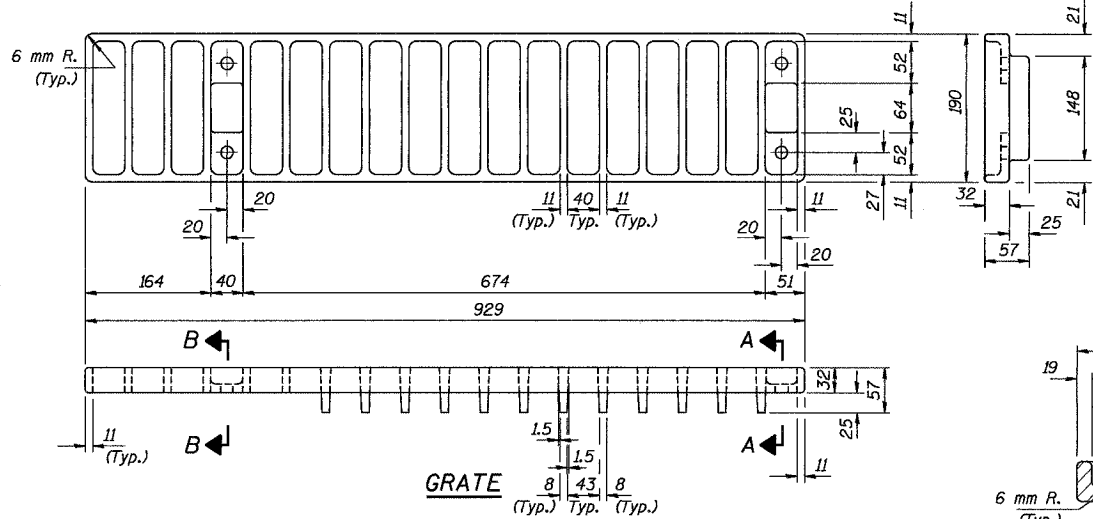
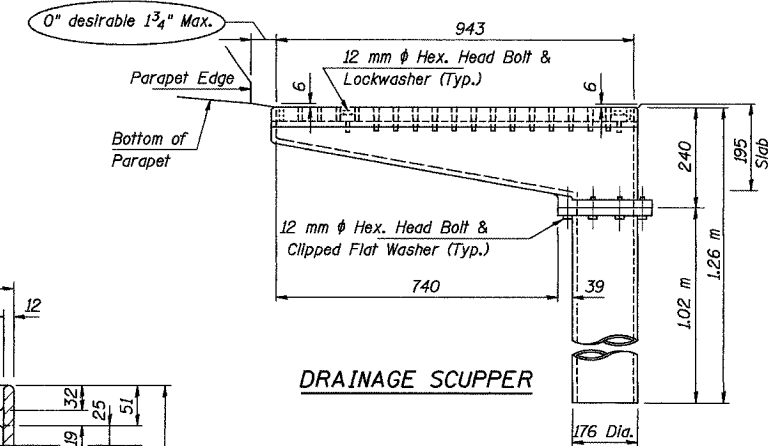
SECTION B-B



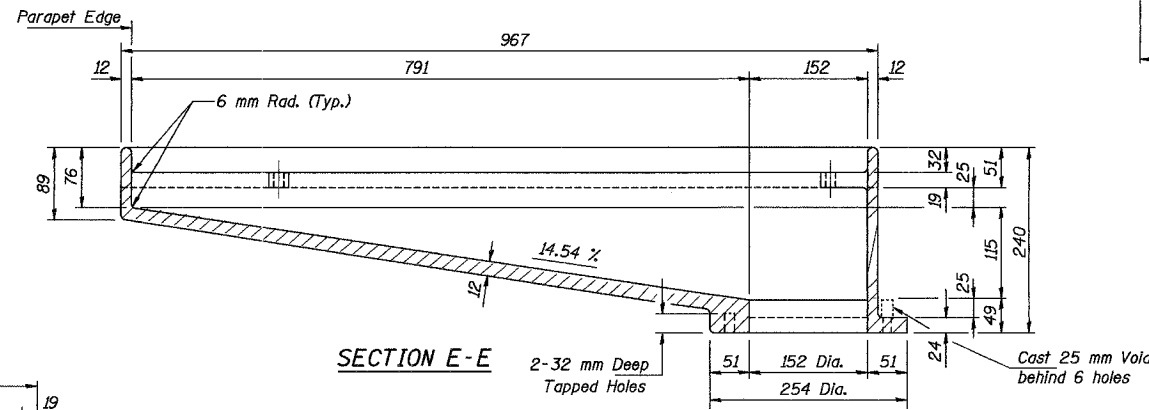
FRAME



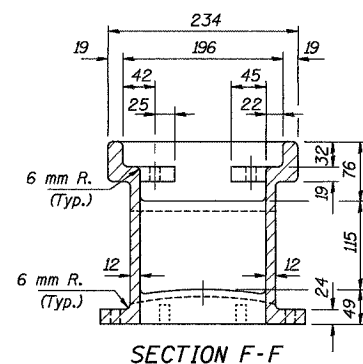
DRAINAGE SCUPPER



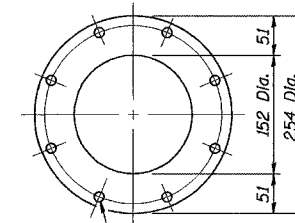
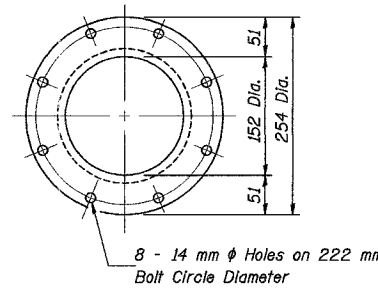
GRATE



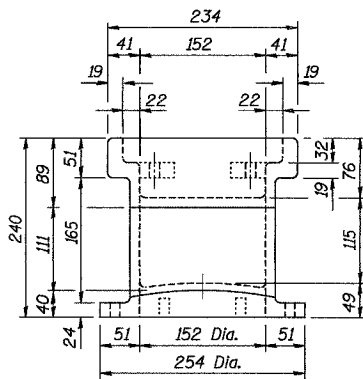
SECTION E-E



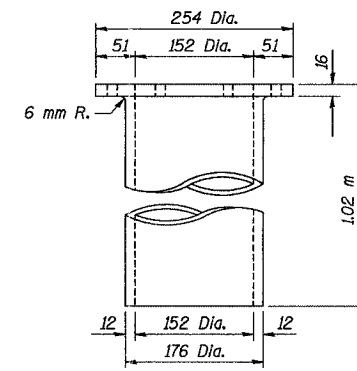
SECTION F-F



VIEW D-D



VIEW C-C



DOWNSPOUT

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Drainage Scupper, Type I	Each	2

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
DRAINAGE SCUPPER, TYPE I		
RAMP A-3 OVER RAMP B-5 F.A.I. ROUTE 74 (SECTION 72-7)R-3 PEORIA COUNTY STA. 10+418.515 (RAMP A-3) STRUCTURE NUMBER 072-0172		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. D-1	SCALE N.T.S.	DATE 11/16/04
		SHEET NO. 16

Notes: All cast iron parts shall be gray iron conforming to the requirements of AASHTO M 105, Class 30.
Bolts and washers shall conform to the requirements of ASTM A 307. All bolts and washers shall be galvanized according to AASHTO M 232. As an alternate bolts and washers may be stainless steel.
Cost of the Grate, Frame, Downspout, bolts and washers including complete installation of Scupper will be paid for at the unit bid price each for "DRAINAGE SCUPPER, TYPE I." The Contractor may use at his option steel drainage scuppers or cast iron drainage scuppers.
All dimensions are in millimeters (mm) except as noted.

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	436	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

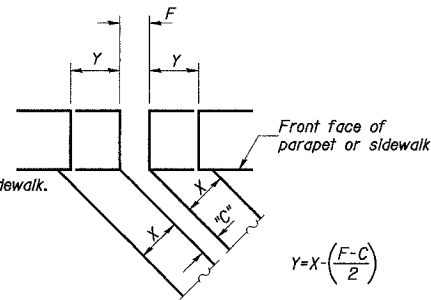
CONTRACT NO. 68200

Joint Size	"C" at 10 °C	"D" at 10 °C
50	50	40 Min.
65	65	45 Min.
100	75	65 Min.

INSTALLATION NOTES

- Install continuous seal in roadway, parapet, curb, and sidewalk.
- Install anchor blocks as indicated.

NOTE A: Maximum spacing of anchor bolts shall be 300 centers.



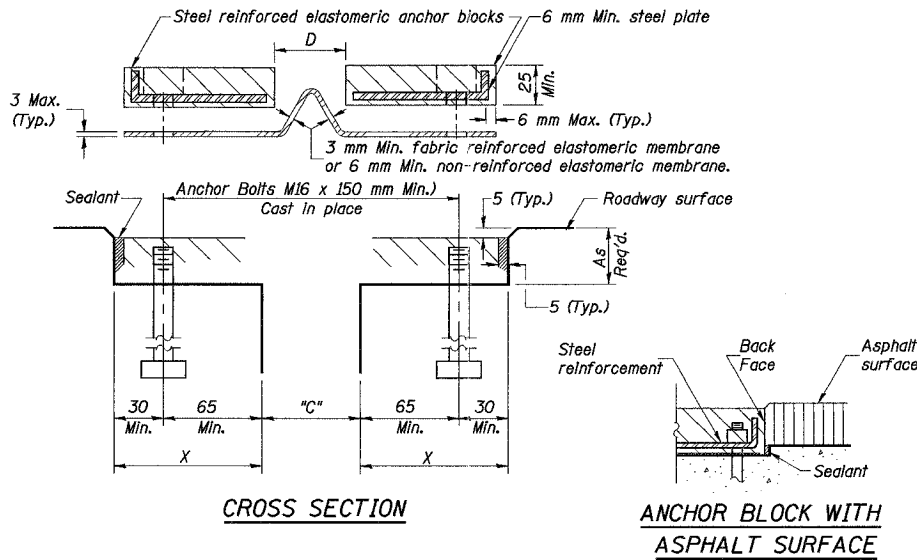
$$Y = X - \left(\frac{F - C}{2} \right)$$

For dimension "F" see dwg DK-1

FORMING BLOCKOUT SKETCH

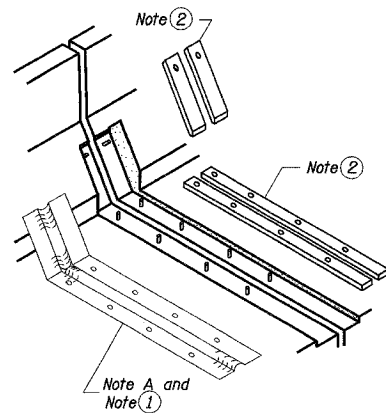
SKREW LIMITATIONS

The details of the anchor blocks and the elastomeric membrane in the parapet, as shown, are for up to 50° skews. For skews greater than 50°, the anchor blocks and the elastomeric membrane, installed according to dimension "D", might require modifications to insure a minimum clearance of 40 mm from centerline of anchor studs to edge of parapet opening. The anchor blocks and the elastomeric membrane shall also be installed to the top of the parapet with the anchor studs spaced at ±300 cts.

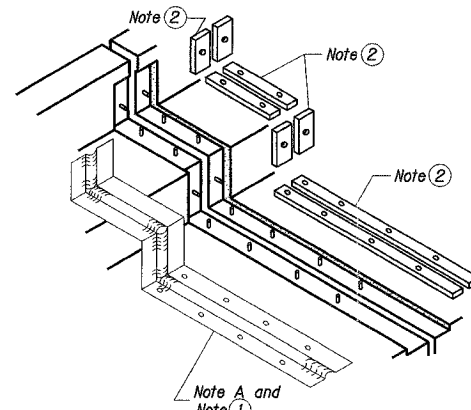


GENERAL NOTES

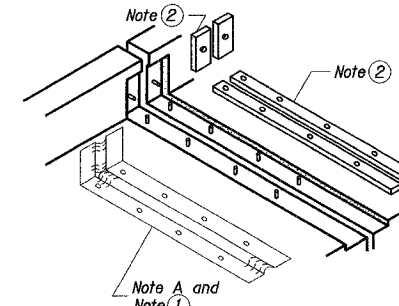
Continuous Seal Neoprene Expansion Joint shall consist of molded anchor blocks of elastomer and steel, field assembled over continuous lengths of elastomeric membrane.
 The elastomeric membrane shall be premolded with a single or a double upward convolution that will have a "memory" to return to its molded position upon joint closure.
 The convolution length shall be such that the extended length will not be greater than the manufactured length when the joint is fully expanded in its design range and will not protrude above the anchor blocks when the joint is fully compressed.
 Joint openings shall be adjusted according to Article 503.10(c) of the Standard Specifications when the deck is poured at an ambient temperature other than 10 °C.
 The parapet and roadway membrane shall be made continuous by an approved vulcanizing process. Lapping will not be permitted.
 All dimensions are in millimeters (mm) except as noted.



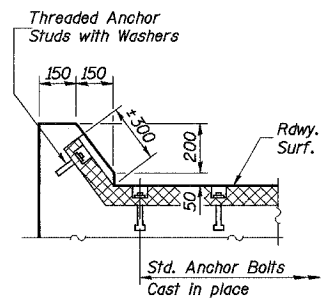
AT PARAPET



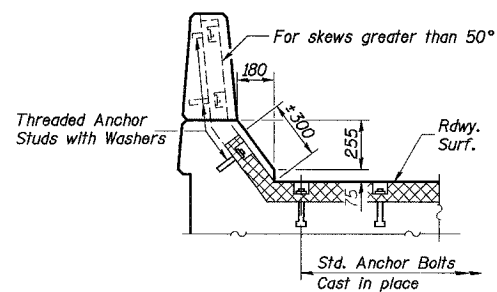
AT SIDEWALK OR MEDIAN



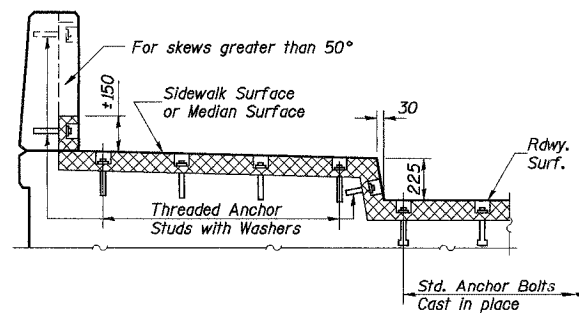
AT WALL



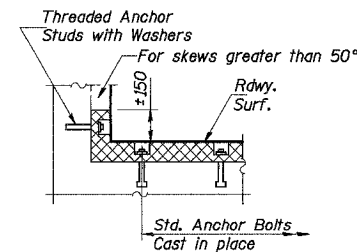
AT CURB



AT PARAPET



**AT SIDEWALK OR MEDIAN
TYPICAL END TREATMENTS**



AT WALL

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION CONTINUOUS SEAL TYPE NEOPRENE EXPANSION JOINTS RAMP A-3 OVER RAMP B-5 F.A.I. ROUTE 74 (SECTION 72-7)R-3 PEORIA COUNTY STA. 10+418.515 (RAMP A-3) STRUCTURE NUMBER 072-0172 PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. EX-1	SCALE N.T.S.	DATE 11/16/04
		SHEET NO. 17

EJ-CS (M)
4-30-97

Date: 11/19/2004 Time: 09:44:20 AM

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ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	437	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200

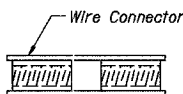
The diameter of this part is the same as the diameter of the bar spliced.

The diameter of this part is equal or larger than the diameter of bar spliced.

ROLLED THREAD DOWEL BAR



** ONE PIECE



WELDED SECTIONS

BAR SPLICER ASSEMBLY ALTERNATIVES

** Heavy Hex Nuts conforming to ASTM A 563M, Grade C, D or DH may be used.

NOTES

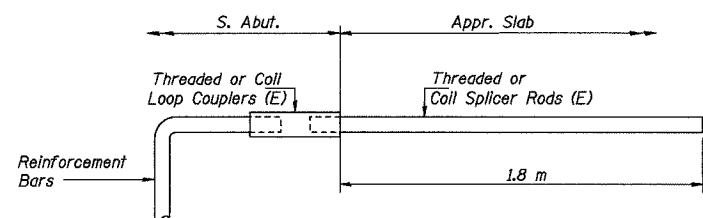
Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars. Splicer rods shall be of minimum 400 MPa yield strength, threaded or rolled full length. All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars. Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

- ① Minimum Capacity = $1.25 \times 10^{-3} \times f_y \times A_t$
(Tension in kN)
- ② Minimum *Pull-out Strength = $1.25 \times 10^{-3} \times f_s \text{ allow} \times A_t$
(Tension in kN)

Where f_y = Yield strength of lapped reinforcement bars in MPa.
 $f_s \text{ allow}$ = Allowable tensile stress in lapped reinforcement bars in MPa (Service Load)
 A_t = Tensile stress area of lapped reinforcement bars (mm^2).
 * = 28 day concrete

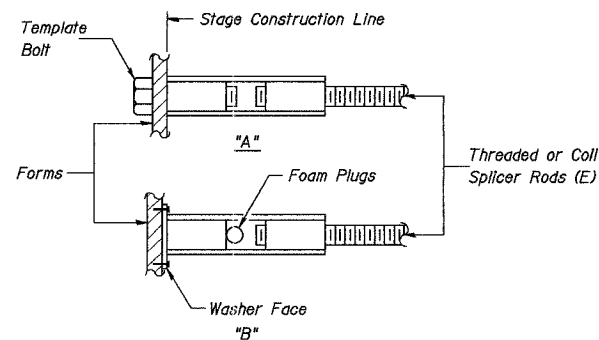
Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kN - tension	Min. Pull-Out Strength kN - tension
#15	610 mm	100	40
#20	790 mm	150	60
#25	1.04 m	250	100
#30	1.37 m	350	140

Bar splicer assemblies shall be according to Section 508 of the Standard Specifications, except as noted. The furnishing and installation of bar splicer assemblies will be measured and paid for at the contract unit price each for "BAR SPLICERS."
 All dimensions are in millimeters (mm) except as noted.



**ABUTMENT
BAR SPLICER ASSEMBLY DETAIL
FOR #15 BAR**

Min. Capacity = 100 kN - tension
Min. Pull-out Strength = 40 kN - tension
No. Required = 78



INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
BAR SPLICER ASSEMBLY DETAILS		
RAMP A-3 OVER RAMP B-5 F.A.I. ROUTE 74 (SECTION 72-7)R-3 PEORIA COUNTY STA. 10+418.515 (RAMP A-3) STRUCTURE NUMBER 072-0172		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. BS-1	SCALE N.T.S.	DATE 11/16/04
		SHEET NO. 18

BSD-1 (M)
4-30-97

Time: 09:44:42 AM

Date: 11/19/2004

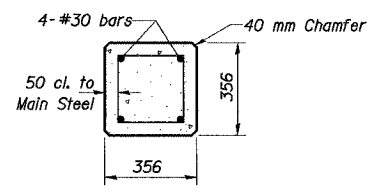
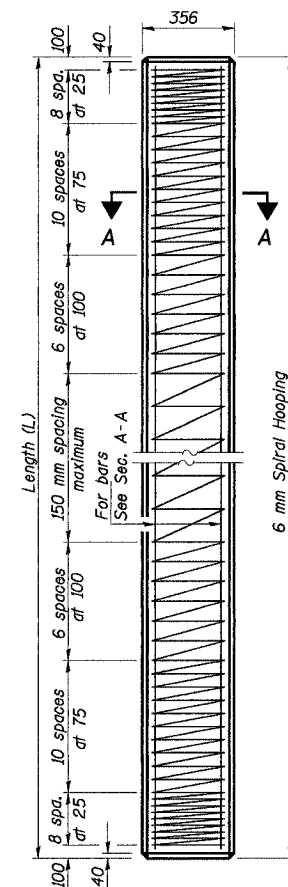
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ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	438	1365
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

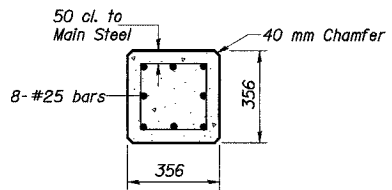
CONTRACT NO. 68200

Date: 11/19/2004 Time: 09:45:06 AM

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**SECTION A-A
FOR PILES UNDER
13.5 m LONG**

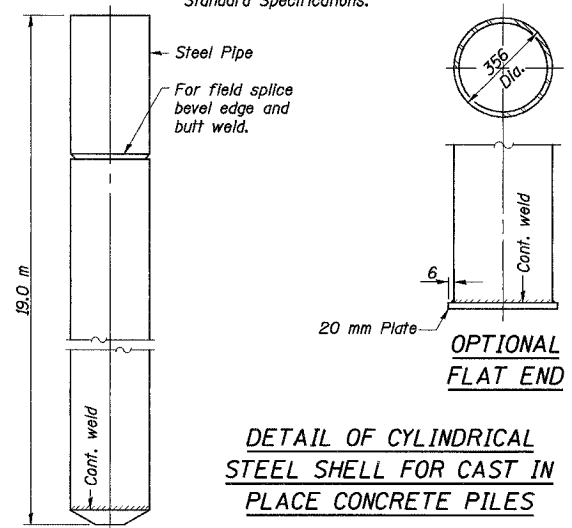


**SECTION A-A
FOR PILES 13.5 m
OR MORE**

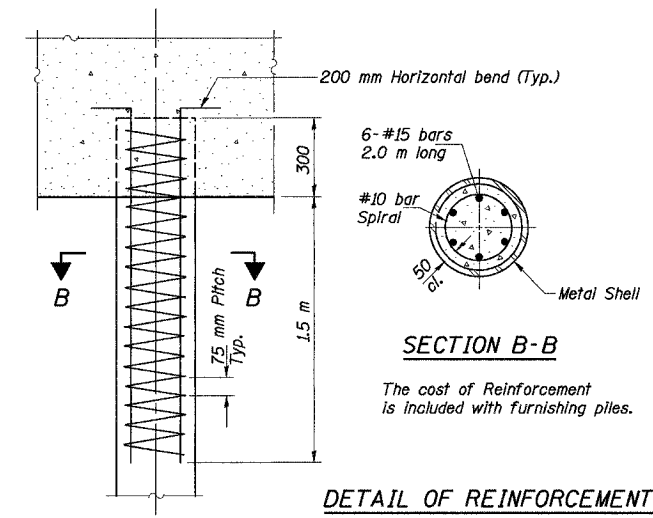
Handling: For Pile lengths up to 13.5 m use two slings placed at a distance of 0.21L from each end. For Piles longer than 13.5 m, use three slings placed at a distance of 0.12L from each end and at mid point of pile.

**DETAIL OF PRECAST
CONCRETE PILES**

Notes: Driving and bearing ends of pipe shall be cut square. The thickness of the shell shall be 6.35 mm with a tolerance of 5%. The shell shall be according to Article 1006.05(a) of the Standard Specifications.

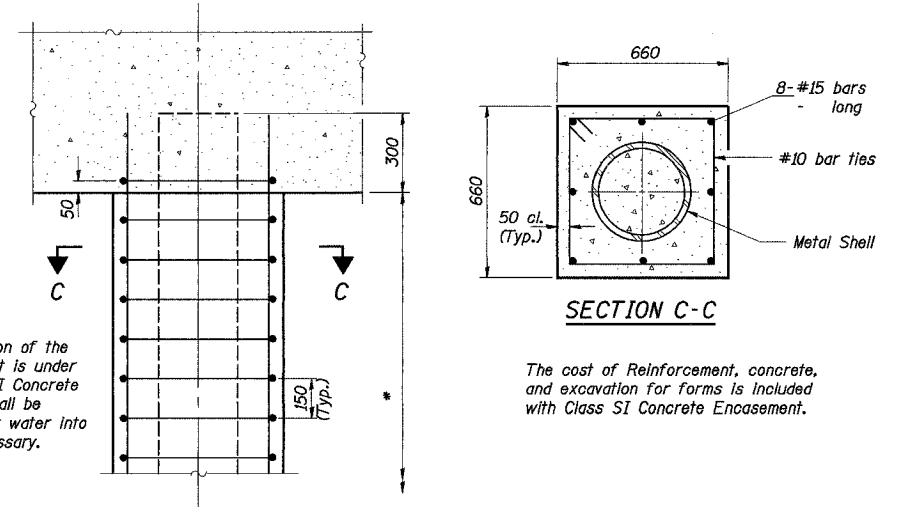


**DETAIL OF CYLINDRICAL
STEEL SHELL FOR CAST IN
PLACE CONCRETE PILES**



**DETAIL OF REINFORCEMENT FOR
METAL SHELLS AT ABUTMENTS**

The cost of Reinforcement is included with furnishing piles.



**DETAIL OF PROTECTION
FOR METAL SHELLS AT PIERS**

The cost of Reinforcement, concrete, and excavation for forms is included with Class SI Concrete Encasement.

All dimensions are in millimeters (mm) except as noted.

X-PB (M)
4-30-97

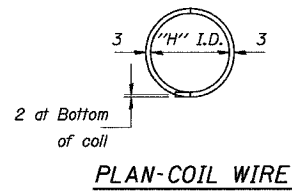
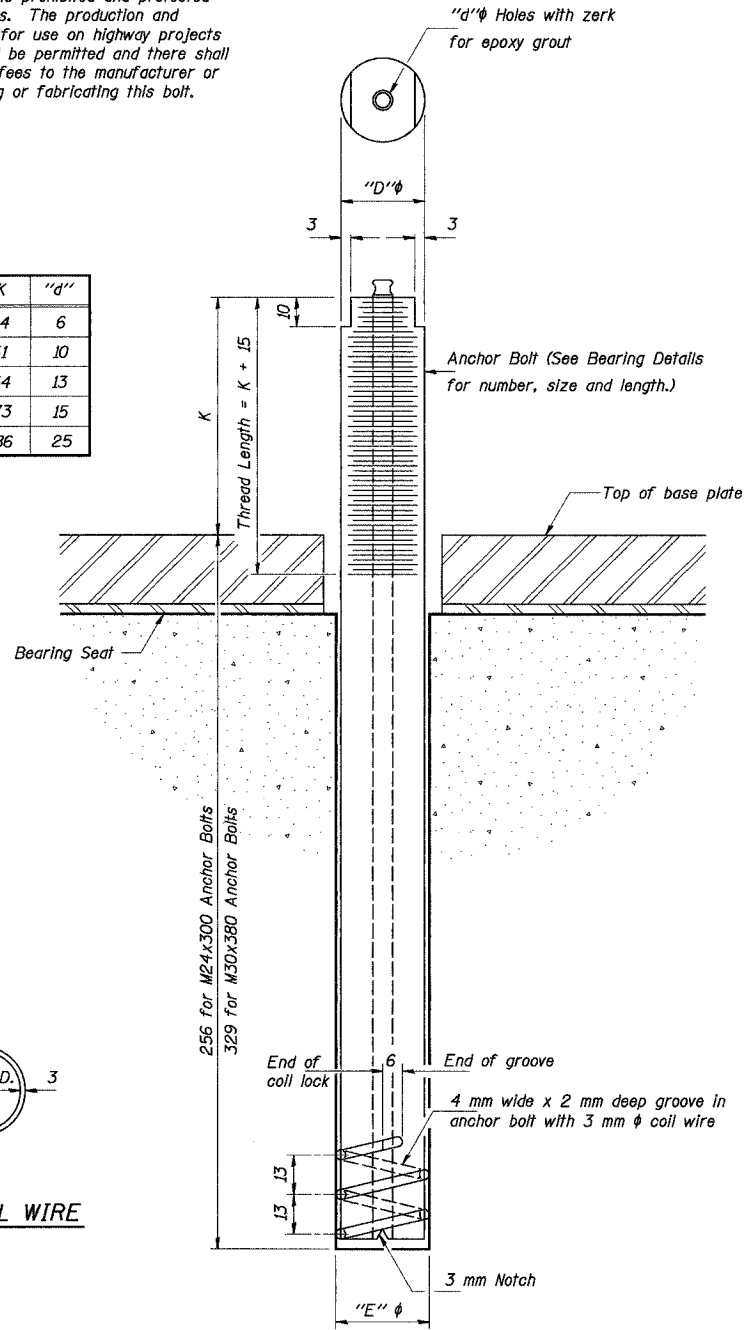
REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
METAL SHELL PILE DETAILS		
RAMP A-3 OVER RAMP B-5 F.A.I. ROUTE 74 (SECTION 72-7)R-3 PEORIA COUNTY STA. 10+418.515 (RAMP A-3) STRUCTURE NUMBER 072-0172		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. PL-1	SCALE N.T.S.	DATE 11/16/04
		SHEET NO. 19

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	439	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200

The Illinois Coil-Lock Anchor Bolt is a proprietary item which is the property of the Illinois Department of Transportation. Use, reproduction or disclosure without express written permission is prohibited and protected under Federal copyright laws. The production and the fabrication of this bolt for use on highway projects in the State of Illinois shall be permitted and there shall be no incurred charges or fees to the manufacturer or the fabricator for producing or fabricating this bolt.

D	E	H	K	"d"
24	27	20	44	6
30	33	26	51	10
36	39	32	54	13
48	51	44	73	15
64	67	60	86	25



ILLINOIS COIL-LOCK ANCHOR BOLT

MATERIALS FOR ILLINOIS COIL-LOCK ANCHOR BOLT

The anchor bolt shall be fabricated from cold drawn or hot finished seamless carbon steel mechanical tubing conforming to ASTM A 519, Grade 1026, CW and supplied with hexagonal nuts and cut washers.
The coil wire shall be made of any suitable soft steel wire.
The finished anchor bolt shall be cleaned of rust and other foreign materials and wrapped or packaged to prevent contamination until they are installed.
The epoxy grout shall be a two-component, epoxy resin bonding system conforming to ASTM C 881, Type I, Grade 1 and of a Class suitable for the temperature at installation.

INSTALLATION PROCEDURE for the ILLINOIS COIL-LOCK ANCHOR BOLT

1. With the coil wire in place, the bolt shall be inserted into the hole and turned clockwise to a snug fit in the hole. Nut and washer shall be placed on the bolt. The nut shall be tensioned until the steel base plates are held securely to the concrete bearing seat.
2. Epoxy grout shall be pumped through the zerk fitting with a pressure gun. Pumping shall continue until the epoxy overflows the hole around the bolt shank. After pumping is discontinued, excess epoxy shall be immediately wiped off.

ALTERNATE ANCHOR BOLTS

The Contractor may use, at his option, the capsule or the adhesive cartridge type anchor rods that have been previously tested and given a prior approval by the Department. The Contractor shall install these anchor rods in pre-drilled holes according to the manufacturer's recommendations and procedures.
The capsule or the adhesive cartridge type anchor rods shall be a two part system composed of:
1. A threaded rod stud with nut and washer of the type specified.
2. A sealed glass capsule or a sealed glass adhesive cartridge containing premeasured amounts of the adhesive chemical.

Location	Type
East Abut.	A307
West Abut.	A307

ASTM F 1554 (Fy = 724 MPa), ASTM A 449 and AASHTO M 314 (Fy = 724 MPa) anchor bolts may be substituted for the anchor bolts shown above.

GENERAL NOTES

Holes in the masonry for anchor bolts shall be drilled through the base plates to the diameter and depth shown or according to the manufacturer's recommendation after beams or girders have been erected and adjusted.
Prior to setting the bolts, the holes shall be dry and all dust and loose particles shall be removed by the use of compressed air or vacuuming.
The anchor bolts, furnished and installed including the epoxy grout or capsules shall not be paid for separately but shall be included in the unit bid price for "Furnishing and Erecting Structural Steel".
All dimensions are in millimeters (mm) except as noted.

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
ANCHOR BOLT DETAILS		
RAMP A-3 OVER RAMP B-5 F.A.I. ROUTE 74 (SECTION 72-7)R-3 PEORIA COUNTY STA. 10+418.515 (RAMP A-3) STRUCTURE NUMBER 072-0172		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. A-1	SCALE N.T.S.	DATE 11/16/04
		SHEET NO. 20

ABB-1 (M)
4-30-99

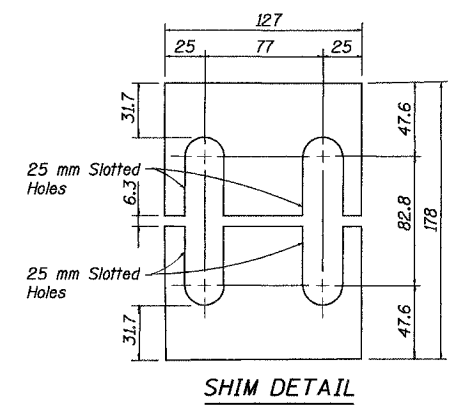
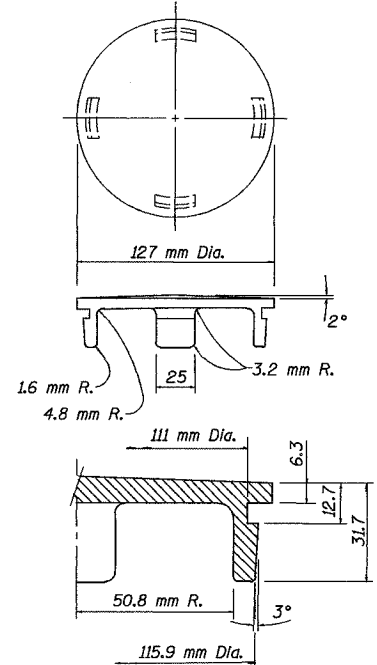
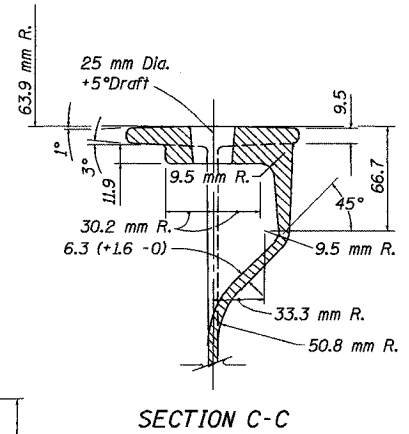
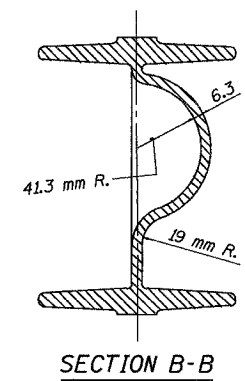
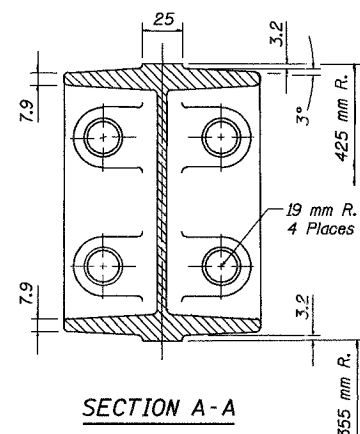
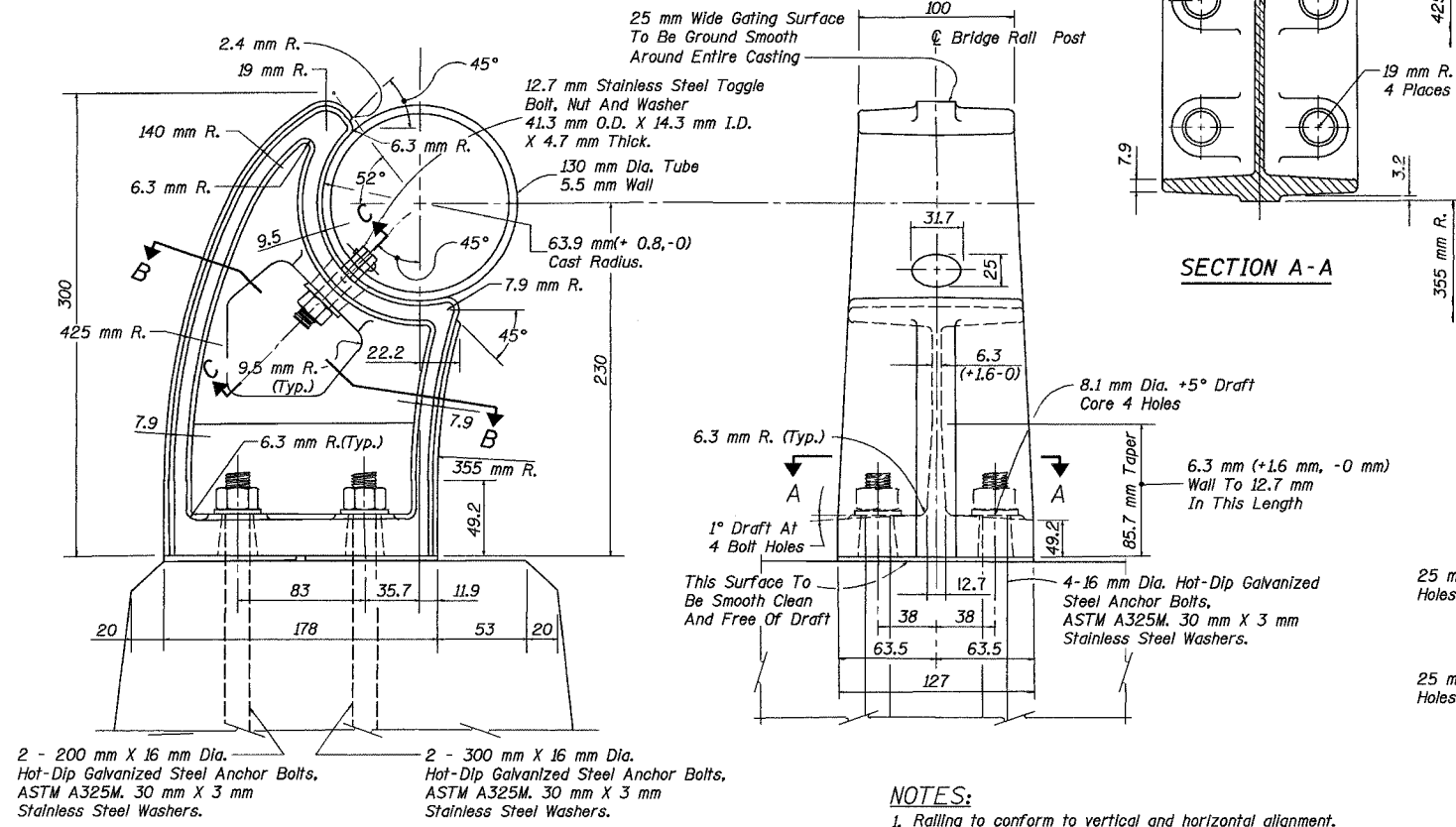
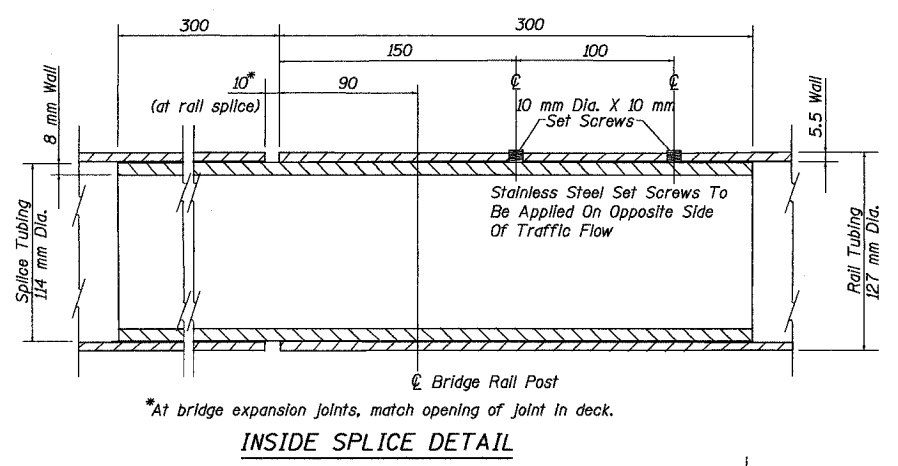
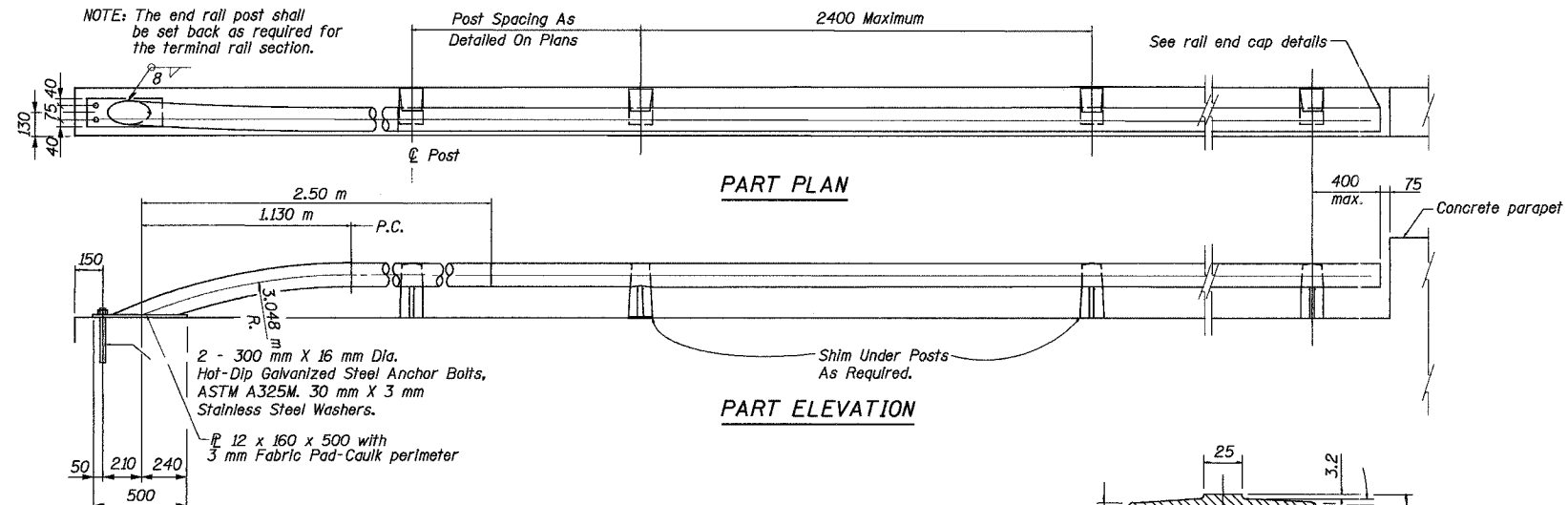
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ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	440	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200



BILL OF MATERIAL

Item	Unit	Quantity
Aluminum Railing, Type H (Special)	m	68.0

TYPE H ALUMINUM RAILING

- NOTES:**
- Railing to conform to vertical and horizontal alignment.
 - Joint to be placed 7500 mm center to center, max.
 - Slip joint to be placed in panels to match expansion joints in deck.
 - Design weight: 9.3 kg. per meter.
 - Unless otherwise specified all draft to be 3°.
 - All unmarked Radius to be 3 mm R.
 - After fabrication, exposed surfaces of aluminum shall be given an anodic oxide coating, dyed black, conforming to the requirements of ASTM designation: B 580, Type B, Architectural Class I.

RAILING DETAILS

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
RAILING DETAILS - SINGLE RAIL		
RAMP A-3 OVER RAMP B-5 F.A.I. ROUTE 74 (SECTION 72-7)R-3 PEORIA COUNTY STA. 10+418.515 (RAMP A-3) STRUCTURE NUMBER 072-0172		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. RL-1	SCALE N.T.S.	DATE 11/16/04
		SHEET NO. 21

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ROUTE NO. SECT. COUNTY SHEET NO. TOTAL SHEETS. F.A.I. 74 (72-7) R-3 PEORIA 441 1365. FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT. CONTRACT NO. 68200

CLAUDE H. HURLEY COMPANY BORING LOG BORING NO. SB-34. PROJECT 3-380-D5 FAI-74 IMPROVEMENTS - WAR MEMORIAL DRIVE CORRIDOR. LOCATION BRIDGE NO. 072-0172 RAMP A-3BL 10+447.7 1.3mL. DRILLING CONTRACTOR CENTRAL ILLINOIS DRILLING COMPANY. DATE OF DRILLING: STARTED 2-28-96 COMPLETED 2-29-96 SURFACE ELEVATION 190.25. DRILLED BY S. SCOTT LOGGED BY S. ARENDS. Includes soil classification and groundwater data tables.

CLAUDE H. HURLEY COMPANY BORING LOG BORING NO. SB-34 (CONT.). PROJECT 3-380-D5 FAI-74 IMPROVEMENTS - WAR MEMORIAL DRIVE CORRIDOR. LOCATION BRIDGE NO. 072-0172 RAMP A-3BL 10+447.7 1.3mL. DRILLING CONTRACTOR CENTRAL ILLINOIS DRILLING COMPANY. DATE OF DRILLING: STARTED 2-28-96 COMPLETED 2-29-96 SURFACE ELEVATION 190.25. DRILLED BY S. SCOTT LOGGED BY S. ARENDS. Includes soil classification and groundwater data tables.

CLAUDE H. HURLEY COMPANY BORING LOG BORING NO. SB-35. PROJECT 3-380-D5 FAI-74 IMPROVEMENTS - WAR MEMORIAL DRIVE CORRIDOR. LOCATION BRIDGE NO. 072-0172 RAMP A-3BL 10+414.1 9.4mR. DRILLING CONTRACTOR CENTRAL ILLINOIS DRILLING COMPANY. DATE OF DRILLING: STARTED 2-28-96 COMPLETED 2-29-96 SURFACE ELEVATION 192.55. DRILLED BY S. SCOTT LOGGED BY S. ARENDS. Includes soil classification and groundwater data tables.

LEGEND - CLAUDE H. HURLEY COMPANY TEST BORING LOGS

A-1 to A-8 (and subgroups) Engineering classifications of soil in accordance with AASHTO M 145 standard specification. Silty Clay Loam Textural classification of soil in accordance with IDOT Triangular Chart. Laminated Coal Shale Textural and engineering classification of bedrock in accordance with conventional practice. N,Bp,0.15m N-value or standard penetration test value. Number of blows required to drive a standard split-spoon sampler 0.15 m as conducted in accordance with AASHTO T 206 standard specification.

Q, kPa Unconfined compression strength of soil in kilopascals determined in accordance with AASHTO T 208 standard specification. w, % Natural moisture content of soil and bedrock in percent determined in accordance with AASHTO T 265 standard specification and AASHTO T 265/ASTM D 2216 for bedrock. d, kgpm³ Dry unit weight of soil and bedrock in kilograms per cubic meter determined in accordance with standard practice.

GROUNDWATER DATA DD Water Level During Drilling BAR Water Level Before Auger Removal AAR Water Level After Auger Removal DC Dry Cave Level WC Wet Cave Level h Hours. DRILLING METHOD FA Flight Auger RW Rotary Wash HSA Hollow Stem Auger. SAMPLE TYPE AU Auger SS Standard Split-barrel ST Thin-walled Tube DB Core Barrel.

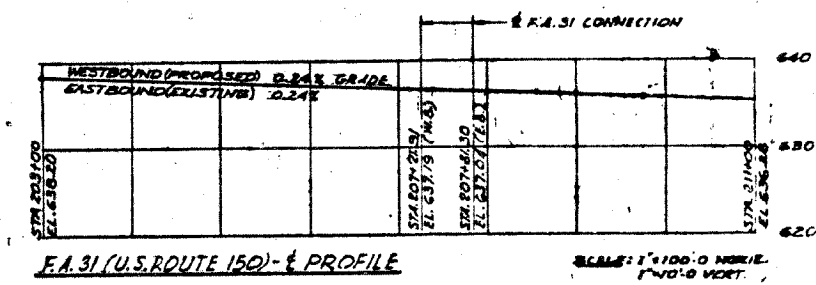
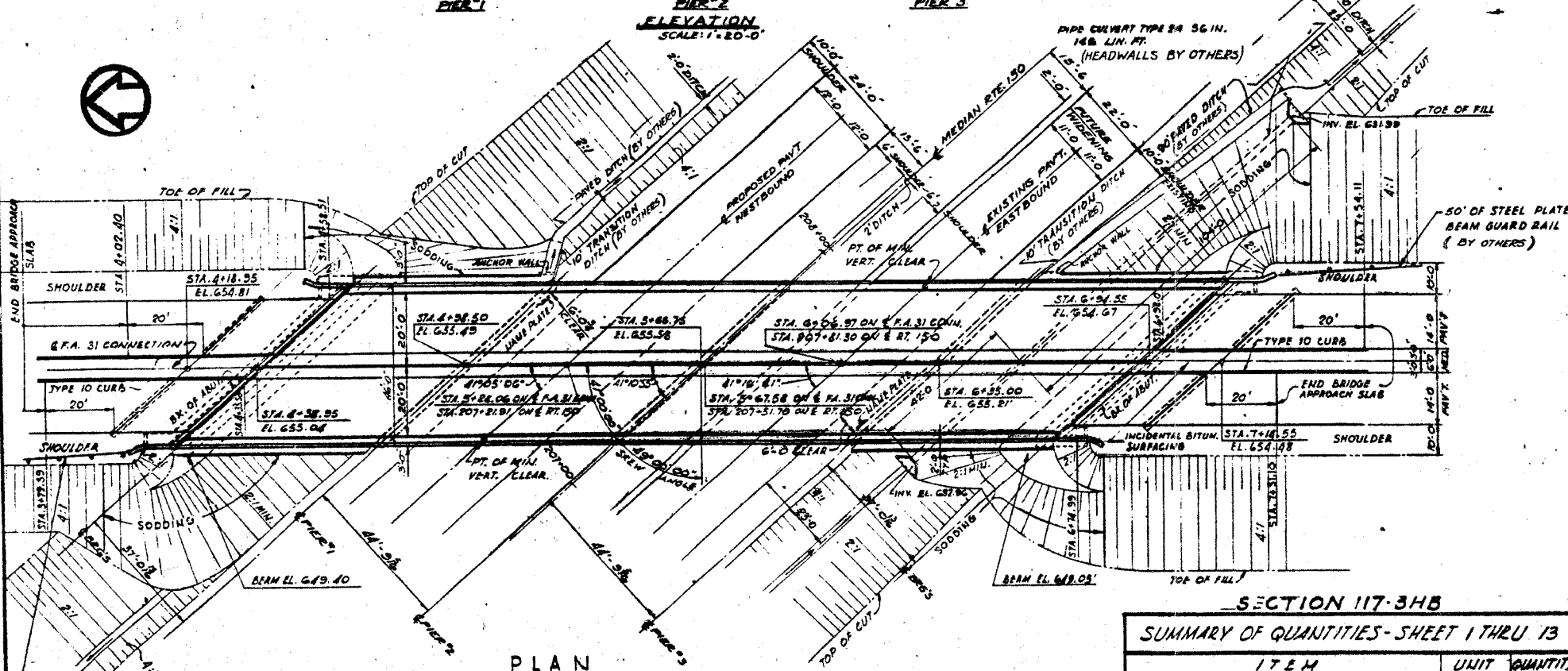
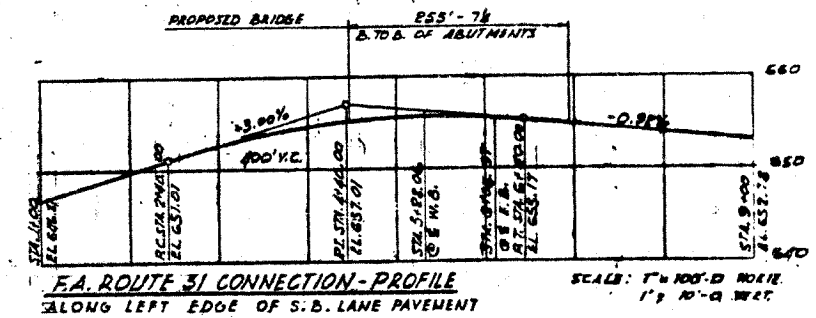
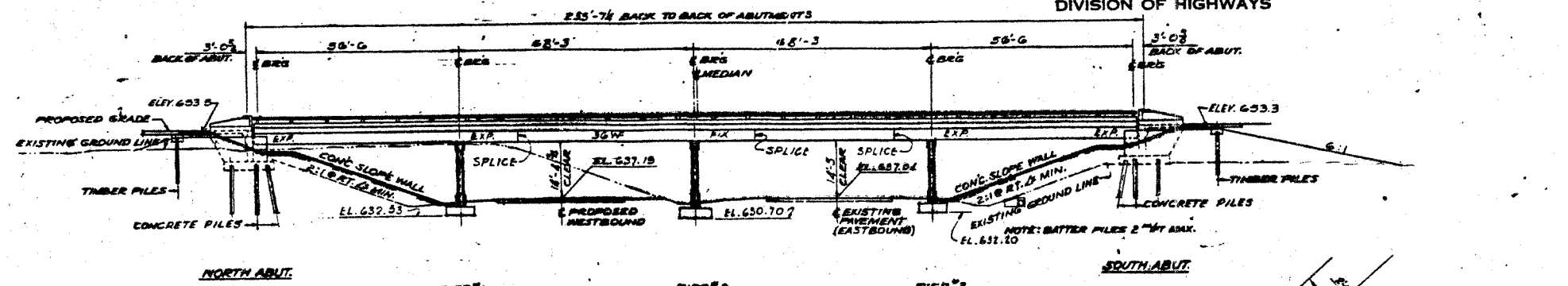
NOTES 1. The abbreviations, symbols and definitions in this Legend are commonly used and understood in the engineering and construction practices and are presented only for information and communication. 2. The Geotechnical Data presented in this Legend and on the Boring Logs are to be interpreted by personnel educated, trained, experienced and licensed to practice Geotechnical Engineering, and in direct communication with the Claude H. Hurley Company.

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION BORING LOGS I. RAMP A-3 OVER RAMP B-5 F.A.I. ROUTE 74 (SECTION 72-7)R-3 PEORIA COUNTY STA. 10+418.515 (RAMP A-3) STRUCTURE NUMBER 072-0172. PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS. DRAWING NO. BL-1 SCALE N.T.S. DATE 11/16/04 SHEET NO. 22

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	DIS.	COUNTY	POST MILE	SHEET NO.
FA 31	117-3HB	PEORIA	29	15
PROJ. ROAD DEPT. NO. 17	ILLINOIS	PROJECT	U-107(12)	

1360 443



- GENERAL NOTES:**
1. ALL CONCRETE SHALL BE CLASS A.
 2. BRIDGE DESIGNED FOR FUTURE TRAFFIC WEARING SURFACE @ 20 P.S.F.
 3. THE CONCRETE FLOOR SLAB SHALL BE FINISHED IN ACCORDANCE WITH ARTICLE 51.13 OF THE STANDARD SPECIFICATIONS. THE CURBS AND SLAB OUTSIDE OF LONGITUDINAL CONSTRUCTION JOINTS SHOWN ON CROSS SECTION SHALL BE POURED INDIVIDUALLY.
 4. NO TRANSVERSE CONSTRUCTION JOINTS SHALL BE MADE EXCEPT BY WRITTEN PERMISSION OF THE ENGINEER.
 5. THE ROADWAY EXPANSION PLATE SHALL BE FABRICATED AND ERECTED IN ACCORDANCE WITH ART. 51.13(d) OF THE STANDARD SPECIFICATIONS AND SHALL FIT THE CROWN OF THE ROADWAY.
 6. ALL ROLLERS, ROCKERS, BEARING PLATES, LEAD PLATES, PINTLES, BOLT PLATES AND ANKOR BOLTS SHALL BE FABRICATED AND SET IN ACCORDANCE WITH ART. 51.15 OF THE STANDARD SPECIFICATIONS AND ARE INCLUDED IN WEIGHT OF STRUCTURAL STEEL. ANCHOR BOLTS SHALL BE SET BEFORE BOLTING DIAPHRAGMS OVER SUPPORTS.
 7. EXPANSION JOINTS ARE INCLUDED IN WEIGHT OF STRUCTURAL STEEL.
 8. EXCEPT AS OTHERWISE PROVIDED ALL STRUCTURAL STEEL SHALL RECEIVE ONE SHOP COAT OF RED LEAD PAINT AND TWO FIELD COATS OF ALUMINUM PAINT. SEE ARTICLE 56.1 TO 56.5 INCLUSIVE OF THE STANDARD SPECIFICATIONS.
 9. STRUCTURE DESIGNED IN ACCORDANCE WITH AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES 1957 EDITION.
 10. THE CONTRACTOR SHALL DRIVE TEST PILES IN A PERMANENT LOCATION, AS SHOWN ON PLANS, BEFORE ORDERING THE REMAINDER OF PILES.
 11. REINFORCING BARS SHALL BE LAPPED A MINIMUM OF 20 BAR DIAMETERS UNLESS OTHERWISE NOTED.
 12. LIVE LOADING: HS-20-SIG-44 AND ALTERNATE.
 13. COARSE AGGREGATE WHICH IS TO BE USED IN PARAPET HANDRAIL AND END POSTS MUST BE ABSOLUTELY FREE FROM GHELT, FLINT, LIMONITE AND SOFT SANDSTONE.
 14. WHEREVER IN THESE PLANS REFERENCE IS MADE TO THE "STANDARD SPECIFICATIONS" IT IS UNDERSTOOD TO INCLUDE THE "SUPPLEMENTAL SPECIFICATION" EFFECTIVE JULY 1, 1961.

