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|-----------------------|----------|--------------|--------------|-----------|
| F.A.P. R.T.E. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 365 | 60 RS-3 | KANE | 23 | 1 |
| FED. ROAD DIST. NO. 1 | ILLINOIS | CONTRACT NO. | 60K81 | |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS**

FOR INDEX OF SHEETS, SEE SHEET NO. 2

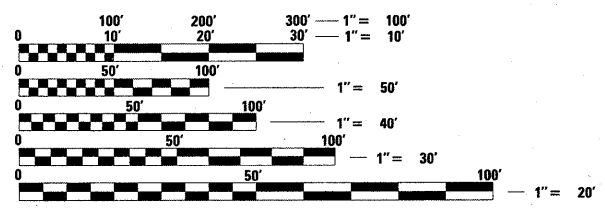
**PROPOSED
HIGHWAY PLANS**

**F.A.P. 365: IL ROUTE 56 (BUTTERFIELD ROAD)
LOCATION 1: IL ROUTE 25 TO MITCHELL ROAD
LOCATION 2: FARNSWORTH AVE. TO DuPAGE COUNTY LINE
SECTION: 60 RS-3
RESURFACING (3P)
PROJECT: *NHF-0365(013)*
KANE COUNTY
C-91-606-10**

**PROJECT LOCATED IN THE
CITY OF AURORA AND VILLAGE OF NORTH AURORA**

TRAFFIC DATA:

2009 ADT = 12,500 - 18,100
SPEED LIMIT = 35-50 MPH

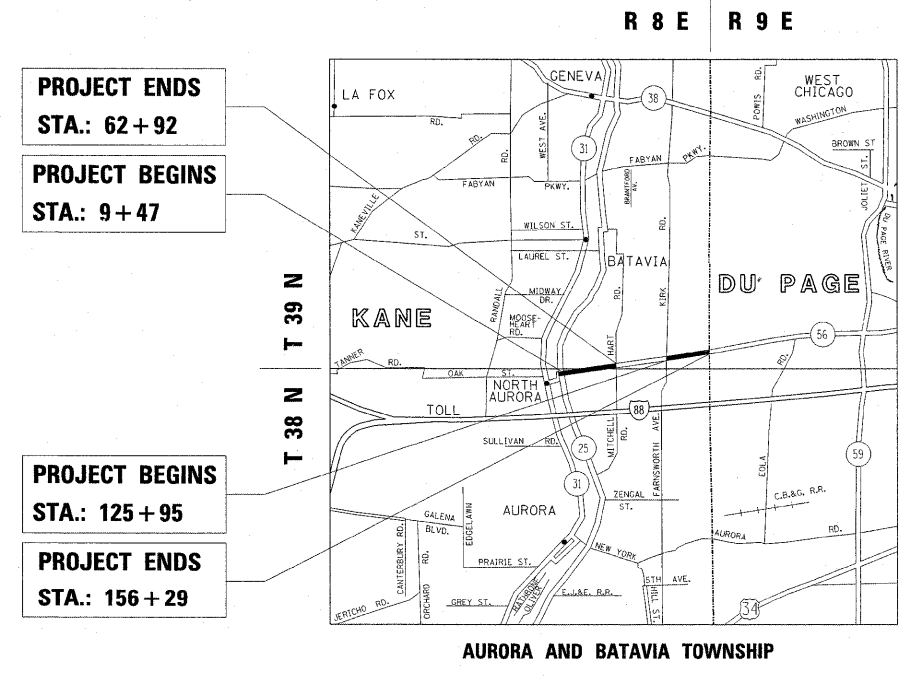
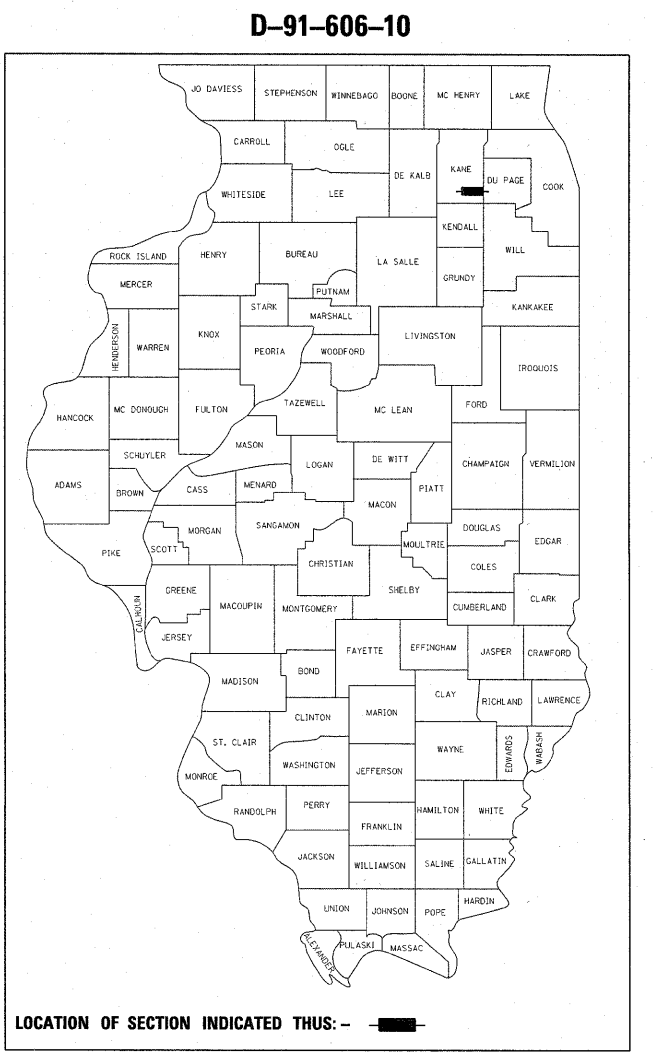


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: JENPAI CHANG (847) 705-4432
PROJECT MANAGER: KEN ENG (847) 705-4247

CONTRACT NO. 60K81



PROJECT ENDS
STA.: 62 + 92

PROJECT BEGINS
STA.: 9 + 47

PROJECT BEGINS
STA.: 125 + 95

PROJECT ENDS
STA.: 156 + 29

LOCATION 1:
GROSS AND NET LENGTH OF PROJECT = 5345 FEET = 1.01 MILES

LOCATION 2:
GROSS AND NET LENGTH OF PROJECT = 3034 FEET = 0.58 MILES

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED JANUARY 24, 20 11

Diane M. O'Keefe DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
March 25 20 11

Scott E. Stitt P.E. acting ENGINEER OF DESIGN AND ENVIRONMENT
March 25 20 11

Christine M. Road DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

INDEX OF SHEETS:

| <u>SHEET NO.</u> | <u>DESCRIPTION</u> |
|------------------|---|
| 1 | COVER SHEET |
| 2 | INDEX OF SHEETS, GENERAL NOTES, AND STATE STANDARDS |
| 3 | SUMMARY OF QUANTITIES |
| 4-5 | EXISTING AND PROPOSED TYPICAL SECTIONS |
| 6-10 | ROADWAY AND PAVEMENT MARKING PLAN |
| 11 | DETECTOR LOOP REPLACEMENT PLAN |
| 12 | DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-08) |
| 13 | PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22) |
| 14 | BUTT JOINT AND HMA TAPER DETAILS (BD-32) |
| 15 | TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10) |
| 16 | TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11) |
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| 18 | TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14) |
| 19 | PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING (TC-16) |
| 20 | ARTERIAL ROAD INFORMATION SIGN (TC-22) |
| 21 | DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS - SHEET 1 (TS-05) |
| 22 | DISTRICT 1 - DETECTOR LOOP INSTALLATION - DETAILS FOR ROADWAY RESURFACING (TS-07) |
| 23 | RUMBLE STRIPES FOR CENTERLINE, NON-FREEWAY |

STATE STANDARDS:

| | |
|----------------------|---|
| 000001-06 | STANDARD SYMBOLS, ABBREVIATION AND PATTERNS |
| 442201-03 | CLASS C AND D PATCHES |
| 604001-03 | FRAME AND LIDS, TYPE 1 |
| 701301-04 | LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS |
| 701306-03 | LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS \geq 45 MPH |
| 701311-03 | LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY |
| 701336-06 | LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES, FOR SPEEDS \geq 45 MPH |
| 701426-04 | LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS \geq 45 MPH |
| 780001-02 | TYPICAL PAVEMENT MARKINGS |
| 701011-02 | |
| 701701-07 | |
| 701901-01 | |

GENERAL NOTES:

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION IS REQUIRED)

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, THE CITY OF AURORA, AND THE VILLAGE OF NORTH AURORA.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470, A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT DON CHIARUGI, AREA TRAFFIC FIELD ENGINEER, AT (847) 741-9857 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM THE BUREAU OF CONSTRUCTION.

THE THICKNESS OF THE HMA MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA MIXTURE IS PLACED

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.

WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 mm) WHERE THE SPEED LIMIT IS 40 MPH (80 km/h) OR LESS AND 1 INCH (25 mm) WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH (80 km/h). WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

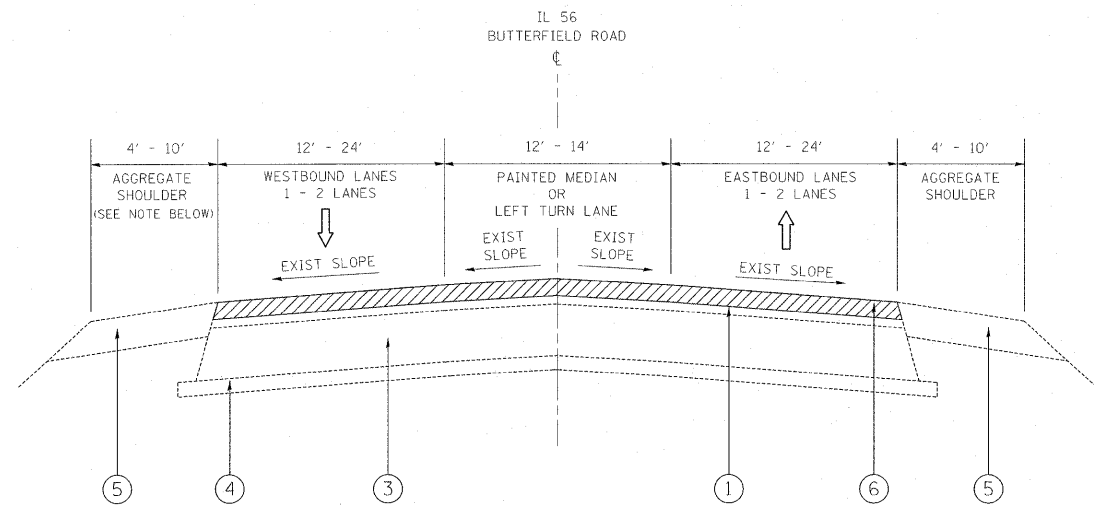
BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

| | | | | | | | | | | |
|---|-----------------------------|------------|-----------|---|--|--------------------|----------------------------------|---|--------------|-----------|
| FILE NAME = | USER NAME = becker.tom | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | IL RTE. 56 (BUTTERFIELD ROAD) INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| at:\pw_work\pwidot\becker.tom\d0244424\0160610-sht-plen.dgn | PLOT SCALE = 50.0000' / IN. | DRAWN - | REVISED - | | | 365 | 60 RS-3 | KANE | 23 | 2 |
| | PLOT DATE = 2/7/2011 | CHECKED - | REVISED - | | | CONTRACT NO. 60K81 | | | | |
| | | DATE - | REVISED - | | | SCALE: | SHEET NO. OF SHEETS STA. TO STA. | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | |

Rev

| SUMMARY OF QUANTITIES | | | URBAN 80% FED. 20% STATE | | | | | CONSTRUCTION TYPE CODE | | | SUMMARY OF QUANTITIES | | | URBAN 80% FED. 20% STATE | | | | | CONSTRUCTION TYPE CODE | | |
|-----------------------|---|--------|--------------------------------|---|--|--|--|------------------------|--|------------|--|-------|------------------|---|--|--|--|--|------------------------|--|--|
| CODE NO | ITEM | UNIT | TOTAL QUANTITIES | LOCATION 1 IL 25 TO MITCHELL RD. 0005 | LOCATION 2 FARNSWORTH AVE. TO DUPAGE CO. LINE 0005 | | | | | CODE NO | ITEM | UNIT | TOTAL QUANTITIES | LOCATION 1 IL 25 TO MITCHELL RD. 0005 | LOCATION 2 FARNSWORTH AVE. TO DUPAGE CO. LINE 0005 | | | | | | |
| 40600200 | BITUMINOUS MATERIALS (PRIME COAT) | TON | 36 | 24 | 12 | | | | | * 78000100 | THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS | SO FT | 878.8 | 696.8 | 182 | | | | | | |
| 40600300 | AGGREGATE (PRIME COAT) | TON | 172 | 120 | 52 | | | | | * 78000200 | THERMOPLASTIC PAVEMENT MARKING - LINE 4" | FOOT | 38100 | 24800 | 13300 | | | | | | |
| 40600400 | MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS | TON | 65 | 45 | 20 | | | | | * 78000400 | THERMOPLASTIC PAVEMENT MARKING - LINE 6" | FOOT | 3090 | 2300 | 790 | | | | | | |
| 40600895 | CONSTRUCTING TEST STRIP | EACH | 2 | 1 | 1 | | | | | * 78000500 | THERMOPLASTIC PAVEMENT MARKING - LINE 8" | FOOT | 160 | 160 | | | | | | | |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SO YD | 380 | 270 | 110 | | | | | * 78000600 | THERMOPLASTIC PAVEMENT MARKING - LINE 12" | FOOT | 1825 | 1350 | 475 | | | | | | |
| 40601005 | HOT-MIX ASPHALT REPLACEMENT OVER PATCHES | TON | 475 | 185 | 290 | | | | | * 78000650 | THERMOPLASTIC PAVEMENT MARKING - LINE 24" | FOOT | 380 | 300 | 80 | | | | | | |
| 40603340 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 | TON | 3615 | 2525 | 1090 | | | | | * 78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 510 | 345 | 165 | | | | | | |
| 44000158 | HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4" | SO YD | 42700 | 29800 | 12900 | | | | | 78300200 | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | EACH | 410 | 275 | 135 | | | | | | |
| 44002232 | HOT-MIX ASPHALT REMOVAL OVER PATCHES, 8" | SO YD | 1050 | 410 | 640 | | | | | * 88600600 | DETECTOR LOOP REPLACEMENT | FOOT | 925 | 925 | | | | | | | |
| 44201753 | CLASS D PATCHES, TYPE II, 9 INCH | SO YD | 648 | 240 | 408 | | | | | X0325866 | CENTER LINE - RUMBLE STRIP | FOOT | 905 | | 905 | | | | | | |
| 44201757 | CLASS D PATCHES, TYPE III, 9 INCH | SO YD | 106 | 53 | 53 | | | | | X2020110 | GRADING AND SHAPING SHOULDERS | UNIT | 142 | 86 | 56 | | | | | | |
| 44201759 | CLASS D PATCHES, TYPE IV, 9 INCH | SO YD | 138 | 55 | 83 | | | | | X4060826 | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 | TON | 1820 | 1275 | 545 | | | | | | |
| 48102100 | AGGREGATE WEDGE SHOULDER, TYPE B | TON | 575 | 350 | 225 | | | | | X6030310 | FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) | EACH | 1 | 1 | | | | | | | |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 6 | 3 | 3 | | | | | △ Z0018500 | DRAINAGE STRUCTURES TO BE CLEANED | EACH | 4 | 2 | 2 | | | | | | |
| 67100100 | MOBILIZATION | L SUM | 1 | 0.5 | 0.5 | | | | | Z0030850 | TEMPORARY INFORMATION SIGNING | SO FT | 102.8 | 51.4 | 51.4 | | | | | | |
| 70100460 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701306 | L SUM | 1 | 0.5 | 0.5 | | | | | | | | | | | | | | | | |
| 70100600 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701336 | L SUM | 1 | 0.5 | 0.5 | | | | | | | | | | | | | | | | |
| 70300100 | SHORT TERM PAVEMENT MARKING | FOOT | 4965 | 3590 | 1375 | | | | | | | | | | | | | | | | |
| 70300210 | TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS | SO FT | 878.8 | 696.8 | 182 | | | | | | | | | | | | | | | | |
| 70300220 | TEMPORARY PAVEMENT MARKING - LINE 4" | FOOT | 38100 | 24800 | 13300 | | | | | | | | | | | | | | | | |
| 70300240 | TEMPORARY PAVEMENT MARKING - LINE 6" | FOOT | 3090 | 2300 | 790 | | | | | | | | | | | | | | | | |
| 70300250 | TEMPORARY PAVEMENT MARKING - LINE 8" | FOOT | 160 | 160 | | | | | | | | | | | | | | | | | |
| 70300260 | TEMPORARY PAVEMENT MARKING - LINE 12" | FOOT | 1825 | 1350 | 475 | | | | | | | | | | | | | | | | |
| 70300280 | TEMPORARY PAVEMENT MARKING - LINE 24" | FOOT | 380 | 300 | 80 | | | | | | | | | | | | | | | | |
| 70301000 | WORK ZONE PAVEMENT MARKING REMOVAL | SO FT | 1660 | 1200 | 460 | | | | | | | | | | | | | | | | |
| 70102635 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701701 | L SUM | 1 | 0.5 | 0.5 | | | | | | | | | | | | | | | | |

