

# CONSTRUCTION PLANS

## FOR

# EDGAR COUNTY AIRPORT

### PARIS, EDGAR COUNTY, ILLINOIS

## CONSTRUCT CROSSWIND RUNWAY 18-36

**SCOPE OF WORK**

THIS PROJECT SHALL CONSIST OF THE CONSTRUCTION OF A 3,200' X 75' RUNWAY 18-36. ASSOCIATED WORK INCLUDES; INSTALLATION OF ELECTRICAL VAULT, LIGHTING, DRAINAGE, PAVEMENT MARKING, CLEARING & GRUBBING, SEEDING AND MULCHING.



Hanson Professional Services Inc.

CIVIL ENGINEER

Submitted by: *Charles A. Hagloch* ENG'R  
 Date Submitted: 03/10/11  
 Lics. Exp. Date: 11/30/11

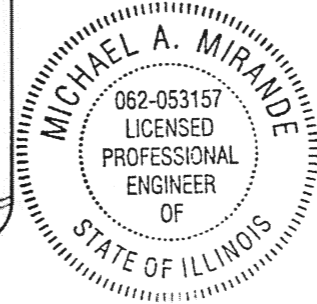


Hanson Professional Services Inc.

ELECTRICAL ENGINEER

SHEET NOS. 33 THRU 41  
 57 THRU 61, & 63

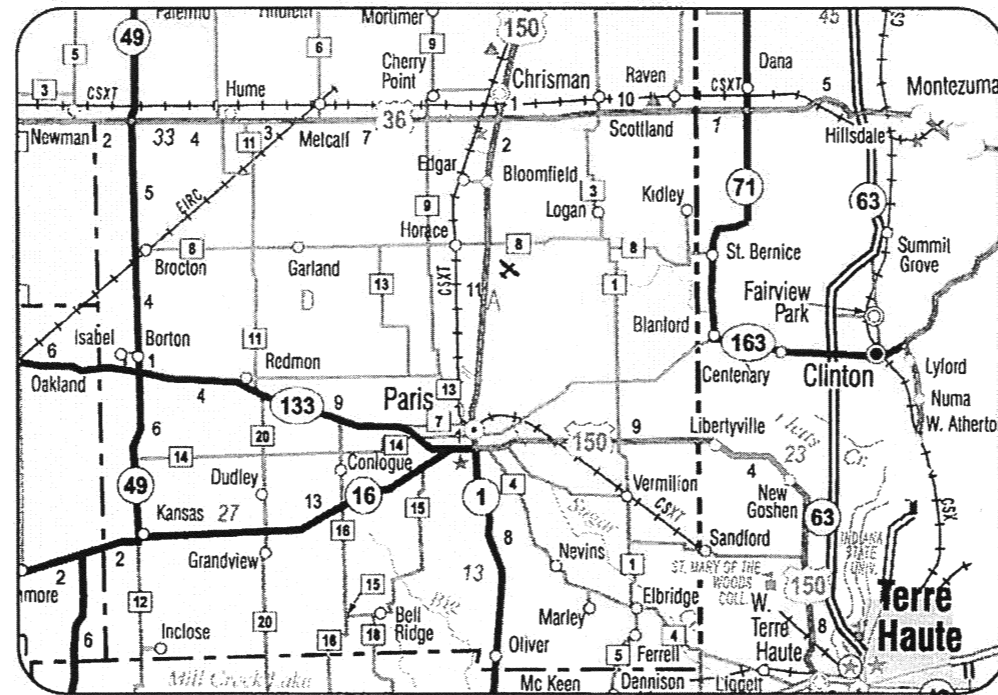
Submitted by: *Kevin N. Lightfoot* ENG'R  
 Date Submitted: 3/10/2011  
 Lics. Exp. Date: 11/30/2011



ELECTRICAL ENGINEER

SHEET NOS. 42 THRU 56

Submitted by: *Michael A. Mirande* ENG'R  
 Date Submitted: 03/10/11  
 Lics. Exp. Date: 11/30/11

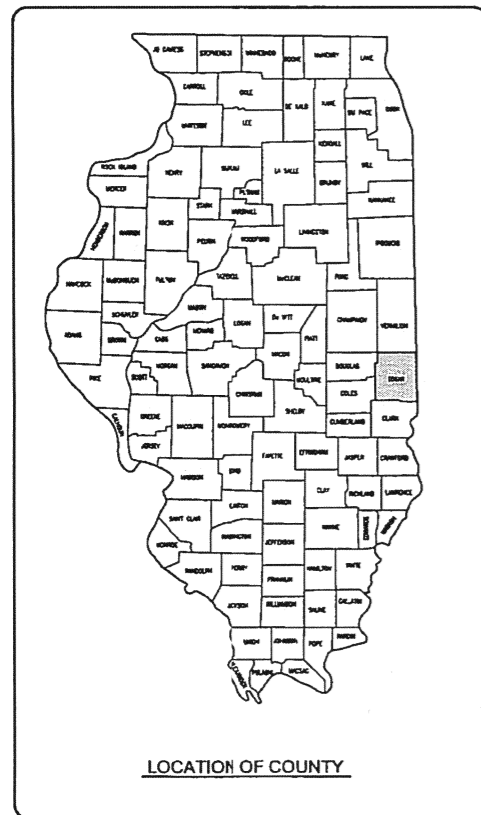


### LOCATION

ILL. PROJ.: PRG-4018  
 A.I.P. PROJ.: 3-17-0077-B13  
 LATITUDE: 39° 42' 00.6"  
 LONGITUDE: 87° 40' 14.4"  
 ELEVATION: 654.0' M.S.L.  
 DATE: MARCH 10, 2011

**EDGAR COUNTY BOARD**

Approved: *James Kehr* CHAIRMAN  
 Date: 2-17-11  
 Approved: *Augusta Hoff* SECRETARY  
 Date: 2/17/11



DATE	REVISION

EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

LAYOUT	BAK	07/10/11
DRAWN	BAK	01/10/11
REVIEWED	CAH	02/04/11

Hanson Professional Services Inc.  
 1525 South Sixth Street  
 Springfield, Illinois 62703-2886  
 www.hanson-inc.com  
 Offices Nationwide

CONSTRUCT CROSSWIND  
RUNWAY 18-36


COVER SHEET

MAR 10, 2011 10:18 AM HARR01115 C:\PWISE\_WORK\DO\_NOT\_DELETE\MS180631P-001\CVR\_1.DWG

INDEX TO SHEETS

SHEET NO.	DESCRIPTION	SHEET NO.	DESCRIPTION
1	COVER SHEET	69	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 96+50 TO STA. 97+00
2	INDEX TO SHEETS	70	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 97+50 TO STA. 98+00
3	SUMMARY OF QUANTITIES	71	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 98+50 TO STA. 99+00
4	PROPOSED SAFETY PLAN	72	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 99+50 TO STA. 100+00
5	PROPOSED STORMWATER POLLUTION PREVENTION PLAN	73	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 100+50 TO STA. 101+00
6	PROPOSED STAGING PLAN	74	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 101+50 TO STA. 102+00
7	PROPOSED CONSTRUCTION PLAN STA. 100+00 TO STA. 105+00	75	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 102+50 TO STA. 103+00
8	PROPOSED CONSTRUCTION PLAN STA. 105+00 TO STA. 115+00	76	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 103+50 TO STA. 104+00
9	PROPOSED CONSTRUCTION PLAN STA. 115+00 TO STA. 119+00	77	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 104+50 TO STA. 105+00
10	PROPOSED CONSTRUCTION PLAN STA. 119+00 TO STA. 123+50	78	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 105+50 TO STA. 106+00
11	PROPOSED CONSTRUCTION PLAN STA. 123+50 TO STA. 132+00	79	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 106+50 TO STA. 107+00
12	RWY 18-36 PLAN AND PROFILE - STA. 96+00.00 TO STA. 109+50.00	80	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 107+50 TO STA. 108+00
13	RWY 18-36 PLAN AND PROFILE - STA. 109+50.00 TO STA. 123+00.00	81	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 108+50 TO STA. 109+00
14	RWY 18-36 PLAN AND PROFILE - STA. 123+00.00 TO STA. 136+00.00	82	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 109+50 TO STA. 110+00
15	PROPOSED STAKING PLAN STA. 97+25 TO STA. 107+00	83	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 111+50 TO STA. 112+00
16	PROPOSED STAKING PLAN STA. 107+00 TO STA. 115+00	84	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 112+50
17	PROPOSED STAKING PLAN STA. 115+00 TO STA. 126+00	85	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 113+00
18	PROPOSED STAKING PLAN STA. 126+00 TO STA. 137+00	86	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 113+50
19	N-S RUNWAY AND TAXIWAY B INTERSECTION STAKING PLAN	87	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 114+00
20	N-S RWY. & TXY. B INTERSECTION STA. 115+28.59 - STA. 115+83.61	88	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 114+50
21	N-S RWY. & TXY. B INTERSECTION STA. 116+01.03 - STA. 116+18.94	89	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 115+00
22	N-S RWY. & TXY. B INTERSECTION STA. 116+73.91	90	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 115+25
23	N-S RUNWAY AND E-W RUNWAY INTERSECTION STAKING PLAN	91	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 115+50
24	N-S RWY. & E-W RWY. INTERSECTION STA. 119+08.30 - STA. 119+63.36	92	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 116+50
25	N-S RWY. & E-W RWY. INTERSECTION STA. 120+01.04 - STA. 120+38.34	93	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 117+00
26	N-S RWY. & E-W RWY. INTERSECTION STA. 120+93.34	94	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 117+50
27	PROPOSED DRAINAGE PLAN STA. 100+00 TO STA. 105+00	95	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 118+00
28	PROPOSED DRAINAGE PLAN STA. 105+00 TO STA. 115+00	96	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 118+50
29	PROPOSED DRAINAGE PLAN STA. 115+00 TO STA. 119+00	97	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 119+00
30	PROPOSED DRAINAGE PLAN STA. 119+00 TO STA. 123+00	98	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 121+00
31	PROPOSED DRAINAGE PLAN STA. 123+00 TO STA. 132+00	99	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 121+50
32	PROPOSED CLEARING PLAN	100	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 122+00
33	PROPOSED TERMINAL AREA ELECTRICAL PLAN	101	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 122+50
34	PROPOSED ELECTRICAL PLAN STA. 98+50 TO STA. 113+00	102	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 123+00
35	PROPOSED ELECTRICAL PLAN STA. 113+00 TO STA. 123+50	103	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 123+50
36	PROPOSED ELECTRICAL PLAN STA. 123+50 TO STA. 134+00	104	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 124+00
37	ELECTRICAL DETAILS SHEET 1	105	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 124+50
38	ELECTRICAL DETAILS SHEET 2	106	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 125+00
39	ELECTRICAL DETAILS SHEET 3	107	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 125+50
40	EXISTING VAULT AREA ELECTRICAL PLAN	108	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 126+00 TO STA. 126+50
41	PROPOSED VAULT AREA ELECTRICAL PLAN	109	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 127+00 TO STA. 127+50
42	ELECTRICAL LEGEND AND ABBREVIATIONS	110	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 128+00 TO STA. 128+50
43	PROPOSED AIRPORT VAULT EQUIPMENT PLAN	111	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 129+00 TO STA. 129+50
44	PROPOSED AIRPORT VAULT LIGHTING AND RECEPTACLE PLAN	112	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 130+00 TO STA. 130+50
45	PROPOSED AIRPORT VAULT WALL ELEVATIONS (SHEET 1)	113	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 131+00 TO STA. 131+50
46	PROPOSED AIRPORT VAULT WALL ELEVATIONS (SHEET 2)	114	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 132+00 TO STA. 132+50
47	LIGHTING CONTACTOR PANEL DETAILS	115	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 133+00 TO STA. 133+50
48	GROUNDING DETAILS	116	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 134+00 TO STA. 134+50
49	PROPOSED AIRPORT VAULT ONE-LINE DIAGRAM (SHEET 1)	117	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 135+00 TO STA. 135+50
50	PROPOSED AIRPORT VAULT ONE-LINE DIAGRAM (SHEET 2)	118	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 136+00
51	LIGHTING CONTACTOR SCHEMATIC	119	CROSS-SECTIONS FOR RUNWAY 18-36 STA. 136+50
52	AIRFIELD LIGHTING CONTROL WIRING SCHEMATIC	120	CROSS-SECTIONS FOR SOUTH TURNAROUND STA. 2+00 TO STA. 2+18
53	HIGH VOLTAGE WIRING SCHEMATIC	121	CROSS-SECTIONS FOR SOUTH TURNAROUND STA. 2+50 TO STA. 3+00
54	LEGEND PLATE SCHEDULES	122	CROSS-SECTIONS FOR SOUTH TURNAROUND STA. 3+25 TO STA. 3+50
55	VAULT GROUND BUS RISER DIAGRAM	123	CROSS-SECTIONS FOR SOUTH TURNAROUND STA. 4+00 TO STA. 4+62
56	GROUNDING NOTES	124	CROSS-SECTIONS FOR TAXIWAY B STA. 406+00
57	L-807 WIND CONE DETAIL	125	CROSS-SECTIONS FOR TAXIWAY B STA. 406+50
58	LIGHTNING PROTECTION DETAILS FOR BEACON	126	CROSS-SECTIONS FOR TAXIWAY B STA. 407+00
59	4'x4' ELECTRICAL MANHOLE	127	CROSS-SECTIONS FOR TAXIWAY B STA. 407+50
60	ELECTRICAL NOTES SHEET 1	128	CROSS-SECTIONS FOR TAXIWAY B STA. 408+00
61	ELECTRICAL NOTES SHEET 2	129	CROSS-SECTIONS FOR TAXIWAY B STA. 410+50
62	PROPOSED PLASI RUNWAY END 36 AND RUNWAY END 18	130	CROSS-SECTIONS FOR TAXIWAY B STA. 411+00
63	RUNWAY 9-27 PLASI WIRING DETAILS AND NOTES	131	CROSS-SECTIONS FOR TAXIWAY B STA. 411+50
64	PROPOSED MARKING PLAN STA. 98+50 TO STA. 113+00	132	CROSS-SECTIONS FOR TAXIWAY B STA. 412+00
65	PROPOSED MARKING PLAN STA. 113+00 TO STA. 123+50	133	CROSS-SECTIONS FOR TAXIWAY B STA. 412+50
66	PROPOSED MARKING PLAN STA. 123+50 TO STA. 134+00	134	CROSS-SECTIONS FOR NORTH TURNAROUND STA. 11+00 TO STA. 11+18
67	N-S RUNWAY EARTHWORK	135	CROSS-SECTIONS FOR NORTH TURNAROUND STA. 11+50 TO STA. 12+00
68	TAXIWAY B AND TURNAROUND EARTHWORK	136	CROSS-SECTIONS FOR NORTH TURNAROUND STA. 12+25 TO STA. 12+50
		137	CROSS SECTIONS FOR NORTH TURNAROUND STA. 13+00 TO STA. 13+62

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REVISION	
DATE	
<b>EDGAR COUNTY AIRPORT</b> <b>PARIS, ILLINOIS</b>	
ILL. PROJ.: PRG-4018 A.I.P. PROJ.: 3-17-0077-B13	
Hanson Proj. No. 10A0053 Filename R-002LP.DWG Scale NTO TO SCALE Date 03/10/11	LAYOUT BAK 01/11/11 DRAWN BAK 01/11/11 REVIEWED xxx xx/xx/xx
 <b>HANSON</b> Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62703-2886 www.hanson-inc.com Offices Nationwide	
<b>CONSTRUCT CROSSWIND</b> <b>RUNWAY 18-36</b> <b>INDEX TO SHEETS</b>	
<span style="font-size: 2em; font-weight: bold;">2</span> 2 of 137 sheets	

SUMMARY OF BASE BID QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITIES	AS BUILT QUANTITIES
AR108084	1/C #4 XLP-USE	L.F.	2,800	
AR108086	1/C #6 XLP-USE	L.F.	2,520	
AR108108	1/C #8 5 KV UG CABLE	L.F.	1,872	
AR108158	1/C #8 5KV UG CABLE IN UD	L.F.	11,162	
AR108554	2/C #4 600V UG CABLE IN UD	L.F.	302	
AR108654	3/C #4 600V UG CABLE IN UD	L.F.	3,381	
AR108656	3/C #6 600V UG CABLE IN UD	L.F.	2,041	
AR108812	12 PAIR CONTROL CABLE	L.F.	900	
AR109110	ERECT PREFABRICATED VAULT	L.S.	1	
AR109200	INSTALL ELECTRICAL EQUIPMENT	L.S.	1	
AR109901	REMOVE ELECTRICAL VAULT	L.S.	1	
AR110014	4" DIRECTIONAL BORE	L.F.	517	
AR110502	2-WAY CONCRETE ENCASED DUCT	L.F.	430	
AR110504	4-WAY CONCRETE ENCASED DUCT	L.F.	682	
AR110610	ELECTRICAL HANDHOLE	EACH	6	
AR110710	ELECTRICAL MANHOLE	EACH	6	
AR125410	MIL-STAKE MOUNTED	EACH	30	
AR125442	TAXI GUIDANCE SIGN, 2 CHARACTER	EACH	2	
AR125444	TAXI GUIDANCE SIGN, 4 CHARACTER	EACH	2	
AR125445	TAXI GUIDANCE SIGN, 5 CHARACTER	EACH	4	
AR125446	TAXI GUIDANCE SIGN, 6 CHARACTER	EACH	2	
AR125505	MIRL, STAKE MOUNTED	EACH	24	
AR125510	MIRL, BASE MOUNTED	EACH	8	
AR125540	MI THRESHOLD LIGHT STAKE MTD	EACH	16	
AR125565	SPLICE CAN	EACH	6	
AR125630	PLASI	EACH	2	
AR125961	RELOCATE STAKE MOUNTED LIGHT	EACH	2	
AR125989	REFURBISH PLASI	EACH	2	
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1	
AR150530	TRAFFIC MAINTENANCE	L.S.	1	
AR150540	HAUL ROUTE	L.S.	1	
AR151450	CLEARING AND GRUBBING	ACRE	27.3	
AR152410	UNCLASSIFIED EXCAVATION	C.Y.	29,797	
AR155540	BY-PRODUCT LIME	TON	1,120	
AR155616	SOIL PROCESSING - 16"	S.Y.	31,115	
AR156510	SILT FENCE	L.F.	2,625	
AR156511	DITCH CHECK	EACH	14	
AR156521	HEADWALL PROTECTION	EACH	6	
AR156530	TEMPORARY SEEDING	ACRE	43.7	
AR156531	EROSION CONTROL BLANKET	S.Y.	12,241	
AR156544	RIPRAP - GRADATION NO. 4	S.Y.	911	
AR162900	REMOVE CLASS E FENCE	L.F.	79	
AR209510	CRUSHED AGGREGATE BASE COURSE	TON	10,884	
AR401614	BIT. SURF. CSE. - METHOD II, SUPERPAVE	TON	2,958	
AR401630	BITUMINOUS SURFACE TEST SECTION	EACH	1	
AR401650	BITUMINOUS PAVEMENT MILLING	S.Y.	2,272	
AR401910	REMOVE & REPLACE BIT. PAVEMENT	S.Y.	367	
AR403614	BIT. BASE CSE. - METHOD II, SUPERPAVE	TON	4,904	
AR403630	BITUMINOUS BASE TEST SECTION	EACH	1	
AR602510	BITUMINOUS PRIME COAT	GAL.	10,884	
AR603510	BITUMINOUS TACK COAT	GAL.	5,609	
AR620520	PAVEMENT MARKING - WATERBORNE	S.F.	20,734	
AR620525	PAVEMENT MARKING - BLACK BORDER	S.F.	7,688	
AR701004	4" PVC STORM SEWER	L.F.	100	
AR701008	8" PVC STORM SEWER	L.F.	100	
AR701010	10" PVC STORM SEWER	L.F.	100	
AR701515	15" RCP, CLASS IV	L.F.	235	
AR701530	30" RCP, CLASS IV	L.F.	188	
AR701536	36" RCP, CLASS IV	L.F.	186	
AR701542	42" RCP, CLASS IV	L.F.	552	
AR705410	POROUS BACKFILL	C.Y.	370	
AR705526	6" PERFORATED UNDERDRAIN W/ SOCK	L.F.	7,198	
AR705630	UNDERDRAIN INSPECTION HOLE	EACH	24	
AR705900	REMOVE UNDERDRAIN	L.F.	1,009	
AR705903	REMOVE UNDERDRAIN INSP. HOLE	EACH	4	
AR752415	PRECAST REINFORCED CONCRETE FES 15"	EACH	4	
AR752430	PRECAST REINFORCED CONCRETE FES 30"	EACH	1	
AR752442	PRECAST REINFORCED CONCRETE FES 42"	EACH	1	
AR752630	CONCRETE HEADWALL 30"	EACH	1	
AR752636	CONCRETE HEADWALL 36"	EACH	2	
AR752642	CONCRETE HEADWALL 42"	EACH	1	
AR800543	FIELD TILE REPAIR	EACH	5	
AR901510	SEEDING	ACRE	43.7	
AR908510	MULCHING	ACRE	43.7	


SUMMARY OF QUANTITIES ADDITIVE ALTERNATE NO. 1

ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITIES	AS BUILT QUANTITIES
AS101580	REFURBISH 36" BEACON	L.S.	1	
AS800591	UPGRADE AIRPORT ROTATING BEACON	L.S.	1	

SUMMARY OF QUANTITIES ADDITIVE ALTERNATE NO. 2

ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITIES	AS BUILT QUANTITIES
AT107712	L-807 WIND CONE - 12' LIGHTED	EACH	1	
AT108656	3/C #6 600V UG CABLE IN UD	L.F.	250	

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REVISION					
DATE					
<b>EDGAR COUNTY AIRPORT PARIS, ILLINOIS</b>					
I.L. PROJ.: PRG-4018 A.I.P. PROJ.: 3-17-0077-B13					
Hanson Proj. No. 10A0053	Filename R-002LP.DWG	Scale NTO TO SCALE	Date 03/10/11	LAYOUT	01/11/11
				DRAWN	BAK 01/11/11
				REVIEWED	CAH 02/14/11
 <b>HANSON</b> Professional Services Inc. 2011 1526 South Sixth Street Springfield, Illinois 62703-2886 www.hanson-inc.com Offices Nationwide					
<b>CONSTRUCT CROSSWIND RUNWAY 18-36</b>					
<b>SUMMARY OF QUANTITIES</b>					
<span style="font-size: 2em; font-weight: bold;">3</span>					
3 of 137 sheets					

**UTILITY NOTE**

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. **CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123.** CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

**HAUL ROUTE AND VEHICLE PARKING**

THE CONTRACTOR WILL USE THE DESIGNATED HAUL ROUTES AND PARKING AREAS AS SHOWN ON THIS SHEET. THE PROPOSED PARKING AREAS WILL BE AS SHOWN. THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN THE PROPOSED HAUL ROUTES AND PARKING AREAS THROUGHOUT THE COURSE OF THE PROJECT. ANY AREAS DAMAGED OUTSIDE OF THESE AREAS WILL BE REPAIRED BY THE CONTRACTOR AND AT THE CONTRACTOR'S OWN EXPENSE. AT THE CONCLUSION OF THE PROJECT THE CONTRACTOR WILL GRADE, FERTILIZE, SEED AND MULCH THE HAUL ROUTES AND PARKING AREAS AS NEEDED TO RESTORE IT TO ITS ORIGINAL STATE. RESTORATION OF THE PRIMARY HAUL ROUTE AND EQUIPMENT STORAGE AREA WILL BE CONSIDERED PART OF THE "HAUL ROUTE" PAY ITEM. RESTORATION OF THE SECONDARY HAUL ROUTE AND PARKING AREA WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

**CONTRACTOR RESPONSIBILITIES**

THE CONTRACTOR'S EQUIPMENT PARKING AND STORAGE AREAS WILL BE AS SHOWN ON THIS SHEET. THE CONTRACTOR'S EMPLOYEES WILL PARK THEIR VEHICLES IN THIS AREA. ONLY CONTRACTOR VEHICLES WILL BE ALLOWED OUTSIDE THIS AREA.

THE CONTRACTOR AND HIS EMPLOYEES WILL BE RESTRICTED TO THE WORK AREA AND ALL OTHER AREAS OF THE AIRPORT ARE "OFF LIMITS" TO THEM.

THE CONTRACTOR SHALL KEEP THE RUNWAY OPEN AS MUCH AS POSSIBLE AND MAINTAIN CONTINUOUS TAXIWAY ACCESS TO ALL HANGARS AND FUEL FACILITY.

ALL WORK PERFORMED SHALL BE DONE IN A ORDERLY AND EFFECTIVE MANNER TO MINIMIZE RUNWAY CLOSURE. SEE STAGING PLAN FOR SEQUENCING.

NO TRENCHES OR HOLES WILL REMAIN OPEN WITHIN THE AIRCRAFT OPERATION LINE WHILE EAST-WEST RUNWAY IS OPEN.

ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2E "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION."

**BARRICADES AND TRAFFIC CONES**

IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AND TRAFFIC CONES AS DIRECTED BY THE AIRPORT MANAGER. THE BARRICADES WILL BE EQUIPPED WITH RED FLASHING OR RED STEADY-BURN LIGHTS AND 20" SQUARE ORANGE FLAGS. THE BARRICADES, THEIR MAINTENANCE, PLACEMENT AND REMOVAL WILL BE CONSIDERED AS PART OF THE "TRAFFIC MAINTENANCE" PAY ITEM AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

**LEGEND**

- EXISTING IMPROVEMENTS
- PROPOSED IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED HAUL ROUTE AND EQUIPMENT PARKING AREA
- PROPOSED BENCHMARK
- PROPOSED BARRICADES

**SCOPE OF WORK**

THIS PROJECT SHALL CONSIST OF THE CONSTRUCTION OF A 3,200' X 75' RUNWAY 18-36. ASSOCIATED WORK INCLUDES; INSTALLATION OF AN ELECTRICAL VAULT, LIGHTING, DRAINAGE, PAVEMENT MARKING, CLEARING & GRUBBING, SEEDING, AND MULCHING.

**AIRPORT SECURITY NOTE**

AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR WILL CLOSE THE EXISTING GATE ON THE SECONDARY HAUL ROUTE AT THE END OF EACH WORKING DAY. THE CONTRACTOR WILL PROVIDE A CABLE AND 2 POSTS OR OTHER APPROVED METHOD TO PREVENT READY ACCESS TO THE HAUL ROUTE AND AIRFIELD PAVEMENTS.

**HEIGHT OF CONSTRUCTION EQUIPMENT**

THE MAXIMUM HEIGHT OF THE CONSTRUCTION EQUIPMENT WILL BE 70 FEET WHICH IS EXPECTED TO BE A CRANE TO REPLACE THE BEACON AND/OR TO SET THE VAULT SHELTER. THE CRANE SHALL BE USED DURING DAYLIGHT HOURS AND VFR CONDITIONS ONLY, AND SHALL BE LOWERED WHEN NOT IN USE, DURING HOURS BETWEEN SUNSET AND SUNRISE, AND/OR DURING IFR WEATHER CONDITIONS. THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT AT THE OTHER LOCATIONS WILL BE 25 FEET, WHICH IS EXPECTED TO BE A TRACTOR AND TRAILER FULLY EXTENDED OR A LINE TRUCK.

**MATERIAL CERTIFICATION**

COMPLETED WORK CANNOT BE PLACED ON A CONSTRUCTION REPORT UNTIL ALL MATERIAL CERTIFICATIONS FOR THAT PAY ITEM HAVE BEEN RECEIVED, REVIEWED AND ACCEPTED BY THE RESIDENT ENGINEER.

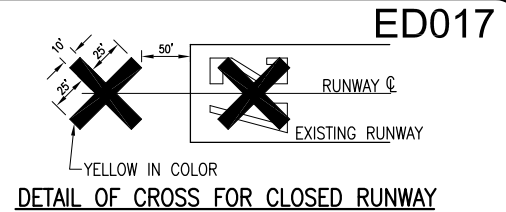
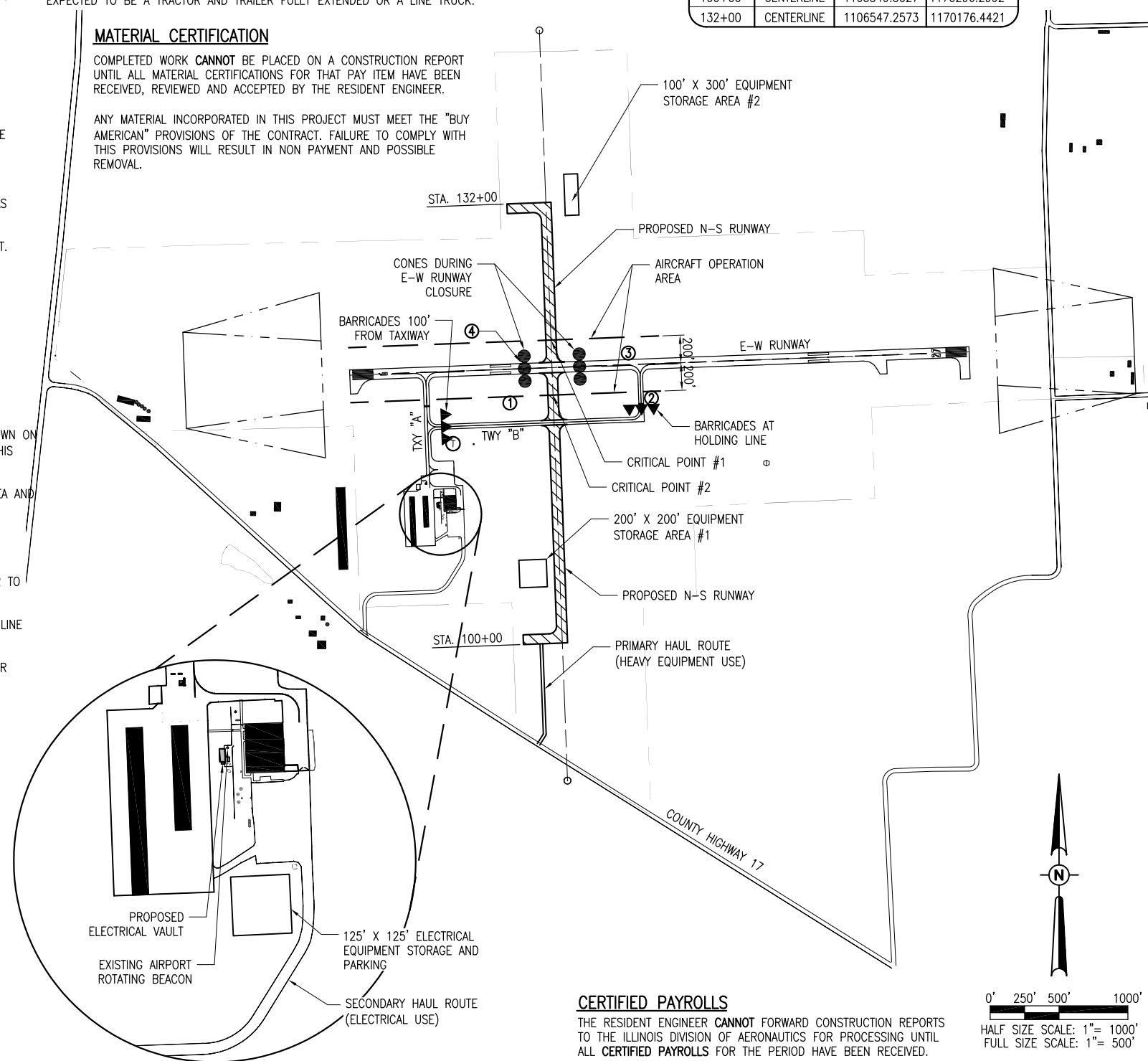
ANY MATERIAL INCORPORATED IN THIS PROJECT MUST MEET THE "BUY AMERICAN" PROVISIONS OF THE CONTRACT. FAILURE TO COMPLY WITH THIS PROVISIONS WILL RESULT IN NON PAYMENT AND POSSIBLE REMOVAL.

**BENCHMARK DATA**

NO.	DESCRIPTION	ELEV.
1	CHISLED SQUARE IN CENTER OF CONCRETE HEADWALL OF 24" RCCP CROSSING RWY. LOCATED ON THE SOUTH SIDE OF RUNWAY, NEAR MIDPOINT OF THE TAXIWAY.	647.61
2	CHISLED SQUARE IN CENTER OF EAST CONCRETE HEADWALL OF 18" RCCP CROSSING THE TAXIWAY. LOCATED AT THE EAST END OF TAXIWAY NEAR HOLDING LINE.	646.69
3	CHISLED SQUARE IN CENTER OF NORTH SIDE OF CONCRETE BASE MOUNTED RUNWAY LIGHT, TAG RI-35. FIRST LIGHT WEST OF THE EAST END OF RWY/TWY INTERSECTION.	648.91
4	CHISLED SQUARE IN CENTER OF NORTH SIDE OF CONCRETE BASE MOUNTED RUNWAY LIGHT, TAG RI-39. NORTH SIDE OF RUNWAY, ±1300 EAST OF WEST END OF RUNWAY.	650.19

**BASELINE COORDINATE TABLE**

STATION	OFFSET	NORTHING	EASTING
100+00	CENTERLINE	1103349.5027	1170296.2992
132+00	CENTERLINE	1106547.2573	1170176.4421



**NOTE:** COST OF CONSTRUCTING, PLACING, MAINTAINING AND REMOVING CROSSES WILL BE CONSIDERED PART OF THE "TRAFFIC MAINTENANCE" PAY ITEM AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE CROSSES WILL BE YELLOW IN COLOR AND SHALL BE MADE OF A SUITABLE MATERIAL AS APPROVED BY THE AIRPORT MANAGER. THE CROSSES WILL BE PLACED OVER THE NUMERALS OR OFF THE END OF THE PROPOSED RUNWAY AND SECURED IN A MANNER APPROVED BY THE MANAGER. THE PROPOSED CROSSES WILL BE PLACED EACH DAY THE RUNWAY IS CLOSED AND REMOVED WHEN THE RUNWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

**J.U.L.I.E. INFORMATION**  
 COUNTY \_\_\_\_\_ EDGAR  
 CITY \_\_\_\_\_ PARIS  
 TOWNSHIP \_\_\_\_\_ EDGAR  
 SECTION NO. \_\_\_\_\_ 5 & 6  
 ADDRESS \_\_\_\_\_ EDGAR COUNTY AIRPORT  
 R.R. #6, BOX 175A  
 PARIS, ILLINOIS 61944

**CRITICAL POINT #1 DATA**  
 LATITUDE: 39° 42' 02.32"  
 LONGITUDE: 87° 40' 20.73"  
 ELEVATION: 647.41 M.S.L.

**CRITICAL POINT #2 DATA**  
 LATITUDE: 39° 41' 58.37"  
 LONGITUDE: 87° 40' 20.58"  
 ELEVATION: 648.35 M.S.L.

**PROPOSED SAFETY PLAN**

GENERAL - THE EDGAR COUNTY AIRPORT IS COMPRISED OF ONE RUNWAY. THE PROPOSED CONSTRUCTION WILL NECESSITATE CLOSING THE RUNWAY. ANY TIME THE CONTRACTOR IS WORKING WITHIN 200' OF THE RUNWAY CENTERLINE THE RUNWAY WILL BE CLOSED. TAXIWAY "B" WILL BE CLOSED THROUGHOUT MOST OF THE PROJECT AND BARRICADED AS SHOWN. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO TURN OFF AND TAG THE E-W RUNWAY LIGHTING CIRCUIT. THE CONTRACTOR WILL PROVIDE AND INSTALL CROSSES OFF EACH END OF THE PROPOSED N-S RUNWAY ONCE THE AREA BEGINS TO LOOK LIKE A RUNWAY. CROSSES WILL BE IN PLACE AFTER THE LUNING OPERATION. ALL WORK INCLUDED IN OPENING AND CLOSING THE RUNWAY WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

IDENTIFICATION - WHEN THE CONTRACTORS VEHICLES AND EQUIPMENT ARE ON THE AIRPORT THEY SHALL BE PROPERLY MARKED WITH THREE (3') FOOT SQUARE CHECKERED FLAGS (INTERNATIONAL ORANGE AND WHITE). THE CRANE SHALL BE MARKED WITH A FLAG AS DESCRIBED ABOVE. THE CONTRACTOR WILL ALSO PROVIDE WORKERS WITH SOME TYPE OF TAG OR GARMENT TO IDENTIFY THE PERSON AS BEING PART OF THE CONSTRUCTION CREW.

RADIO CONTROL - THE CONTRACTOR WILL BE REQUIRED TO BE IN TWO-WAY RADIO CONTACT (123.00 MHZ.) WITH THE AIRPORT UNICOM. THIS WILL KEEP THE CONTRACTOR IN CONSTANT CONTACT WITH THE EDGAR COUNTY AIRPORT AND ENABLE THE AIRPORT TO IMMEDIATELY CONTACT THE CONTRACTOR IN CASE OF AN AERONAUTIC EMERGENCY THAT WOULD REQUIRE ACTION BY THE CONTRACTOR AND/OR HIS PERSONNEL.

**150-ENGINEER'S FIELD OFFICE NOTES**

THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE FURNISHED, MAINTAINED, AND REMOVED IN ACCORDANCE WITH ITEM AR150510 "ENGINEER'S FIELD OFFICE" AS STATED ON PAGE 49 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS NOV. 2, 2009.

THE LOCATION OF THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE DETERMINED AT THE PRE-CONSTRUCTION MEETING.

THE ENGINEERING FIRM WILL MAKE PAYMENT FOR ALL LONG DISTANCE TELEPHONE CALLS IN EXCESS OF ONE HUNDRED DOLLARS (\$100.00) PER MONTH.

THE CONTRACTOR WILL FURNISH A WIRELESS PHONE TO THE RESIDENT ENGINEER FOR HIS EXCLUSIVE USE FOR THE DURATION OF THIS PROJECT. THE RESIDENT ENGINEER WILL USE THIS PHONE FOR PROJECT BUSINESS ONLY. THE CONTRACTOR WILL BE RESPONSIBLE FOR ALL CHARGES ASSOCIATED WITH THIS CELL PHONE.

THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE PAID FOR UNDER ITEMS: AR150510 ENGINEER'S FIELD OFFICE \_\_\_\_ 1 L.S.

**EROSION CONTROL**

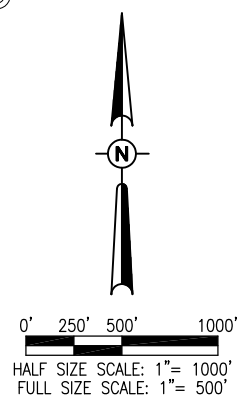
THIS PROJECT WILL DISTURB MORE THAN 1 ACRE OF LAND, THEREFORE A N.P.D.E.S. PERMIT WILL BE REQUIRED.

**AIRCRAFT OPERATION LINE**

THE CONTRACTOR WILL LOCATE THIS LINE AT THE START OF CONSTRUCTION AND WILL PLACE FLAGGED LATHE EVERY 150' ALONG IT. THIS LINE WILL BE THE LIMITS THAT ALL CONTRACTOR PERSONNEL MAY VENTURE WHEN A RUNWAY IS NOT CLOSED. THE CONTRACTOR WILL MAINTAIN THE LATHE LINE FOR RUNWAYS.

**CERTIFIED PAYROLLS**

THE RESIDENT ENGINEER CANNOT FORWARD CONSTRUCTION REPORTS TO THE ILLINOIS DIVISION OF AERONAUTICS FOR PROCESSING UNTIL ALL CERTIFIED PAYROLLS FOR THE PERIOD HAVE BEEN RECEIVED.



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REVISION									
DATE									
EDGAR COUNTY AIRPORT PARIS, ILLINOIS									
A.I.P. PROJ.: 3-17-0077-B13									
IL PROJ.: PRG-4018									
Hanson Prof. No. 10A0053 Filename R-0035FY.DWG Scale 1" = 500' Date 03/10/11									
 Hanson Professional Services Inc. 2011 1526 South Sixth Street Springfield, Illinois 62703-2886 www.hanson-inc.com Offices Nationwide									
LAYOUT BAK 01/11/11 DRAWN BAK 01/11/11 REVIEWED xx xx/xx/xx									
CONSTRUCT CROSSWIND RUNWAY 18-36 PROPOSED SAFETY PLAN									
4									
4 of 137 sheets									



**EROSION CONTROL NOTES**

ALL PROPOSED EROSION CONTROL MEASURES SHALL BE COMPLETED AS DETAILED ON THIS EROSION CONTROL PLAN AND IN ACCORDANCE WITH THE SPECIFICATIONS.

EROSION CONTROL MEASURES ARE COVERED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY'S STANDARDS AND SPECIFICATIONS FOR SOIL EROSIONS AND SPECIFICATION FOR SOIL EROSIONS AND SEDIMENT CONTROL.

FILTER FENCE FOR STRUCTURE PROTECTION AS SHOWN ON THIS SHEET.

FILTER FENCE IS INCLUDED AS PART OF HEADWALL PROTECTION PAY ITEM.

INSPECTION OF THE HEADWALL PROTECTION SHALL BE FREQUENT AND REPAIR/REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.

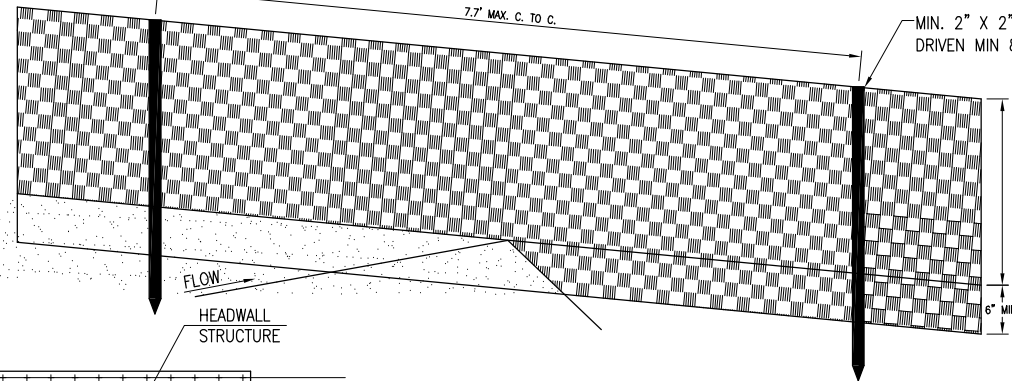
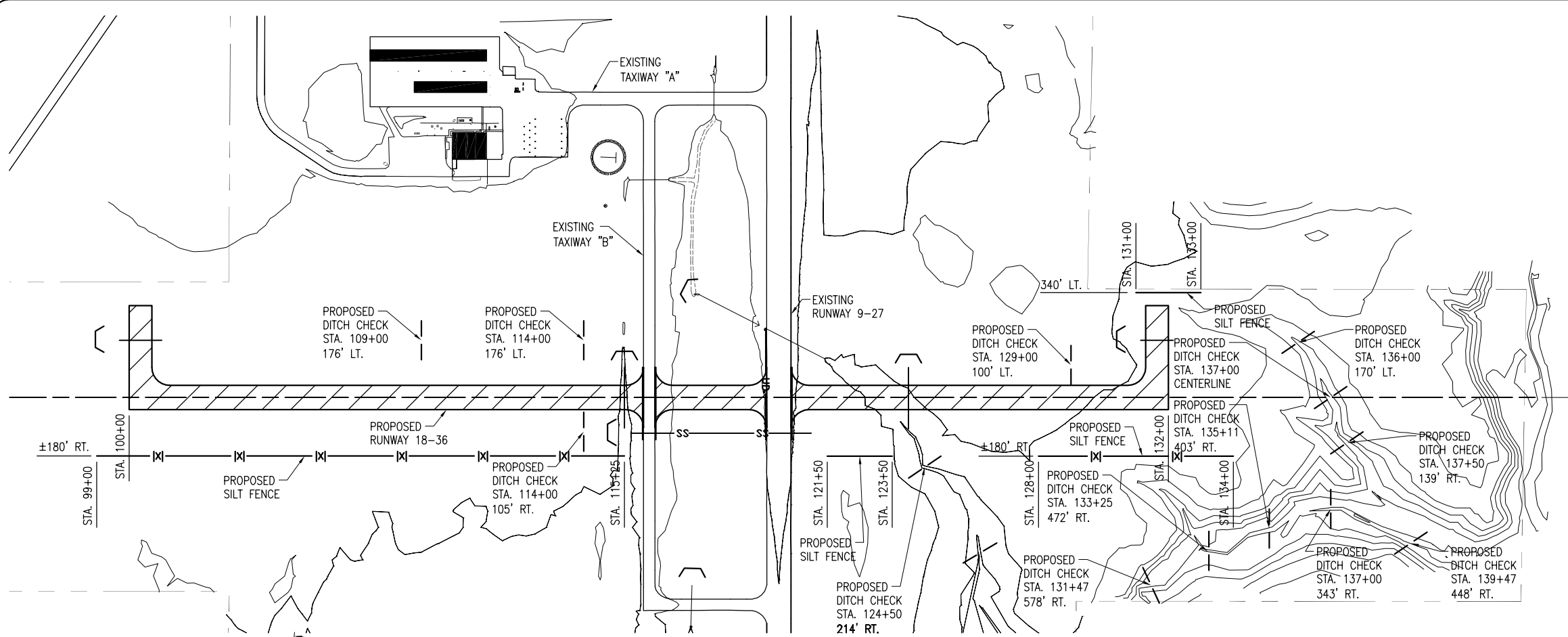
EROSION CONTROL FENCE, HEADWALL PROTECTION AND DITCH CHECKS SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFULNESS SO AS NOT TO BLOCK OR IMPEDE STORM WATER DRAINAGE AND GRASS HAS BEEN ESTABLISHED.

COST OF REMOVAL AND REPLACEMENT SHALL BE INCLUDED IN THE UNIT PRICE FOR ITEM:  
 AR156510 SILT FENCE  
 AR156511 DITCHCHECK  
 AR156521 HEADWALL PROTECTION

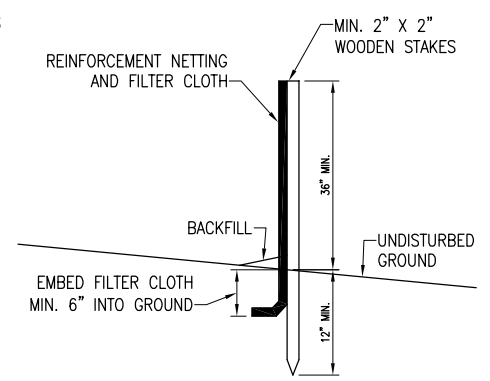
**ADDITIONAL EROSION CONTROL BLANKET NOTES**

AN EROSION CONTROL MATERIAL (KNITTED STRAW MAT) 8' IN WIDTH WILL BE INSTALLED ALONG EITHER SIDE OF THE PROPOSED DITCHES AT THE DISCRETION OF THE RESIDENT ENGINEER AS NEEDED. THE CONTRACTOR WILL COORDINATE WITH THE RESIDENT ENGINEER THE AREA AND QUANTITY TO BE COVERED PRIOR TO INSTALLATION. THE QUANTITY, APPROXIMATELY 4,978 S.Y. WAS BASED ON DITCHES EXCEEDING 3 FEET IN DEPTH. AN ADDITIONAL 3,555 S.Y. WAS ESTIMATED TO ASSIST IN EROSION AREAS THAT MAY OCCUR WITHIN THE CLEARING AND GRUBBING LIMITS.

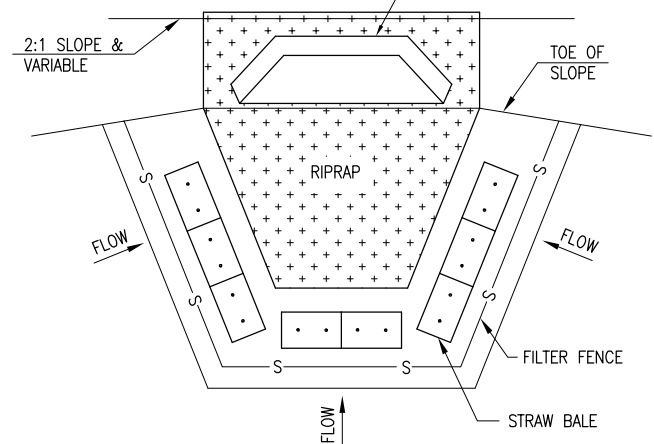
THIS ITEM WILL BE PAID FOR UNDER:  
 AR156531 "EROSION CONTROL BLANKET" \_\_\_\_\_ PER S.Y.



**PERSPECTIVE VIEW**

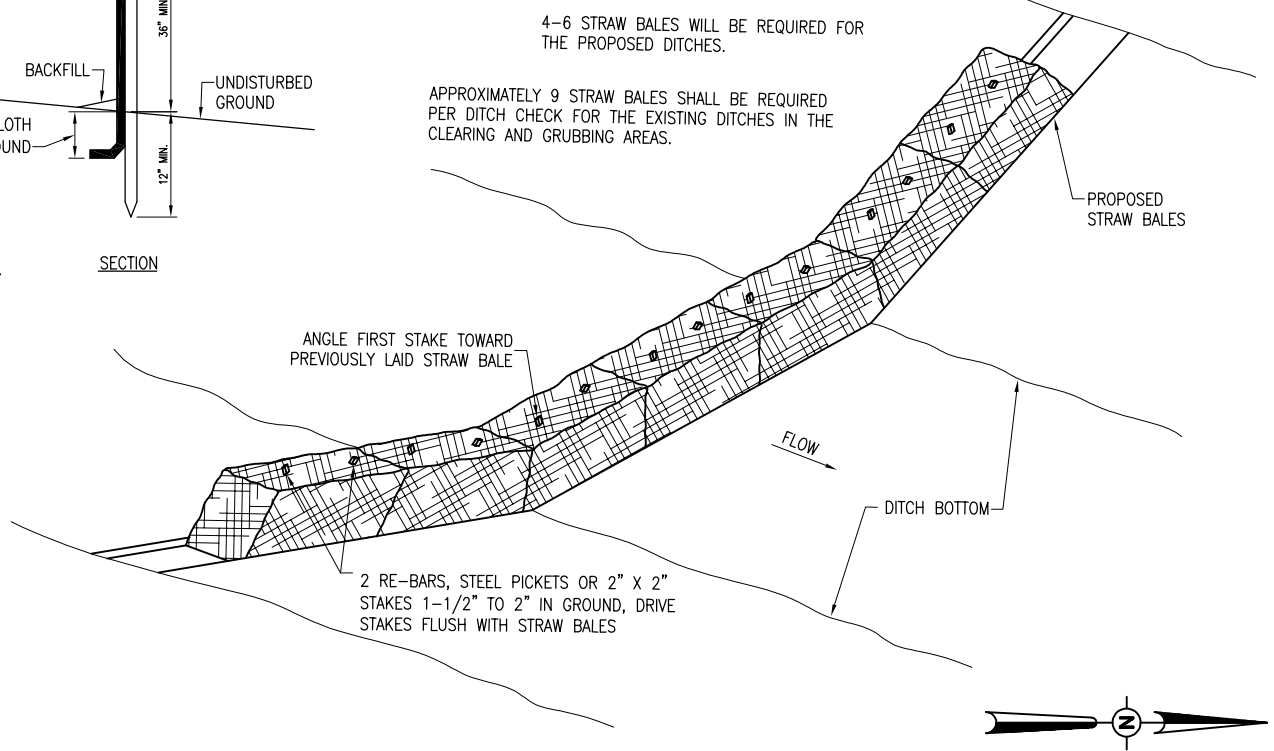


**FILTER FENCE DETAIL**  
 "NOT TO SCALE"



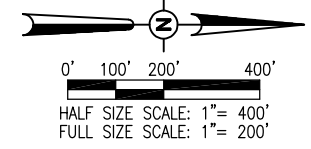
**HEADWALL STRUCTURE PROTECTION**  
 "NOT TO SCALE"

- LEGEND**
- EXISTING IMPROVEMENTS
  - PROPOSED IMPROVEMENTS
  - EXISTING PROPERTY LINE
  - PROPOSED SILT FENCE
  - PROPOSED DITCH CHECK
  - PROPOSED HEADWALL PROTECTION



**ANCHORING DETAIL**

**DITCH CHECK DETAIL**  
 "NOT TO SCALE"



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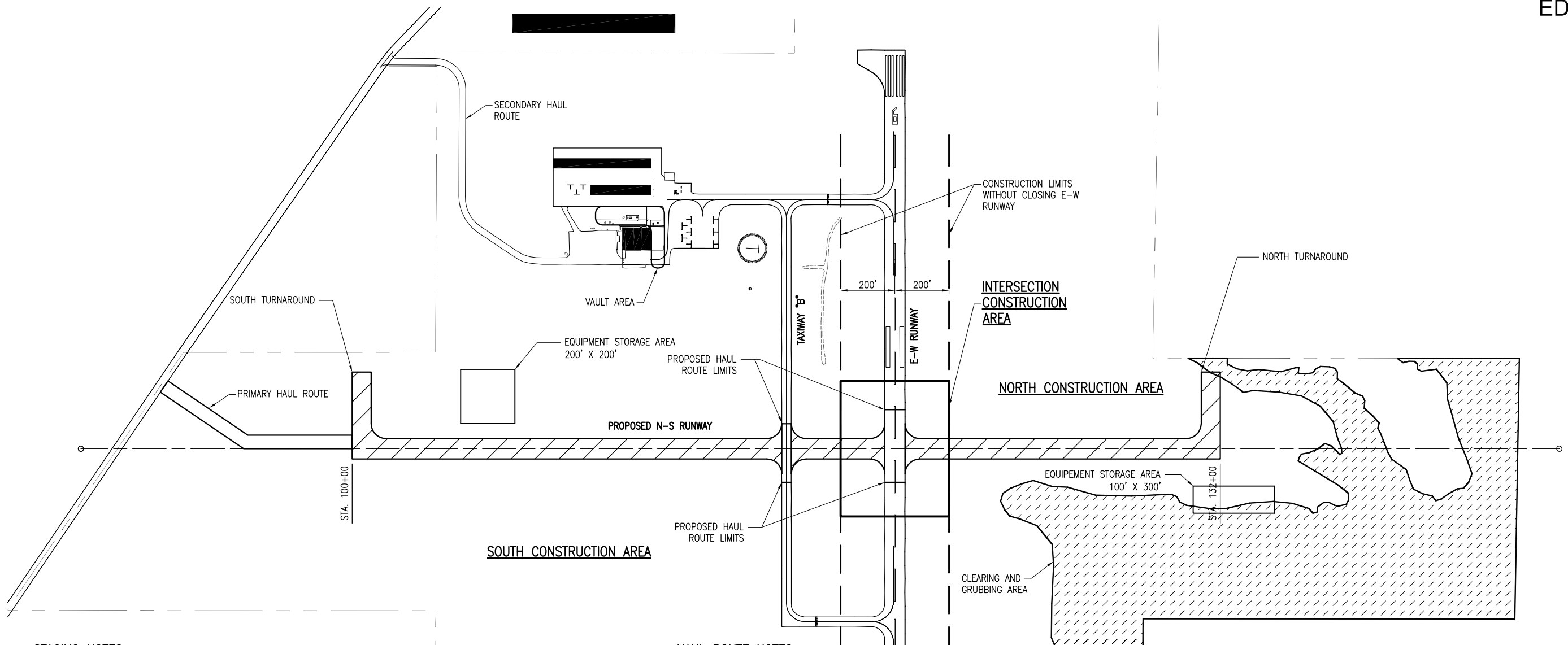
REVISION	DATE

EDGAR COUNTY AIRPORT  
 PARIS, ILLINOIS  
 IL PROJ.: PRG-4018  
 A.I.P. PROJ.: 3-17-0077-B13

Hanson Proj. No. 10A0053	01/11/11
Filename R-181SWP.DWG	BAK
Scale 1" = 200'	DRAWN
Date 03/10/11	BAK
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**CONSTRUCT CROSSWIND  
 RUNWAY 18-36**  
 PROPOSED STORMWATER  
 POLLUTION PREVENTION  
 PLAN



**STAGING NOTES**

THE INTENT OF THIS STAGING OR SEQUENCING PLAN IS TO ACT AS A PROCEDURAL GUIDE TO ACCOMPLISH THE WORK IN A TIMELY MATTER WHILE KEEPING THE E-W RUNWAY OPEN AND HENCE THE AIRPORT OPEN AS MUCH AS POSSIBLE. EVERY EFFORT SHOULD BE MADE TO KEEP THE E-W RUNWAY OPEN FOR OPERATIONS.

ELECTRICAL VAULT WORK AND CABLING/DUCTING OUT TO THE PROPOSED N-S RUNWAY CAN BE DONE WHILE THESE OTHER STAGES ARE OCCURRING. CLEARING AND GRUBBING CAN BE DONE WITHIN PERMITTED MONTHS OF THE YEAR ANY TIME PRIOR TO THE SEEDING OPERATION.

1. MOBILIZE, PLACE EROSION CONTROL, DELIVER MATERIALS, BEGIN STRIPPING TOPSOIL IN PAVEMENT AREAS.
2. CLOSE E-W RUNWAY AND TAXIWAY "B", SAWING AND REMOVING PAVEMENT FOR 42" PIPE CROSSING, BEGIN INSTALLING 30" OR 42" PIPE, BEGIN EARTHWORK IN NORTH CONSTRUCTION AREA. E-W RUNWAY MAY BE CLOSED DURING THIS WEEKEND.
3. FINISH INSTALLING 42" PIPE, REPLACE BITUMINOUS PAVEMENT, CUTTING DITCHES AND PLACING EARTH FILLS IN NORTH CONSTRUCTION AREA. REMOVE EDGEDRAIN, INSTALL ELECTRICAL DUCTS. COMPLETE EARTH FILLS 200' EITHER SIDE OF THE E-W RUNWAY. OPEN RUNWAY FOR WEEKEND. OPEN TAXIWAY "B".
4. COMPLETE EARTHWORK IN NORTH CONSTRUCTION AREA, INSTALL REMAINING PIPE AND HEADWALLS. E-W RUNWAY WILL REMAIN OPEN.
5. BEGIN EARTHWORK IN SOUTH CONSTRUCTION AREA. E-W RUNWAY WILL REMAIN OPEN.
6. EARTHWORK IN SOUTH CONSTRUCTION AREA. E-W RUNWAY WILL REMAIN OPEN.
7. LIME MODIFICATION. CLOSE TAXIWAY "B" CLOSE E-W RUNWAY WHILE WORKING WITHIN 200'. FLAGMAN FOR TRUCK TRAFFIC.
8. PLACING ROCK, BITUMINOUS PRIME. FLAGMAN FOR TRUCK TRAFFIC WHILE E-W RUNWAY OPEN.
9. INSTALL EDGEDRAIN. CLOSE E-W RUNWAY WHILE WORKING WITHIN 200'. MILLING E-W RUNWAY CLOSURES AFTER MILLING OPERATION.
10. CLOSE E-W RUNWAY FOR PAVING, SHOULDER DIRT. OPEN E-W RUNWAY ONCE BEYOND 200'
11. SHOULDER DIRT, LIGHTS AND CABLING PERIODIC CLOSURE OF E-W RUNWAY FOR INSTALLATION OF LIGHTS AND CABLING WITHIN 200'. COORDINATE MARKING WITH THIS OPERATION.
12. LIGHTING AND CABLING.
13. SEEDING, LIGHTING AND CABLING. PERIODIC CLOSURE OF E-W RUNWAY FOR SEEDING OPERATION.

**HAUL ROUTE NOTES**

THE PRIMARY HAUL ROUTE WILL BE USED FOR ALL HEAVY EQUIPMENT AND HEAVY MATERIAL DELIVERIES. THE SECONDARY HAUL ROUTE WILL BE USED FOR ELECTRICAL DELIVERIES TO THE VAULT AREA.

THE PRIMARY HAUL ROUTE WILL BE INSTALLED AS SHOWN ON THIS SHEET. IF REQUIRED, A CORRECTLY SIZED PIPE WILL BE PROVIDED IN THE DITCH. THE HAUL ROUTE WILL BE AN **ALL WEATHER** HAUL ROUTE FROM THE ROAD TO SA. 100+00 CAPABLE OF SUSTAINING THE CONTINUOUS HAULING OPERATIONS. THIS HAUL ROUTE WILL BE BUILT WITH MATERIAL SO AS NOT TO CONTAMINATE THE PREVIOUSLY INSTALLED CRUSHED AGGREGATE OR BITUMINOUS PAVEMENT FOR THE PROPOSED N-S RUNWAY.

THE EXISTING TAXIWAY B AND THE E-W RUNWAY WILL NOT SUPPORT REPETITIVE CONSTRUCTION TRAFFIC. IT WILL BE THE CONTRACTORS RESPONSIBILITY TO PROTECT THESE PAVEMENTS FROM RUTTING AND/OR EXCESSIVE WEAR DUE TO HAULING OPERATIONS. THE CONTRACTOR WILL PROVIDE IRON PLATES FOR ANY HEAVY AND/OR CONTINUOUS HAULING ACROSS TAXIWAY B. THE CONTRACTOR WILL STAGGER WITHIN THE "HAUL ROUTE LIMITS" HIS HAULING ACROSS TAXIWAY B AND THE E-W RUNWAY TO PREVENT RUTTING OR CONCENTRATED DAMAGE TO THESE PAVEMENTS. THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE TO THE EXISTING PAVEMENT. ANY PAVEMENT DAMAGE WILL BE REMOVED AND REPLACED AT HIS EXPENSE TO THE SATISFACTION OF THE AIRPORT AND RESIDENT ENGINEER..

**LEGEND**

- EXISTING IMPROVEMENTS
- PROPOSED IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED HAUL ROUTE AND EQUIPMENT PARKING AREA



0' 100' 200' 400'  
 HALF SIZE SCALE: 1"= 400'  
 FULL SIZE SCALE: 1"= 200'

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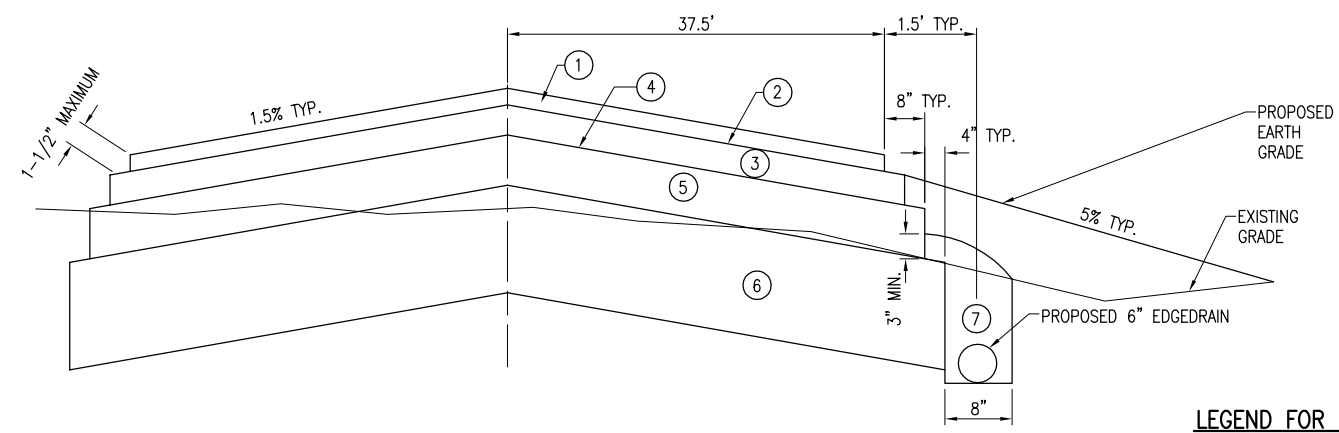
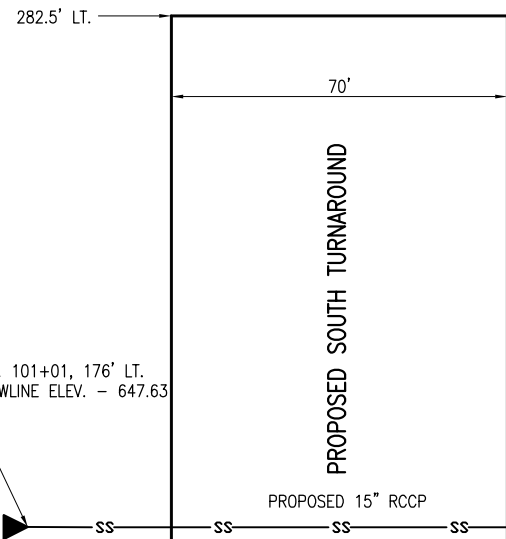
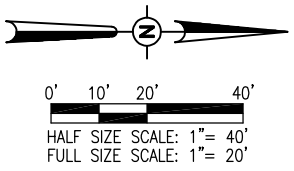
REVISION	DATE

EDGAR COUNTY AIRPORT  
 PARIS, ILLINOIS  
 A.I.P. PROJ.: 3-17-0077-B13  
 IL PROJ.: PRG-4018

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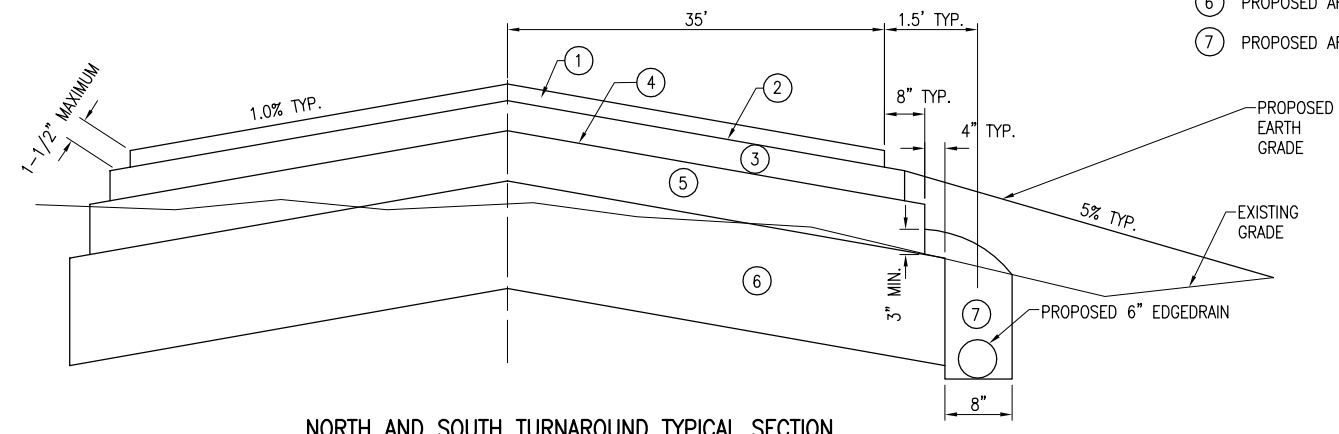
CONSTRUCT CROSSWIND  
 RUNWAY 18-36  
 PROPOSED STAGING PLAN



**N-S RUNWAY TYPICAL SECTION**

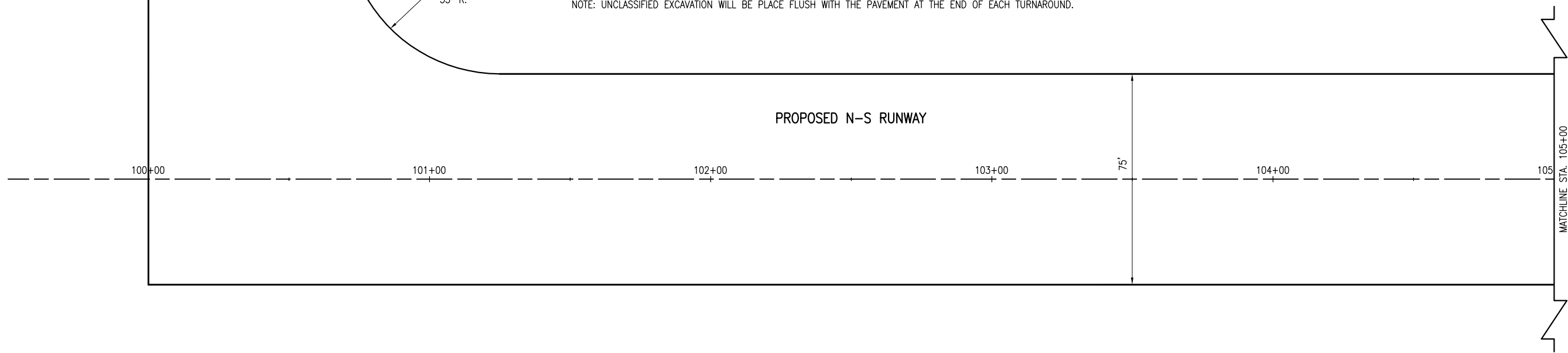
**LEGEND FOR N-S RUNWAY, NORTH AND SOUTH TURNAROUND TYPICAL SECTIONS**

- ① PROPOSED AR 401 - BITUMINOUS SURFACE COURSE (1-1/2" DEPTH)
- ② PROPOSED AR 603 - BITUMINOUS TACK COAT (0.05-0.15 GAL./S.Y.)
- ③ PROPOSED AR 403 - BITUMINOUS BASE COURSE (2-1/2" DEPTH)
- ④ PROPOSED AR 602 - BITUMINOUS PRIME COAT (0.35 GAL./S.Y.)
- ⑤ PROPOSED AR 209 - CRUSHED AGGREGATE BASE COURSE 6" DEPTH)
- ⑥ PROPOSED AR 152 - BY-PRODUCT LIME MODIFICATION (16" DEPTH)
- ⑦ PROPOSED AR 705 - POROUS BACKFILL



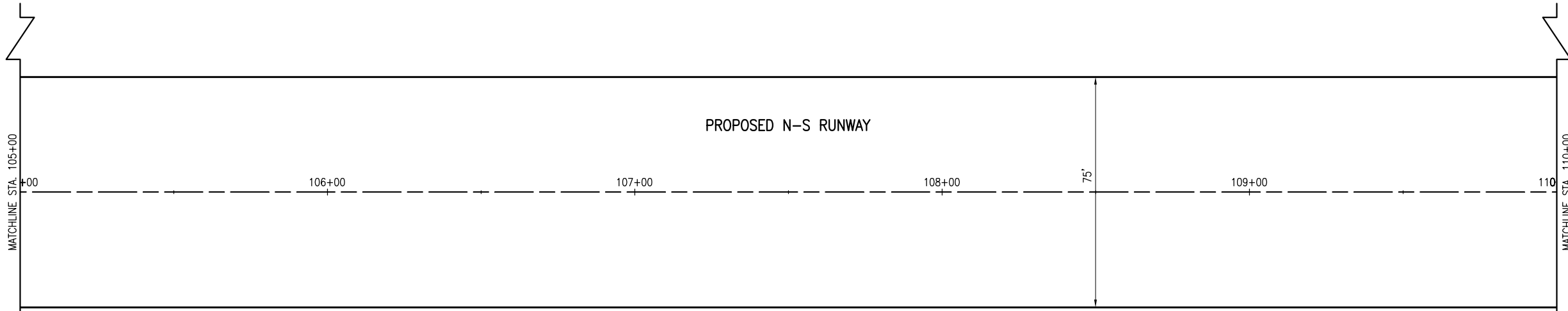
**NORTH AND SOUTH TURNAROUND TYPICAL SECTION**

NOTE: UNCLASSIFIED EXCAVATION WILL BE PLACE FLUSH WITH THE PAVEMENT AT THE END OF EACH TURNAROUND.



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REVISION		<b>EDGAR COUNTY AIRPORT PARIS, ILLINOIS</b>	A.I.P. PROJ.: 3-17-0077-B13 IL PROJ.: PRG-4018
DATE			
Hanson Proj. No. 10A0053 Filename R-12\CON_1.DWG Scale 1" = 20' Date 03/10/11		LAYOUT BAK 01/04/11 DRAWN BAK 01/04/11 REVIEWED CAH xx/xx/xx	 Hanson Professional Services Inc. 2011 1525 South Sixth Street Springfield, Illinois 62703-2886 www.hanson-inc.com Offices Nationwide
<b>CONSTRUCT CROSSWIND RUNWAY 18-36</b>		PROPOSED CONSTRUCTION PLAN STA. 100+00 TO STA. 105+00	
7		7 of 137 sheets	



**603-BITUMINOUS TACK COAT NOTES:**

THE BITUMINOUS TACK COAT (603) SHALL BE PLACED IN ACCORDANCE WITH ITEM AR603 "BITUMINOUS TACK COAT" AS STATED ON PAGE 254 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 2, 2009.

THE PROPOSED BITUMINOUS TACK COAT SHALL BE PLACED ON THE PROPOSED BITUMINOUS PAVEMENT PRIOR TO THE PLACEMENT OF THE NEXT LIFT OF PROPOSED BITUMINOUS SURFACE COURSE. THE PROPOSED BITUMINOUS PAVEMENT SHALL HAVE A TACK COAT OF BITUMINOUS MATERIAL APPLIED IN ACCORDANCE WITH THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS. FOR ESTIMATION PURPOSES 0.15 GAL./S.Y. WAS USED.

THE PROPOSED BITUMINOUS TACK COAT WILL BE PAID FOR UNDER ITEM:  
AR603510 BITUMINOUS TACK COAT \_\_\_\_\_ PER GAL.

**602-BITUMINOUS PRIME COAT NOTES:**

THE BITUMINOUS PRIME COAT (602) SHALL BE PLACED IN ACCORDANCE WITH ITEM AR602 "BITUMINOUS PRIME COAT" AS STATED ON PAGE 251 OF THE STANDARD SPECIFICATION FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 2, 2009.

THE PROPOSED BITUMINOUS PRIME COAT SHALL BE PLACED ON THE PROPOSED AGGREGATE BASE COURSE PRIOR TO THE PLACEMENT OF THE FIRST LIFT OF PROPOSED BITUMINOUS BASE COURSE. THE PROPOSED AGGREGATE BASE COURSE SHALL HAVE A PRIME COAT OF BITUMINOUS MATERIAL APPLIED AT A RATE OF 0.35 GAL./S.Y. AND IN ACCORDANCE WITH THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS.

THE PROPOSED BITUMINOUS PRIME COAT WILL BE PAID FOR UNDER ITEM:  
AR602510 BITUMINOUS PRIME COAT \_\_\_\_\_ PER GAL.

**AR209-CRUSHED AGGREGATE BASE COURSE NOTES**

THE CRUSHED AGGREGATE BASE COURSE (209) SHALL BE PLACED IN ACCORDANCE WITH ITEM 209 "CRUSHED AGGREGATE BASE COURSE" AS STATED ON PAGE 93 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 2, 2009.

THE CRUSHED AGGREGATE BASE COURSE MATERIAL (CA-6) WILL BE USED TO CONSTRUCT A BASE COURSE FOR THE PROPOSED BITUMINOUS BASE COURSE (403). THE CRUSHED AGGREGATE BASE COURSE WILL BE UNIFORMLY BLENDED WITH THE PROPER MOISTURE INCORPORATED INTO THE MATERIAL PRIOR TO DELIVERY (PUGGED). THE CRUSHED AGGREGATE BASE COURSE MATERIAL WILL BE 6" IN DEPTH AND COMPACTED TO NOT LESS THAN 100 PERCENT DENSITY.

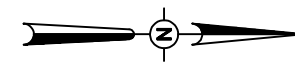
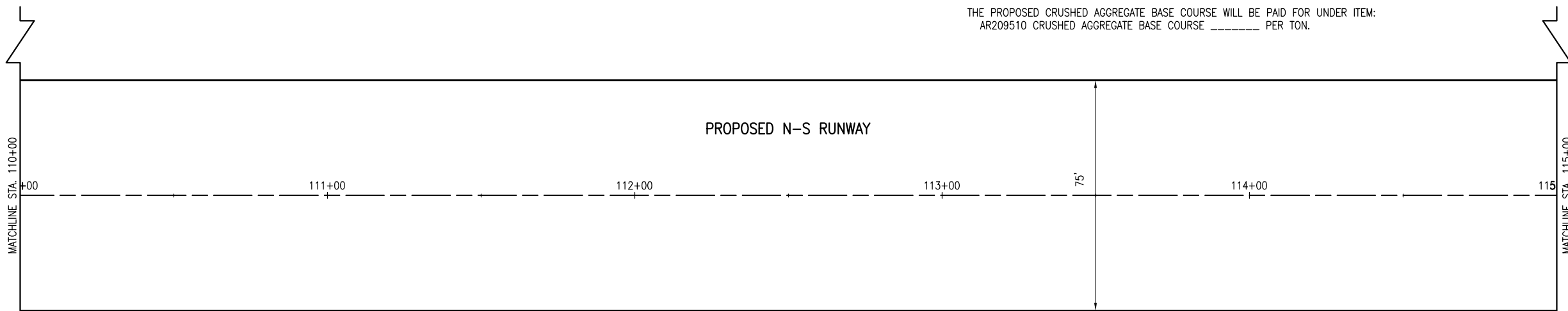
THE CONTRACTOR WILL PROVIDE A PROCTOR INDICATING THE MAXIMUM DENSITY AND OPTIMUM MOISTURE FOR THE CRUSHED AGGREGATE BASE COURSE.

THE CRUSHED AGGREGATE BASE COURSE MATERIAL SHALL BE PLACED USING A SPREADER BOX, PAVER OR OTHER APPROVED DEVICE. DUMPING FROM VEHICLES IN PILES THAT WILL REQUIRE REHANDLING WILL NOT BE PERMITTED.

THE CONTRACTOR MAY PLACE THE CRUSHED AGGREGATE BASE COURSE IN A SINGLE LIFT PROVIDED HE CAN DISPLAY THE ABILITY TO OBTAIN THE PROPOSED SUB-GRADE ELEVATION.

THE PROPOSED AGGREGATE FOR THE BASE COURSE MATERIAL SHALL MEET THE REQUIREMENTS OF GRADATION "B" IN TABLE 1 OF THE STANDARD SPECIFICATIONS.

THE PROPOSED CRUSHED AGGREGATE BASE COURSE WILL BE PAID FOR UNDER ITEM:  
AR209510 CRUSHED AGGREGATE BASE COURSE \_\_\_\_\_ PER TON.



0' 10' 20' 40'  
HALF SIZE SCALE: 1" = 40'  
FULL SIZE SCALE: 1" = 20'

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IL PROJ.: PRG-4018

Hanson Proj. No. 10A0053	Filename R=121CON_1.DWG	Scale 1" = 20'	Date 03/10/11
LAYOUT	BAK	01/04/11	xx/xx/xx
DRAWN	BAK	01/04/11	
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105+00 TO STA. 115+00

**BITUMINOUS PAVEMENT NOTE**

192 TONS OF BITUMINOUS BASE COURSE WAS ESTIMATED FOR OVERLAYING TAXIWAY B.  
73 TONS OF BITUMINOUS SURFACE COURSE WAS ESTIMATED FOR OVERLAYING TAXIWAY B.

**BITUMINOUS PAVEMENT MILLING (TAXIWAY "B")**

THE AREA(S) DESIGNATED AS [Hatched Pattern] ON THIS SHEET WILL BE CUT OR TRIMMED AS SHOWN ON THE CROSS-SECTIONS. THE CUTTING OR TRIMMING WILL BE DONE BY ROTO-MILLING. THE TOLERANCE OF THE MILLING WILL BE AS STATED IN THE STANDARD SPECIFICATIONS. NOTE THAT THE MILLING TAPERS FROM 1-1/2" IN DEPTH AT 37.5' LT. TO 0" IN DEPTH AT 30' LT.

IF A SQUARE STRAIGHT EDGE IS NOT OBTAINED FROM THE MILLING OPERATIONS, THE EXISTING PAVEMENT WILL BE SAWS AS SHOWN ON THIS SHEET. SAWING WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED PAVEMENT MILLING AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ALL MILLED MATERIAL WILL BE DISPOSED OF ON THE AIRPORT SITE.

PRIOR TO APPLYING THE BITUMINOUS OVERLAY ALL MILLED MATERIAL WILL BE BROOMED AND BLOWN CLEAN AND A BITUMINOUS TACK COAT APPLIED. THE VERTICAL FACE OF ALL SAW CUTS WILL BE PAINTED WITH A LIQUID ASPHALT.

ANY ADJACENT PAVEMENT DAMAGED BY THE MILLING OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

ALL BITUMINOUS PAVEMENT MILLING AREAS WILL BE LOCATED AND MARKED BY THE RESIDENT ENGINEER.

THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM:  
AR401650 "BITUMINOUS PAVEMENT MILLING" - PER S.Y.

**REMOVE AND REPLACE BITUMINOUS PAVEMENT (TAXIWAY "B")**

IN ORDER TO PROVIDE FOR THE INSTALLATION OF 42" RCCP, THE AREA DESIGNATED AS [Diagonal Line Pattern] ON THIS SHEET WILL HAVE THE EXISTING BITUMINOUS PAVEMENT REMOVED FULL DEPTH (6" BITUMINOUS PAVEMENT AND 4" CRUSHED AGGREGATE) AND REPLACED WITH A SIMILAR CONFIGURATION. THE BITUMINOUS MATERIAL USED IN THE REPLACEMENT WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED REMOVAL AND REPLACEMENT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. A MINIMUM OF 3 LIFTS OF BITUMINOUS MIX WITH A MINIMUM OF 93% COMPACTION WILL BE REQUIRED. THIS PAVEMENT REPLACEMENT WILL REQUIRE APPROXIMATELY 40 TONS OF BITUMINOUS MIX. ALL 3 LIFTS MAY BE PLACED USING BITUMINOUS BASE COURSE APPROVED FOR USE ON THIS LOCATION BY THE ILLINOIS DIVISION OF AERONAUTICS. ALL REMOVED MATERIAL WILL BE DISPOSED OF OFF THE AIRPORT SITE.

WHERE THE PROPOSED REMOVAL AND REPLACEMENT AREA ABUTS THE EXISTING PAVEMENT, THE PAVEMENT WILL BE SAWS AS SHOWN ON THIS SHEET. THE SAWING WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED PAVEMENT REMOVAL AND REPLACEMENT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ANY ADJACENT PAVEMENT DAMAGED BY THE PAVEMENT REMOVAL AND REPLACEMENT OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

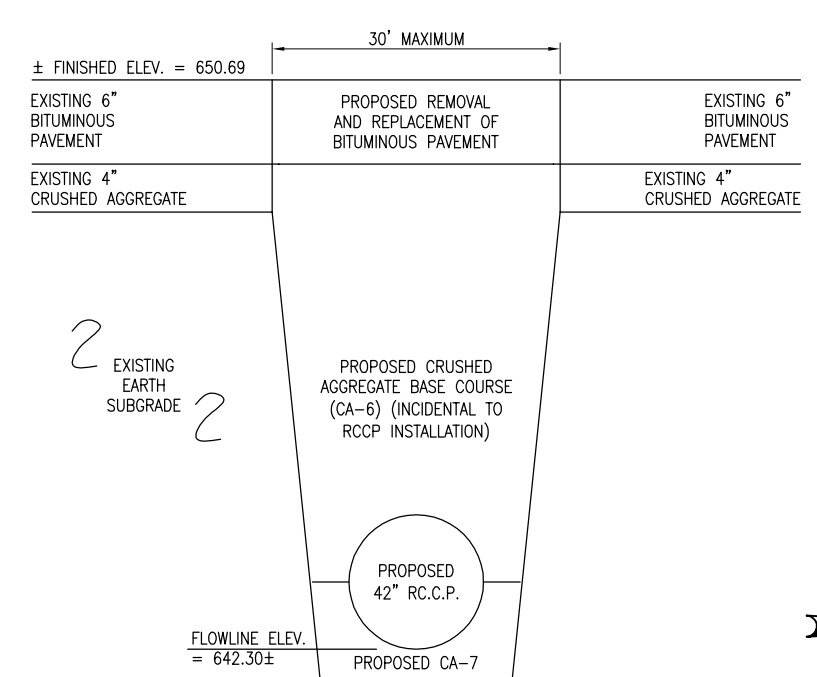
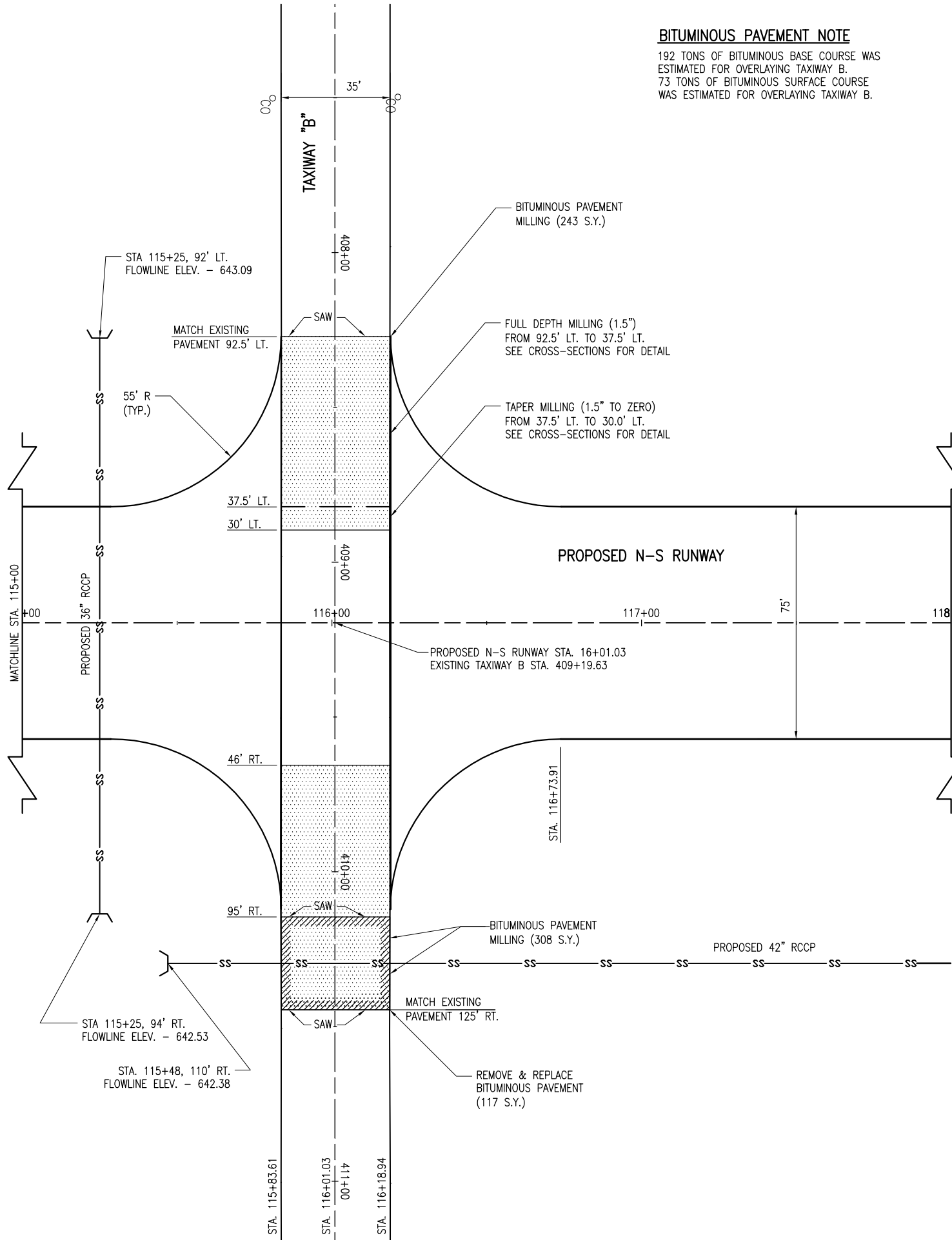
THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM:  
AR401910 "REMOVE & REPLACE BIT. PAVEMENT" - PER S.Y.

**BACKFILL NOTE:**

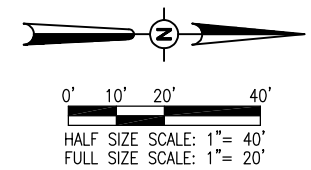
PIPE BACKFILL IN PAVEMENT AREAS SHALL BE CRUSHED AGGREGATE BACKFILL (CA-6) AND WILL BE PLACED FROM THE CA-7 CRADLE TO THE BOTTOM OF THE LIME MODIFIED SOIL ON THE PROPOSED RUNWAY AREAS AND TO THE BOTTOM OF THE BITUMINOUS REPLACEMENT ON THE EXISTING RUNWAY FOR A MINIMUM OF FIVE (5) FEET BEYOND THE EDGE OF ALL PAVEMENT. THE CA-7 CRADLE AS SHOWN IN THE 42" RCCP INSTALLATION DETAIL WILL BE TYPICAL FOR ALL RCCP.

THE CRUSHED AGGREGATED BACKFILL WILL CONTAIN SUFFICIENT MOISTURE TO OBTAIN 95% OF MAXIMUM DENSITY. THE BACKFILL WILL BE PLACED IN LOOSE LAYERS 6 INCHES IN DEPTH. THE CONTRACTOR WILL HAVE A NUCLEAR GAUGE AND QUALIFIED OPERATOR ONSITE FOR PURPOSES OF QUALITY CONTROL TESTING. WHEN THE CONTRACTOR IS SATISFIED WITH THE COMPACTION OF THE LIFT, THE RESIDENT ENGINEER SHALL BE INFORMED THAT THE LIFT IS READY FOR ACCEPTANCE TESTING. THE CONTRACTOR SHALL NOT PROCEED WITH ANOTHER LIFT OF BACKFILL UNTIL THE PREVIOUS LIFT HAS BEEN ACCEPTED BY THE RESIDENT ENGINEER. ANY AREAS DEEMED UNACCEPTABLE TO THE RESIDENT ENGINEER SHALL BE REWORKED AND RE-COMPACTED AT NO ADDITIONAL COST TO THE CONTRACT.

PIPE BACKFILL IN NON-PAVEMENT AREAS SHALL BE EXCAVATED MATERIAL COMPACTED IN ACCORDANCE TO ITEM AR152 FOR AREAS OUTSIDE PAVEMENT AREAS AND TO THE SATISFACTION OF THE RESIDENT ENGINEER.



**42" RCCP INSTALLATION DETAIL**



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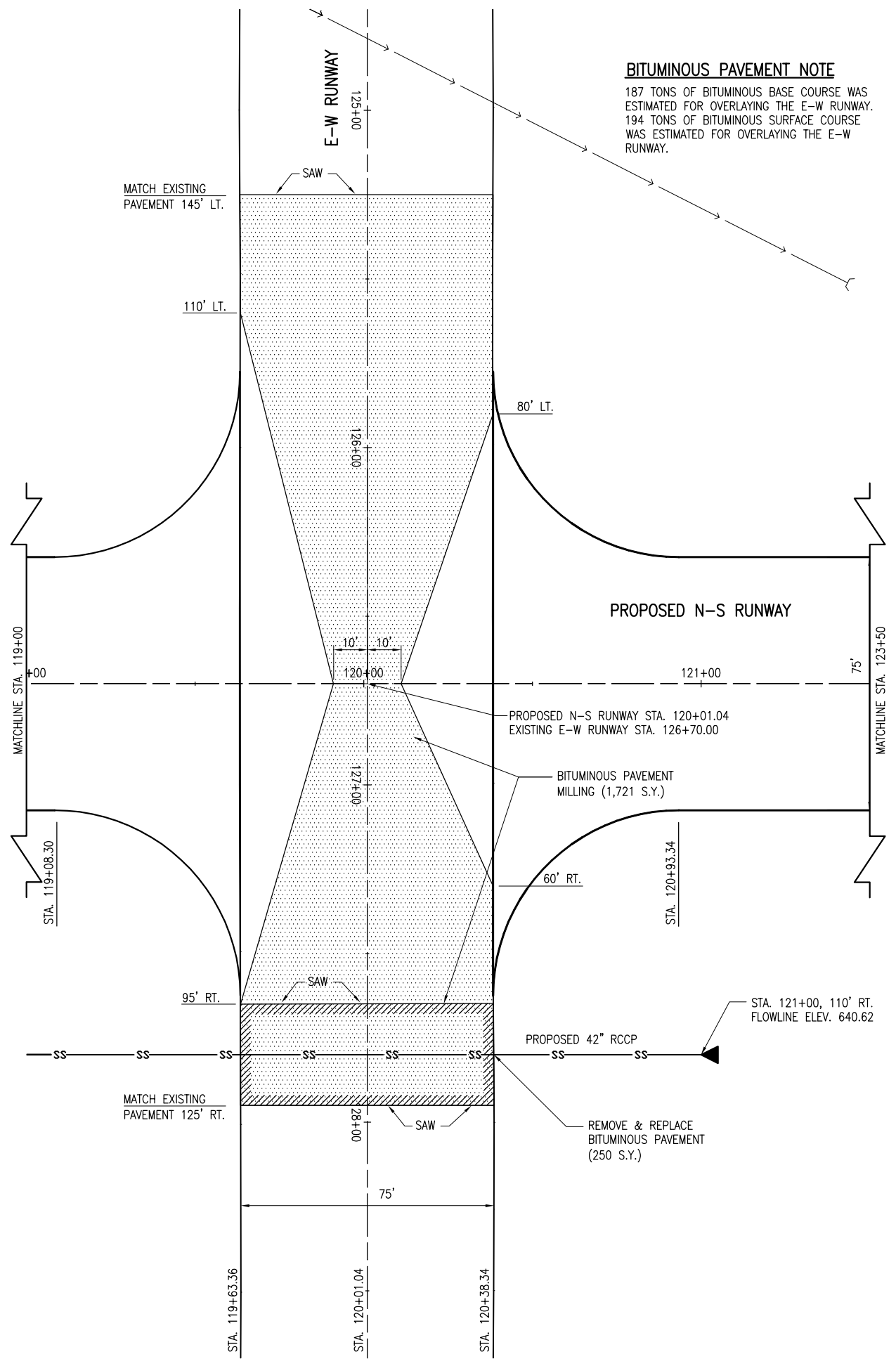
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LAYOUT	MDR	01/04/11	
DRAWN	DPE	01/21/11	
REVIEWED	CAH	2/18/11	

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**BITUMINOUS PAVEMENT NOTE**  
 187 TONS OF BITUMINOUS BASE COURSE WAS ESTIMATED FOR OVERLAYING THE E-W RUNWAY.  
 194 TONS OF BITUMINOUS SURFACE COURSE WAS ESTIMATED FOR OVERLAYING THE E-W RUNWAY.

**BITUMINOUS PAVEMENT MILLING (E-W RUNWAY)**

THE AREA DESIGNATED AS [Hatched Pattern] ON THIS SHEET WILL BE CUT OR TRIMMED AS SHOWN ON THE CROSS-SECTIONS. THE CUTTING OR TRIMMING WILL BE DONE BY ROTO-MILLING. THE TOLERANCE OF THE MILLING UNLESS OTHERWISE STATED IN THE STANDARD SPECIFICATIONS WILL BE 0" TO 1/2".

BOTH SIDES OF THE MILLING MACHINE WILL BE OPERATED ELECTRONICALLY AND THE MACHINE SENSORS WILL BE RUN ON A STRING LINE AND/OR STRING LINE LINE AND MATCHING SHOE.

IF A SQUARE STRAIGHT EDGE IS NOT OBTAINED FROM THE MILLING OPERATIONS, THE EXISTING PAVEMENT WILL BE SAWS AS SHOWN ON THIS SHEET. SAWING WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED PAVEMENT MILLING AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ALL MILLED MATERIAL WILL BE DISPOSED OF ON THE AIRPORT SITE.

PRIOR TO APPLYING THE BITUMINOUS OVERLAY ALL MILLED MATERIAL WILL BE BROOMED AND BLOWN CLEAN AND A BITUMINOUS TACK COAT APPLIED. THE VERTICAL FACE OF ALL SAW CUTS WILL BE PAINTED WITH A LIQUID ASPHALT.

ANY ADJACENT PAVEMENT DAMAGED BY THE MILLING OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

ALL BITUMINOUS PAVEMENT MILLING AREAS WILL BE LOCATED AND MARKED BY THE RESIDENT ENGINEER.

THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM:  
 AR401650 "BITUMINOUS PAVEMENT MILLING" - PER S.Y.

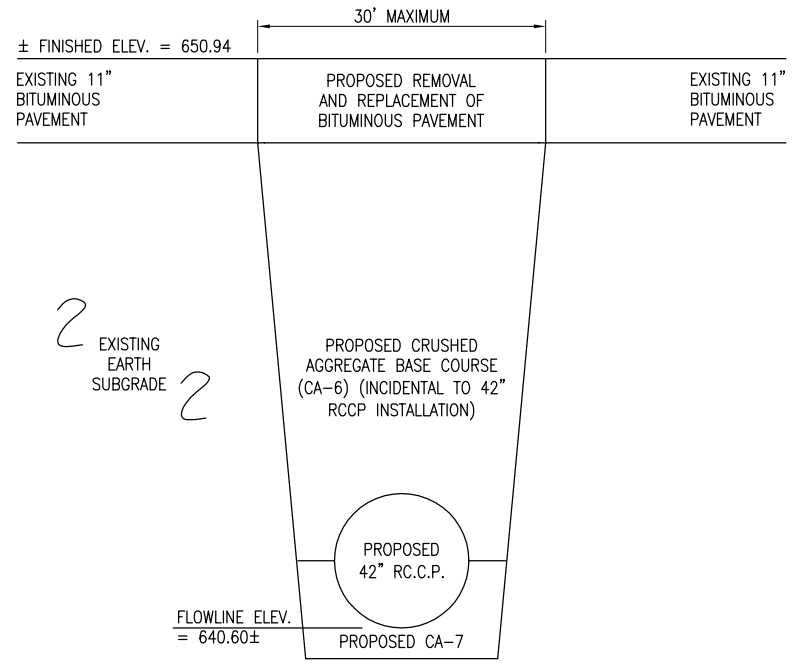
**REMOVE AND REPLACE BITUMINOUS PAVEMENT (E-W RUNWAY)**

IN ORDER TO PROVIDE FOR THE INSTALLATION OF THE 42" RCCP, THE AREA DESIGNATED AS [Diagonal Hatched Pattern] ON THIS SHEET WILL HAVE THE EXISTING BITUMINOUS PAVEMENT REMOVED FULL DEPTH (11" BITUMINOUS PAVEMENT) AND REPLACED WITH A SIMILAR CONFIGURATION. THE BITUMINOUS MATERIAL USED IN THE REPLACEMENT WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED REMOVAL AND REPLACEMENT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. A MINIMUM OF 4 LIFTS WILL BE REQUIRED WITH A MINIMUM OF 93% DENSITY ON EACH LIFT. THIS PAVEMENT REPLACEMENT WILL REQUIRE APPROXIMATELY 160 TONS OF BITUMINOUS MIX. ALL 4 LIFTS MAY BE PLACED USING BITUMINOUS BASE COURSE APPROVED FOR USE ON THIS LOCATION BY THE ILLINOIS DIVISION OF AERONAUTICS. ALL REMOVED MATERIAL WILL BE DISPOSED OF OFF THE AIRPORT SITE.

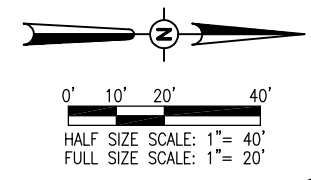
WHERE THE PROPOSED REMOVAL AND REPLACEMENT AREA ABUTS THE EXISTING PAVEMENT, THE PAVEMENT WILL BE SAWS AS SHOWN ON THIS SHEET. THE SAWING WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED PAVEMENT REMOVAL AND REPLACEMENT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ANY ADJACENT PAVEMENT DAMAGED BY THE PAVEMENT REMOVAL AND REPLACEMENT OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM:  
 AR401910 "REMOVE & REPLACE BIT. PAVEMENT" - PER S.Y.



**NOTE:**  
 CRUSHED AGGREGATE BACKFILL WILL BE PLACED A MINIMUM OF FIVE (5) FEET BEYOND THE EDGE OF THE PAVEMENT.



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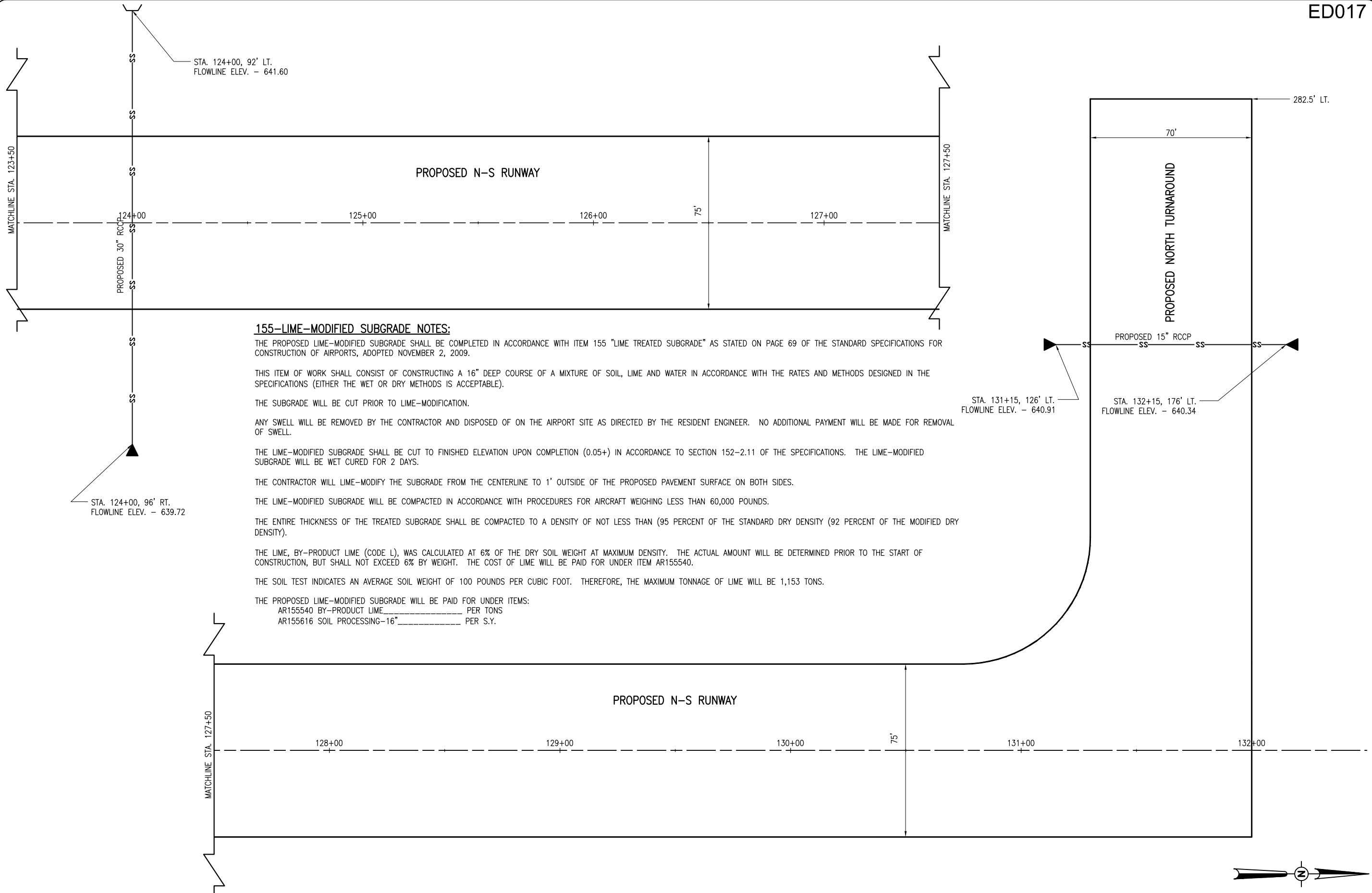
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**155-LIME-MODIFIED SUBGRADE NOTES:**

THE PROPOSED LIME-MODIFIED SUBGRADE SHALL BE COMPLETED IN ACCORDANCE WITH ITEM 155 "LIME TREATED SUBGRADE" AS STATED ON PAGE 69 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 2, 2009.

THIS ITEM OF WORK SHALL CONSIST OF CONSTRUCTING A 16" DEEP COURSE OF A MIXTURE OF SOIL, LIME AND WATER IN ACCORDANCE WITH THE RATES AND METHODS DESIGNED IN THE SPECIFICATIONS (EITHER THE WET OR DRY METHODS IS ACCEPTABLE).

THE SUBGRADE WILL BE CUT PRIOR TO LIME-MODIFICATION.

ANY SWELL WILL BE REMOVED BY THE CONTRACTOR AND DISPOSED OF ON THE AIRPORT SITE AS DIRECTED BY THE RESIDENT ENGINEER. NO ADDITIONAL PAYMENT WILL BE MADE FOR REMOVAL OF SWELL.

THE LIME-MODIFIED SUBGRADE SHALL BE CUT TO FINISHED ELEVATION UPON COMPLETION (0.05+) IN ACCORDANCE TO SECTION 152-2.11 OF THE SPECIFICATIONS. THE LIME-MODIFIED SUBGRADE WILL BE WET CURED FOR 2 DAYS.

THE CONTRACTOR WILL LIME-MODIFY THE SUBGRADE FROM THE CENTERLINE TO 1' OUTSIDE OF THE PROPOSED PAVEMENT SURFACE ON BOTH SIDES.

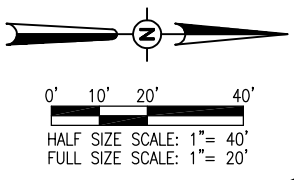
THE LIME-MODIFIED SUBGRADE WILL BE COMPACTED IN ACCORDANCE WITH PROCEDURES FOR AIRCRAFT WEIGHING LESS THAN 60,000 POUNDS.

THE ENTIRE THICKNESS OF THE TREATED SUBGRADE SHALL BE COMPACTED TO A DENSITY OF NOT LESS THAN (95 PERCENT OF THE STANDARD DRY DENSITY (92 PERCENT OF THE MODIFIED DRY DENSITY).

THE LIME, BY-PRODUCT LIME (CODE L), WAS CALCULATED AT 6% OF THE DRY SOIL WEIGHT AT MAXIMUM DENSITY. THE ACTUAL AMOUNT WILL BE DETERMINED PRIOR TO THE START OF CONSTRUCTION, BUT SHALL NOT EXCEED 6% BY WEIGHT. THE COST OF LIME WILL BE PAID FOR UNDER ITEM AR155540.

THE SOIL TEST INDICATES AN AVERAGE SOIL WEIGHT OF 100 POUNDS PER CUBIC FOOT. THEREFORE, THE MAXIMUM TONNAGE OF LIME WILL BE 1,153 TONS.

THE PROPOSED LIME-MODIFIED SUBGRADE WILL BE PAID FOR UNDER ITEMS:  
 AR155540 BY-PRODUCT LIME \_\_\_\_\_ PER TONS  
 AR155616 SOIL PROCESSING-16" \_\_\_\_\_ PER S.Y.

