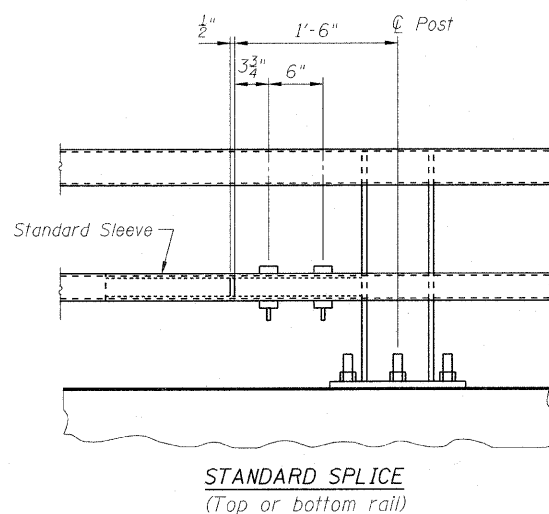
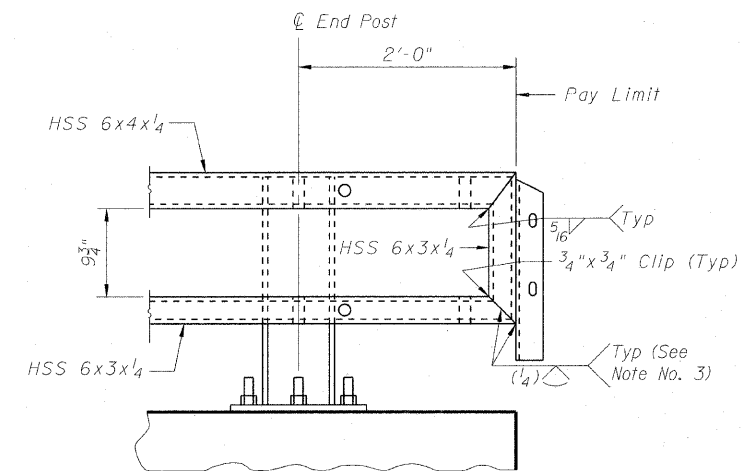


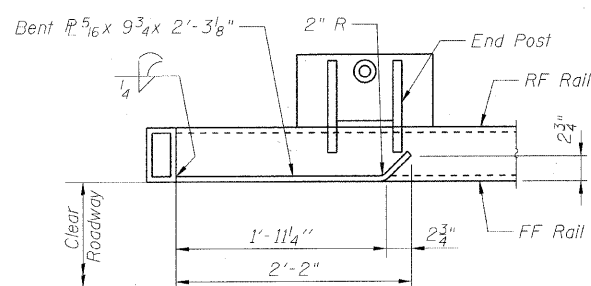
ELEVATION AT TRAFFIC BARRIER TERMINAL (SPECIAL) (DEPARTURE ENDS)



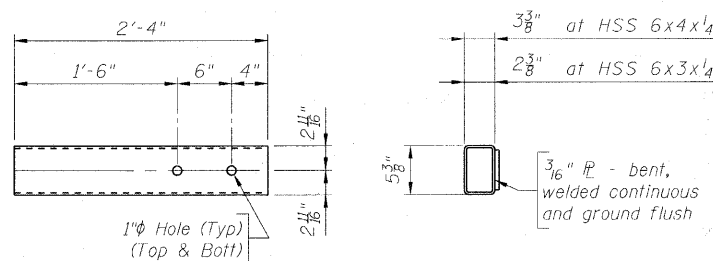
STANDARD SPLICE (Top or bottom rail)



ELEVATION AT TERMINAL TYPE 6A (SPECIAL) (APPROACH ENDS)