△ = Non-participating

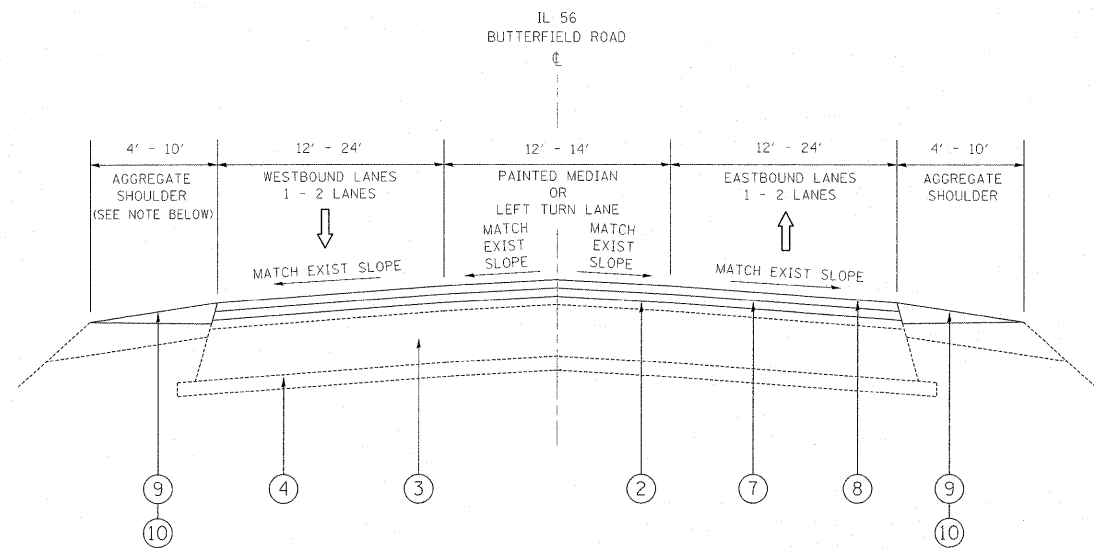


EXISTING TYPICAL SECTION
 IL RTE. 56 (BUTTERFIELD ROAD)
 STA. 9+47 TO STA. 62+92
 STA. 125+95 TO STA. 147+26

NOTE:
 CURB AND GUTTER ON WESTBOUND SIDE FROM:
 STA. 9+47 TO STA. 12+15

LEGEND:

- ① EXISTING HMA SURFACE (VARIES FROM 3" TO 13")
- ② EXISTING HMA SURFACE AFTER MILLING (VARIES FROM 3/4" TO 10-3/4")
- ③ EXISTING P.C.C. BASE COURSE (VARIES FROM 8" TO 10")
- ④ EXISTING SUB-BASE GRANULAR MATERIAL
- ⑤ EXISTING AGGREGATE SHOULDER, 6"
- ⑥ PROPOSED HMA SURFACE REMOVAL, 2-1/4"
- ⑦ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑧ PROPOSED HMA SURFACE COURSE, MIX "D", N70, 1-1/2"
- ⑨ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑩ PROPOSED GRADING AND SHAPING OF SHOULDERS



PROPOSED TYPICAL SECTION
 IL RTE. 56 (BUTTERFIELD ROAD)
 STA. 9+47 TO STA. 62+92
 STA. 125+95 TO STA. 147+26

NOTE:
 CURB AND GUTTER ON WESTBOUND SIDE FROM:
 STA. 9+47 TO STA. 12+15

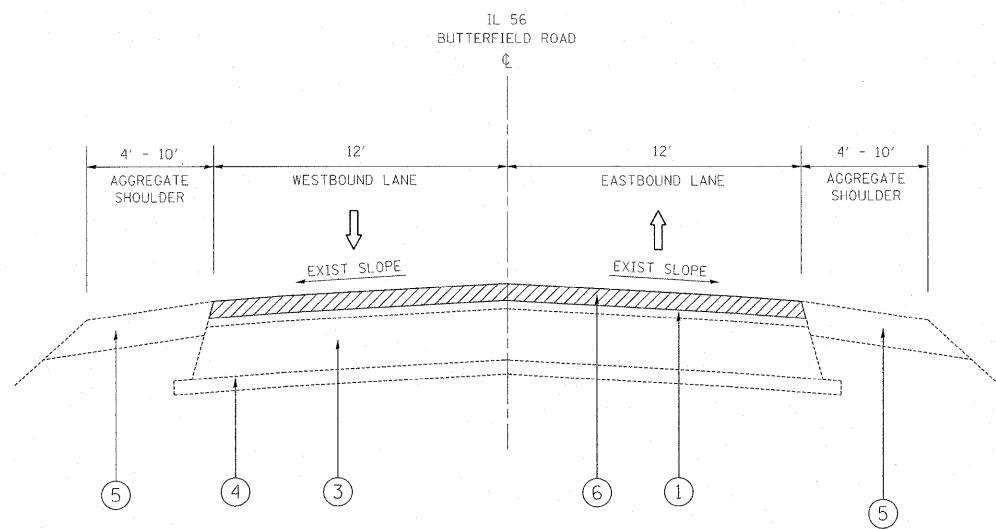
| HOT-MIX ASPHALT MIXTURE REQUIREMENTS | | |
|--------------------------------------|--|------------------|
| OPERATION | MIXTURE TYPE | AIR VOIDS @ Ndes |
| PAVEMENT RESURFACING | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5 MM) | 4% @ 70 GYR. |
| PAVEMENT RESURFACING | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 | 4% @ 50 GYR. |
| PATCHING | CLASS D PATCHES (HMA BINDER IL-19 MM) | 4% @ 70 GYR. |
| PATCHING | HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 MM) | 4% @ 70 GYR. |

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING

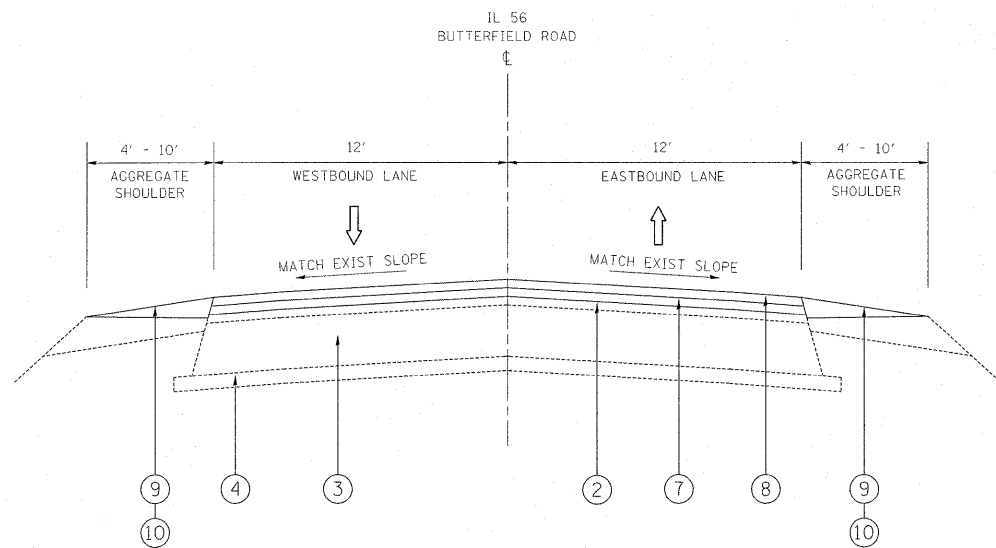
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| FILE NAME = | USER NAME = becker.tom | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | IL RTE. 56 (BUTTERFIELD RD.) EXISTING AND PROPOSED TYPICAL SECTIONS | | F.A.P. RTE. 365 | SECTION 60 RS-3 | COUNTY KANE | TOTAL SHEETS 23 | SHEET NO. 4 | |
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| PLOT SCALE = 50.0000' / IN. | | CHECKED - | REVISED - | | CONTRACT NO. 60K81 | | | | | | | |
| PLOT DATE = 2/7/2011 | | DATE - | REVISED - | | | | | | | | | |



EXISTING TYPICAL SECTION
IL RTE. 56 (BUTTERFIELD ROAD)
STA. 147+26 TO STA. 156+29

LEGEND:

- ① EXISTING HMA SURFACE (VARIES FROM 3" TO 13")
- ② EXISTING HMA SURFACE AFTER MILLING (VARIES FROM 3/4" TO 10-3/4")
- ③ EXISTING P.C.C. BASE COURSE (VARIES FROM 8" TO 10")
- ④ EXISTING SUB-BASE GRANULAR MATERIAL
- ⑤ EXISTING AGGREGATE SHOULDER, 6"
- ⑥ PROPOSED HMA SURFACE REMOVAL, 2-1/4"
- ⑦ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑧ PROPOSED HMA SURFACE COURSE, MIX "D", N70, 1-1/2"
- ⑨ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑩ PROPOSED GRADING AND SHAPING OF SHOULDERS



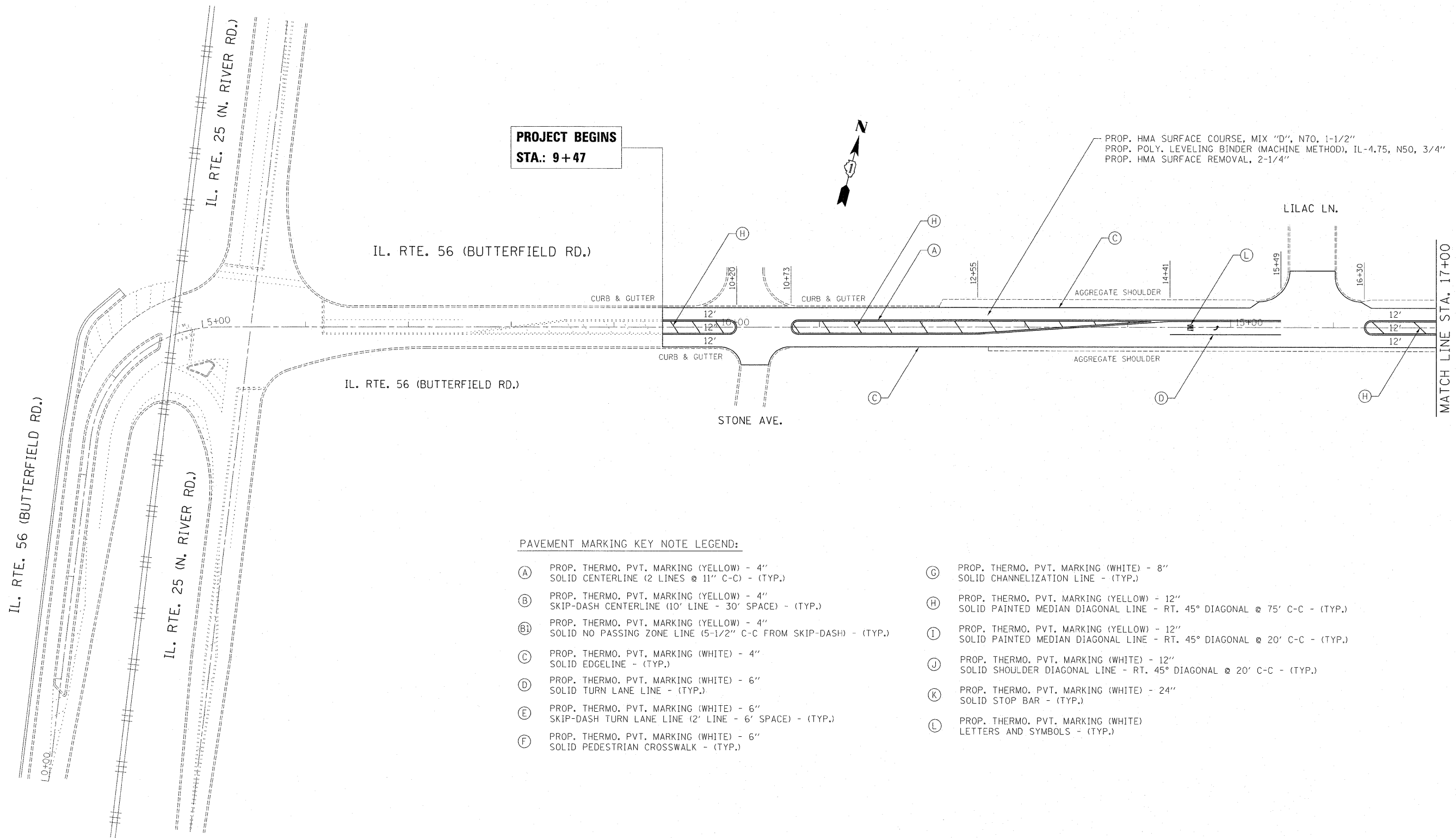
PROPOSED TYPICAL SECTION
IL RTE. 56 (BUTTERFIELD ROAD)
STA. 147+26 TO STA. 156+29

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| PLOT SCALE = 50.0000' / IN. | | CHECKED - | REVISED - |
| PLOT DATE = 2/7/2011 | | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | | | |
|--|-----------|-----------|--------------|
| IL RTE. 56 (BUTTERFIELD RD.) EXISTING AND PROPOSED TYPICAL SECTIONS | | | |
| SCALE: | SHEET NO. | OF SHEETS | STA. TO STA. |

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|--------|--------------|-----------|
| 365 | 60 RS-3 | KANE | 23 | 5 |
| CONTRACT NO. 60K81 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



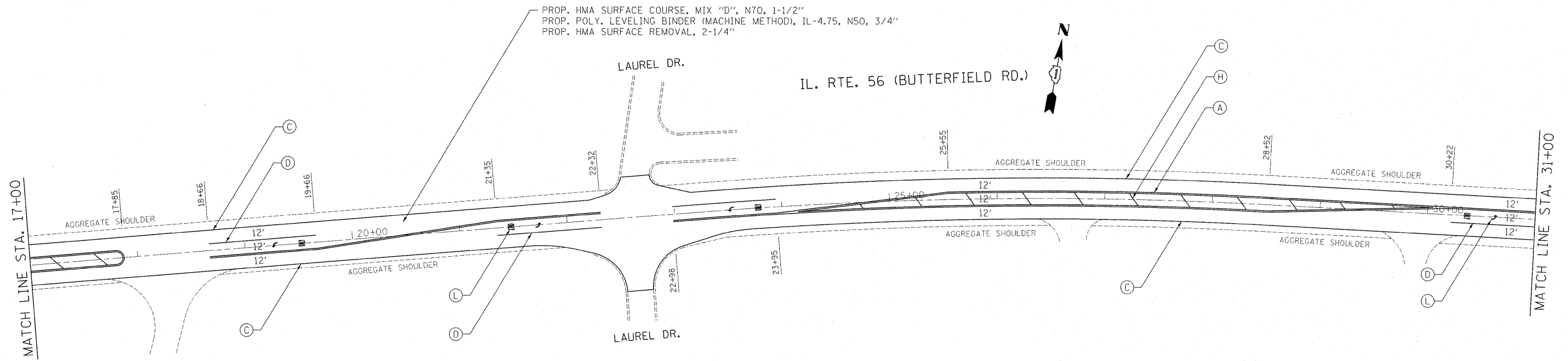
**PROJECT BEGINS
STA.: 9+47**

PAVEMENT MARKING KEY NOTE LEGEND:

- (A) PROP. THERMO. PVT. MARKING (YELLOW) - 4" SOLID CENTERLINE (2 LINES @ 11" C-C) - (TYP.)
- (B) PROP. THERMO. PVT. MARKING (YELLOW) - 4" SKIP-DASH CENTERLINE (10' LINE - 30' SPACE) - (TYP.)
- (B1) PROP. THERMO. PVT. MARKING (YELLOW) - 4" SOLID NO PASSING ZONE LINE (5-1/2" C-C FROM SKIP-DASH) - (TYP.)
- (C) PROP. THERMO. PVT. MARKING (WHITE) - 4" SOLID EDGELINE - (TYP.)
- (D) PROP. THERMO. PVT. MARKING (WHITE) - 6" SOLID TURN LANE LINE - (TYP.)
- (E) PROP. THERMO. PVT. MARKING (WHITE) - 6" SKIP-DASH TURN LANE LINE (2' LINE - 6' SPACE) - (TYP.)
- (F) PROP. THERMO. PVT. MARKING (WHITE) - 6" SOLID PEDESTRIAN CROSSWALK - (TYP.)
- (G) PROP. THERMO. PVT. MARKING (WHITE) - 8" SOLID CHANNELIZATION LINE - (TYP.)
- (H) PROP. THERMO. PVT. MARKING (YELLOW) - 12" SOLID PAINTED MEDIAN DIAGONAL LINE - RT. 45° DIAGONAL @ 75' C-C - (TYP.)
- (I) PROP. THERMO. PVT. MARKING (YELLOW) - 12" SOLID PAINTED MEDIAN DIAGONAL LINE - RT. 45° DIAGONAL @ 20' C-C - (TYP.)
- (J) PROP. THERMO. PVT. MARKING (WHITE) - 12" SOLID SHOULDER DIAGONAL LINE - RT. 45° DIAGONAL @ 20' C-C - (TYP.)
- (K) PROP. THERMO. PVT. MARKING (WHITE) - 24" SOLID STOP BAR - (TYP.)
- (L) PROP. THERMO. PVT. MARKING (WHITE) LETTERS AND SYMBOLS - (TYP.)