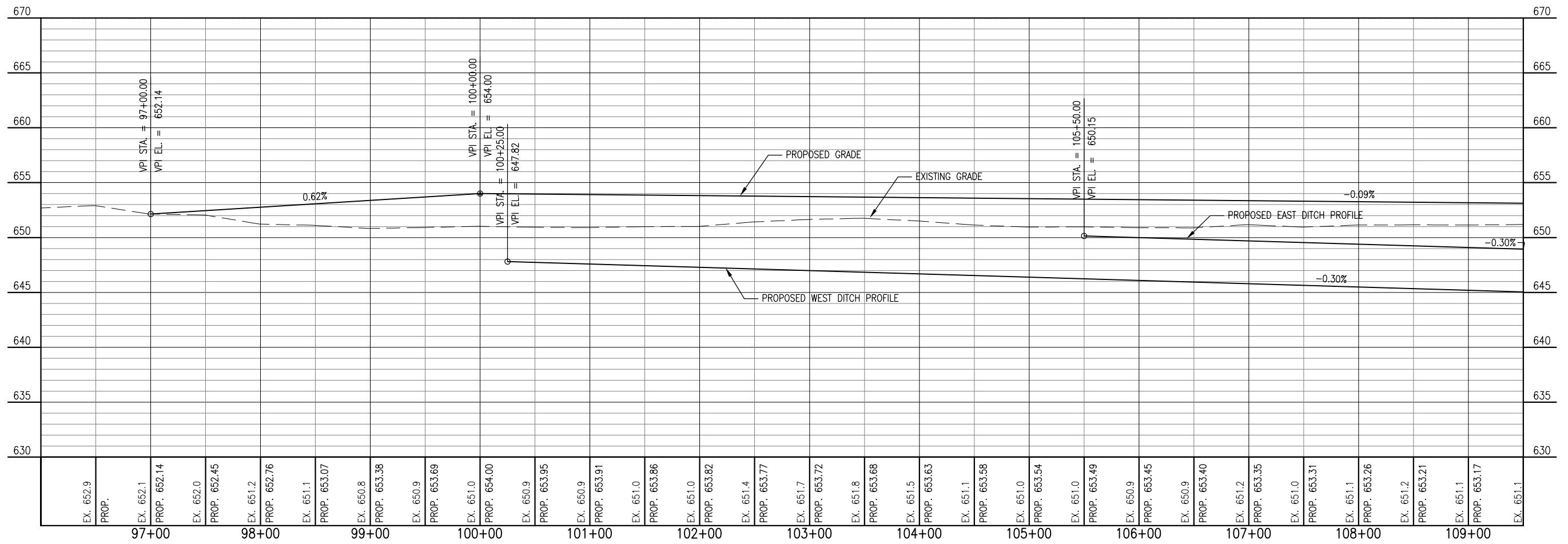
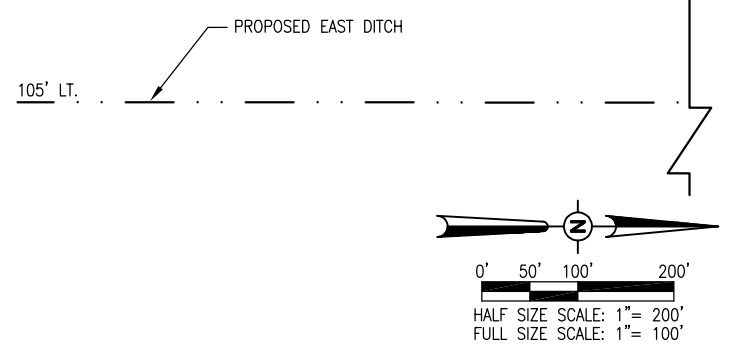
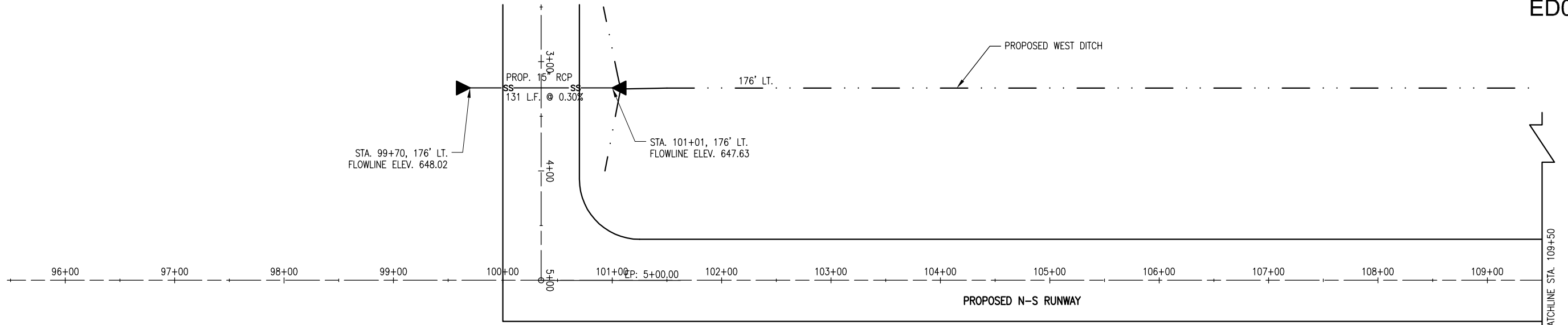


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Hanson Proj. No. 10A00053 Filename R=121CON_1.DWG Scale 1" = 20' Date 03/10/11	LAYOUT 01/04/11 DRAWN BAK REVIEWED CAH	BAK 01/04/11 CAH xx/xx/xx
CONSTRUCT CROSSWIND RUNWAY 18-36 PROPOSED CONSTRUCTION PLAN STA. 123+50 TO STA. 132+00		11 11 of 137 sheets

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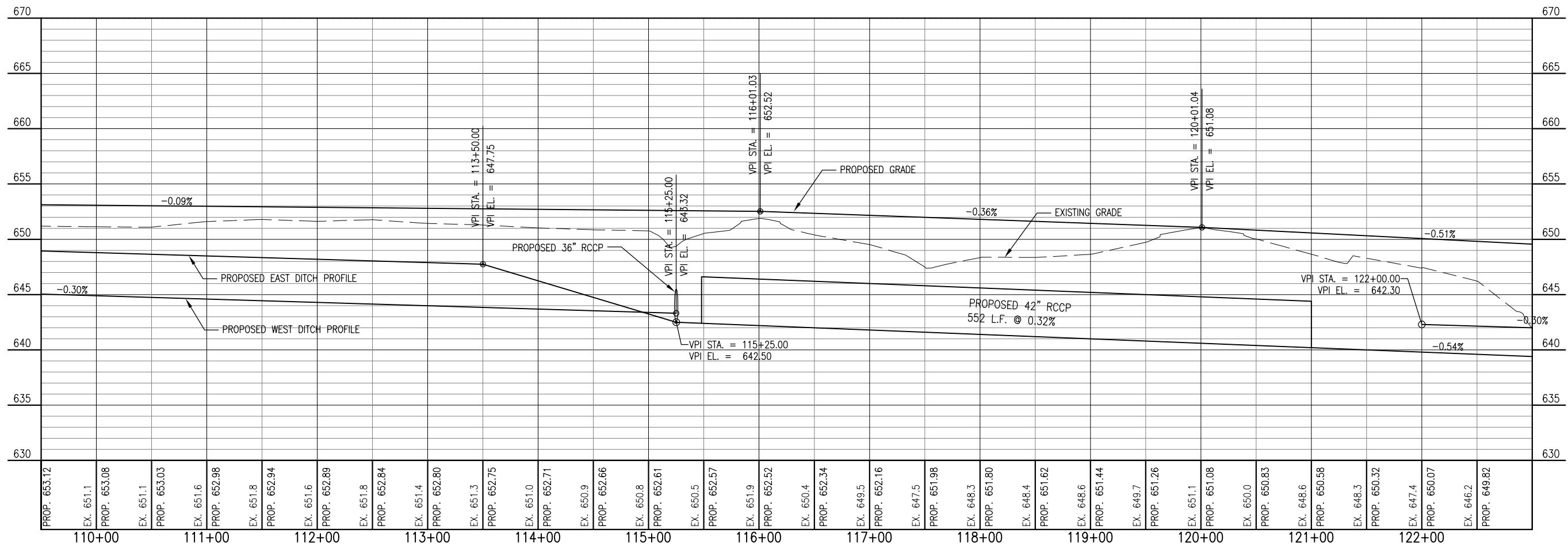
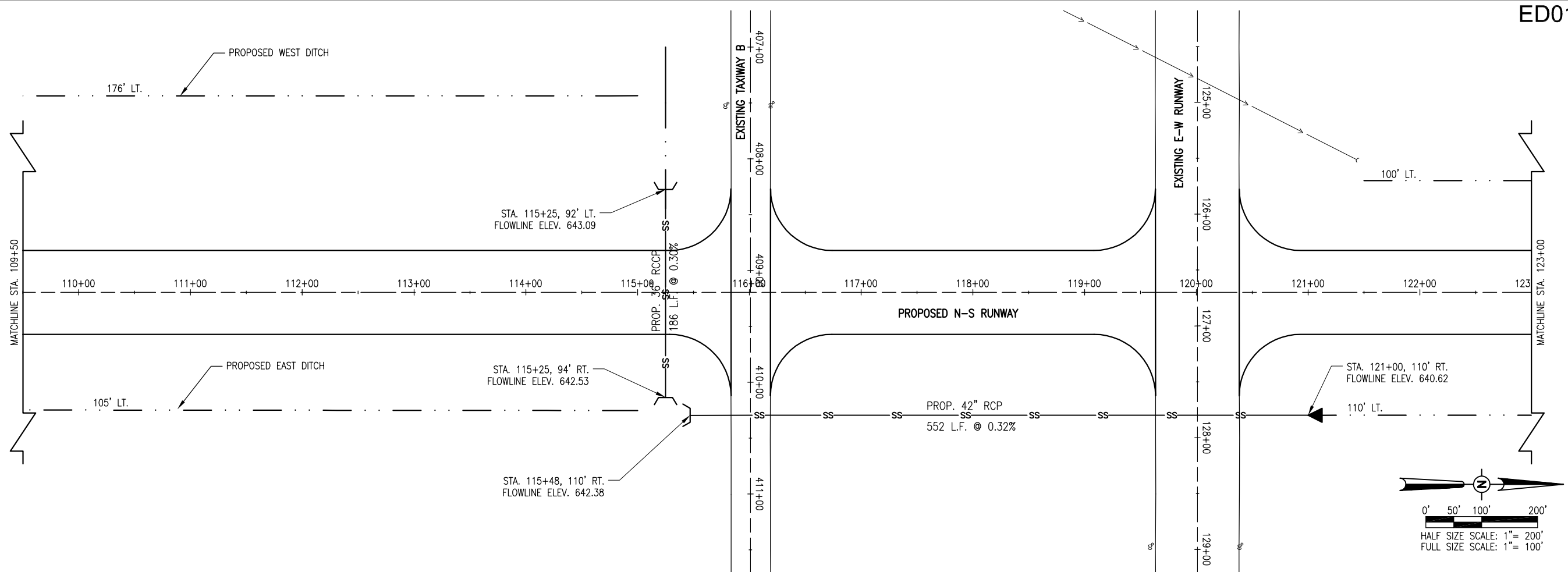
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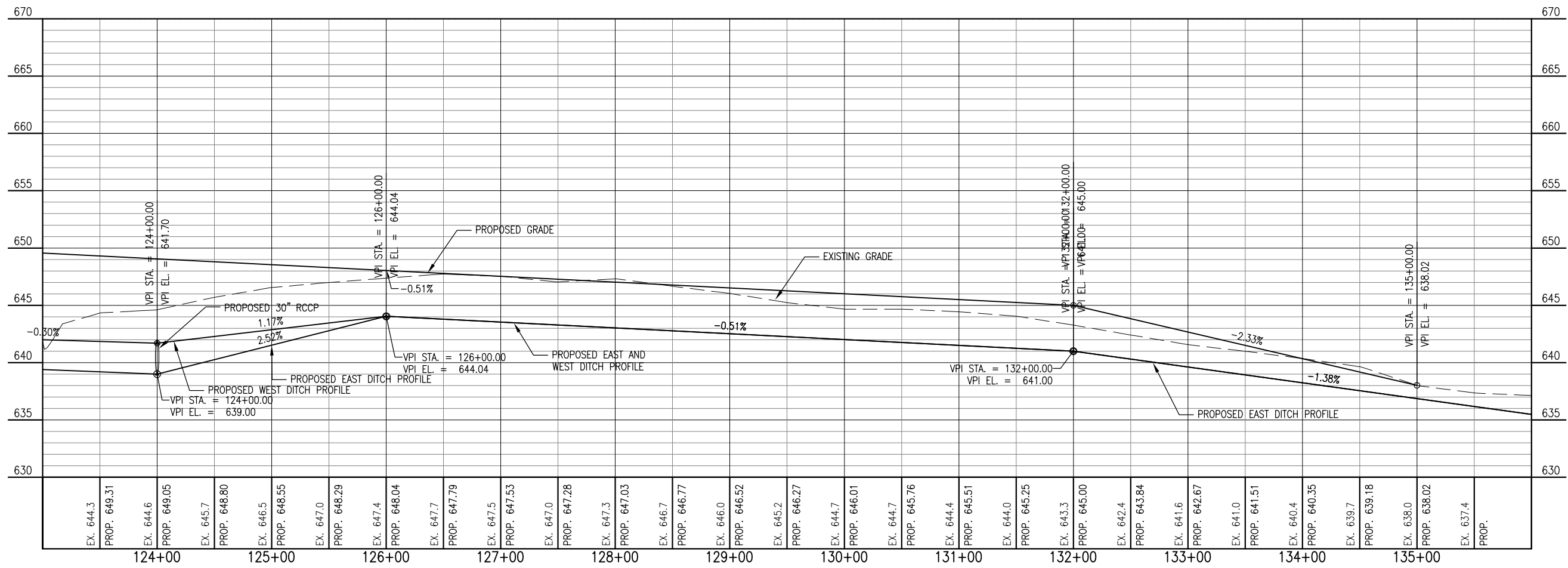
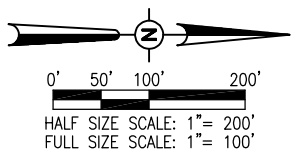
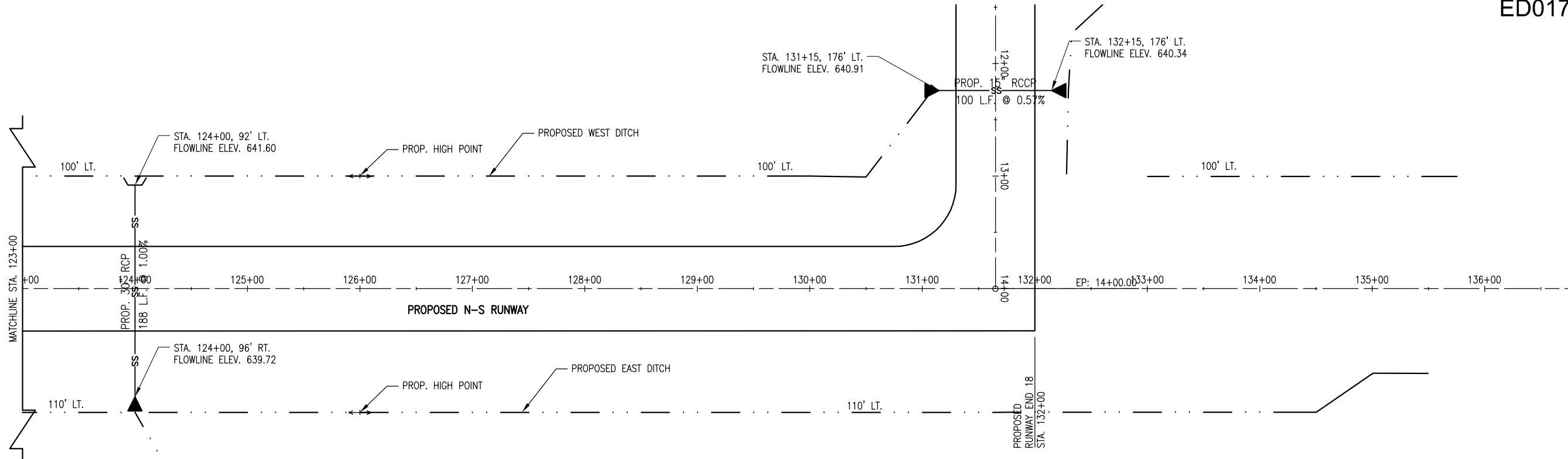
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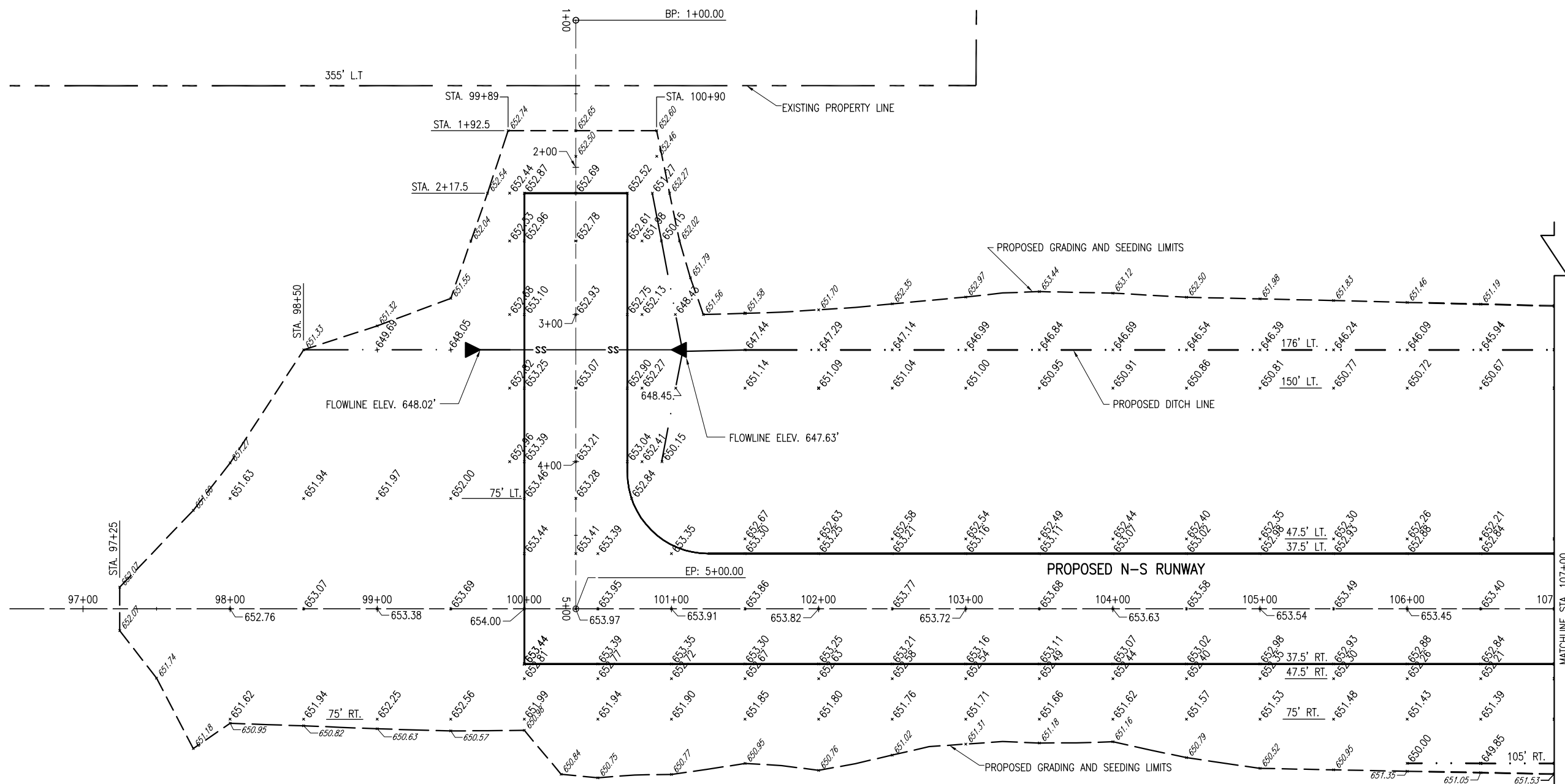
**LEGEND**

- PROPOSED PAVEMENT
- PROPOSED GRADING AND SEEDING LIMITS
- EXISTING GRADE
- PROPOSED GRADE

0' 20' 40' 80'

HALF SIZE SCALE: 1" = 80'

FULL SIZE SCALE: 1" = 40'



**EROSION CONTROL BLANKET NOTES**

AN EROSION CONTROL MATERIAL (KNITTED STRAW MAT) 4' IN WIDTH WILL BE INSTALLED ALONG THE SHOULDER OF ALL PAVEMENTS WITH IN THE GRADING AND SEEDING LIMITS SHOWN ON THESE CONSTRUCTION PLAN SHEETS AND IN ACCORDANCE WITH THE SPECIAL PROVISION

THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM AR156531 "EROSION CONTROL BLANKET" PER S.Y.

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Hanson Proj. No. 10A00553	Filename R-191STK.DWG	Scale 1" = 40'	Date 03/10/11
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PROPOSED STAKING PLAN  
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**901 SEEDING NOTES**

THE PROPOSED SEEDING SHALL BE ACCOMPLISHED IN ACCORDANCE WITH ITEM 901 "SEEDING" AS STATED ON PAGE 324 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 2, 2009.

DUE TO THE TIME RESTRAINTS ON THE CLEARING AND GRUBBING, THE SEEDING CONTRACTOR MAY BE REQUIRED TO MOBILIZE TWICE.

ALL DISTURBED AREAS LOCATED WITHIN THE PROPOSED GRADING AND SEEDING LIMITS AND THE CLEARING AND GRUBBING LIMITS WILL BE SEEDING IN ACCORDANCE WITH THE ABOVE NOTED SPECIFICATION. ALL AREAS DISTURBED OUTSIDE THE DESIGNATED GRADING AND SEEDING LIMITS WILL BE SEEDING AT THE CONTRACTOR'S OWN EXPENSE. ANY FARMING AREAS ADJACENT TO THESE LIMITS THAT HAS BEEN COMPACTED BY VEHICLE TRAFFIC SHALL BE THOROUGHLY DISKED TO LOOSEN THE SOIL TO THE SATISFACTION OF THE RESIDENT ENGINEER.

ALL MATERIALS AND/OR DEBRIS RESULTING FROM THE SEEDING OPERATIONS WILL BE REMOVED FROM THE PAVEMENTS AND MISCELLANEOUS STRUCTURES PRIOR TO OPENING THE RUNWAY.

901-3.4 MAINTENANCE OF SEEDED AREAS. DELETE THE SECOND PARAGRAPH OF THIS SECTION AND ADD THE FOLLOWING:

"THE CONTRACTOR WILL BE REQUIRED TO ESTABLISH A GOOD STAND OF GRASS OF UNIFORM COLOR AND DENSITY TO THE SATISFACTION OF THE RESIDENT ENGINEER. IF AT THE TIME WHEN THE CONTRACT HAS BEEN OTHERWISE COMPLETED, IT IS NOT POSSIBLE TO MAKE AN ADEQUATE DETERMINATION OF COLOR, DENSITY, AND UNIFORMITY OF SUCH STAND OF GRASS, THE ITEM OF WORK WILL BE REVIEWED AT A LATER DATE DETERMINED BY THE ILLINOIS DIVISION OF AERONAUTICS."

DATE SEEDING COMPLETED \_\_\_\_\_.

THE PROPOSED SEEDING WILL BE PAID FOR UNDER ITEMS:  
AR901510 SEEDING \_\_\_\_ PER ACRES.

**908 MULCHING NOTES**

THE PROPOSED MULCHING SHALL BE ACCOMPLISHED IN ACCORDANCE WITH ITEM 908 "MULCHING" AS STATED ON PAGE 334 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 2, 2009.

THIS ITEM SHALL CONSIST OF THE FURNISHING, TRANSPORTING, AND PLACING MULCH OVER THE SEEDED AREA. DISTURBED AREAS OUTSIDE THE GRADING LIMITS SHALL ALSO BE MULCHED IN ACCORDANCE TO THIS SPECIFICATION.

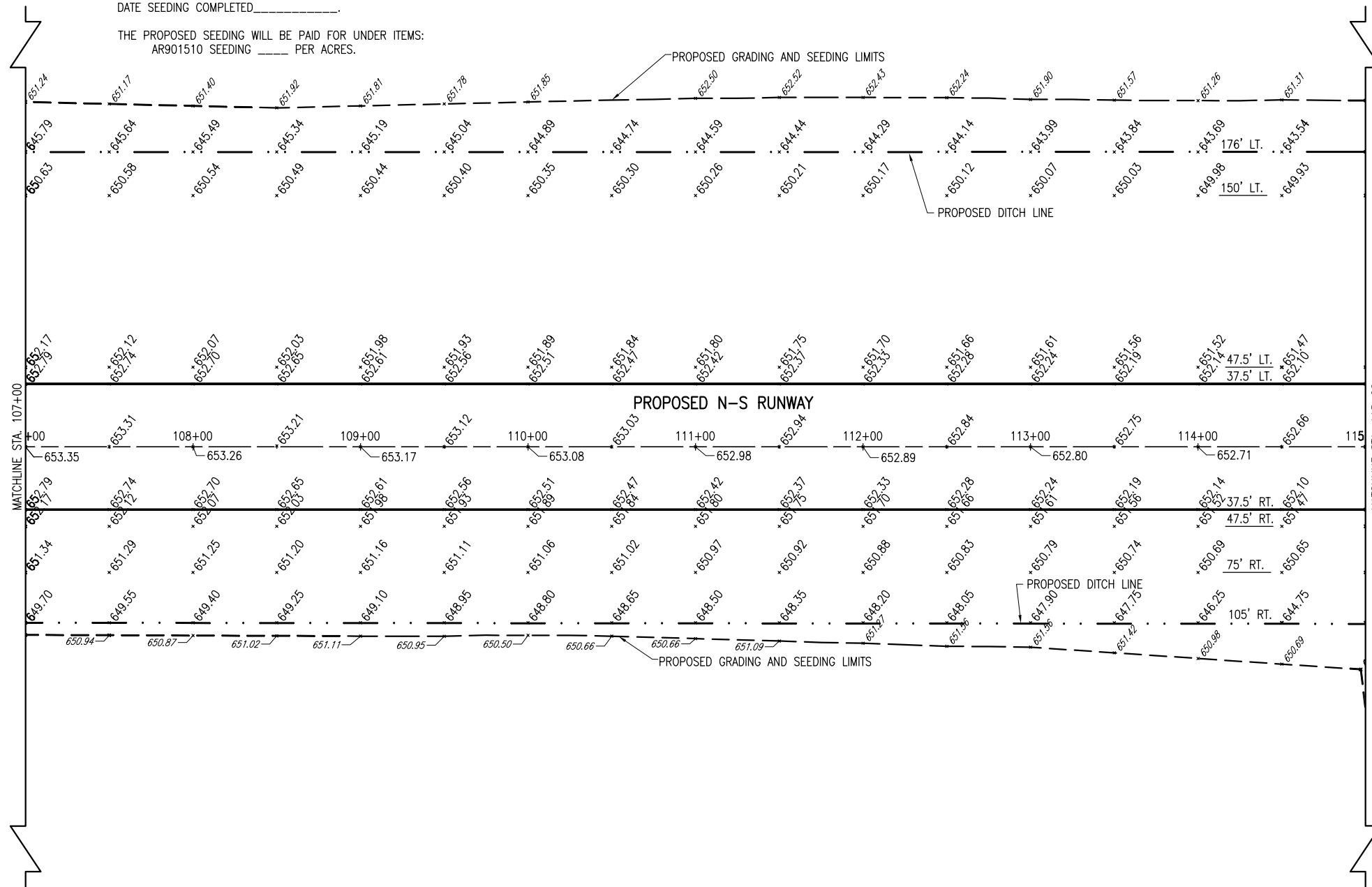
908-2.1 MULCH MATERIAL: THE CONTRACTOR MAY EITHER FURNISH STRAW OR HYDROMULCH AS THE TYPE OF MULCH MATERIAL TO BE USED ON THIS PROJECT.

908-3.1 MULCHING: THE HYDRAULIC MULCH SHALL BE APPLIED AS A SLURRY OF 2,500 POUNDS OF MULCH AND NOT LESS THAN 2,500 GALLONS OF WATER PER ACRE.

908-3.4 STRUCTURE CLEANING: AFTER THE PROPOSED MULCH HAS BEEN APPLIED, THE CONTRACTOR WILL CLEAN THE MULCH OFF ALL STRUCTURES (DRAINAGE, ELECTRICAL, LIGHTS, ETC.).

DATE MULCHING COMPLETED \_\_\_\_\_.

THE PROPOSED MULCHING WILL BE PAID FOR UNDER ITEMS:  
AR908510 MULCHING \_\_\_\_ PER ACRES.



**LEGEND**

- PROPOSED PAVEMENT
- PROPOSED GRADING AND SEEDING LIMITS
- EXISTING GRADE
- PROPOSED GRADE

0' 20' 40' 80'

HALF SIZE SCALE: 1" = 80'  
FULL SIZE SCALE: 1" = 40'

REVISION		EDGAR COUNTY AIRPORT PARIS, ILLINOIS A.I.P. PROJ.: 3-17-0077-B13 IL PROJ.: PRG-4018
DATE		
Hanson Proj. No. 10A0053 Filename R-191STK.DWG Scale 1" = 40' Date 03/10/11		LAYOUT BAK 01/04/11 DRAWN BAK 01/04/11 REVIEWED CAH xx/xx/xx
© Copyright Hanson Professional Services Inc. 2011 Hanson Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62703-2886 www.hanson-inc.com Offices Nationwide		<b>CONSTRUCT CROSSWIND RUNWAY 18-36</b>  PROPOSED STAKING PLAN STA. 107+00 TO STA. 115+00
<span style="font-size: 2em; font-weight: bold;">16</span>		16 of 137 sheets






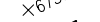


**TEMPORARY SEEDING NOTE**

THE GRADING LIMITS ARE SHOWN ON THE STAKING PLAN SHEETS BY A HEAVY DASHED LINE ( - - - ). ALL AREAS WITHIN THESE LIMITS, AND THE LIMITS OF THE PROPOSED CLEARING AND GRUBBING, EXCEPT THE PROPOSED PAVEMENT, SHALL BE LIMED, FERTILIZED AND SEEDED IN ACCORDANCE WITH THE FOLLOWING FORMULA AND RATES OF APPLICATION.

SEEDING (MINIMUM POUNDS OF PURE LIVE SEED PER ACRE)  
 PERENNIAL RYEGRASS \_\_\_\_\_ 50  
 SPRING OATS\* \_\_\_\_\_ 64  
 \*OTHER SEEDS MAY BE USED IF APPROVED BY THE ENGINEER.  
 AREA TO BE SEEDED \_\_\_\_\_ 43.7 ACRES  
 DATE SEEDING COMPLETED \_\_\_\_\_

**LEGEND**

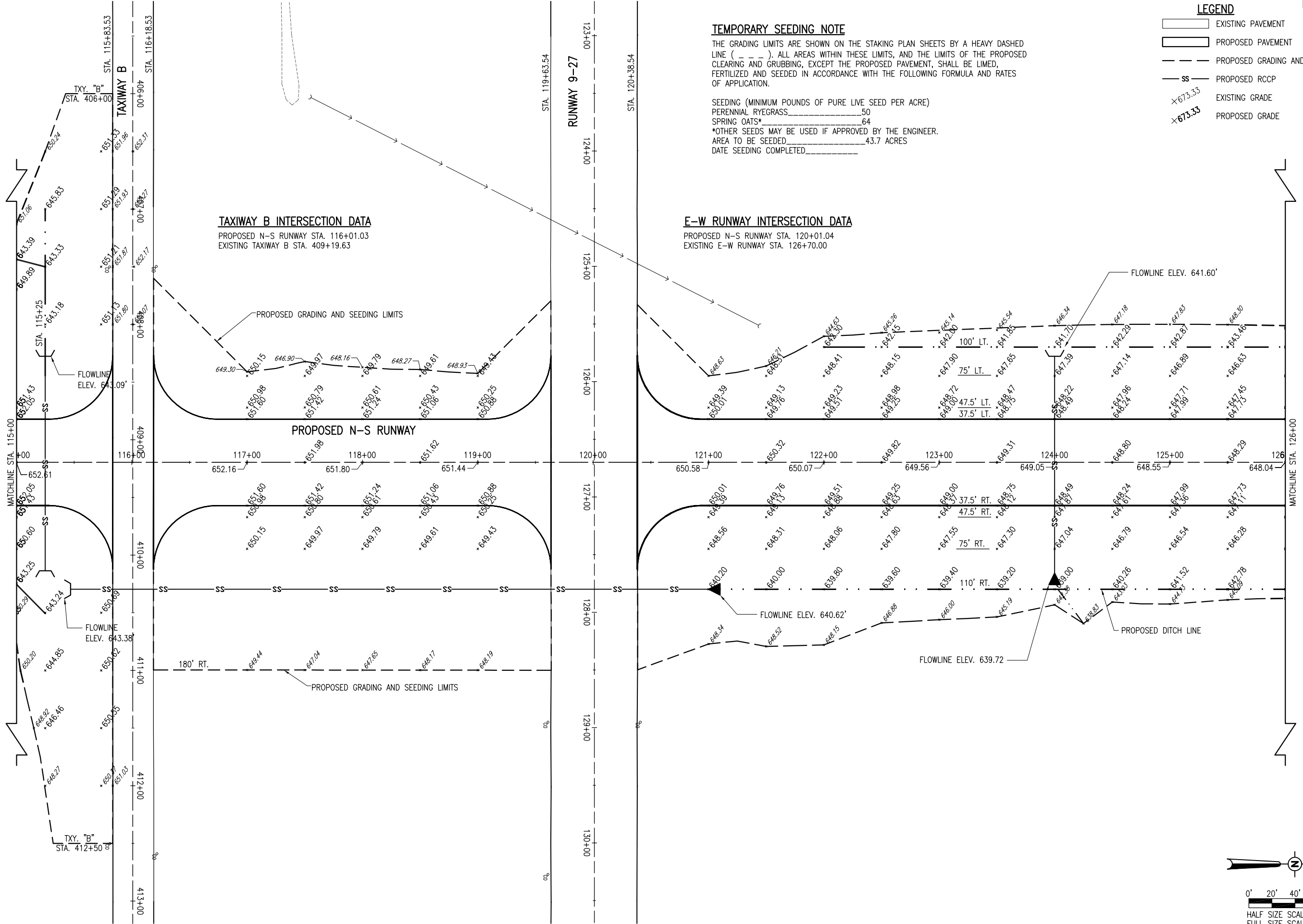
-  EXISTING PAVEMENT
-  PROPOSED PAVEMENT
-  PROPOSED GRADING AND SEEDING LIMITS
-  PROPOSED RCCP
-  EXISTING GRADE
-  PROPOSED GRADE

**TAXIWAY B INTERSECTION DATA**

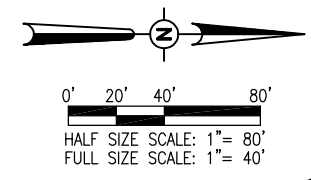
PROPOSED N-S RUNWAY STA. 116+01.03  
 EXISTING TAXIWAY B STA. 409+19.63

**E-W RUNWAY INTERSECTION DATA**

PROPOSED N-S RUNWAY STA. 120+01.04  
 EXISTING E-W RUNWAY STA. 126+70.00

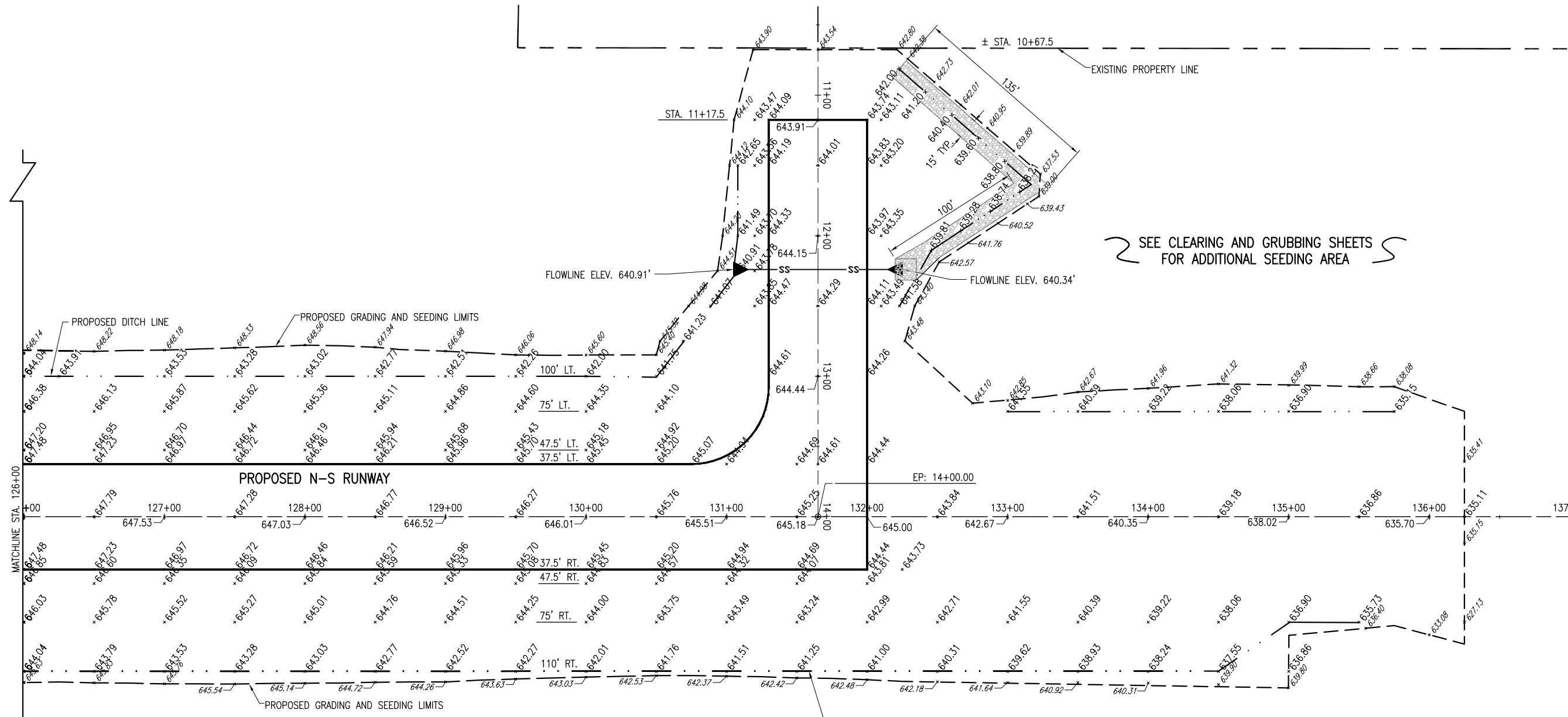


<p>Hanson Proj. No. 10A0053                  Filename R-191STK.DWG                  Scale 1" = 40'                  Date 03/10/11</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>LAYOUT</td> <td>BAK</td> <td>01/04/11</td> </tr> <tr> <td>DRAWN</td> <td>BAK</td> <td>01/04/11</td> </tr> <tr> <td>REVIEWED</td> <td>CAH</td> <td>xx/xx/xx</td> </tr> </table>	LAYOUT	BAK	01/04/11	DRAWN	BAK	01/04/11	REVIEWED	CAH	xx/xx/xx	<p style="text-align: center;"><b>HANSON</b>                  Professional Services Inc. 2011                  1526 South Sixth Street                  Springfield, Illinois 62703-2886                  www.hanson-inc.com                  Offices Nationwide</p> <p style="text-align: center;"><b>CONSTRUCT CROSSWIND                  RUNWAY 18-36</b></p> <p style="text-align: center;"><b>PROPOSED STAKING PLAN                  STA. 115+00 TO STA. 126+00</b></p> <p style="text-align: center; font-size: 2em; font-weight: bold;">17</p> <p style="text-align: center; font-size: 0.8em;">17 of 137 sheets</p>
LAYOUT	BAK	01/04/11								
DRAWN	BAK	01/04/11								
REVIEWED	CAH	xx/xx/xx								
<p>EDGAR COUNTY AIRPORT                  PARIS, ILLINOIS</p> <p>IL PROJ.: PRG-4018                  A.I.P. PROJ.: 3-17-0077-B13</p>	<p>REVISION</p> <p>DATE</p>									



**LEGEND**

	PROPOSED PAVEMENT
	PROPOSED GRADING AND SEEDING LIMITS
	EXISTING GRADE
	PROPOSED GRADE



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REVISION	DATE

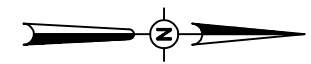
EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

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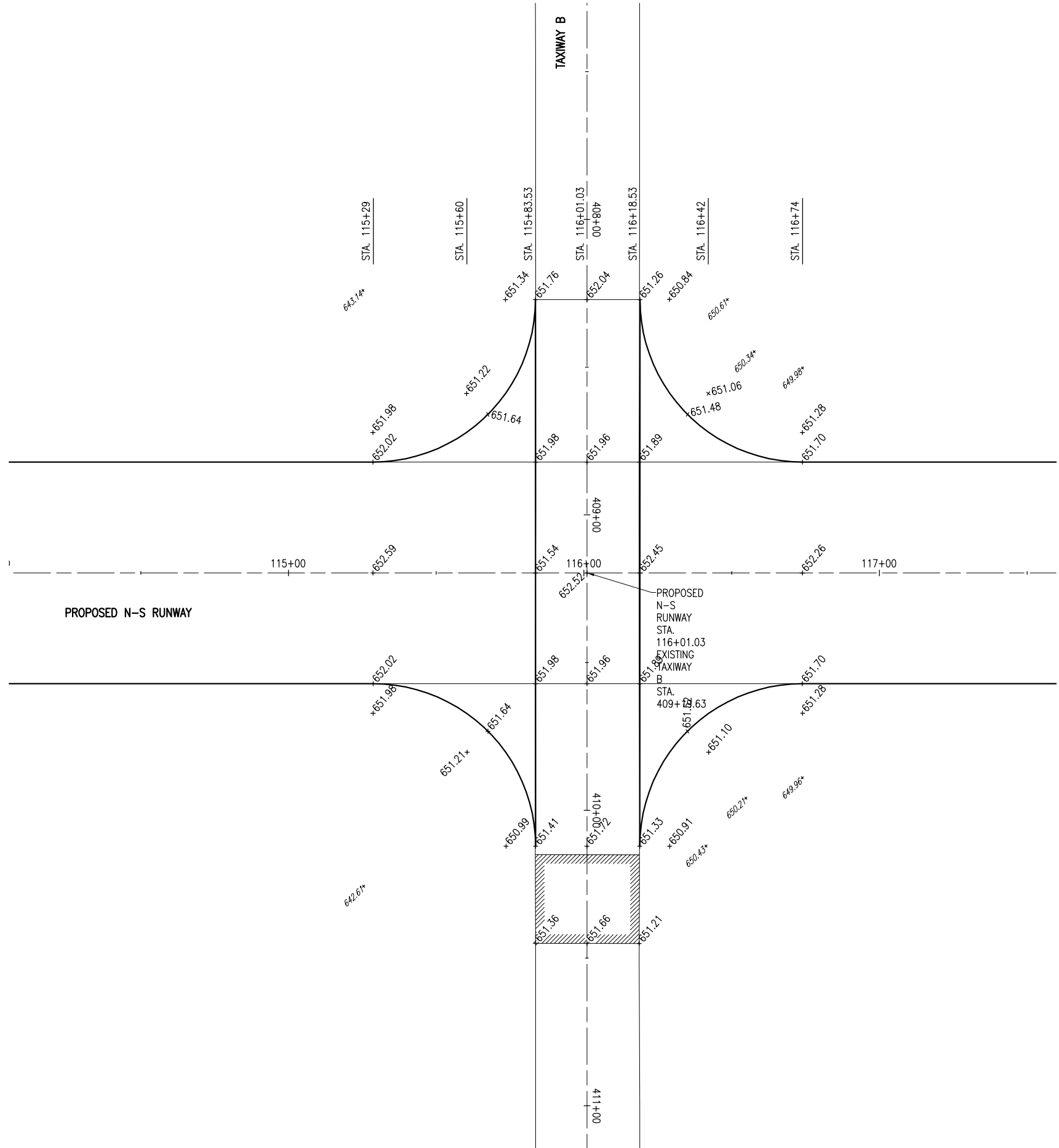
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Scale 1" = 40'	Date 03/10/11
LAYOUT BAK 01/04/11	REVIEWED CAH xx/xx/xx
DRAWN BAK 01/04/11	

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CONSTRUCT CROSSWIND  
RUNWAY 18-36  
PROPOSED STAKING PLAN  
STA. 126+00 TO STA. 137+00



0' 20' 40' 80'  
HALF SIZE SCALE: 1" = 80'  
FULL SIZE SCALE: 1" = 40'

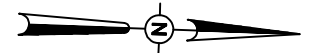


PROPOSED N-S RUNWAY

TAXIWAY B

PROPOSED  
N-S  
RUNWAY  
STA.  
116+01.03  
EXISTING  
TAXIWAY  
B  
STA.  
409+00.63

- LEGEND**
- EXISTING PAVEMENT
  - PROPOSED PAVEMENT
  - EXISTING GRADE
  - PROPOSED GRADE



0' 10' 20' 40'  
 HALF SIZE SCALE: 1" = 40'  
 FULL SIZE SCALE: 1" = 20'

REVISION	DATE

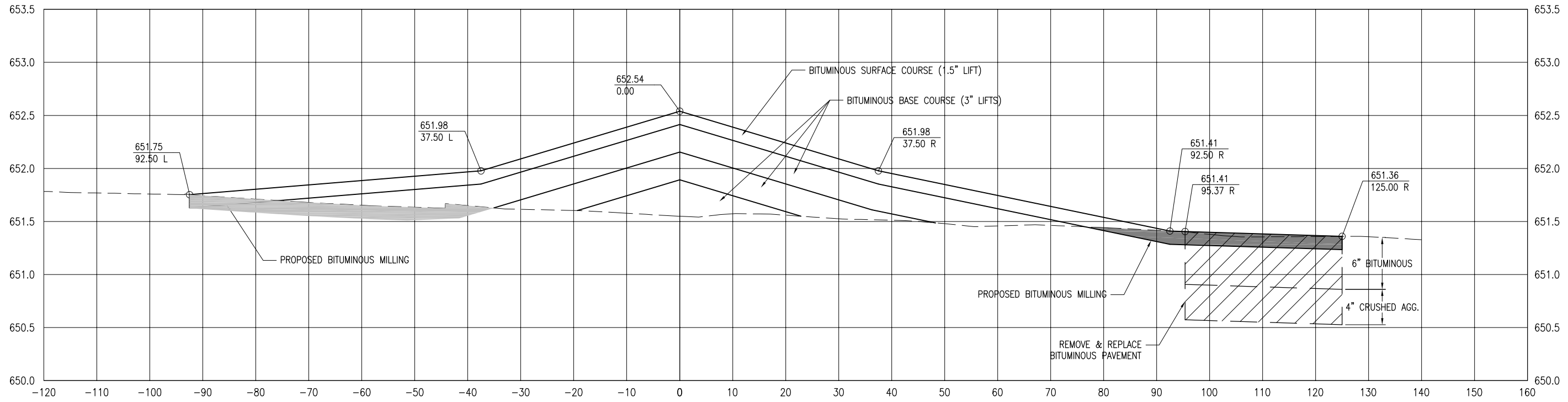
EDGAR COUNTY AIRPORT  
 PARIS, ILLINOIS  
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Date 03/10/11		

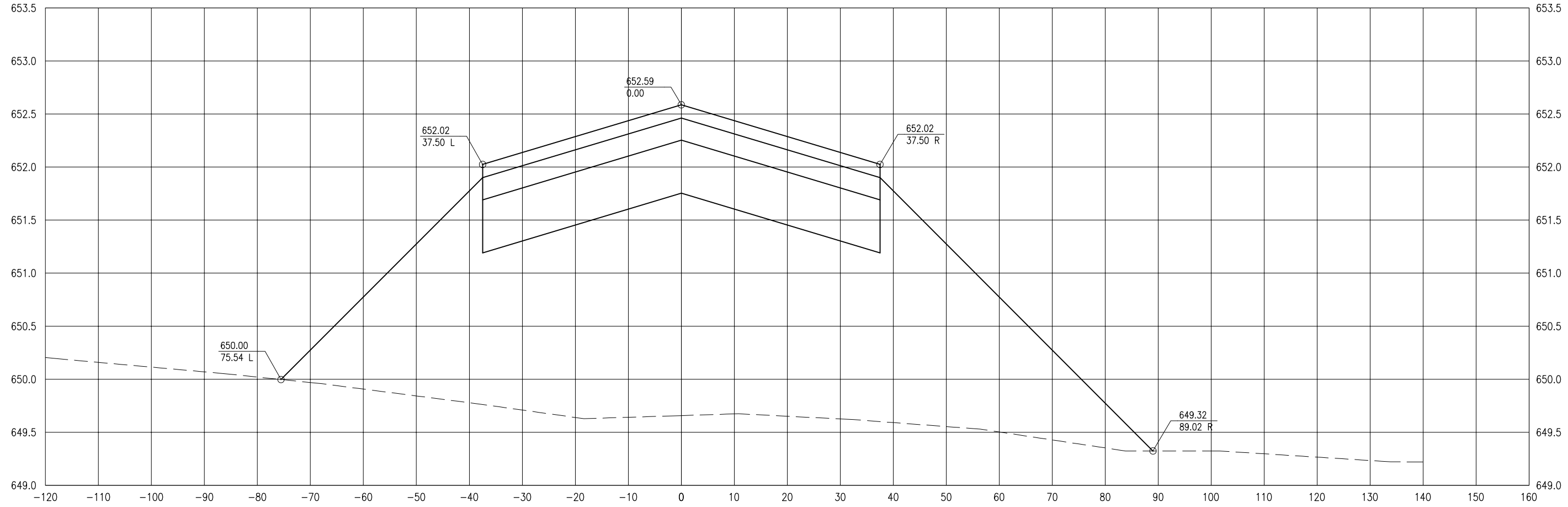
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CONSTRUCT CROSSWIND  
 RUNWAY 18-36  
 N-S RUNWAY AND TAXIWAY B  
 INTERSECTION STAKING PLAN

SOUTH TAXIWAY EDGE  
STA. 115+83.61



STA. 115+28.59



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REVISION	DATE

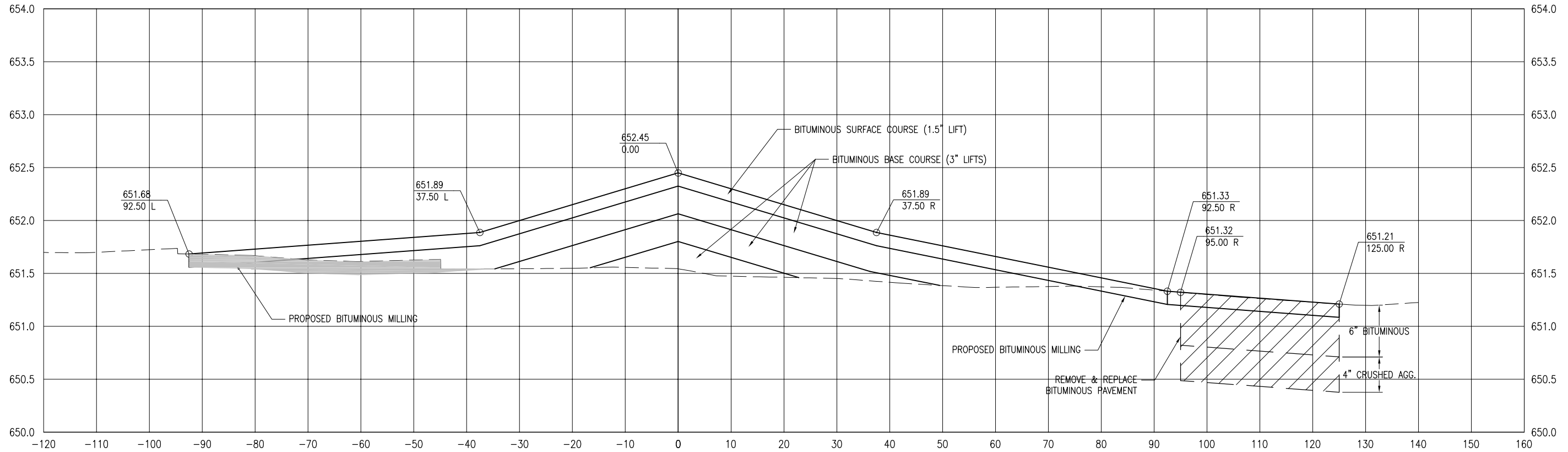
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REVIEWED	CAH	xx/xx/xx	

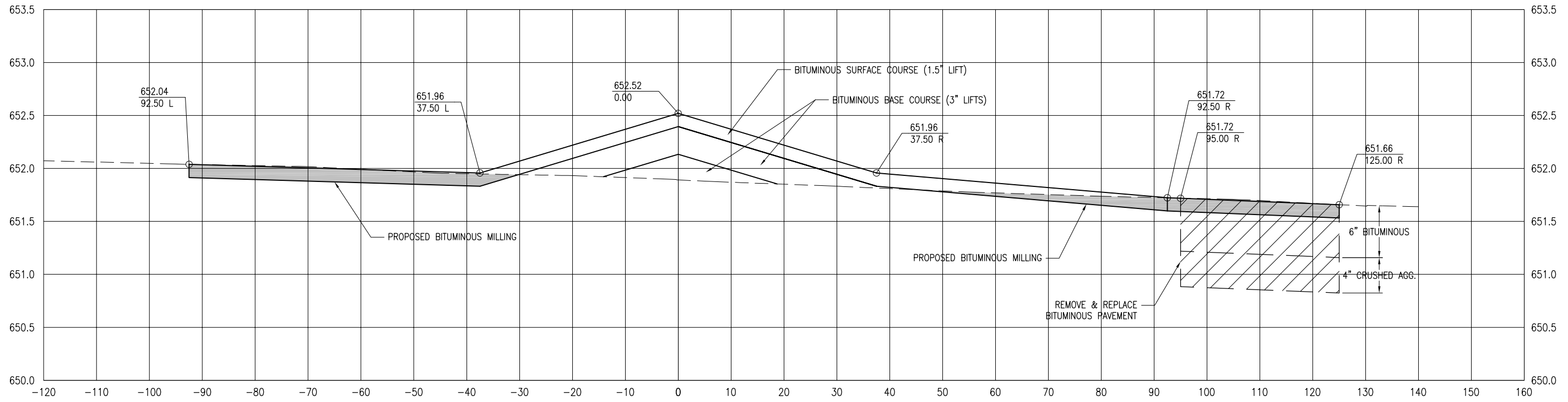
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CONSTRUCT CROSSWIND  
RUNWAY 18-36  
N-S RWY. & TXY. B  
INTERSECTION STA.  
115+28.59 - STA. 115+83.61

NORTH TAXIWAY EDGE  
STA. 116+18.94



TAXIWAY CENTERLINE  
STA. 116+01.03



REVISION	DATE

EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS  
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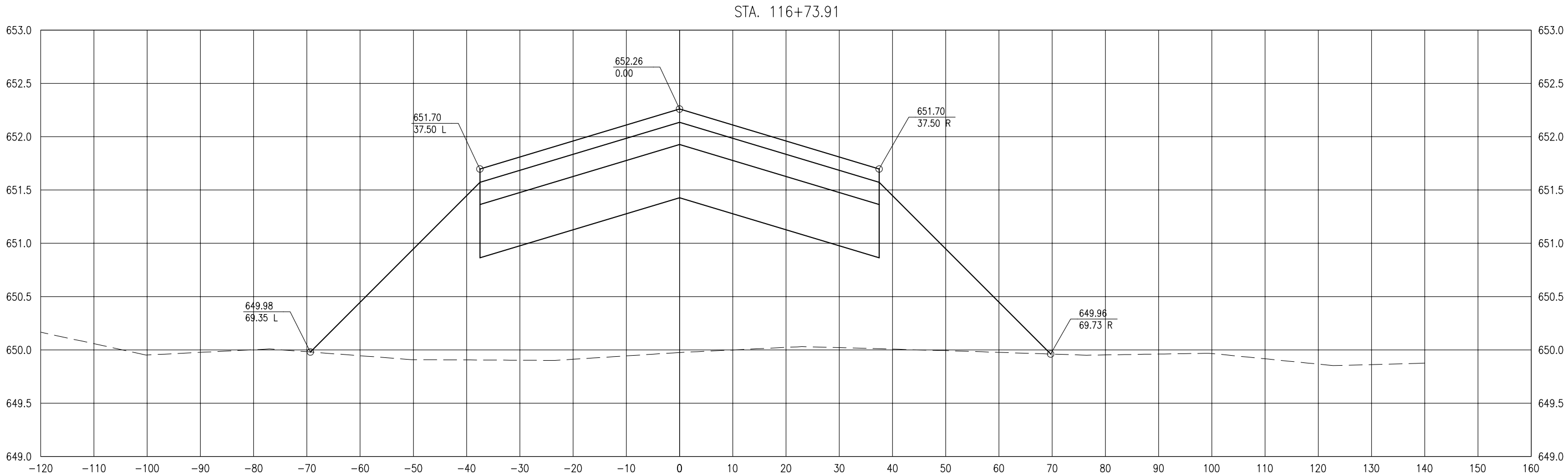
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RUNWAY 18-36  
N-S RWY. & TXY. B  
INTERSECTION STA  
116+01.03 - STA. 116+18.94

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DATE	REVISION

**EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS**

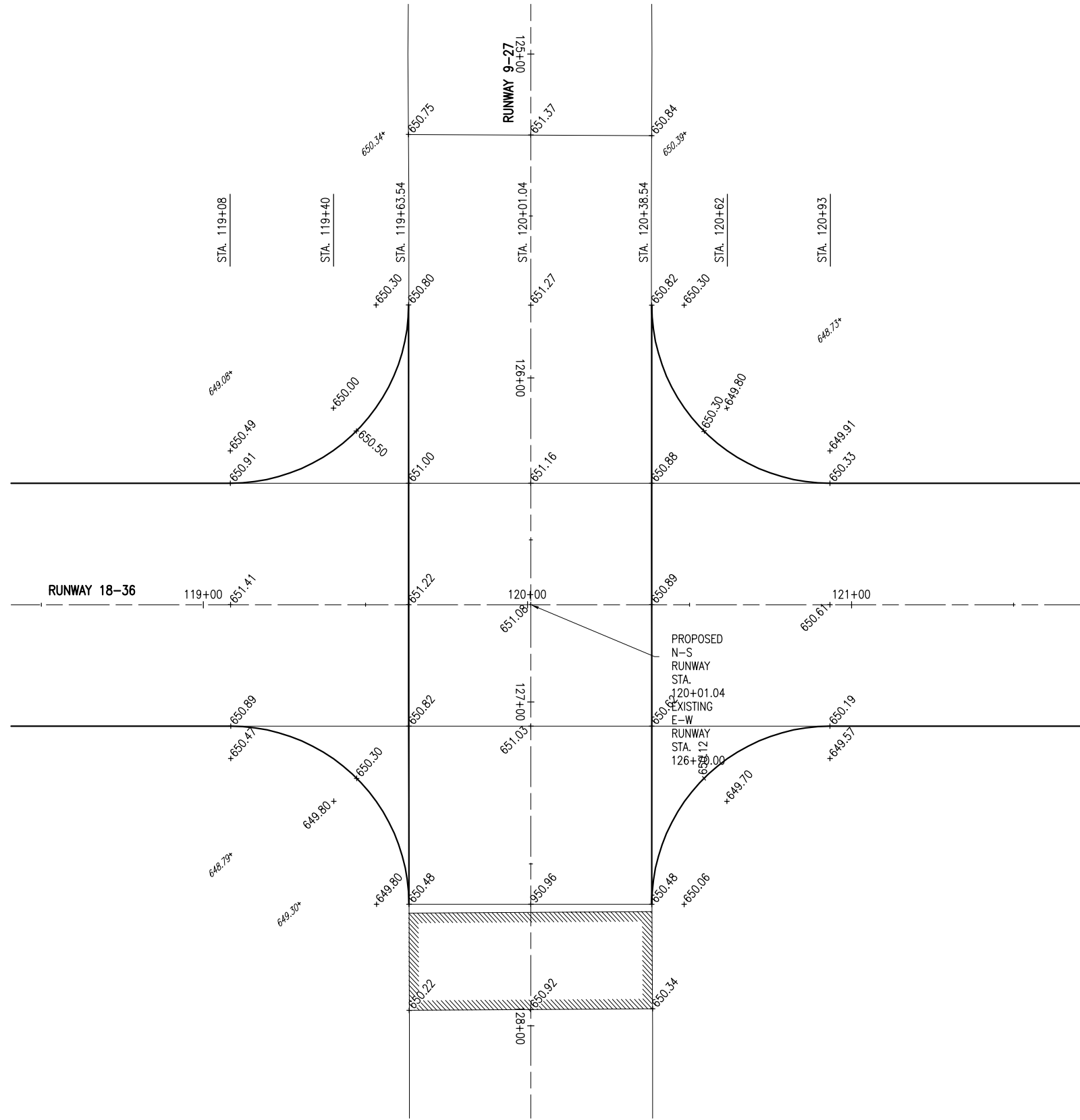
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DRAWN	DPE	01/04/11	
REVIEWED	CAH	xx/xx/xx	

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**CONSTRUCT CROSSWIND  
RUNWAY 18-36**  
 N-S RWY. & TXY. B  
 INTERSECTION  
 STA. 116+73.91





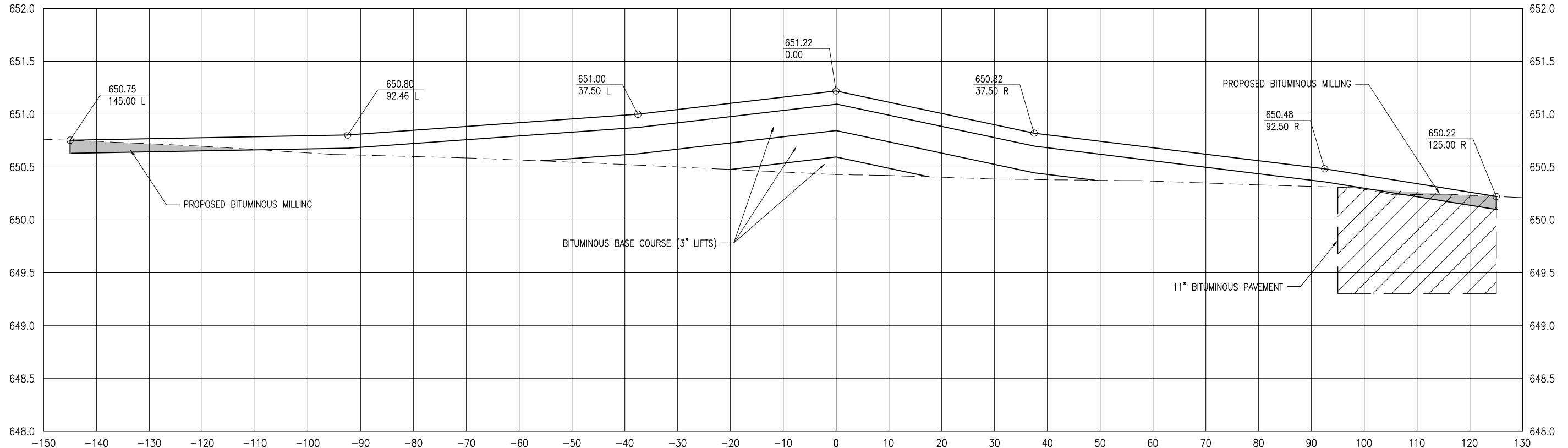
**LEGEND**

- EXISTING PAVEMENT
- PROPOSED PAVEMENT
- EXISTING GRADE
- PROPOSED GRADE

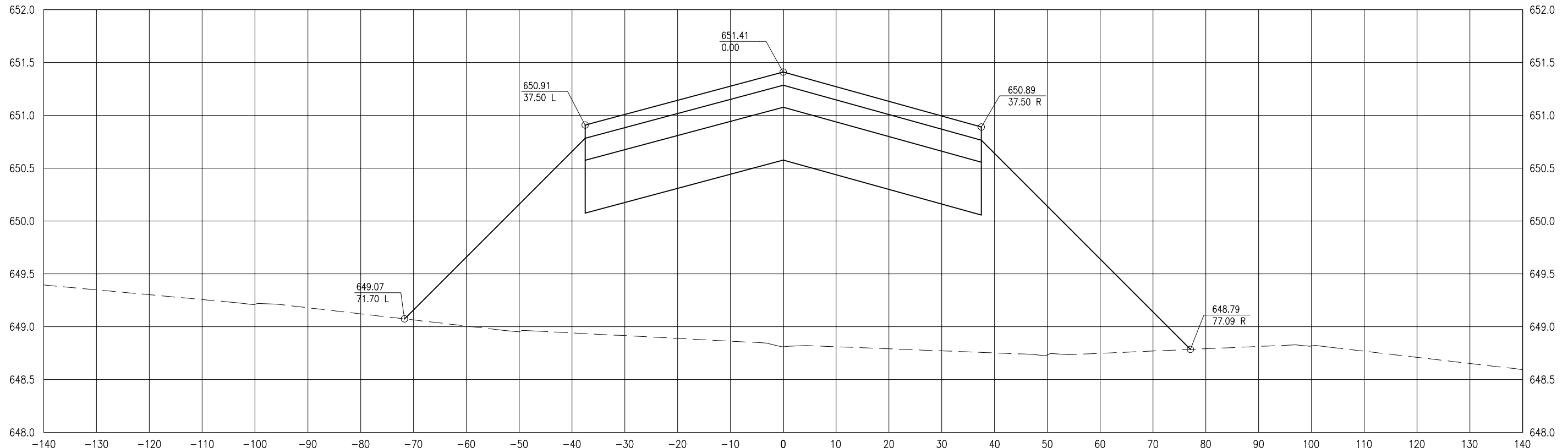
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 FULL SIZE SCALE: 1" = 20'

<b>EDGAR COUNTY AIRPORT PARIS, ILLINOIS</b> <small>IL PROJ.: PRG-4018 A.I.P. PROJ.: 3-17-0077-B13</small>										
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LAYOUT	JEO	01/04/11								
DRAWN	JEO	01/04/11								
REVIEWED	CAH	xx/xx/xx								
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<b>CONSTRUCT CROSSWIND RUNWAY 18-36</b> <b>N-S RUNWAY AND E-W RUNWAY INTERSECTION STAKING PLAN</b>										
<div style="display: flex; justify-content: space-between; align-items: center;"> <span style="font-size: 2em; font-weight: bold;">23</span> <span><small>23 of 137 sheets</small></span> </div>										

E-W RUNWAY EDGE  
STA. 119+63.36



STA. 119+08.30



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REVISION	DATE

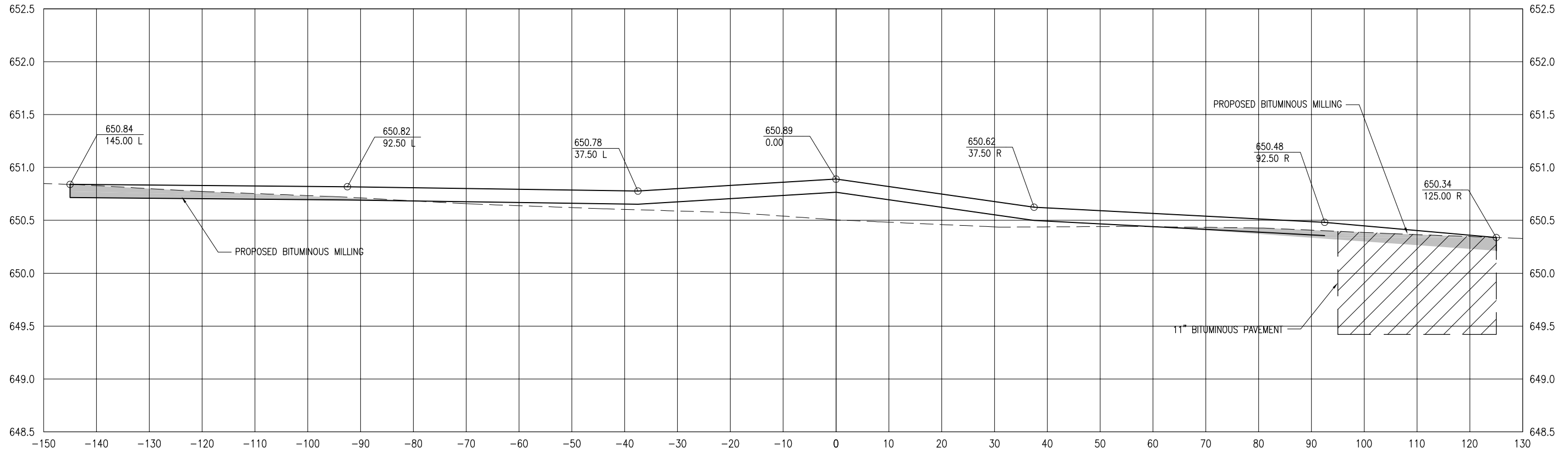
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PARIS, ILLINOIS  
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IL PROJ.: PRG-4018

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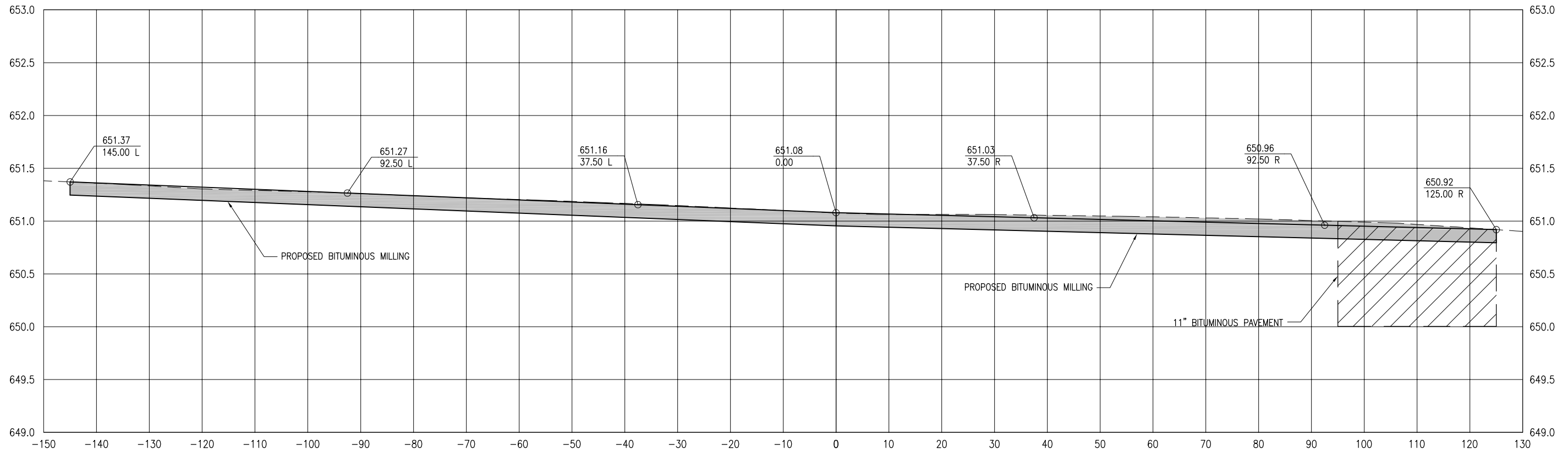
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CONSTRUCT CROSSWIND  
RUNWAY 18-36  
N-S RWY. & E-W RWY.  
INTERSECTION STA.  
119+08.30 - STA. 119+63.36

E-W RUNWAY EDGE  
STA. 120+38.34



E-W RUNWAY CENTERLINE  
STA. 120+01.04



REVISION	DATE

EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS  
A.I.P. PROJ.: 3-17-0077-B13  
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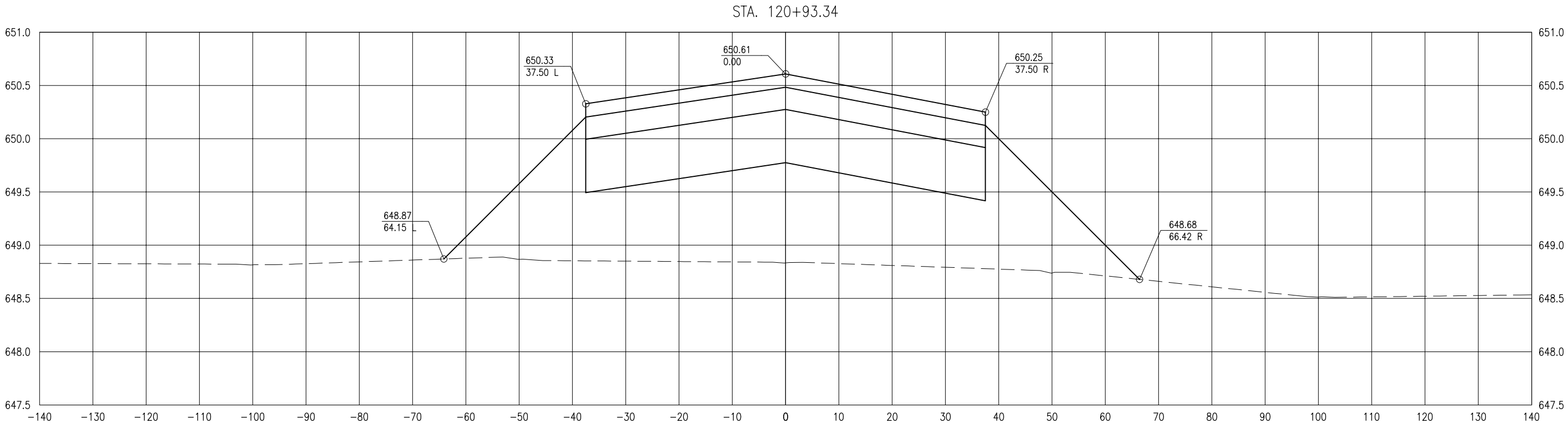
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	DPE	01/04/11	
	CAH	xx/xx/xx	
	REVIEWED		

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RUNWAY 18-36  
N-S RWY. & E-W RWY.  
INTERSECTION STA.  
120+01.04 - STA. 120+38.34

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DATE	REVISION

EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

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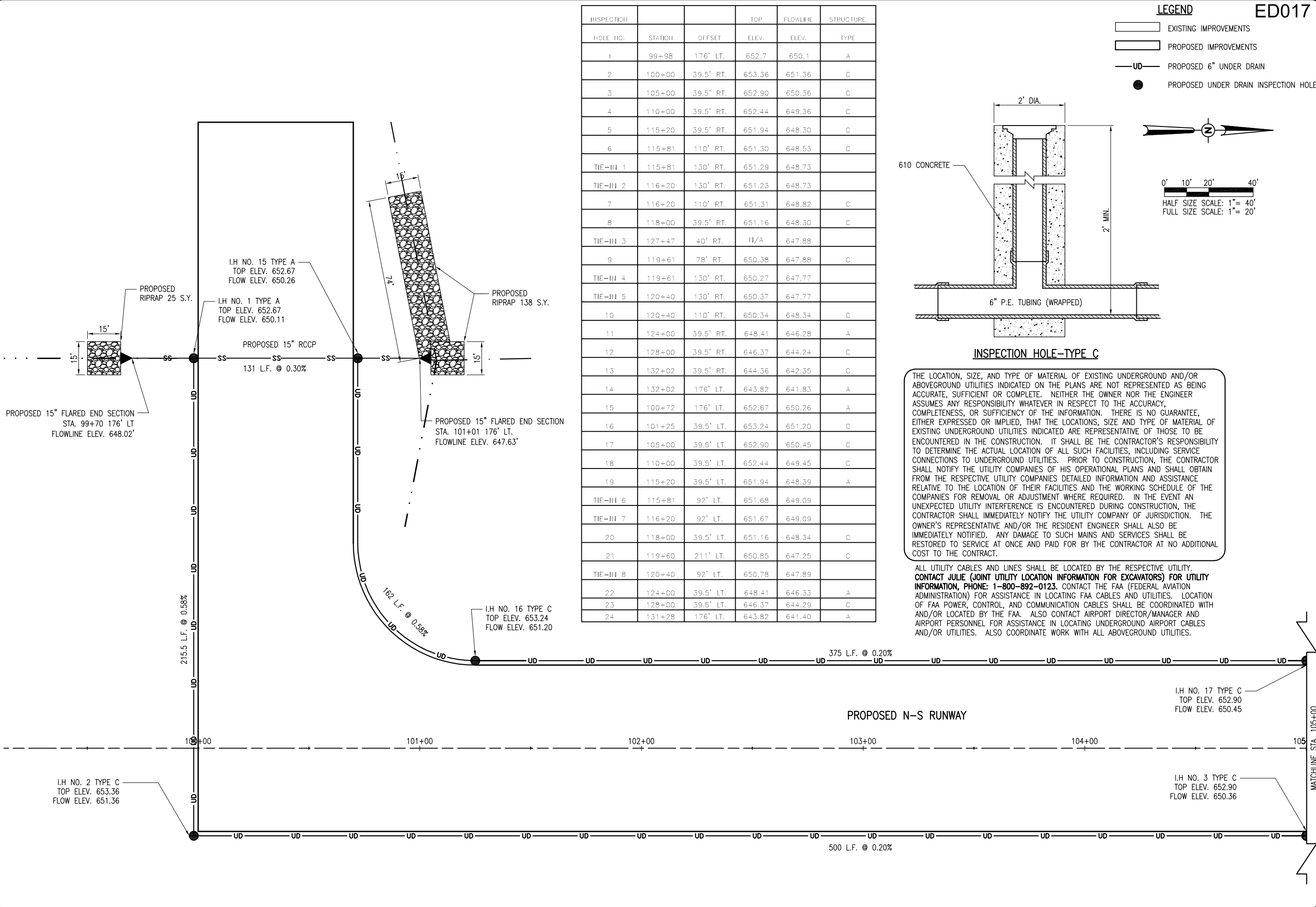
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DRAWN	DEP	01/04/11	
REVIEWED	CAH	xx/xx/xx	

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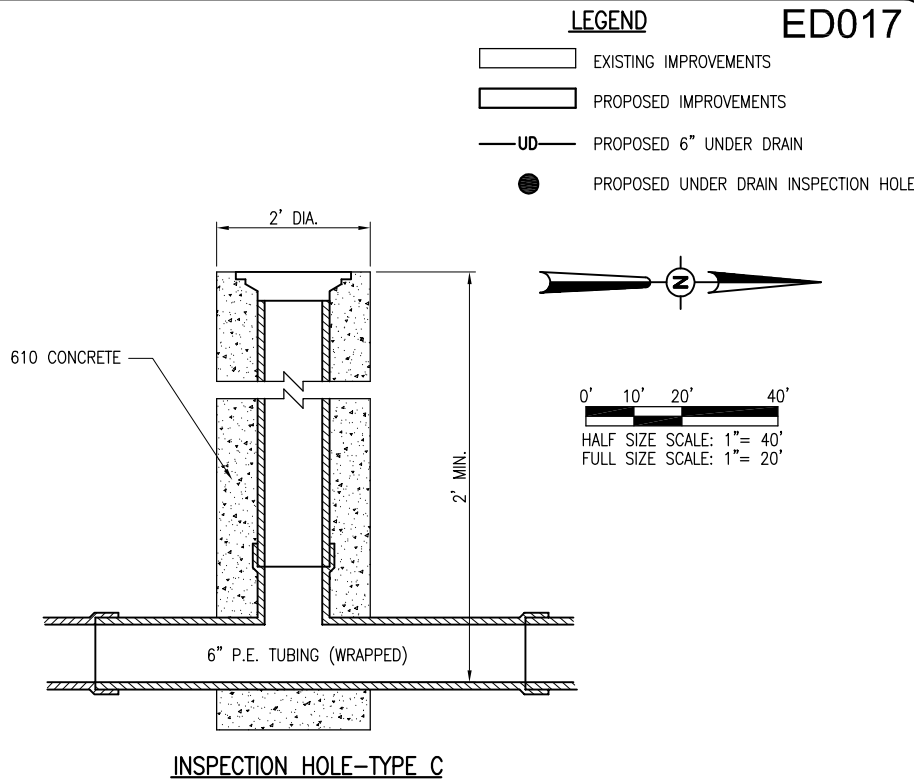
CONSTRUCT CROSSWIND  
RUNWAY 18-36

N-S RWY. & E-W RWY.  
INTERSECTION STA.  
120+93.34

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INSPECTION			TOP	FLOWLINE	STRUCTURE
HOLE NO.	STATION	OFFSET	ELEV.	ELEV.	TYPE
1	99+98	176' LT.	652.7	650.1	A
2	100+00	39.5' RT.	653.36	651.36	C
3	105+00	39.5' RT.	652.90	650.36	C
4	110+00	39.5' RT.	652.44	649.36	C
5	115+20	39.5' RT.	651.94	648.30	C
6	115+81	110' RT.	651.30	648.53	C
TIE-IN 1	115+81	130' RT.	651.29	648.73	
TIE-IN 2	116+20	130' RT.	651.23	648.73	
7	116+20	110' RT.	651.31	648.82	C
8	118+00	39.5' RT.	651.16	648.30	C
TIE-IN 3	127+47	40' RT.	N/A	647.88	
9	119+61	78' RT.	650.38	647.88	C
TIE-IN 4	119+61	130' RT.	650.27	647.77	
TIE-IN 5	120+40	130' RT.	650.37	647.77	
10	120+40	110' RT.	650.34	648.34	C
11	124+00	39.5' RT.	648.41	646.28	A
12	128+00	39.5' RT.	646.37	644.24	C
13	132+02	39.5' RT.	644.36	642.35	C
14	132+02	176' LT.	643.82	641.83	A
15	100+72	176' LT.	652.67	650.26	A
16	101+25	39.5' LT.	653.24	651.20	C
17	105+00	39.5' LT.	652.90	650.45	C
18	110+00	39.5' LT.	652.44	649.45	C
19	115+20	39.5' LT.	651.94	648.39	A
TIE-IN 6	115+81	92' LT.	651.68	649.09	
TIE-IN 7	116+20	92' LT.	651.67	649.09	
20	118+00	39.5' LT.	651.16	648.34	C
21	119+60	211' LT.	650.85	647.25	C
TIE-IN 8	120+40	92' LT.	650.78	647.89	
22	124+00	39.5' LT.	648.41	646.33	A
23	128+00	39.5' LT.	646.37	644.29	C
24	131+28	176' LT.	643.82	641.40	A

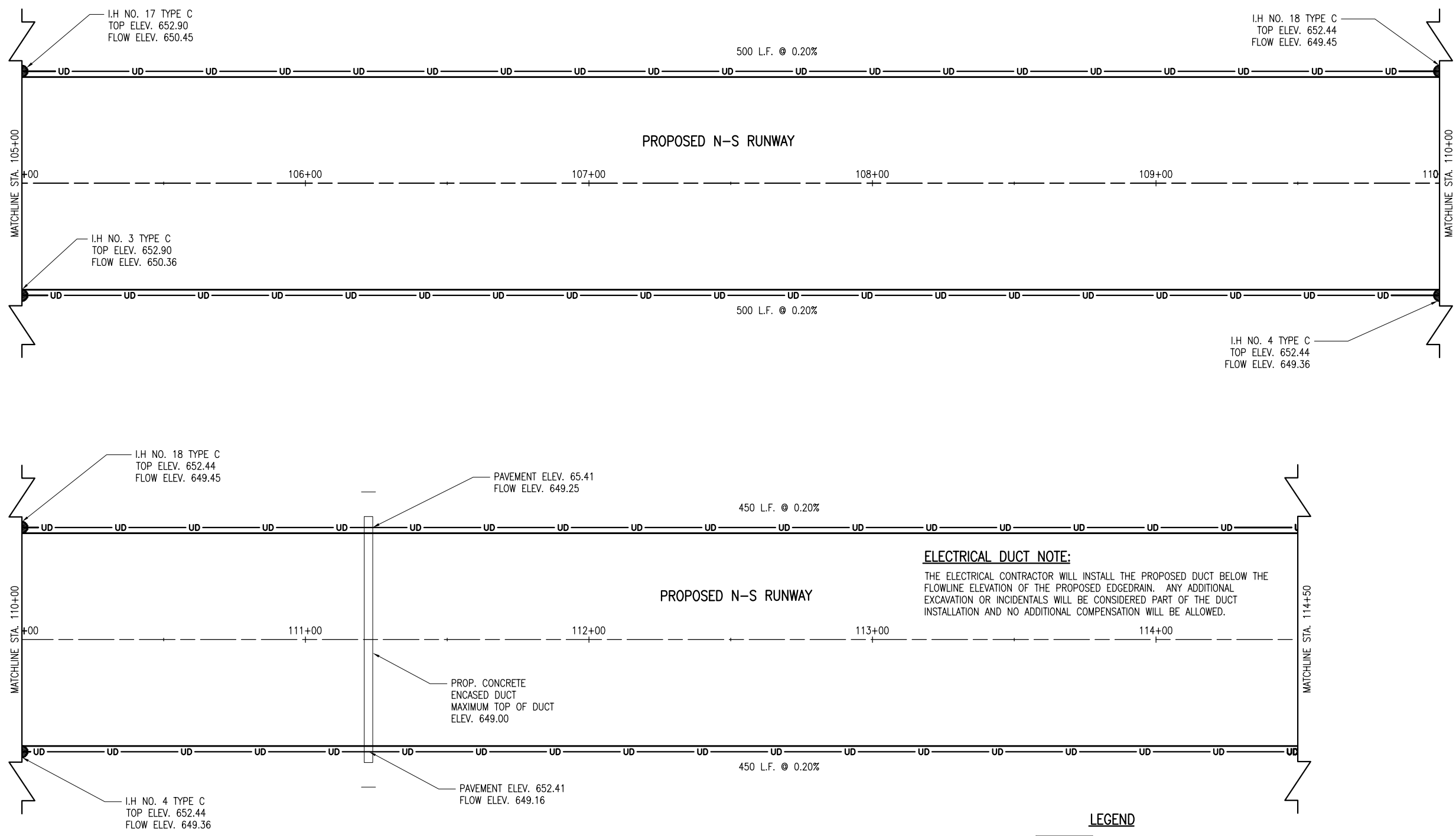


**INSPECTION HOLE-TYPE C**

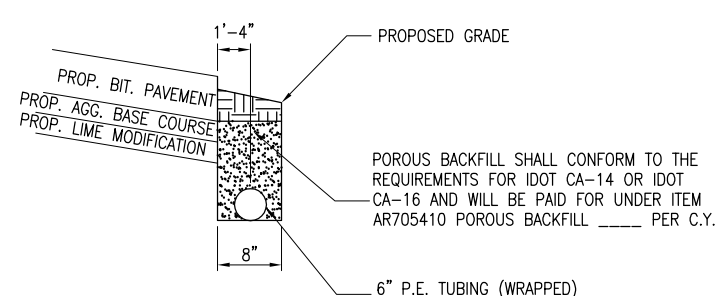
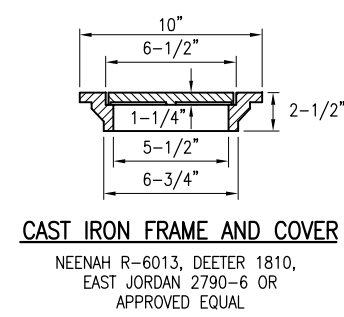
THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123. CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

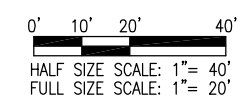
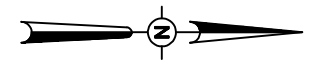
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EDGAR COUNTY AIRPORT PARIS, ILLINOIS																									
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<p>CONSTRUCT CROSSWIND RUNWAY 18-36</p> <p>PROPOSED DRAINAGE PLAN STA. 100+00 TO STA. 105+00</p>																									
<p>27</p> <p>27 of 137 sheets</p>																									



**ELECTRICAL DUCT NOTE:**  
 THE ELECTRICAL CONTRACTOR WILL INSTALL THE PROPOSED DUCT BELOW THE FLOWLINE ELEVATION OF THE PROPOSED EDGEDRAIN. ANY ADDITIONAL EXCAVATION OR INCIDENTALS WILL BE CONSIDERED PART OF THE DUCT INSTALLATION AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



- LEGEND**
- EXISTING IMPROVEMENTS
  - PROPOSED IMPROVEMENTS
  - PROPOSED 6" UNDER DRAIN
  - PROPOSED UNDER DRAIN INSPECTION HOLE



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**CONSTRUCT CROSSWIND  
 RUNWAY 18-36**  
 PROPOSED DRAINAGE PLAN  
 STA. 105+00 TO STA. 115+00



**705-UNDERDRAIN NOTES:**

THE PROPOSED UNDERDRAIN PIPE WILL BE CONSTRUCTED IN ACCORDANCE WITH ITEM 705 "PIPE UNDERDRAINS FOR AIRPORTS" AS STATED ON PAGE 310 OF THE ILLINOIS STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 2, 2009.

THIS ITEM OF WORK SHALL CONSIST OF FURNISHING AND INSTALLING 6" P.E. TUBING (WRAPPED) AND UNDERDRAIN INSPECTION HOLES AT THE LOCATIONS AND TO THE GRADES SHOWN ON THE CONSTRUCTION PLANS.

705-3.3 LAYING AND INSTALLING PIPE. REVISE THIS SECTION AS FOLLOWS:

"PIPE DRAINS SHALL BE INSTALLED AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. THE PIPE SHALL BE BEDDED IN THE UNDERLYING MATERIAL TO A DEPTH NOT LESS THAN 10 PERCENT OF THE EXTERNAL DIAMETER OF THE PIPE, AND WHERE TRENCHING IS REQUIRED, THE TRENCH SHALL HAVE A WIDTH OF NOT LESS 8 IN. THE BOTTOM OF THE TRENCH SHALL BE COMPACTED IN A MANNER MEETING THE APPROVAL OF THE ENGINEER.

JOINTS AND FITTINGS MAY BE ASSEMBLED WITHOUT GASKETS OR SOLVENT CEMENT IF THE JOINT IS SAND TIGHT AND THE SPIGOT ENTERS THE SOCKET NOT LESS THAN 1/3 OF THE SOCKET DEPTH FOR SOLVENT CEMENT JOINTS AND FULL-DEPTH FOR ELASTOMERIC GASKET JOINTS.

NO PIPE SHALL BE PLACED IN THE TRENCH UNTIL IT AND THE PREPARED FOUNDATION HAVE BEEN APPROVED BY THE RESIDENT ENGINEER. THE PIPE SHALL BE LAID SO THAT THE FLOWLINE WILL BE AT THE GRADE SHOWN ON THE PLANS OR ESTABLISHED BY THE RESIDENT ENGINEER. THE PERMISSIBLE MINIMUM COVER OVER A PIPE SHALL BE 12 IN.

LAYING OF PIPES SHALL COMMENCE AT THE OUTLET END AND PROCEED TOWARD THE INLET END WITH THE PIPES TRUE TO LINE AND GRADE.

THE ENDS OF THE PIPE SHALL BE CAREFULLY CLEANED BEFORE THEY ARE PLACED, AND SHALL BE PLACED TO AVOID UNNECESSARY HANDLING ON THE FOUNDATION. AS EACH LENGTH OF PIPE IS LAID, THE ENDS OF THE PIPE SHALL BE PROTECTED TO PREVENT THE ENTRANCE OF ANY MATERIAL.

LONGITUDINAL LAPS SHALL BE PLACED AT THE SIDES AND SEPARATE SECTIONS OF PIPE SHALL BE JOINED WITH TIGHTLY-DRAWN, APPROVED CONNECTING BANDS.

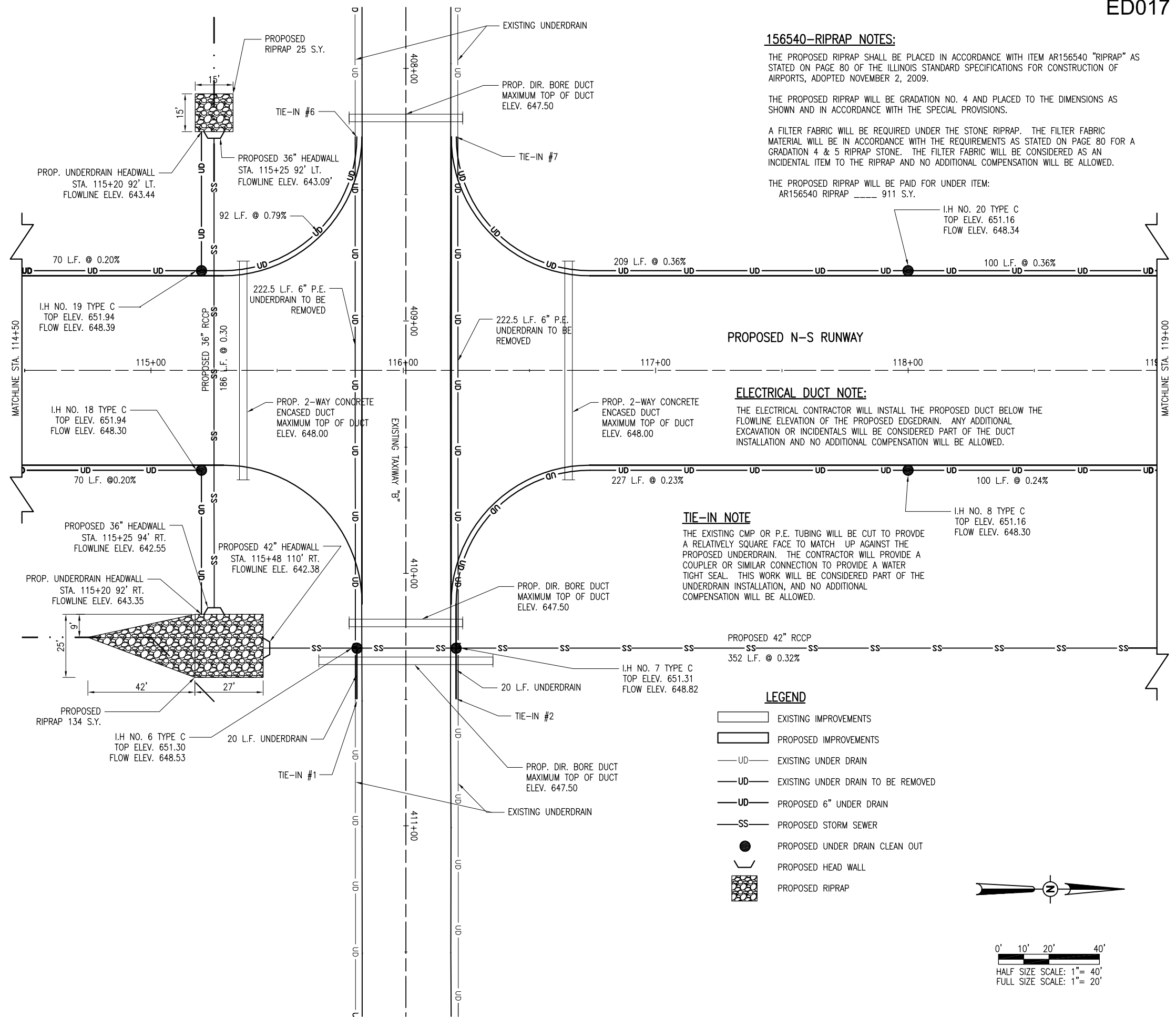
705-3.6 BACKFILLING; ADD THE FOLLOWING TO THIS SECTION:

"THE EDGE DRAIN TRENCH WILL BE BACKFILLED WITH CONSOLIDATED POROUS BACKFILL NO. 1 IN ACCORDANCE WITH THE DETAIL ON THE CONSTRUCTION PLANS. THE POROUS BACKFILL WILL BE COMPACTED IN ACCORDANCE WITH THE SPECIFICATION SET FORTH FOR GRANULAR MATERIAL BACKFILL."

THE PROPOSED UNDERDRAIN PIPE WILL BE PAID FOR UNDER ITEMS:  
 AR70541? POROUS BACKFILL NO. 1 \_\_\_\_\_ 370 C.Y.  
 AR705526 6" PERFORATED UNDERDRAIN W/SOCK \_\_\_\_\_ 7,198 L.F.  
 AR705530 UNDERDRAIN INSPECTION HOLE \_\_\_\_\_ 24 EACH  
 AR705900 REMOVE UNDERDRAIN \_\_\_\_\_ 1,009 L.F.

**UNDERDRAIN HEADWALL NOTE**

THE PROPOSED EDGEDRAIN LATERAL RUNS AT STA 115+20 WILL REQUIRE SMALL CONCRETE HEADWALL STRUCTURES. THESE STRUCTURES WILL BE IN ACCORDANCE TO ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD 601101. THESE TWO STRUCTURES WILL BE CONSIDERED PART OF THE "6" PERFORATED UNDERDRAIN W/ SOCK" PAY ITEM AND NO OTHER COMPENSATION WILL BE ALLOWED.



**156540-RIPRAP NOTES:**

THE PROPOSED RIPRAP SHALL BE PLACED IN ACCORDANCE WITH ITEM AR156540 "RIPRAP" AS STATED ON PAGE 80 OF THE ILLINOIS STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 2, 2009.

THE PROPOSED RIPRAP WILL BE GRADATION NO. 4 AND PLACED TO THE DIMENSIONS AS SHOWN AND IN ACCORDANCE WITH THE SPECIAL PROVISIONS.

A FILTER FABRIC WILL BE REQUIRED UNDER THE STONE RIPRAP. THE FILTER FABRIC MATERIAL WILL BE IN ACCORDANCE WITH THE REQUIREMENTS AS STATED ON PAGE 80 FOR A GRADATION 4 & 5 RIPRAP STONE. THE FILTER FABRIC WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE RIPRAP AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE PROPOSED RIPRAP WILL BE PAID FOR UNDER ITEM:  
 AR156540 RIPRAP \_\_\_\_\_ 911 S.Y.

**ELECTRICAL DUCT NOTE:**

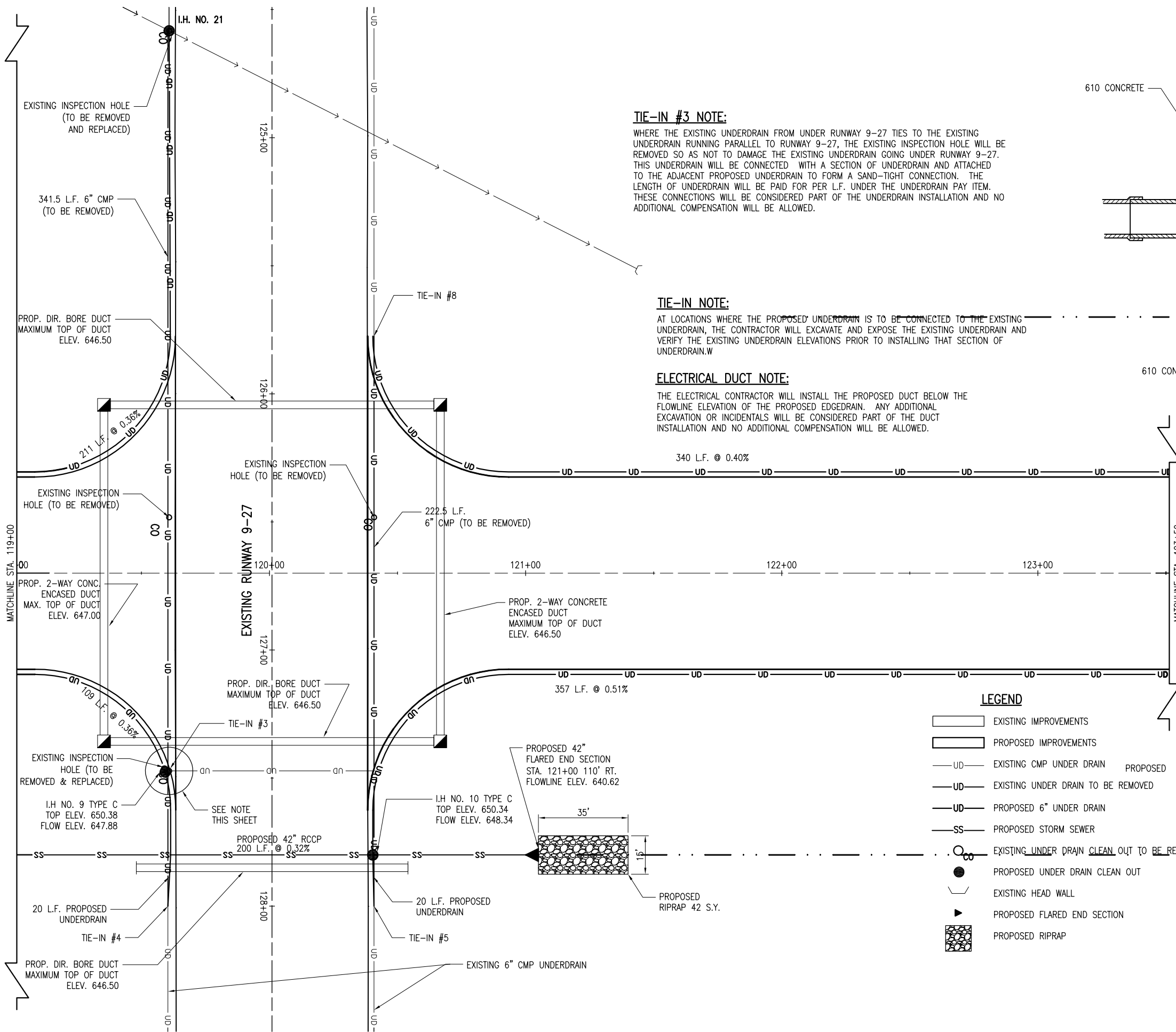
THE ELECTRICAL CONTRACTOR WILL INSTALL THE PROPOSED DUCT BELOW THE FLOWLINE ELEVATION OF THE PROPOSED EDGEDRAIN. ANY ADDITIONAL EXCAVATION OR INCIDENTALS WILL BE CONSIDERED PART OF THE DUCT INSTALLATION AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

**TIE-IN NOTE**

THE EXISTING CMP OR P.E. TUBING WILL BE CUT TO PROVIDE A RELATIVELY SQUARE FACE TO MATCH UP AGAINST THE PROPOSED UNDERDRAIN. THE CONTRACTOR WILL PROVIDE A COUPLER OR SIMILAR CONNECTION TO PROVIDE A WATER TIGHT SEAL. THIS WORK WILL BE CONSIDERED PART OF THE UNDERDRAIN INSTALLATION, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

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CONSTRUCT CROSSWIND RUNWAY 18-36 PROPOSED DRAINAGE PLAN STA. 115+00 TO STA. 119+00				
<b>29</b> 29 of 137 sheets				

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**TIE-IN #3 NOTE:**

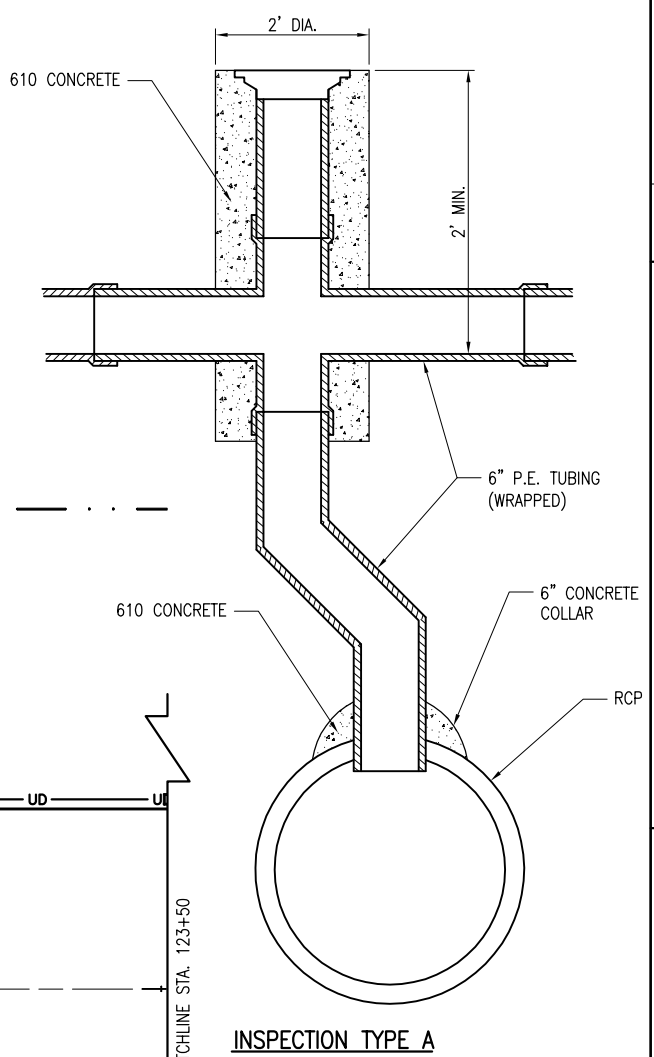
WHERE THE EXISTING UNDERDRAIN FROM UNDER RUNWAY 9-27 TIES TO THE EXISTING UNDERDRAIN RUNNING PARALLEL TO RUNWAY 9-27, THE EXISTING INSPECTION HOLE WILL BE REMOVED SO AS NOT TO DAMAGE THE EXISTING UNDERDRAIN GOING UNDER RUNWAY 9-27. THIS UNDERDRAIN WILL BE CONNECTED WITH A SECTION OF UNDERDRAIN AND ATTACHED TO THE ADJACENT PROPOSED UNDERDRAIN TO FORM A SAND-TIGHT CONNECTION. THE LENGTH OF UNDERDRAIN WILL BE PAID FOR PER L.F. UNDER THE UNDERDRAIN PAY ITEM. THESE CONNECTIONS WILL BE CONSIDERED PART OF THE UNDERDRAIN INSTALLATION AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

**TIE-IN NOTE:**

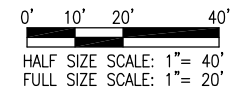
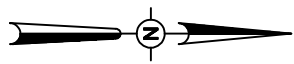
AT LOCATIONS WHERE THE PROPOSED UNDERDRAIN IS TO BE CONNECTED TO THE EXISTING UNDERDRAIN, THE CONTRACTOR WILL EXCAVATE AND EXPOSE THE EXISTING UNDERDRAIN AND VERIFY THE EXISTING UNDERDRAIN ELEVATIONS PRIOR TO INSTALLING THAT SECTION OF UNDERDRAIN.

**ELECTRICAL DUCT NOTE:**

THE ELECTRICAL CONTRACTOR WILL INSTALL THE PROPOSED DUCT BELOW THE FLOWLINE ELEVATION OF THE PROPOSED EDGEDRAIN. ANY ADDITIONAL EXCAVATION OR INCIDENTALS WILL BE CONSIDERED PART OF THE DUCT INSTALLATION AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



- LEGEND**
- EXISTING IMPROVEMENTS
  - PROPOSED IMPROVEMENTS
  - UD— EXISTING CMP UNDER DRAIN
  - UD— EXISTING UNDER DRAIN TO BE REMOVED
  - UD— PROPOSED 6" UNDER DRAIN
  - SS— PROPOSED STORM SEWER
  - CO EXISTING UNDER DRAIN CLEAN OUT TO BE REMOVED
  - PROPOSED UNDER DRAIN CLEAN OUT
  - EXISTING HEAD WALL
  - ▶ PROPOSED FLARED END SECTION
  - ▒ PROPOSED RIPRAP



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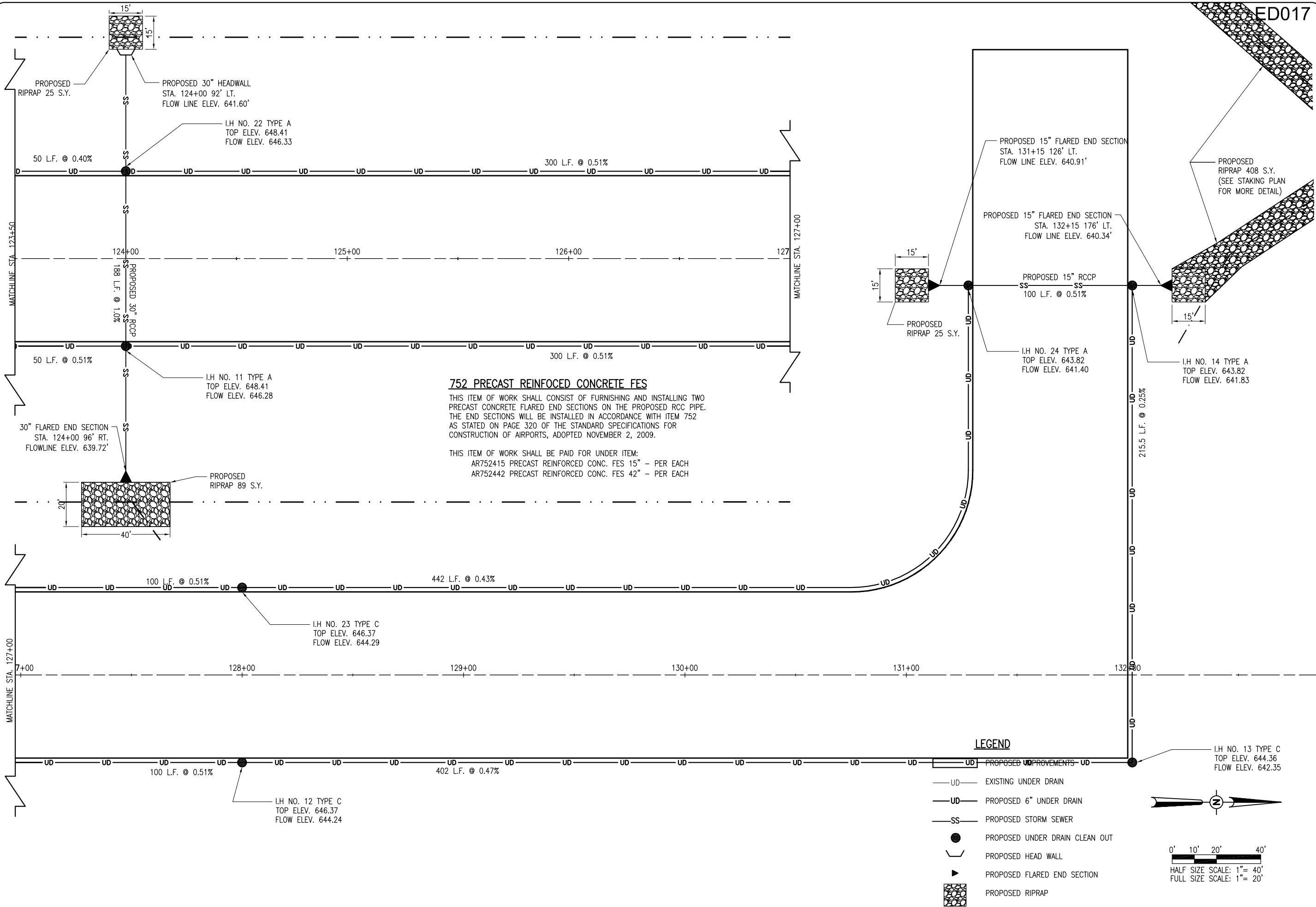
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RUNWAY 18-36

PROPOSED DRAINAGE PLAN  
STA. 119+00 TO STA. 123+00

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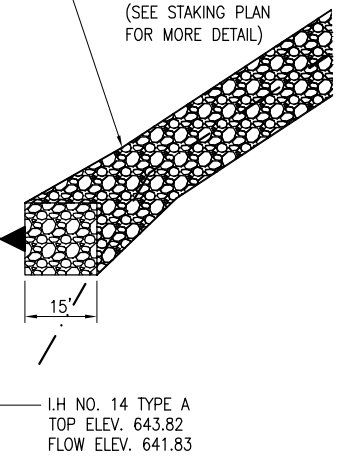
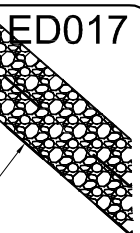
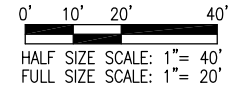
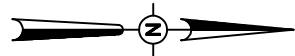
**752 PRECAST REINFORCED CONCRETE FES**

THIS ITEM OF WORK SHALL CONSIST OF FURNISHING AND INSTALLING TWO PRECAST CONCRETE FLARED END SECTIONS ON THE PROPOSED RCC PIPE. THE END SECTIONS WILL BE INSTALLED IN ACCORDANCE WITH ITEM 752 AS STATED ON PAGE 320 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 2, 2009.

THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM:  
 AR752415 PRECAST REINFORCED CONC. FES 15" - PER EACH  
 AR752442 PRECAST REINFORCED CONC. FES 42" - PER EACH

**LEGEND**

- UD — EXISTING UNDER DRAIN
- UD — PROPOSED 6" UNDER DRAIN
- SS — PROPOSED STORM SEWER
- PROPOSED UNDER DRAIN CLEAN OUT
- ⌋ PROPOSED HEAD WALL
- ▶ PROPOSED FLARED END SECTION
- ▨ PROPOSED RIPRAP



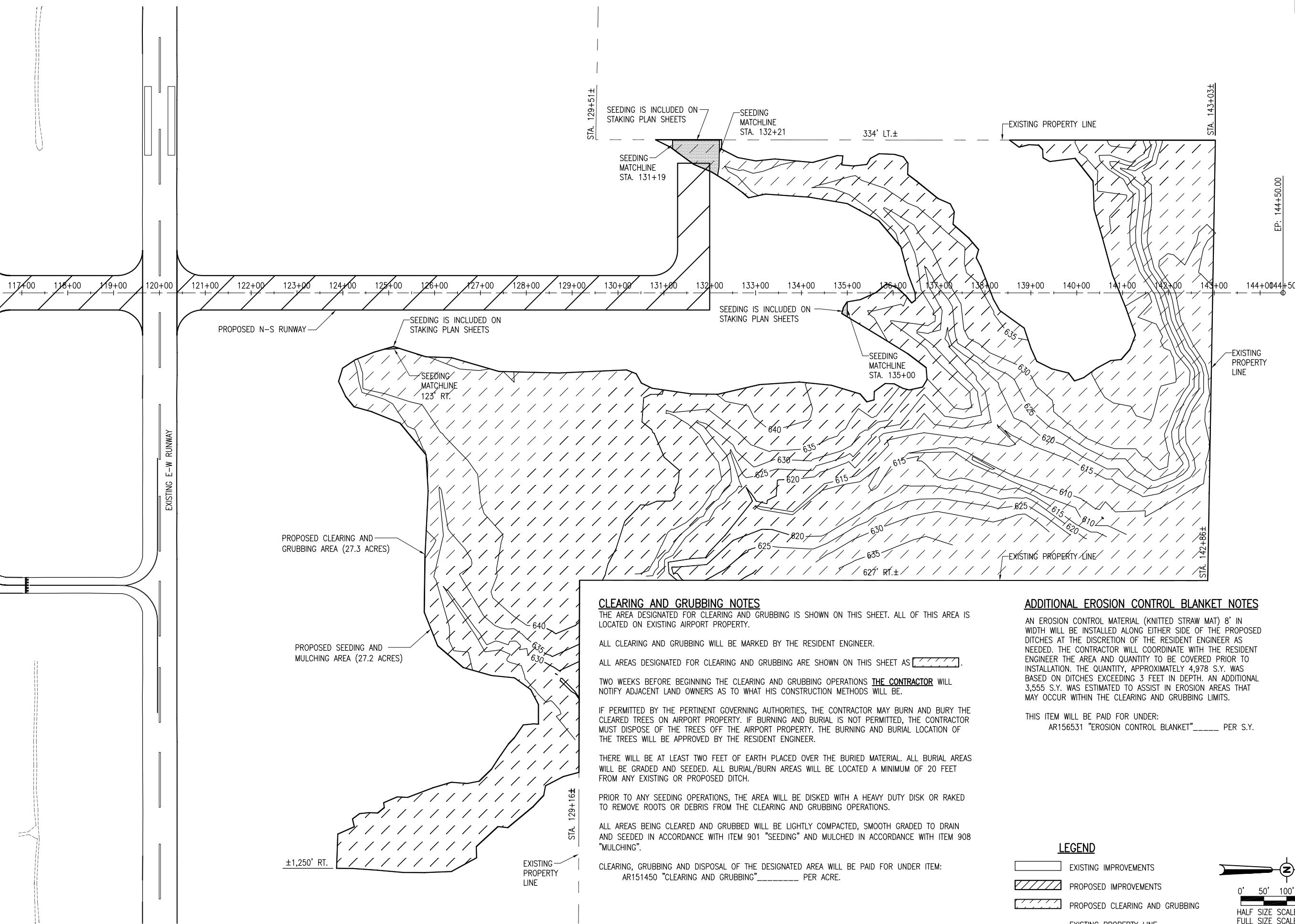
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 PROPOSED DRAINAGE PLAN  
 STA. 123+00 TO STA. 132+00



**CLEARING AND GRUBBING NOTES**

THE AREA DESIGNATED FOR CLEARING AND GRUBBING IS SHOWN ON THIS SHEET. ALL OF THIS AREA IS LOCATED ON EXISTING AIRPORT PROPERTY.

ALL CLEARING AND GRUBBING WILL BE MARKED BY THE RESIDENT ENGINEER.

ALL AREAS DESIGNATED FOR CLEARING AND GRUBBING ARE SHOWN ON THIS SHEET AS .

TWO WEEKS BEFORE BEGINNING THE CLEARING AND GRUBBING OPERATIONS **THE CONTRACTOR WILL** NOTIFY ADJACENT LAND OWNERS AS TO WHAT HIS CONSTRUCTION METHODS WILL BE.

IF PERMITTED BY THE PERTINENT GOVERNING AUTHORITIES, THE CONTRACTOR MAY BURN AND BURY THE CLEARED TREES ON AIRPORT PROPERTY. IF BURNING AND BURIAL IS NOT PERMITTED, THE CONTRACTOR MUST DISPOSE OF THE TREES OFF THE AIRPORT PROPERTY. THE BURNING AND BURIAL LOCATION OF THE TREES WILL BE APPROVED BY THE RESIDENT ENGINEER.

THERE WILL BE AT LEAST TWO FEET OF EARTH PLACED OVER THE BURIED MATERIAL. ALL BURIAL AREAS WILL BE GRADED AND SEEDDED. ALL BURIAL/BURN AREAS WILL BE LOCATED A MINIMUM OF 20 FEET FROM ANY EXISTING OR PROPOSED DITCH.

PRIOR TO ANY SEEDING OPERATIONS, THE AREA WILL BE DISKED WITH A HEAVY DUTY DISK OR RAKED TO REMOVE ROOTS OR DEBRIS FROM THE CLEARING AND GRUBBING OPERATIONS.

ALL AREAS BEING CLEARED AND GRUBBED WILL BE LIGHTLY COMPACTED, SMOOTH GRADED TO DRAIN AND SEEDDED IN ACCORDANCE WITH ITEM 901 "SEEDING" AND MULCHED IN ACCORDANCE WITH ITEM 908 "MULCHING".

CLEARING, GRUBBING AND DISPOSAL OF THE DESIGNATED AREA WILL BE PAID FOR UNDER ITEM: AR151450 "CLEARING AND GRUBBING" \_\_\_\_\_ PER ACRE.

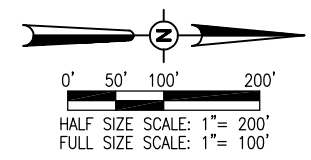
**ADDITIONAL EROSION CONTROL BLANKET NOTES**

AN EROSION CONTROL MATERIAL (KNITTED STRAW MAT) 8' IN WIDTH WILL BE INSTALLED ALONG EITHER SIDE OF THE PROPOSED DITCHES AT THE DISCRETION OF THE RESIDENT ENGINEER AS NEEDED. THE CONTRACTOR WILL COORDINATE WITH THE RESIDENT ENGINEER THE AREA AND QUANTITY TO BE COVERED PRIOR TO INSTALLATION. THE QUANTITY, APPROXIMATELY 4,978 S.Y. WAS BASED ON DITCHES EXCEEDING 3 FEET IN DEPTH. AN ADDITIONAL 3,555 S.Y. WAS ESTIMATED TO ASSIST IN EROSION AREAS THAT MAY OCCUR WITHIN THE CLEARING AND GRUBBING LIMITS.

THIS ITEM WILL BE PAID FOR UNDER:  
AR156531 "EROSION CONTROL BLANKET" \_\_\_\_\_ PER S.Y.

**LEGEND**

- EXISTING IMPROVEMENTS
- PROPOSED IMPROVEMENTS
- PROPOSED CLEARING AND GRUBBING
- EXISTING PROPERTY LINE



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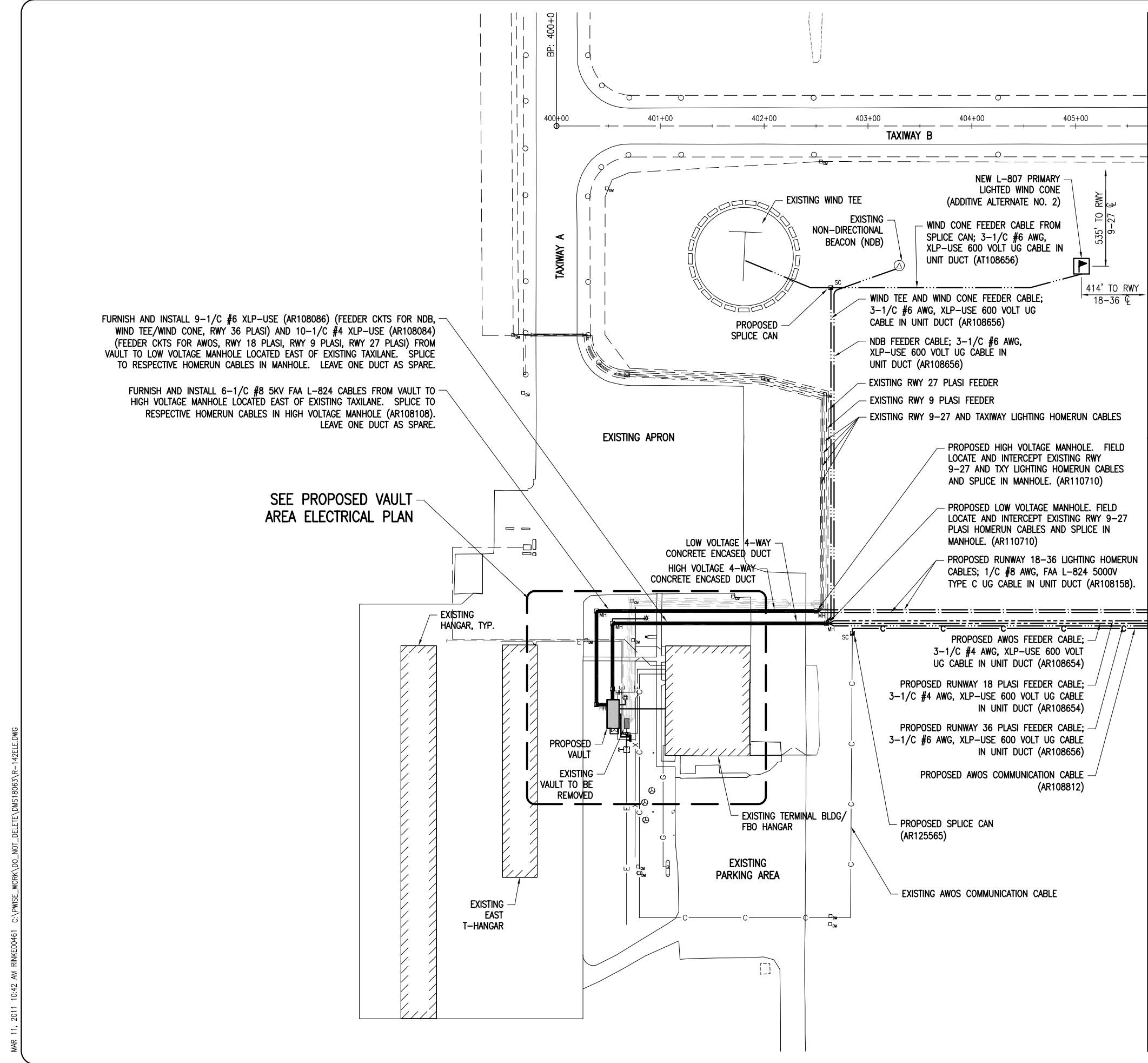
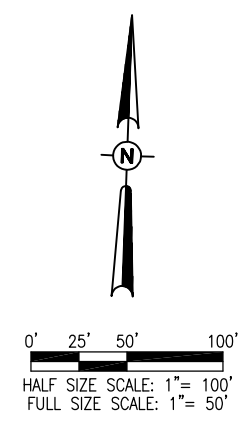
PROPOSED  
CLEARING PLAN

LEGEND

- EXISTING PAVEMENT
- PROPOSED PAVEMENT
- EXISTING ELECTRICAL DUCT
- PROPOSED ELECTRICAL DUCT
- EXISTING ELECTRICAL CABLES
- PROPOSED 1/C #8 AWG, FAA L-824, 5000 VOLT TYPE C UG CABLE IN UNIT DUCT
- PROPOSED 2-1/C #4 AWG, XLP-USE 600 VOLT UG CABLE IN UNIT DUCT
- PROPOSED 3-1/C #6 AWG, XLP-USE 600 VOLT UG CABLE IN UNIT DUCT
- PROPOSED 3-1/C #4 AWG, XLP-USE 600 VOLT UG CABLE IN UNIT DUCT
- PROPOSED 12 PAIR #24 AWG COMMUNICATIONS CABLE IN UNIT DUCT
- EXISTING AWOS COMMUNICATION CABLE
- EXISTING STAKE MOUNTED TAXIWAY LIGHT
- EXISTING BASE MOUNTED TAXIWAY LIGHT
- EXISTING ELECTRICAL STRUCTURE (HANDHOLE, SPLICE CAN)
- PROPOSED ELECTRICAL STRUCTURE (MANHOLE, HANDHOLE, SPLICE CAN)
- EXISTING ELECTRICAL MARKER (DUCT MARKER, CABLE MARKER)

NOTES:

1. HOMERUN CABLES FOR A RESPECTIVE CIRCUIT THAT ARE INSTALLED IN CONDUIT OR DUCT SHALL BE RUN TOGETHER IN THE SAME RACEWAY OR DUCT.
2. ANY AND ALL DISTURBED PAVEMENT AREAS WILL BE RESTORED TO ORIGINAL OR BETTER CONDITION. RESTORATION OF PAVEMENT AREAS DISTURBED DURING THE INSTALLATION OF THE PROPOSED DUCTS WILL BE INCIDENTAL TO THE RESPECTIVE PAY ITEM FOR WHICH THE DUCT IS INSTALLED. THE RESTORATION OF PAVEMENT WILL BE COMPLETED IN ACCORDANCE WITH ITEM 610 FOR SIDEWALKS AND CONCRETE PAVEMENT AND ITEM 401 FOR BITUMINOUS PAVEMENT, BUT WILL BE INCIDENTAL TO THE RESPECTIVE PAY ITEM FOR WHICH THE DUCT IS INSTALLED.



MATCHLINE SEE SHEET 35

MATCHLINE - SEE SHEET 34

FURNISH AND INSTALL 9-1/C #6 XLP-USE (AR108086) (FEEDER CKTS FOR NDB, WIND TEE/WIND CONE, RWY 36 PLASI) AND 10-1/C #4 XLP-USE (AR108084) (FEEDER CKTS FOR AWOS, RWY 18 PLASI, RWY 9 PLASI, RWY 27 PLASI) FROM VAULT TO LOW VOLTAGE MANHOLE LOCATED EAST OF EXISTING TAXILANE. SPLICE TO RESPECTIVE HOMERUN CABLES IN MANHOLE. LEAVE ONE DUCT AS SPARE.

FURNISH AND INSTALL 6-1/C #8 5KV FAA L-824 CABLES FROM VAULT TO HIGH VOLTAGE MANHOLE LOCATED EAST OF EXISTING TAXILANE. SPLICE TO RESPECTIVE HOMERUN CABLES IN HIGH VOLTAGE MANHOLE (AR108108). LEAVE ONE DUCT AS SPARE.

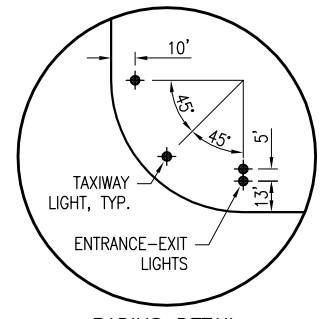
SEE PROPOSED VAULT AREA ELECTRICAL PLAN

REVISION		EDGAR COUNTY AIRPORT PARIS, ILLINOIS	DATE		A.I.P. PROJ.: 3-17-0077-B13	
					IL PROJ.: PRG-4018	
Hanson Proj. No. 10A0053 Filename R-142ELE.DWG Scale 1" = 50' Date 03/10/11		LAYOUT KNL/MLH 01/04/11 DRAWN MLH 01/04/11 REVIEWED CAH 02/04/11	Hanson Professional Services Inc. 1526 South Sixth Street Springfield, Illinois 62703-2886 www.hanson-inc.com Offices Nationwide			
CONSTRUCT CROSSWIND RUNWAY 18-36		PROPOSED TERMINAL AREA ELECTRICAL PLAN	<span style="font-size: 2em; font-weight: bold;">33</span> <small>33 of 137 sheets</small>			

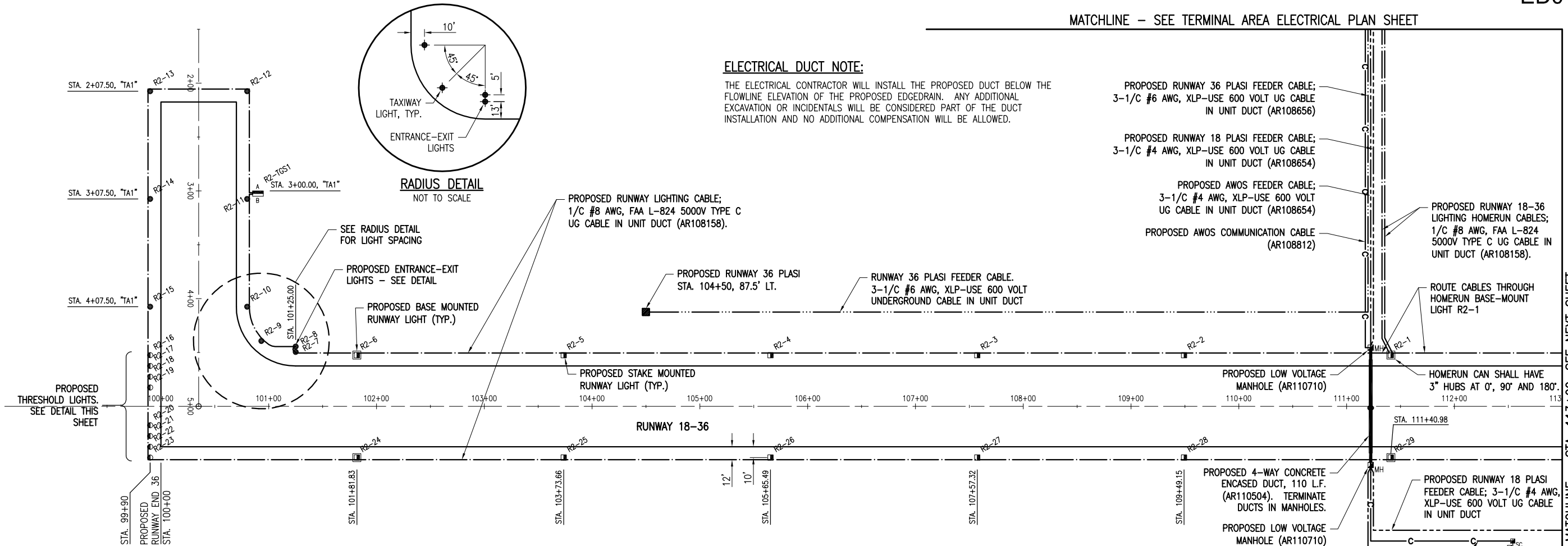
MATCHLINE - SEE TERMINAL AREA ELECTRICAL PLAN SHEET

ELECTRICAL DUCT NOTE:

THE ELECTRICAL CONTRACTOR WILL INSTALL THE PROPOSED DUCT BELOW THE FLOWLINE ELEVATION OF THE PROPOSED EDGEDRAIN. ANY ADDITIONAL EXCAVATION OR INCIDENTALS WILL BE CONSIDERED PART OF THE DUCT INSTALLATION AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



RADIUS DETAIL NOT TO SCALE

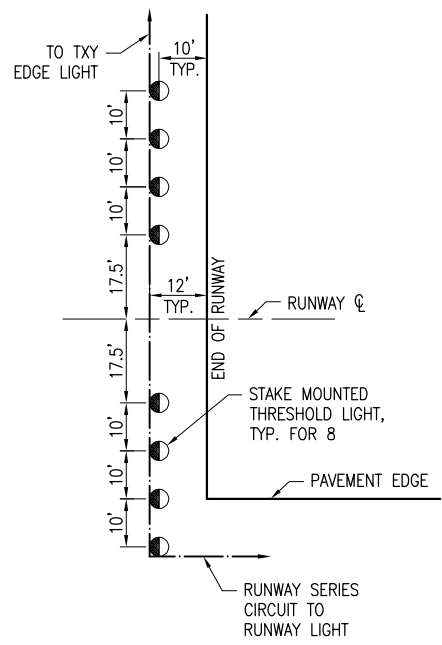


AIRFIELD LIGHTING NOTES

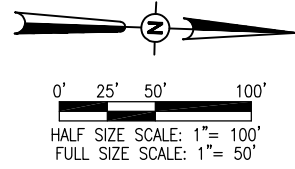
- 1. ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT DIRECTOR/MANAGER. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).

AIRFIELD LIGHTING NOTES (CONT.)

- 9. ALL PROPOSED RUNWAY, THRESHOLD, AND TAXIWAY LIGHTS SHALL BE TAGGED BY THE CONTRACTOR IN ACCORDANCE WITH THE LIGHT NUMBERS SHOWN ON THESE CONSTRUCTION DRAWINGS.
- 10. SEE "TAXI GUIDANCE SIGN SCHEDULE" FOR INFO ON SIGN LEGENDS.
- 11. THE CONTRACTOR SHALL SECURE, IDENTIFY AND PLACE ALL TEMPORARY EXPOSED WIRING IN CONDUIT, DUCT OR UNIT DUCT TO PREVENT ELECTROCUTION AND FIRE IGNITION SOURCES AS PER THE REQUIREMENTS OF FAA AC 150/5370-2E, PART 3-6, C. ALL LABOR, MATERIALS, AND TIME NECESSARY TO COMPLY WITH THIS REQUIREMENT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



PROPOSED RUNWAY END 36 THRESHOLD LIGHT DETAIL NOT TO SCALE



LEGEND

- EXISTING PAVEMENT (dashed line)
- PROPOSED PAVEMENT (solid line)
- EXISTING ELECTRICAL DUCT (dashed line with dots)
- PROPOSED ELECTRICAL DUCT (solid line with dots)
- EXISTING ELECTRICAL CABLES (dashed line)
- PROPOSED 1/C #8 AWG, FAA L-824, 5000 VOLT TYPE C UG CABLE IN UNIT DUCT (solid line with dots)
- PROPOSED 2-1/C #4 AWG, XLP-USE 600 VOLT UG CABLE IN UNIT DUCT (dashed line with dots)
- PROPOSED 3-1/C #6 AWG, XLP-USE 600 VOLT UG CABLE IN UNIT DUCT (solid line with dots)
- PROPOSED 3-1/C #4 AWG, XLP-USE 600 VOLT UG CABLE IN UNIT DUCT (dashed line with dots)
- PROPOSED 12 PAIR #24 AWG COMMUNICATIONS CABLE IN UNIT DUCT (solid line with dots)
- EXISTING AWOS COMMUNICATION CABLE (dashed line with dots)
- EXISTING STAKE MOUNTED TAXIWAY LIGHT (circle with dot)
- EXISTING BASE MOUNTED TAXIWAY LIGHT (square with dot)
- PROPOSED STAKE MOUNTED TAXIWAY LIGHT (circle with dot)
- PROPOSED BASE MOUNTED TAXIWAY LIGHT (square with dot)
- EXISTING STAKE MOUNTED RUNWAY LIGHT (circle with dot)
- EXISTING BASE MOUNTED RUNWAY LIGHT (square with dot)
- PROPOSED STAKE MOUNTED RUNWAY LIGHT (circle with dot)
- PROPOSED BASE MOUNTED RUNWAY LIGHT (square with dot)
- PROPOSED STAKE MOUNTED THRESHOLD LIGHT (circle with dot)
- PROPOSED TAXI GUIDANCE SIGN (square with dot)
- EXISTING ELECTRICAL STRUCTURE (HANDHOLE, SPICE CAN) (square with dot)
- PROPOSED ELECTRICAL STRUCTURE (MANHOLE, HANDHOLE, SPICE CAN) (square with dot)
- EXISTING ELECTRICAL MARKER (DUCT MARKER, CABLE MARKER) (square with dot)

MATCHLINE - STA. 113+00, SEE NEXT SHEET

EDGAR COUNTY AIRPORT PARIS, ILLINOIS

REVISION	
DATE	

IL PROJ.: PRG-4018 A.I.P. PROJ.: 3-17-0077-B13

Hanson Proj. No. 10A00053	Filename R-142ELE.DWG	Scale 1" = 50'	Date 03/10/11
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CONSTRUCT CROSSWIND RUNWAY 18-36

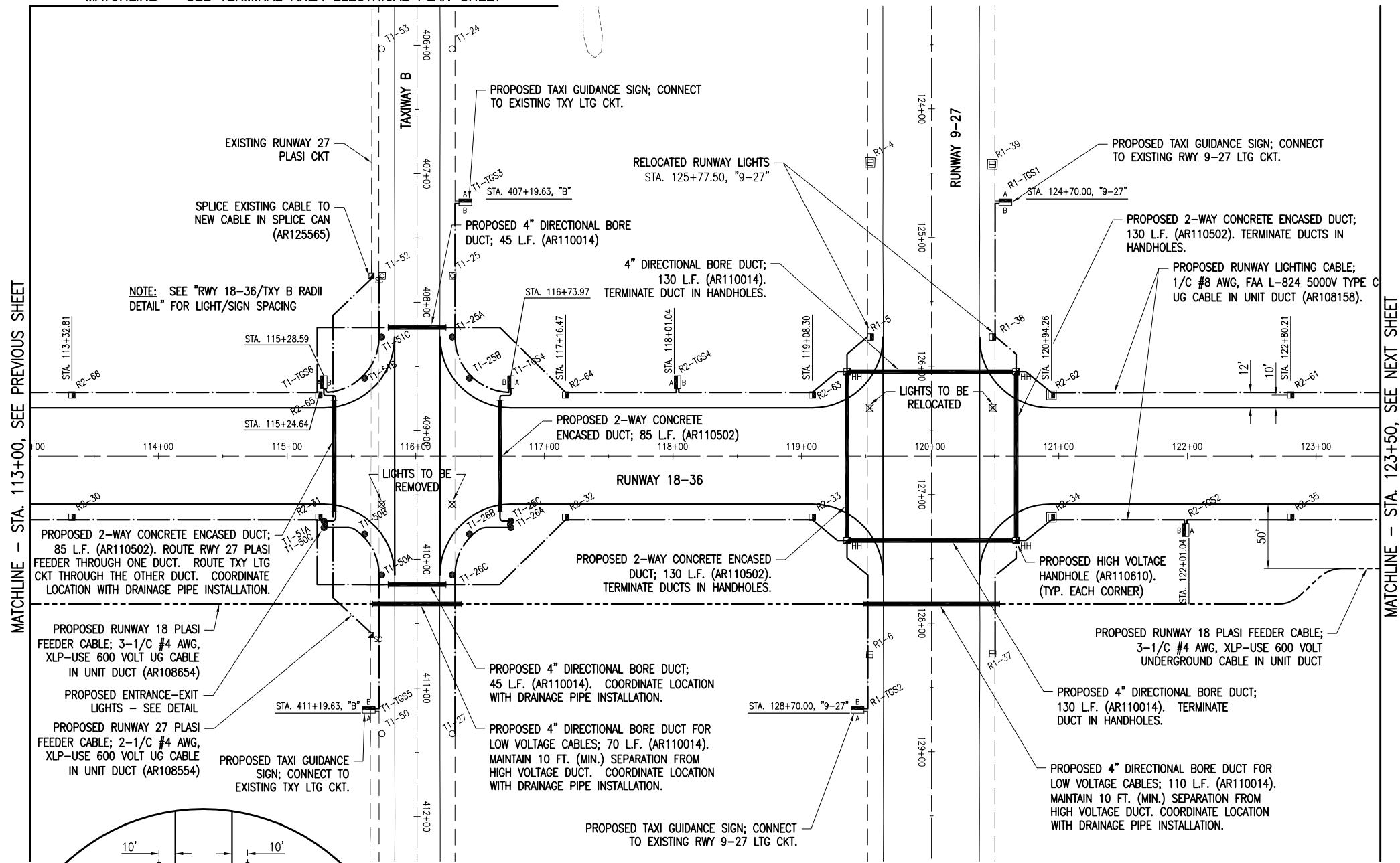
PROPOSED ELECTRICAL PLAN STA. 98+50 TO STA. 113+00

MAR 11, 2011 10:43 AM RINMED0461 C:\PWISE\_WORK\DO\_NOT\_DELETE\DM18063\RP-142ELE.DWG

MATCHLINE - SEE TERMINAL AREA ELECTRICAL PLAN SHEET

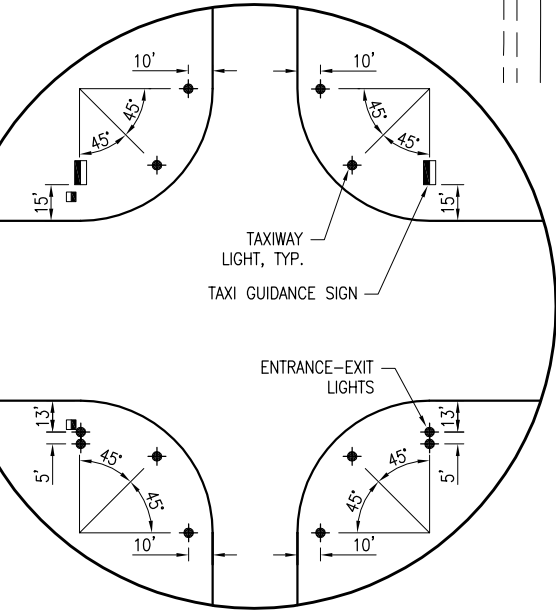
LEGEND

- EXISTING PAVEMENT
- PROPOSED PAVEMENT
- EXISTING ELECTRICAL DUCT
- PROPOSED ELECTRICAL DUCT
- EXISTING ELECTRICAL CABLES
- PROPOSED 1/C #8 AWG, FAA L-824, 5000 VOLT TYPE C UG CABLE IN UNIT DUCT
- PROPOSED 2-1/C #4 AWG, XLP-USE 600 VOLT UG CABLE IN UNIT DUCT
- PROPOSED 3-1/C #6 AWG, XLP-USE 600 VOLT UG CABLE IN UNIT DUCT
- PROPOSED 3-1/C #4 AWG, XLP-USE 600 VOLT UG CABLE IN UNIT DUCT
- PROPOSED 12 PAIR #24 AWG COMMUNICATIONS CABLE IN UNIT DUCT
- EXISTING AWOS COMMUNICATION CABLE
- EXISTING STAKE MOUNTED TAXIWAY LIGHT
- EXISTING BASE MOUNTED TAXIWAY LIGHT
- PROPOSED STAKE MOUNTED TAXIWAY LIGHT
- PROPOSED BASE MOUNTED TAXIWAY LIGHT
- EXISTING STAKE MOUNTED RUNWAY LIGHT
- EXISTING BASE MOUNTED RUNWAY LIGHT
- PROPOSED STAKE MOUNTED RUNWAY LIGHT
- PROPOSED BASE MOUNTED RUNWAY LIGHT
- PROPOSED STAKE MOUNTED THRESHOLD LIGHT
- PROPOSED TAXI GUIDANCE SIGN
- EXISTING ELECTRICAL STRUCTURE (HANDHOLE, SPLICE CAN)
- PROPOSED ELECTRICAL STRUCTURE (MANHOLE, HANDHOLE, SPLICE CAN)
- EXISTING ELECTRICAL MARKER (DUCT MARKER, CABLE MARKER)



MATCHLINE - STA. 113+00, SEE PREVIOUS SHEET

MATCHLINE - STA. 123+50, SEE NEXT SHEET

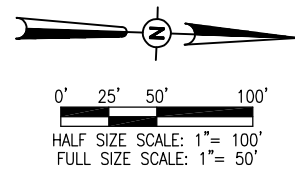


RWY 18-36/TXY B RADII DETAIL  
NOT TO SCALE

LIGHT LENS SCHEDULE		
LIGHT NUMBERS	LENS	ORIENTATION
R2-1 TO R2-6	CLEAR WHITE/AMBER	AMBER SIDE FACING NORTH
R2-7 TO R2-15	BLUE	---
R2-16 TO R2-23	RED/GREEN	RED SIDE FACING NORTH (TOWARDS THRESHOLD)
R2-24 TO R2-31	CLEAR WHITE/AMBER	AMBER SIDE FACING NORTH
R2-32 TO R2-39	CLEAR WHITE/AMBER	AMBER SIDE FACING SOUTH
R2-40 TO R2-47	RED/GREEN	RED SIDE FACING SOUTH (TOWARDS THRESHOLD)
R2-48 TO R2-56	BLUE	---
R2-57 TO R2-64	CLEAR WHITE/AMBER	AMBER SIDE FACING SOUTH
R2-65 TO R2-66	CLEAR WHITE/AMBER	AMBER SIDE FACING NORTH
T1-25A TO T1-26C	BLUE	---
T1-50A TO T1-51C	BLUE	---

ELECTRICAL DUCT NOTE:

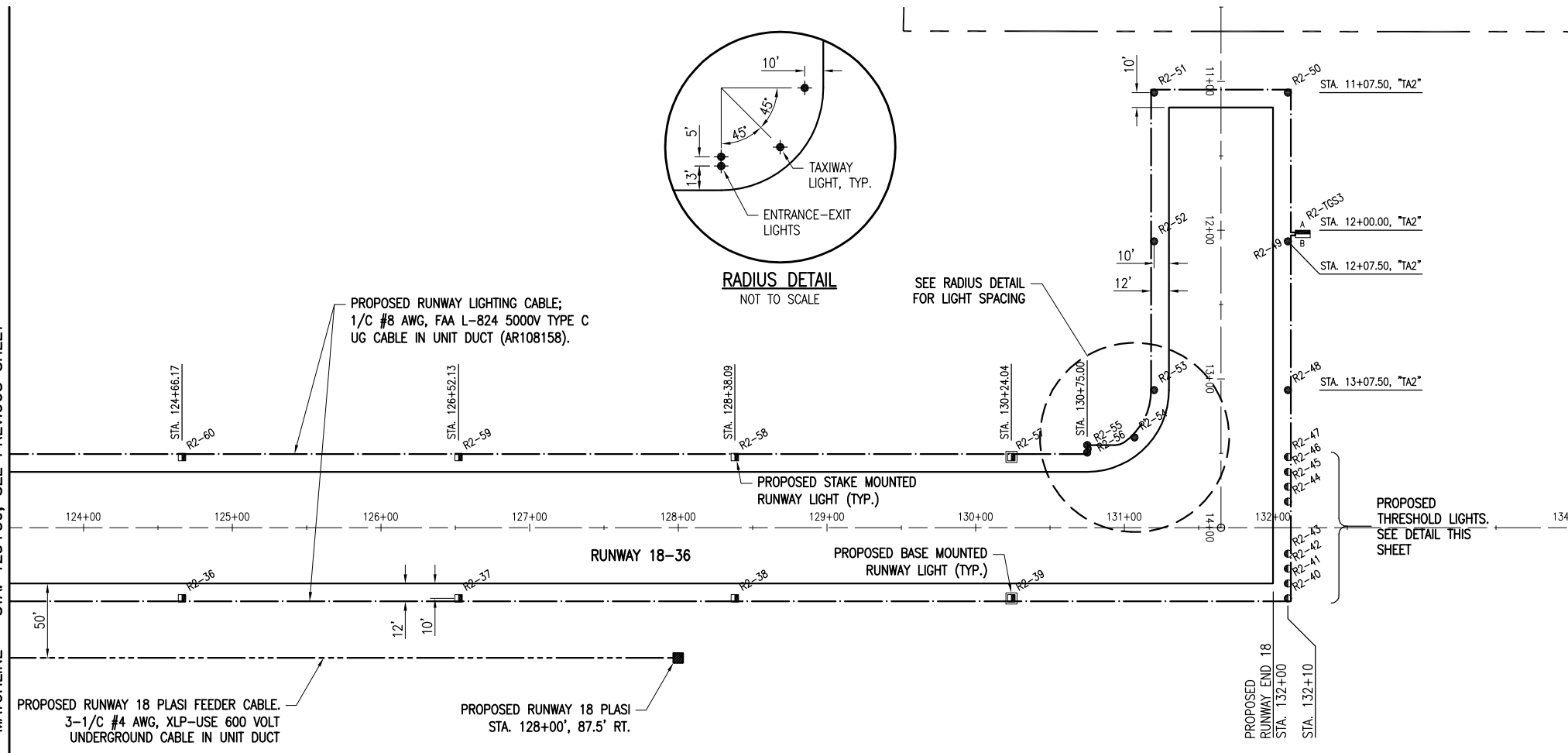
THE ELECTRICAL CONTRACTOR WILL INSTALL THE PROPOSED DUCT BELOW THE FLOWLINE ELEVATION OF THE PROPOSED EDGEDRAIN. ANY ADDITIONAL EXCAVATION OR INCIDENTALS WILL BE CONSIDERED PART OF THE DUCT INSTALLATION AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



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DATE			
		IL PROJ.: PRG-4018 IL PROJ.: 3-17-0077-B13	
Hanson Professional Services Inc. 1526 South Sixth Street Springfield, Illinois 62703-2886 www.hanson-inc.com Offices Nationwide		LAYOUT: KNL/MLH 01/04/11 DRAWN: MLH 01/04/11 REVIEWED: CAH 02/04/11	
CONSTRUCT CROSSWIND RUNWAY 18-36		PROPOSED ELECTRICAL PLAN STA. 113+00 TO STA. 123+50	
35		35 of 137 sheets	

MATCHLINE - STA. 123+50, SEE PREVIOUS SHEET

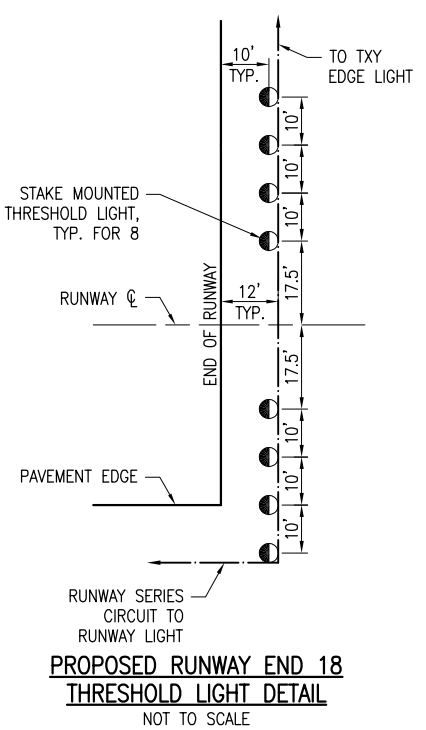


**LEGEND**

- EXISTING PAVEMENT
- PROPOSED PAVEMENT
- EXISTING ELECTRICAL DUCT
- PROPOSED ELECTRICAL DUCT
- EXISTING ELECTRICAL CABLES
- PROPOSED 1/C #8 AWG, FAA L-824, 5000 VOLT TYPE C UG CABLE IN UNIT DUCT
- PROPOSED 2-1/C #4 AWG, XLP-USE 600 VOLT UG CABLE IN UNIT DUCT
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- EXISTING STAKE MOUNTED TAXIWAY LIGHT
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- PROPOSED STAKE MOUNTED TAXIWAY LIGHT
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- PROPOSED ELECTRICAL STRUCTURE (MANHOLE, HANDHOLE, SPLICE CAN)
- EXISTING ELECTRICAL MARKER (DUCT MARKER, CABLE MARKER)

PROPOSED RUNWAY 18 PLASI FEEDER CABLE. 3-1/C #4 AWG, XLP-USE 600 VOLT UNDERGROUND CABLE IN UNIT DUCT

PROPOSED RUNWAY 18 PLASI STA. 128+00', 87.5' RT.



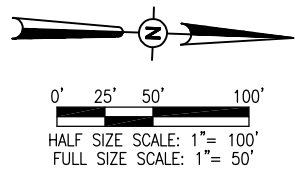
**PROPOSED RUNWAY END 18 THRESHOLD LIGHT DETAIL**  
NOT TO SCALE

TAXI GUIDANCE SIGN SCHEDULE			
SIGN NUMBERS	LOCATION	SIDE A	SIDE B
R1-TGS1	RWY 9-27 INTERSECTION WITH RWY 18-36 (WEST SIDE)	18-36	BLANK
R1-TGS2	RWY 9-27 INTERSECTION WITH RWY 36-18 (EAST SIDE)	36-18	BLANK
R2-TGS1	RWY END 36 TURNAROUND	36	BLANK
R2-TGS2	RWY 18-36 INTERSECTION WITH RWY 27-9 (NORTH SIDE)	27-9	BLANK
R2-TGS3	RWY END 18 TURNAROUND	18	BLANK
R2-TGS4	RWY 18-36 INTERSECTION WITH RWY 9-27 (SOUTH SIDE)	9-27	BLANK
T1-TGS3	TXY B INTERSECTION WITH RWY 18-36 (WEST SIDE)	B 18-36	BLANK
T1-TGS4	RWY 18-36 INTERSECTION WITH TXY B (NORTH SIDE)	RAMP →	BLANK
T1-TGS5	TXY B INTERSECTION WITH RWY 36-18 (EAST SIDE)	B 36-18	BLANK
T1-TGS6	RWY 18-36 INTERSECTION WITH TXY B (SOUTH SIDE)	← RAMP	BLANK

**TAXI GUIDANCE SIGN NOTES**

1. THE PROPOSED TAXI GUIDANCE SIGNS SHALL CONFORM TO ADVISORY CIRCULAR 150/5345 44J (OR LATEST ISSUE IN FORCE) AND BE FAA-APPROVED FOR TYPE L-858Y DIRECTION, DESTINATION, AND BOUNDARY SIGNS (BLACK LEGEND ON YELLOW BACKGROUND); TYPE L-858R MANDATORY INSTRUCTION SIGN (BLACK OUTLINE ON OUTSIDE EDGE OF WHITE LEGEND ON RED BACKGROUND); AND/OR TYPE L-858L LOCATION SIGN (YELLOW LEGEND AND BORDER ON BLACK BACKGROUND). THE SIGNS SHALL BE SIZE 1, 18-IN. SIGN FACE WITH A 12-IN. LEGEND; STYLE 2, POWERED FROM A 4.8 TO 6.6 AMP SERIES LIGHTING CIRCUIT; CLASS 2, FOR OPERATION FROM -40 DEGREES F TO 131 DEGREES F; MODE 2, TO WITHSTAND WIND LOADS OF 200 M.P.H., BASE-MOUNTED, DOUBLE-SIDED, AS SPECIFIED ON THE PLANS.

- TAXI GUIDANCE SIGN LEGEND**
- TYPE L-858L LOCATION SIGN - YELLOW LEGEND AND BORDER ON A BLACK BACKGROUND
  - TYPE L-858R MANDATORY INSTRUCTION SIGN - BLACK OUTLINE ON OUTSIDE EDGE OF WHITE LEGEND ON A RED BACKGROUND
  - TYPE L-858Y DIRECTION, DESTINATION, AND BOUNDARY SIGN - BLACK LEGEND ON A YELLOW BACKGROUND
  - BLANK - BLACK BACKGROUND



REVISION	DATE

EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

IL PROJ.: PRG-4018 A.I.P. PROJ.: 3-17-0077-B13

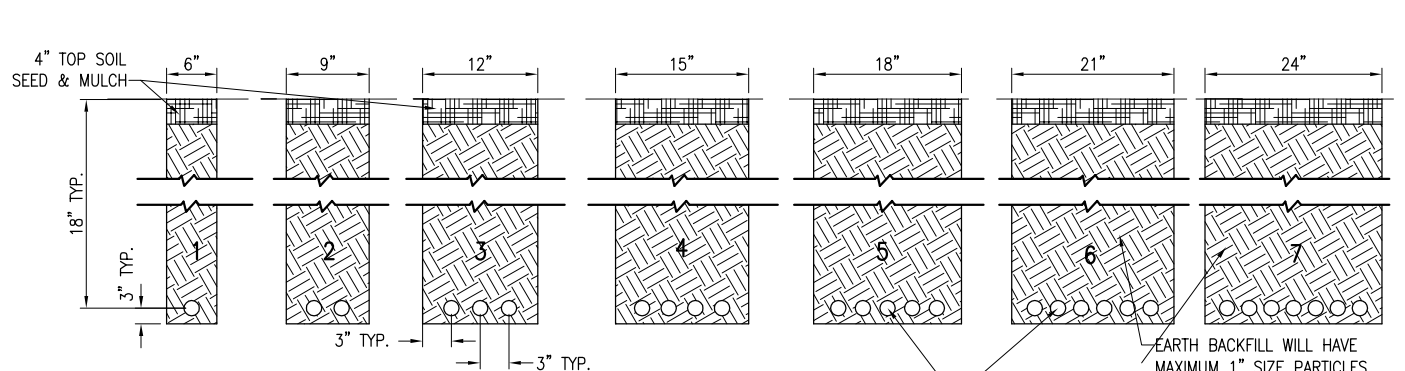
Hanson Proj. No. 10A0053	01/04/11
Filename R-142ELE.DWG	01/04/11
Scale 1" = 50'	
Date 03/10/11	
LAYOUT	
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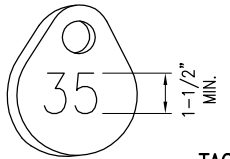
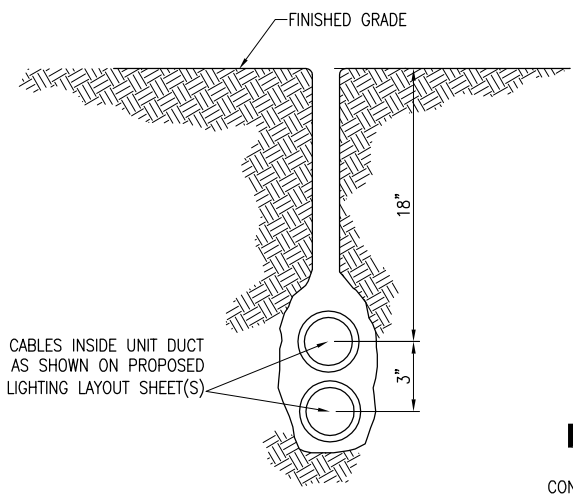
CONSTRUCT CROSSWIND  
RUNWAY 18-36

PROPOSED ELECTRICAL PLAN  
STA. 123+50 TO STA. 134+00

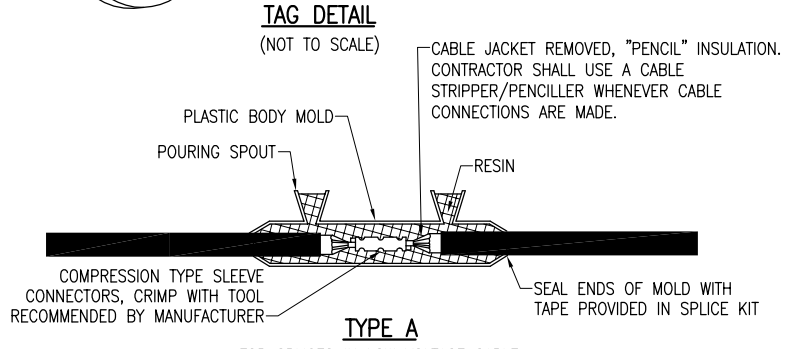




**NOTES:**  
 DETAIL NUMBERS INDICATE NO. OF CABLES.  
 TRENCHES WITH MORE THAN SEVEN CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE; IF SPECIFIED ON PLANS TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.  
 DEPTH OF TRENCHES SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS.  
 ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO TRENCH.

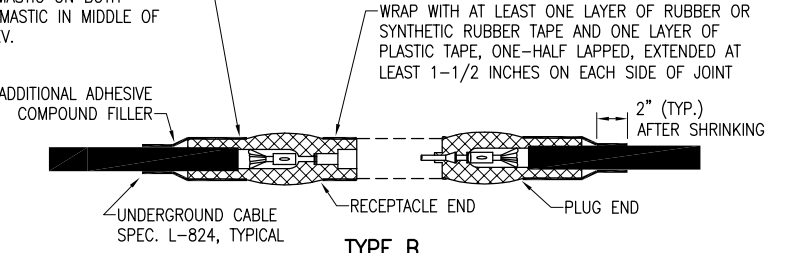


**NOTE:**  
 AFFIX NON-CORROSIVE TAG TO FIXTURE FACING RUNWAY WITH SET SCREW, WIRE TIE, OR METAL BAND. NUMERALS SHALL BE ENGRAVED FOR PERMANENT READABILITY.

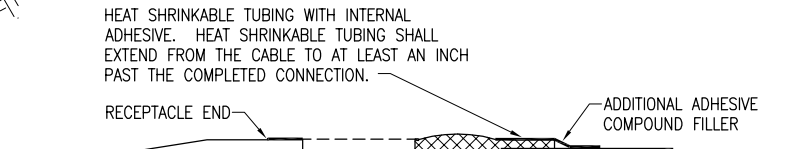


CONTINUOUS HEAT SHRINK TUBING PLACED OVER THE ENTIRE L-823 CONNECTOR(S) BOTH MALE AND FEMALE AT ALL 5KV JUNCTIONS. THE HEAT SHRINK TUBING SHALL BE APPROXIMATELY 18" IN LENGTH WITH 6 INCHES OF MASTIC ON BOTH ENDS AND VOID OF MASTIC IN MIDDLE OF TUBE RATED FOR 5KV.

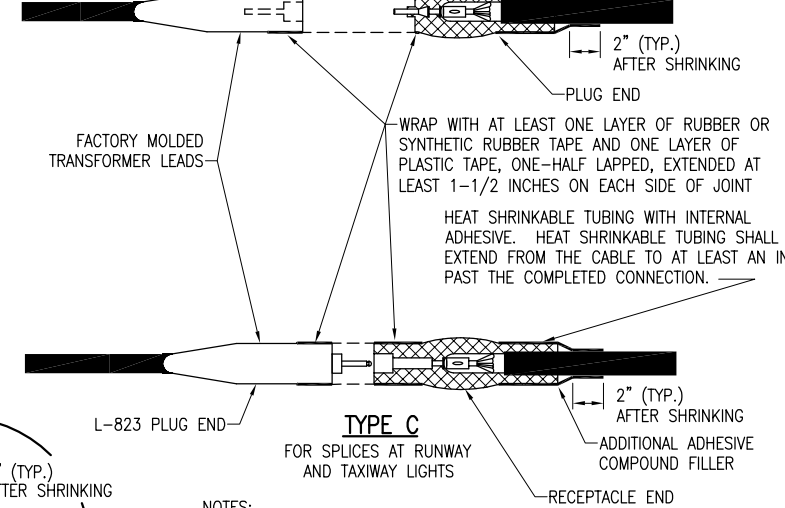
FOR SPLICES IN LOW VOLTAGE CABLE (600V) HOMERUNS FOR EXTENSIONS TO EXISTING LOW VOLTAGE CABLES ONLY. TYPE A SPLICES SHALL BE MADE IN SPLICE CANS, HANDHOLES, MANHOLES, OR JUNCTIONS BOXES



FOR SPLICES AT JUNCTION OF HOMERUN WITH LOOP CIRCUIT AND FOR SPLICES IN HOMERUNS TO EXISTING CABLES

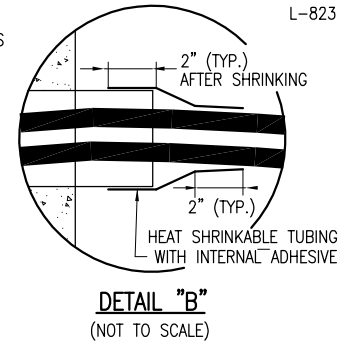


HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE. HEAT SHRINKABLE TUBING SHALL EXTEND FROM THE CABLE TO AT LEAST AN INCH PAST THE COMPLETED CONNECTION.



INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.

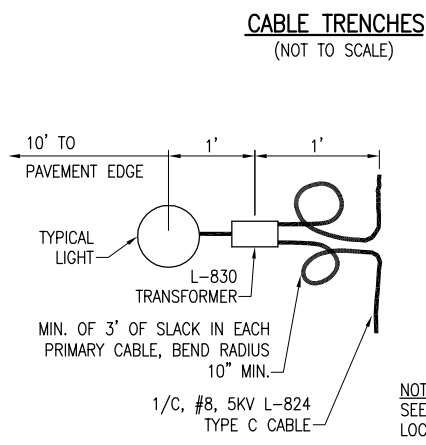
**CABLE SPLICES**  
(NOT TO SCALE)



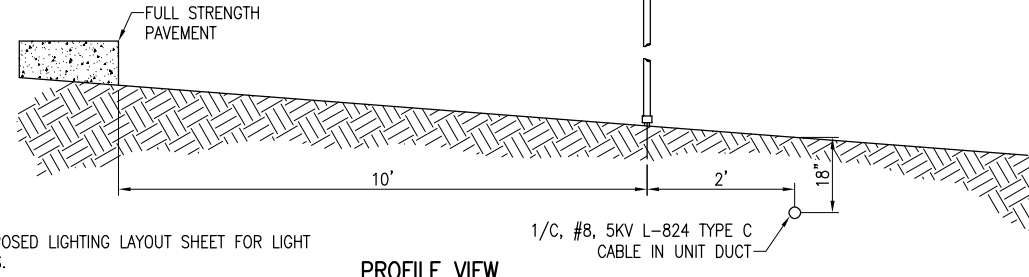
**DETAIL "B"**  
(NOT TO SCALE)

PER FAA AC 150/5340-30E DESIGN AND INSTALLATION DETAILS FOR AIRPORT VISUAL AIDS, A LIGHT BASE GROUND MUST BE INSTALLED AT EACH STAKE MOUNTED LIGHT AND EACH TRANSFORMER BASE/LIGHT CAN ASSOCIATED WITH RUNWAY LIGHTS, TAXIWAY LIGHTS, AND LIGHTED TAXI GUIDANCE SIGNS. THE LIGHT BASE GROUND SHALL BE A #6 AWG BARE COPPER CONDUCTOR BONDED TO THE GROUND LUG ON THE RESPECTIVE L-867 TRANSFORMER BASE/LIGHT CAN OR MOUNTING STAKE AND A 5/8-INCH DIAMETER BY 8-FOOT LONG (MINIMUM) UL LISTED COPPER CLAD GROUND ROD.

**NOTES:**  
 SEE PROPOSED LIGHTING LAYOUT SHEET FOR LIGHT LOCATIONS.

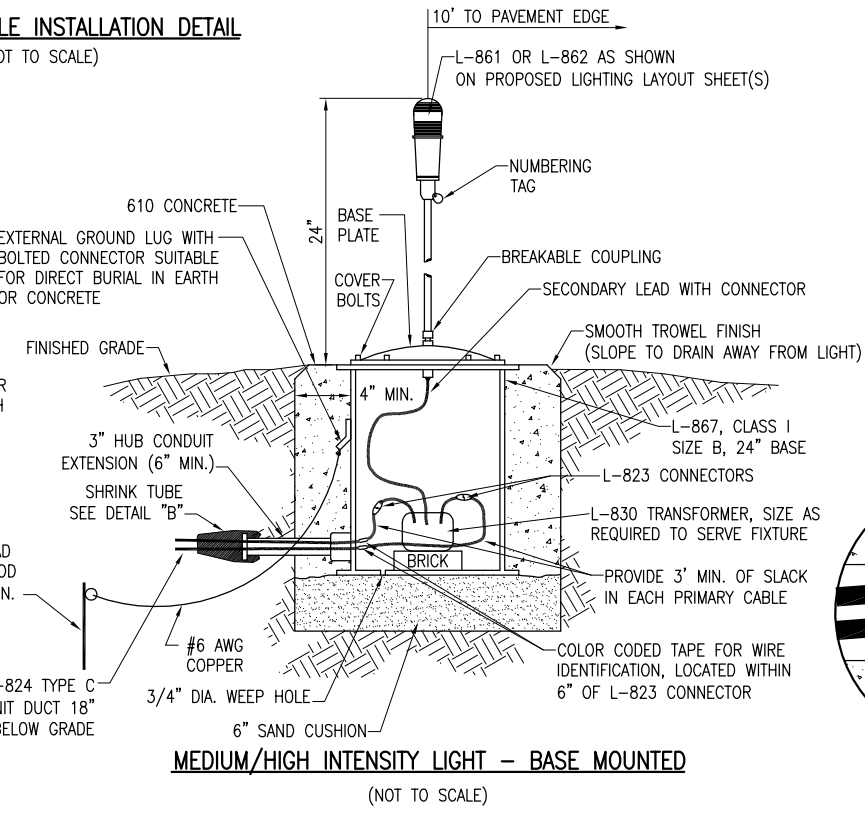


**PLAN VIEW**



**PROFILE VIEW**

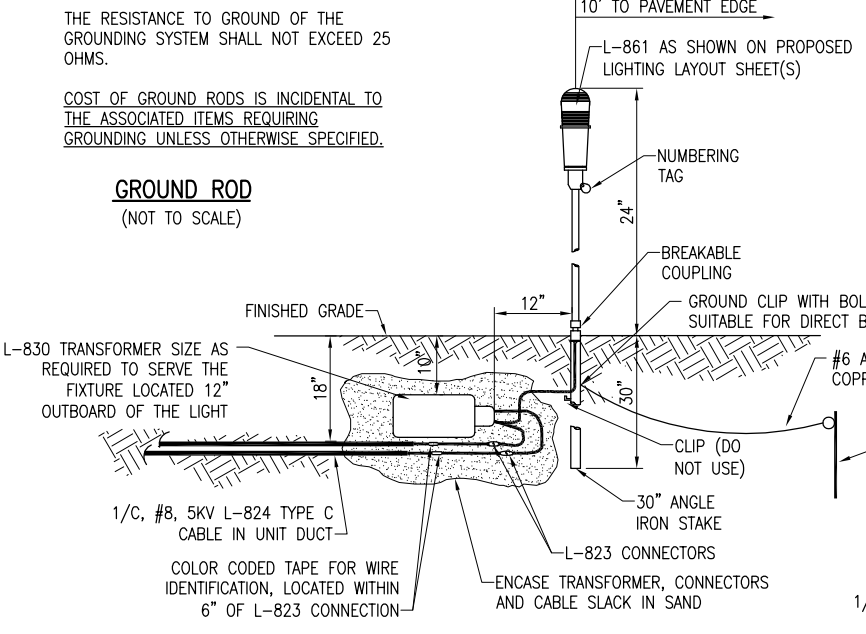
**LIGHT AND CABLE INSTALLATION DETAIL**  
(NOT TO SCALE)



**MEDIUM/HIGH INTENSITY LIGHT - BASE MOUNTED**  
(NOT TO SCALE)

**GROUND ROD**  
(NOT TO SCALE)

**NOTES:**  
 TYPE AND MINIMUM NUMBER OF GROUND RODS SHALL BE AS SPECIFIED ON THE PLAN.  
 THE RESISTANCE TO GROUND OF THE GROUNDING SYSTEM SHALL NOT EXCEED 25 OHMS.  
 COST OF GROUND RODS IS INCIDENTAL TO THE ASSOCIATED ITEMS REQUIRING GROUNDING UNLESS OTHERWISE SPECIFIED.



**MEDIUM INTENSITY LIGHT - STAKE MOUNTED**  
(NOT TO SCALE)

REVISION	DATE

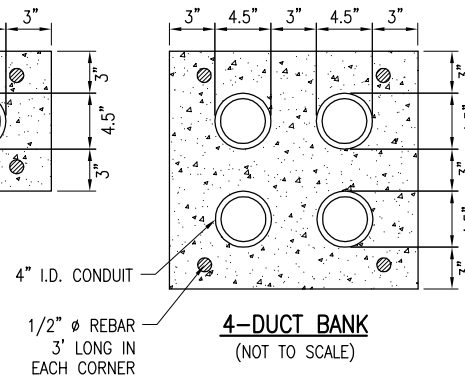
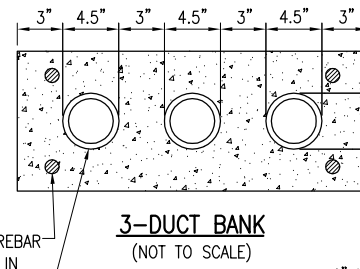
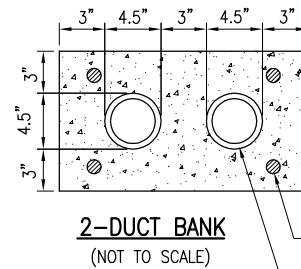
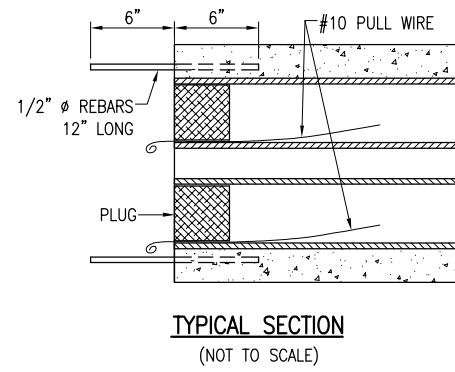
EDGAR COUNTY AIRPORT  
 PARIS, ILLINOIS

A.I.P. PROJ.: 3-17-0077-B13  
 IL PROJ.: PRG-4018

Hanson Proj. No. 10A00553	12/23/10
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CONSTRUCT CROSSWIND  
 RUNWAY 18-36  
 ELECTRICAL DETAILS  
 SHEET 1



TYPICAL SECTION  
"NOT TO SCALE"

2-DUCT BANK  
"NOT TO SCALE"

3-DUCT BANK  
"NOT TO SCALE"

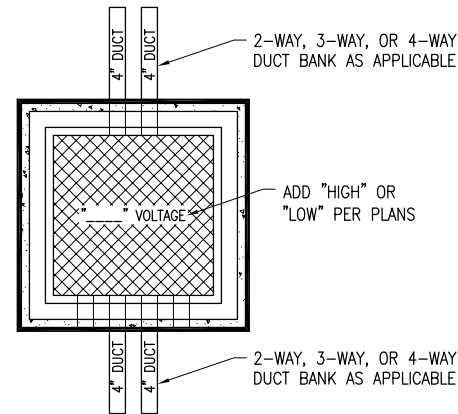
4-DUCT BANK  
"NOT TO SCALE"

DUCT BANK NOTES:

- DIMENSIONS FOR CONCRETE COVERAGE AND SEPARATION BETWEEN DUCTS ARE MINIMUM.
- INCLUDE DUCT SPACERS AS MANUFACTURED BY UNDERGROUND DEVICES INC., OR APPROVED EQUAL TO MAINTAIN PROPER SEPARATION OF CONDUITS.
- REBAR IS REQUIRED TO ACCOMMODATE FUTURE DUCT EXTENSIONS & INTERFACE AT DUCT BANK TERMINATIONS. CONCRETE ENCASED DUCT BANKS TERMINATING IN HANDHOLES REQUIRE REBAR AT TERMINATIONS.
- CONDUITS FOR CONCRETE ENCASED DUCT SHALL BE SCHEDULE 40 PVC CONFORMING TO ITEM 110.
- MINIMUM DEPTH OF TOP OF DUCT ENCASEMENT SHALL BE 18" BELOW FINISHED GRADE.
- HIGH VOLTAGE AND LOW VOLTAGE CIRCUITS SHALL NOT BE INSTALLED IN THE SAME RACEWAY, CONDUIT, DUCT, HANDHOLE, OR MANHOLE.
- HOMERUN CABLES FOR A RESPECTIVE CIRCUIT SHALL BE INSTALLED IN THE SAME RACEWAY OR DUCT.
- DUCT INTERFACE TO HANDHOLES OR MANHOLES WILL BE CONSIDERED INCIDENTAL TO THE RESPECTIVE DUCT PAY ITEM.

CABLE & DUCT MARKER NOTES:

- THE COST OF ALL TURF AND PAVEMENT DUCT MARKERS SHALL BE INCIDENTAL TO THE DUCT. THE COST OF ALL CABLE MARKERS SHALL BE INCIDENTAL TO THE CABLE.
- BITUMINOUS PAVEMENT DUCT MARKER AND CONCRETE DUCT MARKER TO BE PROVIDED AT EACH END OF EACH DUCT AS SHOWN ON THE LOCATION PLAN. FOR CONCRETE PAVEMENT, THE LETTER "D" SHALL BE IMPRESSED IN THE PAVEMENT INSTEAD OF THE MARKER. THE LETTER SHALL BE FORMED AS DESCRIBED IN NOTE 4.
- CABLE MARKERS SHALL BE PLACED AT CHANGES OF DIRECTION AND APPROXIMATELY EVERY 200' ALONG CABLE RUNS.
- CONCRETE CABLE MARKERS AND DUCT MARKERS SHALL HAVE LETTERS 4" HIGH, 3" WIDE WITH WIDTH OF STROKE 1/2" AND 1/4" DEEP. ALL LETTERS, NUMBERS AND ARROWS TO BE IMPRESSED.



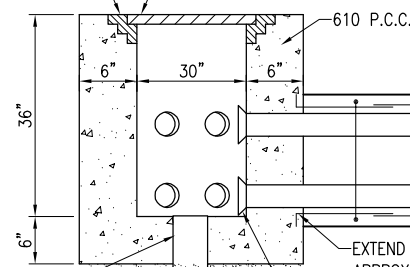
2-WAY, 3-WAY, OR 4-WAY DUCT BANK AS APPLICABLE

ADD "HIGH" OR "LOW" PER PLANS

2-WAY, 3-WAY, OR 4-WAY DUCT BANK AS APPLICABLE

HEAVY DUTY FRAME & LID SUITABLE FOR H-20 LOADING, NEENAH CAT. NO. R-6662-PP OR APPROVED EQUAL

SMOOTH TROWEL FINISH (SLOPE TO DRAIN)



DUCT BANK SHALL TRANSITION TO (OR BE) REINFORCED CONCRETE ENCASED DUCT WHERE ENTERING A HANDHOLE. PROVIDE REINFORCEMENT 3 FT. MIN. BEYOND HANDHOLE. DIRECT BURY DUCT (WITHOUT CONCRETE ENCASEMENT) DOES NOT REQUIRE REBAR & CONCRETE ENCASEMENT AT INTERFACE TO HANDHOLE.

EXTEND NO. 4 REBAR INTO HANDHOLE APPROX 3". PROVIDE 90° "L" HOOK ON REBAR TERMINATION IN HANDHOLE. (TYP.) OR EXTEND REBAR EPOXY ANCHORED INTO HANDHOLE WITH 4" EMBEDMENT.

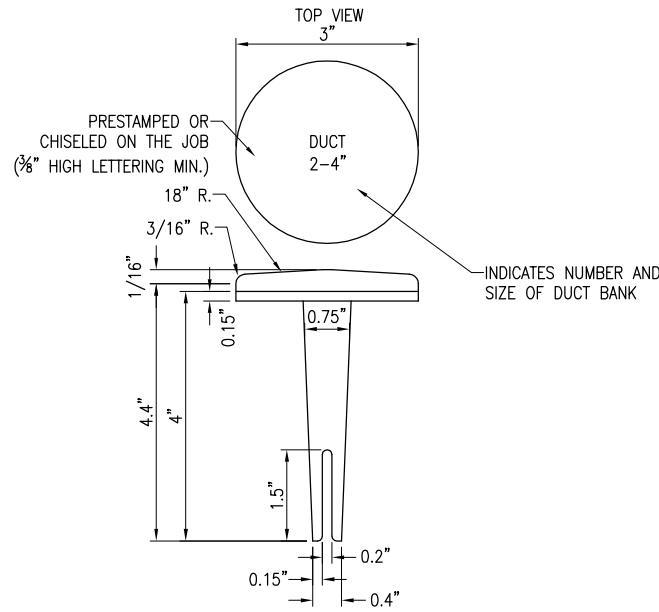
PROVIDE CONDUIT BUSHING OR BELL AT TERMINATION IN HANDHOLE (TYP.)

6" SCHED 40 PVC DRAIN PIPE. FILL WITH PEA GRAVEL TO ACCOMMODATE DRAINAGE. NOTE 6" OF CA-7 GRAVEL MAY BE PROVIDED, INSTEAD OF 6" CONCRETE FLOOR WITH DRAIN PIPE, AT CONTRACTORS OPTION.

NOTES:

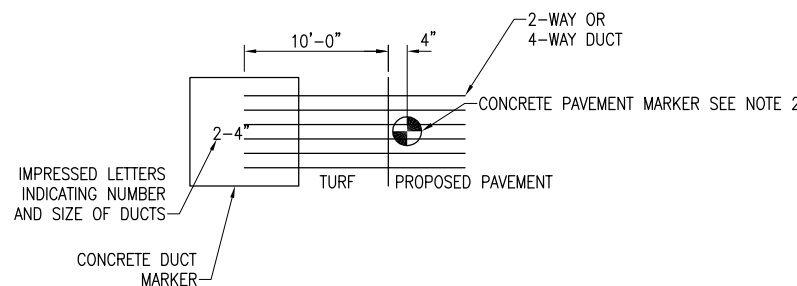
- LIDS FOR LOW VOLTAGE HANDHOLES SHALL BE LABELED "LOW VOLTAGE". LIDS FOR HIGH VOLTAGE HANDHOLES SHALL BE LABELED "HIGH VOLTAGE". COORDINATE LETTERING WITH MFR.
- HANDHOLES MAY BE CAST IN PLACE OR PRECAST. PRECAST MANUFACTURERS MUST BE ON THE IDOT (ILLINOIS DEPT. OF TRANSPORTATION) APPROVED LIST OF CERTIFIED PRECAST CONCRETE PRODUCERS.
- ALL CORING, INTERFACE, AND LABOR ASSOCIATED WITH CONDUIT, DUCT, CABLE IN UNIT DUCT, AND / OR CABLE ENTRIES WILL BE CONSIDERED INCIDENTAL TO THE INSTALLATION OF THE HANDHOLE AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ELECTRICAL HANDHOLE  
"NOT TO SCALE"

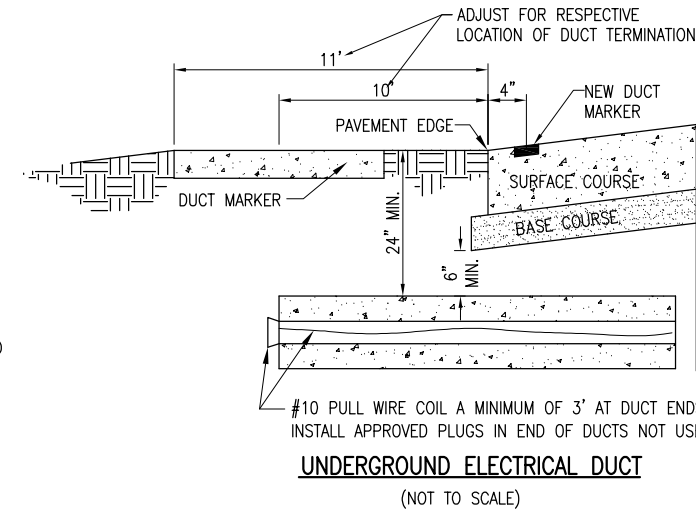


BITUMINOUS PAVEMENT DUCT MARKERS  
"NOT TO SCALE"

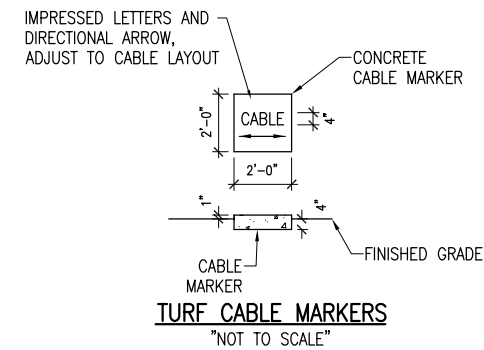
NOTE: TOP OF MARKER SHALL BE FLUSH WITH FINISHED PAVEMENT SURFACE. MARKER MAY BE INSTALLED IN A DRILLED HOLE AND SECURED WITH EPOXY GLUE.



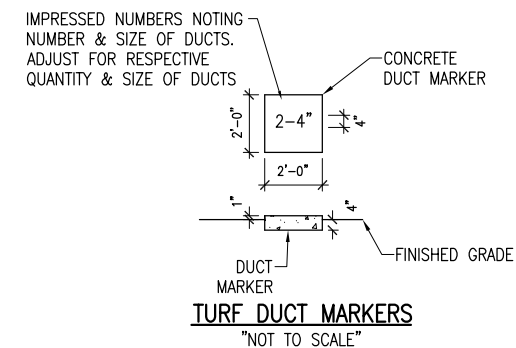
DUCT MARKER DETAIL  
"NOT TO SCALE"



UNDERGROUND ELECTRICAL DUCT  
"NOT TO SCALE"



TURF CABLE MARKERS  
"NOT TO SCALE"



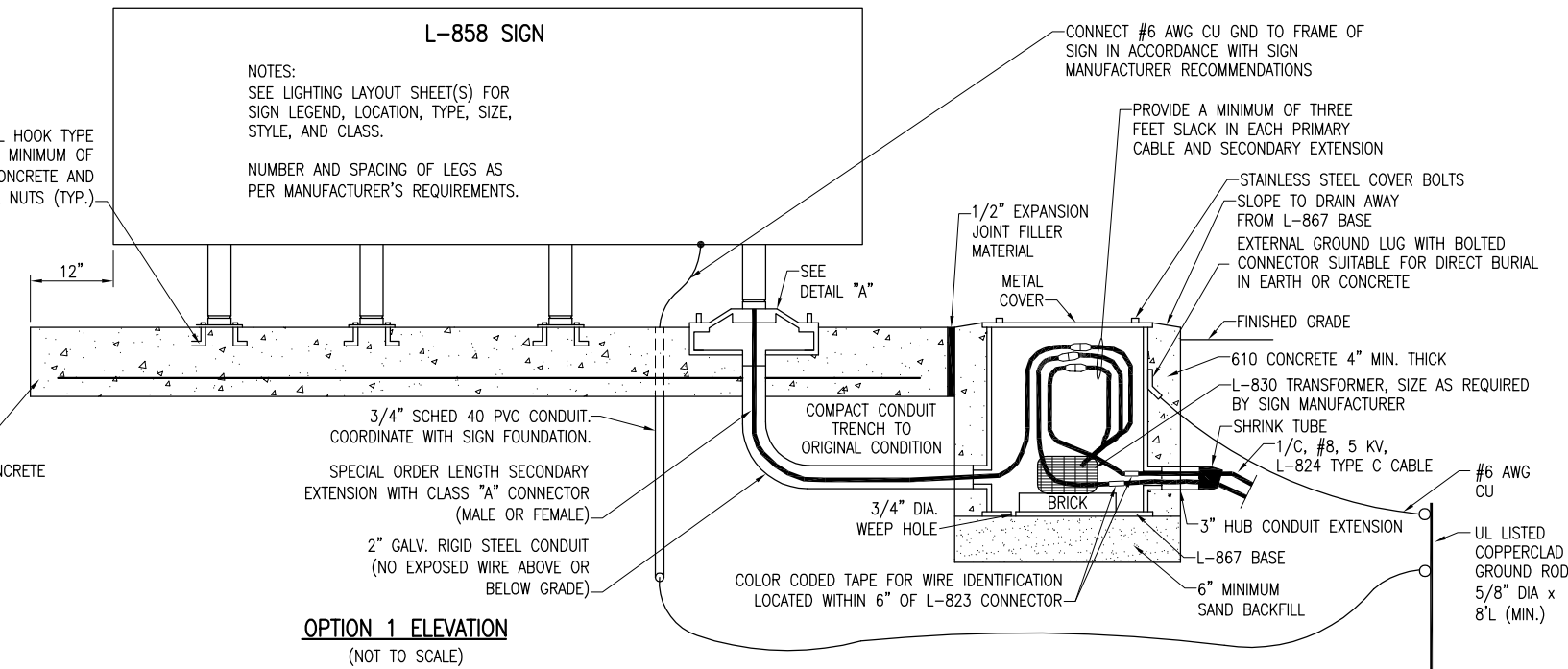
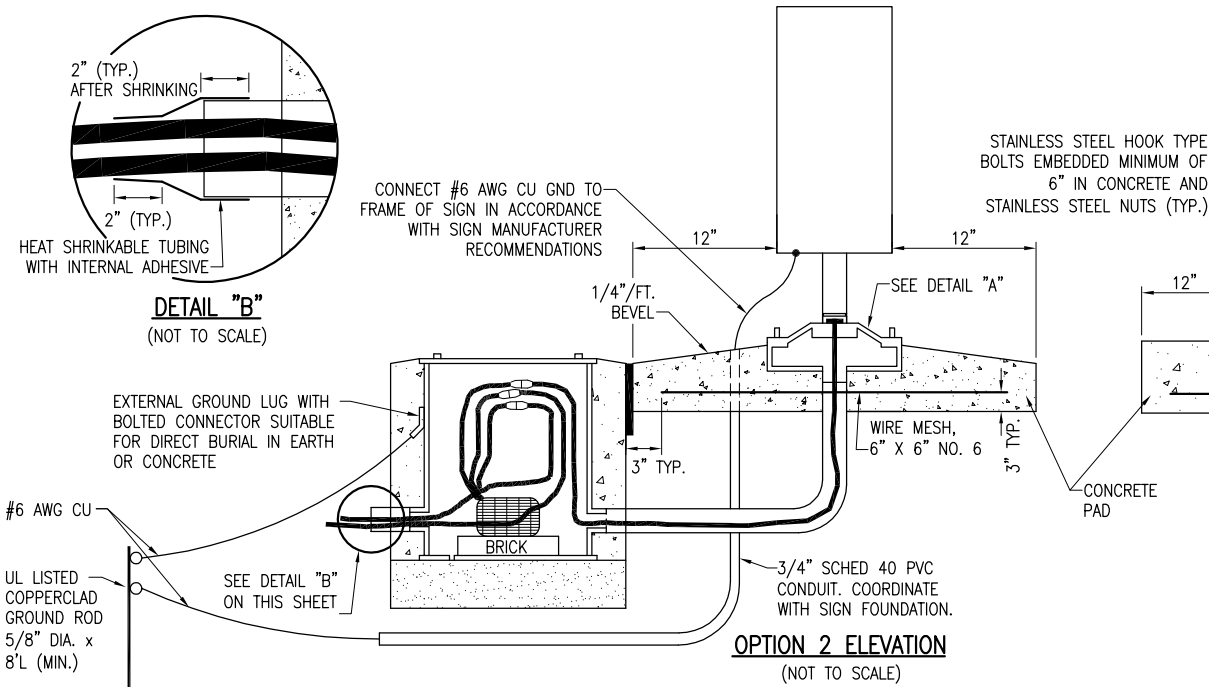
TURF DUCT MARKERS  
"NOT TO SCALE"

REVISION	
DATE	
EDGAR COUNTY AIRPORT PARIS, ILLINOIS	
A.I.P. PROJ.: 3-17-0077-B13	
IL PROJ.: PRG-4018	

Hanson Proj. No. 10A0053	12/23/10
Filename E-502.DWG	KNL
Scale AS SHOWN	MLH
Date 03/10/11	CAH
LAYOUT	REVIEWED
DRAWN	

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CONSTRUCT CROSSWIND  
RUNWAY 18-36  
ELECTRICAL DETAILS  
SHEET 2

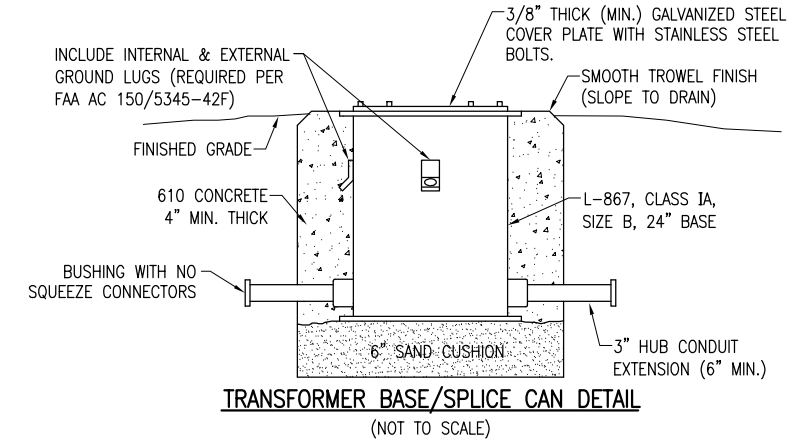
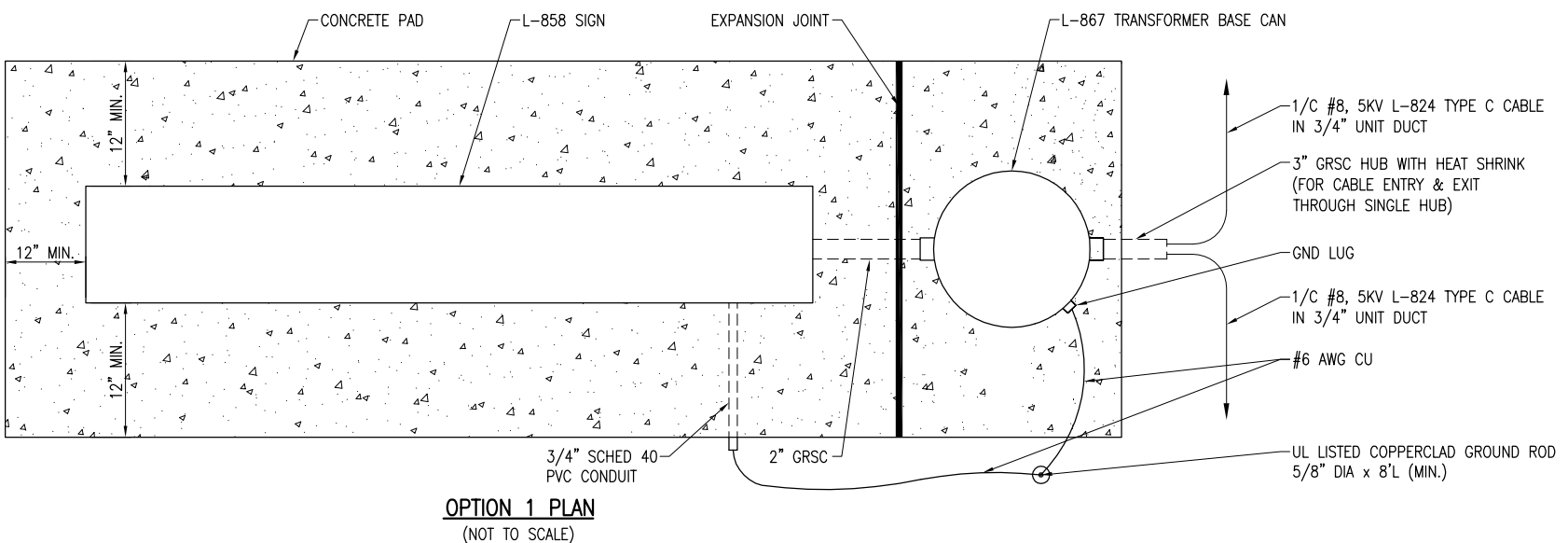
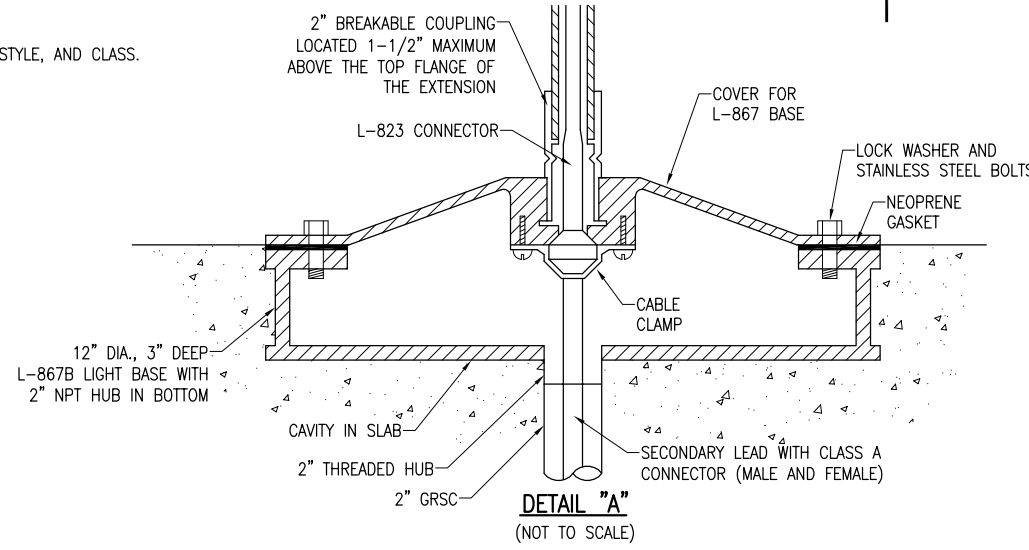
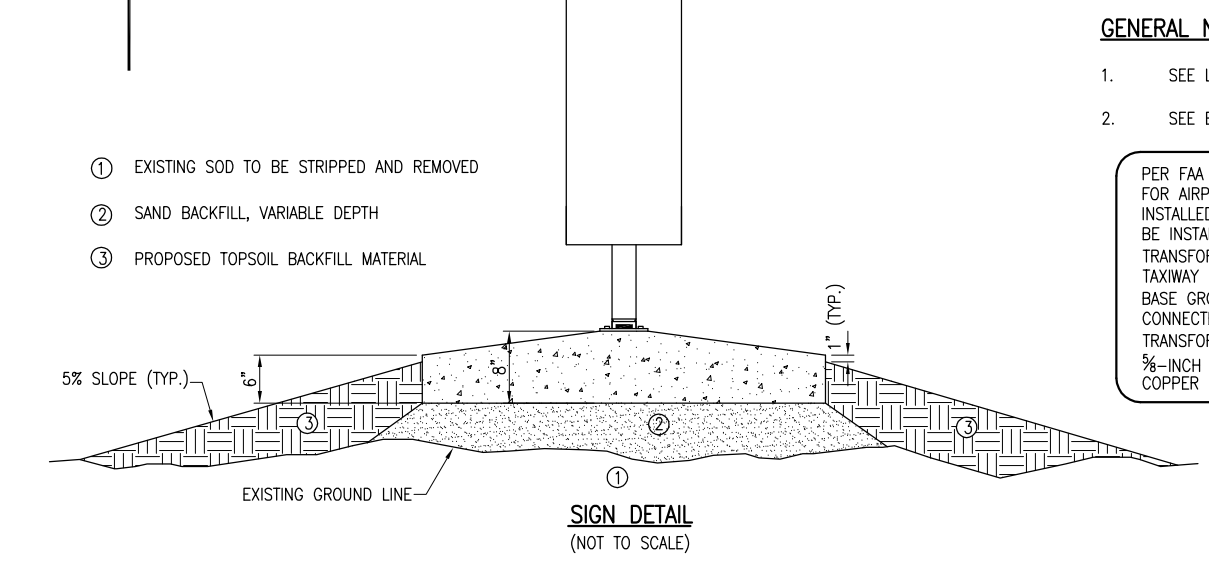


**GENERAL NOTES:**

- SEE LIGHTING LAYOUT SHEET FOR SIGN LEGEND, LOCATION, TYPE, SIZE, STYLE, AND CLASS.
- SEE ELECTRICAL NOTES SHEETS.

PER FAA AC 150/5340-30E DESIGN AND INSTALLATION DETAILS FOR AIRPORT VISUAL AIDS, A LIGHT BASE GROUND MUST BE INSTALLED AT EACH LIGHT FIXTURE. A LIGHT BASE GROUND SHALL BE INSTALLED AT EACH STAKE MOUNTED LIGHT AND EACH TRANSFORMER BASE/LIGHT CAN ASSOCIATED WITH RUNWAY LIGHTS, TAXIWAY LIGHTS, AND LIGHTED TAXI GUIDANCE SIGNS. THE LIGHT BASE GROUND SHALL BE A #6 AWG BARE COPPER CONDUCTOR CONNECTED TO THE GROUND LUG ON THE RESPECTIVE L-867 TRANSFORMER BASE/LIGHT CAN OR MOUNTING STAKE AND A 5/8-INCH DIAMETER BY 8-FOOT LONG (MINIMUM) UL LISTED COPPER CLAD GROUND ROD.

- EXISTING SOD TO BE STRIPPED AND REMOVED
- SAND BACKFILL, VARIABLE DEPTH
- PROPOSED TOPSOIL BACKFILL MATERIAL



NOTE:  
FOR THE PURPOSE OF ENHANCING SAFETY, EACH BASE MUST HAVE INSTALLED, BY THE MANUFACTURER, AN INTERNAL AND EXTERNAL GROUND STRAP THAT IS AVAILABLE FOR THE PURPOSE OF ATTACHING A GROUND LUG THAT IS CONNECTED TO AN EARTH GROUND OR A SAFETY GROUND CONDUCTOR INSTALLED WITH THE RESPECTIVE CIRCUIT. FOR AIRPORT PROJECTS RECEIVING FEDERAL FUNDS THIS REQUIREMENT IS MANDATORY PER FAA AC 150/5345-42F.

REVISION	DATE

EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS  
A.I.P. PROJ.: 3-17-0077-B13  
PRG-4018  
ILL. PROJ.: PRG-4018

PROJ. NO.	FILENAME	SCALE	DATE	LAYOUT	DRAWN	REVIEWED
10A00553	E-503.DWG	AS SHOWN	03/10/11	KNL	MLH	CAH

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CONSTRUCT CROSSWIND  
RUNWAY 18-36  
ELECTRICAL DETAILS  
SHEET 3

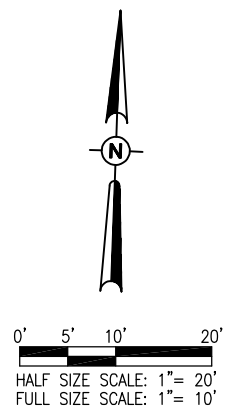
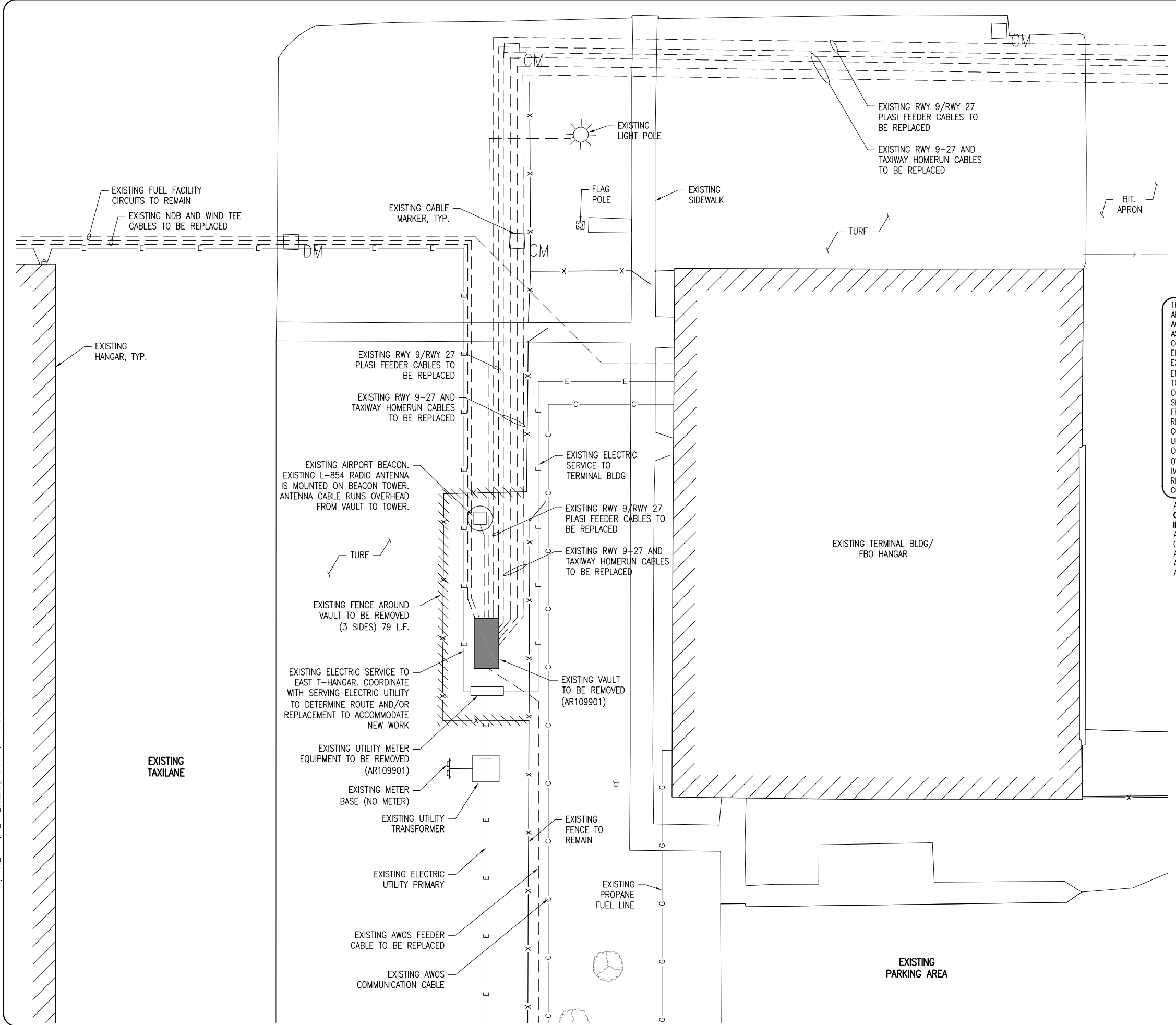
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LEGEND

- EXISTING PAVEMENT
- EXISTING ELECTRICAL DUCT
- EXISTING ELECTRICAL CABLES
- EXISTING ELECTRICAL UTILITY CABLE
- EXISTING COMMUNICATION CABLE
- EXISTING GAS LINE
- EXISTING FENCE
- FENCE TO BE REMOVED
- EXISTING ELECTRICAL STRUCTURE (HANDHOLE, SPLICE CAN)
- EXISTING ELECTRICAL MARKER (DUCT MARKER, CABLE MARKER)

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123. CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.



REVISION	DATE

EDGAR COUNTY AIRPORT  
 PARIS, ILLINOIS  
 ILL. PROJ.: PRG-4018  
 A.I.P. PROJ.: 3-17-0077-B13

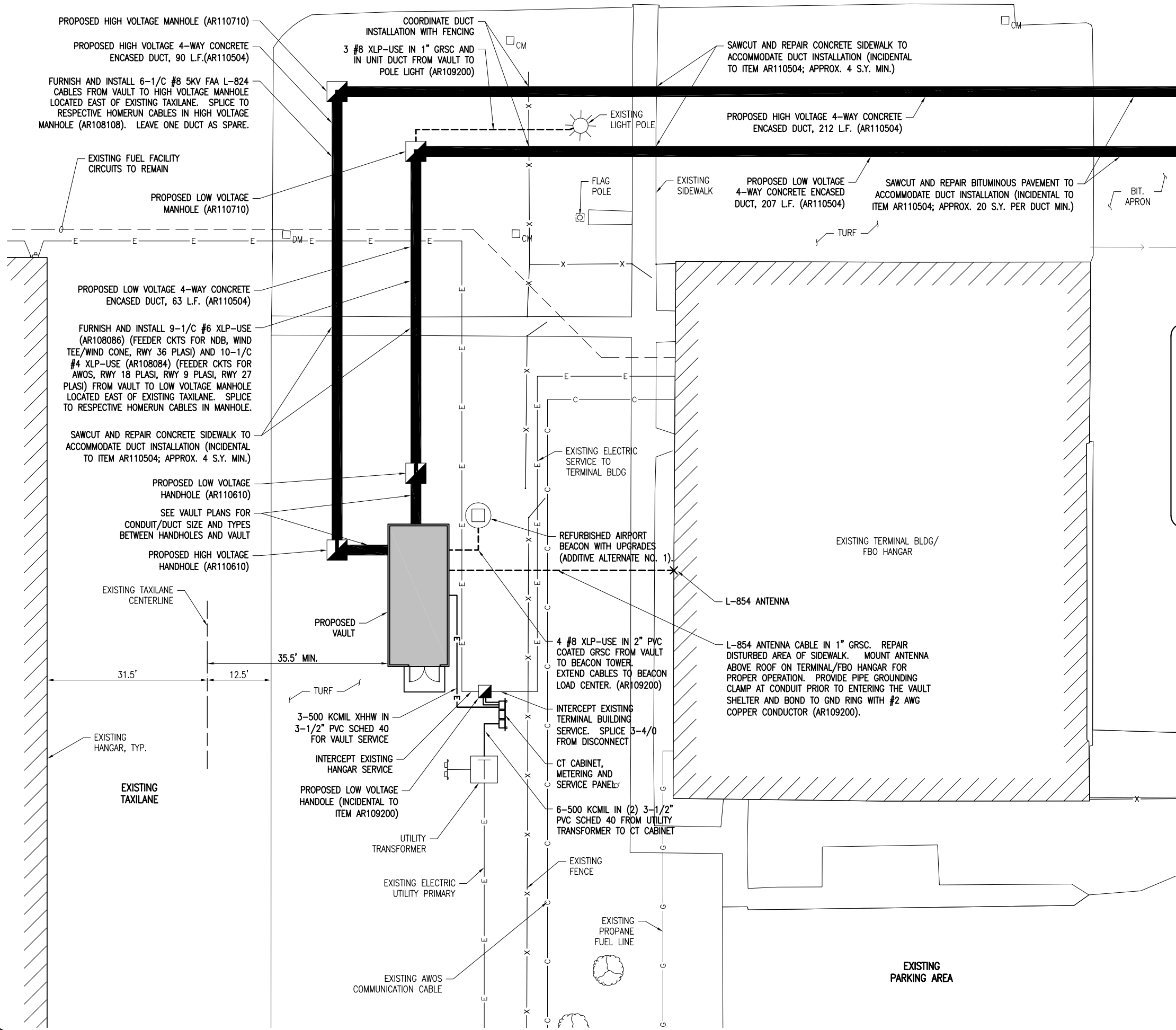
FILENAME	SCALE	DATE	LAYOUT	KNL	MLH	CAH
R-143.VLT.DWG	1" = 10'	03/10/11		01/04/11	01/11/11	02/04/11

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CONSTRUCT CROSSWIND  
 RUNWAY 18-36  
 EXISTING VAULT AREA  
 ELECTRICAL PLAN

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LEGEND

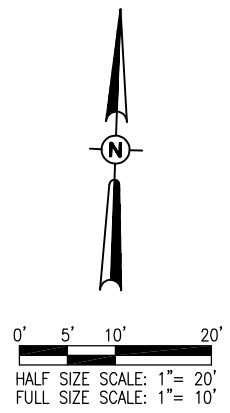
- EXISTING PAVEMENT
- EXISTING ELECTRICAL DUCT
- EXISTING ELECTRICAL DUCT
- EXISTING ELECTRICAL CABLES
- EXISTING ELECTRICAL UTILITY CABLE
- EXISTING COMMUNICATION CABLE
- EXISTING GAS LINE
- EXISTING FENCE
- EXISTING ELECTRICAL STRUCTURE (HANDHOLE, SPLICE CAN)
- EXISTING ELECTRICAL MARKER (DUCT MARKER, CABLE MARKER)
- PROPOSED ELECTRICAL STRUCTURE (MANHOLE, HANDHOLE, SPLICE CAN)

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

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NOTES:

1. HOMERUN CABLES FOR A RESPECTIVE CIRCUIT THAT ARE INSTALLED IN CONDUIT OR DUCT SHALL BE RUN TOGETHER IN THE SAME RACEWAY OR DUCT.
2. ANY AND ALL DISTURBED PAVEMENT AREAS WILL BE RESTORED TO ORIGINAL OR BETTER CONDITION. RESTORATION OF PAVEMENT AREAS DISTURBED DURING THE INSTALLATION OF THE PROPOSED DUCTS WILL BE INCIDENTAL TO THE RESPECTIVE PAY ITEM FOR WHICH THE DUCT IS INSTALLED. THE RESTORATION OF PAVEMENT WILL BE COMPLETED IN ACCORDANCE WITH ITEM 610 FOR SIDEWALKS AND CONCRETE PAVEMENT AND ITEM 401 FOR BITUMINOUS PAVEMENT, BUT WILL BE INCIDENTAL TO THE RESPECTIVE PAY ITEM FOR WHICH THE DUCT IS INSTALLED.



REVISION									
DATE									
EDGAR COUNTY AIRPORT PARIS, ILLINOIS									
A.I.P. PROJ.: 3-17-0077-B13 IL PROJ.: PRG-4018									
Hanson Proj. No. 10A0053 Filename: R-144VLT.DWG Scale: 1" = 20' Date: 03/10/11	LAYOUT	KNL	01/04/11	DRAWN	MLH	01/11/11	REVIEWED	CAH	02/04/11
 Hanson Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62703-2886 www.hanson-inc.com Offices Nationwide									
CONSTRUCT CROSSWIND RUNWAY 18-36 PROPOSED VAULT AREA ELECTRICAL PLAN									
<span style="font-size: 2em; font-weight: bold;">41</span> 41 of 137 sheets									

ELECTRICAL LEGEND - ONE-LINE DIAGRAM	
	CABLE TERMINATOR/LUG
	TRANSFORMER
	DISCONNECT SWITCH
	FUSIBLE DISCONNECT SWITCH
	CIRCUIT BREAKER
	THERMAL MAGNETIC CIRCUIT BREAKER
	FUSE
	TRANSIENT VOLTAGE SURGE SUPPRESSOR OR SURGE PROTECTOR DEVICE
	GROUND - GROUND ROD, GROUNDING ELECTRODE, OR AT EARTH POTENTIAL
	INDICATING LIGHT
	MOTOR
	LOAD, MOTOR, # = HORSEPOWER
	ELECTRIC UTILITY METER BASE
	JUNCTION BOX WITH SPLICE
	EQUIPMENT, XXX = DEVICE DESCRIPTION
	GROUND BUS OR TERMINAL
	NEUTRAL BUS
	PANELBOARD WITH MAIN LUGS
	PANELBOARD WITH MAIN BREAKER
	FUSE PANEL WITH MAIN FUSE PULLOUT
	DUPLEX RECEPTACLE 120V SINGLE PHASE GROUNDING TYPE
	CONTROL STATION
	TRANSFER SWITCH
	ENGINE GENERATOR SET

ELECTRICAL LEGEND - SCHEMATIC	
	NORMALLY OPEN (N.O.) CONTACT
	NORMALLY CLOSED (N.C.) CONTACT
	STARTER COIL, * = STARTER NUMBER
	OVERLOAD RELAY CONTACT
	CONTROL RELAY, * = CONTROL RELAY NUMBER
	RELAY, * = RELAY NUMBER
	TOGGLE SWITCH / 2 POSITION SWITCH
	2-POSITION SELECTOR SWITCH
	3-POSITION SELECTOR SWITCH (H-O-A SHOWN)
	2 POLE DISCONNECT SWITCH
	3 POLE DISCONNECT SWITCH
	PHOTOCELL
	TERMINAL BLOCK, * = TERMINAL NUMBER
	DEVICE TERMINAL, * = DEVICE TERMINAL NUMBER
	INTERNAL PANEL WIRING
	FIELD WIRING
	FUSE
	GROUND BUS OR TERMINAL
	NEUTRAL BUS
	GROUND, GROUND ROD, GROUND BUS
	INDUSTRIAL CONTROL RELAY OR LIGHTING CONTACTOR
	S1 CUTOUT HANDLE REMOVED
	S1 CUTOUT HANDLE INSERTED
	N.O. THERMAL SWITCH
	N.C. THERMAL SWITCH
	L-830 SERIES ISOLATION TRANSFORMER

ELECTRICAL ABBREVIATIONS	
A.F.F.	ABOVE FINISHED FLOOR
A, AMP	AMPERES
ATS	AUTOMATIC TRANSFER SWITCH
AWG	AMERICAN WIRE GAUGE
BKR	BREAKER
C	CONDUIT
CB	CIRCUIT BREAKER
CKT	CIRCUIT
CR	CONTROL RELAY
CU	COPPER
DPDT	DOUBLE POLE DOUBLE THROW
DPST	DOUBLE POLE SINGLE THROW
EM	EMERGENCY
EMT	ELECTRICAL METALLIC TUBING
ENCL	ENCLOSURE
EP	EXPLOSION PROOF
ES	EMERGENCY STOP
ETL	INTERTEK - ELECTRICAL TESTING LABS
ETM	ELAPSE TIME METER
GFCI	GROUND FAULT CIRCUIT INTERRUPTER
GFI	GROUND FAULT INTERRUPTER
GND	GROUND
GRSC	GALVANIZED RIGID STEEL CONDUIT
HID	HIGH INTENSITY DISCHARGE
HOA	HAND OFF AUTOMATIC
HP	HORSEPOWER
HPS	HIGH PRESSURE SODIUM
J	JUNCTION BOX
KVA	KILOVOLT AMPERE(S)
KW	KILOWATTS
LC	LIGHTING CONTACTOR
LTFMC	LIQUID TIGHT FLEXIBLE METAL CONDUIT (UL LISTED)
LTG	LIGHTING
LP	LIGHTING PANEL
MAX	MAXIMUM
MCB	MAIN CIRCUIT BREAKER
MCM	THOUSAND CIRCLUAR MIL
MDP	MAIN DISTRIBUTION PANEL
MFR	MANUFACTURER
MH	METAL HALIDE
MIN	MINIMUM
MLO	MAIN LUGS ONLY
NEC	NATIONAL ELECTRICAL CODE (NFPA 70)
NC	NORMALLY CLOSED
NO	NORMALLY OPEN
NTS	NOT TO SCALE
OHE	OVERHEAD ELECTRIC
OL	OVERLOAD

ELECTRICAL ABBREVIATIONS (CONTINUED)	
PB	PULL BOX
PC	PHOTO CELL
PDB	POWER DISTRIBUTION BLOCK
PNL	PANEL
RCPT	RECEPTACLE
R	RELAY
S	STARTER
SPD	SURGE PROTECTION DEVICE
SPST	SINGLE POLE SINGLE THROW
TVSS	TRANSIENT VOLTAGE SURGE SUPPRESSOR
TYP	TYPICAL
UG	UNDERGROUND
UGE	UNDERGROUND ELECTRIC
UL	UNDERWRITER'S LABORATORIES
V	VOLTS
W/	WITH
W/O	WITHOUT
WP	WEATHER PROOF
XFER	TRANSFER
XFMR	TRANSFORMER

AIRPORT EQUIPMENT/FACILITY ABBREVIATIONS	
ASOS	AUTOMATED SURFACE OBSERVING SYSTEM
ATCT	AIR TRAFFIC CONTROL TOWER
AWOS	AUTOMATED WEATHER OBSERVING SYSTEM
CCR	CONSTANT CURRENT REGULATOR
DME	DISTANCE MEASURING EQUIPMENT
FAR	FEDERAL AVIATION REGULATION
GS	GLIDE SLOPE FACILITY
HIRL	HIGH INTENSITY RUNWAY LIGHT
ILS	INSTRUMENT LANDING SYSTEM
IM	INNER MARKER
LIR	LOW IMPACT-RESISTANT
LOC	LOCALIZER FACILITY
MALS	MEDIUM INTENSITY APPROACH LIGHTING SYSTEM
MALSR	MEDIUM INTENSITY APPROACH LIGHTING SYSTEM WITH RUNWAY ALIGNMENT INDICATING LIGHTS
MIRL	MEDIUM INTENSITY RUNWAY LIGHT
MTL	MEDIUM INTENSITY TAXIWAY LIGHT
NDB	NON-DIRECTIONAL BEACON
PAPI	PRECISION APPROACH PATH INDICATOR
PLASI	PULSE LIGHT APPROACH SLOPE INDICATOR
RAIL	RUNWAY ALIGNMENT INDICATING LIGHTS
REIL	RUNWAY END IDENTIFIER LIGHT
RVR	RUNWAY VISUAL RANGE
VADI	VISUAL APPROACH DESCENT INDICATOR
VASI	VISUAL APPROACH SLOPE INDICATOR
VOR	VERY HIGH FREQUENCY OMNDIRECTIONAL RANGE FACILITY
WC	WIND CONE

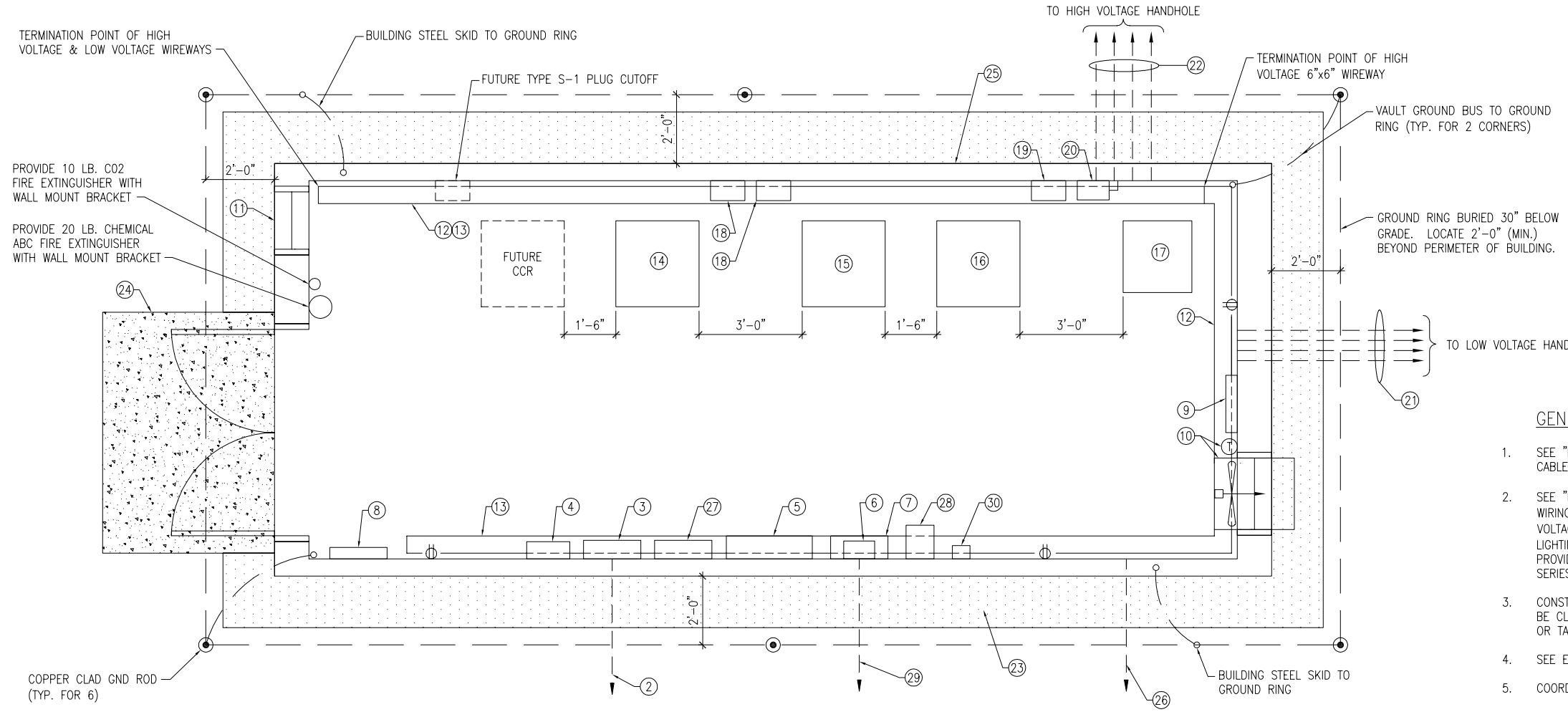
NOTES:

- ALL ELECTRICAL EQUIPMENT SHALL BE INSTALLED IN CONFORMANCE WITH NFPA 70 - NATIONAL ELECTRICAL CODE (NEC) MOST CURRENT ISSUE IN FORCE, THE RESPECTIVE EQUIPMENT MANUFACTURER'S DIRECTIONS AND ALL OTHER APPLICABLE LOCAL CODES, LAWS, ORDINANCES, AND REQUIREMENTS IN FORCE. ANY INSTALLATIONS WHICH VOID THE U.L. LISTING, ETL LISTING (OR OTHER THIRD PARTY LISTING) AND/OR THE MANUFACTURER'S WARRANTY OF A DEVICE WILL NOT BE PERMITTED.
- ALL VAULT WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT MANAGER. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).
- COLOR CODE PHASE AND NEUTRAL CONDUCTOR INSULATION FOR NO. 6 AWG OR SMALLER. PROVIDE COLORED INSULATION OR COLORED MARKING TAPE FOR PHASE AND NEUTRAL CONDUCTORS FOR NO. 4 AWG AND LARGER. INSULATED GROUND CONDUCTORS SHALL HAVE GREEN COLORED INSULATION FOR ALL CONDUCTOR AWG AND/OR KCMIL TO COMPLY WITH NEC 250.119. NEUTRAL CONDUCTORS SHALL HAVE WHITE COLORED INSULATION FOR NO. 6 AWG AND SMALLER TO MEET THE REQUIREMENTS OF NEC 200.6. STANDARD COLORS FOR POWER WIRING AND BRANCH CIRCUITS SHALL BE AS FOLLOWS:  
  

120/240 VAC, 1 PHASE, 3 WIRE	
PHASE A	BLACK
PHASE B	RED
NEUTRAL	WHITE
GROUND	GREEN
- SEE RESPECTIVE SITE PLANS FOR SITE LEGEND INFORMATION.
- LTFMC DENOTES LIQUID TIGHT FLEXIBLE METAL CONDUIT UL LISTED, SUNLIGHT RESISTANT, & SUITABLE FOR GROUNDING. LIQUID TIGHT FLEXIBLE METAL CONDUIT AND ASSOCIATED FITTINGS SHALL BE U.L. LISTED TO MEET THE REQUIREMENTS OF NEC 350.6. LIQUID TIGHT FLEXIBLE METAL CONDUIT THAT IS USED FOR FLEXIBILITY (INCLUDING CONNECTIONS TO CCR'S & TRANSFORMERS) SHALL REQUIRE AN EXTERNAL BONDING JUMPER OR INTERNAL EQUIPMENT GROUNDING CONDUCTOR PER NEC 350.60. EXTERNAL BONDING JUMPERS USED WITH CCR INSTALLATIONS SHALL BE #6 AWG COPPER (MINIMUM). DO NOT INSTALL LTFMC THAT IS NOT UL LISTED. CONFIRM LTFMC BEARS THE UL LABEL PRIOR TO INSTALLATION.