SECTION 117-3HB
SUMMARY OF QUANTITIES - SHEET 1 THRU 13

ITEM	UNIT	QUANTITY
CLASS A EXCAVATION FOR STRUCTURES	CU. YDS.	439
ERECTING STRUCTURAL STEEL	LBS.	356,800
ALUMINUM HANDRAIL	LIN. FT.	505
PIPE CULVERT TYPE 24 36 IN.	LIN. FT.	146
REINFORCEMENT BARS	LBS.	187,500
FURNISHING CROSOATED PILES UP TO 20'	LIN. FT.	300
DRIVING TIMBER PILES	LIN. FT.	300
FURNISHING CONCRETE PILES	LIN. FT.	1,150
DRIVING CONCRETE PILES	LIN. FT.	1,150

BRIDGE APPROACH SLABS - STD. 1909-3 METHOD II (BY OTHERS)

STA. TO STA.	P.C.C. PAVEMENT (10'-10" - 10'-10")	REINF. BARS
RT. 4+18.95 TO 4+58.51	75.2 SQ. YDS	2353 LBS
LT. 4+18.95 TO 4+58.51	75.2 SQ. YDS	2353 LBS
RT. 7+14.55 TO 7+54.11	75.2 SQ. YDS	2353 LBS
LT. 7+14.55 TO 7+54.11	75.2 SQ. YDS	2353 LBS
TOTALS:	300.8 SQ. YDS	9412 LBS

R.C. PILE CAP (BY OTHERS)

STATION	CLASS X CONC.	REINF. BARS
RT. 4+18.95	1.51 CU. YDS	516 LBS
LT. 4+18.95	1.51 CU. YDS	516 LBS
RT. 7+14.55	1.51 CU. YDS	516 LBS
LT. 7+14.55	1.51 CU. YDS	516 LBS
TOTALS	6.04 CU. YDS	2064 LBS

DESIGNED G. BEYKOVICH & K.M.
CHECKED K. MATSEVICH
DRAWN G. BEYKOVICH & K.M.
CHECKED T. WADA

EXAMINED
PASSED
APPROVED
CHIEF HIGHWAY ENGINEER

ITEM	UNIT	QUANTITY
SODDING	SQ. YDS	700
TEST PILES (CONCRETE)	EACH	2
NAME PLATES	EACH	8
SLOPE WALL (4')	SQ. YDS	300
SUPPLEMENTAL WATERING	UNIT	3.0
CLASS X CONCRETE	CU. YDS	810.7

DESIGN STRESSES:
 15,000 P.S.I. - STRUCTURAL STEEL
 30,000 P.S.I. - REINFORCING STEEL
 4,000 P.S.I. - CONCRETE
 90 P.S.I. - SUPERSTRUCTURE
 75 P.S.I. - FOOTINGS
 R. 10

SECTION 117-3HF
FURNISHING STRUCTURAL STEEL 35,856,800

JOHN F. MEISSNER ENGINEERS
CHICAGO, ILLINOIS
DWG. NO. 56-197-6620

GENERAL PLAN & ELEVATION
F.A. ROUTE 31 SECTION 117(3HB) 3HF
PROJECT U-107 (12)
F.A. 31 CONNECTION - GRADE SEPARATION
PEORIA COUNTY - STATION 207+51.70

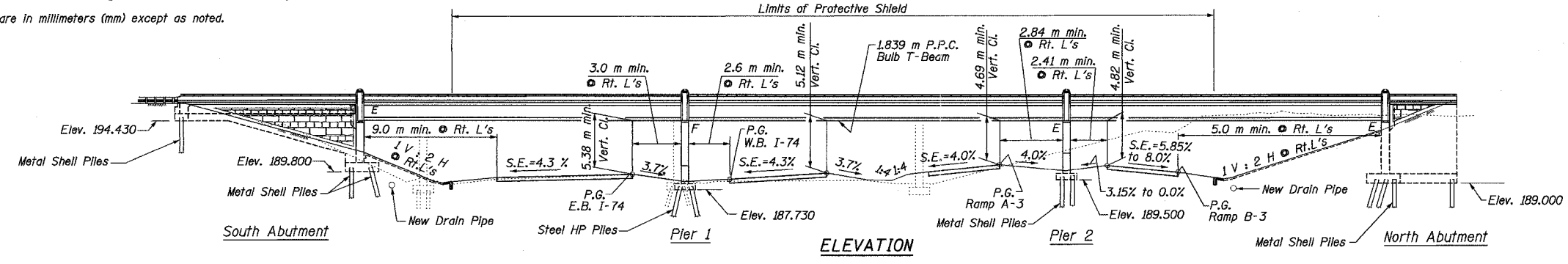
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ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	444	1360
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

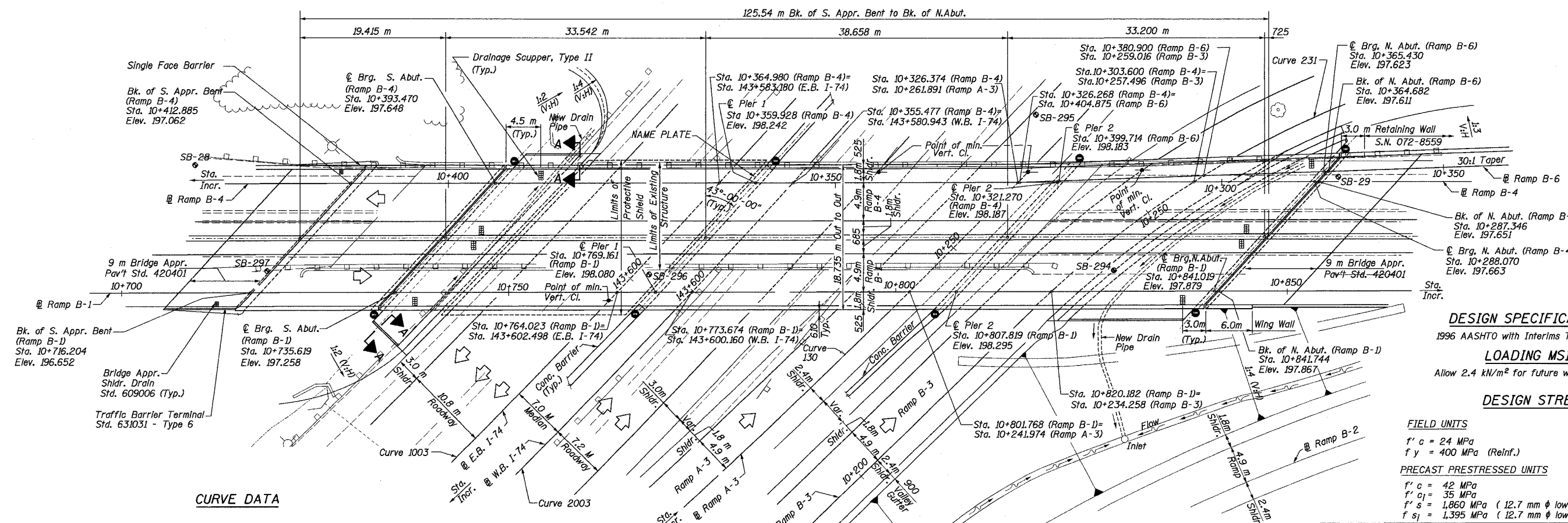
CONTRACT NO. 68200

B. M. TP 1316 : PK Nail on South Side of FA 49 (U.S. 150 Spur) Bridge over I-74 West of I-74 on ramp. Elev. 194.293m
 Existing Structure : S.N. 072-0027 is a four span bridge built as F.A.I. RT. 74, SEC. (72-7HB-D) in 1963.
 The Superstructure consists of a R.C. Deck 86.71 m long by 14 m wide supported on wide flange steel stringers.
 The existing Structure shall be removed completely. Traffic along ramps B-1, B-4 and B-6 will be detoured while traffic along I-74 will be maintained during construction.

Note : All dimensions are in millimeters (mm) except as noted.



SEISMIC DATA
 Seismic Performance Category (SPC) = A
 Bedrock Acceleration Coefficient (A) = 0.043g
 Site Coefficient (S) = 1.0



DESIGN SPECIFICATIONS
 1996 AASHTO with Interims Through 2000.
LOADING MS18
 Allow 2.4 kN/m² for future wearing surface.

DESIGN STRESSES
FIELD UNITS
 f'c = 24 MPa
 fy = 400 MPa (Reinf.)
PRECAST PRESTRESSED UNITS
 f'c = 42 MPa
 f'ci = 35 MPa
 f's = 1,860 MPa (12.7 mm ϕ low lax. strands)
 f'sj = 1,395 MPa (12.7 mm ϕ low lax. strands)

CURVE DATA

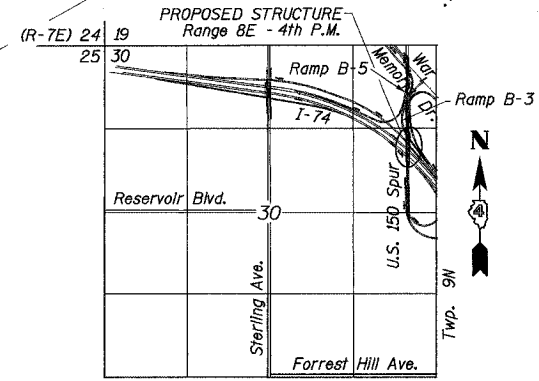
E.B. I-74	W.B. I-74	RAMP A-3	RAMP B-3
Curve 1003	Curve 2003	Curve 130	Curve 231
$\Delta = 100^\circ 30' 47''$	$\Delta = 100^\circ 30' 47''$	$\Delta = 15^\circ 21' 33''$	$\Delta = 47^\circ 56' 01''$
R = 1165.0 m	R = 1172.0 m	R = 1182.822 m	R = 125.0 m
T = 1401.087 m	T = 1409.506 m	T = 159.496 m	T = 55.567 m
L = 2043.743 m	L = 2056.022 m	L = 317.079 m	L = 104.575 m
E = 657.161 m	E = 661.110 m	E = 10.705 m	E = 11.794 m
PC = 142+877.830	PC = 142+877.801	PC = 10+000.000	PC = 10+231.559
PI = 144+278.917	PI = 144+287.307	PI = 10+159.496	PI = 10+287.126
PT = 144+921.573	PT = 144+933.823	PT = 10+317.079	PT = 10+336.134
SE = +4.3 %	SE = +4.3 %	SE = -4.0 %	SE = 8.0 %

Transition in 10+196 to 10+249
 Transition out 10+319 to 10+372

PLAN
 Scale 1 : 250

STATION 10+779.03
 BUILT 200... BY
 STATE OF ILLINOIS
 F.A.I. ROUTE 74 SECTION (72-7) R-3
 LOADING MS-18 & ALT.
 STR. NO. 072-0183

NAME PLATE
 See Std. 515001



LOCATION SKETCH

Designed by:	DL
Checked by:	AK
Drafted by:	JMG
Checked by:	AK

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
GENERAL PLAN		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3		
F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO.	SCALE	DATE
1	N.T.S.	2-18-03
		SHEET NO.
		1

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	445	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

GENERAL NOTES

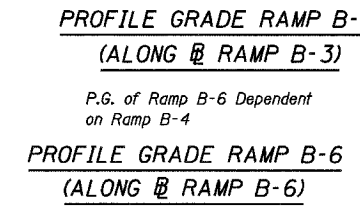
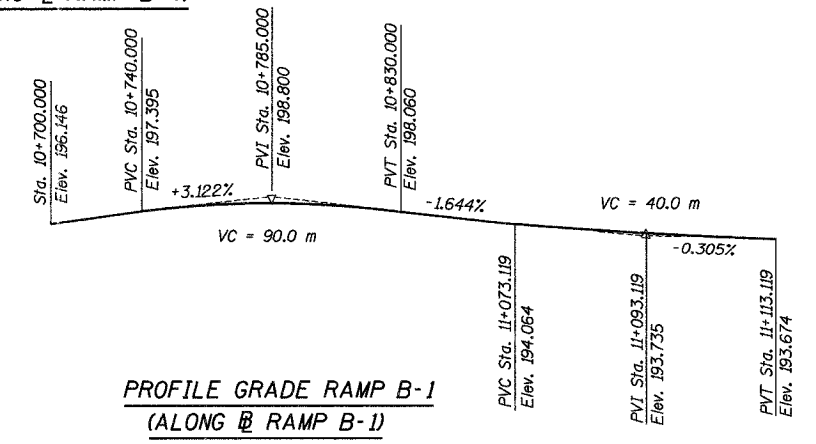
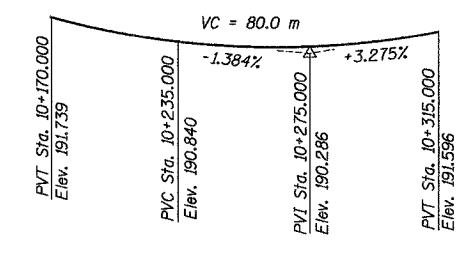
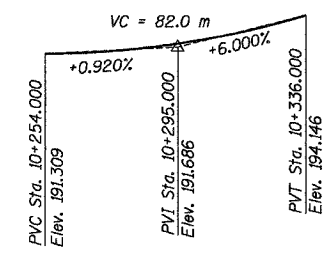
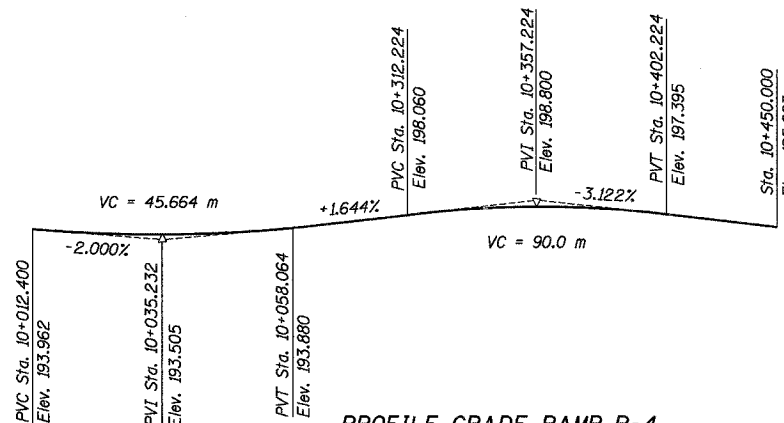
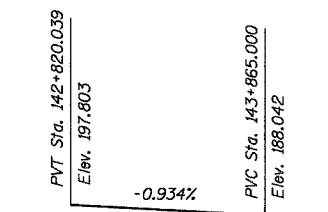
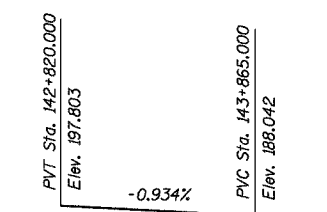
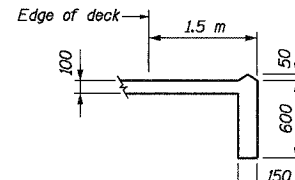
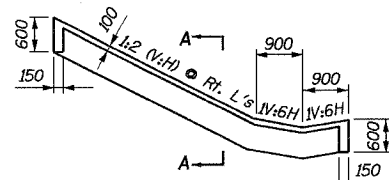
- Side retainers shall be galvanized after shop fabrication according to AASHTO M111 and ASTM A385.
- The structural steel bearing plates of the Elastomeric Bearing Assembly shall conform to the requirements of AASHTO M270M Grade 345.
- Reinforcement bars shall conform to the requirements of AASHTO M 31M, M 42M or M 53M Grade 400.
- Slope wall shall be reinforced with welded wire fabric, 152 x 152 - MW25.8 x MW25.8 with a mass of 2.91 kg/m².
- The back face of Closed Abutments and their wingwalls shall be waterproofed according to Article 503.18 of the Standard Specifications.
- The contractor shall drive four (4) metal shell test piles in permanent locations, two (2) at South Abutment, one (1) at North Abutment and one (1) HP 310x79 test pile in a permanent location at Pier 1, as directed by the Engineer before ordering the remainder of piles.
- Bridge Seat Sealer shall be applied to the seat area of the Abutments
- All dimensions are in millimeters (mm) except as noted.
- The existing structural steel coating contains lead. The contractor should take appropriate precautions to deal with the presence of lead on this project.
- All existing aluminum bridge rail and rail bases are to be salvaged and delivered by the Contractor to the IDOT Bridge Maintenance Yard in East Peoria. The contact person is Dan Edwards at (309) 699-3823. The Contractor shall provide 24 hour notice in advance of delivery. All deliveries shall be Monday through Friday. The Contractor shall replace any rail that is damaged during removal.

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Removal of Existing Structures #1	Each			1
Elastomeric Bearing Assembly, Type I	Each	24		24
Elastomeric Bearing Assembly Type II	Each	8		8
Name Plates	Each		1	1
Porous Granular Embankment	m ³	522		522
Structure Excavation	m ³		2782	2782
Neoprene Expansion Joint 50 mm	m	24.7		24.7
Neoprene Expansion Joint 100 mm	m	26.4		26.4
Concrete Structures	m ³		1215	1215
Concrete Superstructure	m ³	729.8		729.8
Bridge Deck Grooving	m ²	2151		2151
Protective Coat	m ²	2722		2722
Furnishing And Erecting Precast Prestressed Concrete I-Beams 1067 mm	m	161.5		161.5
Furnishing And Erecting Precast Prestressed Concrete Bulb T-Beams 1829 mm	m	846.5		846.5
Furnishing and Erecting Structural Steel	kg		2820	2820
Reinforcement Bars, Epoxy Coated	kg	108000	70860	178860
Slope Wall 100 mm	m ²		465	465
Furnishing Steel Piles HP310x79	m		663	663
Driving Steel Piles	m		663	663
Test Pile Steel HP310x79	Each		1	1
Furnishing Metal Pile Shells 305mm	m		566	566
Furnishing Metal Pile Shells 356mm	m		1266	1266
Driving And Filling Shells	m		1832	1832
Test Pile Metal Shells	Each		4	4
Bridge Seat Sealer	m ²		58	58
Protective Shield	m ²	466		466
Drainage Scouppers, Type II	Each		8	8
Bar Splicers	Each		88	88
Aluminum Railing, Type H (Special)	m	257		257
Form Liner Limestone Surface	m ²		150	150
Form Liner Grid and Fin Surface	m ²		194	194
Install Bollards and Luminares	Each		8	8
Form Liner Parapet Surface	m ²	140		140

INDEX OF DRAWINGS

- General Plan
- General Notes, Total Bill of Material, Misc. Details
- Substructure Layout
- Plan For Top of Slab Elevations
- Top of Slab Elevations - Span 1
- Top of Slab Elevations - Spans 2 to 4
- Top of Slab Elevations - Spans 2 to 4
- Deck Plan and Cross Section - Span 1
- Parapet and Median Details - Span 1
- Diaphragm Details - Span 1
- Deck Plan and Cross Section - Spans 2 & 3
- Deck Plan and Cross Section - Span 4
- Diaphragm Details - Spans 2, 3 & 4
- Diaphragm Details - Pier 1
- Parapet Elevations and Details
- Median Barrier Elevation and Details
- Framing Plan and Temporary Bracing Details
- Beam Reaction and Moment Table
- 1067 mm PPC I-Beam - South Vaulted Beam Nos. 1-9 Span 1
- 1829 mm PPC Bulb T-Beam - Beam Nos. 10-17 Span 2
- 1829 mm PPC Bulb T-Beam - Flared Beam No. 10 Span 3
- 1829 mm PPC Bulb T-Beam - Beam Nos. 11-17 Span 3
- 1829 mm PPC Bulb T-Beam - Flared Beam No. 10 Span 4
- 1829 mm PPC Bulb T-Beam - Flared Beam No. 11 Span 4
- 1829 mm PPC Bulb T-Beam - Flared Beam No. 12 Span 4
- 1829 mm PPC Bulb T-Beam - Beam Nos. 13-17 Span 4
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- Anchor Bolt Details
- Bar Splicer Assembly Details
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- Bollard Luminaire
- Boring Logs I
- Boring Logs II
- Boring Logs III
- Boring Logs IV



P.G. of Ramp B-6 Dependent on Ramp B-4

Designed by: DL
 Checked by: AK
 Drafted by: JV
 Checked by: AK

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
GENERAL NOTES, TOTAL BILL OF MATERIAL, MISC. DETAILS		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3		
F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO.	SCALE	DATE
2	N.T.S.	2-18-03
SHEET NO.		
2		

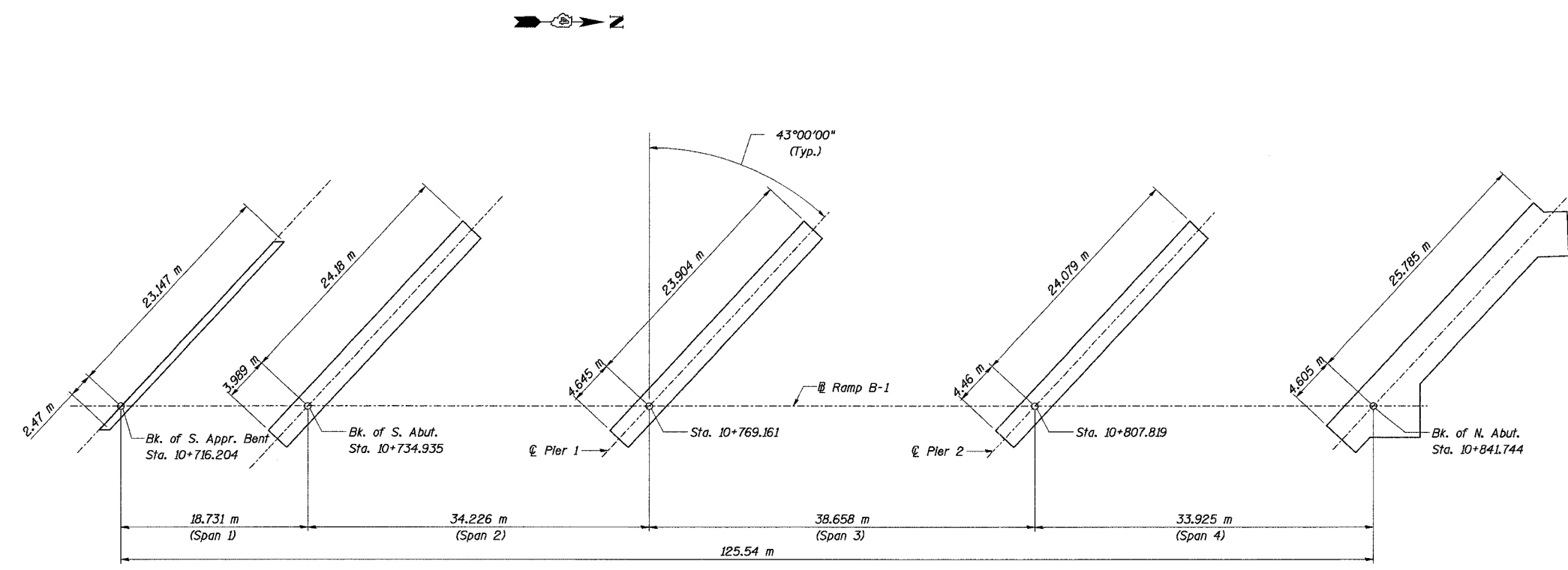
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ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	440	1360
FED. ROAD DIST. NO. 7	ILLINOIS		FED. AID PROJECT	

CONTRACT NO. 68200

Time: 09:36:37 AM

Date: 11/19/2004



FOOTING LAYOUT PLAN

File name: P:\643996\structural\072-0183\Sheet\Tracings\FP0001-A0720183.dgn

Designed by: DL
 Checked by: AK
 Drafted by: RK-S
 Checked by: AK

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
SUBSTRUCTURE LAYOUT		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 3	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 3

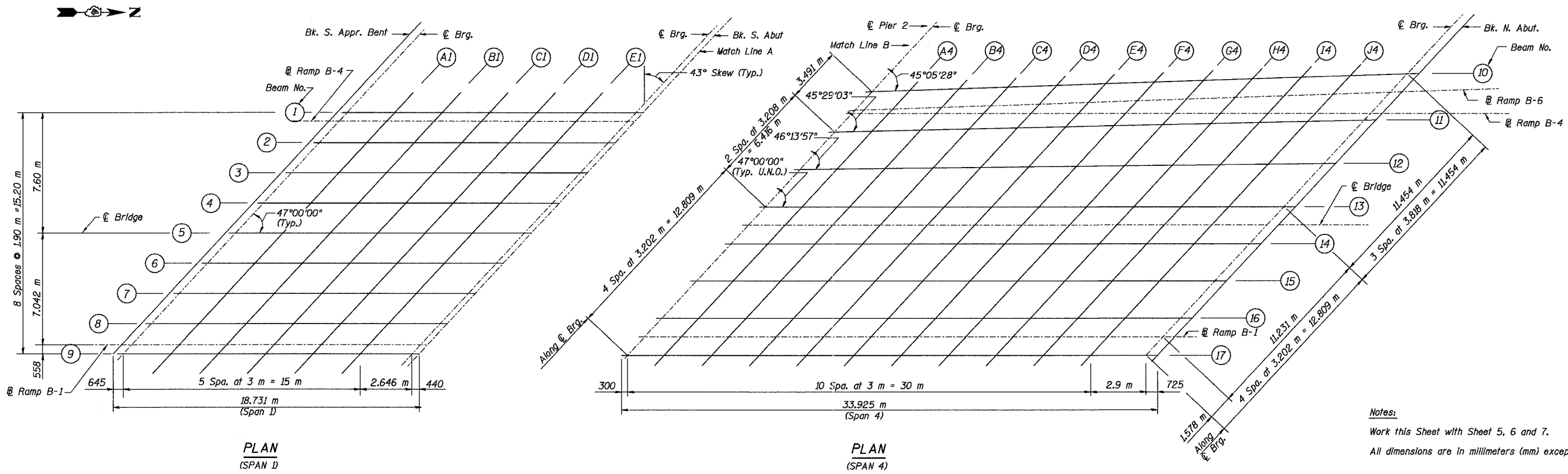
ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	447	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200

Time: 09:36:59 AM

Date: 11/19/2004

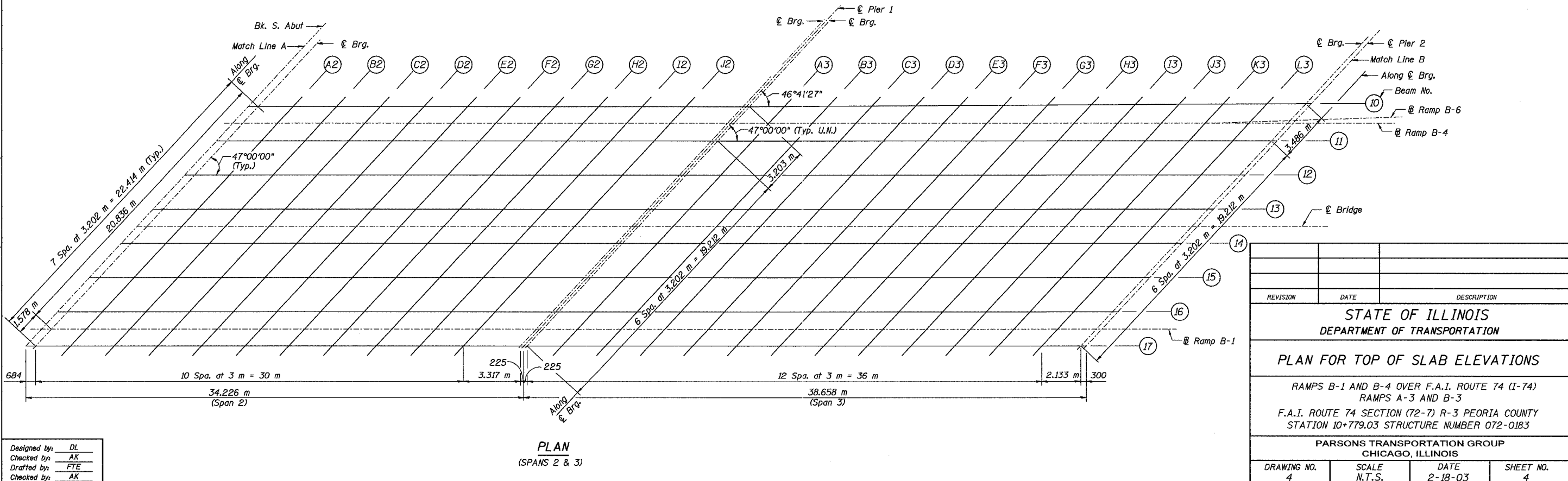
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PLAN
(SPAN 1)

PLAN
(SPAN 4)

Notes:
Work this Sheet with Sheet 5, 6 and 7.
All dimensions are in millimeters (mm) except as noted.



PLAN
(SPANS 2 & 3)

REVISION	DATE	DESCRIPTION

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN FOR TOP OF SLAB ELEVATIONS

RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74)
RAMPS A-3 AND B-3
F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY
STATION 10+779.03 STRUCTURE NUMBER 072-0183

PARSONS TRANSPORTATION GROUP
CHICAGO, ILLINOIS

DRAWING NO.	SCALE	DATE	SHEET NO.
4	N.T.S.	2-18-03	4

Designed by: DL
Checked by: AK
Drafted by: FTE
Checked by: AK

BEAM 10

Table with 5 columns: Location, Station, Offset (m), Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk. S. Abut., various bridge spans (A2 to J4), and Bk. N. Abut.

BEAM 11

Table with 5 columns: Location, Station, Offset (m), Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk. S. Abut., various bridge spans (A2 to J4), and Bk. N. Abut.

BEAM 12

Table with 5 columns: Location, Station, Offset (m), Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk. S. Abut., various bridge spans (A2 to J4), and Bk. N. Abut.

Project information table with columns: ROUTE NO., SECT., COUNTY, SHEET NO., TOTAL SHEETS, FED. ROAD DIST. NO., ILLINOIS, FED. AID PROJECT, CONTRACT NO. 68200.

BEAM 13

Table with 5 columns: Location, Station, Offset (m), Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk. S. Abut., various bridge spans (A2 to J4), and Bk. N. Abut.

BEAM 14

Table with 5 columns: Location, Station, Offset (m), Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk. S. Abut., various bridge spans (A2 to J4), and Bk. N. Abut.

Notes: Work this Sheet with Sheet 4. All elevations and offsets are in meters. All elevations are at top of slab. All stations are based on @ Ramp B-1.

Revision table with columns: REVISION, DATE, DESCRIPTION. Below is project title block: STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION TOP OF SLAB ELEVATIONS - SPANS 2 TO 4 RAMP B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMP A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183 PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS DRAWING NO. 6 SCALE N.T.S. DATE 2-18-03 SHEET NO. 6

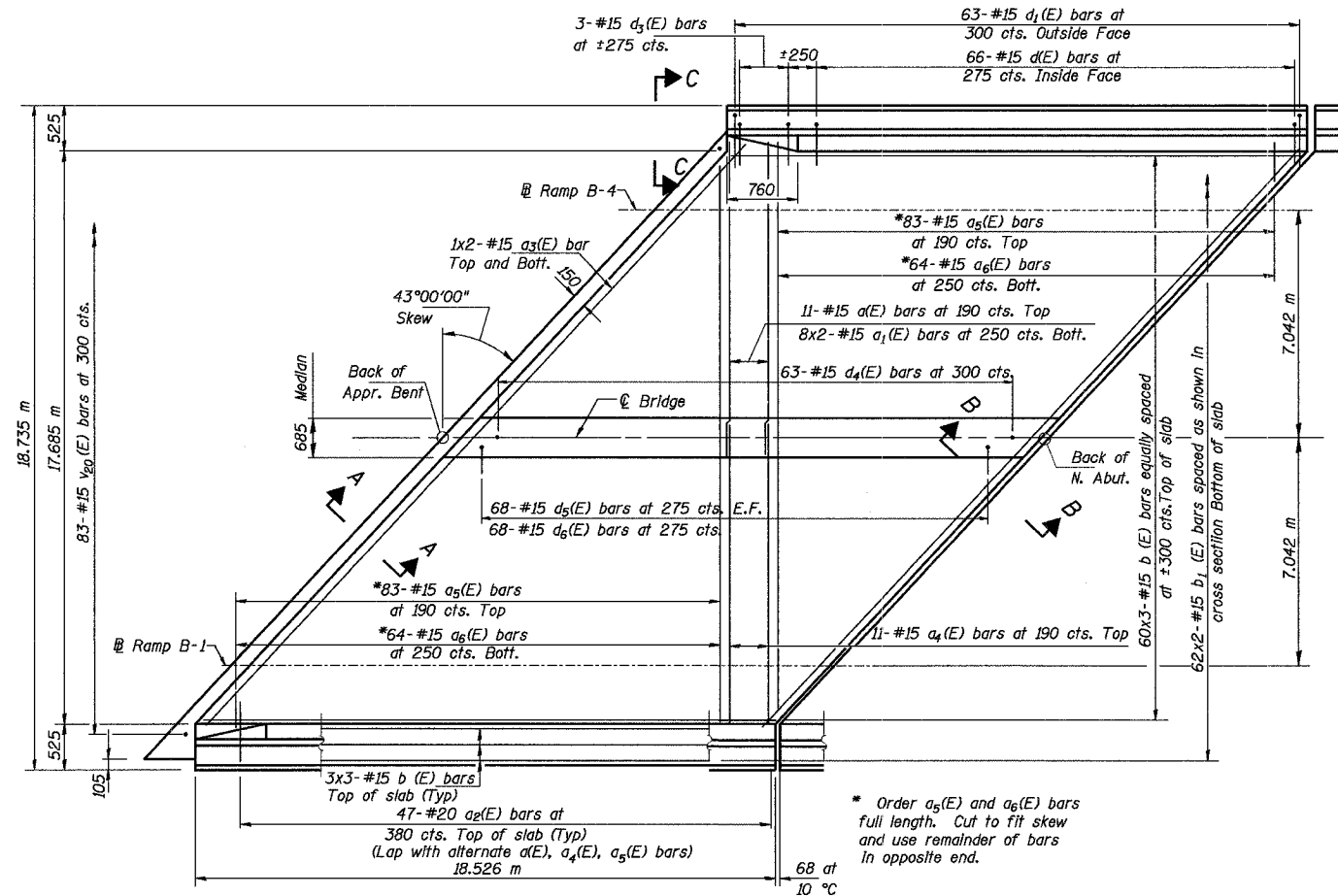
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Designed by: DL Checked by: AK Drafted by: FTE Checked by: AK

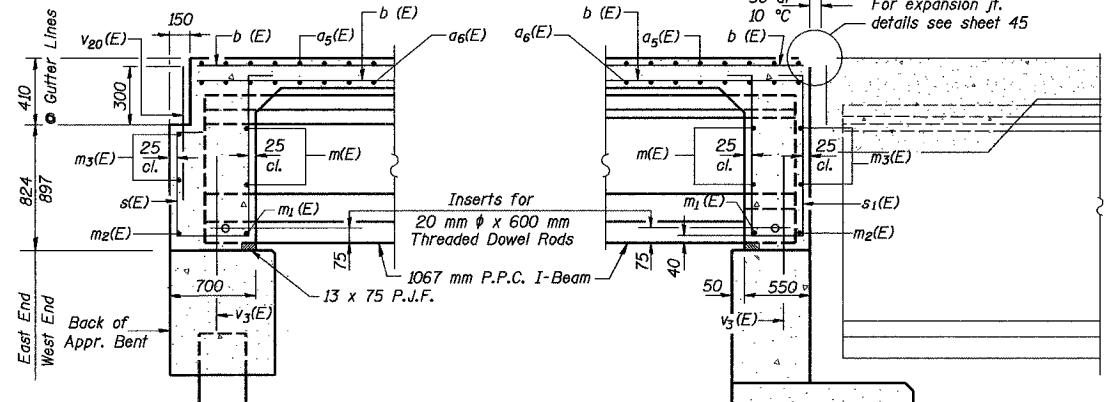
ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	45	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200

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Date: 11/19/2004



PLAN
(Parapet & Median Barrier Joints not shown for clarity)



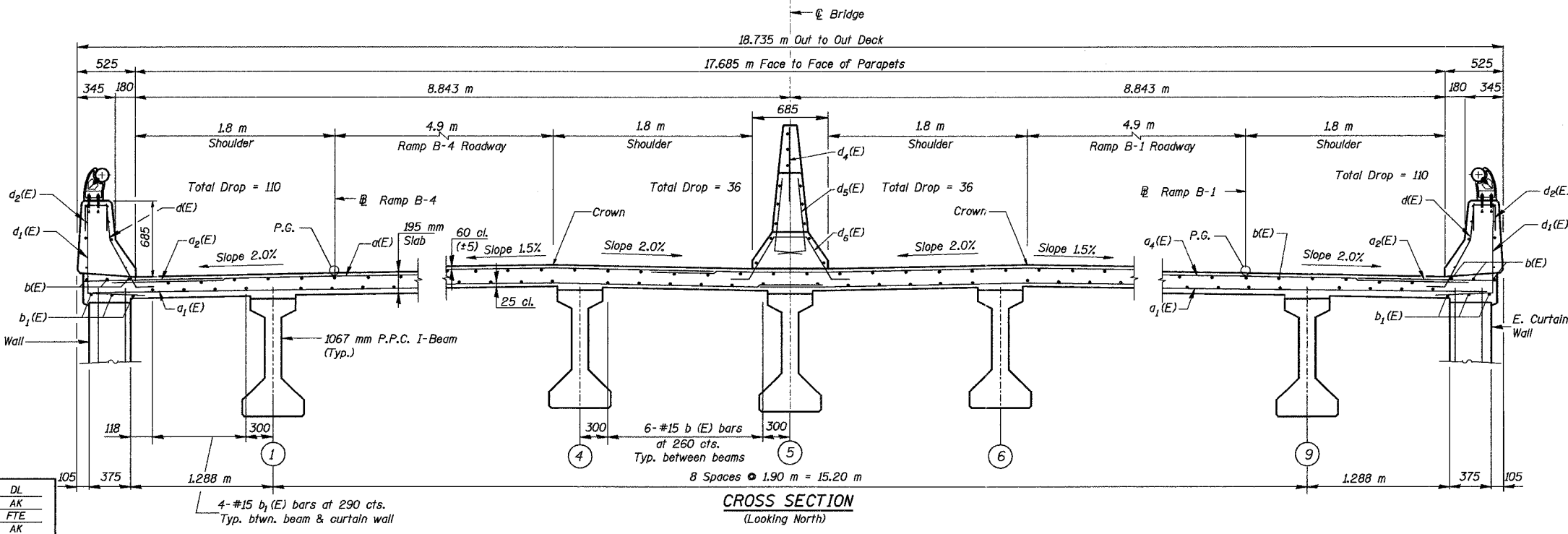
SECTION A-A

SECTION B-B

MIN. BAR. LAP
#15 bar = 640

- Notes:**
 See Sheet 10 for superstructure details and Bill of Material.
 Reinforcement bars designated (E) shall be epoxy coated.
 Bars indicated thus 1x2-#15 etc. indicates 1 line of bars with 2 lengths per line.
 See Sheet 9 for parapet, median reinforcement and view C-C.
 All dimensions are in millimeters (mm) except as noted.
 E.F. = Each Face
 See Sheet 29 for v3(E) bars.

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
DECK PLAN AND CROSS SECTION - SPAN 1		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 8	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 8



CROSS SECTION
(Looking North)

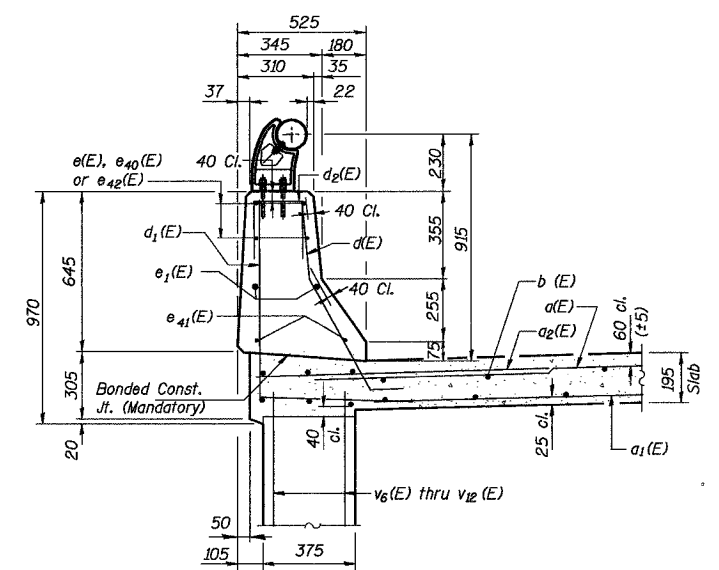
Designed by: DL
Checked by: AK
Drafted by: FTE
Checked by: AK

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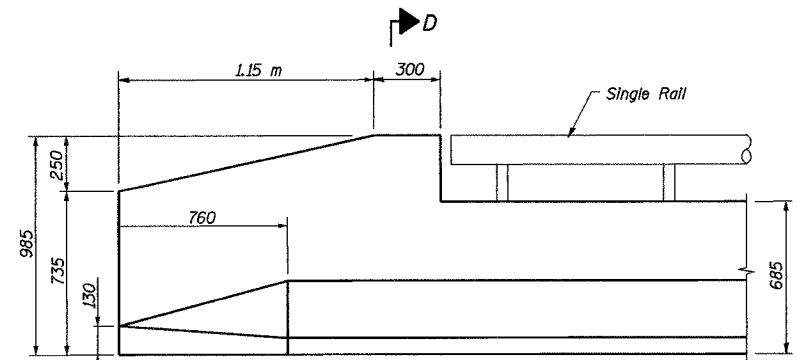
ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	452	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200

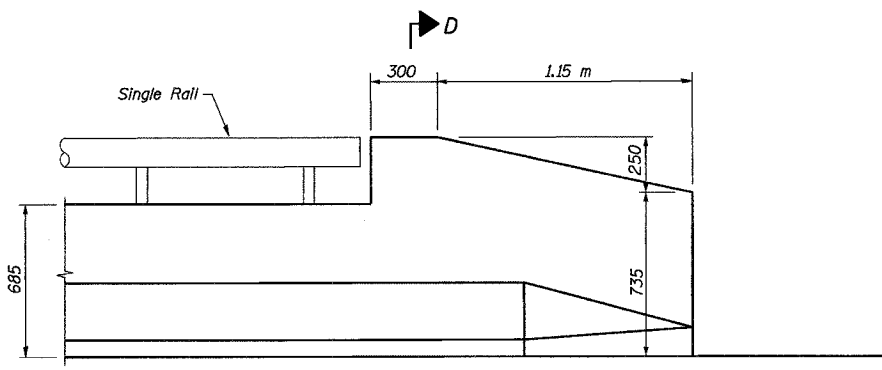
418 Rail Post Spacing



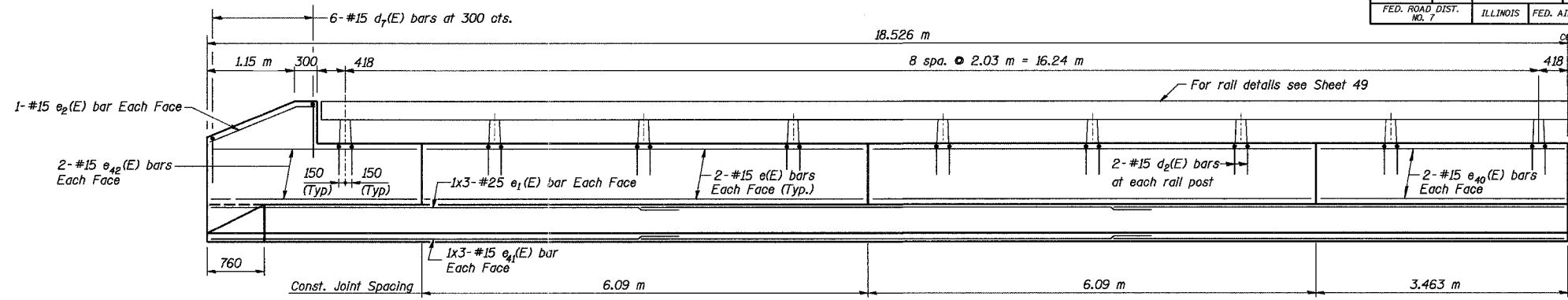
SECTION THRU PARAPET



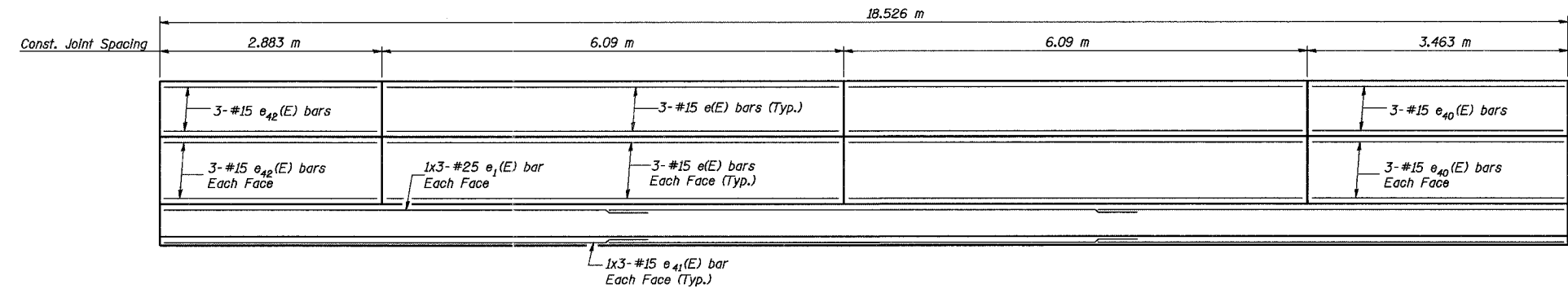
PARAPET END SECTION FOR TERMINAL BARRIER TYPE 6 (Inside View)



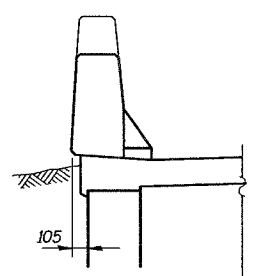
PARAPET END SECTION (Inside View)



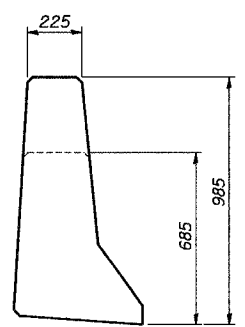
INSIDE ELEVATION OF PARAPET



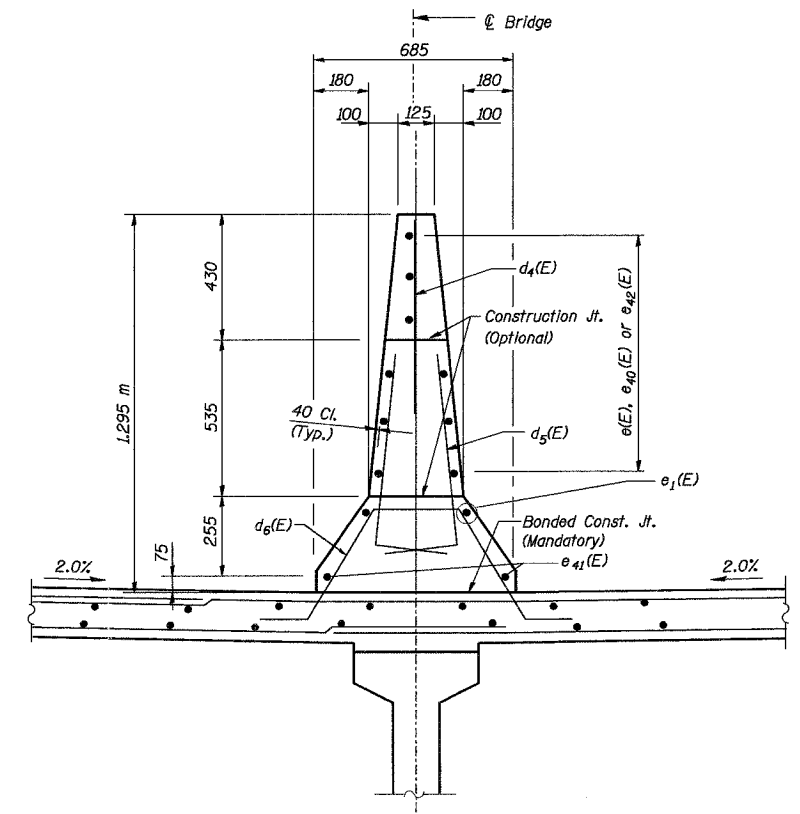
ELEVATION OF MEDIAN



VIEW C-C



SECTION D-D



SECTION THRU MEDIAN

MIN. BAR. LAP

- #15 bar = 640
- #25 bar = 132 m

Notes:
 Parapet shall have a smooth form lined surface on the exterior face.
 Slip forming is not allowed.
 See Sheet 10 for superstructure details and Bill of Material Reinforcement bars designated (E) shall be epoxy coated.
 Bars indicated thus 1x2-#15 etc. indicates 1 line of bars with 2 lengths per line.
 See Sheets 31 and 32 for v6(E) thru v12(E) bars.
 See sheets 15 & 16 for parapet & median joint details.
 All dimensions are in millimeters (mm) except as noted.

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
PARAPET AND MEDIAN DETAILS - SPAN 1		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 9	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 9

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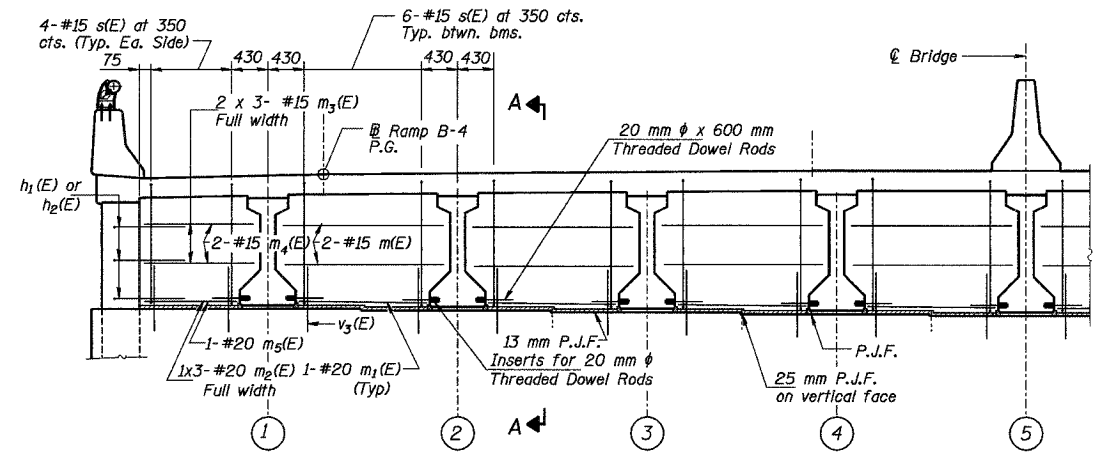
Designed by: DL
 Checked by: AK
 Drafted by: FTE
 Checked by: AK

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	453	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT	CONTRACT NO. 68200	

**ONE APPROACH SPAN
BILL OF MATERIAL**

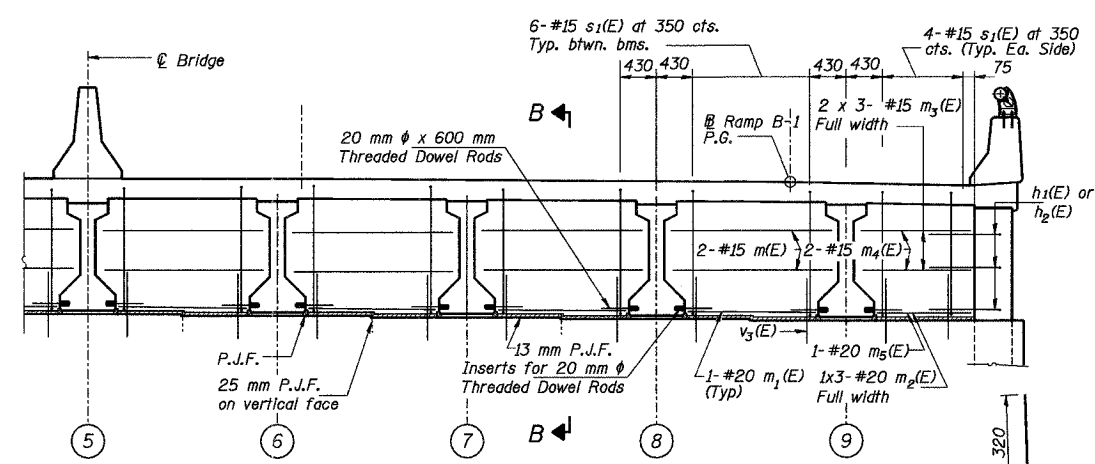
Bar No.	Size	Length (m)	Shape
a(E)	#15	8.49	—
a ₁ (E)	#15	9.59	—
a ₂ (E)	#20	1.20	—
a ₃ (E)	#15	12.54	—
a ₄ (E)	#15	10.39	—
a ₅ (E)	#15	18.75	—
a ₆ (E)	#15	18.75	—
b(E)	#15	6.60	—
b ₁ (E)	#15	9.56	—
d(E)	#15	0.97	—
d ₁ (E)	#15	1.49	—
d ₂ (E)	#15	0.64	—
d ₃ (E)	#6	1.05	—
d ₄ (E)	6	0.82	—
d ₅ (E)	#15	1.01	—
d ₆ (E)	#15	1.49	—
d ₇ (E)	#15	1.34	—
e(E)	#15	5.99	—
e ₁ (E)	#25	7.03	—
e ₂ (E)	#15	1.37	—
e ₃ (E)	#15	3.36	—
e ₄ (E)	#15	6.57	—
e ₅ (E)	#15	2.78	—
m(E)	#15	2.29	—
m ₁ (E)	#20	1.76	—
m ₂ (E)	#20	8.60	—
m ₃ (E)	#15	8.50	—
m ₄ (E)	#15	1.59	—
m ₅ (E)	#20	1.29	—
s(E)	#15	2.85	—
s ₁ (E)	#15	3.10	—
v ₂₀ (E)	#15	0.90	—
Reinforcement Bars, Epoxy Coated	kg	12490	
Concrete Superstructure	m ³	119.5	

Notes:
 Reinforcement bars designated (E) shall be epoxy coated.
 See sheets 8, 29, and 30 for v₃(E) bars.
 Work this sheet with sheet 8.
 All dimensions are in millimeters (mm) except as noted.
 Bars indicated thus 1x2-#15 etc. indicates 1 line of bars with 2 lengths per line.
 For Section A-A & B-B, see sheet 8.



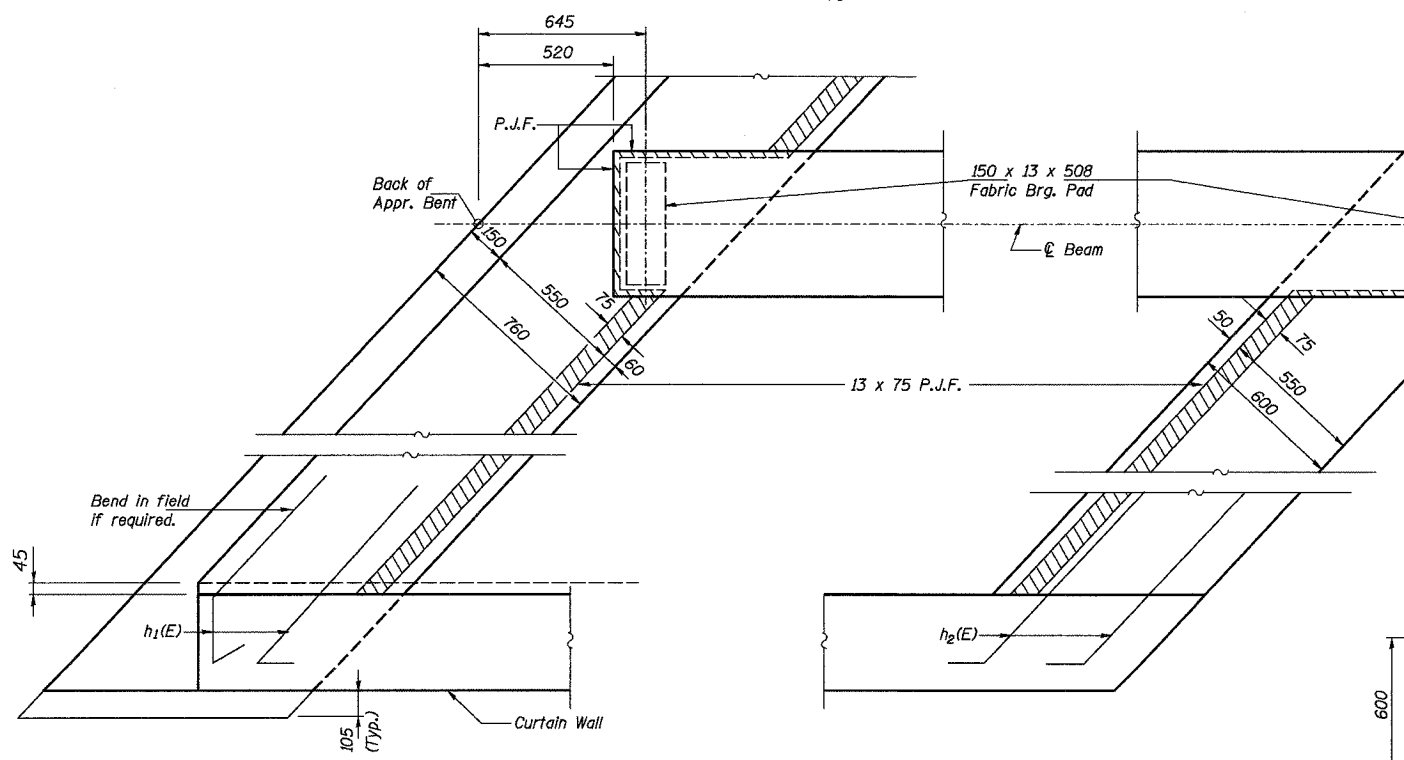
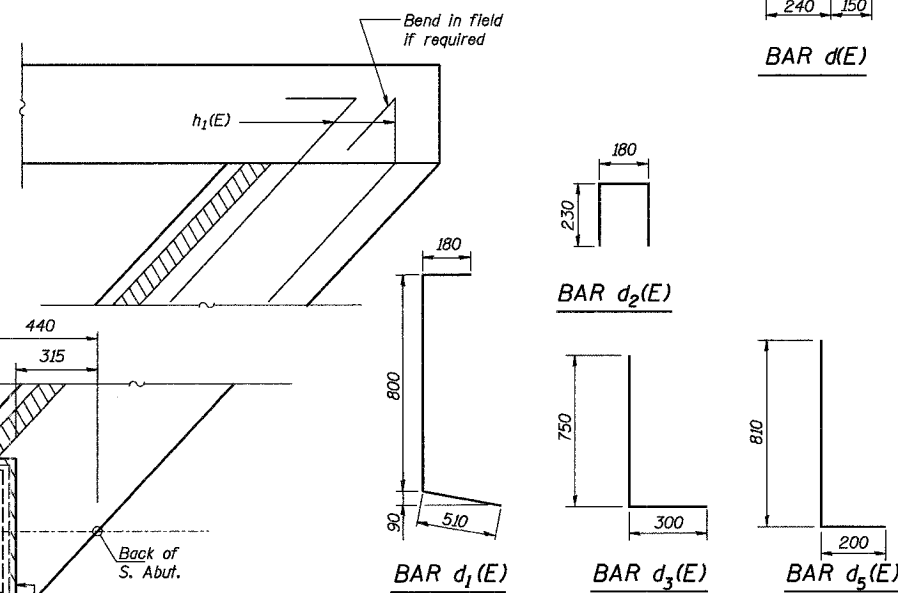
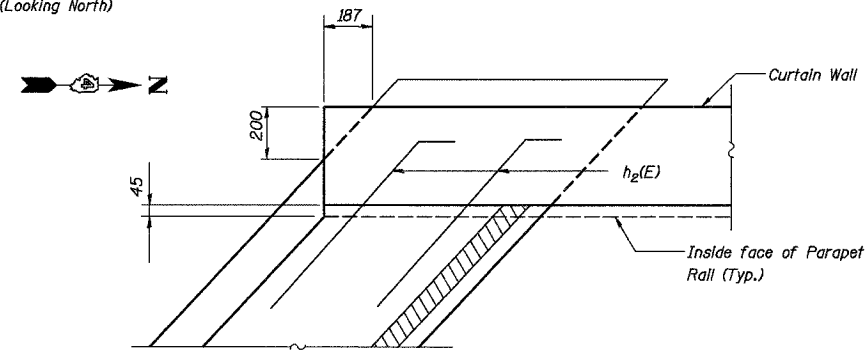
DIAPHRAGM AT SOUTH APPROACH BENT

For location of m(E), m₁(E), m₂(E), and m₃(E) bars see Section A-A on sheet 8.
 (Looking North)



DIAPHRAGM AT SOUTH ABUTMENT

For location of m(E), m₁(E), m₂(E), and m₃(E) bars see Section B-B on sheet 8.
 (Looking North)



PARTIAL PLAN

Top of Appr. Bent and S. Abut. Diaphragm

Designed by: DL
 Checked by: AK
 Drafted by: FTE/JMG
 Checked by: AK

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
DIAPHRAGM DETAILS - SPAN 1		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 10	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 10

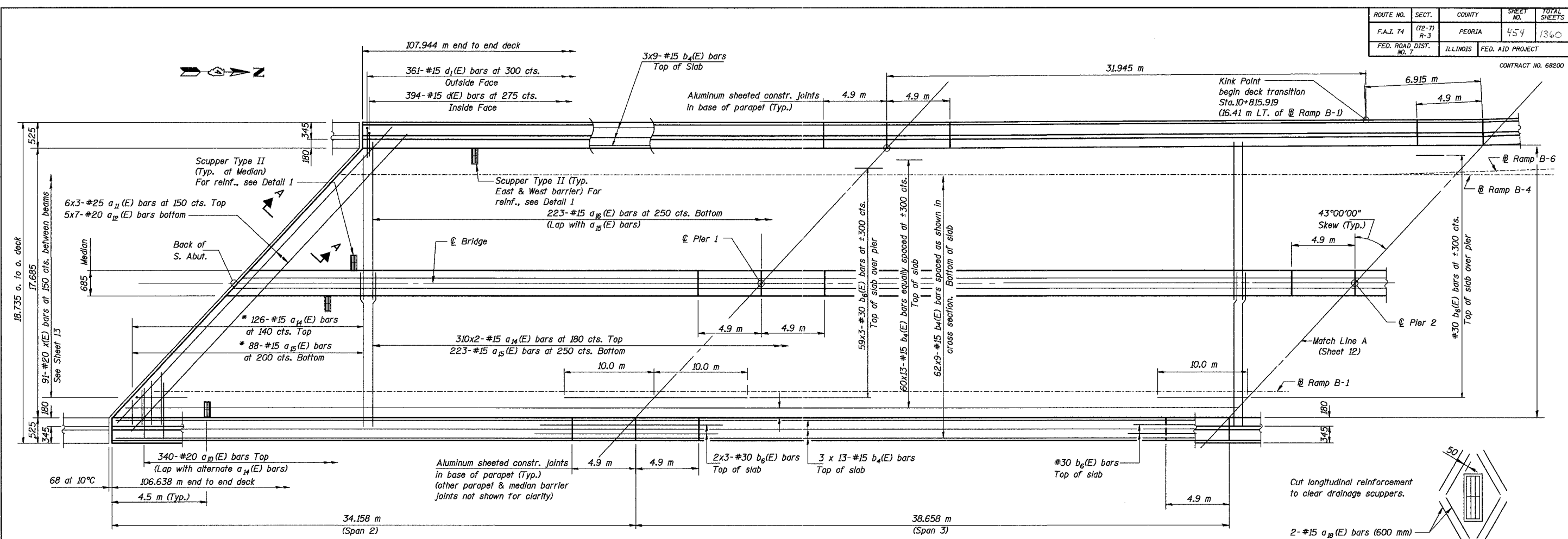
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ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	454	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200

Time: 09:58:32 AM

Date: 11/19/2004



* Order $a_{14}(E)$, $a_{15}(E)$ & $a_{17}(E)$ bars full length. Cut to fit skew and use remainder of bars in opposite end.