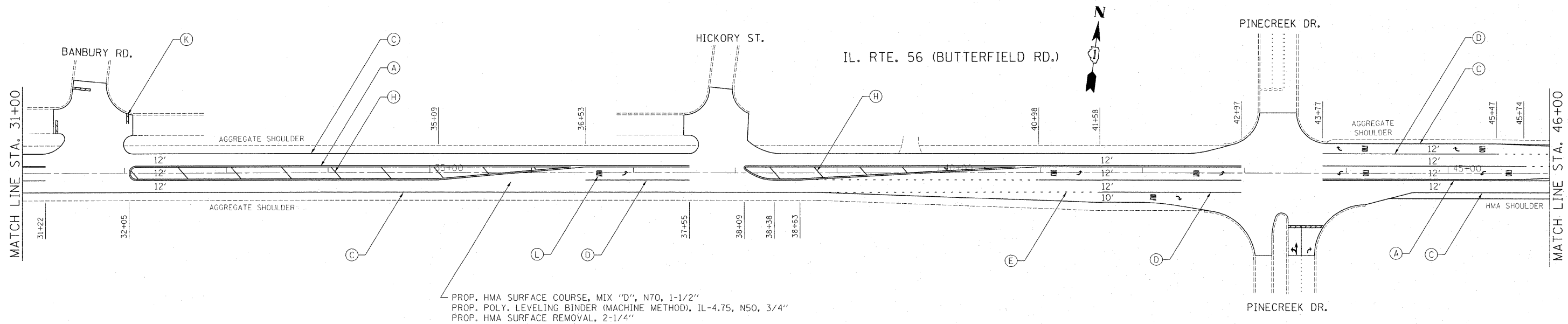
Rev: 3/16/2011

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| FILE NAME = | USER NAME = becker.tom | DESIGNED - | REVISED 3/16/2011 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | IL RTE. 56 (BUTTERFIELD RD.) LOCATION 1 (IL RTE. 25 TO MITCHELL RD.) ROADWAY AND PAVEMENT MARKING PLAN | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 0:\pw\work\p\dot\becker.tom\d0244424\0610-sht-plan.dgn | 0610-sht-plan.dgn | DRAWN - | REVISED - | | | 365 | 60 RS-3 | KANE | 23 | 6 |
| PLOT SCALE = 50,0000 ' / IN. | | CHECKED - | REVISED - | | | CONTRACT NO. 60K81 | | | | |
| PLOT DATE = 3/14/2011 | | DATE - | REVISED - | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



PAVEMENT MARKING KEY NOTE LEGEND:

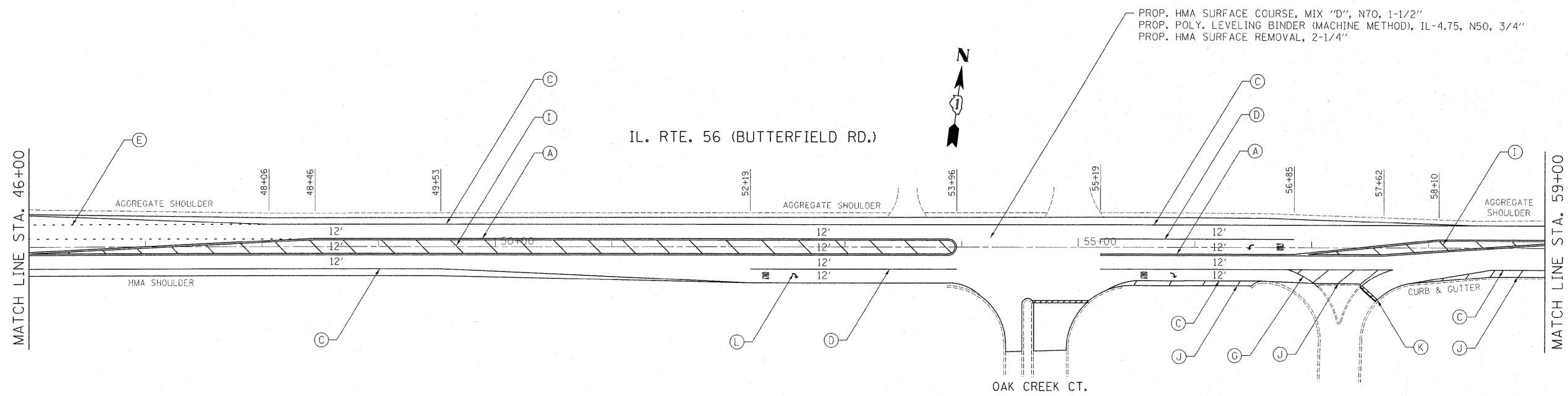
- (A) PROP. THERMO. PVT. MARKING (YELLOW) - 4" SOLID CENTERLINE (2 LINES @ 11" C-C) - (TYP.)
- (B) PROP. THERMO. PVT. MARKING (YELLOW) - 4" SKIP-DASH CENTERLINE (10' LINE - 30' SPACE) - (TYP.)
- (BI) PROP. THERMO. PVT. MARKING (YELLOW) - 4" SOLID NO PASSING ZONE LINE (5-1/2" C-C FROM SKIP-DASH) - (TYP.)
- (C) PROP. THERMO. PVT. MARKING (WHITE) - 4" SOLID EDGELINE - (TYP.)
- (D) PROP. THERMO. PVT. MARKING (WHITE) - 6" SOLID TURN LANE LINE - (TYP.)
- (E) PROP. THERMO. PVT. MARKING (WHITE) - 6" SKIP-DASH TURN LANE LINE (2' LINE - 6' SPACE) - (TYP.)
- (F) PROP. THERMO. PVT. MARKING (WHITE) - 6" SOLID PEDESTRIAN CROSSWALK - (TYP.)
- (G) PROP. THERMO. PVT. MARKING (WHITE) - 8" SOLID CHANNELIZATION LINE - (TYP.)
- (H) PROP. THERMO. PVT. MARKING (YELLOW) - 12" SOLID PAINTED MEDIAN DIAGONAL LINE - RT. 45° DIAGONAL @ 75' C-C - (TYP.)
- (I) PROP. THERMO. PVT. MARKING (YELLOW) - 12" SOLID PAINTED MEDIAN DIAGONAL LINE - RT. 45° DIAGONAL @ 20' C-C - (TYP.)
- (J) PROP. THERMO. PVT. MARKING (WHITE) - 12" SOLID SHOULDER DIAGONAL LINE - RT. 45° DIAGONAL @ 20' C-C - (TYP.)
- (K) PROP. THERMO. PVT. MARKING (WHITE) - 24" SOLID STOP BAR - (TYP.)
- (L) PROP. THERMO. PVT. MARKING (WHITE) LETTERS AND SYMBOLS - (TYP.)



PROP. HMA SURFACE COURSE, MIX "D", N70, 1-1/2"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
 PROP. HMA SURFACE REMOVAL, 2-1/4"

Rev: 3/16/2011

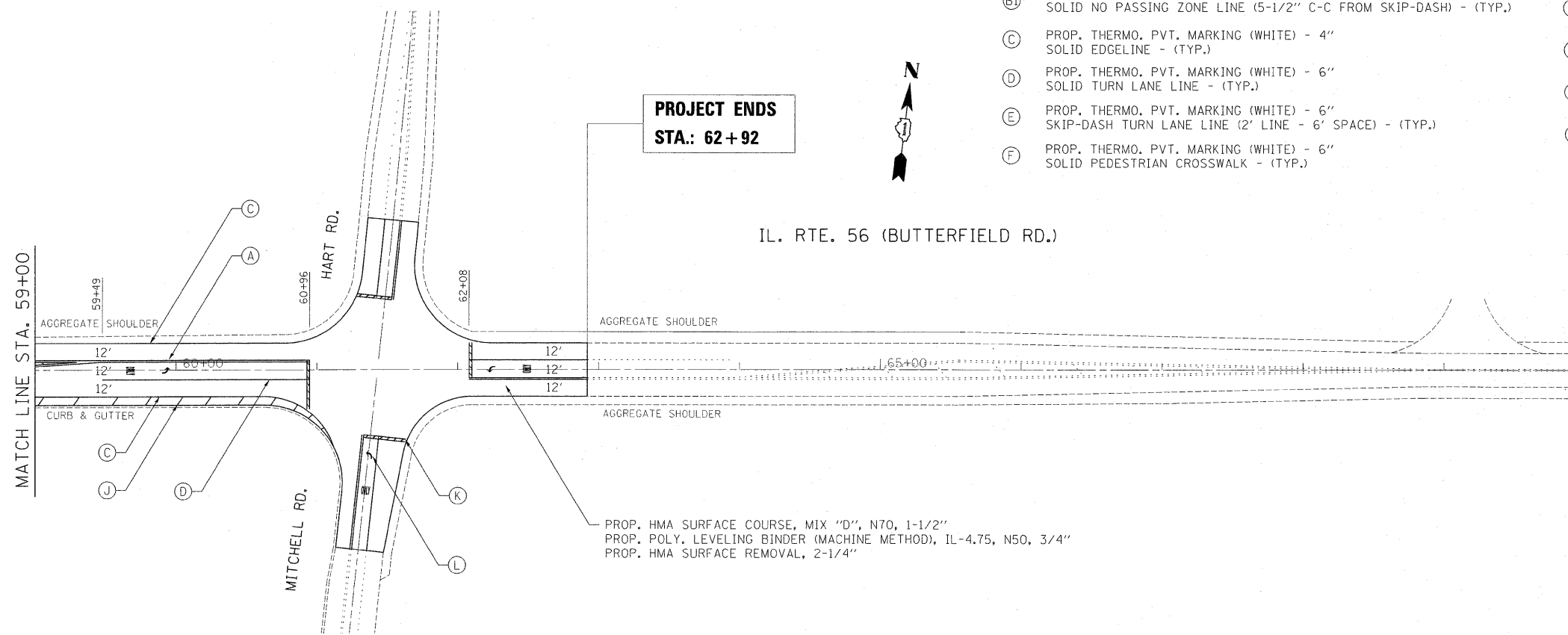
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|--|------------------------------|------------|-------------------|---|---|-----------------|---------------------|--------------|--------------------------------|------------------|--------------------|
| FILE NAME = | USER NAME = beekertem | DESIGNED - | REVISED 3/16/2011 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | IL RTE. 56 (BUTTERFIELD RD.) LOCATION 1 (IL RTE. 25 TO MITCHELL RD.) ROADWAY AND PAVEMENT MARKING PLAN | F.A.P. RTE. 365 | SECTION 60 RS-3 | COUNTY KANE | TOTAL SHEETS 23 | SHEET NO. 7 | |
| et:\pwork\puidot\beekertem\d244424\0610-sht-plan.dgn | PLOT SCALE = 50,0000 "/> IN. | DRAWN - | REVISED - | | | SCALE: | SHEET NO. OF SHEETS | STA. TO STA. | FED. ROAD DIST. NO. 1 ILLINOIS | FED. AID PROJECT | CONTRACT NO. 60K81 |
| | PLOT DATE = 3/14/2011 | CHECKED - | REVISED - | | | | | | | | |



PAVEMENT MARKING KEY NOTE LEGEND:

- (A) PROP. THERMO. PVT. MARKING (YELLOW) - 4" SOLID CENTERLINE (2 LINES @ 11" C-C) - (TYP.)
- (B) PROP. THERMO. PVT. MARKING (YELLOW) - 4" SKIP-DASH CENTERLINE (10' LINE - 30' SPACE) - (TYP.)
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- (K) PROP. THERMO. PVT. MARKING (WHITE) - 24" SOLID STOP BAR - (TYP.)
- (L) PROP. THERMO. PVT. MARKING (WHITE) LETTERS AND SYMBOLS - (TYP.)