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A.I.P. PROJ.: 3-17-0077-B13 IL PROJ.: PRG-4018						
Hanson Proj. No. 10A00053 Filename: E-001.DWG Scale: NONE Date: 03/10/11	LAYOUT	CAT	DRAWN	RSS	REVIEWED	MAM
HANSON			ARMSTRONG			
CONSTRUCT CROSSWIND RUNWAY 18-36			ELECTRICAL LEGEND AND ABBREVIATIONS			
42						
42 of 137 sheets						



**Vault Building Notes:**

THE PROPOSED ELECTRICAL VAULT BUILDING SHALL CONSIST OF A PRE-FABRICATED, PRE-ENGINEERED EQUIPMENT ENCLOSURE BUILDING WITH A CONCRETE FLOOR, STEEL SKID STRUCTURE AND FOUNDATION PEIRS OR WITH CONCRETE SLAB FOUNDATION.

THE PROPOSED ELECTRICAL VAULT BUILDING SHALL HAVE A NOMINAL 12 FOOT WIDE EXTERIOR (INTERIOR WIDTH SHALL NOT BE LESS THAN 11 FEET, ADJUST EXTERIOR WIDTH AS APPLICABLE) BY NOMINAL 29 FEET IN LENGTH (INTERIOR LENGTH SHALL NOT BE LESS THAN 27 FEET, ADJUST EXTERIOR AS APPLICABLE) BY NOMINAL 9 FEET HIGH INTERIOR (FLOOR TO CEILING).

- GENERAL NOTES:**
- SEE "PROPOSED AIRFIELD VAULT ONE-LINE DIAGRAM" FOR RATING AND CONDUIT AND CABLE INFORMATION FOR LOW VOLTAGE EQUIPMENT.
  - SEE "PROPOSED AIRFIELD VAULT ONE-LINE DIAGRAM" FOR LOW VOLTAGE INPUT POWER WIRING REQUIREMENTS TO CCR'S (CONSTANT CURRENT REGULATORS). SEE "HIGH VOLTAGE WIRING SCHEMATIC" FOR CCR OUTPUT WIRING REQUIREMENTS. SEE "AIRFIELD LIGHTING CONTROL WIRING SCHEMATIC" FOR CCR CONTROL WIRING REQUIREMENTS. PROVIDE 5 FEET MINIMUM CLEAR WORKING SPACE IN FRONT OF EACH CCR AND EACH SERIES PLUG CUTOFF.
  - CONSTANT CURRENT REGULATORS AND THEIR RESPECTIVE SERIES PLUG CUTOFFS SHALL BE CLEARLY LABELED TO IDENTIFY THE RESPECTIVE REGULATOR DESIGNATION, RUNWAY OR TAXIWAY SERVED, POWER SOURCE OR CIRCUIT, AND VOLTAGE SYSTEM.
  - SEE ELEVATION VIEWS FOR ADDITIONAL INFORMATION ON PROPOSED EQUIPMENT LAYOUTS.
  - COORDINATE CONDUIT & SLEEVE ENTRANCES THROUGH FLOOR SLAB AND WALLS.
  - SEE "VAULT GROUND BUS RISER DIAGRAM" FOR GROUNDING INFORMATION.

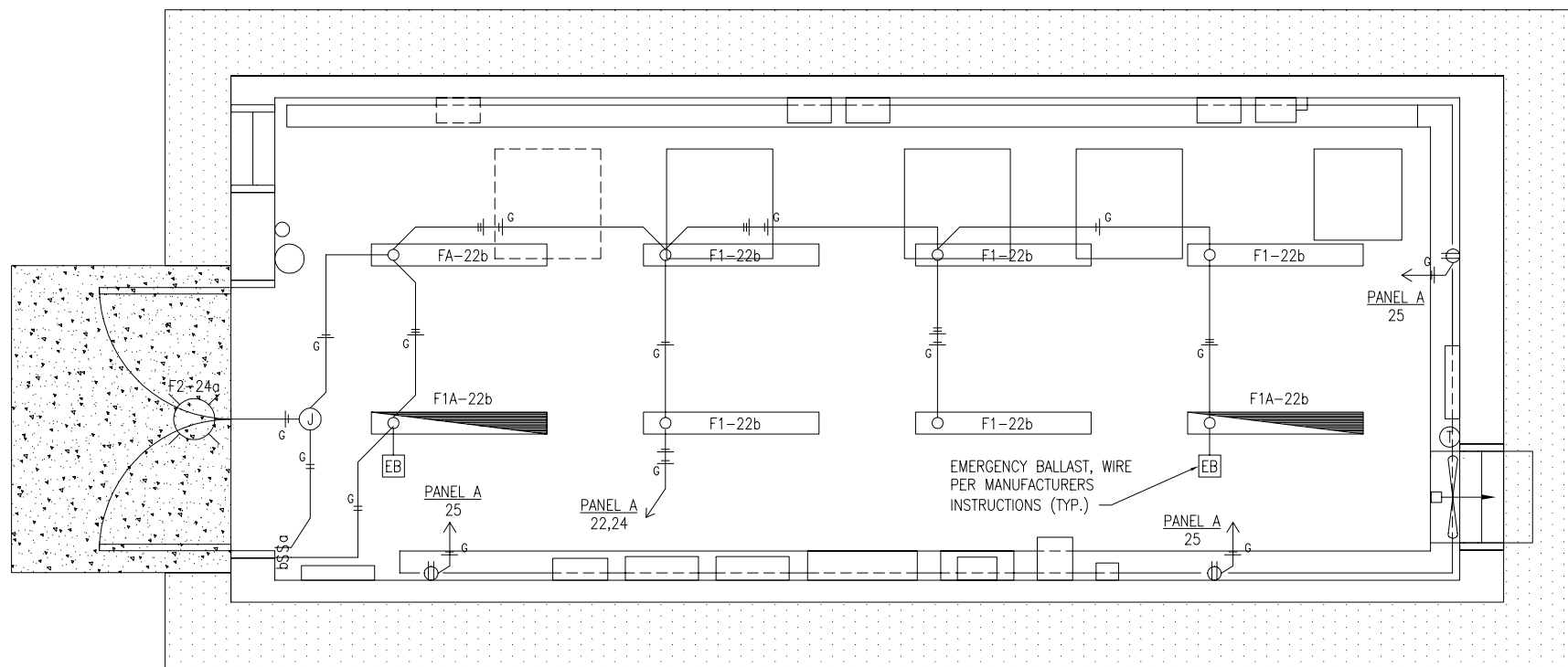
**VAULT ELECTRICAL EQUIPMENT PLAN**  
 SCALE 1/2"=1'-0"  
 1 0 2 4 FEET

**KEY NOTES:** #

- NOT USED.
- SERVICE CONDUCTORS IN 3 1/2" SCHED. 40 PVC CONDUIT FROM SERVICE ENTRANCE RATED ENCLOSED CIRCUIT BREAKER. CONTRACTOR SHALL FURNISH & INSTALL SERVICE CONDUCTORS & CONDUIT SEE "PROPOSED VAULT AREA ELECTRICAL PLAN" FOR ROUTING INFORMATION.
- VAULT MAIN DISTRIBUTION PANEL A. SEE SCHEDULE.
- AC SURGE PROTECTOR/TVSS.
- LIGHTING CONTACTOR PANEL. SEE "LIGHTING CONTACTOR PANEL DETAIL".
- L-854 RADIO CONTROL UNIT.
- RADIO RELAY INTERFACE PANEL WITH PHOTOCELL BYPASS SWITCH FOR AIRFIELD LIGHTING SYSTEM. SEE "AIRFIELD LIGHTING CONTROL WIRING SCHEMATIC" FOR WIRING REQUIREMENTS. MOUNT PHOTOCELL ABOVE ROOF LEVEL. FIELD VERIFY LOCATION FOR PROPER CONTROL AND OPERATION. PROVIDE SCHED 40 PVC NIPPLE AT ENTRY TO VAULT FOR ISOLATION. BOND EXTERIOR METAL CONDUIT TO GND RING WITH PIPE CLAMP AND BONDING CONDUCTOR.
- ELECTRIC WALL HEATER EH-1, SUITABLE FOR SURFACE MOUNTING WITH INTEGRAL THERMOSTAT. Q-MARK MODEL CWH3404, OR APPROVED EQUAL. HEATER SHALL BE MANUFACTURED IN THE UNITED STATES TO COMPLY WITH THE AIRPORT IMPROVEMENT PROGRAM BUY AMERICAN REQUIREMENT, & THE "BUY AMERICAN ACT". LOCATED HEATER SUCH THAT IT IS NOT LESS THAN 8" FROM ADJACENT WALLS OR EQUIPMENT.
- ELECTRIC WALL HEATER EH-2, SUITABLE FOR SURFACE MOUNTING WITH INTEGRAL THERMOSTAT. Q-MARK MODEL CWH3404, OR APPROVED EQUAL. HEATER SHALL BE MANUFACTURED IN THE UNITED STATES TO COMPLY WITH THE AIRPORT IMPROVEMENT PROGRAM BUY AMERICAN REQUIREMENT, & "BUY AMERICAN ACT". BOTTOM OF HEATER SHALL BE 8" (MIN.) ABOVE THE UPPER ELECTRICAL WIREWAY. COORDINATE WITH CCR INSTALLATION & FAN INSTALLATION. LOCATE HEATER ON WALL SUCH THAT IT IS NOT DIRECTLY BEHIND CCR. LOCATE HEATER SUCH THAT IT IS NOT LESS THAN 8" FROM ADJACENT WALLS OR EQUIPMENT.
- EXHAUST FAN EF-1, 3100 CFM (MINIMUM) AT .25" STATIC PRESSURE WITH 1/3 HP (MINIMUM), 120 VAC MOTOR, COOK MODEL 20S10D, OR APPROVED EQUAL. INCLUDE WALL HOUSING WITH GUARD, HEAVY DUTY BACK DRAFT DAMPER, ALUMINUM WEATHER-HOOD PAINTED TO MATCH BUILDING EXTERIOR, STAINLESS STEEL INSECT SCREEN, AND FRACTIONAL HP ELECTRICAL DISCONNECT. INSTALL FAN AS HIGH AS POSSIBLE. PROVIDE 120 VAC THERMOSTAT, AT 48" AFF. SEE EXHAUST FAN CONTROL SCHEMATIC FOR WIRING REQUIREMENTS. FAN SHALL BE MANUFACTURED IN THE UNITED STATES TO COMPLY WITH THE AIRPORT IMPROVEMENT PROGRAM BUY AMERICAN REQUIREMENT, & THE "BUY AMERICAN ACT".
- INTAKE LOUVER L-1, 24" WIDE BY 48" HIGH INTAKE LOUVER WITH STAINLESS STEEL INSECT SCREEN, FLANGED FRAME, 120 VAC LOW LEAK MOTORIZED DAMPER WITH LIMIT SWITCH, KYNAR FINISH MATCHING BUILDING EXTERIOR, RUSKIN MODEL ELF375DX, OR APPROVED EQUAL. SEE EXHAUST FAN CONTROL SCHEMATIC FOR WIRING REQUIREMENTS. LOUVER / DAMPER SHALL BE MANUFACTURED IN THE UNITED STATES TO COMPLY WITH THE AIRPORT IMPROVEMENT PROGRAM BUY AMERICAN REQUIREMENT, & THE "BUY AMERICAN ACT".
- 6" BY 6" LOW VOLTAGE WIREWAY. LABEL "LOW VOLTAGE" EVERY 6 FEET. INSTALL ABOVE HIGH VOLTAGE WIREWAY.
- 6" BY 6" HIGH VOLTAGE WIREWAY. LABEL "HIGH VOLTAGE" EVERY 6 FEET. INSTALL BELOW LOW VOLTAGE WIREWAY.
- NEW 7.5 KW REGULATOR FOR RUNWAY 18-36 (CCR). SEE GENERAL NOTE 1.
- NEW 7.5 KW REGULATOR FOR TAXIWAY 9-27 (CCR). SEE GENERAL NOTE 1.
- NEW 10 KW REGULATOR FOR RUNWAY 9-27 (CCR). SEE GENERAL NOTE 1.
- RELOCATED CROUSE-HINDS ELECTRIC TYPE FAA L-828 PART #82860-D-07-4-66-03, 7.5 KW REGULATOR FOR RUNWAY 9-27 (CCR). SEE GENERAL NOTE 1.
- SERIES PLUG CUTOFF (TYPE S-1) WITH ENCLOSURE.
- TRANSFER PAIR SERIES PLUG CUTOFFS (TYPE S-1) WITH ENCLOSURE, FOR RUNWAY 9-27.
- DOUBLE THROW FUSIBLE SAFETY SWITCH FOR RUNWAY 9-27 CCR'S.
- 4-4" PVC COATED GRSC CONDUITS AND 4-4" PVC COATED GRSC ELBOWS AT VAULT FROM LOW VOLTAGE WIREWAY TO LOW VOLTAGE HANDHOLE, SEE "PROPOSED VAULT AREA ELECTRICAL PLAN" FOR ROUTING INFORMATION.
- 4-4" PVC COATED GRSC CONDUITS AND 4-4" PVC COATED GRSC ELBOWS AT VAULT FROM HIGH VOLTAGE WIREWAY TO HIGH VOLTAGE HANDHOLE, SEE "PROPOSED VAULT AREA ELECTRICAL PLAN" FOR ROUTING INFORMATION.
- VEGETATION BARRIER CONSISTING OF A MIN. 6" PEA GRAVEL SURFACE OVER FILTER OR LANDSCAPING FABRIC. PROPOSED SURFACE TREATMENT WILL COVER ENTIRE AREA BENEATH VAULT STRUCTURE AS WELL AS 18" AROUND THE PERIMETER OF THE BUILDING EDGE. THE STONE AND FABRIC AS WELL AS ANY EQUIPMENT AND LABOR REQUIRED TO COMPLETE THIS TASK WILL BE CONSIDERED INCIDENTAL TO THE INSTALLATION OF THE PROPOSED ELECTRICAL VAULT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ENTRANCE PAD CONSTRUCTED OF 6" MIN. CONCRETE SLAB W/ 6X6-W5XW5 WELDED WIRE FABRIC ON A COMPACTED SUBGRADE. MINIMUM DIMENSIONS OF PAD WILL BE 7'x5'Dx6"H, SLOPED AT A MIN. OF 0.5"/FT AWAY FROM THE VAULT ENTRANCE. THE CONCRETE PAD WILL BE PLACED AT LEAST 3" INTO THE EXISTING GRADE. STEP INTO VAULT BUILDING WILL NOT EXCEED 7". PCC USED TO CONSTRUCT THE PAD WILL CONFORM TO ITEM 610. ALL MATERIALS, LABOR AND EQUIPMENT USED TO CONSTRUCT THE PAD INCLUDING ANY GRADING REQUIRED WILL BE CONSIDERED INCIDENTAL TO THE CONSTRUCTION OF THE PROPOSED ELECTRICAL VAULT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THE NUMBER, SIZE, DEPTH, REINFORCEMENT, AND LOCATION OF THE PROPOSED CONCRETE PIERS WILL BE COORDINATED WITH THE MANUFACTURER OF THE PROPOSED ELECTRICAL VAULT BUILDING. THE TOP OF THE PROPOSED PIERS WILL BE AT LEAST 4" ABOVE THE EXISTING GRADE.
- BRANCH CIRCUIT CONDUCTORS IN 2" PVC COATED GRSC. TO REFURBISHED AIRPORT BEACON, SEE "PROPOSED VAULT AREA ELECTRICAL PLAN" FOR ROUTING INFORMATION.
- PANEL B. SEE SCHEDULE.
- AWOS STEP-UP TRANSFORMER.
- L-854 RADIO ANTENNA CABLE IN 1" GRSC TO EXISTING TERMINAL BUILDING, SEE "PROPOSED VAULT AREA ELECTRICAL PLAN" FOR ROUTING INFORMATION. MOUNT ANTENNA ABOVE ROOF PLAN ON FOR PROPER OPERATION.
- PLASI 27 BOOST TRANSFORMER.

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Scale: AS SHOWN			
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<b>HANSON</b>		<b>ARMSTRONG</b>	
CONSTRUCT CROSSWIND RUNWAY 18-36		PROPOSED AIRPORT VAULT EQUIPMENT PLAN	
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- NOTES:**
- 20 AMP BRANCH CIRCUITS FOR LIGHTING AND RECEPTACLES SHALL USE #12 AWG THWN (MIN.)
  - LIGHT FIXTURES SHALL BE MANUFACTURED IN THE UNITED STATES OF AMERICA TO COMPLY WITH THE AIRPORT IMPROVEMENT PROGRAM BUY AMERICAN REQUIREMENTS. PROVIDE CERTIFICATION OF MANUFACTURE IN THE UNITED STATES WITH SHOP DRAWINGS SUBMITTAL.
  - ADJUST RECEPTACLE LOCATIONS WHERE NECESSARY TO ACCOMMODATE EQUIPMENT LAYOUT.
  - TEST EMERGENCY LIGHTING AND CONFIRM PROPER OPERATION WITH RESIDENT ENGINEER.

**Vault Lighting and Receptacle Plan**  
 SCALE 1/2" = 1'-0"  
 1 0 2 4 FEET

LIGHTING FIXTURE SCHEDULE						
FIXT. TYPE	DESCRIPTION	MANUFACTURER AND CATALOG NO.	LAMPS/WATTS	VOLTS	MOUNTING	REMARKS
F1	4 FT. WET LOCATION LISTED ENCLOSED AND GASKETED INDUSTRIAL FLUORESCENT LIGHT FIXTURE. IMPACT RESISTANT, UV RESISTANT, REINFORCED POLYESTER FIBERGLASS HOUSING, HIGH IMPACT ACRYLIC DIFFUSER, RAPID START, COLD WEATHER 0 DEG. F. ELECTRONIC BALLAST WITH LESS THAN OR EQUAL TO 10% THD	LITHONIA: DMW-2-32-ARDP-120-CW-GEB10RS-WLF-USPOM, OR APPROVED EQUAL	2-32W T8 4100K 59 TOTAL INPUT WATTS	120	SURFACE TO HARD CEILING	PROVIDE WET LOCATION FITTINGS INSTALLED IN TOP OF FIXTURE
F1A	SAME AS F1 EXCEPT PROVIDE AN EMERGENCY BALLAST CAPABLE OF OPERATING 2 LAMPS FOR 90 MINUTES AT 1100-1200 TOTAL LUMENS, BODINE #B50ST. NOTE: CONFIRM WITH LIGHT FIXTURE MFR. IF BALLAST WILL HAVE TO BE REMOTE MOUNTED NEAR FIXTURE AS INDICATED ON THE PLANS.	LITHONIA: DMW-2-32-ARDP-120-CW-GEB10RS-WLF-USPOM, OR APPROVED EQUAL	2-32W T8 4100K 59 TOTAL INPUT WATTS	120	SURFACE TO HARD CEILING	PROVIDE WET LOCATION FITTINGS INSTALLED IN TOP OF FIXTURE
F2	COMPACT FLUORESCENT WALL-PAK, ONE PIECE INJECTION MOLDED UV STABILIZED POLYCARBONATE HOUSING. HIGH PERFORMANCE SPECULAR ANODIZED SEGMENTED REFLECTOR, ONE PIECE HIGH TEMPERATURE SILICONE GASKET, MEDIUM BRONZE FINISH. HIGH POWER FACTOR ELECTRONIC BALLAST WITH LESS THAN OR EQUAL TO 10% THD, UL LISTED FOR WET LOCATIONS	LITHONIA: TWA-42TRT-120-SF-CR-DMB-LPI-USPOM, OR APPROVED EQUAL	1-42W TRT 4100K 47 TOTAL INPUT WATTS	120	SURFACE TO WALL ABOVE EXTERIOR DOOR APPROX. 4" ABOVE TOP OF DOOR FRAME	CONNECT TO WALL SWITCH LOCATED ON THE INSIDE OF THE BUILDING

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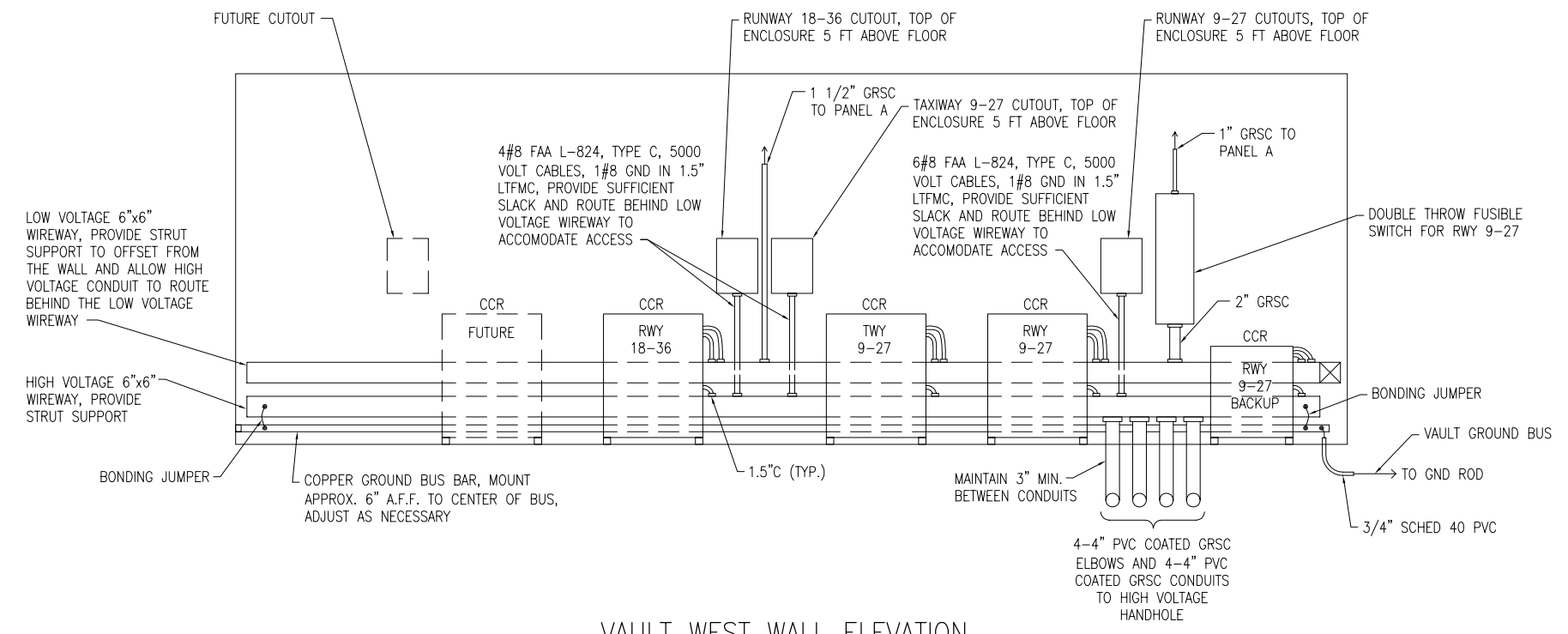
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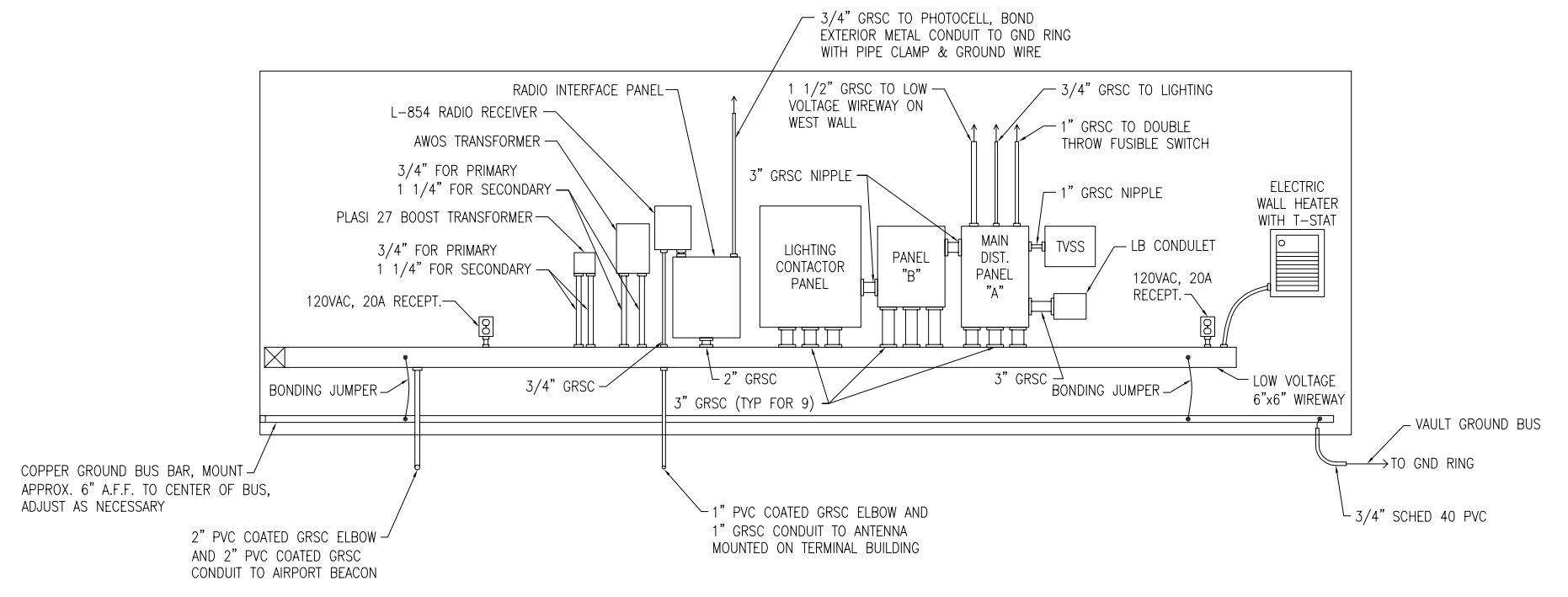
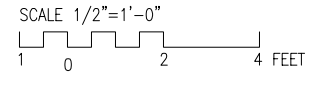


CONSTRUCT CROSSWIND  
 RUNWAY 18-36  
 PROPOSED AIRPORT VAULT  
 LIGHTING AND RECEPTACLE PLAN

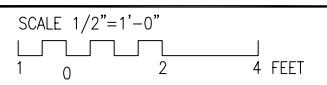




VAULT WEST WALL ELEVATION



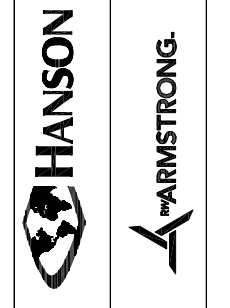
VAULT EAST WALL ELEVATION



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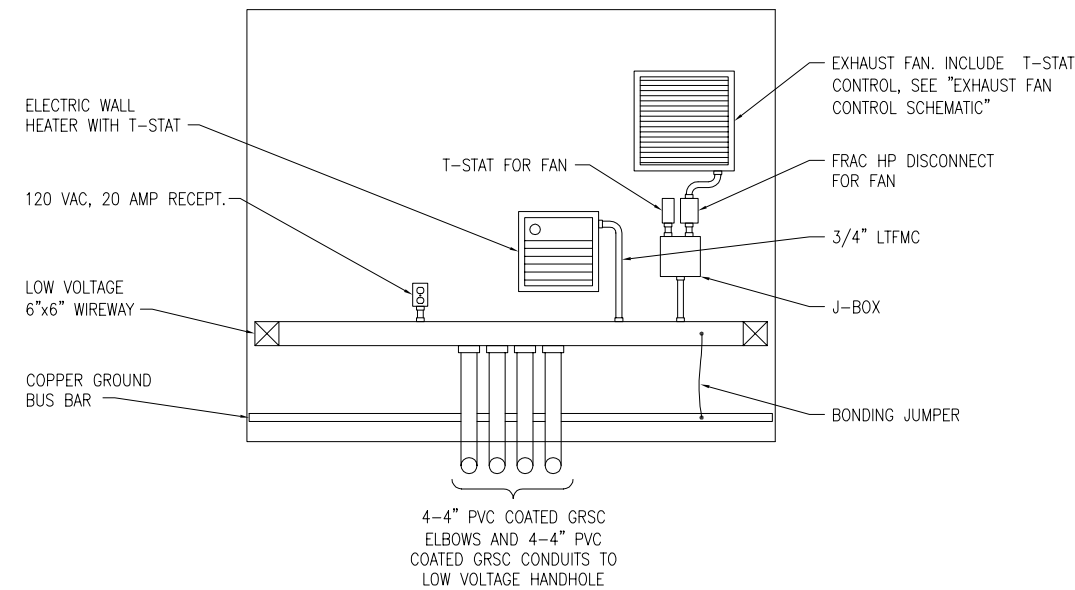
CONSTRUCT CROSSWIND  
RUNWAY 18-36  
PROPOSED AIRPORT VAULT  
WALL ELEVATIONS (SHEET 1)

PANEL A SCHEDULE											
DESIGNATION: PANEL A				MAINS: 400 AMP MAIN CIRCUIT BREAKER							
LOCATION: AIRPORT VAULT				BUS SIZE: 400 AMP							
VOLTAGE: 120/240				PANEL MOUNTING: SURFACE							
PHASE: 1 PHASE, 3 WIRE				ALL BREAKERS: 10,000 A.I.C. (MINIMUM)							
CKT. NO.	LOAD DESCRIPTION	KVA	AMPS	POLE	PHASE	CKT. BKR.	AMPS	POLE	KVA	LOAD DESCRIPTION	CKT. NO.
1	PANEL B	4.19	100	2	A					AC SURGE PROTECTOR / TVSS	2
3		4.25			B						4
5	RUNWAY 9-27 CCR	2.81	80	2	A				2.38	RUNWAY 18-36 CCR	6
7		2.81			B				2.38		8
9	TAXIWAY 9-27 CCR	3.44	60	2	A					FUTURE TAXIWAY 18-36 CCR	10
11		3.44			B						12
13	HEATER EH-1	2.00	30	2	A				2.00	HEATER EH-2	14
15		2.00			B				2.00		16
17	AWOS	0.96	30	2	A					SPARE	18
19		0.96			B						20
21	VAULT EXHAUST FAN	1.00	20	1	A				0.55	VAULT INTERIOR LIGHTING	22
23	NON-DIRECTIONAL BEACON (NDB)	2.00	30	1	A				0.05	VAULT EXTERIOR LIGHTING	24
25	VAULT RECEPTACLES	0.54	20	1	A				1.00	L-854 RADIO POWER & CONTROL	26
27	SPARE		30	1	A					SPARE	28
29	SPARE		20	1	A					SPARE	30
31	BLANK				A					BLANK	32
33	BLANK				B					BLANK	34
35	BLANK				A					BLANK	36
37	BLANK				B					BLANK	38
39	BLANK				A					BLANK	40
41	BLANK				B					BLANK	42
TOTAL CONNECTED LOAD:					20.87	19.89	TOTAL =		40.76		

400 AMP, 120/240 VAC, 1 PHASE, 3 WIRE, 42 CIRCUIT PANELBOARD WITH 400 AMP, 2 POLE MAIN BREAKER WITH 22,000 AIC AT 240 VAC IN A NEMA 1 ENCLOSURE, UL LISTED SUITABLE FOR SERVICE ENTRANCE. INCLUDE SEPARATE COPPER GROUND BAR KIT. ALL FEEDER AND BRANCH BREAKERS SHALL BE BOLT-ON TYPE WITH 10,000 AIC RATING AT 120/240 VAC. PANELBOARD SHALL BE SQUARE D OR APPROVED EQUAL.

NOTES:

- PANELBOARD BUSES SHALL BE COPPER, NEUTRAL SHALL BE COPPER. EQUIPMENT GROUND BAR SHALL BE COPPER.
- ALL BRANCH CIRCUIT & FEEDER BREAKERS SHALL BE BOLT-ON TYPE WITH 10,000 AIC AT 120/240 VAC.
- INCLUDE ENGRAVED, PHENOLIC OR PLASTIC LEGEND PLATE LABELED "VAULT SERVICE PANEL A, 120/240 VAC, 1PH, 3W". INCLUDE LEGEND PLATE FOR THE MAIN BREAKER LABELED "SERVICE DISCONNECT".
- PANELBOARD SHALL BE MANUFACTURED IN THE UNITED STATES TO COMPLY WITH THE AIRPORT IMPROVEMENT PROGRAM BUY AMERICAN REQUIREMENTS. PROVIDE CERTIFICATION OF MANUFACTURE IN THE UNITED STATES WITH SHIP DRAWING SUBMITTAL.
- CIRCUIT BREAKERS AND WIRING SHALL BE SIZED FOR THE ACTUAL EQUIPMENT FURNISHED IN CONFORMANCE WITH THE RESPECTIVE MANUFACTURER'S RECOMMENDATION AND NEC. CONTRACTOR SHALL ADJUST CIRCUIT BREAKER SIZES & WIRING WHERE APPLICABLE TO CONFORM WITH MFR RECOMMENDATIONS AND NEC.



VAULT NORTH WALL ELEVATION

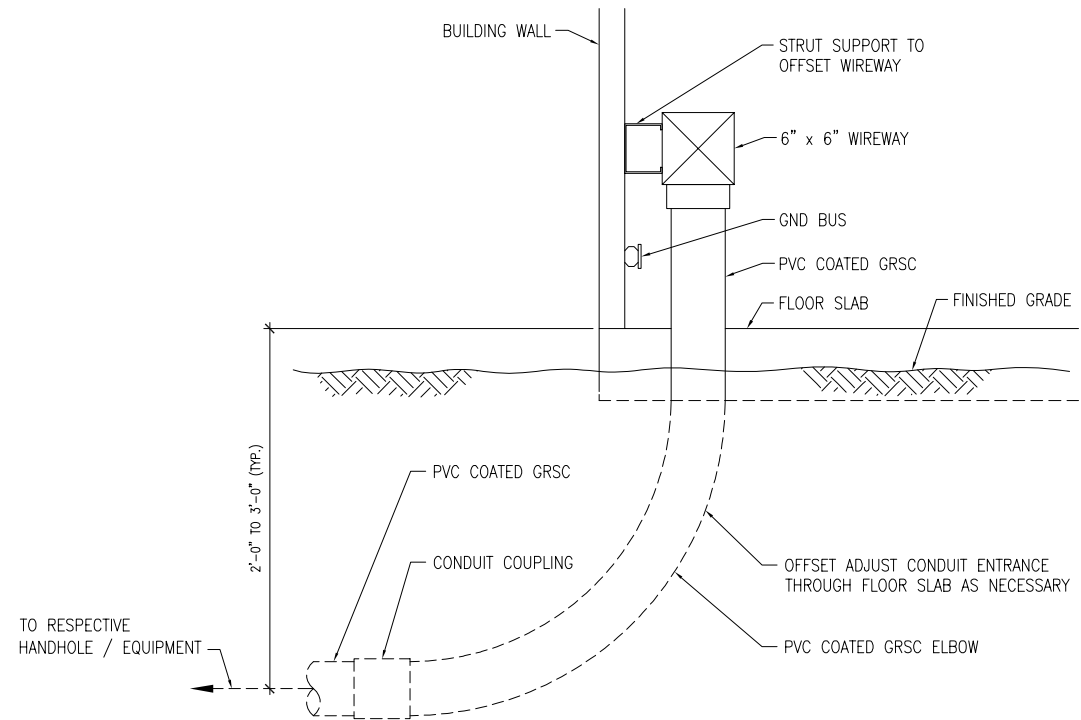
SCALE 1/2"=1'-0"  
 1 0 2 4 FEET

PANEL B SCHEDULE											
DESIGNATION: PANEL B				MAINS: M.L.O.							
LOCATION: AIRPORT VAULT				BUS SIZE: 100 AMP							
VOLTAGE: 120/240				PANEL MOUNTING: SURFACE							
PHASE: 1 PHASE, 3 WIRE				ALL BREAKERS: 10,000 A.I.C. (MINIMUM)							
CKT. NO.	LOAD DESCRIPTION	KVA	AMPS	POLE	PHASE	CKT. BKR.	AMPS	POLE	KVA	LOAD DESCRIPTION	CKT. NO.
1	PLASI 18	1.50	20	1	A				0.75	AIRPORT BEACON	2
3	PLASI 36	1.50	20	1	B				0.75		4
5	PLASI 9	1.50	20	1	A				0.44	APRON LIGHT	6
7	PLASI 27	1.50	20	1	B				0.50	WIND TEE / WIND CONE	8
9	SPARE		20	2	A					SPARE	10
11					B						12
13	SPARE		20	2	A					SPARE	14
15					B						16
17	SPARE		30	2	A					SPARE	18
19					B						20
21	BLANK				A					BLANK	22
23	BLANK				B					BLANK	24
25	BLANK				A					BLANK	26
27	BLANK				B					BLANK	28
29	BLANK				A					BLANK	30
TOTAL CONNECTED LOAD:					4.19	4.25	TOTAL =		8.44		

100 AMP, 120/240 VAC, 1 PHASE, 3 WIRE, 30 CIRCUIT PANELBOARD WITH MAIN LUGS IN A NEMA 1 ENCLOSURE. INCLUDE SEPARATE GROUND BAR KIT. ALL BRANCH BREAKERS SHALL BE BOLT-ON TYPE WITH 10,000 AIC RATING AT 120/240 VAC. PANELBOARD SHALL BE SQUARE D TYPE NQ OR APPROVED EQUAL.

NOTES:

- PANELBOARD BUSES SHALL BE COPPER, NEUTRAL SHALL BE COPPER. EQUIPMENT GROUND BAR SHALL BE COPPER.
- ALL BREAKERS SHALL BE BOLT-ON TYPE WITH 10,000 AIC AT 120/240 VAC.
- PROVIDE LEGEND PLATE FOR PANELBOARD LABELED "PANEL B, 120, 120/240 VAC, 1PH, 3W FED FROM SERVICE PANEL A".
- CIRCUIT BREAKERS AND WIRING SHALL BE SIZED FOR THE ACTUAL EQUIPMENT FURNISHED IN CONFORMANCE WITH THE RESPECTIVE MANUFACTURER'S RECOMMENDATION AND NEC. CONTRACTOR SHALL ADJUST CIRCUIT BREAKER SIZES & WIRING WHERE APPLICABLE TO CONFORM WITH MFR RECOMMENDATIONS AND NEC.
- PANELBOARD SHALL BE MANUFACTURED IN THE UNITED STATES TO COMPLY WITH THE AIRPORT IMPROVEMENT PROGRAM BUY AMERICAN REQUIREMENTS. PROVIDE CERTIFICATION OF MANUFACTURE IN THE UNITED STATES WITH SHIP DRAWING SUBMITTAL.



CONDUIT ENTRANCE DETAIL

SCALE 1 1/2"=1'-0"  
 0.5 0 1 2 FEET

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PARIS, ILLINOIS**

A.I.P. PROJ.: 3-17-0077-B13  
IL PROJ.: PRG-4018

Hanson Proj. No. 10A0053  
 Filename: E-202.DWG  
 Scale: AS SHOWN  
 Date: 03/10/11

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DRAWN	RSS
REVIEWED	MAM

**HANSON**

**ARMSTRONG**

CONSTRUCT CROSSWIND  
RUNWAY 18-36

PROPOSED AIRPORT VAULT  
WALL ELEVATIONS (SHEET 2)

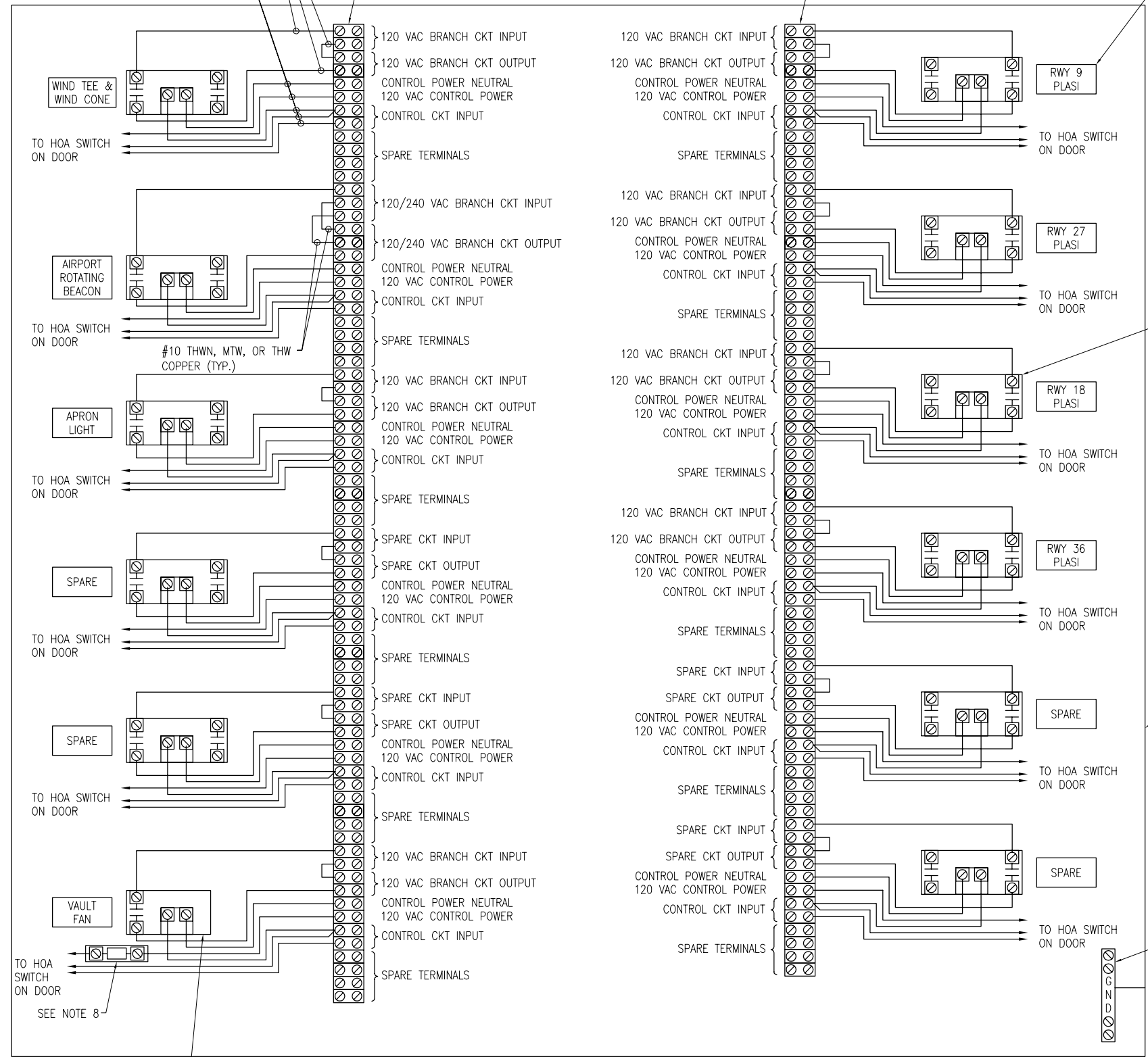
#10 THWN, MTW, OR THW COPPER (TYP. FOR POWER CIRCUITS TO CONTACTORS)

#12 THWN, MTW, OR THW COPPER (TYP. FOR CONTROL CIRCUITS TO LIGHTING CONTACTORS)

NEMA TYPE TERMINAL BLOCKS RATED 85A, 600 VOLT SUITABLE FOR THE FOLLOWING WIRE COMBINATIONS  
 1 #4 AWG  
 1 #6 AWG  
 1-2 #8 AWG  
 1-4 #10 AWG  
 1-5 #12 AWG  
 TERMINAL BLOCKS SHALL BE SQUARE D CLASS 9080 TYPE GC6 OR APPROVED EQUAL. IEC RATED TERMINAL BLOCKS ARE NOT ACCEPTABLE.

NEMA TYPE TERMINAL BLOCKS RATED 85A, 600 VOLT SUITABLE FOR THE FOLLOWING WIRE COMBINATIONS  
 1 #4 AWG  
 1 #6 AWG  
 1-2 #8 AWG  
 1-4 #10 AWG  
 1-5 #12 AWG  
 TERMINAL BLOCKS SHALL BE SQUARE D CLASS 9080 TYPE GC6 OR APPROVED EQUAL. IEC RATED TERMINAL BLOCKS ARE NOT ACCEPTABLE.

FURNISH & INSTALL ENGRAVED LEGEND PLATES TO IDENTIFY EACH RELAY/CONTACTOR



30 AMP, 600 VAC, 2 POLE ELECTRICALLY HELD LIGHTING CONTACTOR WITH 120 VAC COIL, SQUARE D CLASS 8903, TYPE SMO1V02, OR APPROVED EQUAL (TYP. FOR 11)

NEMA 12 ENCLOSURE WITH HINGED DOOR SIZED AS REQUIRED TO HOUSE LIGHTING CONTACTORS, TERMINAL BLOCKS, WIRING & INTERFACE TO EXISTING CONDUITS, MINIMUM 36"Hx24"Wx12"D AS MANUFACTURED BY HOFFMAN OR APPROVED EQUAL

EQUIPMENT GROUND BAR ADEQUATELY SIZED FOR ALL GROUND WIRES TO AND FROM LIGHTING CONTACTOR PANEL. INSTALL ONE GROUND WIRE PER TERMINAL

NOTES:

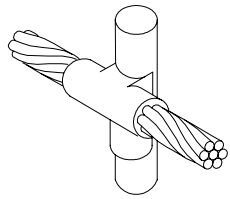
- 15 AMP & 20 AMP INPUT POWER/BRANCH CIRCUITS SHALL BE #10 AWG COPPER THWN FROM THE RESPECTIVE POWER SOURCE TO THE LIGHTING CONTACTOR PANEL. 30 AMP INPUT POWER/BRANCH CIRCUITS SHALL BE #8 AWG COPPER THWN (MIN.) FROM THE RESPECTIVE POWER SOURCE TO THE LIGHTING CONTACTOR PANEL.
- INPUT CONTROL CIRCUITS SHALL BE #12 AWG COPPER THWN.
- FOR 120 VAC BRANCH CIRCUITS THE NEUTRAL CONDUCTOR SHALL NOT BE SWITCHED THROUGH THE RELAY CONTACTS. USE TERMINAL BLOCKS TO TRANSITION FROM VAULT BRANCH CIRCUIT WIRING TO FIELD WIRING.
- PROVIDE #10 AWG COPPER BONDING JUMPER FROM PANEL ENCLOSURE FRAME TO ENCLOSURE DOOR.
- PROVIDE 3-POSITION MAINTAINED CONTACT "HAND-OFF-AUTO" SELECTOR SWITCH FOR EACH LIGHTING CONTACTOR & MOUNT ON LIGHTING CONTACTOR PANEL ENCLOSURE DOOR. SELECTOR SWITCH SHALL BE SQUARE D CLASS 9001, TYPE KS43FBH13, OR APPROVED EQUAL. INCLUDE LEGEND PLATE TO IDENTIFY THE DEVICE CONTROLLED (EX: "WIND CONE" OR "AIRPORT ROTATING BEACON").
- SEE "LIGHTING CONTACTOR SCHEMATIC" AND "EXHAUST FAN CONTROL SCHEMATIC" FOR ADDITIONAL INFORMATION ON WIRING.
- FUSING FOR FAN CIRCUIT CONTROL WIRING SHALL BE 10 AMP, 600 VAC, BUSSMANN CATALOG FNQ-R-10, OR APPROVED EQUAL, WITH FUSE BLOCKS, WITH BOX LUG TERMINALS, SIZED AS REQUIRED FOR THE RESPECTIVE APPLICATION. INCLUDE HARDWARE FOR MOUNTING. PROVIDE ONE BOX (5 MINIMUM QUANTITY) OF EACH TYPE AND SIZE OF FUSE, UPON COMPLETION OF THE JOB FOR USE AS SPARES.
- INCLUDE LEGEND PLATE LABELED "NOTICE: CONTACTORS HAVE REMOTE LOCATED CONTROLS AND MAY ACTIVATE AT ANY TIME".
- 120/240 VAC PHASE "A" CONDUCTORS SHALL HAVE BLACK COLORED INSULATION. 120/240 VAC PHASE "B" CONDUCTORS SHALL HAVE RED COLORED INSULATION. NEUTRAL CONDUCTORS SHALL HAVE WHITE COLORED INSULATION. INSULATED EQUIPMENT GROUND WIRES SHALL HAVE GREEN COLORED INSULATION.
- CONTROL PANEL FOR AIRFIELD NAVAIDS & VAULT FAN SHALL BE MANUFACTURED BY A UL 508 INDUSTRIAL CONTROL PANEL BUILDER OR AN FAA APPROVED L-821 PANEL BUILDER, AND SHALL BE MANUFACTURED IN THE UNITED STATES TO COMPLY WITH THE AIRPORT IMPROVEMENT PROGRAM BUY AMERICAN REQUIREMENT AND THE "BUY AMERICAN ACT".
- THE AIRPORT ROTATING BEACON CIRCUIT SHALL HAVE PHASE 'A' SWITCHED THROUGH THE LIGHTING CONTACTOR. PHASE 'B' SHALL BE UNSWITCHED FROM THE POWER SOURCE TO THE LOAD CENTER AT THE AIRPORT ROTATING BEACON

120 VAC, NEMA SIZE 0 (MINIMUM), 1 POLE, FULL VOLTAGE CONTACTOR, SQUARE D CLASS 8502, TYPE SBG5V02 OR APPROVED EQUAL

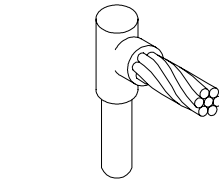
CONTROL PANEL FOR AIRFIELD NAVAIDS AND VAULT FAN

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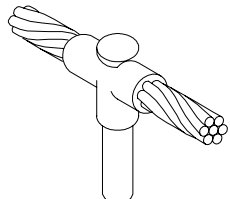
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Filename: E-501.DWG		IL PROJ.: PRG-4018	
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CONSTRUCT CROSSWIND RUNWAY 18-36		LIGHTING CONTACTOR PANEL DETAILS	
47		47 of 137 sheets	



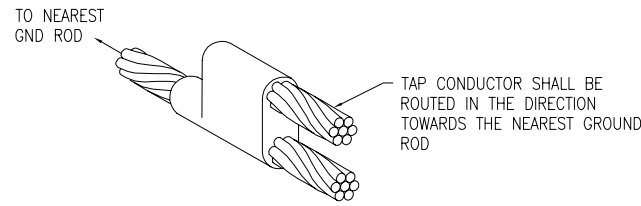
CABLE TO GROUND ROD



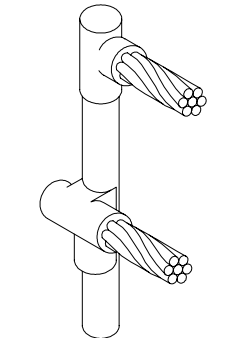
CABLE TO GROUND ROD



CABLE TO GROUND ROD



CABLE TO CABLE HORIZONTAL PARALLEL TAP

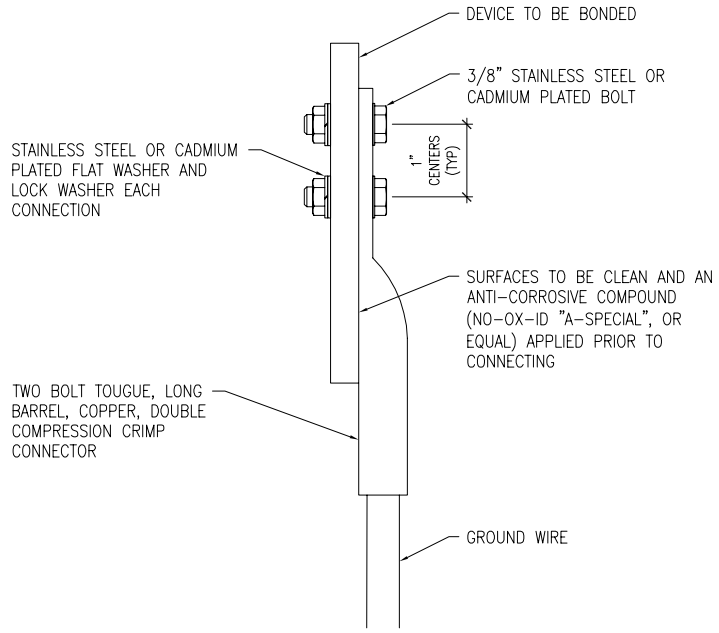


CABLES TO GROUND ROD

NOTES:

- ALL BELOW GRADE CONNECTIONS TO GROUND RODS & GROUND RING CONDUCTORS SHALL BE EXOTHERMIC WELD TYPE CONNECTIONS. EXOTHERMIC WELDS SHALL BE CADWELD AS MANUFACTURED BY ERICO PRODUCTS, SOLON, OHIO, UTLRAWELD AS MANUFACTURED BY HARGER LIGHTNING PROTECTION & GROUNDING EQUIPMENT, GRAYSLAKE, IL, OR THERMOWELD AS MANUFACTURED BY CONTINENTAL INDUSTRIES, TULSA, OKLAHOMA. VERIFY PROPER SIZES, MOLDS, TYPES, AND REQUIREMENTS FOR THE RESPECTIVE APPLICATION WITH THE MANUFACTURER, AND INSTALL PER THEIR DIRECTIONS.
- FOR APPLICATIONS TO GALVANIZED STEEL OR PAINTED STEEL, REMOVE GALVANIZING AND/OR PAINT & CLEAN THE SURFACE TO EXPOSE BARE STEEL BEFORE MAKING EXOTHERMIC WELD CONNECTION.
- INDIVIDUAL GROUNDING ELECTRODE CONDUCTORS SHALL NOT BE INSTALLED IN METAL CONDUIT. INSTALL GROUNDING ELECTRODE CONDUCTORS IN SCHED 40 PVC CONDUIT AS REQUIRED IN FOUNDATIONS, FOR PROTECTION, WHERE ENTERING ENCLOSURES, ETC. WHERE PLASTIC CONDUIT IS USED FOR INDIVIDUAL GROUND WIRES, DO NOT COMPLETELY ENCIRCLE THE CONDUIT WITH FERROUS AND/OR MAGNETIC MATERIALS. WHERE METAL CLAMPS ARE INSTALLED USE NYLON BOLTS, NUTS, WASHERS, & SPACERS TO INTERRUPT A COMPLETE METALLIC PATH FROM ENCIRCLING THE CONDUIT.

EXOTHERMIC WELD DETAILS



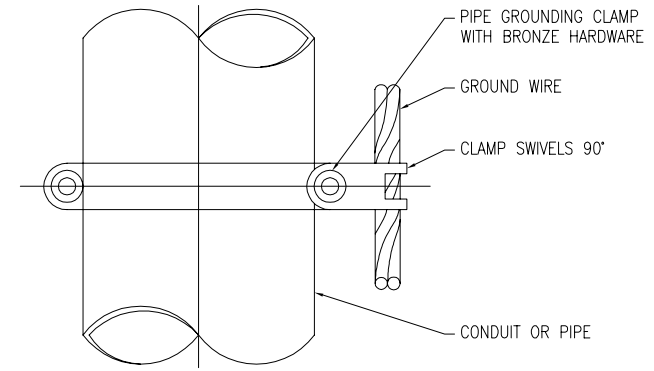
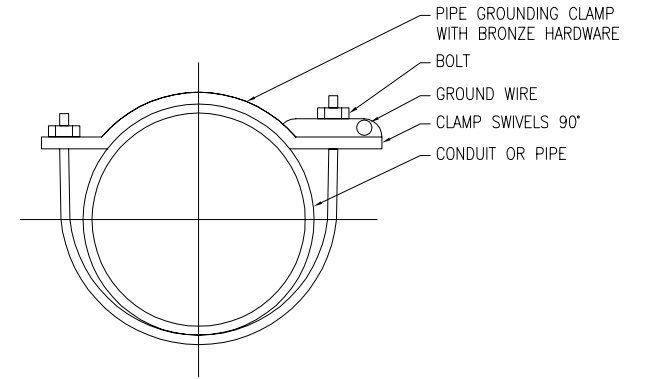
2 HOLE LONG BARREL COMPRESSION LUG TABLE

WIRE SIZE	BURNDY CAT. NO.	THOMAS & BETTS CAT. NO.	PENN-UNION CAT. NO.
#8 AWG STRANDED	YA8C-2TC38	256-30695-1157	BBLU-8D-2TC38
#6 AWG SOLID	YA8C-2TC38 OR YGA6C-2TC38E2G1		
#6 AWG STRANDED	YA6C-2TC38	256-30695-1158	BBLU-6D-2TC38
#4 AWG STRANDED	YA4C-2TC38	256-30695-1159	BBLU-4D-2TC38
#2 AWG STRANDED	YA2C-2TC38	256-30695-1160	BBLU-2D-2TC38
#2 AWG SOLID	YA3C-2TC38	256-30695-1161	BBLU-3D-2TC38
#1/0 AWG STRANDED	YA25-2TC38	256-30695-1162	BBLU-1/0D-2TC38
#2/0 AWG STRANDED	YA26-2TC38	256-30695-1116	BBLU-2/0D-2TC38
#3/0 AWG STRANDED	YA27-2TC38	54816BE	BBLU-3/0D-2TC38
#4/0 AWG STRANDED	YA28-2TC38	256-30695-1117	BBLU-4/0D-2TC38

NOTES:

- ALL CONNECTIONS TO GROUND BUS BAR SHALL BE WITH 2 HOLE LONG BARREL COMPRESSION LUGS BOLTED TO THE BUS BAR.
- GROUND WIRE CONNECTIONS TO EQUIPMENT SHALL BE WITH 2 HOLE TONGUE LONG BARREL COMPRESSION LUGS BOLTED TO THE DEVICE OR WITH THE RESPECTIVE EQUIPT MANUFACTURER'S LUG OR TERMINAL WHERE APPLICABLE.
- GROUNDING ELECTRODE CONDUCTORS, BONDING JUMPERS, & INDIVIDUAL GROUND WIRES SHALL NOT BE INSTALLED IN METAL CONDUIT. WHERE PLASTIC CONDUIT IS USED FOR INDIVIDUAL GROUND WIRES, DO NOT COMPLETELY ENCIRCLE THE CONDUIT WITH FERROUS AND/OR MAGNETIC MATERIALS. WHERE METAL CLAMPS ARE INSTALLED USE NYLON BOLTS, NUTS, WASHERS, & SPACERS TO INTERRUPT A COMPLETE METALLIC PATH FROM ENCIRCLING THE CONDUIT.
- ALL CONNECTIONS SHALL BE COATED WITH A CORROSION PREVENTATIVE COMPOUND (SANCHEM INC. NO-OX-ID "A-SPECIAL", BURNDY PENETROX E, OR EQUAL) BEFORE JOINING. ALL COPPER BUS BARS SHALL BE CLEANED PRIOR TO MAKING CONNECTIONS TO REMOVE SURFACE OXIDATION. CLEAN SURFACES, OF RESPECTIVE DEVICES TO BE BONDED, TO BARE METAL, PER NEC 250-12.

GROUND LUG CONNECTION DETAIL



PIPE GROUNDING CLAMP TABLE

BURNDY CAT. NO.	PIPE SIZE
GAR3902-BU	1/2" - 1"
GAR3903-BU	1 1/4" - 2"
GAR3904-BU	2 1/2" - 3 1/2"
GAR3905-BU	5" - 5"
GAR3906-BU	6"

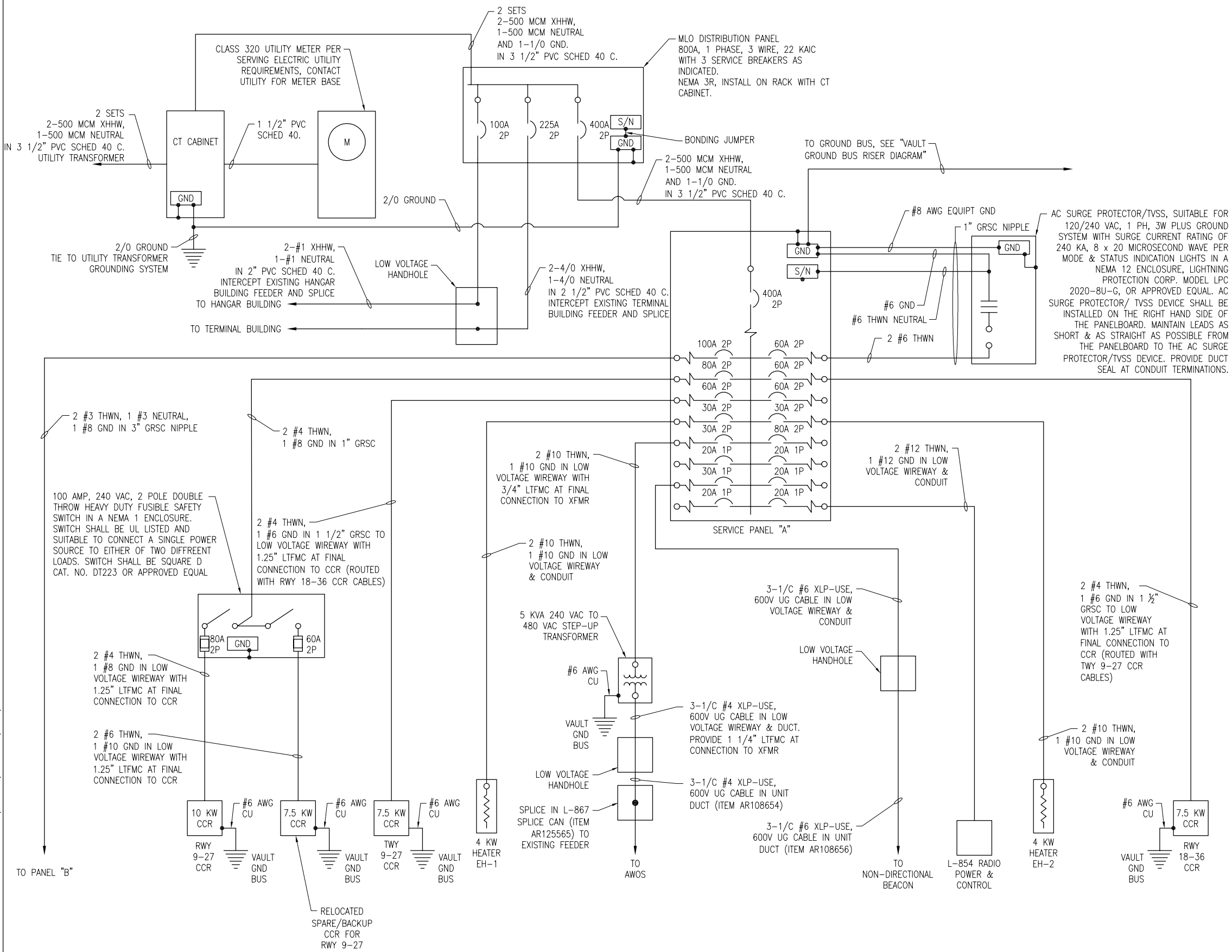
NOTES:

- PIPE GROUNDING CLAMPS SHALL HAVE BRONZE HARDWARE, BE CORROSION RESISTANT, SUITABLE FOR DIRECT BURIAL IN EARTH OR CONCRETE, & UL467 LISTED.

PIPE/CONDUIT GROUNDING CLAMP DETAIL

MAR 11, 2011 10:15 AM MMIRANDE P:\ELEC PROJECTS\2011\4100-000 EDGAR COUNTY ELECTRICAL VAULT DRAWINGS\PRODUCTION\PLANS\E-502.DWG

REVISION									
DATE									
<p><b>EDGAR COUNTY AIRPORT</b> <b>PARIS, ILLINOIS</b></p> <p style="font-size: small;">A.I.P. PROJ.: 3-17-0077-B13 IL PROJ.: PRG-4018</p>									
Hanson Proj. No. 10A0053	Filename: E-502.DWG	Scale: NONE	Date: 03/10/11	CAT	RSS	MAM			
LAYOUT	DRAWN	REVIEWED							
<p><b>HANSON</b></p>					<p><b>ARMSTRONG</b></p>				
<p>CONSTRUCT CROSSWIND RUNWAY 18-36</p>					<p>GROUNDING DETAILS</p>				
<p style="font-size: large; font-weight: bold;">48</p> <p style="font-size: x-small;">48 of 137 sheets</p>									



NOTES:

- ALL VAULT WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT MANAGER. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).
- ALL ELECTRICAL EQUIPMENT SHALL BE INSTALLED IN CONFORMANCE WITH NFPA 70 - NATIONAL ELECTRICAL CODE (NEC) MOST CURRENT ISSUE IN FORCE, THE RESPECTIVE EQUIPMENT MANUFACTURER'S DIRECTIONS AND ALL OTHER APPLICABLE LOCAL CODES, LAWS, ORDINANCES AND REQUIREMENTS IN FORCE. ANY INSTALLATIONS WHICH VOID THE U.L. LISTING, ETL LISTING, (OR OTHER THIRD PARTY LISTING) AND/OR THE MANUFACTURER'S WARRANTY OF A DEVICE WILL NOT BE PERMITTED.
- ALL EQUIPMENT NOT LABELED AS EXISTING IS NEW.
- BEACON FEEDER SHALL HAVE COLOR CODED INSULATION AS FOLLOWS:  

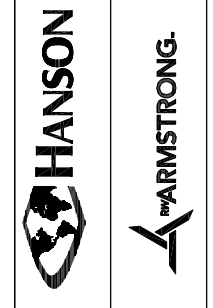
120/240 VAC CIRCUITS	
PHASE A	BLACK
PHASE B	RED OR BLACK WITH RED TAPE
NEUTRAL	WHITE
GROUND	GREEN
- PROVIDE NEMA 4 HUBS FOR ALL CONDUIT ENTRIES INTO NEMA 4 RATED ENCLOSURES. PROVIDE NEMA 4 HUBS FOR ALL CONDUIT ENTRIES INTO THE NEMA 3R LOAD CENTER ENCLOSURE.
- INSTALL OBSTRUCTION LIGHTING ON AIRPORT ROTATING BEACON TOWER IN CONFORMANCE WITH FAA AC NO. 150/5340-30 AND FAA AC NO. 150/5370-10, ITEM L-101, INSTALLATION OF AIRPORT ROTATING BEACONS.
- ALL CONDUCTORS/WIRING SHALL BE COPPER.
- CONTRACTOR SHALL CONFIRM POWER REQUIREMENTS WITH THE ACTUAL NAMEPLATE ON EACH CONSTANT CURRENT REGULATOR (OR OTHER RESPECTIVE EQUIPMENT) AND ADJUST CIRCUIT BREAKER, WIRE SIZES & CONDUIT SIZES TO CONFORM WITH NEC & MANUFACTURER'S RECOMMENDATIONS WHERE APPLICABLE. WIRE SIZES SHOWN ON THE PLANS ARE MINIMUM.
- HIGH VOLTAGE & LOW VOLTAGE CIRCUITS SHALL NOT BE INSTALLED IN THE SAME WIREWAY, CONDUIT, HANDHOLE, JUNCTION BOX, OR RACEWAY.
- LTFMC DENOTES LIQUID TIGHT FLEXIBLE METAL CONDUIT UL LISTED, SUNLIGHT RESISTANT, & SUITABLE FOR GROUNDING. LIQUID TIGHT FLEXIBLE METAL CONDUIT AND ASSOCIATED FITTINGS SHALL BE U.L. LISTED TO MEET THE REQUIREMENTS OF NEC 350.6. LIQUID TIGHT FLEXIBLE METAL CONDUIT THAT IS USED FOR FLEXIBILITY (INCLUDING CONNECTIONS TO CCR'S & TRANSFORMERS) SHALL REQUIRE AN EXTERNAL BONDING JUMPER OR INTERNAL EQUIPMENT GROUNDING CONDUCTOR PER NEC 350.60. EXTERNAL BONDING JUMPERS USED WITH CCR INSTALLATIONS SHALL BE #6 AWG COPPER (MINIMUM). DO NOT INSTALL LTFMC THAT IS NOT UL LISTED. CONFIRM LTFMC BEARS THE UL LABEL PRIOR TO INSTALLATION.
- COORDINATE REMOVAL OF EXISTING ELECTRICAL UTILITY METERS WITH ELECTRICAL UTILITY AND AIRPORT AUTHORITY.

REVISION	DATE

EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

IL PROJ.: PRG-4018  
A.I.P. PROJ.: 3-17-0077-B13

Hanson Proj. No. 10A00053	File Name: E-601.DWG	Scale: NONE	Date: 03/10/11
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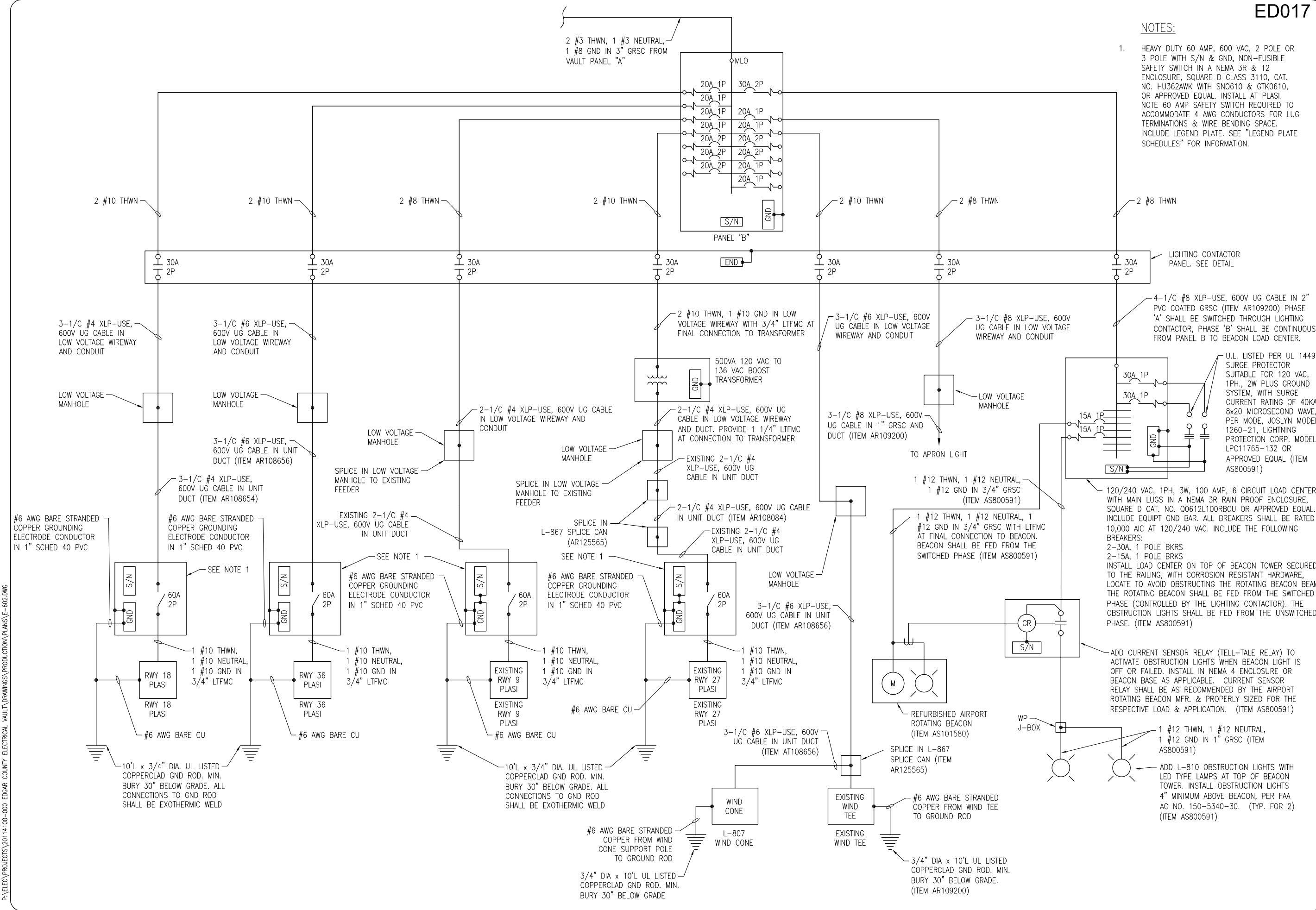
CONSTRUCT CROSSWIND  
RUNWAY 18-36

PROPOSED AIRPORT VAULT  
ONE-LINE DIAGRAM (SHEET 1)

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NOTES:

- HEAVY DUTY 60 AMP, 600 VAC, 2 POLE OR 3 POLE WITH S/N & GND, NON-FUSIBLE SAFETY SWITCH IN A NEMA 3R & 12 ENCLOSURE, SQUARE D CLASS 3110, CAT. NO. HU362AWK WITH SNO610 & GTK0610, OR APPROVED EQUAL. INSTALL AT PLASI. NOTE 60 AMP SAFETY SWITCH REQUIRED TO ACCOMMODATE 4 AWG CONDUCTORS FOR LUG TERMINATIONS & WIRE BENDING SPACE. INCLUDE LEGEND PLATE. SEE "LEGEND PLATE SCHEDULES" FOR INFORMATION.

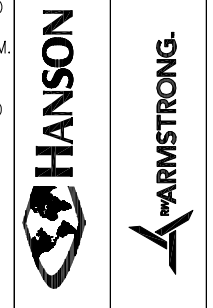


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EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

IL PROJ.: PRG-4018  
A.I.P. PROJ.: 3-17-0077-B13

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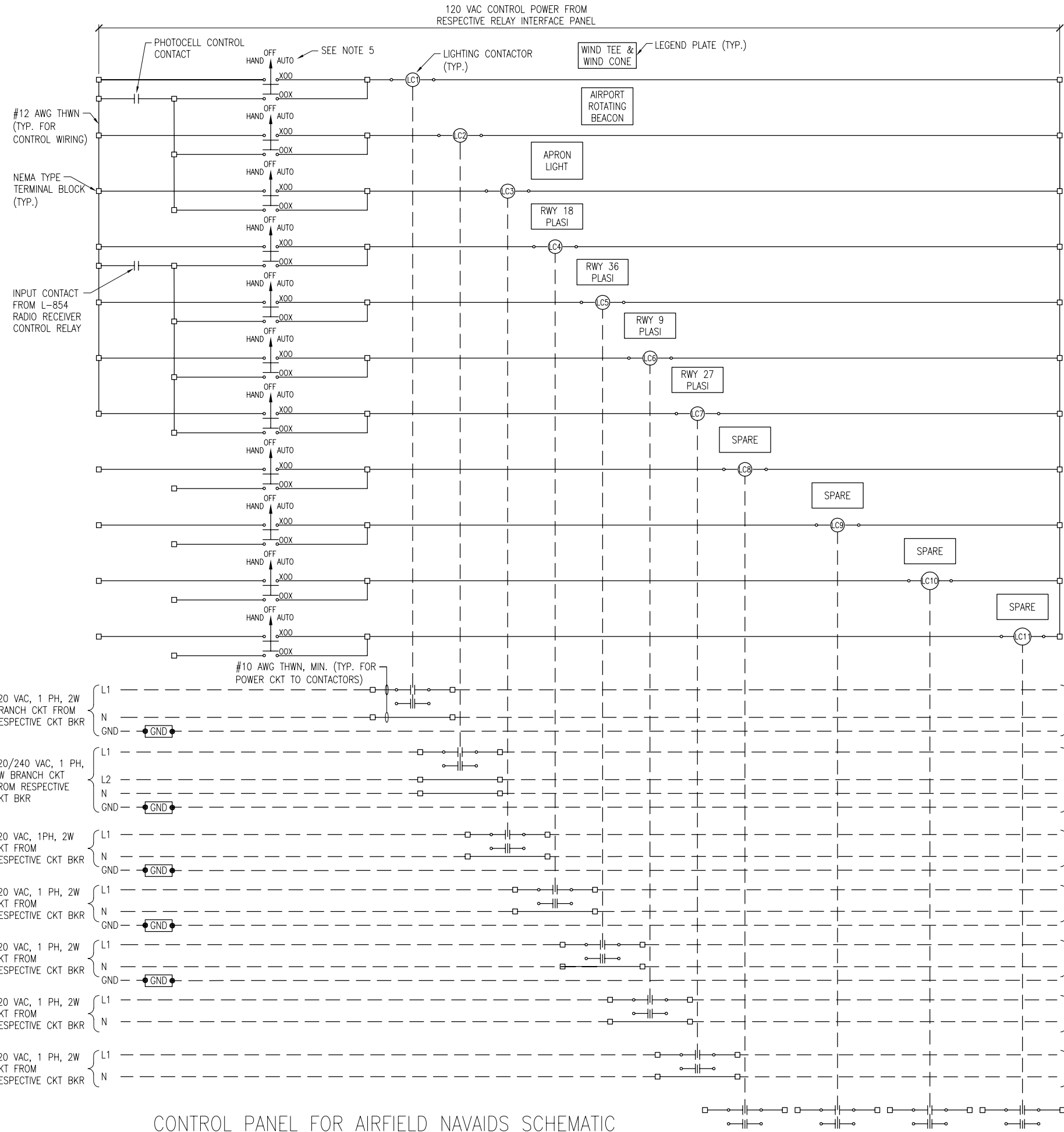
CONSTRUCT CROSSWIND  
RUNWAY 18-36

PROPOSED AIRPORT VAULT  
ONE-LINE DIAGRAM (SHEET 2)

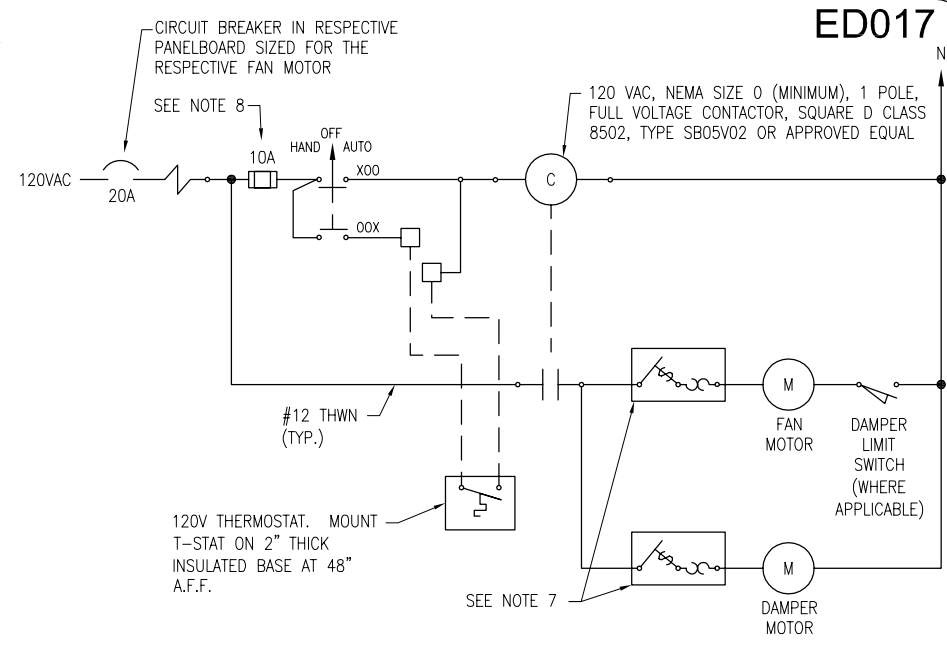
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CONTROL PANEL FOR AIRFIELD NAVAIDS SCHEMATIC



EXHAUST FAN CONTROL SCHEMATIC

NOTES:

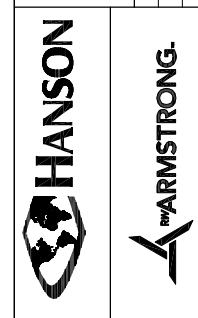
- 15 AMP & 20 AMP INPUT POWER/BRANCH CIRCUITS SHALL BE #10 AWG COPPER THWN FROM THE RESPECTIVE POWER SOURCE TO THE LIGHTING CONTACTOR/RELAY PANEL. 25 AMP AND 30 AMP INPUT POWER/BRANCH CIRCUITS SHALL BE #8 AWG COPPER THWN (MIN.) FROM THE RESPECTIVE POWER SOURCE TO THE LIGHTING CONTACTOR/RELAY PANEL.
- INPUT CONTROL CIRCUITS SHALL BE #12 AWG COPPER THWN.
- FOR 120 VAC BRANCH CIRCUITS THE NEUTRAL CONDUCTOR SHALL NOT BE SWITCHED THROUGH THE RELAY CONTACTS. USE TERMINAL BLOCKS TO TRANSITION FROM VAULT BRANCH CIRCUIT WIRING TO FIELD WIRING.
- PROVIDE #10 AWG COPPER BONDING JUMPER FROM PANEL ENCLOSURE FRAME TO ENCLOSURE DOOR.
- PROVIDE 3-POSITION MAINTAINED CONTACT "HAND-OFF-AUTO" SELECTOR SWITCH FOR EACH LIGHTING CONTACTOR & MOUNT ON LIGHTING CONTACTOR PANEL ENCLOSURE DOOR. SELECTOR SWITCH SHALL BE SQUARE D CLASS 9001, TYPE KS43FBH13, OR APPROVED EQUAL. INCLUDE LEGEND PLATE TO IDENTIFY THE DEVICE CONTROLLED (EX: "WIND CONE" OR "AIRPORT ROTATING BEACON").
- PROVIDE FRACTIONAL HORSEPOWER MOTOR MANUAL STARTER, SQUARE D MANUAL STARTER WITH HANDLE/GUARD/LOCK OFF, IN NEMA 4 ENCLOSURE CLASS 2510, TYPE FG5 OR APPROVED EQUAL FOR FAN MOTOR & DAMPER MOTOR. INCLUDE MELTING ALLOY TYPE THERMAL OVERLOADS SIZED AS REQUIRED TO PROTECT THE RESPECTIVE MOTOR. 120 VAC MOTORS SHALL HAVE SINGLE POLE STARTERS.
- FUSING FOR CONTROL WIRING SHALL BE 10 AMP, 600 VAC, BUSSMANN CATALOG FNQ-R-10, OR APPROVED EQUAL, WITH FUSE BLOCKS, WITH BOX LUG TERMINALS, SIZED AS REQUIRED FOR THE RESPECTIVE APPLICATION. INCLUDE HARDWARE FOR MOUNTING. PROVIDE ONE BOX (5 MINIMUM QUANTITY) OF EACH TYPE AND SIZE OF FUSE, UPON COMPLETION OF THE JOB FOR USE AS SPARES.
- THE AIRPORT ROTATING BEACON CIRCUIT SHALL HAVE PHASE "A" SWITCHED THROUGH THE LIGHTING CONTACTOR. PHASE "B" SHALL BE UNSWITCHED FROM THE POWER SOURCE TO THE LOAD CENTER AT THE AIRPORT ROTATING BEACON.

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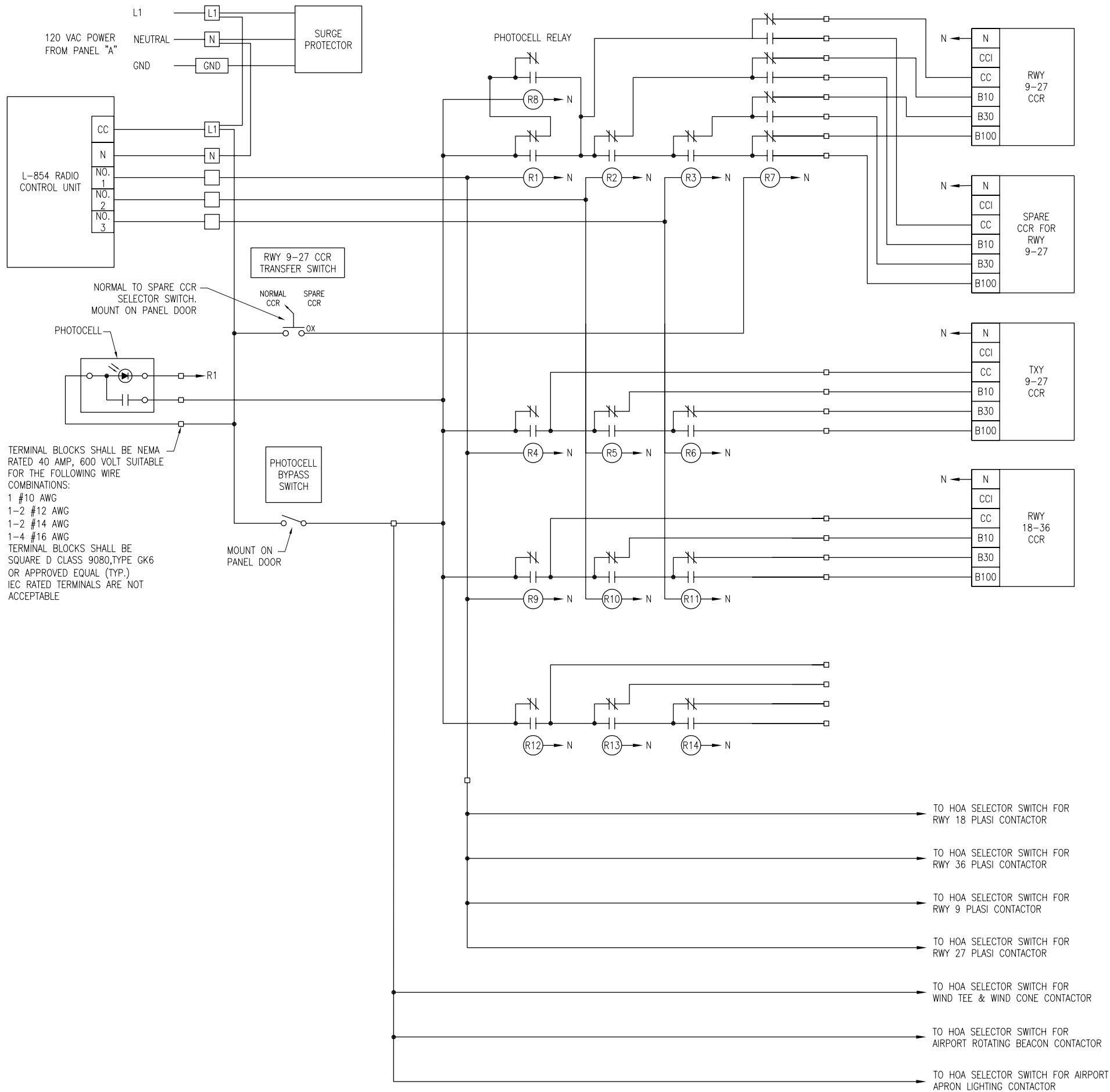
REVISION	DATE

EDGAR COUNTY AIRPORT  
 PARIS, ILLINOIS  
 ILL. PROJ.: PRG-4018  
 A.I.P. PROJ.: 3-17-0077-B13

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CONSTRUCT CROSSWIND  
 RUNWAY 18-36  
 LIGHTING CONTACTOR  
 SCHEMATIC



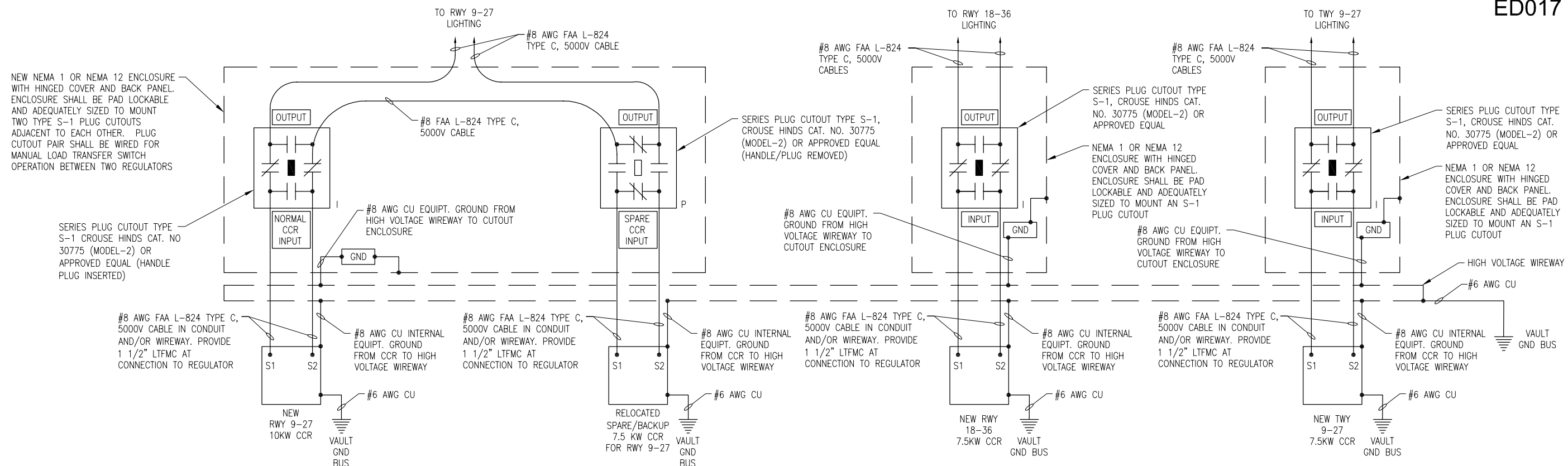
TERMINAL BLOCKS SHALL BE NEMA RATED 40 AMP, 600 VOLT SUITABLE FOR THE FOLLOWING WIRE COMBINATIONS:  
 1 #10 AWG  
 1-2 #12 AWG  
 1-2 #14 AWG  
 1-4 #16 AWG  
 TERMINAL BLOCKS SHALL BE SQUARE D CLASS 9080, TYPE GK6 OR APPROVED EQUAL (TYP.)  
 IEC RATED TERMINALS ARE NOT ACCEPTABLE

NOTES:

- RELAY INTERFACE CONTROL PANEL SHALL BE MANUFACTURED BY AN FAA APPROVED L-821 PANEL BUILDER OR A UL 508 INDUSTRIAL CONTROL PANEL BUILDER, AND SHALL BE MANUFACTURED IN THE UNITED STATES TO COMPLY WITH THE AIRPORT IMPROVEMENT PROGRAM BUY AMERICAN REQUIREMENT AND THE "BUY AMERICAN ACT".
- PANEL SHALL BE IN A NEMA 12 ENCLOSURE WITH HINGED COVER. INSTALL DRAIN IN BOTTOM OF ENCLOSURE TO ALLOW CONDENSATION TO ESCAPE.
- EXTERNAL CONTROL CABLE SHALL BE NO. 12 AWG COPPER, 600 VOLT CABLE. ALL PANEL INTERIOR CONTROL CABLE SHALL BE MINIMUM 16 AWG, COPPER, 600 VOLT CABLE.
- IN THE AUTOMATIC MODE OF OPERATION THE RUNWAY 9-27 CONSTANT CURRENT REGULATOR SHALL BE CONTROLLED BY THE PHOTOCELL & THE L-854 RADIO CONTROL UNIT IN THE FOLLOWING MANNER:  
 PHOTOCELL - 10% BRIGHTNESS & ACTIVATE RADIO CONTROL  
 5 CLICKS - 30% BRIGHTNESS  
 7 CLICKS - 100% BRIGHTNESS
- IN THE AUTOMATIC MODE OF OPERATION THE TAXIWAY 9-27 AND RUNWAY 18-36 CONSTANT CURRENT REGULATORS WILL BE CONTROLLED BY THE PHOTOCELL & THE L-854 RADIO CONTROL UNIT IN THE FOLLOWING MANNER:  
 PHOTOCELL -ACTIVATE RADIO CONTROL  
 3 CLICKS -10% BRIGHTNESS  
 5 CLICKS -30% BRIGHTNESS  
 7 CLICKS -100% BRIGHTNESS
- IN THE AUTOMATIC MODE OF OPERATION THE WIND TEE, WIND CONE, AIRPORT ROTATING BEACON & APRON LIGHT SHALL BE ACTIVATED BY THE PHOTOCELL OR PHOTOCELL BYPASS SWITCH.
- IN THE AUTOMATIC MODE OF OPERATION THE RUNWAY 9-27 AND 18-36 PLASI'S WILL BE CONTROLLED BY THE L-854 RADIO CONTROL UNIT IN THE FOLLOWING MANNER:  
 3 CLICKS - ON  
 5 CLICKS - REMAIN ON  
 7 CLICKS - REMAIN ON
- EQUIPMENT GROUND WIRES SHALL BE INCLUDED WITH EACH BRANCH CIRCUIT & EACH CONTROL CIRCUIT.
- INCLUDE PHOTOCELL BYPASS SWITCH.
- SURGE PROTECTOR SHALL BE UL LISTED PER UL 1449, SUITABLE FOR 120 VAC, 1PH, 2 WIRE PLUS GROUND SYSTEM WITH SURGE CURRENT RATING OF 40 KA (MIN.), 8x20 MICROSECOND WAVE, AND STATUS INDICATION LIGHTS IN A WEATHERPROOF HOUSING, JOSLYN MODEL 1260-21, OR APPROVED EQUAL. MAINTAIN LEADS AS SHORT & AS STRAIGHT AS POSSIBLE. INCLUDE MOUNTING BRACKET.
- INCLUDE EQUIPMENT GROUND BAR, ILSCO D167-12 OR APPROVED EQUAL.
- CONTROL RELAYS SHALL HAVE 10 AMP CONTACT RATINGS AT 240 VAC WITH 120 VAC COILS. PROVIDE 3 SPARE RELAYS FOR EACH TYPE USED IN THE RELAY INTERFACE PANEL.
- COLOR CODING FOR THE CONTROL WIRING TO EACH CONSTANT CURRENT REGULATOR SHALL BE CONSISTENT FOR ALL REGULATORS. COLOR CODING SHALL BE AS FOLLOWS:  
 CC -RED  
 10% -ORANGE  
 30% -YELLOW  
 100% -BLUE  
 NEUTRAL -WHITE  
 EQUIPT. GND -GREEN  
 ALSO TAG THE CONTROL WIRES WITH THE RESPECTIVE DESIGNATION (CC, 10%, 30%, 100%)
- "N" DESIGNATES NEUTRAL CONNECTION OR NEUTRAL CONDUCTOR.

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Hanson Proj. No. 10A0053 Filename: E-604.DWG Scale: NONE Date: 03/10/11		LAYOUT	CAT	A.I.P. PROJ.: 3-17-0077-B13
		DRAWN	RSS	
		REVIEWED	MAM	
CONSTRUCT CROSSWIND RUNWAY 18-36		AIRFIELD LIGHTING CONTROL WIRING SCHEMATIC		
52		52 of 137 sheets		



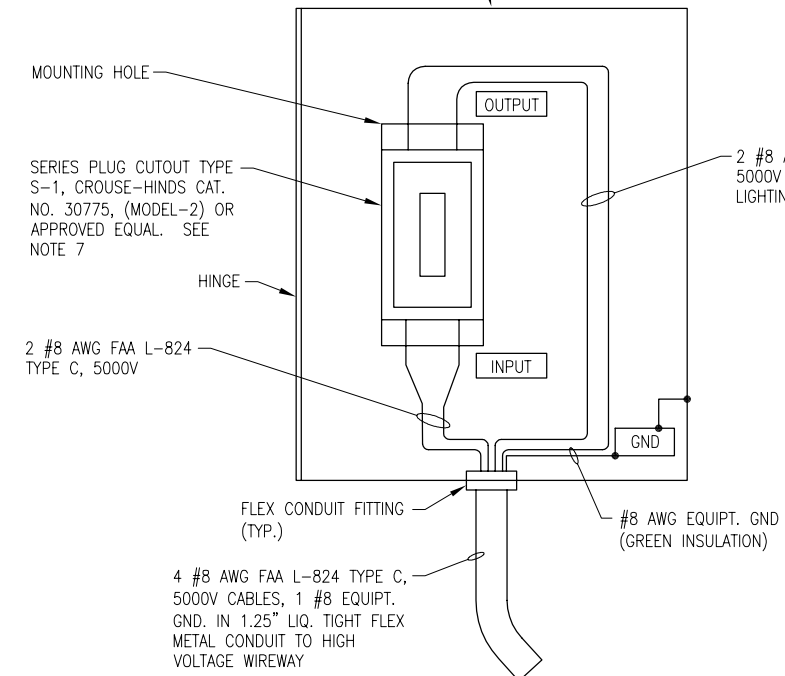


HIGH VOLTAGE WIRING SCHEMATIC

NOTES:

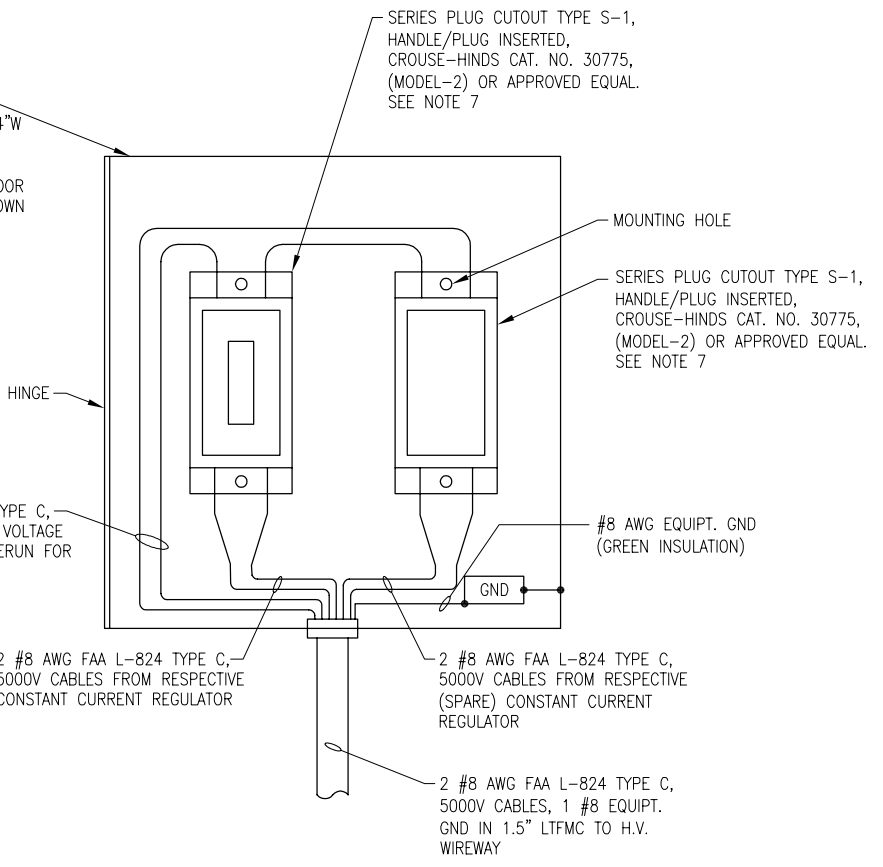
1. PROVIDE PHENOLIC ENGRAVED LEGEND PLATES FOR EACH CONSTANT CURRENT REGULATOR (EXISTING & NEW) NOTING THE RUNWAY AND/OR TAXIWAY SERVED.
2. EACH PLUG CUTOUT CABINET SHALL BE FURNISHED WITH A PHENOLIC ENGRAVED LEGEND PLATE THAT IDENTIFIES THE RESPECTIVE RUNWAY OR TAXIWAY CIRCUIT OR REGULATOR. INCLUDE AN ADDITIONAL LEGEND PLATE LABELED "CAUTION OPERATE CUTOUTS WITH CCR SHUT OFF".
3. PROVIDE PHENOLIC ENGRAVED LEGEND PLATES FOR THE CUTOUTS TO IDENTIFY THE RESPECTIVE REGULATOR OUTPUT CONNECTION AND THE RESPECTIVE CIRCUIT LOAD CONNECTION.
4. BOND REGULATOR FRAME TO VAULT GROUND BUS WITH A DEDICATED #6 AWG BONDING JUMPER.
5. PROVIDE ADEQUATE WORKING SPACE IN FRONT OF EACH CUTOUT ENCLOSURE TO MEET NEC CLEARANCE REQUIREMENTS.
6. LIQUID TIGHT FLEXIBLE METAL CONDUIT AND ASSOCIATED FITTINGS SHALL BE U.L. LISTED TO MEET THE REQUIREMENTS OF NEC 350.6, SUITABLE FOR GROUNDING AND SUNLIGHT RESISTANT. LIQUID TIGHT FLEXIBLE METAL CONDUIT THAT IS USED FOR FLEXIBILITY (INCLUDING CONNECTIONS TO CCR'S & TRANSFORMERS) SHALL REQUIRE AN EXTERNAL BONDING JUMPER OR INTERNAL EQUIPMENT GROUNDING CONDUCTOR PER NEC 350.60. EXTERNAL BONDING JUMPERS USED WITH CCR INSTALLATIONS SHALL BE #6 AWG COPPER (MINIMUM). DO NOT INSTALL LIQUID TIGHT FLEXIBLE METAL CONDUIT THAT IS NOT UL LISTED. CONFIRM LIQUID TIGHT FLEXIBLE METAL CONDUIT BEARS THE UL LABEL PRIOR TO INSTALLING IT.
7. CROUSE-HINDS CAT. NO. 30771, (MODEL-3) SERIES PLUG CUTOUTS ARE NOT ACCEPTABLE, BECAUSE THE HANDLE IS NOT REMOVABLE. ADB SIEMENS SCO SERIES CUTOUTS ARE NOT ACCEPTABLE BECAUSE THEY DO NOT FUNCTION THE SAME AS THE CROUSE-HINDS 30775 CUTOUT. AIRPORT LIGHTING CO. PART NO. S1 CUTOUTS ARE NOT ACCEPTABLE BECAUSE THEY HAVE BEEN OBSERVED TO NOT FUNCTION THE SAME AS THE CROUSE-HINDS 30775 CUTOUT WHEN THE HANDLE IS REMOVED, AND THE MFR DOES NOT RECOMMEND OPERATION OF THE CUTOUT WITH THE HANDLE REMOVED. OTHER CUTOUTS THAT DO NOT FUNCTION THE SAME AS CROUSE-HINDS CAT. NO. 30775 (MODEL-2) ARE NOT ACCEPTABLE.
8. HIGH VOLTAGE & LOW VOLTAGE CIRCUITS SHALL NOT BE INSTALLED IN THE SAME WIREWAY.

14"H x 12"W x 9"D (APPROXIMATE DIMENSIONS) NEMA 1 OR NEMA 12 ENCLOSURE WITH HINGED COVER & BACK PANEL. NOTE FRONT DOOR OF ENCLOSURE NOT SHOWN FOR CLARITY. ADJUST ENCLOSURE DIMENSIONS AS NECESSARY TO ACCOMMODATE THE RESPECTIVE CUTOUT



SERIES PLUG CUTOUT MOUNTING DETAIL FOR TWY 9-27 CIRCUIT & RWY 18-36 CIRCUIT (TYP. FOR 2) NOT TO SCALE

NEMA 1 OR NEMA 12 ENCLOSURE (MINIMUM 14"W x 16"H x 8"D) WITH HINGED COVER & BACK PANEL. NOTE FRONT DOOR OF ENCLOSURE NOT SHOWN FOR CLARITY



SERIES PLUG CUTOUT MOUNTING DETAIL FOR RUNWAY 9-27 CIRCUIT NOT TO SCALE

LEGEND

- "I" DENOTES PLUG CUTOUT WITH PLUG INSERTED
- "P" DENOTES PLUG CUTOUT WITH PLUG PULLED
- "CCR" DENOTES CONSTANT CURRENT REGULATOR

REVISION

DATE

EDGAR COUNTY AIRPORT  
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IL PROJ.: PRG-4018

A.I.P. PROJ.: 3-17-0077-B13

Hanson Proj. No. 10A0053	FILENAME E-605.DWG	Scale NONE	Date 03/10/11
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**HANSON**

**ARMSTRONG**

CONSTRUCT CROSSWIND  
RUNWAY 18-36

HIGH VOLTAGE  
WIRING SCHEMATIC

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LEGEND PLATE SCHEDULE	
DEVICE	LABEL
VAULT PANELBOARD A	SERVICE PANEL "A" 120/240 VAC, 1 PH, 3W
MAIN BREAKER IN VAULT PANEL A	SERVICE DISCONNECT
PANELBOARD B IN VAULT	PANEL "B" 120/240 VAC, 1 PH, 3W FED FROM PANEL "A"
RUNWAY 9-27 CCR	RUNWAY 9-27
SPARE RUNWAY 9-27 CCR	SPARE FOR RUNWAY 9-27
TAXIWAY 9-27 CCR	TAXIWAY A & B
CUTOUT ENCLOSURE FOR RUNWAY 9-27	RUNWAY 9-27 CUTOUT
EACH CUTOUT ENCLOSURE (3 LEGEND PLATES)	CAUTION OPERATE CUTOUTS WITH CCR SHUT OFF
NORMAL CUTOUT INPUT SIDE CONNECTION FOR RUNWAY 9-27	NORMAL CCR INPUT
SPARE CUTOUT INPUT SIDE CONNECTION FOR RUNWAY 9-27	SPARE CCR INPUT
CUTOUT ENCLOSURE FOR TAXIWAY "A" & "B"	TAXIWAY A & B CUTOUT
TAXIWAY CUTOUT INPUT SIDE CONNECTION	INPUT
EACH CUTOUT (RUNWAY & TAXIWAY) OUTPUT SIDE CONNECTION (4 LEGEND PLATES)	OUTPUT
RUNWAY 18-36 CCR	RUNWAY 18-36
CUTOUT ENCLOSURE FOR RUNWAY 18-36	RUNWAY 18-36 CUTOUT
RUNWAY 18-36 CUTOUT INPUT SIDE CONNECTION	INPUT

NOTE: LEGEND PLATES SHALL BE WEATHERPROOF ENGRAVED PLASTIC OR PHENOLIC MATERIAL, 1/4" HIGH BLACK LETTERS ON A WHITE BACKGROUND UNLESS NOTED OTHERWISE. SECURE WITH WEATHERPROOF ADHESIVE AND MACHINE SCREWS. FURNISH ADDITIONAL LEGEND PLATES WHERE REQUIRED BY CODE, FOR ADDITIONAL EQUIPMENT, AS DETAILED HEREIN ON THE PLANS, AND AS NOTED IN THE SPECIAL PROVISION SPECIFICATIONS.