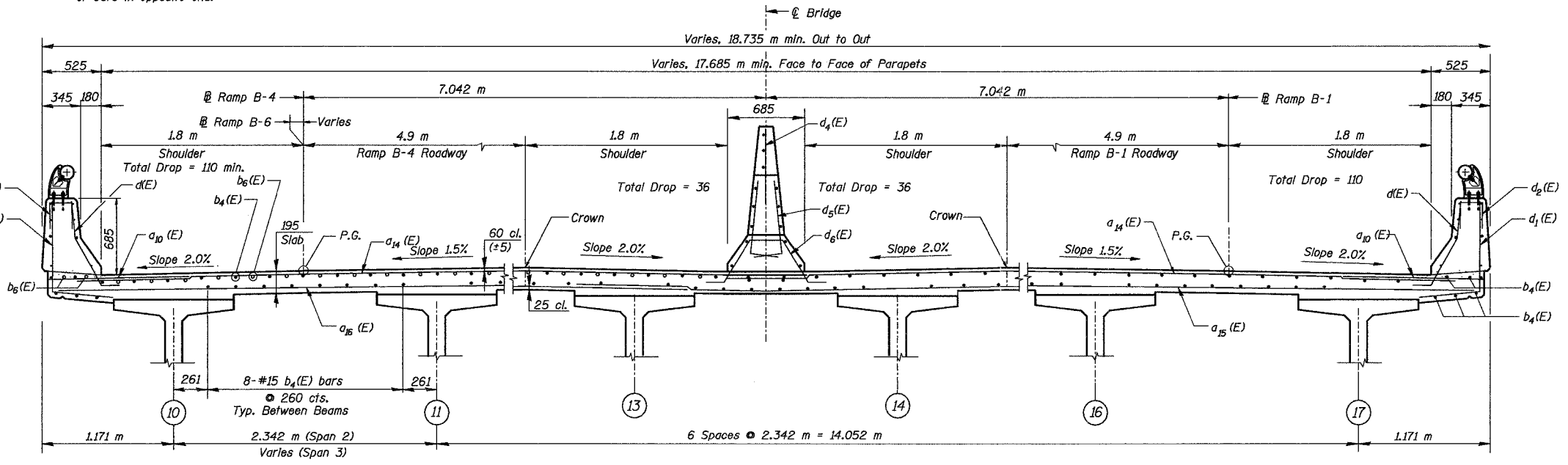
PLAN
(Spans 2 & 3)

Cut longitudinal reinforcement to clear drainage scuppers.

2-#15 $a_{18}(E)$ bars (600 mm) at 100 mm cts., Tied to bottom of top reinforcement (Typ.)

DETAIL 1

Notes:
For superstructure details and Bill of Material, See Sheet 15.
For Section A-A, See Sheet 13.
Reinforcement bars designated (E) shall be epoxy coated.
Bars indicated thus 20 x 3-#15 etc. indicates 20 lines of bars with 3 lengths per line.
All dimensions are in millimeters (mm) except as noted.



NEAR PIER

NEAR MIDSPAN

CROSS SECTION - SPANS 2 AND 3
(Looking North)

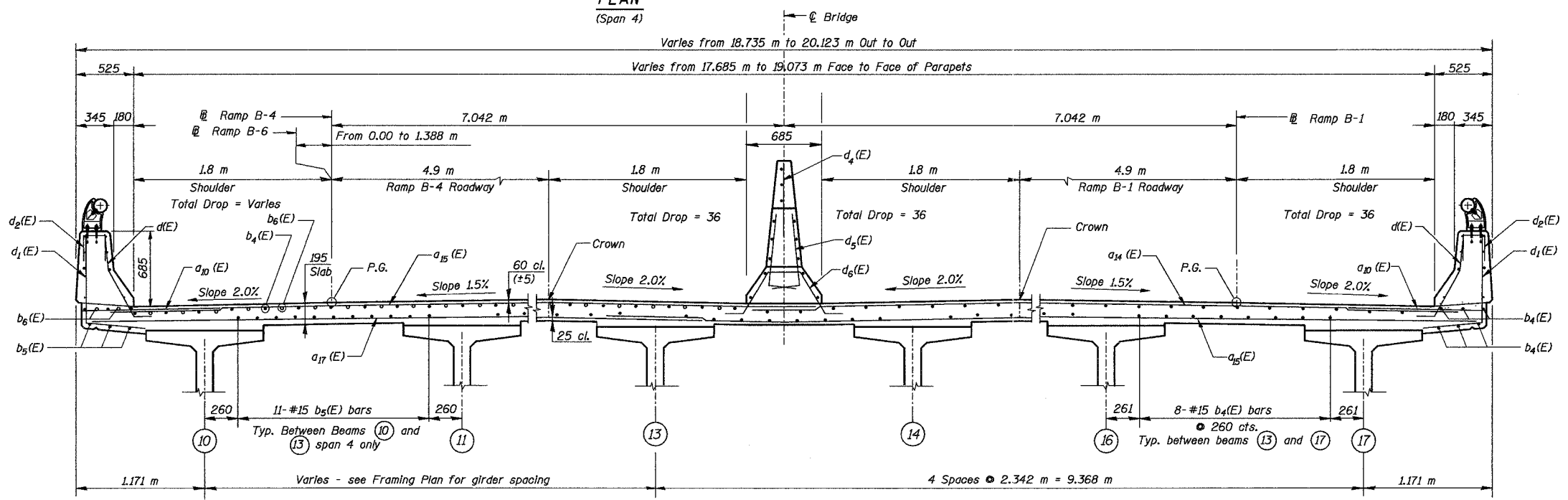
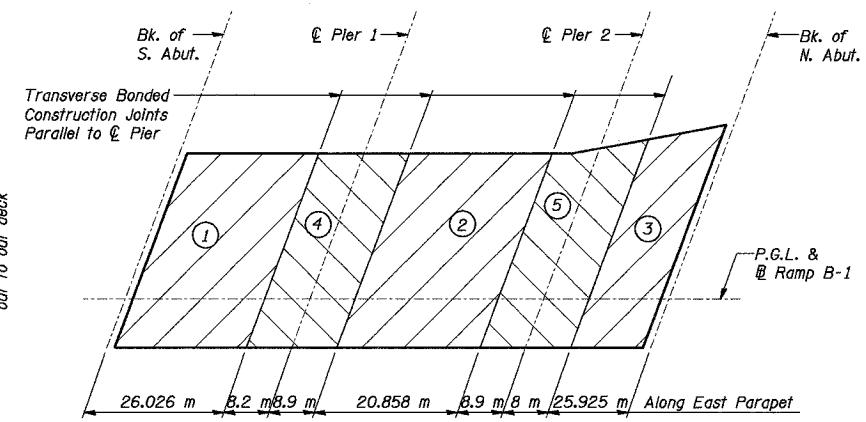
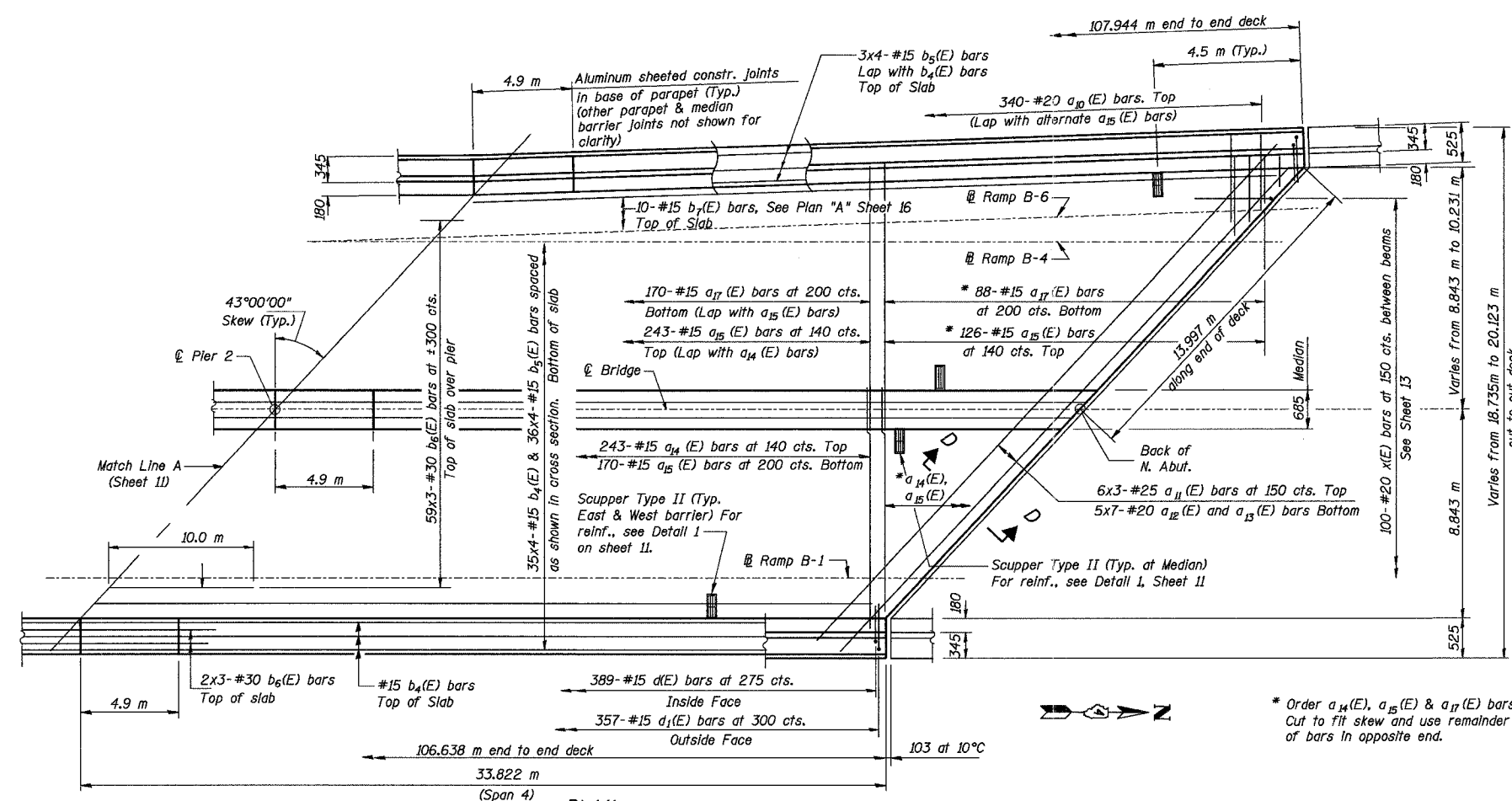
MIN. BAR. LAP
#15 bar = 640
#20 bar = 790
#25 bar = 1.32 m
#30 bar = 1.85 m

Designed by: AMK
Checked by: AK
Drafted by: FTE/SMG
Checked by: AK

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
DECK PLAN AND CROSS SECTION - SPANS 2 & 3		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 11	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 11

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	455	1260
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200



Notes:
 For Superstructure Details and Bill of Material, See Sheet 15.
 For Section D-D, See Sheet 13.
 Reinforcement bars designated (E) shall be epoxy coated.
 Bars indicated thus 20 x 3-#15 etc. indicates 20 lines of bars with 3 lengths per line.
 All dimensions are in millimeters (mm) except as noted.

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
DECK PLAN AND CROSS SECTION - SPAN 4		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 12	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 12

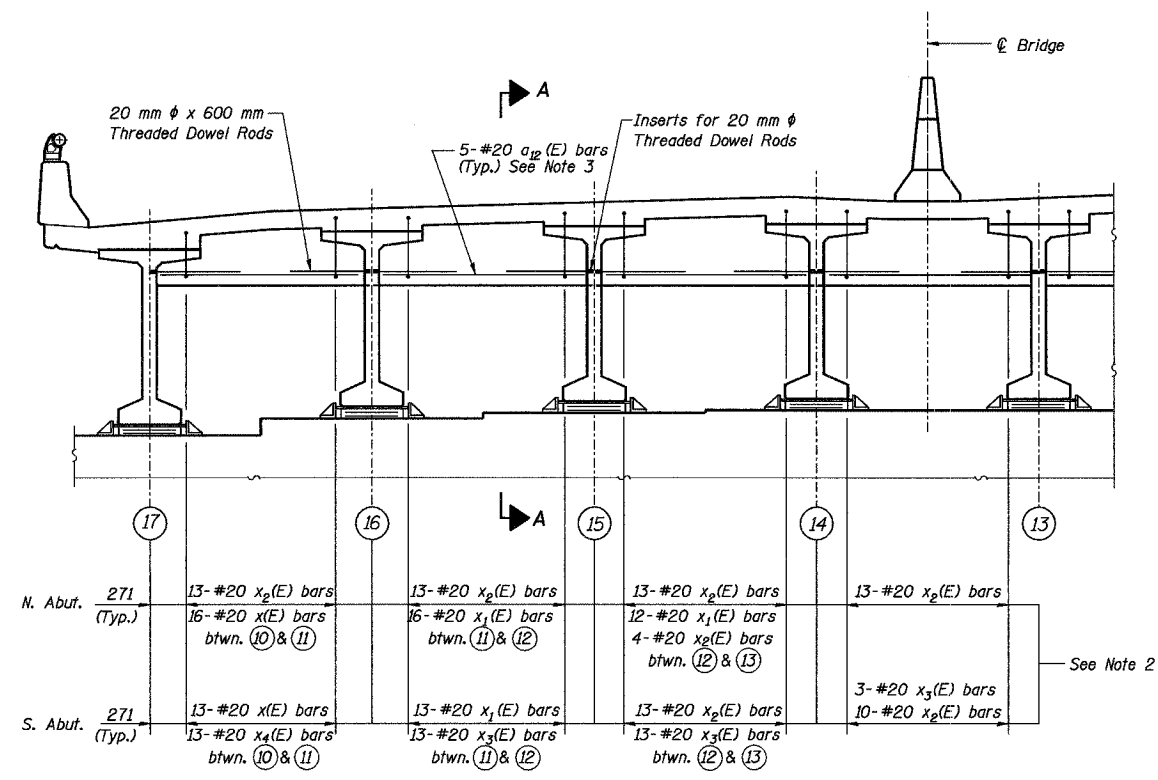
MIN. BAR. LAP
 #15 bar = 640
 #20 bar = 790
 #25 bar = 1.32m
 #30 bar = 1.85m

Date: 1/19/2004 Time: 09:58:47 AM File: P:\643996\structure\072-0183\Sheet1\Tracings\PS0003-10720183.dgn

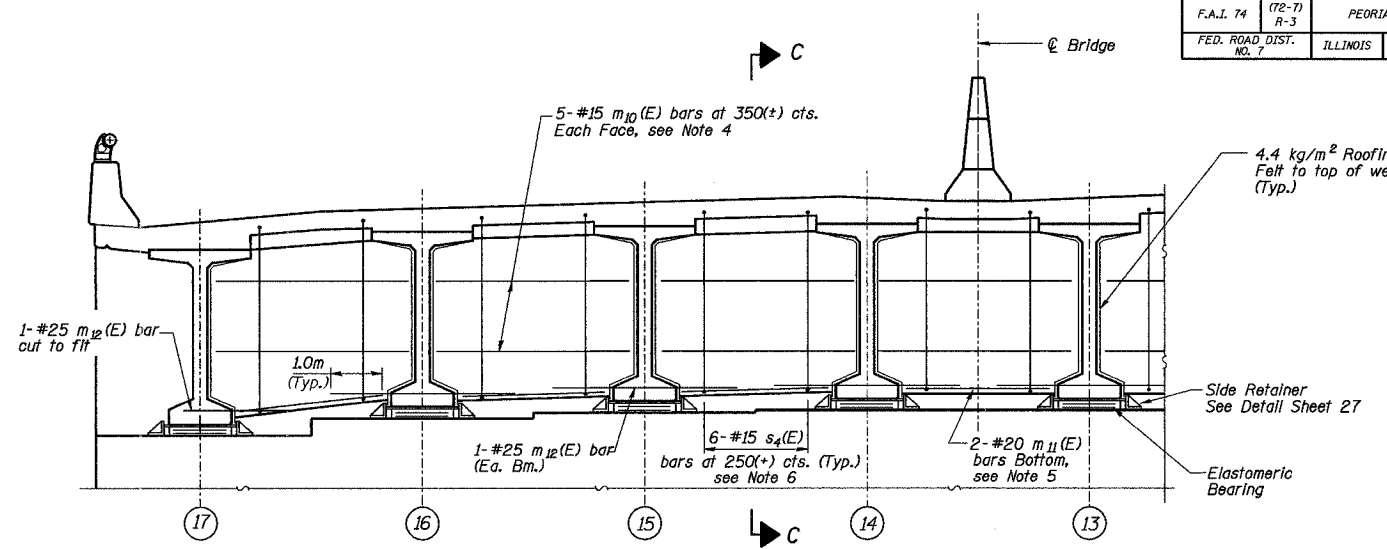
Designed by: AMK
 Checked by: AK
 Drafted by: FTE/SMG
 Checked by: AK

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	456	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

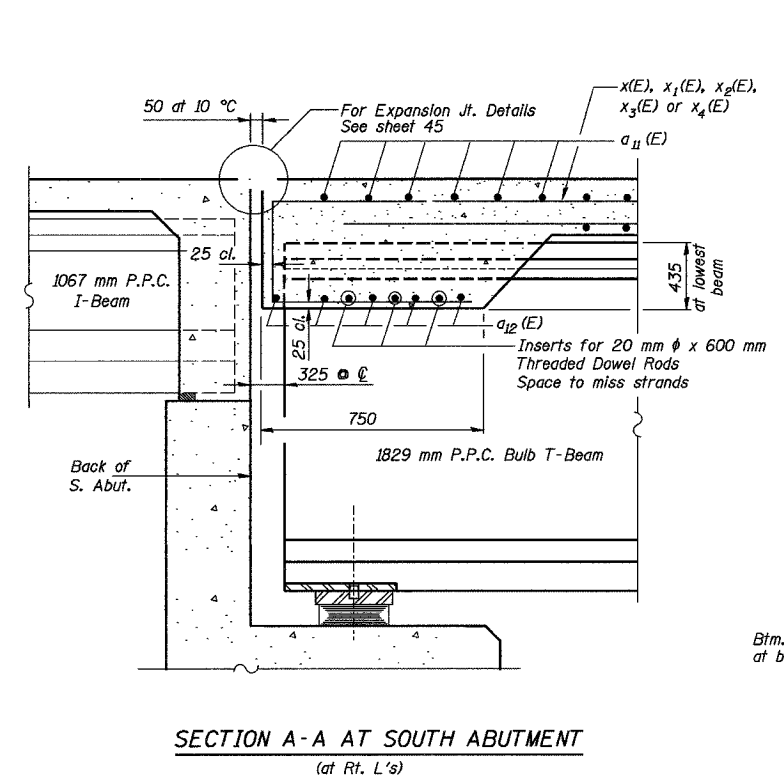
CONTRACT NO. 68200



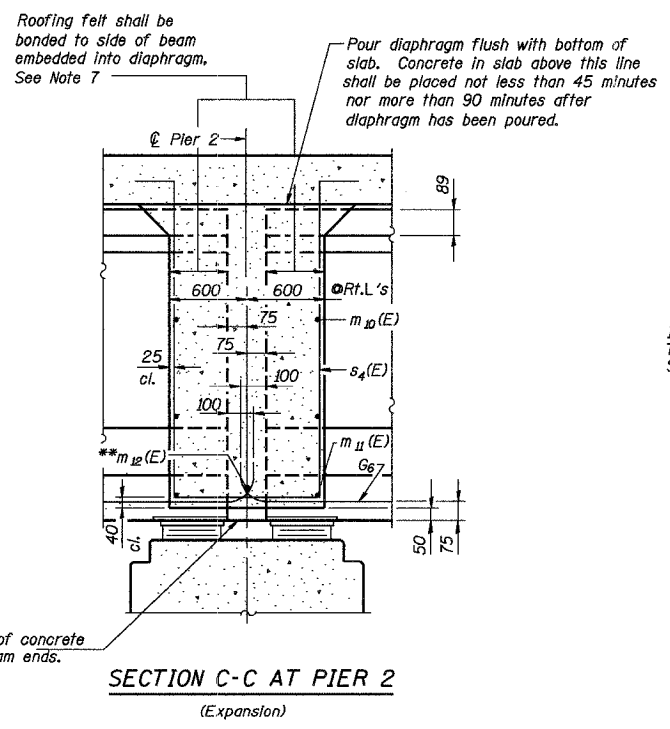
PARTIAL ELEVATION OF DIAPHRAGM AT ABUTMENT
(South Abutment as Shown, North Abutment Similar)



PARTIAL ELEVATION OF DIAPHRAGM AT PIER 2
(Expansion)

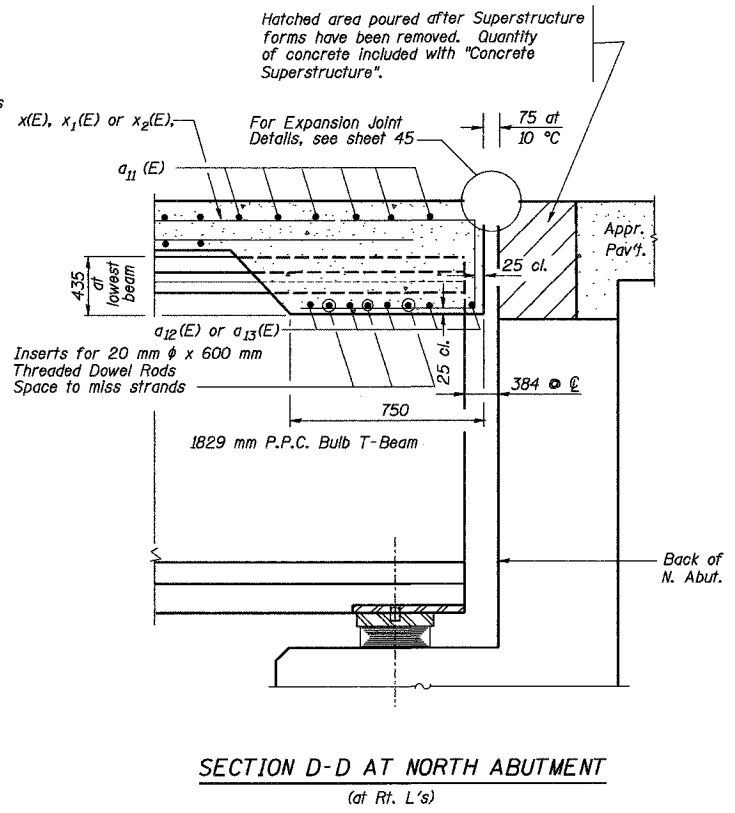


SECTION A-A AT SOUTH ABUTMENT
(at Rt. L's)



SECTION C-C AT PIER 2
(Expansion)

Note: Horizontal dimensions for Sec. C-C are along ϕ of beam unless otherwise noted.
** Tie #25 $m_{12}(E)$ with 3.8 mm wire tightly fastened to prevent any movement between bars.



SECTION D-D AT NORTH ABUTMENT
(at Rt. L's)

- Notes:**
- Reinforcement bars shown on this sheet are included in Bill of Material on Sheet 15.
 - #20 $x(E)$, $x_1(E)$, $x_2(E)$, $x_3(E)$ and $x_4(E)$ bars are spaced at 150 cts. parallel to traffic.
 - Between Beams 10 and 13 at North Abutment, provide 5-#20 $a_{13}(E)$ bars.
 - Between Beams 10 and 11, provide 5-#15 $m_{13}(E)$ bars at 350(+/-) cts., Each Face.
 - Between Beams 10 and 11, provide 2-#20 $m_{14}(E)$ bars bottom.
 - 6-#15 $s_3(E)$ and $s_4(E)$ bars are parallel to ϕ Beams.
 - Cost of 4.4 kg/m² roofing felt is included with "Concrete Superstructure".
 - All dimensions are in millimeters (mm) except as noted.

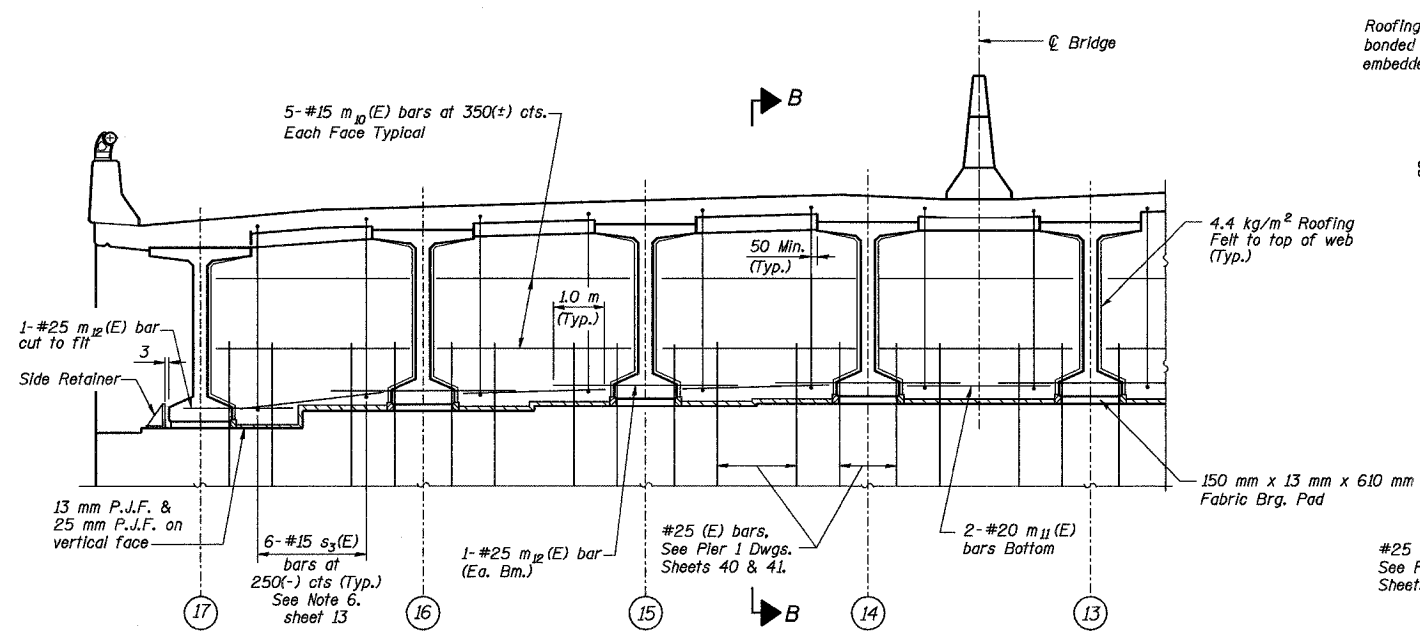
REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
DIAPHRAGM DETAILS - SPANS 2, 3 & 4		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 13	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 13

Time: 09:59:09 AM Date: 1/19/2004 File: P:\649996\struc\trac\072-0183\Sheet\1\Tracings\SD00027-1A0720183.dgn

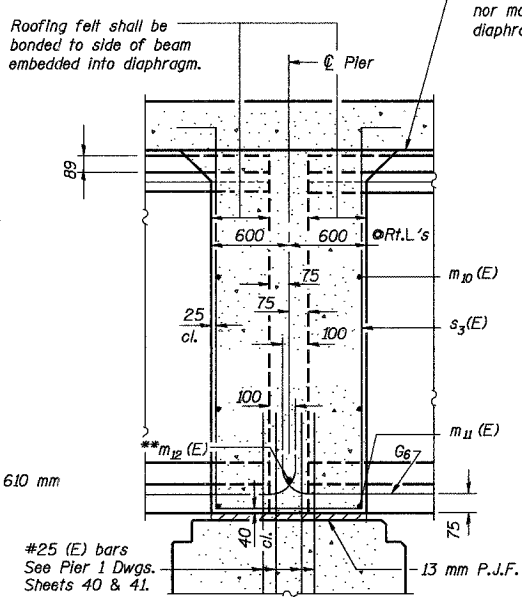
Designed by: AMK
Checked by: AK
Drafted by: JMG
Checked by: AK

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	457	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200



PARTIAL ELEVATION OF DIAPHRAGM AT PIER 1
(Fixed)

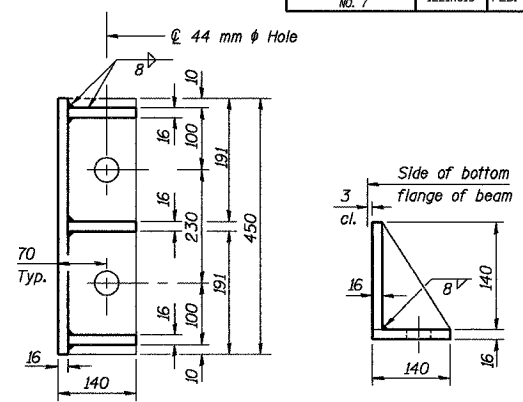


SECTION B-B AT PIER 1
(Fixed)

Note:
Horizontal dimensions for Section B-B are along \bar{c} of beam unless otherwise noted.

** Tie #25 m12 (E) with 3.8 mm wire tightly fastened to prevent any movement between bars.

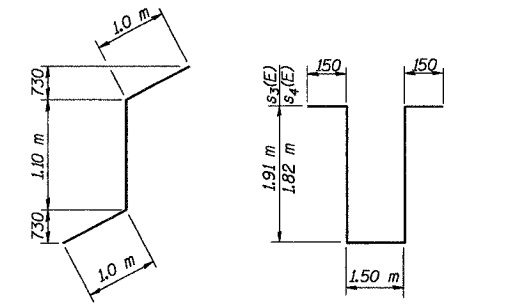
Cast of 4.4 kg/m² roofing felt is included with to "Concrete Superstructure".



PLAN ELEVATION
SIDE RETAINER

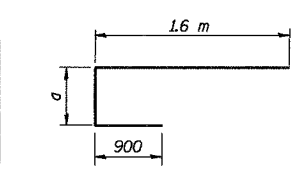
Notes: Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates. Mass included with structural steel.

The side retainer shall be galvanized according to AASHTO M111 and ASTM A385.



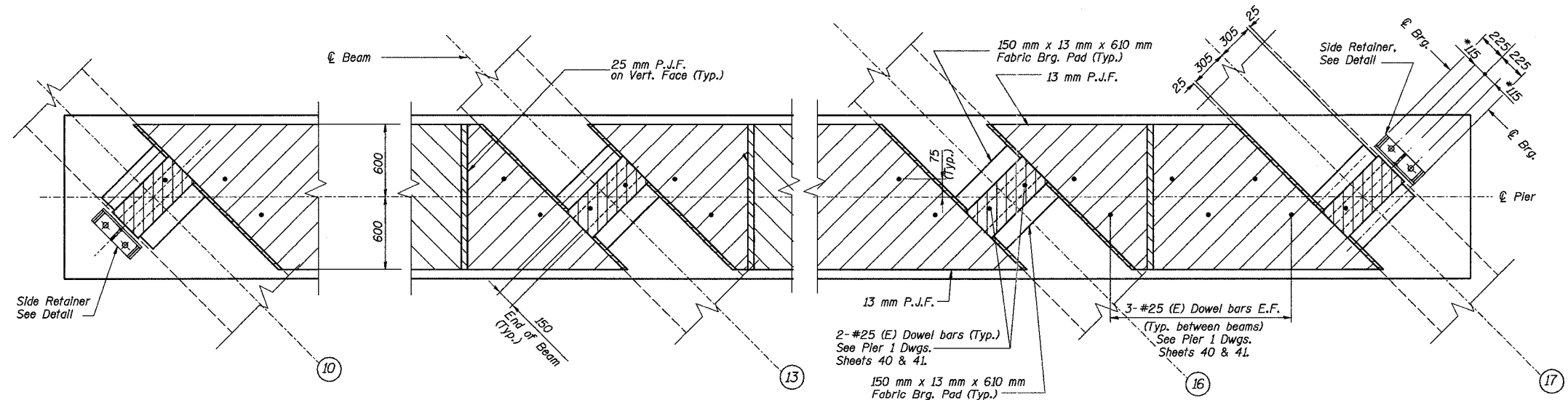
BAR m12 (E) **BARS s3 (E) & s4 (E)**

Bar	a
x(E)	590
x1(E)	700
x2(E)	810
x3(E)	920
x4(E)	1110



BAR x(E)

Notes:
For notes, see Sheet 13.



PLAN - TOP OF PIER 1 DETAIL
(Fixed)

Date: 1/19/2004 Time: 09:59:26 AM File: P:\43996\struc\ur-d\072-0183\Sheet1\Tracings\SD0028-1A0720183.dgn

Designed by: AMK
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Drafted by: JMG
Checked by: AK

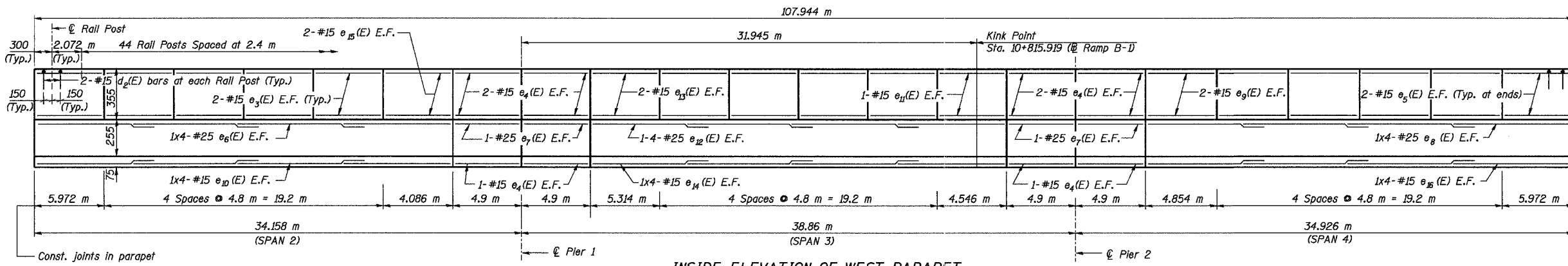
REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIAPHRAGM DETAILS - PIER 1		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 14	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 14

Time: 09:59:58 AM

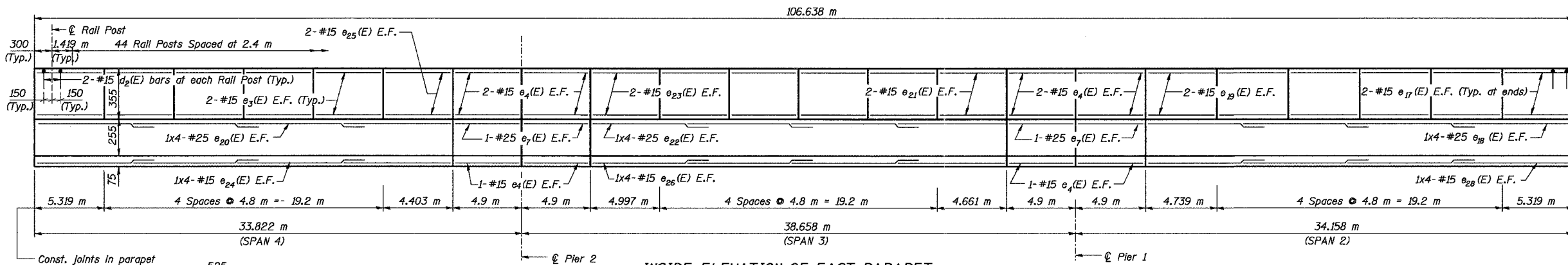
Date: 11/19/2004

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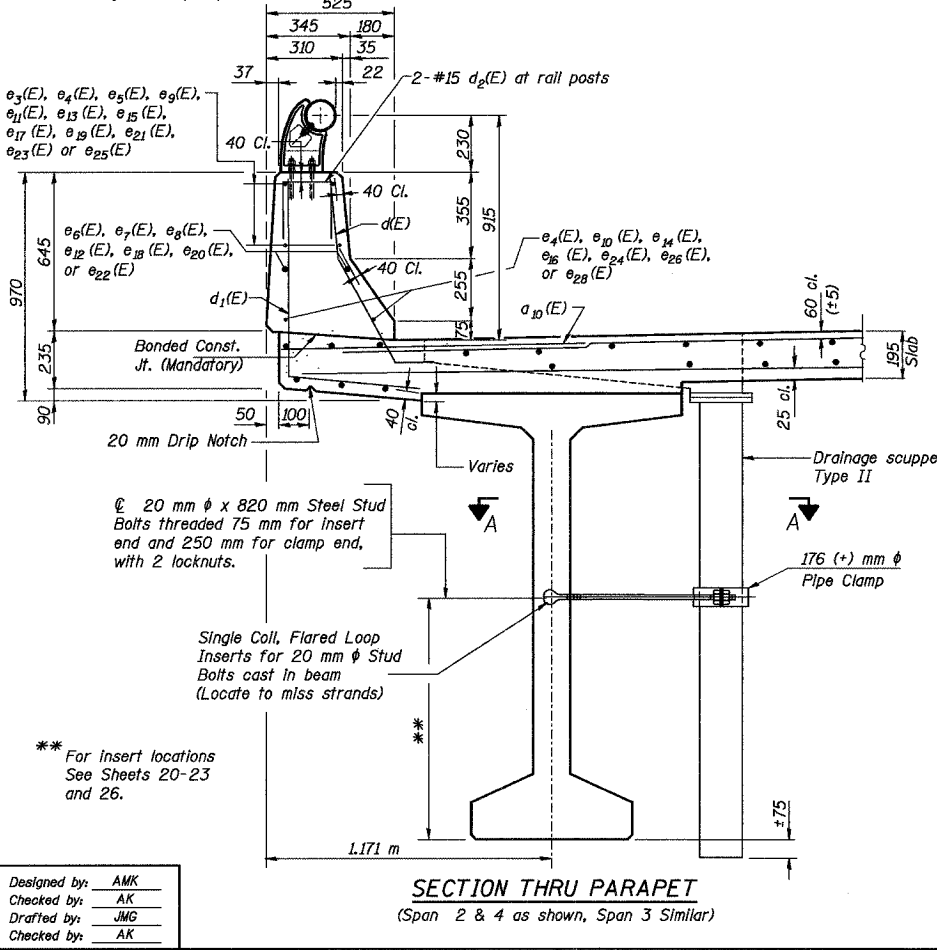
ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	458	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT	CONTRACT NO. 68200	



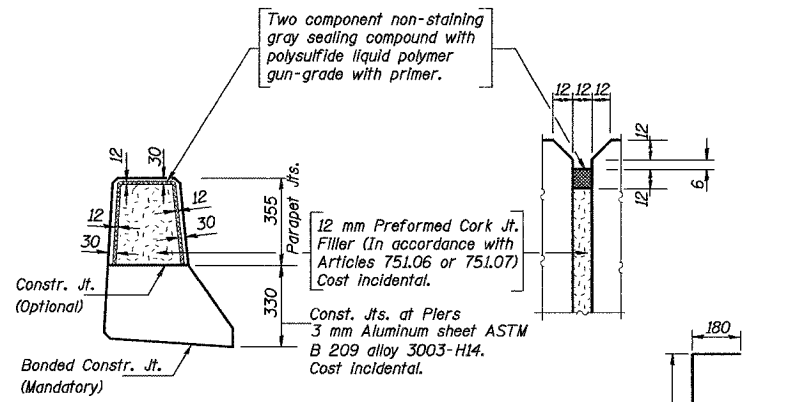
INSIDE ELEVATION OF WEST PARAPET
(Spans 2, 3, & 4)



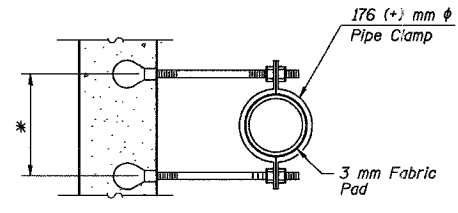
INSIDE ELEVATION OF EAST PARAPET
(Spans 2, 3, & 4)



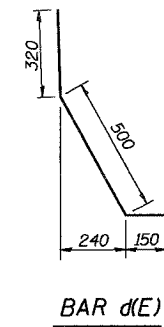
SECTION THRU PARAPET
(Span 2 & 4 as shown, Span 3 Similar)



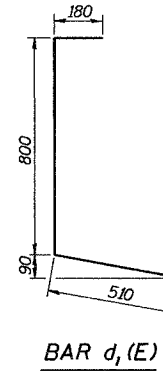
PARAPET JOINT DETAILS



SECTION A-A



BAR d(E)



BAR d1(E)

BAR d2(E)

SUPERSTRUCTURE BILL OF MATERIAL SPANS 2, 3 & 4

Bar	No.	Size	Length (m)	Shape
a10(E)	680	#20	1.20	
a11(E)	36	#25	9.45	
a12(E)	55	#20	2.90	
a13(E)	15	#20	3.20	
a14(E)	989	#15	9.50	
a15(E)	850	#15	10.85	
a16(E)	223	#15	8.50	
a17(E)	258	#15	9.90	
a18(E)	64	#15	.60	
b4(E)	1544	#15	8.80	
b5(E)	156	#15	9.15	
b6(E)	378	#30	7.90	
b7(E)	10	#15	7.50	
m10(E)	130	#15	2.90	
m11(E)	26	#20	2.18	
m12(E)	16	#25	3.10	
m13(E)	10	#15	3.20	
m14(E)	2	#20	2.48	
s3(E)	42	#15	5.62	
s4(E)	42	#15	5.44	
x(E)	29	#20	3.09	
x1(E)	41	#20	3.20	
x2(E)	79	#20	3.31	
x3(E)	29	#20	3.42	
x4(E)	13	#20	3.61	
Reinforcement Bars, Epoxy Coated		kg	95510	
Concrete Superstructure		m ³	610.3	

Bar	No.	Size	Length (m)	Shape
d(E)	783	#15	0.97	
d1(E)	718	#15	1.49	
d2(E)	184	#15	0.64	
d3(E)	357	#15	0.82	
d4(E)	778	#15	1.01	
d5(E)	369	#15	1.49	
e3(E)	96	#15	4.70	
e4(E)	92	#15	4.85	
e5(E)	8	#15	5.87	
e6(E)	16	#25	8.28	
e7(E)	24	#25	4.85	
e8(E)	8	#25	8.48	
e9(E)	4	#15	4.75	
e10(E)	16	#15	7.77	
e11(E)	4	#15	4.44	
e12(E)	8	#25	8.26	
e13(E)	4	#15	5.21	
e14(E)	8	#15	7.75	
e15(E)	4	#15	3.98	
e16(E)	8	#15	7.97	
e17(E)	8	#15	5.21	
e18(E)	8	#25	8.28	
e19(E)	4	#15	4.64	
e20(E)	16	#25	8.20	
e21(E)	4	#15	4.56	
e22(E)	16	#25	8.18	
e23(E)	4	#15	4.89	
e24(E)	16	#15	7.69	
e25(E)	4	#15	4.30	
e26(E)	16	#15	7.67	
e27(E)	45	#15	5.75	
e28(E)	8	#15	7.77	
e29(E)	45	#15	5.67	
e30(E)	45	#15	5.68	

Notes:
 Parapets shall have a smooth form lined surface on the exterior face.
 Slip forming is not allowed.
 Reinforcement bars designated (E) shall be epoxy coated.
 Bars indicated thus 1 x 3-#15 etc. indicates 1 line of bars with 3 lengths per line.
 For location of scuppers see General Plan.
 The clamping device and Inserts shall be galvanized according to with AASHTO M 232.
 All dimensions are in millimeters (mm) except as noted.
 E.F. = Each Face

MIN. BAR LAP
 #15 bar = 640 mm
 #25 bar = 1.32 m

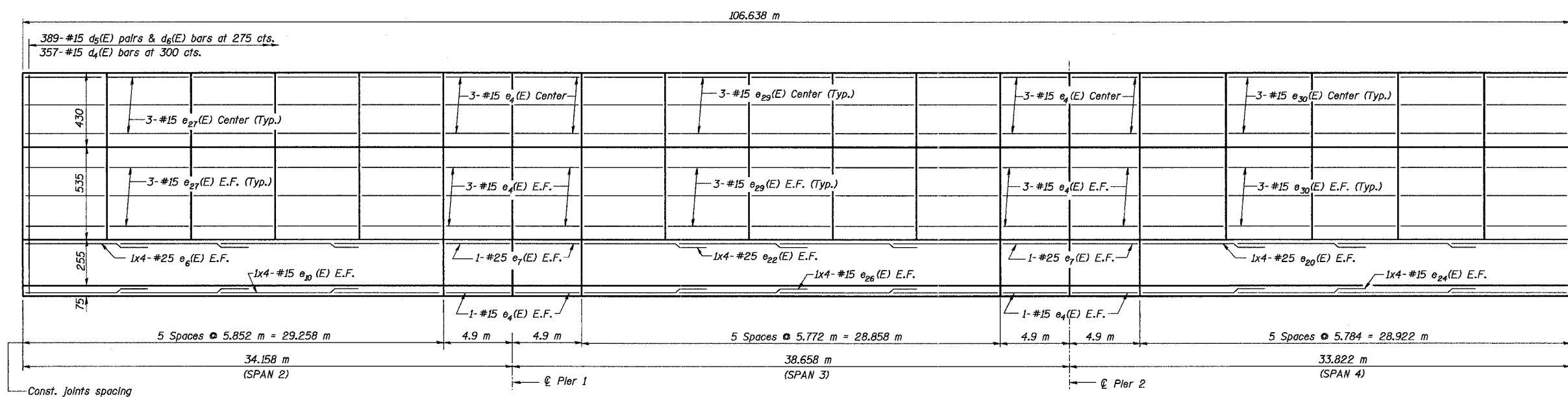
REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
PARAPET ELEVATIONS AND DETAILS		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 15	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 15

Designed by: AMK
 Checked by: AK
 Drafted by: JMG
 Checked by: AK

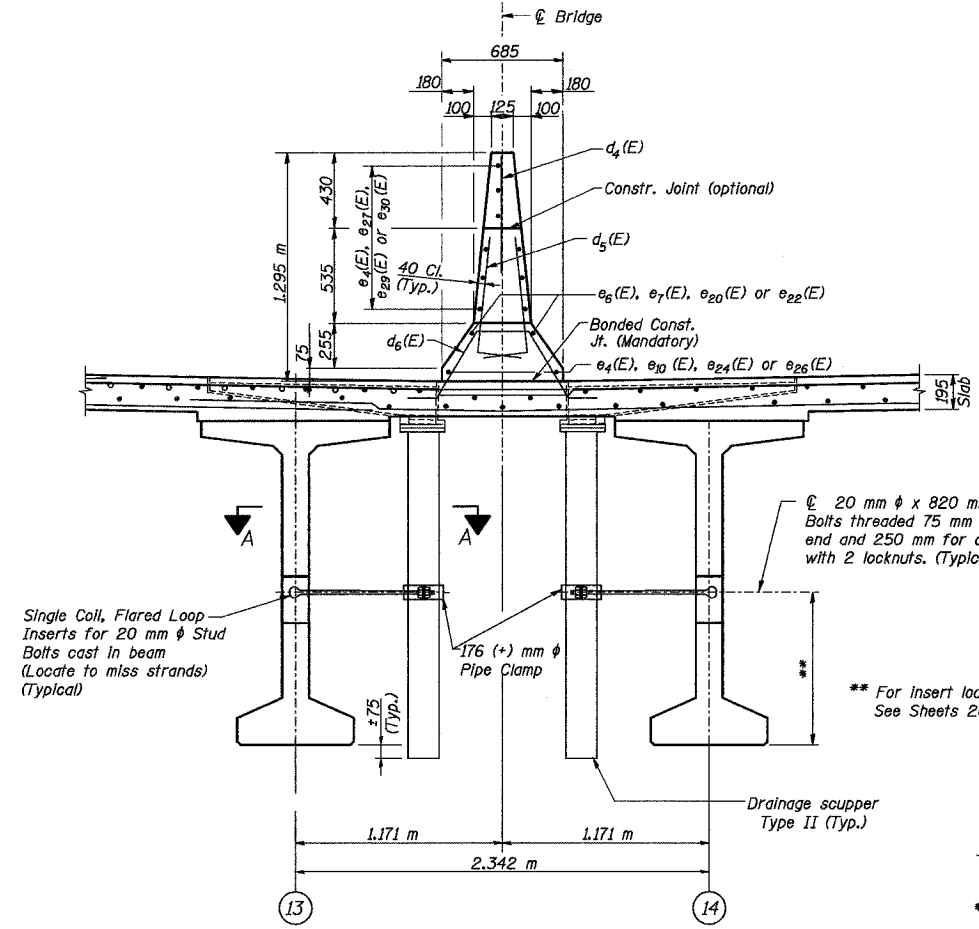
** For Insert locations See Sheets 20-23 and 26.

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	459	1360
FED. ROAD DIST. NO. 7	ILLINOIS FED. AID PROJECT			

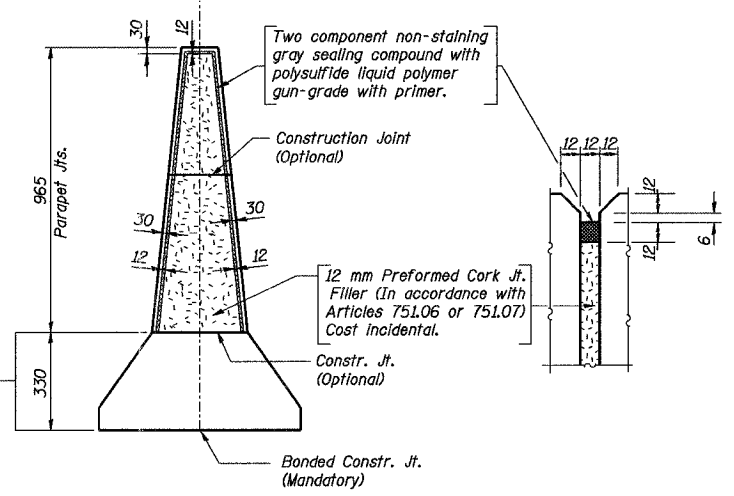
CONTRACT NO. 68200



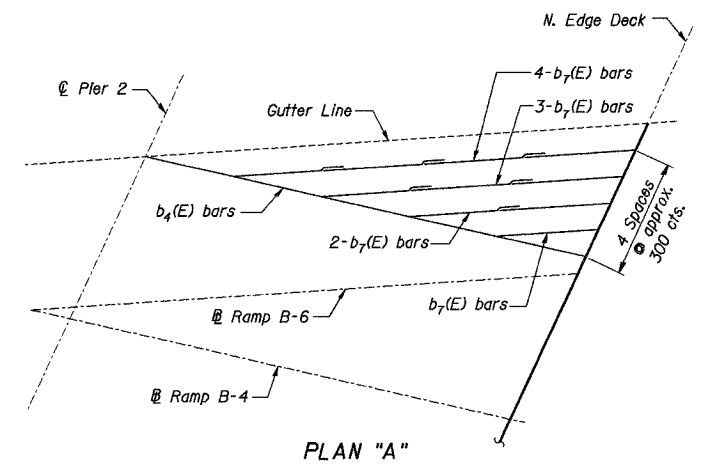
MEDIAN BARRIER ELEVATION
(Spans 2, 3, & 4)



SECTION THRU MEDIAN

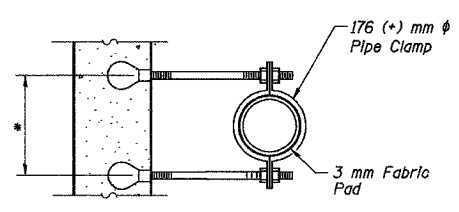


MEDIAN JOINT DETAILS

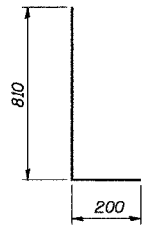


PLAN "A"

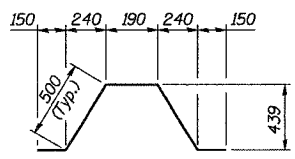
Notes:
For Bill of Material, see Sheet 15.
For location of scuppers, see General Plan.
The clamping device and inserts shall be galvanized according to AASHTO M 232.
All dimensions are in millimeters (mm) except as noted.
E.F. = Each Face



SECTION A-A



BAR d5(E)



BAR d6(E)

MIN. BAR LAP
#15 bar = 640
#25 bar = 1.32 m

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
MEDIAN BARRIER ELEVATION AND DETAILS		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 16	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 16

Date: 11/19/2004 Time: 0:00:33 AM
Filename: P:\643996\Structure\072-0183\Sheet\Tracings\SD00030-1A0720183.dgn

Designed by: AMK
Checked by: AK
Drafted by: JMG
Checked by: AK

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	760	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200

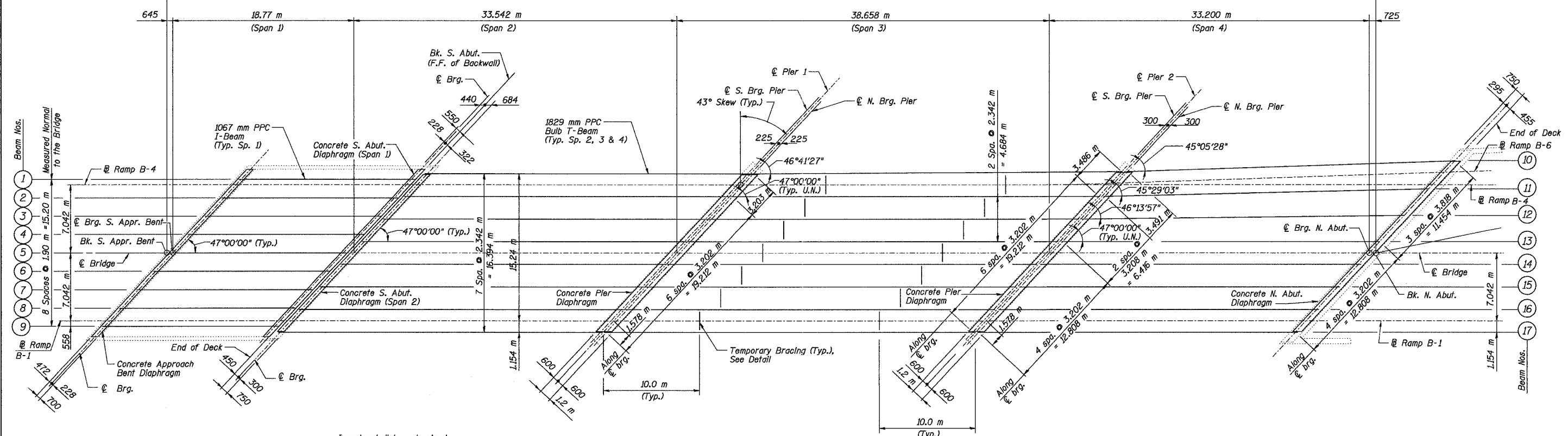
Date: 1/19/2004

Time: 10:04:33 AM

File name: P:\A\43996\structure\072-0183\Sheet1\Tracings\FR0001-10720183.dgn

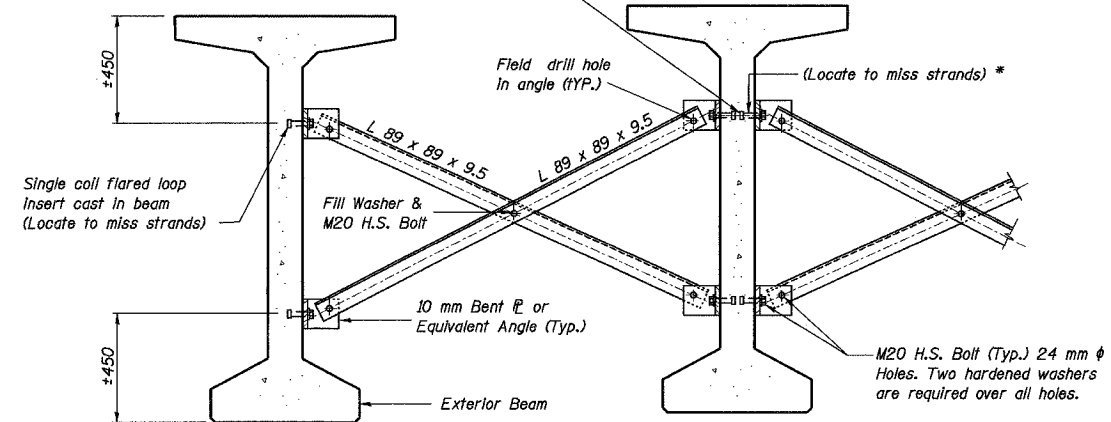


125.54 m Bk. of S. Appr. Bent to Bk. of N. Abut.



FRAMING PLAN

Inserts shall be galvanized according to with AASHTO M232. After removal of the bracing, inserts shall be filled with M20 bolts galvanized according to with AASHTO M232.



TEMPORARY BRACING DETAILS

Note:
Temporary bracing details other than those shown are allowed subject to the approval of the Engineer. Cost of temporary bracing included with furnishing and erecting precast prestressed concrete bulb T-beams, 1829 mm.

* Inserts shall be the single coil flared loop type and shall be offset on each side of the web by an amount equal to the beam spacing multiplied by the tangent of the skew angle to allow temporary bracing to be placed at right angles to the beams.

Designed by:	AMK
Checked by:	AK
Drafted by:	FTE
Checked by:	AK

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
FRAMING PLAN AND TEMPORARY BRACING DETAILS		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3		
F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 17	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 17

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	461	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200

**INTERIOR BEAM REACTION TABLE 1-9
SOUTH VAULTED SPAN 1**

		S. Approach Bent	S. Abutment
$R_{\bar{Q}}$	(kN)	140	140
$R_{s\bar{Q}}$	(kN)	61	61
$R_{\bar{L}}$	(kN)	171	171
Imp.	(kN)	47	47
R (Total)	(kN)	419	419

**INTERIOR BEAM REACTION TABLE
SPANS 2,3, AND 4**

1829 mm Bulb-T PPC I-BEAM	S. Abut.	Pier 1 Span 2	Pier 1 Span 3	Pier 2 Span 3		Pier 2 Span 4		N. Abut.	
	10-17	10-17	10-17	10-12	13-17	10-12	13-17	10-12	13-17
$R_{\bar{Q}}$	(kN)	374	374	428	428	404	370	404	370
$R_{s\bar{Q}}$	(kN)	126	196	196	195	195	195	124	124
$R_{\bar{L}}$	(kN)	222	189	189	203	188	203	256	221
Imp.	(kN)	47	40	38	41	38	44	55	48
R (Total)	(kN)	769	799	851	867	849	846	839	763

**INTERIOR BEAM MOMENT TABLE 1-9
SOUTH VAULTED SPAN 1**

		0.5 Span
I	(10 ⁶ mm ⁴)	37859
I'	(10 ⁶ mm ⁴)	112990
S_b	(10 ³ mm ³)	84431
S_b'	(10 ³ mm ³)	142246
S_t	(10 ³ mm ³)	61221
S_t'	(10 ³ mm ³)	414384
\bar{Q}	(kN/m)	15.8
$M_{\bar{Q}}$	(kN·m)	616
$s_{\bar{Q}}$	(kN/m)	6.88
$M_{s\bar{Q}}$	(kN·m)	299
$M_{\bar{L}}$	(kN·m)	581
M (Imp)	(kN·m)	159

**INTERIOR BEAM MOMENT TABLE
SPANS 2,3 AND 4**

1829 mm Bulb-T PPC I-BEAM	0.4 Span 2	Pier 1	0.5 Span 3	Pier 2		0.6 Span 4	
	10-17	10-17	10-17	10-12	13-17	10-12	13-17
I	(10 ⁶ mm ⁴)	227218	227218	227218	227218	227218	227218
I'	(10 ⁶ mm ⁴)	430400	-	430400	-	452170	430400
S_b	(10 ³ mm ³)	244428	244428	244428	244428	244428	244428
S_b'	(10 ³ mm ³)	321372	-	321372	-	326958	321372
S_t	(10 ³ mm ³)	252689	252689	252689	252689	252689	252689
S_t'	(10 ³ mm ³)	878834	-	878834	-	1013740	878834
\bar{Q}	(kN/m)	22.4	22.4	22.4	22.4	24.5	22.4
$M_{\bar{Q}}$	(kN·m)	3115	0	4075	0	3328	3042
$s_{\bar{Q}}$	(kN/m)	9.79	9.79	9.79	9.79	9.79	9.79
$M_{s\bar{Q}}$	(kN·m)	809	1281	557	1233	1208	807
$M_{\bar{L}}$	(kN·m)	1295	1327	1220	1512	1265	1519
M (Imp)	(kN·m)	276	283	244	325	272	326

Legend:

I and I' are the moment of inertia and composite moment of inertia of the beam section.
 S_b and S_b' are the non-composite and composite section modulus for the bottom fiber of the prestressed beam.
 S_t and S_t' are the non-composite and composite section modulus for the top fiber of the prestressed beam.
 $M_{\bar{Q}}$ is the moment due to dead loads on non-composite prestressed beam. It is conservatively calculated at 0.5 of the span.
 $M_{s\bar{Q}}$ is the moment due to dead loads on the composite section.
 $M_{\bar{L}}$ is the moment due to live load on the composite section.
 M (Imp) is the moment due to live load impact on the composite section.
Pier 1 Span 2, Pier 2 Span 3, etc. are the loads distributed to each girder. The diaphragm weight is not included in the dead load.