**PROJECT ENDS
STA.: 62+92**



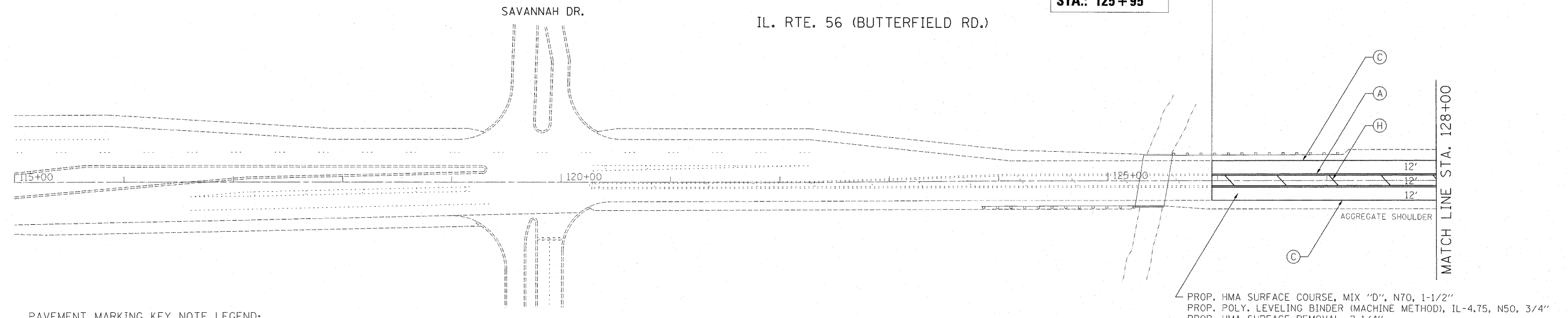
PROP. HMA SURFACE COURSE, MIX "D", N70, 1-1/2"
PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
PROP. HMA SURFACE REMOVAL, 2-1/4"

Rev.: 3/16/2011

| | | | | | | | | | | |
|---|-----------------------------|------------|-------------------|---|---|-----------------|----------------------------------|---|-----------------|-------------|
| FILE NAME = c:\pwork\pwork\beaker\com\d0244424\010610-shl-plan.dgn | USER NAME = beaker\com | DESIGNED - | REVISED 3/16/2011 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | IL RTE. 56 (BUTTERFIELD RD.) LOCATION 1 (IL RTE. 25 TO MITCHELL RD.) ROADWAY AND PAVEMENT MARKING PLAN | F.A.P. RTE. 365 | SECTION 60 RS-3 | COUNTY KANE | TOTAL SHEETS 23 | SHEET NO. 8 |
| | PLOT SCALE = 50.0000' / IN. | CHECKED - | REVISED - | | | SCALE: | SHEET NO. OF SHEETS STA. TO STA. | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | |
| PLOT DATE = 3/18/2011 | DATE - | REVISED - | REVISED - | CONTRACT NO. 60K81 | | | | | | |



**PROJECT BEGINS
STA.: 125+95**



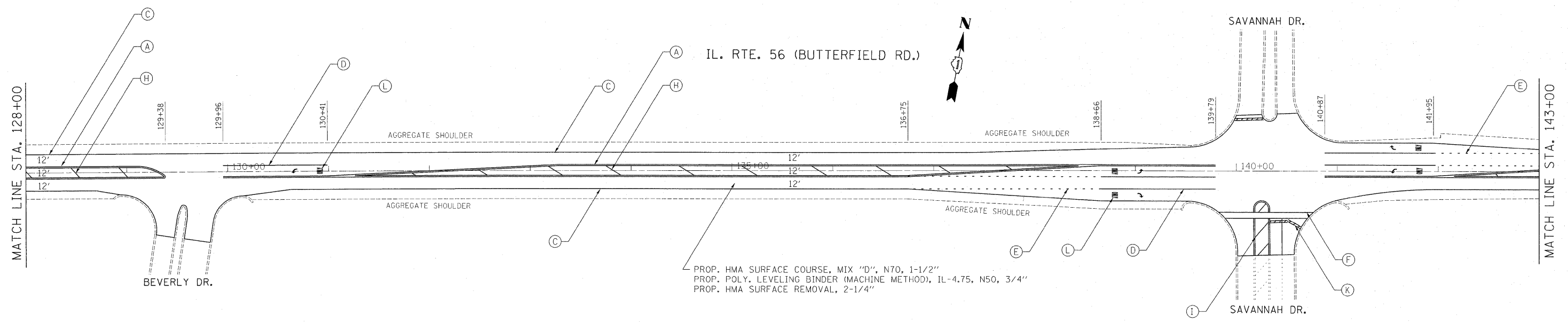
PAVEMENT MARKING KEY NOTE LEGEND:

- (A) PROP. THERMO. PVT. MARKING (YELLOW) - 4" SOLID CENTERLINE (2 LINES @ 11" C-C) - (TYP.)
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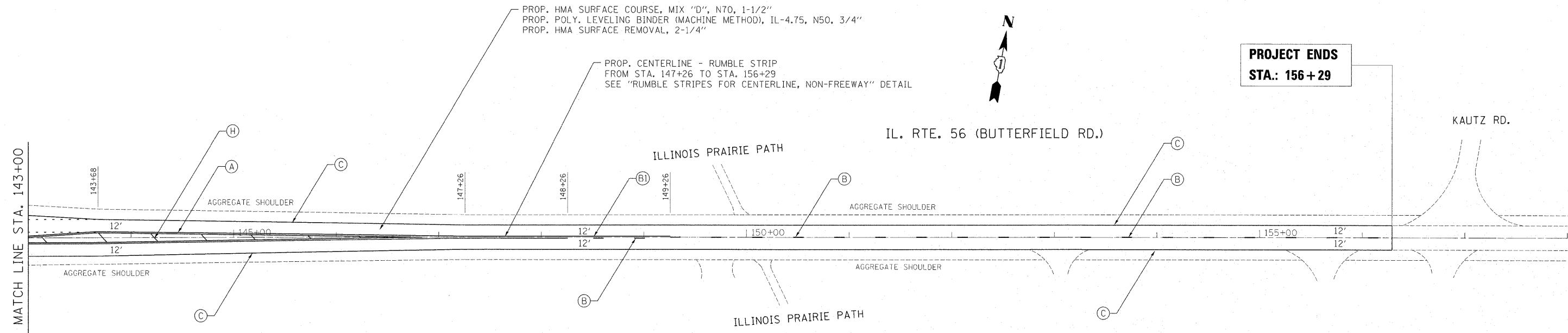
PROP. HMA SURFACE COURSE, MIX "D", N70, 1-1/2"
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Rev: 3/16/2011

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| FILE NAME = | USER NAME = beaker.tom | DESIGNED - | REVISED 3/16/2011 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | IL RTE. 56 (BUTTERFIELD RD.) LOCATION 2 (FARNSWORTH AVE. TO DuPAGE CO. LINE) ROADWAY AND PAVEMENT MARKING PLAN | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| c:\pv_work\p\dot\beaker.tom\d0244424\01506102-sht-plen.dgn | DRAWN - | REVISED - | 365 | | | 60 RS-3 | KANE | 23 | 9 | |
| PLOT SCALE = 50.0000' / IN. | CHECKED - | REVISED - | CONTRACT NO. 60K81 | | | | | | | |
| PLOT DATE = 3/14/2011 | DATE - | REVISED - | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | | | | |
| | | | | | SCALE: | SHEET NO. | OF | SHEETS | STA. | TO STA. |



PAVEMENT MARKING KEY NOTE LEGEND:

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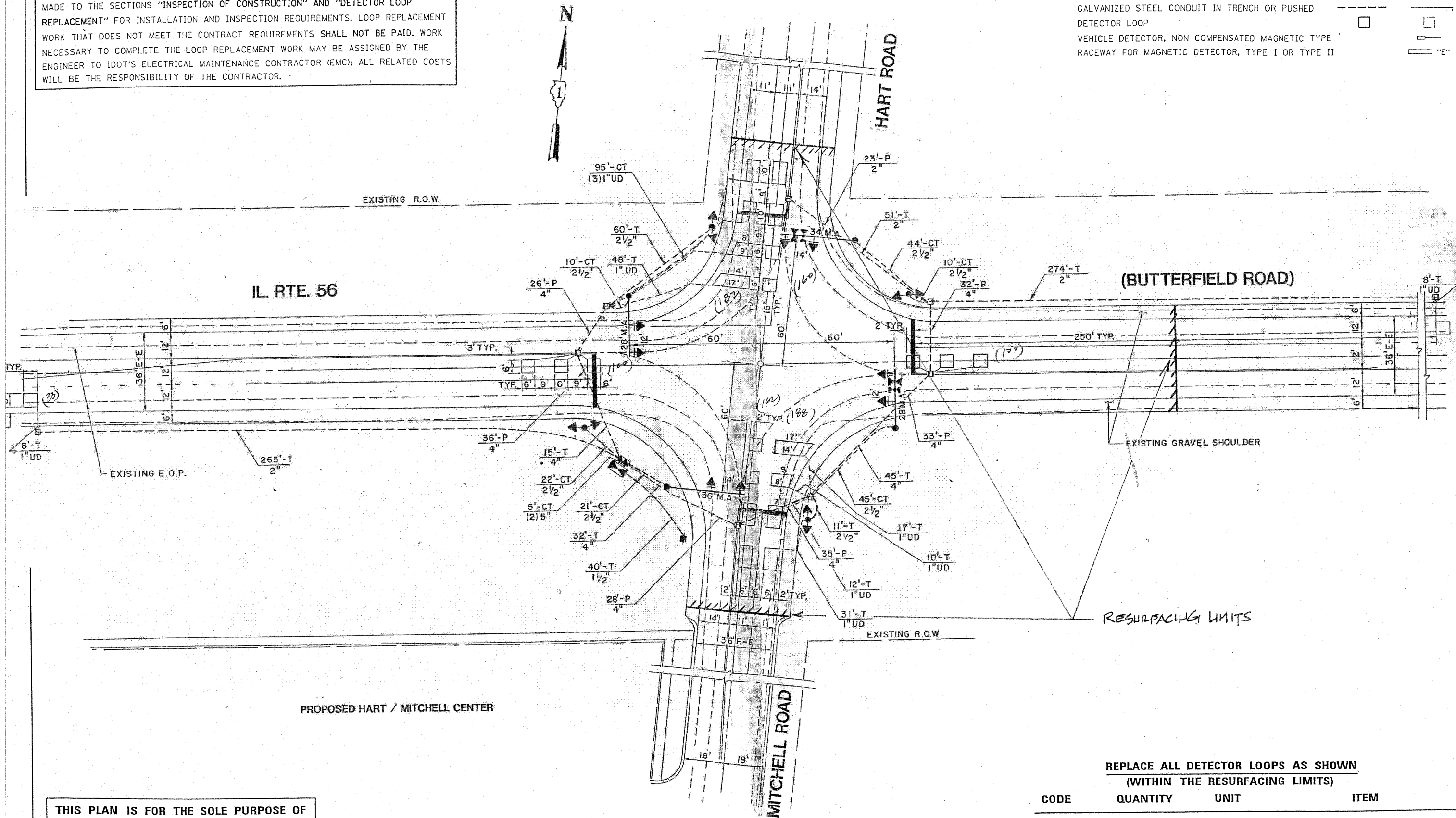
Rev.: 3/16/2011

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| FILE NAME = | USER NAME = becker.tom | DESIGNED - | REVISED 3/16/2011 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | IL RTE. 56 (BUTTERFIELD RD.) LOCATION 2 (FARNSWORTH AVE. TO DuPAGE CO. LINE) ROADWAY AND PAVEMENT MARKING PLAN | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| es:\pw_work\puidot\becker.tom\d0244424\0610-sht-plan.dgn | 0610-sht-plan.dgn | DRAWN - | REVISED - | | | 365 | 60 RS-3 | KANE | 23 | 10 |
| PLOT SCALE = 50,0000 ' / IN. | | CHECKED - | REVISED - | | | CONTRACT NO. 60K81 | | | | |
| PLOT DATE = 3/14/2011 | | DATE - | REVISED - | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |
| | | | | SCALE: | SHEET NO. OF SHEETS STA. TO STA. | | | | | |

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

TRAFFIC SIGNAL LEGEND

| | PROPOSED | EXISTING |
|--|----------|----------|
| SIGNAL HEAD WITH BACKPLATE | | |
| SIGNAL HEAD | | |
| GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED | | |
| DETECTOR LOOP | | |
| VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE | | |
| RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II | | |



**THIS PLAN IS FOR THE SOLE PURPOSE OF
DETECTOR LOOP REPLACEMENTS ONLY**

**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

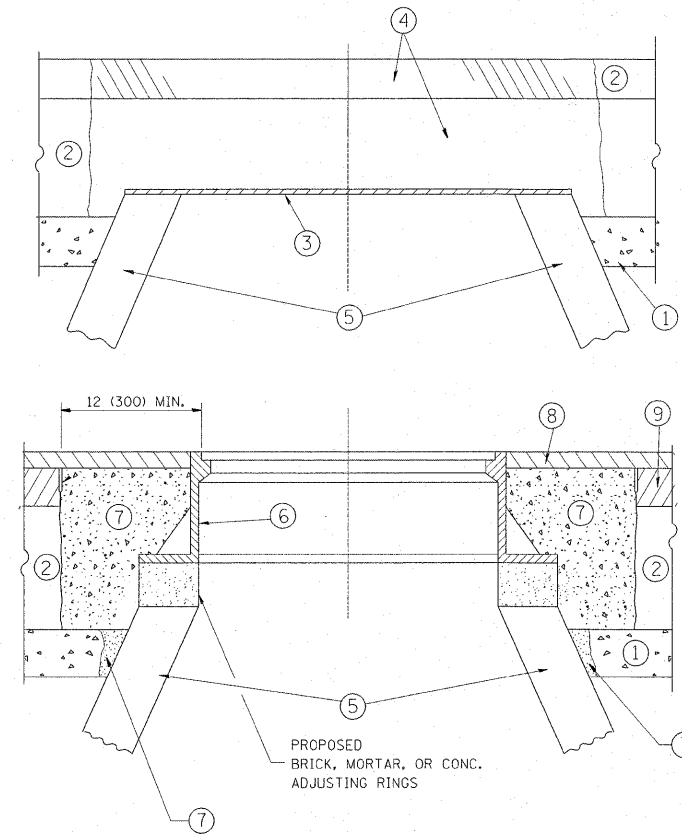
| CODE | QUANTITY | UNIT | ITEM |
|----------|----------|------|----------------------------|
| 88600600 | 925 | FOOT | DETECTOR LOOP, REPLACEMENT |

| | | | |
|--|----------------------|------------|-----------|
| FILE NAME = | USER NAME = nguyensm | DESIGNED - | REVISED - |
| ct:\pw\work\p\dtd\nguyensm\d0112618\1\as | er.dgn | DRAWN - | REVISED - |
| PLOT SCALE = 100.0000 / IN. | | CHECKED - | REVISED - |
| PLOT DATE = 12/1/2010 | | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE - DETECTOR LOOP REPLACEMENT
ILL. ROUTE 56 @ HART/MITCHELL RDS.

| | | | | |
|---------------------|---------|---------------------------|--------------|--------------|
| F.A. P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 365 | 60-RS-3 | KANE | 23 | 11 |
| SCALE: | | SHEET NO. OF SHEETS | | STA. TO STA. |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP CONCRETE OR THE BINDER COURSE.

* THE CLASS OF PP CONCRETE WILL BE AS DIRECTED BY THE ENGINEER.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL" NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

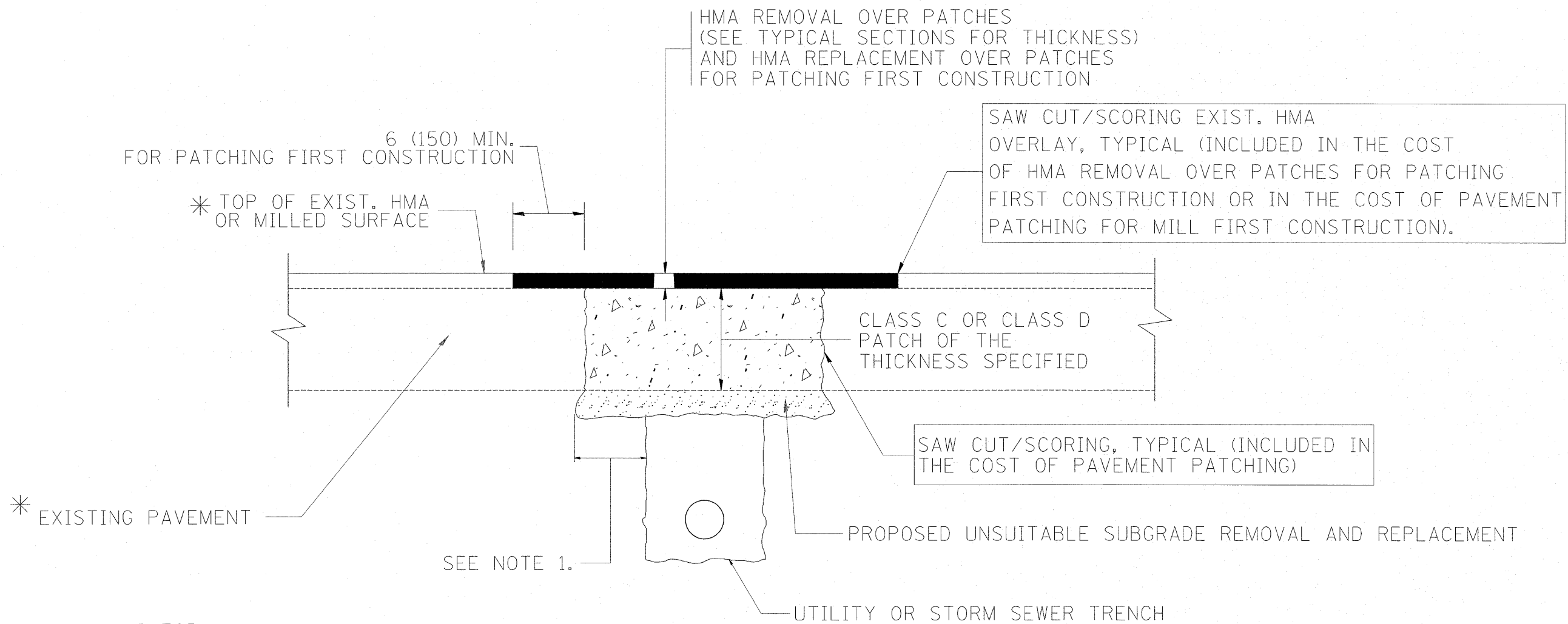
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

| | | | | | | | | | | | | |
|---|------------------------|--------------------|--------------------------------|---|--|-------------------------|--------------|---|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = becker.tom | DESIGNED - R. SHAH | REVISED - A. ABBAS 03-21-97 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ci\pw_work\p\dot\becker.tom\d0244424\d0244424.dgn | | DRAWN - | REVISED - R. WIEDEMAN 05-14-04 | | 365 | 60 RS-3 | KANE | 23 | 12 | | | |
| PLOT SCALE = 50.0000' / IN. | | CHECKED - | REVISED - R. BORO 01-01-07 | | BD600-03 (BD-8) | | | CONTRACT NO. 60K81 | | | | |
| PLOT DATE = 2/7/2011 | | DATE - 10-25-94 | REVISED - R. BORO 02-01-11 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. TO STA. | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