FURNISH & INSTALL A WEATHERPROOF WARNING LABEL FOR EACH SAFETY SWITCH, PANELBOARD, LOAD CENTER, CUTOUT, & CONTROL PANEL TO WARN PERSONS OF POTENTIAL ELECTRIC ARC FLASH HAZARDS, PER THE REQUIREMENTS OF NEC 110.16 "FLASH PROTECTION". LABELS SHALL BE HAZARD COMMUNICATION SYSTEMS, LCC (190 OLD MILFORD RD., BOX 1174, MILFORD, PA 18337, PHONE: 1-877-748-0244) PART NO. H6010-9VWHBJ OR APPROVED EQUAL

LEGEND PLATE SCHEDULE CONTINUED	
DEVICE	LABEL
RADIO RELAY INTERFACE PANEL	RADIO RELAY INTERFACE PANEL
MANUAL TRANSFER SWITCH FOR RUNWAY 9-27 NORMAL CCR AND SPARE/BACKUP CCR	TRANSFER SWITCH FOR RUNWAY 9-27 CONSTANT CURRENT REGULATORS
MANUAL TRANSFER SWITCH FOR RUNWAY 9-27 NORMAL CCR AND SPARE/BACKUP CCR - NORMAL SWITCH POSITION	NORMAL CCR
MANUAL TRANSFER SWITCH FOR RUNWAY 9-27 NORMAL CCR AND SPARE/BACKUP CCR - BACKUP SWITCH POSITION	SPARE/BACKUP CCR
CONTROL PANEL FOR AIRFIELD NAVAIDS AND VAULT FAN	CONTACTOR PANEL FOR AIRFIELD NAVAIDS, APRON LIGHT & VAULT FAN
CONTACTOR PANEL FOR AIRFIELD NAVAIDS AND VAULT FAN	NOTICE CONTACTORS HAVE REMOTE LOCATED CONTROLS AND MAY ACTIVATE AT ANY TIME
LOW VOLTAGE WIREWAY (PROVIDE 4 LEGEND PLATES 1/2" HIGH BLACK LETTERS WHITE BACKGROUND)	LOW VOLTAGE
HIGH VOLTAGE WIREWAY (PROVIDE 4 LEGEND PLATES 1/2" HIGH BLACK LETTERS WHITE BACKGROUND)	HIGH VOLTAGE
VAULT GROUND BUS (PROVIDE 4 LEGEND PLATES 1/2" HIGH WHITE LETTERS GREEN BACKGROUND; INSTALL ABOVE OR BELOW GROUND BUS)	VAULT GROUND BUS
GROUNDING ELECTRODE CONDUCTORS TERMINATED ON VAULT GROUND BUS. (PROVIDE 3 LEGEND PLATES & SECURE TO CONDUCTORS WITH NYLON STRING OR CABLE TIES)	DO NOT DISCONNECT
AWOS STEP-UP TRANSFORMER	AWOS XFMR 5KVA, 240/480 VAC, 1 PH., 2W FED FROM PANEL "A"
PLASI 27 BOOST TRANSFORMER	PLASI 27 BOOST XFMR 500VA, 120/136 VAC, 1 PH., 2W FED FROM PANEL "B"
RUNWAY 9 PLASI DISCONNECT	RWY 9 PLASI 120 VAC FED FROM VAULT
RUNWAY 27 PLASI DISCONNECT	RWY 27 PLASI 120 VAC FED FROM VAULT
RUNWAY 18 PLASI DISCONNECT	RWY 18 PLASI 120 VAC FED FROM VAULT
RUNWAY 36 PLASI DISCONNECT	RWY 36 PLASI 120 VAC FED FROM VAULT

DIRECTIONS TO TRANSFER RUNWAY 9-27 LIGHTING FROM NORMAL CCR TO SPARE/BACKUP CCR.

1. SHUT OFF INPUT POWER (CIRCUIT BREAKER) TO BOTH RWY 9-27 CCR'S & TURN CCR SELECTOR SWITCHES TO OFF.
2. OPERATE MANUAL TRANSFER SWITCH FOR RWY 9-27 AND MOVE HANDLE FROM "NORMAL" POSITION TO "SPARE/BACKUP" POSITION.
3. PULL CUTOUT HANDLE FROM NORMAL CCR UNIT & INSERT INTO SPARE CCR CUTOUT.
4. GO TO RADIO RELAY INTERFACE PANEL & TURN "RWY 9-27 CCR TRANSFER" SELECTOR SWITCH FROM "NORMAL" TO "SPARE" POSITION.
5. TURN ON INPUT POWER (CIRCUIT BREAKER) TO SPARE RWY 9-27 CCR.
6. TURN SELECTOR SWITCH ON SPARE CCR TO "REMOTE" POSITION.

PROVIDE PLACARD OR LEGEND PLATE FOR RUNWAY CONSTANT CURRENT REGULATOR PAIR AS NOTED ABOVE: LETTERING TO BE MIN. 1/4" HIGH, BLACK ON WHITE BACKGROUND. LOCATE PLACARD ABOVE OR ADJACENT TO CUTOUT ENCLOSURE FOR RUNWAY.

CCR TRANSFER PROCEDURE PLACARD DETAIL



"DANGER - HIGH VOLTAGE KEEP OUT" SIGN

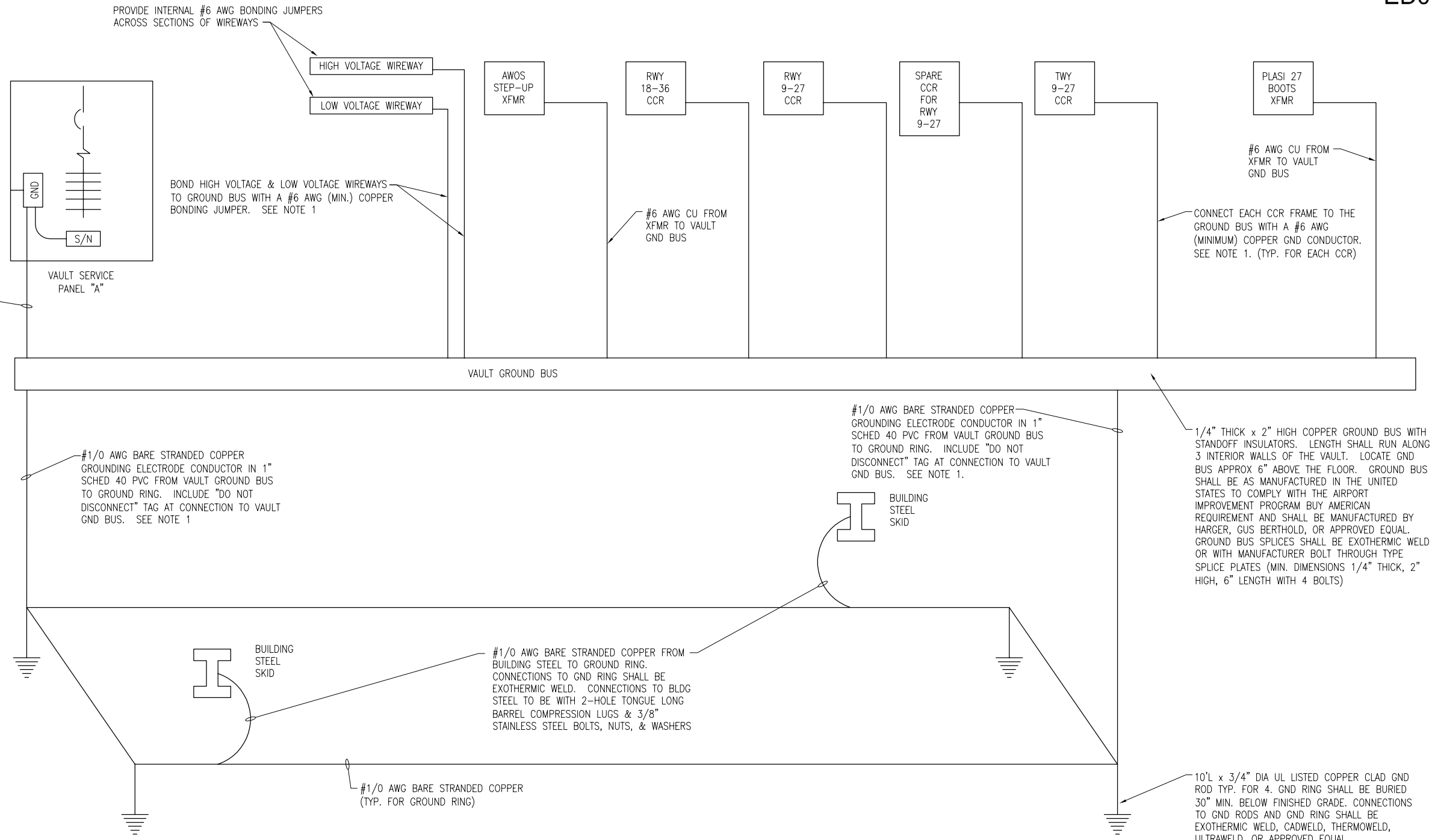
PROVIDE WARNING SIGN ON VAULT EXTERIOR DOORS LABELED "DANGER - HIGH VOLTAGE - KEEP OUT" PER THE REQUIREMENTS OF NEC 110.34 (C). PROVIDE MINIMUM OF 2 SIGNS (ONE ON EACH DOOR TO THE VAULT).



"DANGER - HIGH VOLTAGE" SIGN

FURNISH AND INSTALL "DANGER - HIGH VOLTAGE" LABELS/SIGNS FOR EACH CUTOUT ENCLOSURE, EACH CONSTANT CURRENT REGULATOR, AND THE HIGH VOLTAGE WIREWAY, TO COMPLY WITH FAA AC 150/5340-26B "MAINTENANCE OF AIRPORT VISUAL AID FACILITIES".

REVISION									
DATE									
EDGAR COUNTY AIRPORT PARIS, ILLINOIS									
A.I.P. PROJ.: 3-17-0077-B13 IL PROJ.: PRG-4018									
Hanson Proj. No. 10A00053 Filename: E-606.DWG Scale: NONE Date: 03/10/11	LAYOUT	DRAWN	REVIEWED	CAT	RSS	MAM			
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CONSTRUCT CROSSWIND RUNWAY 18-36				LEGEND PLATE SCHEDULES					
54									
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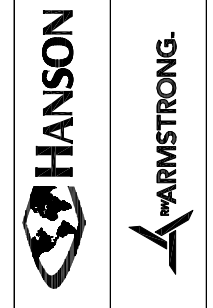
NOTES:

1. CONNECTIONS TO GROUND BUS BAR SHALL BE WITH 2-HOLE TONGUE LONG BARREL COMPRESSION LUGS BOLTED TO THE BUS BAR WITH 3/8" STAINLESS STEEL BOLTS, NUTS, & WASHERS.
2. GROUND BUS BAR SHALL BE MANUFACTURED IN THE UNITED STATES OF AMERICA TO COMPLY WITH THE AIRPORT IMPROVEMENT PROGRAM BUY AMERICAN REQUIREMENTS. GROUND RODS SHALL BE MANUFACTURED IN THE UNITED STATES OF AMERICA FROM 100% DOMESTIC STEEL. INCLUDE CERTIFICATION OF COMPLIANCE WITH SHOP DRAWING SUBMITTAL.

REVISION	DATE

HANSON  
**EDGAR COUNTY AIRPORT**  
**PARIS, ILLINOIS**  
 ILL. PROJ.: PRG-4018  
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**CONSTRUCT CROSSWIND**  
**RUNWAY 18-36**  
**VAULT GROUND BUS**  
**RISER DIAGRAM**

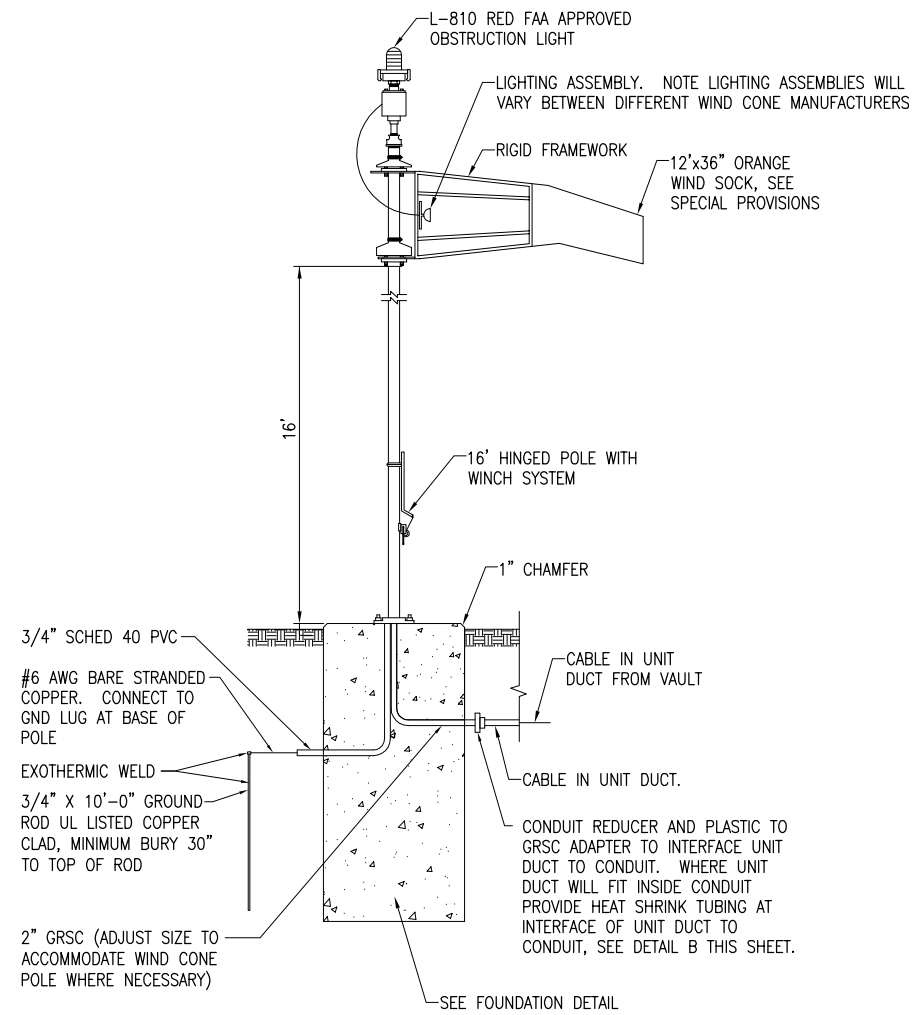
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GROUNDING NOTES:

1. THE CONTRACTOR SHALL FURNISH AND INSTALL ALL GROUNDING AS MAY BE NECESSARY OR REQUIRED TO MAKE A COMPLETE GROUNDING SYSTEM AS REQUIRED BY THE LATEST NATIONAL ELECTRICAL CODE (NFPA 70) IN FORCE AND FAA-STD-019e (LIGHTNING AND SURGE PROTECTION, GROUNDING, BONDING, AND SHIELDING REQUIREMENTS FOR FACILITIES AND ELECTRONIC EQUIPMENT). THE RELIABILITY OF THE GROUNDING SYSTEM IS DEPENDENT ON CAREFUL, PROPER INSTALLATION AND CHOICE OF MATERIALS. IMPROPER PREPARATION OF SURFACES TO BE JOINED TO MAKE AN ELECTRICAL PATH, LOOSE JOINTS OR CORROSION CAN INTRODUCE IMPEDANCE THAT WILL SERIOUSLY IMPAIR THE ABILITY OF THE GROUND PATH TO PROTECT PERSONNEL AND EQUIPMENT AND TO ABSORB TRANSIENTS THAT CAN CAUSE NOISE IN COMMUNICATIONS CIRCUITS. THE FOLLOWING FUNCTIONS ARE PARTICULARLY IMPORTANT TO ENSURE A RELIABLE GROUND SYSTEM:
  2. FURNISH AND INSTALL GROUND RODS AS DETAILED HEREIN. GROUND RODS FOR AIRFIELD LIGHTING (RUNWAY LIGHTING, TAXIWAY LIGHTING, TAXI GUIDANCE SIGNS, & DISTANCE REMAINING SIGNS) SHALL BE MINIMUM 5/8-IN. DIAMETER BY 8-FT LONG, UL-LISTED COPPER CLAD WITH 10-MIL MINIMUM COPPER COATING. GROUND RODS FOR OTHER APPLICATIONS SHALL BE MINIMUM 3/4-IN. DIAMETER BY 10-FT LONG, UL-LISTED, COPPER CLAD WITH 10-MIL MINIMUM COPPER COATING. GROUND RODS SHALL BE SPACED OR AS DETAILED ON THE RESPECTIVE PLANS, AND IN NO CASE SPACED LESS THAN ONE ROD LENGTH APART. ALL CONNECTIONS TO GROUND RODS AND THE GROUND RING SHALL BE MADE WITH EXOTHERMIC WELD TYPE CONNECTORS, CADWELD BY ERICO PRODUCTS, INC., SOLON, OHIO, (PHONE 1-800-248-9353), THERMOWELD BY CONTINENTAL INDUSTRIES, INC., TULSA, OKLAHOMA (PHONE 918-663-1440) OR ULTRAWELD BY HARGER, GRAYSLAKE, ILLINOIS (PHONE 1-800-842-7437). EXOTHERMIC WELD CONNECTIONS SHALL BE INSTALLED IN CONFORMANCE WITH THE RESPECTIVE MANUFACTURER'S DIRECTIONS USING MOLDS AS REQUIRED FOR EACH RESPECTIVE APPLICATION. BOLTED CONNECTIONS WILL NOT BE PERMITTED AT GROUND RODS OR AT BURIED GROUNDING ELECTRODE CONDUCTORS.
  3. CONTRACTOR SHALL TEST EACH MADE ELECTRODE GROUND ROD/GROUND FIELD/GROUND RING WITH AN INSTRUMENT SPECIFICALLY DESIGNED FOR TESTING GROUND FIELD SYSTEMS. IF GROUND RESISTANCE EXCEEDS 10 OHMS, CONTACT THE ENGINEER FOR FURTHER DIRECTION. COPIES OF GROUND FIELD TEST RESULTS SHALL BE FURNISHED TO THE RESIDENT ENGINEER, UPON REQUEST, FOR REVIEW AND RECORD PURPOSES.
  4. ALL PRODUCTS ASSOCIATED WITH THE GROUNDING SYSTEM SHALL BE UL-LISTED AND LABELED.
  5. ALL BOLTED OR MECHANICAL CONNECTIONS SHALL BE COATED WITH A CORROSION PREVENTATIVE COMPOUND BEFORE JOINING, SANCHEM INC. "NO-OX-ID "A-SPECIAL" COMPOUND, BURNDY PENETROX E, OR EQUAL.
  6. METALLIC SURFACES TO BE JOINED SHALL BE PREPARED BY THE REMOVAL OF ALL NON-CONDUCTIVE MATERIAL, PER 2008 NATIONAL ELECTRICAL CODE ARTICLE 250-12. ALL COPPER BUS BARS MUST BE CLEANED PRIOR TO MAKING CONNECTIONS TO REMOVE SURFACE OXIDATION.
  7. METALLIC RACEWAY FITTINGS SHALL BE MADE UP TIGHT TO PROVIDE A PERMANENT LOW IMPEDANCE PATH FOR ALL CIRCUITS. METAL CONDUIT TERMINATIONS IN ENCLOSURES SHALL BE BONDED TO THE ENCLOSURE WITH UL-LISTED FITTINGS SUITABLE FOR GROUNDING. PROVIDE GROUNDING BUSHINGS WITH BONDING JUMPERS FOR ALL METAL CONDUITS ENTERING SERVICE EQUIPMENT (METER BASE, CT CABINET, MAIN SERVICE BREAKER ENCLOSURE, ETC.). PROVIDE GROUNDING BUSHINGS WITH BONDING JUMPERS FOR ALL METAL CONDUITS ENTERING AN ENCLOSURE THROUGH CONCENTRIC OR ECCENTRIC KNOCKOUTS THAT ARE PUNCHED OR OTHERWISE FORMED SO AS TO IMPAIR THE ELECTRICAL CONNECTION TO GROUND. STANDARD LOCKNUTS OR BUSHINGS SHALL NOT BE THE SOLE MEANS FOR BONDING WHERE A CONDUIT ENTERS AN ENCLOSURE THROUGH A CONCENTRIC OR ECCENTRIC KNOCKOUT.
  8. ALL CONNECTIONS, LOCATED ABOVE GRADE, BETWEEN THE DIFFERENT TYPES OF GROUNDING CONDUCTORS SHALL BE MADE USING UL-LISTED DOUBLE COMPRESSION CRIMP TYPE CONNECTORS OR UL-LISTED BOLTED GROUND CONNECTORS. FOR GROUND CONNECTIONS TO ENCLOSURES, CASES AND FRAMES OF ELECTRICAL EQUIPMENT NOT SUPPLIED WITH GROUND LUGS THE CONTRACTOR SHALL DRILL REQUIRED HOLES FOR MOUNTING A BOLTED GROUND CONNECTOR. ALL BOLTED GROUND CONNECTORS SHALL BE BURNDY, THOMAS AND BETTS, OR EQUAL. TIGHTEN CONNECTIONS TO COMPLY WITH TIGHTENING TORQUES IN UL STANDARD 486A TO ASSURE PERMANENT AND EFFECTIVE GROUNDING.
  9. ALL METAL EQUIPMENT ENCLOSURES, CONDUITS, CABINETS, BOXES, RECEPTACLES, MOTORS, ETC. SHALL BE BONDED TO THE RESPECTIVE GROUNDING SYSTEM.
  10. PROVIDE ALL BOXES FOR PROPOSED OUTLETS, SWITCHES, CIRCUIT BREAKERS, ETC. WITH GROUNDING SCREWS. PROVIDE ALL PANELBOARD, SWITCHGEAR, ETC., ENCLOSURES WITH GROUNDING BARS WITH INDIVIDUAL SCREWS, LUGS, CLAMPS, ETC., FOR EACH OF THE GROUNDING CONDUCTORS THAT ENTER THEIR RESPECTIVE ENCLOSURES.
  11. EACH NEW FEEDER CIRCUIT AND/OR BRANCH CIRCUIT SHALL INCLUDE AN EQUIPMENT GROUND WIRE. METAL RACEWAY OR CONDUIT SHALL NOT MEET THIS REQUIREMENT. THE EQUIPMENT GROUND WIRE FROM EQUIPMENT SHALL NOT BE SMALLER THAN ALLOWED BY 2008 NEC TABLE 250-122 "MINIMUM SIZE CONDUCTORS OR GROUNDING RACEWAY AND EQUIPMENT." WHEN CONDUCTORS ARE ADJUSTED IN SIZE TO COMPENSATE FOR VOLTAGE DROP, EQUIPMENT-GROUNDING CONDUCTORS SHALL BE ADJUSTED PROPORTIONATELY ACCORDING TO CIRCULAR MIL AREA. ALL EQUIPMENT GROUND WIRES SHALL BE COPPER, EITHER BARE OR INSULATED GREEN IN COLOR. WHERE THE EQUIPMENT GROUNDING CONDUCTORS ARE INSULATED, THEY SHALL BE IDENTIFIED BY THE COLOR GREEN, AND SHALL BE THE SAME INSULATION TYPE AS THE PHASE CONDUCTORS.
12. ALL EXTERIOR METAL CONDUIT, WHERE NOT ELECTRICALLY CONTINUOUS BECAUSE OF MANHOLES, HANDHOLES, NON-METALLIC JUNCTION BOXES, ETC., SHALL BE BONDED TO ALL OTHER METAL CONDUIT IN THE RESPECTIVE DUCT RUN, AND AT EACH END, WITH A COPPER-BONDING JUMPER SIZED IN CONFORMANCE WITH 2008 NEC 250-102. WHERE METAL CONDUITS TERMINATE IN AN ENCLOSURE (SUCH AS A MOTOR CONTROL CENTER, SWITCHBOARD, ETC) WHERE THERE IS NOT ELECTRICAL CONTINUITY WITH THE CONDUIT AND THE RESPECTIVE ENCLOSURE, PROVIDE A BONDING JUMPER FROM THE RESPECTIVE ENCLOSURE GROUND BUS TO THE CONDUIT SIZED PER 2008 NEC 250-102.
13. IT IS THE INTENT OF THIS SPECIFICATION THAT ALL MOTOR FRAMES, PUMP BASES ELECTRICAL EQUIPMENT ENCLOSURES, PANEL HOUSINGS, CONDUITS, BOXES, ETC. HAVE A CONTINUOUS COPPER WIRE GROUND CONNECTION AND SHALL BE POSITIVELY BONDED TO THE RESPECTIVE GROUNDING SYSTEM. CONDUIT CONNECTORS WILL NOT BE CONSIDERED AS ADEQUATE GROUNDING.
14. PROVIDE A POSITIVE GROUND BOND FOR ALL OUTLET BOXES, ELECTRICAL EQUIPMENT ENCLOSURES, GROUNDING RECEPTACLES, TOGGLE SWITCHES, ETC. INSTALL A GROUNDING CONDUCTOR IN ALL WIRE AND CABLE RACEWAYS. GROUND CONDUCTOR TO HAVE 600-VOLT INSULATION AND BE IDENTIFIED BY A CONTINUOUS GREEN COLOR COATING. THEY SHALL BE USED SOLELY FOR GROUNDING PURPOSES AND BE ENTIRELY SEPARATE FROM WHITE GROUNDED NEUTRAL CONDUCTOR, EXCEPT AT SUPPLY SIDE OF SERVICE DISCONNECTING MEANS, WHERE GROUNDING AND NEUTRAL SYSTEMS ARE TO BE CONNECTED TO SERVICE GROUND.
15. EACH AND ALL GROUNDED CASED AND METAL PARTS ASSOCIATED WITH ELECTRICAL EQUIPMENT SHALL BE TESTED FOR CONTINUITY OF CONNECTION WITH GROUND BUS SYSTEM BY CONTRACTOR IN PRESENCE OF OWNER'S REPRESENTATIVE.
16. ALL CONNECTIONS BETWEEN THE DIFFERENT TYPES OF GROUNDING CONDUCTORS ABOVE GRADE SHALL BE MADE USING BOLTED GROUND CONNECTORS. GROUND LUGS SHALL BE PROVIDED IN ALL ENCLOSURES AND WIRING TERMINATION JUNCTION BOXES. EQUIPMENT GROUNDS AND GROUNDING CONDUCTOR SHALL BE CONNECTED TO THESE GROUND LUGS. FOR GROUND CONNECTIONS TO ENCLOSURES, CASES AND FRAMES OF ELECTRICAL EQUIPMENT NOT SUPPLIED WITH GROUND LUGS THE CONTRACTOR SHALL DRILL REQUIRED HOLES FOR MOUNTING A BOLTED GROUND CONNECTOR. ALL BOLTED GROUND CONNECTORS SHALL BE BURNDY, OR EQUAL.
17. BOND ALL NONCURRENT-CARRYING PARTS OF METAL EQUIPMENT TO GROUND SYSTEM.
18. BUILDING STRUCTURAL STEEL SYSTEM SHALL BE BONDED TO ELECTRICAL GROUND SYSTEM.
19. INSTALL GROUNDING ELECTRODE CONDUCTORS, LIGHTNING PROTECTION DOWN CONDUCTORS AND SEPARATE GROUND CONDUCTORS IN SCHEDULE 40 OR SCHEDULE 80 PVC CONDUIT OR EXPOSED WHERE ACCEPTABLE TO LOCAL CODES. WHERE GROUNDING ELECTRODE CONDUCTORS, LIGHTNING PROTECTION DOWN CONDUCTORS OR INDIVIDUAL GROUND CONDUCTORS ARE RUN IN PVC CONDUIT, DO NOT COMPLETELY ENIRCLE CONDUIT WITH FERROUS AND/OR MAGNETIC MATERIALS. USE NON-METALLIC REINFORCED FIBERGLASS STRUT SUPPORT. WHERE METAL CONDUIT CLAMPS ARE INSTALLED, USE NYLON BOLTS, NUTS, WASHERS AND SPACERS TO INTERRUPT A COMPLETE METALLIC PATH FROM ENCIRCLING THE CONDUIT. THIS IS REQUIRED TO AVOID GIRDLING OF GROUND CONDUCTORS. GIRDLING OF A GROUND CONDUCTOR IS THE RESULT OF PLACING THE CONDUCTOR IN A RING OF MAGNETIC MATERIAL. THIS RING COULD BE A METALLIC CONDUIT, U-BOLT OR STRUT SUPPORT PIPE CLAMP, OR OTHER SUPPORT HARDWARE. THE RESULT OF GIRDLING GROUND CONDUCTORS SIGNIFICANTLY INCREASES THE INDUCTIVE IMPEDANCE OF THE GROUND CONDUCTOR. INDUCTIVE AND CAPACITIVE IMPEDANCE IS A TYPE OF RESISTANCE THAT OPPOSES THE FLOW OF ALTERNATING CURRENT. ANY INCREASE IN THE IMPEDANCE OF A GROUND CONDUCTOR REDUCES ITS ABILITY TO EFFECTIVELY MITIGATE RADIO FREQUENCY NOISE IN THE GROUND SYSTEM. THE CONDITION WHERE A GROUND CONDUCTOR IS GIRDLED DURING A LIGHTNING STRIKE RESULTS IN PHENOMENA KNOWN AS SURGE IMPEDANCE LOADING. SURGE IMPEDANCE LOADING IS A RESULT OF VOLTAGE AND CURRENT REACHING 500,000 VOLTS AND 10,000 AMPS FOR A SHORT DURATION. GIRDLING FURTHER INCREASES THE IMPEDANCE AT LIGHTNING FREQUENCIES OF 100 KILOHERTZ TO 100 MEGAHERTZ. AT THESE POWER AND FREQUENCY LEVELS ANY INCREASE IN THE IMPEDANCE OF THE GROUND CONDUCTOR MUST BE CONTROLLED. DURING LIGHTNING DISCHARGE CONDITIONS A LOW INDUCTIVE IMPEDANCE PATH IS MORE IMPORTANT THAN A LOW DC RESISTANCE PATH.
20. IF LOCAL CODES DICTATE THAT INDIVIDUAL GROUNDING CONDUCTORS MUST BE RUN IN METAL CONDUIT OR RACEWAY, THEN THE CONDUIT OR RACEWAY MUST BE BONDED AT EACH END OF THE RUN WITH A BONDING JUMPER SIZED EQUAL TO THE INDIVIDUAL GROUNDING CONDUCTOR OR AS REQUIRED BY 2008 NEC 250-102. NOTE THIS DOES NOT APPLY TO AC EQUIPMENT GROUNDING CONDUCTORS RUN WITH AC CIRCUITS.
21. WHERE A CONFLICT IS DETERMINED WITH RESPECT TO GROUNDING REQUIREMENTS PER MANUFACTURER INSTALLATION INSTRUCTIONS, NEC, AND/OR THE CONTRACT DOCUMENTS, CONTACT THE RESIDENT ENGINEER OR PROJECT ENGINEER FOR FURTHER DIRECTIONS.

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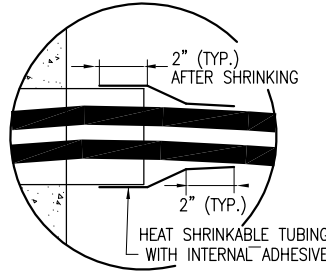
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<p><b>CONSTRUCT CROSSWIND</b> <b>RUNWAY 18-36</b></p>			<p><b>GROUNDING NOTES</b></p>		
<p><b>56</b></p>					
<p>56 of 137 sheets</p>					



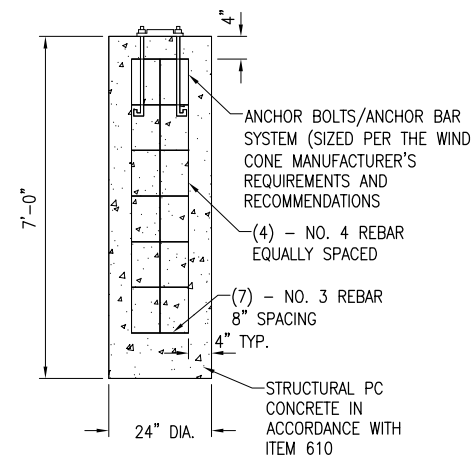
**LIGHTED L-807 WIND CONE**  
NOT TO SCALE

**NOTES**

1. WIND CONE SHALL BE FAA APPROVED L-807, LIGHTED, SIZE 2 WITH ORANGE WIND SOCK, 120 VAC, & WITH L-810 RED LED TYPE OBSTRUCTION LIGHT, SEE SPECIAL PROVISION SPECS.
2. L-807 WIND CONE-12' LIGHTED WILL BE PAID FOR UNDER ITEM AT107712.
3. REBAR SHALL BE MANUFACTURED FROM 100% DOMESTIC STEEL.



**DETAIL B**  
NOT TO SCALE

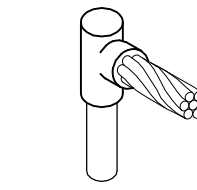


**FOUNDATION DETAIL**  
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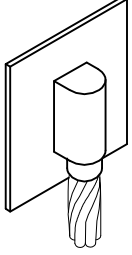
**ITEM AT107712 L-807 WIND CONE-12'  
LIGHTED IS UNDER ADDITIVE ALTERNATE NO. 1.**

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Hanson Proj. No. 10A0053 Filename: E-504.DWG Scale: NONE Date: 03/10/11	
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CONSTRUCT CROSSWIND RUNWAY 18-36	L-807 WIND CONE DETAIL
<b>57</b>	
57 of 137 sheets	



CABLE TO GROUND ROD



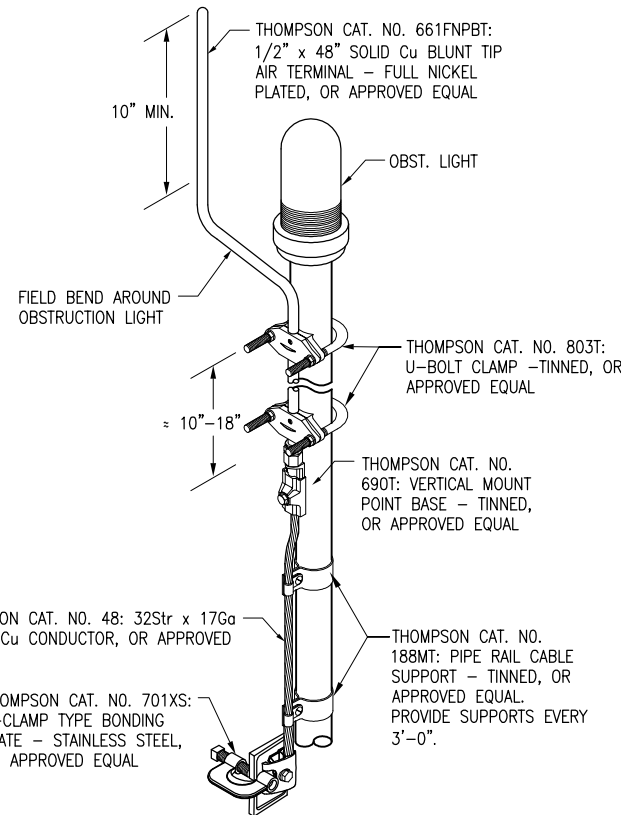
CABLE TO SURFACE

DETAIL NOTES

- EXOTHERMIC WELDS SHALL BE CADWELD AS MANUFACTURED BY ERICO PRODUCTS, SOLON, OHIO, ULTRAWELD AS MANUFACTURED BY HARGER LIGHTNING PROTECTION & GROUNDING EQUIPMENT, GRAYSLAKE, IL, THERMOWELD AS MANUFACTURED BY CONTINENTAL INDUSTRIES, TULSA, OKLAHOMA OR APPROVED EQUAL. VERIFY PROPER SIZES, MOLDS, TYPES, AND REQUIREMENTS FOR THE RESPECTIVE APPLICATION WITH THE MANUFACTURER, AND INSTALL PER THEIR DIRECTIONS.
- FOR APPLICATIONS TO GALVANIZED STEEL OR PAINTED STEEL, REMOVE GALVANIZING AND/OR PAINT & CLEAN THE SURFACE TO EXPOSE BARE STEEL BEFORE MAKING EXOTHERMIC WELD CONNECTION.
- VERIFY EXOTHERMIC MOLDS ARE SUITABLE FOR USE WITH THE RESPECTIVE TYPE (SOLID OR STRANDED) & SIZE CONDUCTOR.

EXOTHERMIC WELD DETAILS

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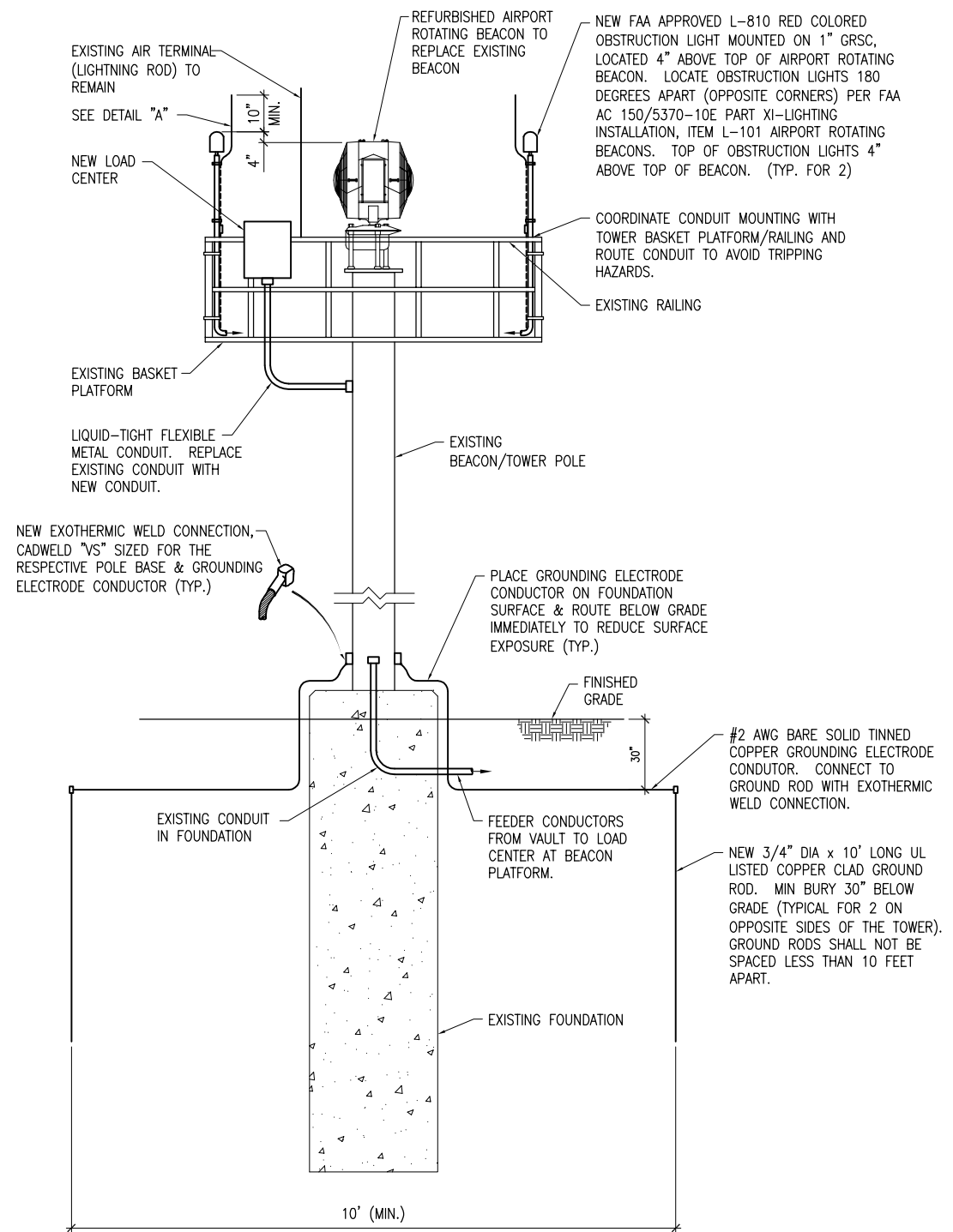


DETAIL A

NOT TO SCALE

NOTES

- REFERENCES TO THOMPSON ARE THOMPSON LIGHTNING PROTECTION INC., 901 SIBLEY MEMORIAL HWY, ST. PAUL, MN 55188, PHONE: 651-455-7661, 800-777-1230, FAX: 651-455-2545.
- VERIFY LIGHTNING PROTECTION COMPONENTS AND CATALOG NUMBERS WITH THE RESPECTIVE LIGHTNING PROTECTION EQUIPMENT MANUFACTURER.



LIGHTNING PROTECTION DETAIL FOR AIRPORT ROTATING BEACON

NOT TO SCALE

REMOVAL & REPLACEMENT OF EXISTING AIRPORT ROTATING BEACON WILL BE PAID FOR UNDER ADDITIVE ALTERNATE NO. 1, ITEM AS101580 REFURBISH 36" BEACON PER EACH. ALL OTHER WORK SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER ADDITIVE ALTERNATE NO. 1, ITEM AS800591 - UPGRADE AIRPORT ROTATING BEACON - PER L.S.

AIRPORT ROTATING BEACON LOAD CENTER SCHEDULE

CKT #	DUTY	SIZE	
1	SURGE PROTECTOR (PHASE A)	30A 1P	
2	SURGE PROTECTOR (PHASE B)	30A 1P	
3	AIRPORT ROTATING BEACON	15A 1P	
4	OBSTRUCTION LIGHTS	15A 1P	
5	BLANK		
6	BLANK		

100 AMP, 120/240 VAC, 1 PHASE, 3 WIRE, 6 CIRCUIT LOAD CENTER WITH MAIN LUGS IN A NEMA 3R RAIN PROOF ENCLOSURE, SQUARE D CAT. NO. Q0612L100RBCU WITH EQUIPMENT GROUND BAR KIT OR APPROVED EQUAL.

NOTES

- INCLUDE EQUIPT GROUND BAR KIT.
- ALL BREAKERS SHALL HAVE 10,000 AIC RATING AT 120/240 VAC.
- PHASE "A" SHALL BE SWITCHED THROUGH A LIGHTING CONTACTOR AT THE VAULT. PHASE "B" SHALL BE UNSWITCHED.
- INCLUDE ENGRAVED PHENOLIC LEGEND PLATE LABELED ARB PANEL, 120/240 VAC, 1PH, 3W, FED FROM VAULT.
- SURGE PROTECTORS SHALL BE SUITABLE FOR 120VAC, 1PH, 2W PLUS GROUND, 30KA (MINIMUM) SURGE CURRENT RATING, JOSLYN MODEL 1260-21 OR LIGHTING PROTECTION CORP. MODEL LPC 11765-132, OR APPROVED EQUAL. FURNISH & INSTALL TWO SURGE PROTECTORS (ONE FOR EACH PHASE).
- LOAD CENTER SHALL BE MANUFACTURED IN THE UNITED STATES TO COMPLY WITH THE AIRPORT IMPROVEMENT PROGRAM BUY AMERICAN REQUIREMENTS. PROVIDE CERTIFICATION OF MANUFACTURE IN THE UNITED STATES WITH SHOP DRAWING SUBMITTAL.

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EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

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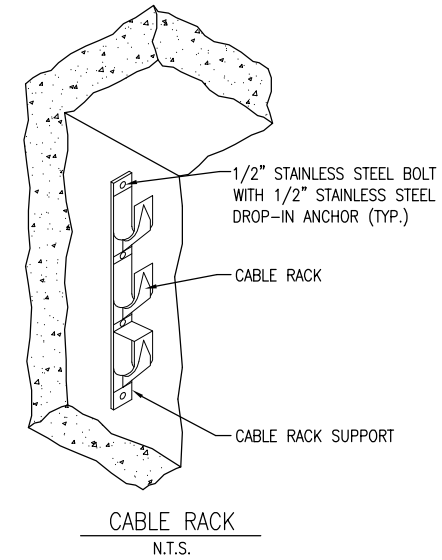
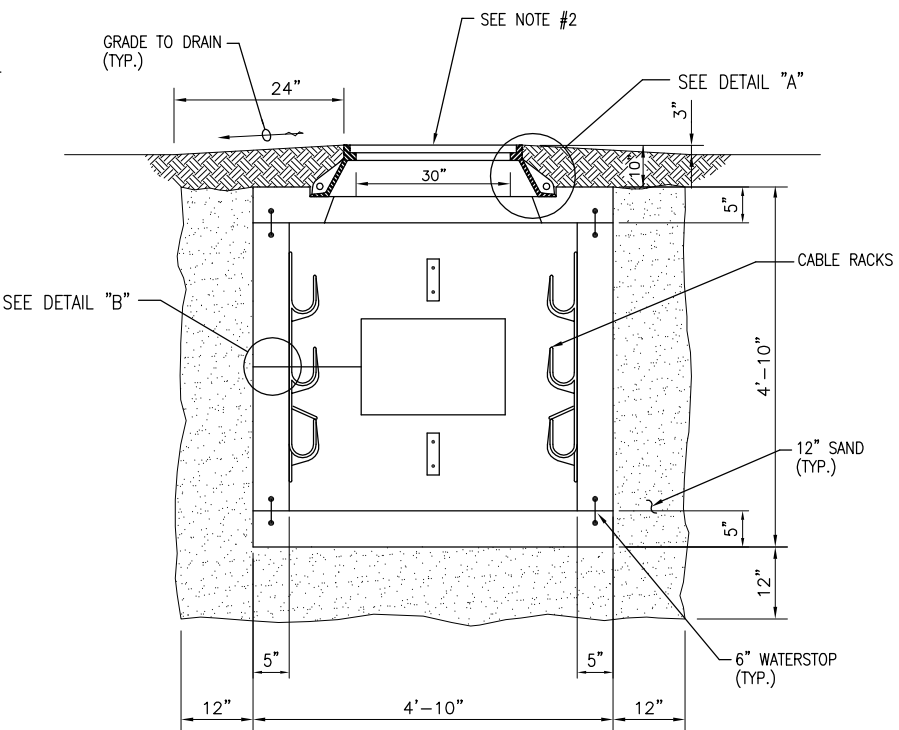
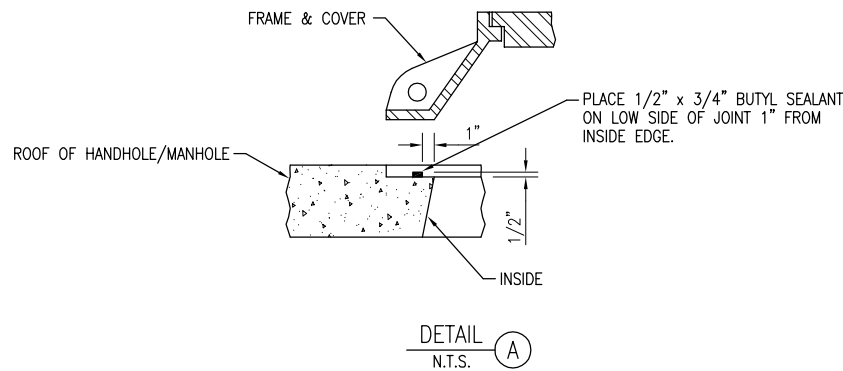
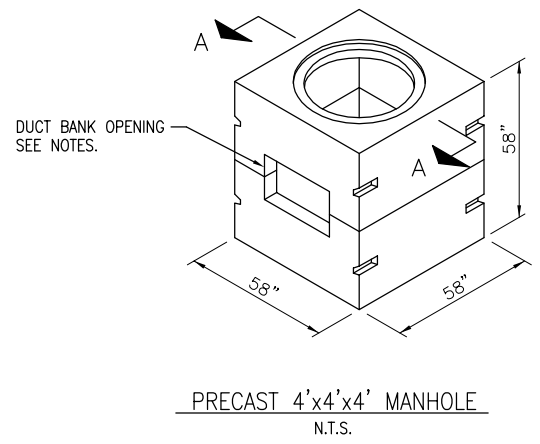
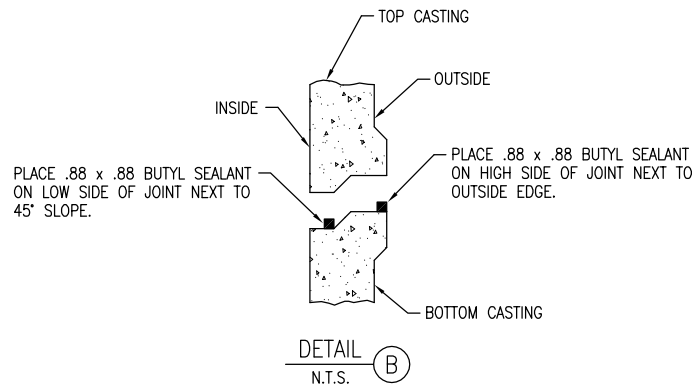
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CONSTRUCT CROSSWIND  
RUNWAY 18-36

LIGHTNING PROTECTION  
DETAILS FOR BEACON

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SECTION A  
N.T.S.  
PRECAST 4' x 4' x 4' MANHOLE DETAILS  
N.T.S. (NOT TO SCALE)

PRECAST 4'x4'x4' ELECTRICAL MANHOLE NOTES

- 4'x4'x4' ELECTRICAL MANHOLE SHALL BE CONSTRUCTED TO MEET THE FOLLOWING:
 

**DESIGN CRITERIA:**

  - DESIGN SPECIFICATION: ACI 318, AASHTO LOAD FACTOR DESIGN METHOD, AND ASTM C858
  - DESIGN LOADING: AASHTO HS20 (32,000 LB/AXLE)
  - LIVE LOAD SURCHARGE: .5% OF THE WHEEL LOADING APPLIED TO 8'-0" OF DEPTH.
  - CONCRETE COMPRESSIVE STRENGTH:  $F'_c = 4500$  PSI
  - REINFORCING STEEL: ASTM A706,  $F_y = 60000$  PSI

**DESIGN ASSUMPTIONS:**

  - GROUND WATER LEVEL: 3'-6" BELOW GRADE.
  - EARTH COVER: 2'-0" MINIMUM TO 5'-0" MAXIMUM
  - LIVE LOAD IMPACT:
    - 2'-0"  $I = 20\%$
    - 2'-1" TO 2'-11"  $I = 10\%$
    - 3'-0" TO 5'-0"  $I = 0\%$
  - COEFFICIENT OF ACTIVE EARTH PRESSURE:  $K_a = 0.3$
  - SPECIFIC WEIGHT OF STD. AGGREGATE CONCRETE" 150 PCF
  - SPECIFIC WEIGHT OF DRY EARTH: 100 PCF
  - SPECIFIC WEIGHT OF SATURATED EARTH: 120 PCF
  - EQUIVALENT FLUID PRESSURE OF DRY EARTH: 30 PSF
  - EQUIVALENT FLUID PRESSURE OF SATURATED EARTH: 80 PSF

THE SUPPLIER SHALL PROVIDE CERTIFICATION THAT THE PRECAST MANHOLES MEET OR EXCEED THESE REQUIREMENTS PRIOR TO INSTALLATION.
- MANHOLE FRAME & LID SHALL BE CAPABLE OF WITHSTANDING MINIMUM 50,000 POUND LOADS. MANHOLE FRAME & LID SHALL BE NEENAH CATALOG NO. R-1640-C OR APPROVED EQUAL. LID FOR HIGH VOLTAGE MANHOLE SHALL BE LABELED "HIGH VOLTAGE". LID FOR LOW VOLTAGE MANHOLE SHALL BE LABELED "LOW VOLTAGE" OR "OV-600V".
- COORDINATE DUCT BANK INTERFACE & OPENINGS WITH THE MANHOLE MFR. CONTRACTOR SHALL SLOPE DUCT BANK TO PRECAST MANHOLE OPENINGS. ALL OPENINGS SHALL BE SEALED WATERTIGHT AFTER DUCT BANK INSTALLATION.
- 4'x4'x4' MANHOLE SHALL BE MANUFACTURED BY A CONCRETE ELECTRICAL MANHOLE PRODUCER ON THE ILLINOIS DEPARTMENT OF TRANSPORTATION APPROVED LIST OF CERTIFIED PRECAST CONCRETE PRODUCERS..
- 4'x4'x4' MANHOLE SHALL BE PAID FOR UNDER ITEM AR110710 ELECTRICAL MANHOLE PER EACH.
- CABLE RACKS SHALL BE HEAVY DUTY CORROSION RESISTANT NYLON MATERIAL WITH CORROSION RESISTANT STAINLESS STEEL MOUNTING HARDWARE; UNDERGROUND DEVICES, INC. CAT. NO. 3SR1N, 3SR2N OR 3SR3N OR EQUAL. PROVIDE AT LEAST TWO TRIPLE HOOK CABLE RACKS ON EACH MANHOLE WALL, SPACED TO SUPPORT RESPECTIVE CABLES.
- COORDINATE INSTALLATION OF MANHOLES WITH RESPECTIVE FINISHED GRADE ELEVATIONS.

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CONSTRUCT CROSSWIND RUNWAY 18-36 4'X4'X4' ELECTRICAL MANHOLE				
59 59 of 137 sheets				

GENERAL NOTES

- 1. ALL ELECTRICAL EQUIPMENT SHALL BE INSTALLED IN CONFORMANCE WITH NFPA 70 - NATIONAL ELECTRICAL CODE (NEC) MOST CURRENT ISSUE IN FORCE, THE RESPECTIVE EQUIPMENT MANUFACTURER'S DIRECTIONS AND ALL OTHER APPLICABLE LOCAL CODES, LAWS, ORDINANCES, AND REQUIREMENTS IN FORCE. ANY INSTALLATIONS WHICH VOID THE U.L. LISTING, ETL LISTING (OR OTHER THIRD PARTY LISTING) AND/OR THE MANUFACTURER'S WARRANTY OF A DEVICE WILL NOT BE PERMITTED.
2. CONTRACTOR SHALL KEEP A COPY OF THE LATEST NEC IN FORCE ON SITE AT ALL TIMES DURING CONSTRUCTION FOR USE AS A REFERENCE.
3. CONTRACTOR SHALL COORDINATE WORK AND ANY POWER OUTAGES AND/OR SHUT DOWN OF SYSTEMS WITH THE RESPECTIVE FACILITY OWNER PERSONNEL AND THE AIRPORT MANAGER/DIRECTOR. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).
4. THE CONTRACTOR SHALL ASCERTAIN THAT ALL LIGHTING SYSTEM COMPONENTS FURNISHED BY HIM, INCLUDING FAA APPROVED EQUIPMENT, ARE COMPATIBLE IN ALL RESPECTS WITH EACH OTHER AND THE REMAINDER OF THE NEW/EXISTING SYSTEM. ANY NONCOMPATIBLE COMPONENTS FURNISHED BY THIS CONTRACTOR SHALL BE REPLACED BY HIM AT NO ADDITIONAL COST TO THE AIRPORT SPONSOR WITH A SIMILAR UNIT, APPROVED BY THE ENGINEER (DIFFERENT MODEL OR DIFFERENT MANUFACTURER) THAT IS COMPATIBLE WITH THE REMAINDER OF THE AIRPORT LIGHTING SYSTEM.
5. IN CASE THE CONTRACTOR ELECTS TO FURNISH AND INSTALL AIRPORT LIGHTING EQUIPMENT REQUIRING ADDITIONAL WIRING, TRANSFORMERS, ADAPTORS, MOUNTINGS, ETC., TO THOSE SHOWN ON THE DRAWINGS AND/OR LISTED IN THE SPECIFICATION, ANY COST FOR THESE ITEMS SHALL BE INCIDENTAL TO THE EQUIPMENT COST.
6. THE CONTRACTOR INSTALLED EQUIPMENT (INCLUDING FAA APPROVED) SHALL NOT GENERATE ANY ELECTROMAGNETIC INTERFERENCE IN THE EXISTING AND/OR NEW COMMUNICATIONS, WEATHER, AIR NAVIGATION, AND AIR TRAFFIC CONTROL EQUIPMENT. ANY EQUIPMENT GENERATING SUCH INTERFERENCE SHALL BE REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST WITH THE EQUIPMENT MEETING THE APPLICABLE SPECIFICATIONS AND NOT GENERATING ANY INTERFERENCE.
7. WHEN A SPECIFIC TYPE, STYLE, CLASS, ETC. OF FAA APPROVED EQUIPMENT IS SPECIFIED ONLY THAT TYPE, STYLE, CLASS, WILL BE ACCEPTABLE, EVEN THOUGH EQUIPMENT OF OTHER TYPES STYLES, CLASSES, ETC. MAY BE APPROVED.
8. ANY AND ALL INSTRUCTIONS FROM THE RESIDENT ENGINEER TO THE CONTRACTOR REGARDING CHANGES IN OR DEVIATIONS FROM THE PLANS AND SPECIFICATIONS SHALL BE IN WRITING WITH COPIES SENT TO THE AIRPORT SPONSOR AND THE ILLINOIS DEPARTMENT OF TRANSPORTATION, DIVISION OF AERONAUTICS. THE CONTRACTOR SHALL NOT ACCEPT ANY VERBAL INSTRUCTIONS FROM THE RESIDENT ENGINEER REGARDING ANY CHANGES FROM THE PLANS AND SPECIFICATIONS.
9. A MINIMUM OF THREE COPIES OF THE INSTRUCTION BOOK SHALL BE SUPPLIED WITH EACH DIFFERENT TYPE OF EQUIPMENT. THE BOOKS DESCRIBING A MORE SOPHISTICATED TYPE OF EQUIPMENT, SUCH AS REGULATORS, PAPI, REIL, ETC. AS A MINIMUM SHALL CONTAIN THE FOLLOWING:
A. A DETAILED DESCRIPTION OF THE OVERALL EQUIPMENT AND ITS INDIVIDUAL COMPONENTS.
B. THEORY OF OPERATION INCLUDING THE FUNCTION OF EACH COMPONENT.
C. INSTALLATION INSTRUCTION.
D. START-UP INSTRUCTIONS.
E. PREVENTATIVE MAINTENANCE REQUIREMENTS.
F. CHART FOR TROUBLE-SHOOTING.
G. COMPLETE POWER AND CONTROL DETAILED WIRING DIAGRAM(S), SHOWING EACH CONDUCTOR/CONNECTION/COMPONENT - "BLACK" BOXES ARE NOT ACCEPTABLE. THE DIAGRAM OF THE NARRATIVE SHALL SHOW VOLTAGE/CURRENTS/WAVE SHAPES AT STRATEGIC LOCATIONS TO BE USED WHEN CHECKING AND/OR TROUBLE-SHOOTING THE EQUIPMENT. WHEN THE EQUIPMENT HAS SEVERAL MODES OF OPERATION, SUCH AS SEVERAL BRIGHTNESS STEPS, THESE PARAMETERS SHALL BE INDICATED FOR ALL DIFFERENT MODES.
H. PARTS LIST WHICH WILL INCLUDE ALL MAJOR AND MINOR COMPONENTS SUCH AS RESISTORS, DIODES, ETC. IT SHALL INCLUDE A COMPLETE NOMENCLATURE OF EACH COMPONENT AND, IF APPLICABLE, THE NAME OF ITS MANUFACTURER AND THE CATALOG NUMBER.
I. SAFETY INSTRUCTIONS.

POWER AND CONTROL NOTES

- 1. PROVIDE LEGEND PLATES FOR ALL ELECTRICAL EQUIPMENT TO IDENTIFY FUNCTION, CIRCUIT VOLTAGE AND PHASE. WHERE THE EQUIPMENT CONTAINS FUSES, ALSO IDENTIFY THE FUSE OR FUSE LINK AMPERE RATING. WHERE THE EQUIPMENT DOES NOT HAVE SUFFICIENT AREA TO INSTALL LEGEND PLATES, THE LEGEND PLATES SHALL BE INSTALLED ON THE WALL NEXT TO THE UNIT. LEGEND PLATES SHALL BE WEATHERPROOF ENGRAVED PLASTIC OR PHENOLIC MATERIAL, 1/4" HIGH BLACK LETTERS ON A WHITE BACKGROUND UNLESS NOTED OTHERWISE. SECURE WITH WEATHERPROOF ADHESIVE AND MACHINE SCREWS. FURNISH ADDITIONAL LEGEND PLATES WHERE REQUIRED BY CODE, FOR ADDITIONAL EQUIPMENT, AS DETAILED HEREIN ON THE PLANS, AND AS NOTED IN THE SPECIAL PROVISION SPECIFICATIONS.
2. COLOR CODE ALL PHASE WIRING BY THE USE OF COLORED WIRE INSULATION AND/OR COLORED TAPE. WHERE TAPE IS USED, THE WIRE INSULATION SHALL BE BLACK. BLACK AND RED SHALL BE USED FOR PHASE CONDUCTORS ON 120/240VAC SINGLE-PHASE, THREE WIRE SYSTEMS AND BLACK, ORANGE (FOR HIGH LEG) AND BLUE SHALL BE USED FOR PHASE CONDUCTORS ON 240/120VAC THREE-PHASE, FOUR WIRE SYSTEMS. NEUTRAL CONDUCTORS, SIZE NO. 6 AWG OR SMALLER, SHALL BE IDENTIFIED BY A CONTINUOUS WHITE OR NATURAL GRAY OUTER FINISH ALONG ITS ENTIRE LENGTH. NEUTRAL CONDUCTORS LARGER THAN NO. 6 AWG SHALL BE IDENTIFIED EITHER BY A CONTINUOUS WHITE OR NATURAL GRAY OUTER FINISH ALONG ITS ENTIRE LENGTH OR BY THE USE OF WHITE TAPE AT ITS TERMINATIONS AND INSIDE ACCESSIBLE WIREWAYS. INSULATED GROUND CONDUCTORS SHALL HAVE GREEN COLORED INSULATION FOR ALL CONDUCTOR SIZES (AWG OR KCMIL).
3. ALL BRANCH CIRCUIT CONDUCTORS CONNECTED TO A PARTICULAR PHASE SHALL BE IDENTIFIED WITH THE SAME COLOR. THE COLOR CODING SHALL BE EXTENDED TO THE POINT OF UTILIZATION.
4. IN CONTROL WIRING THE SAME COLOR SHALL BE USED THROUGHOUT THE SYSTEM FOR THE SAME FUNCTION, SUCH AS 10%, 30%, 100% BRIGHTNESS CONTROL, ETC.
5. LOW VOLTAGE (600 V.) AND HIGH VOLTAGE (5000 V.) CONDUCTORS SHALL BE INSTALLED IN SEPARATE WIREWAYS.
6. NEATLY LACE WIRING IN DISTRIBUTION PANELS, WIREWAYS, SWITCHES AND JUNCTION/PULL BOXES.
7. THE MINIMUM SIZE OF PULL/JUNCTION BOXES, REGARDLESS OF THE QUANTITY AND SIZE OF THE CONDUCTORS SHOWN, SHALL BE AS FOLLOWS:
A. IN STRAIGHT PULLS THE LENGTH OF THE BOX SHALL NOT BE LESS THAN EIGHT TIMES THE TRADE DIAMETER OF THE LARGER CONDUIT. THE TOTAL AREA (INCLUDING THE CONDUIT CROSS-SECTIONAL AREA) OF A BOX END SHALL BE AT LEAST 3 TIMES GREATER THAN THE TOTAL TRADE CROSS-SECTIONAL AREA OF THE CONDUITS TERMINATING AT THE END.
B. IN ANGLE PULLS OR 'U' PULLS THE DISTANCE BETWEEN EACH CONDUIT ENTRY INSIDE THE BOX AND THE OPPOSITE WALL OF THE BOX SHALL NOT BE LESS THAN SIX (6) TIMES THE TRADE DIAMETER OF THE LARGEST CONDUIT. THIS DISTANCE SHALL BE INCREASED FOR ADDITIONAL ENTRIES BY THE AMOUNT OF THE SUM OF THE DIAMETERS OF ALL OTHER CONDUIT ENTRIES ON THE SAME WALL AS THE BOX. THE DISTANCE BETWEEN CONDUIT ENTRIES ENCLOSING THE SAME CONDUCTOR SHALL NOT BE LESS THAN SIX TIMES THE TRADE DIAMETER OF THE LARGEST CONDUIT.
8. A RUN OF CONDUIT BETWEEN TERMINATIONS AT EQUIPMENT ENCLOSURES, SQUARE DUCTS AND PULL/JUNCTION BOXES, SHALL NOT CONTAIN MORE THAN THE EQUIVALENT OF FOUR QUARTER BENDS (360 DEGREES TOTAL), INCLUDING THOSE BENDS LOCATED IMMEDIATELY AT THE TERMINATIONS, CAST, CONDUIT TYPE OUTLETS SHALL NOT BE TREATED AS PULL/JUNCTION BOXES.
9. EQUIPMENT CABINETS SHALL NOT BE USED AS PULL/JUNCTION BOXES. ONLY WIRING TERMINATING AT THE EQUIPMENT SHALL BE BROUGHT INTO THESE ENCLOSURES.
10. SPLICES AND JUNCTION POINTS SHALL BE PERMITTED ONLY IN JUNCTION BOXES, DUCTS EQUIPPED WITH REMOVABLE COVERS, AND AT EASILY ACCESSIBLE LOCATIONS.
11. CIRCUIT BREAKERS IN POWER DISTRIBUTION PANEL(S) SHALL BE THERMAL-MAGNETIC MOLDED CASE, PERMANENT TRIP WITH 100 AMPERE, MINIMUM FRAME.
12. DUAL LUGS SHALL BE USED WHERE TWO (2) WIRES, SIZE NO. 6 OR LARGER, ARE TO BE CONNECTED TO THE SAME TERMINAL.
13. ALL INTERIOR WALL MOUNTED EQUIPMENT ENCLOSURES SHALL BE MOUNTED ON HOT DIPPED GALVANIZED STEEL STRUT SUPPORT, OR STAINLESS STEEL STRUT SUPPORT, WITH CORROSION RESISTANT HARDWARE.
14. SUPPORT FOR EXTERIOR MOUNTED EQUIPMENT SHALL USE HOT DIPPED GALVANIZED STEEL STRUT SUPPORT OR STAINLESS STEEL STRUT SUPPORT WITH STAINLESS STEEL HARDWARE. PROVIDE ZINC RICH PAINT APPLIED TO FIELD CUTS OF GALVANIZED STEEL SUPPORT TO MINIMIZE THE POTENTIAL FOR CORROSION PER THE RESPECTIVE STRUT SUPPORT MANUFACTURER'S RECOMMENDATIONS.

- 15. CONDUITS FOR ELECTRIC SERVICE ENTRANCE AND FEEDERS SHALL BE AS DETAILED HEREIN ON THE PLANS. WHERE GALVANIZED RIGID STEEL CONDUIT IS SPECIFIED IT SHALL HAVE THREADED FITTINGS. SET SCREW TYPE FITTINGS WILL NOT BE ACCEPTABLE. CONDUITS FOR UNDERGROUND APPLICATIONS SHALL BE AS DETAILED HEREIN. CONDUITS FOR GROUNDING ELECTRODE CONDUCTORS OR INDIVIDUAL GROUNDING CONDUCTORS SHALL BE SCHEDULE 40 OR SCHEDULE 80 PVC.
16. PROVIDE LIQUID TIGHT FLEXIBLE METAL CONDUIT AT CONNECTIONS TO EQUIPMENT SUBJECT TO VIBRATION OR WHERE FLEXIBILITY IS REQUIRED. LIQUID TIGHT FLEXIBLE METAL CONDUIT AND ASSOCIATED FITTINGS SHALL BE U.L. LISTED TO MEET THE REQUIREMENTS OF NEC 350.6, SUITABLE FOR GROUNDING, SUNLIGHT RESISTANT, AND RESISTANT TO OIL, GASOLINE, AND GREASE. LIQUID TIGHT FLEXIBLE METAL CONDUIT THAT IS USED FOR FLEXIBILITY (INCLUDING CONNECTIONS TO MOTORS, TRANSFORMERS, & CONSTANT CURRENT REGULATORS) SHALL REQUIRE AN EXTERNAL BONDING JUMPER OR INTERNAL EQUIPMENT GROUNDING CONDUCTOR PER NEC 350.60. DO NOT INSTALL LIQUID TIGHT FLEXIBLE METAL CONDUIT THAT IS NOT U.L. LISTED. CONFIRM LIQUID-TIGHT FLEXIBLE METAL CONDUIT BEARS THE UL LABEL PRIOR TO INSTALLING IT.
17. UNLESS OTHERWISE SHOWN, ALL EXPOSED CONDUITS SHALL BE RUN PARALLEL TO OR AT RIGHT ANGLES WITH THE LINES OF THE STRUCTURE.
18. ALL STEEL CONDUITS, FITTINGS, NUTS, BOLTS, ETC. SHALL BE GALVANIZED.
19. USE CONDUIT BUSHINGS AT EACH CONDUIT TERMINATION. WHERE NO. 4 AWG OR LARGER UNDERGROUND WIRE IS INSTALLED, USE INSULATED BUSHINGS.
20. USE DOUBLE LOCK NUTS AT EACH CONDUIT TERMINATION.
21. WRAP ALL PRIMARY AND SECONDARY POWER TRANSFORMER CONNECTIONS WITH SUFFICIENT LAYERS OF INSULATING TAPE (3M SCOTCH 23 ALL-VOLTAGE SPLICING TAPE, 3M SCOTCH 130C LINERLESS RUBBER SPLICING TAPE, OR APPROVED EQUAL) AND COVER WITH VINYL ELECTRICAL TAPE (3M SCOTCH 88 VINYL ELECTRICAL TAPE OR APPROVED EQUAL) FOR FULL VALUE OF CABLE INSULATION VOLTAGE.
22. UNLESS OTHERWISE NOTED, ALL SINGLE CONDUCTOR CONTROL WIRING SHALL BE NO. 12 AWG. COPPER MINIMUM.
23. THE FOLLOWING SHALL APPLY TO RELAY/CONTACTOR PANELS/ENCLOSURES:
A. FOR INTERIOR LOCATIONS ALL COMPONENTS SHALL BE MOUNTED IN NEMA 12 (DUST TIGHT) ENCLOSURE(S) WITH VERTICALLY HINGED COVERS. FOR EXTERIOR/OUTDOOR LOCATIONS ALL COMPONENTS SHALL BE MOUNTED IN NEMA 4X STAINLESS STEEL ENCLOSURE(S) WITH VERTICALLY HINGED COVERS. ALL CONDUIT ENTRIES INTO NEMA 4, 4X ENCLOSURES SHALL HAVE NEMA 4 HUBS LISTED SUITABLE FOR THE RESPECTIVE ENCLOSURE TO MAINTAIN THE NEMA 4, 4X RATING OF THE ENCLOSURE.
B. THE ENCLOSURE(S) SHALL HAVE AMPLE SPACE FOR THE CIRCUIT COMPONENTS, TERMINAL BLOCKS AND INCOMING AND INTERNAL WIRING.
C. ALL CONTROL CONDUCTOR TERMINATIONS SHALL BE OF THE OPEN-EYE CONNECTOR/SCREW TYPE. SOLDERED CLOSED-EYE TERMINATIONS, OR TERMINATIONS WITHOUT CONNECTORS ARE NOT ACCEPTABLE.
D. WHEN THE ENCLOSURE COVER IS OPENED, ALL CIRCUIT COMPONENTS, WIRING AND TERMINALS SHALL BE EXPOSED AND ACCESSIBLE WITHOUT REMOVAL OF ANY PANELS, COVERS, ETC., EXCEPT THOSE COVERING HIGH VOLTAGE COMPONENTS.
E. ACCESS TO, OR REMOVAL OF A CIRCUIT COMPONENT OR TERMINAL BLOCK WILL NOT REQUIRE THE REMOVAL OF ANY OTHER CIRCUIT COMPONENT OR TERMINAL BLOCK.
F. EACH CIRCUIT COMPONENT SHALL BE CLEARLY IDENTIFIED INDICATING ITS CORRESPONDING NUMBER SHOWN ON THE DRAWINGS AND ITS FUNCTION.
G. A COMPLETE WIRING DIAGRAM SHALL BE MOUNTED ON THE INSIDE OF THE COVER. THE DIAGRAM SHALL REPRESENT EACH CONDUCTOR BY A SEPARATE LINE.
H. THE DIAGRAM SHALL IDENTIFY EACH CIRCUIT COMPONENT AN NUMBERING AND COLOR OF EACH TERMINAL CONDUCTOR AND TERMINAL.
I. ALL WIRING SHALL BE NEATLY TRAINED AND LACED.
J. MINIMUM WIRE SIZE SHALL BE NO. 12 AWG.
24. FURNISH & INSTALL A WEATHERPROOF WARNING LABEL FOR EACH METER SOCKET, SERVICE DISCONNECT, SAFETY SWITCH, CUTOUT, PANELBOARD, & CONTROL PANEL TO WARN PERSONS OF POTENTIAL ELECTRIC ARC FLASH HAZARDS, PER THE REQUIREMENTS OF NEC 110.16 "FLASH PROTECTION".

REVISION table with columns for DATE and REVISION. Project title: EDGAR COUNTY AIRPORT PARIS, ILLINOIS. Project number: A.I.P. PROJ.: 3-17-0077-B13. IL PROJ.: PRG-4018. Drawing title: CONSTRUCT CROSSWIND RUNWAY 18-36 ELECTRICAL NOTES SHEET 1. Drawing number: 60 of 137 sheets. Company logo: HANSON Professional Services Inc. 1525 South Sixth Street, Springfield, Illinois 62703-2886, www.hanson-inc.com, Offices Nationwide.

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**AIRFIELD LIGHTING NOTES**


1. UNLESS OTHERWISE NOTED, ALL UNDERGROUND AIRFIELD LIGHTING SERIES CIRCUIT CONDUCTORS WHETHER DEB OR IN DUCT/CONDUIT SHALL BE FAA APPROVED 5000 VOLT L-824 TYPE. ALL UNDERGROUND FIELD POWER LOW VOLTAGE (600 VOLT & BELOW) CIRCUIT CONDUCTORS WHETHER DEB OR IN DUCT/CONDUIT SHALL BE UL LISTED 600 VOLT, TYPE XLP-USE-2 COPPER CONDUCTORS. CONDUCTOR SIZES SHALL BE AS SPECIFIED, HEREIN.
2. NO COMPONENTS OF PRIMARY CIRCUIT SUCH AS CABLE, CONNECTORS AND TRANSFORMERS SHALL BE BROUGHT ABOVE GROUND AT EDGE LIGHTS, SIGNS, REIL, PAPI, ETC.
3. THERE SHALL BE NO EXPOSED POWER/CONTROL CABLES BETWEEN THE POINT WHERE THEY LEAVE THE UNDERGROUND (DEB OR L-867 BASES) AND WHERE THEY ENTER THE EQUIPMENT (SUCH AS TAXIWAY SIGNS, PAPI, REIL, ETC.) ENCLOSURES. THESE CABLES SHALL BE ENCLOSED IN RIGID CONDUIT OR IN FLEXIBLE, WATERTIGHT CONDUIT WITH BREAKABLE COUPLING(S) AT THE GRADE OR THE HOUSING COVER, AS SHOWN IN APPLICABLE DETAILS.
4. THE JOINTS OF THE L-823 PRIMARY CONNECTORS SHALL BE WRAPPED WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF THE JOINT, AS SHOWN ON ELECTRICAL DETAILS SHEET 1.
5. THE CABLE ENTRANCE INTO THE FIELD-ATTACHED L-823 CONNECTORS SHALL BE ENCLOSED BY A HEAT-SHRINKABLE TUBING WITH CONTINUOUS INTERNAL ADHESIVE, AS SHOWN ON ELECTRICAL DETAILS SHEET 1.
6. L-823 TYPE II, TWO-CONDUCTOR SECONDARY CONNECTORS SHALL BE CLASS 'A' (FACTORY MOLDED).
7. THERE SHALL BE NO SPLICES IN THE SECONDARY CABLE(S) WITHIN THE STEMS OF A RUNWAY/TAXIWAY EDGE/THRESHOLD LIGHTING FIXTURE AND THE WIREWAYS LEADING TO TAXIWAY SIGNS AND PAPI/REIL EQUIPMENT.
8. ELECTRICAL INSULATING GREASE SHALL BE APPLIED WITHIN THE L-823, SECONDARY, TWO CONDUCTOR CONNECTORS TO PREVENT WATER ENTRANCE. THESE CONNECTORS SHALL NOT BE TAPED.
9. DEB ISOLATION TRANSFORMERS SHALL BE BURIED AT A DEPTH OF TEN (10") INCHES ON A LINE CROSSING THE LIGHT AND PERPENDICULAR TO THE RUNWAY/TAXIWAY CENTERLINE AT A LOCATION TWELVE (12") INCHES FROM THE LIGHT OPPOSITE FROM THE RUNWAY/TAXIWAY.
10. A SLACK OF THREE (3') FEET, MINIMUM, SHALL BE PROVIDED IN THE PRIMARY CABLE AT EACH TRANSFORMER/CONNECTOR TERMINATION. AT STAKE-MOUNTED LIGHTS, THE SLACK SHALL BE LOOSELY COILED IMMEDIATELY BELOW THE ISOLATION TRANSFORMER.
11. DIRECTION OF PRIMARY CABLES SHALL BE IDENTIFIED BY COLOR CODING AS FOLLOWS: WHEN FACING LIGHT WITH BACK TO PAVEMENT, CABLE TO THE LEFT IS CODED RED AND CABLE TO RIGHT IS CODED BLUE. THIS APPLIES TO STAKE MOUNTED LIGHTS AND BASE MOUNTED LIGHTS WHERE THE BASE HAS ONLY ONE ENTRANCE.
12. L-867 BASES SHALL BE SIZE B, 24" DEEP, CLASS I, UNLESS OTHERWISE NOTED.
13. BASE MOUNTED BREAKABLE COUPLINGS SHALL NOT HAVE WEEP HOLES TO THE OUTSIDE. PLUGGED UP HOLES SHALL NOT BE ACCEPTABLE. IT SHALL BE A 1/4" DIAMETER, MINIMUM, OR EQUIVALENT OPENING FOR DRAINAGE FROM THE SPACE AROUND THE SECONDARY CONNECTOR INTO THE L-867 BASE.
14. THE ELEVATION OF THE BREAKABLE COUPLING GROOVE SHALL NOT EXCEED 1-1/2" ABOVE THE EDGE OF THE COVER IN CASE OF BASE MOUNTED COUPLINGS, OR THE TOP OF THE STAKE IN CASE OF STAKE MOUNTED COUPLINGS.
15. WHERE THE BREAKABLE COUPLING IS NOT AN INTEGRAL PART OF THE LIGHT FIXTURE STEM OR MOUNTING LEG, A BEAD OF SILICON SEAL SHALL BE APPLIED COMPLETELY AROUND LIGHT STEM OR WIREWAY AT BREAKABLE COUPLING TO PROVIDE A WATERTIGHT SEAL.
16. TOPS OF THE STAKES SUPPORTING LIGHT FIXTURES SHALL BE FLUSH WITH THE SURROUNDING GRADE.
17. PLASTIC LIGHTING FIXTURE COMPONENTS, SUCH AS LAMP HEADS, STEMS, BREAKABLE COUPLINGS, BASE COVERS, BRACKETS, STAKES, SHALL NOT BE ACCEPTABLE.
18. THE TOLERANCE FOR THE HEIGHT OF RUNWAY/TAXIWAY EDGE LIGHTS SHALL BE: ONE (1) INCH. IN CASE OF STAKE MOUNTED LIGHTS, THE SPECIFIED LIGHTING FIXTURE HEIGHT SHALL BE MEASURED BETWEEN THE TOP OF THE STAKE AND THE TOP OF THE LENS. IN CASE OF BASE MOUNTED LIGHTS, THE SPECIFIED LIGHTING FIXTURE HEIGHT SHALL BE MEASURED BETWEEN THE TOP OF THE BASE FLANGE AND THE TOP OF THE LENS, THUS INCLUDING THE BASE COVER, THE FRANGIBLE COUPLING, THE STEM, THE LAMP HOUSING AND THE LENS.
19. THE TOLERANCE FOR THE LATERAL SPACING (LIGHT LANE TO RUNWAY/TAXIWAY CENTERLINE) OF RUNWAY/TAXIWAY EDGE LIGHTS SHALL BE ONE (1) INCH. THIS ALSO APPLIES AT INTERSECTIONS TO LATERAL SPACING BETWEEN LIGHTS OF A RUNWAY/TAXIWAY AND THE INTERSECTING RUNWAY/TAXIWAY.

20. ENTRANCES INTO L-867 BASES SHALL HAVE CONDUIT COUPLINGS OR REDUCERS TO INTERFACE UNIT DUCT/CONDUIT TO L-867 BASE HUBS, OR SHALL BE SEALED WITH HEAT SHRINK AS SHOWN IN DETAIL "B" ON ELECTRICAL DETAILS SHEET 1.
21. GALVANIZED/PAINTED EQUIPMENT/COMPONENT SURFACES SHALL NOT BE DAMAGED BY DRILLING, FILING, ETC. DRAIN HOLES IN METAL TRANSFORMER HOUSINGS SHALL BE MADE BEFORE GALVANIZING.
22. EDGE LIGHT NUMBERING TAGS SHALL BE FACING THE PAVEMENT.
23. CABLE/SPLICE/DUCT MARKERS SHALL BE PRECAST CONCRETE OF THE SIZE SHOWN. LETTERS/NUMBERS/ARROWS FOR THE LEGEND TO BE IMPRESSED INTO THE TOPS OF THE MARKERS SHALL BE PRE-ASSEMBLED AND SECURED IN THE MOLD BEFORE THE CONCRETE IS POURED. LEGEND INSCRIBED BY HAND IN WET CONCRETE SHALL NOT BE ACCEPTABLE.
24. ALL UNDERGROUND CABLE RUNS SHALL BE IDENTIFIED BY CABLE MARKERS AT 200 FEET MAXIMUM SPACING, WITH AN ADDITIONAL MARKER AT EACH CHANGE OF DIRECTION OF THE CABLE RUN. CABLE MARKERS SHALL BE INSTALLED IMMEDIATELY ABOVE THE CABLES.
25. THERE SHALL BE NO SPLICES BETWEEN THE ISOLATION TRANSFORMERS. L-823 CONNECTORS ARE ALLOWED AT TRANSFORMER CONNECTIONS ONLY, UNLESS OTHERWISE SHOWN.
26. APPLY AN OXIDE INHIBITING, ANTI-SEIZING COMPOUND TO ALL SCREWS, NUTS AND BREAKAGE COUPLING THREADS.
27. LOCATIONS OF ENDS OF ALL UNDERGROUND DUCTS SHALL BE IDENTIFIED BY DUCT MARKERS.
28. WHERE A PARALLEL, CONSTANT VOLTAGE PAPI SYSTEM IS PROVIDED, THE "T" SPLICES SHALL BE OF THE CAST TYPE.
29. CONCRETE USED FOR SLABS, FOOTINGS, BACKFILL AROUND TRANSFORMER HOUSINGS, MARKINGS, ETC. SHALL BE 3500 PSI, AIR-ENTRAINED.
30. ALL POWER AND CONTROL CABLES IN MAN/HAND HOLES SHALL BE TAGGED. USE EMBOSSED COPPER STRIPS TO BE ATTACHED AT BOTH ENDS TO THE CABLE BY THE USE OF PLASTIC STRAPS. MINIMUM OF TWO TAGS SHALL BE PROVIDED ON EACH CABLE IN A MAN/HAND HOLE-ONE AT THE CABLE ENTRANCE AND ONE AT THE CABLE EXIT.
31. THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. **CONTACT J.U.L.I.E. FOR UTILITY INFORMATION AT 1-800-892-0123.** ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.
32. WHEN PREPARING CABLE FOR SPLICES, THE CONTRACTOR SHALL USE A CABLE STRIPPER/PENCILLER WHENEVER CABLE CONNECTIONS ARE MADE.

**GROUNDING NOTES FOR AIRFIELD LIGHTING**

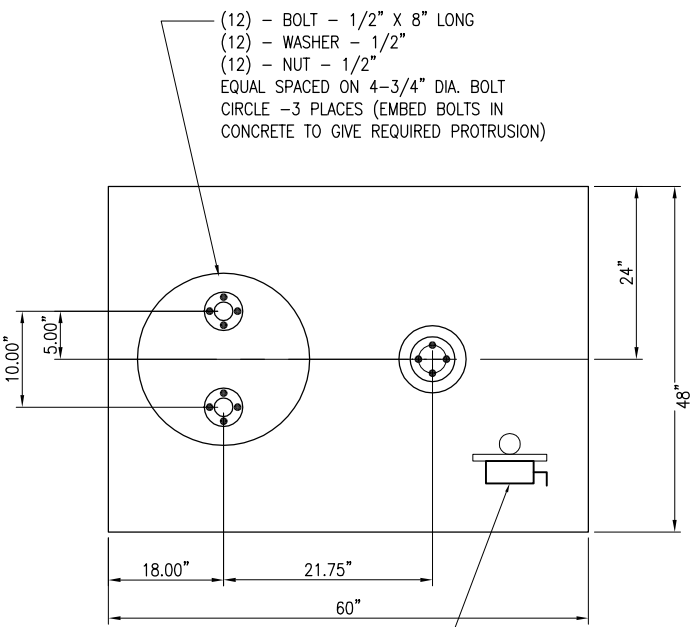
1. GROUNDING FOR RUNWAY LIGHTS, TAXIWAY LIGHTS, AND LIGHTED TAXI GUIDANCE SIGNS SHALL BE AS DETAILED ON THE PLANS AND AS SPECIFIED HEREIN. PER FAA AC 150/5340-30E DESIGN AND INSTALLATION DETAILS FOR AIRPORT VISUAL AIDS, CHAPTER 12, PART 12.6; A GROUND MUST BE INSTALLED AT EACH LIGHT FIXTURE. THE PURPOSE OF THE LIGHT BASE GROUND IS TO PROVIDE A DEGREE OF PROTECTION FOR MAINTENANCE PERSONNEL FROM POSSIBLE CONTACT WITH AN ENERGIZED LIGHT BASE OR MOUNTING STAKE THAT MAY RESULT FROM A SHORTED POWER CABLE OR ISOLATION TRANSFORMER. A LIGHT BASE GROUND SHALL BE INSTALLED AT EACH TRANSFORMER BASE/LIGHT CAN ASSOCIATED WITH RUNWAY LIGHTS, TAXIWAY LIGHTS, AND LIGHTED TAXI GUIDANCE SIGNS. A LIGHT BASE GROUND SHALL ALSO BE INSTALLED AT EACH STAKE MOUNTED LIGHT FIXTURE. A LIGHT BASE GROUND SHALL BE INSTALLED AND CONNECTED TO THE METAL FRAME OF EACH TAXI GUIDANCE SIGN AS DETAILED ON THE PLANS AND IN ACCORDANCE WITH THE RESPECTIVE TAXI GUIDANCE SIGN MANUFACTURER RECOMMENDATIONS. THE LIGHT BASE GROUND SHALL BE A #6 AWG BARE COPPER CONDUCTOR BONDED TO THE GROUND LUG ON THE RESPECTIVE L-867 TRANSFORMER BASE/LIGHT CAN OR MOUNTING STAKE AND A 5/8-INCH DIAMETER BY 8-FOOT LONG (MINIMUM) UL LISTED COPPER CLAD GROUND ROD. CONNECTIONS TO GROUND LUGS ON THE L-867 TRANSFORMER BASE/LIGHT CAN OR MOUNTING STAKE SHALL BE WITH A UL LISTED GROUNDING CONNECTOR. CONNECTIONS TO GROUND RODS SHALL BE MADE WITH EXOTHERMIC WELD TYPE CONNECTORS, CADWELD BY ERICO PRODUCTS, INC., SOLON, OHIO, (PHONE: 800-248-9353), THERMOWELD BY CONTINENTAL INDUSTRIES, INC., TULSA, OKLAHOMA (PHONE: 918-663-1440), ULTRAWELD BY HARGER, GRAYSLAKE, ILLINOIS (PHONE: 800-842-7437), OR APPROVED EQUAL. EXOTHERMIC WELD CONNECTIONS SHALL BE INSTALLED IN CONFORMANCE WITH THE RESPECTIVE MANUFACTURER'S DIRECTIONS USING MOLDS AS REQUIRED FOR EACH RESPECTIVE APPLICATION. BOLTED CONNECTIONS WILL NOT BE PERMITTED AT GROUND RODS. TOP OF GROUND RODS SHALL BE BURIED 12 INCHES MINIMUM BELOW GRADE, UNLESS SPECIFIED OTHERWISE HEREIN, FOR RESPECTIVE APPLICATIONS.
2. CLEAN ALL METAL SURFACES BEFORE MAKING GROUND CONNECTIONS. METALLIC SURFACES TO BE JOINED SHALL BE PREPARED BY THE REMOVAL OF ALL NON-CONDUCTIVE MATERIAL PER 2008 NATIONAL ELECTRICAL CODE ARTICLE 250-12.
3. PER FAA 150/5340-30E THE RESISTANCE TO GROUND OF THE RESPECTIVE MOUNTING STAKE OR LIGHT BASE (WITH GROUND ROD CONNECTED) MUST BE 25 OHMS OR LESS.

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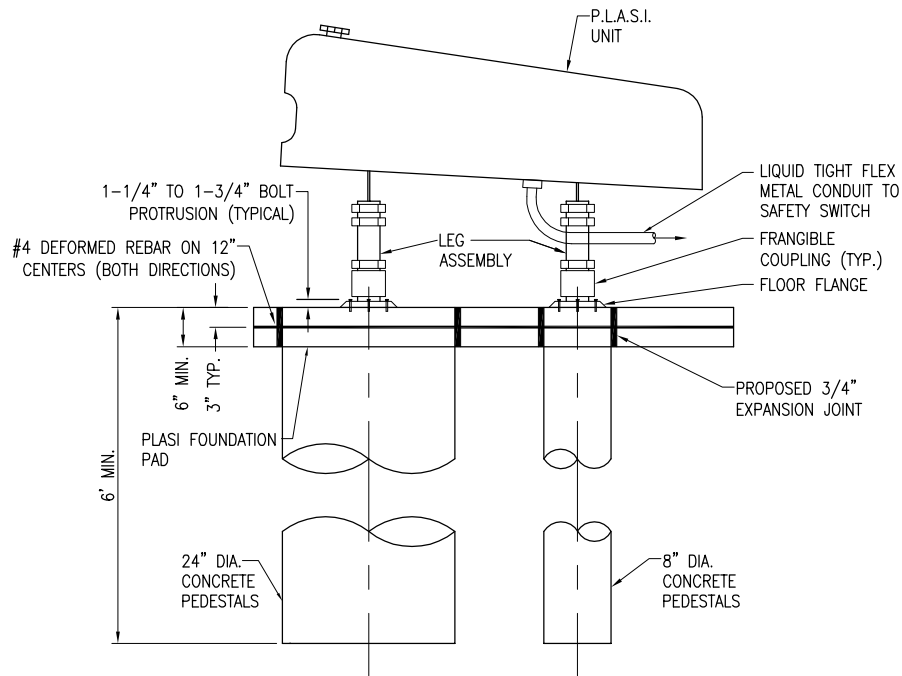
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<p>ILL. PROJ.: PRG-401B A.I.P. PROJ.: 3-17-0077-B13</p>					
<p>Hanson Proj. No. 10A00053 Filename E-003.DWG Scale NONE Date 03/10/11</p>		<p>LAYOUT KNL 12/23/10 DRAWN MLH 12/23/10 REVIEWED CAH 02/04/11</p>			
 <p><b>HANSON</b> Professional Services Inc. 2011 1525 South Sixth Street Springfield, Illinois 62703-2886 www.hanson-inc.com Offices Nationwide</p>					
<p><b>CONSTRUCT CROSSWIND</b> <b>RUNWAY 18-36</b></p> <p><b>ELECTRICAL NOTES</b> <b>SHEET 2</b></p>					
<p><b>61</b></p> <p>61 of 137 sheets</p>					

**PLASI NOTES**

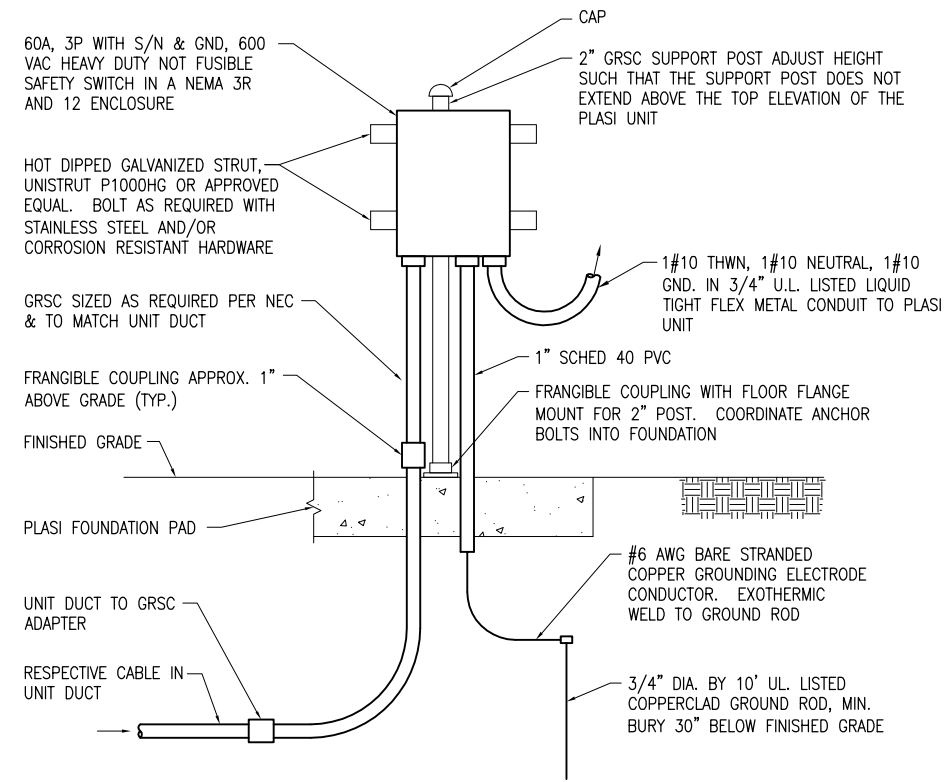
1. THE PROPOSED P.L.A.S.I. UNIT WILL BE LOCATED TO 87.5' LT. STA. 104+50 ALONG RUNWAY 18-36.
2. THE PROPOSED P.L.A.S.I. CONCRETE BASE WILL BE CONSTRUCTED AS SHOWN IN THE DETAIL ON THIS SHEET. CONFIRM DIMENSIONS OF PLASI MOUNTING HARDWARE WITH PLASI MANUFACTURER.
3. THE CONCRETE FOR THE PROPOSED P.L.A.S.I. BASE IN ACCORDANCE WITH ITEM 610.
4. THE POWER CABLES SHALL BE REROUTED AND PLACED A MINIMUM OF 18" BELOW THE FINISHED GRADE AND LOCATED AS SHOWN ON PROPOSED ELECTRICAL PLAN.
5. INSTALLATION OF THE P.L.A.S.I. UNIT, P.L.A.S.I. BASE, AND ALL INCIDENTALS WILL BE PAID FOR UNDER ITEM: AR125630 PLASI \_\_\_\_\_ 1 EACH.



FOUNDATION PAD PLAN  
"NOT TO SCALE"

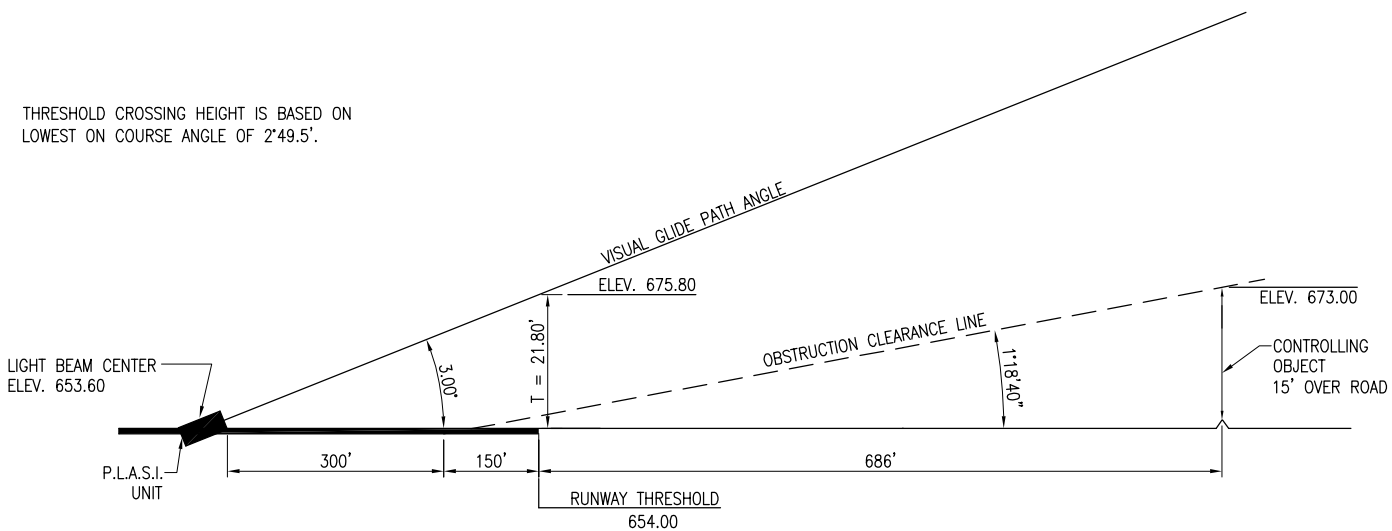


PLASI ELEVATION  
"NOT TO SCALE"



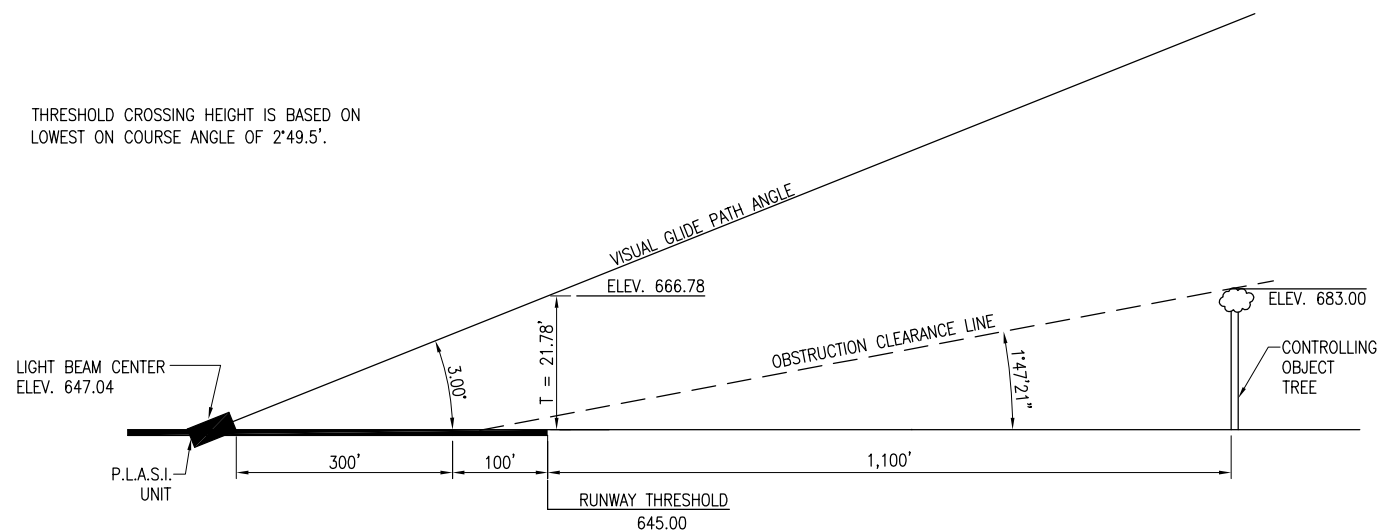
PLASI SAFETY SWITCH ELEVATION  
"NOT TO SCALE"

THRESHOLD CROSSING HEIGHT IS BASED ON LOWEST ON COURSE ANGLE OF 2°49.5'.



P.L.A.S.I. AIMING DIAGRAM 36 END  
"NOT TO SCALE"

THRESHOLD CROSSING HEIGHT IS BASED ON LOWEST ON COURSE ANGLE OF 2°49.5'.



P.L.A.S.I. AIMING DIAGRAM 18 END

REVISION	DATE

EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS  
A.I.P. PROJ.: 3-17-0077-B13  
IL PROJ.: PRG-4018

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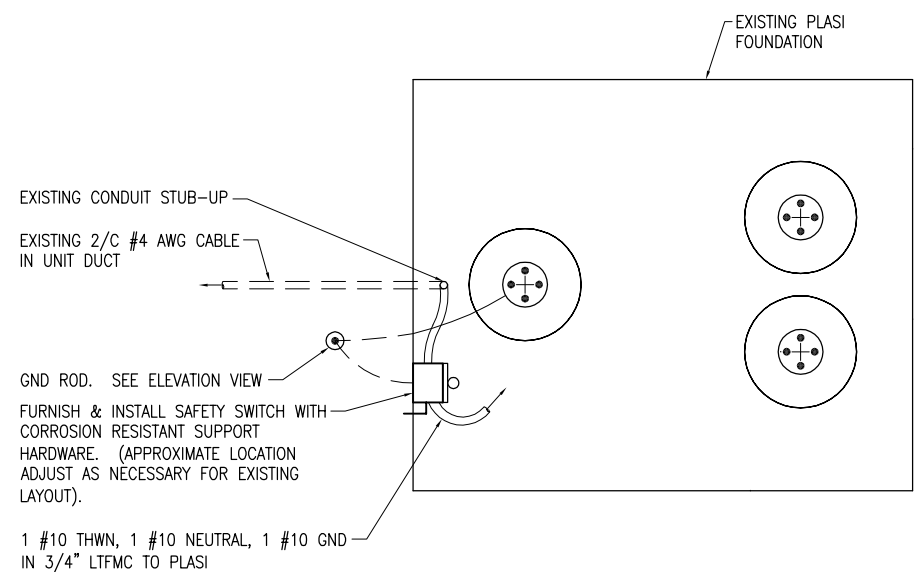
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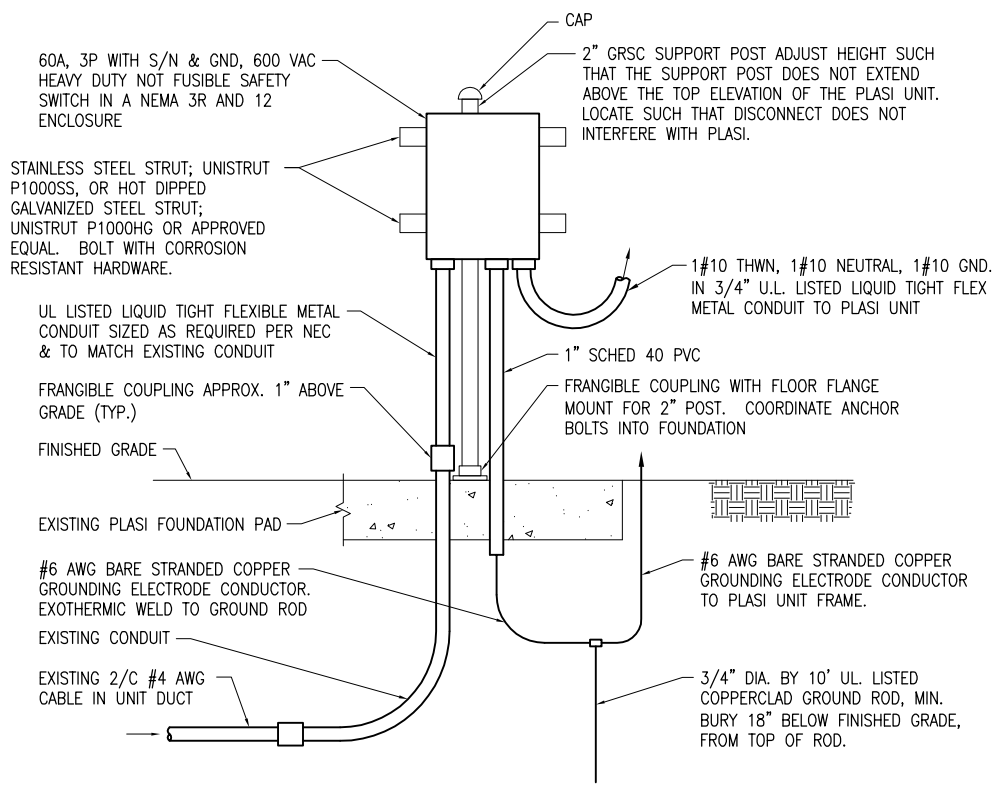
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**NOTES**

1. PLASI UNITS LOCATED ON RUNWAY 9-27 ARE EXISTING. A HEAVY DUTY 60 AMP, 2 POLE OR 3 POLE WITH SOLID NEUTRAL & GND, 600 VAC NOT FUSIBLE SAFETY SWITCH IN A NEMA 3R AND 12 ENCLOSURE SHALL BE FURNISHED & INSTALLED AT EACH PLASI. 60 AMP SAFETY SWITCH IS REQUIRED TO ACCOMMODATE #4 AWG OR #2 AWG CONDUCTORS FOR LUG TERMINATIONS & WIRE BENDING SPACE. NEUTRAL SHALL BE BONDED TO GROUND IN SAFETY SWITCH & GND ROD SHALL BE INSTALLED TO CONFORM WITH NEC 250.32
2. LTFMC DENOTES LIQUID TIGHT FLEXIBLE METAL CONDUIT UL LISTED, SUITABLE FOR GROUNDING, AND SUNLIGHT RESISTANT. NEC 350.6 NOTES LIQUID TIGHT FLEXIBLE METAL CONDUIT AND ASSOCIATED FITTINGS SHALL BE LISTED. DO NOT INSTALL LTFMC THAT IS NOT UL LISTED. CONFIRM LTFMC BEARS THE UL LABEL PRIOR TO INSTALLATION
3. CONTRACTOR SHALL FURNISH & INSTALL BOOST TRANSFORMERS FOR PLASI UNITS IN THE VAULT. BOOST TRANSFORMERS SHALL BE SIZED & CONNECTED TO PROVIDE THE PROPER VOLTAGE AT THE RESPECTIVE PLASI UNIT AS RECOMMENDED BY THE PLASI MANUFACTURER (DEVORE AVIATION CORPORATION, 6104 JEFFERSON BLVD, N.E., ALBUQUERQUE, NEW MEXICO 87109-3410, PHONE: 505-345-8713, FAX: 505-344-3835).
4. INSTALLATION OF THE SAFETY SWITCH AND ASSOCIATED CONDUIT, WIRING, FITTINGS, GROUNDING, & ACCESSORIES FOR EACH PLASI UNIT SHALL BE PAID FOR UNDER ITEM AR125989 REFURBISH PLASI PER EACH.
5. SAFETY SWITCHES SHALL BE MANUFACTURED IN THE UNITED STATES OF AMERICA TO COMPLY WITH THE AIRPORT IMPROVEMENT PROGRAM BUY AMERICAN REQUIREMENTS. PROVIDE CERTIFICATION OF MANUFACTURE IN THE UNITED STATES WITH SHOP DRAWING SUBMITTAL.

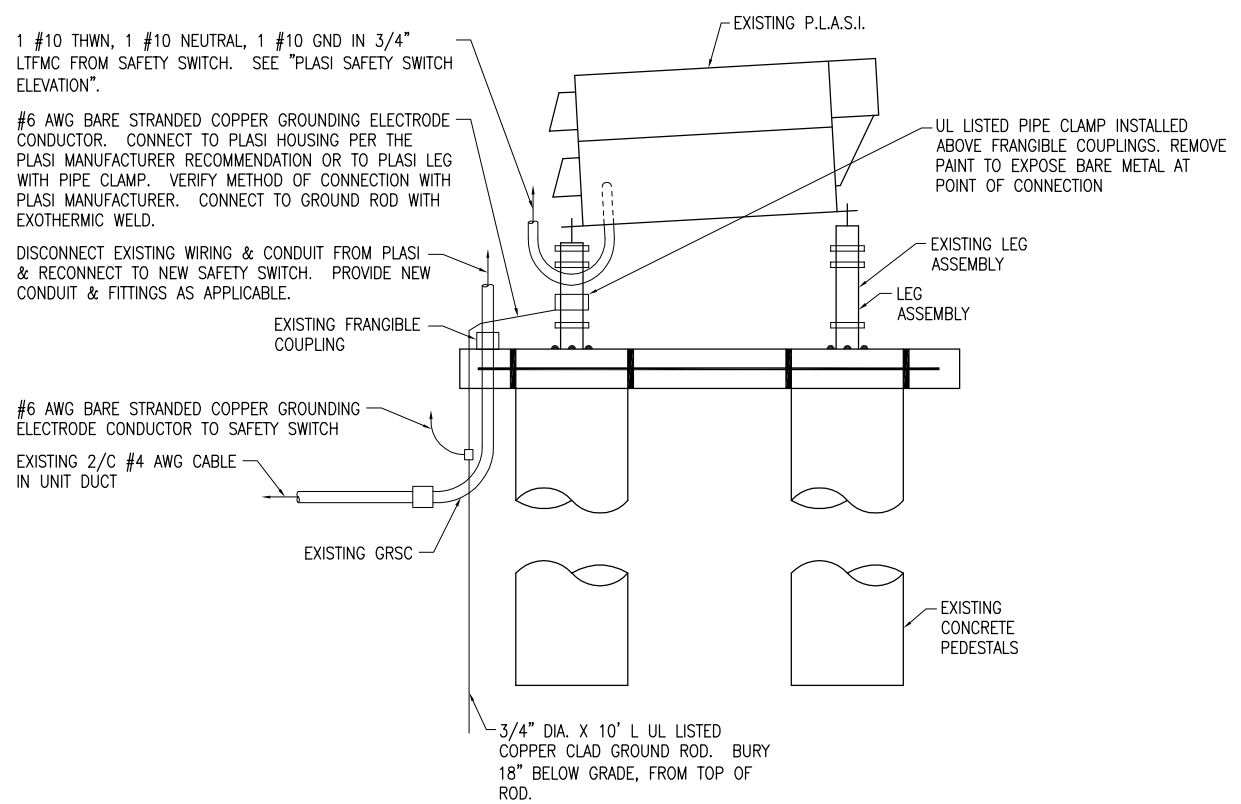


**PLASI FOUNDATION PLAN VIEW**



NOTE: SEE NEW ELECTRICAL ONE LINE DIAGRAM FOR VAULT AND PLASI FOR ADDITIONAL INFORMATION ON EQUIPMENT AND WIRING.