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
BEAM REACTION AND MOMENT TABLE		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 18	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 18

Time: 10:04:41 AM

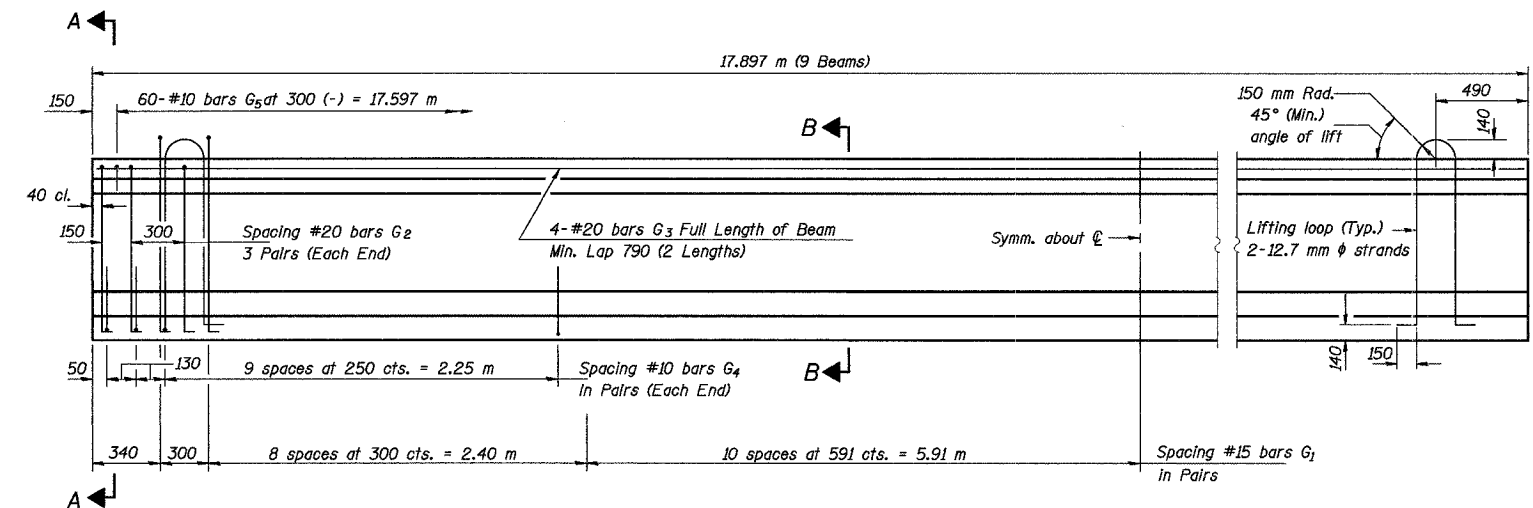
Date: 1/19/2004

File name: P:\643996\structure\072-0183\Sheet\Tracings\ST0002-10720183.dgn

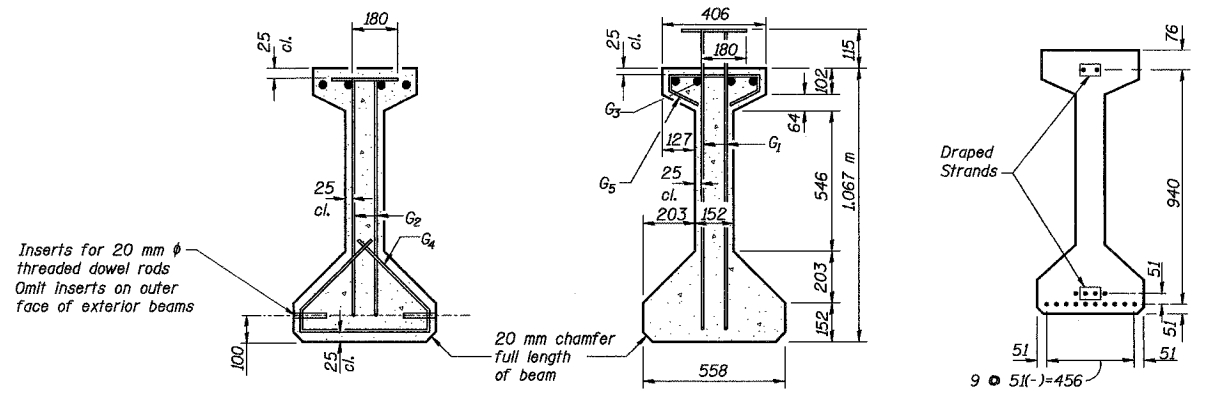
Designed by: AMK
Checked by: WEE
Drafted by: FTE/JMG
Checked by: WEE

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	462	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200



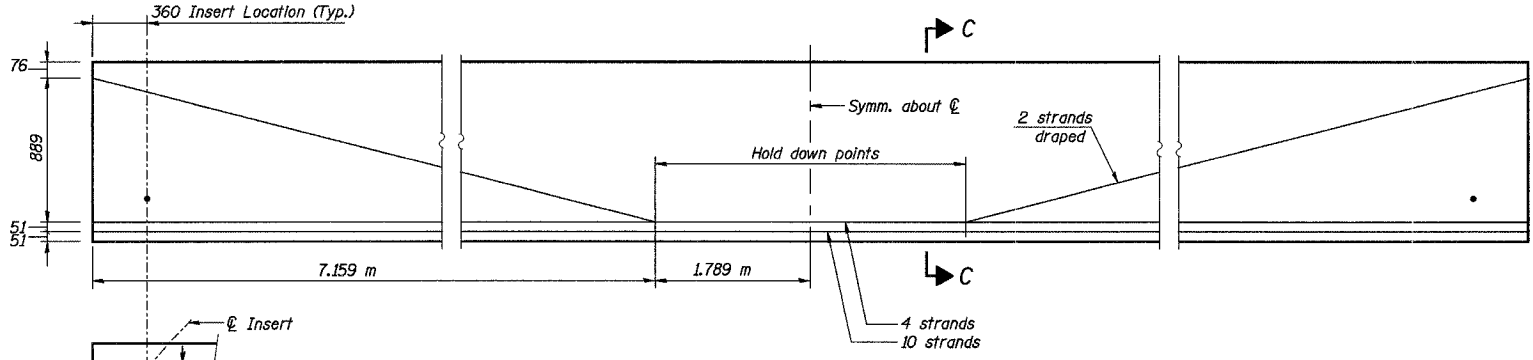
ELEVATION OF BEAM
(Showing Reinforcement & Dimensions)



SECTION A-A

SECTION B-B

SECTION C-C



ELEVATION OF BEAM
(Showing Prestressing Steel)

*** BAR LIST**

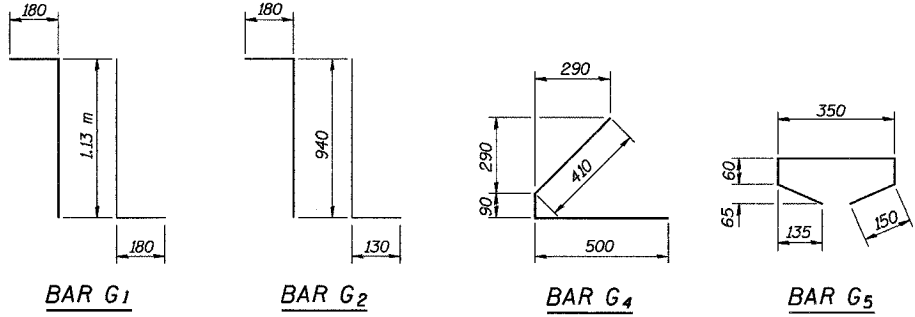
Bar	No.	Size	Length (m)	Shape
G ₁	78	#15	1.49	7L
G ₂	12	#20	1.25	7L
G ₃	8	#20	9.35	—
G ₄	48	#10	1.00	└
G ₅	60	#10	0.77	└

* For one beam only.

BILL OF MATERIAL

Item	Unit	Total
Furnishing and Erecting Precast Prestressed Concrete I-Beams, 1067 mm	m	161.07

Skew Inserts for 20 mm φ x 600 mm Threaded Dowel Rod. Space Inserts to miss strands at South Approach Bent and South Abutment.



BAR G₁

BAR G₂

BAR G₄

BAR G₅

Notes:

- All inserts and threaded dowel rods for inserts, reinforcing and Prestressing Steel, and other items which are cast into the Precast Concrete I-Beams shall be included in the contract unit price per meter of "Furnishing and Erecting Precast Prestressed Concrete I-Beams, 1067 mm".
- Inserts for 20 mm φ threaded dowel rods are to be two strut, coil type for interior I-beams and single coil, flared loop type for exterior I-beams.
- Prestressing steel shall be uncoated high strength, low relaxation 7-wire strand (Fu = 1860 MPa). The nominal diameter shall be 12.7 mm and the nominal cross-sectional area shall be 98.71 mm².
- Required release strength, f'ci, shall be 35 MPa.
- Non-prestressing steel shall conform to AASHTO designation M 31M, M 42M or M 53M Grade 400.
- Reinforcement bars designated (E) shall be epoxy coated.
- Lifting loops shall be 2 -12.7 mm φ strands (Fu=1860 MPa), as shown.
- All dimensions are in millimeters (mm) except as noted.

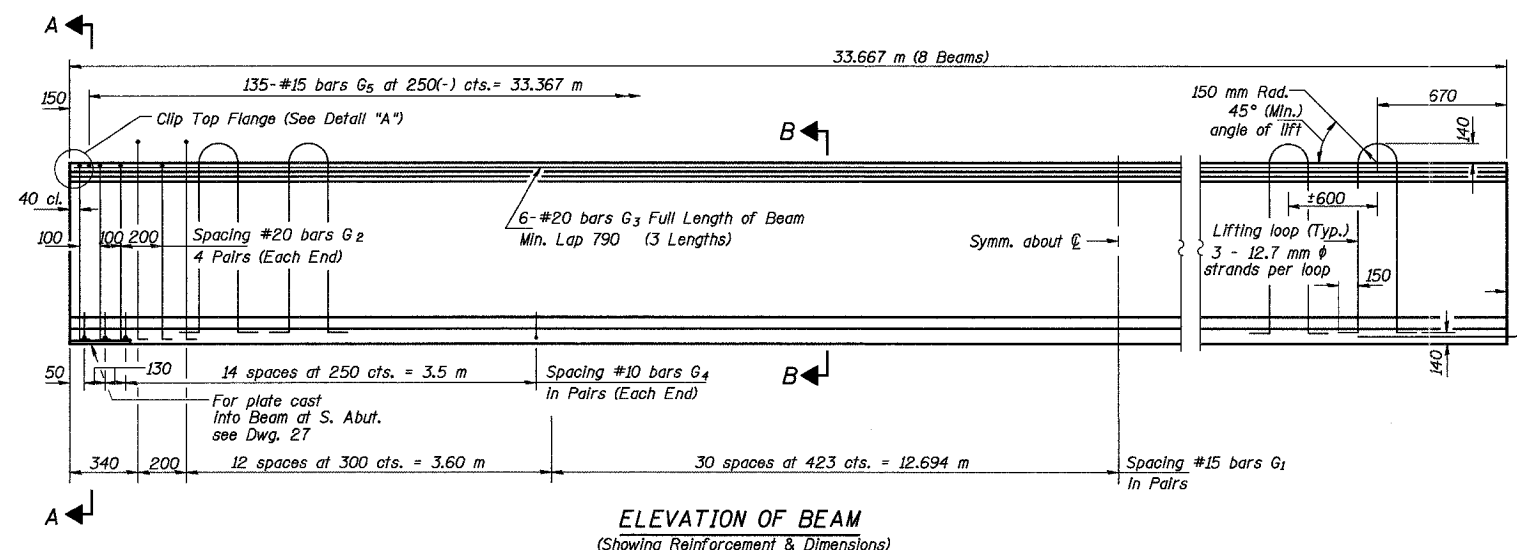
REVISION	DATE	DESCRIPTION	
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION 1067 mm PPC I-BEAM - SOUTH VAULTED BEAM NOS. 1-9 SPAN 1 RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183 PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS			
DRAWING NO.	SCALE	DATE	SHEET NO.
19	N.T.S.	2-18-03	19

Time: 10:02:09 AM
 Date: 11/19/2004
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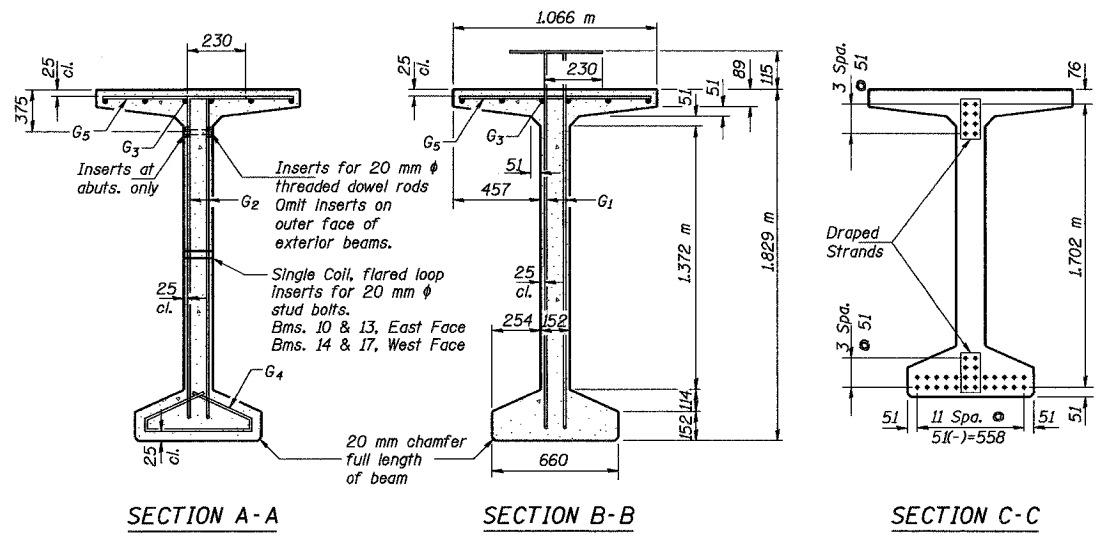
Designed by: AMK
 Checked by: WEE
 Drafted by: FTE/JMG
 Checked by: WEE

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	463	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

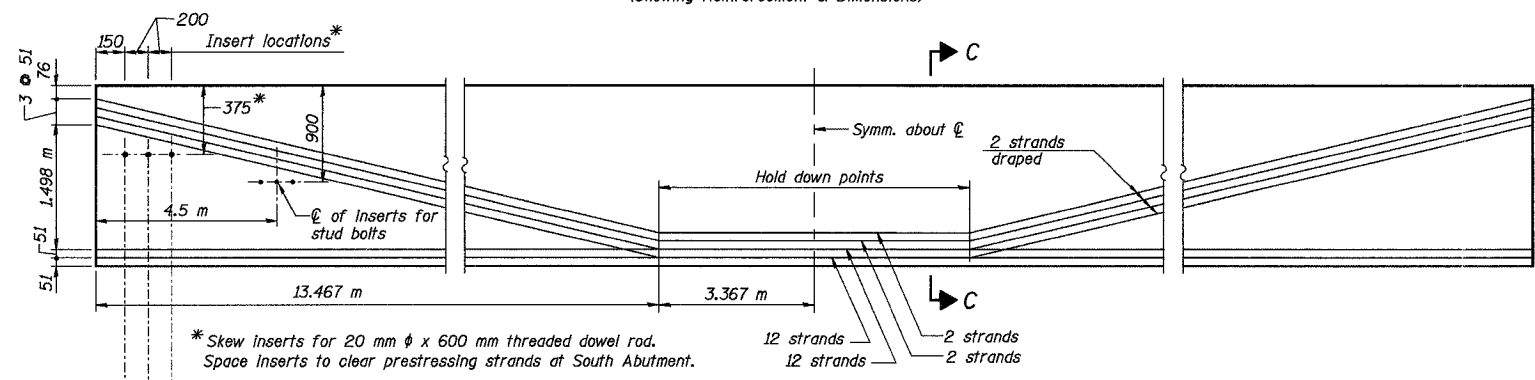
CONTRACT NO. 68200



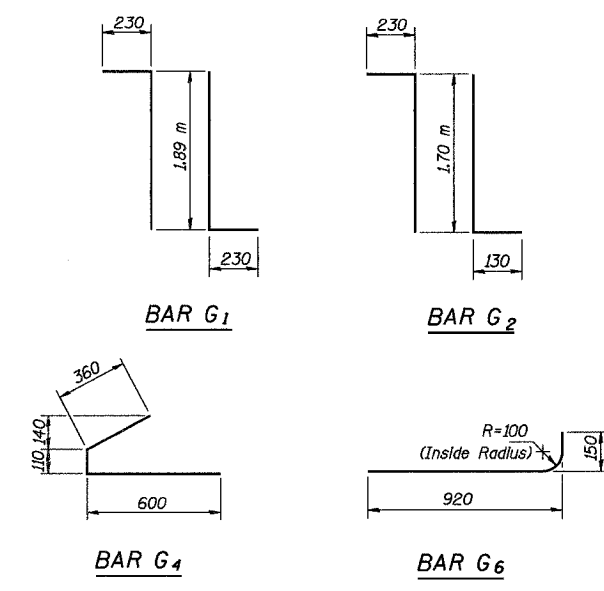
ELEVATION OF BEAM
(Showing Reinforcement & Dimensions)



SECTION A-A **SECTION B-B** **SECTION C-C**



ELEVATION OF BEAM
(Showing Prestressing Steel)
East Face Shown



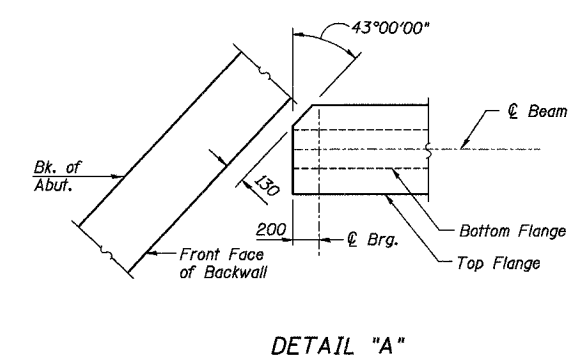
*** BAR LIST**

Bar	No.	Size	Length (m)	Shape
G1	174	#15	2.35	U
G2	16	#20	2.06	U
G3	18	#20	11.75	—
G4	68	#10	1.07	—
G5	135	#15	1.01	—
G6	3	#25	1.07	—

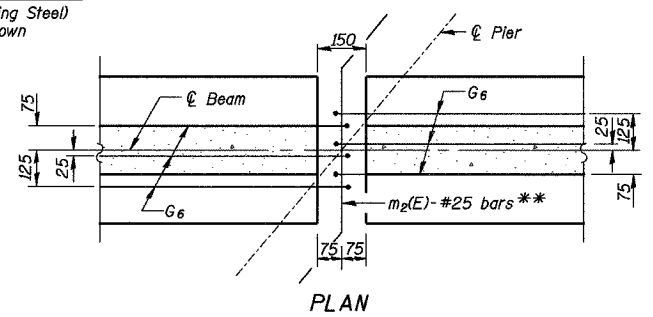
* For one beam only.

BILL OF MATERIAL

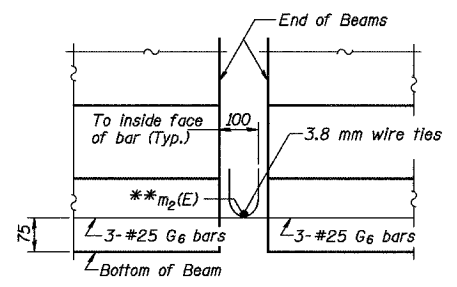
Item	Unit	Total
Furnishing and Erecting Precast Prestressed Concrete Bulb T-Beams, 1829 mm	m	269.34



DETAIL "A"



PLAN



ELEVATION

DETAIL "B"

** Tie #25 m2(E) with 3.8 mm wire tightly fastened to prevent any movement between bars.

Notes:

- All Inserts and threaded dowel rods for Inserts, reinforcing and Prestressing Steel, and other items which are cast into the Precast Concrete Bulb T-Beams shall be included in the contract unit price per meter of "Furnishing and Erecting Precast Prestressed Concrete Bulb T-Beams, 1829 mm".
- Inserts for 20 mm phi threaded dowel rods are to be two strut, coil type for interior beams and single coil, flared loop type for exterior beams.
- Prestressing steel shall be uncoated high strength, low relaxation 7-wire strand (Fu = 1860 MPa). The nominal diameter shall be 12.7 mm and the nominal cross-sectional area shall be 98.71 mm².
- Required release strength, f'ci, shall be 35 MPa.
- Non-prestressing steel shall conform to AASHTO designation M 31M, M 42M or M 53M Grade 400.
- Reinforcement bars designated (E) shall be epoxy coated.
- Lifting loops shall be 3 - 12.7 mm phi strands (Fu = 1860 MPa), as shown.
- All dimensions are in millimeters (mm) except as noted.

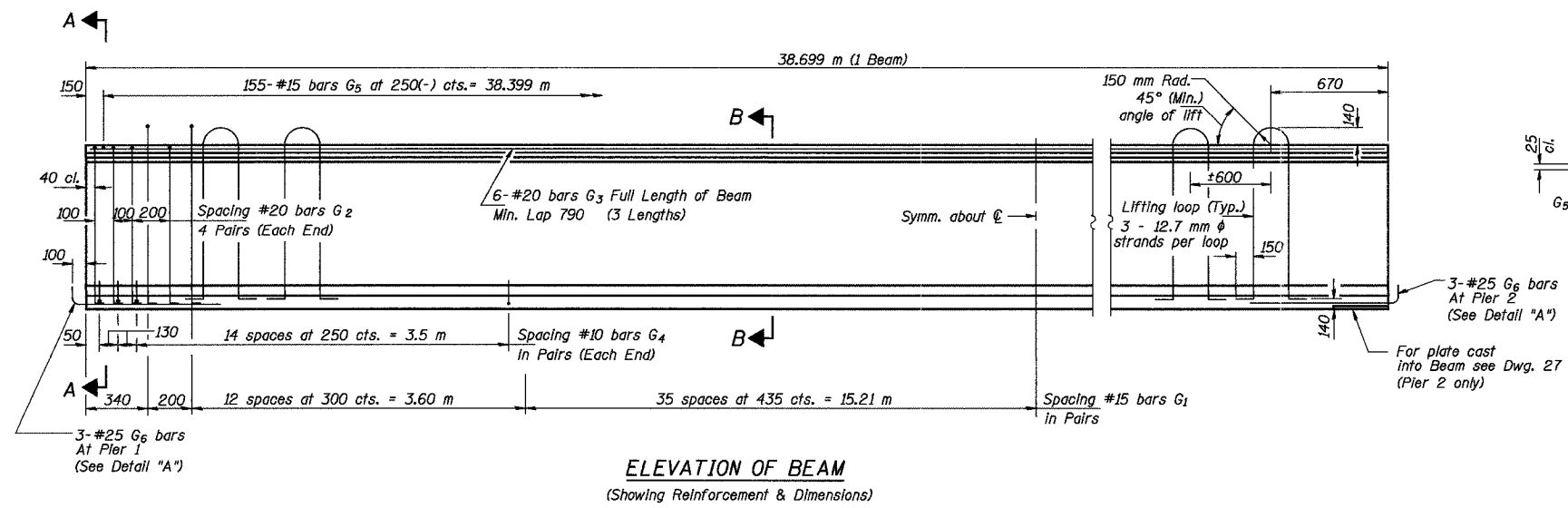
Date: 11/19/2004 Time: 09:02:26 AM File name: P:\643996\structure\ur\d\072-0183\Sheet\Tracings\SD00005-10720183.dgn

Designed by: AMK
Checked by: WEE
Drafted by: FTE/SC
Checked by: WEE

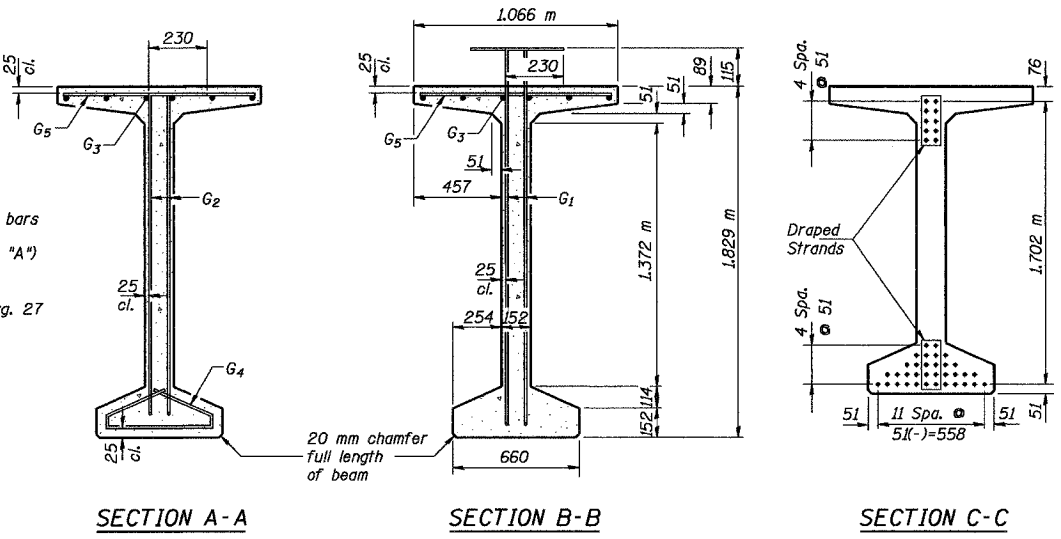
REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
1829 mm PPC BULB T-BEAM - BEAM NOS. 10-17 SPAN 2		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 20	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 20

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	464	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

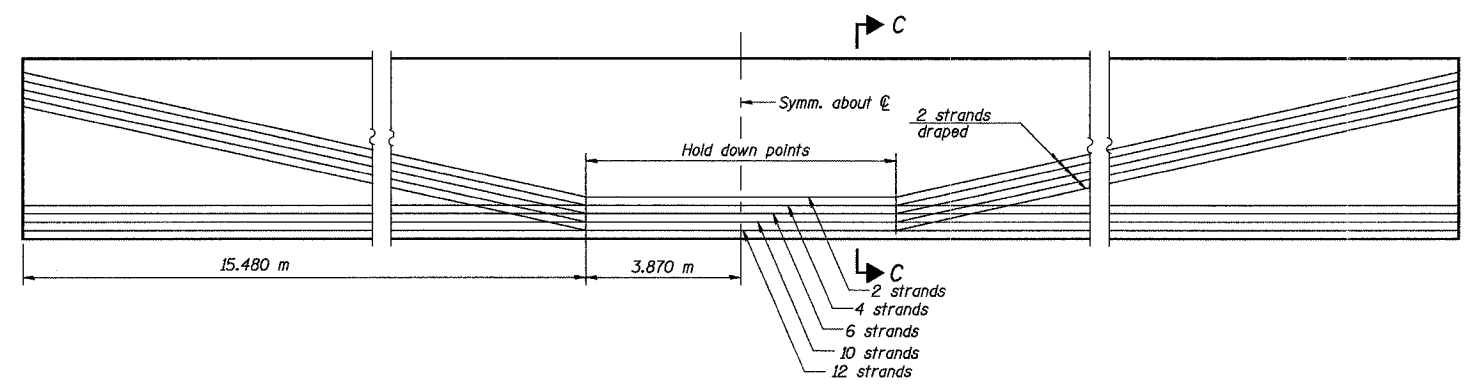
CONTRACT NO. 6R200



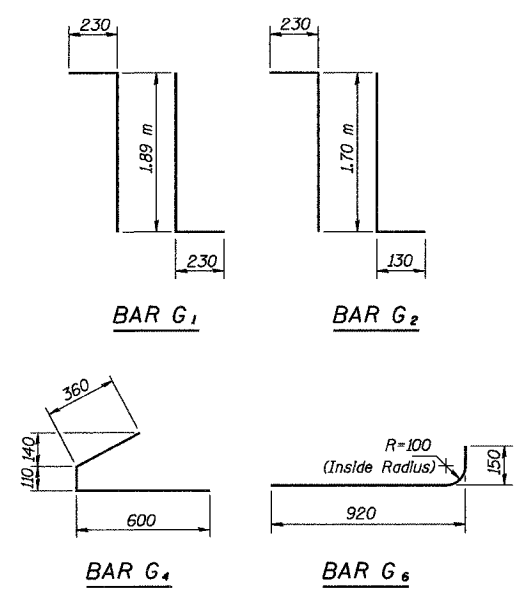
ELEVATION OF BEAM
(Showing Reinforcement & Dimensions)



SECTION A-A **SECTION B-B** **SECTION C-C**



ELEVATION OF BEAM
(Showing Prestressing Steel)



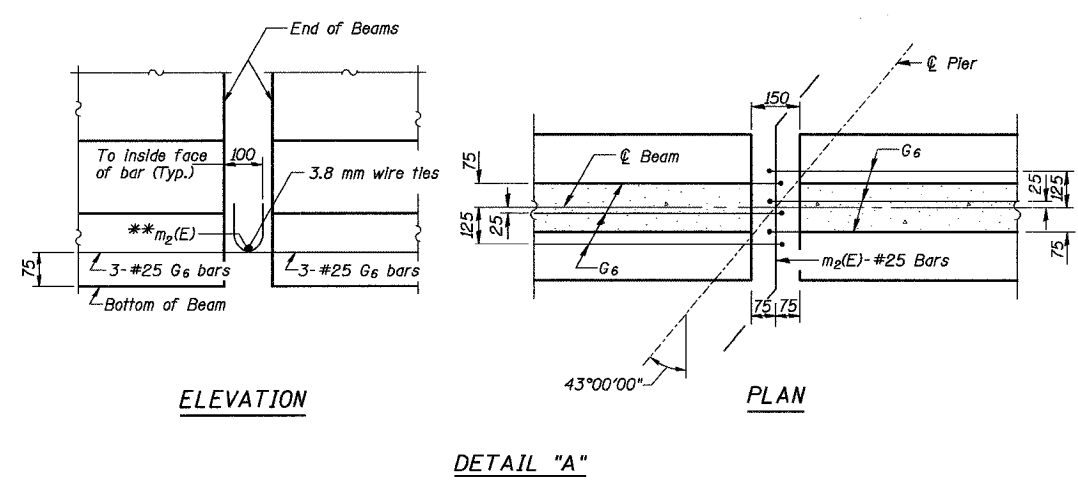
*** BAR LIST**

Bar	No.	Size	Length (m)	Shape
G1	194	#15	2.35	┘┘
G2	16	#20	2.06	┘┘
G3	18	#20	13.45	┘┘
G4	68	#10	1.07	┘┘
G5	155	#15	1.01	┘┘
G6	6	#25	1.07	┘┘

* For one beam only.

BILL OF MATERIAL

Item	Unit	Total
Furnishing and Erecting Precast Prestressed Concrete Bulb T-Beams, 1829 mm	m	38.70



ELEVATION

DETAIL "A"

** Tie #25 m₂(E) with 3.8 mm wire tightly fastened to prevent any movement between bars.

Notes:

- All reinforcing and Prestressing Steel, and other items which are cast into the Precast Concrete Bulb T-Beams shall be included in the contract unit price per meter of "Furnishing and Erecting Precast Prestressed Concrete Bulb T-Beams, 1829 mm".
- Prestressing steel shall be uncoated high strength, low relaxation 7-wire strand (F_u = 1860 MPa). The nominal diameter shall be 12.7 mm and the nominal cross-sectional area shall be 98.71 mm².
- Required release strength, f'ci, shall be 35 MPa.
- Non-prestressing steel shall conform to AASHTO designation M 31M, M 42M or M 53M Grade 400.
- Reinforcement bars designated (E) shall be epoxy coated.
- Lifting loops shall be 3 - 12.7 mm φ strands (F_u = 1860 MPa), as shown.
- All dimensions are in millimeters (mm) except as noted.

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION 1829 mm PPC BULB T-BEAM - FLARED BEAM NO. 10 SPAN 3 RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183 PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO.	SCALE	DATE
21	N.T.S.	2-18-03
		SHEET NO.
		21

Time: 10:02:45 AM

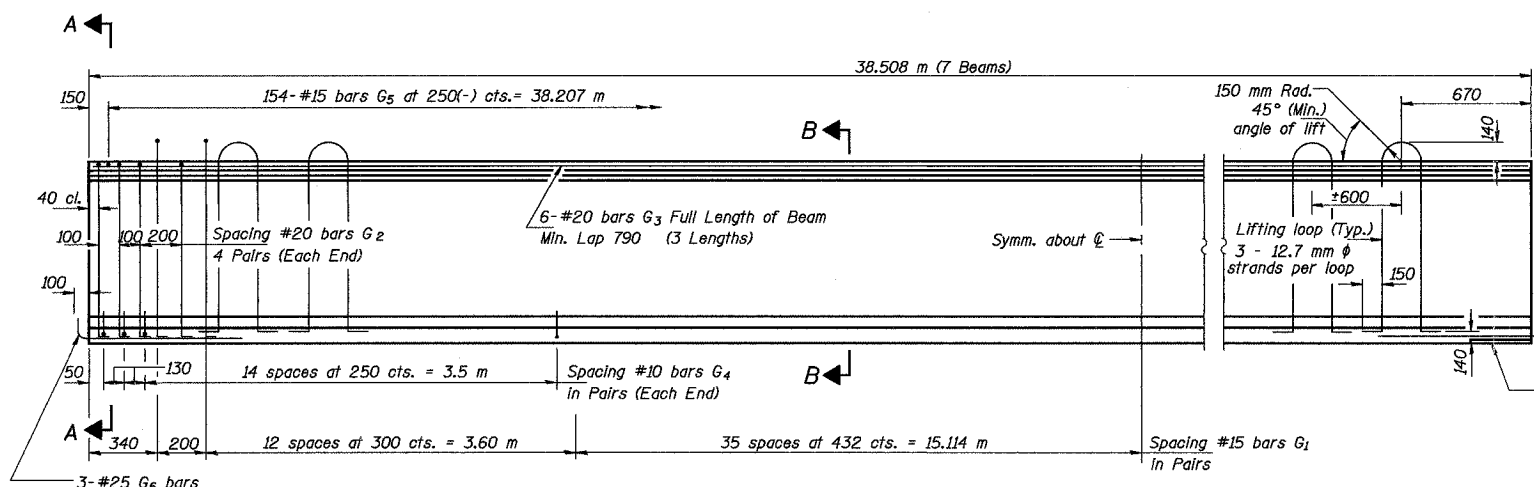
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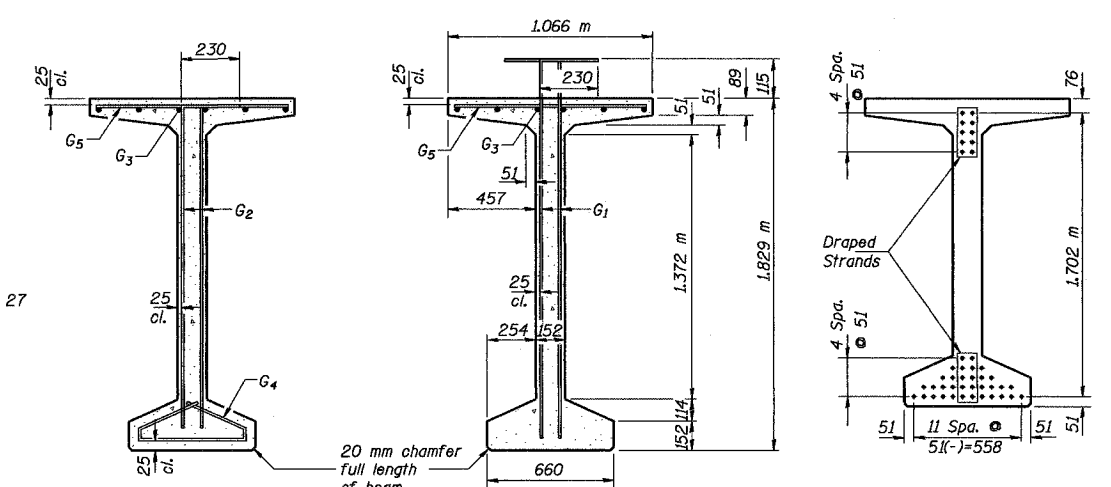
Designed by: AMK
 Checked by: WEE
 Drafted by: FTE/JMG
 Checked by: WEE

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	465	1360
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

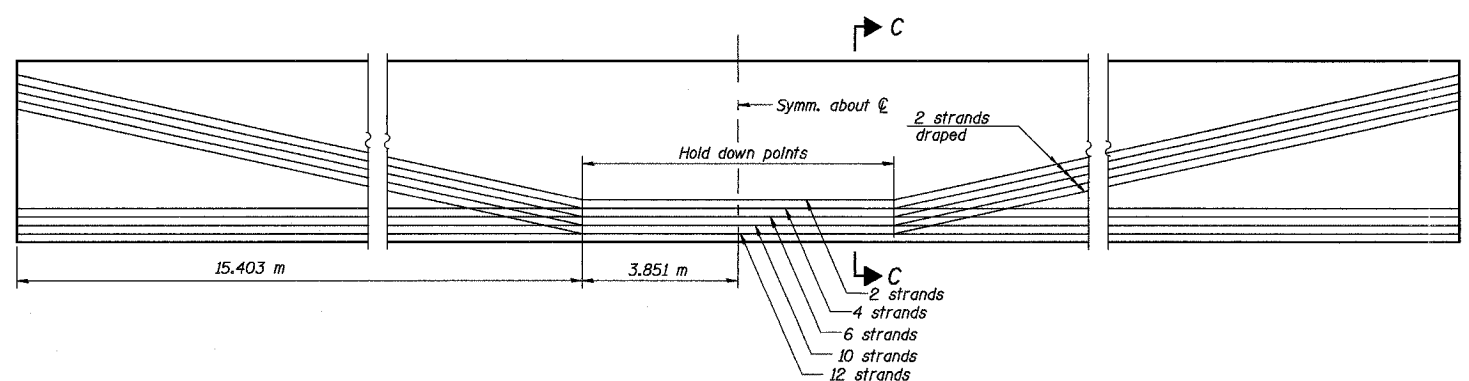
CONTRACT NO. 68200



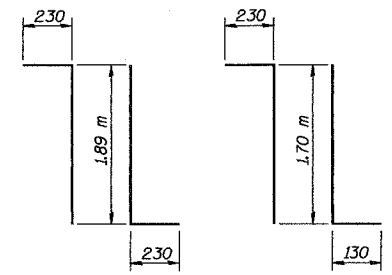
ELEVATION OF BEAM
(Showing Reinforcement & Dimensions)



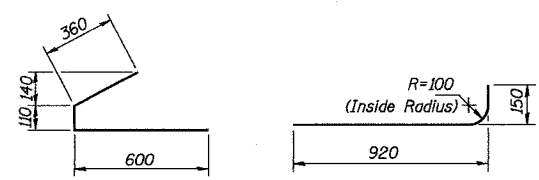
SECTION A-A **SECTION B-B** **SECTION C-C**



ELEVATION OF BEAM
(Showing Prestressing Steel)



BAR G1 **BAR G2**



BAR G4 **BAR G6**

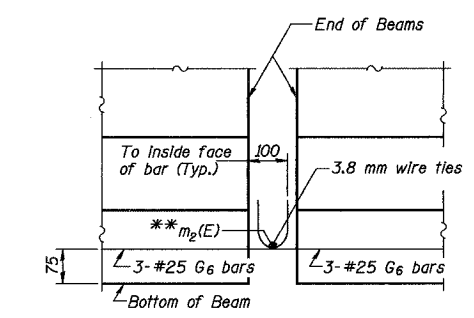
*** BAR LIST**

Bar	No.	Size	Length (m)	Shape
G1	194	#15	2.35	U
G2	16	#20	2.06	U
G3	18	#20	13.40	—
G4	68	#10	1.07	—
G5	154	#15	1.01	—
G6	6	#25	1.07	U

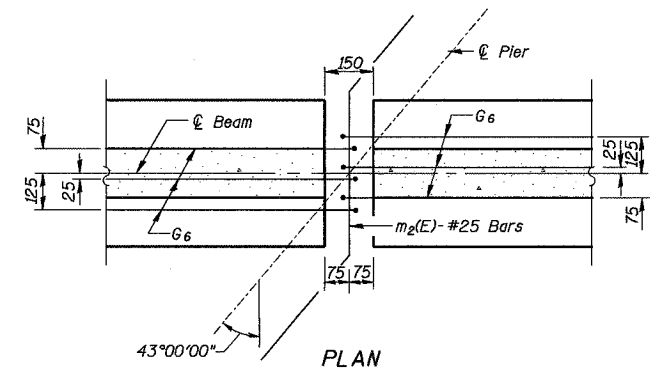
* For one beam only.

BILL OF MATERIAL

Item	Unit	Total
Furnishing and Erecting Precast Prestressed Concrete Bulb T-Beams, 1829 mm	m	269.56



ELEVATION



PLAN

DETAIL "A"

** Tie #25 m₂(E) with 3.8 mm wire tightly fastened to prevent any movement between bars.

NOTES

- All reinforcing and Prestressing Steel, and other items which are cast into the Precast Concrete Bulb T-Beams shall be included in the contract unit price per meter of "Furnishing and Erecting Precast Prestressed Concrete Bulb T-Beams, 1829 mm".
- Prestressing steel shall be uncoated high strength, low relaxation 7-wire strand (Fu = 1860 MPa). The nominal diameter shall be 12.7 mm and the nominal cross-sectional area shall be 98.71 mm².
- Required release strength, f'cl, shall be 35 MPa.
- Non-prestressing steel shall conform to AASHTO designation M 31M, M 42M or M 53M Grade 400.
- Reinforcement bars designated (E) shall be epoxy coated.
- Lifting loops shall be 3 - 12.7 mm strands (Fu = 1860 MPa), as shown.
- All dimensions are in millimeters (mm) except as noted.

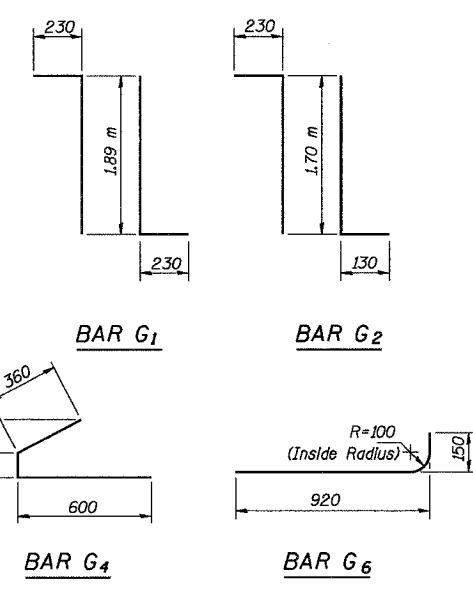
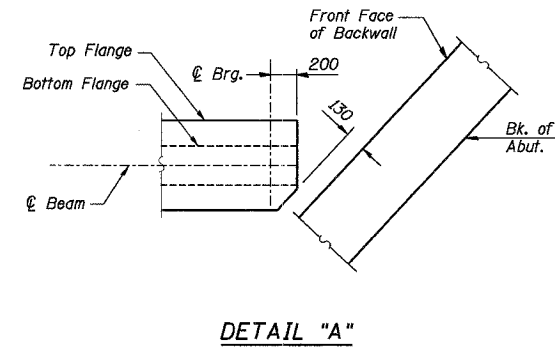
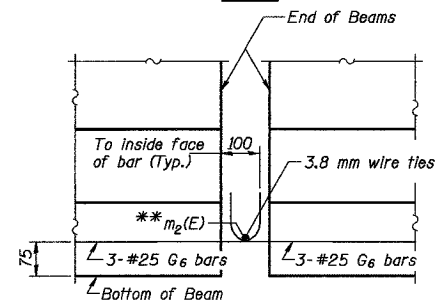
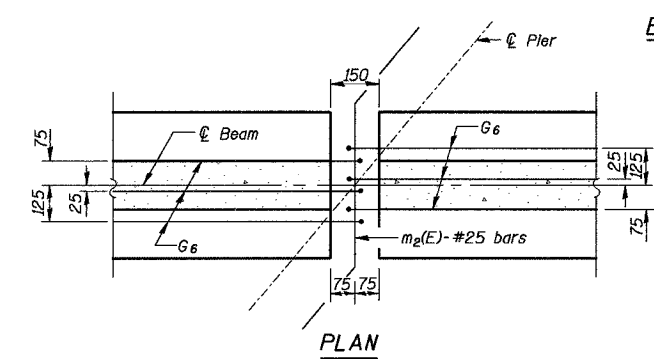
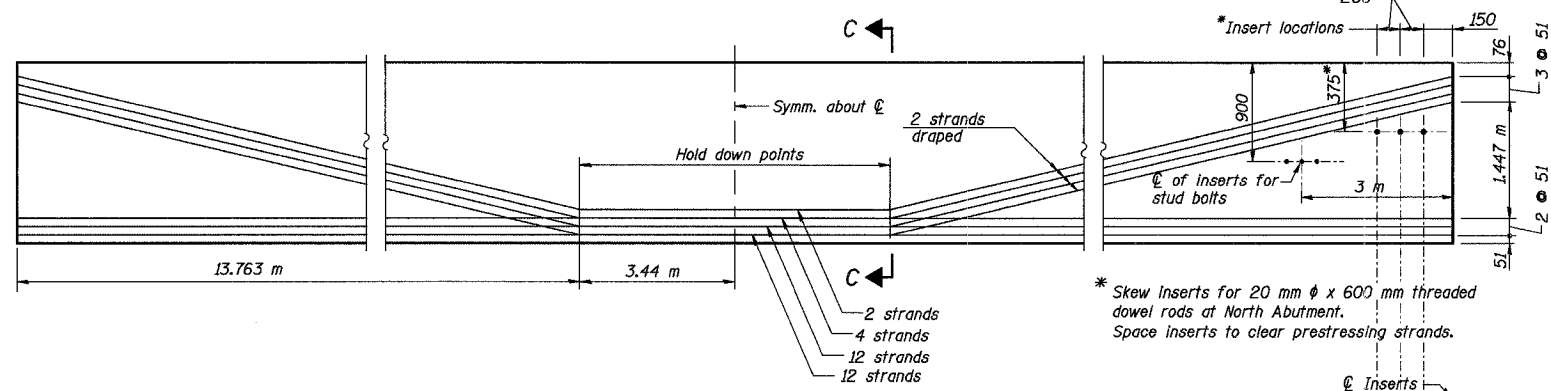
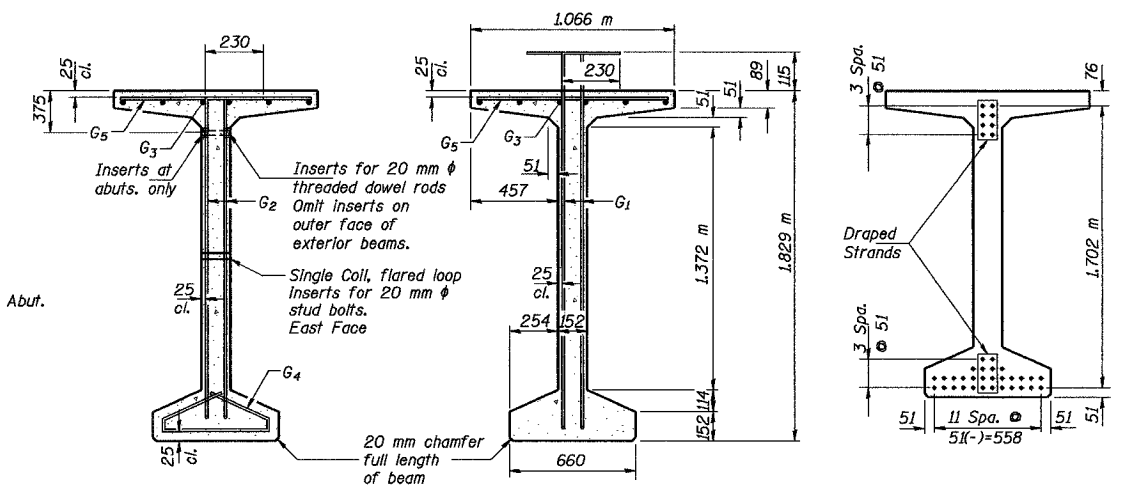
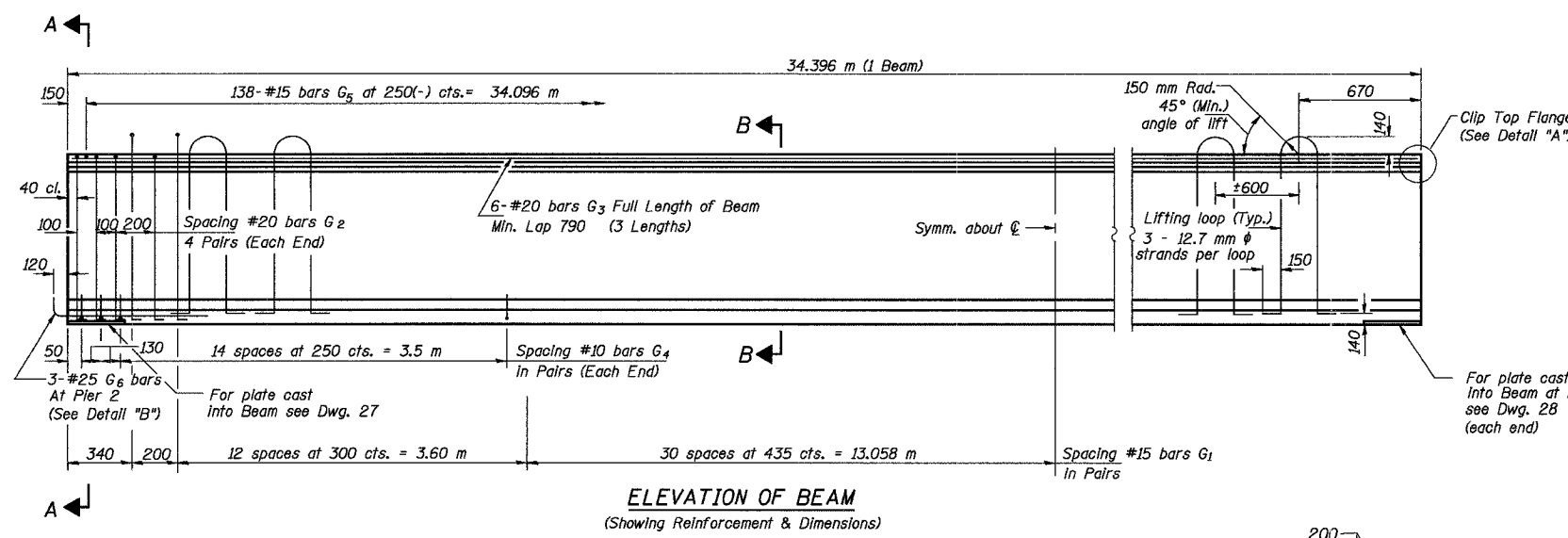
REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION 1829 mm PPC BULB T-BEAM BEAM NOS. 11-17 SPAN 3 RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183 PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO.	SCALE	DATE
22	N.T.S.	2-18-03
SHEET NO.	22	

Date: 11/23/2004
 Time: 02:50:40 PM
 File name: P:\643996\structure\072-0183\Sheet\A0720183.dgn

Designed by: AMK
 Checked by: WEE
 Drafted by: FTE/JMG
 Checked by: WEE

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	7/66	13/60
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

CONTRACT NO. 68200



*** BAR LIST**

Bar	No.	Size	Length (m)	Shape
G1	174	#15	2.35	TL
G2	16	#20	2.06	TL
G3	18	#20	12.00	---
G4	68	#10	1.07	---
G5	138	#15	1.01	---
G6	3	#25	1.07	---

* For one beam only.

BILL OF MATERIAL

Item	Unit	Total
Furnishing and Erecting Precast Prestressed Concrete Bulb T-Beams, 1829 mm	m	34.40

- Notes:**
- All inserts and threaded dowel rods for inserts, reinforcing and Prestressing Steel, and other items which are cast into the Precast Concrete Bulb T-Beams shall be included in the contract unit price per meter of "Furnishing and Erecting Precast Prestressed Concrete Bulb T-Beams, 1829 mm".
 - Inserts for 20 mm ϕ threaded dowel rods are to be two strut, coil type for interior beams and single coil, flared loop type for exterior beams.
 - Prestressing steel shall be uncoated high strength, low relaxation 7-wire strand ($F_u = 1860$ MPa). The nominal diameter shall be 12.7 mm and the nominal cross-sectional area shall be 98.71 mm².
 - Required release strength, f'_{ci} , shall be 35 MPa.
 - Non-prestressing steel shall conform to AASHTO designation M 31M, M 42M or M 53M Grade 400.
 - Reinforcement bars designated (E) shall be epoxy coated.
 - Lifting loops shall be 3 - 12.7 mm ϕ strands ($F_u = 1860$ MPa), as shown.
 - All dimensions are in millimeters (mm) except as noted.

REVISION	DATE	DESCRIPTION	
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION 1829 mm PPC BULB T-BEAM - FLARED BEAM NO. 10 SPAN 4 RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183 PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS			
DRAWING NO.	SCALE	DATE	SHEET NO.
23	N.T.S.	2-18-03	23

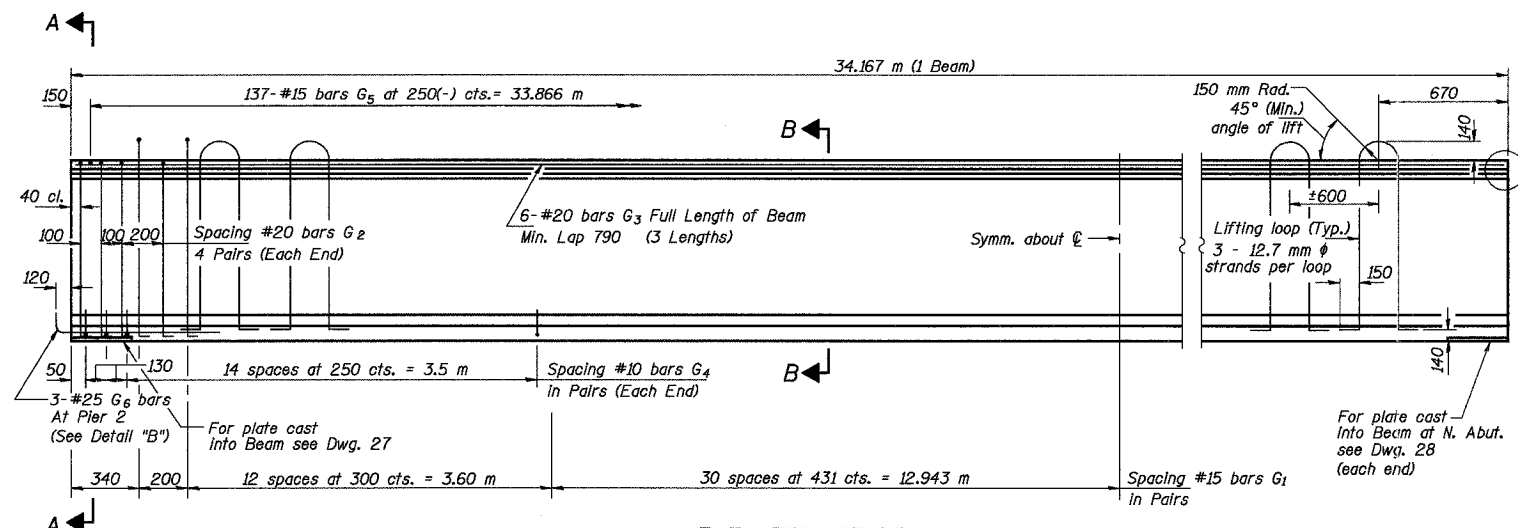
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Designed by: AMK
 Checked by: WEE
 Drafted by: FTE/JMG
 Checked by: WEE

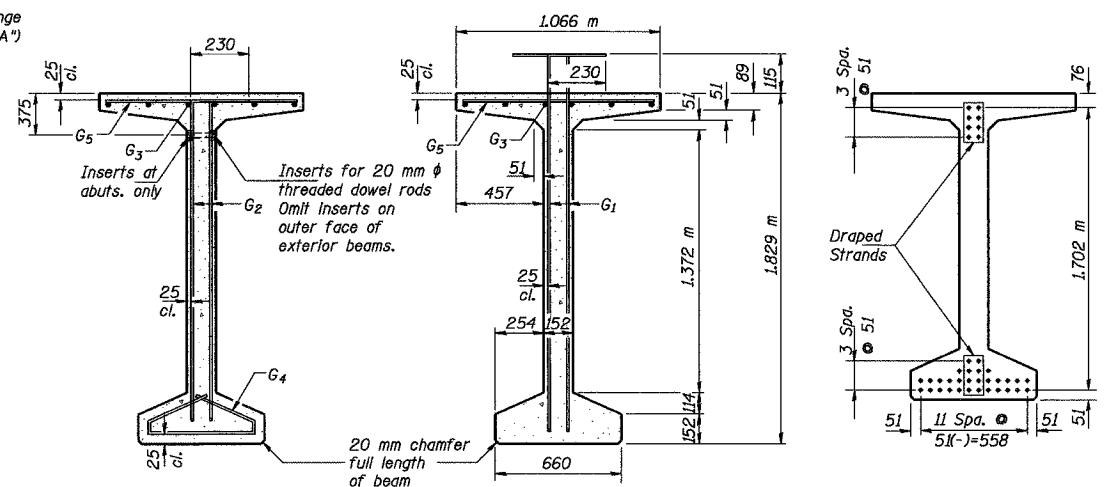
** Tie #25 m2(E) with 3.8 mm wire tightly fastened to prevent any movement between bars.

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	4/67	1360
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

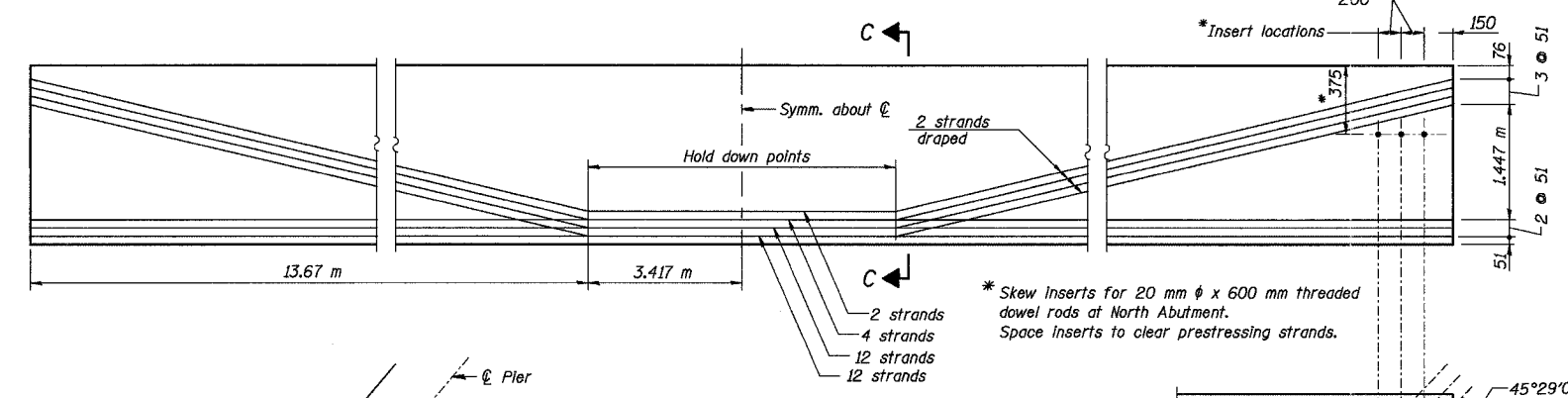
CONTRACT NO. 68200



ELEVATION OF BEAM
(Showing Reinforcement & Dimensions)



SECTION A-A **SECTION B-B** **SECTION C-C**

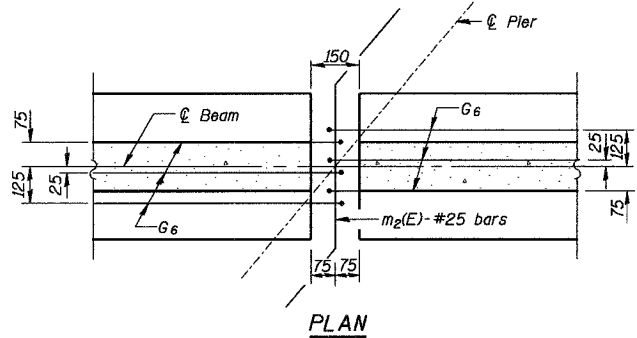
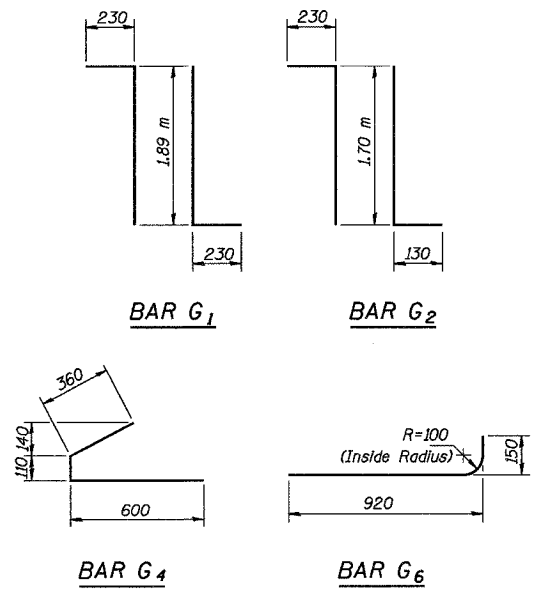


ELEVATION OF BEAM
(Showing Prestressing Steel)

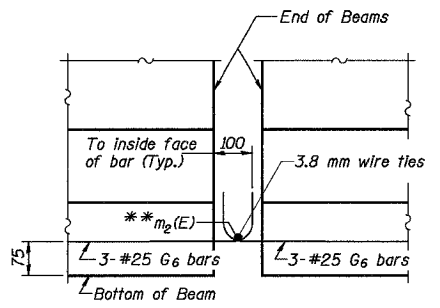
*** BAR LIST**

Bar	No.	Size	Length (m)	Shape
G1	174	#15	2.35	TL
G2	16	#20	2.06	TL
G3	18	#20	11.95	—
G4	68	#10	1.07	—
G5	137	#15	1.01	—
G6	3	#25	1.07	—

* For one beam only.