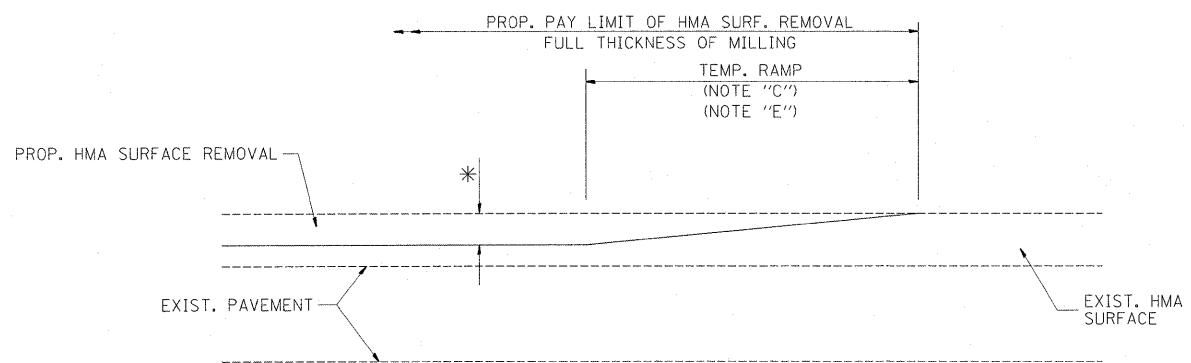
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

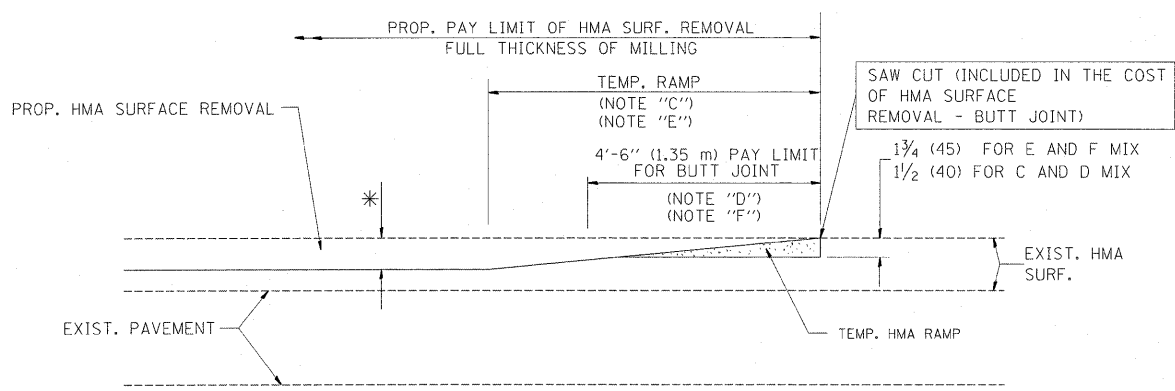
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | | | | | | | | | | |
|--|------------------------|--------------------|-----------------------------|---|--|-------------------------|--------------|---|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = becker.tcm | DESIGNED - R. SHAH | REVISED - A. ABBAS 04-27-98 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ci:\pav_work\pav\becker.tcm\d0244424\01stStd.dgn | | DRAWN - | REVISED - R. BORO 01-01-07 | | 365 | 60 RS-3 | KANE | 23 | 13 | | | |
| PLOT SCALE = 50.0000' / IN. | | CHECKED - | REVISED - R. BORO 09-04-07 | | BD400-04 (BD-22) | | | CONTRACT NO. 60K81 | | | | |
| PLOT DATE = 2/7/2011 | | DATE - 10-25-94 | REVISED - K. ENG 10-27-08 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. TO STA. | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



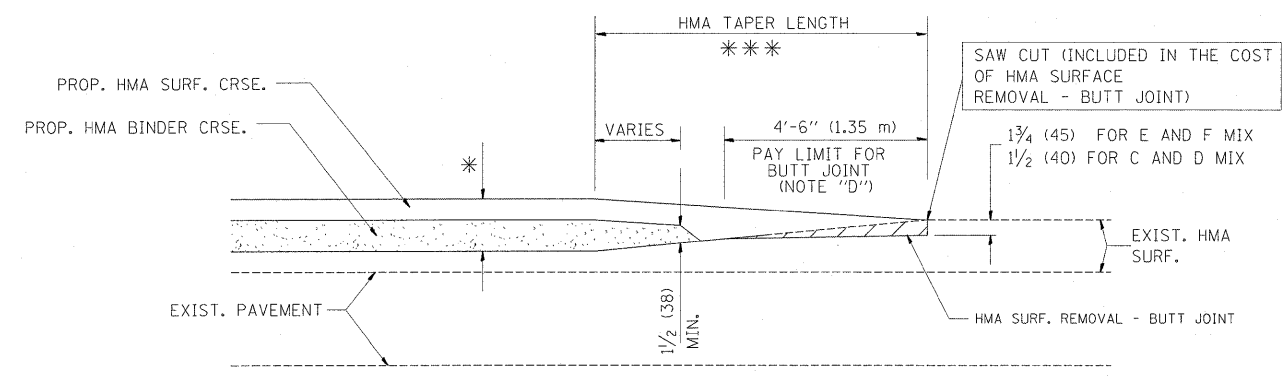
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1



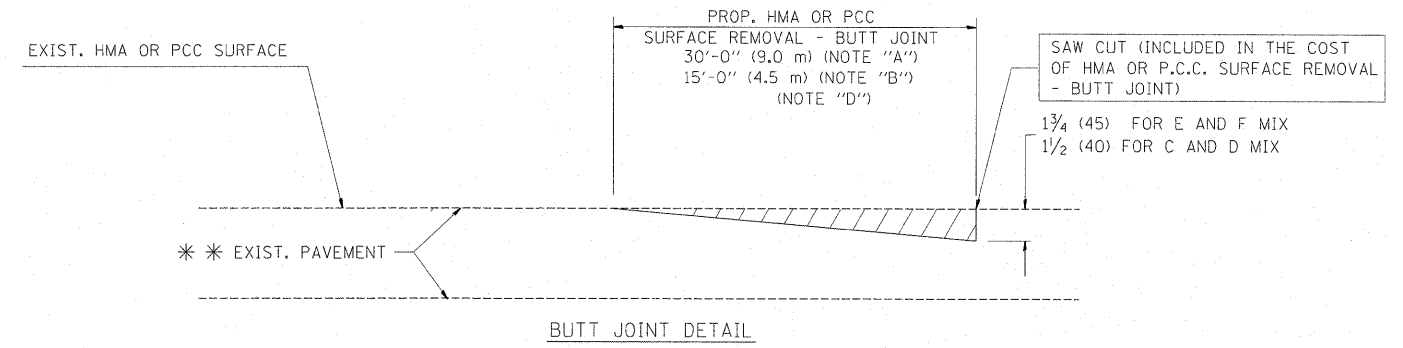
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2
TYPICAL TEMPORARY RAMP

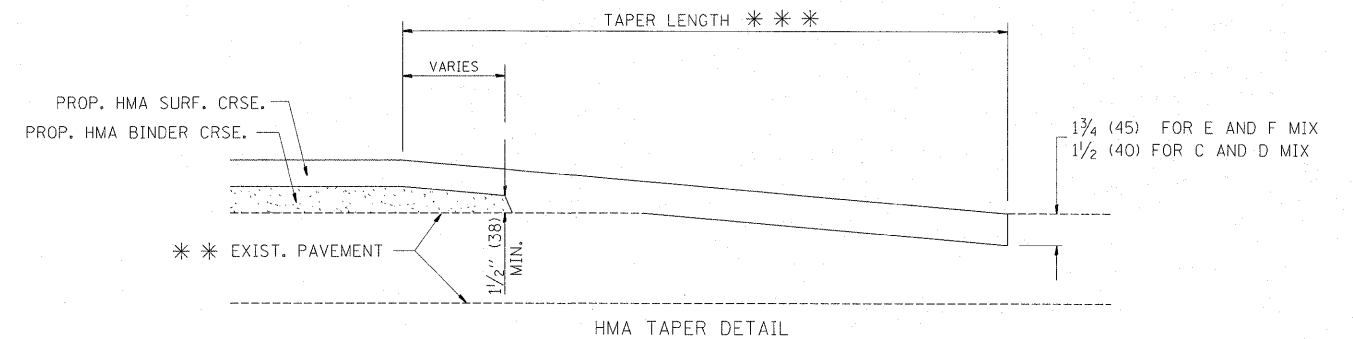


BUTT JOINT AND
HMA TAPER

**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

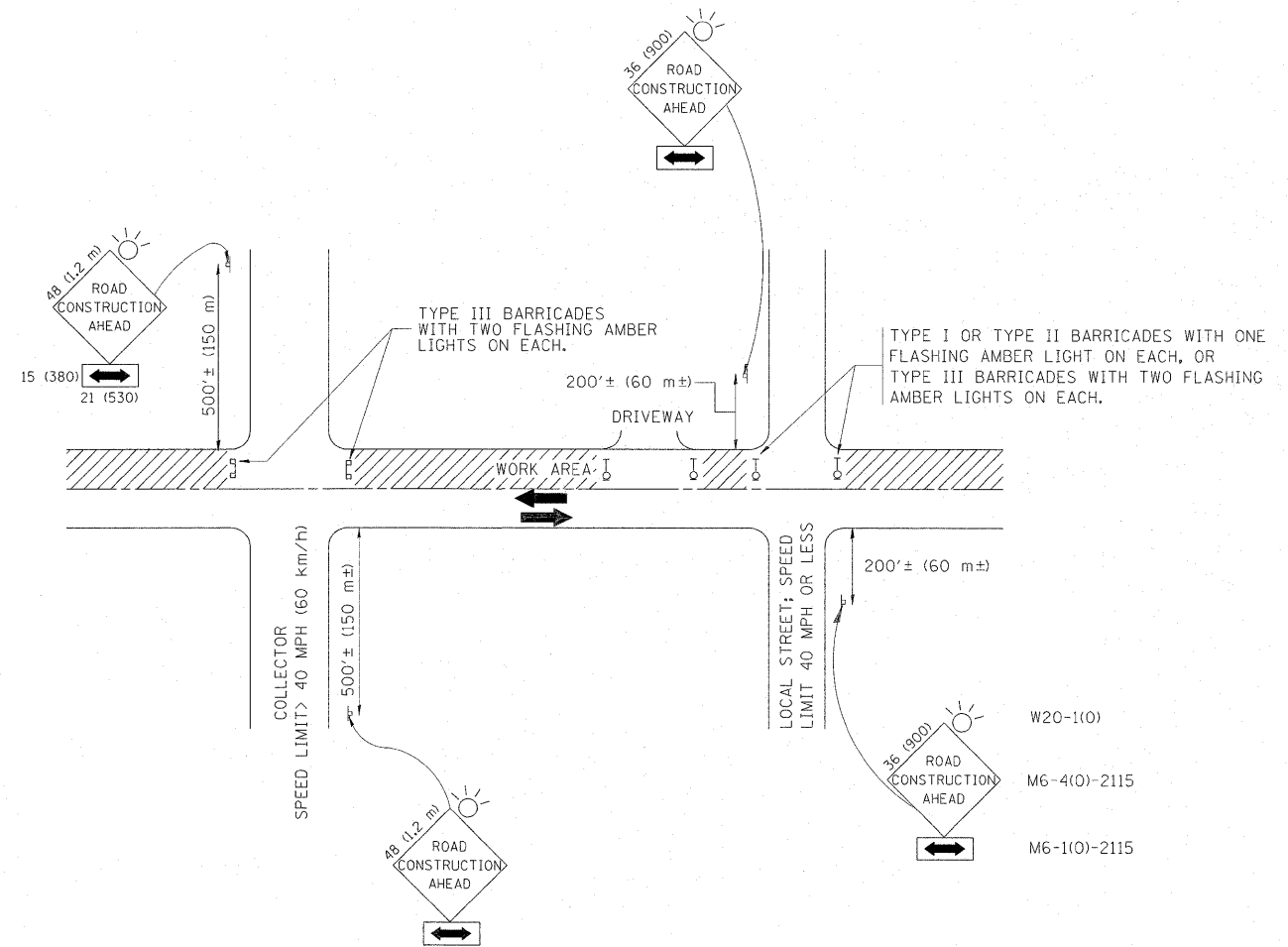
- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | | | | | | | | | | |
|--|------------------------|-----------------------------|-----------------------------|---|---|-------------------------|--------------|----------------------|--------------------|----------------|--------------------|-----------------|
| FILE NAME = c:\pw_work\pwidot\becker\tdm\0244424\d01std.dgn | USER NAME = becker\tdm | DESIGNED - M. DE YONG | REVISED - R. SHAH 10-25-94 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | BUTT JOINT AND HMA TAPER DETAILS | | | F.A.P. RTE. 365 | SECTION 60 RS-3 | COUNTY KANE | TOTAL SHEETS 23 | SHEET NO. 14 |
| PLOT SCALE = 50.0000' / IN. | CHECKED - | REVISED - A. ABBAS 03-21-97 | REVISED - M. GOMEZ 04-06-01 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. TO STA. | BD400-05 BD32 | | | | |
| PLOT DATE = 2/7/2011 | DATE - 06-13-90 | REVISED - R. BORO 01-01-07 | | | CONTRACT NO. 60K81 | | | | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | | | | | | | | | |



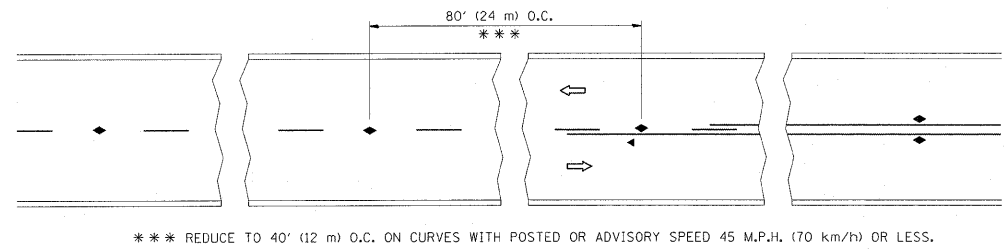
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 - 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 - USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
 - C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
 - D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

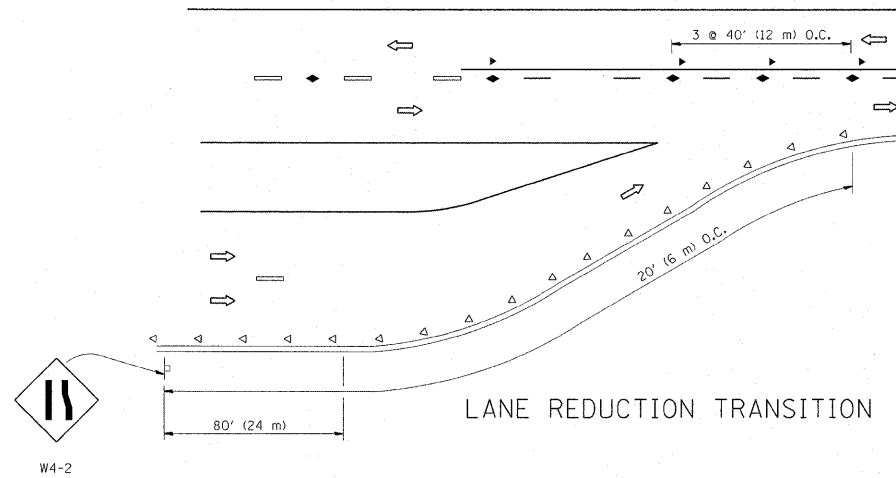
All dimensions are in millimeters (inches) unless otherwise shown.