**PLASI SAFETY SWITCH ELEVATION**  
"NOT TO SCALE"

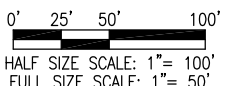
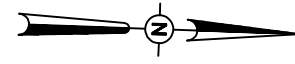
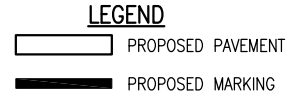
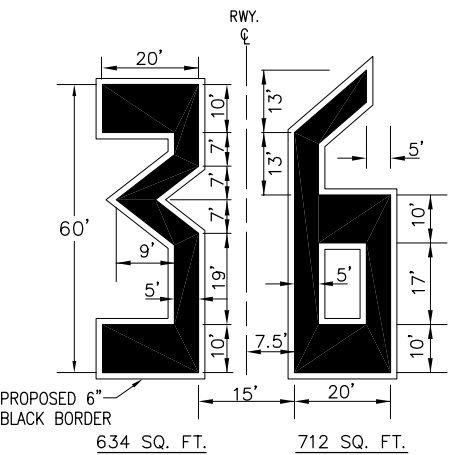
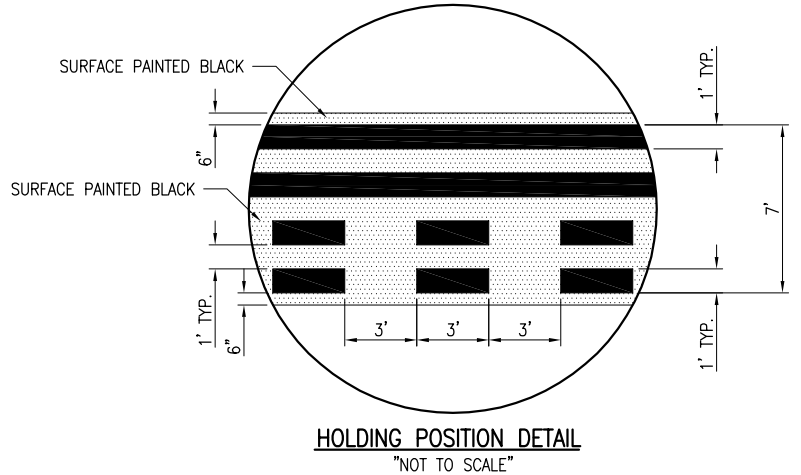
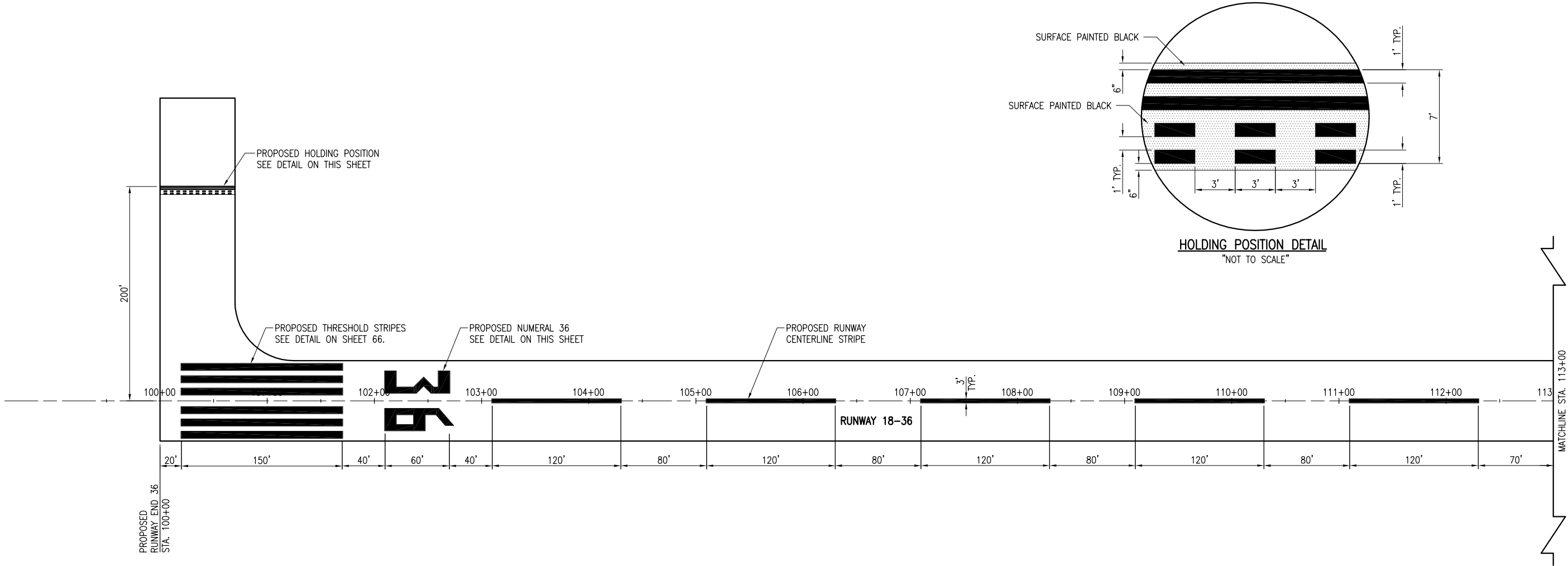


**PLASI ELEVATION**

ALL WORK SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER ITEM AR125989 - REFURBISH PLASI - PER EA.

MAR 11, 2011 11:19 AM RINKED0461 C:\PWISE\_WORK\DO\_NOT\_DELETE\DM18063\R-542ELE.DWG

REVISION	
DATE	
EDGAR COUNTY AIRPORT PARIS, ILLINOIS	
A.I.P. PROJ.: 3-17-0077-B13	
IL PROJ.: PRG-4018	
Hanson Proj. No. 10A0053	
Filename R-542ELE.DWG	
Scale NONE	
Date 03/10/11	
LAYOUT	02/03/11
DRAWN	MLH
REVIEWED	CAH
 © Copyright Hanson Professional Services Inc. 2011 Hanson Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62703-2886 www.hanson-inc.com Offices Nationwide	
CONSTRUCT CROSSWIND RUNWAY 18-36	
RUNWAY 9-27 PLASI WIRING DETAILS AND NOTES	
63	
63 of 137 sheets	



**620-PAVEMENT MARKING-WATERBORNE NOTES**

THE PAVEMENT MARKING-WATERBORNE (620) SHALL BE PLACED IN ACCORDANCE WITH ITEM 620 "PAVEMENT MARKING" AS STATED ON PAGE 277 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOV. 2, 2009.

THIS ITEM SHALL CONSIST OF RUNWAY AND TAXIWAY CENTERLINE, AND MARKING IN ACCORDANCE WITH THESE SPECIFICATIONS AND AT THE LOCATIONS SHOWN ON THE CONSTRUCTION PLANS. ALL TAXIWAY AND TURNAROUND MARKING WILL BE YELLOW IN COLOR WITH A 6-IN BLACK BORDER. THE PROPOSED PAVEMENT MARKING WILL BE APPLIED IN TWO APPLICATIONS

ALL RUNWAY MARKING WILL BE WHITE IN COLOR WITH A 6" BLACK BORDER.

ANY MATERIAL DELIVERED THAT FAILS TO MEET THE SPECIFICATIONS SHALL BE DISPOSED OF BY THE VENDOR AND IMMEDIATELY REPLACED WITH ACCEPTABLE MATERIAL ENTIRELY AT THE VENDOR'S EXPENSE, INCLUDING HANDLING AND TRANSPORTATION CHARGES.

ALL PROPOSED MARKING WILL BE COMPLETED IN ACCORDANCE WITH THE DETAILS SHOWN ON THE CONSTRUCTION PLANS.

GLASS BEADS SHALL BE REQUIRED ONLY ON THE SECOND APPLICATION OF YELLOW MARKING.

CUT-OFF SHEETS WILL BE REQUIRED TO INSURE STRAIGHT EDGES.

ALL PROPOSED PAVEMENT MARKING PAINT SHALL BE WATERBORNE PAINT IN ACCORDANCE WITH ITEM 620.

THE PROPOSED MARKING WILL BE PAID FOR UNDER ITEM:  
 AR620520 PAVEMENT MARKING-WATERBORNE \_\_\_\_ PER S.F.  
 AR620525 PAVEMENT MARKING-BLACK BORDER \_\_\_\_ PER S.F.

REVISION	DATE

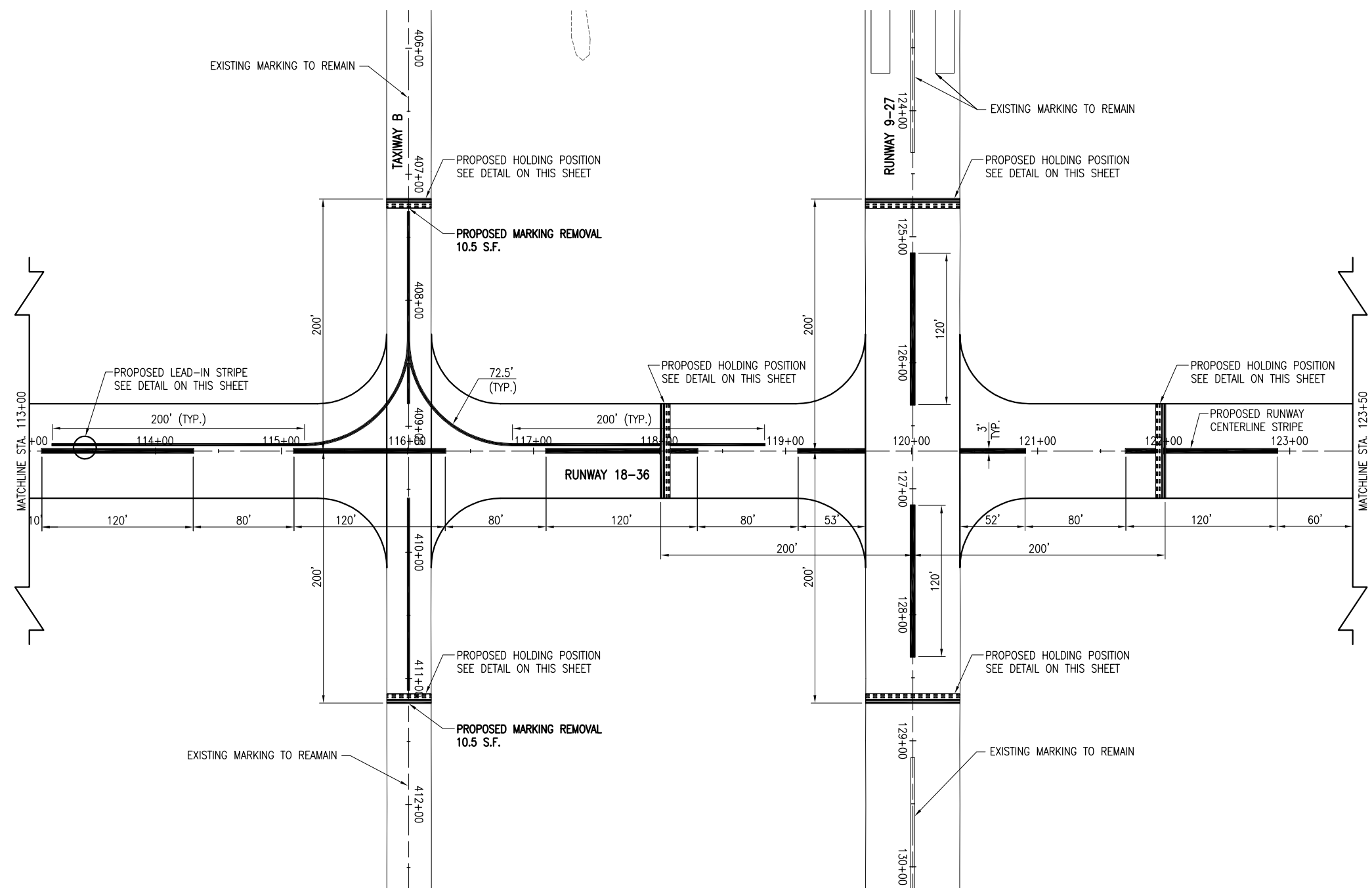
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 PARIS, ILLINOIS  
 ILL. PROJ.: PRG-4018  
 A.I.P. PROJ.: 3-17-0077-B13

Hanson Proj. No. 10A0053	
Filename R-151MRK.DWG	
Scale 1" = 20'	
Date 03/10/11	
LAYOUT	BAK 01/04/11
DRAWN	BAK 01/04/11
REVIEWED	CAH 02/11/11

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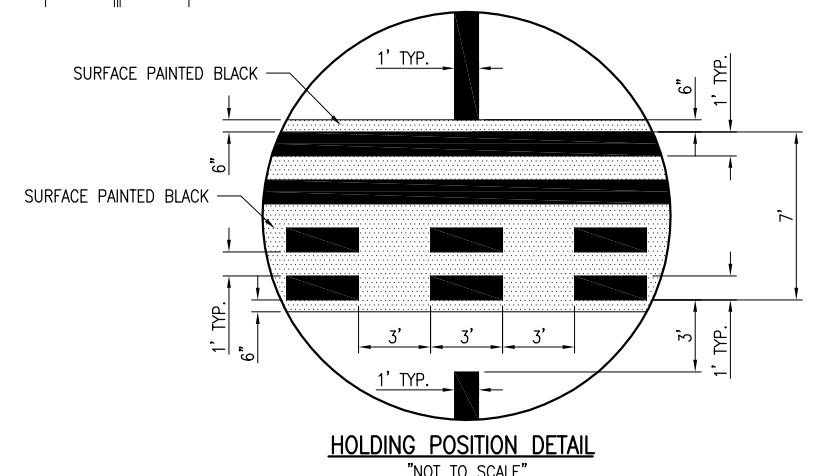
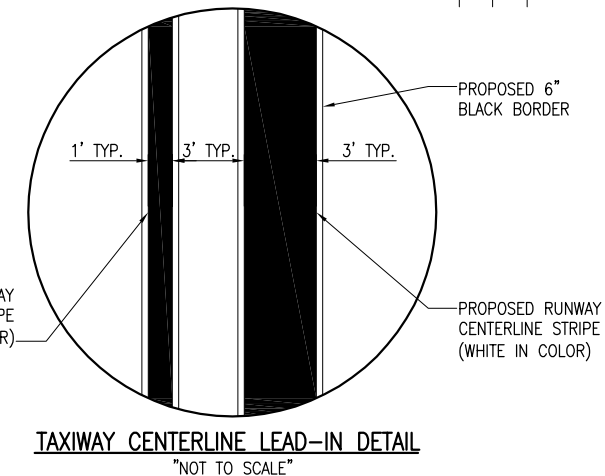
CONSTRUCT CROSSWIND  
 RUNWAY 18-36  
 PROPOSED MARKING PLAN  
 STA. 98+50 TO STA. 113+00

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**MARKING REMOVAL NOTE**  
 THE 21 S.F. OF MARKING REMOVAL WILL BE CONSIDERED INCIDENTAL TO THE PROPOSED MARKING AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

**MARKING NOTE**  
 ANY EXISTING MARKING OBLITERATED OUTSIDE THESE AREAS SHOWN WILL BE REMARKED AT THE CONTRACTOR'S OWN EXPENSE.



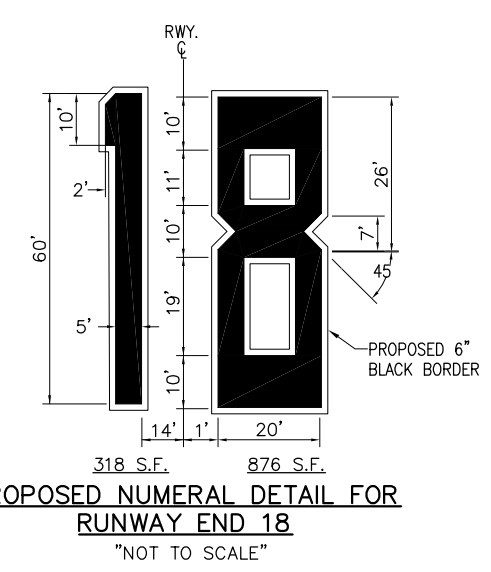
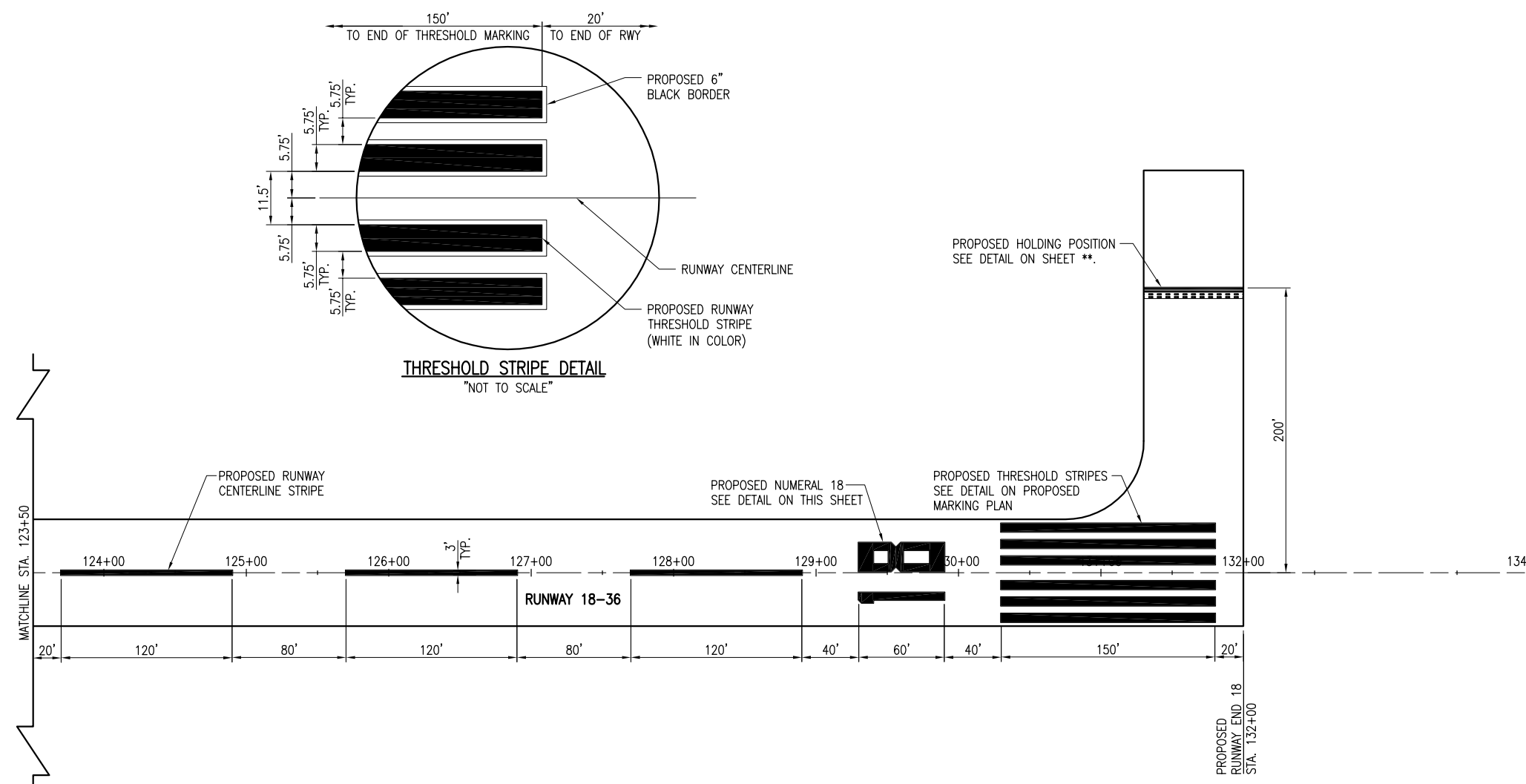
**LEGEND**

- EXISTING PAVEMENT
- PROPOSED PAVEMENT
- EXISTING MARKING
- PROPOSED MARKING

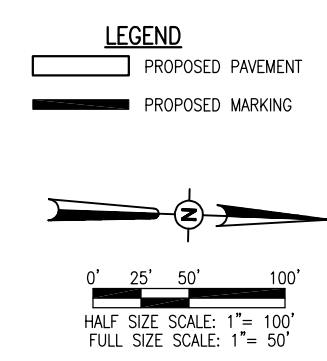
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 HALF SIZE SCALE: 1" = 100'  
 FULL SIZE SCALE: 1" = 50'

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<p>REVISION</p> <p>DATE</p>	<p>A.I.P. PROJ.: 3-17-0077-B13</p> <p>IL PROJ.: PRG-4018</p>									
<p><b>EDGAR COUNTY AIRPORT PARIS, ILLINOIS</b></p>										
<p>Hanson Proj. No. 10A0053                  Filename R=151MRK.DWG                  Scale 1" = 20'                  Date 03/10/11</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>LAYOUT</td> <td>BAK</td> <td>01/04/11</td> </tr> <tr> <td>DRAWN</td> <td>BAK</td> <td>01/04/11</td> </tr> <tr> <td>REVIEWED</td> <td>CAH</td> <td>02/11/11</td> </tr> </table>		LAYOUT	BAK	01/04/11	DRAWN	BAK	01/04/11	REVIEWED	CAH	02/11/11
LAYOUT	BAK	01/04/11								
DRAWN	BAK	01/04/11								
REVIEWED	CAH	02/11/11								
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<p><b>CONSTRUCT CROSSWIND RUNWAY 18-36</b></p> <p>PROPOSED MARKING PLAN                  STA. 113+00 TO STA. 123+50</p>										
<p><b>65</b></p> <p>65 of 137 sheets</p>										



MARKING QUANTITIES			
DESCRIPTION	UNIT AREA	NO. REQUIRED	TOTAL AREA
RUNWAY CENTERLINE STRIPE	360	14.74	5,306
NUMBER 1	318	1	318
NUMBER 3	634	1	634
NUMBER 6	712	1	712
NUMBER 8	876	1	876
THRESHOLD STRIPES	862.50	12	10,350
TOTAL WHITE			18,196
TURNAROUND HOLDING LINE	210	2	420
RUNWAY 18-36 HOLDING LINE	225	2	450
RUNWAY 9-27 HOLDING LINE	225	2	450
TAXIWAY B HOLDING LINE	105	2	210
TAXIWAY CENTERLINE STRIPE	1,008	1	933
TOTAL YELLOW			2,463
RUNWAY CENTERLINE STRIPE	124	14.74	1,828
NUMBER 1	67.25	1	67
NUMBER 3	106	1	106
NUMBER 6	113.37	1	113
NUMBER 8	131.31	1	131
THRESHOLD STRIPES	156.75	12	1,881
TURAROUND HOLDING LINE	350	2	700
RUNWAY 18-36 HOLDING LINE	375	2	750
RUNWAY 9-27 HOLDING LINE	375	2	750
TAXIWAY B HOLDING LINE	177.09	2	354
TAXIWAY CENTERLINE STRIPE	1,008	1	933
TOTAL BLACK			7,613
TOTAL MARKING			28,272



REVISION	DATE

EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

ILL. PROJ.: PRG-4018 A.I.P. PROJ.: 3-17-0077-B13

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CONSTRUCT CROSSWIND  
RUNWAY 18-36

PROPOSED MARKING PLAN  
STA. 123+50 TO STA. 134+00

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Table with 8 columns: STATION, CUT AREA (SF), FILL AREA (SF), CUT VOL (CY), FILL VOL (CY), CUM CUT VOL (CY), CUM FILL VOL (CY), NET VOL (CY). Rows 96+50.00 to 120+50.00.

Table with 8 columns: STATION, CUT AREA (SF), FILL AREA (SF), CUT VOL (CY), FILL VOL (CY), CUM CUT VOL (CY), CUM FILL VOL (CY), NET VOL (CY). Rows 121+00.00 to 143+50.00.

NOTE: CUMULATIVE FILL VOLUMES INCLUDE A 25% SHRINKAGE AND LOSS FACTOR.

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Vertical sidebar containing revision table, project title 'EDGAR COUNTY AIRPORT PARIS, ILLINOIS', company logo 'HANSON Professional Services Inc.', and project description 'CONSTRUCT CROSSWIND RUNWAY 18-36'.

REVISION table with columns for DATE and REVISION.

EDGAR COUNTY AIRPORT PARIS, ILLINOIS  
A.I.P. PROJ.: 3-17-0077-B13  
IL PROJ.: PRG-4018

Table with columns for drawing status: LAYOUT, DRAWN, REVIEWED, BAK, CAH, and dates: 01/11/11, xx/xx/xx.

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CONSTRUCT CROSSWIND RUNWAY 18-36  
N-S RUNWAY EARTHWORK

EARTHWORK SUMMARY – TAXIWAY B							
STATION	CUT AREA (SF)	FILL AREA (SF)	CUT VOL (CY)	FILL VOL (CY)	CUM CUT VOL (CY)	CUM FILL VOL (CY)	NET VOL (CY)
403+50.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
404+00.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
404+50.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
405+00.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
405+50.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
406+00.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
406+50.00	50.03	0.60	46.32	0.69	46.32	0.69	45.63
407+00.00	129.23	0.03	165.98	0.72	212.30	1.41	210.89
407+50.00	244.58	0.00	346.12	0.03	558.42	1.44	556.98
408+00.00	255.35	0.00	462.90	0.00	1021.32	1.44	1019.87
408+50.00	0.00	0.00	236.44	0.00	1257.75	1.44	1256.31
409+00.00	0.00	0.00	0.00	0.00	1257.75	1.44	1256.31
409+50.00	0.00	0.00	0.00	0.00	1257.75	1.44	1256.31
410+00.00	0.00	0.00	0.00	0.00	1257.75	1.44	1256.31
410+50.00	190.79	1.76	176.66	2.04	1434.41	3.49	1430.93
411+00.00	121.28	0.53	288.96	2.65	1723.37	6.14	1717.23
411+50.00	57.81	0.06	165.82	0.68	1889.19	6.82	1882.38
412+00.00	18.67	0.10	70.81	0.19	1960.00	7.00	1953.00
412+50.00	0.00	0.00	17.28	0.12	1977.29	7.12	1970.17
413+00.00	0.00	0.00	0.00	0.00	1977.29	7.12	1970.17
413+50.00	0.00	0.00	0.00	0.00	1977.29	7.12	1970.17
414+00.00	0.00	0.00	0.00	0.00	1977.29	7.12	1970.17

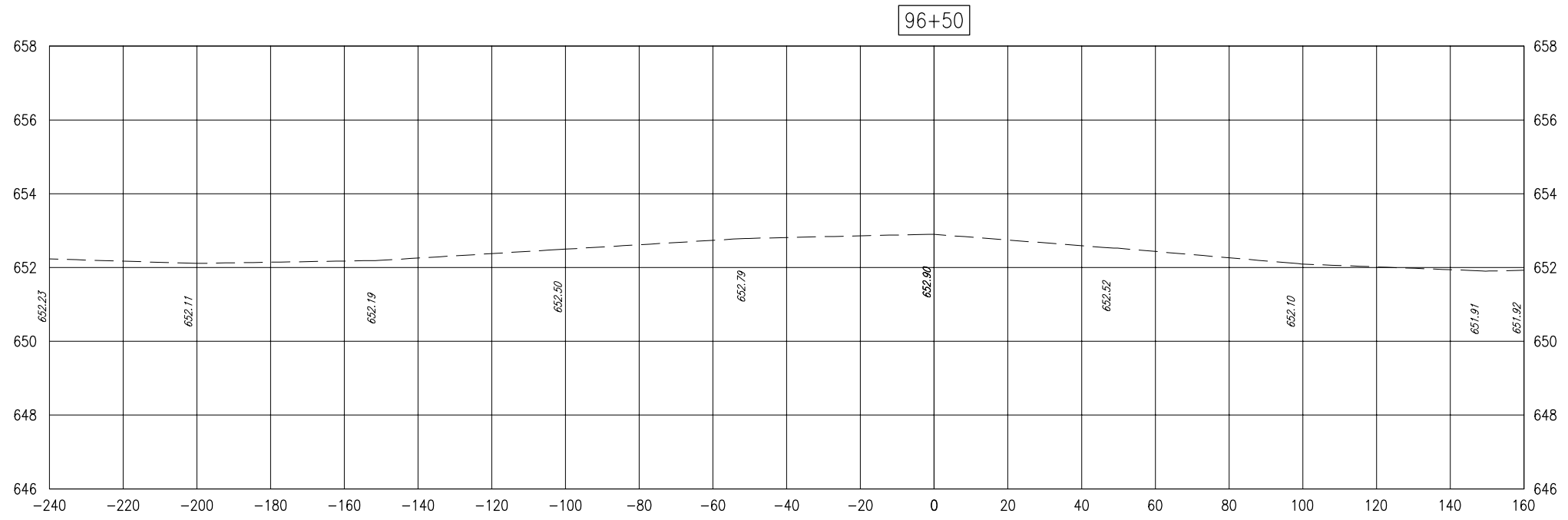
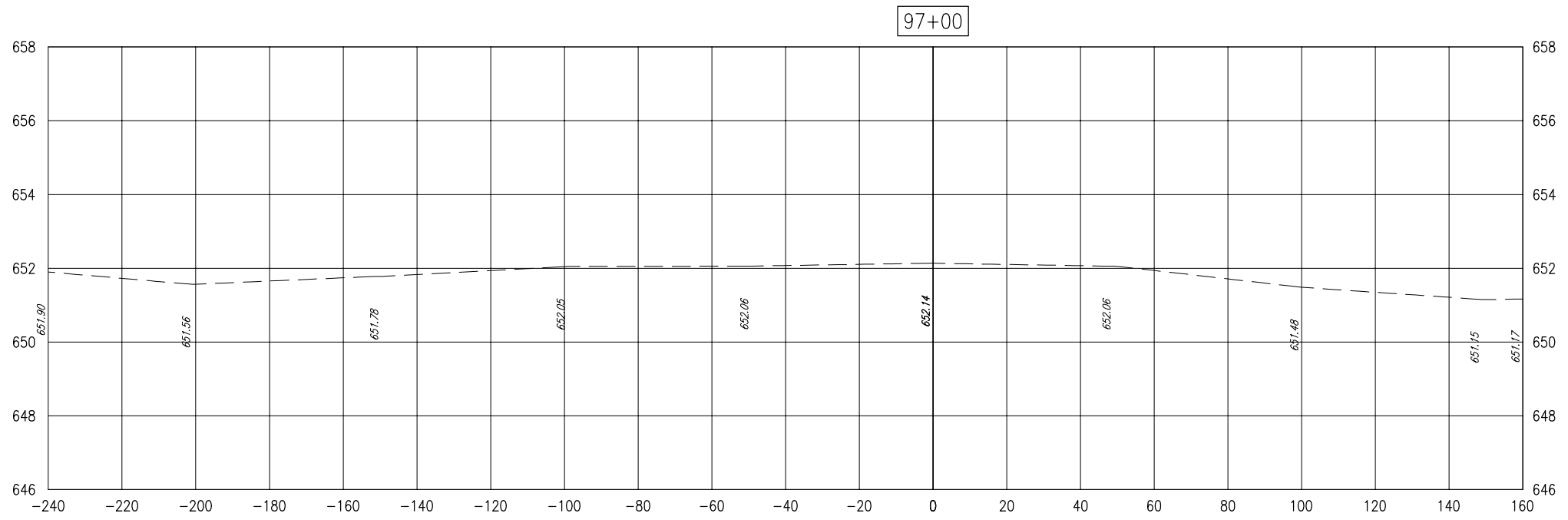
EARTHWORK SUMMARY – SOUTH TURNAROUND							
STATION	CUT AREA (SF)	FILL AREA (SF)	CUT VOL (CY)	FILL VOL (CY)	CUM CUT VOL (CY)	CUM FILL VOL (CY)	NET VOL (CY)
1+25.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
1+50.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
1+75.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
2+00.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
2+17.50	20.94	2.73	6.79	1.11	6.79	1.11	5.68
2+25.00	20.39	5.19	5.74	1.38	12.53	2.48	10.04
2+50.00	27.13	17.42	22.00	13.09	34.52	15.57	18.96
2+75.00	40.62	34.03	31.37	29.78	65.89	45.35	20.54
3+00.00	60.07	59.17	46.62	53.94	112.51	99.28	13.22
3+25.00	85.75	76.65	67.51	78.60	180.01	177.88	2.13
3+50.00	45.45	98.73	60.74	101.49	240.75	279.37	-38.63
3+75.00	21.31	93.76	30.91	111.39	271.65	390.77	-119.11
4+00.00	6.16	95.37	12.72	109.45	284.37	500.22	-215.85
4+25.00	0.01	118.74	2.86	123.91	287.23	624.12	-336.90
4+50.00	0.00	70.70	0.00	109.63	287.23	733.75	-446.52
4+62.50	0.00	57.49	0.00	37.09	287.23	770.84	-483.61

EARTHWORK SUMMARY – NORTH TURNAROUND							
STATION	CUT AREA (SF)	FILL AREA (SF)	CUT VOL (CY)	FILL VOL (CY)	CUM CUT VOL (CY)	CUM FILL VOL (CY)	NET VOL (CY)
10+25.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
10+50.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
10+75.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
11+00.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
11+17.50	0.00	0.00	0.00	0.00	0.00	0.00	0.00
11+25.00	20.69	75.90	2.87	13.18	2.87	13.18	-10.30
11+50.00	22.22	60.36	19.87	78.86	22.74	92.04	-69.30
11+75.00	31.17	35.94	24.72	55.73	47.46	147.77	-100.31
12+00.00	49.08	17.71	37.15	31.04	84.61	178.81	-94.20
12+25.00	89.49	3.76	64.15	12.42	148.76	191.23	-42.46
12+50.00	176.15	0.11	122.98	2.24	271.74	193.47	78.28
12+75.00	249.59	0.00	197.10	0.06	468.84	193.53	275.31
13+00.00	0.00	0.00	115.55	0.00	584.39	193.53	390.86
13+25.00	65.85	0.00	30.48	0.00	614.88	193.53	421.35
13+50.00	115.45	0.00	83.93	0.00	698.81	193.53	505.28
13+62.50	62.77	0.00	41.25	0.00	740.06	193.53	546.53

**NOTE:**  
CUMULATIVE FILL VOLUMES INCLUDE A  
25% SHRINKAGE AND LOSS FACTOR.

REVISION	
DATE	
<b>EDGAR COUNTY AIRPORT PARIS, ILLINOIS</b>	
I.L. PROJ.: PRG-4018      A.I.P. PROJ.: 3-17-0077-B13	
Hanson Proj. No. 10A0053 Filename R-290XS.DWG Scale NOT TO SCALE Date 03/10/11	LAYOUT MDR 01/11/11 DRAWN MDR 01/11/11 REVIEWED CAH xx/xx/xx
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<b>CONSTRUCT CROSSWIND RUNWAY 18-36</b>  <b>TAXIWAY B AND TURNAROUND EARTHWORK</b>	
<span style="font-size: 2em; font-weight: bold;">68</span> 68 of 137 sheets	





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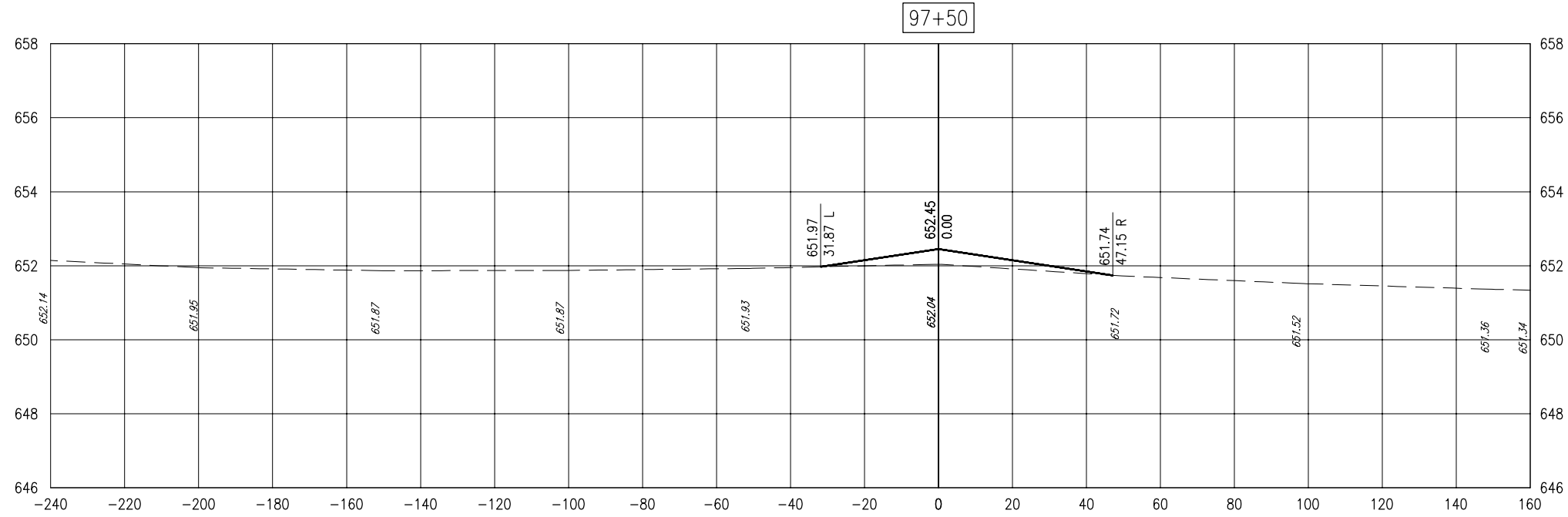
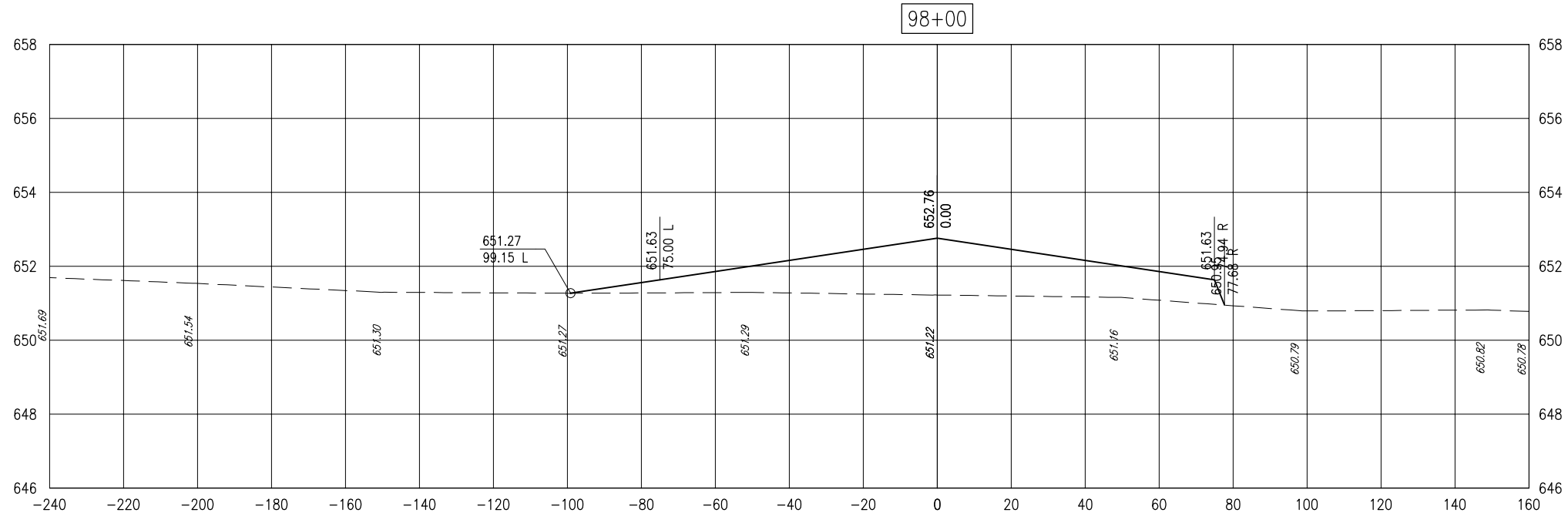
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CONSTRUCT CROSSWIND  
RUNWAY 18-36

CROSS-SECTIONS FOR  
RUNWAY 18-36 STA.  
96+50 TO STA. 97+00



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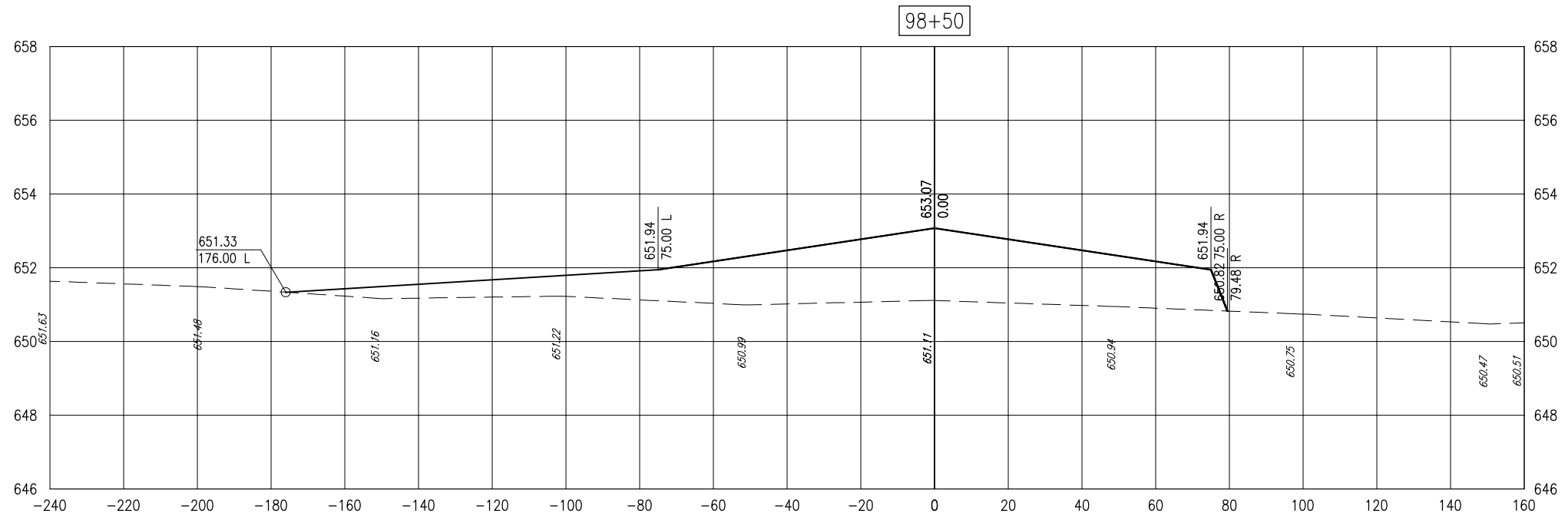
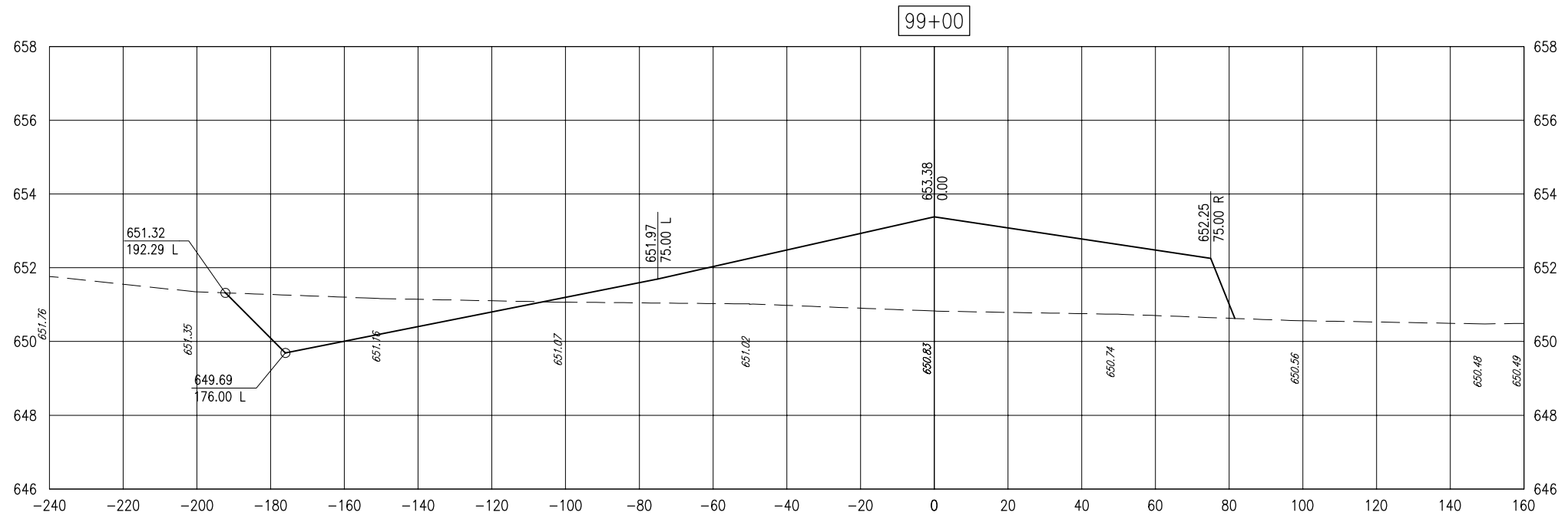
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**CONSTRUCT CROSSWIND  
RUNWAY 18-36**

**CROSS-SECTIONS FOR  
RUNWAY 18-36 STA.  
97+50 TO STA. 98+00**

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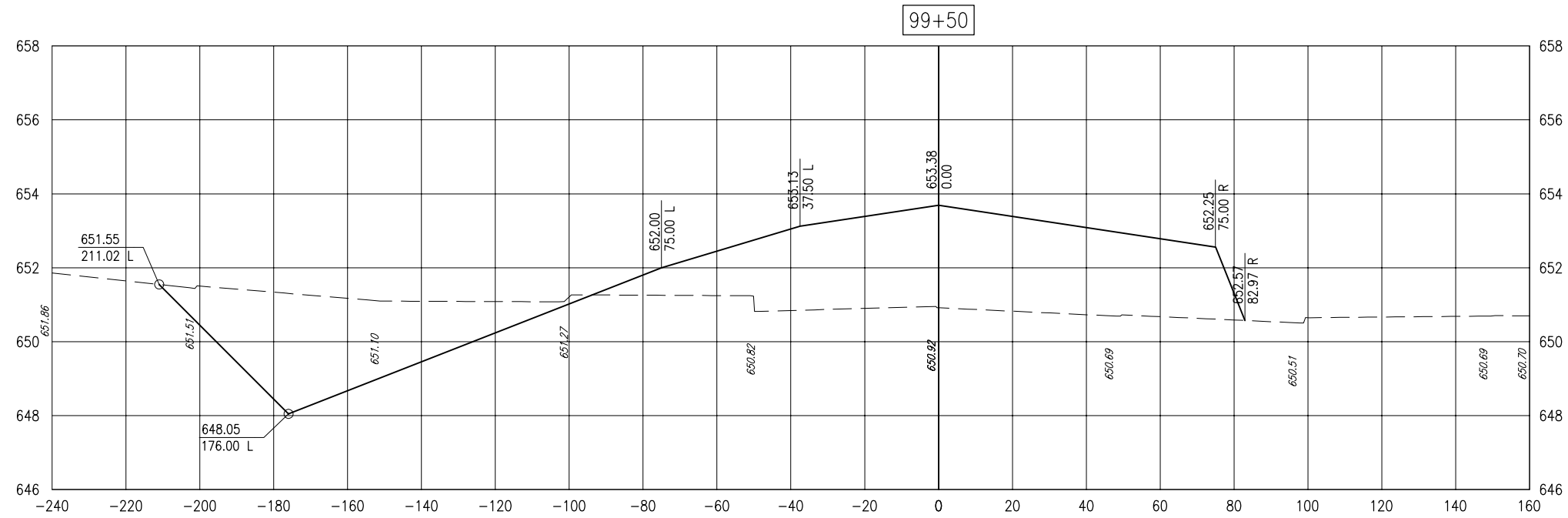
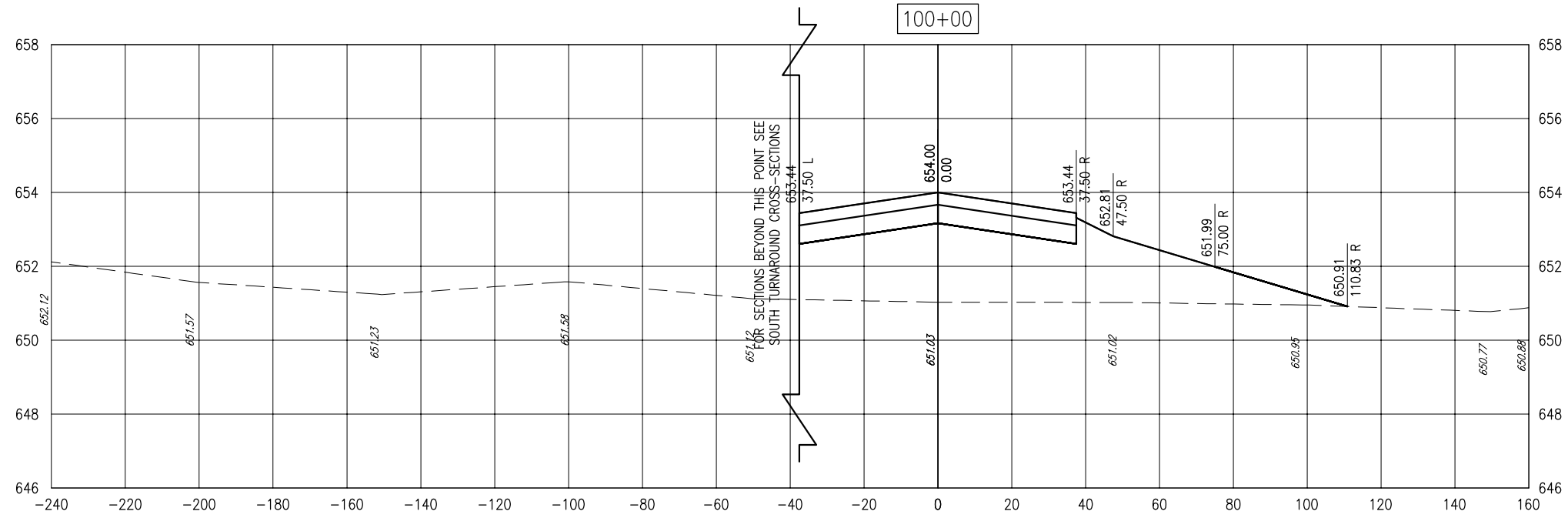
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RUNWAY 18-36

CROSS-SECTIONS FOR  
RUNWAY 18-36 STA.  
98+50 TO STA. 99+00



REVISION	DATE

EDGAR COUNTY AIRPORT  
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IL PROJ.: PRG-4018 A.I.P. PROJ.: 3-17-0077-B13

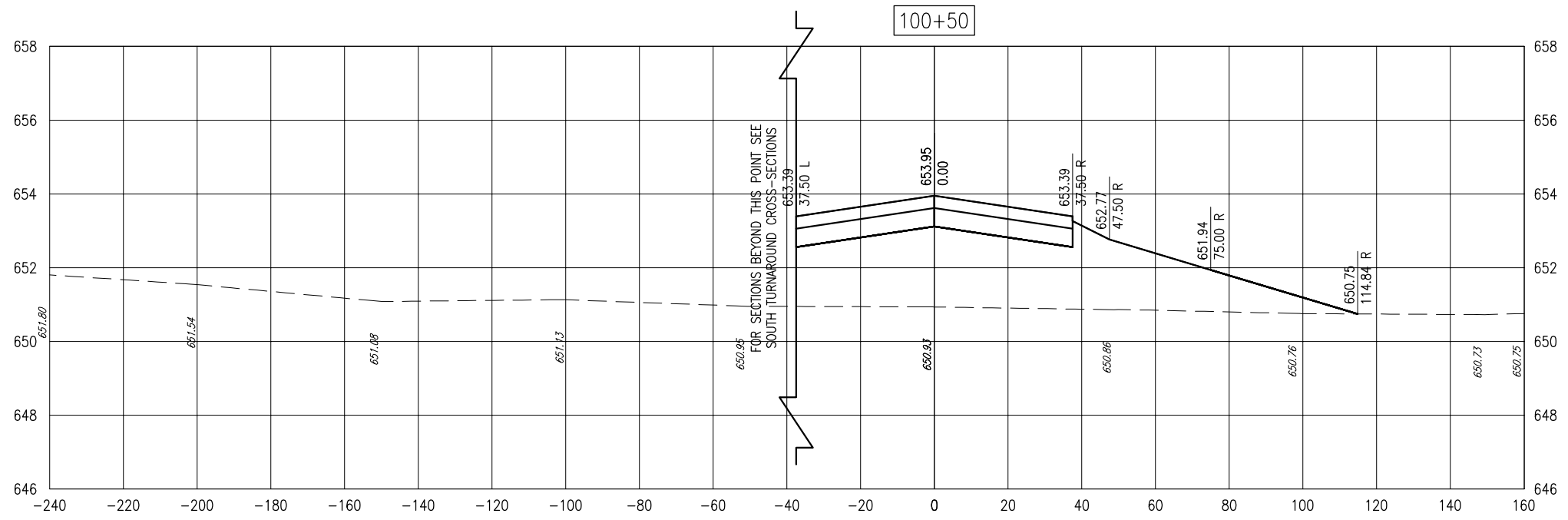
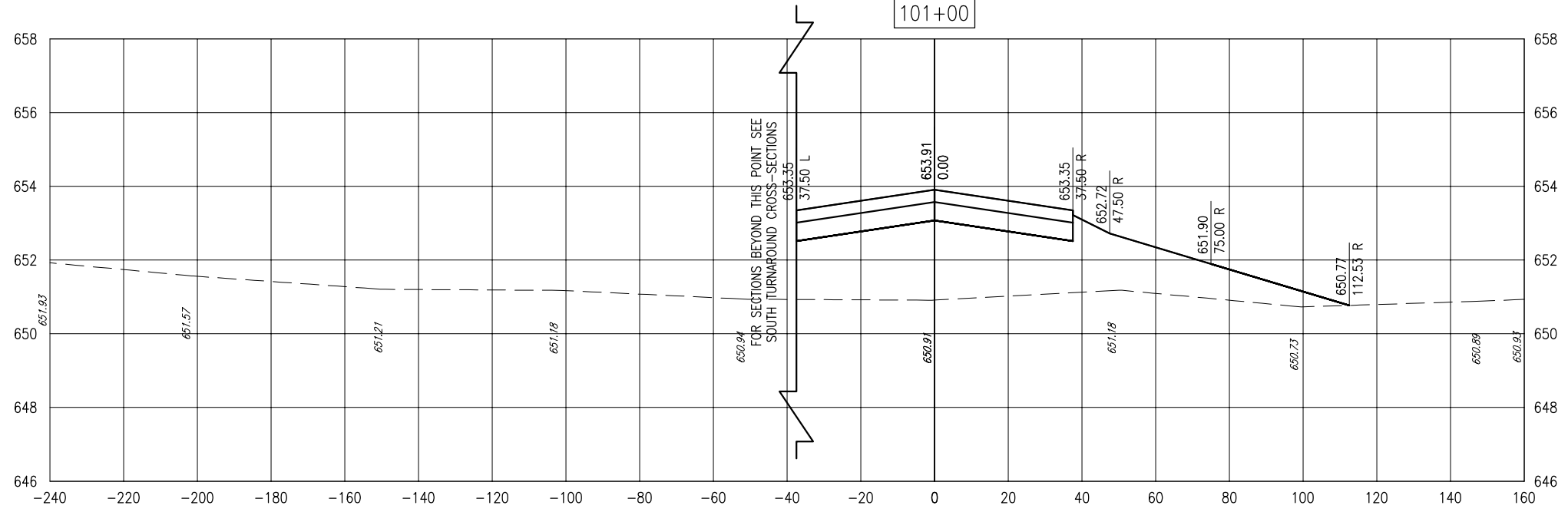
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CONSTRUCT CROSSWIND  
RUNWAY 18-36

CROSS-SECTIONS FOR  
RUNWAY 18-36 STA.  
99+50 TO STA. 100+00



REVISION	DATE

EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

IL PROJ.: PRG-4018 A.I.P. PROJ.: 3-17-0077-B13

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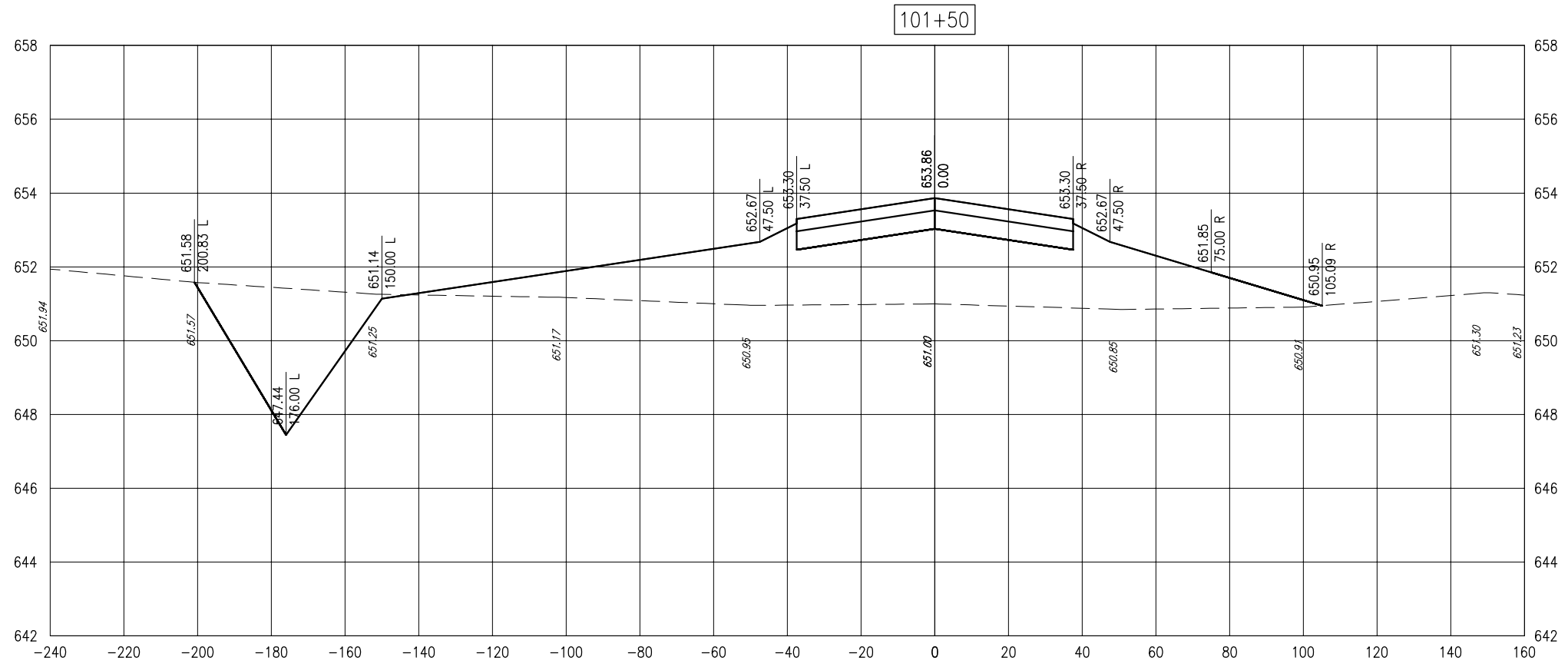
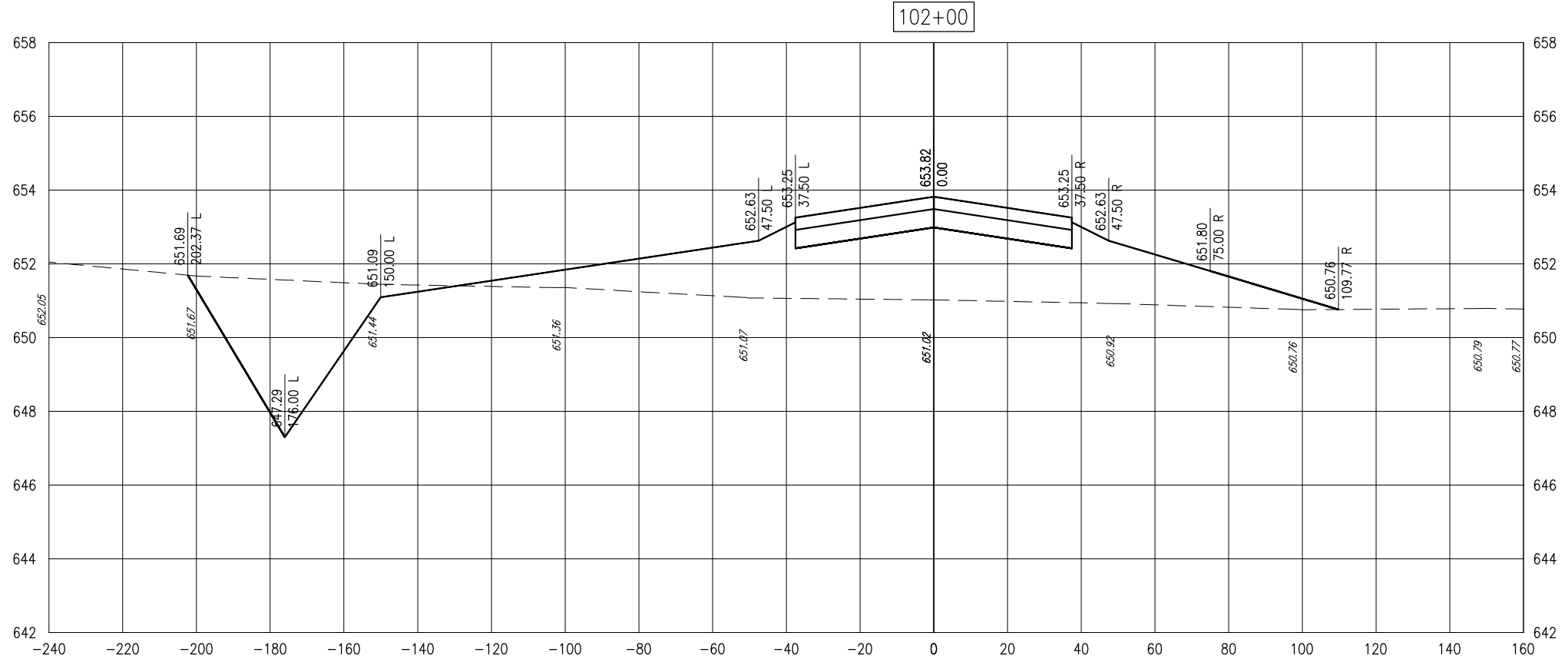
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CONSTRUCT CROSSWIND  
RUNWAY 18-36

CROSS-SECTIONS FOR  
RUNWAY 18-36 STA.  
100+50 TO STA. 101+00

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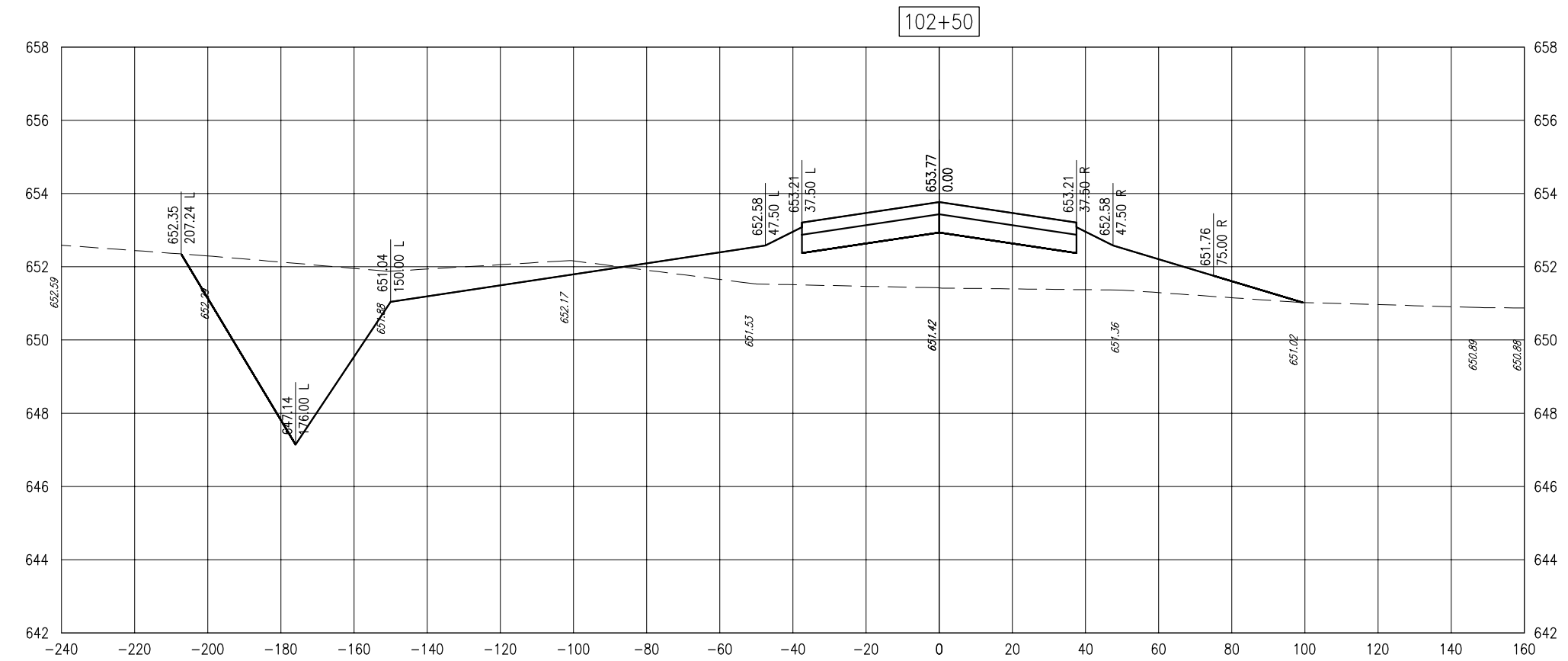
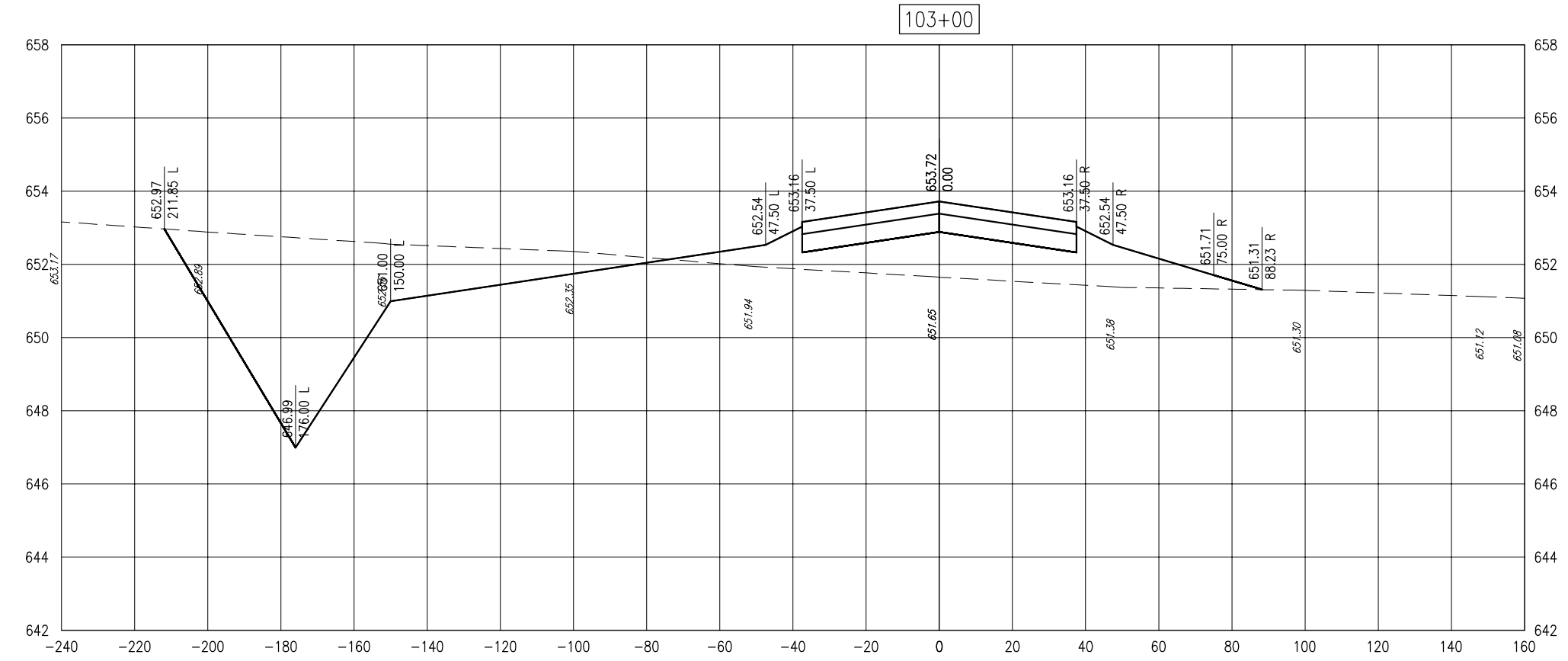
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CROSS-SECTIONS FOR  
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101+50 TO STA. 102+00

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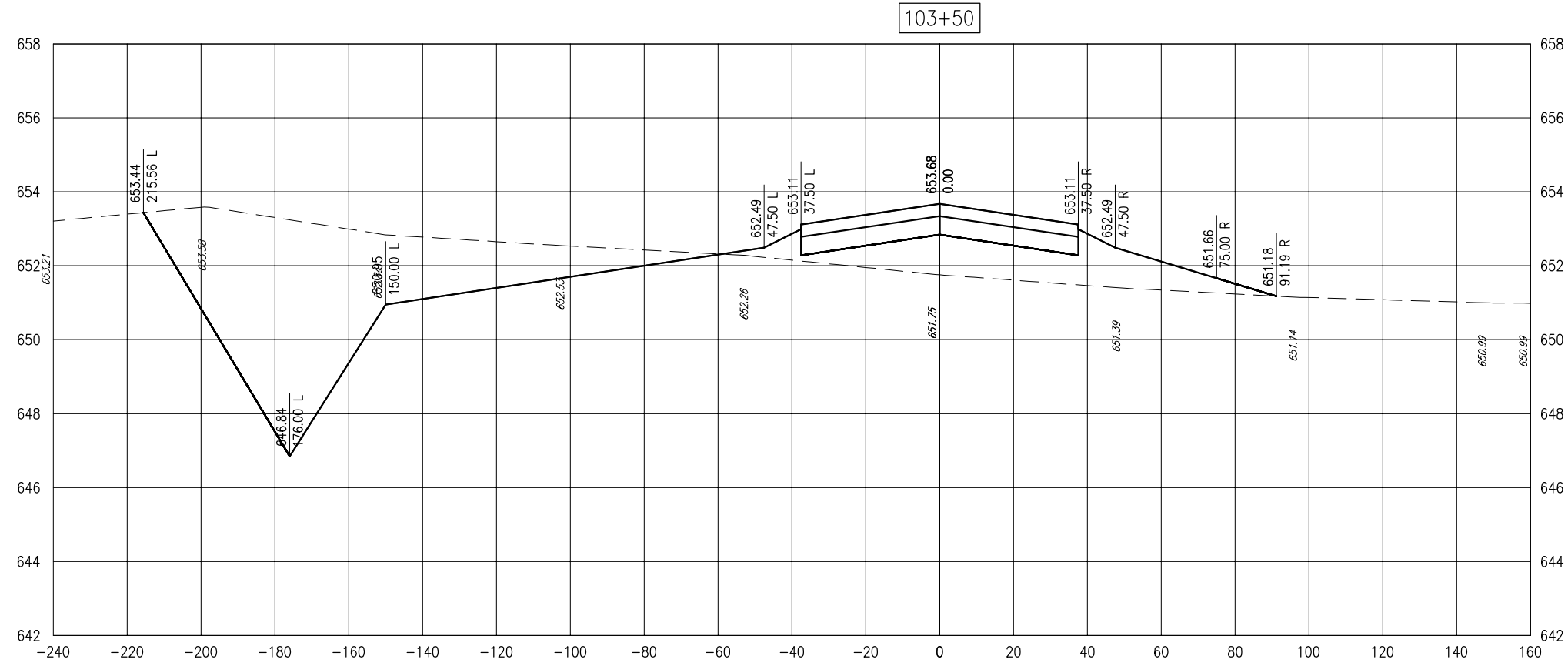
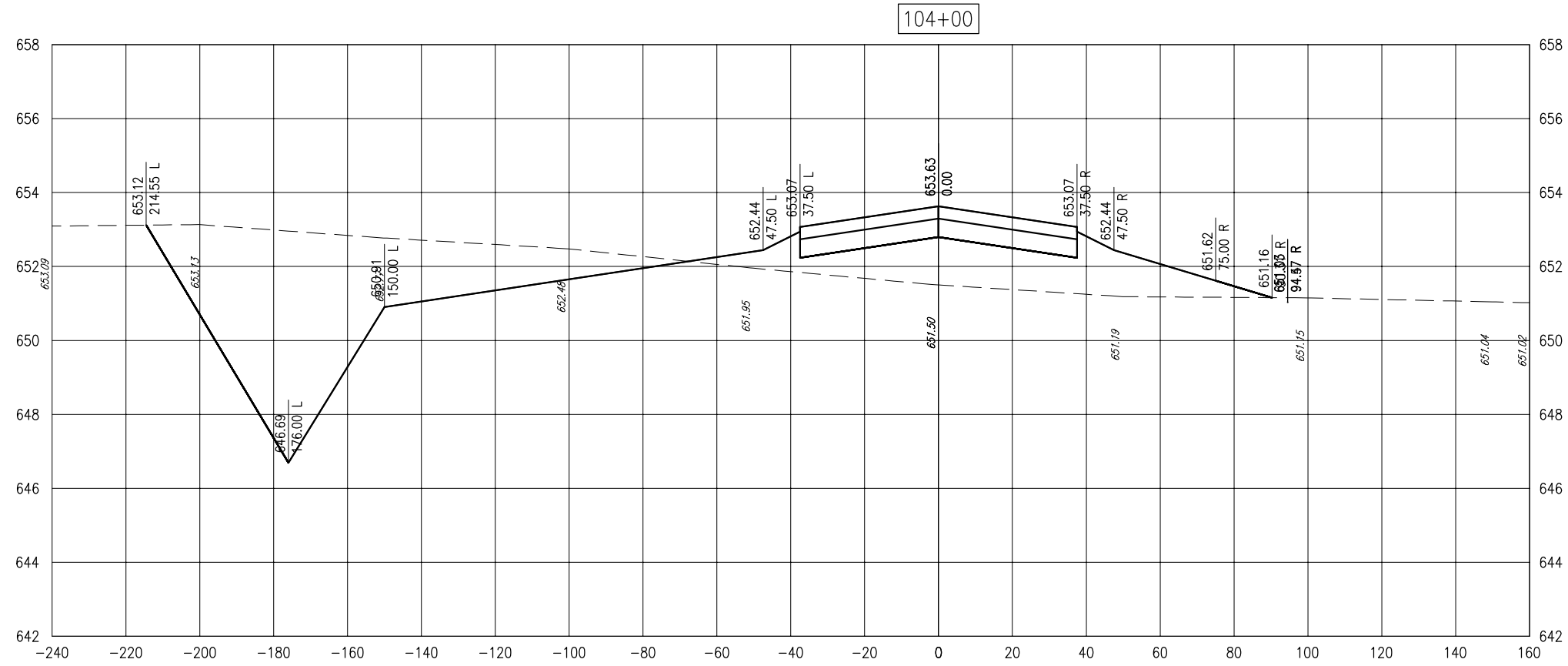
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RUNWAY 18-36 STA.  
102+50 TO STA. 103+00

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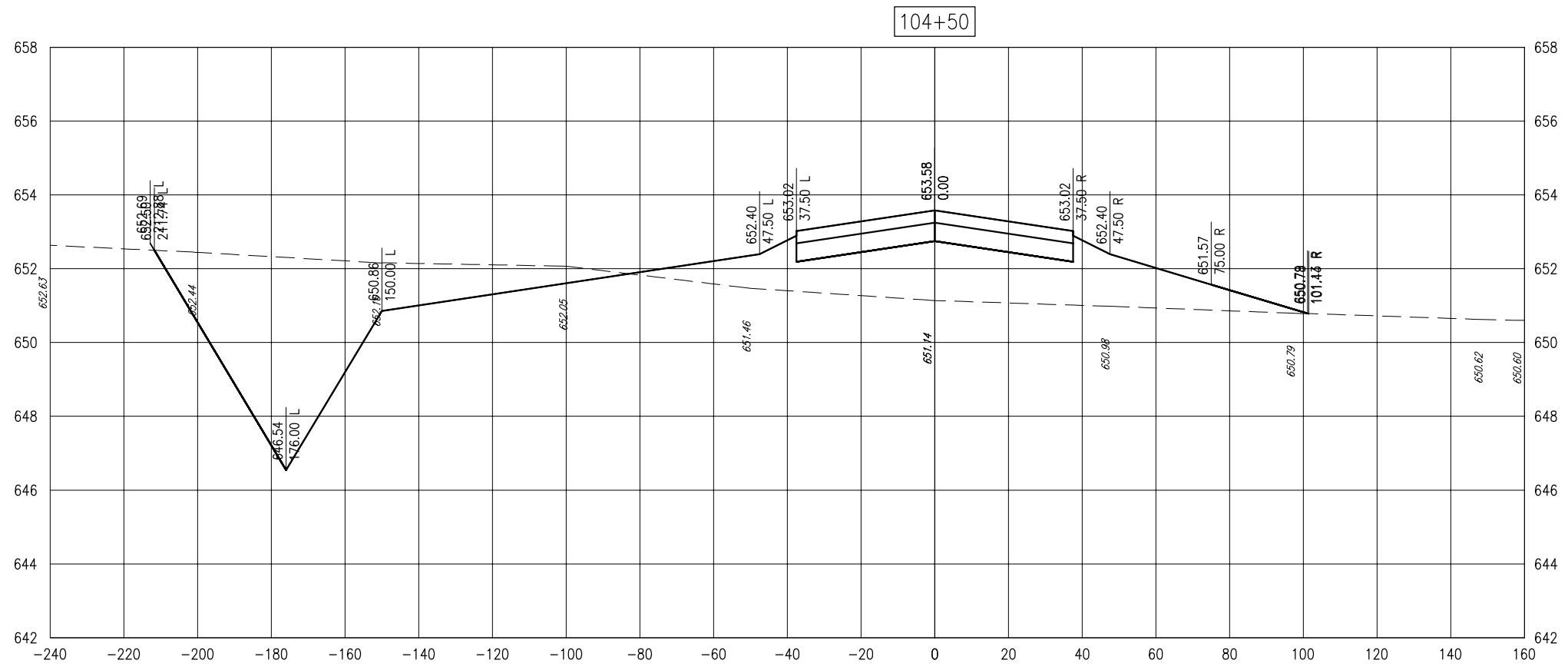
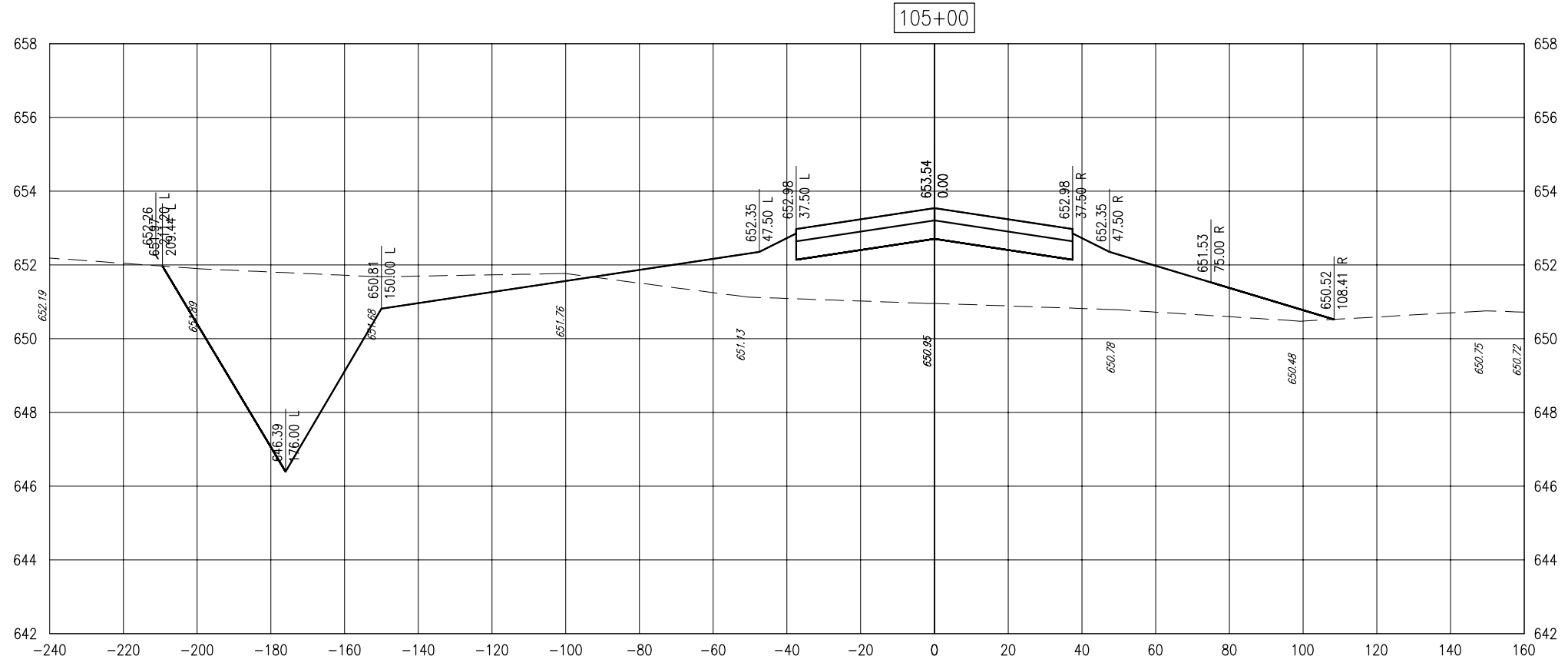
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103+50 TO STA. 104+00



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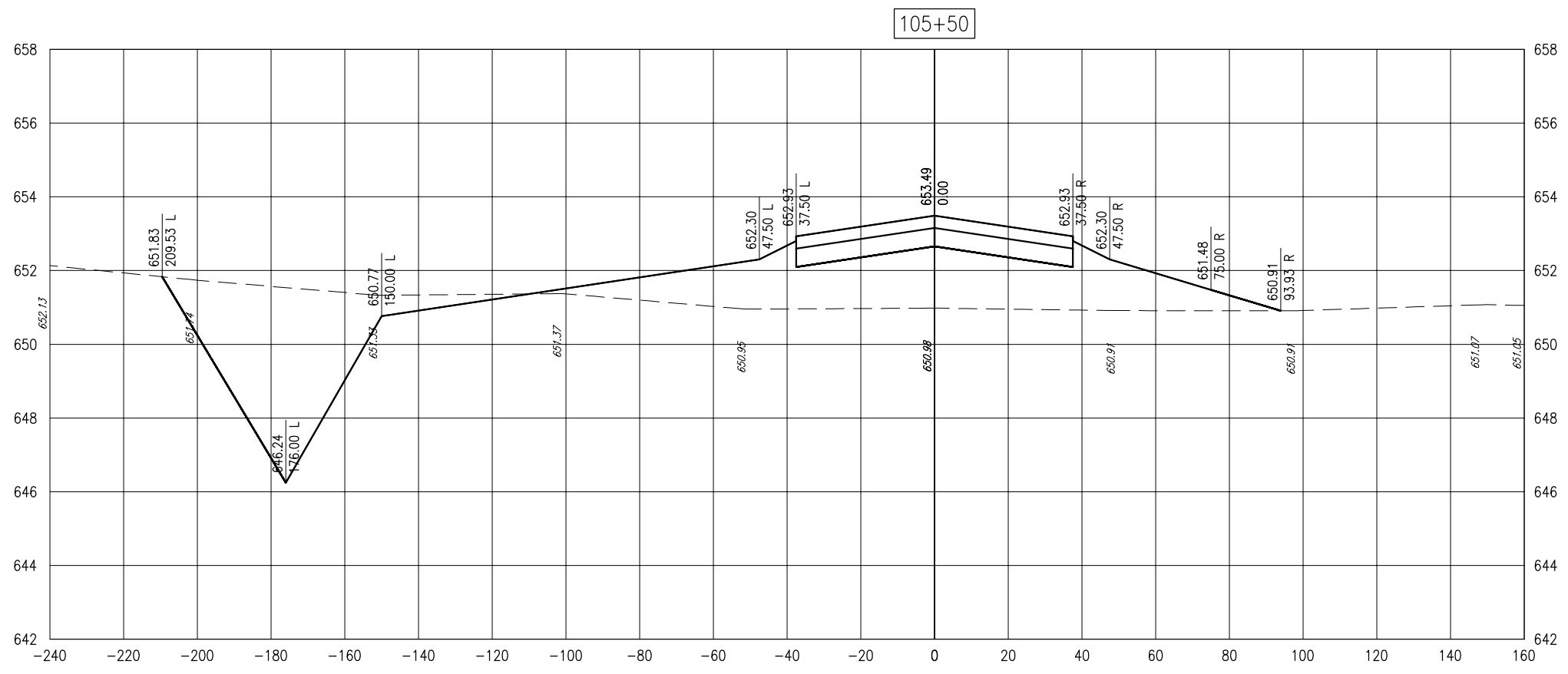
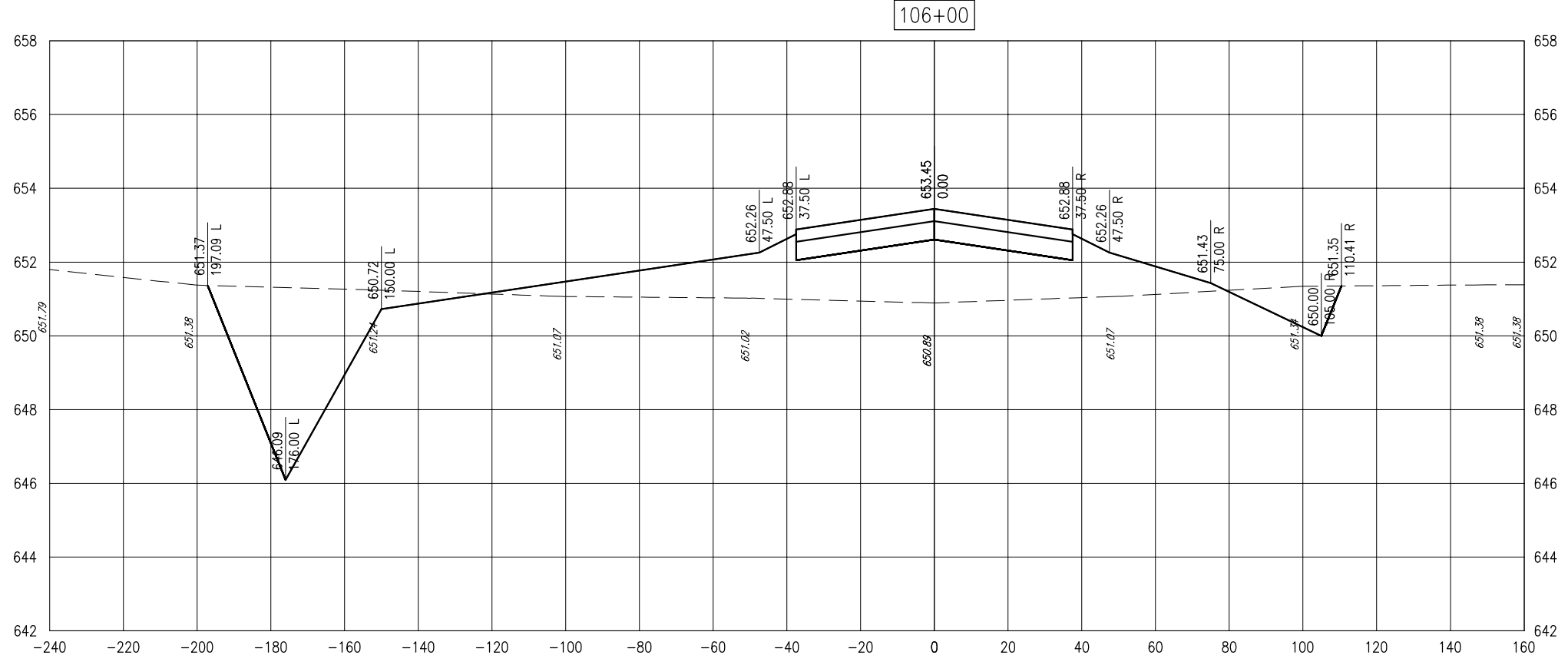
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104+50 TO STA. 105+00

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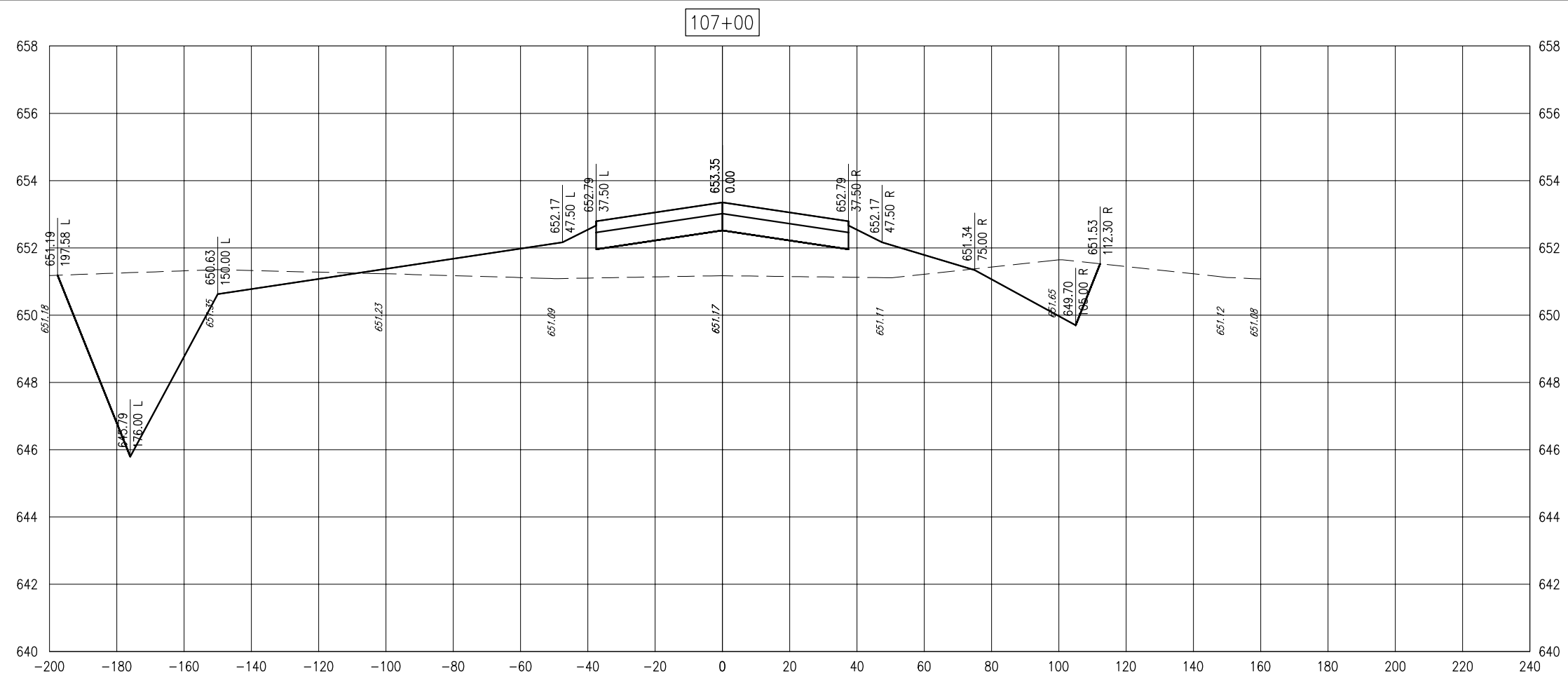
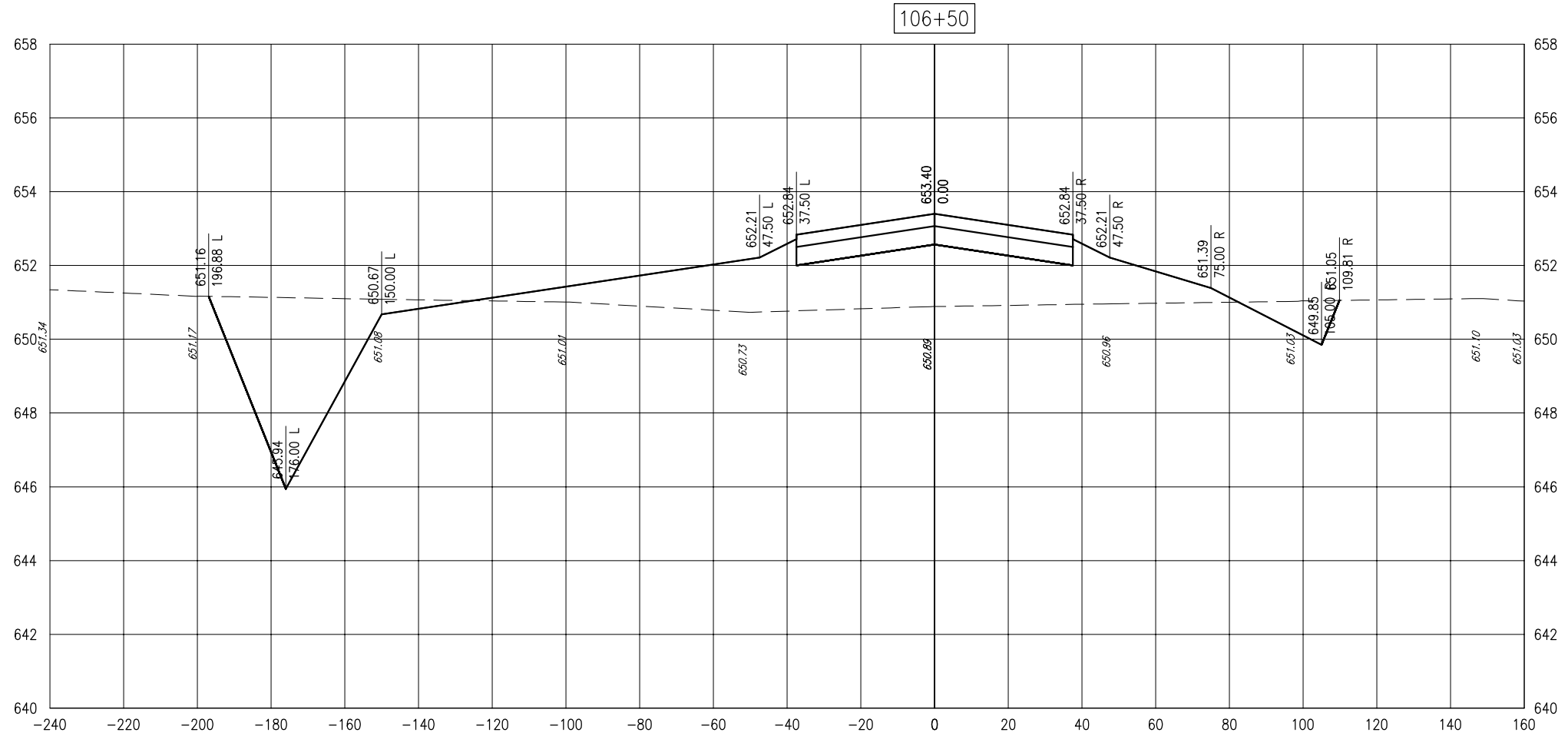
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105+50 TO STA. 106+00

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RUNWAY 18-36**

CROSS-SECTIONS FOR  
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106+50 TO STA. 107+00

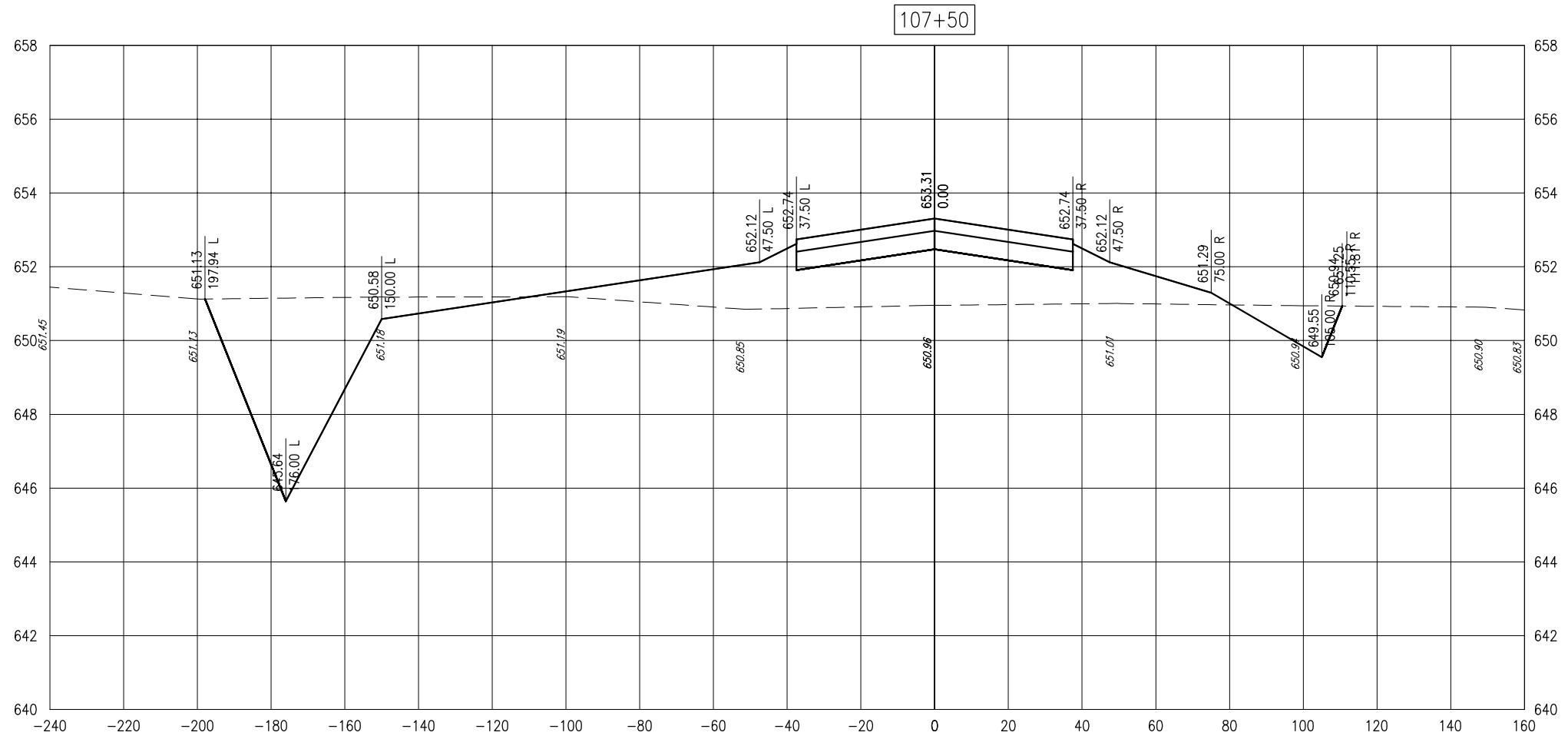
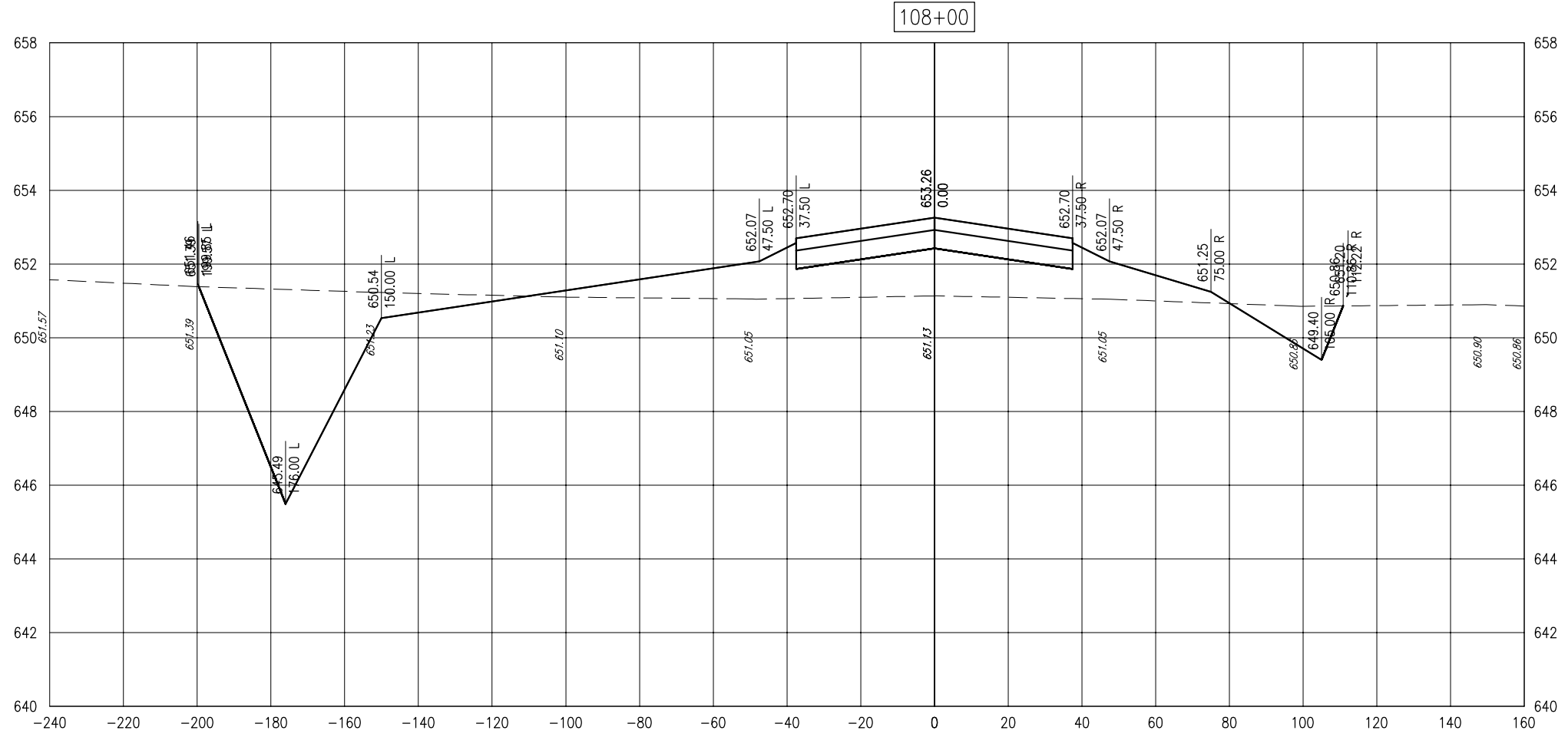
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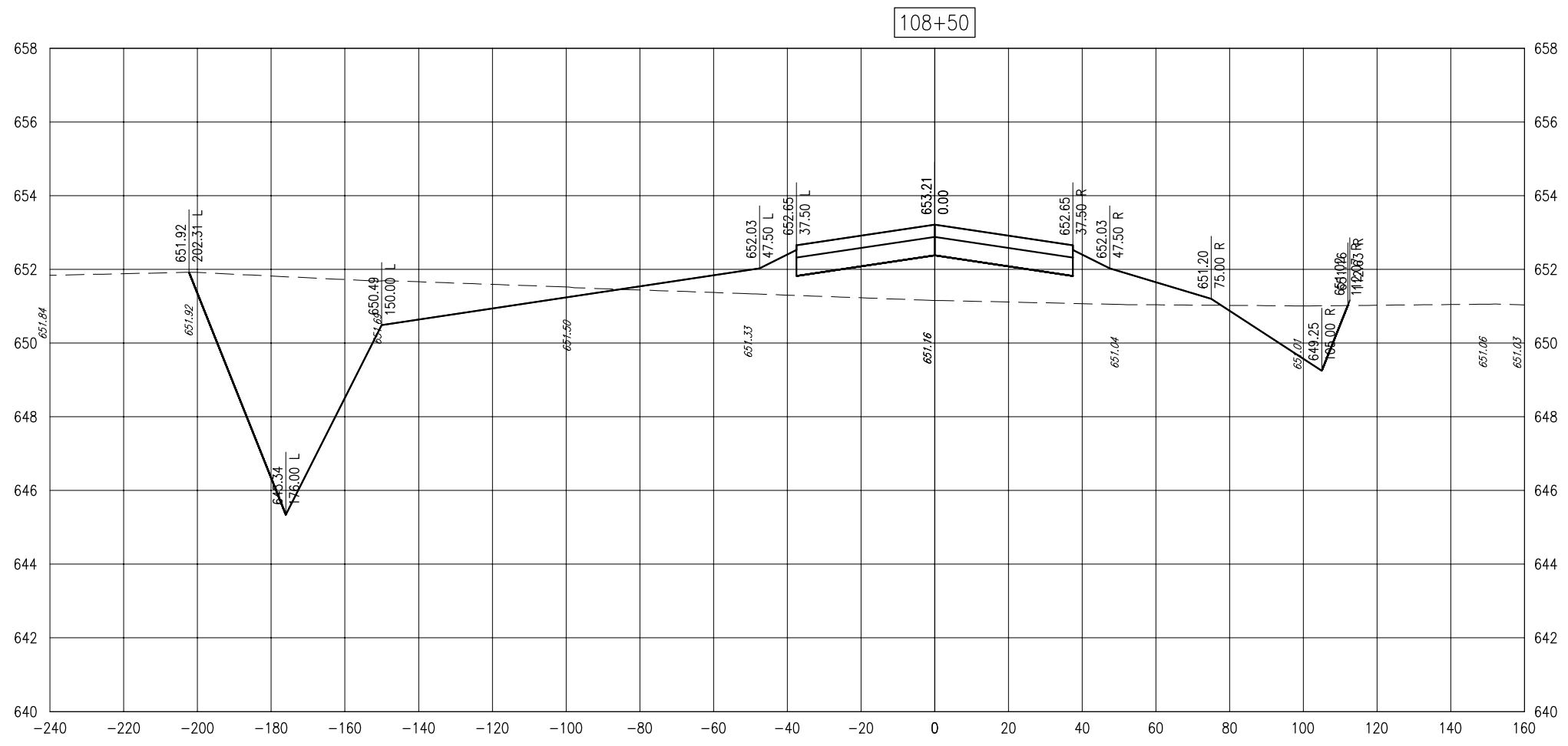
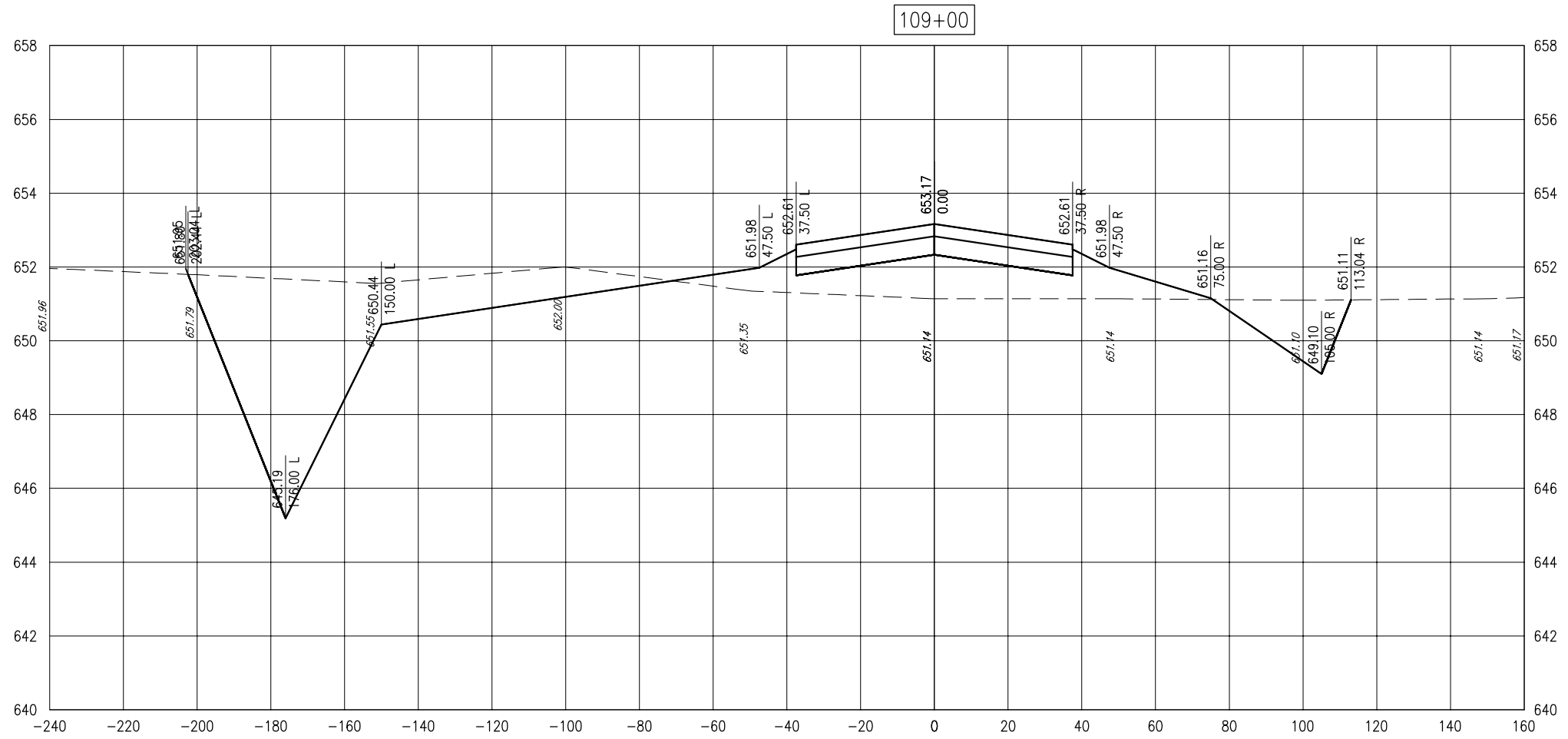
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RUNWAY 18-36 STA.  
107+50 TO STA. 108+00