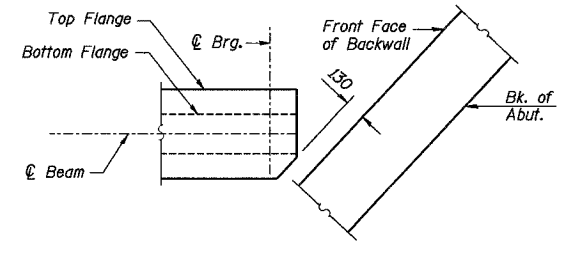


PLAN



ELEVATION

DETAIL "B"



DETAIL "A"

- Notes:**
- All Inserts and threaded dowel rods for Inserts, reinforcing and Prestressing Steel, and other items which are cast into the Precast Concrete Bulb T-Beams shall be included in the contract unit price per meter of "Furnishing and Erecting Precast Prestressed Concrete Bulb T-Beams, 1829 mm".
 - Inserts for 20 mm ϕ threaded dowel rods are to be two strut, coll type for interior beams and single coll, flared loop type for exterior beams.
 - Prestressing steel shall be uncoated high strength, low relaxation 7-wire strand ($F_u = 1860$ MPa). The nominal diameter shall be 12.7 mm and the nominal cross-sectional area shall be 98.71 mm².
 - Required release strength, f'_{ci} , shall be 35 MPa.
 - Non-prestressing steel shall conform to AASHTO designation M 31M, M 42M or M 53M Grade 400.
 - Reinforcement bars designated (E) shall be epoxy coated.
 - Lifting loops shall be 3 - 12.7 mm ϕ strands ($F_u = 1860$ MPa), as shown.
 - All dimensions are in millimeters (mm) except as noted.

BILL OF MATERIAL

Item	Unit	Total
Furnishing and Erecting Precast Prestressed Concrete Bulb T-Beams, 1829 mm	m	34.17

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION 1829 mm PPC BULB T-BEAM - FLARED BEAM NO. 11 SPAN 4 RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183 PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO.	SCALE	DATE
24	N.T.S.	2-18-03
		SHEET NO.
		24

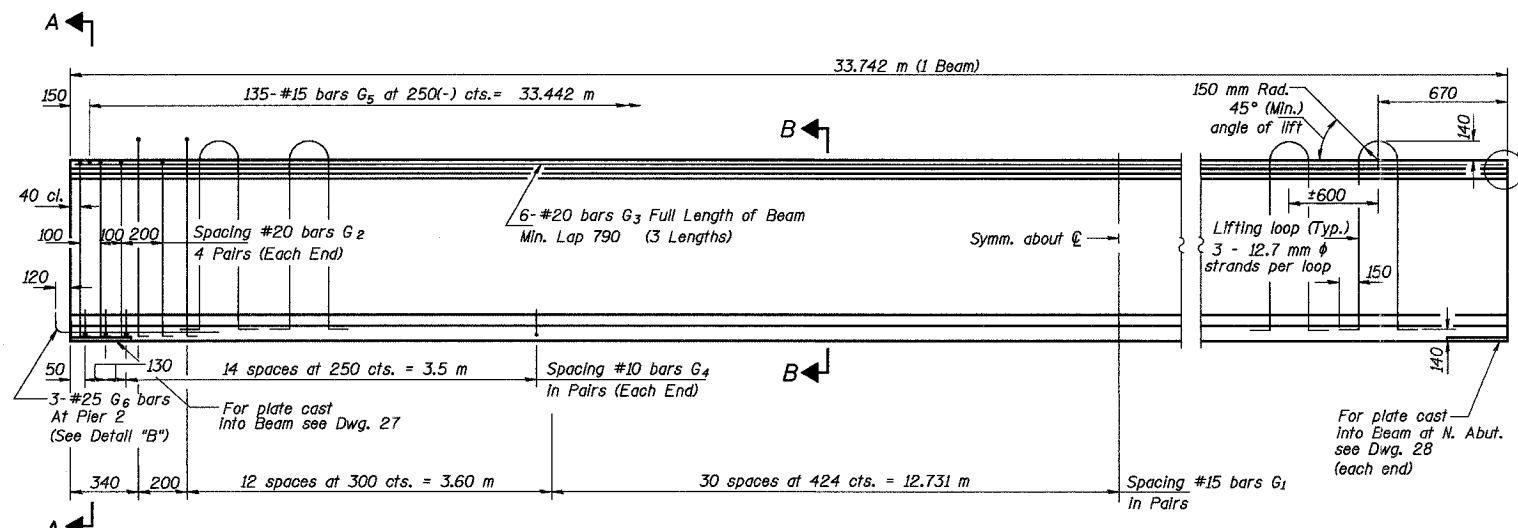
Date: 11/19/2004
 Time: 10:03:57 AM
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Designed by: AMK
 Checked by: WEE
 Drafted by: FTE/JMG
 Checked by: WEE

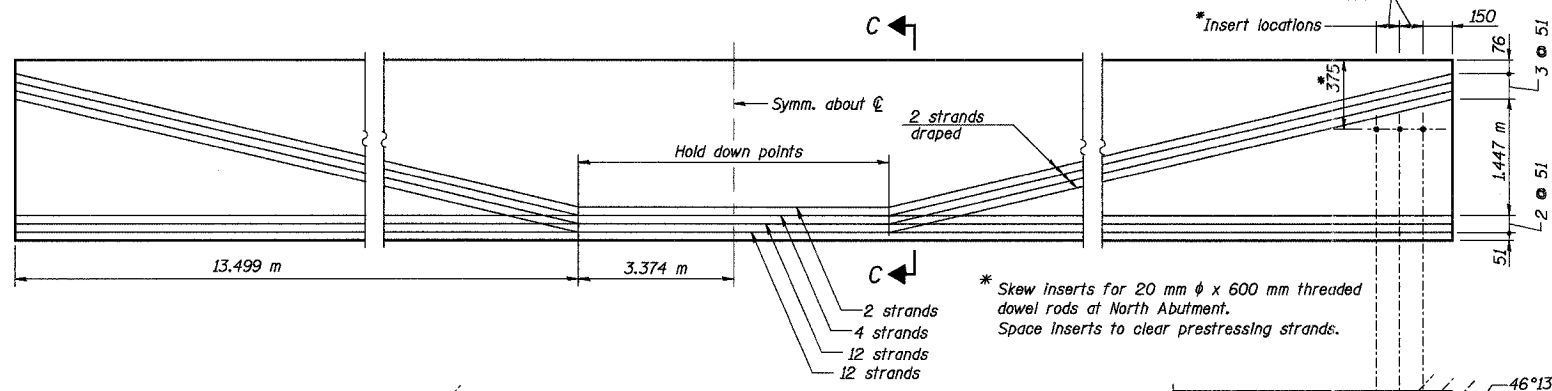
** Tie #25 m₂(E) with 3.8 mm wire tightly fastened to prevent any movement between bars.

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	468	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

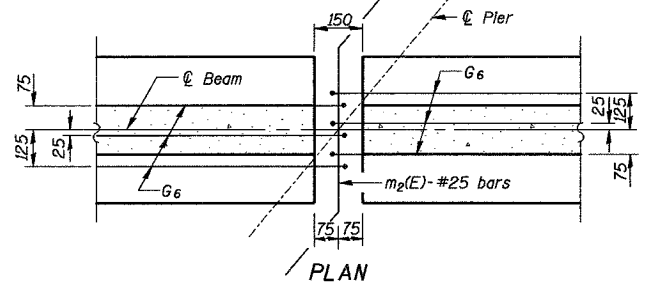
CONTRACT NO. 68200



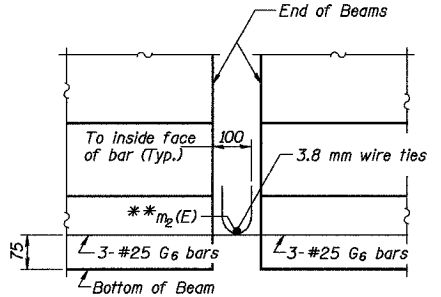
ELEVATION OF BEAM
(Showing Reinforcement & Dimensions)



ELEVATION OF BEAM
(Showing Prestressing Steel)

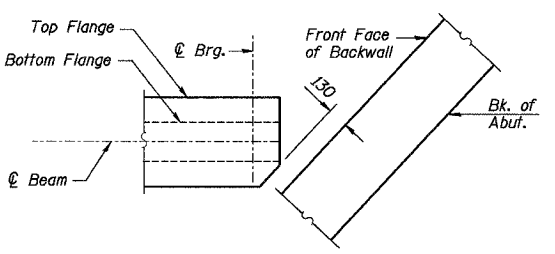


PLAN

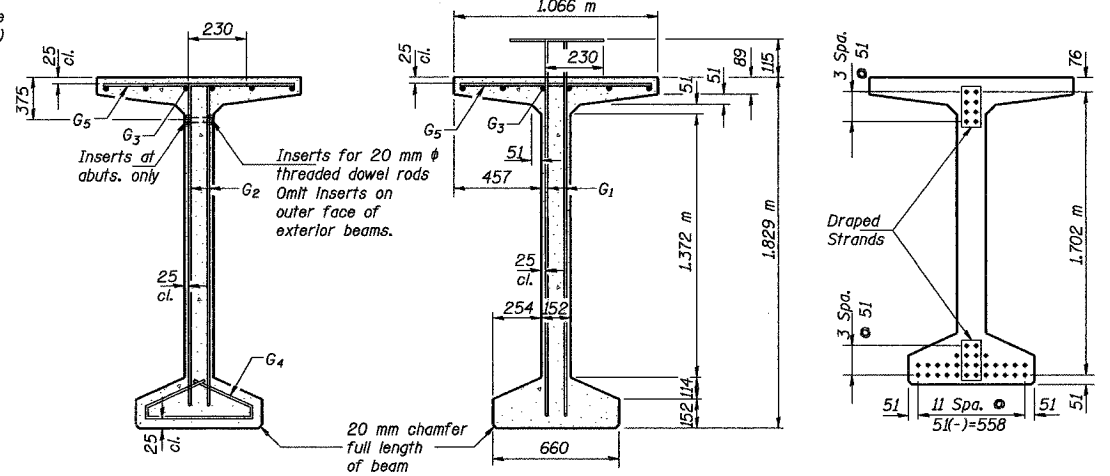


ELEVATION
DETAIL "B"

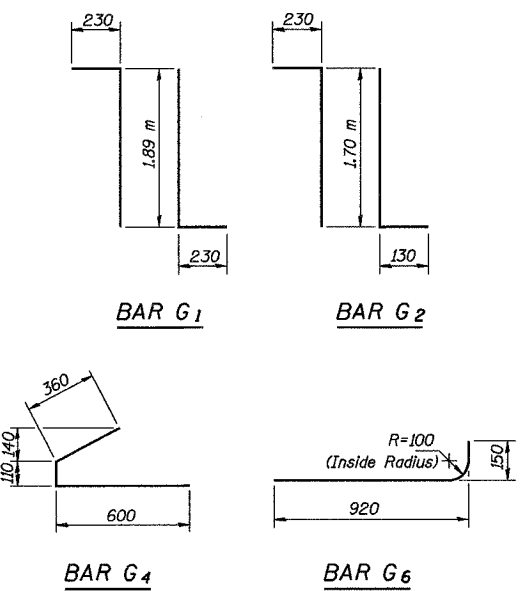
** Tie #25 m₂(E) with 3.8 mm wire tightly fastened to prevent any movement between bars.



DETAIL "A"



SECTION A-A **SECTION B-B** **SECTION C-C**



BAR G1 **BAR G2**
BAR G4 **BAR G6**

*** BAR LIST**

Bar	No.	Size	Length (m)	Shape
G ₁	174	#15	2.35	TL
G ₂	16	#20	2.06	TL
G ₃	18	#20	11.80	—
G ₄	68	#10	1.07	—
G ₅	135	#15	1.01	—
G ₆	3	#25	1.07	—

* For one beam only.

BILL OF MATERIAL

Item	Unit	Total
Furnishing and Erecting Precast Prestressed Concrete Bulb T-Beams, 1829 mm	m	33.74

- Notes:**
- All inserts and threaded dowel rods for inserts, reinforcing and Prestressing Steel, and other items which are cast into the Precast Concrete Bulb T-Beams shall be included in the contract unit price per meter of "Furnishing and Erecting Precast Prestressed Concrete Bulb T-Beams, 1829 mm".
 - Inserts for 20 mm diameter threaded dowel rods are to be two strut, coil type for interior beams and single coil, flared loop type for exterior beams.
 - Prestressing steel shall be uncoated high strength, low relaxation 7-wire strand (F_u = 1860 MPa). The nominal diameter shall be 12.7 mm and the nominal cross-sectional area shall be 98.71 mm².
 - Required release strength, f'_{ci}, shall be 35 MPa.
 - Non-prestressing steel shall conform to AASHTO designation M 31M, M 42M or M 53M Grade 400.
 - Reinforcement bars designated (E) shall be epoxy coated.
 - Lifting loops shall be 3 - 12.7 mm diameter strands (F_u = 1860 MPa), as shown.
 - All dimensions are in millimeters (mm) except as noted.

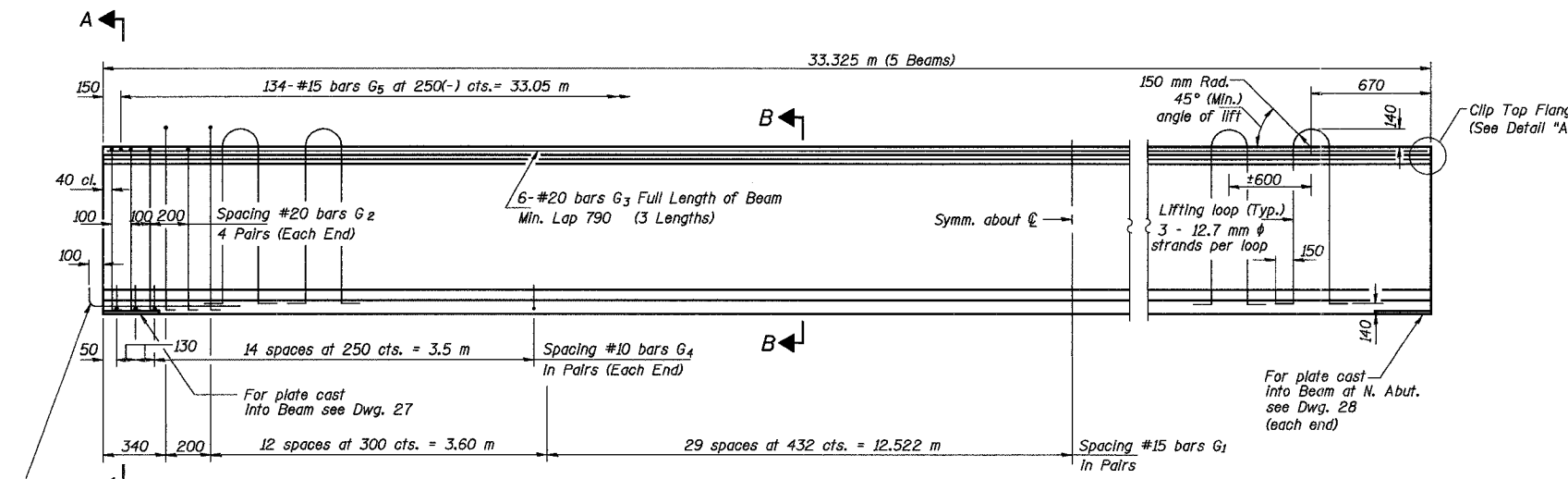
REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
1829 mm PPC BULB T-BEAM - FLARED BEAM NO. 12 SPAN 4		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3		
F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 25	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 25

Date: 11/19/2004
 File name: F:\43996\structure\072-0183\Sheet\Tracings\SD0000-10720183.dgn

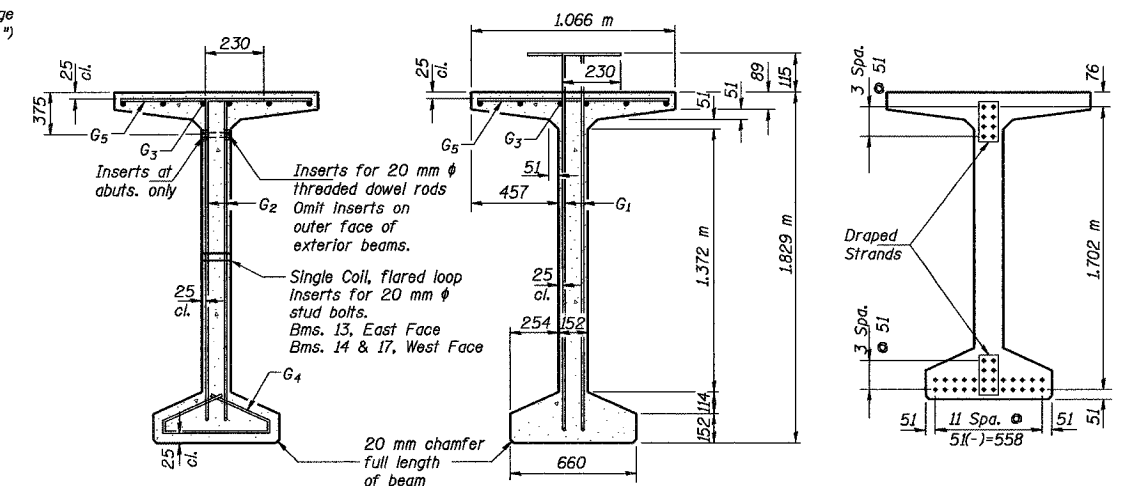
ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	469	1360
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

CONTRACT NO. 68200

Date: 11/19/2004 Time: 09:04:45 AM File: P:\643996\Structure\072-0183\Sheet\Tracings\SD0001-10720183.dgn



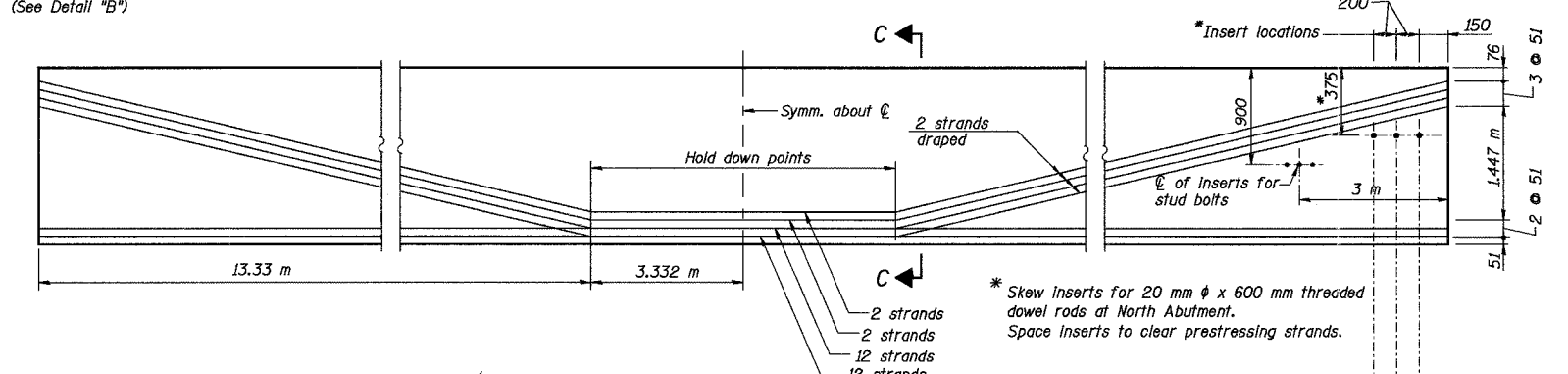
ELEVATION OF BEAM
(Showing Reinforcement & Dimensions)



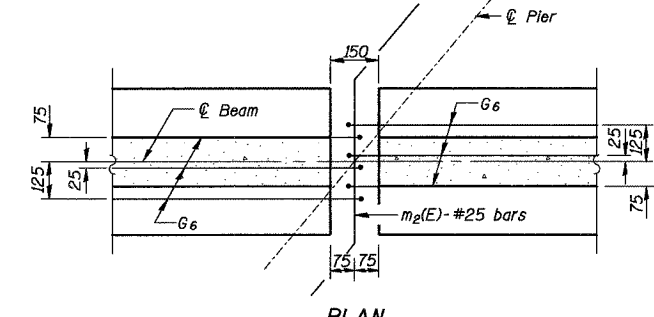
SECTION A-A

SECTION B-B

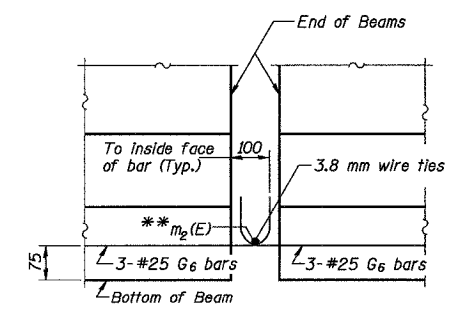
SECTION C-C



ELEVATION OF BEAM
(Showing Prestressing Steel)
East Face Shown

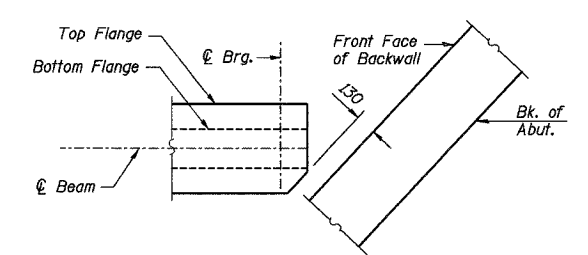


PLAN

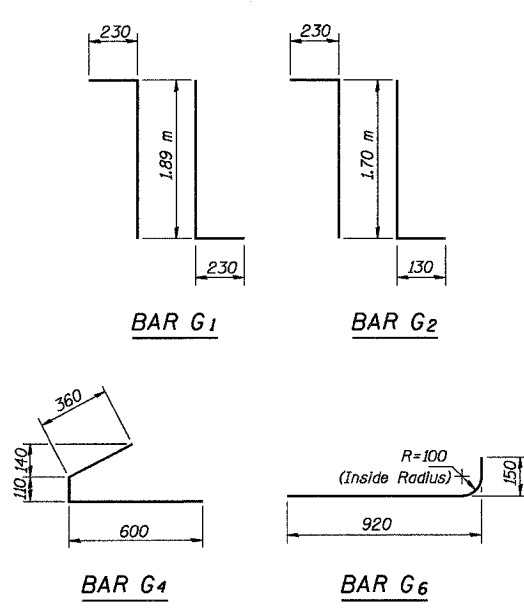


ELEVATION

DETAIL "B"



DETAIL "A"



BAR G1

BAR G2

BAR G4

BAR G6

*** BAR LIST**

Bar	No.	Size	Length (m)	Shape
G1	170	#15	2.35	U
G2	16	#20	2.06	U
G3	18	#20	11.65	—
G4	68	#10	1.07	—
G5	134	#15	1.01	—
G6	3	#25	1.07	—

* For one beam only.

BILL OF MATERIAL

Item	Unit	Total
Furnishing and Erecting Precast Prestressed Concrete Bulb T-Beams, 1829 mm	m	166.62

Notes:

- All Inserts and threaded dowel rods for Inserts, reinforcing and Prestressing Steel, and other items which are cast into the Precast Concrete Bulb T-Beams shall be included in the contract unit price per meter of "Furnishing and Erecting Precast Prestressed Concrete Bulb T-Beams, 1829 mm".
- Inserts for 20 mm threaded dowel rods are to be two strut, coil type for interior beams and single coil, flared loop type for exterior beams.
- Prestressing steel shall be uncoated high strength, low relaxation 7-wire strand (Fu = 1860 MPa). The nominal diameter shall be 12.7 mm and the nominal cross-sectional area shall be 98.71 mm².
- Required release strength, f'ci, shall be 35 MPa.
- Non-prestressing steel shall conform to AASHTO designation M 31M, M 42M or M 53M Grade 400.
- Reinforcement bars designated (E) shall be epoxy coated.
- Lifting loops shall be 3 - 12.7 mm phi strands (Fu = 1860 MPa), as shown.
- All dimensions are in millimeters (mm) except as noted.

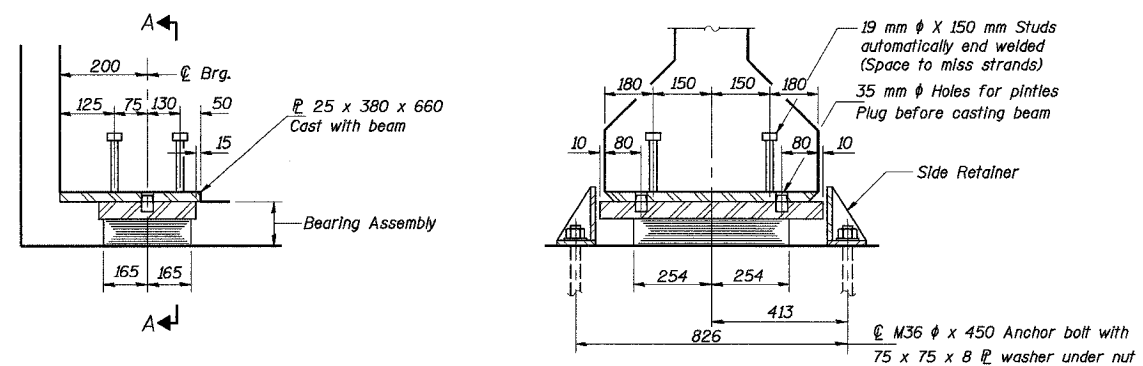
Designed by: AMK
Checked by: WEE
Drafted by: FTE/JMG
Checked by: WEE

** Tie #25 m2(E) with 3.8 mm wire tightly fastened to prevent any movement between bars.

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
1829 mm PPC BULB T-BEAM - BEAM NOS. 13-17 SPAN 4		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3		
F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 26	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 26

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	470	1300
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200

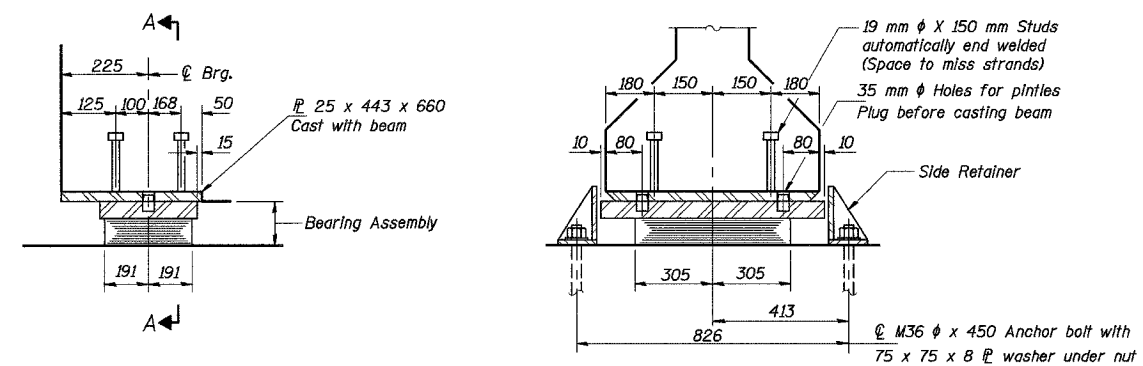


ELEVATION AT S. ABUT.

SECTION A-A

TYPE I ELASTOMERIC EXP. BRG.

(S. Abut. - Span 2)

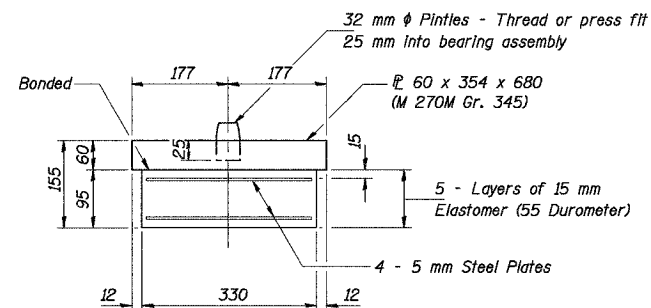


ELEVATION

SECTION A-A

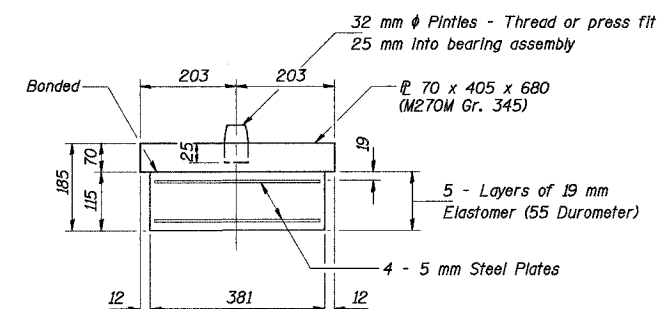
TYPE I ELASTOMERIC EXP. BRG.

(Pier 2 - Span 3 & Pier 2 - Span 4)



BEARING ASSEMBLY (S. ABUT.)

Note: Shim plates shall not be placed under Bearing Assembly.



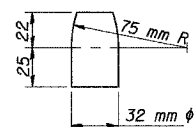
BEARING ASSEMBLY (PIER 2)

Note: Shim plates shall not be placed under Bearing Assembly.

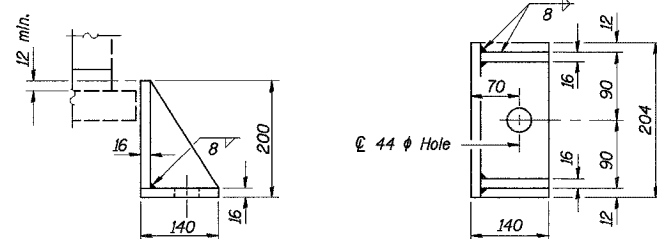
BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type I	Each	24

Notes:
After beams have been erected holes at expansion bearings shall be drilled and anchor bolts grouted in place.
See sheet 47 for anchor bolt installation.
All dimensions are in millimeters (mm) except as noted.



PINTLE



SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates. Mass included with structural steel.

Designed by: DL
Checked by: AK
Drafted by: JV
Checked by: AK

Time: 10:05:04 AM

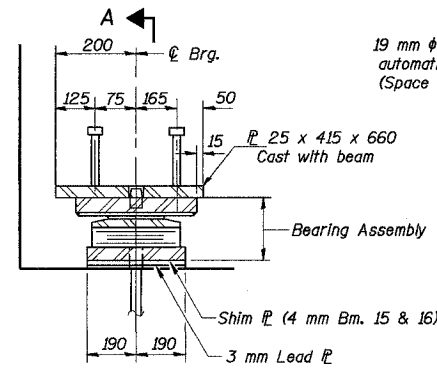
Date: 1/19/2004

File name: P:\643996\structure\072-0183\Sheet\Tracings\SD0004-1A072083.dgn

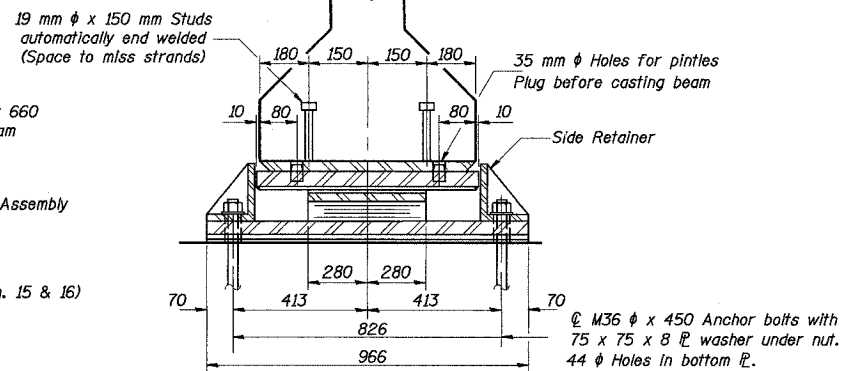
REVISION	DATE	DESCRIPTION	
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION EXPANSION BEARINGS SOUTH ABUTMENT & PIER 2 RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183 PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS			
DRAWING NO.	SCALE	DATE	SHEET NO.
27	N.T.S.	2-18-03	27

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	471	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200

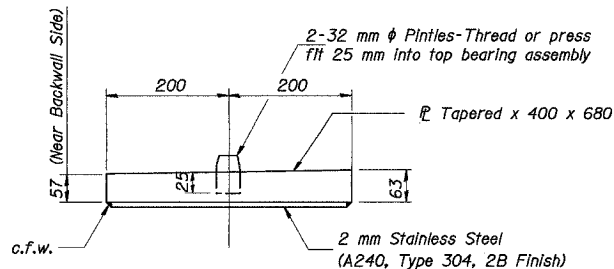


ELEVATION AT N. ABUT.

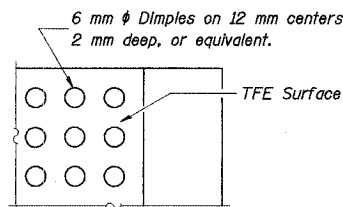


SECTION A-A

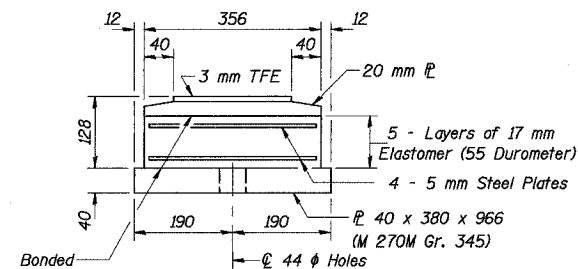
TYPE II TFE ELASTOMERIC EXP. BRG.



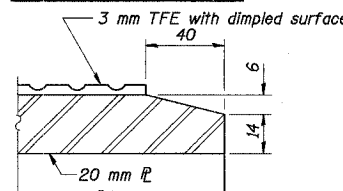
TOP BEARING ASSEMBLY



PLAN-TFE SURFACE



BOTTOM BEARING ASSEMBLY



SECTION THRU TFE

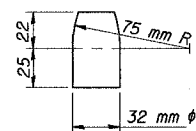
Note: The 3 mm TFE sheet shall be bonded directly to the top steel plate with a two-component, medium viscosity epoxy resin, conforming to the requirements of the Federal Specification MMM-A-134, Type I. The bond agent shall be applied on the full area of the contact surfaces.

Bonding of 3 mm TFE sheet during vulcanizing process will be permitted provided the process and method of adjusting assembly height is approved by the Engineer.

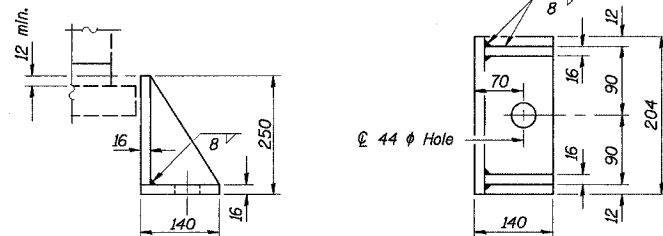
BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type II	Each	8

Notes:
After beams have been erected holes at expansion bearings shall be drilled and anchor bolts grouted in place.
See sheet 47 for anchor bolt installation.
All dimensions are in millimeters (mm) except as noted.

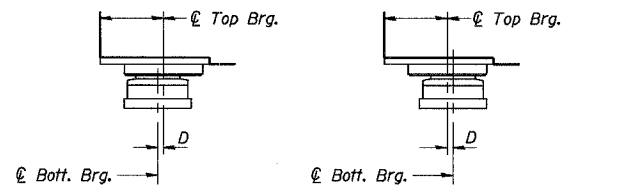


PINTLE



SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.
Mass included with structural steel.



BELOW 10 °C. (Move bott. brg. away from fixed brg.)
ABOVE 10 °C. (Move bott. brg. toward fixed brg.)

SETTING ANCHOR BOLTS AT EXP. BRG.

D=1 mm per each 10 m of expansion for every 8 °C temp. change from the normal temp. of 10 °C.

Designed by:	DL
Checked by:	AK
Drafted by:	JV
Checked by:	AK

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION EXPANSION BEARINGS NORTH ABUTMENT RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183 PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO.	SCALE	DATE
28	N.T.S.	2-18-03
SHEET NO.		
28		

Date: 11/23/2004 Time: 02:50:12 PM

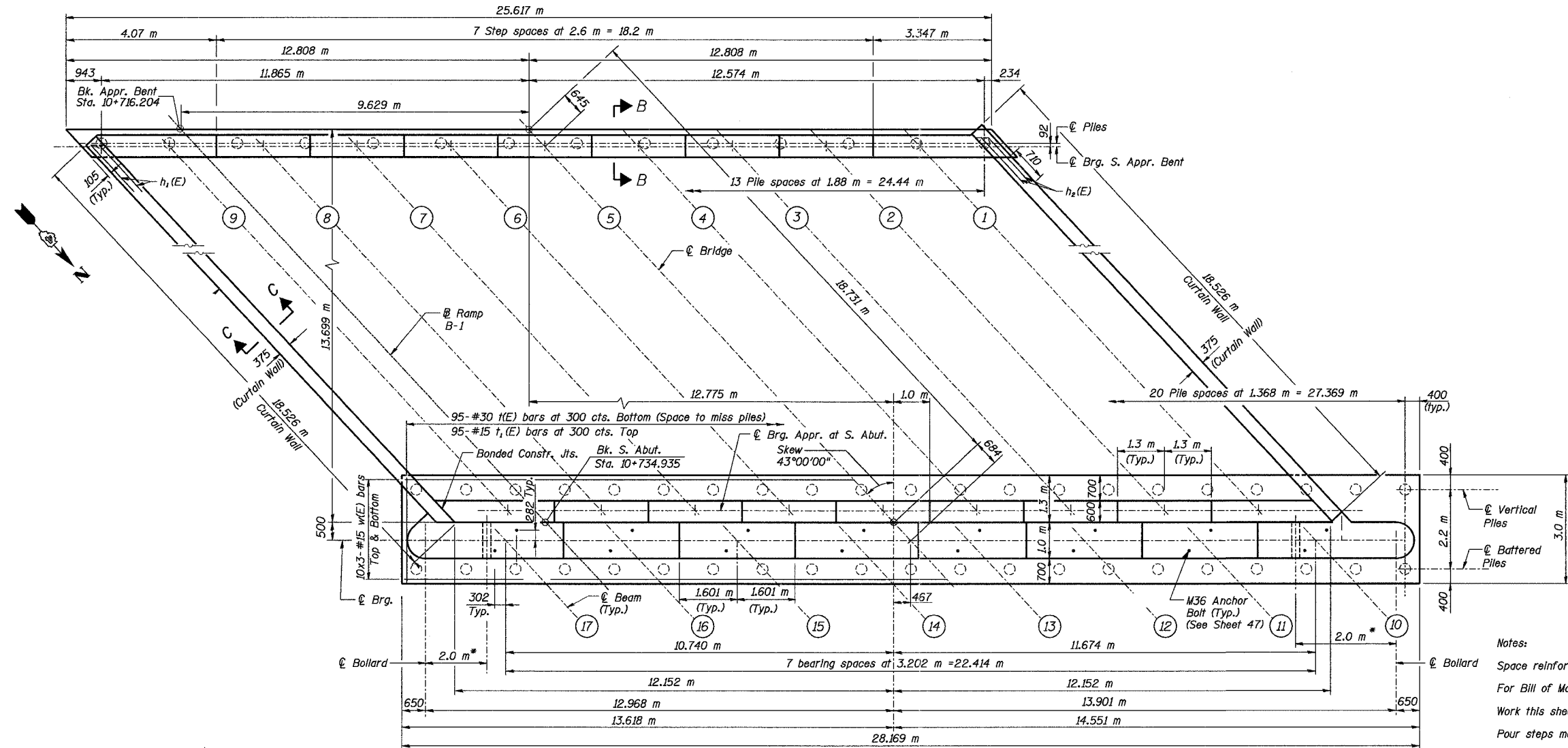
File name: P:\643996\Structur\072-0183_Sheet\SD0022-1A0720183.dgn

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	472	13/13
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200

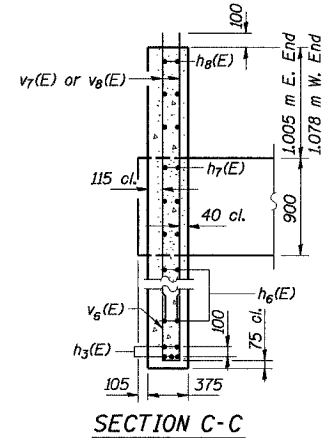
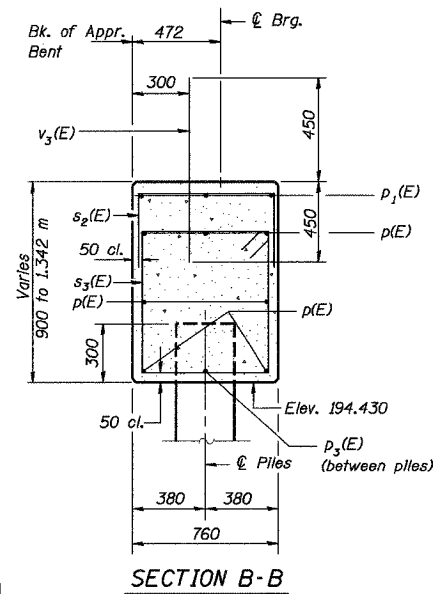
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Date: 1/19/2004

Filename: P:\643996\structure\072-0183\Sheet\Tracings\AB0001-10720183.dgn



- Notes:**
- Space reinforcement in cap to miss anchor bolts.
 - For Bill of Material and reinforcement details, see sheet 32.
 - Work this sheet with sheets 30, 31, and 32.
 - Pour steps monolithically with cap.
 - Bars indicated thus 20 X 3-#15 etc. indicates 20 lines of bars with 3 lengths per line.
 - For Rustication on face of abutment and curtain wall, see sheet 39.
 - E.F. denotes each face.
 - All dimensions are in millimeters (mm) except as noted.
 - Reinforcement bars designated (E) shall be epoxy coated.

* 75 mm ϕ formed drain locations



ABUT.-PILE DATA

Type: 305 mm ϕ metal sheet piles with 6 mm walls
 Capacity: 400 kN
 Est. Length: 10.0 m
 No. Req'd: 41 plus 1 test pile

APPR. BENT-PILE DATA

Type: 305 mm ϕ metal shell piles with 6mm walls
 Capacity: 400 kN
 Est. Length: 12.0 m
 No. Req'd: 13 plus 1 Test Pile

MIN. BAR LAP
 #15 bar = 640

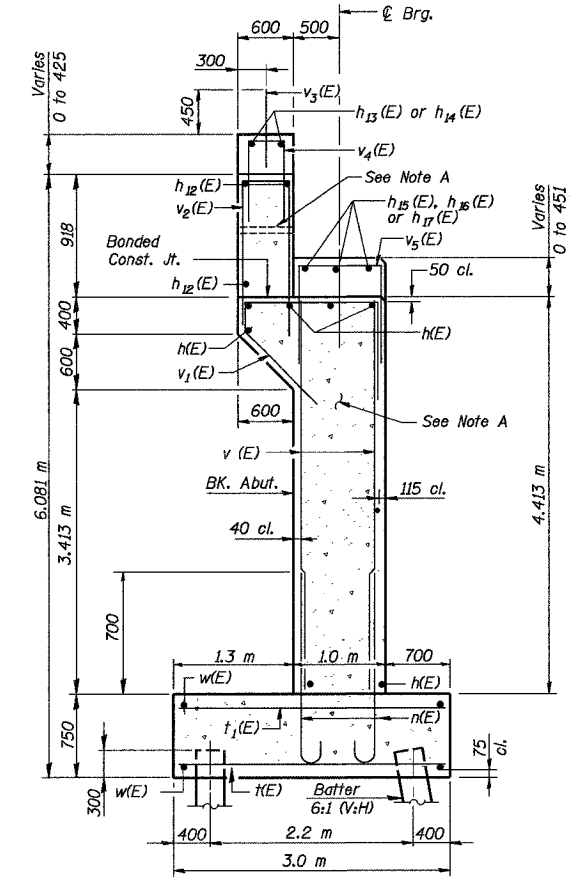
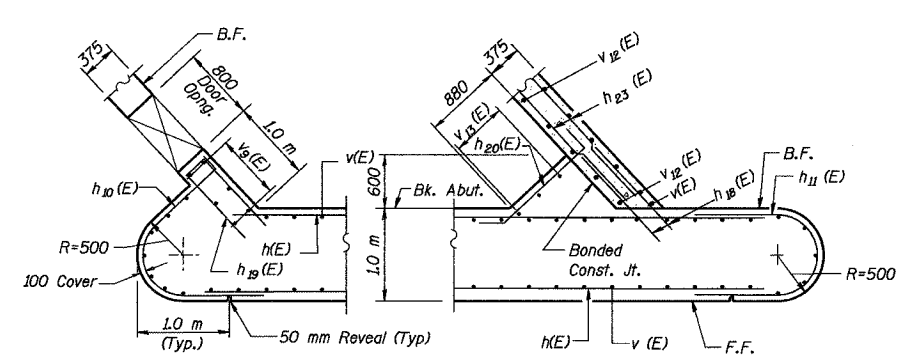
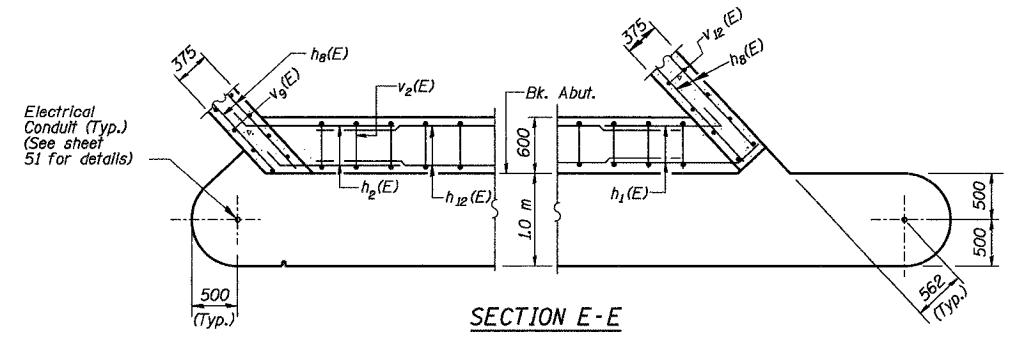
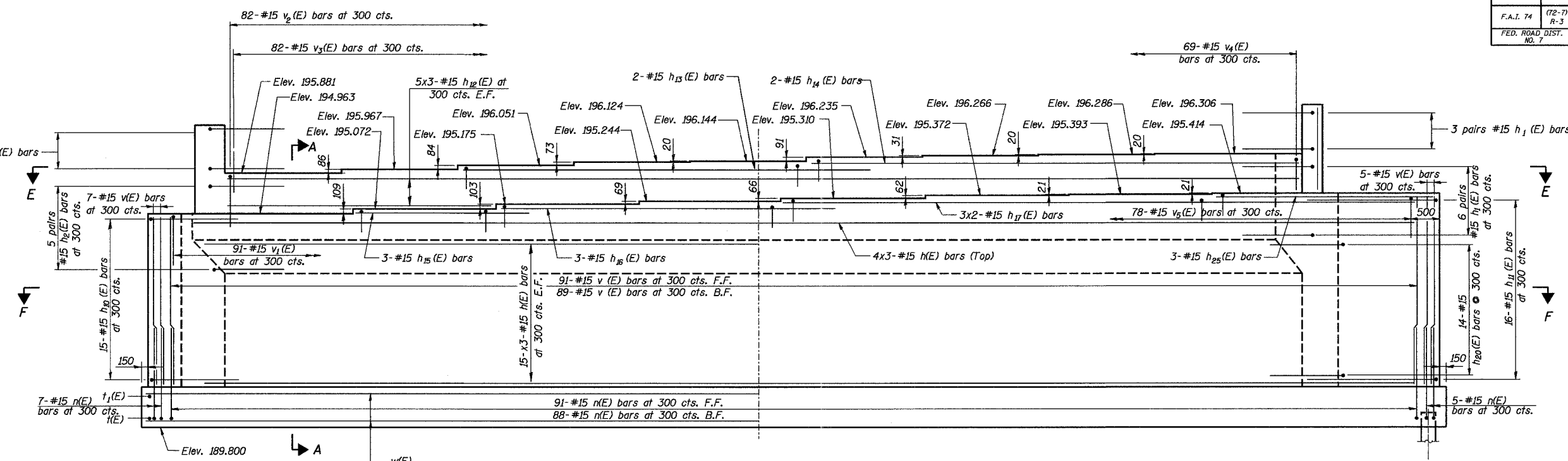
Designed by: DL
 Checked by: MBO
 Drafted by: FTE
 Checked by: AK

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS		
DEPARTMENT OF TRANSPORTATION		
SOUTH ABUTMENT PLAN		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 29	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 29

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	473	1360
FED. ROAD DIST. NO. 7	ILLINOIS		FED. AID PROJECT	

CONTRACT NO. 68200

Time: 10:06:15 AM Date: 11/19/2004 File: P:\A\43996\structure\072-0183\Sheet\Tracings\AB0004-1\0720183.dgn



Notes:
 Space reinforcement in cap to miss anchor bolts.
 For Bill of Material and reinforcement details, see sheet 32.
 Work this sheet with sheet 29.
 Pour steps monolithically with cap.
 Bars indicated thus 20 X 3-#15 etc. indicates 20 lines of bars with 3 lengths per line.
 For Rustication on face of abutment and curtain wall, see sheet 39.
 E.F. denotes each face.
 F.F. denotes front face.
 B.F. denotes back face.
 All dimensions are in millimeters (mm) except as noted.
 Reinforcement bars designated (E) shall be epoxy coated.

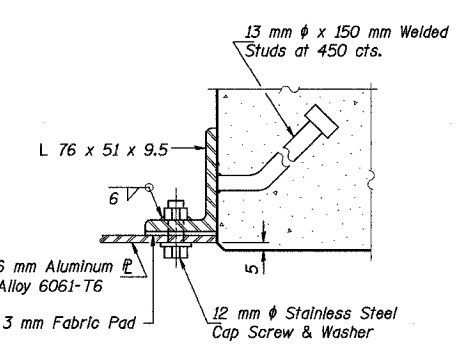
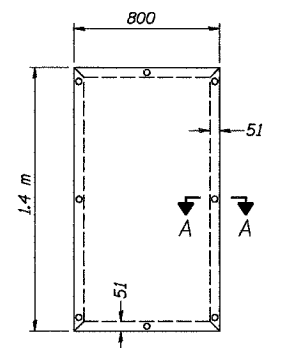
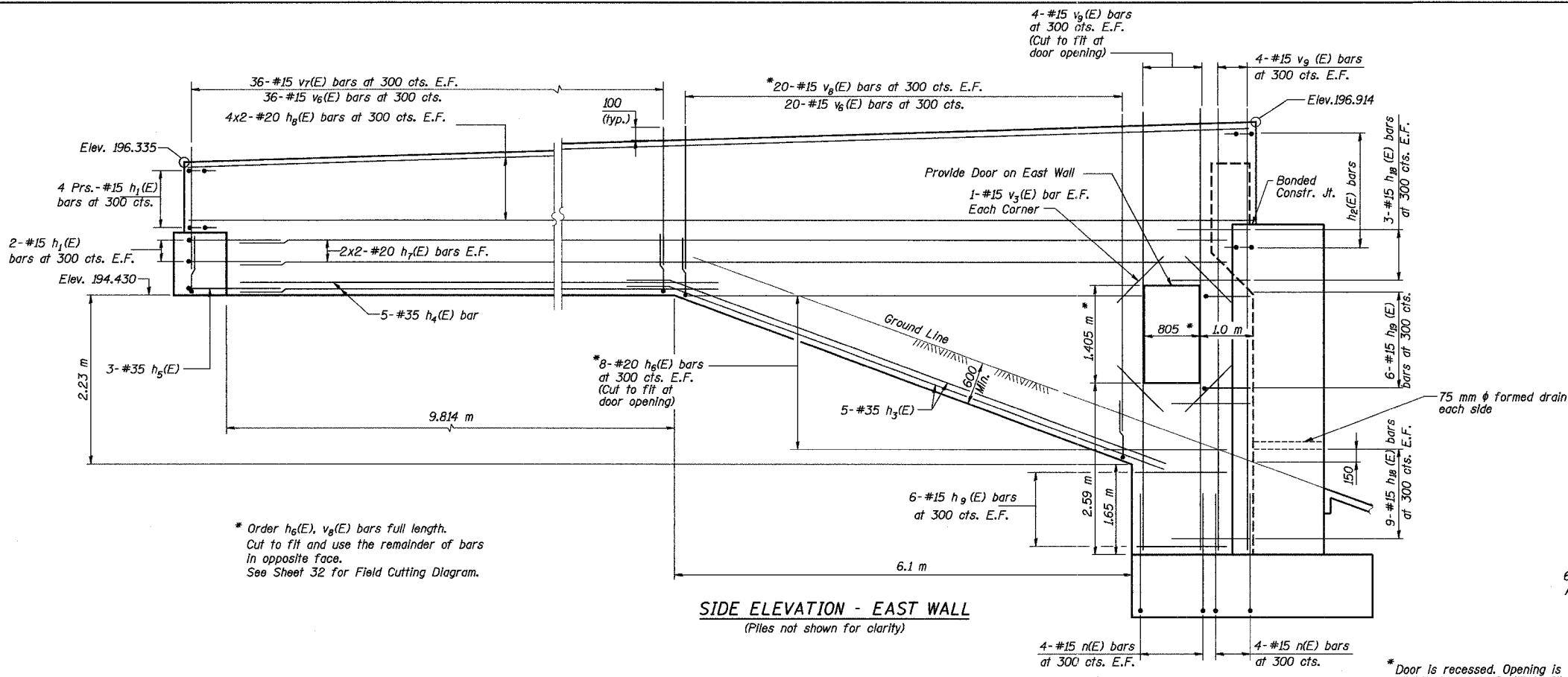
REVISION	DATE	DESCRIPTION	
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION SOUTH ABUTMENT ELEVATION RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183 PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS			
DRAWING NO.	SCALE	DATE	SHEET NO.
30	N.T.S.	2-18-03	30

Designed by: DL
 Checked by: MBQ
 Drafted by: FTE
 Checked by: AK

MIN. BAR LAP
 #15 bar = 640

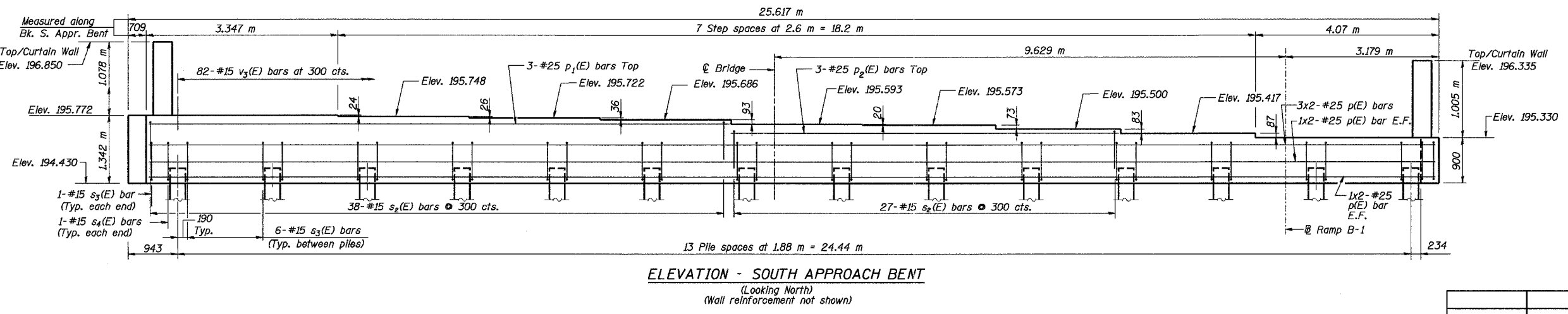
ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	474	1360
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	CONTRACT NO. 68200	
NO. 7				

Time: 10:06:36 AM
 Date: 11/19/2004
 File: P:\443996\struc\struc\1072-0183\Sheet\Tracings\180002-10720183.dgn



* Order h6(E), v8(E) bars full length. Cut to fit and use the remainder of bars in opposite face. See Sheet 32 For Field Cutting Diagram.

* Door is recessed. Opening is slightly larger to facilitate fit-up.



MIN. BAR LAP

#15 bar	= 640
#20 bar	= 790
#25 bar	= 1,85 m
#35 bar	= 2.64 m

Notes:
 Reinforcement bars designated (E) shall be epoxy coated.
 Work this sheet with sheet 29.
 Pour steps monolithically with cap.
 All dimensions are in millimeters (mm) except as noted.
 E.F. denotes each face.
 B.F. denotes back face.
 Bars indicated thus 20 x 3-#15 etc. indicates 20 lines of bars with 3 lengths per line.
 For Bill of Material and Reinforcement details, see sheet 32.