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| FILE NAME = | USER NAME = becker.tcm | DESIGNED - LHA | REVISED - J. OBERLE 10-18-95 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS | | | F.A.P. RTE. 365 | SECTION 60 RS-3 | COUNTY KANE | TOTAL SHEETS 23 | SHEET NO. 15 |
| et:\pw_work\p1dot\becker.tcm\d0244424\DistStd.dgn | | DRAWN - | REVISED - A. HOUSEH 03-06-96 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | TC-10 | | CONTRACT NO. 60K81 | |
| | | CHECKED - | REVISED - A. HOUSEH 10-15-96 | | | | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | |
| | | DATE - 06-89 | REVISED - T. RAMMACHER 01-06-00 | | | | | | | | | |

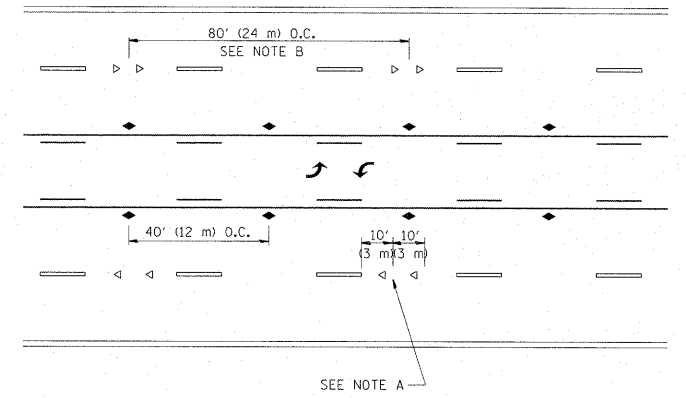


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

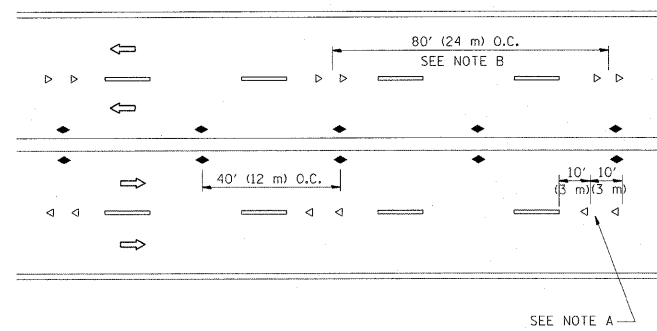
TWO-LANE/TWO-WAY



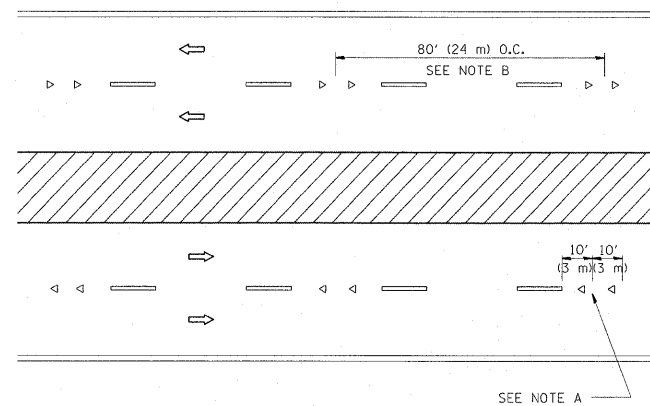
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

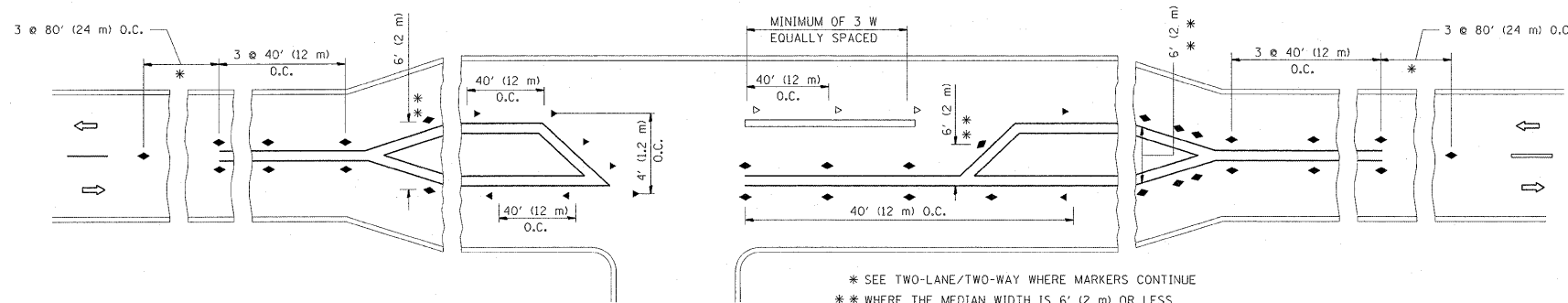
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

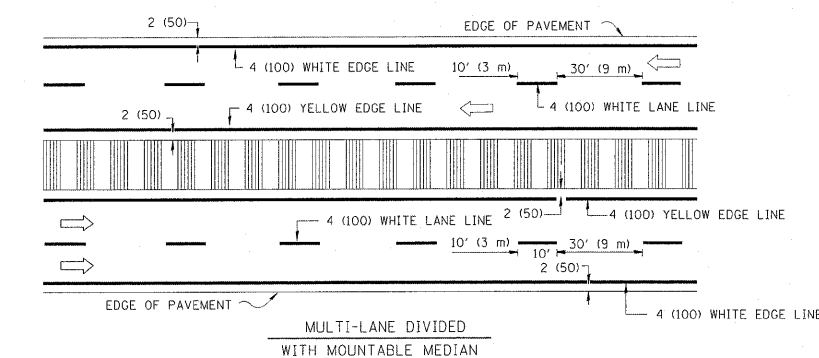
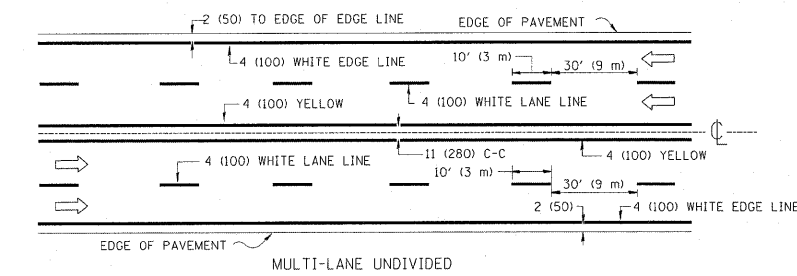
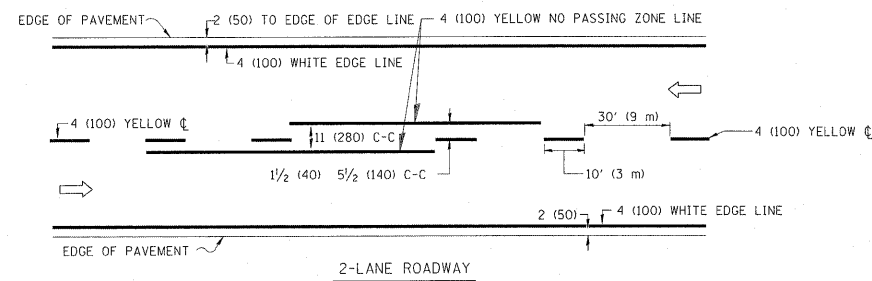
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| | PLOT SCALE = 50.0000' / IN. | CHECKED - | REVISED - T. RAMMACHER 01-06-00 |
| | PLOT DATE = 2/7/2011 | DATE - | REVISED - C. JUCIUS 09-09-09 |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

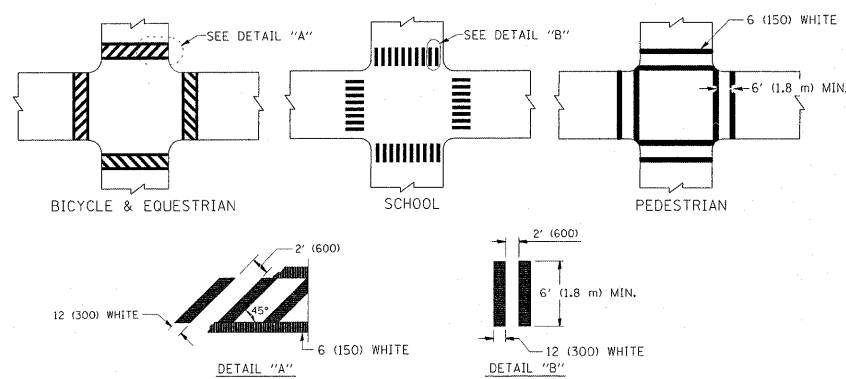
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|--------|--------------------|-----------|
| 365 | 60 RS-3 | KANE | 23 | 16 |
| TC-11 | | | CONTRACT NO. 60K81 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

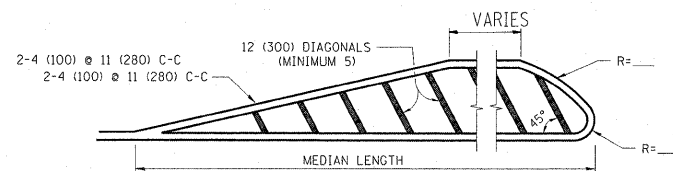
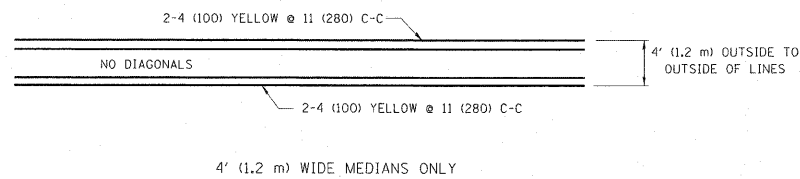


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

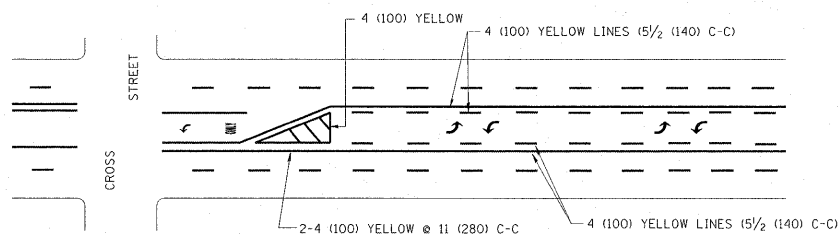


TYPICAL CROSSWALK MARKING

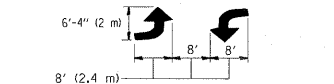


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

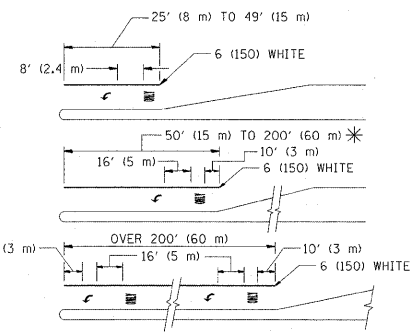
MEDIANS OVER 4' (1.2 m) WIDE



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



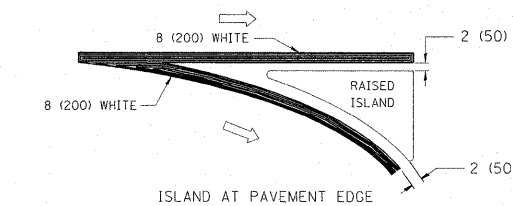
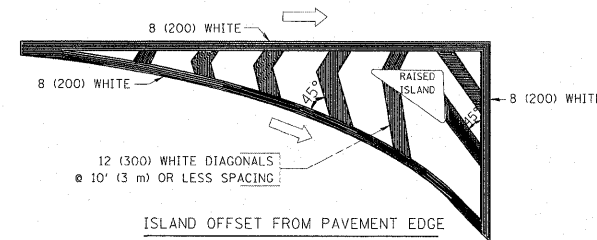
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

| TYPE OF MARKING | WIDTH OF LINE | PATTERN | COLOR | SPACING / REMARKS |
|---|--|------------------------------|---|--|
| CENTERLINE ON 2 LANE PAVEMENT | 4 (100) | SKIP-DASH | YELLOW | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT | 2 @ 4 (100) | SOLID | YELLOW | 11 (280) C-C |
| NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS | 4 (100) 2 @ 4 (100) | SOLID SOLID | YELLOW YELLOW | 5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN |
| LANE LINES | 4 (100) 5 (125) ON FREEWAYS | SKIP-DASH SKIP-DASH | WHITE WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS) | SAME AS LINE BEING EXTENDED | SKIP-DASH | SAME AS LINE BEING EXTENDED | 2' (600) LINE WITH 6' (1.8 m) SPACE |
| EDGE LINES | 4 (100) | SOLID | YELLOW-LEFT WHITE-RIGHT | OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB |
| TURN LANE MARKINGS | 6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m)) | SOLID | WHITE | SEE TYPICAL TURN LANE MARKING DETAIL |
| TWO WAY LEFT TURN MARKING | 2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW | SKIP-DASH AND SOLID IN PAIRS | YELLOW WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL |
| CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL) | 2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90° | SOLID SOLID SOLID | WHITE WHITE WHITE | NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS. |
| STOP LINES | 24 (600) | SOLID | WHITE | PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS | 2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS | SOLID | YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC | 11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING. |
| GORE MARKING AND CHANNELIZING LINES | 8 (200) WITH 12 (300) DIAGONALS @ 45° | SOLID | WHITE | DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h)) |
| RAILROAD CROSSING | 24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X" | SOLID | WHITE | SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²) |
| SHOULDER DIAGONALS | 12 (300) @ 45° | SOLID | WHITE - RIGHT YELLOW - LEFT | 50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h)) |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

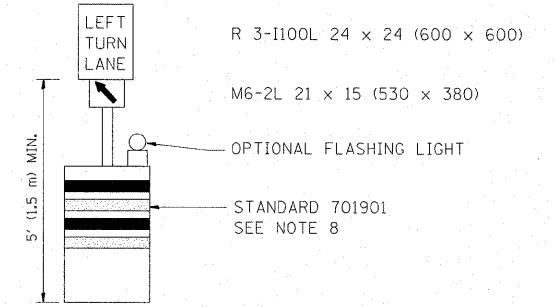
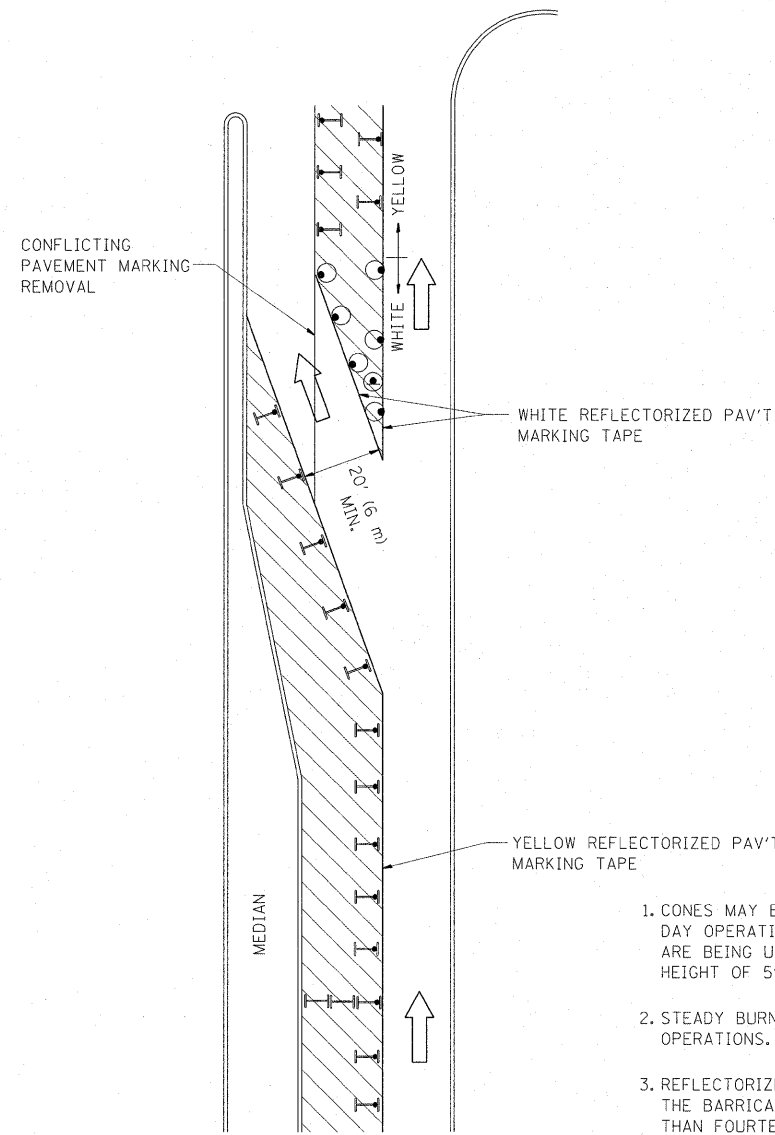
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| PLOT SCALE = 50.0000' / IN. | | CHECKED - | REVISED - |
| PLOT DATE = 2/7/2011 | | DATE = 03-19-90 | REVISED - |