DATE	REVISION

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PARIS, ILLINOIS

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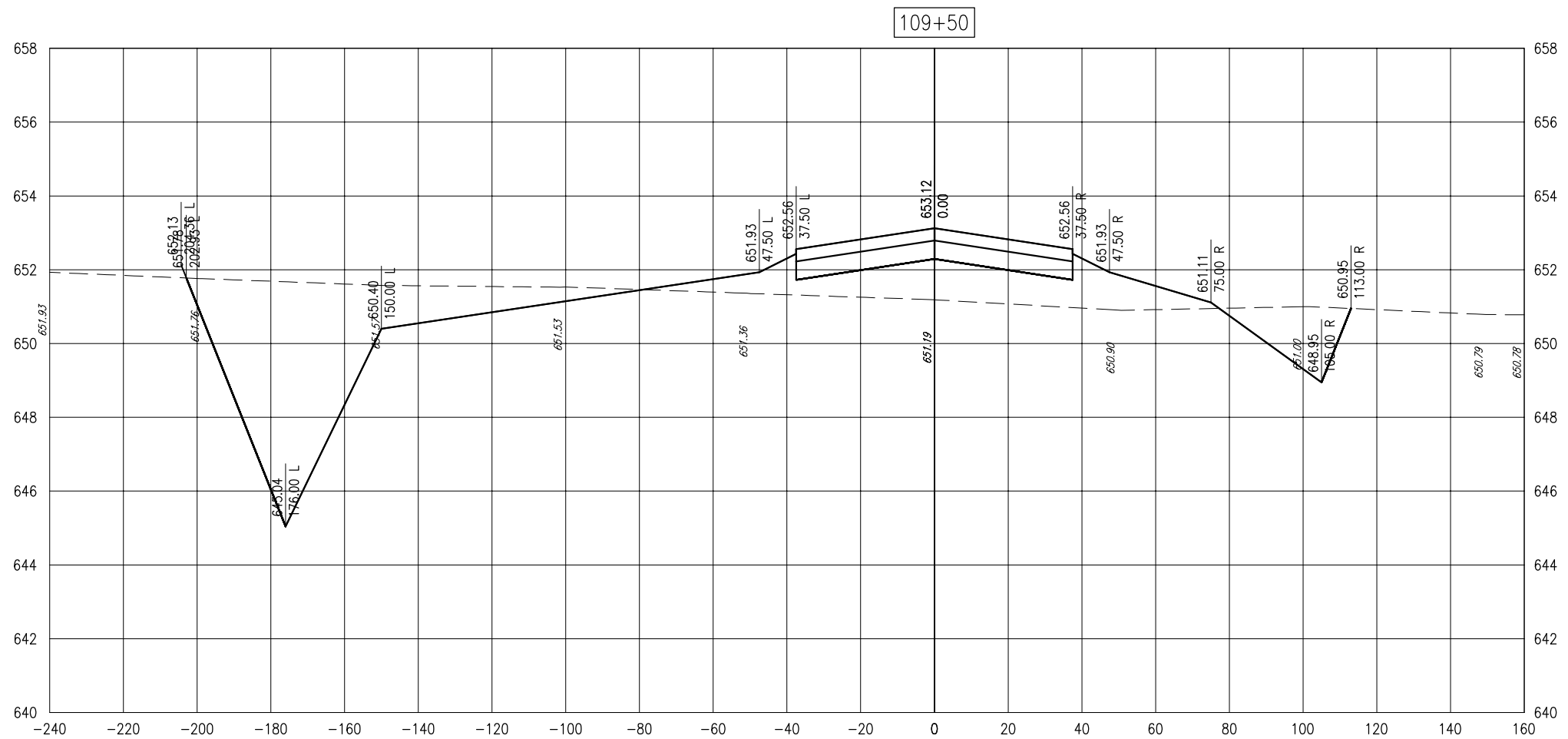
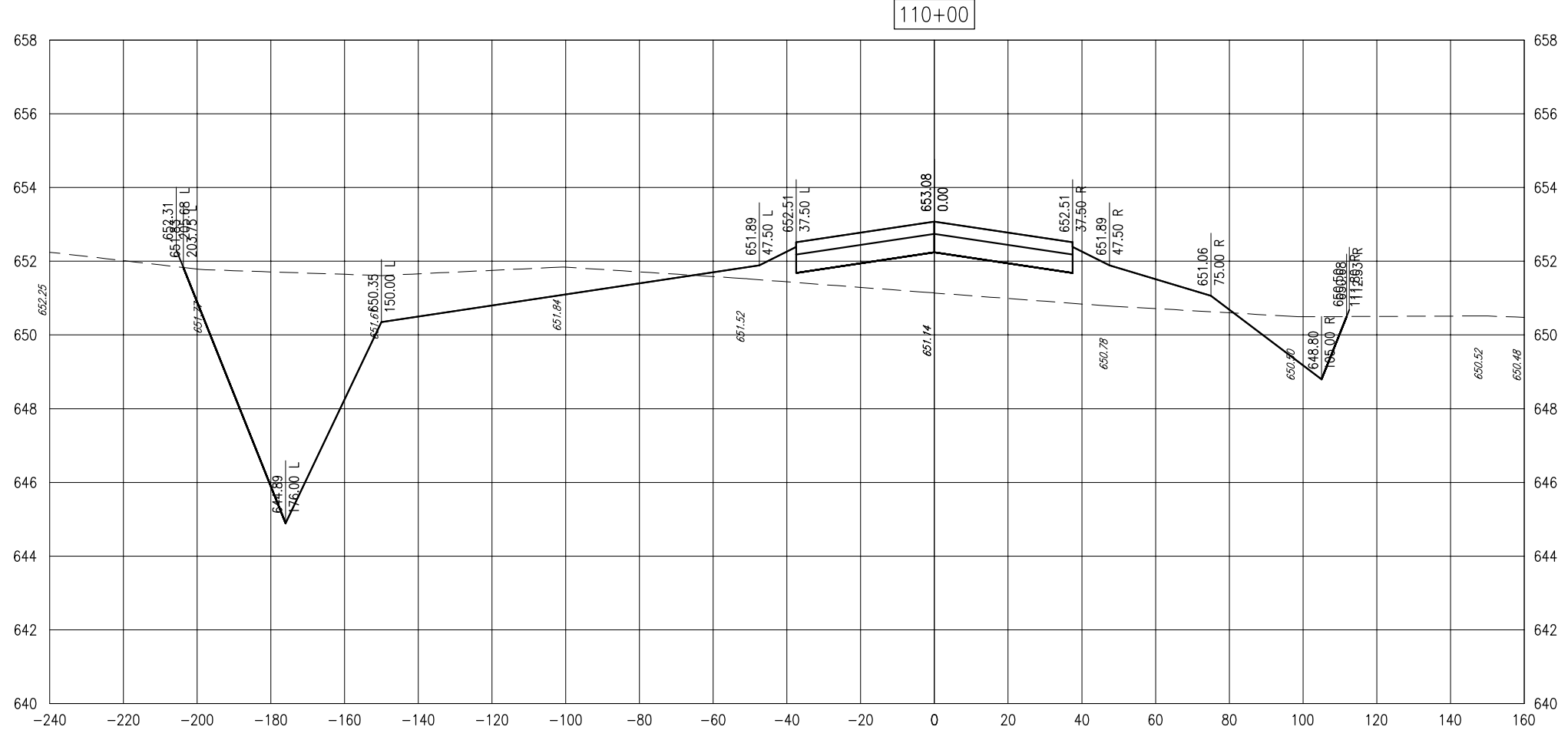
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CROSS-SECTIONS FOR  
RUNWAY 18-36 STA.  
108+50 TO STA. 109+00



DATE	REVISION

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PARIS, ILLINOIS

IL PROJ.: PRG-4018 A.I.P. PROJ.: 3-17-0077-B13

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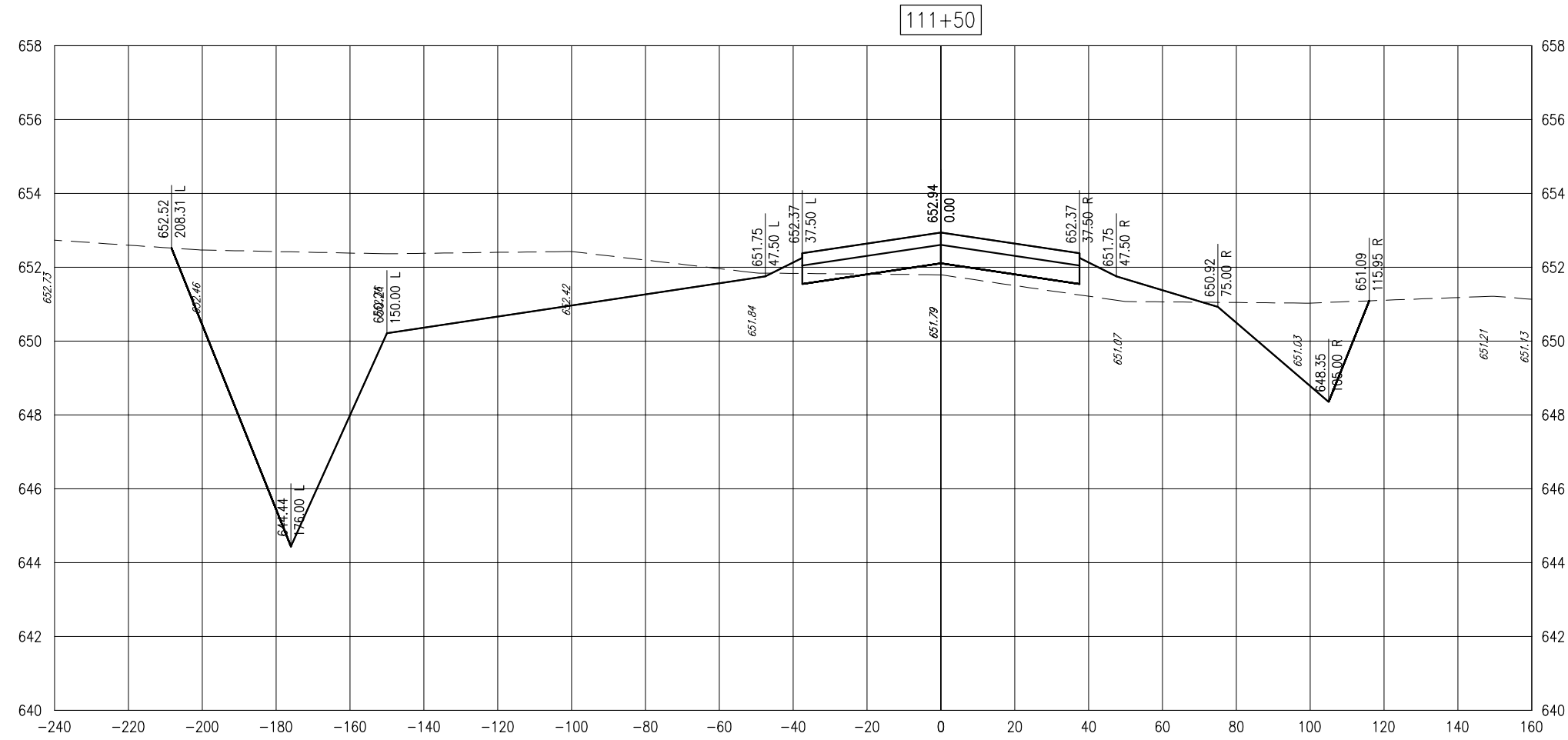
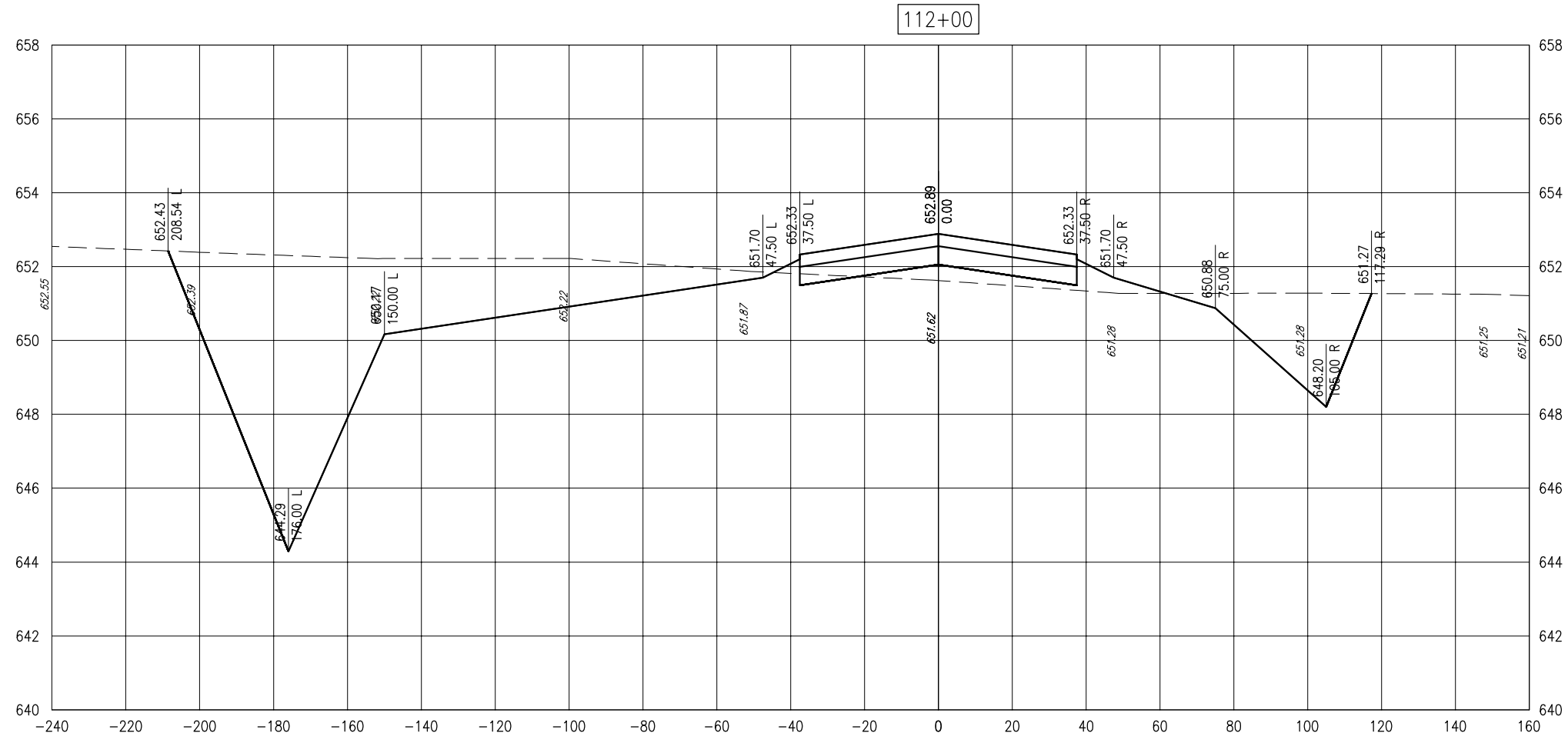
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RUNWAY 18-36

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RUNWAY 18-36 STA.  
109+50 TO STA. 110+00

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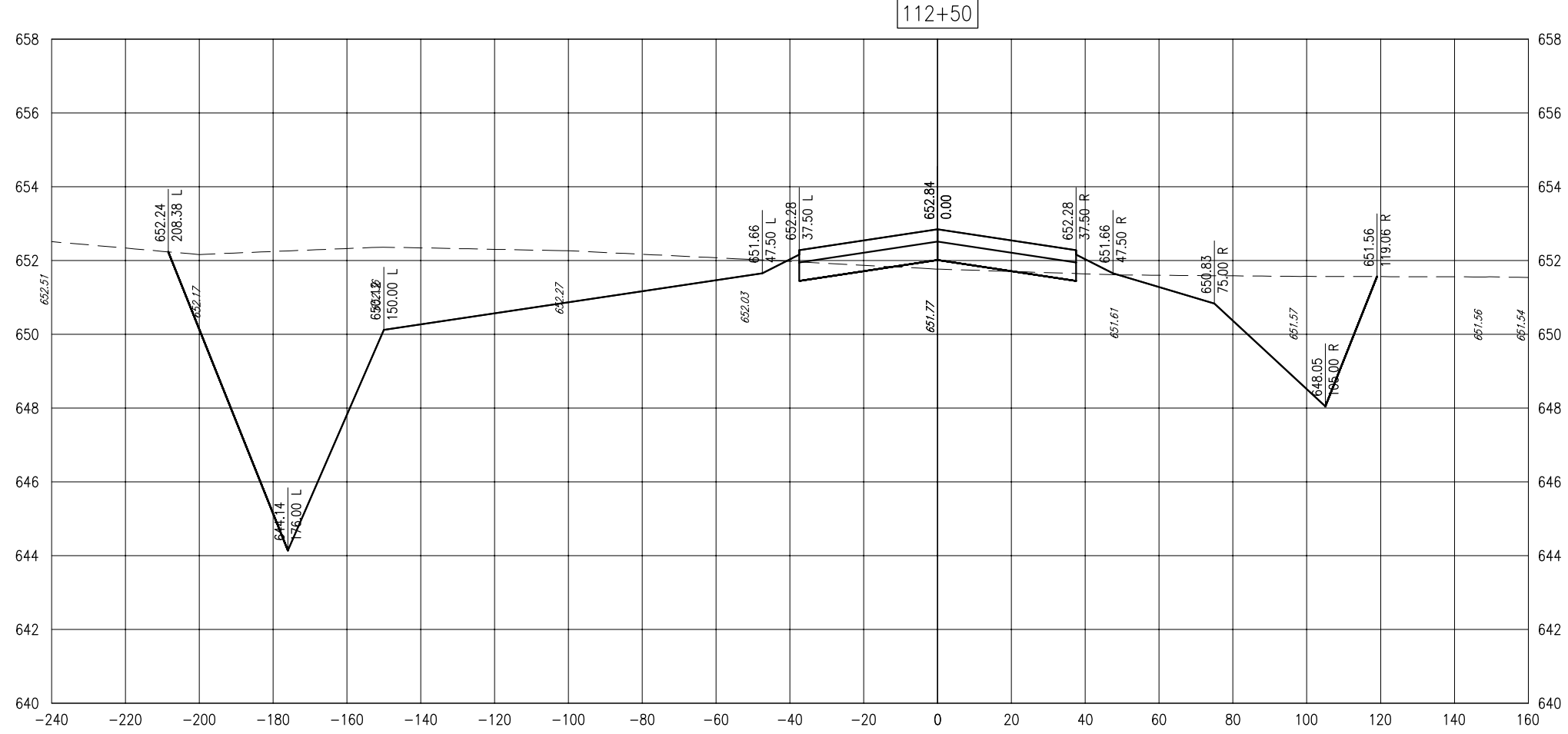
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REVIEWED	CAH	xx/xx/xx	

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RUNWAY 18-36

CROSS-SECTIONS FOR  
RUNWAY 18-36 STA.  
111+50 TO STA. 112+00



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PARIS, ILLINOIS

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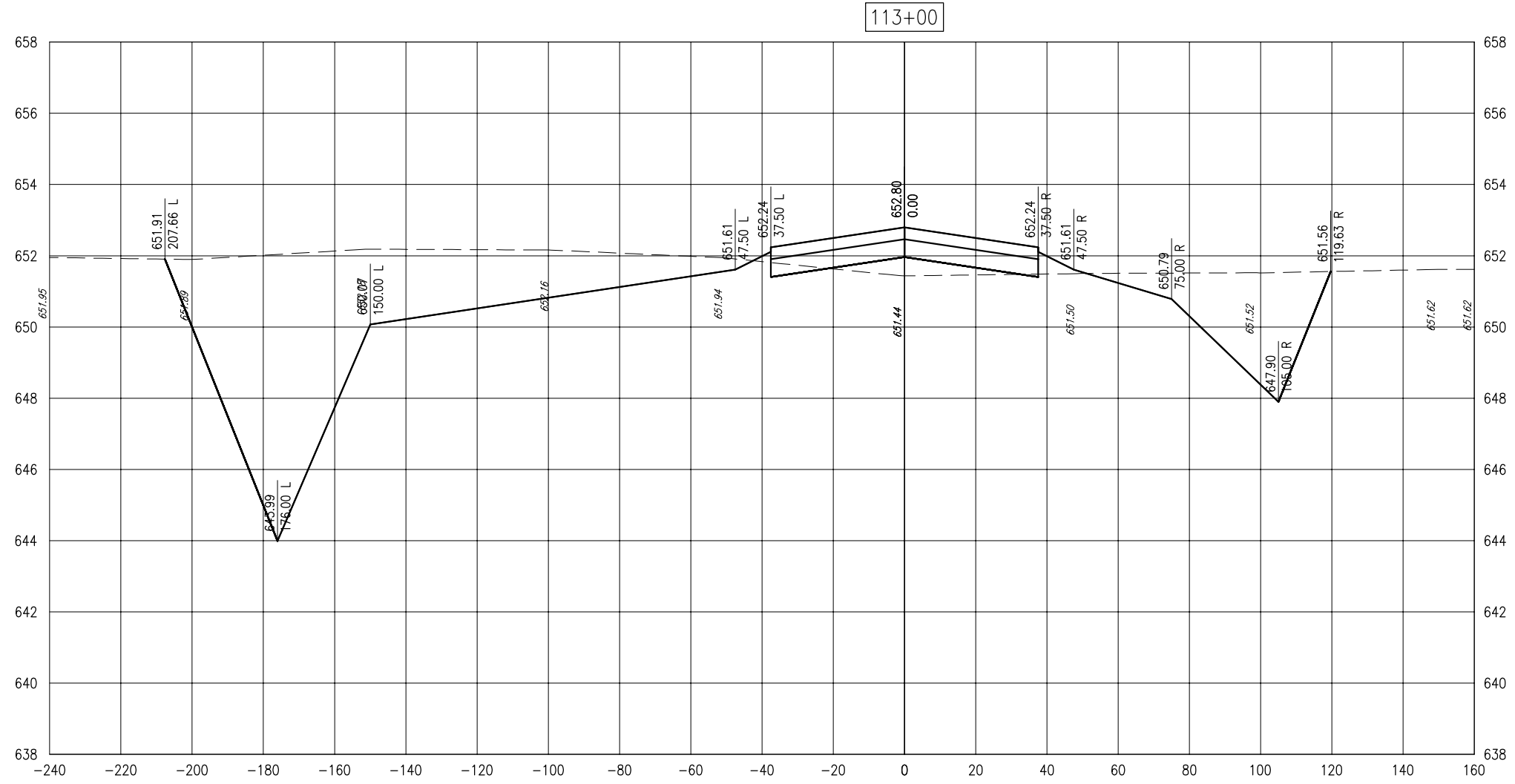
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RUNWAY 18-36

CROSS-SECTIONS  
FOR RUNWAY 18-36  
STA. 112+50





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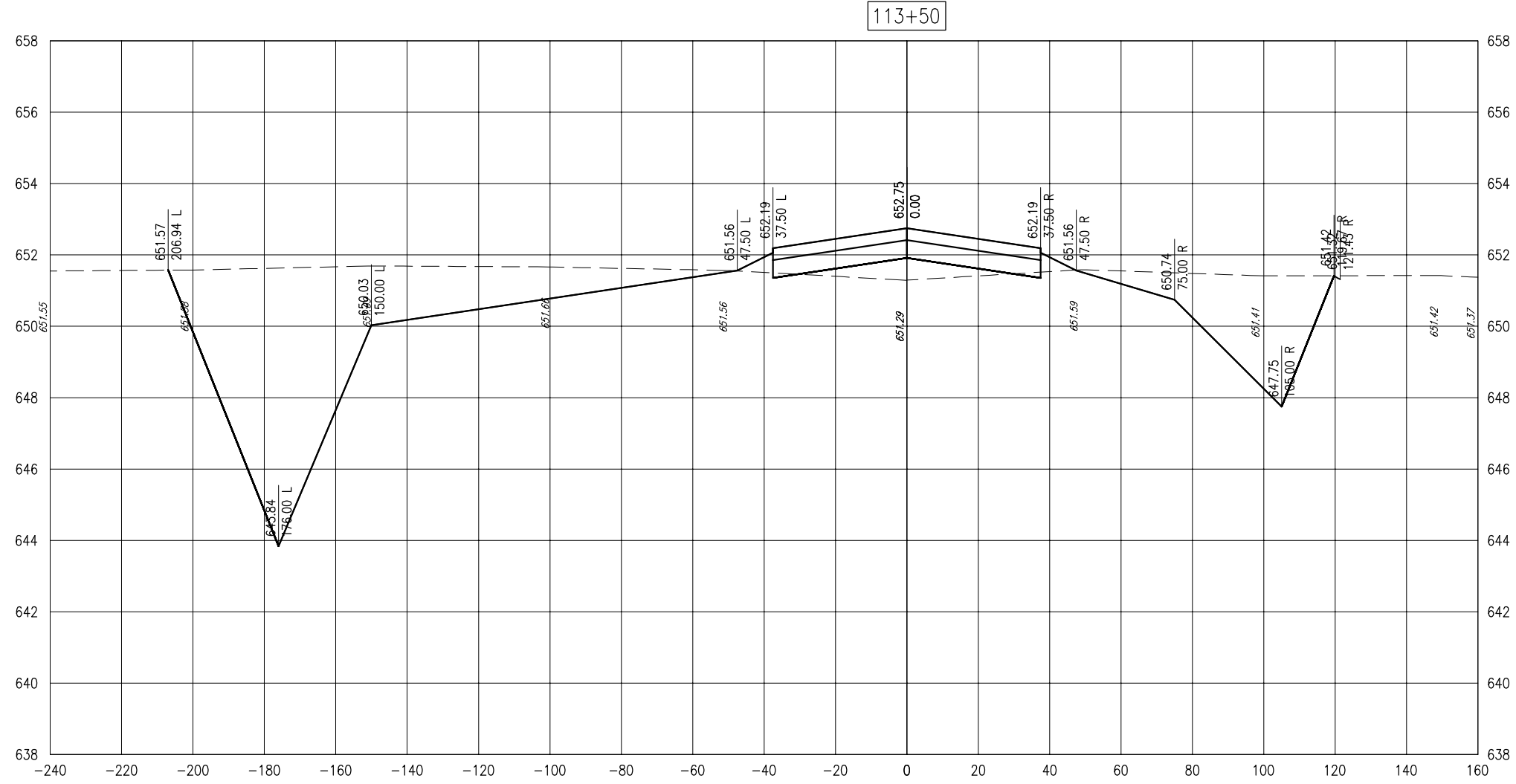
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RUNWAY 18-36

CROSS-SECTIONS  
FOR RUNWAY 18-36  
STA. 113+00



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EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

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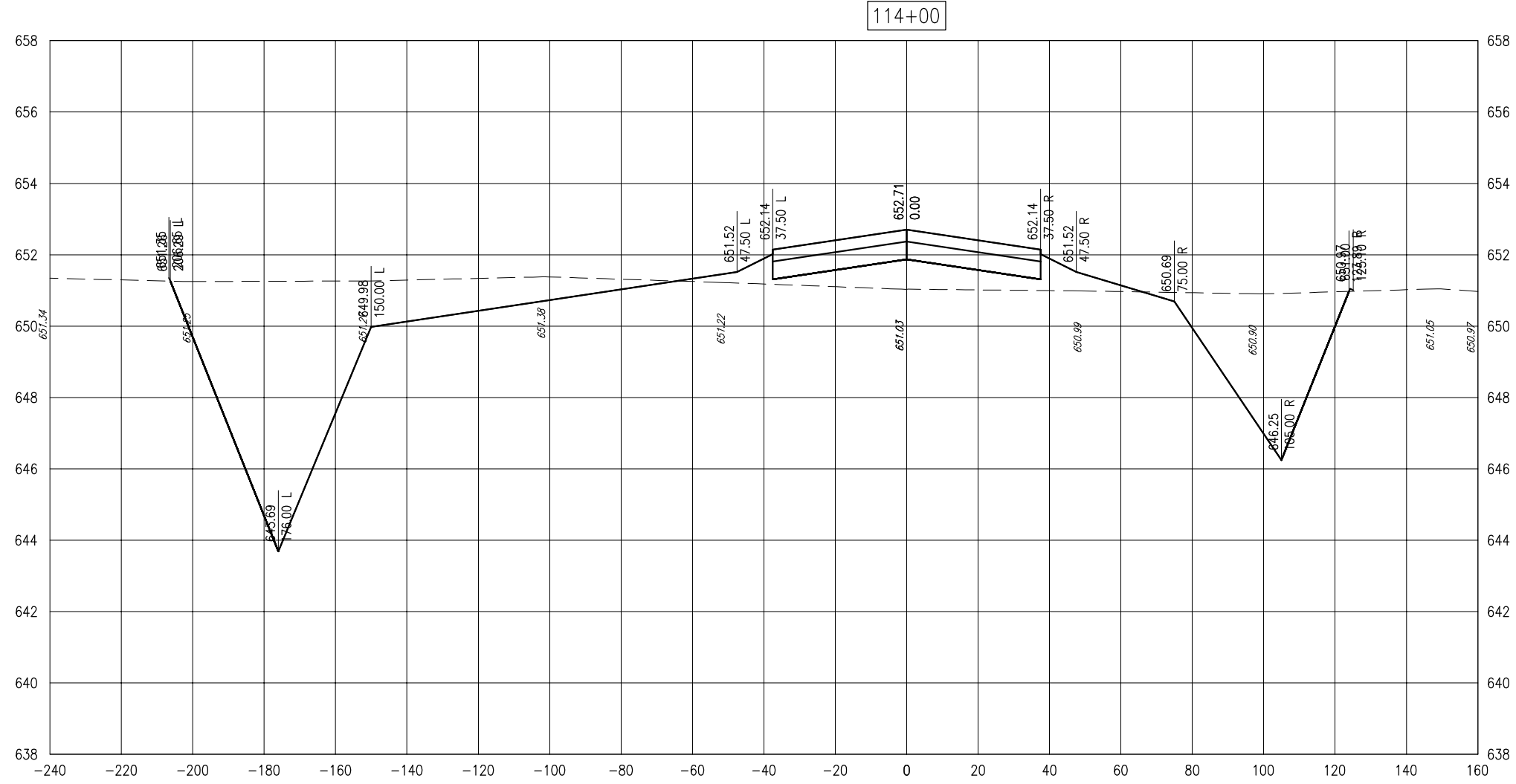
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REVIEWED	CAH	xx/xx/xx	

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RUNWAY 18-36

CROSS-SECTIONS  
FOR RUNWAY 18-36  
STA. 113+50



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PARIS, ILLINOIS

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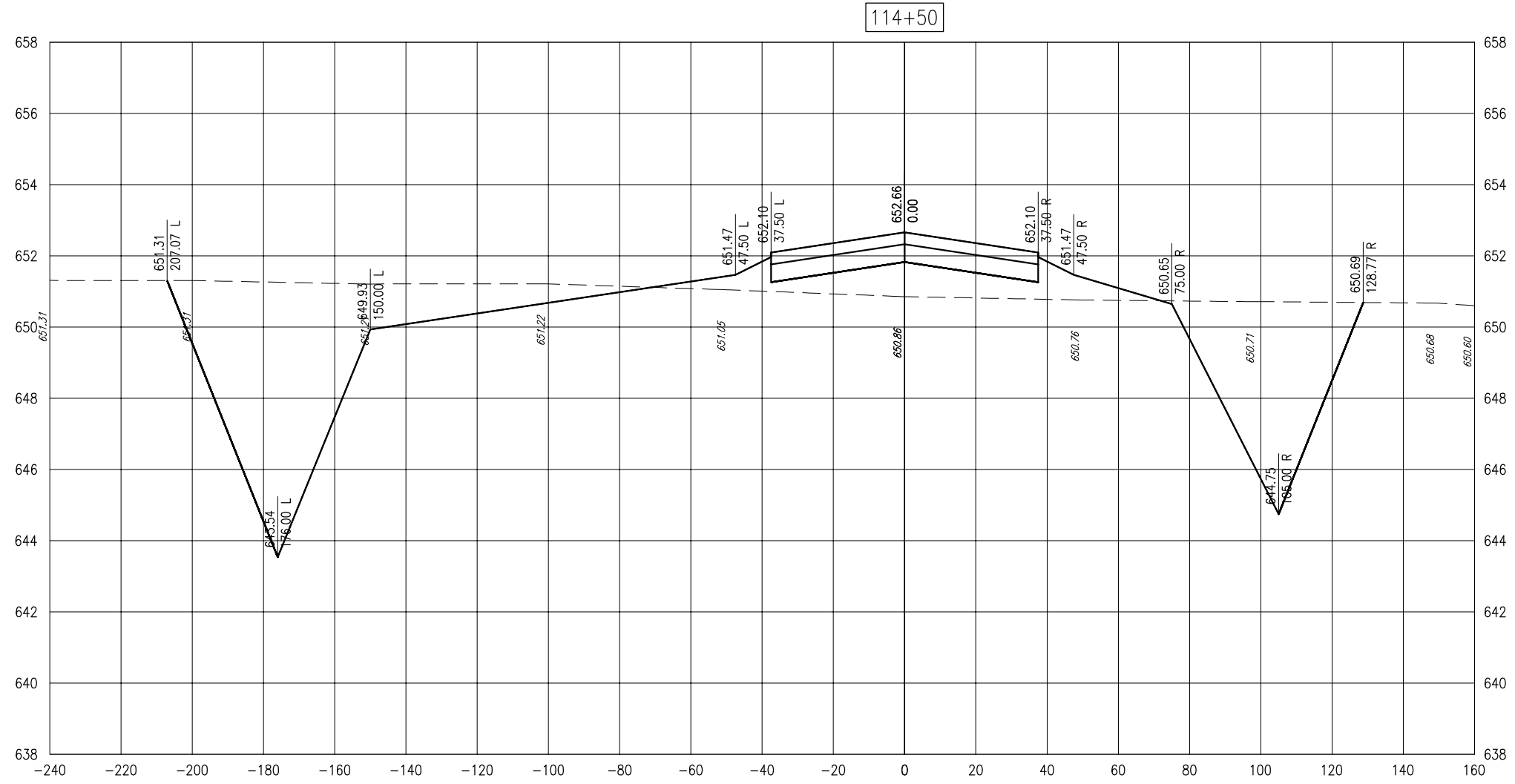
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CROSS-SECTIONS  
FOR RUNWAY 18-36  
STA. 114+00



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EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

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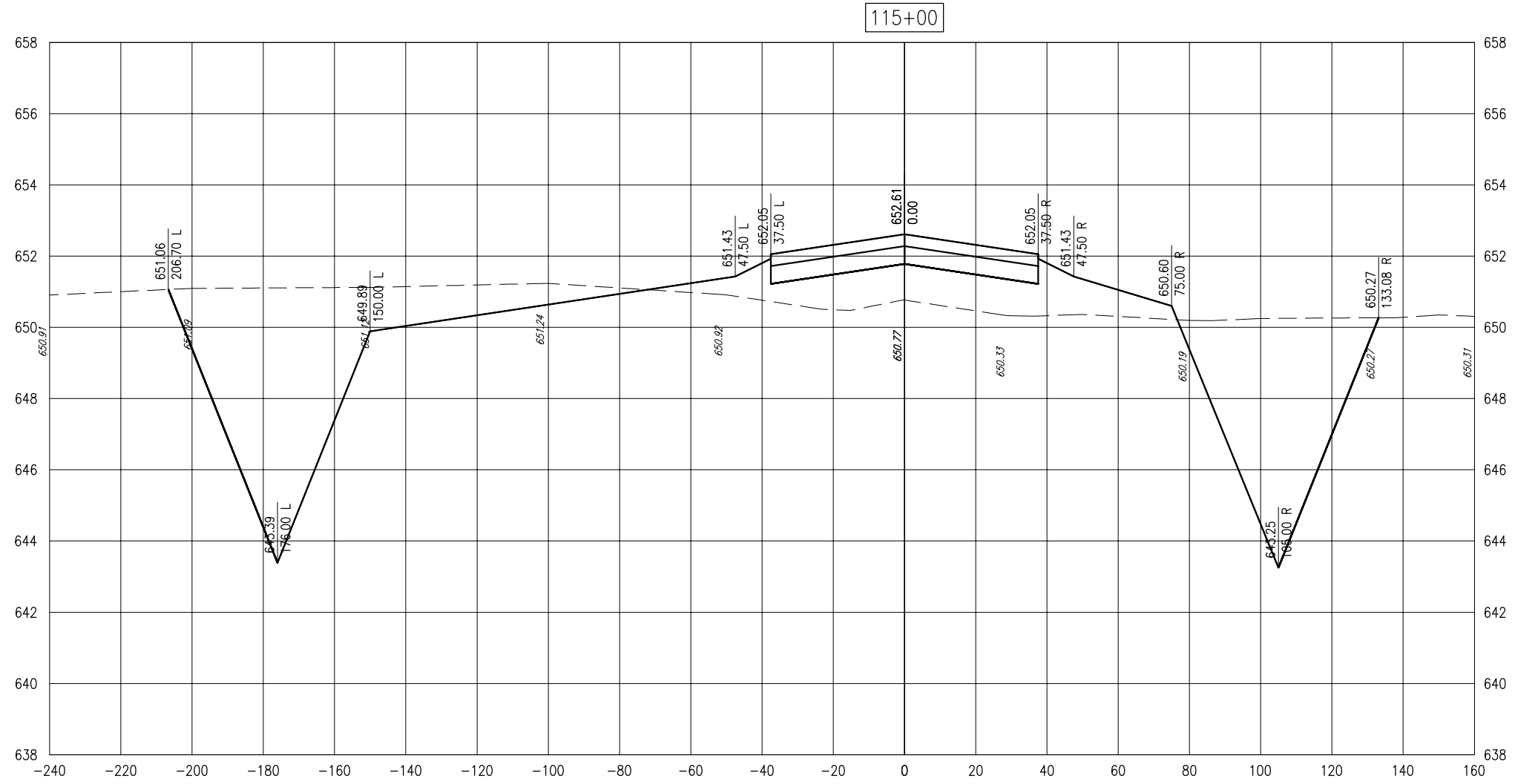
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CROSS-SECTIONS  
FOR RUNWAY 18-36  
STA. 114+50



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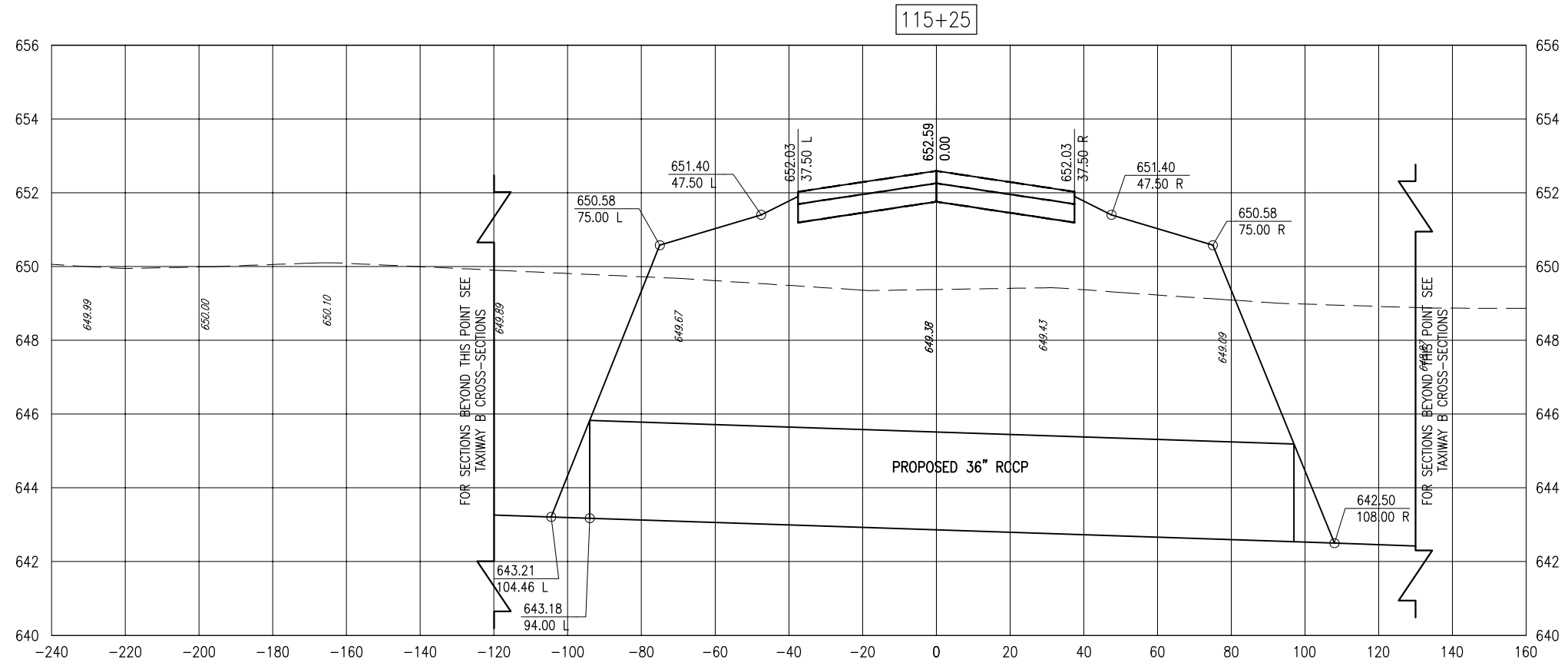
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RUNWAY 18-36

CROSS-SECTIONS  
FOR RUNWAY 18-36  
STA. 115+00



DATE	REVISION

EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

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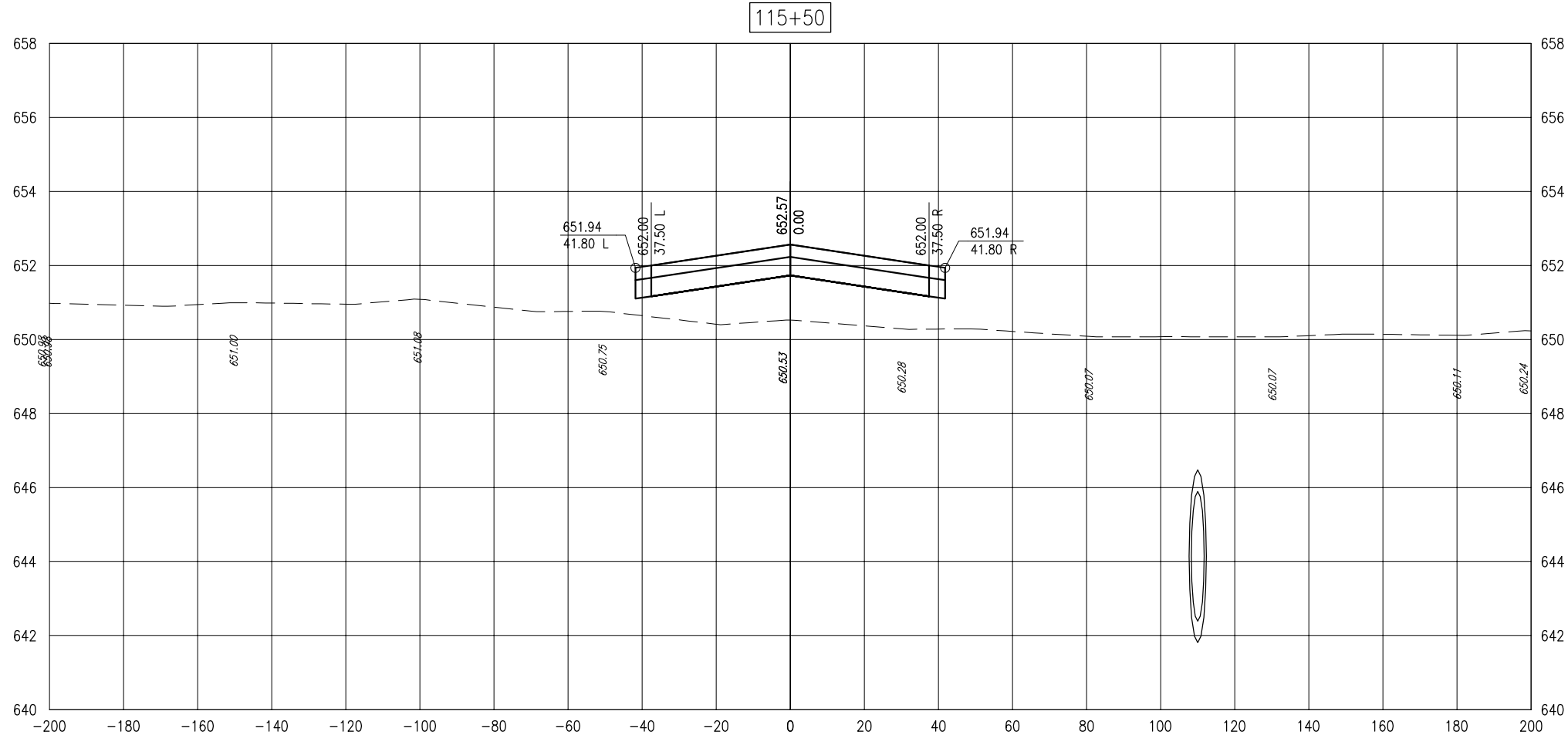
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REVIEWED CAH	xx/xx/xx

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CROSS-SECTIONS  
FOR RUNWAY 18-36  
STA. 115+25

SEE PROPOSED 18-36 RUNWAY AND EXISTING TAXIWAY "B" INTERSECTION CROSS-SECTIONS



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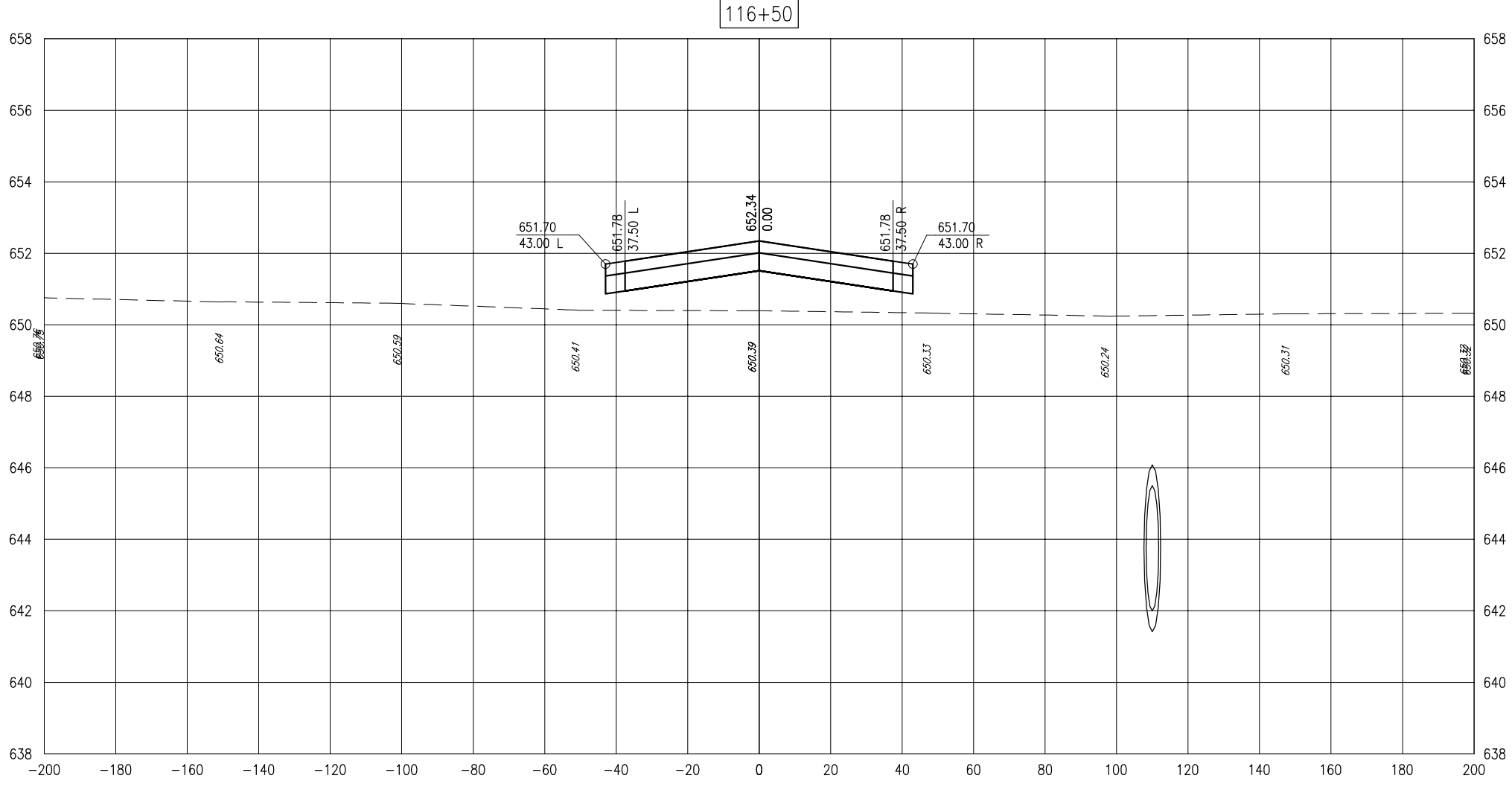
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 RUNWAY 18-36**

**CROSS-SECTIONS  
 FOR RUNWAY 18-36  
 STA. 115+50**

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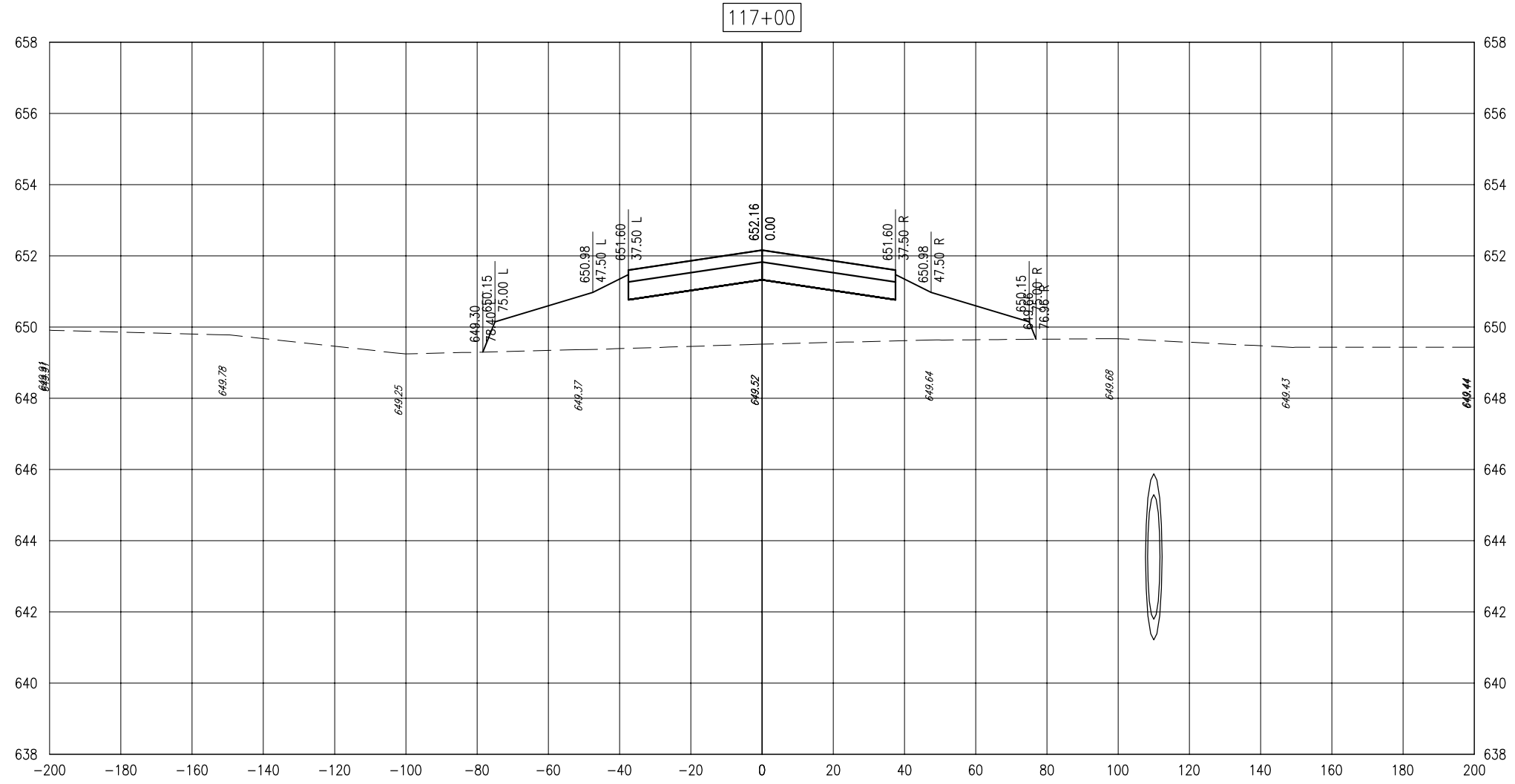
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REVIEWED	CAH		

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CROSS-SECTIONS  
FOR RUNWAY 18-36  
STA. 116+50





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PARIS, ILLINOIS

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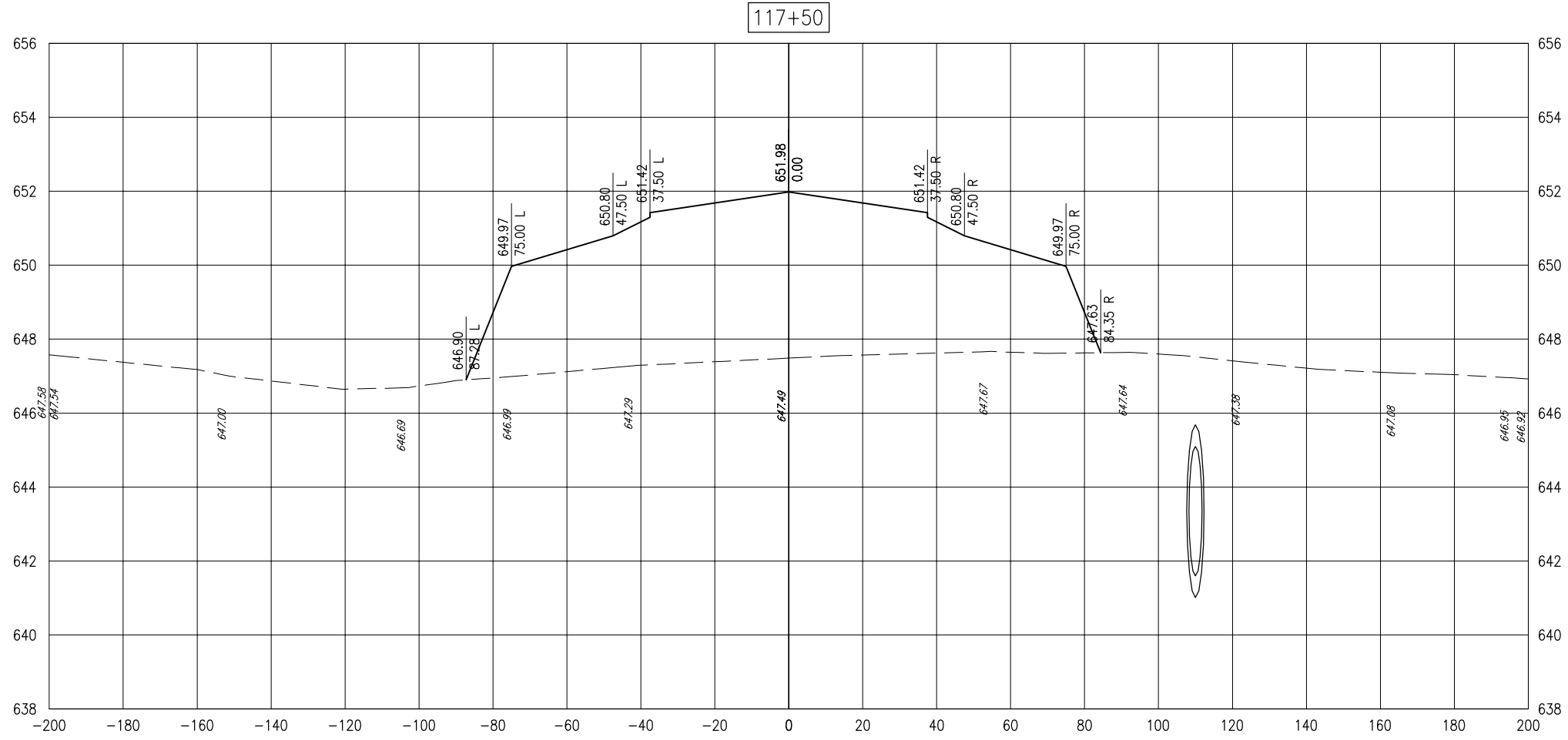
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REVIEWED	CAH	xx/xx/xx	

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RUNWAY 18-36

CROSS-SECTIONS  
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STA. 117+00



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EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

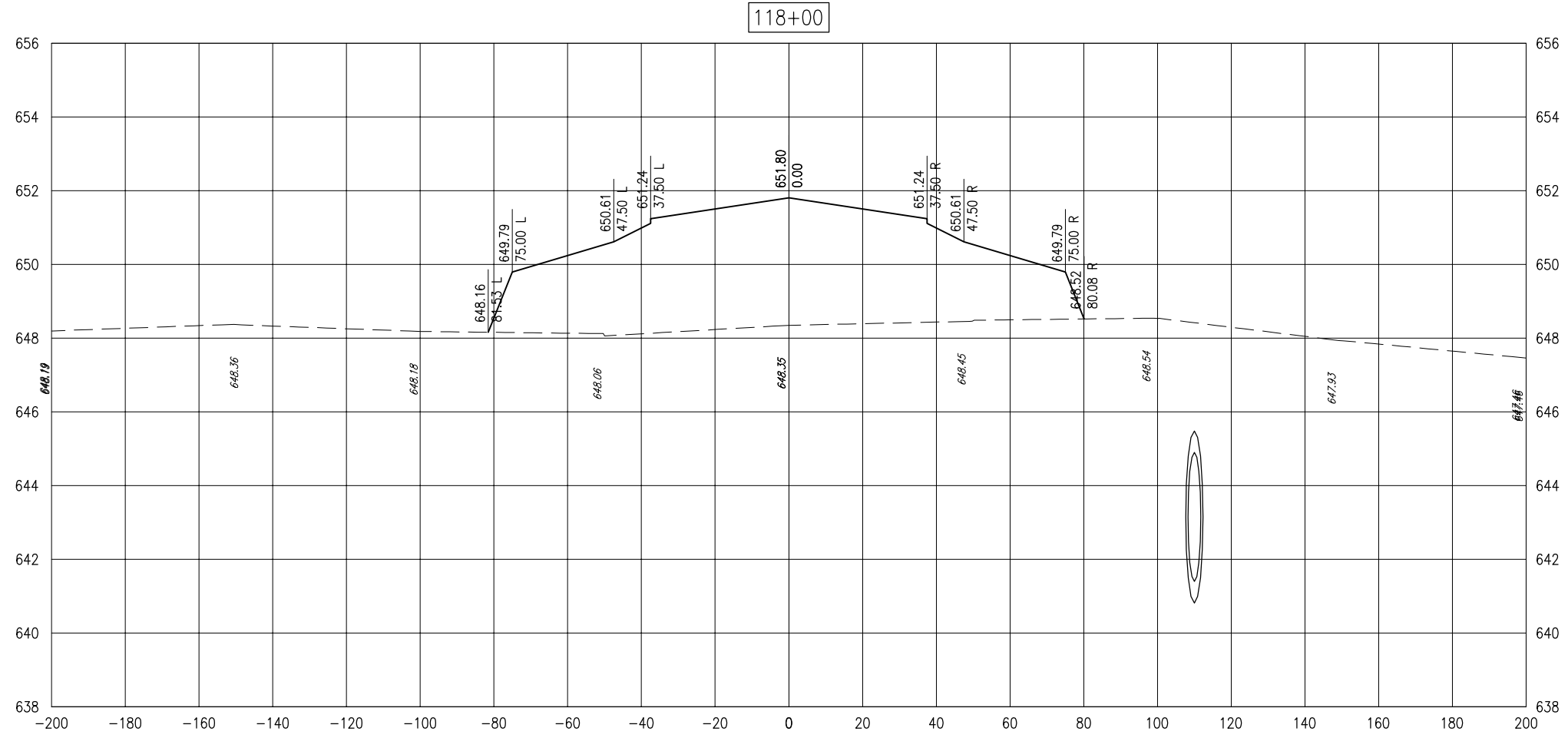
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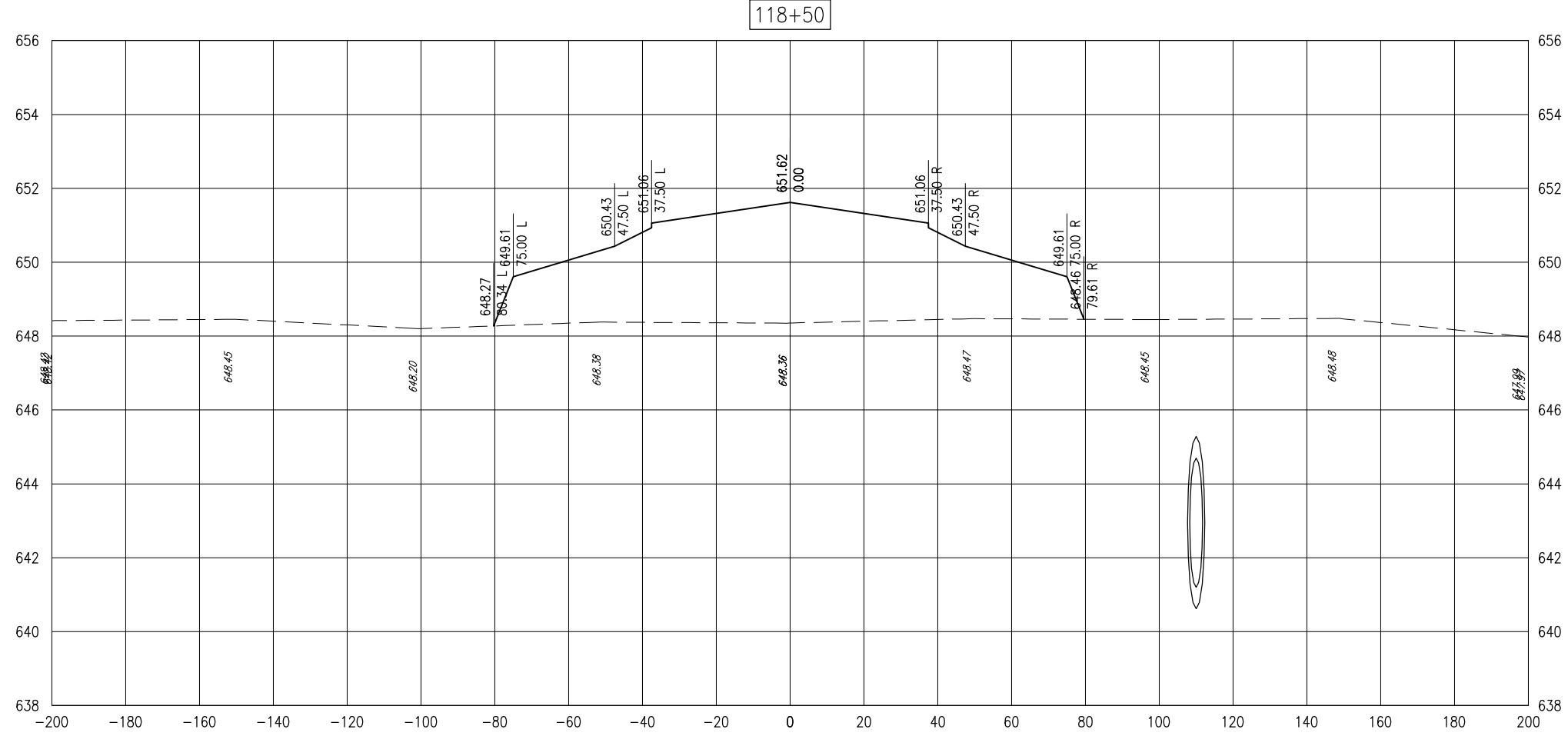
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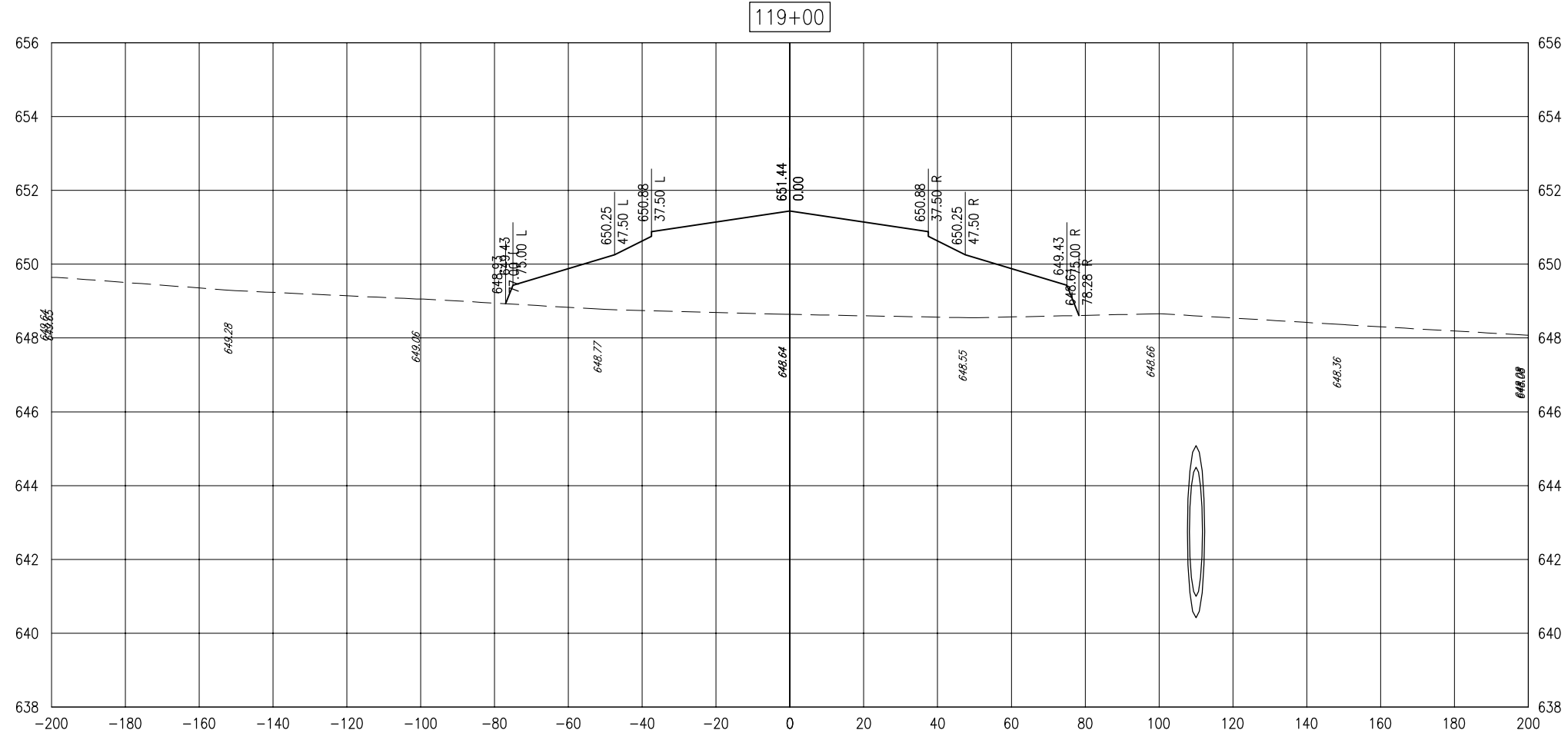
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MAR 11, 2011 3:22 PM RINKED0461 C:\PWISE\_WORK\DO\_NOT\_DELETE\DM18063\18-36\6XS.DWG

SEE PROPOSED 18-36 RUNWAY AND EXISTING RUNWAY 9-27 INTERSECTION CROSS-SECTIONS



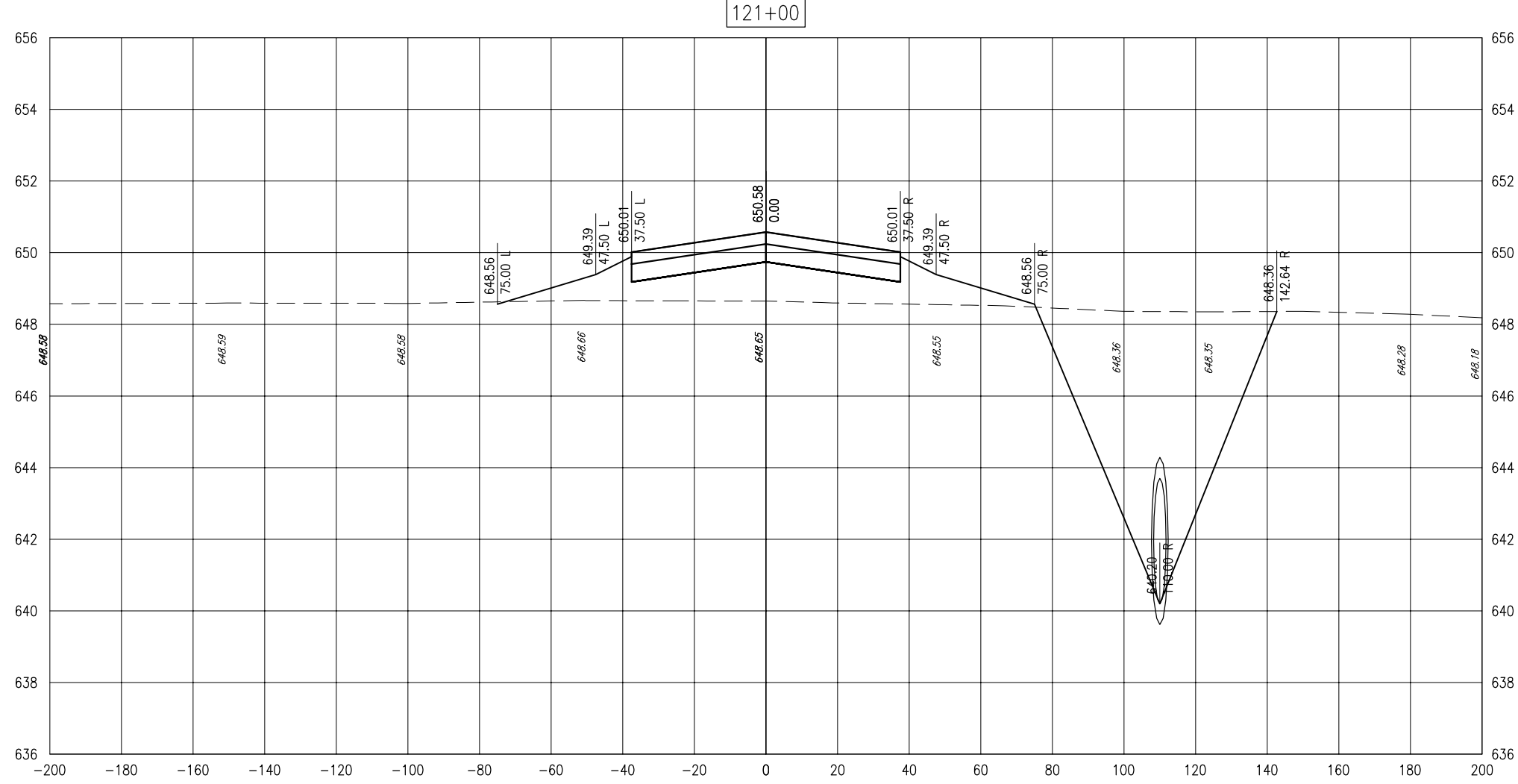
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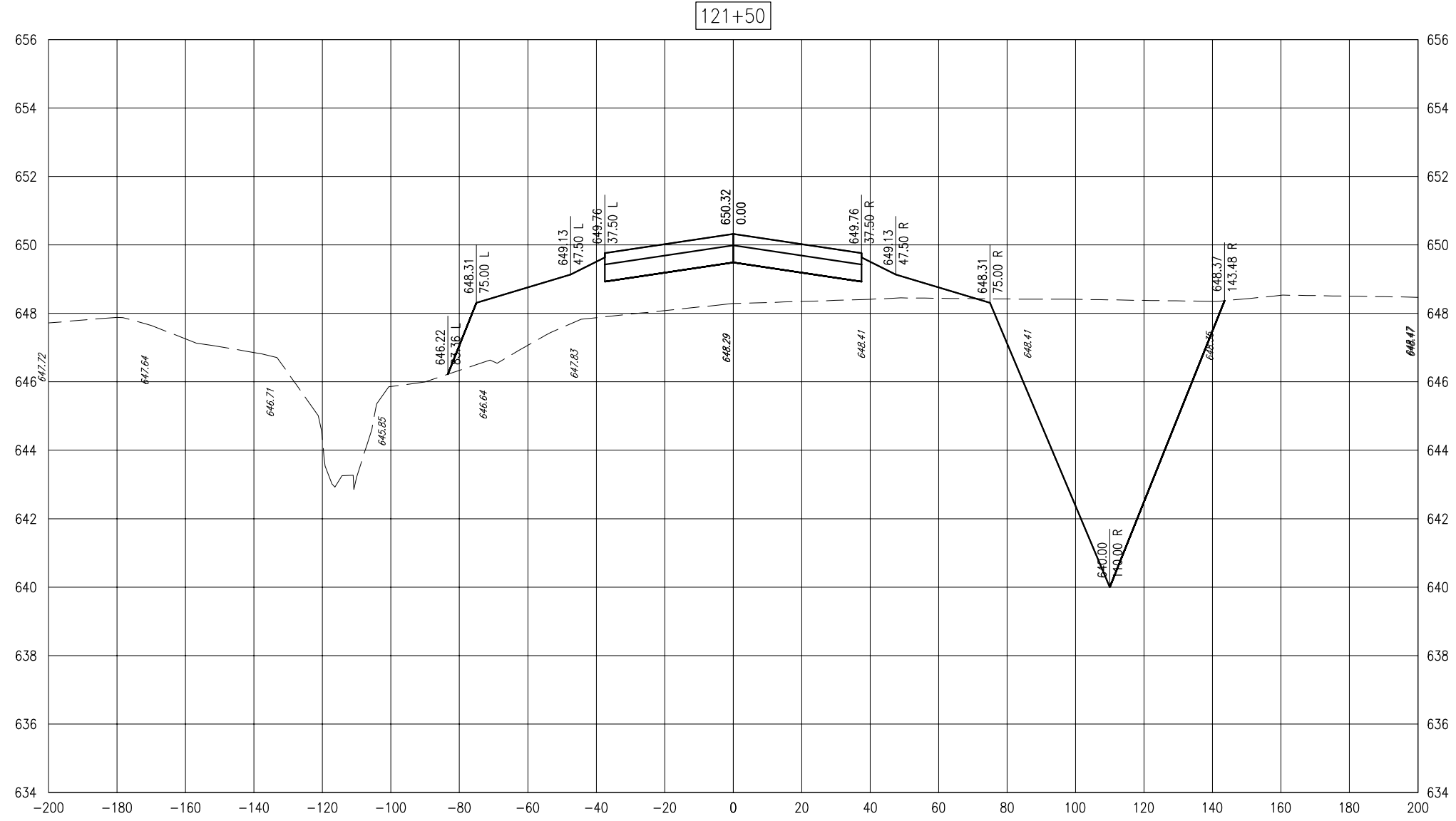
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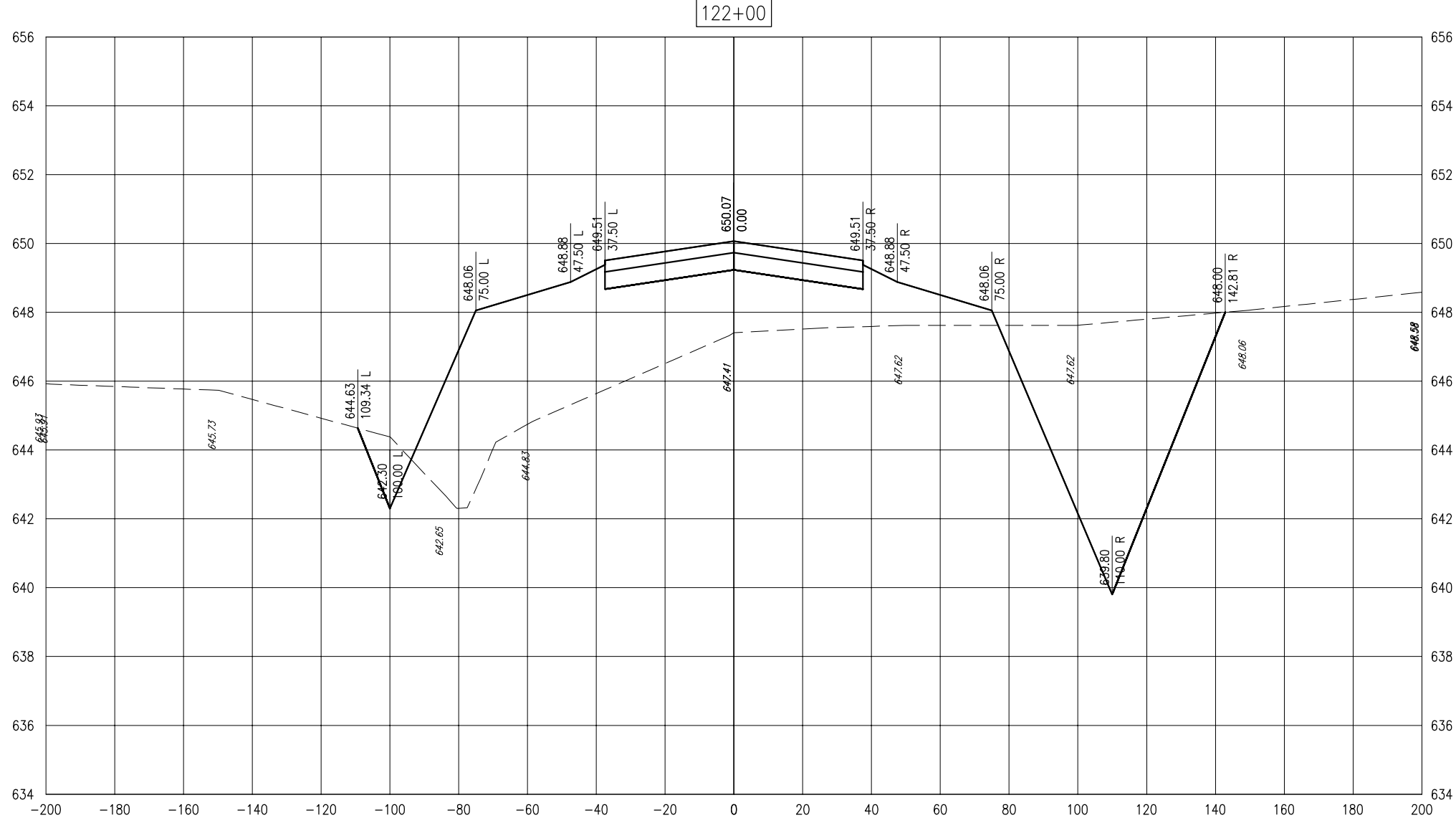
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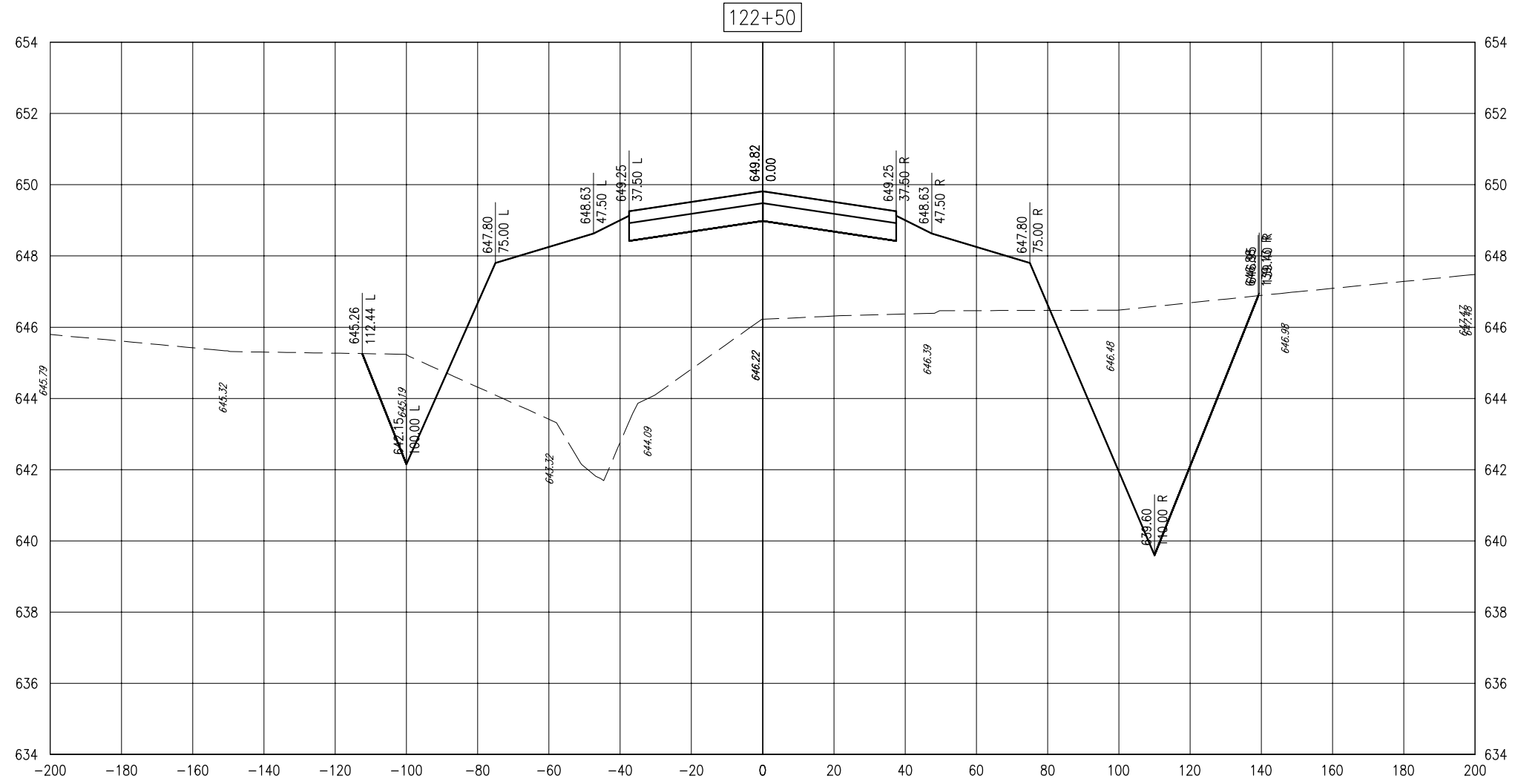
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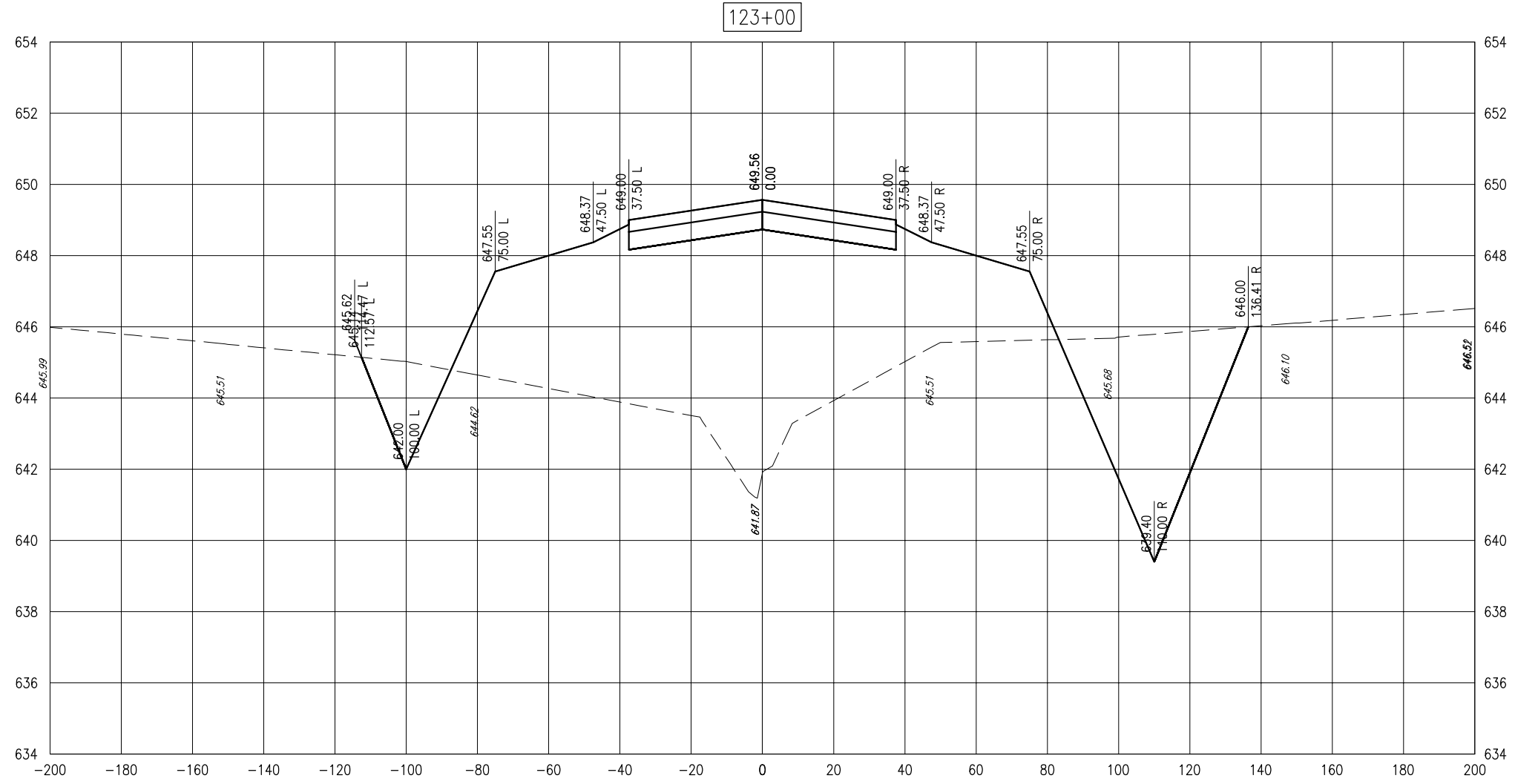
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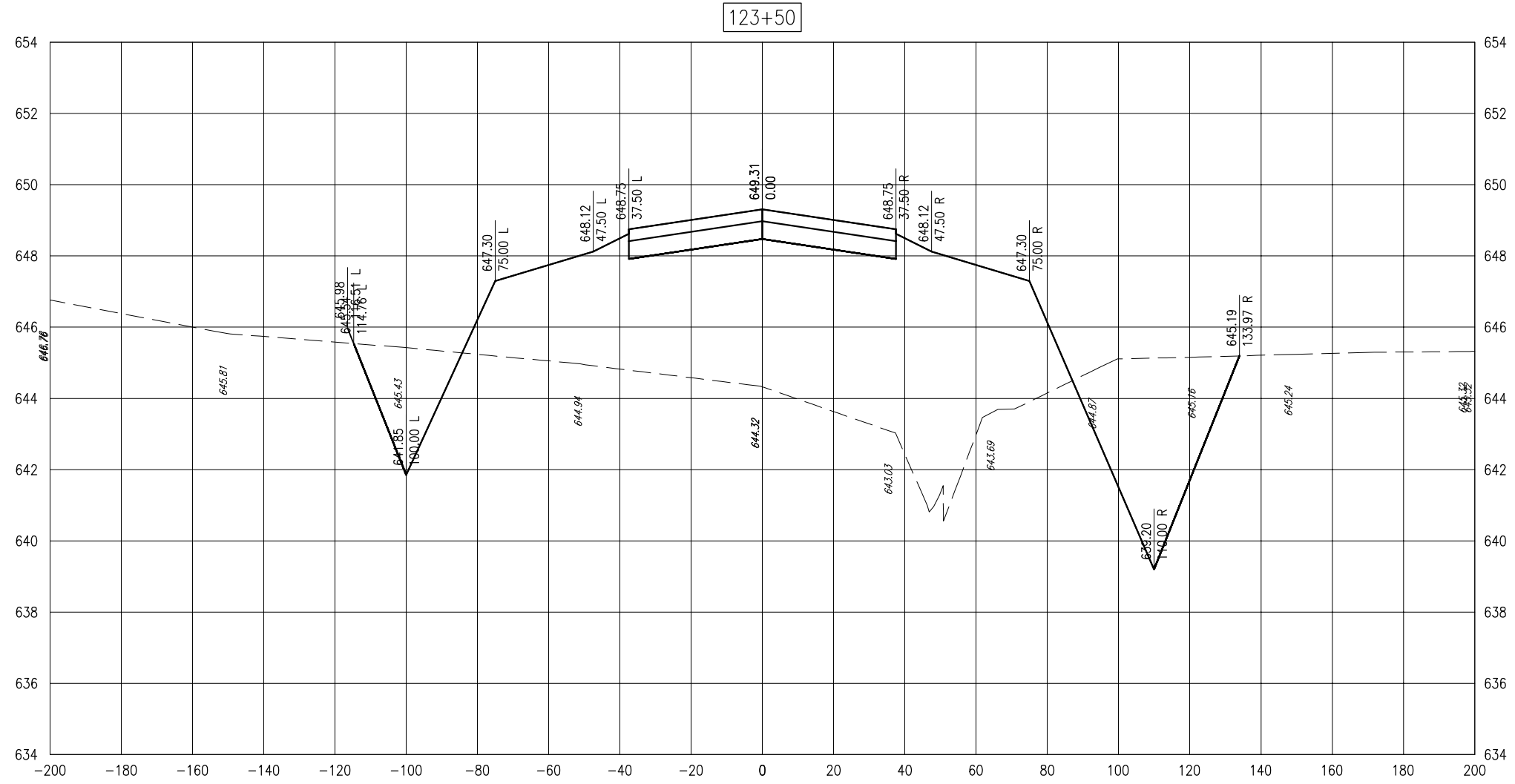
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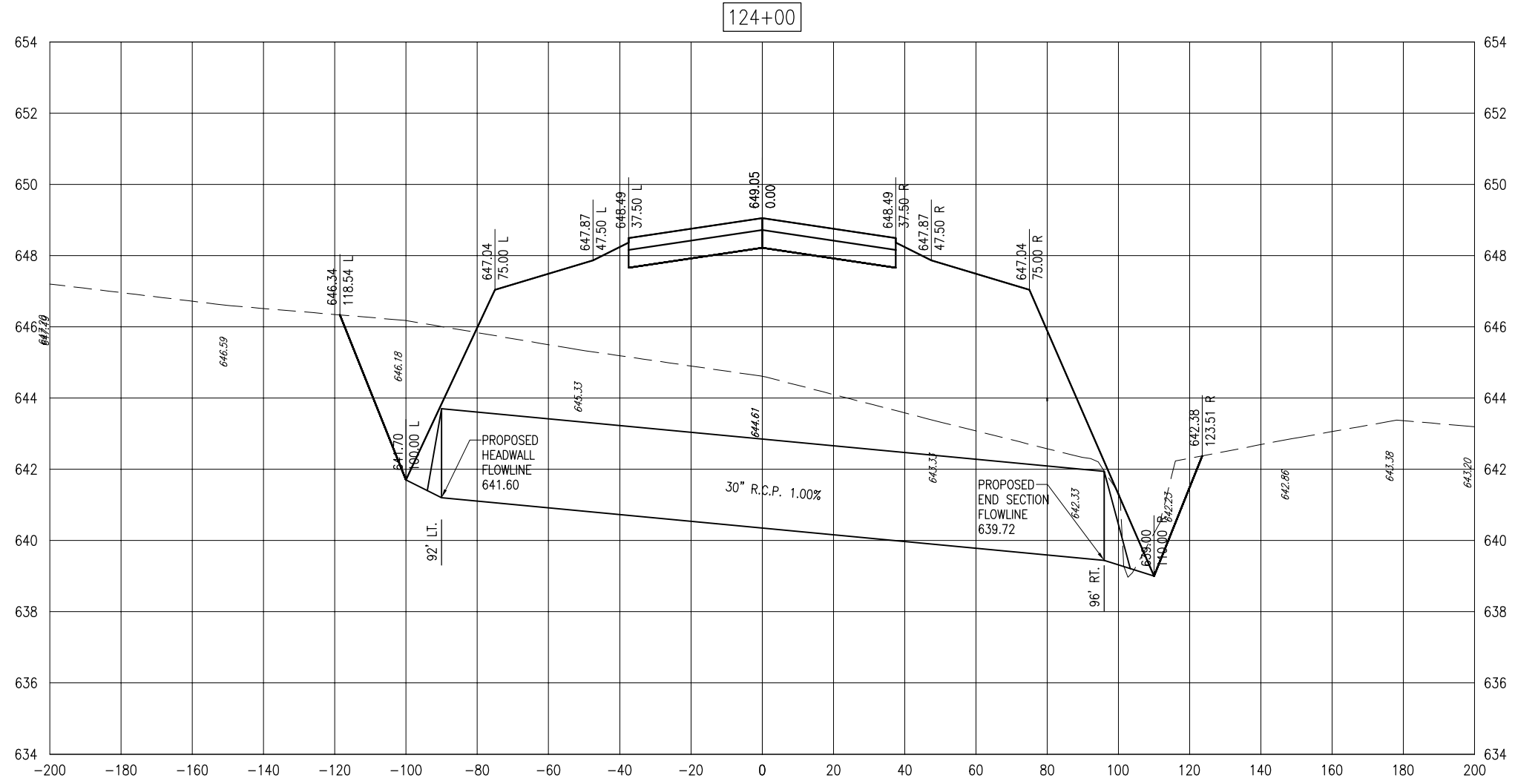
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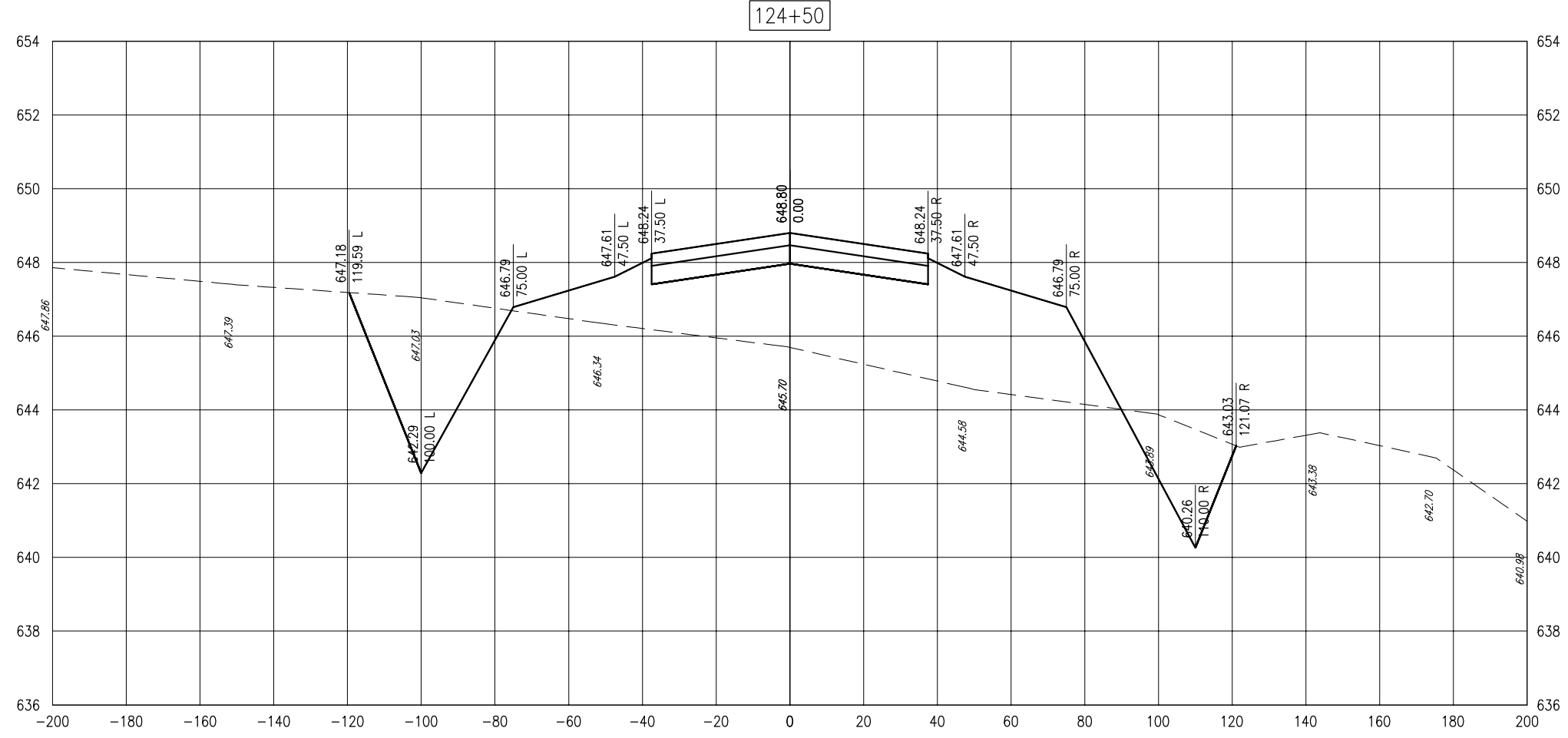
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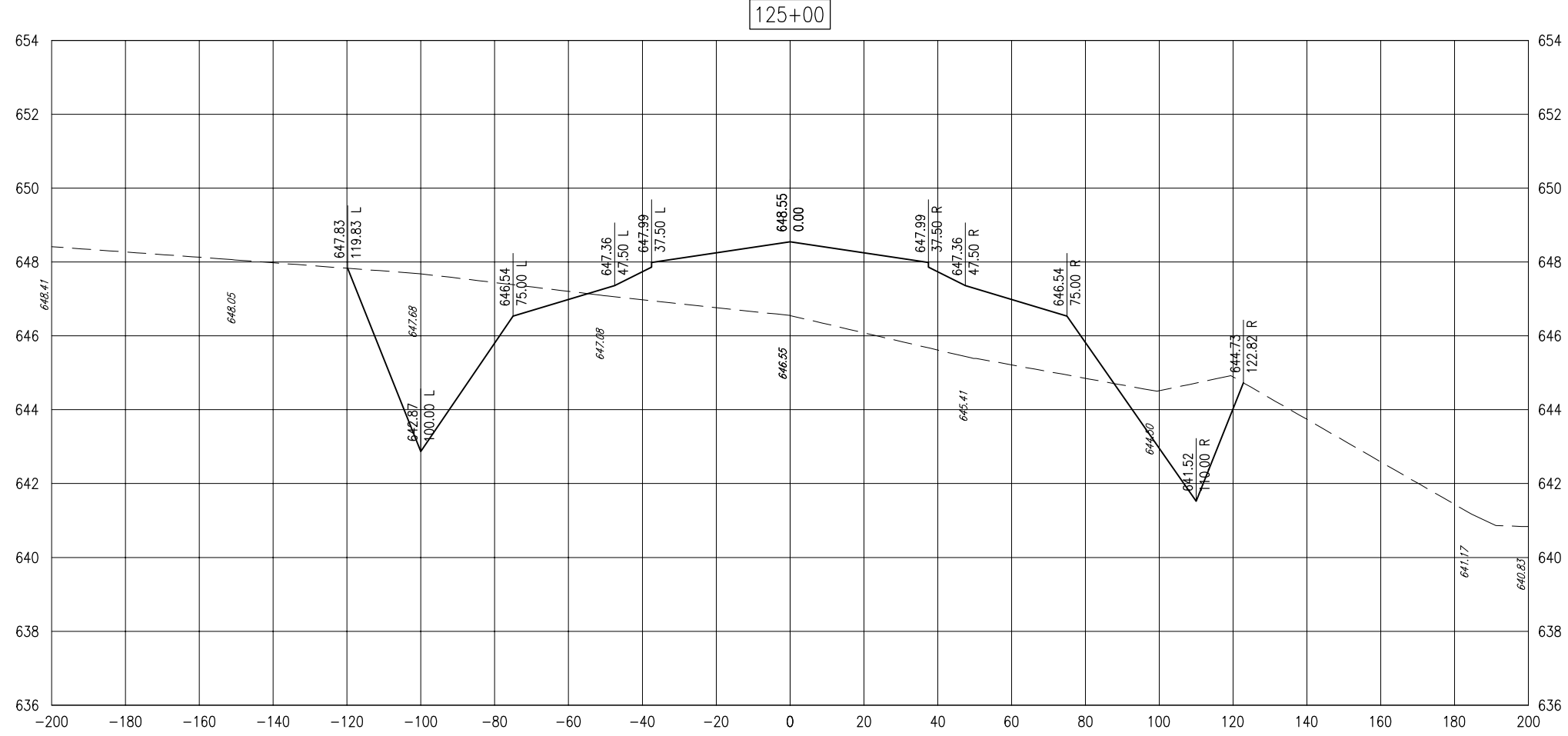
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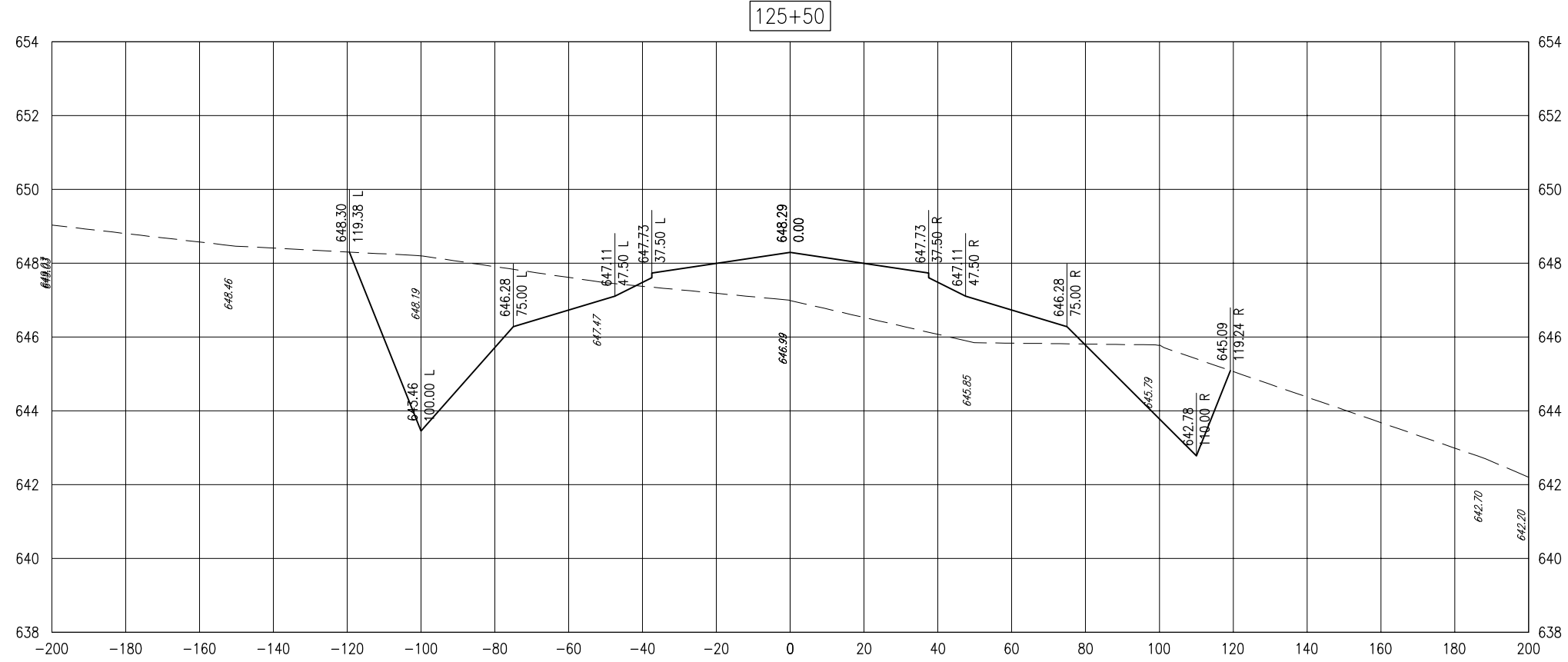
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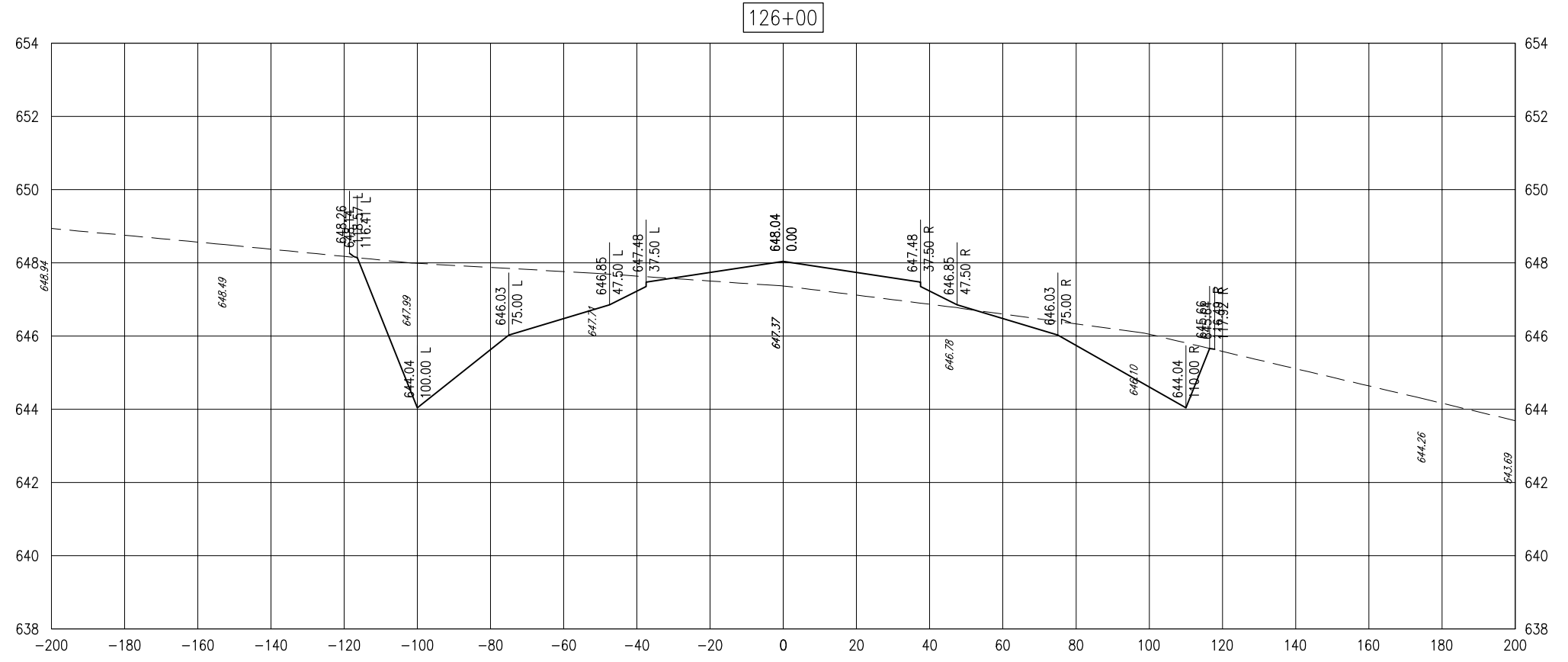
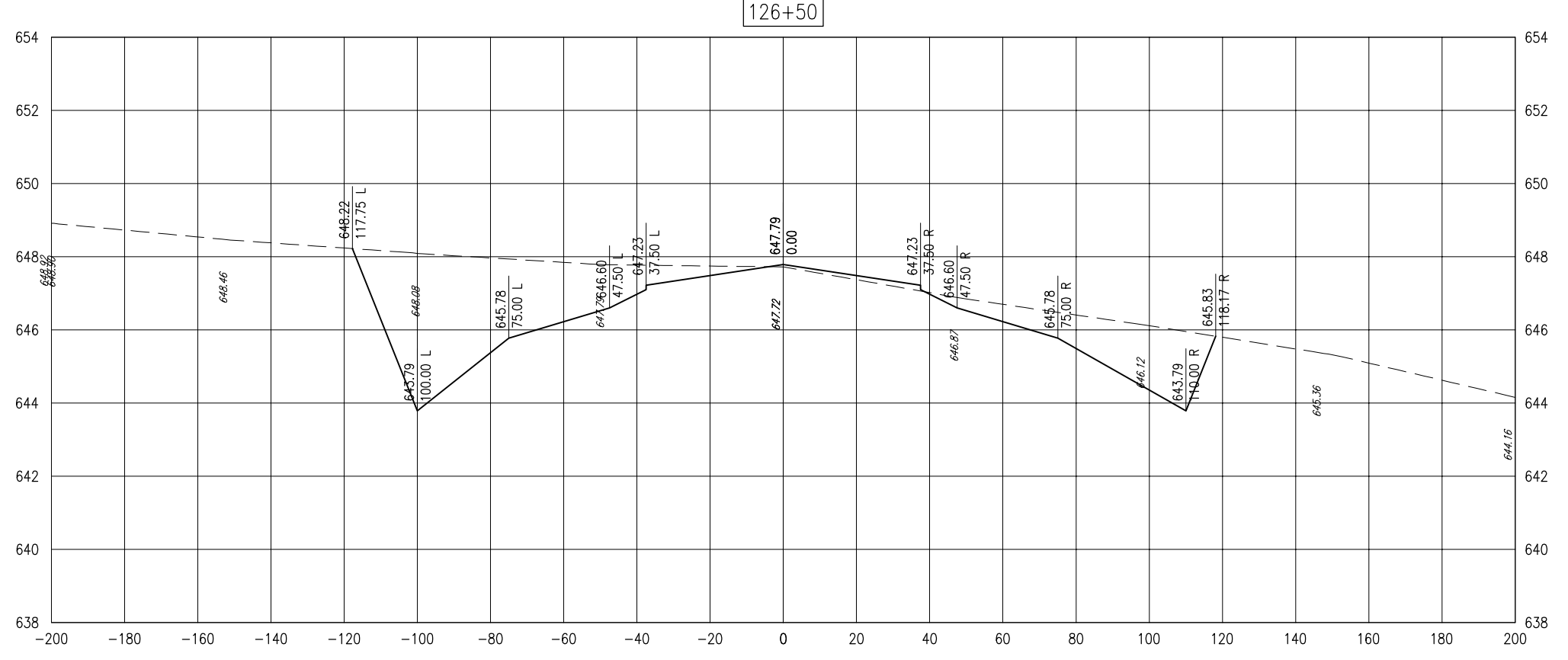
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EDGAR COUNTY AIRPORT  
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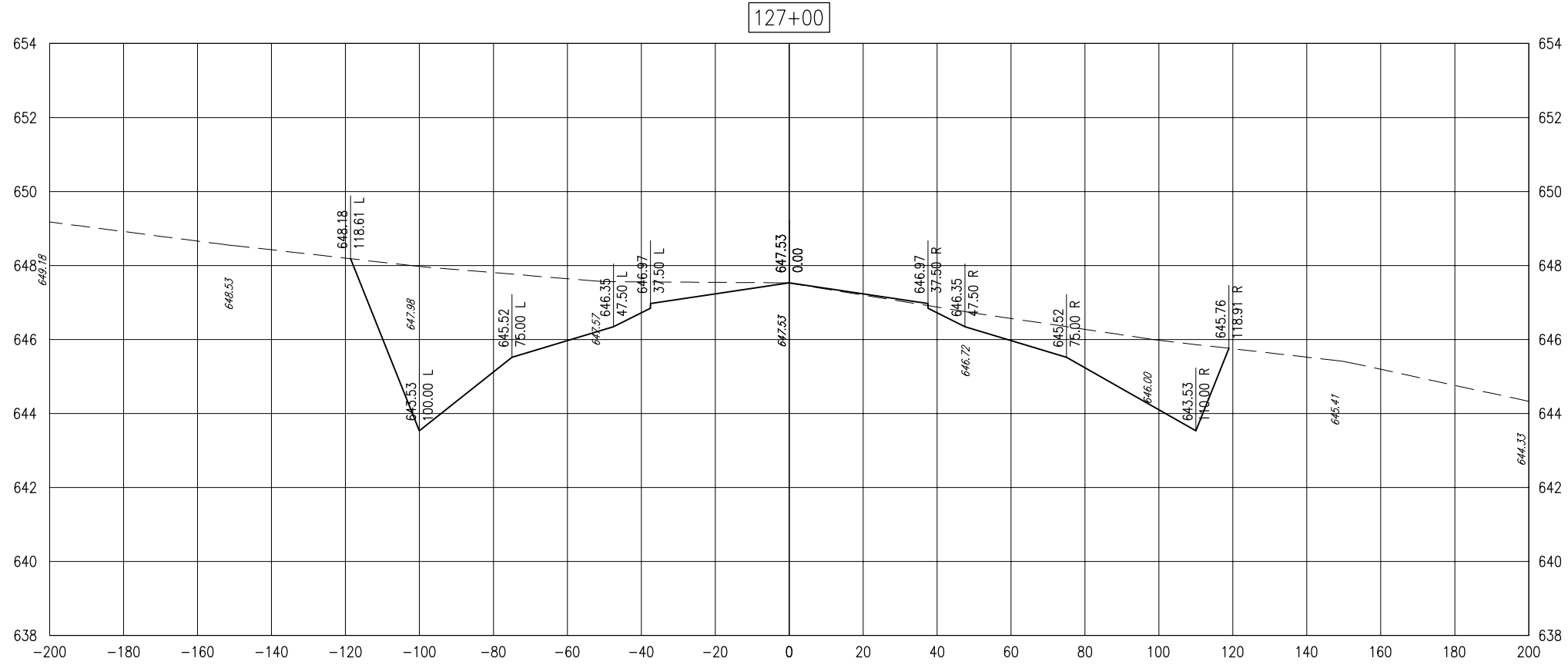
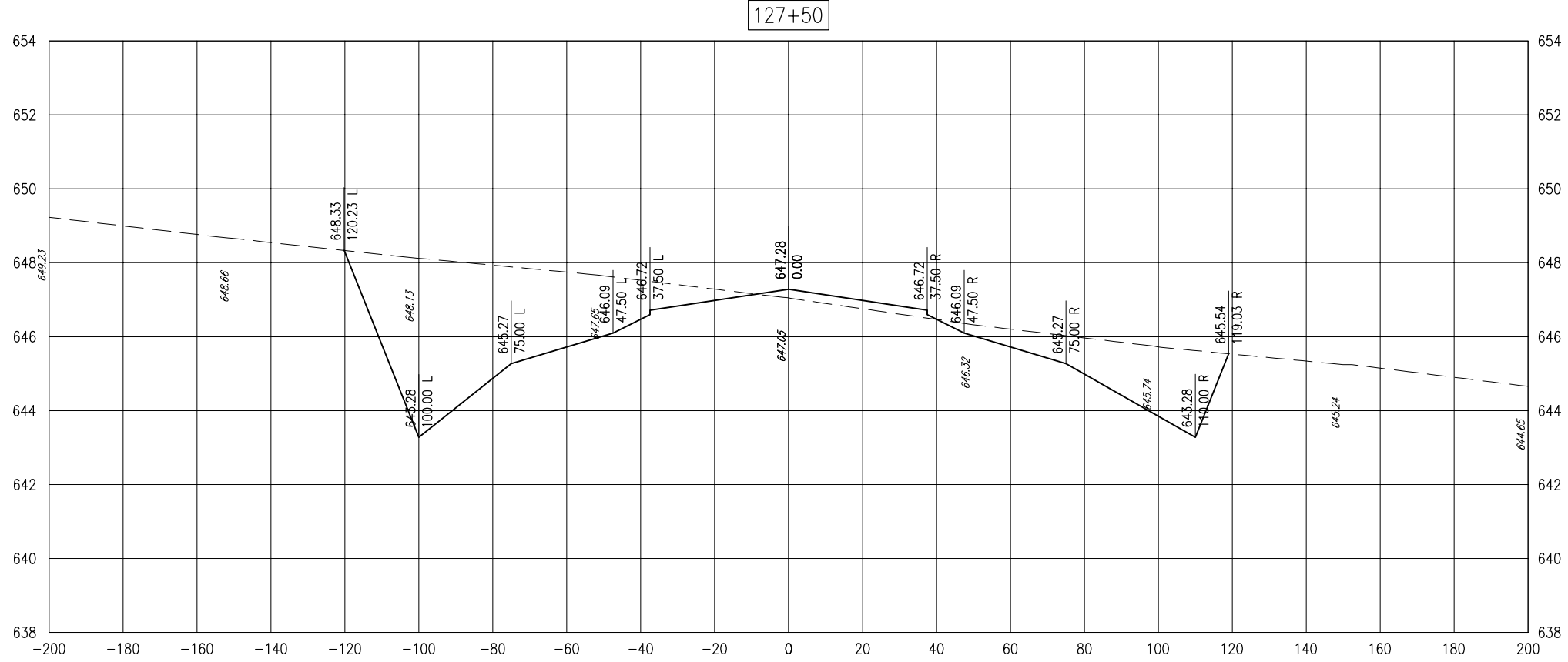
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126+00 TO STA. 126+50