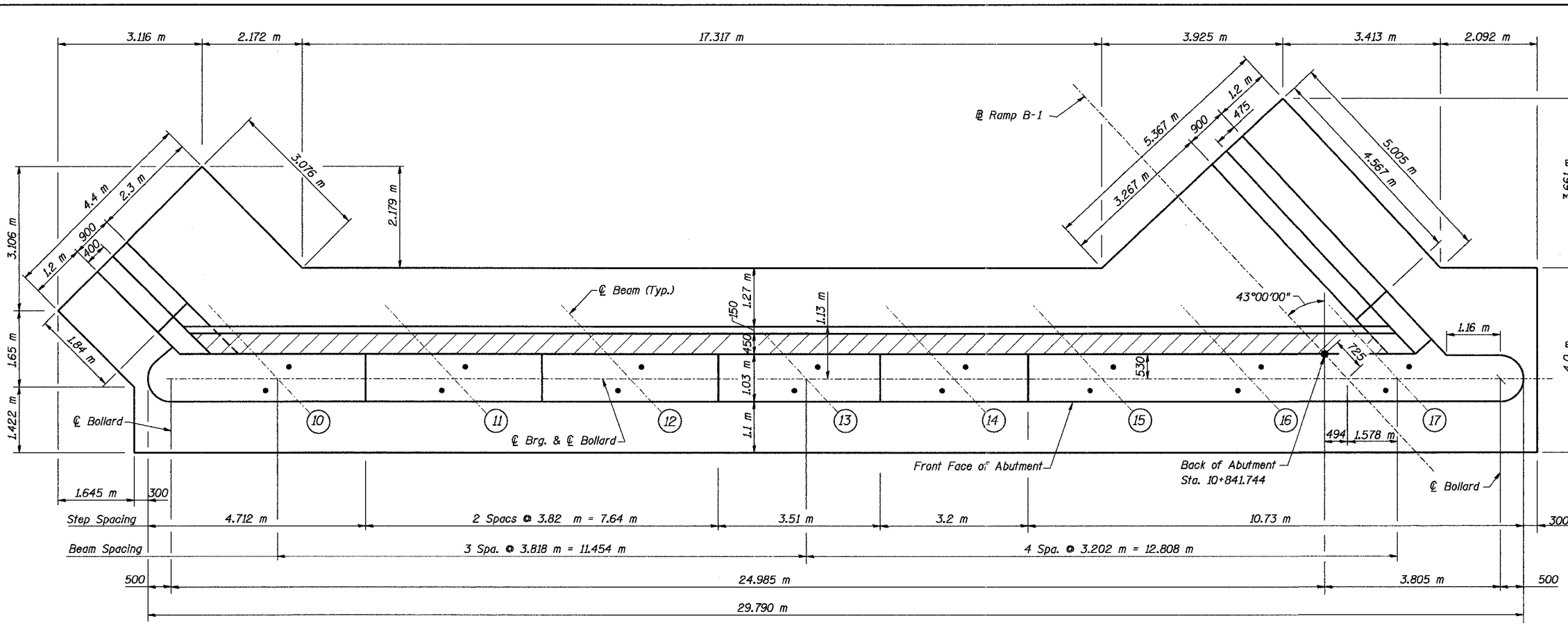
Designed by: DL
 Checked by: MBQ
 Drafted by: FTE
 Checked by: AK

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
SOUTH ABUTMENT DETAILS I		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 31	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 31

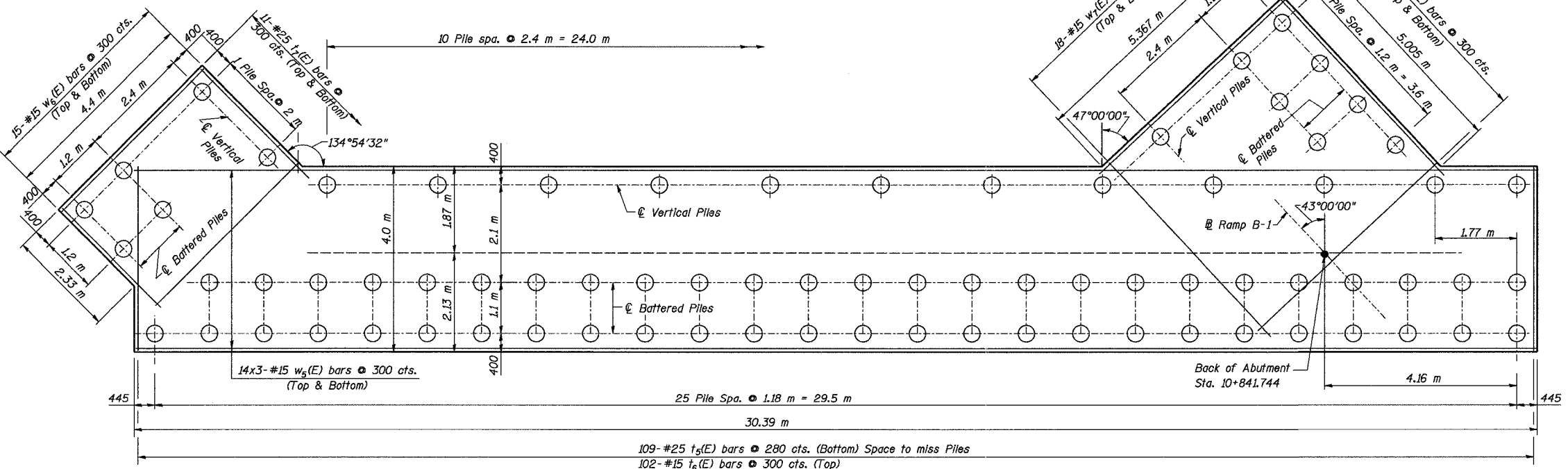
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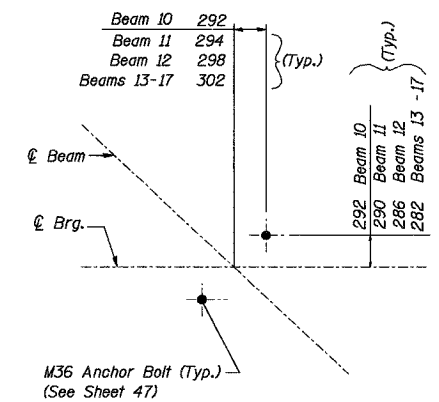
TOP VIEW
(Parapets and coping not shown for clarity)



FOOTING PLAN
(For dimensions not shown see top view)
(Space reinforcement to miss piles)

Designed by: DL
 Checked by: MBQ
 Drafted by: JMG
 Checked by: AK

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	476	1360
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 68200				



ANCHOR BOLTS LOCATION

PILE DATA

Type: 356 mm ϕ Metal Shell Piles with 6 mm Walls
 Capacity: 500 kN
 Est. Length: 10.5 m
 No. Required: 76 Plus 1 Test Pile

Notes:

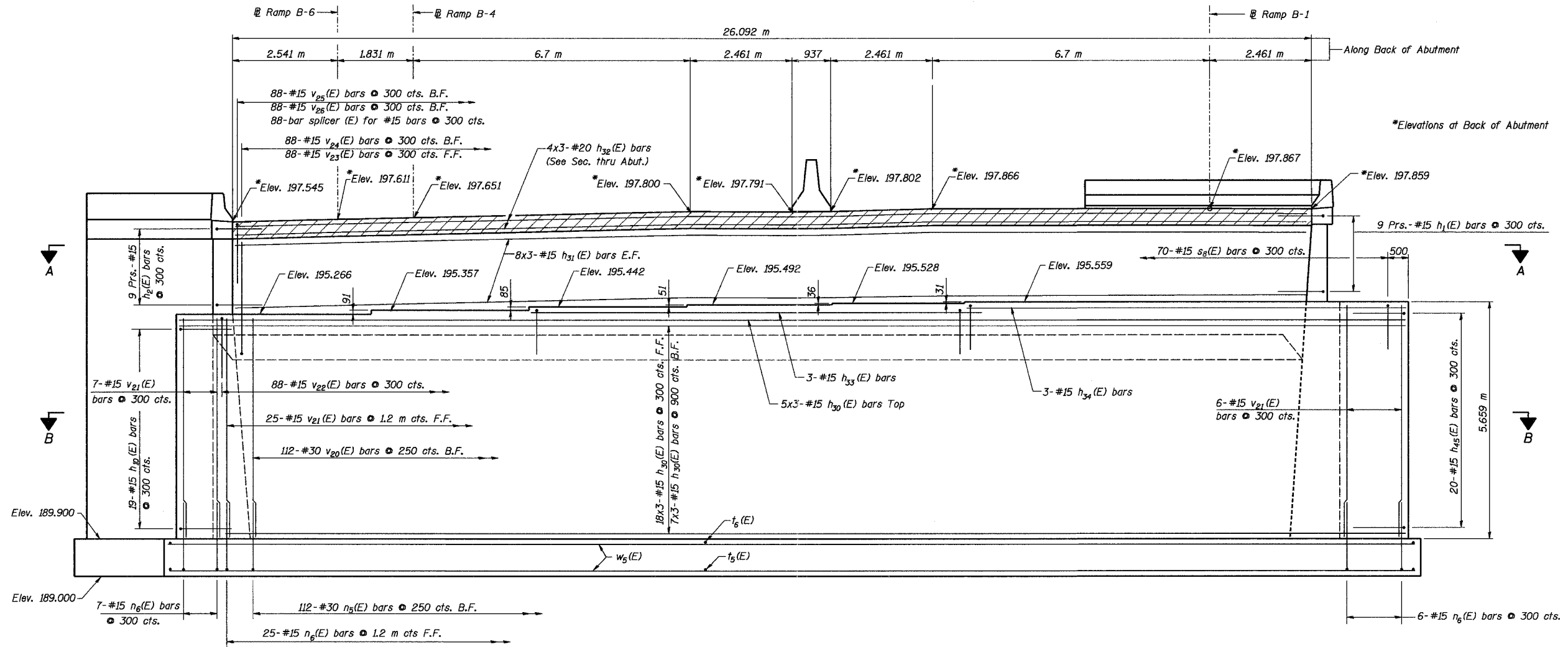
Space reinforcement in cap to miss anchor bolts.
 Reinforcement bars designated (E) shall be epoxy coated.
 Bars indicated thus 11x2-#15 etc. indicates 11 lines of bars with 2 lengths per line.
 All dimensions are in millimeters (mm) except as noted.
 Work this Sheet with Sheets 34 & 35.

MIN. BAR LAP
 #15 bar = 640

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
NORTH ABUTMENT PLAN		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3		
F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 33	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 33

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	477	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200



ELEVATION
(Looking North)
(Railings Not Shown for Clarity)

Notes:

- Space reinforcement in cap to miss anchor bolts.
- Pour steps monolithically with cap.
- Reinforcement bars designated (E) shall be epoxy coated.
- Bars indicated thus 14x2-#15 etc. indicates 14 lines of bars with 2 lengths per line.
- For Bill of Material see Sheet 38.
- For Architectural details on face of abutments and wingwalls see Sheet 39.
- For Sections A-A & B-B see Sheet 35.
- All dimensions are in millimeters (mm) except as noted.
- Work this Sheet with Sheets 33 and 35.
- F.F. = Front Face
- B.F. = Back Face
- E.F. = Each Face

MIN. BAR LAP
#15 bar = 640
#20 bar = 790

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
NORTH ABUTMENT ELEVATION		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 34	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 34

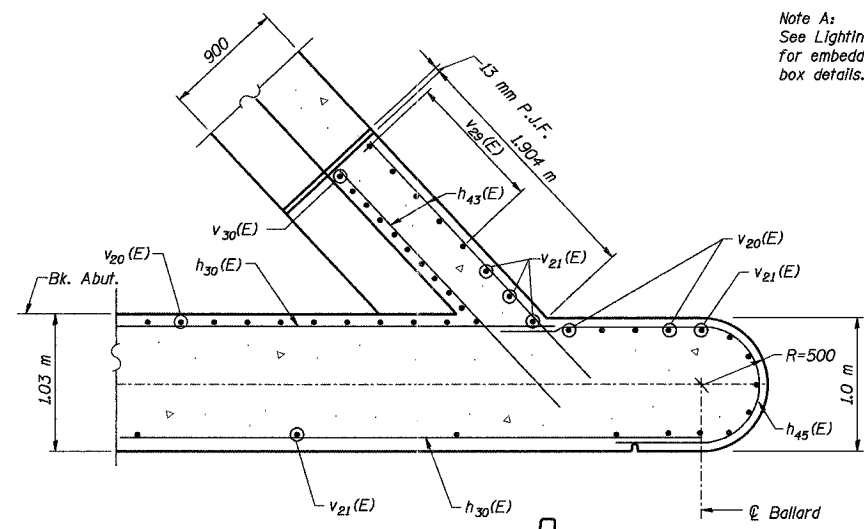
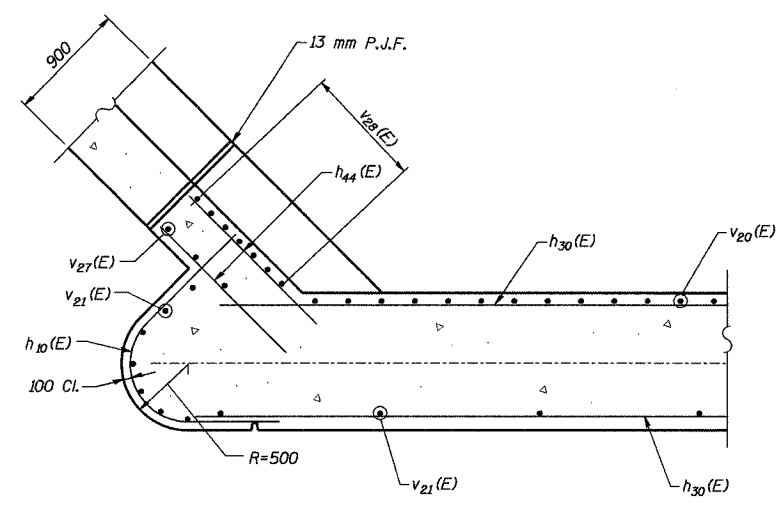
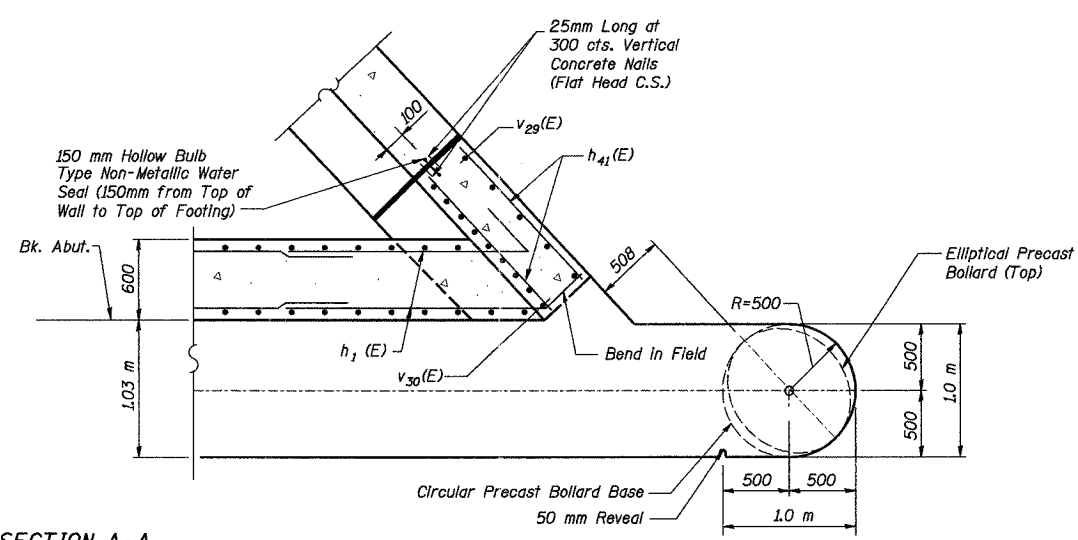
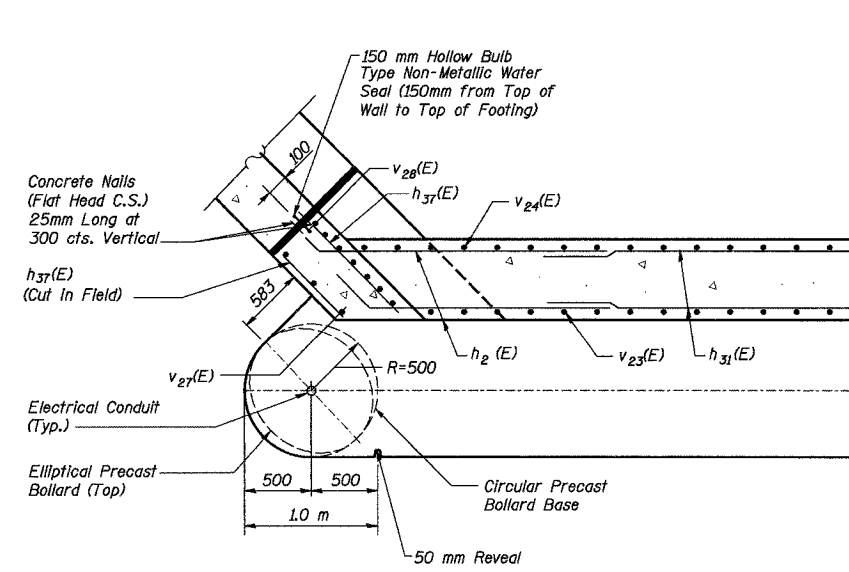
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Date: 1/19/2004

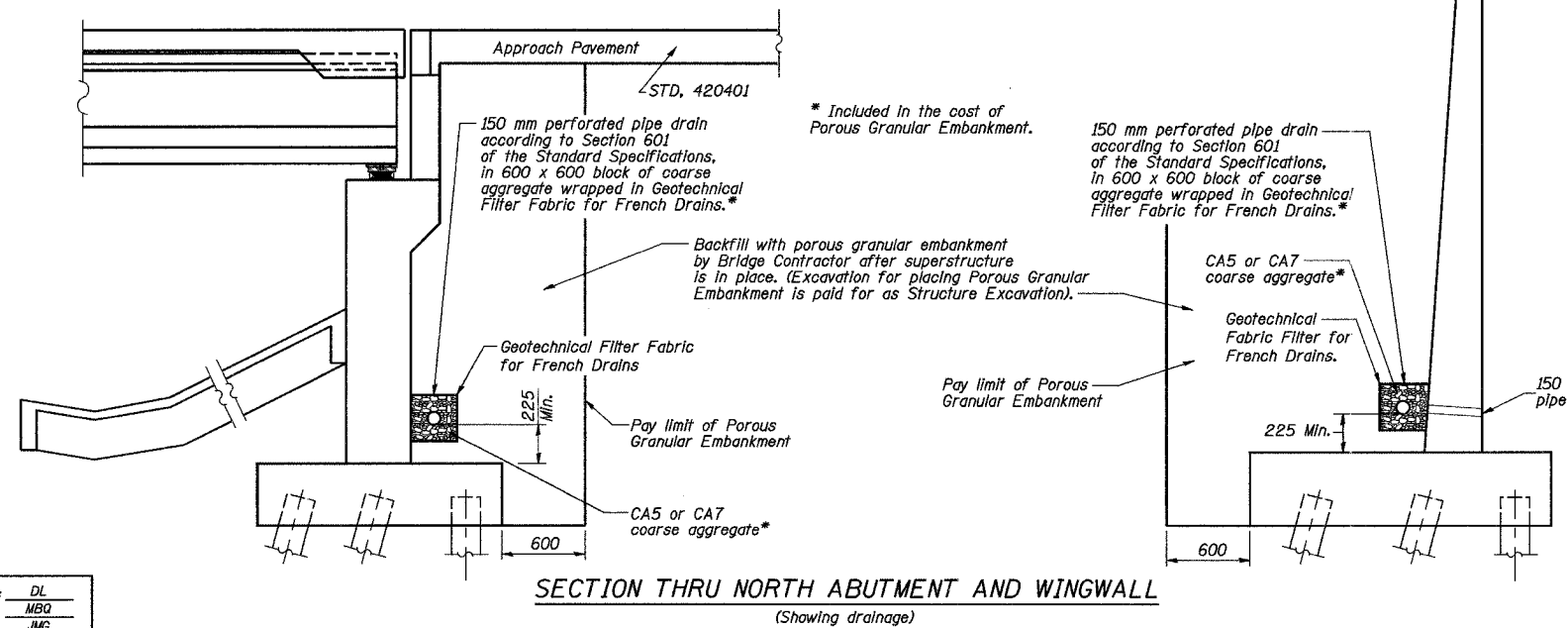
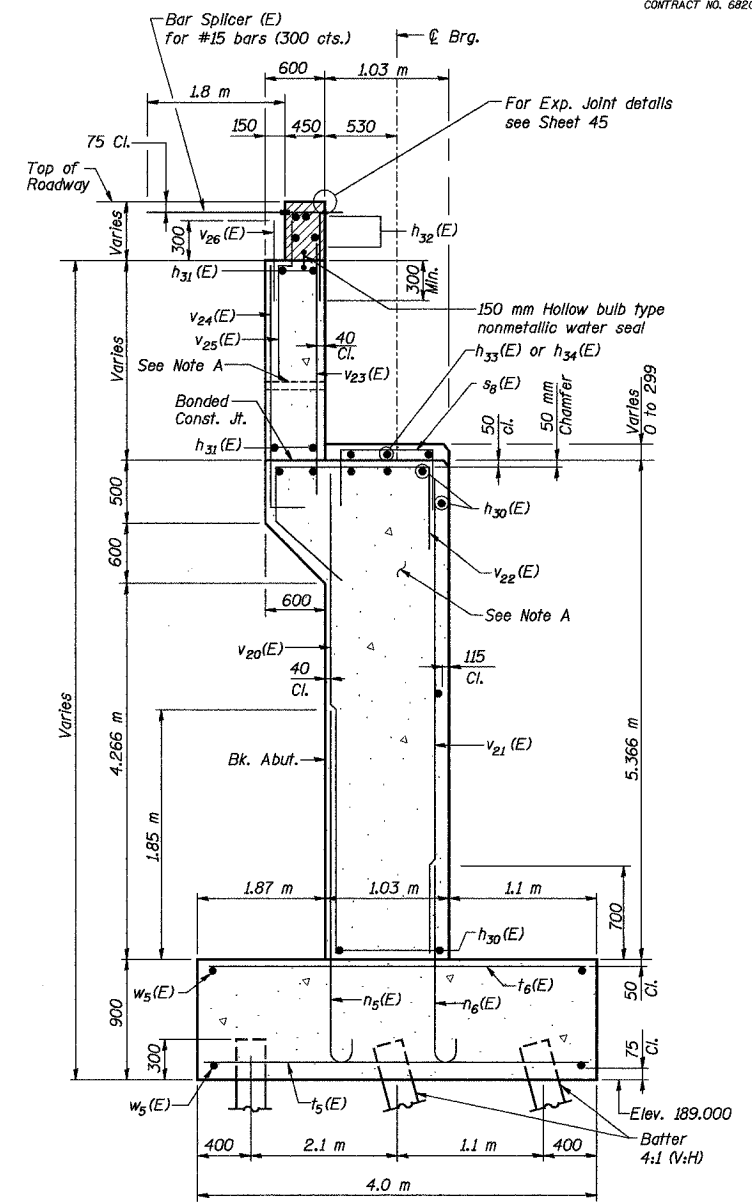
File name: P:\43996\structure\072-0183\Sheet\Tracings\AB0009-10720183.dgn

Designed by:	DL
Checked by:	MBQ
Drafted by:	JMG
Checked by:	AK

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	478	1360
FED. ROAD DIST. NO. 7	ILLINOIS		FED. AID PROJECT	
CONTRACT NO. 68200				



Note A:
See Lighting and Electrical plans for embedded conduit and junction box details.



MIN. BAR LAP
#15 bar = 640
#30 bar = 1.85 m

Notes:
Space reinforcement in cap to miss anchor bolts.
For Bill of Material and reinforcement details, see sheet 38.
Work this sheet with sheet 34.
Pour steps monolithically with cap.
For Rustication on face of abutment and wingwall, see sheet 39.
Hatched area to be poured after superstructure forms have been removed. Quantity of concrete included with Concrete Superstructure.
Connect 150mm pipe drains to drainage structures adjacent to east and west wing walls.
All dimensions are in millimeters (mm) except as noted.

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
NORTH ABUTMENT DETAILS I		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 35	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 35

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Designed by: DL
 Checked by: MBQ
 Drafted by: JMG
 Checked by: AK

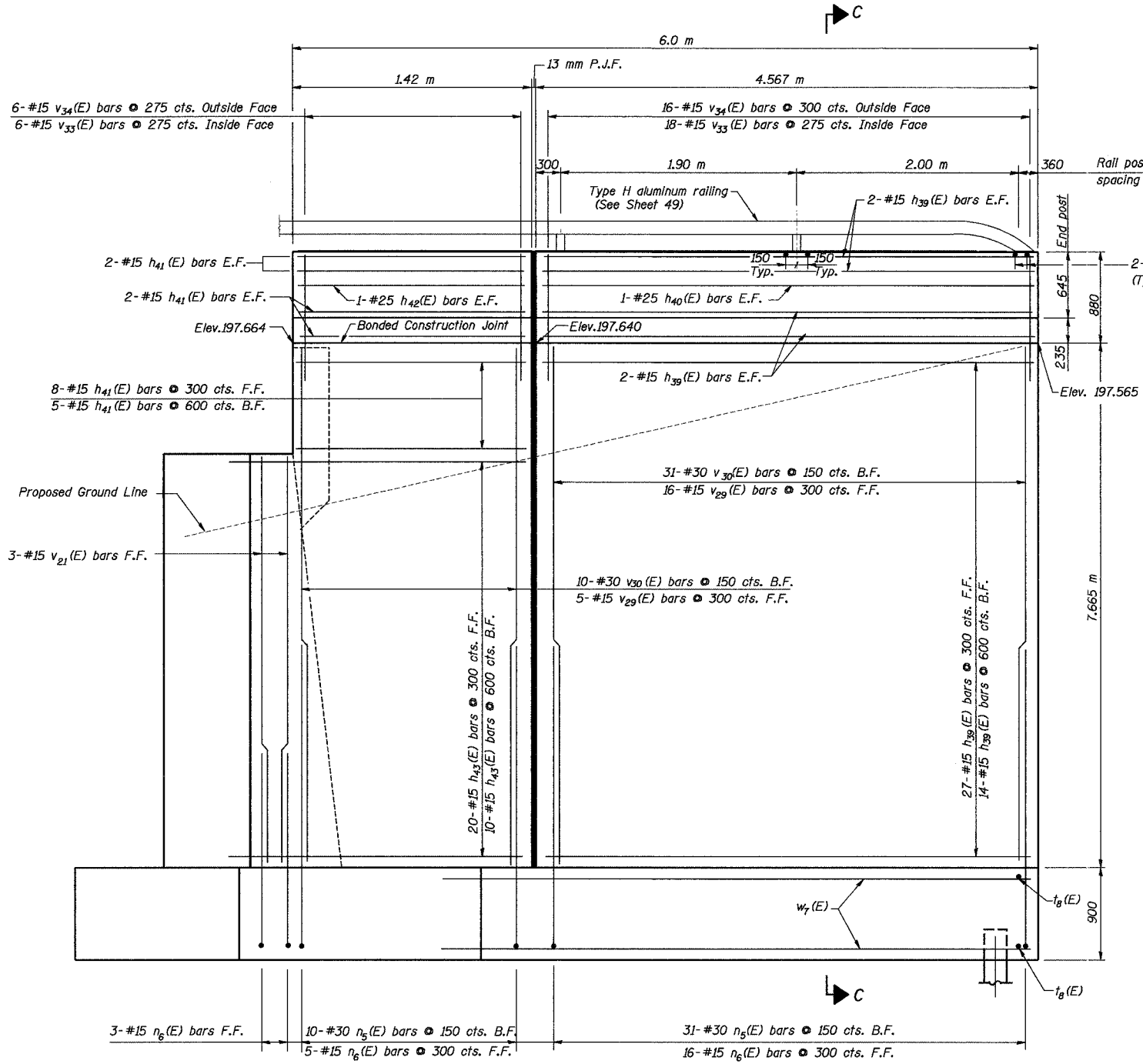
ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	479	1360
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

CONTRACT NO. 68200

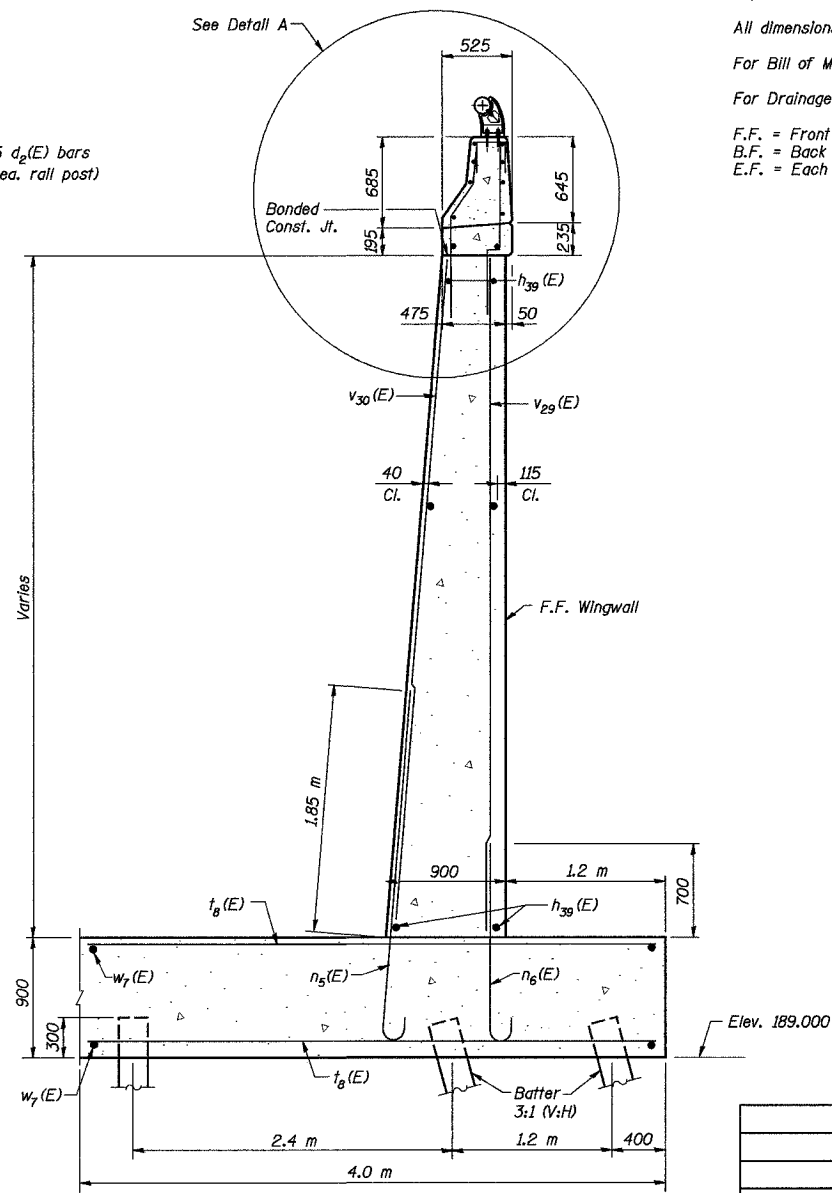
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Date: 1/19/2004

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NORTHEAST WINGWALL
(Looking West)

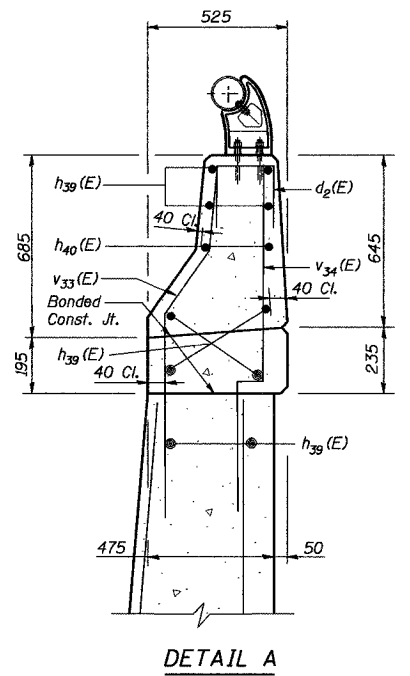
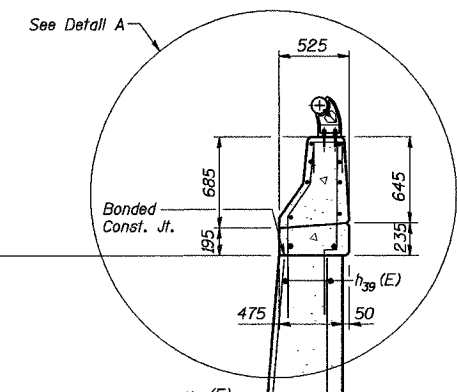


SECTION C-C

MIN. BAR LAP
#15 bar = 640
#30 bar = 1.85 m

Notes:
Reinforcement bars designated (E) shall be epoxy coated.
Quantity of concrete in end post included with Concrete Superstructure on Sheet 15.
All dimensions are in millimeters (mm) except as noted.
For Bill of Material see Sheet 38.
For Drainage behind wingwalls see Sheet 35.

F.F. = Front Face
B.F. = Back Face
E.F. = Each Face



DETAIL A

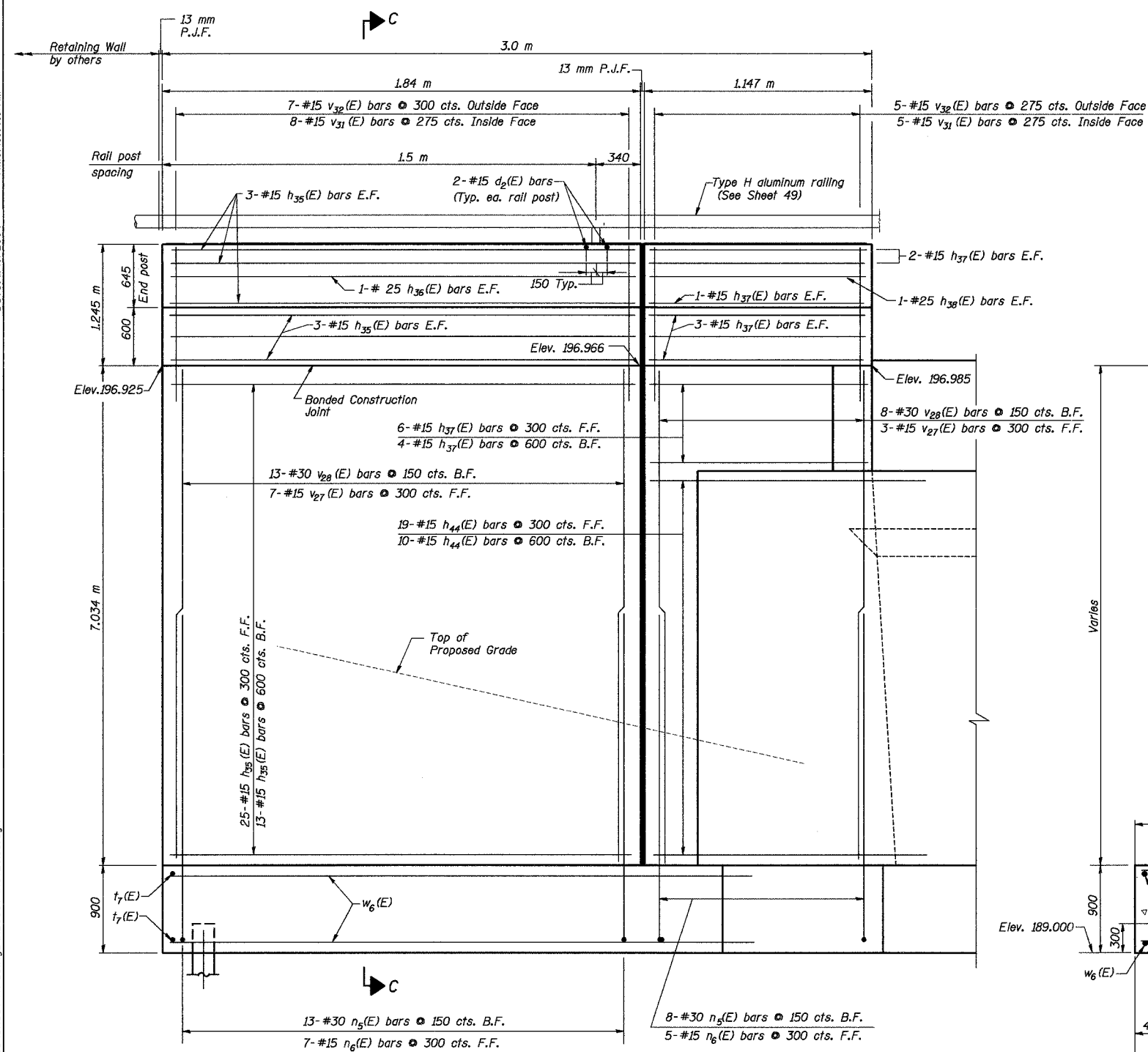
REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
NORTH ABUTMENT DETAILS II		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3		
F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 36	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 36

Designed by: DL
Checked by: MBQ
Drafted by: JMG
Checked by: AK

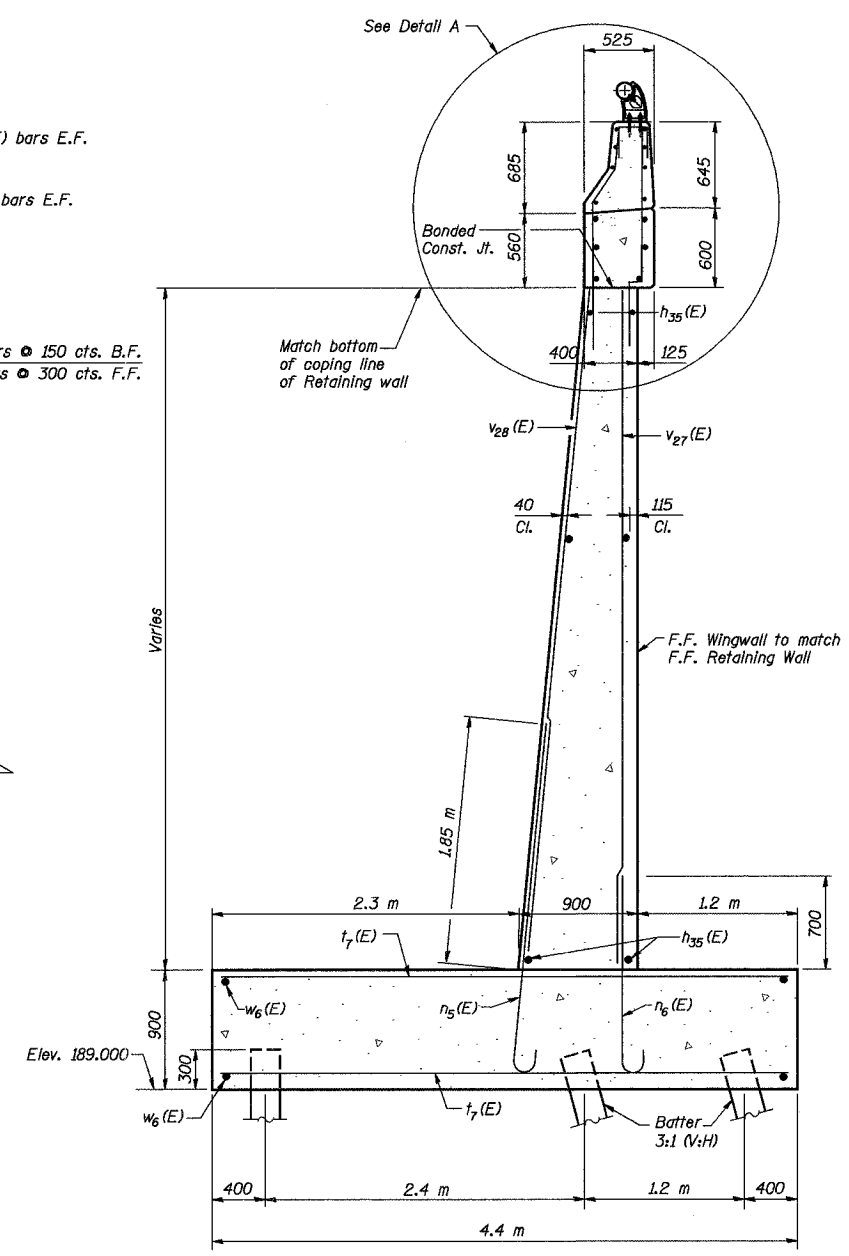
ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	480	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200

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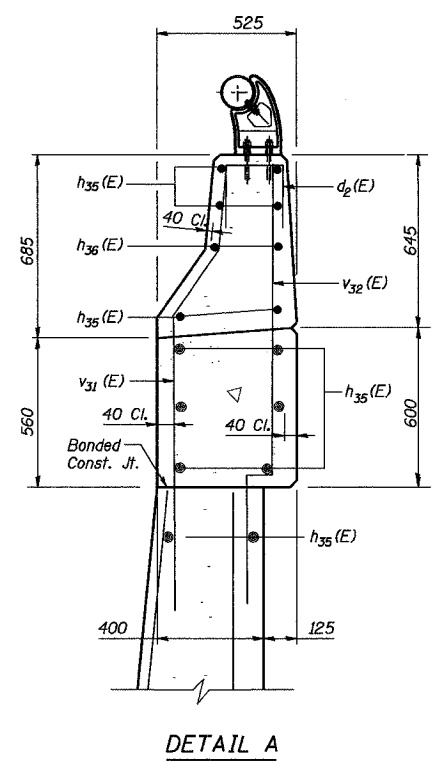


NORTHWEST WINGWALL
(Looking East)



SECTION C-C

MIN. BAR LAP
 #15 bar = 640
 #30 bar = 1.85 m



DETAIL A

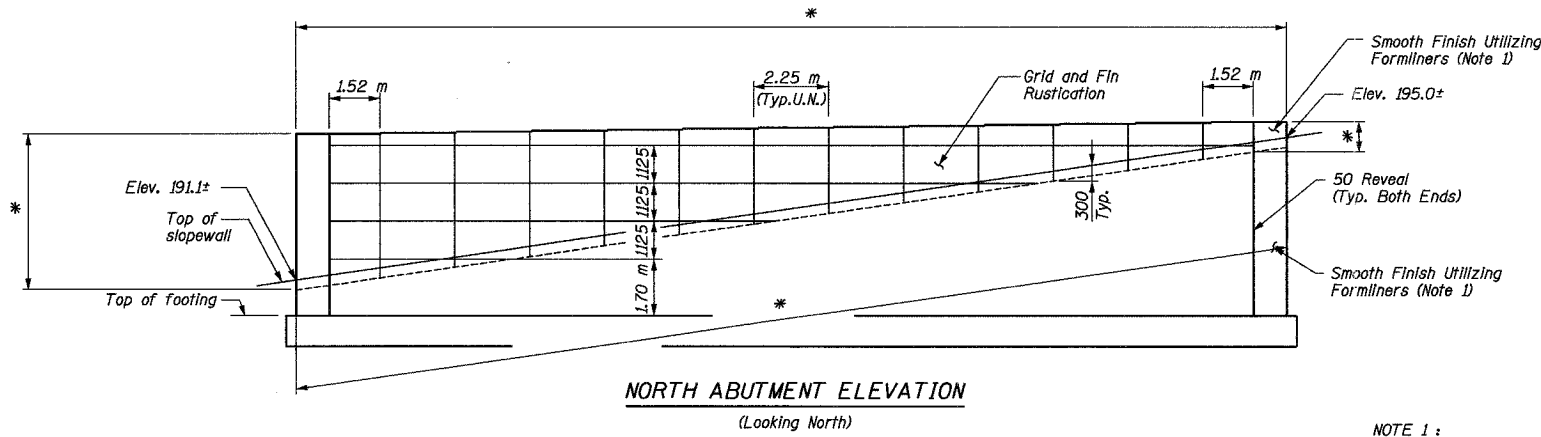
- Notes:
- Reinforcement bars designated (E) shall be epoxy coated.
 - Quantity of concrete in end post included with Concrete Superstructure on Sheet 15.
 - All dimensions are in millimeters (mm) except as noted.
 - For Bill of Material see Sheet 38.
 - For Drainage behind abutments and wingwalls see Sheet 35.
- F.F. = Front Face
 B.F. = Back Face
 E.F. = Each Face

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
NORTH ABUTMENT DETAILS III		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 37	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 37

Designed by: DL
 Checked by: MBO
 Drafted by: JMG
 Checked by: AK

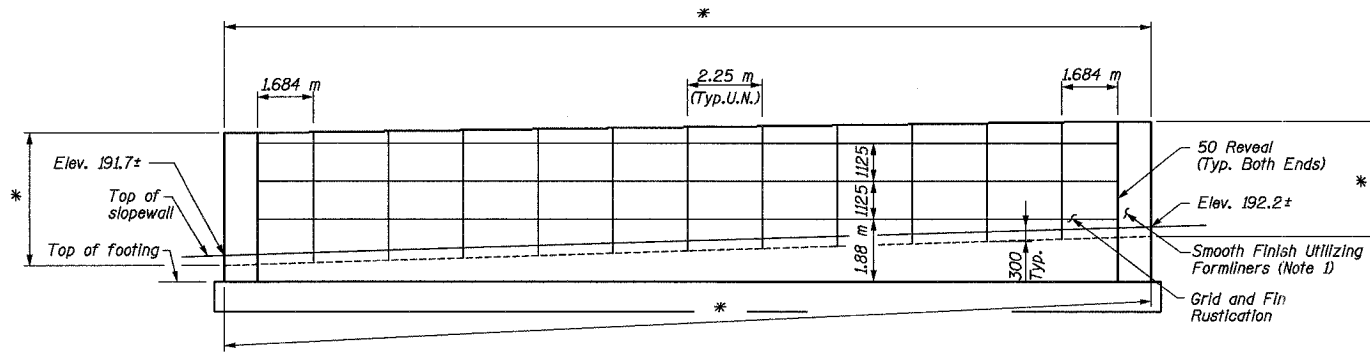
ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	482	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200

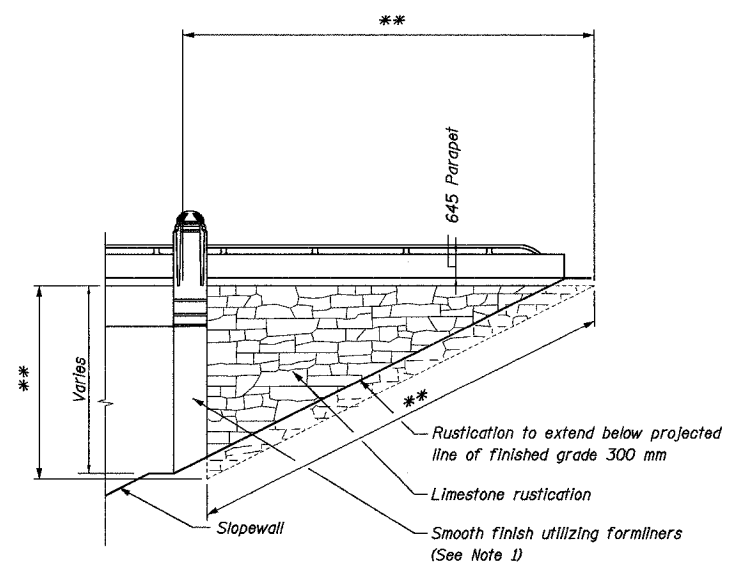


NORTH ABUTMENT ELEVATION
(Looking North)

NOTE 1:
Cost of Smooth Finish Formliners is Included
in the Cost of Form Liner Grid and Fin Surfaces

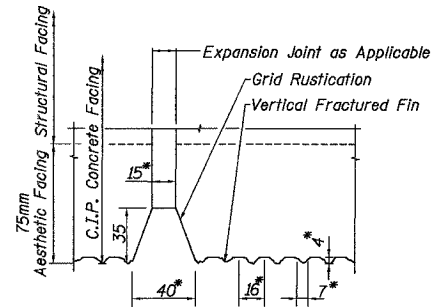


SOUTH ABUTMENT ELEVATION
(Looking South)



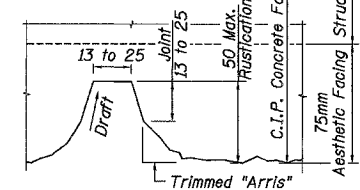
ELEVATION
(Wingwall with single tube rail)
(Curtain Wall Similar)

* Pay limits for Form Liner Grid & Fin Surface
** Pay limits for Form Liner Limestone Surface

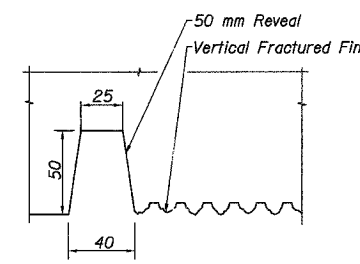


GRID AND FIN RUSTICATION DETAIL
NTS

* Dimensions subject to minor variations within the group of approved formliners.



LIMESTONE RUSTICATION DETAIL
NTS



50mm REVEAL DETAIL
NTS

NOTE: All dimensions are in millimeters (mm) except as noted.

BILL OF MATERIAL

Item	Unit	Quantity
Form Liner Limestone Surface	m ²	150
Form Liner Grid and Fin Surface	m ²	194

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
NORTH & SOUTH ABUTMENT ARCHITECTURAL DETAILS		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 39	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 39

Times: 0:09:43 AM
 Date: 1/19/2004
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 Checked by: AK
 Drafted by: JMG
 Checked by: AK

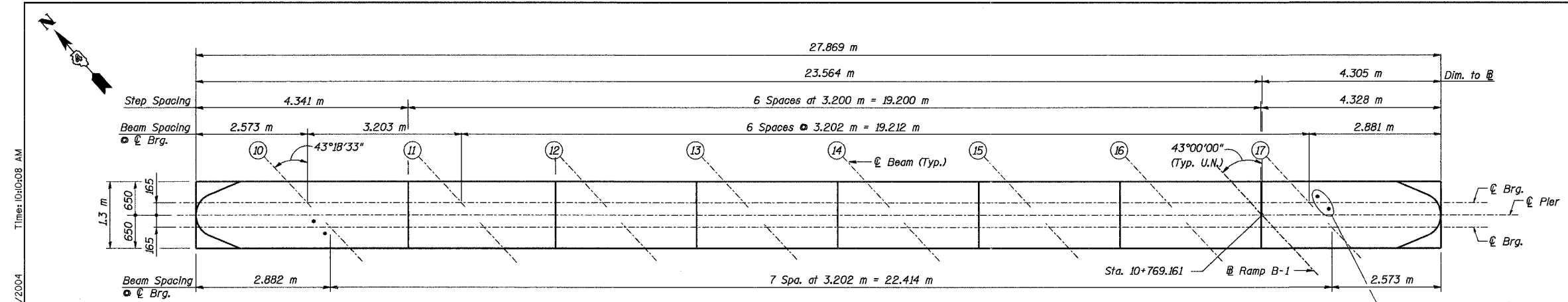
ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	483	1366
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200

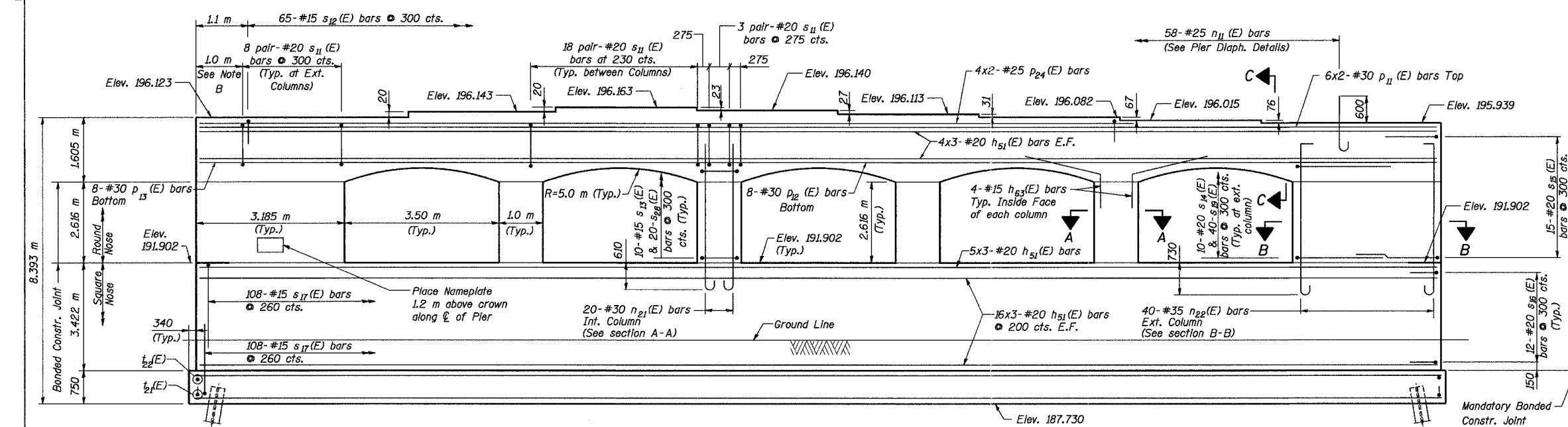
Notes:
 Bars indicated thus 4x2-#25 etc. Indicates 4 lines of bars with 2 lengths per line.
 E.F. = Each Face
 Four steps monolithically with cap.
 Reinforcement bars designated (E) shall be epoxy coated.
 All dimensions are in millimeters (mm) except as noted.
 Space reinforcement in cap to miss anchor bolts.
 For Section A-A, B-B & C-C see Sheet 41.

and Indicate Battered Piles

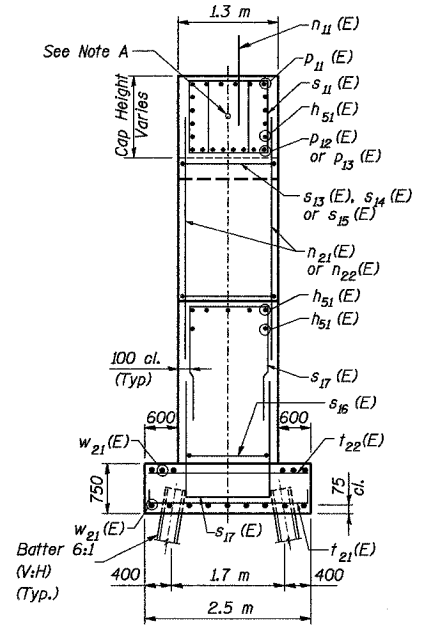
Note A:
 See Lightning and Electrical Plans for embedded conduit and junction box details.



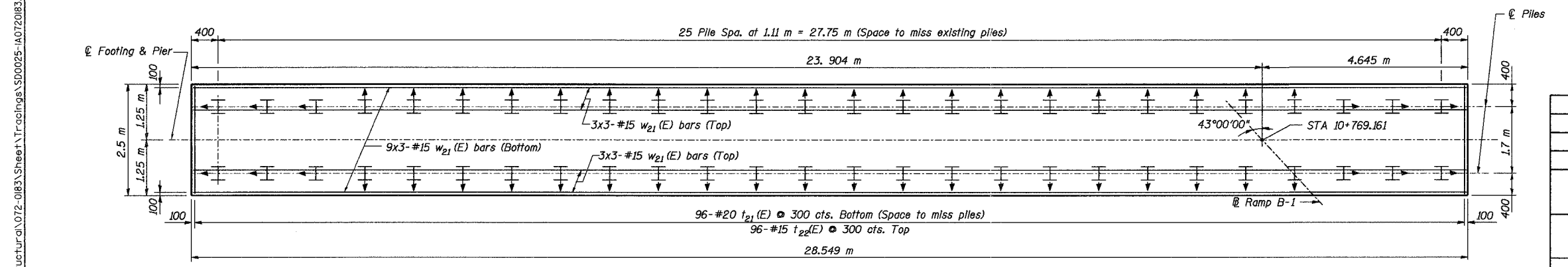
TOP PLAN



ELEVATION
(Looking North)



END VIEW



FOOTING PLAN

MIN. BAR LAP
 #15 bar = 640
 #20 bar = 790
 #25 bar = 1.85 m
 #30 bar = 2.59 m (Top Bars)
 #30 bar = 1.85 m (Others)

PILE DATA
 Type: Steel Piles HP310x79
 Capacity: 400 kN (Driven to 600 kN)
 Est. Length: 13.0 m
 No. Req'd: 51 Plus 1 Test Pile
 (Drive test pile to 750 kN)

Designed by: DL
 Checked by: AK
 Drafted by: JMG
 Checked by: AK

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
PIER 1		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 40	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 40

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	485	1360
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

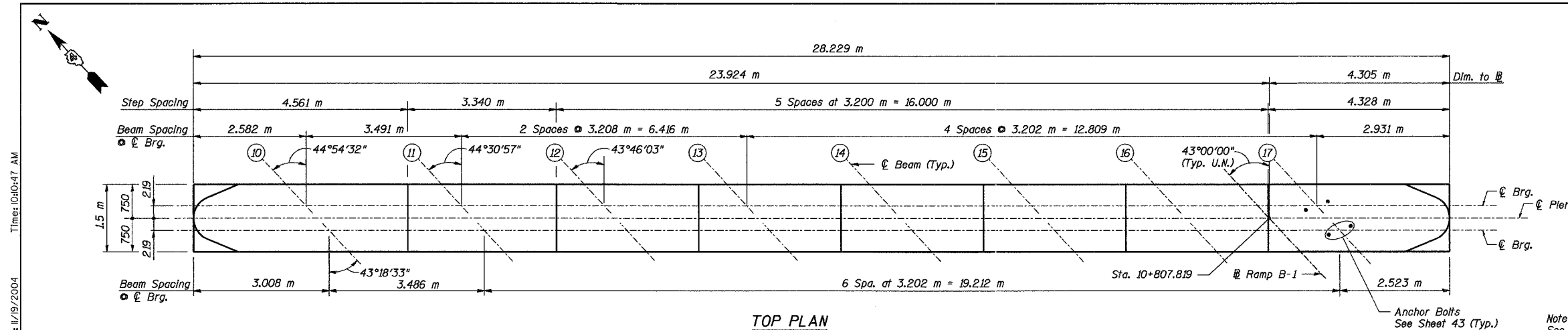
CONTRACT NO. 68200

Notes:

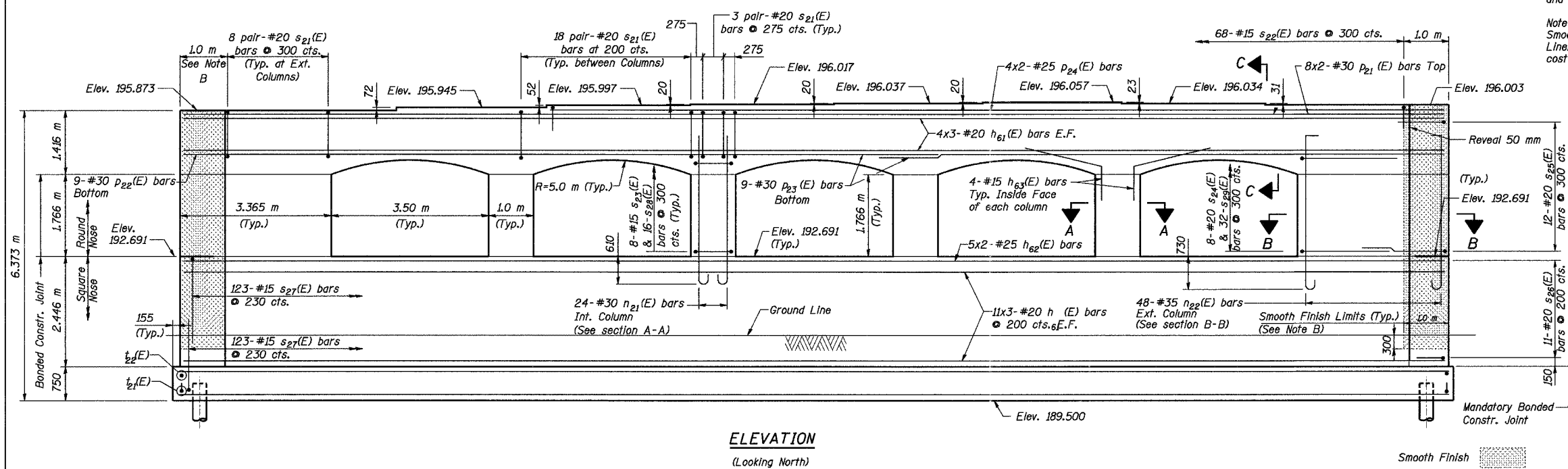
- Bars indicated thus 4x2-#25 etc. indicates 4 lines of bars with 2 lengths per line.
- E.F. = Each Face
- Four steps monolithically with cap.
- Reinforcement bars designated (E) shall be epoxy coated.
- All dimensions are in millimeters (mm) except as noted.
- Space reinforcement in cap to miss anchor bolts.
- For Section A-A, B-B & C-C see Sheet 43.

Note A:
See Lightning and Electrical Plans for embedded conduit and junction box details.

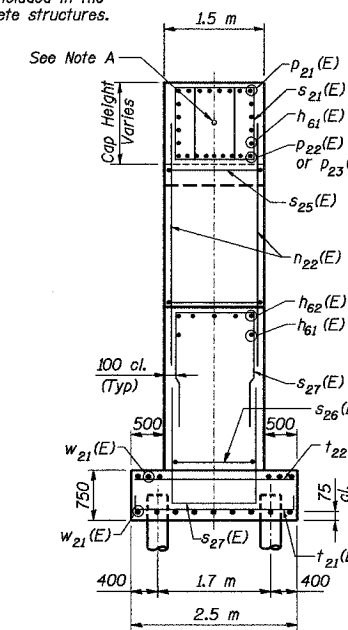
Note B:
Smooth Finish Utilizing Form Liners. Cost Included in the cost of concrete structures.