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|---|-----------------|-------------|--------------------|--------------|
| F.A.P. RTE. 365 | SECTION 60 RS-3 | COUNTY KANE | TOTAL SHEETS 23 | SHEET NO. 17 |
| TC-13 | | | CONTRACT NO. 60K81 | |
| FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT | | | | |

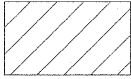







GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

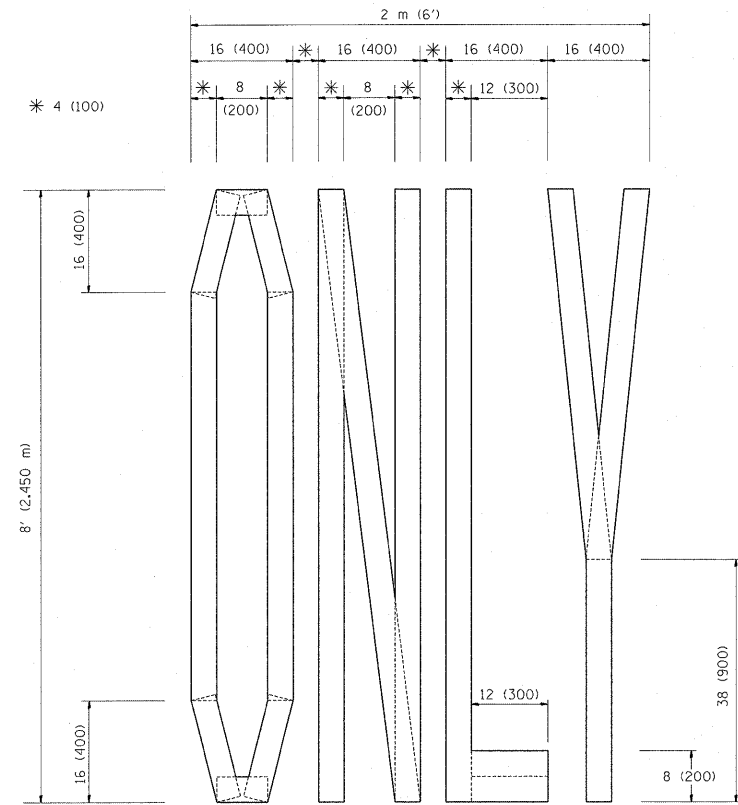
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| | | REVISED - A. HOUSEH 10-12-96 | REVISED - |
| PLOT SCALE = 50.0000' / IN. | | REVISED - T. RAMMACHER 01-06-00 | REVISED - |
| PLOT DATE = 2/7/2011 | | | |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

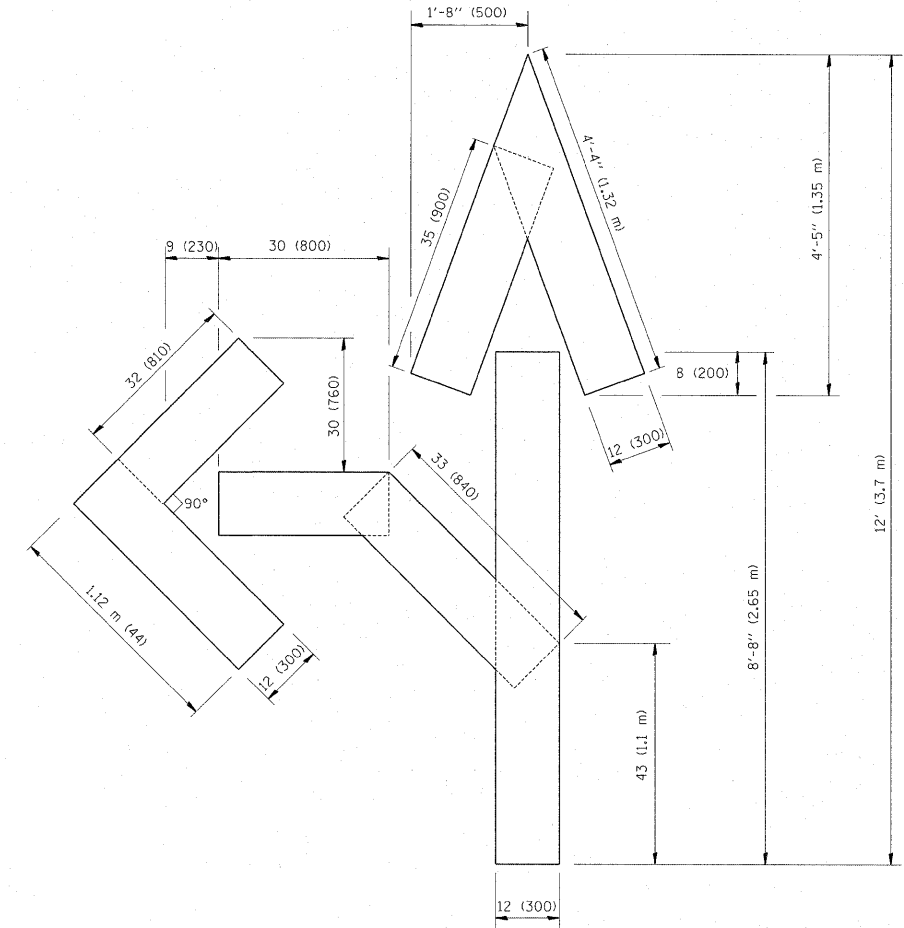
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

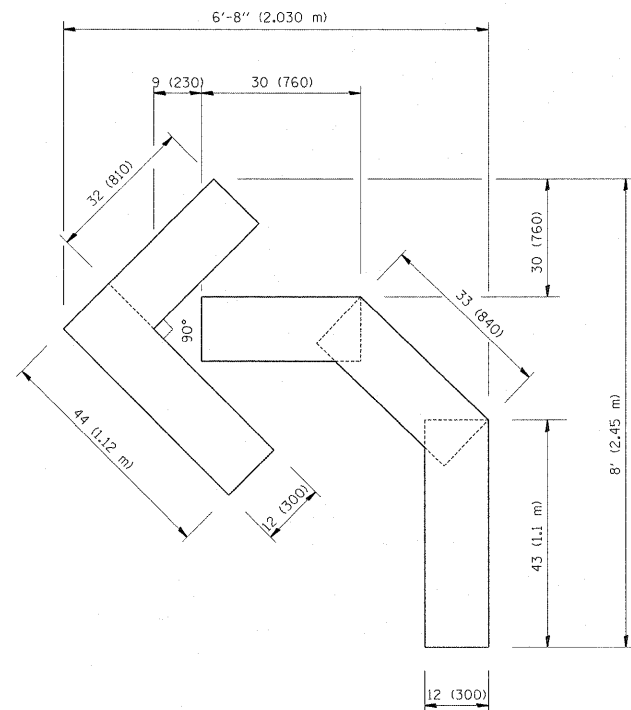
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 365 | 60 RS-3 | KANE | 23 | 18 |
| TC-14 | | CONTRACT NO. 60K81 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

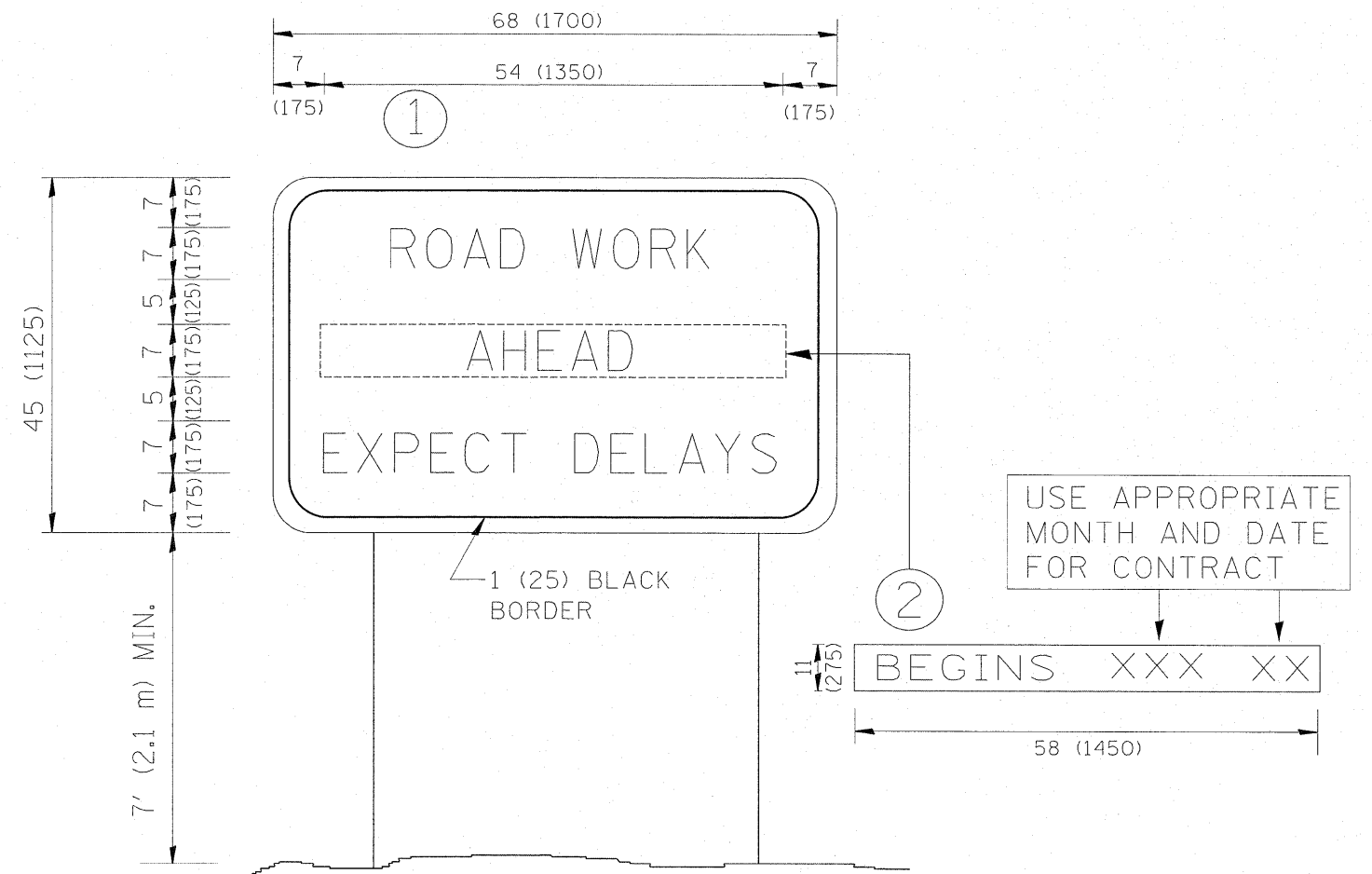
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|--|------------------------|-----------------|--------------------------------|
| FILE NAME = | USER NAME = becker.tcm | DESIGNED - | REVISED -T. RAMMACHER 06-05-96 |
| os:\pwork\pwork\dot\becker.tcm\d0244424\d0244424.dgn | std.dgn | DRAWN - | REVISED -T. RAMMACHER 11-04-97 |
| PLOT SCALE = 58.2000' / 1IN. | | CHECKED - | REVISED -T. RAMMACHER 03-02-98 |
| PLOT DATE = 2/7/2011 | | DATE - 09-18-94 | REVISED -E. GOMEZ 08-28-00 |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|---|---------|--------|--------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 365 | 60 RS-3 | KANE | 23 | 19 |
| TC-16 | | | CONTRACT NO. 60K81 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

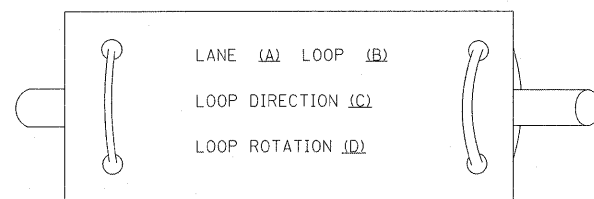
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | | | | | | | | |
|--|------------------------|---------------------------------|----------------------------|---|---|---------------------------|--------------|---|--------------|-----------|
| FILE NAME = | USER NAME = becker.tom | DESIGNED - | REVISED - R. MIRS 09-15-97 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | ARTERIAL ROAD INFORMATION SIGN | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ct:\pw-work\p\dot\becker.tom\d0244424\d0244424.dgn | DRAWN - | REVISED - R. MIRS 12-11-97 | 365 | | | 60 RS-3 | KANE | 23 | 20 | |
| PLOT SCALE = 50.0000' / IN. | CHECKED - | REVISED - T. RAMMACHER 02-02-99 | TC-22 | | | CONTRACT NO. 60K81 | | | | |
| PLOT DATE = 2/7/2011 | DATE - | REVISED - C. JUCIUS 01-31-07 | SCALE: NONE | | | SHEET NO. 1 OF 1 SHEETS | STA. TO STA. | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | |

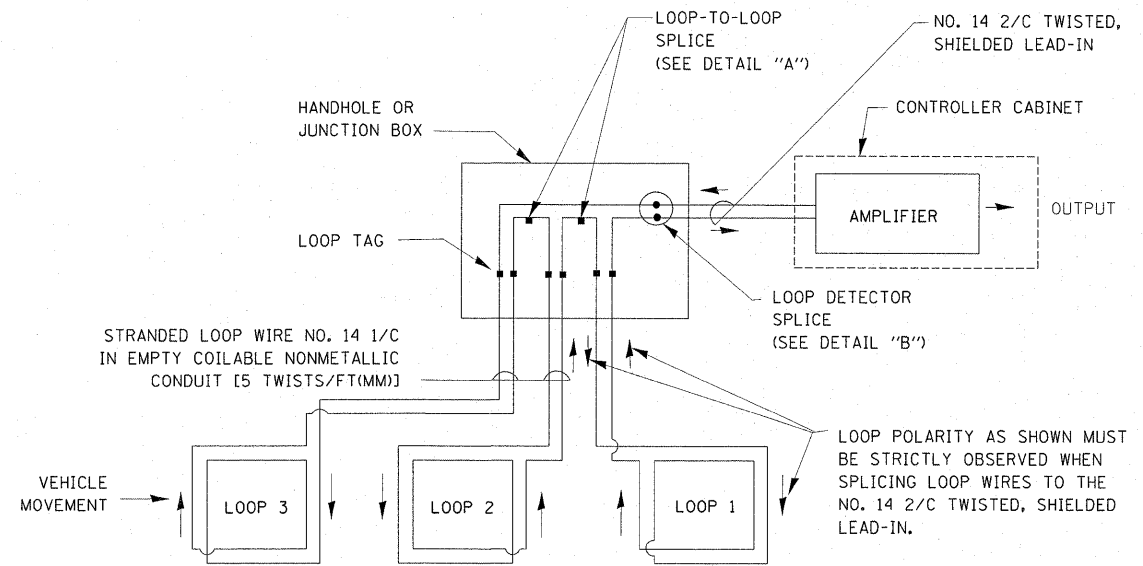
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