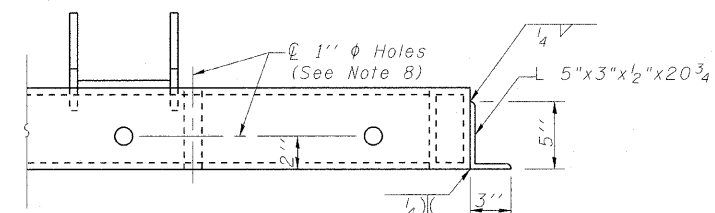


BENT PLATE SECTION (Top rail not shown)

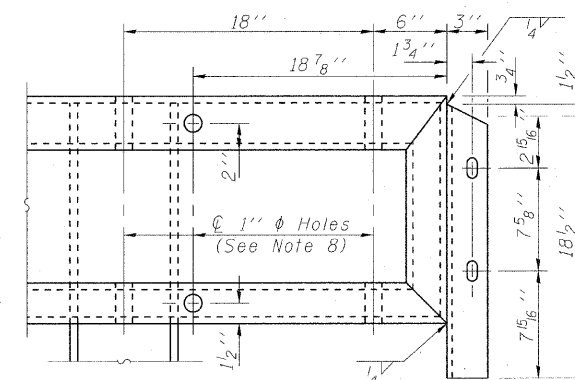


TOP VIEW END VIEW

STANDARD SLEEVE DETAILS

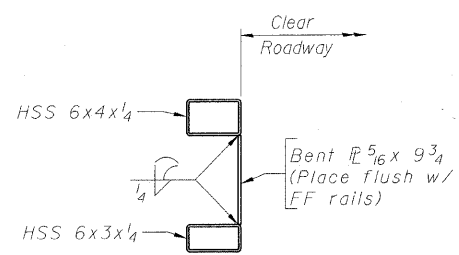


TOP VIEW

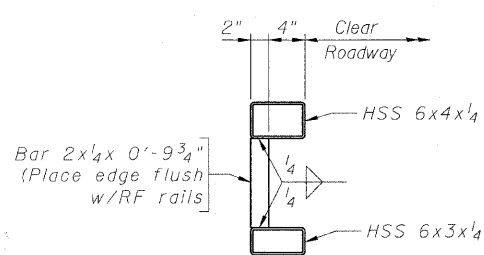


FRONT VIEW

END CONNECTION ANGLES (APPROACH ENDS ONLY)



SECTION C-C



BRACE BAR DETAIL (See Note No. 4)

NOTES

- 1) Either top or bottom rail in terminal section may be the longer rail.
- 2) Ensure each rail length is continuous over a minimum of two posts. Railing that is part of a Traffic Barrier Terminal is continuous if either the top or bottom rail in the terminal is continuous over a minimum of two posts.
- 3) Ensure the fabricator prepares a sample of the indicated joint and it is macroetched to demonstrate that the required effective throat is achieved.
- 4) Ensure a Brace Bar is placed 2'-0" from the splice end of the shorter tube at Traffic Barrier Terminals.
- 5) Splices may be located on either side of post.
- 6) Not more than one splice is permitted per side of post.
- 7) Do not shop splice rails.
- 8) Holes in steel railing may be field drilled for connection to Traffic Barrier Terminals. Any galvanized steel exposed or damaged by drilling operation shall be painted with two coats of a zinc-rich paint conforming to ASTM A 780.

(Sheet 2 of 2)

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FILE NAME =	USER NAME =	DESIGNED - BLB	REVISED - PER MCDOT 12/31/10	MCHENRY COUNTY DIVISION OF TRANSPORTATION GRAF ROAD BRIDGE OVER LAWRENCE CREEK	WYOMING 2-TUBE BRIDGE RAILING DETAILS STRUCTURE NO. 056-3182	TR	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		CHECKED - BAB	REVISED -			0037	06-00322-00-BR	MCHENRY	54	35	
PLOT SCALE =		DRAWN - BCD	REVISED -			C-91-358-06 CONTRACT NO. 63569					
PLOT DATE =		CHECKED - BLB	REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT BROS-00016831					