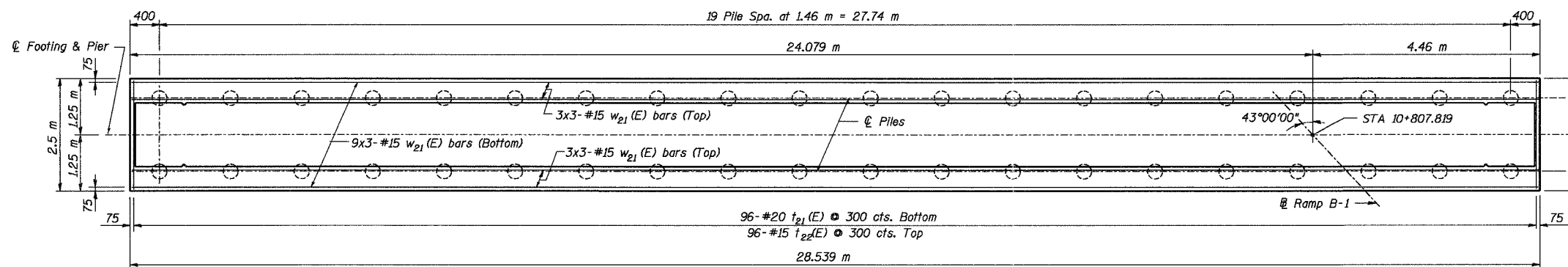
TOP PLAN



ELEVATION
(Looking North)



END VIEW



FOOTING PLAN

MIN. BAR LAP

- #15 Bar = 890 (Top Bars)
- #15 bar = 640 (Others)
- #20 bar = 1.1 m
- #25 bar = 1.85 m
- #30 bar = 2.59 m (Top Bars)
- #30 bar = 1.85 m (Others)

PILE DATA

Type: 356 mm ϕ Metal Shell with 6 mm walls
Capacity: 500 kN
Est. Length: 12.0 m
No. Req'd: 39 Plus 1 Test Pile

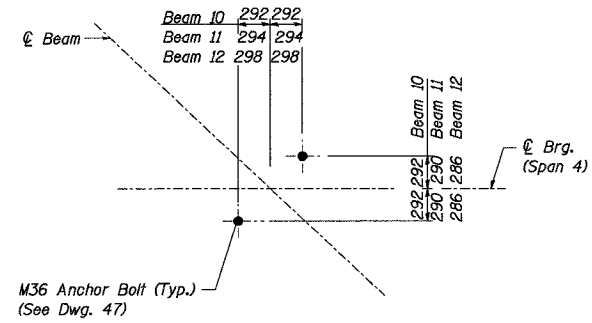
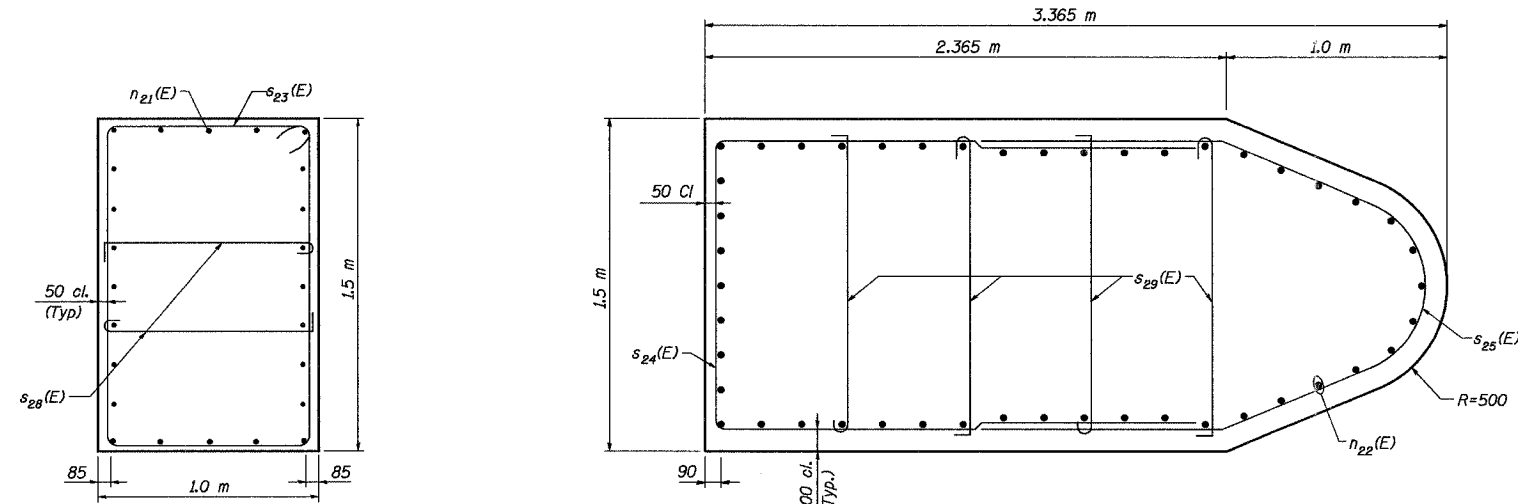
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Designed by: DL
Checked by: WEE
Drafted by: JMG
Checked by: WEE/AK

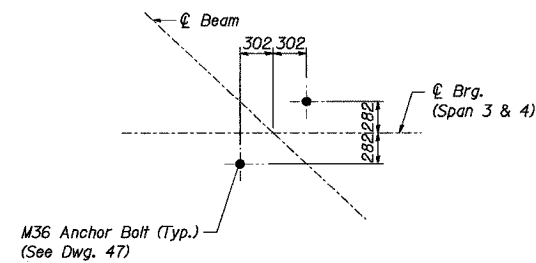
REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
PIER 2		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 42	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 42

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	486	1360
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

CONTRACT NO. 68200



BEAMS 10-12 (SPAN 4)

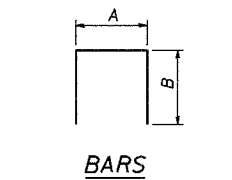
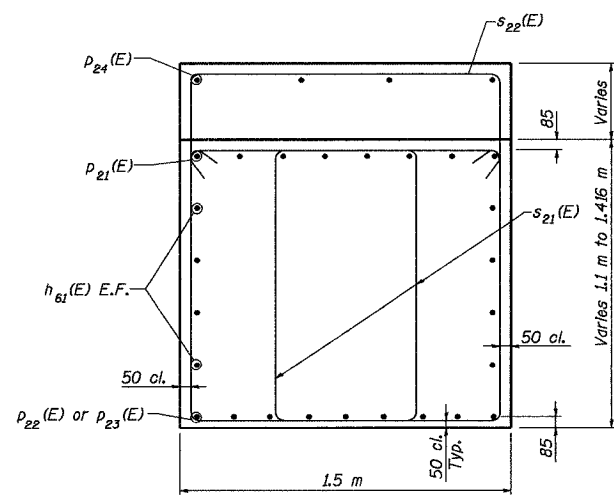


BEAMS 10-17 (SPAN 3) & BEAMS 13-17 (SPAN 4)

ANCHOR BOLTS LOCATION

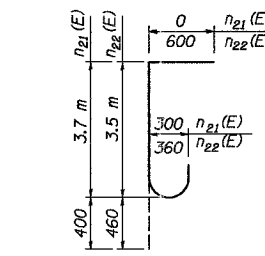
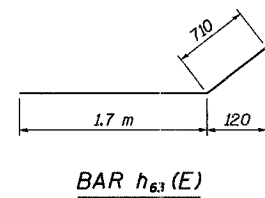
BILL OF MATERIAL

Bar	No.	Size	Length (m)	Shape
h ₆₁ (E)	90	#20	10.12	—
h ₆₂ (E)	10	#25	15.00	—
h ₆₃ (E)	40	#15	2.41	—
n ₂₁ (E)	96	#30	4.10	U
n ₂₂ (E)	96	#35	4.56	U
p ₂₁ (E)	16	#30	14.40	—
p ₂₂ (E)	9	#30	17.24	—
p ₂₃ (E)	9	#30	12.74	—
p ₂₄ (E)	8	#25	10.54	—
s ₂₁ (E)	236	#20	4.20	□
s ₂₂ (E)	68	#15	2.6	□
s ₂₃ (E)	32	#15	4.88	□
s ₂₄ (E)	16	#20	5.94	□
s ₂₅ (E)	24	#20	4.57	□
s ₂₆ (E)	22	#20	3.49	□
s ₂₇ (E)	246	#15	4.34	□
s ₂₈ (E)	64	#15	1.18	□
s ₂₉ (E)	64	#15	1.58	□
t ₂₁ (E)	96	#20	3.04	□
t ₂₂ (E)	96	#15	2.40	□
w ₂₁ (E)	45	#15	10.05	—
Furnishing Metal				
Pile Shells 356 mm	m		468	
Driving and Filling Shells	m		468	
Test Pile Metal Shells	Each		1	
Concrete Structures	m ³		275	
Reinforcement Bars, Epoxy Coated	kg		18800	
Structure Excavation	m ³		496	



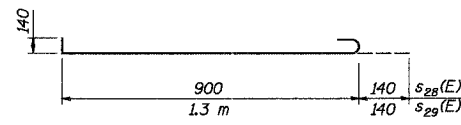
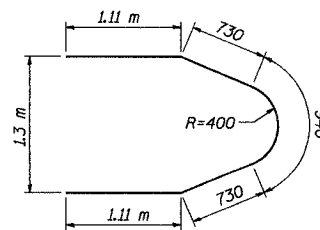
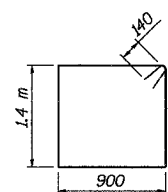
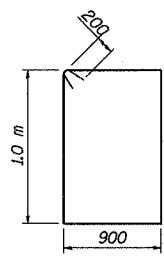
A & B DIMENSIONS

Bar	A	B
s ₂₂ (E)	1300	650
s ₂₄ (E)	1300	2320
s ₂₆ (E)	1270	1111
s ₂₇ (E)	1300	1520

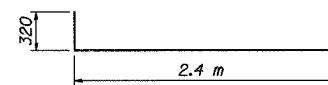


BARS n₂₁(E), n₂₂(E)

Notes:
E.F. - Each Face
Reinforcement bars designated (E) shall be epoxy coated.
All dimensions are in millimeters (mm) except as noted.



BARS s₂₈(E), s₂₉(E)



BAR t₂₁(E)

Date: 11/19/2004 Time: 10:10:06 AM

Filename: P:\643996\structure\072-0183\Sheet\Tracings\SD0021-H0720183.dgn

Designed by: DL
Checked by: WEE
Drafted by: JMG
Checked by: AK/WEE

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
PIER 2 DETAILS		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 43	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 43

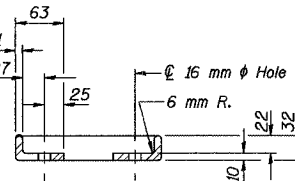
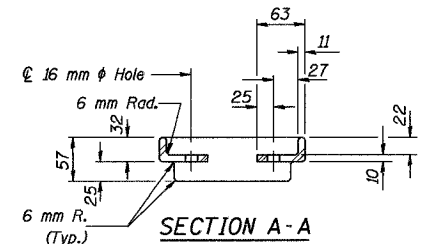
ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	487	1360
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

CONTRACT NO. 68200

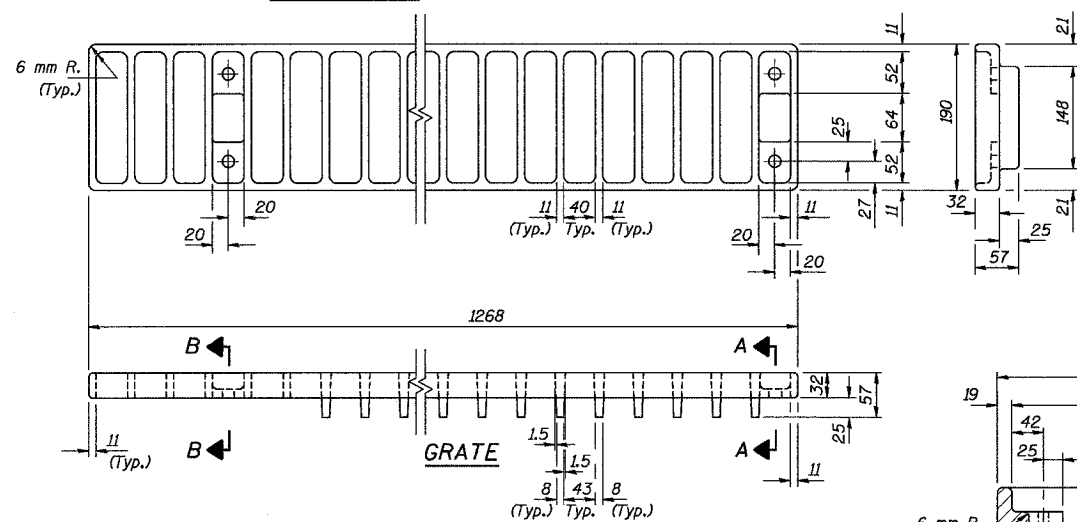
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Date: 1/19/2004

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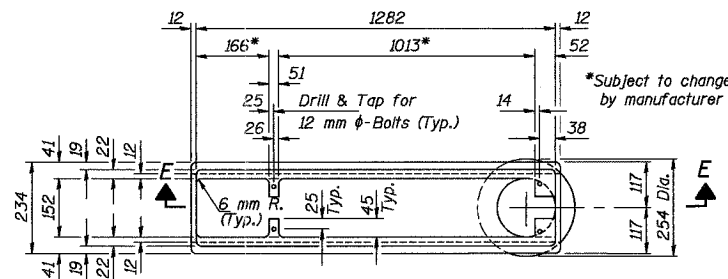
SECTION B-B



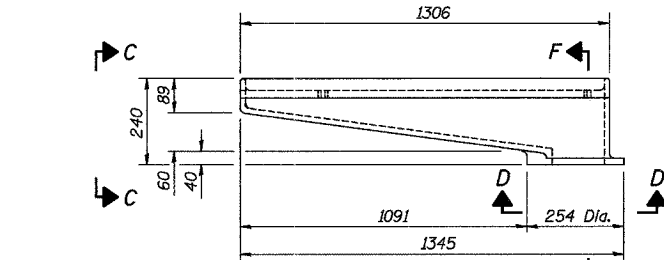
GRATE

Notes: All cast iron parts shall be gray iron conforming to the requirements of AASHTO M 105, Class 30.
Bolts and washers shall conform to the requirements of ASTM A 307.
All bolts and washers shall be galvanized according to AASHTO M 232.
As an alternate bolts and washers may be stainless steel.
Cost of the Grate, Frame, Downspout, bolts and washers including complete installation of Scupper will be paid for at the unit bid price each for "DRAINAGE SCUPPERS, TYPE 2."
The Contractor may use at his option steel drainage scuppers or cast iron drainage scuppers.
All dimensions are in millimeters (mm) except as noted.

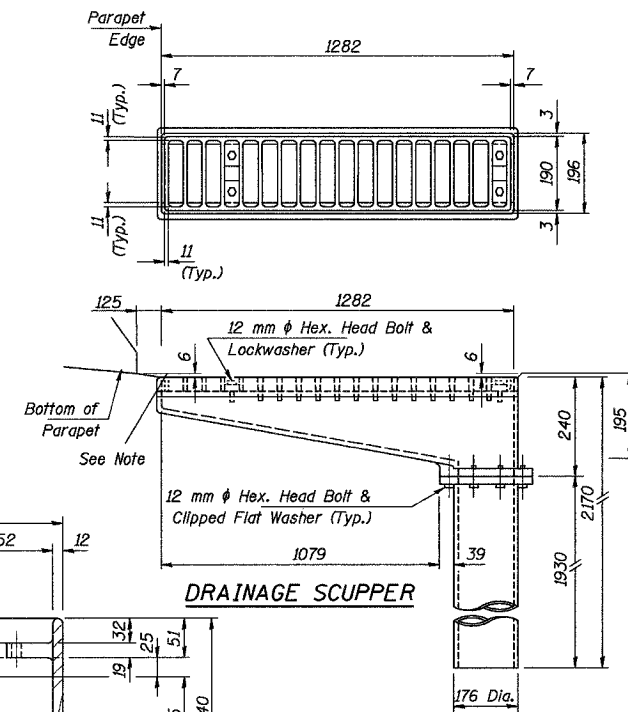
Edge of scupper to be placed at edge of parapet except when prohibited by wide top flange of beam.



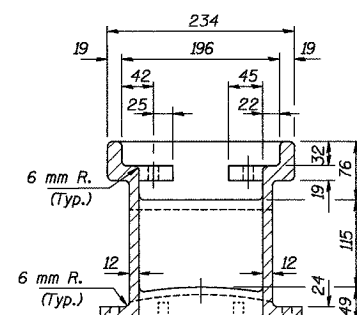
*Subject to change by manufacturer



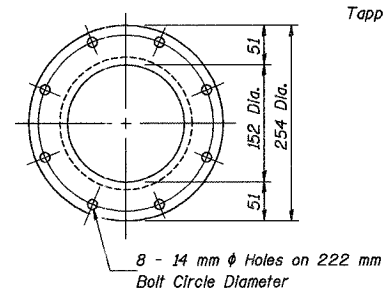
FRAME



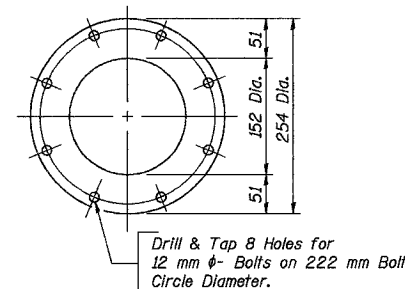
DRAINAGE SCUPPER



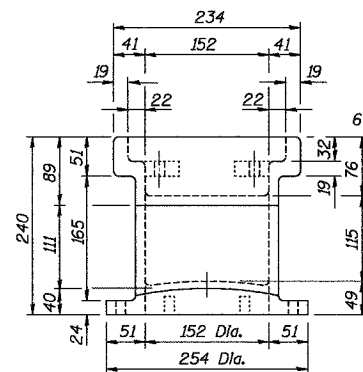
SECTION F-F



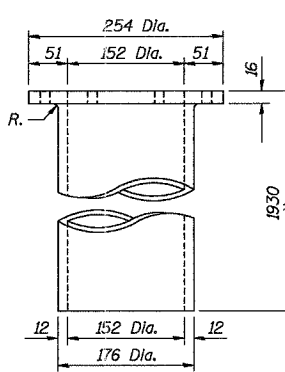
SECTION E-E



VIEW D-D



VIEW C-C



DOWNSPOUT

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Drainage Scuppers Type II	Each	8

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
DRAINAGE SCUPPER, TYPE II		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 44	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 44

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	488	1360
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

CONTRACT NO. 68200

Joint Size	"C" at 10 °C	"D" at 10 °C	
50	50	40 Min.	At S. Abut.
65	65	45 Min.	
100	75	65 Min.	At N. Abut.

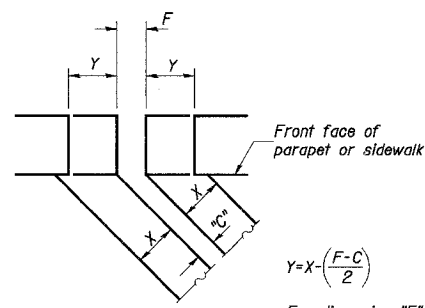
INSTALLATION NOTES

- ① Install continuous seal in roadway, parapet, curb, and sidewalk.
- ② Install anchor blocks as indicated.

NOTE A: Maximum spacing of anchor bolts shall be 300 centers.

SKEW LIMITATIONS

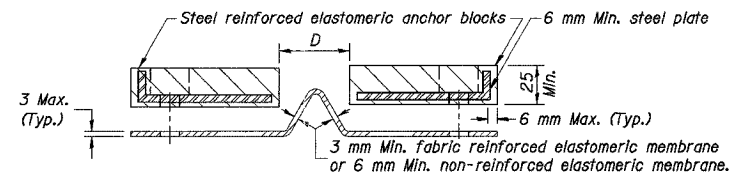
The details of the anchor blocks and the elastomeric membrane in the parapet, as shown, are for up to 50° skews. For skews greater than 50°, the anchor blocks and the elastomeric membrane, installed according to dimension "D", might require modifications to insure a minimum clearance of 40 mm from centerline of anchor studs to edge of parapet opening. The anchor blocks and the elastomeric membrane shall also be installed to the top of the parapet with the anchor studs spaced at ±300 cts.



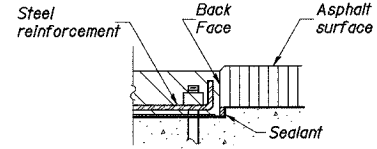
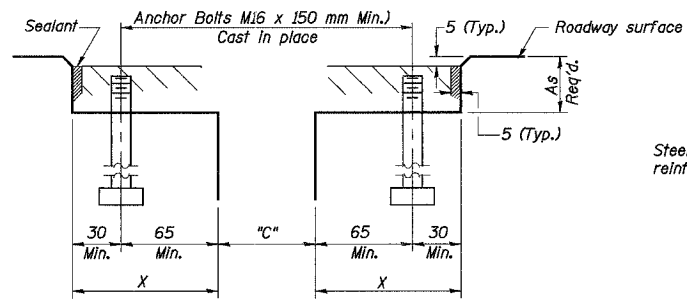
FORMING BLOCKOUT SKETCH

$$Y = X \cdot \left(\frac{F-C}{2} \right)$$

For dimension "F" see sheet 8, 11 and 12.



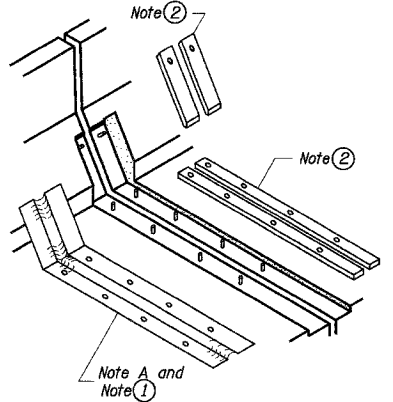
CROSS SECTION



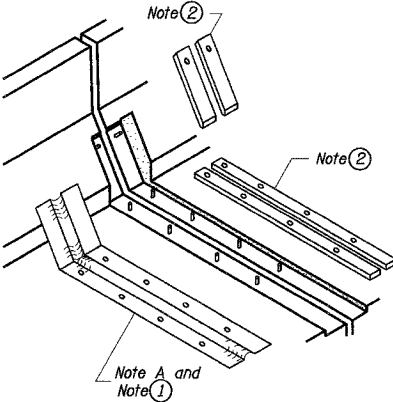
ANCHOR BLOCK WITH ASPHALT SURFACE

GENERAL NOTES

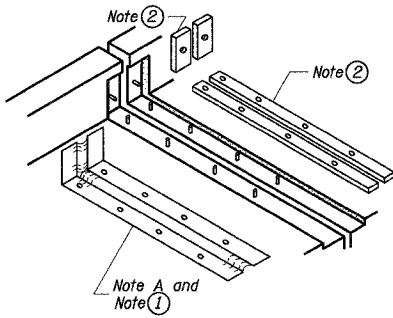
Continuous Seal Neoprene Expansion Joint shall consist of molded anchor blocks of elastomer and steel, field assembled over continuous lengths of elastomeric membrane. The elastomeric membrane shall be premolded with a single or a double upward convolution that will have a "memory" to return to its molded position upon joint closure. The convolution length shall be such that the extended length will not be greater than the manufactured length when the joint is fully expanded in its design range and will not protrude above the anchor blocks when the joint is fully compressed. Joint openings shall be adjusted according to Article 503.10(c) of the Standard Specifications when the deck is poured at an ambient temperature other than 10 °C. The parapet and roadway membrane shall be made continuous by an approved vulcanizing process. Lapping will not be permitted. All dimensions are in millimeters (mm) except as noted.



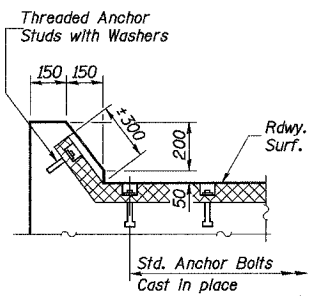
AT PARAPET



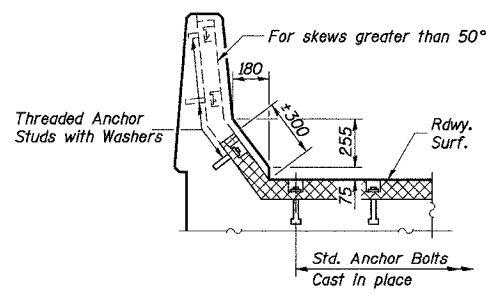
AT MEDIAN



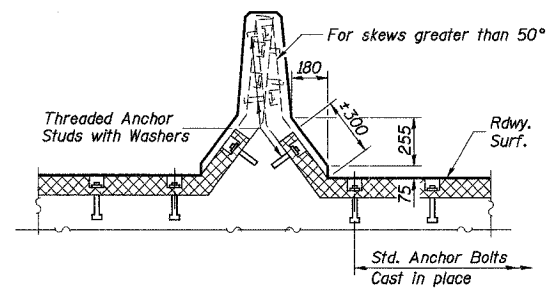
AT WALL



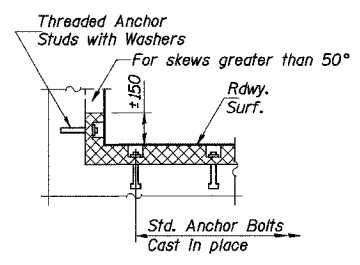
AT CURB



AT PARAPET



AT MEDIAN



AT WALL

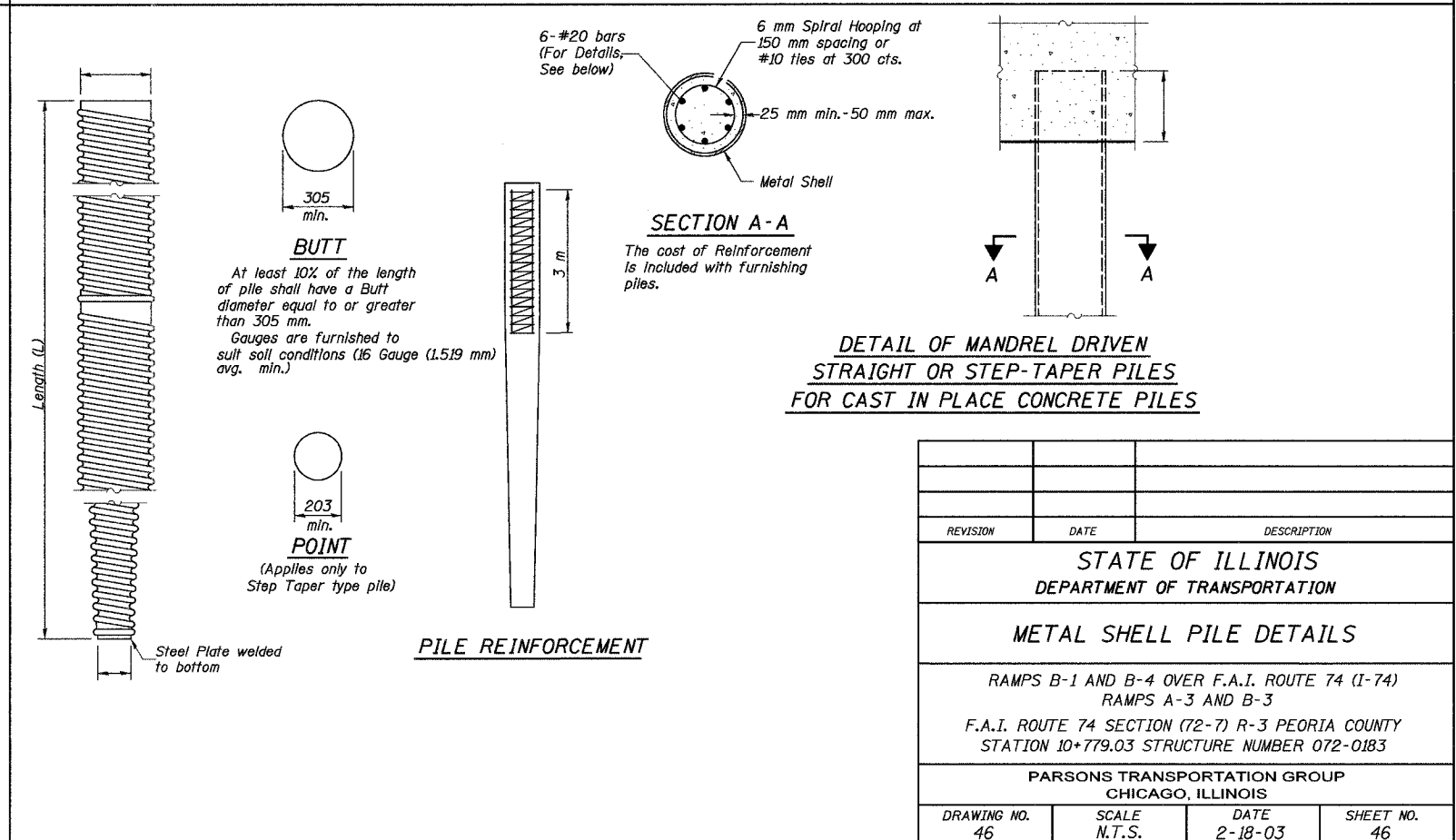
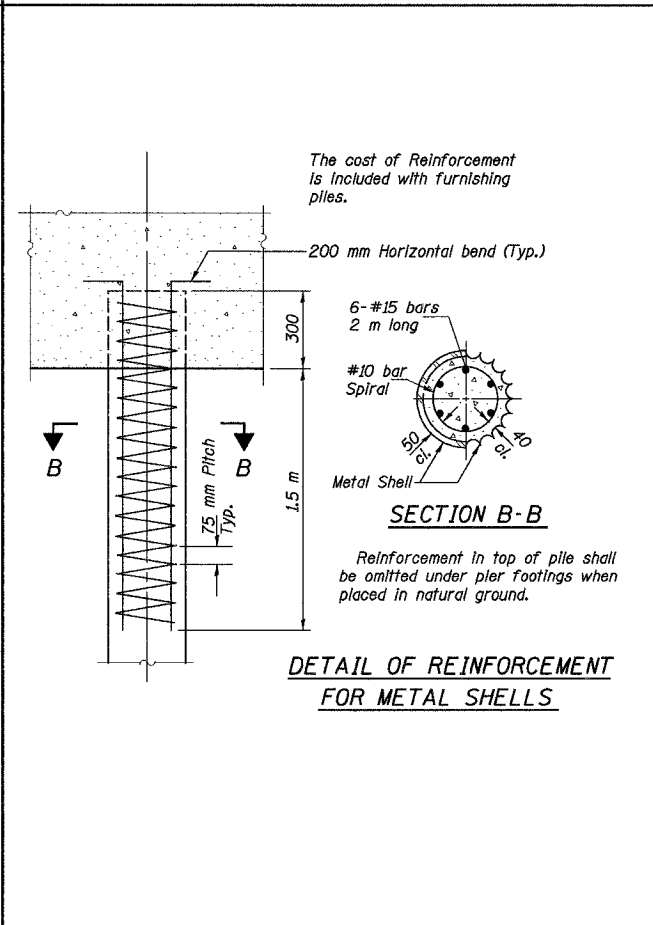
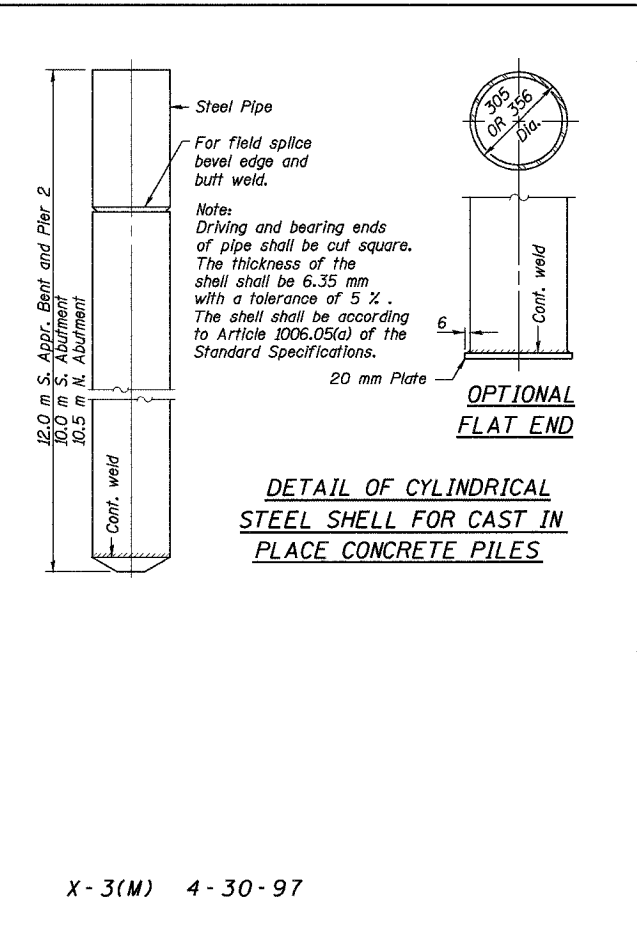
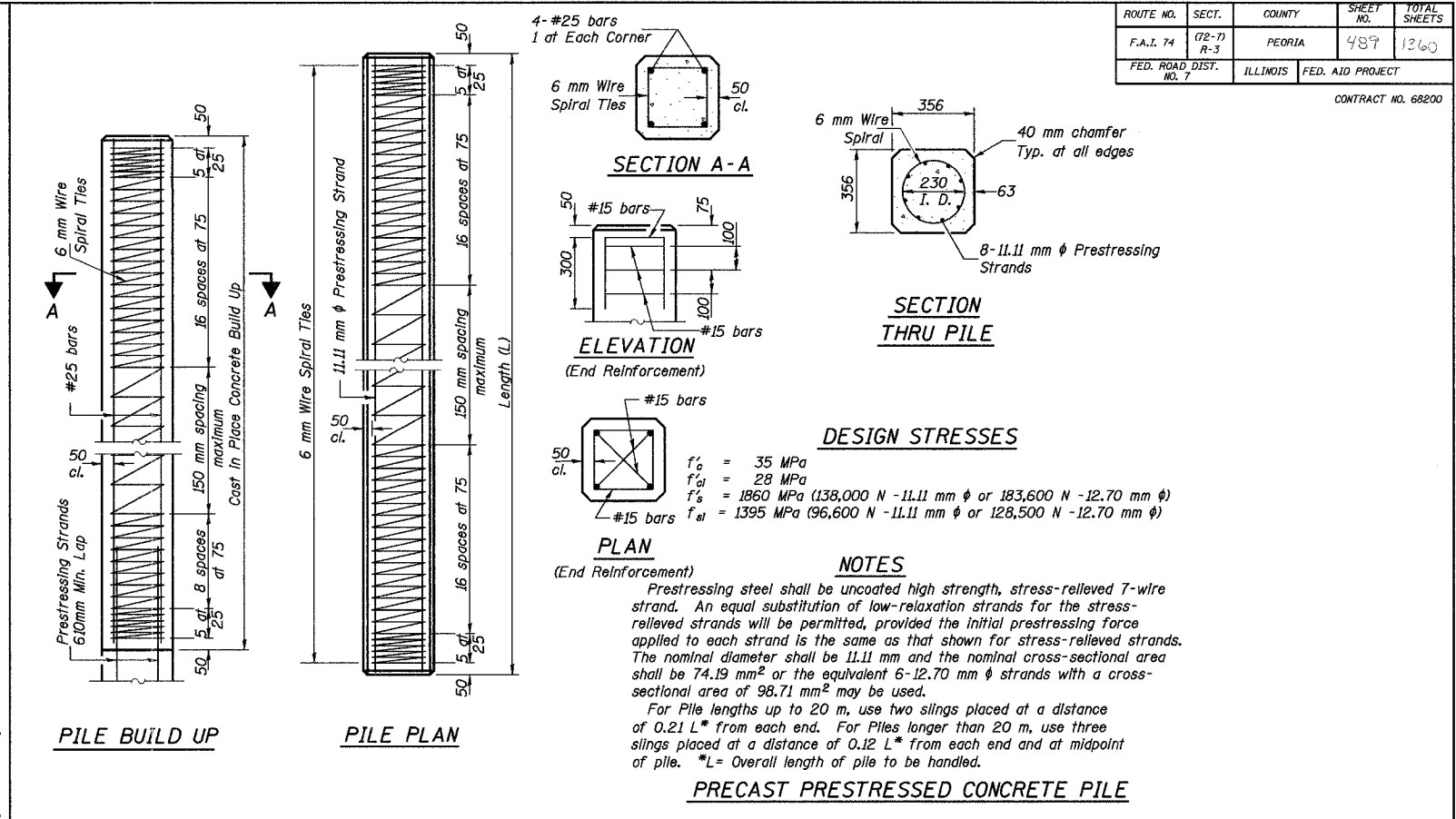
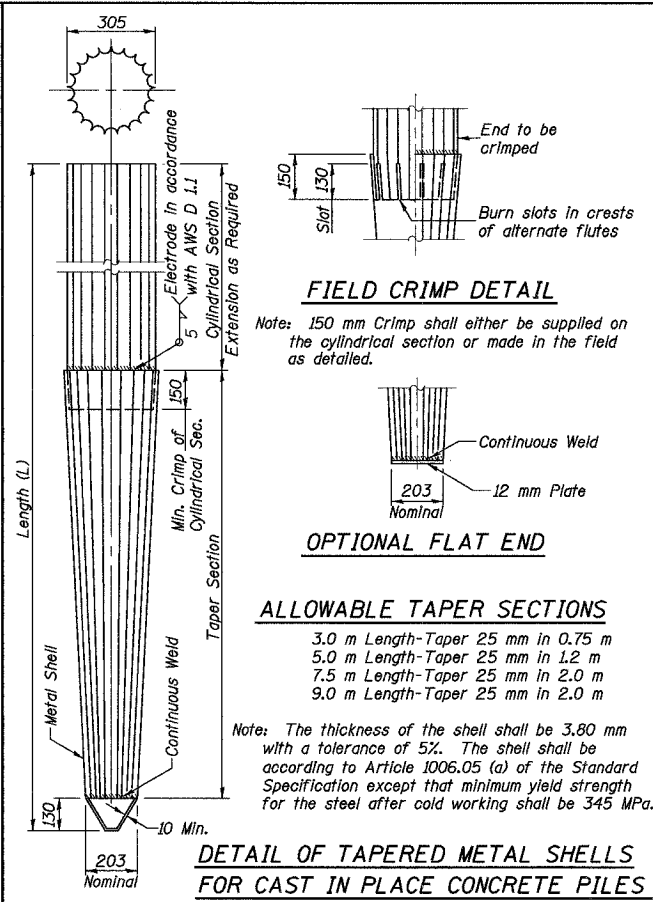
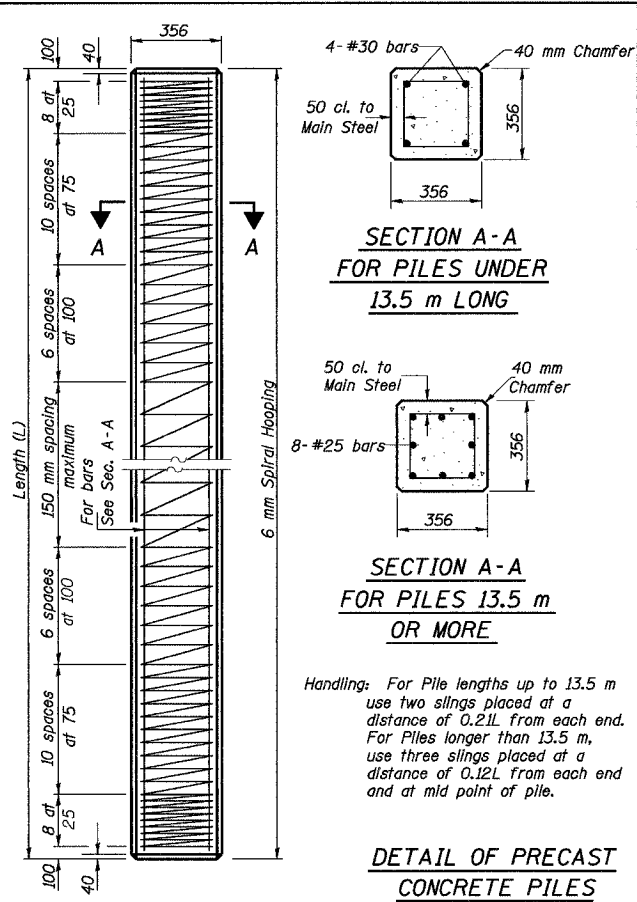
REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
NEOPRENE EXPANSION JOINT DETAILS		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 45	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 45

Date: 11/19/2004 Time: 10:14:49 AM File: P:\643996\structure\072-0183\Sheet\Tracings\JT0001-140720183.dgn

Date: 1/19/2004 Time: 10:21:12 AM

File name: P:\643996\structure\072-0183\Sheet\Tracings\SD0011-140720183.dgn

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	489	1260
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 68200				



REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
METAL SHELL PILE DETAILS		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 46	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 46

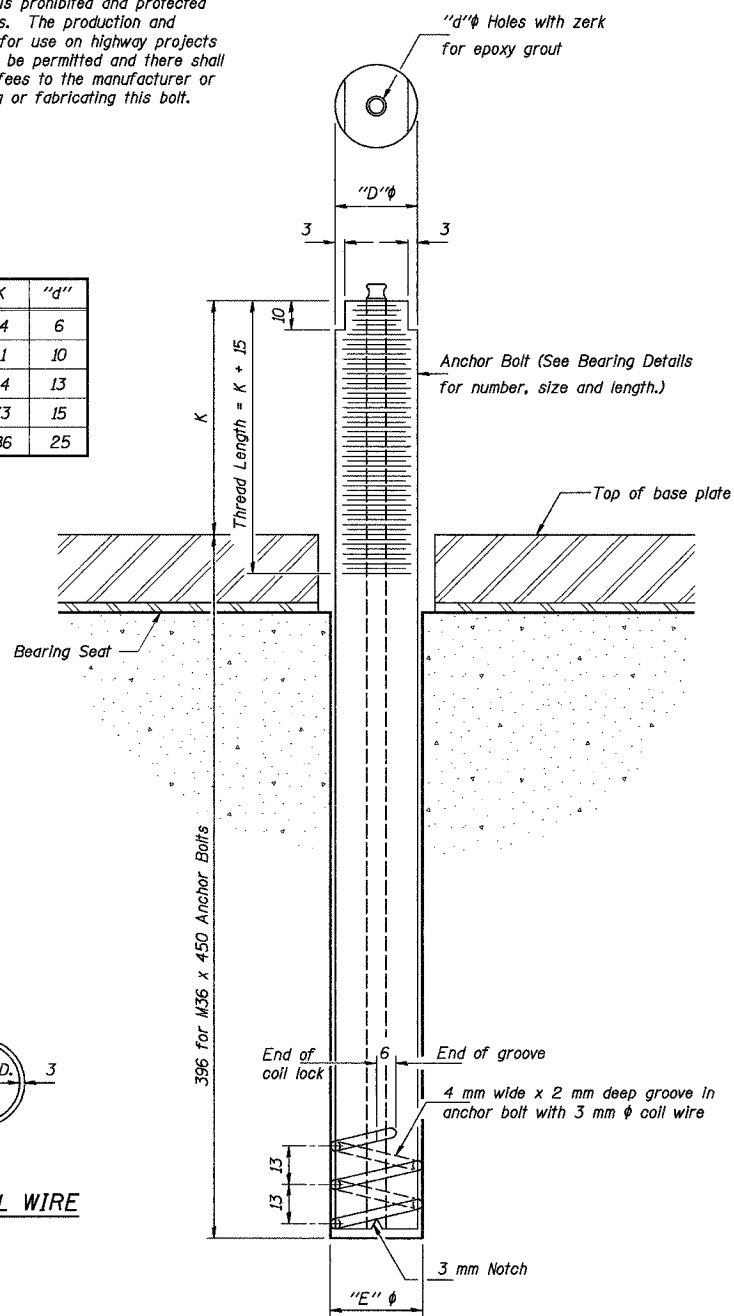
X-3(M) 4-30-97

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	490	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200

The Illinois Coil-Lock Anchor Bolt is a proprietary item which is the property of the Illinois Department of Transportation. Use, reproduction or disclosure without express written permission is prohibited and protected under Federal copyright laws. The production and the fabrication of this bolt for use on highway projects in the State of Illinois shall be permitted and there shall be no incurred charges or fees to the manufacturer or the fabricator for producing or fabricating this bolt.

D	E	H	K	"d"
24	27	20	44	6
30	33	26	51	10
36	39	32	54	13
48	51	44	73	15
64	67	60	86	25



ILLINOIS COIL-LOCK ANCHOR BOLT

MATERIALS FOR ILLINOIS COIL-LOCK ANCHOR BOLT

The anchor bolt shall be fabricated from cold drawn or hot finished seamless carbon steel mechanical tubing conforming to ASTM A 519, Grade 1026, CW and supplied with hexagonal nuts and cut washers.
The coil wire shall be made of any suitable soft steel wire.
The finished anchor bolt shall be cleaned of rust and other foreign materials and wrapped or packaged to prevent contamination until they are installed.
The epoxy grout shall be a two-component, epoxy resin bonding system conforming to ASTM C 881, Type 1, Grade 1 and of a Class suitable for the temperature at installation.

INSTALLATION PROCEDURE for the ILLINOIS COIL-LOCK ANCHOR BOLT

- With the coil wire in place, the bolt shall be inserted into the hole and turned clockwise to a snug fit in the hole. Nut and washer shall be placed on the bolt. The nut shall be tensioned until the steel base plates are held securely to the concrete bearing seat.
- Epoxy grout shall be pumped through the zerk fitting with a pressure gun. Pumping shall continue until the epoxy overflows the hole around the bolt shank. After pumping is discontinued, excess epoxy shall be immediately wiped off.

ALTERNATE ANCHOR BOLTS

The Contractor may use, at his option, the capsule or the adhesive cartridge type anchor rods that have been previously tested and given a prior approval by the Department. The Contractor shall install these anchor rods in pre-drilled holes according to the manufacturer's recommendations and procedures.
The capsule or the adhesive cartridge type anchor rods shall be a two part system composed of:
1. A threaded rod stud with nut and washer of the type specified.
2. A sealed glass capsule or a sealed glass adhesive cartridge containing premeasured amounts of the adhesive chemical.

Location	Type
S. Abut.	M 164M
Pier 1	M 164M
Pier 2	M 164M
N. Abut.	M 164M

ASTM F 1554 (Fy = 724 MPa), ASTM A 449 and AASHTO M 314 (Fy = 724 MPa) anchor bolts may be substituted for the anchor bolts shown above.

GENERAL NOTES

Holes in the masonry for anchor bolts shall be drilled through the base plates to the diameter and depth shown or according to the manufacturer's recommendation after beams or girders have been erected and adjusted.
Prior to setting the bolts, the holes shall be dry and all dust and loose particles shall be removed by the use of compressed air or vacuuming.
The anchor bolts, furnished and installed including the epoxy grout or capsules shall not be paid for separately but shall be included in the unit bid price for "Furnishing and Erecting Structural Steel".
All dimensions are in millimeters (mm) except as noted.

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
ANCHOR BOLT DETAILS		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 47	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 47

ABB-1(M) 4-30-99

Time: 10:52:50 AM

Date: 11/19/2004

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ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	491	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 68200

Time: 10:12:50 AM

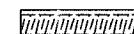
Date: 11/19/2004

Filename: F:\643996\Structure\d\072-0183\Sheet\Tracings\SD0029-1A\0720183.dgn

The diameter of this part is the same as the diameter of the bar spliced.

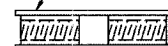
The diameter of this part is equal or larger than the diameter of bar spliced.

ROLLED THREAD DOWEL BAR



** ONE PIECE

Wire Connector



WELDED SECTIONS

BAR SPLICER ASSEMBLY ALTERNATIVES

** Heavy Hex Nuts conforming to ASTM A 563M, Grade C, D or DH may be used.

NOTES

Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars. Splicer rods shall be of minimum 400 MPa yield strength, threaded or coiled full length. All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars. Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

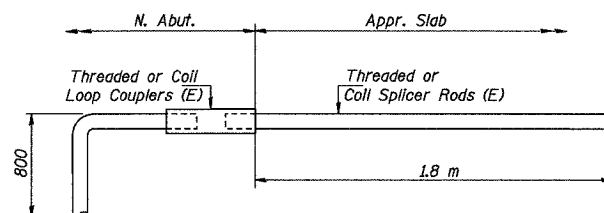
- ① Minimum Capacity = $1.25 \times 10^{-3} \times f_y \times A_s$
(Tension in kN)
- ② Minimum *Pull-out Strength = $1.25 \times 10^{-3} \times f_{s_{allow}} \times A_s$
(Tension in kN)

Where f_y = Yield strength of lapped reinforcement bars in MPa.
 $f_{s_{allow}}$ = Allowable tensile stress in lapped reinforcement bars in MPa (Service Load)
 A_s = Tensile stress area of lapped reinforcement bars (mm²).
 * = 28 day concrete

BAR SPLICER ASSEMBLIES

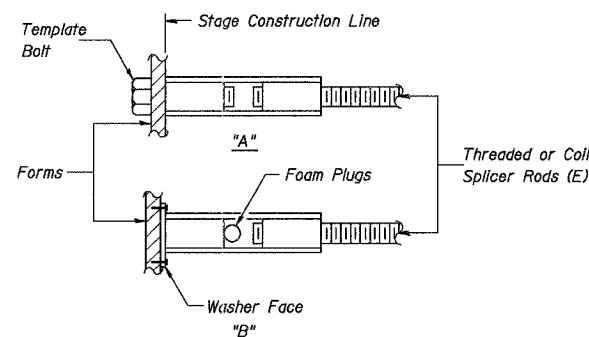
Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kN - tension	Min. Pull-Out Strength kN - tension
#15	610 mm	100	40
#20	790 mm	150	60
#25	1.04 m	250	100
#30	1.37 m	350	140

Bar splicer assemblies shall be according to Section 508 of the Standard Specifications, except as noted. The furnishing and installation of bar splicer assemblies will be measured and paid for at the contract unit price each for "BAR SPLICERS."
 All dimensions are in millimeters (mm) except as noted.



ABUTMENT
BAR SPLICER ASSEMBLY DETAIL
FOR #15 BAR

Min. Capacity = 100 kN - tension
Min. Pull-out Strength = 40 kN - tension
No. Required = 88



INSTALLATION AND SETTING METHODS

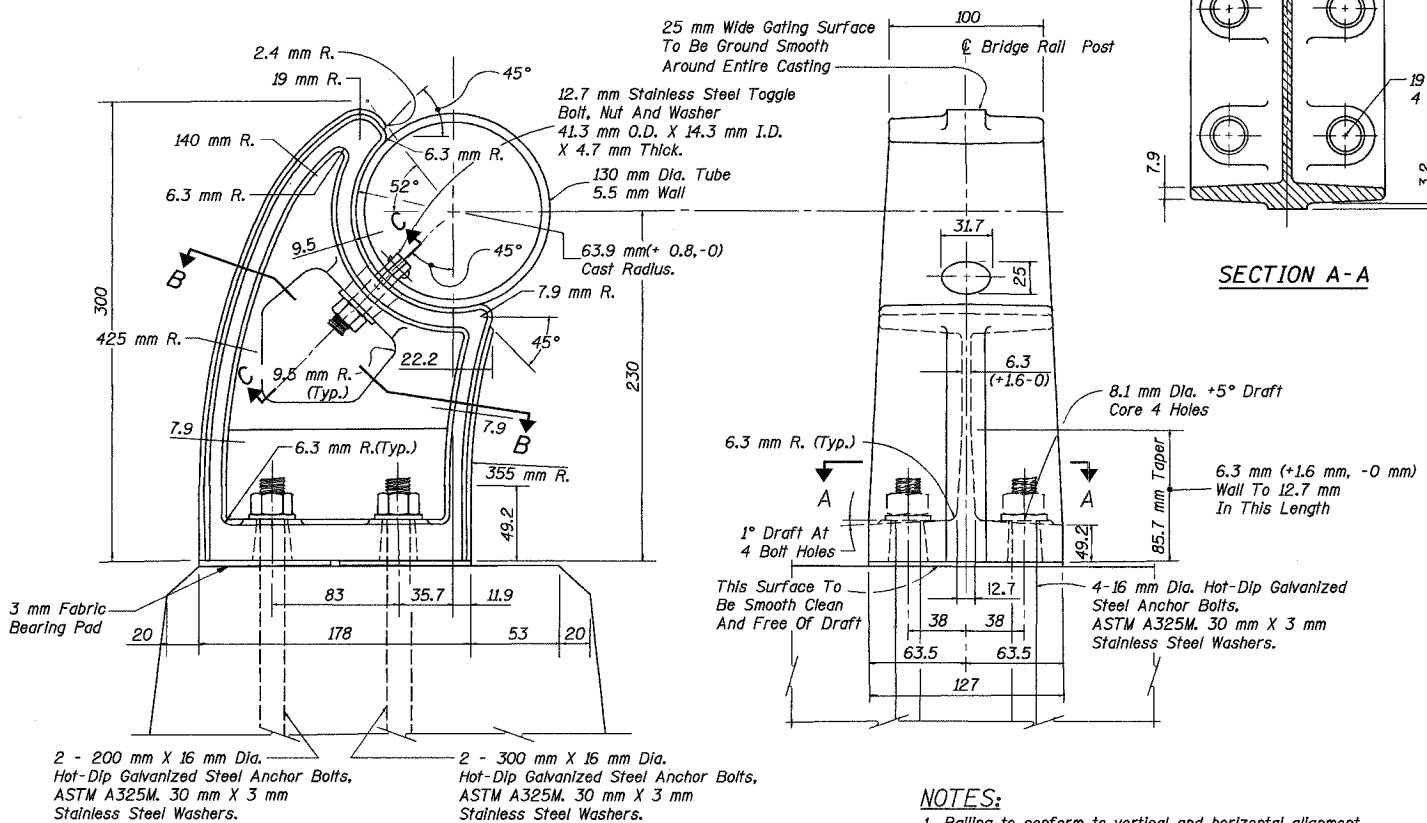
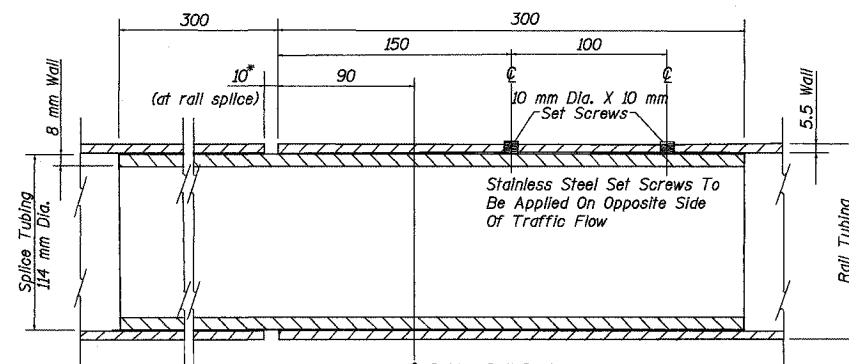
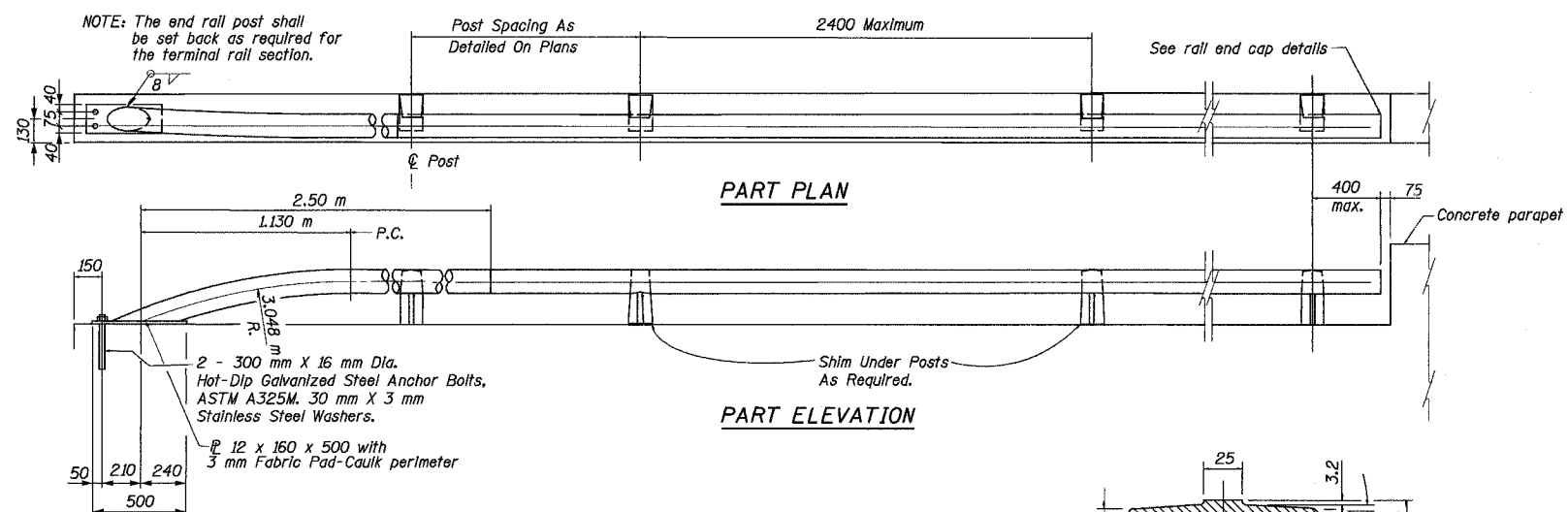
"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
BAR SPLICER ASSEMBLY DETAILS		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 48	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 48

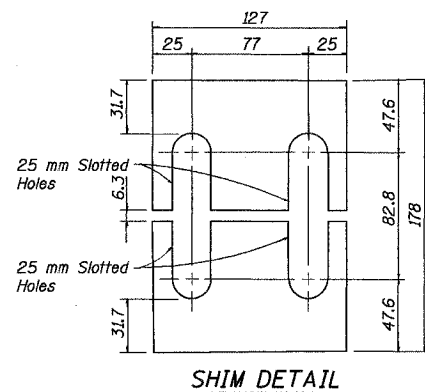
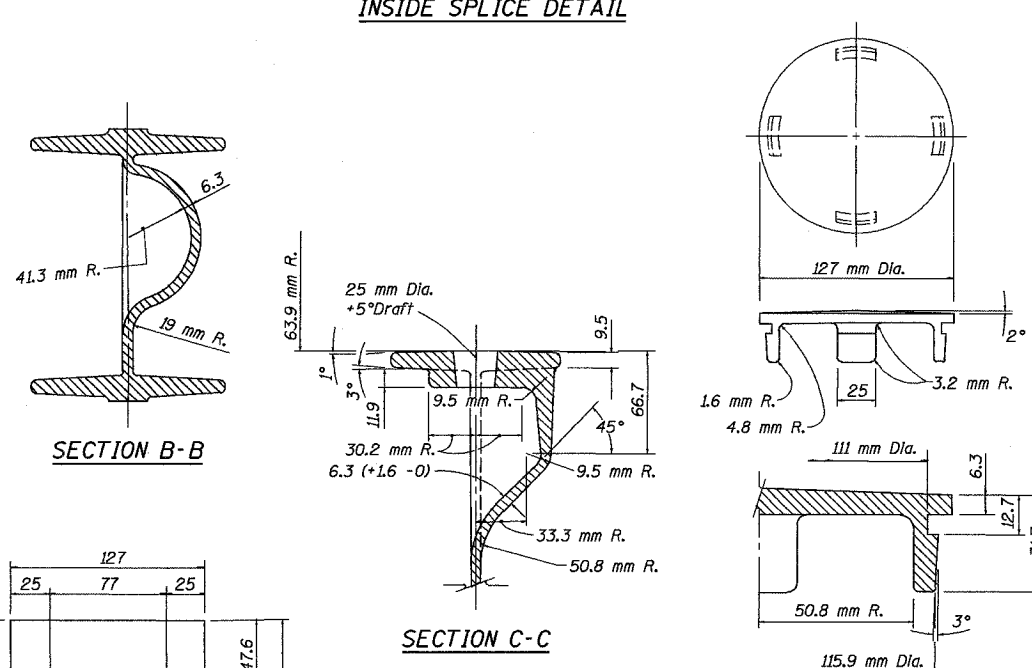
BSD-1 (M)
4-30-97

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	492	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 6B200



- NOTES:**
- Railing to conform to vertical and horizontal alignment.
 - Joint to be placed 7500 mm center to center, max.
 - Slip joint to be placed in panels to match expansion joints in deck.
 - Design weight: 9.3 kg. per meter.
 - Unless otherwise specified all draft to be 3°.
 - All unmarked Radius to be 3 mm R.
 - After fabrication, exposed surfaces of aluminum shall be given an anodic oxide coating, dyed black, conforming to the requirements of ASTM designation: B 580, Type B, Architectural Class I.