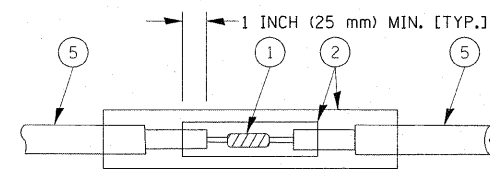


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

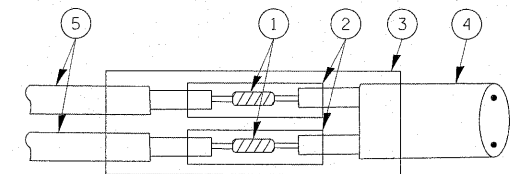


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

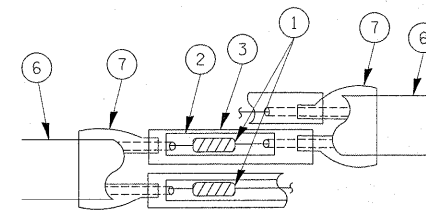


DETAIL "A" LOOP-TO-LOOP SPLICE

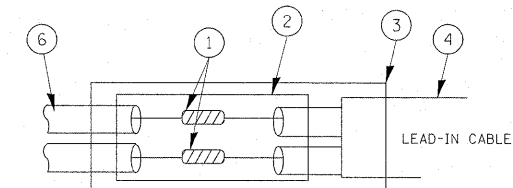


DETAIL "B" LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A" LOOP-TO-LOOP SPLICE



DETAIL "B" LOOP-TO-CONTROLLER SPLICE

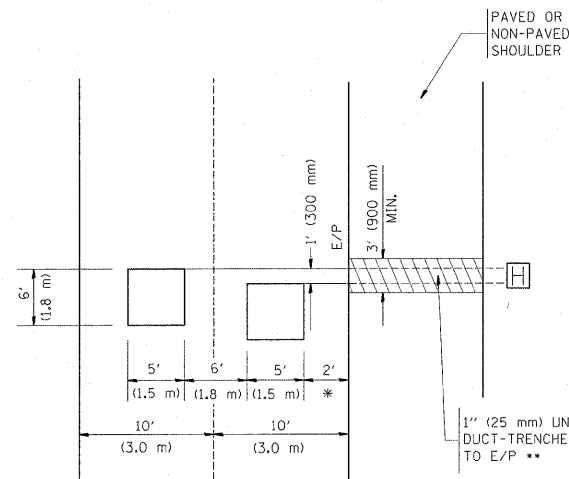
LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

| | | | | | | | | | | |
|--|------------------------|----------------|--------------|---|--|---|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = becker.tom | DESIGNED - DAD | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ci:\pw\work\p\dot\becker.tom\d0244424\dotStd.dgn | DRAWN - BCK | REVISED - | 365 | | | 60 RS-3 | KANE | 23 | 21 | |
| PLOT SCALE = 50.0000' / IN. | CHECKED - DAD | REVISED - | TS-05 | | | CONTRACT NO. 60K81 | | | | |
| PLOT DATE = 2/7/2011 | DATE - 10-28-09 | REVISED - | SCALE: NONE | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |
| | | | | SHEET NO. 1 OF 6 SHEETS STA. TO STA. | | | | | | |

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

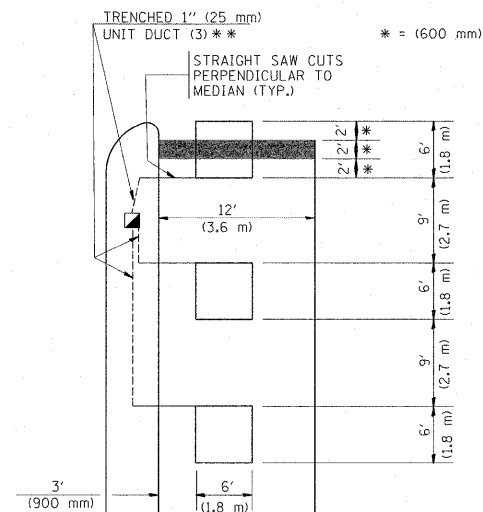


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

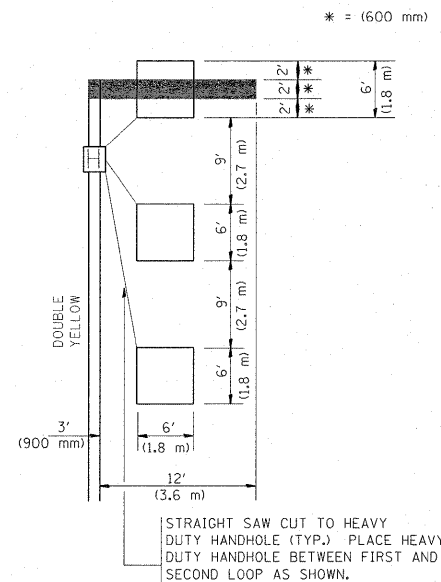
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD B14001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

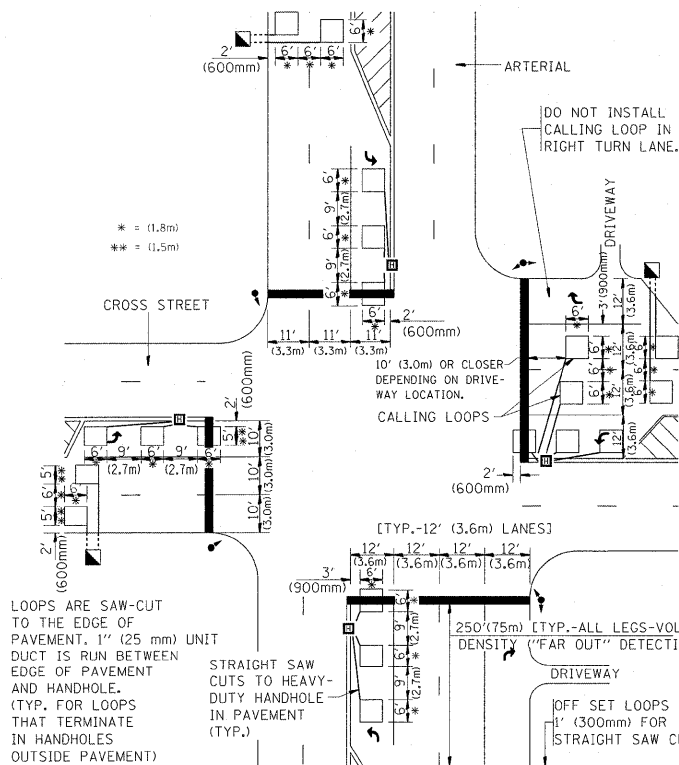
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



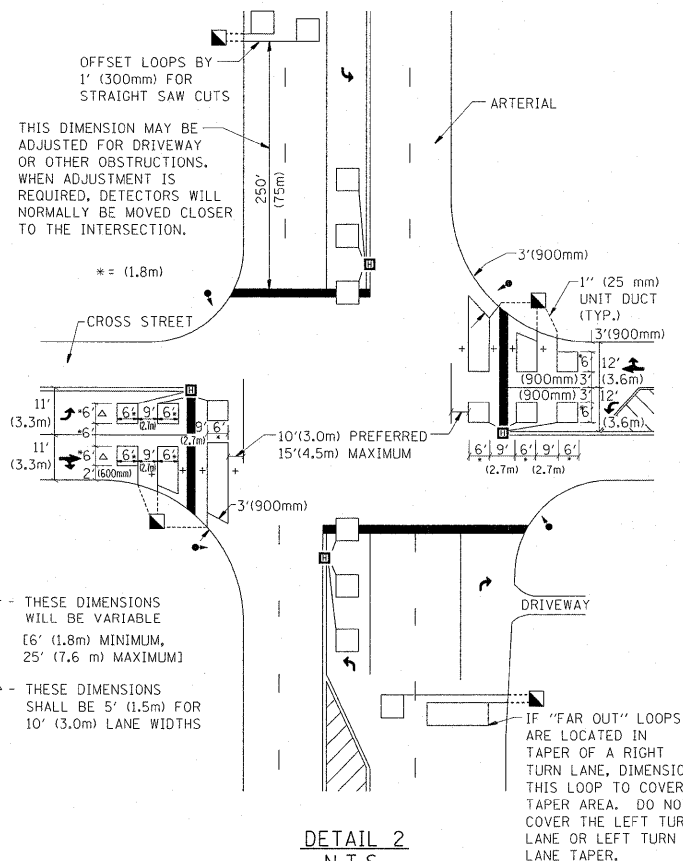
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

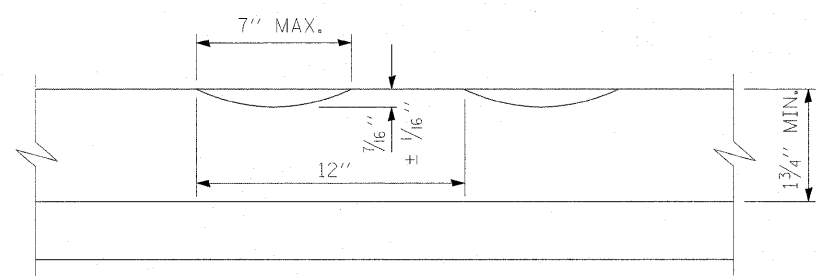
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSS STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

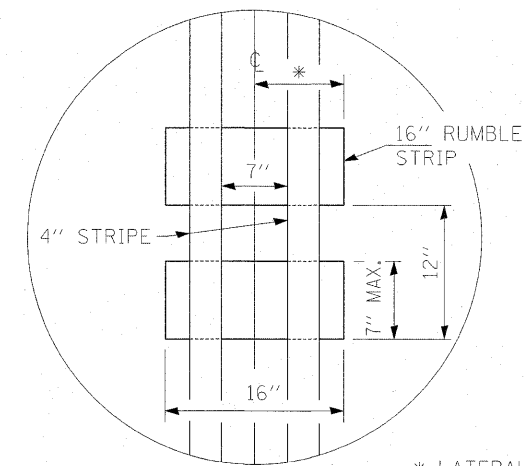
ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

| | | | | | | | | | | | | | | | | |
|---|------------------------|------------------|-----------|---|--|--|--|--|--|--|--|--------------------|---------|--------|--------------|-----------|
| FILE NAME = | USER NAME = becker-tcm | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | | | | DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING | | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ci:\pwwork\pwwork\becker-tcm\02444241\DistStd.dgn | | DRAWN - | REVISED - | | | | | | | | | 365 | 60 RS-3 | KANE | 23 | 22 |
| PLOT SCALE = 50.0000' / IN. | | CHECKED - R.K.F. | REVISED - | SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA. | | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | | | | | |
| PLOT DATE = 2/7/2011 | | DATE | REVISED - | | | | | | | | | CONTRACT NO. 60K81 | | | | |

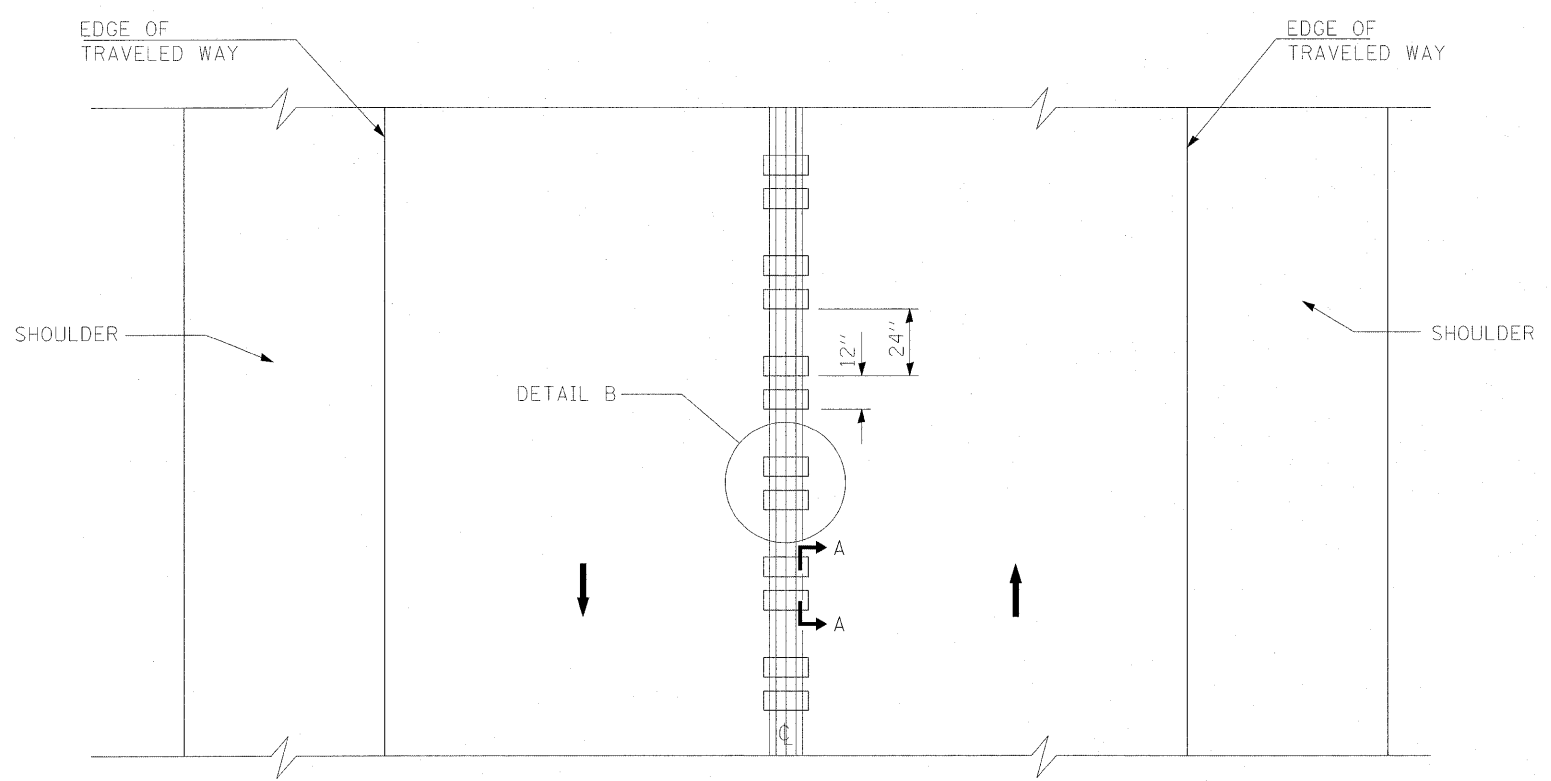


SECTION A-A



DETAIL B

* LATERAL DEVIATION SHALL NOT EXCEED ONE INCH IN 100 FEET.



TWO-WAY ROAD

GENERAL NOTES

- SEE STANDARD 780001 FOR OTHER STRIPING LAYOUTS.
- RUMBLE STRIPS SHALL NOT BE PLACED ON BRIDGES.
- ALL RUMBLE STRIPS SHALL BE MILLED.
- CENTERLINE RUMBLE STRIPS SHALL BE CONTINUOUS THROUGH CONNECTIONS OF SIDEROADS WITH NO LEFT TURN LANES.
- DISCONTINUE CENTERLINE RUMBLE STRIPS THROUGH THE LIMITS OF ALL LEFT TURN LANES, INCLUDING ANY LANE TAPER SECTIONS.
- WHERE USED, ADJUST SPACING OF RAISED REFLECTIVE PAVEMENT MARKERS TO FALL IN WIDER GAP BETWEEN RUMBLE STRIPS.

RUMBLE STRIPES FOR CENTERLINE, NON-FREEWAY

| | | | | | | | | | | |
|---|------------------------|------------|---|---|---|-------------|-----------|-----------|--------------|-----------|
| FILE NAME = | USER NAME = becker.tom | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | RUMBLE STRIPES FOR CENTERLINE, NON-FREEWAY | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| ci:\pw_work\pwidot\becker.tom\d0244424\0100610-shr-plan.dgn | DRAWN - | REVISED - | 365 | | | 60 RS-3 | KANE | 23 | 23 | |
| PLOT SCALE = 50.0000' / IN. | CHECKED - | REVISED - | CONTRACT NO. 60K81 | | | | | | | |
| PLOT DATE = 2/7/2011 | DATE - | REVISED - | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | | | | |
| | | | | | | SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. |