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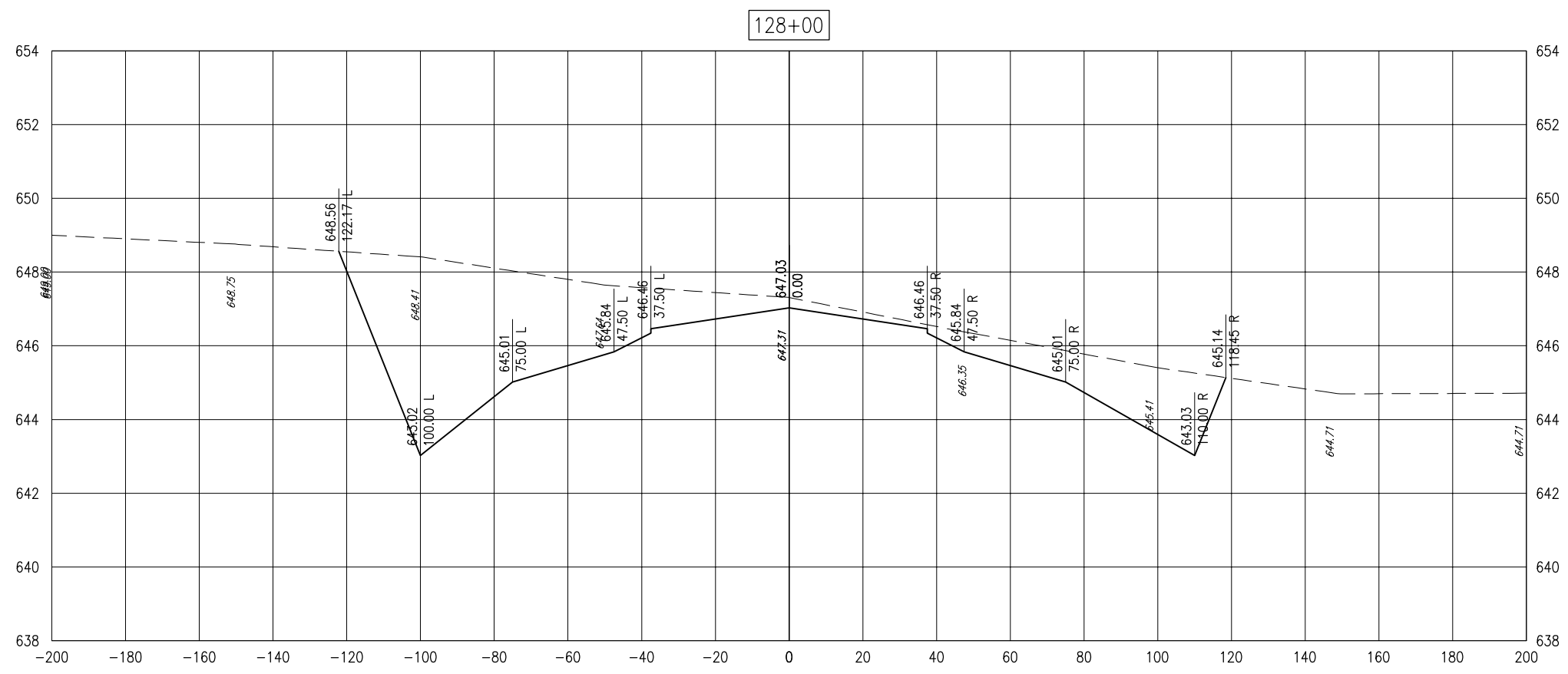
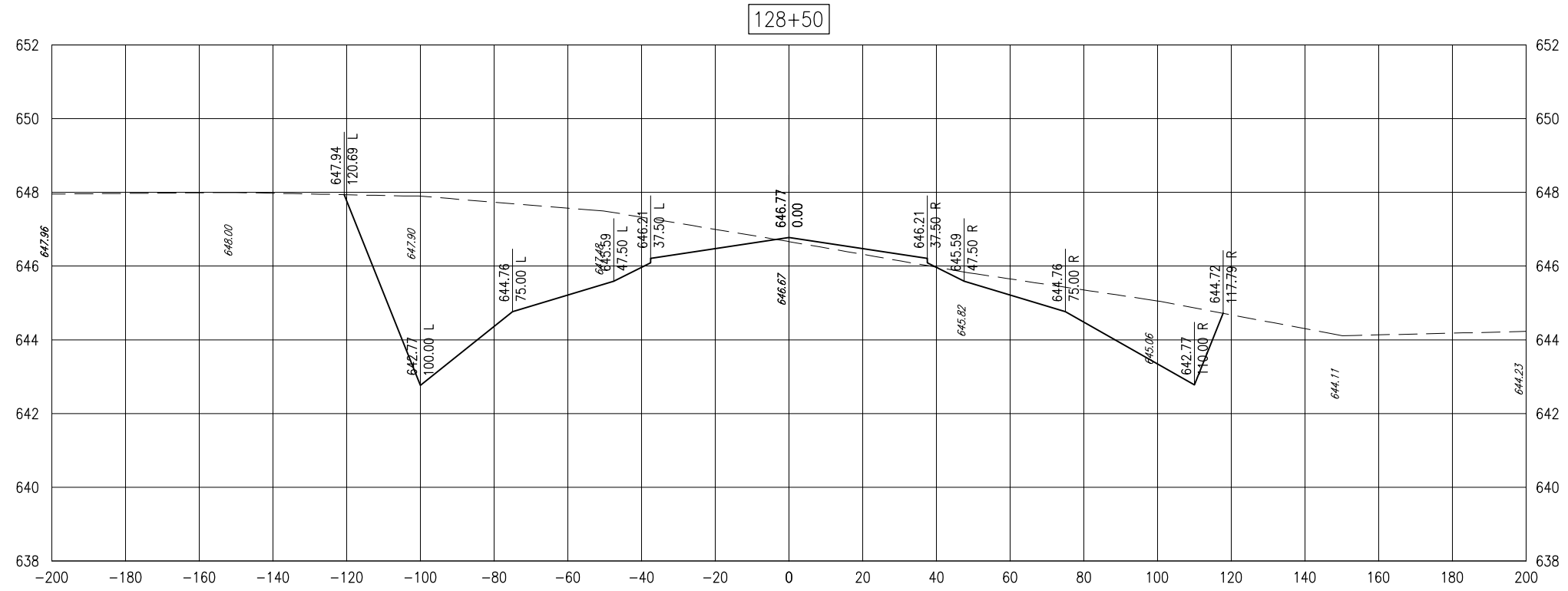
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EDGAR COUNTY AIRPORT  
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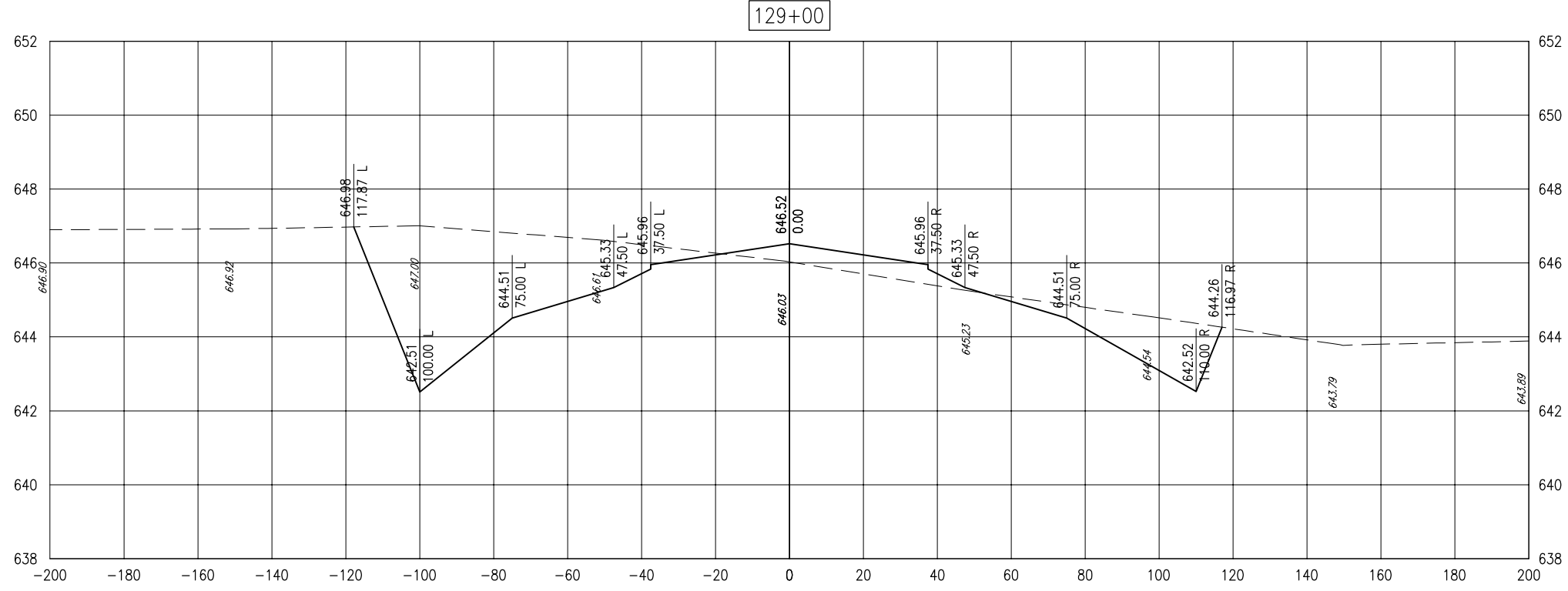
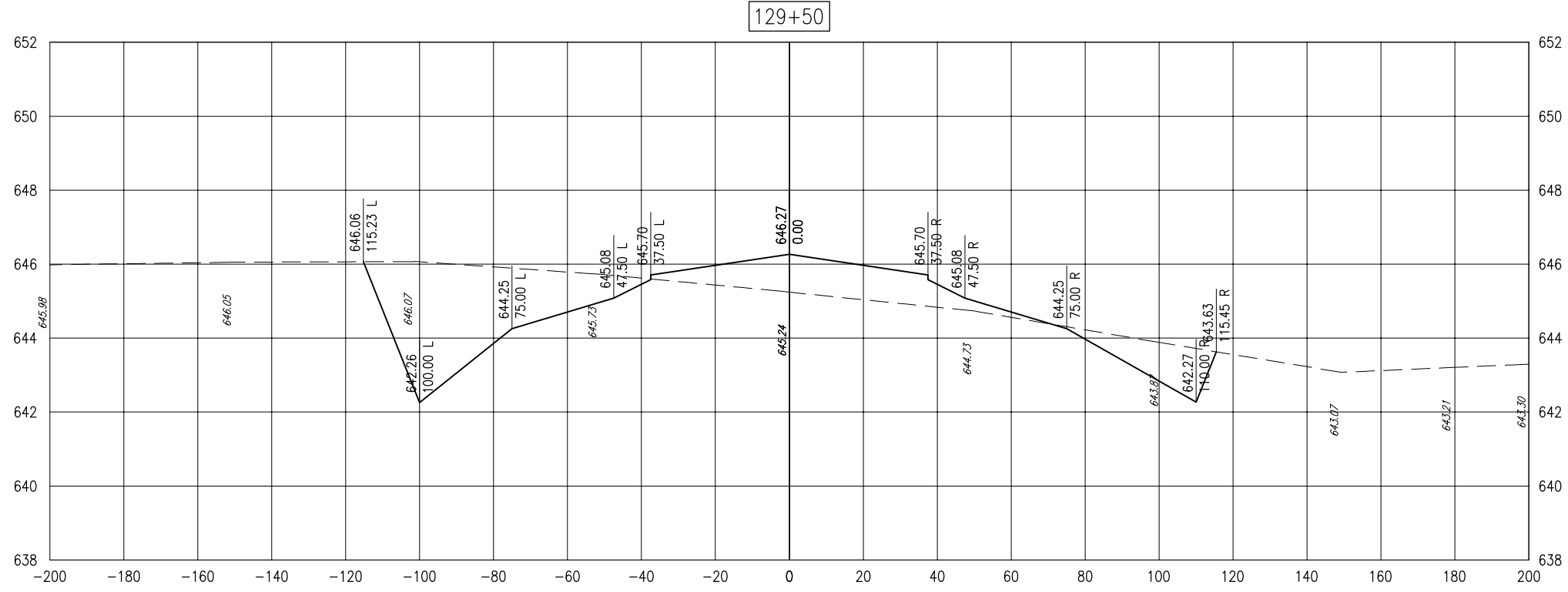
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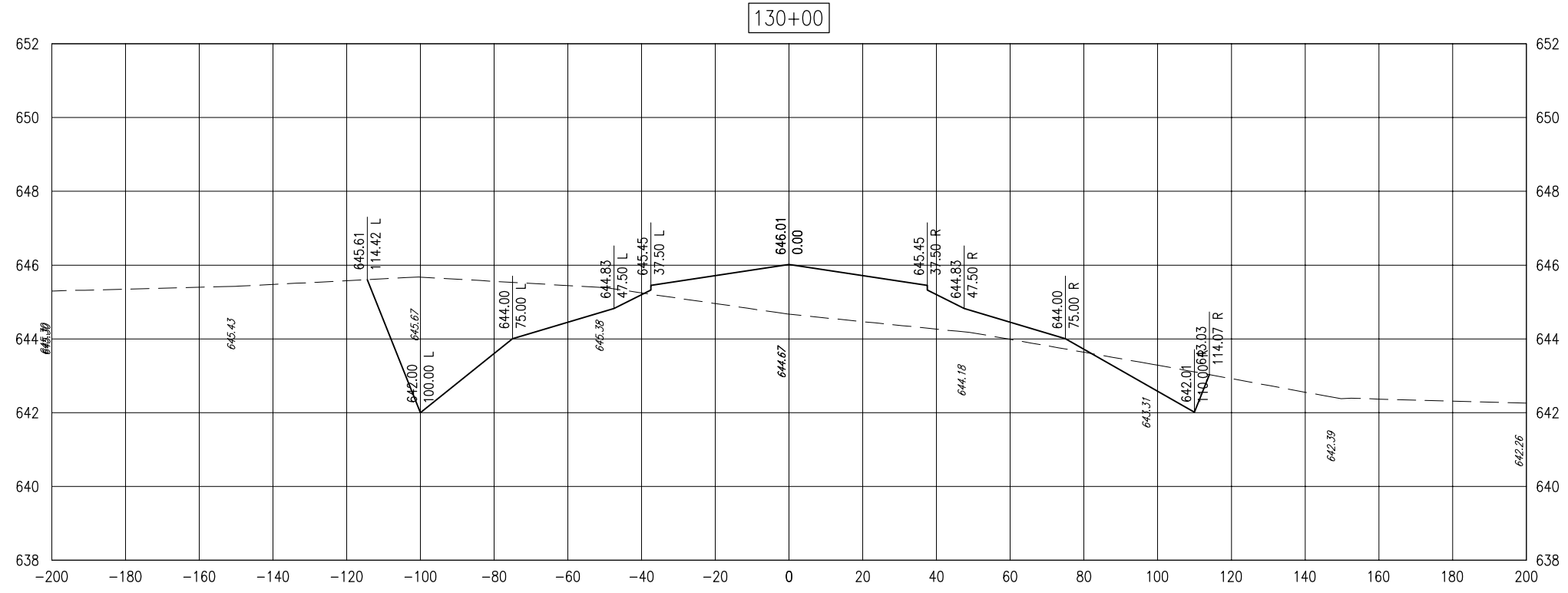
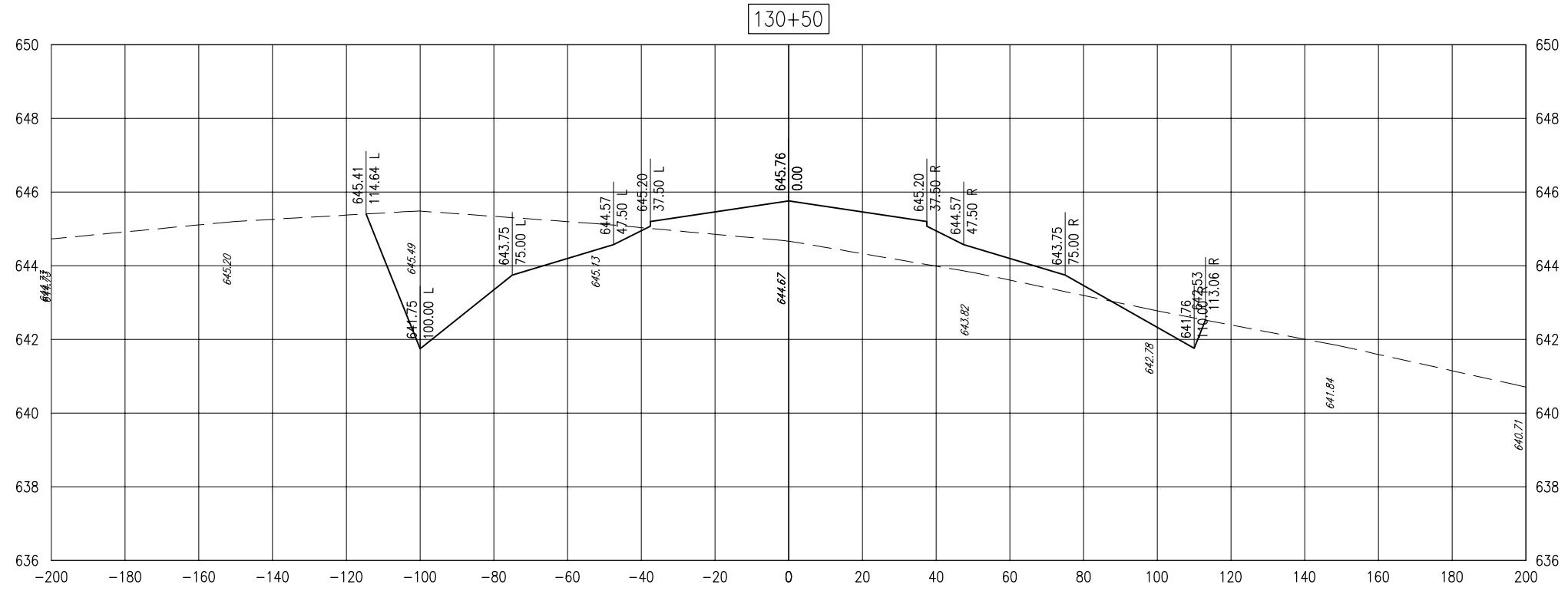
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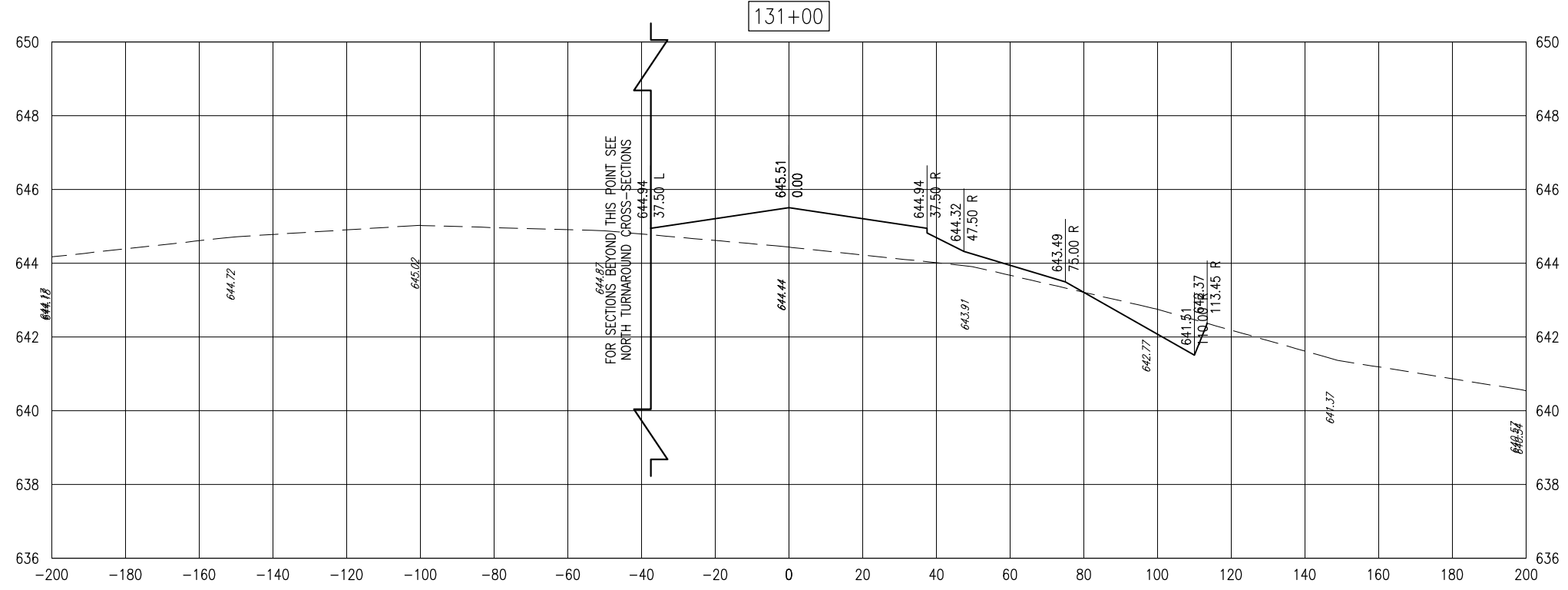
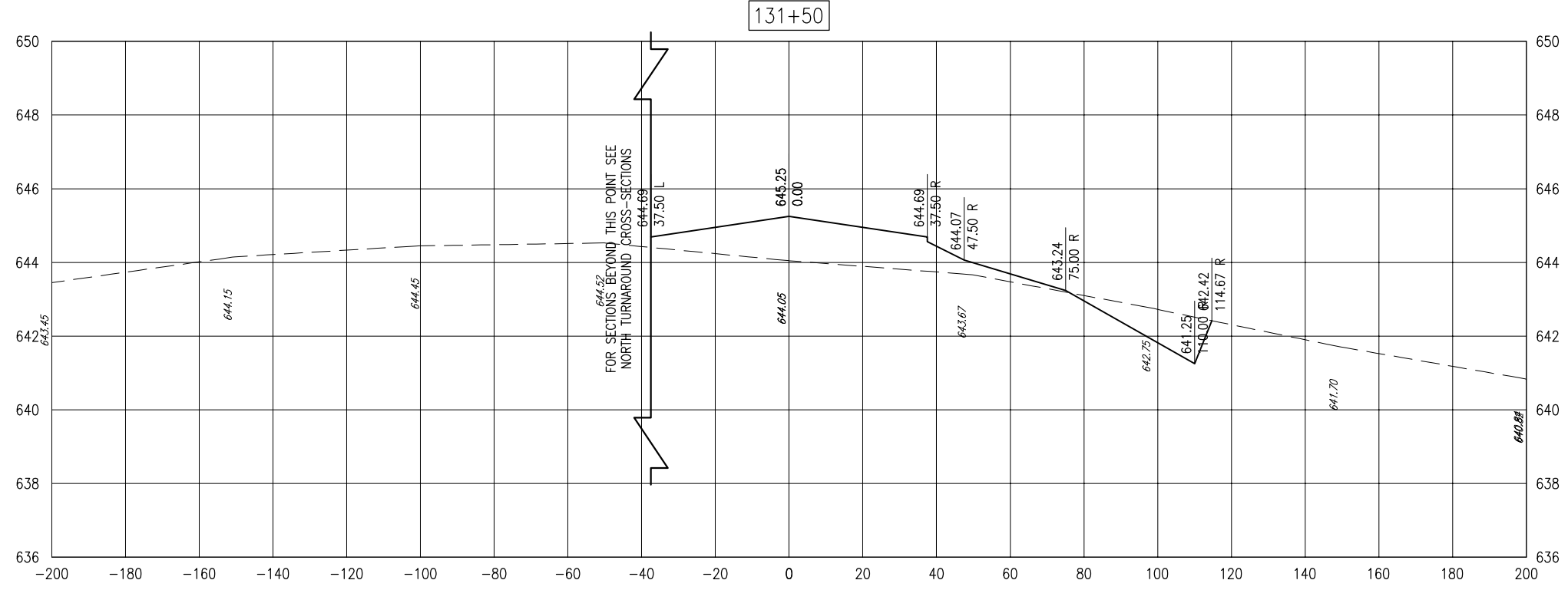
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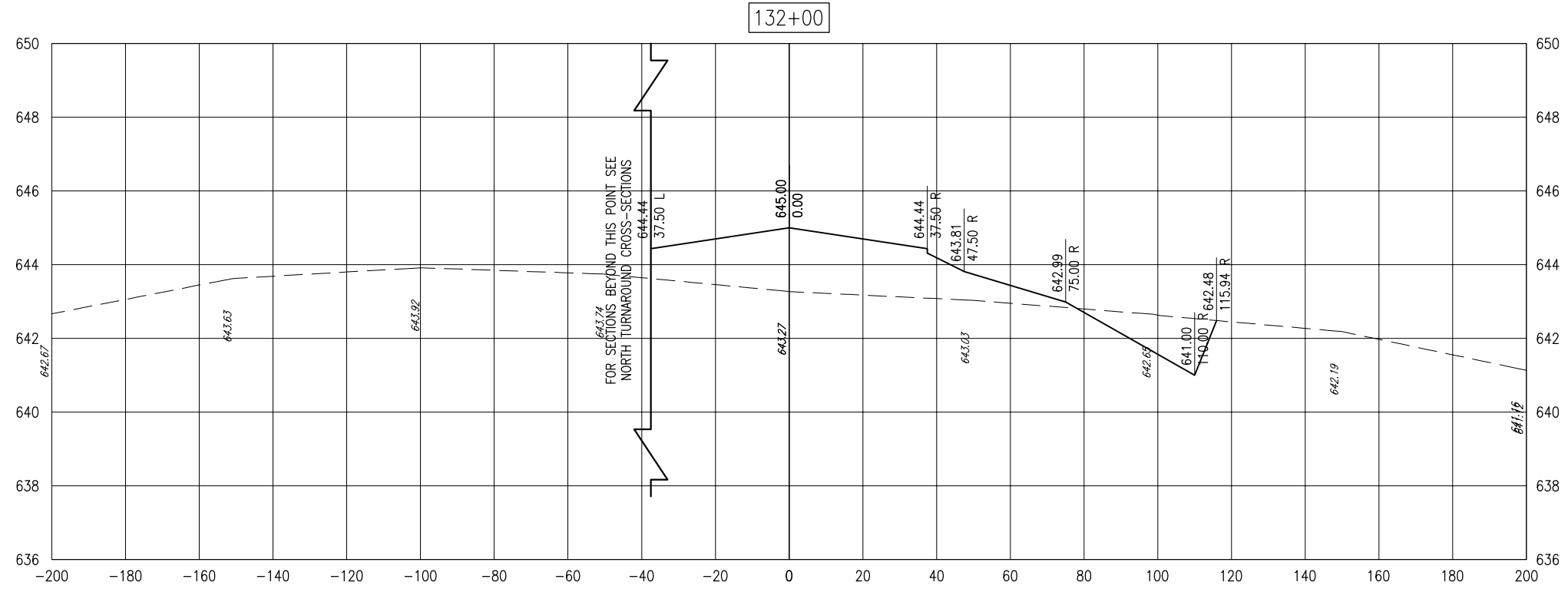
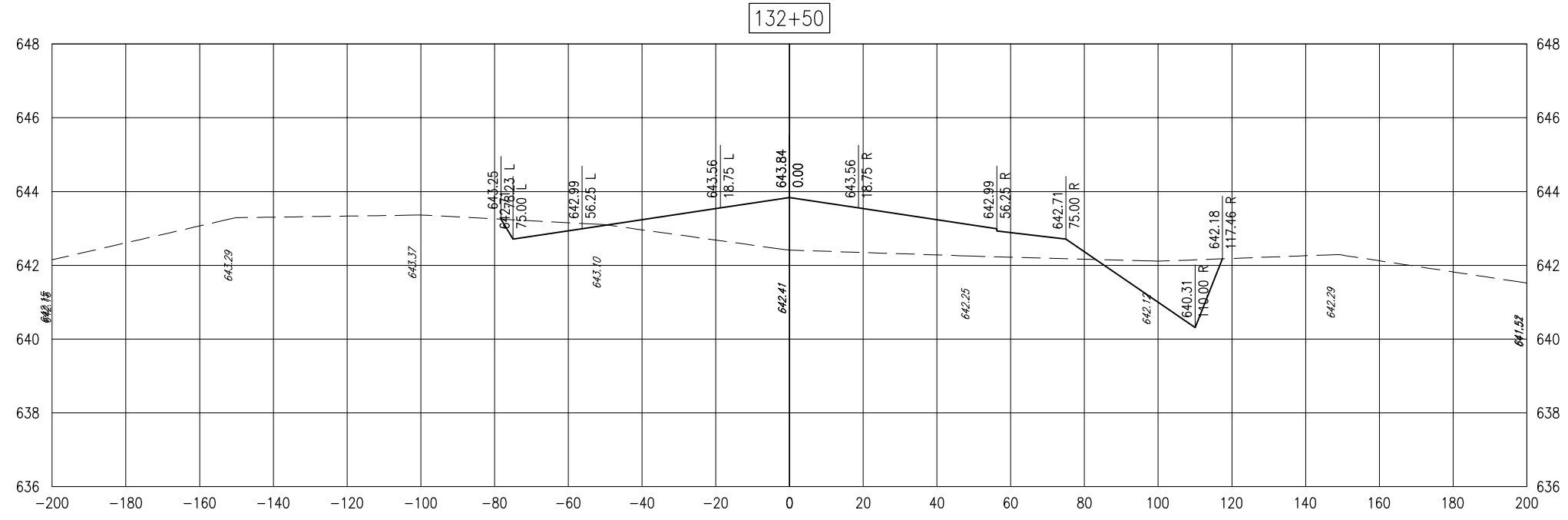
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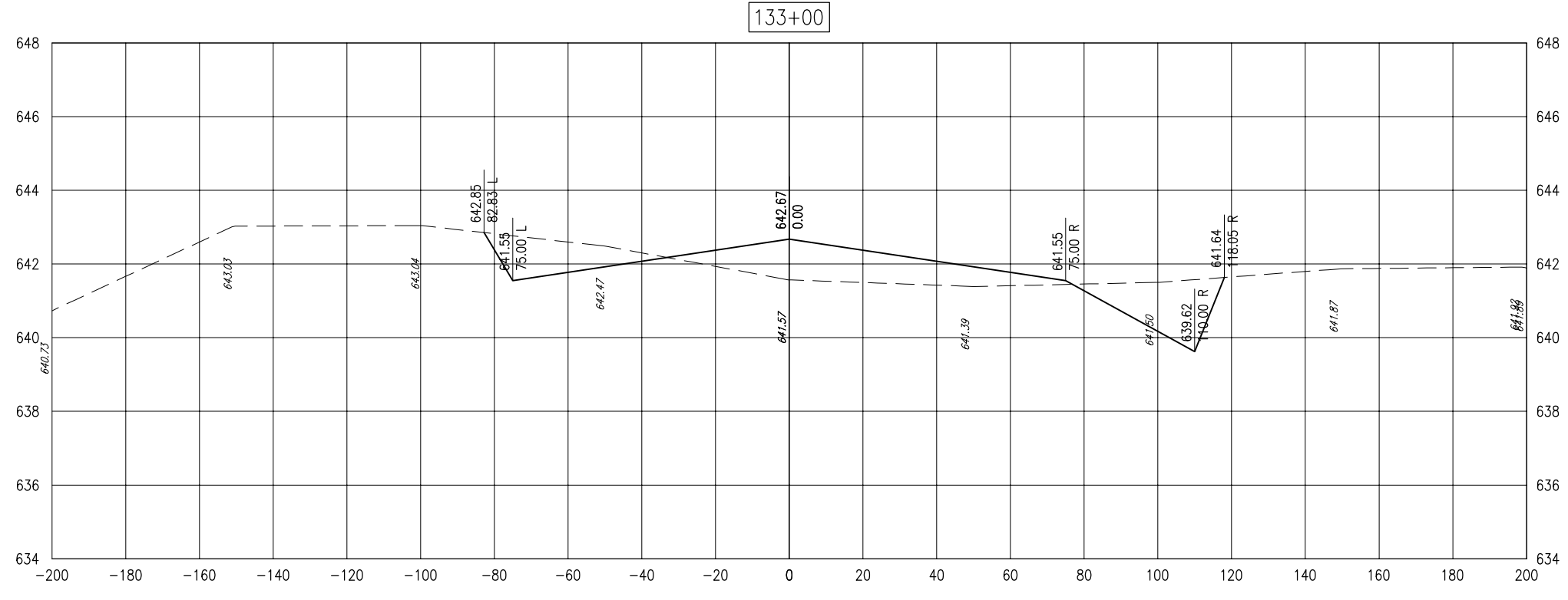
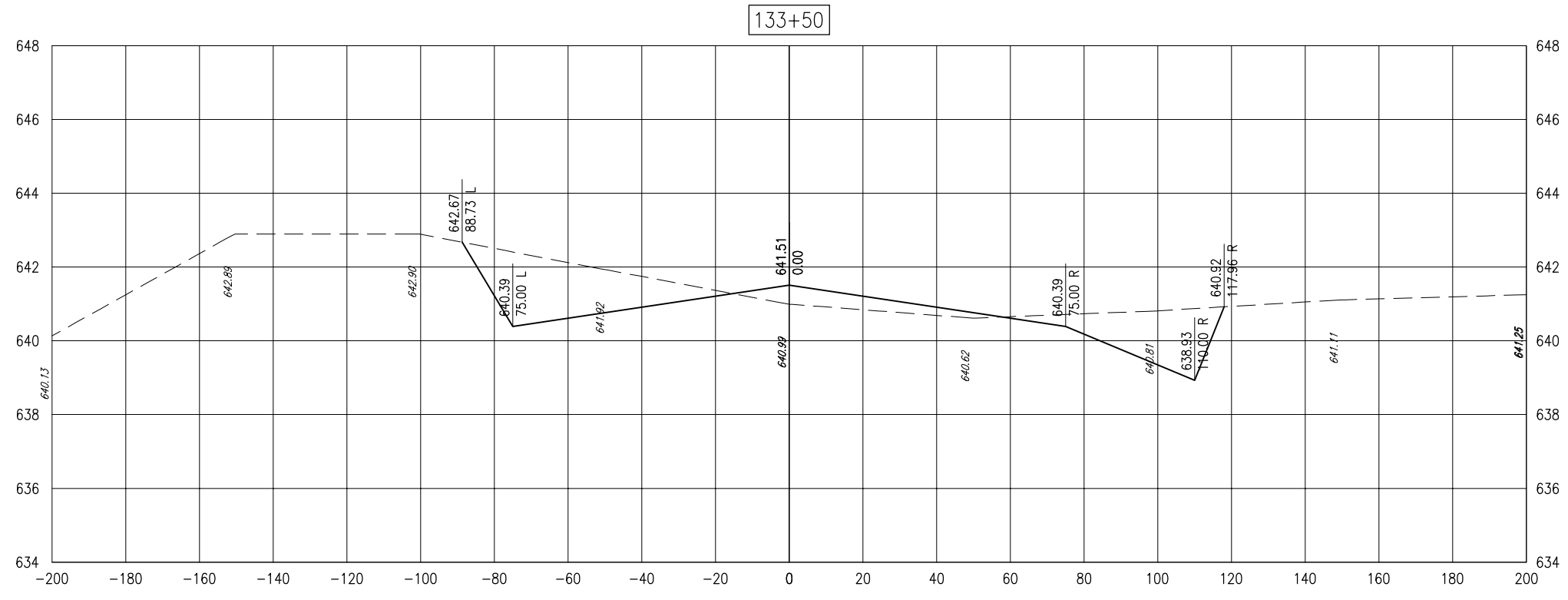
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CONSTRUCT CROSSWIND  
RUNWAY 18-36

CROSS-SECTIONS FOR  
RUNWAY 18-36 STA.  
132+00 TO STA. 132+50



REVISION	DATE

EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

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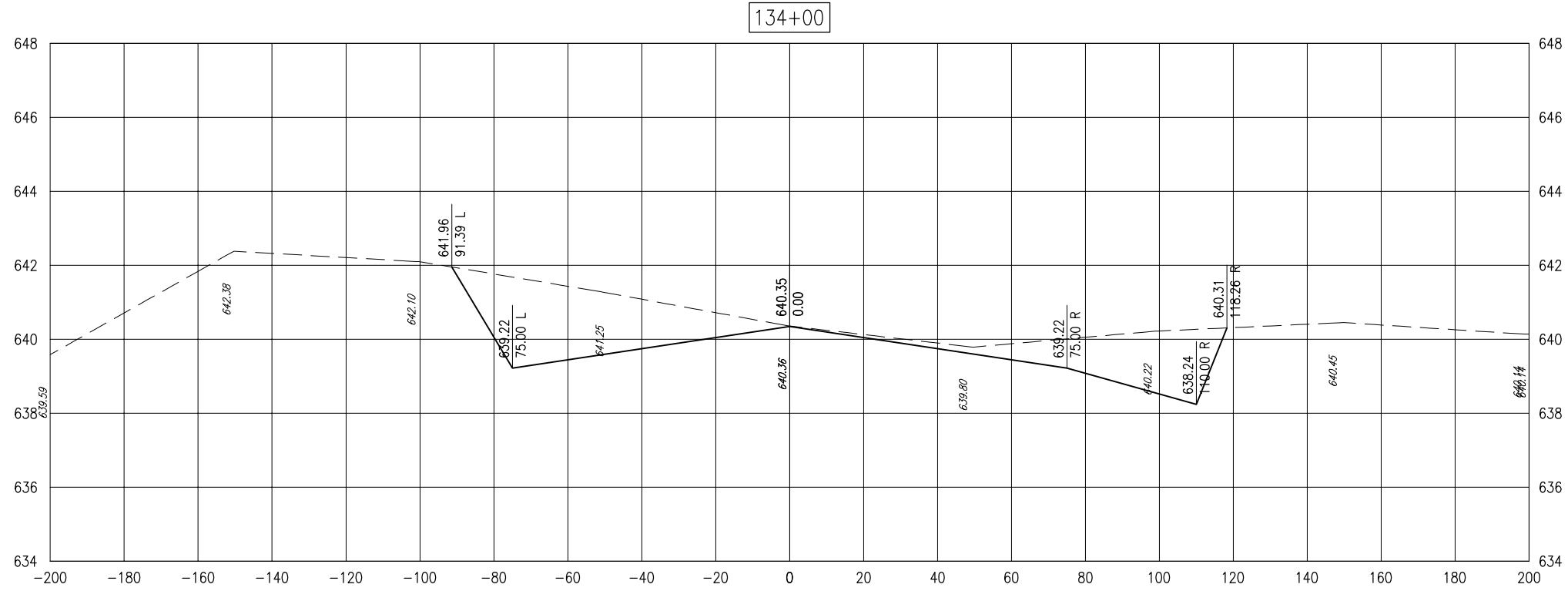
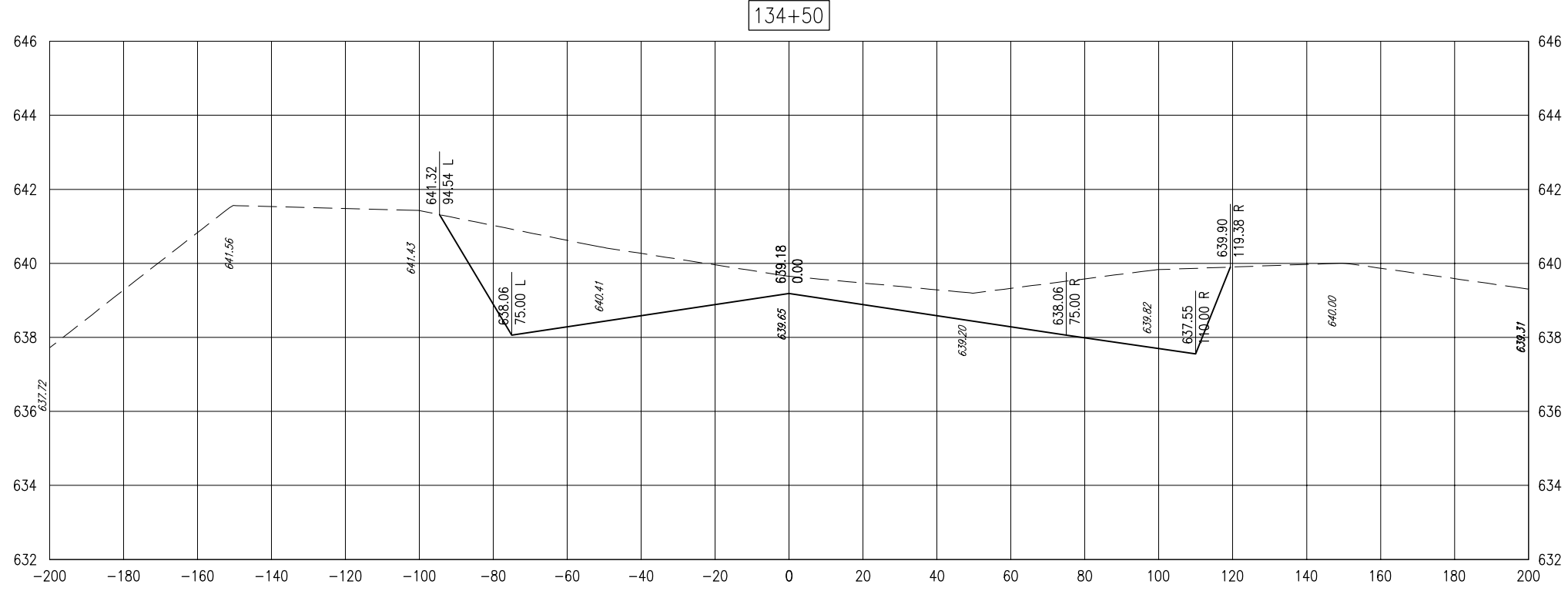
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DRAWN	MDR	01/11/11	
REVIEWED	CAH	xx/xx/xx	

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CROSS-SECTIONS FOR  
RUNWAY 18-36 STA.  
133+00 TO STA. 133+50



REVISION	DATE

**EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS**

IL PROJ.: PRG-4018      A.I.P. PROJ.: 3-17-0077-B13

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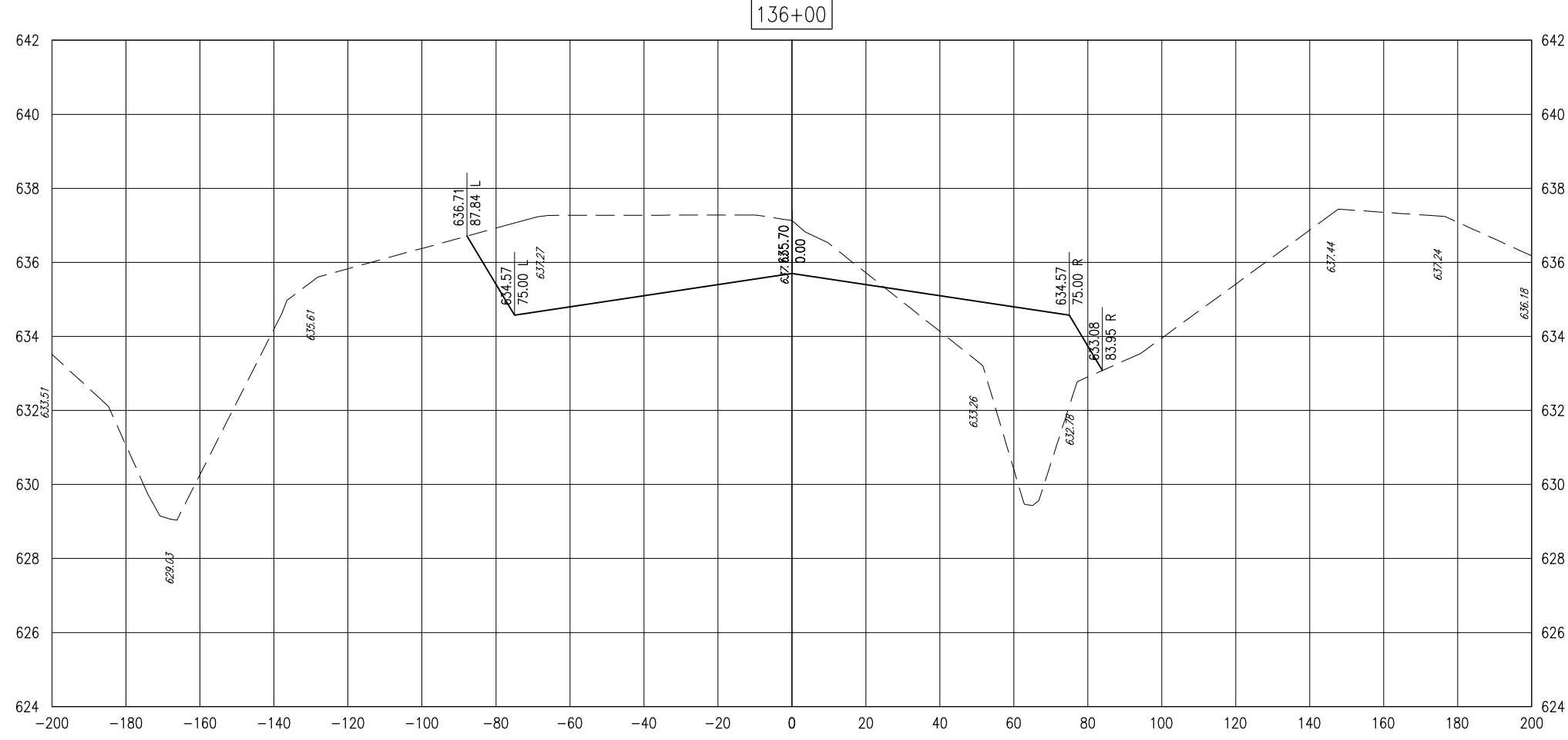
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RUNWAY 18-36**

CROSS-SECTIONS FOR  
RUNWAY 18-36 STA.  
134+00 TO STA. 134+50







**CONSTRUCT CROSSWIND  
RUNWAY 18-36**

**CROSS-SECTIONS  
FOR RUNWAY 18-36  
STA. 136+00**

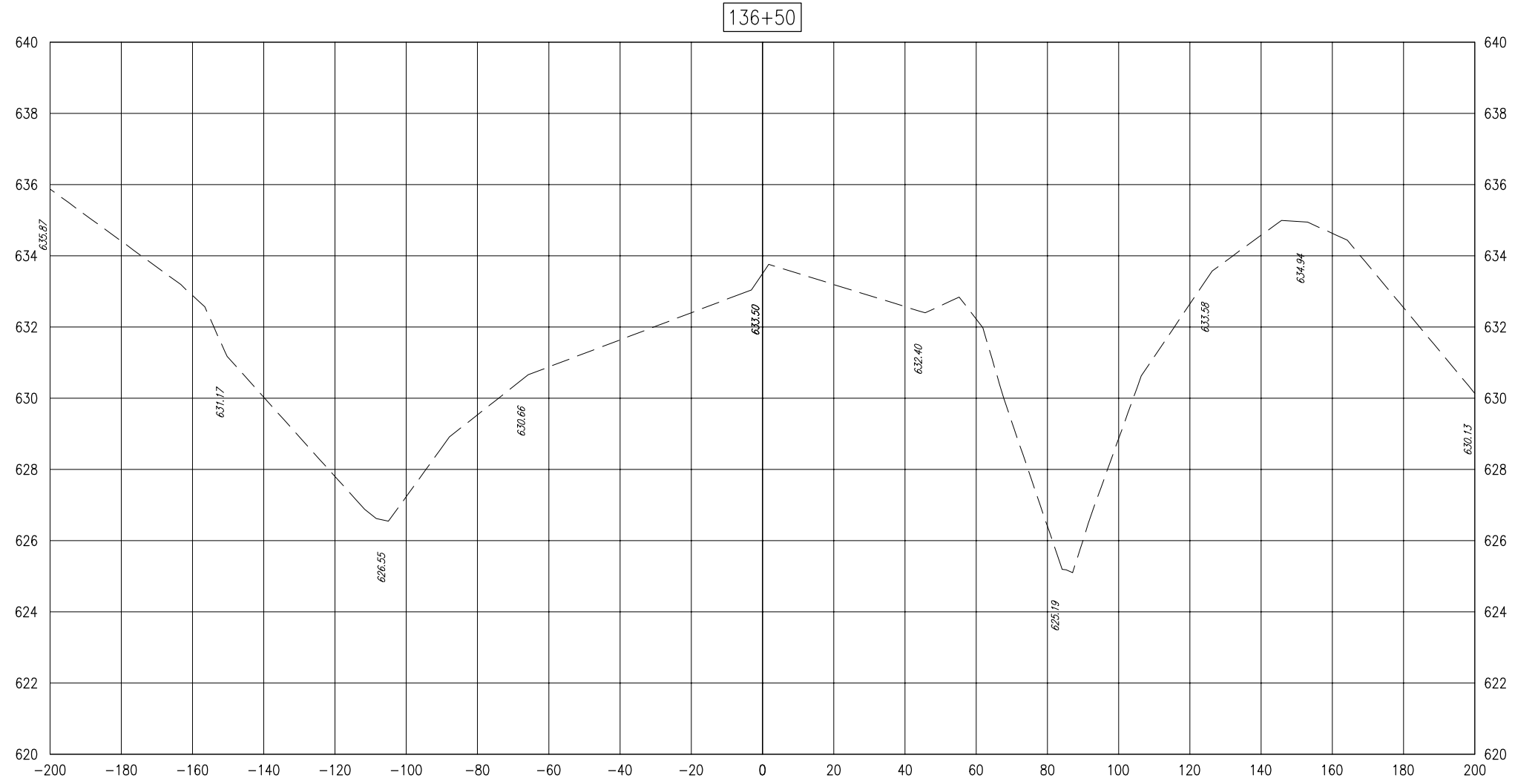


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Date 03/10/11		
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DRAWN		
REVIEWED		

**EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS**

IL PROJ.: PRG-4018 A.I.P. PROJ.: 3-17-0077-B13

DATE	REVISION



DATE	REVISION

EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

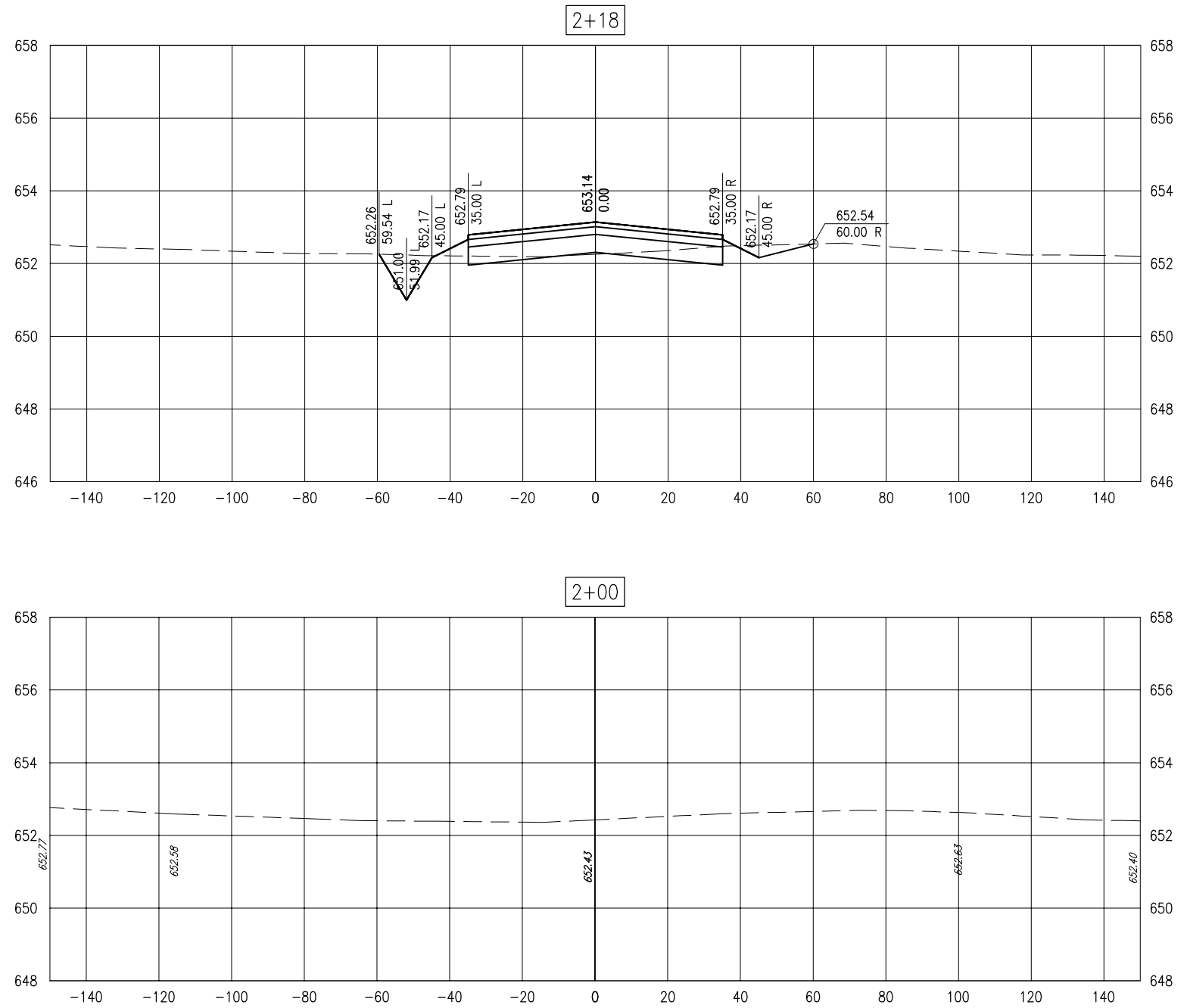
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REVIEWED	CAH	xx/xx/xx	

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CONSTRUCT CROSSWIND  
RUNWAY 18-36

CROSS-SECTIONS  
FOR RUNWAY 18-36  
STA. 136+50



DATE	REVISION

EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

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REVIEWED	CAH	xx/xx/xx	

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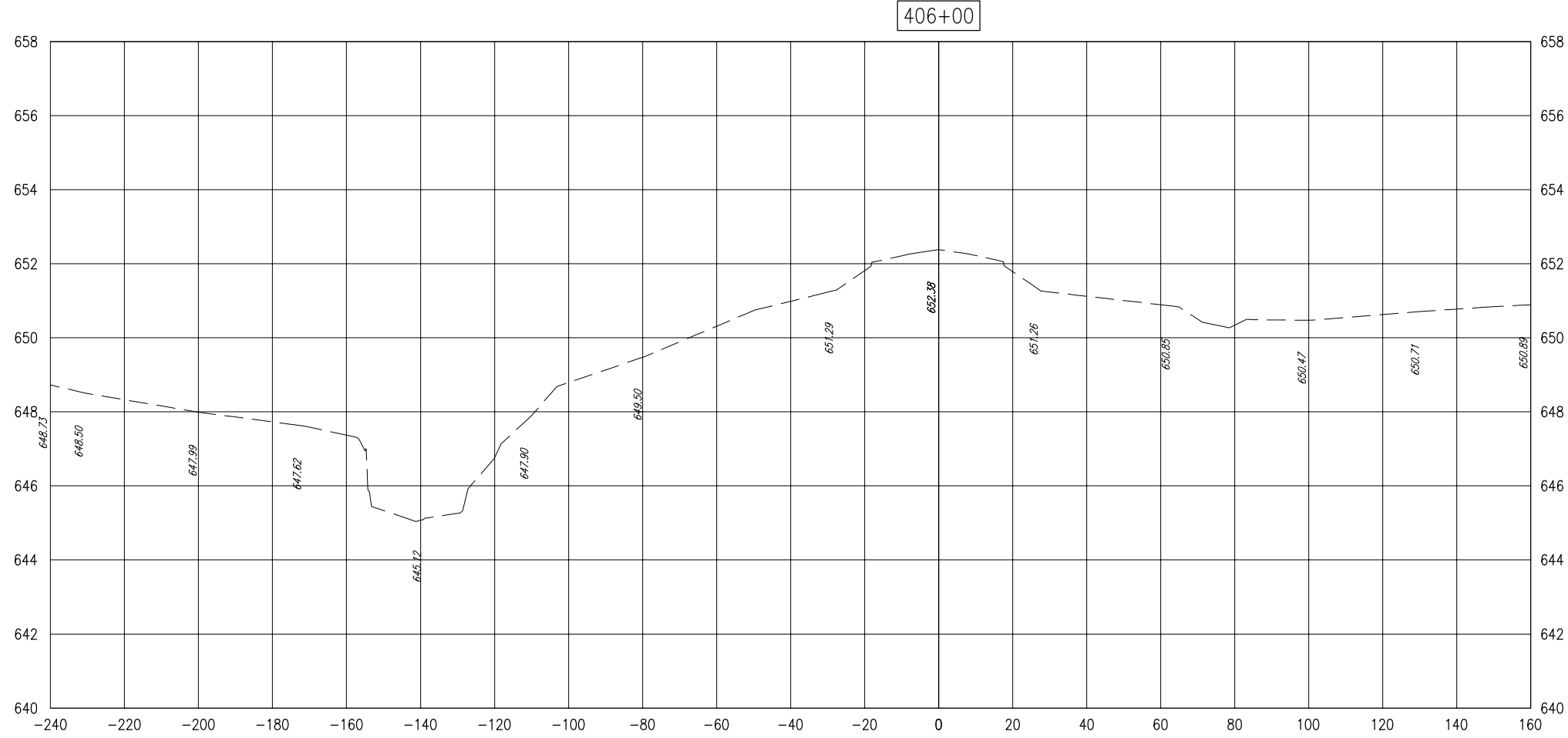
CONSTRUCT CROSSWIND  
RUNWAY 18-36

CROSS-SECTIONS FOR  
SOUTH TURNAROUND STA.  
2+00 TO STA. 2+18









DATE	REVISION

EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

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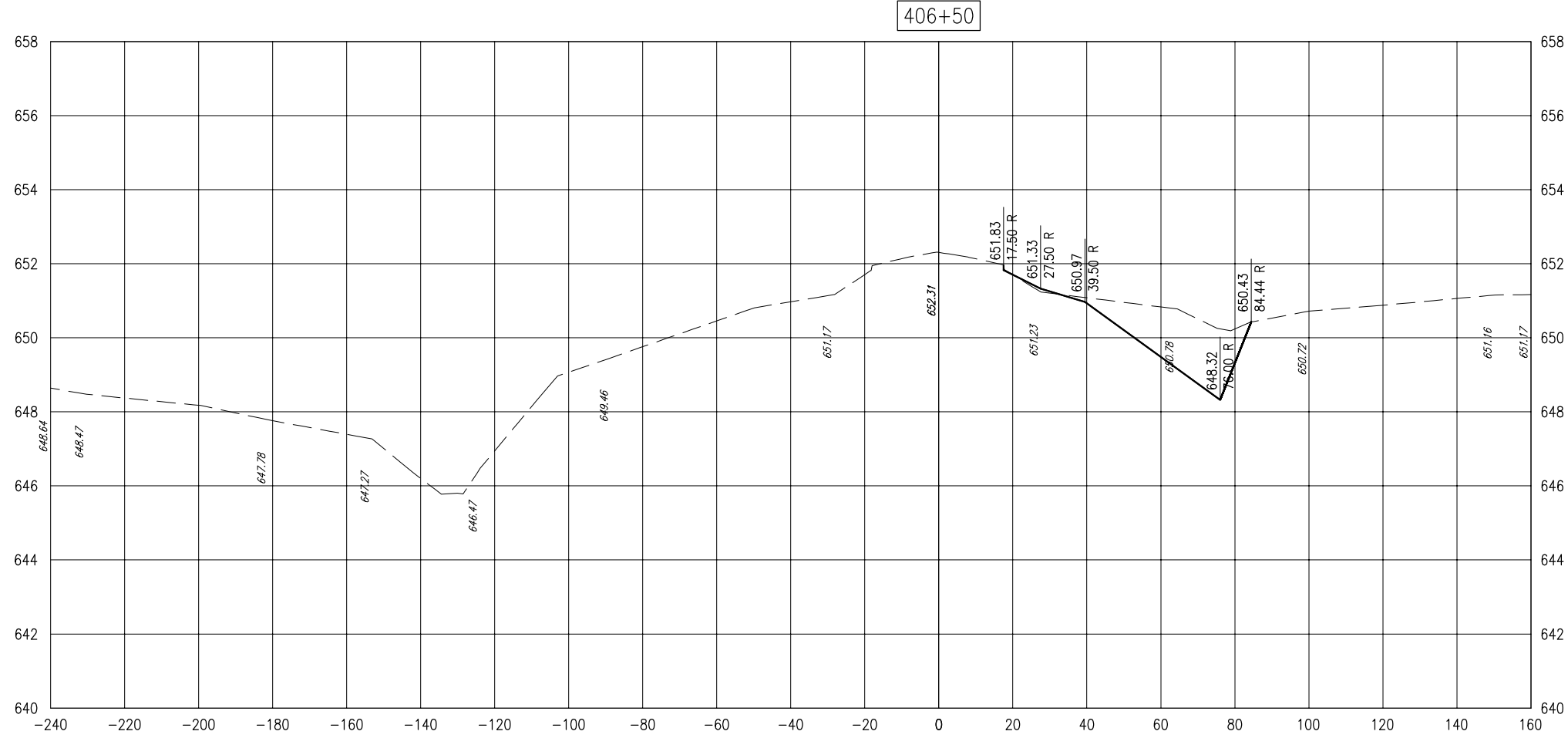
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CONSTRUCT CROSSWIND  
RUNWAY 18-36

CROSS-SECTIONS FOR  
TAXIWAY B STA. 406+00





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RUNWAY 18-36**

**CROSS-SECTIONS FOR  
TAXIWAY B STA. 406+50**

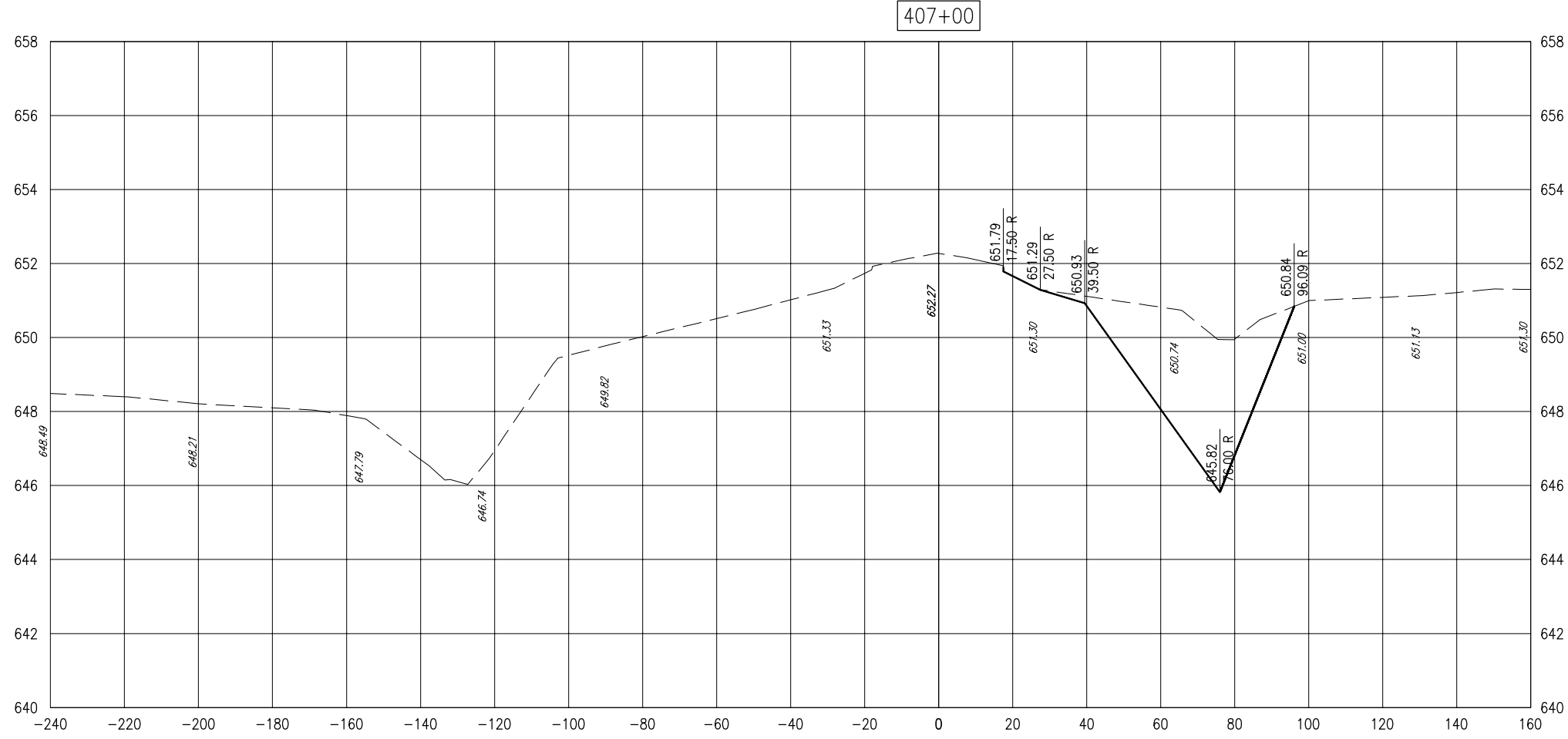


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REVIEWED	CAH xx/xx/xx

**EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS**

IL PROJ.: PRG-4018 A.I.P. PROJ.: 3-17-0077-B13

DATE	REVISION



**CONSTRUCT CROSSWIND  
RUNWAY 18-36**

CROSS-SECTIONS FOR  
TAXIWAY B STA. 407+00

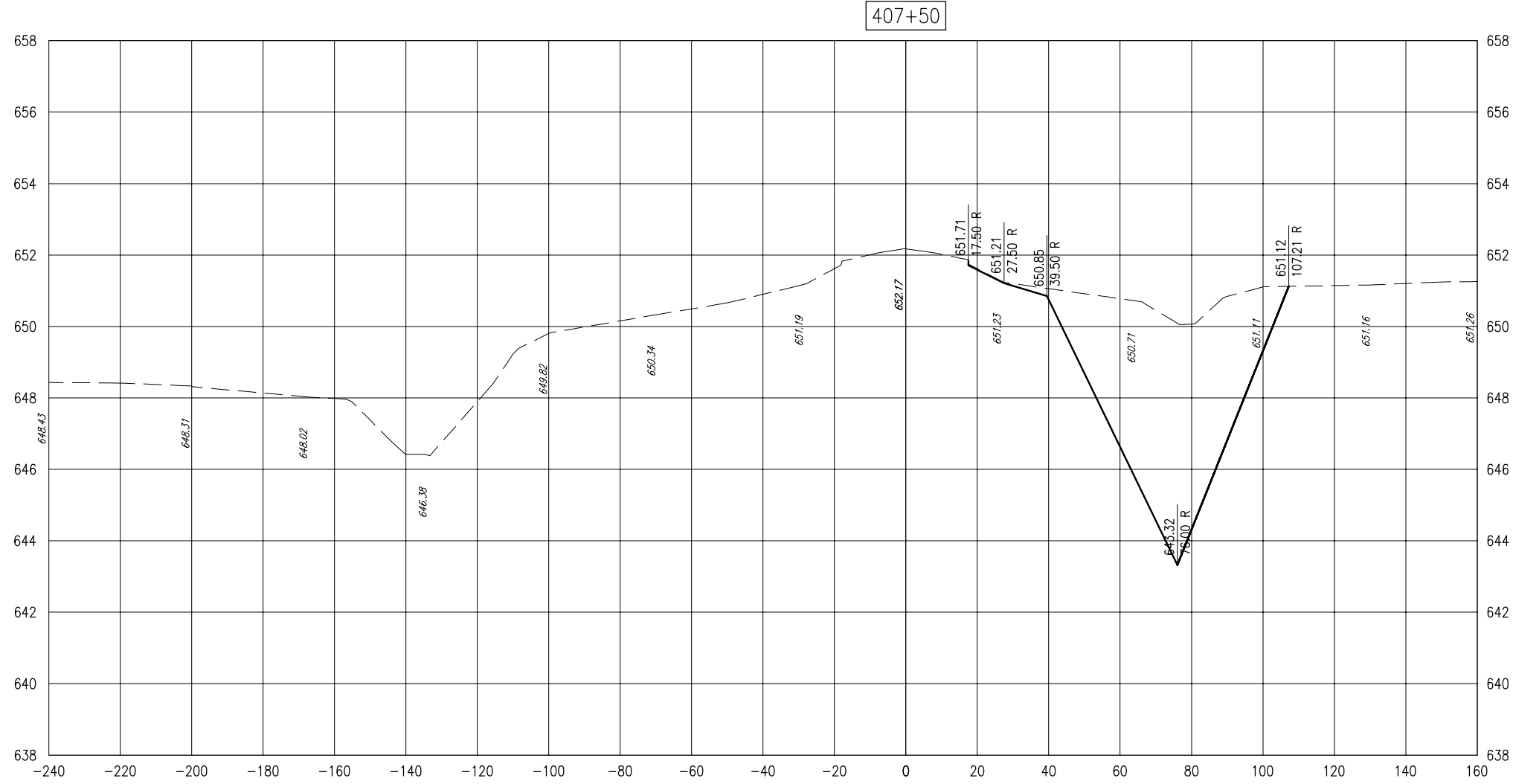
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Date 03/10/11	REVIEWED	

EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

IL PROJ.: PRG-4018 A.I.P. PROJ.: 3-17-0077-B13

DATE	REVISION



REVISION	DATE

EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

IL PROJ.: PRG-4018 A.I.P. PROJ.: 3-17-0077-B13

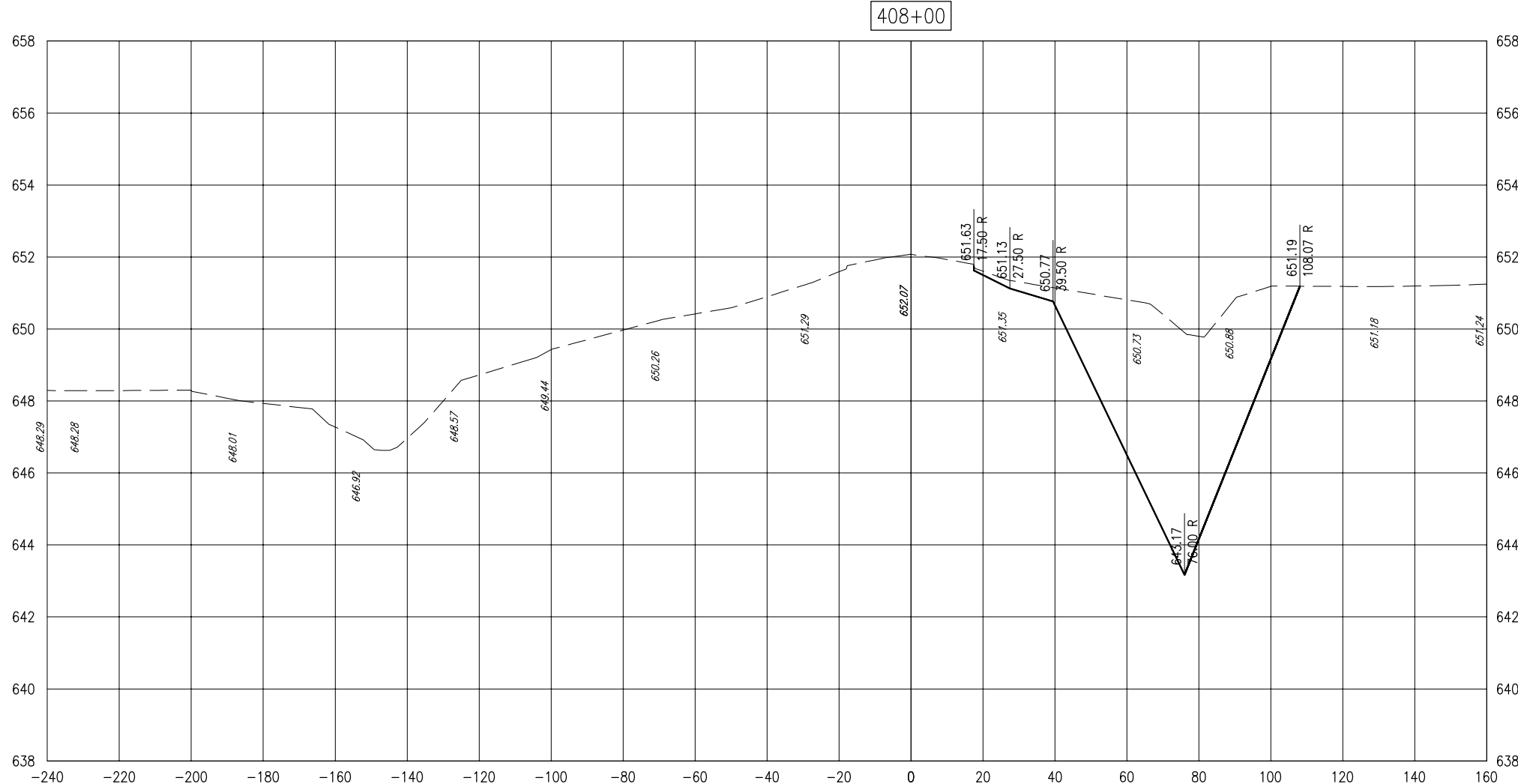
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CONSTRUCT CROSSWIND  
RUNWAY 18-36

CROSS-SECTIONS FOR  
TAXIWAY B STA. 407+50

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408+00

FOR SECTIONS BEYOND THIS POINT SEE N-S  
RUNWAY AND TAXIWAY B CROSS-SECTIONS

DATE	REVISION

EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

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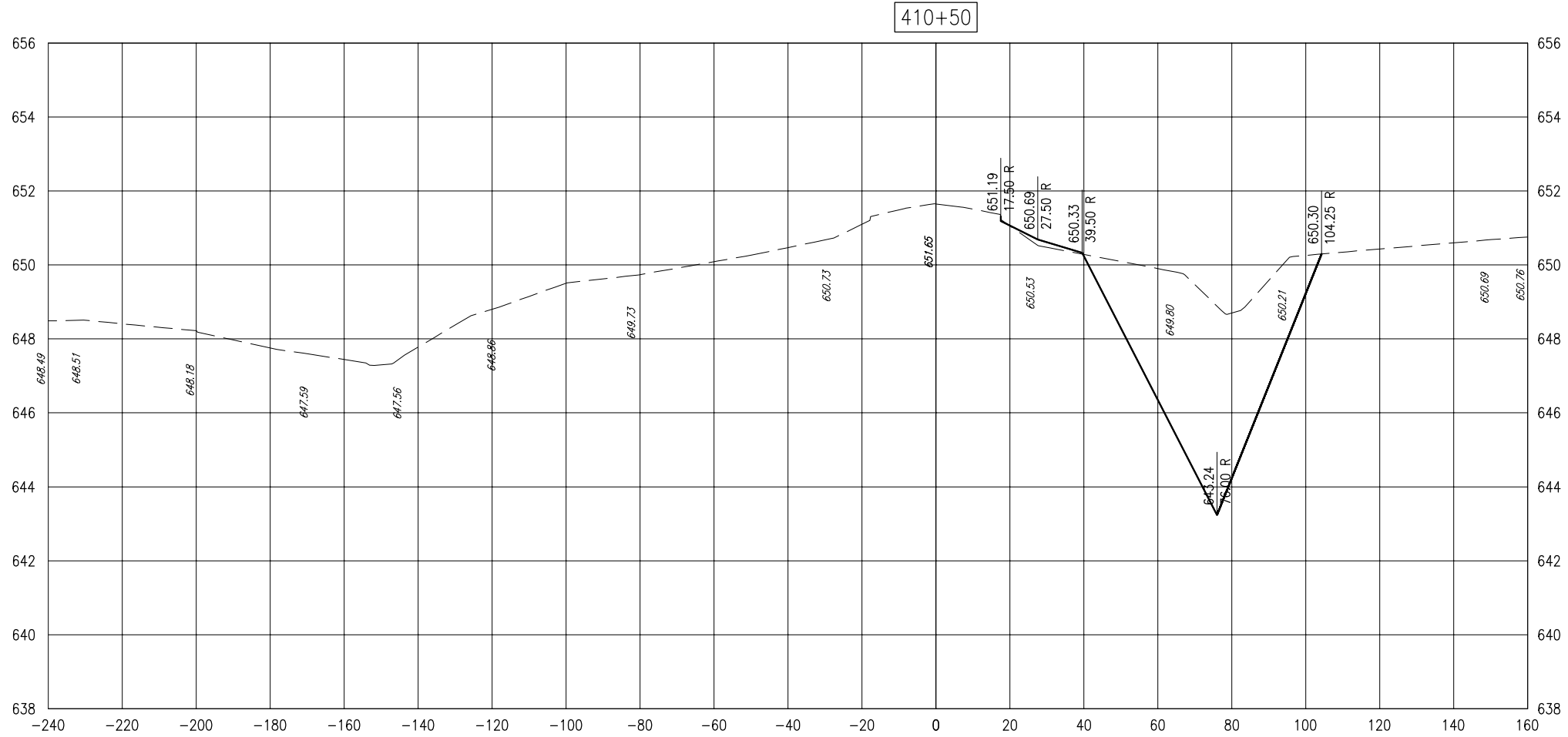
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CONSTRUCT CROSSWIND  
RUNWAY 18-36

CROSS-SECTIONS FOR  
TAXIWAY B STA. 408+00

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410+50

FOR SECTIONS BEFORE THIS POINT SEE N-S RUNWAY AND TAXIWAY B CROSS-SECTIONS

REVISION	DATE

EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

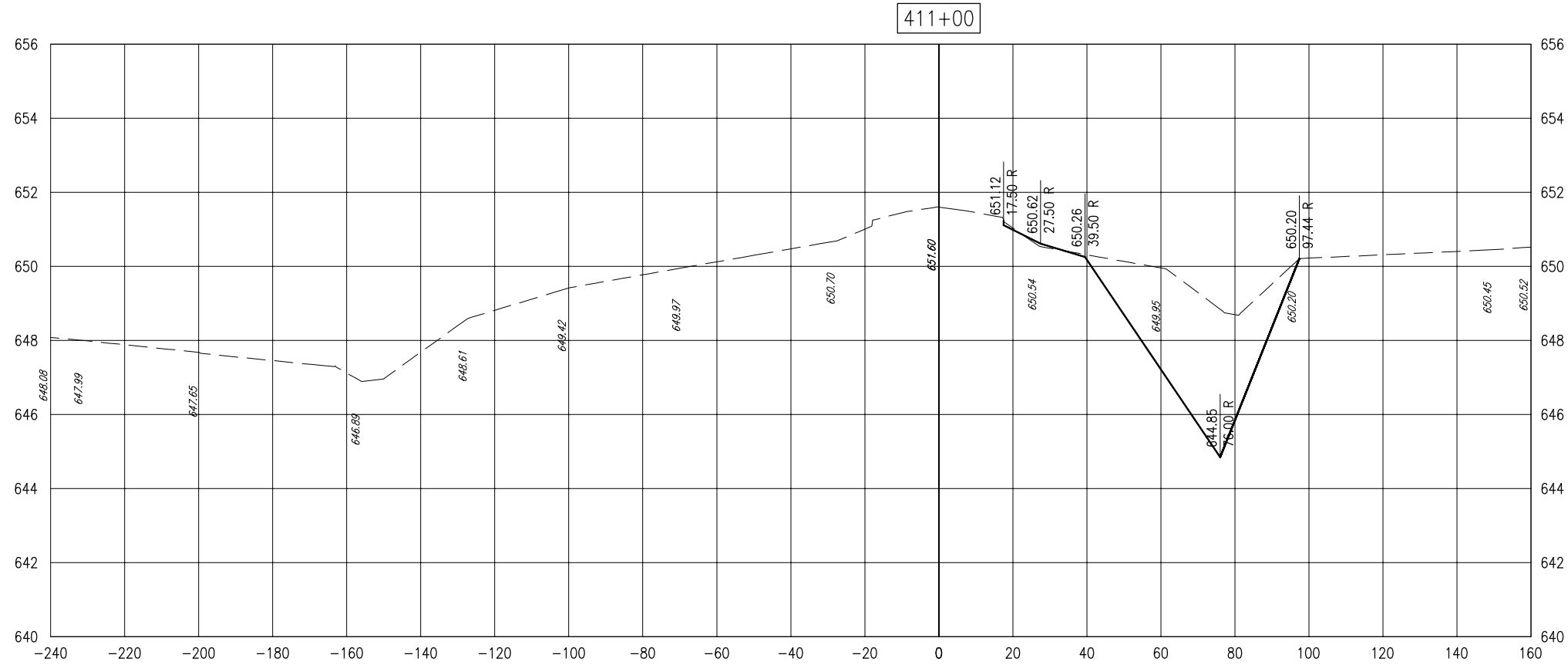
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CONSTRUCT CROSSWIND  
RUNWAY 18-36

CROSS-SECTIONS FOR  
TAXIWAY B STA. 410+50



DATE	REVISION

EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

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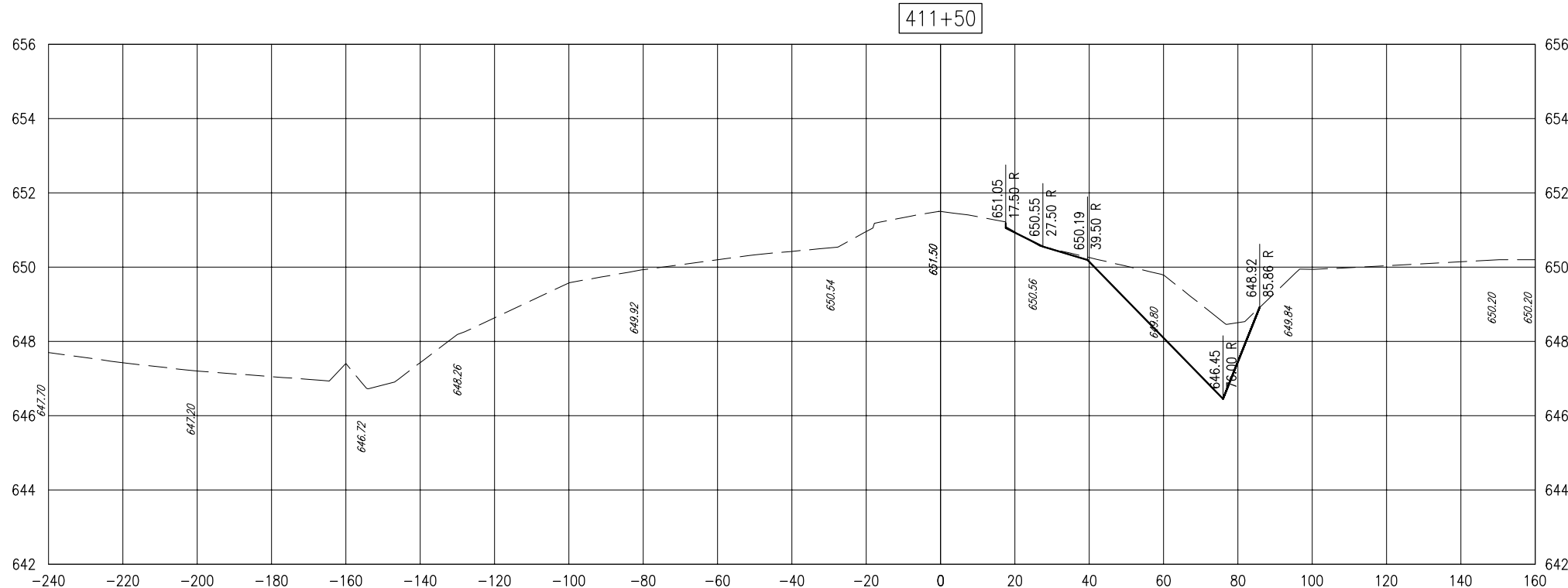
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REVIEWED CAH	xx/xx/xx

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CONSTRUCT CROSSWIND  
RUNWAY 18-36

CROSS-SECTIONS FOR  
TAXIWAY B STA. 411+00



DATE	REVISION

EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

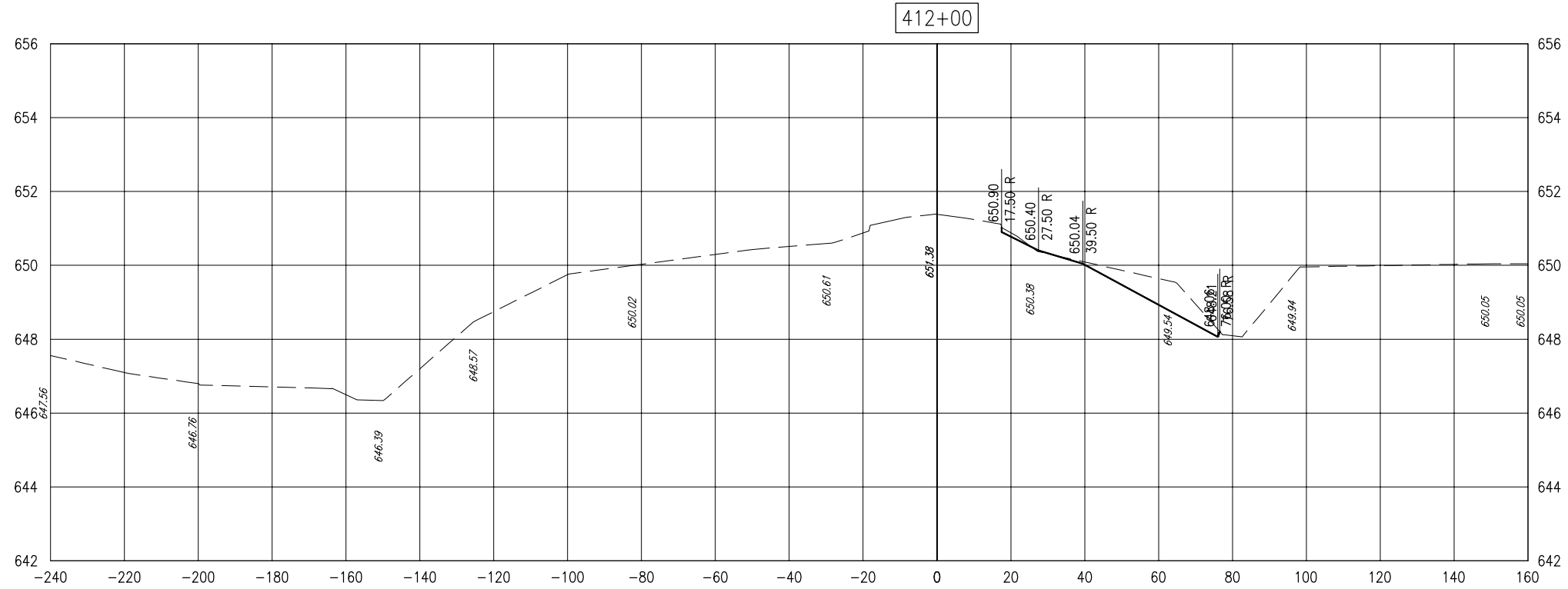
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REVIEWED CAH	xx/xx/xx

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RUNWAY 18-36

CROSS-SECTIONS FOR  
TAXIWAY B STA. 411+50



REVISION	DATE

EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

IL PROJ.: PRG-4018 A.I.P. PROJ.: 3-17-0077-B13

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LAYOUT MDR 01/11/11	REVIEWED CAH xx/xx/xx
DRAWN MDR 01/11/11	

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RUNWAY 18-36

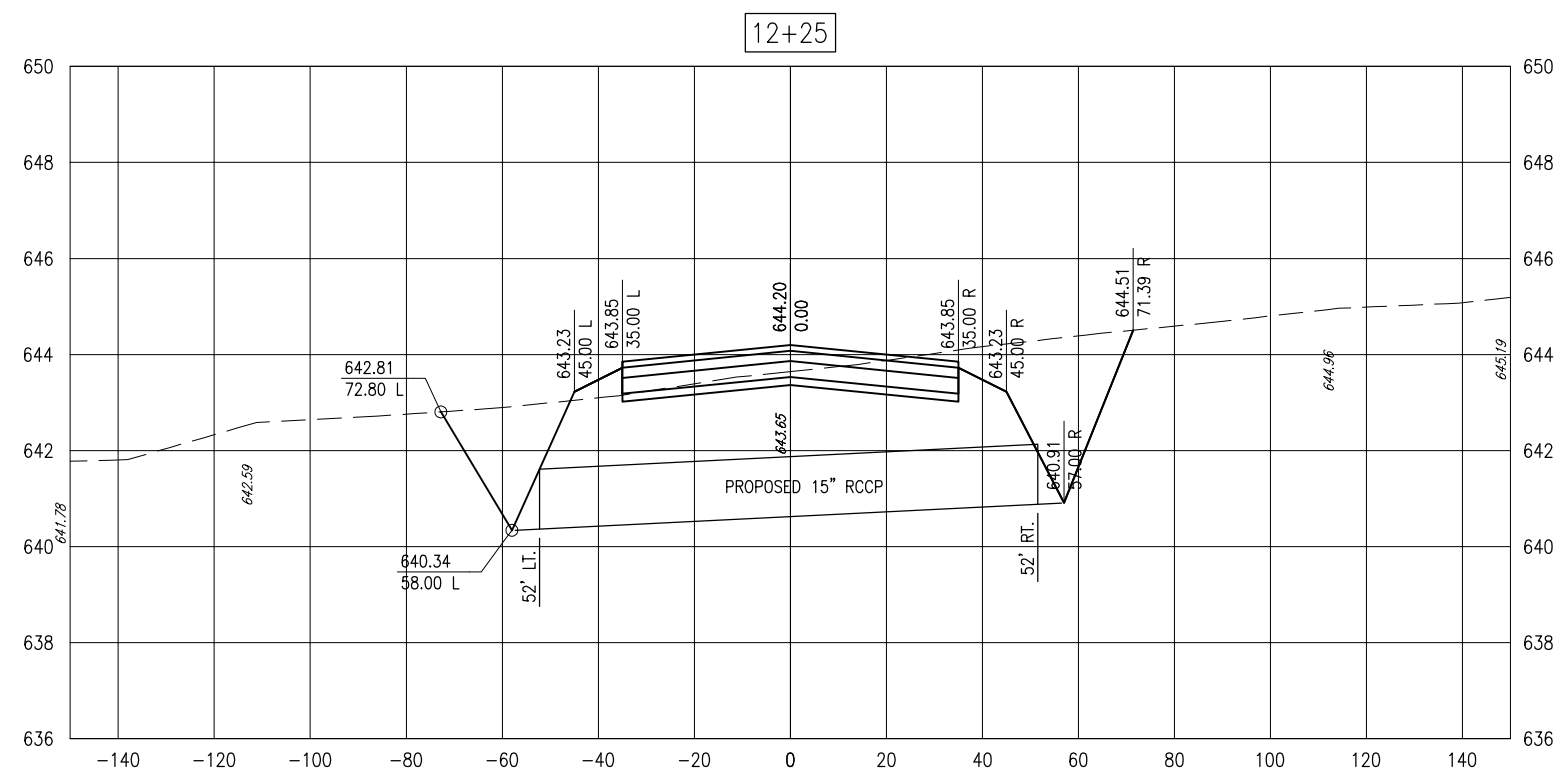
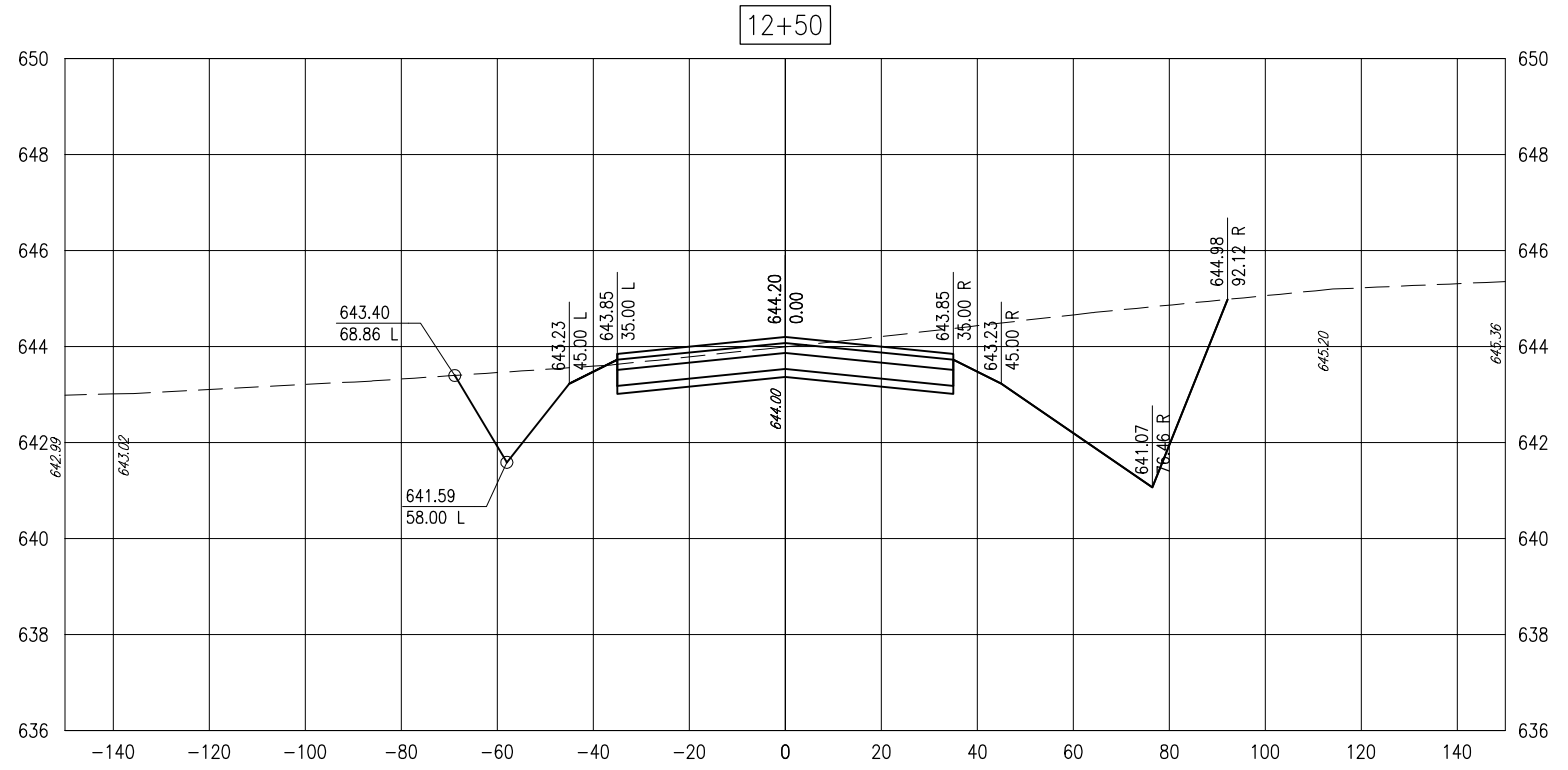
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REVISION	DATE

EDGAR COUNTY AIRPORT  
PARIS, ILLINOIS

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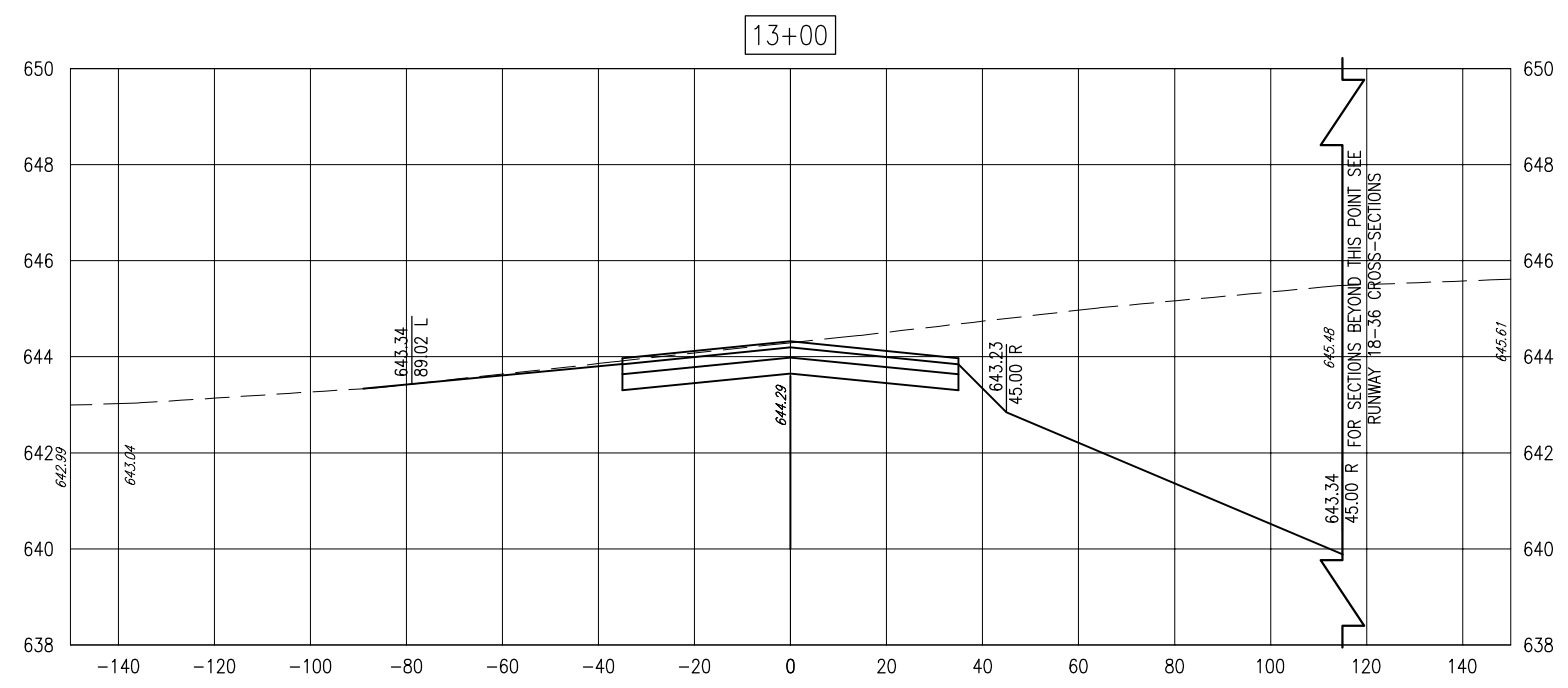
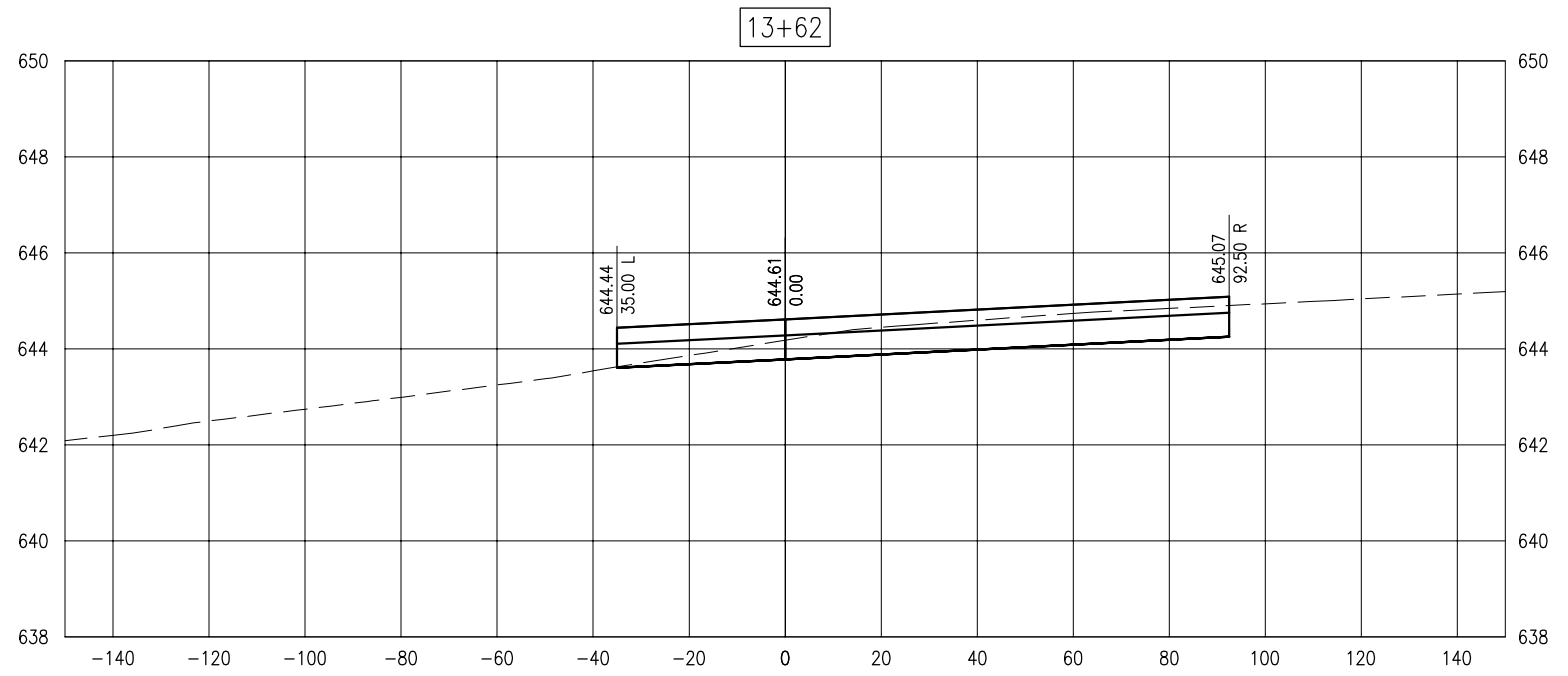
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RUNWAY 18-36

CROSS-SECTIONS FOR  
NORTH TURNAROUND STA.  
12+25 TO STA. 12+50

FOR SECTIONS BEYOND THIS POINT SEE  
N-S RUNWAY CROSS-SECTIONS



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REVIEWED	CAH xx/xx/xx

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CROSS SECTIONS FOR  
NORTH TURNAROUND STA.  
13+00 TO STA. 13+62