BILL OF MATERIAL

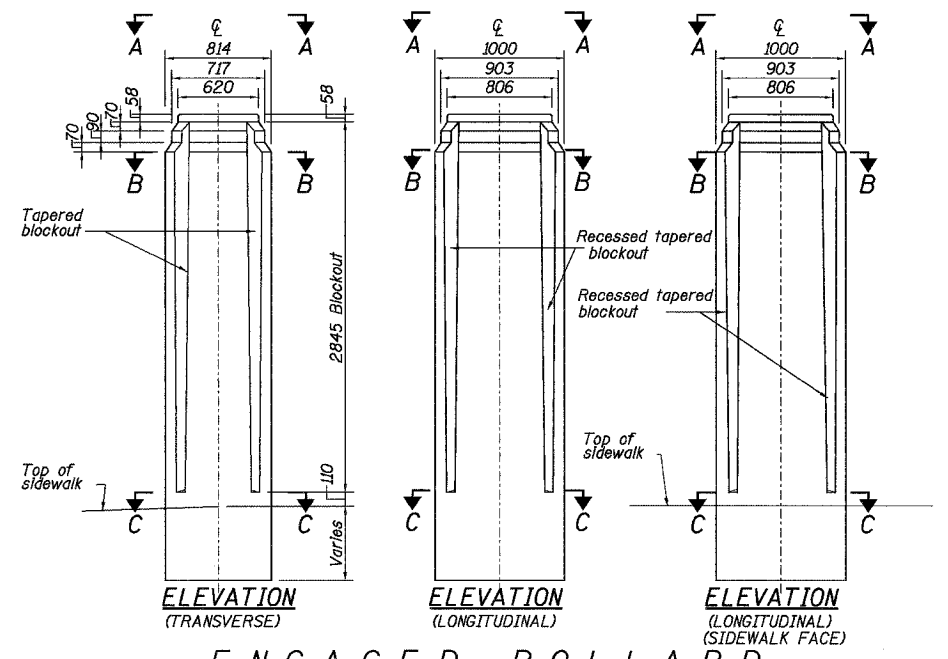
Item	Unit	Quantity
Aluminum Railing, Type H (Special)	m	258

TYPE H ALUMINUM RAILING

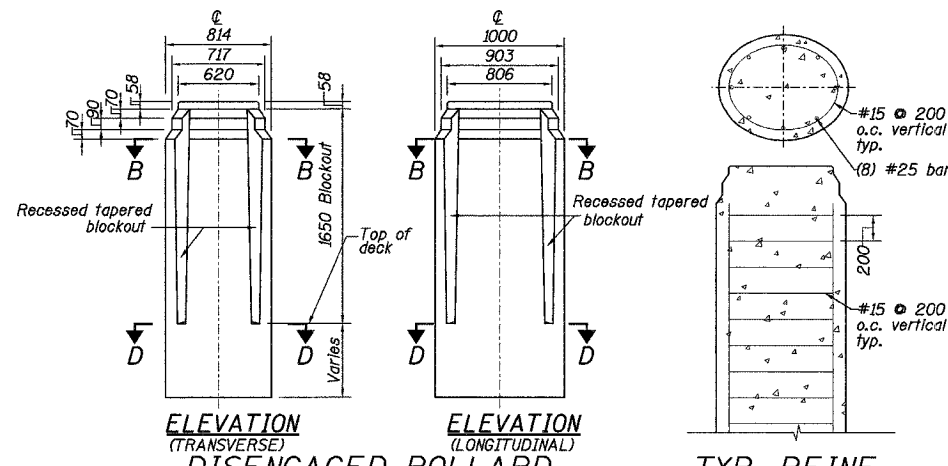
REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS		
DEPARTMENT OF TRANSPORTATION		
RAILING DETAILS - SINGLE RAIL		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3		
F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 49	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 49

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	493	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

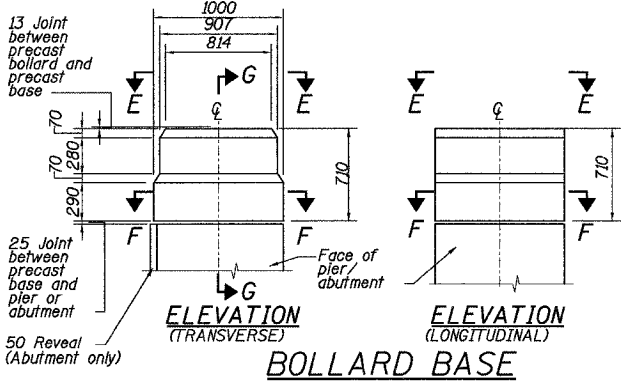
CONTRACT NO. 68200



ENGAGED BOLLARD

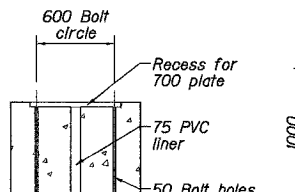


DISENGAGED BOLLARD

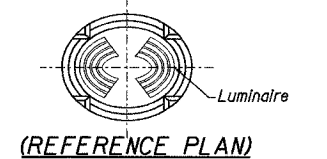


BOLLARD BASE

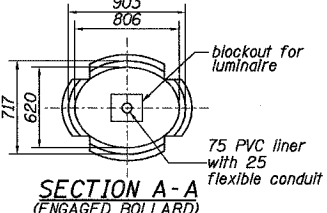
TYP. REINF.
(MIN. REINFORCEMENT SHOWN
FINAL DESIGN BY FABRICATOR)
(SIMILAR REINFORCEMENT
IN BOLLARD BASE)



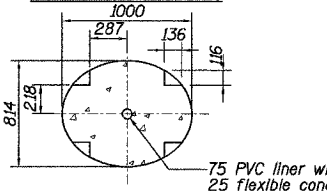
**SECTION G-G
(BOLLARD BASE)**



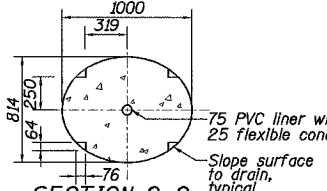
(REFERENCE PLAN)



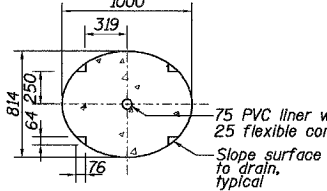
**SECTION A-A
(ENGAGED BOLLARD)
(DISENGAGED BOLLARD)**



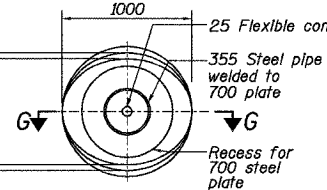
**SECTION B-B
(ENGAGED BOLLARD)
(DISENGAGED BOLLARD)**



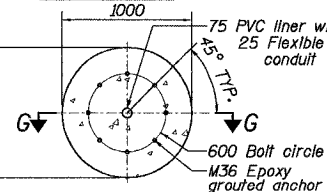
**SECTION C-C
(ENGAGED BOLLARD)**



**SECTION D-D
(DISENGAGED BOLLARD)**

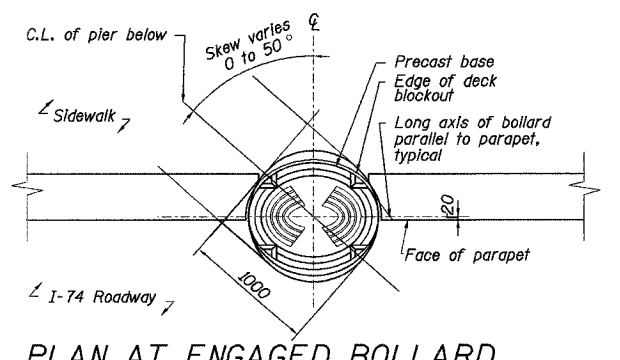


**SECTION E-E
(BOLLARD BASE)**

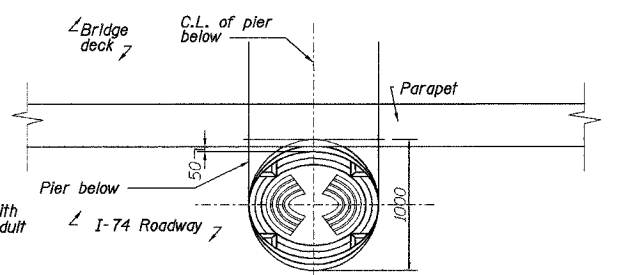


**SECTION F-F
(BOLLARD BASE)**

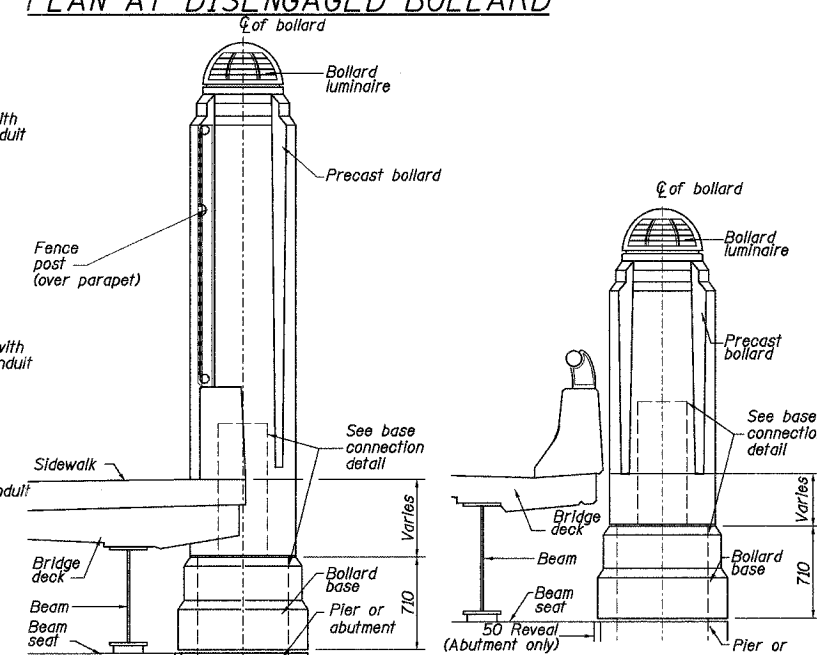
BOLLARD SECTIONS



PLAN AT ENGAGED BOLLARD

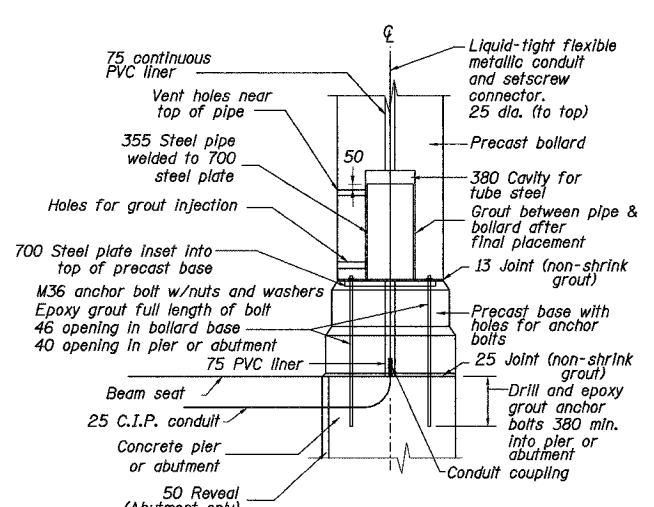


PLAN AT DISENGAGED BOLLARD

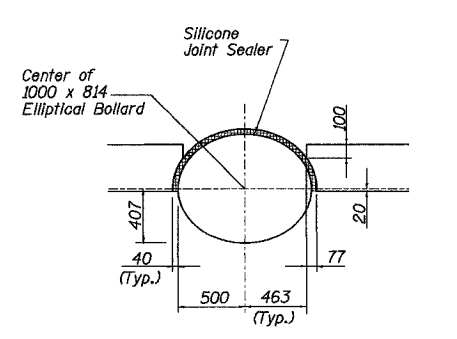


**ENGAGED BOLLARD
DISENGAGED BOLLARD
REFERENCE ELEVATIONS**

NOTES:
1. BOLLARDS, BOLLARD BASES, BOLLARD LUMINAIRES, AND CONNECTION COMPONENTS (UNLESS OTHERWISE NOTED) ARE SUPPLIED BY OTHERS. FABRICATION DETAILS AND DIMENSIONS ARE PRESENTED FOR INFORMATION ONLY. INSTALLATION IS INCLUDED IN THIS CONTRACT.
2. SOME BOLLARD BASE CONNECTION DETAILS NOT SHOWN. FINAL CONNECTION DESIGN AND DETAILING BY BOLLARD FABRICATOR.
3. NON-SHRINK GROUT, EPOXY GROUT, FLEXIBLE CONDUIT AND CONNECTOR, AND SILICONE JOINT SEALER TO BE SUPPLIED AND INSTALLED BY INSTALLATION CONTRACTOR.



BASE CONNECTION DETAIL



**SILICONE JOINT SEALER
Engaged Bollard**

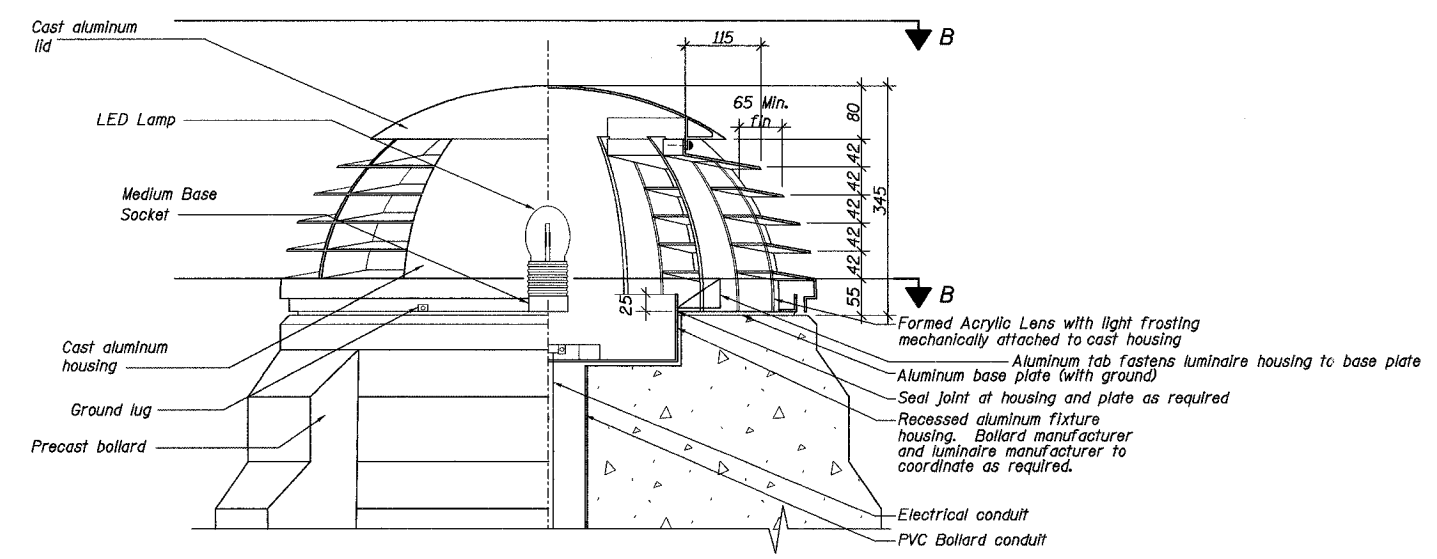
REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
PRECAST CONCRETE BOLLARD		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 50	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 50

Date: 11/19/2004 Time: 10:33:41 AM

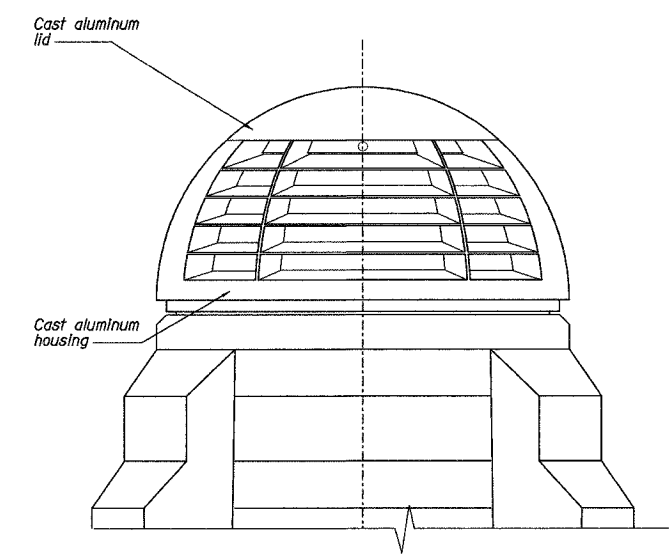
Filename: P:\643996\structure\072-0183\Sheet\Tracings\SD0023-A0720183.dgn

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	494	1360
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
7				

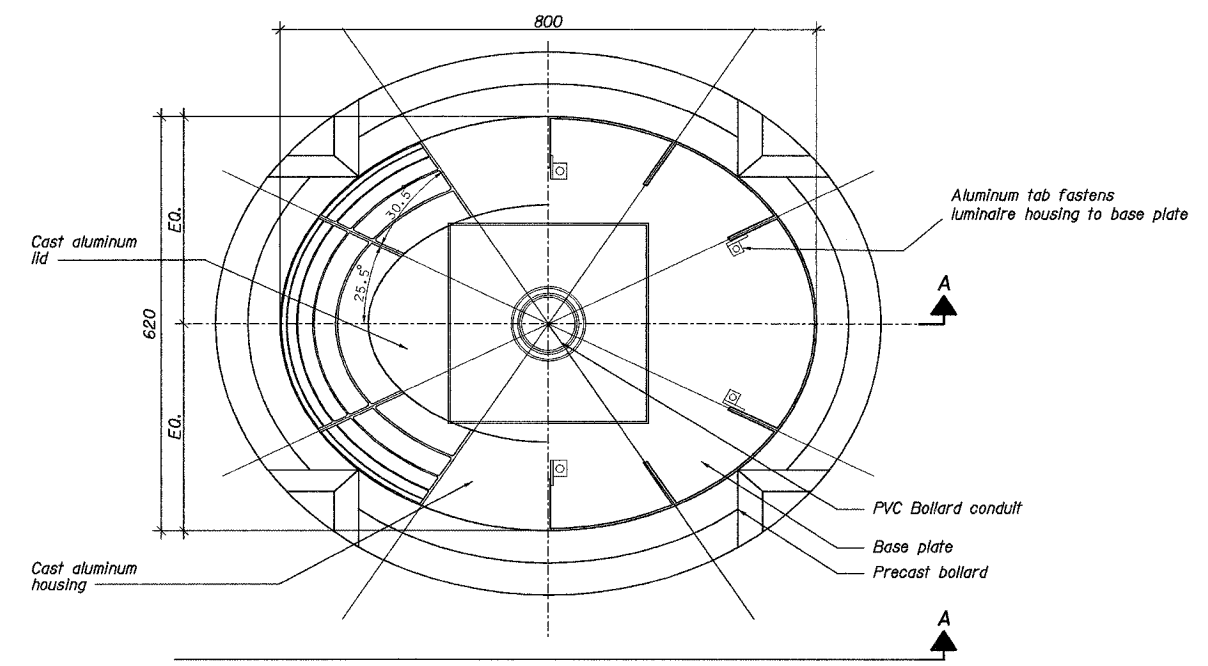
CONTRACT NO. 68200



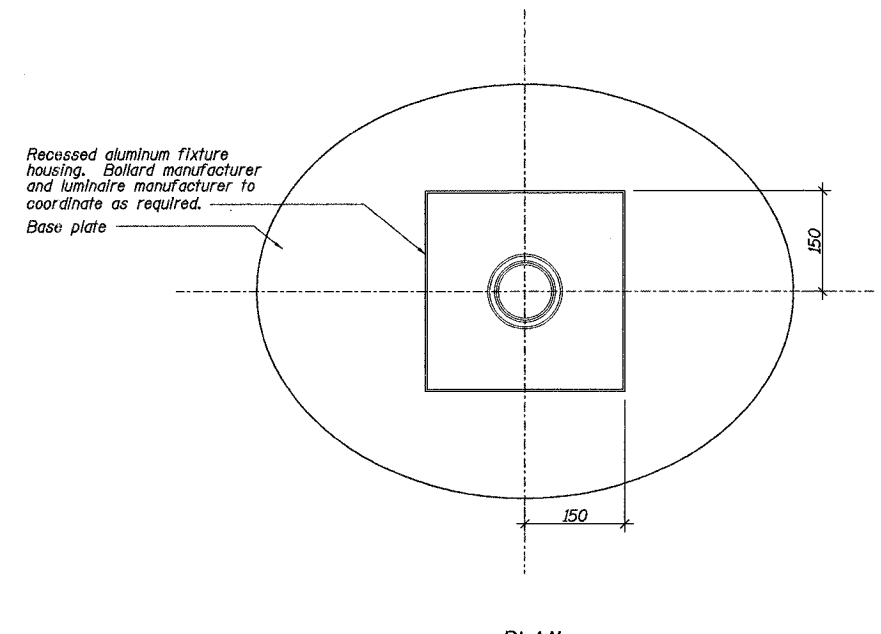
LONGITUDINAL ELEVATION/SECTION A-A



TRANSVERSE ELEVATION



PLAN/SECTION B-B



PLAN

Luminaire Specifications

Cover: 3 thick all aluminum construction
 Pull-down catches and strikes: Stainless steel
 Cover Gasket: 3 thick (die cut) high density closed-cell EPDM with P.S.A. backing
 Fasteners: Stainless steel, cast-in-place Hell-Coll
 Finish: Factory applied - silver metallic

NOTES:

1. Luminaire supplied by others. Installation in this contract.
2. Fixture to Bear "U.L." Label suitable for wet locations"
3. Socket to be supplied with disconnect plugs.
4. Fixture supplied with vinyl coated stainless steel safety cable between cast aluminum lid and baseplate.

REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS		
DEPARTMENT OF TRANSPORTATION		
BOLLARD LUMINAIRE		
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO. 51	SCALE N.T.S.	DATE 2-18-03
		SHEET NO. 51

2-12-02

Date: 11/19/2004 Time: 10:40:08 AM File: P:\643996\structural\072-0183\Sheet\Drawings\SD002-I-A0720183.dgn

CLAUDE H. HURLEY COMPANY		BORING LOG															
PROJECT NO. 3-380-D5		BORING NO. SB-29 (CONT.)															
PROJECT FAI-74 IMPROVEMENTS - WAR MEMORIAL DRIVE CORRIDOR																	
LOCATION BRIDGE NO. 072-0183 RAMP B-4BL 10+285.2 0.7mR PEORIA & TAZEWELL COUNTIES, ILLINOIS																	
DRILLING CONTRACTOR D & G DRILLING, INC.																	
DATE OF DRILLING: STARTED 11-22-94 COMPLETED 11-22-94 SURFACE ELEVATION 196.65																	
DRILLED BY D. ROESEL												LOGGED BY K. OLSON					
Elev	CLASSIFICATION	Depth	GROUNDWATER DATA				DRILLING METHOD										
			N	Qu	w	Yd	DATE	DEPTH	HOUR	RIG TYPE	AUGER TYPE-DEPTH	CASING TYPE-DEPTH	SAMPLER TYPE				
			N	Qu	w	Yd											

CLAUDE H. HURLEY COMPANY		BORING LOG															
PROJECT NO. 3-380-D5		BORING NO. SB-294															
PROJECT FAI-74 IMPROVEMENTS - WAR MEMORIAL DRIVE CORRIDOR																	
LOCATION BRIDGE NO. 072-0183 RAMP B-1BL 10+828.2 2.7mL PEORIA & TAZEWELL COUNTIES, ILLINOIS																	
DRILLING CONTRACTOR CENTRAL ILLINOIS DRILLING COMPANY																	
DATE OF DRILLING: STARTED 3-5-96 COMPLETED 3-7-96 SURFACE ELEVATION 196.38																	
DRILLED BY R. HULF												LOGGED BY S. ARENDS					
Elev	CLASSIFICATION	Depth	GROUNDWATER DATA				DRILLING METHOD										
			N	Qu	w	Yd	DATE	DEPTH	HOUR	RIG TYPE	AUGER TYPE-DEPTH	CASING TYPE-DEPTH	SAMPLER TYPE				
			N	Qu	w	Yd											

CLAUDE H. HURLEY COMPANY		BORING LOG															
PROJECT NO. 3-380-D5		BORING NO. SB-294 (CONT.)															
PROJECT FAI-74 IMPROVEMENTS - WAR MEMORIAL DRIVE CORRIDOR																	
LOCATION BRIDGE NO. 072-0183 RAMP B-1BL 10+828.2 2.7mL PEORIA & TAZEWELL COUNTIES, ILLINOIS																	
DRILLING CONTRACTOR CENTRAL ILLINOIS DRILLING COMPANY																	
DATE OF DRILLING: STARTED 3-5-96 COMPLETED 3-7-96 SURFACE ELEVATION 196.38																	
DRILLED BY R. HULF												LOGGED BY S. ARENDS					
Elev	CLASSIFICATION	Depth	GROUNDWATER DATA				DRILLING METHOD										
			N	Qu	w	Yd	DATE	DEPTH	HOUR	RIG TYPE	AUGER TYPE-DEPTH	CASING TYPE-DEPTH	SAMPLER TYPE				
			N	Qu	w	Yd											

LEGEND

A-1 to A-8 (and subgroups)	Engineering classifications of soil in accordance with AASHTO M 145 standard specification.	Qu, kPa	Unconfined compression strength of soil in kilopascals determined in accordance with AASHTO T 208 standard specification.
Silty Clay Loam	Textural classification of soil in accordance with IDOT Triangular Chart.	w, %	Natural moisture content of soil and bedrock in percent determined in accordance with AASHTO T 265 standard specification and AASHTO T 265/ASTM D 2216 for bedrock.
Laminated Coal Shale	Textural and engineering classification of bedrock in accordance with conventional practice.	Yd, kgpm ³	Dry unit weight of soil and bedrock in kilograms per cubic meter determined in accordance with standard practice.
N, Bp0.15m	N-value or standard penetration test value. Number of blows required to drive a standard split-spoon sampler 0.15 m as conducted in accordance with AASHTO T 206 standard specification.		

DD	Water Level During Drilling	FA	Flight Auger
BAR	Water Level Before Auger Removal	RW	Rotary Wash
AAR	Water Level After Auger Removal	HSA	Hollow Stem Auger
DC	Dry Cave Level	SAMPLE TYPE	
WC	Wet Cave Level	AU	Auger
d	Days	SS	Standard Split-barrel
h	Hours	ST	Thin-walled Tube
		DB	Core Barrel

NOTES

1. The abbreviations, symbols and definitions in this Legend are commonly used and understood in the engineering and construction practices and are presented only for information and communication.

2. The Geotechnical Data presented in this Legend and on the Boring Logs are to be interpreted by personnel educated, trained, experienced and licensed to practice Geotechnical Engineering, and in direct communication with the Claude H. Hurley Company.

Notes:
For additional boring logs of Boring No. SB-29, see Sheet 52.

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION			
BORING LOGS II			
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183			
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS			
DRAWING NO. 53	SCALE N.T.S.	DATE 2-18-03	SHEET NO. 53

Date: 11/23/2004 Time: 10:06 AM File: P:\643996\structure\072-0183\Sheet\Tracings\BL0002-AI0720183.dgn

Time: 09:24:37 AM

Date: 11/23/2004

Filename: P:\643996\structural\072-0183\Sheet\Tracings\BL0003-1A0720183.dgn

CLAUDE H. HURLEY COMPANY											
BORING LOG						BORING NO. SB-295					
PROJECT NO. 3-380-D5											
PROJECT FAI-74 IMPROVEMENTS - WAR MEMORIAL DRIVE CORRIDOR											
LOCATION BRIDGE NO. 072-0183 RAMP B-4BL 10+324.0 8.7mR PEORIA & TAZEWELL COUNTIES, ILLINOIS											
DRILLING CONTRACTOR CENTRAL ILLINOIS DRILLING COMPANY											
DATE OF DRILLING: STARTED 3-5-96 COMPLETED 3-5-96 SURFACE ELEVATION 190.46											
DRILLED BY R. WULF LOGGED BY S. AREMS											
Elev	CLASSIFICATION	Depth	N Bp0.15m	Q _u KPa	w %	Y _d Kgpm ³	GROUNDWATER DATA			DRILLING METHOD	
							DATE	DEPTH	HOUR	RIG TYPE	CME-55
	FILL: BR SILTY CLAY LOAM, A-6						DD	3-5	4.72		AUGER TYPE-DEPTH 0.08m HSA-15.5m
							DC	3-5	6.52	0	CASING TYPE-DEPTH -
								3-6	5.03	24	SAMPLER TYPE AU-SS
189.85	BR SILTY LOAM, A-4	6			20						
189.55	BR GRAVELLY SAND, A-1-b	7			6						
189.48	BR SILT, A-4	6			24						
187.72	BR SILTY CLAY, A-6	6		385	22						
187.11	GR SILTY CLAY, A-6	2		200	23						
186.18	BR SILTY CLAY, A-6	3		430	25						
185.74	BR SAND, A-3	9			13						
185.43	BR SANDY LOAM, A-2-4	6			18						
184.67	BR SILTY LOAM, A-4	10			17						
183.33	GR SILTY CLAY, A-6	7			15						

CLAUDE H. HURLEY COMPANY											
BORING LOG						BORING NO. SB-295 (CONT.)					
PROJECT NO. 3-380-D5											
PROJECT FAI-74 IMPROVEMENTS - WAR MEMORIAL DRIVE CORRIDOR											
LOCATION BRIDGE NO. 072-0183 RAMP B-4BL 10+324.0 8.7mR PEORIA & TAZEWELL COUNTIES, ILLINOIS											
DRILLING CONTRACTOR CENTRAL ILLINOIS DRILLING COMPANY											
DATE OF DRILLING: STARTED 3-5-96 COMPLETED 3-5-96 SURFACE ELEVATION 190.46											
DRILLED BY R. WULF LOGGED BY S. AREMS											
Elev	CLASSIFICATION	Depth	N Bp0.15m	Q _u KPa	w %	Y _d Kgpm ³	GROUNDWATER DATA			DRILLING METHOD	
							DATE	DEPTH	HOUR	RIG TYPE	CME-55
	GR SILTY LOAM, A-4						DD	3-5	4.72		AUGER TYPE-DEPTH 0.08m HSA-15.5m
							DC	3-5	6.52	0	CASING TYPE-DEPTH -
								3-6	5.03	24	SAMPLER TYPE AU-SS
176.29	GR SAND, A-3	15			18						
		20			17						
		27			17						
		30			17						
		15			17						
		8			17						
		18			17						
		27			17						
174.92	END OF BORING										

CLAUDE H. HURLEY COMPANY											
BORING LOG						BORING NO. SB-296					
PROJECT NO. 3-380-D5											
PROJECT FAI-74 IMPROVEMENTS - WAR MEMORIAL DRIVE CORRIDOR											
LOCATION BRIDGE NO. 072-0183 RAMP B-1BL 10+768.1 2.3mL PEORIA & TAZEWELL COUNTIES, ILLINOIS											
DRILLING CONTRACTOR CENTRAL ILLINOIS DRILLING COMPANY											
DATE OF DRILLING: STARTED 3-4-96 COMPLETED 3-4-96 SURFACE ELEVATION 189.00											
DRILLED BY R. WULF LOGGED BY S. AREMS											
Elev	CLASSIFICATION	Depth	N Bp0.15m	Q _u KPa	w %	Y _d Kgpm ³	GROUNDWATER DATA			DRILLING METHOD	
							DATE	DEPTH	HOUR	RIG TYPE	CME-55
	FILL: BR TO DK BR. CLAY LOAM, A-3						DD	3-4	2.90		AUGER TYPE-DEPTH 0.08m HSA-14.3m
							AAR	3-4	4.88	0	CASING TYPE-DEPTH -
							WC	3-8	0.30	4d	SAMPLER TYPE AU-SS
188.39	BR GRAVELLY SAND, A-1-b	2			15						
187.78	BR SILTY CLAY LOAM, A-4	3		240	15						
187.02	BR SAND, A-3	6			16						
		8			16						
		8			16						
		9			16						
		6			14						
		8			14						
		12		450	12						
185.04	BR SILTY CLAY LOAM, A-4	6		385	24						
184.46	BR SANDY LOAM, A-2-4	12			11						
183.97	BR CLAY LOAM, A-4	6			10						
		8			10						
		11			10						
183.21	GR SILTY CLAY LOAM, A-4	20			20						
	COBBLE	20			20						
182.45	GR LOAM, A-4	15			16						
181.99	GR SILTY CLAY LOAM, A-4	6		190	11						
		8		190	18						
		11		190	18						

LEGEND

A-1 to A-8 Engineering classifications of soil in accordance with AASHTO M 145 standard specification.

Silty Clay Loam Textural classification of soil in accordance with IDOT Triangular Chart.

Laminated Coal Shale Textural and engineering classification of bedrock in accordance with conventional practice.

N, Bp0.15m N-value or standard penetration test value. Number of blows required to drive a standard split-spoon sampler 0.15 m as conducted in accordance with AASHTO T 206 standard specification.

Q_u, kPa Unconfined compression strength of soil in kilopascals determined in accordance with AASHTO T 208 standard specification.

w, % Natural moisture content of soil and bedrock in percent determined in accordance with AASHTO T 265 standard specification and AASHTO T 265/ASTM D 2216 for bedrock.

Y_d, kgpm³ Dry unit weight of soil and bedrock in kilograms per cubic meter determined in accordance with standard practice.

GROUNDWATER DATA

DD Water Level During Drilling

BAR Water Level Before Auger Removal

AAR Water Level After Auger Removal

DC Dry Cave Level

WC Wet Cave Level

d Days

h Hours

DRILLING METHOD

FA Flight Auger

RW Rotary Wash

HSA Hollow Stem Auger

SAMPLER TYPE

AU Auger

SS Standard Split-barrel

ST Thin-walled Tube

DB Core Barrel

NOTES

1. The abbreviations, symbols and definitions in this Legend are commonly used and understood in the engineering and construction practices and are presented only for information and communication.

2. The Geotechnical Data presented in this Legend and on the Boring Logs are to be interpreted by personnel educated, trained, experienced and licensed to practice Geotechnical Engineering, and in direct communication with the Claude H. Hurley Company.

Notes:
For additional boring logs of Boring No. SB-296, see Sheet 55.

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION			
BORING LOGS III			
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183			
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS			
DRAWING NO. 54	SCALE N.T.S.	DATE 2-18-03	SHEET NO. 54

Time: 09:17:23 AM

Date: 11/23/2004

Filename: F:\643996\Structural\072-0183\Sheet\Tracings\BL0004-1A0720183.dgn

CLASSIFICATION		Depth	N	Q _v	w	Y _d	GROUNDWATER DATA			DRILLING METHOD					
			Bp0.15m	KPa	%	Kgpm ³	DATE	DEPTH	HOUR	RIG TYPE	CME-55				
GR SAND, A-3		14	15	-	15	-	DD	3-4	2.90	-	AUGER TYPE-DEPTH	0.08m HSA-14.6m			
		15	27	-	-	-	AAR	3-4	4.88	0	CASING TYPE-DEPTH	-			
		17	33	-	18	-	WC	3-8	0.30	4d	SAMPLER TYPE	AU-SS			
		174.22	37	-	-	-	END OF BORING								

CLASSIFICATION		Depth	N	Q _v	w	Y _d	GROUNDWATER DATA			DRILLING METHOD					
			Bp0.15m	KPa	%	Kgpm ³	DATE	DEPTH	HOUR	RIG TYPE	CME-850				
TOPSOIL FILL: BK BR SILTY CLAY LOAM, A-6		11.89	AU	-	17	-	DD	3-4	11.89	-	AUGER TYPE-DEPTH	0.08m HSA-23.2m			
		11.89	NC	-	-	-	NC	3-4	11.89	0	CASING TYPE-DEPTH	-			
		11.80	-	-	-	-	-	3-6	11.80	48	SAMPLER TYPE	AU-SS			
BR, GR & BK BR SILTY CLAY LOAM, A-6		187.77	4	285	19	-	DD	3-4	11.89	-	AUGER TYPE-DEPTH	0.08m HSA-23.2m			
BR TO RD BR CLAY LOAM, A-6		187.01	2	240	24	-	NC	3-4	11.89	0	CASING TYPE-DEPTH	-			
FILL: BR SILTY CLAY LOAM, A-4		187.01	2	95	14	-	DD	3-4	11.89	-	AUGER TYPE-DEPTH	0.08m HSA-23.2m			
SAND, A-1-D SEAM		187.01	2	115	15	-	NC	3-4	11.89	0	CASING TYPE-DEPTH	-			
BR SILTY CLAY LOAM, A-4		185.20	2	105	21	-	DD	3-4	11.89	-	AUGER TYPE-DEPTH	0.08m HSA-23.2m			
BR SANDY LOAM, A-2-A		185.20	2	200	23	-	NC	3-4	11.89	0	CASING TYPE-DEPTH	-			
BR GR SILTY CLAY LOAM, A-4		182.75	6	140	19	-	DD	3-4	11.89	-	AUGER TYPE-DEPTH	0.08m HSA-23.2m			
BR SANDY LOAM, A-2-A		182.75	6	260	25	-	NC	3-4	11.89	0	CASING TYPE-DEPTH	-			
BR SILTY CLAY LOAM, A-6		182.44	4	285	18	-	DD	3-4	11.89	-	AUGER TYPE-DEPTH	0.08m HSA-23.2m			
BR, GR & BK BR SILTY CLAY LOAM, A-6		182.44	4	165	26	-	NC	3-4	11.89	0	CASING TYPE-DEPTH	-			
BR GRAVELLY SAND, A-1-B		182.44	5	-	-	-	DD	3-4	11.89	-	AUGER TYPE-DEPTH	0.08m HSA-23.2m			

CLASSIFICATION		Depth	N	Q _v	w	Y _d	GROUNDWATER DATA			DRILLING METHOD					
			Bp0.15m	KPa	%	Kgpm ³	DATE	DEPTH	HOUR	RIG TYPE	CME-850				
BR GRAVELLY SAND, A-1-B		181.68	6	460	20	-	DD	3-4	11.89	-	AUGER TYPE-DEPTH	0.08m HSA-23.2m			
GR SILTY CLAY LOAM, A-4		174.06	7	290	22	-	NC	3-4	11.89	0	CASING TYPE-DEPTH	-			
GR SAND, A-3		173.30	6	200	22	-	DD	3-4	11.89	-	AUGER TYPE-DEPTH	0.08m HSA-23.2m			
GR SILTY CLAY LOAM, A-4		172.53	12	285	23	-	NC	3-4	11.89	0	CASING TYPE-DEPTH	-			
BR STRATIFIED SILTY CLAY, A-6 SILT, A-4 AND SAND, A-3		171.93	5	200	21	-	DD	3-4	11.89	-	AUGER TYPE-DEPTH	0.08m HSA-23.2m			
BR SAND, A-3		177.87	3	21	-	-	NC	3-4	11.89	0	CASING TYPE-DEPTH	-			
GR SILTY LOAM, A-4		177.11	3	18	-	-	DD	3-4	11.89	-	AUGER TYPE-DEPTH	0.08m HSA-23.2m			
GR SAND, A-3		176.34	8	18	-	-	NC	3-4	11.89	0	CASING TYPE-DEPTH	-			
GR SILTY CLAY LOAM, A-4		175.58	13	490	16	-	DD	3-4	11.89	-	AUGER TYPE-DEPTH	0.08m HSA-23.2m			

LEGEND

A-1 to A-8 (and subgroups)	Engineering classifications of soil in accordance with AASHTO M 145 standard specification.	Q _v , kPa	Unconfined compression strength of soil in kilopascals determined in accordance with AASHTO T 208 standard specification.
Silty Clay Loam	Textural classification of soil in accordance with IDOT Triangular Chart.	w, %	Natural moisture content of soil and bedrock in percent determined in accordance with AASHTO T 265 standard specification and AASHTO T 265/ASTM D 2216 for bedrock.
Laminated Coal Shale	Textural and engineering classification of bedrock in accordance with conventional practice.	Y _d , kgpm ³	Dry unit weight of soil and bedrock in kilograms per cubic meter determined in accordance with standard practice.
N, Bp0.15m	N-value or standard penetration test value. Number of blows required to drive a standard split-spoon sampler 0.15 m as conducted in accordance with AASHTO T 206 standard specification.		

GROUNDWATER DATA	DRILLING METHOD
DD Water Level During Drilling	FA Flight Auger
BAR Water Level Before Auger Removal	RW Rotary Wash
AAR Water Level After Auger Removal	HSA Hollow Stem Auger
DC Dry Cave Level	SAMPLER TYPE
WC Wet Cave Level	AU Auger
d Days	SS Standard Split-barrel
h Hours	ST Thin-walled Tube
	DB Core Barrel

NOTES

- The abbreviations, symbols and definitions in this Legend are commonly used and understood in the engineering and construction practices and are presented only for information and communication.
- The Geotechnical Data presented in this Legend and on the Boring Logs are to be interpreted by personnel educated, trained, experienced and licensed to practice Geotechnical Engineering, and in direct communication with the Claude H. Hurley Company.

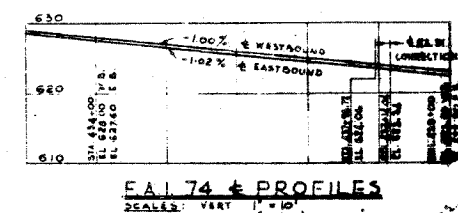
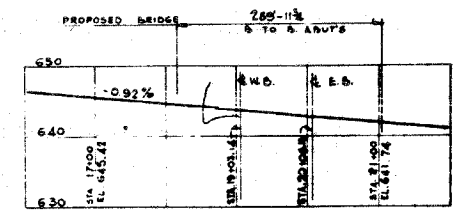
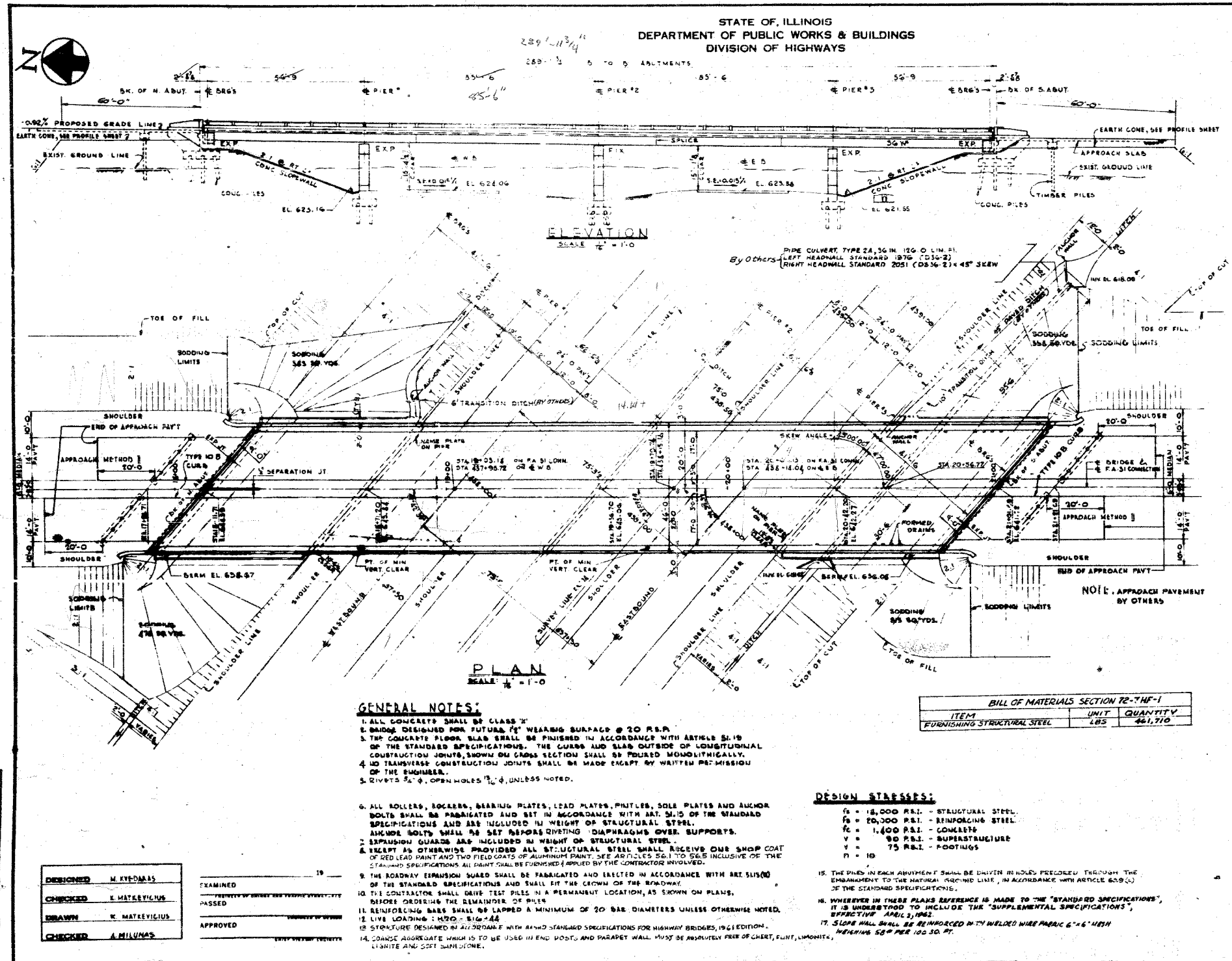
Notes:

For additional boring logs of Boring No. SB-296, see Sheet 54.

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION			
BORING LOGS IV			
RAMPS B-1 AND B-4 OVER F.A.I. ROUTE 74 (I-74) RAMPS A-3 AND B-3 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STATION 10+779.03 STRUCTURE NUMBER 072-0183			
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS			
DRAWING NO. 55	SCALE N.T.S.	DATE 2-18-03	SHEET NO. 55

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

PROJECT NO.	ENC.	COUNTY	SECTION	SHEET No.
FA174	7	PEORIA	26	13 SHEETS
FOR ROAD DIST. NO. 7				PROJECT 1-74-4(63)88
				1300 499



BILL OF MATERIALS SECTION 72-7HB-1

ITEM	UNIT	QUANTITY
EARTH EXCAVATION	CU. YDS.	26,791
CLASS X EXCAVATION FOR STRUCTURES	CU. YDS.	623.0
CLASS X CONCRETE	CU. YDS.	898.9
ERECTING STRUCTURAL STEEL	LBS.	46,710
ALUMINUM HANDRAIL	LIN. FT.	574
PIPE CULVERTS, TYPE 2A, 36"	LIN. FT.	126
STYPED/FLAT, PAPER	LBS.	156,770
FURNISHING GROSSED PILES UP TO 20'	LIN. FT.	400
FURNISHING GROSSED PILES 20.1' TO 36'	LIN. FT.	4100
TEST PILES (TIMBER)	EACH	8
DRIVING TIMBER PILES	LIN. FT.	4088
DRIVING CONCRETE PILES	LIN. FT.	1900
FURNISHING CONCRETE PILES	LIN. FT.	1664
TEST PILES (CONCRETE)	EACH	8
NAME PLATES	EACH	8
SLOPE WALL, 4 INCH	SQ. YDS.	588
Protective Coat	Sq. Yds.	1675
SODDING	SQ. YDS.	1888

GENERAL NOTES:

- ALL CONCRETE SHALL BE CLASS 'X'
- BRIDGE DESIGNED FOR FUTURE 12' WEARING SURFACE @ 20 R.S.P.
- THE CONCRETE FLOOR SLAB SHALL BE FINISHED IN ACCORDANCE WITH ARTICLE 51.10 OF THE STANDARD SPECIFICATIONS. THE CURBS AND SLAB OUTSIDE OF COMPLETIONAL CONSTRUCTION JOINTS, SHOWING ON CROSS SECTION SHALL BE FINISHED MONOLITHICALLY.
- NO TRANSVERSE CONSTRUCTION JOINTS SHALL BE MADE EXCEPT BY WRITTEN PERMISSION OF THE ENGINEER.
- RIVETS 3/4" Ø, OPEN HOLES 1/2" Ø, UNLESS NOTED.
- ALL ROLLERS, ROGERS, BEARING PLATES, LEAD PLATES, PINTLES, SOLE PLATES AND ANCHOR BOLTS SHALL BE FABRICATED AND SET IN ACCORDANCE WITH ART. 51.10 OF THE STANDARD SPECIFICATIONS AND ARE INCLUDED IN WEIGHT OF STRUCTURAL STEEL.
- ANCHOR BOLTS SHALL BE SET BEFORE DRIVING DIAPHRAGMS OVER SUPPORTS.
- EXPANSION GUARDS ARE INCLUDED IN WEIGHT OF STRUCTURAL STEEL.
- EXCEPT AS OTHERWISE PROVIDED ALL STRUCTURAL STEEL SHALL RECEIVE ONE SHOP COAT OF RED LEAD PAINT AND TWO FIELD COATS OF ALUMINUM PAINT. SEE ARTICLES 56.1 TO 56.5 INCLUSIVE OF THE STANDARD SPECIFICATIONS. ALL PAINT SHALL BE FURNISHED & APPLIED BY THE CONTRACTOR INVOLVED.
- THE ROADWAY EXPANSION SHOULDER SHALL BE FABRICATED AND BUILT IN ACCORDANCE WITH ART. 51.10 OF THE STANDARD SPECIFICATIONS AND SHALL FIT THE CROWN OF THE ROADWAY.
- THE CONTRACTOR SHALL DRIVE TEST PILES IN A PERMANENT LOCATION, AS SHOWN ON PLANS, BEFORE ORDERING THE REMAINDER OF PILES.
- REINFORCING BARS SHALL BE LAPPED A MINIMUM OF 20 BAR DIAMETERS UNLESS OTHERWISE NOTED.
- LIVE LOADING: 1400 - 810 - 44
- STRUCTURE DESIGNED BY CONTRACTOR WITH AHEAD STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, 1961 EDITION.
- CONCRETE AGGREGATE WHICH IS TO BE USED IN END POSTS AND PARAPET WALL MUST BE ABSOLUTELY FREE OF CLAY, LUMP, MAGNITE, LISITE AND SFT SANDSTONE.

BILL OF MATERIALS SECTION 72-7HB-1

ITEM	UNIT	QUANTITY
FURNISHING STRUCTURAL STEEL	LBS	46,710

DESIGN STRESSES:

- $f_s = 18,000$ R.S.I. - STRUCTURAL STEEL
- $f_r = 20,000$ R.S.I. - REINFORCING STEEL
- $f_c = 1,400$ R.S.I. - CONCRETE
- $v = 90$ R.S.I. - SUPERSTRUCTURE
- $n = 75$ R.S.I. - FOOTINGS
- $n = 10$
- THE PILES IN EACH ABUTMENT SHALL BE DRIVEN IN HOLES PRECEDED THROUGH THE EMBANKMENT TO THE NATURAL GROUND LINE, IN ACCORDANCE WITH ARTICLE 609 (C) OF THE STANDARD SPECIFICATIONS.
- WHENEVER IN THESE PLANS REFERENCE IS MADE TO THE "STANDARD SPECIFICATIONS", IT IS UNDERSTOOD TO INCLUDE THE "SUPPLEMENTAL SPECIFICATIONS", EFFECTIVE APRIL 2, 1962.
- SLOPE SHALL BE REINFORCED WITH WELDED WIRE FABRIC 6"x6" MESH WEIGHING 50# PER 100 SQ. FT.

DESIGNED	M. KYEMAS	19
CHECKED	K. MATKIVILUIS	PASSED
DRAWN	K. MATKIVILUIS	APPROVED
CHECKED	A. MILLUMS	

JOHN F. MEISSNER ENGINEERS
CHICAGO, ILLINOIS
ENG. NO. 56-107-1-C

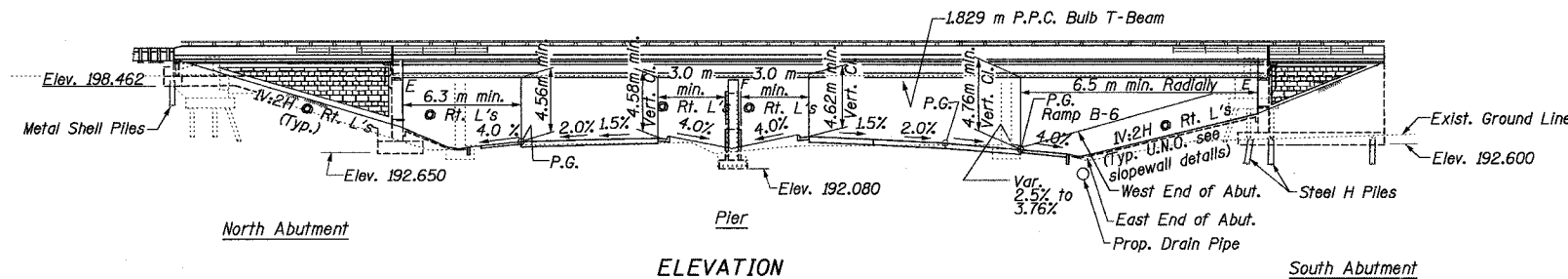
GENERAL PLAN & ELEVATION
F.A. ROUTE 74 SECTION 72-7-HB-1
PROJECT 1-74-4 (63)88
F.A. ROUTE 31 CONNECTION - GRADE SEPARATION
PEORIA COUNTY - STATION 438 + 15.10

ROUTE NO.	SECT.	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.I. 74	(72-7) R-3	PEORIA	500	1360
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

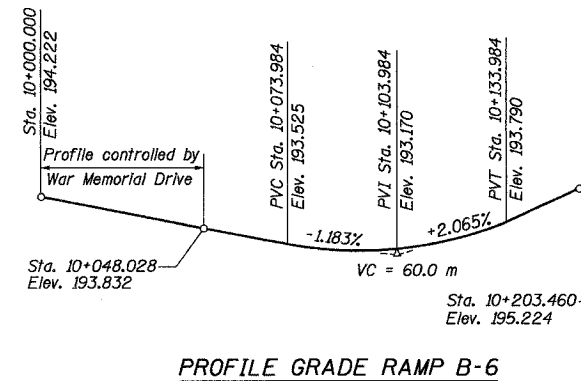
CONTRACT NO. 68200

B.M. : ' ' cut on North headwall N.W. of existing bridge at ramp for W.B. I-74 to W.B. War Memorial Dr. Elev. 194.167
 P.K. Nail on S. Side of F.A. 49 (U.S. 150 Spur) Bridge over I-74 Ramp Elevation 194.293 m.
 Existing Structure : S.N. 072-0026 is a four span bridge built as F.A.I. Rt. 31 Spur over War Memorial Drive (U.S. Rt. 150), Project U-107(12), Section (117-3HB) in 1963.
 The Superstructure consists of a R.C. Deck 77.91 m long by 14.02 m wide supported on wide flange steel stringers.
 The Existing Superstructure, North and South Abutments and Piers shall be removed as required. Traffic along ramps B-3 and B-5 will be detoured while traffic along War Memorial Drive will be maintained during construction.

Note: All dimensions are in millimeters (mm) except as noted.



ELEVATION



PROFILE GRADE RAMP B-6

DESIGN SPECIFICATIONS

1996 AASHTO with Interims through 2000.

LOADING MS18 & ALT.

Allow 2.4 kN/m² for future wearing surface.

DESIGN STRESSES

FIELD UNITS

f' c = 24 MPa
 f' y = 400 MPa (Reinf.)

PRECAST PRESTRESSED UNITS

f' c = 42 MPa
 f' c1 = 35 MPa
 f' s = 1,860 MPa (12.7 mm ϕ low lax. strands)
 f' s1 = 1,395 MPa (12.7 mm ϕ low lax. strands)

SEISMIC DATA

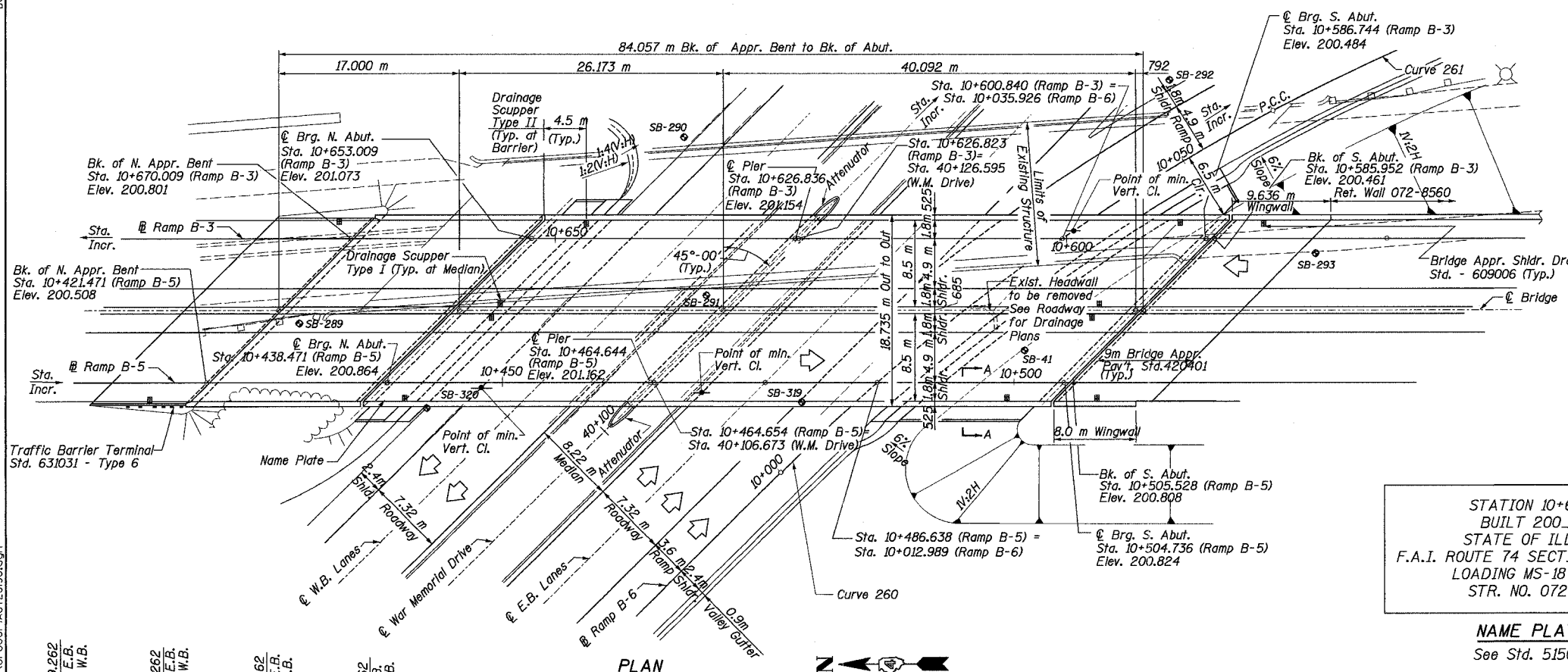
Seismic Performance Category (SPC) = A
 Bedrock Acceleration Coefficient (A) = 0.043g
 Site Coefficient (S) = 1.0

CURVE DATA

War Memorial Dr. Curve 200
$\Delta = 20^{\circ} 20' 45''$
R = 5,374.349 m
T = 964.381 m
L = 1,908.452 m
E = 85.840 m
PC = 38+811.662
PI = 39+776.043
PT = 40+720.114
SE = Normal Crown

Ramp B-6 Curve 260
$\Delta = 17^{\circ} 11' 19''$
R = 200.00 m
T = 30.227 m
L = 60.00 m
E = 2.271 m
PC = 10+000.000
PI = 10+030.227
PCC = 10+060.000
SE = 2.5 %
Transition in : N/A
Transition out : 10+027 to 10+060

Ramp B-6 Curve 261
$\Delta = 18^{\circ} 25' 26''$
R = 125.00 m
T = 20.272 m
L = 40.195 m
E = 1.633 m
PCC = 10+060.000
PI = 10+080.272
PCC = 10+100.195
SE = 8.0 %
Transition in : 10+060 to 10+076
Transition out : N/A

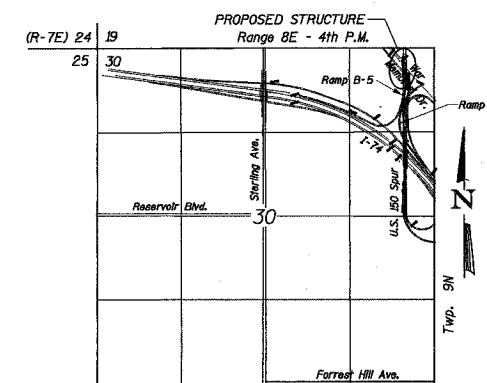


PLAN

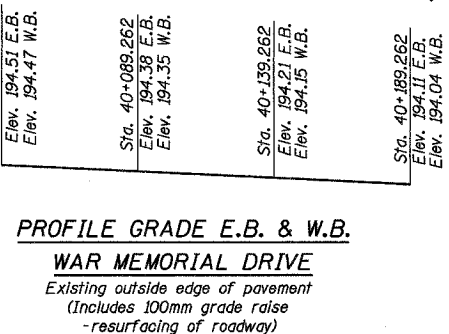
STATION 10+618.61
 BUILT 200_ BY
 STATE OF ILLINOIS
 F.A.I. ROUTE 74 SECTION (72-7) R-3
 LOADING MS-18 & ALT.
 STR. NO. 072-0190

NAME PLATE

See Std. 515001



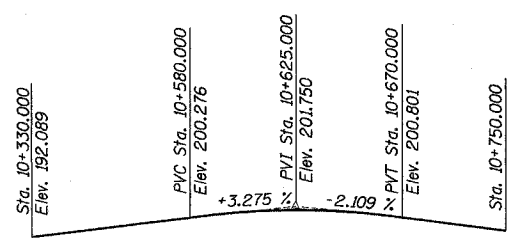
LOCATION SKETCH



PROFILE GRADE E.B. & W.B.

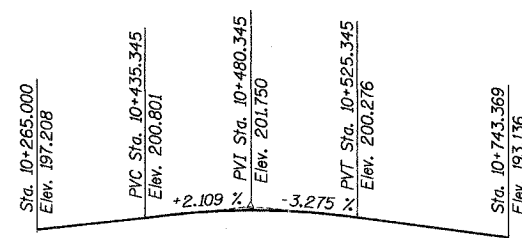
WAR MEMORIAL DRIVE

Existing outside edge of pavement
 (Includes 100mm grade raise
 -resurfacing of roadway)



PROFILE GRADE RAMP B-3

(Along \O Ramp B-3)



PROFILE GRADE RAMP B-5

(Along \O Ramp B-5)



REVISION	DATE	DESCRIPTION
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		
GENERAL PLAN		
RAMPS B-3 AND B-5 OVER WAR MEMORIAL DR. AND RAMP B-6 F.A.I. ROUTE 74 SECTION (72-7) R-3 PEORIA COUNTY STA. 10+618.61 (RAMP B-3) STA. 10+472.60 (RAMP B-5) STRUCTURE NUMBER 072-0190		
PARSONS TRANSPORTATION GROUP CHICAGO, ILLINOIS		
DRAWING NO.	SCALE	DATE
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		SHEET NO.
		1

Time: 10:23:32 AM
 Date: 11/22/2004

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Designed by: MBG
 Checked by: AK
 Drafted by: JV
 Checked by: AK